

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. SHEETS	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	1
ILLINOIS CONTRACT NO. 80B29				

* 21 + 5 = 26 TOTAL SHEETS

D-91-191-25



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN THE VILLAGE
OF LIBERTYVILLE

PROPOSED HIGHWAY PLANS

F.A.P. ROUTE 344: US 45 / LAKE ST.
TEMPEL DR. TO RAMP TO IL 137
SECTION: 2025-2010-RS
PROJECT: NHPP-ZTPQ(221)
DESIGNED OVERLAY
LAKE COUNTY

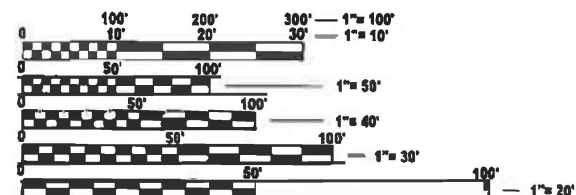
C-91-265-25

TRAFFIC DATA:

2023 ADT - 23100 TO 25900 VPD

SPEED LIMIT - 45 MPH

FUNCTIONAL CLASS: MINOR ARTERIAL



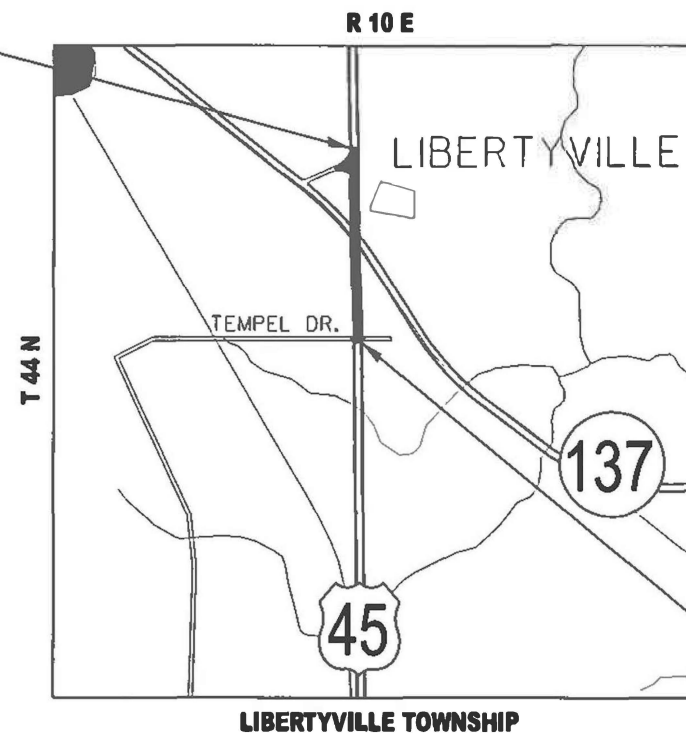
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 80B29

PROJECT ENDS
STA 50+31



PROJECT BEGINS
STA 25+90

GROSS AND NET LENGTH OF PROJECT - 2441 FEET - .46 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Aug 13th 2020
2020 IR

REGIONAL ENGINEER

October 3, 2025
See E.A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

October 3, 2025
Harry
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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STATE STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-07	FRAMES AND LIDS, TYPE 1
604086-05	FRAME AND GRATE, TYPE 23
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE 2L, 2W, SHORT TIME OPERATIONS
701411-09	LANE CLOSURE MULTILANE ENTRANCE OR EXIT RAMP 45 MPH OR MORE
701426-09	LANE CLOSURE MULTILANE INTERMITTENT OR MOVING OPERATIONS 45 MPH OR MORE
701501-06	URBAN LANE CLOSURE 2L, 2W, UNDIVIDED
701502-09	URBAN LANE CLOSURE 2L, 2W, WITH BI-DIRECTIONAL LEFT TURN LANE
701601-09	URBAN LANE CLOSURE MULTILANE 1W, 2W WITH NON-TRANSVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-10	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES

- 1) BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).
- 2) TEN (10) FOOT TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- 3) THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPAINES AND THE VILLAGE OF LIBERTYVILLE.
- 4) THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT INCLUDING THE ROADSIDE DEVELOPMENT UNIT.
- 5) ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- 6) ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- 7) ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 8) LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)], WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 9) DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 10) IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 11) FRAMES AND GRATES ADJUSTMENTS OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.
- 12) BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- 13) WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCH (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THEN SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h), WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM OF (1:3).
- 14) THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR FOR ARTERIALS AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- 15) PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)
- 16) THE RESIDENT ENGINEER SHALL CONTACT MR. FADI SULTAN, AREA TRAFFIC ENGINEER, FADI.SULTAN@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- 17) THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
- 18) THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING LANDSCAPE AND FORESTRY WORK FOR LAYOUT.
- 19) PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR SHORT-TERM PAVEMENT MARKING ON ALL FINAL SURFACES.
- 20) ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- 21) THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
- 22) OVERNIGHT LANE CLOSURE SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- 23) TEMPORARY PAVEMENT MARKINGS OR SHORT TERM PAVEMENT MARKINGS ON INTERMEDIATE SURFACES SHALL NOT BE REMOVED, UNLESS DIRECTED BY THE ENGINEER.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
FAP 344 (US 45 / LAKE ST.)--TEMPEL DR. TO RAMP TO IL 137

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	2
CONTRACT NO. 80B29				
		ILLINOIS	FED. AID PROJECT	

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SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY					
					80% FED 20% STATE	100% STATE				
	Code No.	Item	Unit	Total Quantity	0005	0005				
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	62	62					
	25200110	SODDING, SALT TOLERANT	SQ YD	62	62					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	13065	13065					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	6458	6458					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	30	30					
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	225	225					
	40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	2185	2185					
	40604072	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	1927	1927					
	42001300	PROTECTIVE COAT	SQ YD	72	72					
	44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	19355	19355					
	44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	1406	1406					
	44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	828	828					
	44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	497	497					
	44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	331	331					
	48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	57	57					
	60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5					
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	1	1					
	67100100	MOBILIZATION	L SUM	1	1					
	-									
	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
	-									
		USER NAME = Phillip.Steed	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION					
			DRAWN -	REVISED -						
			CHECKED -	REVISED -						
		PLOT DATE = 8/20/2025	DATE -	REVISED -						

SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY					
					80% FED 20% STATE	100% STATE				
	Code No.	Item	Unit	Total Quantity	0005	0005				
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1352	1352					
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	226	226					
	70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	219	219					
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	14376	14376					
	70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	1152	1152					
	70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	576	576					
	70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	624	624					
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	444	444					
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73					
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4792	4792					
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	384	384					
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	192	192					
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	208	208					
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	148	148					
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	172	172					
*	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	146	146					
	X2020110	GRADING AND SHAPING SHOULDERS	UNIT	15	15					
	X4060995	TEMPORARY RAMP (SPECIAL)	SQ YD	502	502					
	X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	534	534					
	X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	200	200					
* SPECIALTY ITEMS # NON-PARTICIPATING ITEMS										
ILLINOIS TRANSPORTATION		SUMMARY OF QUANTITIES FP 344 (US 45 / LAKE ST.)-TEMPEL DR. TO RAMP TO IL 137			F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
					344	2025-2010-RS		LAKE	21	3
					CONTRACT NO. 80B29					
SCALE:		SHEET	OF	SHEETS	STA.	TO STA.		ILLINOIS FED. AID PROJECT		

[illegible][illegible]

LEGEND:

- 1

EXISTING P.C.C. PAVEMENT, PLUS OR MINUS 9"
- 2

EXISTING HOT-MIX ASPHALT SURFACE, PLUS OR MINUS 6" (AFTER MILLING)
- 3

EXISTING CORRUGATED MEDIAN
- 4

EXISTING BARRIER MEDIAN
- 5

EXISTING LANDSCAPE MEDIAN
- 6

EXISTING AGGREGATE SHOULDER
- 7

EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- 8

EXISTING STABILIZED SUB-BASE
- 9

PROPOSED GRADING AND SHAPING SHOULDERS
- 10

PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- 11

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
- 12

PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- 13

PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- 14

PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1"
- 15

PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', IL-9.5, N70, 1 3/4"
- R

CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
TEMPORARY RAMP		
TEMPORARY RAMP (HMA BINDER, IL-19MM)	4% @ 70 GYR.	QC / QA
PATCHING		
CLASS "D" PATCHES (HMA BINDER, IL-19MM)	4% @ 70 GYR.	QC / QA
PAVEMENT RESURFACING (DESIGNED OVERLAY)		
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"	4% @ 70 GYR.	QCP
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', N70, 1 3/4"	4% @ 70 GYR.	QCP
QMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (QCP); QUALITY CONTROL / QUALITY ASSURANCE (QC / QA); PAY FOR PERFORMANCE (PFP)		

NOTES:

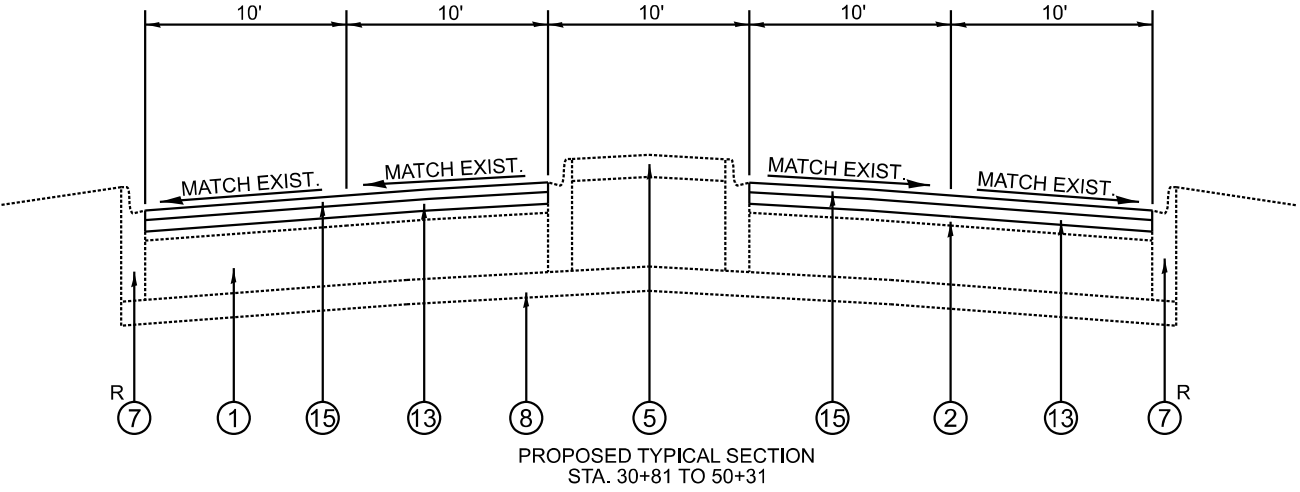
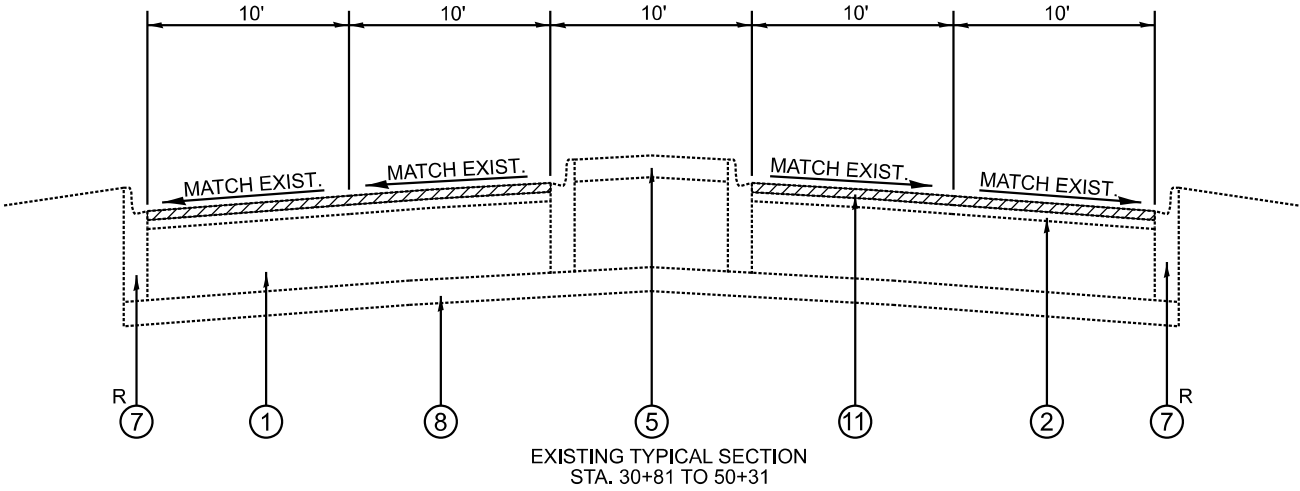
- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS./SQ. YD./ IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIAL SPECIFICATIONS.
- THE MILLING SHALL BE DONE PRIOR TO PATCHING
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70.

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		DRAWN -	REVISED -							344	2025-2010-RS	LAKE	21	5
		CHECKED -	REVISED -							CONTRACT NO. 80B29				
	PLOT DATE = 9/30/2025	DATE -	REVISED -		SCALE:					SHEET	OF	SHEETS	STA.	TO STA.
						ILLINOIS FED. AID PROJECT								

LEGEND:

- ①
- EXISTING P.C.C. PAVEMENT, PLUS OR MINUS 9"
- ②
- EXISTING HOT-MIX ASPHALT SURFACE, PLUS OR MINUS 6" (AFTER MILLING)
- ③
- EXISTING CORRUGATED MEDIAN
- ④
- EXISTING BARRIER MEDIAN
- ⑤
- EXISTING LANDSCAPE MEDIAN
- ⑥
- EXISTING AGGREGATE SHOULDER
- ⑦
- EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑧
- EXISTING STABILIZED SUB-BASE
- ⑨
- PROPOSED GRADING AND SHAPING SHOULDERS
- ⑩
- PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑪
- PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
- ⑫
- PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- ⑬
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑭
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1"
- ⑮
- PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', IL-9.5, N70, 1 3/4"
- R
- CURB AND GUTTER REMOVAL AND REPLACEMENT
(AS DIRECTED BY THE ENGINEER)



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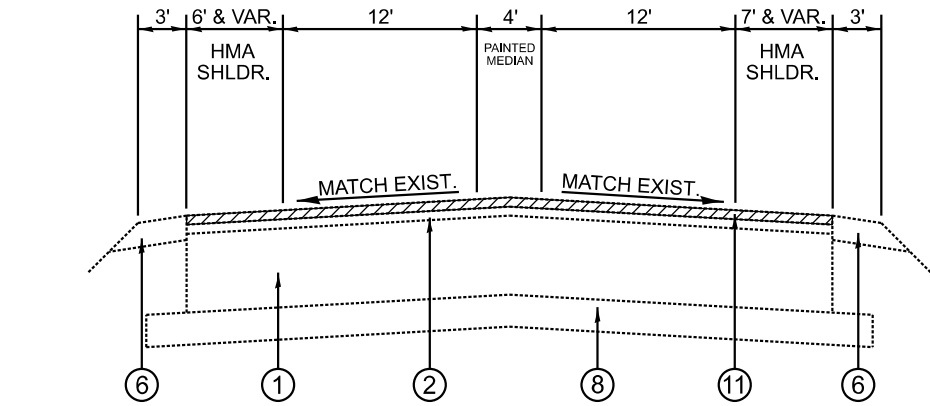
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PLOT DATE = 9/30/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

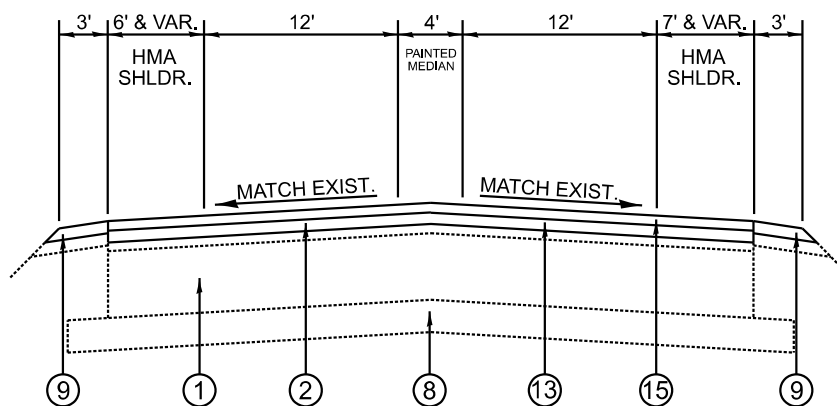
EXISTING AND PROPOSED TYPICAL SECTIONS
FAP 344 (US 45 / LAKE ST.)-TEMPEL DR. TO RAMP TO IL 137

SCALE: SHEET OF SHEETS STA. TO STA.

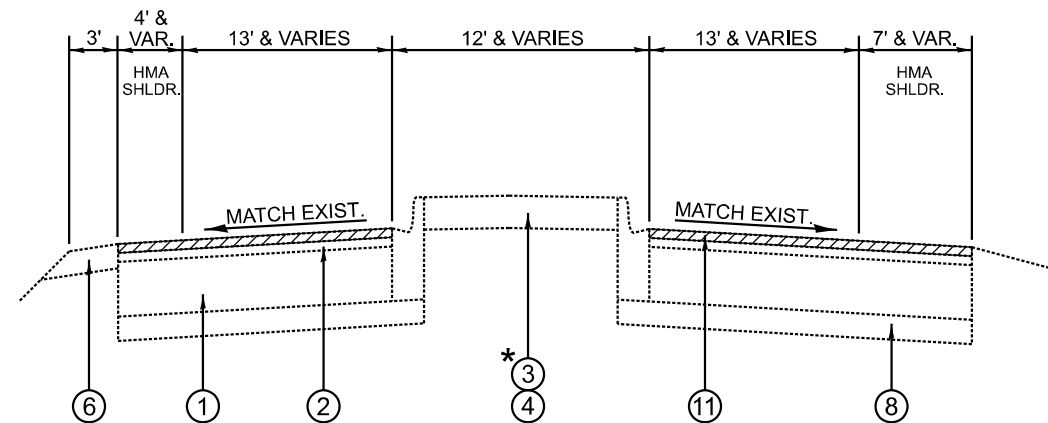
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	COOK	21	6
CONTRACT NO. 80B29				
ILLINOIS FED. AID PROJECT				



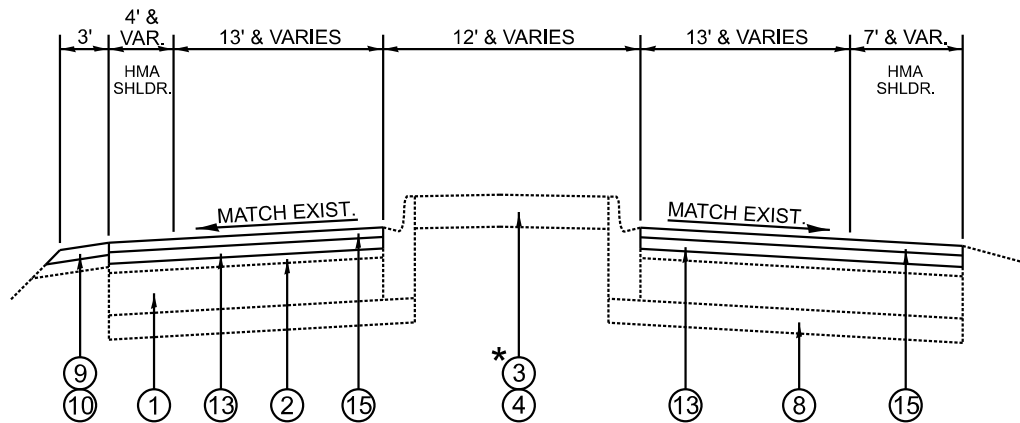
EXISTING TYPICAL SECTION
U.S. 45 RAMP "A"
STA. 0+25 TO 5+60



PROPOSED TYPICAL SECTION
U.S. 45 RAMP "A"
STA. 0+25 TO 5+60



EXISTING TYPICAL SECTION
U.S. 45 RAMP "A"
STA. 5+60 TO 7+32



PROPOSED TYPICAL SECTION
U.S. 45 RAMP "A"
STA. 5+60 TO 7+32

*** LEGEND:**
BARRIER MEDIAN
STA. 6+86 TO STA. 7+32
CORRUGATED MEDIAN
STA. 5+60 TO STA. 6+86

*** LEGEND:**
BARRIER MEDIAN
STA. 6+86 TO STA. 7+32
PAINTED MEDIAN
STA. 5+60 TO STA. 6+86

LEGEND:

- ① EXISTING P.C.C. PAVEMENT, PLUS OR MINUS 9"
- ② EXISTING HOT-MIX ASPHALT SURFACE, PLUS OR MINUS 6" (AFTER MILLING)
- ③ EXISTING CORRUGATED MEDIAN
- ④ EXISTING BARRIER MEDIAN
- ⑤ EXISTING LANDSCAPE MEDIAN
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑩ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
- ⑫ PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- ⑬ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑭ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1"
- ⑮ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', IL-9.5, N70, 1 3/4"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

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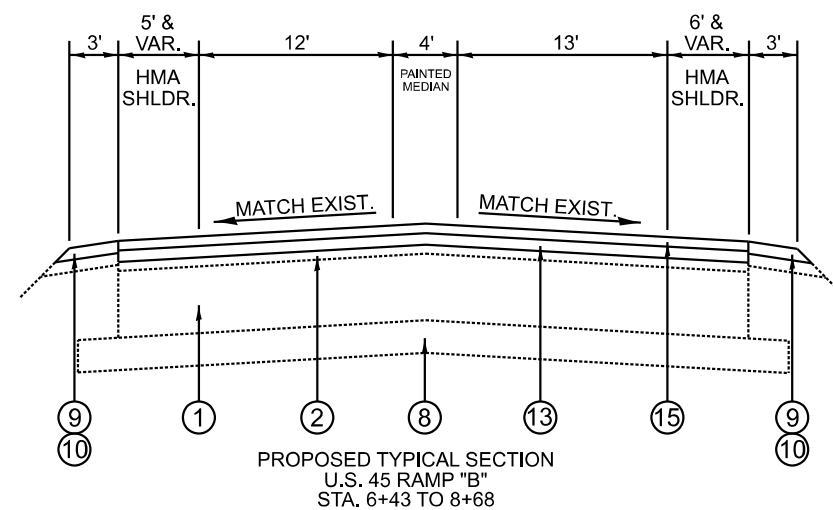
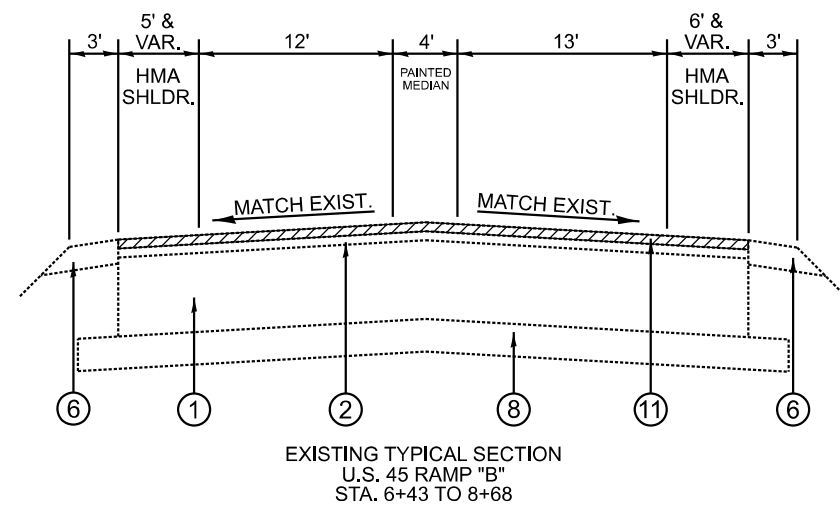
USER NAME = Phillip.Steed	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 9/30/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

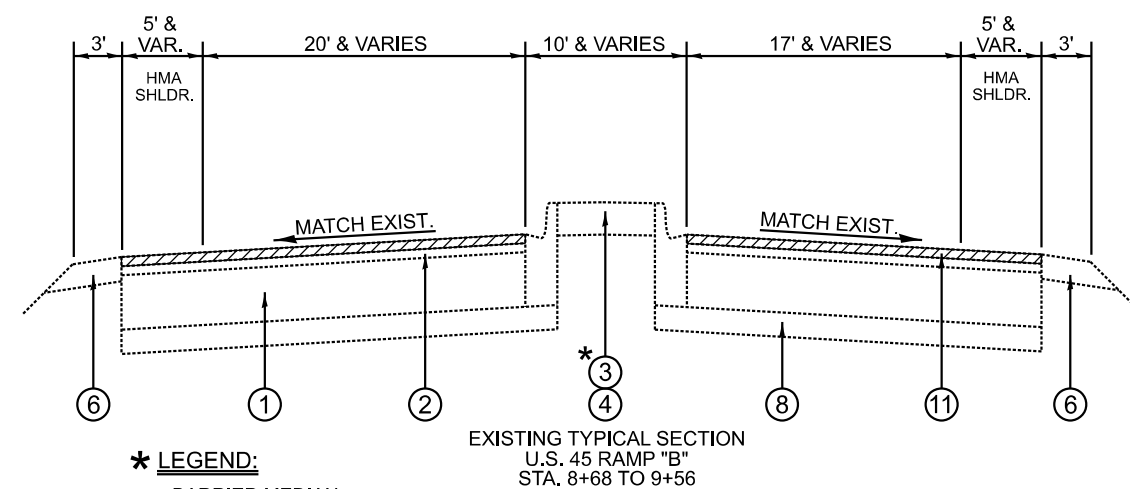
EXISTING AND PROPOSED TYPICAL SECTIONS
FAP 344 (US 45 / LAKE ST.)-TEMPEL DR. TO RAMP TO IL 137

SCALE: SHEET OF SHEETS STA. TO STA.

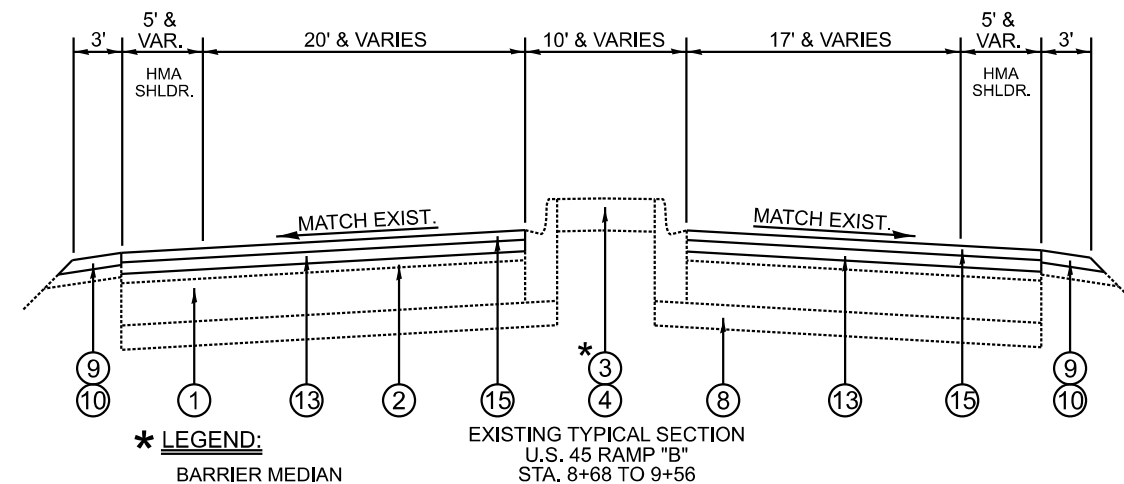
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	COOK	21	7
CONTRACT NO. 80B29				
ILLINOIS FED. AID PROJECT				



- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT, PLUS OR MINUS 9"
 - ② EXISTING HOT-MIX ASPHALT SURFACE, PLUS OR MINUS 6" (AFTER MILLING)
 - ③ EXISTING CORRUGATED MEDIAN
 - ④ EXISTING BARRIER MEDIAN
 - ⑤ EXISTING LANDSCAPE MEDIAN
 - ⑥ EXISTING AGGREGATE SHOULDER
 - ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑧ EXISTING STABILIZED SUB-BASE
 - ⑨ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑩ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
 - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
 - ⑫ PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
 - ⑬ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
 - ⑭ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1"
 - ⑮ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', IL-9.5, N70, 1 3/4"
 - R CURB AND GUTTER REMOVAL AND REPLACEMENT
(AS DIRECTED BY THE ENGINEER)



- * LEGEND:**
 BARRIER MEDIAN
 STA. 9+14 TO STA. 9+56
 CORRUGATED MEDIAN
 STA. 8+68 TO STA. 9+14



- * LEGEND:**
BARRIER MEDIAN
STA. 9+14 TO STA. 9+56
PAINTED MEDIAN
STA. 8+68 TO STA. 9+14

USER NAME = Phillip.Steed	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 9/30/2025	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

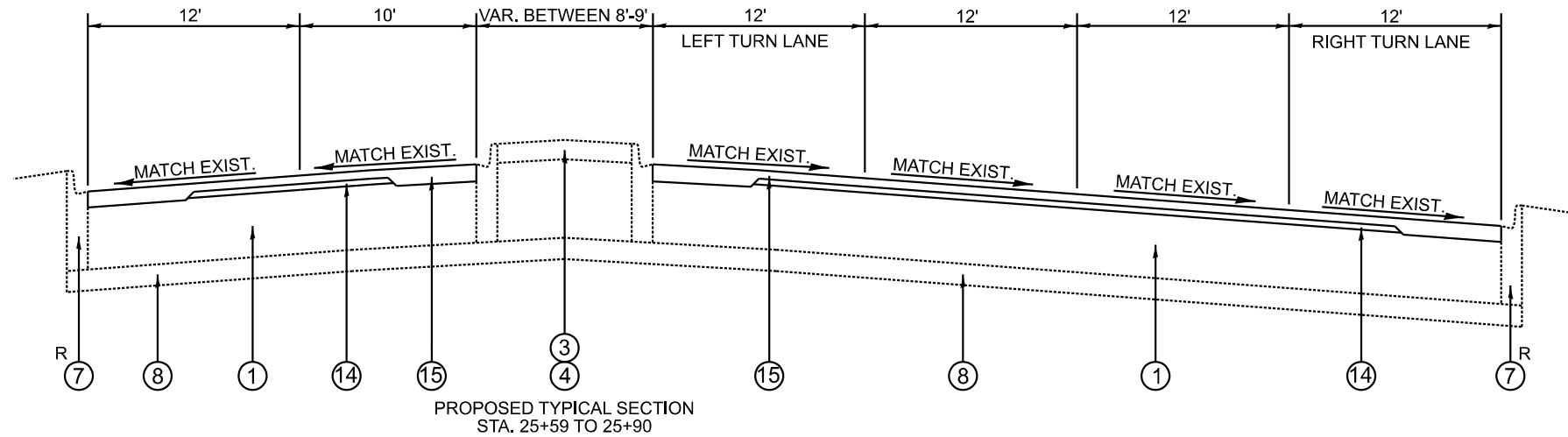
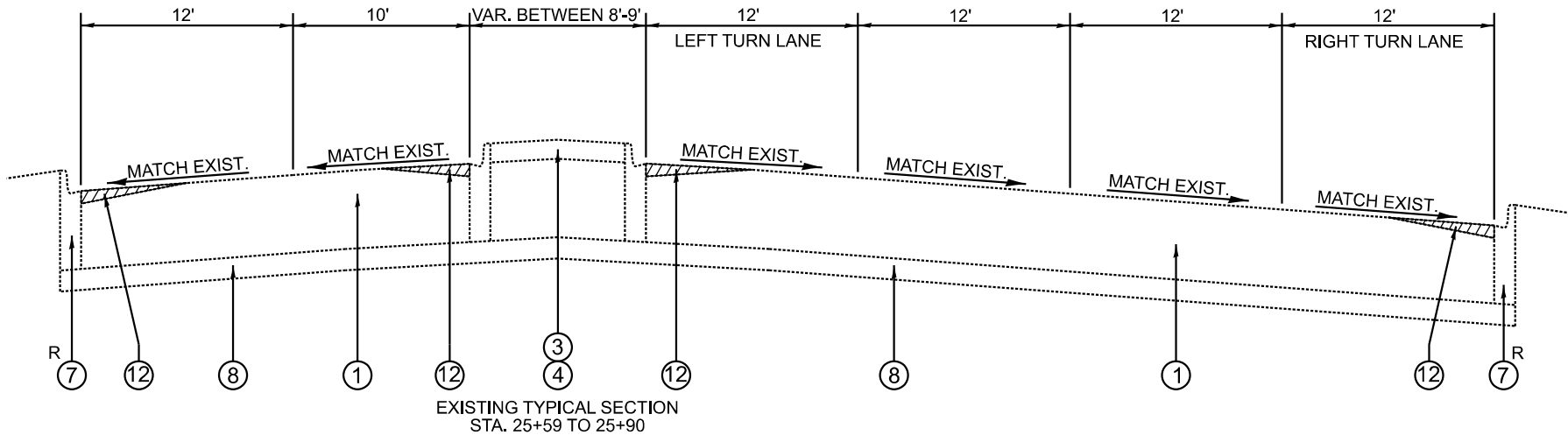
EXISTING AND PROPOSED TYPICAL SECTIONS
FAP 344 (US 45 / LAKE ST.)--TEMPEL DR. TO RAMP TO IL 137

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	COOK	21	8
		CONTRACT NO. 80B29		
		ILLINOIS	FED. AID PROJECT	

LEGEND:

- ① EXISTING P.C.C. PAVEMENT, PLUS OR MINUS 9"
- ② EXISTING HOT-MIX ASPHALT SURFACE, PLUS OR MINUS 6" (AFTER MILLING)
- ③ EXISTING CORRUGATED MEDIAN
- ④ EXISTING BARRIER MEDIAN
- ⑤ EXISTING LANDSCAPE MEDIAN
- ⑥ EXISTING AGGREGATE SHOULDER
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑩ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"
- ⑫ PROPOSED P.C.C. SURFACE REMOVAL (VARIABLE DEPTH)
- ⑬ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑭ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1"
- ⑮ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX 'E', IL-9.5, N70, 1 3/4"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



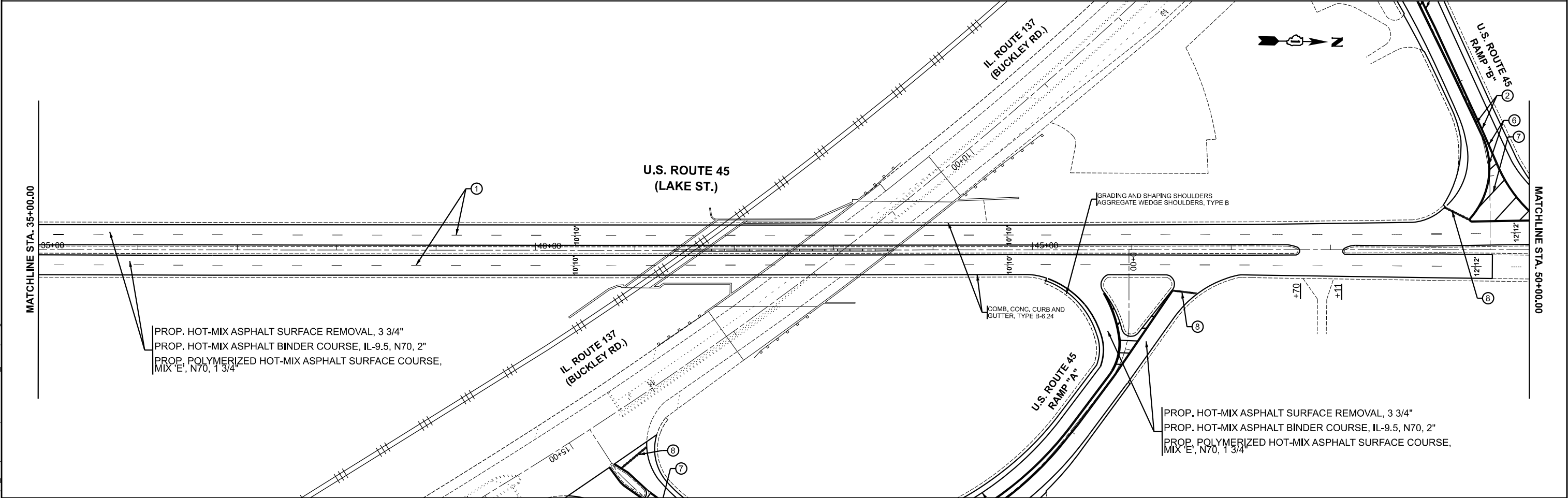
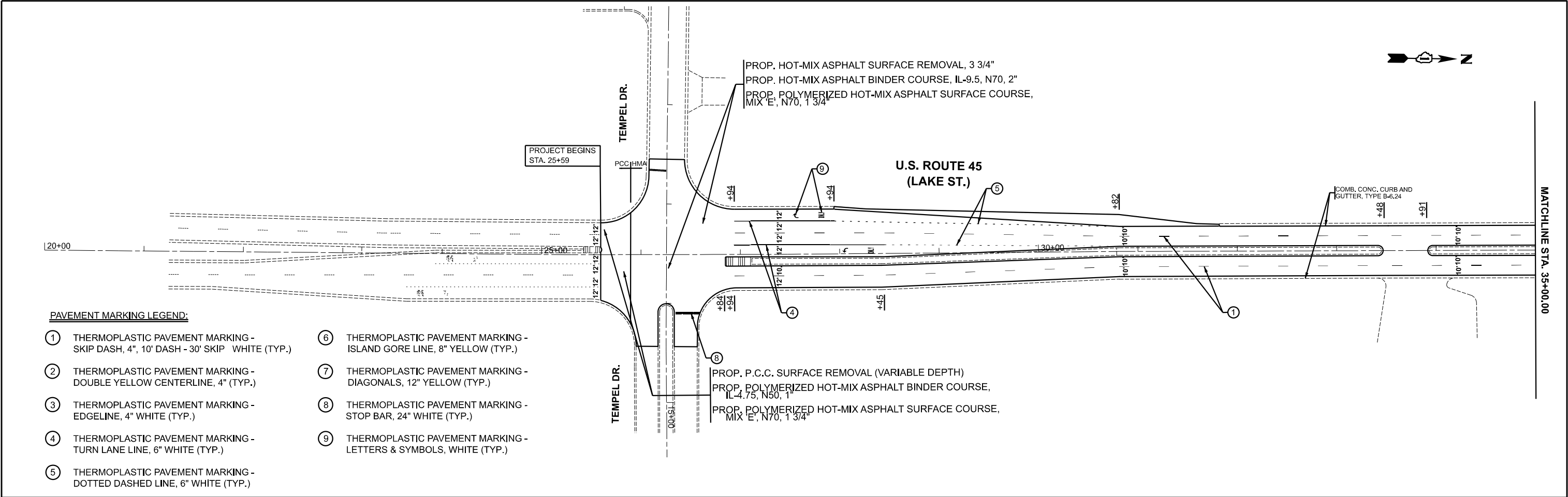
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
FAP 344 (US 45 / LAKE ST.)-TEMPEL DR. TO RAMP TO IL 137

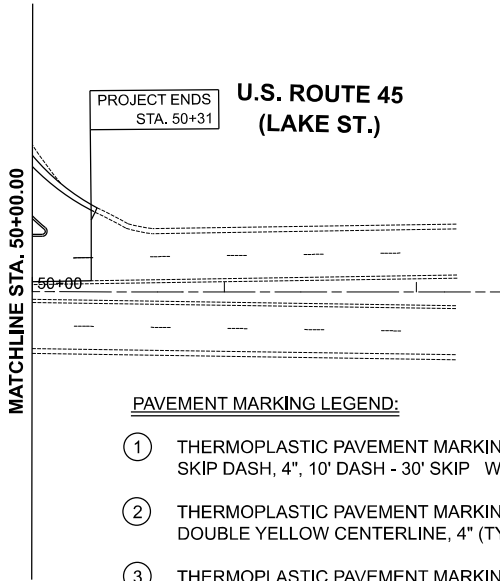
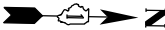
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	COOK	21	8A
CONTRACT NO. 80B29				

SCALE: SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT



MODEL: LAKE - RowyPlan01 [Sheet] FILE NAME: c:\p\work\wido\sted\paid\08842\US_45-sh-plan.dgn	USER NAME = Phillip.Steed		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			DRAWN -	REVISED -		FAP 344 (US 45 / LAKE ST.)-TEMPEL DR. TO RAMP TO IL 137			344	2025-2010-RS	LAKE	21	9
			CHECKED -	REVISED -					CONTRACT NO. 80B29				
	PLOT DATE = 9/30/2025		DATE -	REVISED -		SCALE: 1"=50'	SHEET 1	OF 2 SHEETS	STA. 20+00.00	TO STA. 50+00.00	ILLINOIS FED. AID PROJECT		



PAVEMENT MARKING LEGEND:

- ①

THERMOPLASTIC PAVEMENT MARKING -
SKIP DASH, 4", 10' DASH - 30' SKIP WHITE (TYP.)
- ②

THERMOPLASTIC PAVEMENT MARKING -
DOUBLE YELLOW CENTERLINE, 4" (TYP.)
- ③

THERMOPLASTIC PAVEMENT MARKING -
EDGE LINE, 4" WHITE (TYP.)
- ④

THERMOPLASTIC PAVEMENT MARKING -
TURN LANE LINE, 6" WHITE (TYP.)
- ⑤

THERMOPLASTIC PAVEMENT MARKING -
DOTTED DASHED LINE, 6" WHITE (TYP.)
- ⑥

THERMOPLASTIC PAVEMENT MARKING -
ISLAND GORE LINE, 8" YELLOW (TYP.)
- ⑦

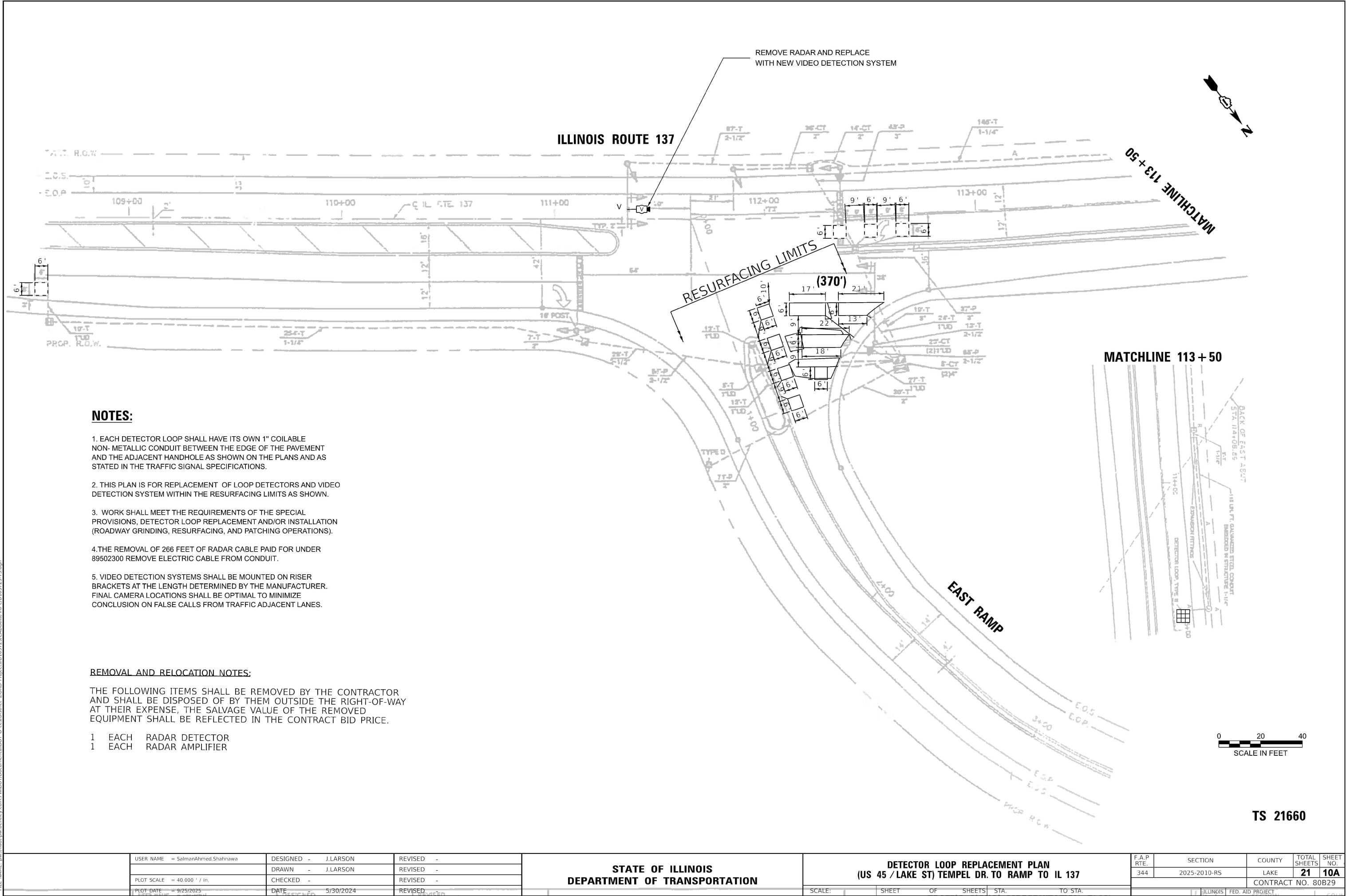
THERMOPLASTIC PAVEMENT MARKING -
DIAGONALS, 12" YELLOW (TYP.)
- ⑧

THERMOPLASTIC PAVEMENT MARKING -
STOP BAR, 24" WHITE (TYP.)
- ⑨

THERMOPLASTIC PAVEMENT MARKING -
LETTERS & SYMBOLS, WHITE (TYP.)

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	USER NAME = Phillip.Steed	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS FAP 344 (US 45 / LAKE ST.)–TEMPEL DR. TO RAMP TO IL 137				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						344	2025-2010-RS	LAKE	21	10
		CHECKED -	REVISED -		CONTRACT NO. 80B29								
	PLOT DATE = 8/21/2025	DATE -	REVISED -		SCALE: 1"=50'	SHEET 2	OF 2	SHEETS	STA. 50+00.00	TO STA. 65+00.00	ILLINOIS	FED. AID PROJECT	



NOTES:

- 1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON- METALLIC CONDUIT BETWEEN THE EDGE OF THE PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.
- 2. THIS PLAN IS FOR REPLACEMENT OF LOOP DETECTORS AND VIDEO DETECTION SYSTEM WITHIN THE RESURFACING LIMITS AS SHOWN.
- 3. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING, AND PATCHING OPERATIONS).
- 4. THE REMOVAL OF 266 FEET OF RADAR CABLE PAID FOR UNDER 89502300 REMOVE ELECTRIC CABLE FROM CONDUIT.
- 5. VIDEO DETECTION SYSTEMS SHALL BE MOUNTED ON RISER BRACKETS AT THE LENGTH DETERMINED BY THE MANUFACTURER. FINAL CAMERA LOCATIONS SHALL BE OPTIMAL TO MINIMIZE CONCLUSION ON FALSE CALLS FROM TRAFFIC ADJACENT LANES.

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE, THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH RADAR DETECTOR
- 1 EACH RADAR AMPLIFIER

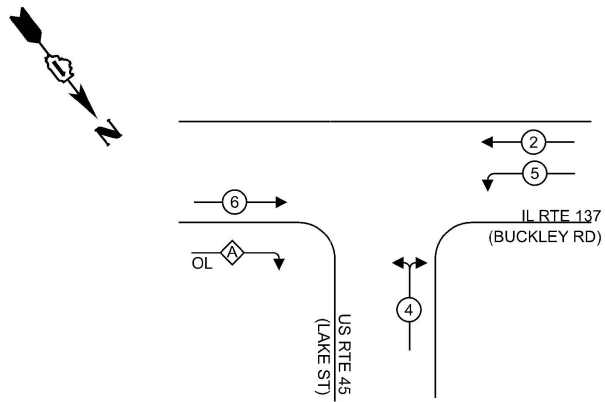
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT PLAN
(US 45 /LAKE ST) TEMPEL DR. TO RAMP TO IL 137

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	10A
CONTRACT NO. 80B29				
ILLINOIS FED. AID PROJECT				

TS 21660

EXISTING CONTROLLER SEQUENCE



LEGEND:

- ← (⊗) → PROTECTED PHASE
- ← - (⊗) - → PROTECTED/PERMITTED PHASE
- ← (⊗) → PEDESTRIAN PHASE
- ← (⊗) OL → OVERLAP

RIGHT TURN OVERLAP

PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	= 6	+ 4

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
DETECTOR LOOP, TYPE 1	FOOT	370
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	266
VIDEO VEHICLE DETECTION SYSTEM, SINGLE APPROACH	EACH	1

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

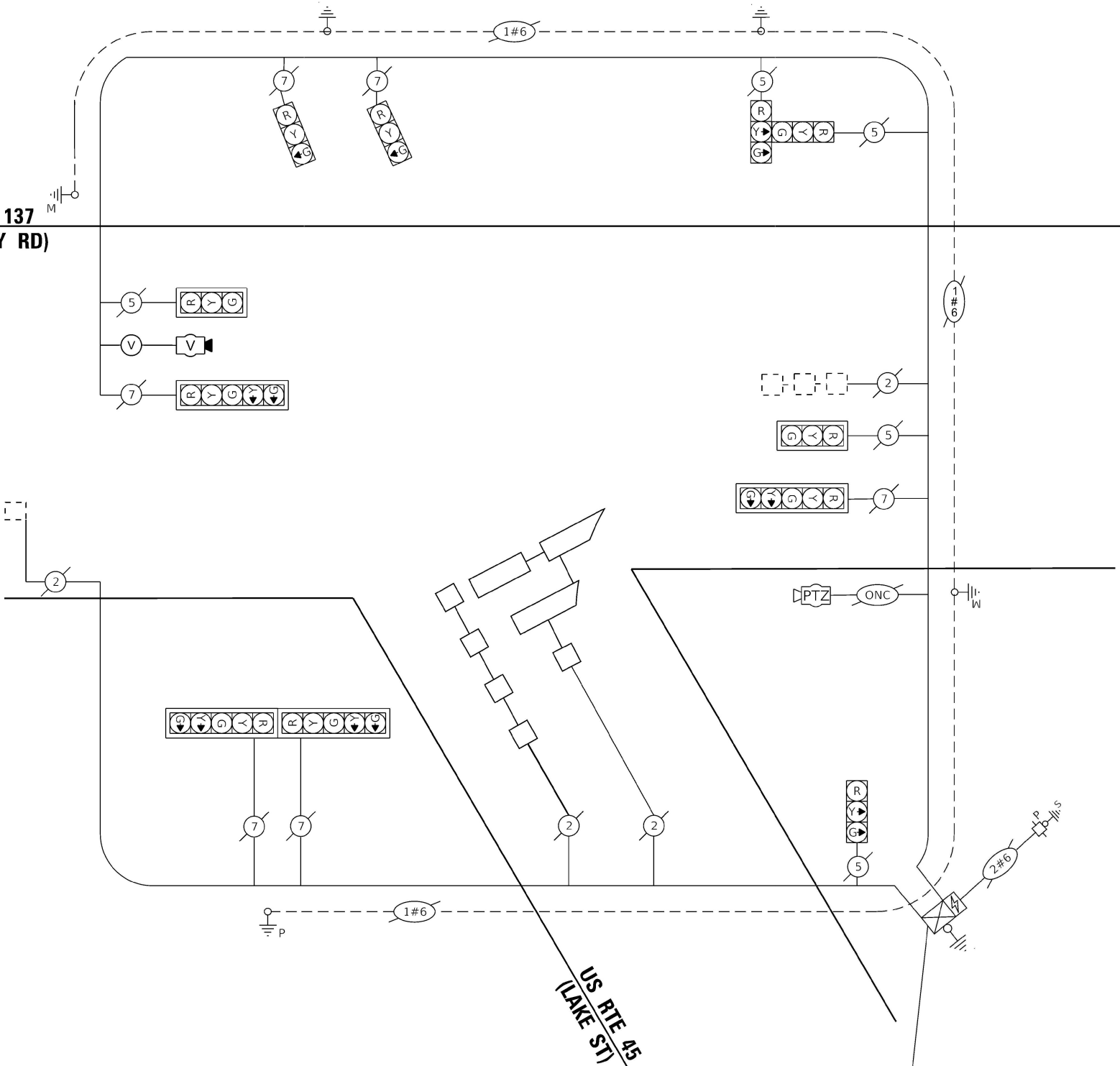
EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	4	11	44
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	-	15	-
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	2	20	40
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING		363	
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING		968	

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W CENTER CT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: DANIEL MENDEZ
PHONE: 773-799-6111
COMPANY: COMED
ACCOUNT NUMBER: 03555-42222
METER NUMBER: ---

IL RTE 137
(BUCKLEY RD)



CABLE PLAN

SERVICE METER ON
CONTROLLER CABINET

TS 21660

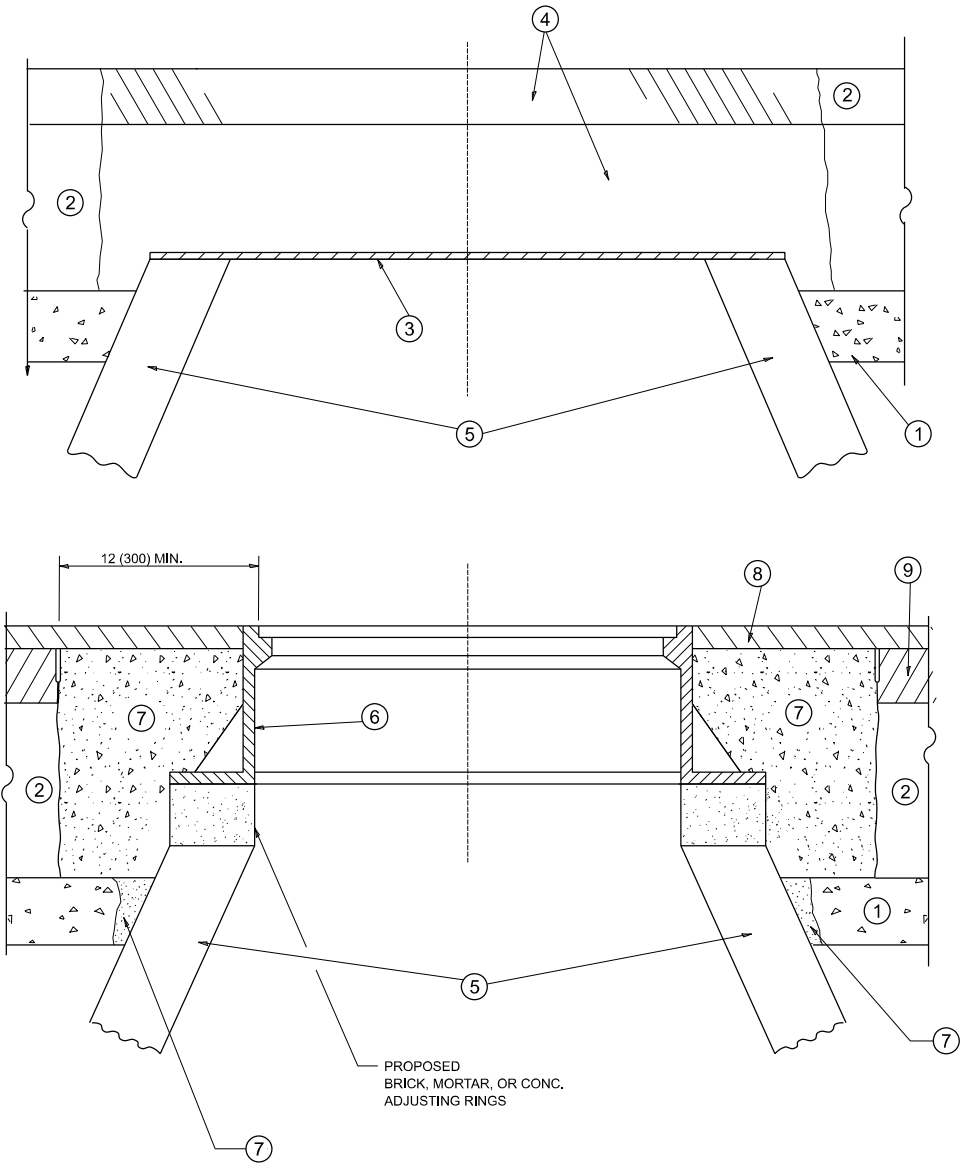
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, SCHEDULE OF QUANTITIES
(US 45 / LAKE ST) TEMPEL DR. TO RAMP TO IL 137

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	COOK	21	10B
CONTRACT NO. 80B29				
ILLINOIS FED. AID PROJECT				

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DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

- STAGE 1** (BEFORE PAVEMENT MILLING)
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

- STAGE 2** (AFTER PAVEMENT MILLING)
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

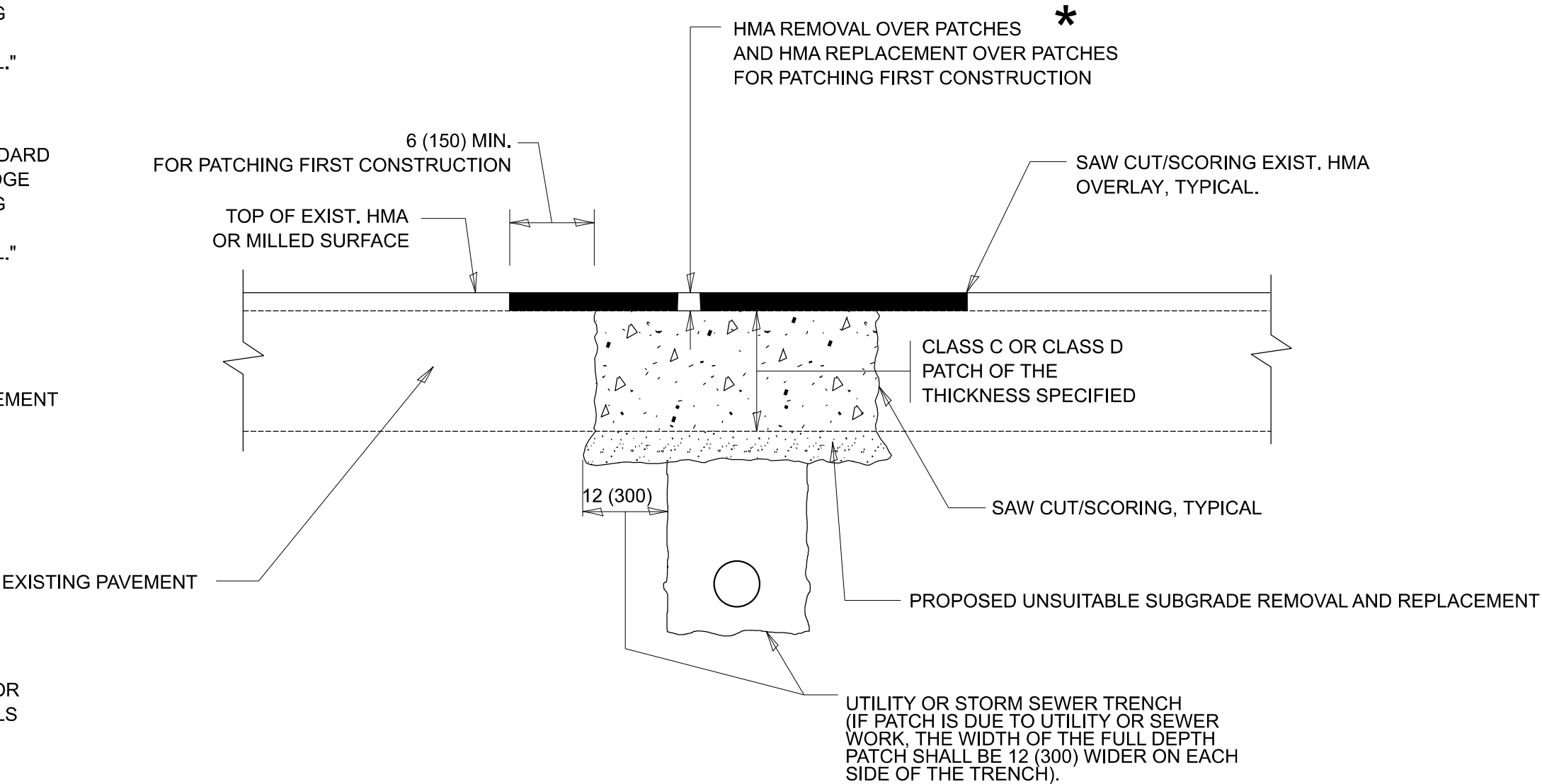
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		DRAWN -	REVISED - R. BORO 12-06-11					344	2025-2010-RS	LAKE	21	11
		CHECKED -	REVISED - K. SMITH 11-18-22		BD600-03 (BD-08)			CONTRACT NO. 80B29				
	PLOT DATE = 8/20/2024	DATE - 10-25-94	REVISED - K. SMITH 09-15-23		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

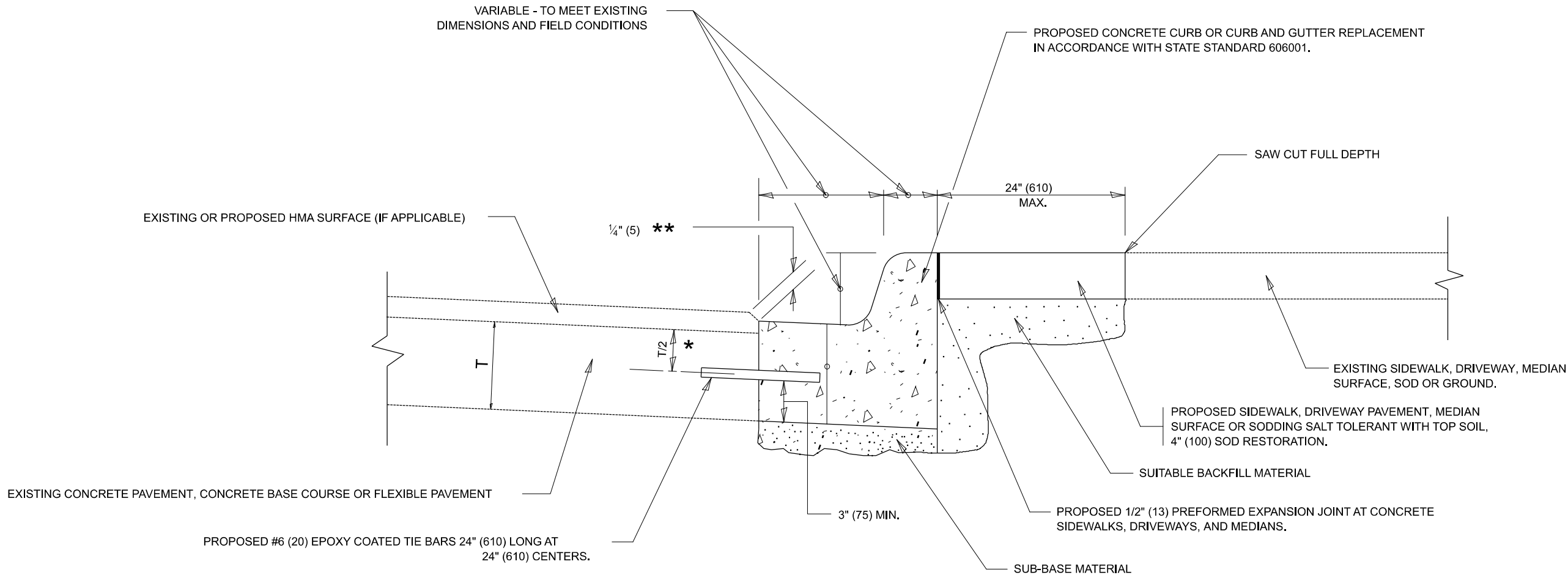
- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	USER NAME = Eric,L.Thomas	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A.P. RTE.	SECTION		COUNTY	TOTAL SHEETS	SHEET NO.
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		CHECKED -	REVISED - K. ENG 10-27-08						BD400-04 (BD-22)		CONTRACT NO. 80B29			
	PLOT DATE = 8/20/2024	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		SCALE: NONE		SHEET 1	OF 1	SHEETS	STA.	TO STA.			
							ILLINOIS FED. AID PROJECT							

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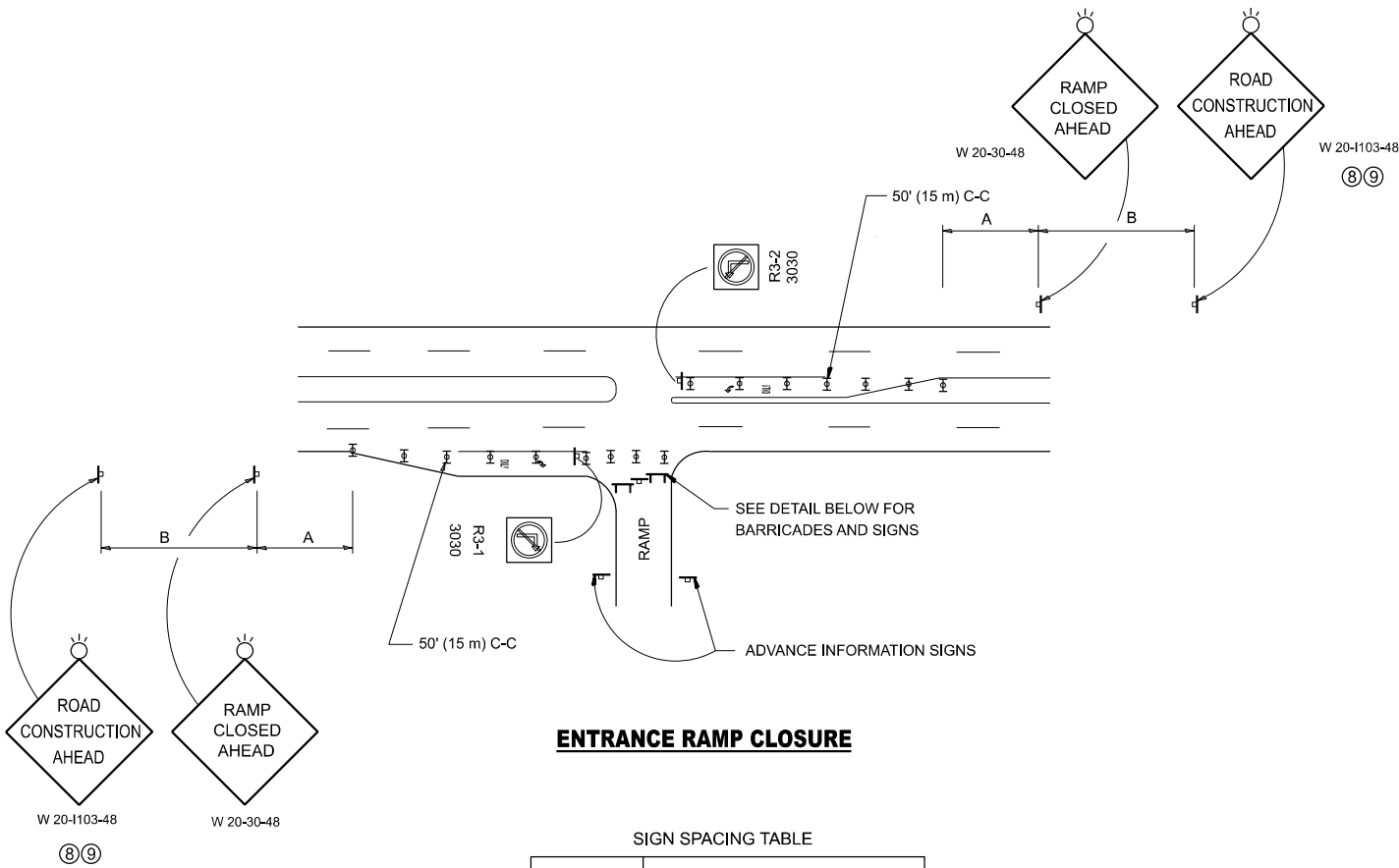
- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME ■ Eric.L.Thomas	DESIGNED - A. HOUSEH	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - M. GOMEZ 01-22-01						344	2025-2010-RS	LAKE	21	13
		CHECKED -	REVISED - R. BORO 12-15-09		BD600-06 (BD-24)			CONTRACT NO. 80B29					
	PLOT DATE ■ 8/20/2024	DATE - 03-11-94	REVISED - K. SMITH 07-11-19		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS	FED. AID PROJECT	

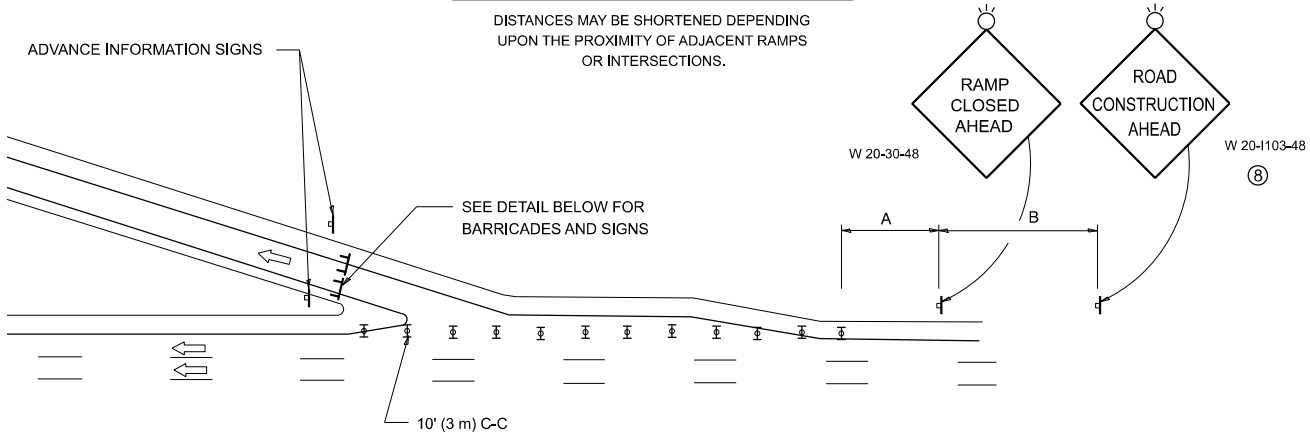
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ENTRANCE RAMP CLOSURE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

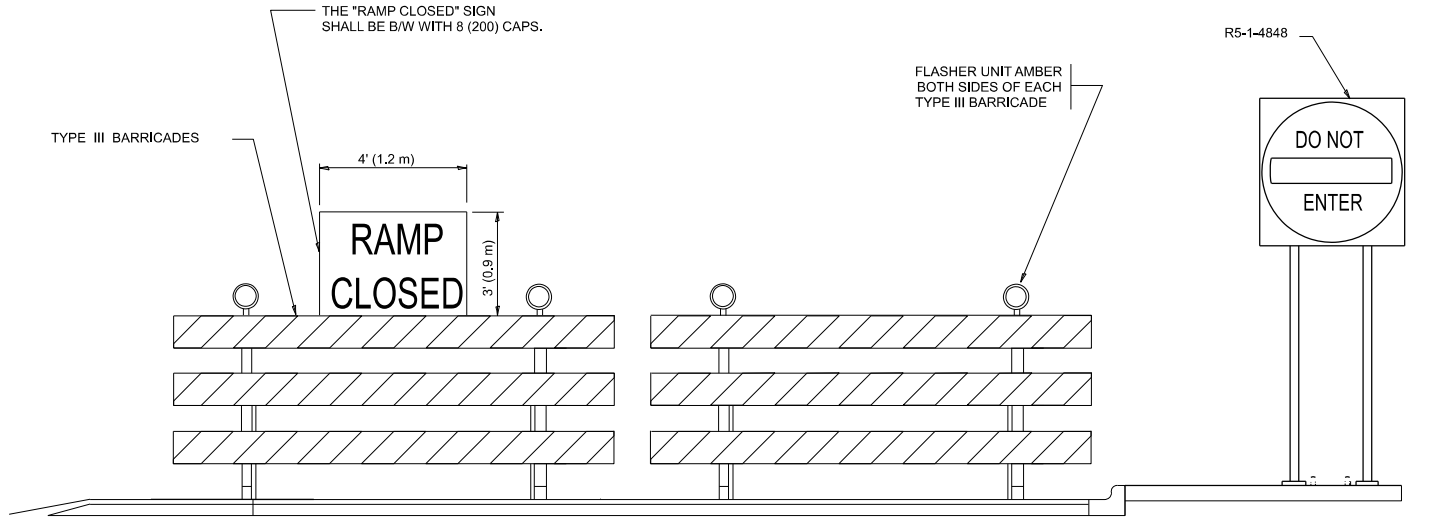
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

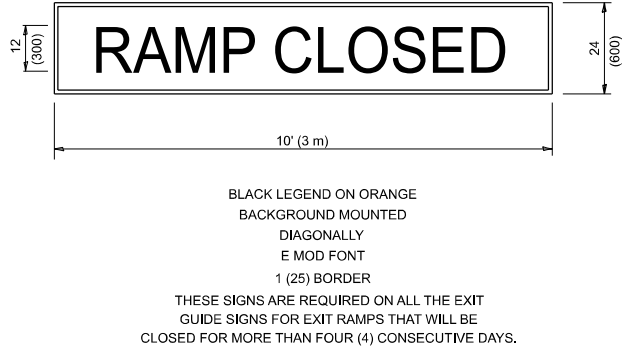
SYMBOLS

- TYPE II BARRICADE OR DRUM
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS



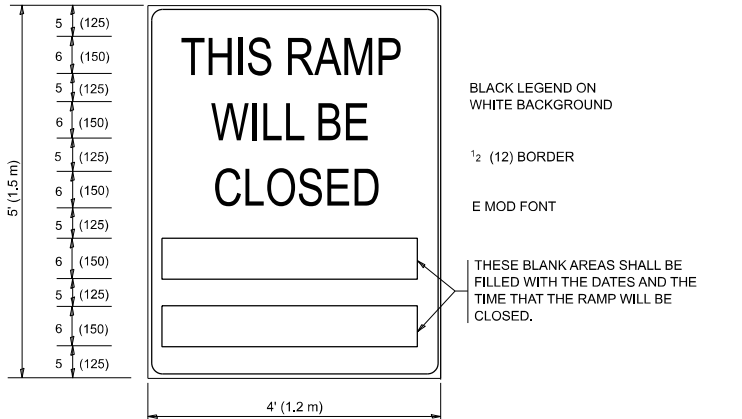
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

RAMP CLOSURE ADVANCE INFORMATION SIGN



GENERAL NOTES:

- CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

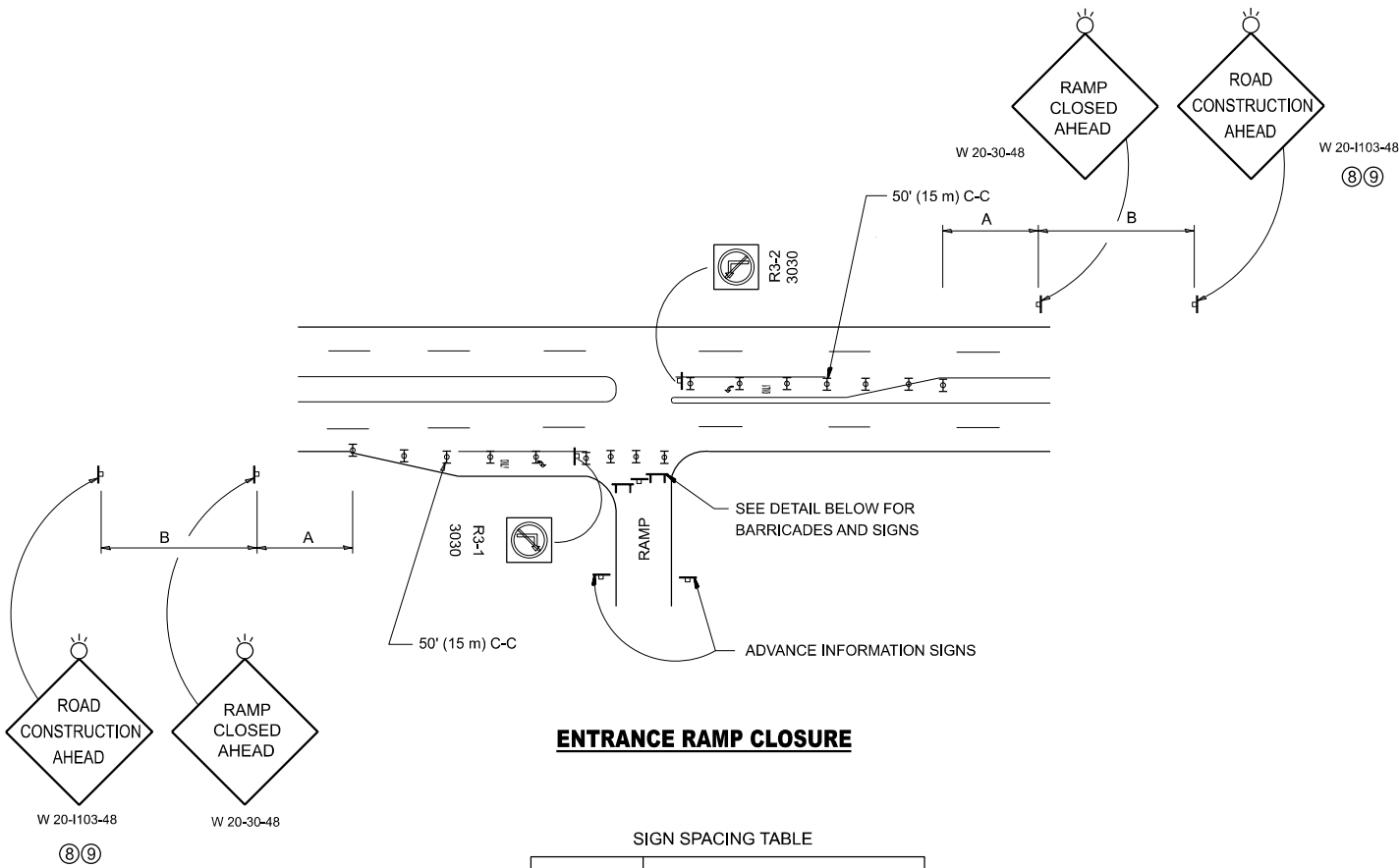
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ENTRANCE AND EXIT RAMP
CLOSURE DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	14A
TC-08		CONTRACT NO. 80B29		
		ILLINOIS FED. AID PROJECT		

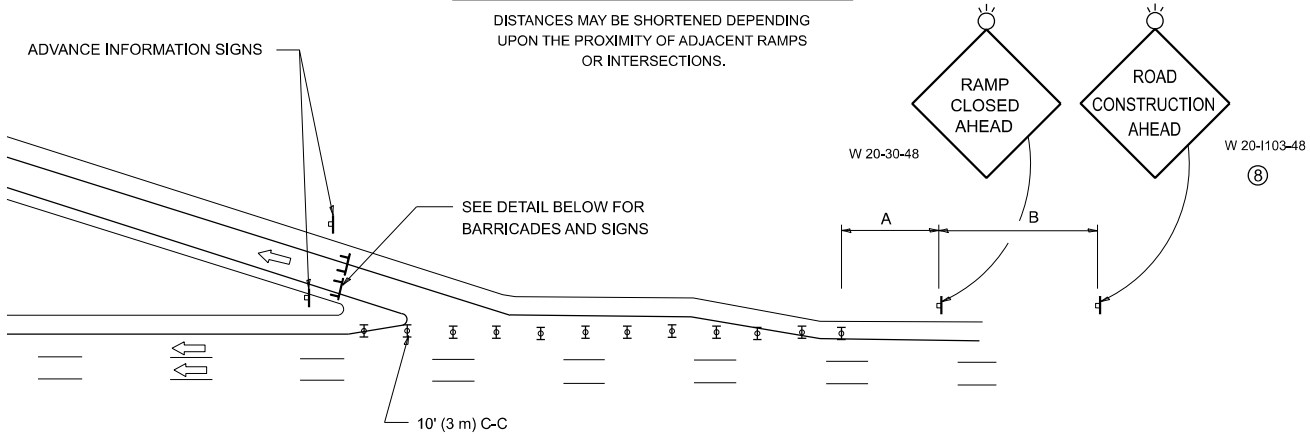
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ENTRANCE RAMP CLOSURE

FACILITY	SIGN SPACING TABLE	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

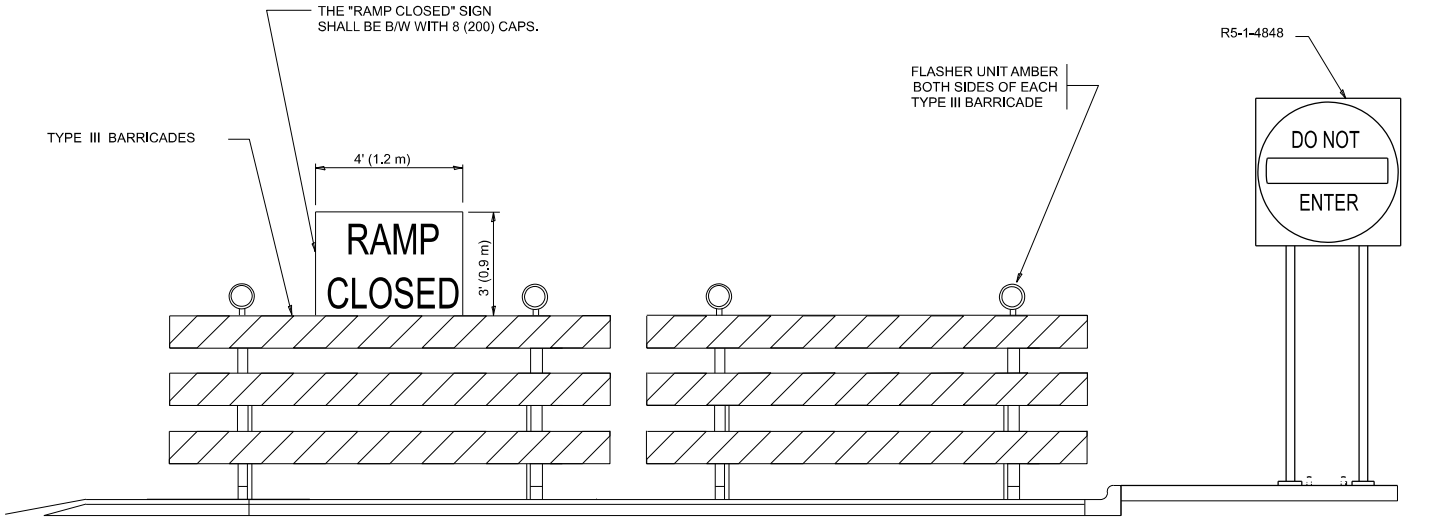
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

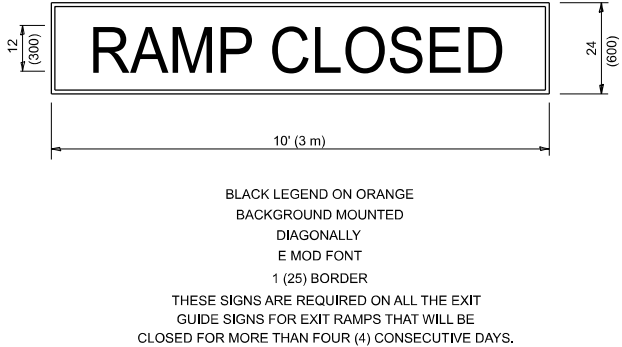
SYMBOLS

- TYPE II BARRICADE OR DRUM
- TYPE III BARRICADE WITH 2 FLASHING LIGHTS



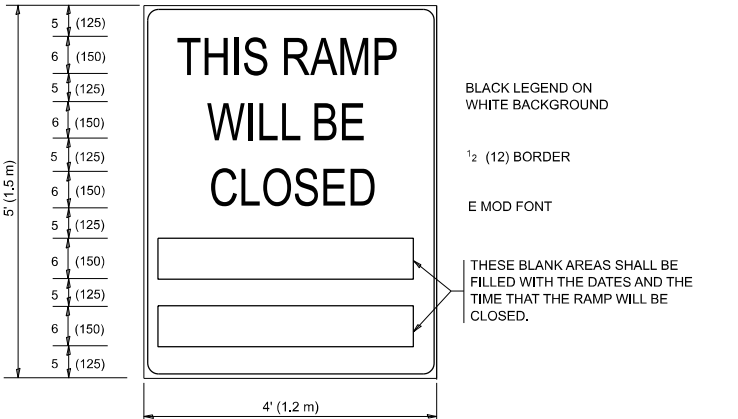
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

RAMP CLOSURE ADVANCE INFORMATION SIGN



GENERAL NOTES:

- CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Eric.L.Thomas	DESIGNED - D.W.S.	REVISED - S.P.B._12-09
	DRAWN -	REVISED - M.D._06-13
	CHECKED -	REVISED - M.D._01-18
PLOT DATE = 8/20/2024	DATE - 02-83	REVISED - D.S._05-24

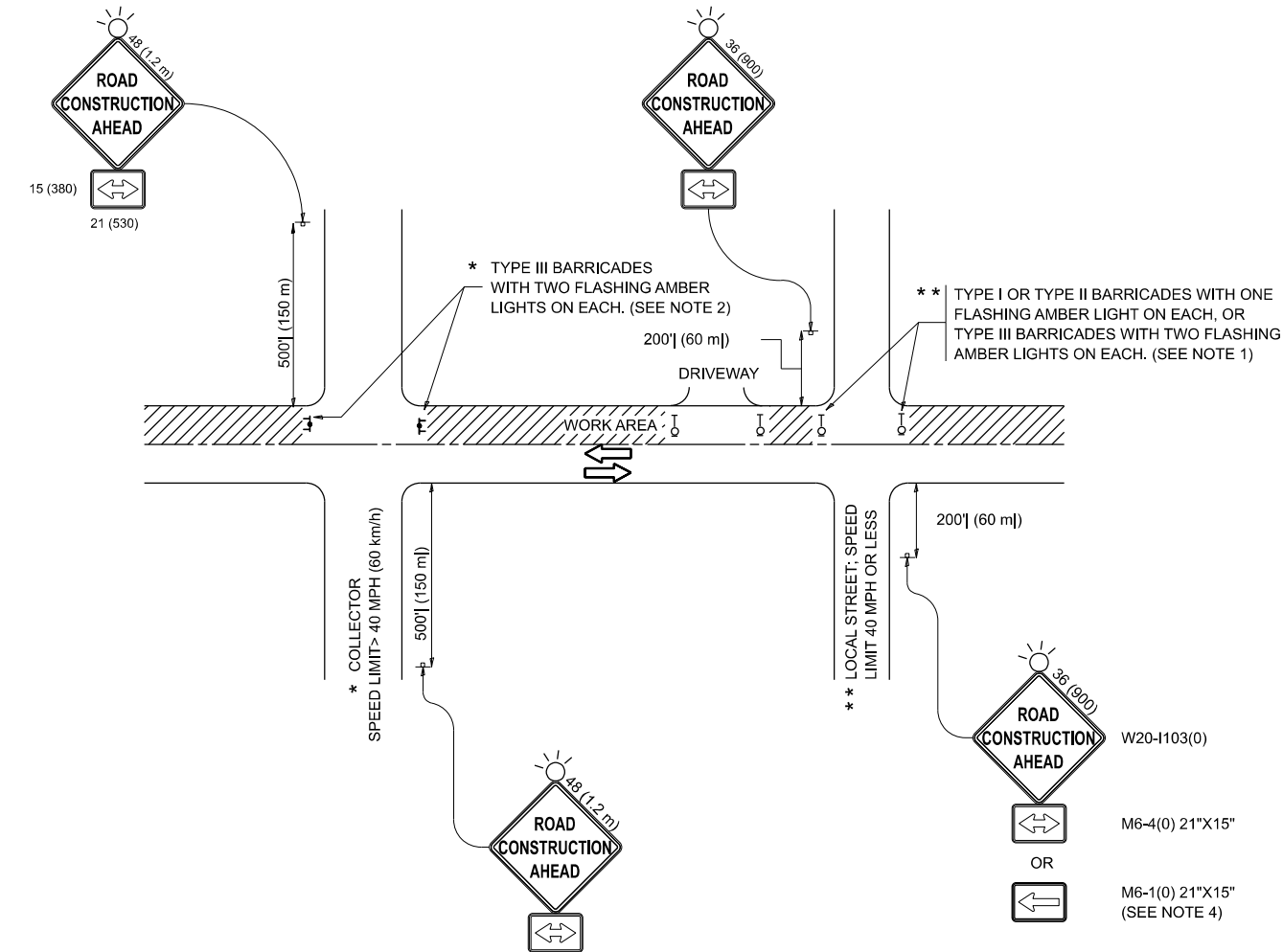
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ENTRANCE AND EXIT_RAMP CLOSURE DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	14A
TC-08		CONTRACT NO. 80B29		
		ILLINOIS FED. AID PROJECT		

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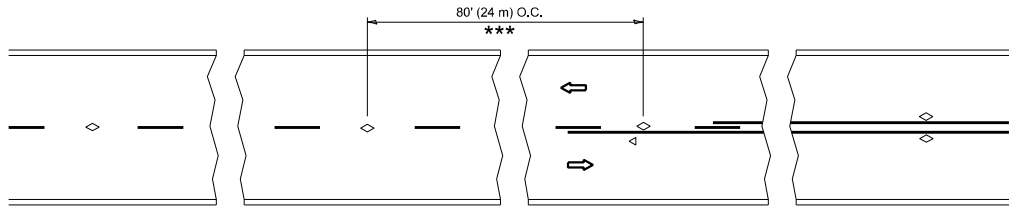
NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

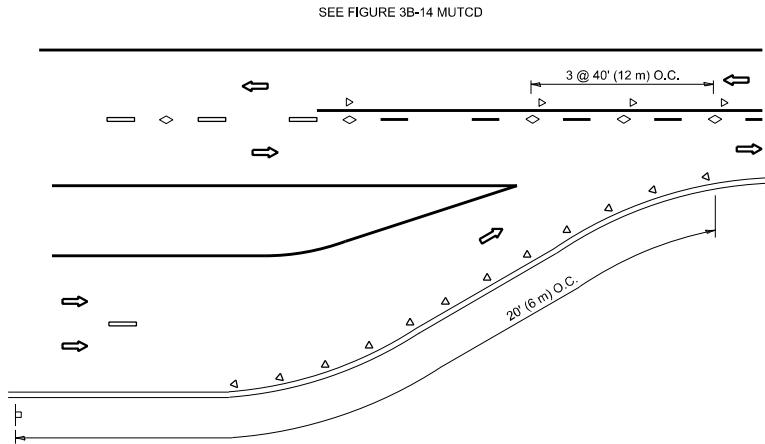
All dimensions are in inches (millimeters)
unless otherwise shown.

	USER NAME = Eric,L.Thomas	DESIGNED - L.H.A.	REVISED - T. RAMMACHER 01-06-00	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	F.A.P. SECTION COUNTY TOTAL SHEETS SHEET NO.					
		DRAWN -	REVISED - A. SCHUETZE 07-01-13			344	2025-2010-RS	LAKE	21	15	
		CHECKED -	REVISED - A. SCHUETZE 09-15-06			TC-10					
						CONTRACT NO. 80B29					
	PLOT DATE = 8/20/2024	DATE - 06-89	REVISED - D. SENDERAK 05-03-24			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

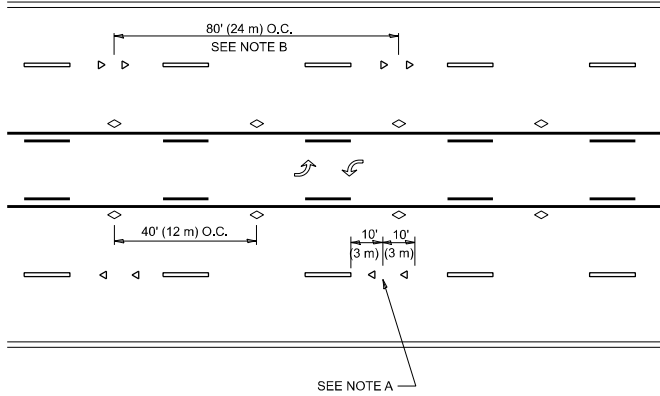
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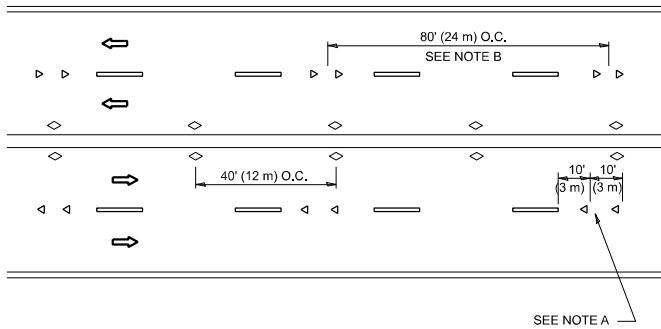
TWO-LANE/TWO-WAY



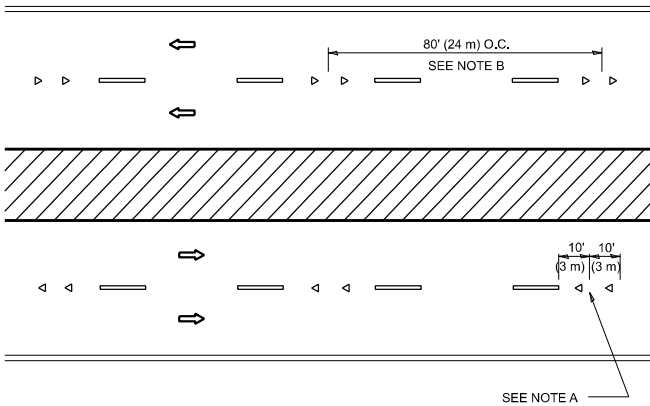
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters)
unless otherwise shown.

USER NAME	= Eric.L.Thomas	DESIGNED	-	REVISED	- T. RAMMACHER 03-12-99
		DRAWN	-	REVISED	- T. RAMMACHER 01-06-00
		CHECKED	-	REVISED	- C. JUCIUS 09-09-09
PLOT DATE	= 8/20/2024	DATE	-	REVISED	- C. JUCIUS 07-01-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	16
TC-11		CONTRACT NO. 80B29		
		ILLINOIS	FED. AID PROJECT	

TURN BAY ENTRANCE AT START
OF LANE CLOSURE TAPER

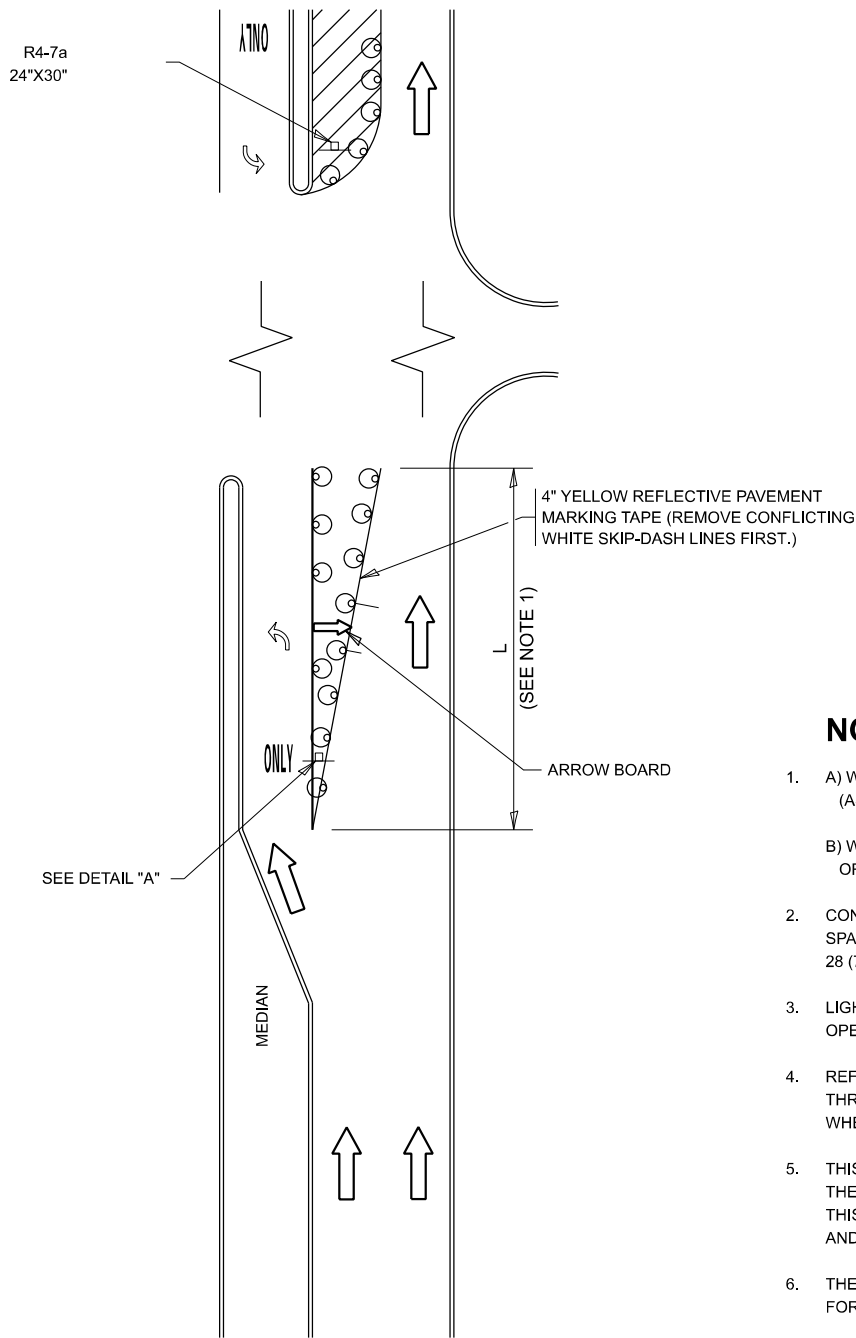


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

- A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE
WITHIN A LANE CLOSURE

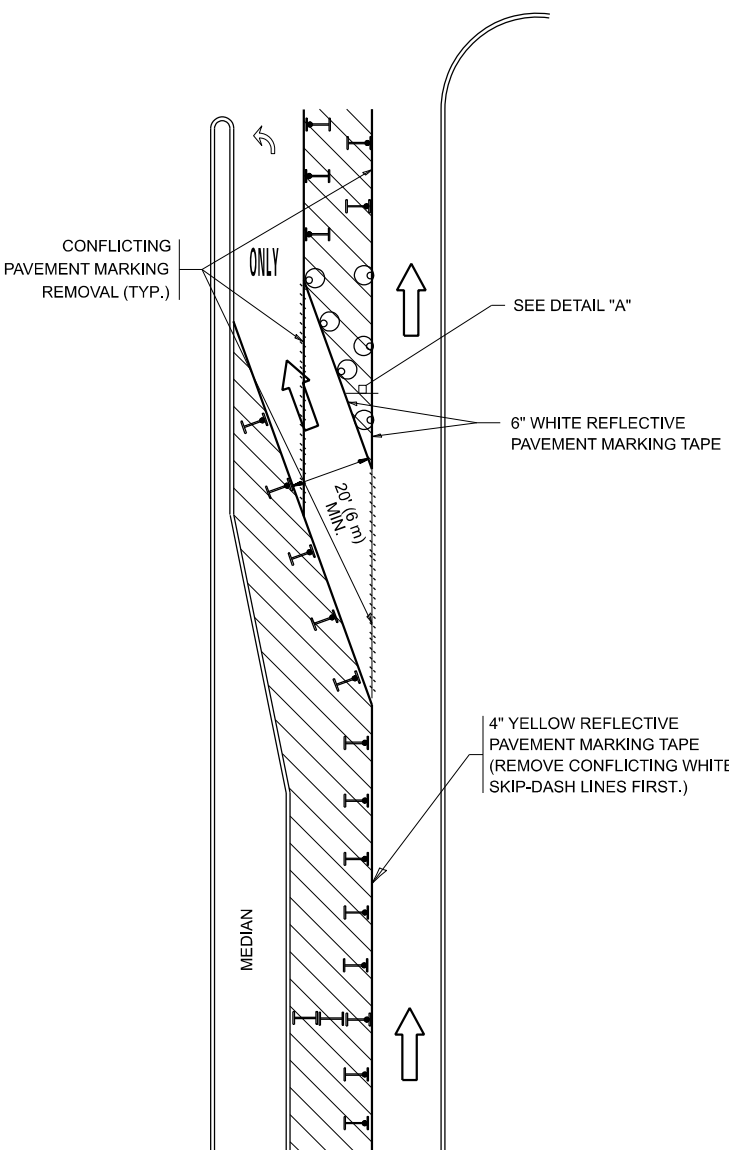
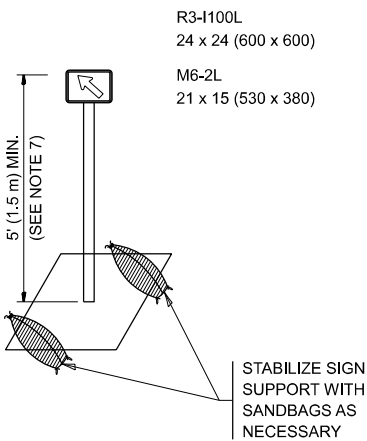


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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USER NAME = Eric.L.Thomas	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 8/20/2024	DATE - T. RAMMACHER 01-06-00	REVISED -

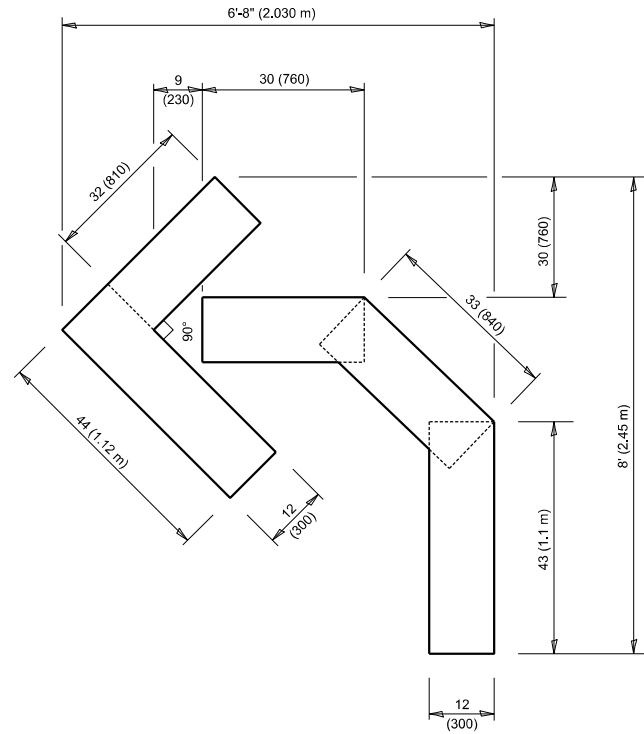
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

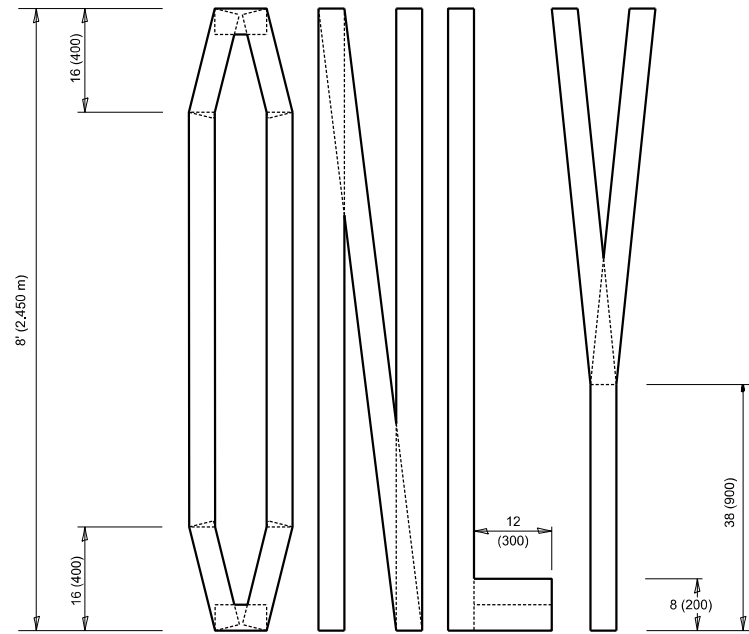
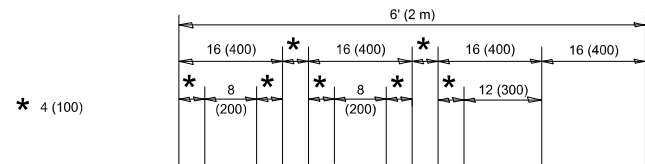
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	18
TC-14		CONTRACT NO. 80B29		
ILLINOIS		FED. AID PROJECT		

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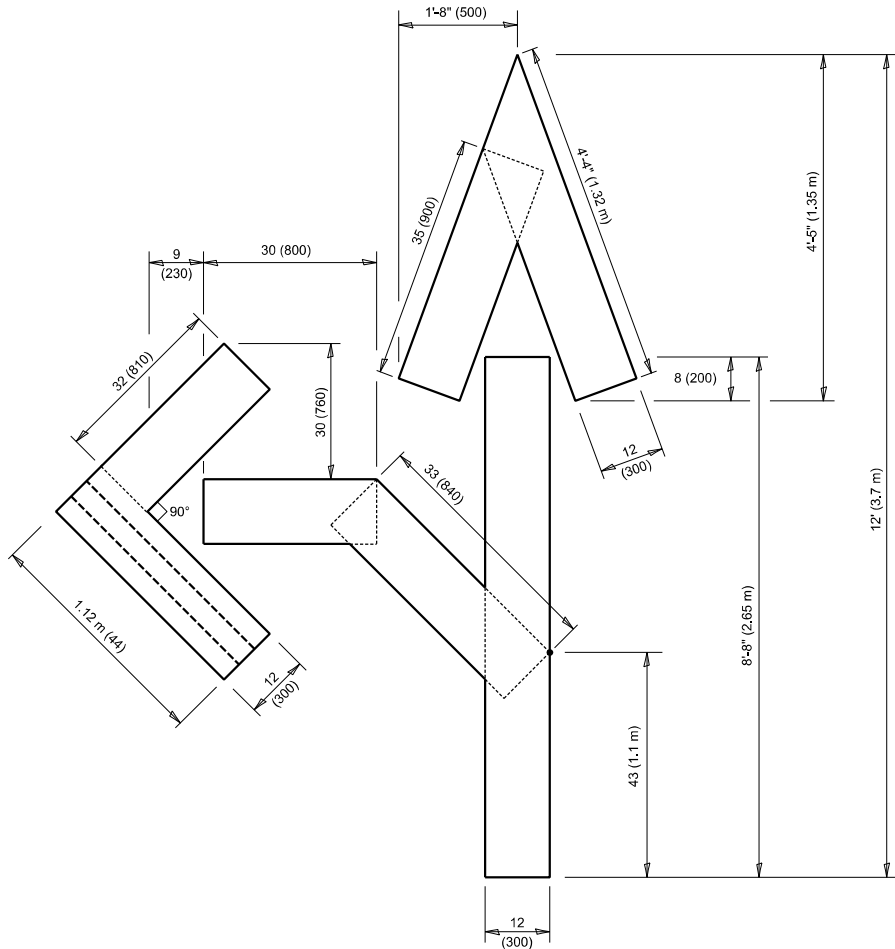
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

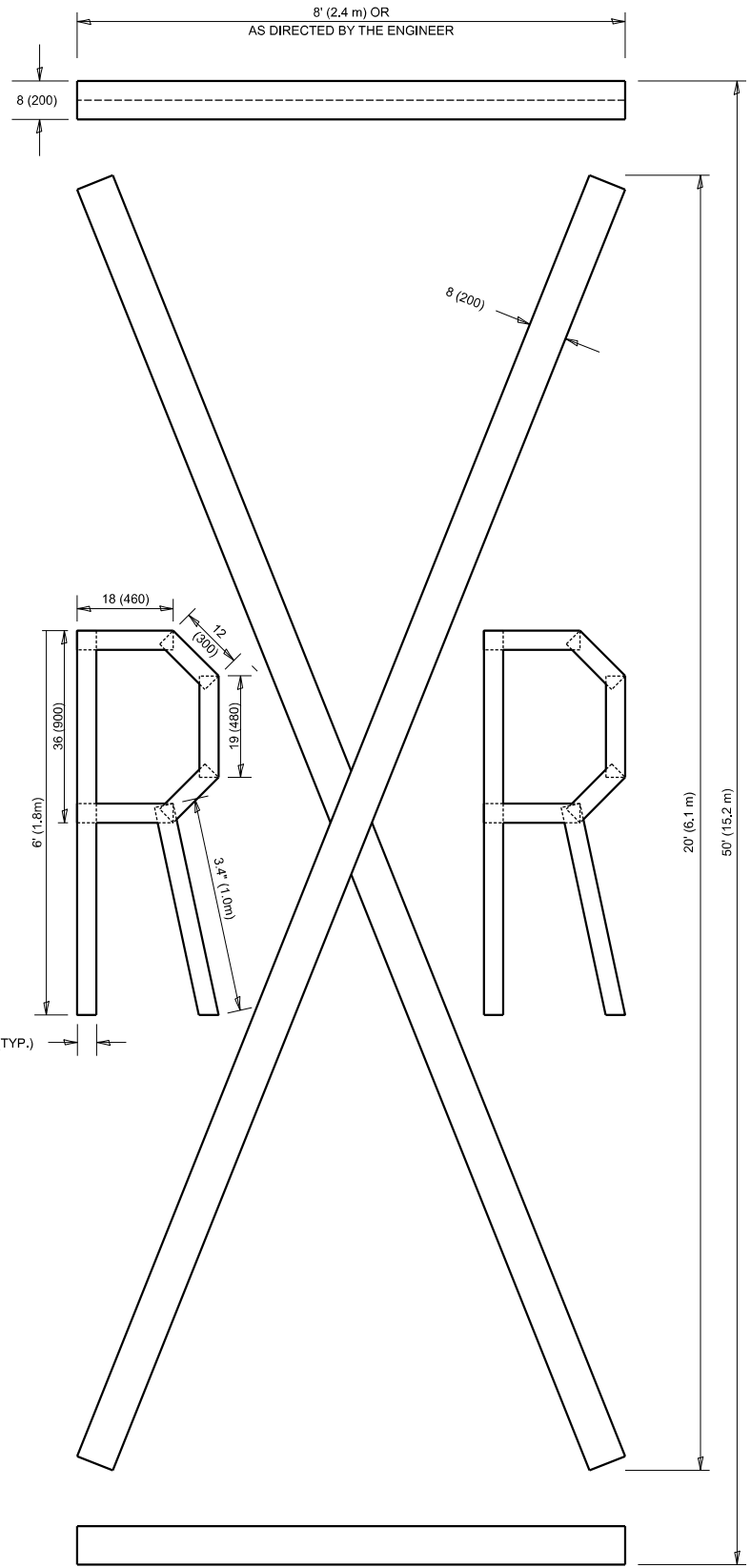


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



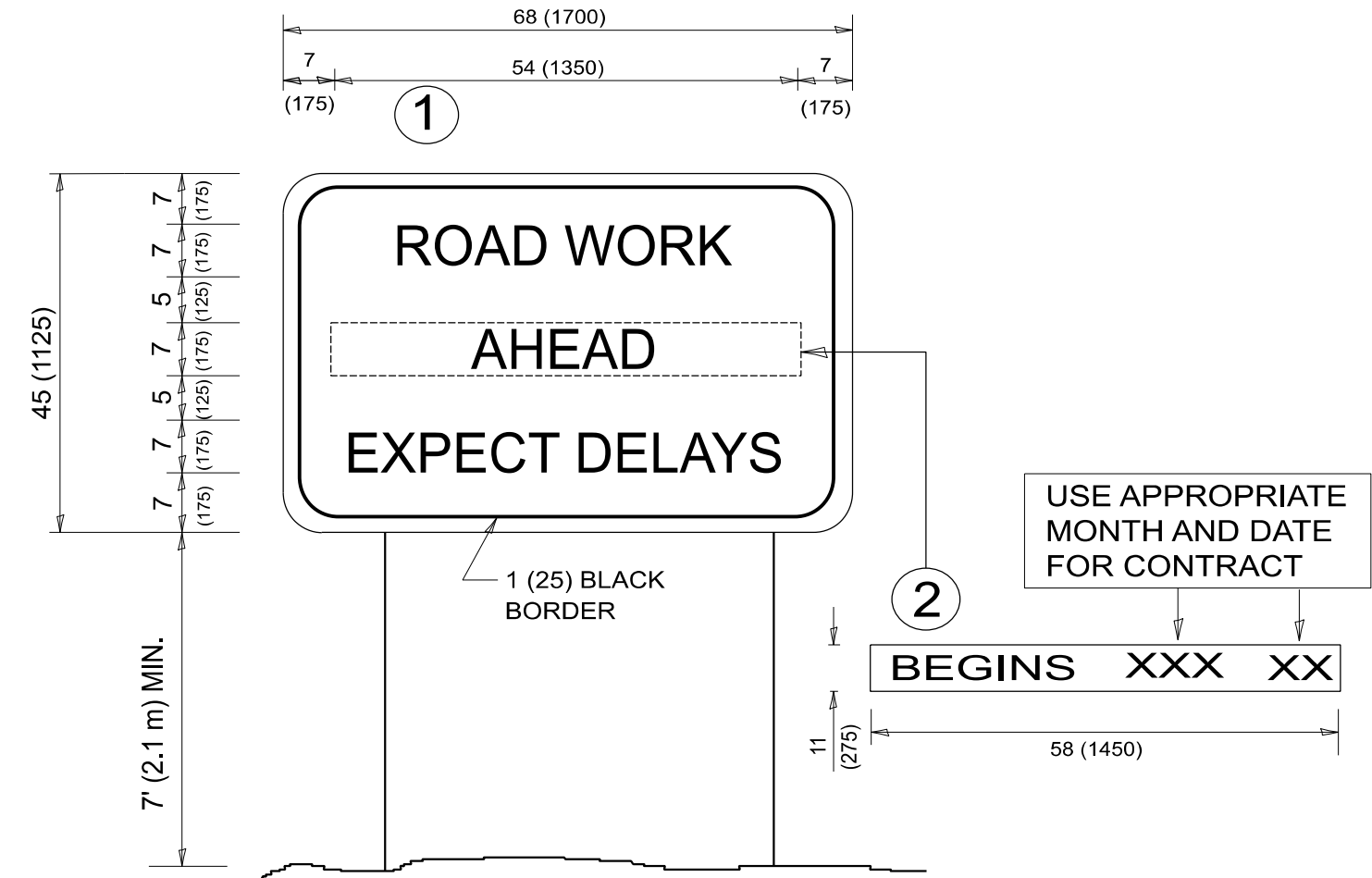
QUANTITY

4 (100) LINE = 225.9 ft. (68,9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
unless otherwise shown.

	USER NAME = Eric,L.Thomas	DESIGNED -	REVISED - T. RAMMACHER 03-02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - E. GOMEZ 08-28-00						344	2025-2010-RS	LAKE	21	19
		CHECKED -	REVISED - E. GOMEZ 08-28-00						TC-16				
	PLOT DATE = 8/20/2024	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16		ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.			

MODEL: Plan Single [Sheet]
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NOTES:

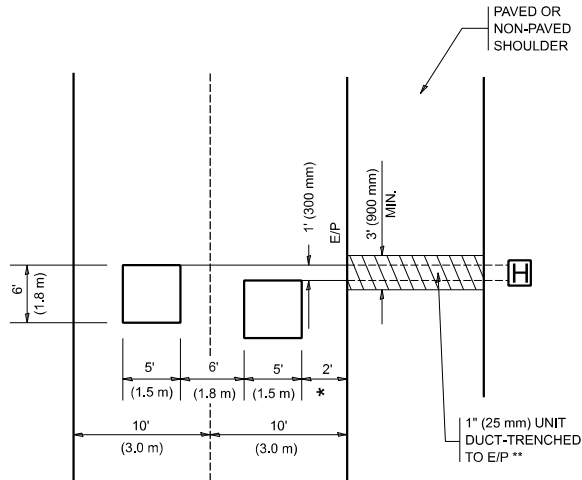
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

	USER NAME = Eric,L.Thomas	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. MIRS 12-11-97						344	2025-2010-RS	LAKE	21	20	
		CHECKED -	REVISED - T. RAMMACHER 02-02-99						TC-22					CONTRACT NO. 80B29
	PLOT DATE = 8/20/2024	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE		SHEET 1	OF 1	SHEETS	STA.	TO STA.			
							ILLINOIS FED. AID PROJECT							

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.



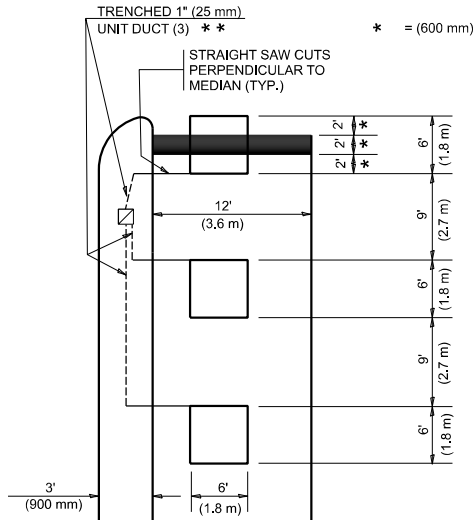
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.

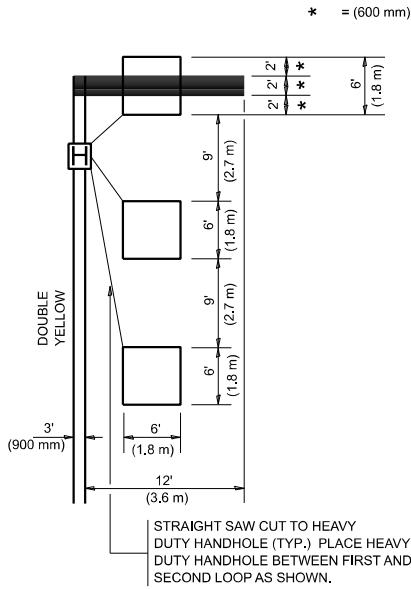


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED,
SHIELDED.

* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE
LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE
PAVEMENT.

* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT
DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST
HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE
SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID
FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM
FOR DETECTOR LOOPS.

* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET
(1.8 m)

* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH
LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE
INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.

* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE
THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR
(i.e. 1-1/2, 1-3/4, 2).

* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN
INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND
INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM
DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A
SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE
AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW
CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE
TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM)
DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN
AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS
UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE
LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR
LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR
SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED
ON ALL SIGNAL LAYOUT PLAN SHEETS.

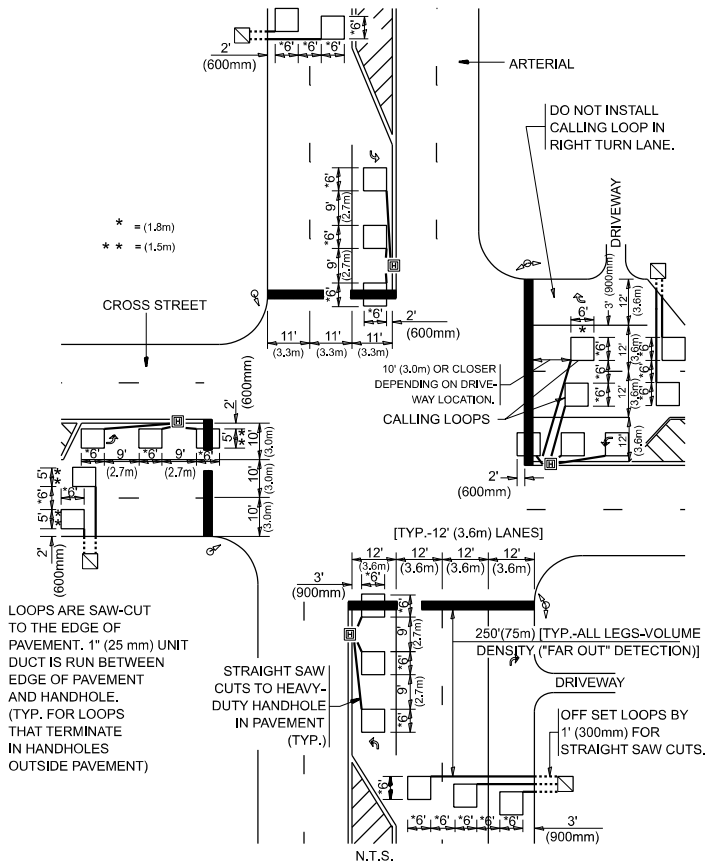
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE
DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT
TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN
ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO
NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND
10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF
PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

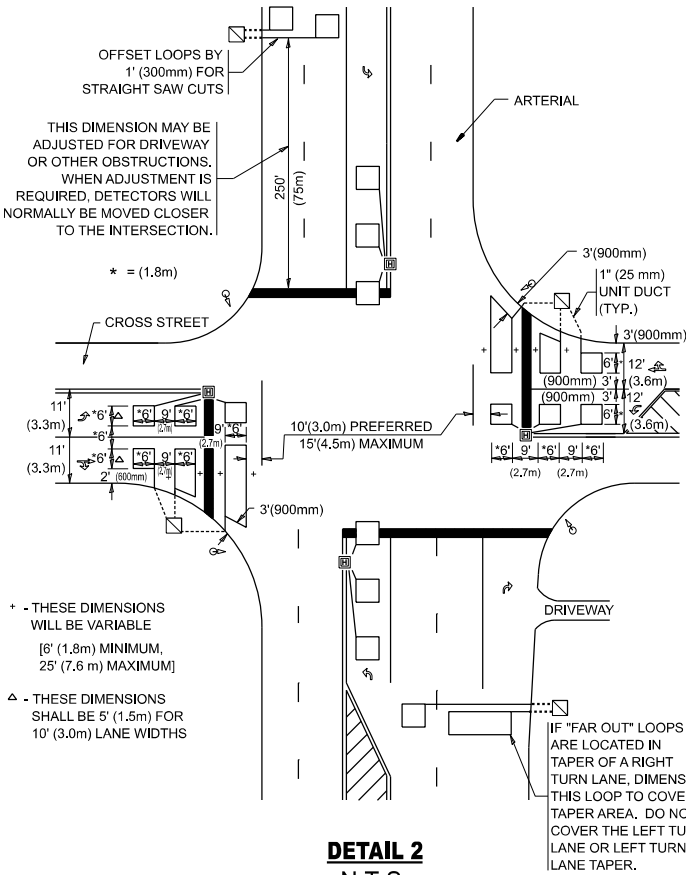
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER
FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE
DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS
FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

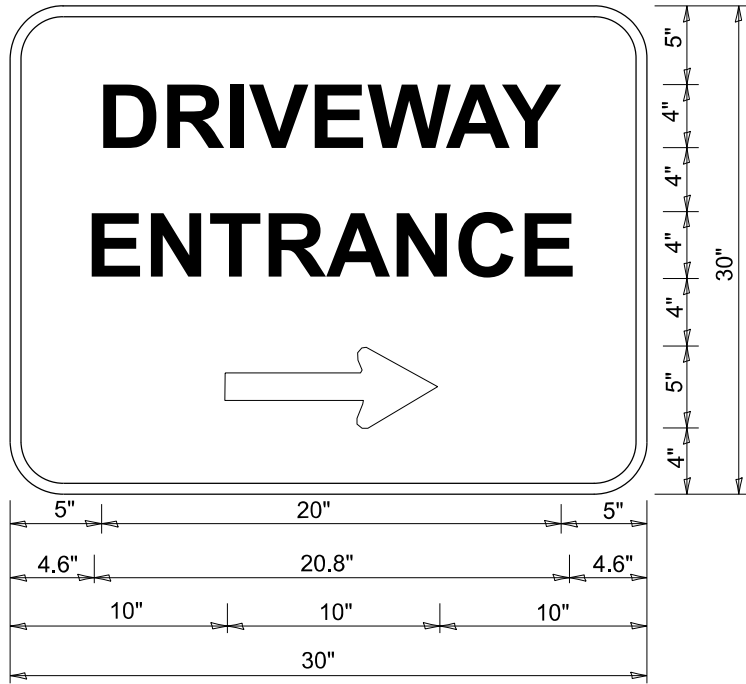
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2025-2010-RS	LAKE	21	21
TS-07		CONTRACT NO. 80B29		
ILLINOIS		FED. AID PROJECT		

MODEL: Plan Single Sheet
FILE NAME: \\illinois.gov\DOT\Users\COTTHOMASELM\My Documents\Bentley\CONNECT1012\Organization-Civil\IDOT_Standards\Signs\Sheet Seeds\Civil_Named_Boundary_SheetSeeds.dgnlib



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

	USER NAME = Eric,L.Thomas	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					344	2025-2010-RS	LAKE	21	21A
		CHECKED -	REVISED -		TC-26			CONTRACT NO. 80B29				
	PLOT DATE = 8/20/2024	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	