

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**
FAP 345: US 20 (LAKE STREET)
FROM OAK AVENUE TO PARK AVENUE
SECTION: 7Y-WRS
WIDENING, AND RESURFACING
PROJECT NO.: *HSIP-0345(057)*
COOK COUNTY
C-91-259-11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 60N18	

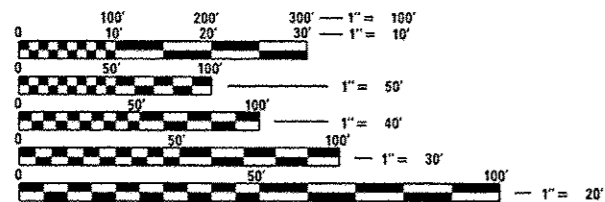
* 97 * 3 = 100

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN THE VILLAGES OF BARTLETT AND STREAMWOOD



LOCATION OF SECTION INDICATED THUS: —

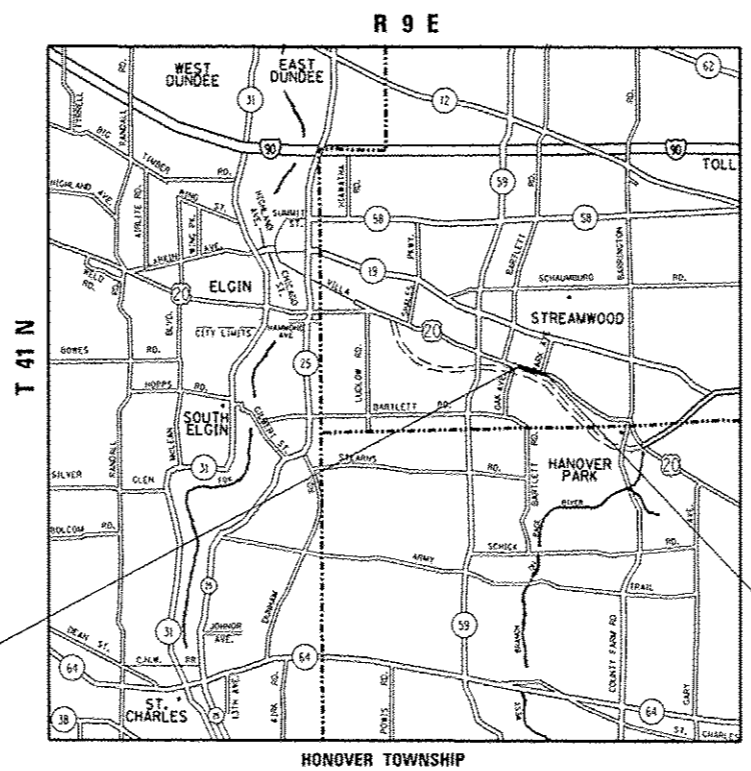


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 (OR 811)

PROJECT ENGINEER: JENPAI CHANG (847) 705 - 4432
PROJECT MANAGER: KEN ENG

CONTRACT NO. 60N18



PROJECT BEGINS
STATION 515 + 78.66

PROJECT ENDS
STATION 539 + 34.72

GROSS LENGTH AND NET LENGTH OF PROJECT = 2,356.06 FEET = 0.45 MILES

TRAFFIC DATA:
2011 ADT = 38,000
SPEED LIMIT = 40-45 MPH

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
SUBMITTED *April 1, 2013*
John F. ...
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
May 10, 2013
John D. Balaram, PE, Ia
ENGINEER OF DESIGN AND ENVIRONMENT
May 10, 2013
Omair Osman, PE, Ia
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

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LIST OF STATE STANDARDS:

STANDARD NO.	DESCRIPTION
000001	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001	TEMPORARY EROSION CONTROL SYSTEM
424001	PERPENDICULAR CURB RAMPS FOR SIDEWALKS

424006-01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-01	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-01	DEPRESSED CORNER RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482011-03	HMA SHLD, STRIPS / SHLDS, WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-03	FRAME AND LIDS TYPE 1
604091-02	FRAME AND GRATE TYPE 24
604036-02	GRATE TYPE 8
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRTE CURB AND GUTTER
701101-03	OFF-RD OPERATIONS, MULTILANE, 15' (4.5m) TO 24' (60mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE MORE THAN 15' (4.5m) AWAY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701421-05	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEED $>$ 45 MPH
701422-05	LANE CLOSURE, MULTILANE, FOR SPEEDS \geq 45 MPH TO 55 MPH
701426-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS \geq 45 MPH
701427-01	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS FOR SPEED \leq 40 MPH
701602-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-08	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-02	TRAFFIC CONTROL DEVICES
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16 FEET THROUGH 55 FEET
878001-09	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF BARTLETT, AND STREAMWOOD.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING, EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (60 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (60 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H)

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.

ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER, WITH THE TYPE SPECIFIED ON THE PLANS), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAMES AND LIDS UNLESS OTHERWISE SPECIFIED IN THE PLANS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

SIDEWALK REMOVAL AND P.C.C. SIDEWALK, 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER (OR TECHNICIAN), AT (847) 741-9857 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM AND FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ADJUTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

FILE NAME :	USER NAME :	DESIGNED :	REVISED :	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES U.S. 20 (OAK AVE. TO PARK AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p\work\pvidot\galbenj\4084020\PI5	408-shs-cover.dgn	DRAWN :	REVISED :			345	7Y-WRS	COOK	97	2
	PLOT SCALE : 100.0000 ' / 1"	CHECKED :	REVISED :			CONTRACT NO. 60N18				
	PLOT DATE : 4/3/2013	DATE :	REVISED :			SCALE:	SHEET NO.	OF	SHEETS	STA.

FOR STORM SEWER CONSTRUCTED UNDER ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF BURIED STRUCTURES ACCORDING TO THE STATION OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT, UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

" POROUS GRANULAR EMBANKMENT, SUBGRADE (P.G.E.S.) HAS BEEN PROVIDED FOR USE FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIAL UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS AND GUIDLINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES OR EMBANKMENT AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR".

THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.

THE CONTRACTOR SHALL CALL "JULIE" (800) 892-0123 OR 811. IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE RELATED PAY ITEMS SUCH AS FOUNDATION, CONDUIT, HANHOLE, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACE SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDD IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS 250 AND 252 RESPECTIVELY.

IF THIS CONTRACT REQUIRES THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR SHALL BE RESPONSIBLE AT HIS/HER OWN EXPENSE FOR LOCATING EXISTING IDOT ELECTRICAL FACILITIES PRIOR TO PERFORMING ANY WORK. IF THIS CONTRACT DOES NOT REQUIRE THE SERVICES OF AN ELECTRICAL CONTRACTOR, THE CONTRACTOR MAY REQUEST ONE FREE LOCATE FOR EXISTING IDOT ELECTRICAL FACILITY FROM THE DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR PRIOR TO THE START OF ANY WORK. ADDITIONAL REQUESTS SHALL BE AT THE EXPENSE OF THE CONTRACTOR. THE LOCATIONS OF UNDERGROUND TRAFFIC FACILITIES DOES NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO REPAIR ANY FACILITIES DAMAGED DURING CONSTRUCTION AT THEIR EXPENSE.

THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.

TEMPORARY TRAFFIC SIGNAL NOTES

ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.

ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1. INSTALLED IN A NEMA TS2 CABINET, ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.

ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.

ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT AT NO ADDITIONAL COST TO THE CONTRACT.

THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE THE TURN ON.

ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12 INCHES (300MM). TRAFFIC SIGNAL SECTIONS SHALL BE LED WITH EXPANDABLE VIEW, UNLESS OTHERWISE APPROVED BY THE ENGINEER. PEDESTRIAN SIGNAL HEADS SHALL BE LIGHT EMITTING DIODE (LED) PEDESTRIAN COUNTDOWN SIGNAL HEADS EXCEPT WHEN A TEMPORARY TRAFFIC SIGNAL IS INSTALLED AT AN INTERSECTION INTERCONNECTED WITH A RAILROAD GRADE CROSSING. WHEN A TEMPORARY TRAFFIC SIGNAL IS INSTALLED AT AN INTERSECTION INTERCONNECTED WITH A RAILROAD GRADE CROSSING, LIGHT EMITTING DIODE (LED) PEDESTRIAN SIGNAL HEADS SHALL BE FURNISHED. THE TEMPORARY TRAFFIC SIGNAL HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH EXTRA CABLE LENGTH TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.

ALL TEMPORARY TRAFFIC SIGNAL INSTALLATIONS SHALL HAVE UNINTERRUPTABLE POWER SUPPLY (UPS). THE UPS CABINET SHALL BE MOUNTED TO THE TEMPORARY TRAFFIC SIGNAL CABINET AND MEET THE REQUIREMENTS OF THE UNINTERRUPTABLE POWER SUPPLY IN DIVISIONS 800 AND 1000 OF THESE SPECIFICATIONS.

TRAFFIC SIGNAL MANAGEMENT SYSTEM SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.

DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. PEDESTRIAN PUSH BUTTONS SHALL BE PROVIDED FOR ALL PEDESTRIAN SIGNAL HEADS/PHASES AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER. DETECTION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN TEMPORARY TRAFFIC SIGNAL PAY ITEM.

WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTENING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE PRESERVATION OF EXISTING TREES IS OF UTMOST IMPORTANCE TO THE VILLAGES OF BARTLETT AND STREAMWOOD. ALL TREE PROTECTION, TREE REMOVAL, PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE CONSTRUCTION OPERATION COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL ERECT A TEMPORARY FENCE AROUND ALL TREES WITHIN THE CONSTRUCTION AREA TO ESTABLISH A "TREE PROTECTION ZONE" BEFORE ANY WORK BEGINS OR ANY MATERIAL IS DELIVERED TO THE JOBSITE. NO WORK IS TO BE PERFORMED (OTHER THAN ROOT PRUNING), MATERIAL STORED OR VEHICLES DRIVEN OR PARKED WITHIN THE "TREE PROTECTION ZONE". REMOVE PROTECTIVE TEMPORARY FENCE ONLY AFTER ALL CONSTRUCTION WORK HAS BEEN COMPLETED.

TREE ROOT PRUNING IS TO BE USED ON EXISTING TREES TO PREVENT THE RIPPING UP OF ROOTS WHEN TRENCHING OR EXCAVATION IS WITHIN THE ROOT ZONE OF ADJACENT TREES TO REMAIN. SUPPLEMENTAL WATERING OF TREES SHOULD BEGIN IMMEDIATELY AFTER ROOT PRUNING OF THE TREES HAS OCCURRED.

FILE NAME :	USER NAME : galbony	DESIGNED -	REVISED -
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	PLOT DATE : 4/3/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
U.S. 20 (OAK AVE. TO PARK AVE.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	3
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

153

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	TRAFFIC SIGNALS						
				ROADWAY ITEMS 90% FEDERAL 10% STATE 0004	90% FEDERAL 6.7% STATE 3.3% BARTLETT 0021	90% FEDERAL 6.7% STATE 3.3% STREAMWOOD 0021	90% FEDERAL 10% STATE INTERCONNECT 0021	100% STREAMWOOD 0021		
*20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	74	74						
*20101000	TEMPORARY FENCE	FOOT	150	150						
*20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	5	5						
*20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	2	2						
20200100	EARTH EXCAVATION	CU YD	2820	2820						
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	338	338						
*20800150	TRENCH BACKFILL	CU YD	511	511						
*21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	1065	1065						
*21101645	TOPSOIL FURNISH AND PLACE, 12"	SO YD	1040	1040						
*21101695	TOPSOIL FURNISH AND PLACE, 30"	SO YD	629	629						
*25000200	SEEDING, CLASS 2	ACRE	0.4	0.4						
*25000210	SEEDING, CLASS 2A	ACRE	0.55	0.55						
*25000400	NITROGEN FERTILIZER NUTRIENT	POUND	85.5	85.5						
*25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	85.5	85.5						
*25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	85.5	85.5						

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
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25100630	EROSION CONTROL BLANKET	SO YD	4603	4603						
25200200	SUPPLEMENTAL WATERING	UNIT	50	50						
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	95	95						
28000305	TEMPORARY DITCH CHECKS	FOOT	496	496						
28000400	PERIMETER EROSION BARRIER	FOOT	4716	4716						
28000500	INLET AND PIPE PROTECTION	EACH	59	59						
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SO YD	1893	1893						
35501300	HOT-MIX ASPHALT BASE COURSE, 4"	SO YD	1590	1590						
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	1232	1232						
35600716	HOT-MIX ASPHALT BASE COURSE WIDENING, 10"	SO YD	793	793						
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	13	13						
40600300	AGGREGATE (PRIME COAT)	TON	65	65						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	25	25						
40600895	CONSTRUCTING TEST STRIP	EACH	2	2						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	151	151						

* SPECIALTY ITEMS
 NON-PARTICIPATING ITEMS

30

FILE NAME :	USER NAME : gbas/jr	DESIGNED -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
FAP 345/ US 20 (LAKE ST.)--OAK AVENUE TO PARK AVENUE			
SCALE:	SHEET NO.	OF	SHEETS
			STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	4
CONTRACT NO. 60N18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	TRAFFIC SIGNALS					
				ROADWAY ITEMS 90% FEDERAL 10% STATE 0004	90% FEDERAL 6.7% STATE 3.3% BARTLETT 0021	90% FEDERAL 6.7% STATE 3.3% STREAMWOOD 0021	90% FEDERAL 10% STATE INTERCONNECT 0021	100% STREAMWOOD 0021	
40603240	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N90	TON	2048	2048					
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	317	317					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	1910	1910					
42001300	PROTECTIVE COAT	SO YD	2913	2913					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	13966	13966					
42400800	DETECTABLE WARNINGS	SO FT	157	157					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	16253	16253					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	441	441					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1392	1392					
44004250	PAVED SHOULDER REMOVAL	SO YD	1017	1017					
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	244	244					
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	147	147					
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	98	98					
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	1					

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	TRAFFIC SIGNALS					
				ROADWAY ITEMS 90% FEDERAL 10% STATE 0004	90% FEDERAL 6.7% STATE 3.3% BARTLETT 0021	90% FEDERAL 6.7% STATE 3.3% STREAMWOOD 0021	90% FEDERAL 10% STATE INTERCONNECT 0021	100% STREAMWOOD 0021	
50102400	CONCRETE REMOVAL	CU YD	1	1					
50300225	CONCRETE STRUCTURES	CU YD	4.4	4.4					
50300300	PROTECTIVE COAT	SO YD	7.30	7.30					
50500505	STUD SHEAR CONNECTORS	EACH	18	18					
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	580	580					
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	1	1					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	419	419					
550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	234	234					
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	313	313					
550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	583	583					
550A0140	STORM SEWERS, CLASS A, TYPE 1 30"	FOOT	1169	1169					
550A0160	STORM SEWERS, CLASS A, TYPE 1 36"	FOOT	156	156					
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	14	14					
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	701	701					
550A0430	STORM SEWERS, CLASS A, TYPE 2 30"	FOOT	1094	1094					

* SPECIALTY ITEMS
 NON-PARTICIPATING ITEMS

FILE NAME:
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 DESIGNED:
 DRAWN:
 CHECKED:
 DATE:
 PLOT SCALE: 1000000 / 1in
 PLOT DATE: 4/4/2013

REVISED:
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 FAP 345/ US 20 (LAKE ST.)--OAK AVENUE TO PARK AVENUE
 SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 345	SECTION 7Y-WRS	COUNTY COOK	TOTAL SHEETS 97	SHEET NO. 5
CONTRACT NO. 60N18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		TRAFFIC SIGNALS				
				ROADWAY ITEMS 90% FEDERAL 10% STATE 0004	90% FEDERAL 6.7% STATE 3.3% BARTLETT 0021	90% FEDERAL 6.7% STATE 3.3% STREAMWOOD 0021	90% FEDERAL 10% STATE INTERCONNECT 0021	100% STREAMWOOD 0021
*87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12	8	4			
*87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	14		14			
*87900200	DRILL EXISTING HANDHOLE	EACH	9	3	5	1		
*88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4	2	2			
*88102747	PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1		1			
*88600100	DETECTOR LOOP, TYPE I	FOOT	445	210	235			
*88800100	PEDESTRIAN PUSH-BUTTON	EACH	6	2	4			
*89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2	1	1			
*89500100	RELOCATE EXISTING SIGNAL HEAD	EACH	5	3	2			
*89501150	RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	2	2				
*89501300	RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE	EACH	1		1			
*89501400	RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	1					1
*89502200	MODIFY EXISTING CONTROLLER	EACH	2	1	1			

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		TRAFFIC SIGNALS				
				ROADWAY ITEMS 90% FEDERAL 10% STATE 0004	90% FEDERAL 6.7% STATE 3.3% BARTLETT 0021	90% FEDERAL 6.7% STATE 3.3% STREAMWOOD 0021	90% FEDERAL 10% STATE INTERCONNECT 0021	100% STREAMWOOD 0021
*89502210	MODIFY EXISTING CONTROL CABINET	EACH	2	1	1			
*89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	991	715	276			
*89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	2	1	1			
*89502376	REBUILD EXISTING HANDHOLE	EACH	2		2			
*89502380	REMOVE EXISTING HANDHOLE	EACH	6	2	1	3		
*89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	3	2	1			
*A2000316	TREE, ACER MIYABEI MORTON (STATE STREET MIYABEI MAPLE), 2" CALIPER, BALLED AND BURLAPPED	EACH	3	3				
*A2002916	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED	EACH	6	6				
*A2005020	TREE, GYMNOCLADUS DIOICUS (KENTUCKY COFFEETREE), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	10	10				
*B2001616	TREE, CRATAEGUS CRUSGALLI INERMIS (THORN LESS COCKSPUR HAWTHORN), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED	EACH	13	13				

* SPECIALTY ITEMS
□ NON-PARTICIPATING ITEMS

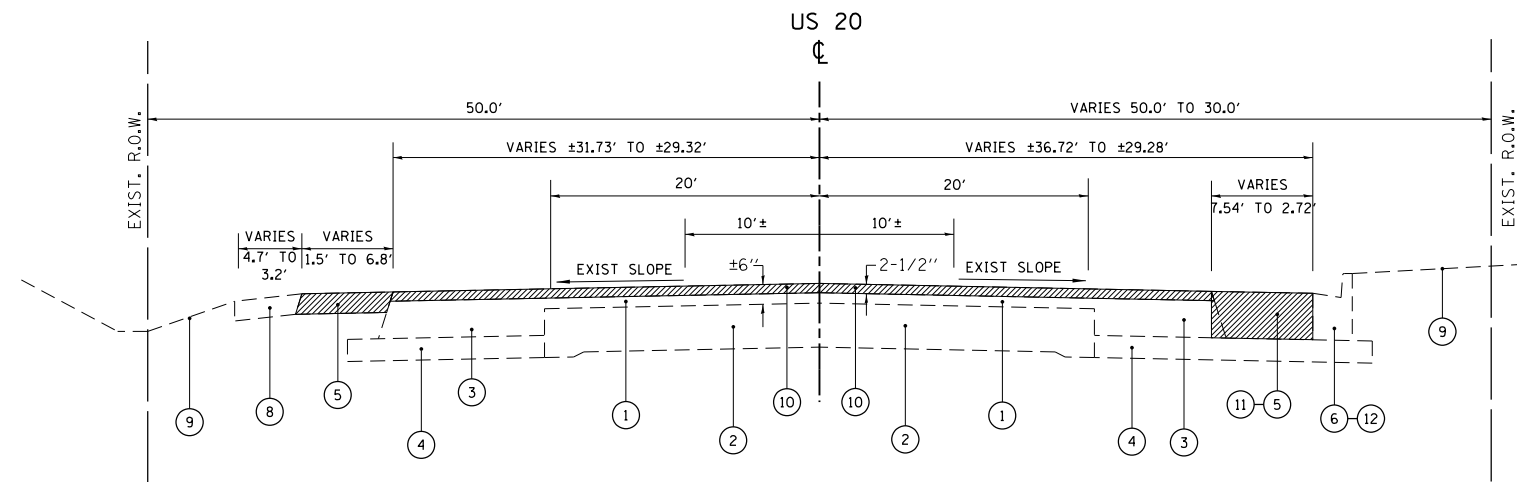
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REVISOR -
REVISOR -
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REVISOR -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

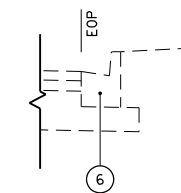
SUMMARY OF QUANTITIES
FAP 345/ US 20 (LAKE ST.)--OAK AVENUE TO PARK AVENUE
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 345	SECTION 7Y-WRS	COUNTY COOK	TOTAL SHEETS 97	SHEET NO. 7A
CONTRACT NO. 60N18				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



U.S. ROUTE 20 EXISTING TYPICAL

STA. 515+78.66 TO STA. 520+04.71

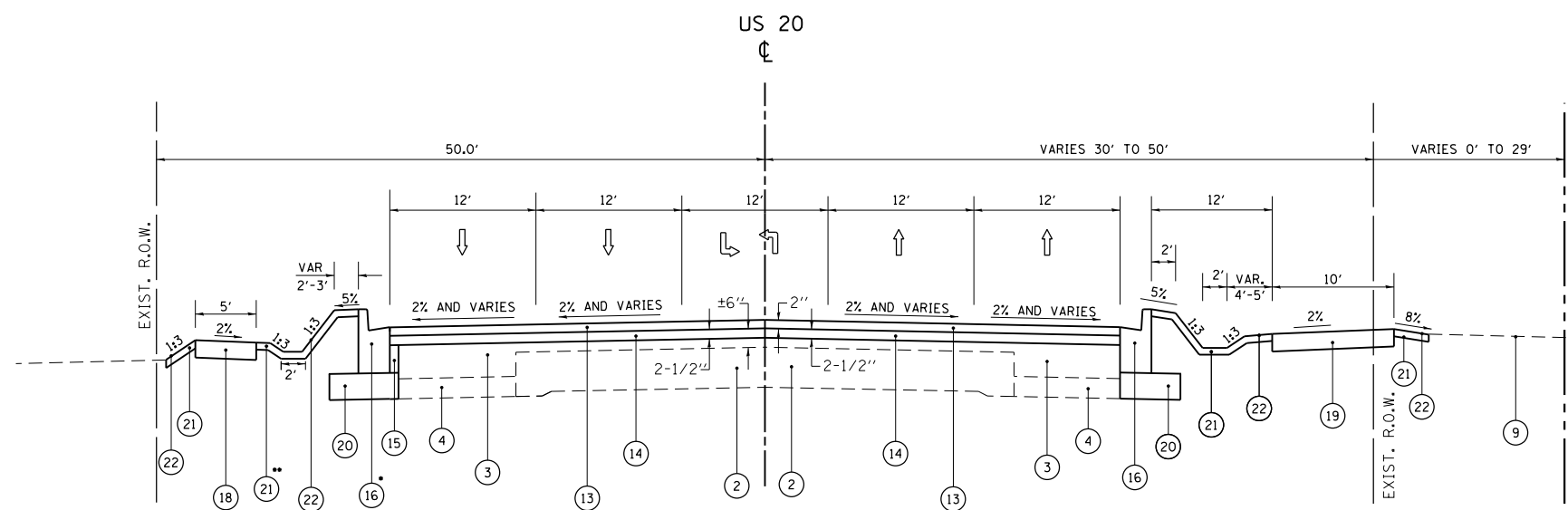


DETAIL B

EXISTING CURB AND GUTTER, TYPE B-6.24
OAK AVE. TO CLUB TREE DR.

LEGEND

- ① EXISTING HMA SURFACING, ±6"
- ② EXISTING PCC PAVEMENT, ±9"
- ③ EXISTING HMA BASE COURSE WIDENING
- ④ EXISTING SUB-BASE GRAN. MATERIAL, TY B, 4"
- ⑤ EXISTING HMA SHOULDER, ±9"
- ⑥ EXISTING COMB. C&G, TYPE B-6.24
- ⑦ EXISTING COMB. C&G, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING GROUND LINE
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2"
- ⑪ PROPOSED PAVED SHOULDER REMOVAL
- ⑫ PROPOSED COMB. CURB AND GUTTER REMOVAL
- ⑬ PROP. POLY HMA SURF. CSE, MIX "F", N90, ±2" (OVERLAY)
- ⑭ PROP. POLY. HMA BINDER COURSE, IL-19.0, N90, 2-1/2"
- ⑮ PROP. HMA BASE COURSE WIDENING
- ⑯ PROP. COMB. CONC CURB AND GUTTER, TYPE B-6.24
- ⑰ PROP. HMA SHOULDER
- ⑱ PROPOSED PCC SIDEWALK, 5"
- ⑲ PROPOSED SHARED PATH, 10'
- ⑳ PROPOSED AGG. SUBGRADE IMPROVEMENT, 12"
- ㉑ TOPSOIL FURNISH AND PLACE, 4"
- ㉒ SEEDING, CLASS 2A



U.S. ROUTE 20 PROPOSED TYPICAL

STA. 515+78.66 TO STA. 520+04.71

*PROP. CURB AND GUTTER
MATCH EXIST. AT STA. 534+03

**SWALE BEGINS AT STA. 511+00

HMA SHOULDER
ROMA JEAN PKWY TO PARK AVE

NOTE:

ADDITIONAL SUBBASE GRANULAR MATERIAL UNDER CURB AND GUTTER AND SHARED PATH SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "AGGREGATE SUBGRADE IMPROVEMENT, 12".

ANY SAW CUTTING REQUIRED TO REMOVE AN ITEM ADJACENT TO AN ITEM TO BE SAVED WILL BE CONSIDERED AS PART OF THE REMOVAL ITEM AND WILL NOT BE PAID FOR SEPARATELY.

"THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING".

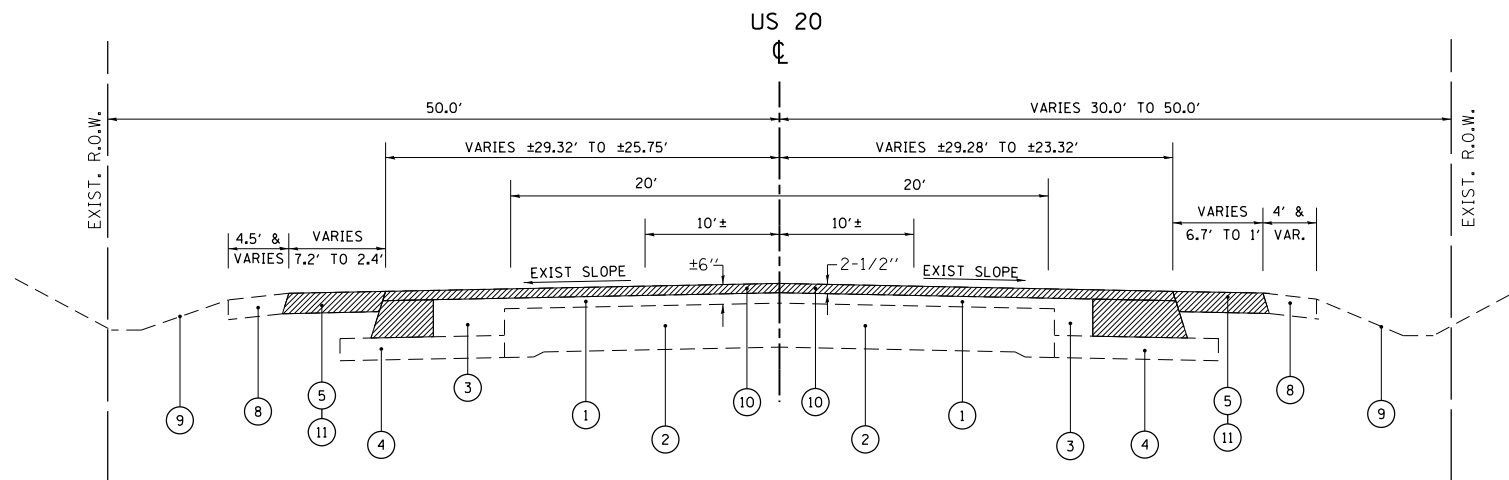
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PLOT SCALE = \$SCALESHORT*	CHECKED RJW	REVISED - 05/18/2011
PLOT DATE = 4/3/2013	DATE 03/25/2011	REVISED - 06/23/2011

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 20 (LAKE STREET)
EXISTING AND PROPOSED TYPICAL CROSS SECTIONS**

SCALE: NTS SHEET NO. 3 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	9
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60N18	

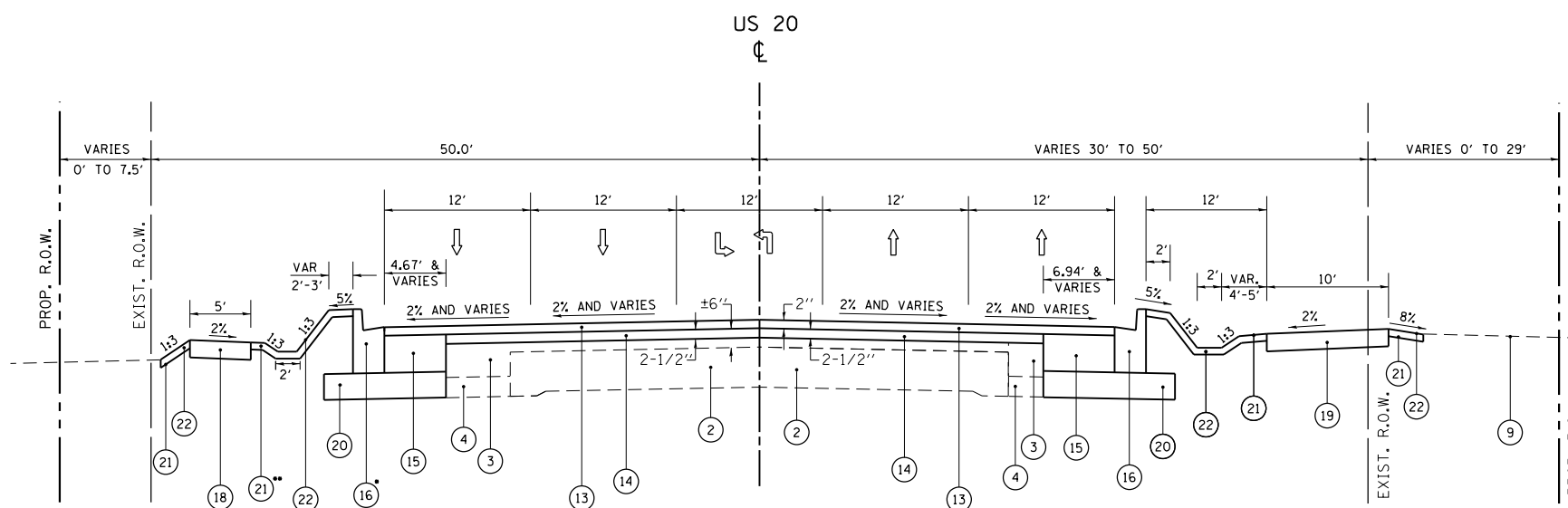


U.S. ROUTE 20 EXISTING TYPICAL

STA. 520+04.71 TO STA. 527+65.56

LEGEND

- ① EXISTING HMA SURFACING, ±6"
- ② EXISTING PCC PAVEMENT, ±9"
- ③ EXISTING HMA BASE COURSE WIDENING
- ④ EXISTING SUB-BASE GRAN. MATERIAL, TY B, 4"
- ⑤ EXISTING HMA SHOULDER, ±9"
- ⑥ EXISTING COMB. C&G, TYPE B-6.24
- ⑦ EXISTING COMB. C&G, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING GROUND LINE
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2"
- ⑪ PROPOSED PAVED SHOULDER REMOVAL
- ⑫ PROPOSED COMB. CURB AND GUTTER REMOVAL
- ⑬ PROP. POLY HMA SURF. CSE, MIX "F", N90, ±2" (OVERLAY)
- ⑭ PROP. POLY. HMA BINDER COURSE, IL-19.0, N90, 2-1/2"
- ⑮ PROP. HMA BASE COURSE WIDENING
- ⑯ PROP. COMB. CONC CURB AND GUTTER, TYPE B-6.24
- ⑰ PROP. HMA SHOULDER
- ⑱ PROPOSED PCC SIDEWALK, 5"
- ⑲ PROPOSED SHARED PATH, 10"
- ⑳ PROPOSED AGG. SUBGRADE IMPROVEMENT, 12"
- ㉑ TOPSOIL FURNISH AND PLACE, 4"
- ㉒ SEEDING, CLASS 2A



U.S. ROUTE 20 PROPOSED TYPICAL

STA. 520+04.71 TO STA. 527+65.56

NOTE:

ADDITIONAL SUBBASE GRANULAR MATERIAL UNDER CURB AND GUTTER AND SHARED PATH SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "AGGREGATE SUBGRADE IMPROVEMENT, 12".

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"THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING".

*PROP. CURB AND GUTTER MATCH EXIST. AT STA. 534+03

**SWALE BEGINS AT STA. 511+00

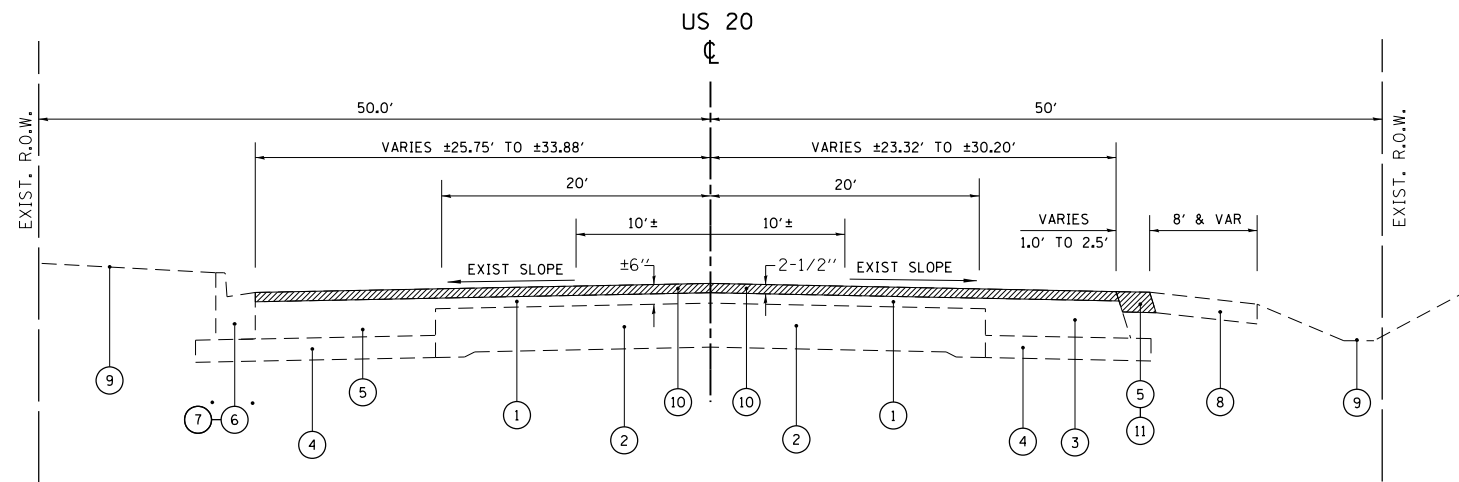
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PLOT DATE = 4/3/2013	DATE 03/25/2011	REVISED - 06/23/2011

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 20 (LAKE STREET)
EXISTING AND PROPOSED TYPICAL CROSS SECTIONS**

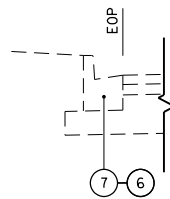
SCALE: NTS SHEET NO. 3 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	10
CONTRACT NO.			60N18	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



U.S. ROUTE 20 EXISTING TYPICAL

STA. 527+65.56 TO STA. 539+34.72

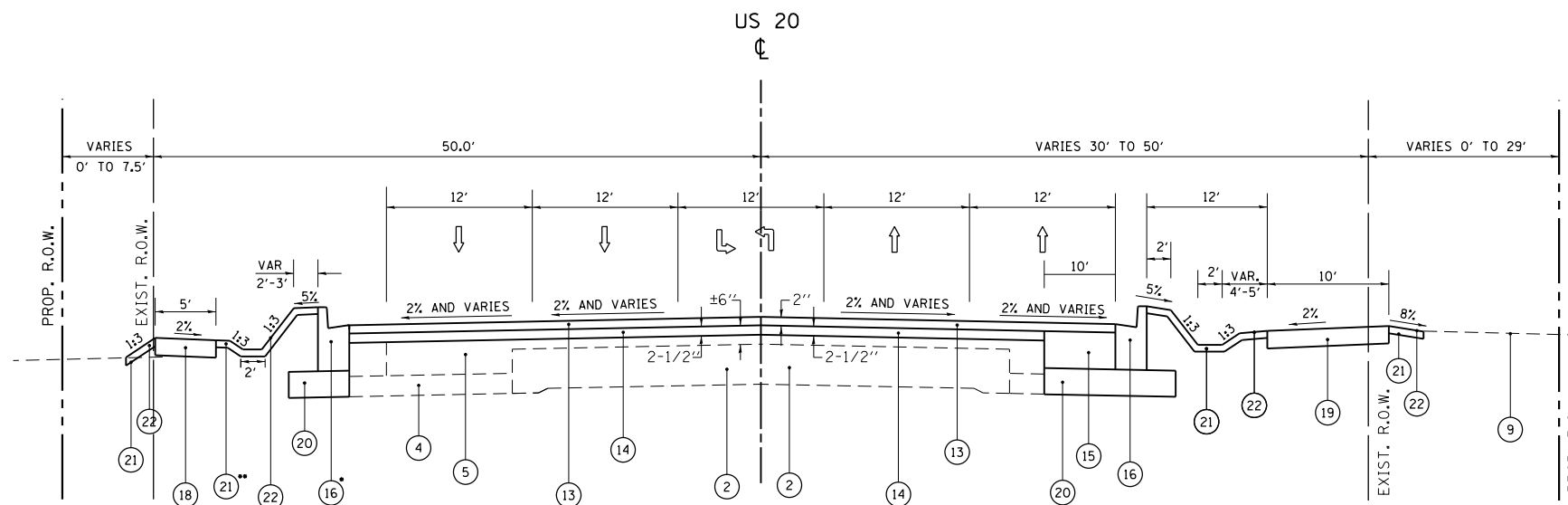


DETAIL A

- EXISTING CURB AND GUTTER, TYPE B-6.12
S. VALLEY LN TO ROMAJEAN PKWY
- EXISTING CURB AND GUTTER, TYPE B-6.24
ROMA JEAN PKWY TO PARK AVE.

LEGEND

- ① EXISTING HMA SURFACING, ±6"
- ② EXISTING PCC PAVEMENT, ±9"
- ③ EXISTING HMA BASE COURSE WIDENING
- ④ EXISTING SUB-BASE GRAN. MATERIAL, TY B, 4"
- ⑤ EXISTING HMA SHOULDER, ±9"
- ⑥ EXISTING COMB. C&G, TYPE B-6.24
- ⑦ EXISTING COMB. C&G, TYPE B-6.12
- ⑧ EXISTING AGGREGATE SHOULDER
- ⑨ EXISTING GROUND LINE
- ⑩ PROPOSED HMA SURFACE REMOVAL, 2-1/2"
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- ⑫ PROPOSED COMB. CURB AND GUTTER
- ⑬ PROP. POLY HMA SURF. CSE, MIX "F", N90, ±2" (OVERLAY)
- ⑭ PROP. POLY. HMA BINDER COURSE, IL-19.0, N90, 2-1/2"
- ⑮ PROP. HMA BASE COURSE WIDENING
- ⑯ PROP. COMB. CONC CURB AND GUTTER, TYPE B-6.24
- ⑰ PROP. HMA SHOULDER
- ⑱ PROPOSED PCC SIDEWALK, 5"
- ⑲ PROPOSED SHARED PATH, 10"
- ⑳ PROPOSED AGG. SUBGRADE IMPROVEMENT, 12"
- ㉑ TOPSOIL FURNISH AND PLACE, 4"
- ㉒ SEEDING, CLASS 2A



U.S. ROUTE 20 PROPOSED TYPICAL

STA. 527+65.56 TO STA. 539+34.72

*PROP. CURB AND GUTTER
MATCH EXIST. AT STA. 534+03

**SWALE BEGINS AT STA. 511+00

HMA SHOULDER
ROMA JEAN PKWY TO PARK AVE

NOTE:

ADDITIONAL SUBBASE GRANULAR MATERIAL UNDER CURB AND GUTTER AND SHARED PATH SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED AS INCLUDED IN THE COST PER SQUARE YARD OF "AGGREGATE SUBGRADE IMPROVEMENT, 12".

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"THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING".

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PLOT DATE = 4/3/2013	DATE 03/25/2011	REVISED - 06/23/2011

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 20 (LAKE STREET)
EXISTING AND PROPOSED TYPICAL CROSS SECTIONS**

SCALE: NTS SHEET NO. 3 OF 5 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	11
FED. ROAD DIST. NO.			CONTRACT NO. 60N18	
ILLINOIS FED. AID PROJECT				

EARTHWORK SCHEDULE (COOK COUNTY)

① LOCATION	② EARTH EXCAVATION	③ EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	④ EMBANKMENT	⑤ EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STATION	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD
U.S. 20 (LAKE ST.)				
STA. 515178.66 TO STA. 516+00	14.82	12.60	4.74	+7.86
STA. 516+00 TO STA. 517+00	50.00	42.50	43.52	-1.02
STA. 517+00 TO STA. 517+65.70	28.59	24.30	31.63	-7.33
STA. 517+65.70 TO STA. 518+00	16.20	13.77	17.15	-3.38
STA. 518+00 TO STA. 519+00	33.33	28.33	88.89	-60.56
STA. 519+00 TO STA. 520+00	80.56	68.48	65.74	+2.74
STA. 520+00 TO STA. 520+41.08	50.97	43.32	1.52	+41.80
STA. 520+41.08 TO STA. 520+77.46	42.11	35.79	0.00	+35.79
STA. 520+77.46 TO STA. 521+00	31.71	26.95	0.40	+26.95
STA. 521+00 TO STA. 522+00	162.04	137.73	33.33	+104.40
STA. 522+00 TO STA. 523+00	177.78	151.11	66.67	+84.44
STA. 523+00 TO STA. 524+00	185.19	157.41	71.30	+86.11
STA. 524+00 TO STA. 524+31.49	51.90	44.12	22.74	+21.38
STA. 524+31.49 TO STA. 525+00	105.94	90.05	39.96	+50.09
STA. 525+00 TO STA. 525+26.14	42.36	36.01	11.38	+24.63
STA. 525+26.14 TO STA. 526+00	79.33	67.43	30.78	+36.65
STA. 526+00 TO STA. 526+46.43	88.99	75.64	15.05	+60.59
STA. 526+46.43 TO STA. 527+00	113.59	96.55	9.92	+86.63
STA. 527+00 TO STA. 528+00	131.48	111.76	15.74	+96.02
STA. 528+00 TO STA. 528+13.64	7.96	6.77	2.65	+4.12
STA. 528+13.64 TO STA. 529+00	71.97	61.17	19.99	+41.18
STA. 529+00 TO STA. 529+40.92	43.19	36.71	6.82	+29.89
STA. 529+40.92 TO STA. 530+00	62.91	53.47	27.90	+25.57
STA. 530+00 TO STA. 530+65.68	54.13	46.01	40.14	+5.87
STA. 530+65.68 TO STA. 531+00	21.61	18.37	37.82	-19.45
STA. 531+00 TO STA. 531+53.56	36.70	31.20	59.51	-28.31
STA. 531+53.56 TO STA. 532+00	13.40	11.39	0.70	+10.69
STA. 532+00 TO STA. 532+58.38	61.08	51.92	3.24	+48.68
STA. 532+58.38 TO STA. 533+00	55.49	47.17	0.77	+46.40
STA. 533+00 TO STA. 533+17.70	26.06	22.15	0.16	+21.99
SUB-TOTAL (1)	1941.39	1650.18	770.16	+880.42

EARTHWORK SCHEDULE CONTINUATION (COOK COUNTY)

① LOCATION	② EARTH EXCAVATION	③ EARTH EXCAVATION ADJUSTED FOR SHRINKAGE	④ EMBANKMENT	⑤ EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STATION	CUBIC YARD	CUBIC YARD	CUBIC YARD	CUBIC YARD
U.S. 20 (LAKE ST.)				
STA. 533+17.70 TO STA. 533+61.79	57.15	48.58	0.41	+48.17
STA. 533+61.79 TO STA. 534+00	55.19	46.91	0.71	+46.20
STA. 534+00 TO STA. 535+00	172.22	146.39	0.93	+145.46
STA. 535+00 TO STA. 536+00	179.63	152.69	0.00	+152.69
STA. 536+00 TO STA. 537+00	181.48	154.26	0.00	+154.26
STA. 537+00 TO STA. 537+85	110.97	94.32	2.36	+91.96
STA. 537+85 TO STA. 538+00	8.33	7.08	2.08	+5.00
STA. 538+00 TO STA. 538+15.82	4.10	3.48	3.52	-0.04
STA. 538+15.82 TO STA. 538+40.82	9.26	7.87	3.01	+4.86
STA. 538+40.82 TO STA. 538+65.82	8.80	7.48	1.16	+6.32
STA. 538+65.82 TO STA. 539+00	21.20	18.02	0.95	+17.07
STA. 539+00 TO STA. 539+34.72	19.93	16.94	0.96	+15.98
STA. 539+34.72 TO STA. 540+00	13.90	11.82	3.02	+8.80
STA. 540+00 TO STA. 540+64.15	35.64	30.29	1.00	+29.29
SUB-TOTAL (2)	877.80	746.74	20.11	+726.02

SUMMARY

SUB-TOTAL (1)	1941.39	1650.18	770.16	+880.42
SUB-TOTAL (2)	877.80	746.74	20.11	+726.02
TOTAL	2819.19	2396.92	790.27	+1606.44

COLUMN 1: LOCATION FROM PLAN

COLUMN 2: QUANTITY OF EARTH EXCAVATION (CUT) FROM CROSS SECTIONS

COLUMN 3: QUANTITY OF EARTH EXCAVATION (CUT) ADJUSTED FOR A SHRINKAGE FACTOR OF 15%

COLUMN 4: QUANTITY OF EMBANKMENT (FILL) FROM CROSS SECTIONS

COLUMN 5: (COLUMN 3 - COLUMN 4)
 (+) = QUANTITY OF FURNISHED EXCAVATION NEEDED
 (-) = QUANTITY TO BE WASTED

TREE REMOVAL SCHEDULE (COOK COUNTY)
U.S. 20 (LAKE ST.)

LOCATION STATION	OFFSTET (FEET)	DESCRIPTION			REASON FOR REMOVAL
		CIRCUMFERENCE (INCH)	6 TO 15 UNITS	OVER 15 UNITS	
516+13.34	54.63 (RT)	31.42	10		SHARED PATH
516+97.95	54.50 (RT)	31.42	10		SHARED PATH
519+05.08	58.63 (RT)	18.82	6		SHARED PATH
519+33.22	58.93 (RT)	25.13	8		SHARED PATH
519+61.33	58.25 (RT)	31.42	10		SHARED PATH
521+04.25	41.05 (RT)	47.12	15		DITCH LINE
521+54.62	39.46 (RT)	47.12	15		DITCH LINE
TOTAL			74		

TREE PROTECTION SCHEDULE (COOK COUNTY)
U.S. 20 (LAKE ST.)

LOCATION STATION	OFFSTET (FEET)	QUANTITY (EACH)	REASON FOR PROTECTION
518+76.95	59.25 (RT)	1	NOT TO DAMAGE
TOTAL		1	

HOT-MIX ASPHALT MIXTURE REQUIREMENTS
THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT

MIXTURE TYPE	AIR VOIDS (%) @ NDES
RESURFACING	
POLY HMA SURFACE COURSE MIX "F", N90 (IL-9.5mm), 2"	4% @ 90 GYR.
POLY HMA BINDER COURSE IL-19, N90, 2-1/2"	4% @ 90 GYR.
WIDENING	
POLY HMA SURFACE COURSE MIX "F", N90 (IL-9.5mm), 2"	4% @ 90 GYR.
POLY HMA BINDER COURSE IL-19, N90, 2-1/2"	4% @ 90 GYR.
HMA BASE COURSE WIDENING (HMA BINDER IL-19mm), 10"	4% @ 90 GYR.
SHOULDER	
HMA SURFACE COURSE MIX "D", N70 (IL-9.5mm), 2"	4% @ 70 GYR.
HMA SHOULDER (HMA BINDER IL-19 mm), 8"	4% @ 70 GYR.
HOT-MIX ASPHALT PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 mm), 13"	4% @ 70 GYR.
SHARED PATH	
HMA SURFACE COURSE MIX "D", N50 (IL-9.5mm), 2"	4% @ 50 GYR.
HMA BINDER COURSE (HMA BINDER IL-19 mm), 4"	4% @ 50 GYR.
HMA DRIVEWAY (C.E.)	
HMA SURFACE COURSE MIX "D", N50, (IL-9.5mm), 2"	4% @ 50 GYR.
HMA BASE COURSE (IL-19mm), 8"	4% @ 50 GYR.

NOTE:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 POUND PER SQUARE YARD-INCH

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

"FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS".

PFP SPECIAL PROVISIONS APPLIES TO POLY. HMA SURF. CSE., MIX "F", N90 MIX ONLY.

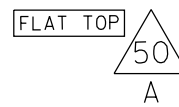
FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SCHEDULE OF QUANTITIES U.S. 20 (OAK AVE. TO PARK AVE.)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwwork\pwwork\galbenjr\d0304020\PI57408-shr-cover.dgn		DRAWN -	REVISED -					345	7Y-WRS	COOK	97	13
\$MODELNAME\$		PLOT SCALE = 100.0000' / in.	CHECKED -		REVISED -	CONTRACT NO. 60N18			ILLINOIS FED. AID PROJECT			
		PLOT DATE = 4/3/2013	DATE -		REVISED -	SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.


STRUCTURES


1	MH, T-A, 5 FT. DIA. W/ T-1 F CL STA. 515+67.48, 46.33 FT. (RT) T.G. 813.38 INV. 810.17 W (EXIST) INV. 808.13 E	14	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 526+18.66, 38.91 FT. (RT) T.G. 805.69 INV. 803.04 N INV. 801.96 W INV. 801.06 E	26	CB, T-C, W/ T-24 F&G STA. 535+35.77, 31 FT. (RT) T.G. 800.02 INV. 796.30 S	39	CB, T-C, W/ T-24 F&G STA. 520+15.41, 33.17 FT. (LT) T.G. 810.78 INV. 804.60 N
2	MH-T-A, 5 FT. DIA. W/ T-8 GRATE STA. 517+35.75, 38.58 FT. (RT) T.G. 811.36 INV. 807.86 W INV. 806.69 E INV. 808.65 N	15	CB, T-C, W/ T-24 F&G STA. 526+18.66, 31 FT. (RT) T.G. 806.56 INV. 803.10 S	FLAT TOP 27	MH, T-A, 4 FT. DIA. W/ T-8 GRATE STA. 535+35.77, 38.90 FT. (RT) T.G. 799.15 INV. 796.23 N INV. 795.90 W INV. 795.73 E	40	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 520+15.41, 39.80 FT. (LT) T.G. 810.19 INV. 804.14 S INV. 802.94 W INV. 802.44 E
3	CB, T-C, W/ T-24 F&G STA. 517+35.75, 31 FT. (RT) T.G. 812.18 INV. 806.69 S	16	CB, T-C, W/ T-24 F&G STA. 529+14.18, 31 FT. (RT) T.G. 803.98 INV. 800.52 S	FLAT TOP 28	CB, T-4, 4 FT. DIA. W/ T-24 F&G STA. 537+67.54, 31 FT. (RT) T.G. 798.71 INV. 794.59 W INV. 794.59 E INV. 795.05 S	41	CB, T-C, W/ T-24 F&G STA. 522+31.47, 31 FT. (LT) T.G. 809.64 INV. 803.32 N
4	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 520+25.18, 38.58 FT. (RT) T.G. 808.40 INV. 807.11 N INV. 806.20 W INV. 804.73 E	FLAT TOP 17	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 529+14.18, 40.10 FT. (RT) T.G. 802.68 INV. 800.46 N INV. 798.22 W INV. 798.05 E	29	CB, T-C, W/ T-8 GRATE STA. 537+67.54, 39.78 FT. (RT) T.G. 797.88 INV. 795.14 N	42	MH, T-A, 5 FT. DIA. W/ T-1F CL STA. 522+31.47, 36.48 FT. (LT) T.G. 809.46 INV. 803.29 S INV. 801.88 W INV. 801.25 E
5	CB, T-C, W/ T-24 F&G STA. 520+25.18, 31 FT. (RT) T.G. 810.64 INV. 807.18 S	18	CB, T-C, W/ T-24 F&G STA. 530+56.54, 31 FT. (RT) T.G. 803.06 INV. 799.60 S	FLAT TOP 30	CB, T-A, 4 FT. DIA. W/ T-24 F&G STA. 538+88.05, 31 FT. (RT) T.G. 797.98 INV. 794.00 W INV. 793.99 E INV. 794.17 S	43	CB, T-C, W/ T-24 F&G STA. 524+97.67, 31 FT. (LT) T.G. 808.03 INV. 804.58 N
6	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 521+62.29, 38.58 FT. (RT) T.G. 809.06 INV. 805.97 N INV. 804.50 W INV. 804.44 E	FLAT TOP 19	MH, T-A, 5 FT. DIA. W/ T-8 GRATE RESTRICTOR PLATE (R3S) STA. 530+56.54, 38.69 FT. (RT) T.G. 802.12 INV. 799.52 N INV. 797.87 W INV. 797.20 SE	31	CB, T-C, W/ T-8 GRATE STA. 538+88.05, 38.26 FT. (RT) T.G. 796.66 INV. 794.25 N	44	MH, T-A, 5 FT. DIA. W/ T-1F CL STA. 524+97.67, 37.17 FT. (LT) T.G. 807.42 INV. 804.55 S INV. 799.80 W INV. 799.30 E
7	CB, T-C, W/ T-24 F&G STA. 521+62.29, 31 FT. (RT) T.G. 809.70 INV. 806.04 S	20	CB, T-C, W/ T-24 F&G STA. 531+64.53, 31 FT. (RT) T.G. 802.20 INV. 799.58 S	FLAT TOP 32	CB, T-A, 4 FT. DIA., W/ T-24 F&G STA. 540+87.48, 31 FT. (RT) T.G. 795.76 INV. 793.40 W INV. 793.08 S	45	CB, T-C, W/ T-24 F&G STA. 527+58.37, 31 FT. (LT) T.G. 805.37 INV. 802.16 N
8	MH, T-A, 5 FT. DIA. W/ T-8 F&G STA. 522+79.29, 38.58 FT. (RT) T.G. 808.12 INV. 805.41 N INV. 804.29 W INV. 803.50 E	20 A	OVERFLOW STRUCTURE W/ GRATE STA. 531+26.11, 57.99 FT. (RT) T.G. INV. 796.89 NW INV. 796.89 NW INV. 796.89 NE INV. 796.89 SE (EXIST)	33	MH, T-A, 4 FT. DIA. W/ T-8 GRATE RESTRICTOR PLATE (R6) STA. 540+87.48, 41.83 FT. (RT) T.G. 795.36 INV. 793.07 N INV. 792.90 E	FLAT TOP 46	MH, T-A, 5 FT. DIA. W/ T-1F CL STA. 528+48.53, 43.00 FT. (LT) T.G. 804.20 INV. 802.10 S INV. 798.97 W INV. 798.83 E
9	CB, T-C, W/ T-24 F&G STA. 522+79.29, 31 FT. (RT) T.G. 808.94 INV. 805.48 S	FLAT TOP 21	MH, T-A, 4 FT. DIA. W/ T-8 GRATE STA. 531+64.53, 39.58 FT. (RT) T.G. 801.05 INV. 799.49 N INV. 796.97 SW INV. 796.97 E	34	15" DIA. PRCF END SECTION STA. 541+04.08, 42.17 FT. (RT) T.G. INV. 792.85 S	FLAT TOP 47	MH, T-A, 5 FT. DIA. W/ T-1F CL STA. 530+07.40, 38.26 FT. (LT) T.G. 802.38 INV. 797.25 NE INV. 797.25 SW INV. 798.95 S
10	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 524+02.50, 38.58 FT. (RT) T.G. 807.45 INV. 804.69 N INV. 803.34 W INV. 802.82 E	22	CB, T-C, W/ T-24 F&G STA. 532+96.40, 31 FT. (RT) T.G. 801.52 INV. 797.89 S	35	CB, T-C, W/ T-24 F&G STA. 515+85.67, 31.77 FT. (LT) T.G. 813.45 INV. 807.14 N	FLAT TOP 48	MH, T-A, 5 FT. DIA. W/ T-1F CL STA. 530+07.40, 39.83 FT. (LT) T.G. 801.96 INV. 799.53 S INV. 799.03 N
11	CB, T-C, W/ T-24 F&G STA. 524+02.51, 31 FT. (RT) T.G. 808.22 INV. 804.76 S	FLAT TOP 23	MH, T-A, 4 FT. DIA. W/ T-8 GRATE STA. 532+96.40, 39.58 FT. (RT) T.G. 800.41 INV. 797.88 N INV. 797.23 W	36	MH, T-A, 5 FT. DIA. W/ T-1 CL STA. 515+85.67, 36.87 FT. (LT) T.G. 813.29 INV. 807.08 S INV. 810.00 W (EXIST) INV. 805.04 E	FLAT TOP 49	CB, T-A, 4 FT. DIA. W/ T-8 GRATE STA. 530+07.40, 39.83 FT. (LT) T.G. 801.96 INV. 799.53 S INV. 799.03 N
FLAT TOP 12	MH, T-A, 5 FT. DIA. W/ T-8 GRATE STA. 524+97.81, 38.58 FT. (RT) T.G. 806.74 INV. 803.98 N INV. 802.70 W INV. 802.12 E	24	CB, T-C, W/ T-24 F&G STA. 534+15.30, 31 FT. (RT) T.G. 800.72 INV. 797.10 S	37	CB, T-C, W/ T-24 F&G STA. 518+17.14, 31 FT. (RT) T.G. 811.74 INV. 805.51 N	50	CB, T-C, W/ T-24 F&G STA. 530+07.40, 31 FT. (LT) T.G. 803.09 INV. 799.59 N
13	CB, T-C, W/ T-24 F&G STA. 524+31.48, 31 FT. (RT) T.G. 807.51 INV. 804.05 S	FLAT TOP 25	MH, T-A, 4 FT. DIA. W/ T-8 GRATE STA. 534+15.30, 39.08 FT. (RT) T.G. 799.77 INV. 797.03 N INV. 796.57 E	38	MH, T-A, 5 FT. DIA. W/ T-1 CL STA. 518+17.14, 37.22 FT. (LT) T.G. 811.64 INV. 805.47 S INV. 803.90 W INV. 803.40 E		


FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED STRUCTURES AND PIPES TABLE U.S. 20 (OAK AVE. TO PARK AVE.)				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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MODELNAME	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60N18				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		

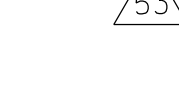
STRUCTURES (CONTINUATION)

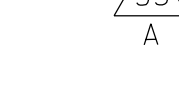
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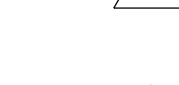
MH, T-A, 5 FT. DIA. W/ T-1F CL (EXIST)
 STA. 530+97.45, 61.62 FT. (LT)
 T.G. 802.66 EXIST
 INV. 797.14 SW
 INV. 798.62 NE (EXIST)
 INV. 798.62 E (EXIST)
 INV. 797.03 SE
- 


CB, T-A, 4 FT. DIA. W/ T-8 GRATE
 STA. 533+21.74, 44.45 FT. (LT)
 T.G. 800.69
 INV. 798.77 SE
- 


CB, T-A, 4 FT. DIA. W/ T-24 F&G (ADJ)
 STA. 533+28.35, 37.05 FT. (LT)
 T.G. 801.33
 INV. 798.29 NW
 INV. 797.79 SE
- 


MH, T-A, 5 FT. DIA. W/ T-1F CL
 STA. 533+33.43, 33.05 FT. (LT)
 T.G. 801.34
 INV. 797.76 NW
 INV. 795.92 E
- 


CB, T-A, 4 FT. DIA. W/ T-8 GRATE
 STA. 535+56.90, 46.01 FT. (LT)
 T.G. 799.33
 INV. 796.91 S
- 


CB, T-A, 4 FT. DIA. W/ T-24 F&G (ADJ)
 STA. 535+56.90, 38.50 FT. (LT)
 T.G. 801.12
 INV. 796.89 N
 INV. 796.38 SE
- 

MH, T-A, 5 FT. DIA. W/ T-1 CL
 STA. 535+63.58, 33.53 FT. (LT)
 T.G. 799.99
 INV. 796.34 NW
 INV. 795.63 W
 INV. 795.13 E
- 

CB, T-A, 4 FT. DIA. W/ T-8 GRATE
 STA. 537+66.10, 45.83 FT. (LT)
 T.G. 798.20
 INV. 796.37 SE
- 

MH, T-A, 5 FT. DIA. W/ T-1F CL
 STA. 537+71.42, 32.51 FT. (LT)
 T.G. 799.57
 INV. 794.86 W
 INV. 794.36 NE
 INV. 796.27 NE
 INV. 796.26 SE (EXIST)
- 

CB, T-C, 2 FT. DIA. W/ T-24 F&G
 STA. 540+06.77, 34.09 FT. (LT)
 T.G. 796.95
 INV. 793.74 NE
- 

MH, T-A, 5 FT. DIA. W/ T-1 CL
 STA. 540+15.00, 43.74 FT. (LT)
 T.G. 799.96
 INV. 793.47 SE
 INV. 793.72 SW
 INV. 793.64 SW
- 

MH, T-A, 5 FT. DIA. W/ T-1F CL
 RESTRICTOR PLATE (R5)
 STA. 542+15.80, 32.97 FT. (LT)
 T.G. 797.28
 INV. 793.27 NW
 INV. 793.06 NE (EXIST)

PIPES

- ① 24" DIA., S.S. CLASS A, T-1, 164.26 FT.
TBF =
- ② 12" DIA., S.S. CLASS A T-1, 3.60 FT.
TBF = 1.29 CU YD
- ③ 24" DIA. S.S. CLASS A, T-1, 285.16 FT.
TBF = 14.75 CU YD
- ④ 12" DIA. S.S. CLASS A, T-1, 4.90 FT.
TBF = 0.65 CU YD
- ⑤ 24" DIA. S.S. CLASS A, T-1, 132.75 FT.
TBF = 12.06 CU YD
- ⑥ 12" DIA. S.S. CLASS A, T-1, 4.76 FT.
TBF = 0.63 CU YD
- ⑦ 30" DIA. S.S. CLASS A, T-1, 113.90 FT.
TBF = 13.99 CU YD
- ⑧ 12" DIA. S.S. CLASS A, T-1, 4.92 FT.
TBF = 0.65 CU YD
- ⑨ 30" DIA. S.S. CLASS A, T-1, 119.21 FT.
TBF = 13.29 CU YD
- ⑩ 12" DIA. S.S. CLASS A, T-1, 4.67 FT.
TBF = 0.62 CU YD
- ⑪ 30" DIA. S.S. CLASS A, T-1, 91.46 FT.
TBF = 15.20 CU YD
- ⑫ 12" DIA. S.S. CLASS A, T-1, 4.64 FT.
TBF = 1.49 CU YD
- ⑬ 30" DIA. S.S. CLASS A, T-2, 116.91 FT.
TBF = 15.14 CU YD
- ⑭ 12" DIA. S.S. CLASS A, T-1, 4.22 FT.
TBF = 0.64 CU YD
- ⑮ 30" DIA. S.S. CLASS A, T-2, 291.62 FT.
TBF = 26.43 CU YD
- ⑯ 12" DIA. S.S. CLASS A, T-1, 4.30 FT.
TBF = 0.57 CU YD
- ⑰ 30" DIA. S.S. CLASS A, T-2, 138.39 FT.
TBF = 13.53 CU YD
- ⑱ 12" DIA. S.S. CLASS A, T-1, 5.05 FT.
TBF = 0.67 CU YD
- ⑲ 24" DIA. S.S. CLASS A, T-2, 67.94 FT.
TBF = 13.53 CU YD
- A ⑲ 30" DIA. S.S. CLASS A, T-1, 121.23 FT.
TBF = 13.17 CU YD
- ⑳ 12" DIA. S.S. CLASS A, T-1, 39.35 FT.
TBF =
- ㉑ 12" DIA. S.S. CLASS A, T-1, 5.79 FT.
TBF = 0.58 CU YD
- ㉒ 12" DIA. S.S. CLASS A, T-1, 127.60 FT.
TBF = 10.96 CU YD
- ㉓ 12" DIA. S.S. CLASS A, T-1, 4.37 FT.
TBF = 0.85 CU YD
- ㉔ 12" DIA. S.S. CLASS A, T-1, 4.48 FT.
TBF = 0.68 CU YD
- ㉕ 12" DIA. S.S. CLASS A, T-1, 116.21 FT.
TBF = 7.22 CU YD
- ㉖ 12" DIA. S.S. CLASS A, T-1, 5.02 FT.
TBF = 0.88 CU YD
- ㉗ 15" DIA. S.S. CLASS A, T-1, 228.21 FT.
TBF = 14.85 CU YD
- ㉘ 12" DIA. S.S. CLASS A, T-1, 6.19 FT.
TBF = 0.53 CU YD
- ㉙ 18" DIA. S.S. CLASS A, T-1, 117.05 FT.
TBF = 12.29 CU YD
- ㉚ 12" DIA. S.S. CLASS A, T-1, 5.08 FT.
TBF = 0.46 CU YD
- ㉛ 18" DIA. S.S. CLASS A, T-1, 195.85 FT.
TBF = 20.56 CU YD
- ㉜ 18" DIA. S.S. CLASS A, T-1, 2.80 FT.
TBF = 0.29 CU YD
- ㉝ 15" DIA. S.S. CLASS A, T-1, 5.78 FT.
TBF =
- ㉞ 12" DIA. S.S. CLASS A, T-2, 3.83 FT.
TBF = 2.42 CU YD
- ㉟ 24" DIA. S.S. CLASS A, T-2, 226.53 FT.
TBF =
- ㊱ 12" DIA. S.S. CLASS A, T-2, 3.05 FT.
TBF = 2.02 CU YD
- ㊲ 24" DIA. S.S. CLASS A, T-2, 193.92 FT.
TBF =
- ㊳ 12" DIA. S.S. CLASS A, T-2, 2.80 FT.
TBF = 1.95 CU YD
- ㊴ 24" DIA. S.S. CLASS A, T-2, 211.78 FT.
TBF = 107.86 FT.
- ㊵ 12" DIA. S.S. CLASS A, T-2, 3.75 FT.
TBF = 2.49 CU YD
- ㊶ 30" DIA. S.S. CLASS A, T-2, 262.02 FT.
TBF =
- ㊷ 12" DIA. S.S. CLASS A, T-1, 4.86 FT.
TBF = 0.64 CU YD
- ㊸ 30" DIA. S.S. CLASS A, T-2, 256.79 FT.
TBF =
- ㊹ 12" DIA. S.S. CLASS A, T-1, 7.49 FT.
TBF = 0.99 CU YD
- ㊺ 30" DIA. S.S. CLASS A, T-1, 86.31 FT.
TBF = 32.28 CU YD
- ㊻ 36" DIA. S.S. CLASS A, T-1, 155.70 FT.
TBF =
- ㊼ 12" DIA. S.S. CLASS A, T-1, 5.18 FT.
TBF = 0.68 CU YD
- ㊽ 12" DIA. S.S. CLASS A, T-1, 9.63 FT.
TBF = 2.07 CU YD
- ㊾ 30" DIA. S.S. CLASS A, T-1, 86.63 FT.
TBF = 30.18 CU YD
- ㊿ 12" DIA. S.S. CLASS A, T-1, 5.13 FT.
TBF = 0.68 CU YD
- 1 12" DIA. S.S. CLASS A, T-1, 2.51 FT.
TBF = 0.33 CU YD
- 2 30" DIA. S.S. CLASS A, T-1, 226.37 FT.
TBF = 28.07 CU YD
- 3 12" DIA. S.S. CLASS A, T-1, 2.52 FT.
TBF = 0.38 CU YD
- 4 12" DIA. S.S. CLASS A, T-1, 4.38 FT.
TBF = 0.67 CU YD
- 5 30" DIA. S.S. CLASS A, T-1, 203.82 FT.
TBF = 25.27 CU YD
- 6 12" DIA. S.S. CLASS A, T-1, 8.52 FT.
TBF = 1.12 CU YD
- 7 30" DIA. S.S. CLASS A, T-1, 241.12 FT.
TBF = 97.58 CU YD
- 8 12" DIA. S.S. CLASS A, T-1, 10.12 FT.
TBF = 2.81 CU YD
- 9 38" x 24" DIA. S.S. (ELLIPTICAL) CLASS A,
T-1, 198.91 FT.
TBF =

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED STRUCTURES AND PIPES TABLE				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pwork\pwork\galbenjr\d0304020\PI57408-shr-cover.dgn		DRAWN -	REVISED -		U.S. 20 (OAK AVE. TO PARK AVE.)				345	7Y-WRS	COOK	97	15
#MODELNAME#	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 60N18				
	PLOT DATE = 4/3/2013	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								

EXIST. CURVE E_US20-1
 PI STA. = 497+43.85
 $\Delta = 10^\circ 42' 41''$ (RT)
 $D = 0^\circ 56' 04''$
 $R = 6,130.75'$
 $T = 574.75'$
 $L = 1,146.15'$
 $E = 26.88'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 491+69.10$
 $P.T. STA. = 503+15.25$

PROP. CURVE US01
 PI STA. = 497+43.74
 $\Delta = 10^\circ 42' 41''$ (RT)
 $D = 0^\circ 56' 04''$
 $R = 6,130.75'$
 $T = 574.75'$
 $L = 1,146.15'$
 $E = 26.88'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 491+68.99$
 $P.T. STA. = 503+15.14$

PROP. CURVE US02
 PI STA. = 508+97.25
 $\Delta = 2^\circ 43' 55''$ (LT)
 $D = 0^\circ 32' 08''$
 $R = 10,698.11'$
 $T = 255.10'$
 $L = 510.10'$
 $E = 3.04'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 506+42.15$
 $P.T. STA. = 511+52.25$

PROP. CURVE US03
 PI STA. = 514+42.84
 $\Delta = 2^\circ 13' 11''$ (LT)
 $D = 0^\circ 22' 55''$
 $R = 15,000.00'$
 $T = 290.58'$
 $L = 581.10'$
 $E = 2.81'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 511+52.26$
 $P.T. STA. = 517+33.35$

PROP. CURVE US05
 PI STA. = 542+49.82
 $\Delta = 5^\circ 02' 48''$ (RT)
 $D = 2^\circ 09' 29''$
 $R = 2,655.00'$
 $T = 117.00'$
 $L = 233.85'$
 $E = 2.58'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 541+32.82$
 $P.T. STA. = 543+66.67$

EXIST. CURVE E_ROMA-1
 PI STA. = 101+87.03
 $\Delta = 29^\circ 09' 31''$ (RT)
 $D = 17^\circ 39' 02''$
 $R = 324.61'$
 $T = 84.43'$
 $L = 165.20'$
 $E = 10.80'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 101+02.60$
 $P.T. STA. = 102+67.80$

EXIST. CURVE E_ROMA-2
 PI STA. = 103+89.02
 $\Delta = 47^\circ 12' 09''$ (LT)
 $D = 30^\circ 46' 40''$
 $R = 186.16'$
 $T = 81.34'$
 $L = 153.37'$
 $E = 16.99'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 103+07.69$
 $P.T. STA. = 104+61.05$

EXIST. CURVE E_BART-1
 PI STA. = 205+36.57
 $\Delta = 18^\circ 15' 57''$ (RT)
 $D = 9^\circ 01' 35''$
 $R = 634.75'$
 $T = 102.04'$
 $L = 202.36'$
 $E = 8.15'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 204+34.52$
 $P.T. STA. = 206+36.88$

EXIST. CURVE E_US20-2
 PI STA. = 509+50.45
 $\Delta = 3^\circ 18' 01''$ (LT)
 $D = 0^\circ 32' 08''$
 $R = 10,698.11'$
 $T = 308.19'$
 $L = 616.21'$
 $E = 4.44'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 506+42.26$
 $P.T. STA. = 512+58.47$

EXIST. CURVE OAK
 PI STA. = 295+58.65
 $\Delta = 25^\circ 37' 37''$ (LT)
 $D = 3^\circ 59' 42''$
 $R = 1,434.16'$
 $T = 326.19'$
 $L = 641.46'$
 $E = 36.63'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 292+32.46$
 $P.T. STA. = 298+73.92$

PROP. CURVE OAK01
 PI STA. = 295+56.36
 $\Delta = 25^\circ 37' 37''$ (LT)
 $D = 3^\circ 59' 42''$
 $R = 1,434.16'$
 $T = 326.19'$
 $L = 641.46'$
 $E = 36.63'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 292+30.17$
 $P.T. STA. = 298+71.63$

EQUATION:
 Sta 514+02.31 BK =
 Sta 100+42.21 AH

EXIST. US 20	PT# A10	N 1944744.7800	E 1022249.3614	STA. 487+02.3	
	PC CURVE 1	N 1944653.2288	E 1022707.0931	STA. 491+69.1	
	PI CURVE 1	N 1944540.5050	E 1023270.6817	STA. 497+43.85	
	PT CURVE 1	N 1944324.9944	E 1023803.4987	STA. 503+15.25	
	PC CURVE 2	N 1944202.3778	E 1024106.6495	STA. 506+42.26	
	PI CURVE 2	N 1944086.8184	E 1024392.3524	STA. 509+50.45	
	PT CURVE 2	N 1943987.8979	E 1024684.2340	STA. 512+58.47	
	EQUATION: STA. 514+02.3 (BK) = STA. 100+42.21 (AH)				
	PT# A11	N 1943941.2417	E 1024820.3016	STA. 100+42.21	
	PT# ADD1005	N 1943916.2665	E 1024891.3719	STA. 101+17.54	
PT# ADD1007	N 1943632.5208	E 1025815.3956	STA. 110+84.15		
PT# 1024	N 1943068.8816	E 1027667.8591	STA. 130+20.46		
PT# US001	N 1944744.7594	E 1022249.4646	STA. 487+02.30		
PROP. US 20	PC CURVE 1	N 1944653.2288	E 1022707.0931	STA. 491+68.99	
	PI CURVE 1	N 1944540.5050	E 1023270.6817	STA. 497+43.74	
	PT CURVE 1	N 1944324.9944	E 1023803.4987	STA. 503+15.14	
	PC CURVE 2	N 1944202.3778	E 1024106.6495	STA. 506+42.15	
	PI CURVE 2	N 1944106.7252	E 1024343.1360	STA. 508+97.25	
	PT CURVE 2	N 1944022.4530	E 1024583.9128	STA. 511+52.25	
	PC CURVE 3	N 1944022.4522	E 1024583.9149	STA. 511+52.26	
	PI CURVE 3	N 1943926.4571	E 1024858.1858	STA. 514+42.84	
	PT CURVE 3	N 1943841.1566	E 1025135.9687	STA. 517+33.35	
	PT# US010	N 1943632.5208	E 1025815.3956	STA. 524+44.09	
EXIST. OAK AVE.	PC CURVE 4	N 1943140.9525	E 1027430.9901	STA. 541+32.82	
	PI CURVE 4	N 1943106.8946	E 1027542.9250	STA. 542+49.82	
	PT CURVE 4	N 1943063.1224	E 1027651.4300	STA. 543+66.67	
	PT# 1020	N 1942931.5572	E 1024622.1055	STA. 289+64.48	
PROP. OAK AVE.	PC CURVE 1	N 1943171.7143	E 1024741.0093	STA. 292+32.46	
	PI CURVE 1	N 1943464.0345	E 1024885.7395	STA. 295+58.65	
	PT CURVE 1	N 1943790.1962	E 1024889.8017	STA. 298+73.92	
	PT# OAK101	N 1942931.5572	E 1024622.1055	STA. 289+62.19	
EXIST. BARTLETT RD.	PC CURVE 1	N 1943171.7142	E 1024741.0093	STA. 292+30.17	
	PI CURVE 1	N 1943464.0345	E 1024885.7395	STA. 295+56.36	
	PT CURVE 1	N 1943790.1963	E 1024889.8017	STA. 298+71.63	
	PT# OAK105	N 1943918.5523	E 1024891.4004	STA. 300+00.00	
EXIST. PARK BLVD.	PT# A20	N 1944244.5014	E 1024002.5246	STA. 200+00.00	
	PC CURVE 1	N 1944668.7355	E 1024096.5152	STA. 204+34.52	
	PI CURVE 1	N 1944768.3643	E 1024118.5883	STA. 205+36.57	
	PT CURVE 1	N 1944856.0548	E 1024170.7754	STA. 206+36.88	
EXIST. ROMA JEAN PKWY.	PT# A21	N 1945099.1025	E 1024315.4201	STA. 209+19.71	
	PT# ADD1010	N 1943226.0695	E 1027151.2433	STA. 500+00.00	
	PT# 1023	N 1943792.2396	E 1027324.4579	STA. 505+92.07	
	PT# A55	N 1943486.2883	E 1026418.6131	STA. 100+40.00	
EXIST. ROMA JEAN PKWY.	PC CURVE 1	N 1943545.8336	E 1026437.9400	STA. 101+02.60	
	PI CURVE 1	N 1943626.1390	E 1026464.0051	STA. 101+87.03	
	PT CURVE 1	N 1943683.5679	E 1026525.8943	STA. 102+67.80	
	PC CURVE 2	N 1943710.6974	E 1026555.1308	STA. 103+07.69	
	PI CURVE 2	N 1943766.0220	E 1026614.7522	STA. 103+89.02	
	PT CURVE 2	N 1943847.3578	E 1026614.6647	STA. 104+61.05	
PT# A56	N 1943947.6189	E 1026614.5569	STA. 105+61.31		

CP# E5	N 1944639.8601	E 1022530.3899
CP# E6	N 1943588.4991	E 1026428.2962
CP# E7	N 1943721.0111	E 1026525.1731
CP# 50	N 1944337.5809	E 1023993.4069
CP# 56	N 1943614.4160	E 1025766.8209
CP# 57	N 1943472.9969	E 1026485.5454
CP# 101	N 1943838.6019	E 1024940.4481
CP# 102	N 1943790.0355	E 1025464.7194
CP# 103	N 1943604.7693	E 1025816.3759
CP# 104	N 1943436.2696	E 1026633.7923
CP# 105	N 1943172.0982	E 1027136.3825
CP# 106	N 1943342.1693	E 1024852.7216
CP# 108	N 1943588.4482	E 1027309.5476

BENCHMARKS

TBM "A" - "+" CUT IN T.S. CONC BASE ON SW CORNER OF US 20/PARK AVE.. ELEV. 798.55
 TBM "B" - "□" CUT IN NE CORNER CONC BASE OF T.S. IN SE CORNER OAK ST. & US 20. ELEV. 815.27
 * BM 1 - "□" IN SOUTHERLY CORNER TRAFFIC CONTROL BOX IN NW CORNER US 20 & BARTLETT RD. ELEV. 827.59
 * FROM EXTENDED SURVEY

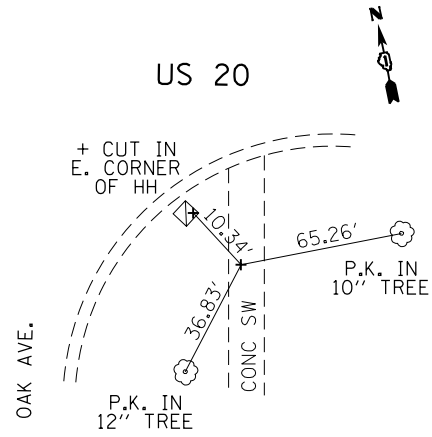
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Default	PLOT SCALE = 400.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 4/3/2013	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

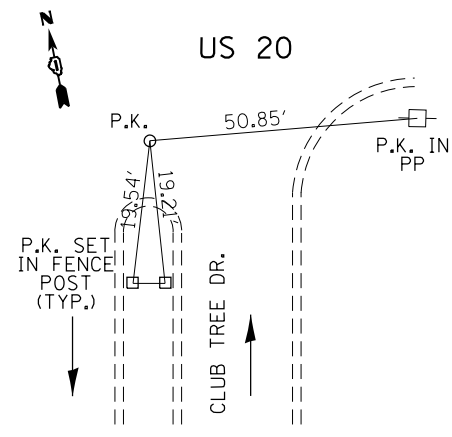
**ALIGNMENT AND BENCHMARKS
 U.S. ROUTE 20 (LAKE ST.) OAK AVE. TO PARK AVE.**

SCALE: 1"=100' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	16
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

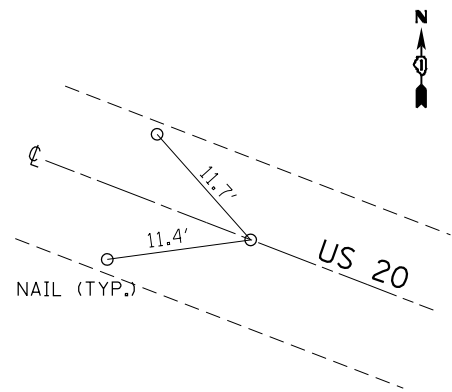


CP #1

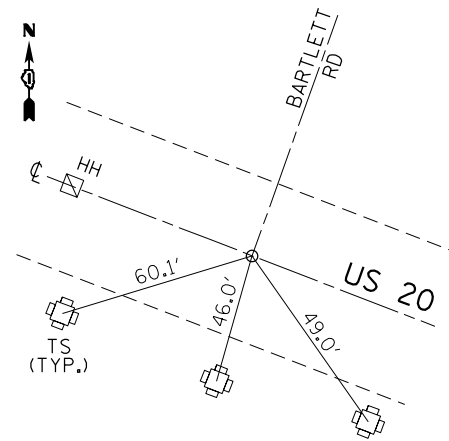


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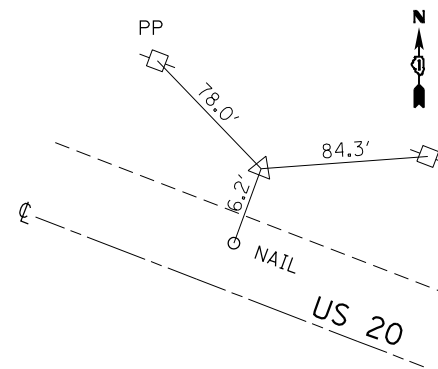
TIES FROM EXTENDED SURVEY



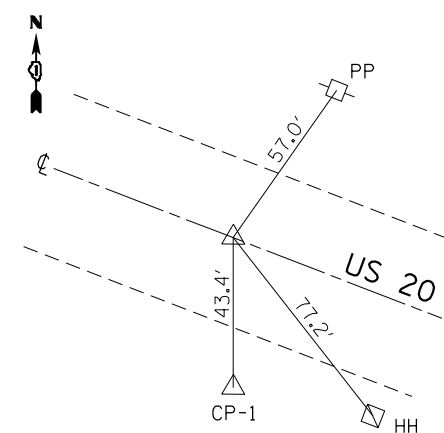
PT #115
STA. 481+69.18



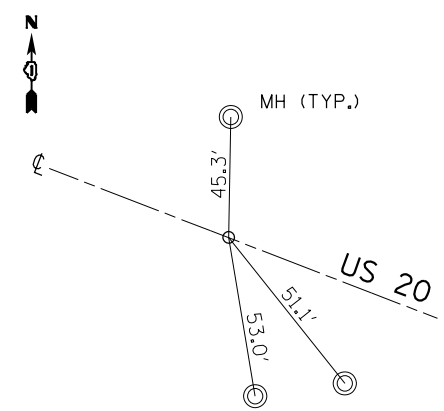
PT #124
C-C



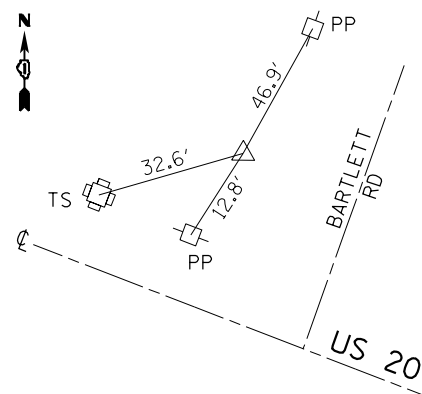
PT #116
STA. 497+43.85



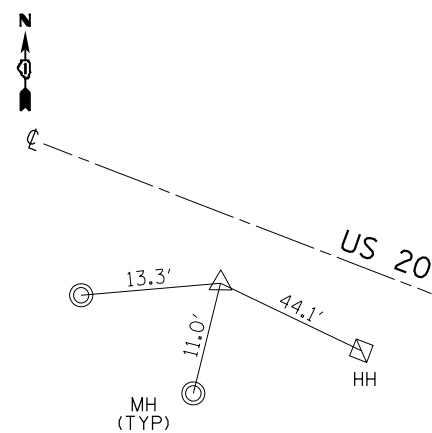
PT #125
PI
STA. 504+17.33



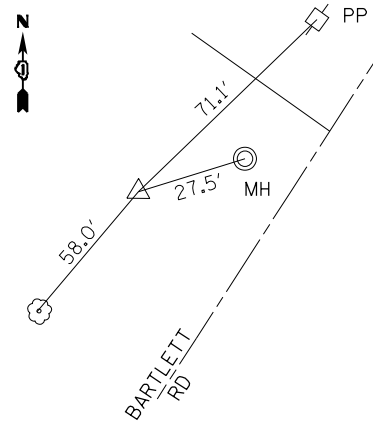
PT #110
END POINT
STA. 508+69.24



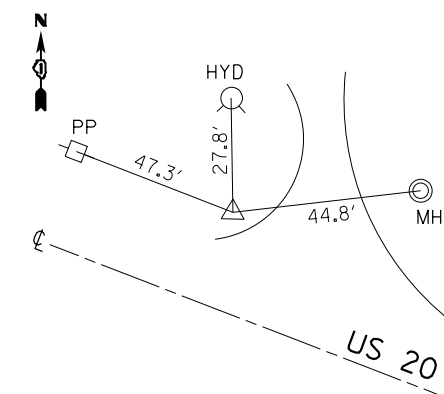
CP-2



CP-1



CP-3



CP-4

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -
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Default	PLOT SCALE = 400.0000' / in.	CHECKED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

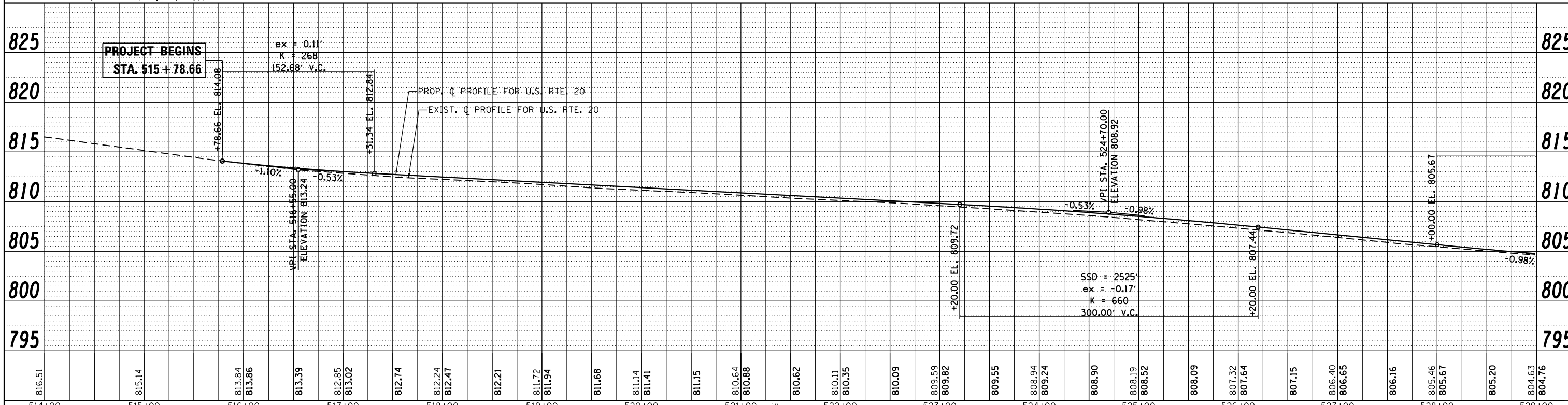
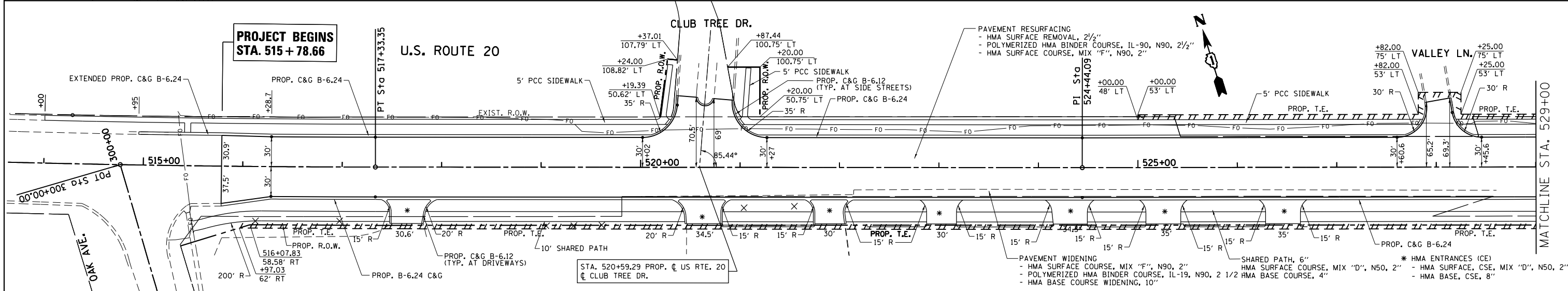
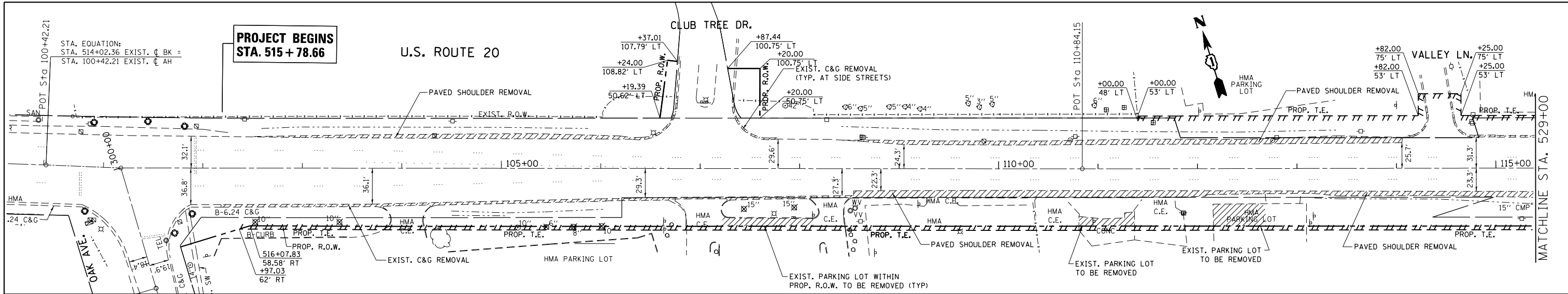
**EXISTING ALIGNMENT TIES
U.S. ROUTE 20 (LAKE ST.) OAK AVE. TO PARK AVE.**

SCALE: 1"=100' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	17
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	GRADES CHECKED		
	STRUCTURE		
	NOTATIS		
	CPWD		

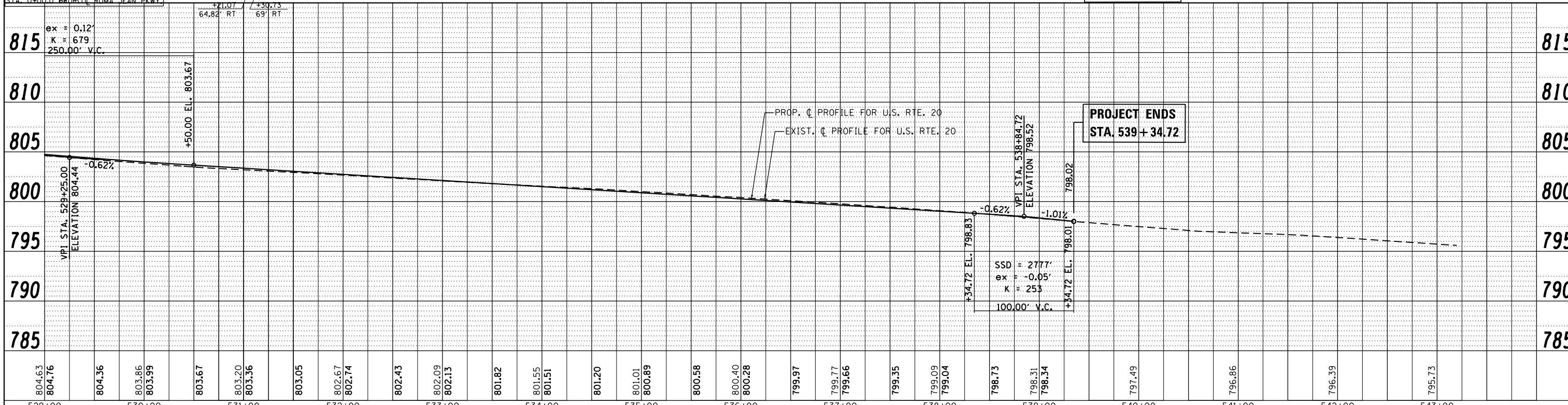
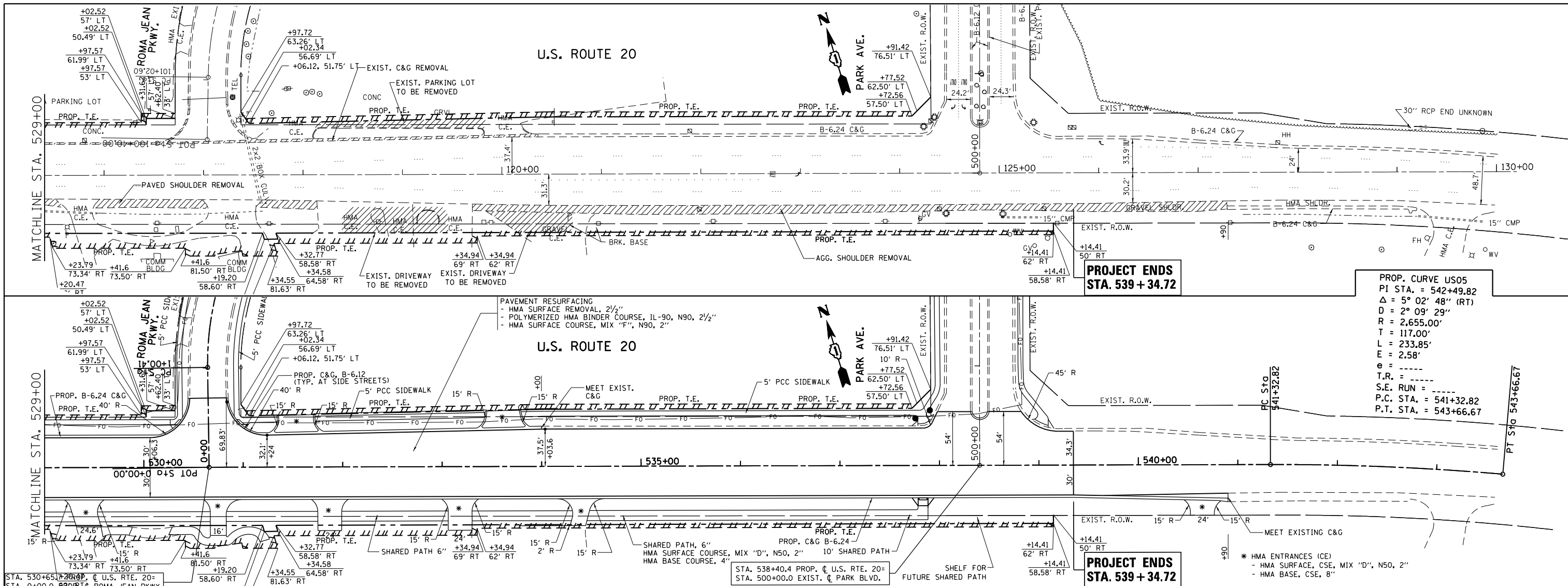
PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	GRADES CHECKED		
	STRUCTURE		
	NOTATIS		
	CPWD		



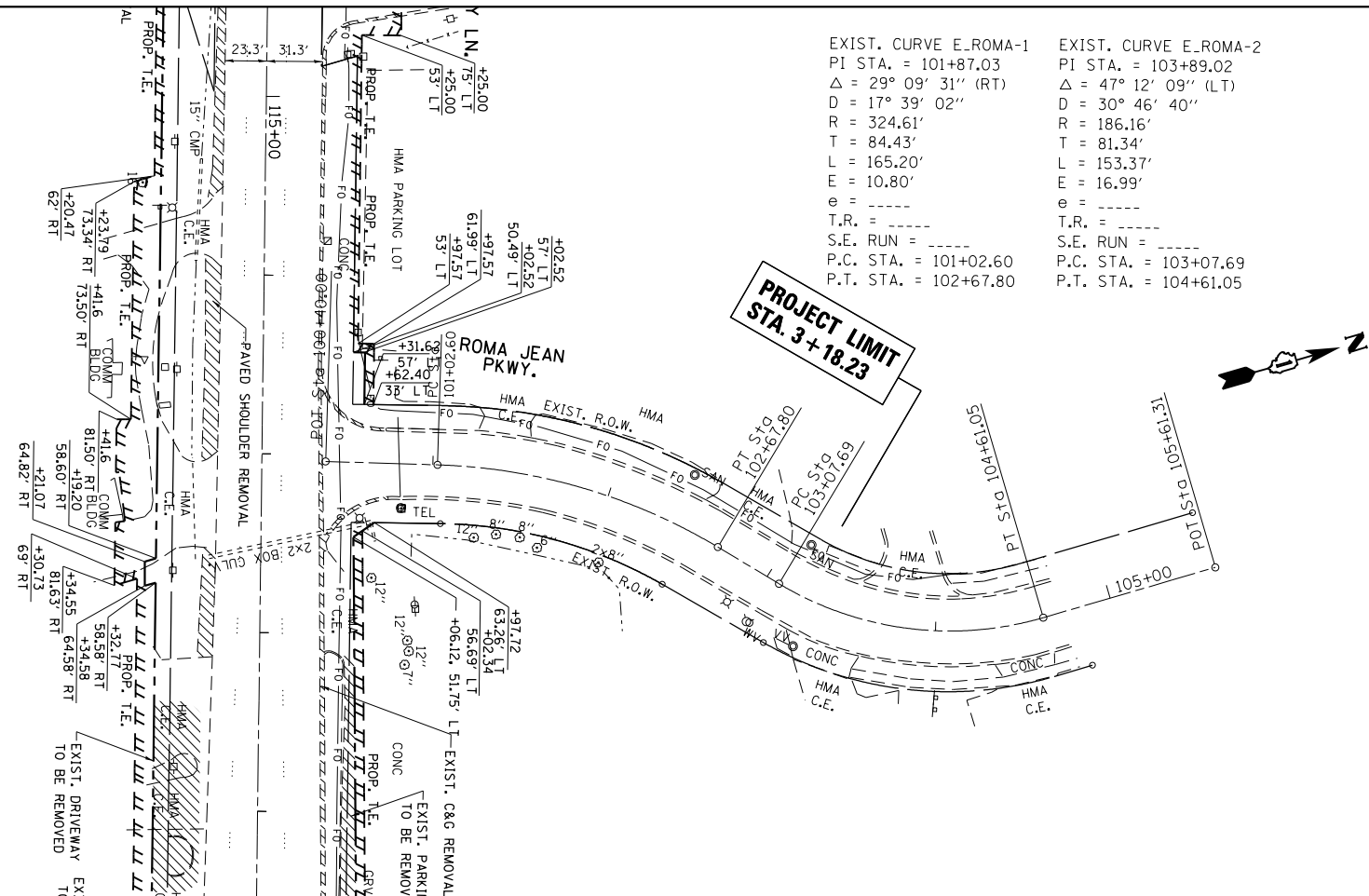
816.51	815.14	813.84	813.86	813.39	812.85	813.02	812.74	812.24	812.47	812.21	811.72	811.94	811.68	811.14	811.41	811.15	810.64	810.88	810.62	810.11	810.35	810.09	809.59	809.82	809.55	808.94	809.24	808.90	808.19	808.52	808.09	807.32	807.64	807.15	806.40	806.65	806.16	805.46	805.67	805.20	804.63	804.76
514+00	515+00	516+00	517+00	518+00	519+00	520+00	521+00	522+00	523+00	524+00	525+00	526+00	527+00	528+00	529+00																											

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	FILED		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES		
	CHECKED		
	STRUCTURE		
	NOTATIONS		
	CPAD		
	NO.		

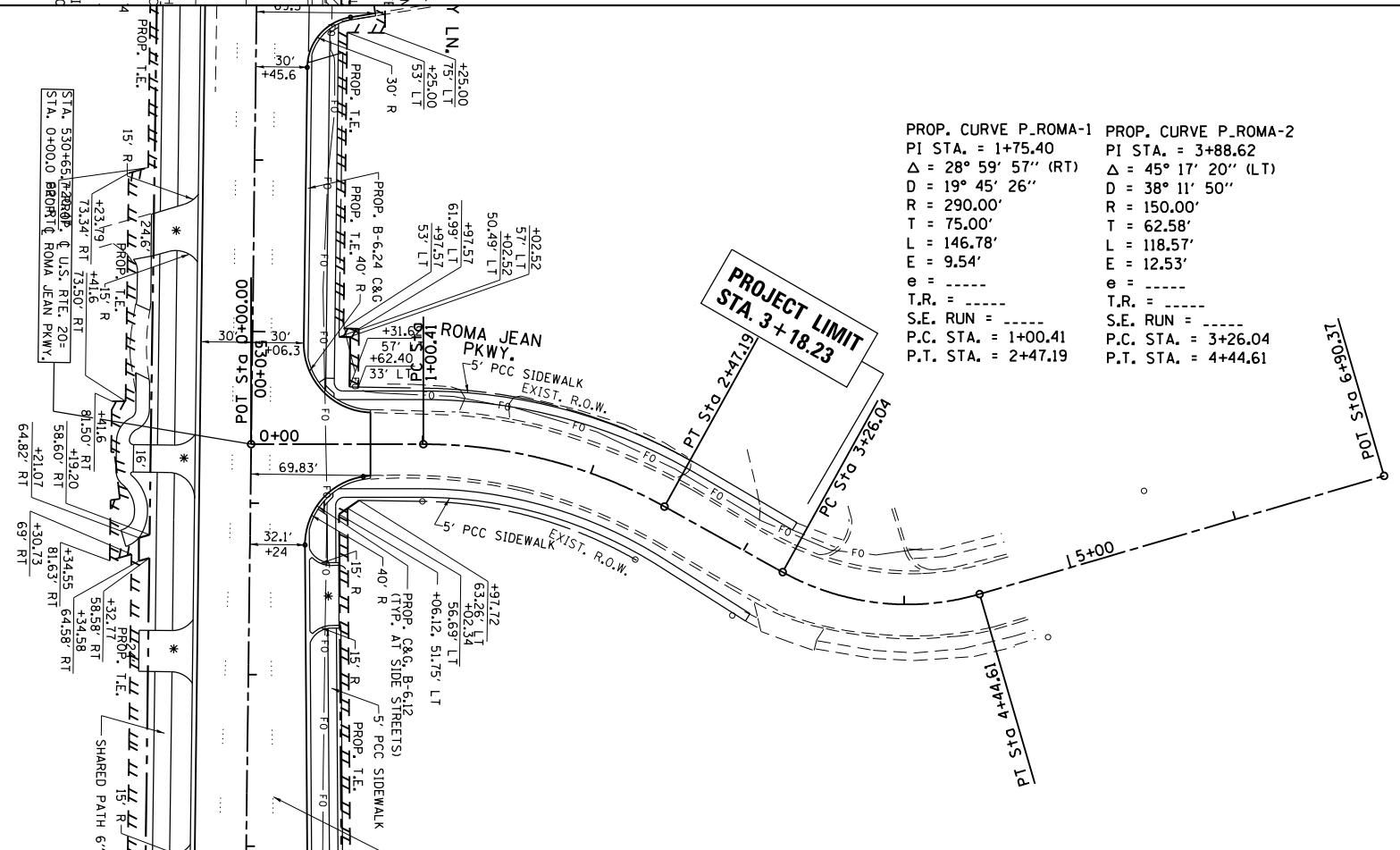


804.63	804.76	804.36	803.86	803.99	803.67	803.20	803.36	803.05	802.67	802.74	802.43	802.09	802.13	801.82	801.55	801.51	801.20	801.01	800.89	800.58	800.40	800.28	799.97	799.77	799.66	799.35	799.09	799.04	798.73	798.31	798.34	797.49	796.86	796.39	795.73		
529+00	530+00	531+00	532+00	533+00	534+00	535+00	536+00	537+00	538+00	539+00	540+00	541+00	542+00	543+00																							



EXIST. CURVE E_ROMA-1
 PI STA. = 101+87.03
 $\Delta = 29^\circ 09' 31''$ (RT)
 D = 17° 39' 02"
 R = 324.61'
 T = 84.43'
 L = 165.20'
 E = 10.80'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 101+02.60
 P.T. STA. = 102+67.80

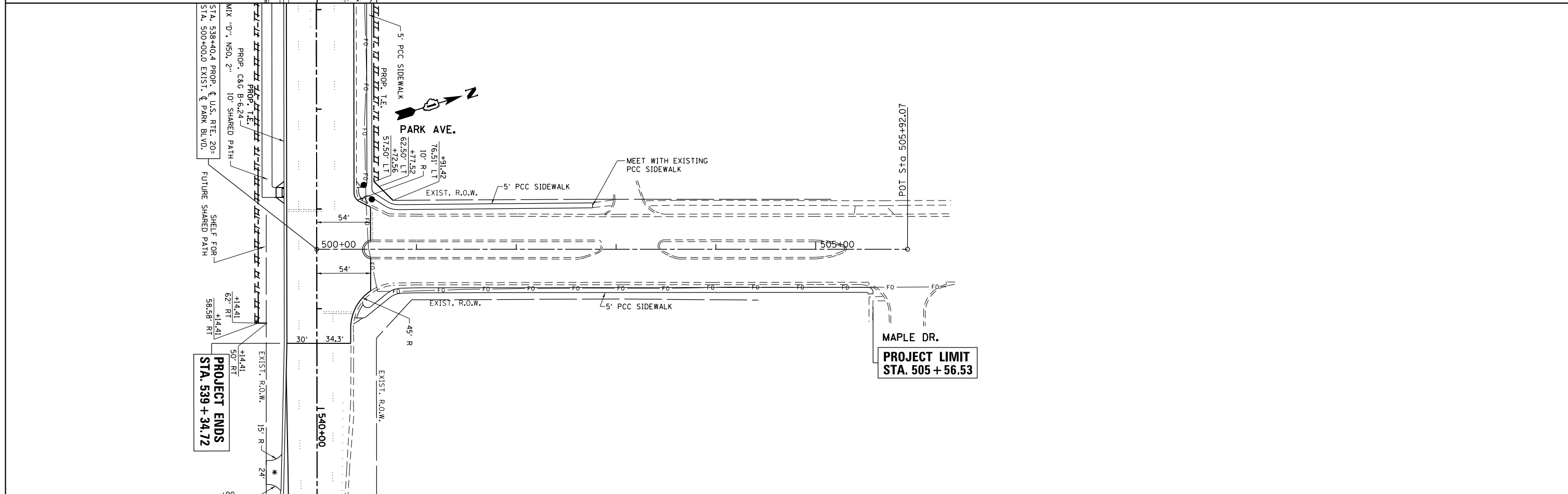
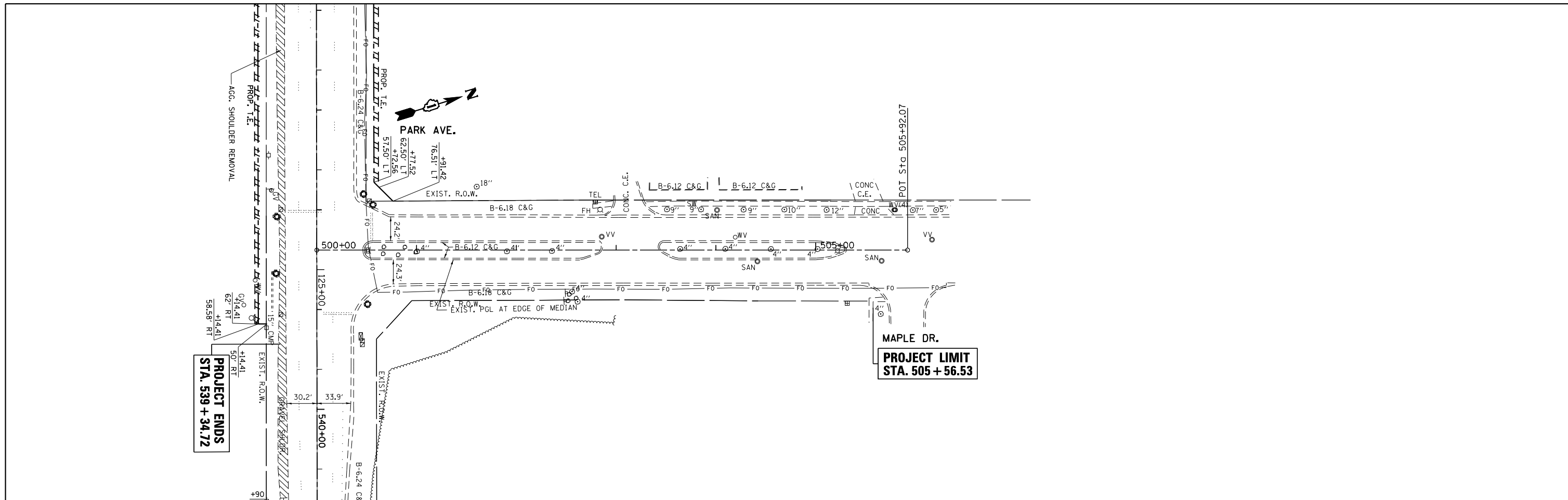
EXIST. CURVE E_ROMA-2
 PI STA. = 103+89.02
 $\Delta = 47^\circ 12' 09''$ (LT)
 D = 30° 46' 40"
 R = 186.16'
 T = 81.34'
 L = 153.37'
 E = 16.99'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 103+07.69
 P.T. STA. = 104+61.05



PROP. CURVE P_ROMA-1
 PI STA. = 1+75.40
 $\Delta = 28^\circ 59' 57''$ (RT)
 D = 19° 45' 26"
 R = 290.00'
 T = 75.00'
 L = 146.78'
 E = 9.54'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 1+00.41
 P.T. STA. = 2+47.19

PROP. CURVE P_ROMA-2
 PI STA. = 3+88.62
 $\Delta = 45^\circ 17' 20''$ (LT)
 D = 38° 11' 50"
 R = 150.00'
 T = 62.58'
 L = 118.57'
 E = 12.53'
 e = ----
 T.R. = ----
 S.E. RUN = ----
 P.C. STA. = 3+26.04
 P.T. STA. = 4+44.61

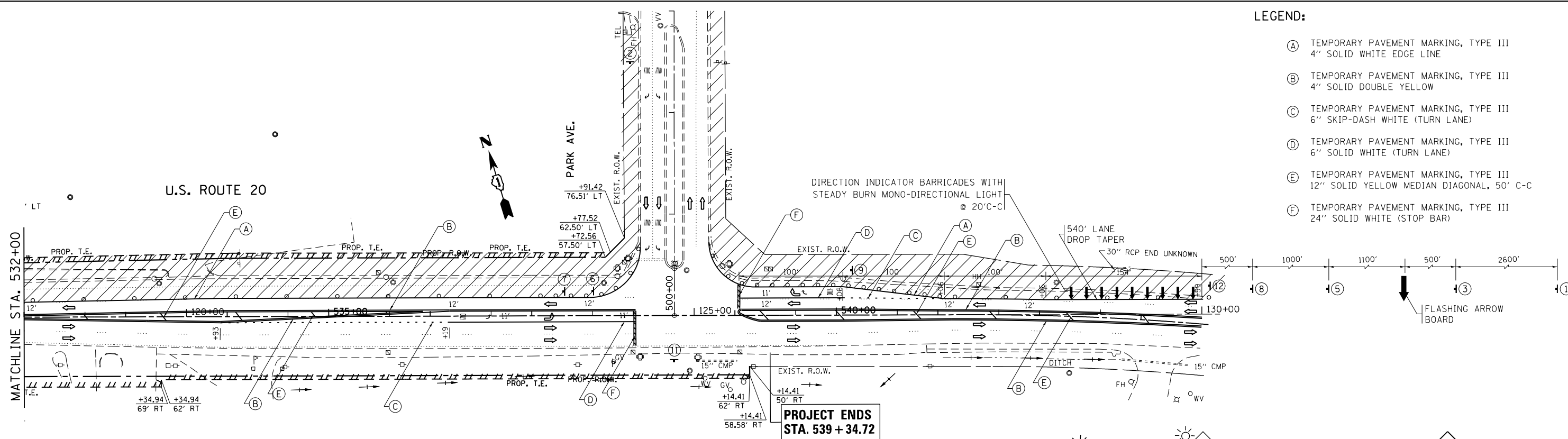
FILE NAME =	USER NAME = galbenjr	DESIGNED =	REVISED =	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. ROUTE 20 (OAK AVE. - PARK AVE.)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ei:\pw_work\p1d01\galbenjr\157408-sht-plnpr.f.dgn		DRAWN =	REVISED =		345	7Y-WRS	COOK	97	20			
#MODELNAME#	PLOT SCALE = 100.0000' / in.	CHECKED =	REVISED =		CONTRACT NO. 60N18							
	PLOT DATE = 4/3/2013	DATE =	REVISED =		SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT



FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. ROUTE 20 (OAK AVE. - PARK AVE.)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw\work\p\idot\galbenjr\d0304019\P157408-sht-plnpr.f.dgn		DRAWN -	REVISED -		345	7Y-WRS	COOK	97	21	CONTRACT NO. 60N18			
MODELNAME		CHECKED -	REVISED -		SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT	
	PLOT DATE = 4/3/2013	DATE -	REVISED -										

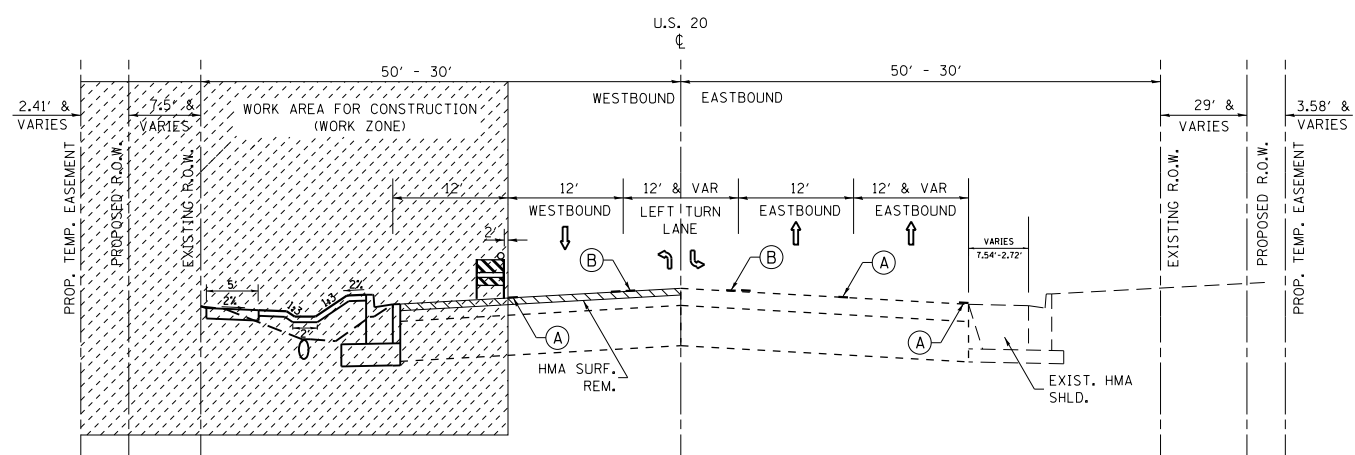
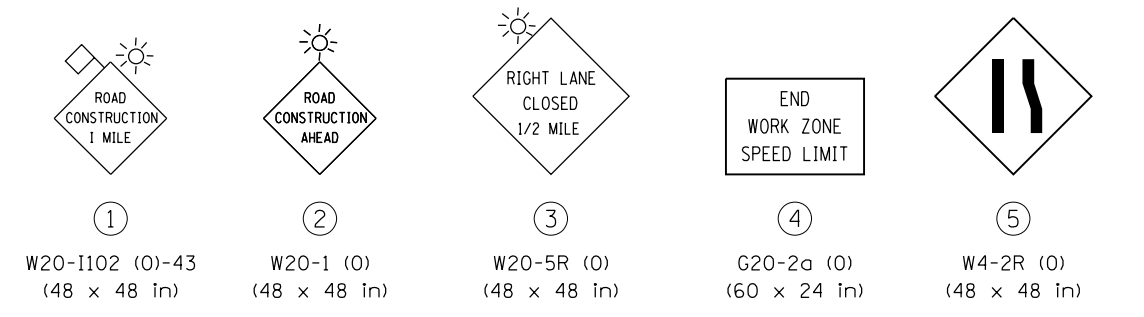
LEGEND:

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
- (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
- (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SKIP-DASH WHITE (TURN LANE)
- (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
- (E) TEMPORARY PAVEMENT MARKING, TYPE III
12" SOLID YELLOW MEDIAN DIAGONAL, 50' C-C
- (F) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)



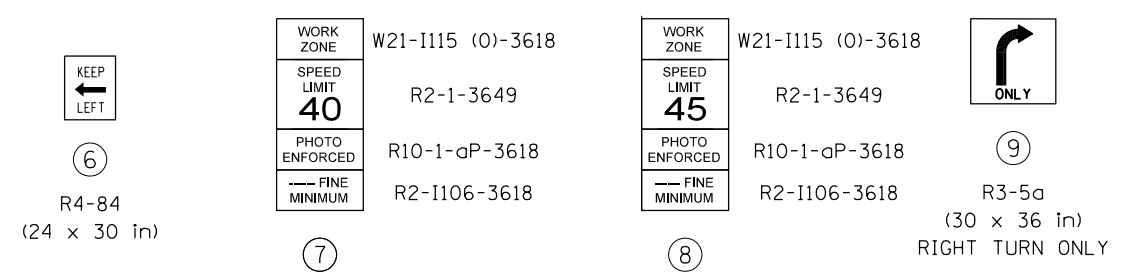
FOR WIDENING AND RESURFACING OPERATIONS THE SIGNING, BARRICADING AND LANE CLOSURE SHALL BE IN ACCORDANCE WITH STDS. 701602 AND 701606 IN ADDITION TO THE SIGNS SHOWN.

**PROJECT ENDS
STA. 539 + 34.72**



**U.S. 20 (LAKE STREET)
STAGE I CONSTRUCTION
STA. 515+78.66 TO STA. 539+34.72**

WESTBOUND: (WIDENING SECTION)
2" HMA SURFACE COURSE, MIX "F", N90
2-1/2" POLY. HMA BINDER COURSE, IL-19, N90
8" HMA BASE COURSE
12" AGG. SUBGRADE IMPROVEMENT



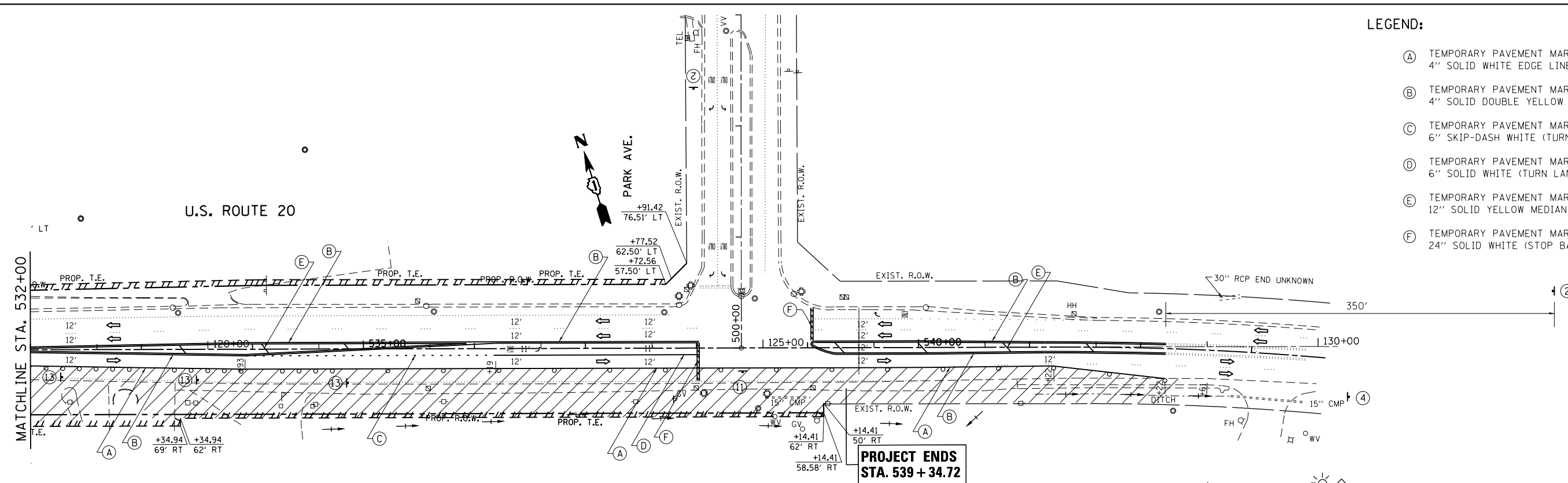
- (10) R3-5b (30 x 36 in) LEFT TURN ONLY
 - (11) W1-7(0) (48 x 24 in)
 - (12) W21-1a(0)-48
* MUST BE REMOVED WHEN WORKERS ARE NOT PRESENT
 - (13) BLACK ON WHITE REFLECTIVE BACKGROUND 1/2" BORDER
- BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT @ 50'C-C (TYP), 25'C-C IN CURVES AND RADII, AND 20'C-C IN TAPERS.
- TYPE III BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
- CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
- TRAFFIC LANE
- ARROW BOARD
- SHOULDER REMOVAL
- HMA SURF. REMOVAL
- WORK ZONE

TRAFFIC CONTROL SIGNS

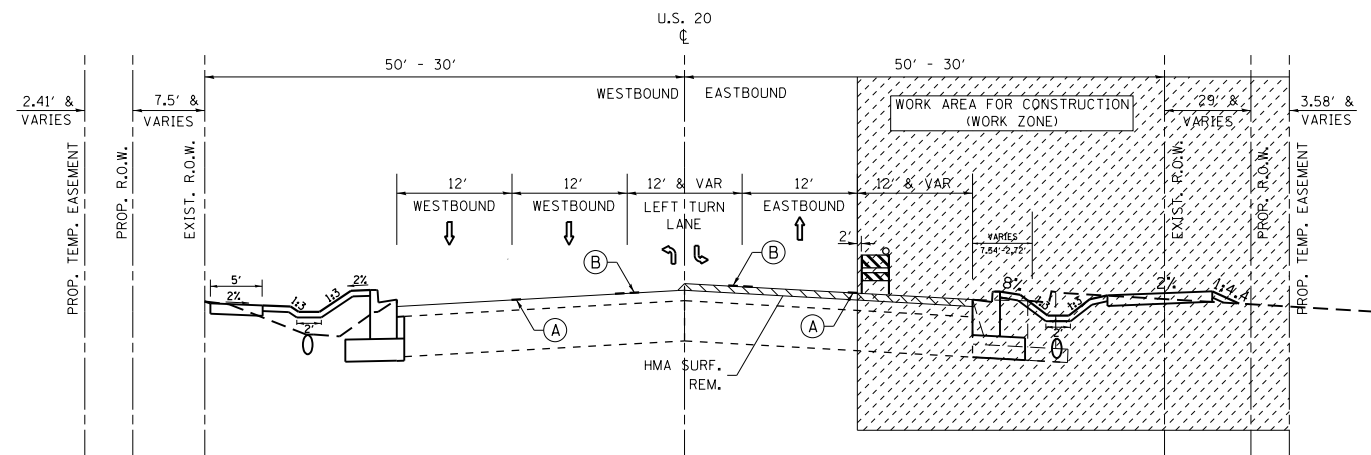
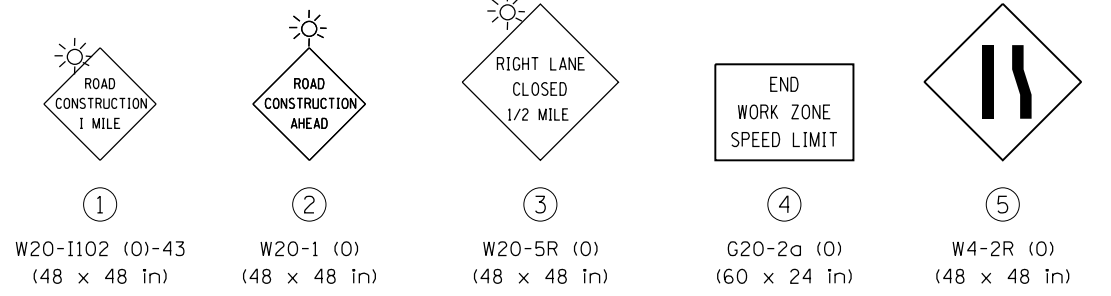
FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGING & TRAFFIC CONTROL PLAN, STAGE 1 U.S. 20 (OAK AVE. TO PARK AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\p\work\p\dot\galbenjr\d0304020\PI57	408-sh-t-Design-staging1.dgn	DRAWN -	REVISED -			345	7Y-WRS	COOK	97	23	
#MODELNAME#	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 60N18					
	PLOT DATE = 4/3/2013	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

LEGEND:

- (A) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID WHITE EDGE LINE
- (B) TEMPORARY PAVEMENT MARKING, TYPE III
4" SOLID DOUBLE YELLOW
- (C) TEMPORARY PAVEMENT MARKING, TYPE III
6" SKIP-DASH WHITE (TURN LANE)
- (D) TEMPORARY PAVEMENT MARKING, TYPE III
6" SOLID WHITE (TURN LANE)
- (E) TEMPORARY PAVEMENT MARKING, TYPE III
12" SOLID YELLOW MEDIAN DIAGONAL, 50' C-C
- (F) TEMPORARY PAVEMENT MARKING, TYPE III
24" SOLID WHITE (STOP BAR)



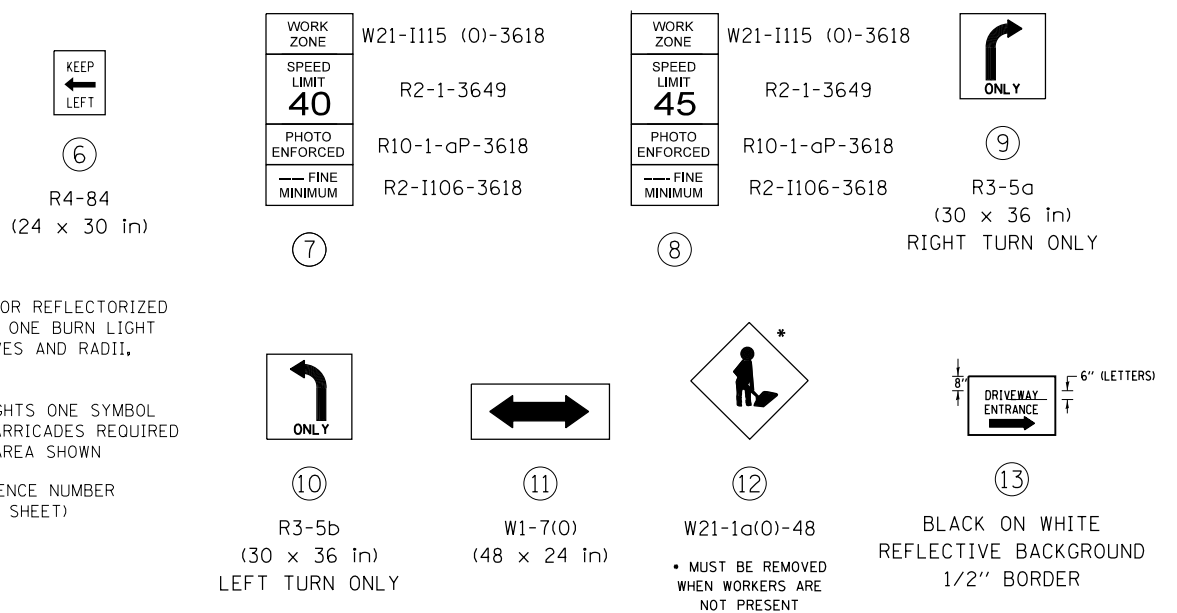
FOR WIDENING AND RESURFACING OPERATIONS THE SIGNING, BARRICADING AND LANE CLOSURE SHALL BE IN ACCORDANCE WITH STDS. 701602 AND 701606 IN ADDITION TO THE SIGNS SHOWN.



**U.S. 20 (LAKE STREET)
STAGE II CONSTRUCTION
STA. 515+78.66 TO STA. 539+34.72**

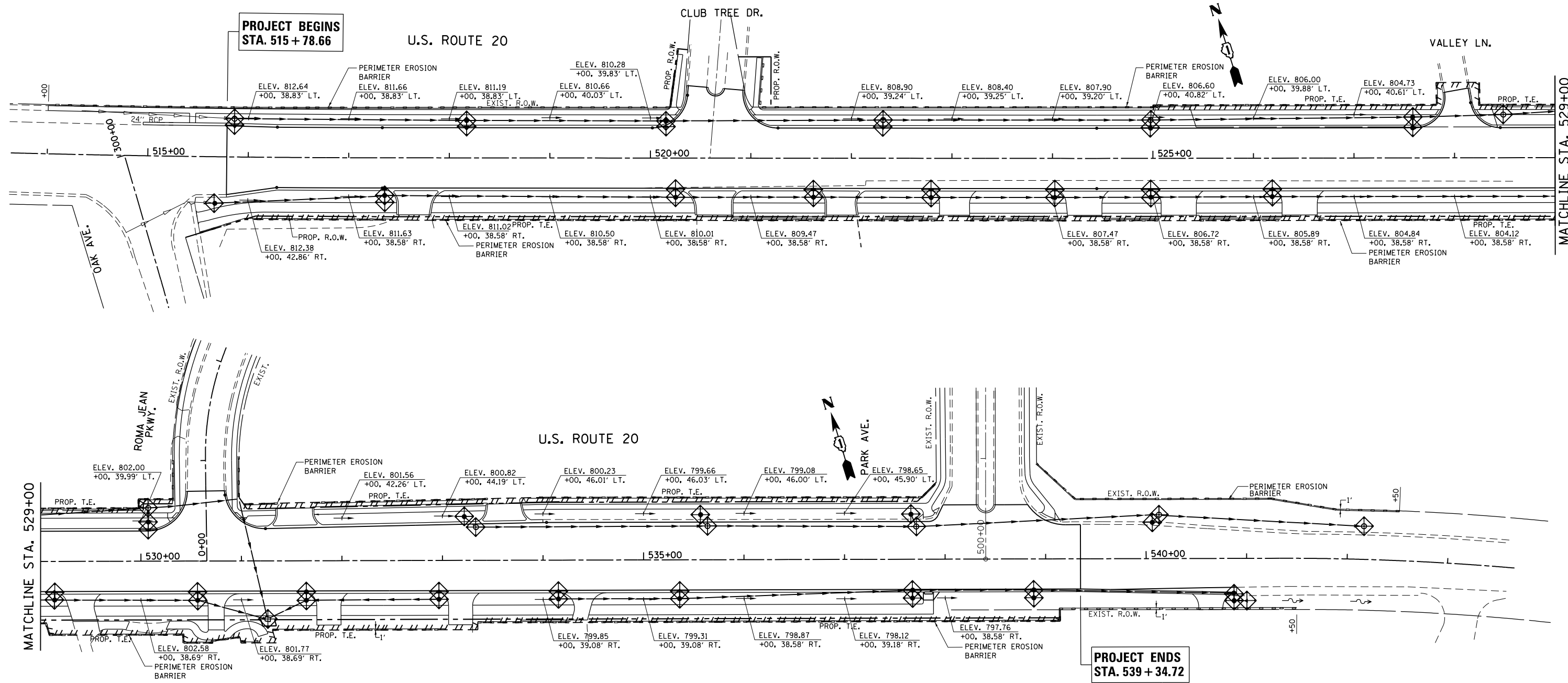
EASTBOUND: (WIDENING SECTION)
2" HMA SURFACE COURSE, MIX "F", N90
2-1/2" POLY. HMA BINDER COURSE, IL-19, N90
8" HMA BASE COURSE
12" AGG. SUBGRADE IMPROVEMENT

- BARRICADE TYPE I OR TYPE II OR REFLECTORIZED DRUM OR VERTICAL PANEL WITH ONE BURN LIGHT @ 50'C-C (TYP), 25'C-C IN CURVES AND RADII, AND 20'C-C IN TAPERS.
- ⊥ TYPE III BARRICADE WITH 2 LIGHTS ONE SYMBOL REPRESENTS ANY NUMBER OF BARRICADES REQUIRED TO ADEQUATELY PROTECT THE AREA SHOWN
- + CONSTRUCTION SIGN AND REFERENCE NUMBER (REFER TO SIGN DETAIL LEGEND SHEET)
- ↑ TRAFFIC LANE
- ARROW BOARD
- SHOULDER REMOVAL
- ▨ HMA SURF. REMOVAL
- ▩ WORK ZONE



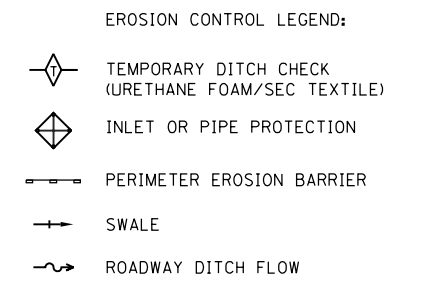
TRAFFIC CONTROL SIGNS

FILE NAME = c:\pwwork\pwwork\galbenj\d0304020\PI57	USER NAME = galbenj 408-sh-t-Design-staging2.dgn	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUGGESTED STAGING & TRAFFIC CONTROL PLAN, STAGE II U.S. 20 (OAK AVE. TO PARK AVE.)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
MODELNAME	PLOT DATE = 4/3/2013	DRAWN -	REVISED -					345	7Y-WRS	COOK	97	25
		CHECKED -	REVISED -		SCALE: 1"=50'			CONTRACT NO. 60N18				
		DATE -	REVISED -		SHEET OF SHEETS STA. TO STA.			ILLINOIS FED. AID PROJECT				



EROSION CONTROL GENERAL NOTES:

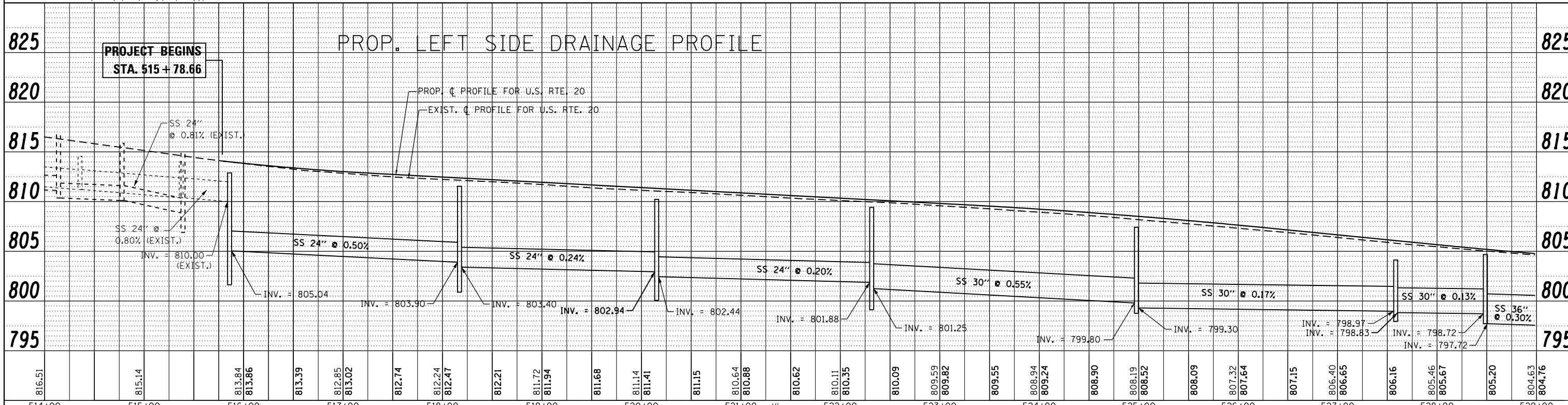
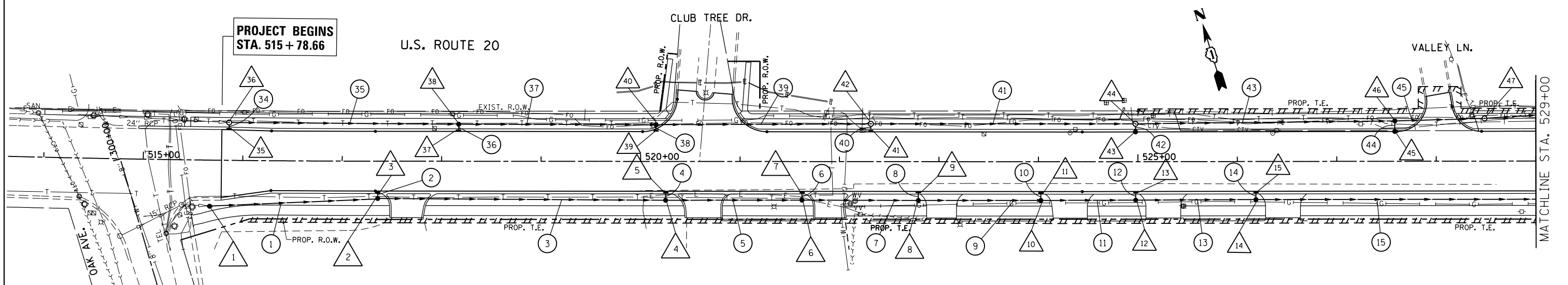
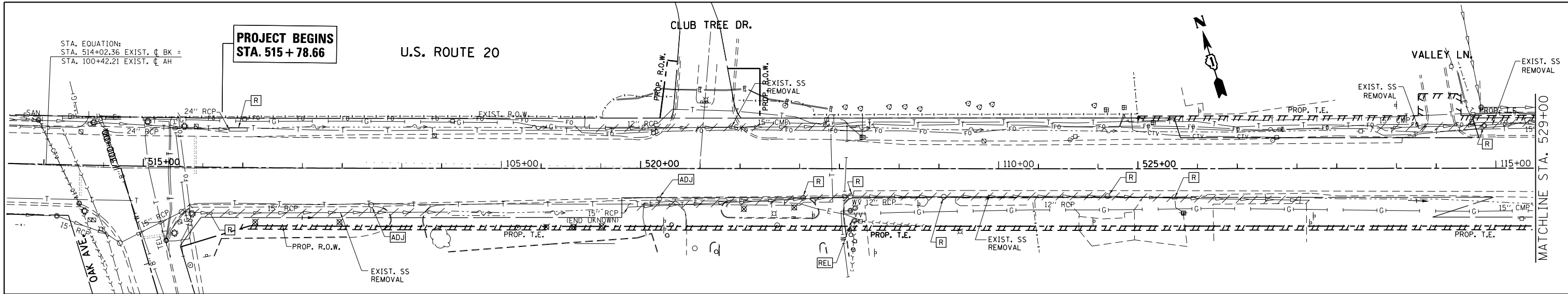
1. TEMPORARY EROSION CONTROL SEEDING TO BE PROVIDED AT ALL ERODIBLE BARE EARTH AREAS.
2. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN INLET AND OR PIPE PROTECTION AT ALL DRAINAGE STRUCTURES AND AT OTHER LOCATIONS AS DIRECTED BY THE ENGINEER. INLET PROTECTION SHALL BE PLACED IMMEDIATELY AFTER THE AREA INVOLVED HAS BEEN DISTURBED.
3. ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE STATE STANDARD FOR THE ENTIRE DURATION OF THE CONTRACT OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.
4. PERIMETER EROSION BARRIER SHALL BE PLACED 12 INCHES FROM THE R.O.W. OR EASEMENT LINE AS DIRECTED BY THE ENGINEER.
5. AT ANY AREA WHERE THERE IS NO PROPOSED GRADING, THE EXISTING GROUND COVER SHALL REMAIN.
6. DURING CONSTRUCTION AND WHEN ERODIBLE SOILS ARE EXPOSED, ALL DRAINAGE STRUCTURES NOT PROTECTED WITH INLET OR PIPE PROTECTION SHALL BE KEPT SEALED TO PREVENT ACCESS FROM EROSION. THIS WORK SHALL BE CONSIDERED AS PART OF THE COST OF THE VARIOUS DRAINAGE STRUCTURES.
7. EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE BEGINNING ANY CONSTRUCTION ACTIVITIES WHICH POTENTIALLY CREATE ERODIBLE CONDITIONS.
8. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THE PLANS ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
9. THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN SEDIMENT CONTROL MEASURES PRIOR TO STRIPPING EXISTING VEGETATION.
10. ANY AREA WHERE THERE IS NO PROPOSED GRADING THE EXISTING GROUND COVER SHALL REMAIN.
11. THE ENGINEER WILL CONTACT FABIOLA QUIROZ OF THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4596, AT LEAST 7 DAYS PRIOR TO THE PLANTING FOR LAYOUT APPROVAL OF THE TREES.



FILE NAME =		USER NAME = galbenjr		DESIGNED -		REVISED -		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		EROSION CONTROL PLAN U.S. ROUTE 20 (OAK AVE. - PARK AVE.)				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\galbenjr\d0304019\P157408-sht-er.os.dgn		PLOT SCALE = 100.0000' / 1" =		DRAWN -		REVISED -				345	7Y-WRS	COOK	97	26				
#MODELNAME#		PLOT DATE = 4/3/2013		CHECKED -		REVISED -				CONTRACT NO. 60N18				ILLINOIS FED. AID PROJECT				
				DATE -		REVISED -				SCALE: 1"=50'	SHEET	OF	SHEETS	STA.	TO	STA.		

DATE	
BY	
SURVEYED	
PLOTTED	
ALIGNED	
CHECKED	
FILE NAME	
NO.	

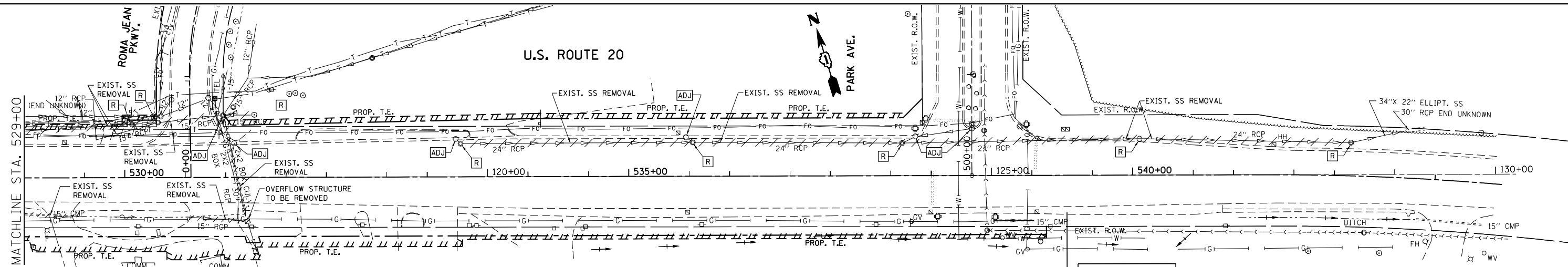
DATE	
BY	
SURVEYED	
PLOTTED	
GRADES CHECKED	
STRUCTURE	
NOTATION	
NO.	



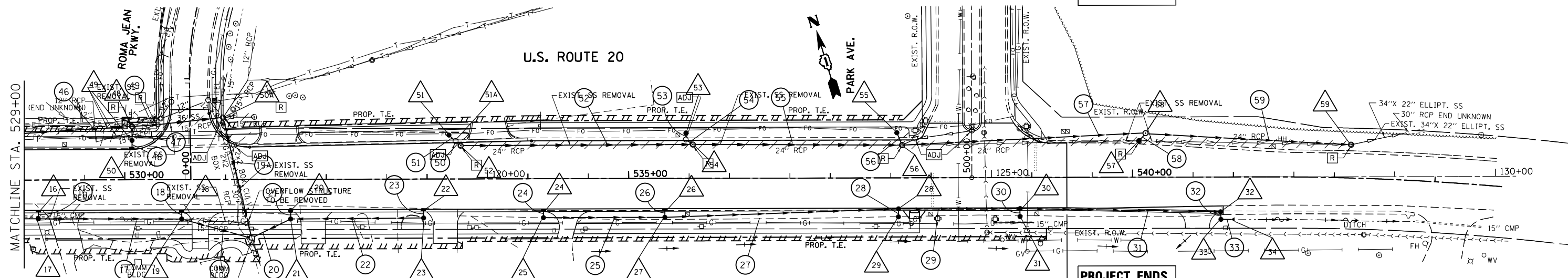
FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRAINAGE PLAN AND PROFILE (LEFT SIDE) U.S. ROUTE 20 (OAK AVE. - PARK AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	
c:\pwork\pwork\galbanjr\d0304019\p157408-sht-drain.dgn		DRAWN -	REVISED -			345	7Y-WRS	COOK	97	27
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -			CONTRACT NO. 60N18				
PLOT DATE = 4/3/2013		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	NO.	

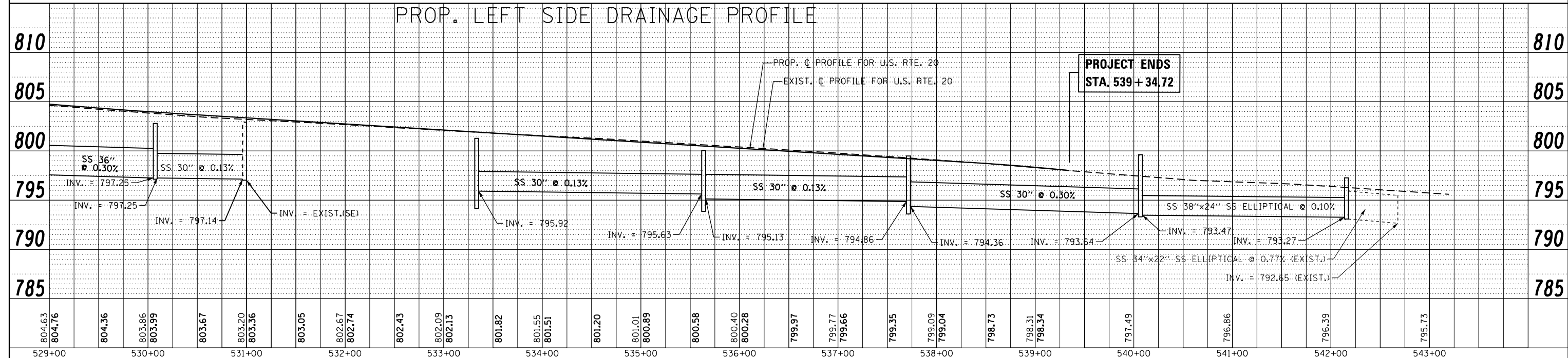


**PROJECT ENDS
STA. 539 + 34.72**



**PROJECT ENDS
STA. 539 + 34.72**

PROP. LEFT SIDE DRAINAGE PROFILE

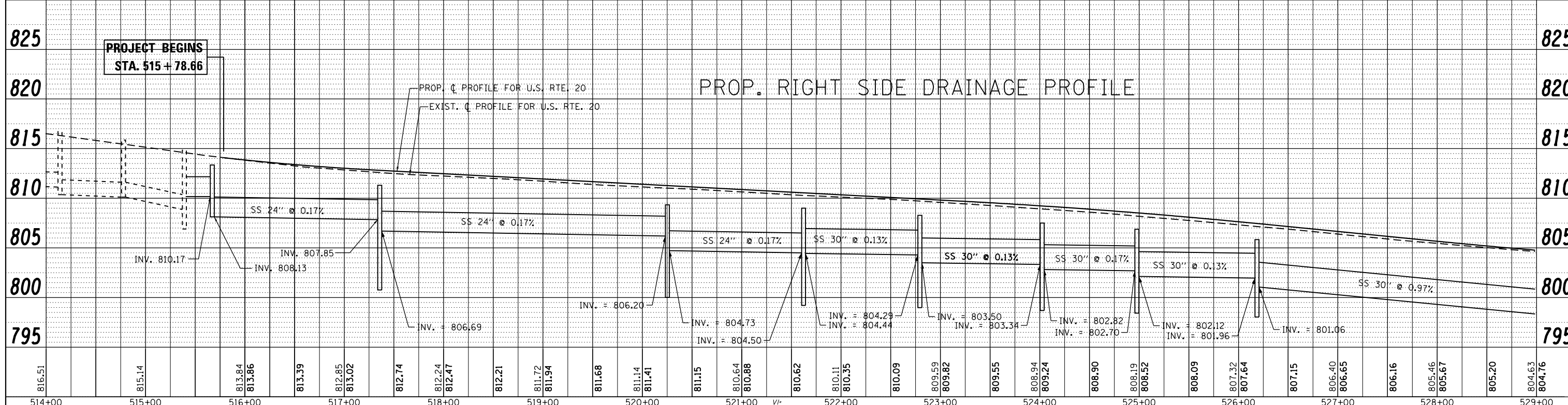
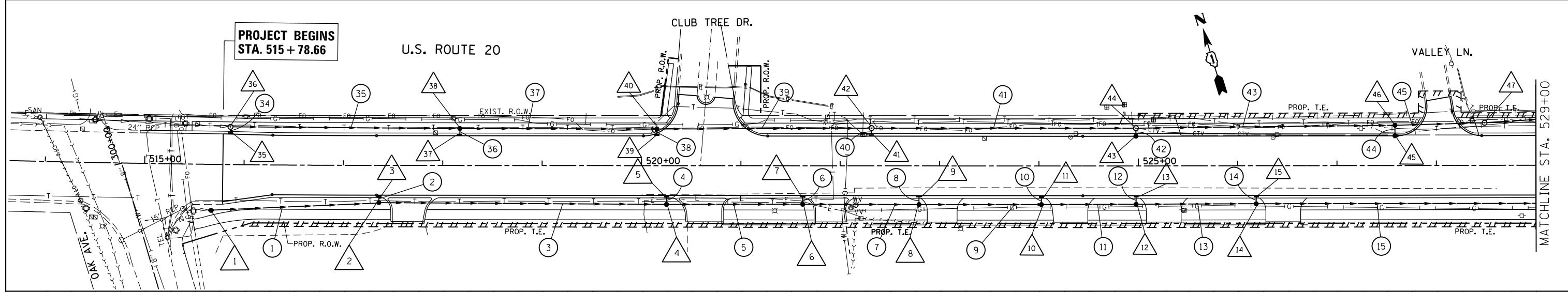
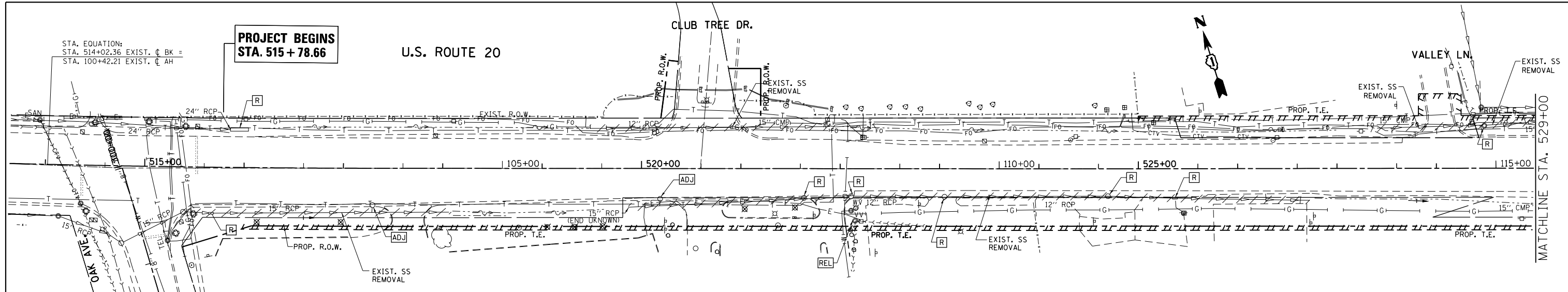


**PROJECT ENDS
STA. 539 + 34.72**

FILE NAME =	USER NAME = galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRAINAGE PLAN AND PROFILE (LEFT SIDE)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\galbanjr\0304019P157408-shd-drain.dgn		DRAWN -	REVISED -		U.S. ROUTE 20 (OAK AVE. - PARK AVE.)			345	7Y-WRS	COOK	97	28
PLOT SCALE = 100.0000' / in.		CHECKED -	REVISED -		SCALE: 1"=50'	SHEET NO.	OF SHEETS	STA. 529+00	TO STA. 543+66.67		CONTRACT NO. 60N18	
PLOT DATE = 4/3/2013		DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

DATE	
BY	
SURVEYED	
PLOTTED	
ALIGNED	
CHECKED	
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	

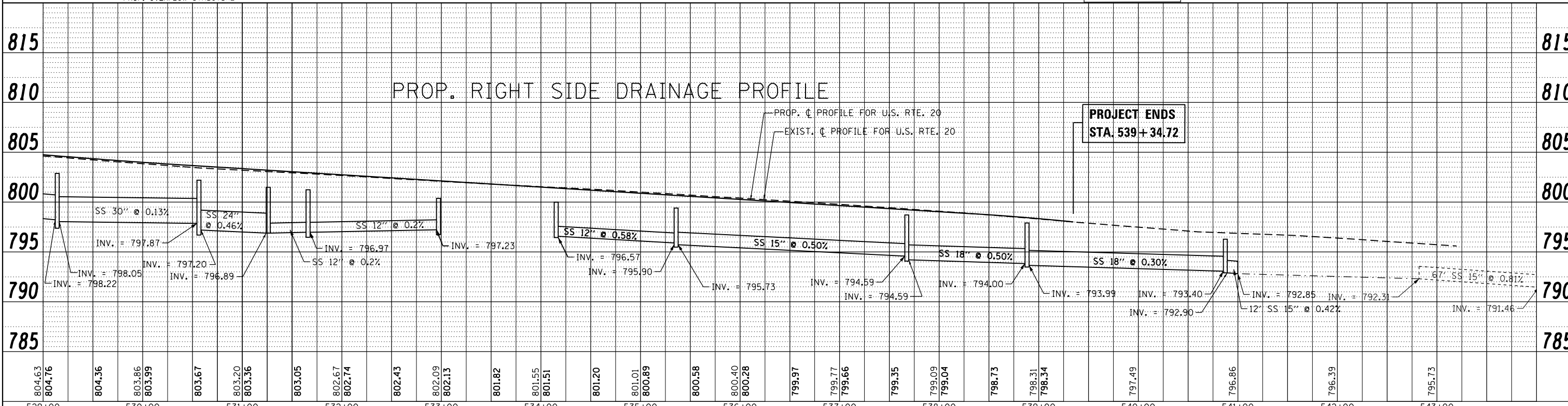
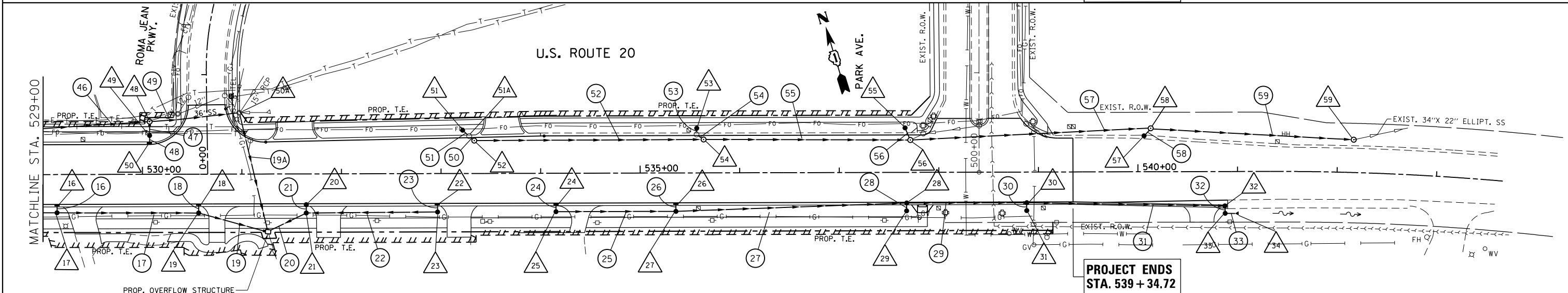
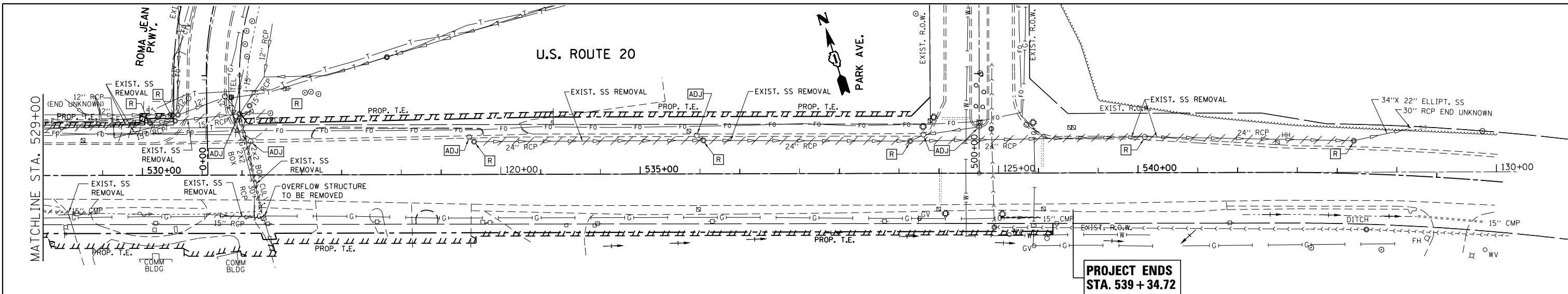
DATE	
BY	
SURVEYED	
PLOTTED	
GRADES CHECKED	
STRUCTURE	
NOTATIONS CHECKED	
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	



825	PROJECT BEGINS STA. 515 + 78.66																				825																					
820	PROP. C. PROFILE FOR U.S. RTE. 20																				820																					
815	EXIST. C. PROFILE FOR U.S. RTE. 20																				815																					
810																					810																					
805	SS 24" @ 0.17%																				805																					
800	SS 30" @ 0.13%																				800																					
795	SS 30" @ 0.97%																				795																					
816.51	815.14	813.84	813.86	813.39	812.85	813.02	812.74	812.24	812.47	812.21	811.72	811.94	811.68	811.14	811.41	811.15	810.64	810.88	810.62	810.11	810.35	810.09	809.59	809.82	809.55	808.94	809.24	808.90	808.19	808.52	808.09	807.32	807.64	807.15	806.40	806.65	806.16	805.46	805.67	805.20	804.63	804.76
514+00	515+00	516+00	517+00	518+00	519+00	520+00	521+00	522+00	523+00	524+00	525+00	526+00	527+00	528+00	529+00																											

PLAN	SURVEYED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	FILED	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIS	
	CPAD	
	NO.	



804.63	804.76	804.36	803.86	803.99	803.67	803.20	803.36	803.05	802.67	802.74	802.43	802.09	802.13	801.82	801.55	801.51	801.20	801.01	800.89	800.58	800.40	800.28	799.97	799.77	799.66	799.35	799.09	799.04	798.73	798.31	798.34	797.49	796.86	796.39	795.73	815			
529+00	530+00	531+00	532+00	533+00	534+00	535+00	536+00	537+00	538+00	539+00	540+00	541+00	542+00	543+00																									

PART OF THE SW 14 OF SECTION 26, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	REMAINDER AREA ACRES	PREVIOUSLY DEDICATED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
OHV0001	NORTH STAR TRUST CO. TRUST NO. 04-7948	3.450	0.115	3.335	N/A	N/A	N/A	06-26-302-012	
OHV0001TE			N/A	N/A	N/A	0.032	CONSTRUCTION	06-26-302-013	

LEGEND

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINES
 PROPERTY (DEED) LINE
 APPARENT PROPERTY LINE
 EXISTING CENTERLINE
 PROPOSED CENTERLINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORDED DIMENSION
 EXISTING BUILDING

SECTION CORNER
 QUARTER SECTION CORNER

GRAPHIC SCALE
 FEET
 0 50.100
 20.40
 30.60
 SCALE: 1" = 30'

Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

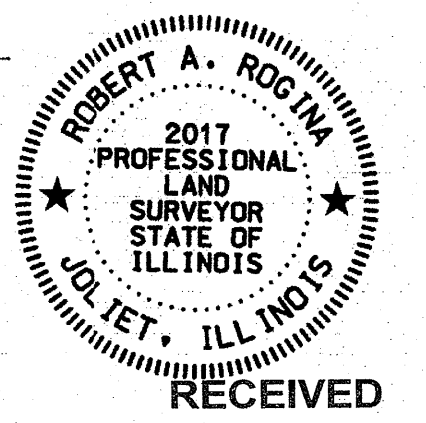
STATE OF ILLINOIS)
 COUNTY OF WILL)
 THIS IS TO CERTIFY THAT I, ROBERT A. ROGINA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HERE ON IN SECTIONS 26 AND 35, TOWNSHIP 41 NORTH, AND RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS, THIS ___ DAY OF ___, 2012 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
 LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.



RECEIVED
 JUN 19 2012
 PLATS & LEGALS

C-01-OAK AVE.
 $\Delta = 25^\circ 37' 37''$ (LT)
 $CB = N13^\circ 31' 37'' E$
 $CH = 636.13'$
 $L = 641.46'$
 $R = 1,434.16'$
 $T = 326.19'$
 $P.C. STA. = 292+30.17$
 $PI STA. = 295+56.36$
 $P.T. STA. = 298+71.63$

NW CRN OF NW 1/4
 SEC 35-41-9
 (972)
 $N = 1943458.66300$
 $E = 1024859.22200$

C-3 (PROPOSED)
 $\Delta = 2^\circ 13' 11''$
 $CB = S71^\circ 49' 11'' E$
 $CH = 581.06'$
 $L = 581.10'$
 $R = 15,000.00'$
 $T = 290.58'$
 $PC STA. = 511+52.26$
 $PI STA. = 514+42.84$
 $PT STA. = 517+33.35$

C7 (EXISTING)
 $\Delta = 04^\circ 53' 00''$
 $CB = S70^\circ 33' 51'' E$
 $CH = 976.37'$
 $L = 976.66'$
 $R = 11,459.20'$
 $T = 488.63'$
 $PC STA. = 242+16.10$
 $PI STA. = 247+04.73$
 $PT STA. = 251+92.76$

THE OAKS UNIT NO. 2
 RECORDED APRIL 19, 1966
 DOC. NO. 1980129
 AND PLAT OF CORRECTION
 DOC. NO. 22977911

TOTAL HOLDING
 PARCEL OHV0001
 06-26-302-013

OAKFIELD CENTER SUBDIVISION
 RECORDED AUGUST 29, 1983
 DOC. NO. 26752891

PREPARED BY: **ROGINA & ASSOCIATES, LTD.**
 ENGINEERS SURVEYORS PLANNERS
 93 Caterpillar Drive · Joliet, Illinois · 815/729-0777 · FAX 815/729-0782
 PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.
 SECTION: COUNTY: COOK
 PROJECT: JOB NO.: R-90-019-11
 STATION: 515+00 TO STATION: 520+00
 SCALE: 1"=30' SHEET 2 OF

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

REVISION DATE: 06/12/12

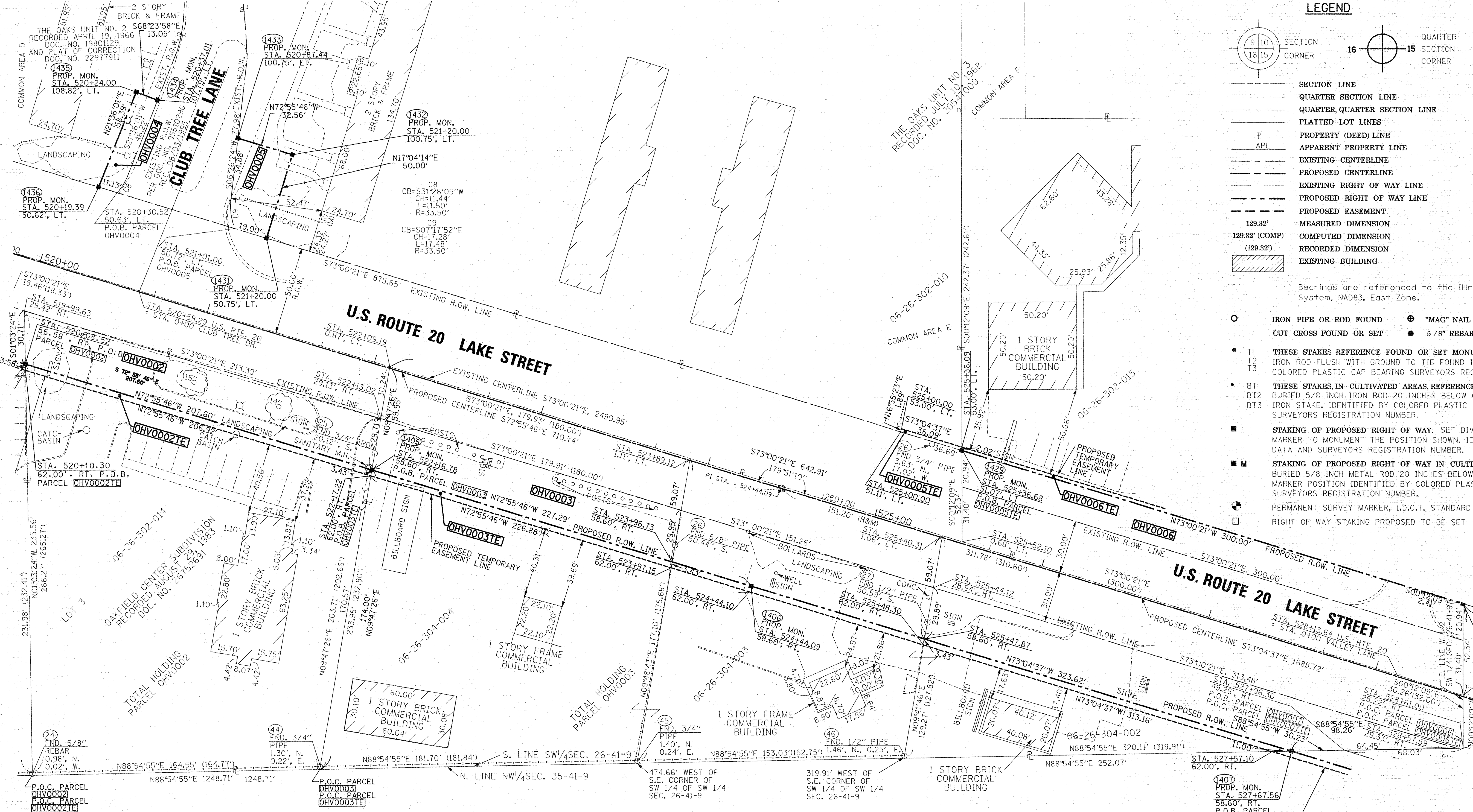
REVISION: PER IDOT

MADE BY: JPS/MRR

97

31

PART OF THE SW 14 OF SECTION 26, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.



LEGEND

SECTION CORNER: 9 10 16 15
 QUARTER CORNER: 16 15

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINES
 PROPERTY (DEED) LINE
 APPARENT PROPERTY LINE
 EXISTING CENTERLINE
 PROPOSED CENTERLINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 COMPUTED DIMENSION
 RECORDED DIMENSION
 EXISTING BUILDING

GRAPHIC SCALE
 FEET
 0 50.00
 20.00
 30.00
 SCALE: 1" = 30'

Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

○ IRON PIPE OR ROD FOUND ⊕ "MAG" NAIL SET
 + CUT CROSS FOUND OR SET ● 5/8" REBAR SET

• T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 • T2
 • T3

• BT1 THESE STAKES IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION.
 • BT2 BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
 • BT3

■ STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

■ M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)

□ RIGHT OF WAY STAKING PROPOSED TO BE SET

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	REMAINDER AREA ACRES	PREVIOUSLY DEDICATED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
OHV0002	GILL PROPERTIES LLC -SERIES 3907	0.997	0.142	0.855	N/A	N/A	N/A	06-26-302-014	
OHV0002TE			N/A	N/A	N/A	0.016	CONSTRUCTION		
OHV0003	SEM VANDERBILT, LLC	1.910	0.851	1.059	0.445	N/A	N/A	06-26-304-002 06-26-304-003 06-26-304-004	
OHV0003TE			N/A	N/A	N/A	0.043	CONSTRUCTION		
OHV0004	THE OAKS IMPROVEMENT ASSOCIATION	5.004	0.017	4.987	N/A	N/A	N/A	06-26-302-007	
OHV0005	THE OAKS IMPROVEMENT ASSOCIATION	7.943	0.031	7.912	N/A	N/A	N/A	06-26-302-010	
OHV0005TE			N/A	N/A	N/A	0.002	CONSTRUCTION		69 SO. FT.

SEE SHEET 7 FOR TOTAL HOLDING OF PARCEL OHV0004 AND OHV0005

REVISION DATE: 09/12/12

REVISION: ADD PARCELS OHV0004 & OHV0005

MADE BY: JCC/MRR

STATE OF ILLINOIS)
 COUNTY OF WILL)

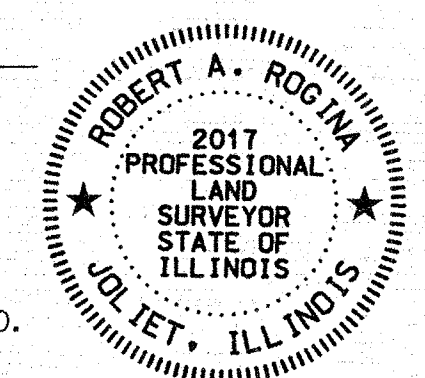
THIS IS TO CERTIFY THAT I, ROBERT A. ROGINA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HERE ON IN SECTIONS 26 AND 35, TOWNSHIP 41 NORTH, AND RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS ___ DAY OF ___, 2012 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
 LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.



RECEIVED
 SEP 14 2012
 PLATS & LEGALS

PREPARED BY: **ROGINA & ASSOCIATES, L.T.D.**
 ENGINEERS SURVEYORS PLANNERS
 93 Caterpillar Drive • Joliet, Illinois • 815/729-0777 • FAX 815/729-0782
 PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

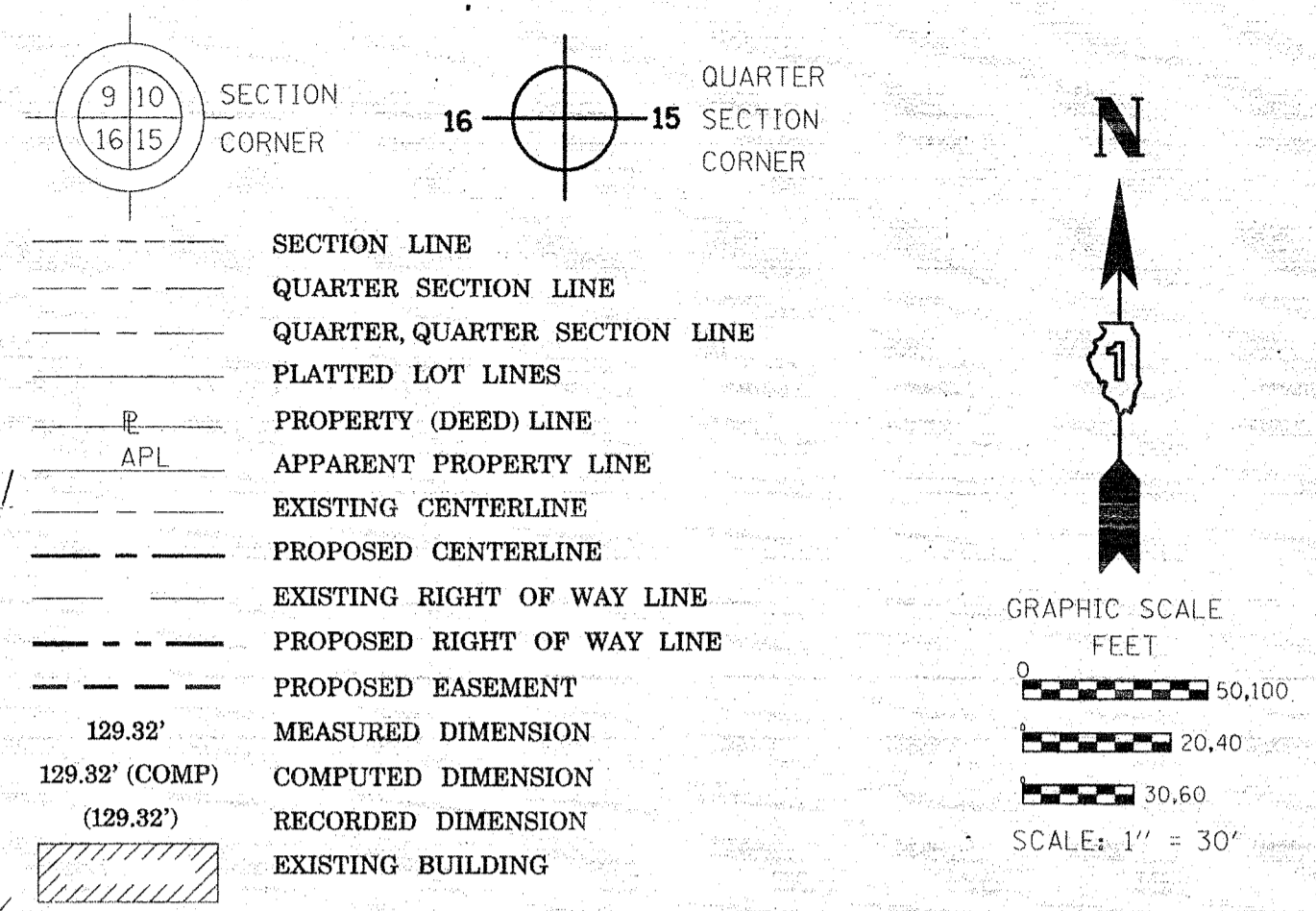
PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.

SECTION: COUNTY: COOK
 PROJECT: JOB NO.: R-90-019-11
 STATION: 520+00 TO STATION: 529+00
 SCALE: 1"=30' SHEET 3 OF

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

PART OF THE SW 14 OF SECTION 26, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.
 PART OF THE NW 14 OF SECTION 35, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

LEGEND



Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
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- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
 COUNTY OF WILL)

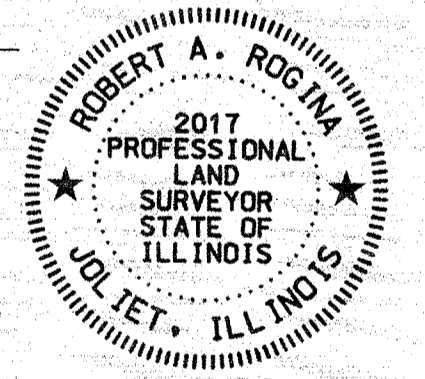
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DATED AT JOLIET, ILLINOIS THIS ___ DAY OF ___, 2012 A.D.

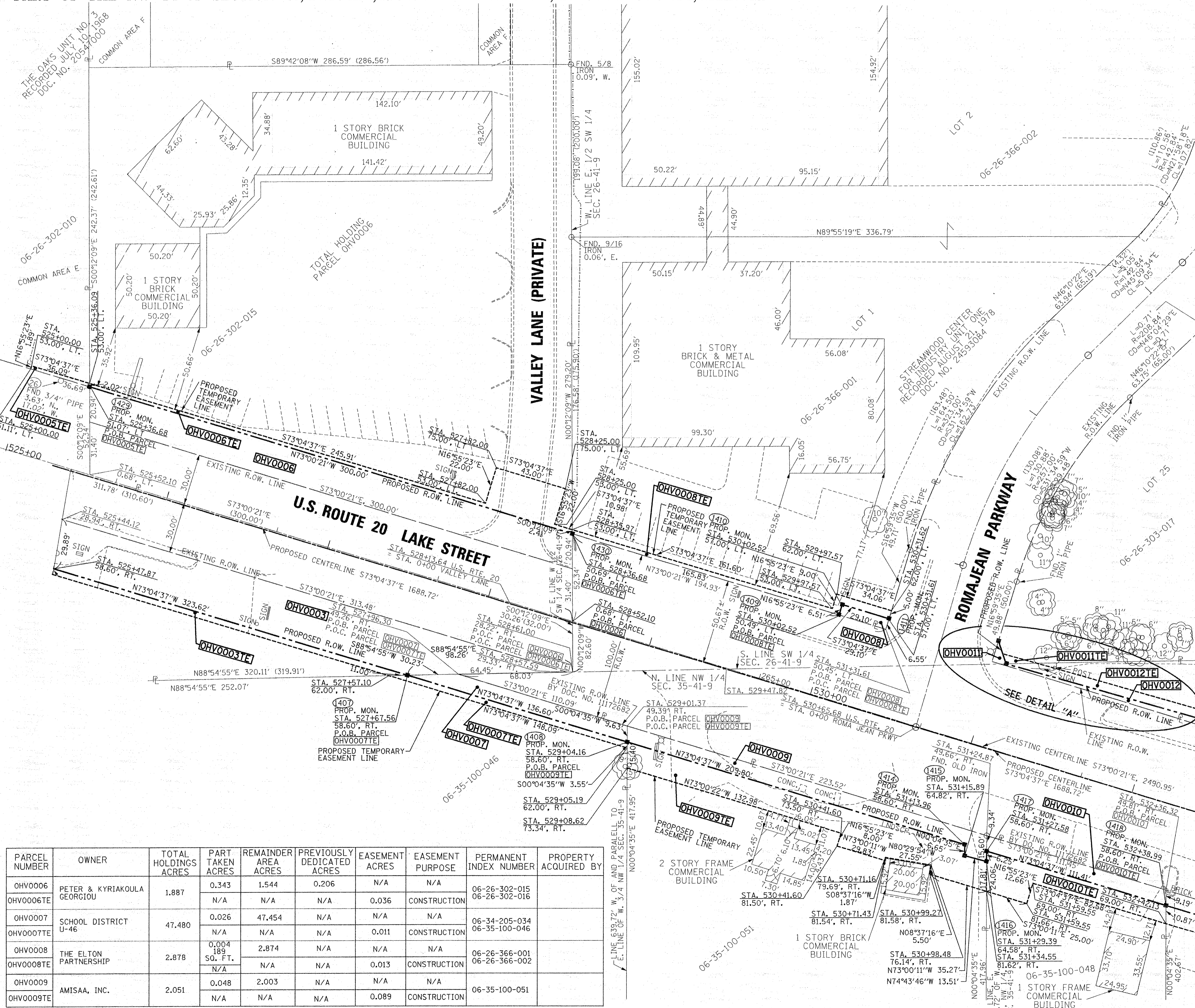
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
 LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.



RECEIVED
 SEP 19 2012
 PLATS & LEGALS



PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	REMAINDER AREA ACRES	PREVIOUSLY DEDICATED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
OHV0006	PETER & KYRIAKOULA GEORGIU	1.887	0.343	1.544	0.206	N/A	N/A	06-26-302-015	
OHV0006TE			N/A	N/A	N/A	0.036	CONSTRUCTION	06-26-302-016	
OHV0007	SCHOOL DISTRICT U-46	47.480	0.026	47.454	N/A	N/A	N/A	06-34-205-034	
OHV0007TE			N/A	N/A	N/A	0.011	CONSTRUCTION	06-35-100-046	
OHV0008	THE ELTON PARTNERSHIP	2.878	0.004	2.874	N/A	N/A	N/A	06-26-366-001	
OHV0008TE			189 SO. FT.	N/A	N/A	N/A	0.013	CONSTRUCTION	
OHV0009	AMISAA, INC.	2.051	0.048	2.003	N/A	N/A	N/A	06-35-100-051	
OHV0009TE			N/A	N/A	N/A	0.089	CONSTRUCTION		

SEE SHEET 7 FOR TOTAL HOLDING OF PARCEL OHV0007, OHV0008 AND OHV0009

REVISION DATE: 09/18/12

REVISION: REVISE PARCEL OHV0010TE

MADE BY: JCC/MRR

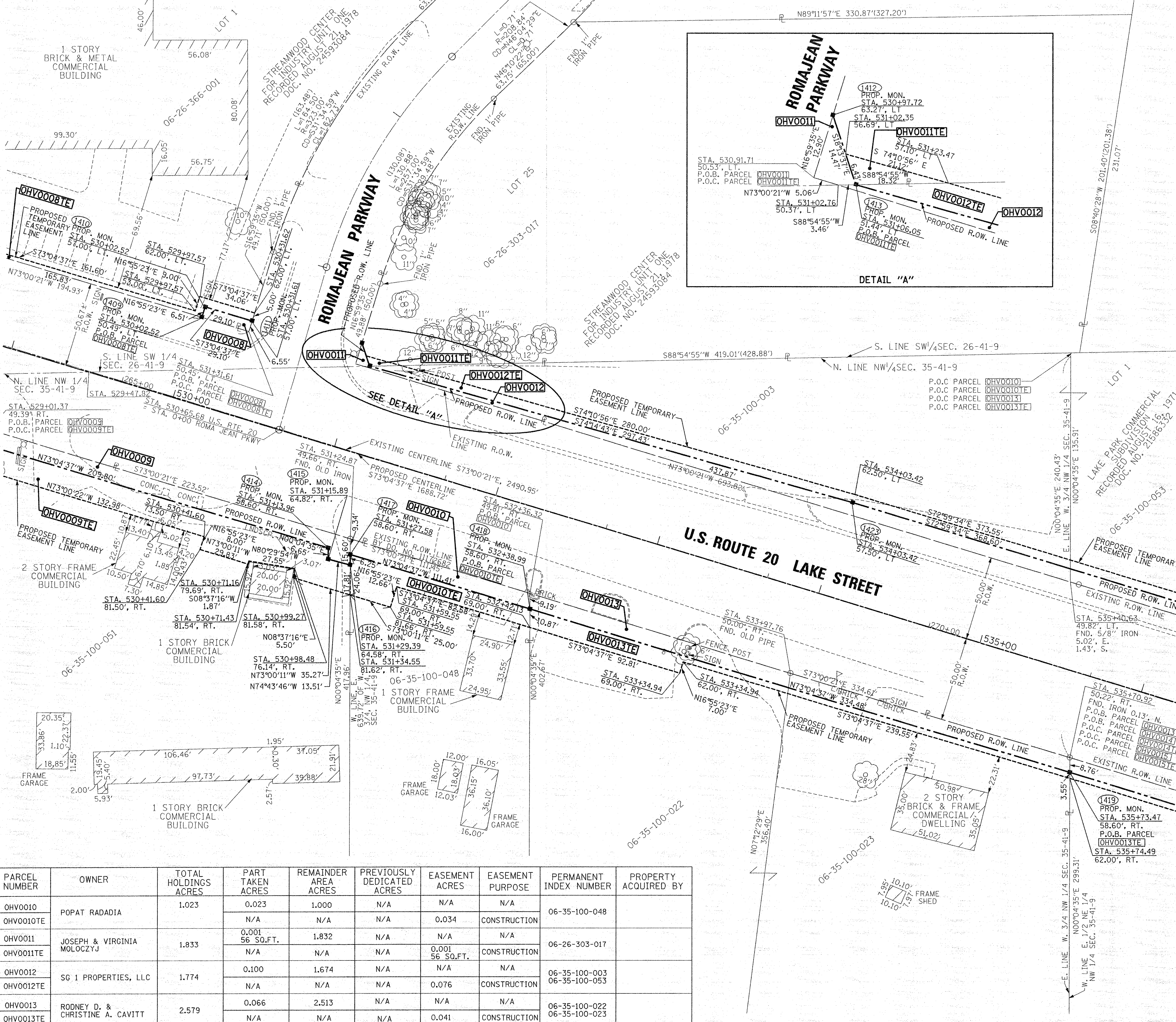
PREPARED BY: **ROGINA & ASSOCIATES, LTD.**
 ENGINEERS SURVEYORS PLANNERS
 93 Caterpillar Drive - Joliet, Illinois - 815/729-0777 - FAX 815/729-0782
 PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.

SECTION: COUNTY: COOK
 PROJECT: JOB NO.: R-90-019-11
 STATION: 529+00 TO STATION: 532+50
 SCALE: 1"=30' SHEET 4 OF

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

PART OF THE SW 1/4 OF SECTION 26 AND NW 1/4 OF SECTION 35, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.



LEGEND

SECTION CORNER 16 SECTION CORNER

SECTION LINE
 QUARTER SECTION LINE
 QUARTER, QUARTER SECTION LINE
 PLATTED LOT LINES
 PROPERTY (DEED) LINE
 APPARENT PROPERTY LINE
 EXISTING CENTERLINE
 PROPOSED CENTERLINE
 EXISTING RIGHT OF WAY LINE
 PROPOSED RIGHT OF WAY LINE
 PROPOSED EASEMENT
 MEASURED DIMENSION
 129.32'
 129.32' (COMP)
 129.32'
 EXISTING BUILDING

GRAPHIC SCALE
 FEET
 0 50.00
 20.40
 30.60
 SCALE: 1" = 30'

Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

- IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- TI
- T2
- BT1
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
 COUNTY OF WILL)

THIS IS TO CERTIFY THAT I, ROBERT A. ROGINA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HERE ON IN SECTIONS 26 AND 35, TOWNSHIP 41 NORTH, AND RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS THIS ___ DAY OF ___, 2012 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
 LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.

RECEIVED
 SEP 19 2012
 PLATS & LEGALS

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	REMAINDER AREA ACRES	PREVIOUSLY DEDICATED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
OHV0010	POPAT RADADIA	1.023	0.023	1.000	N/A	N/A	N/A	06-35-100-048	
OHV0010TE			N/A	N/A	N/A	0.034	CONSTRUCTION		
OHV0011	JOSEPH & VIRGINIA MOLOCZYJ	1.833	0.001 56 SQ.FT.	1.832	N/A	N/A	N/A	06-26-303-017	
OHV0011TE			N/A	N/A	N/A	0.001 56 SQ.FT.	CONSTRUCTION		
OHV0012	SG 1 PROPERTIES, LLC	1.774	0.100	1.674	N/A	N/A	N/A	06-35-100-003	
OHV0012TE			N/A	N/A	N/A	0.076	CONSTRUCTION	06-35-100-053	
OHV0013	RODNEY D. & CHRISTINE A. CAVITT	2.579	0.066	2.513	N/A	N/A	N/A	06-35-100-022	
OHV0013TE			N/A	N/A	N/A	0.041	CONSTRUCTION	06-35-100-023	

SEE SHEET 7 FOR TOTAL HOLDING OF PARCEL OHV0010, OHV0011, OHV0012 AND OHV0013

REVISION DATE: 09/18/12

REVISION: REVISE PARCEL OHV0010TE

MADE BY: JCC/MRR

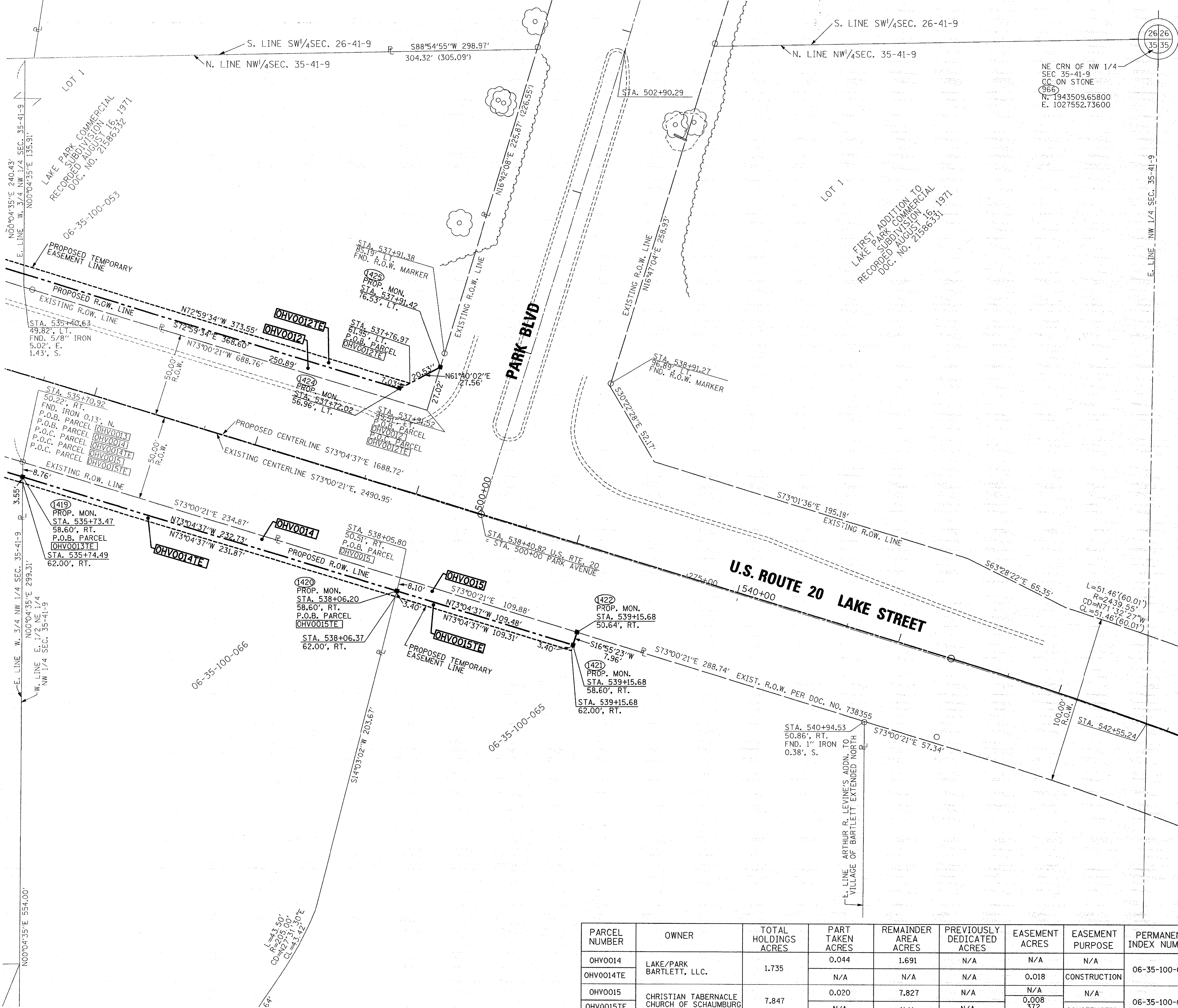
PREPARED BY: **ROGINA & ASSOCIATES, LTD.**
 ENGINEERS SURVEYORS PLANNERS
 93 Caterpillar Drive - Joliet, Illinois - 815/729-0777 - FAX 815/729-0782
 PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.

SECTION: COUNTY: COOK
 PROJECT: JOB NO.: R-90-019-11
 STATION: 532+50 TO STATION: 536+00
 SCALE: 1"=30' SHEET 5 OF

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

PART OF THE NW 1/4 OF SECTION 35, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.



LEGEND

SECTION CORNER: 9 10 16 15

QUARTER SECTION CORNER: 16

GRAPHIC SCALE: 0 50,100 FEET; 0 20,40 30,60 FEET; SCALE: 1" = 30'

Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING

IRON PIPE OR ROD FOUND

CUT CROSS FOUND OR SET

"MAG" NAIL SET

5/8" REBAR SET

TI THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.

BT2

BT3

STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.

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PERMANENT SURVEY MARKER, I.D.O.T. STANDARD, 2135 (TO BE SET BY OTHERS)

RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
) SS
 COUNTY OF WILL)

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DATED AT JOLIET, ILLINOIS THIS ___ DAY OF ___, 2012 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
 LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.

RECEIVED
 JUN 28 2012
 PLATS & LEGALS

PREPARED BY: **ROGINA & ASSOCIATES, LTD.**
 ENGINEERS SURVEYORS PLANNERS
 93 Caterpillar Drive · Joliet, Illinois · 815/729-0777 · FAX 815/729-0782
 PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.

SECTION: COUNTY: COOK
 PROJECT: JOB NO.: R-90-019-11
 STATION: 536+00 TO STATION: 542+00
 SCALE: 1"=30' SHEET 6 OF

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	REMAINDER AREA ACRES	PREVIOUSLY DEDICATED ACRES	EASEMENT ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
OHV0014	LAKE/PARK BARTLETT, LLC.	1.735	0.044	1.691	N/A	N/A	N/A	06-35-100-066	
OHV0014TE			N/A	N/A	N/A	0.018	CONSTRUCTION		
OHV0015	CHRISTIAN TABERNAACLE CHURCH OF SCHAUMBURG	7.847	0.020	7.827	N/A	N/A	N/A	06-35-100-065	
OHV0015TE			N/A	N/A	N/A	0.008	CONSTRUCTION		

PART OF THE SW 1/4 SECTION 26, NE 1/4 SECTION 34 AND NW 1/4 OF SECTION 35, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

PROPOSED MONUMENTATION

POINT NUMBER	NORTHING	EASTING
1400	1943822.6999	1024925.5231
1401	1943822.6344	1024998.4563
1402	1943776.1354	1025148.0849
1404	1943704.1675	1025382.4498
1405	1943643.2277	1025580.9016
1406	1943576.5046	1025798.1868
1407	1943482.3016	1026107.7949
1408	1943442.5384	1026238.4811
1409	1943518.2717	1026364.3366
1410	1943524.5008	1026366.2319
1411	1943516.0305	1026394.0707
1412	1943502.7863	1026459.1357
1413	1943489.0391	1026463.6627
1414	1943381.4690	1026439.1921
1415	1943374.8139	1026439.1832
1416	1943371.2556	1026452.2162
1417	1943377.5038	1026452.2246
1418	1943345.0747	1026558.8065
1419	1943247.7113	1026878.8022
1420	1943179.9652	1027101.4574
1421	1943148.0972	1027206.1950
1422	1943155.7105	1027208.5114
1423	1943408.2814	1026749.9179
1424	1943300.4697	1027102.3978
1425	1943313.5475	1027126.6527
1429	1943654.4228	1025918.8430
1430	1943566.7407	1026205.7434

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APL APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
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- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING

GRAPHIC SCALE
FEET
0 50,100
20,40
30,60
SCALE: 1" = 200'

Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

- IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- "MAG" NAIL SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
)SS
COUNTY OF WILL)

THIS IS TO CERTIFY THAT I, ROBERT A. ROGINA, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HERE ON IN SECTIONS 26 AND 35, TOWNSHIP 41 NORTH, AND RANGE 9 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT JOLIET, ILLINOIS, THIS ____ DAY OF ____, 2012 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.



RECEIVED
JUN 28 2012
PLATS & LEGALS

PREPARED BY: **ROGINA & ASSOCIATES, LTD.**
ENGINEERS SURVEYORS PLANNERS
93 Caterpillar Drive · Joliet, Illinois · 815/729-0777 · FAX 815/729-0782
PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.

SECTION: COUNTY: COOK
PROJECT: JOB NO.: R-90-019-11
STATION: TO STATION:
SCALE: 1"= SHEET 7 OF

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PART OF THE SW 14 SECTION 26, NE 14 SECTION 34 AND NW 14 OF SECTION 35, TWP. 41N., R. 9E. OF THE 3RD. P.M., IN COOK COUNTY, ILLINOIS.

C13 CB=N72°25'21"W CH=232.38' L=232.38' R=11,409.20'	C19 CB=S00°12'09"E CH=226.68' L=233.03' R=287.05'	C25 CB=S58°35'53"E CH=57.45' L=62.47' R=44.43'	C31 CB=N05°38'53"W CH=261.11' L=274.70' R=250.25'
C14 CB=S34°09'35"E CH=42.40' L=44.69' R=40.00'	C20 CB=S42°32'47"W CH=114.36' L=139.75' R=65.02'	C26 CB=N86°18'22"E CH=10.43' L=10.45' R=57.73'	C32 CB=N03°45'33"E CH=153.21' L=155.53' R=259.53'
C15 CB=S05°23'32"E CH=39.03' L=39.13' R=163.47'	C21 CB=N51°26'06"W CH=23.61' L=24.59' R=25.00'	C27 CB=S81°02'46"E CH=104.83' L=106.20' R=190.25'	C33 CB=N28°54'03"E CH=18.40' L=20.19' R=13.67'
C16 CB=N61°28'21"W CH=66.39' L=68.94' R=72.75'	C22 CB=N85°13'57"W CH=76.76' L=90.09' R=46.71'	C28 CB=S39°12'14"E CH=51.95' L=52.70' R=90.15'	C34 CB=N79°55'36"W CH=108.30' L=108.72' R=357.34'
C17 CB=S77°51'34"E CH=63.32' L=79.43' R=34.850'	C23 CB=S56°48'02"W CH=18.51' L=20.70' R=12.80'	C29 CB=S04°18'26"W CH=40.13' L=40.41' R=90.15'	
C18 CB=S05°09'56"W CH=140.50' L=142.55' R=242.18'	C24 CB=S53°19'45"E CH=12.37' L=12.75' R=15.00'	C30 CB=N17°42'16"W CH=232.70' L=236.24' R=393.16'	

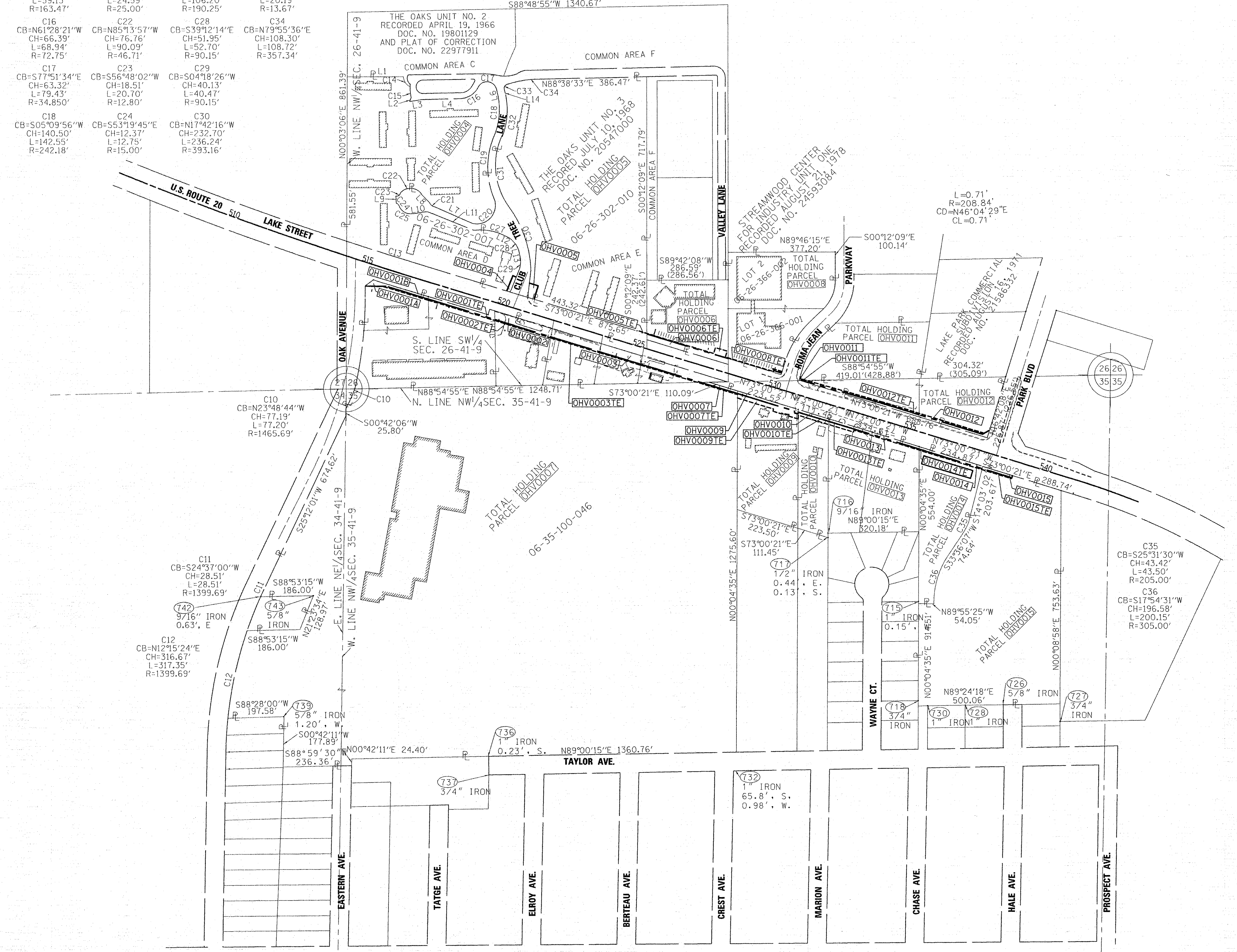
L1 = S89°55'34"E 197.80'	L2 = S22°26'58"W 14.44'	L3 = N81°23'44"E 44.68'	L4 = N88°37'12"E 133.81'	L5 = N30°24'21"E 15.94'	L6 = S12°33'56"E 22.80'	L7 = N66°20'47"E 144.47'	L8 = N44°15'35"E 38.23'	L9 = S01°11'11"E 25.00'	L10 = N81°07'20"E 42.21'	L11 = S68°06'39"E 176.67'	L12 = S63°10'23"E 22.79'	L13 = S13°17'45"E 29.54'	L14 = N13°24'32"E 14.79'
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LEGEND

- SECTION LINE
- QUARTER SECTION LINE
- QUARTER, QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- 129.32' (COMP) COMPUTED DIMENSION
- 129.32' (129.32') RECORDED DIMENSION
- EXISTING BUILDING

Bearings are referenced to the Illinois Coordinate System, NAD83, East Zone.

GRAPHIC SCALE
0 50,100
20,40
30,60
SCALE: 1" = 200'



- IRON PIPE OR ROD FOUND
- + CUT CROSS FOUND OR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
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DATED AT JOLIET, ILLINOIS THIS ___ DAY OF ___, 2012 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2017
LICENSE EXPIRATION DATE: 11/30/12

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS STANDARDS FOR A BOUNDARY SURVEY

SHEET ONE IS A COVER SHEET AND IS NOT RECORDED.

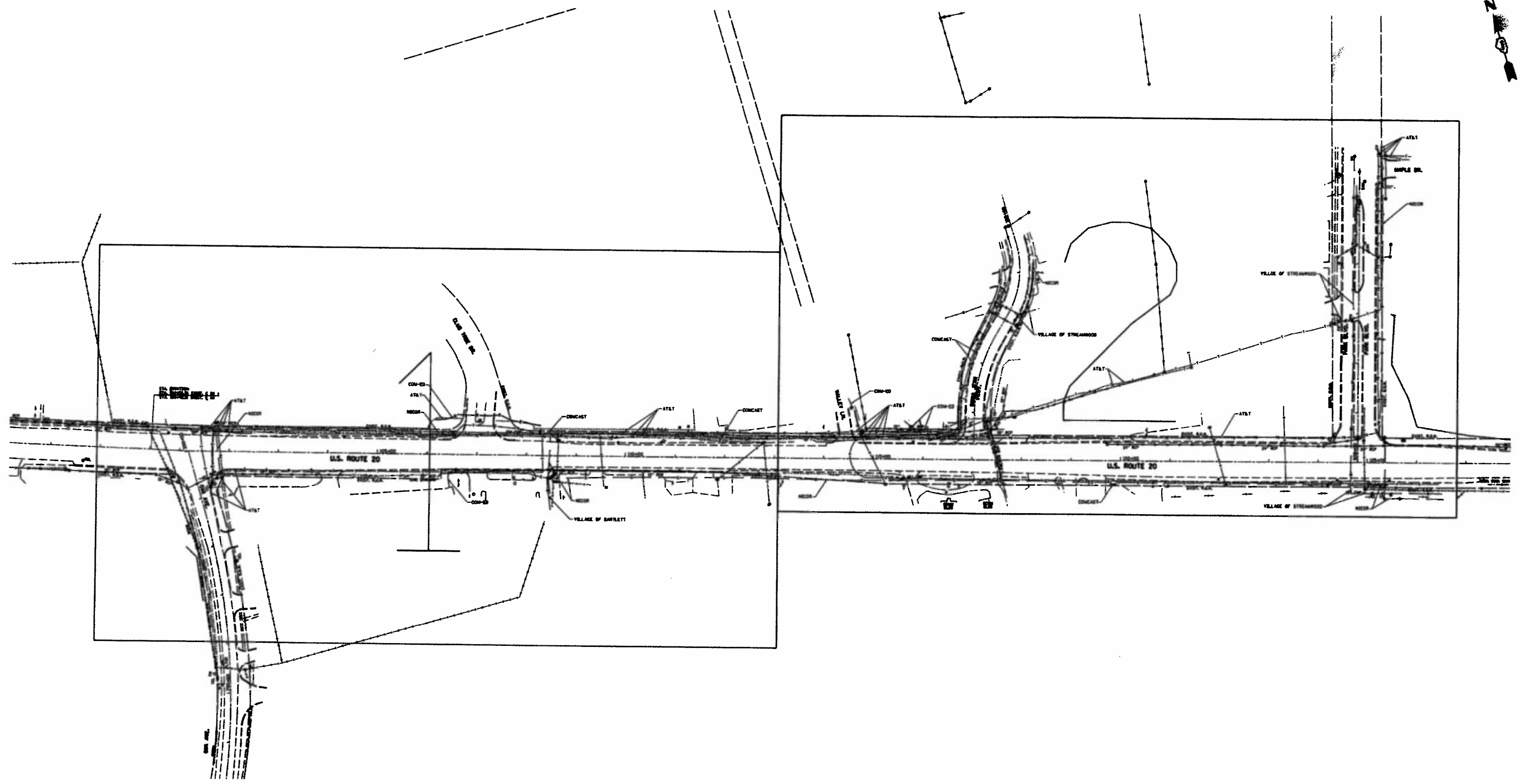
RECEIVED
SEP 14 2012
PLATS & LEGALS

PREPARED BY: **ROGINA & ASSOCIATES, L.T.D.**
ENGINEERS SURVEYORS PLANNERS
93 Caterpillar Drive · Joliet, Illinois · 815/729-0777 · FAX 815/729-0782
PROFESSIONAL DESIGN FIRM LICENSE NO. 184-001106

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
U.S. ROUTE 20 - OAK AVE. TO PARK BLVD.

SECTION: COUNTY: COOK
PROJECT: JOB NO.: R-90-019-11
STATION: TO STATION:
SCALE: 1"=200' SHEET 8 OF

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196



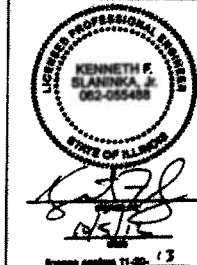
- A — A — AERIAL
 - CT — CT — CABLE TV
 - T — T — TELEPHONE
 - G — G — GAS
 - E — E — ELECTRIC
 - W — W — WATER
 - FO — FO — FIBER OPTIC
- ◆ TBE TEST HOLE

UTILITY OWNERS

AT&T = FIBER OPTIC
 AT&T = TELEPHONE
 COMCAST = CABLE TV
 COMCAST = FIBER OPTIC
 COM-ED = ELECTRIC
 NICOR = GAS
 VILLAGE OF BARTLETT = WATER
 VILLAGE OF STREAMWOOD = WATER

Utilities shown on these plans as depicted in the legend have been investigated by Cardno TBE in accordance with SUE Industry Standards. All other information shown has been provided to Cardno TBE by others. Cardno TBE's QL "B" SUE field investigation was performed 9/25/12 through 9/27/12. Changes to utilities after 9/27/12 may have been made and therefore may result in variances from this plan. Consideration should be given to updating this plan if deemed advisable prior to final design and construction.

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. 109340506
 SEP Plan From: Engr

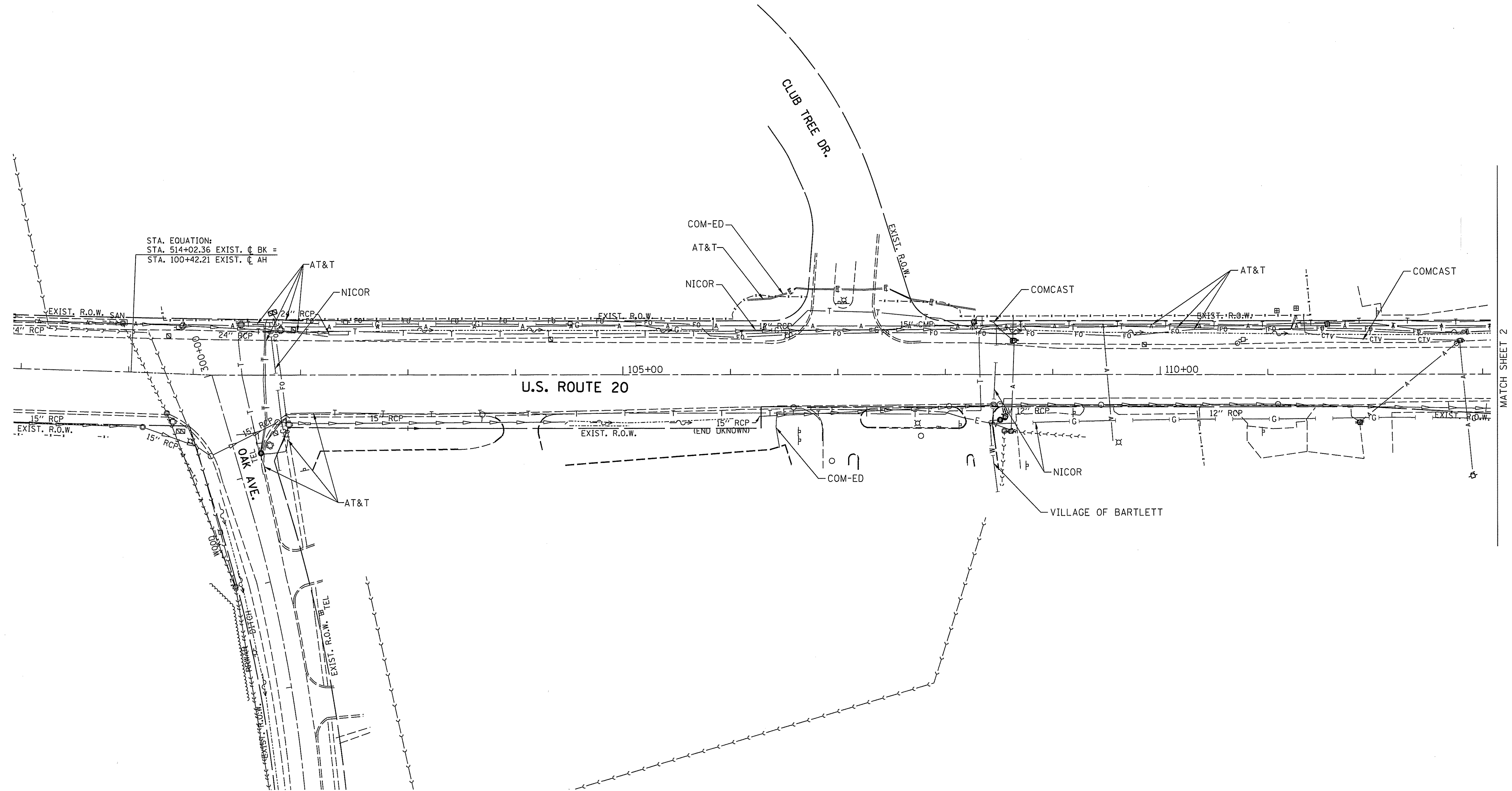
Utility Quality Level "A": Visually Verified Test Hole
 Utility Quality Level "B": Designating/non Visually Verified Test Hole
 Utility Quality Level "C": Research with Survey
 Utility Quality Level "D": Records Research

DESIGNED	LP	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	10/05/12	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

US 20 (Lake St.) from Oak Ave. to Park Ave.
 Bartlett, Illinois

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	38
			CONTRACT NO. 60N18	
[ILLINOIS] FED. AID PROJECT				



STA. EQUATION:
 STA. 514+02.36 EXIST. C BK =
 STA. 100+42.21 EXIST. C AH

- A — A — AERIAL
- — — — — UNKNOWN
- CTV — CTV — CABLE TV
- T — T — TELEPHONE
- G — G — GAS
- E — E — ELECTRIC
- W — W — WATER
- FO — FO — FIBER OPTIC

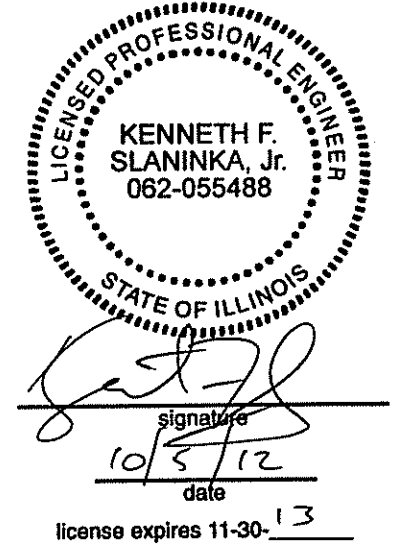
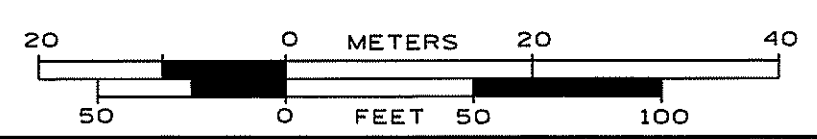
⊕ TBE TEST HOLE

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ALL UTILITIES SHOWN QUALITY LEVEL "B"
 UNLESS NOTED OTHERWISE.



TBE Job No. IL09510506
 SUE Plan Page: 1 of 2

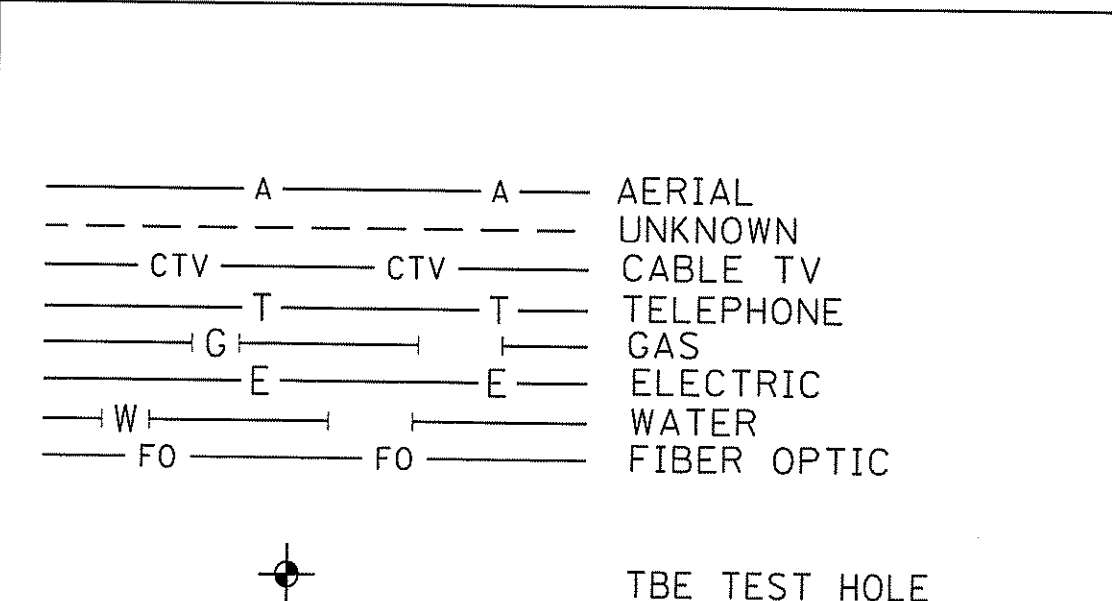
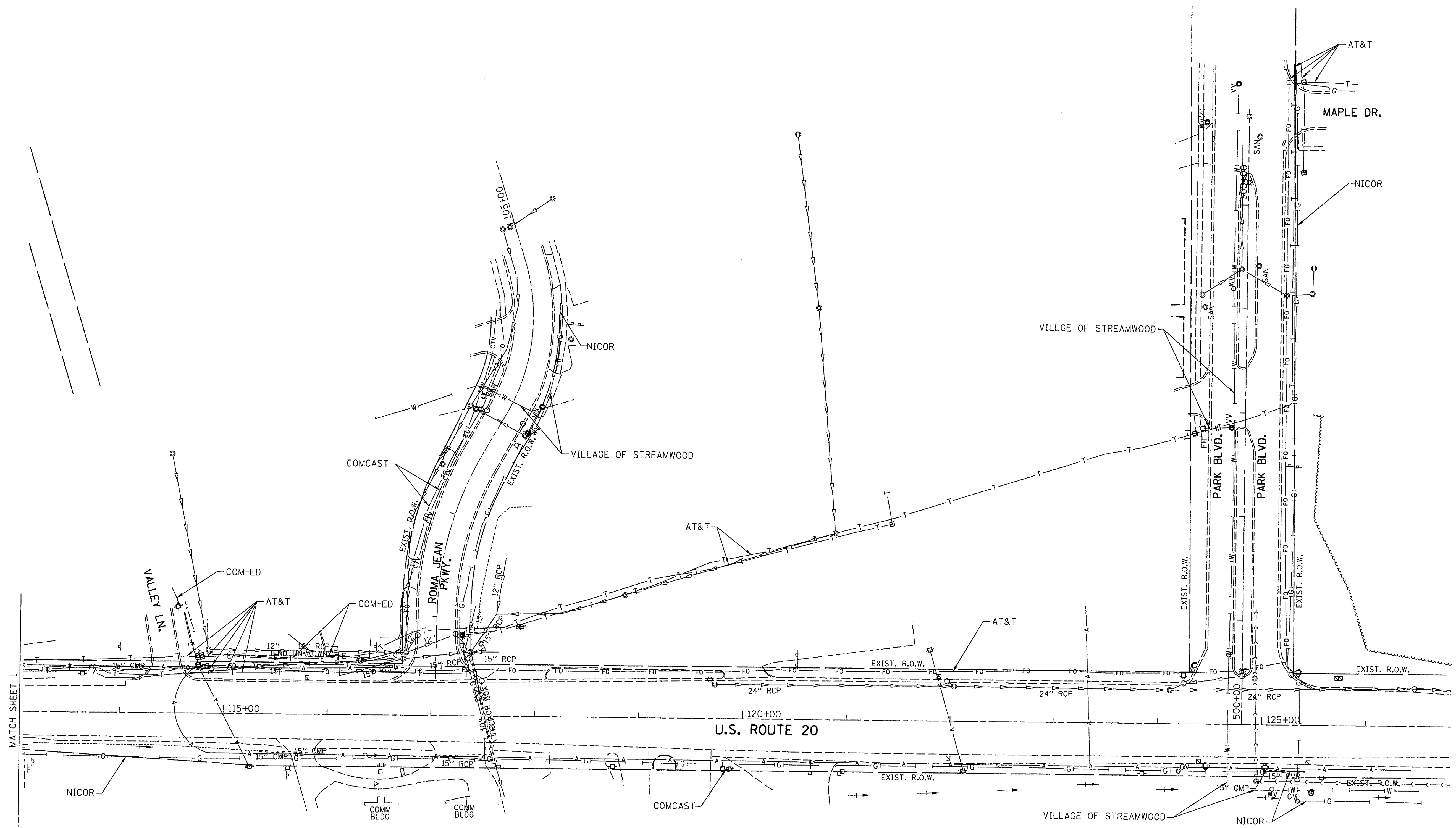
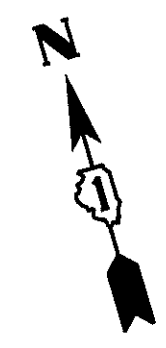
Utility Quality Level "A" : Visually Verified Test Hole
 Utility Quality Level "B" : Designating/non Visually Verified Test Hole
 Utility Quality Level "C" : Research with Survey
 Utility Quality Level "D" : Records Research

DESIGNED	LP	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	10/05/12	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**US 20 (Lake St.) from Oak Ave. to Park Ave.
 Bartlett, Illinois**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	Cook	97	39
Contract No. 60N18				
FED. ROAD DIST. NO. - ILLINOIS IDOT Project No.				

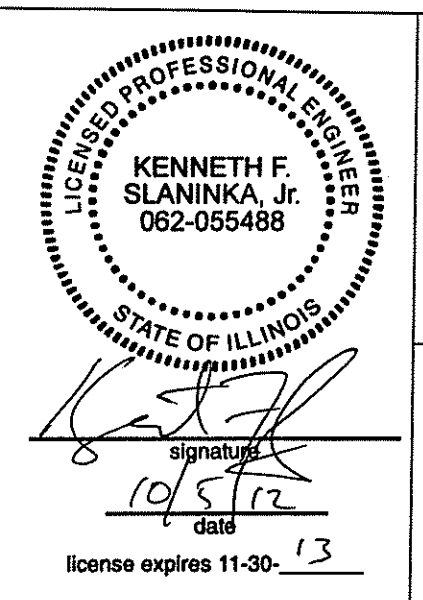


UTILITY OWNERS

AT&T = FIBER OPTIC
 AT&T = TELEPHONE
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ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



Cardno TBE
 CIVIL ENGINEERING * TRANSPORTATION * ENVIRONMENTAL * PLANNING * UTILITY ENGINEERING/LOCATING

Dynasty Group
 Engineers & Surveyors

TBE Job No. IL09510506
 SUE Plan Page: 2 of 2



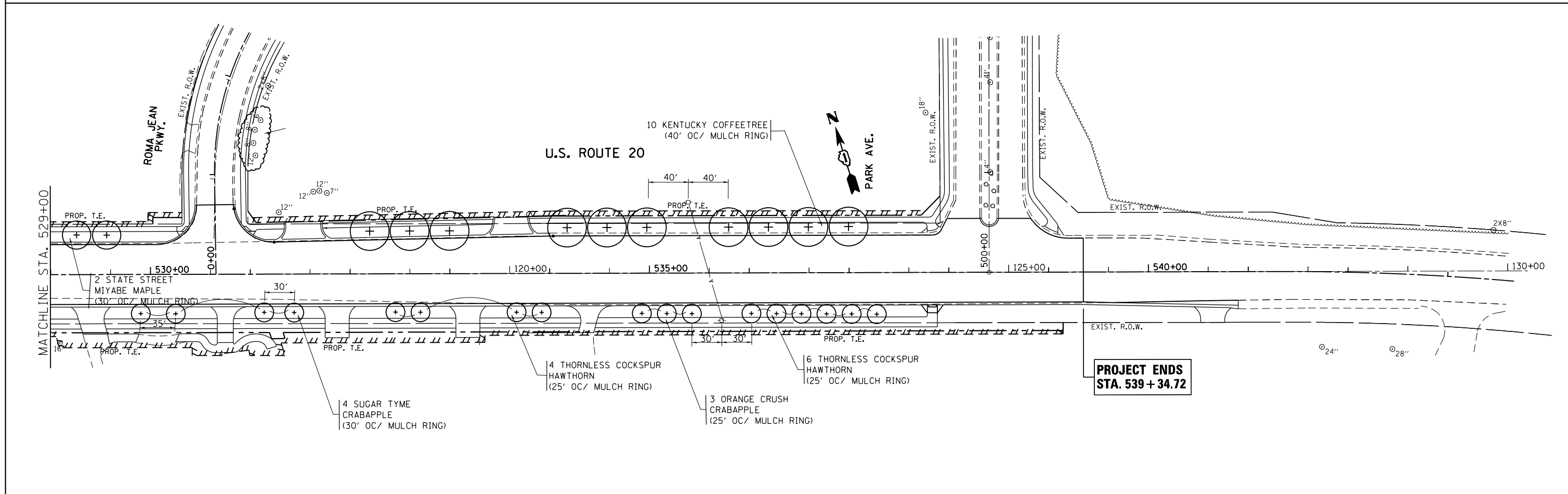
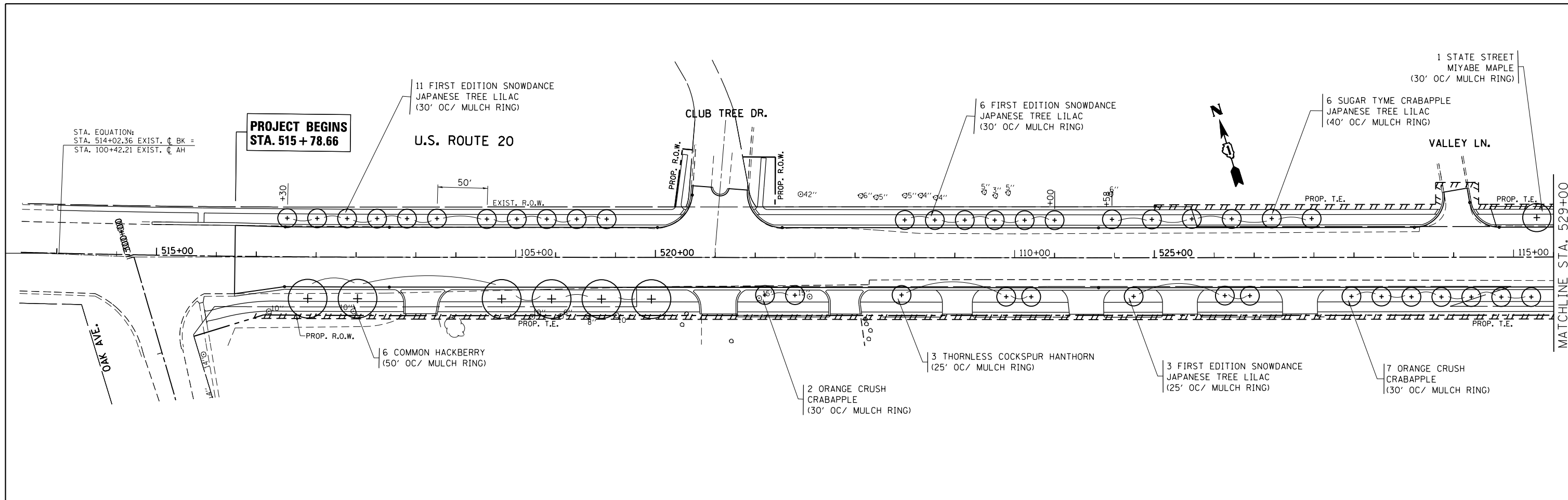
Utility Quality Level "A": Visually Verified Test Hole
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 Utility Quality Level "C": Research with Survey
 Utility Quality Level "D": Records Research

DESIGNED LP	REVISED
DRAWN SRK	REVISED
CHECKED KFS	REVISED
DATE 10/05/12	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

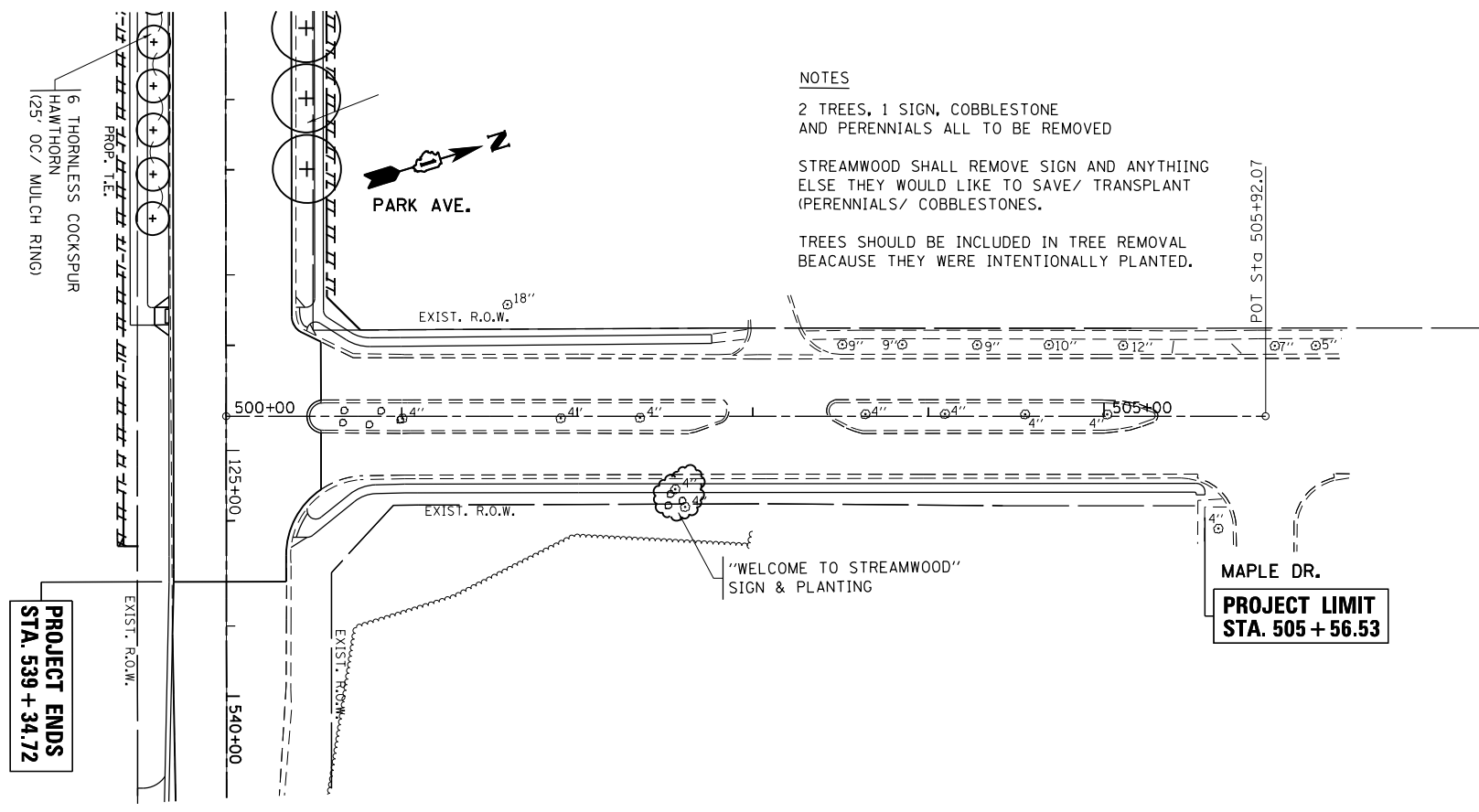
US 20 (Lake St.) from Oak Ave. to Park Ave.
Bartlett, Illinois

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	Cook	97	40
Contract No. 60N18			FED. ROAD DIST. NO. - ILLINOIS IDOT Project No.	

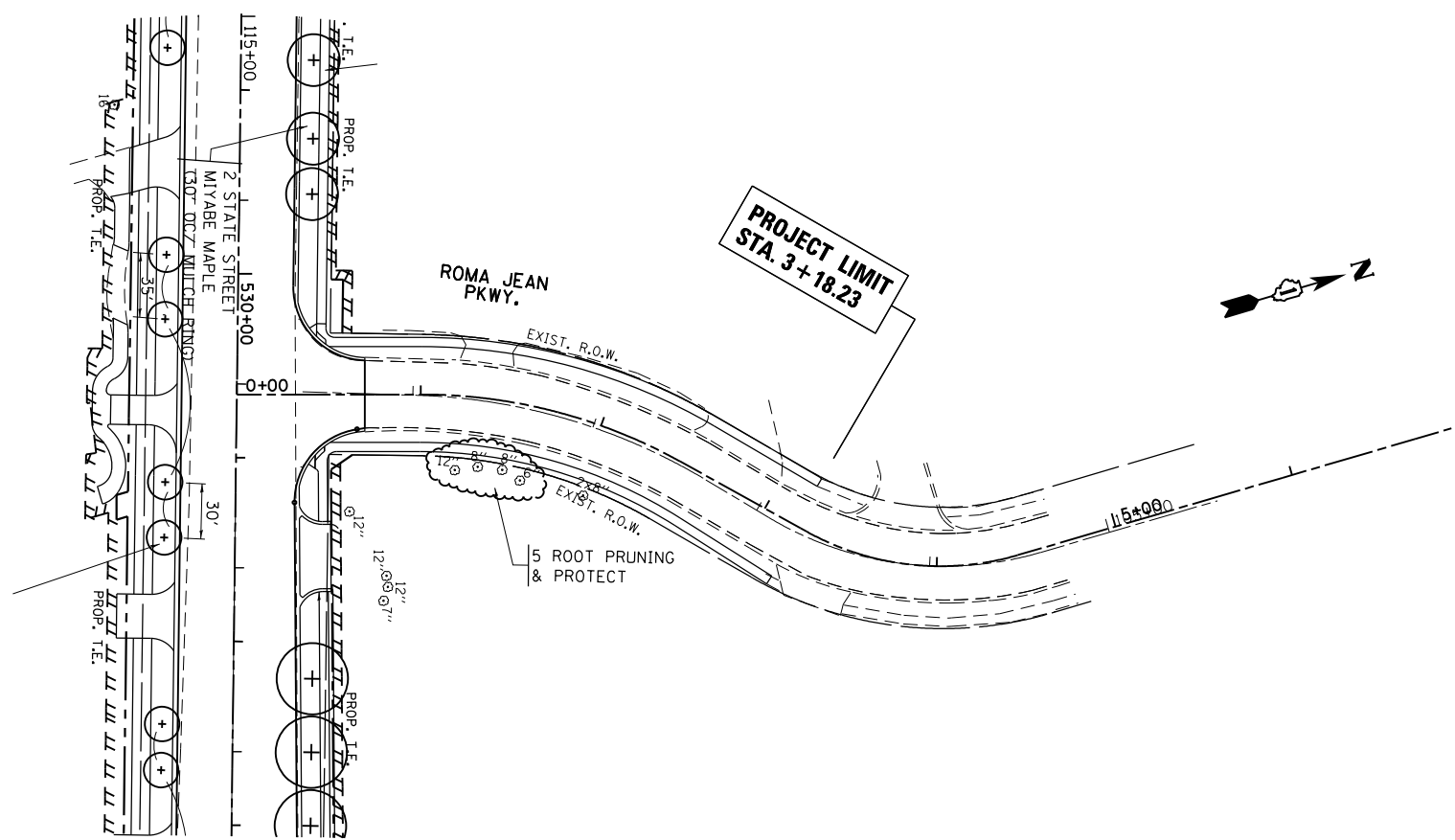


FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED LANDSCAPING PLAN U.S. ROUTE 20 (OAK AVE. - PARK AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pwwork\pwwork\galbenjr\d0304019\P157408-sht-landscap.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -			345	7Y-WRS	COOK	97	41	
MODELNAME	PLOT DATE = 4/3/2013	CHECKED -	REVISED -			CONTRACT NO. 60N18					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

SCALE: 1"=50' SHEET OF SHEETS STA. 515+78.66 TO STA. 539+34.72



NOTES
 2 TREES, 1 SIGN, COBBLESTONE
 AND PERENNIALS ALL TO BE REMOVED
 STREAMWOOD SHALL REMOVE SIGN AND ANYTHING
 ELSE THEY WOULD LIKE TO SAVE/ TRANSPLANT
 (PERENNIALS/ COBBLESTONES.
 TREES SHOULD BE INCLUDED IN TREE REMOVAL
 BECAUSE THEY WERE INTENTIONALLY PLANTED.

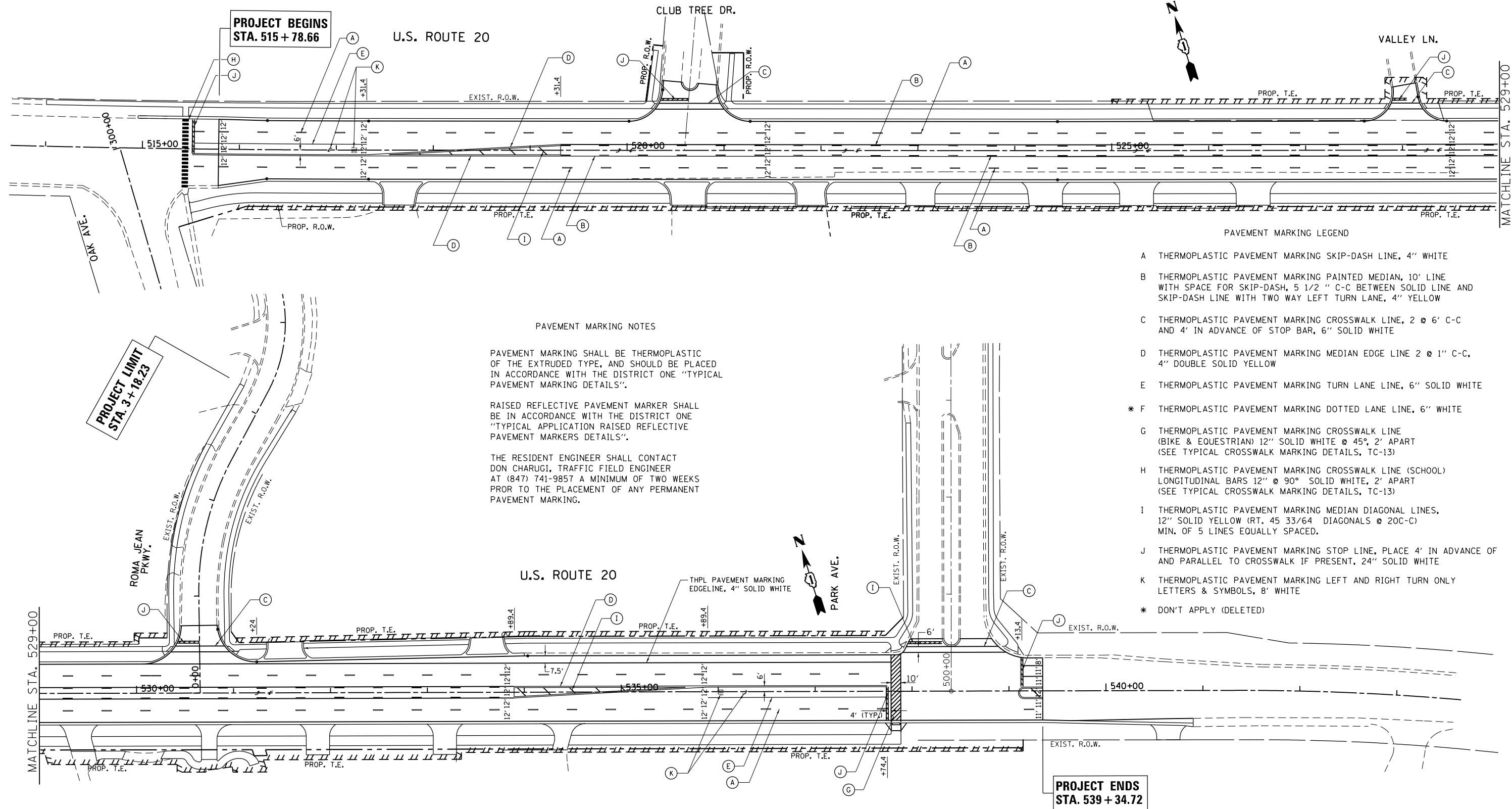


FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -
c:\pwork\work\pwork\galbenjr\d0304019\P157408-sht-landscp.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
MODELNAME	PLOT DATE = 4/3/2013	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PROPOSED LANDSCAPING PLAN
 U.S. ROUTE 20 (OAK AVE. - PARK AVE.)
 SCALE: 1"=50'
 SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	42
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING NOTES

PAVEMENT MARKING SHALL BE THERMOPLASTIC OF THE EXTRUDED TYPE, AND SHOULD BE PLACED IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL PAVEMENT MARKING DETAILS".

RAISED REFLECTIVE PAVEMENT MARKER SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS DETAILS".

THE RESIDENT ENGINEER SHALL CONTACT DON CHARUGI, TRAFFIC FIELD ENGINEER AT (847) 741-9857 A MINIMUM OF TWO WEEKS PRIOR TO THE PLACEMENT OF ANY PERMANENT PAVEMENT MARKING.

PAVEMENT MARKING LEGEND

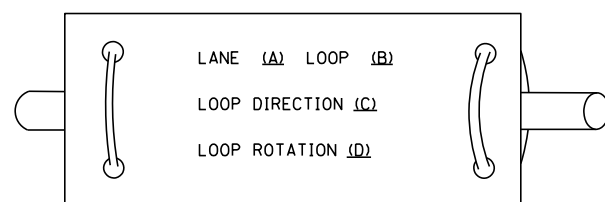
- A THERMOPLASTIC PAVEMENT MARKING SKIP-DASH LINE, 4" WHITE
- B THERMOPLASTIC PAVEMENT MARKING PAINTED MEDIAN, 10' LINE WITH SPACE FOR SKIP-DASH, 5 1/2" C-C BETWEEN SOLID LINE AND SKIP-DASH LINE WITH TWO WAY LEFT TURN LANE, 4" YELLOW
- C THERMOPLASTIC PAVEMENT MARKING CROSSWALK LINE, 2 @ 6' C-C AND 4' IN ADVANCE OF STOP BAR, 6" SOLID WHITE
- D THERMOPLASTIC PAVEMENT MARKING MEDIAN EDGE LINE 2 @ 1' C-C, 4" DOUBLE SOLID YELLOW
- E THERMOPLASTIC PAVEMENT MARKING TURN LANE LINE, 6" SOLID WHITE
- * F THERMOPLASTIC PAVEMENT MARKING DOTTED LANE LINE, 6" WHITE
- G THERMOPLASTIC PAVEMENT MARKING CROSSWALK LINE (BIKE & EQUESTRIAN) 12" SOLID WHITE @ 45°, 2' APART (SEE TYPICAL CROSSWALK MARKING DETAILS, TC-13)
- H THERMOPLASTIC PAVEMENT MARKING CROSSWALK LINE (SCHOOL) LONGITUDINAL BARS 12" @ 90° SOLID WHITE, 2' APART (SEE TYPICAL CROSSWALK MARKING DETAILS, TC-13)
- I THERMOPLASTIC PAVEMENT MARKING MEDIAN DIAGONAL LINES, 12" SOLID YELLOW (RT. 45 33/64 DIAGONALS @ 20C-C) MIN. OF 5 LINES EQUALLY SPACED.
- J THERMOPLASTIC PAVEMENT MARKING STOP LINE, PLACE 4' IN ADVANCE OF AND PARALLEL TO CROSSWALK IF PRESENT, 24" SOLID WHITE
- K THERMOPLASTIC PAVEMENT MARKING LEFT AND RIGHT TURN ONLY LETTERS & SYMBOLS, 8" WHITE
- * DON'T APPLY (DELETED)

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING PLAN U.S. ROUTE 20 (OAK AVE. - PARK AVE.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ei:\pwork\pwork\pwork\galbenjr\d0304019\P157408-sht.pmk.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -			345	7Y-WRS	COOK	97	43	
MODELNAME	PLOT DATE = 4/3/2013	CHECKED -	REVISED -			CONTRACT NO. 60N18					
		DATE -	REVISED -			SCALE: 1"=50'	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		

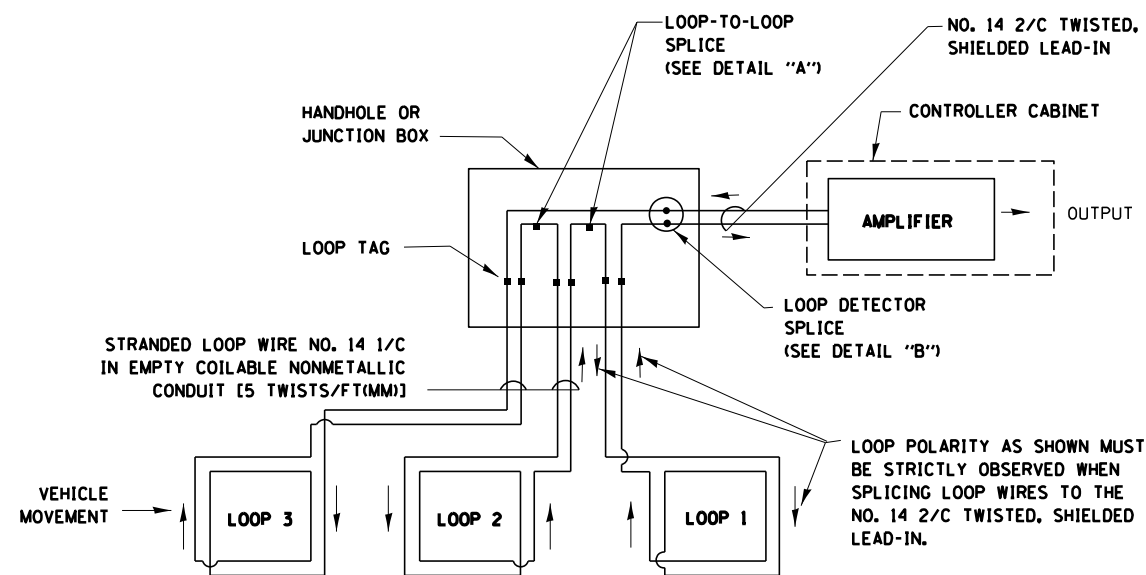
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

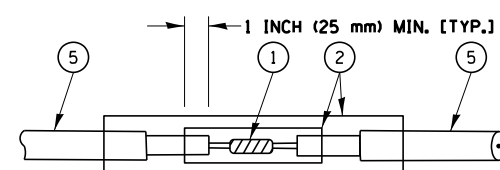


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

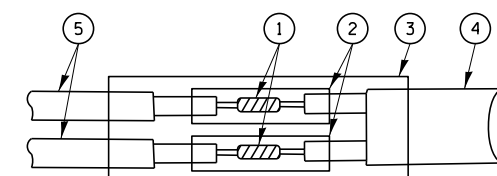


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

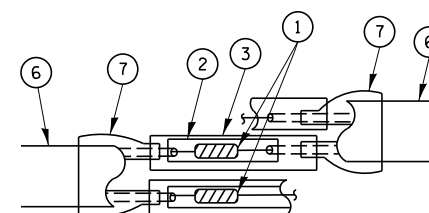


DETAIL "A"
LOOP-TO-LOOP SPLICE

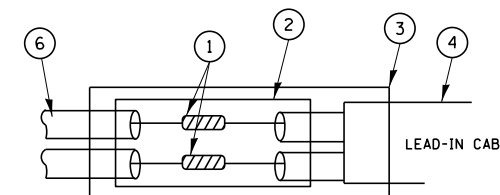


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = pociechal	DESIGNED - DAD	REVISED - DAG 11-1-12
et:\pw_work\p1dot\pociechal\0318457\PI57408-TS.dgn		DRAWN - BCK	REVISED -
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	PLOT DATE = 1/3/2013	DATE - 10-28-09	REVISED -

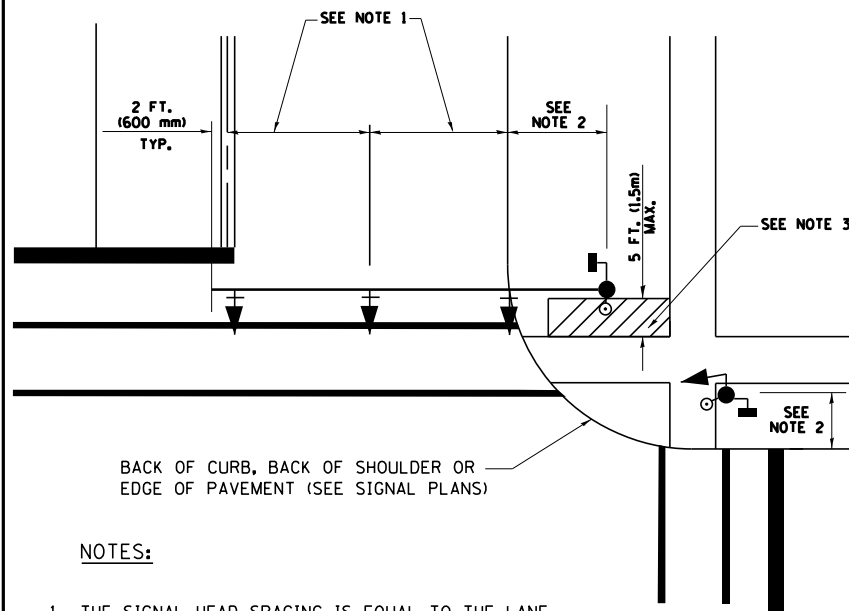
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 1 OF 6 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	44
TS-05			CONTRACT NO. 60N18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

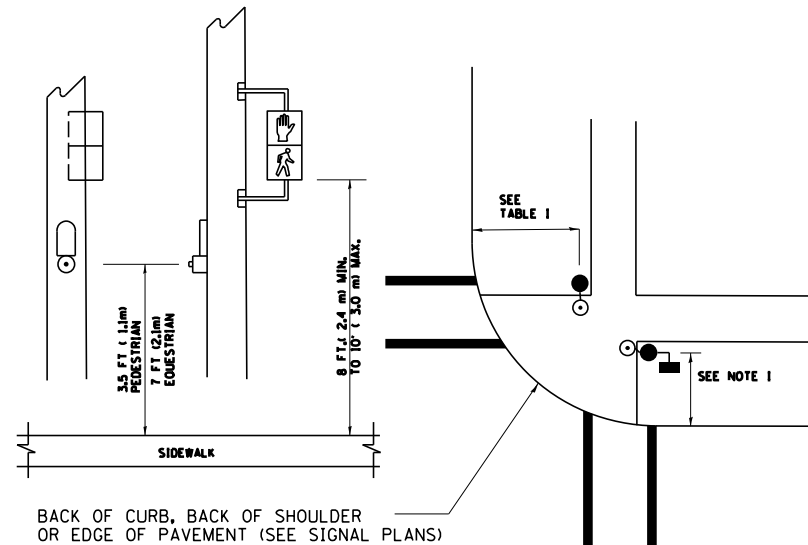
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

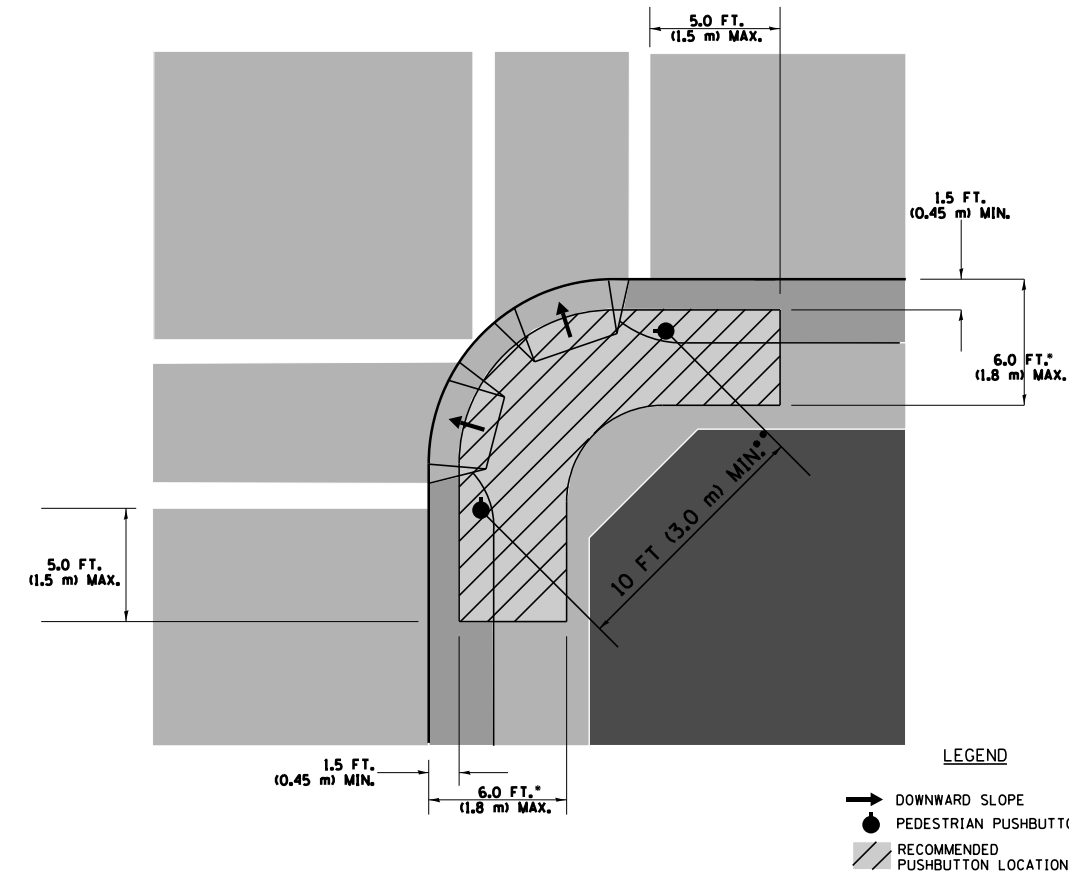
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

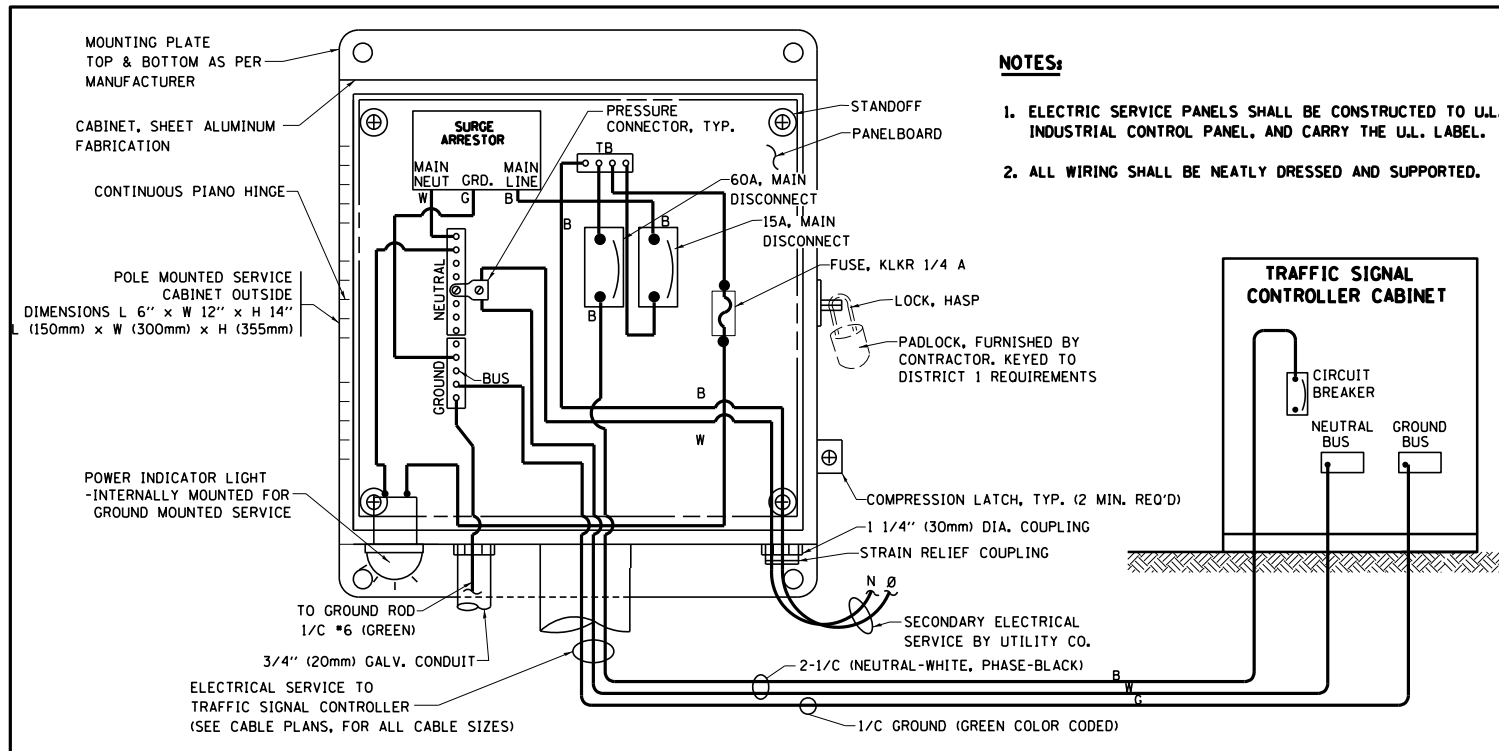
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

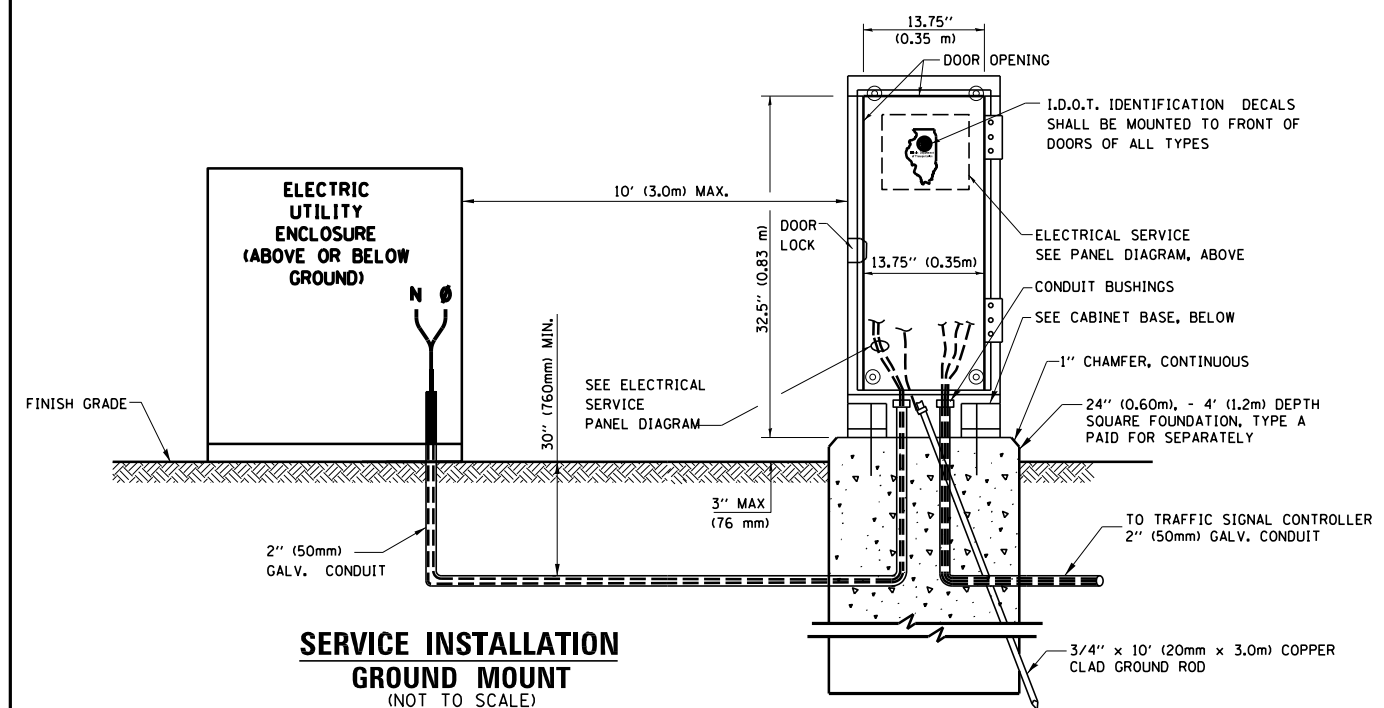
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

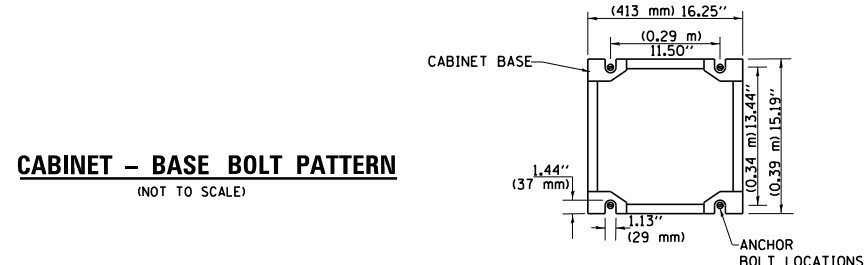
1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
 (NOT TO SCALE)

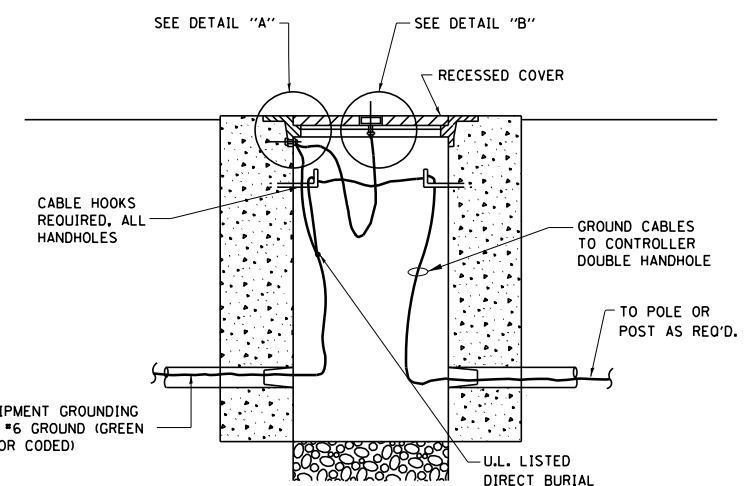
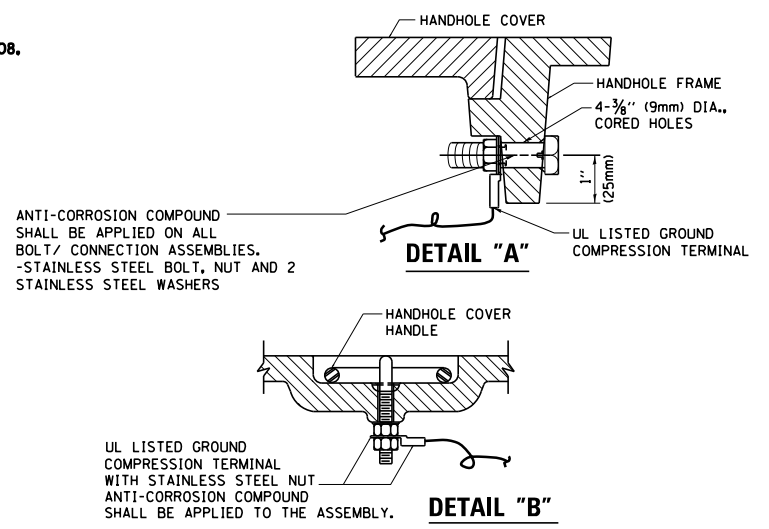
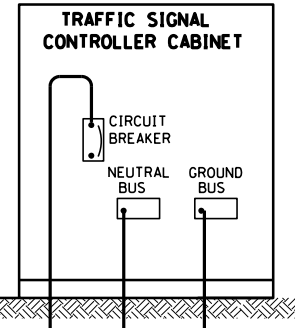


SERVICE INSTALLATION GROUND MOUNT
 (NOT TO SCALE)

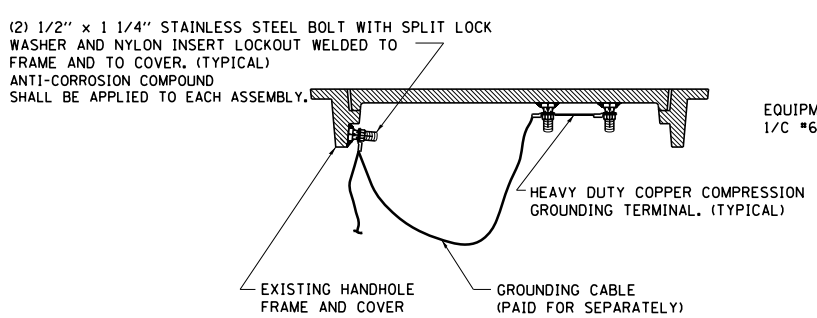


CABINET - BASE BOLT PATTERN
 (NOT TO SCALE)

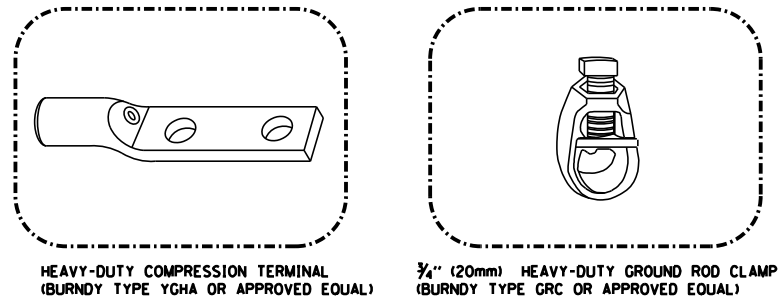
- NOTES:**
1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
 2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



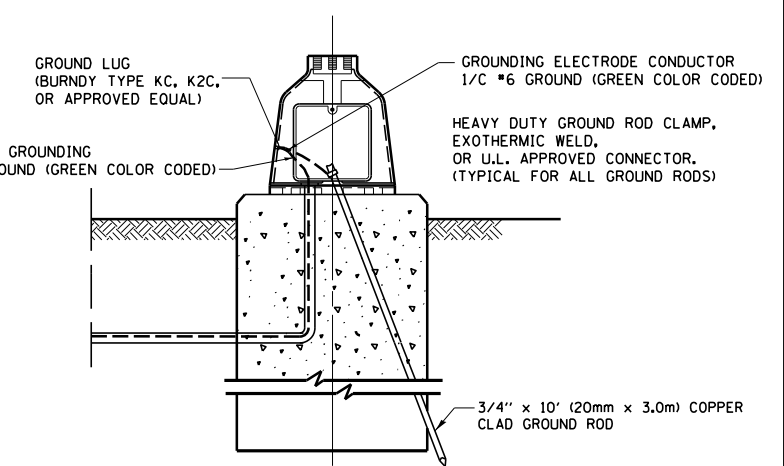
HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL
 (NOT TO SCALE)

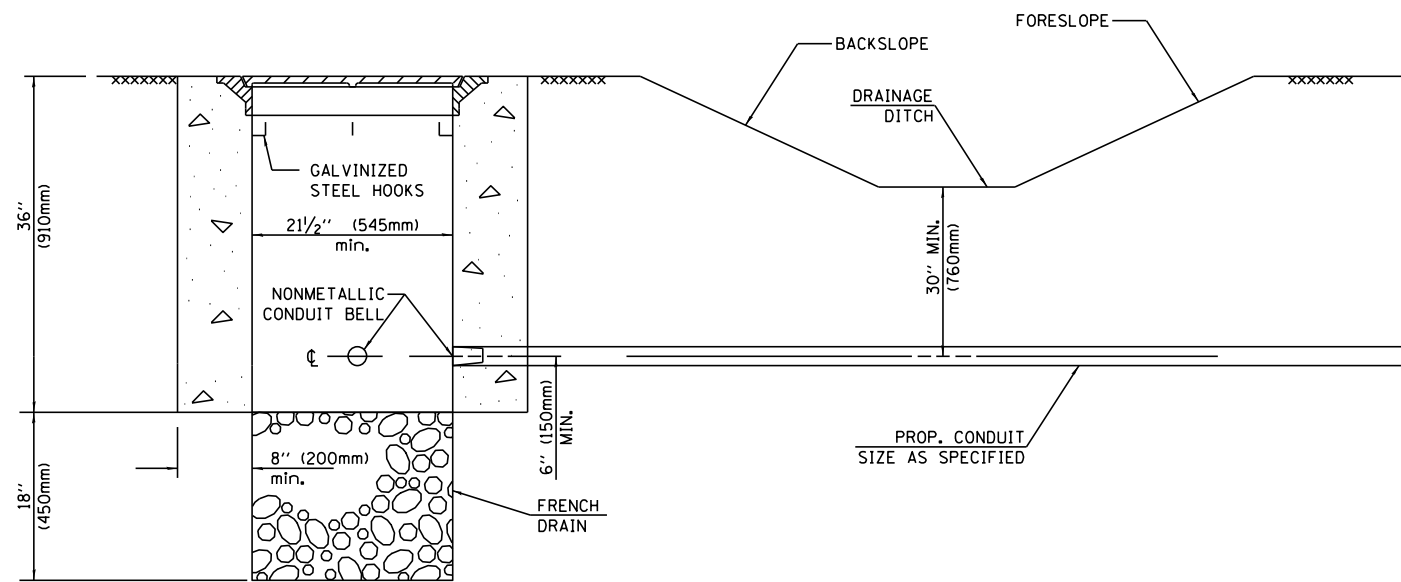


- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

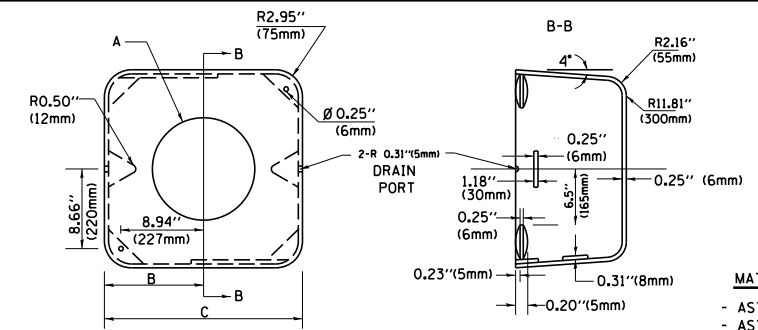


MAST ARM POLE / POST-GROUNDING DETAIL
 (NOT TO SCALE)

FILE NAME =	USER NAME = pociechal	DESIGNED - DAD	REVISED - DAG 11-1-12	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS			F.A.P. RTE. 345	SECTION 7Y-WRS	COUNTY COOK	TOTAL SHEETS 97	SHEET NO. 46
et:\pwork\pwork\pociechal\0318457\157408-TS.dgn	PLOT SCALE = 100.0000' / 1"	DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 3 OF 6 SHEETS	STA. TO STA.	TS-05		CONTRACT NO. 60N18		
PLOT DATE = 1/3/2013	DATE - 10-28-09	CHECKED - DAD	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE - 10-28-09	REVISED -									



HANDHOLE WITH MINIMUM CONDUIT DEPTH

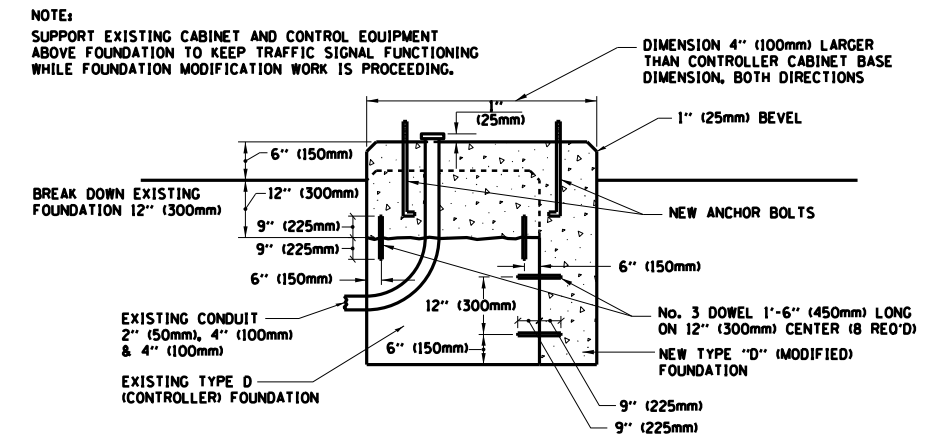


	A	B	C	HEIGHT	WEIGHT
	VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
	VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
	VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
	VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

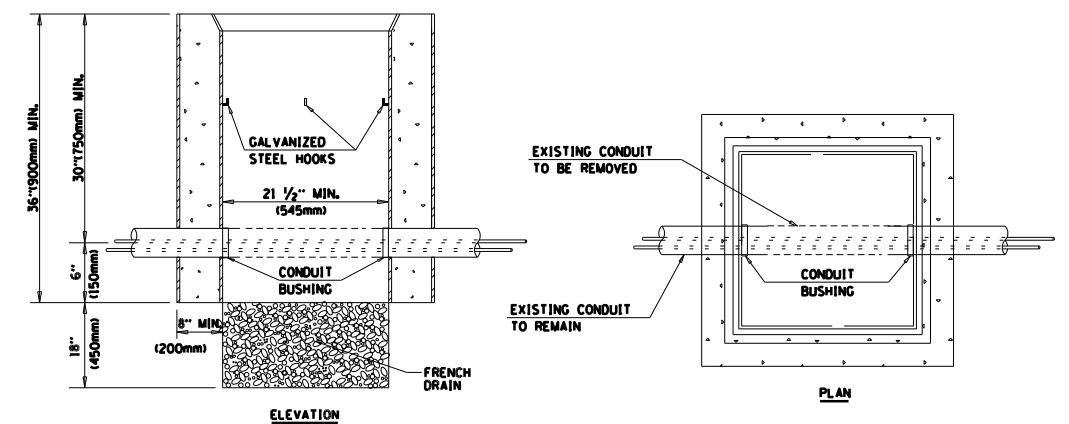
SHROUD

NOTES:

- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



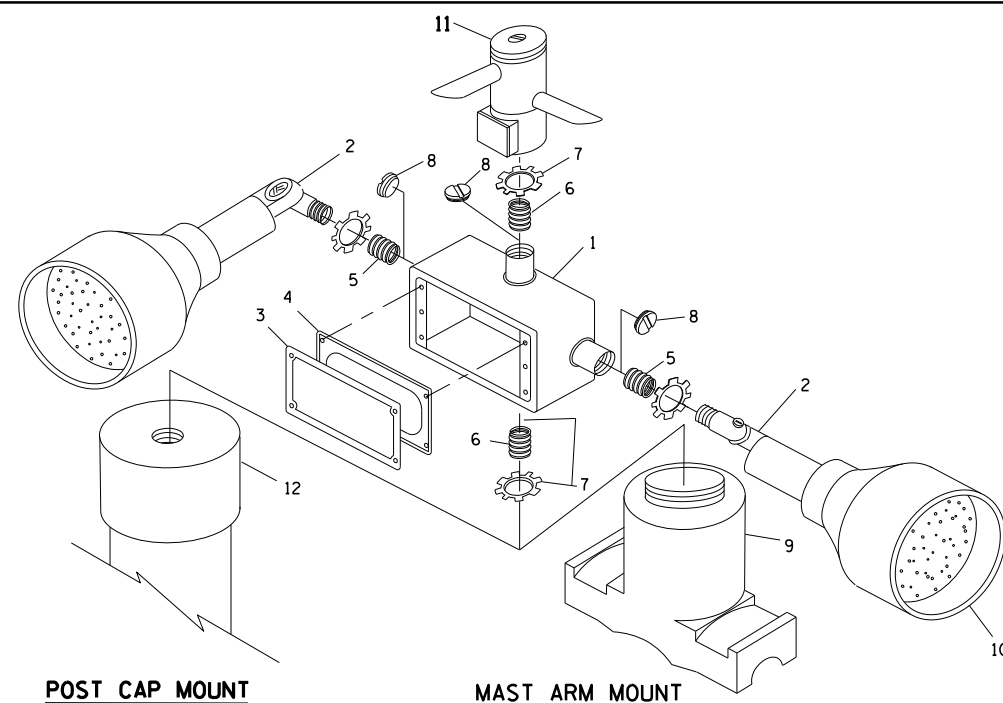
MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-0-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.

POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

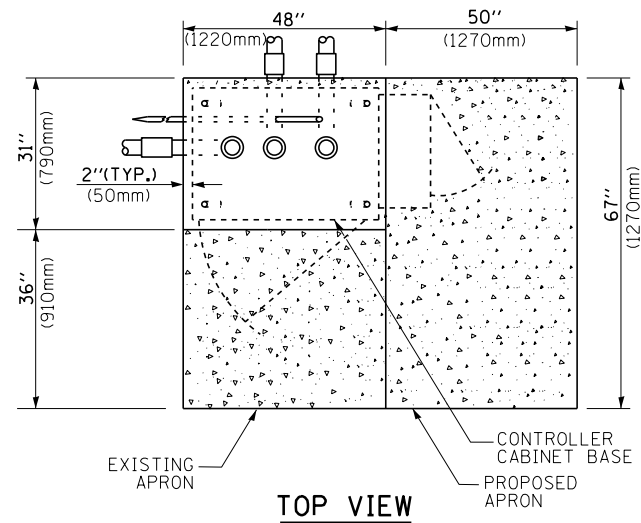
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et:\pw_work\p1dot\poc1echn\d0318457\PI57408-TS.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

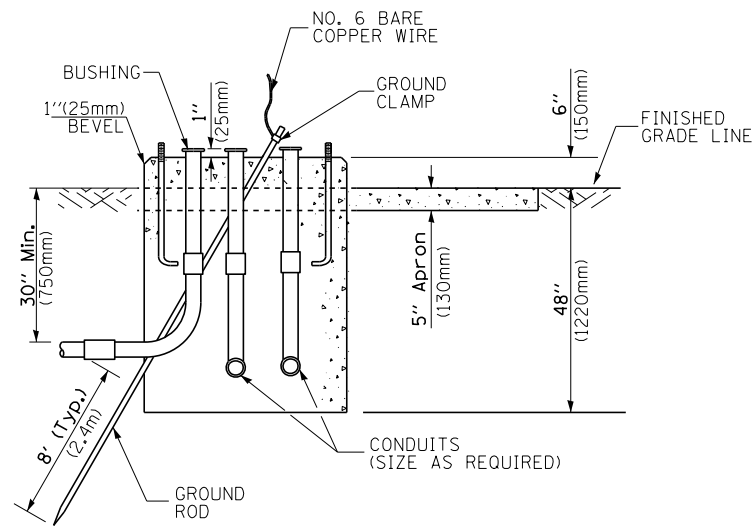
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 4 OF 6 SHEETS STA. TO STA.

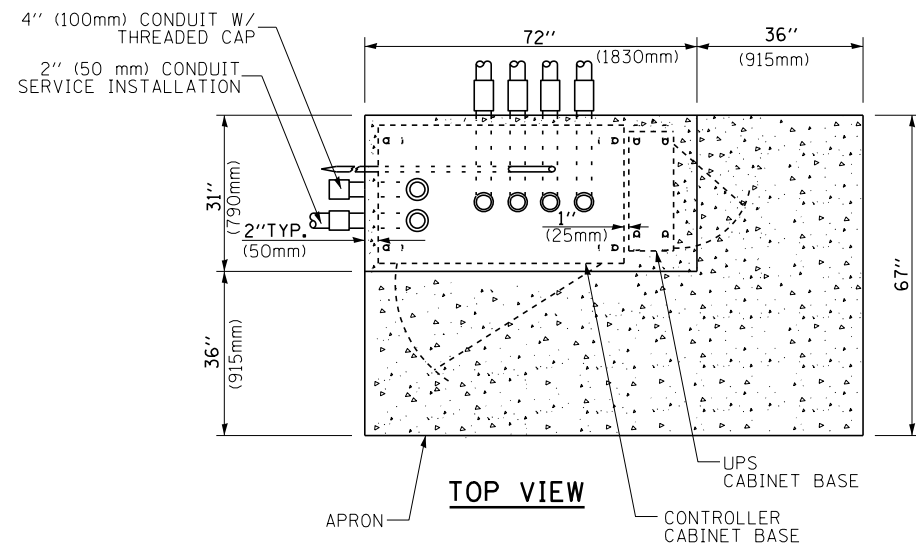
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	47
TS-05		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



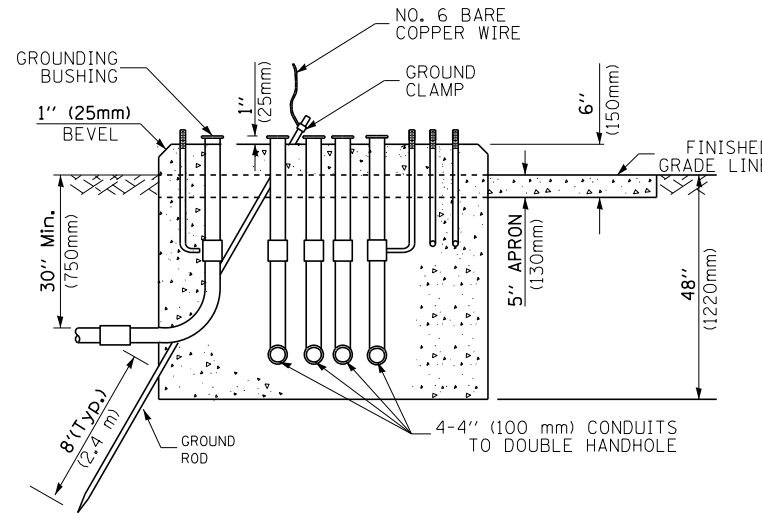
TOP VIEW



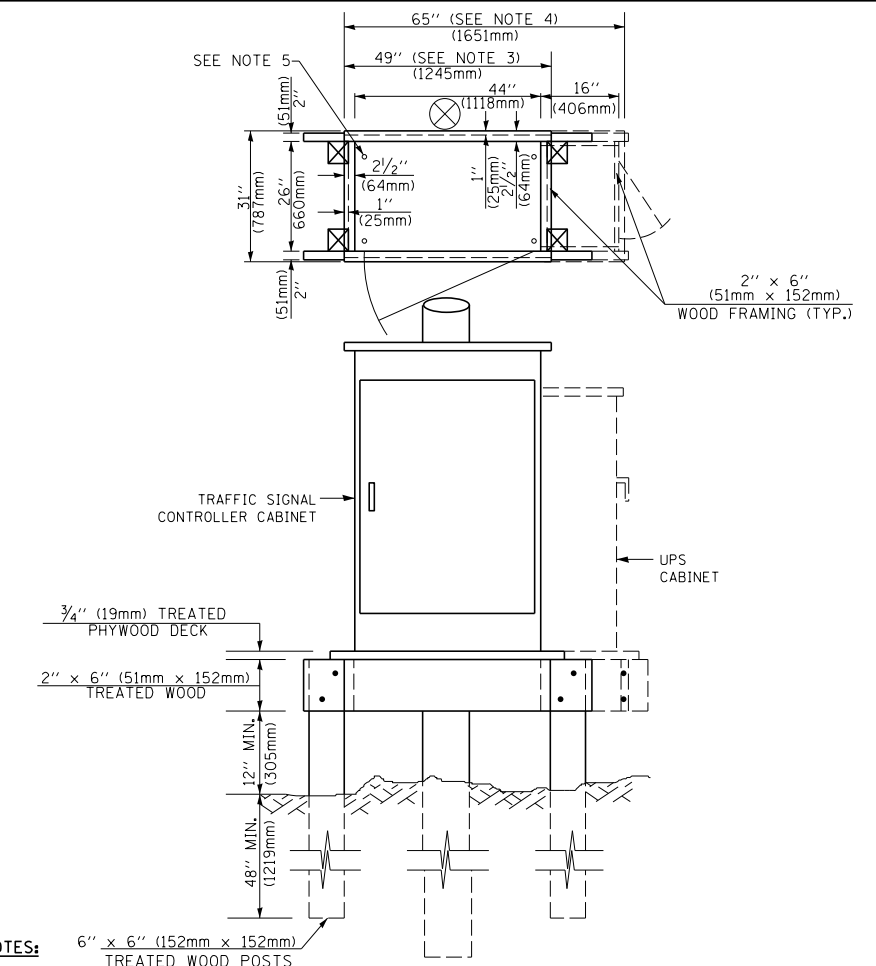
**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



TOP VIEW



**TYPE C
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**



- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
 3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
 4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
 5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
 6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

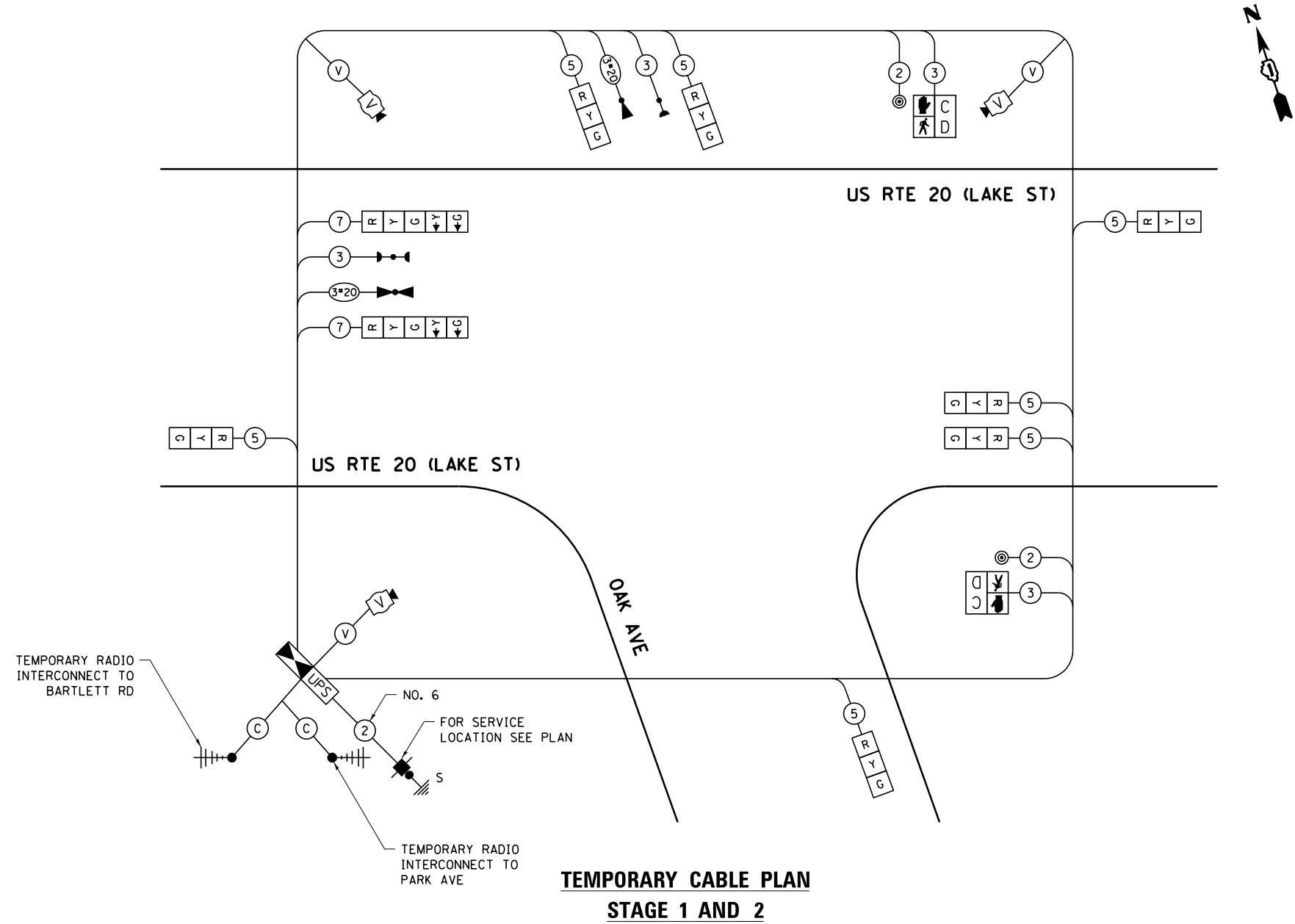
Mast Arm Length	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (Q_u) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
 2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
 3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
 4. For mast arm assemblies with dual arms refer to state standard 878001.

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED																		
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE																					
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE																					
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA																					
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED																					
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F																					
UNINTERRUPTABLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F																					
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																					
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F																					
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE																					
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED																					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM				STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED																					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM				ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED																					
SIGNAL POST				REMOVE ITEM				STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED																					
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM				SIGNAL POST AND FOUNDATION TO BE REMOVED																					
GUY WIRE				ABANDON ITEM				INTERSECTION & SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR																					
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED SAMPLING (SYSTEM) DETECTOR																					
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				<h2 style="margin: 0;">RAILROAD SYMBOLS</h2> <table style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 50%;"></th> <th style="width: 25%; text-align: center;">EXISTING</th> <th style="width: 25%; text-align: center;">PROPOSED</th> </tr> </thead> <tbody> <tr> <td>RAILROAD CONTROL CABINET</td> <td></td> <td></td> </tr> <tr> <td>RAILROAD CANTILEVER MAST ARM</td> <td></td> <td></td> </tr> <tr> <td>FLASHING SIGNAL</td> <td></td> <td></td> </tr> <tr> <td>CROSSING GATE</td> <td></td> <td></td> </tr> <tr> <td>CROSSBUCK</td> <td></td> <td></td> </tr> </tbody> </table>					EXISTING	PROPOSED	RAILROAD CONTROL CABINET			RAILROAD CANTILEVER MAST ARM			FLASHING SIGNAL			CROSSING GATE			CROSSBUCK		
	EXISTING	PROPOSED																											
RAILROAD CONTROL CABINET																													
RAILROAD CANTILEVER MAST ARM																													
FLASHING SIGNAL																													
CROSSING GATE																													
CROSSBUCK																													
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED																									
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID																									
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER																									
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT																									
DETECTOR LOOP, TYPE I				RADIO REPEATER																									
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED																									
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)																									
VIDEO DETECTION CAMERA																													
VIDEO DETECTION ZONE																													
PAN, TILT, ZOOM CAMERA																													
WIRELESS DETECTOR SENSOR																													
WIRELESS ACCESS POINT																													



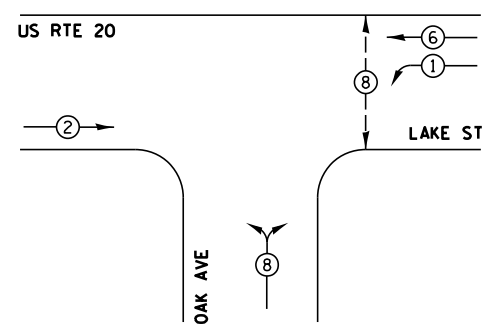
NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**TEMPORARY CABLE PLAN
STAGE 1 AND 2**

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	9		17	0.50	76.50
(YELLOW)	9		25	0.25	56.25
(GREEN)	9		15	0.25	33.75
ARROW	4		12	0.10	4.80
PED. SIGNAL	2		25	1.00	50.00
CONTROLLER	1		100	1.00	100.00
ILLUM. SIGN	-		25	0.05	-
VIDEO SYSTEM	1		150	1.00	150.00
FLASHER				0.50	
ENERGY COSTS TO:					TOTAL = 471.30

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630) 424-5704
COMPANY: COMED

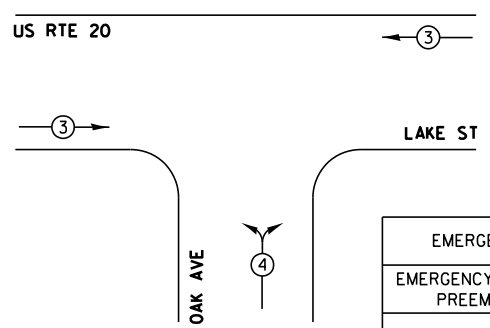
TEMPORARY CONTROLLER SEQUENCE



TEMPORARY PHASE DESIGNATION DIAGRAM

- LEGEND**
- ⊙ DUAL ENTRY PHASE
 - ⊠ SINGLE ENTRY PHASE
 - ⊠ OL OVERLAP
 - ⊙ PEDESTRAIN PHASE
 - * NUMBER REFERS TO ASSOCIATED PHASE

**TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE**



EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←→	↕

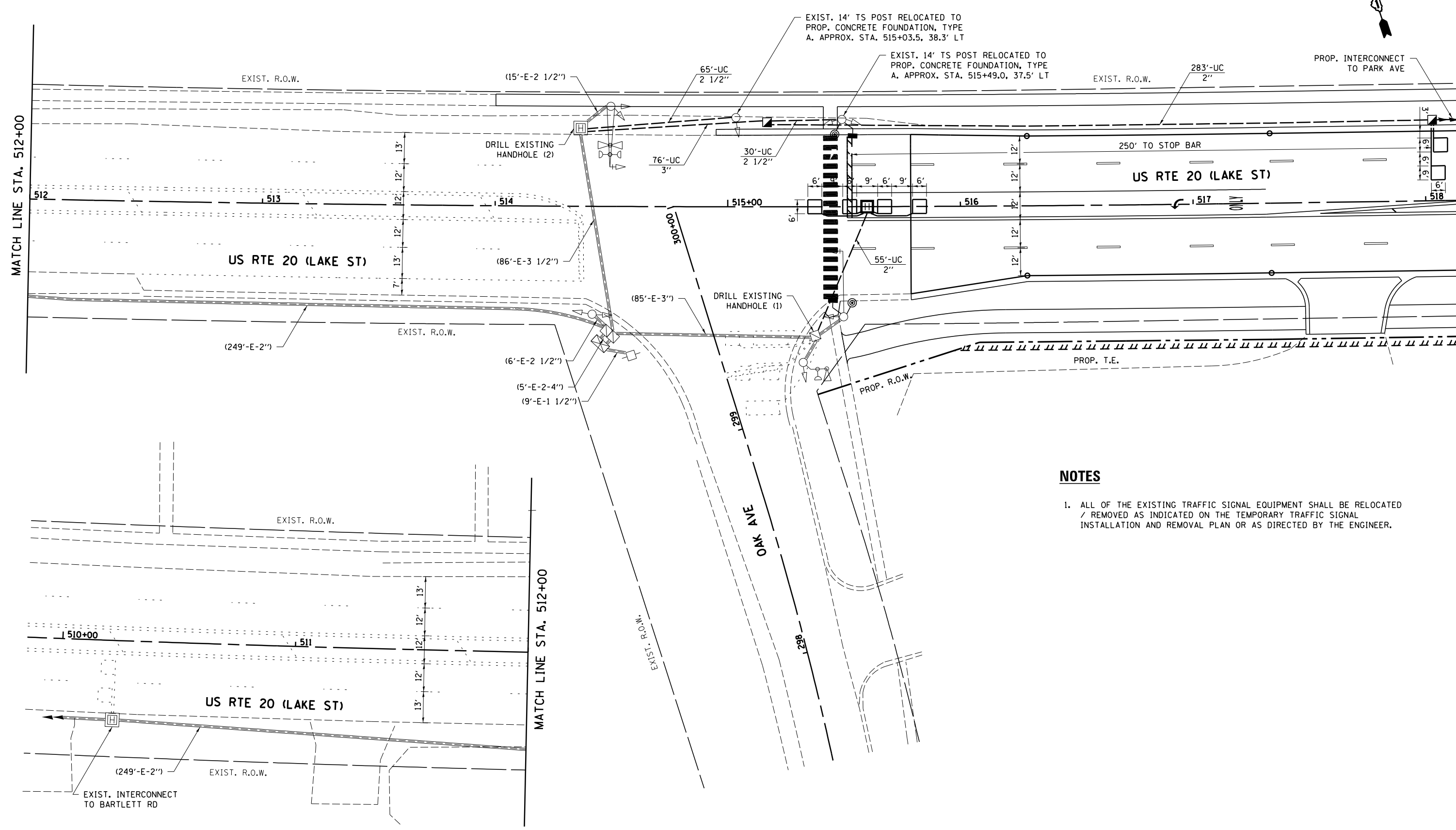
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	PLOT DATE = 1/3/2013	DATE - 01/03/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE (STAGE 1 AND 2)**
US RTE 20 (LAKE ST) AT OAK AVE
SCALE: NTS SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	51
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

TS#1325



NOTES

1. ALL OF THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RELOCATED / REMOVED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN OR AS DIRECTED BY THE ENGINEER.

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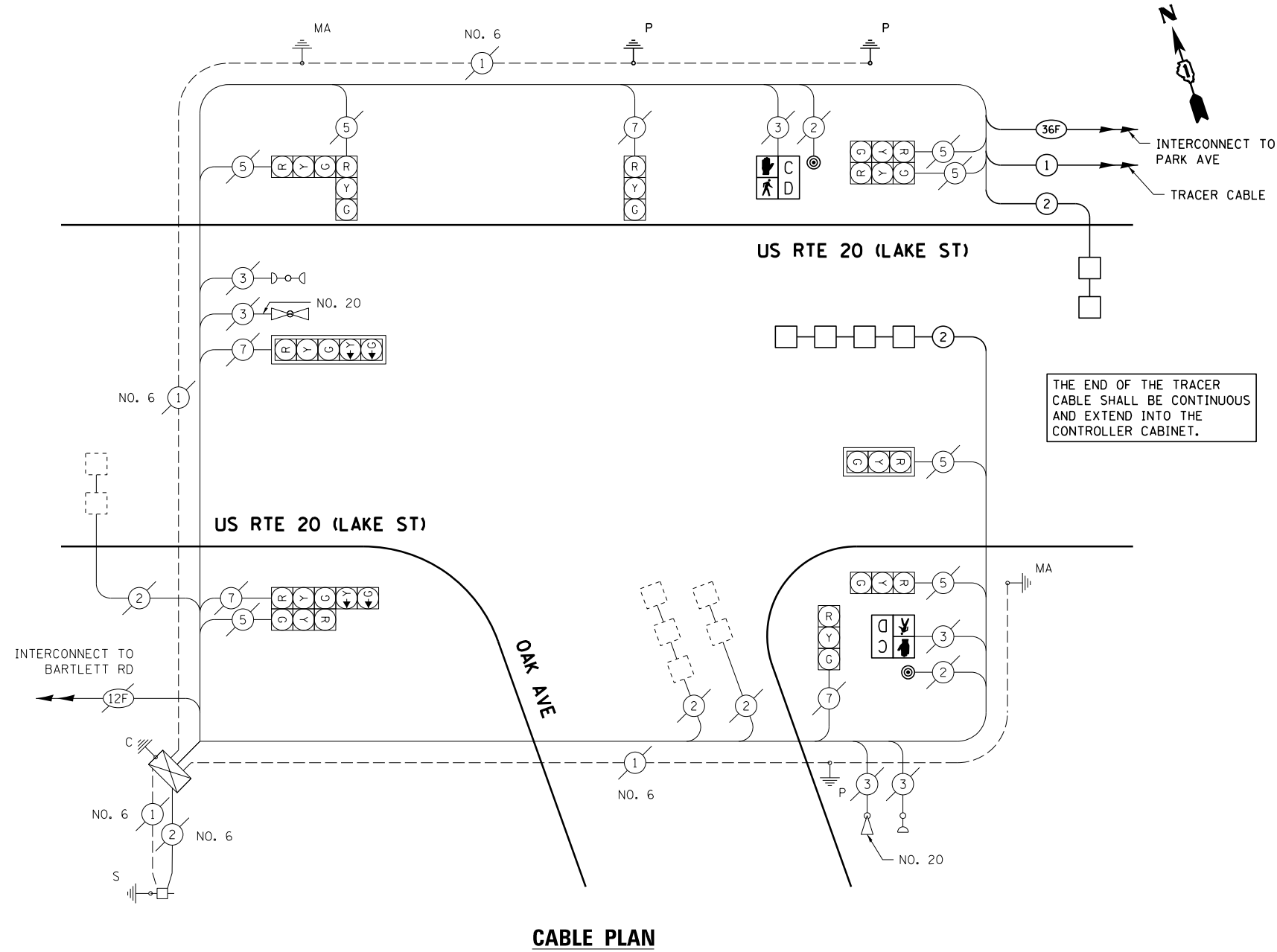
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN
US RTE 20 (LAKE ST) AT OAK AVE**

SCALE: 1"=20'
SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 345	SECTION 7Y-WRS	COUNTY COOK	TOTAL SHEETS 97	SHEET NO. 52
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

TS#1325



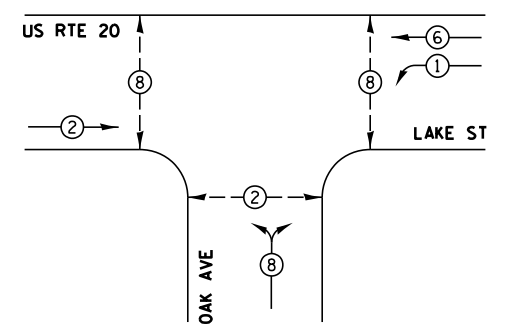
SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	338
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	95
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	76
HANDHOLE	EACH	2
HEAVY-DUTY HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	663
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	215
CONCRETE FOUNDATION, TYPE A	FOOT	8
DRILL EXISTING HANDHOLE	EACH	3
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
DETECTOR LOOP, TYPE I	FOOT	210
PEDESTRIAN PUSH-BUTTON	EACH	2
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
RELOCATE EXISTING SIGNAL HEAD	EACH	3
RELOCATE EXISTING TRAFFIC SIGNAL POST	EACH	2
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	715
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	2
REMOVE EXISTING CONCRETE FOUNDATION	EACH	2
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

CABLE PLAN

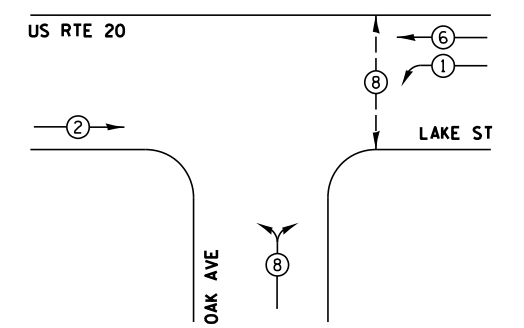
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	11		17	0.50	93.50
(YELLOW)	11		25	0.25	68.75
(GREEN)	11		15	0.25	41.25
ARROW	4		12	0.10	4.80
PED. SIGNAL	2		25	1.00	50.00
CONTROLLER	1		100	1.00	100.00
ILLUM. SIGN	-		25	0.05	-
VIDEO SYSTEM	-		150	1.00	-
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	358.30
ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAY/DISTRICT 1 201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096 ENERGY SUPPLY: CONTACT: JOE STACHO PHONE: (630) 424-5704 COMPANY: COMED					

EXISTING CONTROLLER SEQUENCE



EXIST. PHASE DESIGNATION DIAGRAM

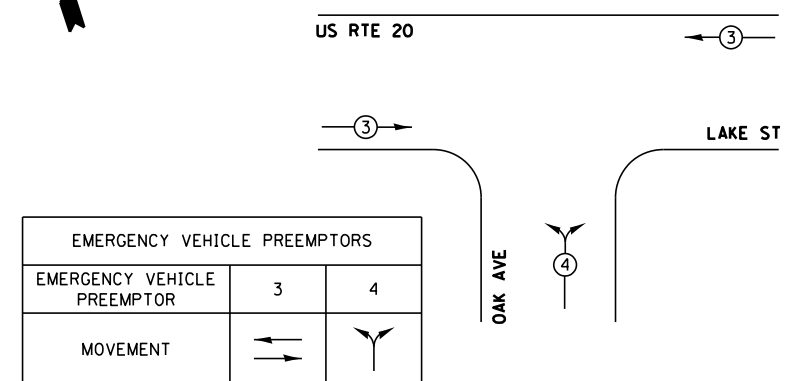
PROPOSED CONTROLLER SEQUENCE



PROP. PHASE DESIGNATION DIAGRAM

- LEGEND**
- ⊙ DUAL ENTRY PHASE
 - ⊠ SINGLE ENTRY PHASE
 - ⊙ OL OVERLAP
 - ⊙ PEDESTRIAN PHASE
 - * NUMBER REFERS TO ASSOCIATED PHASE

**EXISTING AND PROPOSED
EMERGENCY VEHICLE
PREEMPTION SEQUENCE**

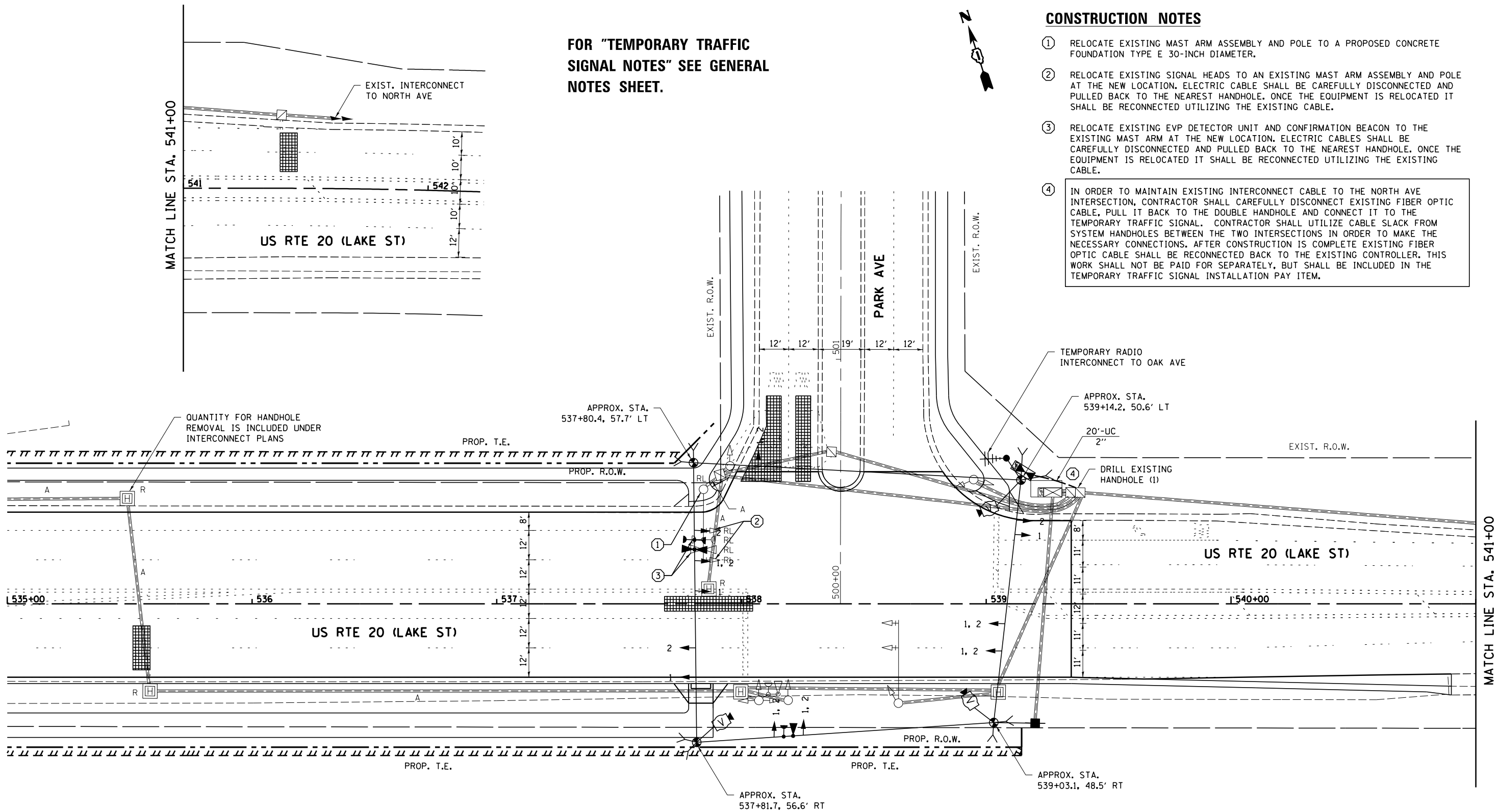


FOR "TEMPORARY TRAFFIC SIGNAL NOTES" SEE GENERAL NOTES SHEET.



CONSTRUCTION NOTES

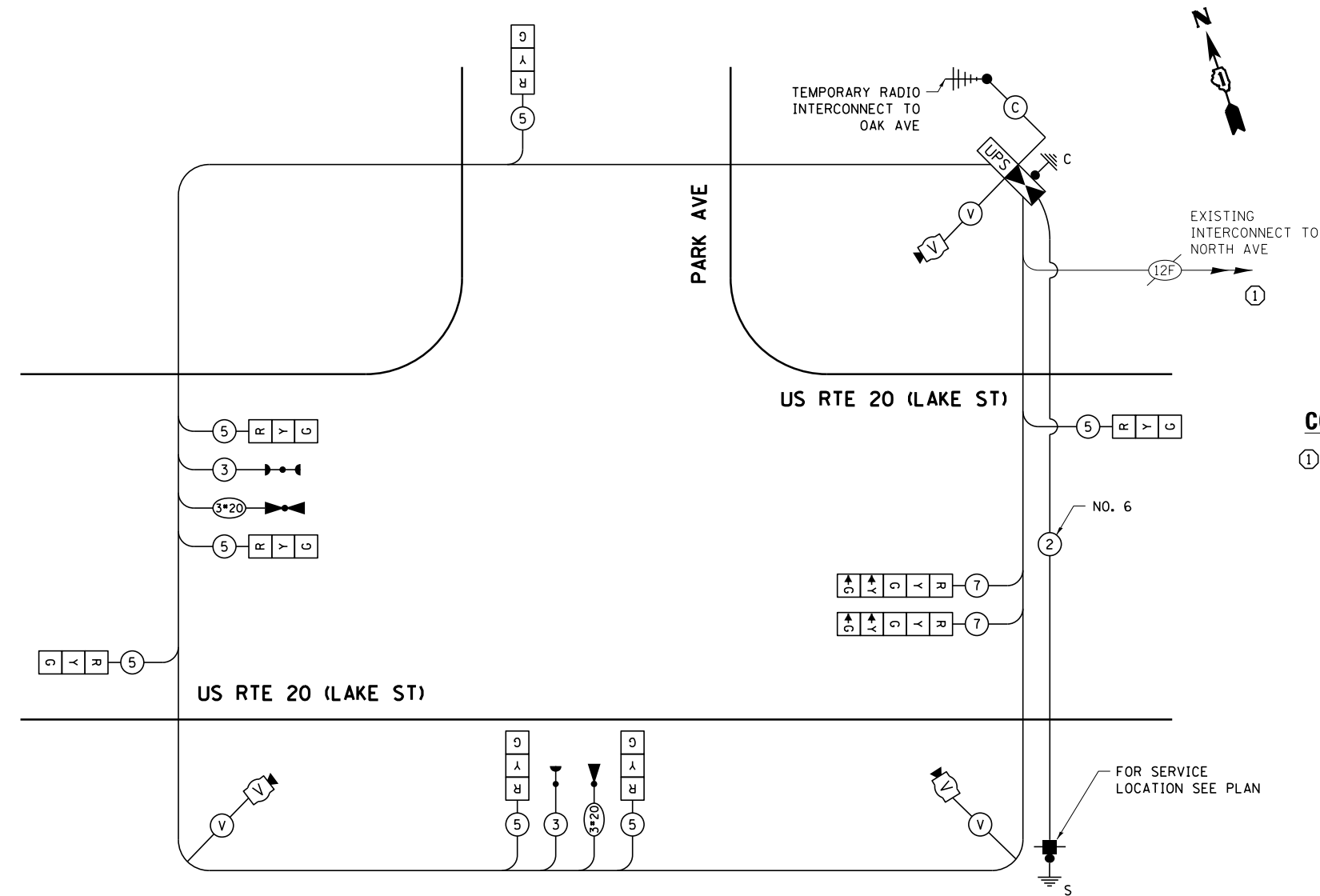
- ① RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE TO A PROPOSED CONCRETE FOUNDATION TYPE E 30-INCH DIAMETER.
- ② RELOCATE EXISTING SIGNAL HEADS TO AN EXISTING MAST ARM ASSEMBLY AND POLE AT THE NEW LOCATION. ELECTRIC CABLE SHALL BE CAREFULLY DISCONNECTED AND PULLED BACK TO THE NEAREST HANDHOLE. ONCE THE EQUIPMENT IS RELOCATED IT SHALL BE RECONNECTED UTILIZING THE EXISTING CABLE.
- ③ RELOCATE EXISTING EVP DETECTOR UNIT AND CONFIRMATION BEACON TO THE EXISTING MAST ARM AT THE NEW LOCATION. ELECTRIC CABLES SHALL BE CAREFULLY DISCONNECTED AND PULLED BACK TO THE NEAREST HANDHOLE. ONCE THE EQUIPMENT IS RELOCATED IT SHALL BE RECONNECTED UTILIZING THE EXISTING CABLE.
- ④ IN ORDER TO MAINTAIN EXISTING INTERCONNECT CABLE TO THE NORTH AVE INTERSECTION, CONTRACTOR SHALL CAREFULLY DISCONNECT EXISTING FIBER OPTIC CABLE, PULL IT BACK TO THE DOUBLE HANDHOLE AND CONNECT IT TO THE TEMPORARY TRAFFIC SIGNAL. CONTRACTOR SHALL UTILIZE CABLE SLACK FROM SYSTEM HANDHOLES BETWEEN THE TWO INTERSECTIONS IN ORDER TO MAKE THE NECESSARY CONNECTIONS. AFTER CONSTRUCTION IS COMPLETE EXISTING FIBER OPTIC CABLE SHALL BE RECONNECTED BACK TO THE EXISTING CONTROLLER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PAY ITEM.



NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

FILE NAME =	USER NAME = pocitech	DESIGNED - LP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN STAGE 1 AND 2 US RTE 20 (LAKE ST) AT PARK AVE			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 40.0000' / in.	CHECKED - PB / SM	REVISED -		CONTRACT NO. 60N18							
	PLOT DATE = 1/3/2013	DATE - 01/03/2013	REVISED -		ILLINOIS FED. AID PROJECT							

NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.



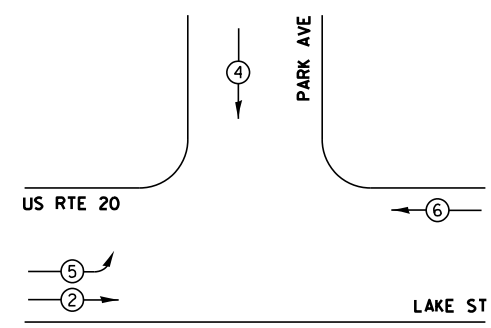
CONSTRUCTION NOTES
① EXISTING INTERCONNECT TO NORTH AVE SHALL BE MAINTAINED AS SHOWN ON THE PREVIOUS SHEET.

**TEMPORARY CABLE PLAN
STAGE 1 AND 2**

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	9	17	0.50	76.50	
(YELLOW)	9	25	0.25	56.25	
(GREEN)	9	15	0.25	33.75	
ARROW	4	12	0.10	4.80	
PED. SIGNAL	-	25	1.00	-	
CONTROLLER	1	100	1.00	100.00	
ILLUM. SIGN	-	25	0.05	-	
VIDEO SYSTEM	1	150	1.00	150.00	
FLASHER			0.50		
ENERGY COSTS TO:				TOTAL =	421.30

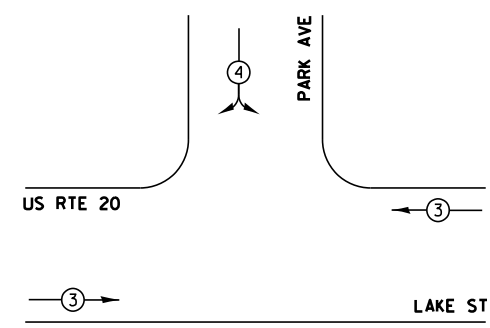
ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630) 424-5704
COMPANY: COMED

TEMPORARY CONTROLLER SEQUENCE



TEMPORARY PHASE DESIGNATION DIAGRAM

**TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE**



- LEGEND**
- ⊙ DUAL ENTRY PHASE
 - ⊠ SINGLE ENTRY PHASE
 - ⊙ OL OVERLAP
 - ⊙ PEDESTRAIN PHASE
 - * NUMBER REFERS TO ASSOCIATED PHASE

EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	←→	↗

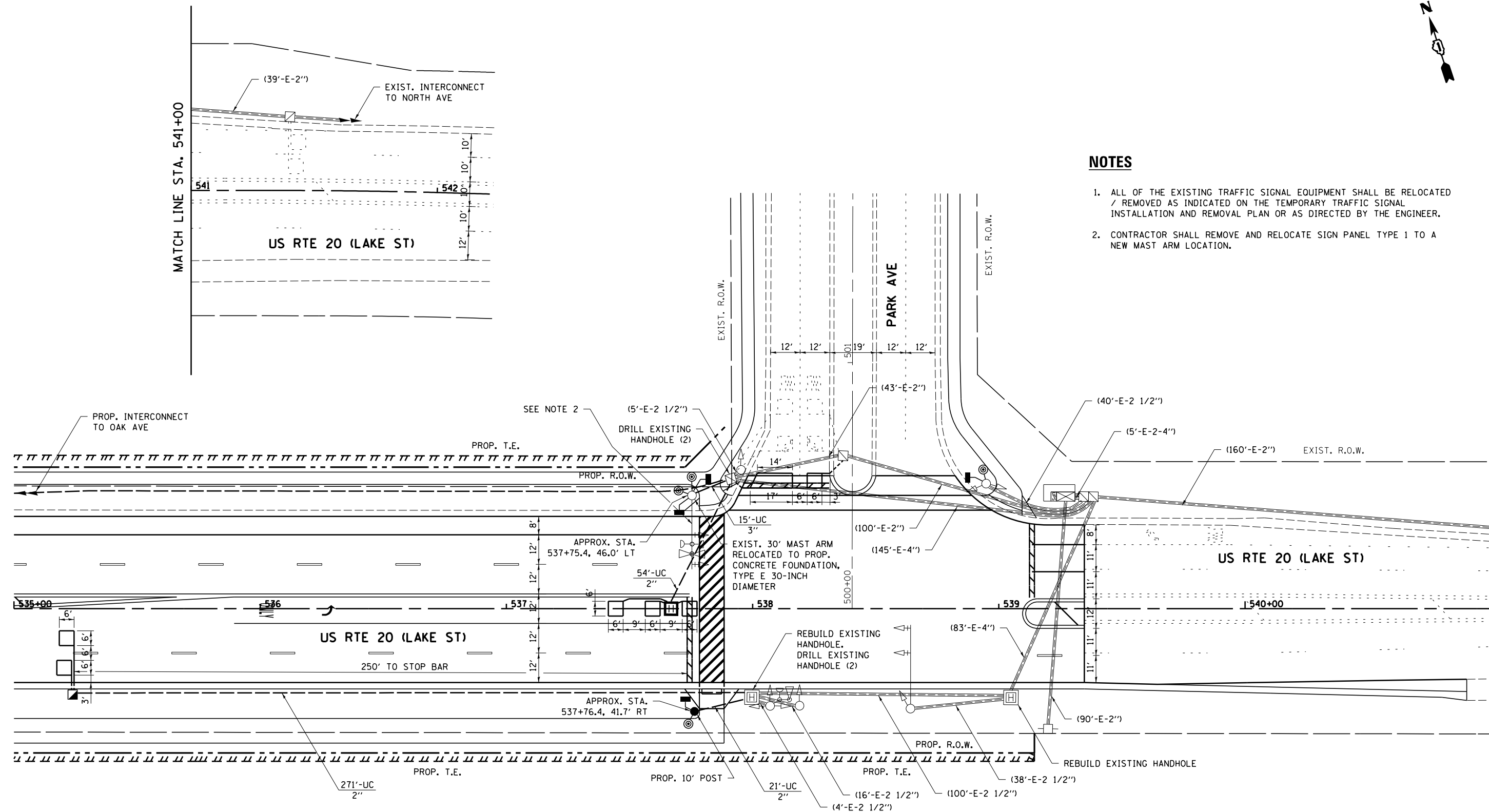
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM
AND EMERGENCY VEHICLE PREEMPTION SEQUENCE (STAGE 1 AND 2)
US RTE 20 (LAKE ST) AT PARK AVE
SCALE: NTS SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	55
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

TS#1330



NOTES

1. ALL OF THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RELOCATED / REMOVED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN OR AS DIRECTED BY THE ENGINEER.
2. CONTRACTOR SHALL REMOVE AND RELOCATE SIGN PANEL TYPE 1 TO A NEW MAST ARM LOCATION.

MATCH LINE STA. 541+00

MATCH LINE STA. 541+00

TS#1330

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	PLOT DATE = 1/3/2013	DATE - 01/03/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN
US RTE 20 (LAKE ST) AT PARK AVE**

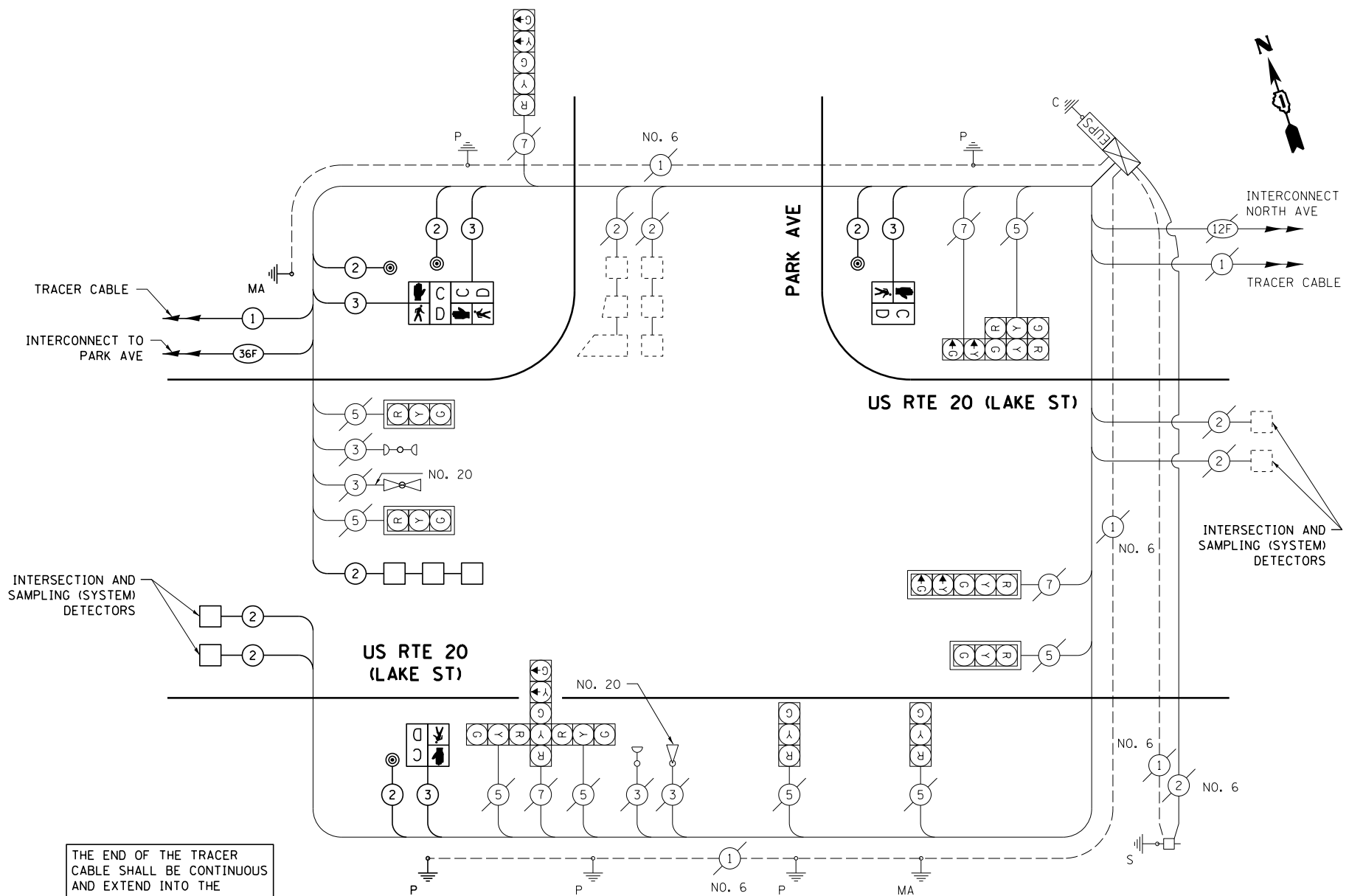
SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	56
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY.
REMOVE SIGN PANEL - TYPE 1	SO FT	6
RELOCATE SIGN PANEL - TYPE 1	SO FT	6
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	366
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	15
HANDHOLE	EACH	1
HEAVY-DUTY HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	725
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	753
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	731
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	68
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	4
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	14
DRILL EXISTING HANDHOLE	EACH	5
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1
DETECTOR LOOP, TYPE I	FOOT	235
PEDESTRIAN PUSH-BUTTON	EACH	4
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
RELOCATE EXISTING SIGNAL HEAD	EACH	2
RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE	EACH	1
RELOCATE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, DETECTOR UNIT	EACH	1
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	276
REBUILD EXISTING HANDHOLE	EACH	2
REMOVE EXISTING HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

* 100% COST TO THE VILLAGE OF STREAMWOOD



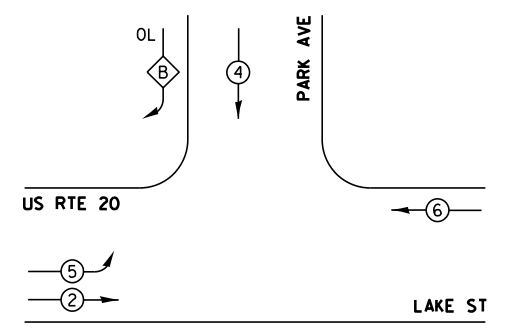
THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

CABLE PLAN

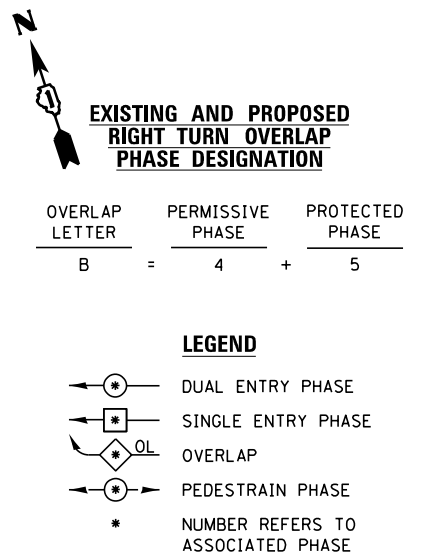
I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. OF LAMPS	WATTAGE INCAND.	LED	% OPERATION	
SIGNAL (RED)	12		17	0.50	102.00
(YELLOW)	12		25	0.25	75.00
(GREEN)	12		15	0.25	45.00
ARROW	8		12	0.10	9.60
PED. SIGNAL	4		25	1.00	100.00
CONTROLLER	1		100	1.00	100.00
ILLUM. SIGN	-		25	0.05	-
VIDEO SYSTEM	-		150	1.00	-
FLASHER				0.50	
ENERGY COSTS TO:				TOTAL =	431.60

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096
ENERGY SUPPLY: CONTACT: JOE STACHO
PHONE: (630) 424-5704
COMPANY: COMED

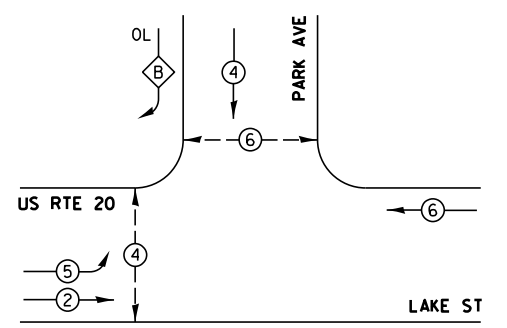
EXISTING CONTROLLER SEQUENCE



EXIST. PHASE DESIGNATION DIAGRAM

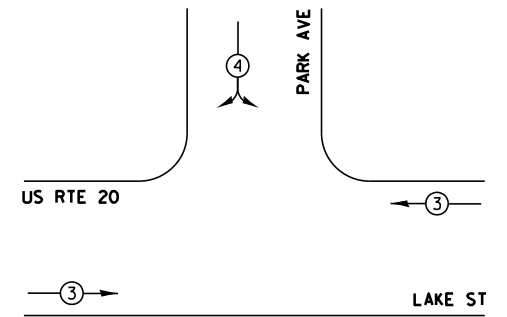


PROPOSED CONTROLLER SEQUENCE

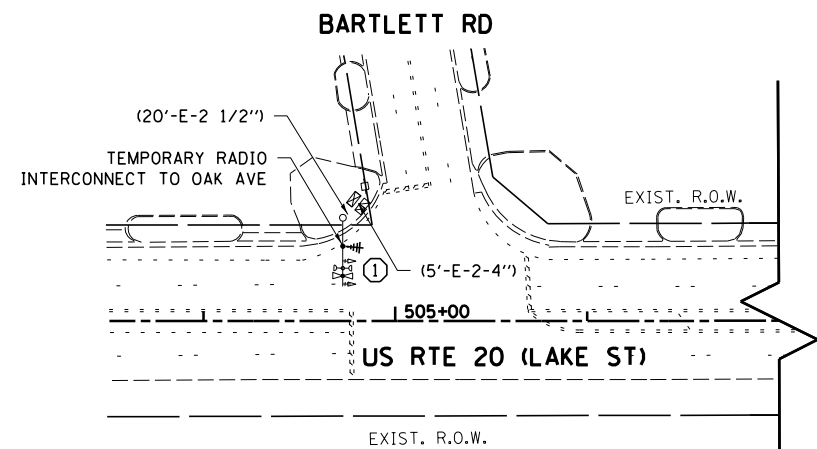


PROP. PHASE DESIGNATION DIAGRAM

EXISTING AND PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

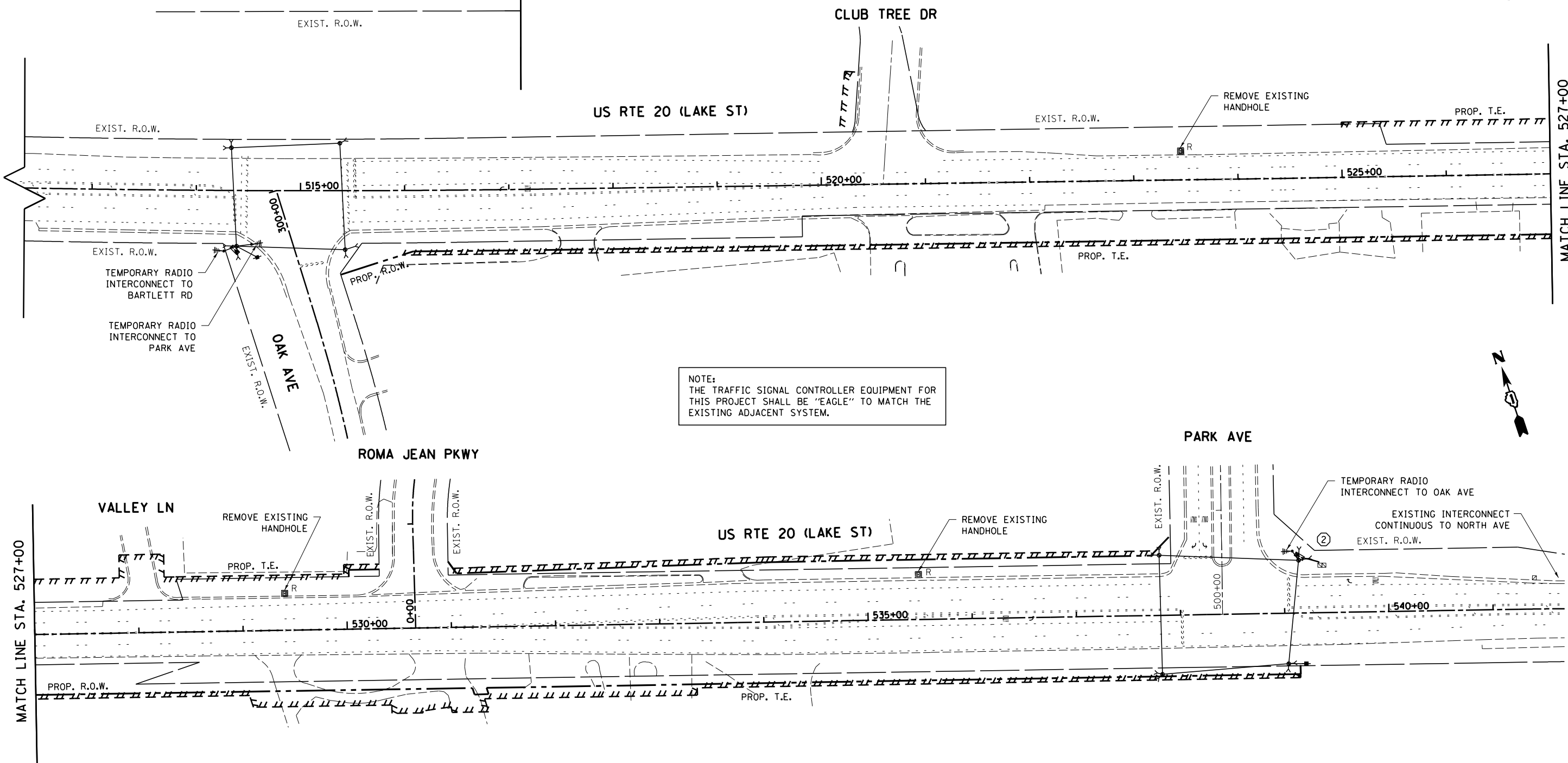


EMERGENCY VEHICLE PREEMPTORS		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT	← →	↖ ↗



CONSTRUCTION NOTES

- ① TEMPORARY RADIO INTERCONNECT ANTENNA AT THE INTERSECTION OF US RTE 20 AND BARTLETT RD SHALL BE INSTALLED ON THE MAST ARM ASSEMBLY AND POLE AS SHOWN. THE CABLE FROM THE ANTENNA SHALL BE INSTALLED IN THE EXISTING CONDUITS BETWEEN THE ANTENNA AND THE EXISTING CONTROLLER. THE ANTENNA AND ALL CABLES SHALL BE REMOVED UPON COMPLETION OF THE PROPOSED FIBER OPTIC INTERCONNECT SYSTEM. ANY HOLES IN THE MAST ARM SHALL BE PLUGGED. THIS WORK SHALL BE INCLUDED IN THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
- ② EXISTING INTERCONNECT TO NORTH AVE SHALL BE MAINTAINED AS SHOWN ON THE TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVAL PLAN.



NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "EAGLE" TO MATCH THE EXISTING ADJACENT SYSTEM.

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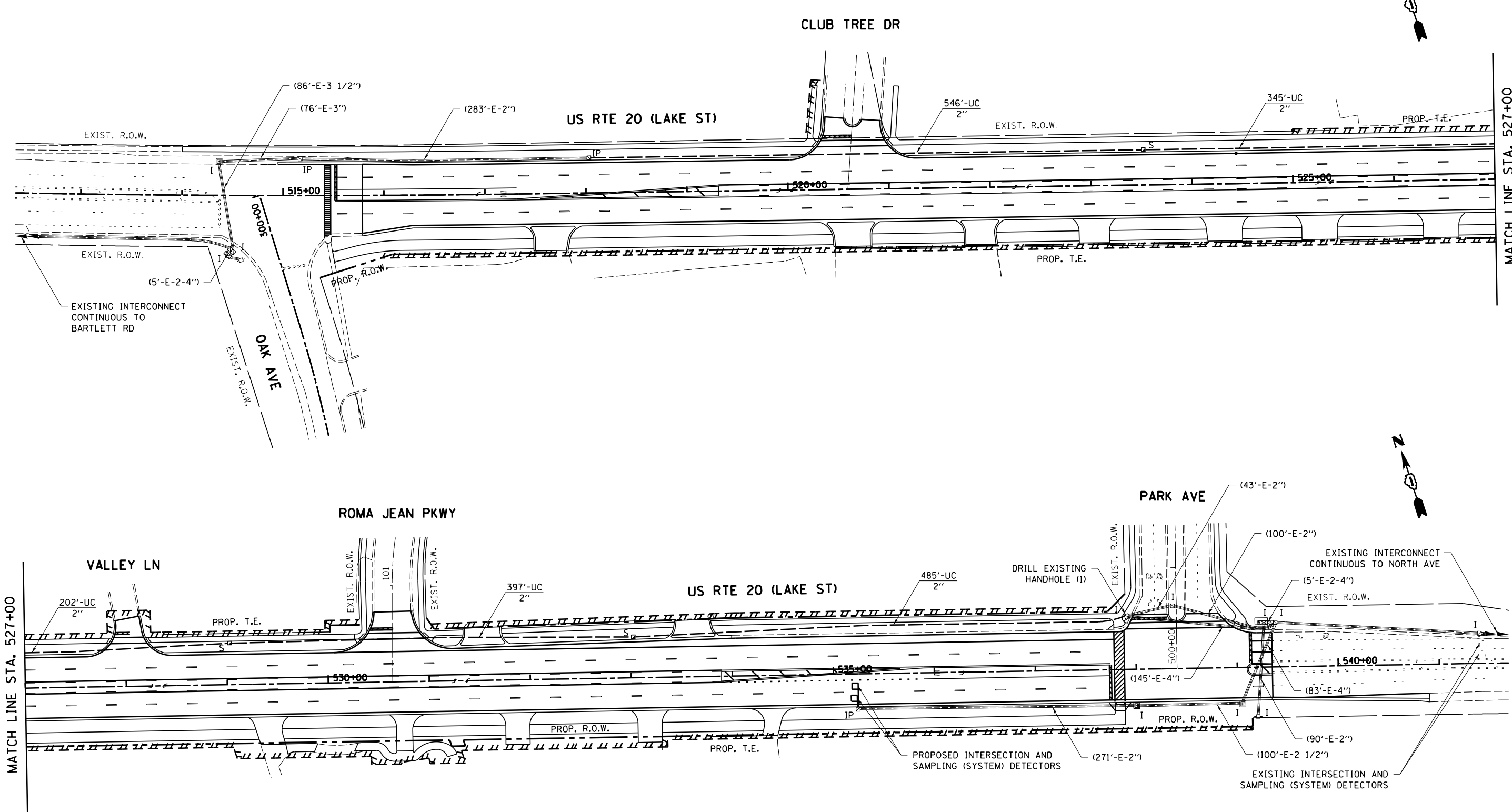
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT PLAN
US RTE 20 (BARTLETT RD TO NORTH AVE)**

SCALE: NTS SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	58
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

EAGLE 5F

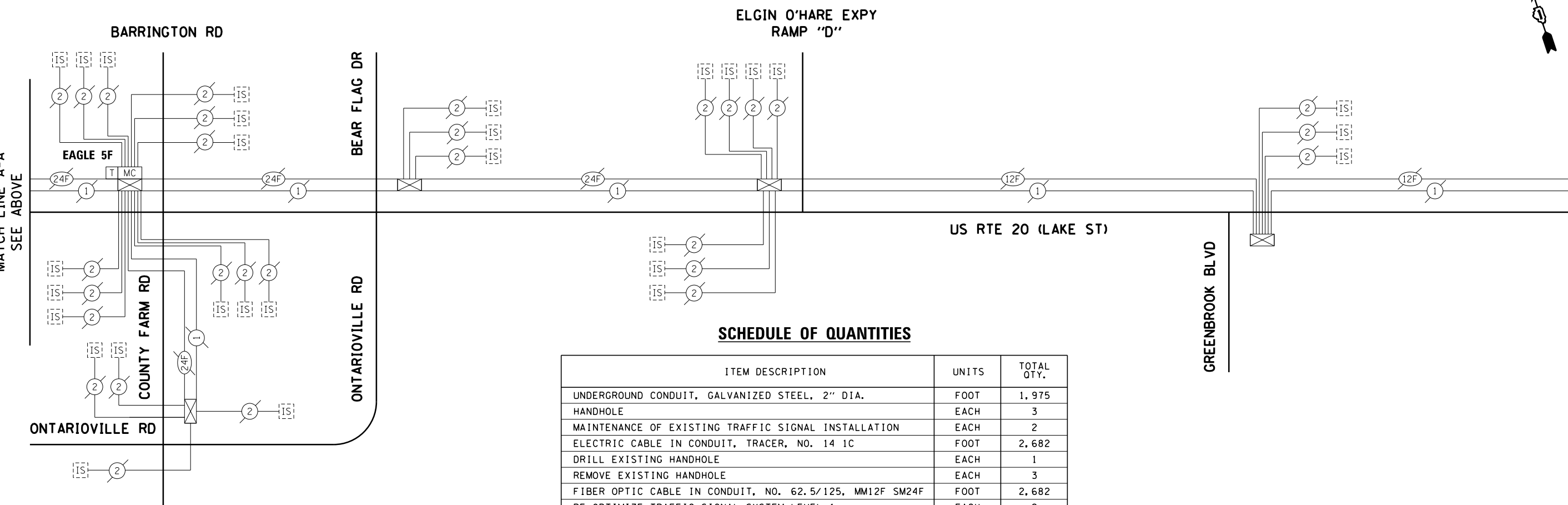
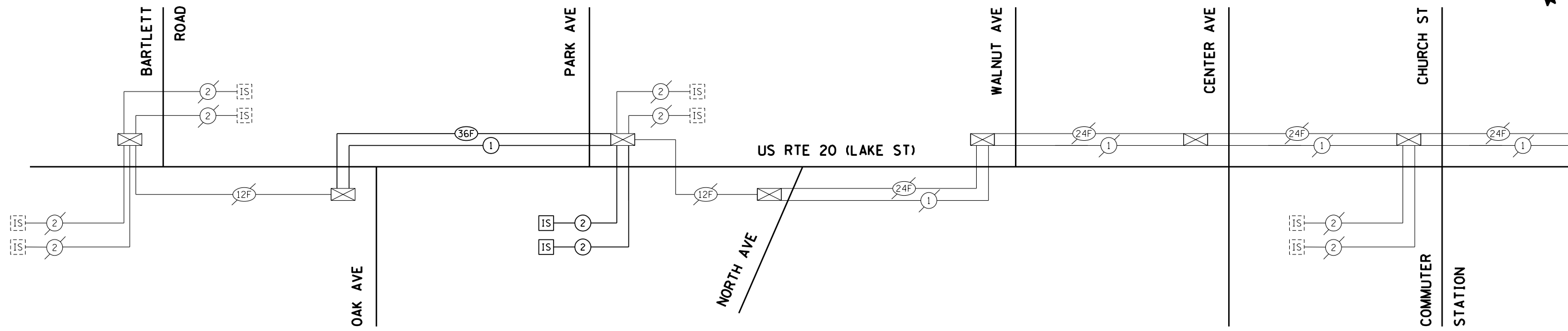


MATCH LINE STA. 527+00

MATCH LINE STA. 527+00

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	SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.						CONTRACT NO. 60N18 <small>ILLINOIS FED. AID PROJECT</small>			

EAGLE 5F



SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1,975
HANDHOLE	EACH	3
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2,682
DRILL EXISTING HANDHOLE	EACH	1
REMOVE EXISTING HANDHOLE	EACH	3
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2,682
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	2

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	PLOT DATE = 1/3/2013	DATE - 01/03/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT SCHEMATIC
US RTE 20 (BARTLETT RD TO SPRINGFIELD DR)**

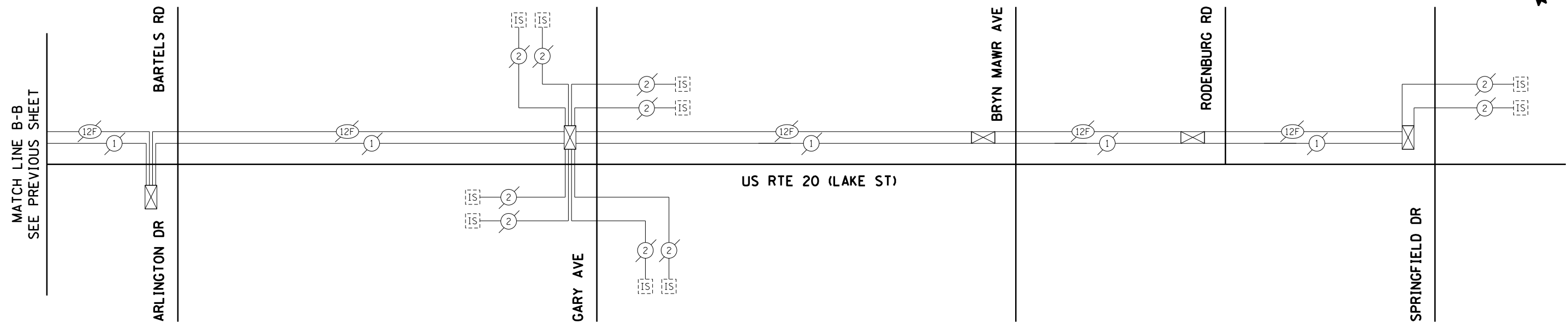
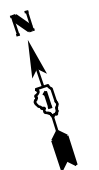
SCALE: NTS SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	61
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				

MATCH LINE A-A
SEE BELOW

MATCH LINE B-B
SEE NEXT SHEET

EAGLE 5F



EAGLE 5F

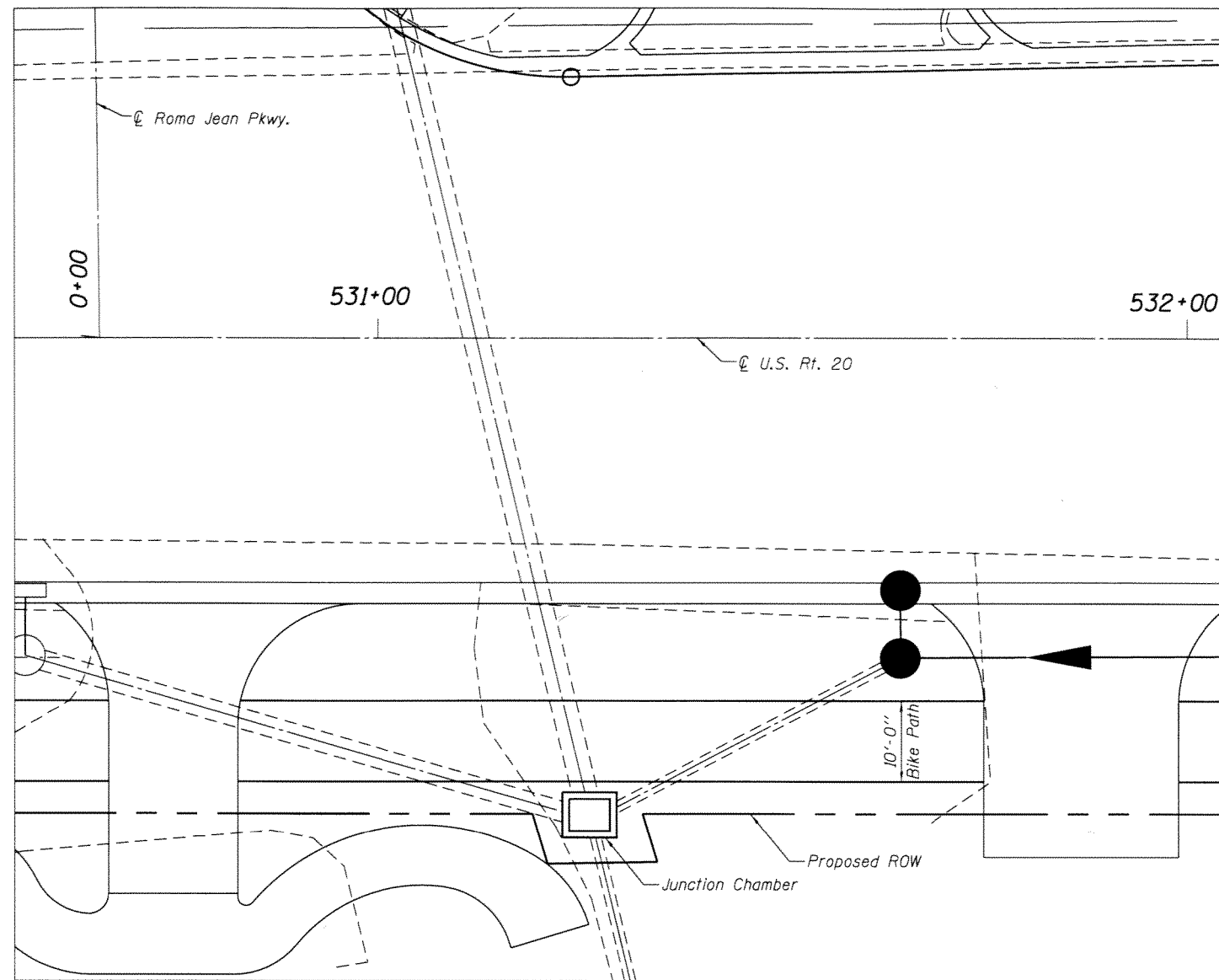
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

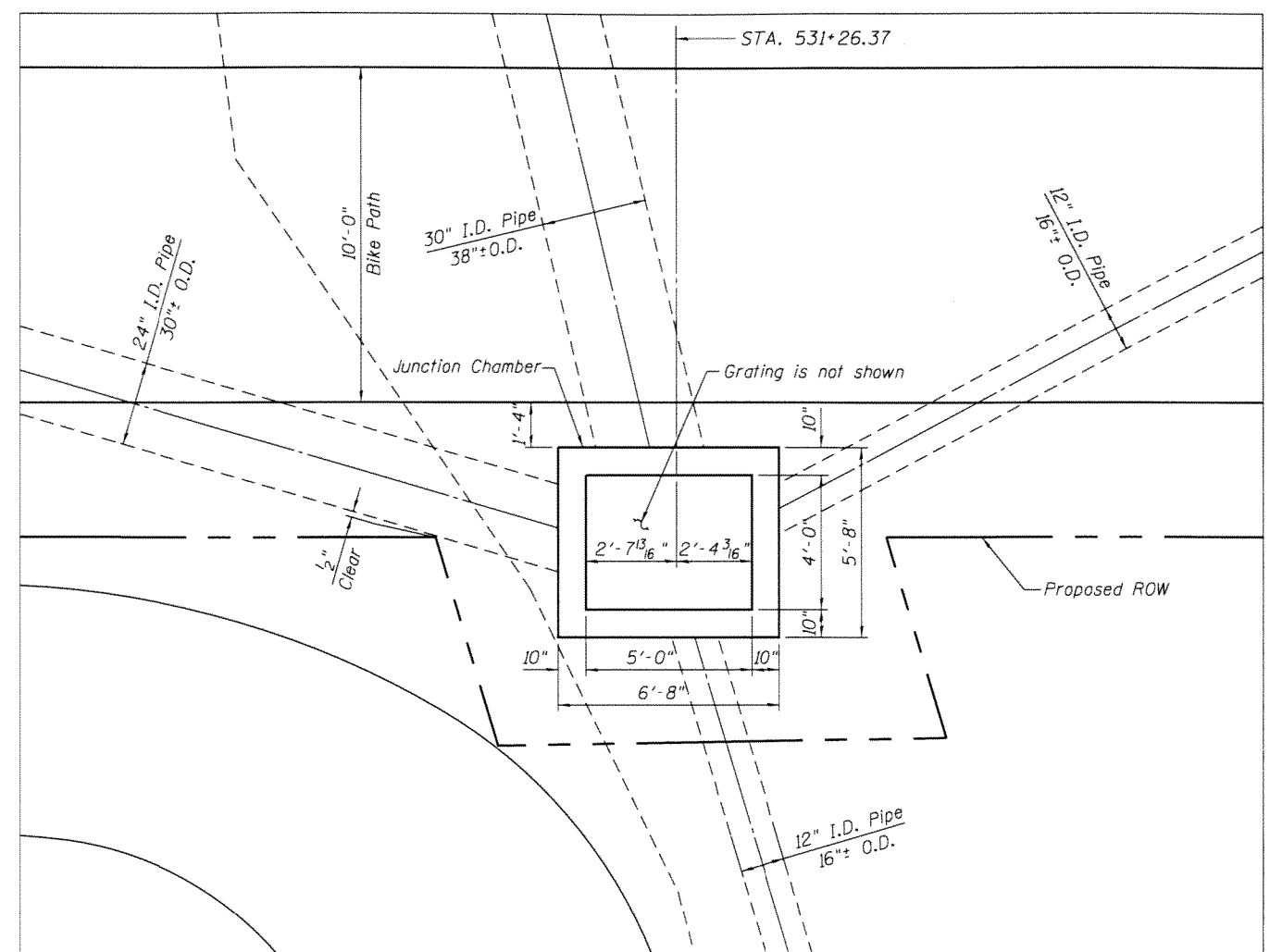
**INTERCONNECT SCHEMATIC
US RTE 20 (BARTLETT RD TO SPRINGFIELD DR)**

SCALE: NTS SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	62
CONTRACT NO. 60N18				
ILLINOIS FED. AID PROJECT				



GENERAL PLAN



JUNCTION CHAMBER PLAN VIEW

GENERAL NOTES

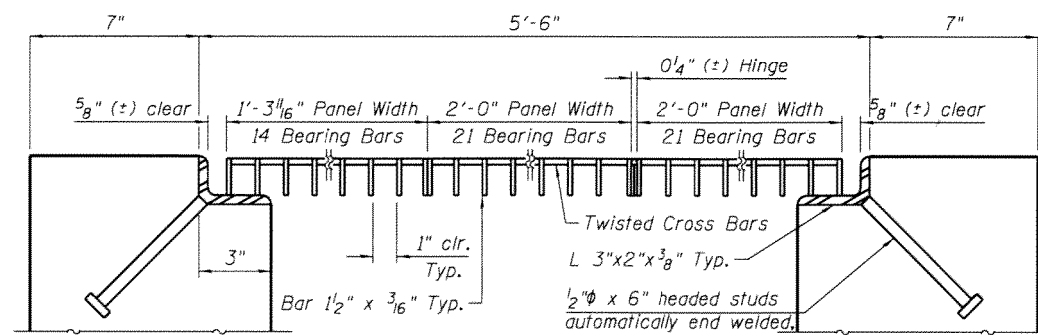
- All exposed concrete edges shall have a 3/4" x 45° chamfer except where shown otherwise. Chamfer on vertical edges shall be continued a minimum of one foot below finished ground level.
- Reinforcement bar bending details shall be in accordance with the latest "Manual Of Standard Practice For Detailing Reinforced Concrete Structures", ACI 315.
- Reinforcement bar bending dimensions are out to out.
- Cover from the face of concrete to face of reinforcement bars shall be 3" for surfaces cast against earth and 2" for all other surfaces unless otherwise shown.
- Reinforcement bars designated (E) shall be epoxy coated.
- Protective coat shall be applied to the concrete surfaces at the junction chamber located in the top 1'-0" of the junction chamber.
- Steel grating has been designed for a 1,000 pound wheel load.
- Reduce A dimension of 3 v₁(E) and 3 v₇(E) bars to 4'-6" for use in south wall to clear rectangular openings.
- Excavation and backfill shall not be paid for separately but shall be considered as included with payment for Concrete Structures.

ABBREVIATIONS

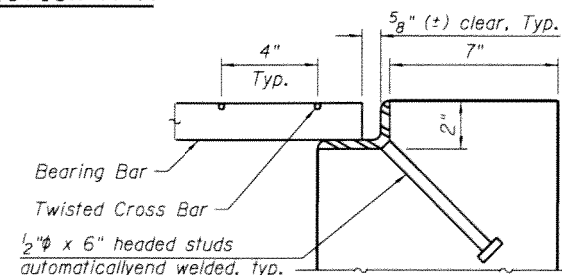
- Clr. Clearance
 cfs. centers (center to center)
 E.F. Each Face
 Elev. Elevation
 I.D. Inside Diameter
 I.F. Inside Face
 O.D. Outside Diameter
 O.F. Outside Face
 psi Pounds Per Square Inch
 Typ. Typical
 U.N.O Unless Noted Otherwise

DESIGN STRESSES

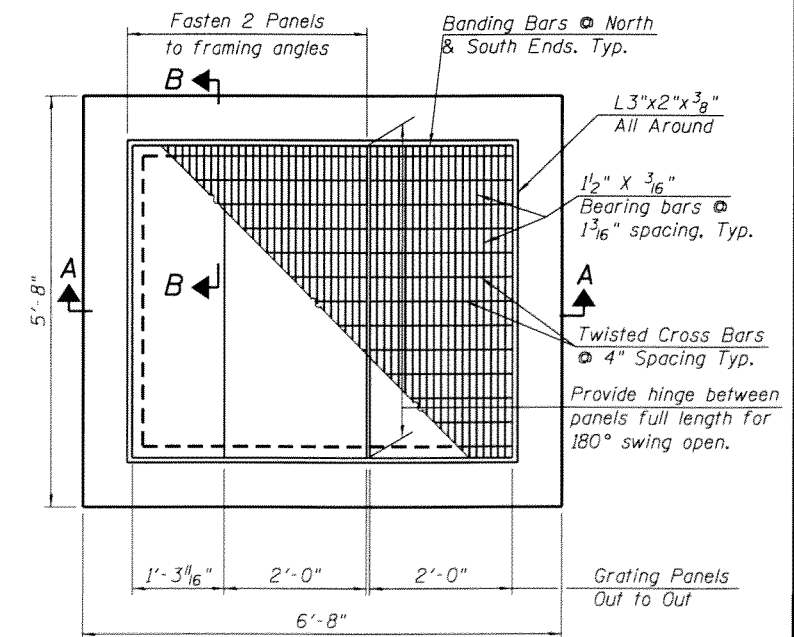
f'c = 3,500 psi @ 14 days, Class SI Concrete
 fy = 60,000 psi, Reinforcement Bars
 fy = 33,000 psi min., Grating Bars (fs=18,000 psi)



SECTION A-A



SECTION B-B



JUNCTION CHAMBER TOP (GRATING) PLAN

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GRAF
 8501 W. Higgins Road, Suite 280
 Chicago, Illinois 60631 (773) 399-0112

USER NAME =	DESIGNED - J.J.G.	REVISED -
PLOT SCALE =	CHECKED - J.A.Z.	REVISED -
PLOT DATE =	DRAWN - G.A.B.	REVISED -
	DATE - 11/26/2012	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

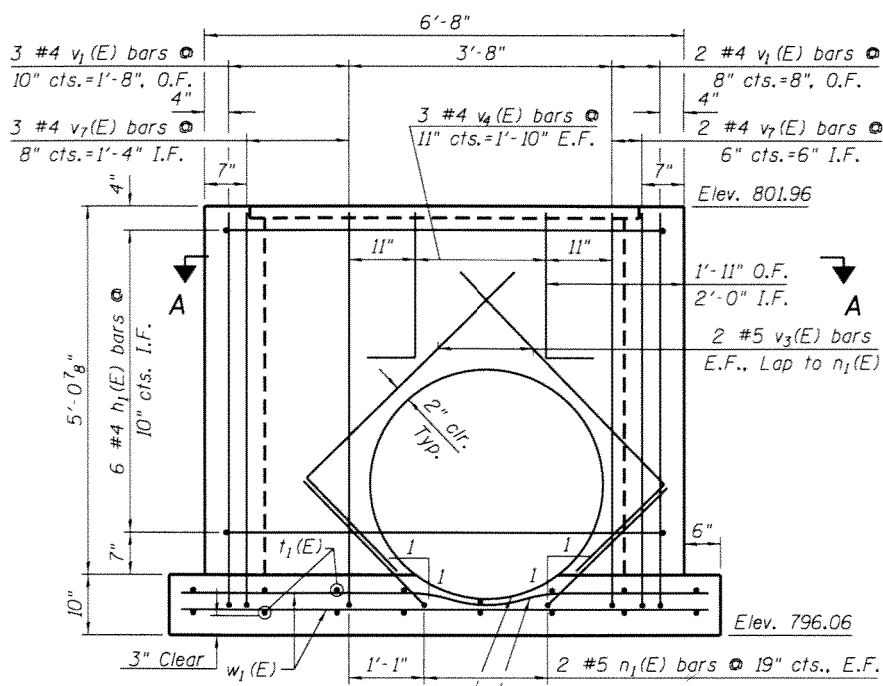
**U.S. ROUTE 20 (LAKE STREET)-OAK AVENUE TO PARK AVENUE
 JUNCTION CHAMBER, STA 531 + 26.37**

F.A.U. RTE. 345	SECTION 7Y-WRS	COUNTY COOK	TOTAL SHEETS 97	SHEET NO. 63
CONTRACT NO. 60N18				ILLINOIS FED. AID PROJECT

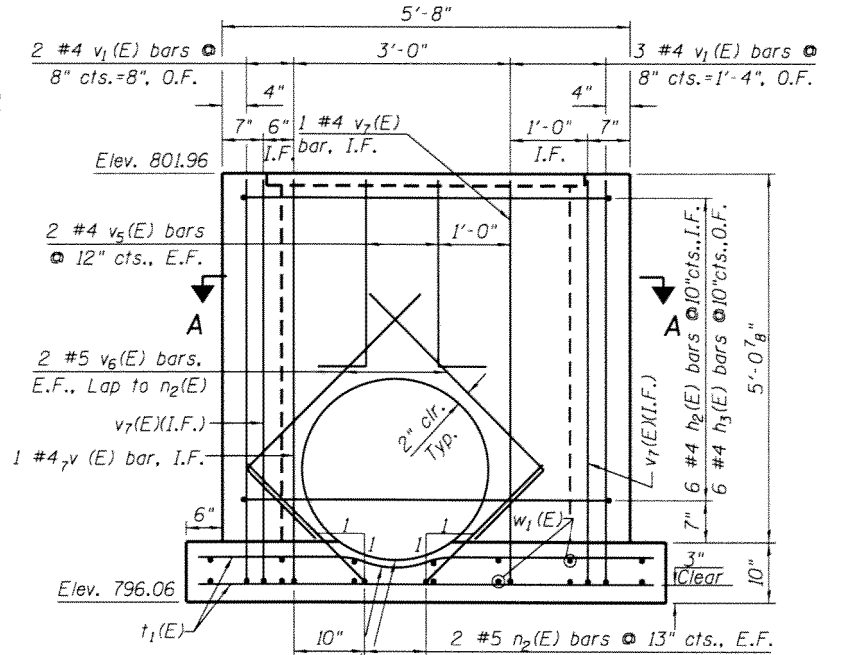
SHEET NO. 51 OF 2 SHEETS

BILL OF MATERIAL

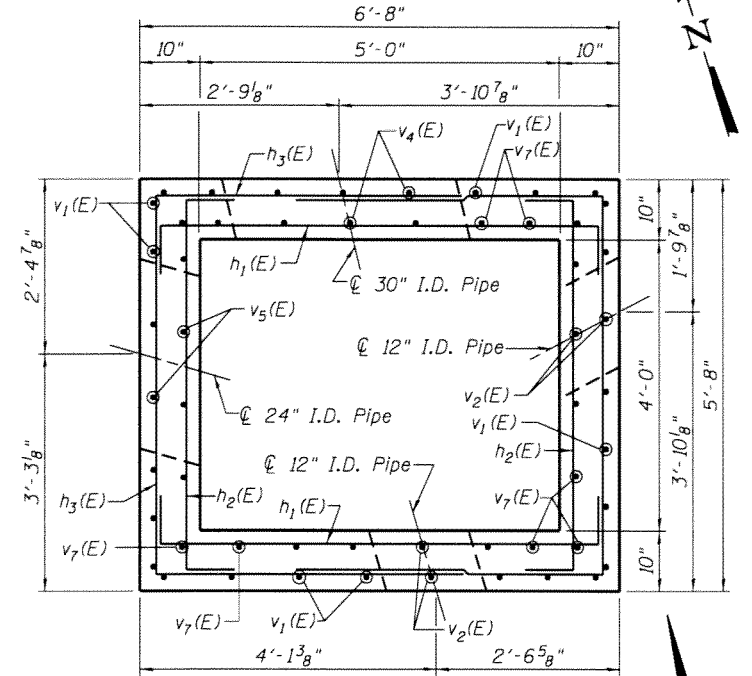
Bar	No.	Size	Length	Shape
$h_1(E)$	12	#4	7'-7"	[Shape]
$h_2(E)$	12	#4	6'-7"	[Shape]
$h_3(E)$	12	#4	13'-11"	[Shape]
$h_4(E)$	4	#5	4'-6"	[Shape]
$n_1(E)$	4	#5	3'-4"	[Shape]
$n_2(E)$	4	#5	3'-1"	[Shape]
$t_1(E)$	8	#4	6'-4"	[Shape]
$v_1(E)$	23	#4	6'-1"	[Shape]
$v_2(E)$	4	#4	4'-3"	[Shape]
$v_3(E)$	4	#5	5'-10"	[Shape]
$v_4(E)$	6	#4	2'-7"	[Shape]
$v_5(E)$	4	#4	3'-2"	[Shape]
$v_6(E)$	4	#5	4'-9"	[Shape]
$v_7(E)$	17	#4	6'-0"	[Shape]
$w_1(E)$	7	#4	7'-4"	[Shape]
Item	Unit	Quantity		
Concrete Structures	Cu. Yd.	4.4		
Protective Coat	Sq. Yd.	7.3		
Stud Shear Connectors	Each	18		
Reinforcement Bars, Epoxy Coated	Pound	580		
Metal Bar Grating	Pound	370		



NORTH WALL ELEVATION LOOKING SOUTH



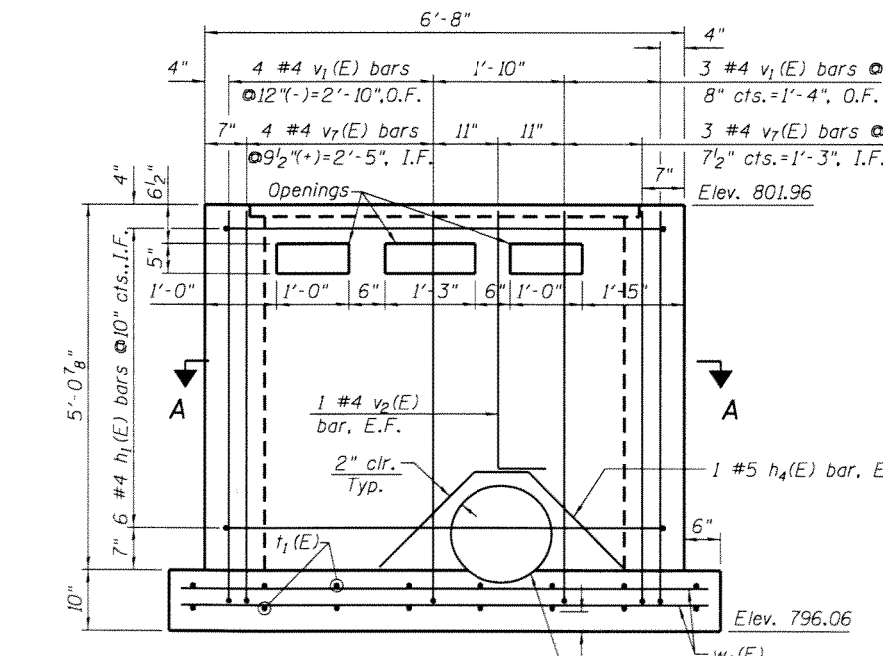
WEST WALL ELEVATION LOOKING EAST



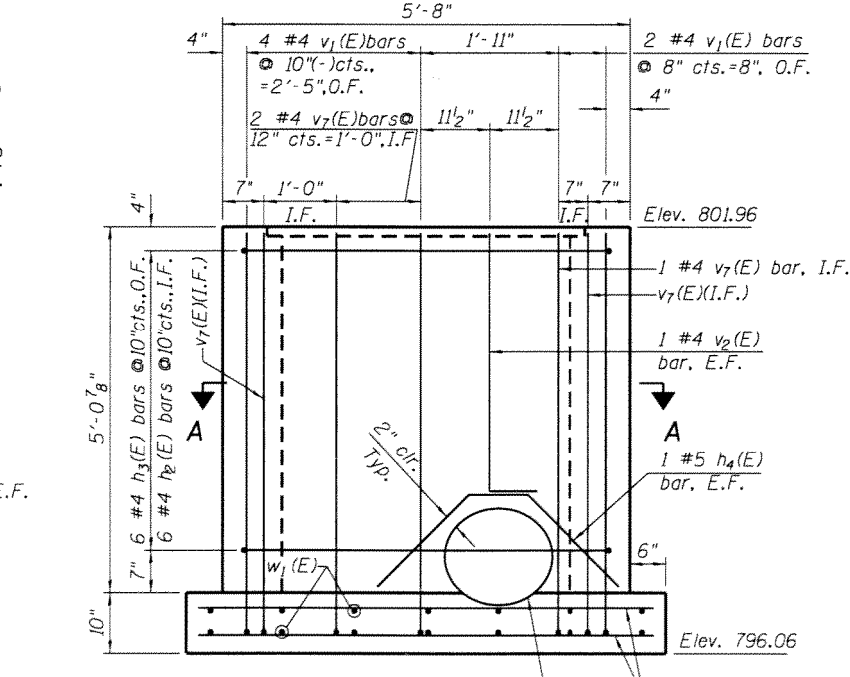
SECTION A-A

3'-2" (±) X 3'-3" (±) Opening for 30" I.D. pipe. Match Pipe Invert to Top of Base Slab Elev. Cut & remove a piece of 2 $w_1(E)$ & 1 $t_1(E)$ bars in this area to clear pipe wall.

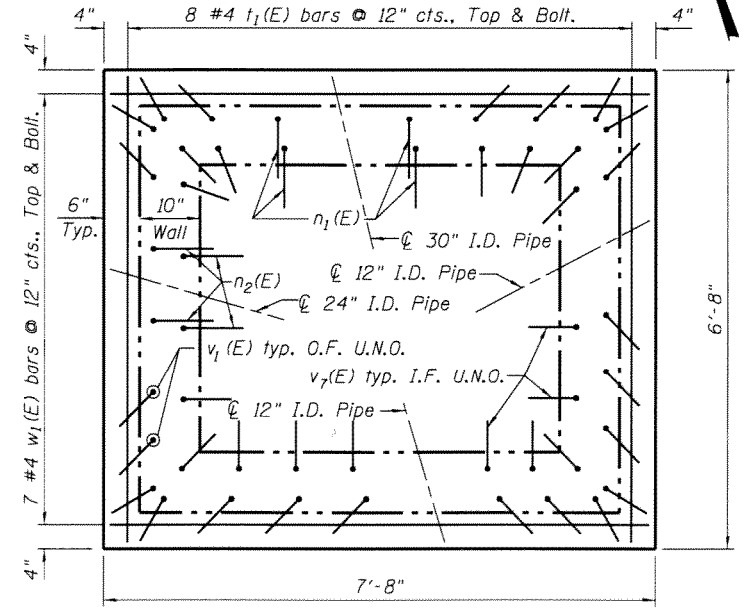
2'-6" (±) X 2'-7" (±) Opening for 24" I.D. Pipe. Match Pipe Invert to Top of Base Slab Elev. Cut & remove a piece of 2 $t_1(E)$ bars in this area to clear pipe wall.



SOUTH WALL ELEVATION LOOKING NORTH



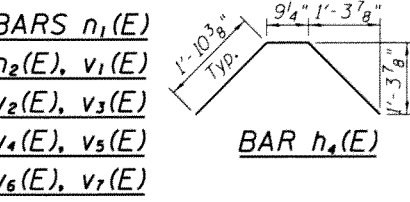
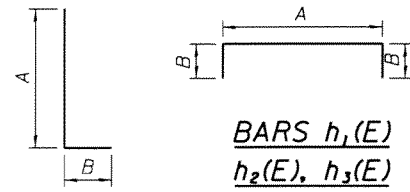
EAST WALL ELEVATION LOOKING WEST



FOOTING PLAN

BAR BENDING DIMENSIONS

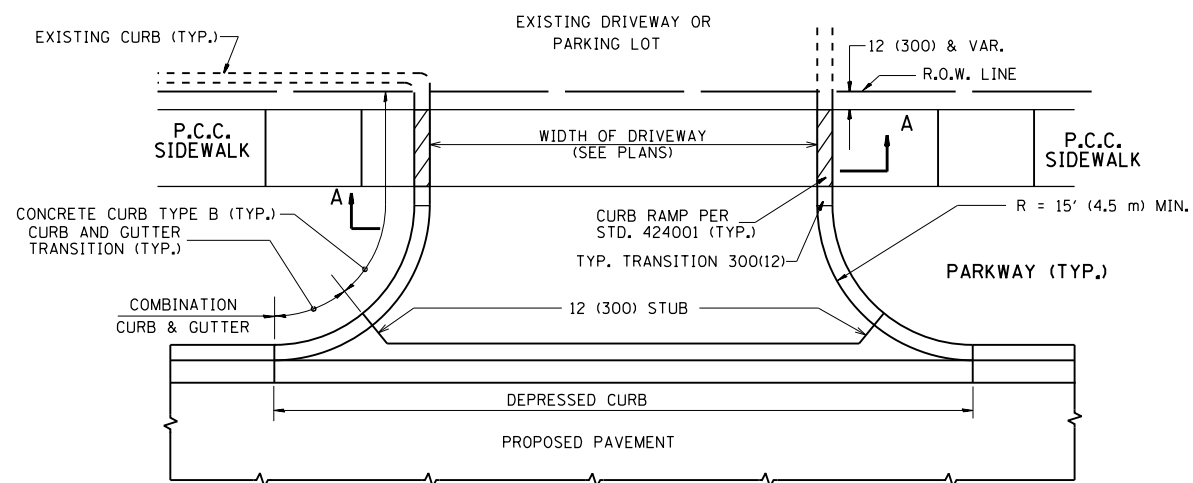
Bar	A	B
$h_1(E)$	6'-3"	8"
$h_2(E)$	5'-3"	8"
$h_3(E)$	5'-3"	4'-4"
$n_1(E)$	2'-6"	10"
$n_2(E)$	2'-3"	10"
$v_1(E)$	5'-5" (2)	8"
$v_2(E)$	3'-7"	8"
$v_3(E)$	4'-1 1/4"	1'-8 3/4"
$v_4(E)$	1'-11"	8"
$v_5(E)$	2'-6"	8"
$v_6(E)$	3'-5"	1'-4"
$v_7(E)$	5'-4" (2)	8"



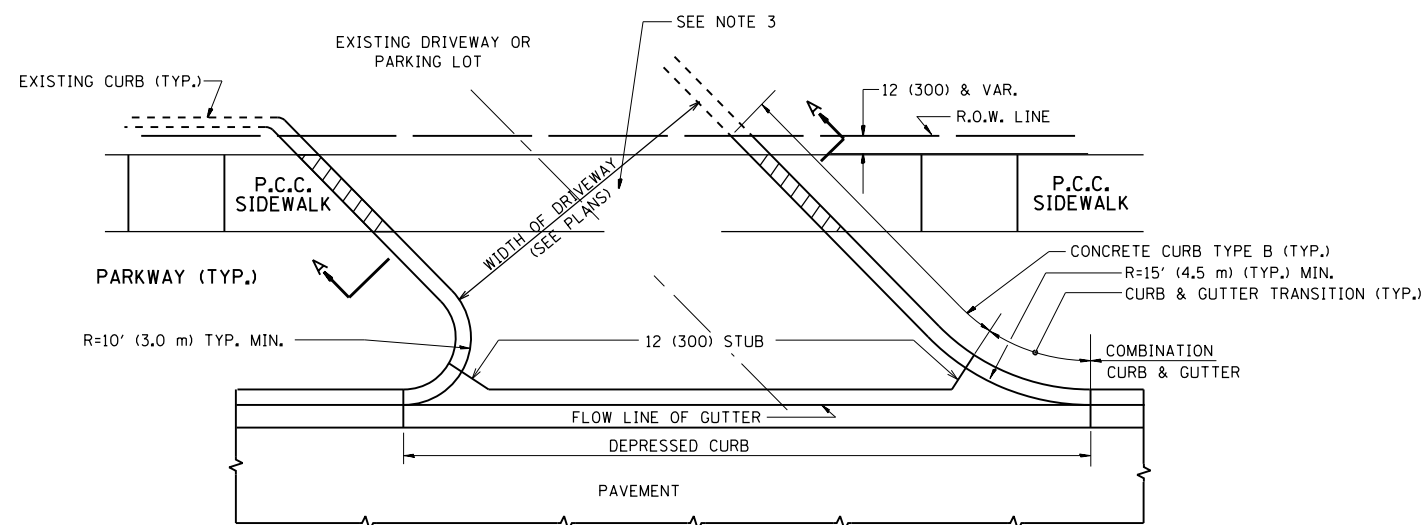
NOTES:

- Provide Openings thru horizontal reinforcement bars to clear pipe openings.
- See General Notes, Note B.

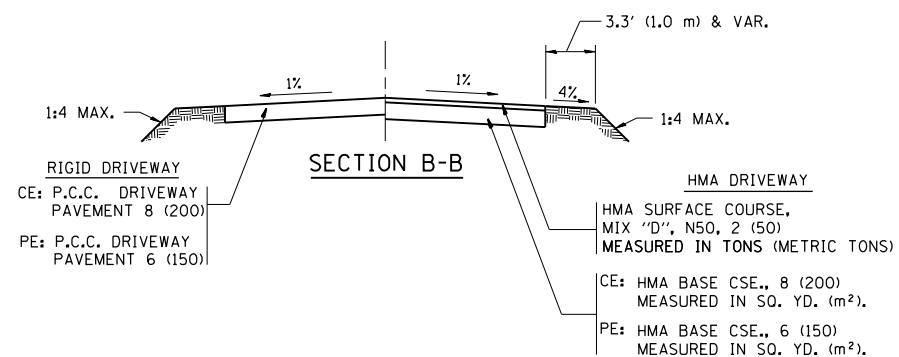
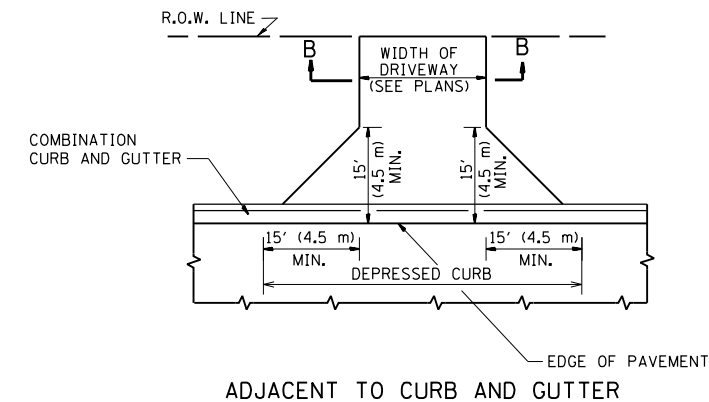
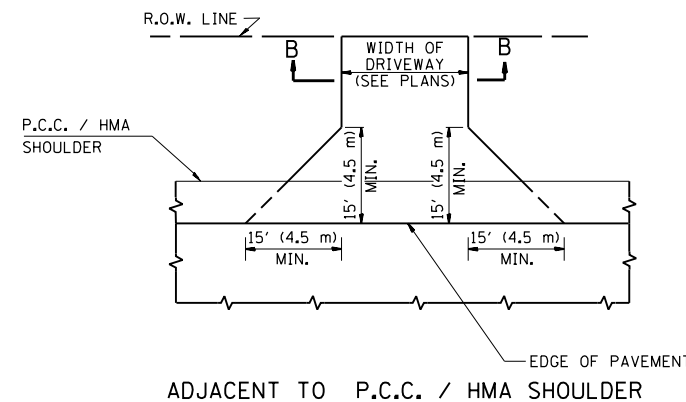
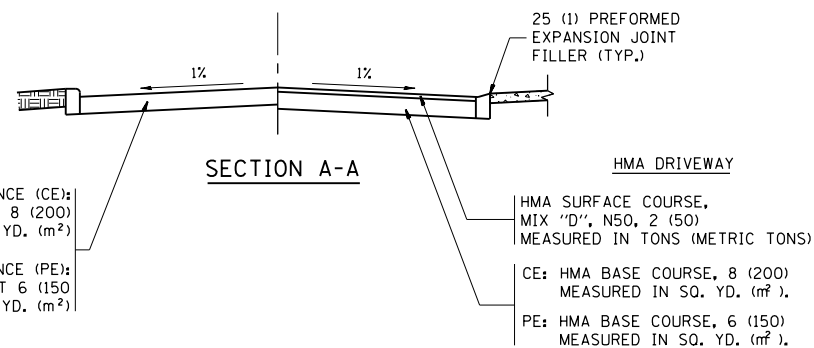
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WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)
HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)
AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

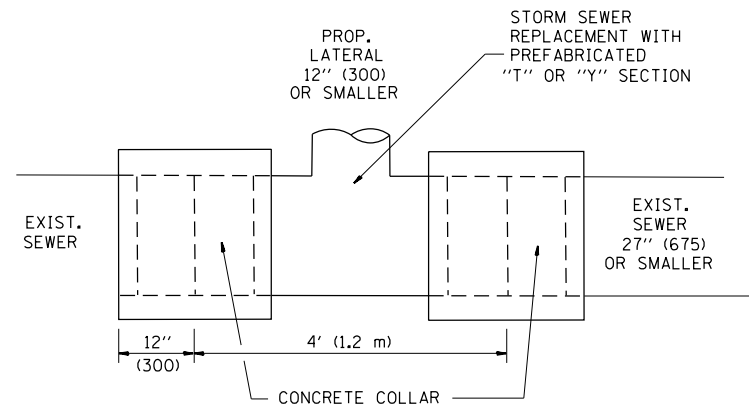
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = galbenjr	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
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	PLOT DATE = 4/3/2013	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

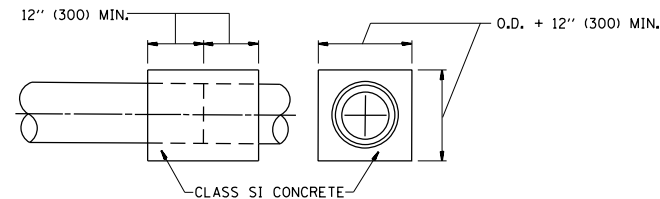
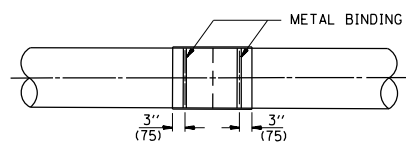
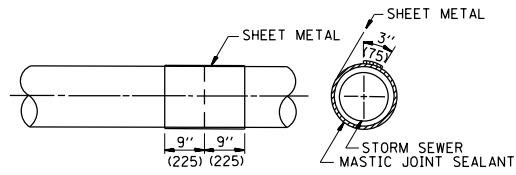
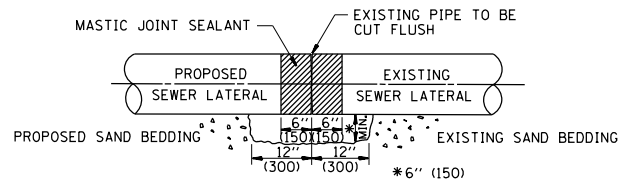
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WR5	COOK	97	65
BD0156-07 (BD-01)		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER



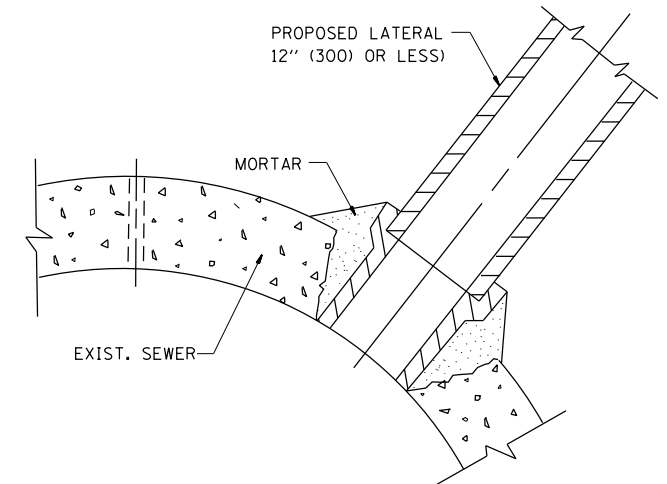
DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

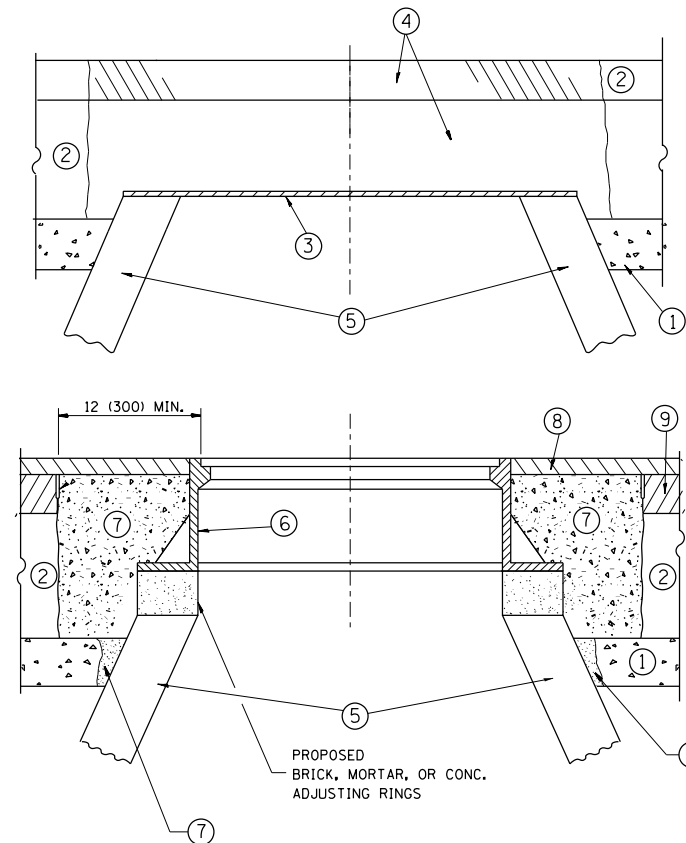
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. SHAH 10-25-94
	PLOT DATE = 4/3/2013	DATE - 07-25-90	REVISED - R. SHAH 06-12-96

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WR5	COOK	97	66
BD500-01 (BD-7)		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

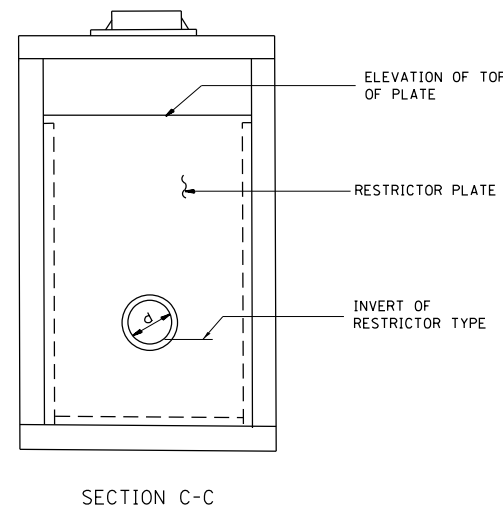
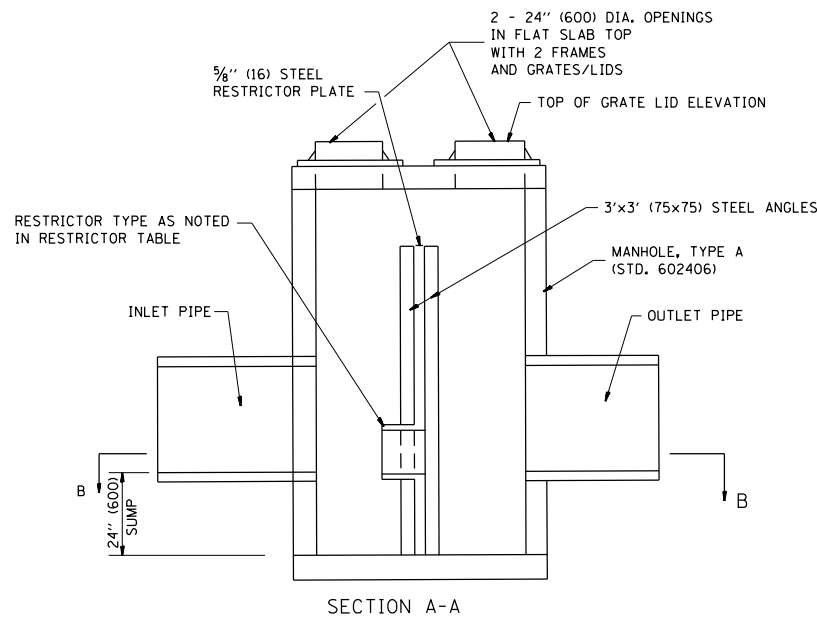
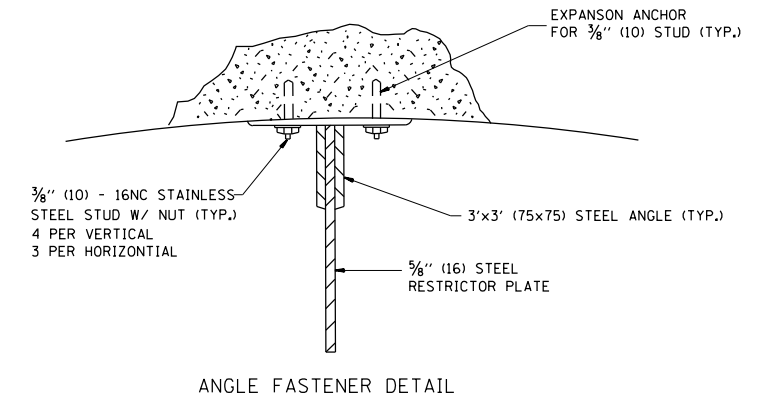
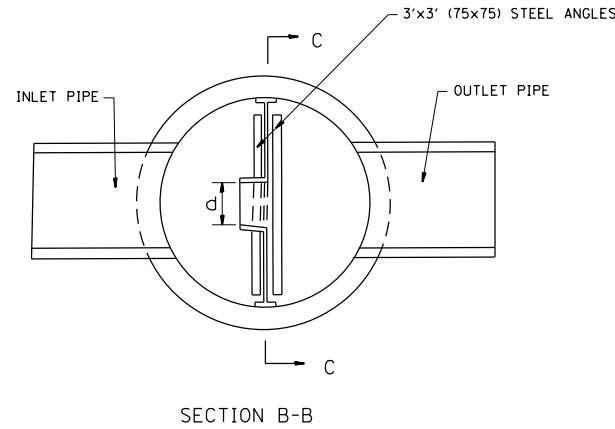
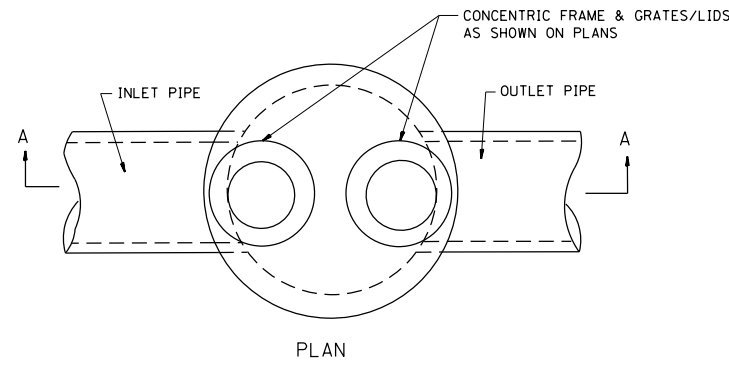
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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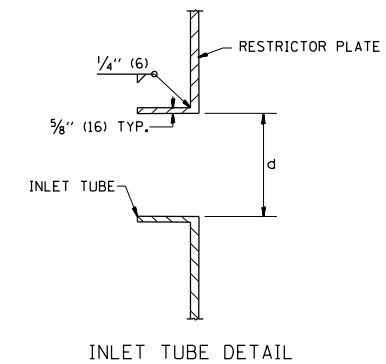
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

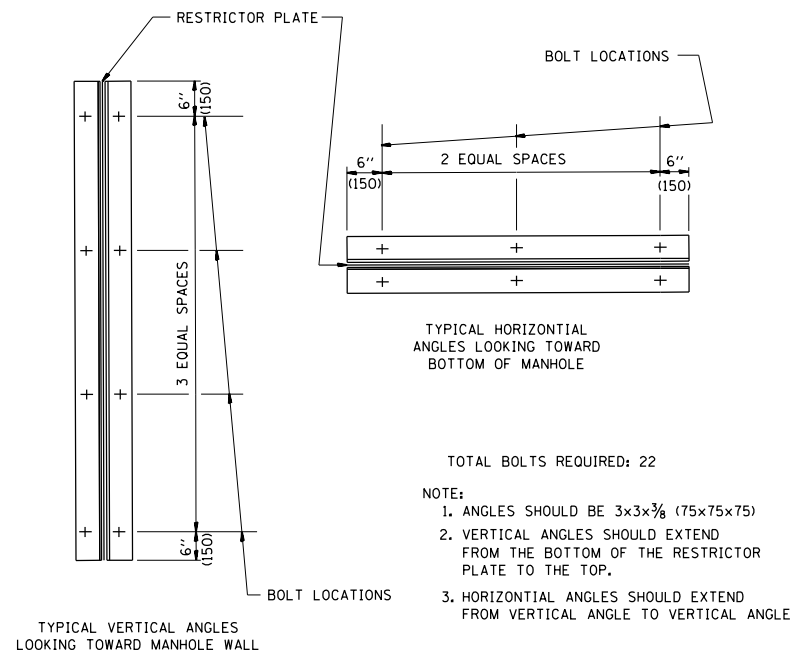
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WR5	COOK	97	67
BD600-03 (BD-8)		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
 2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
 3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
530+56.54	5 FEET	T-8 GRATE	PLATE	22.7 in.	797.20'	799.70'
540+87.48	4 FEET	T-8 GRATE	PLATE	12.8 in.	792.90'	794.94'
530+07.40	5 FEET	T-1F CL	PLATE	29.5 in.	797.10''	800.10'
542+15.80	5 FEET	T-1F CL	PLATE	20 in.	793.08'	795.51'



- TOTAL BOLTS REQUIRED: 22
- NOTE:
1. ANGLES SHOULD BE 3x3x3/8 (75x75x75)
 2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
 3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98
VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES					

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

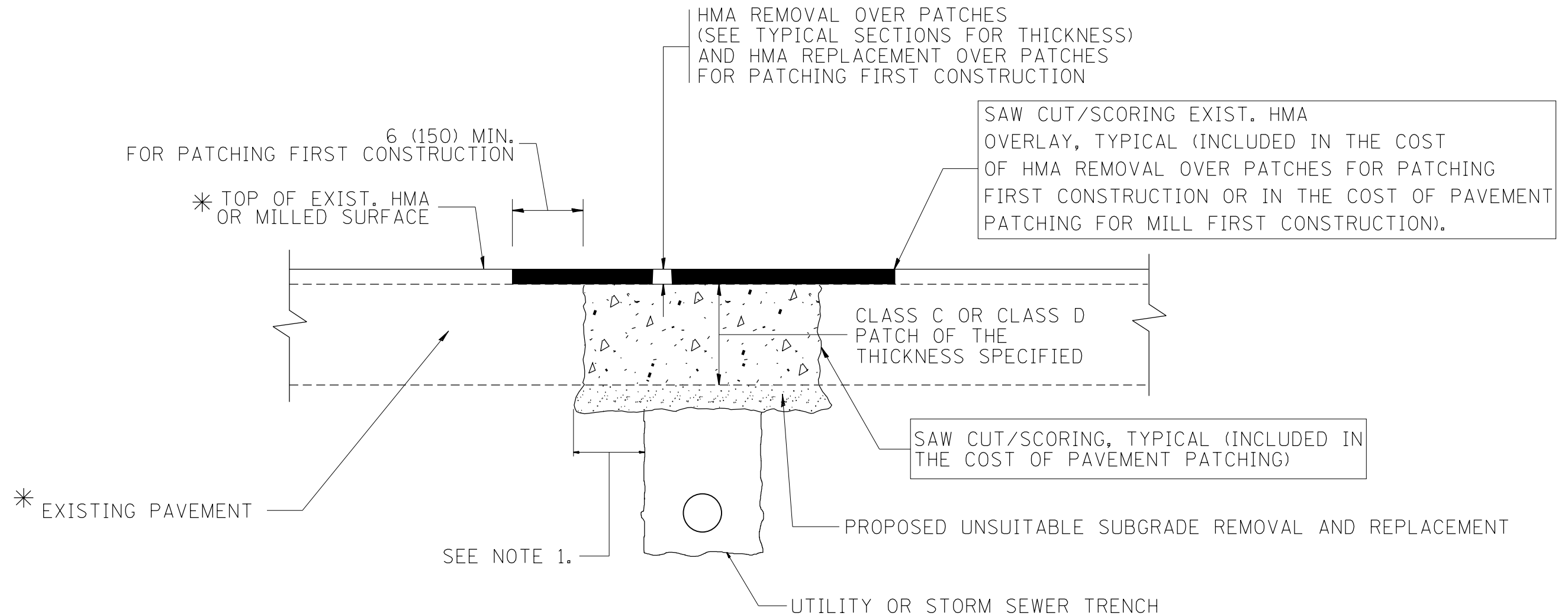
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-08-01
	PLOT DATE = 4/3/2013	DATE - 09-09-94	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MANHOLE WITH
RESTRICTOR PLATE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	68
BD600-04 (BD-12)		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
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	PLOT DATE = 4/3/2013	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WR5	COOK	97	69
BD400-04 (BD-22)			CONTRACT NO. 60N18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

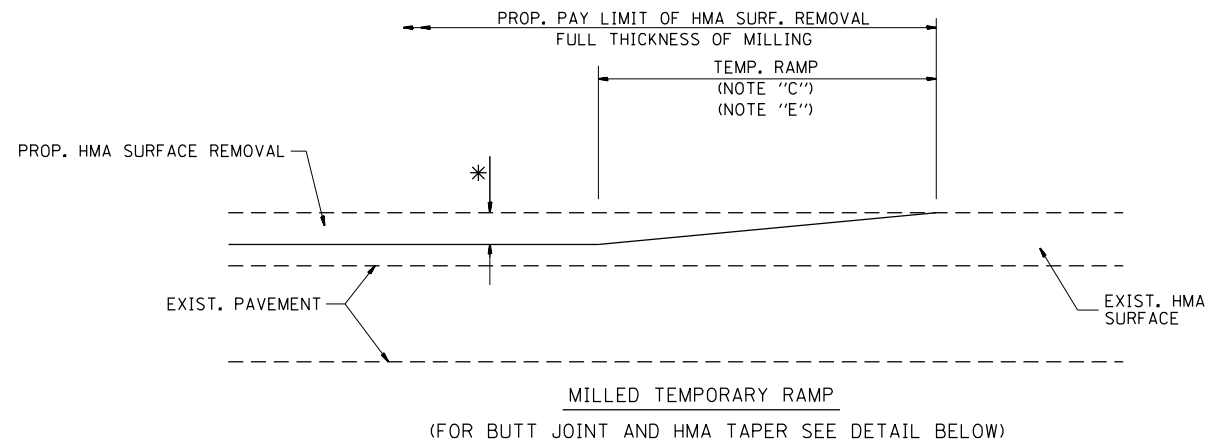
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

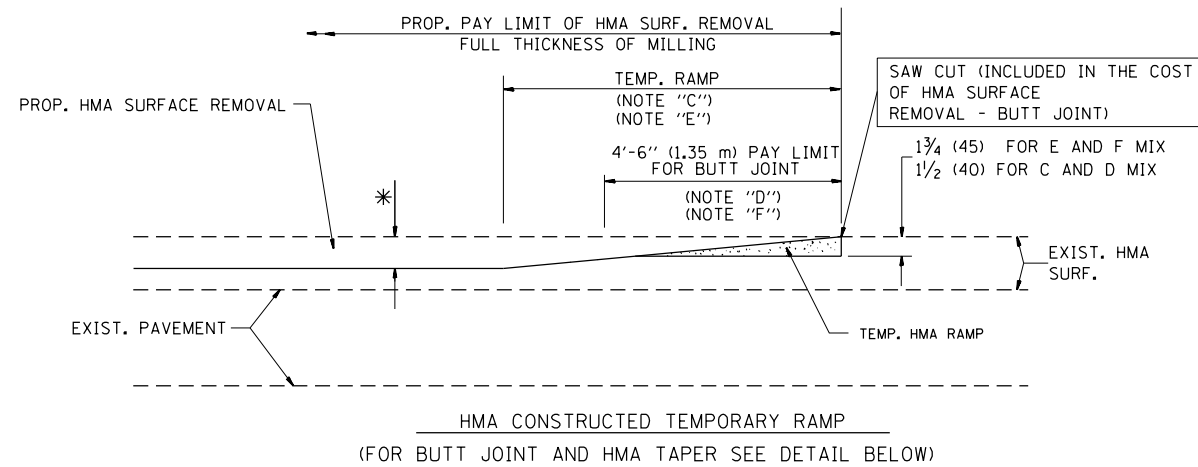
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et:\pwork\pwork\galbenjr\d0304020\Dist	Std.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97					345	7Y-WRS	COOK	97	70
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01					BD600-06 (BD-24)		CONTRACT NO. 60N18		
	PLOT DATE = 4/3/2013	DATE - 03-11-94	REVISED - R. BORO 12-15-09					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

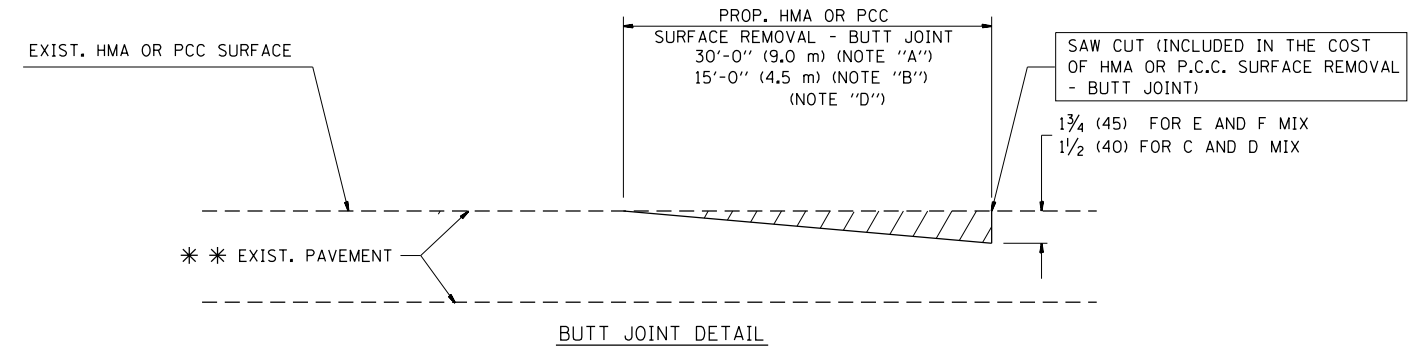


OPTION 1

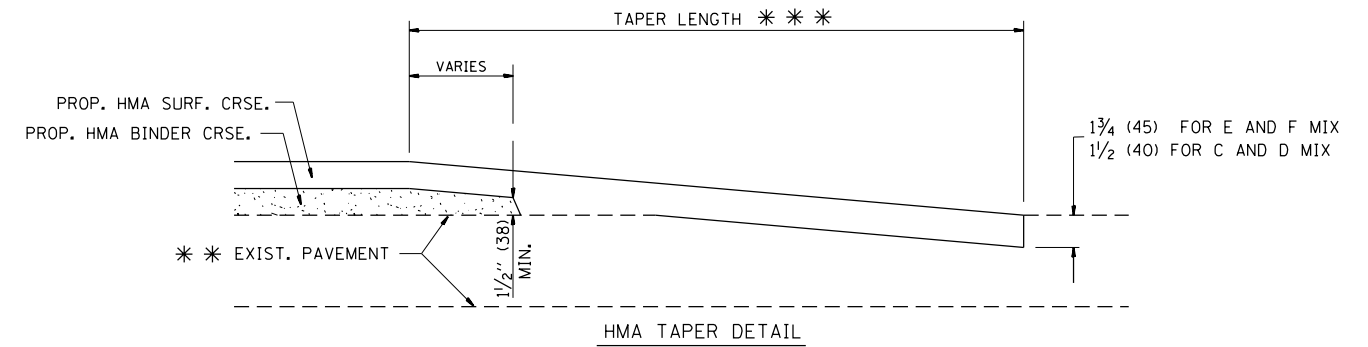


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

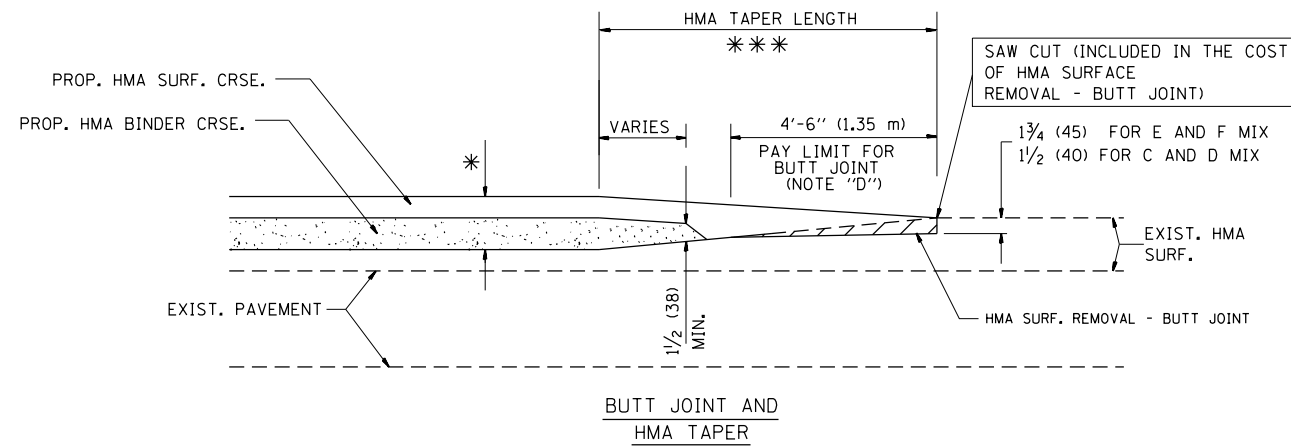
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



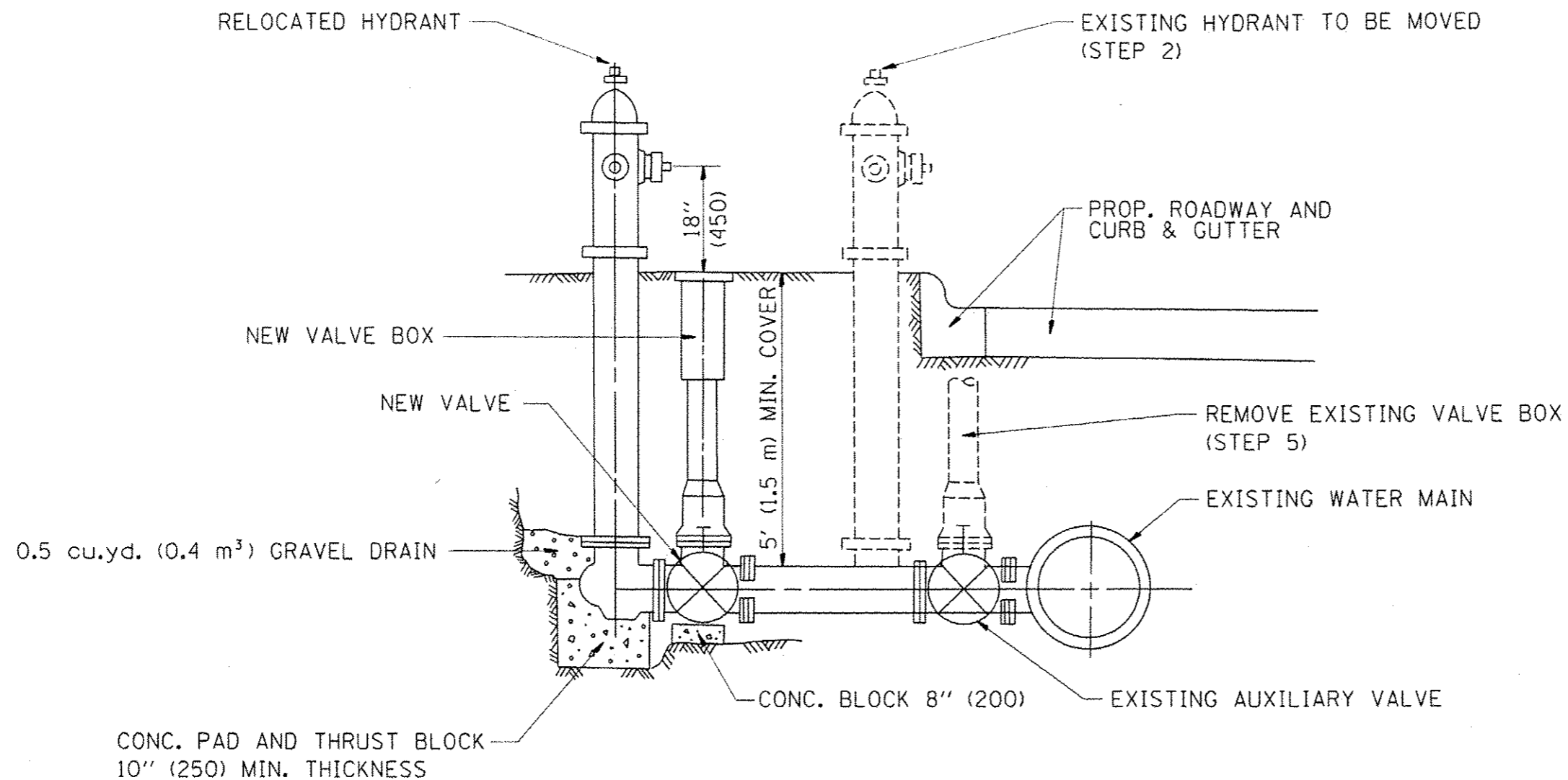
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = galbenjr	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT DATE = 4/3/2013	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WR5	COOK	97	71
BD400-05 BD32		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SEQUENCE OF CONSTRUCTION:

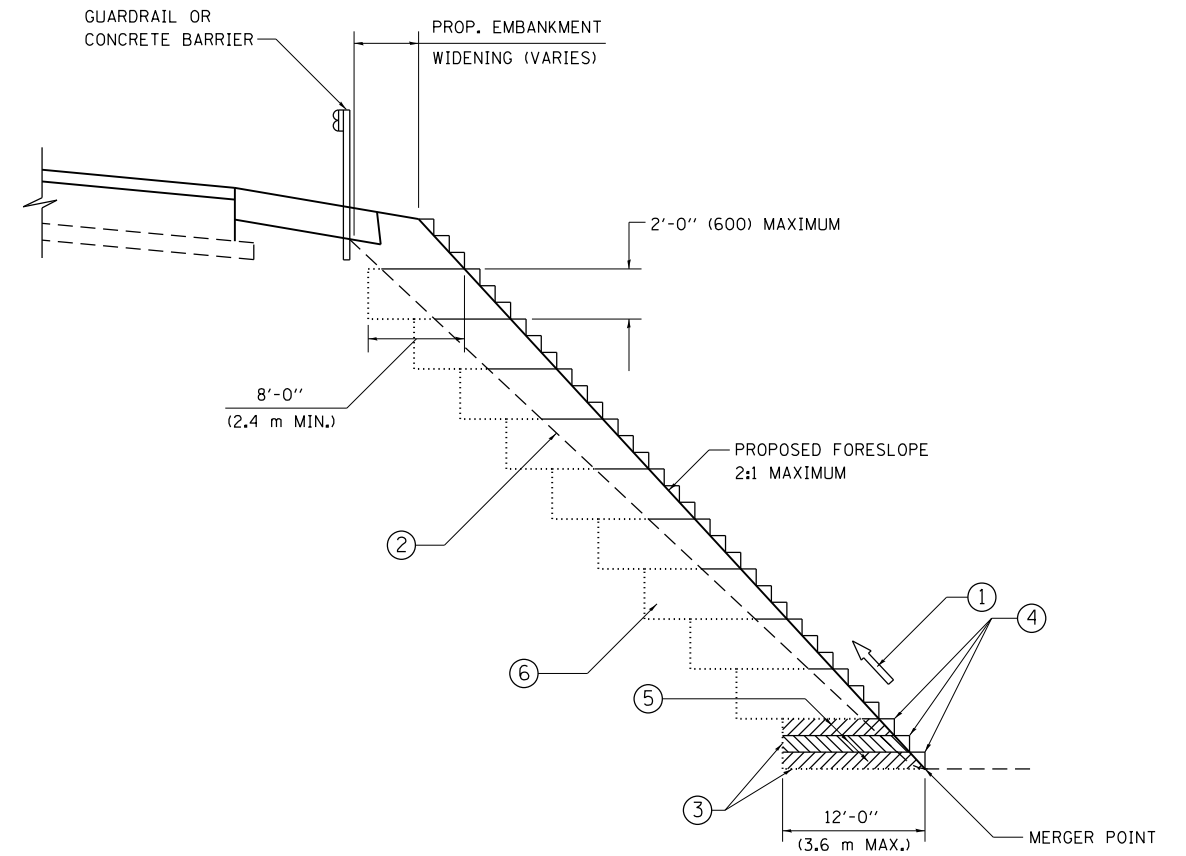
1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME *	USER NAME *	DESIGNED -	REVISED - R, SHAH 09-09-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FIRE HYDRANT TO BE MOVED		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cd:\p\work\p\idost\galbanj\980304020\04	Std.dgn	DRAWN -	REVISED - R, SHAH 10-25-94		345	TY-WRS	COOK	97	71A			
		CHECKED -	REVISED -		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		CONTRACT NO. 60N18	
		DATE -	REVISED -		BD-36		FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT			



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

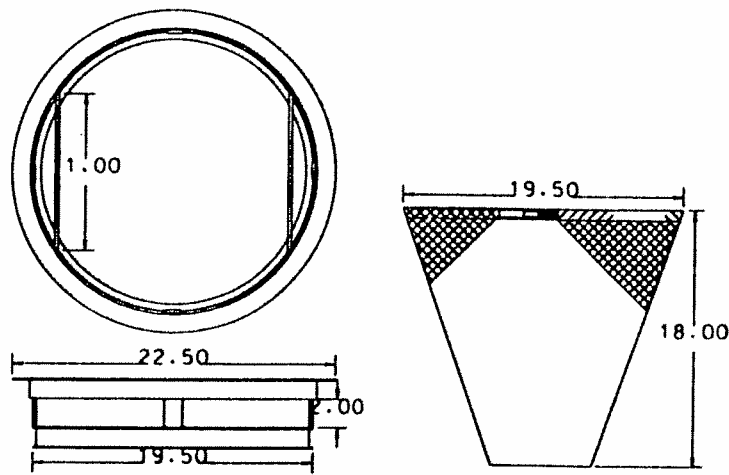
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -
et:\pw\work\pwidot\galbenjr\d0304020\Dis	Std.dgn	DRAWN - CADD	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED - S.E.B.	REVISED -
	PLOT DATE = 4/3/2013	DATE - 06-16-04	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BENCHING DETAIL FOR EMBANKMENT WIDENING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

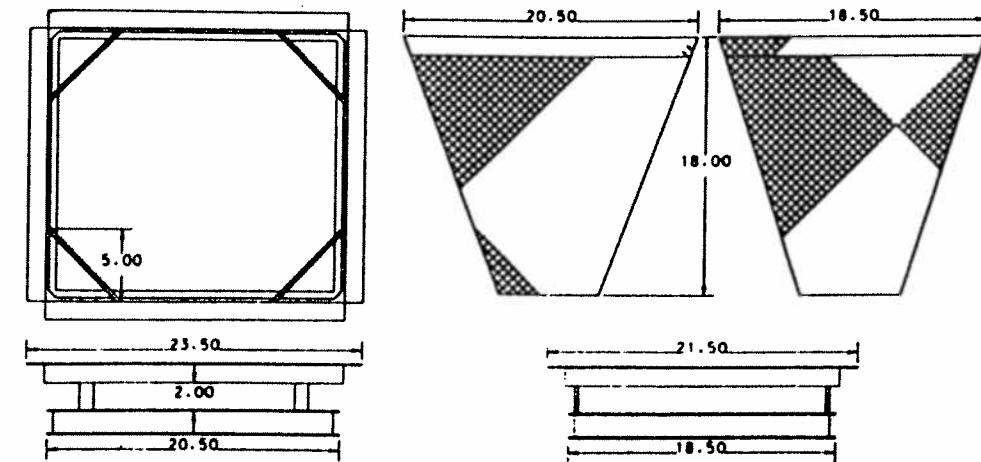
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345	7Y-WR5	COOK	97	72
BD-51			CONTRACT NO. 60N18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TYPE I INLET FILTER

NOTE:

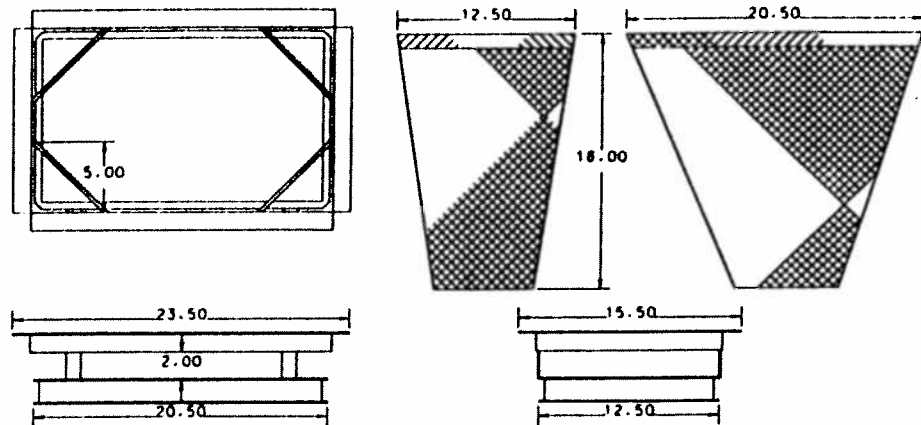
FRAME: TOP RING SHALL BE FABRICATED FROM 1-1/4" x 1-1/4" x 1/8" ANGLE.
 BASE RING SHALL BE FABRICATED FROM 1-1/2" x 1/2" x 1/8" CHANNEL. HANDLES
 AND SUSPENSION BRACKETS SHALL BE FABRICATED FROM 1-1/4" x 1/4" FLAT STOCK.



TYPE 24 INLET FILTER

NOTE:

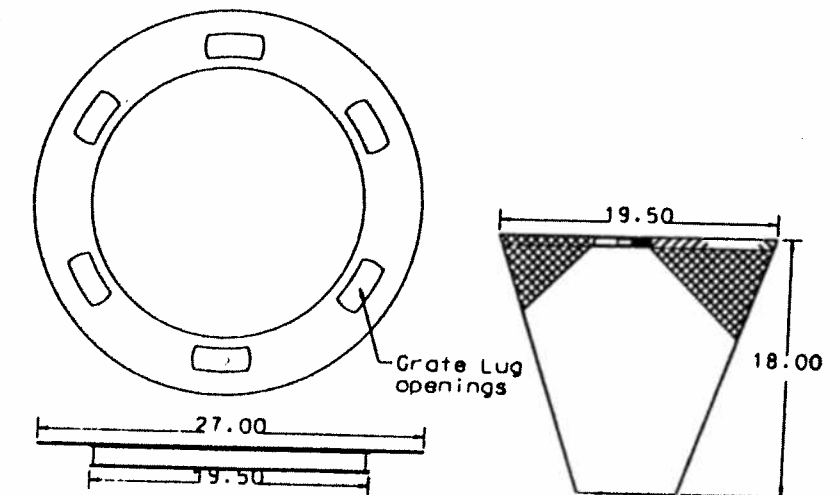
FRAME: TOP PIECE SHALL BE FABRICATED FROM 1-1/4" x 1-1/4" x 1/8" ANGLE.
 BASE PIECE SHALL BE FABRICATED FROM 1-1/2" x 1-1/2" x 1/8" CHANNEL.
 HANDLES AND SUSPENSION BRACKETS SHALL BE FABRICATED FROM 1-1/4" x 1/4"
 FLAT STOCK.



TYPE 23 INLET FILTER

NOTE:

FRAME: TOP PIECE SHALL BE FABRICATED FROM 1-1/4" x 1-1/4" x 1/8" ANGLE.
 BASE PIECE SHALL BE FABRICATED FROM 1-1/2" x 1/2" x 1/8" CHANNEL. HANDLES
 AND SUSPENSION BRACKETS SHALL BE FABRICATED FROM 1-1/4" x 1/4" FLAT STOCK.

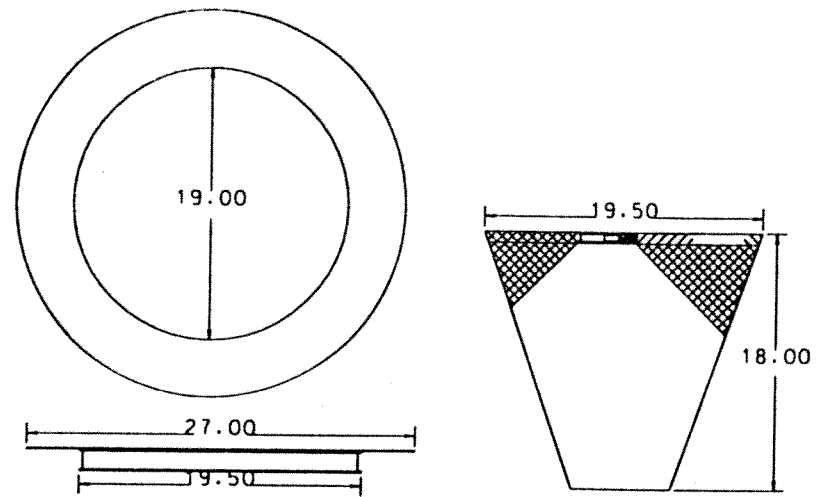


TYPE 8 INLET FILTER

NOTE:

FRAME: FLANGE SHALL BE FABRICATED FROM 1/8" FLAT STOCK.
 BASE RING SHALL BE FABRICATED FROM 1-1/2" x 1/2" x 1/8"
 CHANNEL.

FILE NAME *	USER NAME * galbany	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INLET FILTER DETAILS			P.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pe_work\pwsdot\galbany\ad384828\PI5	488-Design.dgn	DRAWN -	REVISED -					345	TY-WRS	COOK	97	73
MODEL NAME #	PLOT SCALE * 100.0000 1/ in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60N18				
	PLOT DATE * 2/1/2013	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

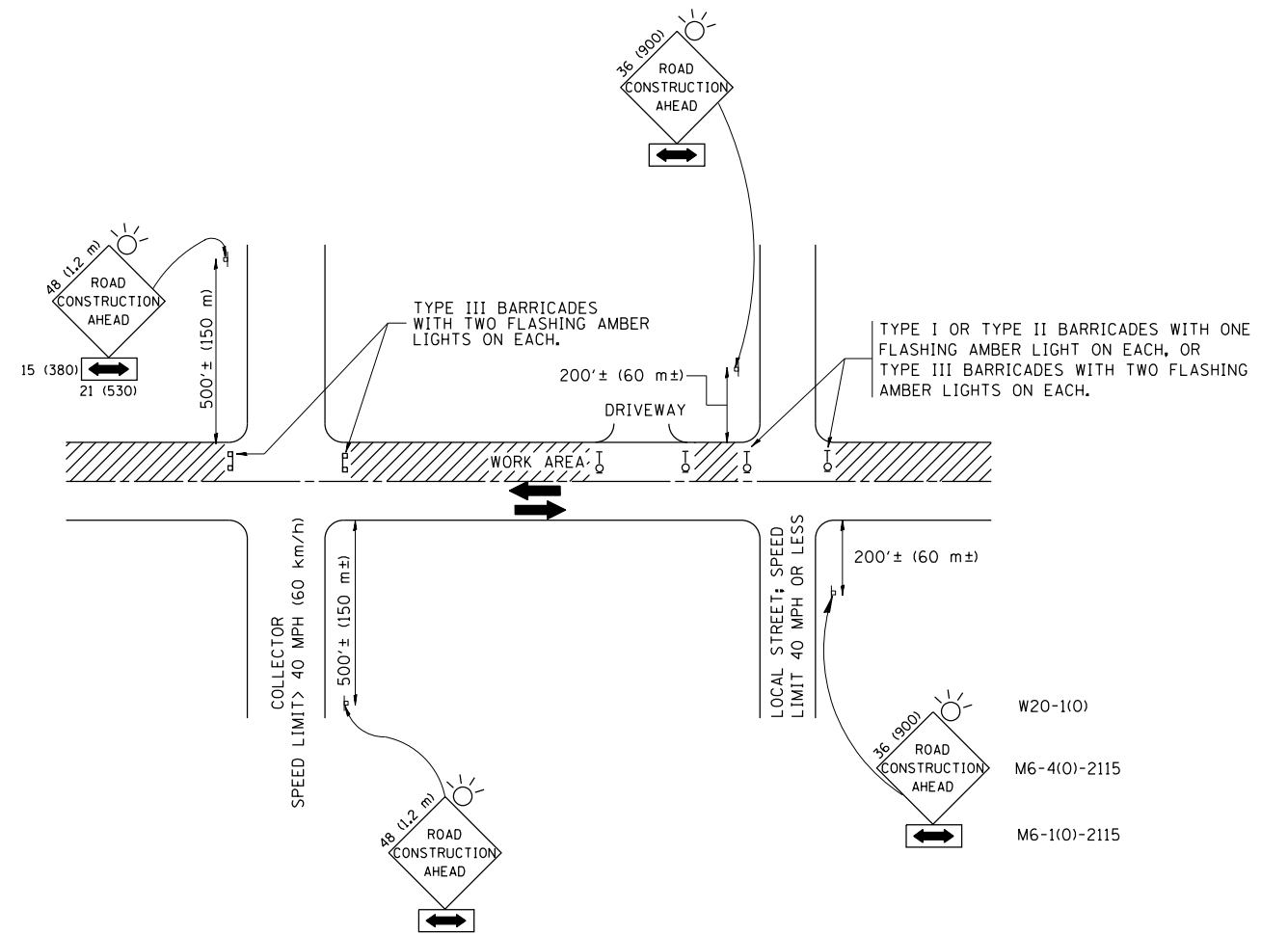


CATCH BASIN & INLET FILTER
(TO BE INSTALLED ON BURIED STRUCTURES WHILE PAVING)

NOTE:

FRAME: FLANGE SHALL BE FABRICATED FROM 1/8" FLAT STOCK.
BASE RING SHALL BE FABRICATED FROM 1-1/2" x 1/2" x 1/8"
CHANNEL.

FILE NAME *	USER NAME * galbanjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INLET FILTER DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\dot\galbanjr\0304020\PI5	408-Design\dgn	DRAWN -	REVISED -					345	7Y-WRS	COOK	97	74
MODEL NAME *	PLOT SCALE * 100.0000 1' = in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 60N18				
	PLOT DATE * 2/11/2013	DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

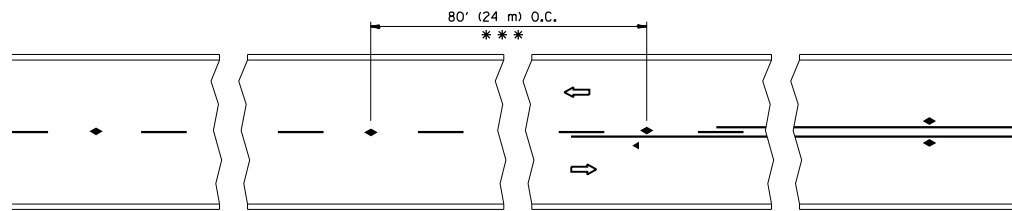
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et:\pwork\pwork\galbenjr\d0304020\Dis	Std.dgn	DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 4/3/2013	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

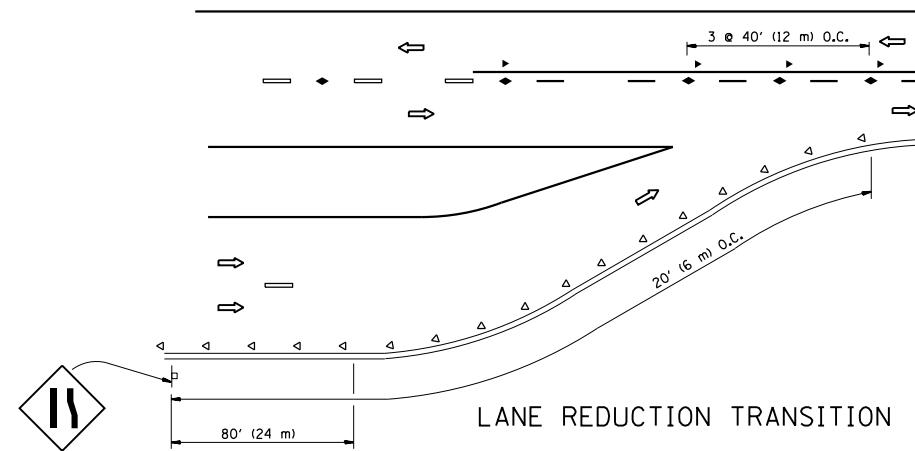
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60N18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

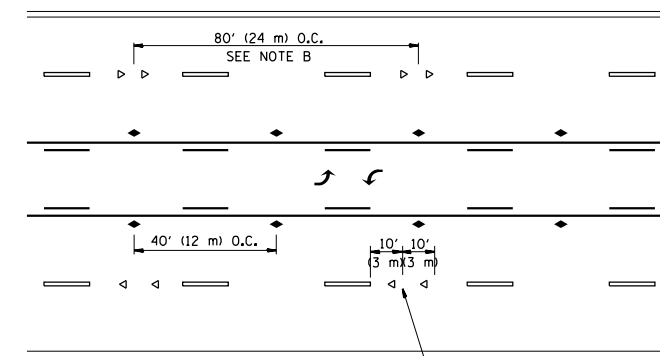


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

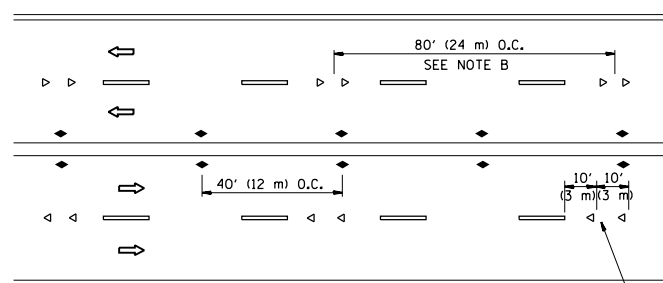
TWO-LANE/TWO-WAY



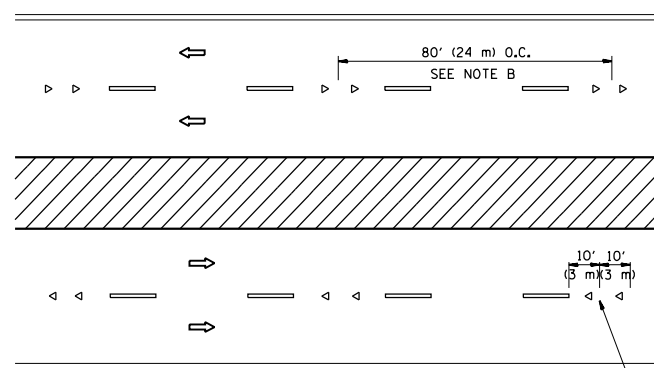
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

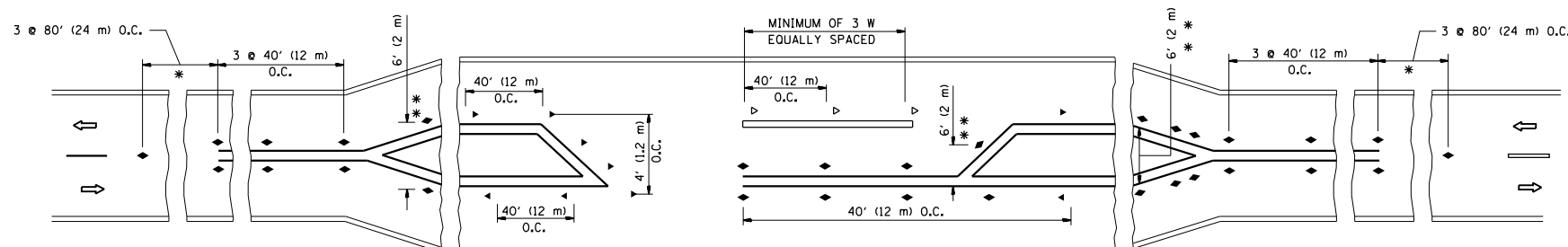
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

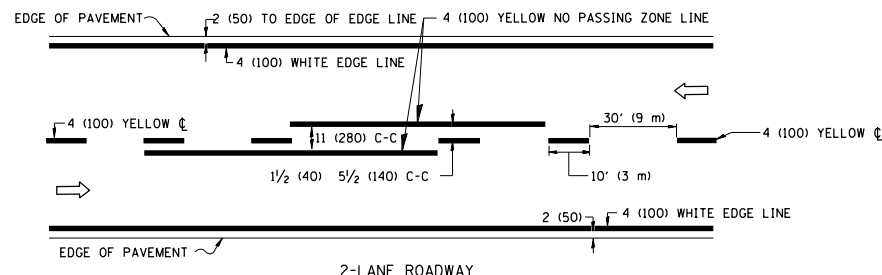
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 4/3/2013	DATE -	REVISED - C. JUCIUS 09-09-09

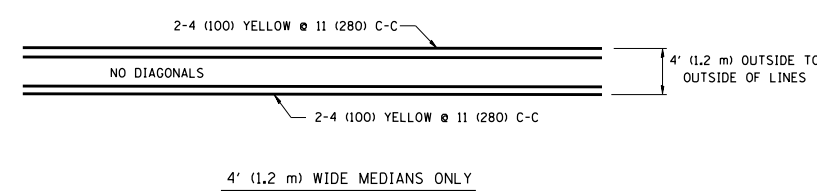
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

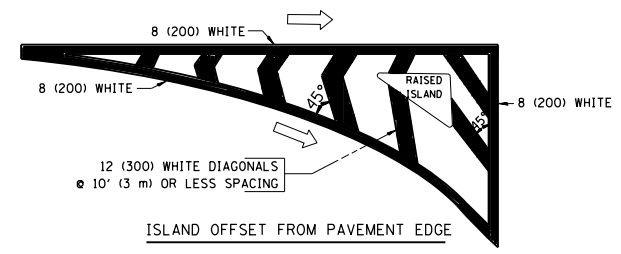
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WR5	COOK	97	76
TC-11		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



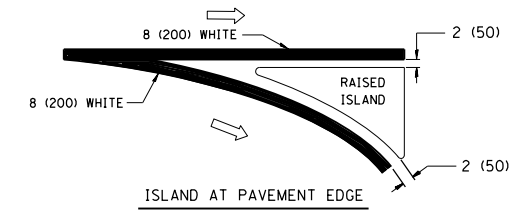
2-LANE ROADWAY



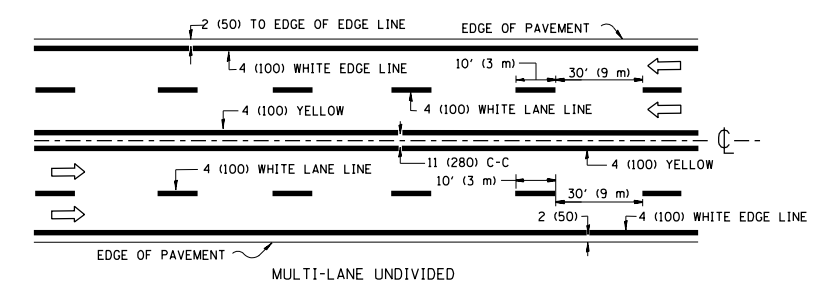
4' (1.2 m) WIDE MEDIANS ONLY



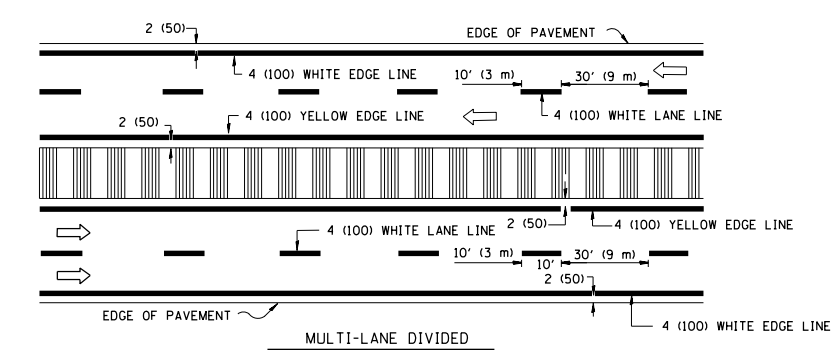
ISLAND OFFSET FROM PAVEMENT EDGE



TYPICAL ISLAND MARKING



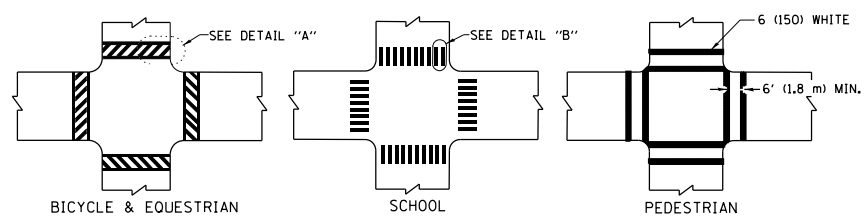
MULTI-LANE UNDIVIDED



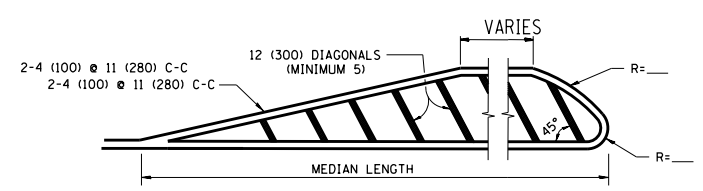
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



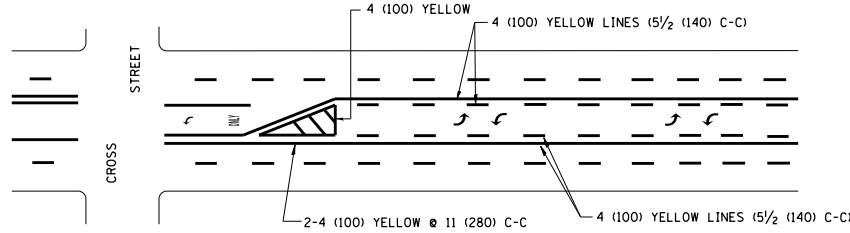
TYPICAL CROSSWALK MARKING



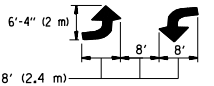
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

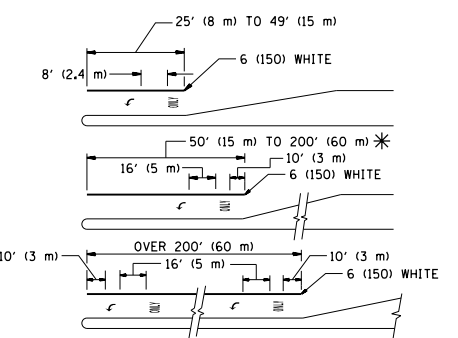


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

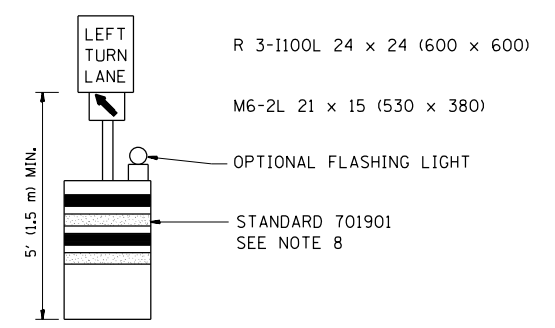
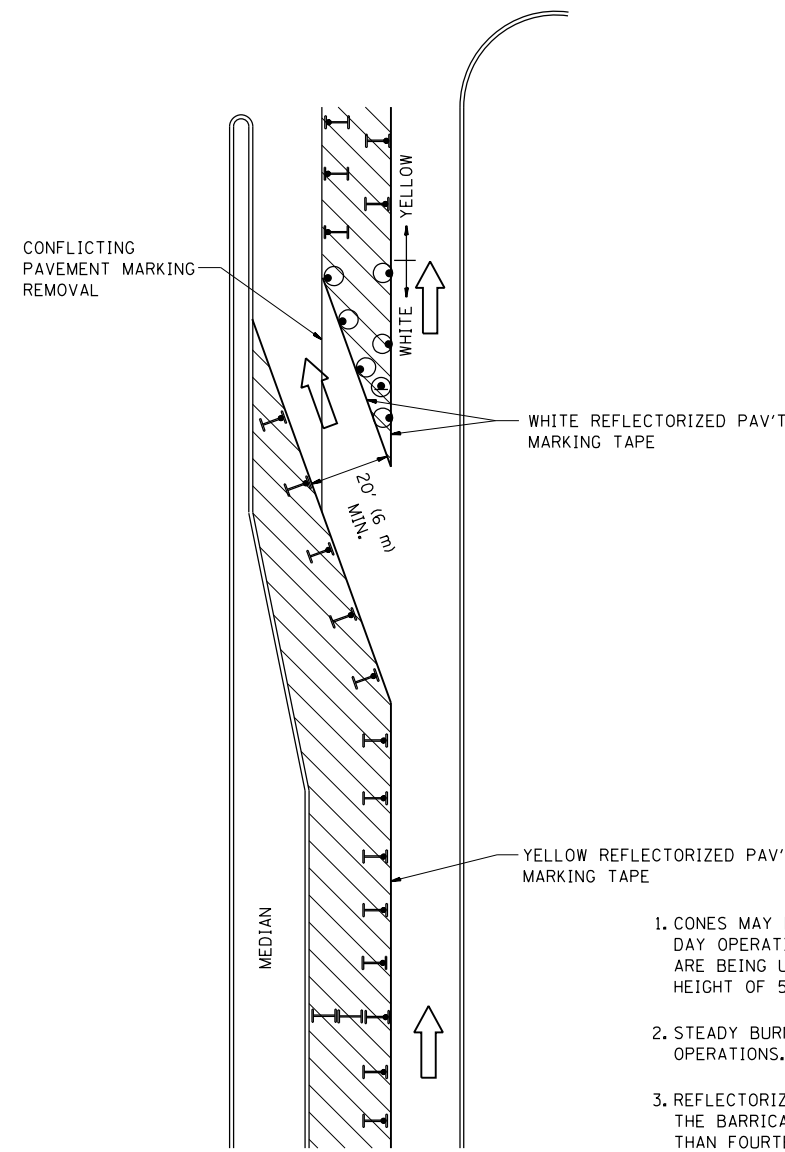
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = galbenjr	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
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	PLOT DATE = 4/3/2013	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	77
TC-13		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

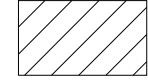







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

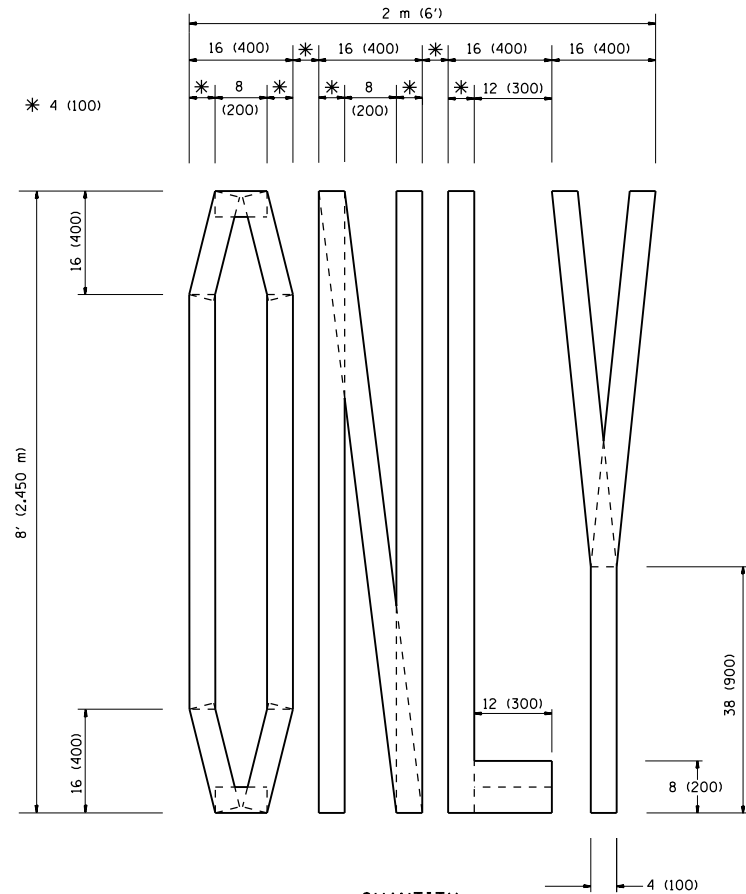
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et:\pwork\pwork\galbenjr\d0304020\Dist	Std.dgn	REVISED - A. HOUSEH 11-07-95	REVISED -
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED -
	PLOT DATE = 4/3/2013	REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

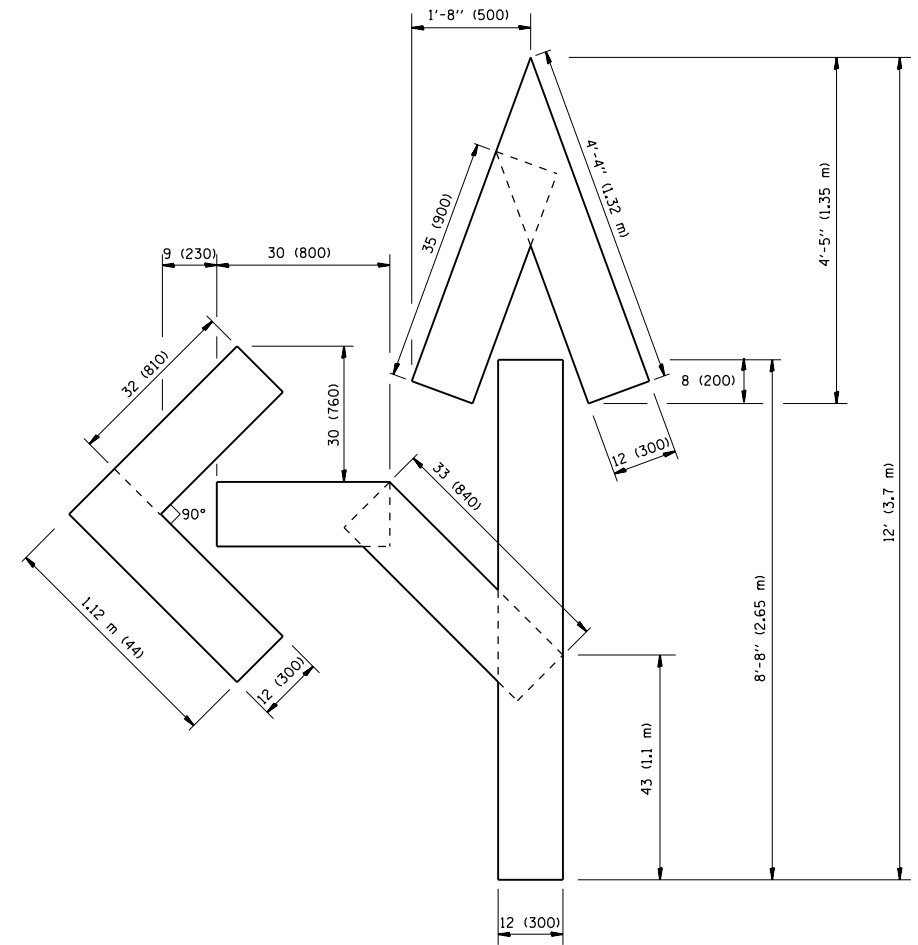
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

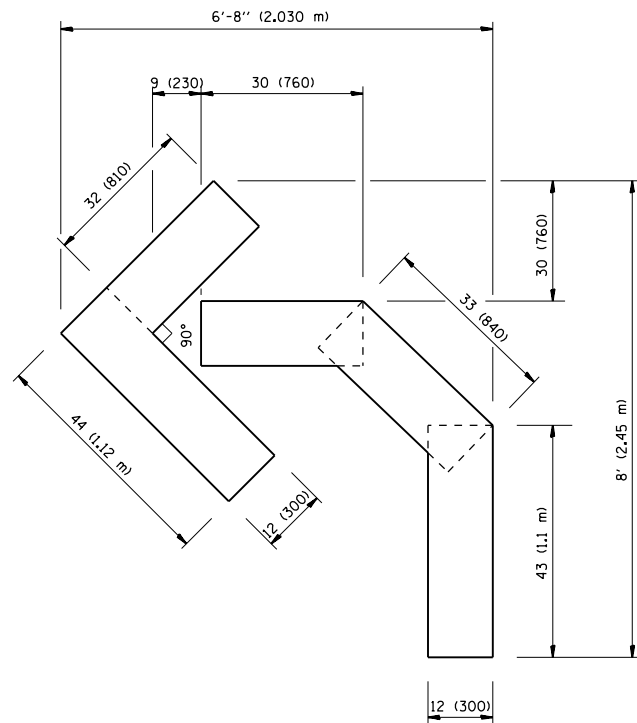
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	7Y-WRS	COOK	97	78
TC-14		CONTRACT NO. 60N18		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
et:\pw\work\p\idot\galbenjr\d0304020\Dis	Std.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 4/3/2013	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

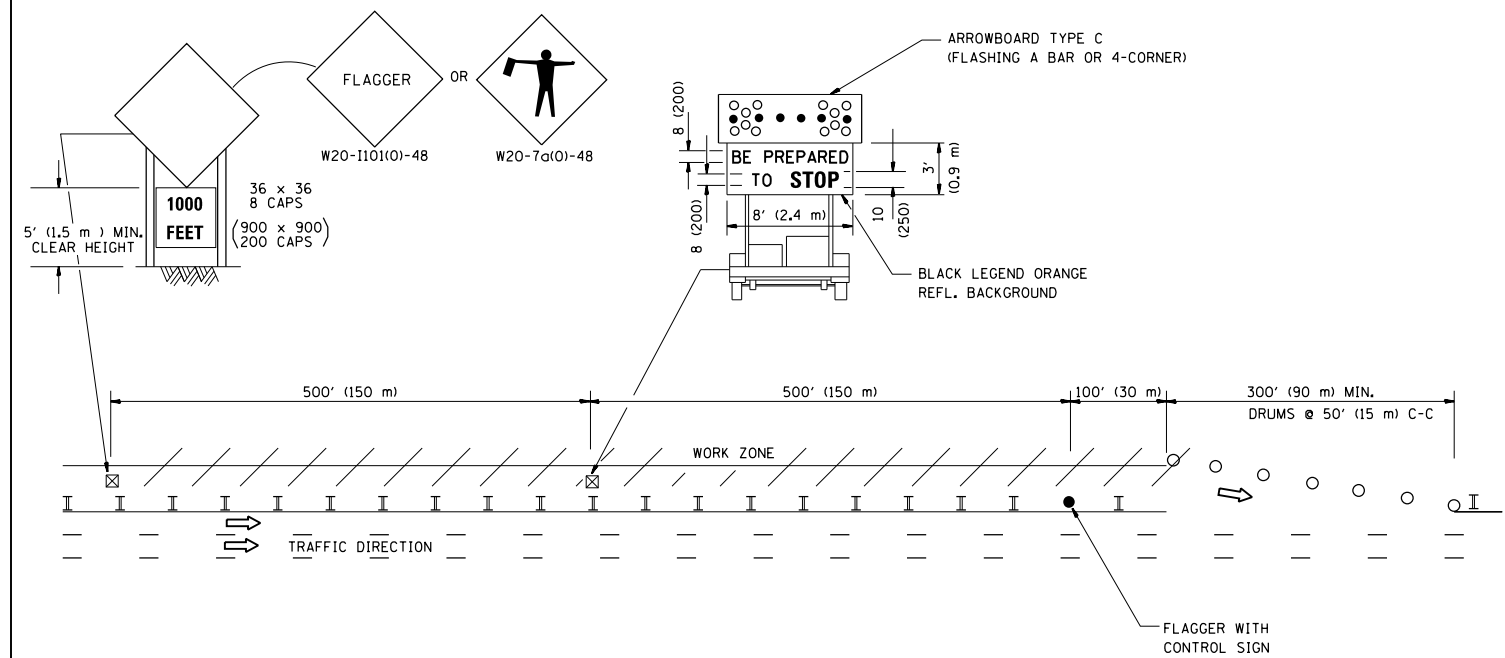
PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

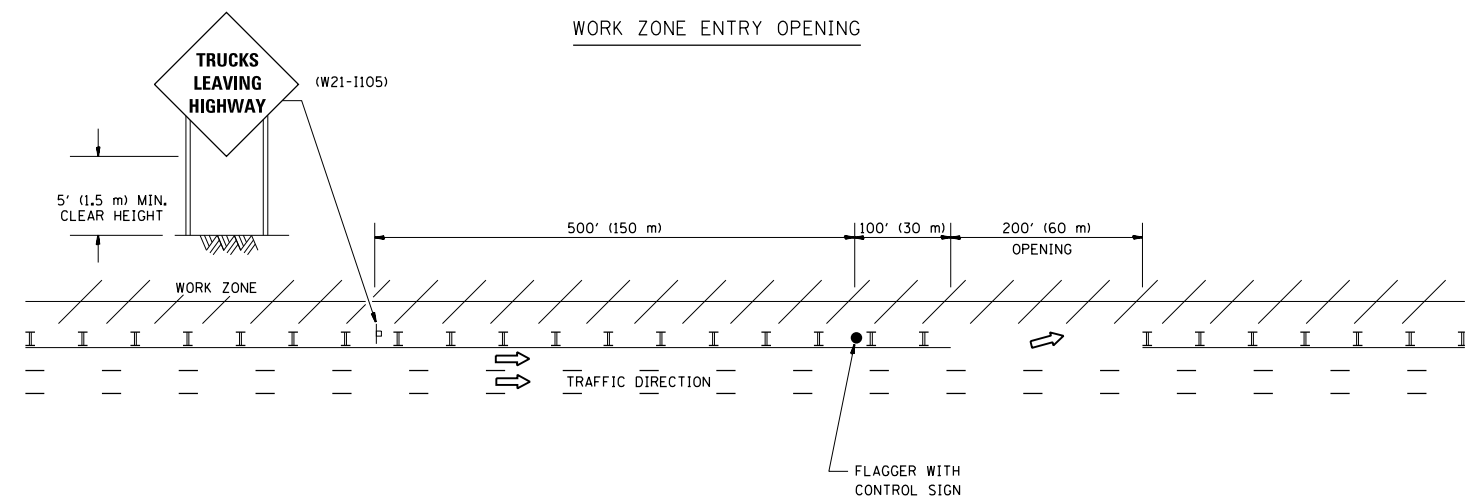
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TC-16		CONTRACT NO. 60N18		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

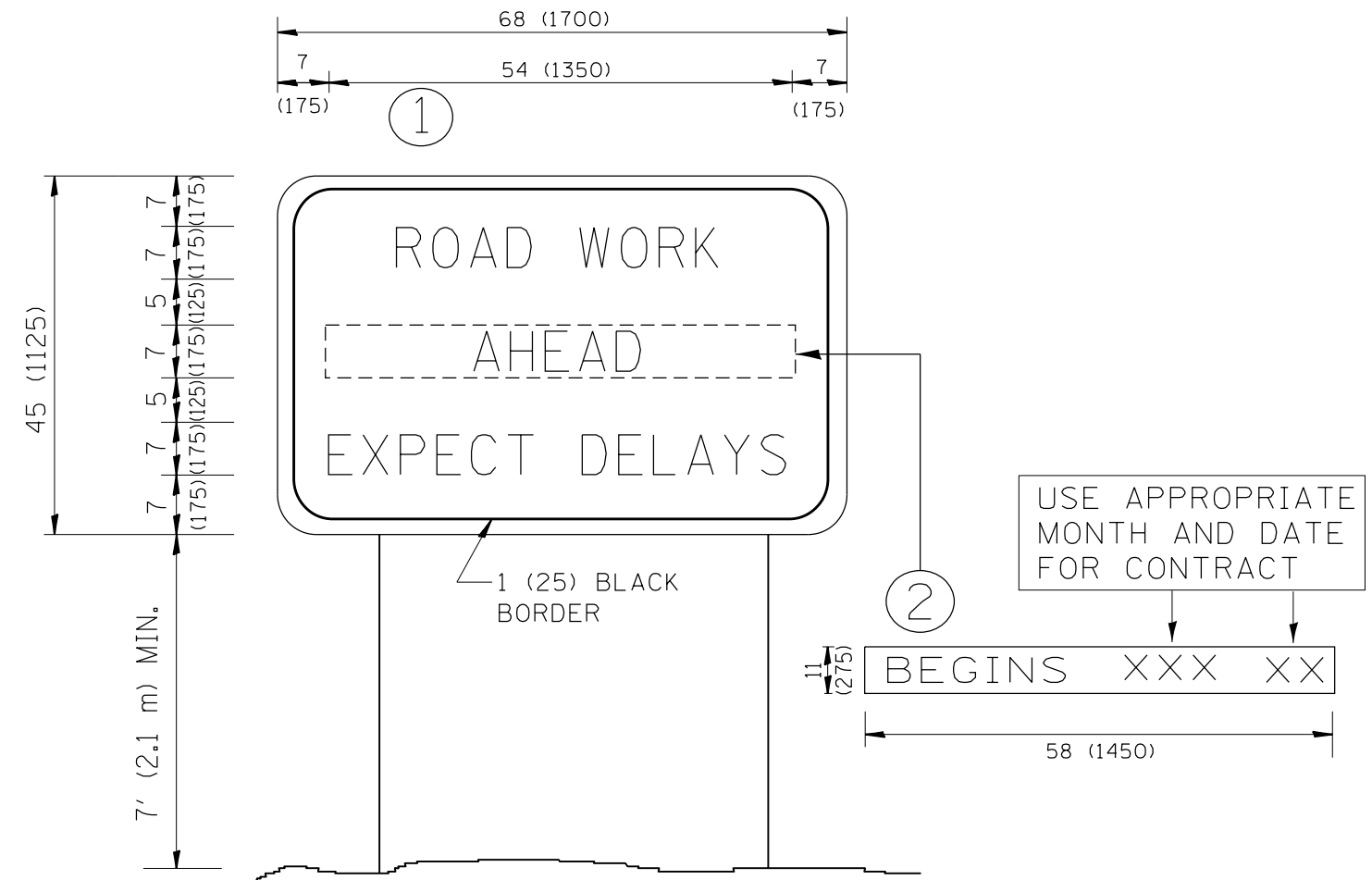
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 4/3/2013	DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-18			CONTRACT NO. 60N18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

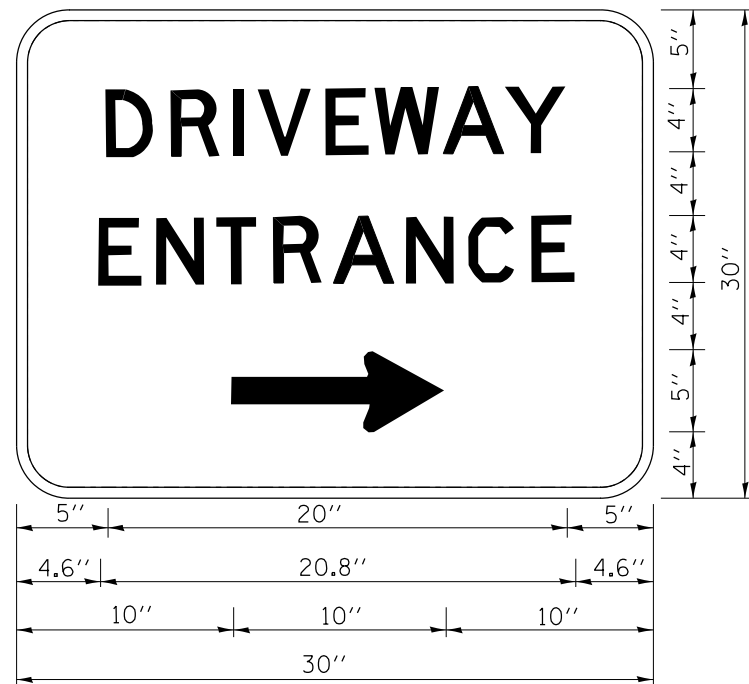


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 60N18		
	PLOT DATE = 4/3/2013	DATE -	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED - C. JUCIUS 02-15-07
et:\pwork\pwork\galbenjr\d0304020\Dist	Std.dgn	DRAWN -	REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

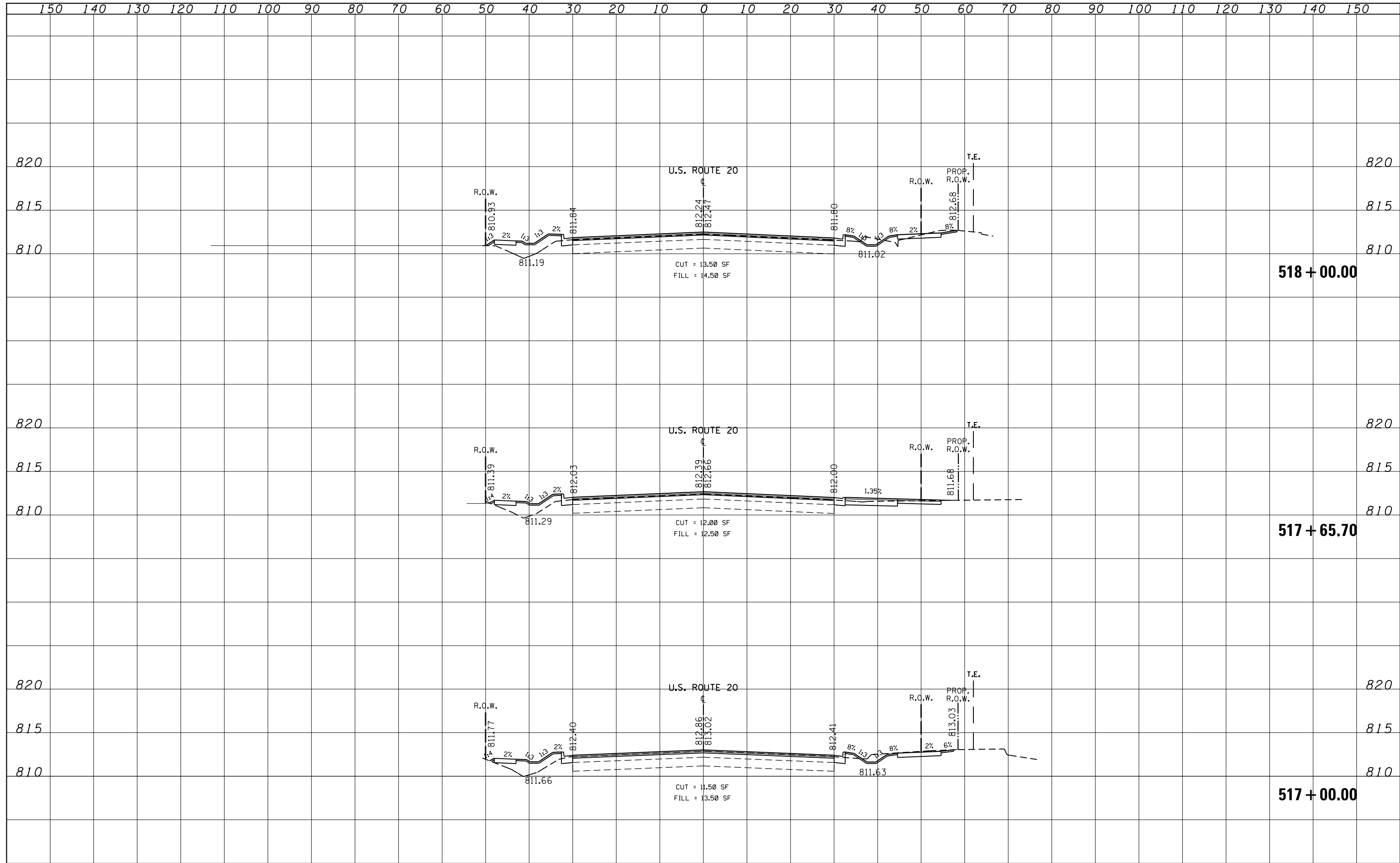
DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-26			CONTRACT NO. 60N18	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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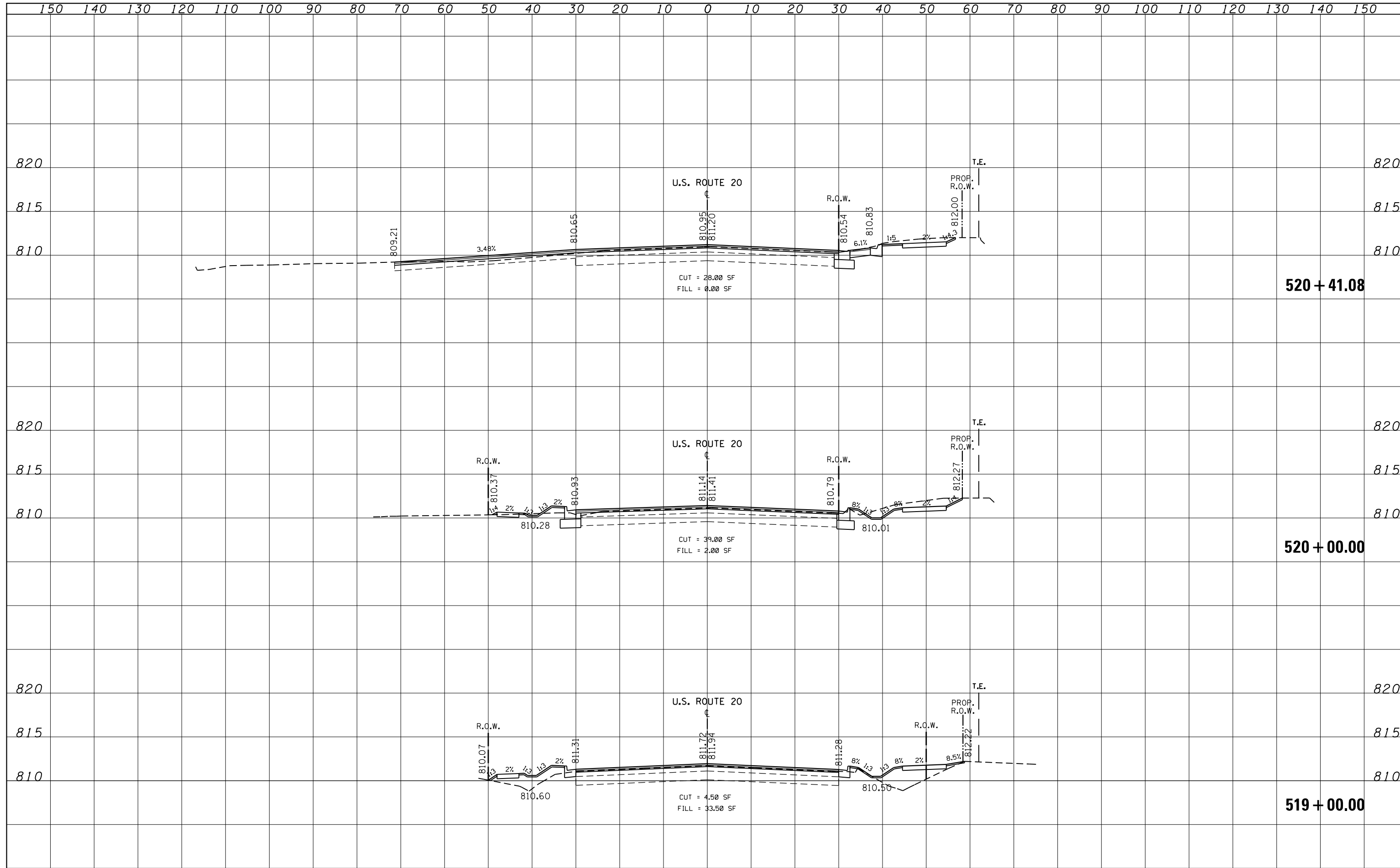
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MODELNAME	PLOT DATE = 4/3/2013	DATE -	REVISED -												
											CONTRACT NO. 60N18 ILLINOIS FED. AID PROJECT				

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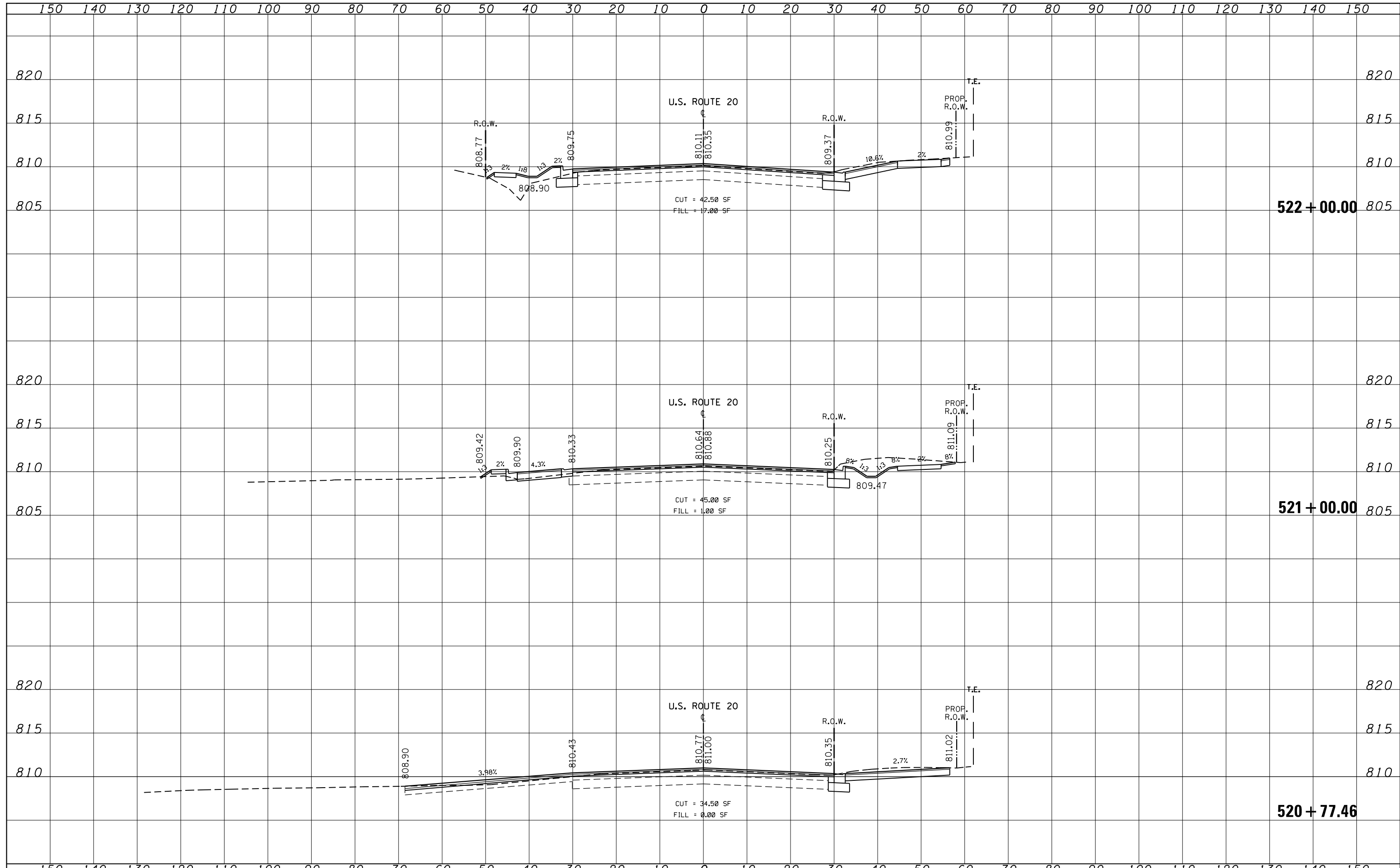
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MODELNAME	PLOT DATE = 4/3/2013	DATE -	REVISED -												
											CONTRACT NO. 60N18 ILLINOIS FED. AID PROJECT				

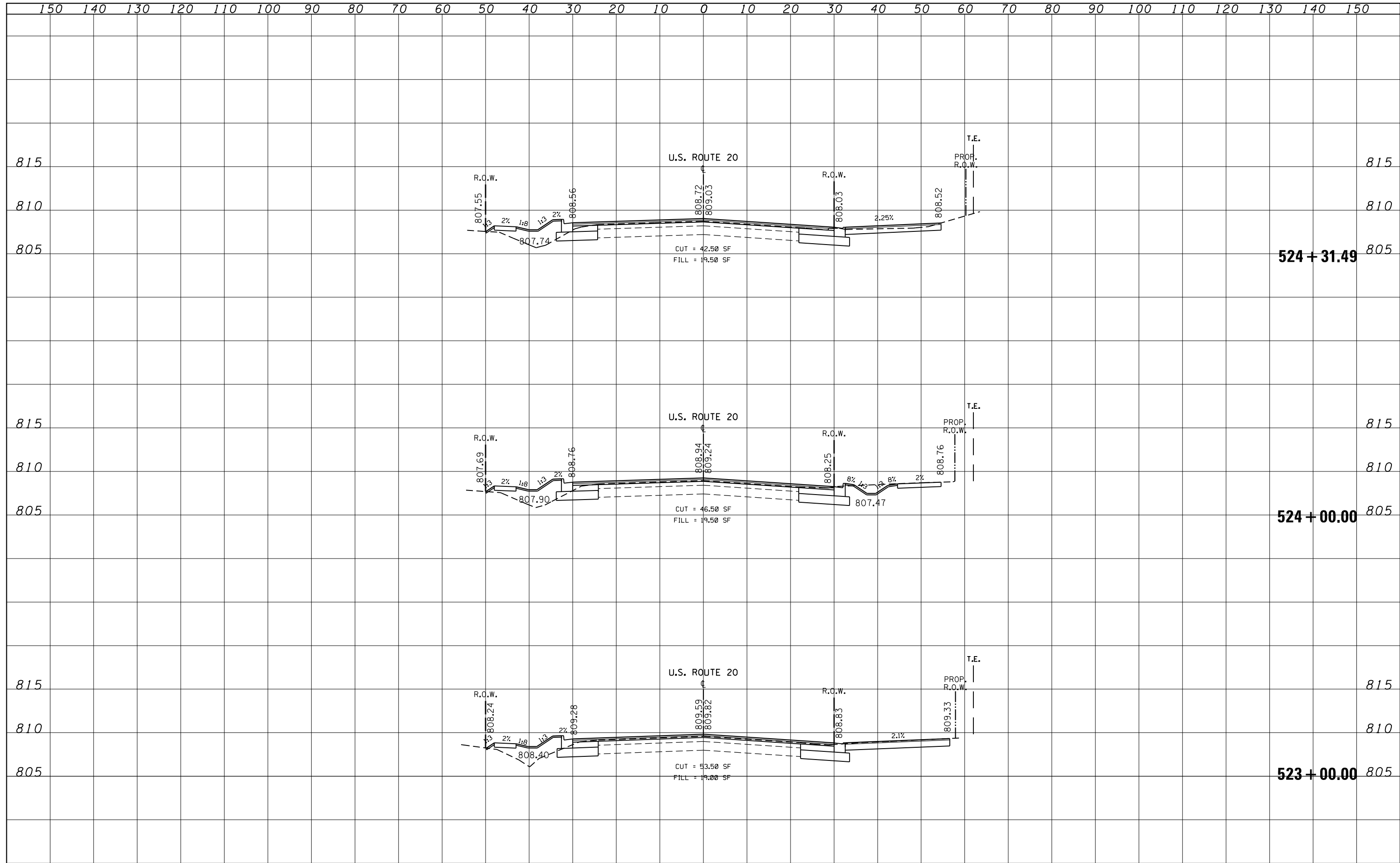
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NOTE BOOK	PLOTTED
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	AREAS
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ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
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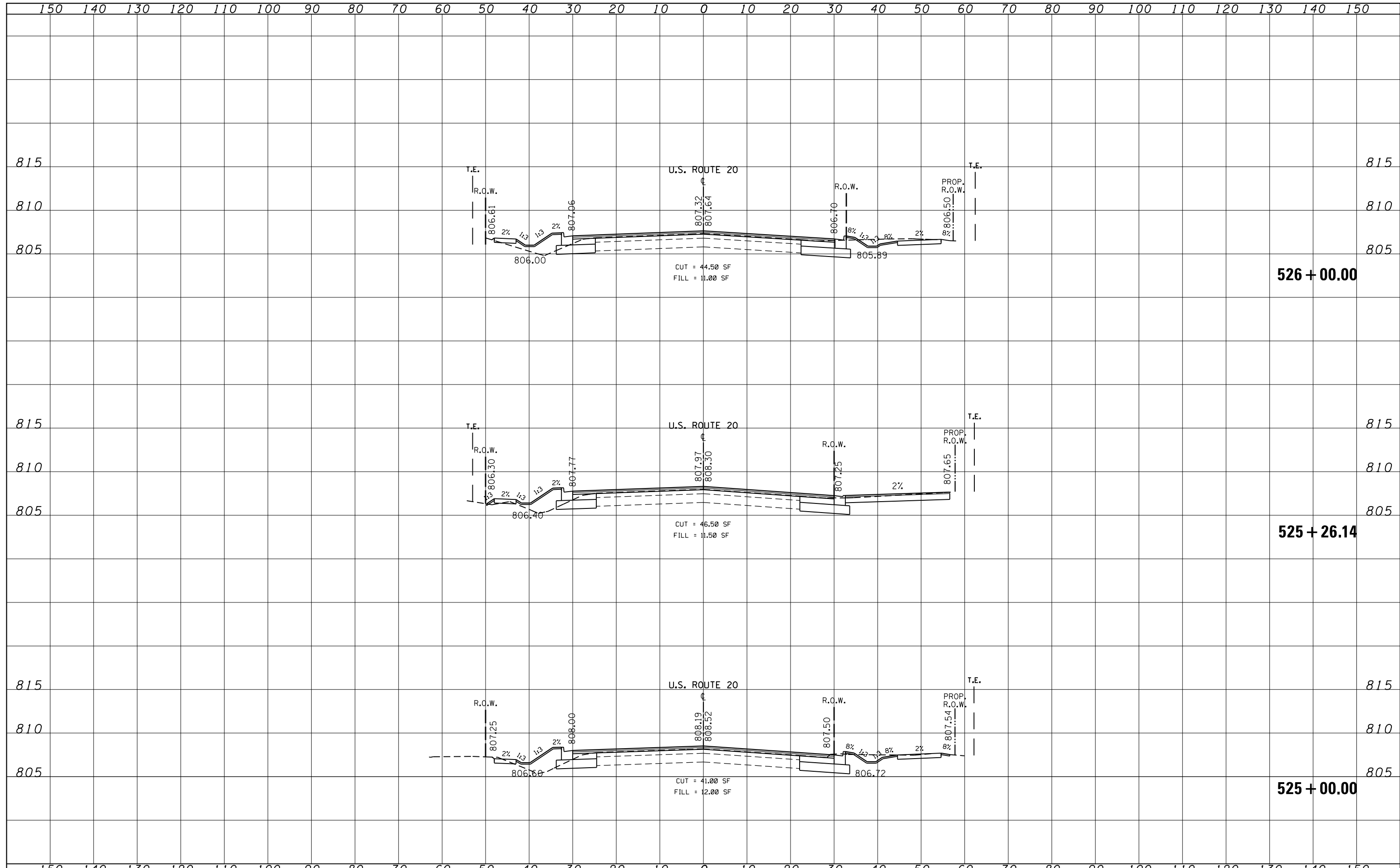
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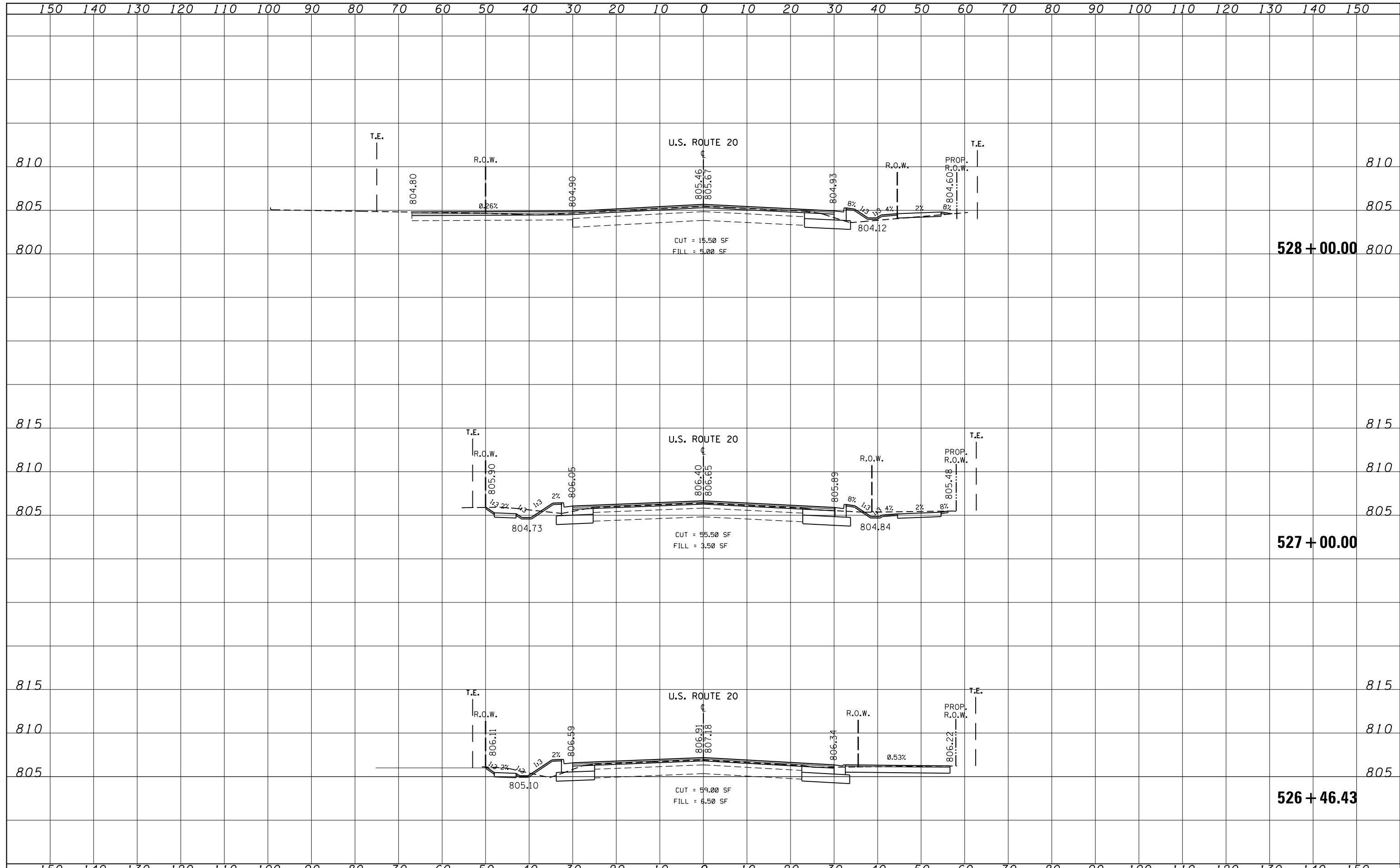
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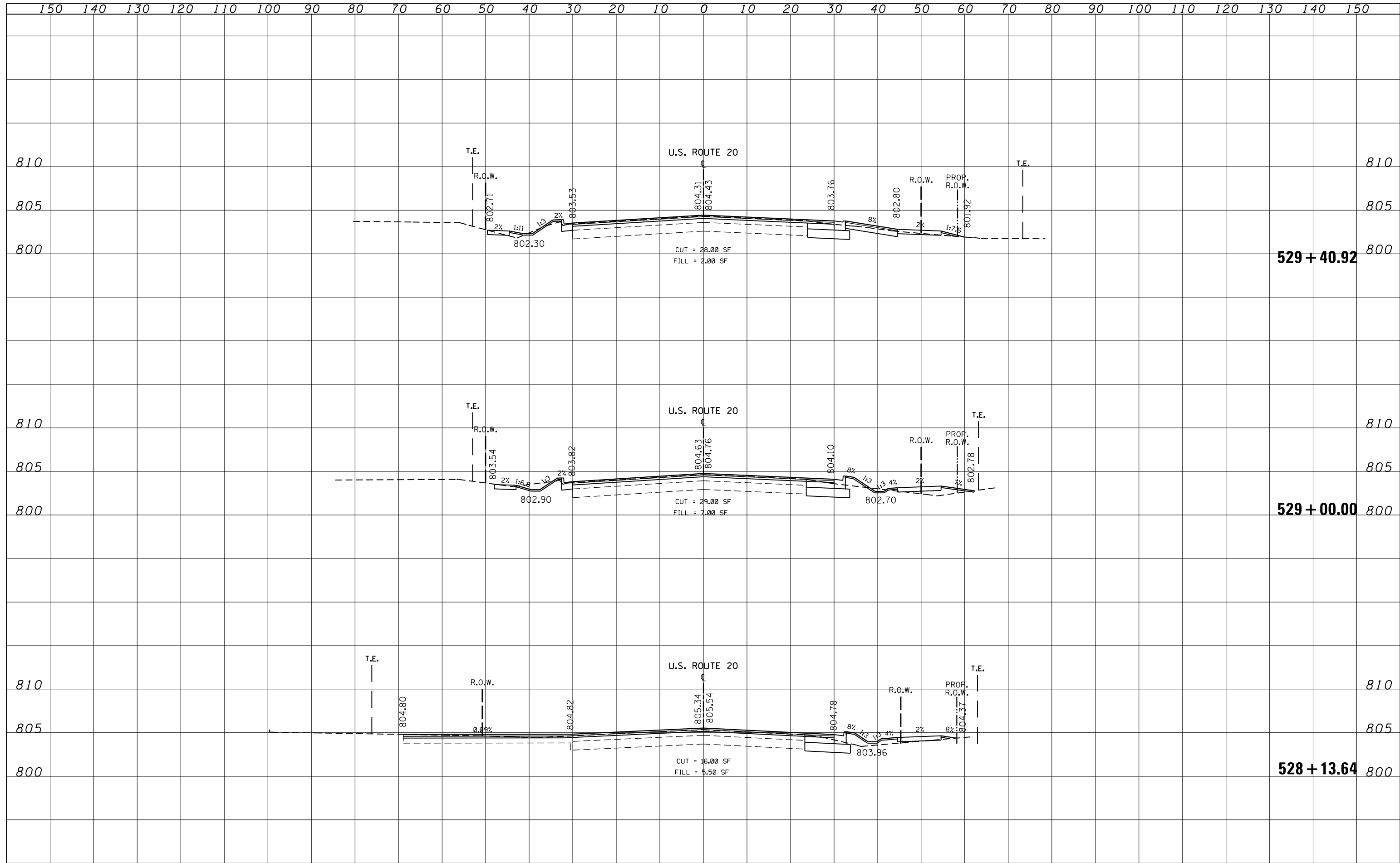
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MODELNAME	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 526+46.43 TO STA. 528+00.00	ILLINOIS FED. AID PROJECT		

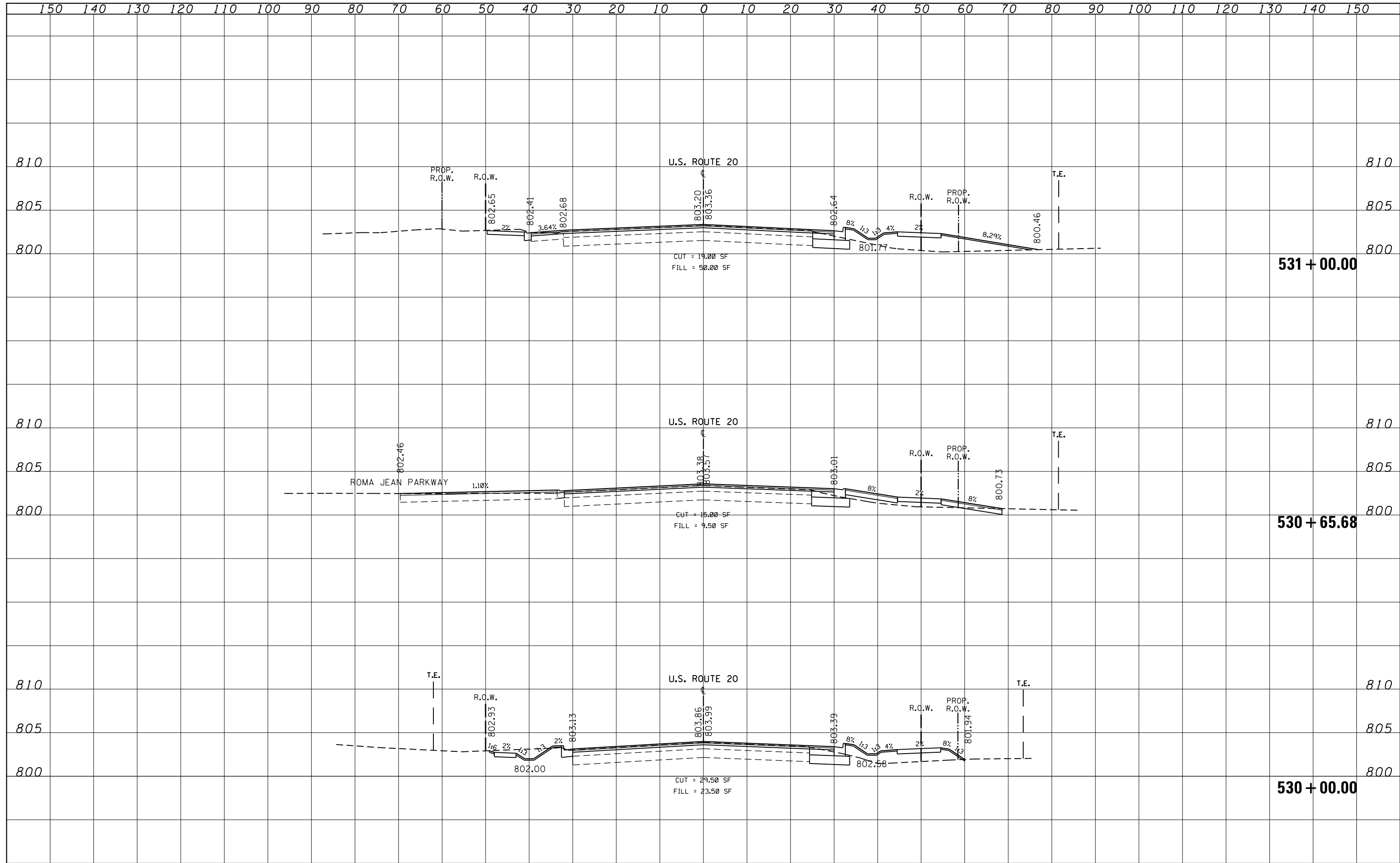
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FINAL SURVEY	
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ORIGINAL SURVEY	
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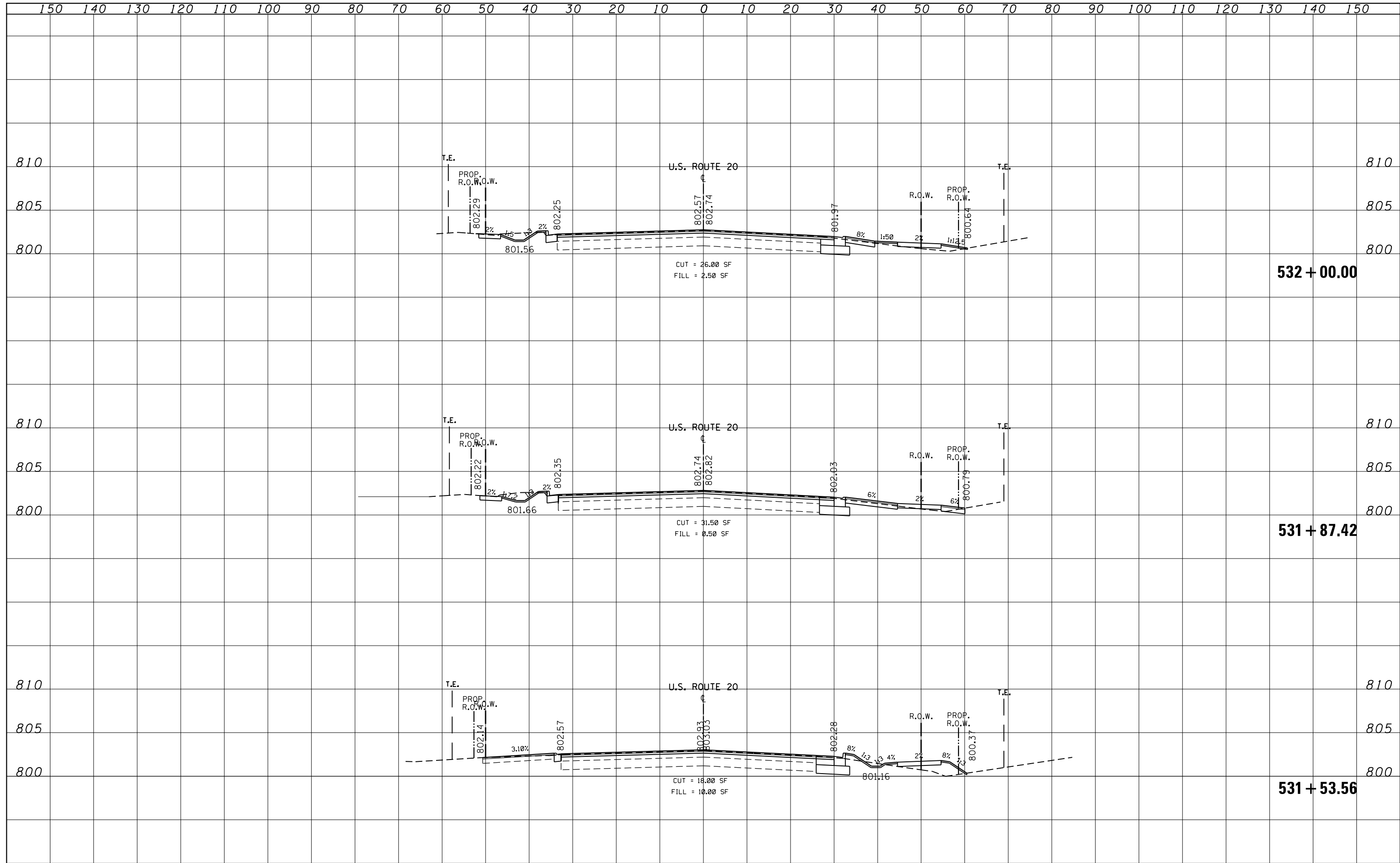
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FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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MODELNAME	PLOT SCALE = 20.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60N18							
	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 530+00.00 TO STA. 531+00.00	ILLINOIS FED. AID PROJECT		

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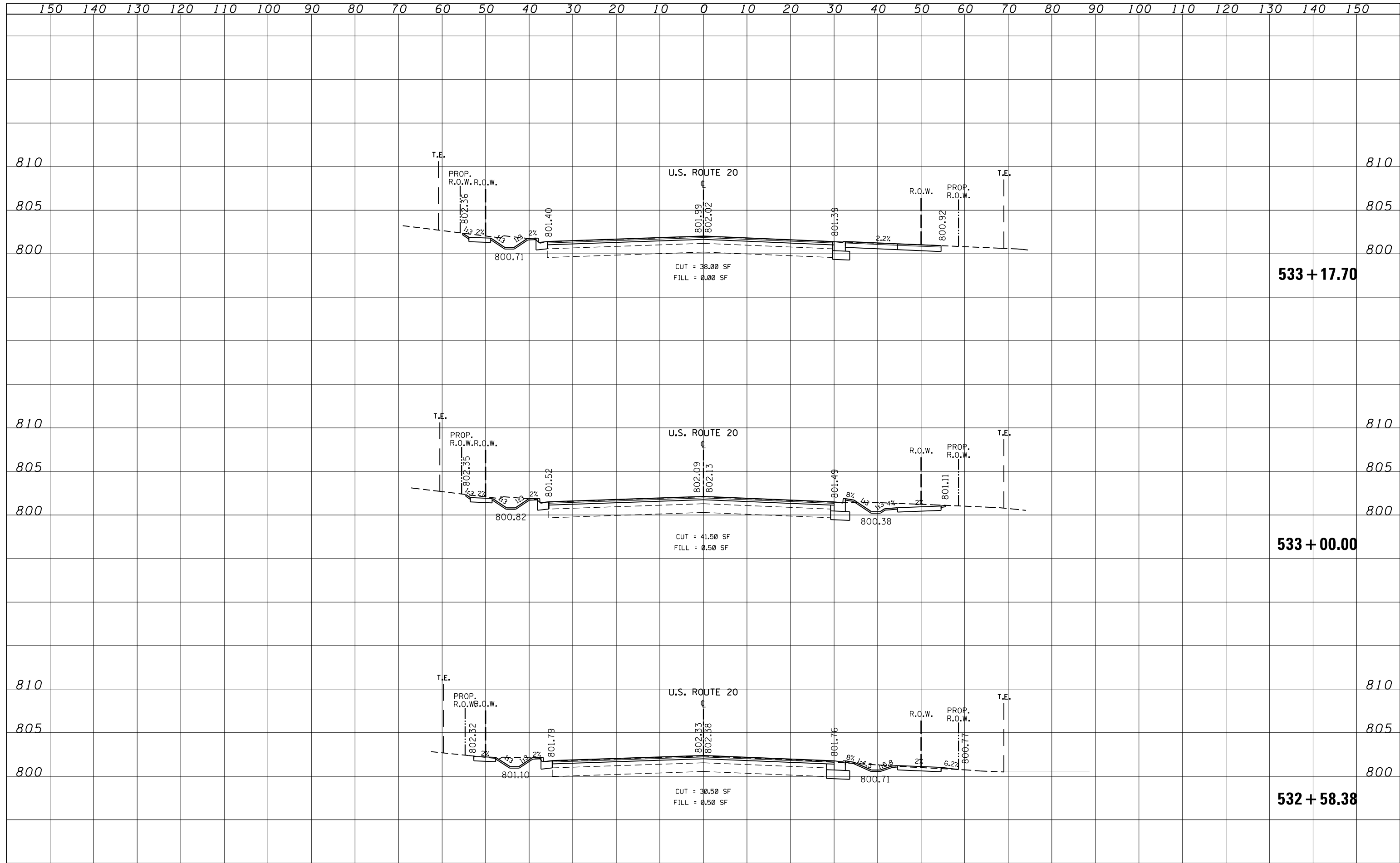
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FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 4/3/2013	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

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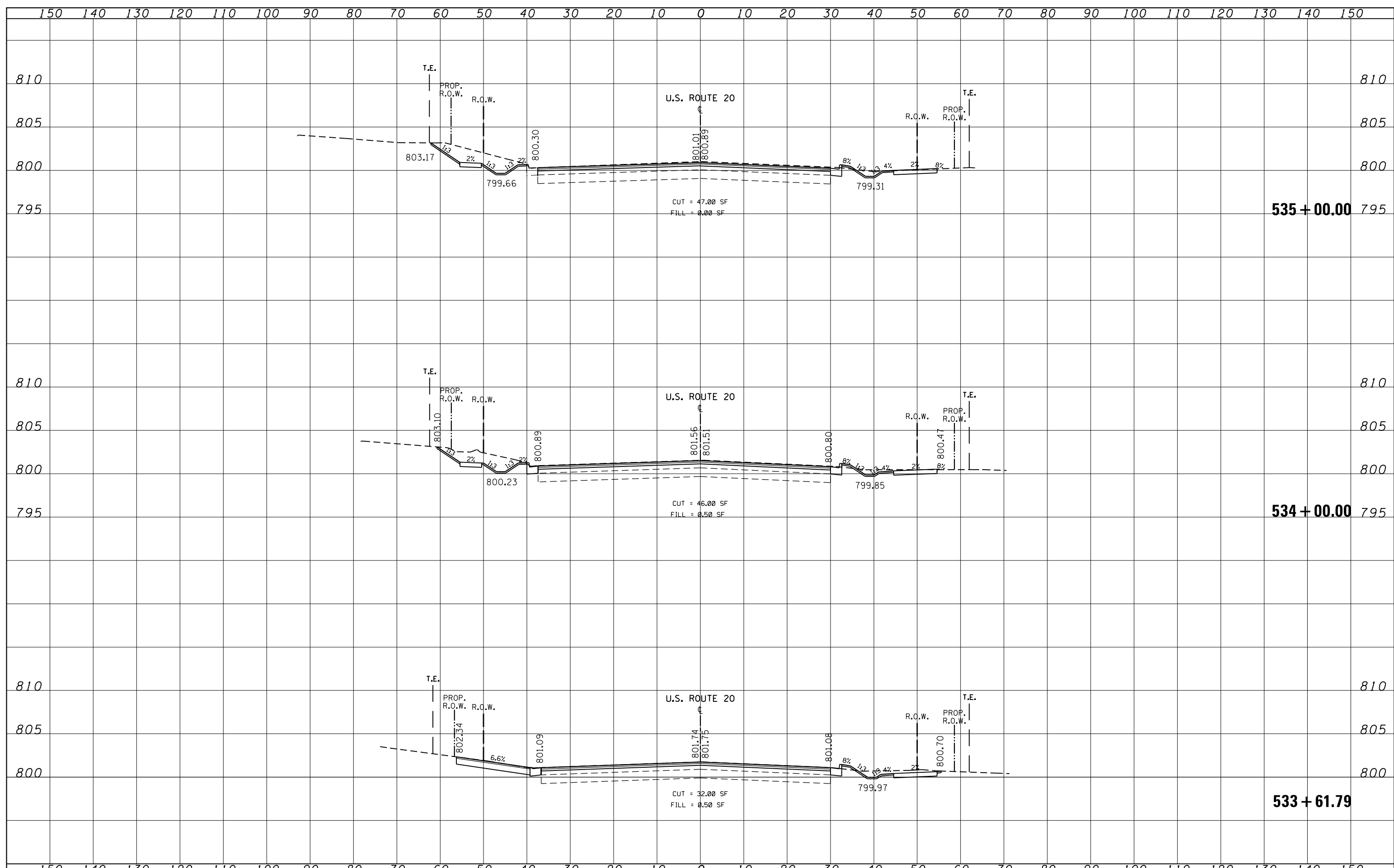
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	PLOT DATE = 4/3/2013	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

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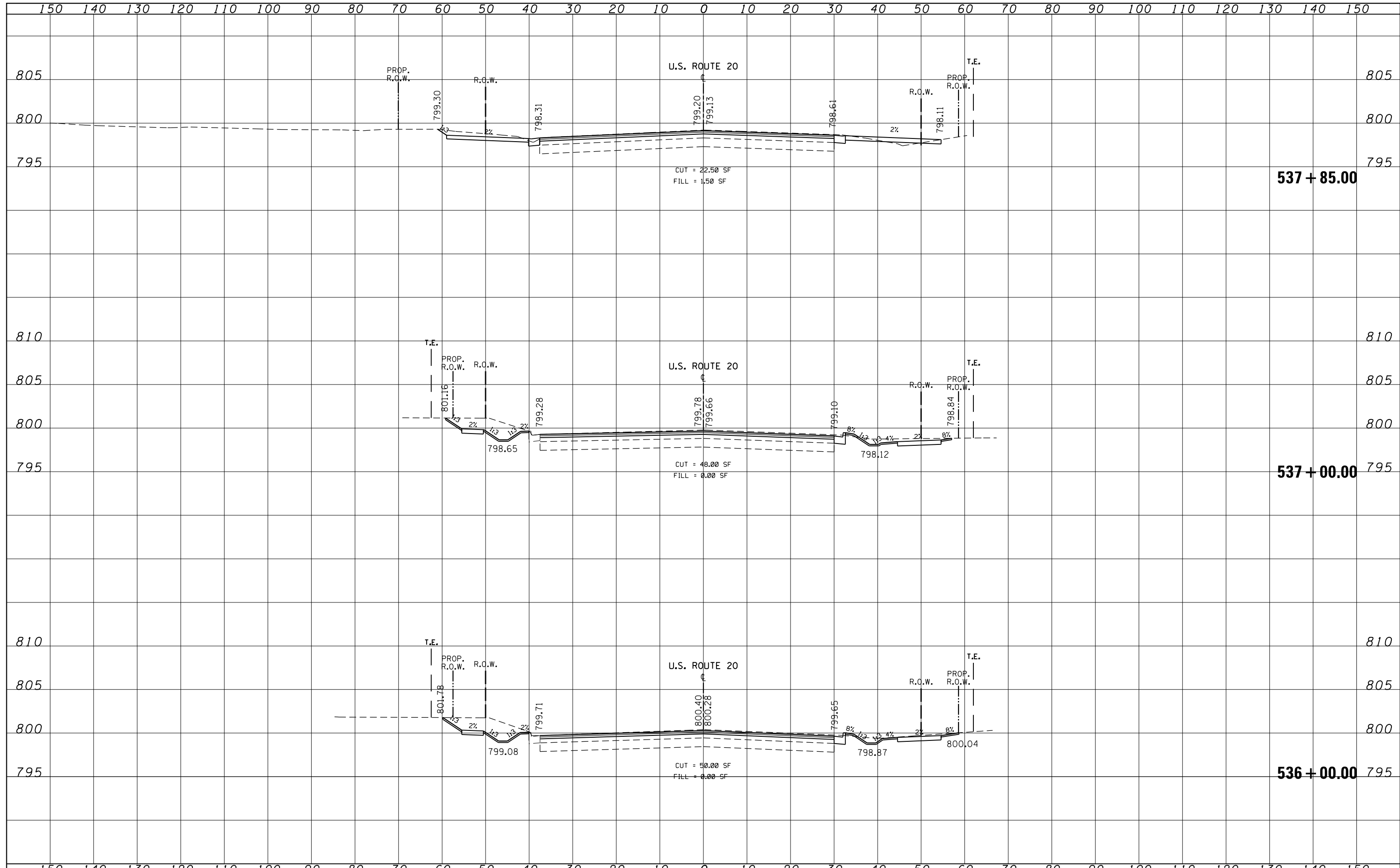
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FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 533+61.79 TO STA. 535+00.00			

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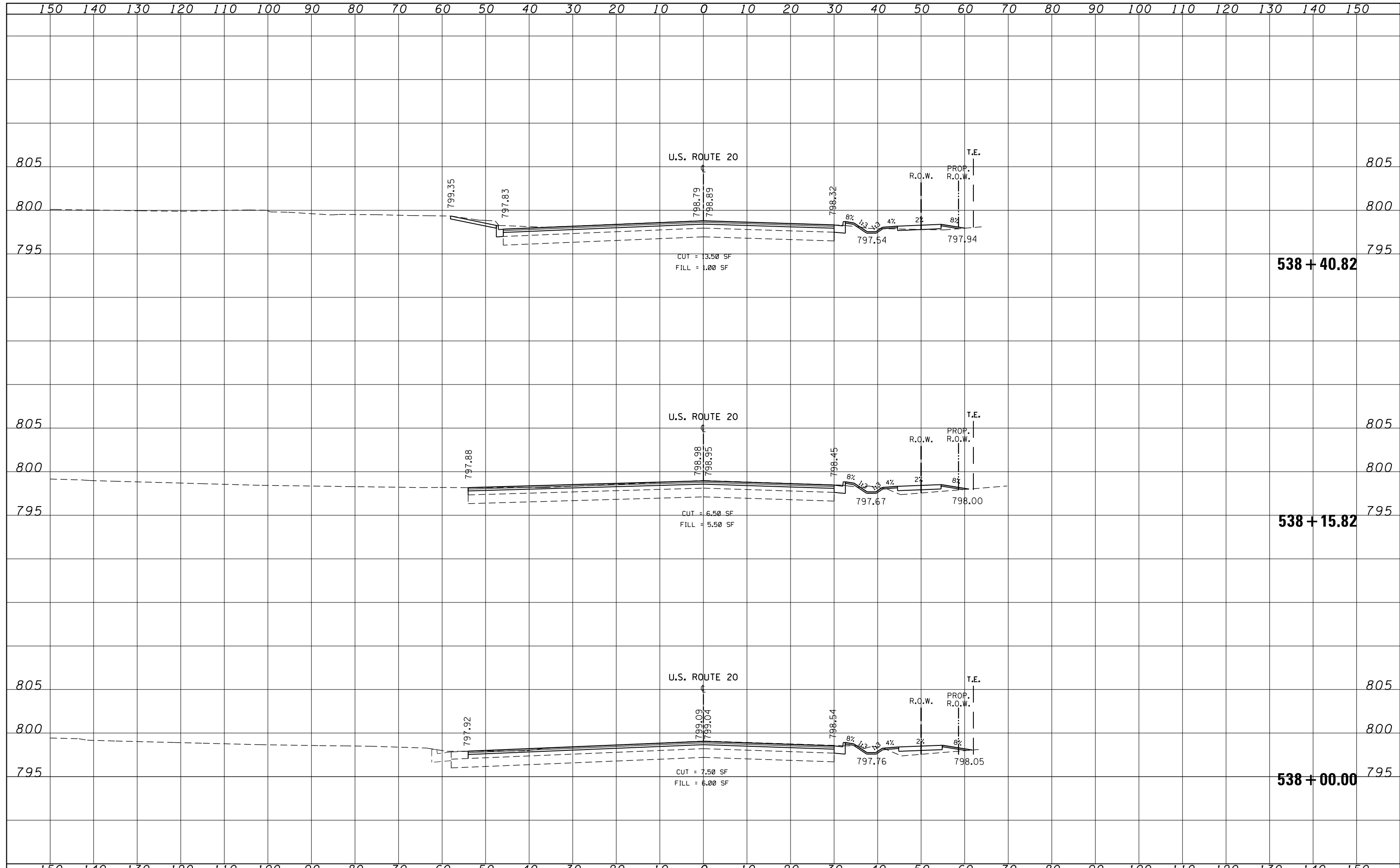
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AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\p\wdot\galbenjr\0304020\157408-sh	xsc-US20.dgn	DRAWN -	REVISED -					345	7Y-WR5	COOK	97	95
MODELNAME	PLOT SCALE = 20.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60N18							
	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 536+00.00 TO STA. 537+85.00	ILLINOIS FED. AID PROJECT		

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

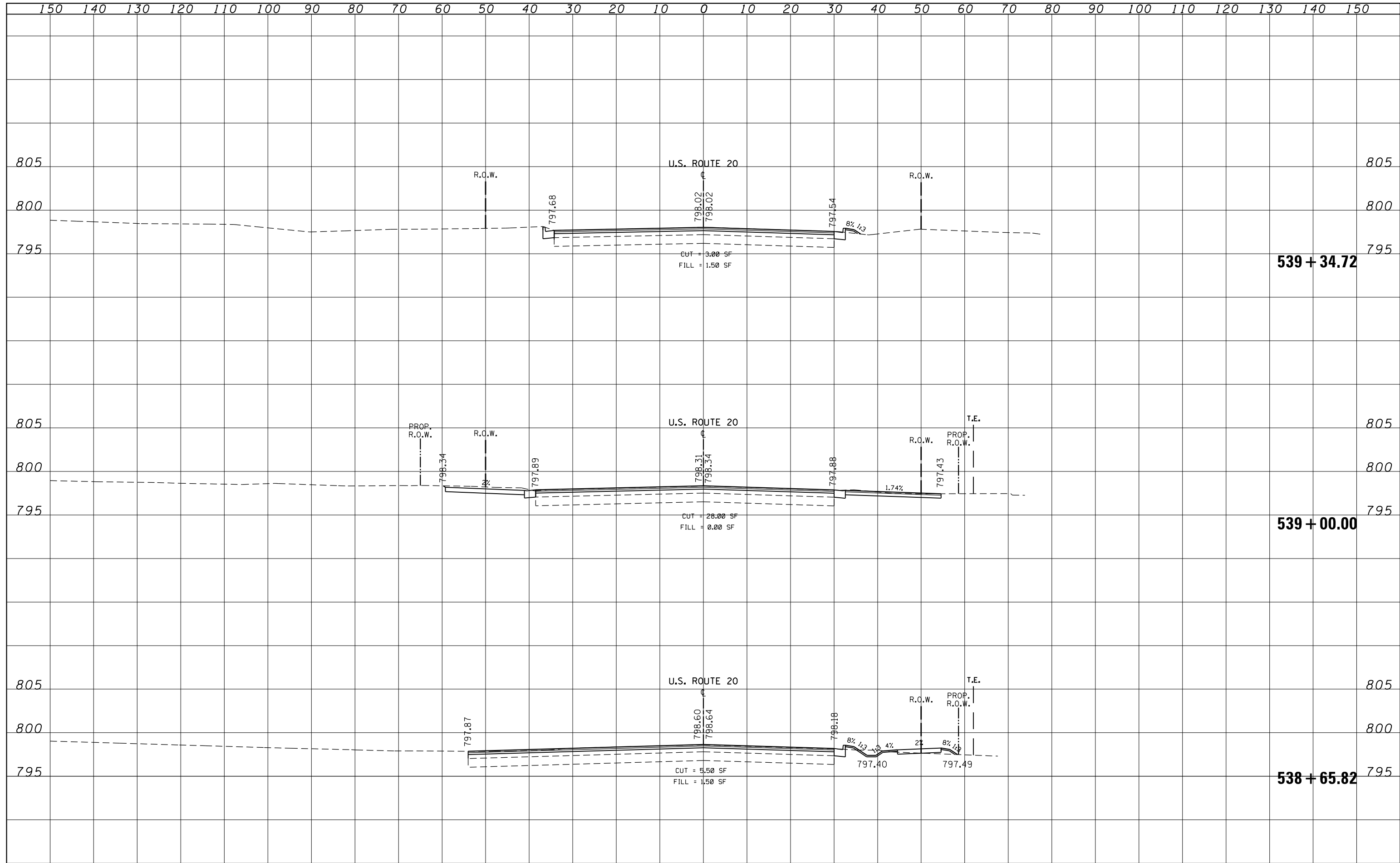
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BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\pwidot\galbenjr\10304020\PI57408-sh	xsc-US20.dgn	DRAWN -	REVISED -					345	7Y-WRS	COOK	97	96
MODELNAME	PLOT SCALE = 20.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60N18			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 538+00.00 TO STA. 538+40.82			

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	CHECKED
AREAS	CHECKED
NO.	

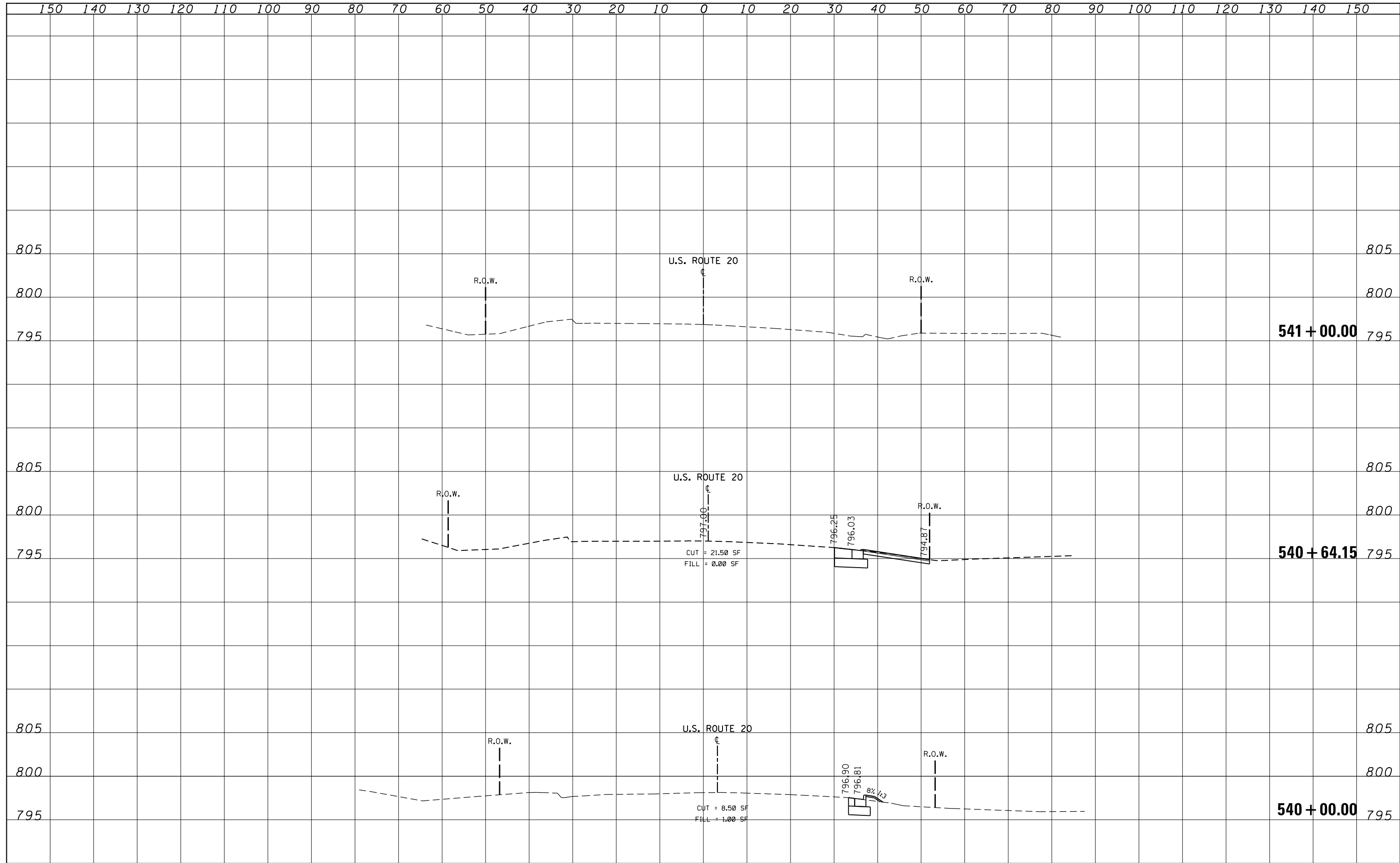
DATE	
BY	
SURVEYED	
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AREAS	CHECKED
AREAS	CHECKED
NO.	



FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\pwidot\galbenjr\0304020\PI57408-sh	xsc-US20.dgn	DRAWN -	REVISED -					345	7Y-WRS	COOK	97	96A
MODELNAME	PLOT SCALE = 20.000' / in.	CHECKED -	REVISED -		CONTRACT NO. 60N18			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 4/3/2013	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA. 538+65.82 TO STA. 539+34.72			

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



FILE NAME =	USER NAME = galbenjr	DESIGNED -	REVISIED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS U.S. ROUTE 20			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw\work\pwidot\galbenjr\10304020\157408-sh	xsc-US20.dgn	DRAWN -	REVISIED -					345	7Y-WRS	COOK	97	97
MODELNAME	PLOT SCALE = 20.000' / in.	CHECKED -	REVISIED -		CONTRACT NO. 60N18							
	PLOT DATE = 4/3/2013	DATE -	REVISIED -		SCALE:	SHEET	OF	SHEETS	STA. 540+00.00 TO STA. 541+00.00	ILLINOIS FED. AID PROJECT		