

Page 1 of 1

Date 3/3/25

ROUTE	FAI 290	DESCRIPTION	Bridge Boring	LOGGED BY	DV
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SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8739528, Longitude -87.7304833	
		DRILLING METHOD	Mobile B-57	
			HSA	HAMMER TYPE
				HAMMER EFF (%)
				AUTO
				92.1

STRUCT. NO. 016-2093	D	B	U	M	Surface Water Elev. N/A ft	D	B	U	M
Station 203+76.63	E	L	C	O	Stream Bed Elev. N/A ft	E	L	C	O
	P	O	S	I		P	O	S	I
BORING NO. BSB-05	T	W		S	Groundwater Elev.:	T	W		S
Station * 15+31.74	H	S	Qu	T	First Encounter None ft	H	S	Qu	T
Offset * 23.26ft RT					Upon Completion N/A ft				
Ground Surface Elev. 7.12 ft	(ft)	(/6")	(tsf)	(%)	After N/A Hrs. N/A ft	(ft)	(/6")	(tsf)	(%)

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Page 1 of 2

Date 3/3/25

ROUTE	FAI 290	DESCRIPTION	Bridge Boring	LOGGED BY	DV
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SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8739583, Longitude -87.7302139	
		DRILLING METHOD	Mobile B-57	
			HSA	AUTO
				HAMMER TYPE
				HAMMER EFF (%)
				92.1

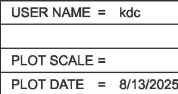
STRUCT. NO.	016-2093	D	B	U	M	Surface Water Elev.	N/A	ft	D	B	U	M			
Station	203+76.63	E	L	C	O	Stream Bed Elev.	N/A	ft	E	L	C	O			
		T	W	S	T				H	S	Qu	T			
BORING NO.	BSB-06	H	S	Qu	T	Groundwater Elev.:									
Station	* 16+05.14					First Encounter	None	ft							
Offset	* 22.48ft RT					Upon Completion	N/A	ft							
Ground Surface Elev.	6.62	ft	(ft)	(/6")	(tsf)	(%)	After	N/A	Hrs.	N/A	ft	(ft)	(/6")	(tsf)	(%)

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

* Boring No. BSB-05 is located at Keeler Avenue Station is 204+17.01 Offset 40.65' RT.

* Boring No. BSB-06 is located at Keeler Avenue Station is 204+17.12 Offset 32.73' RT.



DESIGNED	-	RH
CHECKED	-	GJH
DRAWN	-	RH
CHECKED	-	GJH

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORINGS (5 OF 11)
STRUCTURE NO. 016-2093

SHEET S-57 OF 63 SHEETS

F.A.I
RTE

290

250

SECTION

22 KEELI

	11.1.2015
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TOTAL SHEETS	SHEET NO.
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SHEETS	NO.
283	21

203	20
10 62144	

NO. 62041

PROJECT

MODEL: Soil Borings 5 [Sheet]
FILE NAME: pwr/cvilltech-pw.bentley.com/cvilltech-ow/Documents/Projects/3855/CAD/CADD Steels/17-Structures/D162U41-SHT-057-Soil Borings5

8/14/2025 8:32:28 AM

Page 2 of 2

Date 3/3/25

STRUCT. NO. 016-2093	D E P T H	B L O W S	U C S	M O I S T	Surface Water Elev. N/A	ft	
Station 203+76.63					Stream Bed Elev. N/A	ft	
BORING NO. BSB-06					Groundwater Elev.:		
Station * 16+05.14					First Encounter	None	ft
Offset * 22.48ft RT					Upon Completion	N/A	ft
Ground Surface Elev. 6.62	ft	(ft)	(/6")	(tsf)	(%)	After N/A Hrs. N/A	ft

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Page 1 of 2

Date 3/2/25

STRUCT. NO. _____	016-2093	D E P T H	B L O W S	U C S	M O I S T	Surface Water Elev. _____	N/A	ft	D E P T H	B L O W S	U C S	M O I S T	
Station _____	203+76.63					Stream Bed Elev. _____	N/A	ft					
BORING NO. _____	BSB-07					Groundwater Elev.:							
Station _____	* 14+66.76					First Encounter _____	-26.4	ft ▼					
Offset _____	* 28.17ft LT					Upon Completion _____	N/A	ft					
Ground Surface Elev. _____	7.12	ft	(ft)	(/6")	(tsf)	(%)	After _____	N/A	ft	(ft)	(/6")	(tsf)	(%)

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

SOIL BORINGS (6 OF 11)
STRUCTURE NO. 016-2093

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	202
		CONTRACT NO. 62U41		
		ILLINOIS FED. AID PROJECT		

Page 2 of 2

Date 3/2/25

ROUTE FAI 290 DESCRIPTION Bridge Boring LOGGED BY DV

SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8737389, Longitude -87.7304778	
		DRILLING METHOD	Mobile B-57	
			HSA	
			HAMMER TYPE	AUTO
			HAMMER EFF (%)	92.1

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

* Boring No. BSB-07 is located at Keeler Avenue Station is 203+41.49 Offset 41.15' LT.

Page 1 of 1

Date 3/2/25

ROUTE FAI 290 DESCRIPTION Bridge Boring LOGGED BY DV

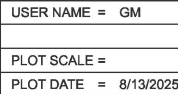
SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8737389, Longitude -87.7302056	HAMMER TYPE	AUTO
		DRILLING METHOD	Mobile B-57	HAMMER EFF (%)	92.1
			HSA		

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

* Boring No. BSB-08 is located at Keeler Avenue Station is 203+39.63 Offset 32.95' LT.



DESIGNED	-	RH
CHECKED	-	GJH
DRAWN	-	RH
CHECKED	-	GJH

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORINGS (7 OF 11)
STRUCTURE NO. 016-2093

SHEET S-59 OF 63 SHEETS

F.A.I
PTE

SECTION

COUNTY

TOTAL	5
EFFECT	

EEIS	
see	

283

COOK	283	
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		CONTRACT
ILLINOIS	FED. AID PROJECT	



Illinois Department
of Transportation
Division of Highways
GSG Consultants, Inc.

SOIL BORING LOG

Page 1 of 1

Date 2/5/24

ROUTE FAI 290 DESCRIPTION Retaining Wall Boring LOGGED BY MA

SECTION Keeler Avenue over FAI 290 LOCATION SEC. 15, TWP. 39N, RNG. 13E

Latitude 41.8735444, Longitude -87.7311806

COUNTY COOK DRILLING RIG DRILLING METHOD Mobile B-57 HSA HAMMER TYPE HAMMER EFF (%) AUTO 89.0

STRUCT. NO. N/A Station N/A BORING NO. RWB-01 Station * 12+74.20 Offset * 39.48ft RT Ground Surface Elev. 8.00 ft

D E P T H (ft)	B L O W S (/6")	U C S (tsf)	M O I S T (%)	Surface Water Elev. N/A ft	Stream Bed Elev. N/A ft	D E P T H (ft)	B L O W S (/6")	U C S (tsf)	M O I S T (%)
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6 inches of Topsoil	7.50									Very Stiff to Hard Gray, Moist SILTY CLAY, trace gravel (CL/ML) (continued)
Gray, Moist		5					3			
FILL: SILTY CLAY, trace gravel		3	4.2	14			6	4.8	22	
		5	B				9	B		
	4.50									
Hard		3					4			
Gray, Moist		5	5.0	16			7	5.4	13	
SILTY CLAY, trace gravel (CL/ML)		6	B				7	B		
		4					3			
		5	4.4	16			7	5.2	13	
		9	B				7	B		
	-0.50	5					6			
Very Stiff		12	3.0	19			26	4.5	11	
Gray, Moist		16	P				19	P		
SILTY CLAY LOAM, trace gravel (ML/CL)										
	-3.00	4								
Very Stiff to Hard		7	4.0	13						
Gray, Moist		7	B							
SILTY CLAY, trace gravel (CL/ML)										
		4					9			
		7	6.0	15			43		8	
		8	B				50/2"			
		3								
		7	6.3	18						
		11	B							
	-29.50									
WEATHERED LIMESTONE										
		4								
Auger refusal at 38.5 feet		9	6.0	20						
End of Boring		11	B							
	-20									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)



Illinois Department
of Transportation
Division of Highways
GSG Consultants, Inc.

SOIL BORING LOG

Page 1 of 1

Date 2/5/24

ROUTE FAI 290 DESCRIPTION Retaining Wall Boring LOGGED BY MA

SECTION Keeler Avenue over FAI 290 LOCATION SEC. 15, TWP. 39N, RNG. 13E

Latitude 41.8735667, Longitude -87.7307694

COUNTY COOK DRILLING RIG DRILLING METHOD Mobile B-57 HSA HAMMER TYPE HAMMER EFF (%) AUTO 89.0

STRUCT. NO. N/A Station N/A BORING NO. RWB-02 Station * 13+86.29 Offset * 33.25ft RT Ground Surface Elev. 7.50 ft

D E P T H (ft)	B L O W S (/6")	U C S (tsf)	M O I S T (%)	Surface Water Elev. N/A ft	Stream Bed Elev. N/A ft	D E P T H (ft)	B L O W S (/6")	U C S (tsf)	M O I S T (%)
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9 inches of Asphalt										Hard Gray, Moist SILTY CLAY, trace gravel (CL/ML) (continued)
6 inches of Gravel Base	6.75									
	6.25	2					5			
Gray, Moist		4	1.3	15			8	5.6	9	
FILL: SILTY CLAY, trace gravel		2	B				16	B		
	4.00									
Hard		3					4			
Gray, Moist		4	4.6	18			7	4.5	18	
SILTY CLAY, trace gravel (CL/ML)		4	B				9	P		
		4					6			
		5	6.3	19			6	4.8	10	
		11	B				12	B		
	-1.00	5					9			
Hard		13	4.5	13			7	4.2	12	
Gray, Moist		13	P				9	B		
SILTY CLAY LOAM, trace gravel (ML/CL)										
	-3.50	8								
Dense		15		20						
Gray, Wet		22								
SILTY SAND, trace gravel (SM)										
	-6.00	6								
Hard		9	7.3	12						
Gray, Moist		11	B				50/4"		12	
SILTY CLAY, trace gravel (CL/ML)										
		6								
		6	6.5	19						
		11	B							
		4								
		6	6.3	20						
		11	B							
	-20									

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

* Boring No. RWB-01 is located at Keeler Avenue Station is 202+75.63 Offset 234.31' LT.

* Boring No. RWB-02 is located at Keeler Avenue Station is 202+80.85 Offset 122.17' LT.



USER NAME = GM	DESIGNED - RH	REVISED -
CHECKED - GJH	REVISED -	
PLOT SCALE =	DRAWN - RH	REVISED -
PLOT DATE = 8/13/2025	CHECKED - GJH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORINGS (8 OF 11)
STRUCTURE NO. 016-2093

SHEET S-60 OF 63 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	204
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

Page 1 of 1

Date 2/5/24

ROUTE FAI 290 DESCRIPTION Retaining Wall Boring LOGGED BY MA

SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8735972, Longitude -87.7298472	
		DRILLING METHOD	Mobile B-57	AUTO
		HSA	HAMMER TYPE	
			HAMMER EFF (%)	89.0

STRUCT. NO. _____	N/A	D E P T H (ft)	B L O W S (/6")	U C S (tsf)	M O I S T (%)	Surface Water Elev. _____	N/A	ft	D E P T H (ft)	B L O W S (/6")	U C S (tsf)	M O I S T (%)
Station _____	N/A					Stream Bed Elev. _____	N/A	ft				
BORING NO. _____	RWB-03					Groundwater Elev.:						
Station _____	* 16+37.61					First Encounter _____	-26.5	ft				
Offset _____	* 26.32ft RT					Upon Completion _____	N/A	ft				
Ground Surface Elev. _____	7.00	ft	(ft)	(/6")	(tsf)	After _____	N/A	ft	(ft)	(/6")	(tsf)	(%)
						Hrs.	N/A	ft				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

* Boring No. RWB-03 is located at Keeler Avenue Station is 202+85.52 Offset 129.20' RT.

Page 1 of 1

Date 2/9/24

ROUTE FAI 290 DESCRIPTION Retaining Wall Boring LOGGED BY DV

SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8735944, Longitude -87.7294361	HAMMER TYPE	AUTO
		DRILLING METHOD	Mobile B-57	HAMMER EFF (%)	89.0
			HSA		

STRUCT. NO. _____	N/A	D	B	U	M	Surface Water Elev. _____	N/A	ft	D	B	U	M
Station _____	N/A	E	L	C	O	Stream Bed Elev. _____	N/A	ft	E	L	C	O
		P	W	S	I				P	W	S	I
BORING NO. _____	RWB-04	T	H	Qu	T	Groundwater Elev.: _____			T	H	Qu	T
Station _____	*17+49.55					First Encounter _____	-30.0	ft				
Offset _____	*29.21ft RT					Upon Completion _____	N/A	ft				
Ground Surface Elev. _____	7.00	ft	(ft)	(/6")	(tsf)	After _____	N/A	ft	(ft)	(/6")	(tsf)	(%)
						Hrs.	N/A					

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

* Boring No. RWB-04 is located at Keeler Avenue Station is 202+81.62 Offset 241.11' RT.



USER NAME = GM	DESIGNED - RH	REVISED -
	CHECKED - GJH	REVISED -
PLOT SCALE =	DRAWN - RH	REVISED -
PLOT DATE = 8/13/2025	CHECKED - GJH	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SOIL BORINGS (9 OF 11)
STRUCTURE NO. 016-2093

SHEET S-61 OF 63 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	205
		CONTRACT NO. 62U41		
ILLINOIS		FED. AID PROJECT		

Page 1 of 1

Date 2/4/24

ROUTE	<u>FAI 290</u>	DESCRIPTION	<u>Retaining Wall Boring</u>	LOGGED BY	<u>MA</u>
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SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Mobile B-57	HAMMER TYPE	AUTO
		DRILLING METHOD	HSA	HAMMER EFF (%)	89.0

STRUCT. NO.	N/A	D	B	U	M	Surface Water Elev.	N/A	ft	D	B	U	M
Station	N/A	E	L	C	O	Stream Bed Elev.	N/A	ft	E	L	C	O

BORING NO. <u>RWB-05</u>	T	W		S	Groundwater Elev.:	T	W		S	
Station <u>* 13+34.65</u>	H	S	Qu	T		H	S	Qu	T	
Offset <u>* 45.84 ft LT</u>						First Encounter <u>None</u> ft				
Ground Surface Elev. <u>9.00</u> ft	(ft)	(/6")	(tsf)	(%)		Upon Completion <u>N/A</u> ft				
					After <u>N/A</u> Hrs. <u>N/A</u> ft	(ft)	(/6")	(tsf)	(%)	

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

* Boring No. RWB-05 is located at Keeler Avenue Station is 204+87.88 Offset 237.12' LT.

USER NAME = GM	DESIGNED - RH	REVISED -
	CHECKED - GJH	REVISED -
PLOT SCALE =	DRAWN - RH	REVISED -
PLOT DATE = 8/13/2025	CHECKED - GJH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Page 1 of 1

Date 2/4/24

ROUTE	<u>FAI 290</u>	DESCRIPTION	<u>Retaining Wall Boring</u>	LOGGED BY	<u>MA</u>
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SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Mobile B-57	HAMMER TYPE	AUTO
		DRILLING METHOD	HSA	HAMMER EFF (%)	89.0

STRUCT. NO.	N/A	D	B	U	M	Surface Water Elev.	N/A	ft	D	B	U	M
Station	N/A	E	L	C	O	Stream Bed Elev.	N/A	ft	E	L	C	O

BORING NO. <u>RWB-06</u>	T	W	S	Groundwater Elev.:	T	W	S
Station <u>* 14+44.10</u>	H	S	Qu	First Encounter <u>-25.5</u> ft <u>▼</u>	H	S	Qu
Offset <u>* 39.34ft LT</u>				Upon Completion <u>N/A</u> ft			
Ground Surface Elev. <u>8.00</u> ft	(ft)	(/6")	(tsf)	After <u>N/A</u> Hrs. <u>N/A</u> ft	(ft)	(/6")	(tsf)
			(%)				(%)

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

* Boring No. RWB-06 is located at Keeler Avenue Station is 204+80.39 Offset 127.73' LT.

SOIL BORINGS (10 OF 11) STRUCTURE NO. 016-2093			F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			290	FAI 290 22 KEELER BR	COOK	283	206
SHEET S-62 OF 63 SHEETS			CONTRACT NO. 62U41				
				ILLINOIS	FED. AID PROJECT		

MODEL: Soil Borings 10 [Sheet]
FILE NAME: pw:/civilltech-pw.bentley.com/Projects/3855/CAD/CADD Sheets/17-Structures/D162/U41-SHT-062-Soil Borings10

Page 1 of 1

Date 2/8/24

ROUTE	FAI 290	DESCRIPTION	Retaining Wall Boring	LOGGED BY	DV
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SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.8741028, Longitude -87.7298611 Mobile B-57	HAMMER TYPE	AUTO
		DRILLING METHOD	HSA	HAMMER EFF (%)	89.0

STRUCT. NO. _____	N/A					Surface Water Elev. _____	N/A	ft					
Station _____	N/A					Stream Bed Elev. _____	N/A	ft					
BORING NO. _____	RWB-07					Groundwater Elev.:							
Station _____	* 17+2.09					First Encounter _____	-25.0	ft	▼				
Offset _____	* 28.55ft LT					Upon Completion _____	N/A	ft					
Ground Surface Elev. _____	7.00	ft	(ft)	(/6")	(tsf)	After _____	N/A	Hrs.					
					(%)		N/A	ft					

12 inches of Asphalt				Very Stiff to Very Hard			
12 inches of Aggregate Base	6.00	6		Gray, Moist	6		
		5	8.1	SILTY CLAY, trace gravel	6	3.3	18
	5.00	7	B	(CL/ML) (continued)	9	B	
Very Stiff to Very Hard							
Gray, Moist		5			4		
SILTY CLAY, trace gravel		4			6		
(CL/ML)		6	4.2		6	3.8	24
Sand seam at 2.5 feet		-5	B		-25	8	B
		5			-19.00	2	
		7	2.5	Stiff		4	1.3
		8	B	Gray, Moist		8	B
				SILTY CLAY, trace sand and			21
				gravel (CL/ML)			
		3				3	
		9	4.0			8	1.9
		-10	B			-30	9
							B
		7					
		12	8.1				
		14	B				
					-25.00 ▼	16	
				WEATHERED LIMESTONE		50/2"	
					-26.00		6
		5		Auger refusal at 33 feet			
		10	5.0	End of Boring			
		-15	B				
		5					
		7	7.9				
		11	B				
		3					
		8	6.0				
		-20	B				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)

Page 1 of 1

Date 2/8/24

ROUTE	FAI 290	DESCRIPTION	Retaining Wall Boring	LOGGED BY	DV
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SECTION Keeler Avenue over FAI 290 **LOCATION** , SEC. 15, TWP. 39N, RNG. 13E,

COUNTY	COOK	DRILLING RIG	Latitude 41.87411111, Longitude -87.7294778	HAMMER TYPE	Auto
		DRILLING METHOD	Mobile B-57	HAMMER EFF (%)	89.0
			HSA		

STRUCT. NO. _____	N/A					Surface Water Elev. _____	N/A	ft					
Station _____	N/A					Stream Bed Elev. _____	N/A	ft					
BORING NO. _____	RWB-08					Groundwater Elev.: _____							
Station _____	*18+6.53					First Encounter _____	-31.0	ft	▼				
Offset _____	* 29.84ft LT					Upon Completion _____	N/A	ft					
Ground Surface Elev. _____	7.00	ft	(ft)	(/6")	(tsf)	After _____	N/A	Hrs.	ft	(ft)	(/6")	(tsf)	(%)

[illegible]

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, form 137 (Rev. 8-99)



USER NAME = GM	DESIGNED - RH	REVISED -
	CHECKED - GJH	REVISED -
PLOT SCALE =	DRAWN - RH	REVISED -
PLOT DATE = 8/13/2025	CHECKED - GJH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORINGS (11 OF 11)
STRUCTURE NO. 016-2093

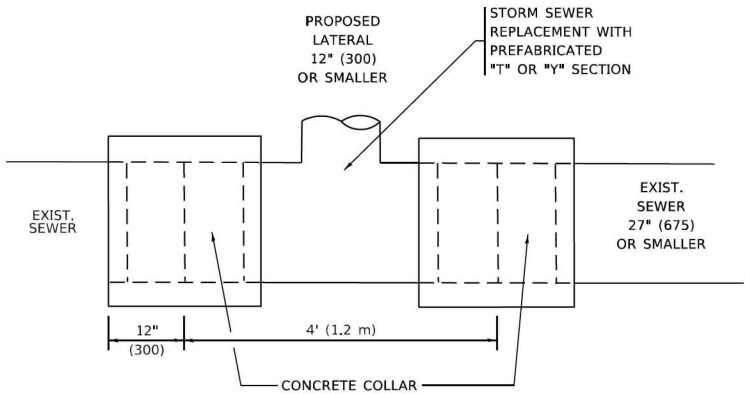
SHEET S-63 OF 63 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	207
		CONTRACT NO. 62U41		
ILLINOIS		FED. AID PROJECT		

8/14/2025 8:33:17 AM

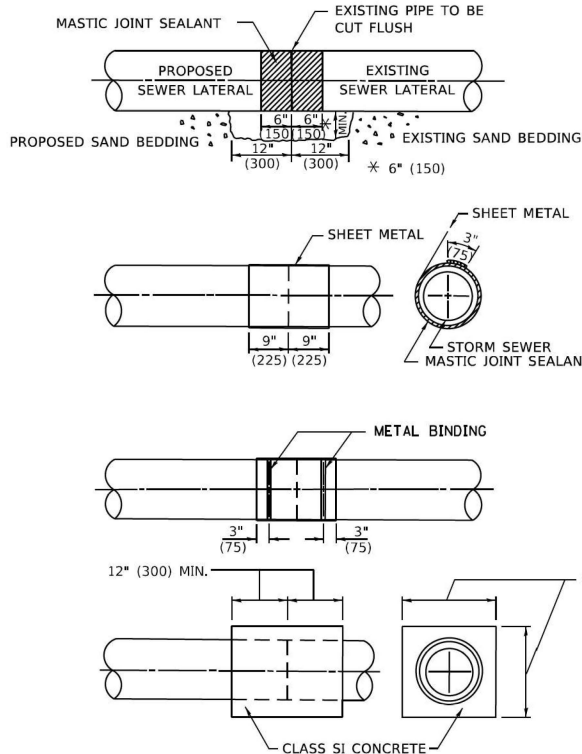
* Boring No. RWB-08 is located at Keeler Avenue Station is 204+67.63 Offset 234.60' RT.

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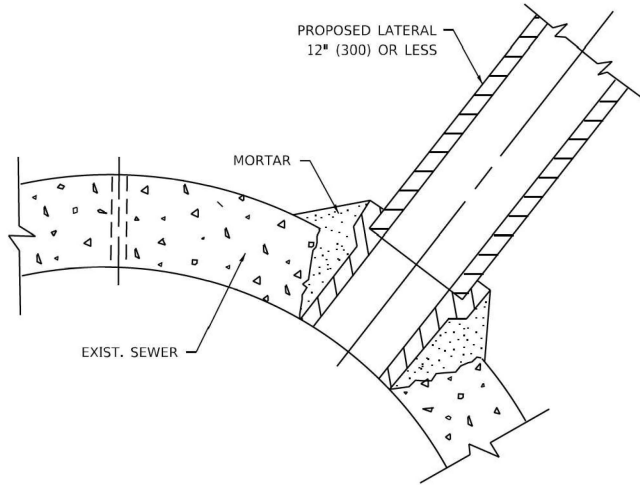
DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER
OF 27" (675) OR SMALLER



DETAIL "B"

CLASS SI CONCRETE COLLAR



DETAIL "C"

PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 30" (750) OR LARGER

NOTES:

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

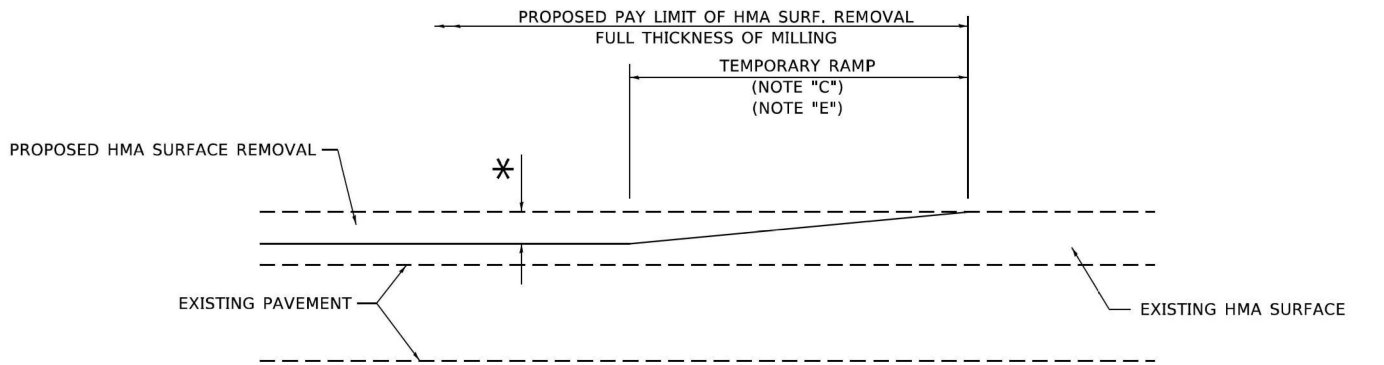
- CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.
- CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

- TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.
- REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.
- TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.
- CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

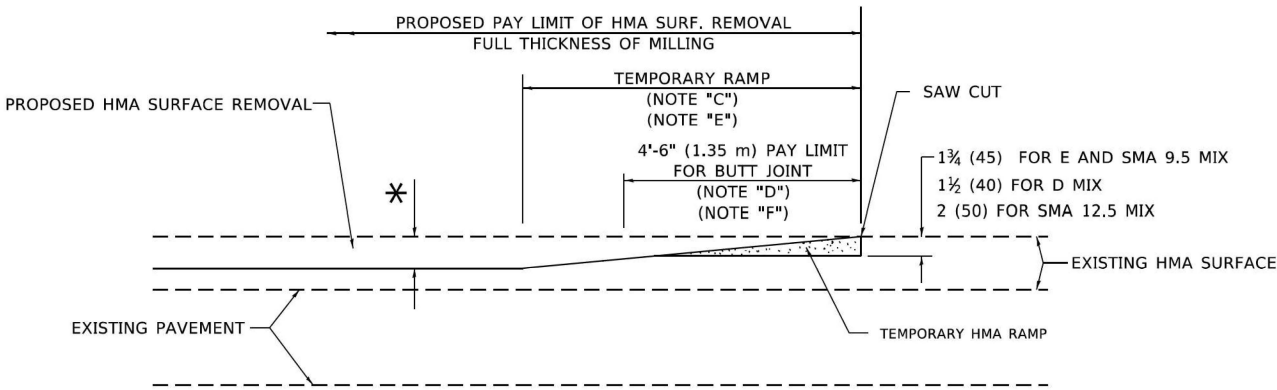
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		DRAWN -	REVISED - R. SHAH 10-25-94						290	FAI 290 22 KEELER BR	COOK	283	208
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. SHAH 06-12-96		BD500-01 (BD-07)		CONTRACT NO.62U41						
	PLOT DATE = 11/18/2022	DATE - 07-25-90	REVISED - K. SMITH 11-18-22		SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.		ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

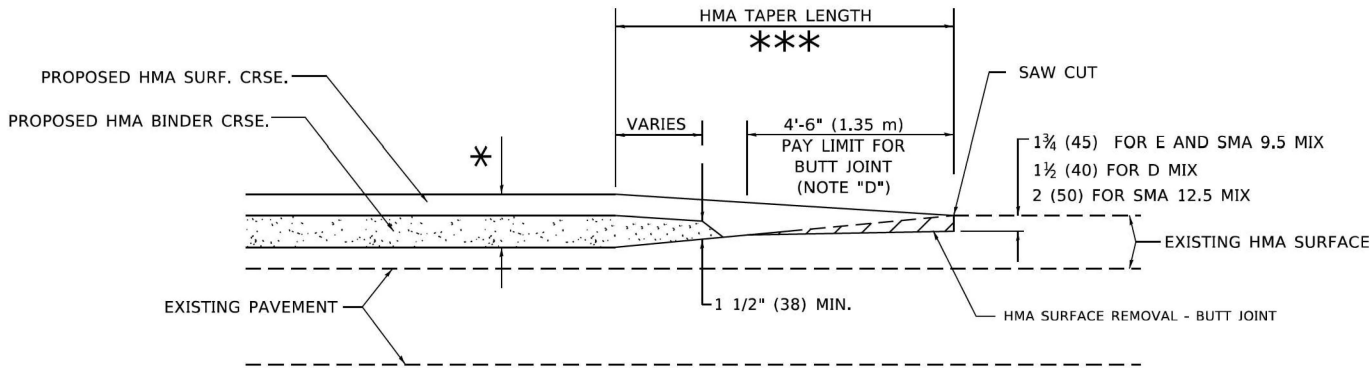


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

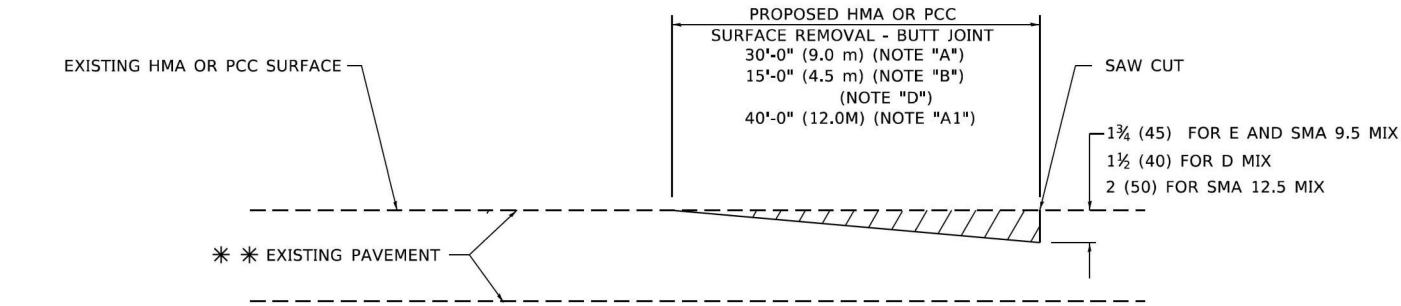
OPTION 2

TYPICAL TEMPORARY RAMP

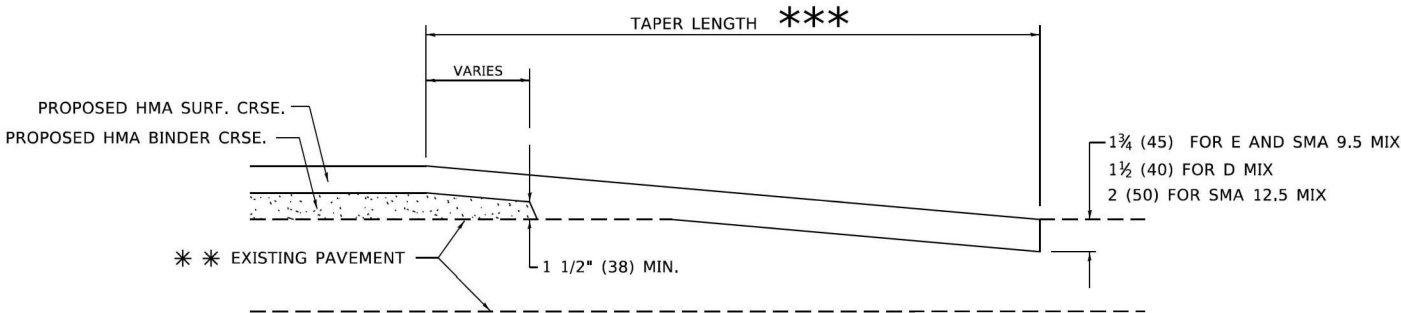


**BUTT JOINT AND
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

****** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- INTERSTATES
- MINOR SIDE ROADS.
- THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
***** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
******* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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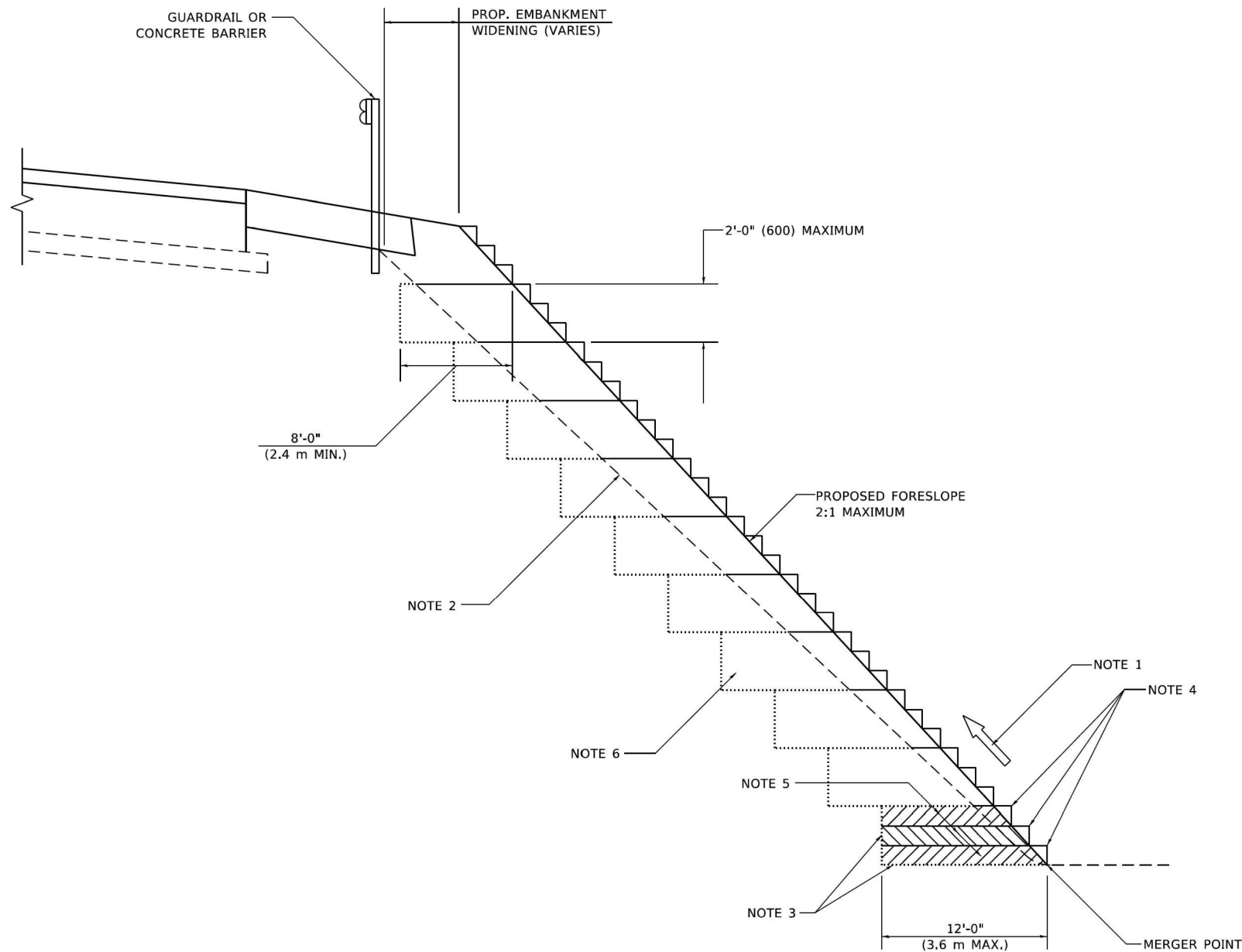
USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	209
BD400-05 BD-32		CONTRACT NO.62U41		
ILLINOIS		FED. AID PROJECT		



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

GENERAL NOTES

1. CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
2. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
3. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
4. TRIM TO FINAL SLOPE.
5. EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.

BASIS OF PAYMENT

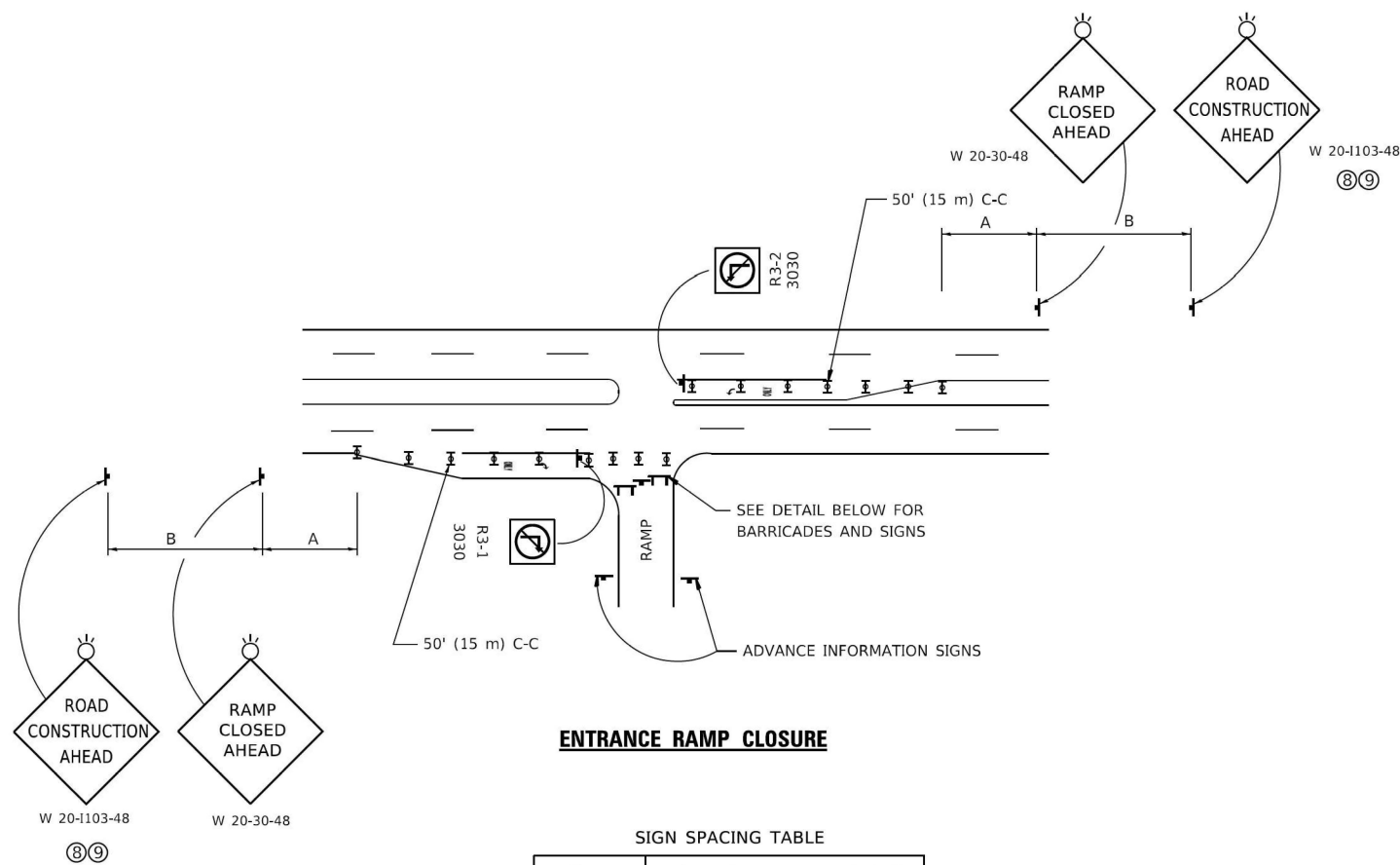
1. EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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	USER NAME = Lawrence.DeManche	DESIGNED -	REVISED - K. SMITH 11-18-22	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BENCHING DETAIL FOR EMBANKMENT WIDENING			F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - CADD	REVISED -					290	FAI 290 22 KEELER BR	COOK	283	210
	PLOT SCALE = 100.0000 ' / in.	CHECKED - S.E.B.	REVISED -		80-51			CONTRACT NO.62U41				
	PLOT DATE = 11/18/2022	DATE - 06-16-04	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

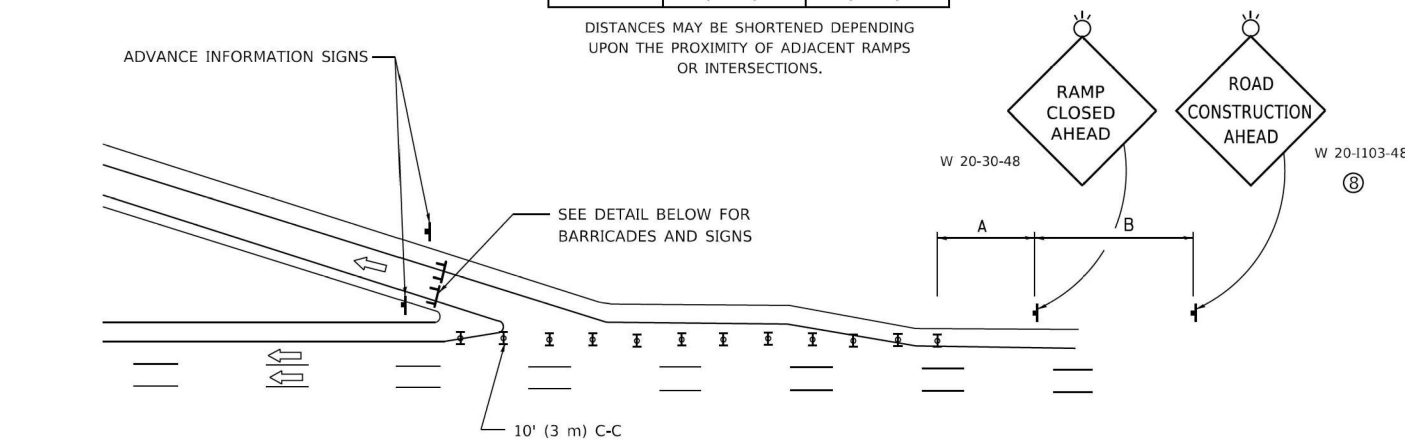
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ENTRANCE RAMP CLOSURE

FACILITY	SIGN SPACING TABLE	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

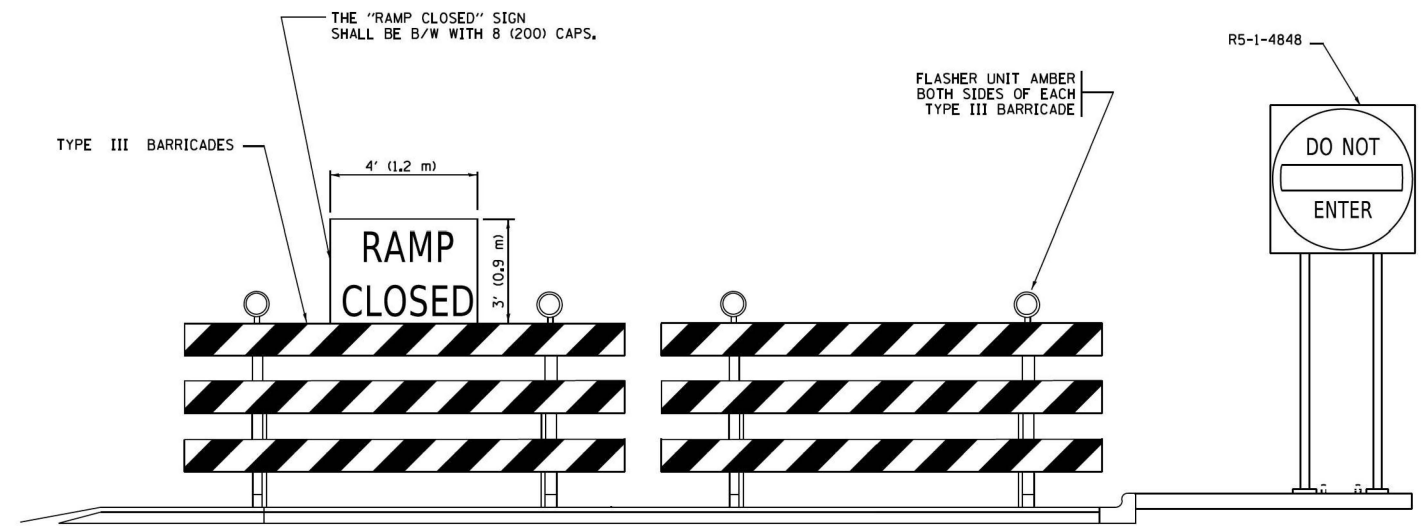
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



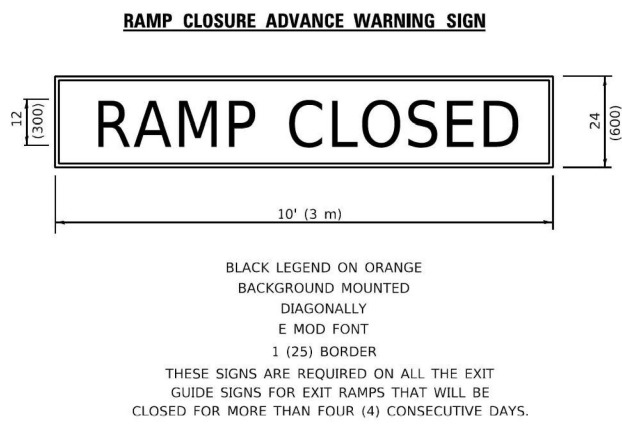
EXIT RAMP CLOSURE

SYMBOLS

- TYPE II BARRICADE OR DRUM
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS

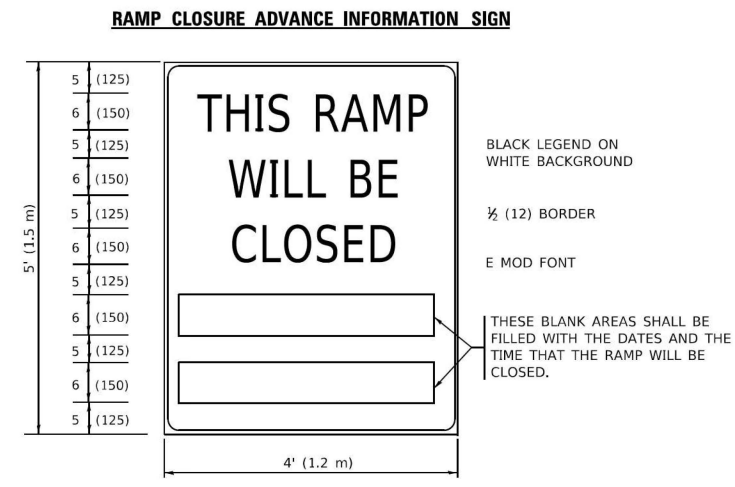


DETAIL FOR REQUIRED BARRICADES & SIGNS



RAMP CLOSURE ADVANCE WARNING SIGN

BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.



RAMP CLOSURE ADVANCE INFORMATION SIGN

BLACK LEGEND ON WHITE BACKGROUND
½ (12) BORDER
E MOD FONT
THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Lawrence,DeManche	DESIGNED - D.W.S.	REVISED - S.P.B._12-09
	DRAWN -	REVISED - M.D._06-13
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M.D._01-18
PLOT DATE = 5/3/2024	DATE - 02-83	REVISED - D.S._05-24

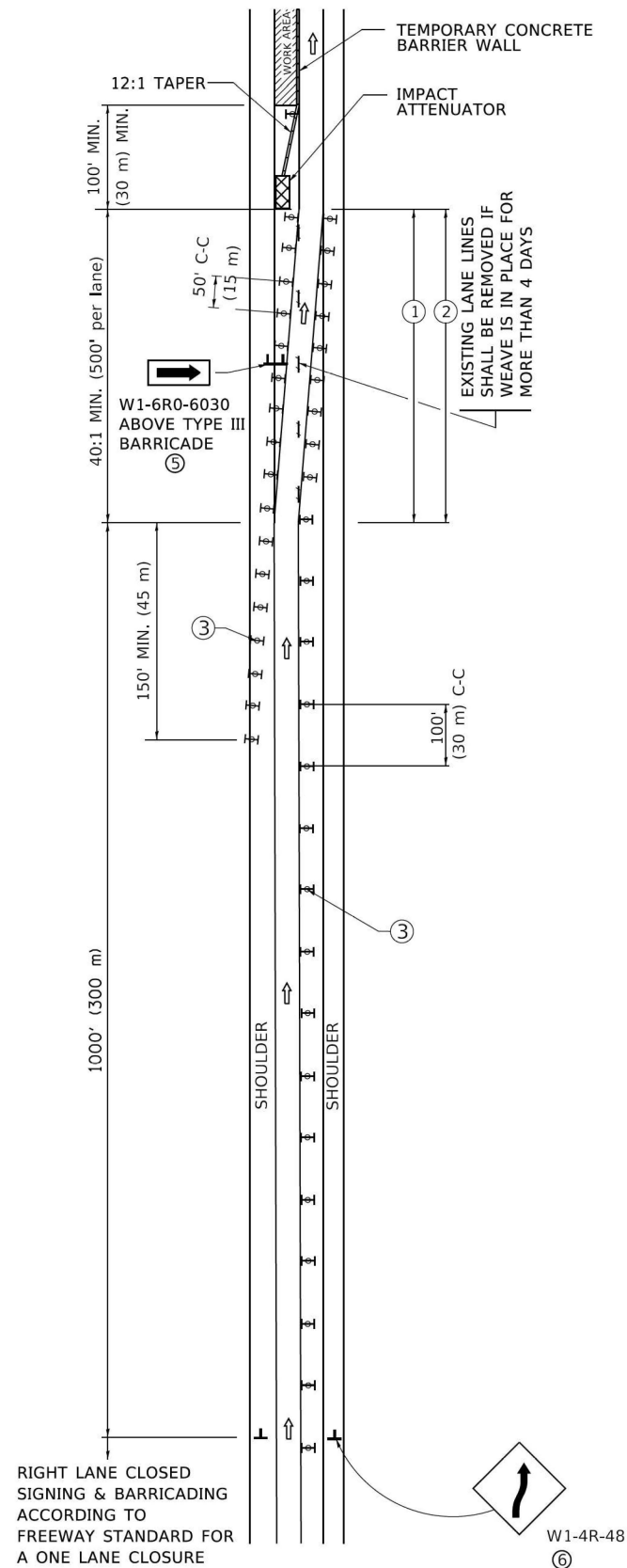
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

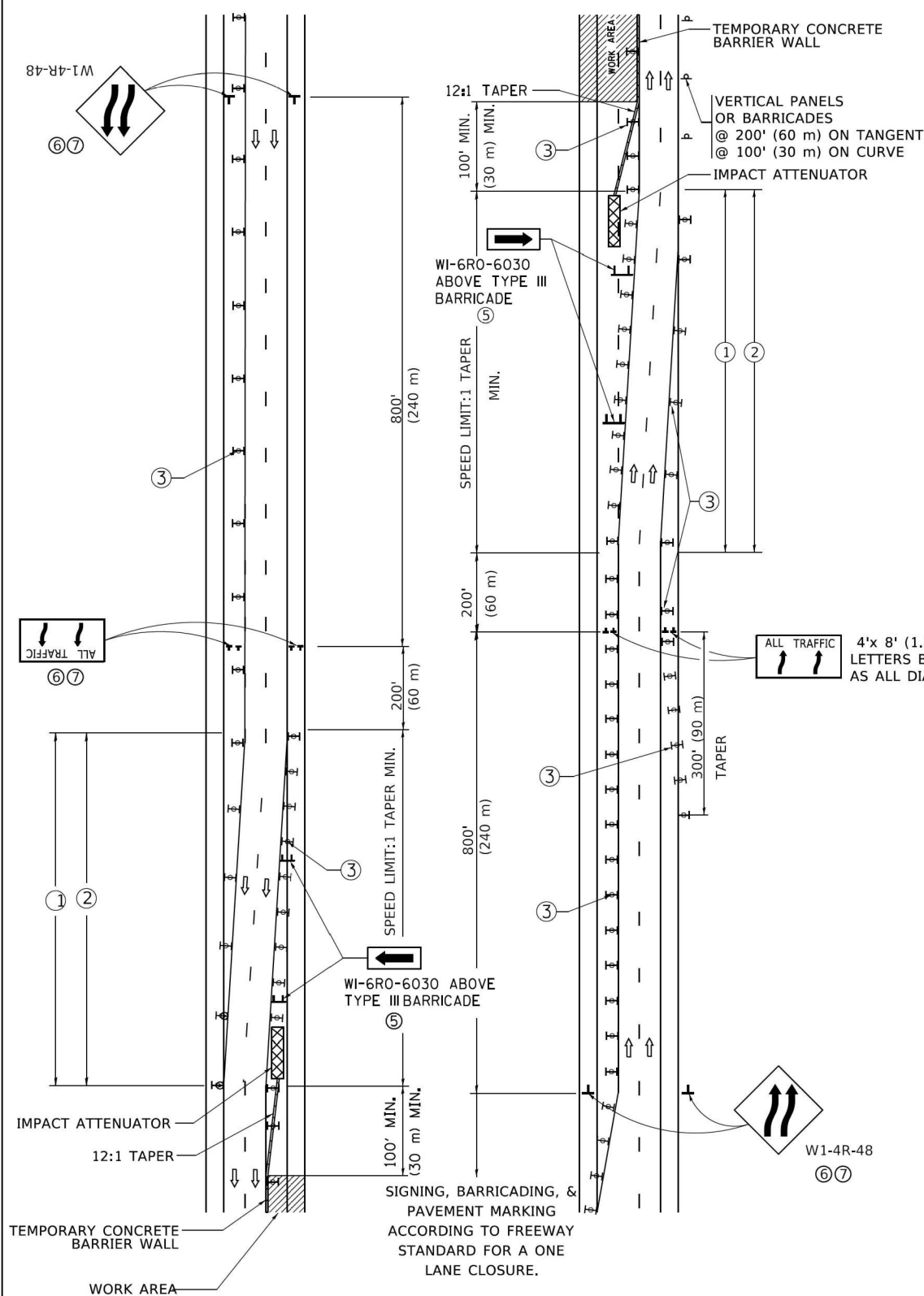
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	211
TC-08		CONTRACT NO.62U41		
		ILLINOIS	FED. AID PROJECT	

SINGLE LANE WEAVE










MULTI-LANE WEAVE



- GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

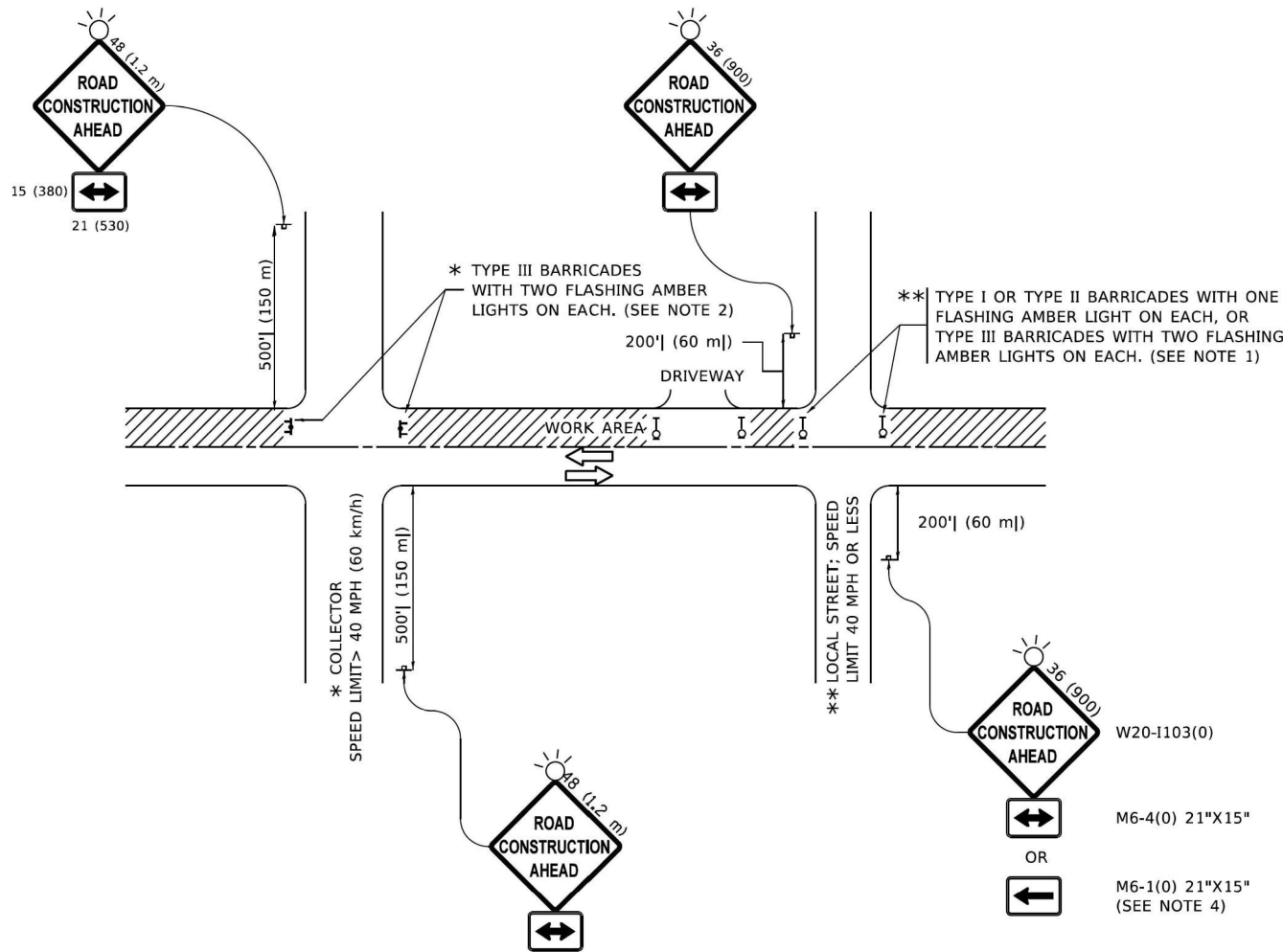
SYMBOLS

- | | |
|---|--|
|  | DIRECTION OF TRAFFIC |
|  | WORK AREA |
|  | SIGN ON PORTABLE OR PERMANENT SUPPORT |
|  | TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT |
|  | TEMPORARY CONCRETE BARRIER WALL |
|  | IMPACT ATTENUATOR |
|  | |
| W24-1-48 | |
| (7) | |

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN

USER NAME = footemj	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE				F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED - S.P.B. 01-07						290	FAI 290 22 KEELER BR	COOK	283	212
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - S.P.B. 12-09		TC-09 CONTRACT NO. 62UJ1				ILLINOIS FED. AID PROJECT				
PLOT DATE = 3/4/2019	DATE = 02-87	REVISED - M.D. 06-13										
			SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.				

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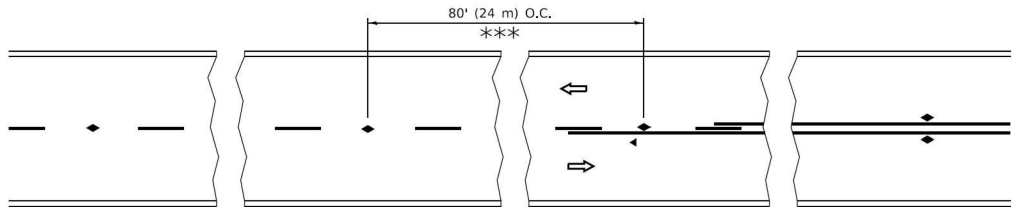


NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

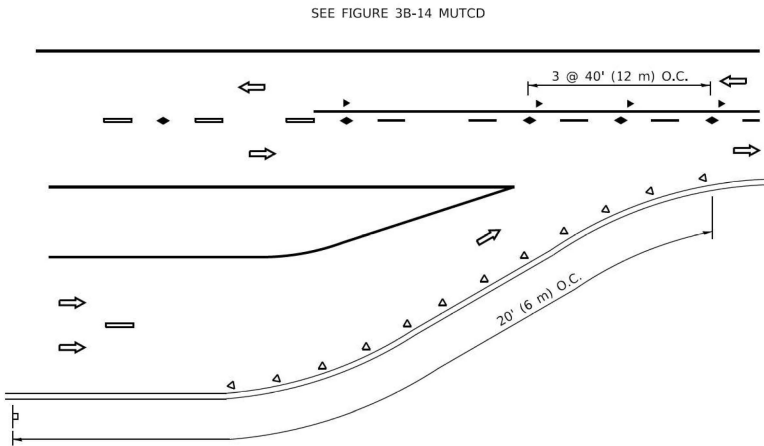
All dimensions are in inches (millimeters) unless otherwise shown.

	USER NAME = Lawrence,DeManche	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. SCHUETZE 07-01-13						290	FAI 290 22 KEELER BR	COOK	283	213
	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 09-15-16						TC-10		CONTRACT NO.62U41		
	PLOT DATE = 5/3/2024	DATE - 06-89	REVISED - D. SENDERAK 05-03-24						ILLINOIS		FED. AID PROJECT		

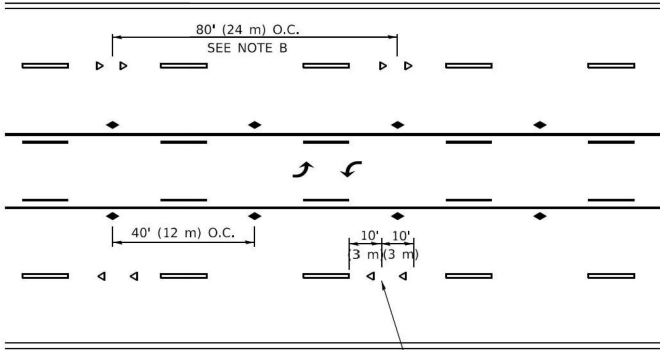


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

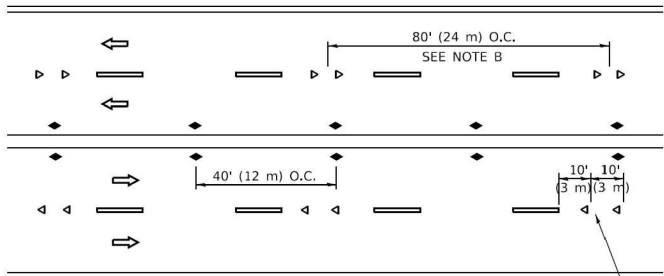
TWO-LANE/TWO-WAY



LANE REDUCTION TRANSITION

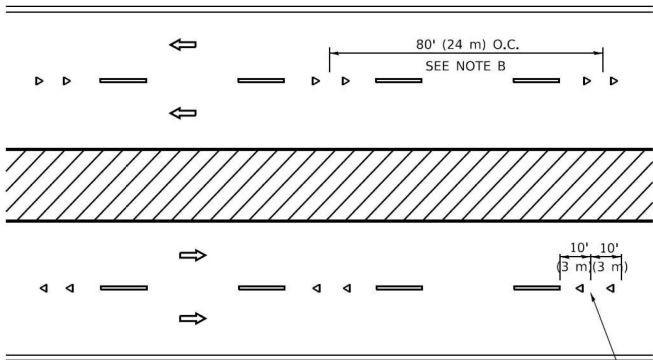


TWO-WAY LEFT TURN



SEE NOTE A

MULTI-LANE/UNDIVIDED



SEE NOTE A

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

LANE MARKER NOTES

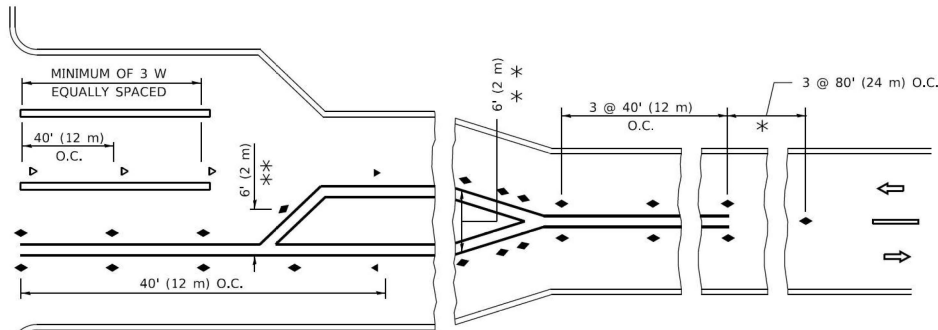
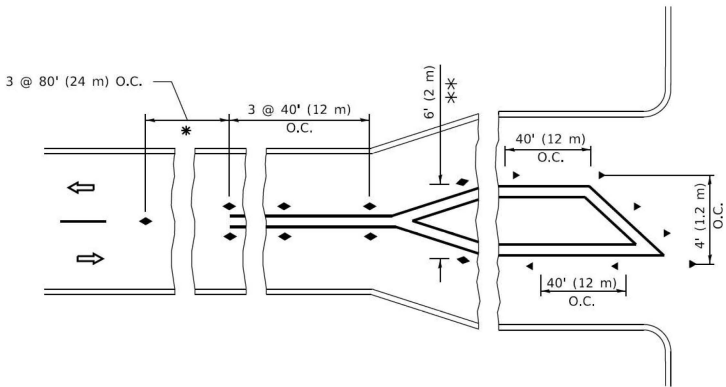
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS
USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters)
unless otherwise shown.

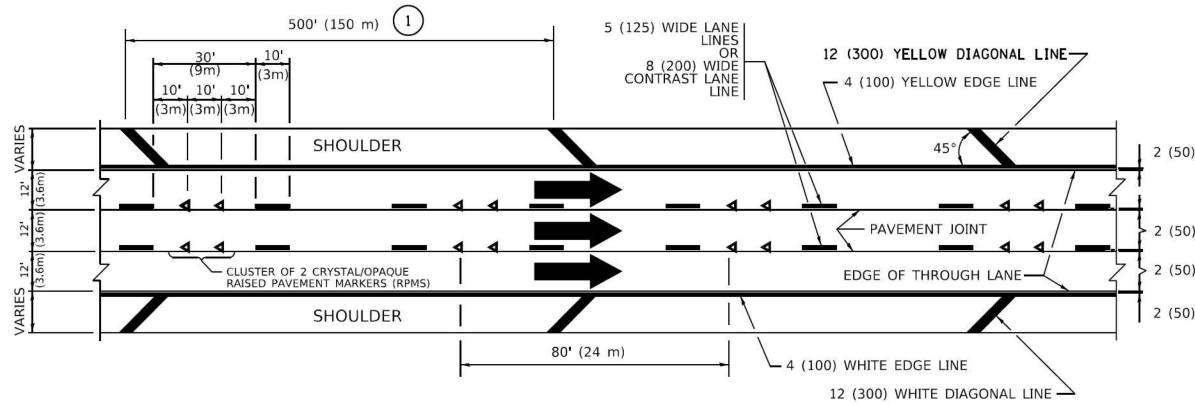
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET 1	OF 1	SHEETS
STA.	TO STA.		

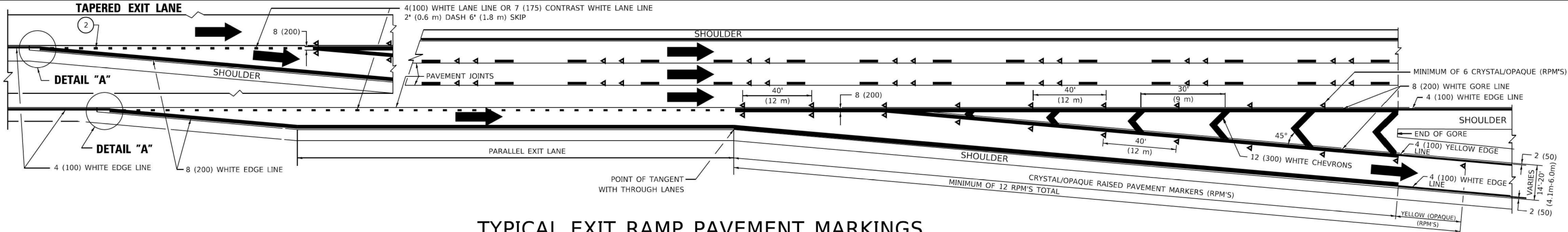
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	214
TC-11		CONTRACT NO. 62U41		
		ILLINOIS	FED. AID PROJECT	



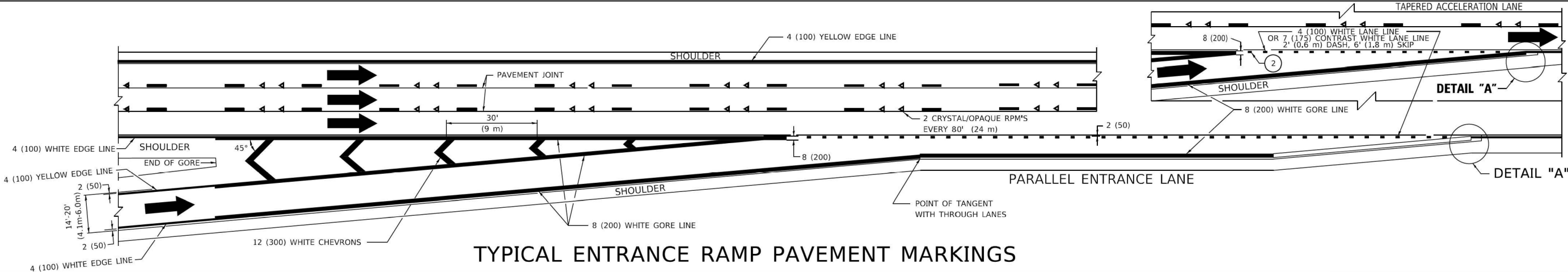
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

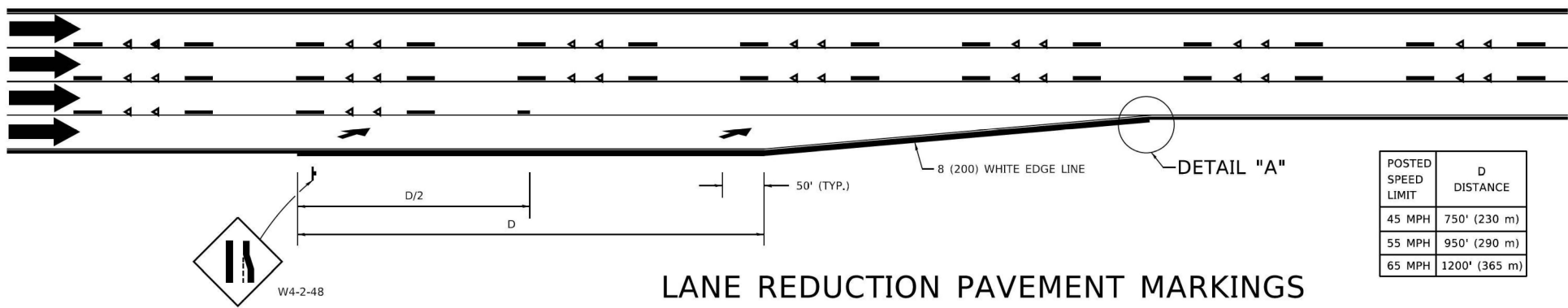
- 1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
- 2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
- 3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE D, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
- 4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



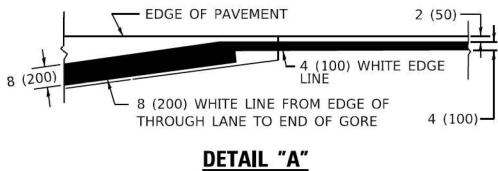
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS



NOTES:

- 1 THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- 2 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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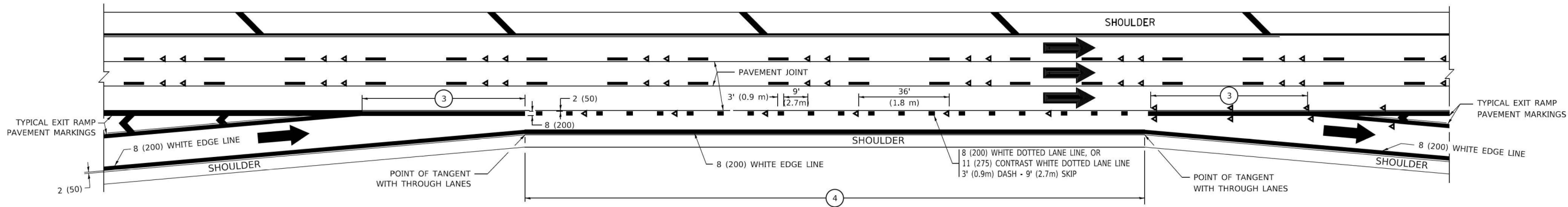
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PLOT DATE	= 11/18/2022	DATE -	01-90	REVISED -	K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

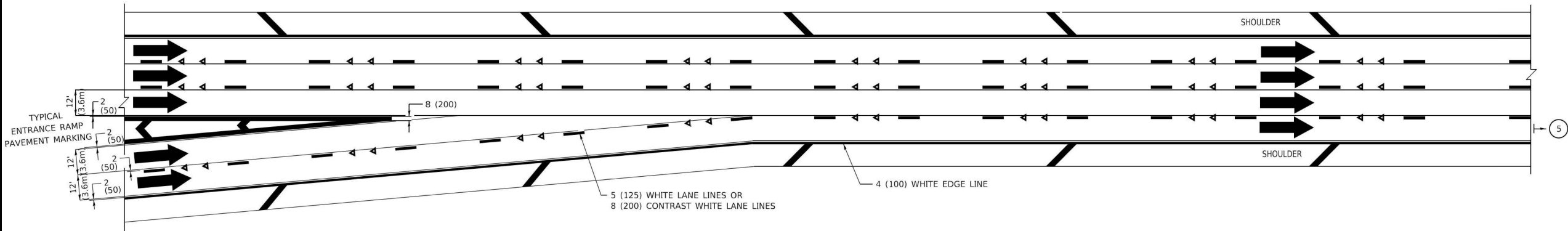
MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

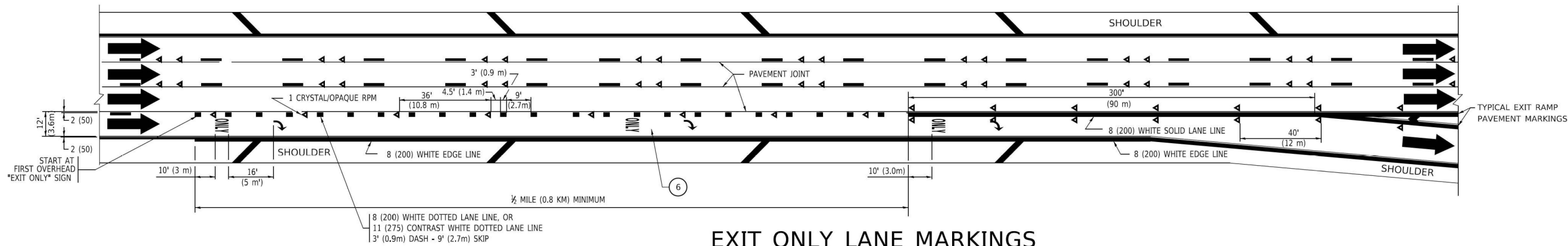
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-12		CONTRACT NO.62U41		
		ILLINOIS FED. AID PROJECT		



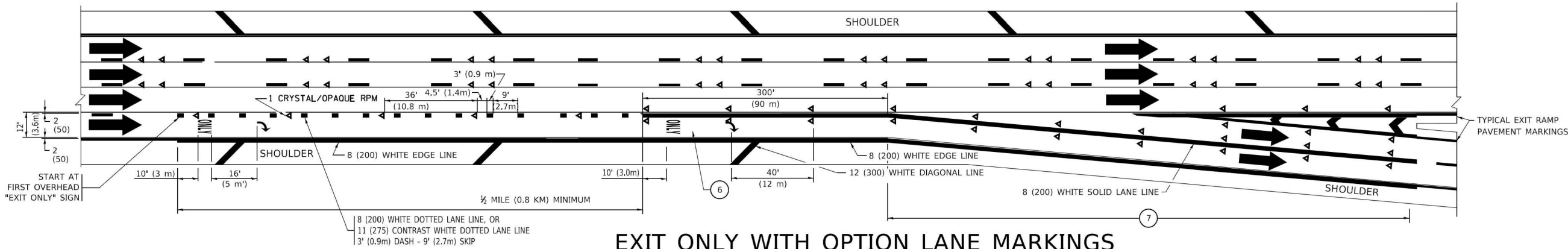
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

NOTES:

3 OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).

4 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.

5 FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.

6 ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED..

7 CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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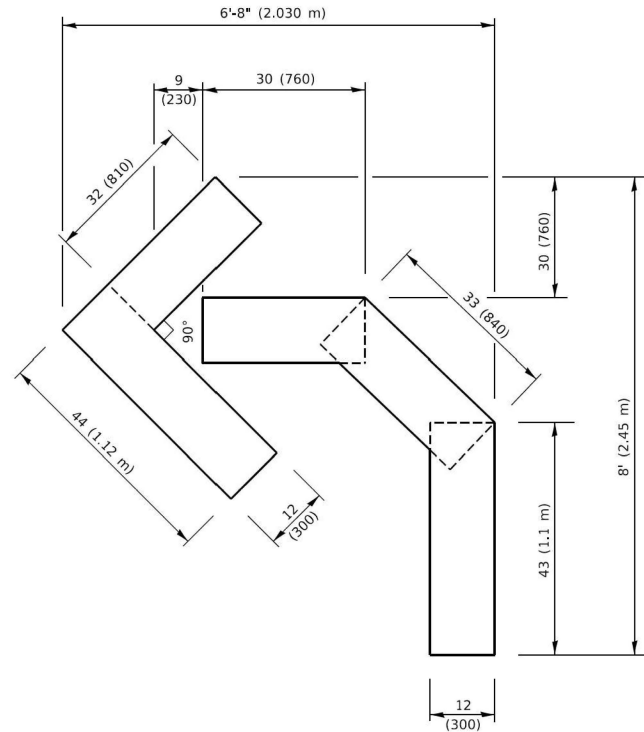
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PLOT DATE	= 3/4/2019	DATE	- 01-90	REVISED	- M.D. 09-17

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS

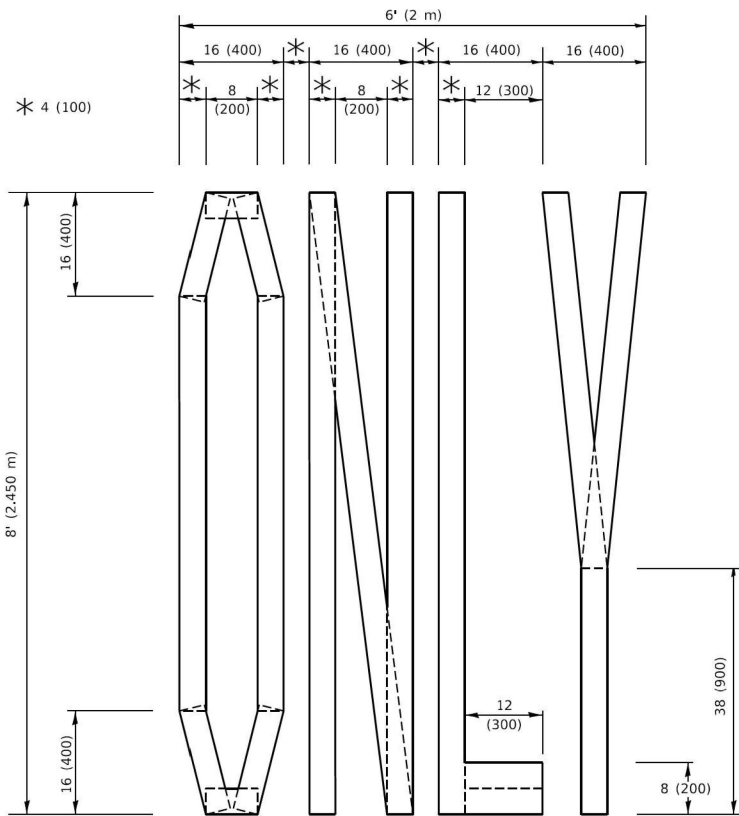
SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-12		CONTRACT NO.62U41		
ILLINOIS		FED. AID PROJECT		



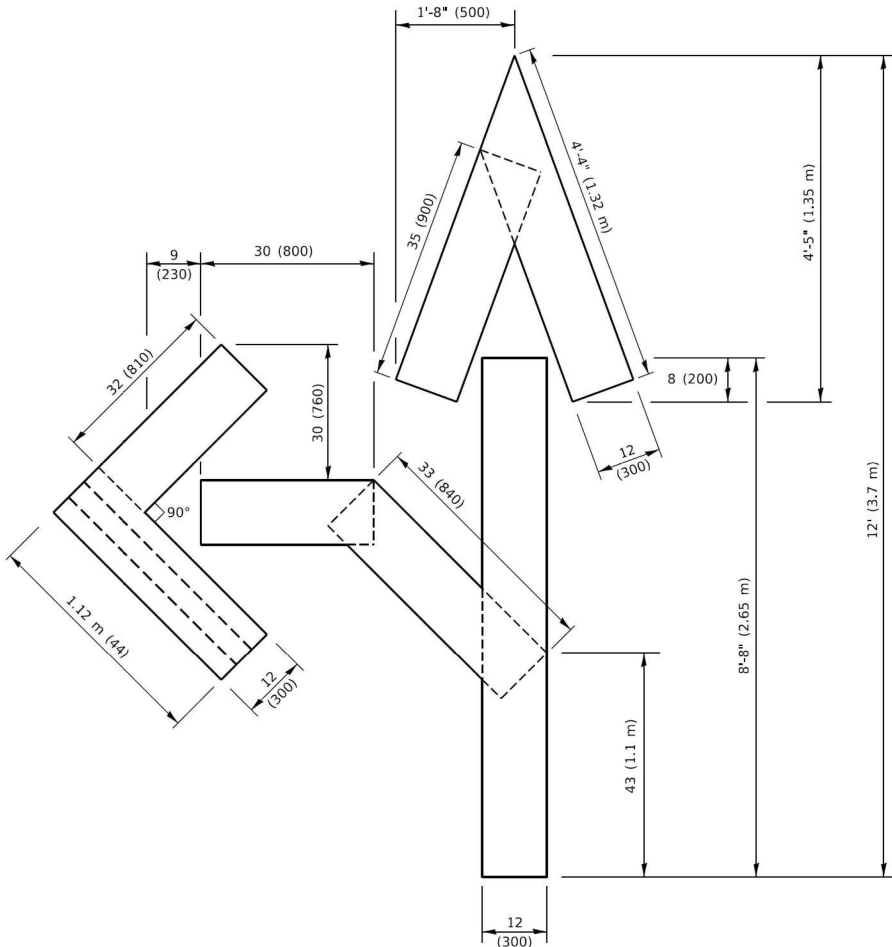
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

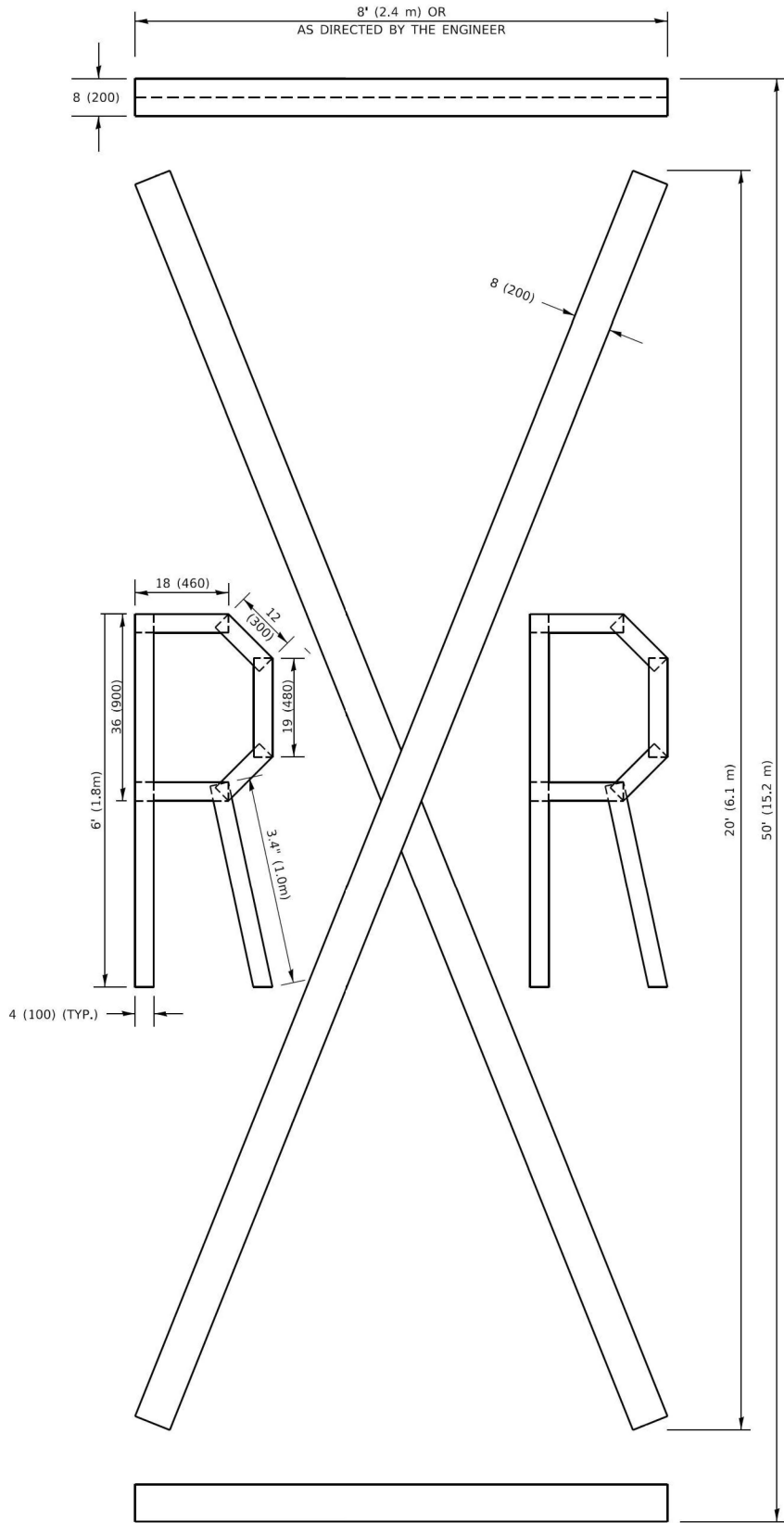


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

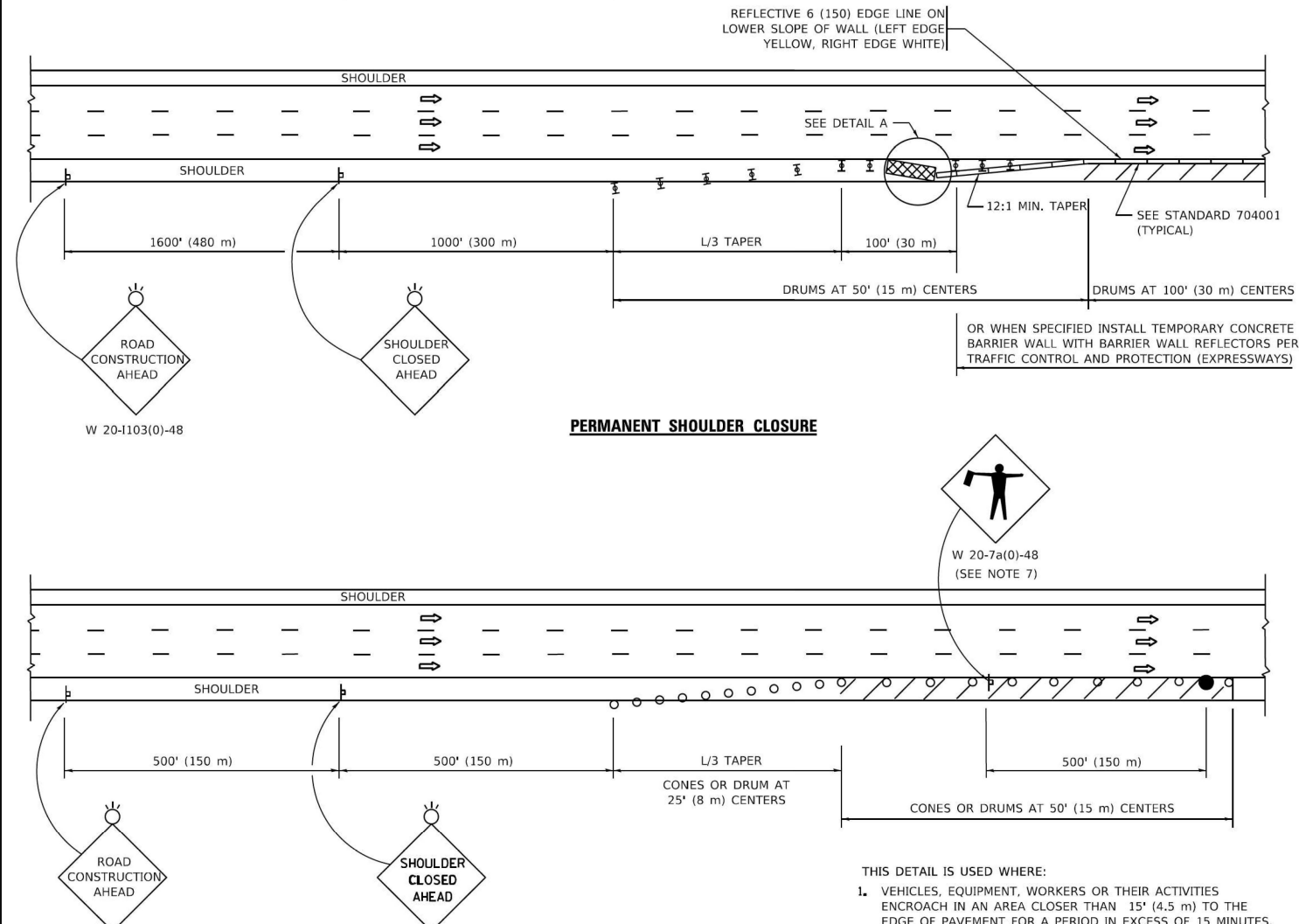
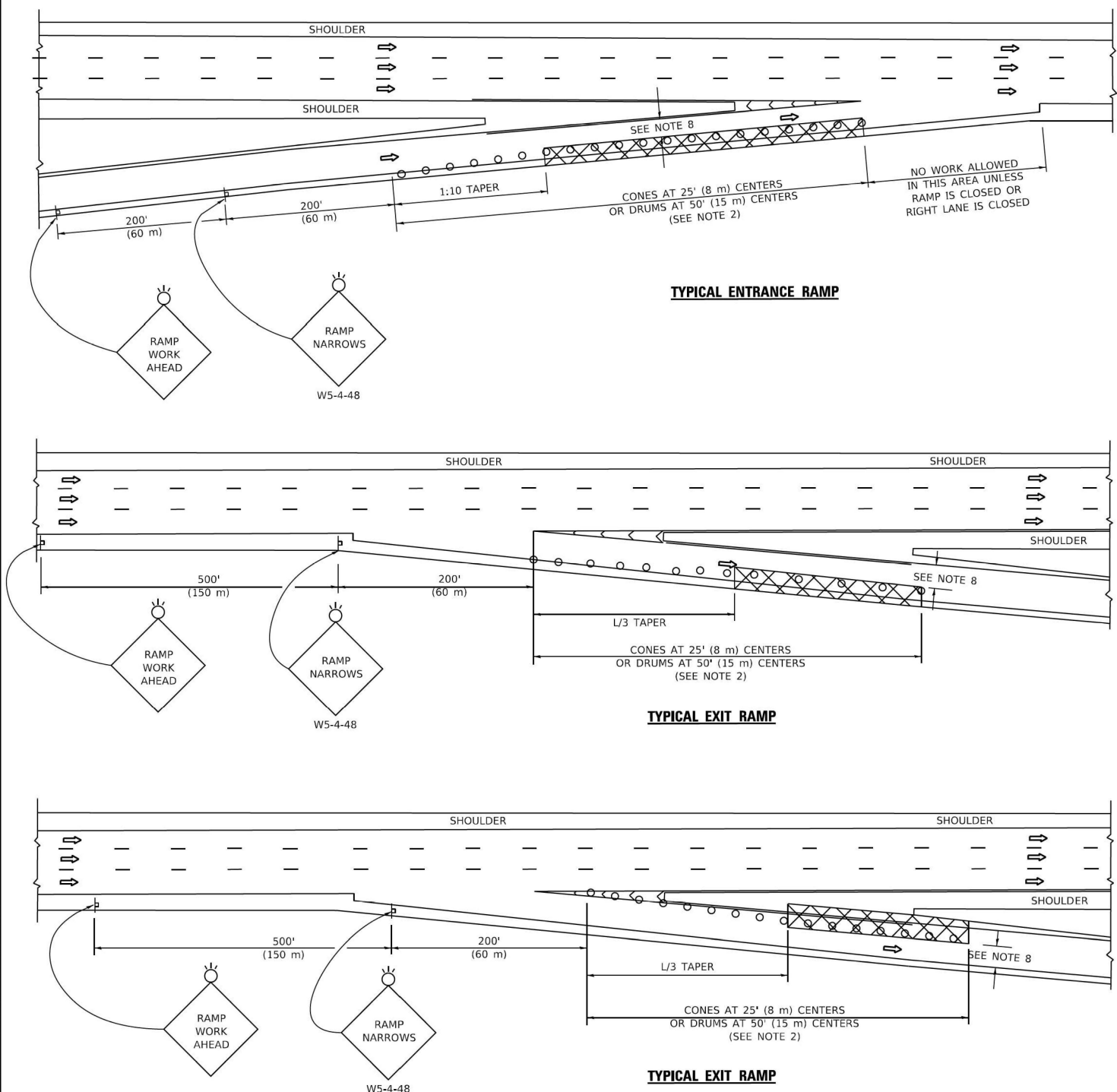
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	217
TC-16		CONTRACT NO.62U41		
		ILLINOIS FED. AID PROJECT		

PARTIAL RAMP CLOSURE DETAILS

SHOULDER CLOSURE DETAILS



SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

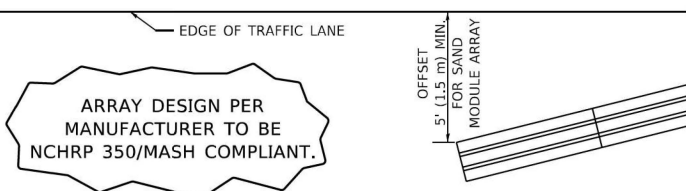
GENERAL NOTES:

- THE "L" DISTANCE EQUALS:
SPEED LIMIT FORMULAS
45 mph (80 km/h) METRIC ENGLISH
OR GREATER L=0.65(WXS) L=(WXS)
W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
- TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

- 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.
- TEMPORARY SHOULDER AND NON-SYSTEM INTERCHANGE PARTIAL RAMP CLOSURES ARE ALLOWED WEEKDAYS BETWEEN 9:00 A.M. AND 3:00 P.M. AND BETWEEN 7:00 P.M. AND 5:00 A.M. OR AS APPROVED BY THE EXPRESSWAY TRAFFIC OPERATIONS ENGINEER. PERMANENT SHOULDER AND PARTIAL RAMP CLOSURES WILL ONLY BE PERMITTED IF CALLED FOR IN THE PLANS OR AS APPROVED BY THE EXPRESSWAY OPERATIONS ENGINEER.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

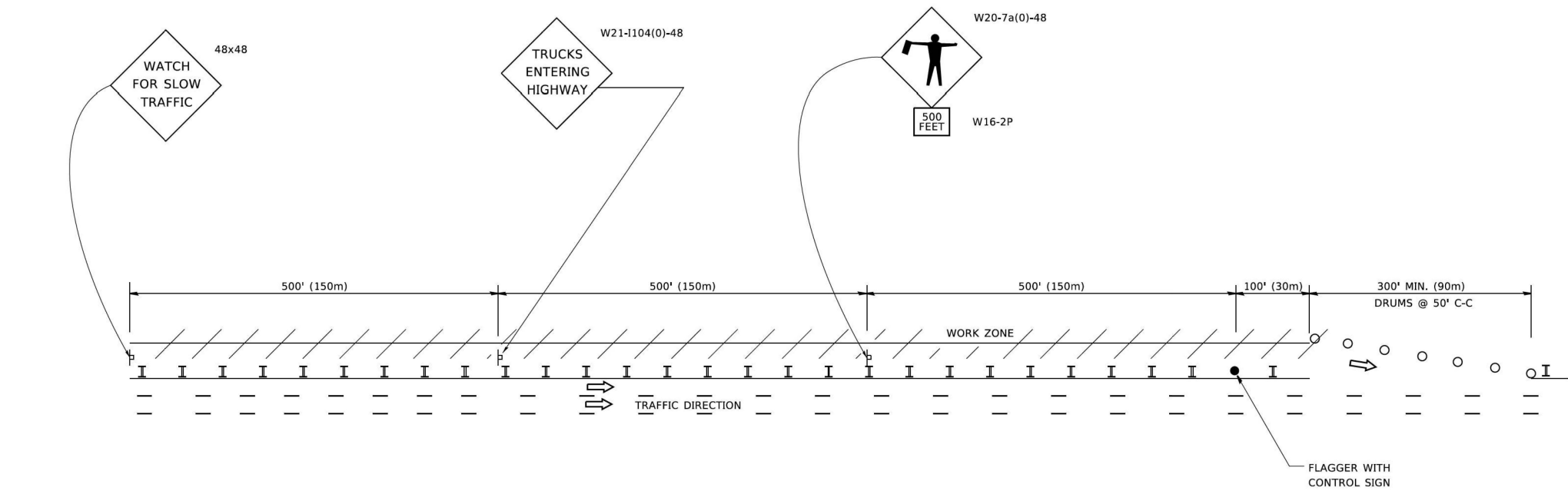
TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

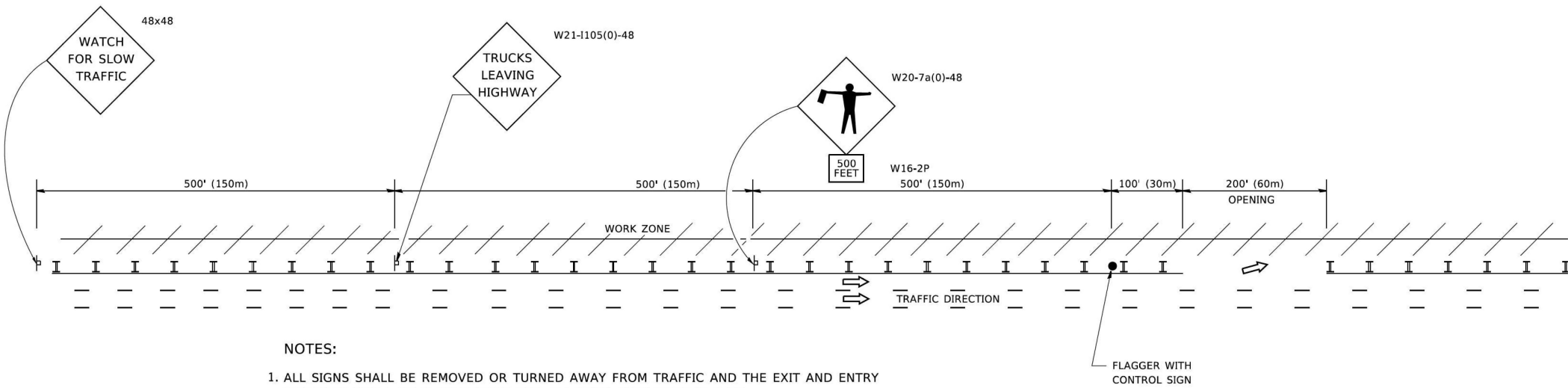
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290	FAI 290 22 KEELER BR	COOK	283	218
TC-17		CONTRACT NO62U41		
		ILLINOIS FED. AID PROJECT		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:


1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN


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		DRAWN -	REVISED - S.P.B. 01-07									290	FAI 290 22 KEELER BR	COOK	283	219
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FOR U.S. ROUTES
M1-40-2424




FOR ILLINOIS ROUTES
M1-50-2424




R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND




M5-1L-2115




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M6-1-2115




M6-3-2115




M3-1-2412




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M3-3-2412




M3-4-2412




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DETOUR
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DETOUR
WEST
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
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DETOUR
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DETOUR
EAST
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
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
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* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

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		DRAWN	-	REVISED	-	R. BORO 09-14-09
PLOT SCALE	= 50.0000' / in.	CHECKED	-	REVISED	-	
PLOT DATE	= 3/4/2019	DATE	-	REVISED	-	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-21		CONTRACT NO.62U41		
ILLINOIS		FED. AID PROJECT		

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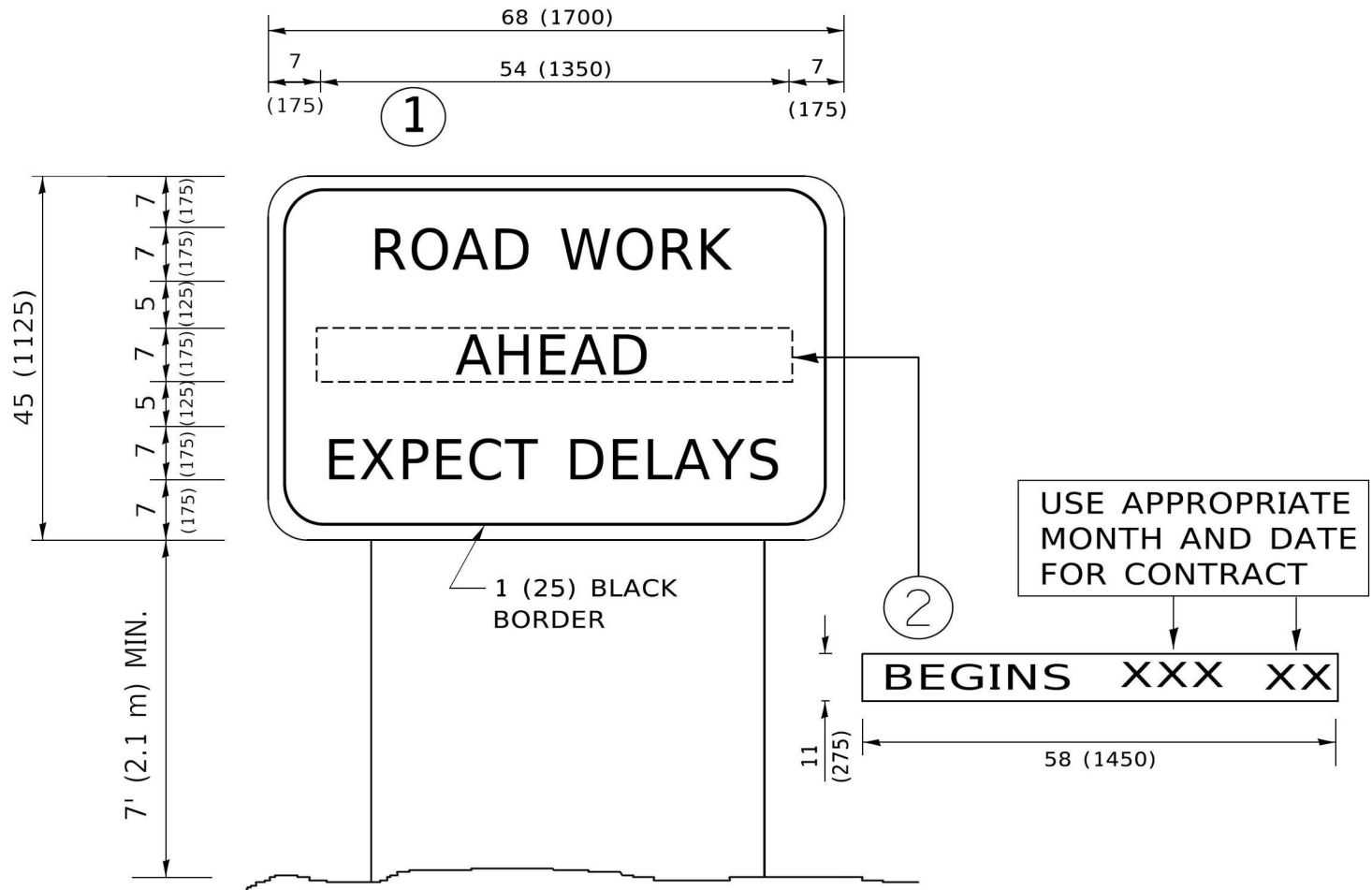
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		DRAWN	-	REVISED	-	R. MIRS	12-11-97
PLOT SCALE	= 50.0000 ' / in.	CHECKED	-	REVISED	-	T. RAMMACHER	02-02-99
PLOT DATE	= 3/4/2019	DATE	-	REVISED	-	C. JUCIUS	01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	221
TC-22		CONTRACT NO.62U41		
ILLINOIS		FED. AID PROJECT		

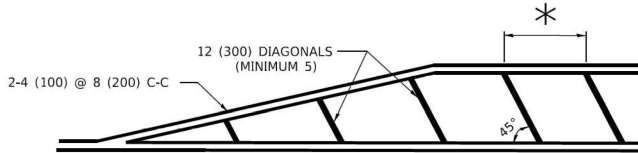
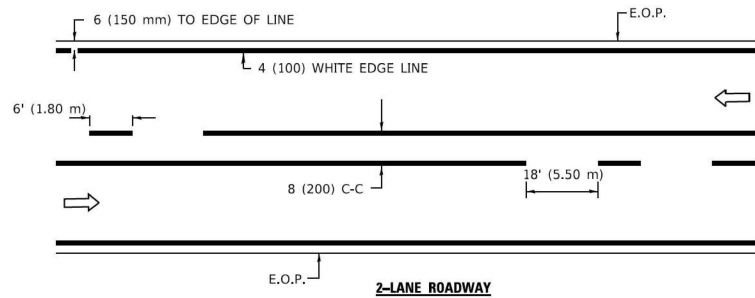


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN①WITH INSTALLED PANEL②ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL②SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

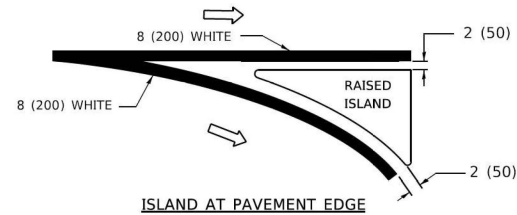
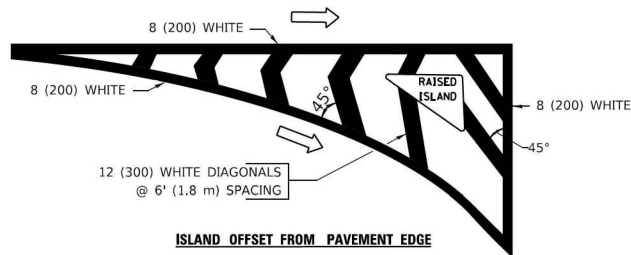
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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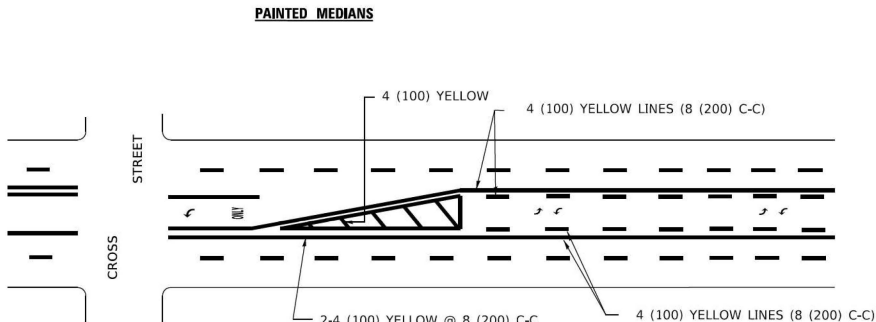
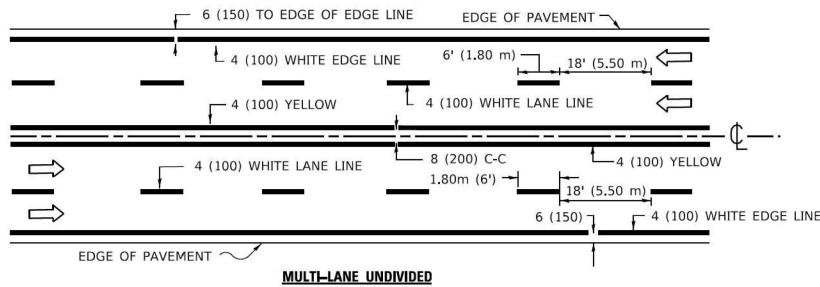


* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

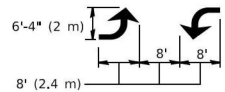
* DIAGONAL LINE SPACING: 20' (6.1 m) C-C



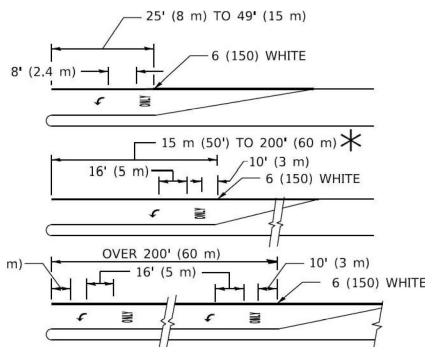
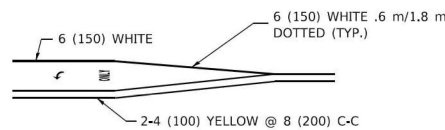
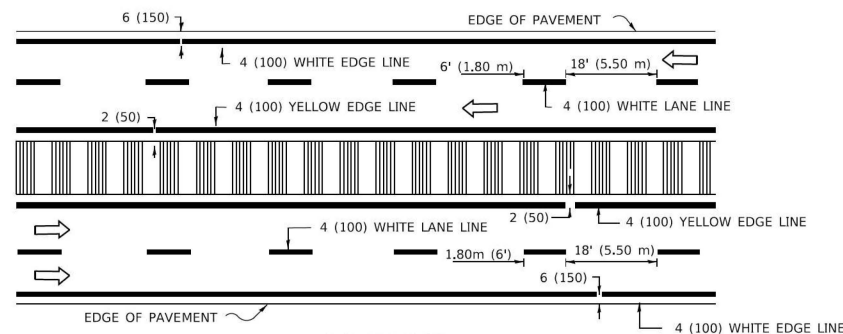
TYPICAL ISLAND MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



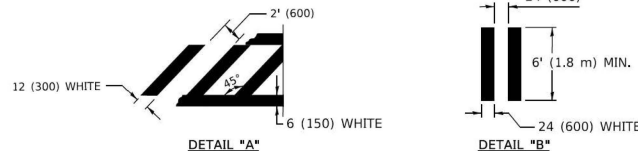
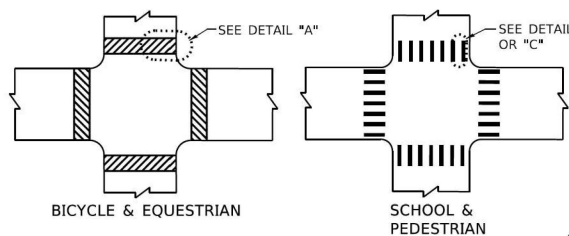
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - *ONLY* INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - *ONLY*.

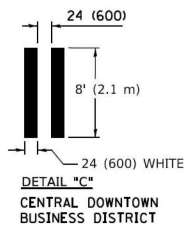
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footemj	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
	DRAWN -	REVISED - K. ENG 02-28-12
PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	222
TC-24		CONTRACT NO.62U41		
		ILLINOIS	FED. AID PROJECT	

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		CHECKED	- DJK	REVISED	-
PLOT DATE	= 8/13/2025	DATE	- 8/13/2025	REVISED	-

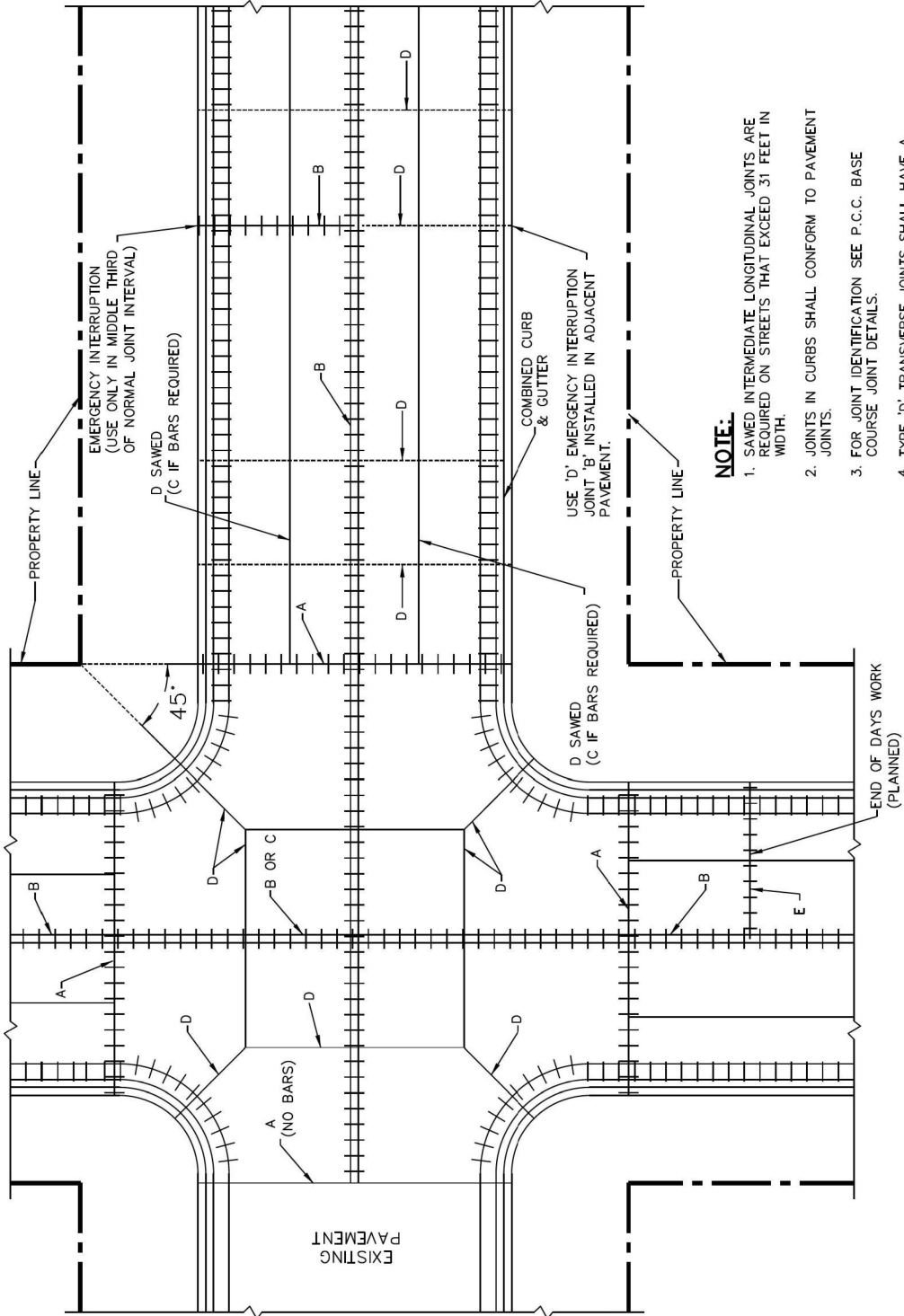
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 1 OF 18 SHEETS STA. TO STA.

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	223
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	TYPICAL JOINT LAYOUT FOR P.C. CONCRETE PAVEMENTS		
			DATE	SHEET	DRAWN BY
			12/26/06	A-2-3A	CDOT



- NOTE:**
- SAWED INTERMEDIATE LONGITUDINAL JOINTS ARE REQUIRED ON STREETS THAT EXCEED 31 FEET IN WIDTH.
 - JOINTS IN CURBS SHALL CONFORM TO PAVEMENT JOINTS.
 - FOR JOINT IDENTIFICATION SEE P.C.C. BASE COURSE JOINT DETAILS.
 - TYPE 'D' TRANSVERSE JOINTS SHALL HAVE A SPACING OF 20 FEET ON ALL LONGITUDINAL CUTS THAT EXCEED 20 FEET IN LENGTH.
 - SEE SPECIAL PROVISION FOR JOINING P.C.C. PAVEMENT AND P.C.C. BASE.



USER NAME = kdc
DRAWN = KDC
CHECKED = DKJ
DATE = 8/13/2025

DESIGNED - KDC
DRAWN - KDC
CHECKED - DKJ
DATE - 8/13/2025

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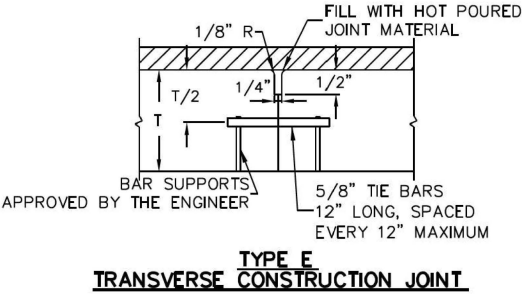
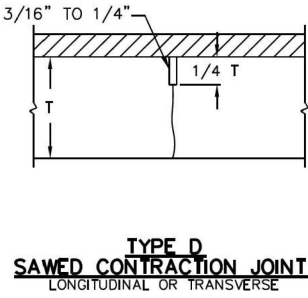
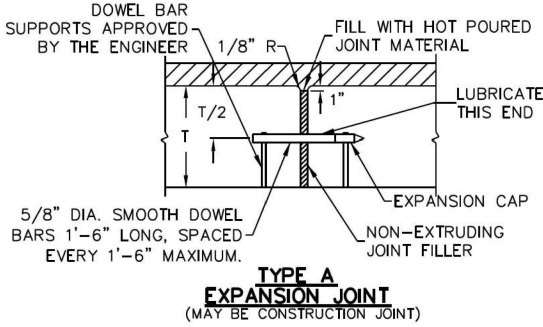
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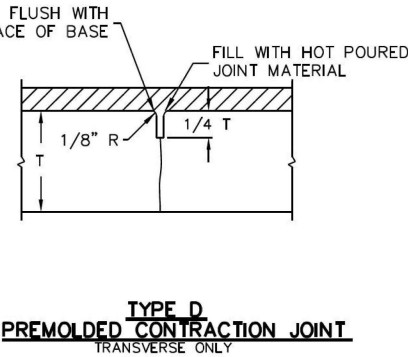
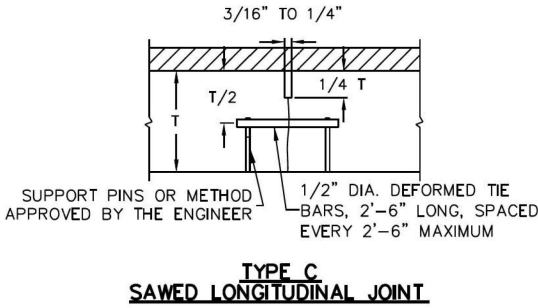
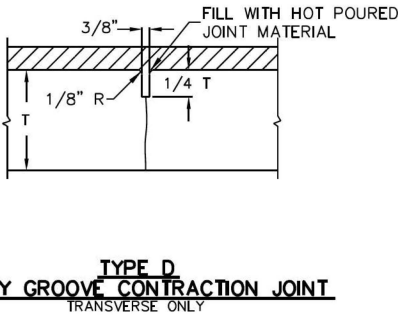
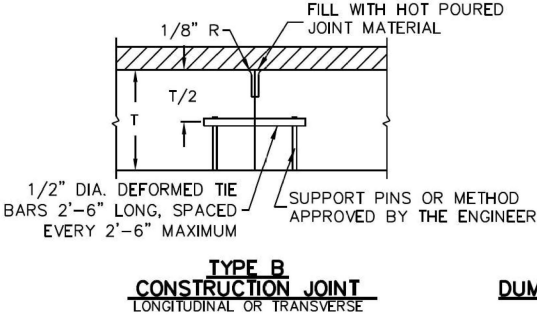
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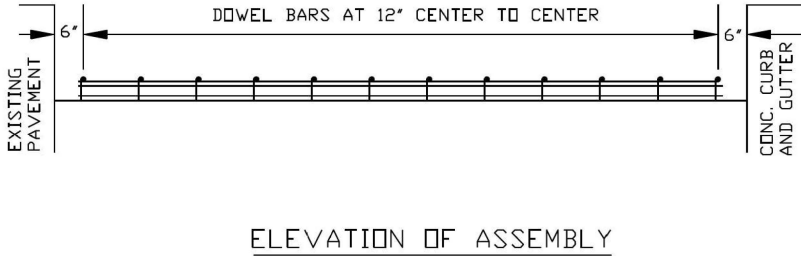
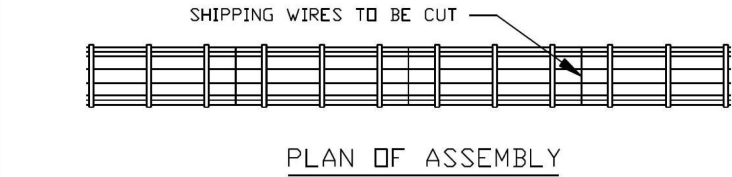
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	DATE	12/27/06	SHEET	A-2-3C	
	DRAWN BY	CDOT			
	P.C.C. BASE COURSE JOINT DETAILS				



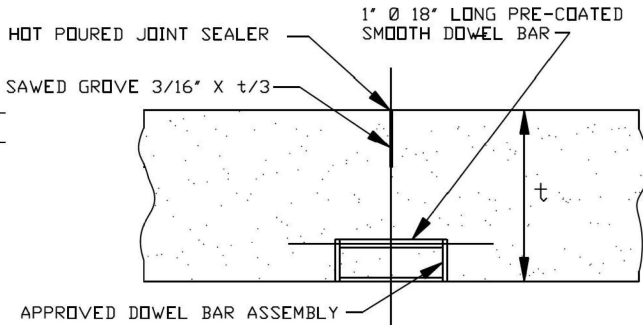
- NOTES:**
1. DEFORMED TIE BARS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M-31 OR M-53 WITH AN ELONGATION NOT LESS THAN 20%.
 2. HOT POURED JOINT MATERIAL SHALL CONFORM TO THE REQUIREMENTS OF AASHTO SPECIFICATION M-173-60 FOR CONCRETE JOINT SEALER.
 3. ALL TIE BARS AND DOWEL BARS ARE TO BE EPOXY COATED.
 4. SPLIT BOARD HEADERS WILL NOT BE ALLOWED.



	DATE		REVISION		CITY OF CHICAGO
	DATE	1/1/14	SHEET	A-2-3F	
	DRAWN BY	CDOT			
	CONTRACTION JOINT ASSEMBLY FOR BUS PAD				

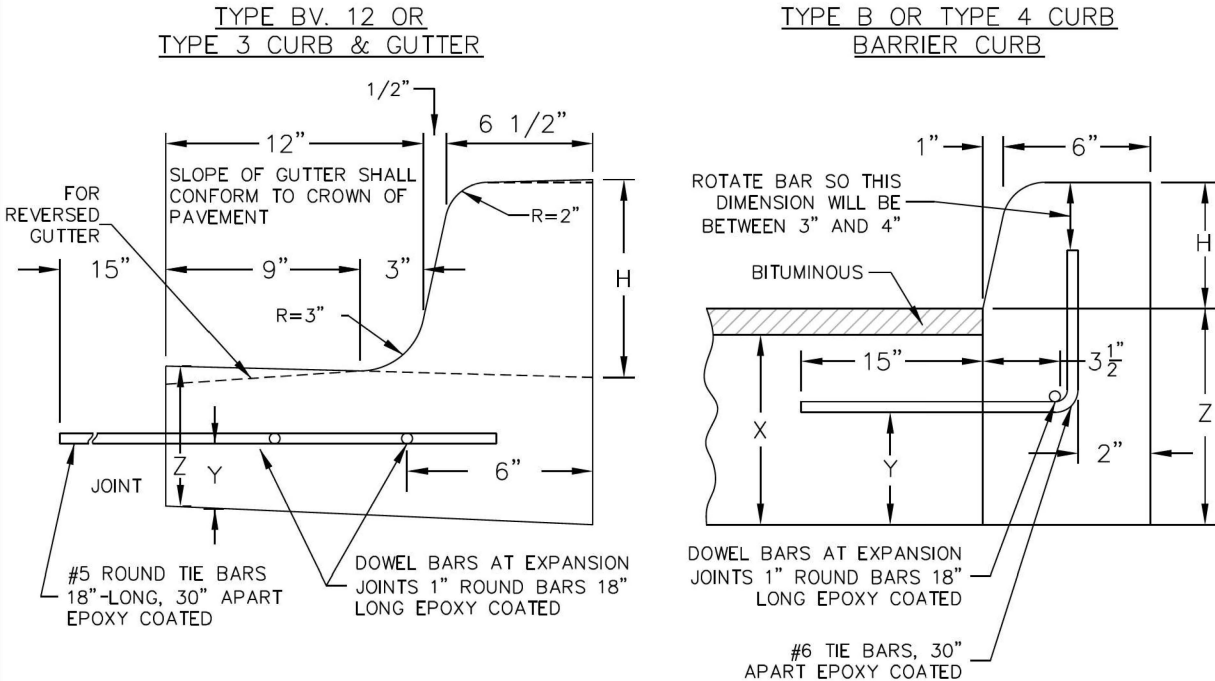


- NOTE:**
1. MINIMUM WIRE SIZES, W7 OUTSIDE SPACERS, BEARING MEMBERS AND UPRIGHT SUPPORT, W5 INSIDE SPACERS W3 TIE WIRES.
 2. DOWELS MUST BE PLASTIC COATED, REBARS MUST BE EPOXY COATED.
 3. DOWEL BASKET MUST BE INSTALLED EVERY 20 FEET.
 4. TRANSVERSE JOINTS SHALL BE SPACED EVERY 20 FEET.
 5. TRANSVERSE JOINTS MUST BE PROVIDED AT THE CENTER OF EVERY STRUCTURES IN THE BUS PAD.



~~INCIDENTAL TO PAY ITEM PAVEMENT REMOVAL & REPLACEMENT FOR 10-INCH, HES~~
INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE PAVEMENT, 10"

NOTE: H = VARIABLE 3" TO 9"
X = THICKNESS OF PAVEMENT
Y = ONE HALF THE THICKNESS OF CONCRETE PAVEMENT OR CONCRETE BASE.
Z = 10" OR THICKNESS OF PAVEMENT - WHICHEVER IS GREATER



JOINTS IN CURB, COMBINED CURB & GUTTER

TRANSVERSE JOINTS OF A TYPE SIMILAR TO THAT USED IN THE ADJACENT PAVEMENT SHALL BE INSTALLED IN THE CURB, GUTTER AND COMBINED CURB & GUTTER IN PROLONGATION WITH THE JOINTS IN THE PAVEMENT. THE DETAILS OF THE TRANSVERSE JOINTS IN THE CURB, GUTTER AND COMBINED CURB & GUTTER SHALL BE APPROVED BY THE COMMISSIONER. CURB, GUTTER OR COMBINED CURB & GUTTER IS CONSTRUCTED ADJACENT TO A FLEXIBLE BASE PAVEMENT, 1" THICK EXPANSION JOINTS COMPOSED OF BITUMINOUS PERFORMED JOINT FILLER SHALL BE INSTALLED IN THE CURB AND/OR GUTTER AT POINTS OF CURVATURE AND AT CONSTRUCTION JOINTS. CONTRACTION JOINTS SHALL ALSO BE PLACED BETWEEN THESE EXPANSION JOINTS AT DISTANCES NOT EXCEEDING 20 FEET. ALL TIE BARS SHALL BE DEFORMED-ALL DOWEL BARS SHALL BE SMOOTH.

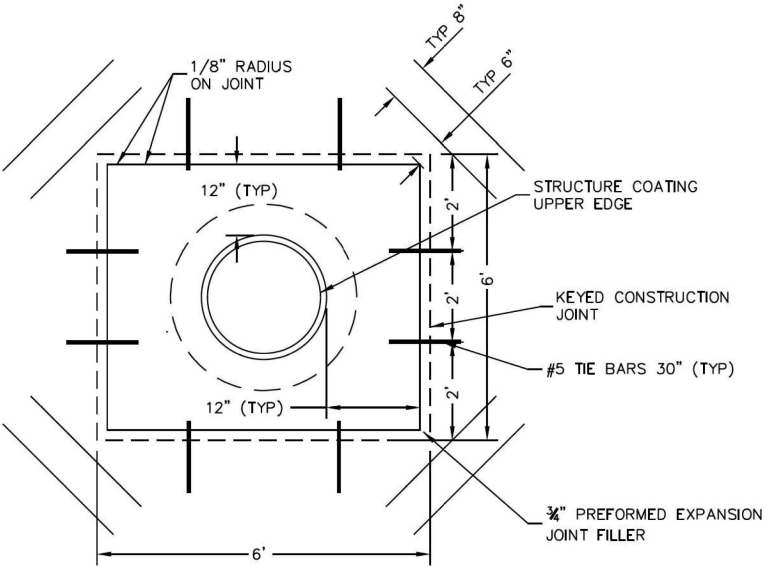
NOTE: ALL TIE BARS AND DOWEL BARS TO BE EPOXY COATED.

*AT LOCATIONS REQUIRING DEPRESSED CURBS SEE THE ADA STANDARDS FOR CONSTRUCTION DETAILS



DATE	REVISION	CITY OF CHICAGO		
1/1/2014	REVISION 1	CONCRETE CURB & GUTTER DETAIL		
		DATE	SHEET	DRAWN BY
		12/12/06	A-2-6	CDOT

2-#4 DEFORMED TIE BARS 30" LONG AT A MID DEPTH (TYPICAL) SEE SPECIAL CONDITION BELOW.



SPECIAL CONDITIONS:

TIE BARS SHALL NOT BE INSTALLED AT ISOLATION BOX CORNERS WHERE EITHER SIDE OF THE BOX FORMING SAID CORNER IS A LONGITUDINAL OR TRAVERSE JOINT. MOREOVER, AT NO TIME SHALL A TIE BAR CROSS A JOINT (ALREADY FORMED OR PROPOSED) IN THE VICINITY OF THE ISOLATION BOX. IF THIS SITUATION OCCURS, THE TIE BAR SHALL BE ADJUSTED PARALLEL TO THE AXIS OF THE BAR SO THAT THE END OF THE BAR IS NO CLOSER THAN 1 1/2" TO THE JOINT.

ONLY BY THE DIRECTION OR APPROVAL OF THE COMMISSIONER SHALL THE DISTANCE BETWEEN THE UPPER EXTERNAL CASTING EDGE AND THE EDGE OF STANDARD ISOLATION BOX, SHOWN AS 12", BE INCREASED SO THAT AN IMMOVABLE LONGITUDINAL JOINT AND (OR) TRAVERSE JOINT WILL THEN THENCE FORM (O) SIDE (S) OF THE BOX. THIS ADJUSTMENT WILL BE ALLOWED ONLY WHEN THE DISTANCE BETWEEN THE SIDE OF THE STANDARD ISOLATION BOX AND IMMOVABLE JOINT IS 18" OR LESS.

BACKFILL MATERIAL AROUND STRUCTURE WILL BE COMPACTED TO 95% MODIFIED PROCTOR PRIOR TO THE PLACEMENT OF CONCRETE WITHIN THE ISOLATION BOX.



DATE	REVISION	CITY OF CHICAGO		
1/1/2014	REVISION 1	DETAILS OF STRUCTURE CASTING ISOLATION BOX		
		DATE	SHEET	DRAWN BY
		12/21/06	A-2-10A	CDOT



USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

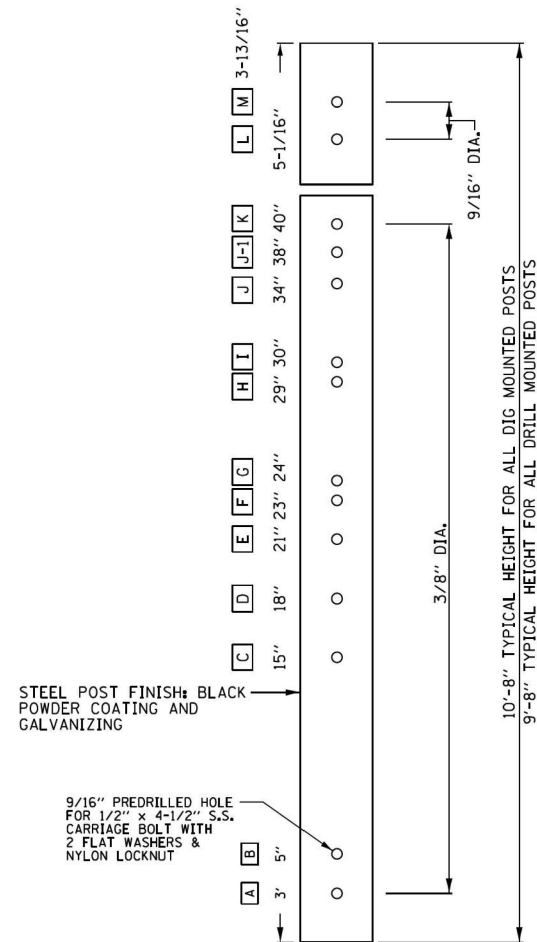
I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 3 OF 17 SHEETS

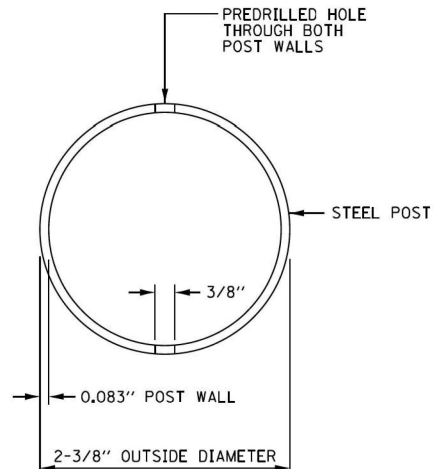
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	225
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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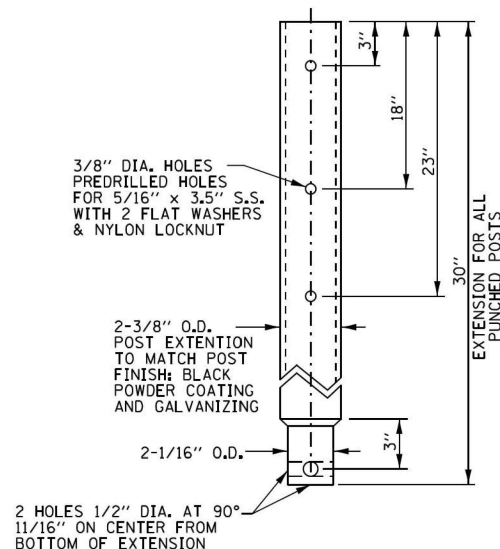


① ELEVATION: SIGN POST
NOT TO SCALE



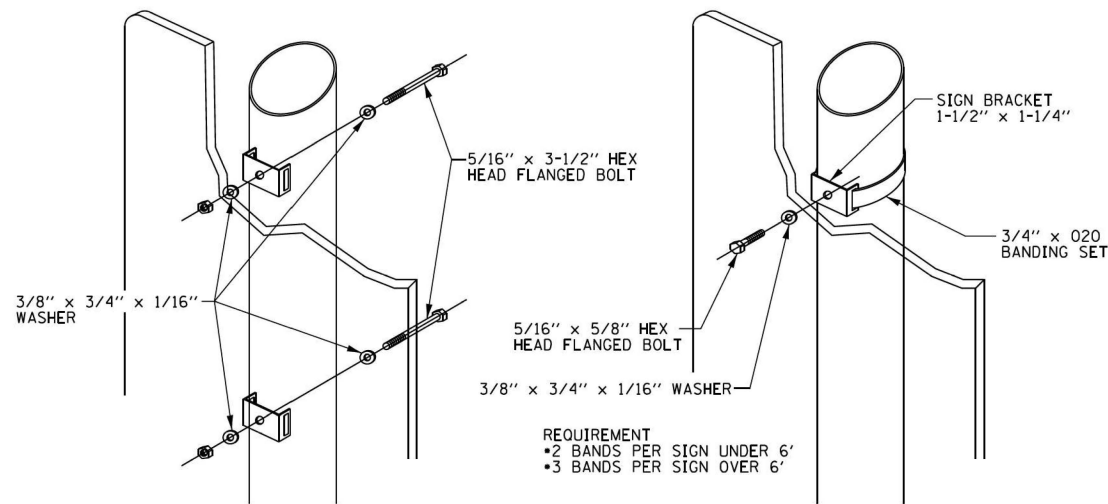
② SECTION: SIGN POST
NOT TO SCALE

SIGN POST DETAILS
NOT TO SCALE



SECTION: DRILLED POST EXTENSION
NOT TO SCALE

NOTES:
PROVIDE ADDITIONAL TWO SETS OF PREDRILLED HOLES ON EXTENSION. HOLES SHALL BE LOCATED AT 30° ANGLE TO HOLES SHOWN IN SECTION 3. HOLES SHALL ACCOMMODATE 5/16"x 3.5" S.S. SOCKET HEAD SCREW WITH 2 FLAT WASHERS AND NYLON LOCKNUT.
2 HOLE PATTERN 3" FROM REDUCED COLLAR
3 HOLE PATTERN FROM TOP OF POST 3", 18", 23" 23*8" x 0.08" WALL

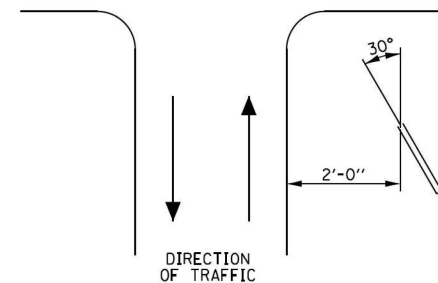


2-3/8" O.D. PIPE
SIGN ASSEMBLY

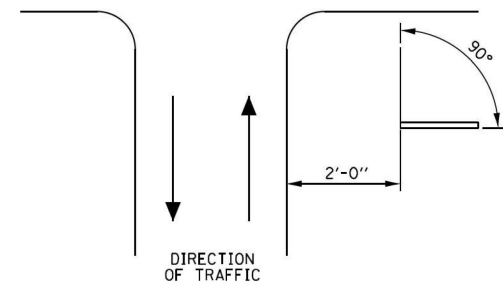
LIGHT STANDARD
ASSEMBLY

NOTE:
PROVIDE MINIMUM 7'-0" CLEARANCE BETWEEN GROUND AND BOTTOM OF SIGN. RAISE INITIAL INSTALLATION HEIGHT IF REQ'D TO MAINTAIN 7' CLEAR.

CENTER MOUNTED SIGN ASSEMBLY
NOT TO SCALE

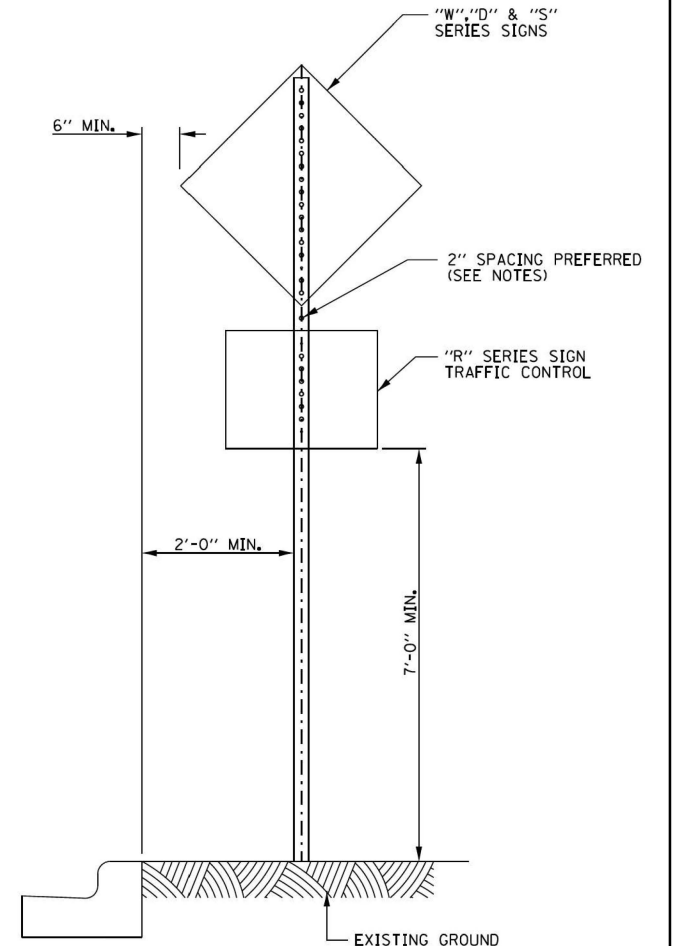


MOUNTING DETAIL
FOR: TRAFFIC CONTROL SIGNS WITH DIRECTIONAL ARROWS
PARKING REGULATIONS SIGNS WITH DIRECTIONAL ARROWS



TYPICAL MOUNTING DETAIL

SIGN MOUNTING DETAILS
NOT TO SCALE



NOTE:
2" SPACING MAY BE REDUCED IF REQUIRED TO MAINTAIN MIN. 7'-0" CLEARANCE TO BASE OF SIGN.

CENTER MOUNTED SIGN INSTALLATION
NOT TO SCALE

DATE		REVISION		CITY OF CHICAGO	
				SIGN POST MOUNTING AND INSTALLATION DETAILS	
				DATE	DRAWN BY
				12/21/2020	



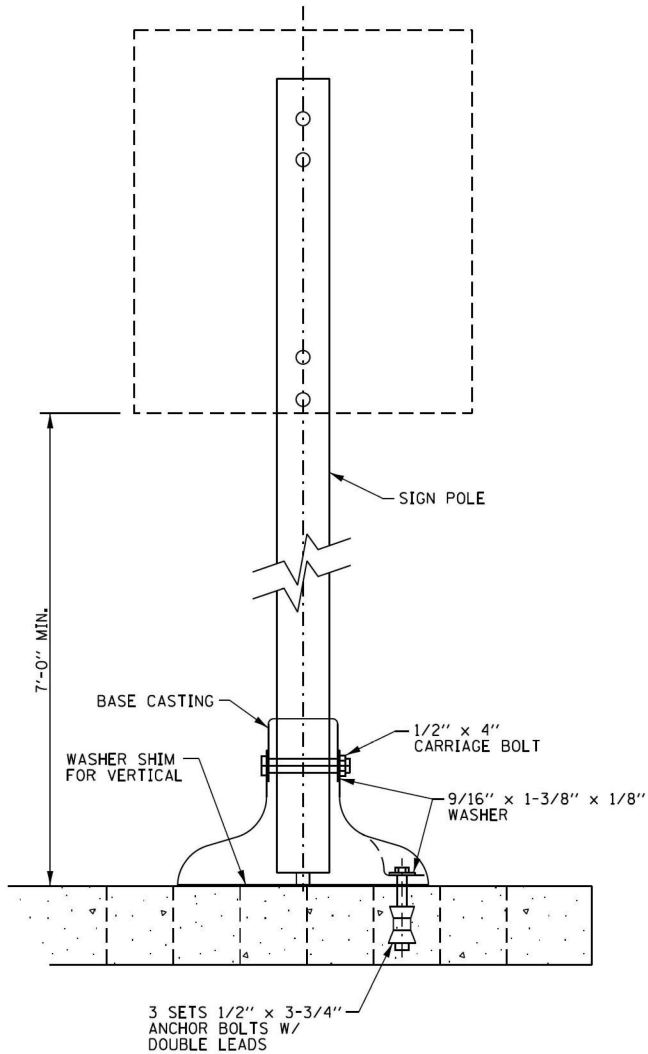
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	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

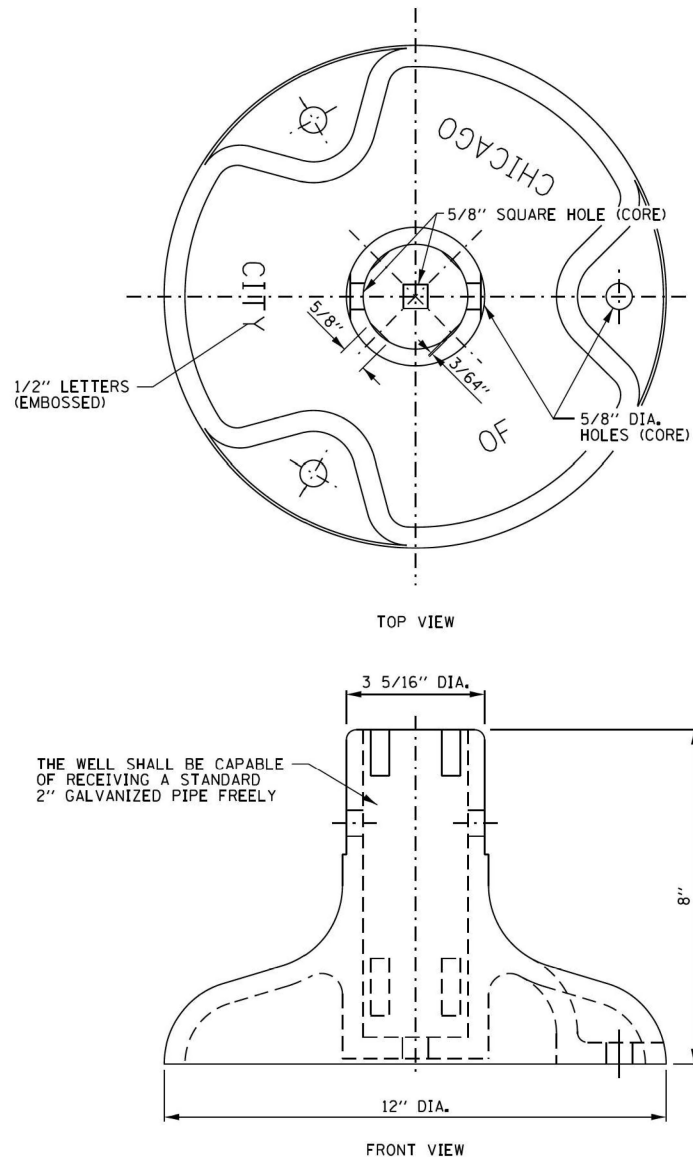
I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 4 OF 17 SHEETS

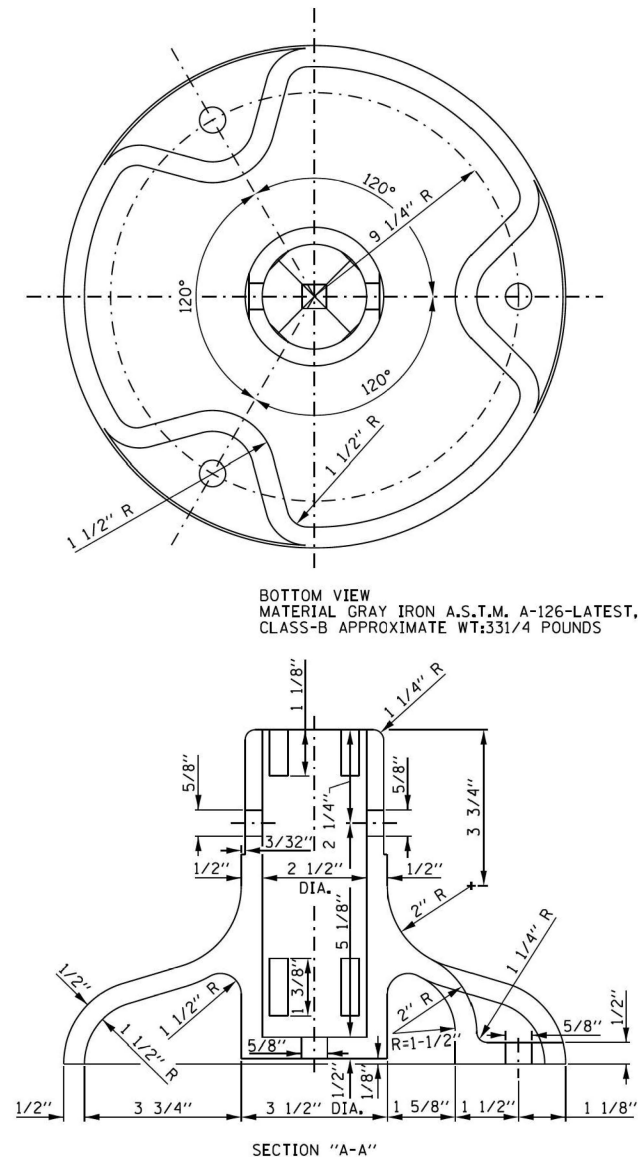
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	226
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



DRILL MOUNTED INSTALLATION DETAIL
NOT TO SCALE



CITY OF CHICAGO SIGN BASE
NOT TO SCALE



DATE		REVISION		CITY OF CHICAGO		
				DRILL - SIGN POST MOUNTING AND INSTALLATION DETAILS		
				DATE		DRAWN BY
				12/21/2020		

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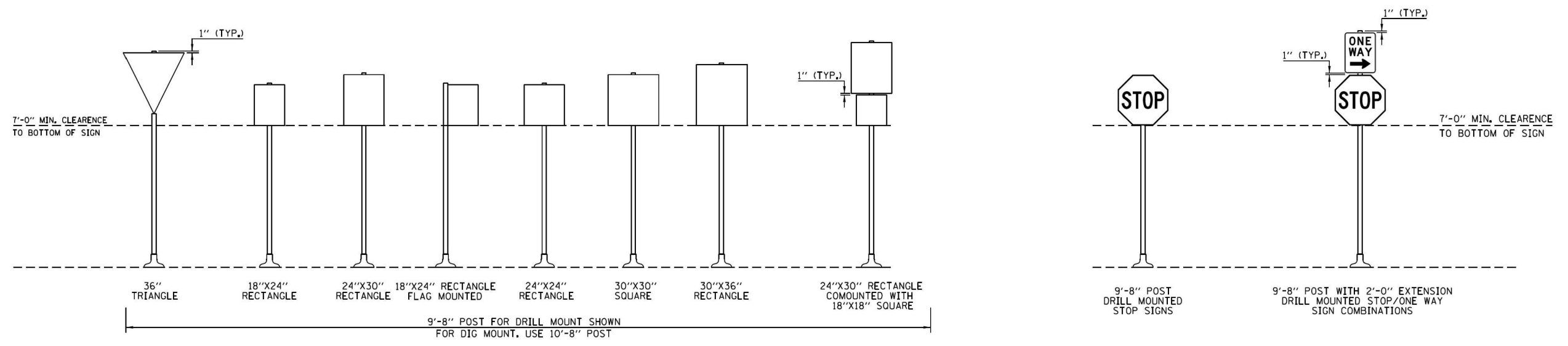
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PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 5 OF 17 SHEETS

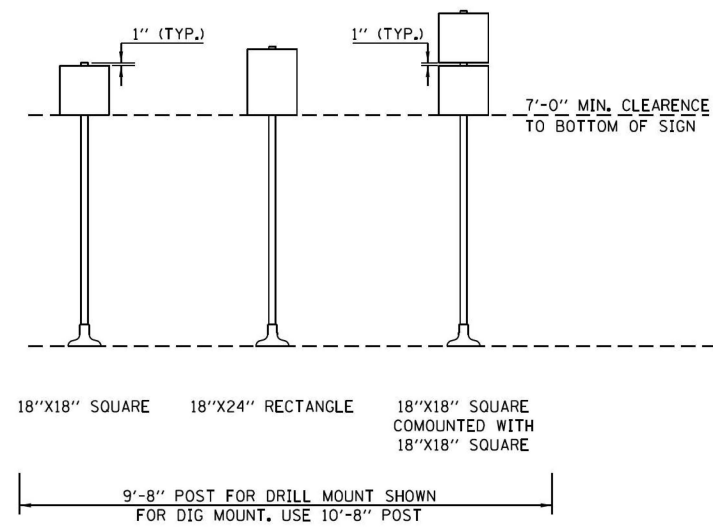
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290	FAI 290 22 KEELER BR	COOK	283	227
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



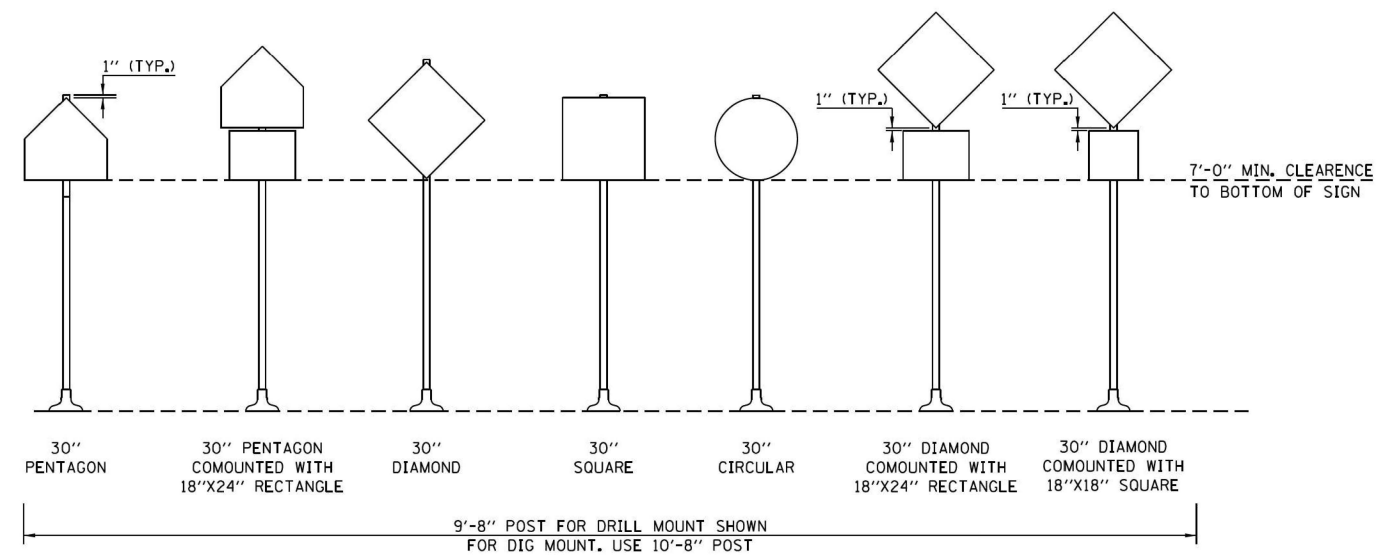
REGULATORY TRAFFIC SIGNS
NOT TO SCALE

NOTES:
THIS EXHIBIT ONLY SHOWS DRILL MOUNT FOR VISUAL PURPOSES. DIG MOUNT INSTALLATIONS SHOULD BE 10'-8".
IF THE MINIMUM CLEARANCE OF 7'-0" CANNOT BE ACHIEVED, INSTALL WITH A POST EXTENSION.

STOP AND ONE-WAY SIGNS
NOT TO SCALE



REGULATORY PARKING SIGNS
NOT TO SCALE



WARNING AND SCHOOL SIGNS
NOT TO SCALE

DATE	REVISION	CITY OF CHICAGO			
		SIGN LAYOUT INSTALLATION			
		DATE		DRAWN BY	
		12/21/2020			

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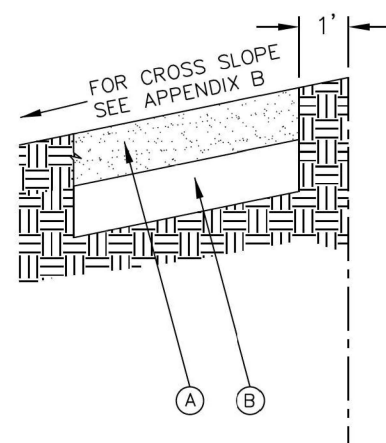
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PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 6 OF 17 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	228
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



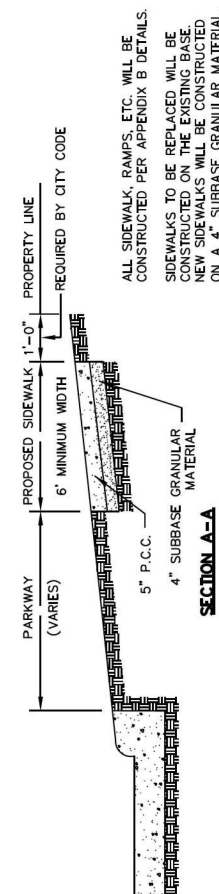
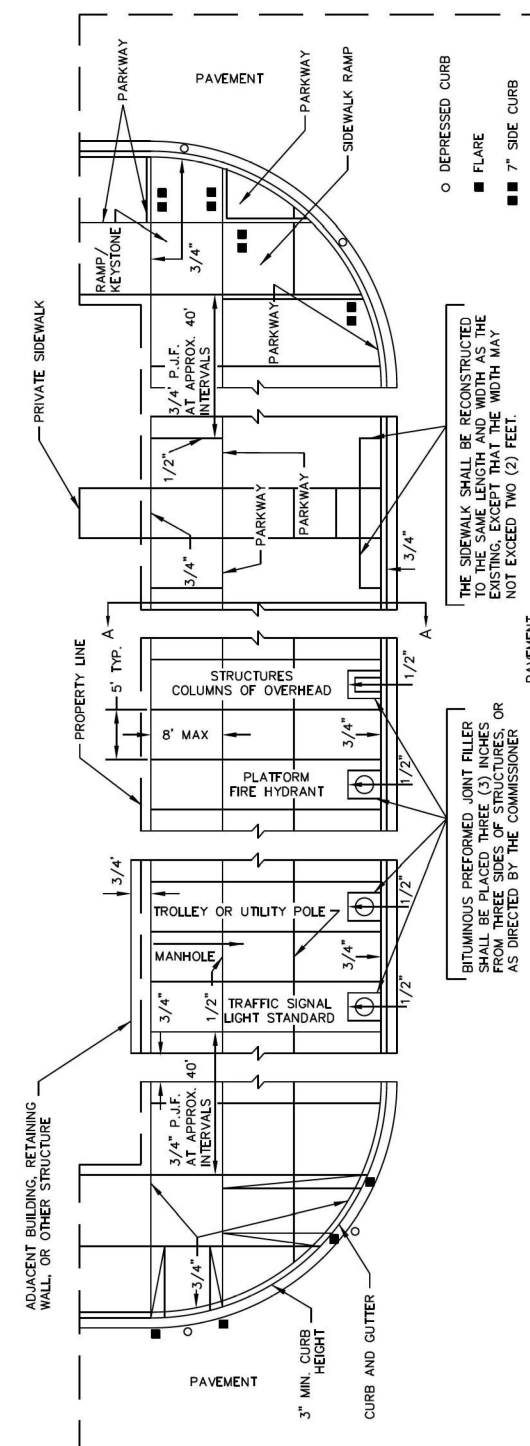
LEGEND:

- (A) PROPOSED 5" P.C.C. SIDEWALK *
- (B) SUBBASE GRANULAR MATERIAL, TYPE B OR C, 4"

* CURB RAMP & KEYSTONE 8" P.C.C. AT SIGNALIZED AND INDUSTRIAL STREET INTERSECTIONS



DATE	REVISION	CITY OF CHICAGO		
1/1/2014	REVISION 1	ADA COMPLIANT SIDEWALK CONSTRUCTION DETAILS		
		DATE	SHEET A-3-1	DRAWN BY
		12/20/06		CDOT



* THE 1/2" AND 3/4" DIMENSIONS REFER TO THE THICKNESS OF THE BITUMINOUS PREFORMED JOINT FILLER REQUIRED AT THE VARIOUS LOCATIONS.



DATE	REVISION	CITY OF CHICAGO		
1/1/2014	REVISION 1	DETAILS OF PORTLAND CEMENT CONCRETE CONSTRUCTION		
		DATE	SHEET	DRAWN BY
		12/22/06	A-3-2	CDOT



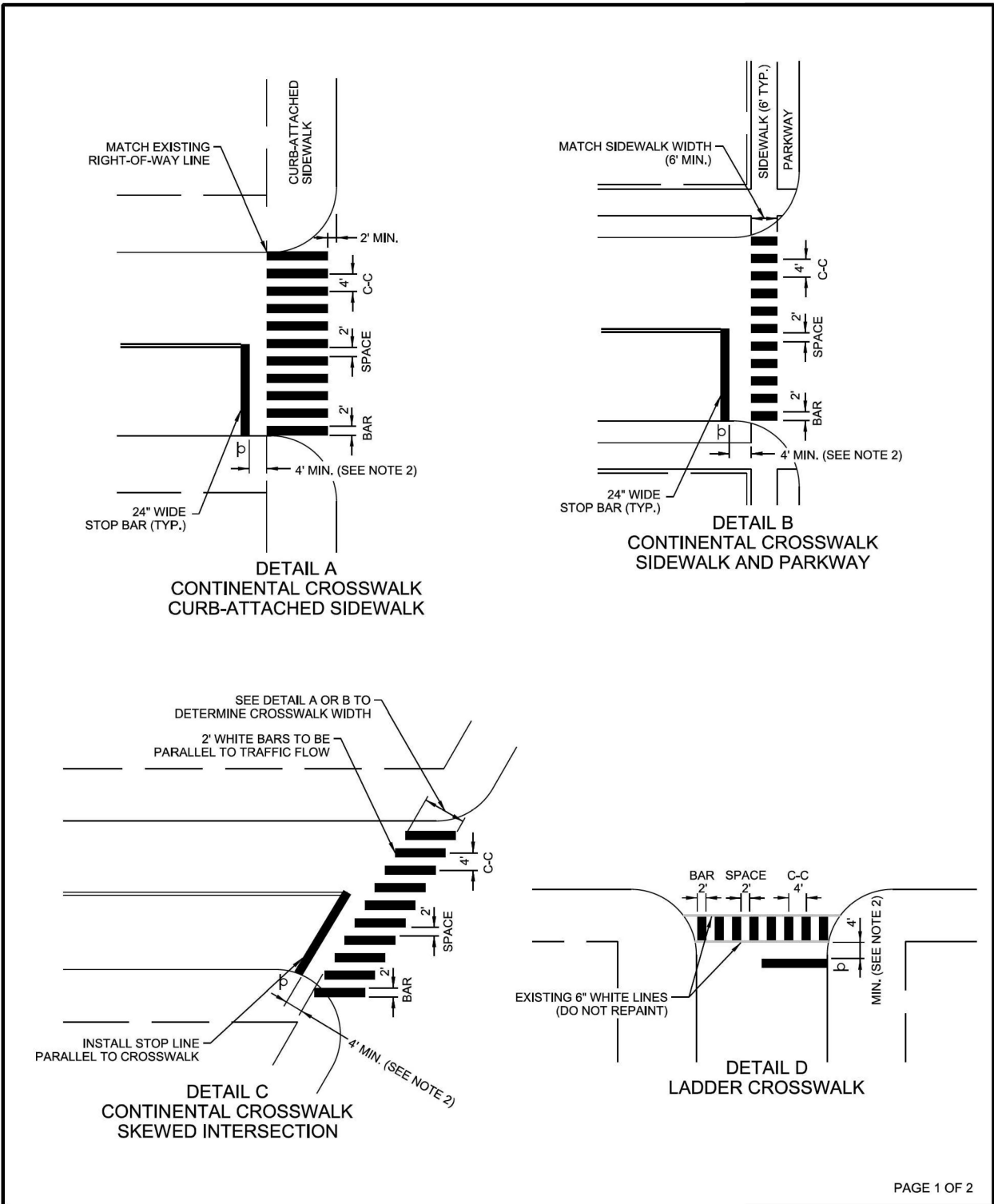
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PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

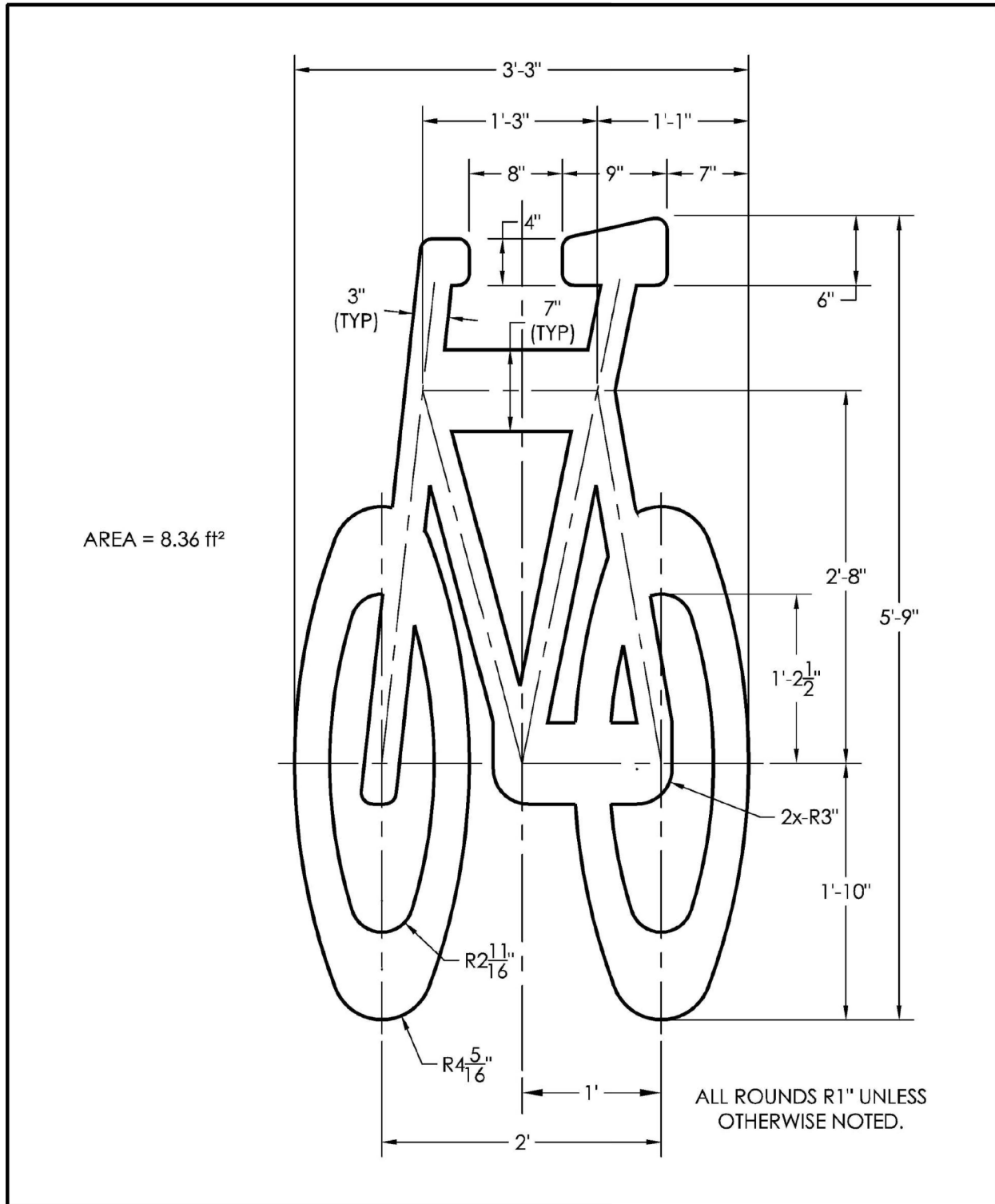
I-290 AT KEELER AVENUE CDOT STANDARD DETAILS


SCALE: N.T.S.	SHEET 8	OF 17	SHEETS
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F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	230
		CONTRACT NO. 62U41		
	ILLINOIS	FED. AID PROJECT		



 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	CROSSWALK MARKING DETAIL		
			DATE	SHEET	DRAWN BY
			06/25/12	A-7-1A	CDOT



 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	6' BIKE SYMBOL		
			DATE	SHEET	DRAWN BY
			06/25/12	A-7-2	CDOT

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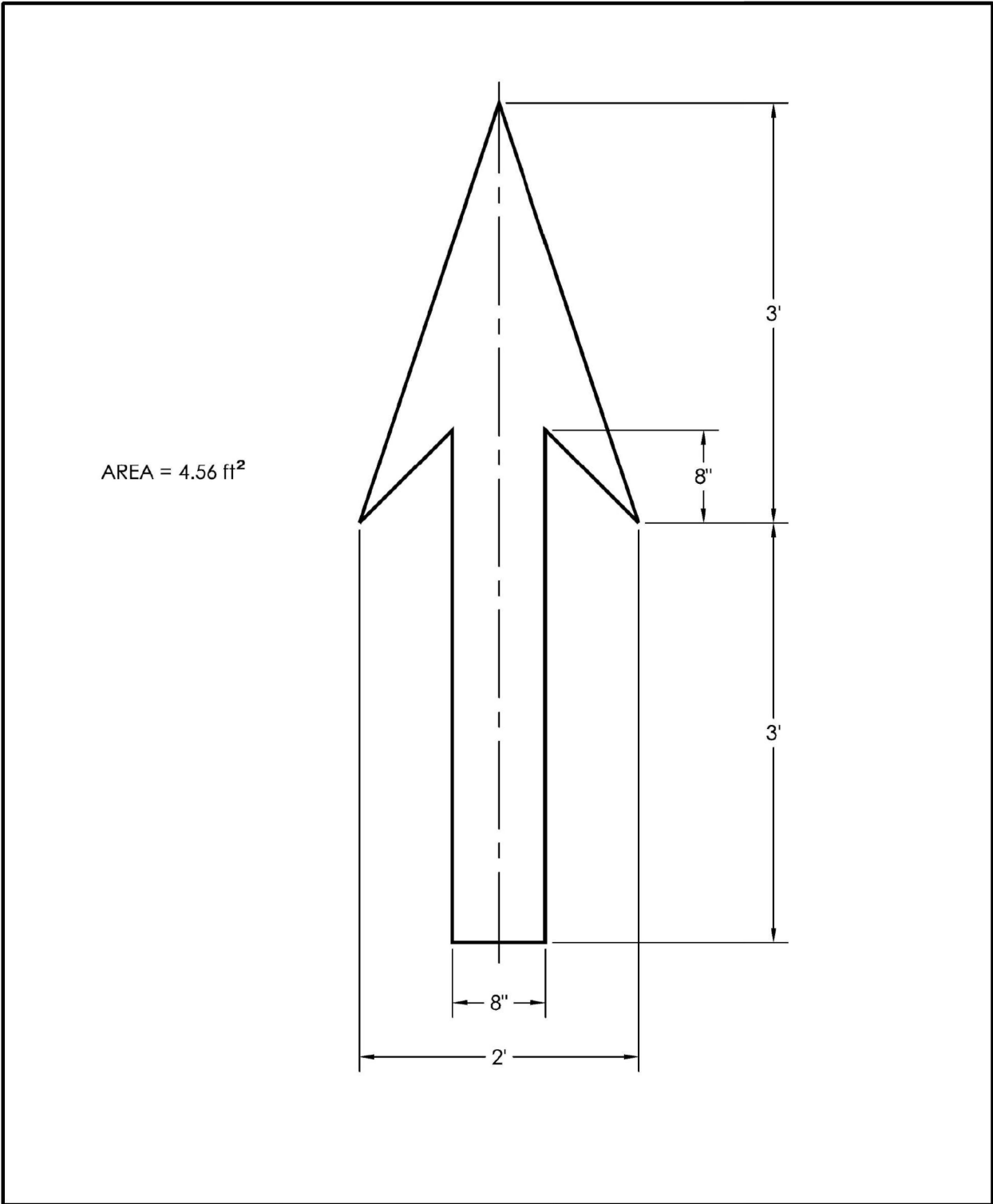
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PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

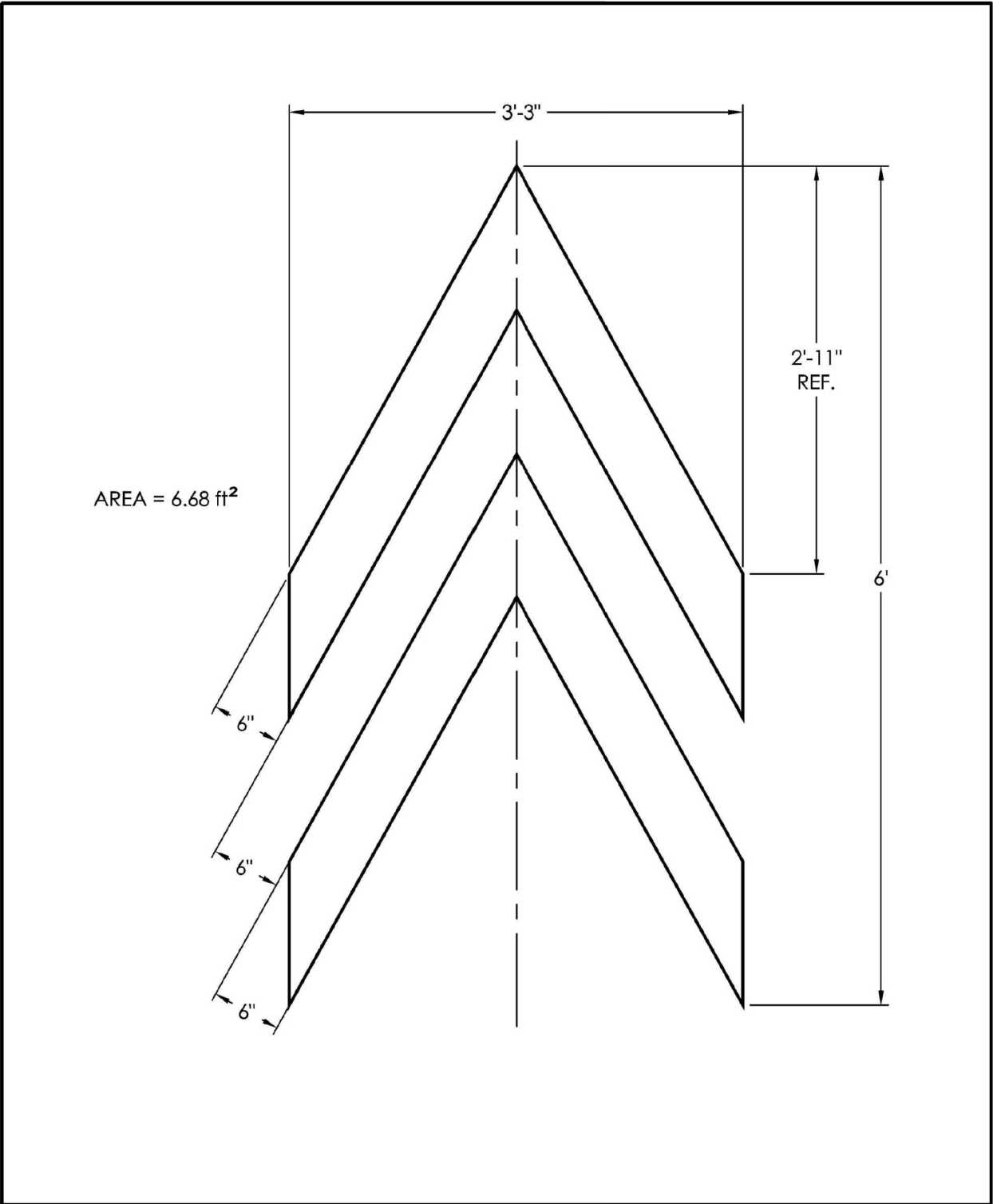
I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS


SCALE: N.T.S. SHEET 9 OF 17 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	231
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	BIKE ARROW		
			DATE	SHEET	DRAWN BY
			06/25/12	A-7-4	CDOT



 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	BIKE CHEVRON		
			DATE	SHEET	DRAWN BY
			06/25/12	A-7-6	CDOT

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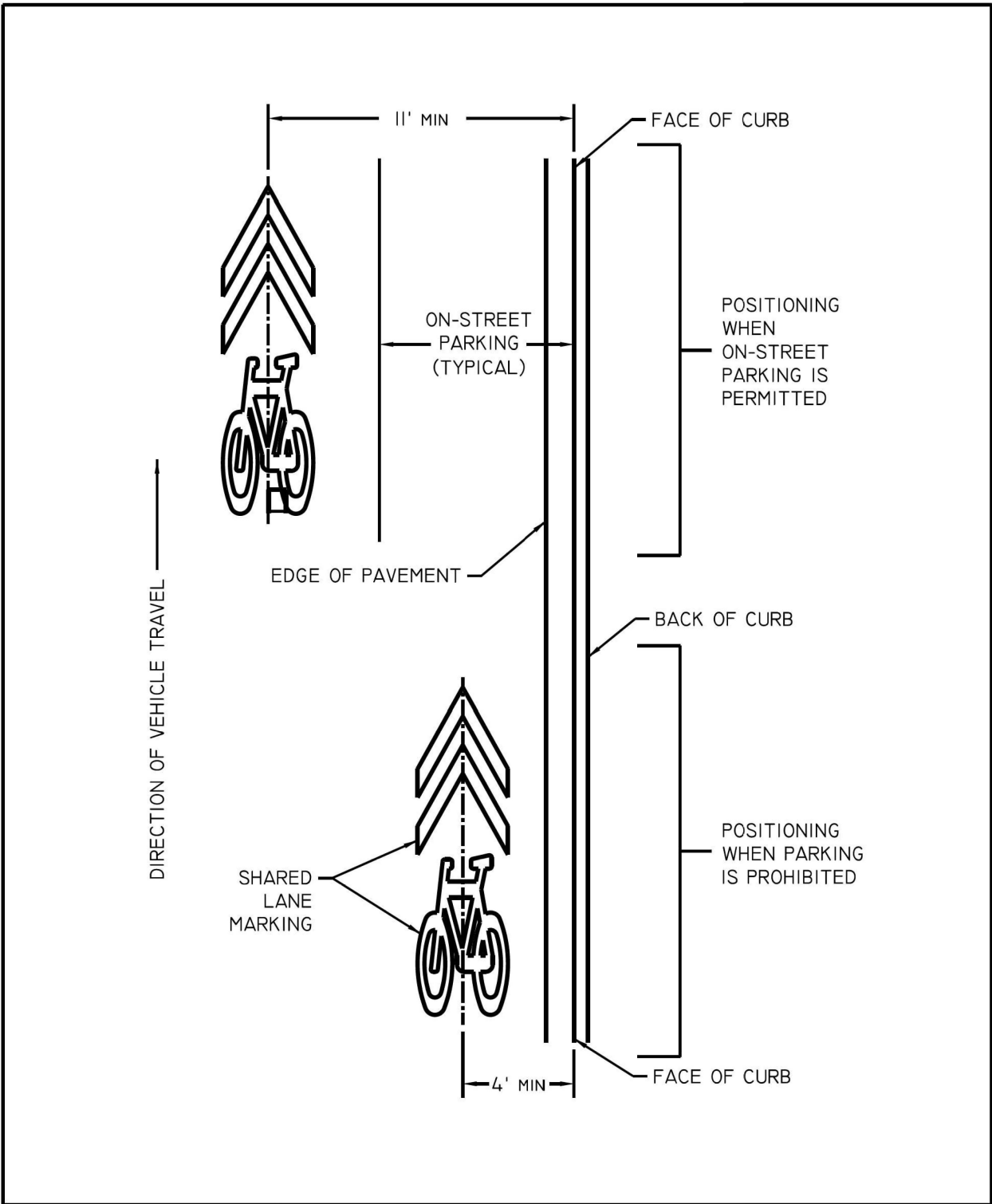
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PLOT DATE	= 8/13/2025	DATE	- 8/13/2025	REVISED	-


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

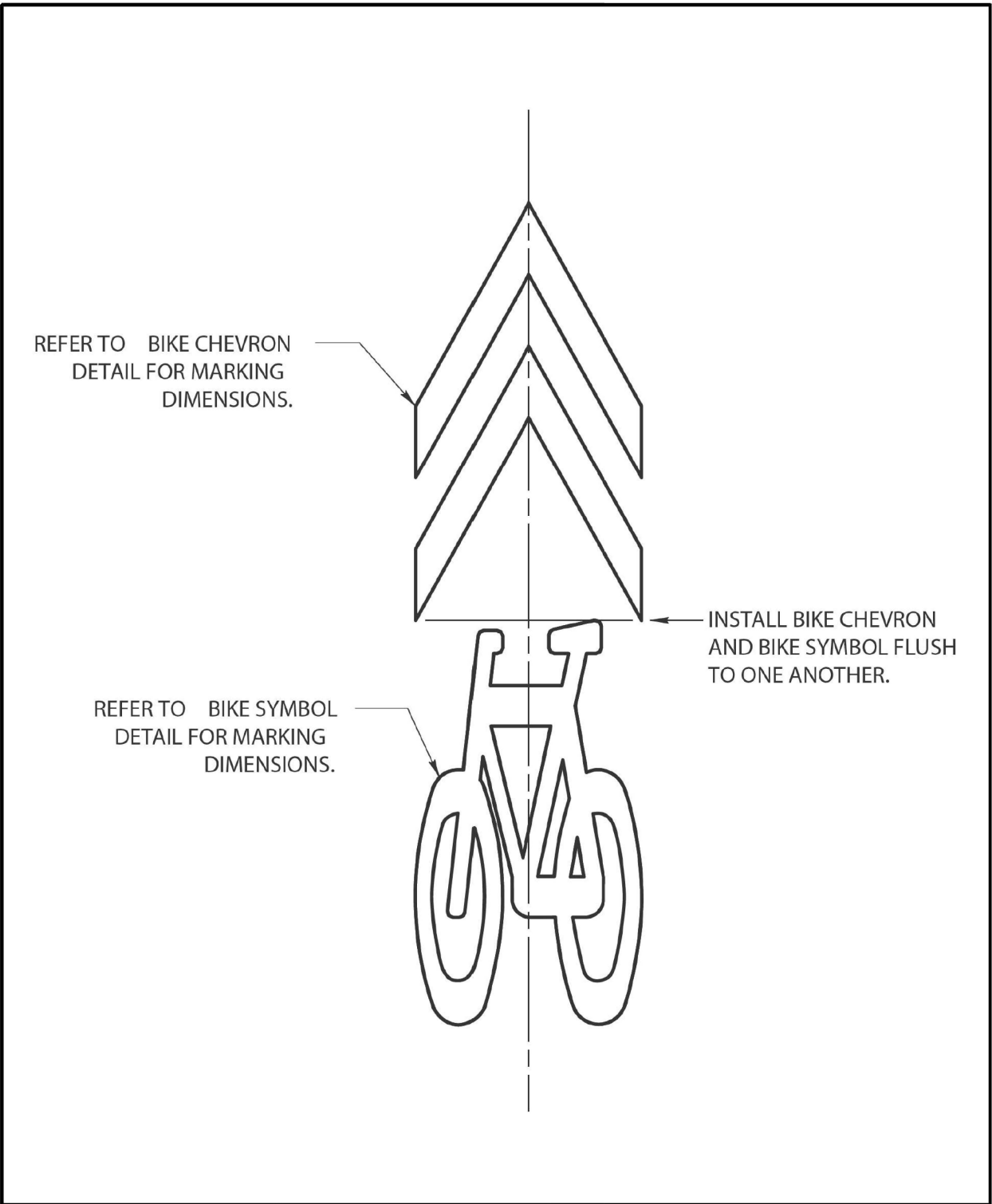
I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS


SCALE: N.T.S. SHEET 10 OF 17 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	232
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	SHARED LANE MARKING LATERAL POSITIONING		
			DATE	SHEET	DRAWN BY
			09/15/05	A-7-8	CDOT



 CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	SHARED LANE MARKING		
			DATE	SHEET	DRAWN BY
			10/02/07	A-7-11	CDOT

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	CHECKED - DJK	REVISED -
PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

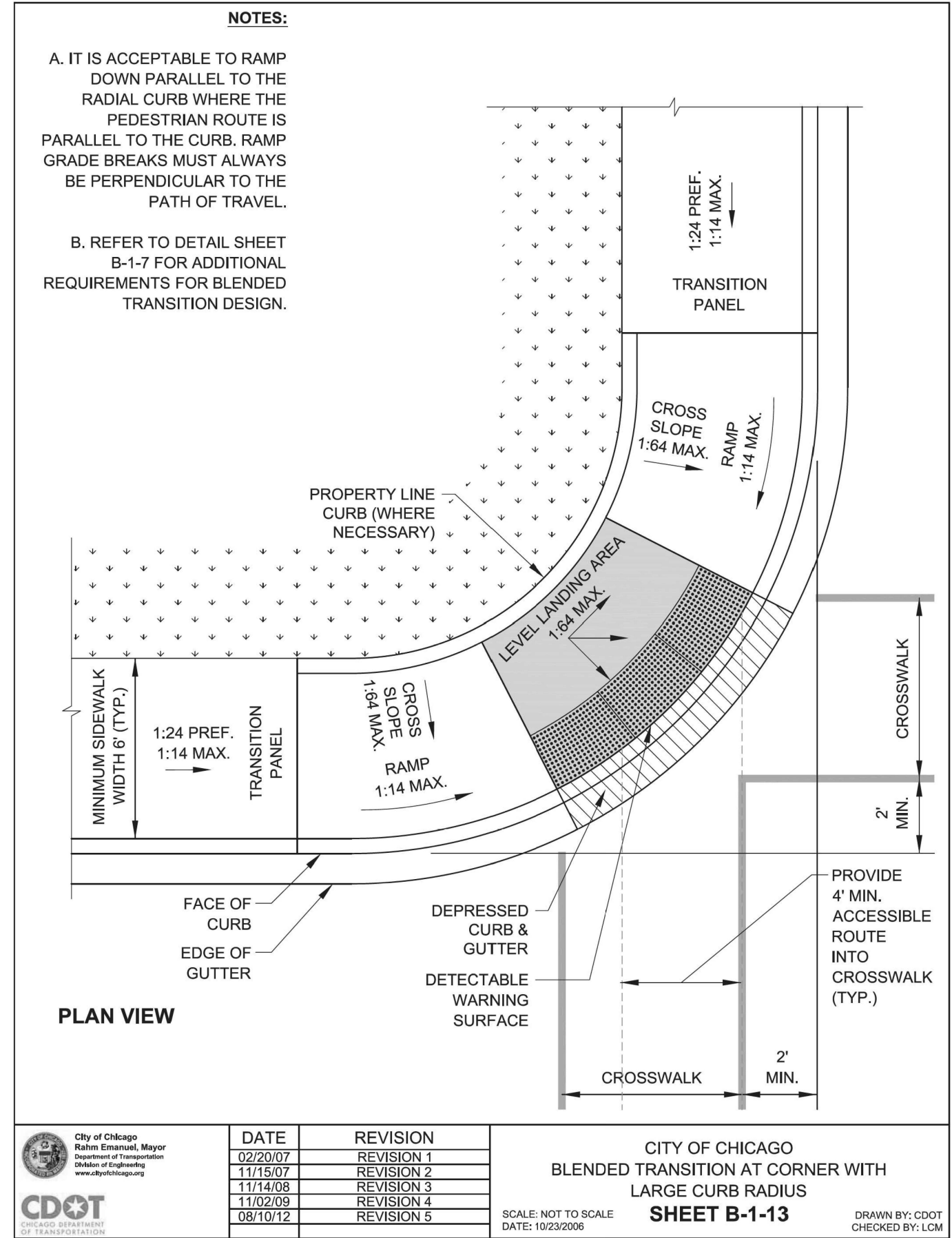
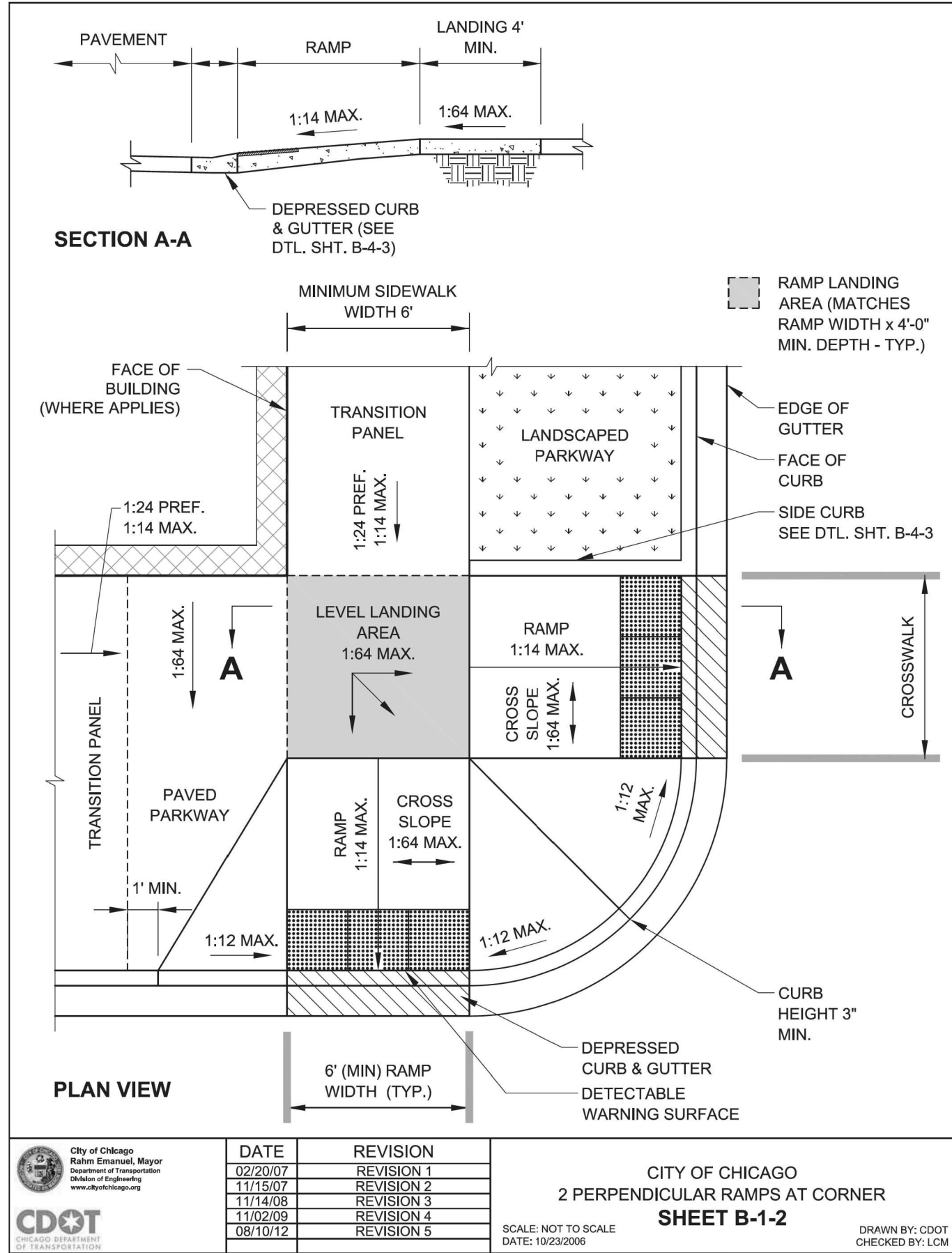
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 11 OF 17 SHEETS

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	233
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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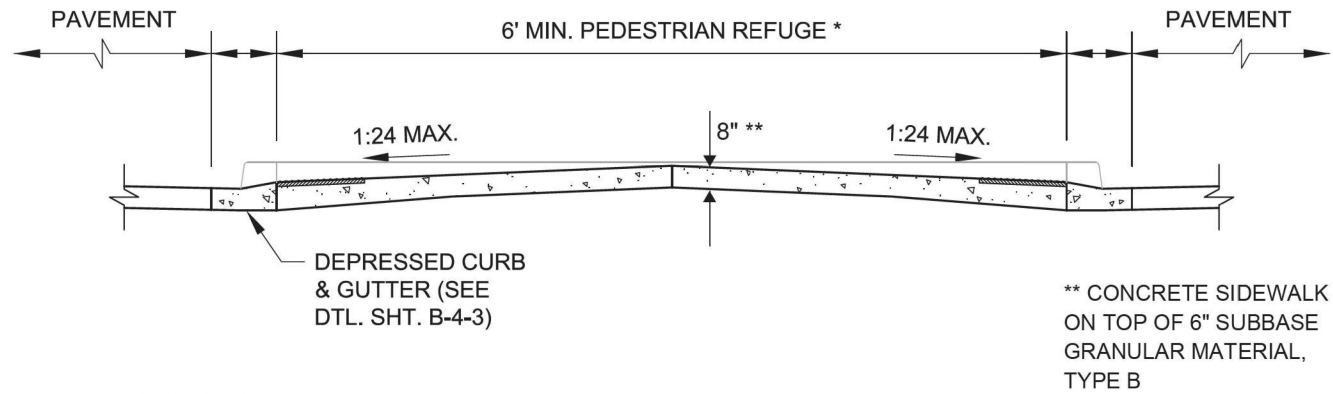
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

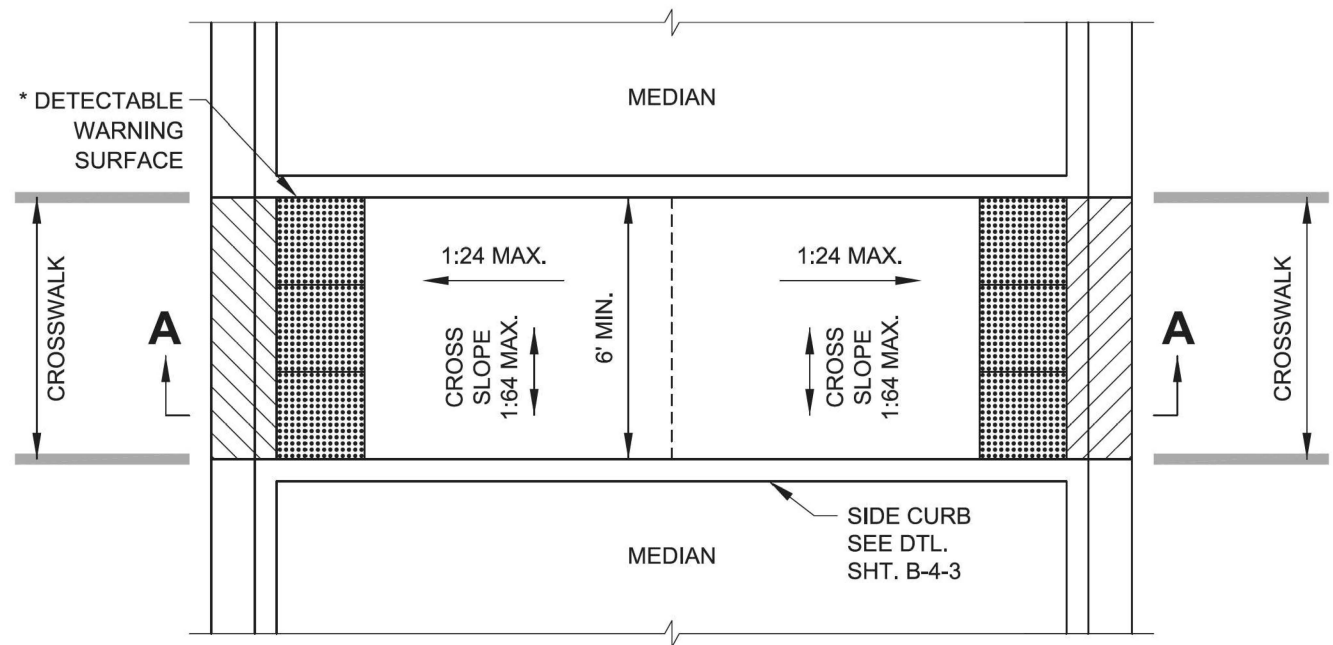
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	234
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

* IF LENGTH OF 6' MIN.
CANNOT BE PROVIDED
FOR PEDESTRIAN
REFUGE, DETECTABLE
WARNING IS NOT TO BE
PROVIDED



SECTION A-A



PLAN VIEW



DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5
01/01/14	REVISION 6

CITY OF CHICAGO
MEDIAN PASS-THROUGH
SHEET B-1-19

SCALE: NOT TO SCALE
DATE: 10/23/2006

DRAWN BY: CDOT
CHECKED BY: GK

SLOPE CONVERSION CHART

% SLOPE	SLOPE RATIO	INCHES PER FOOT	DECIMAL FEET PER FOOT
16.67%	1:6	2"	0.167'
10%	1:10	1 1/4"	0.104'
8.33%	1:12	1"	0.083'
7.14%	1:14	7/8"	0.073'
5%	1:20	5/8"	0.052'
4.17%	1:24	1/2"	0.042'
2%	1:50	1/4"	0.021'
1.56%	1:64	3/16"	0.016'



DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5

CITY OF CHICAGO
CONVERSION CHARTS
SHEET B-3-1

SCALE: NOT TO SCALE
DATE: 10/23/2006

DRAWN BY: CDOT
CHECKED BY: LCM



USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 13 OF 17 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	235
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: CDOT_9 (Sheet)
FILE NAME: c:\bms\civiltech-pw\kyle.dary\0108547\0102141-sht-details.dgn

GENERAL NOTES:

1. THE DETECTABLE WARNING USED SHALL BE CHOSEN FROM THE CHICAGO DEPARTMENT OF TRANSPORTATION LIST OF APPROVED DETECTABLE WARNING PRODUCTS (AVAILABLE ON THE CITY OF CHICAGO WEBSITE). IT IS NOT ACCEPTABLE TO INSTALL TWO DIFFERENT DETECTABLE WARNING PRODUCTS ADJACENT TO ONE ANOTHER AT ANY LOCATION. IN THE CENTRAL BUSINESS DISTRICT, GRANITE OR OTHER SPECIALTY PAVING MATERIALS MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.

2. THE DETECTABLE WARNING MUST BE INSTALLED A MAXIMUM OF 8" OR LESS FROM FACE OF CURB (SEE DETAIL SHEET B-4-2).

3. THE DETECTABLE WARNING MUST COVER FULL WIDTH OF RAMP EXCLUDING SIDE FLARES FOR A MINIMUM UNOBSTRUCTED DEPTH OF 24". THE DETECTABLE WARNING LOCATED ON THE SURFACES OF RAMPS IS TYPICALLY ORIENTED PERPENDICULAR TO THE RUN OF THE RAMP UNLESS SPECIAL CIRCUMSTANCES OCCUR (SEE DETAIL SHEET B-1-5). THE DETECTABLE WARNING MUST BE PROVIDED FOR A MINIMUM DEPTH OF 24" FOR THE ENTIRE LENGTH OF THE SIDEWALK WHERE THE SIDEWALK IS FLUSH WITH THE STREET (DEPRESSED CURB OR FLUSH TRANSITION). IF IT IS NECESSARY TO CUT A UNIT(S) IN THE PROVISION OF A COMPLIANT RAMP OR SIDEWALK WITH 24" MINIMUM DEPTH OF DETECTABLE WARNING, THE UNITS SHALL BE CUT IN A NEAT AND WORKMAN LIKE MANNER PER MANUFACTURER'S REQUIREMENTS WITH A MINIMUM OF THREE PINS OR ANCHOR POINTS (WHERE APPLICABLE). THE UNITS SHALL BE ARRANGED SO THAT THE CUT UNITS ARE LARGE ENOUGH TO BE PROPERLY AND ADEQUATELY SECURED. CUT UNITS SHALL NOT BE USED UNLESS ALL OTHER DESIGN OPTIONS HAVE BEEN EXHAUSTED. THE USE OF SALVAGE PIECES FROM UNITS THAT ARE CUT WILL NOT BE PERMITTED WITHOUT WRITTEN APPROVAL OF THE COMMISSIONER. CUT UNIT SALVAGE PIECES NOT APPROVED FOR USE MUST BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY.

4. WHERE APPLICABLE, A COMBINATION OF STRAIGHT AND RADIAL DETECTABLE WARNING UNITS MAY BE USED ON COMPOUND AND LARGE RADII. CONTRACTOR MUST MAKE THIS DETERMINATION AND VERIFY IN FIELD.

5. THE DETECTABLE WARNING MUST CONTRAST WITH ADJACENT PAVEMENT. IF LIGHT COLORED PAVEMENT IS USED THE DETECTABLE WARNING COLOR SHALL BE RED. IF A DARK COLORED PAVEMENT IS USED THE DETECTABLE WARNING COLOR SHALL BE YELLOW. CONTRACTOR TO VERIFY THAT PROPER CONTRAST IS OBTAINED.

6. PRIOR TO PLACING CONCRETE FOR DEPRESSED CURBS, RAMPS, OR SIDEWALKS THE CONTRACTOR SHALL VERIFY THAT LAYOUT OR DESIGN COMPLIES WITH THE REQUIREMENTS OF THE CDOT ADA STANDARDS.



7. RAMP WIDTH MUST BE A MINIMUM OF 6'-0" AND IN INCREMENTS OF 1'-0", EXCEPT WHEN USING THE PERPENDICULAR RAMP AT CORNER (OR OTHER SPECIAL CDOT APPROVED CONDITIONS), WHICH HAS A MINIMUM WIDTH OF 4'-0".

8. THE MAXIMUM ALLOWABLE RAMP RUNNING SLOPE IS 1:14, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED. GRADE BREAKS AT THE TOP AND BOTTOM OF RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF RAMP RUN.

9. THE MAXIMUM ALLOWABLE RAMP CROSS SLOPE IS 1:64, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED.

10. THE MAXIMUM ALLOWABLE RAMP LANDING SLOPE IS 1:64, MEASURED AT ANY LOCATION AND IN ANY DIRECTION ON THE LANDING. THE RAMP LANDING WIDTH SHALL MATCH THE FULL WIDTH OF THE RAMP FOR A MINIMUM UNOBSTRUCTED DEPTH OF 4'-0". RAMP LANDINGS SHALL BE PROVIDED AT THE TOP AND/OR BOTTOM OF RAMPS WHERE TURNING IS REQUIRED.

11. RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK. RAMP SIDE FLARES ARE NOT REQUIRED WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS LANDSCAPED OR IS OCCUPIED BY A BARRIER THAT BLOCKS PEDESTRIAN ACCESS. EXCEPTIONS TO THIS RULE MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.

 <div>City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering www.cityofchicago.org</div> 	DATE	REVISION	<div>CITY OF CHICAGO GENERAL NOTES SHEET B-3-2</div> <div>SCALE: NOT TO SCALE DATE: 10/23/2006</div> <div>DRAWN BY: CDOT CHECKED BY: LCM</div>
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	11/02/09	REVISION 4	
	08/10/12	REVISION 5	

GENERAL NOTES (CONTINUED):

12. UTILITIES, SUCH AS LIGHT POLES, TRAFFIC POLES AND HYDRANTS, MAY BE LOCATED IN THE FLARE OF THE RAMP BUT ARE NOT ALLOWED ON THE RAMP SURFACE OR LANDING AREAS. EXISTING UTILITY STRUCTURE LIDS MAY REMAIN WITHIN THE FLARE OR ON THE SURFACE OF THE RAMP IF THE REQUIREMENTS OF GENERAL NOTE #19 ARE MET.

13. ALL LOCATIONS WITH TYPE 4 OR TYPE B CURB (EXCEPT ALLEY APRONS) SHALL BE CONSTRUCTED AS CURB AND GUTTER TYPE BV.12 THROUGH THE LIMITS OF THE CORNER AND THE CURB RAMPS.

14. ALTERATIONS SHALL NOT DECREASE THE ACCESSIBILITY TO EXISTING FACILITIES, SIDEWALKS LEADING TO EXISTING FACILITIES, OR DOOR OR GATE ACCESS POINTS TO FACILITIES. THE ELEVATION AT THE EXISTING PROPERTY LINE OR FACILITY ACCESS POINT SHALL BE MAINTAINED AT A MINIMUM. ANY ALTERATIONS ADJACENT TO OR AFFECTING A FACILITY ACCESS POINT SHALL RESULT IN IMPROVED ACCESS OR AT A MINIMUM A REPLICATION OF EXISTING CONDITIONS, INCLUDING SIDEWALK SLOPES AND SURFACE CONDITIONS. FACILITIES INCLUDE, BUT ARE NOT LIMITED TO PRIVATE BUSINESSES, PUBLIC BUILDINGS, RESIDENCES, BUS STOPS, PUBLIC BENCHES, PAY PHONES, AND PARKING METERS.

15. THE MINIMUM CROSSWALK WIDTH IS 6'-0". CROSSWALKS SHALL BE LOCATED AS SHOWN IN THE PLAN SHEETS DEPENDING ON THE TYPE OF CURB RAMP USED. BEYOND THE CURB FACE AT THE BASE OF CURB RAMPS, A CLEAR SPACE OF 4'-0" BY 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE STRIPES OF THE CROSSWALK (WHERE PROVIDED).

16. IF SIDEWALK AND ALLEY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED. IF SIDEWALK AND DRIVEWAY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED BUT DETECTABLE WARNING UNITS ARE STILL REQUIRED IF THE DRIVEWAY HAS TRAFFIC CONTROL DEVICES (I.E. TRAFFIC SIGNALS).

17. MAIN LINE SIDEWALK SHALL HAVE A MAXIMUM CROSS SLOPE NOT TO EXCEED 1:64 FOR THE FULL WIDTH OF WALK UNLESS OTHERWISE APPROVED BY THE COMMISSIONER. WHERE TURNING IS REQUIRED AND WHERE SIDEWALKS INTERSECT, THE SLOPE OF THE SIDEWALK SHALL NOT EXCEED 1:64 IN ANY DIRECTION.

18. MAIN LINE SIDEWALK RUNNING SLOPES SHALL NOT EXCEED 1:24 OR THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT STREET, WHICH EVER IS HIGHER.

19. THERE SHALL BE NO VERTICAL LEVEL DIFFERENCES BETWEEN SURFACES GREATER THAN 1/4" ON THE MAIN LINE SIDEWALK. THERE SHALL BE NO HORIZONTAL GAPS OR OPENINGS GREATER THAN 1/2" ON THE MAIN LINE SIDEWALK.



20. WHERE OBSTRUCTIONS EXIST ON THE MAINLINE SIDEWALK, THE CLEAR WIDTH OF USEABLE SIDEWALK SHALL NOT BE LESS THAN 4'-0". OBSTRUCTIONS INCLUDE, BUT ARE NOT LIMITED TO SIDEWALK BENCHES, FIRE HYDRANTS, SIGNAL OR LIGHT POLES, NEWSPAPER DISPENSERS, TRASH RECEPTACLES, AND UTILITY PEDESTALS.

21. CURB RAMPS AND LANDING (KEYSTONE) TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIAL STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE TO BE USED.

22. DEPRESSED CURB, RAMP, OR SIDEWALK DESIGNS OR LAYOUTS SHALL MAINTAIN OR IMPROVE EXISTING DRAINAGE AND THE EXISTING INTERSECTION GEOMETRY SHALL NOT BE MODIFIED WITHOUT CDOT APPROVAL.

23. ALL CONSTRUCTION DOCUMENTS MUST BE STAMPED BY A LICENSED ARCHITECT/LANDSCAPE ARCHITECT/ ENGINEER TO CERTIFY THAT THEY ARE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND ALL CODES AND BUILDING ORDINANCES OF THE CITY OF CHICAGO AND THE STATE OF ILLINOIS.

24. NO DEVIATIONS FROM THESE STANDARDS ARE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.

 <div>City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering www.cityofchicago.org</div> 	DATE	REVISION	<div>CITY OF CHICAGO GENERAL NOTES (CONTINUED) SHEET B-3-3</div> <div>SCALE: NOT TO SCALE DATE: 10/23/2006</div> <div>DRAWN BY: CDOT CHECKED BY: GK</div>
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	11/02/09	REVISION 4	
	08/10/12	REVISION 5	
	01/01/14	REVISION 6	

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		DRAWN - KDC	REVISED -					290	FAI 290 22 KEELER BR	COOK	283	236
		CHECKED - DJK	REVISED -		CONTRACT NO. 62U41							
	PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -		SCALE: N.T.S.		SHEET 14 OF 17 SHEETS	ILLINOIS FED. AID PROJECT				

MODEL: CDOT_10 [Sheet]
FILE NAME: c:\msb\civiltech\pkw\kyle.dary\0108547\0102141-sht-details.dgn

ADA COMPLIANCE AND TRANSITION GUIDELINES

POLICY STATEMENT: ANY ALTERATION OF THE PUBLIC WAY MUST BE RESTORED IN AN ADA COMPLIANT MANNER

I. STREET/ALLEY RESTORATION

FOR ANY PROJECT WHERE, WITHIN THE PROJECT LIMITS, A CROSSWALK IS ENCOUNTERED OR WHERE THE PROJECT LIMITS TERMINATE WITHIN 4' OR LESS OF A CROSSWALK, THOSE CROSSWALKS AND THE ASSOCIATED CURB RAMPS MUST BE IMPROVED TO CURRENT ADA STANDARDS IF THEY ARE NOT COMPLIANT (SEE APPENDIX A.)

WHEN A PROJECT CALLS FOR ONLY AN INTERSECTION TO BE REPAVED, THE INTERSECTION LIMITS AS DEFINED BY THE AREA OUTLINED BY OUTERMOST CROSSWALK LINES AND ADJACENT CURB FACES AND ALL ADJOINING CROSSWALKS AND CURB RAMPS MUST BE IMPROVED TO CURRENT ADA STANDARDS IF THEY ARE NOT COMPLIANT (SEE APPENDIX A).

WHEN WORK IS LIMITED TO A SINGLE CORNER OF AN INTERSECTION, THE CURB RAMP MUST BE IMPROVED TO CURRENT ADA STANDARDS AND THE ADJACENT PAVEMENT MUST BE RESURFACED, AS NECESSARY TO PROVIDE FOR A FLUSH TRANSITION (SEE APPENDIX A).

WHEN ADA WORK IS LIMITED TO A SINGLE CORNER OF AN INTERSECTION, THE ADJACENT PAVEMENT MUST BE RESTORED (SEE APPENDIX A).

FOR ANY CONSTRUCTION WHERE, WITHIN THE PROJECT LIMITS, AN ALLEY APRON IS ENCOUNTERED, THE ASSOCIATED CURB RAMPS, ALLEY APRON, AND SIDEWALKS MUST BE IMPROVED TO CURRENT ADA STANDARDS IF THEY ARE NOT COMPLIANT (SEE APPENDIX A).

II. SIDEWALK INSTALLATION / REPAIRS / RECONSTRUCTION

THE LIMITS OF ANY MAINLINE SIDEWALK REPLACEMENT, GREATER THAN TEN FEET (10') IN LENGTH, THAT ABUT AN EXISTING RAMP, KEYSTONE, TRANSITION PANEL, AND/OR LANDING AREA (THIS TOTAL LENGTH INCLUDES THE PRIOR ELEMENTS), SHALL BE EXTENDED TO INCLUDE THE AFFECTED RAMPS AND THESE RAMPS SHALL BE RECONSTRUCTED TO CURRENT ADA STANDARDS. IN ADDITION, ALL NEWLY PLACED SIDEWALK TEN FEET (10') OR MORE IN LENGTH SHALL BE CONSTRUCTED IN ACCORDANCE WITH ALL CURRENT APPLICABLE STANDARDS WHICH INCLUDE PROVIDING A MINIMUM FOUR FEET (4') WIDTH ACCESSIBLE PATHWAY WITH A CROSS SLOPE NOT TO EXCEED 1:64 (SEE APPENDIX A).

III. GUIDELINES FOR TRANSITIONING TO EXISTING NON-COMPLIANT CONDITION

NEW SIDEWALK PLACEMENTS GREATER THAN TEN FEET IN CONTIGUOUS LENGTH:

THE LIMITS OF ANY MAINLINE SIDEWALK REPLACEMENT, GREATER THAN TEN FEET (10') IN LENGTH, MUST BE EXTENDED FOR A MINIMUM FIVE ADDITIONAL FEET (5') EITHER SIDE IN ORDER TO PROVIDE A TRANSITION TO MATCH THE EXISTING SIDEWALK. THE LENGTH OF TRANSITION SHALL BE LENGTHENED AS NECESSARY TO ENSURE THAT THE RUNNING SLOPE OF THE TRANSITION DOES NOT EXCEED A SLOPE OF 1:24 (PREFERRED) OR 1:14 (MAXIMUM) AT ANY POINT.


NEW SIDEWALK REPLACEMENTS TEN FEET OR LESS IN CONTIGUOUS LENGTH (REPAIRS):

IT IS ACCEPTABLE PRACTICE TO MATCH ADJACENT SIDEWALKS AT THE EXISTING SLOPE.


CURB RAMP REPLACEMENTS

WHEN REPLACING AN ADA RAMP, THE SIDEWALK REPLACEMENT MUST EXTEND BEYOND THE LIMITS OF THE LANDING AREA AND/OR THE "KEYSTONE" A MINIMUM OF AN ADDITIONAL FIVE FEET (5') ON EITHER SIDE IN ORDER TO PROVIDE A TRANSITION TO MATCH THE EXISTING SIDEWALK. THE TRANSITION PANEL SHALL BE LENGTHENED AS NECESSARY TO ENSURE THAT THE RUNNING SLOPE OF THE TRANSITION PANEL DOES NOT EXCEED A SLOPE OF 1:24 (PREFERRED) OR 1:14 (MAXIMUM) AT ANY POINT.

NO EXCEPTIONS TO THE ABOVE WILL BE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.



City of Chicago
Rahm Emanuel, Mayor
Department of Transportation
Division of Engineering
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CHICAGO DEPARTMENT
OF TRANSPORTATION

DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5
01/01/14	REVISION 6

CITY OF CHICAGO
ADA COMPLIANCE AND TRANSITION GUIDELINES
SHEET B-3-4

SCALE: NOT TO SCALE
DATE: 10/23/2006


DRAWN BY: CDOT
CHECKED BY: GK

STRAIGHT DETECTABLE WARNING UNITS


RADIAL DETECTABLE WARNING UNITS

DETECTABLE WARNING UNIT SIZES

- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.
- APPROVED LIST OF DETECTABLE WARNING PRODUCTS CAN BE FOUND ON CDOT'S WEBSITE (www.cityofchicago.org).



City of Chicago
Rahm Emanuel, Mayor
Department of Transportation
Division of Engineering
www.cityofchicago.org



CHICAGO DEPARTMENT
OF TRANSPORTATION

DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5

CITY OF CHICAGO
DETECTABLE WARNING UNIT SIZES
SHEET B-4-1

SCALE: NOT TO SCALE
DATE: 10/23/2006

DRAWN BY: CDOT
CHECKED BY: LCM

USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

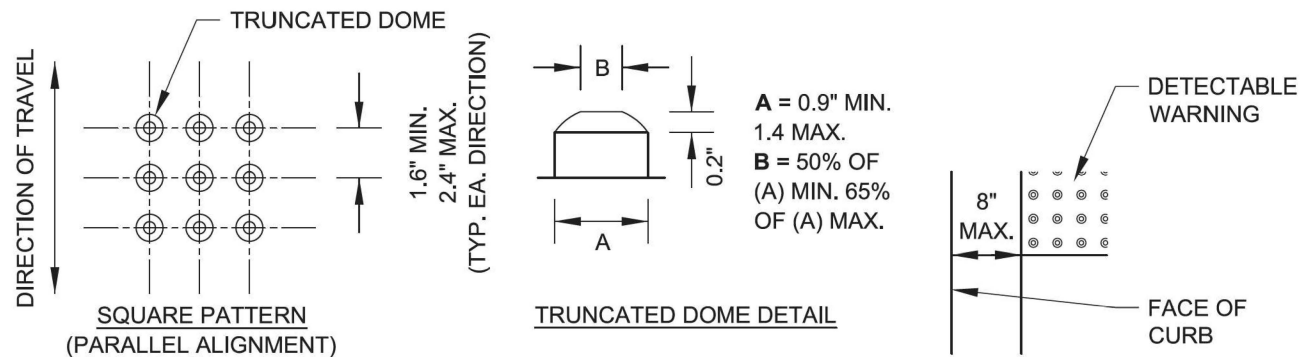
I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS

SCALE: N.T.S. SHEET 15 OF 17 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	237
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

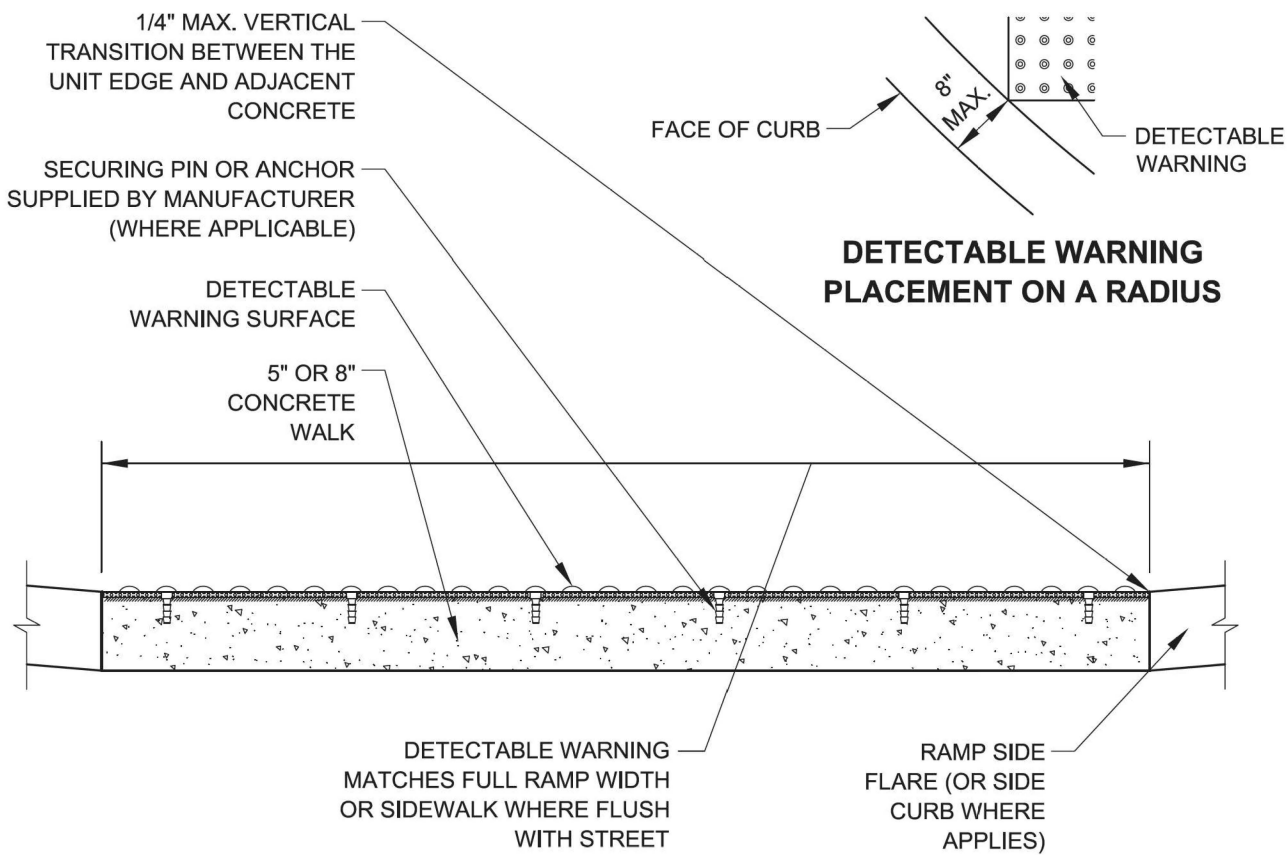
GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



UNIT PATTERN & DOME DETAIL

TYPICAL DETECTABLE WARNING PLACEMENT



DETECTABLE WARNING UNIT SECTION

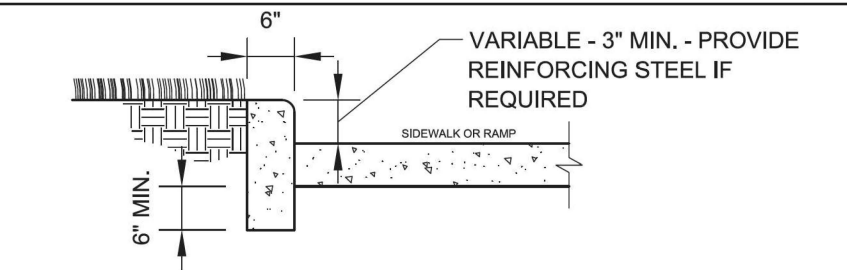


DATE	REVISION
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11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5

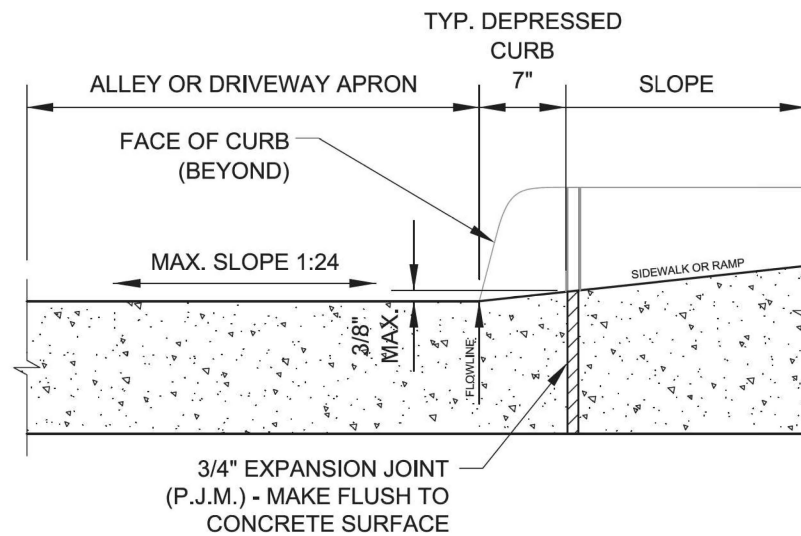
**CITY OF CHICAGO
DETECTABLE WARNING UNIT DETAILS
SHEET B-4-2**

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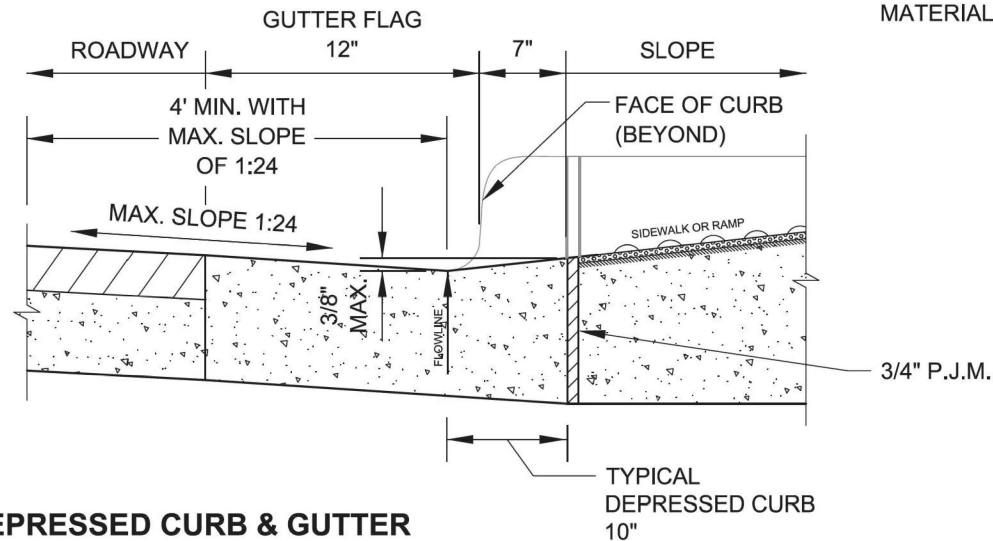
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CHECKED BY: LCM



SIDE CURB - SECTION



**DEPRESSED CURB & GUTTER
AT ALLEY/DRIVEWAY APRON (TYPE 4 OR B CURB)**



**DEPRESSED CURB & GUTTER
AT BOTTOM OF TYPICAL CURB RAMP**



DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5

**CITY OF CHICAGO
CURB & GUTTER DETAILS
SHEET B-4-3**

SCALE: NOT TO SCALE
DATE: 10/23/2006

DRAWN BY: CDOT
CHECKED BY: LCM

**NOTES FOR CURB & GUTTER
DETAILS THIS SHEET:**

A. CROSS SLOPE AT DEPRESSED CURB & GUTTER NOT TO EXCEED 1:64.

B. DETECTABLE WARNING SURFACE AT DRIVEWAYS REQUIRED ONLY FOR COMMERCIAL DRIVEWAYS WITH TRAFFIC CONTROL DEVICES, I.E. SIGNALS.

C. REFER TO *REGULATIONS FOR OPENINGS, CONSTRUCTION AND REPAIR IN THE PUBLIC WAY (CDOT)* FOR ADDITIONAL REQUIREMENTS FOR CURB AND GUTTER INSTALLATION.

D. RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR SIDE CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK.

E. 'P.J.M.' THIS SHEET REFERS TO PREFORMED JOINT MATERIAL.

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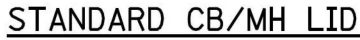
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PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-290 AT KEELER AVENUE
CDOT STANDARD DETAILS**

SCALE: N.T.S. SHEET 16 OF 17 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	238
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



DRAWN: <u>SRW</u> DESIGNED: _____ CHECKED: _____ GD, GC, SO REVIEWED: _____	<div style="font-size: 2em; font-weight: bold;">A.21</div>
_____ OF _____	
PN _____	

Left Diagram: Standard Manhole Construction

- Labels:** CHICAGO FRAME & LID, PRECAST CONCRETE ADJUSTMENT RING, SWIRL CHAMBER, VORTEX or ORIFICE RESTRICTOR (SEE MISC DETAILS SHEET A.26), PRECAST REINF. CONC. OFFSET CONE, TYPE B CURB & GUTTER Per CDOT Details, 1/2" ROUND TIE BARS 30" LONG, 30" C/C, 2" MIN 8" MAX 2 RING MAXIMUM, 8" VIT. CLAY, 1' VERTICAL HALF TRAP, SUMP 3'-4", 1'-0", 4'-0" DIA., 4'-6", 7'-4", 5", 2'-10", 4" MAXIMUM (TYP), Note: Positive Summit for Drainage, *5 DOWEL BAR @ CONSTRUCTION JOINT.
- Dimensions:** 8" VIT. CLAY, 1' VERTICAL HALF TRAP; SUMP 3'-4"; 1'-0"; 4'-0" DIA.; 4'-6"; 7'-4"; 5"; 2'-10"; 4" MAXIMUM (TYP); 2'-0" DIA.; 4"; 2'-10"; 4" MAXIMUM (TYP).

Right Diagram: Manhole with Type B Curb and Gutter

- Labels:** CHICAGO FRAME & LID, PRECAST CONCRETE ADJUSTMENT RING, PRECAST REINF. CONC. OFFSET CONE, VORTEX or ORIFICE RESTRICTOR (SEE MISC DETAILS SHEET A.26), TYPE B-V.12 CURB AND GUTTER Per CDOT Details, 1'-4", 7", 2'-0", 4'-0", 6'-0", 4'-0", 3'-0" DIA., 2'-0" DIA., 1'-0", 4", 4" MAXIMUM (TYP), Note: Positive Summit for Drainage, 2-#7 DEFORMED BARS, 2" MIN 8" MAX 2 RING MAXIMUM, 8" VIT. CLAY, 1' VERTICAL HALF TRAP, SUMP 3'-10", 1'-0", 4'-0" DIA., 4'-0", 6'-0", 4'-0", 3'-0" DIA., 2'-0" DIA., 1'-0", 4", 4" MAXIMUM (TYP).
- Dimensions:** 8" VIT. CLAY, 1' VERTICAL HALF TRAP; SUMP 3'-10"; 1'-0"; 4'-0" DIA.; 4'-0"; 6'-0"; 4'-0"; 3'-0" DIA.; 2'-0" DIA.; 1'-0"; 4"; 4" MAXIMUM (TYP); 2'-0" DIA.; 1'-0"; 4"; 4" MAXIMUM (TYP); 2'-0" DIA.; 1'-0"; 4"; 4" MAXIMUM (TYP).

Common Labels: PRECAST REINFORCED BASE AND RISER WITH MINIMUM 6" EMBEDMENT.

STANDARD CATCH BASIN-3' DIA.

CHICAGO
FRAME & LID

2" MIN
8" MAX
2 RING
MAXIMUM

2'-0" DIA.

2'-10"

8" VIT. CLAY
HALF TRAP

B

ELEVATION A

ORIFICE
RESTRICTOR
(SEE MISC
DETAILS
SHEET A.26)

4'-0" DIA.

5"

4'-6"

5"

MIN. SUMP
3'-4"
W/DIRECT
SURFACE
RUNOFF

2'-0" W/O
DIRECT
SURFACE
RUNOFF

5"

PRECAST REINFORCED BASE AND
RISER WITH MINIMUM 6"
EMBEDMENT

INFLOW PIPE INVERT

NOTE: THE INVERT ON INFLOW PIPE
OR UNDERDRAIN MUST BE AT
OR ABOVE THE HALF-TRAP
ELEVATION-A

N.T.S.

1. CATCH BASIN TO CATCH BASIN CONNECTIONS ARE ALLOWED IN PRIVATE SITES & ALLEYS. ONLY THE DOWNSTREAM CATCH BASIN IS REQUIRED TO HAVE A HALF-TRAP.
2. IF $B < 4$ FEET, THEN USE A FLAT TOP SLAB CATCH BASIN AS NECESSARY.
3. FOR TRENCH BACKFILL, REFER TO IDOT SSRBC, ARTICLE 1003.04.
4. FOR GRANULAR EMBEDMENT, USE CA-11, CRUSHED GRAVEL, CRUSHED STONE, OR CRUSHED CONCRETE.
5. FOR STABILIZATION STONE, 12" OF CA-1 STONE IS ONLY REQUIRED WHEN UNSTABLE MATERIAL IS ENCOUNTERED AT TRENCH BOTTOM.
6. INLETS AND 3' DIAMETER CATCH BASINS ARE TO BE USED ONLY WITH PRIOR APPROVAL OF THE DEPT. OF BUILDINGS STORMWATER REVIEWER.
7. IF COVER OVER VCP IS LESS THAN 3', USE 45 DEGREE DIP BENDS WITH 1 FOOT DROP AS HALF TRAPS AS NEEDED FOR IEPA CLEARANCE REQUIREMENTS



DRAWN: <u>SW</u> DESIGNED: _____ CHECKED: _____ REVIEWED: _____	A.22
_____ OF _____	
PN _____	

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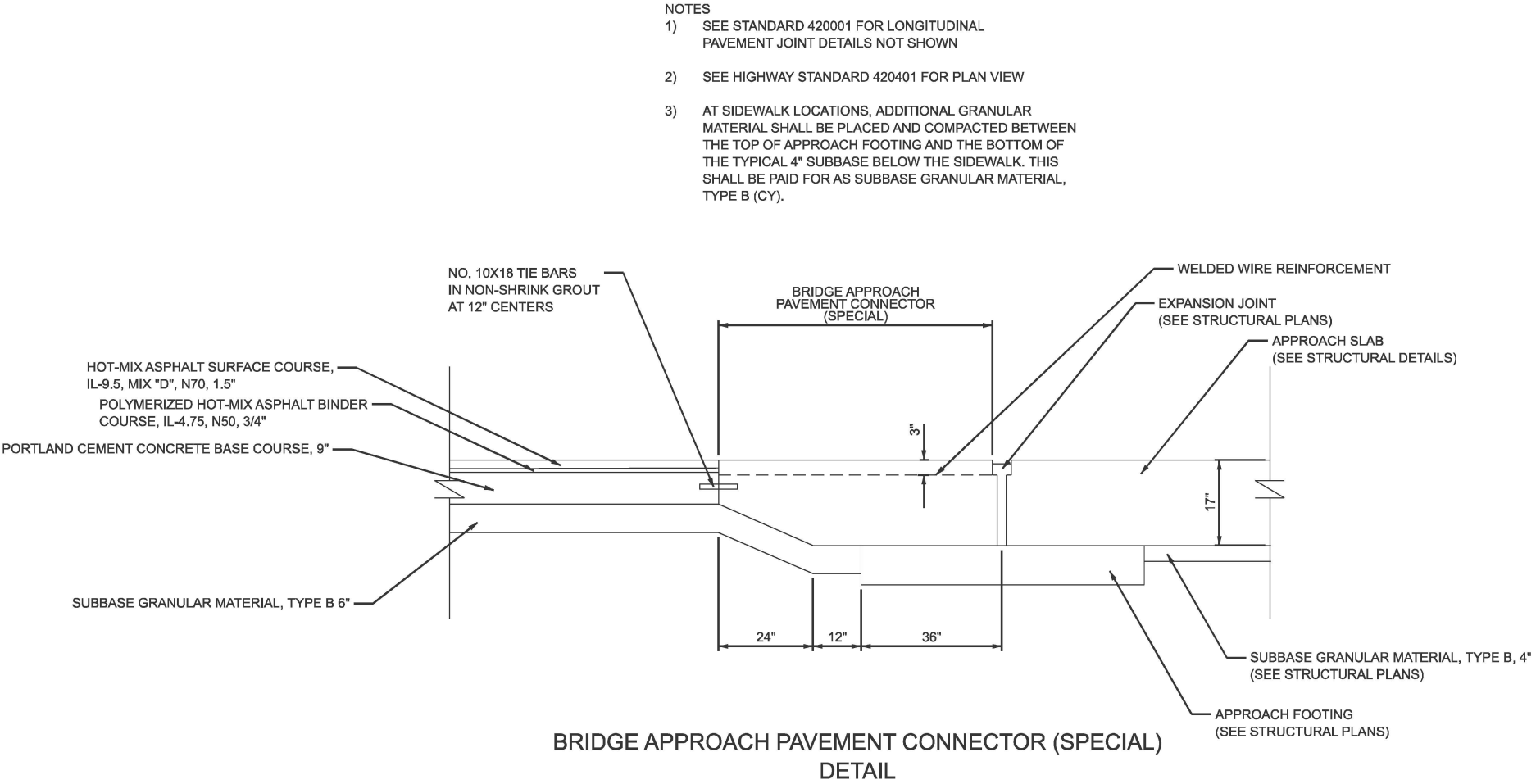
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PLOT DATE	= 8/27/2025	DATE	- 8/29/2025	REVISED	-

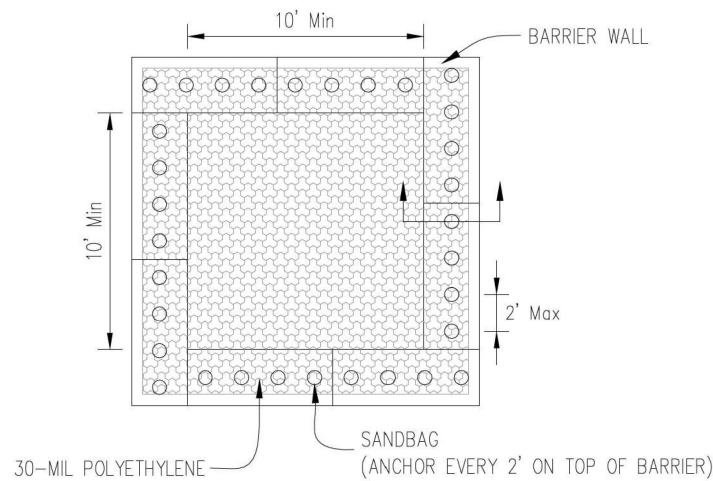
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CONSTRUCTION DETAILS

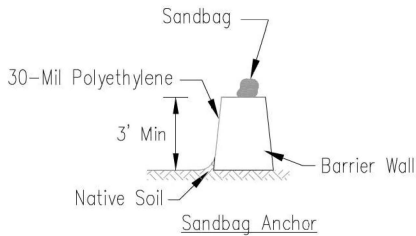
SHEET 1 OF 4 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	241
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

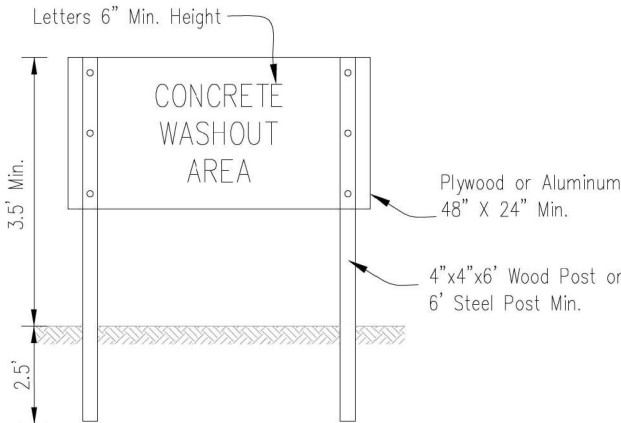




PLAN VIEW



BARRIER WALL ANCHOR SECTION

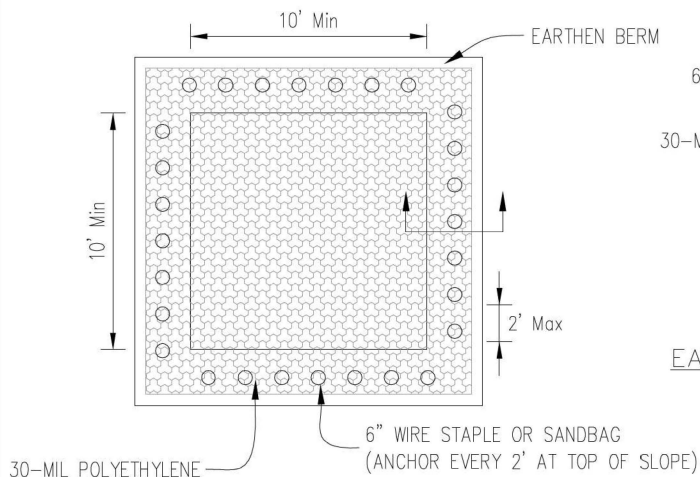


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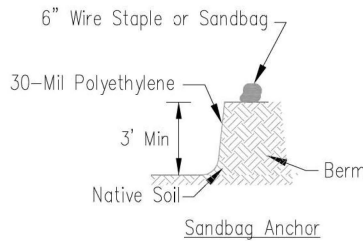
NOTES:

1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardend concrete and/or slurry and returning the facilities to a functional condition.
2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.

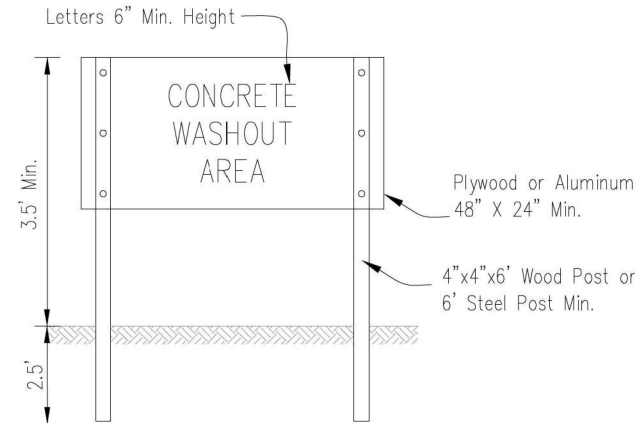
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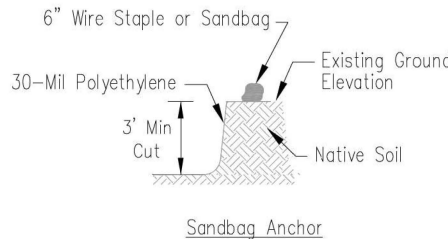
PLAN VIEW



EARTHEN BERM ANCHOR SECTIONS



SIGN DETAIL

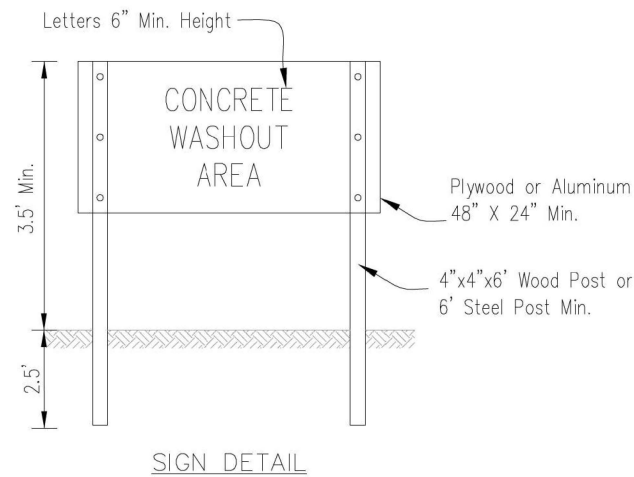
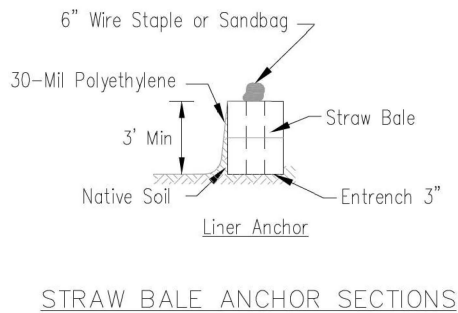
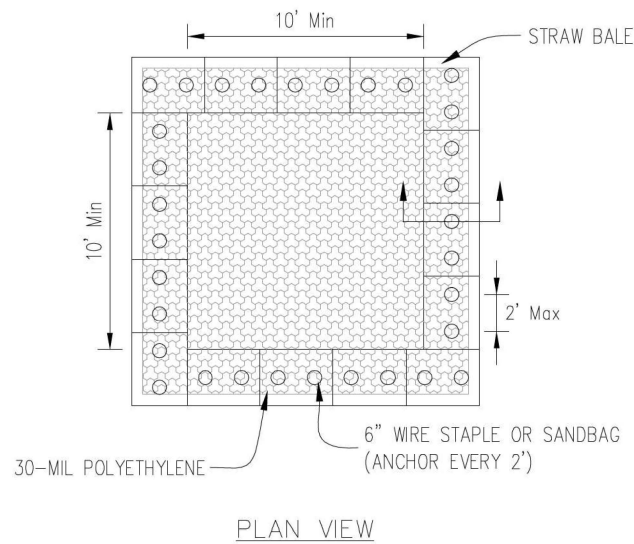


SUBGRADE ANCHOR SECTIONS

NOTES:

1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardend concrete and/or slurry and returning the facilities to a functional condition.
2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.

Sheet 2 of 3	File No. JWM-6548T Drawing No.	DESIGNED - KDC DRAWN - KDC CHECKED - DJK DATE - 8/13/2025	TEMPORARY CONCRETE WASHOUT FACILITY – EARTHEN TYPE	Designed _____ Drawn B. JOHNSON 6/08 Checked _____ Approved _____	Date _____
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- NOTES:
1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardend concrete and/or slurry and returning the facilities to a functional condition.
 2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
 3. Each straw bale is to be staked in place using (2) 2"x2"x4' wooden stakes.

Sheet 1 of 3	File No.	TEMPORARY CONCRETE WASHOUT FACILITY – STRAW BALE	Date
	Drawn		Designed
	Checked		Drawn B. JOHNSON 6/08
	Approved		Checked
			Approved

MODEL: Washout 2 (Sheet)
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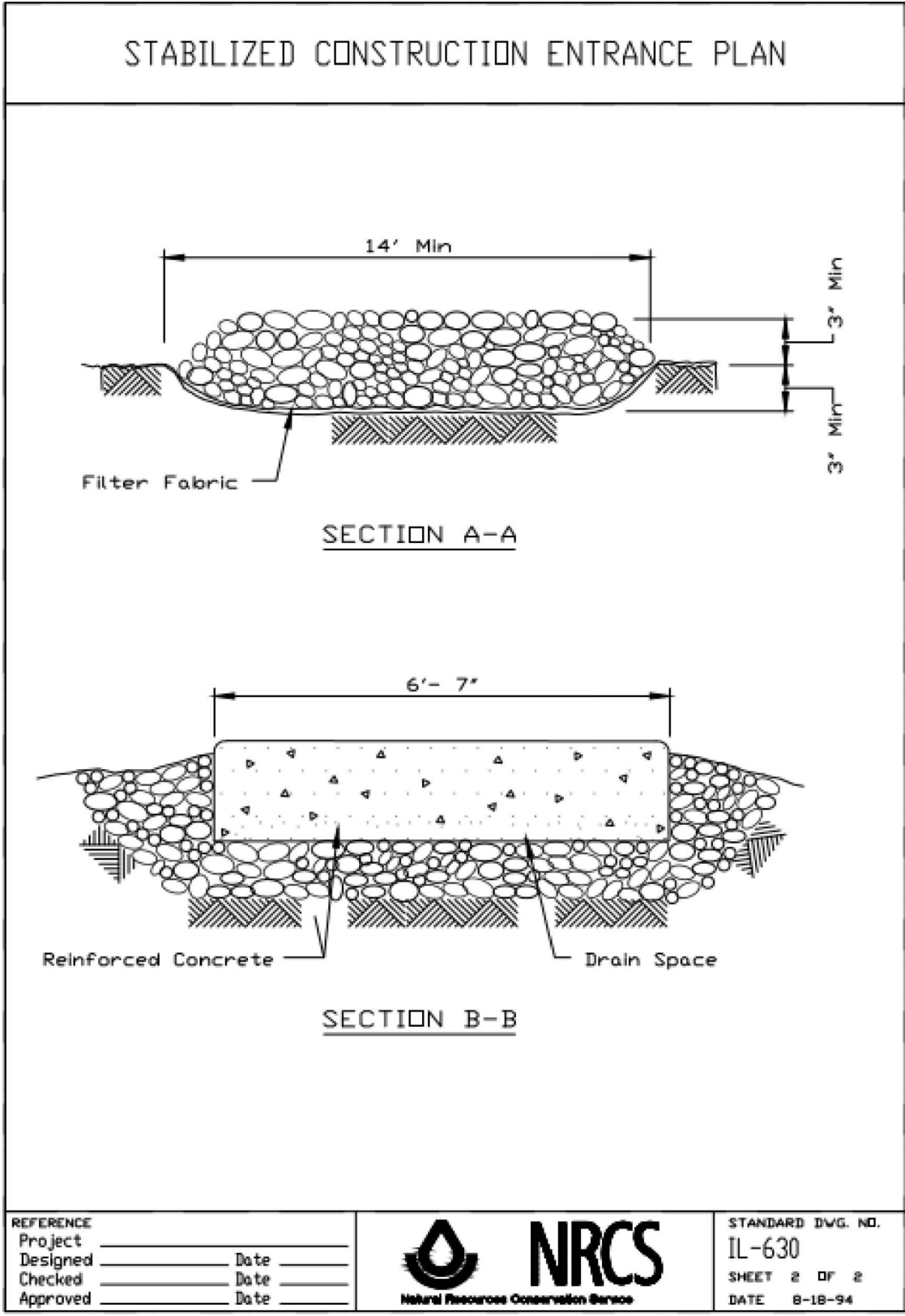
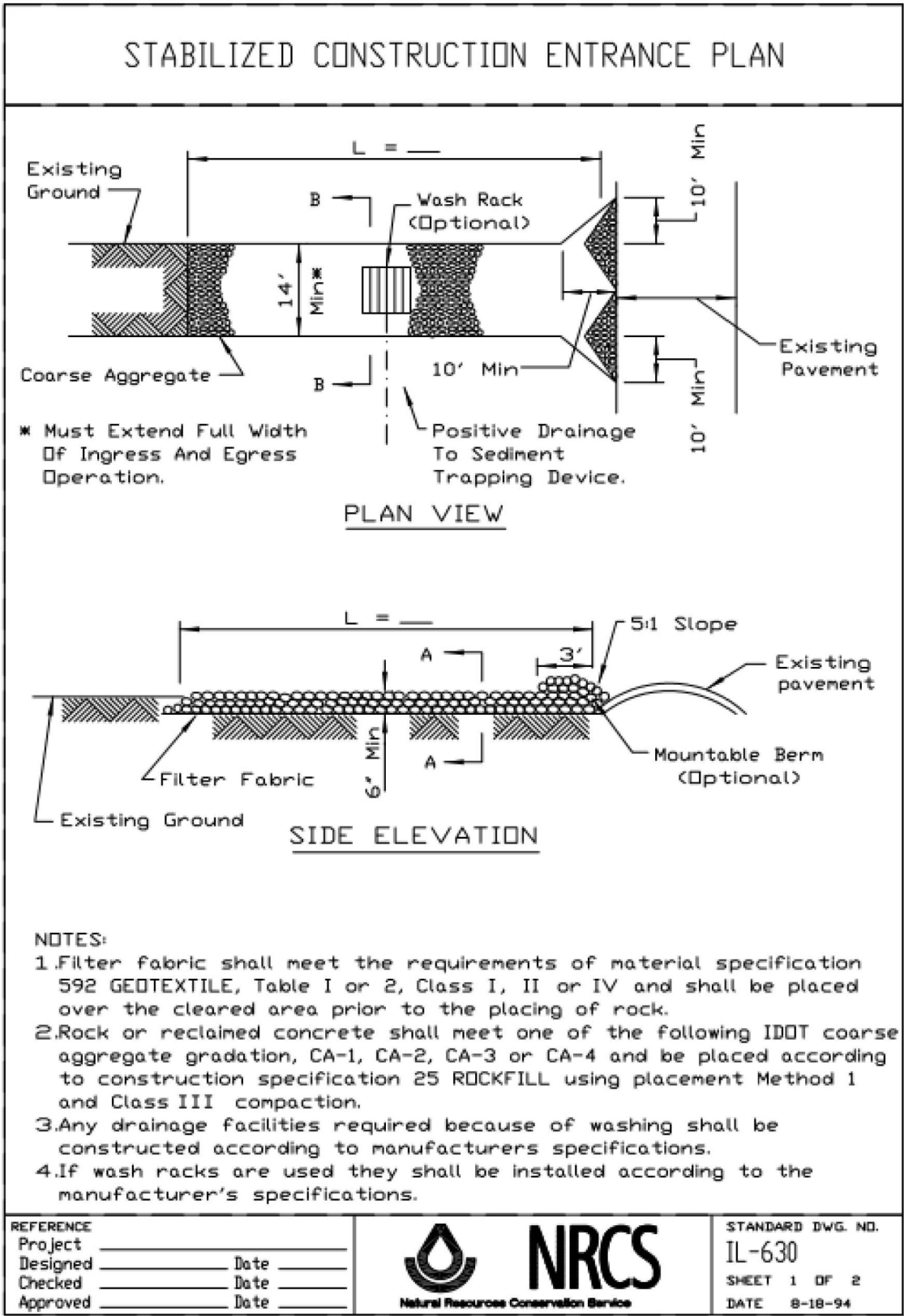
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CONSTRUCTION DETAILS

SHEET 3 OF 4 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	243
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**I-290 AT KEELER AVENUE
CONSTRUCTION DETAILS**

SHEET 4 OF 4 SHEETS

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	244
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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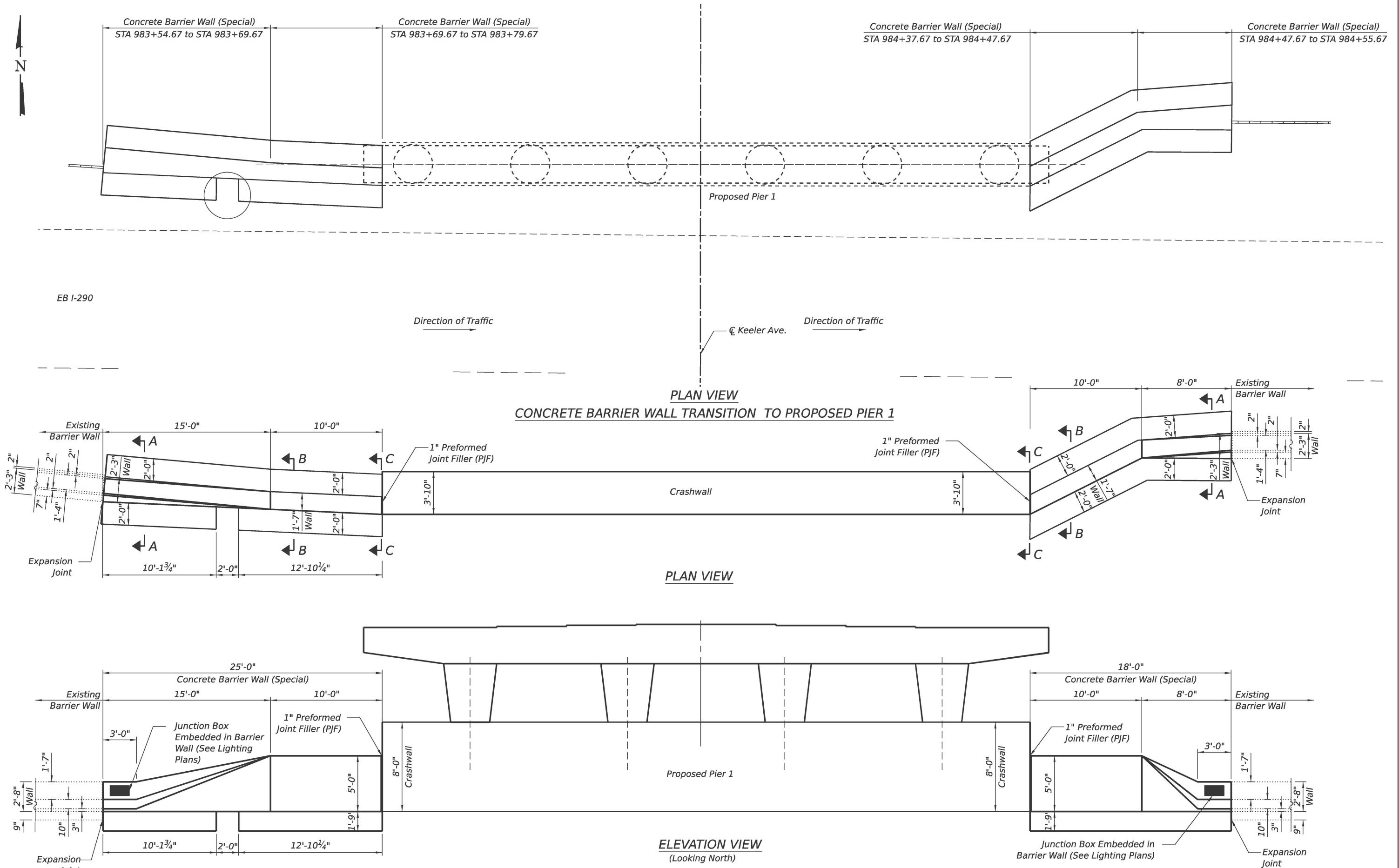
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 1- CONCRETE BARRIER WALL TRANSITION PLAN & ELEVATION
STRUCTURE NO. 016-2093

SHEET 1 OF 3 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



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PLOT DATE = 8/26/2025	CHECKED - GJH	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2 - CONCRETE BARRIER WALL TRANSITION PLAN & ELEVATION
STRUCTURE NO. 016-2093

SHEET 2 OF 3 SHEETS

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	246
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

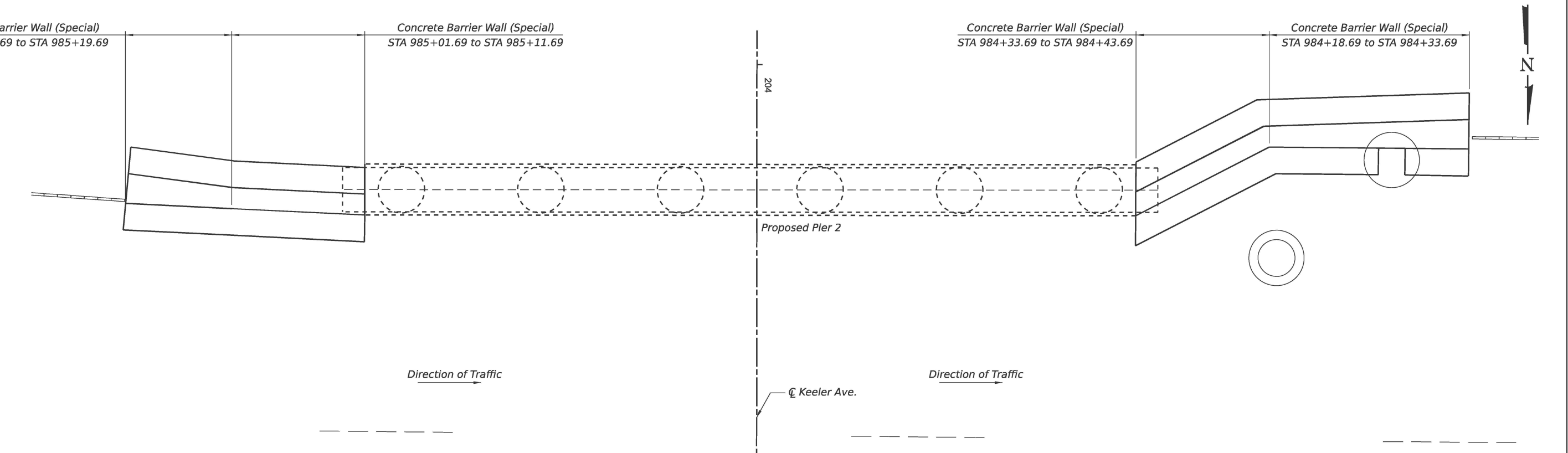
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Concrete Barrier Wall (Special)
STA 985+01.69 to STA 985+11.69

Concrete Barrier Wall (Special)
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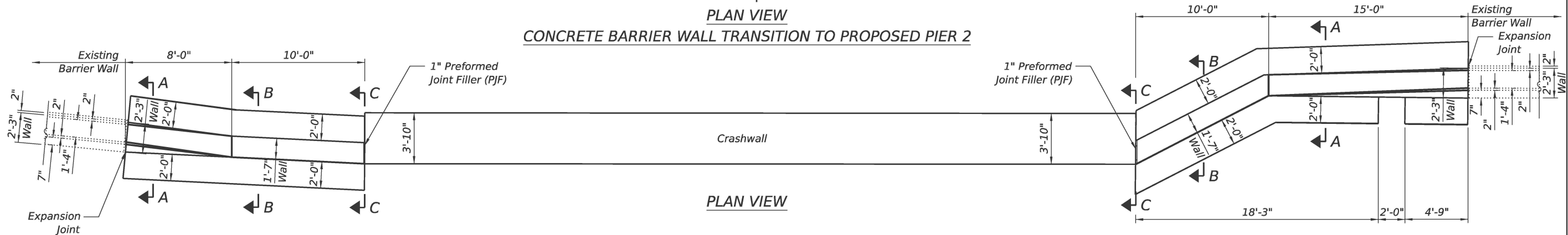
Concrete Barrier Wall (Special)
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WB I-290

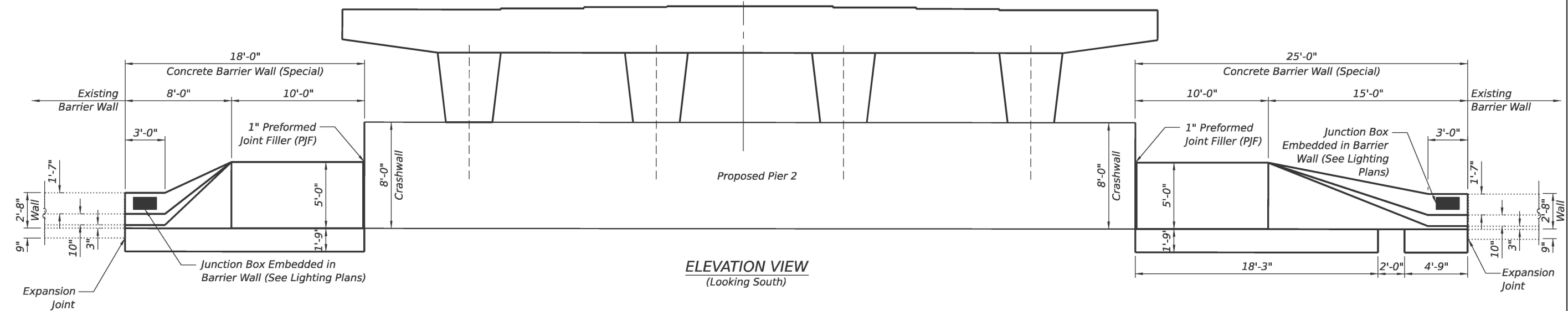


PLAN VIEW

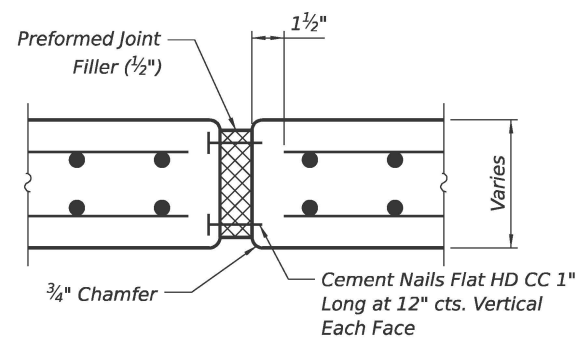
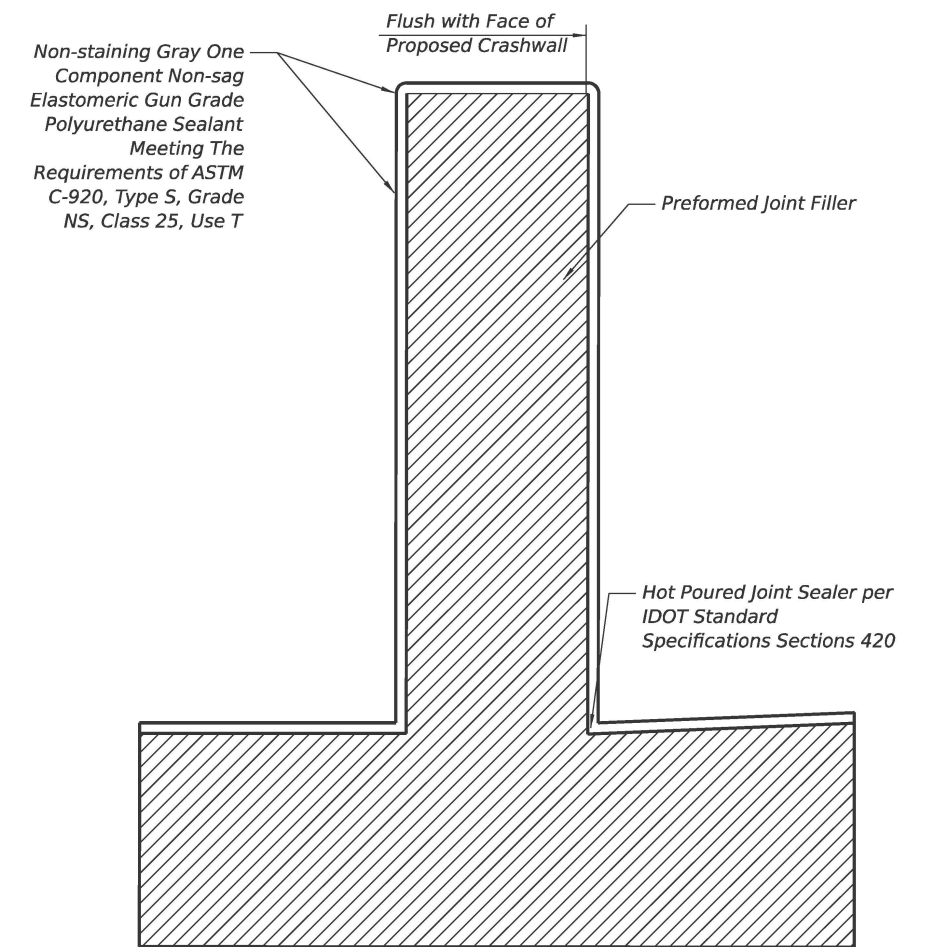
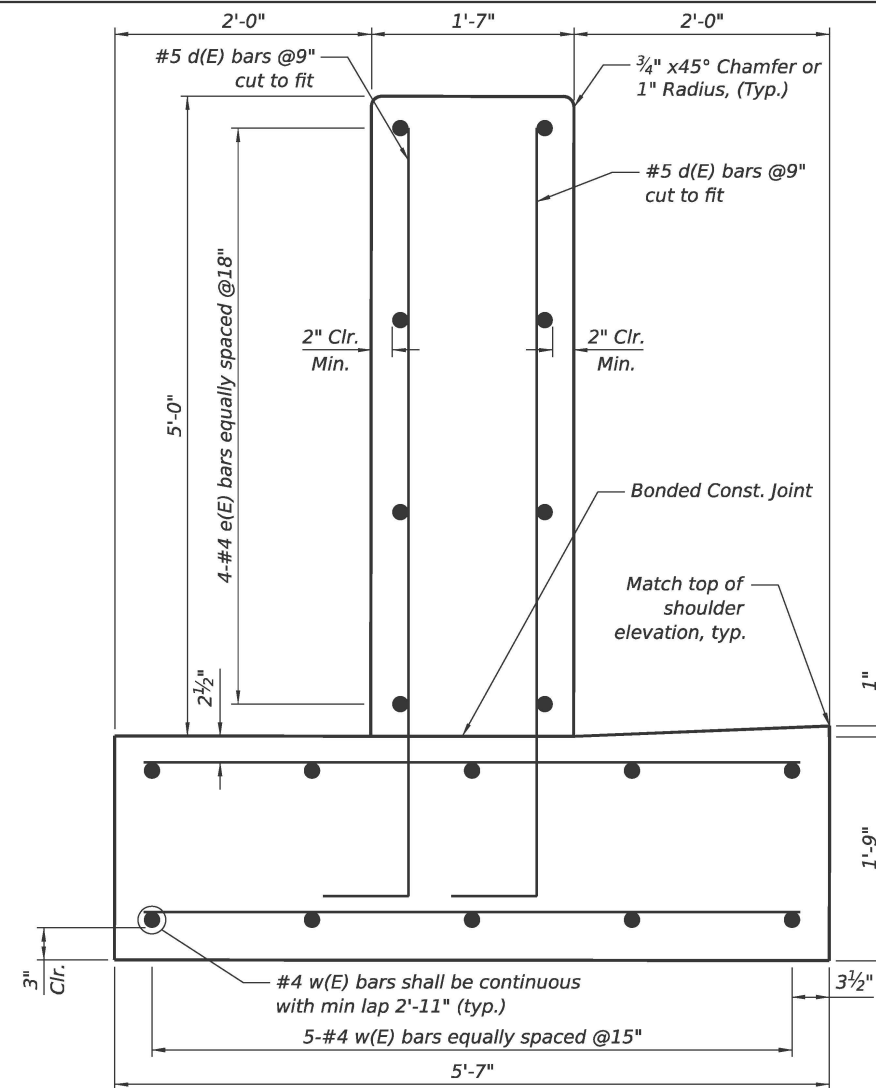
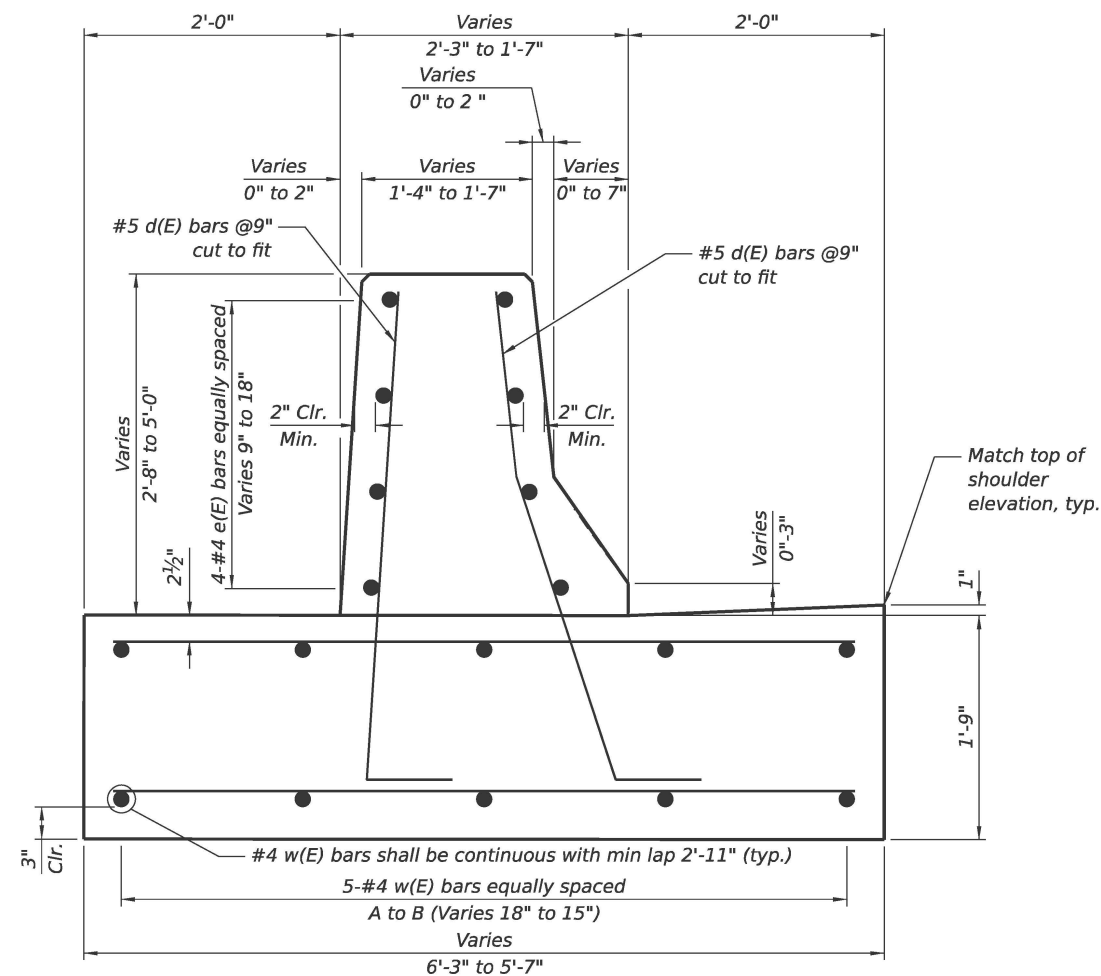
CONCRETE BARRIER WALL TRANSITION TO PROPOSED PIER 2



PLAN VIEW

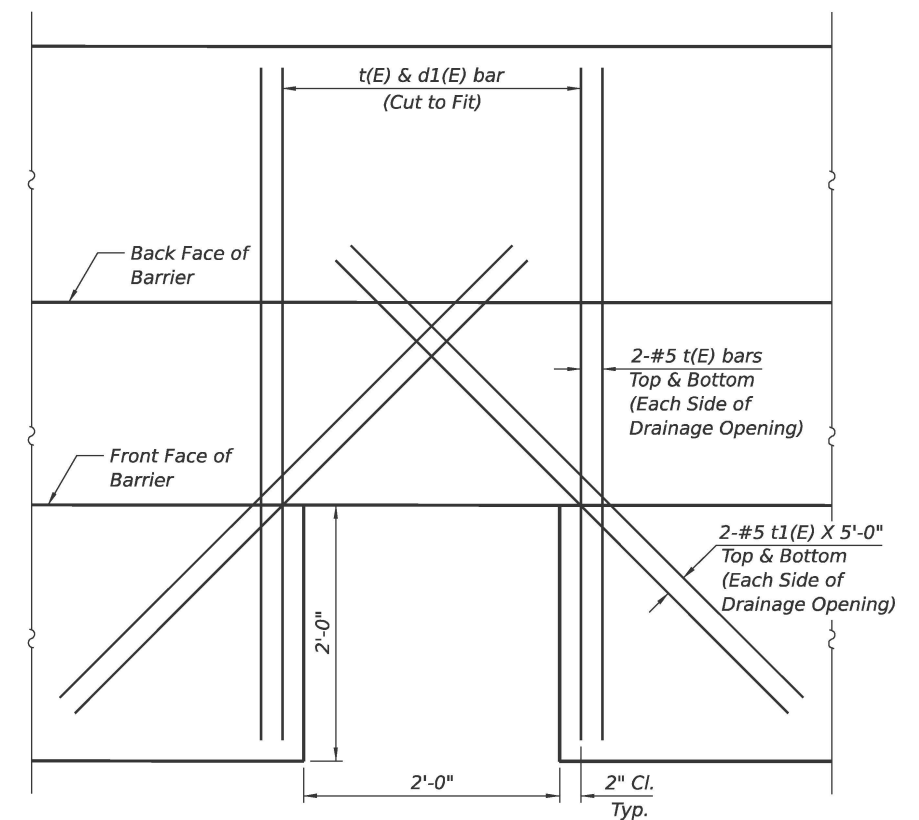


ELEVATION VIEW
(Looking South)

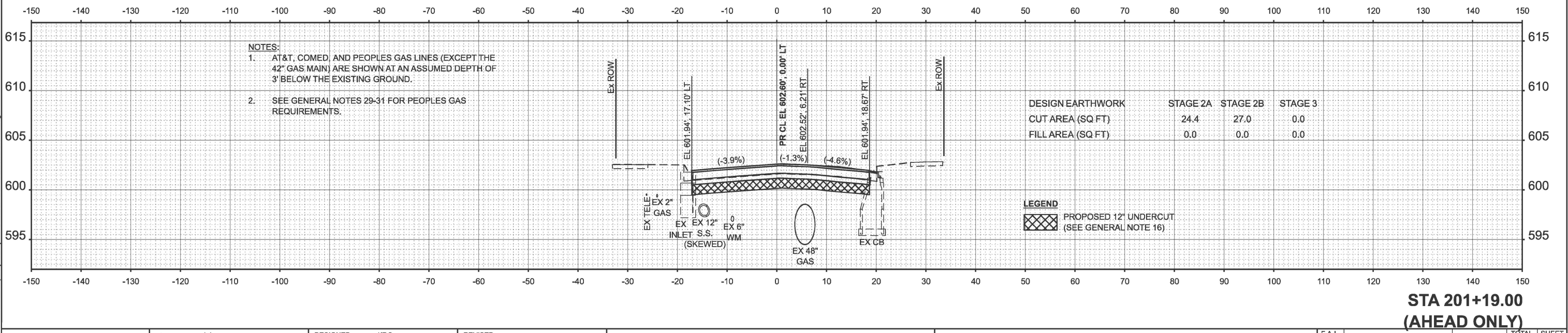
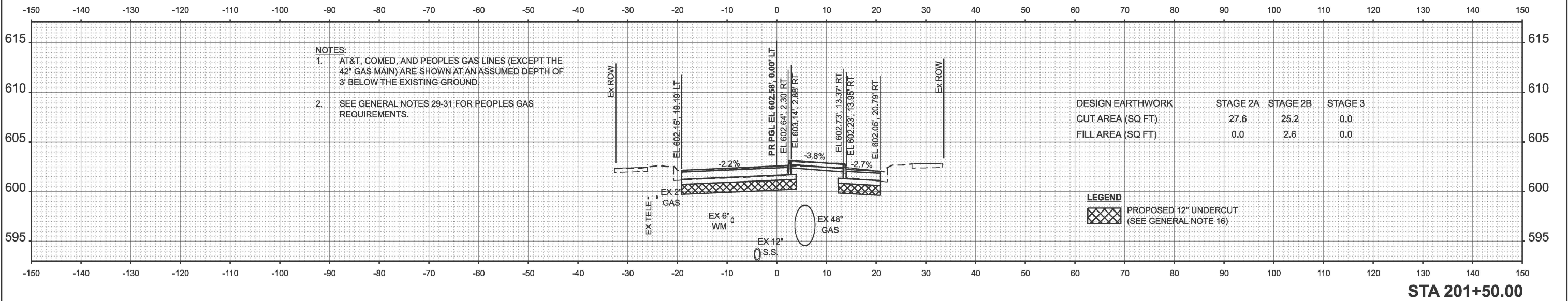


- Notes:*

1. *Expansion joints shown on this drawing shall be preformed joint material (bituminous type) filler shall meet AASHTO designation M-33.*
2. *All work detailed herein shall be included in the cost of the various concrete barrier pay items unless otherwise noted.*
3. *Preformed joint filler shall be included in the cost of the concrete barrier transition*
4. *Joints shall be constructed as shown on the plans and in accordance with article 637.08 of the standard specifications*



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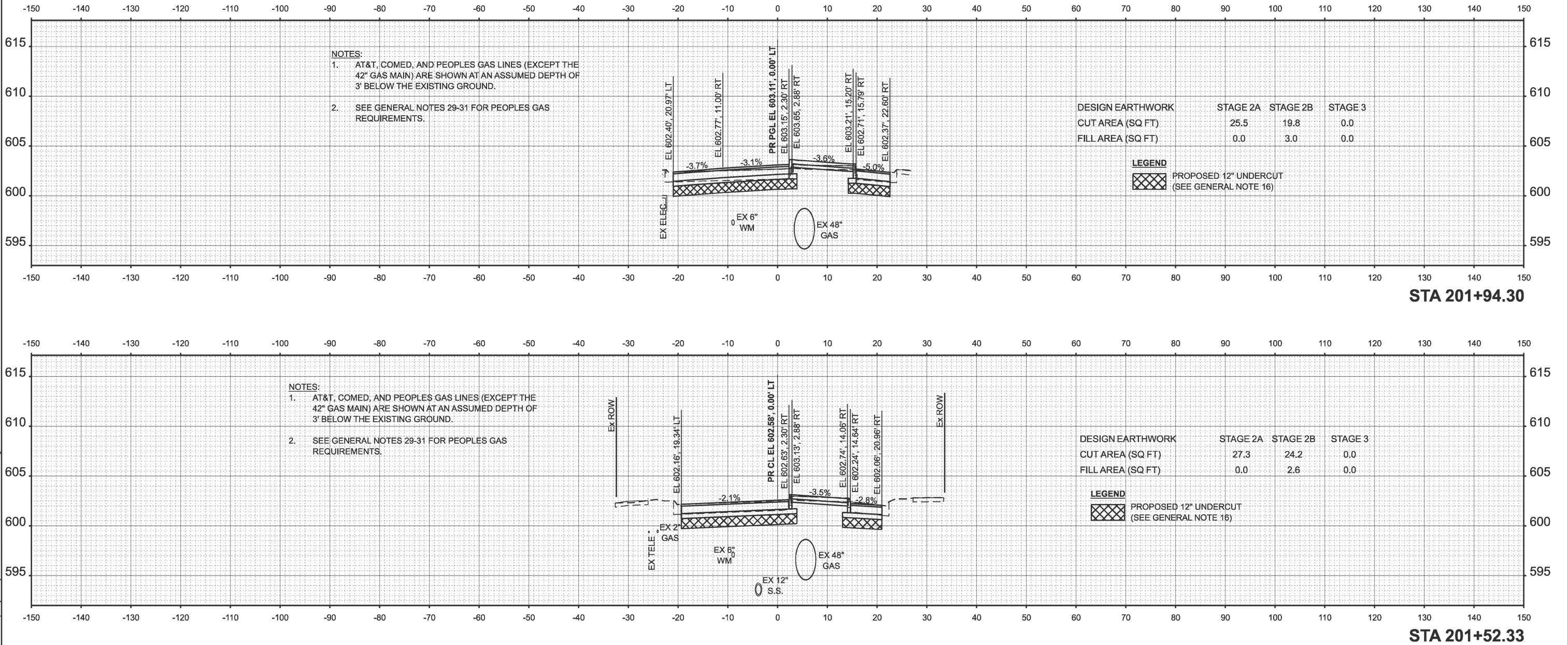
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - KEELER AVENUE

SCALE: 1"=10' SHEET 1 OF 36 SHEETS STA. 201+19.00 TO STA. 201+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	248
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Keeler Avenue - 201+52.33 [Sheet]
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - KEELER AVENUE

SCALE: 1"=10' SHEET 2 OF 36 SHEETS STA. 201+52.33 TO STA. 201+94.30

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	249
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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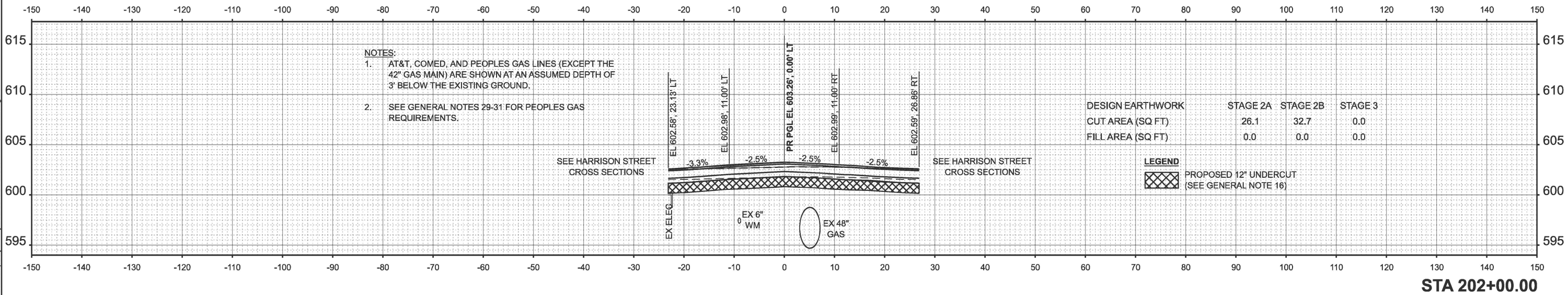
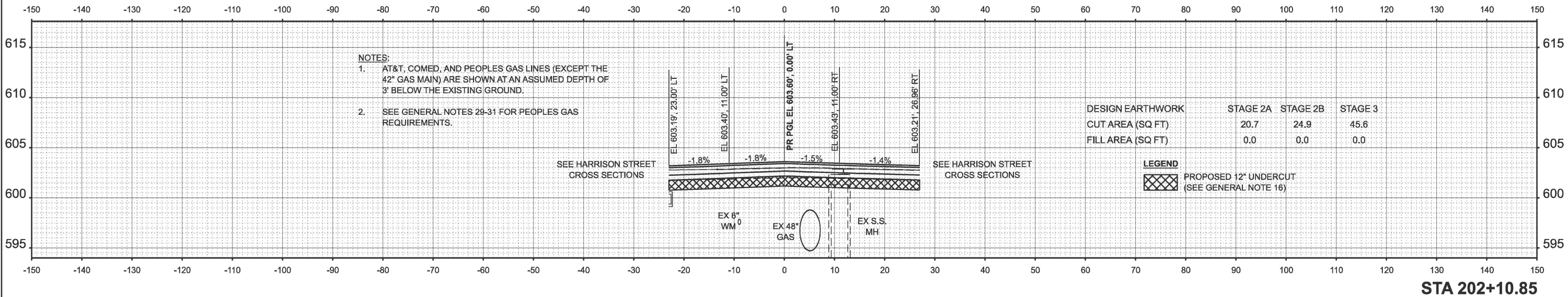
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

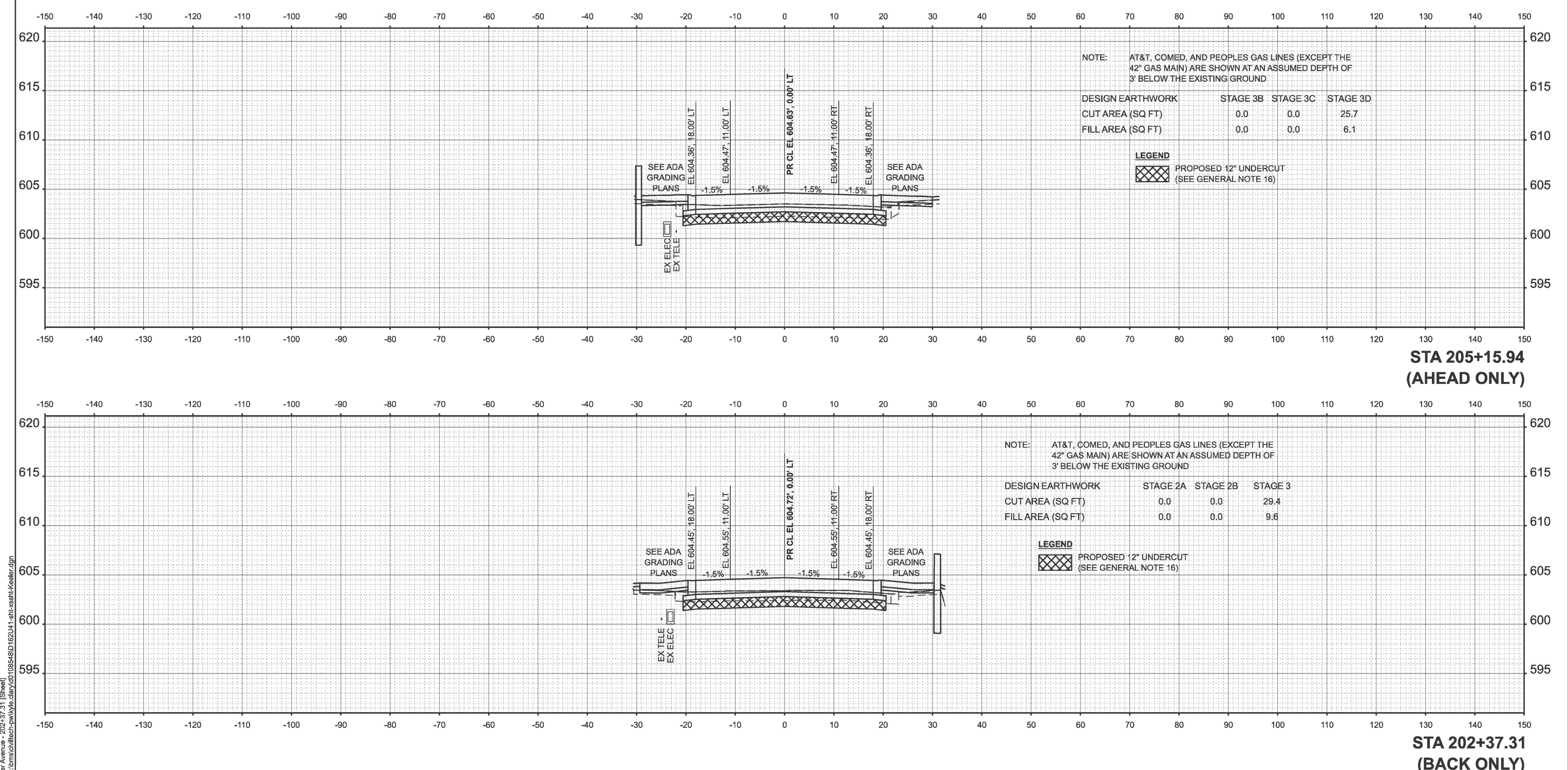
I-290 AT KEELER AVENUE
CROSS SECTIONS - KEELER AVENUE

SCALE: 1"=10'
SHEET 3 OF 36 SHEETS
STA. 202+00.00 TO STA. 202+10.85

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	250
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



MODEL: Keeler Avenue - 202+37.31 [Sheet]
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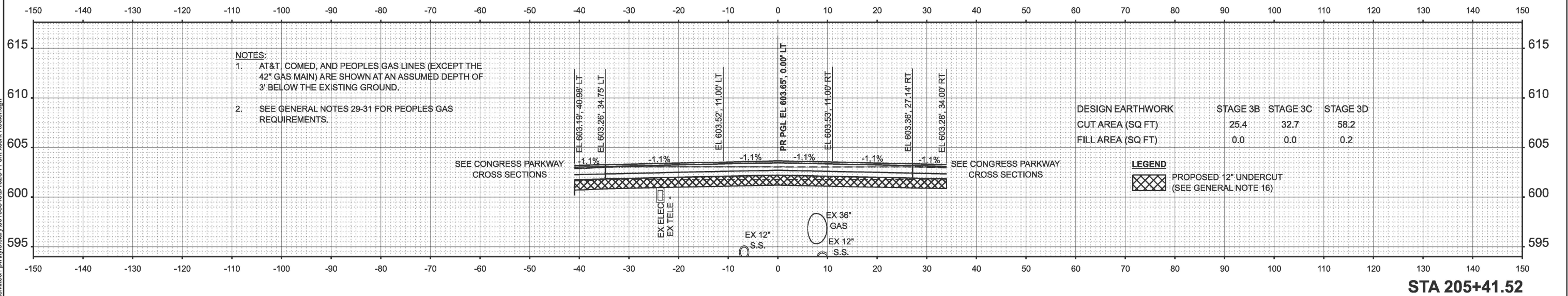
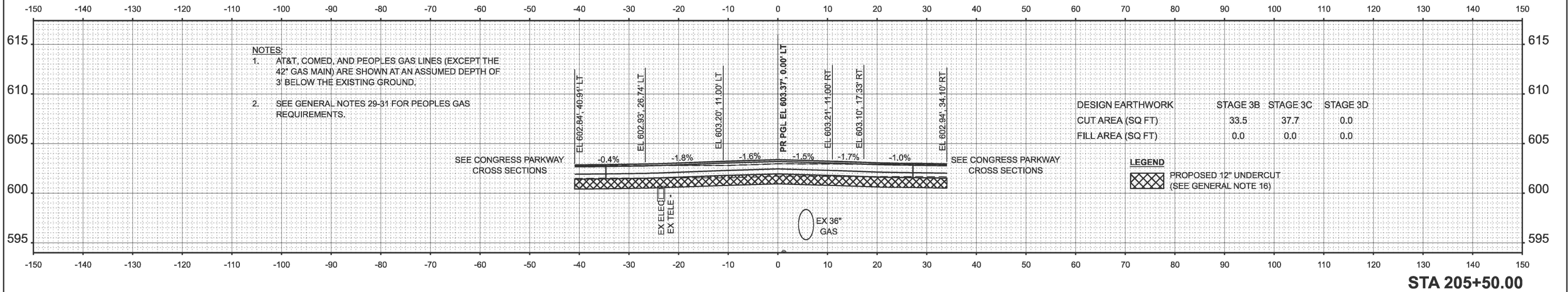
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - KEELER AVENUE

SCALE: 1"=10' SHEET 4 OF 36 SHEETS STA. 202+37.31 TO STA. 205+15.94

F&I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	251
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Keeler Avenue - 205+41.52 (Sheet)
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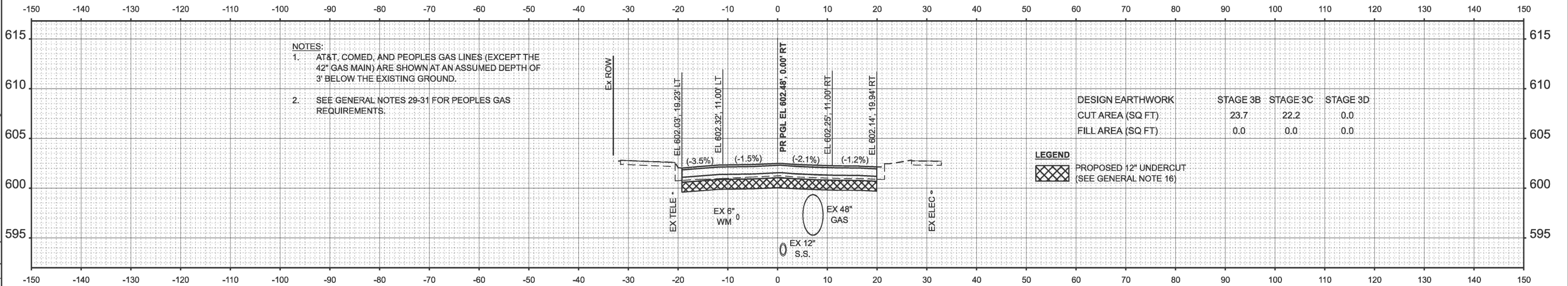
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - KEELER AVENUE

SCALE: 1"=10' SHEET 5 OF 36 SHEETS STA. 205+41.52 TO STA. 205+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	252
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Keeler Avenue - 206+00.01
FILE NAME: c:\bms\civiltech\p\k\kyle.dgn



STA 206+00.00
(BACK ONLY)



USER NAME = kdc	DESIGNED - KDC	REVISED -
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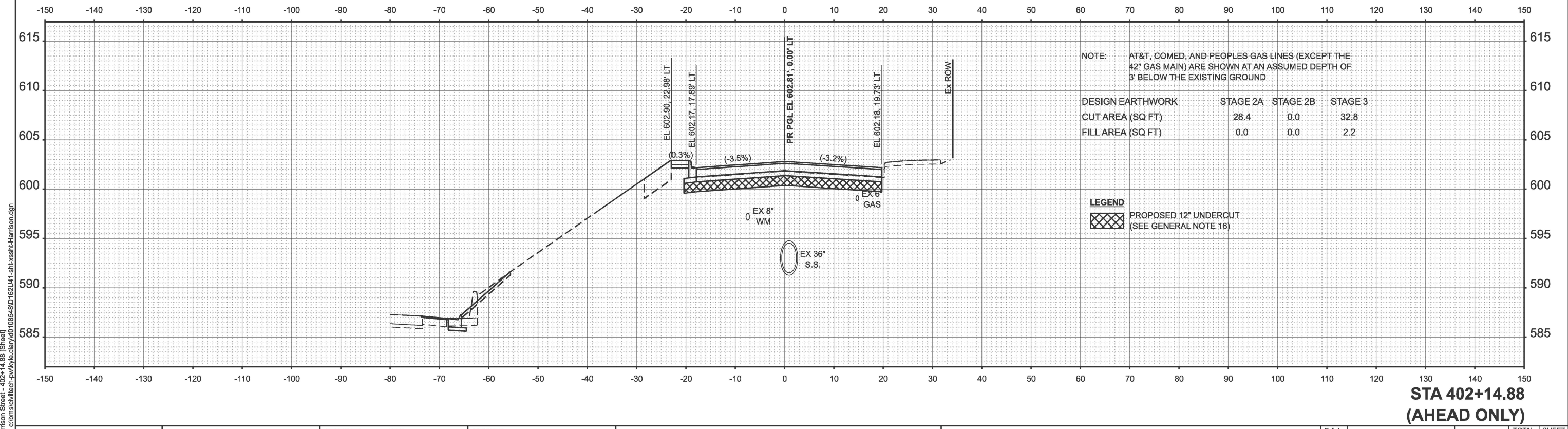
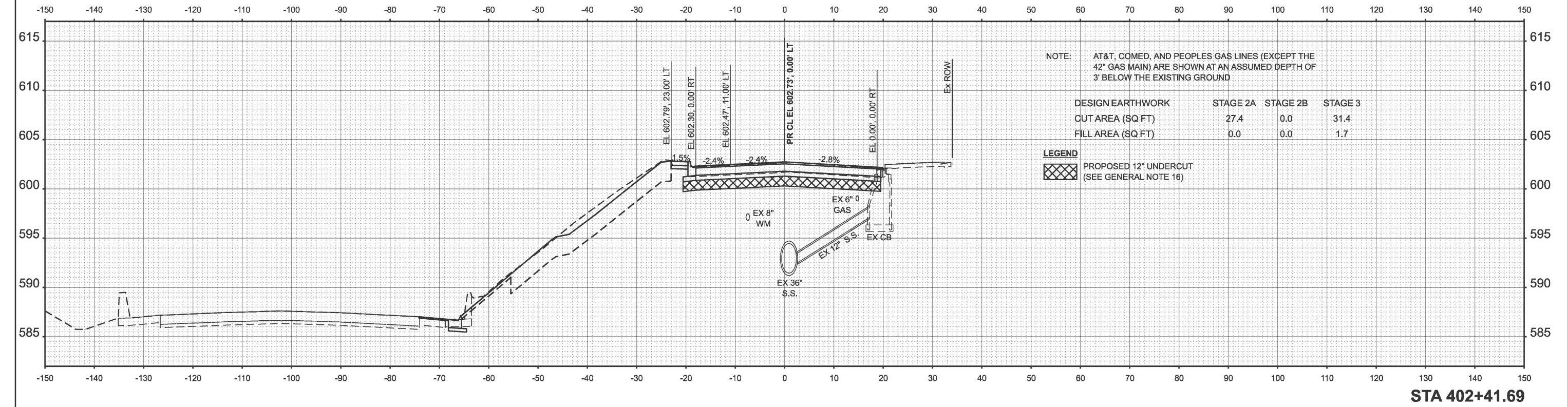
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - KEELER AVENUE

SCALE: 1"=10' SHEET 6 OF 36 SHEETS STA. 206+00.00 TO STA. 206+00.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	253
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Harrison Street - 402+14.88 (Sheet)
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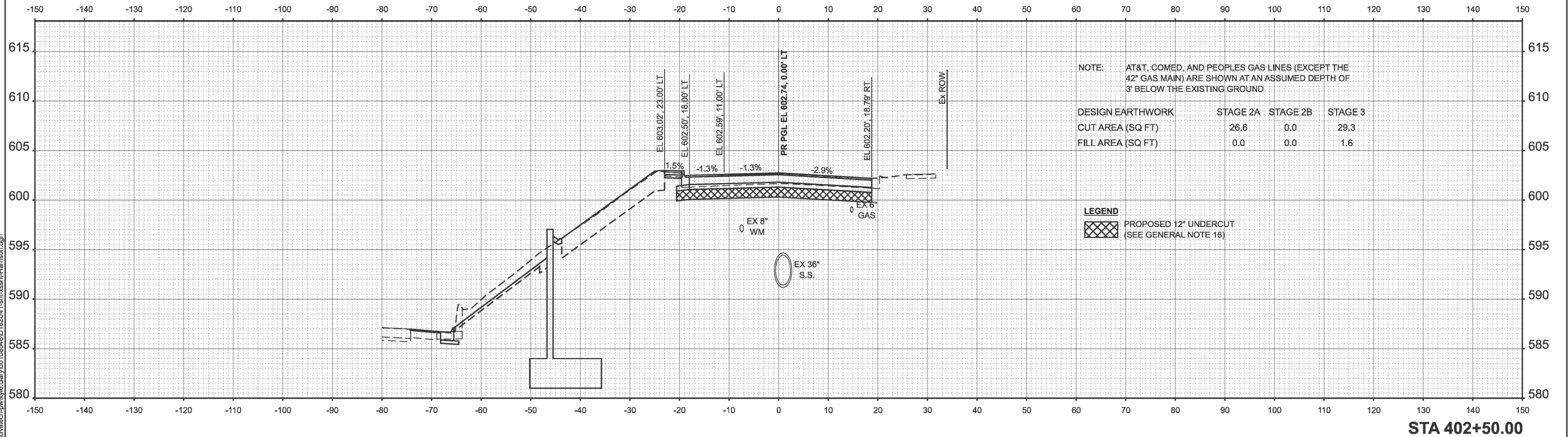
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - HARRISON STREET

SCALE: 1"=10' SHEET 7 OF 36 SHEETS STA. 402+14.88 TO STA. 402+41.69

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	254
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Harrison Street - 402+50.00 [Sheet]
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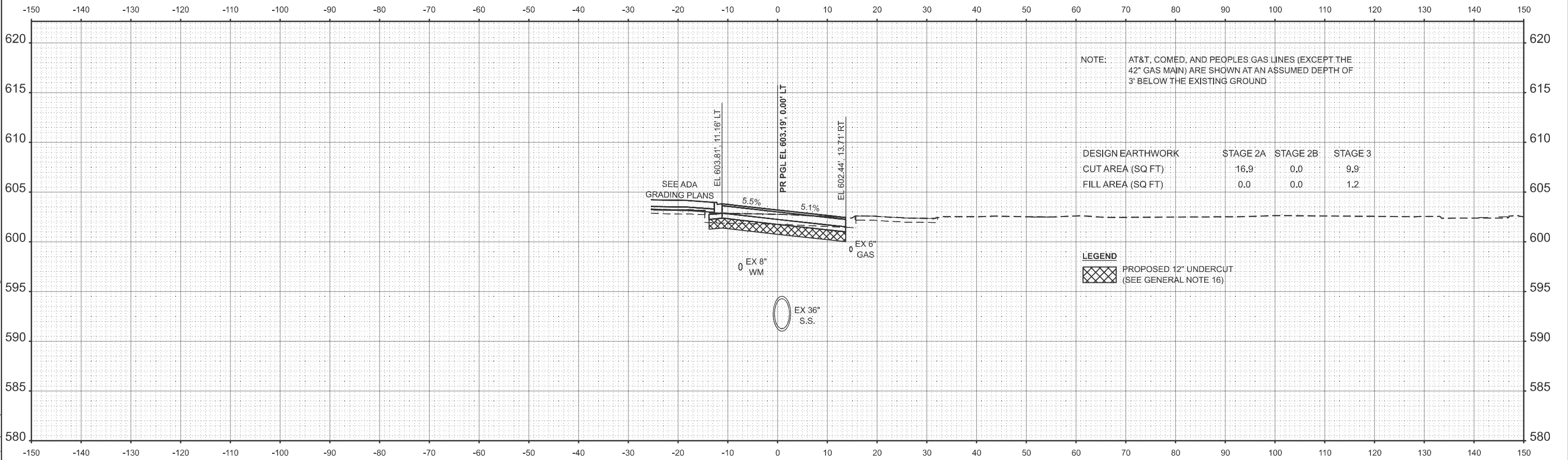
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - HARRISON STREET

SCALE: 1"=10' SHEET 8 OF 36 SHEETS STA. 402+50.00 TO STA. 402+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	255
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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STA 403+00.00
(BACK ONLY)



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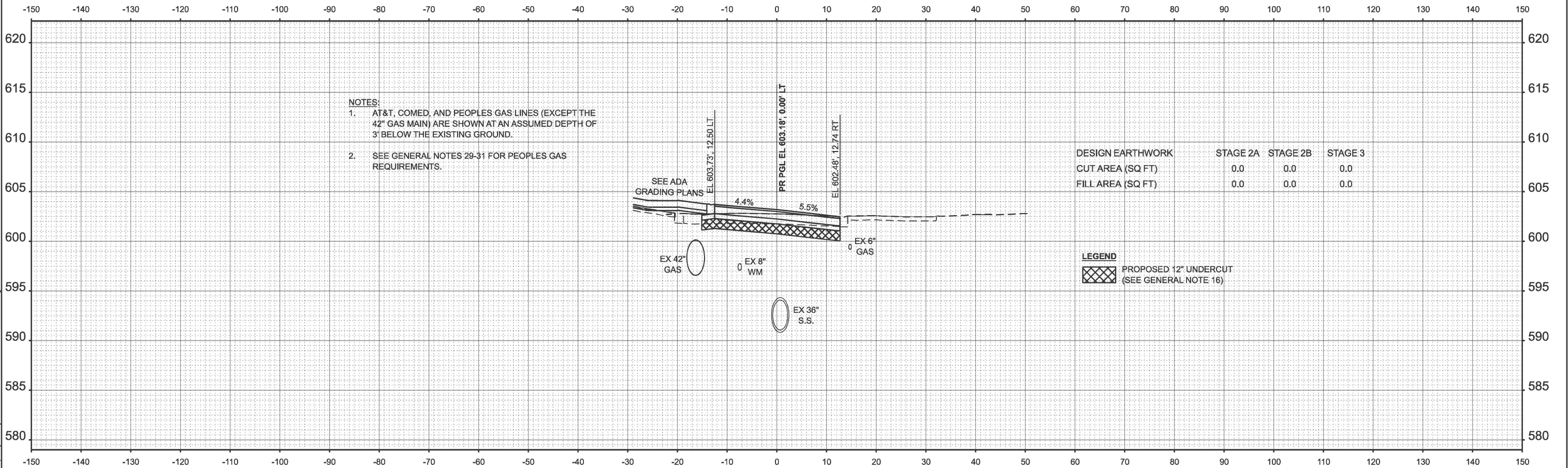
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - HARRISON STREET

SCALE: 1"=10' SHEET 9 OF 36 SHEETS STA. 403+00.00 TO STA. 403+00.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	256
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Harrison Street - 403+50.00 [Sheet]
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STA 403+50.00
(AHEAD ONLY)



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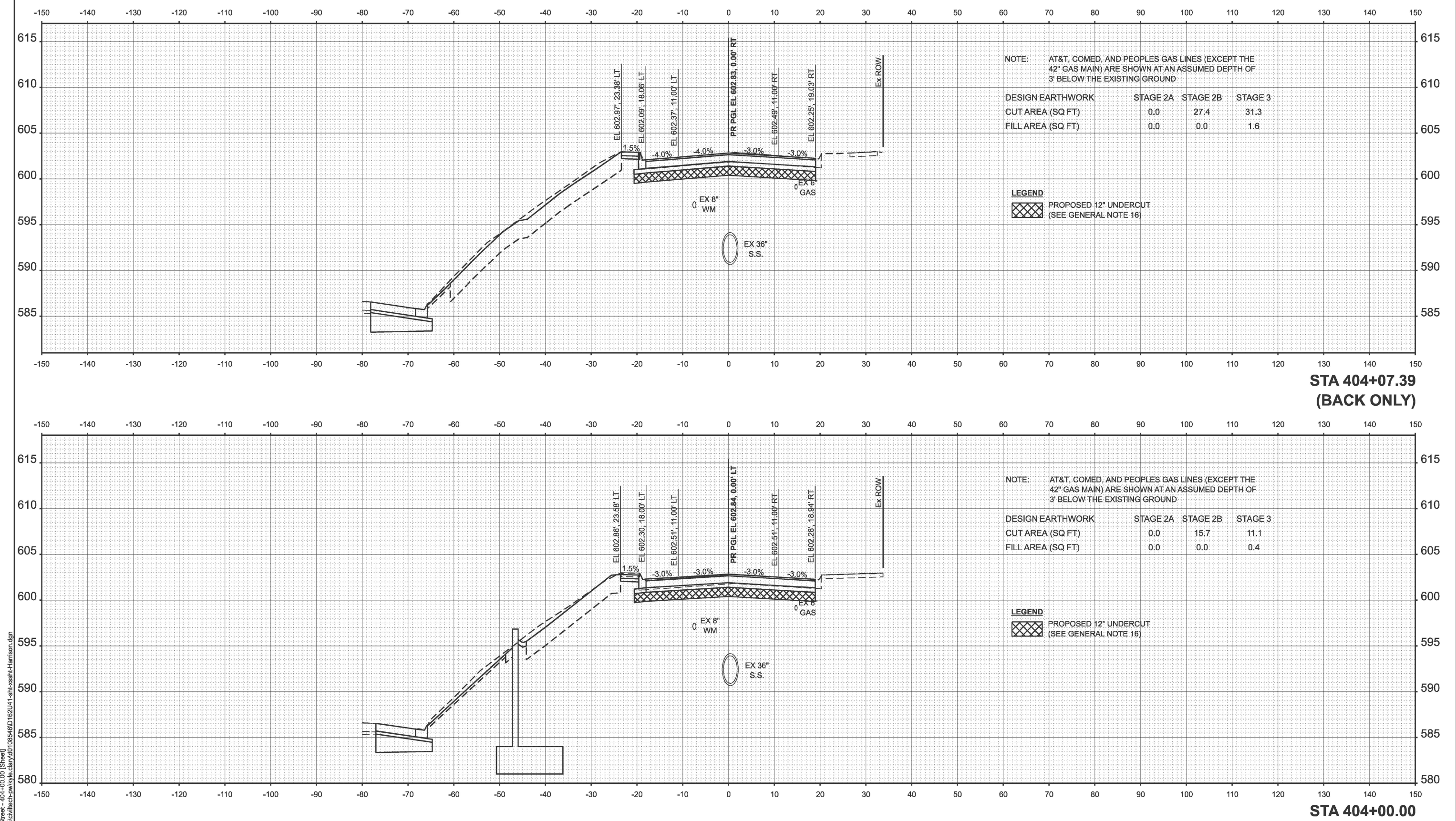
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - HARRISON STREET

SCALE: 1"=10' SHEET 10 OF 36 SHEETS STA. 403+50.00 TO STA. 403+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	257
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Harrison Street - 404+00.00 [Sheet]
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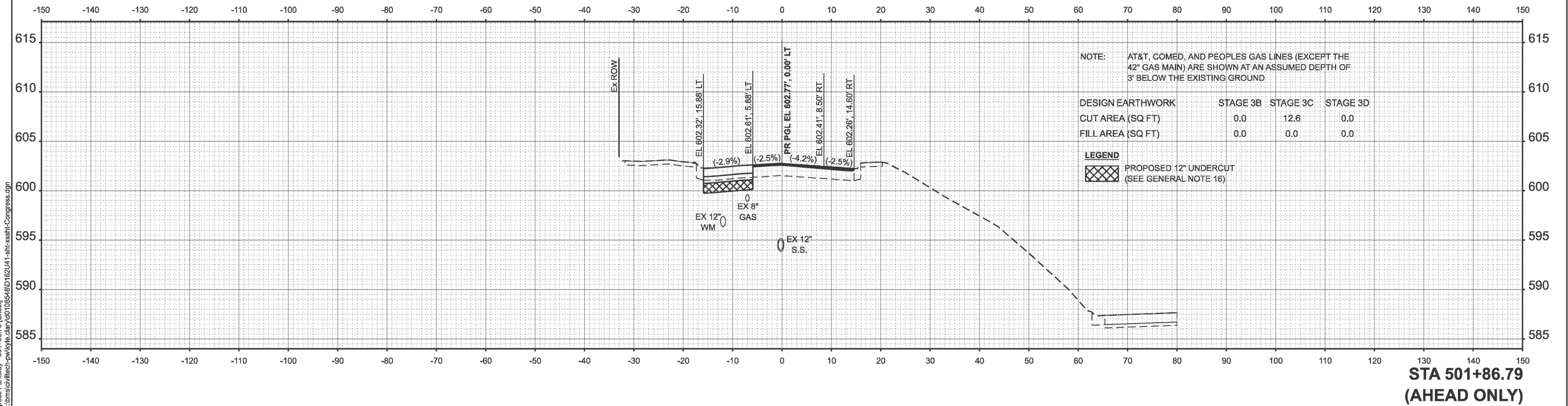
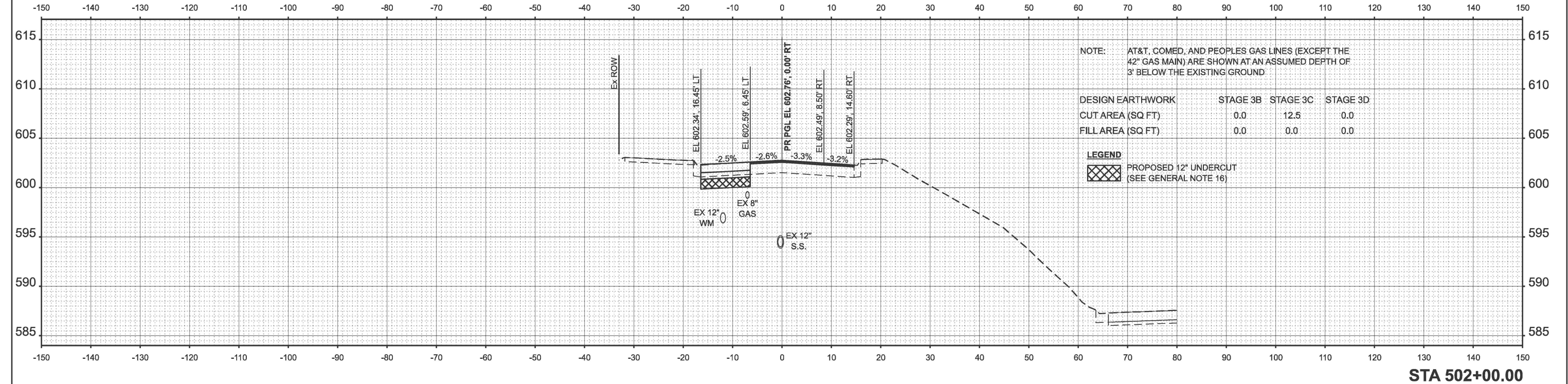
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - HARRISON STREET

SCALE: 1"=10' SHEET 11 OF 36 SHEETS STA. 404+00.00 TO STA. 404+07.39

F&I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	258
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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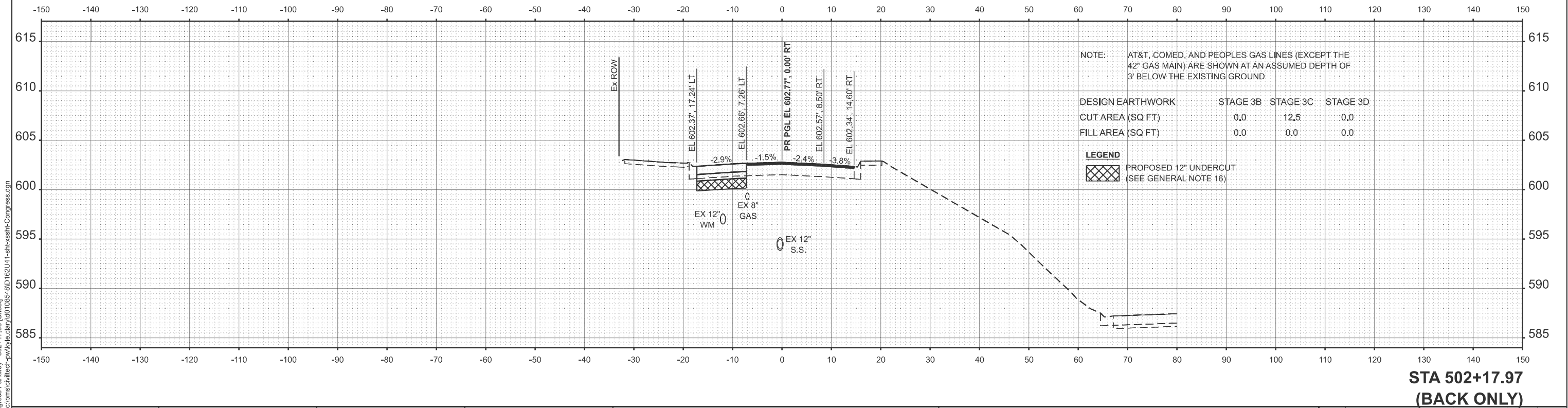
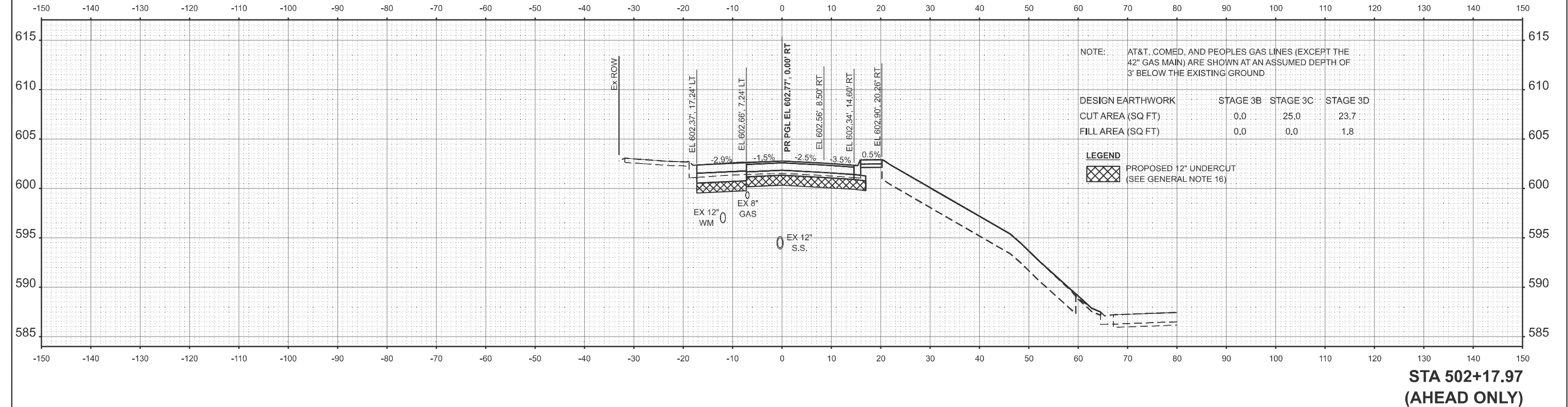
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - CONGRESS PARKWAY

SCALE: 1"=10'
SHEET 12 OF 36 SHEETS
STA. 501+86.79 TO STA. 502+00.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	259
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: Congress Parkway - 502+17.96 [Sheet]
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - CONGRESS PARKWAY

SCALE: 1"=10'
SHEET 13 OF 36 SHEETS
STA. 502+17.97 TO STA. 502+17.97

F&I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	260
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

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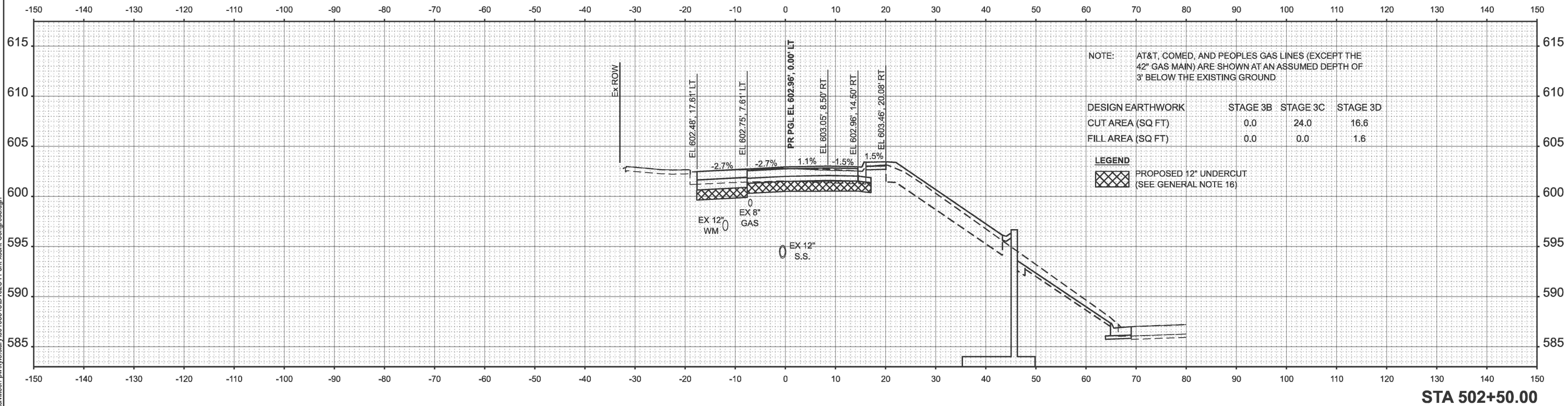
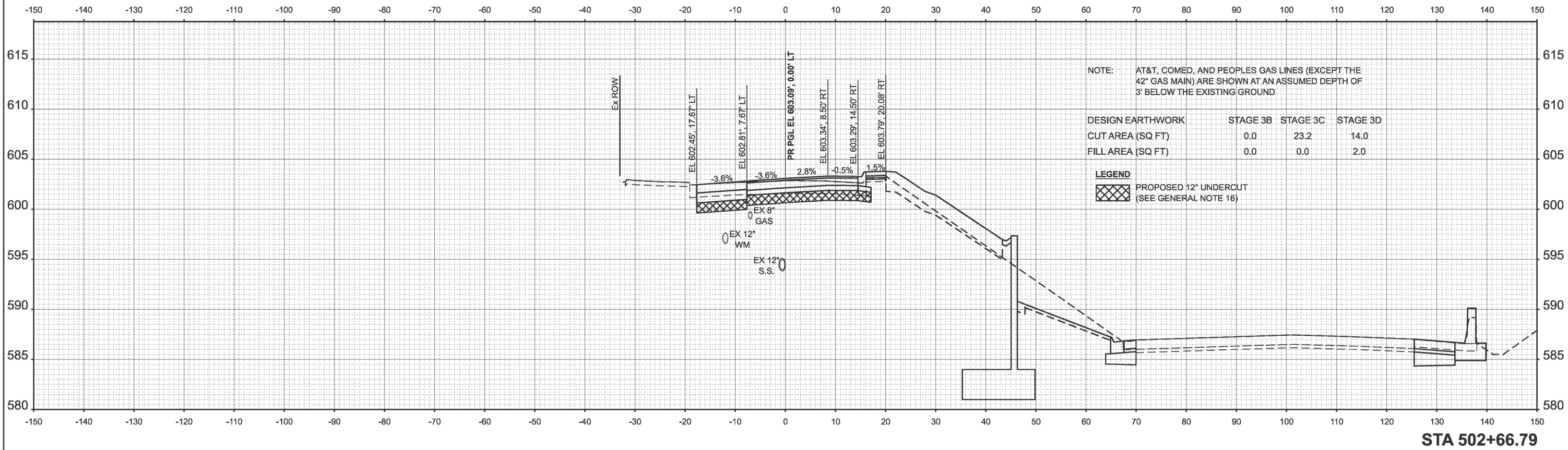
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - CONGRESS PARKWAY

SCALE: 1"=10'
SHEET 14 OF 36 SHEETS
STA. 502+50.00 TO STA. 502+66.79

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	261
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



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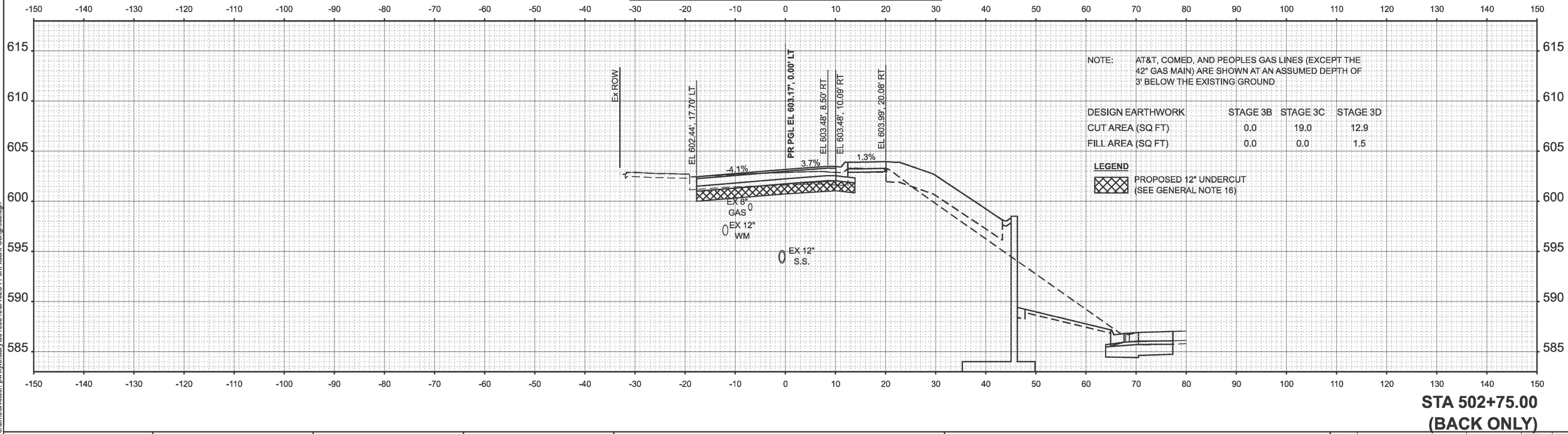
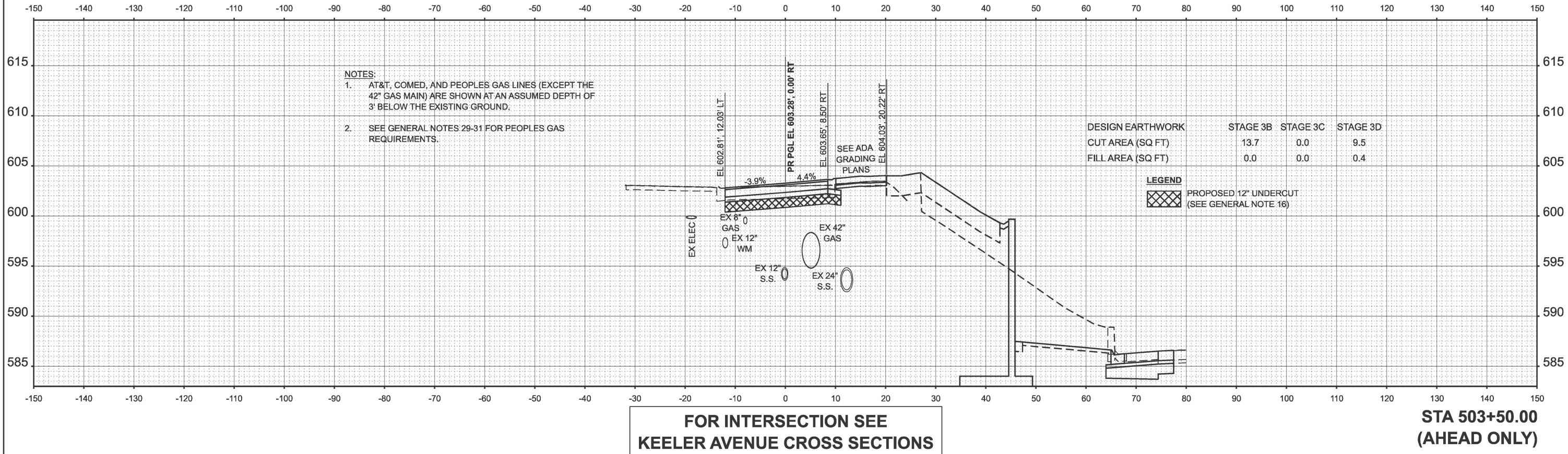
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - CONGRESS PARKWAY

SCALE: 1"=10'
SHEET 15 OF 36 SHEETS
STA. 502+75.00 TO STA. 503+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	262
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



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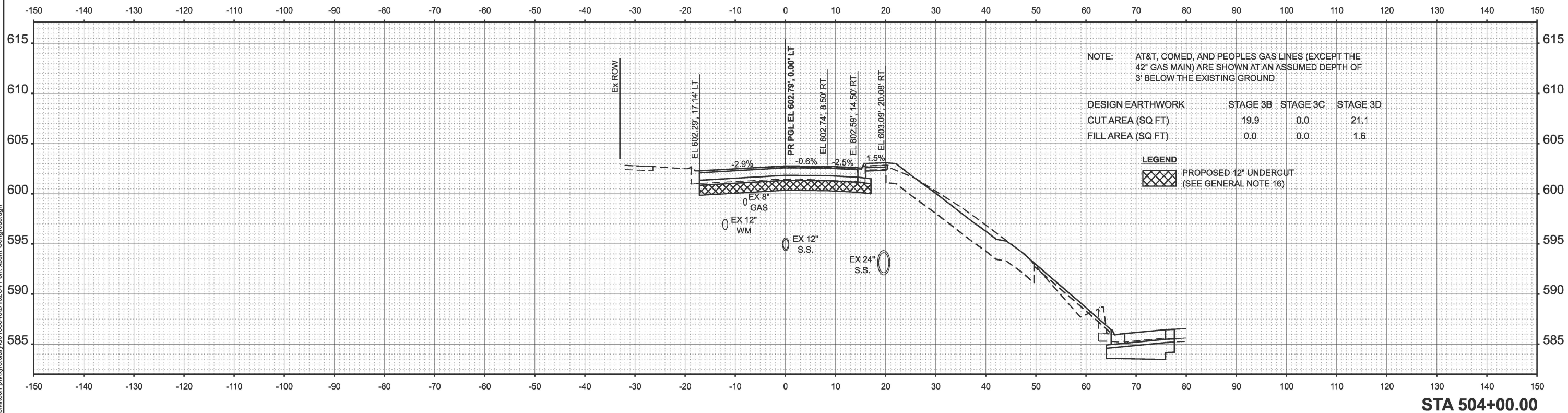
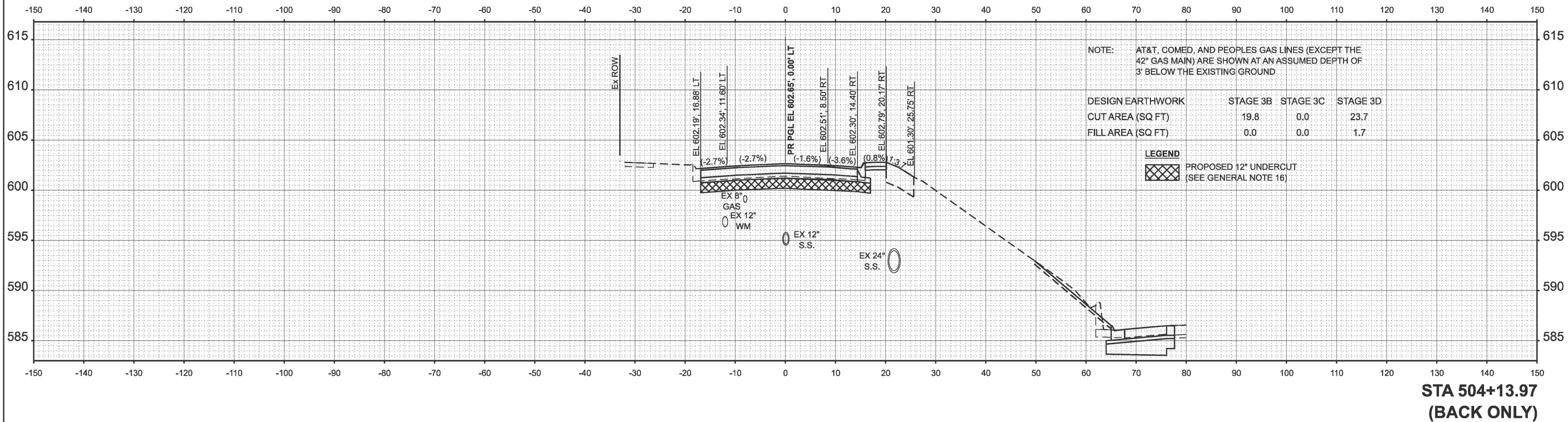
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

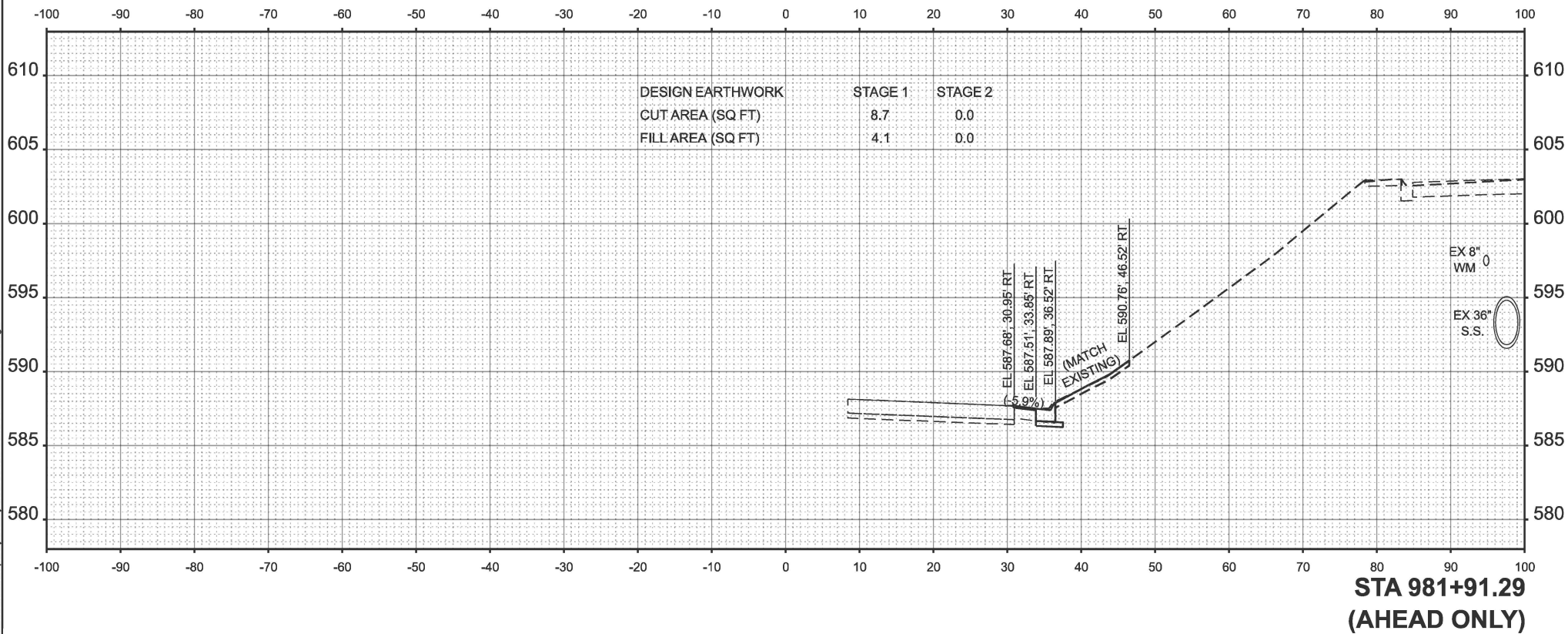
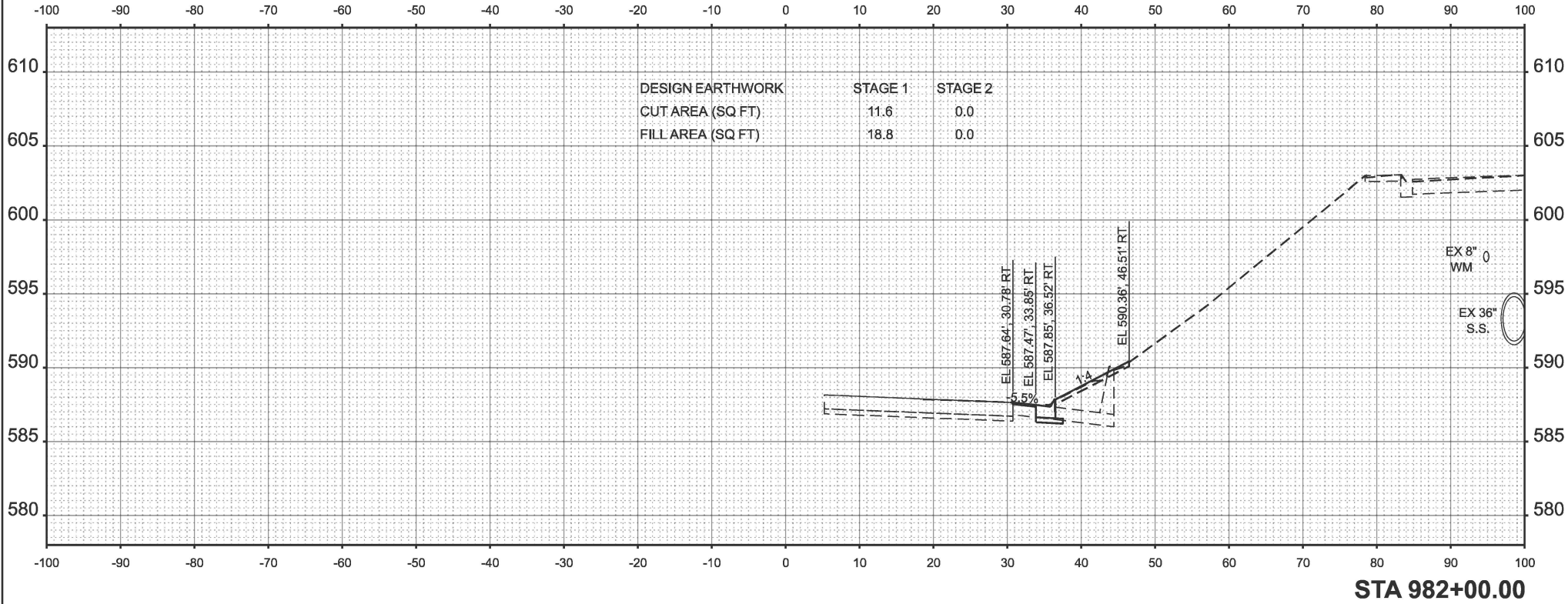
I-290 AT KEELER AVENUE
CROSS SECTIONS - CONGRESS PARKWAY

SCALE: 1"=10'
SHEET 16 OF 36 SHEETS
STA. 504+00.00 TO STA. 504+13.97

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	263
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

SCALE: 1"=10' SHEET 17 OF 36 SHEETS STA. 981+91.29 TO STA. 982+00.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	284
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Eastbound Ext - 982+50.00-3
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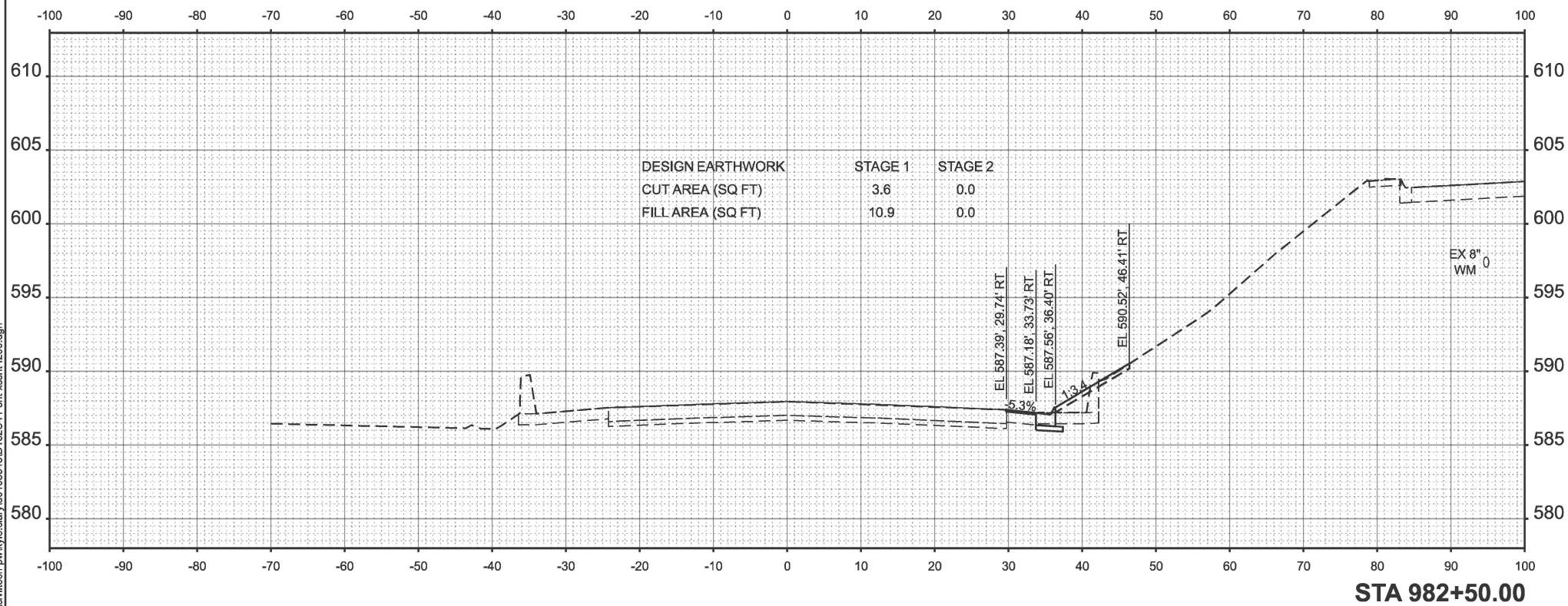
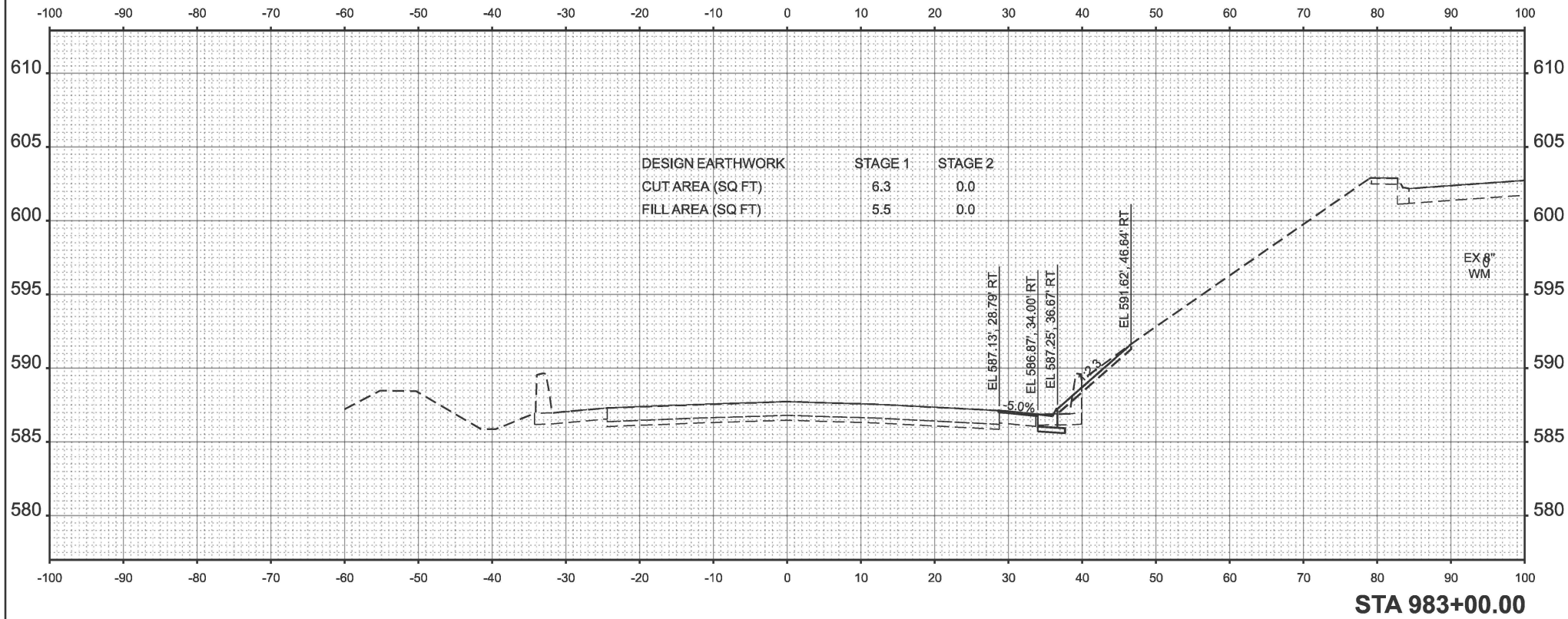
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

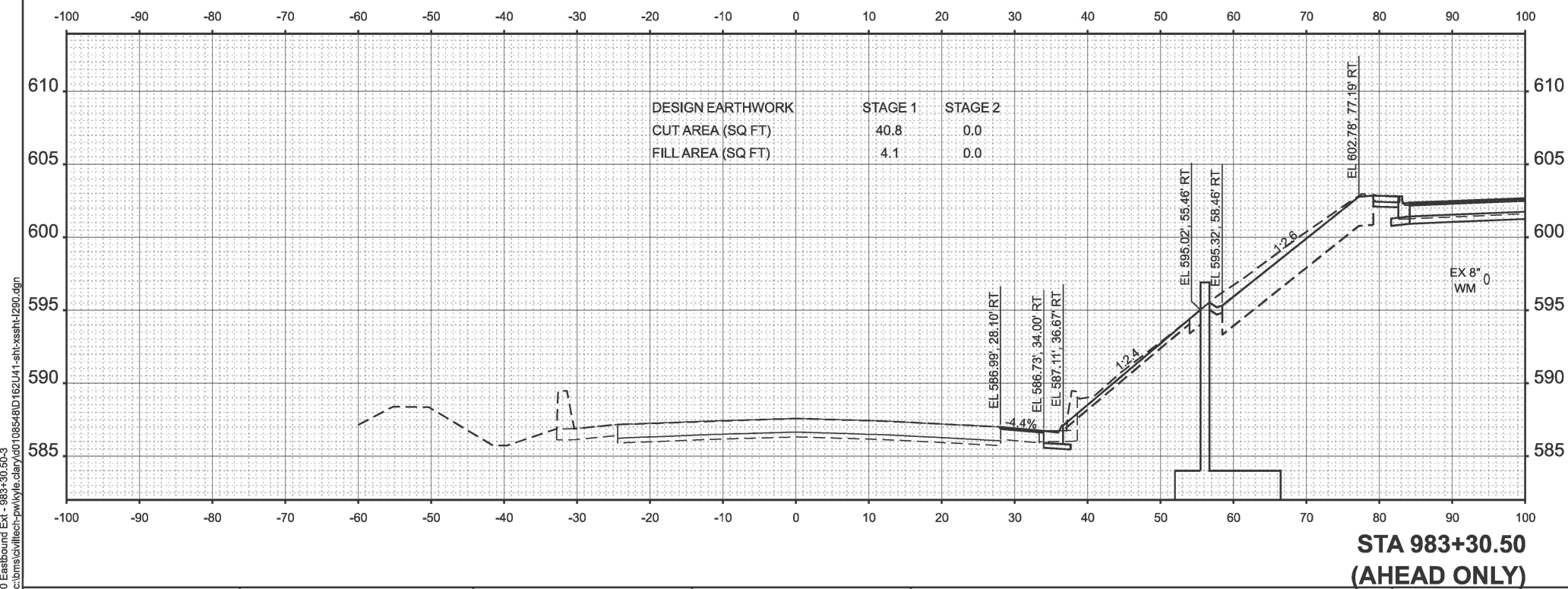
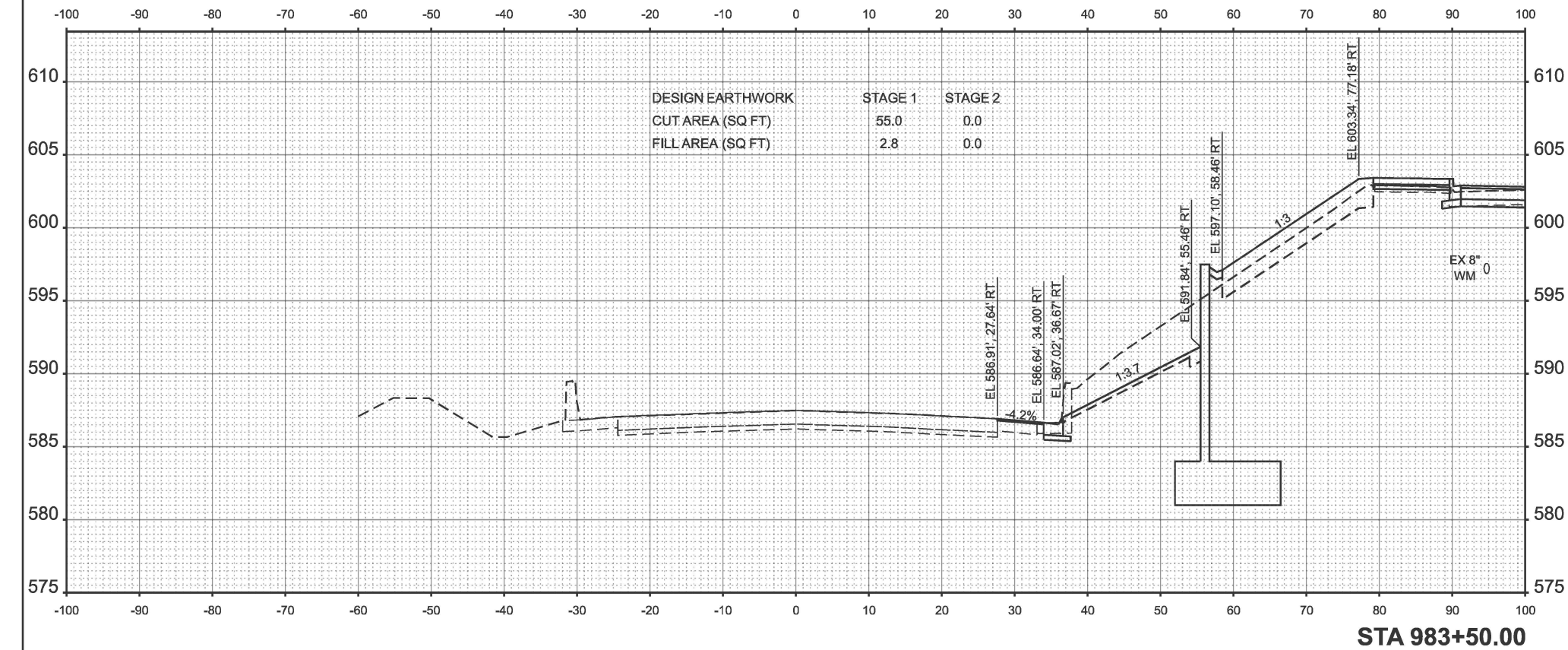
I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

SCALE: 1"=10' SHEET 18 OF 36 SHEETS STA. 982+50.00 TO STA. 983+00.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	265
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



MODEL: I-290 Eastbound Ext - 983+30.50-3
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	CHECKED - DJK	REVISED -
	DATE - 8/29/2025	REVISED -
PLOT DATE = 8/27/2025		

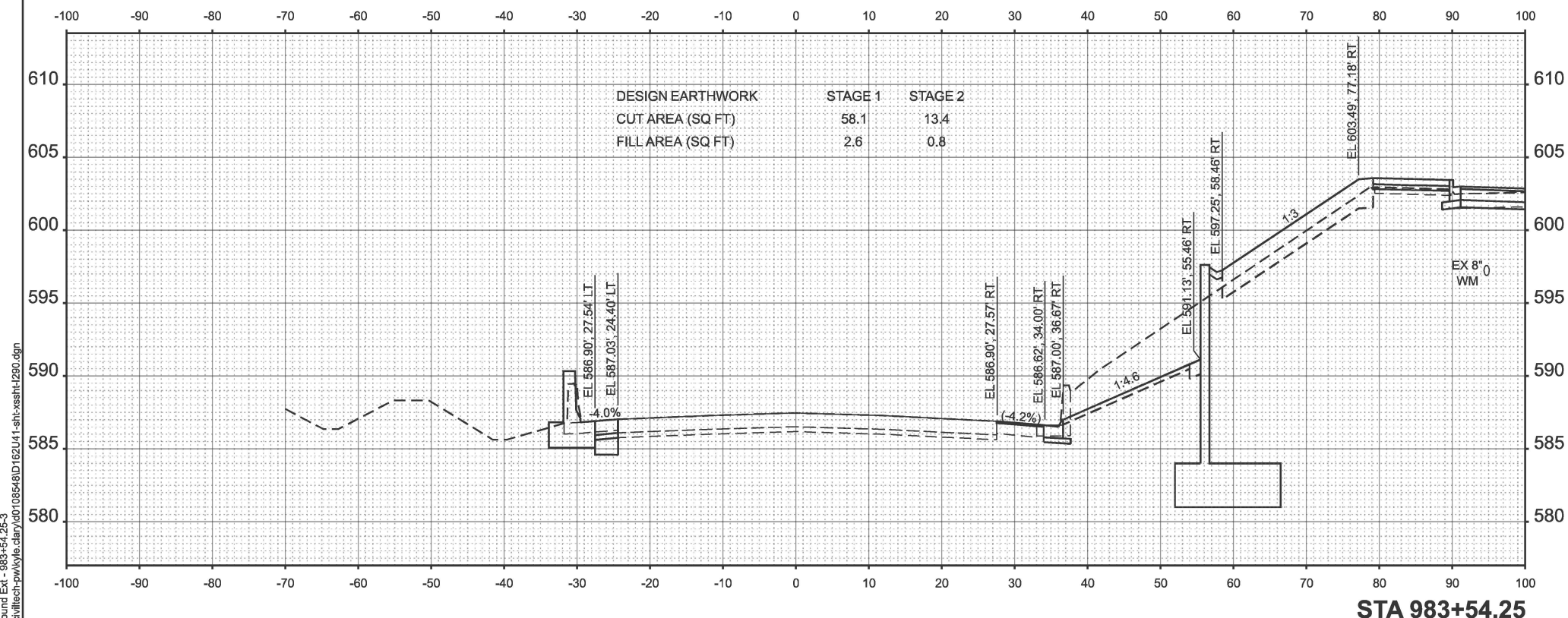
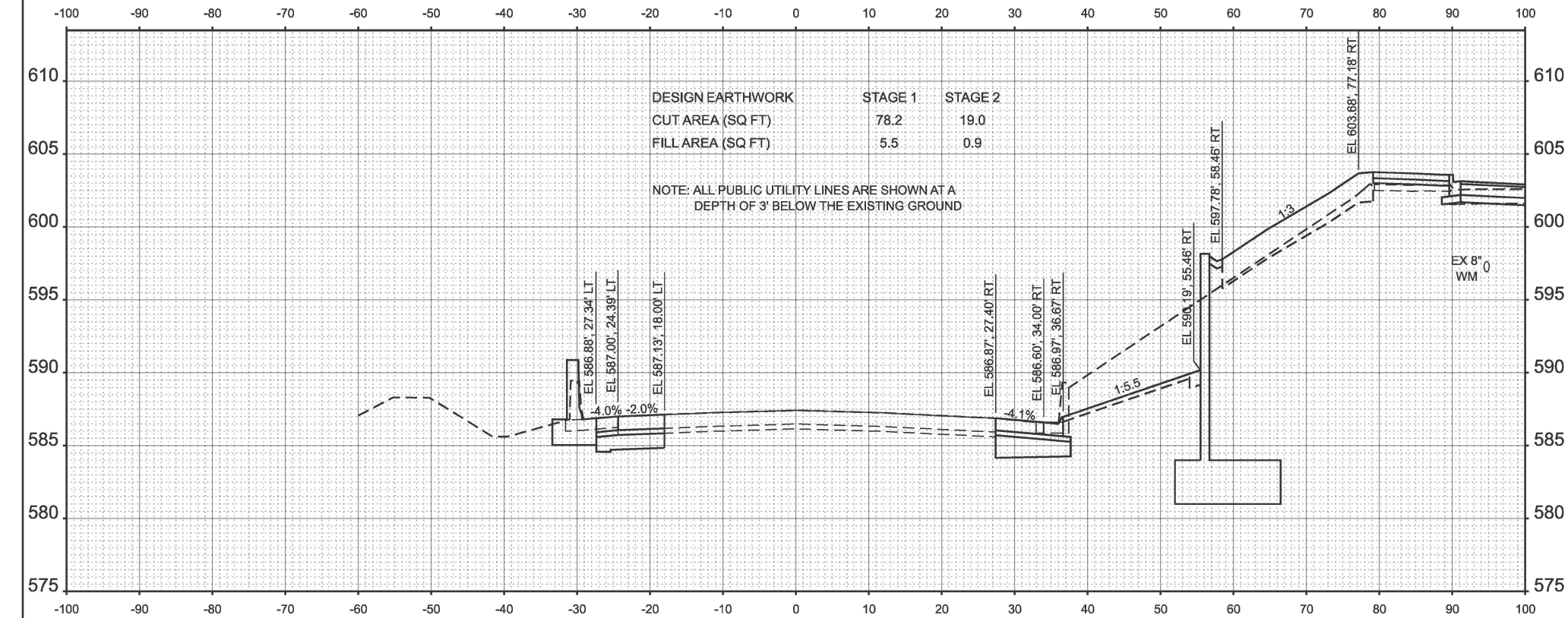
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

SCALE: 1"=10' SHEET 19 OF 36 SHEETS STA. 983+30.50 TO STA. 983+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	266
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Eastbound Ext - 983+54.25-3
FILE NAME: c:\bms\civiltech\pw\kyle.dary\0108548\162141-sht-ssht-290.dgn



USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/27/2025	DATE - 8/29/2025	REVISED -

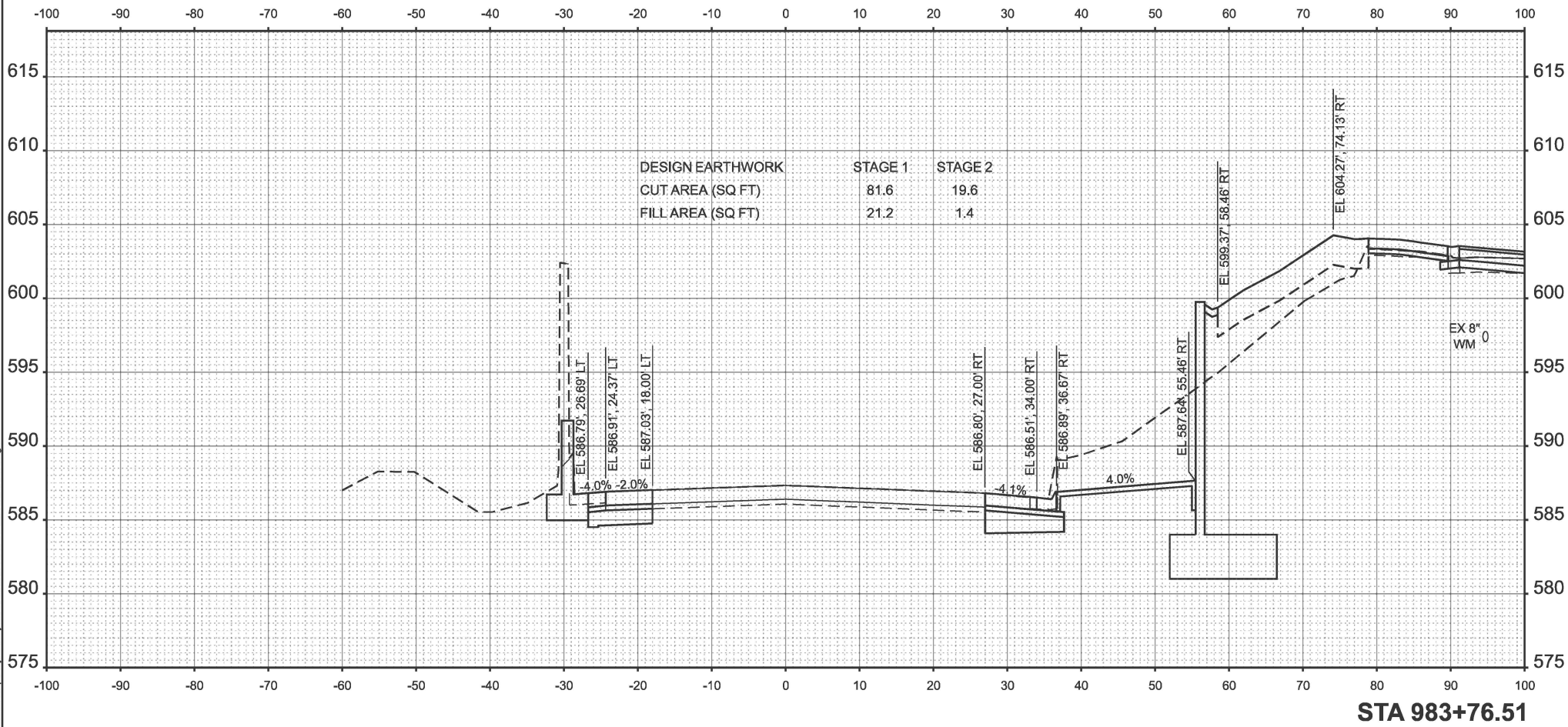
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

SCALE: 1"=10' SHEET 20 OF 36 SHEETS STA. 983+54.25 TO STA. 983+59.82

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	267
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Eastbound Ext - 983+76.50-3
FILE NAME: c:\bms\civiltech-pw\kyle.dwg\0108548\162141-shp-ssht-290.dgn



USER NAME = kdc

DESIGNED - KDC

REVISED -

DRAWN - KDC

REVISID -

CHECKED - DJK

REVISID -

DATE - 8/27/2025

DATE - 8/29/2025

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

SCALE: 1"=10'

SHEET 21

OF 36

SHEETS

STA. 983+76.51

TO STA. 983+76.51

F.A.I

RTE.

SECTION

COUNTY

TOTAL

SHEETS

SHEET

NO.

290

FAI 290 22 KEELER BR

COOK

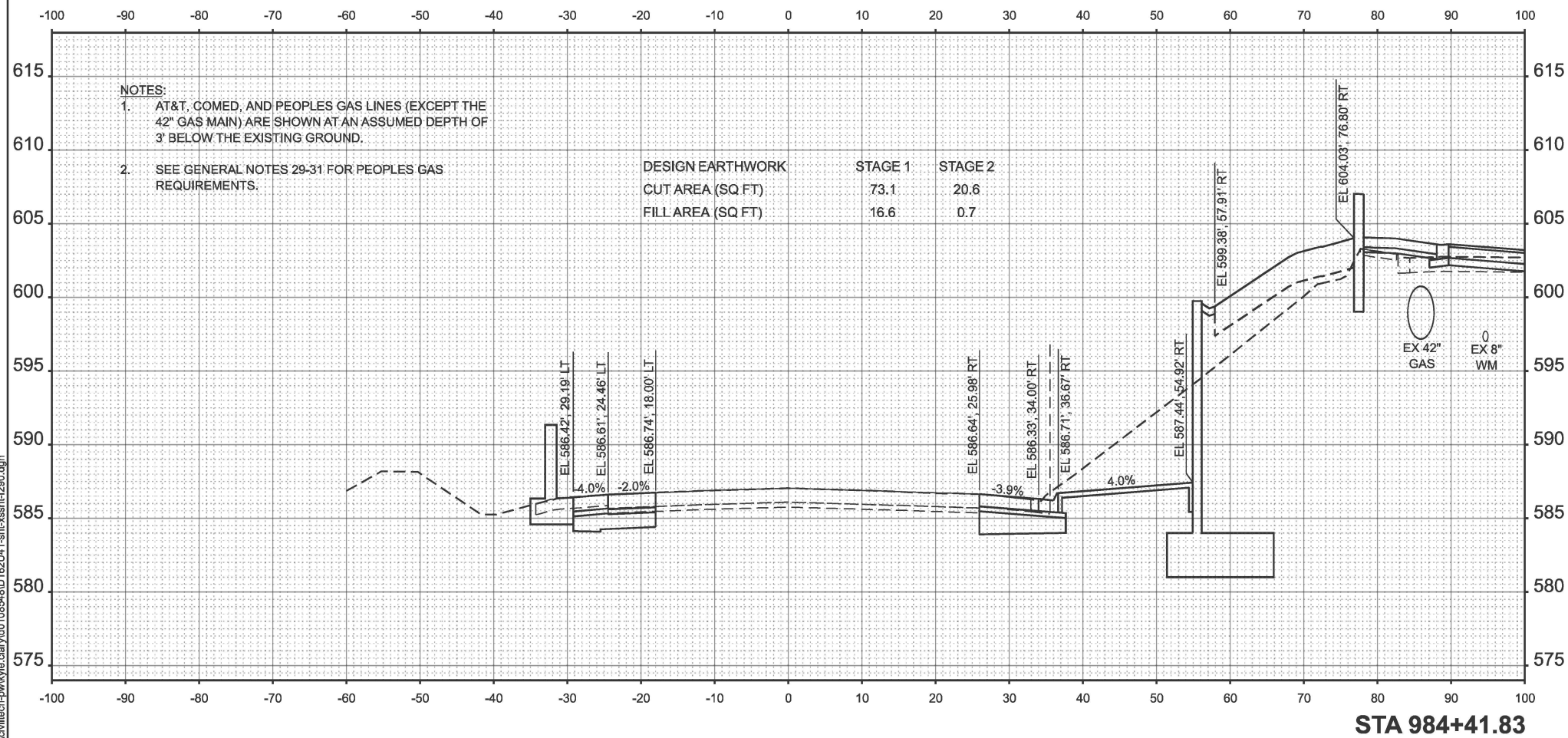
283

268

CONTRACT NO. 62U41

ILLINOIS FED. AID PROJECT

MODEL: I-290 Eastbound Ext - 984+41.83-3
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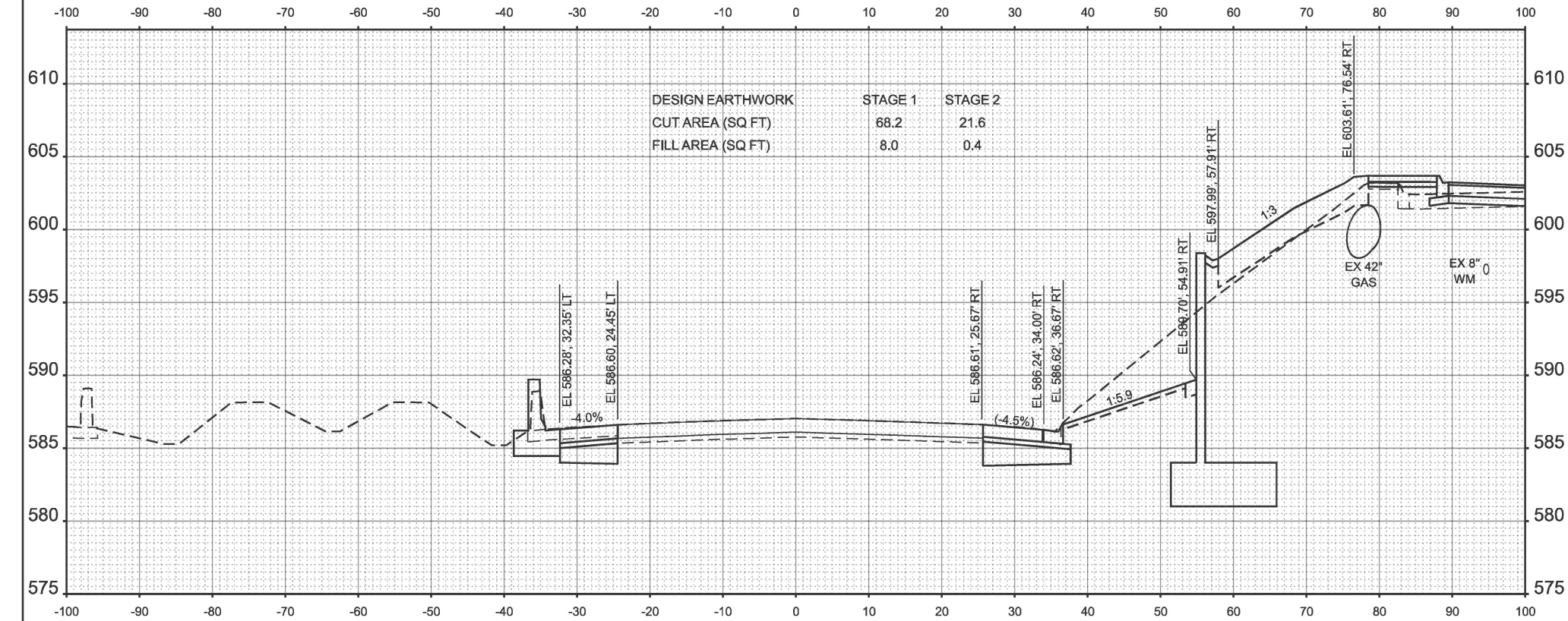
USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/27/2025	DATE - 8/29/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

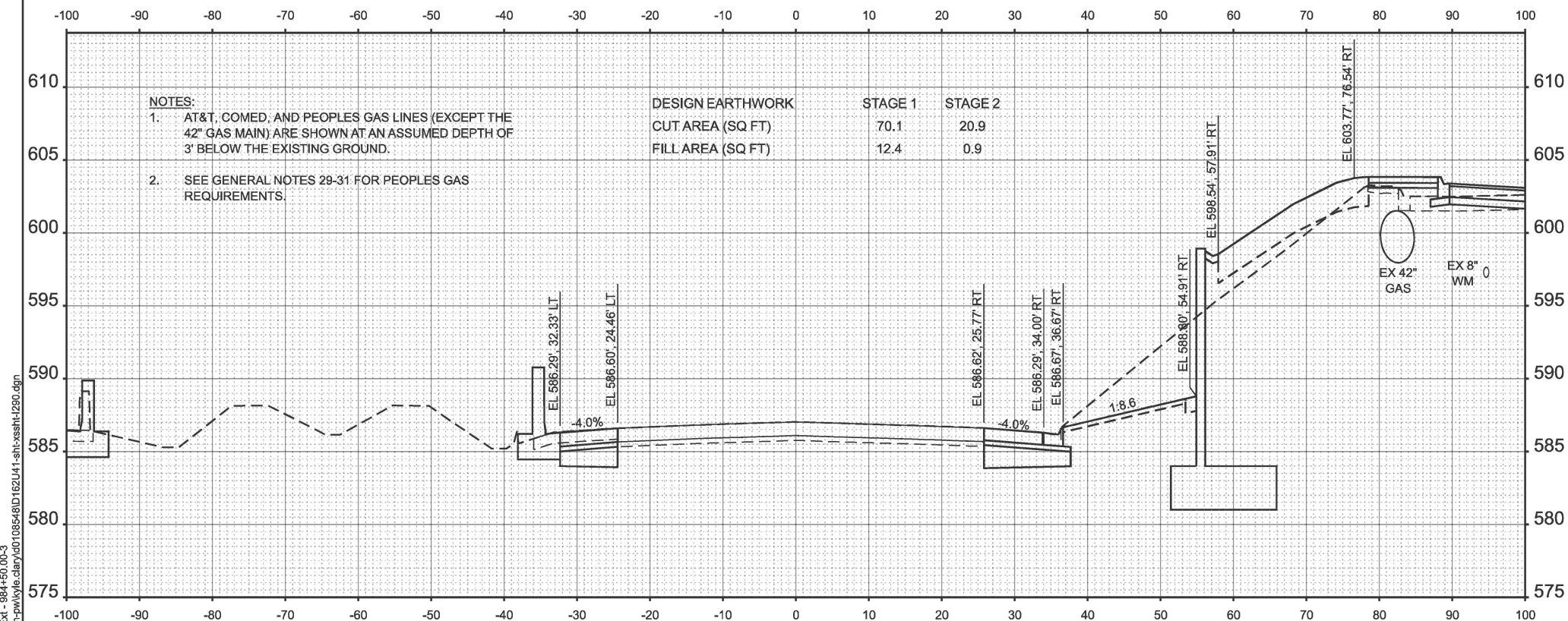
I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

SCALE: 1"=10' SHEET 23 OF 36 SHEETS STA. 984+41.83 TO STA. 984+41.83

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	270
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



STA 984+55.41
(BACK ONLY)



STA 984+50.00

MODEL: I-290 Eastbound Ext - 984+50.00-3
FILE NAME: c:\bms\civiltech\pw\kyle.dary\0106548\162\141-shp-ssht-1290.dgn



USER NAME = kdc PLOT DATE = 8/27/2025	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
	DATE - 8/29/2025	REVISED -

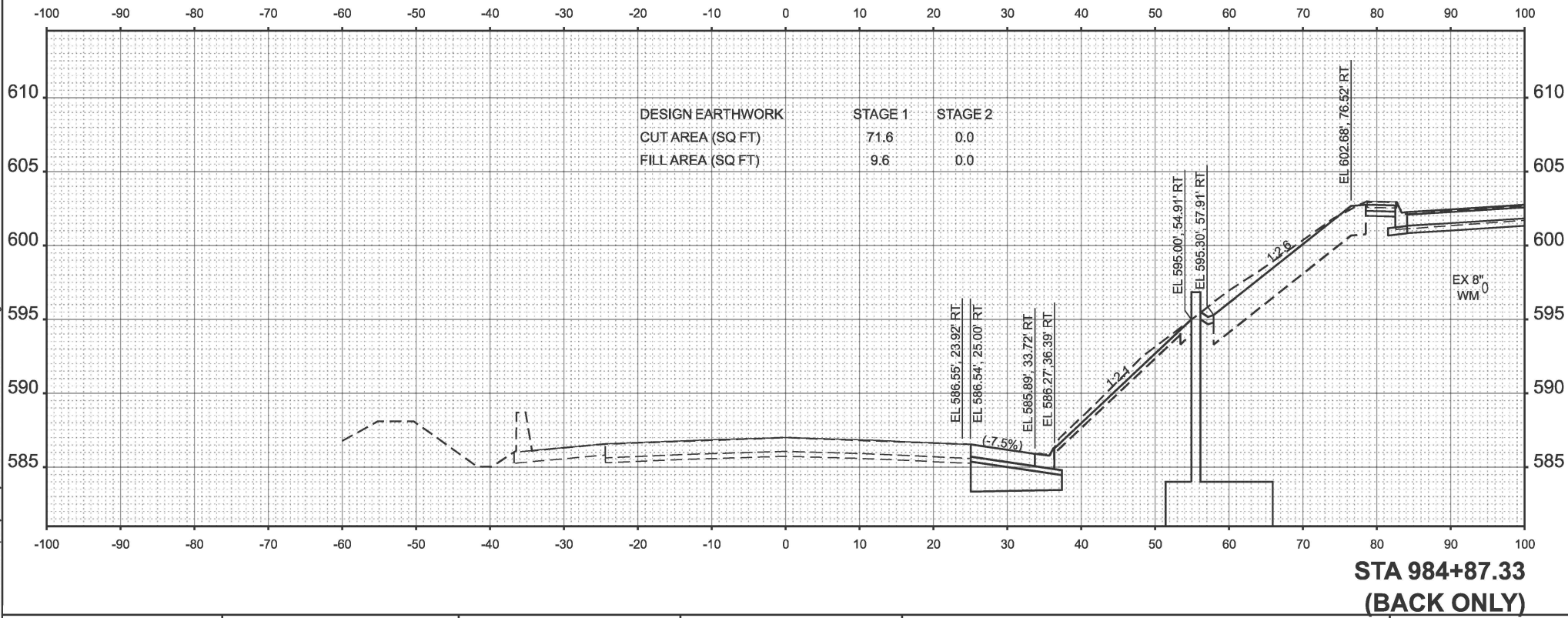
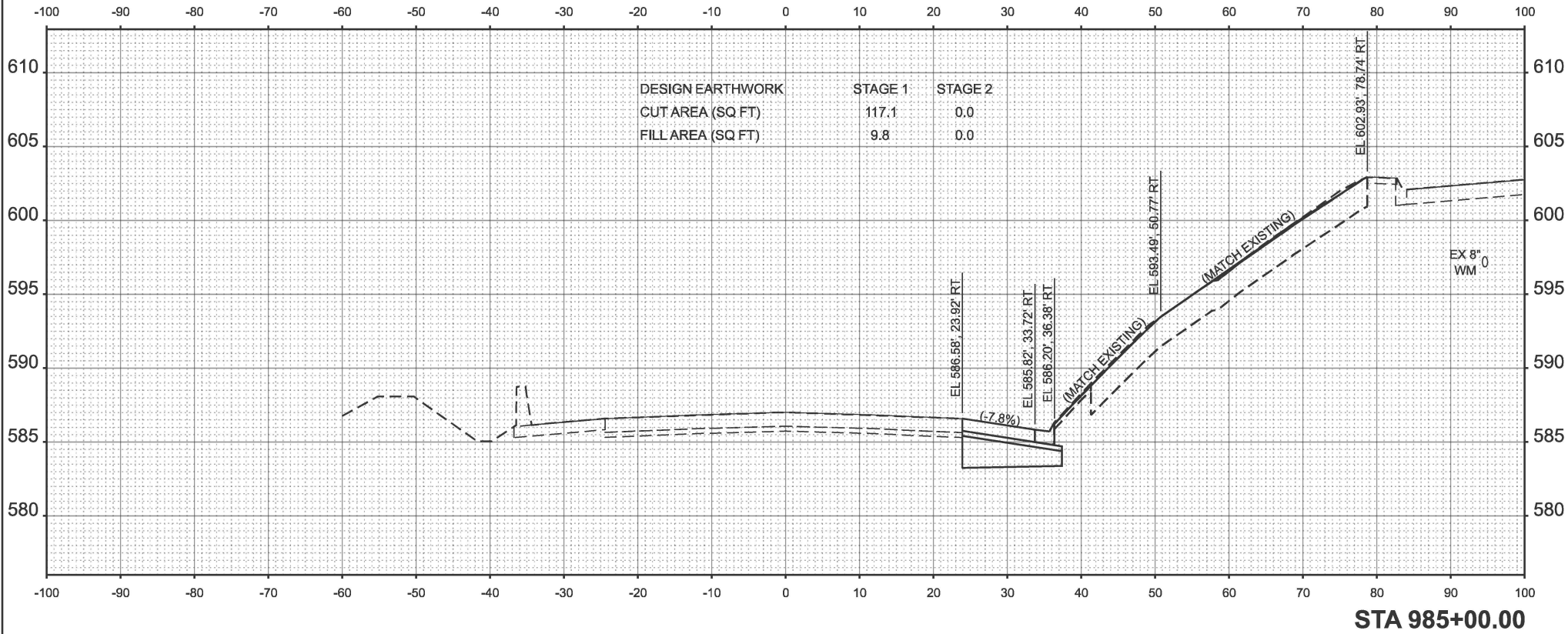
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - EASTBOUND I-290

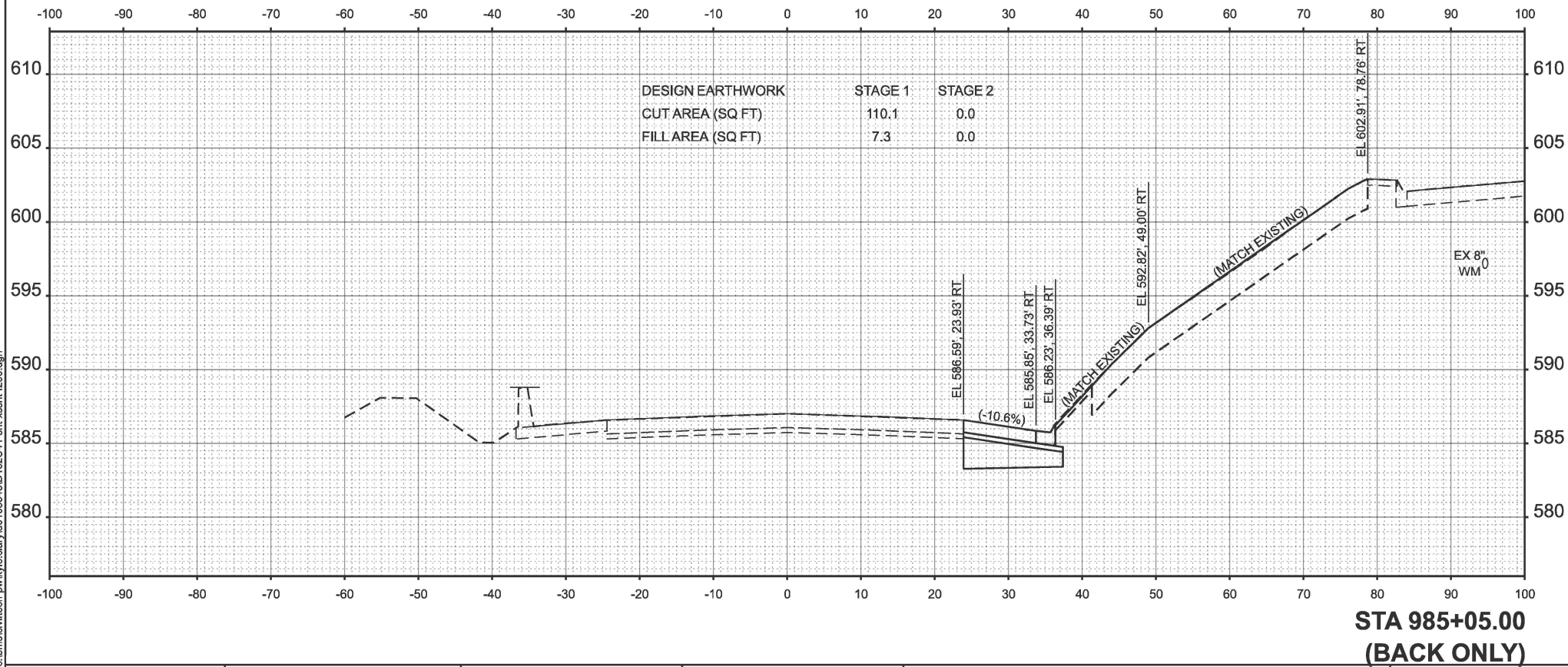
SCALE: 1"=10' SHEET 24 OF 36 SHEETS STA. 984+50.00 TO STA. 984+62.41

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	271
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Eastbound Ext - 984+87.33-3
FILE NAME: c:\bms\civiltech\pw\kyle.dary\0108548\12\12\141-sht-ssht-1290.dgn



MODEL: I-290 Eastbound Ex - 985+05.00-3
FILE NAME: c:\bms\civiltech\pw\kyle.dwg\0108548\0102141-shr-ssht-I290.dgn



	USER NAME = kdc	DESIGNED - KDC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-290 AT KEELER AVENUE CROSS SECTIONS - EASTBOUND I-290				F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - KDC	REVISED -		290	FAI 290 22 KEELER BR	COOK	283	273				
		CHECKED - DJK	REVISED -		CONTRACT NO. 62U41								
	PLOT DATE = 8/27/2025	DATE - 8/29/2025	REVISED -		SCALE: 1"=10'	SHEET 26	OF 36	SHEETS	STA. 985+05.00	TO STA. 985+05.00	ILLINOIS FED. AID PROJECT		

MODEL: I-290 Westbound Ext - 983+78.75 (Sheet)
FILE NAME: c:\bms\civiltech-pw\kyle.dary\0108548\0162\041-sht-ssht-I290.dgn



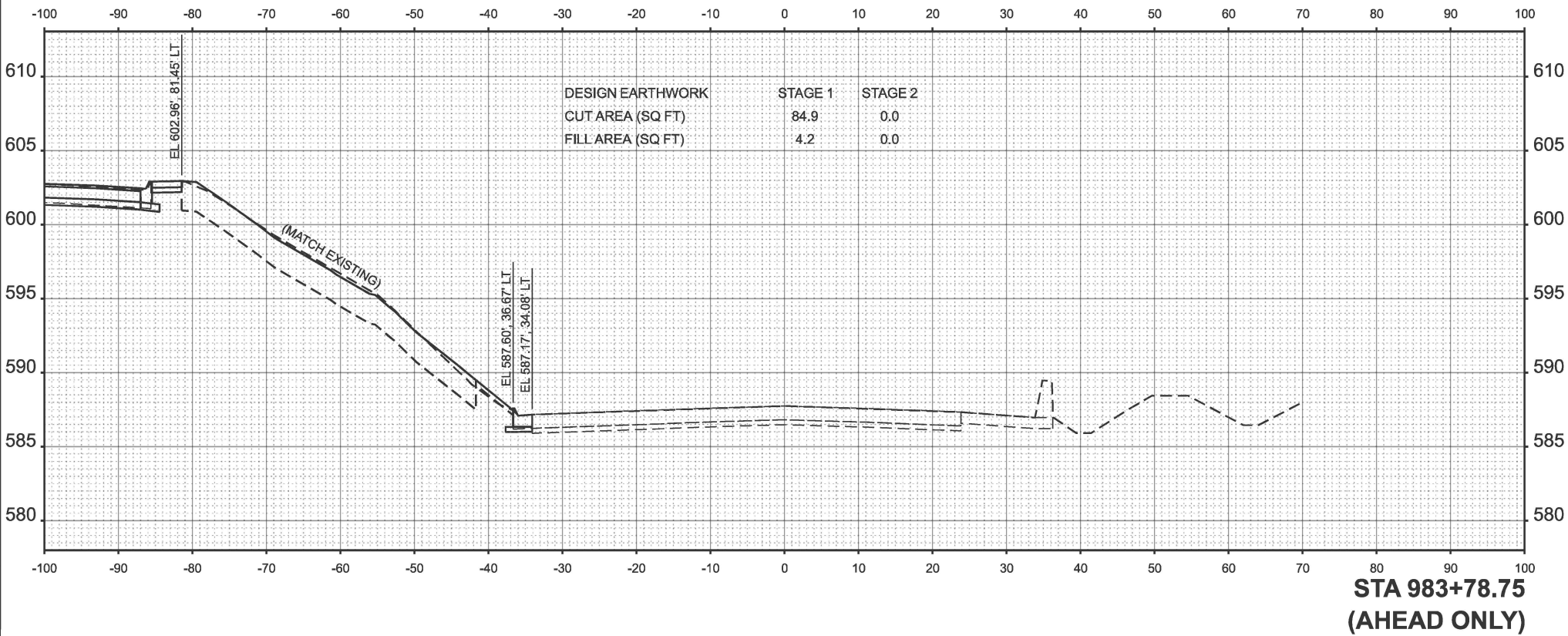
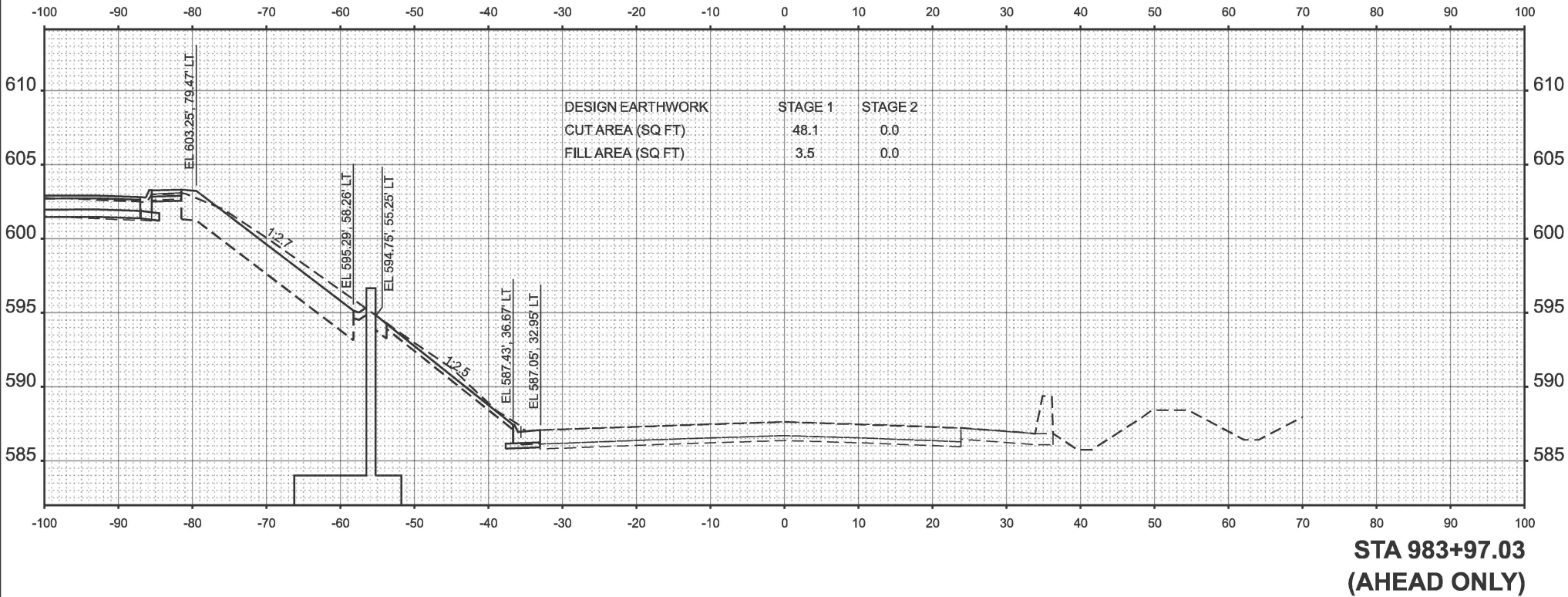
USER NAME = kdc PLOT DATE = 8/27/2025	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
	DATE - 8/29/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

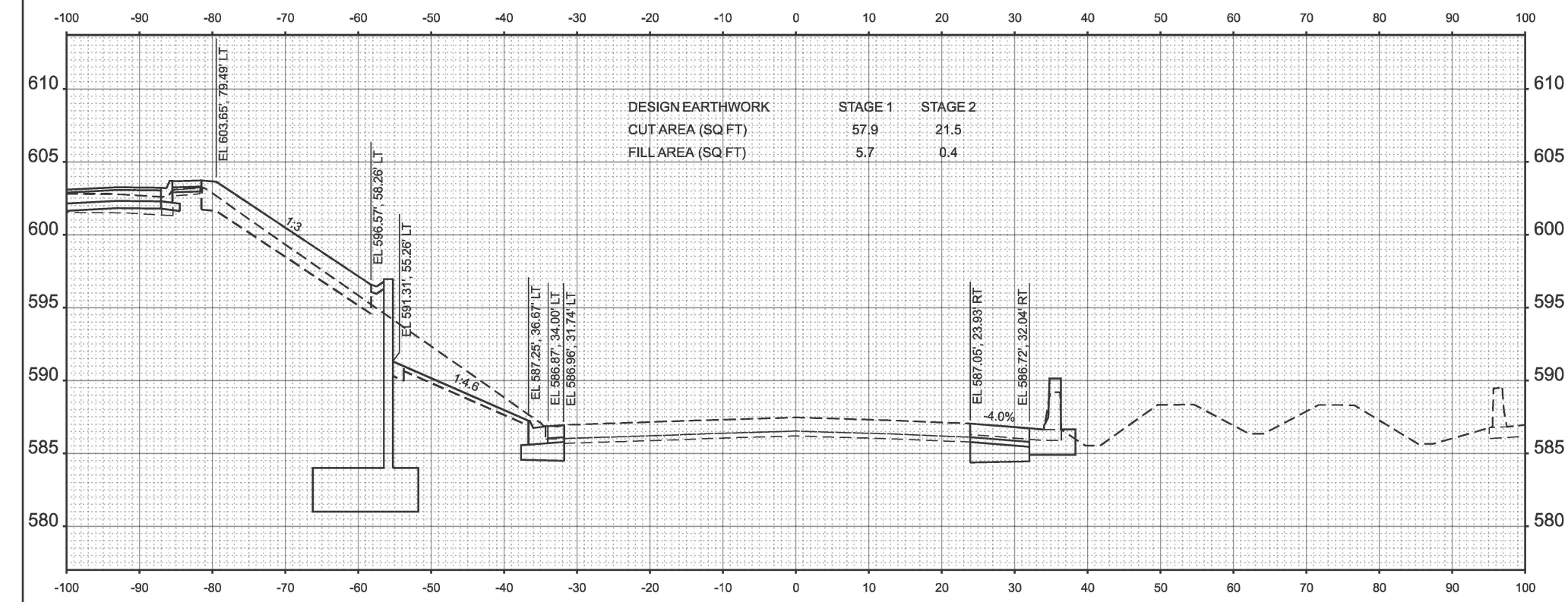
I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10' SHEET 27 OF 36 SHEETS STA. 983+78.75 TO STA. 983+97.03

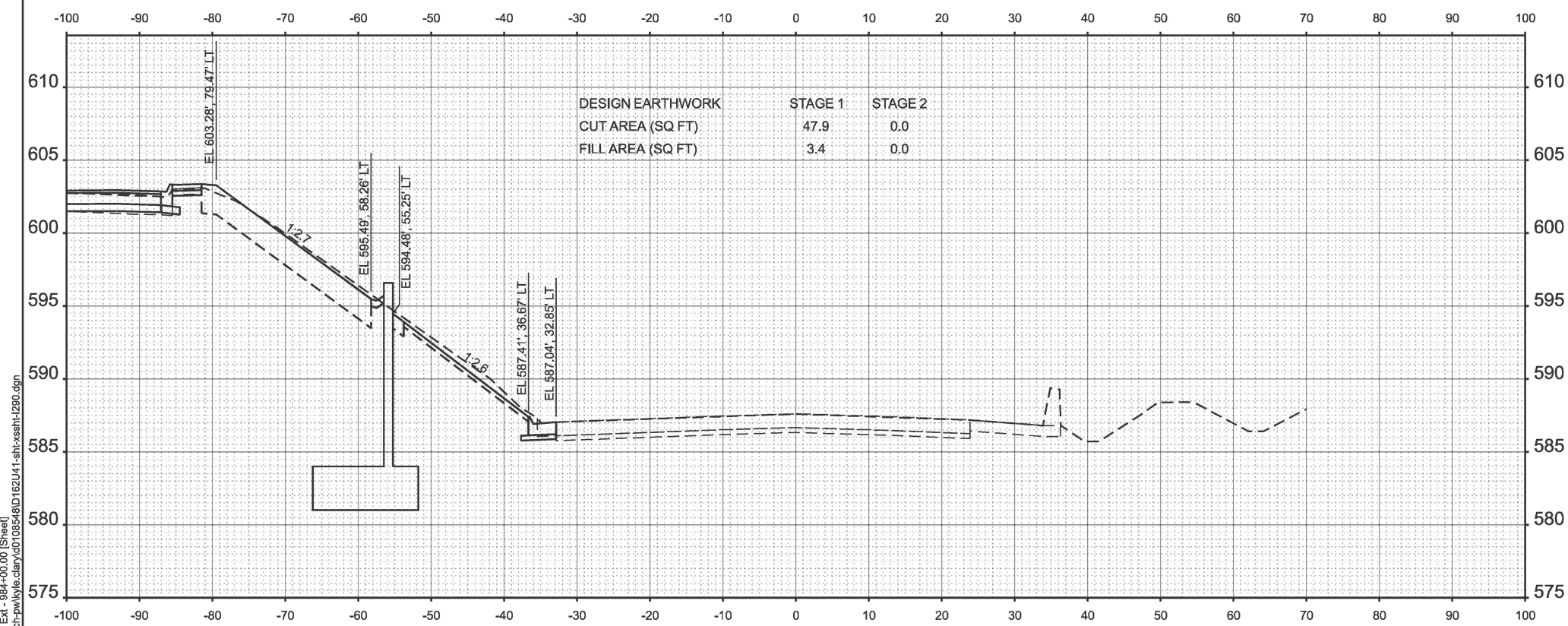
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	274
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				



MODEL: I-290 Westbound Ext - 984+00.00 (Sheet)
FILE NAME: c:\bms\civiltech-pw\kyle.dary\0106548\162U41-shp-ssht-1290.dgn



STA 984+18.95
(AHEAD ONLY)



STA 984+00.00



USER NAME = kdc PLOT DATE = 8/27/2025	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
	DATE - 8/29/2025	REVISED -

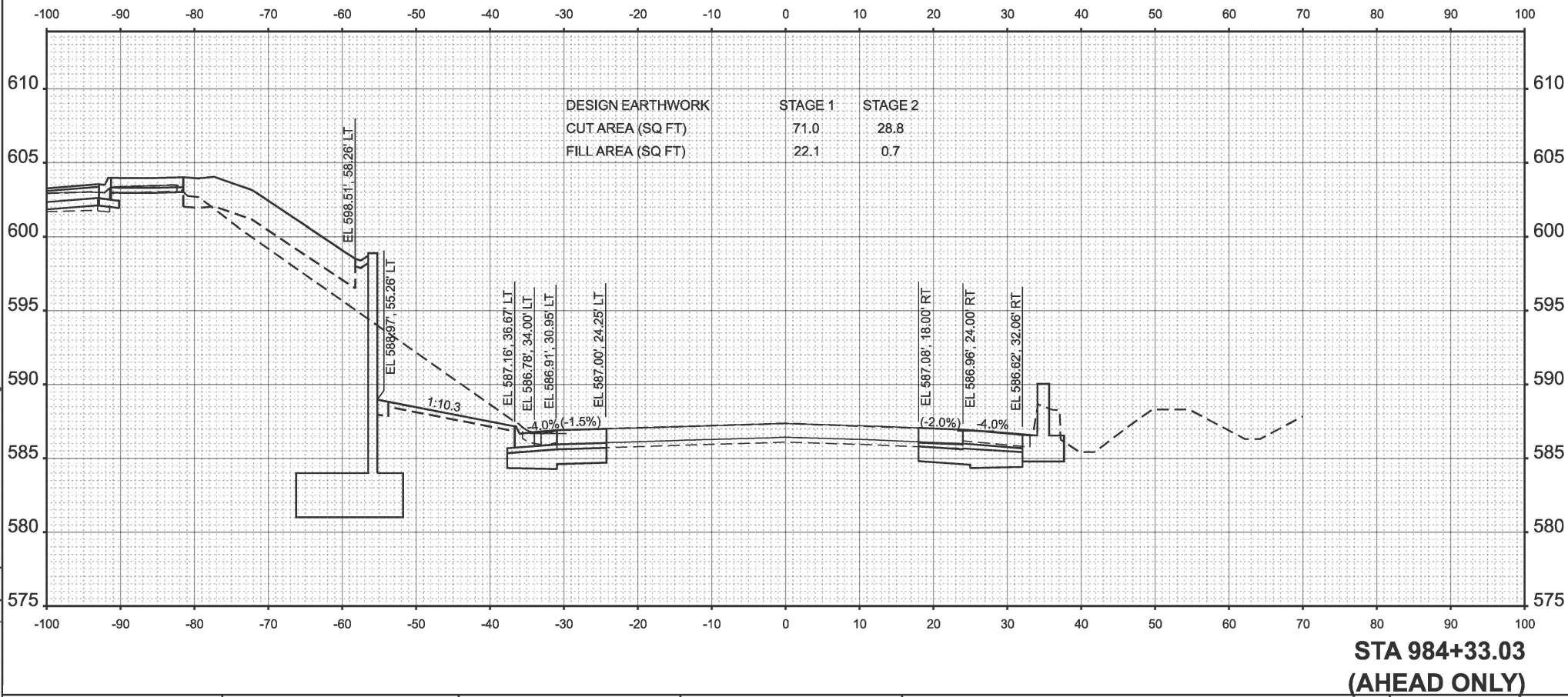
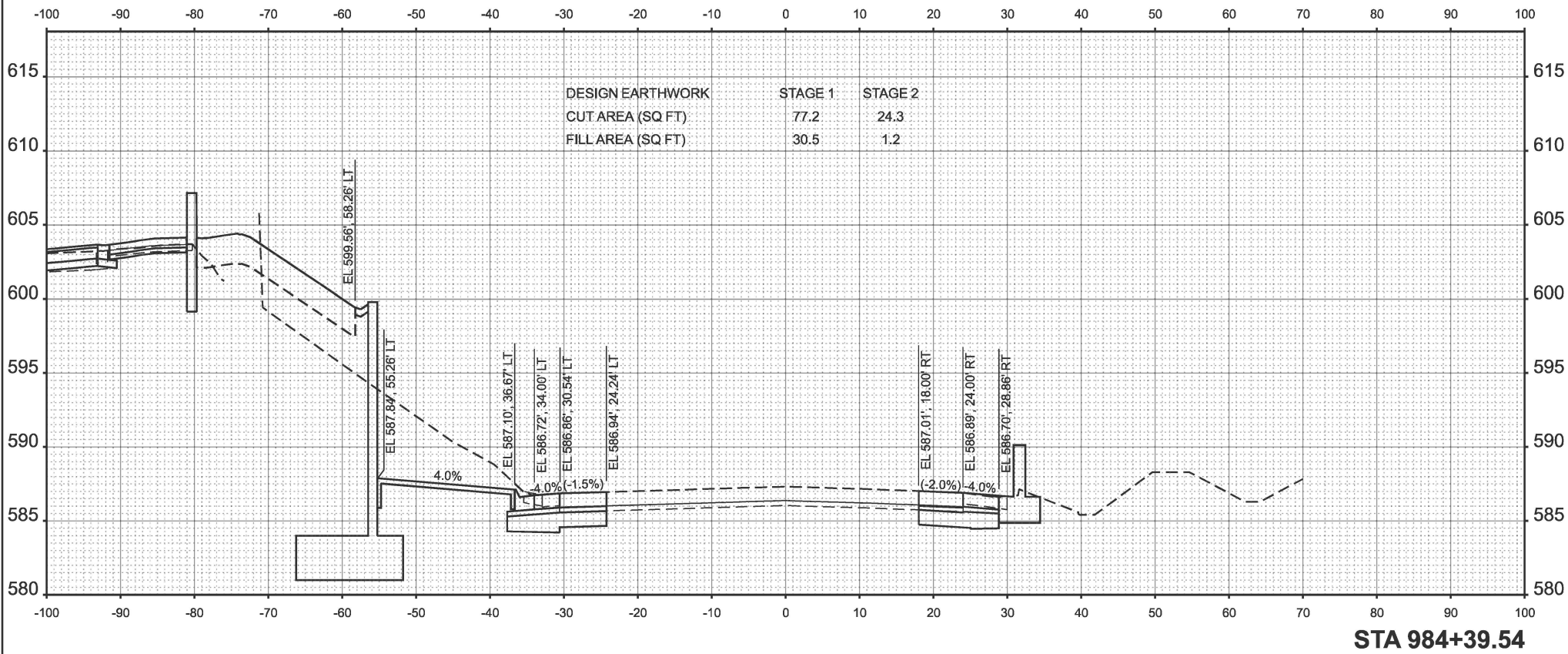
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10'
SHEET 28 OF 36 SHEETS
STA. 984+00.00 TO STA. 984+18.95

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	275
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Westbound Ext - 984+33.03 [Sheet]
FILE NAME: c:\bms\civiltech\pkw\kyle.dwg\1036548\012141-sht-ssshd1290.dgn



USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
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PLOT DATE = 8/27/2025	DATE - 8/29/2025	REVISED -

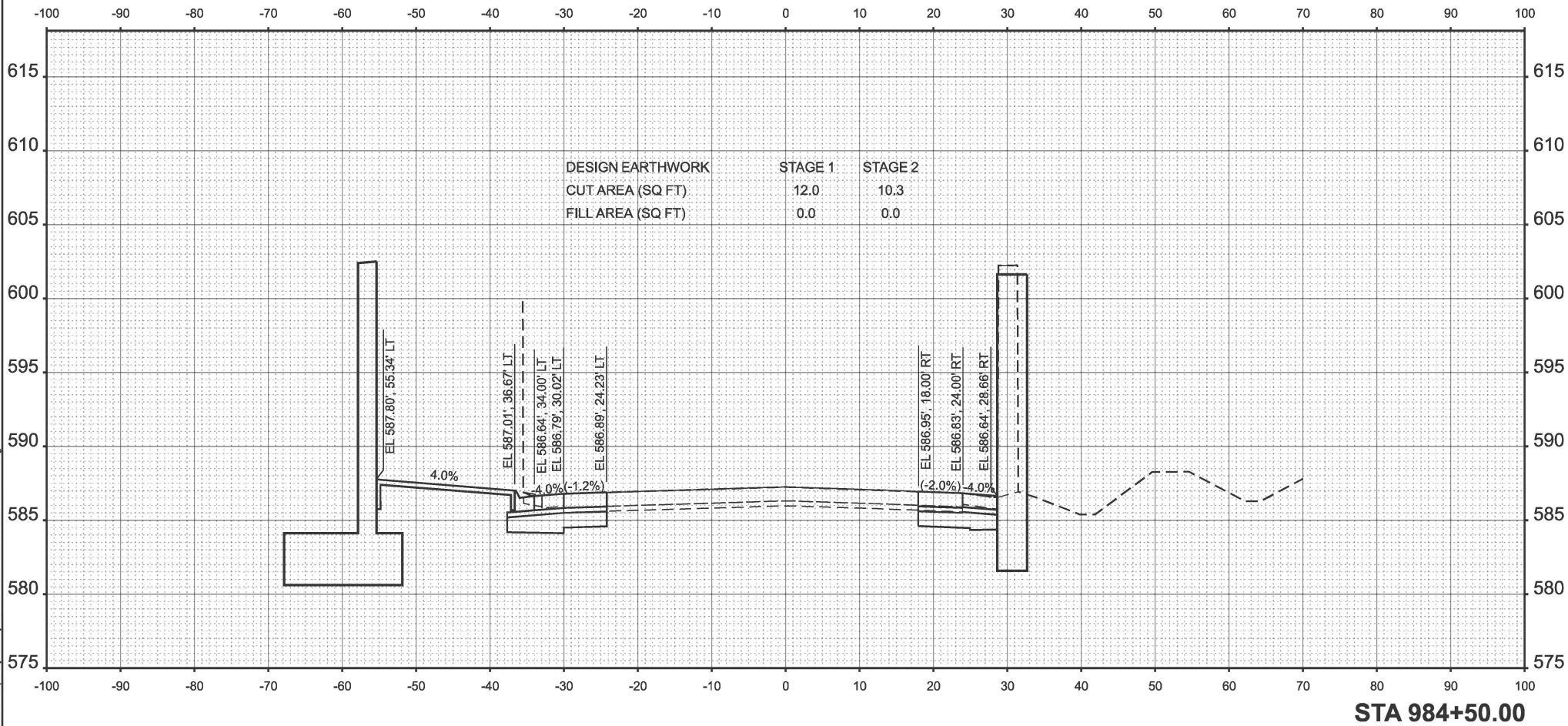
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10' SHEET 29 OF 36 SHEETS STA. 984+33.03 TO STA. 984+39.54

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	276
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Westbound Ext - 984+50.00 (Sheet)
FILE NAME: c:\bms\civiltech-pw\kyle.dwg\0106548D162U41-shp-sssh-I290.dgn



USER NAME = kdc

DESIGNED - KDC

REVISED -

DRAWN - KDC

REVIS

REVISED -

CHECKED - DJK

REVIS

REVISED -

PLOT DATE = 8/13/2025

DATE - 8/13/2025

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10'

SHEET 30

OF 36

SHEETS

STA. 984+50.00

TO STA. 984+50.00

F.A.I

RTE.

SECTION

COUNTY

TOTAL

SHEETS

SHEET

NO.

290

FAI 290 22 KEELER BR

COOK

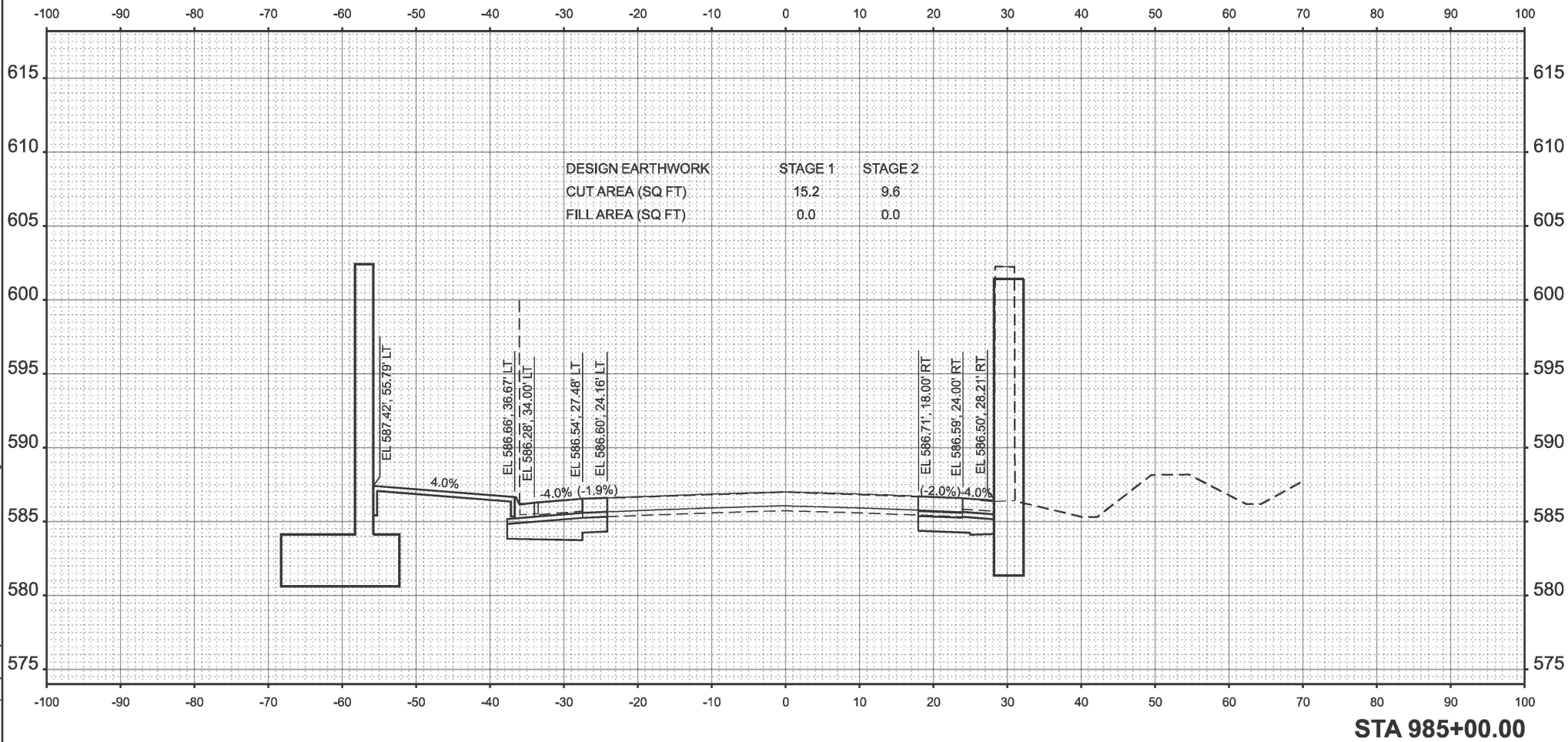
283

277

CONTRACT NO. 62U41

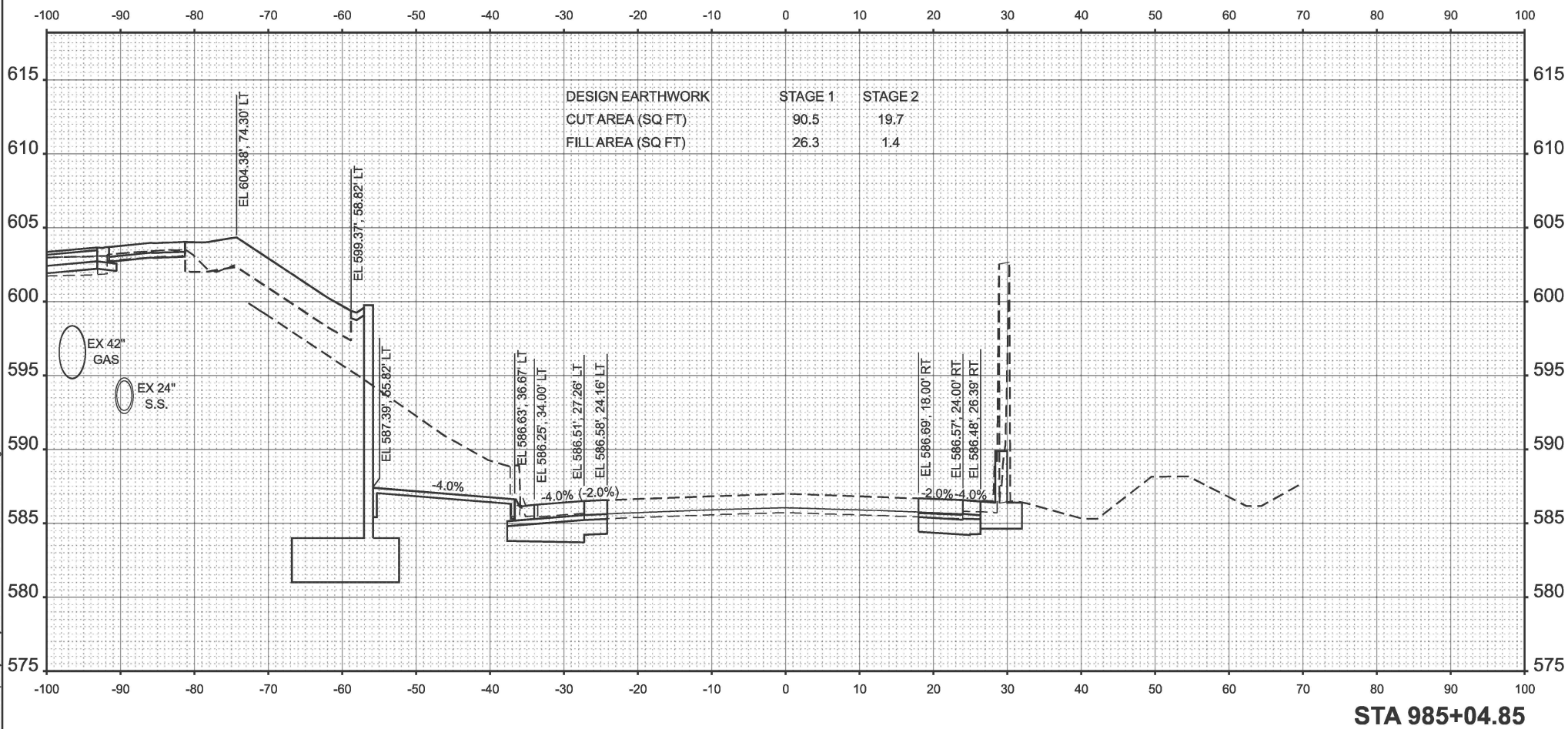
ILLINOIS FED. AID PROJECT

MODEL: I-290 Westbound Ext - 985+00.00 (Sheet)
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	USER NAME = kdc	DESIGNED - KDC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-290 AT KEELER AVENUE CROSS SECTIONS - WESTBOUND I-290			F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - KDC	REVISED -					290	FAI 290 22 KEELER BR	COOK	283	278
		CHECKED - DJK	REVISED -		CONTRACT NO. 62U41							
	PLOT DATE = 8/13/2025	DATE - 8/13/2025	REVISED -		SCALE: 1"=10'	SHEET 31	OF 36	SHEETS	STA. 985+00.00	TO STA. 985+00.00	ILLINOIS FED. AID PROJECT	

MODEL: I-290 Westbound Ext - 985+04.85 (Sheet)
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USER NAME = kdc

DESIGNED - KDC

REVISED -

DRAWN - KDC

REVIS

REVISED -

CHECKED - DJK

REVIS

REVISED -

PLOT DATE = 8/27/2025

DATE - 8/29/2025

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10'

SHEET 32

OF 36

SHEETS

STA. 985+04.85

TO STA. 985+04.85

F.A.I
RTE.

290

SECTION

FAI 290 22 KEELER BR

COUNTY

COOK

TOTAL
SHEETS

283

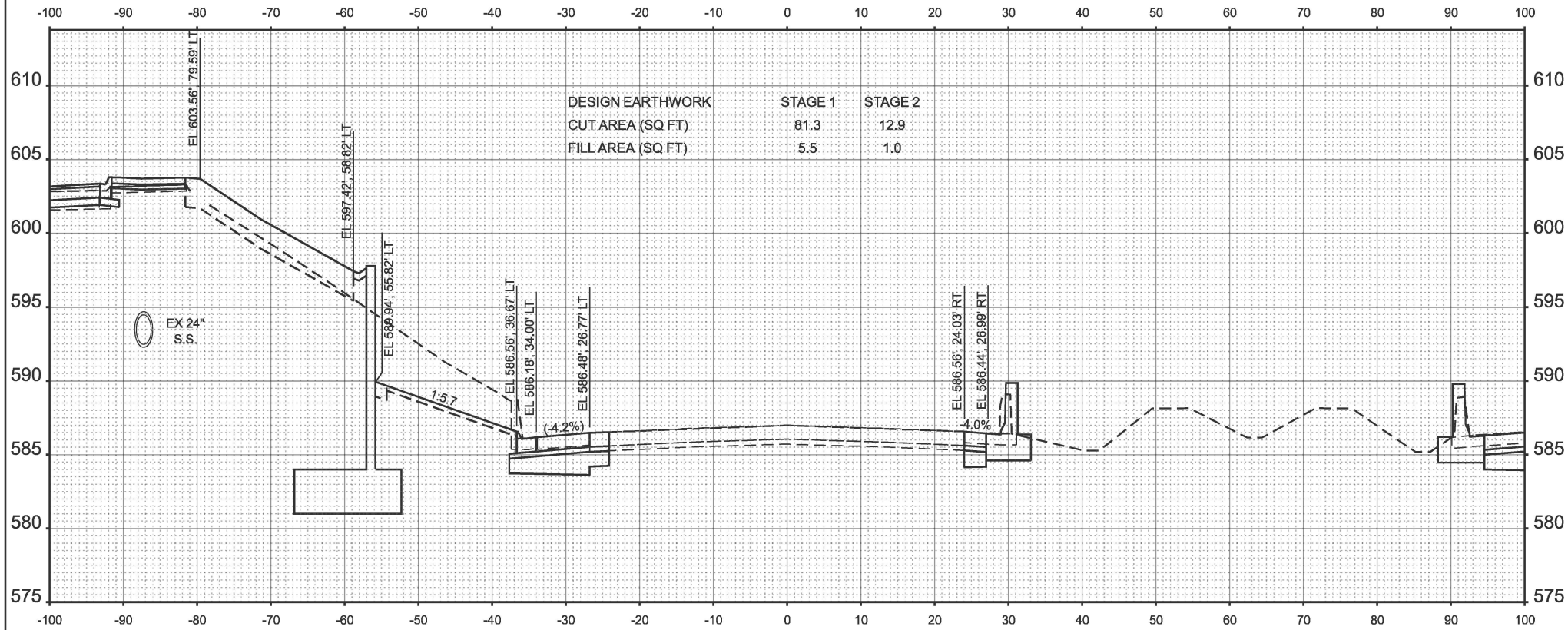
SHEET
NO.

279

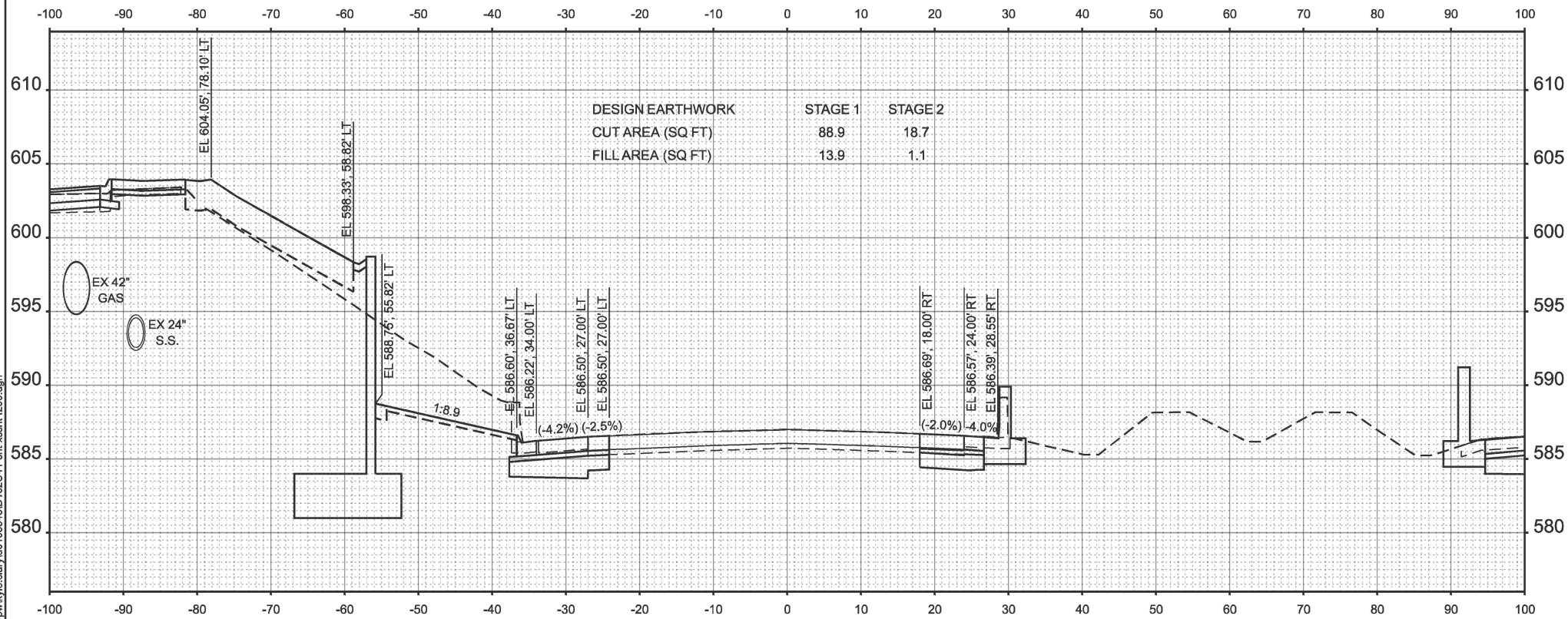
CONTRACT NO. 62U41

ILLINOIS FED. AID PROJECT

MODEL: I-290 Westbound Ext - 985+13.00 (Sheet)
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STA 985+20.12
(BACK ONLY)



STA 985+13.00
(BACK ONLY)



USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 6/30/2025	DATE - 8/29/2025	REVISED -

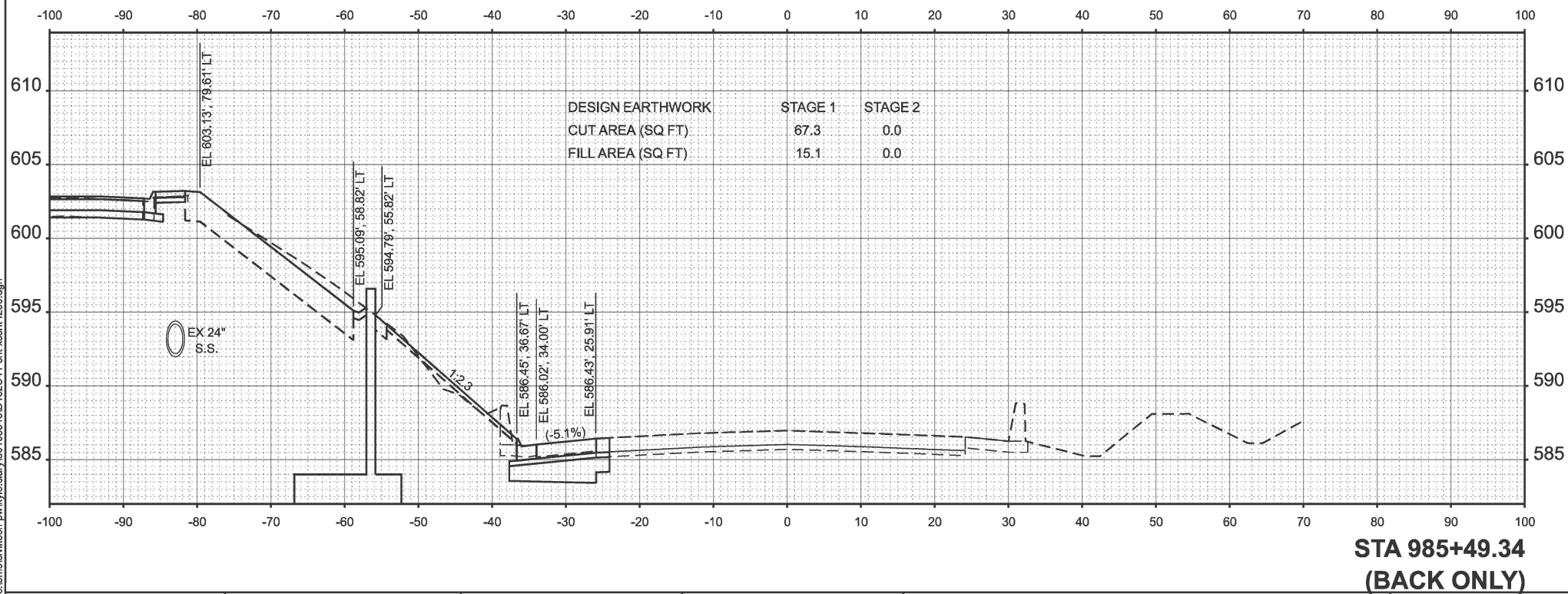
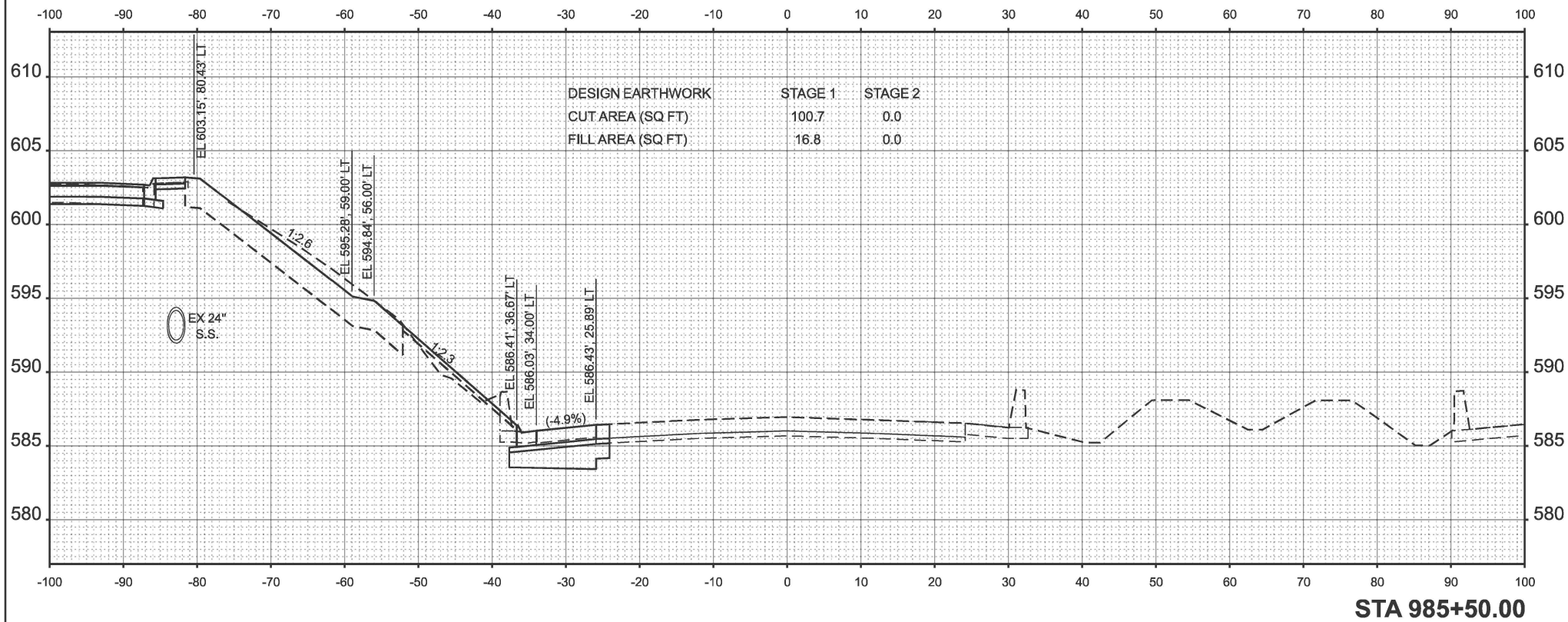
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10'
SHEET 33 OF 36 SHEETS
STA. 985+13.00 TO STA. 985+27.12

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	280
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Westbound Ext - 985+49.34 (Sheet)
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USER NAME = kdc

DESIGNED - KDC

REVISED -

DRAWN - KDC

REVISED -

CHECKED - DJK

REVISED -

PLOT DATE = 8/29/2025

DATE - 8/29/2025

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10'

SHEET 34

OF 36

SHEETS

STA. 985+49.34

TO STA. 985+50.00

F.A.I
RTE.

290

SECTION

FAI 290 22 KEELER BR

COUNTY

COOK

TOTAL
SHEETS

283

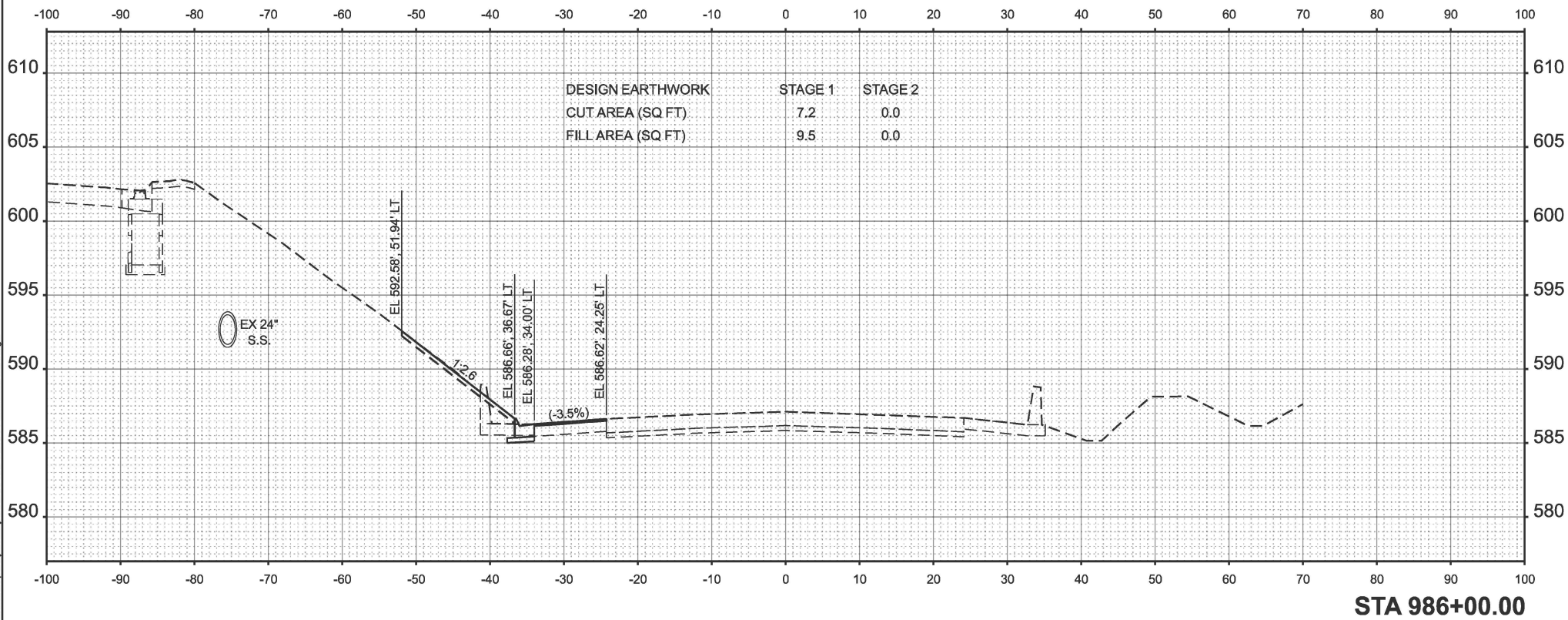
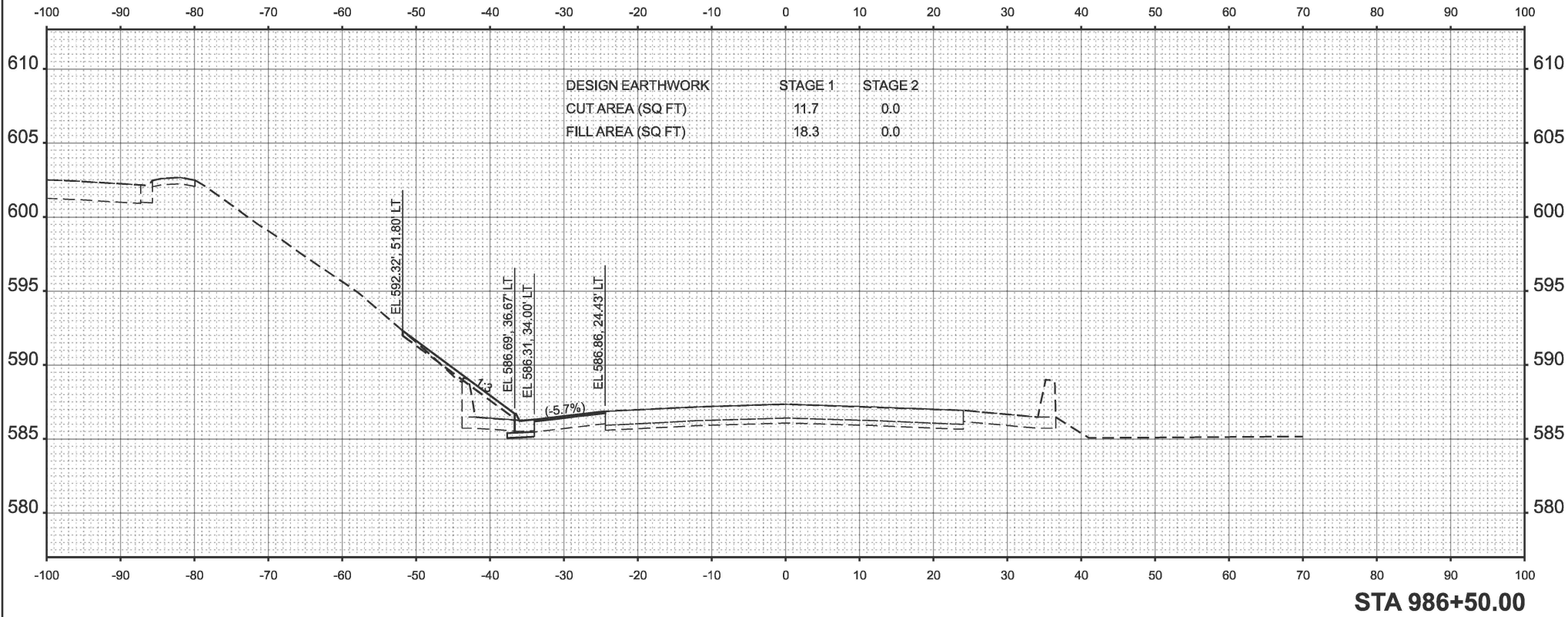
SHEET
NO.

281

CONTRACT NO. 62U41

ILLINOIS FED. AID PROJECT

MODEL: I-290 Westbound Ext - 986+00.00 (Sheet)
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USER NAME = kdc	DESIGNED - KDC	REVISED -
	DRAWN - KDC	REVISED -
	CHECKED - DJK	REVISED -
PLOT DATE = 8/29/2025	DATE - 8/29/2025	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10'
SHEET 35 OF 36 SHEETS
STA. 986+00.00 TO STA. 986+50.00

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	282
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

MODEL: I-290 Westbound Ext - 986+60.10 (Sheet)
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USER NAME = kdc	DESIGNED -	KDC	REVISED -
	DRAWN -	KDC	REVISED -
	CHECKED -	DJK	REVISED -
	DATE -	8/29/2025	REVISED -
PLOT DATE = 8/27/2025		DATE -	8/29/2025

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-290 AT KEELER AVENUE
CROSS SECTIONS - WESTBOUND I-290

SCALE: 1"=10' SHEET 36 OF 36 SHEETS STA. 986+60.10 TO STA. 986+71.11

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
290	FAI 290 22 KEELER BR	COOK	283	283
CONTRACT NO. 62U41				
ILLINOIS FED. AID PROJECT				

