

| | | | | |
|---------------------|----------------|-----------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1057 | 12-00097-00-RS | COOK | 24 | 1 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED AID PROJECT M-4003(077) | | |

CONTRACT NO. 63740

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF PARK FOREST

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
FAU 1057 (BLACKHAWK DRIVE)
FAU 2830 (MONEE ROAD) TO FAU 1632 (SAUK TRAIL)
RESURFACING
SECTION: 12-00097-00-RS
PROJECT No.: M-4003(077)
VILLAGE OF PARK FOREST
COOK AND WILL COUNTIES
C-91-566-12

INDEX OF SHEETS

| SHEET NO. | TITLE |
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| 2 | HIGHWAY STANDARDS, GENERAL NOTES AND HMA MIXTURE REQUIREMENTS |
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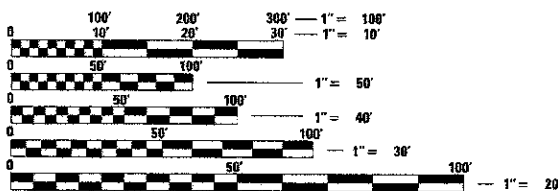


TRAFFIC DATA

BLACKHAWK DRIVE
POSTED SPEED LIMIT = 30 MPH
DESIGN SPEED LIMIT = 35 MPH
2009 ADT = 3,200 VPD

DESIGN DESIGNATION
COLLECTOR

FAU 1057 BLACKHAWK DRIVE
IMPROVEMENT ENDS
STA 96+43



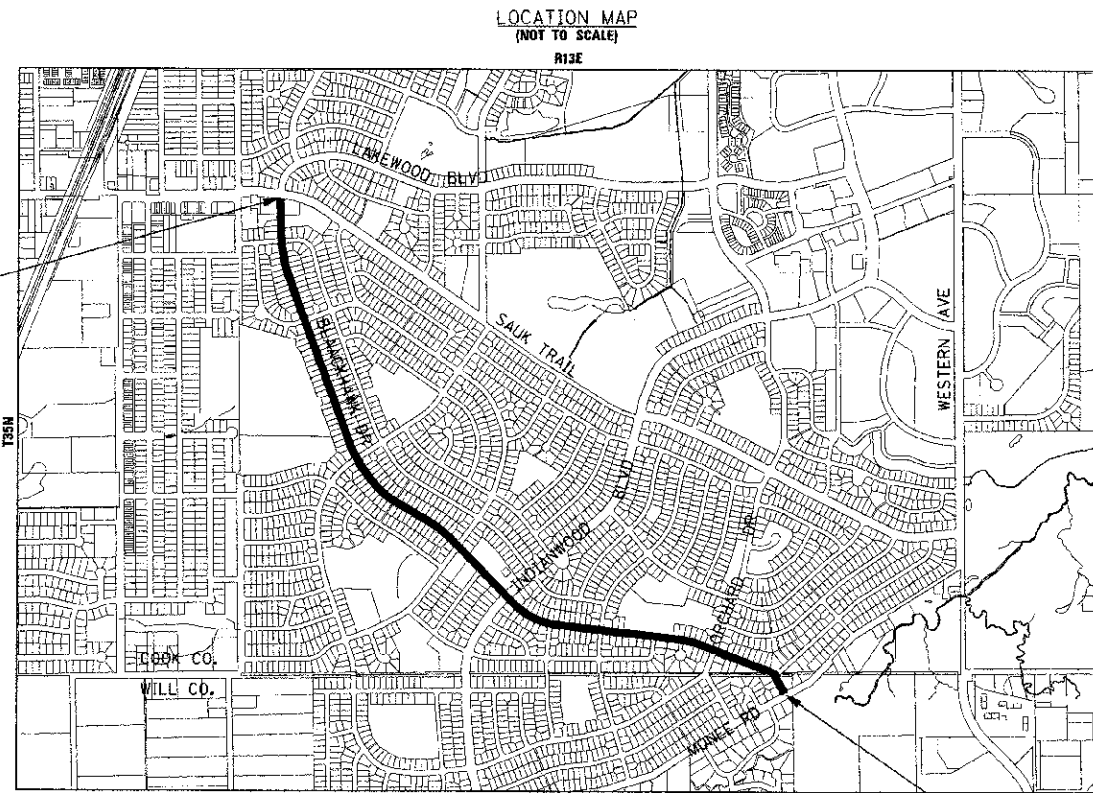
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. A1522373



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:
COUNTY = COOK & WILL
CITY-TWNSHP. = PARK FOREST - RICH & MONEE
SEC. & 1/4 SEC. NO. T35N R13E
48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 63740



SECTION 35 & 36
3RD PRINCIPAL MERIDIAN
RICH & MONEE TOWNSHIPS
GROSS & NET LENGTH OF IMPROVEMENT = 8,613 FEET (1.631 MILES)

FAU 1057 BLACKHAWK DR
IMPROVEMENT BEGINS
STA 10+30

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

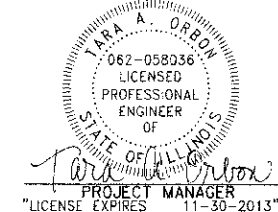
APPROVED August 15, 2012
Village of Park Forest, Director of Public Works

PASSED August 21, 2012
District 1 Engineer of Local Roads and Streets

RELEASING FOR BID
BASED ON LIMITED REVIEW August 27, 2012
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
LICENSE NO. - 184-001121 - EXPIRES 4/30/2013



GENERAL NOTES

1. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
2. THE LOCATIONS OF PUBLIC AND PRIVATE UTILITIES SHOWN ON PLANS ARE APPROXIMATE AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED IN THE STANDARD SPECIFICATIONS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER OR THE OWNER OR REPLACED. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

4. ALL SAW CUTTING SHALL BE INCLUDED IN THE UNIT COST OF THE PAY ITEM INDICATED FOR REMOVAL.

5. ALL CONSTRUCTION PERSONNEL WILL BE REQUIRED TO WEAR A FLUORESCENT VEST PER ARTICLE 701.12 AND 701.13 OF THE STANDARD SPECIFICATIONS AT ALL TIMES WHILE ON THE CONSTRUCTION SITE. COMPLIANCE WITH THIS REQUIREMENT SHALL BE INCLUDED IN THE CONTRACT.

6. NO STREET CLOSURES WILL BE ALLOWED.

7. THE CONTRACTOR SHALL REPLACE ALL STREET SIGNS AND MAIL BOXES REMOVED DURING CONSTRUCTION AS NEAR AS POSSIBLE TO THEIR ORIGINAL LOCATION OR AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE CONTRACT.

8. ALL OPEN HOLE, BROKEN PAVEMENT AND TRENCHES RESULTING FROM STRUCTURE ADJUSTMENTS, OR CURB REPAIRS WORK SHALL BE BACKFILLED TO GRADE BY THE END OF THE DAY.

9. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)

10. 10 FEET TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

11. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGE OF PARK FOREST PUBLIC WORKS DEPARTMENT, AND BAXTER AND WOODMAN, INC.

12. BARRICADE: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAGS ACROSS EACH BOTTOM RAIL.

13. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETING EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

14. ACCESS TO DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ADJACENT CONSTRUCTION ADJACENT THERETO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS. UTILIZING CRUSHED STONE. THIS WORK SHALL BE INCLUDED IN THE COST OF DRIVEWAY PAVEMENT REMOVAL.

15. THE CONTRACTOR SHALL NOTIFY VILLAGE PUBLIC WORKS AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.

16. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE VILLAGE, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.

17. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING" BAN IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE OF PARK FOREST WATER IF DEEMED NECESSARY.

18. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.

19. IN AREAS WHERE THE EXISTING DRIVEWAY OR SIDEWALK TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK OR CURB & CUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE PAY ITEM FOR DRIVEWAY PAVEMENT REMOVAL.

20. THE CONTRACTOR SHALL LIMIT THE WORK TO ONE SIDE OF THE STREET UNLESS AS DIRECTED OTHERWISE BY THE ENGINEER.

21. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".

22. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE CONTRACTOR SHALL PRIME IMMEDIATELY PRECEDING THE PAVING OPERATION.

23. THE LOCATIONS OF THE CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

24. THE DAYS PAVING OPERATION SHALL RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.

25. ANY ANTI-STRIPPING ADDITIVE REQUIRED SHALL BE INCLUDED IN THE COST OF THE SURFACE COURSE.

26. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HOT-MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

27. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.

28. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.

29. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25-INCH ABOVE THE GUTTER FLAG FOR ENTIRE RESURFACING.

30. NEW OR REPLACEMENT CLOSED LIDS SHALL BE STAMPED TO INDICATE THE STRUCTURE TYPE. STORM LIDS SHALL BE STAMPED WITH "STORM", SANITARY LIDS SHALL BE STAMPED WITH "SANITARY" AND WATER VALVE VAULT LIDS SHALL BE STAMPED WITH "WATER". STAMPING SHALL BE INCLUDED IN THE COST OF THE NEW LID. ALL NEW TYPE 1 OPEN LIDS SHALL BE BICYCLE SAFE.

31. CONNECTION OF PROPOSED STORM SEWER INTO EXISTING STORM SEWER OR EXISTING STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF STORM SEWERS.

32. CONNECTION OF EXISTING STORM SEWER INTO PROPOSED STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE. ANY ADDITIONAL STORM SEWER PIPE REQUIRED TO MAKE THE CONNECTION SHALL BE OF THE SAME SIZE AND MATERIAL TYPE AS THE EXISTING STORM SEWER AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE.

33. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS."

34. ANY DAMAGE TO PAVEMENT, SIDEWALK, CURB OR ANY OTHER PORTION OF THE ROADWAY NOT SPECIFICALLY TO BE REMOVED AND REPLACED SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL CHARGE.

35. CURB AND GUTTER AND DRIVEWAYS PROVIDING ACCESS SHALL BE REMOVED AND REPLACED WITHIN 3 DAYS.

36. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.

37. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE EXISTING STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR MANHOLES TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR MANHOLES TO BE ADJUSTED.

38. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET. THIS WORK SHALL BE INCLUDED IN THE COST OF SIDEWALK.

39. A 1/2-INCH EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK-5 INCH.

40. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE "ARMOR-TILE" 24"x48" NOMINAL PANEL WIDTH AS MANUFACTURED BY "ENGINEERED PLASTICS INC." (800) 682-2525 OF WILLIAMSVILLE, NY OR AN APPROVED EQUAL. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE PANEL COLOR SHALL BE SELECTED BY THE ENGINEER. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.

41. THE CONTRACTOR SHALL INFORM THE CCHD DESIGN ENGINEER (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS NOTICE IS REQUIRED TO SCHEDULE MAINTENANCE TRANSFER OF THE TRAFFIC SIGNAL.

42. THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS S1 CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG. IF THE CONCRETE IS POURED HIGHER THAN 2-1/2" FROM THE GUTTER FLAG FOR STREETS TO BE RESURFACED, THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED 2-1/2" DEPTH. THE AREA BEHIND THE PROPOSED CURB AND GUTTER SHALL BE RESTORED WITH NATIVE SOD IN ACCORDANCE WITH ARTICLE 1081.03 RATHER THAN SALT TOLERANT SOD.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

| ITEM | AIR VOIDS @ Ndes |
|---|------------------|
| RESURFACING | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm) - 1 1/2" | 4% @ 50 Gyr |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50 - VARIES 3/4" TO 1 3/4" | 3.5% @ 50 Gyr |
| PATCHING | |
| CLASS D PATCHES (HMA BINDER IL-19 mm) - 6" (2 LIFTS) | 4% @ 70 Gyr |
| DRIVEWAY | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm) - 4" (2 LIFTS) | 4% @ 50 Gyr |
| INCIDENTAL | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm) | 4% @ 50 Gyr |

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22 AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PC 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS." FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

HIGHWAY STANDARDS

| | |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 424001-06 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 424006 | DIAGONAL CURB RAMPS FOR SIDEWALKS |
| 424011 | CORNER PARALLEL CURB RAMPS FOR SIDEWALKS |
| 424016 | MID-BLOCK CURB RAMPS FOR SIDEWALKS |
| 424021 | DEPRESSED CORNER FOR SIDEWALKS |
| 442201-03 | CLASS C AND D PATCHES |
| 602301-03 | INLET, TYPE A |
| 602306-03 | INLET, TYPE B |
| 604001-03 | FRAMES AND LIDS, TYPE 1 |
| 604006-04 | FRAME AND GRATE, TYPE 3 |
| 604066-02 | FRAME AND LID, TYPE 15 |
| 701501-06 | URBAN LANE CLOSURE, 2L 2W UNDIVIDED |
| 701701-08 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-05 | LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE |
| 701901-02 | TRAFFIC CONTROL DEVICES |
| 886001-01 | DETECTOR LOOP INSTALLATION |
| 886006-01 | TYPICAL LAYOUTS FOR DETECTOR LOOPS |

CONTRACT NO. 120499-GEN NOTICES
 VILLAGE OF PARK FOREST, ILLINOIS
 BLACKHAWK DRIVE RESURFACING
 DATE: 08/17/12
 DESIGNED: DNP
 DRAWN: BCD
 CHECKED: TAO
 DATE: 08/17/12
 REVISED: REVISED
 REVISED: REVISED
 FILE: 120499-GenNotices.sht
 SCALE: NONE
 STA. TO STA.
 F.A.U. RTE. 1057
 SECTION 12-00091-00-R5
 COUNTY COOK-WILL 24
 SHEETS TOTAL SHEETS NO. 2
 CONTRACT NO. 63740
 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4303(677)



| | |
|-----------------|------------------------------|
| DESIGNED - DNP | REVISED |
| DRAWN - BCD | REVISED |
| CHECKED - TAO | REVISED |
| DATE - 08/17/12 | FILE - 120499-GenNotices.sht |

**VILLAGE OF PARK FOREST, ILLINOIS
BLACKHAWK DRIVE
RESURFACING**

**GENERAL NOTES, HOT-MIX ASPHALT MIXTURE
REQUIREMENTS AND HIGHWAY STANDARDS**

| | | | | |
|------------------|------------------------|---------------------|---|-------------|
| F.A.U. RTE. 1057 | SECTION 12-00091-00-R5 | COUNTY COOK-WILL 24 | TOTAL SHEETS NO. 2 | SHEET NO. 2 |
| SCALE: NONE | | | CONTRACT NO. 63740 | |
| STA. TO STA. | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4303(677) | |

DRIVEWAY SCHEDULE

| ADDRESS | HMA DRIVEWAY PAVEMENT, 4" (SQ YD) | PCC DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT (SQ YD) | HMA DRIVEWAY PAVEMENT REMOVAL (SQ YD) |
|--------------|-----------------------------------|---|---------------------------------------|
| 52 | 7.2 | | 7.2 |
| 53 | | 13.6 | |
| 55 | | 8.9 | |
| 57 | | 7.9 | |
| 62 | 8.4 | | 8.4 |
| 73 | | 23.7 | |
| 64/66 | 17.1 | | 17.1 |
| 68/70 | | 12.6 | |
| 73 | | 40.9 | |
| 72/74 | 20.0 | | 20.0 |
| 75/77 | 17.6 | | 17.6 |
| 76 | 3.9 | | 3.9 |
| 78 | 3.7 | | 3.7 |
| 80 | | 3.9 | |
| 401 | | 5.8 | |
| 358 | | 7.6 | |
| 102 | | 7.0 | |
| 103 | | 6.5 | |
| 104 | | 8.1 | |
| 105 | | 12.8 | |
| 107 | 12.8 | | 12.8 |
| 106 | | 9.3 | |
| 108 | | 6.7 | |
| 110 | | 7.9 | |
| 112 | | 7.8 | |
| 121 | 11.1 | | 11.1 |
| 123 | | 11.3 | |
| 130 (SCHOOL) | 8.1 | | 8.1 |
| 130 (SCHOOL) | 34.4 | | 34.4 |
| 125 | 7.6 | | 7.6 |
| 127 | | 7.0 | |
| 129 | | 8.0 | |
| 130 (SCHOOL) | 26.1 | | 26.1 |
| 134 | | 15.3 | |
| 131 | | 5.7 | |
| 133 | | 12.2 | |
| 136 | 6.8 | | 6.8 |
| 138 | | 7.2 | |
| 135 | | 16.2 | |
| 143 | 9.4 | | 9.4 |
| 144/146 | 15.7 | | 15.7 |
| 145/147 | 15.4 | | 15.4 |
| 148/150 | 18.0 | | 18.0 |
| 152/154 | 33.0 | | 33.0 |
| 149/151 | 17.8 | | 17.8 |
| 153 | 13.9 | | 13.9 |
| 156/158 | 23.6 | | 23.6 |
| 157 | | 8.3 | |
| 159 | 7.2 | | 7.2 |
| 161 | | 13.1 | |
| 160/162 | 20.2 | | 20.2 |
| 164/166 | 42.8 | | 42.8 |
| 163 | 5.2 | | 5.2 |
| 165 | 6.8 | | 6.8 |
| 168 | 6.7 | | 6.7 |
| 167 | 7.9 | | 7.9 |
| 169 | | 7.3 | |
| 201 | 12.4 | | 12.4 |
| 211 | | 8.9 | |
| 217 | 9.8 | | 9.8 |
| 217 | 7.8 | | 7.8 |
| DRIVE | 47.1 | | 47.1 |
| 230 | 12.8 | | 12.8 |
| 230 | | 9.3 | |
| 233/235 | 17.4 | | 17.4 |
| 232/234 | 16.0 | | 16.0 |
| 237 | 6.8 | | 6.8 |
| 236 | 6.9 | | 6.9 |
| 238 | 16.1 | | 16.1 |
| 239/241 | 12.2 | | 12.2 |
| 240/242 | 16.8 | | 16.8 |
| 243 | 6.6 | | 6.6 |
| 245/247 | 15.7 | | 15.7 |
| 249/251 | 16.7 | | 16.7 |

DRIVEWAY SCHEDULE (CONT.)

| ADDRESS | HMA DRIVEWAY PAVEMENT, 4" (SQ YD) | PCC DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT (SQ YD) | HMA DRIVEWAY PAVEMENT REMOVAL (SQ YD) |
|------------|-----------------------------------|---|---------------------------------------|
| 244 | 6.4 | | 6.4 |
| 246 | 6.9 | | 6.9 |
| 248/250 | 19.6 | | 19.6 |
| 253/255 | 14.2 | | 14.2 |
| 252/254 | 15.3 | | 15.3 |
| 257 | 9.8 | | 9.8 |
| 259/261 | 16.8 | | 16.8 |
| 256/258 | 17.4 | | 17.4 |
| 260 | 7.1 | | 7.1 |
| 262 | 6.8 | | 6.8 |
| 263/265 | 16.7 | | 16.7 |
| 267 | 7.6 | | 7.6 |
| 264 | 8.0 | | 8.0 |
| 266 | 6.2 | | 6.2 |
| 269 | | 9.7 | |
| 268/270 | 15.7 | | 15.7 |
| 271/273 | 13.1 | | 13.1 |
| 302 | 7.2 | | 7.2 |
| 303 | 6.6 | | 6.6 |
| 305/307 | 16.7 | | 16.7 |
| 304/306 | 15.8 | | 15.8 |
| 309/311 | 16.3 | | 16.3 |
| 312/314 | 17.0 | | 17.0 |
| 313/315 | 16.0 | | 16.0 |
| 316 | | 6.2 | |
| 318 | 11.1 | | 11.1 |
| 320 | 11.4 | | 11.4 |
| 321/323 | 17.8 | | 17.8 |
| 322/324 | 16.8 | | 16.8 |
| 325/327 | 15.8 | | 15.8 |
| 326/328 | 36.7 | | 36.7 |
| 329/331 | 15.1 | | 15.1 |
| 330/332 | 14.6 | | 14.6 |
| 333/335 | | 16.3 | |
| 334/336 | 15.8 | | 15.8 |
| 337 | 7.8 | | 7.8 |
| 338 | 7.3 | | 7.3 |
| 339 | 8.9 | | 8.9 |
| 340 | 7.2 | | 7.2 |
| 341 | 8.7 | | 8.7 |
| 343 | | 7.2 | |
| 342 | 7.3 | | 7.3 |
| 344 | 6.1 | | 6.1 |
| 345 | 7.7 | | 7.7 |
| 353 | 6.9 | | 6.9 |
| 355 | 9.8 | | 9.8 |
| 352/354 | 21.3 | | 21.3 |
| 357/359 | 17.0 | | 17.0 |
| 356/358 | 33.9 | | 33.9 |
| 361/363 | 15.6 | | 15.6 |
| 360/362 | | 16.0 | |
| 365/367 | 15.7 | | 15.7 |
| 364/366 | 16.8 | | 16.8 |
| 369/371 | 19.4 | | 19.4 |
| 368 | | 8.1 | |
| 370 | 7.2 | | 7.2 |
| 373 | 12.0 | | 12.0 |
| 375 | 12.9 | | 12.9 |
| 372/374 | 16.7 | | 16.7 |
| N MIAMI RT | 11.6 | | 11.6 |
| 377 | 11.3 | | 11.3 |
| 381 | 23.7 | | 23.7 |
| 381 | 14.3 | | 14.3 |
| 382/384 | 24.1 | | 24.1 |
| GAS (RT) | 22.0 | | 22.0 |

HMA DRIVEWAY PAVEMENT 4" TOTAL= 1,462.6 SQ YD
 PCC DRIVEWAY REMOVAL AND REPLACEMENT TOTAL= 384.4 SQ YD
 HMA DRIVEWAY PAVEMENT REMOVAL= 1,462.6 SQ YD

| INLET FILTERS | | |
|---------------|--------|------|
| STATION | O/S | EACH |
| 12+01 | 1' LT | 1 |
| 12+21 | RT | 1 |
| 12+73 | RT | 1 |
| 15+68 | LT | 1 |
| 15+68 | RT | 1 |
| 19+40 | 23' LT | 1 |
| 19+46 | LT | 1 |
| 19+52 | 31' RT | 1 |
| 19+57 | 50' RT | 1 |
| 19+75 | 21' RT | 1 |
| 19+92 | RT | 1 |
| 20+08 | LT | 1 |
| 20+08 | RT | 1 |
| 23+52 | RT | 1 |
| 23+54 | 1' LT | 1 |
| 23+82 | LT | 1 |
| 26+50 | RT | 1 |
| 26+57 | 1' LT | 1 |
| 26+80 | LT | 1 |
| 29+37 | RT | 1 |
| 29+37 | LT | 1 |
| 29+45 | 1' LT | 1 |
| 36+58 | 28' RT | 1 |
| 38+00 | 43' LT | 1 |
| 38+15 | 2' LT | 1 |
| 38+18 | RT | 1 |
| 38+19 | 41' LT | 1 |
| 40+60 | 30' RT | 1 |
| 41+00 | CL | 1 |
| 43+41 | RT | 1 |
| 43+94 | CL | 1 |
| 44+37 | RT | 1 |
| 48+05 | CL | 1 |
| 51+19 | LT | 1 |
| 51+30 | RT | 1 |
| 51+65 | CL | 1 |
| 52+00 | 23' RT | 1 |
| 54+77 | 1' RT | 1 |
| 54+84 | 32' RT | 1 |
| 55+16 | RT | 1 |
| 55+16 | LT | 1 |
| 60+96 | 25' RT | 1 |
| 65+05 | CL | 1 |
| 65+08 | LT | 1 |
| 65+13 | RT | 1 |
| 66+66 | CL | 1 |
| 66+90 | 30' LT | 1 |
| 66+95 | RT | 1 |
| 67+00 | LT | 1 |
| 67+08 | RT | 1 |
| 72+84 | LT | 1 |
| 73+18 | 1' LT | 1 |
| 73+19 | RT | 1 |
| 76+19 | 27' RT | 1 |
| 79+70 | RT | 1 |
| 79+70 | LT | 1 |
| 79+75 | 22' RT | 1 |
| 79+82 | CL | 1 |
| 79+83 | 25' RT | 1 |
| 82+77 | 40' RT | 1 |
| 82+78 | 27' RT | 1 |
| 82+90 | CL | 1 |
| 83+06 | 40' RT | 1 |
| 85+94 | 25' RT | 1 |
| 87+46 | CL | 1 |
| 87+68 | RT | 1 |
| 87+68 | LT | 1 |
| 89+49 | 17' RT | 1 |
| 91+83 | 34' LT | 1 |
| 91+96 | 44' RT | 1 |
| 92+08 | 1' RT | 1 |
| 92+18 | 44' RT | 1 |
| 96+46 | RT | 1 |

TOTAL: 73

| FRAMES AND LIDS, TYPE 1 | | |
|-------------------------|--------|------|
| CLOSED LIDS | | |
| STATION | O/S | EACH |
| 41+00 | CL | 1 |
| 43+94 | CL | 1 |
| 51+65 | CL | 1 |
| 54+77 | 1' RT | 1 |
| 66+66 | CL | 1 |
| 79+82 | CL | 1 |
| 82+90 | CL | 1 |
| 87+46 | CL | 1 |
| SUBTOTAL: 8 | | |
| OPEN LIDS | | |
| STATION | O/S | EACH |
| 20+08 | LT | 1 |
| 38+00 | 43' LT | 1 |
| SUBTOTAL: 2 | | |

| SANITARY MANHOLES TO BE ADJUSTED | | |
|----------------------------------|--------|------|
| STATION | O/S | EACH |
| 19+52 | 31' RT | 1 |
| 36+58 | 28' RT | 1 |
| 40+60 | 30' RT | 1 |
| 52+00 | 23' RT | 1 |
| 54+84 | 32' RT | 1 |
| 60+96 | 25' RT | 1 |
| 66+95 | 30' RT | 1 |
| 76+19 | 27' RT | 1 |
| 79+75 | 22' RT | 1 |
| 82+78 | 27' RT | 1 |
| 85+94 | 25' RT | 1 |
| 89+49 | 17' RT | 1 |
| TOTAL: 12 | | |

| INLETS TO BE ADJUSTED | | |
|-----------------------|--------|------|
| STATION | O/S | EACH |
| 12+21 | RT | 1 |
| 12+73 | RT | 1 |
| 15+68 | LT | 1 |
| 15+68 | RT | 1 |
| 19+57 | 50' RT | 1 |
| 19+92 | 50' RT | 1 |
| 20+08 | LT | 1 |
| 20+08 | RT | 1 |
| 23+52 | RT | 1 |
| 23+82 | LT | 1 |
| 26+50 | RT | 1 |
| 26+80 | LT | 1 |
| 29+37 | RT | 1 |
| 29+37 | LT | 1 |
| 38+00 | 43' LT | 1 |
| 38+18 | RT | 1 |
| 38+19 | 41' LT | 1 |
| 43+41 | RT | 1 |
| 44+37 | RT | 1 |
| 51+19 | LT | 1 |
| 51+30 | RT | 1 |
| 65+08 | LT | 1 |
| 65+13 | RT | 1 |
| 67+00 | LT | 1 |
| 67+08 | RT | 1 |
| 72+84 | LT | 1 |
| 73+18 | 1' LT | 1 |
| 73+19 | RT | 1 |
| 76+19 | 27' RT | 1 |
| 79+70 | RT | 1 |
| 79+70 | LT | 1 |
| 79+75 | 22' RT | 1 |
| 79+82 | CL | 1 |
| 79+83 | 25' RT | 1 |
| 82+77 | 40' RT | 1 |
| 82+78 | 27' RT | 1 |
| 82+90 | CL | 1 |
| 83+06 | 40' RT | 1 |
| 85+94 | 25' RT | 1 |
| 87+46 | CL | 1 |
| 87+68 | RT | 1 |
| 87+68 | LT | 1 |
| 89+49 | 17' RT | 1 |
| 91+83 | 34' LT | 1 |
| 91+96 | 44' RT | 1 |
| 92+08 | 1' RT | 1 |
| 92+18 | 44' RT | 1 |
| 96+46 | RT | 1 |
| TOTAL: 36 | | |

| VALVE BOXES TO BE ADJUSTED | | |
|----------------------------|--------|------|
| STATION | O/S | EACH |
| 19+40 | 23' LT | 1 |
| 19+46 | LT | 1 |
| 86+90 | 30' LT | 1 |
| TOTAL: 3 | | |

| MANHOLES TO BE ADJUSTED | | |
|-------------------------|--------|------|
| STATION | O/S | EACH |
| 12+01 | 1' LT | 1 |
| 19+76 | 21' RT | 1 |
| 23+54 | 1' LT | 1 |
| 26+57 | 1' LT | 1 |
| 29+45 | 1' LT | 1 |
| 38+15 | 2' LT | 1 |
| 41+00 | CL | 1 |
| 43+94 | CL | 1 |
| 48+05 | CL | 1 |
| 51+65 | CL | 1 |
| 54+77 | 1' RT | 1 |
| 65+05 | CL | 1 |
| 66+66 | CL | 1 |
| 73+18 | 1' LT | 1 |
| 79+82 | CL | 1 |
| 82+90 | CL | 1 |
| 87+46 | CL | 1 |
| 91+83 | 34' RT | 1 |
| 92+08 | 1' RT | 1 |
| TOTAL: 19 | | |

| INLETS, TYPE A, TYPE 1 FRAME, OPEN LID | | |
|--|-----|------|
| STATION | O/S | EACH |
| 55+16 | RT | 1 |
| 56+16 | LT | 1 |
| TOTAL: 2 | | |

| FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | | |
|--|--------|------|
| STATION | O/S | EACH |
| 79+83 | 25' RT | 1 |
| TOTAL: 1 | | |

| TREE ROOT PRUNING | | |
|-------------------|-----|------|
| STATION | O/S | EACH |
| 18+34 | LT | 1 |
| 18+51 | LT | 1 |
| 18+89 | LT | 1 |
| 19+05 | LT | 1 |
| 19+18 | LT | 1 |
| 19+31 | LT | 1 |
| TOTAL: 6 | | |

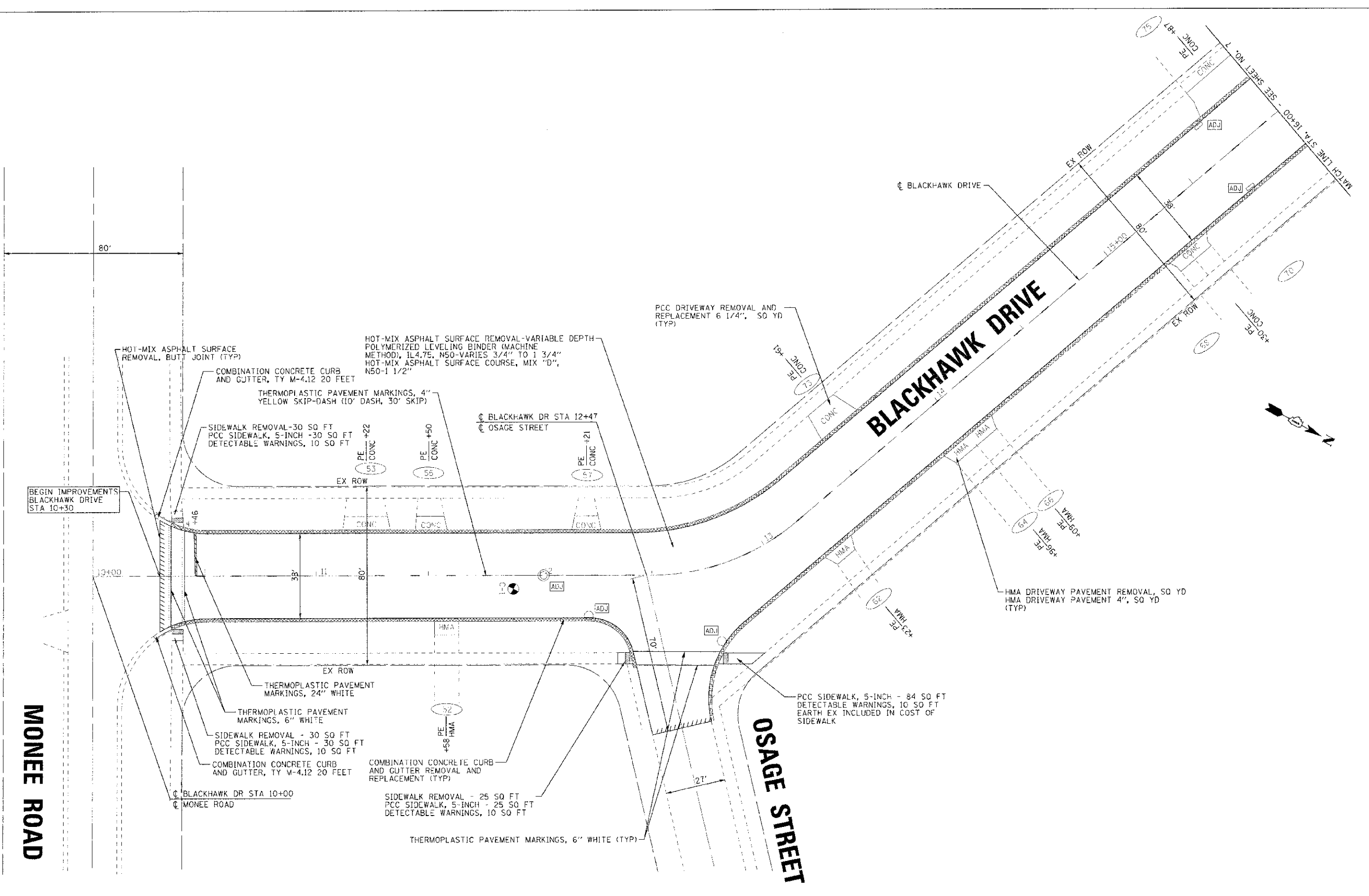
| REMOVING INLETS | | |
|-----------------|--------|------|
| STATION | O/S | EACH |
| 55+16 | RT | 1 |
| 55+16 | 21' RT | 1 |
| 55+16 | LT | 1 |
| 55+16 | 21' LT | 1 |
| TOTAL: 4 | | |

| PARKWAY RESTORATION | |
|------------------------------------|--------------|
| TOTAL C&G R/R | 16771.0 FEET |
| 3' BEHIND | 5590.3 SQ YD |
| SIDEWALK REMOVED BUT NOT REPLACED | 245.0 SQ FT |
| | 27.2 SQ YD |
| TOTAL SIDEWALK | 2943.0 SQ FT |
| LENGTH OF SW | 588.6 FEET |
| 3' ON BOTH SIDES | 392.4 SQ YD |
| NUMBER OF DRIVEWAYS | 139.0 |
| 3' ON BOTH SIDES, 6" NOMINAL DEPTH | 556.0 SQ YD |
| 24+90-WORK IN PARKWAY | 6.7 SQ YD |
| TOTAL: 6016.7 SQ YD | |

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 514 E. WASHINGTON ST. SUITE 200, CHICAGO, IL 60601
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 WWW.BAXTERWOODMAN.COM



| | |
|-----------------|-----------|
| DESIGNED - BNF | REVISED - |
| DRAWN - DCC | REVISED - |
| CHECKED - TAO | REVISED - |
| DATE - 08/21/12 | FILE - 1 |



MONNEE ROAD

BLACKHAWK DRIVE

OSAGE STREET



| | | | |
|------------|----------|-----------|-------------------|
| DESIGNED - | BNP | REVISED - | |
| DRAWN - | SCD | REVISED - | |
| CHECKED - | TAO | REVISED - | |
| DATE - | 08/17/12 | FILE - | 120499-Plan01.sht |

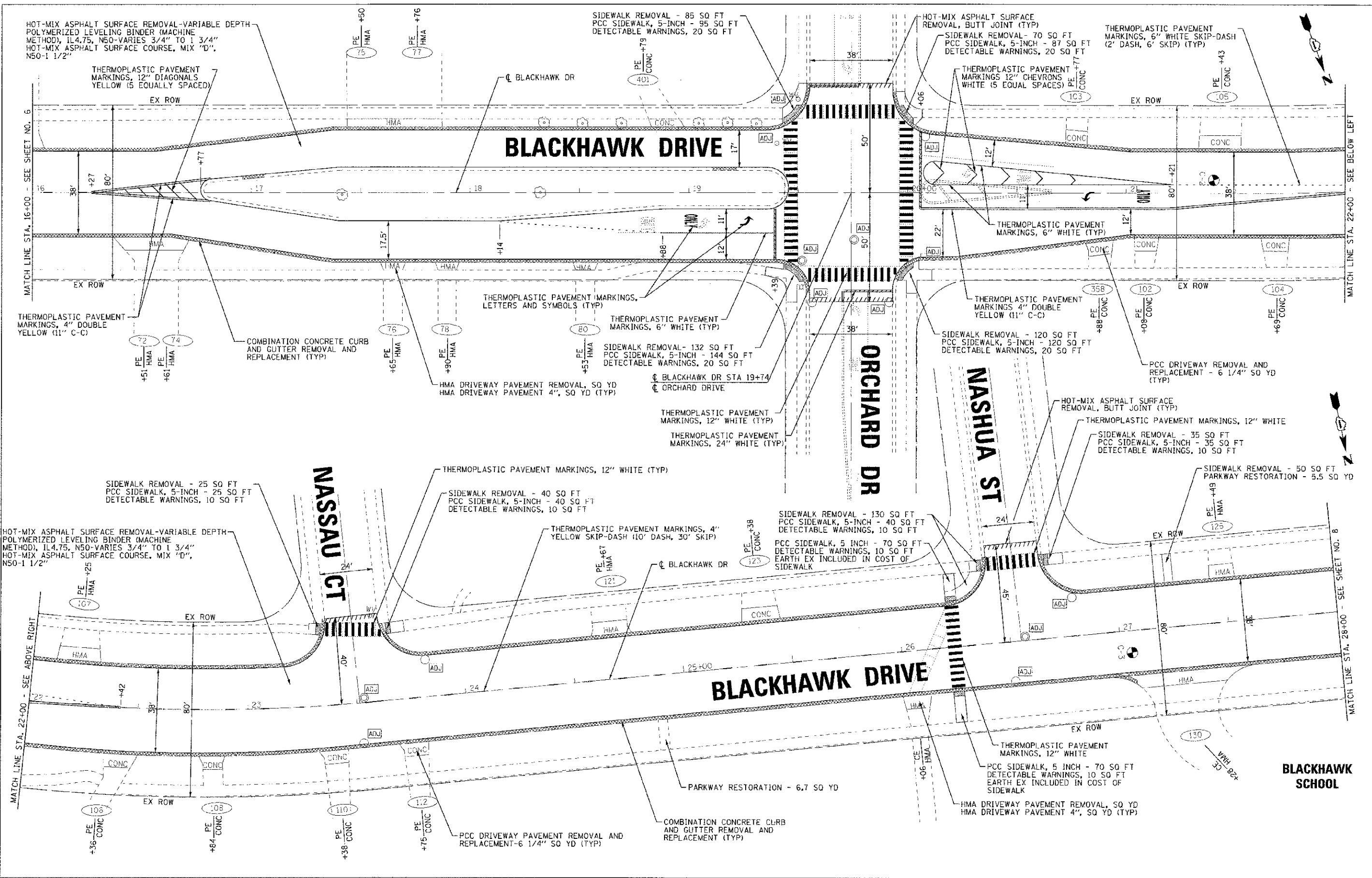
**VILLAGE OF PARK FOREST, ILLINOIS
BLACKHAWK DRIVE
RESURFACING**

BLACKHAWK DRIVE PLAN

SCALE: 1"=20'
STA. 10+00 TO STA. 16+00

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|-----------|--------------------|-----------|
| 1057 | 12-00097-00-RS | COOK-WILL | 24 | 6 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 63740 | |
| | | | M-4003(77) | |

CONTRACT NO. 63740, DRAWING NO. 12-00097-00-RS, SHEET NO. 6, DATE: 08/17/12, PROJECT: BLACKHAWK DRIVE RESURFACING, VILLAGE OF PARK FOREST, ILLINOIS.



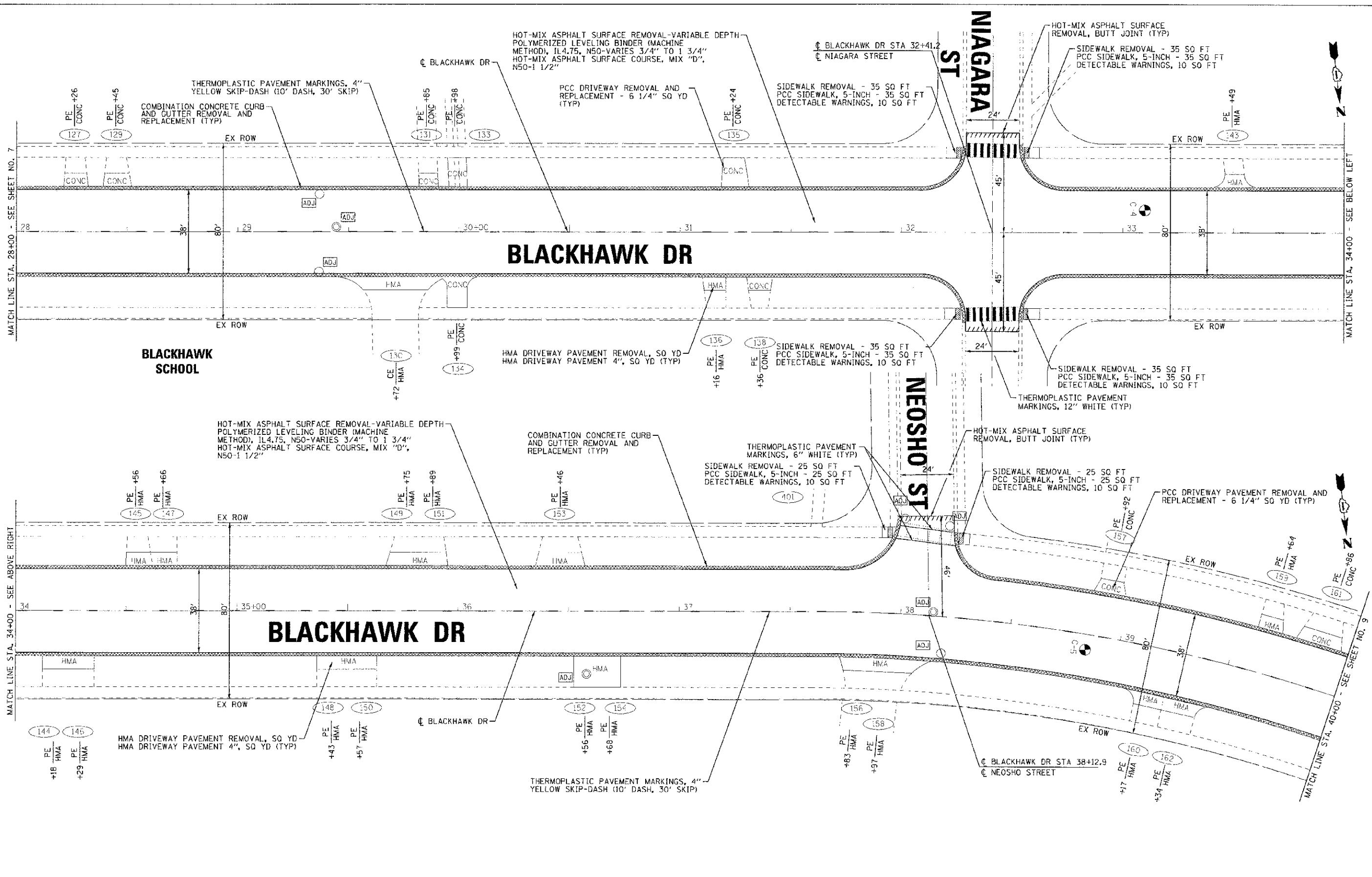
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 STATE ST. CLINGS - PROFESSIONAL DESIGN FIRM
 5150 N. WILSON AVENUE, SUITE 100, CHICAGO, IL 60630
 TEL: (773) 442-1100 FAX: (773) 442-1101
 WWW.BAXTERANDWOODMAN.COM

| | |
|-----------------|--------------------------|
| DESIGNED - BAP | REVISED - |
| UNAWN - BCU | REVISED - |
| CHECKED - AO | REVISED - |
| DATE - 08/21/12 | FILE - 120499-Plan02.sh1 |

VILLAGE OF PARK FOREST, ILLINOIS
BLACKHAWK DRIVE
RESURFACING

| | |
|-----------------------------|--------------------------|
| BLACKHAWK DRIVE PLAN | |
| SCALE: 1"=20' | STA. 16+00 TO STA. 28+00 |

| | | | | |
|--|------------------------|------------------|--------------------|-------------|
| F.A.J. R.T.E. 1057 | SECTION 12-00097-00-RS | COUNTY COOK WILL | TOTAL SHEETS 24 | SHEET NO. 7 |
| FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT M-40031077 | | | CONTRACT NO. 63740 | |



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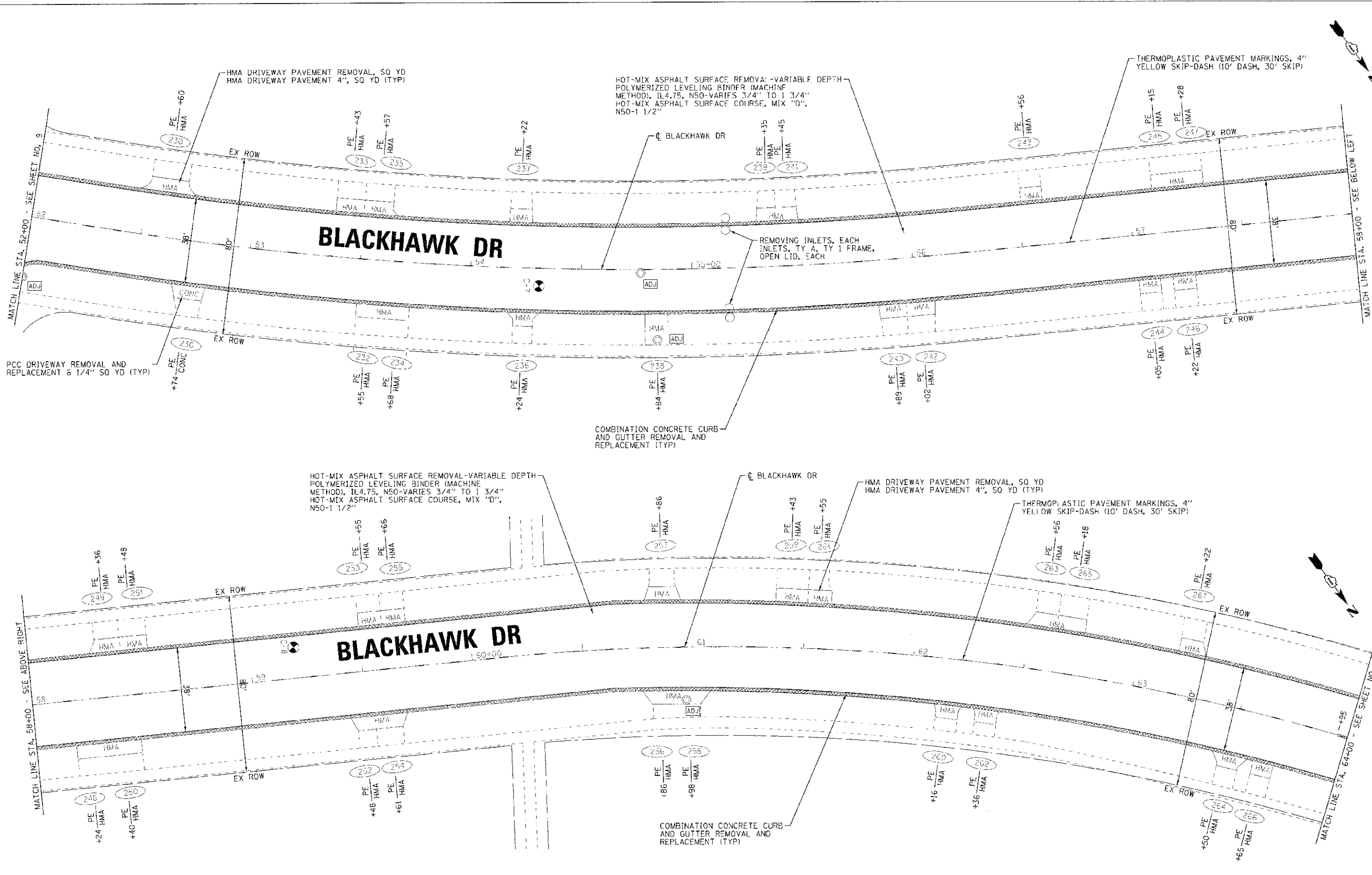
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|-----------------|--------------------------|
| DESIGNED - BNP | REVISED - |
| DRAWN - BCD | REVISED - |
| CHECKED - TAO | REVISED - |
| DATE - 06/21/12 | FILE - 120499-Plan03.sht |

VILLAGE OF PARK FOREST, ILLINOIS
BLACKHAWK DRIVE
RESURFACING

BLACKHAWK DRIVE PLAN

SCALE: 1"=20' STA. 28+00 TO STA. 40+00

| | | | | |
|--------------------------------|----------------|-----------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1057 | 12-00097-00-RS | COOK-WILL | 24 | 8 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63740 | |
| FED. AID PROJECT | | | N-403077 | |



HOT-MIX ASPHALT SURFACE REMOVAL-VARIABLE DEPTH
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL4.75, N50-VARIES 3/4" TO 1 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50-1 1/2"

THERMOPLASTIC PAVEMENT MARKINGS, 4" YELLOW SKIP-DASH (10' DASH, 30' SKIP)

BLACKHAWK DR

BLACKHAWK DR

HOT-MIX ASPHALT SURFACE REMOVAL-VARIABLE DEPTH
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL4.75, N50-VARIES 3/4" TO 1 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50-1 1/2"

THERMOPLASTIC PAVEMENT MARKINGS, 4" YELLOW SKIP-DASH (10' DASH, 30' SKIP)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (TYP)

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 S.A. # 21 033, #055550004, 03/01/05
 PROJECT NO. - 12-00097-00-RS
 DATE 08/17/12



| | | | |
|----------|----------|---------|-------------------|
| DESIGNED | BNP | REVISED | - |
| DRAWN | BCD | REVISED | - |
| CHECKED | TAO | REVISED | - |
| DATE | 08/17/12 | FILE | 120499-Plan05.snt |

VILLAGE OF PARK FOREST, ILLINOIS
BLACKHAWK DRIVE
RESURFACING

BLACKHAWK DRIVE PLAN

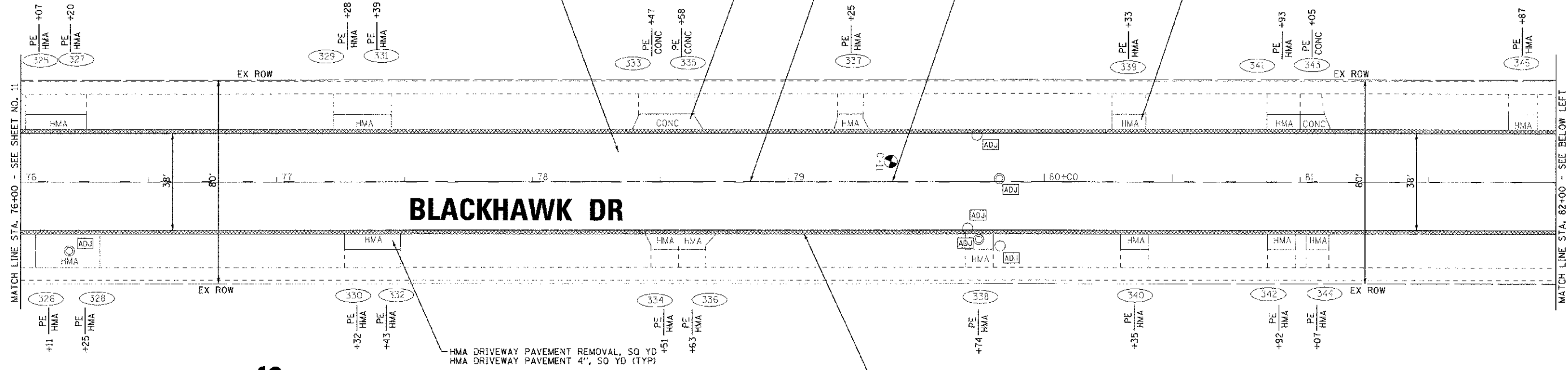
SCALE: 1"=20'
 STA. 52+00 TO STA. 64+00

| | | | | |
|--------------------------------|------------------------|------------------|--------------------|--------------|
| F.A.D. RILEY | SECTION 12-00097-00-RS | COUNTY COOK-WILL | TOTAL SHEETS 24 | SHEET NO. 10 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63740 | |
| M-4303077 | | | | |

HOT-MIX ASPHALT SURFACE REMOVAL-VARIABLE DEPTH
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL4.75, N50-VARIES 3/4" TO 1 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50-1 1/2"

PCC DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT-6 1/4", SQ YD (TYP)

THERMOPLASTIC PAVEMENT MARKINGS, 4" YELLOW SKIP-DASH (10' DASH, 30' SKIP)



SUWANEE STREET

HOT-MIX ASPHALT SURFACE REMOVAL-VARIABLE DEPTH
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL4.75, N50-VARIES 3/4" TO 1 3/4"
 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50-1 1/2"

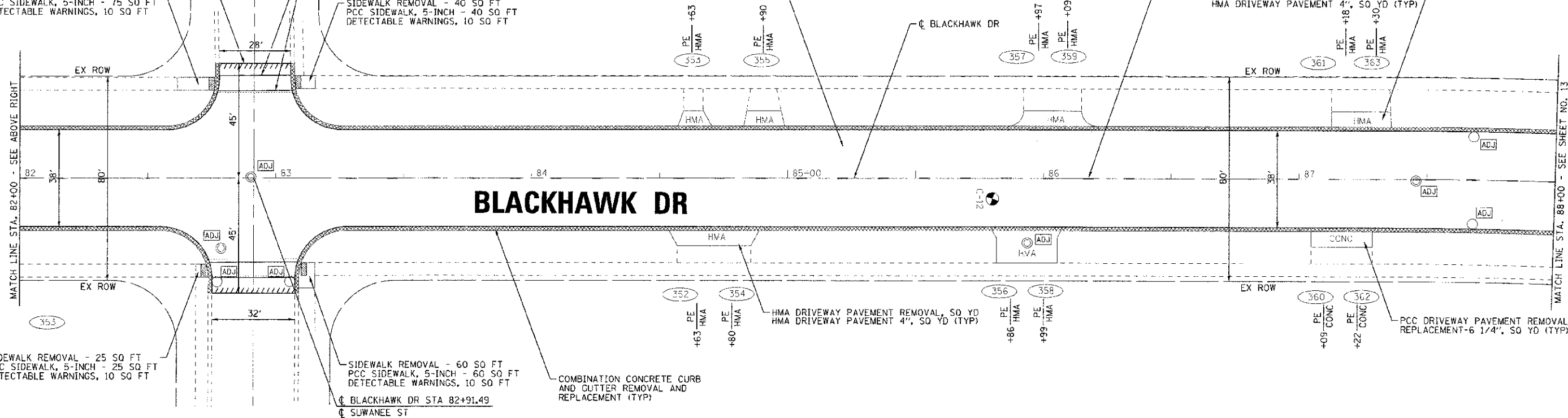
THERMOPLASTIC PAVEMENT MARKINGS, 6" WHITE (TYP)

SIDEWALK REMOVAL - 75 SQ FT
 PCC SIDEWALK, 5-INCH - 75 SQ FT
 DETECTABLE WARNINGS, 10 SQ FT

SIDEWALK REMOVAL - 40 SQ FT
 PCC SIDEWALK, 5-INCH - 40 SQ FT
 DETECTABLE WARNINGS, 10 SQ FT

THERMOPLASTIC PAVEMENT MARKINGS, 4" YELLOW SKIP-DASH (10' DASH, 30' SKIP)

HMA DRIVEWAY PAVEMENT REMOVAL, SQ YD
 HMA DRIVEWAY PAVEMENT 4", SQ YD (TYP)



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 120499-Plan07.sht

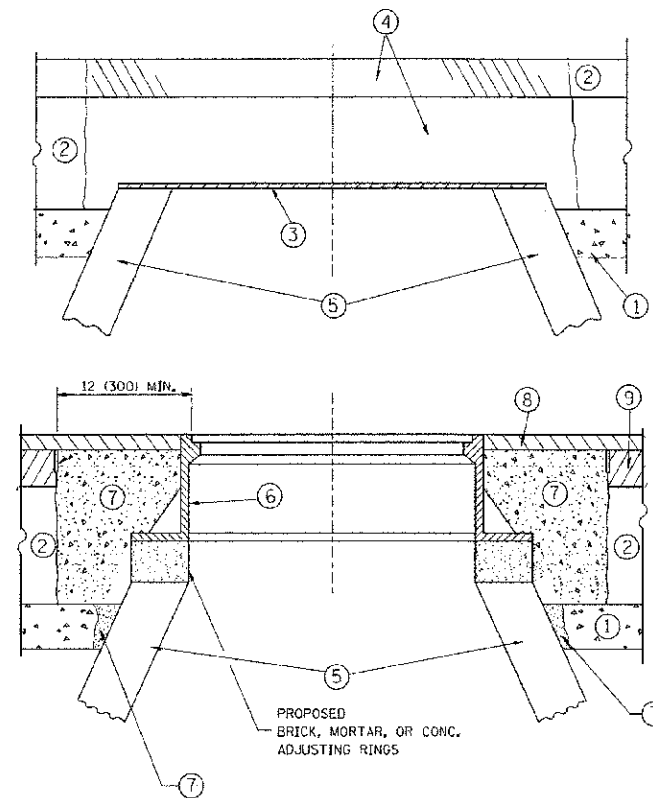


| | | | |
|----------|----------|---------|-------------------|
| DESIGNED | BNP | REVISED | - |
| DRAWN | BED | REVISED | - |
| CHECKED | TAG | REVISED | - |
| DATE | 08/21/12 | FILE | 120499-Plan07.sht |

**VILLAGE OF PARK FOREST, ILLINOIS
 BLACKHAWK DRIVE
 RESURFACING**

| | |
|-----------------------------|--------------------------|
| BLACKHAWK DRIVE PLAN | |
| SCALE: 1"=20' | STA. 76+00 TO STA. 88+00 |

| | | | | |
|--------------------------------|------------------------|------------------|--------------------|--------------|
| F.A.J. RTE. 1057 | SECTION 12-00097-00-RS | COUNTY COOK-WILL | TOTAL SHEETS 24 | SHEET NO. 12 |
| FED. ROAD DIST. NO. 1 ILLINOIS | | | CONTRACT NO. 63740 | |
| FED. AID PROJECT | | | M-4033077 | |



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 355, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

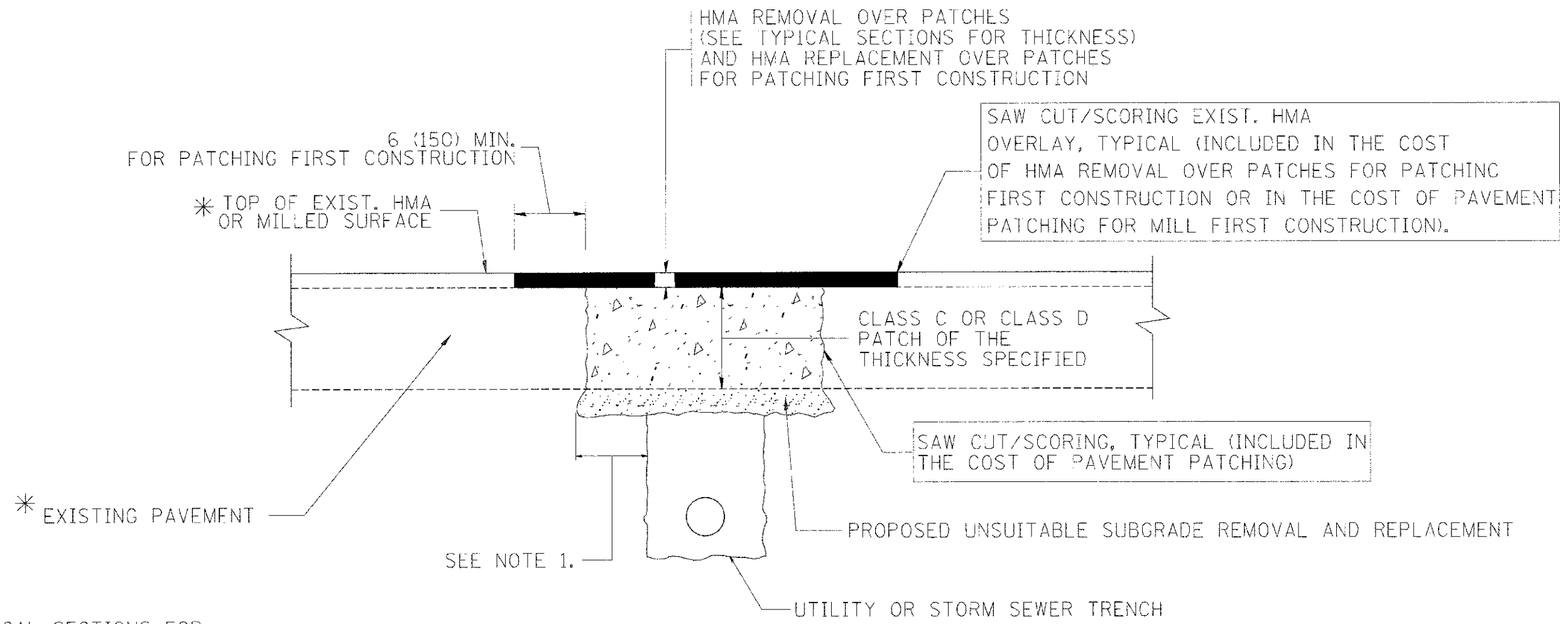
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | | | | | | | | | | |
|---|---------------------|----------------------------|--------------------------------|---|--|-------------------------|------|------------------------|---------------------------|---------------------|-----------------------|--------------------|
| FILE NAME : | USER NAME : bauerds | DESIGNED - R. SHAH | REVISED - R. WIEDEMAN 05-14-04 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING | | | F.A.S. RTE. 1057 | SECTION 12-00097-00-RS | COUNTY COOK-WILL | TOTAL SHEETS 24 | SHEET NO. 14 |
| ca:\pwwork\pindar\bauerds\106185313\106185313.dwg | DRAWN - | REVISED - R. BORO 01-01-07 | REVISED - R. BORO 03-09-11 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. | BD800-03 (BD-8) | | CONTRACT NO. 63740 | |
| PLOT SCALE - 1/8"=1'-0" | CHECKED - | REVISED - R. BORO 12-06-11 | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(077) | | | | | | | |
| PLOT DATE - 12/6/2011 | DATE - 10-25-94 | | | | | | | | | | | |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

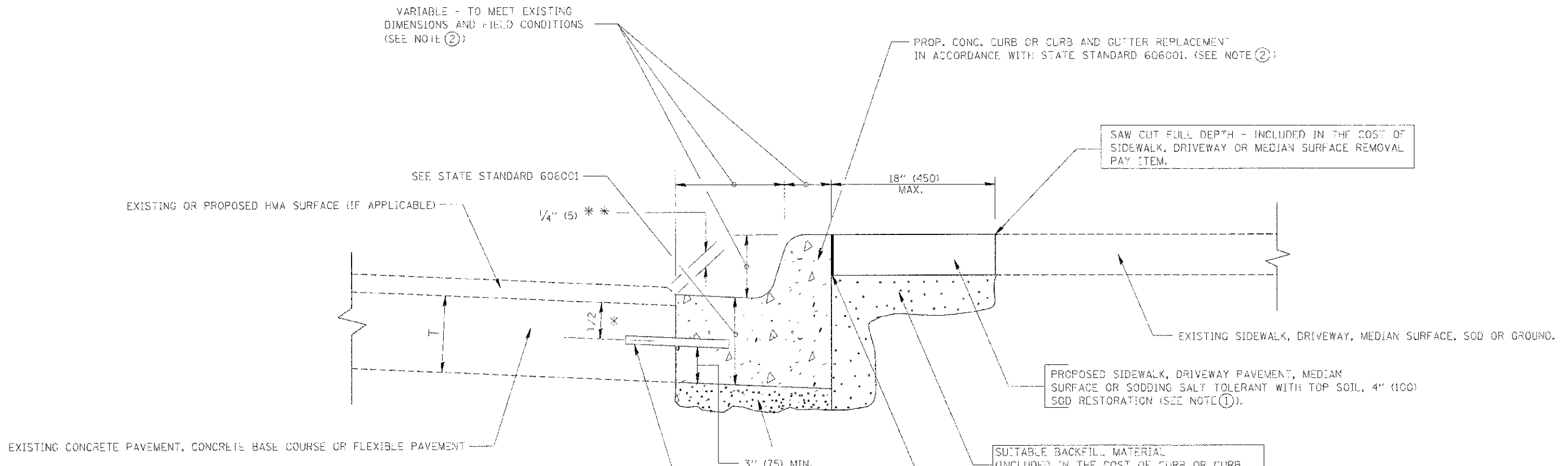
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|----------------------------|--------------------|-----------------------------|---|--|----------------|-----------|--------------------|---------|---------|---|--------------|
| FILE NAME = c:\proje\sta\distsd22x34\bd22.dgn | USER NAME = baseral | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT | | | F.A.D. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - R. BORO 01-01-07 | | 1057 | 12-00097-00-RS | COOK-WILL | 24 | 15 | | | |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - R. BORO 09-04-07 | | BD400-04 (BD-22) | | | CONTRACT NO. 63740 | | | | |
| | PLOT DATE = 10/27/2008 | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 | | SCALE: NONE | SHEET NO. 1 | OF 1 | SHEETS | STA. | TO STA. | FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT | M-4003(077) |



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- * * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED.
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

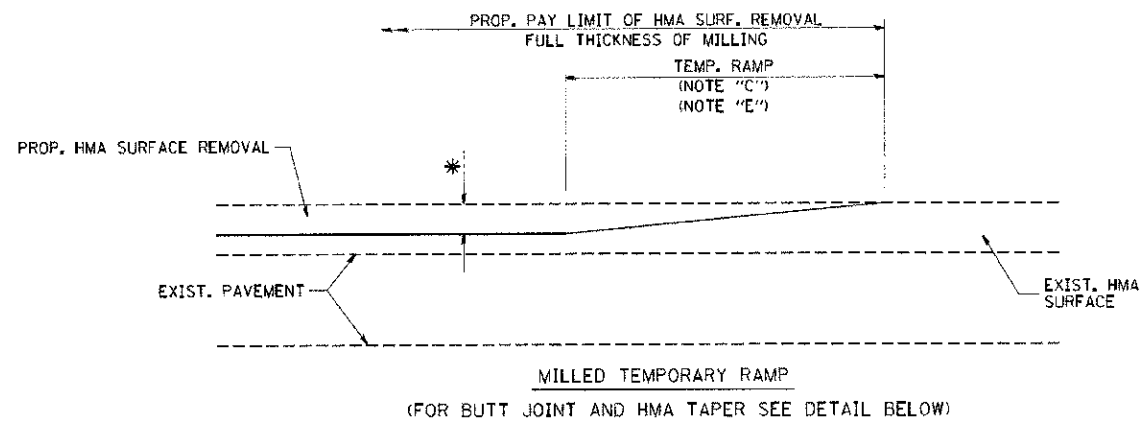
- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

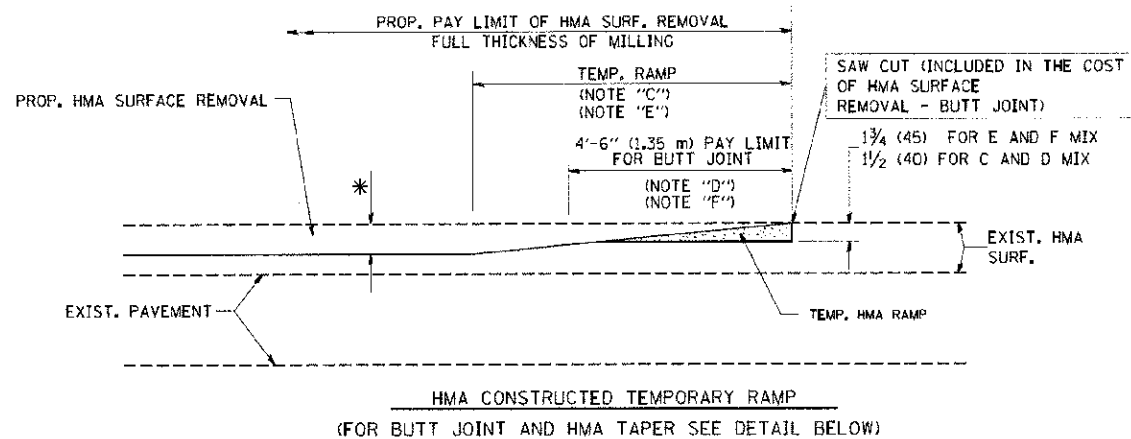
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

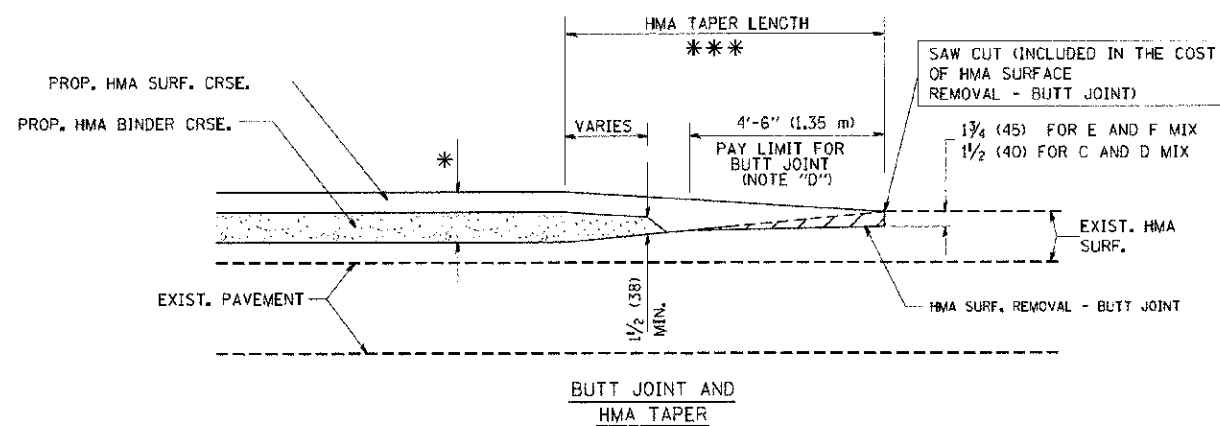
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|-----------------------------|------------------------|----------------------|-----------------------------|---|--|-------------------------|------|---------|--------|-------------------------|---------------------------|-------------|----|
| FILE NAME = | USER NAME = drvalkoagn | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | | | SECTION | COUNTY | TOTAL SHEET NO. | | | |
| PLT SCALE = 50:1 (1" = 50') | DATE = 03-11-94 | CHECKED - | REVISED - M. GCMEZ 01-22-01 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TC STA. | 1057 | 12-00097-00 RS | COCK-WILL | 24 | 16 |
| PLT DATE = 12/16/2009 | DATE = 03-11-94 | REVISOR - | REVISED - R. BORC 12-15-09 | | | | | | | BD600-06 (BD-24) | CONTRACT NO. 63740 | | |
| | | | | | | | | | | FED. ROAD DIST. NO. 1 | ILLINOIS FED. AID PROJECT | M-4003(677) | |



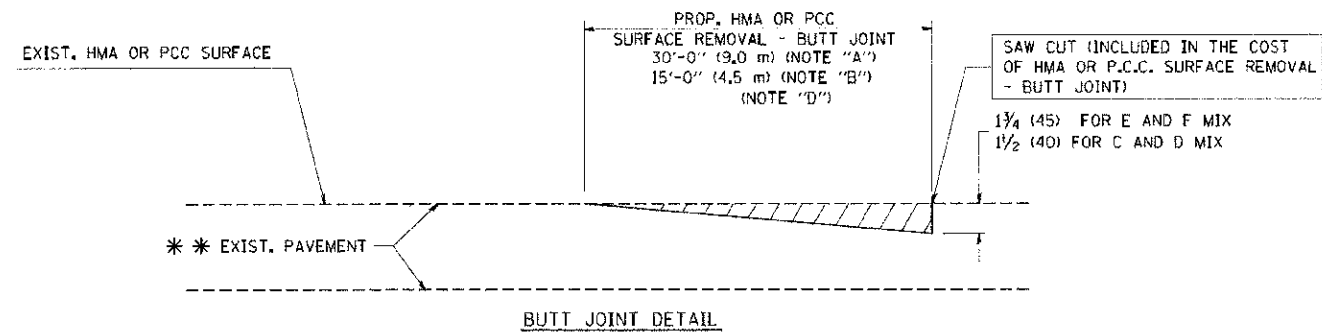
OPTION 1



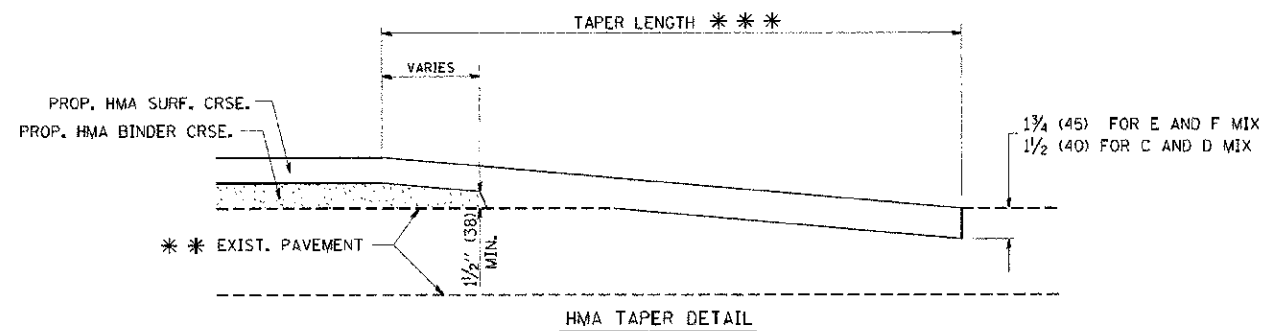
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

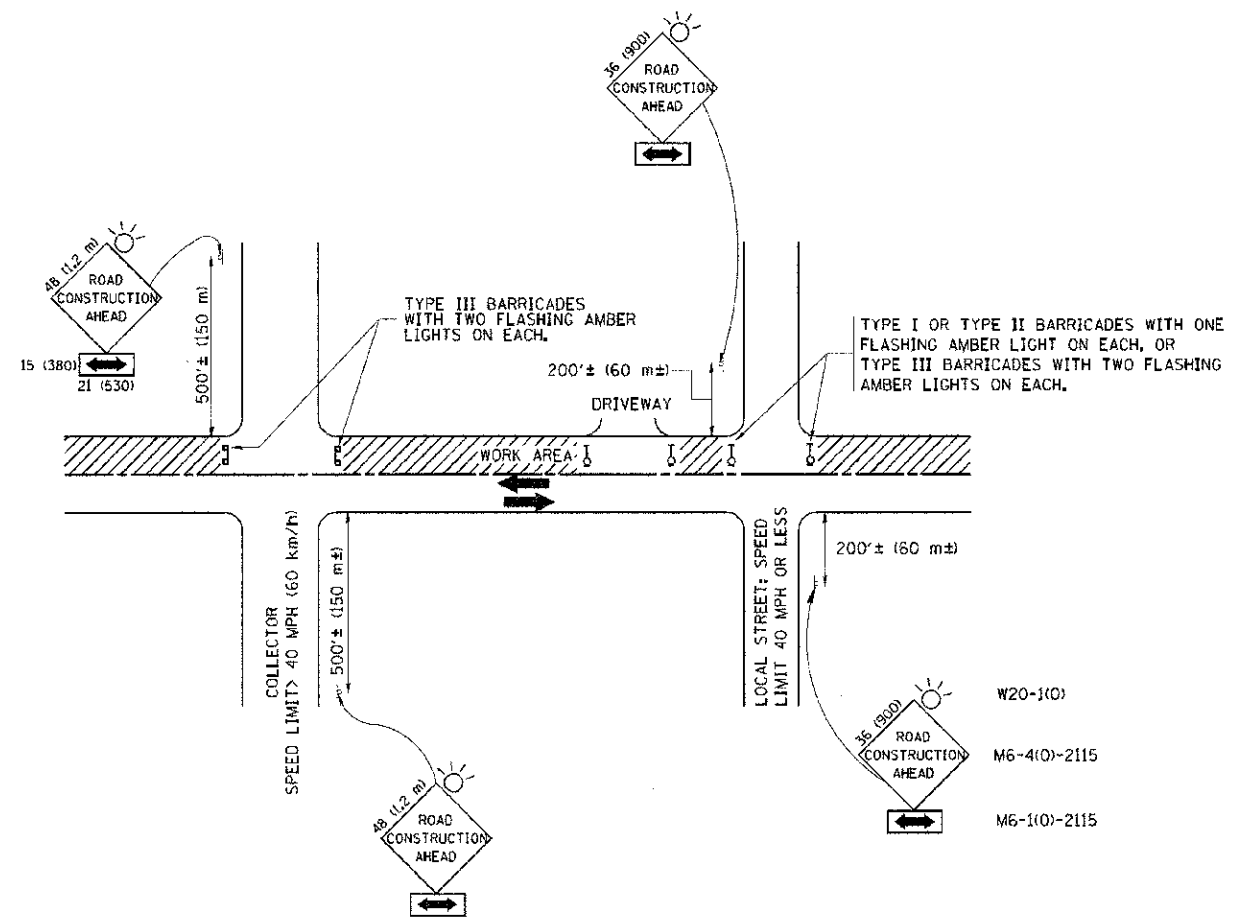
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
|---|-----------------------------|--------------------------|--------------------------------|
| FILE NAME = W:\dscstd\22x34\ba32.dgn | USER NAME = gegianeob | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | PLDT SCALE = 50,0000 "/ IN. | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | PLDT DATE = 1/4/2008 | DATE - 06-13-90 | REVISED - R. BORD 01-01-07 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | |
|-------------------------------------|--|-------------------------|--|------|---------|
| SCALE: NONE | | SHEET NO. 1 OF 1 SHEETS | | STA. | TO STA. |
| BUTT JOINT AND HMA TAPER DETAILS | | | | | |

| | | | | |
|---|----------------|--------------------|--------------|-----------|
| F.A.D. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1057 | 12-00097-00-RS | COOK-WILL | 24 | 17 |
| BD400-05 BD32 | | CONTRACT NO. 63740 | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-40C3(077) | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

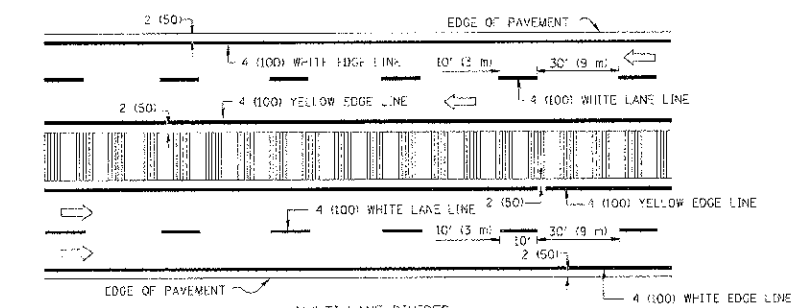
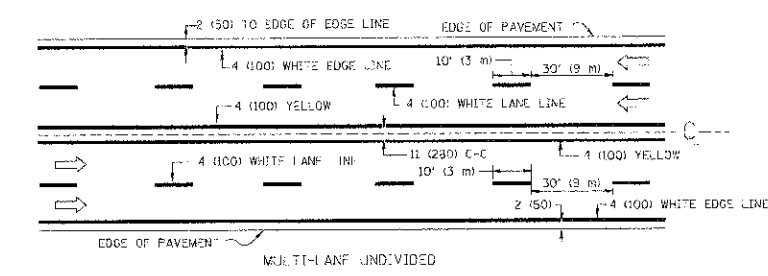
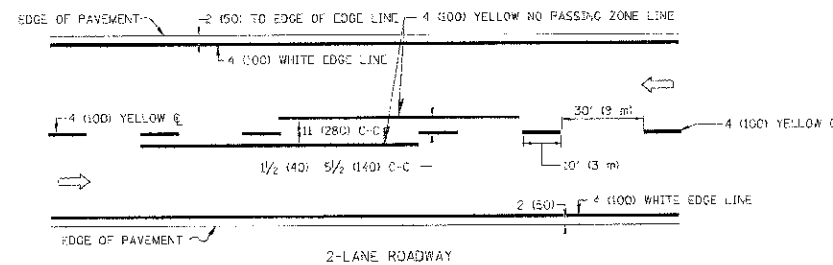
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|---|------------------------------|-------------------|------------------------------------|
| FILE NAME - W:\d:\ata\22x34\ta10.dgn | USER NAME - ggg10nbt | DESIGNED - LHA | REVISOR - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISOR - A. HOUSEH 03-06-96 |
| | PLOT SCALE - 50.000 / IN. | CHECKED - | REVISOR - A. HOUSEH 10-15-96 |
| | PLOT DATE - 1/4/2000 | DATE - 06-89 | REVISOR - T. RAMMACHER 01-06-00 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

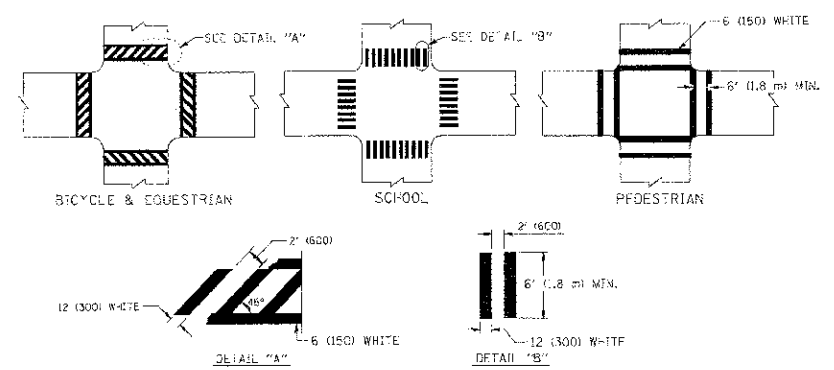
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|--|----------------|-----------|--------------------|--------------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1057 | 12-00097-00-RS | COOK-WILL | 24 | 18 |
| TC-10 | | | CONTRACT NO. 63740 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-403(077) | | | | |

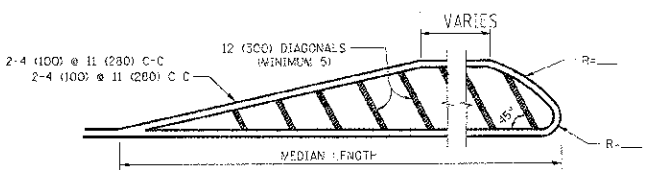
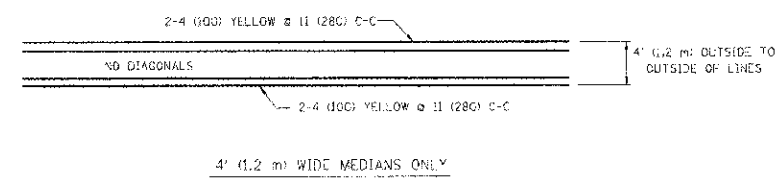


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

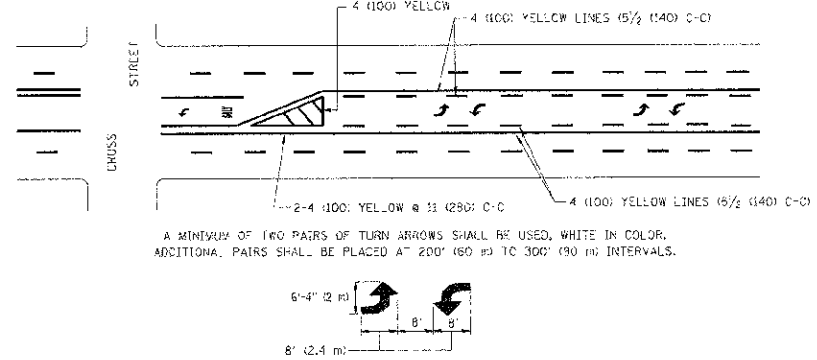


TYPICAL CROSSWALK MARKING

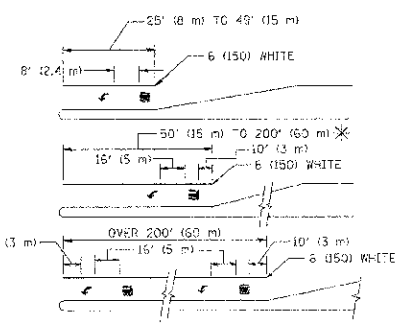


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C LESS THAN 30MPH (50 km/h)
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



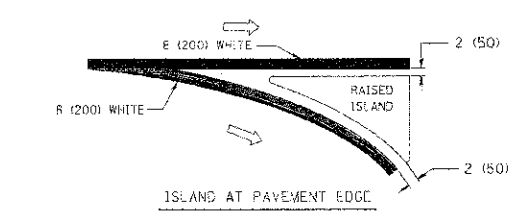
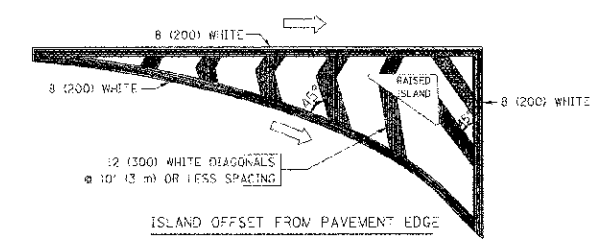
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8" (21.4 cm) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

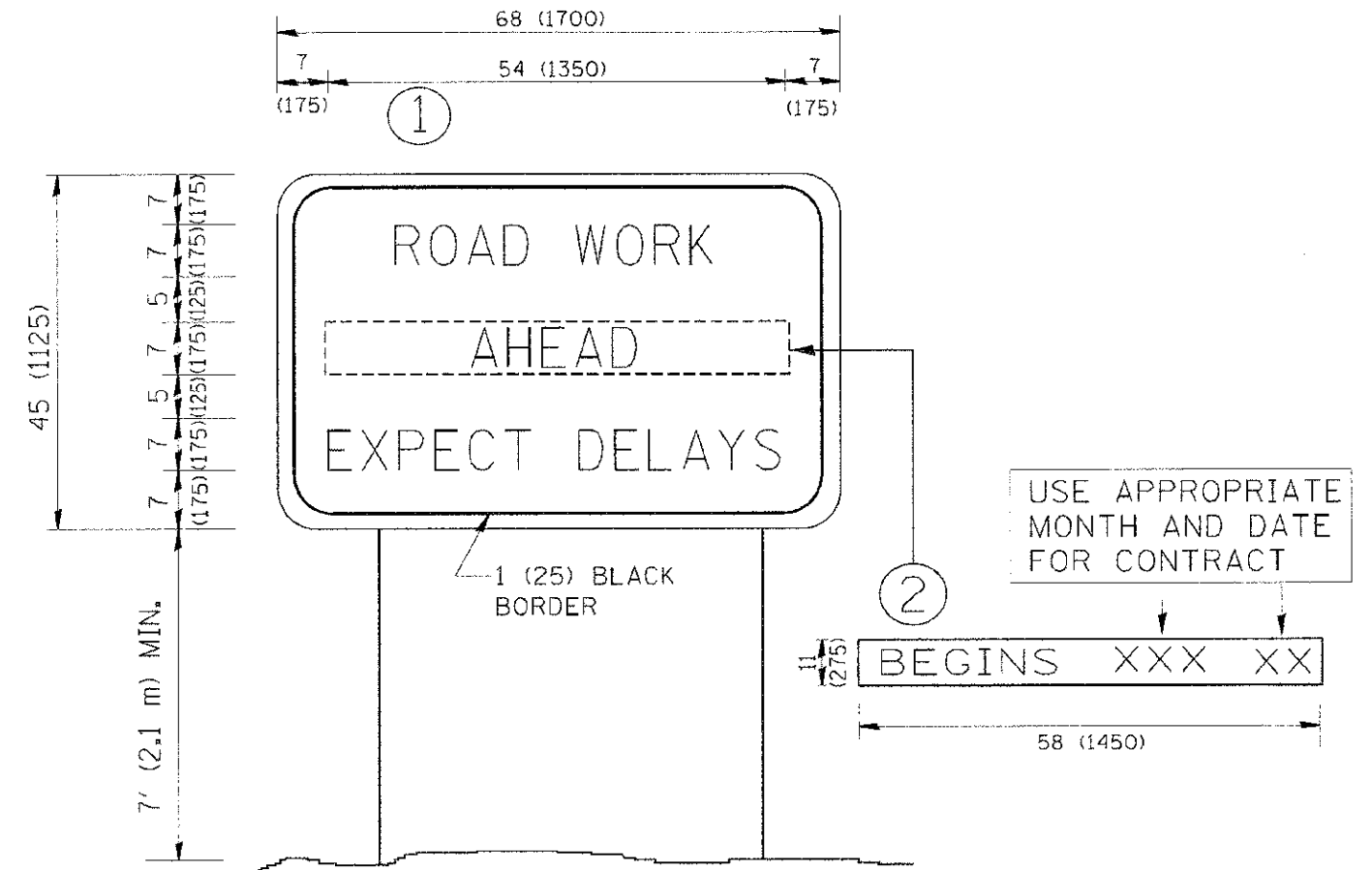


TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|--------------------------------|---|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE FULL SIZE LETTERS & SYMBOLS (5' (12.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SC-100) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF POSSIBLE; OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PADDED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW WHITE WHITE | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING |
| GOVE MARKING AND CHANNELIZING LINES | 6 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C LESS THAN 30MPH (50 km/h) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C OVER 45MPH (70 km/h) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R" - 3.6 SQ. FT. (0.33 m ²) EACH "X" - 54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 60' (18 m) C-C LESS THAN 30MPH (50 km/h) 15' (4.5 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C OVER 45MPH (70 km/h) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001

All dimensions are in inches (millimeters) unless otherwise shown.



NOTES:

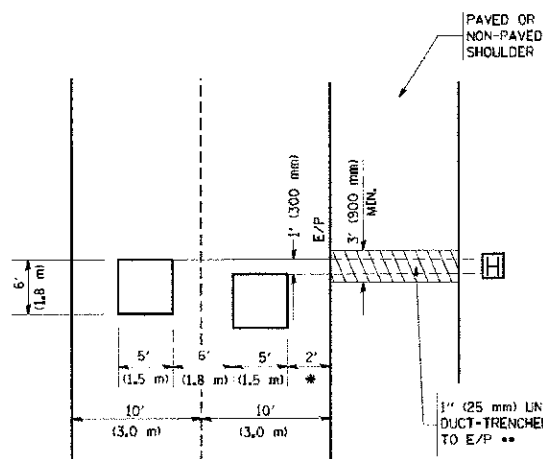
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|---------------------------|------------|---------------------------------|---|---|-------------|------|--------------------------|---------------|------------------|---|--------------|
| FILE NAME = M:\distate\22x34\to22.dgn | USER NAME = goglienob | DESIGNED - | REVISED - R. MIRS 09-15-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | ARTERIAL ROAD INFORMATION SIGN | | | F.A. RTE. 12-00297-00-RS | SECTION TC-22 | COUNTY COCK-WILL | TOTAL SHEETS 24 | SHEET NO. 20 |
| | PLOT SCALE = 50.000 / IN. | DRAWN - | REVISED - R. MIRS 12-11-97 | | SCALE: NONE | SHEET NO. 1 | OF 1 | SHEETS | STA. | TG STA. | CONTRACT NO. 63740 | |
| | PLT DATE = 1/4/2009 | CHECKED - | REVISED - T. RAMMACHER 02-02-99 | | | | | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | |
| | | DATE - | REVISED - C. JUCIUS 01-31-07 | | | | | | | | | |

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

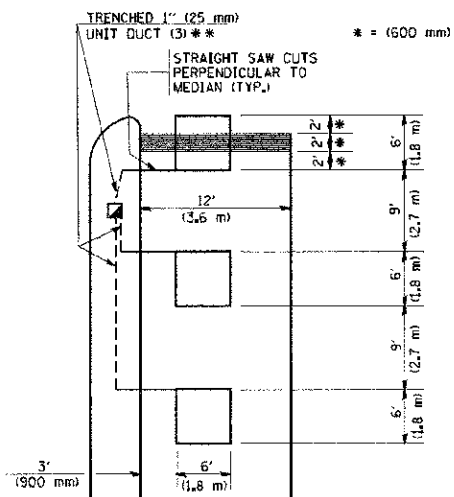


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

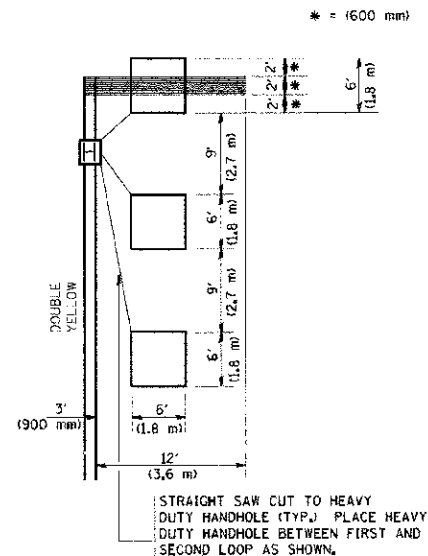
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

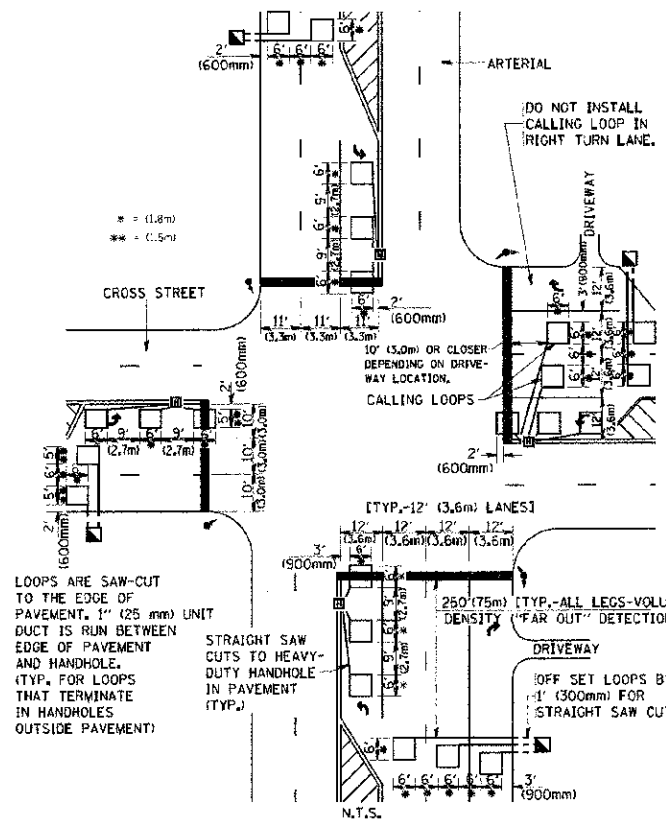
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



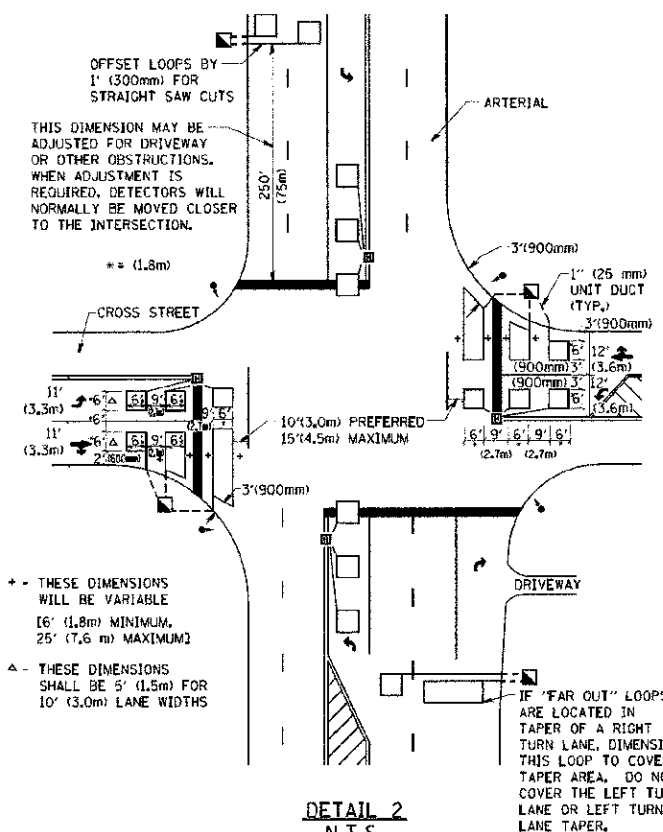
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.O.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

| | |
|--------------------------------------|-----------------------|
| FILE NAME = | USER NAME = goglianob |
| W:\dvs\td\22x34\ts87.dgn | |
| PLOT SCALE = 50.0000 "/td> <td></td> | |
| PLOT DATE = 1/4/2008 | |

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| DESIGNED | REVISD |
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| DRAWN | REVISD |
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

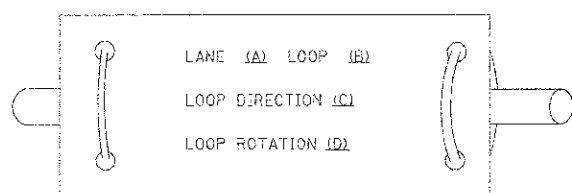
| | | | |
|--|-------------------------|------|---------|
| DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING | | | |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| | | | | |
|---|----------------|-----------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1087 | 12-00097-CO-RS | COOK-WILL | 24 | 21 |
| TS-07 | | | CONTRACT NO. 63740 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT W-4003(077) | | | | |

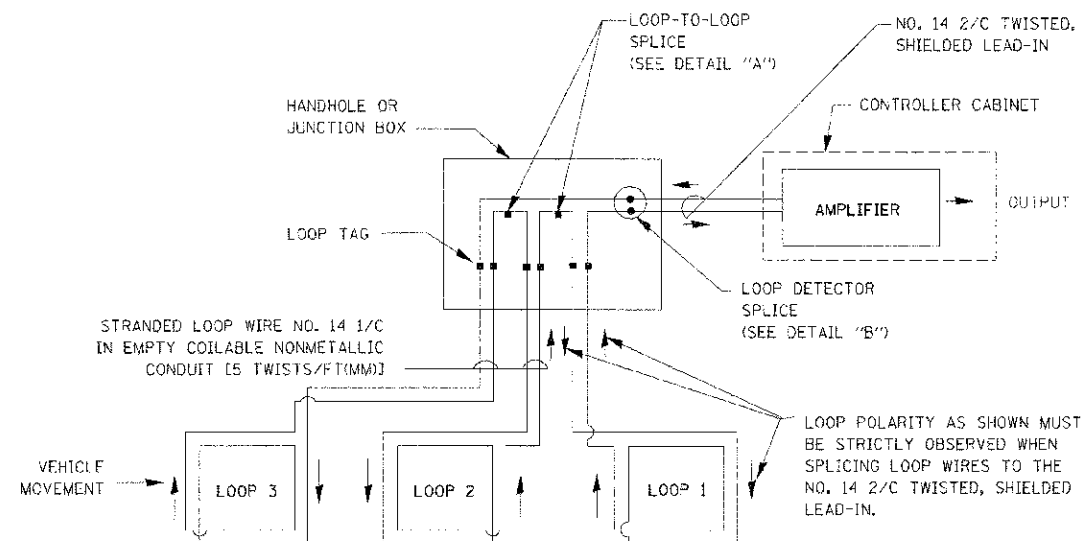
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

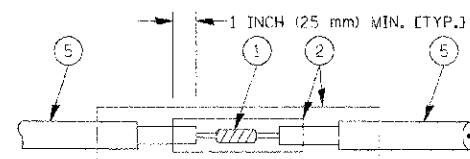


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

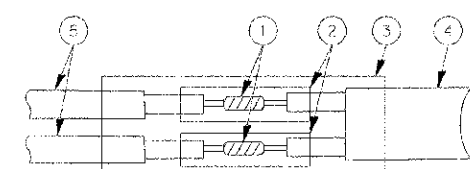


DETECTOR LOOP WIRING SCHEMATIC

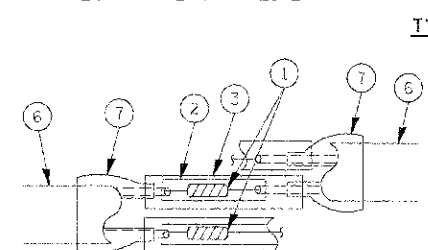
- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



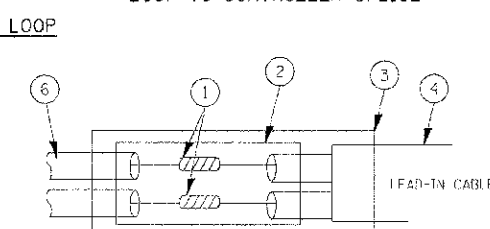
DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 20C/75D HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XI POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCC CBR-2 OR APPROVED EQUAL

| | | | |
|--|---------------------|-----------------|-----------|
| FILE NAME = | USER NAME = bauer.d | DESIGNED - DAD | REVISED - |
| C:\pwwork\p1007\BAUER.D\0100315\1036.dwg | | DRAWN - BCK | REVISED - |
| PLOT SCALE = 50.0000 1/16" | | CHECKED - DAD | REVISED - |
| PLOT DATE = 11/4/2009 | | DATE - 10-28-09 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|--|-------------------------|------|---------|
| DISTRICT ONE | | | |
| STANDARD TRAFFIC SIGNAL DESIGN DETAILS | | | |
| SCALE: NONE | SHEET NO. 1 OF 6 SHEETS | STA. | TO STA. |

| | | | | |
|---|------------------------|------------------|--------------------|--------------|
| F.A.D. NO. 1057 | SECTION 12-00097-00-R5 | COUNTY COOK-WILL | TOTAL SHEETS 24 | SHEET NO. 22 |
| TS-05 | | | CONTRACT NO. 63740 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(077) | | | | |

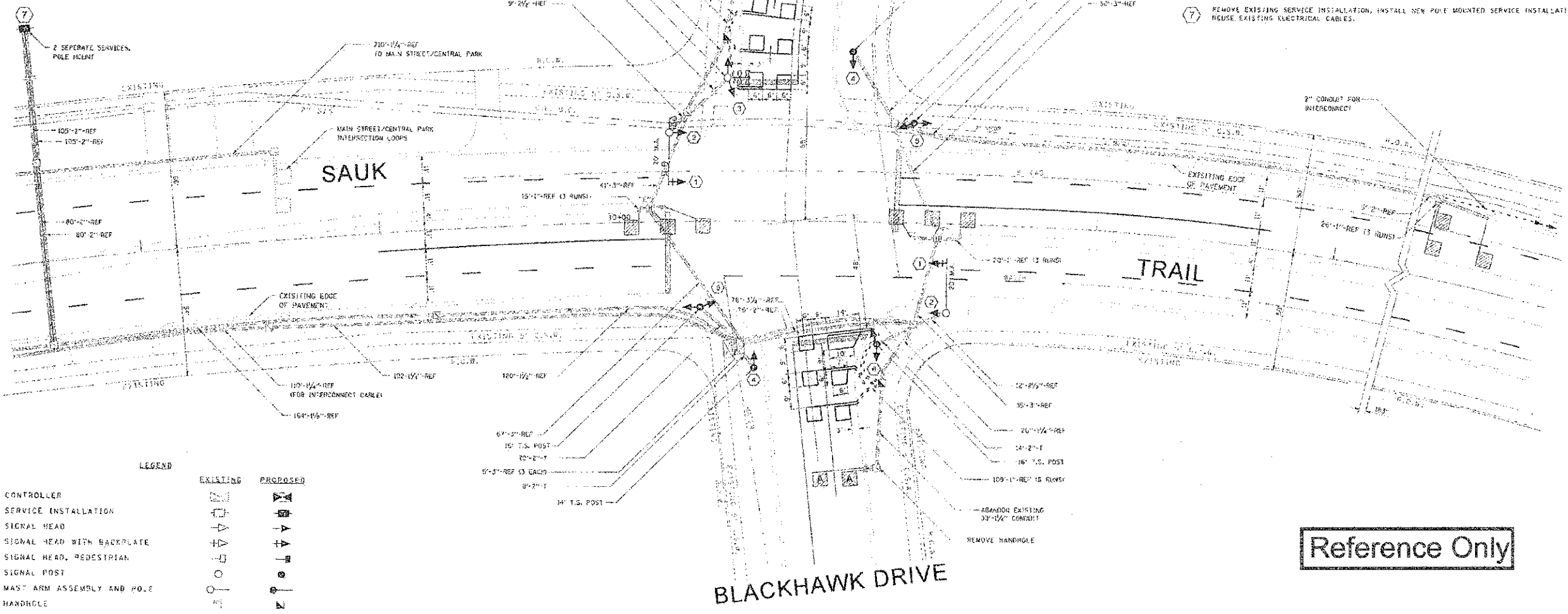
| COUNTY HIGHWAY | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|----------------|-------------|-----------|--------------|
| CH | | | |

SECTION: 12-00097-00-RS

- NOTES:**
- THE EXACT LOCATIONS OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE THE INSTALLATION OF ANY COMPONENTS OF THE TRAFFIC SIGNAL SYSTEM. FOR EXACT LOCATIONS OF THE UTILITIES, CALL 800-331-1234 TOLL FREE AT (800) 331-1234.
 - VEHICLE LOOP DETECTOR SHALL BE COMPATIBLE WITH THE CONTROL EQUIPMENT.
 - ALL ELECTRICAL CABLE SHALL HAVE POLYVINYL JACKET.
 - THE CONTRACTOR SHALL INFORM THE CDD DESIGN ENGINEER AT (312) 603-1730 PRIOR TO THE START OF ANY WORK ON THE CONTRACT. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
 - THE CONTRACTOR SHALL MARK THE LOCATIONS OF ALL DETECTOR LOOPS AND CONTACT THE CDD DESIGN ENGINEER AT (312) 603-1730 FOR LOCATION APPROVAL PRIOR TO THE CUTTING OF THE LOOPS. A MINIMUM OF FIVE (5) WORKING DAYS ADVANCE NOTICE IS REQUIRED.
 - ALL SIGNAL POSTS SHALL BE SET BACK FOUR (4) FEET MINIMUM AND ALL MAST ARM POLLS SHALL BE SET BACK SIX (6) FEET MINIMUM FROM THEIR CENTERLINE TO THE BACK OF CURB UNLESS OTHERWISE NOTED. IN NON-TURBO AREAS THE MAST ARM POLL AND SIGNAL POST SHALL BE LOCATED A MINIMUM OF TEN (10) FEET BEHIND THE EDGE OF PAVEMENT OR TWO (2) FEET BEHIND THE EDGE OF SHOULDER, WHICHEVER DISTANCE IS GREATER.
 - ALL EXISTING LOOP DETECTORS ARE TO BE ANALYZED AND RE-CUT IF NECESSARY.
 - HANDHOLE UNDER ROADWAYS AND DRIVEWAYS TO BE PLACED IN TRENCH BEFORE PAVEMENT IS PLACED.
 - ALL MAST ARM MOUNTED SIGNAL HEADS ARE TO BE ATTACHED TWO (2) FEET FROM THE END OF THE MAST ARM UNLESS OTHERWISE NOTED.
 - ALL 1" CONDUIT TO BE 1" EMPTY DUCT.
 - CARE IS TO BE TAKEN NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUIT AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED THE CONTRACTOR SHALL REPAIR OR REPLACE THE CONDUIT AND/OR EQUIPMENT AT HIS COST TO THE COUNTY.

- THE CORNERS OF ALL NEW DETECTOR LOOPS SHALL BE CORNER DRILLED INCLUDING PAVEMENT JOINTS AND CRACKS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAKE SURE ALL NEW AND EXISTING TRAFFIC SIGNAL POSTS ON EXISTING OR NEW CONCRETE FOUNDATIONS ARE STRAIGHT UP. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. SHIMS ARE NOT ACCEPTABLE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE THE EXISTING TRAFFIC SIGNAL AND STREET LIGHTING CONDUIT AND CABLE. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

- NOTES:**
- REMOVE EXISTING SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED. INSTALL NEW SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, MAST ARM MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW 14 FOOT SIGNAL POST AND SIGNAL HEAD, L.E.D., 1-FACE, 3-SECTION, BRACKET MOUNTED ON EXISTING TYPE A FOUNDATION. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 2-FACE, 1-3-SECTION, 1-5-SECTION, BRACKET MOUNTED. INSTALL NEW 16 FOOT SIGNAL POST AND SIGNAL HEAD, L.E.D., 2-FACE, 1-3-SECTION, 1-5-SECTION BRACKET MOUNTED ON EXISTING TYPE A FOUNDATION. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 2-FACE, 3-SECTION, BRACKET MOUNTED. INSTALL NEW 14 FOOT POST ON EXISTING TYPE A FOUNDATION. INSTALL NEW SIGNAL HEAD, L.E.D., 2-FACE, 3-SECTION, BRACKET MOUNTED. REUSE EXISTING ELECTRICAL CABLE.
 - REMOVE EXISTING SERVICE INSTALLATION. INSTALL NEW POLE MOUNTED SERVICE INSTALLATION. REUSE EXISTING ELECTRICAL CABLES.



LEGEND

| | EXISTING | PROPOSED |
|-----------------------------------|----------|----------|
| CONTROLLER | [Symbol] | [Symbol] |
| SERVICE INSTALLATION | [Symbol] | [Symbol] |
| SIGNAL HEAD | [Symbol] | [Symbol] |
| SIGNAL HEAD WITH BACKPLATE | [Symbol] | [Symbol] |
| SIGNAL HEAD, PEDESTRIAN | [Symbol] | [Symbol] |
| SIGNAL POST | [Symbol] | [Symbol] |
| MAST ARM ASSEMBLY AND POLE | [Symbol] | [Symbol] |
| HANDHOLE | [Symbol] | [Symbol] |
| HEAVY DUTY HANDHOLE | [Symbol] | [Symbol] |
| DOUBLE HANDHOLE | [Symbol] | [Symbol] |
| GALVANIZED STEEL CONDUIT | [Symbol] | [Symbol] |
| EMERGENCY VEHICLE SYSTEM DETECTOR | [Symbol] | [Symbol] |
| CONFIRMATION BEACON | [Symbol] | [Symbol] |
| PEDESTRIAN PUSHBUTTON | [Symbol] | [Symbol] |
| DETECTOR LOOP | [Symbol] | [Symbol] |

Reference Only

SCALE 1"=20'

| | |
|--------------------------------|---------------------|
| COUNTY OF COOK | |
| DEPARTMENT OF HIGHWAYS | |
| TRAFFIC SIGNAL INSTALLATION | |
| SAUK TRAIL AT | |
| LAKEWOOD BLVD. - BLACKHAWK DR. | |
| DESIGNED BY: B.N.P. | DATE: June 17, 2003 |
| DRAWN BY: B.A.C. | |
| CHECKED BY: P.L.E. | |

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 081-001833-0000-0000-0000-0000
 PROJECT NO. 120499-SIGNAL PLANS

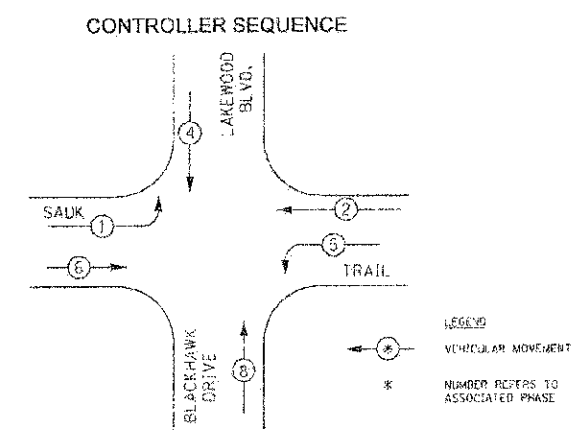


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| DESIGNED - BNP | REVISED - |
| DRAWN - BCO | REVISED - |
| CHECKED - IAO | REVISED - |
| DATE - 08/17/12 | FILE - 120499-SIGNAL PLANS.sht |

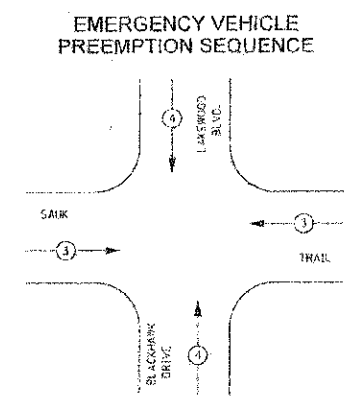
**COOK COUNTY HIGHWAY DEPARTMENT
SIGNAL PLANS - REFERENCE ONLY**

| | | | | |
|------------------|------------------------|--------------------|-----------------|--|
| F.A.U. RTE. 1057 | SECTION 12-00097-00-RS | COUNTY COOK-WILL | TOTAL SHEETS 24 | SHEET NO. 23 |
| NONE | | CONTRACT NO. 63740 | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-40030771 |

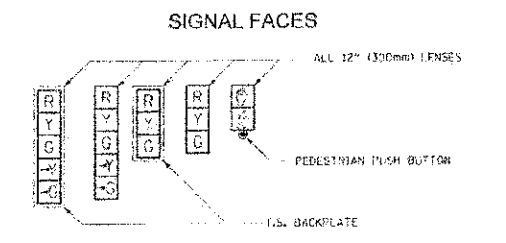
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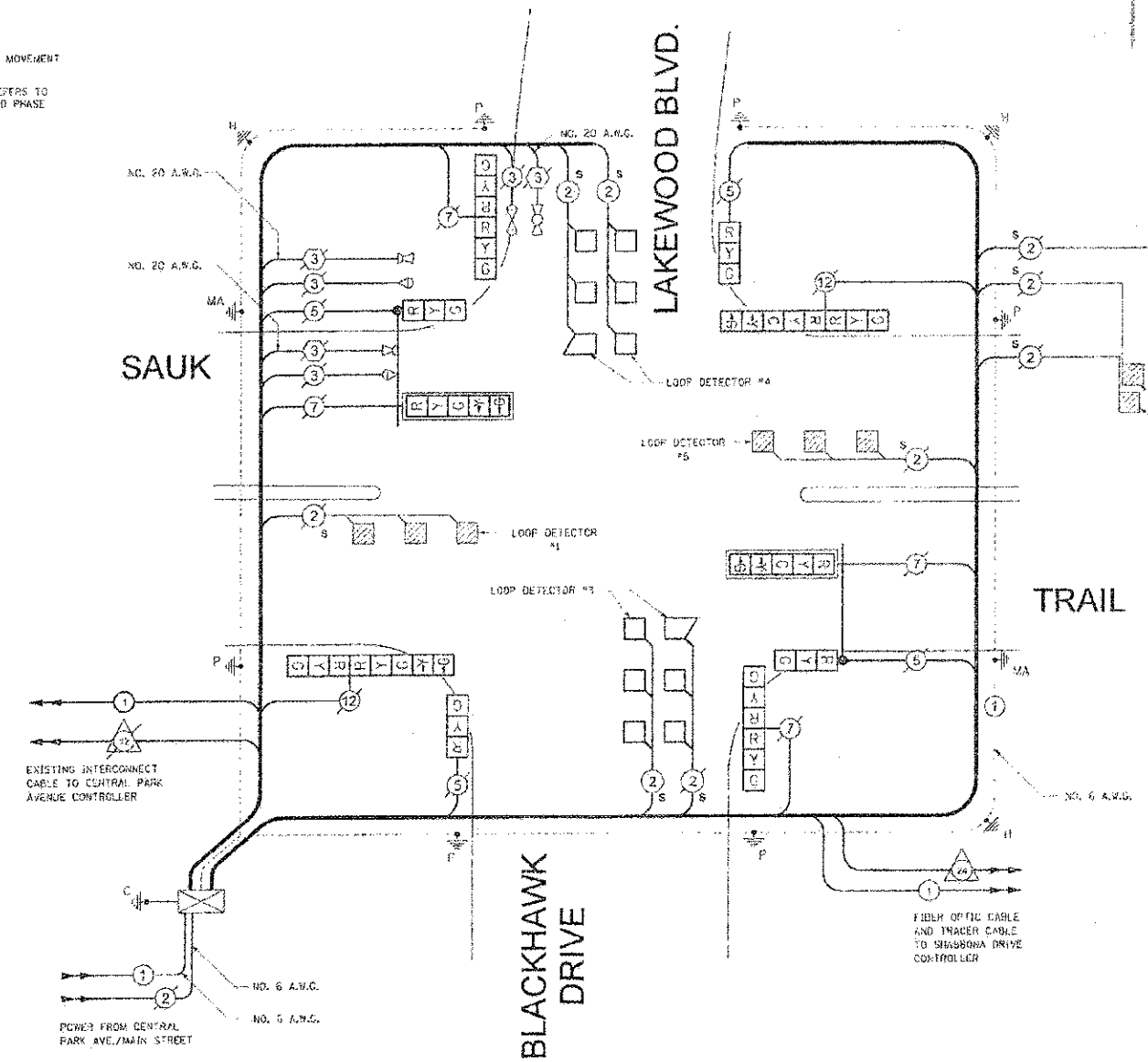
PHASE DESIGNATION DIAGRAM
 DUAL ENTRY - ALL LEGS
 PROTECTED / PERMITTED LEFT TURN PHASING



| PROPOSED EMERGENCY VEHICLE PREEMPTION | 3 | 4 |
|---------------------------------------|---|---|
| EMERGENCY VEHICLE PREEMPTION | | |
| MOVEMENT | | |



- SIGNAL LENSES**
- R RED
 - Y YELLOW
 - G GREEN
 - Y GREEN TURN INDICATOR
 - G GREEN TURN INDICATOR



CABLE PLAN LEGEND

| | PROPOSED | EXISTING |
|--|----------|----------|
| CONTROLLER CABINET | [Symbol] | [Symbol] |
| SERVICE INSTALLATION | [Symbol] | [Symbol] |
| 12" TRAFFIC SIGNAL SECTION | [Symbol] | [Symbol] |
| (DENOTES NUMBER OF CONDUCTORS ALL CABLE NO. 10 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE TWISTED AND SHIELDED.) | [Symbol] | [Symbol] |
| VEHICLE DETECTOR INDUCTION LOOP | [Symbol] | [Symbol] |
| SHIELDED & TWISTED | [Symbol] | [Symbol] |
| EMERGENCY VEHICLE LIGHT DETECTOR | [Symbol] | [Symbol] |
| CONFIRMATION BEACON | [Symbol] | [Symbol] |
| OPTICOM CABLE | [Symbol] | [Symbol] |
| 300mm PEDESTRIAN SIGNAL HEAD | [Symbol] | [Symbol] |
| PUSHBUTTON DETECTOR | [Symbol] | [Symbol] |
| GROUND ROD AT HANDHOLE HD. COUPLE HANDHOLE 4" DIA. OR CONTROLLER I.D. | [Symbol] | [Symbol] |
| GROUND ROD AT POST 4" DIA. OR 3/4" DIA. ARM POLE DIA. | [Symbol] | [Symbol] |
| GROUND ROD AT ELECTRIC SERVICE INSTALLATION | [Symbol] | [Symbol] |
| 2" FBIR OPTIC INTERCONNECT CABLE NO. 62.5/125, 127C | [Symbol] | [Symbol] |
| 1" FBIR OPTIC CABLE NO. 62.5/125 MULTIMODE 12 FIBERS AND SINGLE MORE 12 FIBERS | [Symbol] | [Symbol] |

REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF ON THEIR OUTSIDE THE RIGHT-OF-WAY AT THEIR EXTREME. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2' EACH 14" T.S. POSTS
- 3' EACH 18" T.S. POSTS
- 10' EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 4' EACH SIGNAL HEAD, 1-FACE, 3-SECTION

NOTES:

- THIS IS A SYSTEM GROUND THAT SHALL INTERCONNECT ALL GROUND RODS WITH NO. 5 1/2 COPPER AWG.
- THE NEUTRAL AND GROUND SHALL BE TIED AT THE SERVICE INSTALLATION BUT SEPARATE AT THE TRAFFIC CABINET.

Reference Only

| | |
|--|-------------------------|
| COUNTY OF COOK DEPARTMENT OF HIGHWAYS | |
| SAUK-TRAIL AT LAKEWOOD BLVD. - BLACKHAWK DR. CABLE DIAGRAM | |
| DESIGNED: R.L.S. | DATE: 08/17/12 |
| CHECKED: T.A.O. | PROJECT: 12-00097-00 R5 |
| DRAWN: J.L.W. | CONTRACT NO. 63740 |

COUNTY OF COOK, DEPARTMENT OF HIGHWAYS, 1057 S. LAKE STREET, CHICAGO, ILLINOIS 60607
 PROJECT: 12-00097-00 R5, CONTRACT NO. 63740, SHEET NO. 24 OF 24
 DATE: 08/17/12, FILE: 120499-SIGNAL PLANS.spt



| | |
|-----------------|--------------------------------|
| DESIGNED - BNP | REVISED - |
| DRAWN - ECC | REVISED - |
| CHECKED - TAO | REVISED - |
| DATE - 08/17/12 | FILE - 120499-SIGNAL PLANS.spt |

**COOK COUNTY HIGHWAY DEPARTMENT
 SIGNAL PLANS - REFERENCE ONLY**

| | | | | |
|---|----------------|------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1057 | 12-00097-00 R5 | COOK-ILL. | 24 | 24 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | M-40031077 | | |