

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| 2592 | 531-T | COOK | 10 | 1 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PROPOSED
HIGHWAY PLANS**

FAU ROUTE 2592: HICK'S ROAD
OVER SALT CREEK
SN 016-2008
SECTION 531-T
CULVERT REPAIR
COOK COUNTY

C-91-217-05

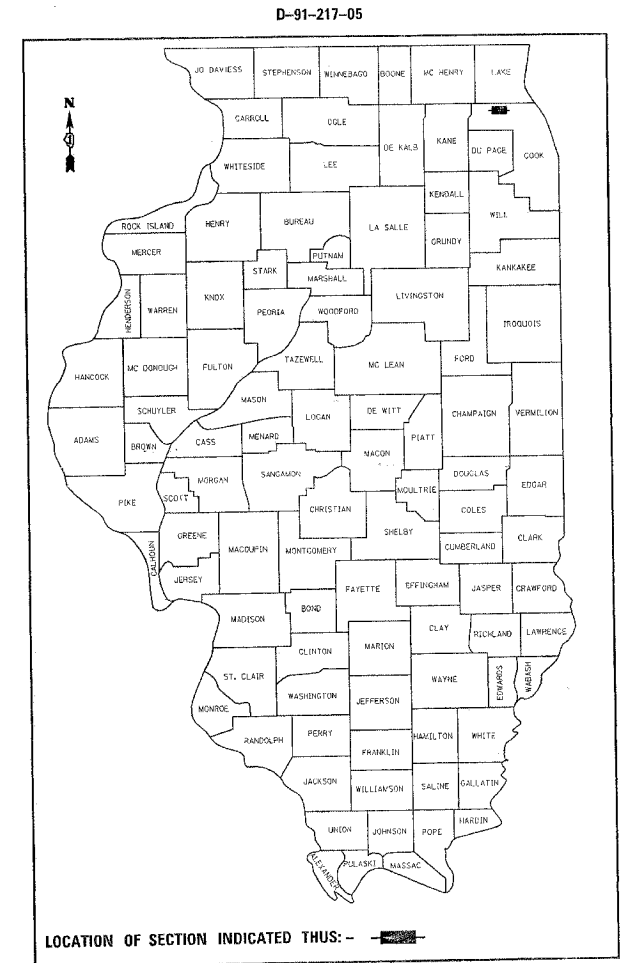
FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS IMPROVEMENT IS LOCATED IN
THE VILLAGE OF PALATINE

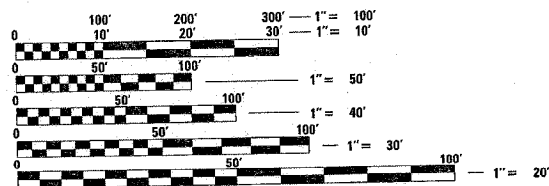
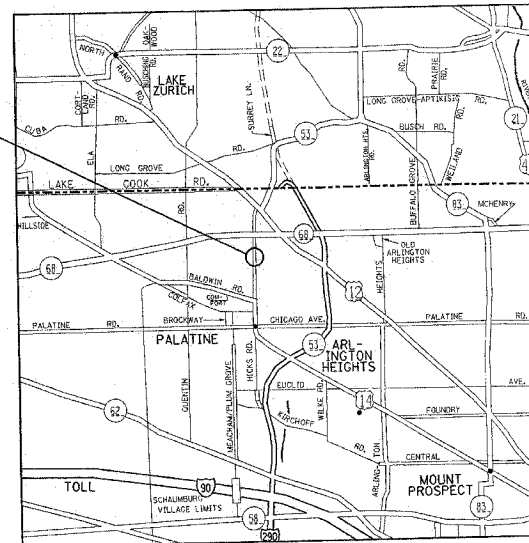
TRAFFIC DATA:

ADT (2002): 21200

SPEED LIMIT: 40 MPH



LOCATION
HICK'S ROAD OVER SALT CREEK
SN 016-2008



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 62964

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 8/30 2006

Brian O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

October 13, 2006
Mike Hine
ENGINEER OF DESIGN AND ENVIRONMENT

October 13, 2006
Milton R. See, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

DISTRICT ONE DESIGN PLAN PREPARATION ENGINEER
KEN ENG/ROBERT BORO (847) 705-4178

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| 2592 | 531-T | COOK | 10 | 2 |

INDEX OF SHEETS

1. COVER SHEET
2. INDEX OF SHEETS, GENERAL NOTES, HIGHWAY STANDARDS
3. SUMMARY OF QUANTITIES
4. TYPICAL SECTION, ROADWAY PLANS
5. CULVERT WALL REPAIRS
6. CULVERT TOP SLAB REPAIRS
7. CROSS SECTIONS AND BAR LIST
8. TRAFFIC CONTROL AND PROTECTION AT SIDEROADS, INTERSECTIONS & DRIVEWAYS
9. DISTRICT ONE TYPICAL PAVEMENT MARKINGS
10. TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

HIGHWAY STANDARDS

- | | |
|-----------|---|
| 420001-06 | PAVEMENT JOINTS |
| 420701-01 | PAVEMENT FABRIC |
| 442101-05 | CLASS B PATCHES |
| 701301-02 | LANE CLOSURE, 2L 2W, SHORT TIME OPERATIONS |
| 701606-04 | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIUM |
| 701801-03 | LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE |
| 702001-06 | TRAFFIC CONTROL DEVICES |
| 704001-02 | TEMPORARY CONCRETE BARRIER |

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E" AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT

USE #8 EPOXY COATED TIE BARS, CONFORMING TO ARTICLE 1006.10 OF THE STANDARD SPECS. FOR ALL TIE BARS. USE THE "LONGITUDINAL CONSTRUCTION JOINT (TIE BARS. GROUTED IN PLACE)" DETAIL SHOWN ON HWY STD 420001 FOR ALL LONGITUDINAL JOINTS.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS

BRIDGE GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING STRUCTURE HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

THE CONTRACTOR SHALL DIVERT THE WATER FROM THE WORK AREAS BY A METHOD APPROVED BY THE ENGINEER.

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS
STATE STANDARDS
GENERAL NOTES

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 2592 | 531-T | COOK | 10 | 3 |

| SUMMARY OF QUANTITIES | | | CONSTRUCTION TYPE CODE | | | | | | |
|-----------------------|--|-------|------------------------|------------|---------|---|--|--|--|
| CODE NO | ITEM | UNIT | TOTAL QUANTITIES | URBAN | | | | | |
| | | | | 100% STATE | | | | | |
| | | | | SFTY-2A | SFTY-3N | | | | |
| 20101700 | SUPPLEMENTAL WATERING | UNIT | 10 | 10 | | | | | |
| 20200100 | EARTH EXCAVATION | CU YD | 72 | 72 | | | | | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 65 | 65 | | | | | |
| 25000210 | SEEDING, CLASS 2A | ACRE | 0.05 | 0.05 | | | | | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 10 | 10 | | | | | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 10 | 10 | | | | | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 10 | 10 | | | | | |
| 25002300 | TEMPORARY SEEDING | ACRE | 0.05 | 0.05 | | | | | |
| 25100630 | EROSION CONTROL BLANKET | SQ YD | 65 | 65 | | | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 180 | 180 | | | | | |
| 28100807 | STONE DUMPED RIPRAP, CLASS A4 | TON | 27 | 27 | | | | | |
| 42101300 | PROTECTIVE COAT | SQ YD | 240 | 240 | | | | | |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SQ FT | 540 | 540 | | | | | |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 540 | 540 | | | | | |
| 44001700 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 108 | 108 | | | | | |
| 44200976 | CLASS B PATCHES, TYPE IV, 10 INCH | SQ YD | 144 | 144 | | | | | |
| 44213200 | SAW CUTS | FOOT | 396 | 396 | | | | | |
| 50102400 | CONCRETE REMOVAL | CU YD | 30.2 | 30.2 | | | | | |
| 50800105 | REINFORCEMENT BARS | POUND | 8860 | 8860 | | | | | |
| 54003000 | CONCRETE BOX CULVERTS | CU YD | 27.0 | 27.0 | | | | | |
| 63301000 | REMOVE AND RE-ERECT STEEL PLATE BEAM GUARD RAIL | FOOT | 108 | 108 | | | | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | | | | |
| 70102625 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701606 | L SUM | 1 | 1 | | | | | |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 | 1 | | | | | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 220 | 220 | | | | | |
| X0322194 | POLYMER MODIFIED PORTLAND CEMENT MORTAR | SQ FT | 15 | 15 | | | | | |
| X0322467 | TEMPORARY INFORMATION SIGNING FOR LANE CLOSURE | SQ FT | 48 | 48 | | | | | |
| X0323988 | TEMPORARY SOIL RETENTION SYSTEM | SQ FT | 266 | 266 | | | | | |
| X0325305 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES) | SQ FT | 143 | 143 | | | | | |
| *XX006058 | POLYUREA PAVEMENT MARKING - LINE 4" | FOOT | 20 | 20 | | | | | |
| Z0017100 | DOWEL BARS | EACH | 40 | 40 | | | | | |
| Z0030240 | IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE) TEST LEVEL 2 | EACH | 2 | 2 | | 2 | | | |
| Z0075325 | TIE BARS 1" | EACH | 212 | 212 | | | | | |
| 20201550 | SUBBASE GRANULAR MATERIAL, TYPE B | CU YD | 72 | 72 | | | | | |

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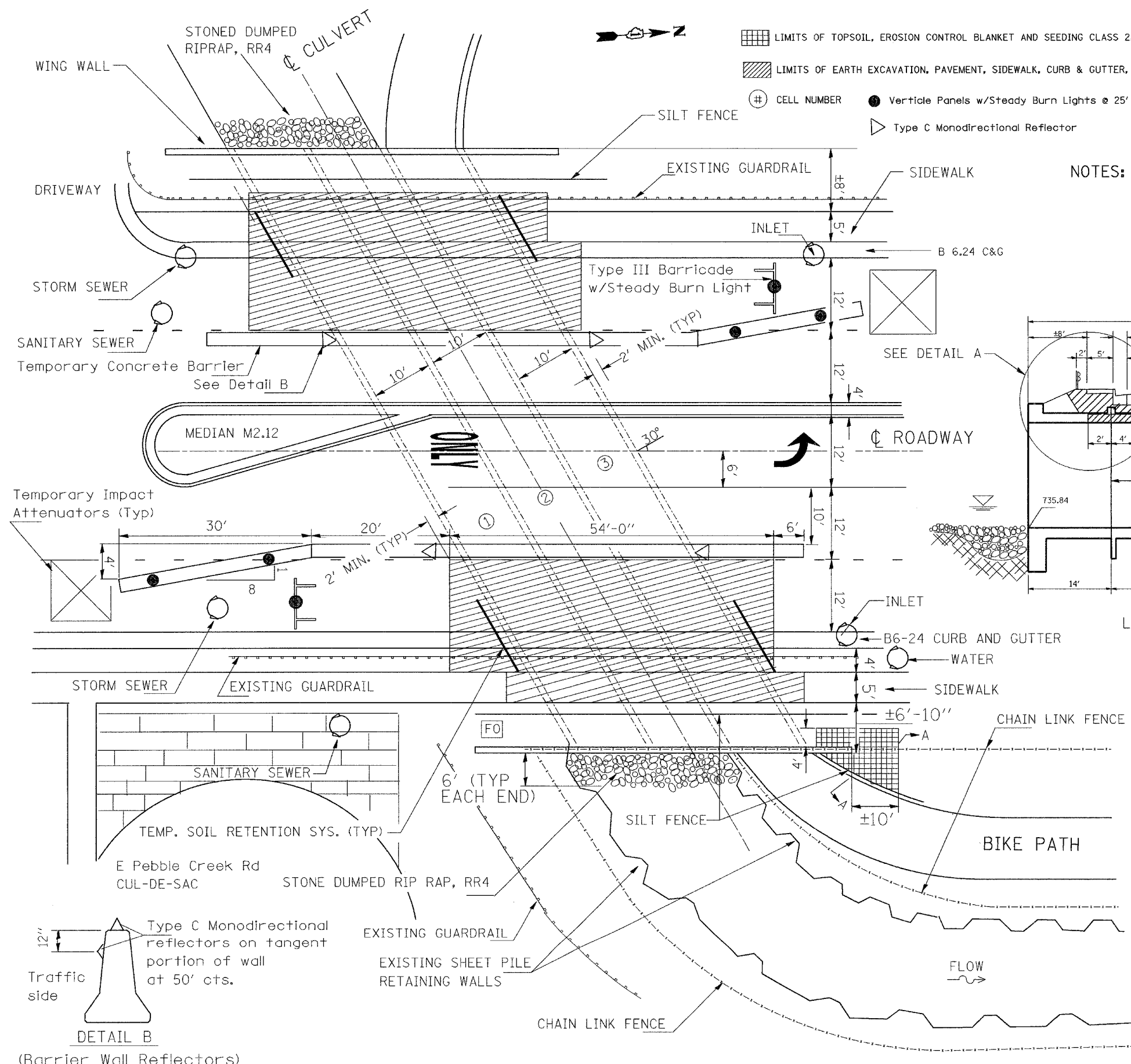
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|-----------|------|--|
| NAME | DATE | |
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PLOT DATE: 9/12/2006

*SPECIALTY ITEMS

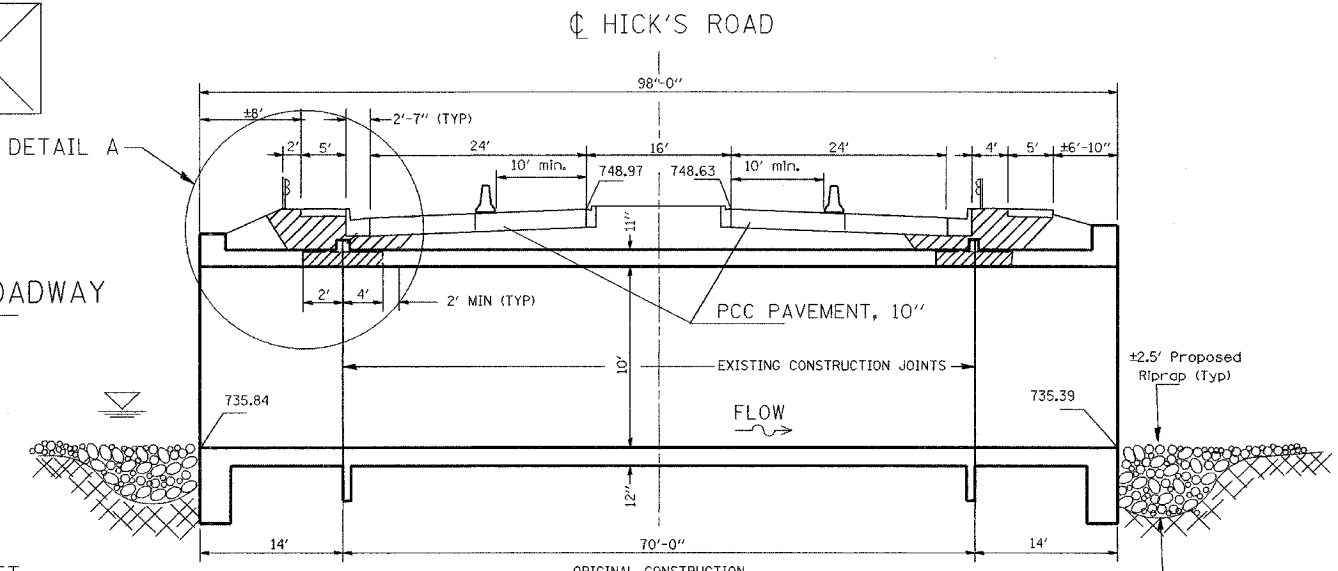
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 2592 | 531-T | COOK | 10 | 4 |



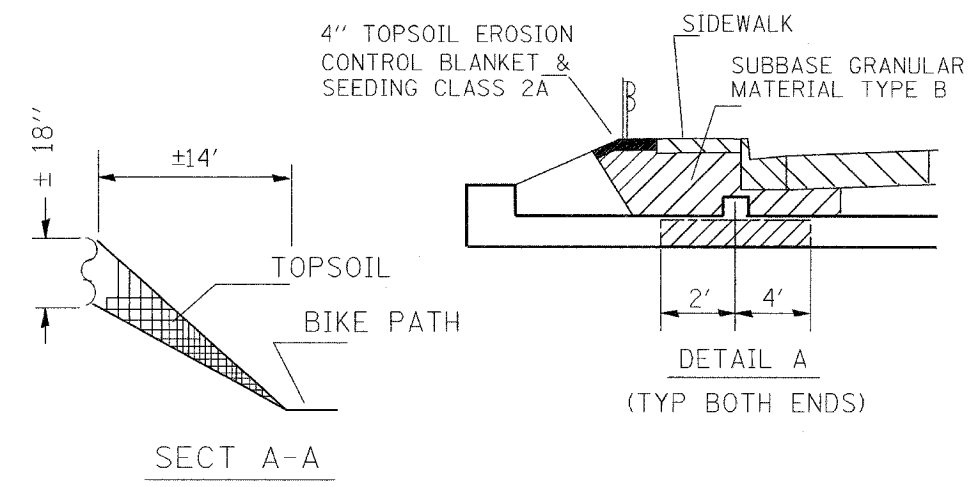
NOTES: BIKE/PEDESTRIAN PATH SHOULD BE CLOSED AND ADVANCED WARNING SIGNS SHOULD BE PLACED FAR ENOUGH AHEAD TO WARN BICYCLISTS AND PEDESTRIANS TO CROSS AT FIRST BANK DRIVE ACCORDING TO STANDARD 701801 WHEN WORKING IN CELL 3.

STONE DUMPED RIPRAP SHALL NOT REQUIRE FILTER FABRIC OR A BEDDING LAYER.



LONGITUDINAL SECTION (LOOKING NORTH)

ALL DIMENSIONS @ RIGHT ANGLES TO C OF ROADWAY



SECT A-A

| REVISIONS | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION

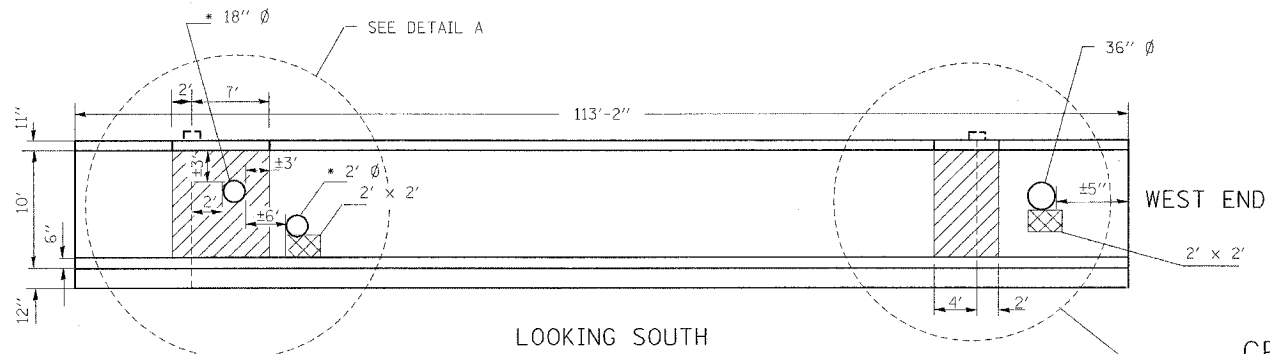
TYPICAL SECTION, ROADWAY PLANS

SCALE: VERT. HORIZ. DATE

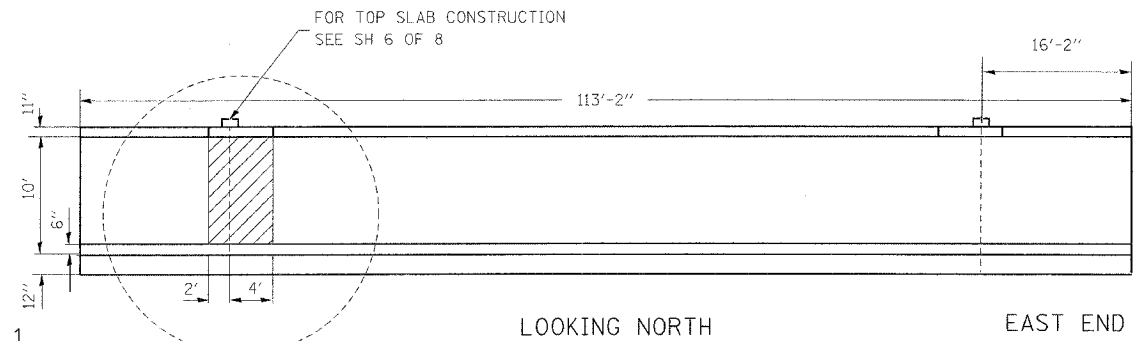
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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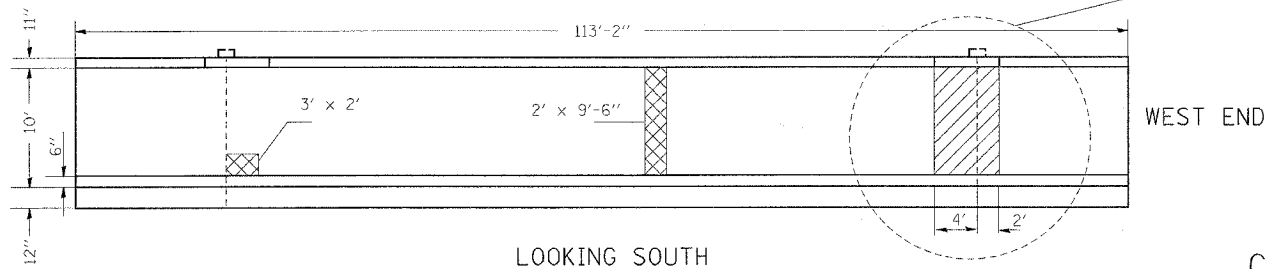


CELL 1

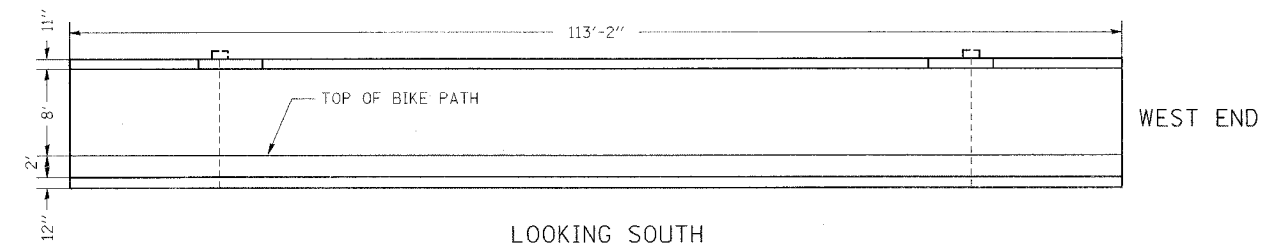
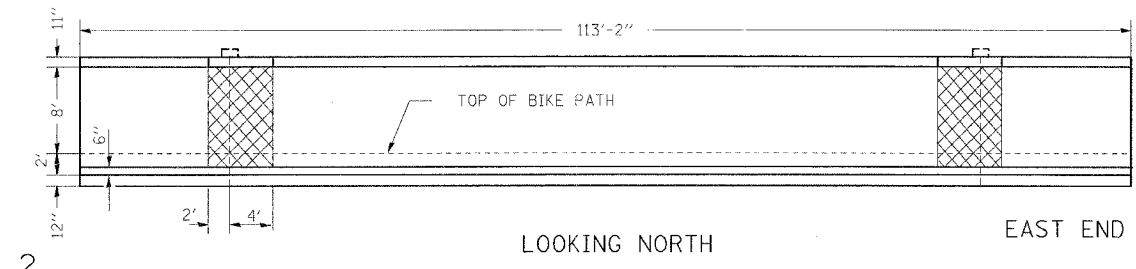


* PIPES SHALL BE SUPPORTED DURING WALL REPAIRS. METHOD TO BE APPROVED BY THE ENGINEER. PIPE SHALL BE CUT FLUSH WITH THE INSIDE FACE OF WALL. COST INCLUDED IN CONCRETE REMOVAL.

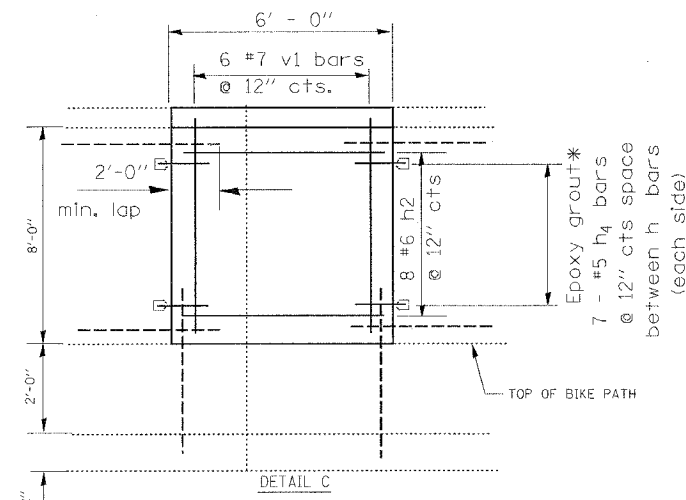
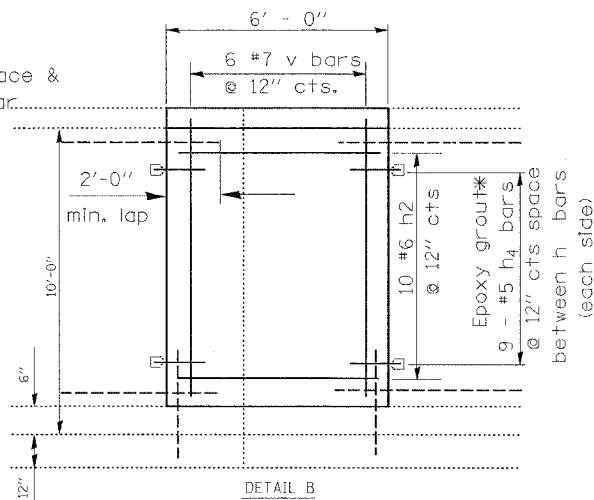
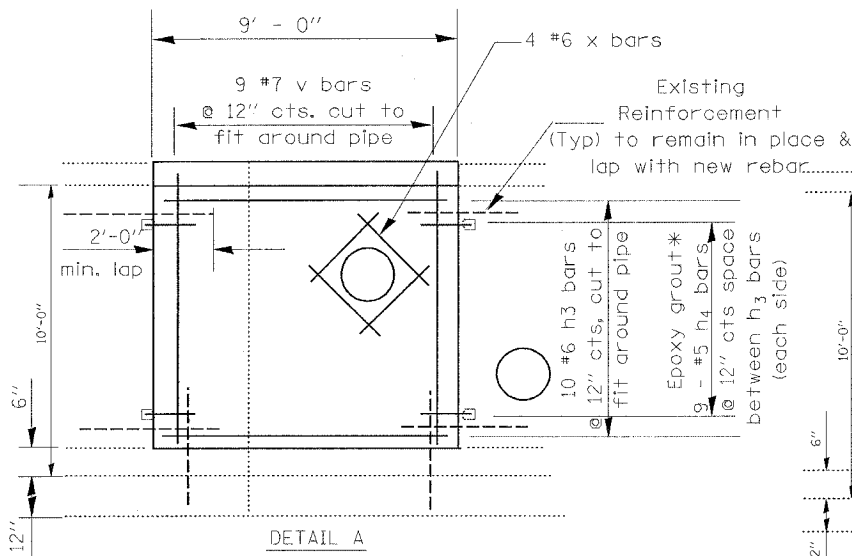
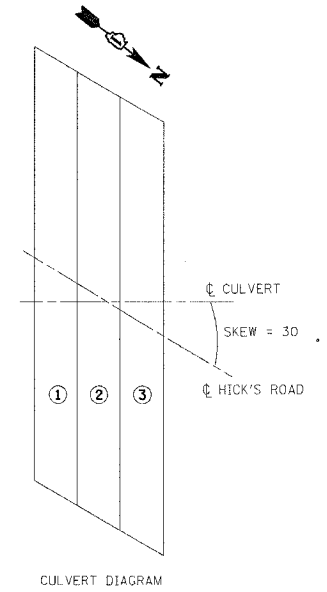
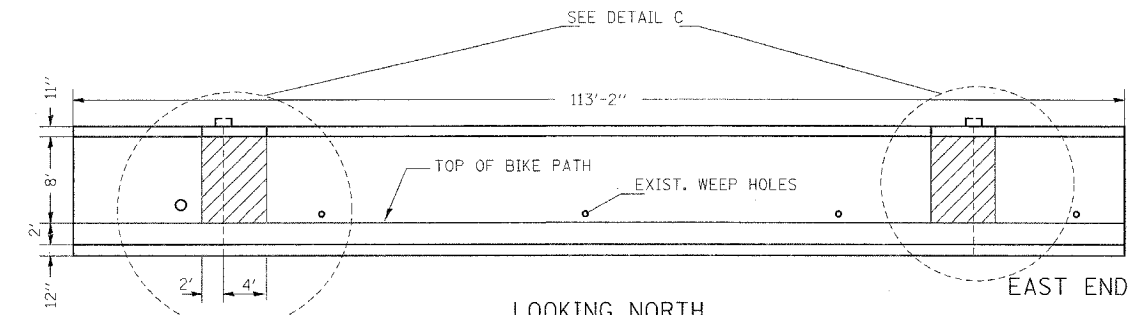
SEE DETAIL B



CELL 2



CELL 3

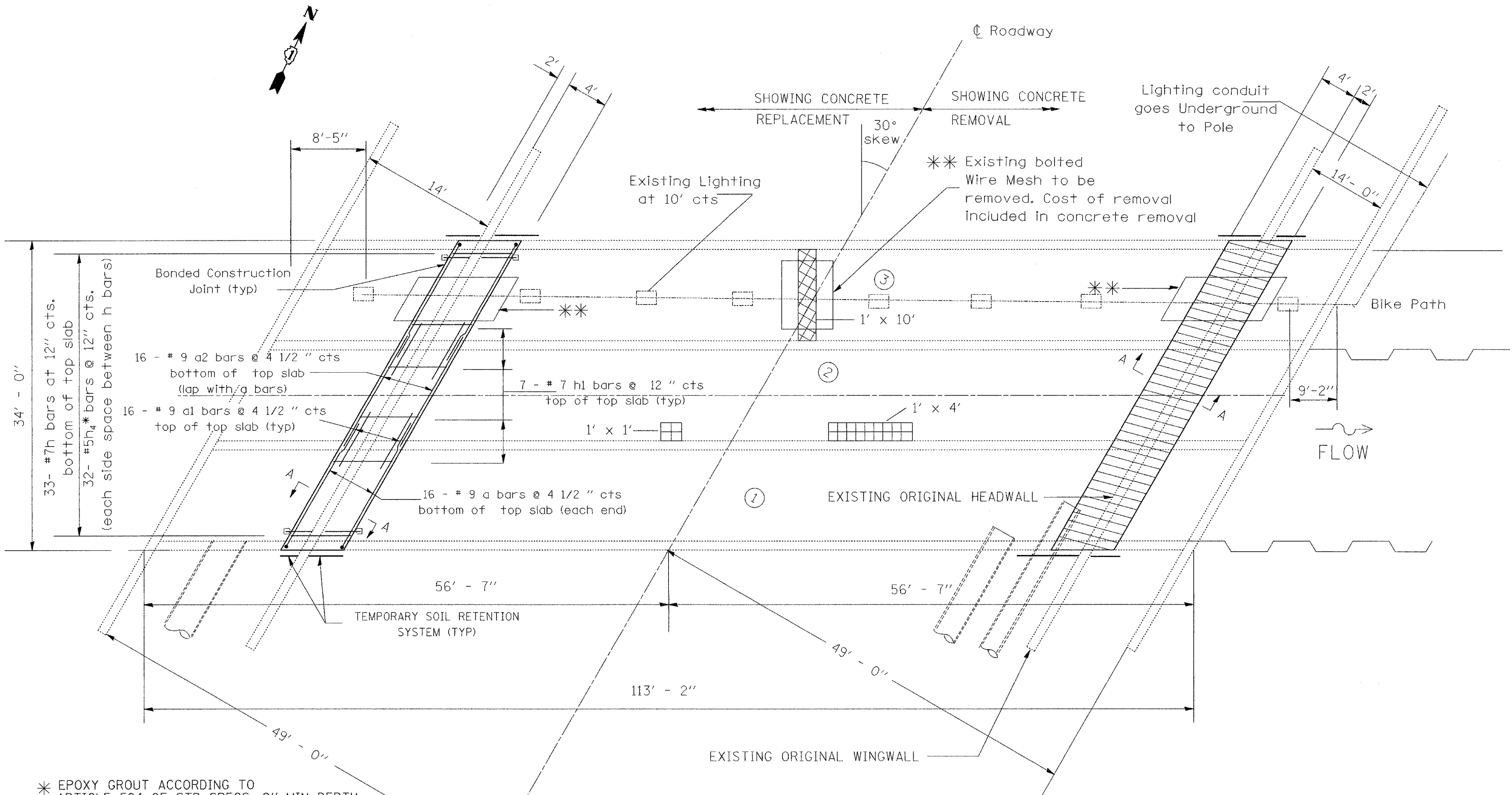


- CONCRETE REMOVAL AND CONCRETE BOX CULVERT
- FORMED CONCRETE REPAIR, ≤ 5"
- * ACCORDING TO ART. 584 OF STD SPEC 9" MIN DEPTH



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ILLINOIS DEPARTMENT OF TRANSPORTATION
CULVERT WALL REPAIRS
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 USER NAME = banks1



TOP SLAB PLAN

- * EPOXY GROUT ACCORDING TO ARTICLE 584 OF STD SPECS. 9" MIN DEPTH
 -  CONCRETE REMOVAL AND CONCRETE BOX CULVERT (ACCESSED FROM ABOVE ROADWAY)
 -  POLYMER MODIFIED PORTLAND CEMENT MORTAR BOTTOM OF TOP SLAB (THIS WORK WILL BE PERFORMED FROM INSIDE THE CULVERT)
- FOR SECTION A-A SEE SH 7 OF 8

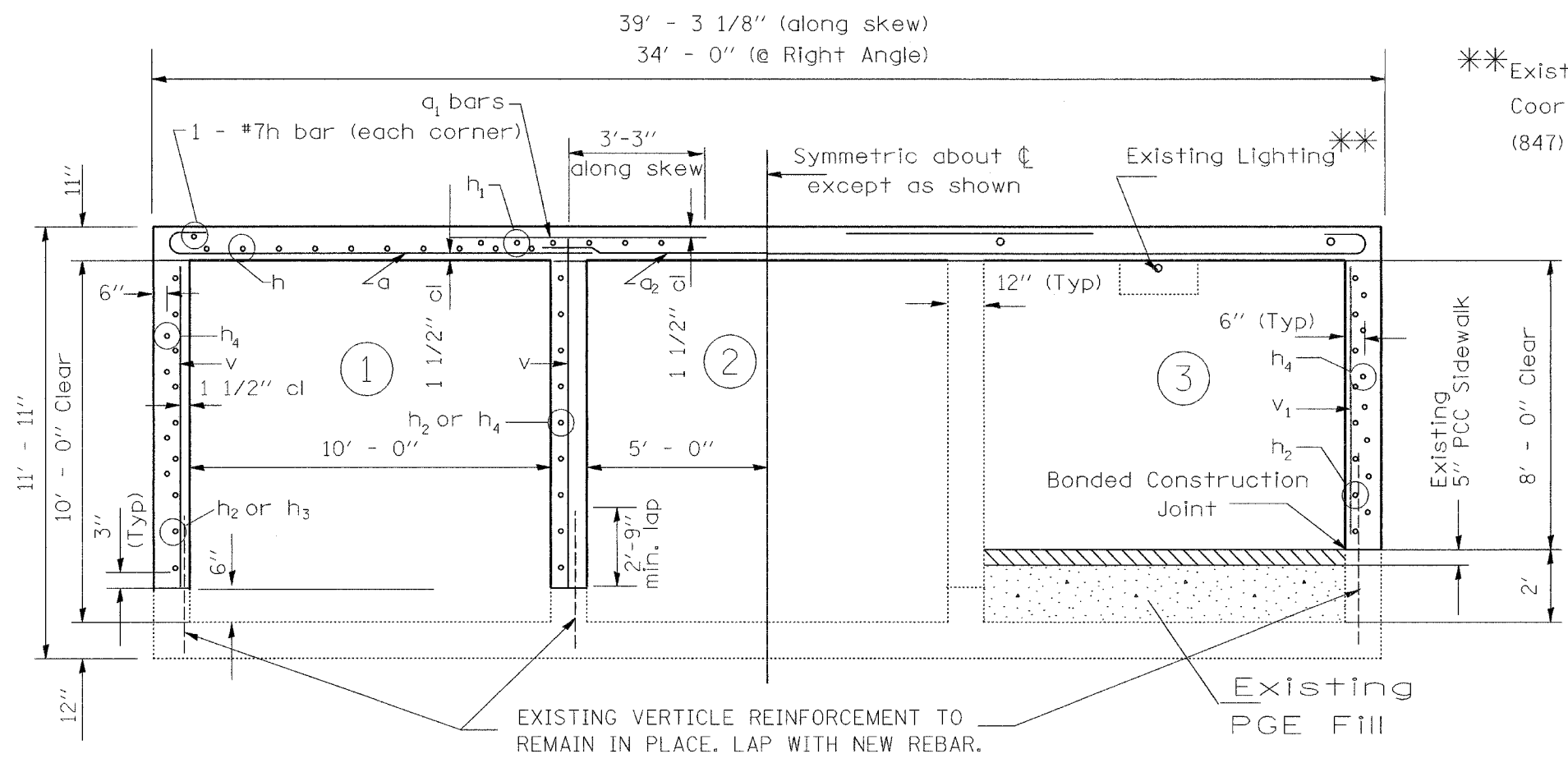
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ILLINOIS DEPARTMENT OF TRANSPORTATION

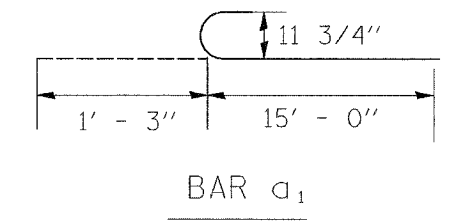
TOP SLAB REPAIRS

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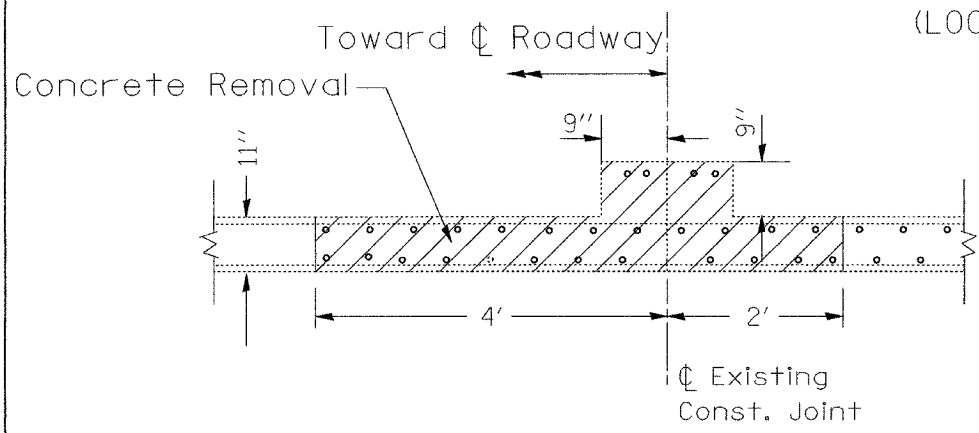
** Existing Lighting shall be protected and maintained. Coordinate with the Palatine Park District, Ed Tynczuk, (847) 705-5131.



BILL OF MATERIAL

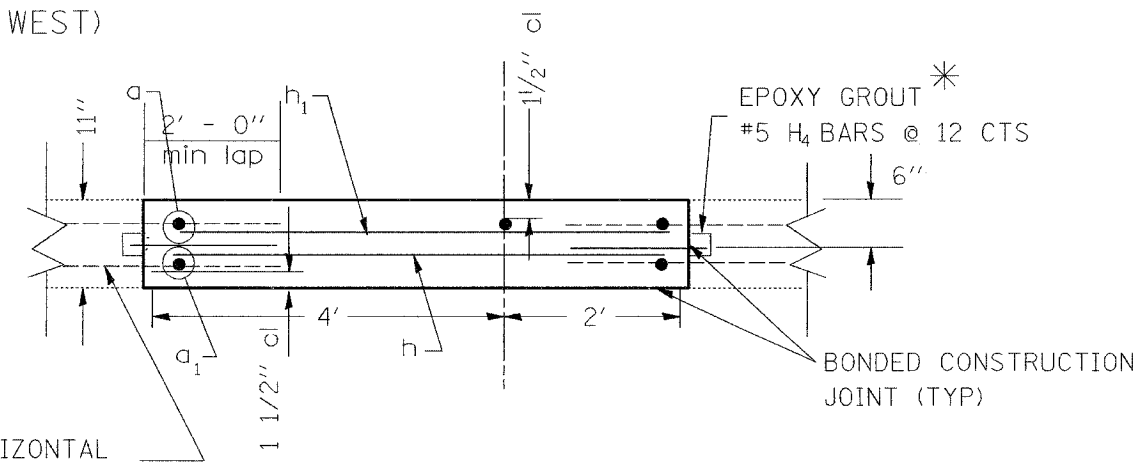
| BAR | No. | SIZE | LENGTH | SHAPE |
|-----------------------|-----|-------|--------|-------|
| a | 64 | #9 | 16'-3" | |
| a_1 | 64 | #9 | 6'-6" | |
| a_2 | 32 | #8 | 12'-9" | |
| h | 70 | #7 | 6'-8" | |
| h_1 | 28 | #7 | 6'-8" | |
| h_2 | 36 | #6 | 5'-9" | |
| h_3 | 10 | #6 | 8'-9" | |
| h_4 | 210 | #5 | 3'-6" | |
| v | 21 | #7 | 10'-0" | |
| v_1 | 12 | #7 | 8'-6" | |
| x | 4 | #6 | 3'-9" | |
| Concrete Box Culverts | | CU YD | 27.0 | |
| Reinforcement Bars | | POUND | 8860 | |

HALF SECTION THROUGH BARREL
(Dimensions @ RT Angles to Barrel)



SECTION A-A
(EXISTING TOP SLAB)

(LOOKING WEST)



SECTION A-A
(PROPOSED TOP SLAB)

* ACCORDING TO ARTICLE 584 OF STANDARD SPECS. MIN. 9" DEPTH

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ILLINOIS DEPARTMENT OF TRANSPORTATION

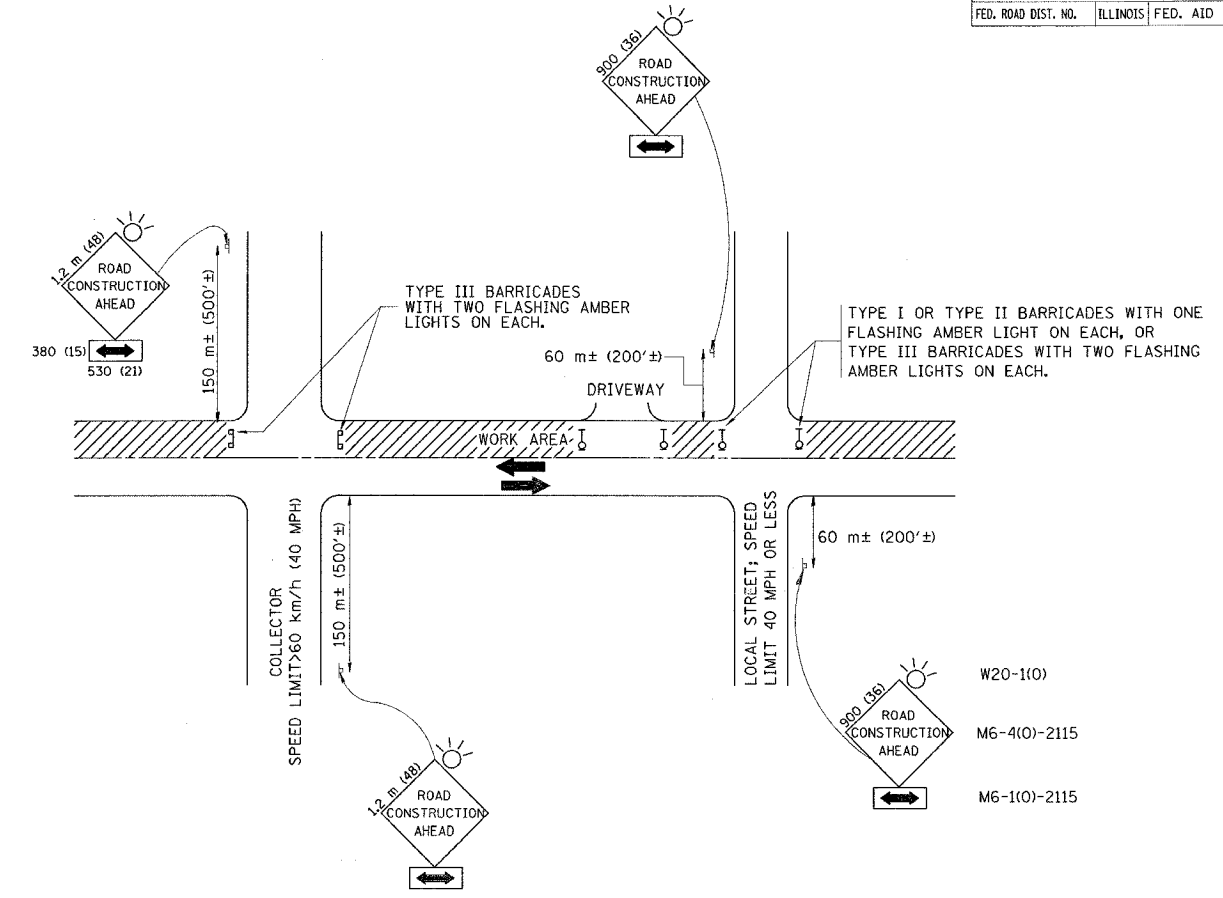
CELL TYPICAL SECTION
BAR LIST

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DATE

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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 25th | 531-T | Cook | 10 | 8 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS**
- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:**
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.**
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.**

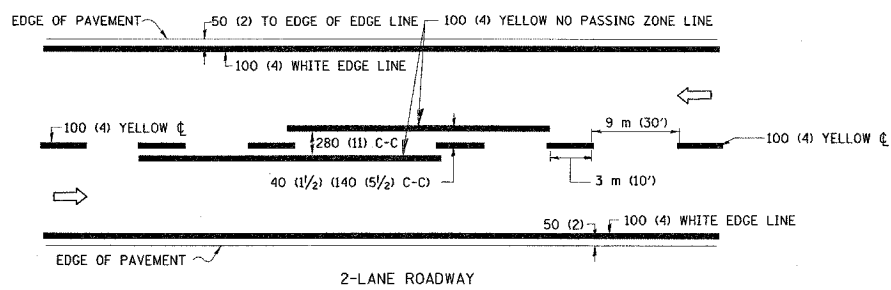
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|--------------|----------|
| NAME | DATE |
| LHA | 6/89 |
| T. RAMMACHER | 09/08/94 |
| J. OBERLE | 10/18/95 |
| A. HOUSEH | 03/06/96 |
| A. HOUSEH | 10/15/96 |
| T. RAMMACHER | 01/06/00 |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

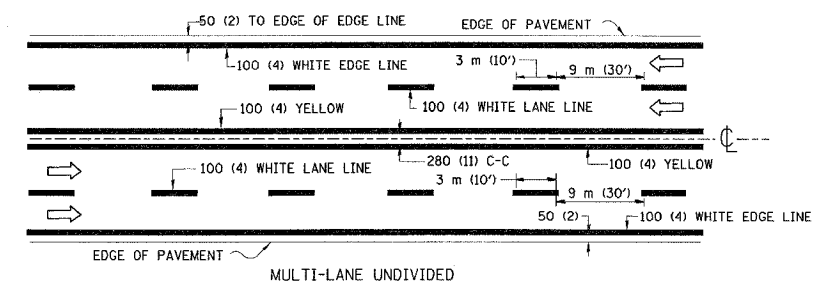
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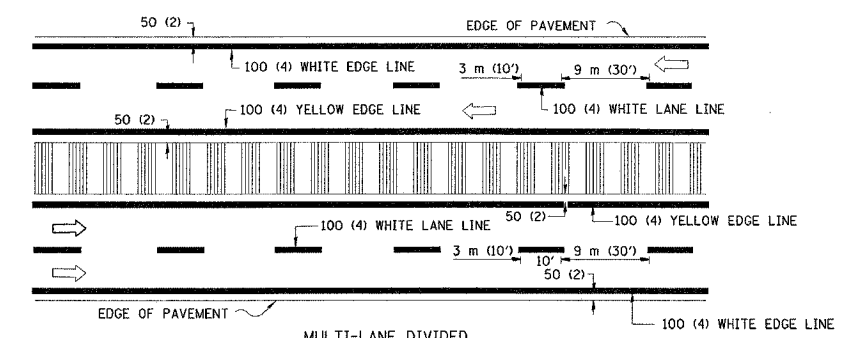
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|---------------------------|--------|--------------|-----------|
| 2592 | 531-T | COOK | 10 | 9 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |



2-LANE ROADWAY



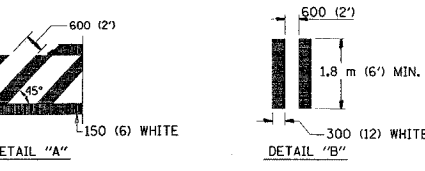
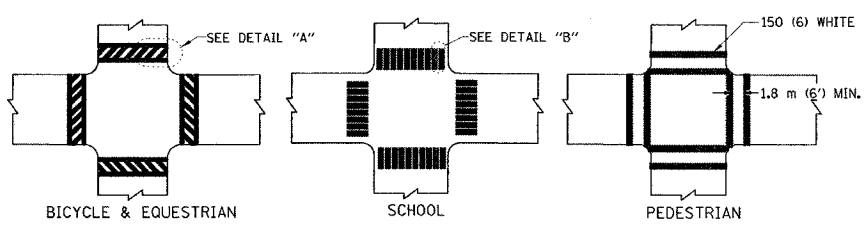
MULTI-LANE UNDIVIDED



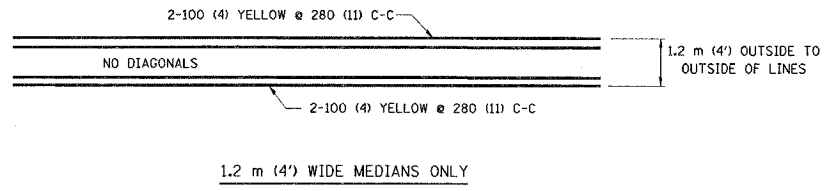
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

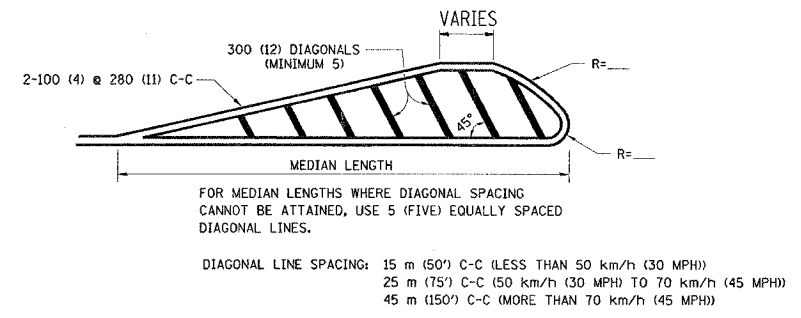
TYPICAL LANE AND EDGE LINE MARKING



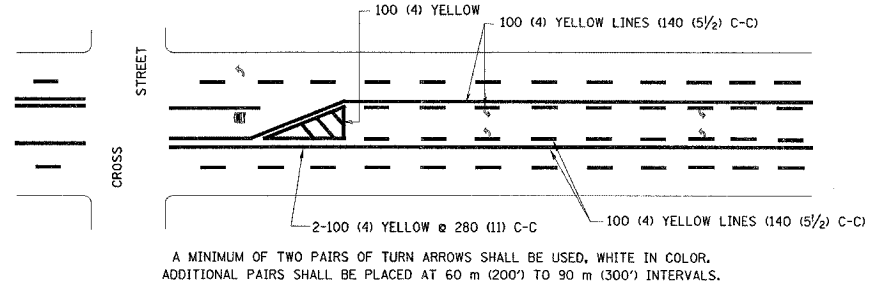
TYPICAL CROSSWALK MARKING



1.2 m (4') WIDE MEDIANS ONLY

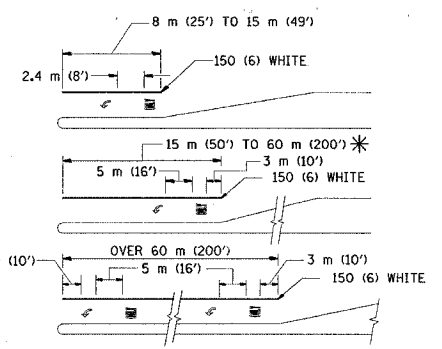


MEDIANS OVER 1.2 m (4') WIDE



TYPICAL PAINTED MEDIAN MARKING

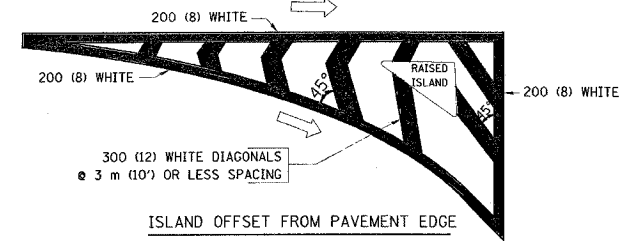
MEDIAN WITH TWO-WAY LEFT TURN LANE



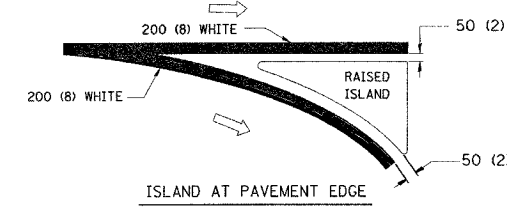
FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 100 (4) | SKIP-DASH | YELLOW | 3 m (10') LINE WITH 9 m (30') SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 100 (4) | SOLID | YELLOW | 280 (11) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 100 (4) 2 @ 100 (4) | SOLID SOLID | YELLOW YELLOW | 140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 100 (4) 125 (5) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 3 m (10') LINE WITH 9 m (30') SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 600 (2') LINE WITH 1.8 m (6') SPACE |
| EDGE LINES | 100 (4) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8')) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 1.8 m (6') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 600 (24) | SOLID | WHITE | PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 200 (8) WITH 300 (12) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH)) |
| RAILROAD CROSSING | 600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=0.33m ² (3.6 SQ. FT.) EACH "X"=5.0 m ² (54.0 SQ. FT.) |
| SHOULDER DIAGONALS | 300 (12) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

| REVISIONS | |
|--------------|----------|
| NAME | DATE |
| EVERS | 03-19-90 |
| T. RAMMACHER | 10-27-94 |
| ALEX HOUSEH | 10-09-96 |
| ALEX HOUSEH | 10-17-96 |
| T. RAMMACHER | 01-06-00 |
| | |
| | |
| | |
| | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS

SCALE: NONE
 DATE: 9/7/2006
 DRAWN BY CADD
 CHECKED BY
 TC-13
 REVISION DATE: 01/06/00

DATE = 9/7/2006
 USER = NVC
 PLOT SCALE = 38.4000 / IN.
 USER NAME = bentel

