

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FEDERAL AID ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	1

CONTRACT NUMBER IS 20583
WHICH IS SAME AS BRIDGE SET
074000_FA10072_20583_5018X.PDF

PLAN 1 INCH = 100 FT.
PROFILE HOR. 1 INCH = 100 FT.
PROFILE VERT. 1 INCH = 10 FT.
CROSS-SECTIONS 1 INCH = 10 FT.

F.A. ROUTE 135 - SEC. 74-69
PROJECT F-18 (49)
PIATT COUNTY

INDEX OF SHEETS - SEE SHEET NO. 6
SUMMARY OF QUANTITIES - SEE SHEET NO. 7



LOCATION OF SECTION INDICATED THUS:—

ROAD CLASSIFICATION 840 T-70

OMISSION FROM PROJECT F-18 (49)

OMISSION FROM SECTION 74-69

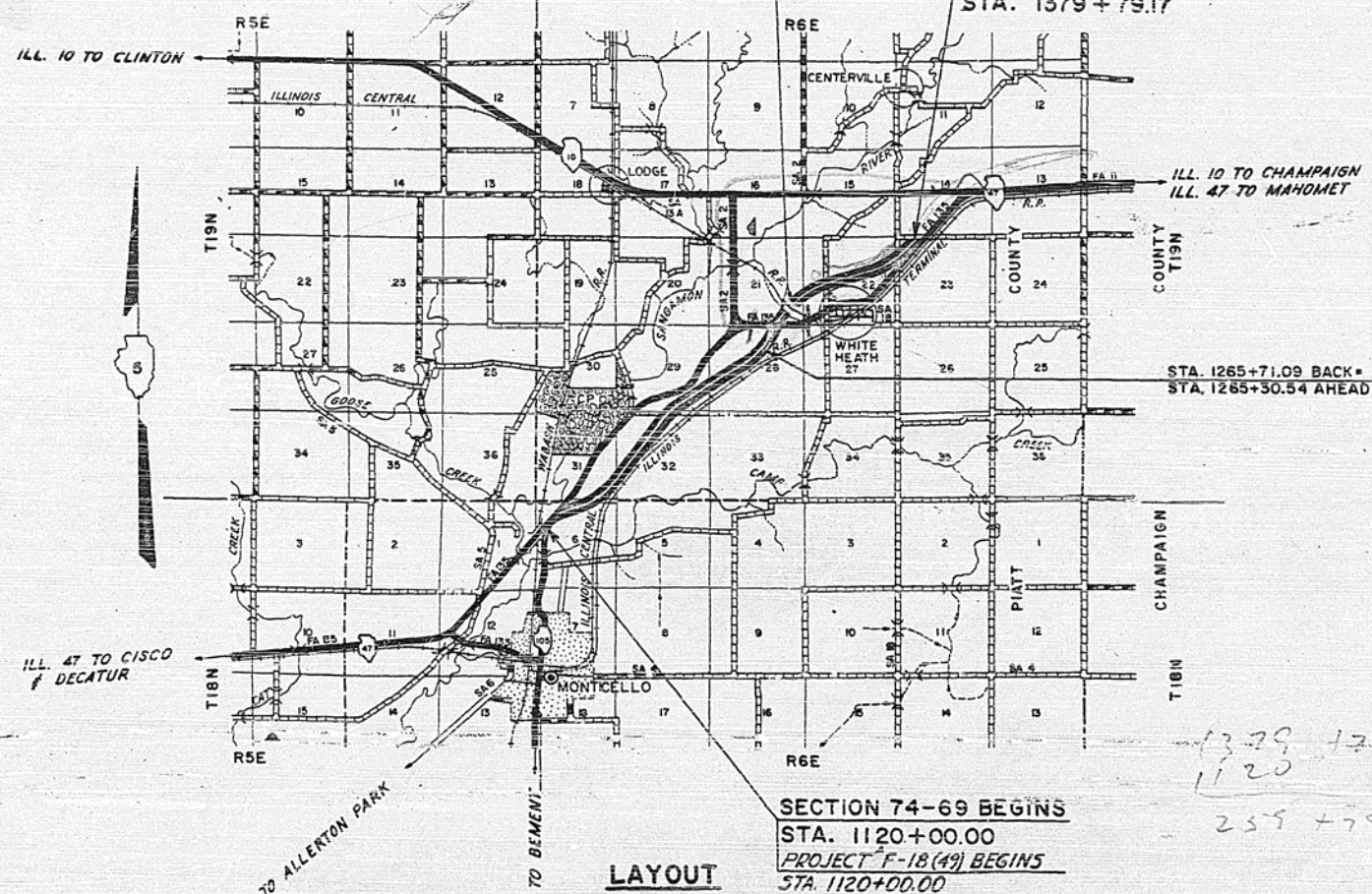
STA. 1300+78.30 TO STA. 1302+29.98

PROJECT F-18 (49) ENDS

STA. 1379+79.17

SECTION 74-69 ENDS

STA. 1379+79.17



SECTION 74-69 BEGINS
STA. 1120+00.00
PROJECT F-18 (49) BEGINS
STA. 1120+00.00

LAYOUT

SCALE 1" = 1 MI.

TOTAL LENGTH OF SECTION 74-69 = 26,019.72 FEET = 4.928 MILES
NET LENGTH OF SECTION 74-69 = 25,868.04 FEET = 4.899 MILES
NET LENGTH OF PROJECT = 25,868.04 FEET = 4.899 MILES

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

SUBMITTED *August 30, 1960*
BY *J. J. Chisholm*

EXAMINED *September 1, 1960*
BY *J. J. Chisholm*

APPROVED *September 1, 1960*
BY *J. J. Chisholm*

APPROVED *September 1, 1960*
BY *J. J. Chisholm*

PLANS FOR STRUCTURES EXAMINED *SEPT. 16, 1960*
BY *H. E. Bauman*

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS

APPROVED

DIVISION ENGINEER

DATE

HOMER L. CHASTAIN & ASSOCIATES
CONSULTING ENGINEERS
DECATUR ILLINOIS

DETAIL OF INSIDE SHOULDER

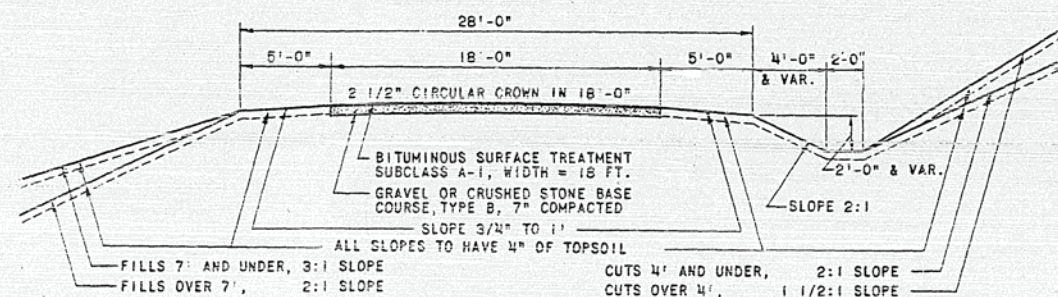
1 STANDARD TANGENT TYPICAL SECTION FOR F.A.-135

2 STANDARD SUPERELEVATED TYPICAL SECTION FOR FA-135

TYPICAL SECTIONS FOR MAINLINE		
STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS		
REVISIONS	F.A.-125	SECTION 74-60
No. DATE INITIALS		
1		
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5		
6		
7	PIATT	COUNTY
HOMER L. CHASTAIN & ASSOCIATES CONSULTING ENGINEERS		
8		
9		
10		

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-60	PIATT	155	3

3 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
18 FT. GRAVEL OR CRUSHED STONE BASE COURSE
WITH 18 FT. A-1 SURFACE TREATMENT
ON 28 FT. ROADBED

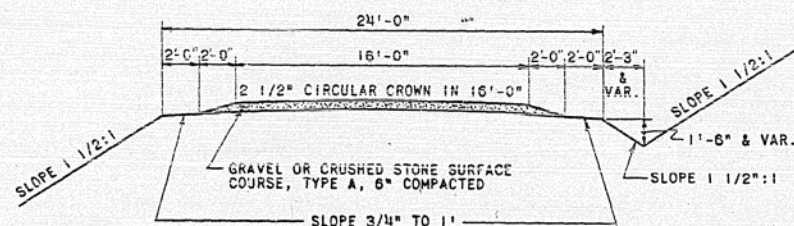


RELOCATED CROSS ROAD AT STA. 1225+59 (T.R.-87)
STA. 0+00 TO STA. 25+50
ROAD TO BE SUPERELEVATED STA. 15+24.6 TO STA. 24+66.7

RELOCATED CROSS ROAD AT STA. 1358+00 (T.R.-154)
STA. 42+00 TO STA. 76+00
ROAD TO BE SUPERELEVATED STA. 42+50.0 TO STA. 48+50.0
STA. 52+72.0 TO STA. 61+74.0
STA. 69+45.4 TO STA. 76+00.0

RELOCATED CROSS ROAD, LT. OF STA. 1328+ TO 1358+ (T.R.-134)
STA. 19+50 TO STA. 55+99
ROAD TO BE SUPERELEVATED STA. 19+50.0 TO STA. 29+03.3
STA. 42+13.6 TO STA. 56+60.0

5 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
16 FT. GRAVEL OR CRUSHED STONE SURFACE COURSE
ON 24 FT. ROADBED



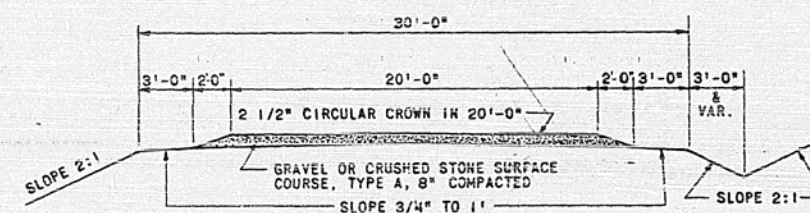
FRONTAGE ROAD, RT. OF STA. 1100+ TO 1139+
STA. 0+00 TO STA. 42+00
ROAD TO BE SUPERELEVATED STA. 7+30 TO STA. 29+71
STA. 39+70 TO STA. 42+00

FRONTAGE ROAD, RT. OF STA. 1216+ TO 1229+
STA. 15+12 TO STA. 27+50

FRONTAGE ROAD, RT. OF STA. 1284+ TO 1290+
STA. 3+75 TO STA. 16+00

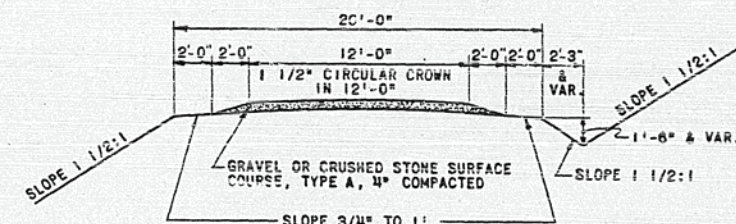
FRONTAGE ROAD, LT. OF STA. 1283+ TO 1286+
STA. 5+83 TO STA. 16+00

4 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
20 FT. GRAVEL OR CRUSHED STONE SURFACE COURSE
ON 30 FT. ROADBED



DETOUR ROAD, LT. OF STA. 1121+ TO 1126+
STA. 0+00 TO STA. 6+25

6 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
12 FT. GRAVEL OR CRUSHED STONE SURFACE COURSE
ON 20 FT. ROADBED



ACCESS LANE, LT. OF STA. 1133+ TO 1138+
STA. 132+85 TO STA. 139+30
NOTE: USE SALVAGED AGGREGATE

ACCESS LANE, RT. OF STA. 1229+ TO 1246+
STA. 27+50 TO STA. 44+46

FRONTAGE RD. LT. OF STA. 1286+ TO 1291+
STA. 16+00 TO STA. 21+00

ACCESS LANE, LT. OF STA. 1328+ TO 1330+
STA. 0+00 TO 2+82

FRONTAGE ROAD, RT. OF STA. 1277+ TO 1284+
STA. 6+00 TO 14+96
16+00 - 24+96

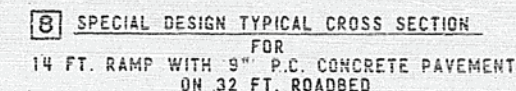
TYPICAL SECTIONS FOR CROSS ROADS

REVISIONS		STATE OF ILLINOIS		DATE
NO.	DATE	INITIALS	DESCRIPTION	DATE
1				
2				
3				
4				
5				
6				
7				
8				
9				
10				

FA-135	SECTION 74-69	COUNTY
HOMER L. CHASTAIN & ASSOCIATES	CONSULTING ENGINEERS	DECATUR, ILLINOIS

PROJECT NO.	SHEET NO.
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[7] SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
FA-105 WITH VARIABLE WIDTH AT RAMPS



32'-0"

2'-0" 12'-0" 8'-0" 14'-0" 10'-0"

11 1/4" 4'-0" 4'-0" 10' SURVEY LINE AND BASE LINE 8'-0" 2'-0"

10' 1/4" .01'/' ON TAN.

PROFILE GRADE APPLIES HERE

10' 1/4"

SLOPE 4:1

RADIUS 10'-0"

SLOPE 1/4" TO 1' SLOPE 1/2" TO 1'

STRAIGHT LINES

SLOPE 1/4" TO 1' SLOPE 1/2" TO 1'

FOR PAVEMENT DETAILS, SEE STANDARD 2068R AND OMIT LONGITUDINAL METAL JOINT

SUB-BASE GRANULAR MATERIAL, TYPE A (5" COMPACTED)

LEFT RAMP SHOULDER (SEE DETAIL)

RIGHT RAMP SHOULDER (SEE DETAIL)

ALL SLOPES TO HAVE 4" OF TOPSOIL

FILLS UP TO 15', 4:1 SLOPE

FILLS 15' TO 25', 3:1 SLOPE

FILLS OVER 25', 2:1 SLOPE

RAMP A. LT. OF STA. 1274± TO 1263±

STA. 100+94 TO STA. 111+84.02

RAMP TO BE SUPERELEVATED STA. 102+08.20 TO STA. 111+84.02

RAMP B. LT. OF STA. 1279± TO 1283±

STA. 206+31.21 TO STA. 215+84

RAMP TO BE SUPERELEVATED STA. 206+31.21 TO STA. 214+37.48

RAMP C. RT. OF STA. 1283± TO 1288±

STA. 301+06 TO STA. 311+23.17

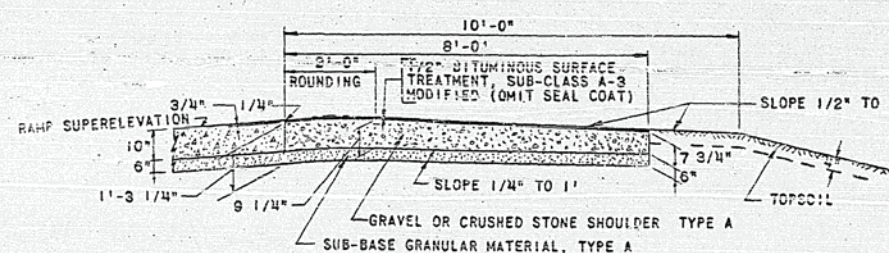
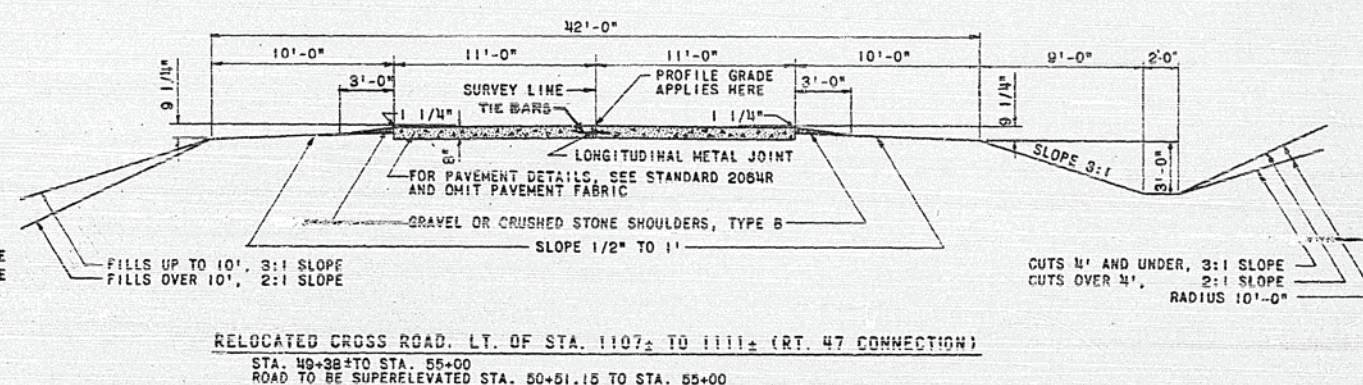
RAMP TO BE SUPERELEVATED STA. 301+93.70 TO STA. 311+23.17

RAMP D. RT. OF STA. 1277± TO 1288±

STA. 405+31.21 TO STA. 418+93

RAMP TO BE SUPERELEVATED STA. 405+31.21 TO STA. 418+01.48

SPECIAL DESIGN TYPICAL CROSS SECTION
FDR
22 FT. NON-REINFORCED PORTLAND CEMENT
CONCRETE PAVEMENT
ON 42 FT. ROADBED



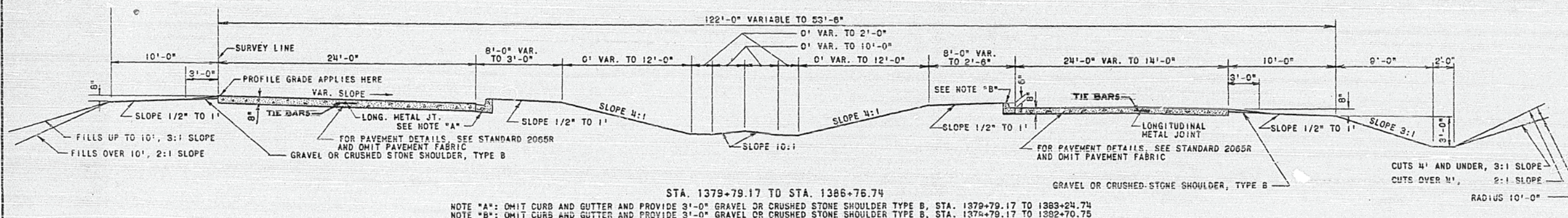
NOTE: THE 2 FOOT ROUNDING OF SHOULDER AT PAVEMENT EDGE TO BE OMITTED WHERE SHOULDER IS ALONG TANGENT SECTION OF RAMP OR ON THE INSIDE OF A CURVED RAMP. HERE THE SHOULDER SLOPE SHALL BE THE SAME AS THE RAMP SUPER-ELEVATION RATE WITH A MINIMUM SLOPE OF 1/2" PER FT.

E		TYPICAL SECTIONS FOR RAMPS AND CROSS ROADS		DATE BY	
FT.		REVISIONS No. DATE DESCRIPTION 1 2 3 4 5 6 7 8 9		STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS F.A. - 135 SECTION 74-69 PIATT COUNTY HOMER L. CHASTAIN & ASSOCIATES CONSULTING ENGINEERS	
				GAC 6-10-72 CHECKED BY SA M.H. 7-2 SPECIAL	
				PROJECT NO.	
				SHEET NO.	

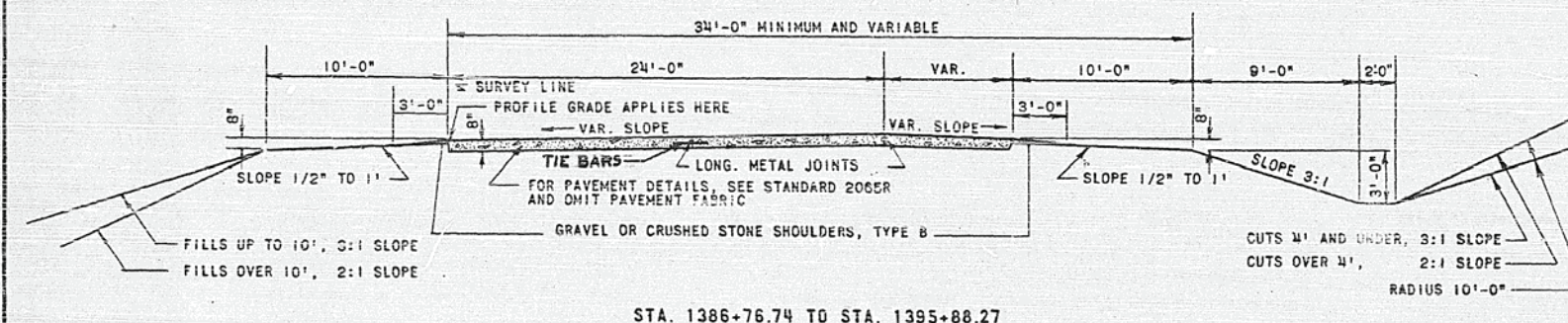
DETAIL OF RIGHT RAMP SHOULDER

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	5
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		

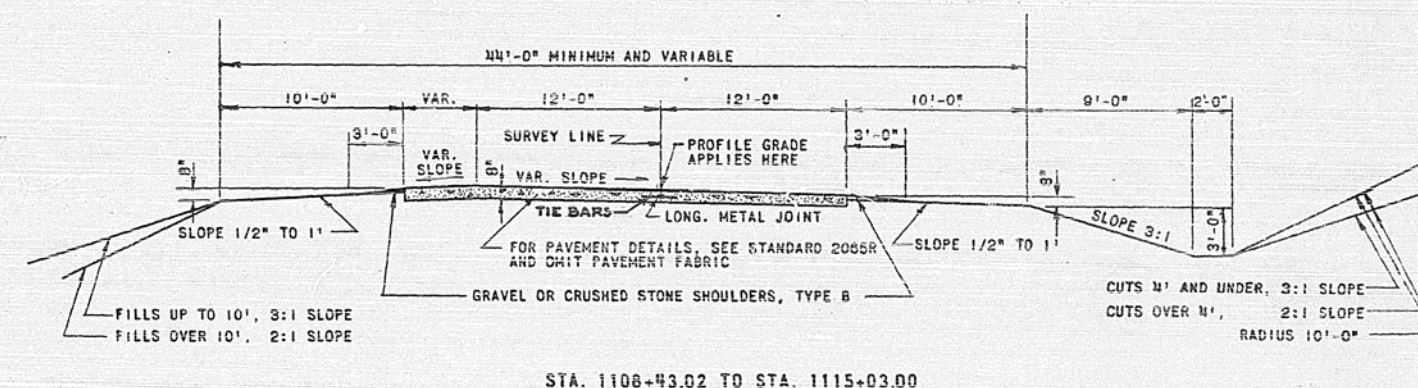
11 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
FA-135 EAST TERMINATION



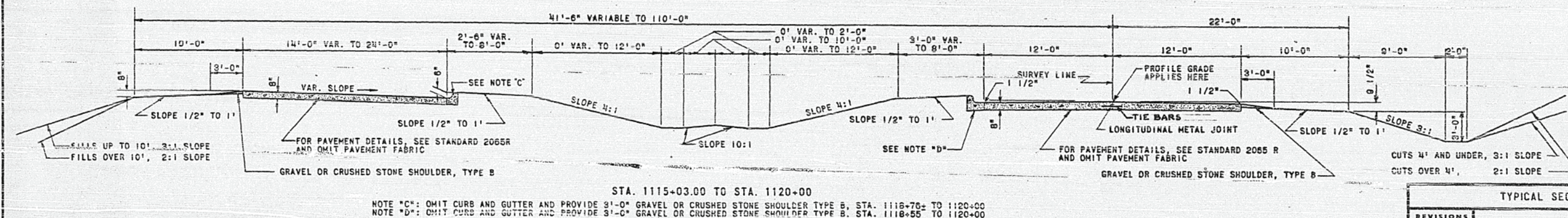
12 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
FA-135 EAST TERMINATION



13 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
FA-135 WEST TERMINATION



14 SPECIAL DESIGN TYPICAL CROSS SECTION
FOR
FA-135 WEST TERMINATION



TYPICAL SECTIONS FOR TERMINATIONS				
STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS				
FA-135 SECTION 74-69				
PIATT COUNTY				
HOMER L. CHASTAIN & ASSOCIATES CONSULTING ENGINEERS DECATUR, ILLINOIS				
<div> <div> REVISIONS 1 2 3 4 5 6 7 8 9 10 </div> <div> DATE </div> <div> INITIALS </div> </div>				
<div> <div> DRAWN BY CHKD BY DATE 6-28-60 7-12-60 </div> <div> PROJECT NO. SHEET NO. </div> </div>				

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.-185 74-88	PIATT	ILLINOIS	155	6
FED. ROAD DIST. NO.	ILLINOIS PROJECT			

INDEX OF SHEETS

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GENERAL NOTES

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3	TYPICAL SECTIONS FOR CROSSROADS		
4	TYPICAL SECTIONS FOR RAMP		
5	TYPICAL SECTIONS FOR TERMINATIONS		
6	INDEX OF SHEETS AND GENERAL NOTES		
7	SUMMARY OF QUANTITIES AND SUMMARY OF CLASS X CONCRETE		
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18	PLAN AND PROFILE, S.B.I. 10 (RT. 47) CONNECTION		
19	PLAN AND PROFILE, FRONTAGE ROAD TO T.R. 95		
20	PLAN AND PROFILE, RELOCATED T.R. 87		
21	PLAN AND PROFILE, CRESAP-PATTON FR. RD. AND McCARTNEY		
22	PLAN AND PROFILE, RELOCATED ROUTE 47		
23	GENERAL LAYOUT, INTERCHANGE AT RELOC. RT. 47 WITH F.A. 135		
24	PLAN AND PROFILE, RAMPS A & B INTERCHANGE AT RELOC. RT. 47 WITH F.A. 135		
25	PLAN AND PROFILE, RAMPS C & D INTERCHANGE AT RELOC. RT. 47 WITH F.A. 135		
26	PLAN AND PROFILE, BARKER FRONTAGE ROAD		
27	PLAN AND PROFILE, RELOC. T.R. 154 AND T.R. 134 CONN.		
28	PLAN AND PROFILE, RELOC. T.R. 154		
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30	PAVEMENT ELEVATIONS, INTERCHANGE AT RELOC. RT. 47 WITH F.A. 135		
31	NOSE DETAILS, INTERCHANGE AT RELOC. RT. 47 WITH F.A. 135		
32	INTERSECTION DETAILS, INTERCHANGE AT RELOC. RT. 47 WITH F.A. 135		
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34	MISCELLANEOUS DETAILS		
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38	SPECIAL BOX CULVERT, A.R. STA. 1367+00 E.B. LANES		
39	SPECIAL BOX CULVERT, A.R. STA. 1381+08		
40	SPECIAL BOX CULVERT, A.R. STA. 65+40 RELOC. RT. 47		
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116	STATION CROSS SECTIONS, I.C.R.R. AND I.T.R.R.		
117	STATION CROSS SECTIONS, CRESAP-PATTON FRONTAGE ROAD		
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122-123 INC.	STATION CROSS SECTIONS, RAMP A		
124	STATION CROSS SECTIONS, RAMP B		
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130	STATION CROSS SECTIONS, McCARTNEY FRONTAGE ROAD		
131-132 INC.	STATION CROSS SECTIONS, BARKER FRONTAGE ROAD		
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143	STANDARD NOS. 1538R, 1744-1, 1776R, 1971-2		
144	STANDARD NOS. 1687-2, 1766-2		
145	STANDARD NOS. 1790F, 1897T		
146	STANDARD NOS. 1909-3		
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152	STANDARD NOS. 2129-1		
153	STANDARD NOS. 2138-1		
154	STANDARD NOS. 2149, 2150		
155	STANDARD NOS. 1972-1, 1973		
1.	TREES THAT INTERFERE WITH CONSTRUCTION OPERATION SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. ESTIMATED QUANTITIES: 800 IN. DIA. TREE REMOVAL (8" TO 15" DIA.) 850 IN. DIA. TREE REMOVAL (OVER 15" DIA.)	010001 010002	
2.	HEDGES THAT INTERFERE WITH CONSTRUCTION OPERATION SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. ESTIMATED QUANTITY 6 UNITS HEDGE REMOVAL	010008	
3.	SUB-BASE GRANULAR MATERIAL, TYPE A, SHALL BE PLACED 6" THICK COMPACTED UNDER THE PROPOSED 10" P.C. CONCRETE PAVEMENT, 6" THICK COMPACTED UNDER THE BITUMINOUS SURFACED SHOULDERS AND 4" THICK COMPACTED UNDER THE PROPOSED 8" P.C. CONCRETE PAVEMENT IN ACCORDANCE WITH THE TYPICAL CROSS SECTIONS. ESTIMATED QUANTITY 79,000 TONS SUB-BASE GRANULAR MATERIAL, TYPE A	024001	
4.	A GRAVEL OR CRUSHED STONE SHOULDER, TYPE A, SHALL BE PLACED ON THE MAINLINE AND RAMP SHOULDERS IN ACCORDANCE WITH THE TYPICAL CROSS SECTIONS SHOWN IN THE PLANS. ESTIMATED QUANTITY 38,500 TONS GRAVEL OR CRUSHED STONE SHOULDERS, TYPE A	026001	
5.	A GRAVEL OR CRUSHED STONE SHOULDER, TYPE B SHALL BE BUILT IN ACCORDANCE WITH THE TYPICAL CROSS SECTIONS SHOWN IN THE PLANS. ESTIMATED QUANTITY 670 TONS GRAVEL OR CRUSHED STONE SHOULDERS, TYPE B	026003	
6.	TOPSOIL SHALL BE PLACED ON SHOULDERS, SIDESLOPES AND BACKSLOPES FOR THE ENTIRE MAINLINE AND ALL CROSSROADS EXCEPT FRONTAGE ROADS AND ACCESS LANES AS INDICATED ON THE TYPICAL SECTIONS. ESTIMATED QUANTITY 59,000 CU. YDS. TOP SOIL	027001	
7.	A GRAVEL OR CRUSHED STONE BASE COURSE, TYPE B SHALL BE PLACED ON RELOCATED CROSSROAD AT STATION 1225+59, RELOCATED CROSSROAD AT STATION 1358+00 AND RELOCATED CROSSROAD LEFT OF STATION 1328+ TO 1358+ AS INDICATED ON THE TYPICAL SECTIONS AND AS OTHERWISE SHOWN ON THE PLANS. ESTIMATED QUANTITY 7,400 TONS GRAVEL OR CRUSHED STONE BASE COURSE, TYPE B	029003	
8.	A GRAVEL OR CRUSHED STONE SURFACE COURSE, TYPE A SHALL BE PLACED ON ALL FRONTAGE ROADS, ACCESS LANES, DETOUR ROADS AND MAILBOX TURNOUTS AS INDICATED ON THE PLANS IN ACCORDANCE WITH THE TYPICAL SECTIONS. ESTIMATED QUANTITY 6,200 TONS GRAVEL OR CRUSHED STONE SURFACE COURSE, TYPE A	036001	
9.	A BITUMINOUS SURFACE TREATMENT, SUBCLASS A-1, SHALL BE PLACED ON THE RELOCATED CROSSROAD AT STATION 1225+59, RELOCATED CROSSROAD AT STATION 1358+00 AND RELOCATED CROSSROAD LEFT OF STATION 1328+ TO 1358+ AS INDICATED ON THE TYPICAL SECTIONS. ESTIMATED QUANTITIES 5,200 GALS. BITUMINOUS MATERIALS (SEAL COAT) 190 TONS SEAL COAT AGGREGATE	037001 037002	
10.	A BITUMINOUS SURFACE TREATMENT, SUBCLASS A-3 (MODIFIED) SHALL BE PLACED ON THE MAINLINE AND RAMP SHOULDERS IN ACCORDANCE WITH THE TYPICAL SECTIONS AND SPECIAL PROVISIONS. ESTIMATED QUANTITIES 36,800 GALS. BITUMINOUS MATERIALS (PRIME COAT) A-3 42,900 GALS. BITUMINOUS MATERIALS (COVER COAT) A-3 2,200 TONS COVER AGGREGATE	039001 039002 039003	
11.	INCIDENTAL BITUMINOUS SURFACING SHALL BE PLACED ON MEDIAN ISLANDS, PRIVATE ENTRANCES, APPROACHES AND MAILBOX TURNOUTS AS SHOWN IN THE PLANS AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS. ESTIMATED QUANTITIES 200 GALS. BITUMINOUS MATERIALS (PRIME COAT) I-II 41 TONS INCIDENTAL BITUMINOUS SURFACING I-II	046001 200142	
12.	A PORTLAND CEMENT CONCRETE PAVEMENT OF 9" UNIFORM THICKNESS SHALL BE CONSTRUCTED AT S.B.I. 10 (ROUTE 47) CONNECTION AND AT TERMINATIONS IN ACCORDANCE WITH THE TYPICAL SECTIONS. ESTIMATED QUANTITY 12,119 SQ. YDS. PORTLAND CEMENT CONCRETE PAVEMENT, (9 INCH)	048006	
13.	EXCEPT AS OTHERWISE INDICATED ON THE PLANS, PORTLAND CEMENT CONCRETE PAVEMENT OF 9" UNIFORM THICKNESS AND 22'-0" WIDTH SHALL BE CONSTRUCTED AT S.B.I. 10 (ROUTE 47) CONNECTION AND RELOCATED ROUTE 47 IN ACCORDANCE WITH THE TYPICAL SECTIONS. ESTIMATED QUANTITY 9,805 SQ. YDS. PORTLAND CEMENT CONCRETE PAVEMENT, (9 INCH)	048007	
14.	EXCEPT AS OTHERWISE INDICATED ON THE PLANS, PORTLAND CEMENT CONCRETE PAVEMENT OF 10" UNIFORM THICKNESS SHALL BE CONSTRUCTED ON THE MAINLINE WITH 2 # 24'-0" WIDTH AND ON THE (4) RAMPS (A, B, C, AND D) OF THE INTERCHANGE AT RELOCATED ROUTE 47 WITH VARIABLE WIDTH, AS SHOWN IN THE PLANS AND ON THE TYPICAL SECTIONS. ESTIMATED QUANTITY 144,580 SQ. YDS. PORTLAND CEMENT CONCRETE PAVEMENT (10 INCH)	048008	
15.	THE VARIABLE WIDTH PORTIONS OF THE RAMPS OF THE INTERCHANGE OF RELOCATED ROUTE 47 AT THE ACCELERATION AND DECELERATION LANES, SHALL BE CONSTRUCTED OF PORTLAND CEMENT CONCRETE PAVEMENT, 10" UNIFORM THICKNESS, COLORED AS SHOWN ON THE DETAILS INCLUDED IN THE PLANS AND STATED IN THE SPECIAL PROVISIONS. ESTIMATED QUANTITY 4,981 SQ. YDS. PORTLAND CEMENT CONCRETE PAVEMENT (COLORED) 10 INCH	048010	
16.	PAVEMENT FABRIC SHALL BE PLACED FOR ALL PORTLAND CEMENT CONCRETE PAVEMENT, EXCEPT FOR THE PORTLAND CEMENT CONCRETE PAVEMENT OF 8" THICKNESS, IN ACCORDANCE WITH THE TYPICAL SECTIONS. ESTIMATED QUANTITY 159,271 SQ. YDS. PAVEMENT FABRIC	048019	
17.	CURING COVER SHALL BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER. ESTIMATED QUANTITY 60 UNITS REMOVING AND REPLACING CURING COVERING	048018	
18.	THE REMOVAL OF EXISTING PAVEMENT AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR PAVEMENT REMOVAL. ESTIMATED QUANTITY 8,189 SQ. YDS. PAVEMENT REMOVAL	082001	
19.	SALVAGED AGGREGATE SHALL BE REMOVED FROM THE DETOUR ROAD AFTER THE DETOUR ROAD HAS SERVED ITS PURPOSE AND SHALL BE REUSED IN SURFACING ACCESS LANE LEFT OF STA. 1133 TO STA. 1138 AND ALL PRIVATE ENTRANCES ADJACENT TO FRONTAGE ROAD RIGHT OF STA. 1100 TO STA. 1139. ESTIMATED QUANTITY 128 CU. YDS. SALVAGED AGGREGATE	101005	
20.	CALCIUM CHLORIDE SHALL BE APPLIED ON THE DETOUR ROAD AS DIRECTED BY THE ENGINEER. ESTIMATED QUANTITY 3.9 TONS CALCIUM CHLORIDE APPLIED	102001	
21.	SHOULDERS, SIDESLOPES, BACKSLOPES AND OTHER PORTIONS OF THE RIGHT-OF-WAY HAVING INSUFFICIENT VEGETATION SHALL BE SEED AS DIRECTED BY THE ENGINEER. ONLY EARTH SHOULDERS AND SLOPES 4:1 AND GREATER SHALL BE MULCHED. ESTIMATED QUANTITIES 86 ACRES TEMPORARY SEEDING 86 ACRES COMPLETE SEEDING 200 TONS STRAW FOR ASPHALT-COATED MULCH 20,000 GALS. EMULSIFIED ASPHALT	110001 110004 111002 111003	
22.	ON ALL AREAS TO BE SEED, FERTILIZER, NUTRIENTS AND AGRICULTURAL GROUND LIME/STONE SHALL BE APPLIED AS DIRECTED BY THE ENGINEER. ESTIMATED QUANTITIES 14 TONS FERTILIZER NUTRIENTS 520 TONS AGRICULTURAL GROUND LIME/STONE	110005 110006	
23.	A STRIP OF SOD 18" WIDE SHALL BE PLACED ON EACH SIDE OF ALL PAVED DITCHES, AND AS DIRECTED BY THE ENGINEER. ESTIMATED QUANTITIES 1,300 SQ. YDS. SODDING 6 UNITS SUPPLEMENTAL WATERING	112001 112002	
24.	DELINEATORS SHALL BE INSTALLED ON OUTSIDE SHOULDERS FOR MAINLINE AT A SPACING OF 200 FEET BETWEEN STA. 1120+00 AND STA. 1179+00, AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS. ESTIMATED QUANTITY 500 EACH DELINEATORS	220278	
25.	PERMANENT SURVEY MARKERS SHALL BE SET ON THE SURVEYED CENTERLINE FOR F.A. 135, S.B.I. 10, RELOCATED T.R. 87, RELOCATED ROUTE 47, AND RELOCATED T.R. 154 AT THE LOCATIONS SHOWN IN THE PLANS INDICATED BY THE SYMBOL Δ . ESTIMATED QUANTITIES 18 EACH PERMANENT SURVEY MARKERS, TYPE I 32 EACH PERMANENT SURVEY MARKERS, TYPE II	200350 200351	

INDEX OF SHEETS AND GENERAL NOTES			
REVISIONS		STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS	
1.	DATE	F.A.-185	
2.	INITIALS	SECTION 7-65	
3.		PIATT	
4.		COUNTY	
5.		HOMER L. CHASTAIN & ASSOCIATES	
6.		CONSULTING ENGINEERS	
7.		DECATUR, ILLINOIS	
8.		PROJECT NO.	
9.		SHEET NO.	
10.		DATE	

SUMMARY OF CLASS X CONCRETE

SIDE	STATION	TYPE OF STRUCTURE	CU. YDS.	QUANTITY	UNIT	ITEM
L	1118+55	SPECIAL INLET FOR TYPE 3 CURB & GUTTER	0.4	1535	800	IN. DIA.
L	1118+76	SPECIAL INLET FOR TYPE 6 CURB & GUTTER	0.5	1054	350	IN. DIA.
L & R	1135+00	STANDARD 1976 D24-2 (4) HEADWALL	4.0*			
AR	1135+08	STANDARD 1976 D36-2 (2) HEADWALL	3.5*			
L & R	1189+00	STANDARD 1976 D24-2 (4) HEADWALL	15.2*			
R	1187+00	STANDARD 1976 D42-2 (2) HEADWALL	2.0*			
R	1205+35	SPECIAL PAVED DITCH INLET	2.0*			
L	1206+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
R	1207+65	SPECIAL PAVED DITCH INLET	2.8			
AR	1208+50	STANDARD 1997 D42-2 (2) HEADWALL	6.2*			
R	1209+35	SPECIAL PAVED DITCH INLET	2.8			
R	1212+65	SPECIAL PAVED DITCH INLET	2.0*			
L	1226+50	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
L	1244+35	SPECIAL PAVED DITCH INLET	2.0*			
L	1246+65	SPECIAL PAVED DITCH INLET	2.0*			
AR	1256+00	STANDARD 1997 D42-2 (2) HEADWALL	6.2*			
R	1259+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
AR	1267+00	STANDARD 1976 D36-2 (4) HEADWALL	7.0*			
R	1286+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
L	1289+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
L	1289+35	SPECIAL PAVED DITCH INLET	2.8			
R	1290+00	SPECIAL PAVED DITCH INLET	2.8			
R	1294+92	SPECIAL BOX CULVERT, DOUBLE 7'X6'	57.9			
R	1300	STANDARD 1973 D12-2 (1) HEADWALL	0.5*			
L	1300	SPECIAL CURB & GUTTER TRANSITION	5.6			
L	1300	STANDARD 1897T OUTLET	4.2			
R	1300	SPECIAL CURB & GUTTER TRANSITION	5.6			
R	1300	STANDARD 1897T OUTLET	2.1			
AR	1301+51.39	4 APPROACH SLABS FOR STRUCTURE 74-69VB	6.7			
L	1303+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
AR	1303+00	CONCRETE COLLAR	0.2*			
AR	1308+00	STANDARD 1976 D36-2 (2) HEADWALL	3.3*			
AR	1313+35	SPECIAL PAVED DITCH INLET	3.0*			
AR	1320+00	STANDARD 1976 D24-2 (4) HEADWALL	10.0*			
AR	1331+30	STANDARD 2051 D24-2 (4) HEADWALL	7.2*			
R	1342+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
AR	1356+00	STANDARD 1997 D42-2 (4) HEADWALL	15.2*			
AR	1367+00	2 SPECIAL 6'X2' BOX CULVERTS	53.7			
AR	1381+08	SPECIAL 6'X4' BOX CULVERT	85.1			
R	1382+71	STANDARD 1897T OUTLET	3.5			
R	1383+25	STANDARD 1897T OUTLET	3.5			
R	56+85	S.B.I.-10(RT. 47) CONNECTION	2.3			
R	56+85	SPECIAL PAVED DITCH INLET	2.3			
AR	68+85	SPECIAL PAVED DITCH INLET	1.3*			
AR	71+21	STANDARD 1976 D18-2 (2) HEADWALL	1.3*			
R	0+20	RELOC. T.R.-87	1.9			
L	0+20	SPECIAL PAVED DITCH INLET	1.9			
L	4+65	SPECIAL PAVED DITCH INLET	1.9			
L	4+65	SPECIAL PAVED DITCH INLET	1.9			
L	5+79.27	STANDARD 1897T OUTLET	3.5			
L	5+79.27	STANDARD 1897T OUTLET	3.5			
L	9+49.43	STANDARD 1897T OUTLET	3.5			
L	10+35	SPECIAL PAVED DITCH INLET	1.9			
L	10+35	SPECIAL PAVED DITCH INLET	1.9			
L	13+44	SPECIAL PAVED DITCH INLET	1.9			
R	13+44	SPECIAL PAVED DITCH INLET	1.9			
R	18+35	SPECIAL PAVED DITCH INLET	1.9			
R	21+65	SPECIAL PAVED DITCH INLET	1.9			
R	50+00	RELOC. RT. 47	3.3			
AR	50+50	2 APPROACH SLABS FOR STRUCTURE 74-69VB-2	10.4			
AR	50+75	2 SPECIAL CURB & GUTTER TRANSITIONS	13.8			
AR	59+00	STANDARD 1997 D42-2 (2) HEADWALL	6.2*			
AR	65+40	SPECIAL BOX CULVERT, DOUBLE 8'X5'	77.6			
L	100+84	STANDARD 1897T OUTLET	3.5			
R	100+84	STANDARD 1897T OUTLET	3.7			
AR	207+60	WHITE HEATH INTERCHANGE-RAMP B	2.0*			
L	215+63	STANDARD 1976 D24-2 (2) HEADWALL	3.5*			
R	215+63	STANDARD 1897T OUTLET	3.7			
R	216+70	SPECIAL INLET-TYPE 6 CURB & GUTTER	0.5			
R	300+20	WHITE HEATH INTERCHANGE-RAMP C	0.5			
L	300+82	SPECIAL INLET-TYPE 6 CURB & GUTTER	0.5			
R	300+82	STANDARD 1897T OUTLET	3.5			
AR	300+82	STANDARD 1897T OUTLET	7.6			
AR	309+00	STANDARD 1976 D24-2 (2) HEADWALL	2.0*			
AR	415+70	WHITE HEATH INTERCHANGE-RAMP D	3.5*			
L	419+80	STANDARD 1897T OUTLET	3.5			
L	419+80	STANDARD 1897T OUTLET	7.6			
L	48+25	RELOC. T.R. 154	5.2			
L	48+30	STANDARD 1897T OUTLET	5.2			
L	48+60	SPECIAL CURB & GUTTER TRANSITION	5.2			
L	48+60	SPECIAL CURB & GUTTER TRANSITION	5.2			
L	51+30	SPECIAL CURB & GUTTER TRANSITION	5.2			
L	51+30	SPECIAL CURB & GUTTER TRANSITION	5.2			
L	51+60	STANDARD 1897T OUTLET	5.6			
L	51+60	STANDARD 1897T OUTLET	6.2			
L	22+85	T.R. - 134 CONNECTION	1.9			
L	23+65	SPECIAL PAVED DITCH INLET	1.9			
L	23+65	SPECIAL PAVED DITCH INLET	1.9			
*CLASS X CONCRETE (NDWL.)						
TOTAL CLASS X CONCRETE (NDWL.)			105.0	CU. YDS.		
TOTAL CLASS X CONCRETE			952.1	CU. YDS.		

SUMMARY OF QUANTITIES

CODE NUMBER	QUANTITY	UNIT	ITEM	CODE NUMBER
010001	279	240	LIN. FT.	066025
010002	1970	846	LIN. FT.	066026
010006	2	690	LIN. FT.	066392
011001	276	78	EACH	075038
012001	466	550	LIN. FT.	078001
013001	1376	1,325	LIN. FT.	080006
024001	822	64	LIN. FT.	080008
026001	49	30	SO. YDS.	080010
026003	8141	8,189	SO. YDS.	081018
027001	7334	3,372	SO. YDS.	082001
029003	1125	1,106	LIN. FT.	082001
035001	97	572	LIN. FT.	091005
037001	225	120	CU. YDS.	094001
037002	3.2	8.9	TONS	094001
039001	219	216	EACH	098001
039003	172	25	ACRES	101006
039006	172	25	ACRES	102001
046001	5226	14	TONS	104001
048006	237	200	TONS	104001
048007	2900	1,500	TONS	110004
048008	20,000	845	TONS	110005
048011	75	4	TONS	110006
048012	75	4	TONS	110008
048018	10	16	EACH	111002
049001	38	32	EACH	111003
052003	590	500	EACH	112001
052016	1	1	LUMP SUM	112001
058076	1	1	LUMP SUM	112001
058080	1	1	LUMP SUM	112001
058082	1	1	LUMP SUM	112001
058089	1	1	LUMP SUM	112001
058103	1	1	LUMP SUM	112001
058104	1	1	LUMP SUM	112001
058126	1	1	LUMP SUM	112001
058169	1	1	LUMP SUM	112001
058200	1	1	LUMP SUM	112001
058202	1	1	LUMP SUM	112001
058206	1	1	LUMP SUM	112001
058207	1	1	LUMP SUM	112001
058208	1	1	LUMP SUM	112001
058222	1	1	LUMP SUM	112001
058223	1	1	LUMP SUM	112001
058397	1	1	LUMP SUM	112001
059001	1	1	LUMP SUM	112001
066003	1	1	LUMP SUM	112001
066024	1	1	LUMP SUM	112001

SUMMARY OF QUANTITIES AND CLASS X CONCRETE

REVISIONS	STATE OF ILLINOIS	DATE	BY
1	DEPARTMENT OF PUBLIC WORKS AND BUILDINGS		
2	DIVISION OF HIGHWAYS		
3	P.A.-135		
4	SECTION 74-69		
5	PIATT		
6	COUNTY		
7	HOMER L. CHASTAIN & ASSOCIATES		
8	CONSULTING ENGINEERS		
9	DECATUR, ILLINOIS		
10			

N

F.A. ROUTE 135-SECTION 74-69
 LOTS 1, 2, 3, AND 4, SUBDIVISION
 OF THE S 2/3, NW 1/4, SEC. 6, T18N, R6E, 3PM
 PIATT COUNTY, ILLINOIS

SURVEYED C.L. OF PROPOSED
 S.B.I. 10(RTE. 47) CONNECTION

LOT 2

LOT 1

S.B.I. 10(ROUTE 47) CURVE DATA

$\Delta = 55^{\circ}15'00''$
 $L_s = 200.00'$
 $L_c = 763.66'$
 $T_s = 623.85'$
 $D_c = 5^{\circ}44'00''$
 $\theta_s = 5^{\circ}44'00''$
 $R_c = 999.35'$
 $E_s = 130.47'$

LOT 3

LOT 4

PROPOSED R.O.W.

WABASH R.R.

LOCAL TANGENT

$53^{\circ}02'25''$

P.L. STA. 57+10.3

EXISTING ILL. RTE. 47

SURVEYED C.L. OF PROPOSED F.A.-135

EXISTING R.O.W. LINES

F.A.-135 CURVE DATA

$\Delta = 25^{\circ}23'13''$
 $D = 1^{\circ}06'00''$
 $R = 5,203.71'$
 $T = 1,173.21'$
 $L = 2,307.90'$
 $E = 130.49'$

S.C. STA. 52+51.15

T.S. STA. 50+51.15

STA. 1106+82.43 (F.A.-135)
 = STA. 49+84.47 S.B.I. 10(ROUTE 47) CONNECTION

$36^{\circ}40'05''$ TO LOCAL TANGENT

TRACT NO. 74-69-5
 KILLIS DOSS AND
 ELFREDA DOSS

TOTAL AREA = 10.18 ACRES±

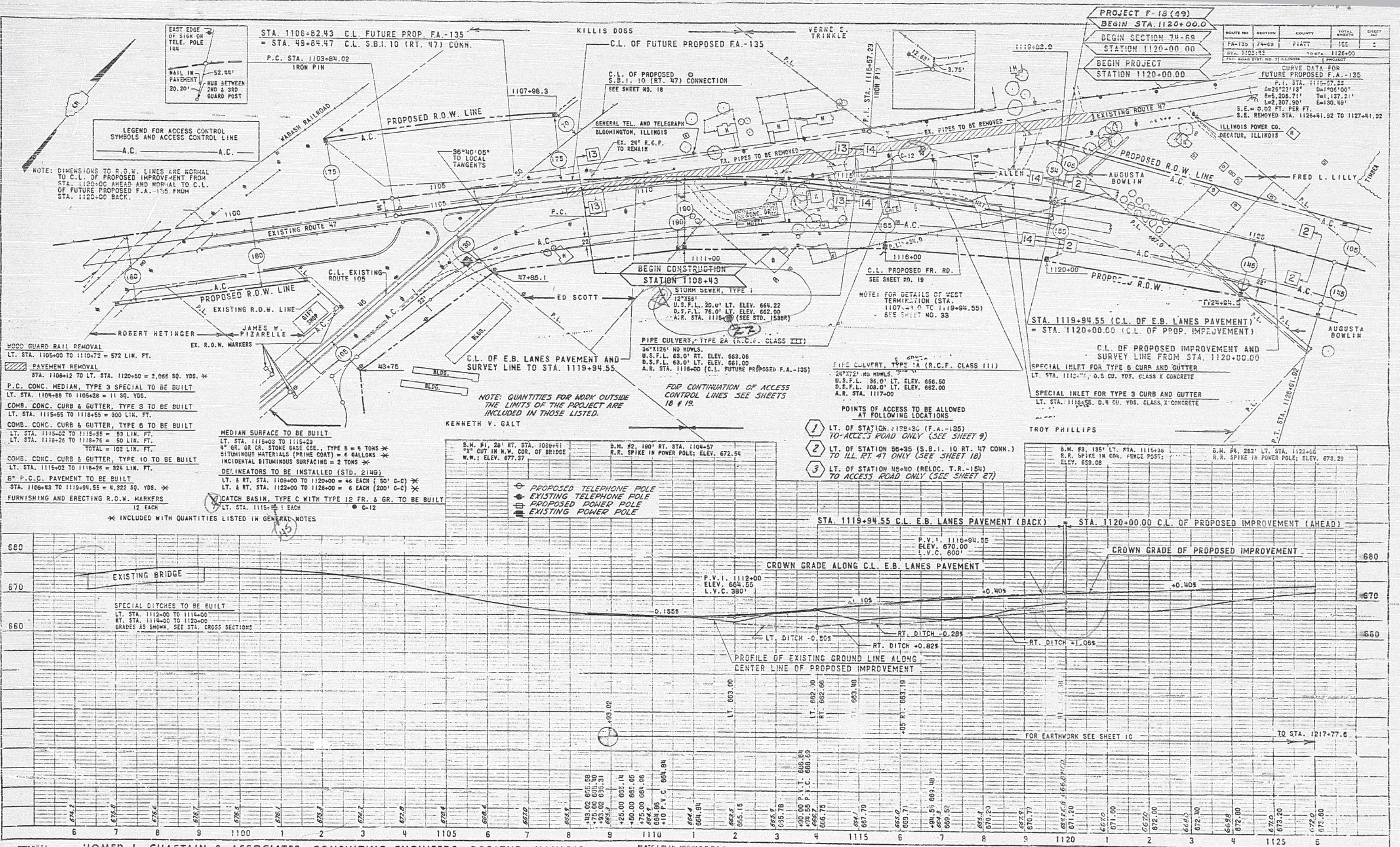
DWN. E.H.	CKD. R.W.	SCALE 1"=100'	DATE Feb. '60
REVISIONS	REV. BY	DATE	CKD. BY DATE
AREA	C.W.R.	7/8/60	

P.C. STA. 1103+84.02

DATE: 12-24-60
BY: H.L.C.
CHECKED: H.L.C.
APPROVED: H.L.C.
PROJECT: 1120-00
SHEET: 10
H-2-8

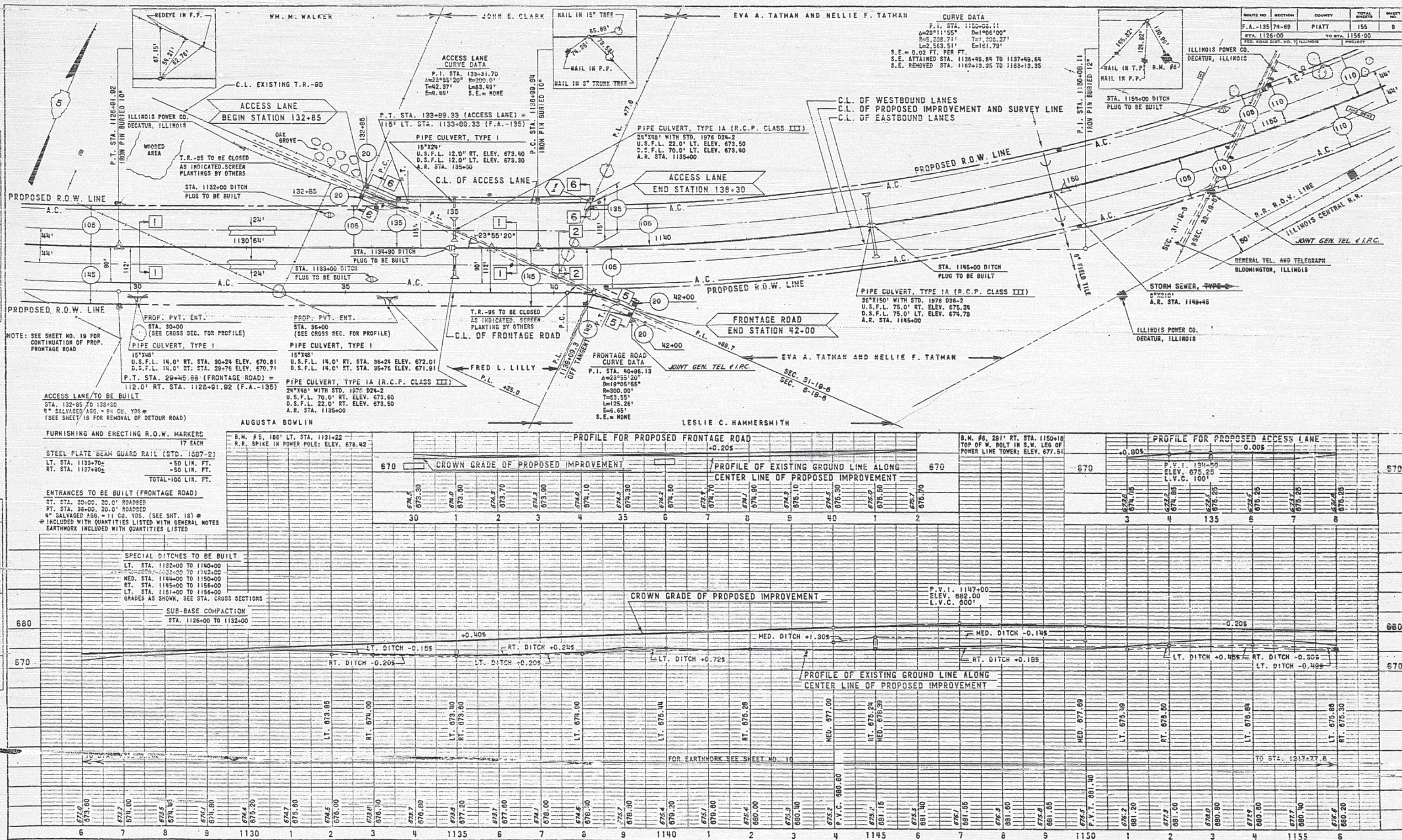
ENTIRE SECTION INSPECTED
AND APPROVED AS TO
DISTRICT ENGINEER
DATE: 1-30-61

DATE: 12-24-60
BY: H.L.C.
CHECKED: H.L.C.
APPROVED: H.L.C.
PROJECT: 1120-00
SHEET: 10
H-2-8

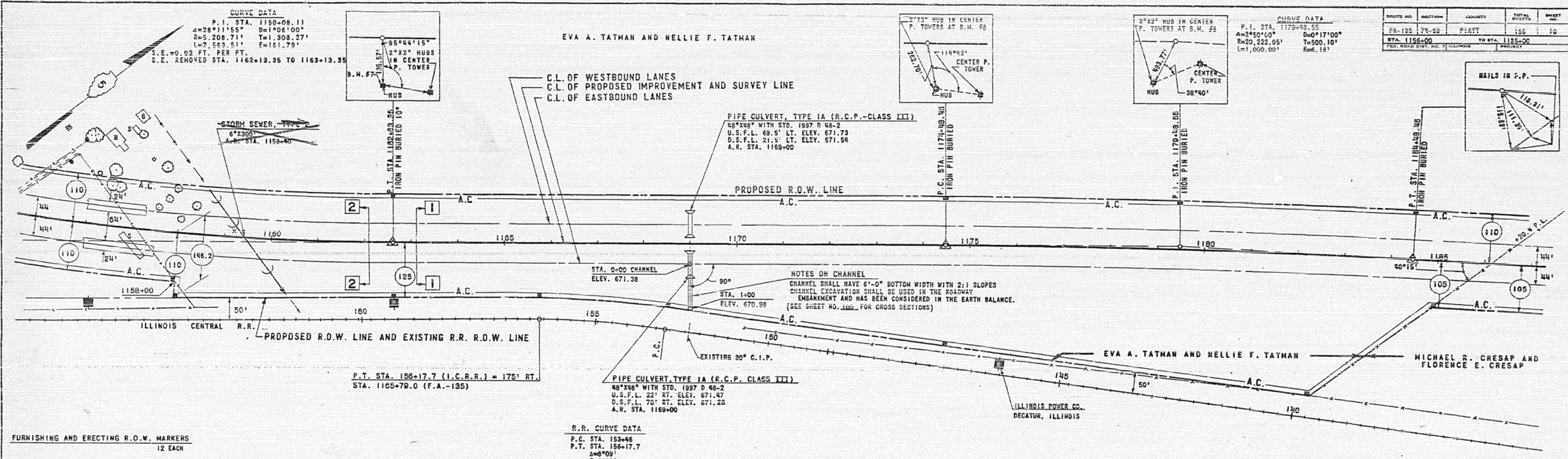


DATE	BY	REVISION
10-13-59	C.L.	1. PREPARED
10-13-59	C.L.	2. CHECKED
10-13-59	C.L.	3. APPROVED
10-13-59	C.L.	4. REVISION
10-13-59	C.L.	5. REVISION
10-13-59	C.L.	6. REVISION
10-13-59	C.L.	7. REVISION
10-13-59	C.L.	8. REVISION
10-13-59	C.L.	9. REVISION
10-13-59	C.L.	10. REVISION

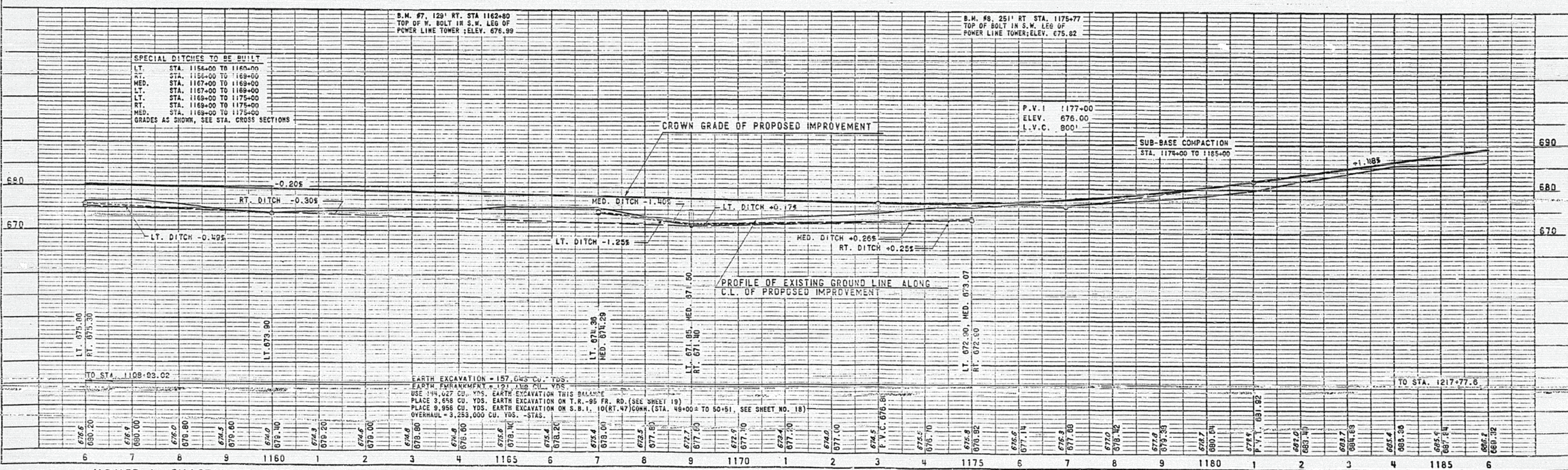
DATE	BY	REVISION
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10-13-59	C.L.	2. CHECKED
10-13-59	C.L.	3. APPROVED
10-13-59	C.L.	4. REVISION
10-13-59	C.L.	5. REVISION
10-13-59	C.L.	6. REVISION
10-13-59	C.L.	7. REVISION
10-13-59	C.L.	8. REVISION
10-13-59	C.L.	9. REVISION
10-13-59	C.L.	10. REVISION

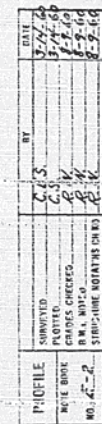


DATE 3-27-49
 BY C.E.S.
 CHECKED S.C.
 REVISIONS
 NO. 0-1



DATE 3-27-49
 BY C.E.S.
 CHECKED S.C.
 REVISIONS
 NO. 0-1

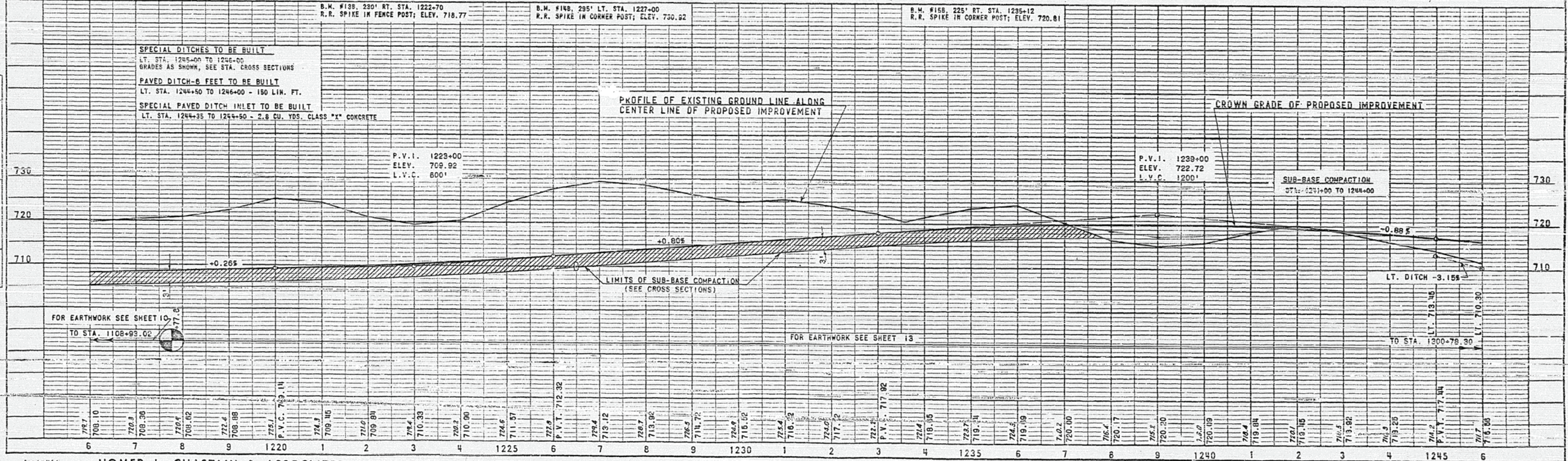
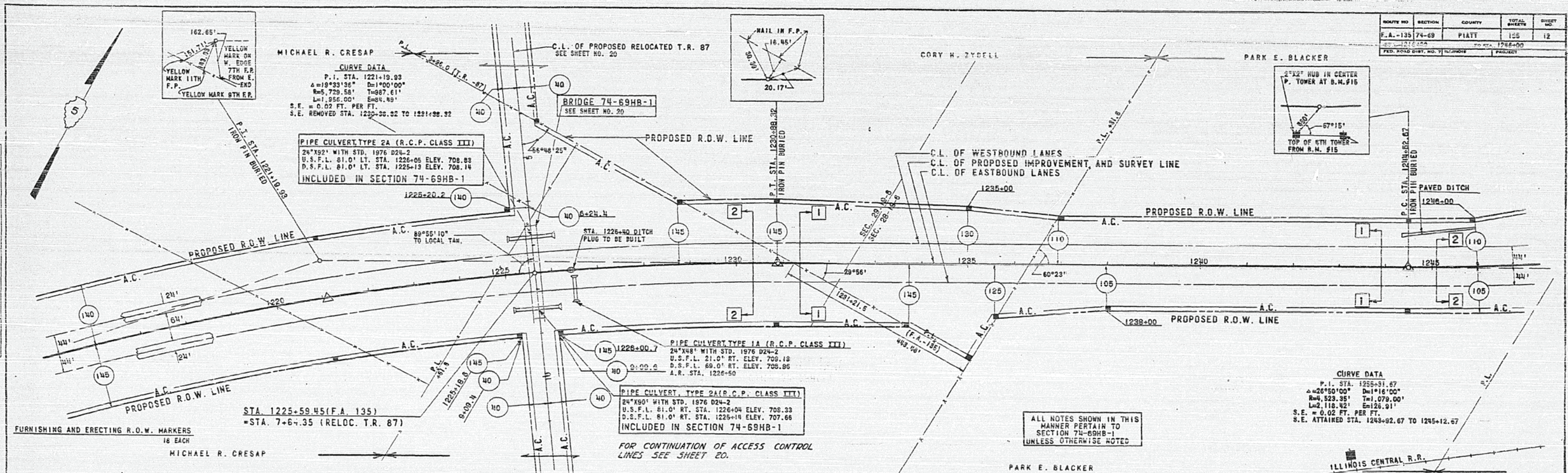


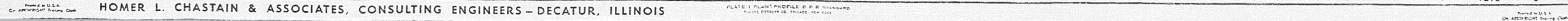


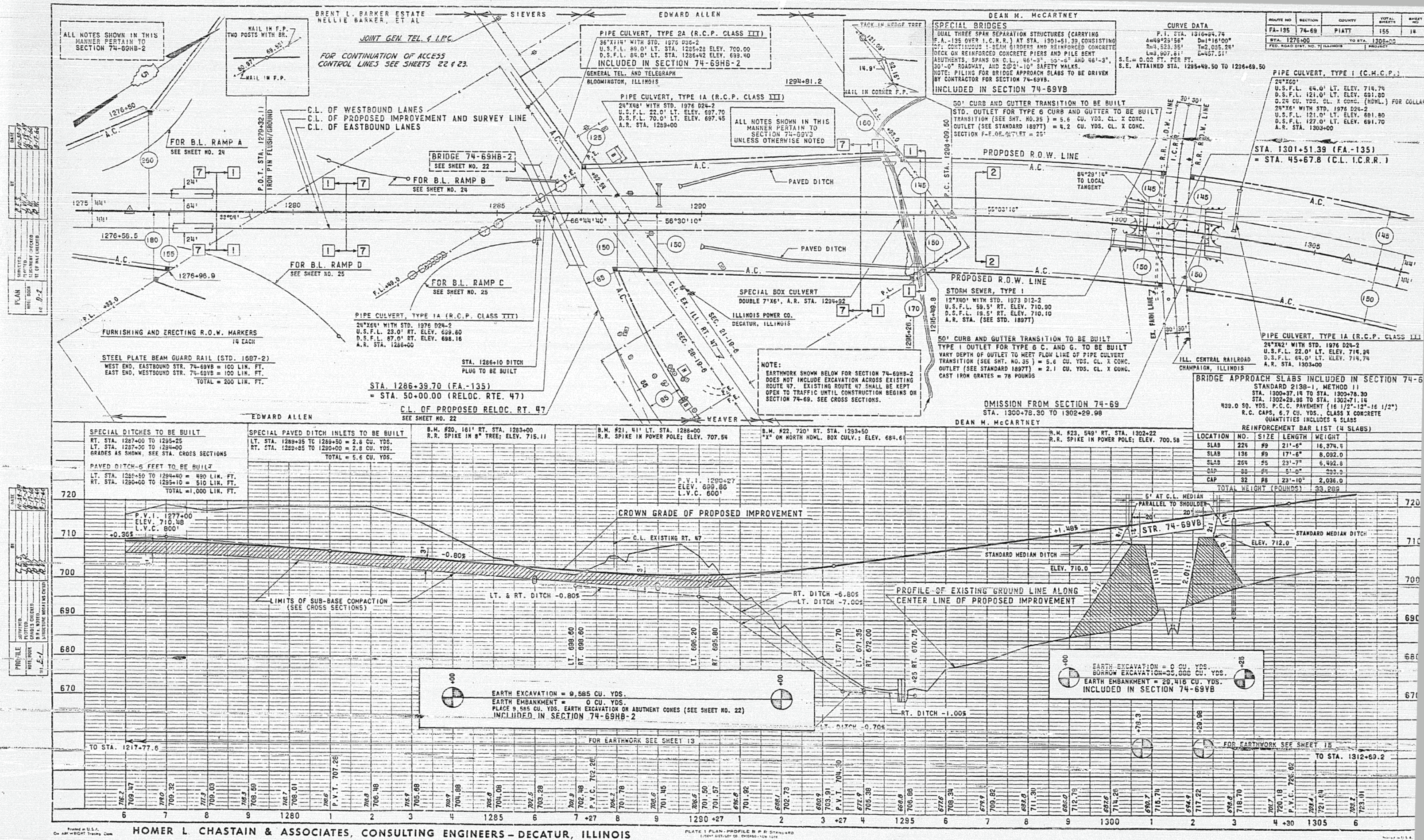
DATE	BY	REVISION
3-22-60	J.E.S.	DESIGNED
3-24-60	J.E.S.	FORWARDED
3-24-60	J.E.S.	ALIGNED CHECKED
3-24-60	J.E.S.	BY OF WAY CHECKED
3-24-60	J.E.S.	NO. 0-2

DATE	BY	REVISION
3-22-60	J.E.S.	DESIGNED
3-24-60	J.E.S.	FORWARDED
3-24-60	J.E.S.	ALIGNED CHECKED
3-24-60	J.E.S.	BY OF WAY CHECKED
3-24-60	J.E.S.	NO. 0-2

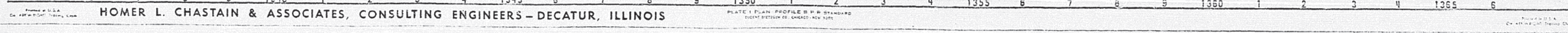
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
E.A.-135	74-69	PIATT	125	12
PROJECT				







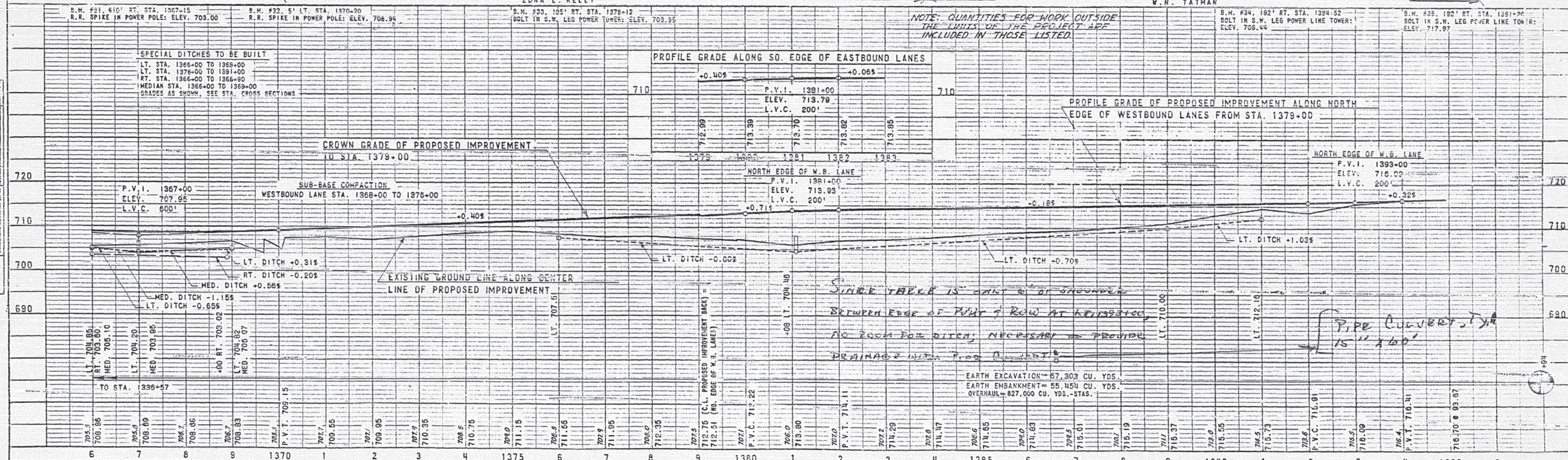
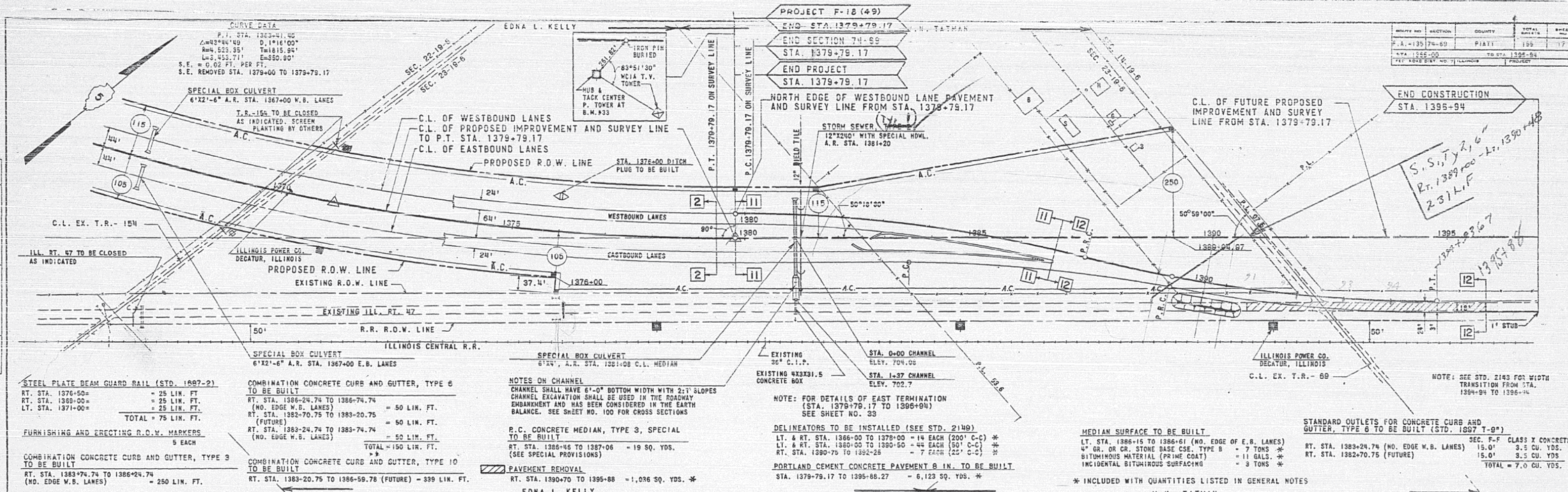
PLAN	DATE	BY	DATE
NOTE BOOK	5-17-60	C. L. E.	
	5-18-60	C. A. E.	
	8-11-60	R. W.	
NO. 1-2	8-11-60	R. W.	

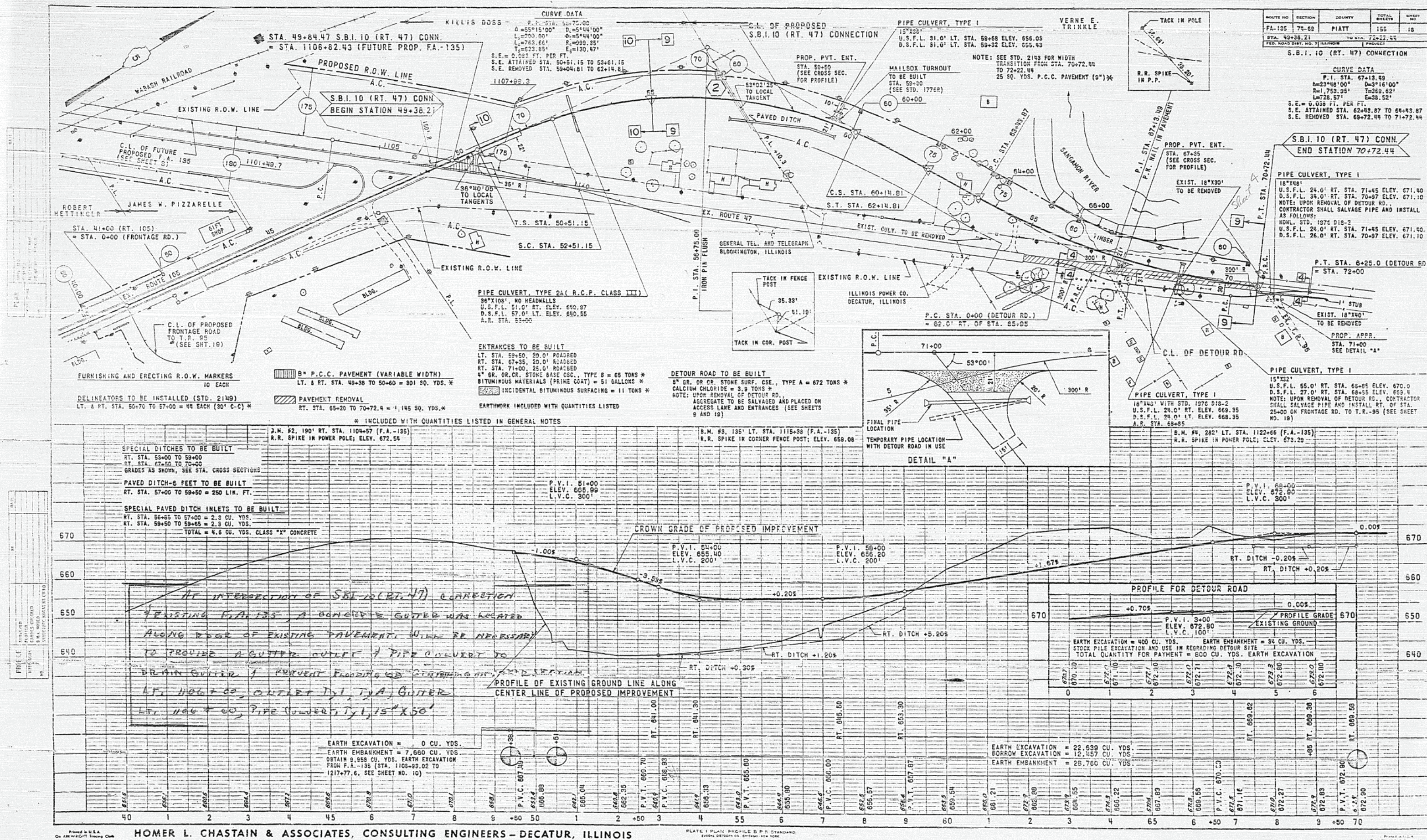


DATE: 10-1-64
BY: H.L.C.
CHECKED: H.L.C.
NO. 428

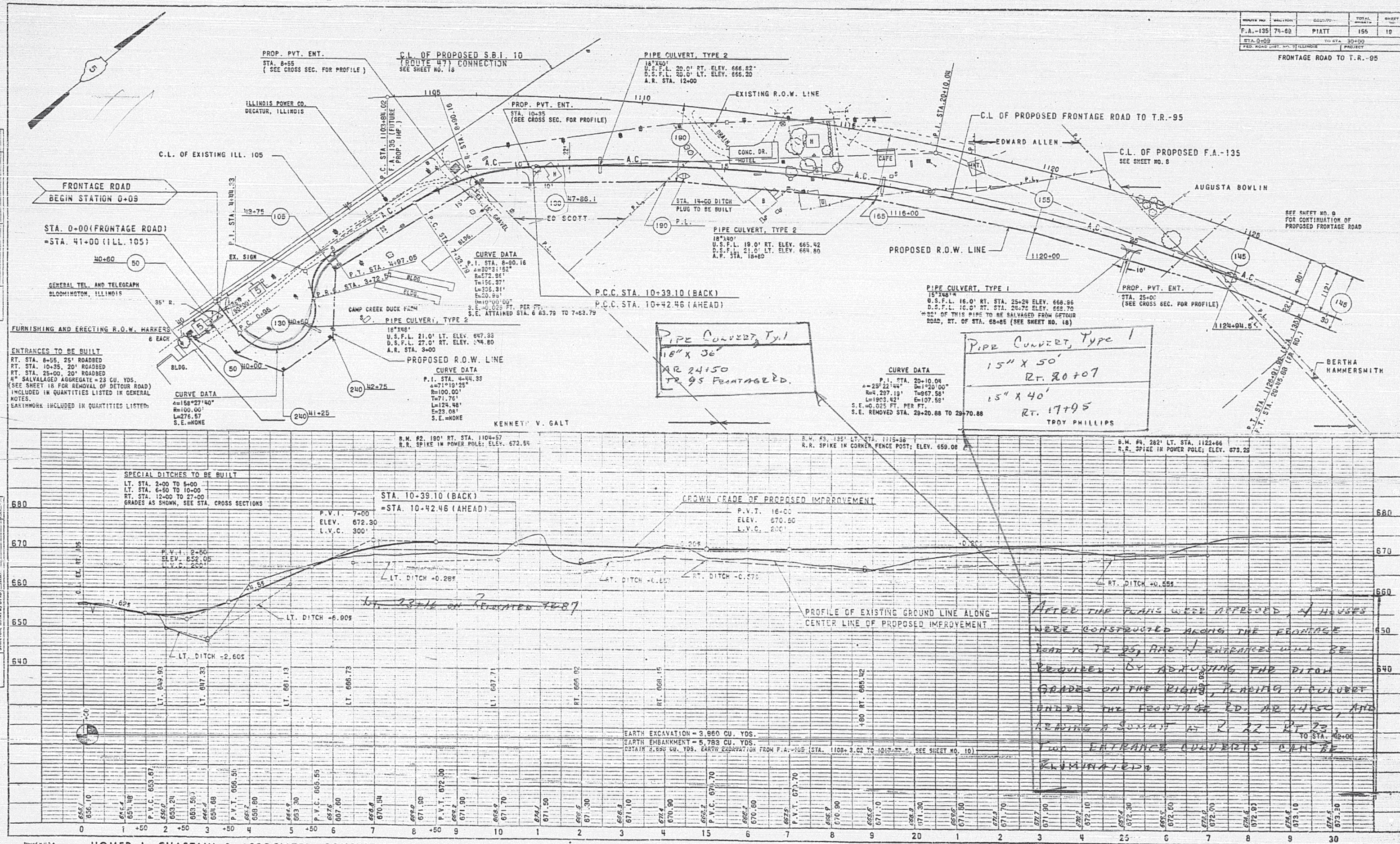
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NO. 428

DATE: 10-1-64
BY: H.L.C.
CHECKED: H.L.C.
NO. 428



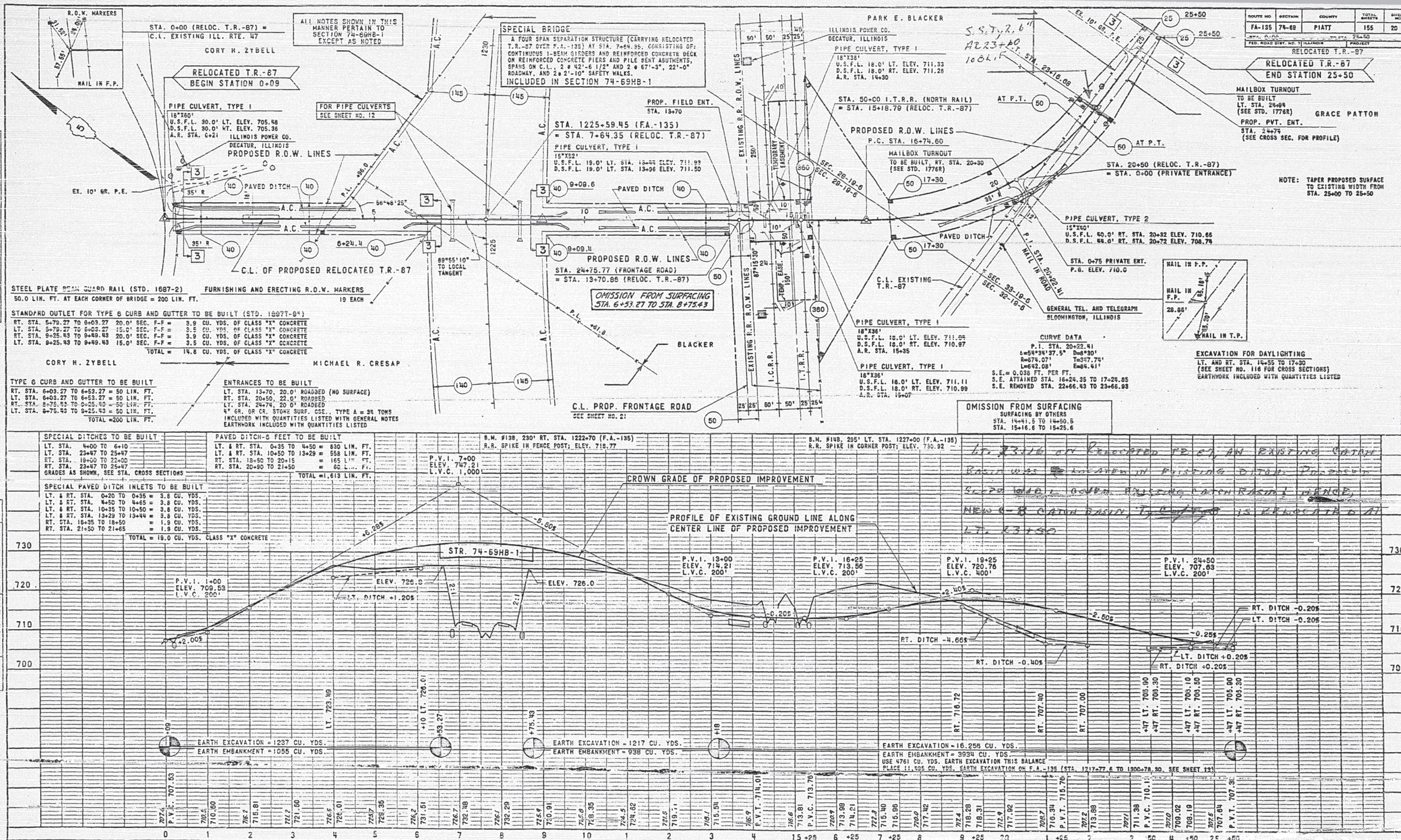


ROUTE NO.	SECTION	COUNTY	TOTAL MILEAGE	SHEET
F.A.-135	74-00	PIATT	155	19
STA. 0+00		TO STA. 30+00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		
FRONTAGE ROAD TO T.R.-05				



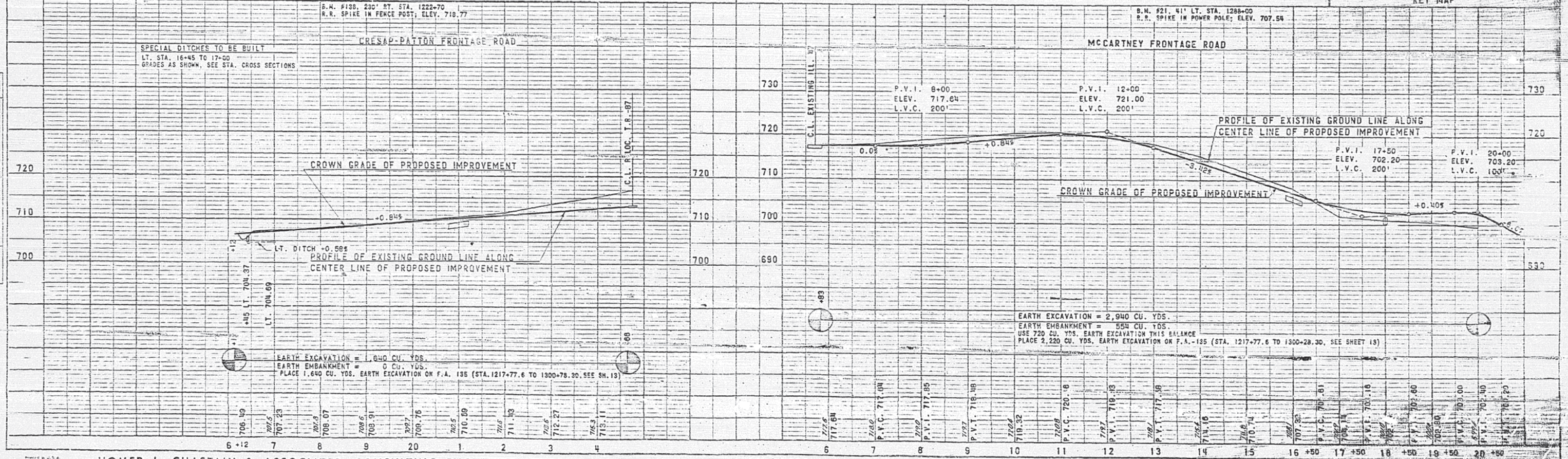
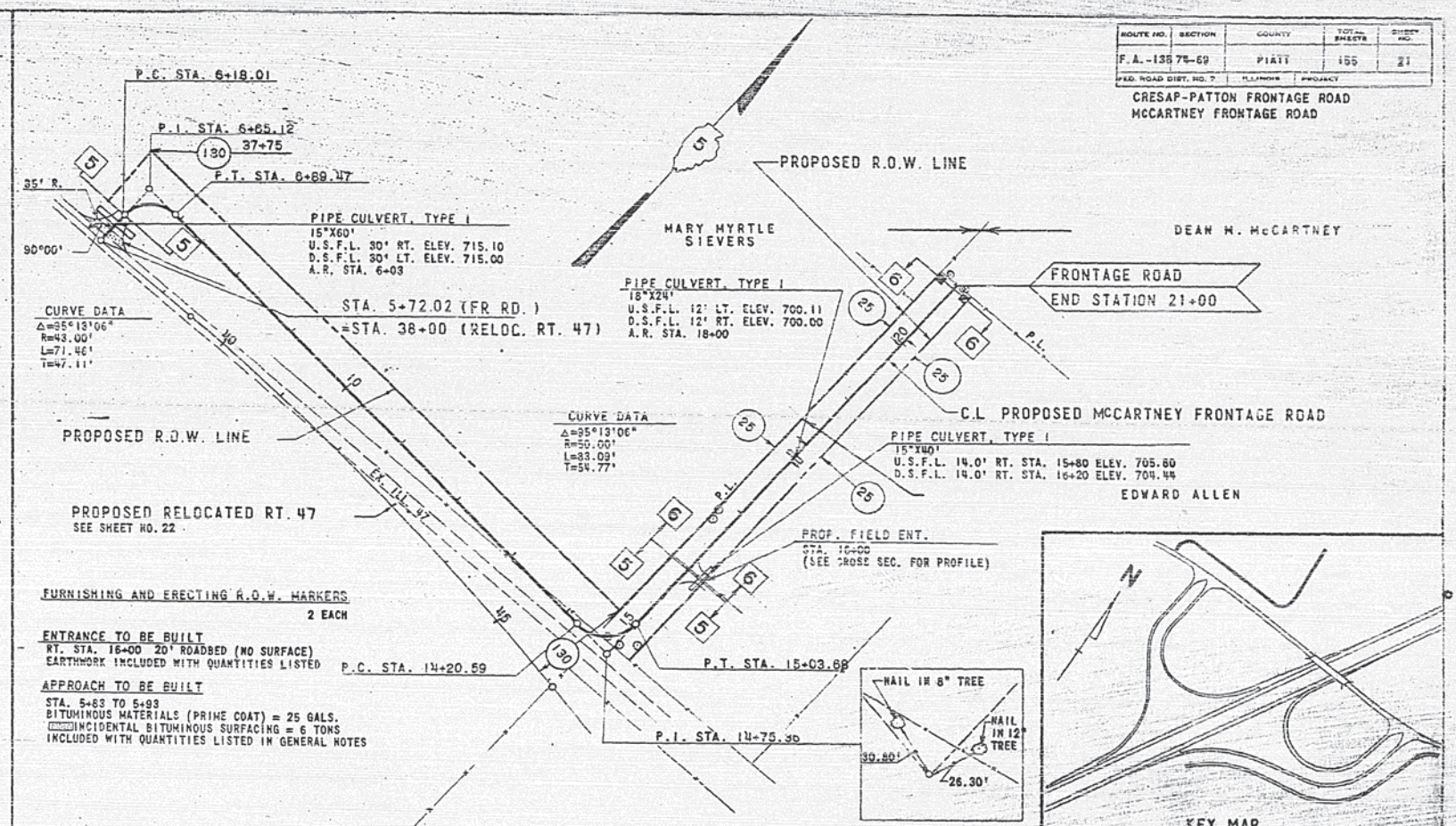
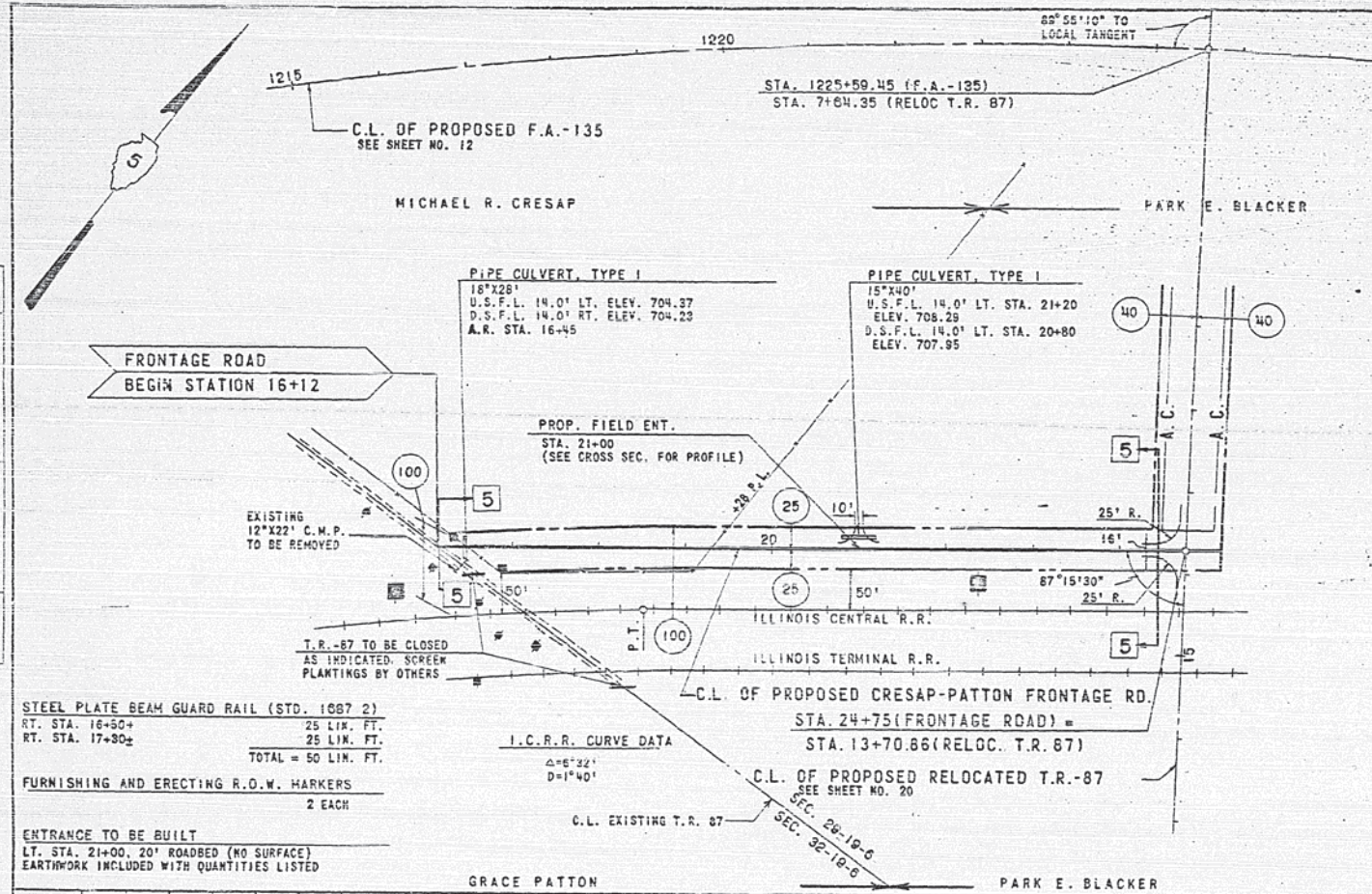
DATE	BY	REVISION
5-12-66	C.H.C.	1. PREPARED
5-12-66	C.H.C.	2. CHECKED
5-12-66	C.H.C.	3. APPROVED
5-12-66	C.H.C.	4. REVISION
5-12-66	C.H.C.	5. REVISION
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5-12-66	C.H.C.	12. REVISION
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5-12-66	C.H.C.	61. REVISION
5-12-66	C.H.C.	62. REVISION
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5-12-66	C.H.C.	79. REVISION
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5-12-66	C.H.C.	81. REVISION
5-12-66	C.H.C.	82. REVISION
5-12-66	C.H.C.	83. REVISION
5-12-66	C.H.C.	84. REVISION
5-12-66	C.H.C.	85. REVISION
5-12-66	C.H.C.	86. REVISION
5-12-66	C.H.C.	87. REVISION
5-12-66	C.H.C.	88. REVISION
5-12-66	C.H.C.	89. REVISION
5-12-66	C.H.C.	90. REVISION
5-12-66	C.H.C.	91. REVISION
5-12-66	C.H.C.	92. REVISION
5-12-66	C.H.C.	93. REVISION
5-12-66	C.H.C.	94. REVISION
5-12-66	C.H.C.	95. REVISION
5-12-66	C.H.C.	96. REVISION
5-12-66	C.H.C.	97. REVISION
5-12-66	C.H.C.	98. REVISION
5-12-66	C.H.C.	99. REVISION
5-12-66	C.H.C.	100. REVISION

DATE	BY	REVISION
5-12-66	C.H.C.	1. PREPARED
5-12-66	C.H.C.	2. CHECKED
5-12-66	C.H.C.	3. APPROVED
5-12-66	C.H.C.	4. REVISION
5-12-66	C.H.C.	5. REVISION
5-12-66	C.H.C.	6. REVISION
5-12-66	C.H.C.	7. REVISION
5-12-66	C.H.C.	8. REVISION
5-12-66	C.H.C.	9. REVISION
5-12-66	C.H.C.	10. REVISION
5-12-66	C.H.C.	11. REVISION
5-12-66	C.H.C.	12. REVISION
5-12-66	C.H.C.	13. REVISION
5-12-66	C.H.C.	14. REVISION
5-12-66	C.H.C.	15. REVISION
5-12-66	C.H.C.	16. REVISION
5-12-66	C.H.C.	17. REVISION
5-12-66	C.H.C.	18. REVISION
5-12-66	C.H.C.	19. REVISION
5-12-66	C.H.C.	20. REVISION
5-12-66	C.H.C.	21. REVISION
5-12-66	C.H.C.	22. REVISION
5-12-66	C.H.C.	23. REVISION
5-12-66	C.H.C.	24. REVISION
5-12-66	C.H.C.	25. REVISION
5-12-66	C.H.C.	26. REVISION
5-12-66	C.H.C.	27. REVISION
5-12-66	C.H.C.	28. REVISION
5-12-66	C.H.C.	29. REVISION
5-12-66	C.H.C.	30. REVISION
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5-12-66	C.H.C.	32. REVISION
5-12-66	C.H.C.	33. REVISION
5-12-66	C.H.C.	34. REVISION
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5-12-66	C.H.C.	38. REVISION
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5-12-66	C.H.C.	40. REVISION
5-12-66	C.H.C.	41. REVISION
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5-12-66	C.H.C.	44. REVISION
5-12-66	C.H.C.	45. REVISION
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5-12-66	C.H.C.	47. REVISION
5-12-66	C.H.C.	48. REVISION
5-12-66	C.H.C.	49. REVISION
5-12-66	C.H.C.	50. REVISION
5-12-66	C.H.C.	51. REVISION
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5-12-66	C.H.C.	60. REVISION
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5-12-66	C.H.C.	62. REVISION
5-12-66	C.H.C.	63. REVISION
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5-12-66	C.H.C.	65. REVISION
5-12-66	C.H.C.	66. REVISION
5-12-66	C.H.C.	67. REVISION
5-12-66	C.H.C.	68. REVISION
5-12-66	C.H.C.	69. REVISION
5-12-66	C.H.C.	70. REVISION
5-12-66	C.H.C.	71. REVISION
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5-12-66	C.H.C.	73. REVISION
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5-12-66	C.H.C.	75. REVISION
5-12-66	C.H.C.	76. REVISION
5-12-66	C.H.C.	77. REVISION
5-12-66	C.H.C.	78. REVISION
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5-12-66	C.H.C.	82. REVISION
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5-12-66	C.H.C.	94. REVISION
5-12-66	C.H.C.	95. REVISION
5-12-66	C.H.C.	96. REVISION
5-12-66	C.H.C.	97. REVISION
5-12-66	C.H.C.	98. REVISION
5-12-66	C.H.C.	99. REVISION
5-12-66	C.H.C.	100. REVISION



DATE	BY	REVISION
6-17-69	C.E.S.	1. PREPARED
8-18-69	E.R.W.	2. CHECKED
8-17-69	R.W.	3. IN CHARGE
9-17-69	R.W.	4. IN CHARGE
10-12-69	R.W.	5. IN CHARGE

DATE	BY	REVISION
6-17-69	C.E.S.	1. PREPARED
8-18-69	E.R.W.	2. CHECKED
8-17-69	R.W.	3. IN CHARGE
9-17-69	R.W.	4. IN CHARGE
10-12-69	R.W.	5. IN CHARGE



BRENT L. BARKER ESTATE
NELLIE BARKER, ET AL

MARY MYRTLE SIEVERS

RELOCATED RT. 47
CURVE DATA
P.I. STA. 42+80.51
Δ=95°57'00"
D=1°30'00"
R=3,819.72'
T=332.50'
L=663.33'
E=14.44'

FURNISHING AND ERECTING R.O.W. MARKERS
5 EACH

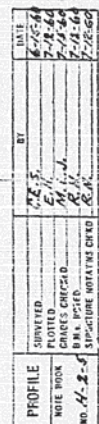
RELOCATED RT. 47
CURVE DATA
P.I. STA. 59+66.83
Δ=41°05'00"
D=5°16'00"
R=1,753.95'
T=669.33'
L=1,278.63'
E=13.37'

DOROTHY JEAN WEAVER

GENERAL PLAN-INTERSECTION RELOC. RT. 47 WITH FA-135

REVISIONS		STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS		DESIGNED BY CHECKED BY DATE
1		F.A.-135	SECTION 74-69	PIATT
2		PIATT	COUNTY	
3		HOMER L. CHASTAIN & ASSOCIATES CONSULTING ENGINEERS DECATUR, ILLINOIS		

NOTE: RAMP PAYMENT FROM STA. 200+00.00 TO STA. 206+81.21 (RAMP B) SHALL BE CONSTRUCTED IN CONTRASTING COLOR, SEE SPECIAL PROVISIONS

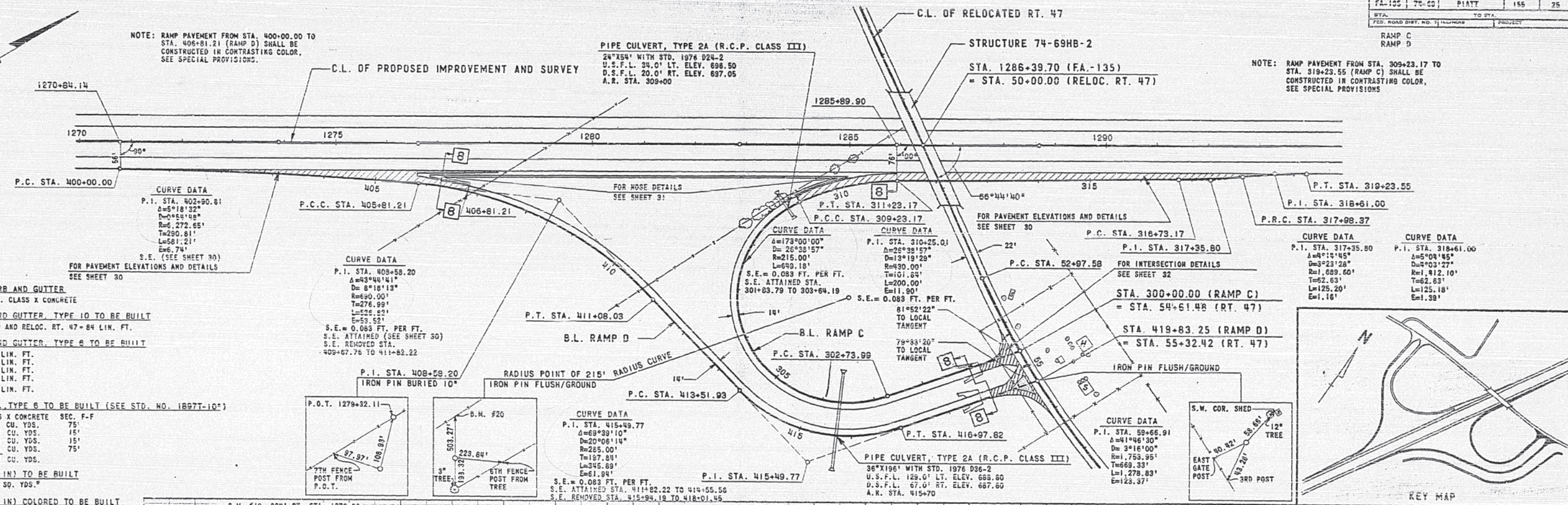


ROUTE NO.	SECTION	COUNTY	TOTAL LENGTH	SHEET NO.
FA-135	75-50	PIATT	155	25
STA.		TO STA.		
FED. ROAD DIST. NO. 71		ALPHABETIC	PROJECT	

RAMP C
RAMP D

NOTE: RAMP PAVEMENT FROM STA. 309+23.17 TO STA. 319+23.55 (RAMP C) SHALL BE CONSTRUCTED IN CONTRASTING COLOR, SEE SPECIAL PROVISIONS

NOTE: RAMP PAVEMENT FROM STA. 400+00.00 TO STA. 405+81.21 (RAMP D) SHALL BE CONSTRUCTED IN CONTRASTING COLOR, SEE SPECIAL PROVISIONS.



SPECIAL INLET FOR TYPE 6 CURB AND GUTTER
RT. OF STA. 300+20±, 0.5 CU. YDS. CLASS X CONCRETE

COMBINATION CONCRETE CURB AND GUTTER, TYPE 10 TO BE BUILT
ISLAND AT INTERSECTION OF RAMP D AND RELOC. RT. 47-84 LIN. FT.

COMBINATION CONCRETE CURB AND GUTTER, TYPE 6 TO BE BUILT
RT. STA. 300+11 TO 300+82 = 112 LIN. FT.
LT. STA. 300+11 TO 300+82 = 73 LIN. FT.
LT. STA. 418+80 TO 419+62 = 90 LIN. FT.
RT. STA. 418+80 TO 419+72 = 153 LIN. FT.
TOTAL = 428 LIN. FT.

STD. OUTLETS FOR CONC. C&G, TYPE 6 TO BE BUILT (SEE STD. NO. 1897T-10*)

CLASS X CONCRETE	SEC. F-F
RT. STA. 300+82 TO 301+02 = 7.6 CU. YDS.	75'
LT. STA. 300+82 TO 301+02 = 2.5 CU. YDS.	15'
LT. STA. 418+80 TO 419+80 = 2.5 CU. YDS.	15'
RT. STA. 418+80 TO 419+80 = 7.6 CU. YDS.	75'
TOTAL = 22.2 CU. YDS.	

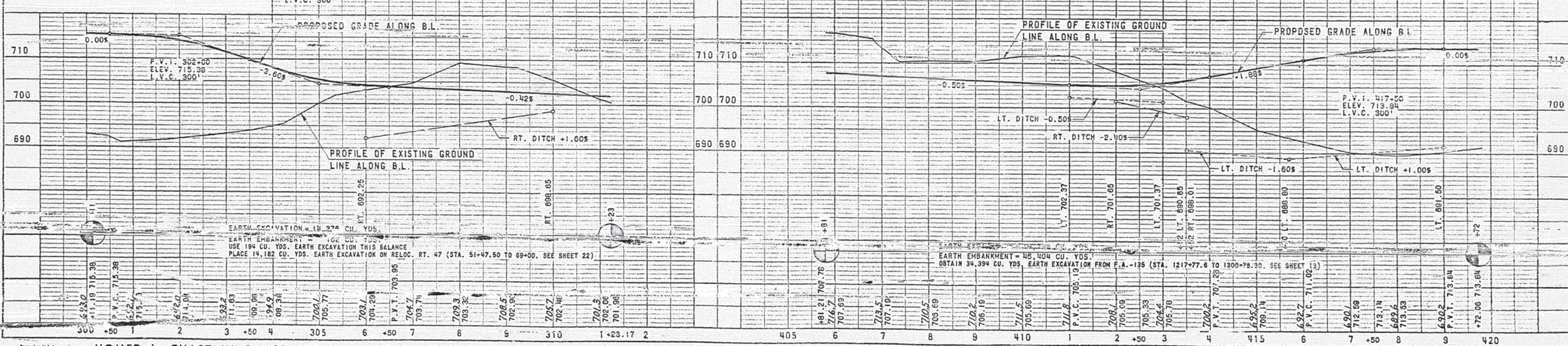
P.C.C. PAVEMENT (10 IN) TO BE BUILT
RAMP C AND RAMP D 572 SQ. YDS.*

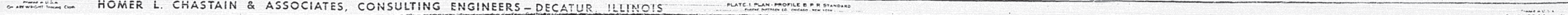
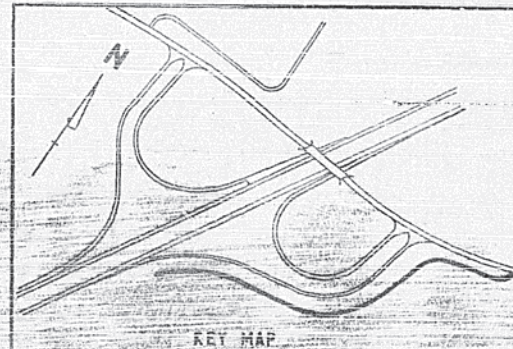
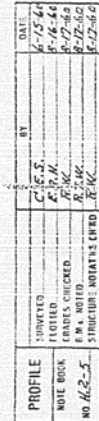
P.C.C. PAVEMENT (10 IN) COLORED TO BE BUILT
STA. 400+00 TO 405+81.21 1,041 SQ. YDS.*
STA. 309+23.17 TO 319+23.55 1,438 SQ. YDS.*

ISLAND SURFACE TO BE BUILT
STA. 419+47 TO 419+70
4" GR. OR CR. STONE BASE CSE, TYPE B = 8 TONS*
BITUMINOUS MATERIALS (PRIME COAT) = 12 GALST*
INCIDENTAL BITUMINOUS SURFACING = 3 TONS*

DELINEATORS TO BE INSTALLED (STANDARD 2149)
RT. STA. 309+00 TO 318+00 = 10 EACH (100' C-C)*
RT. STA. 400+00 TO 407+00 = 8 EACH (100' C-C)*

*INCLUDED WITH QUANTITIES LISTED IN GENERAL NOTES



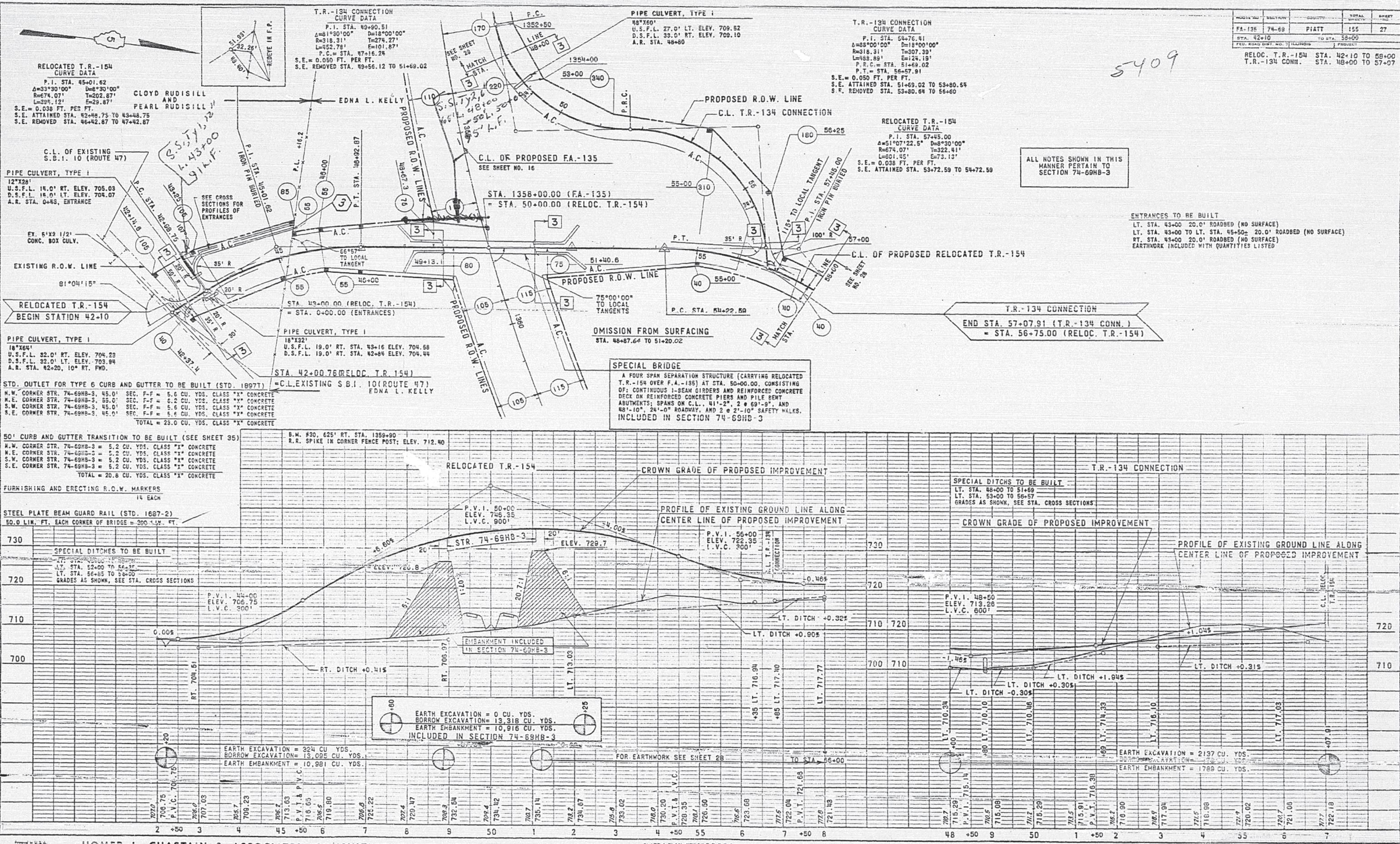
BARKER FRONTAGE ROAD

SECTION	DATE	BY	CHECKED	TOTAL	SHEET
FA-135	7-4-68	PIATT		155	27
STA. 42+10				TO STA. 58+00	
RELOC. T.R.-154				STA. 42+10 TO 58+00	
T.R.-134 CONN.				STA. 48+00 TO 57+07	

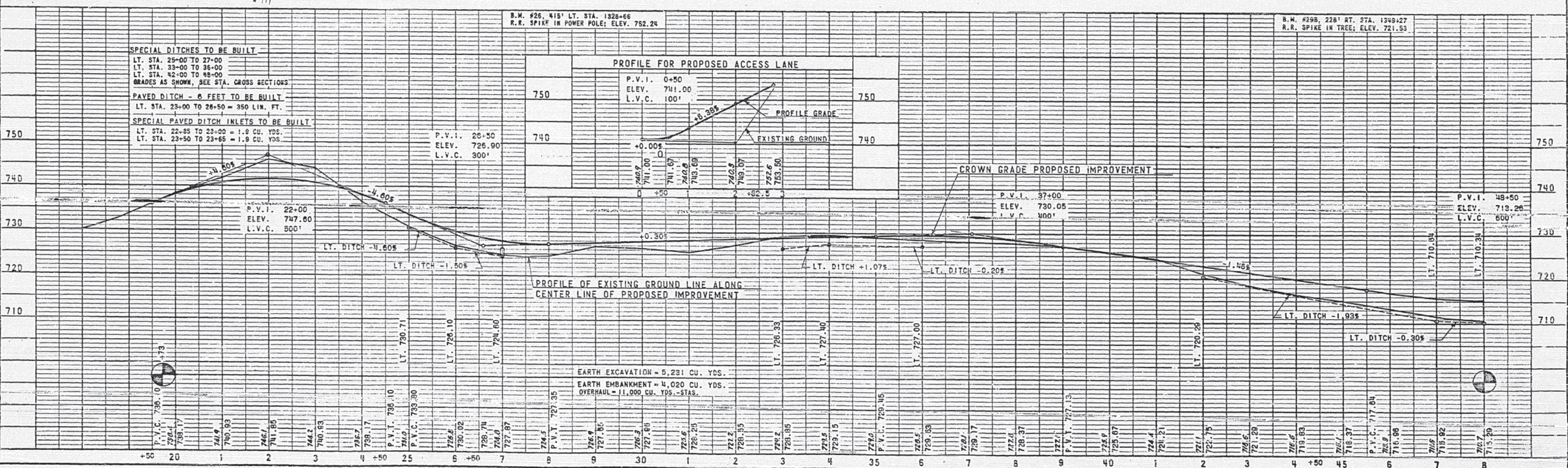
5409

DATE	BY	CHECKED	REVISION
7-4-68	PIATT		
7-11-68	PIATT		
8-1-68	PIATT		
8-15-68	PIATT		
8-19-68	PIATT		

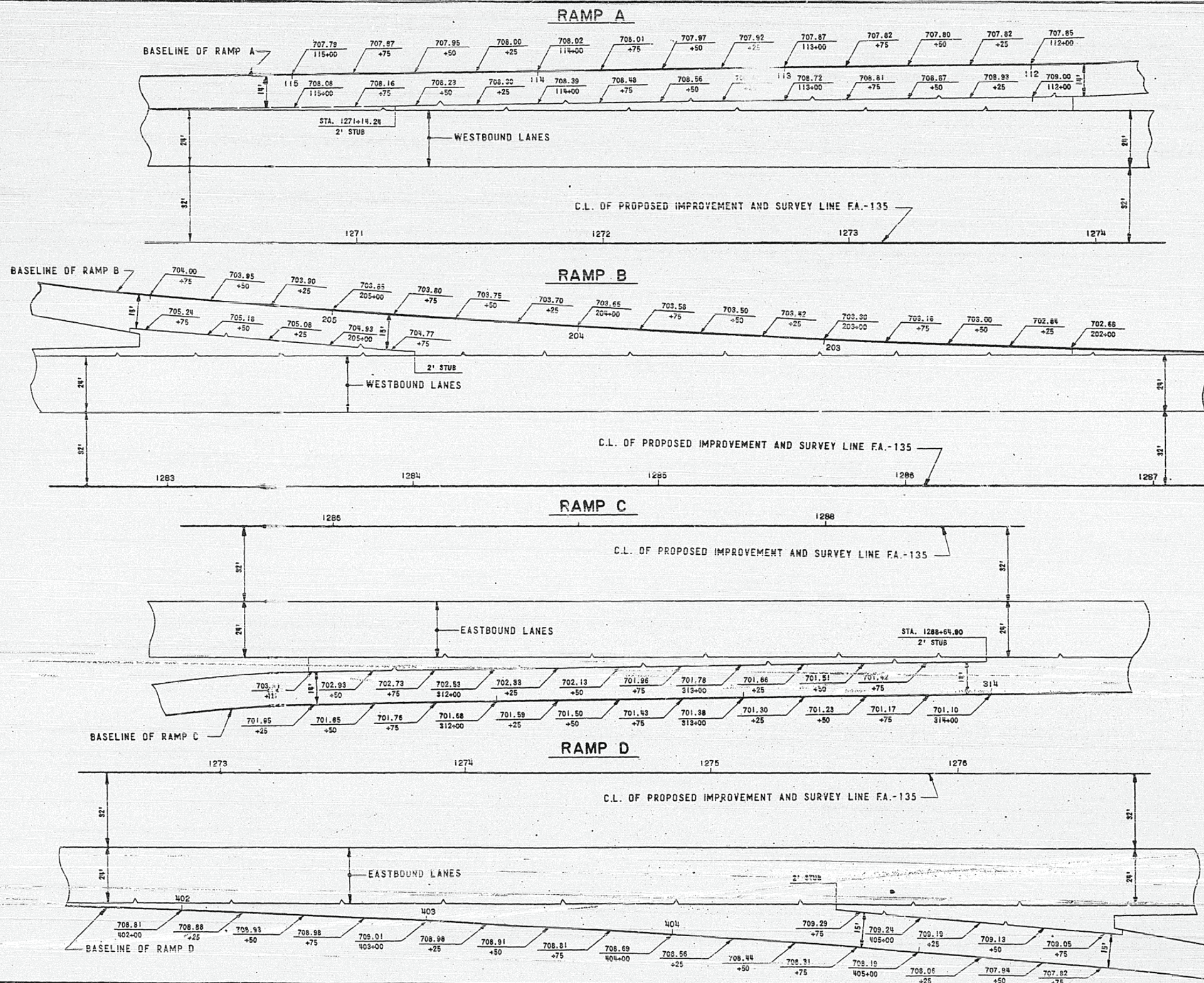
DATE	BY	CHECKED	REVISION
7-4-68	PIATT		
7-11-68	PIATT		
8-1-68	PIATT		
8-15-68	PIATT		
8-19-68	PIATT		



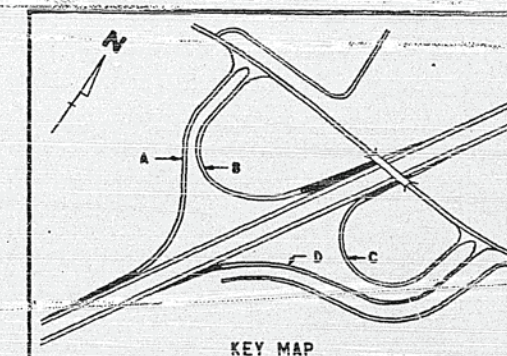
PLAN	SUBMITTED	BY	DATE
NOTE BOOK	PLOTTED	C.F.S.	4-2-60
NO. H-2-3	ALIGNMENT CHECKED	F.H.	5-13-60
	RT. OF WAY CHECKED	R.W.	8-16-60
		R.W.	9-16-60



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET
FA-135	74-69	PIATT	155	30
FED. ROAD DIST. NO. 7 ILLINOIS PROJECT				



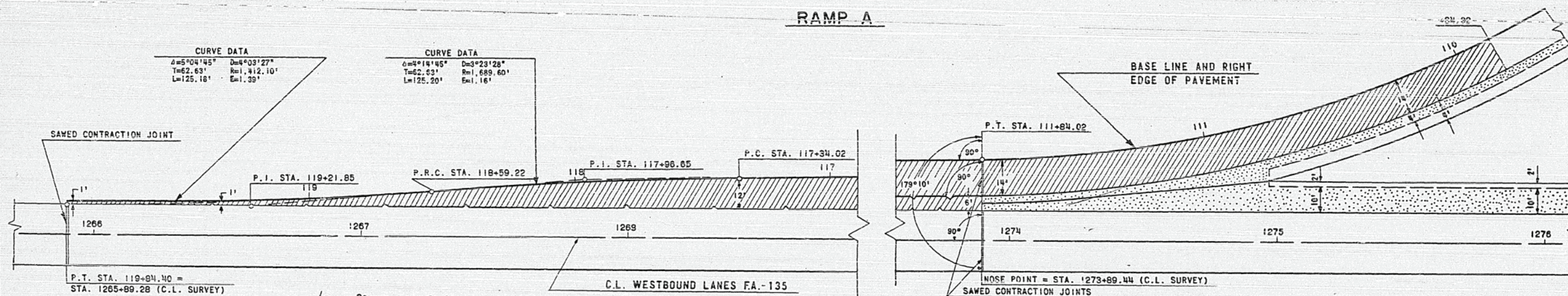
NOTE:
 SLOPE RAMP PAVEMENT DOWNWARD FROM EDGE OF THROUGH PAVEMENT
 0.02 FT. PER FT. AT FOLLOWING LOCATIONS:
 RAMP A STA. 115+00 TO STA. 119+84.40
 RAMP B STA. 200+00 TO STA. 202+00
 RAMP C STA. 314+00 TO STA. 319+23.55
 RAMP D STA. 400+00 TO STA. 402+00



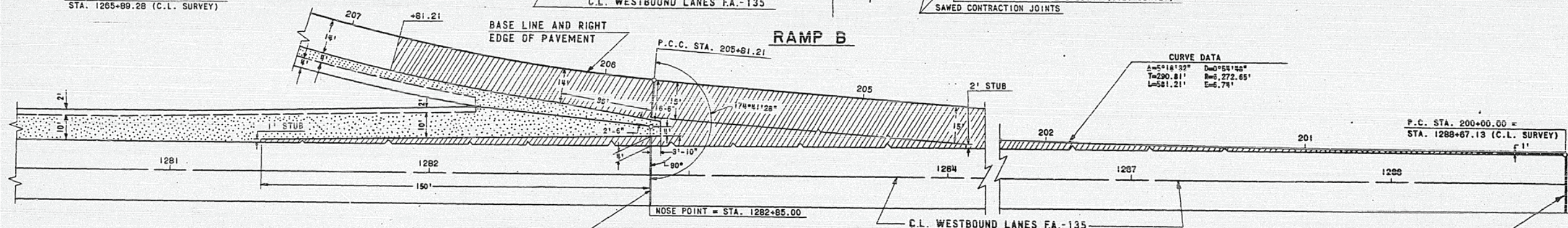
SCALE: 1"=20'		PAVEMENT ELEVATIONS-INTERSECTION RELOC. RT. 47 WITH FA.-135	
REVISIONS		STATE OF ILLINOIS	
1. DATE / INITIALS		DEPARTMENT OF PUBLIC WORKS AND BUILDINGS	
2.		DIVISION OF HIGHWAYS	
3.		F.A.-135 SECTION 74-69	
4.		PIATT COUNTY	
5.		HOMER L. CHASTAIN & ASSOCIATES	
6.		CONSULTING ENGINEERS	
7.		DECATUR, ILLINOIS	
8.		PROJECT	
9.		SHEET NO.	
10.			

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	31

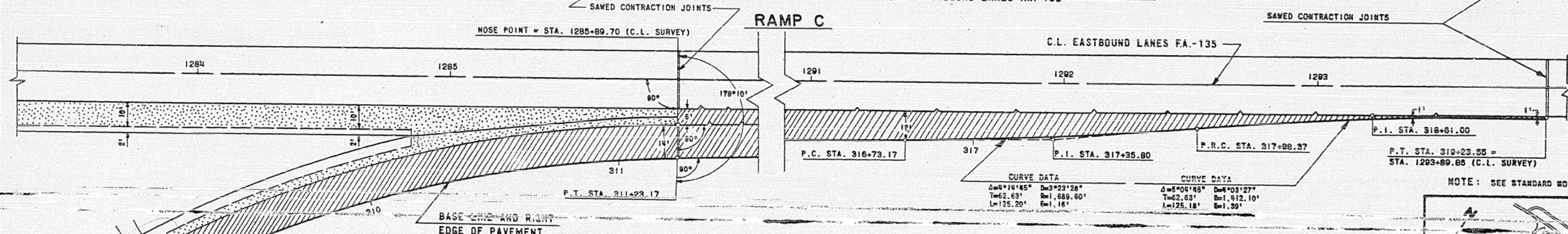
RAMP A



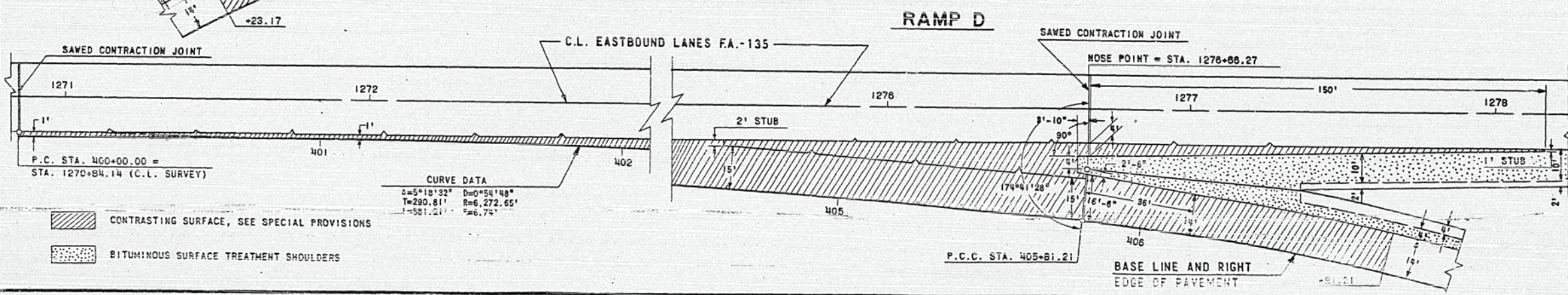
RAMP B



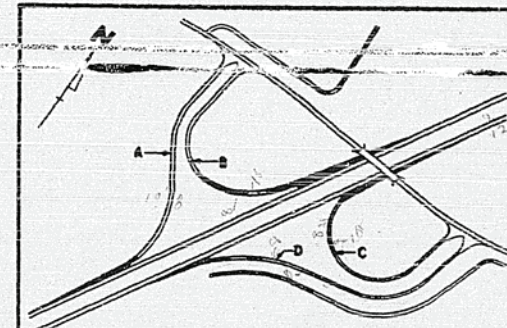
RAMP C



RAMP D



NOTE: SEE STANDARD NO. 2129-1 FOR JOINT DETAILS

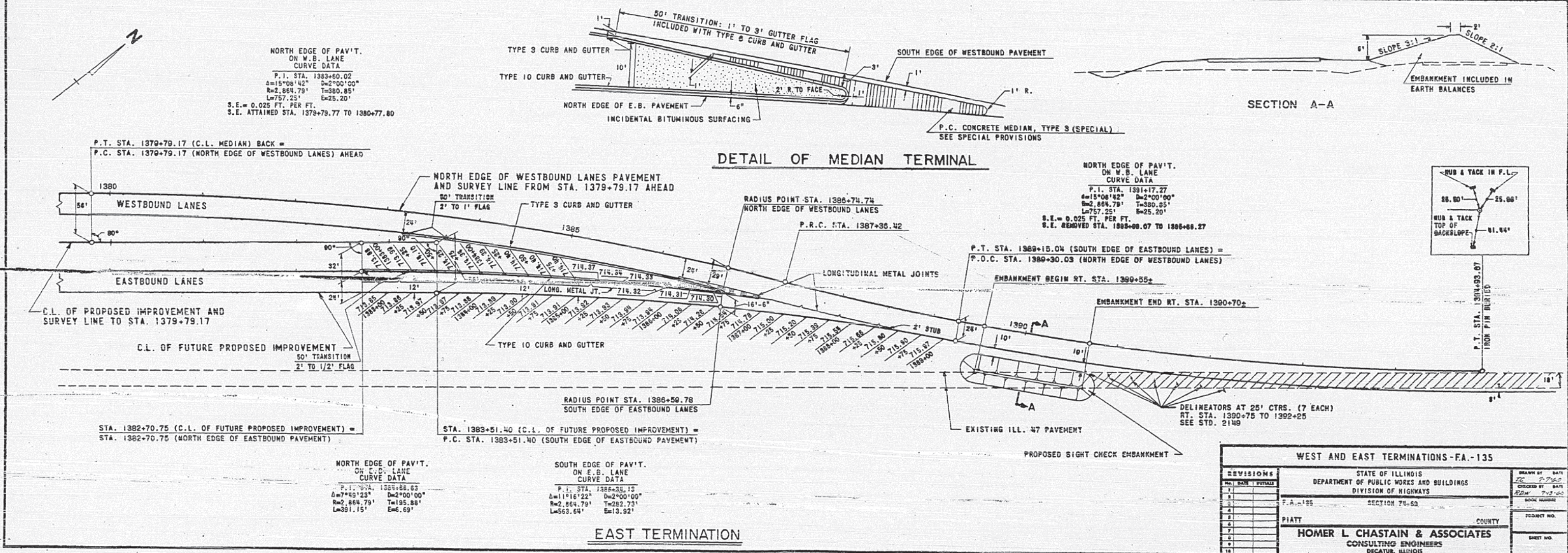
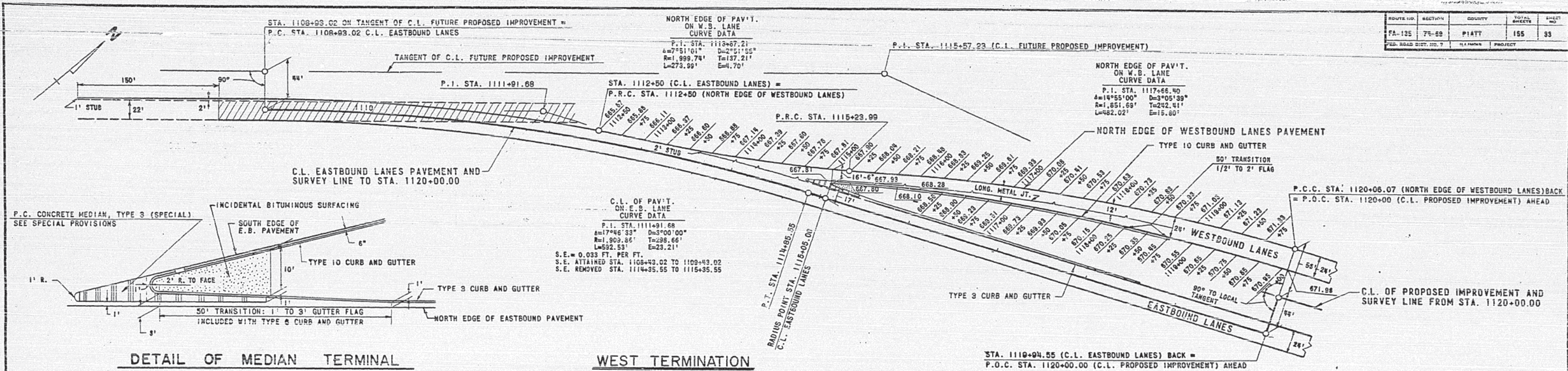


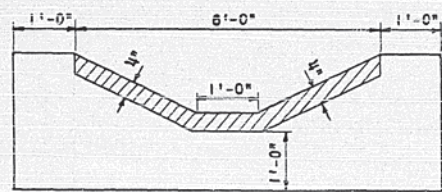
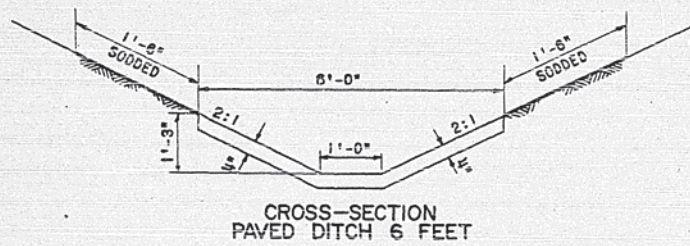
SCALE: 1"=20'

NOSE DETAILS-INTERSECTION RELOC. RT. 47 WITH FA-135

REVISIONS			STATE OF ILLINOIS		DRAWN BY: DATE	
No.	DATE	INITIALS	DEPARTMENT OF PUBLIC WORKS AND BUILDINGS		CHECKED BY: DATE	
			DIVISION OF HIGHWAYS			
			F.A.-135		PROJECT NO.	
			SECTION 74-69		SHEET NO.	
			PIATT		COUNTY	
			HOMER L. CHASTAIN & ASSOCIATES			
			CONSULTING ENGINEERS			
			DECATUR, ILLINOIS			

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	75-50	PIATT	155	33
FED. ROAD DIST. NO. 7				
PROJECT				



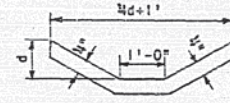
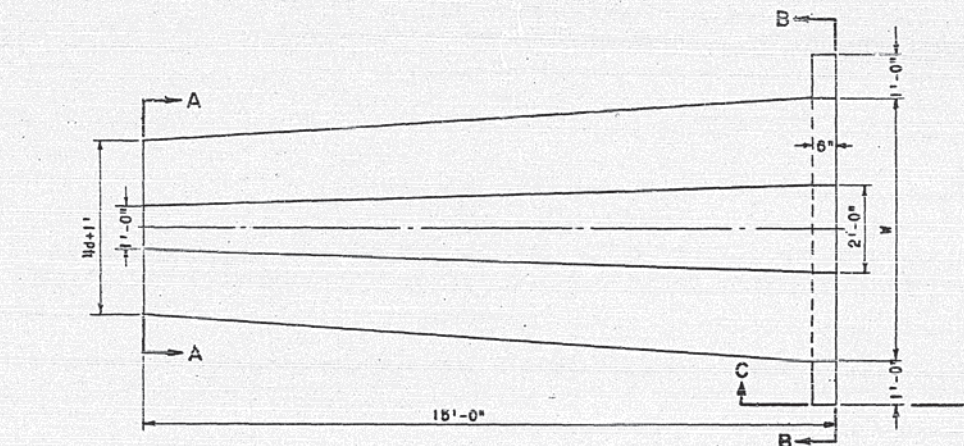


DETAIL OF ANCHOR WALL FOR PAVED DITCH

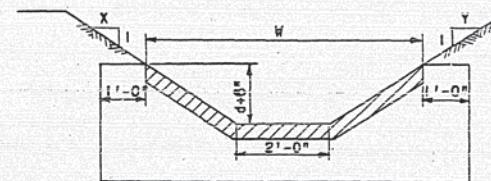
NOTE: PAVED DITCHES SHALL BE CONSTRUCTED OF CLASS "X" CONCRETE THROUGHOUT AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAL FOOT FOR PAVED DITCH OF THE WIDTH SPECIFIED, WHICH PRICE OR PRICES SHALL INCLUDE THE ANCHOR WALLS, ALL MATERIALS AND INSTALLATIONS.

ANCHOR WALLS SHALL BE PROVIDED AT A MAXIMUM OF 100 FOOT CENTERS AND AT CONSTRUCTION JOINTS. ANCHOR WALLS SHALL BE MONOLITHIC WITH THE PAVED DITCH. AT POINTS OF CHANGE IN GRADE THERE SHALL BE A CONSTRUCTION JOINT.

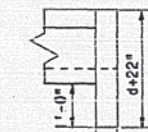
SPECIAL DESIGN FOR PAVED DITCH



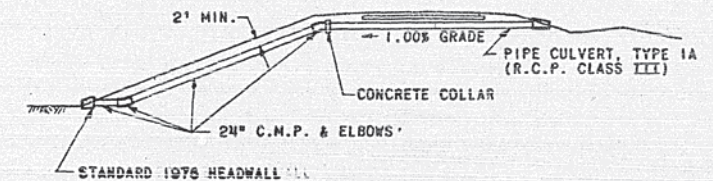
SECTION A-A



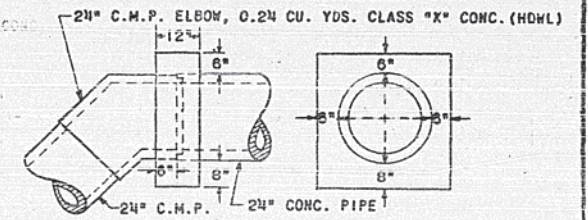
SECTION B-B



SECTION C-C



TYPICAL SECTION



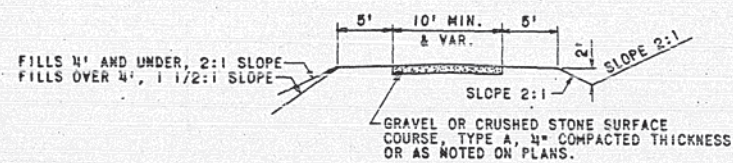
CONCRETE COLLAR

NOTE: THE CONCRETE COLLAR SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CLASS "X" CONCRETE. THE 24" PIPE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LINEAL FOOT FOR CORRUGATED METAL PIPE, WHICH PRICE SHALL INCLUDE ALL BENDS, ELBOWS AND FITTINGS WHICH ARE REQUIRED.

SLOPES		PAVED DITCH-6 FEET		
X:1	Y:1	d	W	CL. X CONC. CU. YDS.
4:1	4:1	15"	16'-0"	2.8
2:1	4:1	15"	12'-6"	2.3
2.5:1	3:1	15"	11'-7 1/2"	2.5
2:1	3:1	15"	10'-0"	2.1
3:1	3:1	15"	12'-6"	2.3
2:1	2:1	15"	9'-0"	1.9

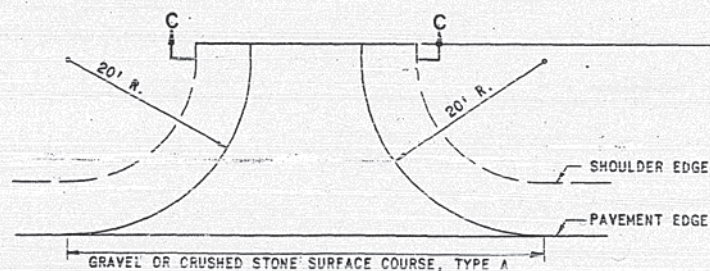
NOTE: PAVED DITCH INLETS SHALL BE CONSTRUCTED OF CLASS "X" CONCRETE THROUGHOUT AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CLASS "X" CONCRETE.

SPECIAL DESIGN FOR PAVED DITCH INLETS

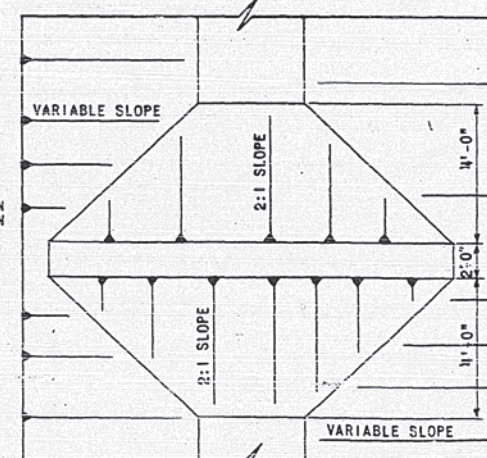


SECTION C-C

NOTE: DITCH PLUGS SHALL BE INCLUDED IN THE QUANTITY OF EARTH EXCAVATION AND PAVEMENT FOR EARTH EXCAVATION SHALL BE CONSIDERED AS PAYMENT IN FULL FOR DITCH PLUGS.

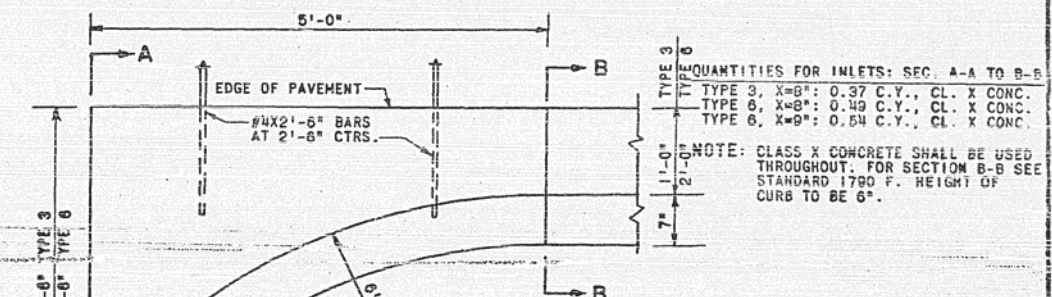


PLAN FOR PRIVATE ENTRANCES



PLAN VIEW OF DITCH PLUG

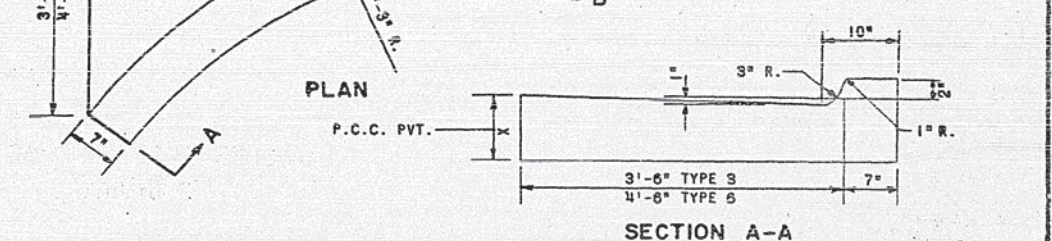
DETAIL OF MEDIAN DITCH FOR HIGH FILL



QUANTITIES FOR INLETS: SEC. A-A TO B-B
TYPE 3, X=8": 0.37 C.Y., CL. X CONC.
TYPE 6, X=8": 0.49 C.Y., CL. X CONC.
TYPE 6, X=9": 0.54 C.Y., CL. X CONC.

NOTE: CLASS X CONCRETE SHALL BE USED THROUGHOUT. FOR SECTION B-B SEE STANDARD 1790 F. HEIGHT OF CURB TO BE 6".

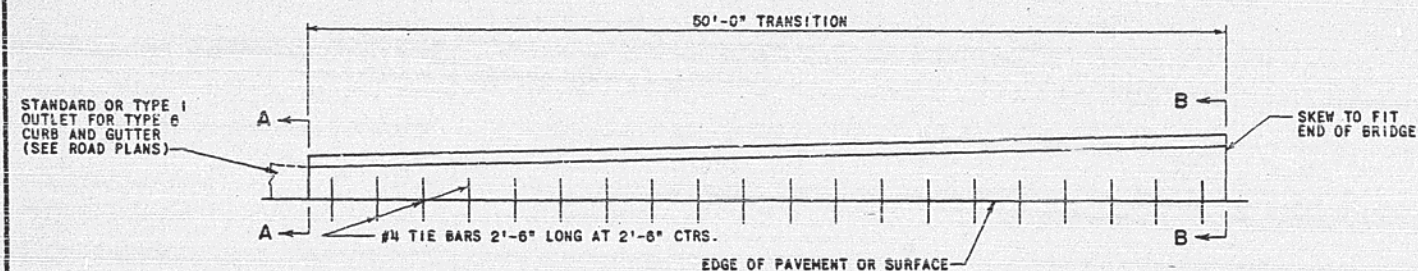
DETAIL OF SPECIAL INLET FOR COMBINATION CONCRETE CURB & GUTTER TYPE 3 & TYPE 6



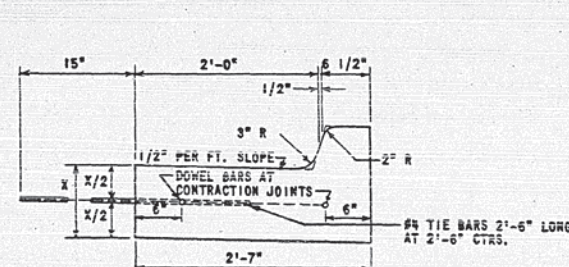
SECTION A-A

REVISIONS		MISCELLANEOUS DETAILS	
NO.	DATE	STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS	PROJECT NO.
1		F.A.-135 SECTION 74-69	
2		PIATT COUNTY	
3		HOMER L. CHASTAIN & ASSOCIATES CONSULTING ENGINEERS DECATUR, ILLINOIS	SHEET NO.

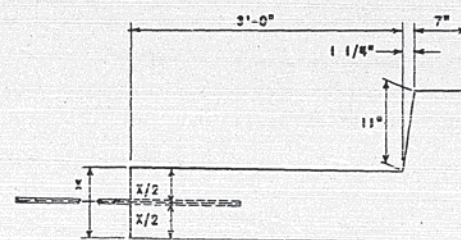
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-68	PIATT	155	85
DESIGN DIST. NO. 9	ILLINOIS	PROJECT		



PLAN



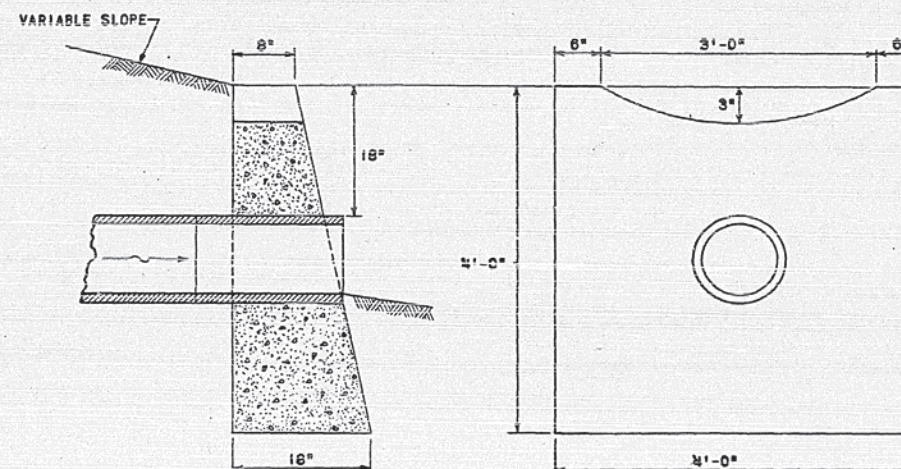
SECTION A-A



SECTION B-B

NOTE: TIE BARS REQUIRED ONLY ADJACENT TO P.C. CONCRETE PAVEMENT. CLASS "X" CONCRETE SHALL BE USED THROUGHOUT. THE CURB AND GUTTER TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR CLASS "X" CONCRETE WHICH PRICE SHALL INCLUDE TIE BARS.

QUANTITIES
5.2 CU. YDS. CLASS "X" CONCRETE FOR X = 9"
5.6 CU. YDS. CLASS "X" CONCRETE FOR X = 10"



SECTION

ELEVATION

NOTE: CLASS "X" CONCRETE SHALL BE USED THROUGHOUT

SIZE OF TILE	VOLUME OF ONE HEADWALL CU. YDS.
6"	0.62
8"	0.61
10"	0.60
12"	0.59
14"	0.58
15"	0.57

DETAILS OF CURB AND GUTTER TRANSITIONS AT BRIDGES

SPECIAL HEADWALL FOR TILE OUTLET

REVISIONS				DRAWN BY		DATE	
NO.	DATE	INITIALS	DESCRIPTION	BY	DATE	BY	DATE
1							
2							
3							
4							
5							
6							
7							
8							
9							
10							

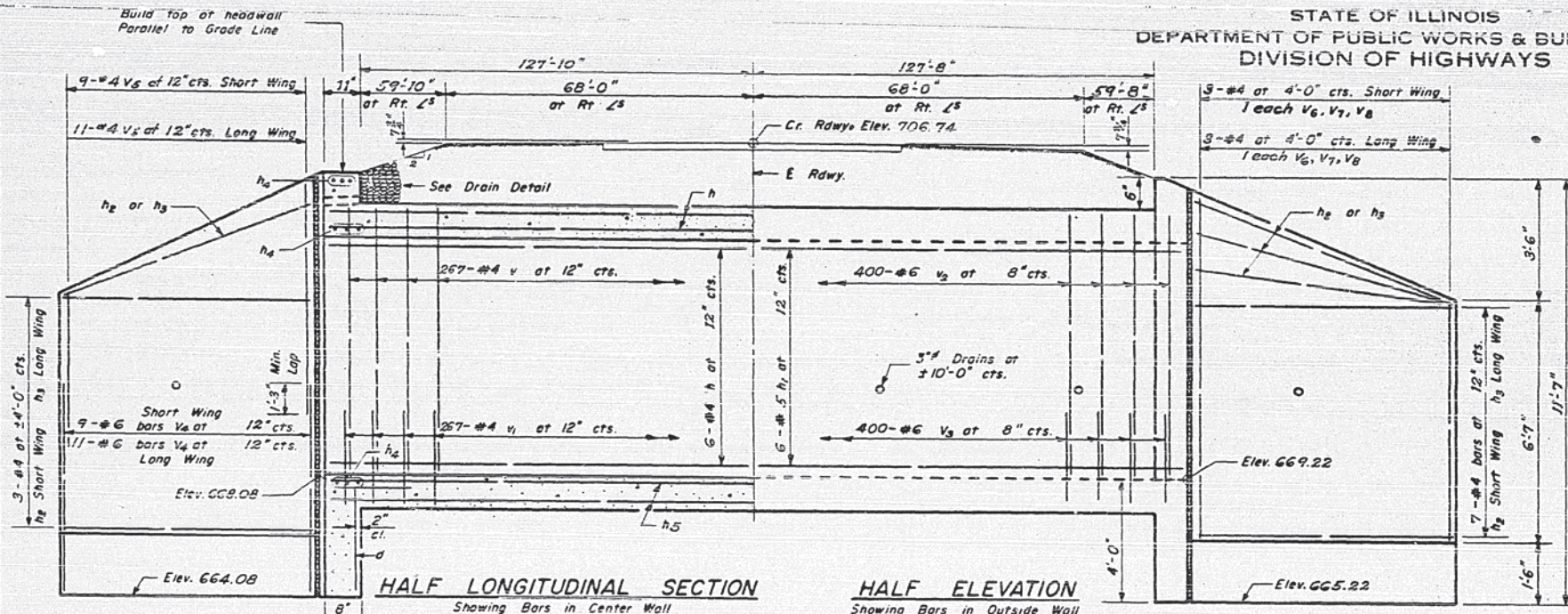
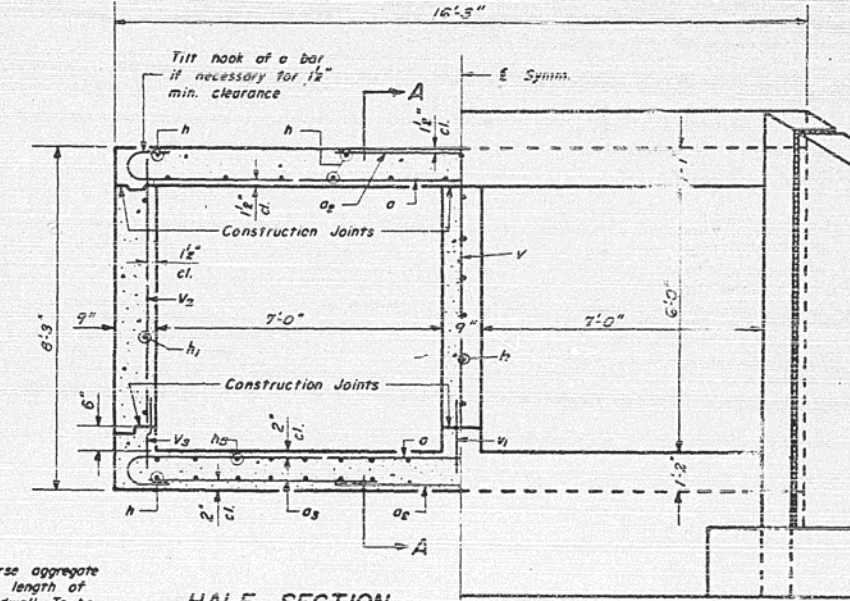
DRAINAGE DETAILS			
STATE OF ILLINOIS			
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS			
DIVISION OF HIGHWAYS			
F.A.-135	SECTION 74-68	COUNTY	PROJECT NO.
PIATT			
HOMER L. CHASTAIN & ASSOCIATES			PICT NO.
CONSULTING ENGINEERS			
DECATUR, ILLINOIS			

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

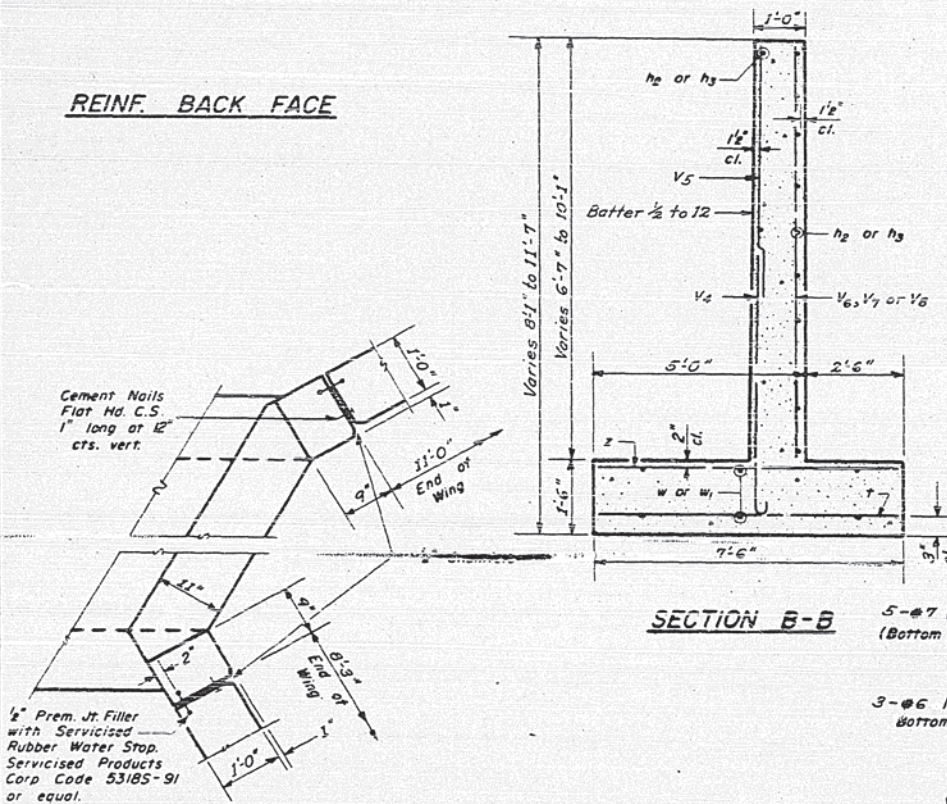
ROUTE NO.	SECTION	LENGTH	TOTAL SHEETS	SHEET NO.	SHEET NO.
FA135	74-69	PIATT	155	36	1 SHEET

GENERAL NOTES

Class X Concrete shall be used thru-out.
Exposed edges shall be beveled $\frac{3}{8}$ " except shown.
For backfilling and embankment see Standard Specifications.
All bars shall be lapped 20 diameters unless otherwise shown.
The top of the culvert, the backs of the exterior walls above the lower construction joint and backs of the wings above the tops of the footings shall be waterproofed in accordance with the Standard Specifications Article 51.21.

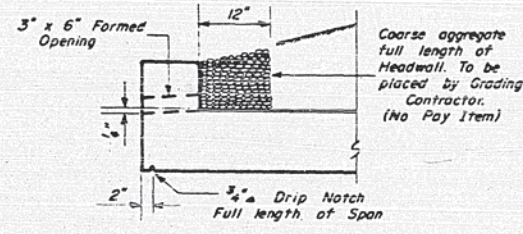
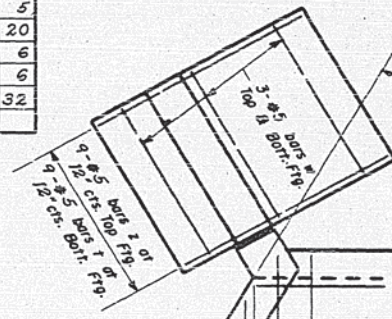


REIN. BACK FACE



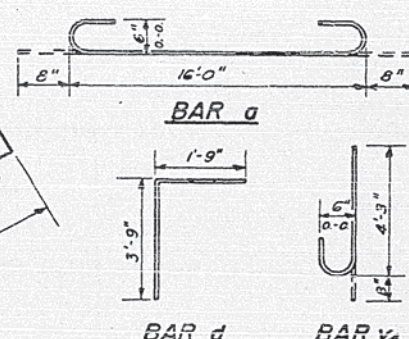
Location of h bars	No.
Top of Top Slab	5
Bottom of Top Slab	20
Each Outside Wall	6
Center Wall	6
Bottom Slab	32

REIN. FRONT FACE



HALF SECTION THRU BARREL

HALF END ELEVATION



BILL OF MATERIAL

Bar	No.	Size	Length	Bar	No.	Size	Length
a	916	#6	17'-4"	v	267	#4	6'-3"
a ₂	1080	#7	7'-0"	v ₂	800	#6	6'-3"
a ₃	136	#4	5'-9"	v ₃	800	#6	3'-0"
d	30	#1	1'-6"	v ₄	40	#6	4'-11"
h	310	#4	27'-	v ₅	4	#4	8'-0"
h ₁	120	#5	28'-6"	v ₆	4	#4	9'-9"
h ₂	28	#4	8'-0"				
h ₃	28	#4	10'-9"				
h ₄	18	#6	16'-6"				
h ₅	320	#6	25'-5"				
t	40	#5	7'-3"				
Class X Concrete				Cu. Yd.			527.9
Reinforcement Bars				Lb.			77,150

DESIGN LIMITS

Vert. Opening \pm 8'-0"
or Crown of Roadway to
Flow Line over 16'-0"

DESIGN STRESSES

f_s = 20,000 psi Reinforcement
f_c = 1,000 psi for Wings and Footings
f_c = 1,400 psi for Slabs and Walls
n = 10

SPECIAL BOX CULVERT.

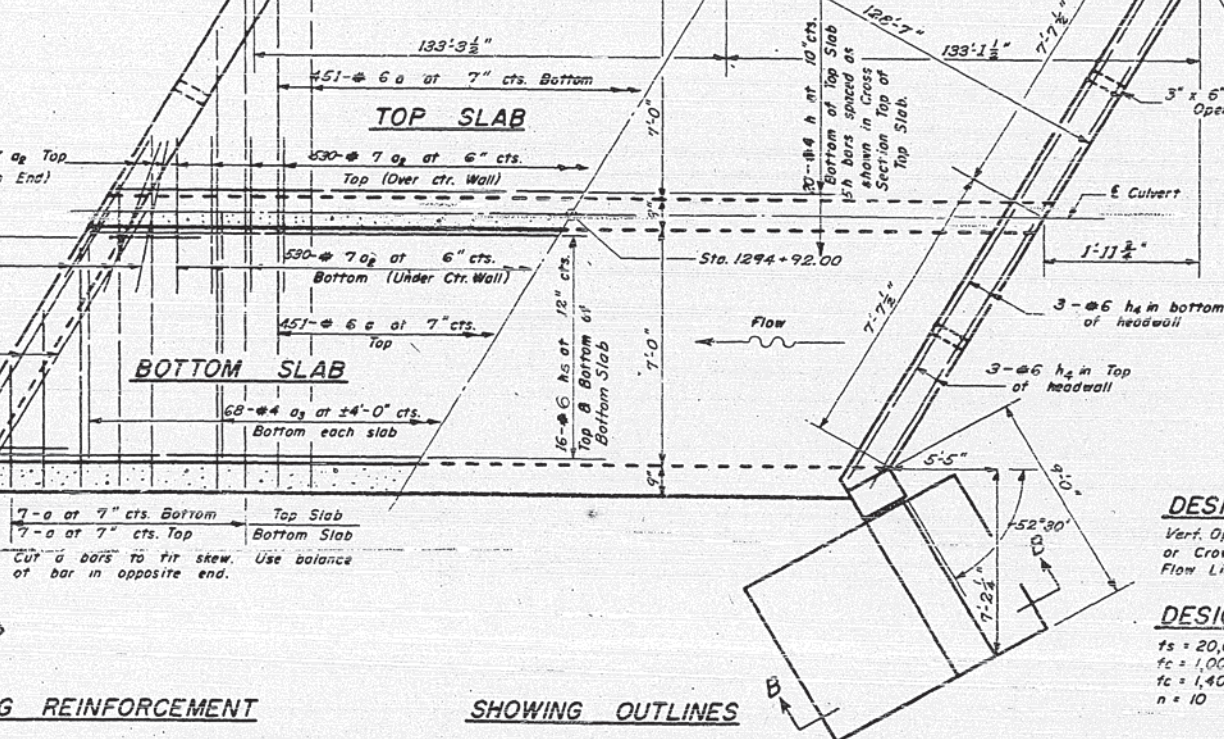
F.A. ROUTE 135, SEC. 74-69
PIATT COUNTY
A.R. STA. 1294 + 92

DESIGNED	M.L. Jackson
CHECKED	W.L. Cottle
DRAWN	S. Corson W.A. Sausaman
CHECKED	M.L. Jackson

EXAMINED	W.L. Cottle
PASSED	S. Corson
APPROVED	W.A. Sausaman

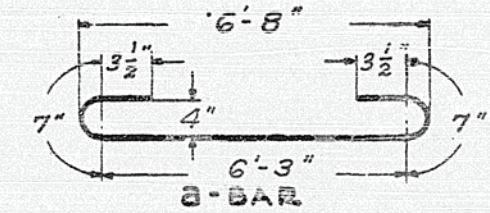
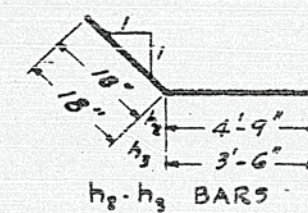
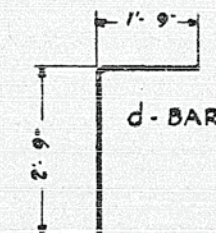
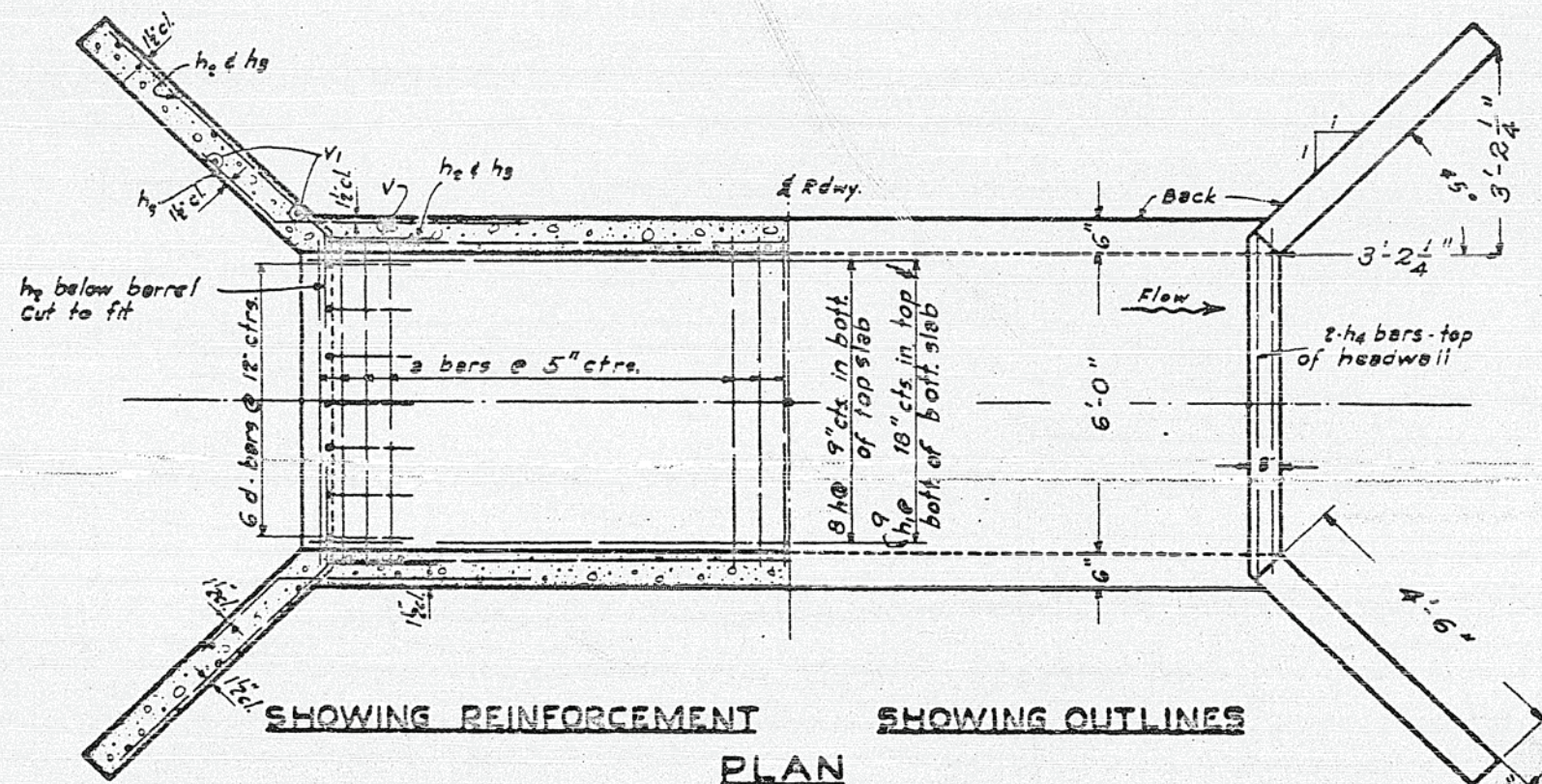
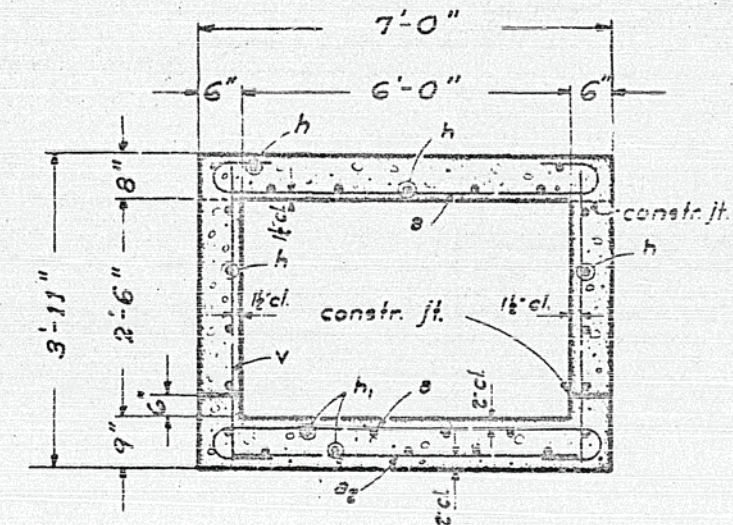
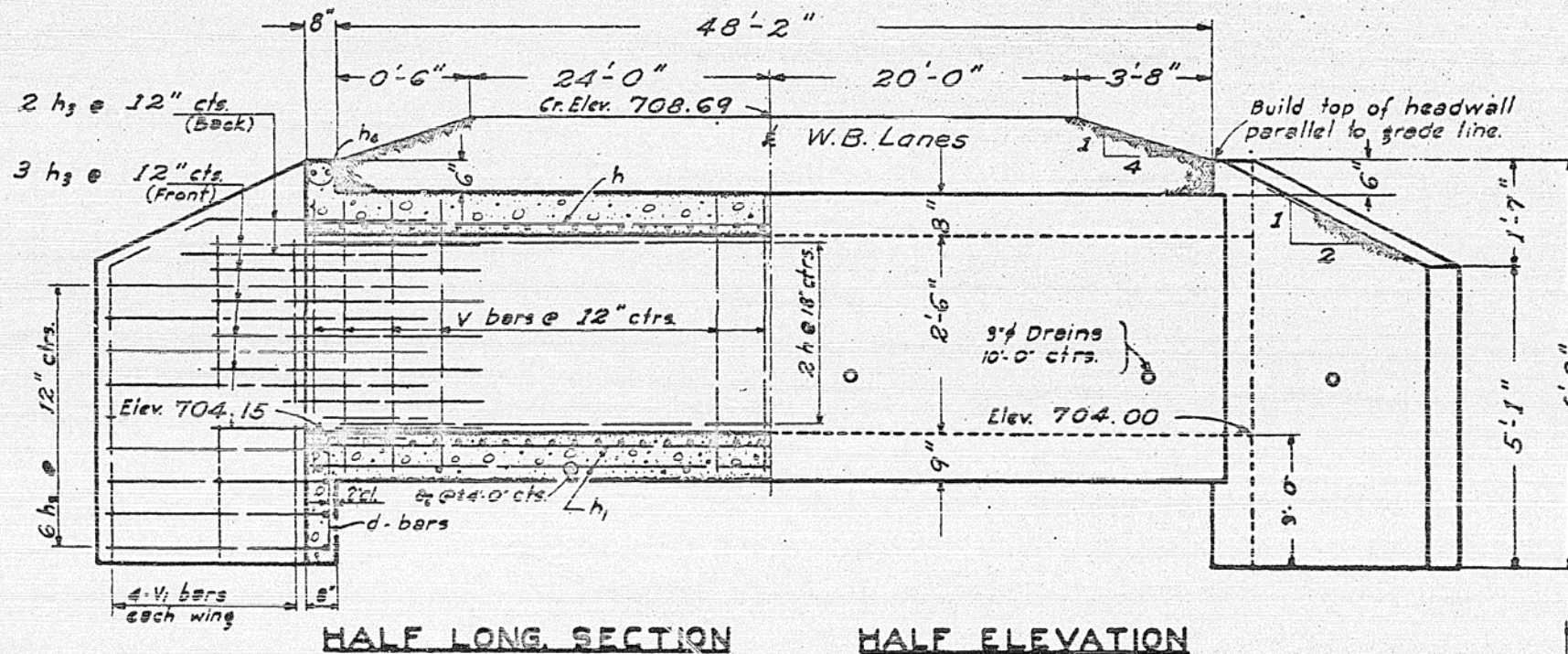
SHOWING REINFORCEMENT

SHOWING OUTLINES



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

WORK SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET
FA 135 74-69	PIATT	155	37		1
FOR ROAD DIST. NO. 1	HAZARD	FOR AID PROJECT			



BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH
a	238	#5	8'-0"
a ₂	13	#4	6'-3"
h	23	#2	25'-2"
h ₁	18	#4	25'-2"
h ₂	24	#4	6'-3"
h ₃	20	#4	5'-0"
h ₄	4	#6	6'-6"
v	100	#4	3'-6"
v ₁	16	#6	5'-3"
d	12	#4	6'-0"

Class X concrete curbs 26.4
Reinforcement bars Lbs. 3346
Note: All bars shall be round ASTM A305-49.
The size number is the number of
1/8 inches in the nominal diameter.

SPECIAL BOX CULVERT

F.A. ROUTE 135 SEC. 74-69
PIATT COUNTY

A.R. STA. 1367+00 W.B. LANES

STANDARD 2020

COMPUTED	MLT 8-15-60
CHECKED	RDW 8-16-60
DRAWN	-C.L. HAZLETT-
CHECKED	
ASSEMBLED	
CHECKED	

EXAMINED *Sgt 16-60*
DESIGNED *[Signature]*
APPROVED *[Signature]*

GENERAL NOTES

Class X concrete shall be used throughout.
At least six feet of Barrel shall be poured
monolithically with wing walls.
Exposed edges shall be beveled 3".
For backfilling & embankments see st'd. Spec's.
Tilt hook of "d" bars, if necessary, to obtain 1/8"
minimum clearance at top of hook.

f_s = 20,000 #/sq.
f_c = 1200 #/sq.
n = 10

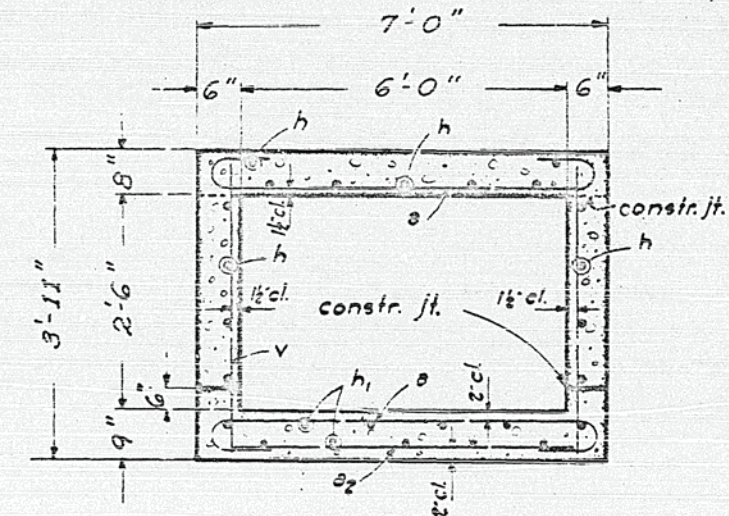
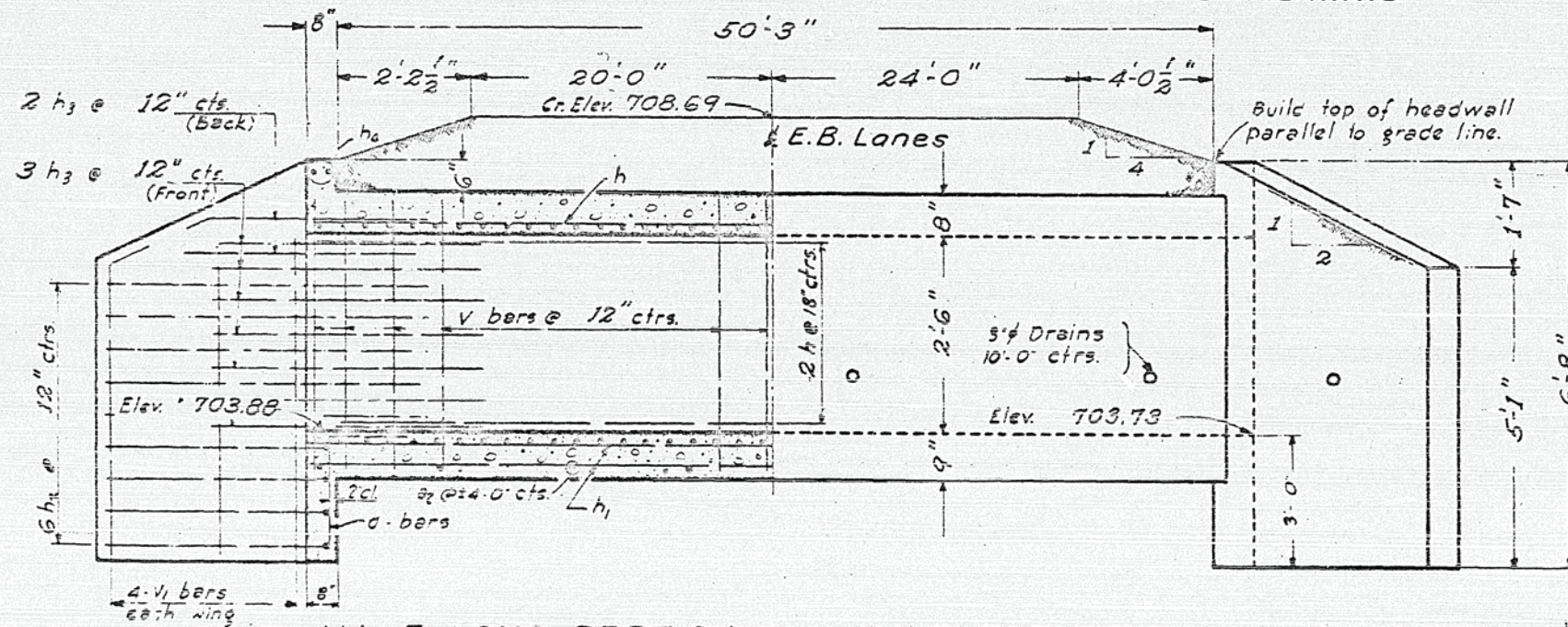
H-20 LOADING

Reinf. Revised - 11-50 - J.S.M.

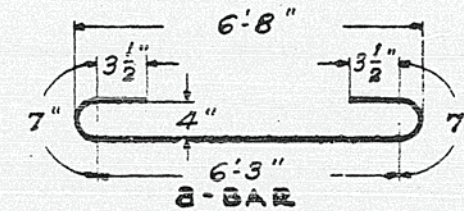
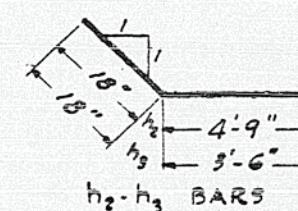
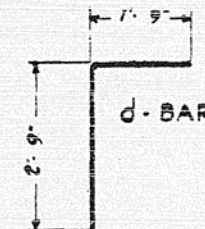
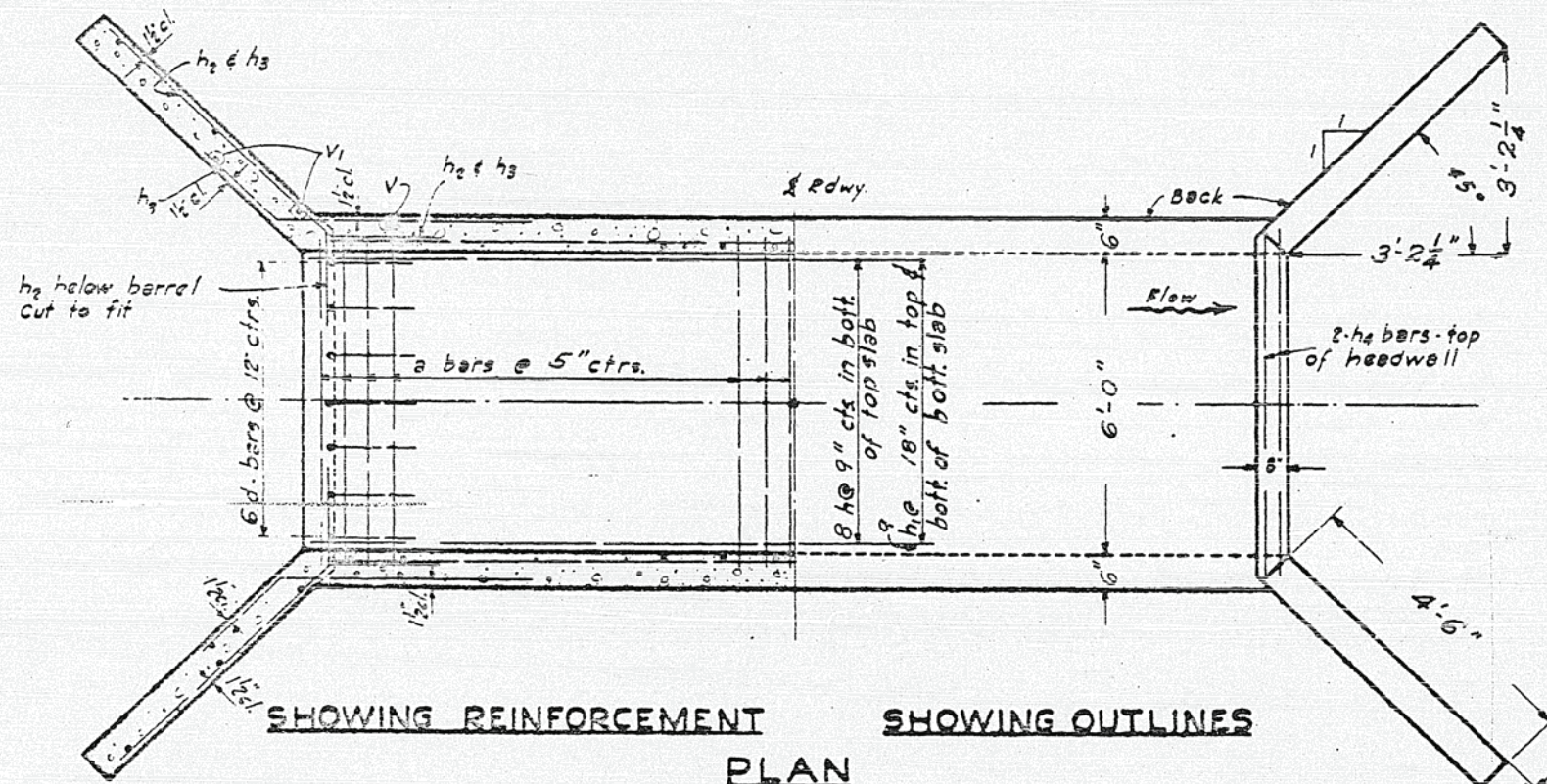
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	PROJECT	TOTAL	SHEET
FA 135	74-69	PIATT	155	38
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

SHEET NO. 1
1 SHEET



SECTION THRU BARREL



BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH
a	248	#5	8'-0"
a2	14	#4	6'-3"
h	28	#4	26'-3"
h1	18	#4	26'-3"
h2	24	#4	6'-3"
h3	20	#4	5'-0"
h4	4	#6	6'-6"
v	104	#4	3'-6"
v1	16	#4	5'-3"
d	12	#4	4'-6"

Class 'X' Concrete cu. yds. 27.3
Reinforcement bars Lbs. 3476

NOTE: All bars shall be round ASTM A305-40.
The size number is the number of
1/8 inches in the nominal diameter.

SPECIAL BOX CULVERT

F.A. ROUTE 135 SEC. 74-69

PIATT COUNTY

A.R. STA. 1367+00 E.B. LANES

STANDARD 2020

COMPUTED	MLJ 8-15-60
CHECKED	RDW 8-16-60
DRAWN	-C.L. HAZLETT-
CHECKED	
SPECIAL	ASSEMBLED
	CHECKED

EXAMINED 8-16-60

PASSED

APPROVED

APPROVED

APPROVED

GENERAL NOTES

Class 'X' concrete shall be used throughout.
At least six feet of Barrel shall be poured
monolithically with wing walls.
Exposed edges shall be beveled 3/4".
For backfilling & embankments see St'd. Spec's.
Tilt hook of "a" bars, if necessary, to obtain 1/4"
minimum clearance at top of hook

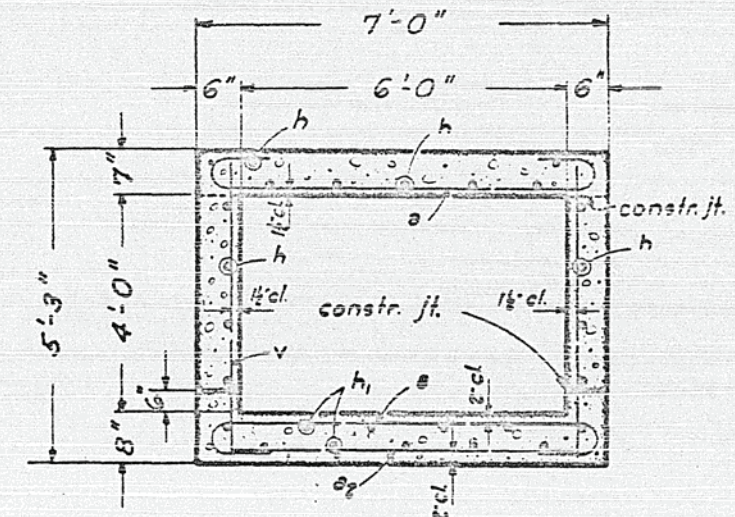
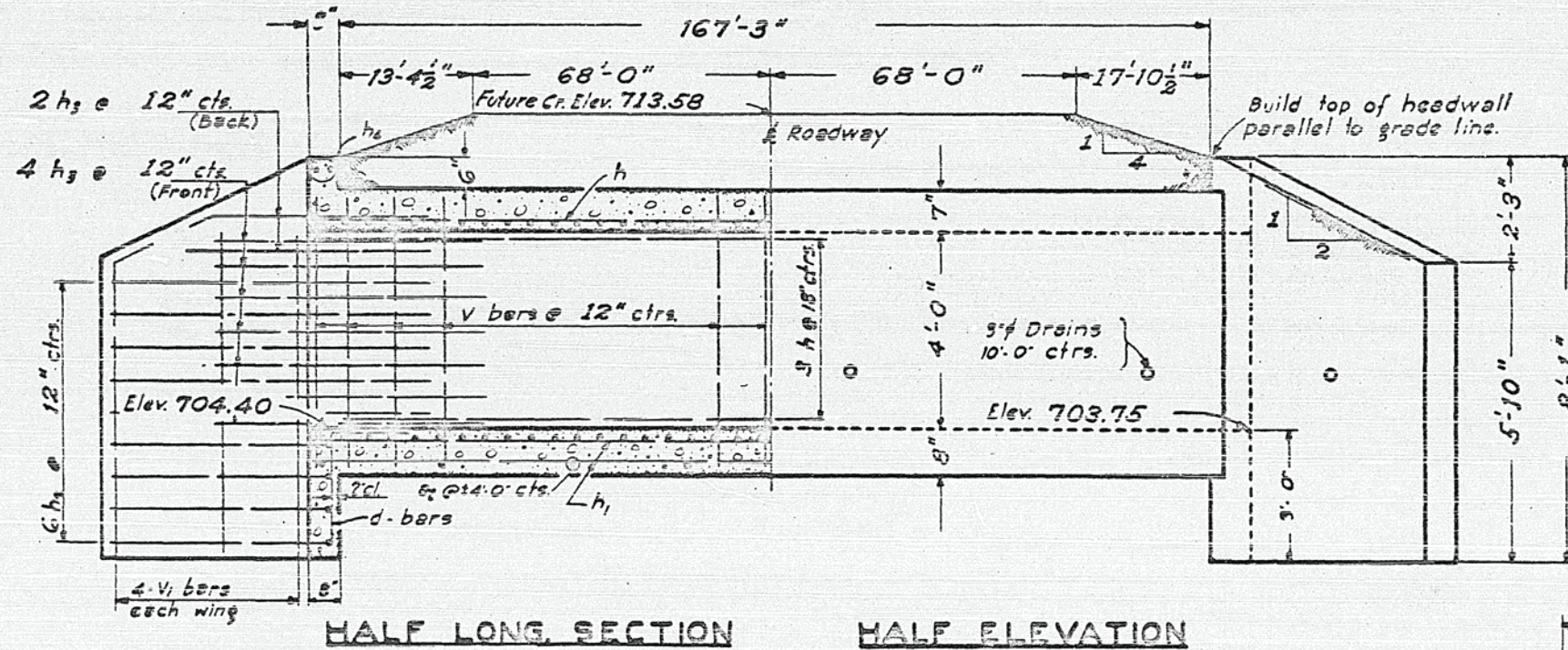
f_s = 20,000 #/sq.
f_c = 1200 #/sq.
n = 10

H-20 LOADING

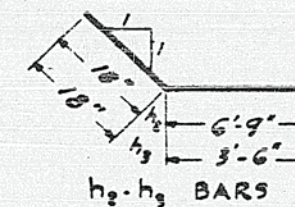
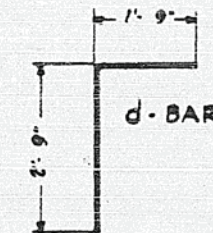
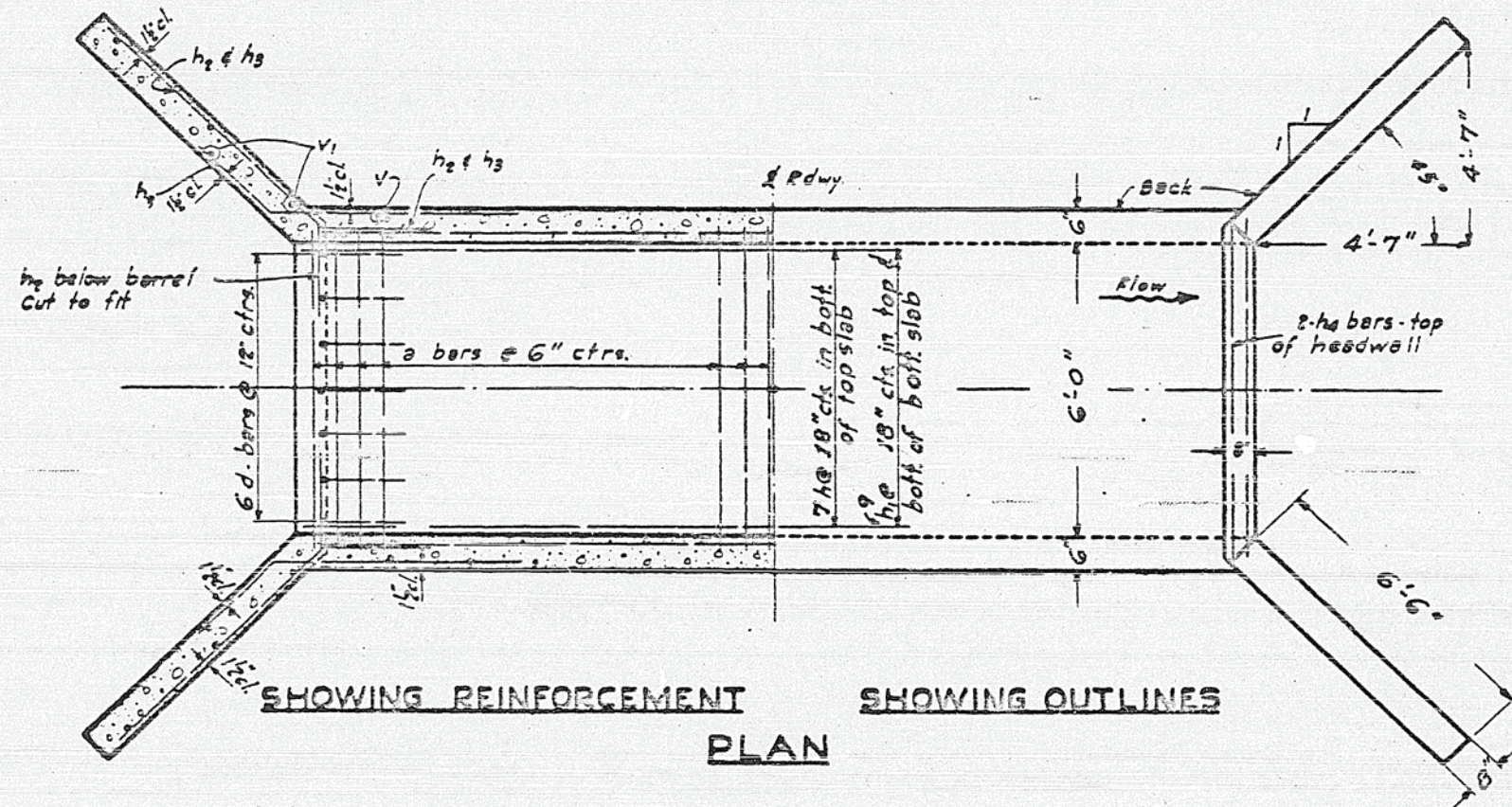
Reinf. Revised - 11-50-J.S.M

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	PIATT	155	39	SHEET NO. 1
FA 135	74-69	PIATT	155	39		1
FOR ROAD DIST. NO. 1						
FOR A.R. PROJECT						



SECTION THRU BARREL



BILL OF MATERIAL

BAR	NO.	SIZE	LENGTH
a	674	#5	8'-0"
a2	43	#4	6'-3"
h	76	#4	29'-1"
h1	54	#4	29'-1"
h2	24	#4	8'-3"
h3	24	#4	5'-0"
h4	4	#4	6'-6"
v	338	#4	4'-9"
v1	16	#4	6'-9"
d	12	#4	4'-6"

Class 'X' Concrete cu. yds. 85.1
Reinforcement bars Lbs. 9800

NOTE: All bars shall be round ASTM A305-69.
The size number is the number of
1/8 inches in the nominal diameter.

SPECIAL BOX CULVERT
F.A. ROUTE 135 SEC. 74-69
PIATT COUNTY
A.R. STA. 1381+08

$f_s = 20,000 \text{ psi}$
 $f_c = 1200 \text{ psi}$
 $n = 10$

H-20 LOADING

COMPUTED	MLJ 8-15-60
CHECKED	RDW 8-16-60
DRAWN	-G.L. HAZLETT-
CHECKED	
ASSEMBLED	MLJ
CHECKED	

EXAMINED	Sept 16 1960
PASSED	
APPROVED	

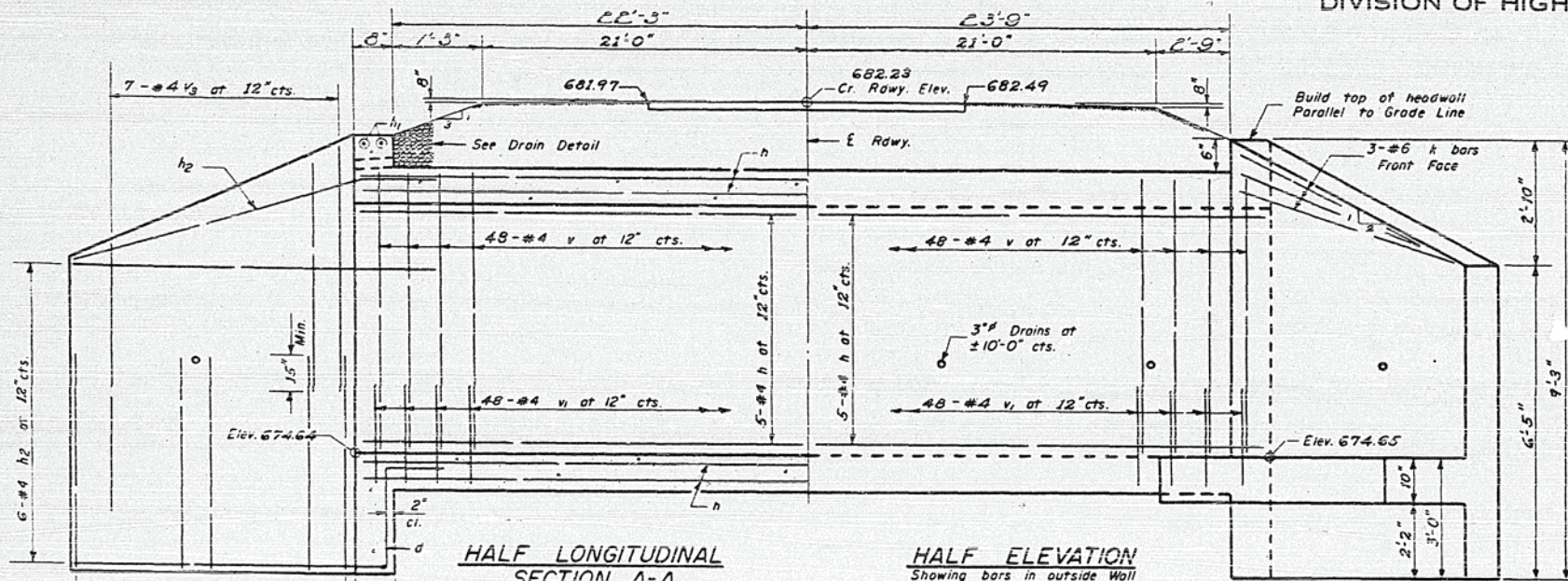
GENERAL NOTES
Class X concrete shall be used throughout.
At least six feet of Barrel shall be poured
monolithically with wing walls.
Exposed edges shall be beveled 3".
For backfilling & embankments see std. Spec's.
Tilt hook of "a" bars, if necessary, to obtain 16"
minimum clearance at top of hook.

Reinf. Revised - 11-50 - J.S.M.

STANDARD 2020

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

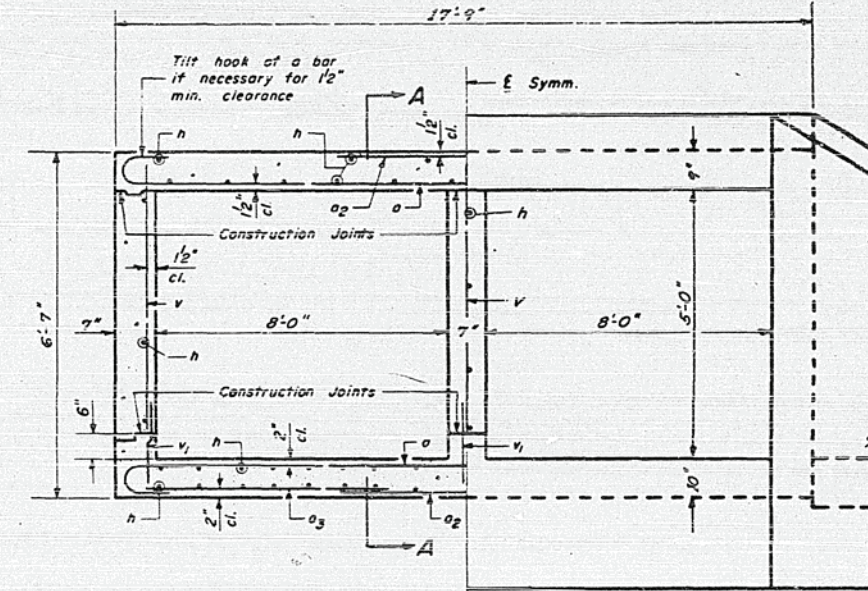
ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
F.A. 135	74-69	PIATT	155	40
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



HALF LONGITUDINAL
SECTION A-A
Showing Bars in Center Wall

HALF ELEVATION
Showing bars in outside Wall

GENERAL NOTES
Class X Concrete shall be used thru-out.
Exposed edges shall be beveled $\frac{1}{4}$ except as shown.
For backfilling and embankment see Standard Specifications.
All bars shall be lapped 20 diameters unless otherwise shown.
The top of the culvert, the backs of the exterior walls above the lower construction joint and backs of the wings above the tops of the footings shall be waterproofed in accordance with the Standard Specifications, Article 51.21.

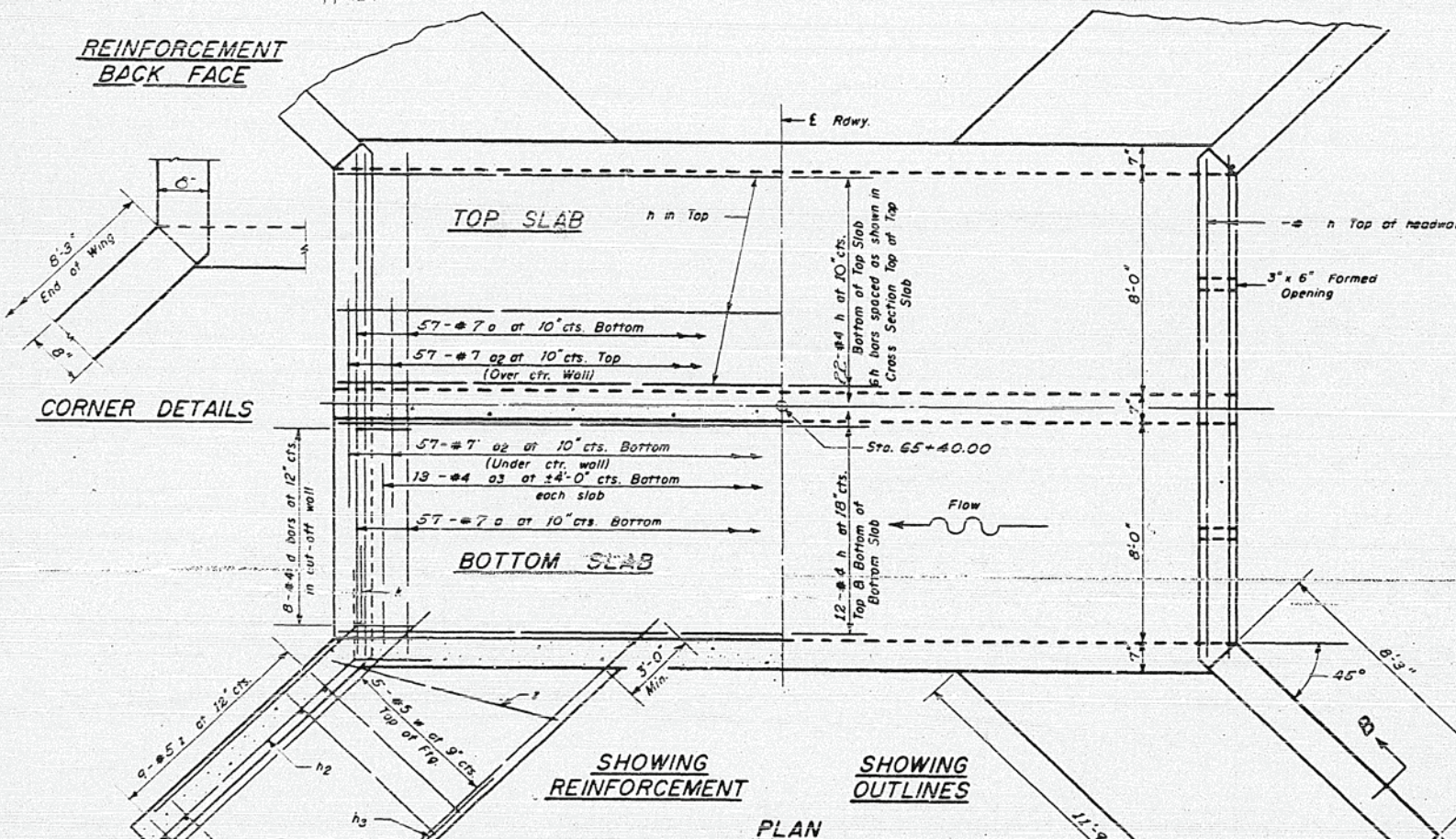


HALF SECTION
THRU BARREL

HALF END ELEVATION

Location of h bars	No.
Top of Top Slab	6
Bottom of Top Slab	22
Each Outside Wall	5
Center Wall	5
Top of Bottom Slab	12
Bottom of Bottom Slab	12

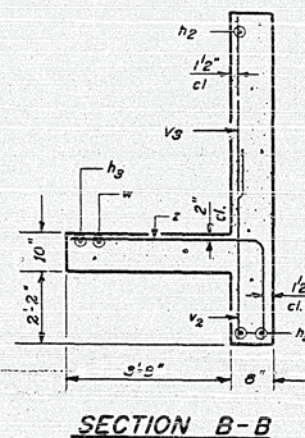
REINFORCEMENT
BACK FACE



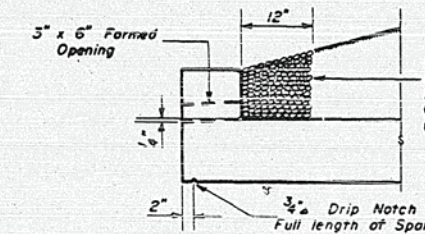
CORNER DETAILS

SHOWING
REINFORCEMENT
PLAN

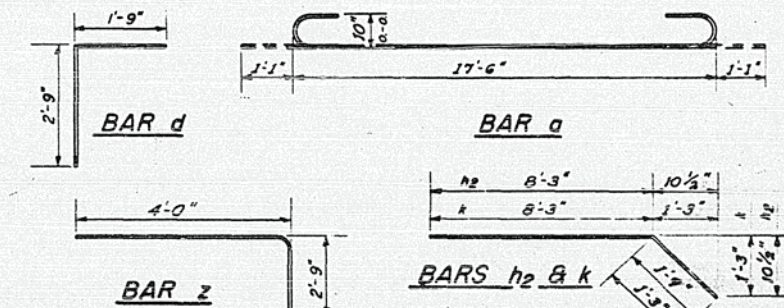
SHOWING
OUTLINES
PLAN



SECTION B-B



DRAIN DETAIL



BILL OF MATERIAL

Bar	No.	Size	Length	Bar	No.	Size	Length
a	114	#7	19'-8"	v	144	#4	5'-0"
o2	114	#7	11'-6"	v2	32	#4	2'-4"
o3	26	#4	4'-0"	v3	28	#4	5'-6"
z	32	#4	4'-6"				
h	134	#4	24'-2"				
h1	4	#6	17'-3"				
h2	32	#4	9'-6"				
h3	4	#4	8'-6"				
				w	20	#5	14'-8"
k	12	#6	10'-0"	z	40	#5	6'-9"
Class X Concrete			77.5	Cu Yds.			
Reinforcement Bars			11600	Lbs.			

DESIGN LIMITS

Opening up to 8'-0" and
Crown of Roadway to
Flow Line up to 16'-0".

DESIGN STRESSES

$f_s = 20,000$ psi Reinforcement
 $f_c = 1,000$ psi for Wings & Footings
 $f_c = 1,400$ psi for Slabs & Walls
 $n = 10$

SPECIAL BOX CULVERT

F.A. ROUTE 135 SEC. 74-69
PIATT COUNTY

RELOCATED ROUTE 47

A.R. STA. 65+40

LOADING H20-S16-44

DESIGNED M.L. Jackson	EXAMINED
CHECKED W.L. Cottle	PASSED
DRAWN W.A. Sausman	APPROVED
CHECKED	

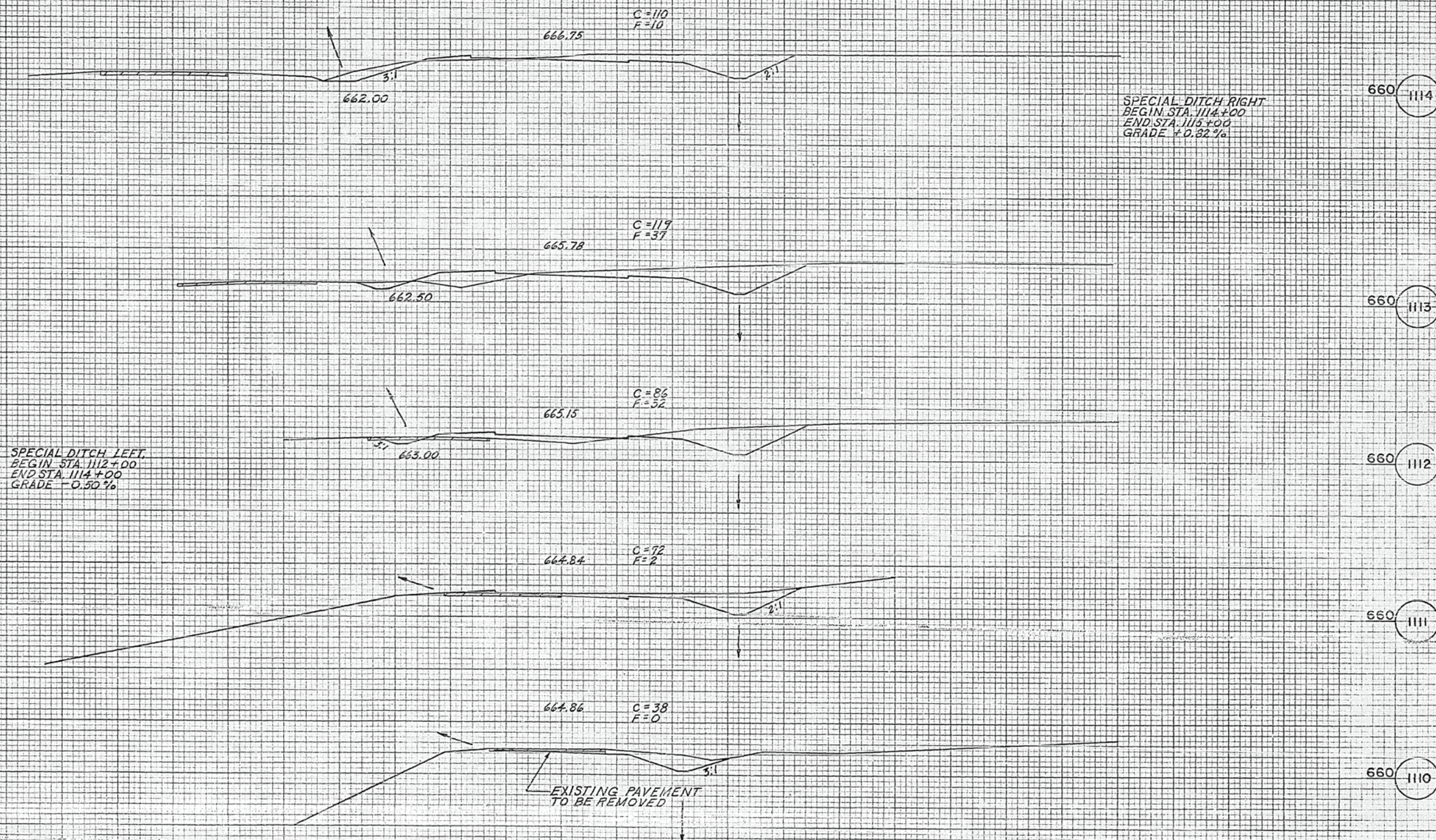
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ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	41
STA. 1108+93.02		TO STA. 1109+00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		

FINAL SURVEY	DATE
NO. 1	
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NO.	FINAL SURVEY NOTE BOOK	DATE
	UNWEYED	
	FLOTTED	
	TEMPERATE AREAS	
	AREAS CHECKED	

ORIGINAL SURVEY	SURVIVED	IV	DATE
NOTE BOOK	PLANTED	C. F. S.	6-24-64
	TEMPERATURE	P. F. W. (C)	6-27-64
	AREA 1	T. I. C.	7-7-64
	AREAS CHECKED	A. R. M.	7-9-60
			7-11-60

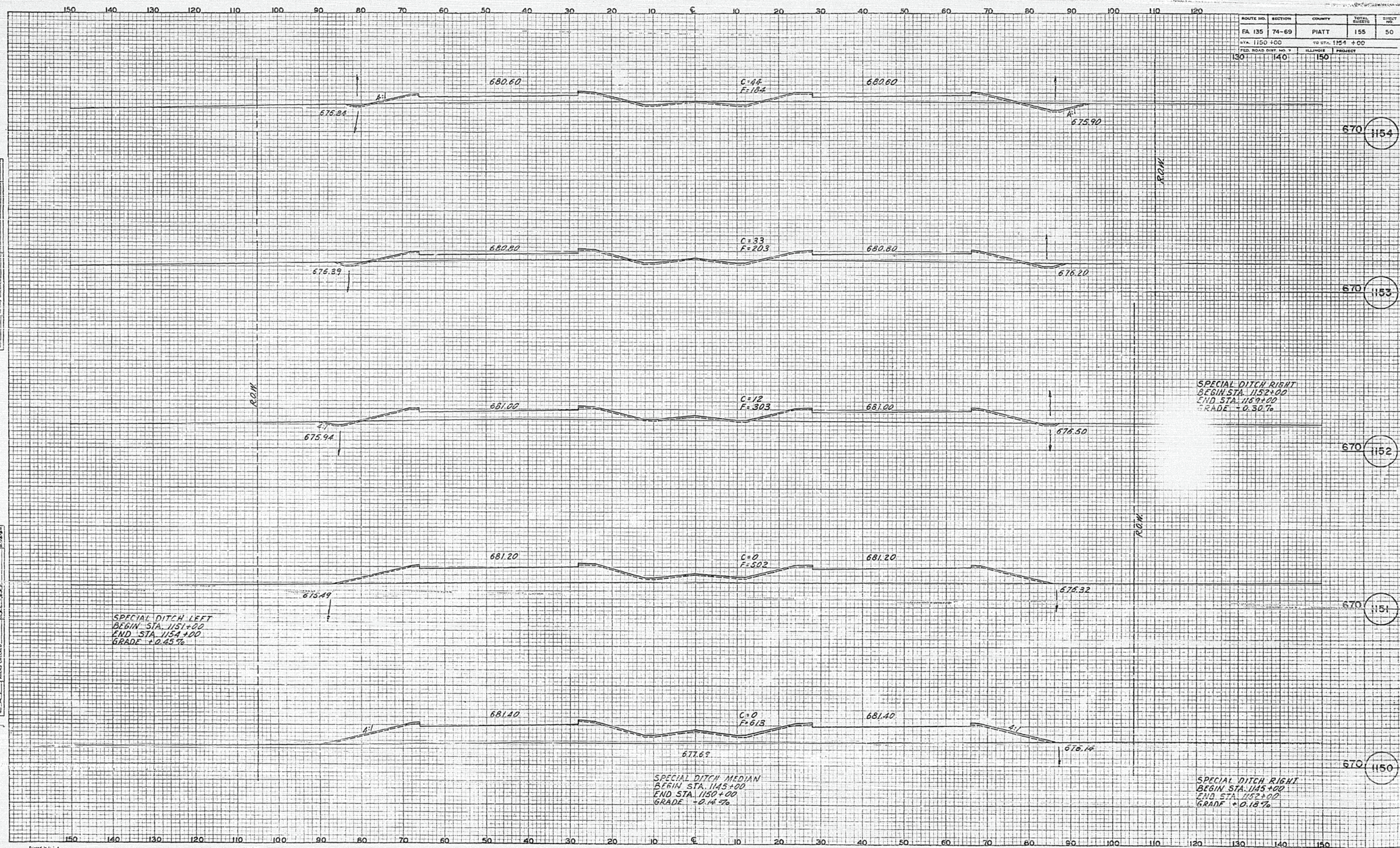


ORIGINAL SURVEY	DATE
SURVIVED	6-24-69
PLOTTED	9-25-69
TEMPERATURE	6-28-69
AREA	7-7-69
ANALYSIS	7-9-69
ANALYSIS CHECKED	7-11-69

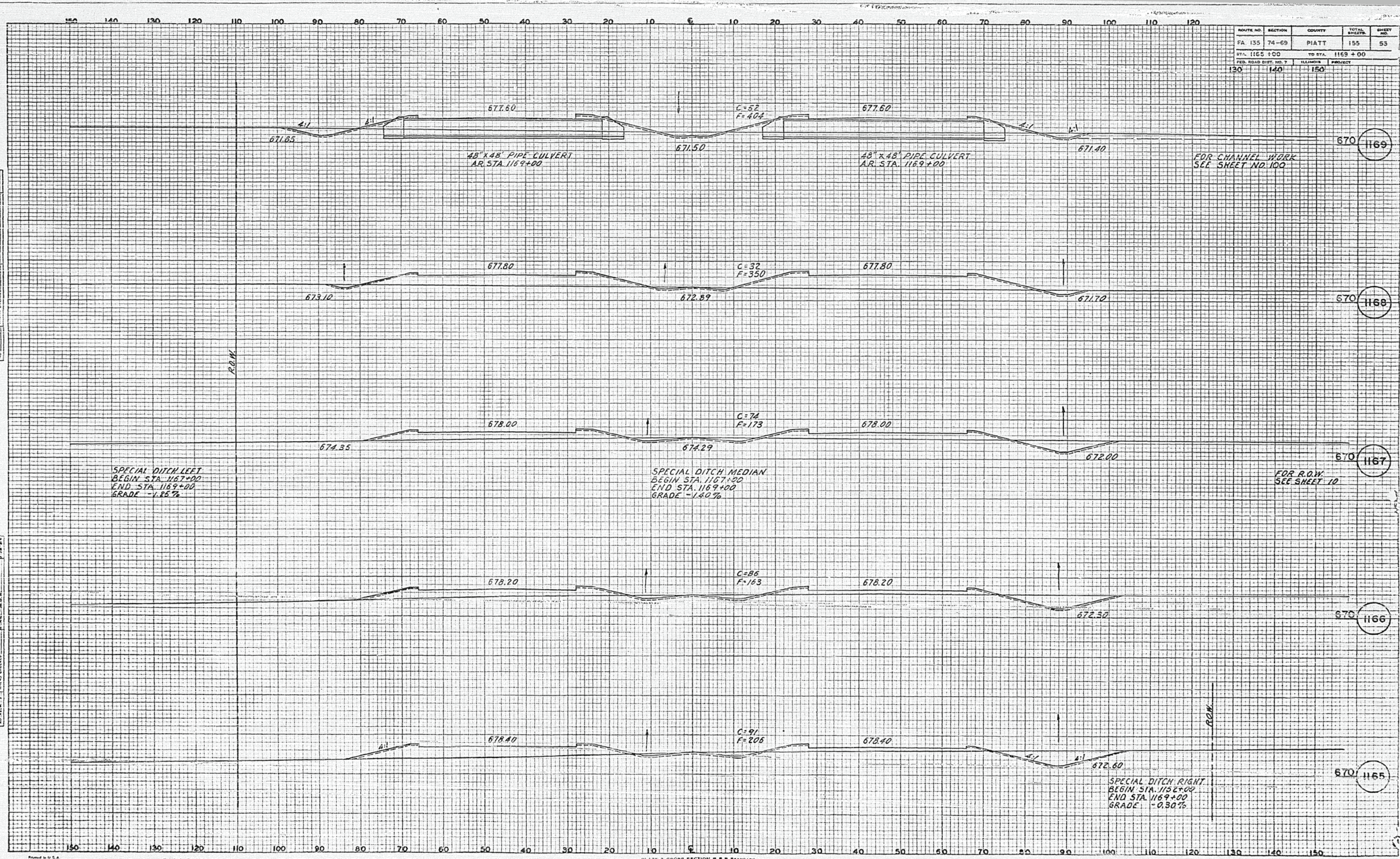
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ORIGINAL SURVEY	DATE	BY	SURVEYED
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11-58	10-23-58	CH	TEMPERATURE
12-58	11-10-58	CH	AFIAS
1-59	7-14-59	CH	AFIAS CHECKED
2-59	8-18-59	CH	

ORIGINAL SURVEY	DATE
SERVICED	12-28
PILOTED	10-17-59
TEMPLATE	11-21-59
AIRALS	1-12-60
AREAS CHANGED	8-11-60

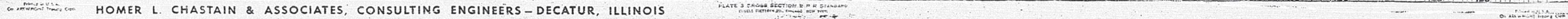


ORIGINAL SURVEY	SURVEYED	BY	DATE
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	TEMPLATE		
	AREAS		
	AREAS CHECKED		

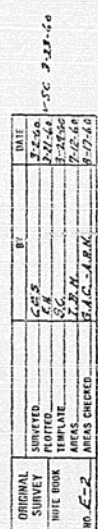


FINAL SURVEY	NO.	SURVEYED	BY	DATE
PLOTTED				
TEMPLATE				
AREAS				
AREAS CHECKED				

ORIGINAL SURVEY	SURVEYED	CITY	DATE
	PUBLISHED	<i>CEC</i>	<i>10-57</i>
	TITLE	<i>E.H.</i>	<i>11-4-59</i>
	AUTHOR	<i>K.L.M.</i>	<i>11-21-59</i>
		<i>J.E.H.</i>	<i>7-12-60</i>
		<i>G.A.C.</i>	<i>8-16-60</i>
		<i>F.W.S. CHICAGO</i>	



NO.	FINAL SURVEY	BY	DATE
	SUBMITTED		
	EDITED		
	NOTE BOOK		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		



SPECIAL DITCH RIGHT
BEGIN STA. 1169+00
END STA. 1175+00
GRADE +0.25%

FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
TEMPLATE		
AREAS		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SOLVED	<i>CL</i>	<i>2-17-60</i>
PLUDED	<i>EL</i>	<i>2-17-60</i>
TEPLATE	<i>EL</i>	<i>3-29-60</i>
AREAS	<i>1.8-11</i>	<i>2-12-60</i>
CHECKED	<i>1.8-11 - 1.8-11</i>	<i>2-12-60</i>

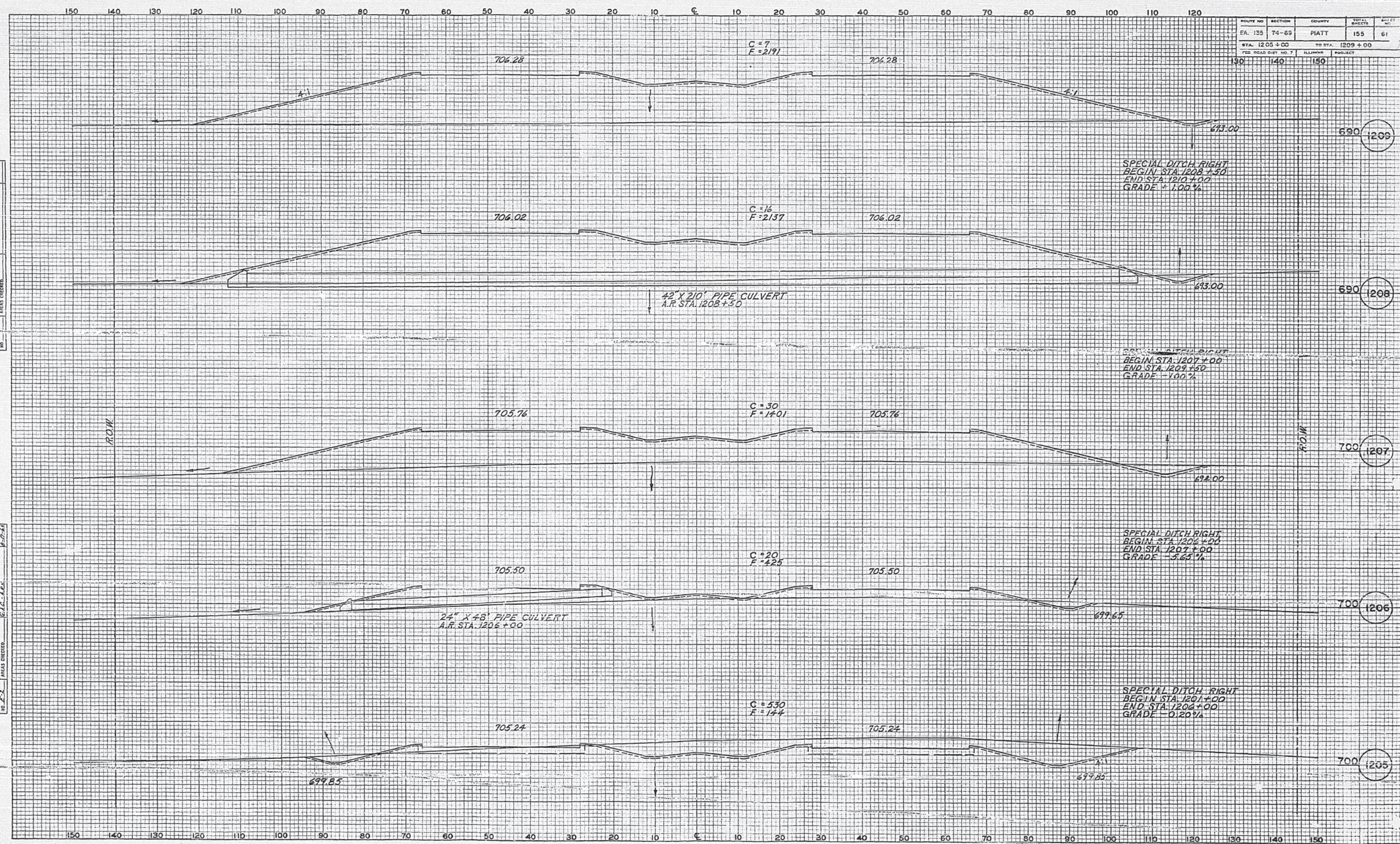


PLATE 3 CROSS SECTION B P R STANDARD
EUGENE DETIGER CO. CHICAGO, NEW YORK

Made in U.S.A.
by AIRWEIGHT Tension Cook

DATE	
BY	
PROJECT	
SECTION	
COUNTY	
TOTAL SHEETS	
SHEET NO.	
STA. 1205+00	
TO STA. 1209+00	
FED. ROAD DIST. NO. 7	
ILLINOIS PROJECT	
NO.	
DATE	
BY	
PROJECT	
SECTION	
COUNTY	
TOTAL SHEETS	
SHEET NO.	
STA. 1205+00	
TO STA. 1209+00	
FED. ROAD DIST. NO. 7	
ILLINOIS PROJECT	
NO.	

DATE	
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STA. 1205+00	
TO STA. 1209+00	
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ILLINOIS PROJECT	
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TO STA. 1209+00	
FED. ROAD DIST. NO. 7	
ILLINOIS PROJECT	
NO.	



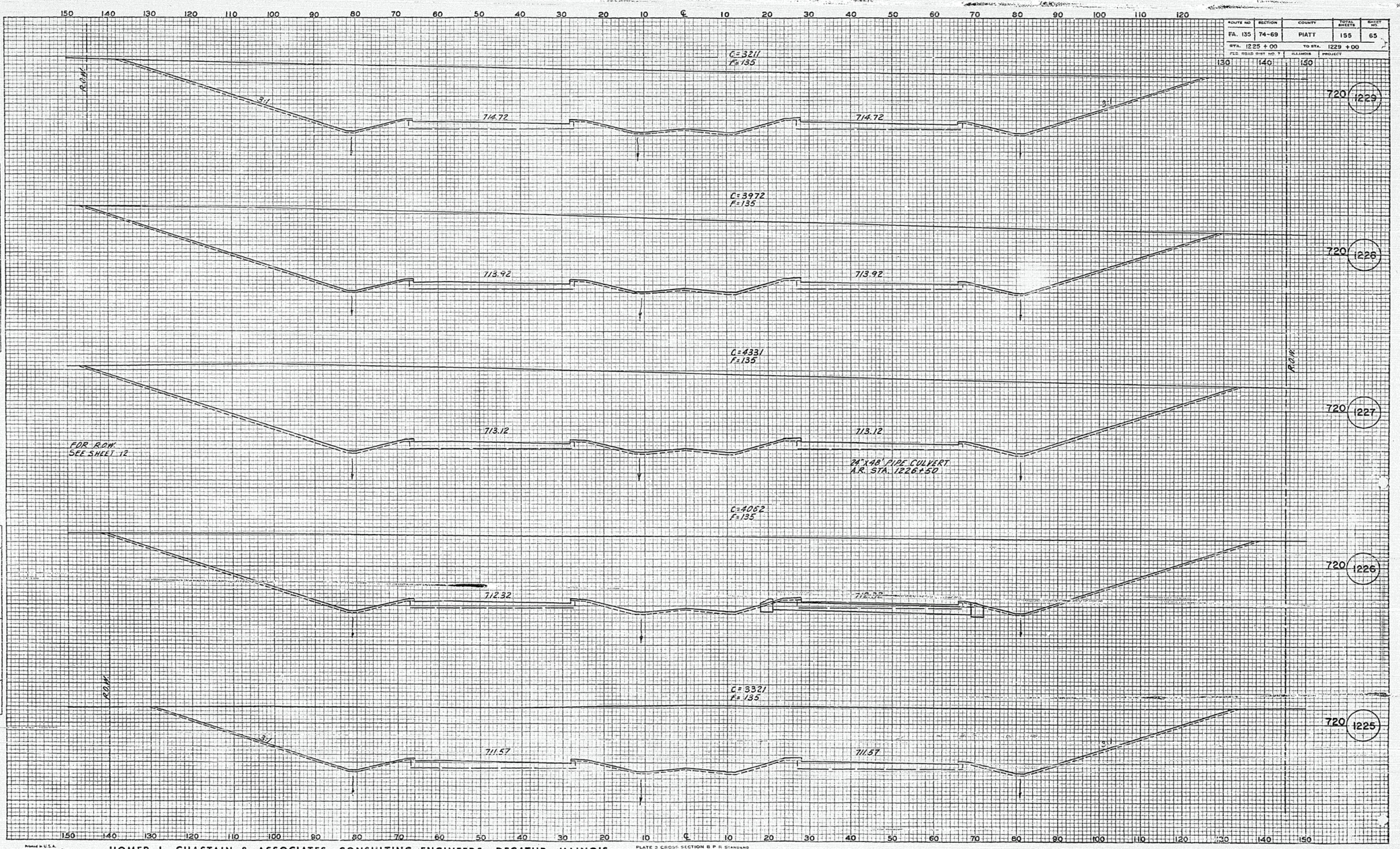
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3. CROSS SECTION B.P.R. STANDARD
EUGENE DETZLER CO. CHICAGO - NEW YORK

Printed in U.S.A.
On ARROW-POINT Tinting Cloth

DATE: _____
BY: _____
NO. _____
FINAL SURVEY
NOTE BOOK
NO. _____
AREA CHECKED

DATE: 8-28-60
BY: S.C. 8-28-60
NO. 100
ORIGINAL SURVEY
NOTE BOOK
NO. 100
AREA CHECKED



DATE	BY
3-22-60	J.C.F.
4-5-60	J.C.F.
7-12-60	J.C.F.
8-16-60	J.C.F.

DATE	BY
3-22-60	J.C.F.
4-5-60	J.C.F.
7-12-60	J.C.F.
8-16-60	J.C.F.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	C.
FA. 135	74-69	PIATT	155	66
STA. 1230 + 00	TO STA. 1234 + 00	ILLINOIS	PROJECT	



HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 2 CROSS SECTION B.P.R. STANDARD
INCH DETAIL CO. CHICAGO, ILL. 1964

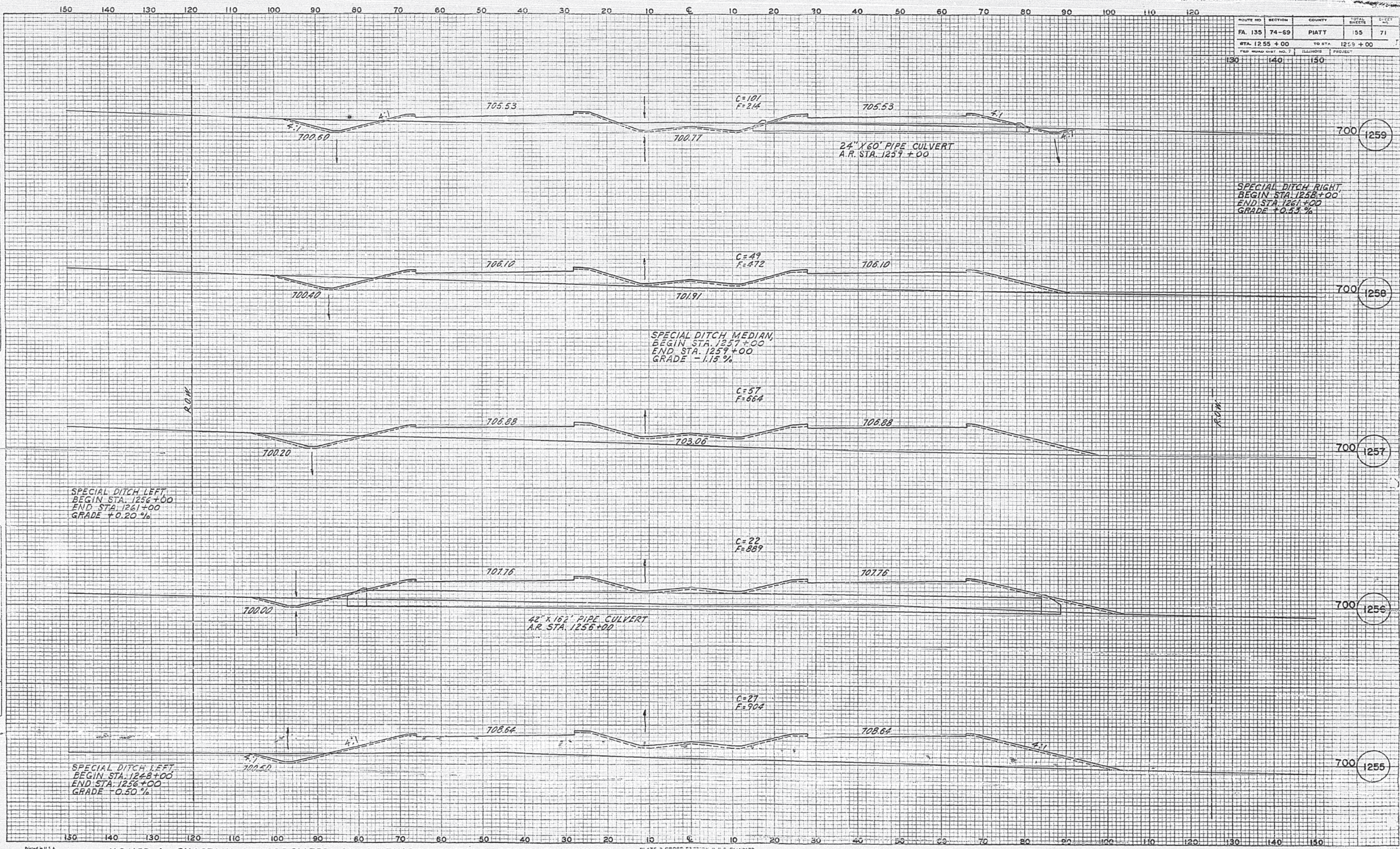
Printed in U.S.A.
On ARW 7001 Tracing Cloth

Printed in U.S.A.
On ARW 7001 Tracing Cloth

DATE	
BY	
CHECKED	
APPROVED	
DESIGNED	
PLANNED	
CONSTRUCTED	
MAINTAINED	
REMOVED	
NO.	

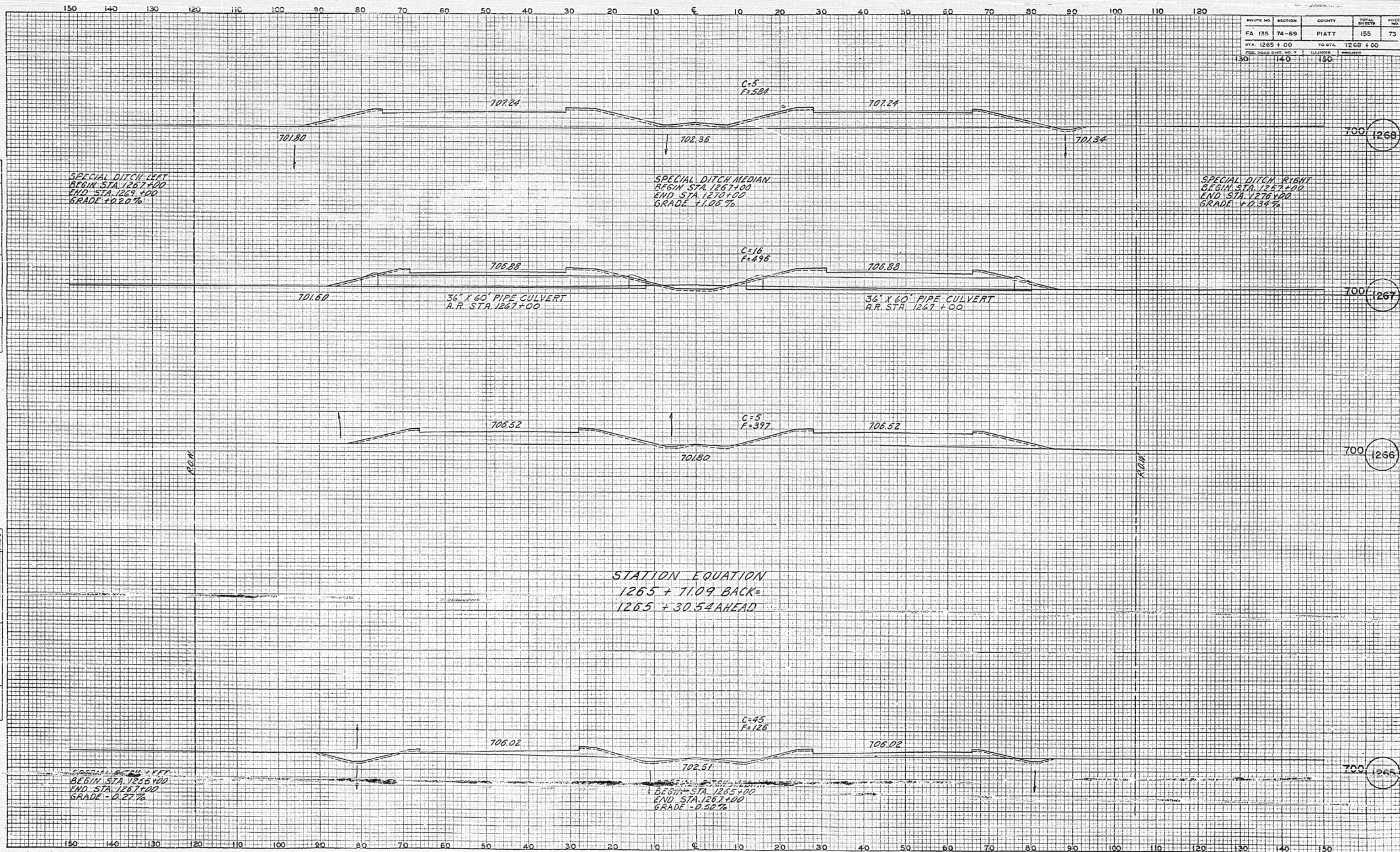
DATE	3-17-54
BY	J. L. B. C.
CHECKED	J. L. B. C.
APPROVED	J. L. B. C.
DESIGNED	J. L. B. C.
PLANNED	J. L. B. C.
CONSTRUCTED	J. L. B. C.
MAINTAINED	J. L. B. C.
REMOVED	J. L. B. C.
NO.	22

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-89	PIATT	155	71
STA. 1255 + 00	TO STA. 1259 + 00			
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		



NO	FINAL SURVEY	SURVEYED	BY	DATE
	NOTE BOOK	PLOTTED		
		TEMPLATE		
		AREAS		
		AREAS CHECKED		

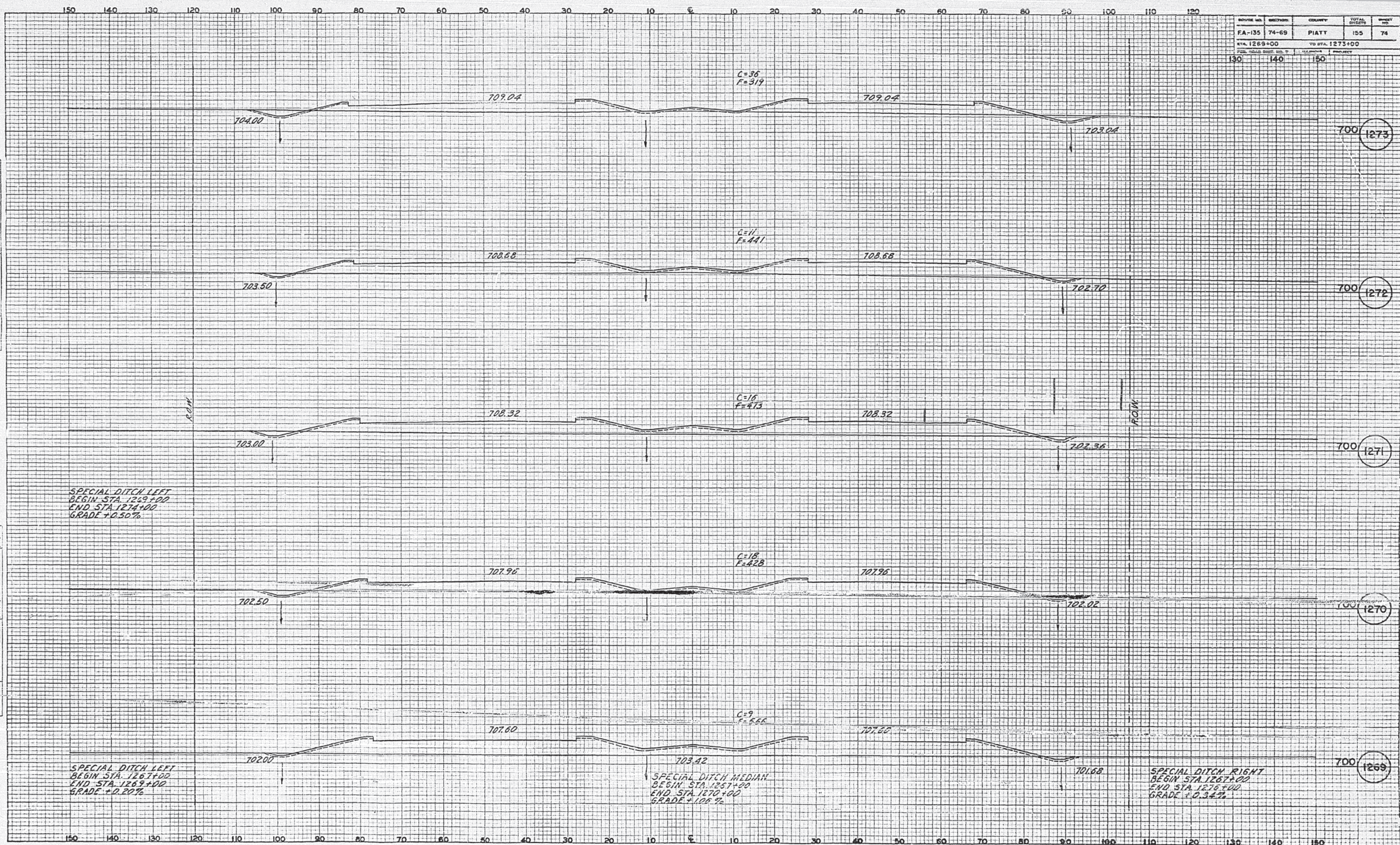
ORIGINAL	SURVEYED	BT	DATE
SURVEY	6/3		10-22-58
NOTE BOOK	6/4		11-7-58
	TEMP 1 2 3 4 5 6 40		12-24-58
	ARIAS		7-11-58
	ARIAS CHECKED		7-12-58



DATE	BY	REVISION	REVISION
FINAL SURVEY	REVISION	REVISION	REVISION
DATE	BY	REVISION	REVISION

DATE	BY	REVISION	REVISION
ORIGINAL SURVEY	REVISION	REVISION	REVISION
DATE	BY	REVISION	REVISION

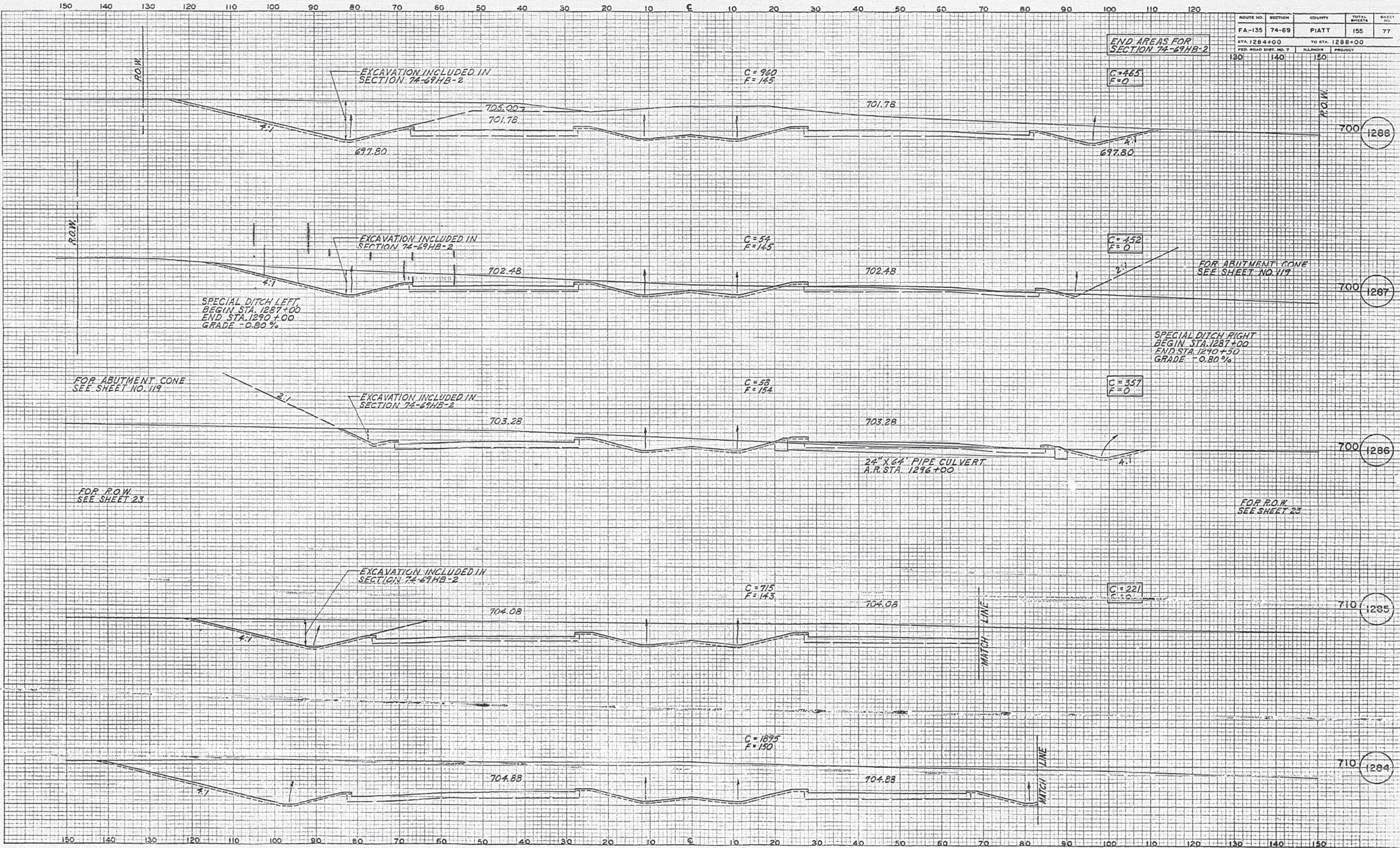
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	74
STA. 1269+00		TO STA. 1273+00		
130		140		



DATE	
BY	
DESIGNED	
CHECKED	
APPROVED	
NO.	

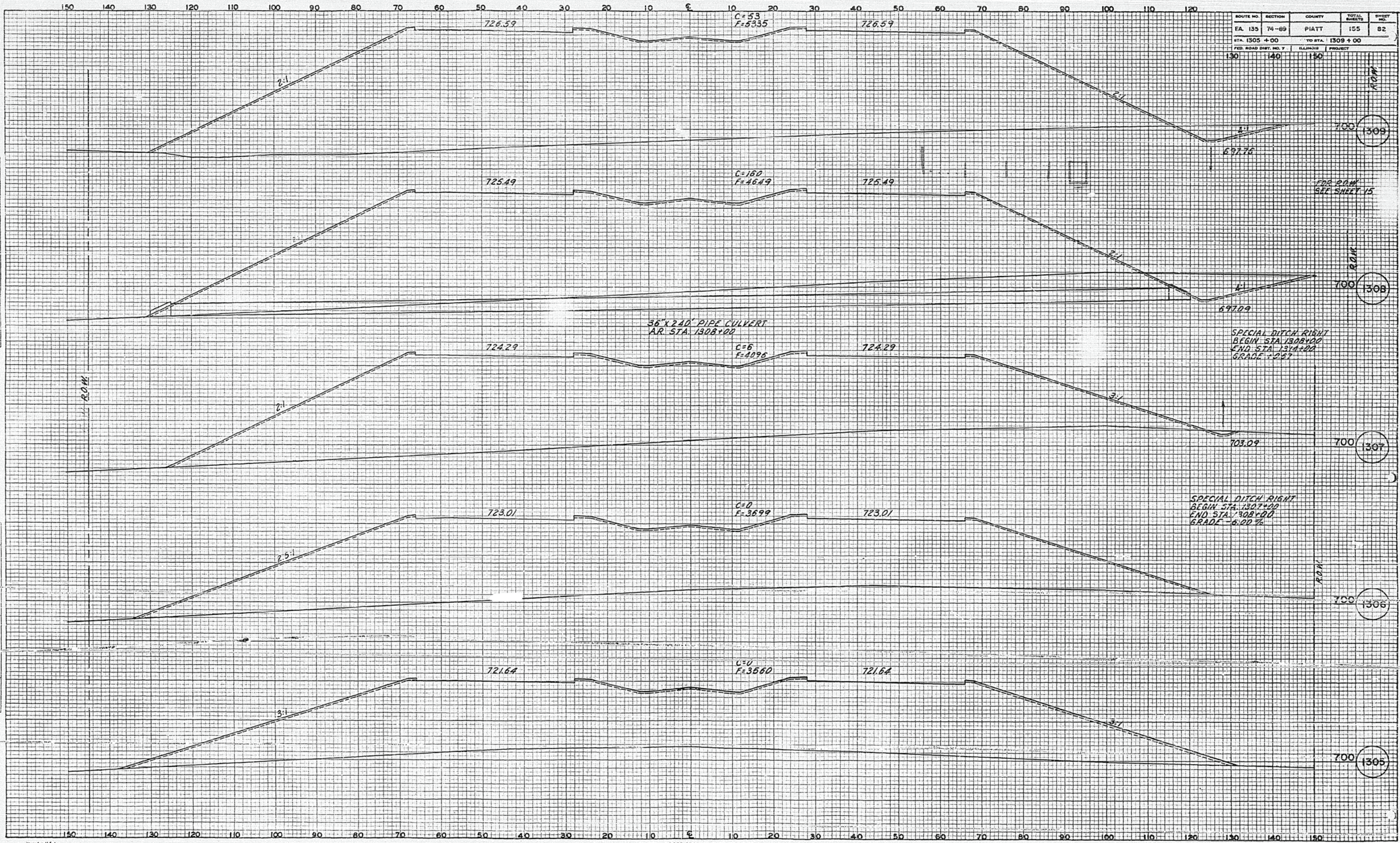
DATE	12-22-57
BY	J. M. C. 11-1-59
DESIGNED	C.H.
CHECKED	T.H.
APPROVED	A.H.H.
NO.	5-1

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	77
STA. 1284+00		TO STA. 1286+00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		



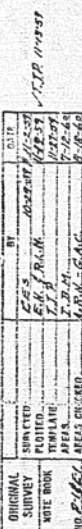
DATE	BY	DATE	BY
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES

DATE	BY	DATE	BY
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES
11-15-54	SES	11-15-54	SES



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA 135	74-89	PIATT	155	82
STA. 1305 + 00	TO STA. 1309 + 00			
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		

FINAL SURVEY	SURVEYED	B1	0112
NOTE BOOK	PLOTTED		
	TEMPLATE		
#0	APL5		
	ASEA3 CHECKED		



150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120

ROUTE NO.	SECTION	COUNTY	TOTAL SHEET	SHEET NO.
FA. 135	74-69	PIATT	155	86
STA. 1325 + 00	TO STA.	1329 + 00		
FED. ROAD DIST. NO. 7	HAZARD	PROJECT		
130	140	150		

FOR ROW
SEE SHEET 15

C=506
F=16

730 1329

C=1568
F=0

740 1328

C=3708
F=0

750 1327

C=4574
F=0

750 1326

C=5728
F=0

750 1325

SPECIAL DITCH LEFT
BEGIN STA. 1321+00
END STA. 1326+50
GRADE +0.95%

PLATE 3 CROSS SECTION D P.P. STANDARD
ENGINEERING CO. CHICAGO - ILL. 1934

HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

Printed in U.S.A.
On ARK-WIGHT Trough Cloth

DATE	
BY	
PROJECT	
LOCATION	
AREA	
DATE CHECKED	
NO.	

DATE	11-18-51
BY	J.P.
PROJECT	135
LOCATION	74-59
AREA	155
DATE CHECKED	11-18-51
NO.	1

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA. 135	74-59	PIATT	155	66
STA. 1335 + 00			TO STA. 1339 + 00	
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		

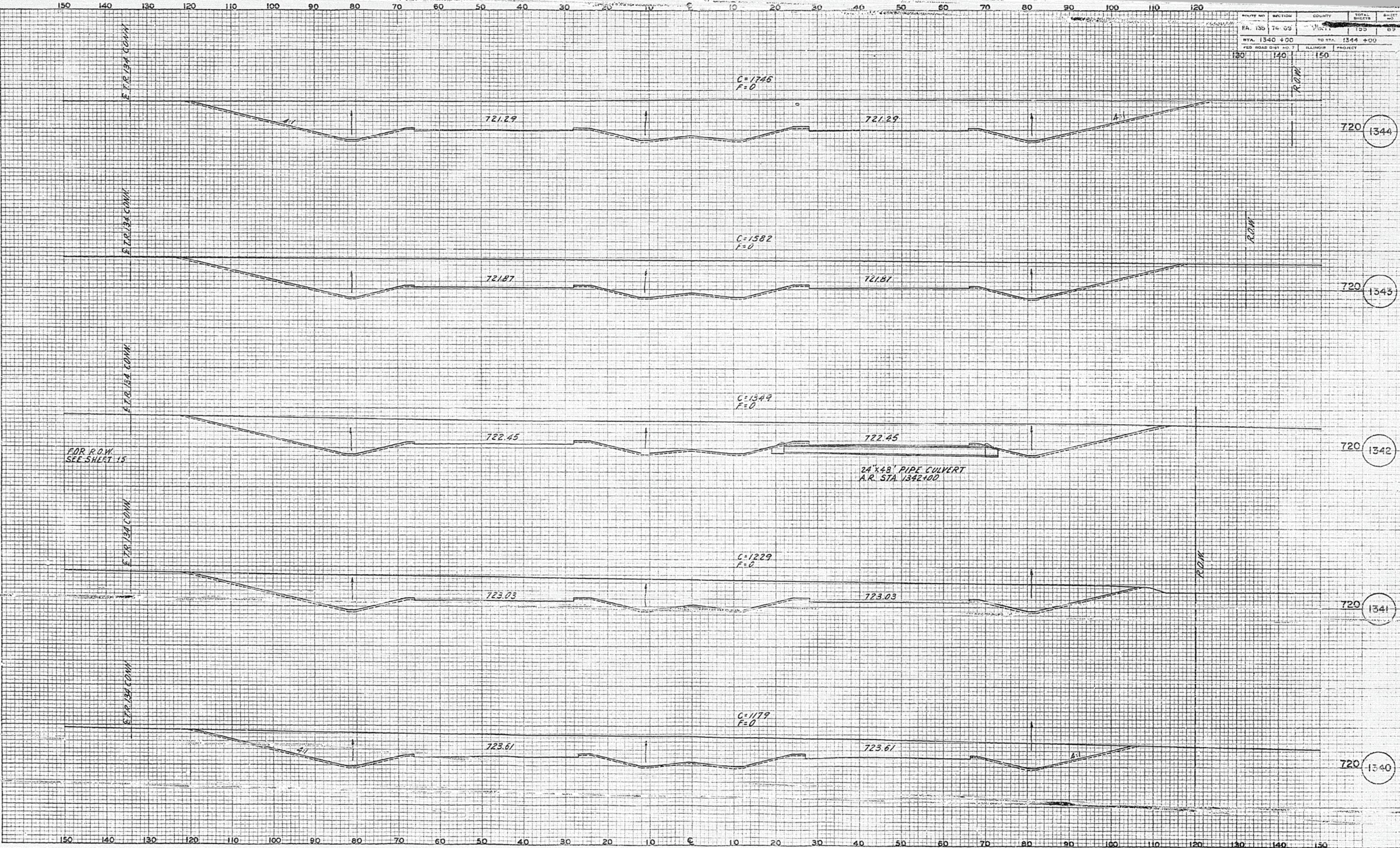


HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION E.T.R. STANDARD
ELEVATION BETWEEN 120 - CHICAGO - 1950

DATE	BY
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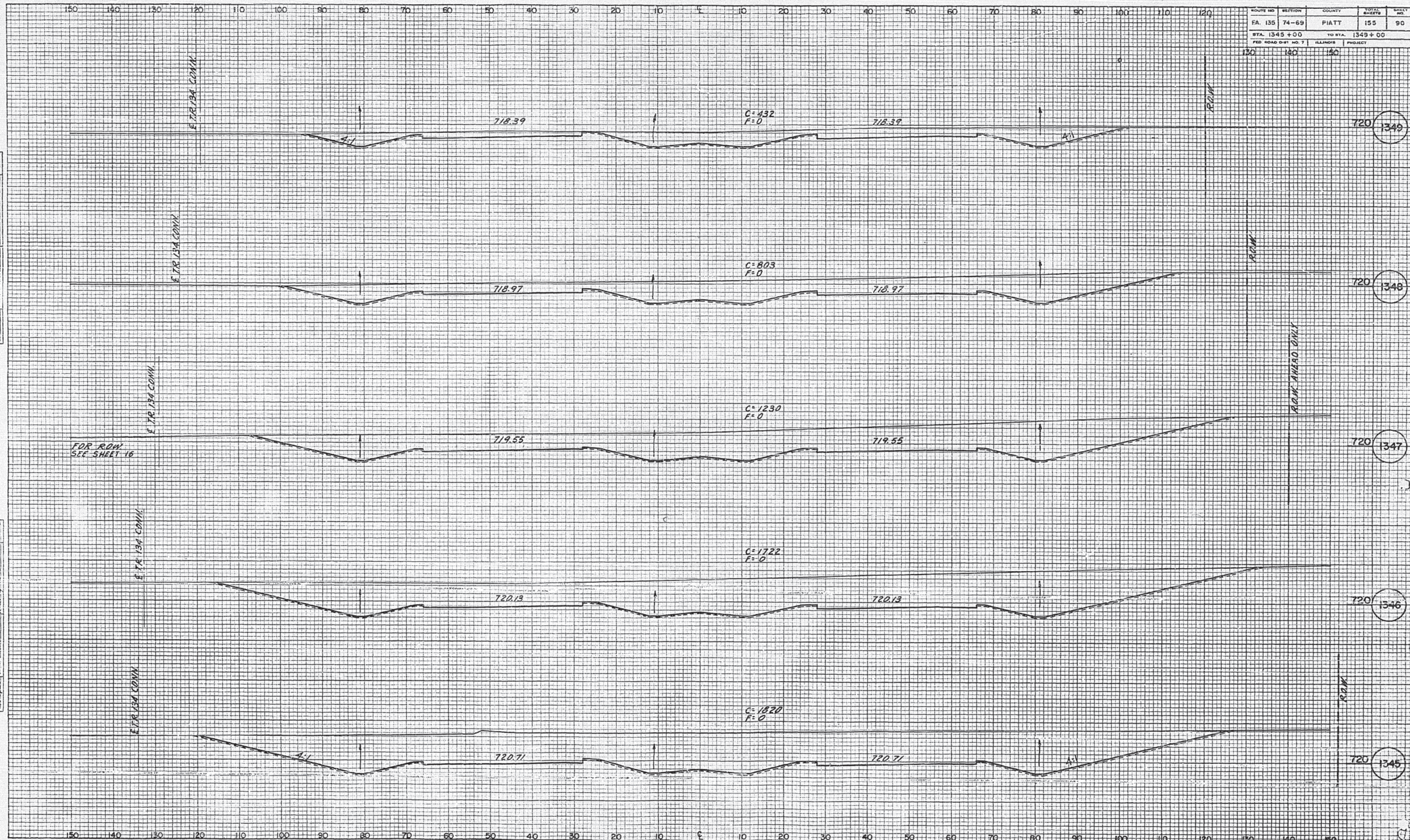
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION D P H STANDARD

DATE _____
BY _____
FINAL SURVEY _____
NOTED BOOK _____
MP _____

DATE _____
BY _____
ORIGINAL SURVEY _____
NOTED BOOK _____
MP _____

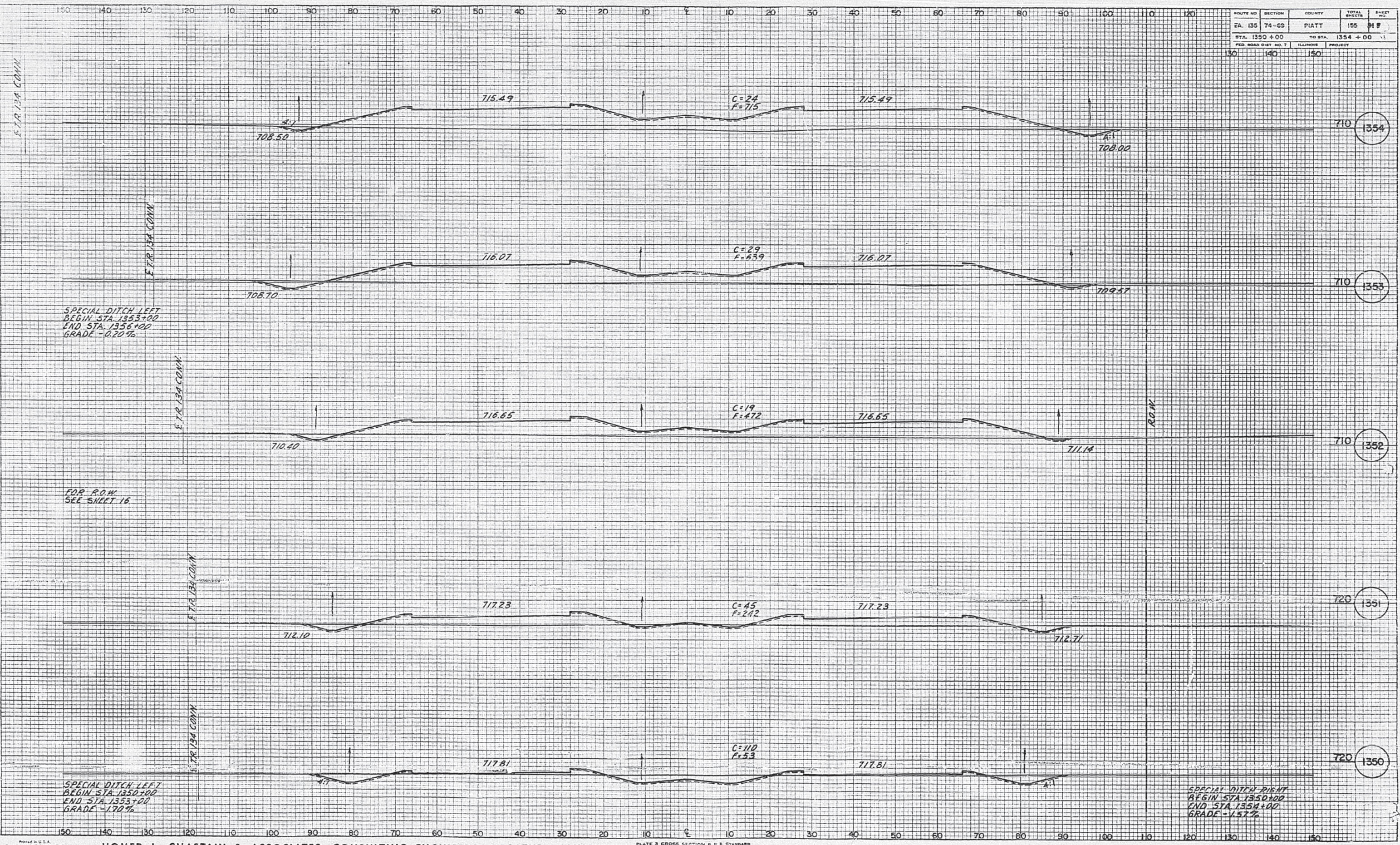
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA. 1345+00	TO STA. 1349+00			
FED. ROAD DIST. NO. 7	ILLINOIS PROJECT			



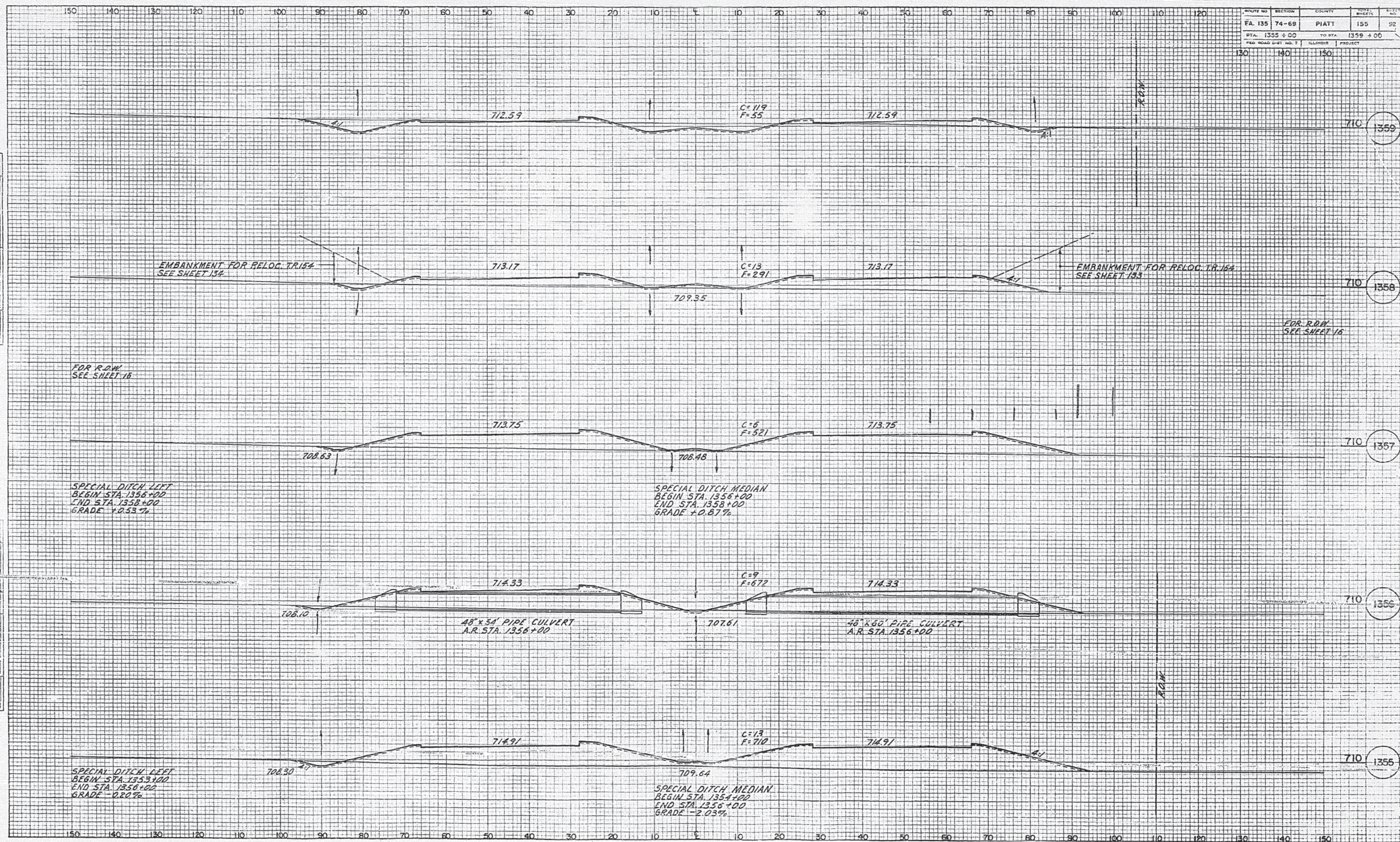
DATE	BY
APPROVED	DATE
NOTED	DATE
AREA	DATE
AREA	DATE

DATE	BY
APPROVED	DATE
NOTED	DATE
AREA	DATE
AREA	DATE

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA. 135	74-63	PLATT	155	918
STA. 1350+00	TO STA. 1354+00			
FED. ROAD DIST. NO. 7	ILLINOIS PROJECT			
130	140	150		



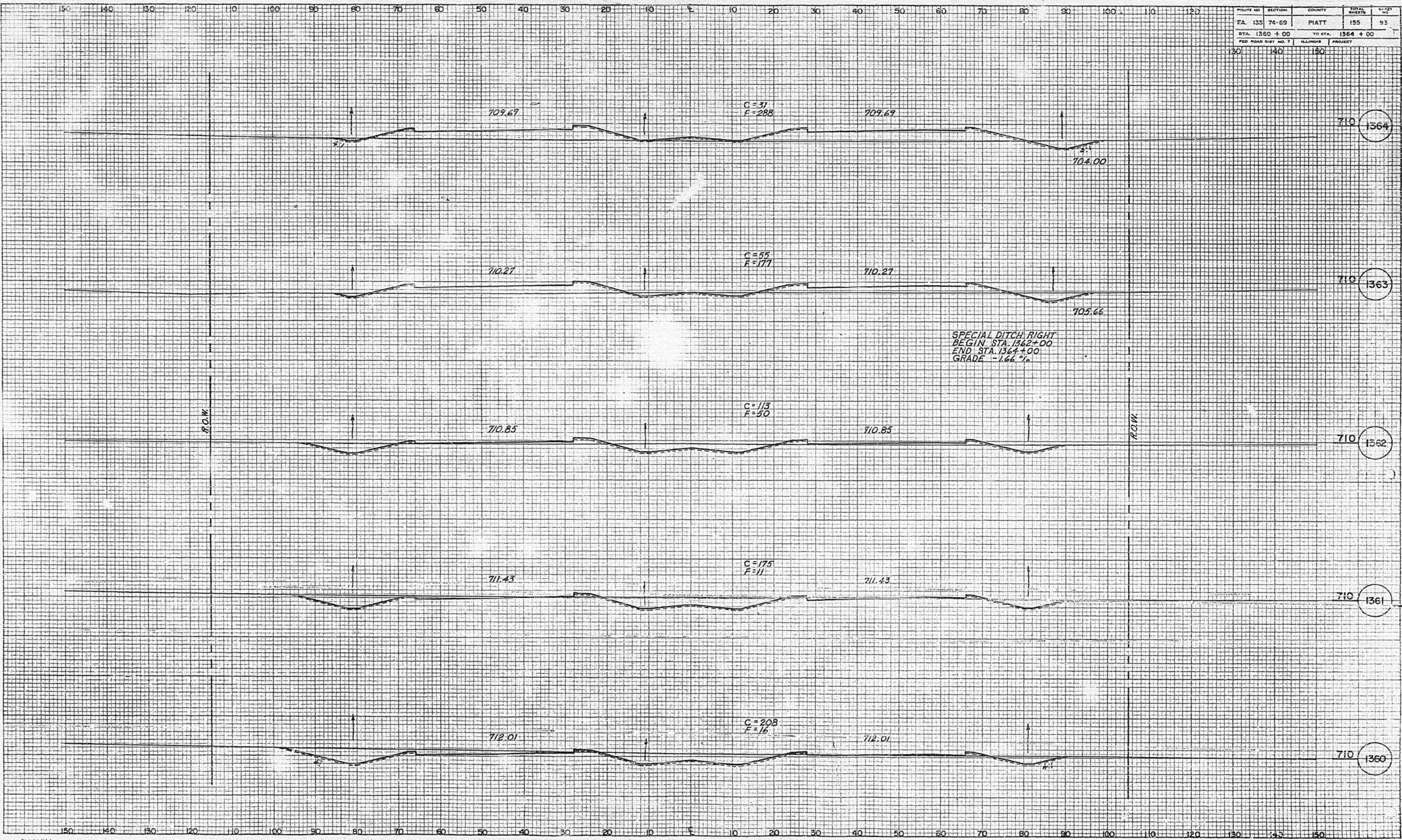
ORIGINAL SURVEY	SURVEYED	DATE
IE BOOK	CFS	4-18-60
	GAC (-E)	4-18-60
	GAC	4-18-60
	LEH	4-18-60
	ARN & GAC	4-18-60



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA. 135	74-69	PIATT	155	93
STA. 1360 + 00		TO STA. 1364 + 00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		

DATE	
BY	
FINAL SURVEY	
NOTED BY	
NO.	

DATE	10/1/54
BY	GLC
ORIGINAL SURVEY	
NOTED BY	GLC
NO.	10/1/54

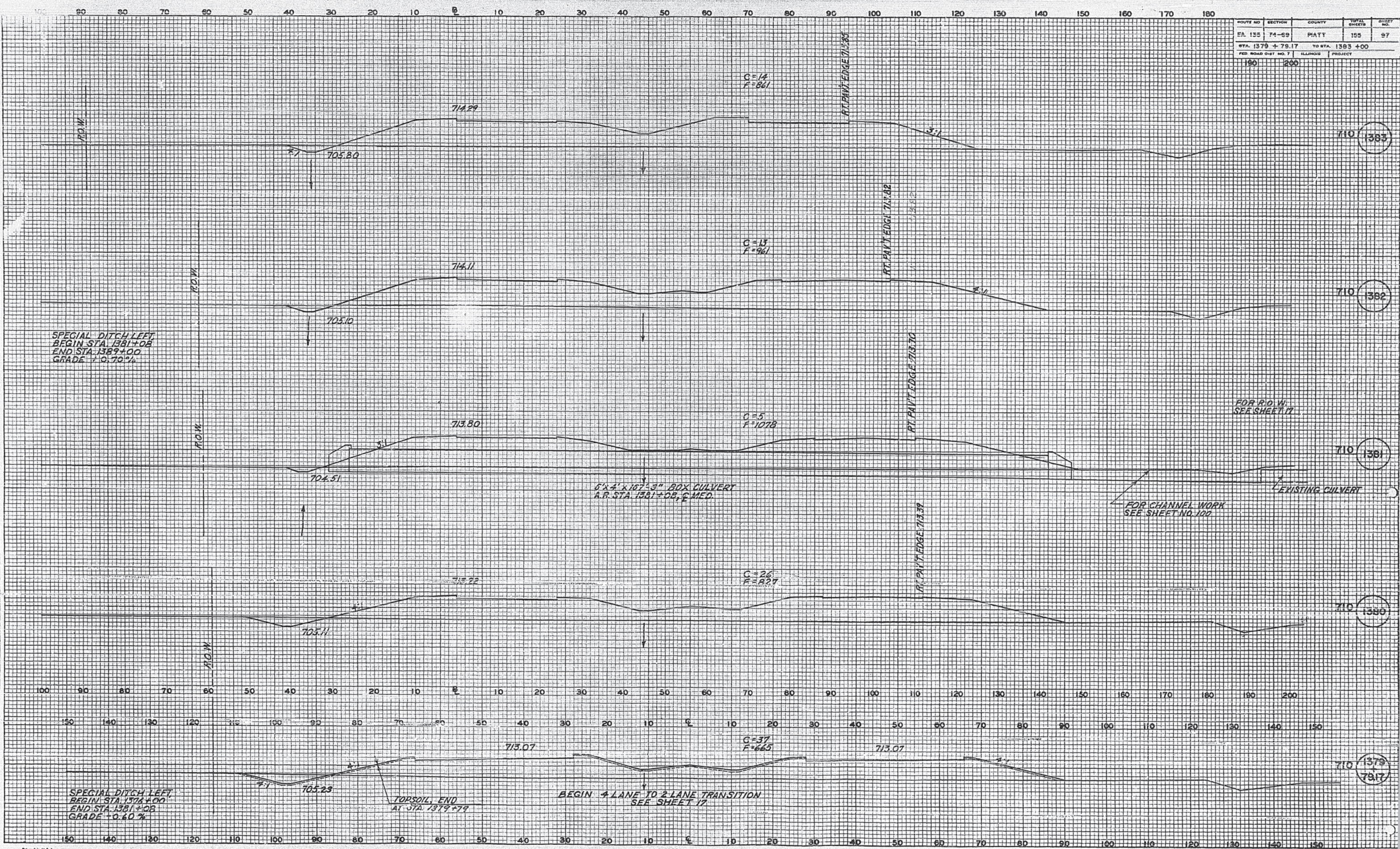


HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION B P R STANDARD
ENGINE BENTON CO. CHICAGO - NEW YORK

Printed in U.S.A.
On ADWRIGHT Tracing Cloth

ROUTE NO.	SECTION	COUNTY	TYPAL SHEETS	SHEET NO.
EA 135	74-59	PIATT	155	97
STA. 1379 + 79.17		TO STA. 1383 + 00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		
100	200			



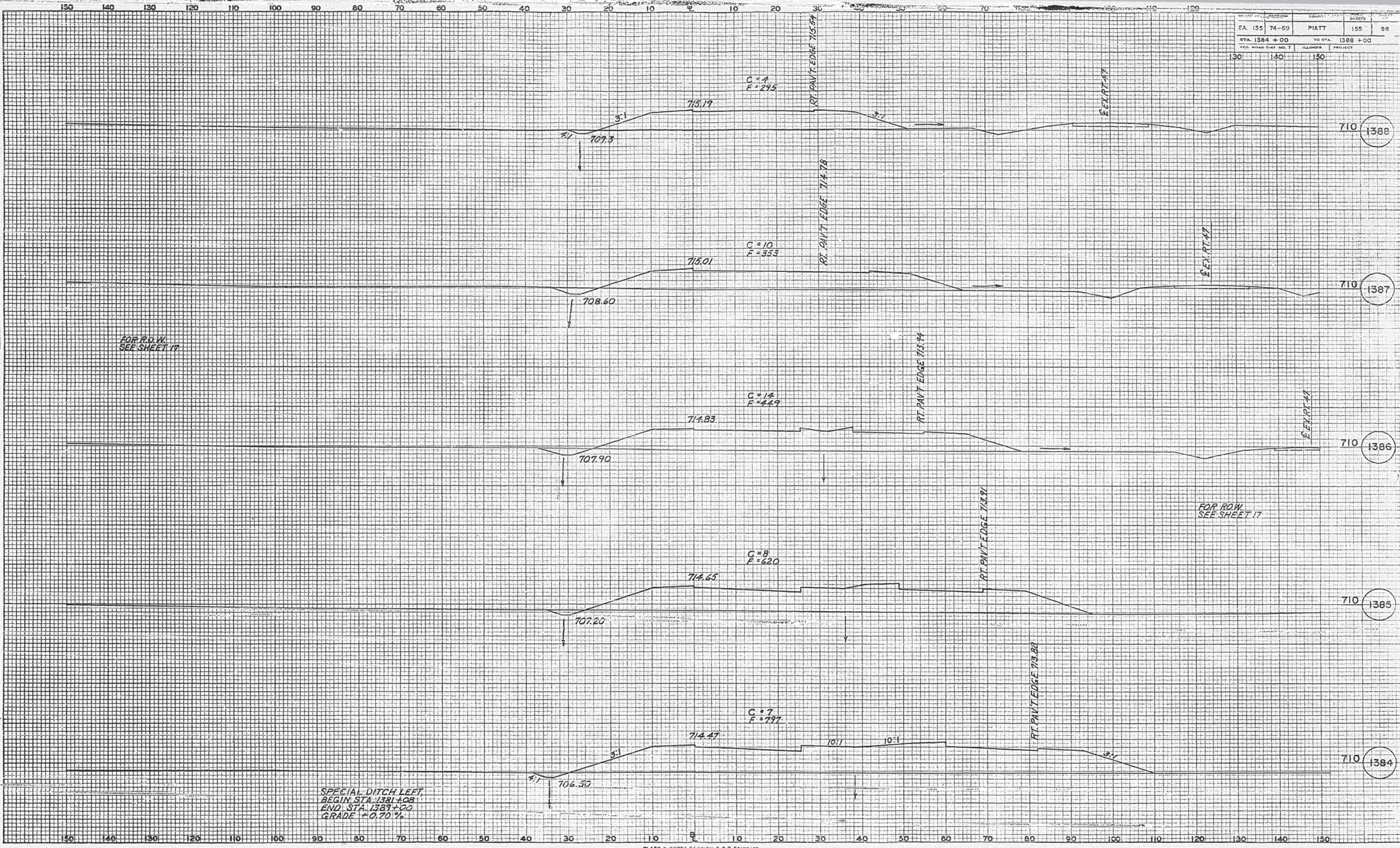
DATE	BY
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5/19/64	W.L.C.
5/20/64	W.L.C.
5/21/64	W.L.C.
5/22/64	W.L.C.
5/23/64	W.L.C.
5/24/64	W.L.C.
5/25/64	W.L.C.
5/26/64	W.L.C.
5/27/64	W.L.C.
5/28/64	W.L.C.
5/29/64	W.L.C.
5/30/64	W.L.C.
5/31/64	W.L.C.

DATE	BY
5/18/64	W.L.C.
5/19/64	W.L.C.
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5/29/64	W.L.C.
5/30/64	W.L.C.
5/31/64	W.L.C.

ROUTE 74-69	PIATT	155	98
STA. 1384 + 00	TO STA. 1388 + 00		
RED ROAD DIST. NO. 7	ILLINOIS	PROJECT	

FINAL	BY	DATE
SURVEY		
NOTED		
PLATTED		
AREAS		
AREAS		
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ORIGINAL	BY	DATE
SURVEY		
NOTED		
PLATTED		
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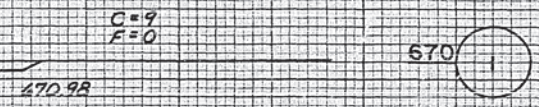
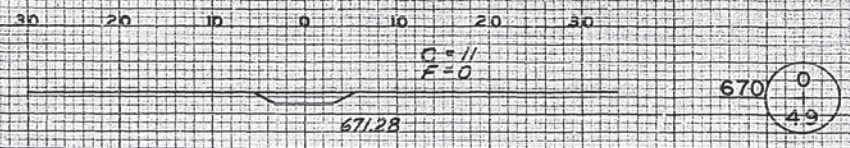
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION 3 P R STANDARD
EUGENE DETZER CO. CHICAGO - NEW YORK

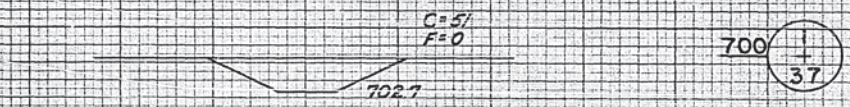
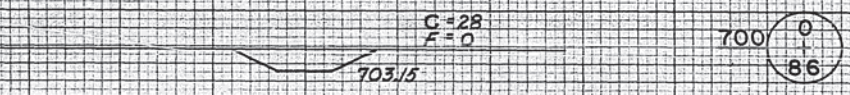
Printed in U.S.A.
On AIRWRIGHT Tracing Cloth

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA. 155	74-69	PIATT	155	100
STA. 1394+00		TO STA. 1396+93.67		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		

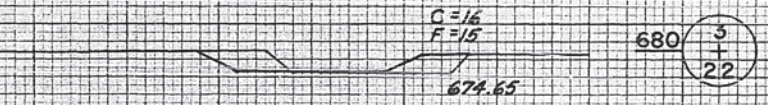
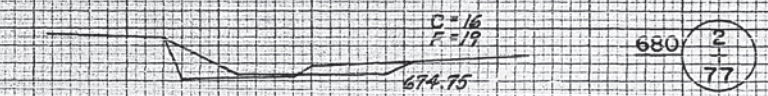
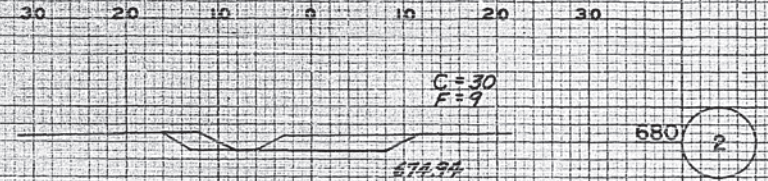
CHANNEL RT. STA. 1169+00



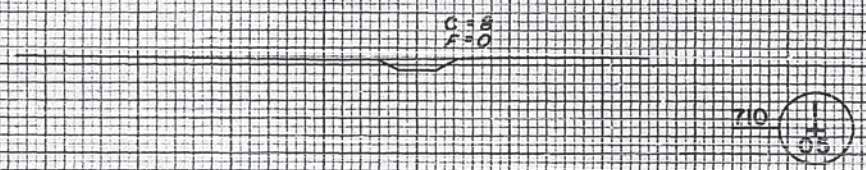
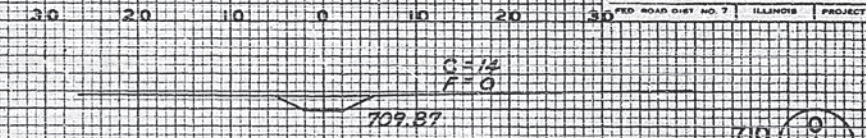
CHANNEL RT. STA. 1381+08



CHANNEL LT. & RT. STA. 65+40 (RELOC. RT. 47)



CHANNEL RT. STA. 72+00 (RELOC. RT. 154)



FOR R.O.W. SEE SHEET 17

EX. RT. 47
716.70
C=3
F=0

EX. RT. 47
716.41
C=5
F=0

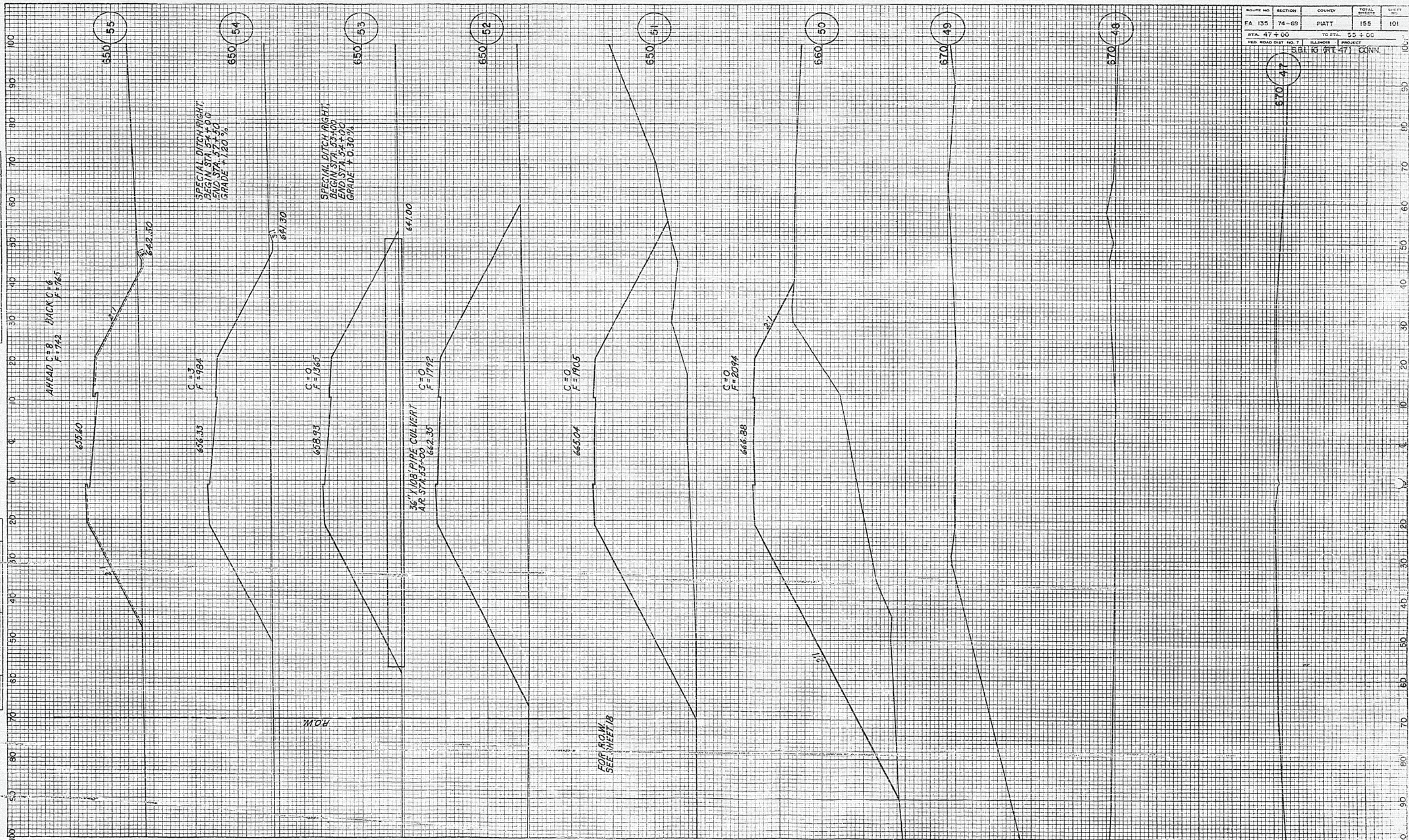
120 + 93.67

120 1394

FINAL	DATE
SURVEY	BY
NOTED	BY
REVISION	BY
NO.	

ORIGINAL	DATE
SURVEY	BY
NOTED	BY
REVISION	BY
NO.	

12th 7-6-60 M.L.I.



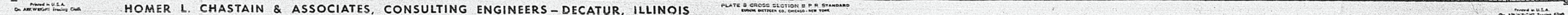
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA 135	74-69	PIATT	155	101
STA. 47+00		TO STA. 55+00		
FED. ROAD DIST. NO. 7		ILLINOIS		PROJECT
		SHEET NO. 47		CONN.

HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CRO'S SECTION B P H STANDARD
FIDELITY ENGINEERING CO., CHICAGO, ILL. 1957

Printed in U.S.A.
On ARK-WIGHT Tracing Cloth

ORIGINAL	DATE
SURVEYED	10-2-59
PLOTTED	1-13-60
TEMPERATURE	62.0
AREAS	1-13-60
AREAS CHECKED	1-13-60



ROUTE NO	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
FA. 135	74-59	PIATT	155	102
STA. 56 + 00		TO STA. 64 + 00		
FED ROAD DIST NO. 7		II JNGIS	PROJECT	
SR110 (RT 47) - CONN				

DATE	BY	FINAL SURVEY	NOTED	NOTES
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60

DATE	BY	ORIGINAL SURVEY	NOTED	NOTES
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60
1/1/60	1/1/60	1/1/60	1/1/60	1/1/60

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-69	PIATT	155	103
STA. 65 + 00	TO STA. 71 + 60	ILLINOIS	PROJECT	
FED. ROAD DIST. NO. 7	SB. 110 (RT. 47) DOWN			



HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION B P R STANDARD
ENGINE DETROIT CO. CHICAGO - NEW YORK

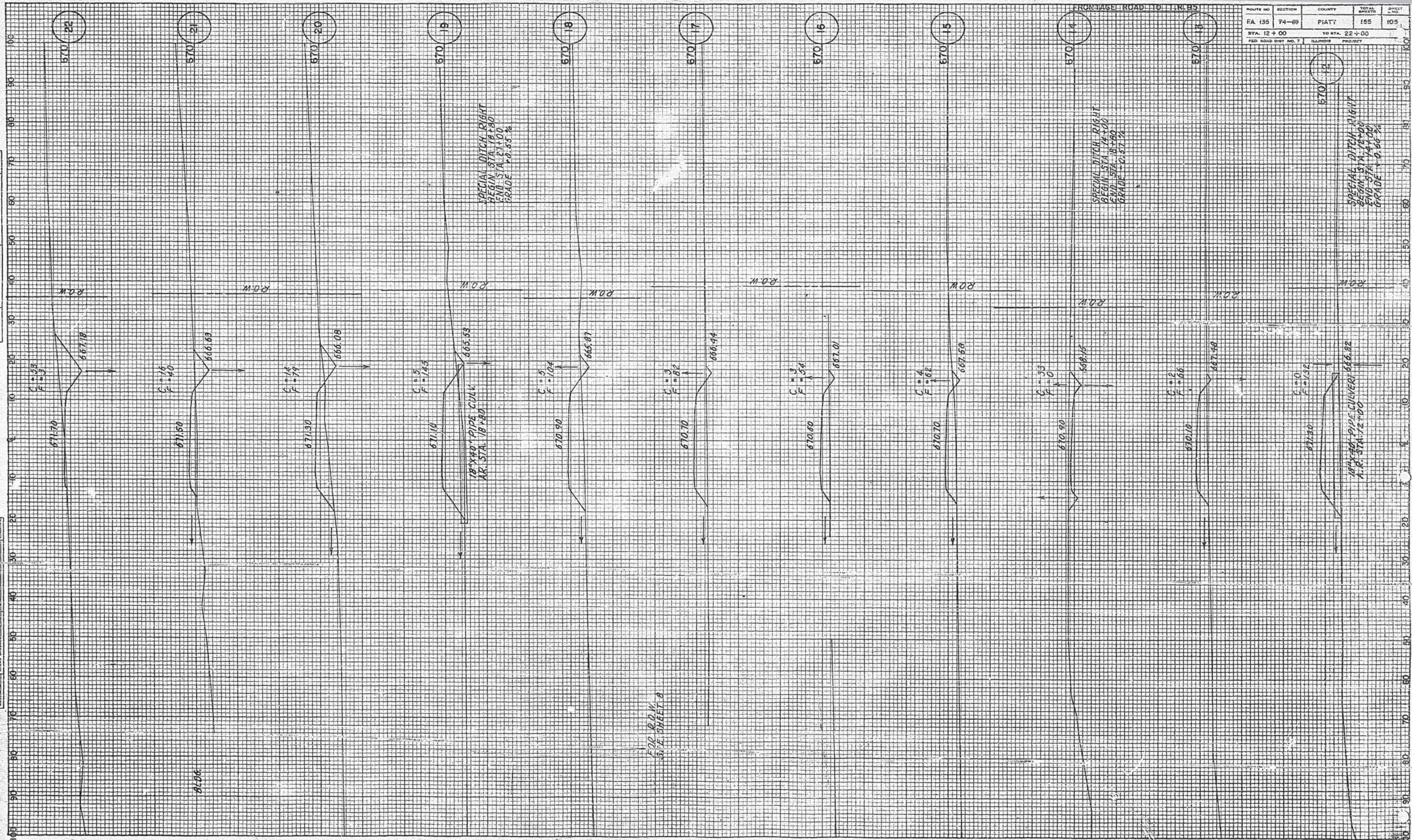
Printed in U.S.A.
By ARNOLD & SONS, Inc.

ORIGINAL		SURVEYED	DATE
SURVEY	<i>d.c.</i>	<i>6-2-58</i>	<i>7-2-58</i>
NOTE BOOK	PLOTTED (<i>M.S.C. 212/CD</i>)	<i>E.H.</i>	<i>7-8-60</i>
	TEMPERATURE	<i>E.H. (C.L.M. 212/2)</i>	<i>7-12-59</i>
	AREAS	<i>E.P.H.</i>	<i>6-27-58</i>
	AREAS CHECKED	<i>E.Y.</i>	<i>6-13-58</i>



FINAL	DATE	BY
SURVEY		
PLATT		
NOTE BOOK		
AREA		
NO.		

ORIGINAL	DATE	BY
SURVEY		
PLATT		
NOTE BOOK		
AREA		
NO.		



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA 135	74-80	PIATT	155	105
STA. 12+00		TO STA. 22+00		
74-80 DIST. NO. 7		ILLINOIS PROJECT		

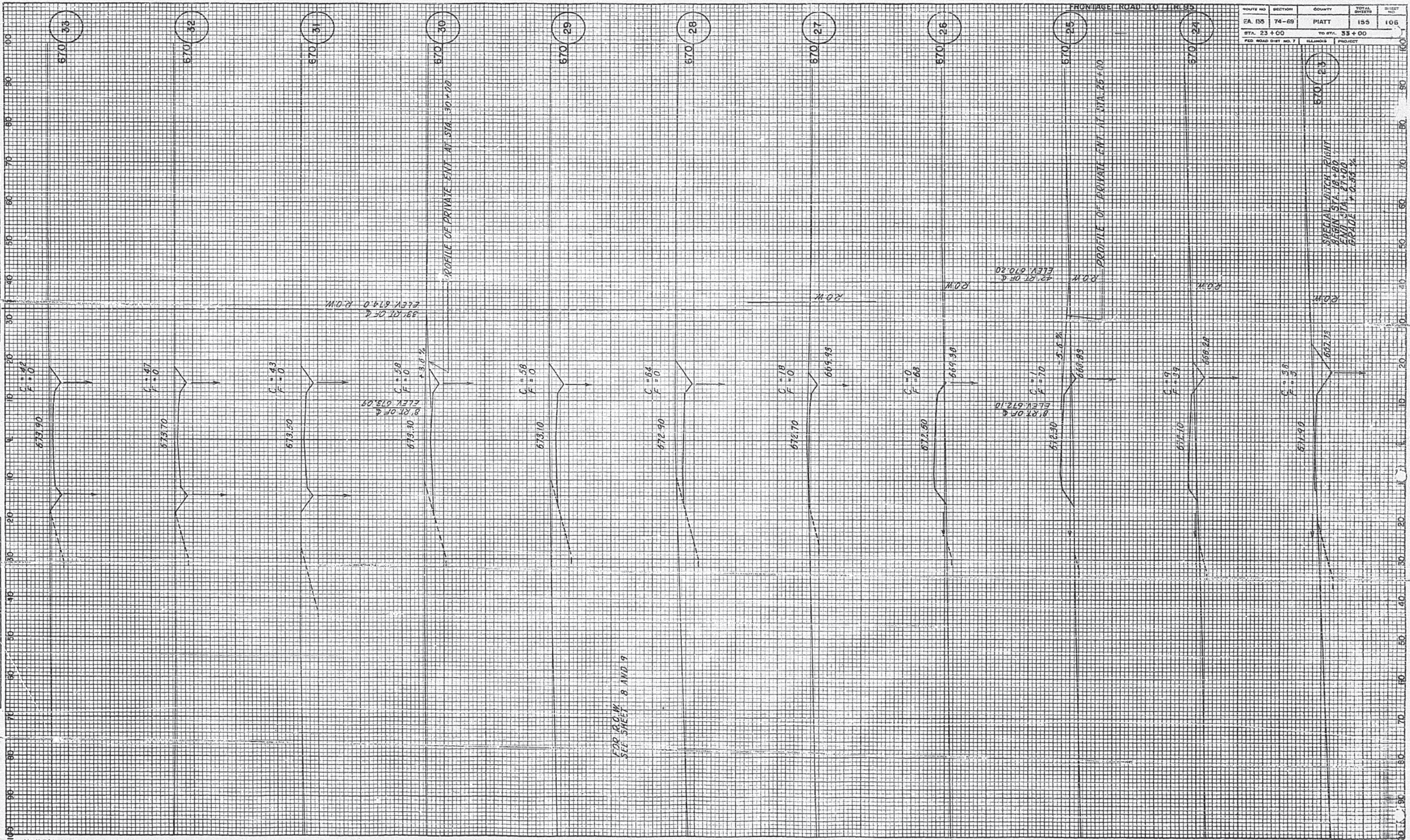
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION 6 P R STANDARD
SWENSON & SUTHERLAND CO. CHICAGO - NEW YORK

Printed in U.S.A.
On 40% Recycled Paper

ORIGINAL SURVEY	DATE
NO. 111-A	6-11-35
NOTE BOOK	6-11-35
THIN PLATE	6-11-35
AREA	6-11-35
AREAS CHECKED	6-11-35

FINAL SURVEY	DATE
NO. 111-A	6-11-35
NOTE BOOK	6-11-35
THIN PLATE	6-11-35
AREA	6-11-35
AREAS CHECKED	6-11-35

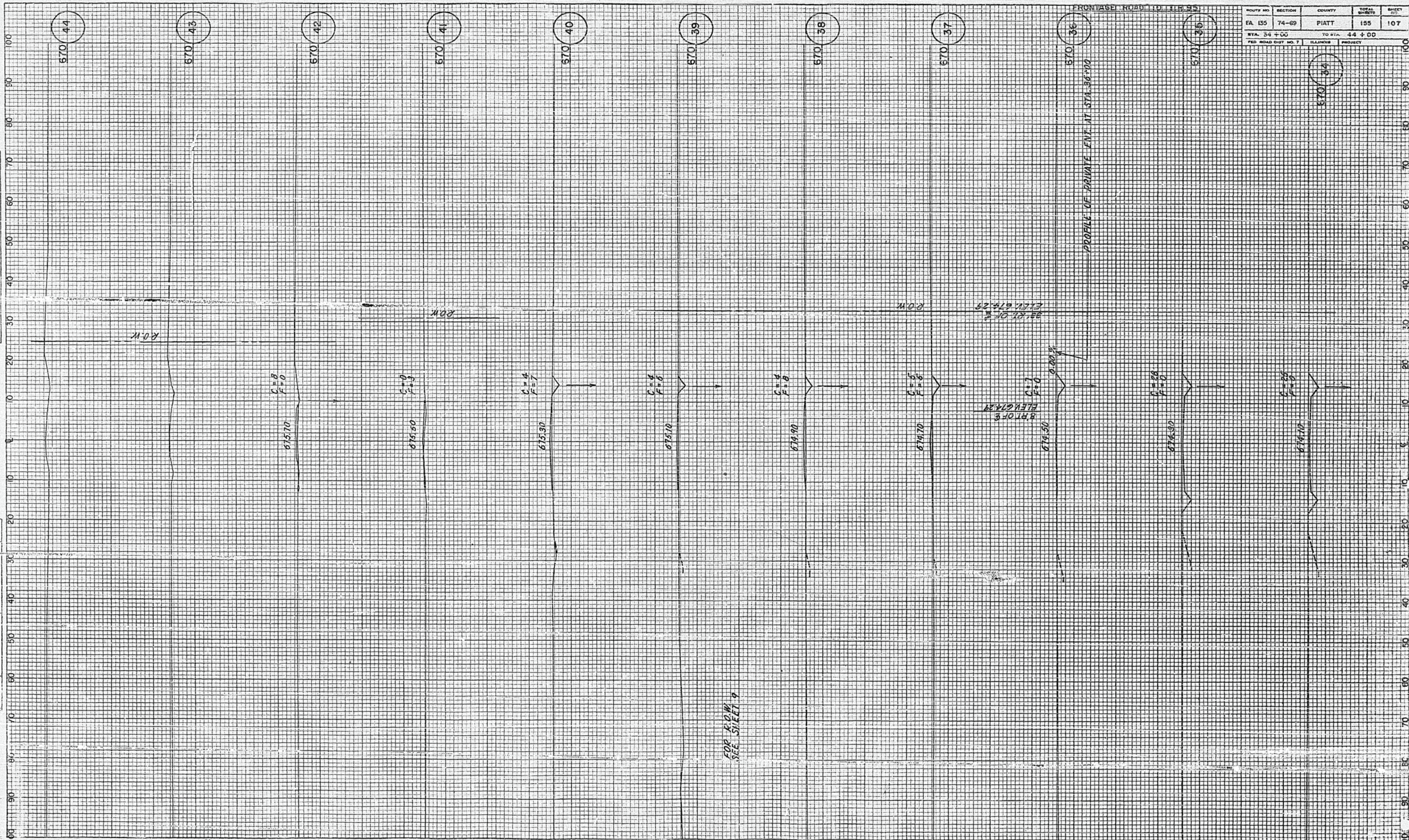


FOR R.O.W.
SEE SHEET 8 AND 9

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2A 155	74-69	PIATT	155	106
STA. 23+00	TO STA. 33+00			
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		

FINAL	BY	DATE
SURVEY		
NOTE BOOK		
NO.		

ORIGINAL	BY	DATE
SURVEY		
NOTE BOOK		
NO.		



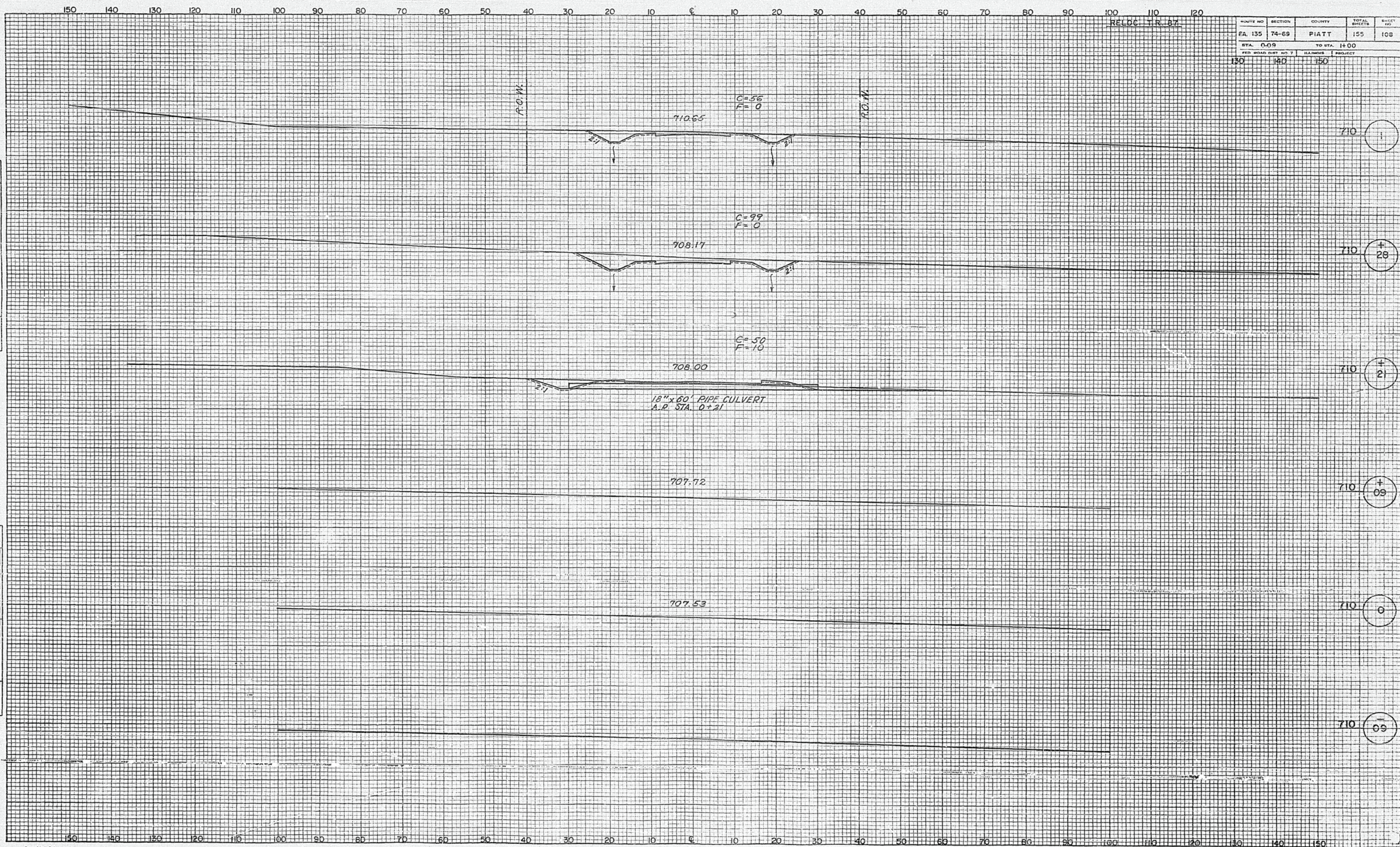
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET
EA 135	74-69	PIATT	155	107
STA. 34+00	TO STA. 44+00	ILLINOIS	PROJECT	
FED. ROAD DIST. NO. 7				

HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

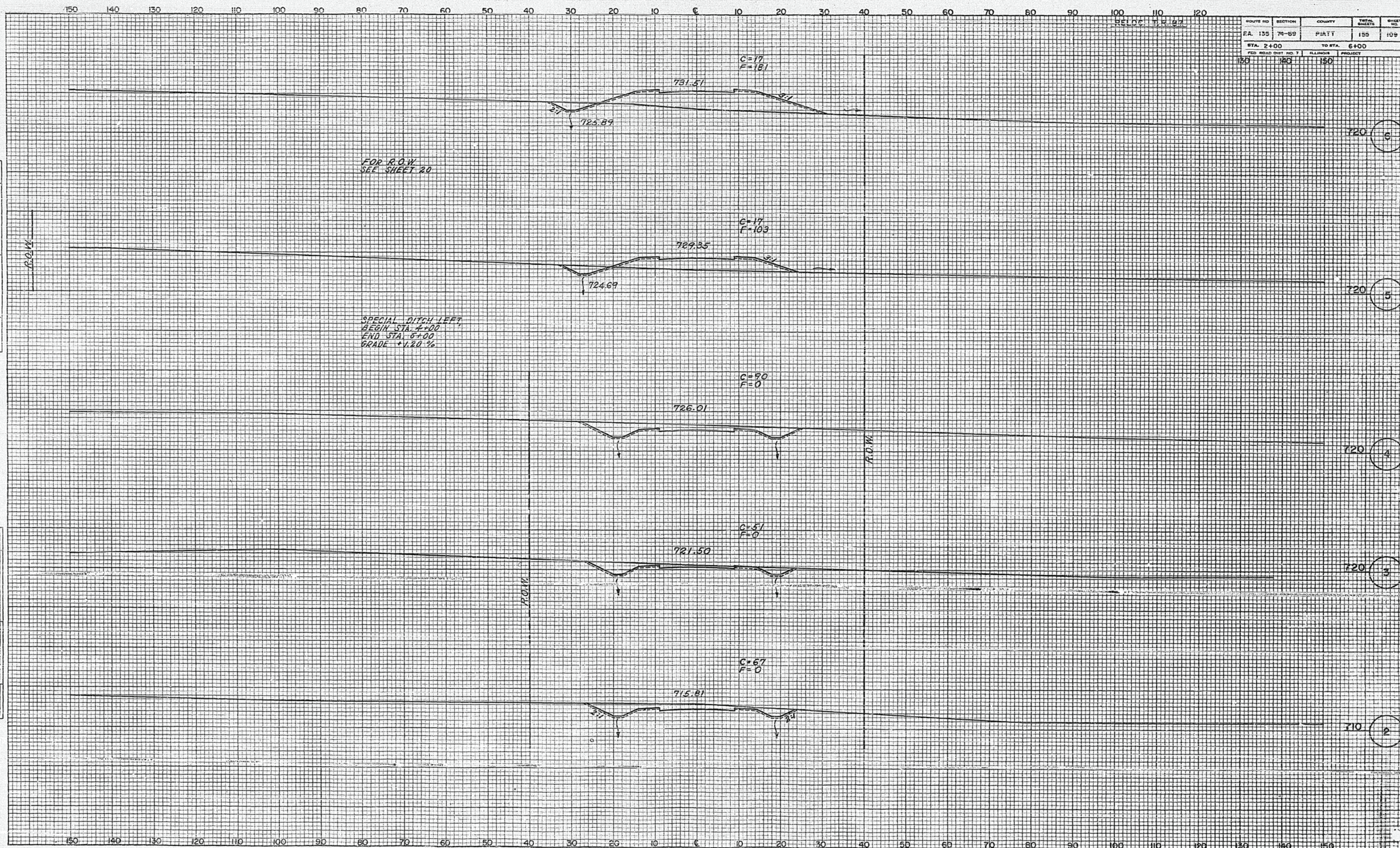
PLATE 3 CROSS SECTION B P R STANDARD
ENGINE DESIGN CO., CHICAGO - ILL. 1934

Printed in U.S.A.
G.W. ABEWEIGHT, Troy, N.Y.

ORIGINAL SURVEY	BY	DATE
NOTE BOOK	SURVEYED	C.C.S.
	PLOTTED	R.H. VERN
	TEMPERATURE	2-21-60
	AREAS	7-12-60
	AREAS CHECKED	8-18-60

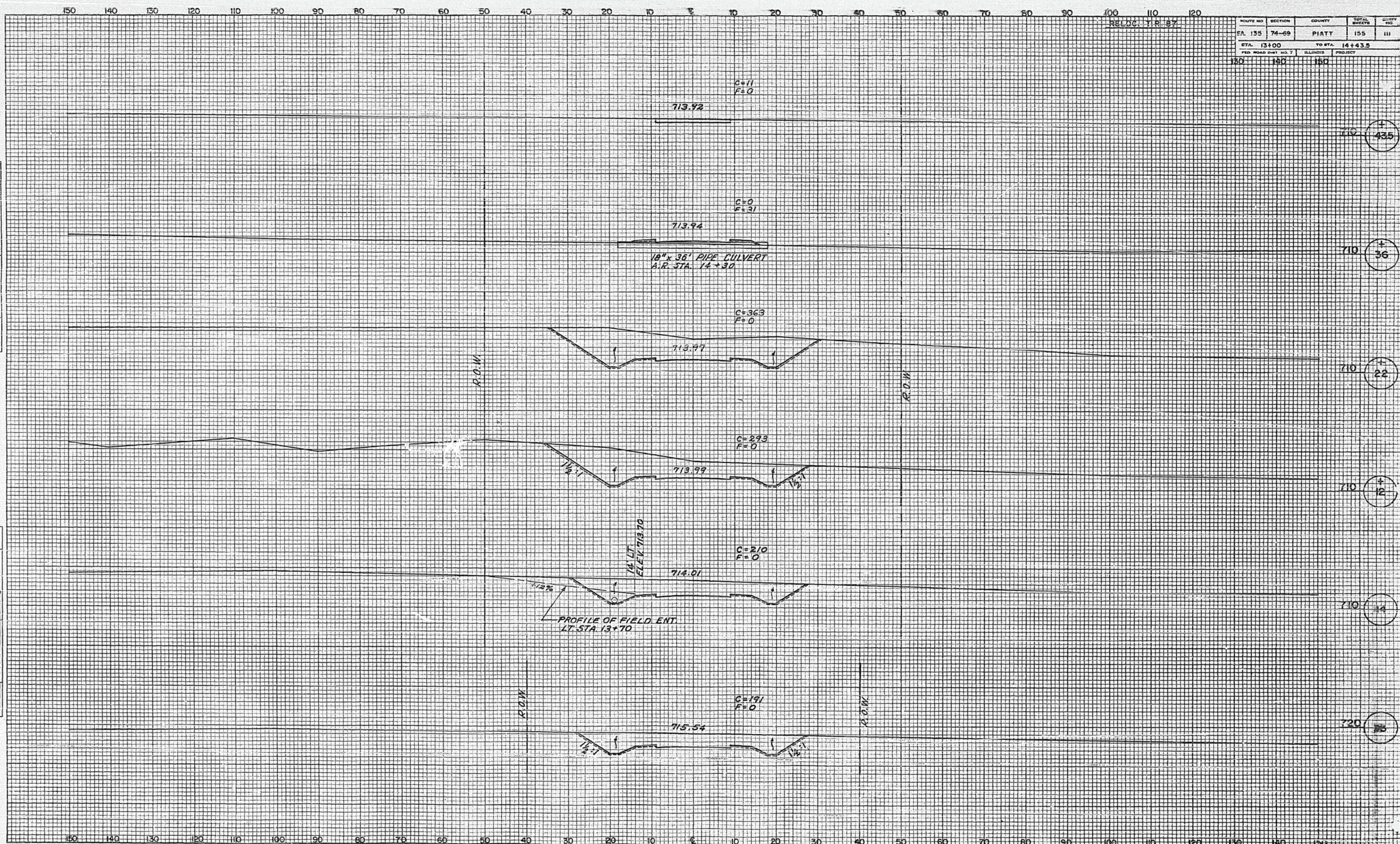


ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED	C.E.S.	2-19-60
H-2-6	TEMPLATE	K.C.M. V.F.R.N. 22166	2-21-60
	AREAS	P.H.	5-24-60
	AREAS CHECKED	P.H.	7-12-60
		A.R.N. 1-6-60	8-19-60



	BY	DATE
SURVEY		
PLOTTED		
TITLE DATE		
A REAS.		
A REAS. CHECKED		

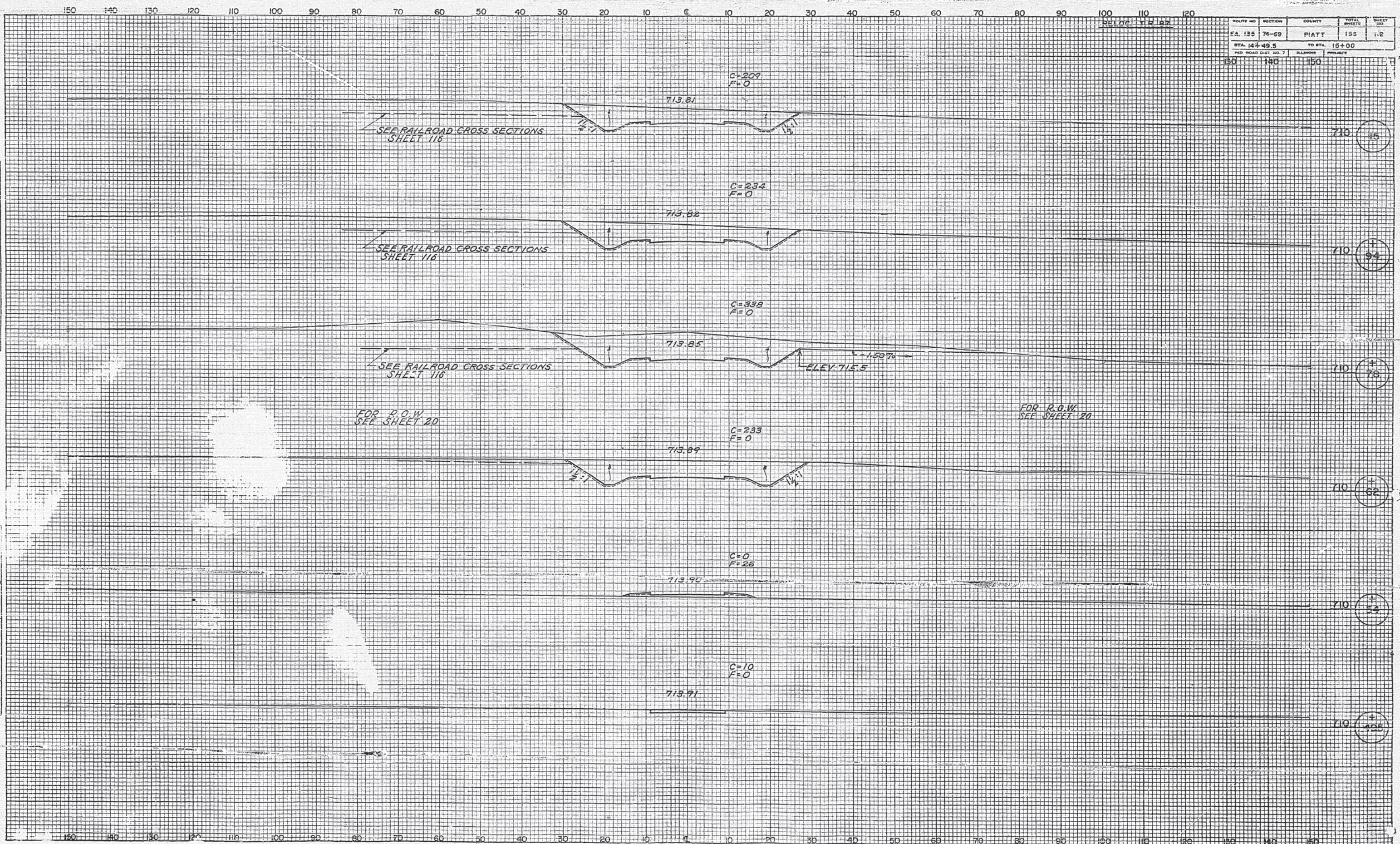
ORIGINAL SURVEY	DATE
NO. 11-2	2-19-60
	2-21-60
	3-21-60
	5-18-60
	7-4-60
	1-11-61



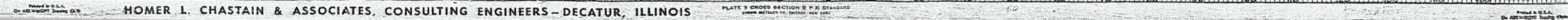
DATE	BY	REVIEWED	DATE
		DATE	
FINAL SURVEY	NOTE BOOK	DATE	
NO.		DATE	

DATE	BY	REVIEWED	DATE
		DATE	
ORIGINAL SURVEY	NOTE BOOK	DATE	
NO.		DATE	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET
EA. 135	74-59	PIATT	155	112
STA. 144+49.5		TO STA. 154+00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		



ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED	C.E.S.	2-17-60
	TERMINAL	P.C.M. V.E.H. 2-21-60	2-21-60
	AREA 1	P.H.	2-22-60
	AREA 2	P.H.	2-22-60
	AREA 3	P.H.	2-22-60
	AREA 4	P.H.	2-22-60
	AREA 5	P.H.	2-22-60
	AREA 6	P.H.	2-22-60
	AREA 7	P.H.	2-22-60
	AREA 8	P.H.	2-22-60
	AREA 9	P.H.	2-22-60
	AREA 10	P.H.	2-22-60
	AREA 11	P.H.	2-22-60
	AREA 12	P.H.	2-22-60
	AREA 13	P.H.	2-22-60
	AREA 14	P.H.	2-22-60
	AREA 15	P.H.	2-22-60
	AREA 16	P.H.	2-22-60
	AREA 17	P.H.	2-22-60
	AREA 18	P.H.	2-22-60
	AREA 19	P.H.	2-22-60
	AREA 20	P.H.	2-22-60
	AREA 21	P.H.	2-22-60
	AREA 22	P.H.	2-22-60
	AREA 23	P.H.	2-22-60
	AREA 24	P.H.	2-22-60
	AREA 25	P.H.	2-22-60
	AREA 26	P.H.	2-22-60
	AREA 27	P.H.	2-22-60
	AREA 28	P.H.	2-22-60
	AREA 29	P.H.	2-22-60
	AREA 30	P.H.	2-22-60
	AREA 31	P.H.	2-22-60
	AREA 32	P.H.	2-22-60
	AREA 33	P.H.	2-22-60
	AREA 34	P.H.	2-22-60
	AREA 35	P.H.	2-22-60
	AREA 36	P.H.	2-22-60
	AREA 37	P.H.	2-22-60
	AREA 38	P.H.	2-22-60
	AREA 39	P.H.	2-22-60
	AREA 40	P.H.	2-22-60
	AREA 41	P.H.	2-22-60
	AREA 42	P.H.	2-22-60
	AREA 43	P.H.	2-22-60
	AREA 44	P.H.	2-22-60
	AREA 45	P.H.	2-22-60
	AREA 46	P.H.	2-22-60
	AREA 47	P.H.	2-22-60
	AREA 48	P.H.	2-22-60
	AREA 49	P.H.	2-22-60
	AREA 50	P.H.	2-22-60
	AREA 51	P.H.	2-22-60
	AREA 52	P.H.	2-22-60
	AREA 53	P.H.	2-22-60
	AREA 54	P.H.	2-22-60
	AREA 55	P.H.	2-22-60
	AREA 56	P.H.	2-22-60
	AREA 57	P.H.	2-22-60
	AREA 58	P.H.	2-22-60
	AREA 59	P.H.	2-22-60
	AREA 60	P.H.	2-22-60
	AREA 61	P.H.	2-22-60
	AREA 62	P.H.	2-22-60
	AREA 63	P.H.	2-22-60
	AREA 64	P.H.	2-22-60
	AREA 65	P.H.	2-22-60
	AREA 66	P.H.	2-22-60
	AREA 67	P.H.	2-22-60
	AREA 68	P.H.	2-22-60
	AREA 69	P.H.	2-22-60
	AREA 70	P.H.	2-22-60
	AREA 71	P.H.	2-22-60
	AREA 72	P.H.	2-22-60
	AREA 73	P.H.	2-22-60
	AREA 74	P.H.	2-22-60
	AREA 75	P.H.	2-22-60
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	AREA 77	P.H.	2-22-60
	AREA 78	P.H.	2-22-60
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	AREA 80	P.H.	2-22-60
	AREA 81	P.H.	2-22-60
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	AREA 83	P.H.	2-22-60
	AREA 84	P.H.	2-22-60
	AREA 85	P.H.	2-22-60
	AREA 86	P.H.	2-22-60
	AREA 87	P.H.	2-22-60
	AREA 88	P.H.	2-22-60
	AREA 89	P.H.	2-22-60
	AREA 90	P.H.	2-22-60
	AREA 91	P.H.	2-22-60
	AREA 92	P.H.	2-22-60
	AREA 93	P.H.	2-22-60
	AREA 94	P.H.	2-22-60



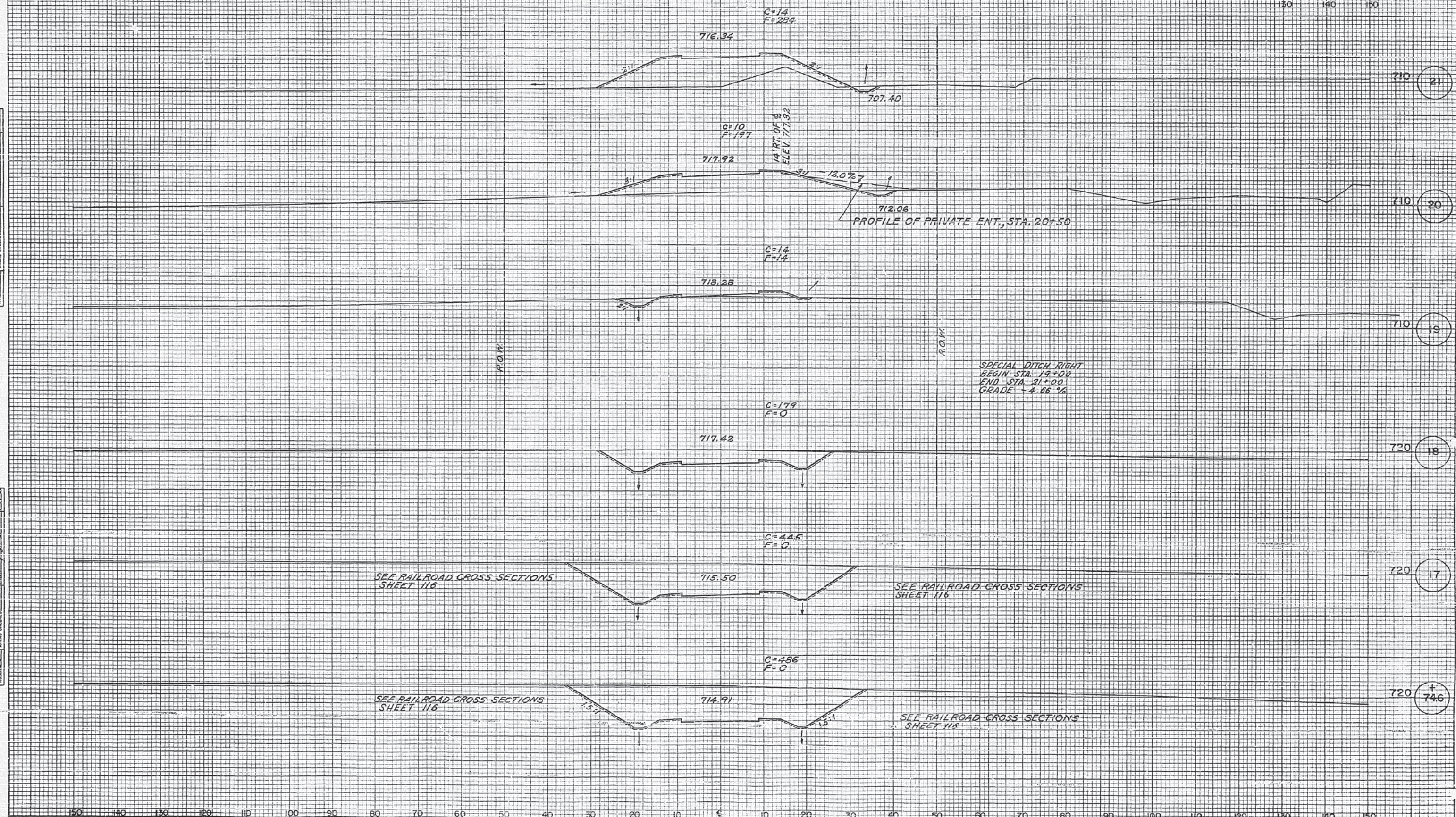
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RELOC. Y.R. 87

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA 135	74-69	PIATT	155	114
STA. 16+74.60		TO STA. 21+00		
FED. ROAD DIST. NO. 7		PROJECT		

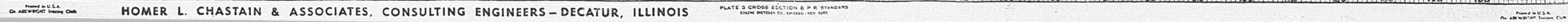
DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	2-17-60
BY	C. L. S.
ORIGINAL SURVEY	
NOTE BOOK	
NO.	112-2



		BY	DATE
FINAL SURVEY NOTE BOOK	SURVEYED PLOTTED		
	TEMPLATE		
	AREAS		
	AREAS CHECKED		

ORIGINAL SURVEY		SURVEYED	C.E.S.	DATE	2-7-60
NOTE BOOK		PLOTTED	K.C.M.		2-21-60
H22-6		TEMPLATE	A.F.H.R.		2-27-60
		AREAS	L.N.		5-8-60
		AREAS CHECKED	R.L.N. J.G.A.D.		7-17-60



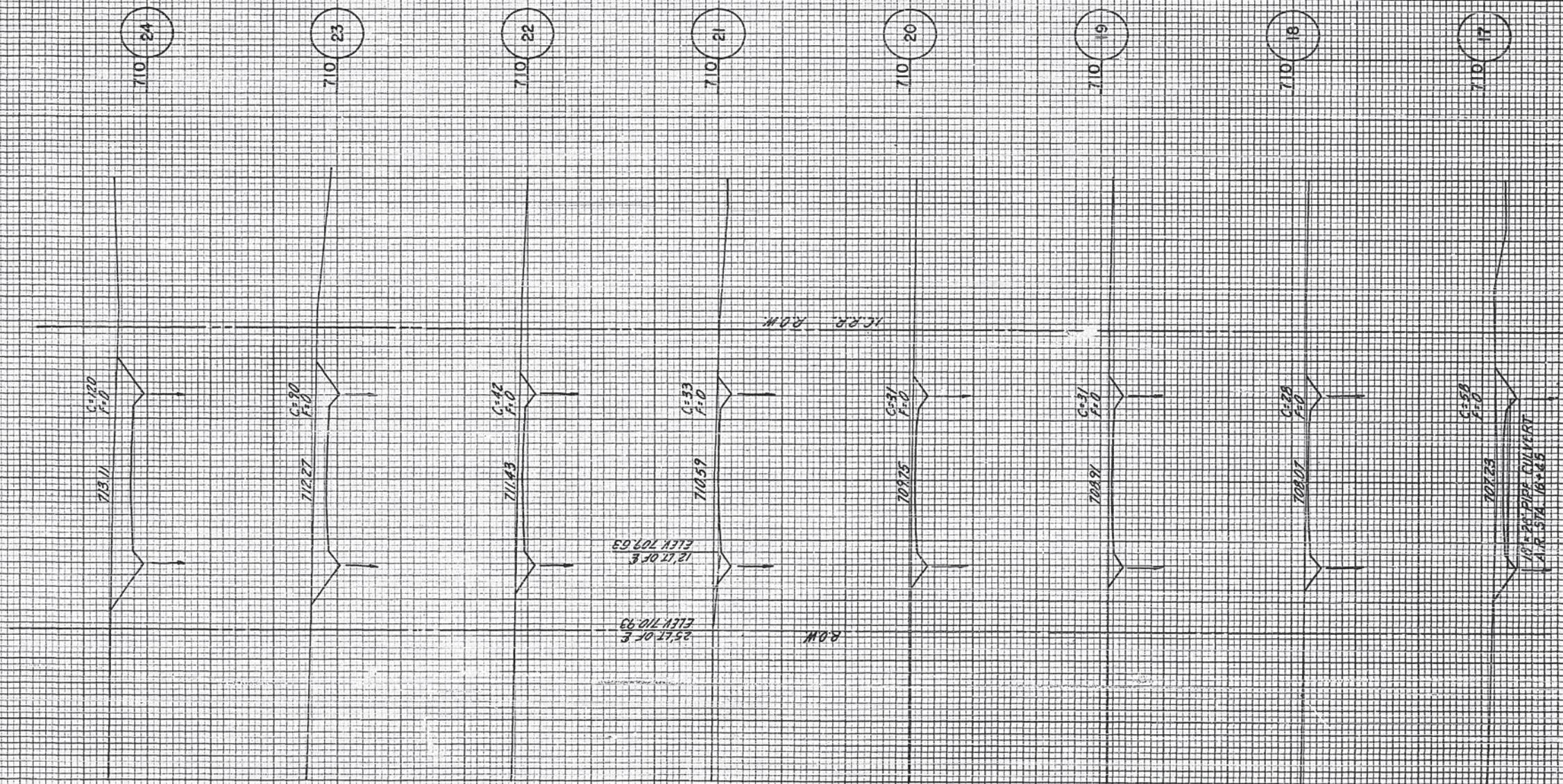
ORIGINAL SURVEY	DATE
SURVEYED	5-23-66
PLOTTED	5-30-66
TEMPERATURE	6-7-66
AREAS	7-13-66
AREAS CHECKED	7-13-66



FINAL SURVEY	BY	DATE
NO. 1		
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NO. 100		

ORIGINAL SURVEY	BY	DATE
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NO. 97		
NO. 98		
NO. 99		
NO. 100		

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA. 135	74-69	PIATT	155	117
STA. 17+00		TO STA. 24+00		
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		
GRESAP PATTON FR. RD.				



Printed in U.S.A.
On ARCWIGHT Tracing Cloth

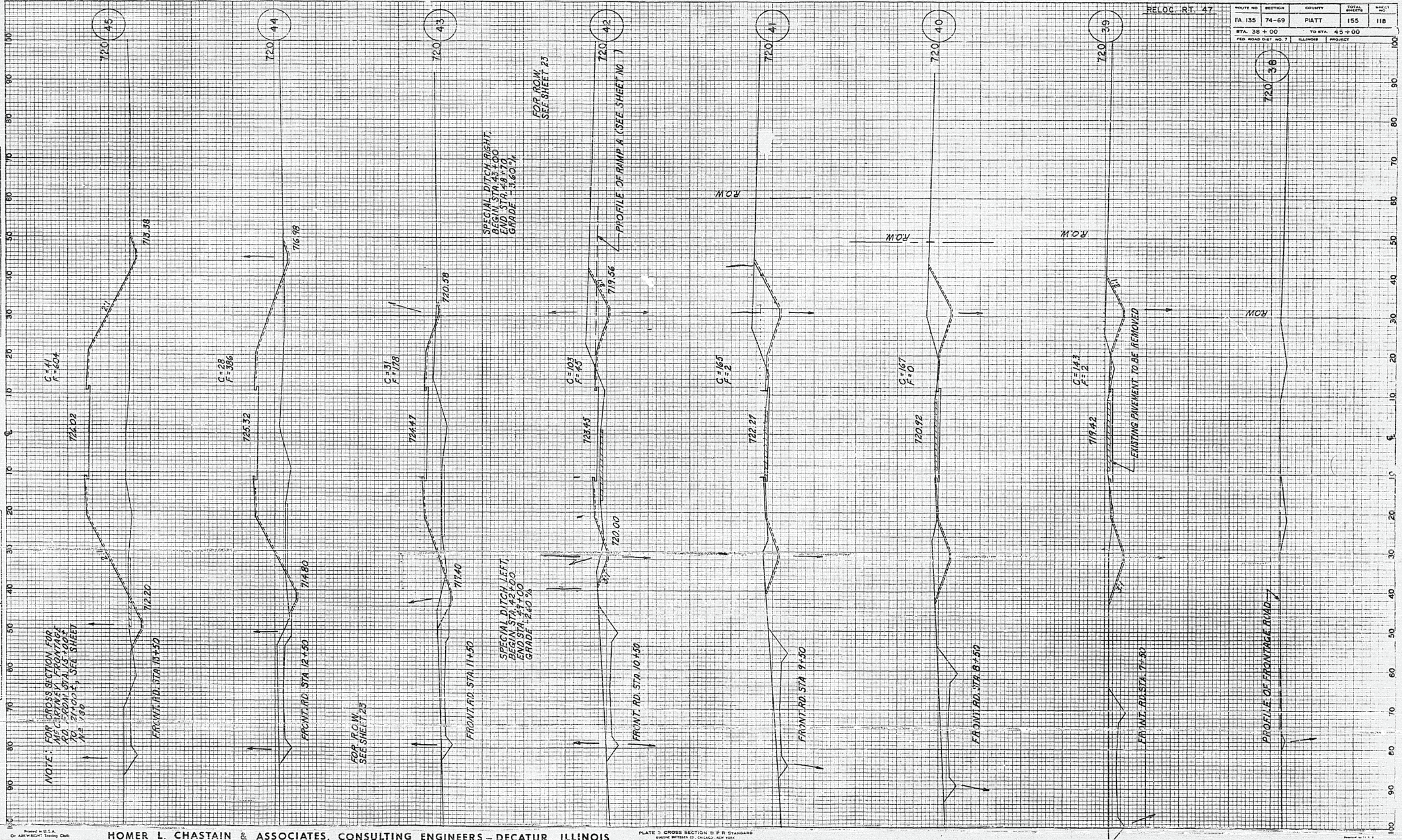
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION B P R STANDARD
EUGENE DETZEN CO. CHICAGO - NEW YORK

Printed in U.S.A.

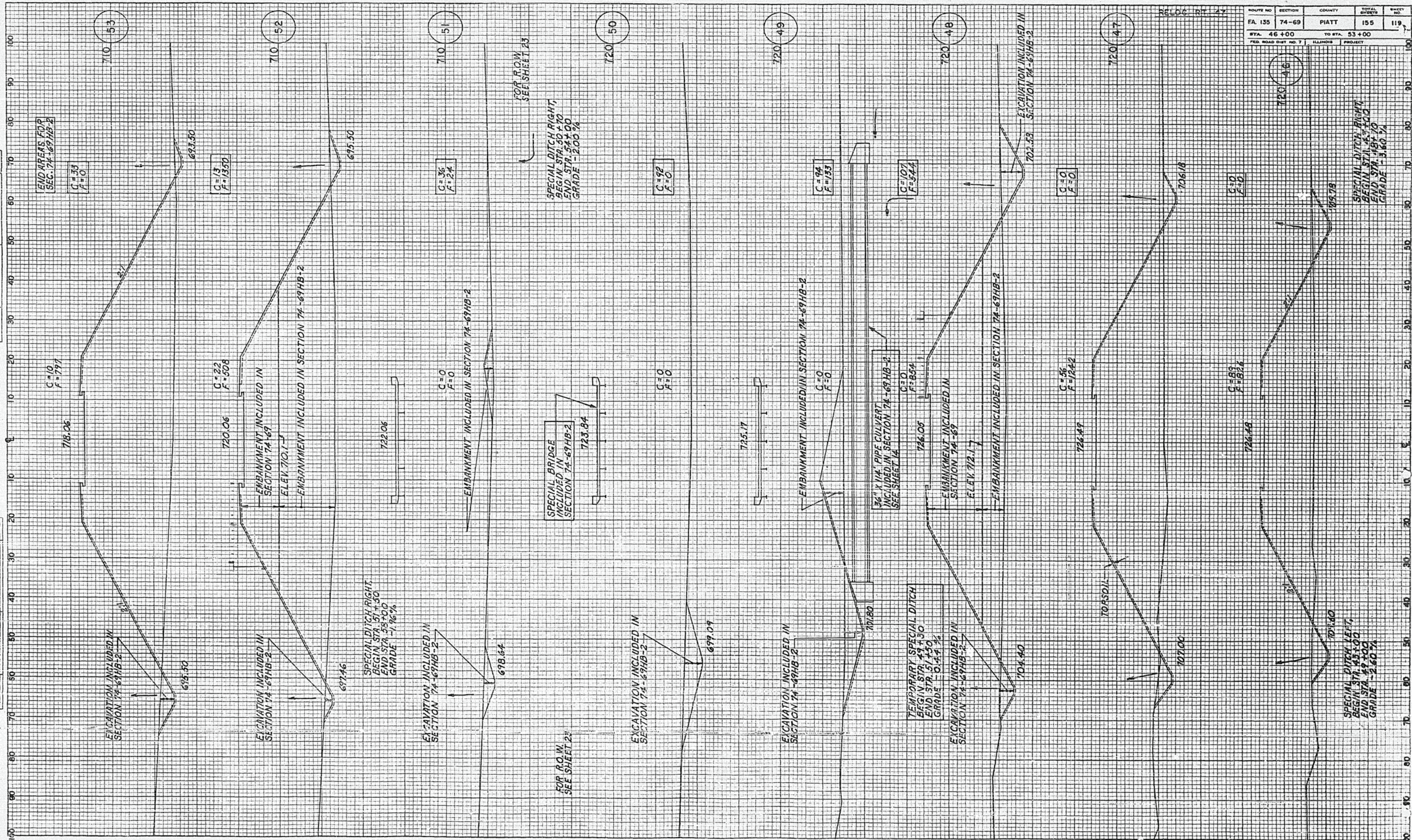
ORIGINAL SURVEY	SURVIVED	BY	DATE
NOTE BOOK	FLIPPED	W.P.	7-2-60
	TEMPLATE	S.P.H. (C.E.H. L.H. 10)	11/10/60
	AREAS	1/2	7-4-60
H-2-5	AREAS CHECKED	ARM	7-12-60

NOTE: FOR CROSS SECTION FOR
MCC RTNRY FRONTAGE
RD. 5-RDN STA 15+00±
TO 21+00±, SEE SHEET
N.º 130



ROUTE NO	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
FA. 135	74-69	PIATT	155	118
STA. 38 + 00		TO STA. 45 + 00		
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		

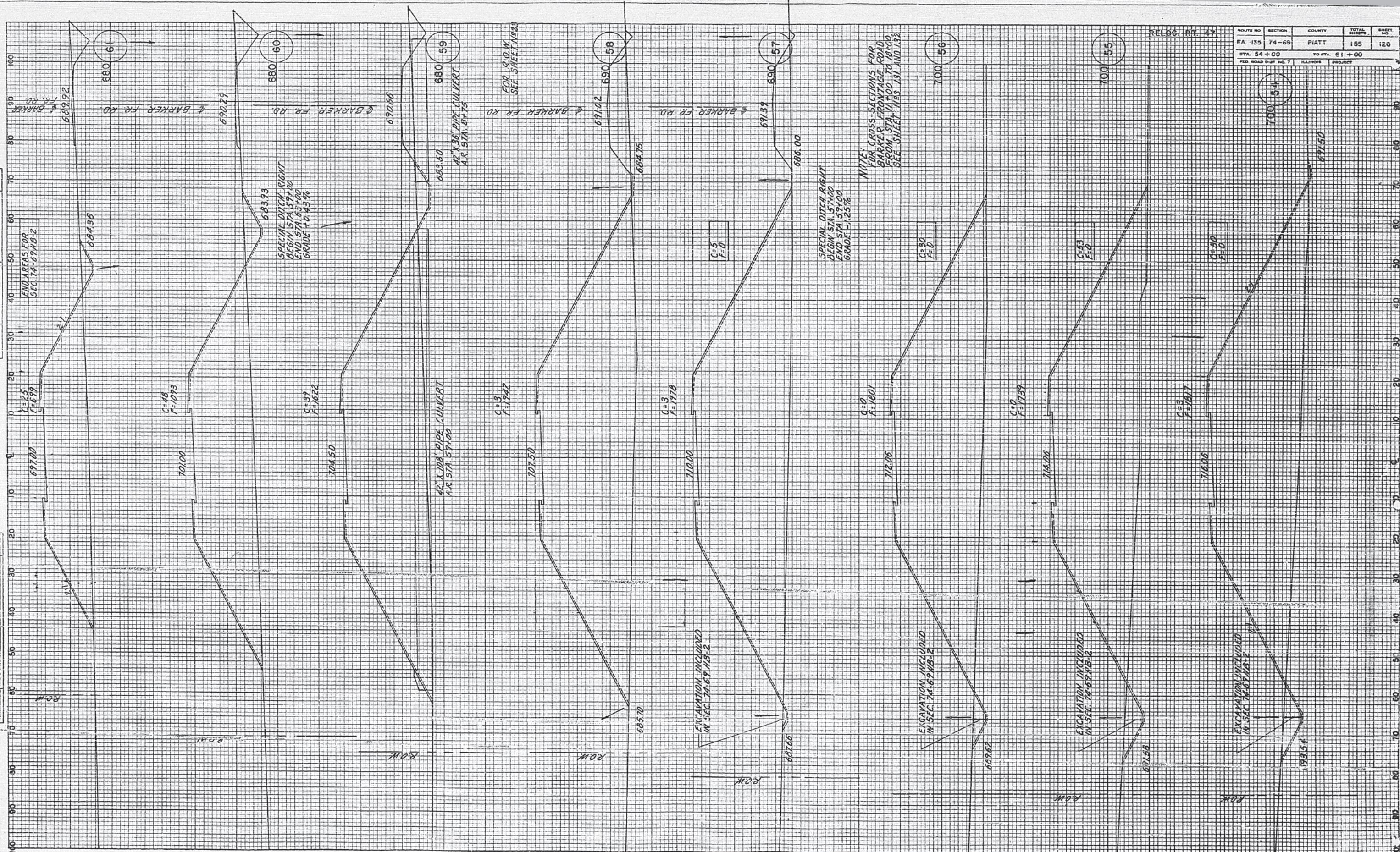
ORIGINAL SURVEY	BY	DATE
SUNNYT	SFA	7-12-60
PLOTTED	PLN	7-12-60
TEMPLE	PLN	7-12-60
AREAS	PLN	7-12-60
AREAS CHECKED	PLN	7-12-60



ROUTE NO	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-69	PIATT	155	119
STA. 46+00 TO STA. 53+00				
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		

FINAL SURVEY	DATE
NO. 1	BY
NO. 2	BY
NO. 3	BY
NO. 4	BY
NO. 5	BY
NO. 6	BY
NO. 7	BY
NO. 8	BY
NO. 9	BY
NO. 10	BY

ORIGINAL SURVEY	DATE
NO. 1	BY
NO. 2	BY
NO. 3	BY
NO. 4	BY
NO. 5	BY
NO. 6	BY
NO. 7	BY
NO. 8	BY
NO. 9	BY
NO. 10	BY



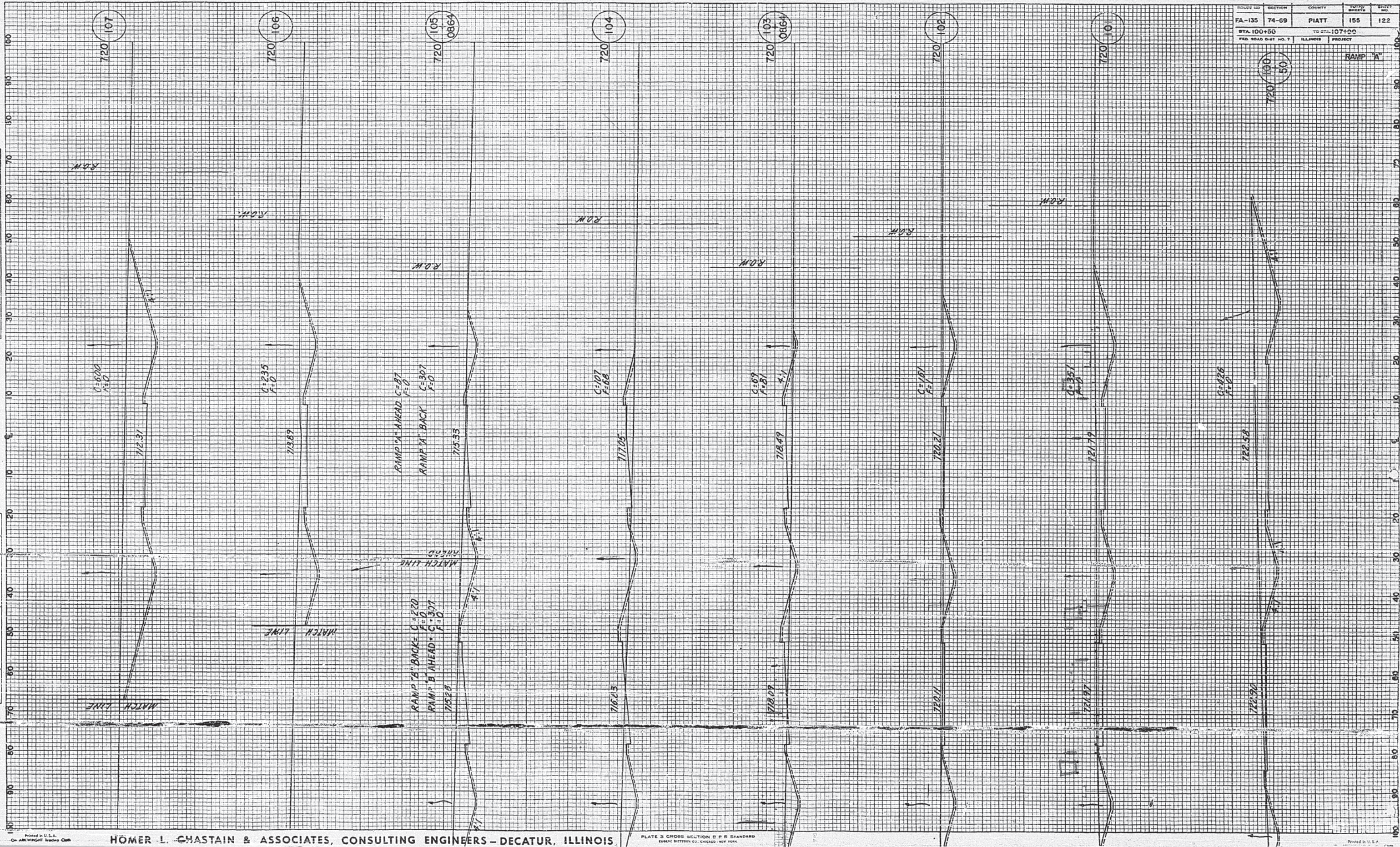
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-69	PIATT	155	120
STA. 54+00	TO STA. 61+00	ILLINOIS	PROJECT	

ORIGINAL	BY	DATE
SURVEY	GLS	2-2-60
NOTE BOOK	SLM P. L. H.	2-2-60
	PLAN P. L. H.	2-2-60
	T.L.C.	2-2-60
	AREAS	2-2-60
	AREAS CHANGED	2-2-60



DATE	BY	FINAL SURVEY	SURVEY	PLOTTED	NOTE BOOK	AREAS CHECKED

DATE	BY	ORIGINAL SURVEY	SURVEY	PLOTTED	NOTE BOOK	AREAS CHECKED
4-20-54		C.S.				
6-10-56		F.C.				
7-25-56		L.C.				
7-15-56		L.C.				



ROUTE NO.	SECTION	COUNTY	LOT/BLK	SHEET NO.
FA-135	74-69	PIATT	155	122

STA. 100+50	TO STA. 107+00
FED. ROAD DIST. NO. 7	ILLINOIS PROJECT

HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

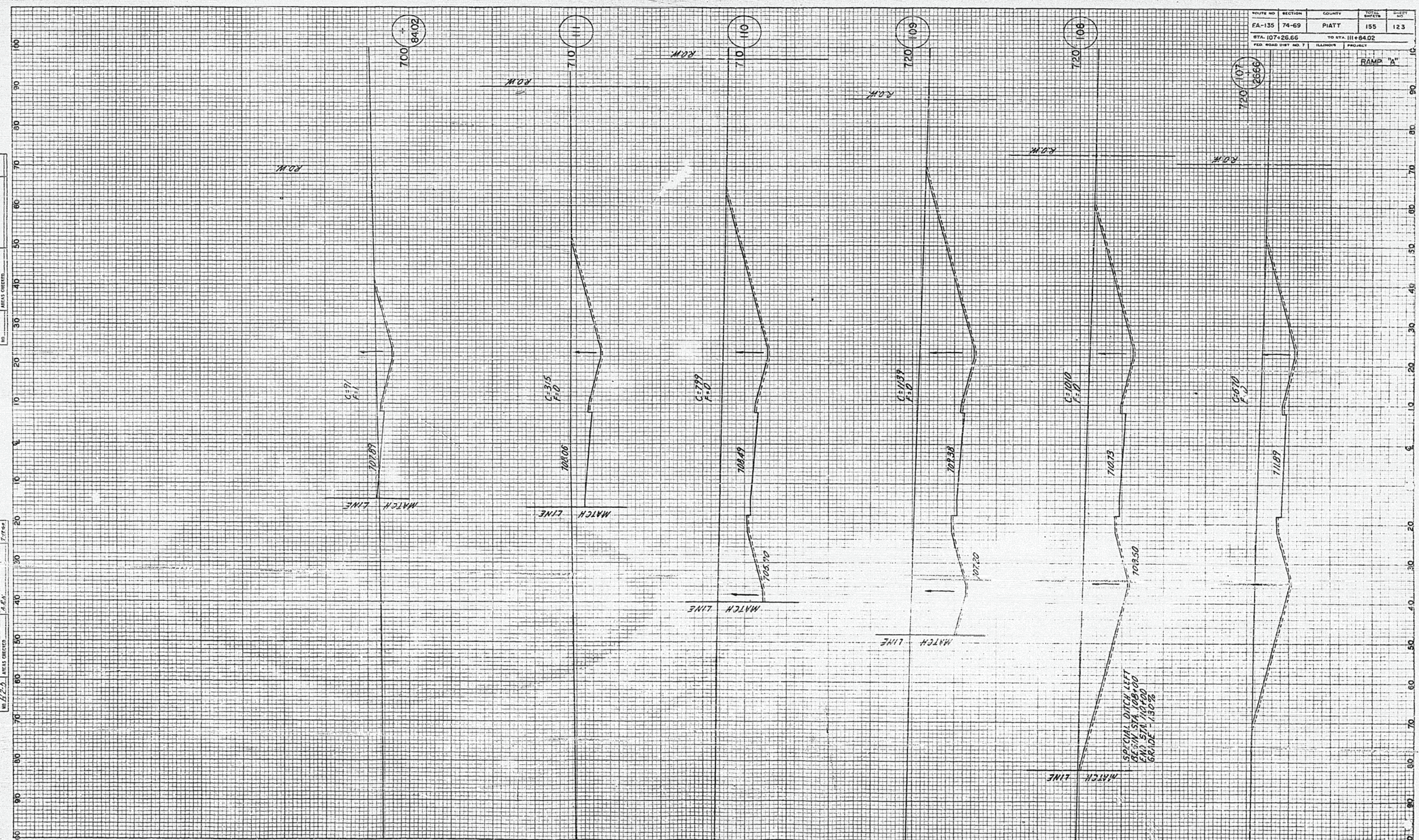
PLATE 3 CROSS SECTION B P R STANDARD
ENGINEERING CO. CHICAGO - NEW YORK

FINAL	REVISION	DATE	BY	CHK
SURVEY	PROPOSED			
NOTE BOOK	TEMPLATE			
NO.	AREAS CHECKED			

ORIGINAL	REVISION	DATE	BY	CHK
SURVEY	PROPOSED			
NOTE BOOK	TEMPLATE			
NO. H2-5	AREAS CHECKED			

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	123
STA. 107+26.66		TO STA. 111+84.02		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		

RAMP "A"



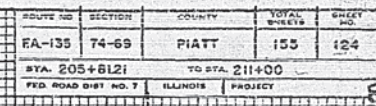
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION B P R STANDARD
ELEVATION SECTION 20, CHICAGO - NEW YORK

Printed in U.S.A.
C. W. WRIGHT, Printing Co.

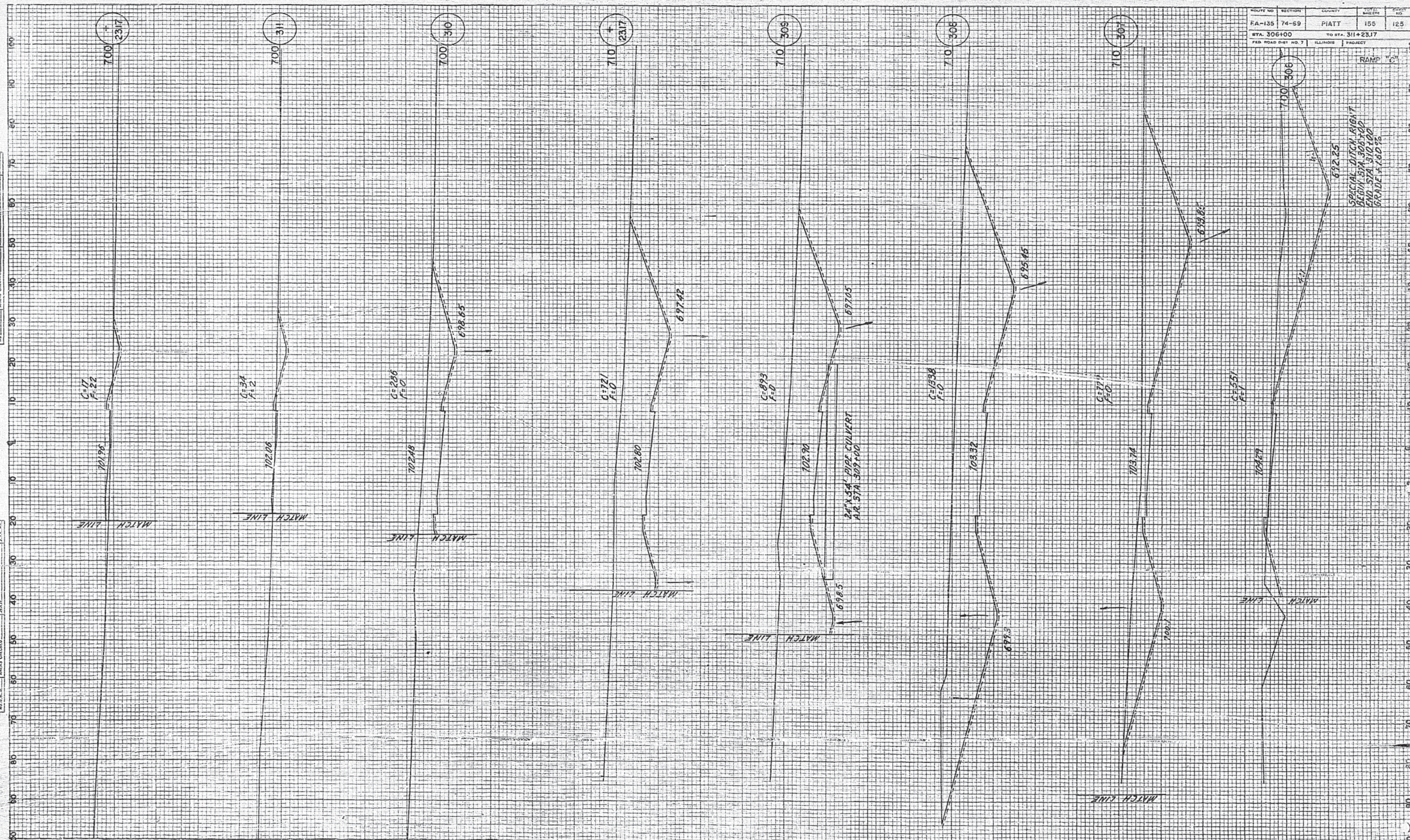
ORIGINAL SURVEY	SURVETED	BY	DATE
	PLOTTED	C.S.	8-29-60
NOTE: BOOK	TITLE	A.C.V.	7-1-60
	AREAS	A.C.V.	7-1-60
	AREAS CHANGED	A.C.V.	7-1-60

L.E.H. 7-1-60



FINAL SURVEY	SURVEYED	S'.	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	K&EAS.		
HQ	A&EAS CHECKED		

ORIGINAL SURVEY	BY	DATE
WHITE ROCK	G.S.	6-10-60
	APN	7-1-60
	A.M.	7-1-60
	B.	7-6-60
NO. 22	APN	7-1-60



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA-135	74-69	PIATT	155	126
STA. 405+81.21 TO STA. 408+00				
FED. ROAD DIST. NO. 2 ILLINOIS PROJECT				

30 140 150 RAMP D

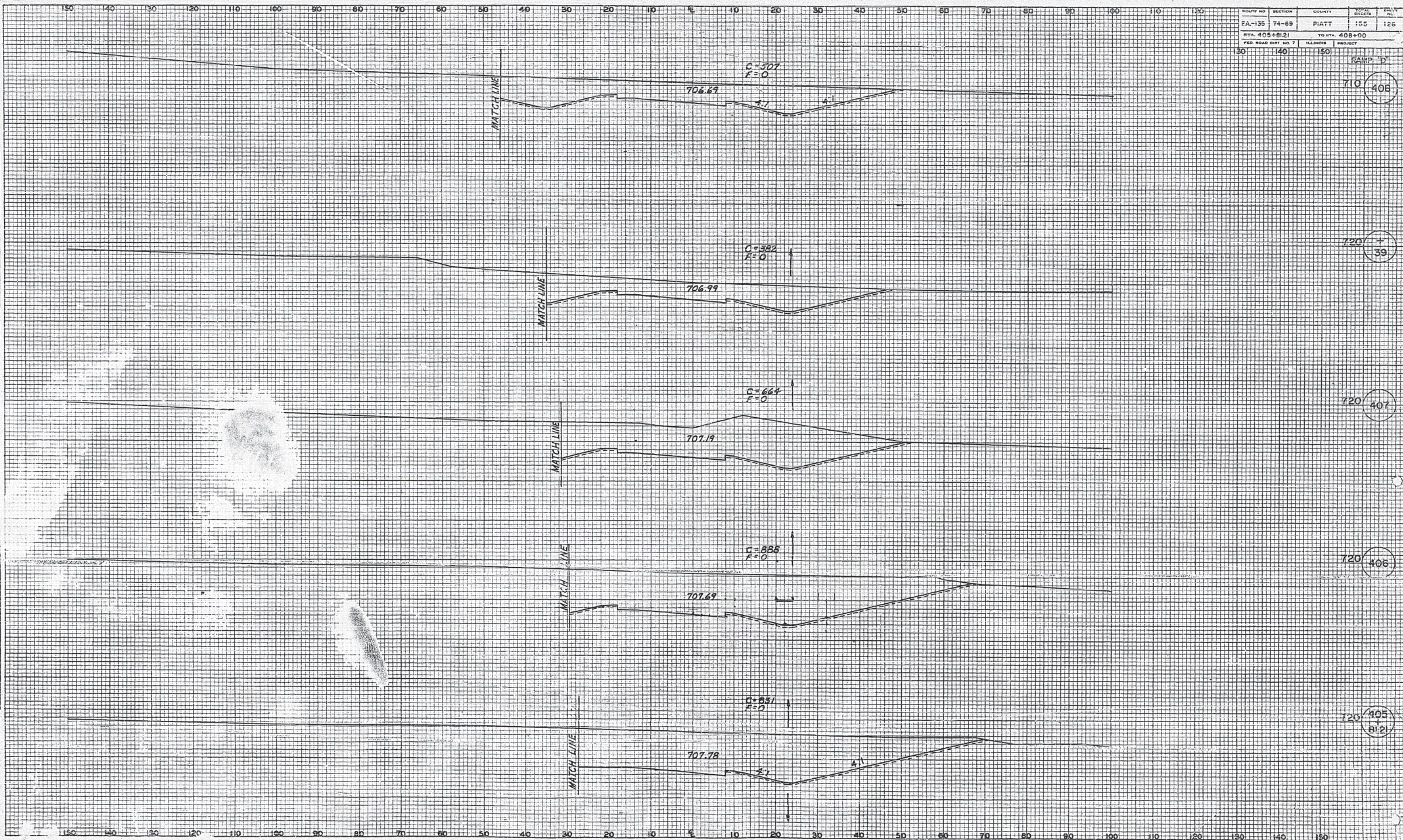
710 406

720 39

720 407

720 406

720 405
81.21



HOMER L. CHAMBERLAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION D P.R. STANDARD
EUGENE DITTMER CO. CHICAGO - NEW YORK

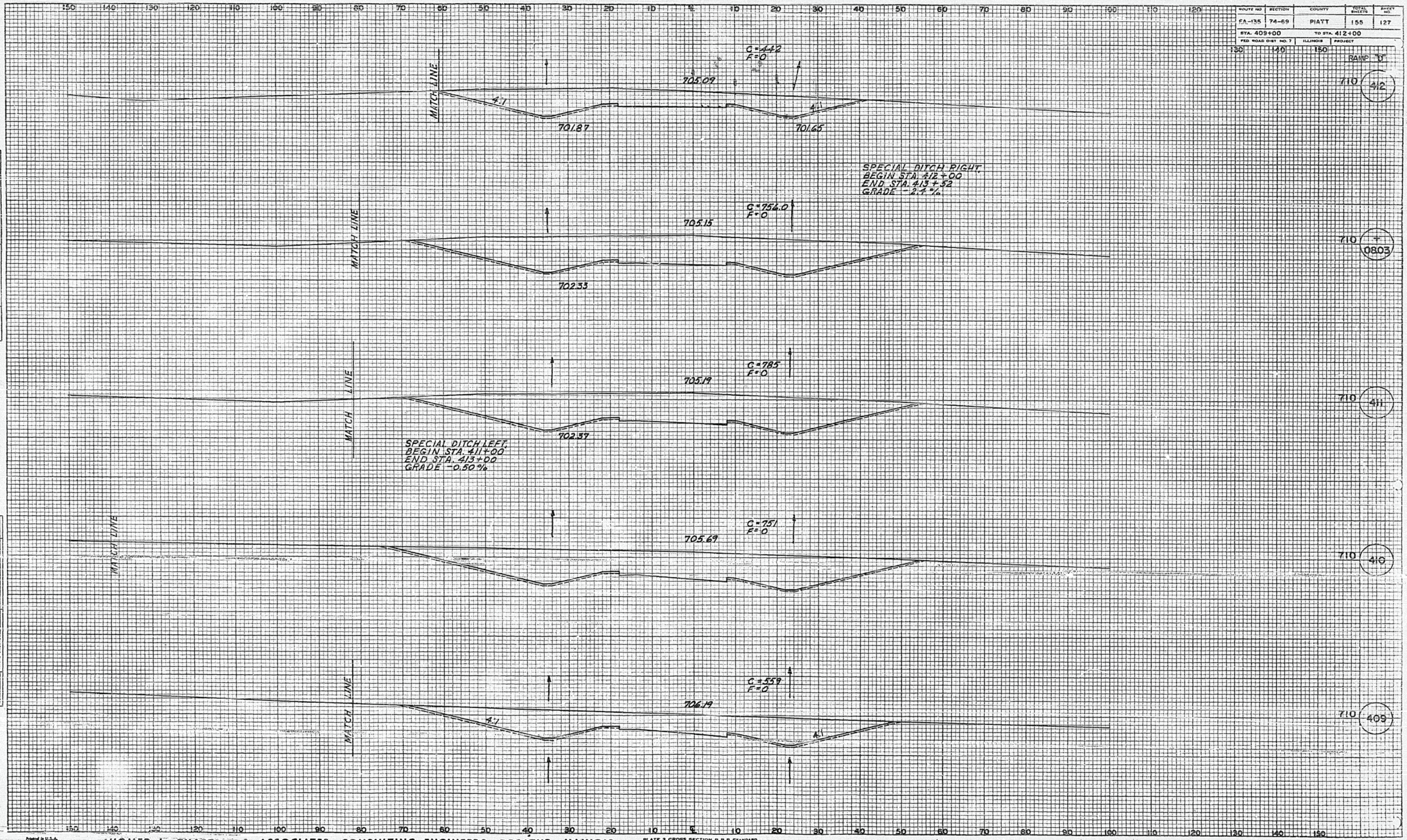
Printed in U.S.A.

FINAL SURVEY	DATE
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	
NO. 6	
NO. 7	
NO. 8	
NO. 9	
NO. 10	

ORIGINAL SURVEY	DATE
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	
NO. 6	
NO. 7	
NO. 8	
NO. 9	
NO. 10	

DATE	
BY	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	6-18-99
BY	W. J. S.
ORIGINAL SURVEY	W. J. S.
NOTE BOOK	W. J. S.
NO.	W. J. S.



ROUTE NO.	74-69
SECTION	PIATT
COUNTY	PIATT
TOTAL SHEETS	155
SHEET NO.	127
STA. 409+00	TO STA. 412+00
FED. ROAD DIST. NO. 7	ILLINOIS
PROJECT	

RAMP "D"

710 412

710 0803

710 411

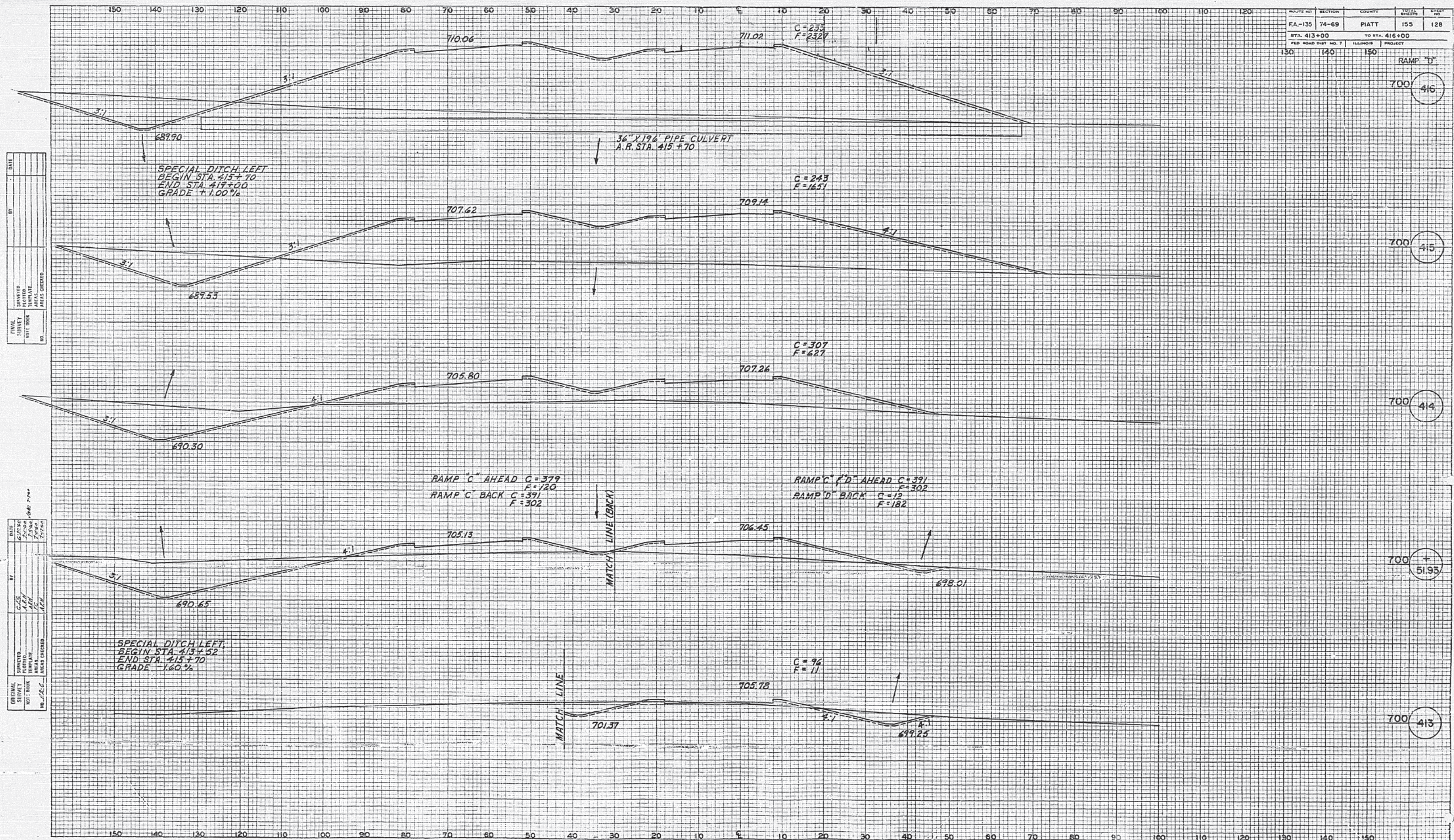
710 410

710 409

DATE	
BY	
FINAL SURVEY	
NOTED BOOK	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
NOTED BOOK	
NO.	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-69	PIATT	155	128
STA. 413+00		TO STA. 416+00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		



PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA-135	74-59	PIATT	155	129
STA. 417+00			TO STA. 419+83.25	
FED. ROAD DIST. NO. 7			ILLINOIS PROJECT	
130	140	150	RAMP "D"	

SEE RELOCATION RT. 47 CROSS SECTIONS

700 + 83.25

700 + 419

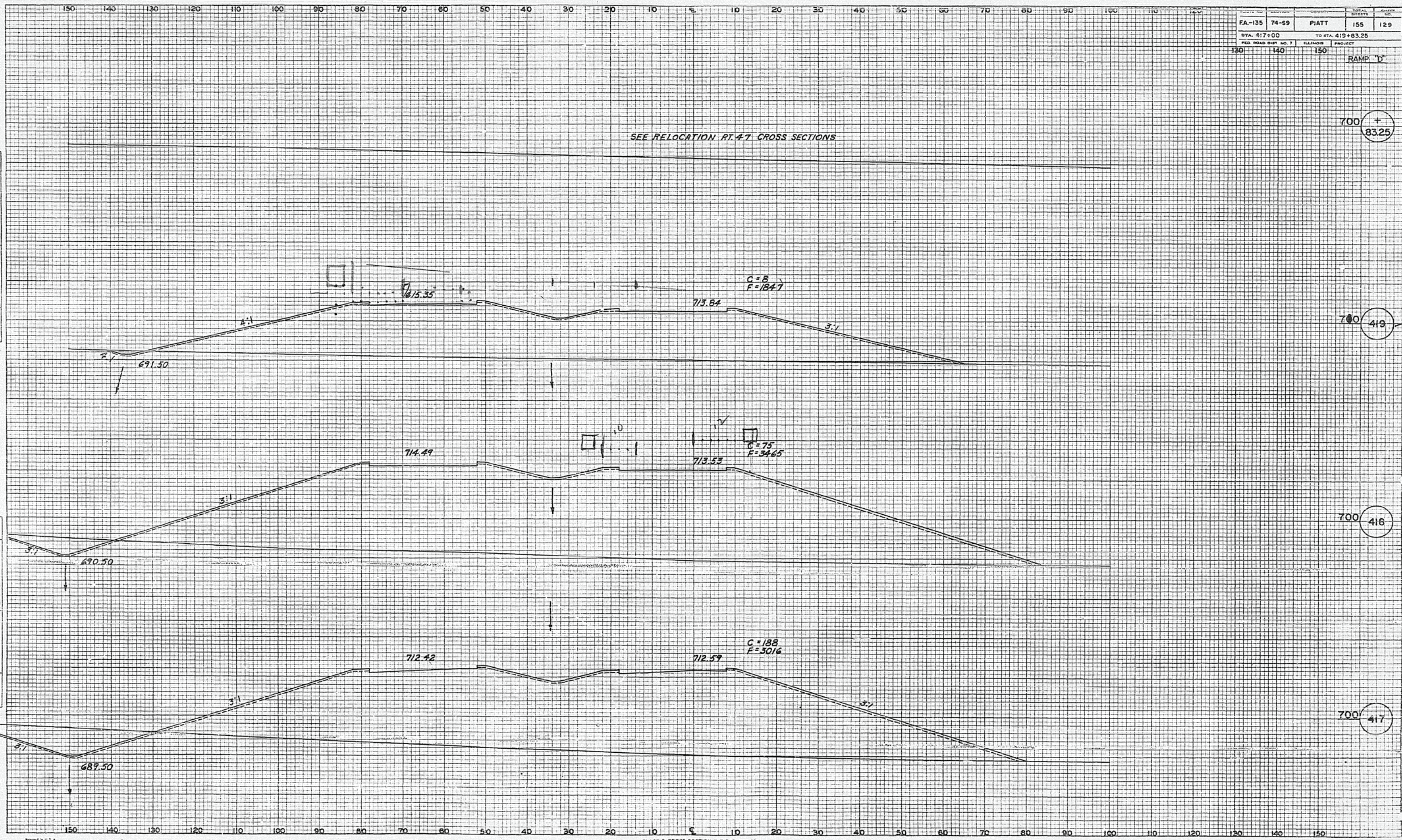
700 + 418

700 + 417

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
NO	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
	PLOTTED	C.S.	6-30-60
	TEMPLATE	APN	1-1-60
	AREAS	APN	1-25-60
	AREAS CHECKED	LG	1-30-60
		APN	7-15-60

ORIGINAL SURVEY	NOTE BOOK	NO H2-5
--------------------	-----------	---------

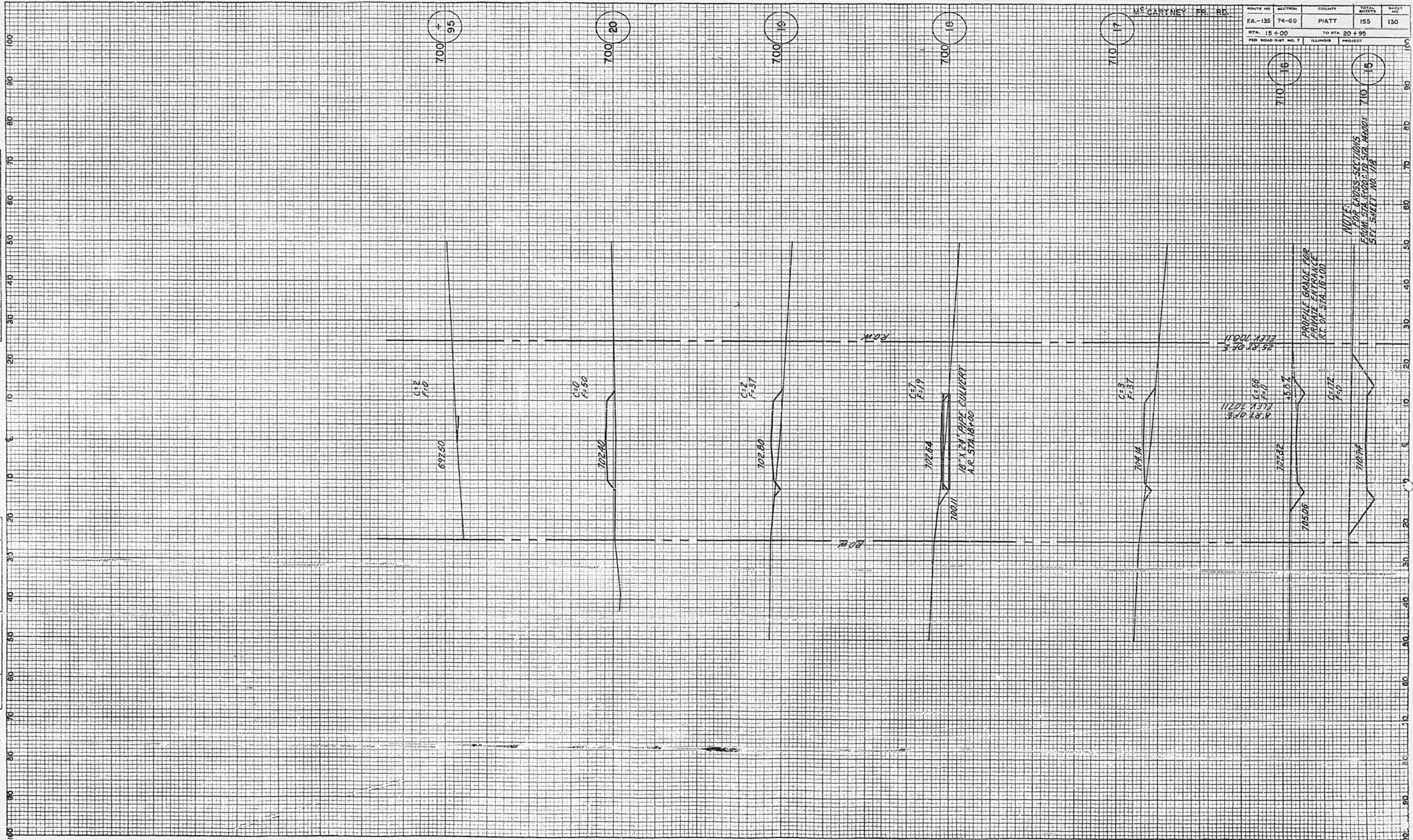


DATE	BY	FINAL SURVEY	SURVEYED
		PLOTTED	PLOTTED
		NOTE BOOK	NOTE BOOK
		AREAS CHECKED	AREAS CHECKED

WAC 6-18-60

DATE	BY	C.E.S.	DATE	BY	C.E.S.
5-7-60		AKN	5-7-60		AKN
7-1-60		AKN	7-1-60		AKN
7-12-60		AKN	7-12-60		AKN

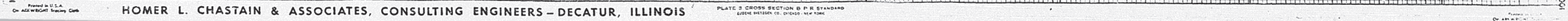
DATE	BY	C.E.S.	DATE	BY	C.E.S.
5-7-60		AKN	5-7-60		AKN
7-1-60		AKN	7-1-60		AKN
7-12-60		AKN	7-12-60		AKN



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA-135	74-69	PIATT	155	130
STA. 15+00		TO STA. 20+95		
FED. ROAD DIST. NO. 7		ILLINOIS		PROJECT

NOTE:
FOR CROSS-SECTIONS
FROM STA. 15+00 TO STA. 18+00
SEE SHEET NO. 118

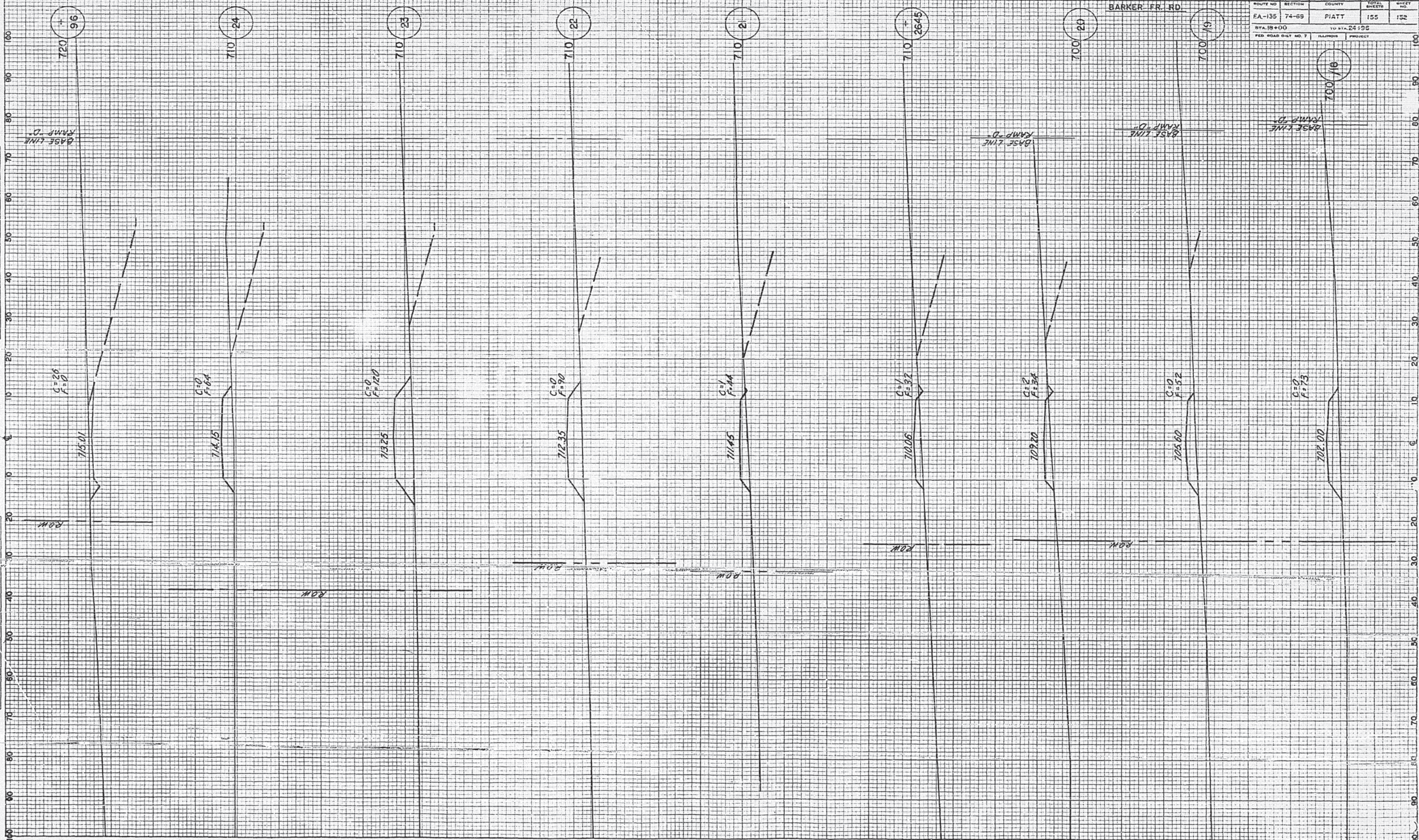
DATE	6-25-60
SURVEYED	R.H.
FLOTTED	A.R.N.
TEMPLATE	TTC
AREAS	7-11-60
AREAS CHECKED	7-12-60
NO. H-2-5	
NOTE BOOK	
SURVEY	
JOB NAME	



FINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
TEMPERATURE	
WIND	
MO.	

ORIGINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
TEMPERATURE	
WIND	
MO.	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA-135	74-69	PIATT	155	152
STA. 18+00		TO STA. 24+00		
FED. ROAD DIST. NO. 7		ILLINOIS PROJECT		



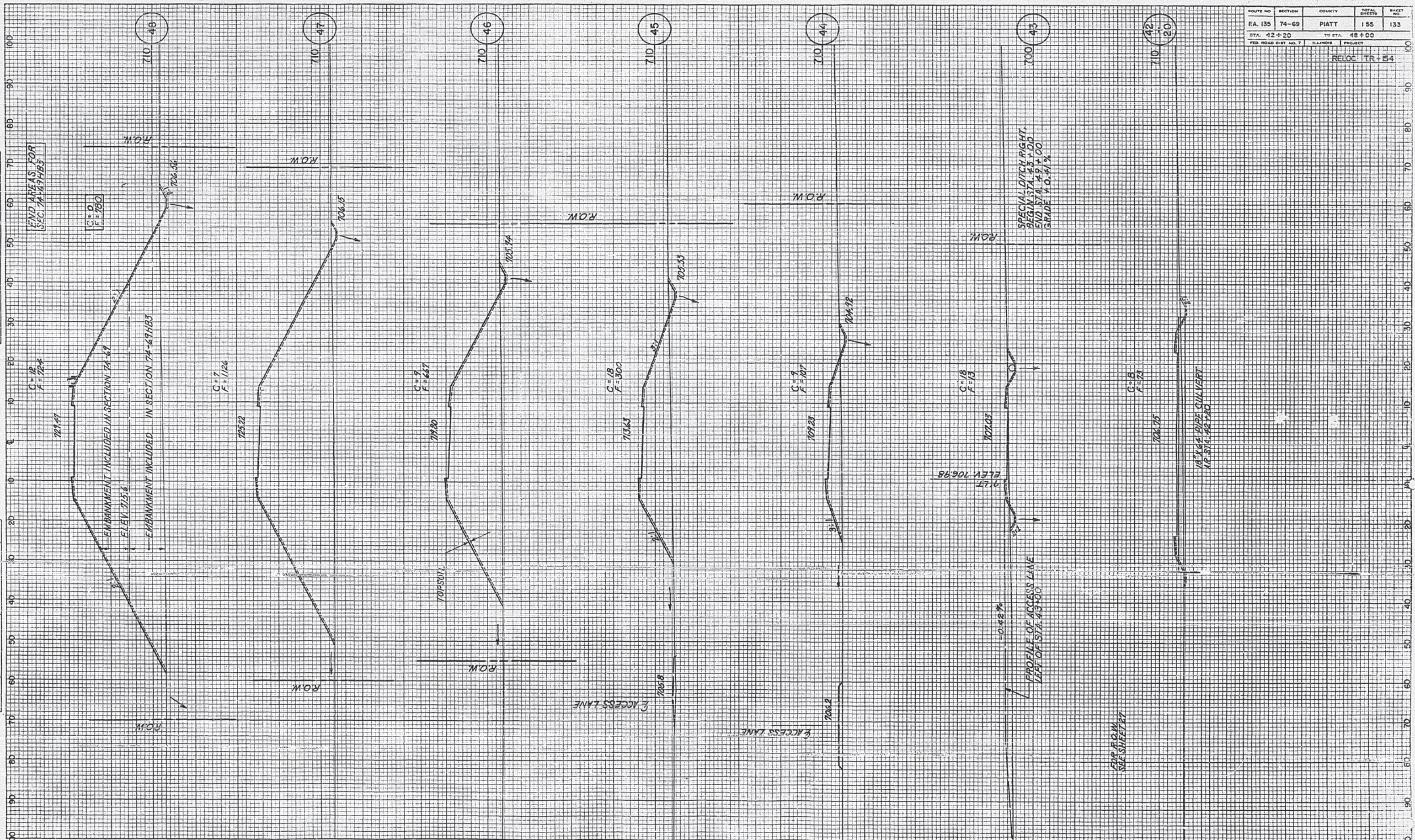
HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION B P R STANDARD
ENGINE DISTRICT CO. CHICAGO - NEW YORK

Drawn in U.S.A.
On A31 WEIGHT Tracing Paper

DATE	BY	FINAL SURVEY	NO.
		SURVEYED	
		NOTED	
		REMARKS	
		AREAS CHECKED	

DATE	BY	ORIGINAL SURVEY	NO.
6-6-59	C.C.S.	SURVEYED	
6-12-59	C.C.S.	NOTED	
7-12-59	C.C.S.	REMARKS	
8-12-59	C.C.S.	AREAS CHECKED	



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-69	PIATT	155	133
STA. 42+20	TO STA. 48+00	ILLINOIS	PROJECT	
FED. ROAD DIST. NO. 7				

HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

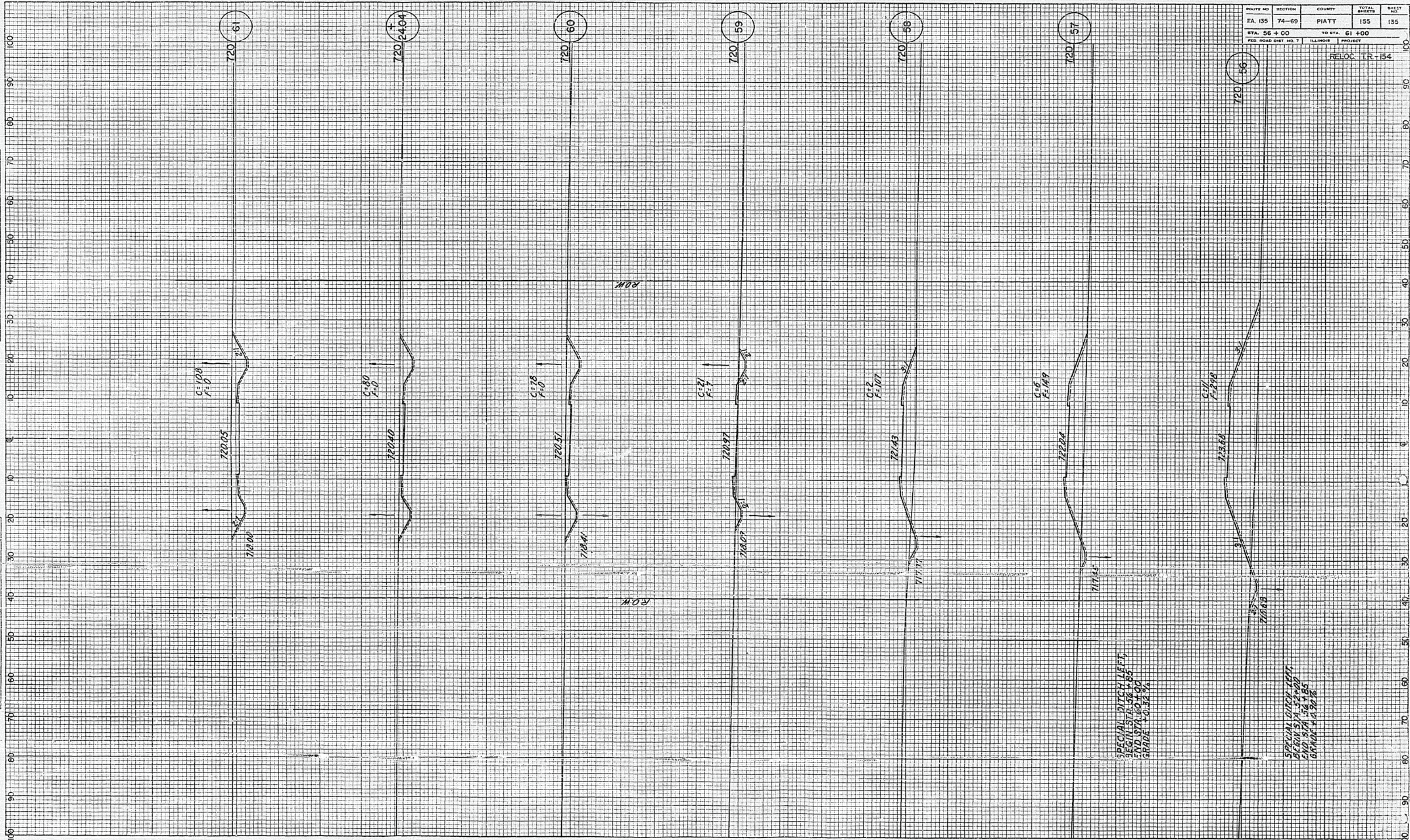
PLATE 3 CROSS SECTION B P R STANDARD
ELEV. DETTGEN CO. CHICAGO - NEW YORK

FOR ROW
SEE SHEET 77

FINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTED	
AREAS CHECKED	
NO.	

ORIGINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTED	
AREAS CHECKED	
NO.	

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-69	PIATT	155	135
STA. 56 + 00		TO STA. 61 + 00		
FED. ROAD DIST. NO. 7		ILLINOIS	PROJECT	
RELOC. TR-154				



HOMER L. CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION D P.R. STANDARD
DESIGN BETWEEN CH. CHICAGO - ILL. 1954

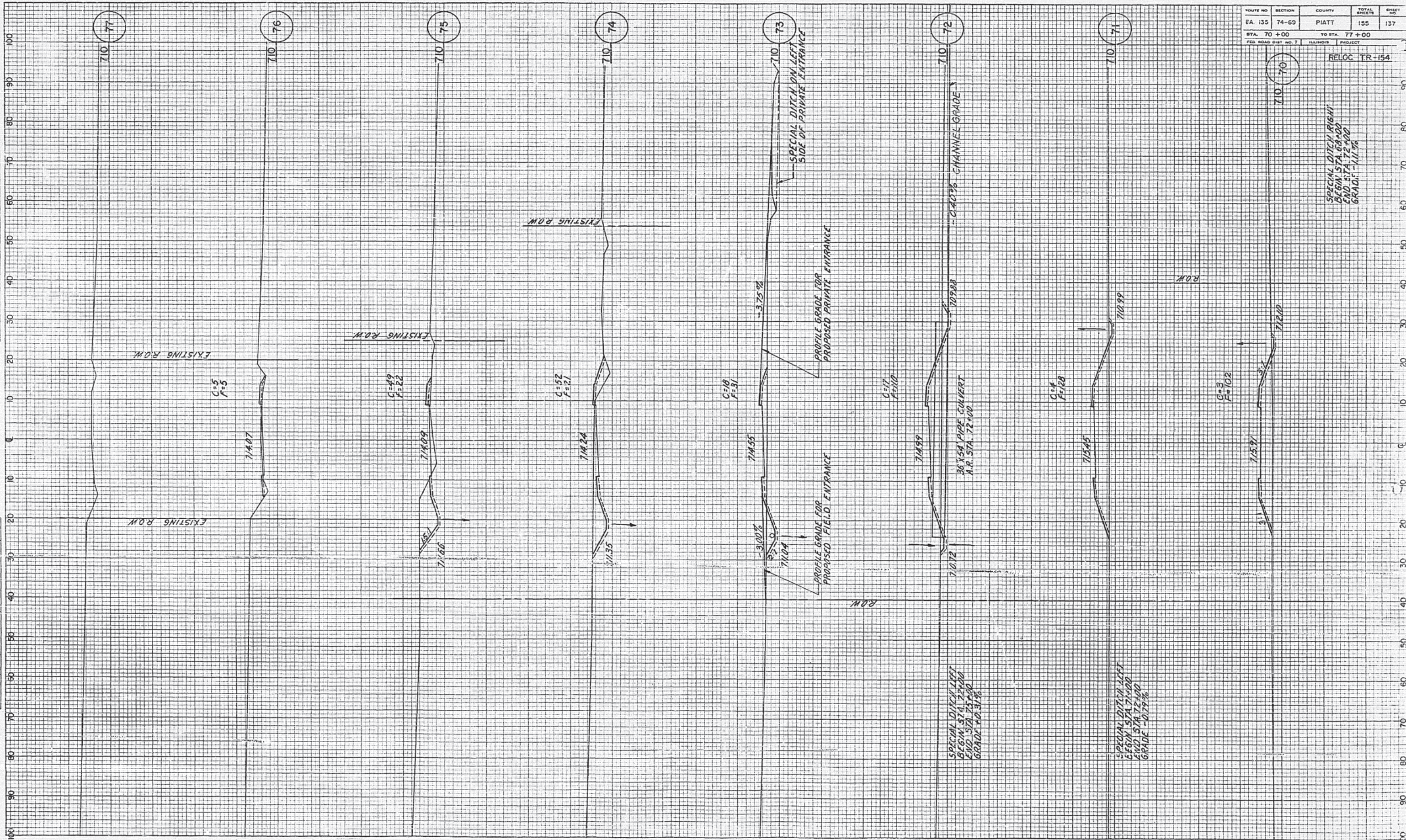
Printed in U.S.A.
On ABBEWEIGHT Tracing Cloth

ORIGINAL	DATE
SURVEYED	4-10-60
PLOTTED	4-11-60
TEMPLATE	4-11-60
AREAS	4-12-60
AREAS CHECKED	6-12-60

Printed in U.S.A.
On AIRWEIGHT Tracing Cloth

FINAL	BY	DATE
SURVEY		
NOTE BOOK		
AREAS		
AREAS CHECKED		

ORIGINAL	BY	DATE
SURVEY		
NOTE BOOK		
AREAS		
AREAS CHECKED		

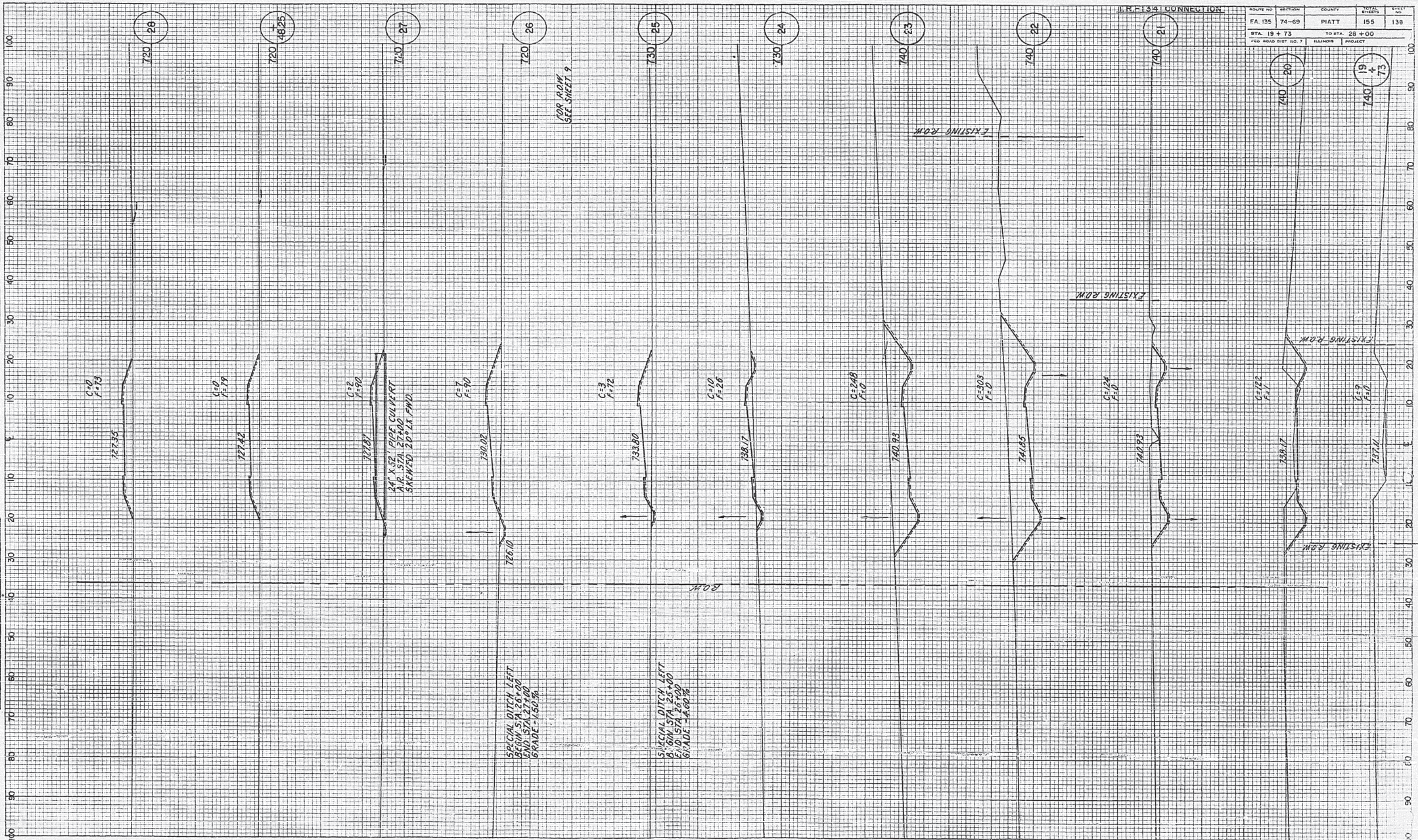


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA 135	74-69	PIATT	155	137

STA.	TO STA.	PROJECT
70+00	77+00	RELOC TR-154

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	NOTED		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	DATE
NOTE BOOK	BY
NO.	AREAS CHECKED



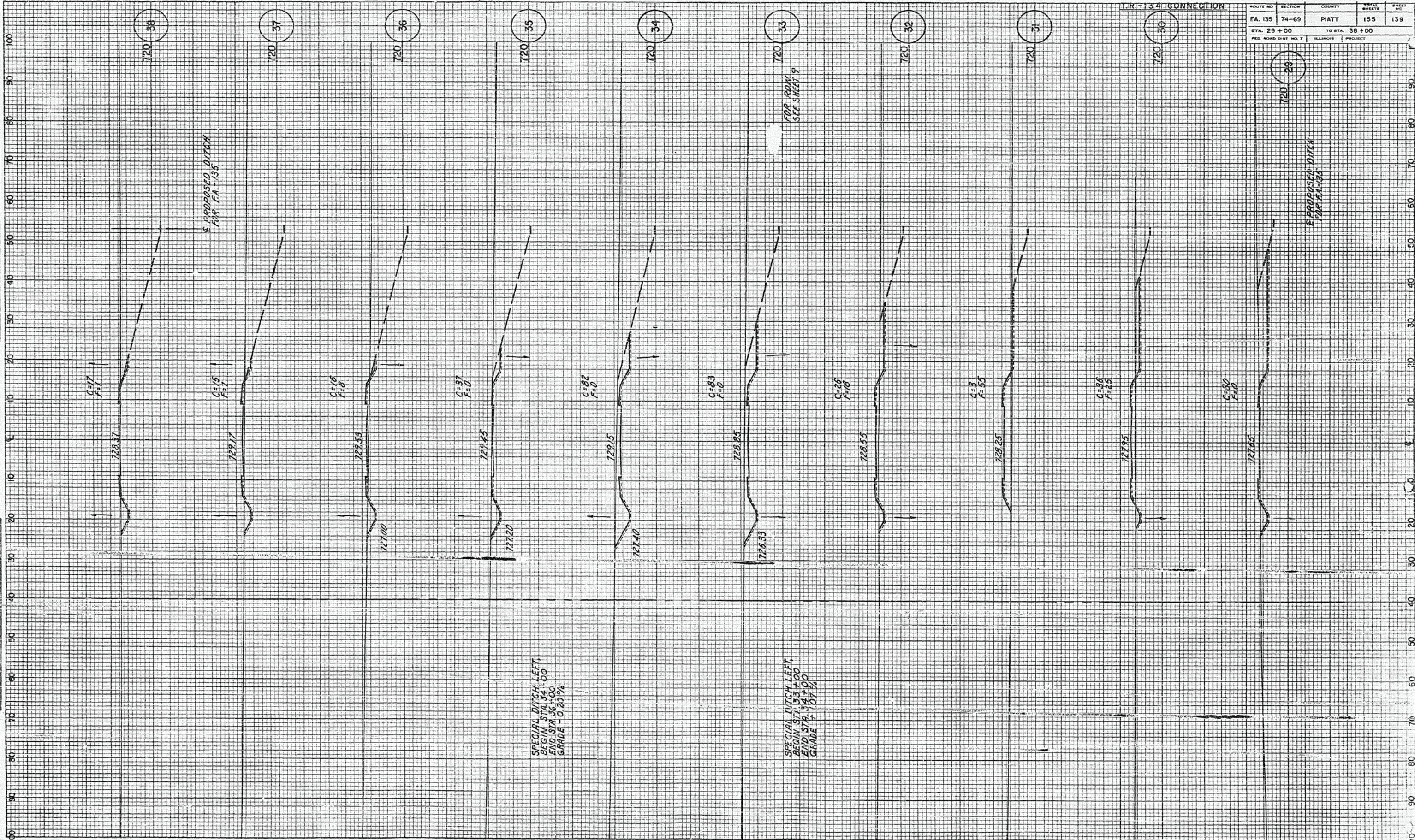
HOMER I CHASTAIN & ASSOCIATES, CONSULTING ENGINEERS - DECATUR, ILLINOIS

PLATE 3 CROSS SECTION B: P. R. STANDARD
ENGINE DESIGN CO. CHICAGO, NEW YORK

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EA. 135	74-69	PIATT	155	138
STA. 19 + 73	TO STA. 28 + 00			
FED. ROAD DIST. NO. 7	ILLINOIS	PROJECT		

FINAL SURVEY	DATE
NOTE BOOK	
AREA CHECKED	
NO.	

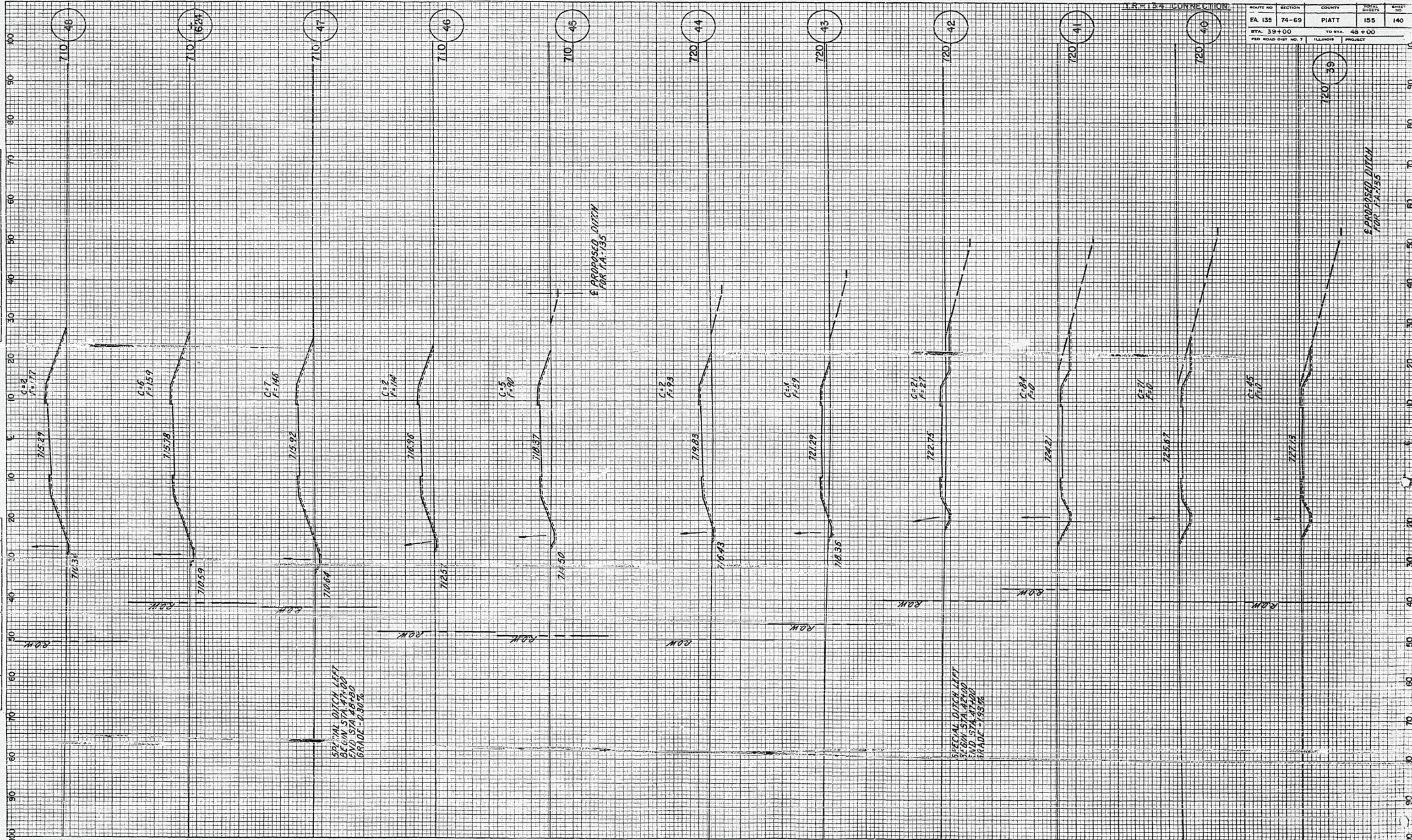
ORIGINAL SURVEY	DATE
NOTE BOOK	
AREA CHECKED	
NO.	



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA. 135	74-69	PIATT	155	139
STA. 29+00	TO STA. 38+00			
FED. ROAD DIST. NO. 7	ILLINOIS PROJECT			

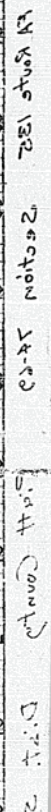
FINAL	BY	DATE
SURVEY		
PLOTTED		
TEMPLATE		
AREA		
ADJUST		

ORIGINAL	BY	DATE
SURVEY		
PLOTTED		
TEMPLATE		
AREA		
ADJUST		

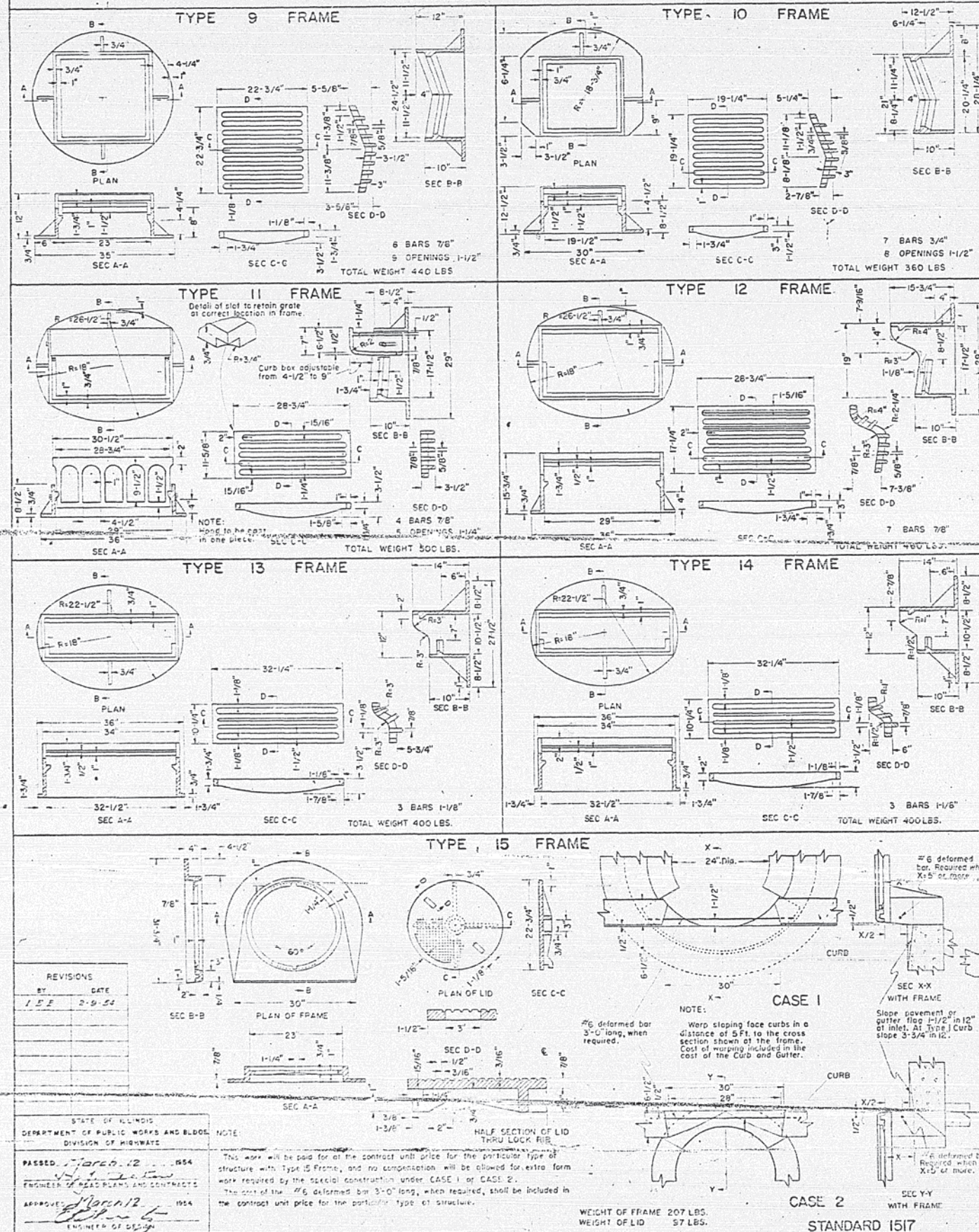


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA 135	74-69	PIATT	155	140
STA. 39+00			TO STA. 45+00	
FED. ROAD DIST. NO. 7		ILLINOIS	PROJECT	

ORIGINAL SURVEY	DATE
	4-9-66
	4-15-66
	4-18-66
	4-21-66
	4-25-66



DETAILS OF STANDARD FRAMES AND GRATES TYPES 9 TO 15



STANDARD SYMBOLS THESE SYMBOLS AND ABBREVIATIONS ARE USED THROUGHOUT THESE PLANS UNLESS OTHERWISE NOTED

State Line	County Line	Trolley Pole
City, Village or Town Limits	Township Line	Telephone or Telegraph Pole
Section or Grant Line	Section Corner	Power Line Pole
Unfenced Property Line.	Right of Way Line	
Fence Line		
Expansion Joint	Contraction Joint	
Retaining Wall	Curb Wall	
Base or Survey Line	Levee	
Culvert to be Constructed	Existing Culvert	
Culvert with Drop Inlet		
Marsh	Stream	
Trees		
Traveled Way		
Storm Sewer (Direction of Flow & Invert Elevation Indicated)		
Tile Drain (Direction of Flow & Invert Elevation Indicated)		
Guard Rail		
Steam or Electric Railroad, or Utility Tracks		
Hedge		
Longitudinal Construction Joint		
Longitudinal Metal Joint		
Existing Pavement, Curb & Gutter, Driveway Pavement & Sidewalk to be Removed		
Elevation of Surface of Finished Pavement at Point Indicated.		
Elevation, Top of Curb at Point Indicated.		
Elevation of Flow Line of Gutter at Point Indicated.		
Existing Inlet, Inlet to be Adjusted, or Inlet to be Reconstructed.		
Inlet to be Constructed		
Inlet to be filled with Sand & Connection Blocked with Brick & Mortar		
Existing Catch Basin, Catch Basin to be Adjusted, or Catch Basin to be Reconstructed.		
Catch Basin to be Constructed		
Catch Basin to be filled with Sand & Connection Blocked with Brick & Mortar		
Existing Manhole, Manhole to be Adjusted, or Manhole to be Reconstructed		
Manhole to be Constructed.		
Manhole to be filled with Sand & Connection Blocked with Brick and Mortar		
Existing Valve Vault, Valve Vault to be Adjusted, or Valve Vault to be Reconstructed.		
Valve Vault to be Constructed		
Valve Vault to be filled with Sand & Connection Blocked with Brick and Mortar		
Existing Fire Hydrant, or Fire Hydrant to be Adjusted		
Fire Hydrant & Auxiliary Valve to be Moved (Symbol with Letter Indicates New Location)		
Existing Light Standard, or Light Standard to be Adjusted		
Light Standard to be Moved (Symbol with Letter Indicates New Location)		
Existing Stop & Go Light, or Stop & Go Light to be Adjusted		
Stop & Go Light to be Moved (Symbol with Letter Indicates New Location)		
Existing Traffic Sign, or Traffic Sign to be Adjusted		
Traffic Sign to be Moved (Symbol with Letter Indicates New Location)		
Existing House Service Box or House Meter Vault, or House Service		
Box or House Meter Vault to be Adjusted		
House Service Box or House Meter Vault to be Moved (Symbol with Letter Indicates New Location)		
Existing Main Service Box or Main Meter Vault, or Main Service		
Box or Main Meter Vault to be Adjusted		
Main Service Box or Main Meter Vault to be Moved (Symbol with Letter Indicates New Location)		
Right of Way Markers		

PLAN ABBREVIATIONS

T.D.	Tile Drain
S.S.	Storm Sewer (Existing)
S.S. 18" X 24" TYPE 1	Storm Sewer (Size, Length and Type)
S.S. 18" X 24" TYPE 1 SSRCPP	Storm Sewer (Size, Length, Type and Material)
CMP	Corrugated Metal Pipe
CIP	Cast Iron Pipe
PC	Pipe Culvert (Existing)
PC 18" X 24" TYPE 1	Pipe Culvert (Size, Length and Type)
PC 18" X 24" SSRCPP	Pipe Culvert (Size, Length, Type and Material)
F.F.	Face to Face of Curb
B.B.	Back to Back of Curb
E-F.	Center Line to Face of Curb
E-B.	Center Line to Back of Curb
Δ	Central Angle
D.	Degree of Curve
T.	Tangent Length
L.	Curve Length
R.	Radius of Curve
E.	External Distance
S.	Superelevation (ft. per ft. of width)
P.C.	Point of Curvature
P.I.	Point of Intersection
P.T.	Point of Tangency
P.O.T.	Point on Tangent
P.C.C.	Point of Compound Curvature
P.R.C.	Point of Reverse Curvature
V.C.	Vertical Curve
X.	External Distance of Vertical Curve
S.B.I.	State Bond Issue
F.A.	Federal-aid
S.A.	State-aid
Rt.	Route
Sec.	Section
Sta.	Station
R.P.S.	Reference Point Stake
I.P.	Iron Pipe
N&W	Nail & Washer
T.P.	Telephone Pole
P.P.	Power Pole
F.P.	Fence Post
F.H.	Fire Hydrant
B.M.	Bench Mark
R.R.S.	Rail Road Spike
R.O.W.	Right of Way
Invt.	Invert
FL.	Flow Line
+	Summit
Elev.	Elevation
P.C.C.	Portland Cement Concrete
U.S.G.S.	US Geological Survey or US Coast & Geodetic Survey
F.A.S.	Federal-aid Secondary
M.F.T.	Motor Fuel Tax
C.S.	City Streets
Proj.	Project

• If it is definitely known that adjustment or reconstruction is required, place A or R inside the symbol. If a new casting is required, show the casting number. Use P for open, C for closed lid. Example - Catch Basin to be reconstructed with new type 3 frame, open lid - (A)3P.

• First character denotes type of structure. Use Ss for special design. Second character denotes number of frame or grate. Example - Type A manhole with type 1 frame & closed lid - (A)1C.

STANDARD 1686-1

DETAILS OF GRAVEL OR CRUSHED STONE AT SIDE ROADS AND MAIL BOXES

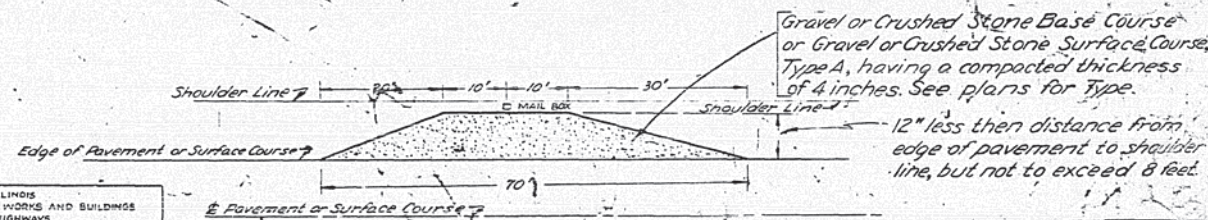
In general, same as width of R.O.W. of side road. May be varied as directed by the Engineer.

Gravel or Crushed Stone Base Course or Gravel or Crushed Stone Surface Course, Type A, having a compacted thickness of 4 inches. See plans for type.

Edge of Pavement or Surface Course

Edge of Pavement or Surface Course

DETAIL OF SIDE ROAD APPROACH



DETAIL OF MAIL BOX TURNOUT

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

PASSED *Nov 2, 1952*
BY *H. J. Cullen*
ENGINEER OF ROAD PLANS AND CONTRACTS

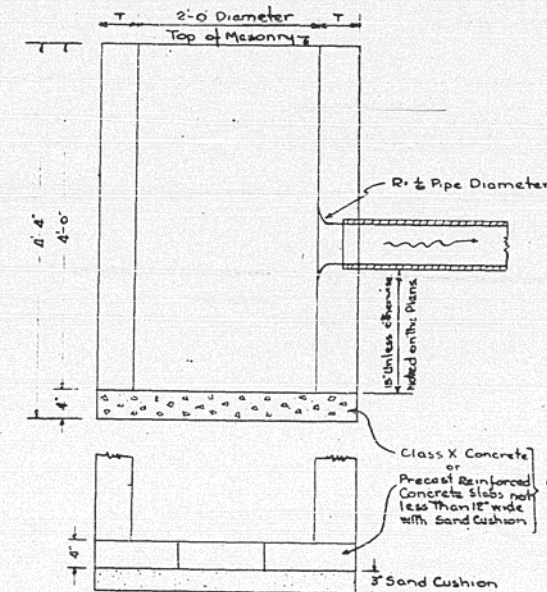
APPROVED *Nov 2, 1952*
BY *E. J. Sherry*
ENGINEER OF DESIGN

REVISIONS

Rev. 2 11-22-52

STANDARD 1776R

STANDARD DESIGN FOR CATCH BASIN TYPE C



ALTERNATE MATERIALS FOR WALLS	T
Precast Reinforced Concrete Risers	3"
Concrete Masonry Units	5"
Monolithic Concrete	6"
Building Brick, Grade SW, from Clay or Shale	6"
Concrete Building Brick, Grade A	6"

Type C Catch Basin to be provided with:

- Type 1 Frame & Open Lid
- or Type 2 Frame
- or Type 3 Frame
- or Type 5 Frame & Open Lid
- or Type 6 Frame
- or Type 8 Grate
- or Type 9 Frame or Type 13 Frame
- or Type 10 Frame or Type 14 Frame
- or Type 11 Frame or Type 15 Frame
- or Type 12 Frame

Brick, Concrete Masonry Units and Precast R.C. Risers are to be laid in full mortar beds with flush joints. The Contract Unit Price for Catch Basins, Type C, shall include frame and lid or grate, as specified.

Note: Furnishing & Installing Sand Cushion to be included in the Contract Unit Price for Catch Basins, Type C.

STANDARD NO. 1538R

STANDARD DESIGN FOR ROAD UNDER CONSTRUCTION SIGN

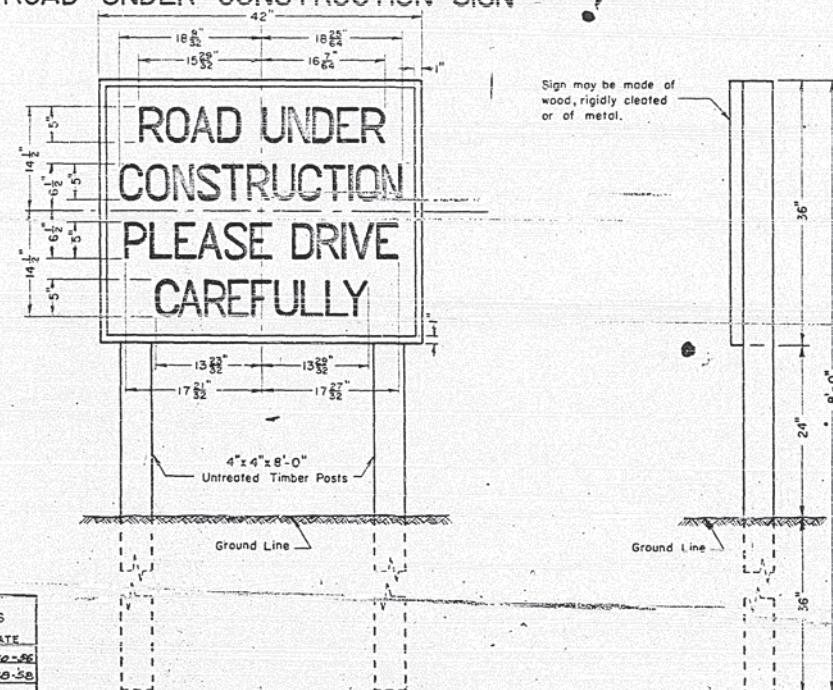
GENERAL NOTES

The border and letters shall be printed black on a white background.

The letters shall have a stroke $\frac{3}{8}$ " wide and a width in proportion to the dimensions shown.

The Contractor shall furnish all materials and labor for constructing and erecting this sign at each end of the construction section, or as directed by the Engineer. The signs shall be placed prior to the actual starting of construction operations.

Furnishing and erecting these signs will be considered incidental to the contract and no additional compensation will be allowed.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

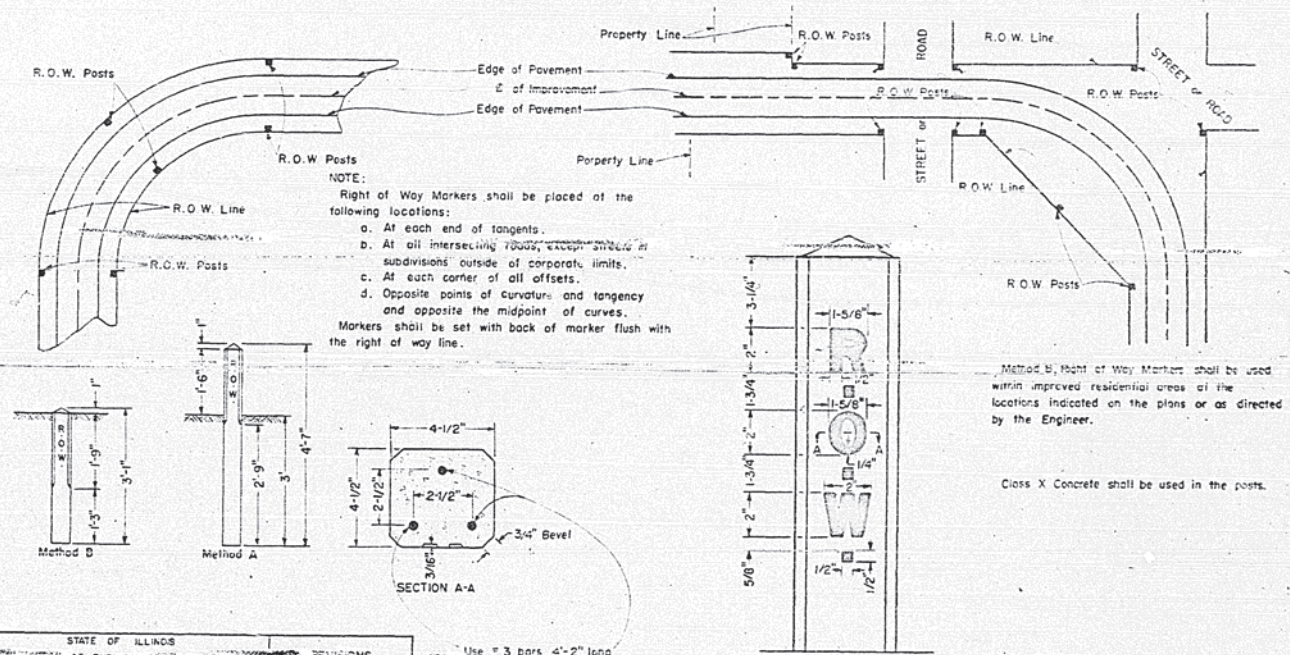
PASSED *Nov 2, 1951*
BY *H. J. Cullen*
ENGINEER OF ROAD PLANS AND CONTRACTS

APPROVED *Nov 2, 1951*
BY *E. J. Sherry*
ENGINEER OF DESIGN

REVISIONS	BY	DATE
1	JFL	11-10-58

STANDARD 1971-2

STANDARD DESIGN FOR RIGHT OF WAY MARKERS



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BLDGS.
DIVISION OF HIGHWAYS

PASSED *August 15, 1954*
BY *H. J. Cullen*
ENGINEER OF ROAD PLANS AND CONTRACTS

APPROVED *August 15, 1954*
BY *E. J. Sherry*
ENGINEER OF DESIGN

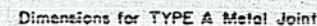
Use 3 bars 4'-2" long for Method A

Use 3 bars 2'-8" long for Method B

Specification Reference: Section 104

STANDARD 1744-1

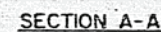
TYPE A



Dimensions A, B, and C
given in table

Dimensions A, B, and C
given in table

TYPE C



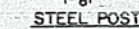
NOTE:-
This type permitted only where pavement on each side of the joint is constructed in separate operations. The metal joint shall remain in place



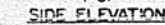
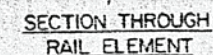
STANDARD 1766-2

REVISIONS	
BY	DATE
W.H.F.	11-9-59

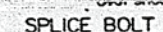
STEEL PLATE BEAM GUARD RAIL
(WOOD OR STEEL POST)



SECTION SHOWING CONNECTION OF RAIL TO POST



DETAIL OF STEEL POST



Post Bolt is similar except length

NOTE: The cost of End Sections shall be included in the unit price bid per lineal foot for Steel Plate Beam Guard Rail.

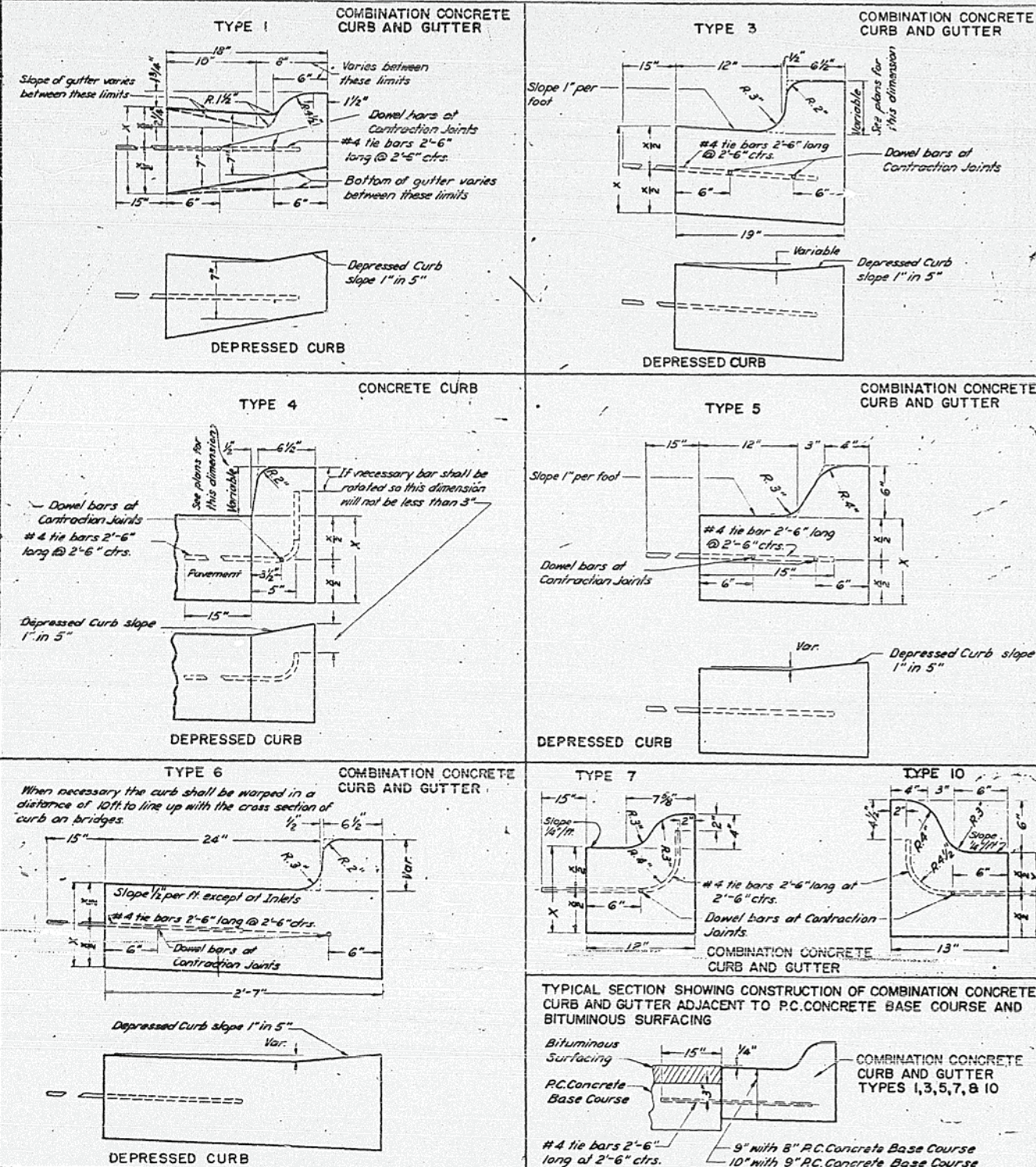
Adjacent to Bridges: Adjacent to bridges the guard rail shall be tapered in from its usual location so that the face of the rail at the end of the guard rail shall line up with the inside of the bridge rail or post. The transition from the usual location to the bridge shall be made by means of a straight line and at the rate of 12.5 feet parallel to the center line of road to one foot perpendicular to the center line.

REVISIONS	
BY	DATE

Specification Reference: Section 94

STANDARD 1687-2
Redrawn 8/20/59 WT

DETAILS OF CURB AND GUTTERS TYPES 1,3,4,5,6,7 AND 10



Dimension "X" is equal to the adjacent pavement edge thickness.

DEPRESSED CURBS—The top of curbs shall be depressed where the Curb and Gutter is constructed across street intersections and for private drives opposite each lot, tract or parcel of land fronting on the improvement upon request of the owner or owners thereof or where directed by the Engineer.

CONTRACTION JOINTS—Contraction joints of a type similar to that used in the adjacent pavement shall be installed in the Curb and Gutter in prolongation with the joints in the pavement. The details of the transverse joints in the Curb and Gutter shall be approved by the Engineer. If Contraction Joints are not provided in the pavement, Contraction Joints are not required in the Curb or Gutter. The cost of Contraction Joints including dowel bars shall be included in the contract unit price per lineal foot for the particular type of Curb and Gutter specified.

The cost of furnishing and installing tie bars shall be included in the contract unit price per lineal foot for the particular type of Curb and Gutter specified.

Where Curb and Gutter are not adjacent to P.C. Concrete Pavement or P.C. Concrete Base Course, tie bars, Contraction Joints and dowel bars shall be omitted.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

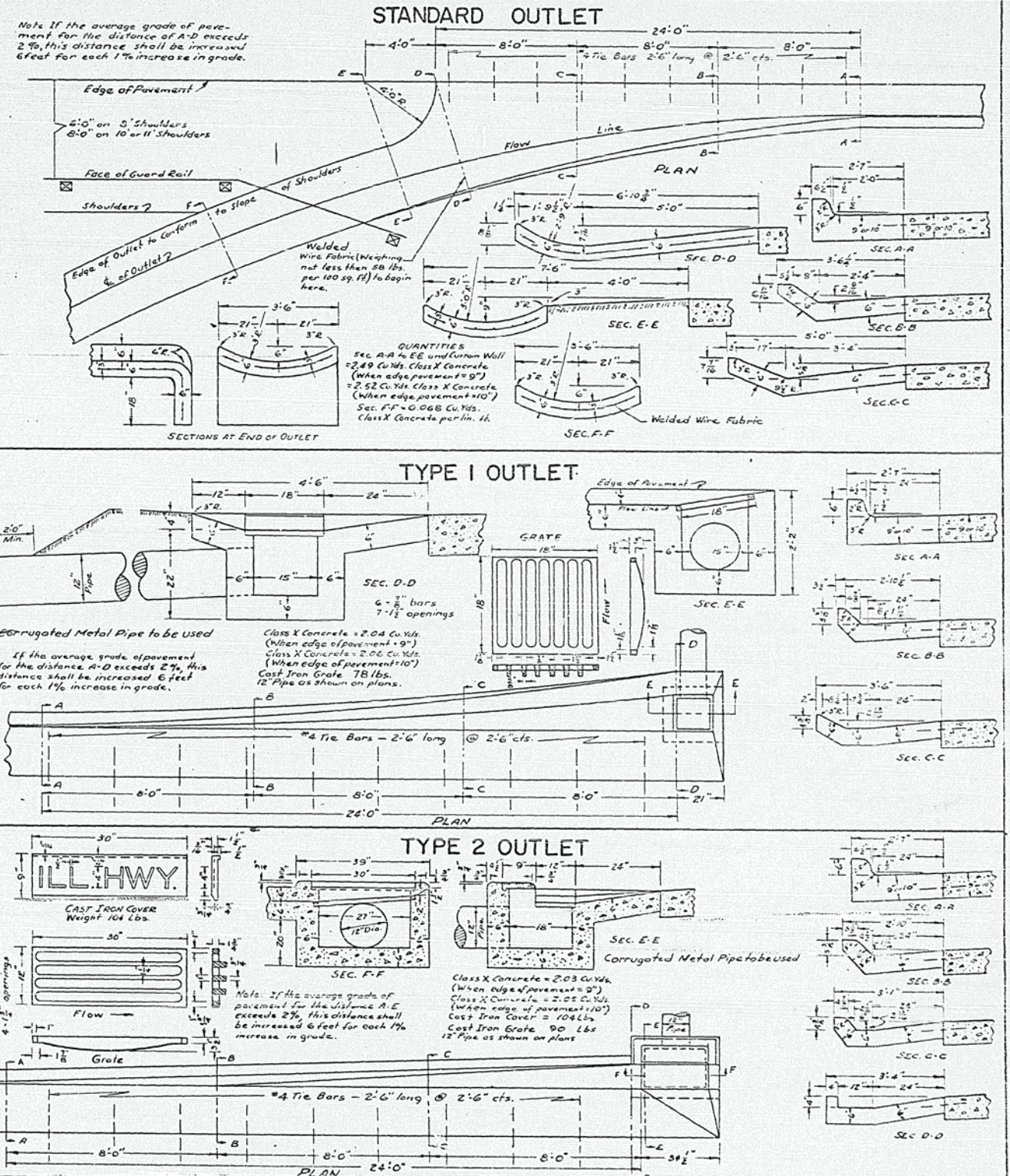
DESIGNED: January 10, 1955
CHECKED: [Signature]
APPROVED: January 19, 1955
[Signature]

See Relation Reference Section 81

STANDARD 1790 F

Drawn 12-9-56 JF

OUTLETS FOR CONCRETE CURB & GUTTER TYPE 6



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

DESIGNED: March 2, 1956
CHECKED: [Signature]
APPROVED: March 2, 1956
[Signature]

Note: Class X Concrete shall be used throughout. The Gutter Outlet will be paid for at the contract unit price per cubic yard for CLASS X CONCRETE, which price shall include the cost of furnishing and placing the reinforcement bars and welded wire fabric in accordance with the Standard Specifications and the details shown.

The 12" pipe will be paid for at the contract unit price per lin. foot for CORRUGATED METAL PIPE, which price shall include all bends or elbows which are required.

The Cast Iron Grates and Covers shall be paid for at the Contract unit price per pound for CAST IRON Grates, which price shall include the Cast Iron Grate and Cover complete in place.

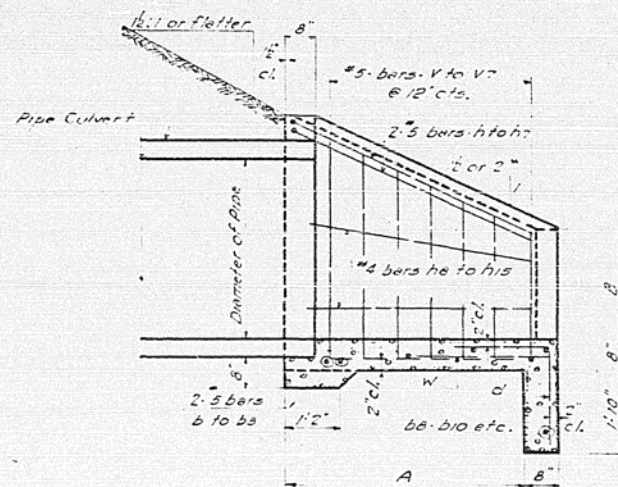
Contraction joints of a type similar to that used in the adjacent pavement shall be installed in the Gutter Outlet in prolongation with the joints in the pavement. The details of the transverse joints in the Gutter Outlet shall be approved by the Engineer. If Contraction Joints are not provided in the pavement, Contraction Joints will not be required in the Gutter Outlet. The cost of Contraction Joints shall be included in the Contract unit price per cu. yd. for CLASS X CONCRETE.

STANDARD 1897 F

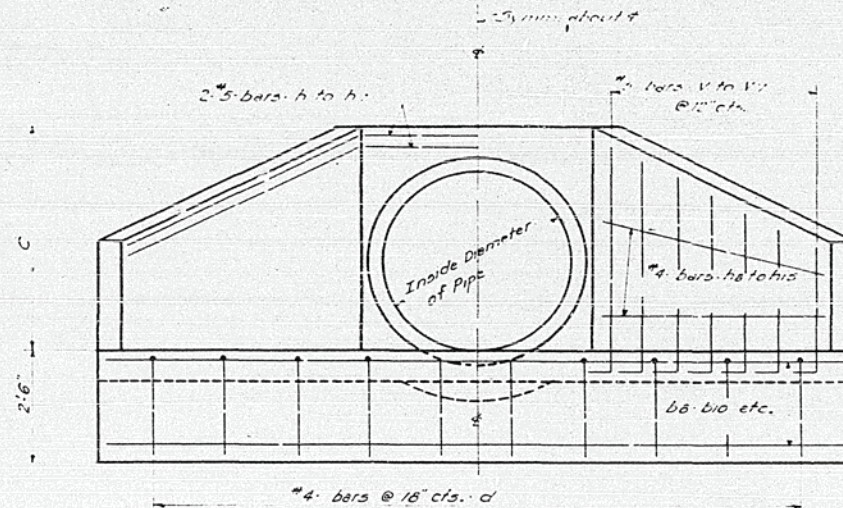
Revised 12-10-56

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

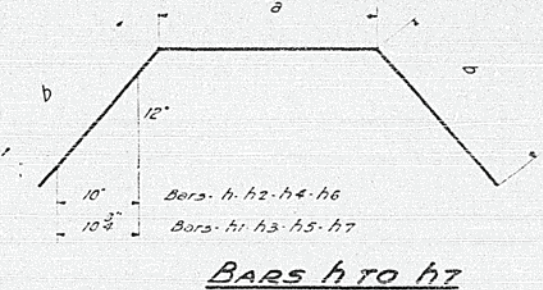
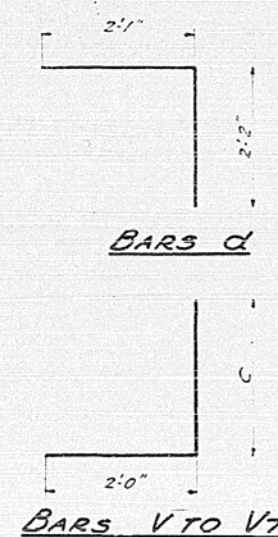
PROJECT NO.	SECTION	SHEET	DATE	BY	CHECKED	APPROVED	SHEET NO. 1
100-100-100	100-100-100	100-100-100	100-100-100	100-100-100	100-100-100	100-100-100	100-100-100



SECTION A-A



END ELEVATION



DIMENSIONS OF BENT BARS

5 Bars V to V7			5 Bars h to h7		
Bars	C	Total Lgth.	Bars	a	Total Lgth.
V	6'-0"	8'-0"	h	4'-8"	5'-2"
V1	5'-6"	7'-6"	h1	4'-8"	6'-8"
V2	5'-0"	7'-0"	h2	5'-3"	5'-10"
V3	4'-6"	6'-6"	h3	5'-3"	7'-6"
V4	4'-0"	6'-0"	h4	5'-10"	6'-7"
V5	3'-6"	5'-6"	h5	5'-10"	8'-4"
V6	3'-0"	5'-0"	h6	6'-5"	7'-2"
V7	2'-6"	4'-6"	h7	6'-5"	9'-3"

BARS IN ONE HEADWALL

42" Pipe		48" Pipe		54" Pipe		60" Pipe	
D42-16	D42-2	D48-16	D48-2	D54-16	D54-2	D60-16	D60-2
Bars	No.	Bars	No.	Bars	No.	Bars	No.
d	8	d	9	d	10	d	11
b	2	b	2	b1	2	b2	2
b3	1	b4	1	b5	1	b6	1
b8	2	b9	1	b10	1	b11	1
		b12	2	b13	1	b14	2
		b15	2	b16	2	b17	2
h	2	h1	2	h2	2	h3	2
h4	4	h5	4	h6	4	h7	4
V3	2	V4	2	V5	2	V6	2
V7	2	V8	2	V9	2	V10	2
V11	2	V12	2	V13	2	V14	2
V15	2	V16	2	V17	2	V18	2
V19	2	V20	2	V21	2	V22	2
V23	2	V24	2	V25	2	V26	2
V27	2	V28	2	V29	2	V30	2
W	2	W	2	W	2	W	2

DIMENSIONS AND QUANTITIES

Design No.	Inside Diam. of Pipe	Slope of Fill	Dimensions						Ct. Conc. 2' Hdwls. Cu. Yds.	Reinf. Bars 2' Hdwls. Lbs.
			A	B	C	D	E	F		
D42-16	42"	12:1	3'-4"	2'-2"	4'-4"	4'-10"	11'-10"	5'-0"	4.8	330
D42-2	42"	2:1	4'-5"	2'-2"	4'-4"	4'-10"	14'-0"	6'-6"	6.2	400
D48-16	48"	12:1	3'-9"	2'-5"	4'-11"	5'-5"	13'-3"	5'-7"	5.6	360
D48-2	48"	2:1	5'-0"	2'-5"	4'-11"	5'-5"	15'-9"	7'-4"	7.6	460
D54-16	54"	12:1	4'-2"	2'-8"	5'-5"	6'-0"	14'-8"	6'-2"	6.6	430
D54-2	54"	2:1	5'-7"	2'-8"	5'-5"	6'-0"	17'-6"	8'-2"	9.2	550
D60-16	60"	12:1	4'-7"	2'-11"	6'-0"	6'-7"	16'-12"	6'-9"	8.0	490
D60-2	60"	2:1	6'-2"	2'-11"	6'-0"	6'-7"	19'-3"	9'-0"	10.8	620

* If embankment slope above headwall is flatter than 2:1 provide wings for 2:1 slope.

Note: Class 'X' Concrete shall be used throughout.

SIZES OF STRAIGHT BARS

Bars	Size	Length
b	#5	6'-3"
b1	#5	6'-9"
b2	#5	7'-3"
b3	#5	8'-0"
b4	#5	8'-3"
b5	#5	9'-0"
b6	#5	10'-0"
b7	#5	10'-6"
b8	#5	11'-3"
b9	#5	12'-0"
b10	#5	13'-0"
b11	#5	13'-6"
b12	#5	14'-3"
b13	#5	15'-0"
b14	#5	15'-6"
b15	#5	16'-3"
b16	#5	17'-0"
b17	#5	17'-6"
h8	#5	4'-9"
h9	#5	6'-3"
h10	#5	5'-3"
h11	#5	7'-3"
h12	#5	6'-0"
h13	#5	8'-0"
h14	#5	6'-6"
h15	#5	8'-9"
W	#5	4'-0"

REINFORCED CONCRETE HEADWALLS
FOR
42" 48" 54" & 60" DIAMETER
PIPE CULVERTS
AT RIGHT ANGLES WITH ROADWAY

STANDARD 1997

DESIGNED
CHECKED
DRAWN
CHECKED

OCT. 5 1959
EXAMINED
PASSED
APPROVED

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

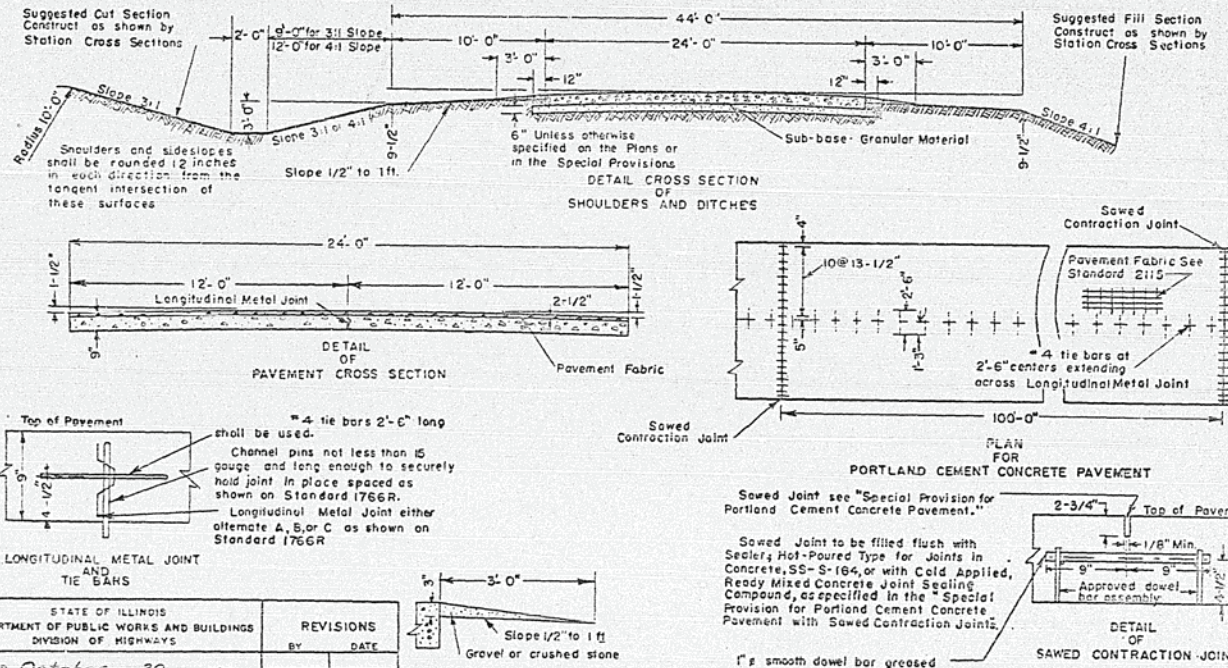
PROJECT NO.	DATE	BY	CHECKED	APPROVED
15-18-30-36"	10-1-59	J. S. Malock		

WINGS FOR 12:1 SLOPE

Skew Angle	Design No.	Ins. Dia. of Pipe	Dimensions for Concrete													Conc. 2-H.W. Cu.Yds.	Reinf. Bars - 2-H.W.				Bars 2-H.W. Lbs.	
			A	B	C	D	E	F	G	H	J	K	M	N	α		h-bars					
5°	DS 15-1/2	15"	2-4	0-10	2-5	1-7	6-11	3-5	3-2	1-7	3-5	3-6	0-2	0-2	85°	1.4	3-6	1-9	3-9	9-0	28	70
	DS 18-1/2	18"	2-4	1-1	2-8	1-10	7-2	3-5	3-2	1-10	3-7	3-7	"	"	"	1.6	3-6	2-0	3-9	9-3	28	70
	DS 24-1/2	24"	2-10	1-4	3-3	2-6	8-10	4-2	3-10	2-6	4-5	4-5	"	"	"	2.2	4-3	2-8	4-7	11-6	32	80
	DS 30-1/2	30"	3-3	1-7	3-9	3-0	10-3	4-9	4-5	3-0	5-1	5-1	"	"	"	2.7	4-10	3-3	5-2	13-3	36	90
	DS 36-1/2	36"	3-9	1-10	4-4	3-6	11-11	5-6	5-1	3-8	5-11	5-11	0-2	0-2	85°	3.3	5-7	3-11	6-0	15-6	42	100
10°	DS 15-1/2	15"	2-4	0-10	2-5	1-7	6-11	3-5	3-2	1-7	3-5	3-6	0-2	0-2	80°	1.5	3-4	1-10	3-10	9-0	28	70
	DS 18-1/2	18"	2-4	1-1	2-8	1-10	7-2	3-5	3-2	1-10	3-7	3-7	"	"	"	1.6	3-4	2-1	3-10	9-3	28	70
	DS 24-1/2	24"	2-10	1-4	3-3	2-6	8-10	4-2	3-10	2-6	4-5	4-5	"	"	"	2.2	4-1	2-9	4-8	11-6	34	80
	DS 30-1/2	30"	3-3	1-7	3-9	3-0	10-4	5-0	4-3	3-0	5-1	5-1	"	"	"	2.8	4-9	3-3	5-6	13-6	36	90
	DS 36-1/2	36"	3-9	1-10	4-4	3-6	12-0	5-10	4-10	3-8	6-0	6-0	0-2	0-2	80°	3.5	5-6	3-11	6-4	15-9	42	110
15°	DS 15-1/2	15"	2-4	0-10	2-5	1-7	6-11	3-5	3-2	1-7	3-5	3-6	0-3	0-2	75°	1.5	3-4	1-10	4-1	9-3	28	70
	DS 18-1/2	18"	2-4	1-1	2-8	1-10	7-2	3-5	3-2	1-10	3-7	3-7	"	"	"	1.7	3-4	2-1	4-1	9-6	28	70
	DS 24-1/2	24"	2-10	1-4	3-3	2-7	9-2	4-7	3-6	2-6	4-6	4-7	"	"	"	2.3	4-0	2-10	4-11	11-9	34	80
	DS 30-1/2	30"	3-3	1-7	3-9	3-1	10-6	5-4	4-1	3-0	5-2	5-2	"	"	"	2.9	4-8	3-4	5-9	13-9	40	100
	DS 36-1/2	36"	3-9	1-10	4-4	3-9	12-3	5-2	4-8	3-8	6-1	6-2	0-3	0-2	75°	3.8	5-3	4-0	6-6	15-9	46	110
20°	DS 15-1/2	15"	2-4	0-10	2-5	1-6	7-4	4-0	3-10	1-7	3-7	3-8	0-3	0-2	70°	1.6	3-3	1-11	4-4	9-6	28	70
	DS 18-1/2	18"	2-4	1-1	2-8	1-11	7-7	4-0	3-10	1-7	3-9	3-9	"	"	"	1.7	3-3	2-2	4-4	9-8	28	70
	DS 24-1/2	24"	2-10	1-4	3-3	2-8	9-4	4-1	3-11	2-6	4-7	4-9	"	"	"	2.4	3-11	2-11	5-2	12-0	38	90
	DS 30-1/2	30"	3-3	1-7	3-9	3-2	10-9	5-8	3-11	3-0	5-4	5-5	"	"	"	3.1	4-5	3-5	5-11	13-9	42	100
	DS 36-1/2	36"	3-9	1-10	4-4	3-10	12-7	6-6	5-7	3-8	6-2	6-2	0-3	0-2	70°	4.0	5-3	4-1	6-11	16-3	50	120
25°	DS 15-1/2	15"	2-4	0-10	2-5	1-9	7-7	4-4	3-11	1-7	3-8	3-10	0-3	0-2	65°	1.6	3-3	1-11	4-7	9-9	28	70
	DS 18-1/2	18"	2-4	1-1	2-8	2-0	7-10	4-4	3-11	1-10	3-10	4-0	"	"	"	1.8	3-2	2-3	4-7	10-0	32	80
	DS 24-1/2	24"	2-10	1-4	3-3	2-9	9-8	5-3	3-12	2-6	4-9	4-11	"	"	"	2.5	3-10	2-11	5-6	12-3	38	90
	DS 30-1/2	30"	3-3	1-7	3-9	3-3	11-2	6-0	3-12	3-0	5-6	5-8	"	"	"	3.3	4-5	3-6	6-4	14-3	44	110
	DS 36-1/2	36"	3-9	1-10	4-4	4-0	13-0	6-11	4-5	3-8	6-5	6-7	0-3	0-2	65°	4.3	5-0	4-3	7-3	16-6	50	120
30°	DS 15-1/2	15"	2-4	0-10	2-5	1-10	7-10	4-8	3-12	1-7	3-10	4-0	0-3	0-2	60°	1.7	3-1	2-0	4-11	10-0	36	90
	DS 18-1/2	18"	2-4	1-1	2-8	2-1	8-2	4-8	3-12	1-10	4-0	4-2	"	"	"	1.9	3-2	2-4	5-0	10-6	36	90
	DS 24-1/2	24"	2-10	1-4	3-3	2-10	10-1	5-8	3-13	2-6	4-11	5-1	"	"	"	2.7	3-9	3-1	5-11	12-9	40	100
	DS 30-1/2	30"	3-3	1-7	3-9	3-5	11-7	6-6	3-9	3-0	5-8	5-11	"	"	"	3.5	4-4	3-8	6-9	14-9	46	110
	DS 36-1/2	36"	3-9	1-10	4-4	4-2	13-7	7-6	4-4	3-8	6-8	6-10	0-3	0-2	60°	4.6	5-0	4-5	7-10	17-3	54	130
35°	DS 15-1/2	15"	2-4	0-10	2-5	1-11	8-3	5-0	3-13	1-7	4-0	4-3	0-3	0-2	55°	1.8	3-1	2-2	5-11	10-6	36	90
	DS 18-1/2	18"	2-4	1-1	2-8	2-3	8-7	5-0	3-13	1-10	4-2	4-5	"	"	"	2.0	3-1	2-5	5-3	10-9	36	90
	DS 24-1/2	24"	2-10	1-4	3-3	3-0	10-7	6-1	3-13	2-6	5-2	5-5	"	"	"	2.9	3-8	3-3	6-4	13-3	40	100
	DS 30-1/2	30"	3-3	1-7	3-9	3-8	12-3	7-0	3-8	3-0	6-0	6-3	"	"	"	3.7	4-2	3-11	7-2	15-3	50	120
	DS 36-1/2	36"	3-9	1-10	4-4	4-3	14-3	8-1	4-2	3-8	7-0	7-3	0-3	0-2	55°	4.9	4-11	4-8	8-5	18-0	56	140
40°	DS 15-1/2	15"	2-4	0-10	2-5	1-12	8-10	5-6	3-14	1-7	4-3	4-6	0-3	0-2	50°	1.9	3-1	2-3	5-8	11-0	38	90
	DS 18-1/2	18"	2-4	1-1	2-8	2-4	9-1	5-6	3-14	1-10	4-5	4-8	"	"	"	2.2	3-0	2-7	5-8	11-3	38	90
	DS 24-1/2	24"	2-10	1-4	3-3	3-1	11-2	6-8	3-15	2-6	5-6	5-9	"	"	"	3.1	3-8	3-6	6-10	14-0	48	110
	DS 30-1/2	30"	3-3	1-7	3-9	3-11	13-0	7-8	3-7	3-0	6-5	6-7	"	"	"	4.0	4-2	4-2	7-11	16-3	54	130
	DS 36-1/2	36"	3-9	1-10	4-4	4-7	15-3	8-10	4-1	3-8	7-6	7-9	0-3	0-2	50°	5.3	4-10	5-0	9-2	19-0	62	150
45°	DS 15-1/2	15"	2-4	0-10	2-5	1-13	9-6	6-1	3-15	1-7	4-7	4-10	0-4	0-3	45°	2.1	3-0	2-5	6-1	11-6	40	100
	DS 18-1/2	18"	2-4	1-1	2-8	2-7	9-10	6-1	3-15	1-10	4-9	5-0	"	"	"	2.4	3-0	2-10	6-2	12-0	44	100
	DS 24-1/2	24"	2-10	1-4	3-3	3-6	12-2	7-4	3-16	2-6	5-11	6-3	"	"	"	3.4	3-8	3-9	7-7	15-0	50	120
	DS 30-1/2	30"	3-3	1-7	3-9	4-3	14-1	8-6	3-16	3-0	6-11	7-2	"	"	"	4.4	4-2	4-5	8-8	17-3	62	150
	DS 36-1/2	36"	3-9	1-10	4-4	5-2	16-5	9-9	4-0	3-8	8-1	8-4	0-4	0-3	45°	5.7	4-10	5-5	10-0	20-3	66	160
50°	DS 15-1/2	15"	2-4	0-10	2-5	1-14	10-4	6-10	3-17	1-7	5-0	5-4	0-4	0-3	40°	2.3	2-11	2-8	6-11	12-6	46	110
	DS 18-1/2	18"	2-4	1-1	2-8	2-10	10-9	6-10	3-17	1-10	5-2	5-6	"	"	"	2.6	3-0	3-1	6-11	13-0	46	110
	DS 24-1/2	24"	2-10	1-4	3-3	3-10	13-4	8-3	3-18	2-6	6-6	6-10	"	"	"	3.7	3-7	4-1	8-4	16-0	56	130
	DS 30-1/2	30"	3-3	1-7	3-9	4-8	15-5	9-6	3-18	3-0	7-6	7-10	"	"	"	4.6	4-1	4-10	9-7	18-6	66	160
	DS 36-1/2	36"	3-9	1-10	4-4	5-8	18-0	10-11	4-0	3-8	8-10	8-12	0-4	0-3	40°	6.8	4-9	5-11	11-1	21-9	74	180
55°	DS 15-1/2	15"	2-4	0-10	2-5	1-15	11-6	7-9	3-19	1-7	5-7	5-11	0-4	0-3	35°	2.5	2-11	3-0	7-10	13-5	50	120
	DS 18-1/2	18"	2-4	1-1	2-8	3-2	11-11	7-9	3-19	1-10	5-7	5-11	"	"	"	2.9	3-0	3-5	7-10	14-3	50	120
	DS 24-1/2	24"	2-10	1-4	3-3	4-4	14-10	9-5	3-19	2-6	7-3	7-7	"	"	"	4.2	3-6	4-7	9-5	17-6	62	150
	DS 30-1/2	30"	3-3	1-7	3-9	5-2	17-2	10-9	3-5	3-0	8-5	8-9	"	"	"	5.4	4-1	5-6	10-11	20-6	74	180
	DS 36-1/2	36"	3-9	1-10	4-4	6-4	20-16	12-5	3-11	3-8	9-10	10-12	0-4	0-3	35°	7.1	4-9	6-7	12-8	24-0	86	200
60°	DS 15-1/2	15"	2-4	0-10	2-5	1-16	13-1	9-0	4-1	3-1	6-4	6-8	0-4	0-3	30°	2.9	2-10	3-5	9-0	15-3	54	130
	DS 18-1/2	18"	2-4	1-1	2-8	3-8	13-7	9-0	4-1	3-10	6-7	6-11	"	"	"	3.2	2-10	3-11	9-0	15-9	58	140
	DS 24-1/2	24"	2-10	1-4	3-3	5-0	16-11	10-11	2-6	8-3	8-7	8-11	"	"	"	4.7	3-6	5-3	11-0	19-9	72	170
	DS 30-1/2	30"	3-3	1-7	3-9	6-0	19-7	12-6	3-4	3-0	9-7	9-11	"	"	"	6.1	4-1	6-3	12-8	23-0	82	200
	DS 36-1/2	36"	3-9	1-10	4-4	7-4	22-11	14-5	3-10	3-8	11-3	11-7	0-4	0-3	30°	8.1	4-7	7-4	14-7	26-9	98	230

WINGS FOR 2:1 SLOPE

STANDARD DESIGN FOR 24 FT. PORTLAND CEMENT CONCRETE PAVEMENT WITH 10 FT. SHOULDERS

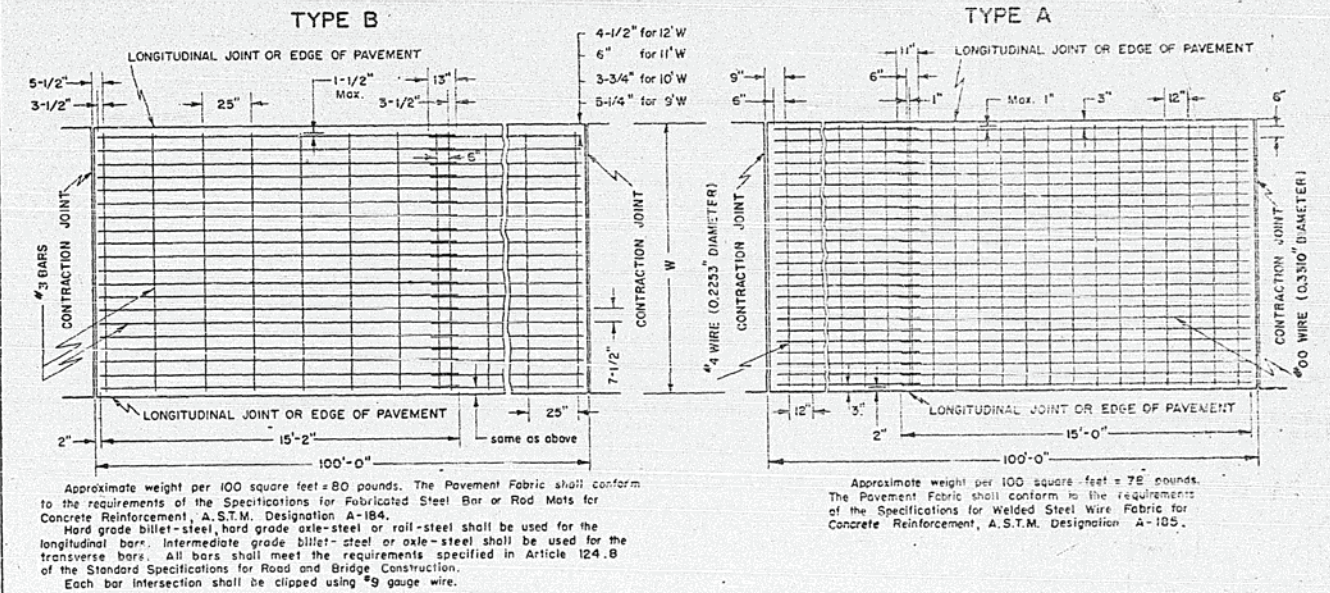


STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS	REVISIONS BY DATE
PASSED <u>October 22</u> 1954 ENGINEER OF ROAD PLANS AND CONTRACTS	
APPROVED <u>October 22</u> 1954 ENGINEER OF DESIGN	

Specification Reference: Section 48

STANDARD 2065R

STANDARD DESIGN FOR PAVEMENT FABRIC

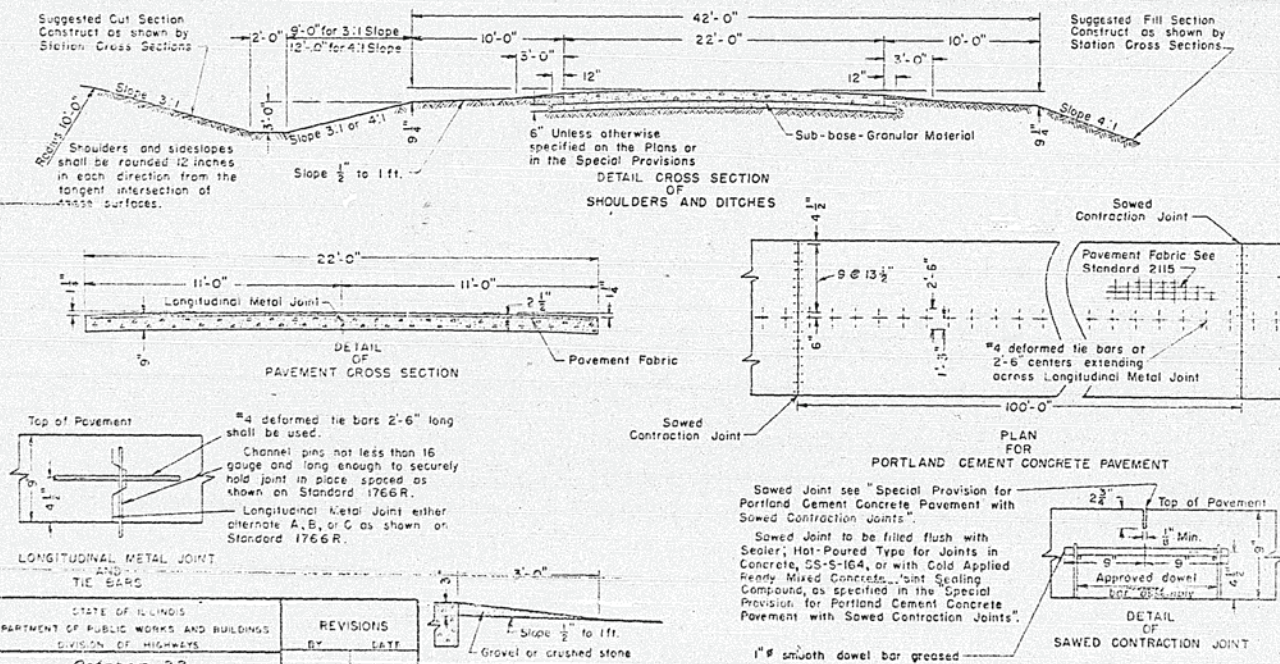


STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BLDGS. DIVISION OF HIGHWAYS	REVISIONS BY DATE
PASSED <u>Sept. 24</u> 1954 ENGINEER OF ROAD PLANS AND CONTRACTS	
APPROVED <u>Sept. 24</u> 1954 ENGINEER OF DESIGN	

Specification References: A-Ries 48J2, 48J7, 48J8, 48J9 and 124.8

STANDARD 2115

STANDARD DESIGN FOR 22 FT. PORTLAND CEMENT CONCRETE PAVEMENT WITH 10 FT. SHOULDERS

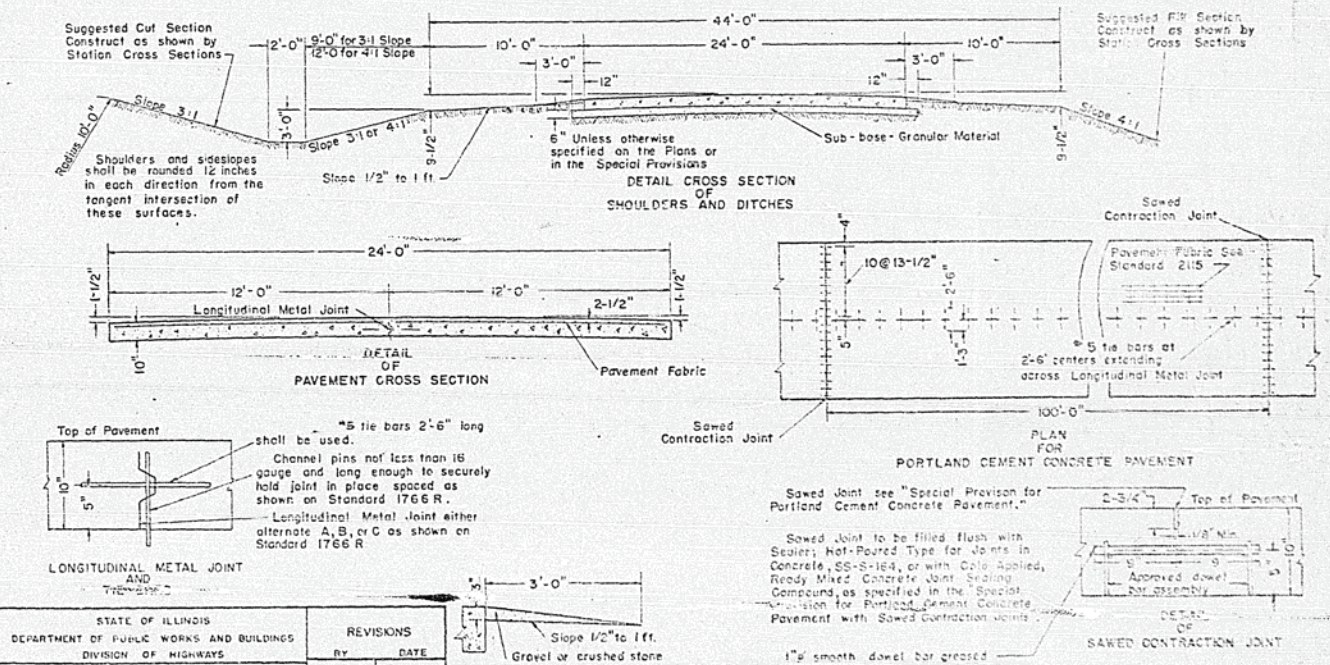


STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS	REVISIONS BY DATE
PASSED <u>October 22</u> 1954 ENGINEER OF ROAD PLANS AND CONTRACTS	
APPROVED <u>October 22</u> 1954 ENGINEER OF DESIGN	

Specification Reference: Section 48

STANDARD 2064R

STANDARD DESIGN FOR 24 FT. PORTLAND CEMENT CONCRETE PAVEMENT WITH 10 FT. SHOULDERS

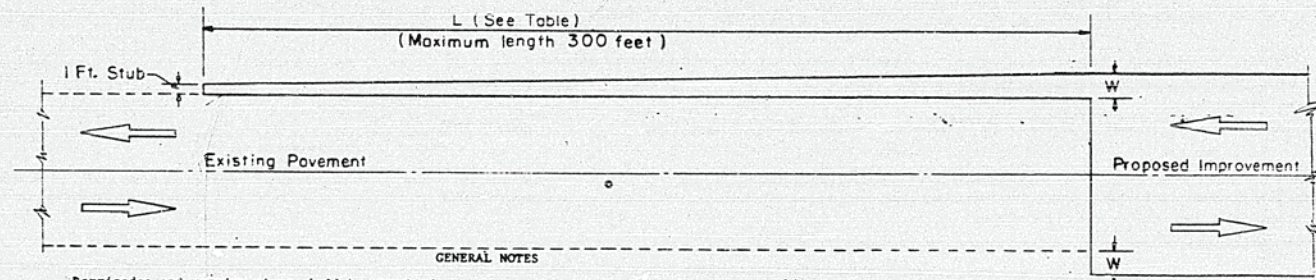


STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS AND BUILDINGS DIVISION OF HIGHWAYS	REVISIONS BY DATE
PASSED <u>October 22</u> 1954 ENGINEER OF ROAD PLANS AND CONTRACTS	
APPROVED <u>October 22</u> 1954 ENGINEER OF DESIGN	

Specification Reference: Section 48

STANDARD 2068R

STANDARD DESIGN FOR WIDTH TRANSITION



GENERAL NOTES

Barricades and warning signs shall be provided, erected, and maintained by the Contractor in accordance with Article 7.14 of the Standard Specifications. The barricades and warning signs shall conform with those shown on Standard 1977-4, and they shall be placed at the locations designated by the Engineer.

At locations where there will be a temporary gap in the widening due to a narrow bridge which is being, or is to be, reconstructed or widened, the length of transition from the wider surface to the bridge shall be as shown in the table.

The material used for the transition shall be the same type as was used to construct the widening or pavement. The thickness shall be the same as that of the widening or pavement being constructed.

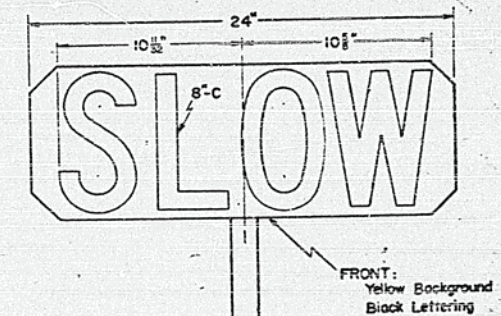
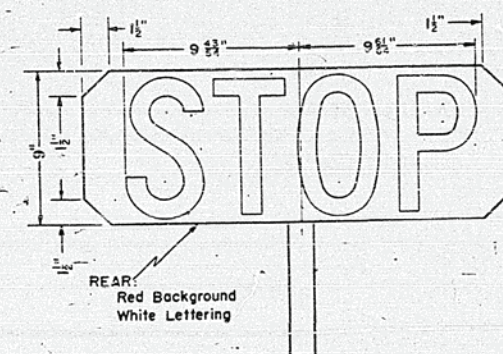
This work will be paid for at the contract unit price included in the contract for the type of widening or pavement being constructed.

TRANSITION LENGTHS FOR VARIOUS WIDTHS	
W feet	L feet
2	150
3	200
4	250
6	300

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS & BUILDINGS DIVISION OF HIGHWAYS	
PASSED	REVISIONS
July 1, 1959	BY DATE
<i>A. H. Ganss</i>	
Engineer of Road Plans and Contracts	
APPROVED July 3, 1959	
<i>C. L. Shantz</i>	
Engineer of Design	

STANDARD 2143
Drawn By WF 7/1/59

STANDARD DESIGN FOR FLAGMAN TRAFFIC CONTROL SIGN



NOTE

The paddle type sign used by the flagman to direct traffic shall be constructed with "STOP" on one side and "SLOW" on the reverse side.

Signs may be constructed with a short handle or a long staff which rests on the ground.

The material for the sign shall be 1/4" Exterior-Grade A-A Plywood or Exterior D.F.P.A.-2 sides sound.

These signs shall be furnished by the Contractor and shall be used by the flagmen in lieu of red flags or other signaling devices.

The cost of furnishing and maintaining the signs will be considered incidental to the contract and no additional compensation will be allowed.

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS & BUILDINGS DIVISION OF HIGHWAYS	
PASSED	REVISIONS
April 10, 1954	BY DATE
<i>H. J. Quinn</i>	
Engineer of Road Plans and Contracts	
APPROVED April 12, 1954	
<i>C. L. Shantz</i>	
Engineer of Design	

STANDARD 2114

DETAILS OF PERMANENT SURVEY MARKERS



THE MARKERS MAY BE EITHER PRECAST OR CONSTRUCTED ON THE SITE EXCEPT WHERE IT IS NECESSARY TO INSTALL THE TABLET IN AN EXISTING ROCK LEDGE, THE CONCRETE PAVEMENT, OR A STRUCTURE.

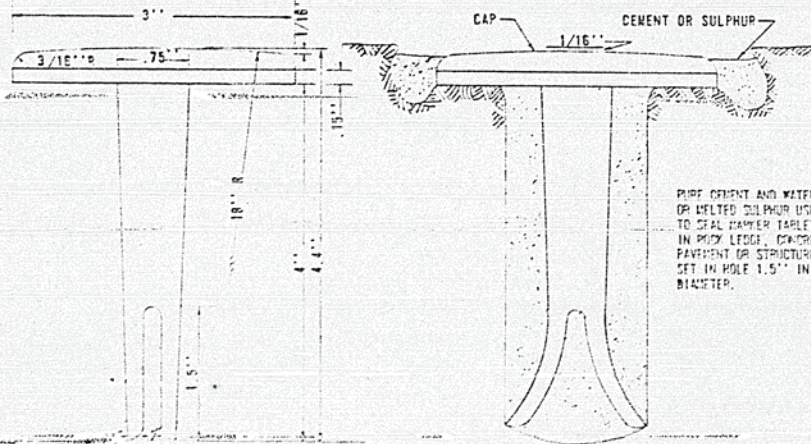
THE LOCATION OF THE MARKERS SHALL BE IN ACCORDANCE WITH THE PLANS. IN GENERAL, THE MARKERS WILL BE PLACED AT THE P.T.'S AND P.C.'S OF HORIZONTAL CURVES AND SPACED ALONG THE TANGENTS IN A WAY THAT A MINIMUM OF TWO MARKERS ARE ALWAYS INTER-VISIBLE.

THE MARKERS SHALL BE PLACED UNDER THE DIRECTION OF THE ENGINEER AND SHALL BE INSTALLED IN A WORKMANLIKE MANNER IN ORDER THAT THERE BE NO FUTURE SETTLEMENT OR HORIZONTAL

SHIFTING. THE MONUMENTS SHALL BE PLACED IN A WAY THAT THE SURVEY POINT WILL FALL WITHIN THE PORTION OF THE PLAQUE PROVIDED FOR THAT PURPOSE.

THE PROJECT DESIGNATION, THE CENTERLINE STATION, THE SURVEY POINT, AND THE ELEVATION SHALL BE PERMANENTLY MARKED BY THE USE OF METAL DIES AFTER THE MARKER HAS BEEN INSTALLED.

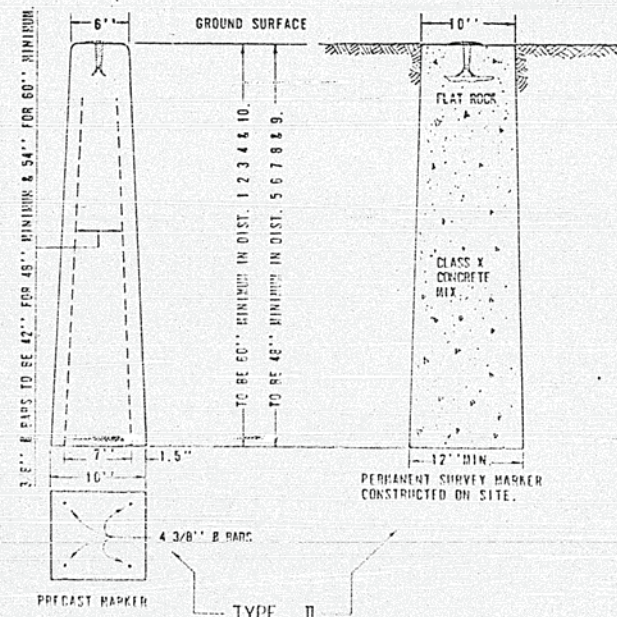
THE CONTRACT UNIT PRICE FOR PERMANENT SURVEY MARKERS WILL BE PAID IN FULL FOR FURNISHING, INSTALLING, AND PERMANENTLY MARKING THE TYPE SPECIFIED.



PURE CEMENT AND WATER OR MELTED SULPHUR USED TO SEAL MARKER TABLET IN ROCK LEDGE, CONCRETE PAVEMENT OR STRUCTURE SET IN HOLE 1.5" IN DIAMETER.

TABLET CONSTRUCTED IN ROCK LEDGE OR CONCRETE. SCALE FULL SIZE

TYPE I



PRECAST MARKER

TYPE II

PERMANENT SURVEY MARKER CONSTRUCTED ON SITE.

STANDARD 2135

STANDARD DESIGN FOR SIGN FOR HIGHWAY IMPROVEMENT

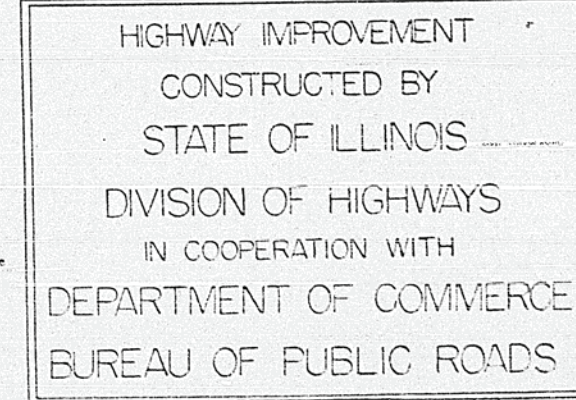
GENERAL NOTES

Signs shall be made of wood (1" lumber rigidly cleated), plywood (5/8" thick), or of metal, (16 gage or equivalent).

The Contractor shall furnish all material and labor for constructing and erecting the signs. The signs shall be placed prior to the starting of actual construction operations. Before any sign is erected it shall be approved by the Engineer as to appearance and quality of construction. The signs shall remain in place and shall be maintained in a satisfactory condition until the project is accepted by the Department. The Contractor shall then remove the signs and the material will become his.

The border shall be black and the letters printed black on a white background. The letters, width of stroke, width of letters and shape shall be Series C of the "Standard Alphabets for Highway Signs, Public Roads Administration, Federal Works Agency, 1945."

The number of signs and their location will be shown on the plans. The cost of the signs, the erection and later removal of the signs shall be incidental to the cost of the construction.



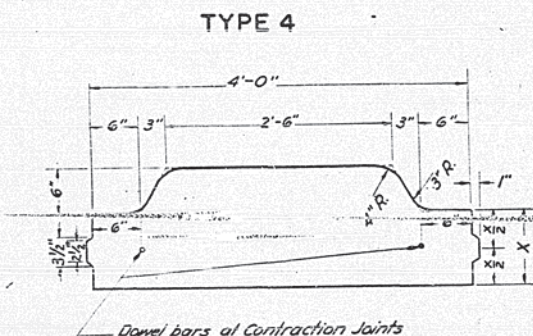
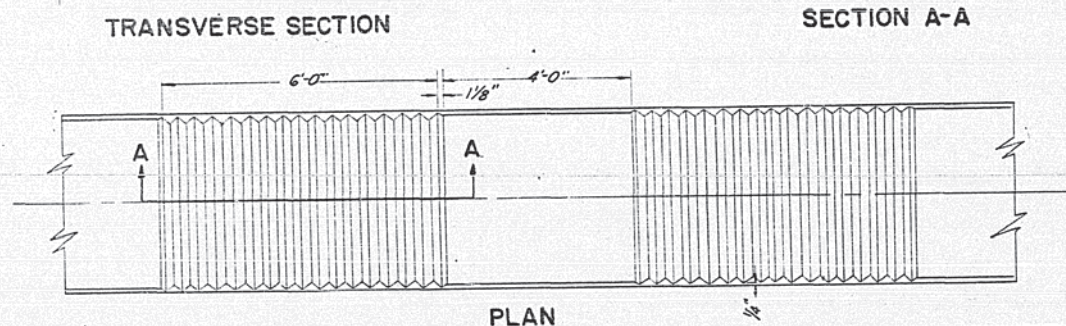
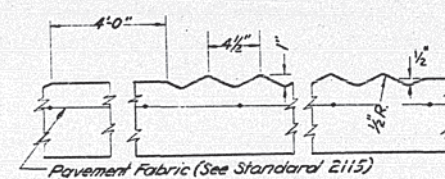
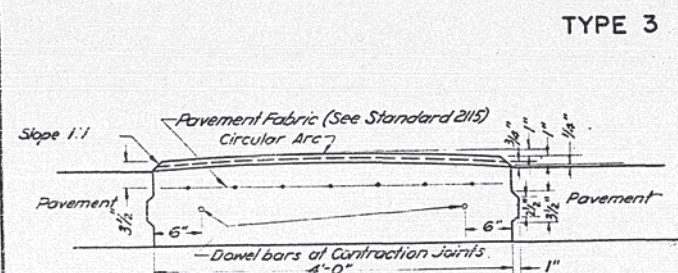
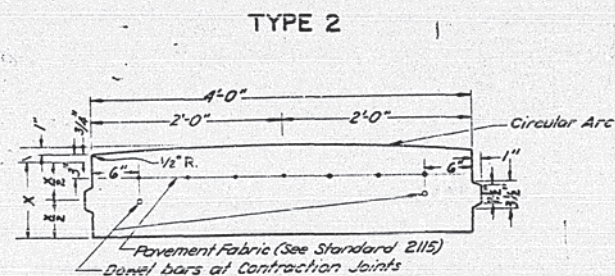
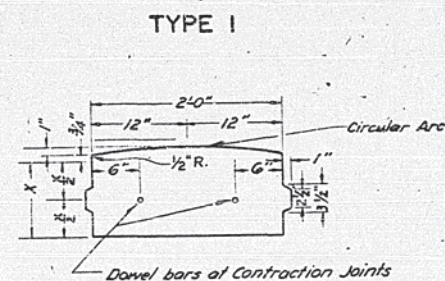
4" x 4" x 12'-0" Unfinished Timber Posts

Ground Line

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS & BUILDINGS DIVISION OF HIGHWAYS	
PASSED	REVISIONS
August 5, 1958	BY DATE
<i>A. H. Ganss</i>	
Engineer of Road Plans and Contracts	
APPROVED August 5, 1958	
<i>C. L. Shantz</i>	
Engineer of Design	

STANDARD 2136

DETAILS OF CONCRETE MEDIANS TYPES 1,2,3 AND 4



Dimension "X" is equal to the adjacent pavement edge thickness or thickness of base course plus the thickness of the bituminous surfacing.

CONTRACTION JOINTS - Contraction Joints of a type similar to that used in the adjacent pavement shall be installed in the Concrete Median in prolongation with the joints in the pavement. The details of the transverse joints in the Concrete Median shall be approved by the Engineer. If Contraction Joints are not provided in the pavement, Contraction Joints will not be required in the Concrete Median. The cost of Contraction Joints including dowel bars shall be included in the contract unit price per lineal foot for the particular type of Concrete Median specified.

The cost of furnishing and installing pavement fabric shall be included in the unit price for each foot for the type of Concrete Median specified.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

PASSED January 3 --- 1935 ---
H. J. Nelson
ENGINEER OF ROAD PLANS AND CONTRACTS

APPROVED January 5 --- 1935 ---
E. Frank
ENGINEER OF L.S.'S

REVISIONS

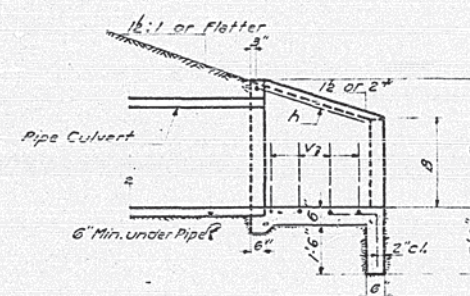
Specification Reference: Section 2.1A

STANDARD 2122

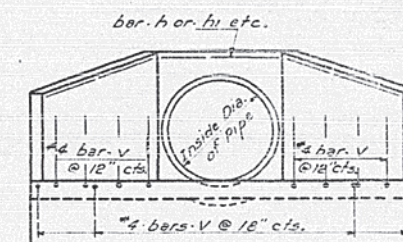
DRAIN 12-14-54 AF

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

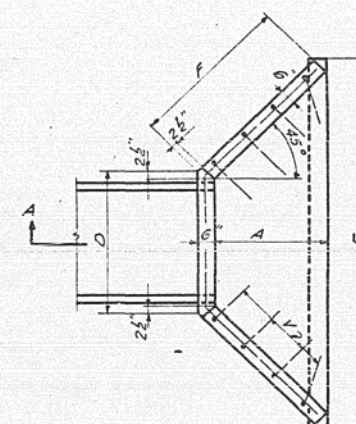
CHERRY 1 CR!



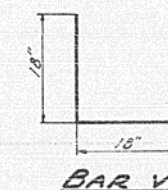
SECTION A-A



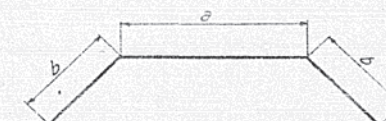
END VIEW



PLAN



BAR



BARS h to h9

Bend in field
One Reg'd in each headwall

Mark	a	b
\hat{h}	1:10"	2:5"
\hat{h}_1	1:10"	3:2"
\hat{h}_2	2:1"	2:5"
\hat{h}_3	2:7"	3:2"
\hat{h}_4	2:9"	3:3"
\hat{h}_5	2:9"	4:16"
\hat{h}_6	3:3"	3:1"
\hat{h}_7	3:3"	4:16"
\hat{h}_8	3:11"	4:6"
\hat{h}_9	3:11"	5:9"

Note:
Class-X Concrete shall
be used throughout.

TABLE OF DIMENSIONS

Design No.	Inside Dia. of Pipe	Slope of Fill	Dimensions						Cl. & Grd. 2.10 H.W. 2.40 V.O.S.	Reinforcement Bars			
			A	B	C	D	E	F		h Bars		V. Bars	Total h
										Mark	Length		
D15-1/2	15"	12:1	1'7"	10"	1'11"	2'0"	5'58"	2'58"	Q.9	h	6'9"	16	40
D15-2	15"	2:1	2'2"	10"	1'11"	2'0"	6'78"	3'58"	2.0	h1	6'3"	22	60
D18-1/2	18"	2:1	1'7"	11"	2'5"	2'0"	2'58"	2'58"	1.0	h2	7'0"	16	40
D18-2	18"	2:1	2'2"	11"	2'2"	2'3"	6'108"	3'38"	1.3	h3	8'6"	22	60
D24-1/2	24"	12:1	2'1"	1'4"	2'9"	2'11"	7'48"	3'2"	1.5	h4	9'3"	22	60
D24-2	24"	2:1	2'10"	1'4"	2'9"	2'11"	8'108"	4'28"	2.0	h5	11'0"	28	70
D30-1/2	30"	12:1	2'6"	1'7"	3'3"	3'5"	8'88"	3'9"	2.0	h6	11'0"	28	70
D30-2	30"	2:1	3'4"	1'7"	3'3"	3'5"	10'48"	4'11"	2.6	h7	13'0"	34	80
D36-1/2	36"	12:1	3'0"	1'10"	3'10"	4'1"	10'48"	4'58"	2.6	h8	13'3"	30	80
D36-2	36"	2:1	4'0"	1'10"	3'10"	4'1"	12'48"	5'108"	3.5	h9	15'6"	40	100

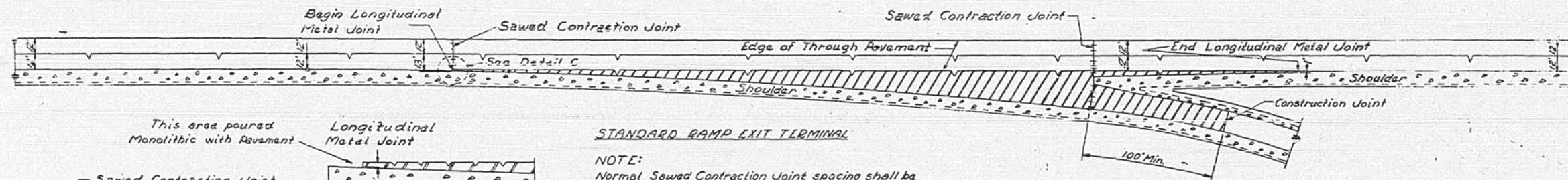
* If embankment slope above headwall is flatter than 2:1, provide wings for 2:1 slope.

OCT. 5 1959
 CLARENCE J. [Signature]
 ENGINEER OF BRIDGES AND TRAFFIC STRUCTURES
 PASSED - [Signature]
 ENGINEER OF DESIGN
 APPROVED [Signature]
 CHIEF HIGHWAY INSPECTOR

M. Miller Sign's. Rev. Nov. 58

REINFORCED CONCRETE HEADWALLS
FOR
15'-18'-24'-30'-& 36" DIAMETER
PIPE CULVERTS
AT RIGHT ANGLES WITH ROADWAY
STANDARD 1976

STANDARD JOINT DETAILS FOR AUXILIARY PAVEMENT AT RAMP TERMINALS

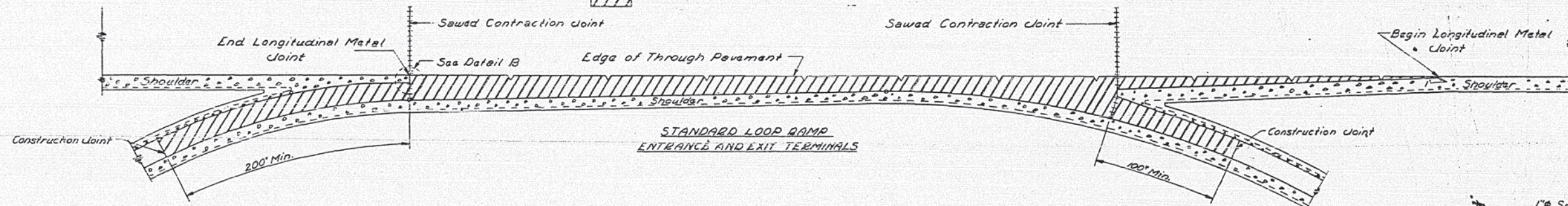
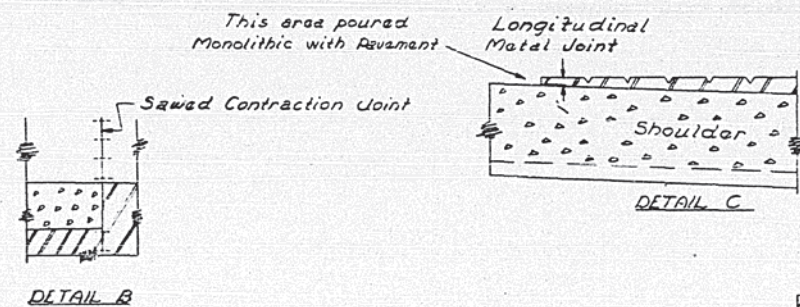


STANDARD RAMP EXIT TERMINAL

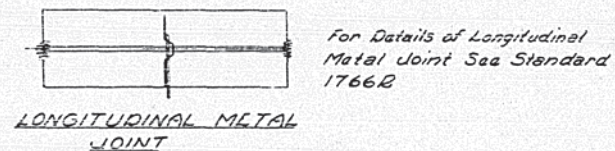
NOTE:
Normal Sawed Contraction Joint spacing shall be used within the limits of Auxiliary Pavement except that, where a normal joint falls within 25 feet of specially located joints shown herein, it shall be eliminated.

The normal Sawed Contraction Joints in the pavement shall be extended through the Auxiliary Pavement.

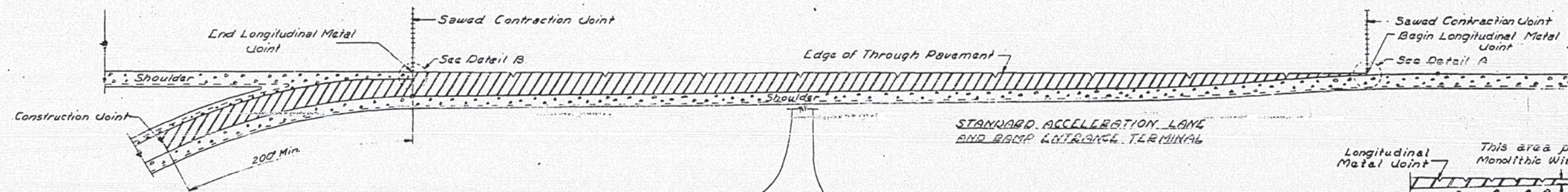
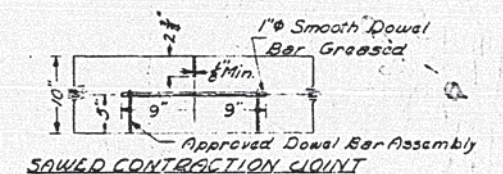
Contrasting Pavement, See Special Provisions



STANDARD LOOP RAMP ENTRANCE AND EXIT TERMINALS

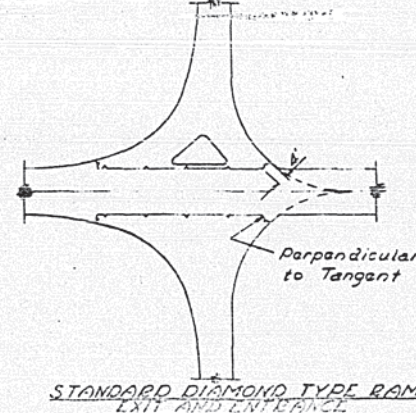
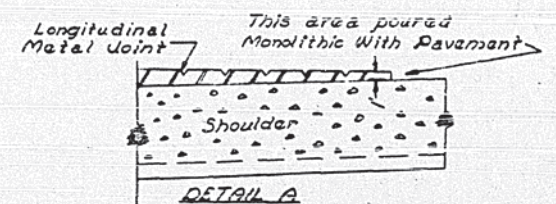


For Details of Longitudinal Metal Joint See Standard 1766B



STANDARD ACCELERATION LANE AND RAMP ENTRANCE TERMINAL

NOTE:
Longitudinal Metal Joint Not Required when abutting Existing Pavement.



STANDARD DIAMOND TYPE RAMP EXIT AND ENTRANCE

STATE OF ILLINOIS		REVISIONS	
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS		BY DATE	
DIVISION OF HIGHWAYS		J.L.B. 11-7-58	
DESIGNED BY		J.T.M. 11-17-58	
CHECKED BY		J.T.M. 1-8-59	
APPROVED BY			
1-8-59			

STANDARD 2129-1

Drawn by W.E.M. 11-17-58

DETAILS OF BRIDGE APPROACHES FOR FEDERAL AID INTERSTATE ROUTES

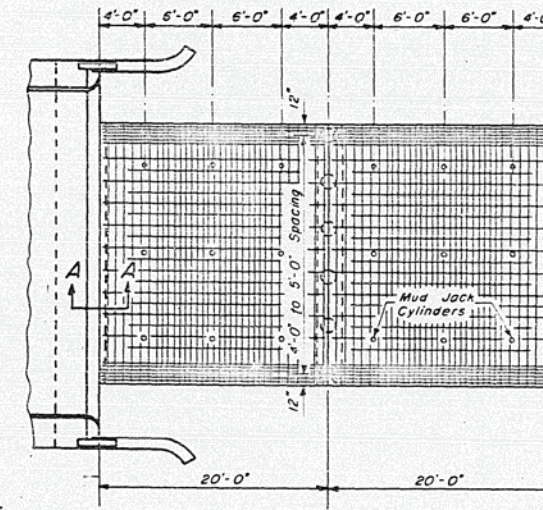
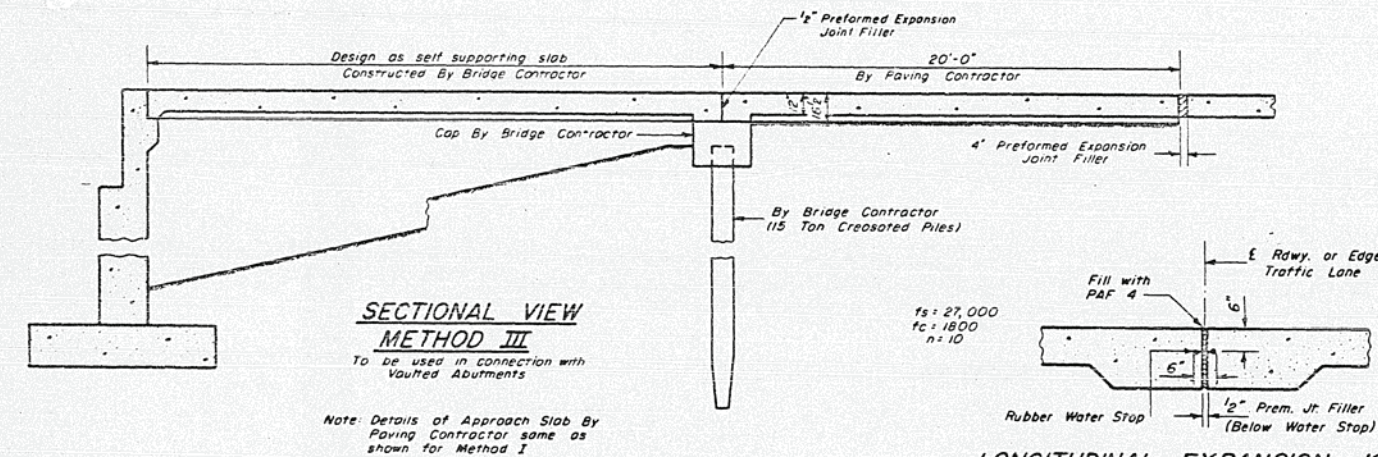
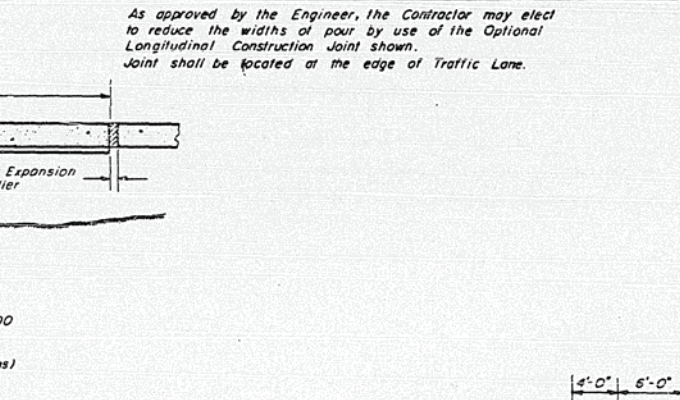
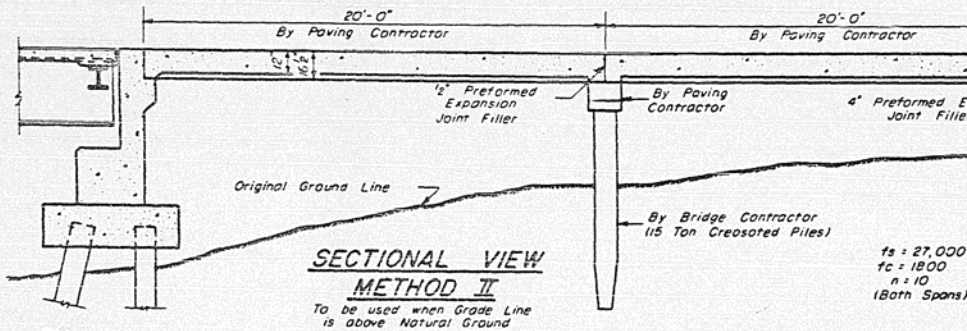
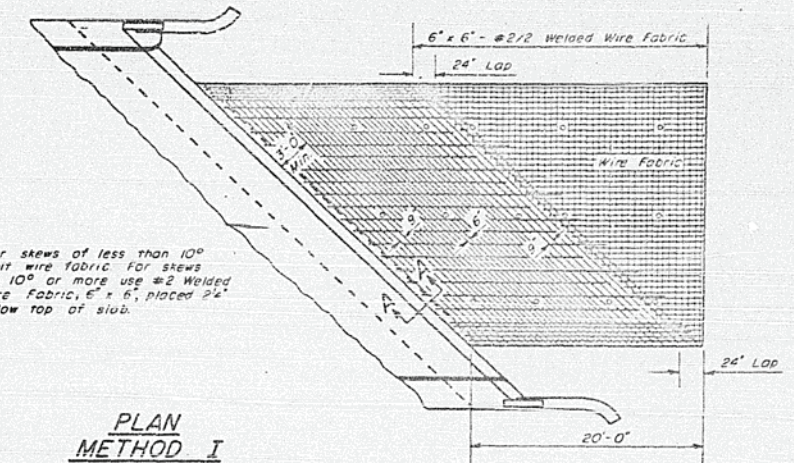
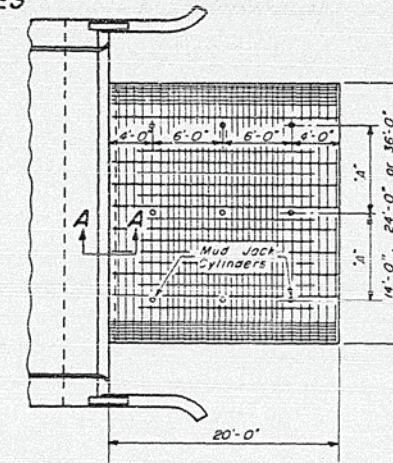
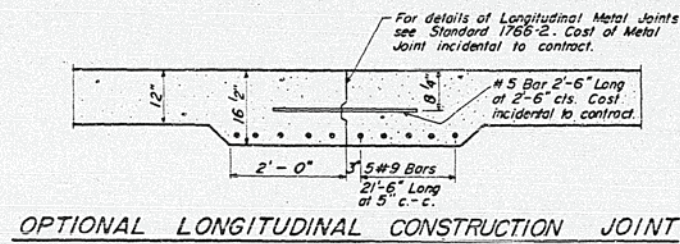
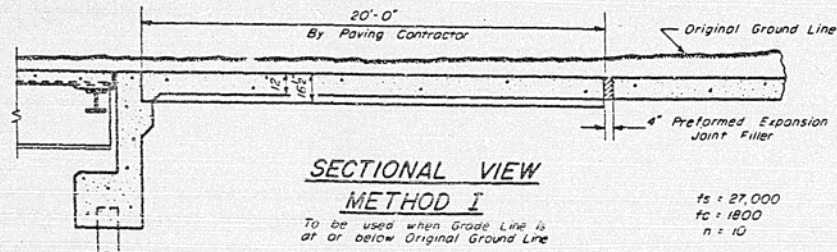
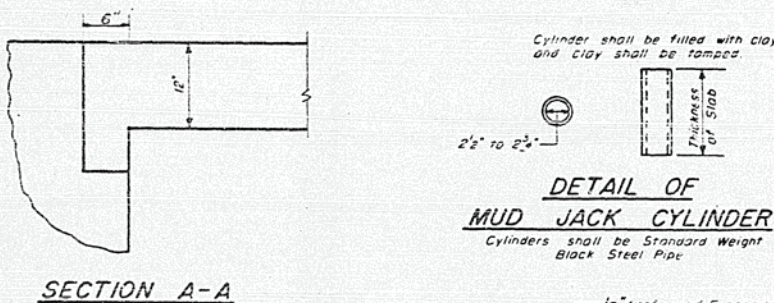
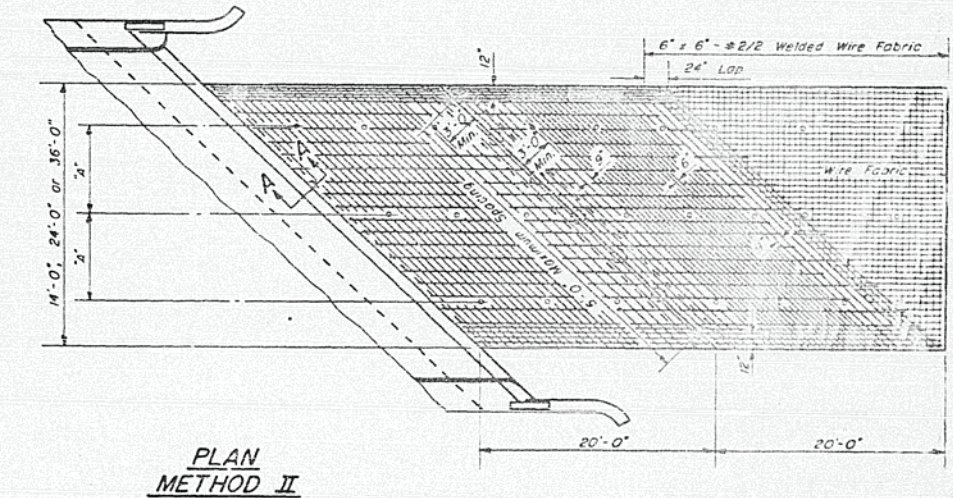


TABLE OF "A" DIMENSIONS

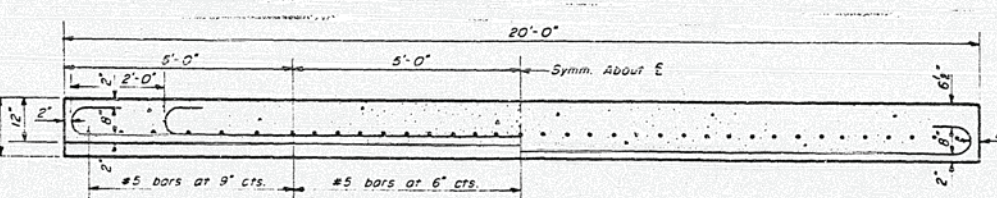
Width of Approach Slab	Dimension "A" (Spacing of Mud Jack Cylinders)
14'	6'-0"
24'	6'-0"
36'	2 Spaces at 8'-0"

Expanded Metal weighing not less than 78 Lbs. per 100 sq.ft. or a welded bar mat weighing not less than 78 Lbs. per 100 sq.ft. having members of equal size in both directions and spaced not over 6" apart may be used instead of the #2 Welded Wire Fabric, 6" x 6", provided the expanded metal or bar mat is furnished at no additional cost to the State.



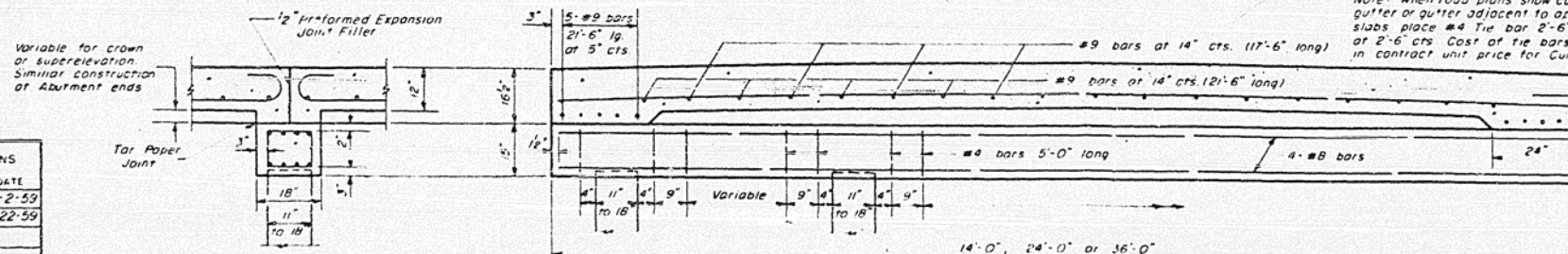
LONGITUDINAL SECTION THRU CENTER OF SLAB

To be used when Approach Slabs are greater than 36'-0" wide. Joint shall be placed at edge of Traffic Lane nearest to the E of the total width of Approach Slab.



LONGITUDINAL SECTION THRU THICKENED EDGE OF SLAB

Note: When road plans show curb and gutter or gutter adjacent to approach slabs place #4 tie bar 2'-6" long at 2'-6" c.c. Cost of the bars included in contract unit price for Curb & Gutter or Gutter.



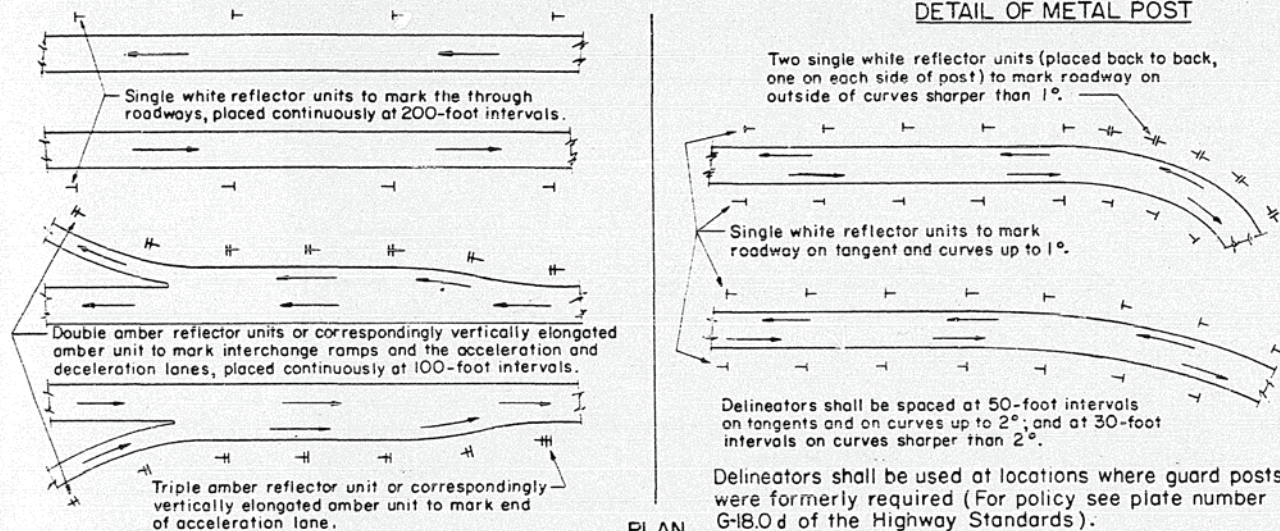
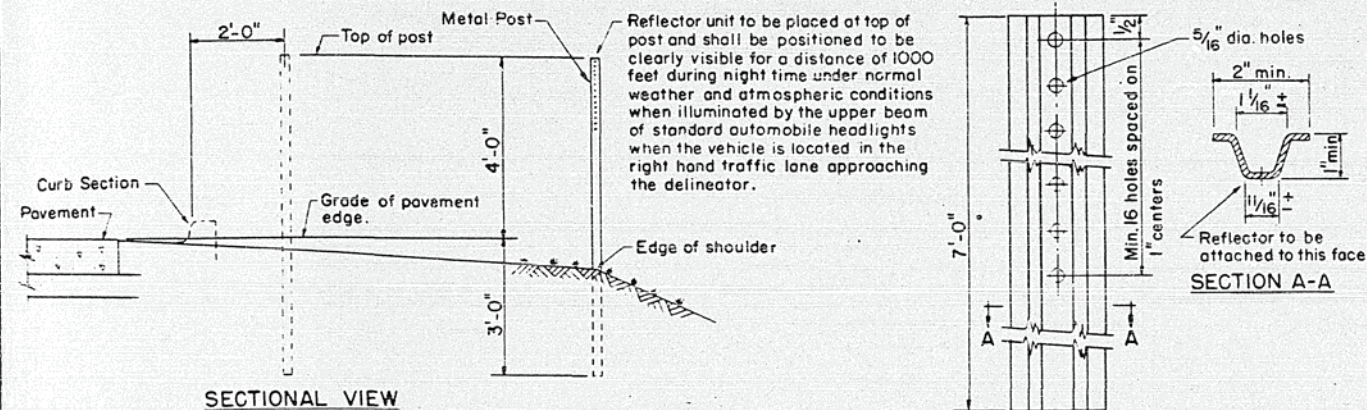
SECTIONAL VIEW OF CONCRETE SLAB AND CAP

GENERAL NOTES

The slab or slabs will be paid for at the contract unit price for PORTLAND CEMENT CONCRETE PAVEMENT (16'-12" to 16'-12"). The concrete cap will be paid for at the contract unit price for CLASS 1 CONCRETE. All Reinforcement Bars, except tie bars for curb and gutter or gutter, will be paid for at the contract unit price for REINFORCEMENT BARS. The Welded Wire Fabric, Mud Jack Cylinders and Preformed Expansion Joint Filler shall be included in the unit price for PORTLAND CEMENT CONCRETE PAVEMENT (16'-12" to 16'-12"). Preformed Expansion Joint Filler shall conform to Section 129 of the Standard Specifications. Width of Bridge Approach Slab pours shall be determined before the reinforcement bars are fabricated. Quantities shown for Reinforcement Bars are for two (2) thickened edges only.

STATE OF ILLINOIS	REVISIONS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS	BY DATE
DIVISION OF HIGHWAYS	
PASSED <u>DEC 18</u> 19 <u>58</u> WBS 2-2-59	
ENGINEER OF BRIDGES AND TRAFFIC STRUCTURES	CET 10-22-59
APPROVED <u>DEC 18</u> 19 <u>58</u>	
ENGINEER OF DESIGN	

STANDARD DESIGN FOR DELINEATORS



FOR INTERSTATE HIGHWAYS

FOR OTHER THAN INTERSTATE HIGHWAYS

PLACEMENT OF DELINEATORS

General Notes

METAL POST: The metal post, galvanized or aluminum, shall conform to the requirements stipulated in the Specifications for Metal Posts For Signs, Markers and Delineators, Serial Number: M15.

REFLECTOR: The reflector unit shall conform to one of the types stipulated in the Specifications for Delineators For Highway Marking, Serial Number: M46. Only one type of reflector will be permitted within the limits of a contract.

HARDWARE: The delineator reflectors shall be fastened to the posts with aluminum bolts and vandal proof nuts conforming to A.S.T.M. Specifications B211, alloy CG 42A-T4, anodically treated in accordance with Navy Specification AN-22-A696.

BASIS OF PAYMENT: This work will be paid for at the contract unit price each for DELINEATORS which price shall include furnishing and driving the metal posts, furnishing and attaching the reflector units to posts, furnishing all hardware, and performing the work as specified. No additional compensation will be allowed for double or triple reflector units nor for two single reflector units placed back to back.

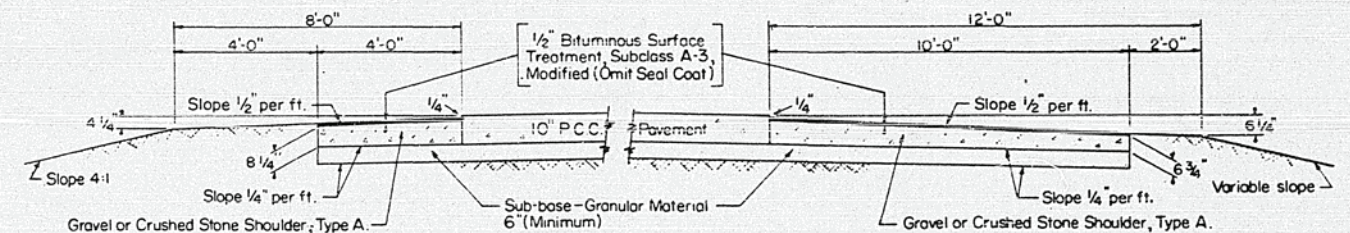
STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS & BUILDINGS DIVISION OF HIGHWAYS			REVISIONS	
PASSED	BY	DATE	BY	DATE
PASSED	June 15, 1960	1960		
APPROVED	June 15, 1960	1960		

STANDARD 2149

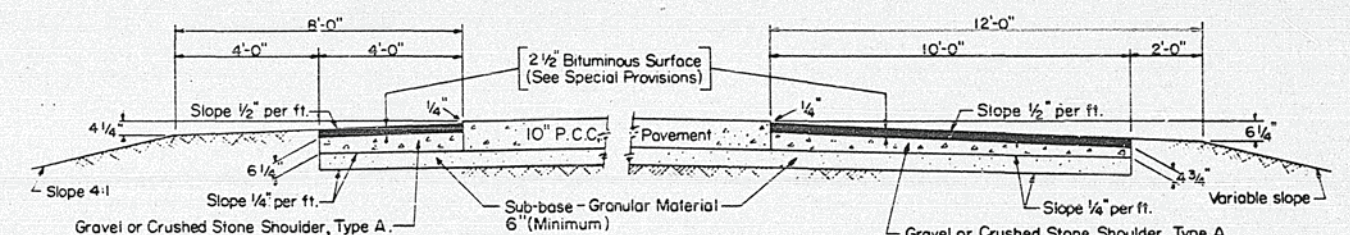
WHF 6-1-60

STANDARD DESIGN FOR BITUMINOUS SURFACED SHOULDERS

DETAIL OF SHOULDERS FOR THROUGH PAVEMENTS

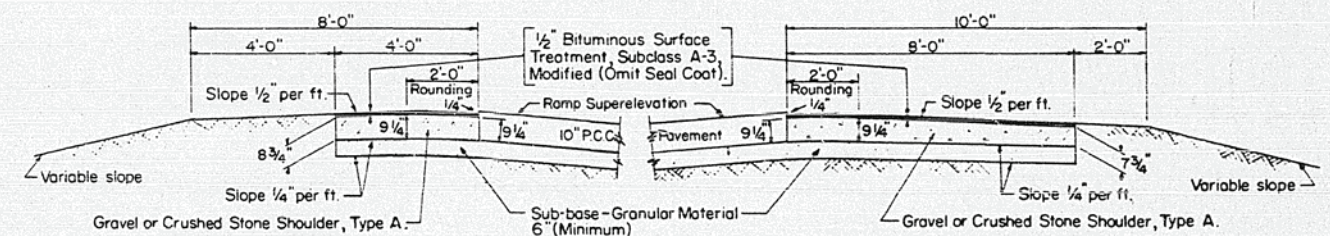


SHOULDERS FOR RURAL AREA



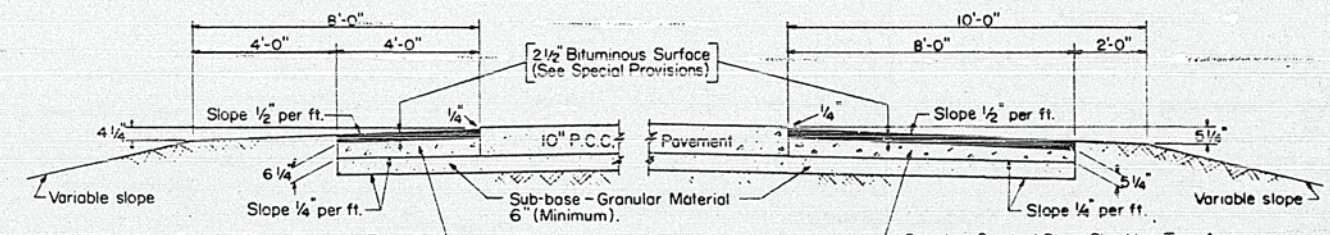
SHOULDERS FOR URBAN AREA

DETAIL OF SHOULDERS FOR RAMPS



NOTE: The 2 foot rounding of shoulder at pavement edge to be omitted where shoulder is along tangent section of ramp or on the inside of a curved ramp. Here the shoulder slope shall be the same as the ramp super-elevation rate with a minimum slope of 1/2 per ft.

SHOULDERS FOR RURAL AREA



SHOULDERS FOR URBAN AREA

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS & BUILDINGS DIVISION OF HIGHWAYS			REVISIONS	
PASSED	BY	DATE	BY	DATE
PASSED	June 1, 1960	1960		
APPROVED	June 1, 1960	1960		

STANDARD 2150

WHF 6-1-60