

11-9-12 LETTING ITEM 003

INDEX OF SHEETS

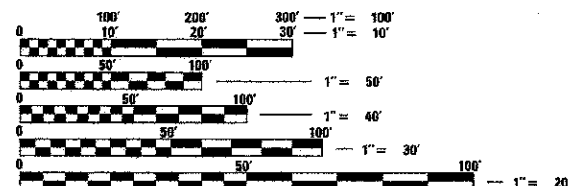
SHEET NO.	DESCRIPTION
1	COVER
2	GENERAL NOTES AND STANDARDS
3	SUMMARY OF QUANTITIES / SCHEDULES
4	TYPICAL SECTIONS
5-8	PROPOSED PLAN & PROFILE
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14-22	CROSS SECTIONS

PROJECT LOCATED IN WILL COUNTY

DESIGN SPEED
20 MPH

CLASSIFICATION
BIKE TRAIL

SPONSORING AGENCY
FOREST PRESERVE DISTRICT OF WILL COUNTY



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

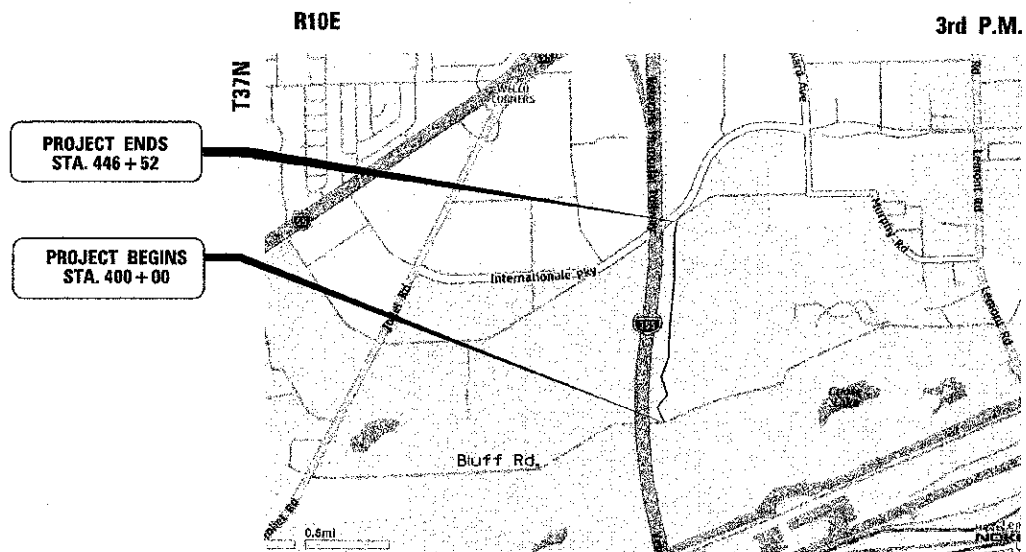
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 (JULIE)
OR 811

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406, SCHAUMBURG, IL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID PROJECT
VETERANS TRAIL NORTH BIKE TRAIL**

BLUFF ROAD TO INTERNATIONALE PARKWAY
SECTION NO. 11-F3000-00-BT
PROJECT NO. TE-00D1(858)
FOREST PRESERVE DISTRICT OF WILL COUNTY
WILL COUNTY
JOB NO: C-91-430-11



PROJECT ENDS
STA. 446 + 52

PROJECT BEGINS
STA. 400 + 00



BM 1:
SOUTHWEST CONCRETE
WINGWALL OF HAUL
ROAD BRIDGE LOCATED
257' WEST OF THE
CENTERLINE OF THE
EXISTING COOK COUNTY
CENTENNIAL TRAIL.

N=1,823,322.18
E=1,067,268.28
EL = 595.82

LICENSED PROFESSIONAL ENGINEER
DAVID D. LANDEWEER
062.042363
STATE OF ILLINOIS

Signature: *[Signature]*
DATE: 6/26/12

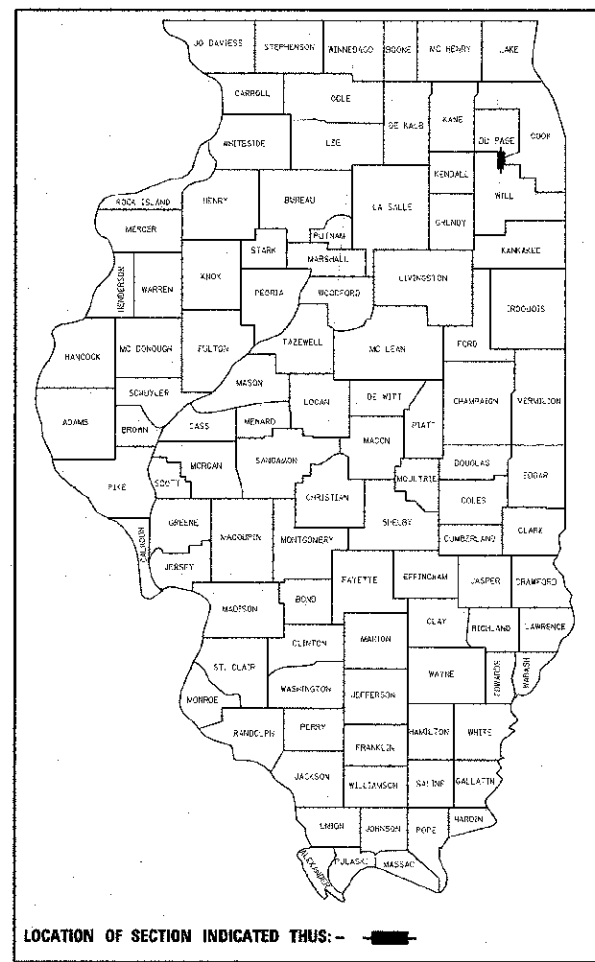
LICENSE EXPIRES 11/30/2013

Sec. 13 & 24, DuPage Township LOCATION MAP
(NOT TO SCALE)

GROSS LENGTH AND NET LENGTH = 4,652 FEET (0.88 MILES)

CONTRACT NO. 63728

F.A. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
	11-F3000-00-BT	WILL	27 1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 63728	
		NOVEMBER	2012 LETTING



LOCATION OF SECTION INDICATED THUS: —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: June 27 2012
Karen Forte, CEA
FOREST PRESERVE DISTRICT OF WILL COUNTY

Passed: August 23 2012
[Signature]
DISTRICT #1 ENGINEER OF LOCAL ROADS AND STREETS

Releasing for Bid
Based on Limited
Review: August 23 2012
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION #1 ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

PLANS PREPARED BY:
URS
100 SOUTH WACKER DRIVE, SUITE 500
CHICAGO, IL 60606
TEL. (312) 939-1000
FAX (312) 939-4198

PLAN NOTES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 (811) FOR FIELD LOCATIONS OF BURIED UTILITY FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).

ALL STATION OFFSET CALL OUTS AND CURVE DATA ON THE PLANS REFER TO THE PROPOSED CENTERLINE, UNLESS OTHERWISE SHOWN.
IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS

SPECIFICATIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN ILLINOIS", JULY 2009 EDITION, THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", LATEST EDITION, THE DETAILS INCLUDED IN THE PLANS, AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

COORDINATION

THE CONTRACTOR SHALL NOTIFY THE FOREST PRESERVE DISTRICT OF WILL COUNTY (815-722-9412) AND THE VILLAGE OF WOODRIDGE (630-719-2940) AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK, AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.

PUBLIC OR PRIVATE UTILITIES

THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE, AND THE DEPARTMENT DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATIONS OF UTILITIES AND EXERCISE CARE DURING CONSTRUCTION OPERATIONS TO AVOID DAMAGE, IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 107.20 OF THE "STANDARD SPECIFICATIONS". THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE UTILITY OWNERS SO THAT THEIR FACILITIES MAY BE ADJUSTED OR RELOCATED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS UNLESS OTHERWISE NOTED IN THE PLANS. ALL RELOCATION WORK ON EXISTING PRIVATE UTILITIES WILL BE DONE BY THE OWNER OF THAT UTILITY.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR DESTRUCTION OF PUBLIC OR PRIVATE PROPERTY, AND SHALL RESTORE SUCH PROPERTY AT HIS/HER OWN EXPENSE.

SURVEY MONUMENTS

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE MONUMENTS ARE REMOVED.

DISPOSAL OF MATERIALS

THE CONTRACTOR WILL BE REQUIRED TO DISPOSE OF ALL SIDEWALK, CURB AND GUTTER, PAVEMENT, AND ALL OTHER MATERIALS EXCAVATED OR REMOVED DUE TO CONSTRUCTION OPERATIONS WHICH SHALL BE INCLUDED IN THE COST OF THE INDIVIDUAL ITEMS. NO PAYMENT WILL BE MADE FOR HAULING OR TRUCKING MATERIAL TO LOCATIONS, PROVIDED BY THE CONTRACTOR, OUTSIDE THE LIMITS OF THE IMPROVEMENT.

EXISTING DRAINAGE STRUCTURES

DURING CONSTRUCTION OPERATIONS, WHENEVER ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED BY THE CONTRACTOR AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT AND WILL NOT BE PAID AS A SEPARATE ITEM.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS, AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT SHALL BE INCLUDED IN THE CONTRACT.

UNSUITABLE MATERIAL

BEFORE REMOVAL OF ANY UNSUITABLE MATERIAL, THE CONTRACTOR SHALL TREAT THE SUBGRADE AS PER ARTICLE 301.03 OF THE "STANDARD SPECIFICATIONS" TO THE SATISFACTION OF THE ENGINEER. UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT OR FILL UNDER THE PROPOSED PATH AS SHOWN ON THE TYPICAL CROSS SECTIONS.

STOCKPILES

STOCKPILES OF TOPSOIL AND OTHER MATERIALS SHALL NOT BE LOCATED WITHIN A SPECIAL MANAGEMENT AREA. APPROVAL OF THE LOCATION MUST BE OBTAINED FROM THE ENGINEER PRIOR TO PLACEMENT. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, THE EROSION CONTROL MEASURES SHALL BE PROVIDED. STOCKPILES TO REMAIN IN PLACE FOR 30 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.

TREE TRIMMING

ALL BRANCHES THAT ARE LESS THAN 12 FEET ABOVE THE SURFACE OF THE TRAIL SHALL BE REMOVED BY THE CONTRACTOR. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH ARTICLE 201.05(G) FOR WOODY PLANT MAINTENANCE, TREE CARE OPERATIONS, PRUNING, TRIMMING, REPAIRING, MAINTAINING AND REMOVING TREES AND CUTTING BRUSH. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION AND WILL NOT BE PAID FOR AS SEPARATE ITEMS.

ACCESS TO ABUTTING PROPERTY

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ABUTTING PROPERTY DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION AS APPROVED BY THE ENGINEER.

SAW CUTTING

THE LIMITS OF REMOVAL OF ALL CONCRETE OR BITUMINOUS PAVEMENTS, CURBING OR SIDEWALK ADJACENT TO EXISTING LIKE PAVEMENTS, CURBING OR SIDEWALKS SHALL BE SAWCUT IN ACCORDANCE WITH SECTION 440 OF THE "STANDARD SPECIFICATIONS" AND AT THE DIRECTION OF THE ENGINEER. THE SAW CUTTING OF BITUMINOUS PAVEMENT, DRIVEWAYS, CURBING OR SIDEWALK SHALL BE INCLUDED IN THE CONTRACT AND WILL NOT BE PAID FOR AS SEPARATE ITEMS.

SIDEWALKS

SIDEWALK RAMP FOR THE HANDICAPPED SHALL BE PROVIDED AT ALL ROADWAY AND CURBED DRIVEWAY INTERSECTIONS AS SHOWN IN THE STANDARDS.

ADJACENT PAVEMENT OR SIDEWALK DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO THE ORIGINAL CONDITION BY THE CONTRACTOR. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE CONTRACT.

RESTORATION

ALL DISTURBED AREA OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST.

GRASS AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESTORED WITH SIX INCHES TOPSOIL AND SEED OR SOD AS SPECIFIED.

ACCESS

THE CONTRACTOR SHALL BE AWARE OF POTENTIAL LIMITED ACCESS TO PORTIONS OF THE PROJECT. TEMPORARY DRAINAGE CROSSINGS APPROVED BY THE ENGINEER MAY BE INSTALLED BY THE CONTRACTOR AT HIS/HER EXPENSE TO GAIN ACCESS FOR CONSTRUCTION. THE CONTRACTOR MAY ALSO APPROACH ADJACENT OWNERS FOR THE PURPOSE OF OBTAINING ADDITIONAL ACCESS. SUCH NEGOTIATIONS, HOWEVER, ARE STRICTLY BETWEEN PROPERTY OWNERS AND THE CONTRACTOR. ANY ADDITIONAL COSTS INCURRED BY THE CONTRACTOR AS A RESULT SHALL BE AT HIS/HER EXPENSE.

SEDIMENTATION AND EROSION CONTROL NOTES

- A. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- B. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE.
- C. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DISTURBED AREAS WITHIN 14 CALENDAR DAYS OF THE END OF ACTIVE HYDROLOGIC DISTURBANCE. PERMANENT STABILIZATION SHALL BE DONE WITHIN 14 DAYS AFTER COMPLETION OF FINAL GRADING OF THE SOIL.
- D. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED, OR OTHERWISE TREATED TO REMOVE SEDIMENT.
- E. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- F. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE GENERAL CONTRACTOR WILL BE RESPONSIBLE FOR INSPECTION AND REPAIR DURING CONSTRUCTION.
- G. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.
- H. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL PRIOR TO THE START OF ANY EARTHWORK.
- I. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN THREE DAYS, EROSION CONTROL MEASURES SHALL BE PROVIDED.
- J. EROSION CONTROL MEASURES SHALL COMPLY WITH THE MINIMUM REQUIREMENTS OF THE WILL COUNTY STORMWATER AND FLOODPLAIN ORDINANCE SPECIFICATIONS AT ALL TIMES.

CONSTRUCTION ACCESS LOCATIONS

THE CONSTRUCTION ACCESS LOCATIONS SHALL AVOID WETLAND AND WETLAND BUFFER AREAS. IN ADDITION, NO ACCESS WILL BE ALLOWED FROM THE VETERANS TOLLWAY.

HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 280001-06 TEMPORARY EROSION CONTROL SYSTEMS
- 424001-06 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
- 542301-03 PRECAST REINFORCED CONCRETE FLARED END SECTION
- 606001-04 CONCRETE CURB TYPE B & COMBINATION CONCRETE CURB AND GUTTER
- 701301-04 LANE CLOSURE, 2L, 2S, SHORT TIME OPERATIONS
- 701901-02 TRAFFIC CONTROL DEVICES
- 720001-01 SIGN PANEL MOUNTING DETAILS
- 720006-03 SIGN PANEL ERECTION DETAILS
- 720011-01 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 729001-01 APPLICATION OF TYPE A AND B METAL POSTS (FOR SIGNS & MARKERS)

DISTRICT 1 STANDARDS

- TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS

TOLLWAY STANDARDS

- D1 01 RIGHT OF WAY FENCE

ILLINOIS TOLLWAY REQUIREMENTS

NO ACCESS WILL BE ALLOWED FROM THE TOLLWAY.

THE CONTRACTOR SHALL COMPLY WITH THE INDEMNIFICATION PROVISION CONTAINED IN THE CURRENT TOLLWAY SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION.

THE CONTRACTOR SHALL INCLUDE THE TOLLWAY AND THE VILLAGE OF WOODRIDGE AS ADDITIONAL INSURED PARTIES IN THEIR GENERAL LIABILITY INSURANCE FOR THE PROJECT. THE TOLLWAY AND THE VILLAGE SHALL ALSO BE ADDED AS AN ADDITIONAL PROTECTED PARTY ON ALL PERFORMANCE BONDS PROVIDED BY THE CONTRACTOR.

COMMITMENTS

THE CONTRACTOR SHALL INSTALL THE TOLLWAY FENCE PRIOR TO PERFORMING ANY OTHER WORK EXCEPT FOR THE CROSS CULVERT AT STATION 426+50. UPON COMPLETION OF THAT CULVERT, THE CONTRACTOR SHALL COMPLETE THE INSTALLATION OF THE FENCE AT THAT LOCATION. ANY DAMAGE TO THE FENCE DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST PRIOR TO FINAL ACCEPTANCE.

BENCH MARK

P.K. NAIL IN PAVED SHOULDER, S. SIDE OF BLUFF ROAD, 83.4' W OF CENTERLINE ELEV = 609.61 USGS DATUM



USER NAME: David.Lendrum	DESIGNED	REVISED
PLT: SCALE: 1/8"=1'-0"	DRAWN	REVISED
PLT: DATE: 6/29/2012	CHECKED	REVISED
	DATE: 6/20/12	REVISED

FOREST PRESERVE DISTRICT OF WILL COUNTY, ILLINOIS

**VETERANS TRAIL - NORTH
GENERAL NOTES AND STANDARDS**

SCALE:	SHEET NO.	DF	SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-F3000-00-BT	WILL	27	2
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 63728				

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY 0028
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	649
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	1,257
20101100	TREE TRUNK PROTECTION	EACH	20
20101200	TREE ROOT PRUNING	EACH	50
20200100	EARTH EXCAVATION	CU YD	4,163
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	4,784
20400800	FURNISHED EXCAVATION	CU YD	913
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	7,373
21101825	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	17,910
25000115	SEEDING, CLASS 1B	ACRE	0.5
25000312	SEEDING, CLASS 4A	ACRE	3.2
25000350	SEEDING, CLASS 7	ACRE	3.7
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	333
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	333
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	333
25100630	EROSION CONTROL BLANKET	SQ YD	17,910
28000400	PERIMETER EROSION BARRIER	FOOT	4,693
28000500	INLET AND PIPE PROTECTION	EACH	7
28100103	STONE RIPRAP, CLASS A2	SQ YD	42
28200200	FILTER FABRIC	SQ YD	42
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	1,030
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	6,181
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	124
40603335	HOT-MX ASPHALT SURFACE COURSE, MIX "D", N50	TON	692
42001300	PROTECTIVE COAT	SQ YD	21

CODE NO.	ITEM	UNIT	TOTAL QUANTITY 0028
42400300	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH	SQ FT	187
42400800	DETECTABLE WARNINGS	SQ FT	28
542A0217	PIPE CULVERTS, CLASS A, TYPE 1 12"	FOOT	48
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	284
542A0223	PIPE CULVERTS, CLASS A, TYPE 1 18"	FOOT	70
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	2
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	10
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	2
60600605	CONCRETE CURB, TYPE B	FOOT	27
* 66400305	CHAIN LINK FENCE, 6'	FOOT	1,870
67100100	MOBILIZATION	L SUM	1
72000100	SIGN PANEL - TYPE 1	SQ FT	18.0
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	2
72900100	METAL POST - TYPE A	FOOT	72
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	5,441
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	20
Z0022800	FENCE REMOVAL	FOOT	2,275
* X0321322	DROP GATE	EACH	1
* X0326895	FENCE CORNER POST	EACH	6
X2010507	CLEARING, SPECIAL	ACRE	2
* Z0020210	PULL POST ARRANGEMENT	EACH	2
* XX008728	SOLAR-POWERED LED FLASHING WARNING SIGN	EACH	2

TREE REMOVAL SCHEDULE

STATION	OFFSET		6-15 UNITS	>15 UNITS
	LT	RT		
400+03	19.5		12	
400+13	5.5		12	
400+16		5.0		18
400+33	27.5			18
400+28	1.0		12	
400+36	7.5		6	
400+42	2.0		6	
400+55		16.5		36
400+66		25.5		48
400+81	25.5		6	
400+86		24.0	12	
400+88		22.0	15	
401+03		13.5		36
401+11	13.5			24
401+14	5.5		12	
401+39		18.0		24
401+41		30.0		24
401+57	15.5			36
401+68	25.5			38
401+93		12.0	14	
402+14		7.0	0.5	
402+39	18.5			37
402+50		22.0	12	
402+71		6.0	12	
402+75	5.5		8	
402+86		22.5	10	
402+89	10.5		14	
403+28		24.0	10	
403+46		12.0		52
404+79	27.0			28
404+91	19.5		11	
404+92	6.5		6	
404+93		10.0		24
404+94		23.0	10	
405+09		14.0		24
405+23		7.0	8	
405+33	20.0			26
405+40	35.0		10	
405+49	2.5		8	
405+56		3.5	8	
405+61	27.0		15	
405+89		14.0		21
405+96	20.0			34
406+14	0.0		8	
406+15	27.0			18
406+29	7.5		8	
406+48	30.0		13	
406+70	18.0		15	
406+79		18.5	10	
407+03	11.5		10	
407+11		23.0	11	
407+22		7.0	10	
407+25		16.5	12	
407+44		8.0		36
407+47		4.5		26
407+62		2.5		22
407+89	2.0			26
408+29	18.5			22
408+81		33.0	12	
408+96		30.0	8	
408+99		25.0	14	

STATION	OFFSET		6-15 UNITS	>15 UNITS
	LT	RT		
409+00		36.5	10	
409+23		33.0	0.5	
409+40		14.5		24
409+42		27.0		32
409+78		16.0		36
410+45		23.0		19
410+66		19.0		24
410+84		11.0		24
411+67		14.5		21
412+12	10.5			38
412+16	13.5			36
412+21		20.0		18
412+28		1.0		24
412+42	7.5			18
413+29		7.5		30
413+46		17.5	0.5	
413+58		7.5	0.5	
413+69	0.0		12	
413+78		2.0	0.5	
413+89	3.5		0.5	
413+96	8.0		6	
413+98	4.0		0.5	
414+02		15.0		28
414+03	6.0		15	
414+14	17.5		0.5	
414+35	12.0		12	
414+38	14.0		12	
421+62	23.5		15	
421+89	23.0			16
422+06		6.0	8	
422+07	0.0		15	
422+39		5.0		21
422+40	27.0		8	
422+41	19.5		12	
422+42	4.0		8	
422+48	12.5		10	
422+58	22.5		2	
422+83		2.5	10	
422+70	4.5		12	
443+65	11.5		12	
444+00	1.0		12	
444+46	2.5		15	
444+63		3.5		18
444+86		11.5		18
10% GROWTH			59	114
SUBTOTAL			649	1257

EARTHWORK SCHEDULE

Earth Excavation (Suitable Material)	Earth Excavation Adjusted for Shrinkage (25%)	Suitable Embankment	Earthwork Balance (Furnished Excavation)	Unsuitable Excavation
CY	CY	CY	CY	CY
4131	3,098	4,028	(929)	5,022

* SPECIALTY ITEM

FILE NAME: D:\N\11\PD_25357\BMS_Cement\BMS_Traffic\Construction\Subcontract\Segment_3\BMS\traff\1101.dwg



USER NAME: David.Landwehr
 PLOT SCALE: 1/8"=1'-0"
 PLOT DATE: 8/21/2012

DESIGNED -
 DRAWN -
 CHECKED -
 DATE: 8/20/12

REVISED -
 REVISED -
 REVISED -
 REVISED -

FOREST PRESERVE DISTRICT
 OF WILL COUNTY, ILLINOIS

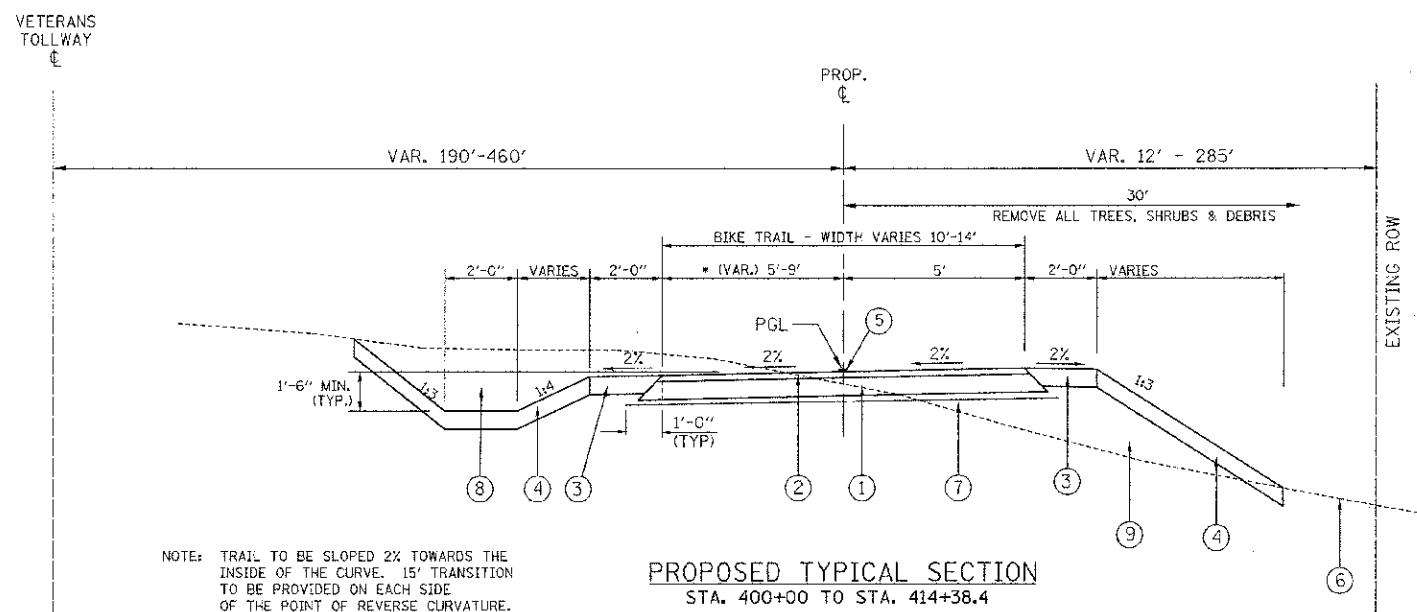
VETERANS TRAIL - NORTH
 SUMMARY OF QUANTITIES / SCHEDULES

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-F3000-00-BT	WILL	27	3
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63728	

LEGEND:

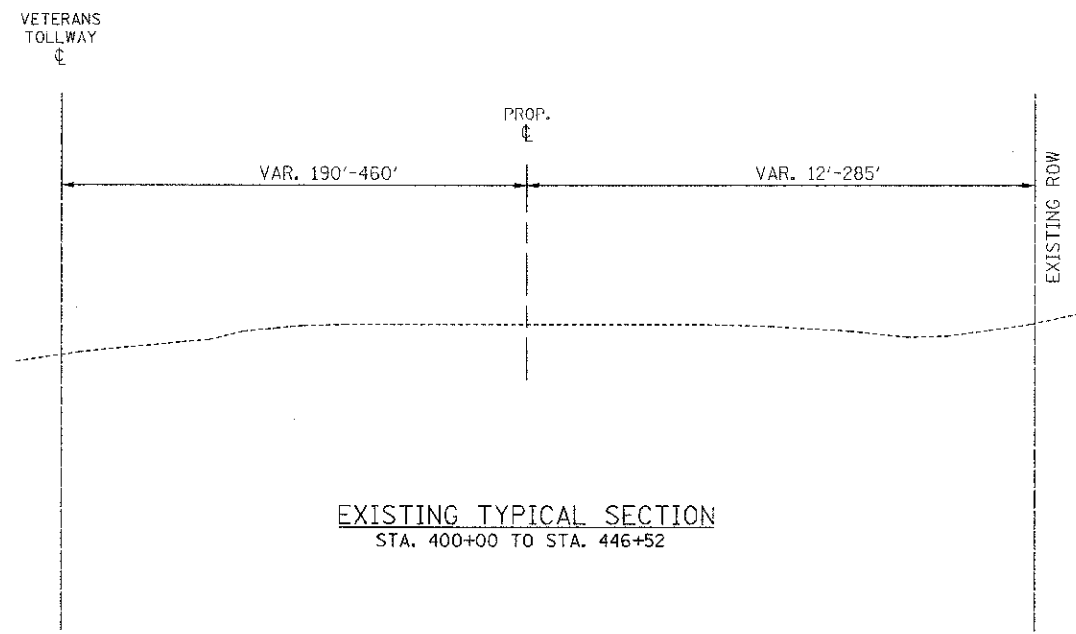
- ① AGGREGATE BASE COURSE, TYPE B, 8"
- ② HOT-MIX ASPHALT SURFACE COURSE, MIX "D", NSO, 2"
- ③ TOPSOIL FURNISH AND PLACE, 6", AND SEEDING, CLASS 1B
- ④ TOPSOIL FURNISH AND PLACE, 6", AND SEEDING, CLASS 4A
- ⑤ PAINT PAVEMENT MARKING, 4" YELLOW
- ⑥ EXISTING GROUND
- ⑦ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ⑧ EARTH EXCAVATION
- ⑨ SUITABLE EMBANKMENT MATERIAL (NO TOPSOIL)



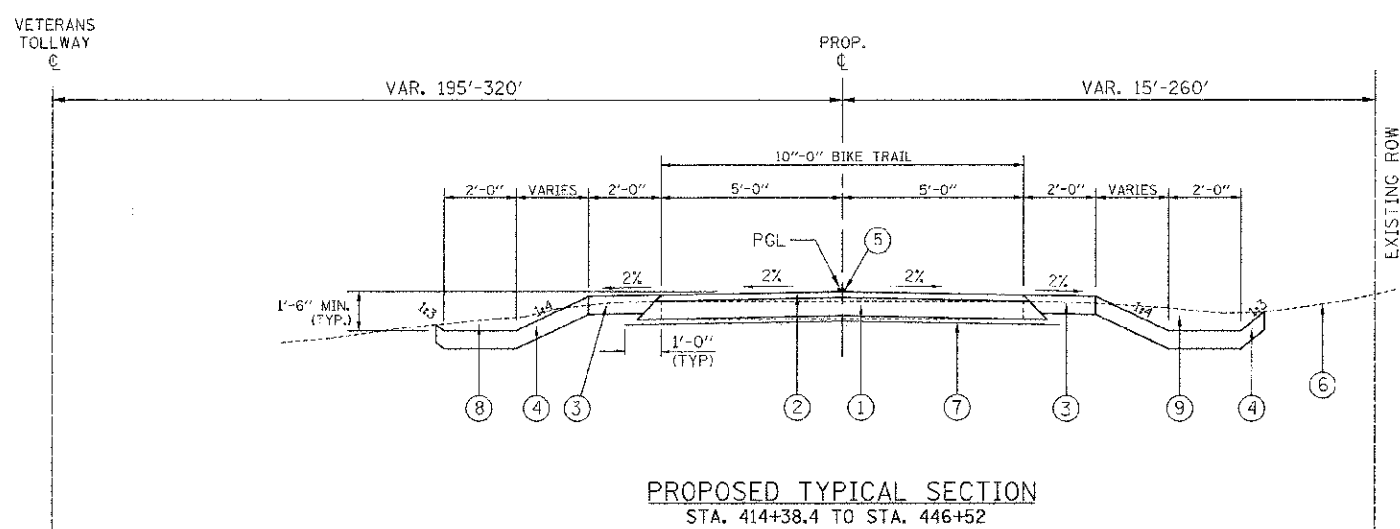
NOTE: TRAIL TO BE SLOPED 2% TOWARDS THE INSIDE OF THE CURVE. 15' TRANSITION TO BE PROVIDED ON EACH SIDE OF THE POINT OF REVERSE CURVATURE.

- 9' STA. 400+00.0 TO 400+58.7
- 8' STA. 400+58.7 TO 401+85.6
- 7' STA. 401+85.6 TO 402+47.7
- 6' STA. 402+47.7 TO 414+38.4

PROPOSED TYPICAL SECTION
STA. 400+00 TO STA. 414+38.4



EXISTING TYPICAL SECTION
STA. 400+00 TO STA. 446+52



PROPOSED TYPICAL SECTION
STA. 414+38.4 TO STA. 446+52

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ITEM	AIR VOIDS @ NDES
HOT-MIX ASPHALT SURFACE COURSE MIX "D", NSC (IL 9.5mm; 2")	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/50 YD³/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

- NOTE 1:**
WHERE UNSUITABLE MATERIAL UNDERLIES THE BIKE TRAIL, SHOULDERS AND/OR EMBANKMENT, AS DETERMINED BY THE ENGINEER, THE SUBGRADE TREATMENT WILL CONSIST OF EXCAVATION OF SUCH UNSUITABLE MATERIAL TO A DEPTH 12" BELOW AGGREGATE BASE COURSE AND PLACEMENT OF 12" OF POROUS GRANULAR EMBANKMENT SUBGRADE AND A GEOTECHNICAL FABRIC FOR GROUND STABILIZATION.
- NOTE 2:**
WHERE THE TRAIL RUNS ADJACENT TO BLUFF ROAD, THE GROUND AREA BETWEEN THE TRAIL AND BLUFF ROAD SHALL BE SHAPED TO ELIMINATE ANY SWALES, DITCHES, OR LOW SPOTS.
- NOTE 3:**
ADDITIONAL FILL MATERIAL TO BE PLACED ABOVE FABRIC IN CUT SECTIONS SHALL NOT BE MEASURED FOR PAYMENT. MATERIAL SHALL BE SUITABLE EMBANKMENT MATERIAL.
- NOTE 4:**
AT ALL INTERSECTIONS OF BIKE PATH AND ROADWAYS, DEPRESS THE CURB (IF APPLICABLE) AND MAINTAIN EXISTING PAVEMENT ELEVATIONS.



USER NAME	DESIGNED
PLLOT SCALE - 50.0000' / 1" IN	DRAWN
PLGT DATE - 8/21/2012	CHECKED
	DATE - 8/20/12

REVISED
REVISED
REVISED
REVISED

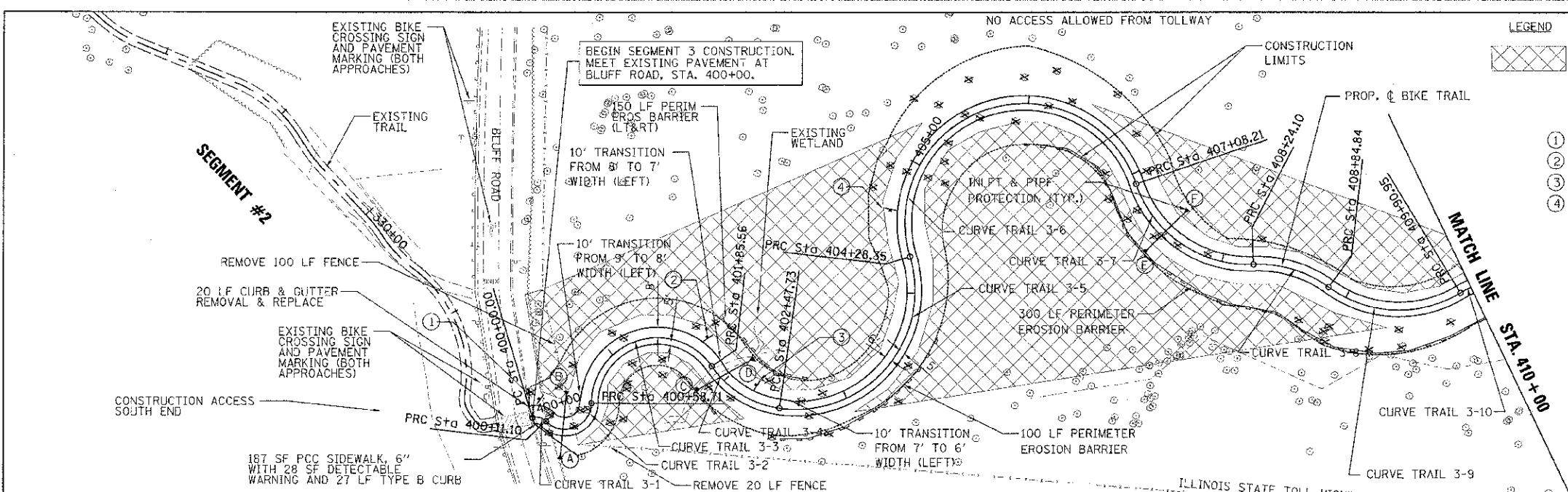
**FOREST PRESERVE DISTRICT
OF WILL COUNTY, ILLINOIS**

**VETERANS TRAIL - NORTH
TYPICAL SECTIONS**

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
	11-F3000-00-BT	WILL	27 4
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT
			CONTRACT NO. 63728

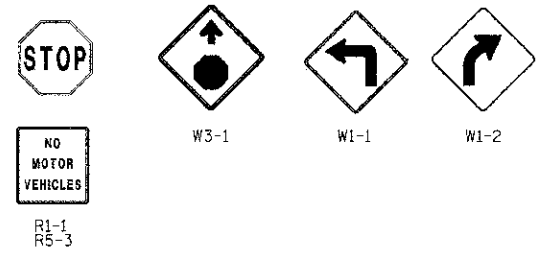
DATE	
SCALE	
PROJECT	
NO. OF SHEETS	
DATE	
BY	
CHECKED	
DATE	
BY	
CHECKED	
DATE	



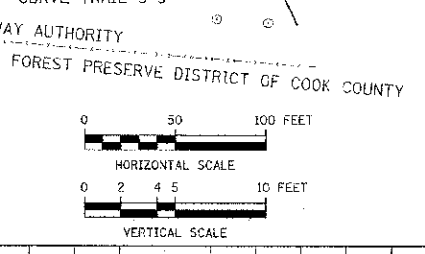
LEGEND

SELECTIVE CLEARING AREA. ALL SHRUBS AND MOST TREES LESS THAN 6" IN SHALL BE REMOVED. REMOVAL SHALL CONSIST OF CUTTING THE TRUNK AT A HEIGHT OF 2" ABOVE GRADE AND THEN APPLYING HERBICIDE. ENGINEER WILL MARK TREES TO REMAIN. THIS WORK WILL BE PAID FOR PER ACRE OF CLEARING, SPECIAL.

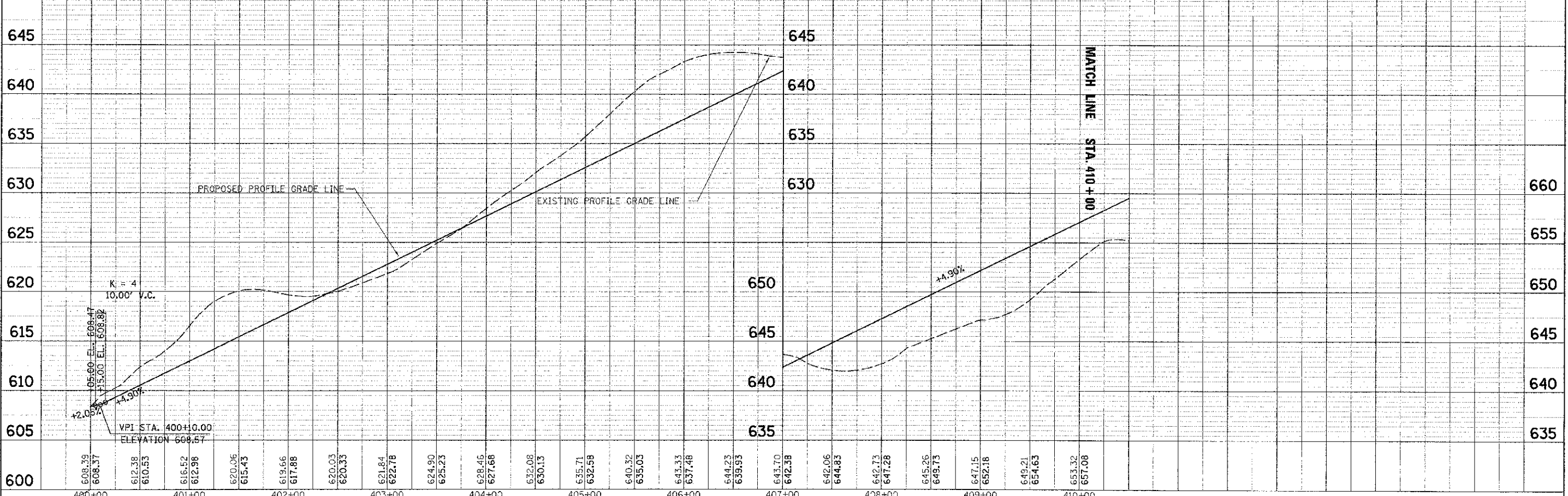
- ① R1-1, STOP, 18" X 18" & NO MOTOR VEHICLES, 18" X 18"
- ② W3-1, STOP, AHEAD 18" X 18"
- ③ W1-1, SHARP CURVE, 18" X 18"
- ④ W1-2, CURVE, 18" X 18"



CURVE 3-1	CURVE 3-2	CURVE 3-3	CURVE 3-4	CURVE 3-5	CURVE 3-6	CURVE 3-7	CURVE 3-8	CURVE 3-9
PI STA. = 400+06.20	PI STA. = 400+61.10	PI STA. = 402+19.04	PI STA. = 402+18.56	PI STA. = 403+88.96	PI STA. = 460+40.02	PI STA. = 407+75.76	PI STA. = 408+55.68	PI STA. = 409+45.04
$\Delta = 63^\circ 34' 34''$ (RT)	$\Delta = 136^\circ 24' 12''$ (LT)	$\Delta = 145^\circ 21' 36''$ (RT)	$\Delta = 47^\circ 29' 50''$ (LT)	$\Delta = 114^\circ 59' 02''$ (LT)	$\Delta = 178^\circ 09' 44''$ (RT)	$\Delta = 73^\circ 46' 55''$ (LT)	$\Delta = 38^\circ 40' 06''$ (RT)	$\Delta = 67^\circ 32' 57''$ (LT)
D = 572' 57" 28"	D = 286' 28" 44"	D = 114' 35" 30"	D = 76' 23" 40"	D = 63' 39" 43"	D = 63' 39" 43"	D = 63' 39" 43"	D = 63' 39" 43"	D = 63' 39" 43"
R = 10.00'	R = 20.00'	R = 50.00'	R = 75.00'	R = 90.00'	R = 90.00'	R = 90.00'	R = 90.00'	R = 90.00'
T = 6.20'	T = 50.01'	T = 160.33'	T = 33.00'	T = 141.23'	T = 5.611.67'	T = 67.55'	T = 31.58'	T = 60.19'
L = 11.10'	L = 47.61'	L = 126.85'	L = 62.17'	L = 180.62'	L = 279.86'	L = 115.90'	L = 60.74'	L = 106.11'
E = 1.76'	E = 33.86'	E = 117.95'	E = 6.94'	E = 77.47'	E = 5.522.39'	E = 22.53'	E = 5.38'	E = 18.27'
e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%
P.C. STA. = 400+00.00	P.C. STA. = 400+11.10	P.C. STA. = 400+58.71	P.C. STA. = 401+85.56	P.C. STA. = 402+47.73	P.C. STA. = 404+28.35	P.C. STA. = 407+08.21	P.C. STA. = 408+24.10	P.C. STA. = 408+84.84
P.T. STA. = 400+11.10	P.T. STA. = 400+58.71	P.T. STA. = 401+85.56	P.T. STA. = 402+47.73	P.T. STA. = 404+28.35	P.T. STA. = 407+08.21	P.T. STA. = 408+24.10	P.T. STA. = 408+84.84	P.T. STA. = 409+90.95

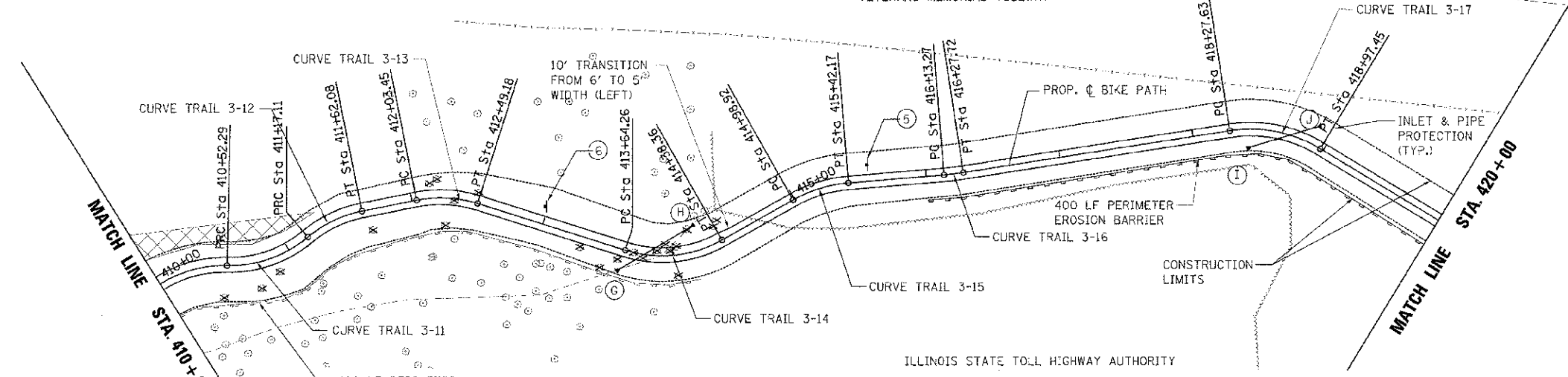


- STA. 400+25, 12" CLASS A, TYPE 1
- (A) INV=607.50, 12" FES WITH RIP RAP
- (B) INV=607.75, 12" FES WITH RIP RAP
- STA. 401+96, 15" CLASS A, TYPE 1
- (C) INV=615.25, 15" FES WITH RIP RAP
- (D) INV=615.75, 15" FES WITH RIP RAP
- STA. 407+50, 15" CLASS A, TYPE 1
- (E) INV=640.00, 15" FES WITH RIP RAP
- (F) INV=641.00, 15" FES WITH RIP RAP
- CURVE 3-10
- PI STA. = 410+22.86
- $\Delta = 39^\circ 02' 53''$ (RT)
- D = 63' 39" 43"
- R = 90.00'
- T = 31.91'
- L = 61.34'
- E = 5.49'
- e = 2.0%
- P.C. STA. = 409+90.95
- P.T. STA. = 410+52.29



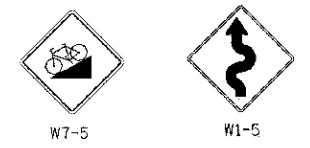
<p>URS 180 SOUTH WILSON AVENUE, SUITE 500 CHICAGO, IL 60606 (312) 975-1000</p>	DESIGNED	NSB	REVISED		FOREST PRESERVE DISTRICT OF WILL COUNTY, ILLINOIS	VETERANS TRAIL - NORTH PLAN-PROFILE DRAWING	SECTION	COUNTY	TOTAL SHEETS	
	DRAWN	NSB	REVISED				11-F3000-00-BT	WILL	27	5
	CHECKED	DDL	REVISED				SCALE: 1"= 50'	SHEET NO. OF SHEETS	STA. 400+00 TO STA. 409+00	CONTRACT NO. 63728
	DATE	8/20/12	REVISED							

NO ACCESS ALLOWED FROM TOLLWAY
 EXISTING EDGE OF SHOULDER
 VETERANS MEMORIAL TOLLWAY
 ILLINOIS STATE TOLL HIGHWAY AUTHORITY



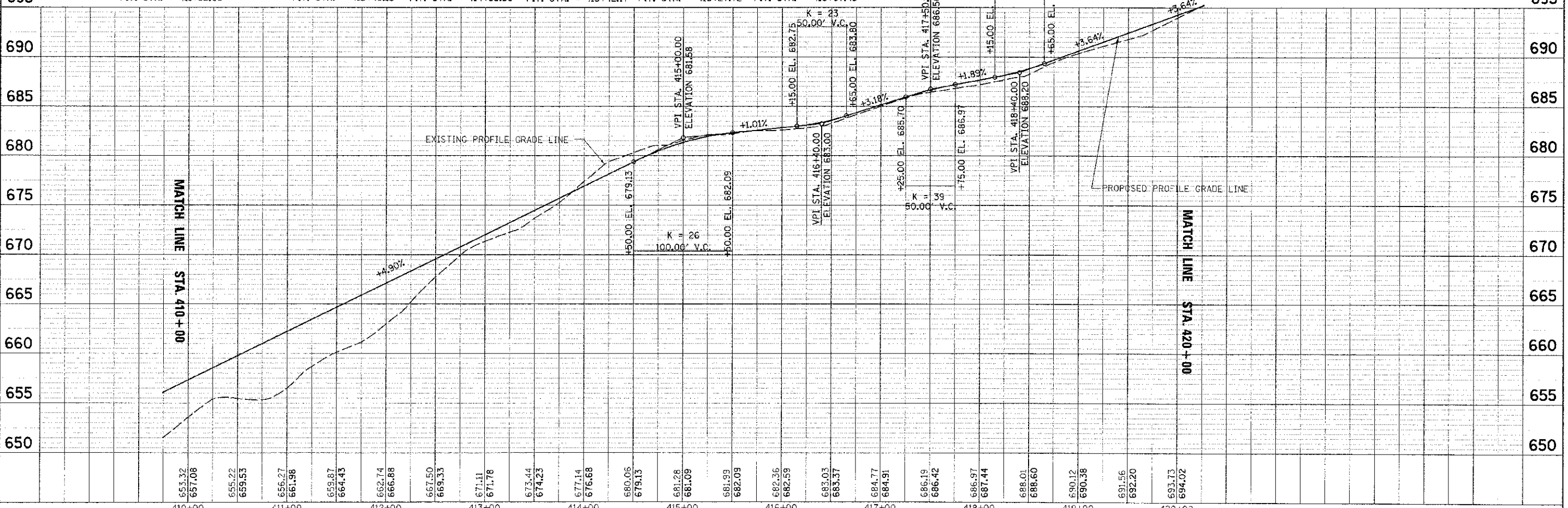
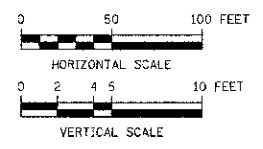
LEGEND
 SELECTIVE CLEARING AREA. ALL SHRUBS AND MOST TREES LESS THAN 6" IN SHALL BE REMOVED. REMOVAL SHALL CONSIST OF CUTTING THE TRUNK AT A HEIGHT OF 2" ABOVE GRADE AND THEN APPLYING HERBICIDE. ENGINEER WILL MARK TREES TO REMAIN. THIS WORK WILL BE PAID FOR PER ACRE OF CLEARING, SPECIAL.

- (5) W7-5, HILL, 18" X 18"
- (6) W1-5, CURVES, 18" X 18"



CURVE 3-11	CURVE 3-12	CURVE 3-13	CURVE 3-14	CURVE 3-15	CURVE 3-16	CURVE 3-17
PI STA. = 410+86.18	PI STA. = 411+40.08	PI STA. = 412+26.82	PI STA. = 414+03.56	PI STA. = 415+20.89	PI STA. = 416+20.50	PI STA. = 418+64.03
Δ = 41° 16' 10" (LT)	Δ = 28° 37' 42" (RT)	Δ = 29° 06' 37" (RT)	Δ = 47° 10' 31" (LT)	Δ = 24° 46' 50" (RT)	Δ = 4° 08' 22" (LT)	Δ = 40° 00' 23" (RT)
D = 63° 39' 43"	D = 63° 39' 43"	D = 63° 39' 43"	D = 63° 39' 43"	D = 57° 17' 45"	D = 28° 38' 52"	D = 57° 17' 45"
R = 90.00'	R = 90.00'	R = 90.00'	R = 90.00'	R = 100.00'	R = 200.00'	R = 100.00'
T = 33.89'	T = 22.96'	T = 23.37'	T = 39.30'	T = 21.97'	T = 7.23'	T = 36.40'
L = 64.83'	L = 44.97'	L = 45.73'	L = 74.10'	L = 43.25'	L = 14.45'	L = 69.82'
E = 6.17'	E = 2.88'	E = 2.98'	E = 8.21'	E = 2.38'	E = 0.13'	E = 6.42'
e = 2.0%	e = 2.0%	e = 2.0%	e = 2.0%	e =	e =	e =
P.C. STA. = 410+52.29	P.C. STA. = 411+17.11	P.C. STA. = 412+03.45	P.C. STA. = 413+64.26	P.C. STA. = 414+98.92	P.C. STA. = 416+13.27	P.C. STA. = 418+27.63
P.T. STA. = 411+17.11	P.T. STA. = 411+62.08	P.T. STA. = 412+49.18	P.T. STA. = 414+38.36	P.T. STA. = 415+42.17	P.T. STA. = 416+27.72	P.T. STA. = 418+97.45

- STA. 413+79, 15" CLASS A, TYPE 1
- (6) INV=672.0, 15" FES WITH RIP RAP
- (H) INV=674.0, 15" FES WITH RIP RAP
- STA. 418+73, 15" CLASS A, TYPE 1
- (I) INV=687.0, 15" FES WITH RIP RAP
- (J) INV=688.5, 15" FES WITH RIP RAP



653.32	657.08	655.22	659.53	656.27	661.98	659.87	664.43	662.74	666.88	667.50	669.33	671.11	671.78	673.44	674.23	677.14	676.68	680.06	679.13	681.28	681.09	681.99	682.09	682.36	682.59	683.03	683.37	684.77	684.91	686.19	686.42	686.97	687.44	688.01	688.60	690.12	690.38	691.56	692.20	693.73	694.02	
410+00	411+00	412+00	413+00	414+00	415+00	416+00	417+00	418+00	419+00	420+00																																

PLAN
 DATE: 8/20/12
 DRAWN: NSB
 CHECKED: DDL
 DATE: 8/20/12

PROFILE
 DATE: 8/20/12
 DRAWN: NSB
 CHECKED: DDL
 DATE: 8/20/12



DESIGNED: NSB
 DRAWN: NSB
 CHECKED: DDL
 DATE: 8/20/12

FOREST PRESERVE DISTRICT
 OF WILL COUNTY, ILLINOIS

VETERANS TRAIL - NORTH
 PLAN-PROFILE DRAWING

SECTION: 11-F3000-00-BT
 COUNTY: WILL
 TOTAL SHEET NO.: 27
 SHEET NO.: 6
 CONTRACT NO.: 63728

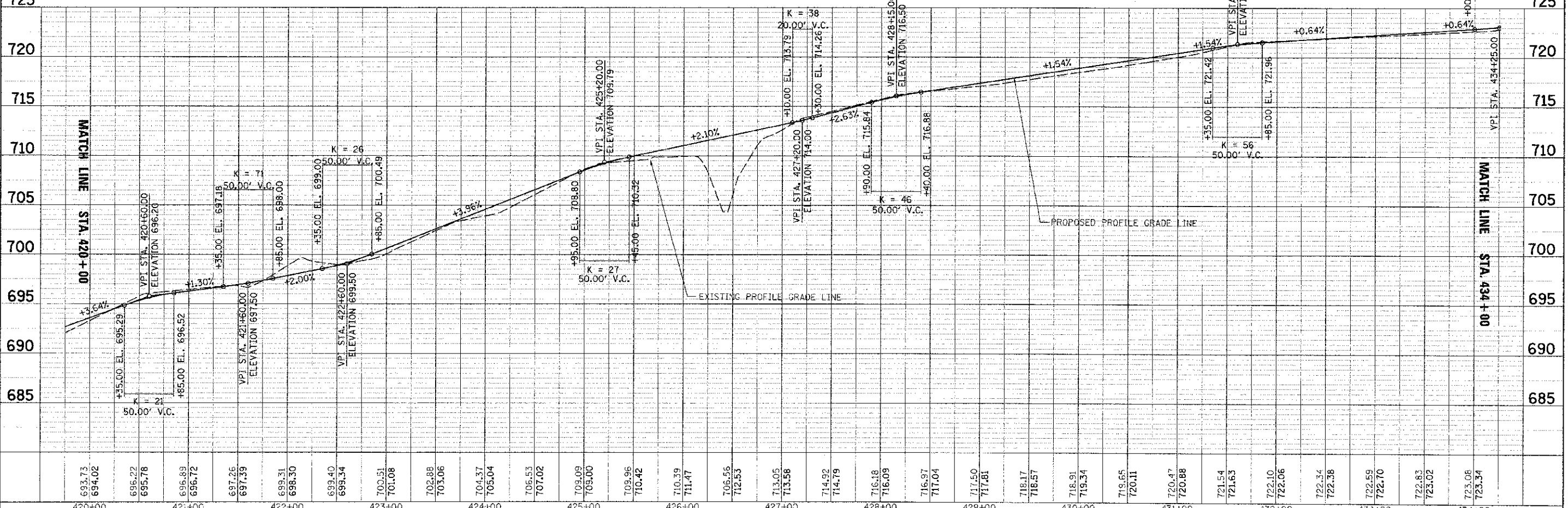
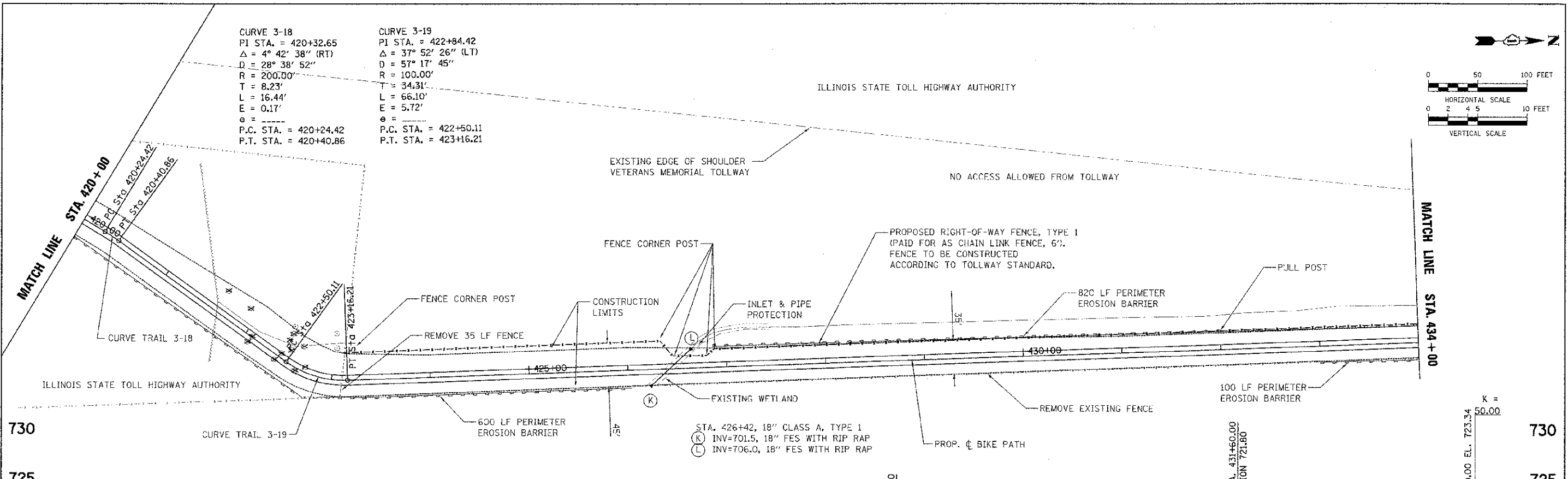
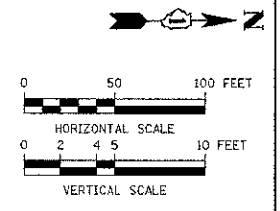
SCALE: 1" = 50'
 SHEET NO. OF SHEETS: STA. 409+00 TO STA. 418+00

DATE	3/1/10
BY	NSB
CHECKED	DDL
DATE	8/20/12

PROJECT	FOREST PRESERVE DISTRICT OF WILL COUNTY, ILLINOIS
SHEET NO.	27
TOTAL SHEETS	7

CURVE 3-18
 PI STA. = 420+32.65
 $\Delta = 4^\circ 42' 38''$ (RT)
 $D = 28^\circ 38' 52''$
 $R = 200.00'$
 $T = 8.23'$
 $L = 16.44'$
 $E = 0.17'$
 $e =$
 P.C. STA. = 420+24.42
 P.T. STA. = 420+40.86

CURVE 3-19
 PI STA. = 422+84.42
 $\Delta = 37^\circ 52' 26''$ (LT)
 $D = 57^\circ 17' 45''$
 $R = 100.00'$
 $T = 34.31'$
 $L = 66.10'$
 $E = 5.72'$
 $e =$
 P.C. STA. = 422+50.11
 P.T. STA. = 423+16.21



693.73	694.02	696.22	695.78	696.89	696.72	697.25	697.39	699.31	698.30	699.40	699.34	700.51	701.08	702.88	703.06	704.37	705.04	706.53	707.02	709.09	709.00	709.96	710.42	710.39	711.47	706.56	712.53	713.05	713.58	714.92	714.79	716.18	716.09	716.97	717.04	717.50	717.81	718.17	718.57	718.91	719.34	719.65	720.11	720.47	720.88	721.54	721.63	722.10	722.06	722.34	722.38	722.59	722.70	722.83	723.02	723.08	723.34
420+00	421+00	422+00	423+00	424+00	425+00	426+00	427+00	428+00	429+00	430+00	431+00	432+00	433+00	434+00																																											



DESIGNED - NSB
 DRAWN - NSB
 CHECKED - DDL
 DATE - 8/20/12

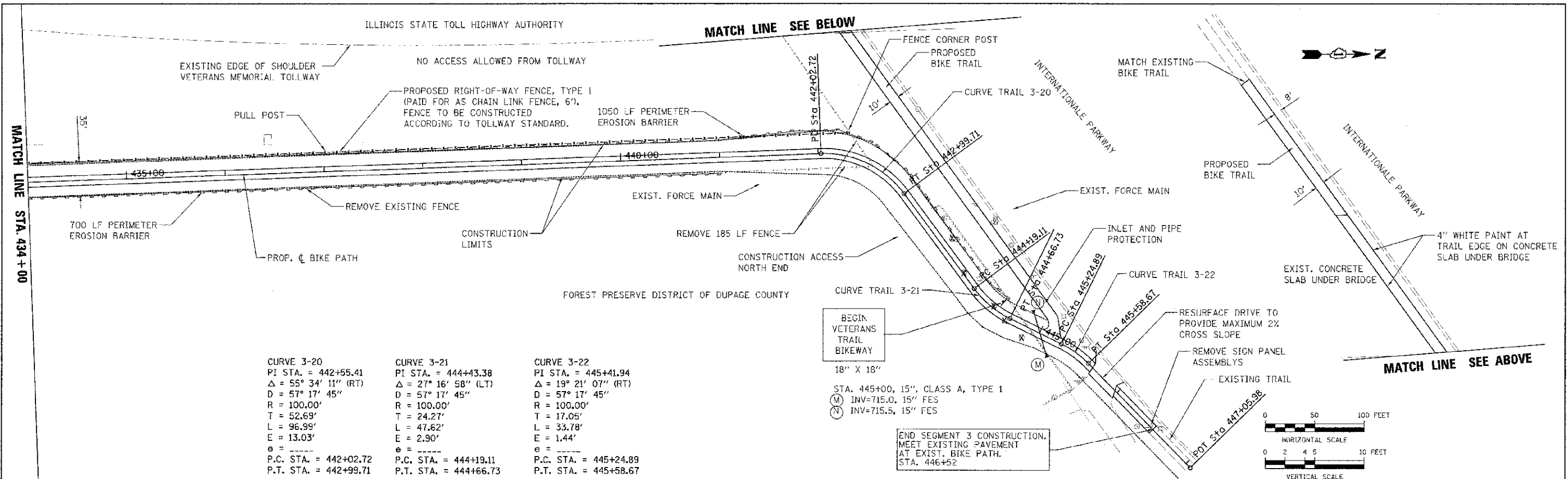
REVISED
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**FOREST PRESERVE DISTRICT
 OF WILL COUNTY, ILLINOIS**

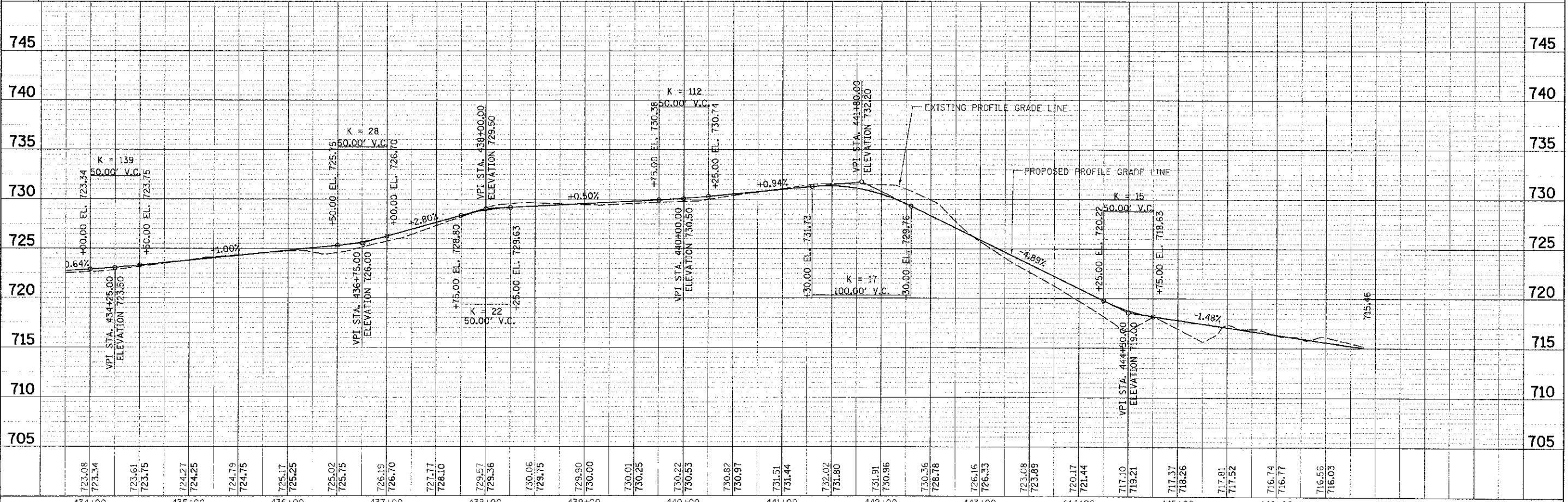
**VETERANS TRAIL - NORTH
 PLAN-PROFILE DRAWING**

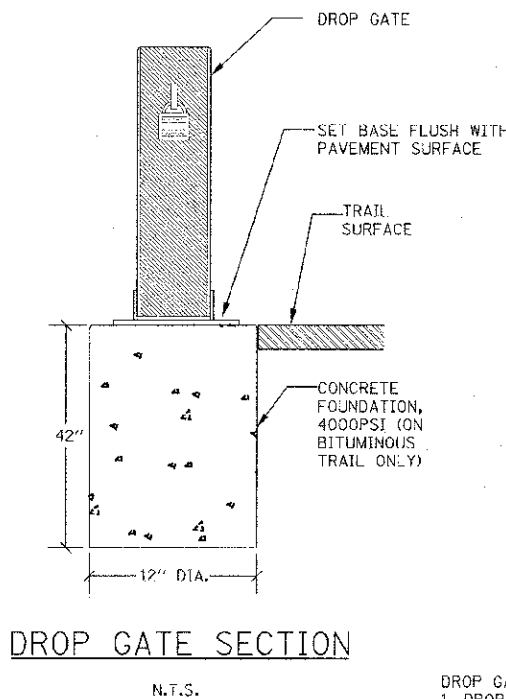
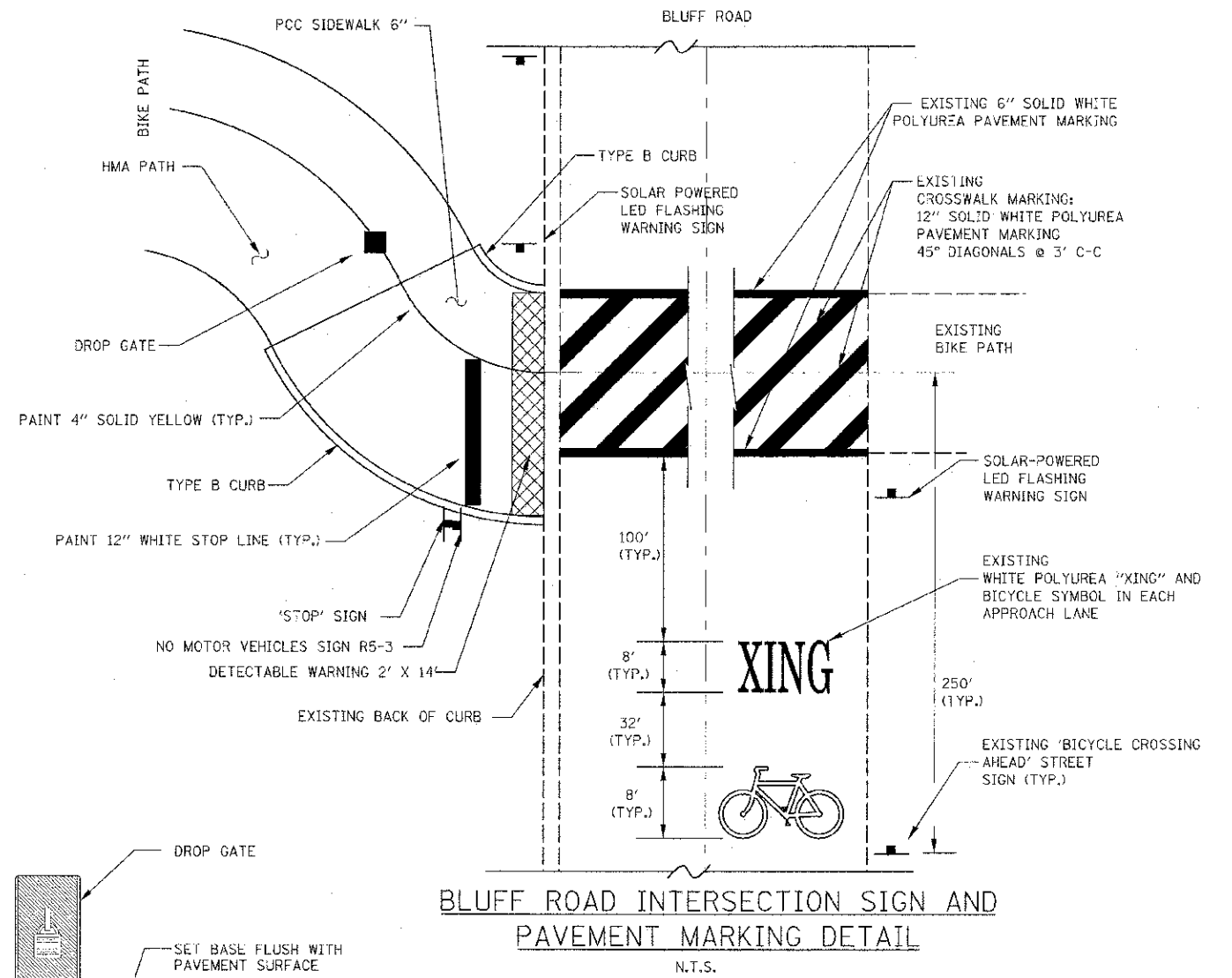
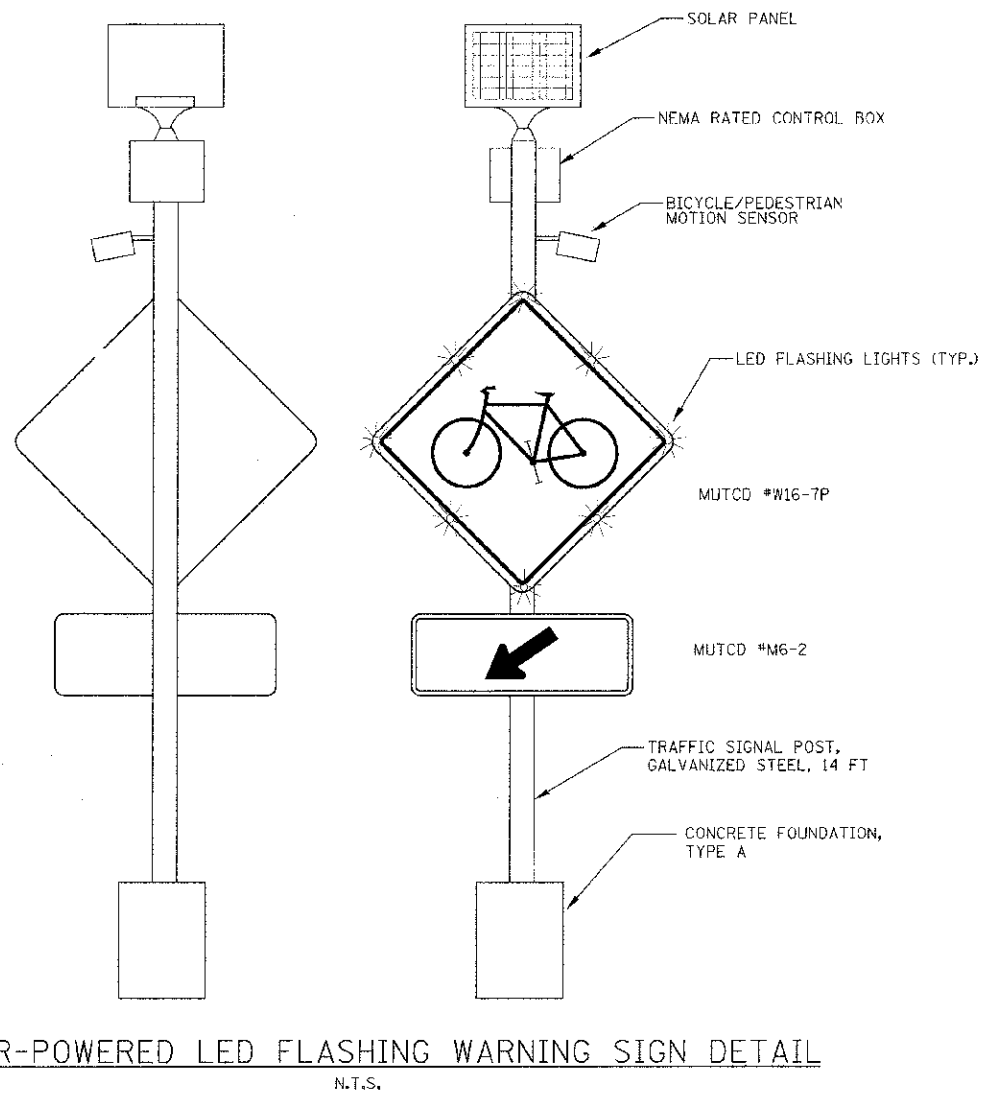
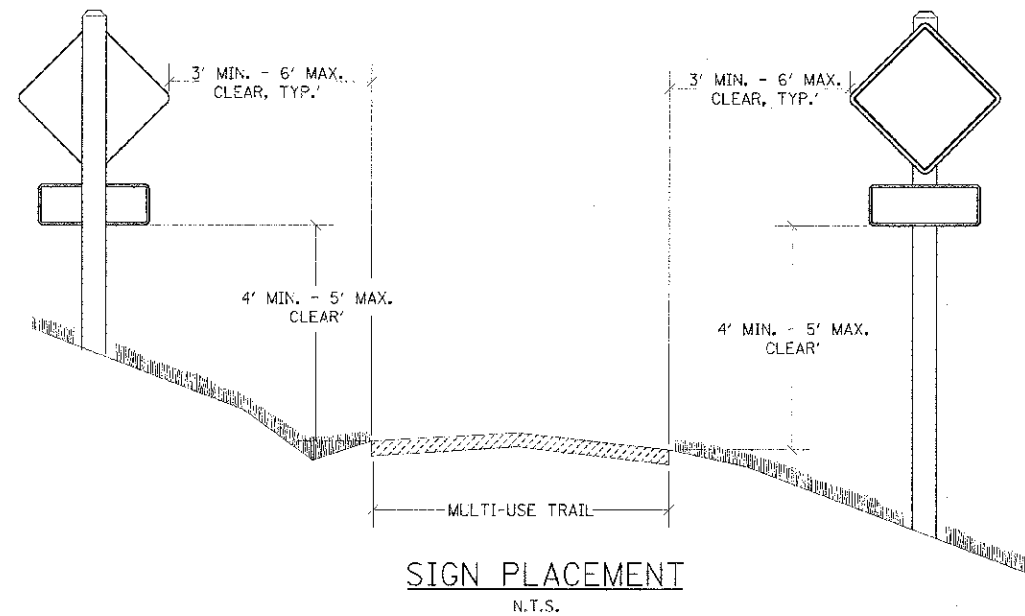
SECTION	11-F3000-00-BT	COUNTY	WILL	TOTAL SHEETS	27	SHEET NO.	7
CONTRACT NO. 63728							

SCALE: 1" = 50' SHEET NO. OF SHEETS STA. 418+00 TO STA. 431+00

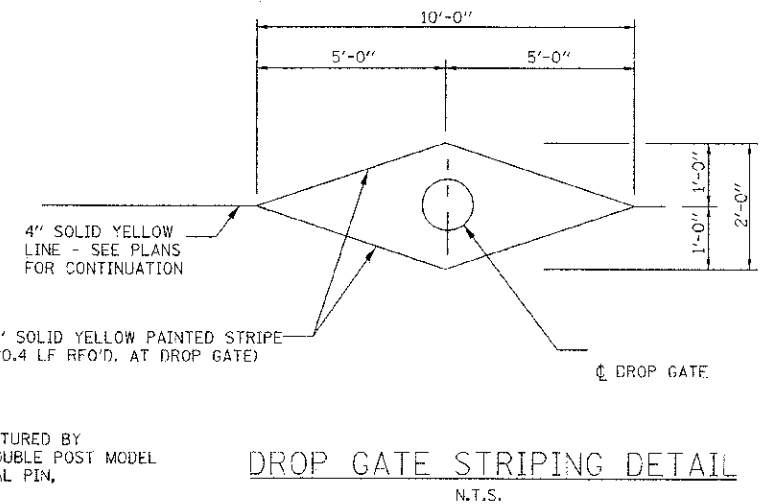


CURVE 3-20	CURVE 3-21	CURVE 3-22
PI STA. = 442+55.41	PI STA. = 444+43.38	PI STA. = 445+41.94
$\Delta = 55^\circ 34' 11''$ (RT)	$\Delta = 27^\circ 16' 58''$ (LT)	$\Delta = 19^\circ 21' 07''$ (RT)
$D = 57^\circ 17' 45''$	$D = 57^\circ 17' 45''$	$D = 57^\circ 17' 45''$
$R = 100.00'$	$R = 100.00'$	$R = 100.00'$
$T = 52.69'$	$T = 24.27'$	$T = 17.05'$
$L = 96.99'$	$L = 47.62'$	$L = 33.78'$
$E = 13.03'$	$E = 2.90'$	$E = 1.44'$
$e = \dots$	$e = \dots$	$e = \dots$
P.C. STA. = 442+02.72	P.C. STA. = 444+19.11	P.C. STA. = 445+24.89
P.T. STA. = 442+99.71	P.T. STA. = 444+66.73	P.T. STA. = 445+58.67





DROP GATE NOTES:
 1. DROP GATE SHALL BE MANUFACTURED BY TRAFFICGUARD (877-727-7347), DOUBLE POST MODEL WITH DOUBLE LOCKS AND OPTIONAL PIN, OR APPROVED EQUAL.
 2. INSTALL SO THAT DROP GATE LIES FLAT ON PAVEMENT WHEN UNLOCKED. DROP GATE TO BE FLUSH WITH PAVEMENT AND PARALLEL TO EDGES OF TRAIL WHEN OPEN.



FILE NAME: D:\V\F\2012\046_Compound\11-F3000-00-BT\Miscellaneous-Detail.dwg



USER NAME: David J. ...	DESIGNED: -	REVISED: -
PROJECT: 11-F3000-00-BT	DRAWN: -	REVISED: -
PL01 061c x 3/21/2012	CHECKED: /DAK	REVISED: -
	DATE: 8/20/12	REVISED: -

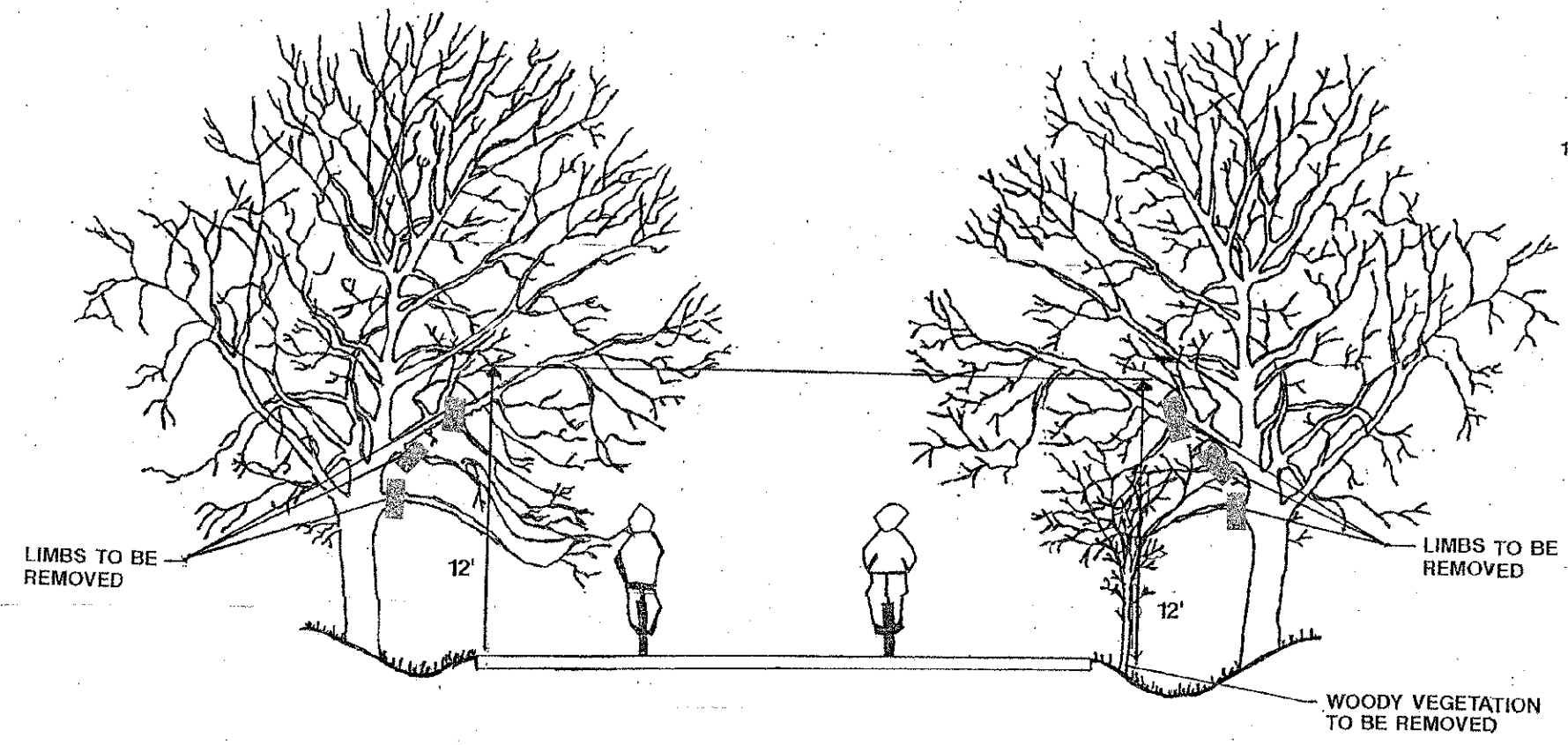
FOREST PRESERVE DISTRICT OF WILL COUNTY, ILLINOIS

VETERANS TRAIL - NORTH MISCELLANEOUS DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-F3000-00-BT	WILL	27	9
CONTRACT NO. 63728				

SCALE: NTS SHEET NO. 1 OF 4 SHEETS STA. --- TO STA. ---

SECTION	COUNTY	DATE	SHEET
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



GENERAL NOTES
 1. THE NATIONAL ARBORIST ASSOCIATION'S PRUNING STANDARDS FOR SHADE TREES CLASS II - STANDARD PRUNING SPECIFICATIONS SHALL BE FOLLOWED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

PRUNING FOR SAFETY AND EQUIPMENT CLEARANCE

SCALE: NTS

DATE

DRAWN BY
DESIGNED BY
CHECKED BY

FILE NAME: G:\V\JPC\2012\20120820\Forest Preserve District of Will County\Veterans Trail\Tree Trimming.dwg



USER NAME: David.Levickner	DESIGNED	REVIS
PI 01 SCALE: 7/16/10 1/2" = 1'	DRAWN	REVIS
PLC1 DATE: 8/21/2012	CHECKED	REVIS
DATE: 8/20/12		REVIS

FOREST PRESERVE DISTRICT OF WILL COUNTY, ILLINOIS

VETERANS TRAIL - NORTH TREE TRIMMING DETAIL

SCALE: NTS SHEET NO. 4 OF 4 SHEETS STA. — TO STA. —

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-F3000-00-BT	WILL	27	10
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
	CONTRACT NO. 6372B			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND CUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL. 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND CUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

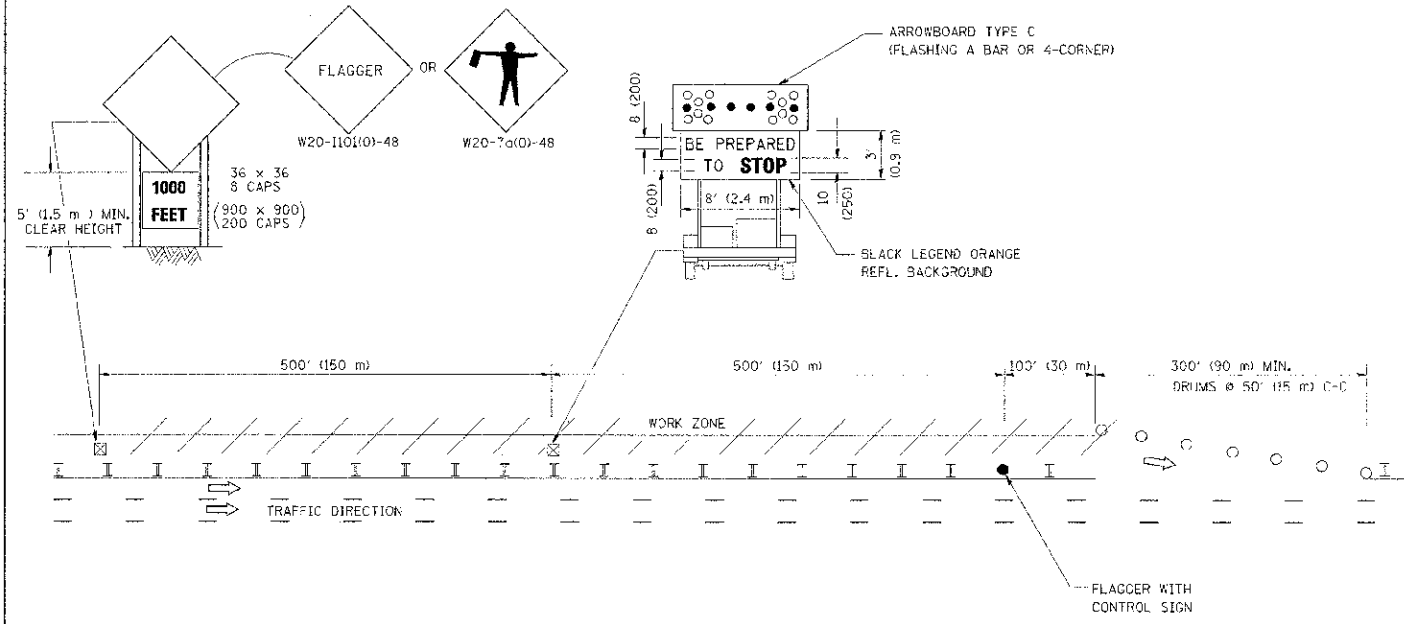
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

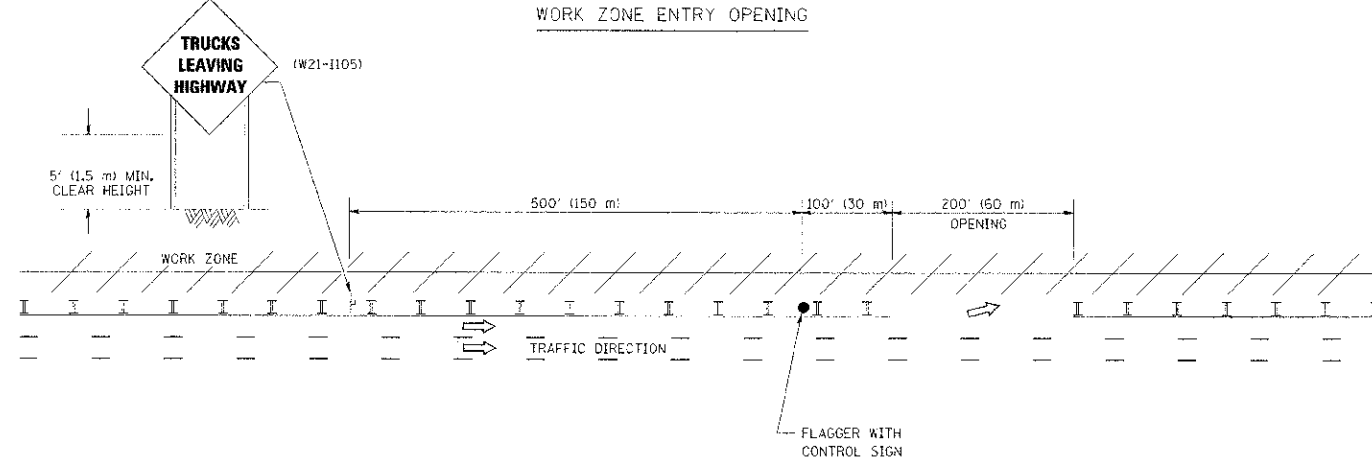
DESIGNED - A. HOUSEH DRAWN - CHECKED - DATE - 03-11-94	REVISED - R. SHAH 10-03-96 REVISED - A. ARBAS 03-21-97 REVISED - M. GOMEZ 01-22-01 REVISED - R. BORO 12-15-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A. RTE. 11-F3000-00-BT COUNTY WILL SECTION 11-F3000-00-BT SHEET NO. 27 OF 11 CONTRACT NO. 63728
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SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



- NOTES:
1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
 2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
 3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
 4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

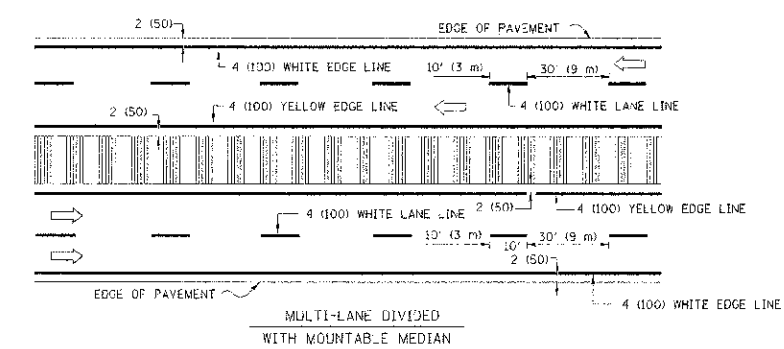
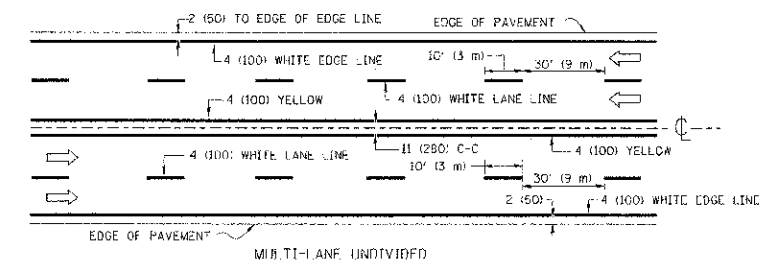
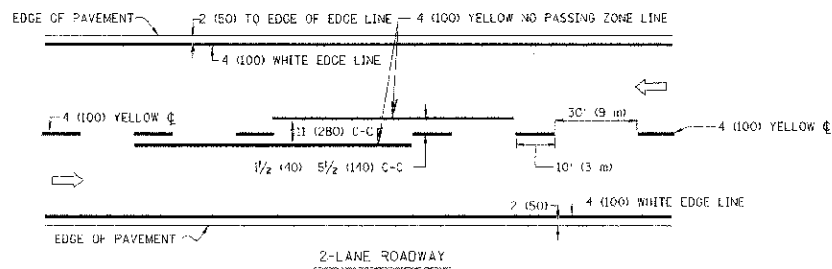
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

PLT.C NAME = W:\detroit\22\24\to18.dgn	USER NAME = lsgco	DESIGNED -	REVISED - J.A.F. 04-03
		DRAWN -	REVISED - J.A.F. 02-06
		CHECKED -	REVISED - S.P.B. 01-07
		DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

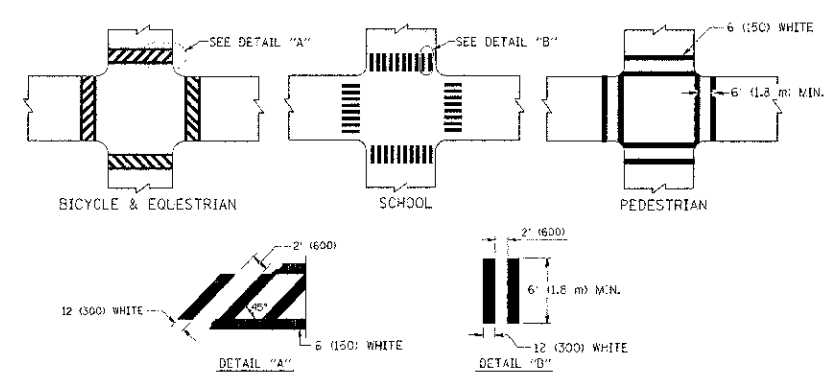
SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
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F.A. I RTE.	SECTION 11-F3000-00-BT	COUNTY WILL	TOTAL SHEET NO. 27
TC-18		CONTRACT NO. 63728	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

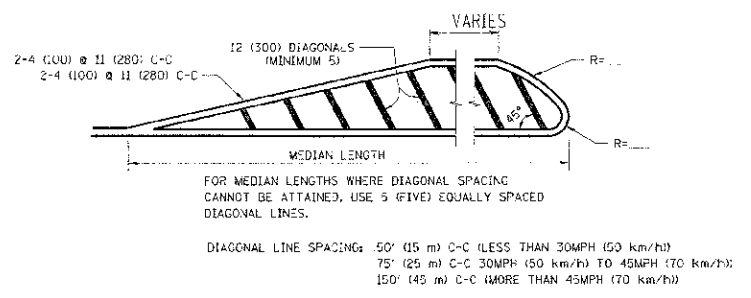
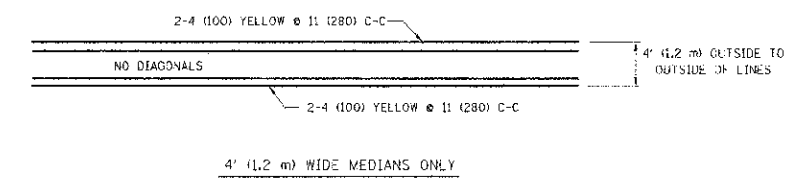


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

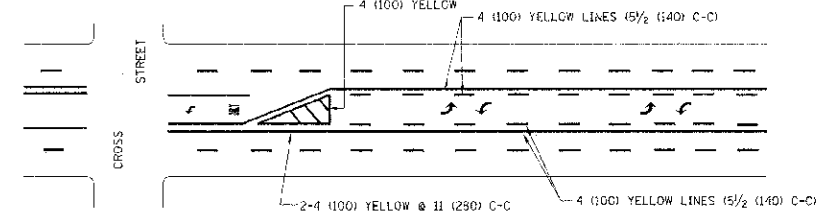
TYPICAL LANE AND EDGE LINE MARKING



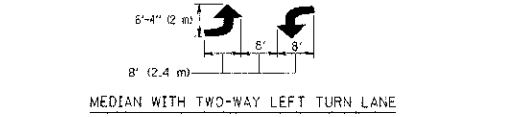
TYPICAL CROSSWALK MARKING



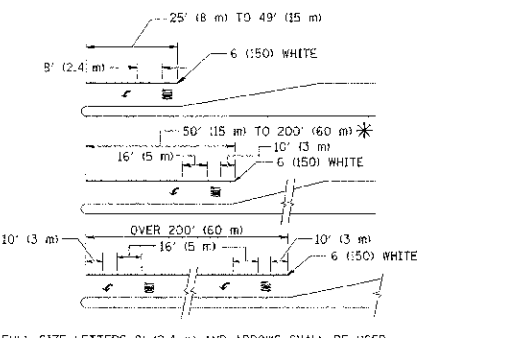
MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING

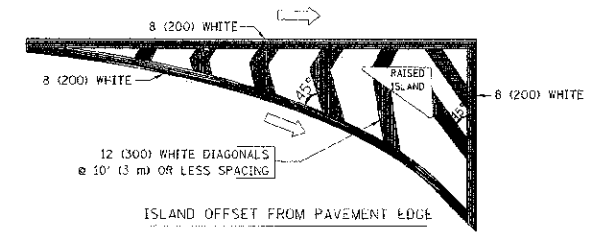


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

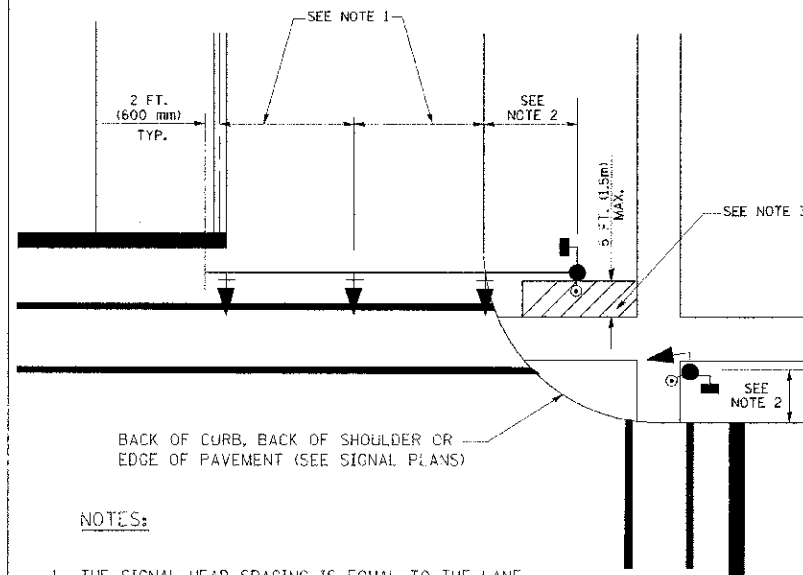
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE
FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100)	SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
FOR ONE DIRECTION	5 (125)	SKIP-DASH	WHITE	
FOR BOTH DIRECTIONS	5 (125)	SKIP-DASH	WHITE	
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN & BICYCLE & EQUESTRIAN)	2 @ 5 (150)	SOLID	WHITE	NOT LESS THAN 6' (1.8 m) APART
DIAGONALS (BICYCLE & EQUESTRIAN)	12 (300) @ 45°	SOLID	WHITE	2' (600) APART
LONGITUDINAL BARS (SCHOOL)	12 (300) @ 90°	SOLID	WHITE	2' (600) APART
				SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: 10' x 16 SO. FT. (0.33 m ²) EACH "X" = 54.0 SO. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

TRAFFIC SIGNAL MAST ARM AND SIGNAL POST

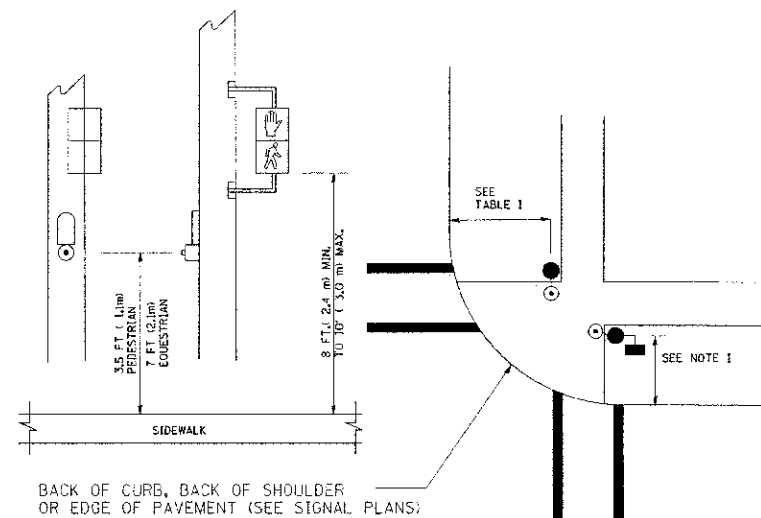
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

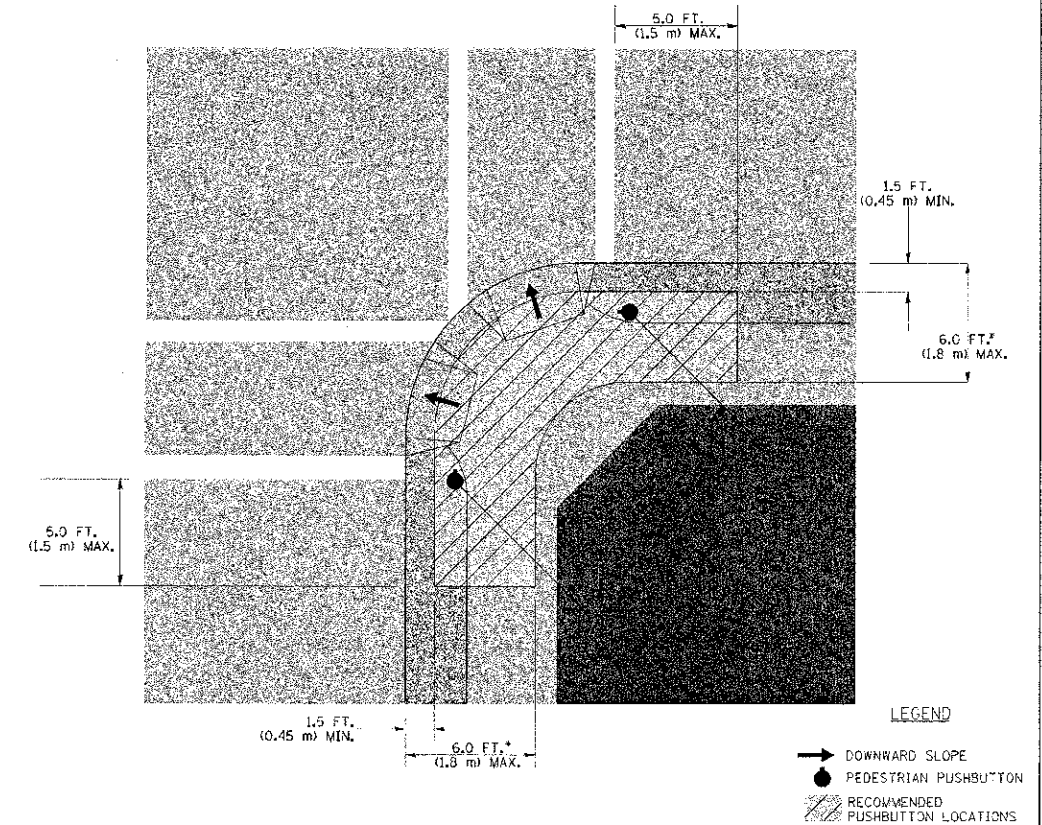
PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

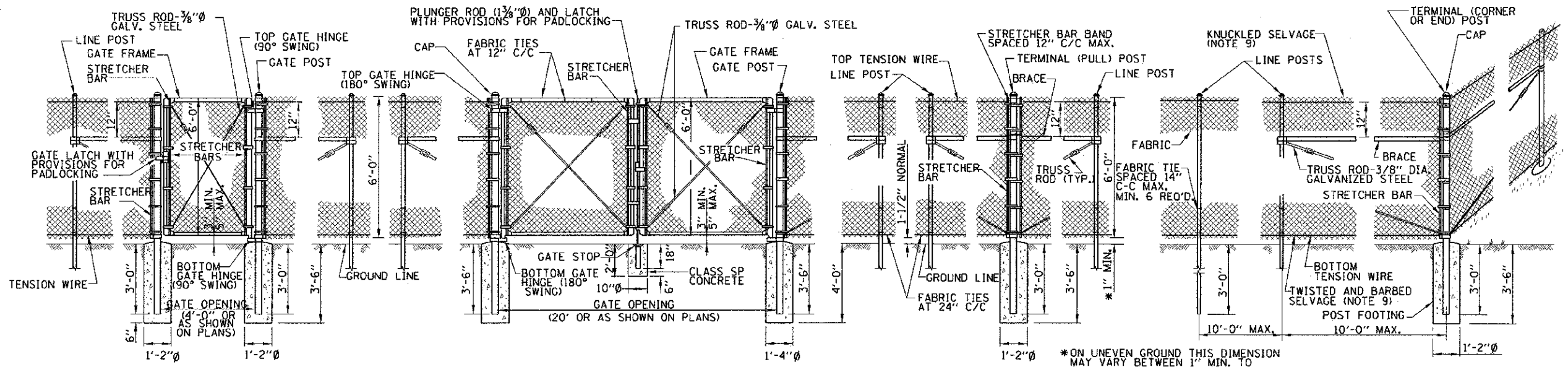
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROL CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.



PEDESTRIAN GATE

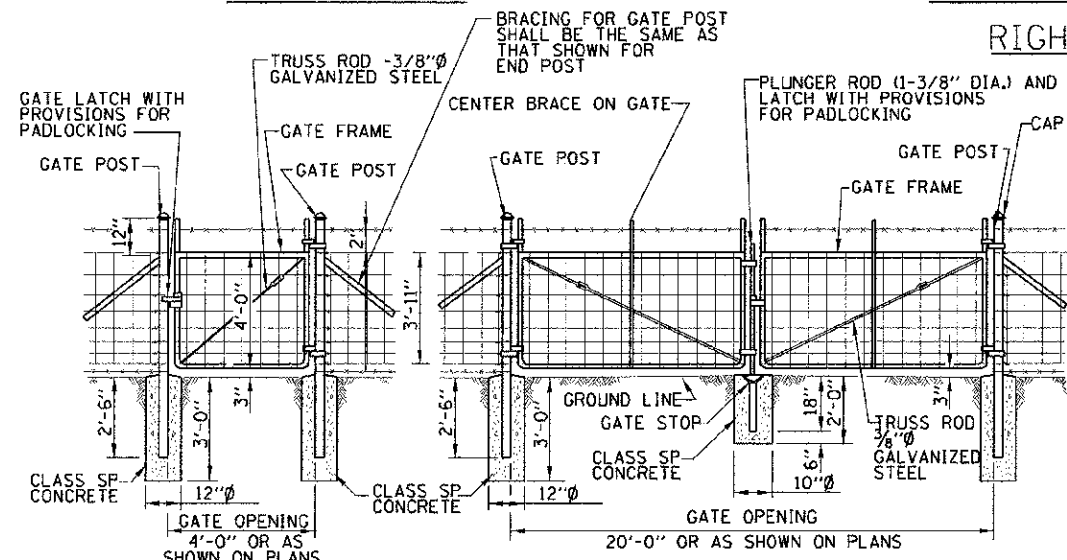
VEHICULAR GATE

PULL POST

LINE POST

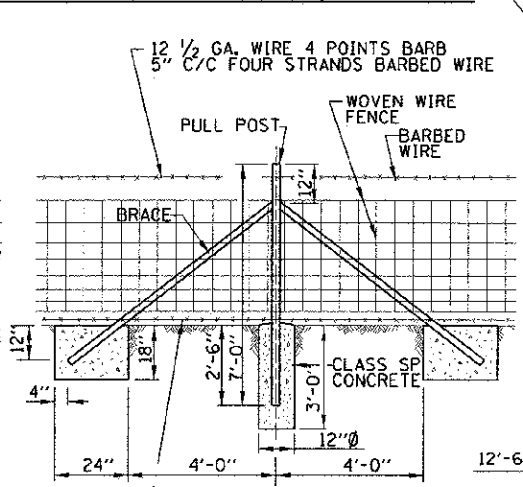
CORNER OR END POST

RIGHT OF WAY FENCE, TYPE 1

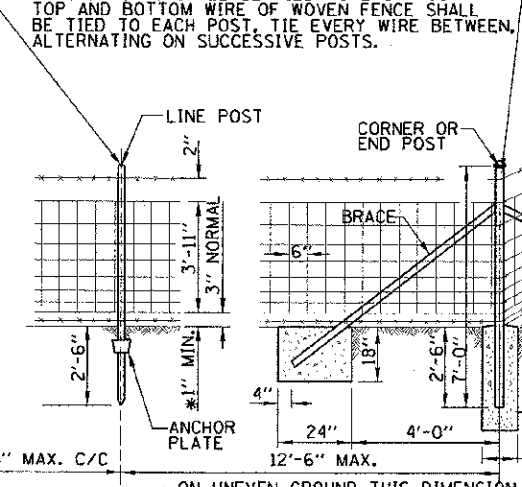


PEDESTRIAN GATE

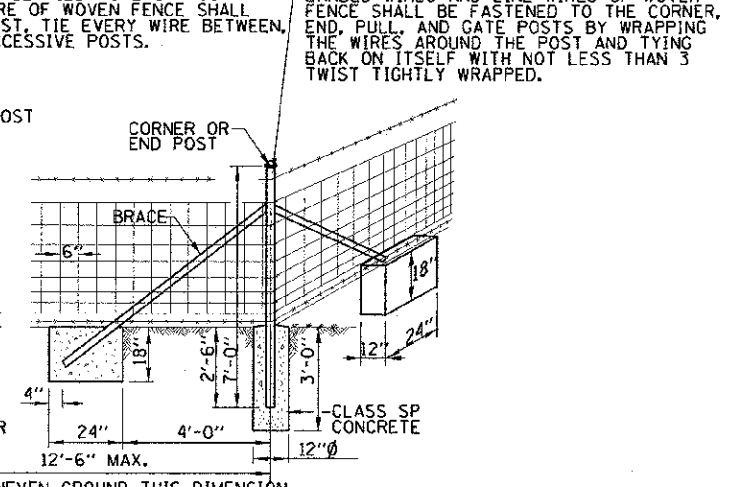
VEHICULAR GATE



PULL POST



LINE POST

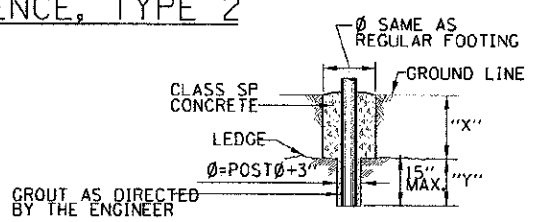


CORNER OR END POST

GENERAL NOTES

RIGHT OF WAY FENCE, TYPE 2

- ON STRAIGHT RUNS OF FENCE, PULL POSTS SHALL BE USED AT 500' CENTERS FOR TYPE 1 AND 330' CENTERS FOR TYPE 2.
- ALL FENCING MATERIALS SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- WHERE R.O.W. FENCE FOLLOWS R.O.W. LINE IT SHALL BE INSTALLED PARALLEL TO AND 6" INSIDE THE R.O.W. LINE ON TOLLWAY PROPERTY.
- LINE POSTS AND BRACES SHALL BE ON TOLLWAY SIDE OF FENCE FABRIC.
- WHEN THE TENSION OF THE FENCE TENDS TO PULL THE POSTS FROM THE GROUND, THE LINE POSTS SHALL BE ANCHORED WITH ANCHORAGE SPECIFIED FOR CORNER POSTS.
- AT LOCATION WHERE THE PROPOSED FENCE IS TO BE CONNECTED TO AN EXISTING POST, THE REQUIRED CONNECTIONS AND BRACING INCLUDING ALL NECESSARY HARDWARE SHALL BE CONSIDERED INCIDENTAL TO THE FENCE OF THE TYPE SPECIFIED.
- WHEN THE FENCE LINE HAS A CHANGE IN DIRECTION OF 10° OR MORE, A CORNER POST SHALL BE PLACED AT THE POINT OF CHANGE, WHERE THE ANGLE OF CHANGE IS LESS THAN 10° A PULL POST SHALL BE USED.
- WHERE GRADE LINE HAS A CHANGE IN SLOPE OF 10° OR MORE, A CORNER POST WITH BRACING AS REQUIRED SHALL BE PLACED, WHERE ANGLE IS LESS THAN 10° LINE POST MAY BE USED.
- WHERE TYPE 1 FENCE IS USED, THE FABRIC SHALL BE KNUCKLED SELVAGE ON TOP AND TWISTED AND BARBED SELVAGE ON BOTTOM.
- PLACEMENT OF BRACED END POSTS OR CORNER POSTS WITHIN THE CLEAR ZONE SHALL BE AVOIDED.



FOOTING FOR POST WHEN ROCK LEDGE IS ENCOUNTERED

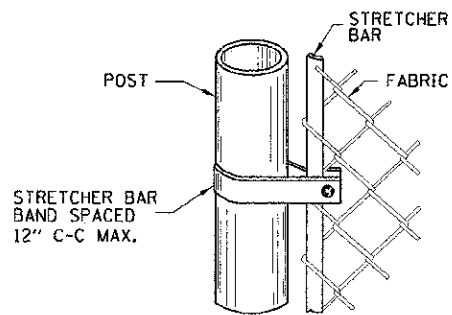
NOTE:
 "X" + "Y" SHALL NOT EXCEED 30"
 WHEN "X" IS 0" TO 15" "Y" = 15", AND
 THE POST SHALL BE SHORTENED AS
 REQUIRED, WHEN "X" EXCEEDS 15"
 "Y" SHALL BE DECREASED
 ACCORDINGLY.

APPROVED: *Paul Kovacs*
 CHIEF ENGINEER DATE 7-1-2009

DATE	REVISIONS
7-1-2009	R.O.W. FENCE TYPES 1 AND 2 FENCE DETAILS

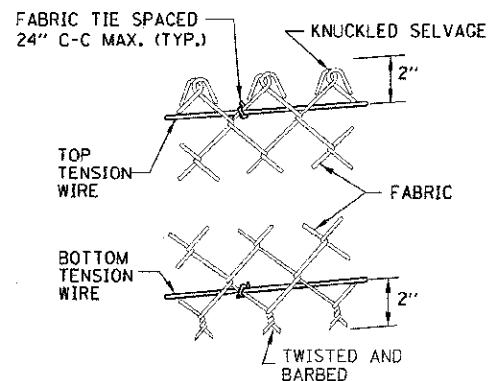
Illinois Tollway
Open Roads for a Faster Future

RIGHT OF WAY FENCE
 Sheet 16 of 27
 CONTRACT NO. 63728
 STANDARD D1-01

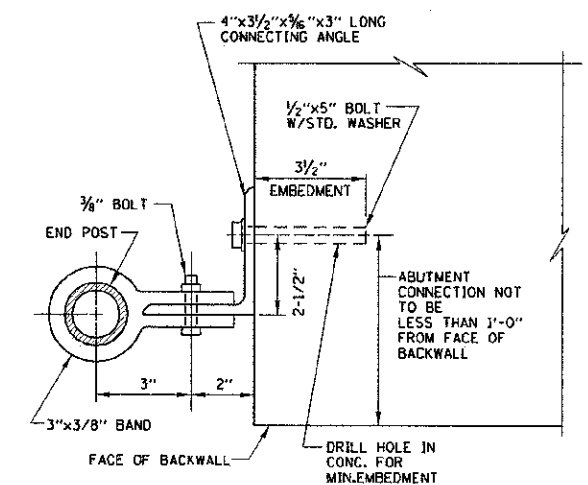


STRETCHER BARS SHALL BE GALVANIZED FLAT STEEL BAR NOT LESS THAN 1/4"x3/4" AND THE STRETCHER BAR BANDS SHALL BE GALVANIZED FLAT STEEL BAR NOT LESS THAN 1/8"x 1" WITH A 3/8" GALVANIZED CARRIAGE BOLT.

METHOD OF FASTENING STRETCHER BAR TO POST



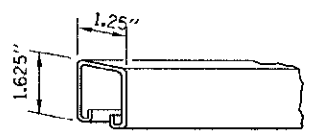
METHOD OF TYING FABRIC TO TENSION WIRES



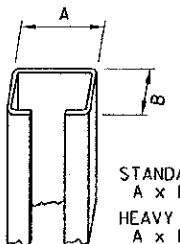
ABUTMENT CONNECTION DETAIL

NOTES FOR ABUTMENT CONNECTION:

1. WHEN ROLL FORMED SECTION IS USED IN LIEU OF PIPE AS END POST, THE POST SHALL BE BOLTED DIRECTLY TO THE ABUTMENT WALL WITH 2 1/2" x 5" BOLTS WITH STANDARD WASHERS MEETING THE APPROVAL OF THE ENGINEER.

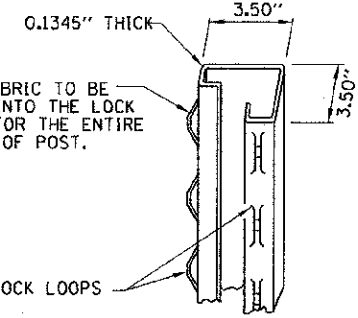


BRACE SECTION 1.25 LBS/LF



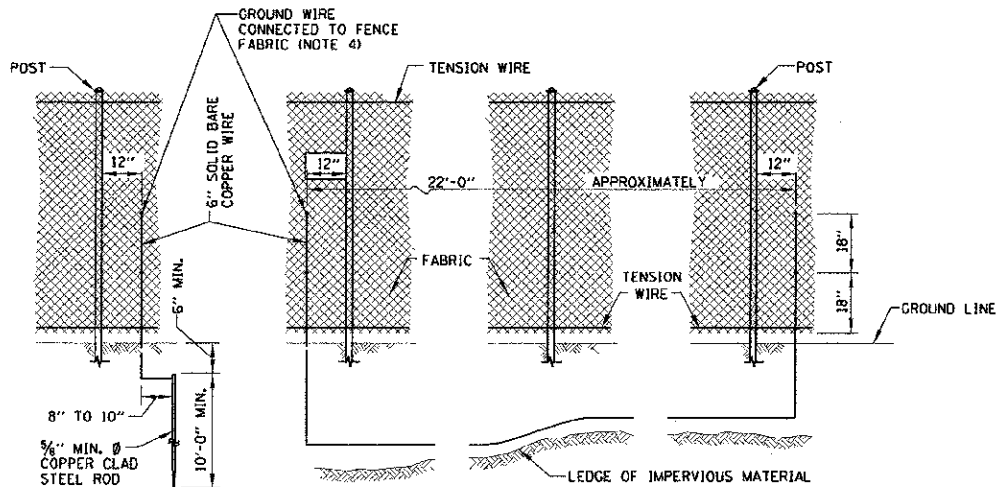
STANDARD "C" - 2.28 LBS/LF
A x B = 1.875" x 1.625"
HEAVY "C" - 2.70 LBS/LF
A x B = 2.250" x 1.625"

LINE POST "C" SECTION



TERMINAL POST SECTION 5.10 LBS/LF

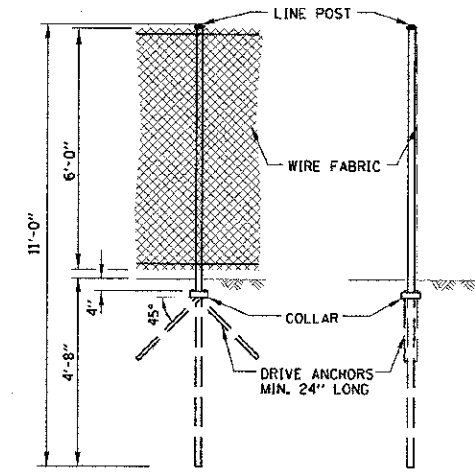
DETAILS OF ROLL FORMED SECTIONS



STANDARD GROUND COUNTERPOISE GROUND (ALTERNATE)

NOTES FOR STANDARD AND COUNTERPOISE GROUND:

1. THE INTERVALS FOR GROUNDING CONTINUOUS FENCING SHALL NOT EXCEED 500 FEET IN URBAN AREAS AND 1000 FEET IN RURAL AREAS. FENCE ADJACENT TO A GATE SHALL BE GROUND A MAXIMUM DISTANCE 100 FEET EACH SIDE OF THE GATE.
2. FENCE CROSSING UNDER A POWER LINE SHALL BE GROUND, ONCE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE AT 25 TO 50 FEET AWAY. FENCE LOCATED DIRECTLY UNDER A TELEPHONE WIRE OR CABLE CROSSING SHALL HAVE A SINGLE GROUND.
3. COUNTERPOISE GROUNDS SHALL BE USED AT LOCATIONS WHERE GROUND RODS CAN NOT BE DRIVEN DUE TO IMPERVIOUS EARTH MATERIALS.
4. THE GROUND WIRES SHALL BE CONNECTED TO FENCE FABRIC AND GROUND ROD BY STAINLESS STEEL BOLTS AND WASHERS. THE LOWER CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE BOTTOM TENSION WIRE.
5. SEE SHEET 2 (OF 2) IN THIS SERIES FOR ADDITIONAL DETAILS AND GENERAL NOTES.



ALTERNATE DRIVEN LINE POST ANCHORAGE WITH OR WITHOUT DRIVE ANCHORS

NOTE FOR FENCE POST:

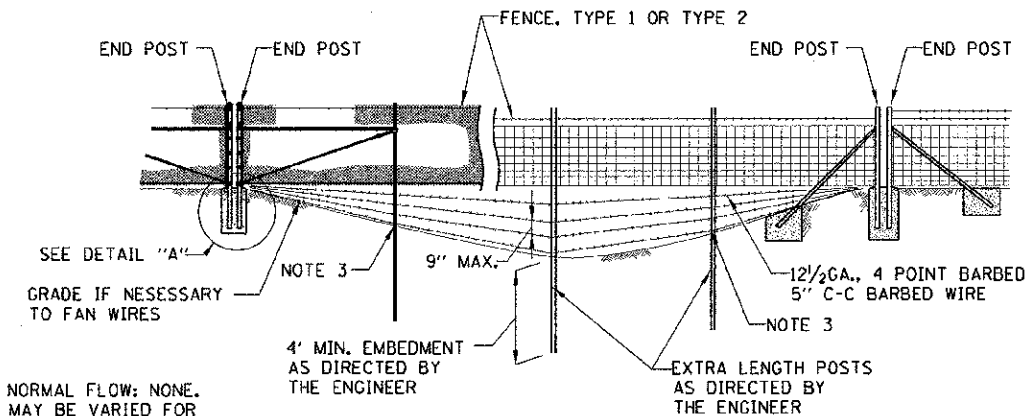
ALTERNATE DRIVEN LINE POST ANCHORAGE IS OPTIONAL. DRIVEN LINE POST ANCHORAGE WITHOUT DRIVE ANCHORS MAY BE USED IN AVERAGE TO GOOD SOIL CONDITIONS. WHEN SOIL IS WEAKER (0.4 < 1.25 TONS/ SQ. FT.) AND STABILITY OF THE POST IS QUESTIONABLE, DRIVE ANCHORS SHALL BE USED. TYPES, SHAPES, DIMENSIONS AND COATING REQUIREMENTS OF DRIVE ANCHORS (ANCHOR BLADES AND COLLARS) FOR DIFFERENT TYPE OF POSTS SHALL BE AS RECOMMENDED BY THE MANUFACTURER.

APPROVED *Paul Kovacs* CHIEF ENGINEER DATE 7-1-2009

ELECTRICAL GROUNDING DETAILS

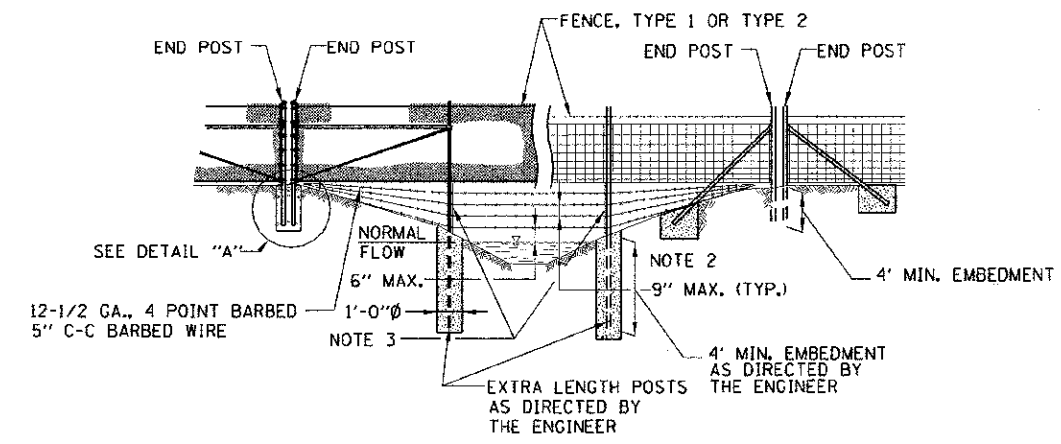
SHEET 2 OF 3

RIGHT OF WAY FENCE
Sheet 17 of 27
CONTRACT NO. 63728
STANDARD D1-01

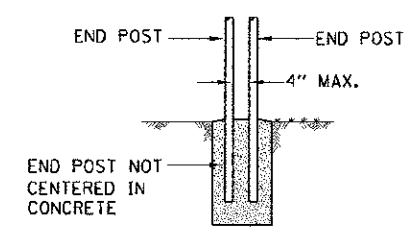


NORMAL FLOW: NONE.
MAY BE VARIED FOR
STEEPER BANKS TO FIT
VARIOUS CHANNEL SECTIONS.

STREAM CROSSING TYPE I



STREAM CROSSING TYPE II

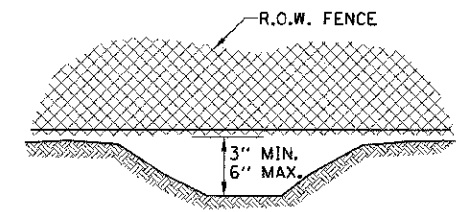


THE FENCE FABRIC SHALL BE REPLACED BY BARBED WIRE STRANDS AT 12" MAXIMUM CENTERS BETWEEN THE END POSTS WHEN SHOWN ON THE PLANS THE BARBED WIRE STRANDS, IF REQUIRED, SHALL BE INCIDENTAL TO THE VARIOUS TYPES OF STREAM CROSSING REQUIRED.

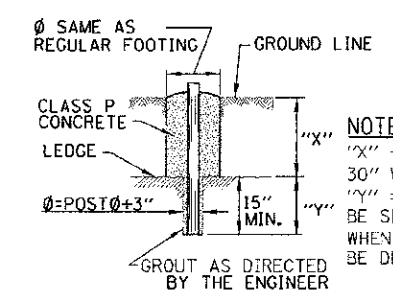
DETAIL A

NOTES FOR STREAM CROSSING TYPE I AND TYPE II:

1. THESE INSTALLATION CONDITIONS ARE TYPICAL AND ARE NOT TO BE CONSTRUED AS REPRESENTATIVE OF ALL CONDITIONS WHICH WILL BE ENCOUNTERED CONSTRUCTION WILL BE VARIED AS REQUIRED OR DIRECTED TO MEET FIELD CONDITIONS.
2. FOR STREAM CROSSING OF THE TYPE REQUIRED THE BOTTOM BARBED WIRE SHALL BE ANCHORED TO CONCRETE FOOTING OR TO HOLES DRILLED IN POSTS, AND INTERMEDIATE WIRES SHALL BE TIED TO THE BOTTOM WIRE AND TO POSTS IN AN EVENLY SPACED FASHION TO PREVENT SLIPPAGE.
3. CONCRETE AND FITTINGS FOR ALL TYPES OF FENCE SHALL BE AS DETAILED FOR SIMILAR CONDITIONS ON STANDARD DRAWING SD XX-8A AND SD XX-8C.

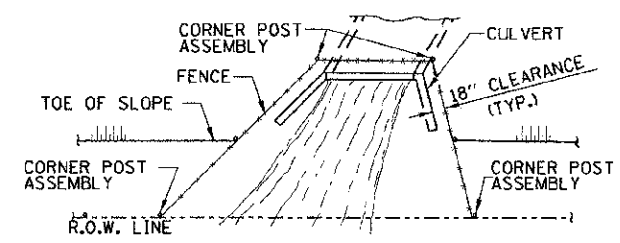


FENCE INSTALLATION OVER DITCH

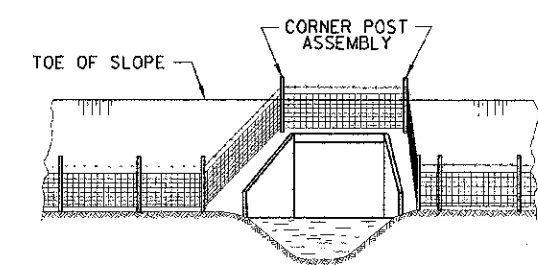


NOTE:
"X" + "Y" SHALL NOT EXCEED 30" WHEN "X" IS 0" TO 15" "Y" = 15", AND THE POST SHALL BE SHORTENED AS REQUIRED. WHEN "X" EXCEEDS 15" "Y" SHALL BE DECREASED ACCORDINGLY.

FOOTING FOR POST WHEN ROCK LEDGE IS ENCOUNTERED



PLAN AT HEADWALL



ELEVATION

NOTES FOR INSTALLATION AROUND HEADWALL:

1. THIS TYPE OF INSTALLATION IS TO BE USED ONLY WHEN SPECIFICALLY CALLED FOR IN THE CONTRACT PLANS.
2. WHEN THE WIDTH OF THE CULVERT MAKES NECESSARY TO ANCHOR A POST TO THE TOP OF THE CULVERT, A CAST IRON SHOE OR OTHER DEVICE APPROVED BY THE ENGINEER SHALL BE USED. THE COST OF ANCHORING THE POST SHALL BE INCIDENTAL TO THE TYPE OF FENCE REQUIRED.

INSTALLATION AROUND HEADWALL

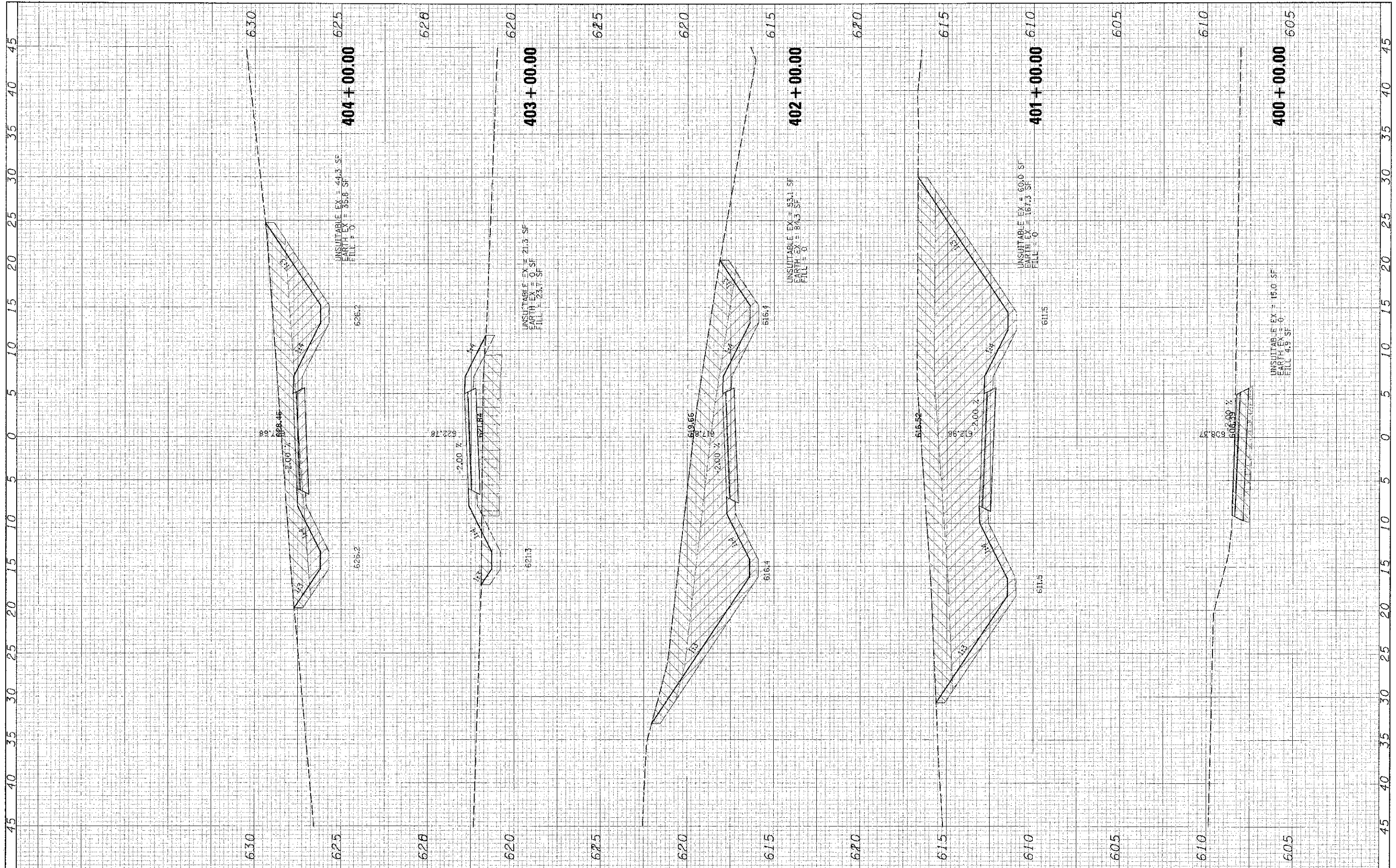


RIGHT OF WAY FENCE
Sheet 18 of 27
CONTRACT NO. 6372B
STANDARD D1-01

APPROVED: *Paul Kovacs*
CHIEF ENGINEER DATE 7-1-2009

FILE	DATE	BY	DATE
DESIGNED	8/20/12	DR	
DRAWN			
CHECKED			
REVISIONS			
NO.	DESCRIPTION	DATE	BY

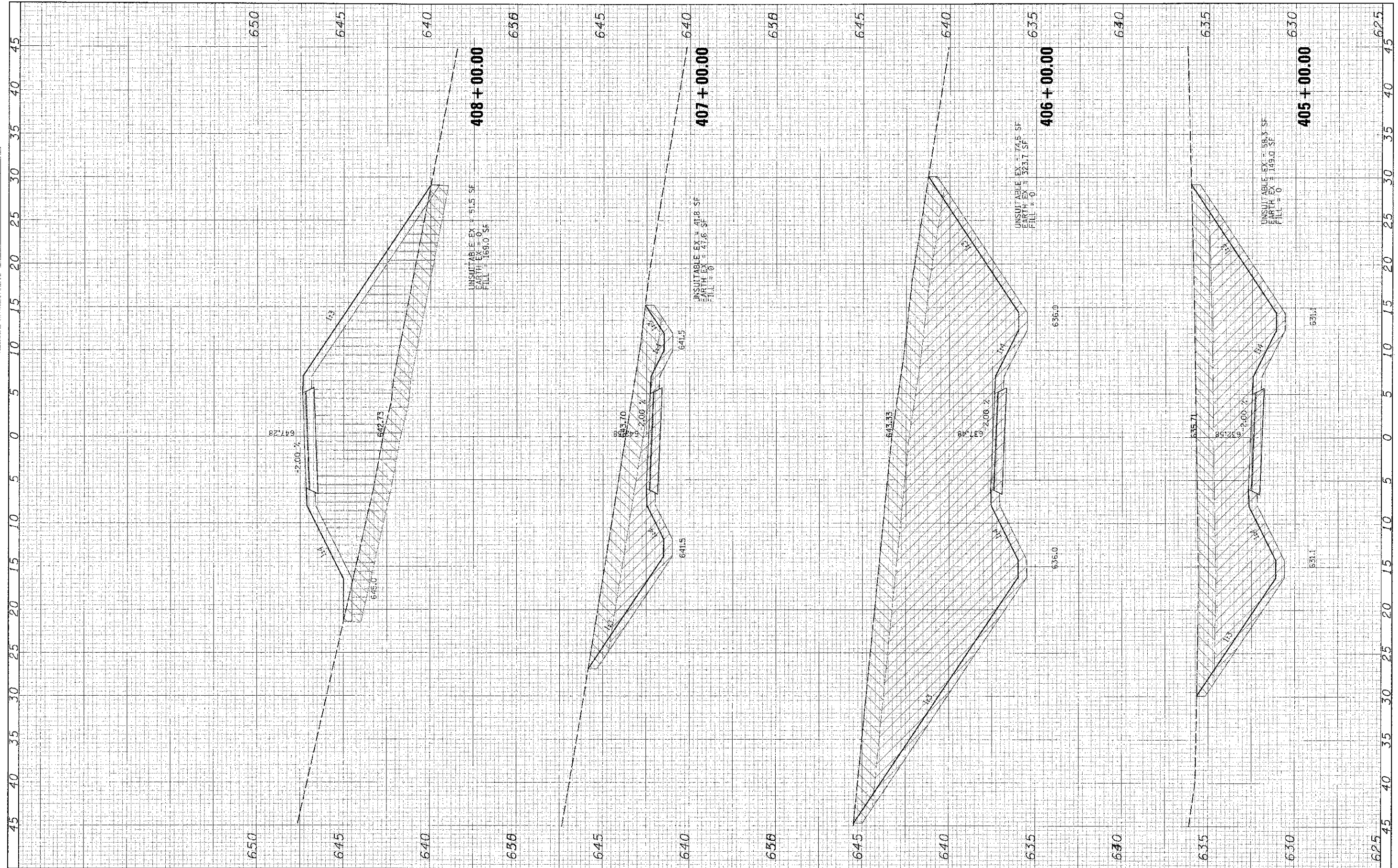
DATE	BY	DATE
8/20/12	DR	



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	PLANT SCALE: 5/32" = 1' IN.	DRAWN: -	REVISIONS: -			
	PLANT DATE: 8/21/2012	CHECKED: -	REVISIONS: -			
		DATE: 8/20/12	REVISIONS: -			

DATE	BY	DATE
DESIGNED		
DRAWN		
CHECKED		
DATE		

DATE	BY	DATE
DESIGNED		
DRAWN		
CHECKED		
DATE		



FILE NAME: 6-FILEP
 USER NAME: David.Lancewear
 PLOT SCALE: 5.0000" = 1' IN.
 PLOT DATE: 8/21/2012

DESIGNED: -
 DRAWN: -
 CHECKED: -
 DATE: 8/20/12

REVISED: -
 REVISED: -
 REVISED: -
 REVISED: -

UNSUITABLE EX = 51.5 SF
 EARTH EX = 0
 FILL = 169.0 SF

UNSUITABLE EX = 41.8 SF
 EARTH EX = 47.6 SF
 FILL = 0

UNSUITABLE EX = 74.5 SF
 EARTH EX = 523.7 SF
 FILL = 0

UNSUITABLE EX = 58.3 SF
 EARTH EX = 149.0 SF
 FILL = 0

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

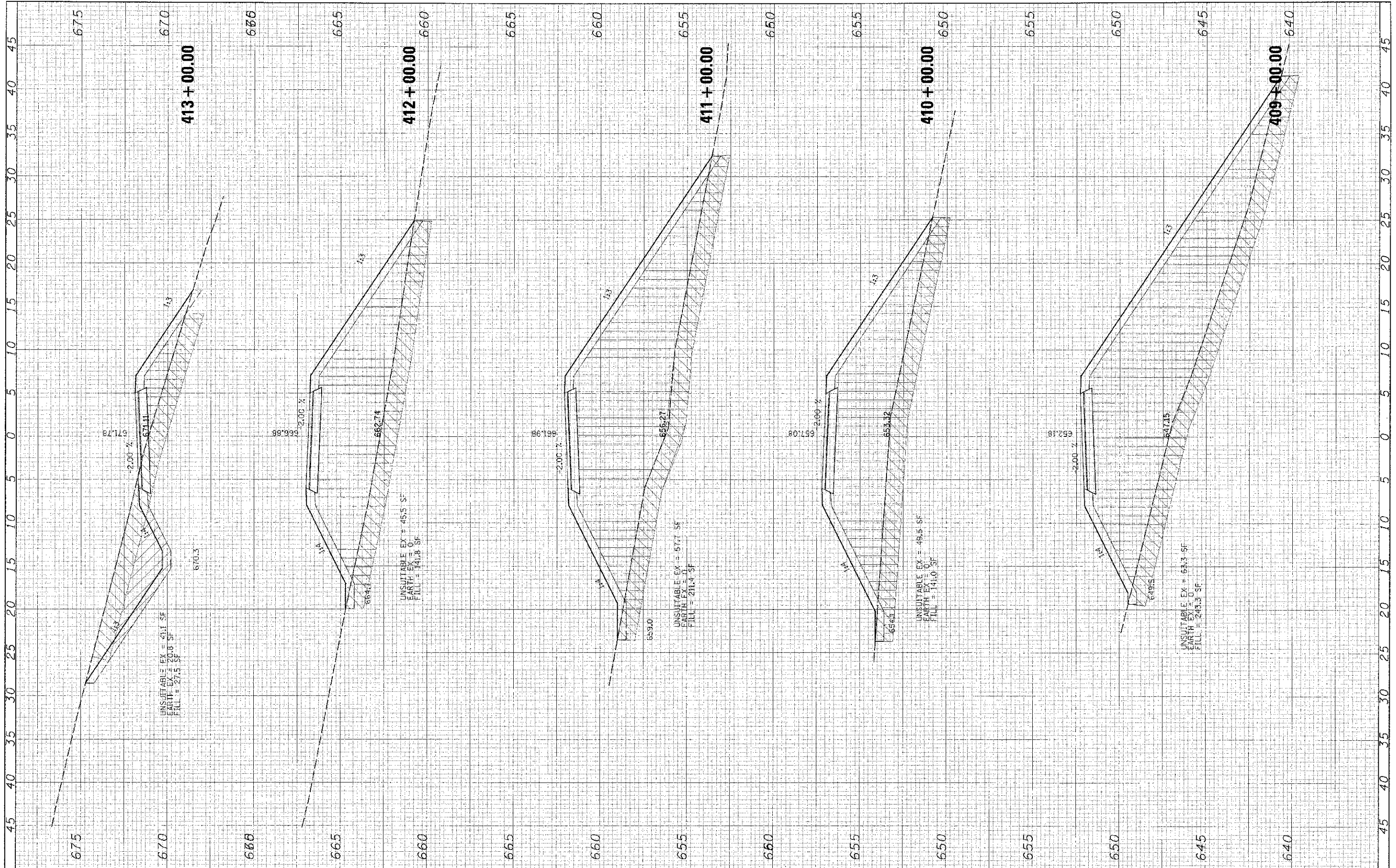
BICYCLE PATH CROSS SECTIONS

SCALE: SHEET NO. OF SHEETS STA. 405+00.00 TO STA. 408+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS
	11-F3000-00-0T	WILL	27
			20
			CONTRACT NO. 63728
			ILLINOIS, FED. AID PROJECT

DATE	NO.
DESIGNED	NO.
CHECKED	NO.
DRAWN	NO.
REVISIONS	NO.

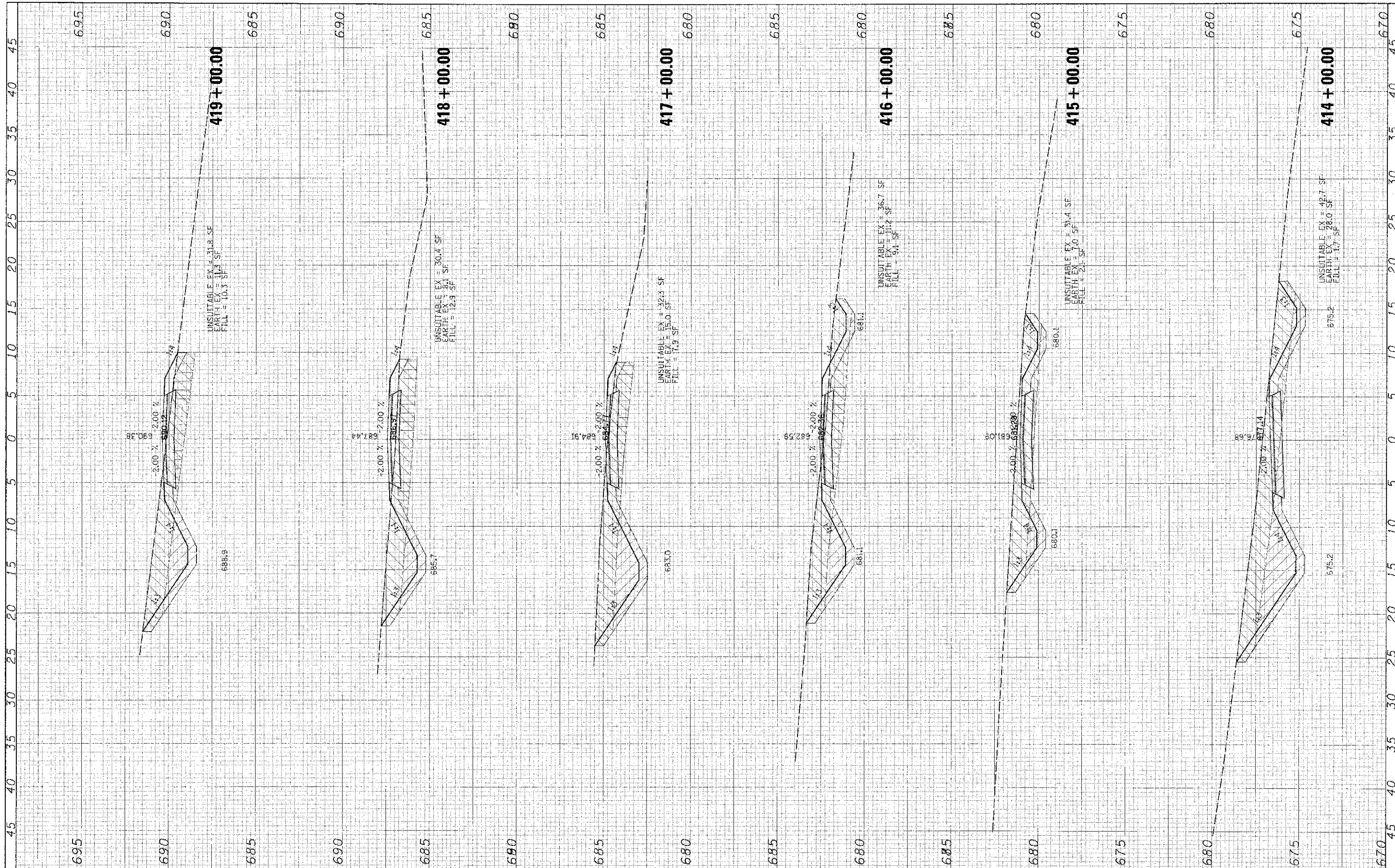
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CHECKED	NO.
DRAWN	NO.
REVISIONS	NO.



FILE NAME: 11-F3000-00-BT	USER NAME: Ryan.Lawlor	DESIGNED: -	REVISIONS: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BICYCLE PATH CROSS SECTIONS SCALE: SHEET NO. OF SHEETS STA. 409+00.00 TO STA. 413+00.00		COUNTY: WILL CONTRACT NO. 6372B
FILE DATE: 8/20/12	DRAWN: -	REVISIONS: -					
FILE SCALE: 5/8" = 1'	CHECKED: -	REVISIONS: -					
FILE DATE: 8/21/2012	DATE: 8/20/12	REVISIONS: -					

DESIGNED	DATE
DRAWN	DATE
CHECKED	DATE
REVISIONS	
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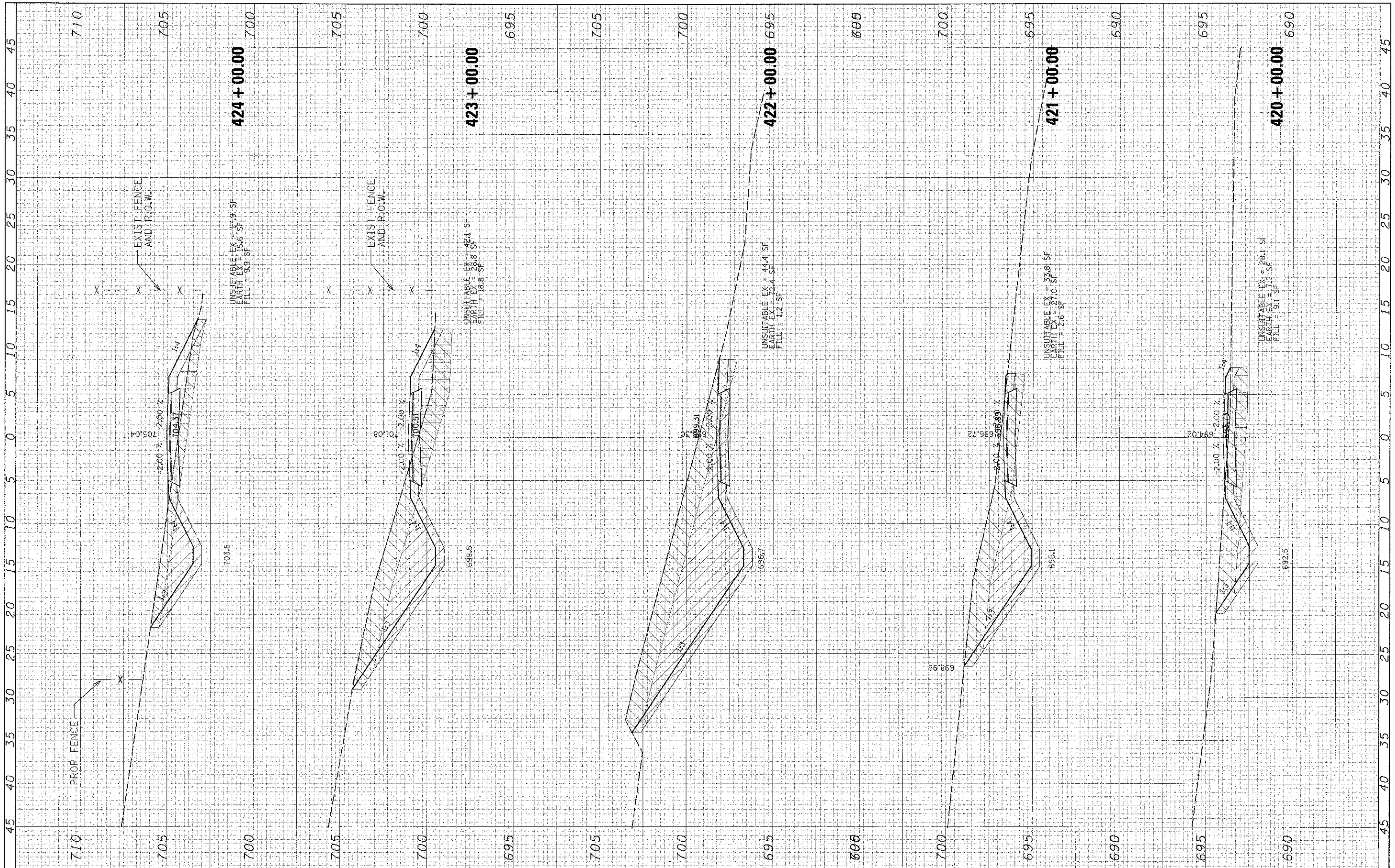
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FILE NAME	USER NAME	DESIGNED	REVISIONS	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BICYCLE PATH CROSS SECTIONS	SCALE: SHEET NO. OF SHEETS: STA. 414+00.00 TO STA. 419+00.00	P.A. RTE. SECTION 11-F3000-00-3T COUNTY WILL CONTRACT NO. 63728
NO.	DATE	DRAWN	REVISIONS				
NO.	DATE	CHECKED	REVISIONS				
NO.	DATE	DATE	REVISIONS				

DATE: _____ BY: _____
 DRAWN: _____
 CHECKED: _____
 DESIGNED: _____
 REVISIONS: _____

PROJECT: _____
 SHEET NO.: _____
 TOTAL SHEETS: _____
 CONTRACT NO.: _____



FILE NAME: _____ #FILE: _____	USER NAME: _____ PLOT SCALE: _____ PLOT DATE: _____	DESIGNED: _____ DRAWN: _____ CHECKED: _____ DATE: 8/20/12	REVISED: _____ REVISED: _____ REVISED: _____ REVISED: _____	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BICYCLE PATH CROSS SECTIONS SCALE: _____ SHEET NO. _____ OF _____ SHEETS STA. 420+00.00 TO STA. 424+00.00	F.A. RTE. _____ SECTION _____ COUNTY _____ WILL 27 23 CONTRACT NO. 63728 ILLINOIS FED. AID PROJECT
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FILE NO.	REVISED	BY	DATE
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DESIGNED	REVISIONS
DRAWN	REVISIONS
CHECKED	REVISIONS
DATE	REVISIONS
8/20/12	

DESIGNED	REVISIONS
DRAWN	REVISIONS
CHECKED	REVISIONS
DATE	REVISIONS
8/20/12	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BICYCLE PATH CROSS SECTIONS

SCALE: SHEET NO. 0F SHEETS STA. 425+00.00 TO STA. 430+00.00

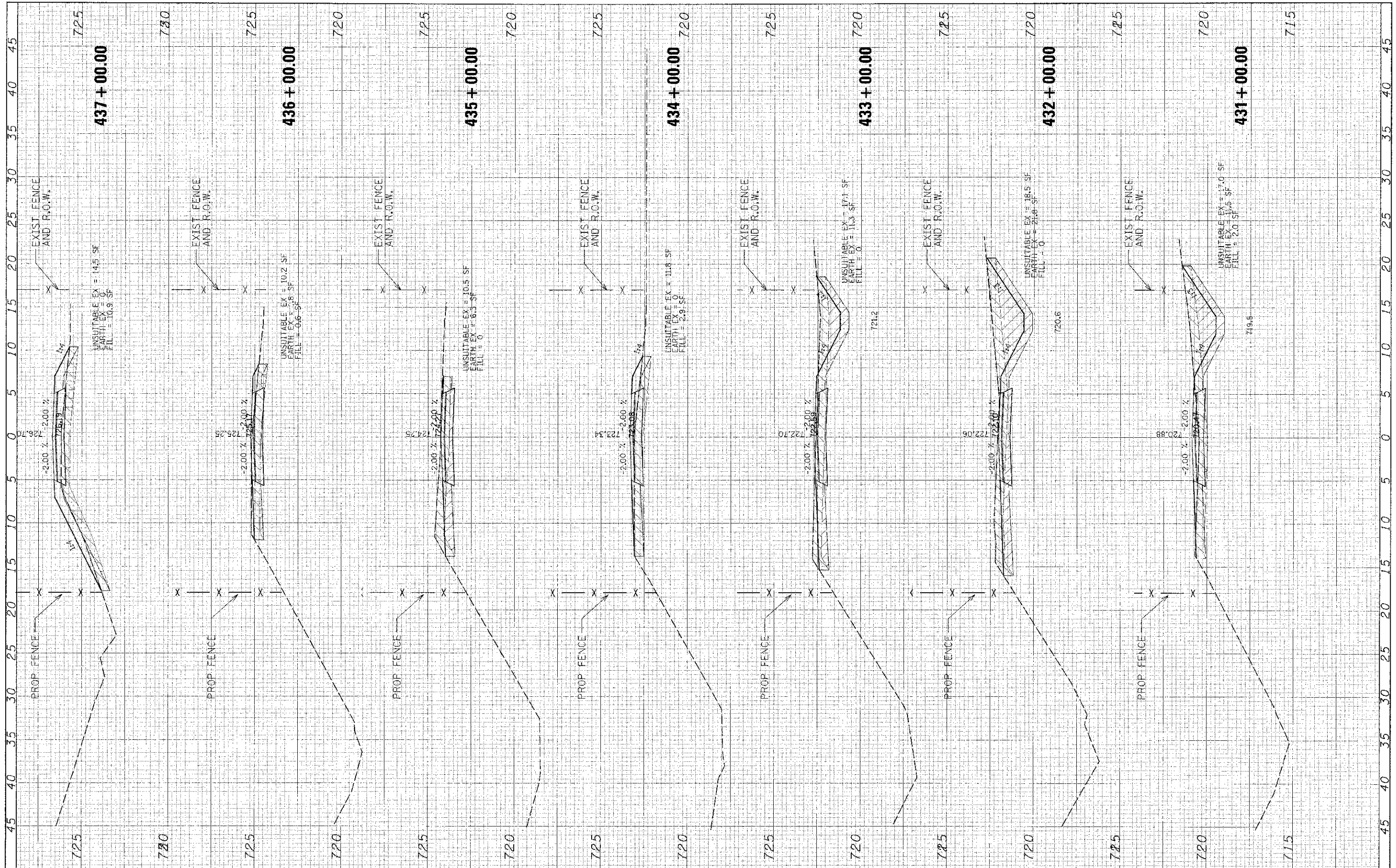
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	11-F3000-00-BT	WILL	27	24
		CONTRACT NO. 63728		
ILLINOIS FED. AID PROJECT				

DATE	3/11/12
DESIGNED	DAVID ANDREWS
DRAWN	DAVID ANDREWS
CHECKED	DAVID ANDREWS
DATE	8/26/12

DATE	3/11/12
DESIGNED	DAVID ANDREWS
DRAWN	DAVID ANDREWS
CHECKED	DAVID ANDREWS
DATE	8/26/12

730

FILE NAME: S1117.dwg
 USER: DAVID ANDREWS
 PLOT SCALE: 1/8" = 1'-0"
 PLOT DATE: 8/26/12



730
45
40
35
30
25
20
15
10
5
0
5
10
15
20
25
30
35
40
45

DESIGNED	DAVID ANDREWS
DRAWN	DAVID ANDREWS
CHECKED	DAVID ANDREWS
DATE	8/26/12

REVISED	
REVISED	
REVISED	
REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BICYCLE PATH CROSS SECTIONS

SCALE: SHEET NO. OF SHEETS STA. 431+00.00 TO STA. 437+00.00

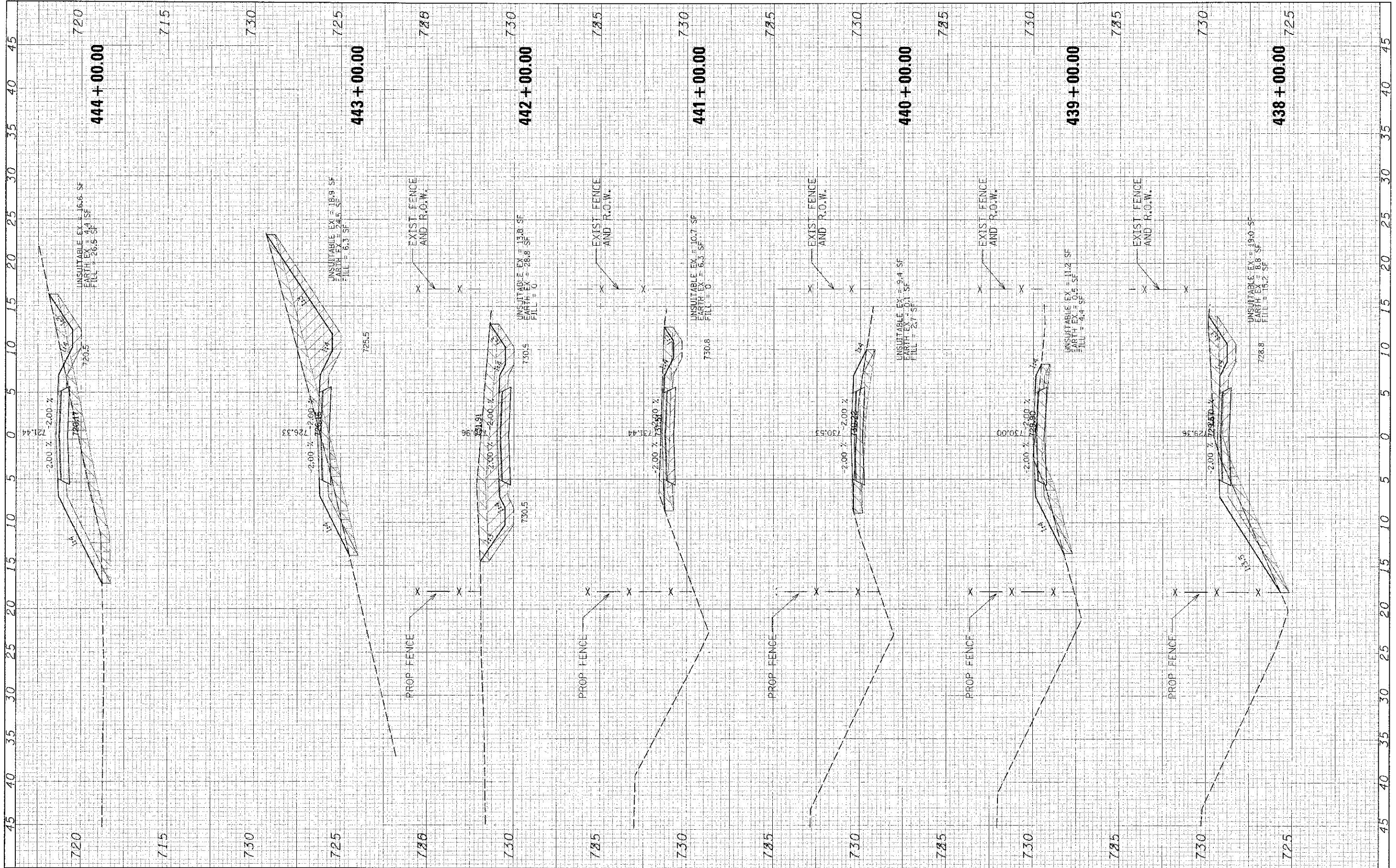
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11-F3000-00-BT	WILL	27	25
CONTRACT NO. 63728			
ILLINOIS FED. AID PROJECT			

DESIGNED BY: []
 DRAWN BY: []
 CHECKED BY: []
 DATE: []

PROJECT NO.: []
 SHEET NO.: []
 OF [] SHEETS

725

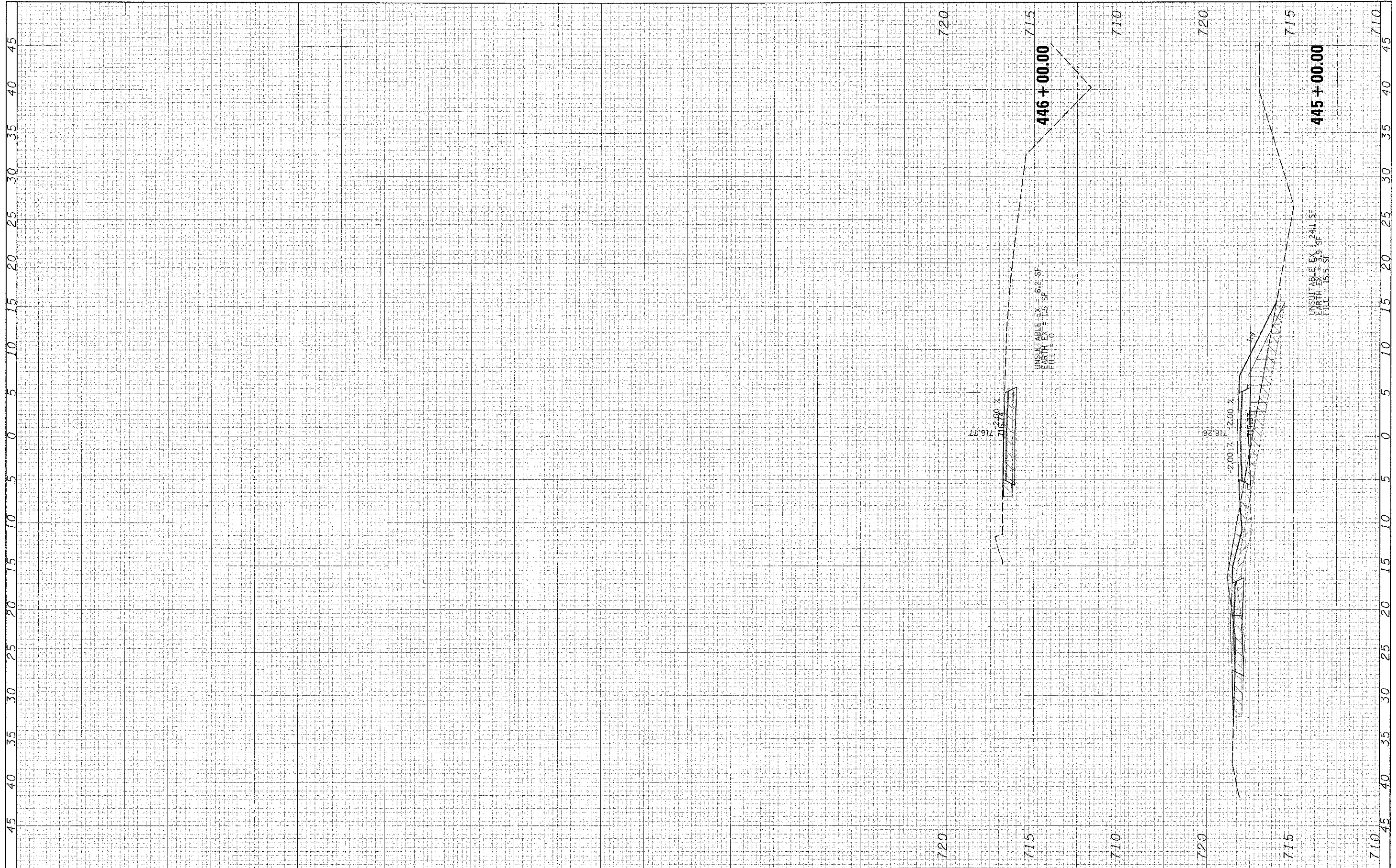
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 DATE: 8/26/12
 CHECKED: [redacted]
 DESIGNED: [redacted]

ORIGINAL: [redacted]
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		DATE: 8/26/12	REVISIONS:				