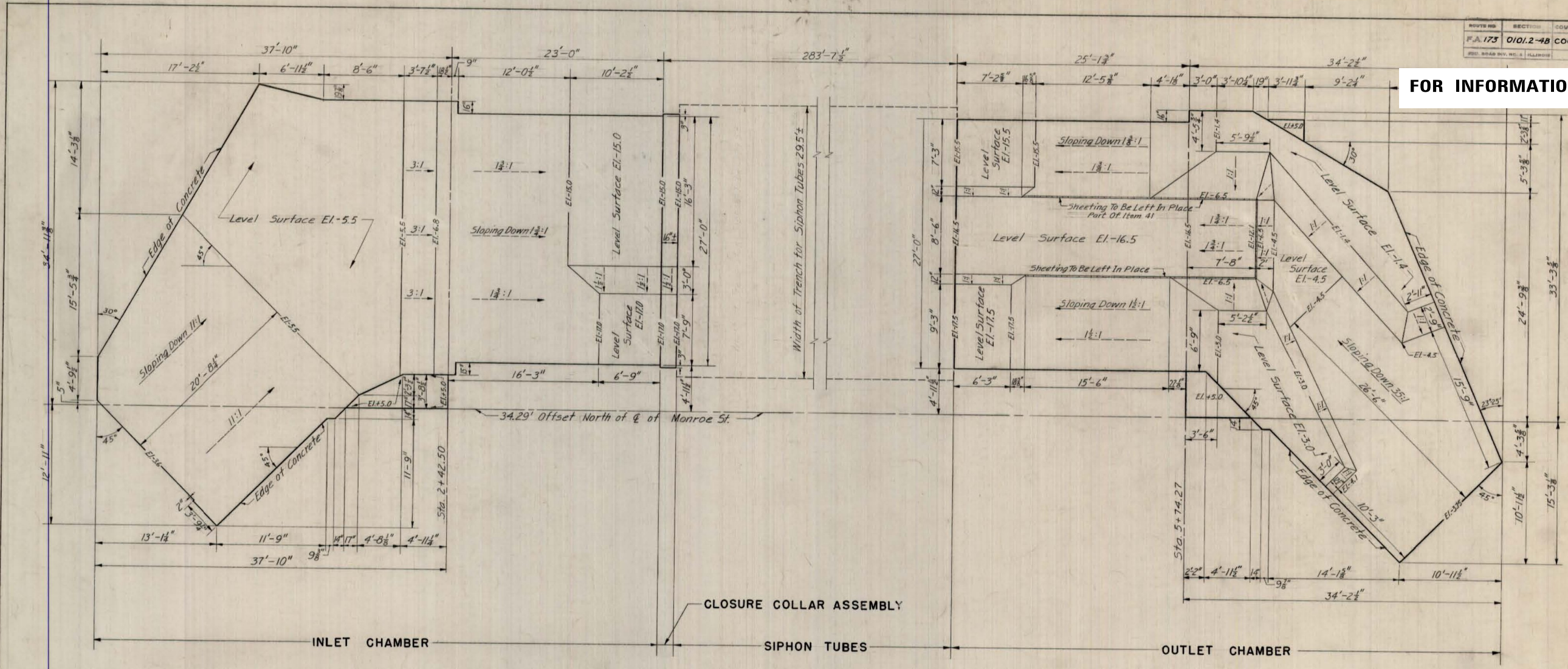


| | | | | |
|-----------|-----------|--------|--------------|-----------|
| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FA 173 | 0101.2-48 | COOK | 60 | 42 |

FOR INFORMATION ONLY



EXCAVATION PLAN

General Notes For Sewer Siphon:

The Location And Sizes of The Conduits, Pipes, Manholes And Other Sub-Surface Utility Equipment Shown On The Plans Are Based On The Best Information Obtainable And Are Believed To Be Accurate But Cannot Be Guaranteed. Additional Abandoned Utilities May Be Encountered. For Procedure in Handling These Abandoned Utilities See Section 18 of Special Provisions (City of Chicago).

All Elevations Are Measured From Chicago City Datum.

All Concrete Shall Conform To The Requirements of Article 52 of Standard Specifications, Except That Type II Portland Cement Shall Be Used.

The New Sewers Shall Be Completed Before The Existing Sewers Are Taken out of Service.

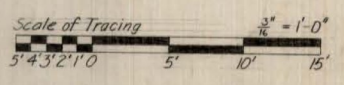
Stop Planks for Inlet And Outlet Chambers Are Not Included in This Contract.

For Location of Piles, Keyways and Reinforcement Steel Details, See Structural Sheets Identified By The Prefix Letter S.

See Special Provisions For Description of Pay Items.

DRAWN BY G. TANAKA
 TRACED BY M. WILLIAMS
 CHECKED BY H.H. CHU
 EXAMINED BY *Clint J. Keller*
 Asst. Engineer of Sewer Design

| REVISIONS | |
|-----------|-------------|
| DATE | DESCRIPTION |
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CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
 SEWER PLANNING DIVISION
 NORTHWEST ROUTE SUPERHIGHWAY
 MONROE ST. BRIDGE - SECTION 0101.2-48
**EXCAVATION PLAN FOR
 SEWER SIPHON**
 SCALE: 3/16" = 1'-0"
 DATE: FEBRUARY, 1954
 CONTRACT SHEET NO. 42

| | | | |
|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |

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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |

FILE NAME = T:\168988\Civil\68988\Monroe Street\Plotting\Print Export\168988-shr-EXSIPHON-98.dgn

**PARSONS
 BRINCKERHOFF**

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - MCC | REVISED - |
| PLOT SCALE = 1"=50' | DRAWN - MCC | REVISED - |
| PLOT DATE = 4/18/2017 | CHECKED - RT | REVISED - |
| | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

AS BUILT SIPHON
 PLAN AND DETAILS

SCALE: N.T.S. SHEET NO. 8 OF 13 SHEETS STA. TO STA.

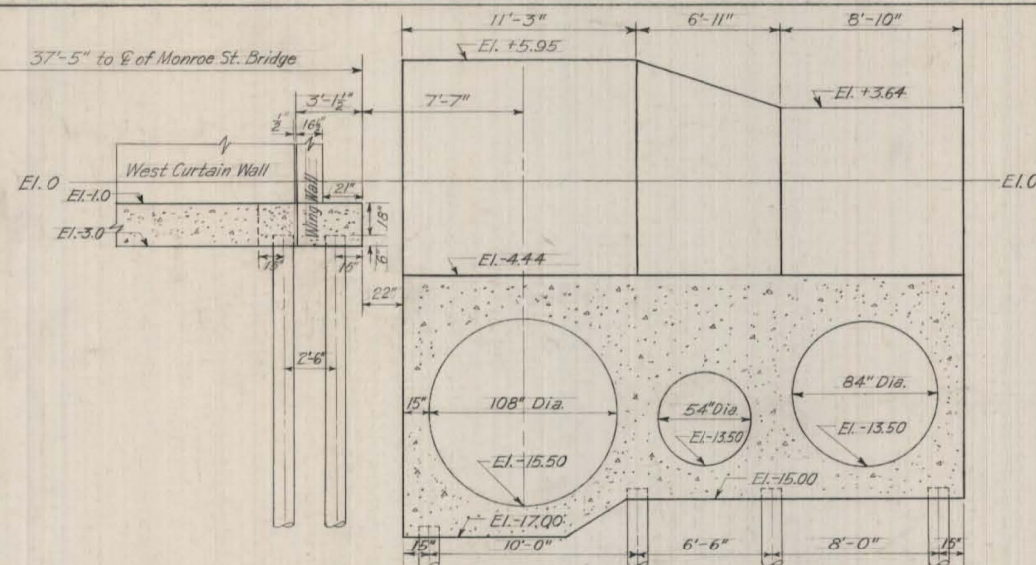
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|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 103 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

| | | | | |
|-----------|----------|--------|--------------|-----------|
| ROUTE NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| F.A. 173 | 01012-4B | COOK | 60 | 44 |
| PROJECT | | | UT 265 (6) | |

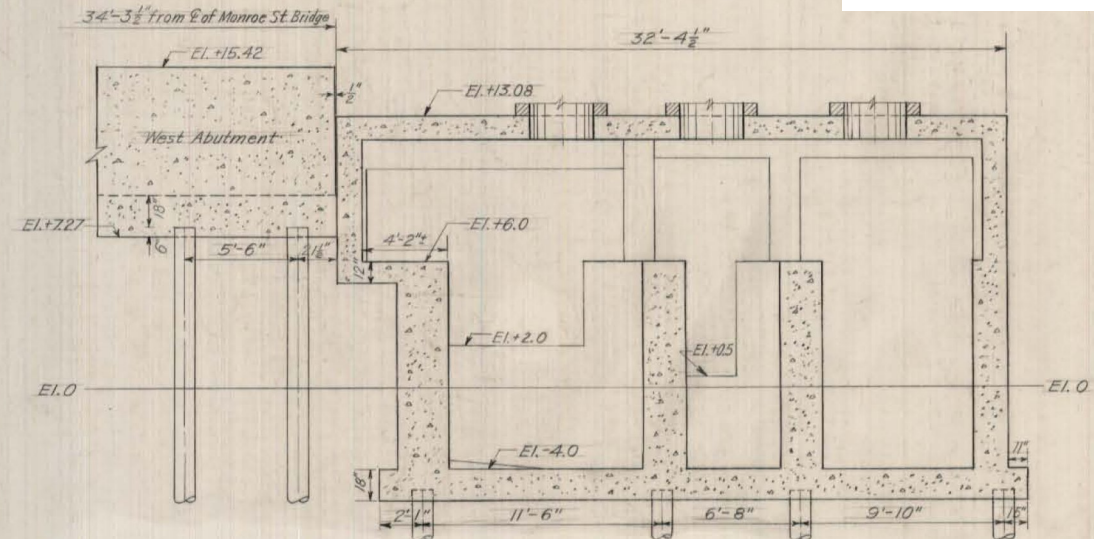
FOR INFORMATION ONLY

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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| | BY | |

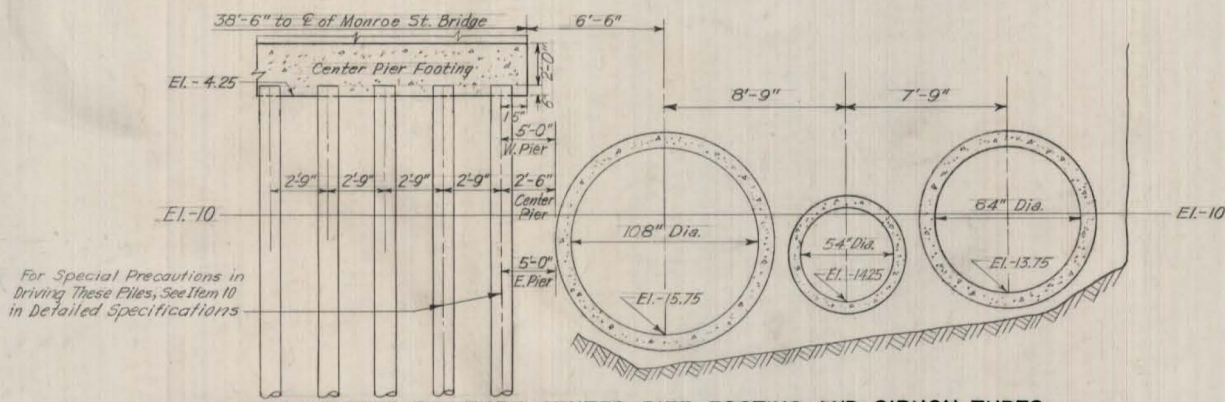
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| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| | BY | |



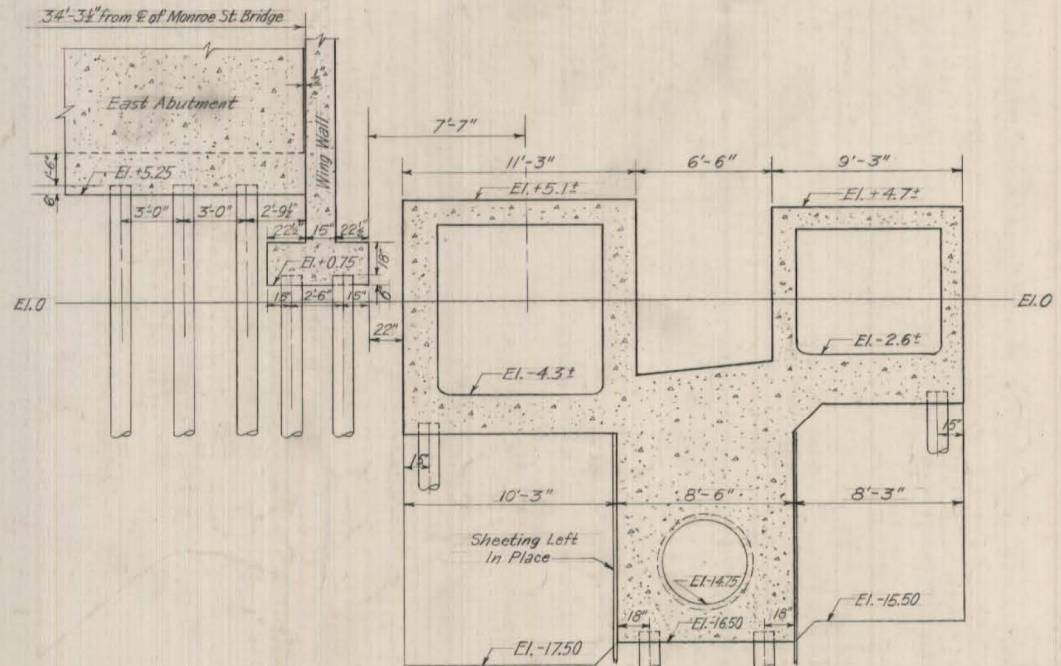
SECTION 2-2 THRU W. CURTAIN WALL & SIPHON INLET CHAMBER



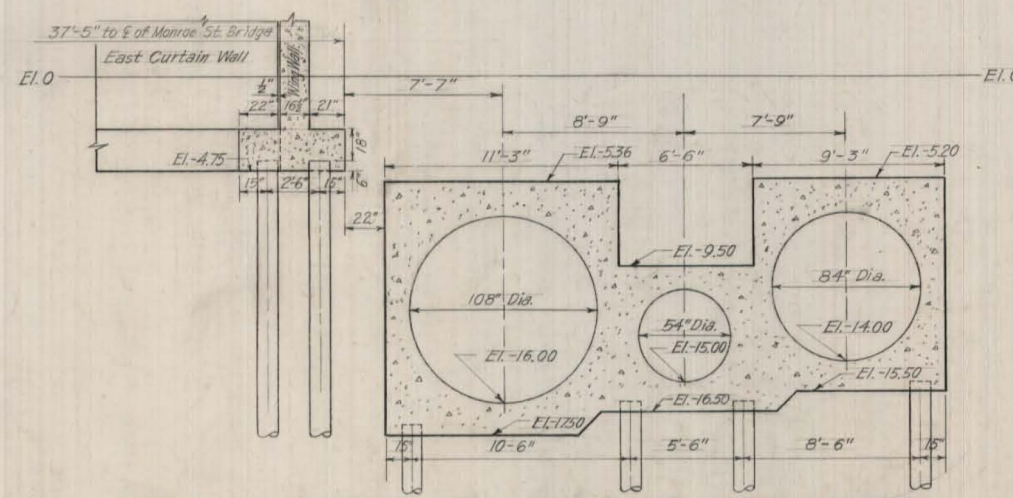
SECTION 1-1 THRU W. ABUTMENT AND SIPHON INLET CHAMBER



SECTION 3-3 THRU CENTER PIER FOOTING AND SIPHON TUBES

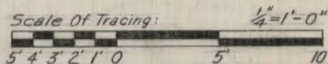


SECTION 5-5 THRU E. ABUTMENT AND SIPHON OUTLET CHAMBER



SECTION 4-4 THRU E. WING WALL & SIPHON OUTLET CHAMBER

DRAWN BY L. S. Abington
 TRACED BY L. S. Abington
 CHECKED BY H. M. Chu
 EXAMINED BY Cliff J. Reifer
 Asst. Engineer Of Sewer Design



Notes:
 For Location of Sections 1-1, 2-2, etc., See Sheet No. 35, 'General Plan & Profile'
 For Detail of Siphon Inlet Chamber See Sheet Nos. 38 & 39
 For Detail of Siphon Outlet Chamber See Sheet Nos. 40 & 41
 See Sheet 5-1 For Piling Plan.

| REVISIONS | |
|-----------|-------------|
| DATE | DESCRIPTION |
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CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
 SEWER PLANNING DIVISION
 NORTHWEST ROUTE SUPERHIGHWAY
 MONROE ST. BRIDGE - SECTION 0101.2-4B
**SECTIONS THRU SIPHON
 AND BRIDGE SUB-STRUCTURES**
 SCALE: 1/4" = 1'-0"
 DATE: FEBRUARY, 1954
 CONTRACT SHEET NO. **44**

FILE NAME = T:\168988\Civil\68988\Monroe Street\Plotting\Print Export\01012-4B-44-EXSIPHON-10.dgn

**PARSONS
 BRINCKERHOFF**

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|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - MCC | REVISED - |
| PLOT SCALE = 1"=50' | DRAWN - MCC | REVISED - |
| PLOT DATE = 4/18/2017 | CHECKED - RT | REVISED - |
| | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

AS BUILT SIPHON
 PLAN AND DETAILS

SCALE: N.T.S. SHEET NO. 10 OF 13 SHEETS STA. TO STA.

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 105 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

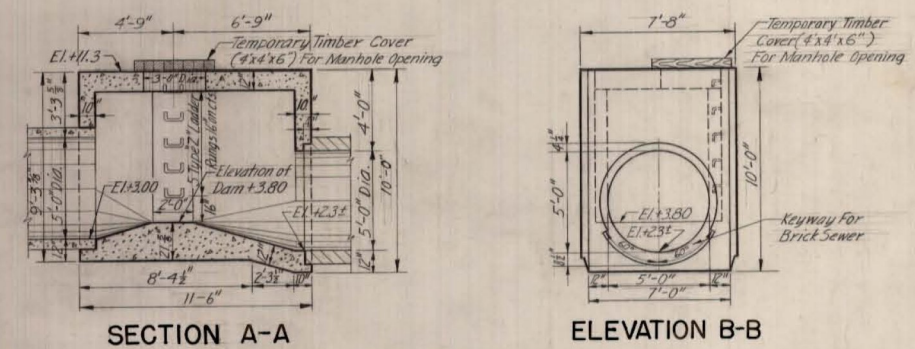
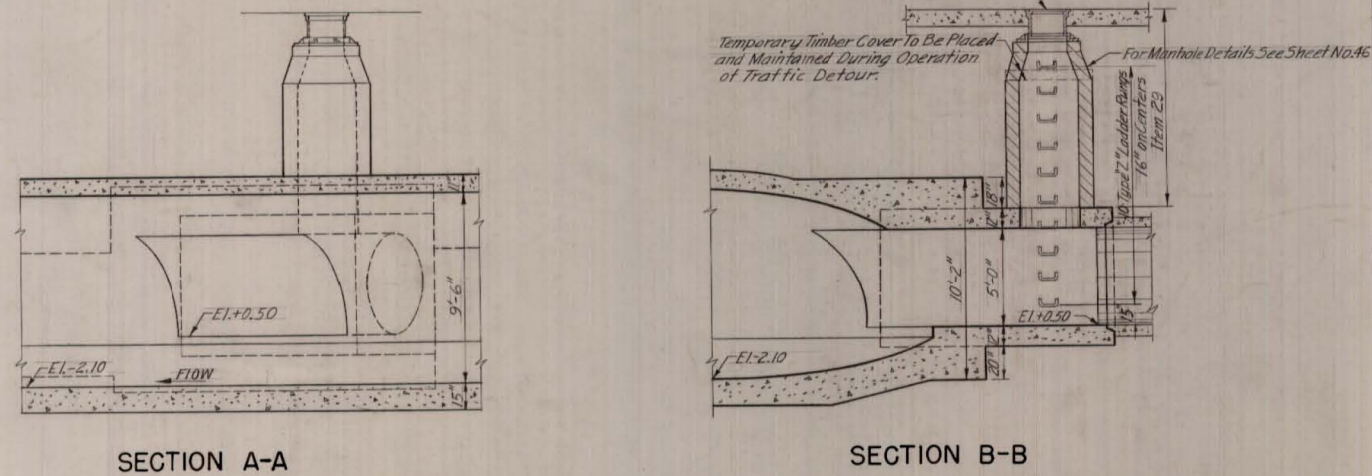
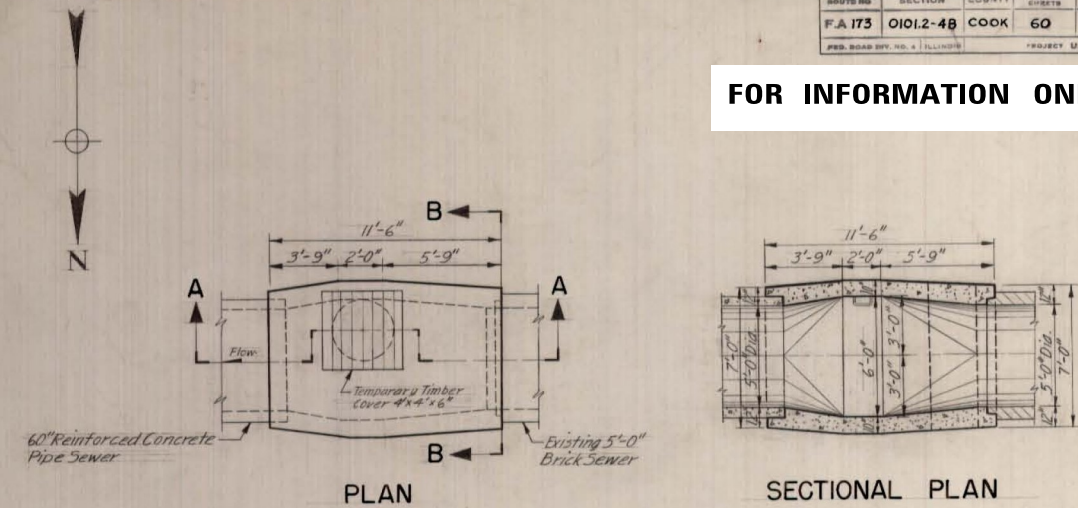
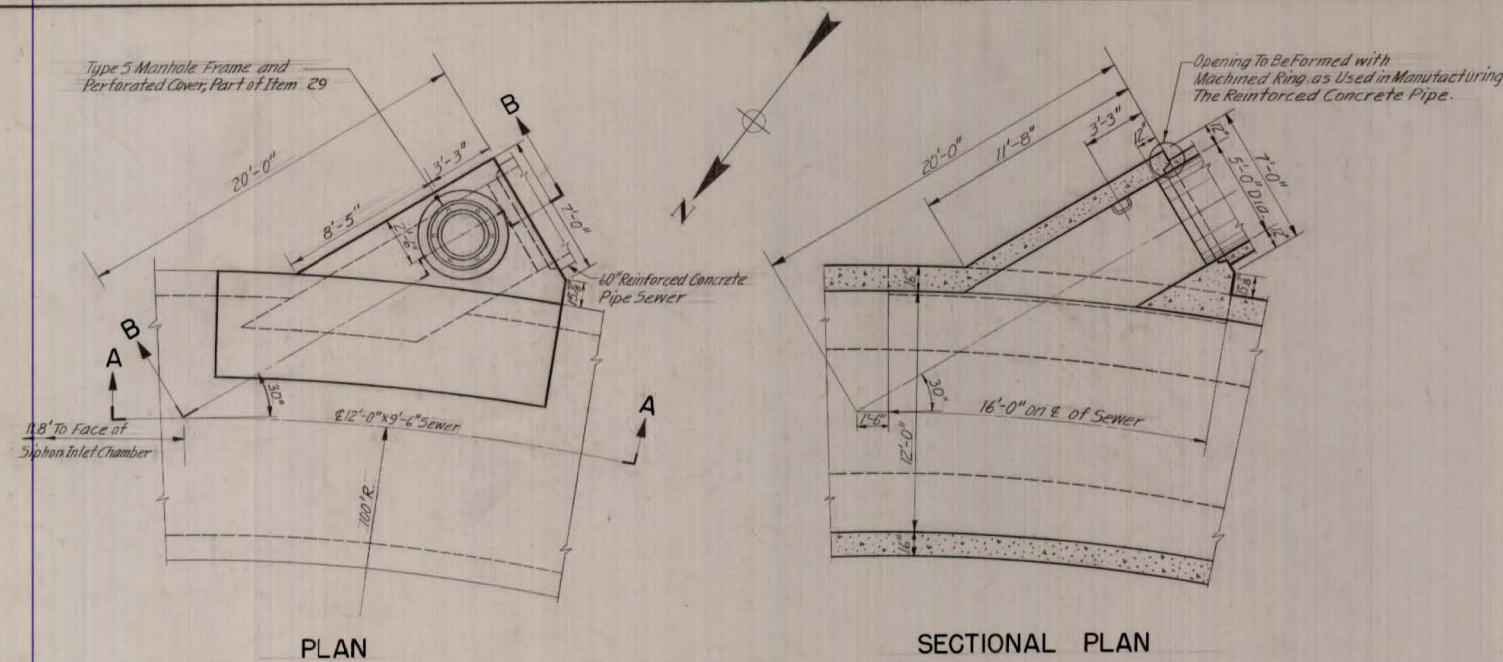
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| STRUCTURE | |
| NOTATIONS | |
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|--------------------------------|----------|-------------------|--------------|-----------|
| SHEET NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| F.A. 173 | 01012-4B | COOK | 60 | 45 |
| FED. ROAD DIST. NO. 4 ILLINOIS | | PROJECT 01215 (S) | | |

FOR INFORMATION ONLY

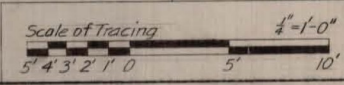


SEWER CONNECTION AT MADISON STREET
Item 48

SEWER JUNCTION AT MONROE STREET
Part of Item 47

DRAWN BY: W.W. WALTER
 REAGED BY: M. WILLIAMS
 CHECKED BY: H.H. CHU
 EXAMINED BY: *Clayton J. Neff*
 Asst. Engineer of Sewer Design

Notes:
 For Location of Structures See Sheet Nos. 35 & 37
 For Reinforcement Steel Details See Sheet Nos. S11 & S12
 For General Notes See Sheet No. 42



| REVISIONS | |
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| DATE | DESCRIPTION |
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CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
 SEWER PLANNING DIVISION
 NORTHWEST ROUTE SUPERHIGHWAY
 MONROE ST. BRIDGE — SECTION 01012-4B
 SEWER CONNECTION AT MADISON STREET
 AND SEWER JUNCTION AT MONROE ST.
 SCALE: 1/4"=1'-0"
 DATE: FEBRUARY, 1954
 CONTRACT SHEET NO. 45

PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - MCC | REVISED - |
| | DRAWN - MCC | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RT | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

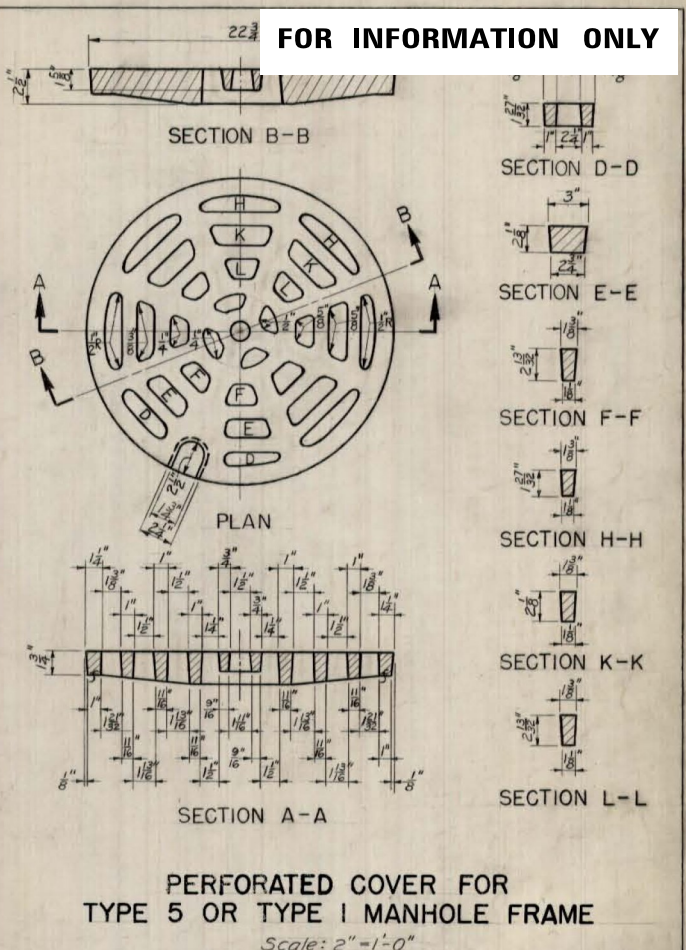
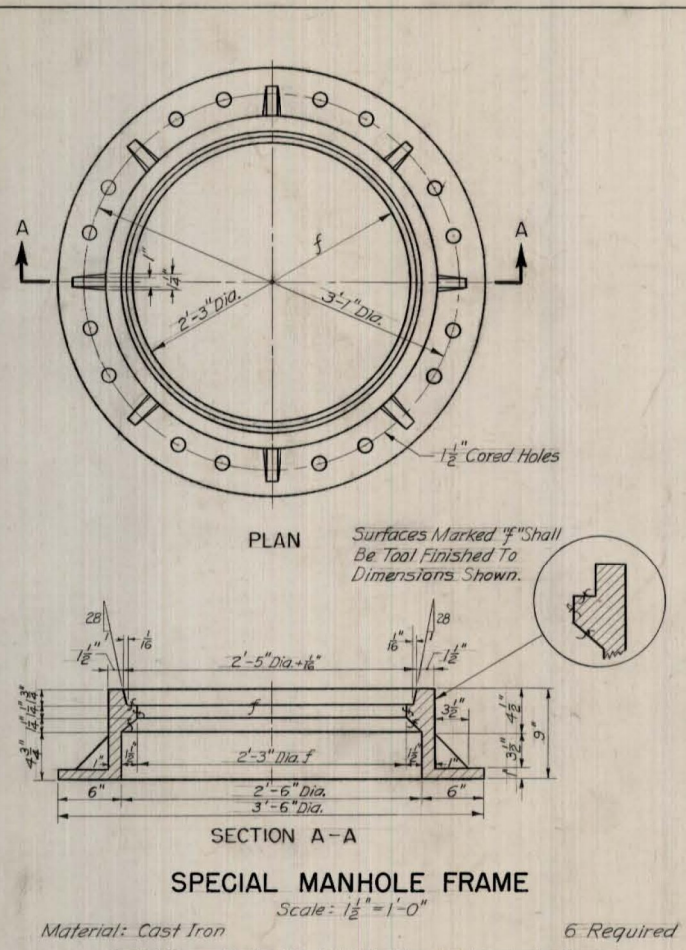
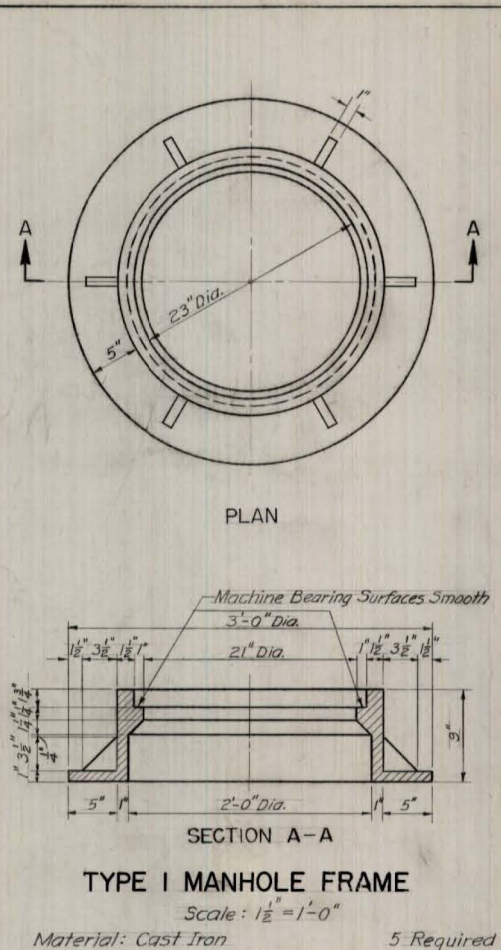
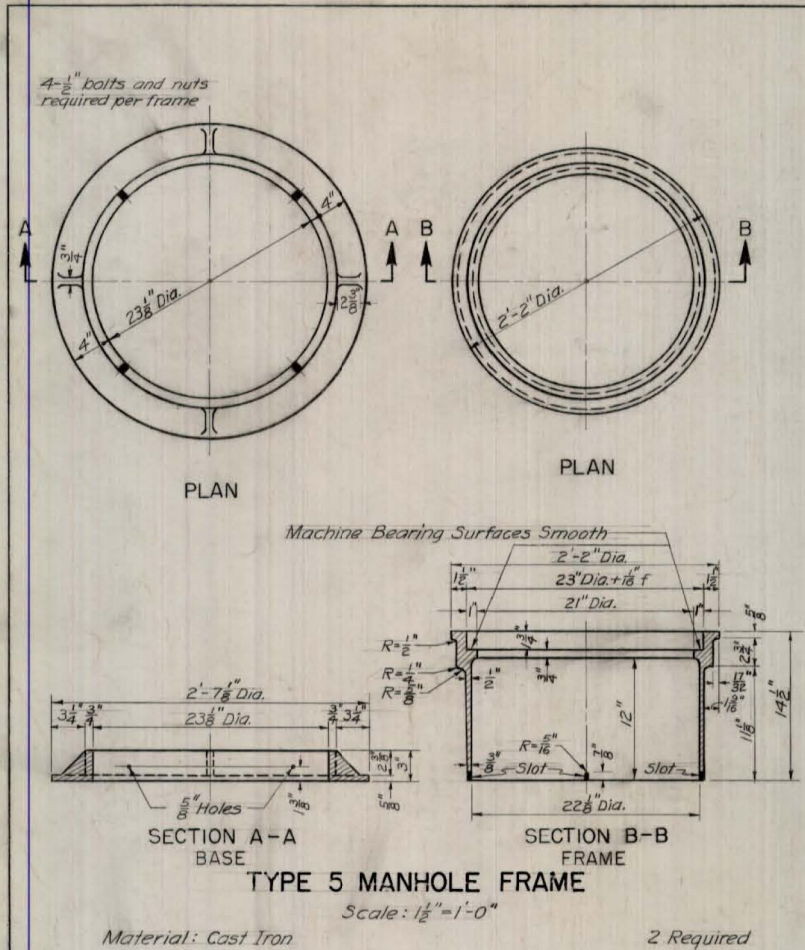
AS BUILT SIPHON
 PLAN AND DETAILS

SCALE: N.T.S. SHEET NO. 11 OF 13 SHEETS STA. TO STA.

| | | | | |
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| F.A.I. 90/94 | 2014-016R&B | COOK | 378 | 106 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

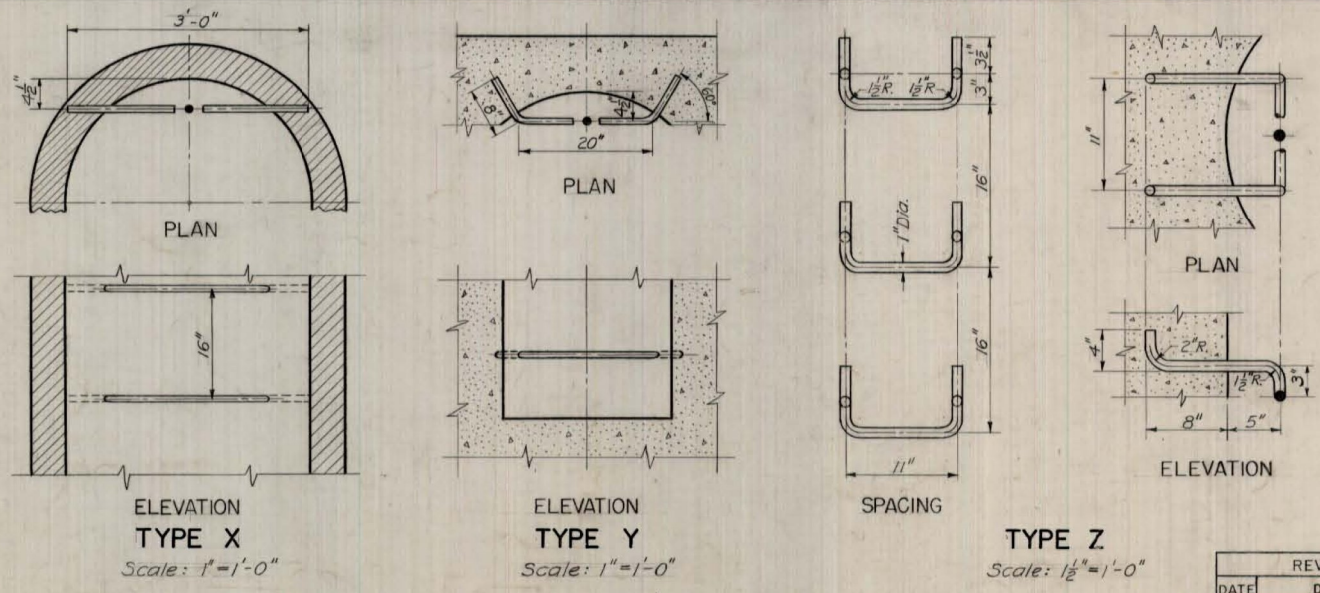
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| STRUCTURE NOTATIONS OK'D | |
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FOR INFORMATION ONLY

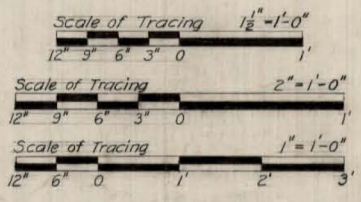
| |
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| SECTION D-D |
| SECTION E-E |
| SECTION F-F |
| SECTION H-H |
| SECTION K-K |
| SECTION L-L |



All Ladder Rungs Shall Be of 1\"/>

DRAWN BY G. G. TANAKA
TRACED BY L. CALVANO
CHECKED BY H. H. CHU
EXAMINED BY Asst. Engineer Of Sewer Design

| REVISIONS | |
|-----------|-------------|
| DATE | DESCRIPTION |
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CITY OF CHICAGO
DEPARTMENT OF PUBLIC WORKS
BUREAU OF ENGINEERING
SEWER PLANNING DIVISION
NORTHWEST ROUTE SUPERHIGHWAY
MONROE ST. BRIDGE - SECTION 0101.2-4B
MANHOLE FRAMES, COVERS AND LADDER RUNG DETAILS
SCALE: AS SHOWN
DATE: FEBRUARY, 1954
CONTRACT SHEET NO. 47

FILE NAME = T:\169988\Civil\680x95 Monroe Street\Plotting\Print Export\0160x95-shr-EXSPHON-13.dgn

PARSONS BRINCKERHOFF

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| USER NAME = Kucharski | DESIGNED - MCC | REVISED - |
| | DRAWN - MCC | REVISED - |
| | CHECKED - RT | REVISED - |
| | DATE - 05/02/2017 | REVISED - |

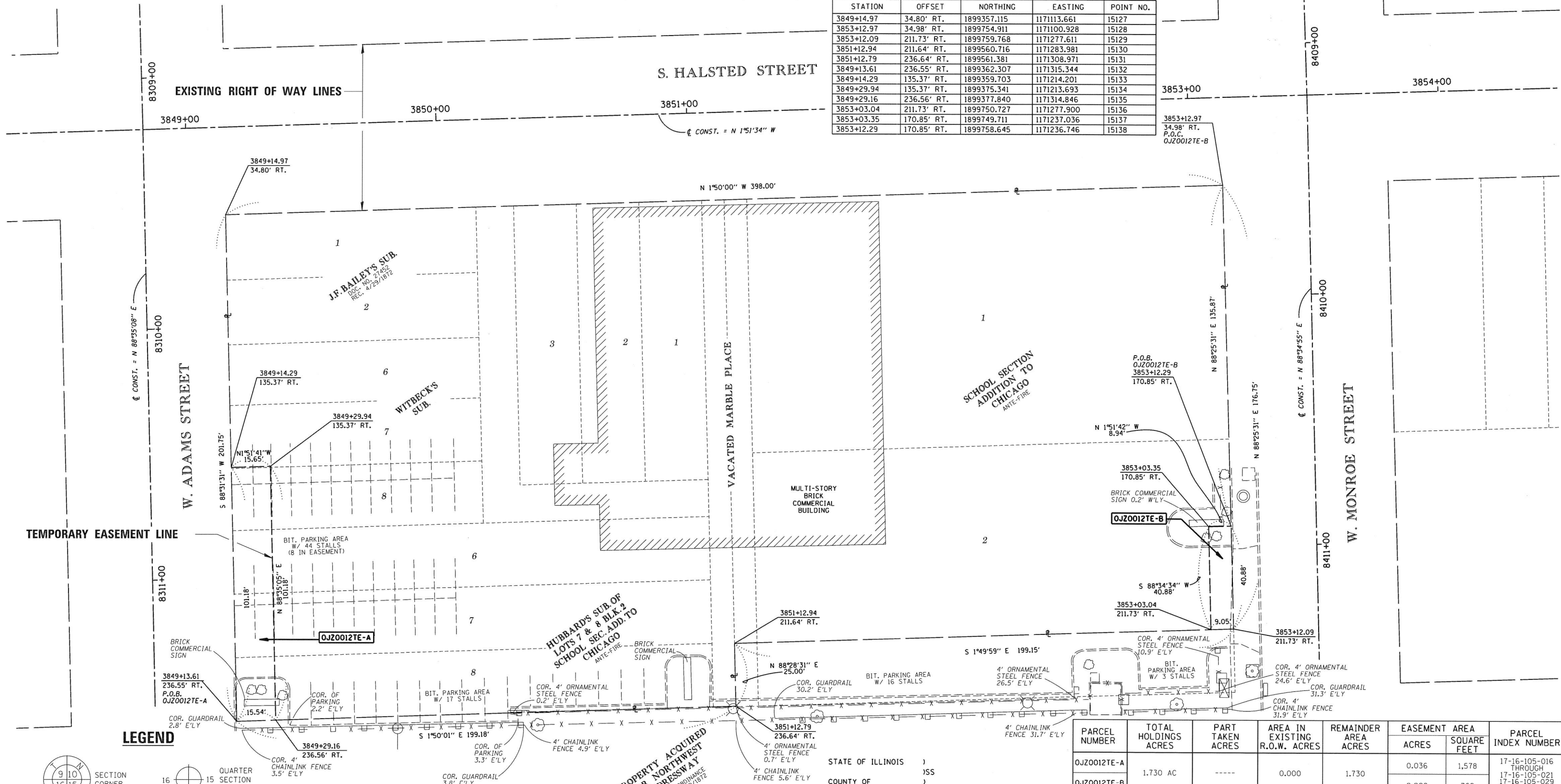
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | |
|-------------------------------------|---------------|---------------------------|------|---------|
| AS BUILT SIPHON PLAN AND DETAILS | SCALE: N.T.S. | SHEET NO. 13 OF 13 SHEETS | STA. | TO STA. |
|-------------------------------------|---------------|---------------------------|------|---------|

| | | | | |
|---------------------------|---------------------|-------------|--------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 108 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

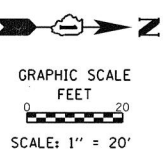
BOUNDARY COORDINATE TABLE

| STATION | OFFSET | NORTHING | EASTING | POINT NO. |
|------------|-------------|-------------|-------------|-----------|
| 3849+14.97 | 34.80' RT. | 1899357.115 | 1171113.661 | 15127 |
| 3853+12.97 | 34.98' RT. | 1899754.911 | 1171100.928 | 15128 |
| 3853+12.09 | 211.73' RT. | 1899759.768 | 1171277.611 | 15129 |
| 3851+12.94 | 211.64' RT. | 1899560.716 | 1171283.981 | 15130 |
| 3851+12.79 | 236.64' RT. | 1899561.381 | 1171308.971 | 15131 |
| 3849+13.61 | 236.55' RT. | 1899362.307 | 1171315.344 | 15132 |
| 3849+14.29 | 135.37' RT. | 1899359.703 | 1171214.201 | 15133 |
| 3849+29.94 | 135.37' RT. | 1899375.341 | 1171213.693 | 15134 |
| 3849+29.16 | 236.56' RT. | 1899377.840 | 1171314.846 | 15135 |
| 3853+03.04 | 211.73' RT. | 1899750.727 | 1171277.900 | 15136 |
| 3853+03.35 | 170.85' RT. | 1899749.711 | 1171237.036 | 15137 |
| 3853+12.29 | 170.85' RT. | 1899758.645 | 1171236.746 | 15138 |



LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION / QUARTER SECTION LINE
- PLATTED LOT LINES
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- EXISTING CENTERLINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- EXISTING EASEMENT
- PROPOSED EASEMENT
- EXISTING ACCESS CONTROL LINE
- PROPOSED ACCESS CONTROL LINE
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORDED DIMENSION
- EXISTING BUILDING



- IRON PIPE OR ROD FOUND
- CUT CROSS FOUND OR SET
- MAG NAIL SET
- 5 / 8" REBAR SET
- THESE STAKES REFERENCE FOUND OR SET MONUMENTATION, SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO TIE FOUND IRON STAKE, IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION, BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY, SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS, BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T. STANDARD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET

STATE OF ILLINOIS)
 COUNTY OF)

THIS IS TO CERTIFY THAT I, COVENTINE FIDAS, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, (WE, AMERICAN SURVEYING AND ENGINEERING, AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-003192,) HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 16, TOWNSHIP 39 NORTH, RANGE 14 EAST OF THE THIRD PRINCIPAL MERIDIAN, COOK COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED, MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

DATED AT CHICAGO, ILLINOIS THIS 13TH DAY OF SEPTEMBER, 2016 A.D.

ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 2159
 LICENSE EXPIRATION DATE: 11/30/2016

THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.

REVISION DATE: 09/13/2016 REVISION MADE BY:

| PARCEL NUMBER | TOTAL HOLDINGS ACRES | PART TAKEN ACRES | AREA IN EXISTING R.O.W. ACRES | REMAINDER AREA ACRES | EASEMENT AREA ACRES | EASEMENT AREA SQUARE FEET | PARCEL INDEX NUMBER |
|---------------|----------------------|------------------|-------------------------------|----------------------|---------------------|---------------------------|-------------------------------------|
| OJZ0012TE-A | 1.730 AC | ----- | 0.000 | 1.730 | 0.036 | 1,578 | 17-16-105-016 THROUGH 17-16-105-021 |
| OJZ0012TE-B | | | | | 0.008 | 368 | 17-16-105-029 17-16-105-036 |

(CONSULTANT INFORMATION)



PLAT OF HIGHWAYS
 STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 (ROAD NAME)

IDOT USE ONLY

RECEIVED
 SEP 13 2016
 PLATS & LEGALS

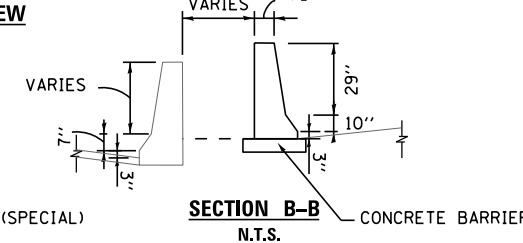
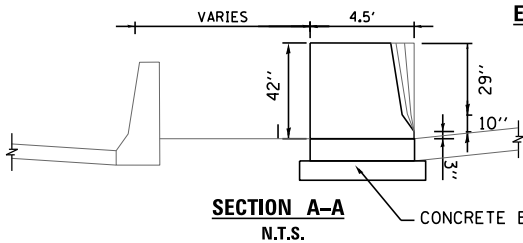
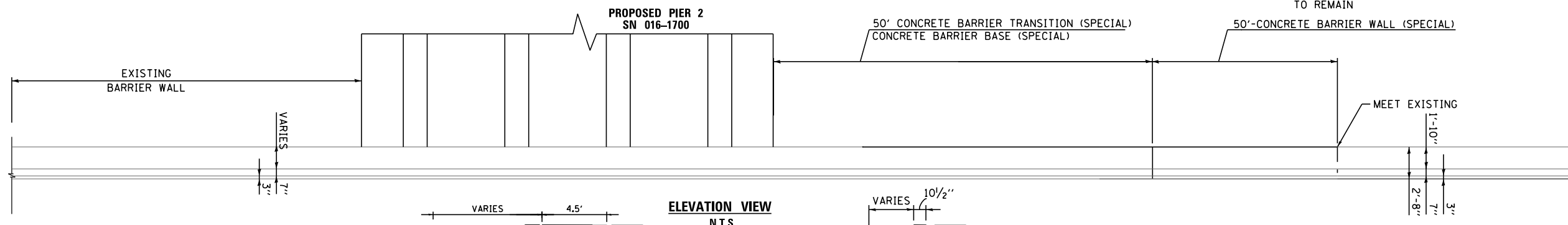
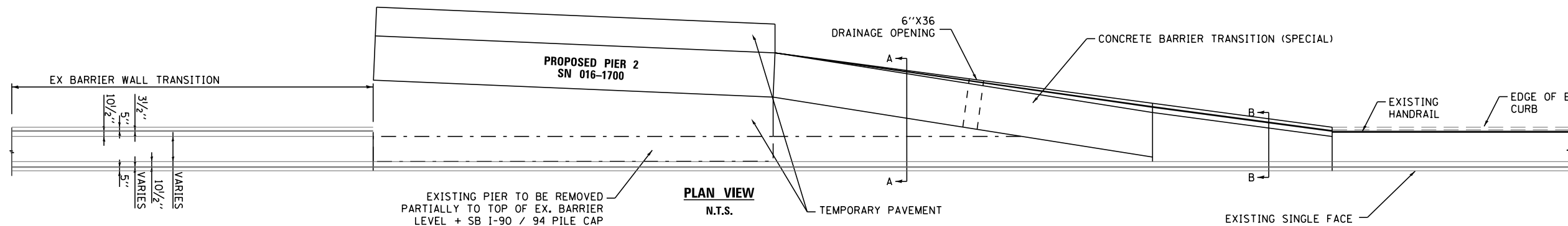
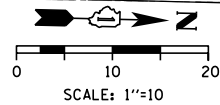
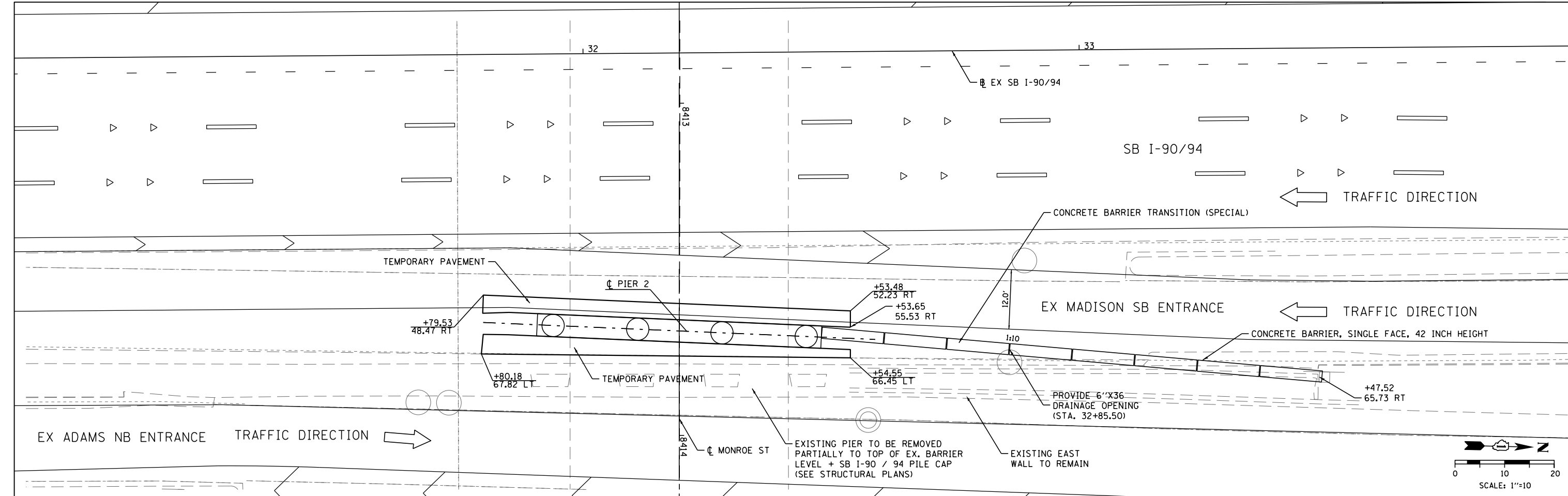
LIMITS: SECTION: CIRCLE INTERCHANGE TO STA. 3849+13.61 TO STA. 3853+12.29
 COUNTY: COOK JOB NO.: R-90-031-13
 SCALE: 1" = 20' SHEET 109 OF 378 SHEETS

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAMBURG, ILLINOIS 60196

BEARINGS ARE REFERENCED TO THE ILLINOIS STATE PLANE COORDINATE SYSTEM, NAD83 (2007 ADJUSTMENT), EAST ZONE.

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| PLAN | SURVEYED | DATE |
| | PLOTTED | BY |
| | NOTE BOOK | |
| | NO. | |
| | ALIGNED | |
| | CHECKED | |
| | FILE NAME | |

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| PROFILE | SURVEYED | DATE |
| | GRADES CHECKED | BY |
| | STRUCTURE | |
| | NOTATIONS | |
| | CHFD | |



SEE SHEET 112 FOR BARRIER REINFORCEMENT

PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = donahuek1 | DESIGNED - MMA | REVISED - |
| PLOT SCALE = 1"=50' | DRAWN - MMA | REVISED - |
| PLOT DATE = 8/31/2017 | CHECKED - RT | REVISED - |
| | DATE - 09/01/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY DETAIL

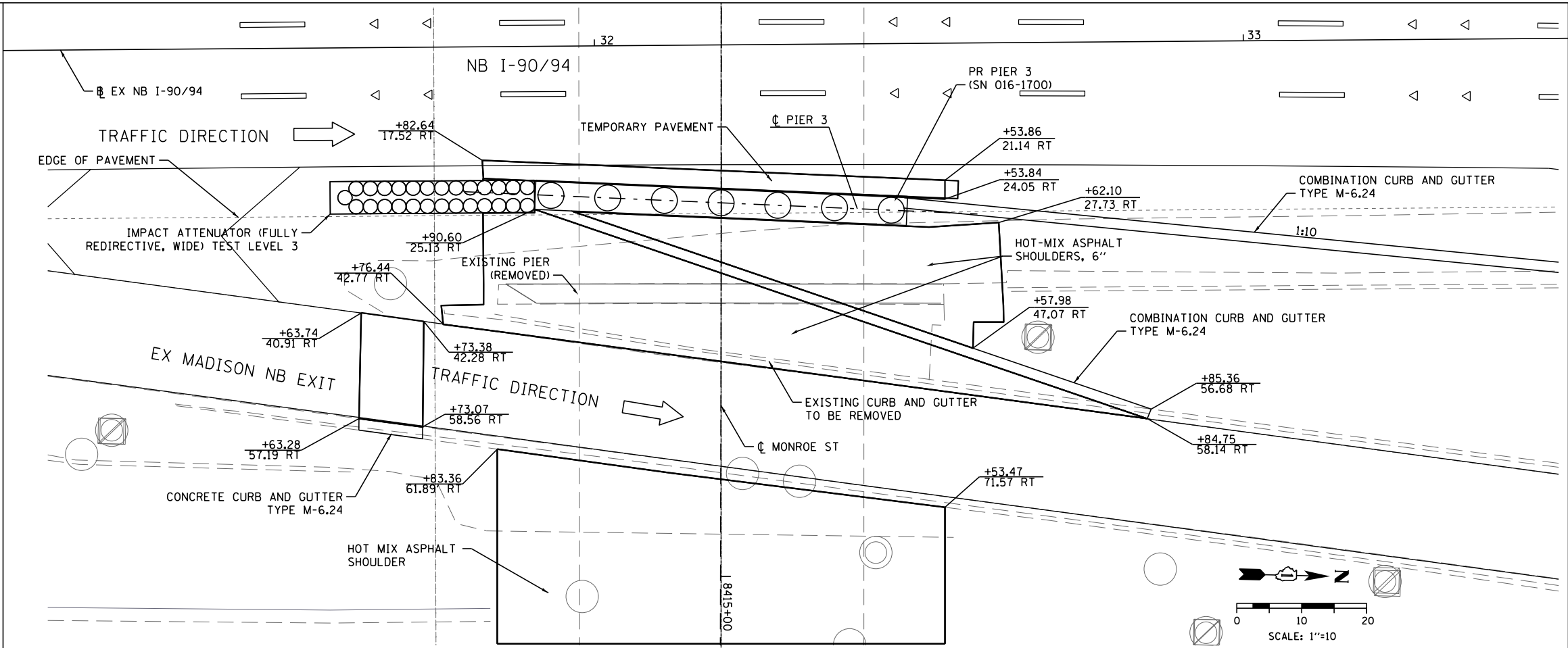
SCALE: 1"=10' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

| | | | | |
|---------------------------|---------------------|-------------|--------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 111 |
| | | | CONTRACT NO. 60X95 | |
| ILLINOIS FED. AID PROJECT | | | | |

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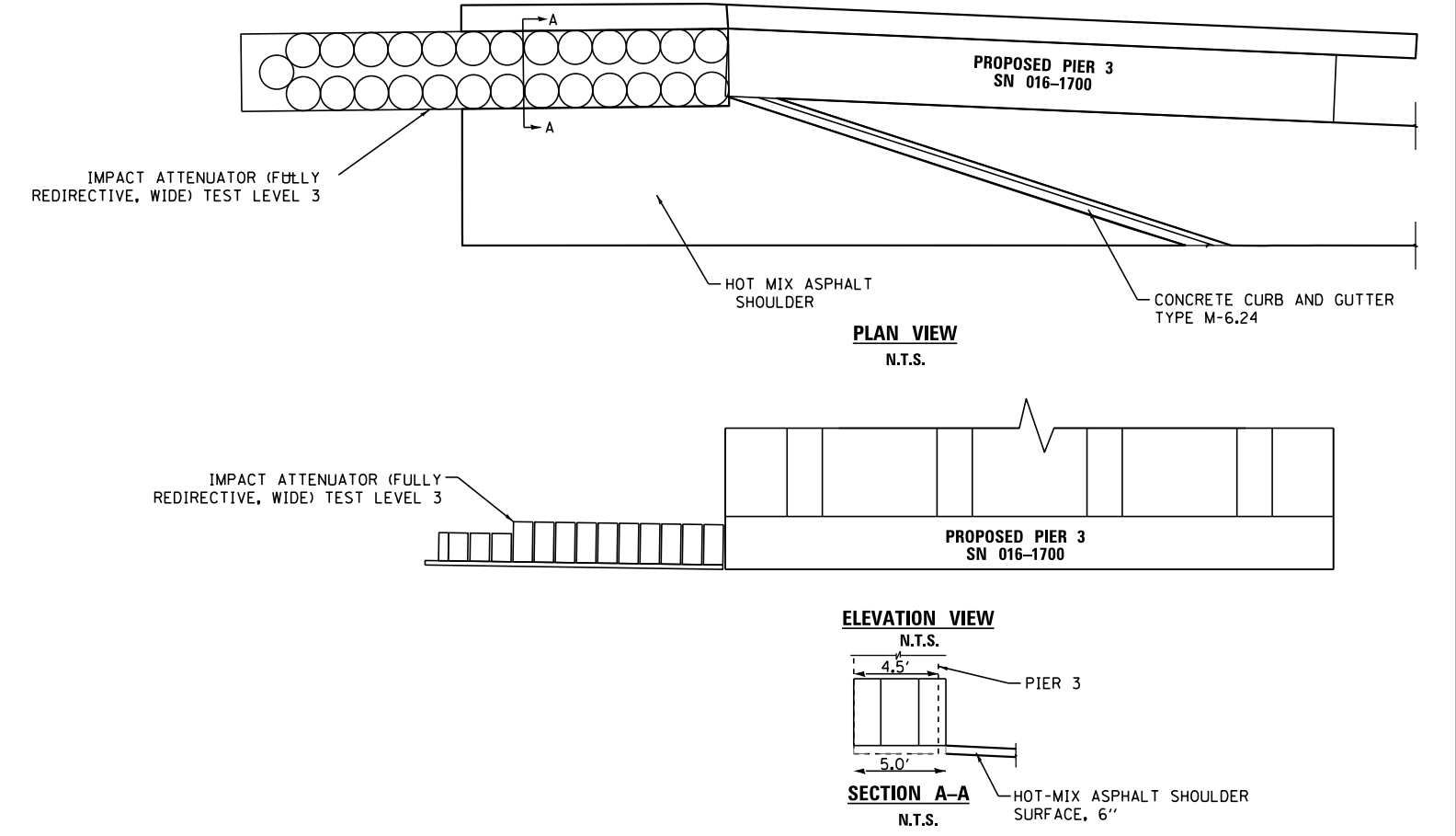
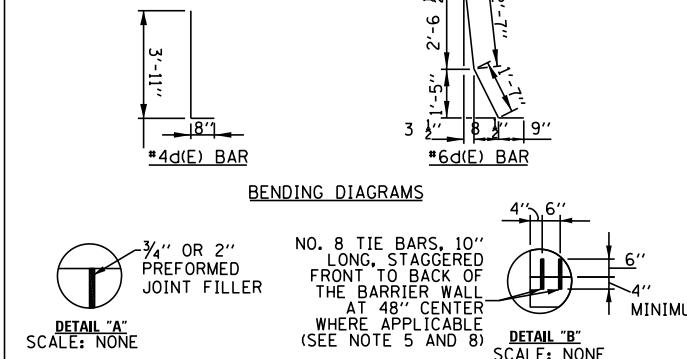
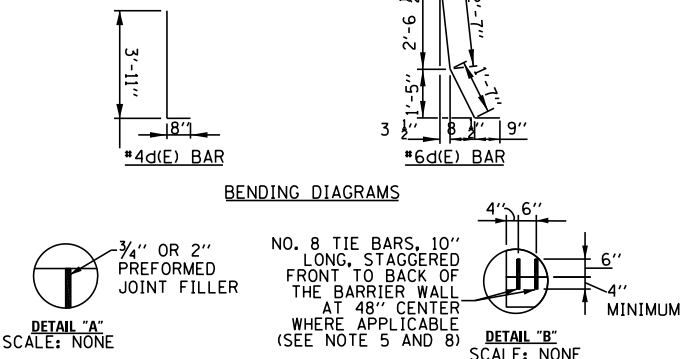
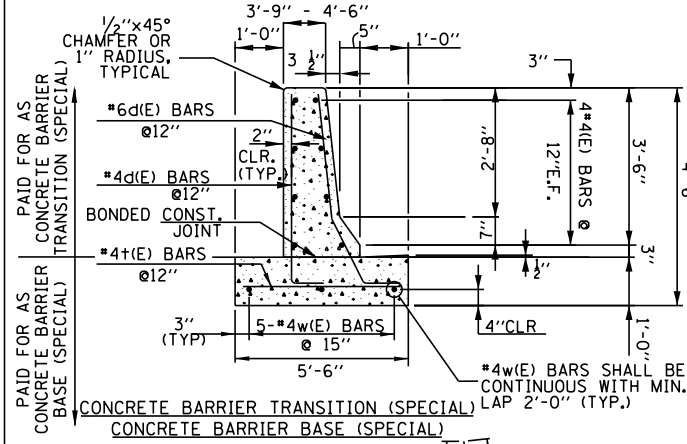
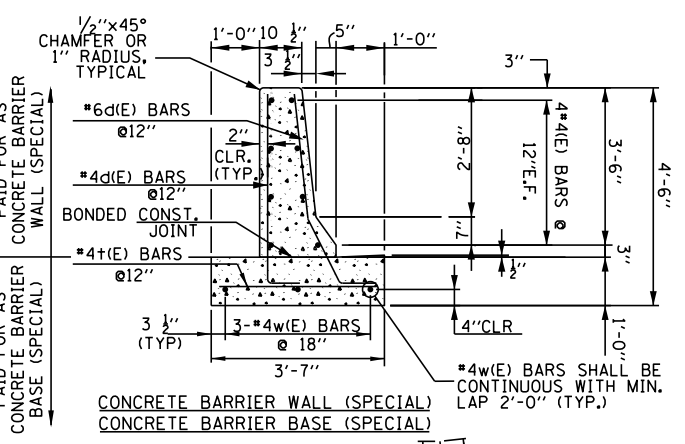
NOTES:

- TOP SHOULDER EDGE OF BARRIER BASE GUTTER SHALL MATCH THE TOP OF SHOULDER ELEVATION.
- 1" DEEP CONTRACTION JOINTS SHALL BE CONSTRUCTED IN BOTH THE REINFORCED CONCRETE BARRIER WALL AND BASE.
- THE FORMING OF CONTRACTION JOINTS SHALL BE DONE WITH AN APPROVED FINISHING TOOL OR BY SAWING AT THE DISCRETION OF THE ENGINEER SUBJECT TO THE SATISFACTORY CONTROL OF CRACKING.
- REINFORCING BARS DESIGNATED "(E)" SHALL BE EPOXY COATED.
- REINFORCEMENT BENDING DETAILS SHALL BE IN ACCORDANCE WITH THE "MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES", ACI 315, LATEST EDITION.
- REINFORCEMENT BAR BENDING DIMENSIONS ARE OUT TO OUT.
- BARRIER SHALL BE USED WITH ALL NEW CONSTRUCTION, OR RECONSTRUCTION OF EXISTING BARRIERS.
- EXPANSION JOINTS SHOWN ON THIS DRAWING SHALL BE PREFORMED JOINT MATERIAL (BITUMINOUS TYPE) FILLER AND SHALL MEET AASHTO DESIGNATION M-33.
- ALL WORK AND MATERIALS DETAILED HEREIN SHALL BE INCLUDED IN THE COST OF THE VARIOUS CONCRETE BARRIER PAY ITEMS UNLESS OTHERWISE NOTED.
- PREFORMED JOINT FILLER SHALL BE INCLUDED IN THE COST OF OF CONCRETE BARRIER WALL (SPECIAL) OR CONCRETE BARRIER TRANSITION (SPECIAL).
- JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND IN ACCORDANCE WITH ARTICLE 637.08 OF THE STANDARD SPECIFICATIONS
- TWO VERTICAL EPOXY COATED, NO. 8 TIE BARS, 10" LONG, SHALL BE PLACED STAGGERED FRONT TO BACK OF THE BARRIER WALL AT 48" CENTERS ALONG THE CONCRETE BARRIER TRANSITION. TIE BARS SHALL BE INCLUDED IN THE COST OF CONCRETE BARRIER TRANSITION (SPECIAL).
- HORIZONTAL TIE BARS SHALL BE NO. 6 EPOXY COATED, 24" LONG, 24" C-C AND SHALL BE INCLUDED IN THE COST OF THE CONCRETE BARRIER BASE (SPECIAL). HORIZONTAL TIE BARS ARE REQUIRED WHEN PAVEMENT IS PCC.
- EXPANSION JOINTS SHALL BE CONSTRUCTED IN BARRIER WALL AT MAXIMUM JOINT SPACING OF 90 FEET.
- VERTICAL TIE BARS ARE REQUIRED WHEN CONCRETE BARRIER AND CONCRETE BARRIER BASE ARE NOT POURED MONOLITHICALLY.



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| BY | |
| PLAN | |
| NO. | |
| DATE | |
| BY | |
| PROFILE | |
| NO. | |

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|---------------------------------------|--|
| DATE | |
| BY | |
| PROFILE | |
| NO. | |
| DATE | |
| BY | |
| CONCRETE BARRIER WALL (SPECIAL) | |
| CONCRETE BARRIER TRANSITION (SPECIAL) | |
| CONCRETE BARRIER BASE (SPECIAL) | |



PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = donahuek1 | DESIGNED - MMA | REVISED - |
| PLOT SCALE = 1"=50' | DRAWN - MMA | REVISED - |
| PLOT DATE = 8/31/2017 | CHECKED - RT | REVISED - |
| | DATE - 09/01/2017 | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | | |
|---------------|-------------------------|--------------------------|
| SCALE: 1"=10' | SHEET NO. 3 OF 3 SHEETS | STA. 30+50 TO STA. 33+50 |
|---------------|-------------------------|--------------------------|

| | | | | |
|--------------------|---------------------|-------------|---------------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 112 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

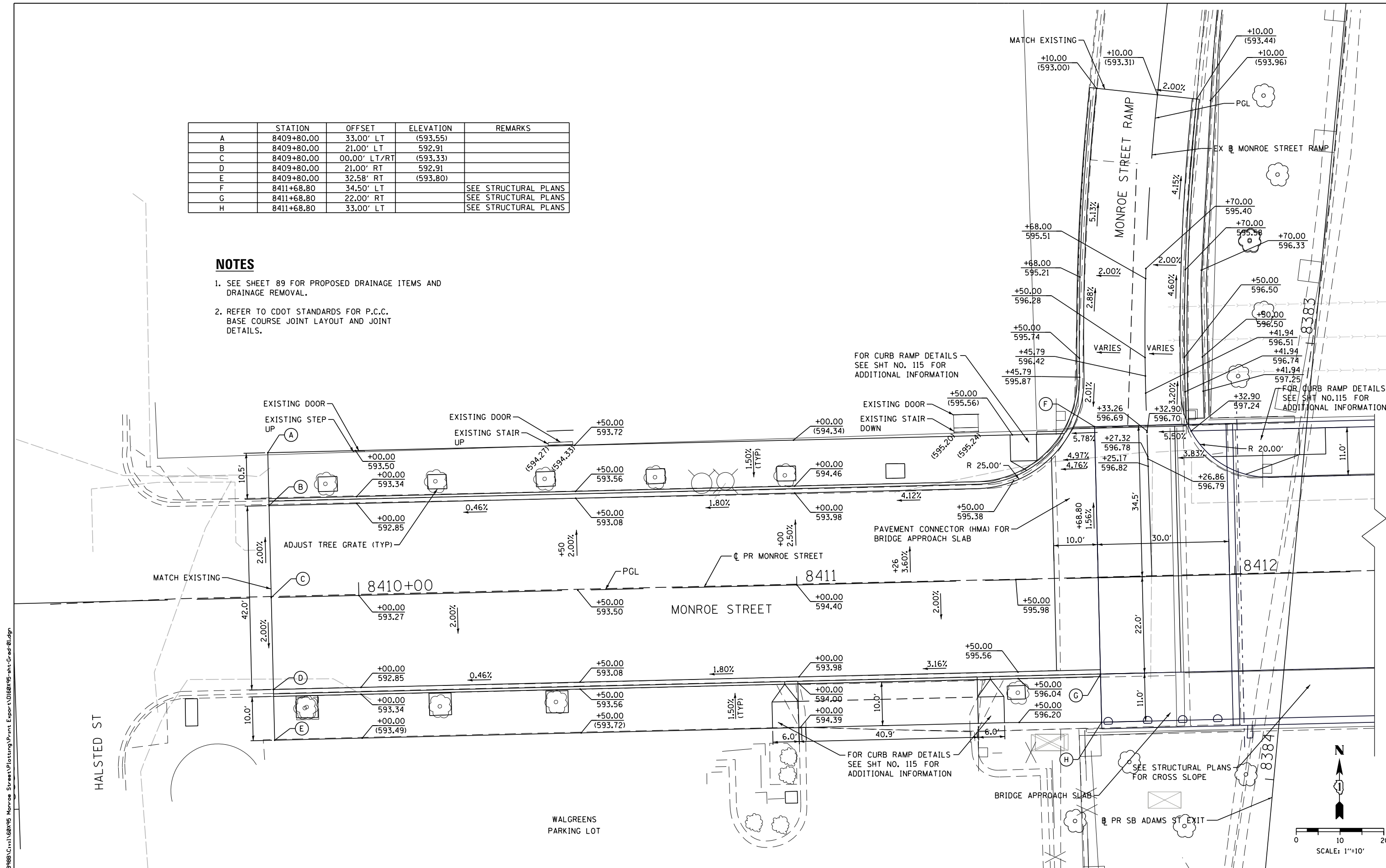
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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | ALIGNED | |
| | CHECKED | |
| | FILED | |
| NOTE BOOK NO. | BY | DATE |

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|---------------|--------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| NOTE BOOK NO. | BY | DATE |

| | STATION | OFFSET | ELEVATION | REMARKS |
|---|------------|--------------|-----------|----------------------|
| A | 8409+80.00 | 33.00' LT | (593.55) | |
| B | 8409+80.00 | 21.00' LT | 592.91 | |
| C | 8409+80.00 | 00.00' LT/RT | (593.33) | |
| D | 8409+80.00 | 21.00' RT | 592.91 | |
| E | 8409+80.00 | 32.58' RT | (593.80) | |
| F | 8411+68.80 | 34.50' LT | | SEE STRUCTURAL PLANS |
| G | 8411+68.80 | 22.00' RT | | SEE STRUCTURAL PLANS |
| H | 8411+68.80 | 33.00' LT | | SEE STRUCTURAL PLANS |

NOTES

- SEE SHEET 89 FOR PROPOSED DRAINAGE ITEMS AND DRAINAGE REMOVAL.
- REFER TO CDOT STANDARDS FOR P.C.C. BASE COURSE JOINT LAYOUT AND JOINT DETAILS.



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PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - JH | REVISED - |
| DRAWN - JH | REVISOR - | |
| PLOT SCALE = 1"=50' | CHECKED - RT | REVISOR - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISOR - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY GRADING PLAN DETAILS
MONROE STREET (WEST)**

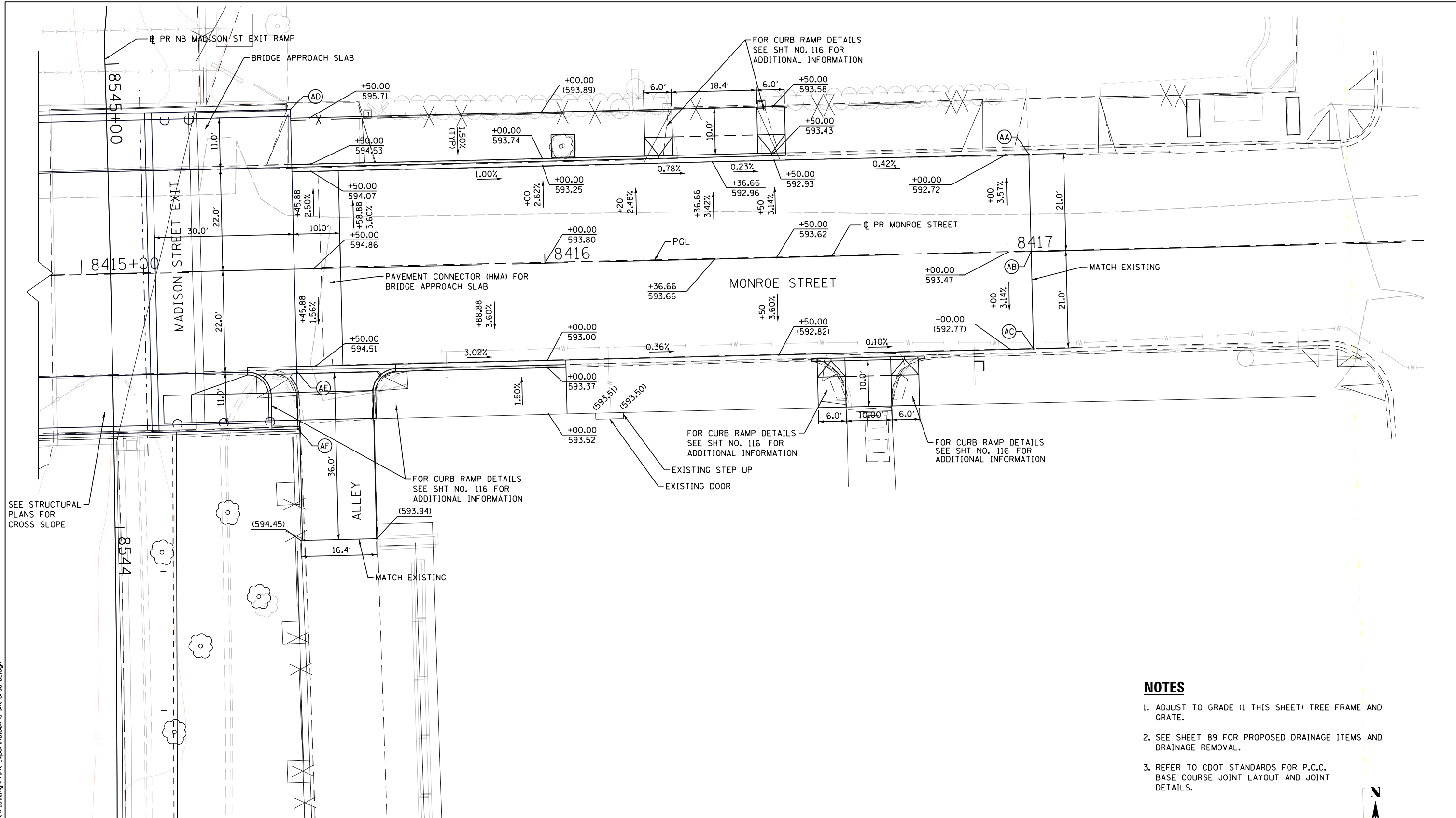
SCALE: 1" = 10' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

| | | | | |
|---------------------------|---------------------|-------------|--------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 113 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

| | |
|------|-----------|
| DATE | |
| BY | |
| PLAN | SURVEYED |
| | PLOTTED |
| | NOTE BOOK |
| | NO. |

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| DATE | |
| BY | |
| PROFILE | SURVEYED |
| | PLOTTED |
| | NOTE BOOK |
| | NO. |

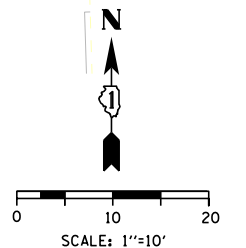
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NOTES

1. ADJUST TO GRADE (1 THIS SHEET) TREE FRAME AND GRATE.
2. SEE SHEET 89 FOR PROPOSED DRAINAGE ITEMS AND DRAINAGE REMOVAL.
3. REFER TO CDOT STANDARDS FOR P.C.C. BASE COURSE JOINT LAYOUT AND JOINT DETAILS.

| | STATION | OFFSET | ELEVATION | REMARKS |
|----|------------|-------------|-----------|----------------------|
| AA | 8417+05.00 | 21.00' LT | (592.72) | |
| AB | 8417+05.00 | 0.00' LT/RT | (593.45) | |
| AC | 8417+05.00 | 21.00' RT | (592.77) | |
| AD | 8417+05.00 | 32.58' RT | | SEE STRUCTURAL PLANS |
| AE | 8415+45.88 | 34.50' LT | | SEE STRUCTURAL PLANS |
| AF | 8415+45.88 | 34.50' RT | | SEE STRUCTURAL PLANS |



PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - JH | REVISED - |
| | DRAWN - JH | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RT | REVISED - |
| PLOT DATE = 4/28/2017 | DATE - 05/02/2017 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY GRADING PLAN DETAILS
MONROE STREET (EAST)**

SCALE: 1" = 10' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

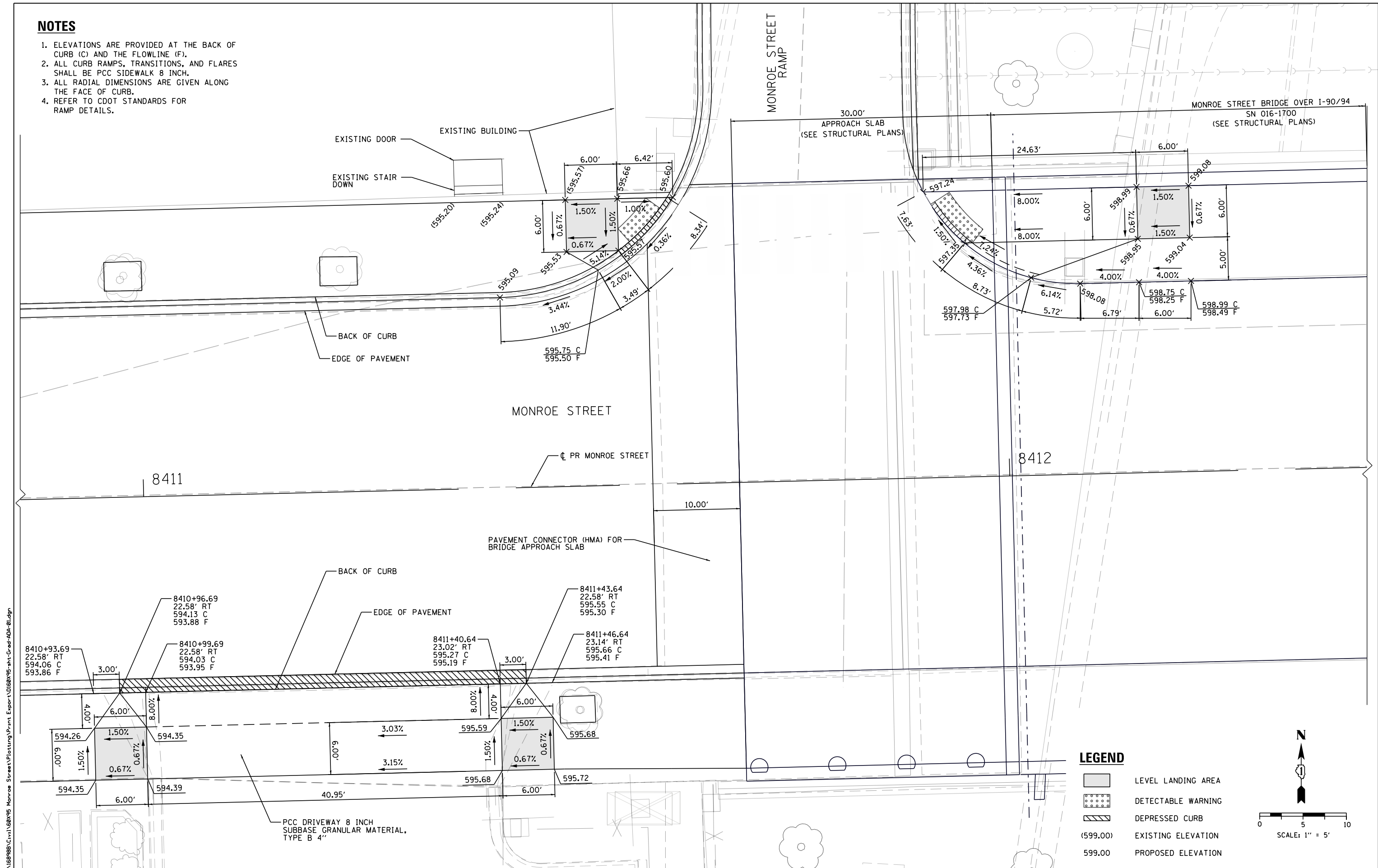
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|---------------------------|---------------------|-------------|------------------|--------------------|
| F.A.I. RE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 114 |
| | | | | CONTRACT NO. 60X95 |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES

- ELEVATIONS ARE PROVIDED AT THE BACK OF CURB (C) AND THE FLOWLINE (F).
- ALL CURB RAMPS, TRANSITIONS, AND FLARES SHALL BE PCC SIDEWALK 8 INCH.
- ALL RADIAL DIMENSIONS ARE GIVEN ALONG THE FACE OF CURB.
- REFER TO CDOT STANDARDS FOR RAMP DETAILS.

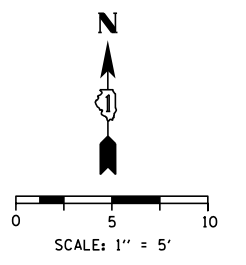
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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | ALIGNED | |
| | STRUCTURE NOTATIONS OK'D | |
| | FILE NAME | |
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| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | ALIGNED | |
| | STRUCTURE NOTATIONS OK'D | |
| | FILE NAME | |
| | NO. | |



LEGEND

- LEVEL LANDING AREA
- DETECTABLE WARNING
- DEPRESSED CURB
- (599.00) EXISTING ELEVATION
- 599.00 PROPOSED ELEVATION



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| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - JH | REVISED - |
| PLOT SCALE = 1"=50' | DRAWN - JH | REVISED - |
| PLOT DATE = 4/18/2017 | CHECKED - RT | REVISED - |
| | DATE - 05/02/2017 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CURB RAMP DETAILS
MONROE STREET (WEST)**

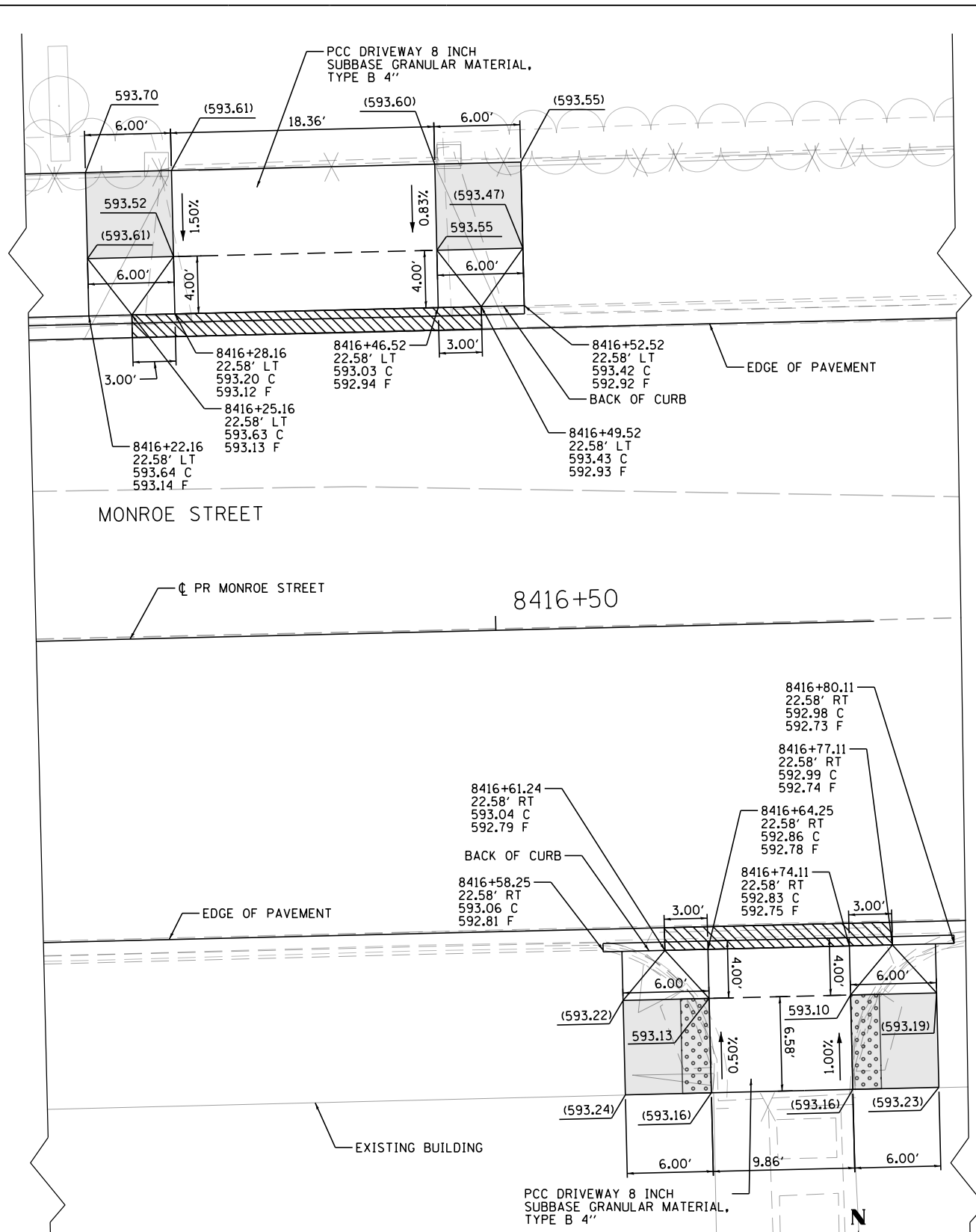
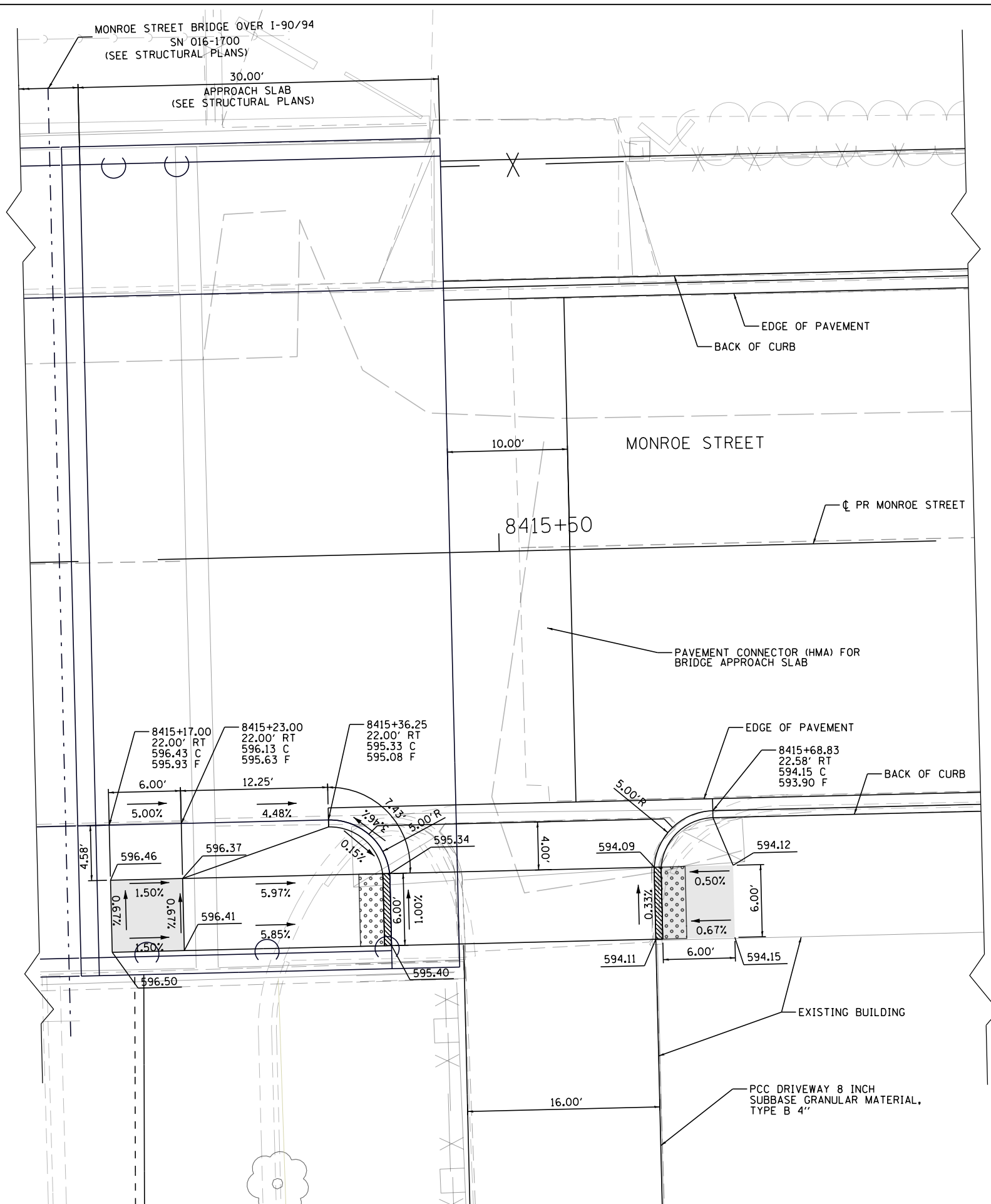
SCALE: 1" = 10' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

| | | | | |
|--------------------|---------------------|-------------|---------------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 115 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

| | | |
|------|----------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | ALIGNED | |
| | FILED | |
| | NO. _____ | |

| | | |
|---------|--------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| | NO. _____ | |

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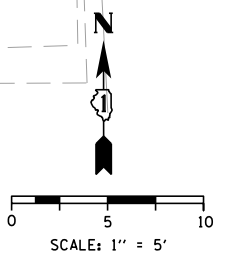


NOTES

- ELEVATIONS ARE PROVIDED AT THE BACK OF CURB (C) AND THE FLOWLINE (F).
- ALL CURB RAMP, TRANSITIONS, AND FLARES SHALL BE PCC SIDEWALK 8 INCH.
- ALL RADIAL DIMENSIONS ARE GIVEN ALONG THE FACE OF CURB.

LEGEND

- LEVEL LANDING AREA
- DETECTABLE WARNING
- DEPRESSED CURB
- (599.00) EXISTING ELEVATION
- 599.00 PROPOSED ELEVATION



PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - JH | REVISED - |
| | DRAWN - JH | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RT | REVISED - |
| PLOT DATE = 4/28/2017 | DATE = 05/02/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB RAMP DETAILS
MONROE STREET (EAST)

SCALE: 1" = 10' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

| | | | | |
|---------------------------|---------------------|-------------|------------------|--------------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 116 |
| ILLINOIS FED. AID PROJECT | | | | CONTRACT NO. 60X95 |

PAVEMENT MARKING SCHEDULE

| | | | |
|------|-----------------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |

| | | | |
|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NOTE BOOK NO. | | |

| LOCATION | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | | POLYUREA PAVEMENT MARKING TYPE 1 - LETTERS AND SYMBOLS | | POLYUREA PAVEMENT MARKING TYPE 1 - LINE 4" | | POLYUREA PAVEMENT MARKING TYPE 1 - LINE 24" | | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | | MODIFIED URETHANE PAVEMENT MARKING - LINE 5", 5' | | MODIFIED URETHANE PAVEMENT MARKING - LINE 6", 6' | | MODIFIED URETHANE PAVEMENT MARKING - LINE 8", 8' | | MODIFIED URETHANE PAVEMENT MARKING - LINE 12", 12' | | MODIFIED URETHANE PAVEMENT MARKING - LINE 24", 24' | | MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS | | RAISED REFLECTIVE PAVEMENT MARKER | | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | |
|-----------------------------|--|------|---|------|--|------|--|------|---|------|--|------|--|------|--|------|--|------|--|--|--|--|--|--|-----------------------------------|--|---|--|
| | FOOT | FOOT | SO FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | FOOT | SO FT | EACH | EACH | | | | | | | | | |
| MONROE STREET BRIDGE | 1035 | 67 | 18 | 943 | 54 | | | | | | | | | | | | | | | | | | | | | | | |
| SB I-90/94 (INCLUDES RAMPS) | | | | | | 2063 | 811 | 40 | 4853 | 388 | 23 | 182 | 156 | 234 | | | | | | | | | | | | | | |
| NB I-90/94 (INCLUDES RAMPS) | | | | | | 2840 | 1331 | | 2798 | 224 | | | 164 | 165 | | | | | | | | | | | | | | |
| TOTAL | 1035 | 67 | 18 | 943 | 54 | 4903 | 2142 | 40 | 7651 | 612 | 23 | 182 | 320 | 399 | | | | | | | | | | | | | | |

NOTE: SEE STAGING SCHEDULE FOR PAVEMENT MARKING REMOVAL - WATER BLASTING.

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| | | |
|-----------------------|-------------------|-----------|
| USER NAME = donahuekl | DESIGNED - MMA | REVISED - |
| | DRAWN - MMA | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RT | REVISED - |
| PLOT DATE = 7/13/2017 | DATE - 07/14/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: SHEET NO. OF SHEETS STA. TO STA.

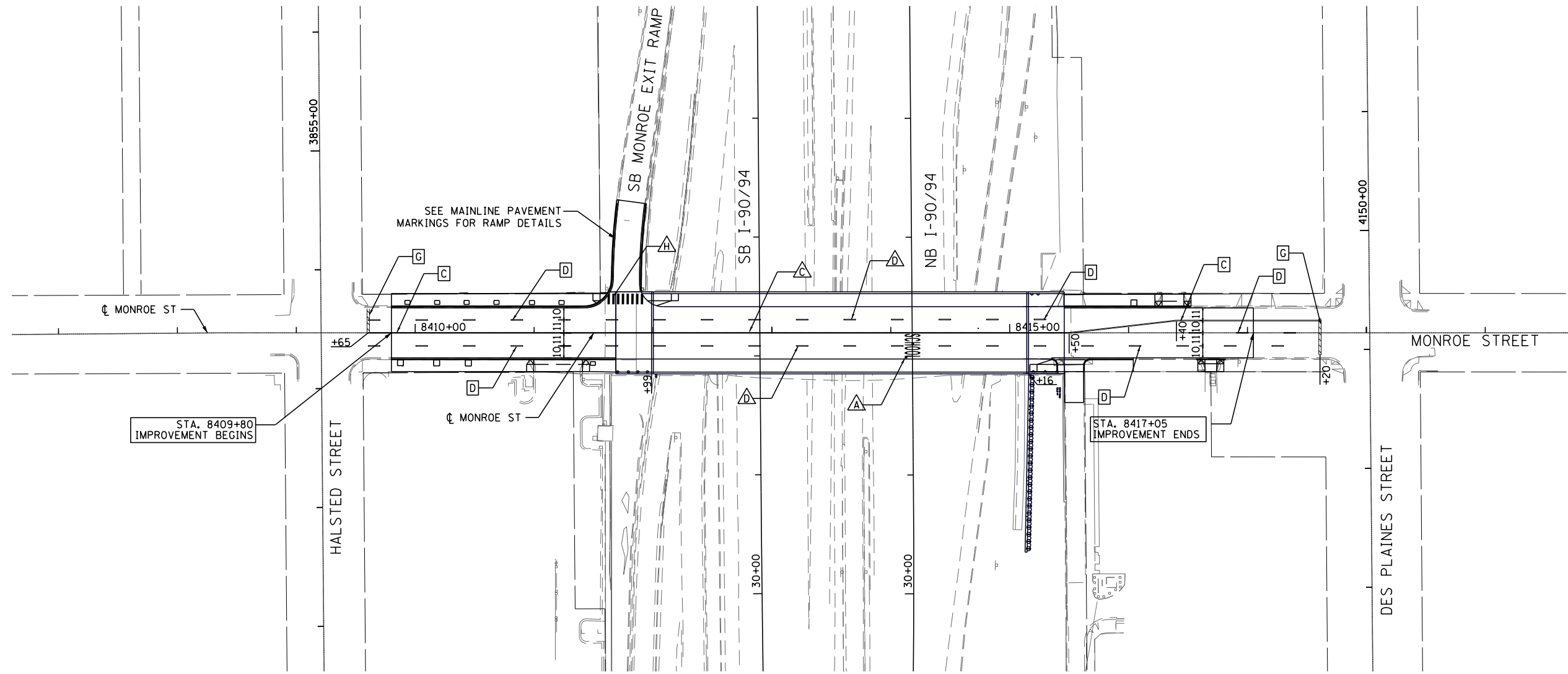
PAVEMENT MARKING SCHEDULE

| | | | | |
|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 117 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

| | | | |
|------|-----------------------------|----|------|
| PLAN | SUBMITTED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | PAID FILE NAME | | |
| | NO. | | |

| | | | |
|---------|-----------------------------|----|------|
| PROFILE | SUBMITTED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | ALIGNMENT CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NO. | | |

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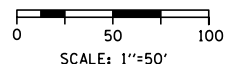


PAVEMENT MARKING NOTES

- ALL FINAL PAVEMENT MARKING MATERIALS PLACED ON PCC PAVEMENT SHALL BE POLYUREA TYPE I EXCEPT WHERE NOTED IN THE PLANS.
- ALL FINAL PAVEMENT MARKING MATERIALS PLACED ON BITUMINOUS PAVEMENT SHALL BE THERMOPLASTIC EXCEPT WHERE NOTED IN THE PLANS.
- FINAL PAVEMENT MARKING SYMBOLS SHALL BE OF SMALL SIZE ON MONROE STREET. SEE IDOT STANDARD 780001-05 FOR DETAILS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE OF THE IMPROVEMENT LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN ALL EXISTING PAVEMENT MARKINGS AND MARKERS FOR FUTURE REFERENCE. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE DIRECTED BY THE ENGINEER.
- THE RESIDENT ENGINEER SHALL CONTACT EXPRESSWAY FIELD ENGINEER MATTHEW DAEDA AT 847-705-4155 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- FOR ADDITIONAL DETAILS, SEE TYPICAL PAVEMENT MARKINGS TC-11, TC-12, AND TC-24. SEE CDOT DETAIL A-7-1A FOR CROSSWALK INFORMATION.
- THE STATION AND OFFSET FOR STOP BAR LOCATIONS IS GIVEN AT THE MIDDLE OF THE STOP BAR.
- STATIONS AND OFFSETS ARE FROM THE PROPOSED MONROE STREET ALIGNMENT.
- SEE SHEETS 119 AND 120 FOR PROPOSED PAVEMENT MARKING LOCATIONS ON I-90/94.

LEGEND

| PAVEMENT MARKING MATERIAL | PAVEMENT MARKING TYPE |
|---|--|
| <input type="checkbox"/> THERMOPLASTIC PAVEMENT MARKING | A - LETTERS AND SYBOLS (WHITE) |
| <input type="checkbox"/> POLYUREA PAVEMENT MARKING TYPE I | B - 4" YELLOW LINE |
| | C - 4" DOUBLE YELLOW LINE (8" C-C) |
| | D - 4" WHITE LINE (6' DASH, 18' SKIP) |
| | E - 4" WHITE LINE |
| | F - 6" WHITE LINE |
| | G - 24" WHITE STOP BAR |
| | H - 24" WHITE CROSSWALK, 9' WIDTH, SPACED @ 4' CENTERS |



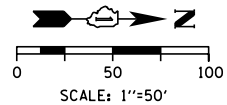
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|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
MONROE STREET

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. 8415+46.00 TO STA. 8417+62.00

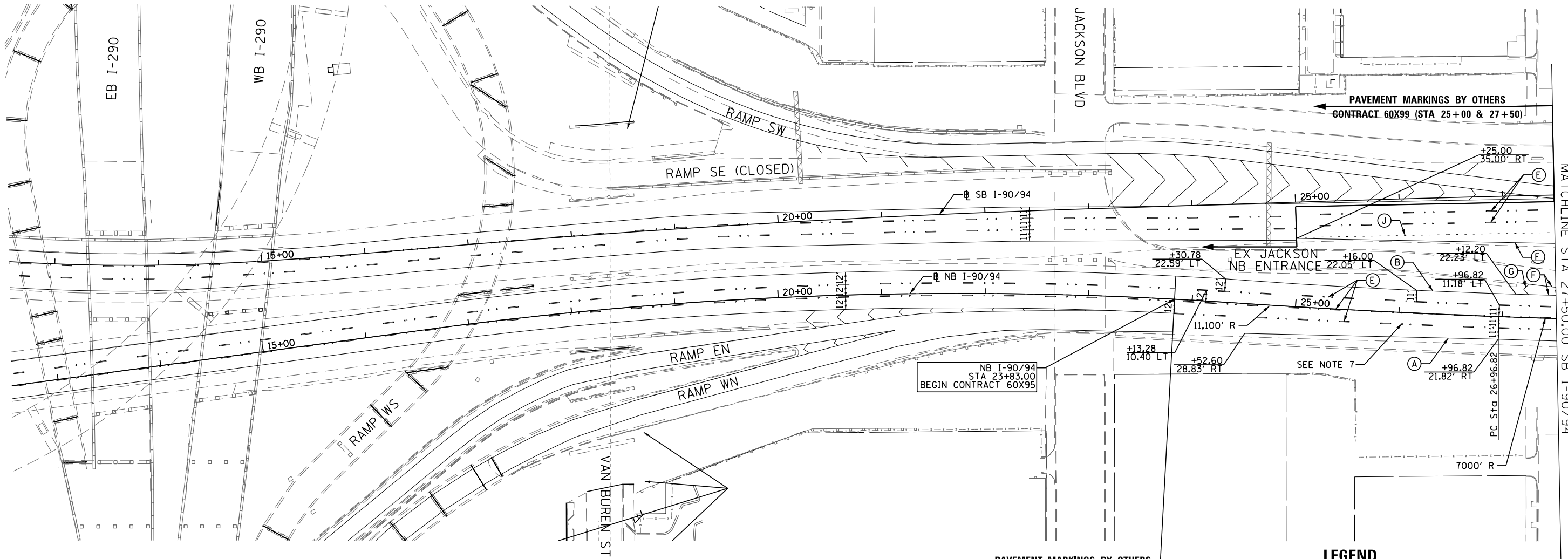
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|--------------------|---------------------|-------------|---------------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 118 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |



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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | ALIGNED | |
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| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| | NO. _____ | |

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PAVEMENT MARKING NOTES

- ALL FINAL PAVEMENT MARKING MATERIALS PLACED ON I-90 / 94 SHALL BE MODIFIED URETHANE EXCEPT WHERE NOTED IN THE PLANS.
- SEE TC-12 FOR RAISED REFLECTIVE PAVEMENT MARKINGS AT GORE AREAS.
- FINAL PAVEMENT MARKING SYMBOLS SHALL BE OF LARGE SIZE ON I-90 / 94. SEE IDOT STANDARD 780001-05 FOR DETAILS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE OF THE IMPROVEMENT LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN ALL EXISTING PAVEMENT MARKINGS AND MARKERS FOR FUTURE REFERENCE. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE DIRECTED BY THE ENGINEER.
- THE RESIDENT ENGINEER SHALL CONTACT IDOT EXPRESSWAY FIELD ENGINEER AT 847-705-4153 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- PROPOSED MARKINGS SHALL BE LAID OUT IN ORDER TO UTILIZE EXISTING RAISED REFLECTIVE MARKERS LOCATIONS. PROPOSED RAISED REFLECTIVE MARKER LIMITS ARE SHOWN IN THE PLANS.

- ALL REMOVED, DAMAGED, OR BROKEN RAISED REFLECTIVE MARKER REFLECTORS SHALL BE REPLACED AT THE DIRECTION OF THE RESIDENT ENGINEER AND PAID FOR AS REPLACEMENT REFLECTOR
- FOR ADDITIONAL DETAILS, SEE IDOT DISTRICT 1 STANDARDS TC-11, TC-12, T-13 AND TC-24.
- SEE PLAN SHEET 118 FOR MONROE STREET BRIDGE PAVEMENT MARKINGS.

PAVEMENT MARKINGS BY OTHERS
CONTRACT 60X99 (STA 24+83.00)

LEGEND

| PAVEMENT MARKING TYPE | |
|---------------------------|--|
| A | 4" WHITE EDGE LINE |
| B | 4" YELLOW EDGE LINE |
| C | 4" DOUBLE YELLOW LINE (8" C-C) |
| D | 4" WHITE LINE (6' DASH, 18' SKIP) |
| E | 5" WHITE (10' DASH, 30' SKIP) |
| F | 8" WHITE EDGE LINE |
| G | 12" WHITE CHEVRONS SPACED @ 30' CENTERS |
| H | 24" WHITE STOP BAR |
| I | 24" WHITE CROSSWALK, 6' WIDTH, SPACED @ 4' CENTERS |
| J | 4" WHITE (2' DASH, 6' SKIP) |
| K | 6" WHITE CENTER LINE |
| L | WHITE LETTERS AND SYMBOLS |
| M | 8" WHITE (3' DASH, 9' SKIP) |
| PAVEMENT MARKING MATERIAL | |
| ◁ ◁ | RAISED REFLECTIVE PAVEMENT MARKER |
| ○ | MODIFIED URETHANE PAVEMENT MARKING |

**PARSONS
BRINCKERHOFF**

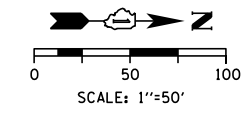
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|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
I-90 / 94 AND RAMPS

SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. 27+50 TO STA.

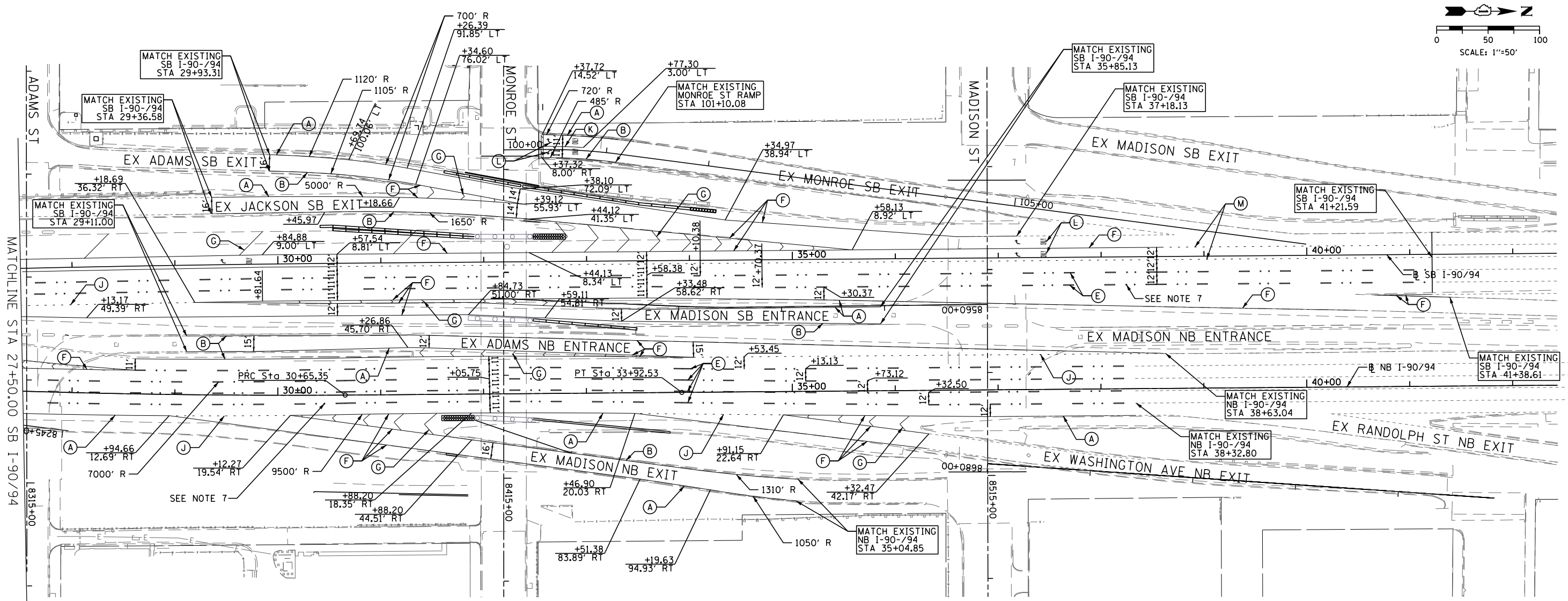
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| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 119 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |



| | |
|---------------|--|
| DATE | |
| BY | |
| PLAN | |
| NOTE BOOK NO. | |
| SURVEYED | |
| ALIGNED | |
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| DATE | |
| BY | |
| PROFILE | |
| NOTE BOOK NO. | |
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| GRADES | |
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| STRUCTURE | |
| NOTATIONS | |
| CHPNO | |

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PAVEMENT MARKING NOTES

- ALL FINAL PAVEMENT MARKING MATERIALS PLACED ON I-90 / 94 SHALL BE MODIFIED URETHANE EXCEPT WHERE NOTED IN THE PLANS.
- SEE TC-12 FOR RAISED REFLECTIVE PAVEMENT MARKINGS AT GORE AREAS.
- FINAL PAVEMENT MARKING SYMBOLS SHALL BE OF LARGE SIZE ON I-90 / 94. SEE IDOT STANDARD 780001-05 FOR DETAILS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE OF THE IMPROVEMENT LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN ALL EXISTING PAVEMENT MARKINGS AND MARKERS FOR FUTURE REFERENCE. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE DIRECTED BY THE ENGINEER.
- THE RESIDENT ENGINEER SHALL CONTACT IDOT EXPRESSWAY FIELD ENGINEER AT 847-705-4153 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- FOR ADDITIONAL DETAILS, SEE IDOT DISTRICT 1 STANDARDS TC-11, TC-12, T-13 AND TC-24.
- STATIONS AND OFFSETS ARE FROM THE EXISTING I-90/94 NORTHBOUND OR SOUTHBOUND ALIGNMENT.
- SEE PLAN SHEET 118 FOR MONROE STREET BRIDGE PAVEMENT MARKINGS.

LEGEND

| PAVEMENT MARKING TYPE | |
|---------------------------|--|
| A | 4" WHITE EDGE LINE |
| B | 4" YELLOW EDGE LINE |
| C | 4" DOUBLE YELLOW LINE (8" C-C) |
| D | 4" WHITE LINE (6' DASH, 18' SKIP) |
| E | 5" WHITE (10' DASH, 30' SKIP) |
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| G | 12" WHITE CHEVRONS SPACED @ 30' CENTERS |
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| J | 4" WHITE (2' DASH, 6' SKIP) |
| K | 6" WHITE CENTER LINE |
| L | WHITE LETTERS AND SYMBOLS |
| M | 8" WHITE (3' DASH, 9' SKIP) |
| PAVEMENT MARKING MATERIAL | |
| ◁ ◁ | RAISED REFLECTIVE PAVEMENT MARKER |
| ○ | MODIFIED URETHANE PAVEMENT MARKING |

PARSONS BRINCKERHOFF

| | | |
|------------------------|-------------------|-----------|
| USER NAME = Kucherski | DESIGNED - AMK | REVISED - |
| PLLOT SCALE = 1"=50' | DRAWN - AMK | REVISED - |
| PLLOT DATE = 4/18/2017 | CHECKED - RPH | REVISED - |
| | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN
I-90 /94 AND RAMPS

SCALE: 1"=50' SHEET NO. 3 OF 3 SHEETS STA. 27+50 TO STA.

| | | | | |
|--------------------|---------------------|-------------|---------------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 120 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

SIGNING SCHEDULE – MONROE STREET

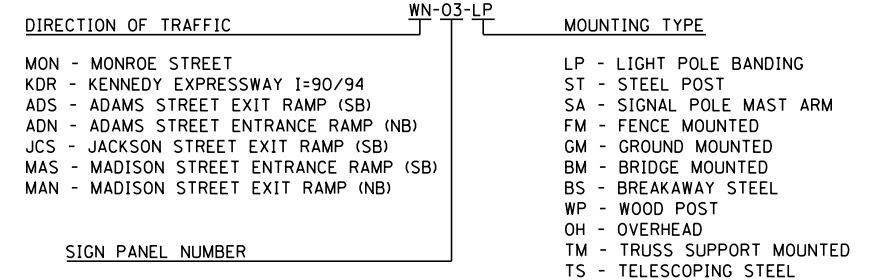
| LOCATION | SIGN NO. | LEGEND / DESCRIPTION | CODE | ACTION | LOCATION (STATION / OFFSET) | | | | EXISTING PANEL DIMENSIONS | | PROPOSED PANEL DIMENSIONS | | STEEL POSTS, SPECIAL | REMOVE SIGN PANEL TYPE 1 (SQ FT) | REMOVE SIGN PANEL ASSEMBLY - TYPE A (EACH) | SIGN PANEL TYPE 1 (SQ FT) |
|---------------|-----------|-------------------------|--------------------|-------------------------|-----------------------------|-----------|------------|-------------|---------------------------|-------------|---------------------------|------|----------------------|----------------------------------|--|---------------------------|
| | | | | | EXISTING | PROPOSED | WIDTH (FT) | HEIGHT (FT) | WIDTH (FT) | HEIGHT (FT) | | | | | | |
| MONROE STREET | MON-01-LP | NO PARKING TOW ZONE | R7-201-4 | EXISTING SIGN TO REMAIN | 8410+78.84 | 23.71' LT | - | - | 1.50 | 1.50 | - | - | | | | |
| | MON-02-LP | ONE WAY | R6-2 | EXISTING SIGN TO REMAIN | 8417+50.73 | 24.07' RT | - | - | 1.50 | 2.00 | - | - | | | | |
| | MON-03-LP | NO PARKING TOW ZONE | R7-201-4 | REMOVE AND REPLACE | 8415+77.84 | 24' RT | 8415+77.84 | 24' RT | 1.50 | 1.50 | 1.50 | 1.50 | | 2.25 | | 2.25 |
| | MON-04-LP | SCHOOL ZONE SPEED LIMIT | S4-3P, R2-1, S4-3P | REMOVE AND REPLACE | 8415+77.84 | 24' RT | 8415+77.84 | 24' RT | 2.50 | 2.50 | 2.50 | 2.50 | | 6.25 | | 6.25 |
| | MON-05-ST | SCHOOL | S1-1 | REMOVE AND REPLACE | 8414+24.18 | 26' RT | 8414+24.18 | 26' RT | 2.50 | 2.50 | 2.50 | 2.50 | 1 | | 1 | 6.25 |
| | MON-06-LP | NO PARKING TOW ZONE | R7-201-4 | REMOVE AND REPLACE | 8411+72.64 | 23.44' RT | 8411+72.64 | 23.44' RT | 1.50 | 1.50 | 1.50 | 1.50 | | 2.25 | | 2.25 |
| | MON-07-LP | NO PARKING TOW ZONE | R7-201-4 | REMOVE AND REPLACE | 8410+12.10 | 23.69' RT | 8410+12.10 | 23.69' RT | 1.50 | 1.50 | 1.50 | 1.50 | | 2.25 | | 2.25 |
| | MON-08-ST | NO RIGHT TURN | R3-1 | REMOVE AND REPLACE | 8412+08.00 | 23.95' LT | 8412+08.00 | 23.95' LT | 2.00 | 2.00 | 2.00 | 2.00 | 1 | | 1 | 4.00 |
| | MON-09-ST | ONE-WAY | R6-2L | REMOVE AND REPLACE | 8411+90.83 | 36.59' LT | 8411+90.83 | 36.59' LT | 1.50 | 2.00 | 1.50 | 2.00 | 1 | | 1 | 3.00 |
| | MON-10-ST | DO NOT ENTER | R5-1 | REMOVE AND REPLACE | 8411+90.83 | 36.59' LT | 8411+90.83 | 36.59' LT | 2.50 | 2.50 | 2.50 | 2.50 | 1 | | | 6.25 |
| | MON-11-ST | DO NOT ENTER | R5-1 | REMOVE AND REPLACE | 8411+61.23 | 37.14' LT | 8411+61.23 | 39.31' LT | 2.50 | 2.50 | 2.50 | 2.50 | 1 | | 1 | 6.25 |
| | MON-12-ST | STOP | R1-1 | REMOVE AND REPLACE | 8411+61.23 | 37.14' LT | 8411+61.23 | 37.14' LT | 2.50 | 2.50 | 2.50 | 2.50 | 1 | | 1 | 6.25 |
| TOTAL | | | | | | | | | | | | 6 | 13.00 | 4 | 45.00 | |

SIGNING SCHEDULE – EXPRESSWAY

| LOCATION | SIGN NO. | LEGEND / DESCRIPTION | CODE | ACTION | LOCATION (STATION / OFFSET) | | | | EXISTING PANEL DIMENSIONS | | PROPOSED PANEL DIMENSIONS | | NUMBER OF POSTS | REMOVE SIGN PANEL TYPE 1 (SQ FT) | REMOVE SIGN PANEL TYPE 2 (SQ FT) | REMOVE SIGN PANEL TYPE 3 (SQ FT) | SIGN PANEL TYPE 2 (SQ FT) | OVERHEAD SIGN STRUCTURE BRIDGE MOUNTED (FOOT) | REMOVE, STORE AND RE-ERECT SIGN PANEL (SPECIAL) (EACH) | REMOVE, STORE AND RE-ERECT SIGN PANEL ASSEMBLY (SPECIAL) (*) (EACH) |
|----------------------------|-----------|---|--------|---|-----------------------------|------------|------------|-------------|---------------------------|-------------|---------------------------|-------|-----------------|----------------------------------|----------------------------------|----------------------------------|---------------------------|---|--|---|
| | | | | | EXISTING | PROPOSED | WIDTH (FT) | HEIGHT (FT) | WIDTH (FT) | HEIGHT (FT) | | | | | | | | | | |
| NB I-90/94 | KDR-01-ST | GATEWAY GREEN | CUSTOM | REMOVE | 30+24.02 | 69.60' RT | NOTE 4 | NOTE 4 | 6.00 | 6.00 | - | - | - | - | - | - | - | - | 1 | |
| | KDR-02-ST | SPEED LIMIT (45 MPH) | R2-1 | REMOVE & REPLACE EXISTING SIGN | 29+14.00 | 46.00' RT | - | - | 3.00 | 4.00 | 3.00 | 4.00 | - | 12.00 | - | - | - | - | - | |
| | KDR-03-BM | EXIT 51C EXIT EAST WASHINGTON BLVD | CUSTOM | REMOVE, STORE & REINSTALL EXISTING SIGN | 31+82.31 | 13.00' RT | NOTE 3 | NOTE 3 | 17.00 | 9.00 | 17.00 | 9.00 | - | - | - | - | 17.00 | 1 | | |
| | KDR-04-BM | EXIT 51D EXIT MADISON STREET | CUSTOM | REMOVE, STORE & REINSTALL EXISTING SIGN | 31+83.41 | 52.00' RT | NOTE 3 | NOTE 3 | 15.00 | 6.50 | 15.00 | 6.50 | - | - | - | - | 15.00 | 1 | | |
| ADAMS ENTR. RAMP (NB) | ADN-01-ST | MERGING TRAFFIC | W4-1R | REPLACE EXISTING SIGN WITH W4-3L(X) | 31+19.86 | 39.94' LT | 32+14.32 | 36.83' LT | 4.00 | 4.00 | 4.00 | 4.00 | - | | 16.00 | | 16.00 | | | |
| MADISON ENTRANCE RAMP (SB) | MAS-01-ST | MERGING TRAFFIC | W4-1R | REMOVE AND REPLACE EXISTING SIGN | 33+83.30 | 105.20' RT | - | - | 4.00 | 4.00 | 4.00 | 4.00 | - | | 16.00 | | 16.00 | | | |
| ADAMS EXIT RAMP (SB) | ADS-01-ST | DOUBLE ARROW EXIT | W12-1 | EXISTING SIGN TO REMAIN | 31+32.19 | 70.35' LT | - | - | 3.00 | 2.00 | - | - | - | - | - | - | - | - | | |
| SB I-90/94 | KDR-01-BM | EXIT 51 I EAST CONGRESS PKWY / CHICAGO LOOP EXIT 51 H WEST I-290 WEST SUBURBS EXIT ONLY | CUSTOM | REMOVE, STORE & REINSTALL EXISTING SIGN | 32+56.74 | 0.00' | NOTE 3 | NOTE 3 | 36.50 | 12.00 | 36.50 | 12.00 | - | - | - | - | 36.50 | 1 | | |
| | KDR-02-BM | EXIT 51G EAST JACKSON BLVD | CUSTOM | REMOVE, STORE & REINSTALL EXISTING SIGN | 32+55.14 | 47.28' LT | NOTE 3 | NOTE 3 | 16.00 | 11.00 | 16.00 | 11.00 | - | - | - | - | 16.00 | 1 | | |
| | KDR-03-BM | EXIT 51F WEST ADAMS ST | CUSTOM | REMOVE, STORE & REINSTALL EXISTING SIGN | 32+55.24 | 59.37' LT | NOTE 3 | NOTE 3 | 12.50 | 11.00 | 12.50 | 11.00 | - | - | - | - | 12.50 | 1 | | |
| TOTAL | | | | | | | | | | | | 0 | 12 | 32 | 36 | 32 | 97 | 5 | 1 | |

(*) PROPOSED POST INCLUDED WITH SPECIAL PROVISION

SIGN NUMBERING CODE EXAMPLE



R7-201-4
SEE NOTE 1
DOUBLE SIDED
REFLECTIVE: NO
MOUNT: FLAG
(18"x18")

MON-01-LP
MON-03-LP
MON-06-LP
MON-07-LP

R1-1-1
REFLECTIVE: YES
MOUNT: CENTER
(30"x30")

MON-12-ST

NOTES:

- FOR CDOT SIGN PANEL DETAILS SEE THE MOST CURRENT EDITION OF "THE FIELD MANUAL FOR SIGN INSTALLATION GUIDE" AND "SIGN FABRICATION AND INSTALLATIONS" MANUALS. ALL SIGNS ALONG MONROE STREET MUST ACCOMMODATE THESE CDOT MANUALS.
- ALL STATION AND OFFSETS ARE FROM THE BASELINE ALONG WHERE THE SIGN IS LOCATED.
- FOR BRIDGE MOUNTED SIGN PANEL PLACEMENT, SEE PROPOSED ELEVATION VIEWS.
- THE CONTRACTOR SHALL CONTACT MELISSA DEL ROSARIO AT 847-705-4391 BEFORE REMOVING THE SIGN AND WILL DELIVER THE SIGN FOR STORAGE AT THE IDOT FACILITY AT 1260 WEST AUGUSTA IN CHICAGO. CONTRACTOR SHALL PICK UP THE SIGN AT THE SAME LOCATION FOR INSTALLATION ON SITE AFTER GRADING IS COMPLETE.



| | | |
|-----------------------|-------------------|-----------|
| USER NAME = ohmedmm | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 6/29/2017 | DATE - 06/30/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING PLAN
SCHEDULE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 121 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

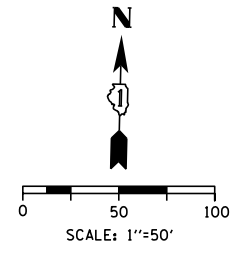
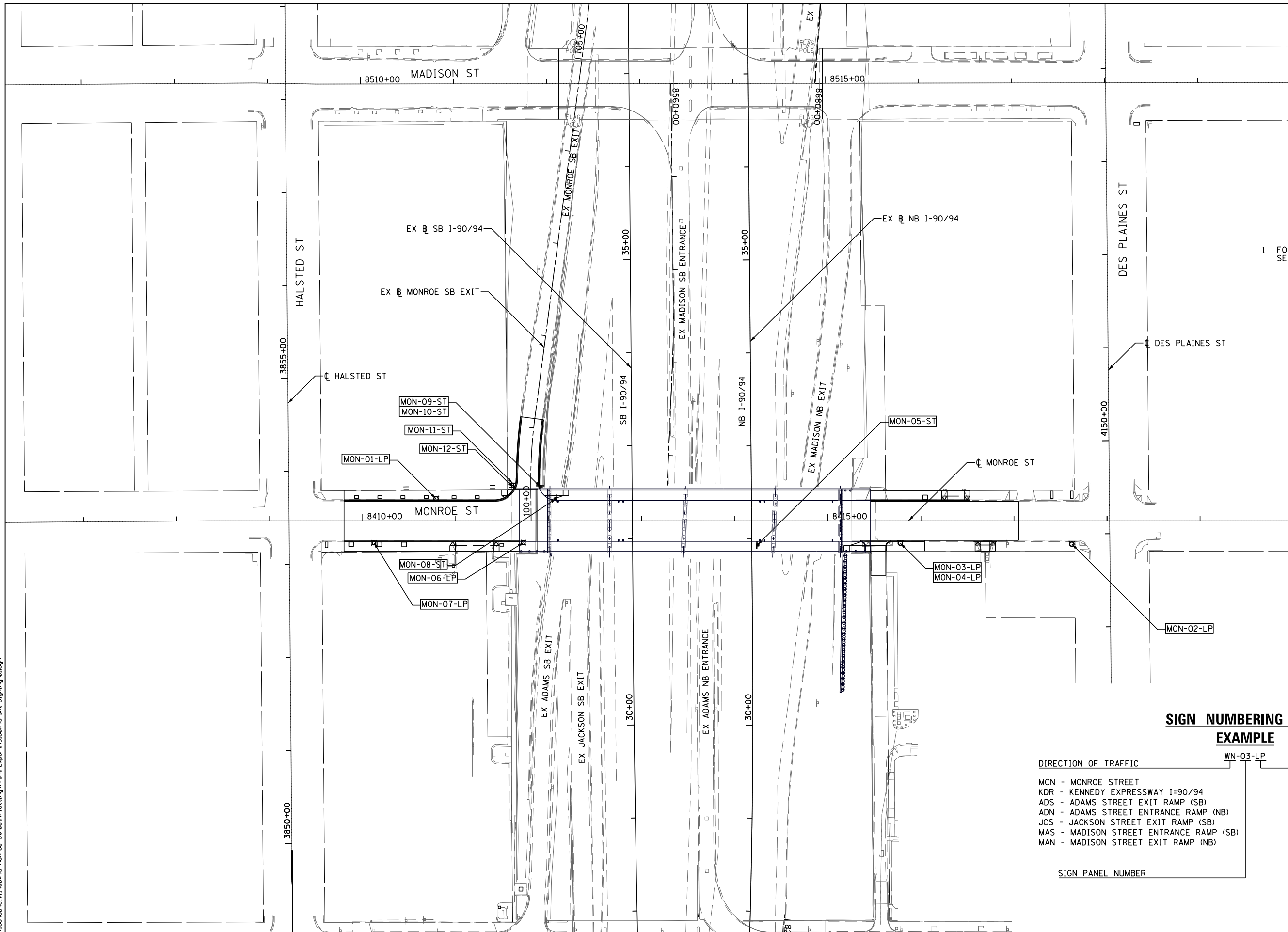
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| | ALIGNMENT CHECKED | |
| | FILE NAME | |
| | NOTE BOOK NO. | |

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| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS CHKD | |
| | NOTE BOOK NO. | |

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| PLAN | SURVEYED | DATE |
| | PLOTTED | BY |
| | NOTE BOOK | |
| | NO. | |
| | ALIGNED | |
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| PROFILE | SURVEYED | DATE |
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| | NOTE BOOK | |
| | NO. | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |

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1 FOR STATION AND OFFSETS SEE SIGNING PLAN SCHEDULE.

SIGN NUMBERING CODE EXAMPLE

| | | |
|---|----------|----------------------------|
| DIRECTION OF TRAFFIC | WN-03-LP | MOUNTING TYPE |
| MON - MONROE STREET | | LP - LIGHT POLE BANDING |
| KDR - KENNEDY EXPRESSWAY I-90/94 | | ST - STEEL POST |
| ADS - ADAMS STREET EXIT RAMP (SB) | | SA - SIGNAL POLE MAST ARM |
| ADN - ADAMS STREET ENTRANCE RAMP (NB) | | FM - FENCE MOUNTED |
| JCS - JACKSON STREET EXIT RAMP (SB) | | GM - GROUND MOUNTED |
| MAS - MADISON STREET ENTRANCE RAMP (SB) | | BM - BRIDGE MOUNTED |
| MAN - MADISON STREET EXIT RAMP (NB) | | BS - BREAKAWAY STEEL |
| | | WP - WOOD POST |
| | | OH - OVERHEAD |
| | | TM - TRUSS SUPPORT MOUNTED |
| | | TS - TELESCOPING STEEL |

PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

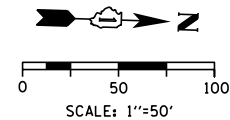
**SIGNING PLAN
MONROE STREET**

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

| | | | | |
|---------------------------|---------------------|-------------|--------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 122 |
| | | | CONTRACT NO. 60X95 | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES

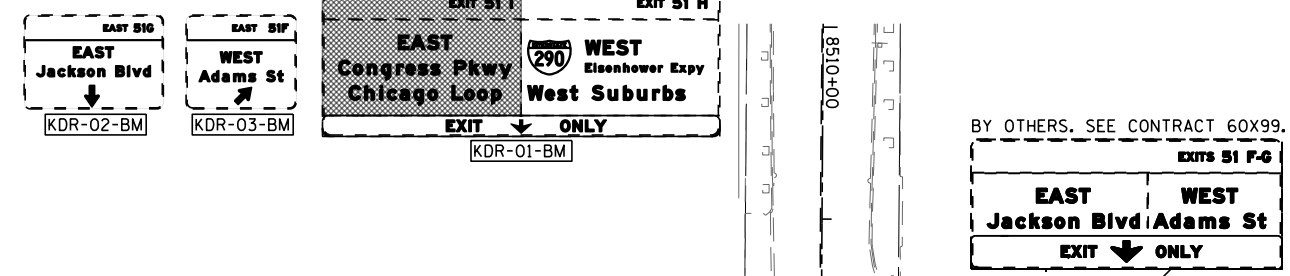
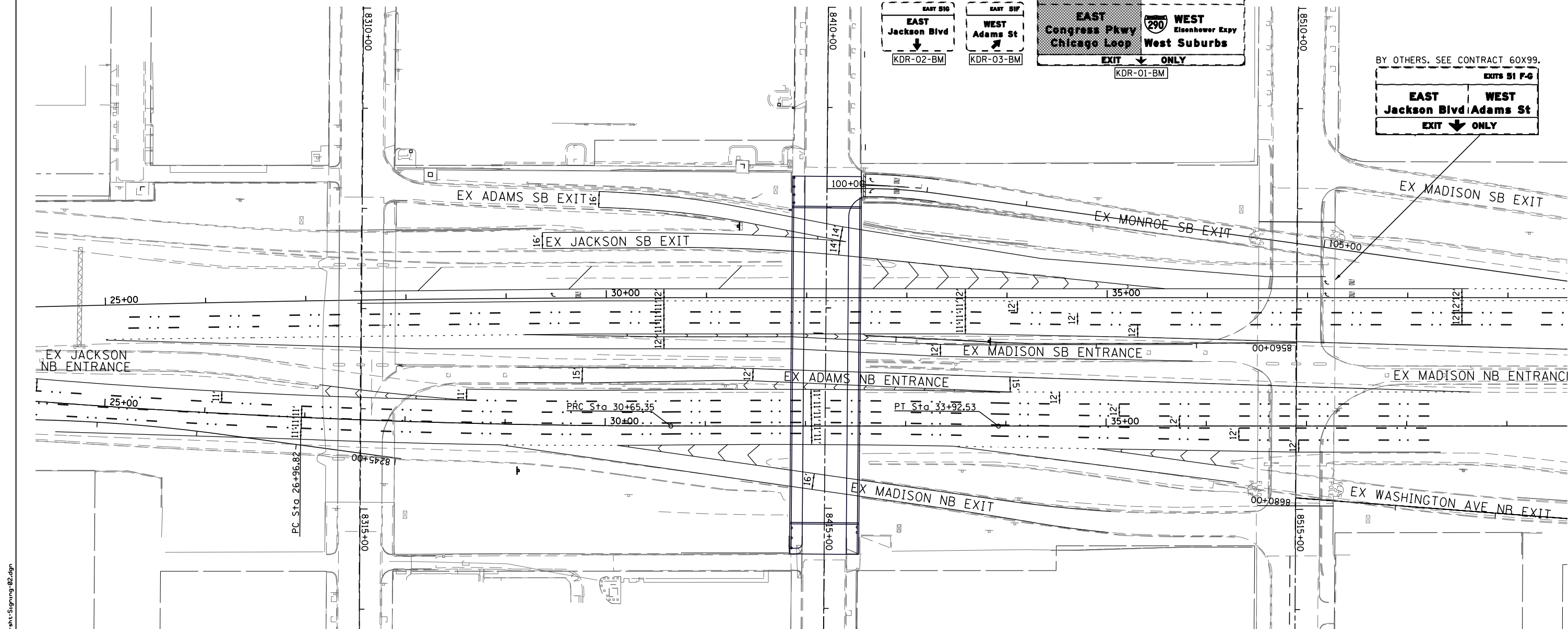
① FOR STATION AND OFFSETS SEE SIGNING PLAN SCHEDULE.



NOTE:
FOR SIGN KDR-01-BM SEE CONTRACT
60X99 FOR SIGN COVERING.

| | | |
|------|-----------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | ALIGNED | |
| | CHECKED | |
| | FILE NAME | |
| | NO. | |

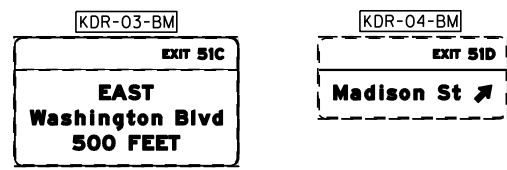
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| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES | |
| | CHECKED | |
| | STRUCTURE | |
| | NOTATIONS | |
| | CHFD | |



BY OTHERS. SEE CONTRACT 60X99.

SIGN NUMBERING CODE EXAMPLE

| DIRECTION OF TRAFFIC | WN-03-LP | MOUNTING TYPE |
|---|----------|----------------------------|
| MON - MONROE STREET | | LP - LIGHT POLE BANDING |
| KDR - KENNEDY EXPRESSWAY I-90/94 | | ST - STEEL POST |
| ADS - ADAMS STREET EXIT RAMP (SB) | | SA - SIGNAL POLE MAST ARM |
| ADN - ADAMS STREET ENTRANCE RAMP (NB) | | FM - FENCE MOUNTED |
| JCS - JACKSON STREET EXIT RAMP (SB) | | GM - GROUND MOUNTED |
| MAS - MADISON STREET ENTRANCE RAMP (SB) | | BM - BRIDGE MOUNTED |
| MAN - MADISON STREET EXIT RAMP (NB) | | BS - BREAKAWAY STEEL |
| | | WP - WOOD POST |
| | | OH - OVERHEAD |
| | | TM - TRUSS SUPPORT MOUNTED |
| | | TS - TELESCOPING STEEL |



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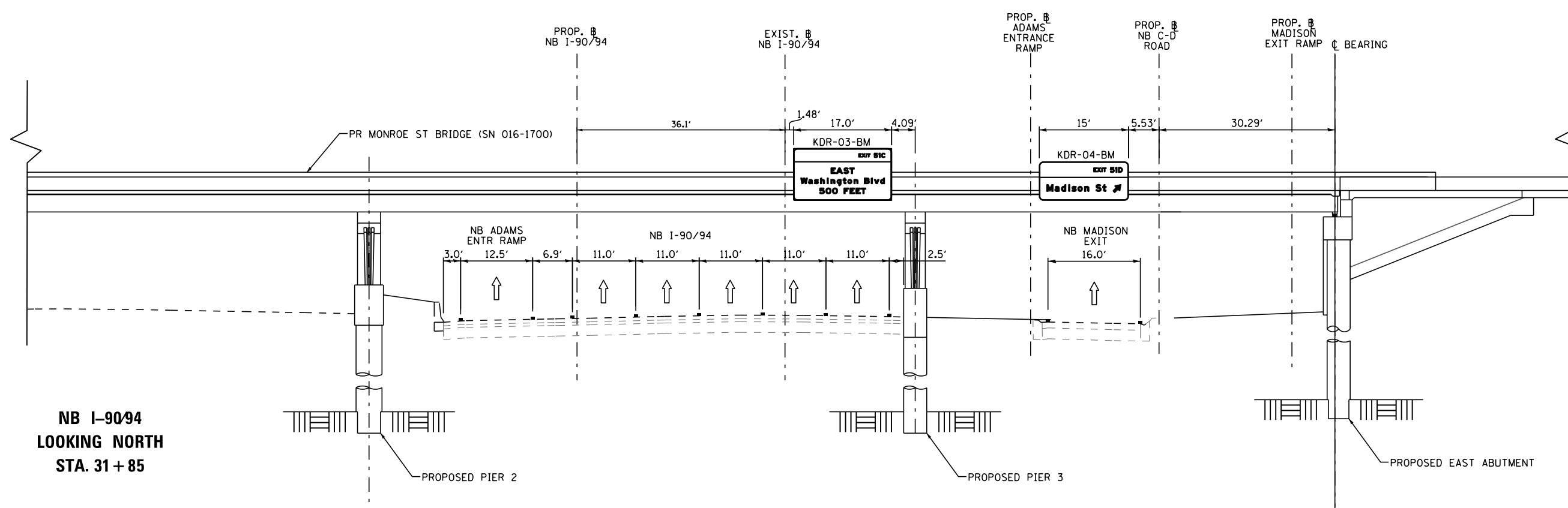
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|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | |
|---------------|-------------------------|
| SIGNING PLAN | |
| I-90/94 | |
| SCALE: 1"=50' | SHEET NO. 2 OF 2 SHEETS |
| STA. | TO STA. |

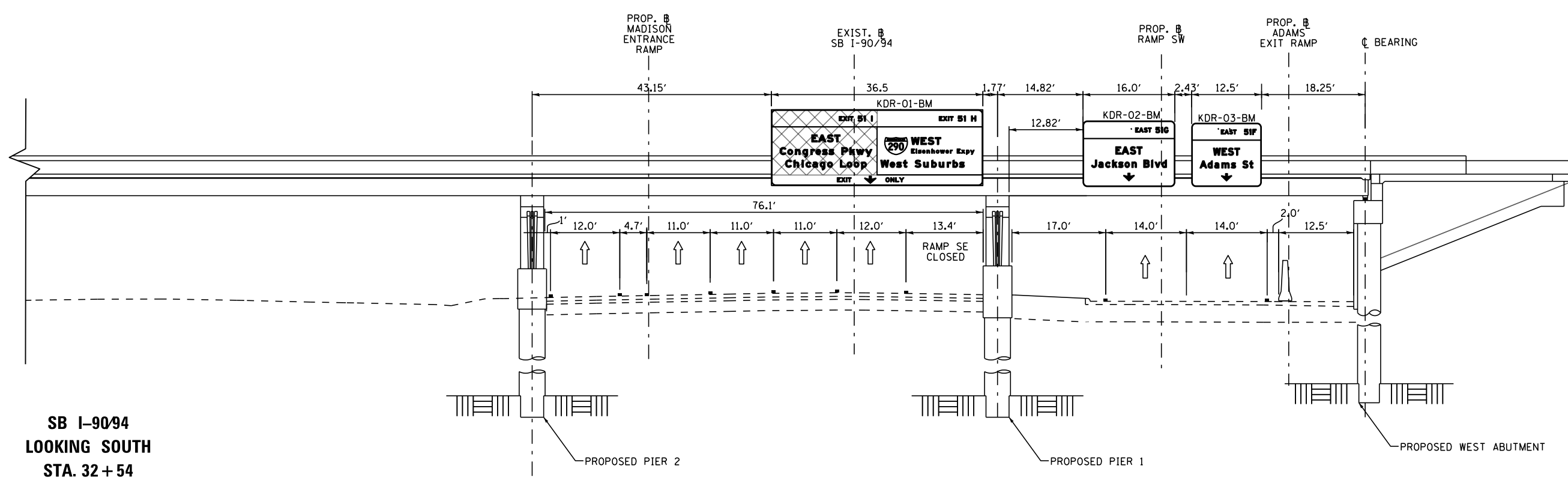
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|--------------------|---------------------|-------------|---------------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 123 |
| CONTRACT NO. 60X99 | | | ILLINOIS FED. AID PROJECT | |

| | |
|-----------|--|
| DATE | |
| BY | |
| PLAN | |
| SURVEYED | |
| PLOTTED | |
| NOTE BOOK | |
| NO. | |
| ALIGNED | |
| CHECKED | |
| FILE NAME | |



**NB I-90/94
LOOKING NORTH
STA. 31 + 85**

| | |
|----------------|--|
| DATE | |
| BY | |
| PROFILE | |
| SURVEYED | |
| PLOTTED | |
| NOTE BOOK | |
| NO. | |
| GRADES CHECKED | |
| STRUCTURE | |
| NOTATMS | |
| CHKD | |



**SB I-90/94
LOOKING SOUTH
STA. 32 + 54**

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**PARSONS
BRINCKERHOFF**

| | | | | | |
|--------------|-----------|------------|------------|-----------|--|
| USER NAME = | ahmedmm | DESIGNED - | MMA | REVISED - | |
| | | DRAWN - | MMA | REVISED - | |
| PLOT SCALE = | | CHECKED - | RT | REVISED - | |
| PLOT DATE = | 6/29/2017 | DATE - | 06/30/2017 | REVISED - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

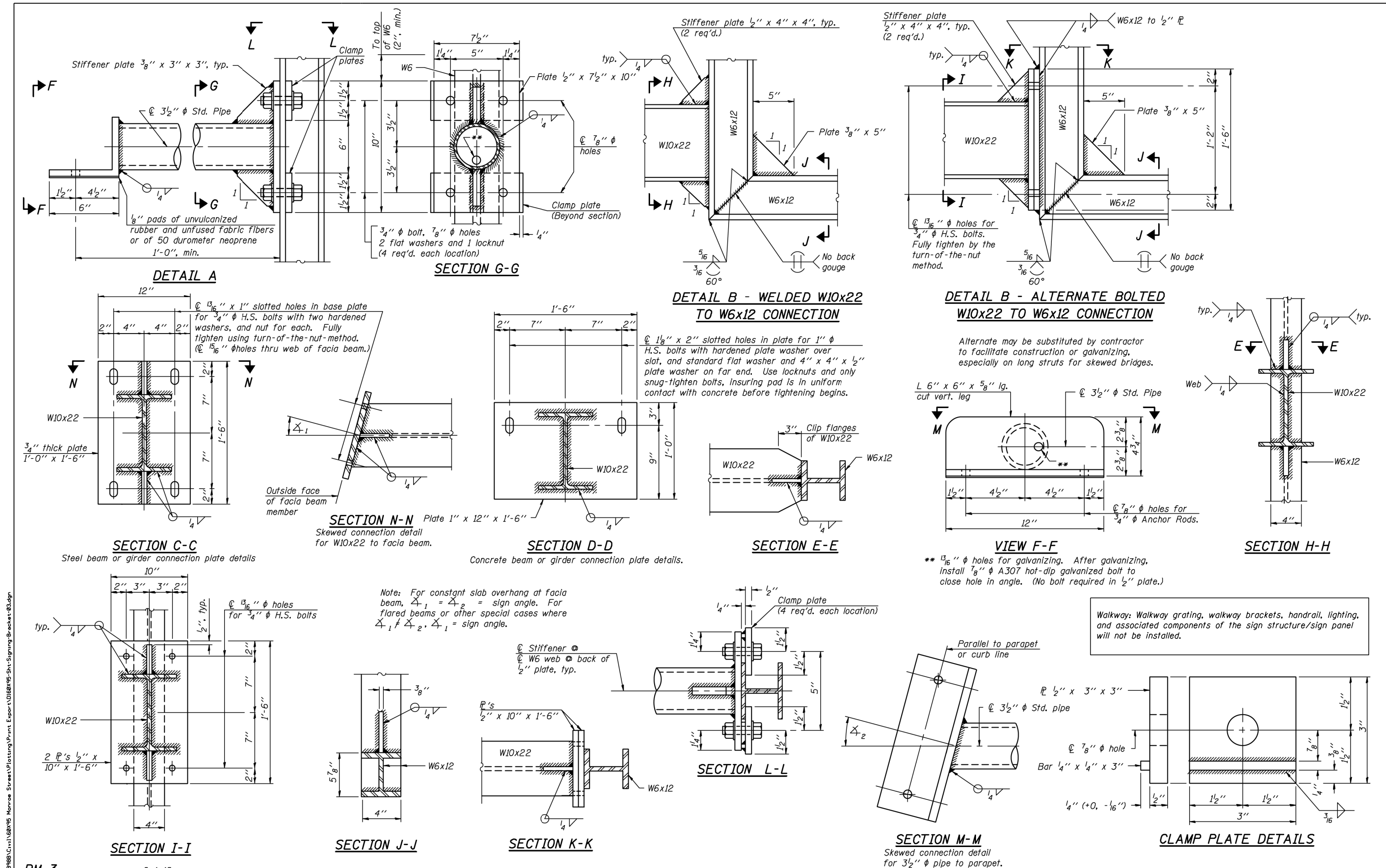
**OVERHEAD SIGN STRUCTURES SIGN PANEL PLACEMENT
MONROE STREET BRIDGE**

SCALE: SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 124 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

| | |
|----------|--|
| DATE | |
| BY | |
| REVIEWED | |
| PLANNED | |
| NOTED | |
| FILED | |
| NO. | |

| | |
|----------|--|
| DATE | |
| BY | |
| REVIEWED | |
| PLANNED | |
| NOTED | |
| FILED | |
| NO. | |



BM-3 6-1-12

PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = ahmedmm | DESIGNED - RPH | REVISED - |
| PLOT SCALE = 1"=50' | DRAWN - RPH | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - HA | REVISED - |
| | DATE - 06/30/2017 | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

BRIDGE MOUNT SIGN STRUCTURES CONNECTION DETAILS
SCALE: SHEET NO. 3 OF 3 SHEETS STA. TO STA.

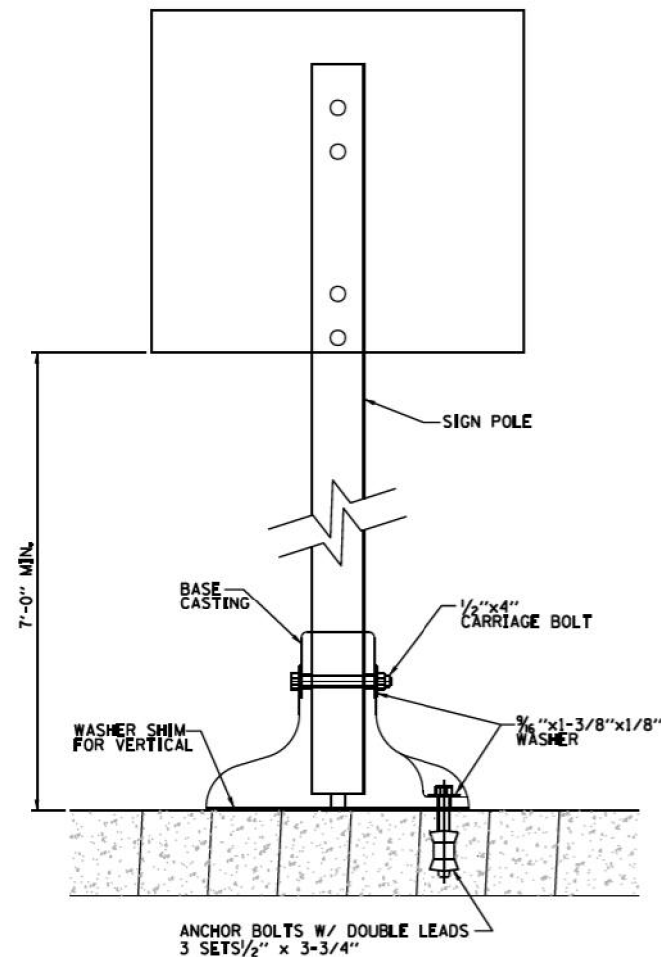
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|---------------------------|---------------------|-------------|--------------------|---------------|
| F.A.I. R.T.E. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 127 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

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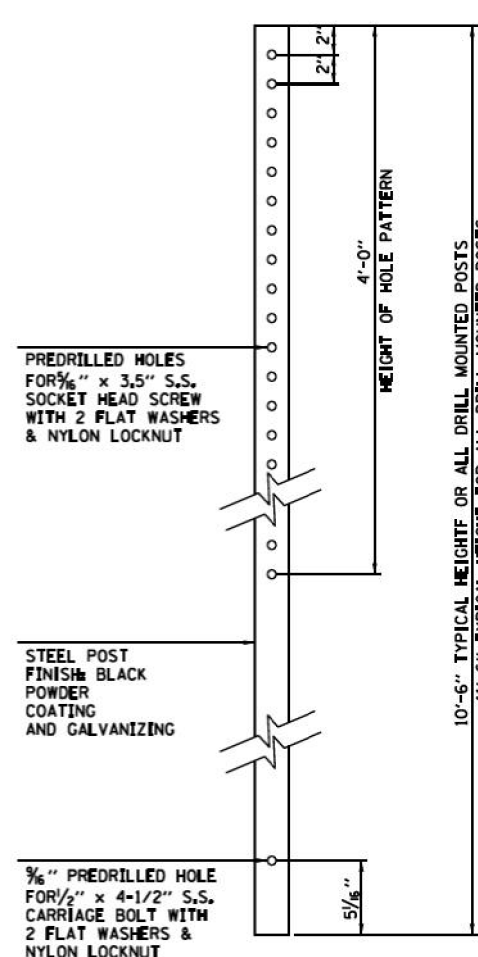
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|------|--------------------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| | BY | |
| | DATE | |

| | | |
|---------|--------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | STRUCTURE NOTATIONS OK'D | |
| | BY | |
| | DATE | |

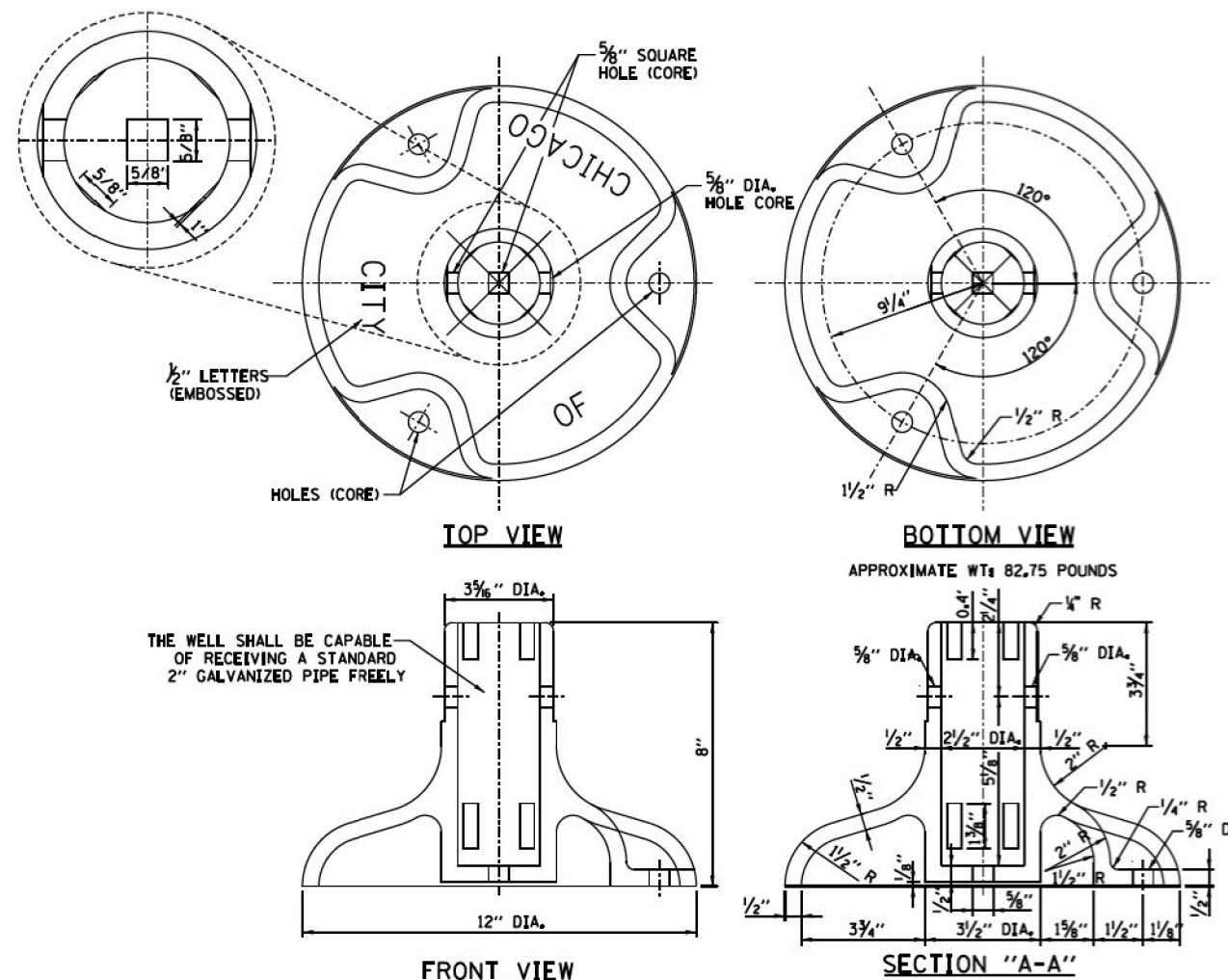
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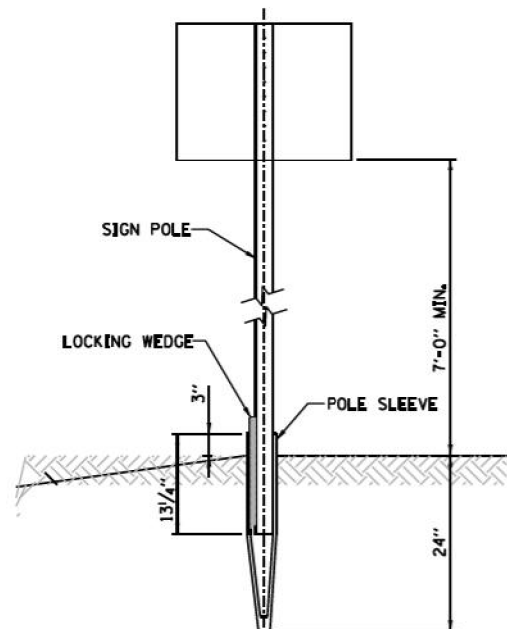
DRILL MOUNTED INSTALLATION DETAIL
NOT TO SCALE



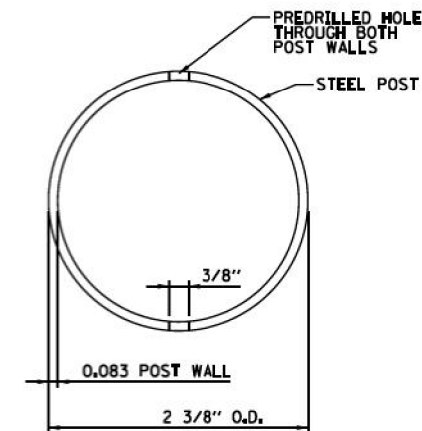
1 ELEVATION: DRILLED SIGN POST
NOT TO SCALE



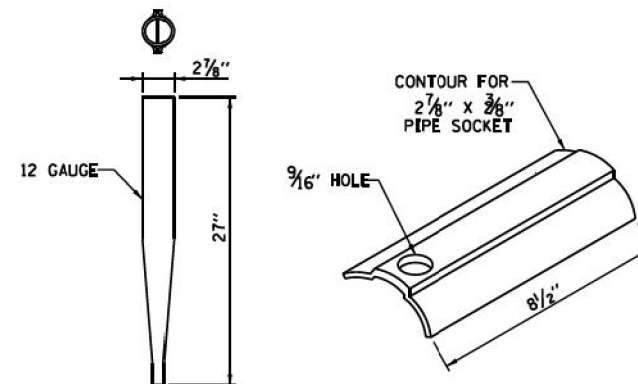
SIGN POLE BASE DETAIL
NOT TO SCALE



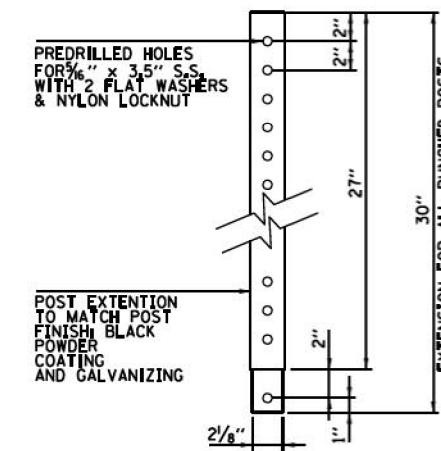
GROUND DIG INSTALLATION DETAIL
NOT TO SCALE



2 ELEVATION: DRILLED POST
NOT TO SCALE



PIPE SOCKET AND WEDGE DETAIL
NOT TO SCALE



SECTION: DRILLED POST EXTENSION
NOT TO SCALE

NOTE:
PROVIDE ADDITIONAL TWO SETS OF PREDRILLED HOLES ON EXTENSION. HOLES SHALL BE LOCATED AT 30° ANGLE TO HOLES SHOWN IN SECTION 3. HOLES SHALL ACCOMMODATE 5/8\"/>

PARSONS BRINCKERHOFF

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING DETAILS

SCALE: NONE SHEET NO. 1 OF 3 SHEETS STA. TO STA.

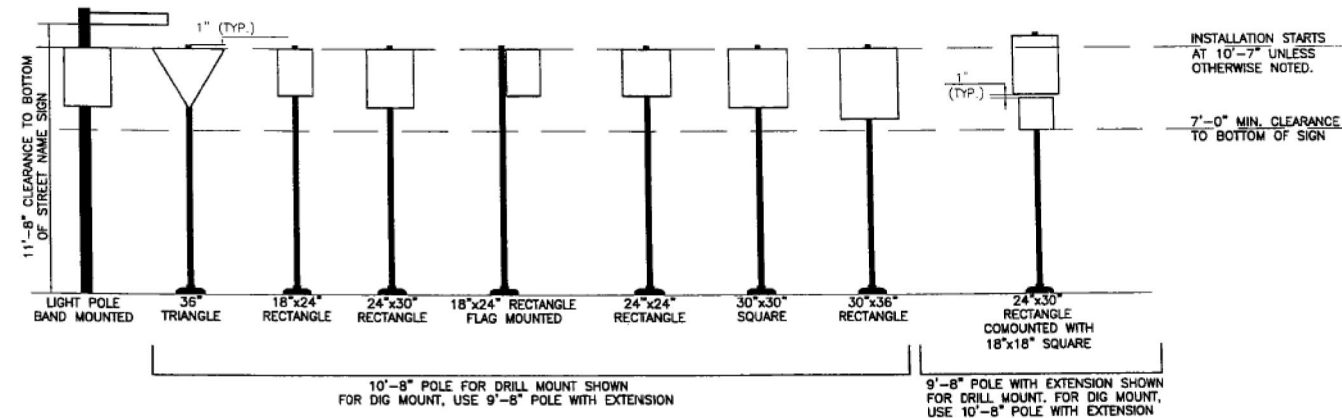
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|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 128 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |

| | |
|-----------------------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| GRADES CHECKED | |
| ALIGNMENT CHECKED | |
| STRUCTURE NOTATIONS CHECKED | |
| NOTE BOOK NO. | |
| PLAN | |

| | |
|-----------------------------|--|
| DATE | |
| BY | |
| SURVEYED | |
| PLOTTED | |
| GRADES CHECKED | |
| ALIGNMENT CHECKED | |
| STRUCTURE NOTATIONS CHECKED | |
| NOTE BOOK NO. | |
| PROFILE | |

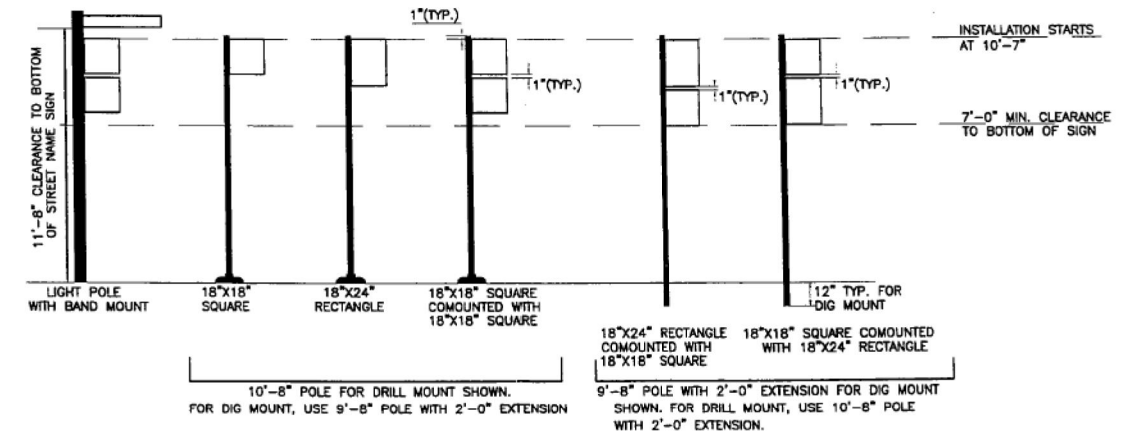
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REGULATORY TRAFFIC SIGNS (CHICAGO STYLE)



TYPICAL LAYOUT
NOT TO SCALE

REGULATORY PARKING SIGNS (CHICAGO STYLE)



TYPICAL LAYOUT
NOT TO SCALE

**PARSONS
BRINCKERHOFF**

| | | |
|-----------------------|-------------------|-----------|
| USER NAME = Kucharski | DESIGNED - AMK | REVISED - |
| | DRAWN - AMK | REVISED - |
| PLOT SCALE = 1"=50' | CHECKED - RPH | REVISED - |
| PLOT DATE = 4/18/2017 | DATE - 05/02/2017 | REVISED - |

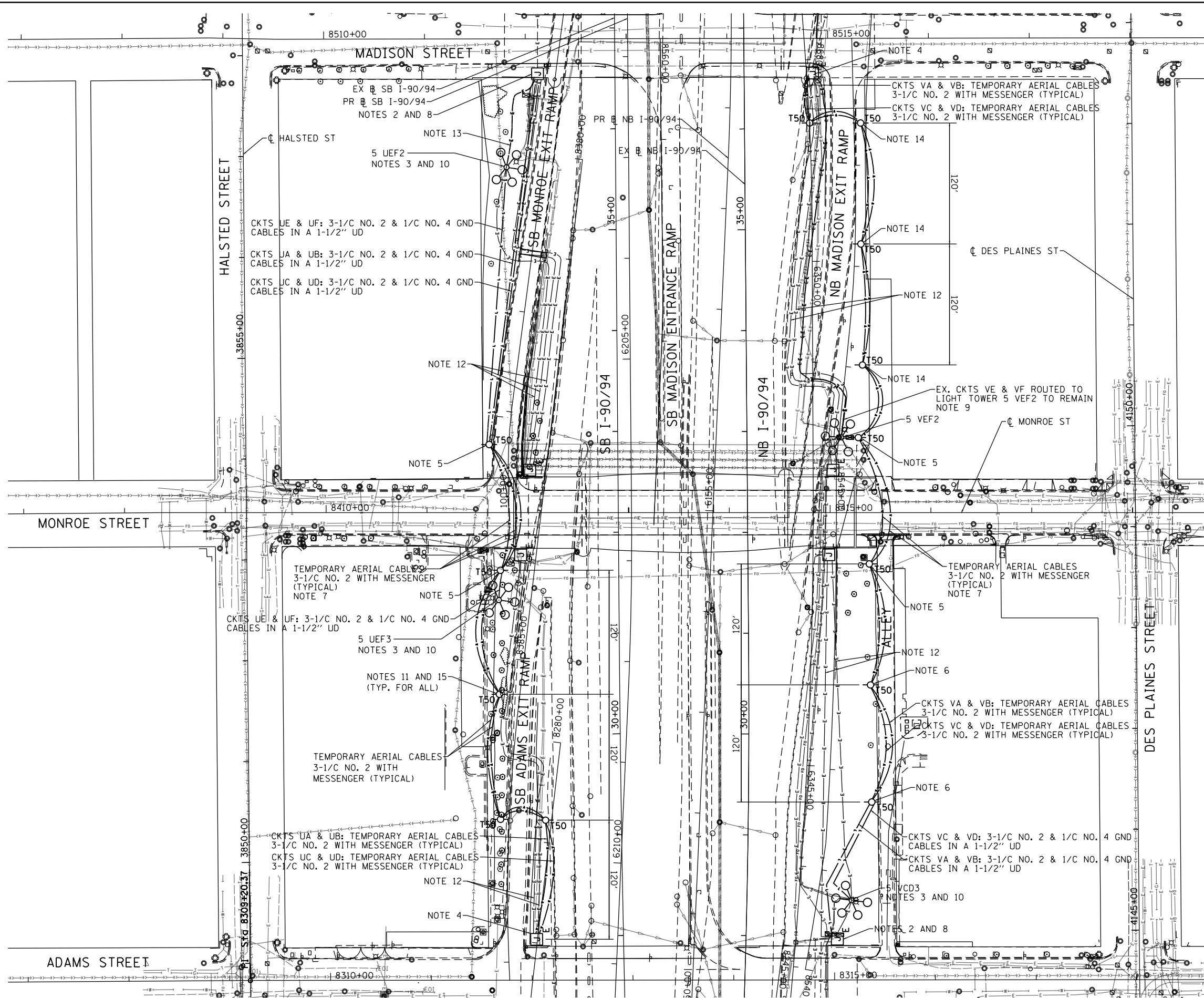
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING DETAILS

SCALE: NONE SHEET NO. 3 OF 3 SHEETS STA. TO STA.

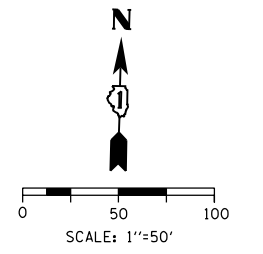
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|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 130 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE PATH = p:\617479-PM\INT\rescom\line\local\AECOM\DS02_NA\Documents\01_Americas\T\engor\station\602694238_Circle\Phase_11\000_CAD\016_Electrical\Sheets\60X95_Sht-Light-02



NOTES:

1. SEE DRAWING E-01 FOR IDOT ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. ROUTE NEW UNIT DUCT UP INTO THE EXISTING CONDUITS ATTACHED TO STRUCTURE INTO THE EXISTING JUNCTION BOX ATTACHED TO STRUCTURE AND SPLICE THE TEMPORARY CIRCUIT CABLES TO EXISTING LIGHTING CIRCUITS LOCATED WITHIN THE JUNCTION BOX.
3. ROUTE NEW UNIT DUCT INTO EXISTING LIGHT TOWER THROUGH THE EXISTING CONDUIT SLEEVE IN THE CONCRETE FOUNDATION AND CONNECT THE NEW LIGHTING CIRCUIT CABLES TO LIGHT TOWER LIGHTING CABLES.
4. SEE IDOT STANDARD BE-801 FOR THE "AERIAL CABLE ATTACHED TO STRUCTURE" DETAIL FOR ROUTING THE AERIAL CABLES INTO THE JUNCTION BOX ATTACHED TO STRUCTURE. CONNECT THE TEMPORARY CABLES TO THE LIGHTING CIRCUITS WITHIN THE JUNCTION BOX.
5. INSTALL WOOD POLES IN THE GENERAL AREAS SHOWN TO PROVIDE A TEMPORARY AERIAL FEED OVER MONROE ST. THE WOOD POLES' INSTALLATION LOCATIONS SHALL BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THE LOCATIONS SHALL BE CHOSEN AND COORDINATED TO AVOID CONFLICTS WITH THE EXISTING SIPHON, BRIDGE, AND ROADWAY RECONSTRUCTION WORK.
6. THE TEMPORARY WOOD POLE SHALL BE INSTALLED APPROXIMATELY 5 FEET WEST OF THE ALLEY.
7. PROVIDE TEMPORARY AERIAL CABLES OVER MONROE ST. AS SHOWN. SEE IDOT STANDARDS BE-800 AND BE-801 FOR TEMPORARY WOOD POLE AND CABLE INSTALLATION DETAILS.
8. REMOVE THE EXISTING LIGHTING CIRCUIT CABLES FROM THE EXISTING VERTICAL CONDUITS ATTACHED TO STRUCTURE PRIOR TO INSTALLING THE TEMPORARY UNIT DUCT WITH CABLES.
9. EXISTING LIGHTING FEED TO LIGHT TOWER 5 VEF2 TO REMAIN AND BE PROTECTED FOR THE DURATION OF THE CONTRACT.
10. DO NOT DISCONNECT THE EXISTING LIGHT TOWER FEEDS UNTIL THE TEMPORARY FEEDS HAVE BEEN COMPLETELY INSTALLED AND ARE READY FOR THE SWITCH OVER.
11. PROVIDE 50 FEET OF SLACK AT EACH TEMPORARY WOOD POLE FOR EACH SET OF AERIAL CABLES TO ALLOW FOR FUTURE ADJUSTMENTS OF THE WOOD POLES IN THE FIELD.
12. EXISTING LIGHTING CIRCUITS TO BE ABANDONED IN PLACE, UNLESS OTHERWISE NOTED.
13. EXISTING LIGHTING FEED TO LIGHT TOWER 5 UEF2 TO REMAIN AND BE PROTECTED FOR THE DURATION OF THE CONTRACT.
14. THE TEMPORARY WOOD POLE SHALL BE INSTALLED APPROXIMATELY 10 FEET WEST OF THE PARKING LOT.
15. COORDINATE THE FINAL INSTALLATION LOCATION OF THE TEMPORARY LIGHTING WOOD POLES WITH THE LOCATIONS OF THE ITS AND COMCAST TEMPORARY WOOD POLES TO AVOID ANY CONFLICTS.



E-02



D160X95-Sht-Light-02
 USER NAME = myersc
 PLOT SCALE = 100.0000' / in.
 PLOT DATE = 4/26/2017

| | |
|-----------------|-----------|
| DESIGNED - PFD | REVISED - |
| DRAWN - CAM | REVISED - |
| CHECKED - WDS | REVISED - |
| DATE - 05/02/17 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

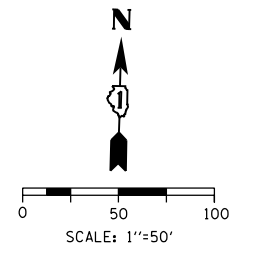
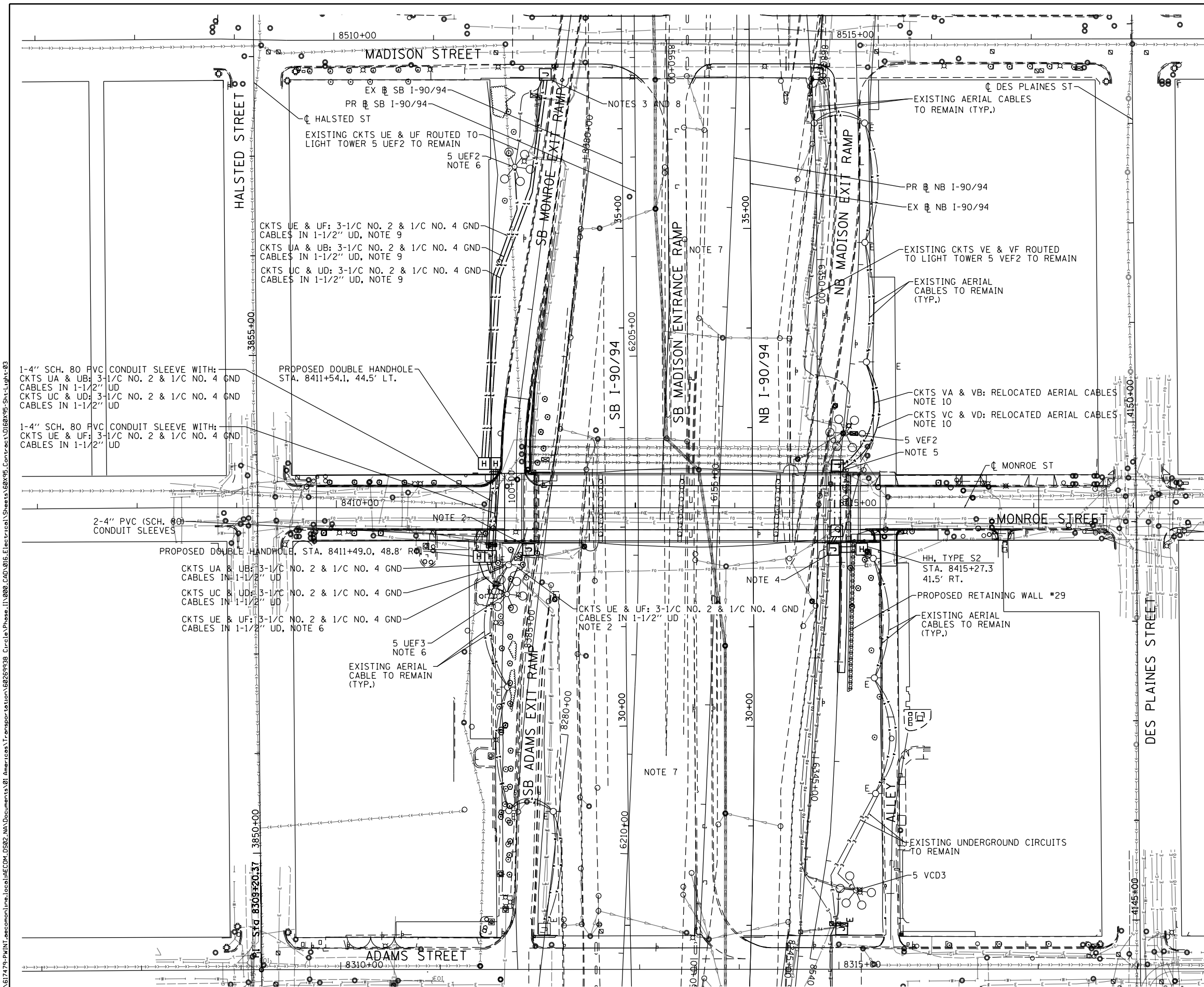
EXISTING/TEMPORARY IDOT LIGHTING PLAN MONROE STREET

SCALE: 1"=50' SHEET 2 OF 16 SHEETS STA. 8409+20 TO STA. 8418+00

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 132 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES:

1. SEE DRAWING E-01 FOR IDOT ELECTRICAL SYMBOLS AND ABBREVIATIONS.
2. ROUTE THE NEW UNIT DUCT UP THE VERTICAL CONDUIT ATTACHED TO STRUCTURE INTO THE NEW JUNCTION BOX ATTACHED TO STRUCTURE. CONNECT LIGHTING CIRCUITS WITHIN JUNCTION BOX. SEE DRAWING E-04 FOR CONTINUATION OF THE CIRCUIT.
3. ROUTE THE NEW UNIT DUCT INTO THE EXISTING CONDUIT UP TO THE EXISTING JUNCTION BOX ATTACHED TO STRUCTURE. CONNECT THE NEW CABLES TO THE EXISTING LIGHTING CIRCUITS WITHIN THE JUNCTION BOX. REMOVE THE TEMPORARY LIGHTING CIRCUIT CABLES FROM THE CONDUIT PRIOR TO INSTALLING THE NEW LIGHTING CIRCUITS.
4. SEE THE CONDUIT INSTALLATION DETAIL ON SHEET E-08.
5. SEE IDOT STANDARD BE-801 FOR THE "AERIAL CABLE ATTACHED TO STRUCTURE" DETAIL FOR ROUTING THE AERIAL CABLES INTO THE JUNCTION BOX ATTACHED TO STRUCTURE. CONNECT THE TEMPORARY CABLES TO THE LIGHTING CIRCUITS WITHIN THE JUNCTION BOX.
6. ROUTE NEW UNIT DUCT INTO THE EXISTING LIGHT TOWER THROUGH THE EXISTING CONDUIT SLEEVE IN THE CONCRETE FOUNDATION AND CONNECT THE NEW LIGHTING CIRCUIT CABLES TO THE EXISTING LIGHT TOWER CABLES.
7. THE EXISTING UTILITIES ARE NOT SHOWN ON THIS SHEET FOR CLARITY TO SHOW THE NEW ELECTRICAL WORK MORE CLEARLY, THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL NEW ELECTRICAL WORK WITH THE EXISTING AND PROPOSED UTILITIES IN THIS AREA. SEE DRAWING E-02 FOR THE LOCATION OF ALL THE EXISTING UTILITIES.
8. THE REMOVAL OF THE EXISTING UNIT DUCT SHALL BE PAID FOR BY THE "REMOVE EXISTING CABLE" PAY ITEM.
9. THE NEW UNIT DUCT SHALL BE INSTALLED A MAXIMUM OF 5 FEET EAST OF THE IDOT RIGHT OF WAY LINE.
10. REUSE AND RELOCATE THE EXISTING AERIAL CABLES AS SHOWN. REMOVE THE TEMPORARY WOOD POLE JUST NORTH OF MONROE STREET AND REROUTE THE AERIAL CABLES TO THE NEW JUNCTION BOX MOUNTED ON THE NORTH SIDE OF THE EAST ABUTMENT WINGWALL. THE WORK TO RELOCATE THE EXISTING AERIAL CABLES WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THE "AERIAL CABLE, 3-1/C NO. 2 WITH MESSENGER WIRE" PAY ITEM.



E-03

FILE PATH = p:\617479-PM\INT.\p\COM\0502_M\Documents\01_Americas\T\engp\station\60269438_Circle\Phase_11\000_CAD\016_Electrical\Sheets\60x95_Sht-Light-03



D160X95-Sht-Light-03
 USER NAME = myersc
 PLOT SCALE = 100.0000' / in.
 PLOT DATE = 4/26/2017

| | |
|-----------------|-----------|
| DESIGNED - PFD | REVISED - |
| DRAWN - CAM | REVISED - |
| CHECKED - WDS | REVISED - |
| DATE - 05/02/17 | REVISED - |

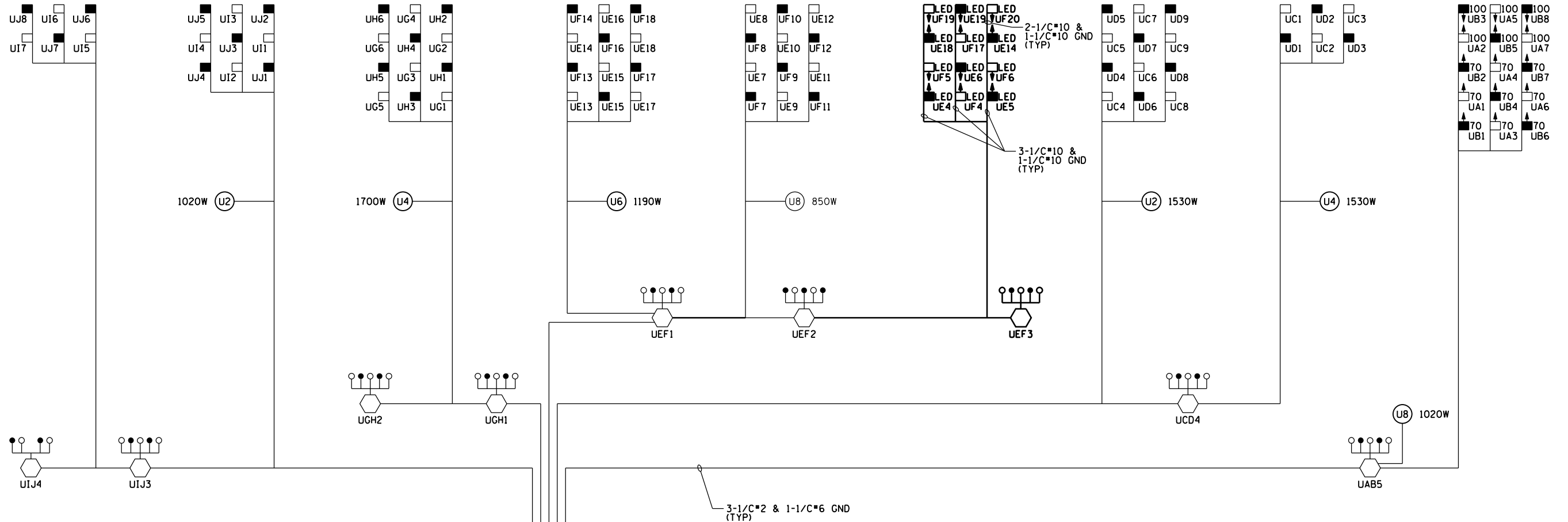
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PROPOSED IDOT LIGHTING PLAN MONROE STREET

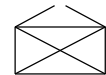
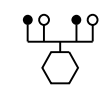

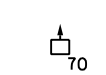
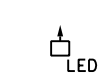

SCALE: 1"=50' SHEET 3 OF 16 SHEETS STA. 8409+20 TO STA. 8418+00

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 133 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

C FULTON STREET C LAKE STREET C RANDOLPH STREET C WASHINGTON STREET C MADISON STREET C MONROE STREET C ADAMS STREET C JACKSON STREET C VAN BUREN STREET



SYMBOLS LEGEND

-  IDOT LIGHTING CONTROLLER
-  HIGH MAST LIGHT TOWER
400 WATT HPS LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  UNDERPASS LIGHTING UNIT
55 WATT LPS LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  UNDERPASS LIGHTING UNIT
70 OR 100 WATT HPS LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  UNDERPASS LIGHTING UNIT
LED LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  EXISTING SIGN STRUCTURE

LIGHTING CONTROLLER 'U'
 240/480, 1 PHASE, 3 WIRE
 (2)-175A, 2-POLE MAIN BREAKERS
 (2)-200A, 2-POLE CONTACTORS
 (16)-70A, 1-POLE, BRANCH BREAKERS

| LOAD TABLE LIGHTING CONTROLLER "U" | | | | | |
|---------------------------------------|-----------|-------|---------|-------------|-------|
| CIRCUIT | RED PHASE | | CIRCUIT | BLACK PHASE | |
| | AMPS | WATTS | | AMPS | WATTS |
| A | 11.4 | 2736 | B | 10.2 | 2448 |
| C | 23.2 | 5555 | D | 21.3 | 5099 |
| E | 36.0 | 8630 | F | 34.1 | 8174 |
| G | 18.8 | 4507 | H | 15.0 | 3595 |
| I | 21.6 | 5186 | J | 20.9 | 5026 |
| K | - | - | L | - | - |
| M | - | - | N | - | - |
| O | - | - | P | - | - |
| TOTAL | 110.9 | 26615 | TOTAL | 101.9 | 24445 |



E-05

FILE PATH = p:\6179-PMINT\pccom\line\local\AECOM\0502\MA\Documents\01_Americas\T\engor\station\60269438_Circle\Phase_11\000_CAD\016_Electrical\Sheets\60x95_Contract\0160x95_sht-Light-05



| | | |
|-----------------------------|-----------------|-----------|
| D160x95-sht-Light-05 | DESIGNED - PFD | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |







STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IDOT LIGHTING CONTROLLER 'U'
 WIRING DIAGRAM

SCALE: N.T.S. SHEET 5 OF 16 SHEETS STA. TO STA.

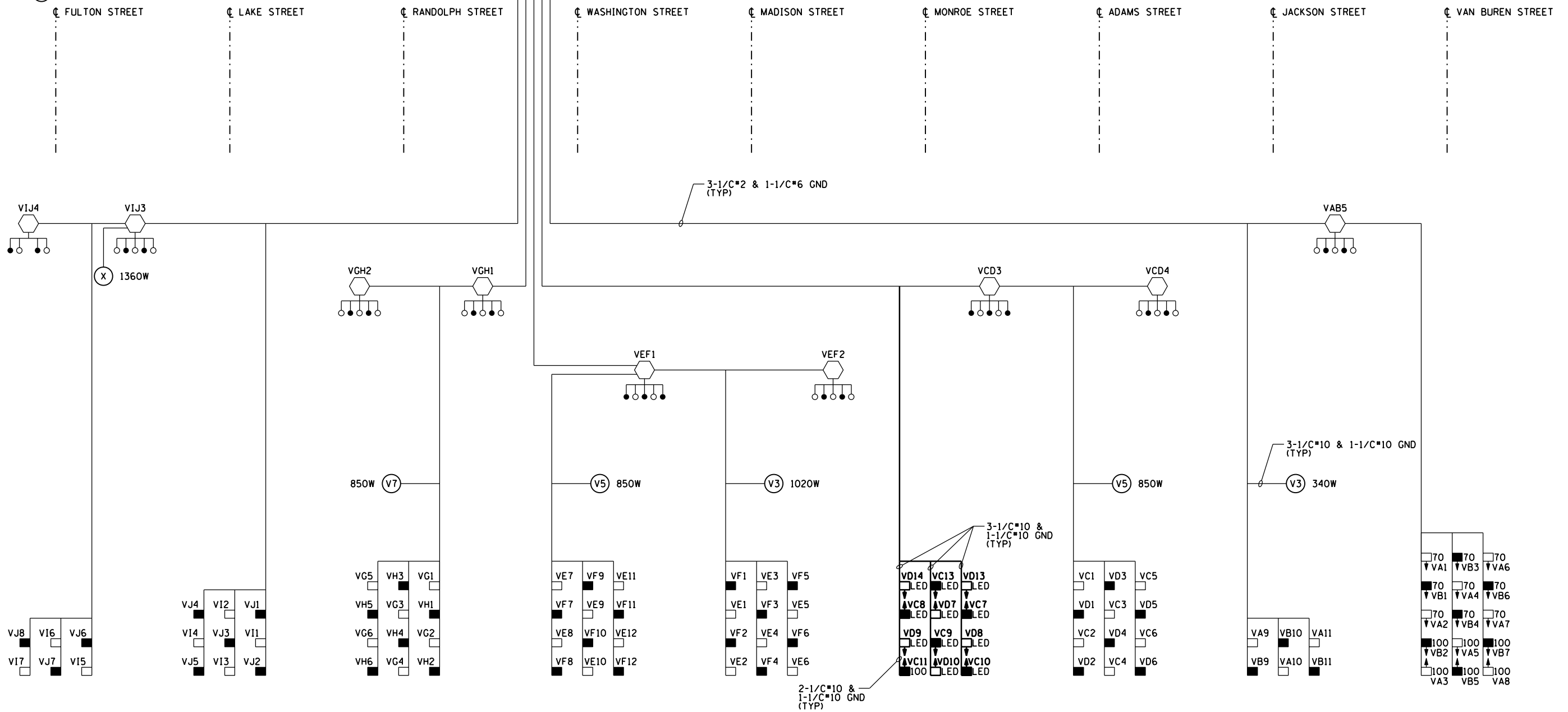
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|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 135 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

SYMBOLS LEGEND

-  IDOT LIGHTING CONTROLLER
-  HIGH MAST LIGHT TOWER
400 WATT HPS LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  UNDERPASS LIGHTING UNIT
55 WATT LPS LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  UNDERPASS LIGHTING UNIT
70 OR 100 WATT HPS LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  UNDERPASS LIGHTING UNIT
LED LUMINAIRE
(BLACK PHASE - SOLID SYMBOL
RED PHASE - OPEN SYMBOL)
-  EXISTING SIGN STRUCTURE

LIGHTING CONTROLLER 'V'
240/480, 1 PHASE, 3 WIRE
(2)-175A, 2-POLE MAIN BREAKERS
(2)-200A, 2-POLE CONTACTORS
(16)-70A, 1-POLE, BRANCH BREAKERS

| LOAD TABLE LIGHTING CONTROLLER "V" | | | | | |
|---------------------------------------|-----------|-------|---------|-------------|-------|
| CIRCUIT | RED PHASE | | CIRCUIT | BLACK PHASE | |
| | AMPS | WATTS | | AMPS | WATTS |
| A | 13.6 | 3274 | B | 11.7 | 2818 |
| C | 22.9 | 5491 | D | 22.9 | 5491 |
| E | 28.2 | 6758 | F | 28.2 | 6758 |
| G | 27.9 | 6703 | H | 24.1 | 5791 |
| I | 20.9 | 5027 | J | 20.3 | 4866 |
| K | - | - | L | - | - |
| M | - | - | N | - | - |
| O | - | - | P | - | - |
| TOTAL | 114.0 | 27356 | TOTAL | 107.2 | 25724 |



E-06

FILE PATH = p:\61779-PM\INT.dwg\AECOM\0502\MA\Documents\01_Americas\T_engorg\station\60261938_Circle\Phase_11\000_Cad\016_Electrical\Sheets\60X95_sht-Light-06



| | | |
|-----------------------------|-----------------|-----------|
| D160X95-sht-Light-06 | DESIGNED - PFD | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|--|----------------------|------|---------|
| IDOT LIGHTING CONTROLLER 'V' WIRING DIAGRAM | | | |
| SCALE: N.T.S. | SHEET 6 OF 16 SHEETS | STA. | TO STA. |

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FA1 90/94 | 2014-016R&B | COOK | 378 | 136 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE PATH = p:\6179-PMINT.aecom\line\local\AECOM_D902_NA\Documents\01_Americas\T_engpor\station\60269438_Circle\Phase_1\1000_CAD\016_Electrical\Sheets\60X95_Contract\0160X95-sht-Light-07

SHEET LEFT INTENTIONALLY BLANK

E-07



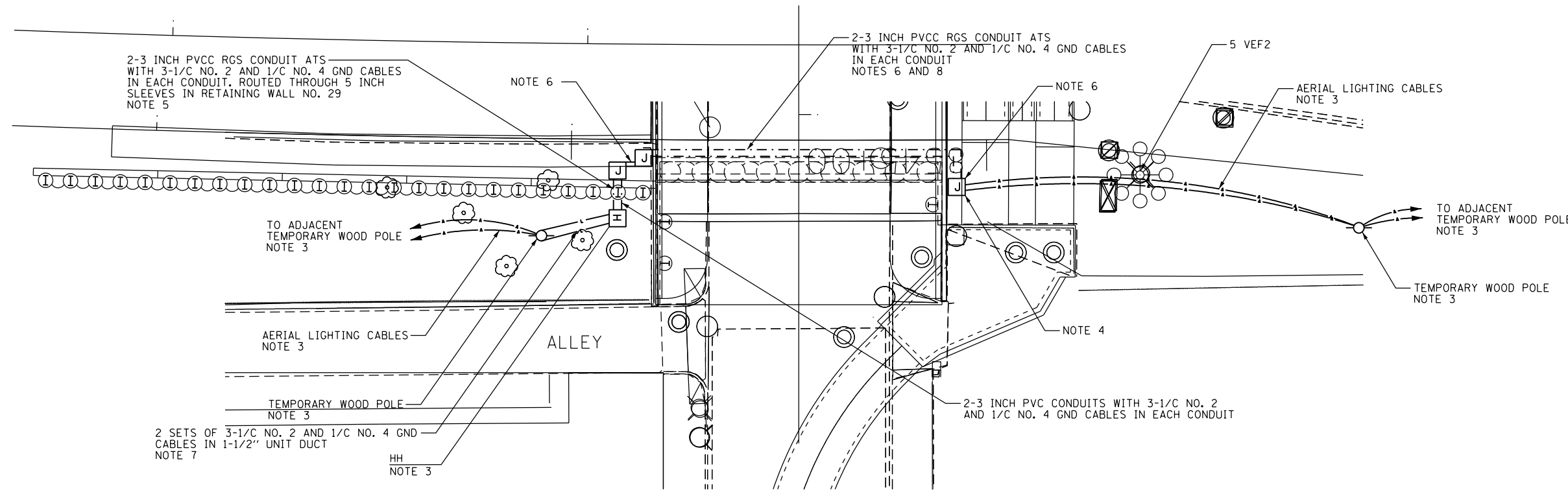
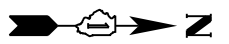
| | | |
|----------------------------|-----------------|-----------|
| D160X95-sht-Light-07 | DESIGNED - PFD | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 8/29/2017 | DATE - 9/1/2017 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS ELECTRICAL DETAILS

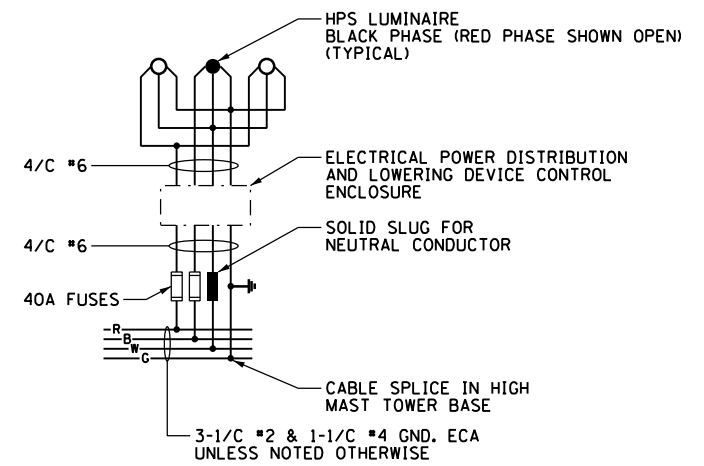
SCALE: N.T.S. SHEET 7 OF 16 SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-------------|--------|---------------------------|-----------|
| FAI 90/94 | 2014-016R&B | COOK | 378 | 137 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

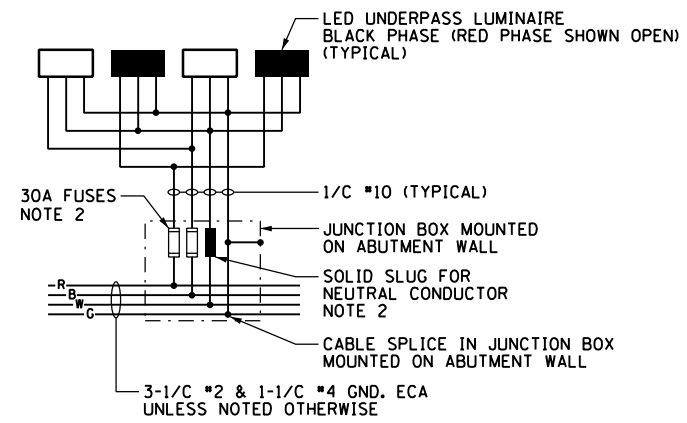


CONDUIT INSTALLATION DETAIL - MONROE ST. EAST ABUTMENT
NOT TO SCALE

- NOTES:**
- SEE DRAWING E-01 FOR IDOT ELECTRICAL SYMBOLS AND ABBREVIATIONS.
 - THE FUSES, FUSE HOLDERS, AND SOLID SLUGS SHALL BE PROVIDED ACCORDING TO ARTICLE 1065.01 OF THE IDOT STANDARDS. THE COST OF PROVIDING THE FUSES, FUSE HOLDERS, AND SOLID SLUGS IN THE JUNCTION BOX WILL NOT BE PAID FOR SEPARATELY AND WILL BE INCLUDED IN THE COST OF THE JUNCTION BOX IN WHICH THEY ARE INSTALLED.
 - SEE DRAWING E-03 FOR PROPOSED LIGHTING PLANS, THE CONTINUATION OF THE LIGHTING CIRCUITS AND ADDITIONAL DETAILS.
 - SEE IDOT STANDARD BE-801 FOR THE "AERIAL CABLE ATTACHED TO STRUCTURE" DETAIL FOR ROUTING THE AERIAL CABLES INTO THE JUNCTION BOX ATTACHED TO STRUCTURE. CONNECT THE TEMPORARY CABLES TO THE LIGHTING CIRCUITS WITHIN THE JUNCTION BOX.
 - SEE STRUCTURAL PLANS FOR THE LOCATION OF THE CONDUIT SLEEVES IN RETAINING WALL NO. 29.
 - SEE DRAWING E-04 SHOWING THE UNDERPASS LIGHTING SYSTEM AND ADDITIONAL DETAILS REGARDING THE JUNCTION BOXES AND CONDUITS.
 - SEE DRAWING E-09 FOR DETAILS ON ROUTING TEMPORARY UNIT DUCT UP A TEMPORARY WOOD POLE.
 - THE 3-INCH CONDUITS ATTACHED TO THE EAST ABUTMENT SHALL BE INSTALLED A MAXIMUM OF 5 FEET BELOW THE TOP OF THE ABUTMENT WALL. COORDINATE THE LOCATION OF THE JUNCTION BOXES AND CONDUITS WITH THE INSTALLATION OF THE UNDERPASS LIGHTING SYSTEM SHOWN ON DRAWING E-04.



TYPICAL HIGH MAST LIGHT TOWER WIRING DIAGRAM
NOT TO SCALE



TYPICAL UNDERPASS LIGHTING UNIT WIRING DIAGRAM
NOT TO SCALE

FILE PATH = p:\6179-PM\INT\pccommon\line\local\AECOM\0502_MIA\Documents\01_Americas\Tr\engp\station\60269938_Circle\Phase_11\000_CAD\016_Electrical\Sheets\60x95_sht-Light-08



| | | |
|-----------------------------|-----------------|-----------|
| D160X95-sht-Light-08 | DESIGNED - WDS | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 30.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

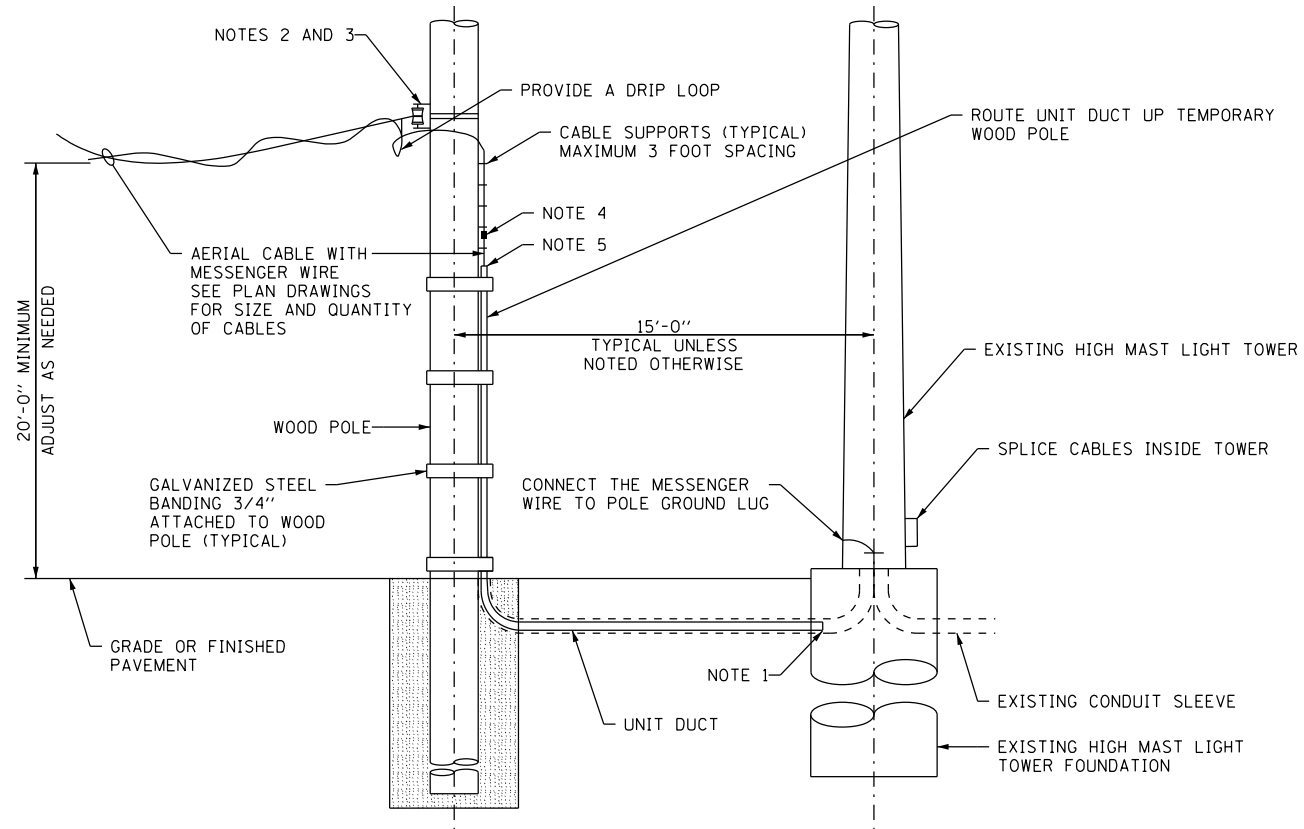
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS ELECTRICAL DETAILS

SCALE: N.T.S. SHEET 8 OF 16 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 138 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE PATH = p:\61779-PMINT\pcomon\line\local\AECOM\01_Americas\T\engp\station\60269938_Circle\Phase_11\000_CAD\016_Electrical\Sheets\60X95_Contract\0160X95-sht-Light-09



NOTES:

1. ROUTE UNIT DUCT INTO LIGHT TOWER CONDUIT SLEEVE.
2. ALL WORK TO FURNISH AND INSTALL ALL HARDWARE REQUIRED FOR MOUNTING THE AERIAL CABLE TO THE STRUCTURE WILL BE INCLUDED IN THE COST OF THE AERIAL CABLE PAY ITEM.
3. HEAVY DUTY INSULATED PULLEY CLEVIS AND CLAMPS MAY BE SUBSTITUTED WITH OTHER EQUIPMENT AND MOUNTING METHOD UPON APPROVAL FROM THE ENGINEER.
4. SPLICE AERIAL CABLES TO UNIT DUCT CABLES USING WATERTIGHT HEAT SHRINK CAP. SEE IDOT STANDARD BE-702 FOR TYPICAL SPLICE DETAIL.
5. INSTALL SEALANT IN THE UNIT DUCT TO STOP INFILTRATION OF MOISTURE INTO THE VERTICAL RACEWAY.

TEMPORARY CONNECTION TO EXISTING HIGH MAST LIGHT TOWER
SCALE: NOT TO SCALE



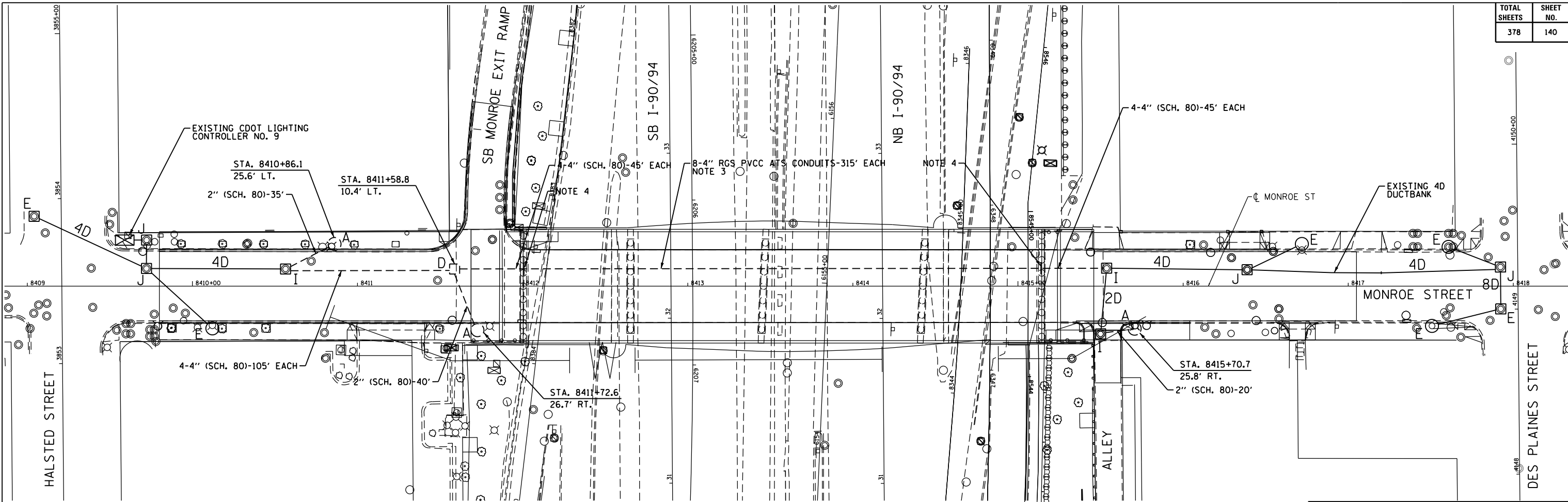
| | | |
|----------------------------|-----------------|-----------|
| D160X95-sht-Light-09 | DESIGNED - WDS | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS ELECTRICAL DETAILS

SCALE: N.T.S. SHEET 9 OF 16 SHEETS STA. TO STA.

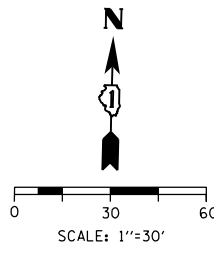
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|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 139 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |



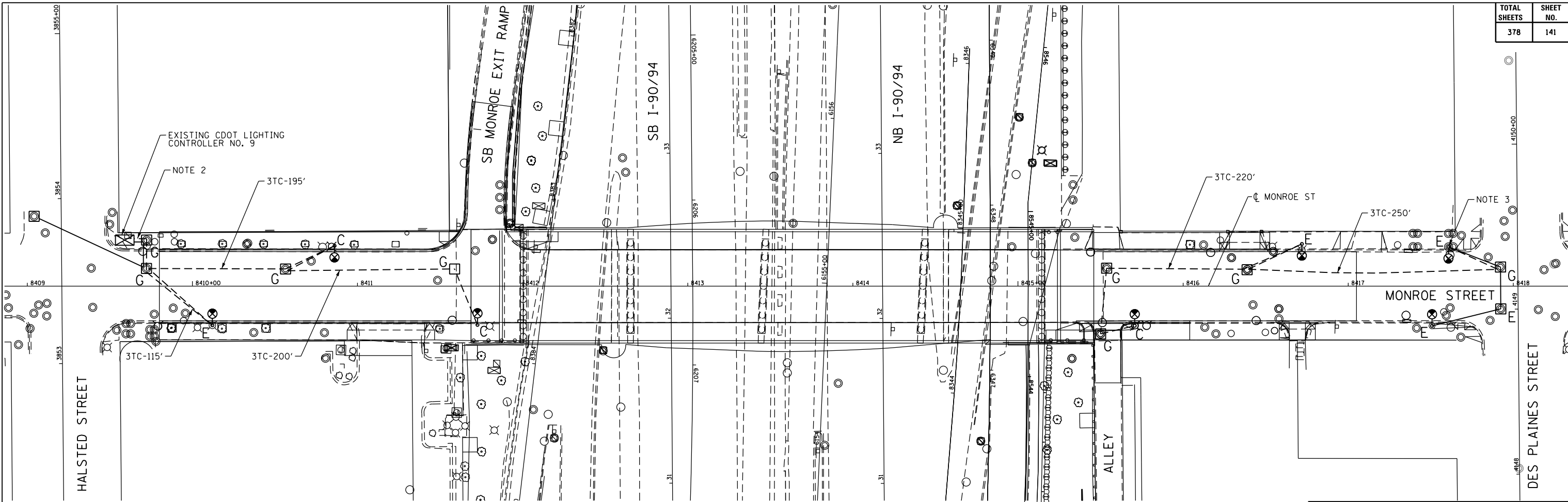
"A" PROVIDE 24"x7", 1-1/4" A.R., 15" B.C. FOUNDATION FOR LIGHT POLE PER DWG. NOS. 818 & 837.
 "D" PROVIDE 3'x4'x4' CONCRETE MANHOLE WITH 30" FRAME AND COVER PER DWG. NOS. 729 & 874.
 "E" EXISTING TO REMAIN
 "I" DRILL/CLEAN EXISTING MANHOLE
 "J" CLEAN MANHOLE

NOTES:

- SEE DRAWING E-13 FOR CDOT ELECTRICAL SYMBOLS AND ABBREVIATIONS.
- ALL STATION AND OFFSETS ARE BASED ON THE PROPOSED MONROE STREET BASELINE.
- FURNISH EIGHT 4" PVC COATED RIGID GALVANIZED CONDUITS (TO BE INSTALLED BY OTHERS).
- INTERCEPT AND CONNECT THE FOUR 4" PVC COATED RIGID GALVANIZED CITY CONDUITS (INSTALLED BY OTHERS) TO THE FOUR 4" SCHEDULE 80 CITY CONDUITS IN GRADE.



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| | |
| DATE | REVISION |
| CITY OF CHICAGO | |
| DEPARTMENT OF TRANSPORTATION DIVISION OF ENGINEERING | |
| CONSULTANT: AECOM <small>303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-5276 PHONE: (312) 373-7700 FAX: (312) 373-6800</small> | |
| WORK ORDER NO. _____ | DATE _____ |
| COST ALLOCATION ACCOUNT _____ | |
| APPROPRIATION ACCOUNT _____ | MATERIAL _____ LABOR _____ |
| MONROE STREET CONDUIT AND FOUNDATION PLAN | |
| CITY OF CHICAGO DEPT. OF TRANSPORTATION DIVISION OF ELECTRICAL OPERATIONS | |
| DRAFTSMAN: CAM | ENGINEER: PFD |
| SUPERVISING ENGINEER: WDS | ELEC. DESIGN ENGR. |
| ENGINEER OF ELECTRICITY: | E-10 |
| GEN'L SUPT. OF ELECTRICITY: | |
| DEPUTY COMMISSIONER: | |
| SIZE: 22" 34" | |
| DATE: 9/1/2017 | DATE: 9/1/2017 |
| C.D.O.T. PROJECT NO.: | DWG. NO.: 10 OF 16 |



"C" RE-ERECT EXISTING LIGHTING UNIT ON A NEW FOUNDATION
 "E" EXISTING TO REMAIN
 "G" RACK CABLES IN MANHOLE

NOTES:

- SEE DRAWING E-13 FOR CDOT ELECTRICAL SYMBOLS AND ABBREVIATIONS.
- CONNECT PROPOSED TRIPLEX CABLES TO THE EXISTING CDOT LIGHTING CONTROLLER NO. 9.
- CONNECT PROPOSED TRIPLEX CABLES TO THE EXISTING LIGHTING CABLES WITHIN THE BASE OF THE LIGHT POLE.



| DATE | REVISION |
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CITY OF CHICAGO
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF ENGINEERING

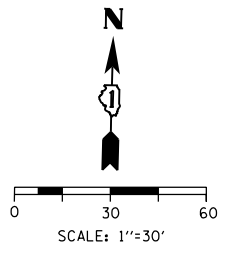
CONSULTANT:
AECOM
303 EAST WACKER DRIVE, SUITE 1400
 CHICAGO, IL 60601-5276
 PHONE: (312) 373-7700 FAX: (312) 373-6800

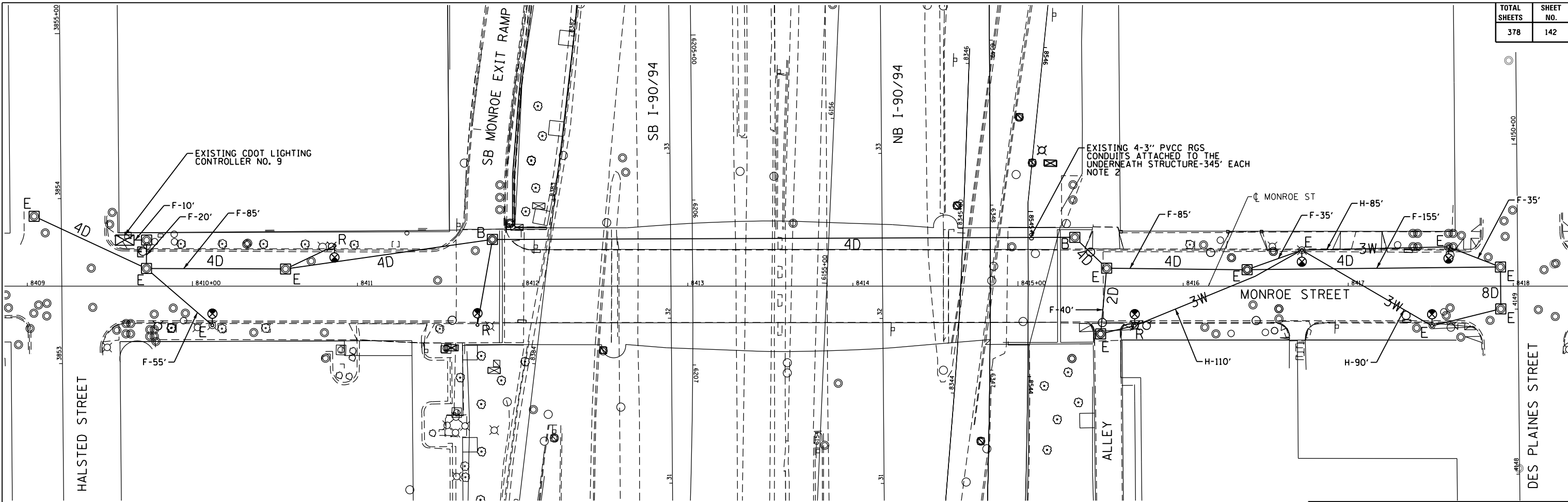
WORK ORDER NO. _____ DATE _____
 COST ALLOCATION ACCOUNT _____
 APPROPRIATION ACCOUNT MATERIAL _____ LABOR _____

MONROE STREET
 LIGHTING CABLE AND
 EQUIPMENT INSTALLATION PLAN

CITY OF CHICAGO
 DEPT. OF TRANSPORTATION
 DIVISION OF ELECTRICAL OPERATIONS

| | | |
|--|-------------------------|------------------|
| DRAFTSMAN: CAM | CHIEF DRAFTSMAN: WDS | ENGINEER: PFD |
| SUPERVISING ENGINEER: ELEC. DESIGN ENGR. WDS | | E-11 |
| ENGINEER OF ELECTRICITY: | | |
| GEN'L SUPT. OF ELECTRICITY: | | |
| DEPUTY COMMISSIONER: | | |

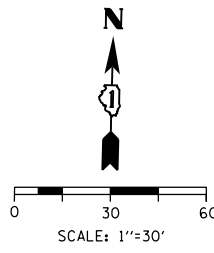




"B" BREAK DOWN EXISTING MANHOLE
 "R" REMOVE EXISTING STEEL POLE, LUMINAIRE AND MAST ARM. BREAK DOWN FOUNDATION COMPLETE
 "E" EXISTING TO REMAIN
 "F" REMOVE EXISTING LIGHTING CABLES IN CONDUIT. ROD AND CLEAN CONDUIT.
 "H" REMOVE AERIAL CABLE

NOTES:

- SEE DRAWING E-13 FOR CDOT ELECTRICAL SYMBOLS AND ABBREVIATIONS.
- THE EXISTING CDOT CONDUITS ATTACHED TO THE UNDERSIDE OF THE BRIDGE STRUCTURE MAY CONTAIN ASBESTOS MATERIALS. THE CONDUITS SHALL BE REMOVED AND DISPOSED OF PROPERLY PRIOR TO THE BRIDGE DEMOLITION, CONDUITS TESTED AND CONFIRMED TO CONTAIN ASBESTOS MUST BE REMOVED IN ACCORDANCE WITH THE SPECIAL PROVISION "REMOVAL OF ASBESTOS CEMENT CONDUIT". ASBESTOS CONDUIT REMOVAL SHALL BE PAID FOR UNDER THE "REMOVAL OF ASBESTOS CEMENT CONDUIT" PAY ITEM.



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| | |
| DATE | REVISION |
| CITY OF CHICAGO | |
| DEPARTMENT OF TRANSPORTATION DIVISION OF ENGINEERING | |
| CONSULTANT: AECOM <small>303 EAST WACKER DRIVE, SUITE 1400 CHICAGO, IL 60601-5276 PHONE: (312) 373-7700 FAX: (312) 373-6800</small> | |
| WORK ORDER NO. _____ | DATE _____ |
| COST ALLOCATION ACCOUNT _____ | |
| APPROPRIATION ACCOUNT _____ | MATERIAL _____ LABOR _____ |
| MONROE STREET LIGHTING REMOVAL PLAN | |
| CITY OF CHICAGO DEPT. OF TRANSPORTATION DIVISION OF ELECTRICAL OPERATIONS | |
| DRAFTSMAN: CAM | CHIEF DRAFTSMAN: PFD |
| SUPERVISING ENGINEER: WDS | ELEC. DESIGN ENGR. |
| ENGINEER OF ELECTRICITY: | |
| GEN'L SUPT. OF ELECTRICITY: | |
| DEPUTY COMMISSIONER: | |
| SIZE: 22" 34" | SCALE: 1" = 30' |
| DATE: 05/02/17 | DATE: 05/02/17 |
| C.D.O.T. PROJECT NO.: | DWG. NO.: 12 OF 16 |

E-12

FILE PATH = p:\6179-PMINT\pccommon\line\local\AECOM\0522\MA\Documents\01\Americas\Tran\engor\station\60269938 Circle\Phase_11\000_CAD\016_Electrical\Sheets\60x95_sht-Light-13

| PROPOSED | PRESENT | |
|----------|---------|---|
| | | SIGNAL, TRAFFIC 3 SECTION 1-WAY ADJUSTABLE, 12" OR AS NOTED |
| | | SIGNAL, TRAFFIC 3 SECTION 2-WAY ADJUSTABLE, 12" OR AS NOTED |
| | | SIGNAL OPTICALLY PROGRAMMED |
| | | SIGNAL, PEDESTRIAN, DON'T WALK/WALK |
| | | SIGNAL FACE ARROW, 12" COLOR AS NOTED |
| | | SIGNAL FACE, 1 SECTION YELLOW/GREEN ARROW DUAL INDICATION |
| | | PUSH BUTTON, PEDESTRIAN |
| | | SIGN, ILLUMINATED, WITH MESSAGE OR SYMBOL AS INDICATED |
| | | MAST ARM, MONOTUBE, STEEL. SIZE AS INDICATED (SEE DWG. #870) |
| | | MAST ARM, TRUSS, ALUMINUM. SIZE AS INDICATED |
| | | CONTROLLER, TRAFFIC SIGNAL. PEDESTAL OR BASE MOUNTED AS INDICATED |
| | | CONTROLLER, STREET LIGHTING. PEDESTAL OR BASE MOUNTED. (DWG. 876 or 880) |
| | | CONTROLLER, STREET LIGHTING. POLE MOUNTED (DWG. #11940) |
| | | POLE, WOOD. COMMONWEALTH EDISON COMPANY, SERVICE |
| | | POLE, CITY STEEL, ANCHOR BASE, 34'-6", 7 GA. 10" DI A. AND 15" B.C. 24"X7' FND. W/1 1/4" ANCHOR RODS DRG. #818. |
| | | POLE, CITY STEEL, ANCHOR BASE, 34'-6", 3 GA. 10" DIA. AND 15" B.C. 24"X9' FND. W/1 1/4" ANCHOR RODS DRG. #818 (16', 20' or 26' M.A.) |
| | | POLE, CITY STEEL, ANCHOR BASE, 34'-6", 3 GA., 11" DIA. AND 17 1/4" B.C. 30"X9' FND. W/1 1/4" ANCHOR RODS DRG. #816. (30' M.A.) |
| | | POLE, CITY STEEL, ANCHOR BASE 34'-6", 3 GA. 12 1/2" DIA. AND 16 1/2" B.C. 30"X11' FND. W/1 1/2" ANCHOR RODS DRG.#817. (35", 40' or 44' M.A.) |
| | | POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA. 10" DIA., WITH 3 GA. BAL. HSG. BASE AND 17 1/4" B. C. ON 30"X9' FND. W/ 11/4" ANCHOR RODS DRG. #816. |
| | | POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6", 7 GA. WITH STEEL BAL. HSG. BASE AND FND. W/10" D. B.C. AND 1" ANCHOR RODS DRG. #716. |
| | | POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6", 3 GA., WITH STEEL BAL. HSG. BASE AND FND. W/10" D. B.C. AND 1" ANCHOR RODS DRG.#719. |
| | | POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6" 7 GA., AND FND. WITH 10" B.C. AND 1" ANCHOR RODS DRG. #11408B. |
| | | POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6" 3 GA., AND FND. WITH 10" B.C. AND 1" ANCHOR RODS DRG. #11408B. |
| | | POLE, CITY STEEL, ANCHOR BASE, 32'-6", 7 GA., AND FND. WITH 11 1/2" B.C. AND 1" ANCHOR RODS DRG. #753. |
| | | POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA., AND FND. WITH 11 1/2" B.C. AND 1" ANCHOR RODS DRG. #753. |
| | | POLE, CITY STEEL, ANCHOR BASE, 32'-6", 7 GA., ALUM. BHB AND FND. WITH 15" B.C.-24"X7' WITH 1" ANCHOR RODS DRG. #691. |
| | | POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA., ALUM. BHB AND FND. WITH 15" B.C. 24"X7' WITH 1" ANCHOR RODS DRG. #691. |
| | | POLE, CITY ALUMINUM, WITH ROUND BAL. HSG. BASE, 25', 28', or 30' ON FND. WITH 14" B.C., ACQUIRED FROM CHICAGO PARK DISTRICT. |
| | | POLE, CITY STEEL, EMBEDDED, 4"X 9"X 35' 7 GA., TAPERED TUBULAR. (DWG. #658) |
| | | POLE, CITY STEEL, EMBEDDED, 4"X 9"X 35' 3 GA., TAPERED TUBULAR. (DWG. #658) |
| | | POLE, CITY STEEL, EMBEDDED. (ACQUIRED FROM CTA) |
| | | COLUMN, ELEVATED STRUCTURE |
| | | POLE, WOOD. (SIZE AS NOTED) |
| | | POLE, FOUNDATION WITH ELBOWS AS INDICATED. (SIZE AS NOTED) |
| | | POLE, ORNAMENTAL OR OTHER, AS INDICATED ON THE PLANS |

| PROPOSED | PRESENT | |
|----------|---------|--|
| | | HANDHOLE, HEAVY DUTY, 36" I.D. (DWG.#866) |
| | | HANDHOLE, CIRCULAR WITH 24" FRAME & COVER, 30" I.D. (#867) |
| | | MANHOLE, CITY 3'X4'X4' DWG. #729 or 730; 4'X6'X6' DWG. #732 or 733. |
| | | FOUNDATION, CONTROLLER OR PEDESTAL, 13" B.C., 20"X5' (DWG. #709) |
| | | FOUNDATION, TRAFFIC CONTROLLER DWG. #854. F.A. TERMINAL FND. DWG. #11972 |
| | | FOUNDATION, TRAFFIC TYPE "P", BASE MOUNT. (DWG. #888) |
| | | FOUNDATION, CONTROLLER STREET LIGHT, SPECIAL, 100A & 200A. (DWG.#876 & # 880) |
| | | FOUNDATION, TRANSCLOSURE; TRANSCLOSURE HOUSING. (DWG.# 583 & #891) |
| | | CONTROLLER, UNDERPASS LIGHTING 120V. & 240V. (DWG. #860 & #861) |
| | | MANHOLE, UTILITY, E=COMMONWEALTH EDISON; T=ILL.BELL TEL.; G=PEOPLES GAS; W=CITY WATER; P=CHGO PARK DISTRICT; CTA=C.T.A.; S=SEWER |
| | | JUNCTION BOX, IN PAVEMENT (DWG. #815) |
| | | DETECTOR LOOP IN PAVEMENT |
| | | CONDUIT or P.V.C., NUMBER, SIZE & TYPE. (AS NOTED) |
| | | CONDUIT or P.V.C. ENCASED IN CONCRETE. (SECTION or NUMBER OF CONDUIT INDICATED) |
| | | LUMINAIRE, H.P.S.V. 400W LAMP, 240V, SEMI-CUTOFF |
| | | LUMINAIRE, H.P.S.V. 400W LAMP, 240V, CUTOFF |
| | | LUMINAIRE, H.P.S.V. 310W LAMP, 240V |
| | | LUMINAIRE, H.P.S.V. 310W LAMP 240V, CUTOFF |
| | | LUMINAIRE, H.P.S.V. 150W LAMP, 240V |
| | | LUMINAIRE, H.P.S.V. 150W LAMP, 120V |
| | | LUMINAIRE, H.P.S.V. 250W LAMP, 120V, (ALLEY LIGHT) |
| | | LUMINAIRE, H.P.S.V. 250W LAMP, 120V |
| | | TERMINAL, CABINET F.A. & P.C. |
| | | FIRE ALARM BOX, MOUNTED |
| | | FIRE ALARM BOX, POLE MOUNTED |
| | | CABLE, TRAFFIC SIGNAL, COMMUNICATION, 1-PAIR #14 SHIELDED, IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2/C-#4, 600 V. EPR. IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2 1/C-#2 or #1/0 600V. EPR IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2/C-#10 or #6, 600V NSRI, IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL, 7/C-#12 or #14, 600V, EPR IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL, 10/C-#12 or #14, 600V, EPR IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL, 14/C-#12 600V. EPR IN CONDUIT |
| | | CABLE, TRAFFIC SIGNAL, 19/C-#12 or #14, 600V, EPR IN CONDUIT |
| | | CABLE, STREET LIGHT, 2 1/C-#6, 600V. RINS IN PARKWAY |
| | | CABLE, STREET LIGHT, 2 1/C-#6, 600V. RINS IN CONDUIT |
| | | CABLE, STREET LIGHT, 2 1/C-#6 EPRN 600V. & 1 1/C-#8 GREEN, TRIPLEXED, IN CONDUIT |
| | | CABLE, STREET LIGHT, 3 1/C-#1/0, or #2/0, or #4, 600V. EPR IN CONDUIT |
| | | WIRE, STREET LIGHT, 2 1/C-#6, HDNS. AERIAL |
| | | WIRE, STREET LIGHT, 2 1/C-#6 & 1 1/C #8, HDNS. AERIAL |
| | | CABLE, STREET LIGHT AERIAL, 3 1/C-#4 or #2 SELF SUPPORTING, 600V EPR |
| | | WIRE, F.A. & P.C. AERIAL, 1/C-#10, NUMERAL DENOTES QUANTITY |
| | | CABLE, F.A. & P.C. AERIAL, W/ MESSENGER #19-(NUMBER OF PAIRS AS INDICATED) |
| | | CABLE, F.A. & P.C. AERIAL, SELF SUPPORTING, #19-(NUMBER OF PAIRS AS INDICATED) |
| | | CABLE, F.A. & P.C., IN CONDUIT, #19-(NUMBER OF PAIRS AS INDICATED) |
| | | DOWNLIGHT ASSEMBLY. (DWG. #850) |
| | | LIGHT, TRAFFIC SAFETY ISLAND |
| | | FLASHING BEACON & DOWNLIGHT |

ORNAMENTAL LUMINAIRES

| PROPOSED | EXISTING | |
|----------|----------|---------------------|
| | | 310W PENDANT (240V) |
| | | 400W PENDANT (240V) |
| | | 250W PENDANT (240V) |
| | | 150W ACORN (120V) |
| | | 150W ACORN (240V) |
| | | 50W ACORN (240V) |
| | | 100W ACORN (240V) |
| | | 150W GLOBE (240V) |
| | | 100W GLOBE (240V) |
| | | 50W GLOBE (240V) |

| | | |
|---|-------------------------------|-------------------------------|
| C 04-01-02 | REVISED/REDRAW | R. POOL/B. I. |
| B 12-4-01 | ADDED ORNAMENTAL SYMBOLS | |
| A 8-6-96 | REDRAWN | |
| DATE | REVISION | |
| STANDARD CODE FOR TRAFFIC SIGNALS/ STREET LIGHTING | | |
| CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING | | |
| DRAFTSMAN: R. IVY | CHIEF DRAFTSMAN: R. CARTER | ENGINEER: R. POOL/R.C/W.T. |
| SUPERVISING ENGINEER: | ELEC. DESIGN ENGR. | |
| ENGINEER OF ELECTRICITY: | | DWG. NO. |
| GENL. SUPT. OF CONSTRUCTION: | | 826 |
| DEPUTY COMMISSIONER: | | |
| SIZE: 22" X 36" | SCALE: | DATE: |



| | | |
|----------------------------|-----------------|-----------|
| D160X95-sht-Light-13 | DESIGNED - PFD | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

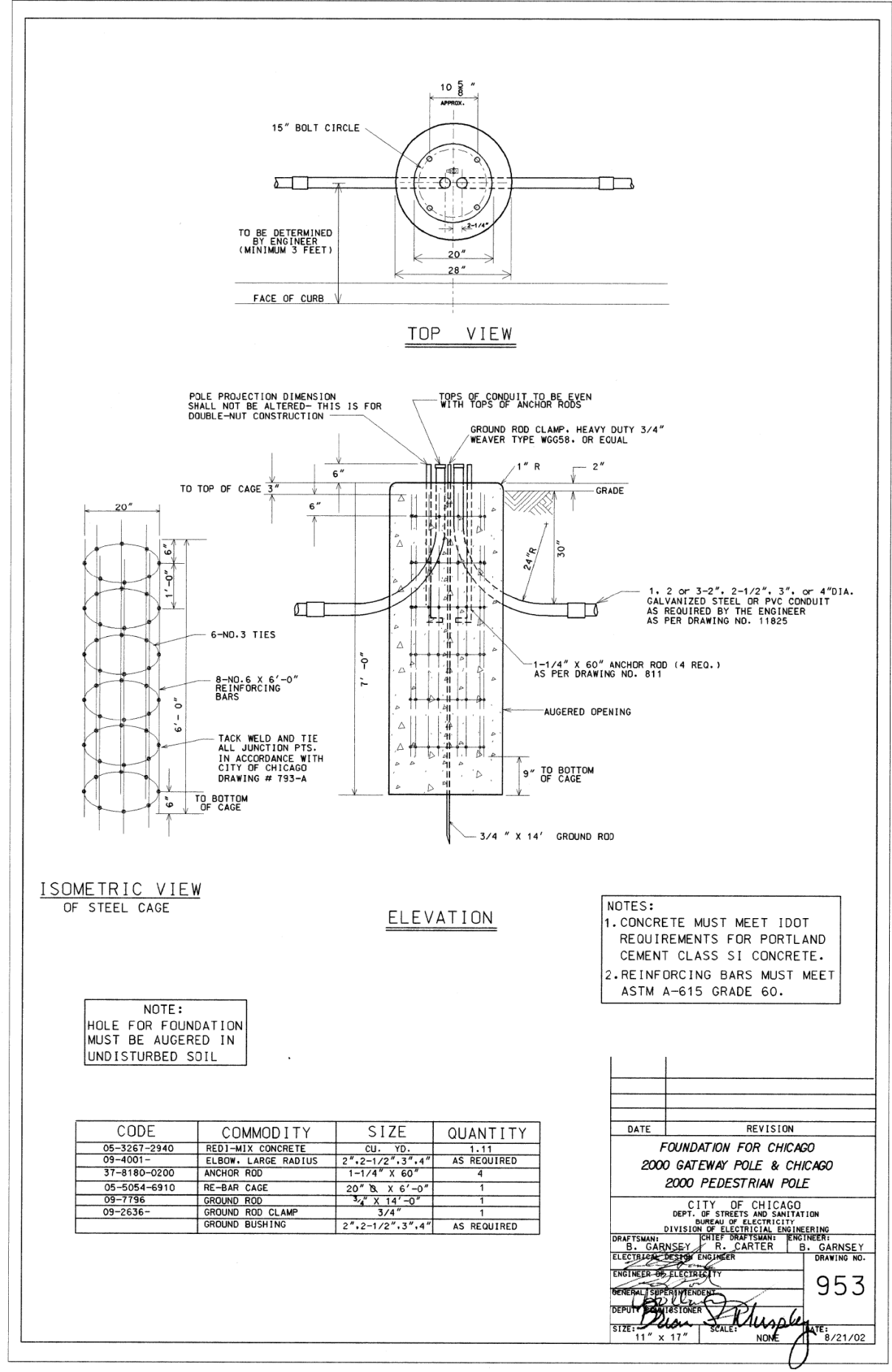
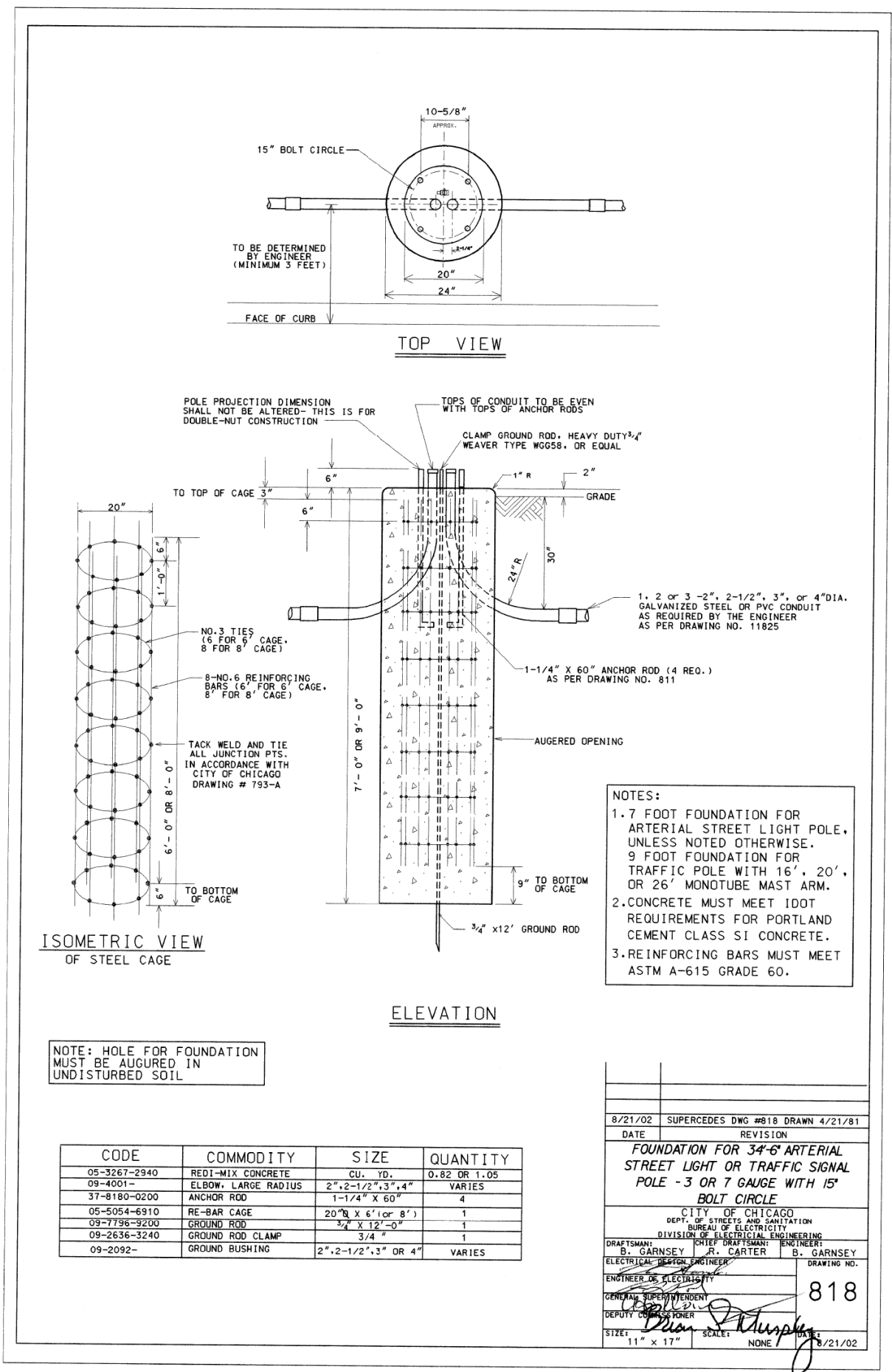
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CDOT DETAILS

SCALE: N.T.S. SHEET 13 OF 16 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 143 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE PATH = p:\617479-PMINT\pccom\line\local\AECOM\0902\MA\Documents\01\Americas\1\Engineering\station\60269938\Circles\Phase_11\000_CAD\016_Electrical\Sheets\60x95_sht-Light-14



| | | |
|----------------------------|-----------------|-----------|
| D160X95-sht-Light-14 | DESIGNED - PFD | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| CDOT DETAILS | |
|---------------|------------------------------------|
| SCALE: N.T.S. | SHEET 14 OF 16 SHEETS STA. TO STA. |

| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| FAI 90/94 | 2014-016R&B | COOK | 378 | 144 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

INSTALLATION NOTES

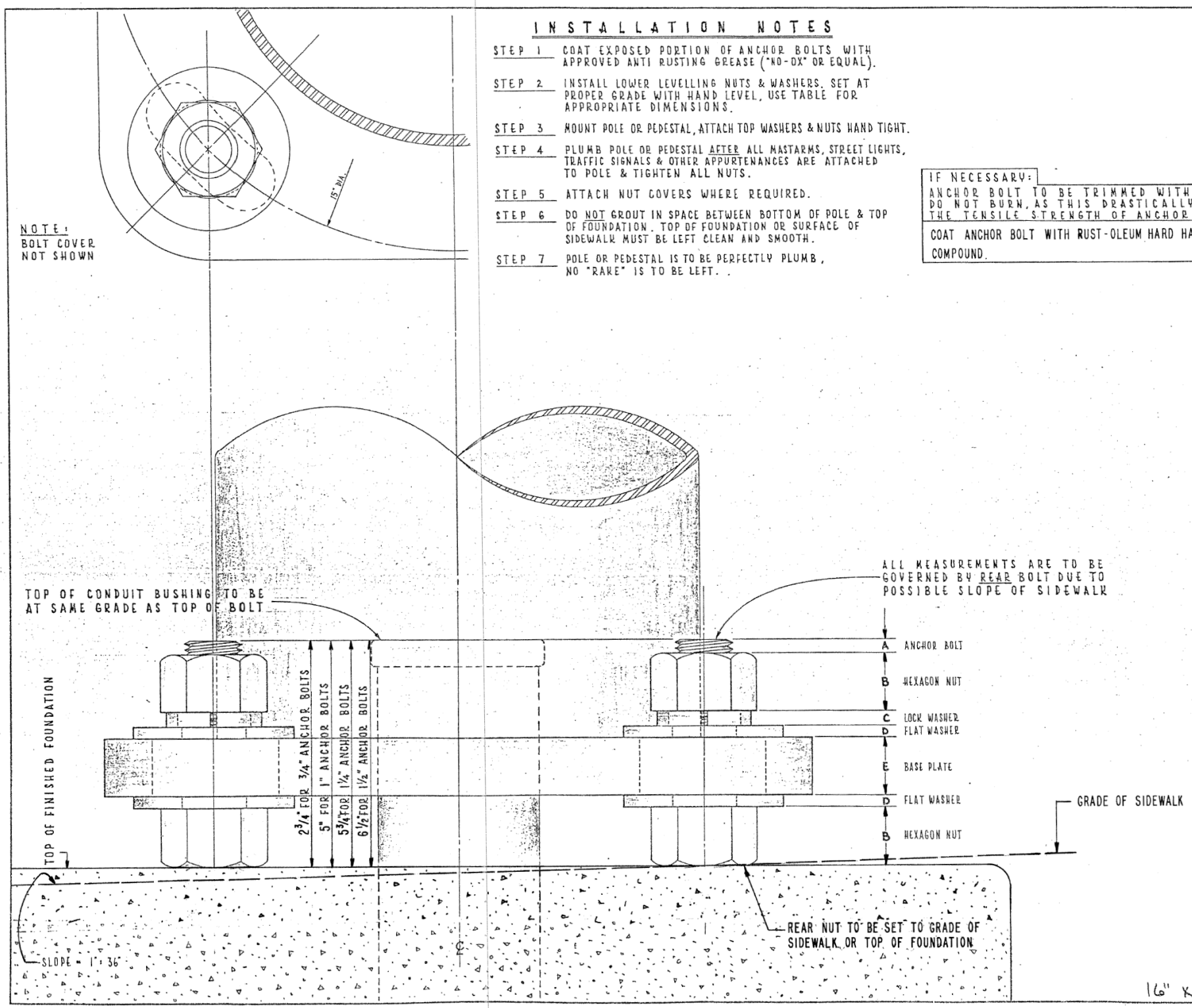
- STEP 1 COAT EXPOSED PORTION OF ANCHOR BOLTS WITH APPROVED ANTI RUSTING GREASE ("NO-DX" OR EQUAL).
- STEP 2 INSTALL LOWER LEVELLING NUTS & WASHERS. SET AT PROPER GRADE WITH HAND LEVEL, USE TABLE FOR APPROPRIATE DIMENSIONS.
- STEP 3 MOUNT POLE OR PEDESTAL, ATTACH TOP WASHERS & NUTS HAND TIGHT.
- STEP 4 PLUMB POLE OR PEDESTAL AFTER ALL MASTARMS, STREET LIGHTS, TRAFFIC SIGNALS & OTHER APPURTENANCES ARE ATTACHED TO POLE & TIGHTEN ALL NUTS.
- STEP 5 ATTACH NUT COVERS WHERE REQUIRED.
- STEP 6 DO NOT GROUT IN SPACE BETWEEN BOTTOM OF POLE & TOP OF FOUNDATION. TOP OF FOUNDATION OR SURFACE OF SIDEWALK MUST BE LEFT CLEAN AND SMOOTH.
- STEP 7 POLE OR PEDESTAL IS TO BE PERFECTLY PLUMB, NO "RAKE" IS TO BE LEFT.

IF NECESSARY:
ANCHOR BOLT TO BE TRIMMED WITH SAW. DO NOT BURN, AS THIS DRASTICALLY REDUCES THE TENSILE STRENGTH OF ANCHOR BOLT.
COAT ANCHOR BOLT WITH RUST-OLEUM HARD HAT ZINC RICH COMPOUND.

T A B L E

| T I M E | ANCHOR BOLT SIZE | | | | POLE GAUGES | | | | ALUM. PED. |
|------------------|------------------|--------|--------|---------|-------------|--------|-------|--|---------------|
| | 1" | 1 1/4" | 1 1/2" | 3/4" | | | | | |
| | 7 GA. | 3 GA. | 7 GA. | 3 GA. | 7 GA. | 3 GA. | | | |
| A | 7/16" | 7/16" | 1 1/8" | 1 5/16" | 1 1/8" | 7/8" | 3/4" | | |
| B | 1" | 1" | 1 1/4" | 1 1/4" | 1 1/2" | 1 1/2" | 3/4" | | |
| C | 1/4" | 1/4" | 5/16" | 5/16" | 3/8" | 3/8" | 3/16" | | |
| D | 5/32" | 5/32" | 1/4" | 1/4" | 1/4" | 1/4" | 5/32" | | |
| E | 1" | 1" | 1 1/4" | 1 1/2" | 1 1/2" | 1 3/4" | 5/8" | | |

NOTE:
BOLT COVER
NOT SHOWN



CONSTRUCTION METHOD FOR "DOUBLE-NUT" INSTALLATION OF POLES AND PEDESTALS

CITY OF CHICAGO
DEPT. OF STREETS AND SANITATION
BUREAU OF ELECTRICITY
DIVISION OF ELECTRICAL ENGINEERING

DRAFTSMAN: EUGENE GERULIS
SUPERVISING ENGINEER: [Signature]
ENGINEER: [Signature]
ELEC. DESIGN ENGR. [Signature]

837

DATE: 6-7-85

FILE PATH = p:\61779-PMINT\pcom\line\local\pcom\01\americas\tr\engor\station\60269438 Circle\Phase_11\000_CAD\016_Electrical\Sheets\60X95_Contract\0160X95-shr-Light-15



| | | |
|----------------------------|-----------------|-----------|
| D160X95-shr-Light-15 | DESIGNED - PFD | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 2.0000' / in. | CHECKED - WDS | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

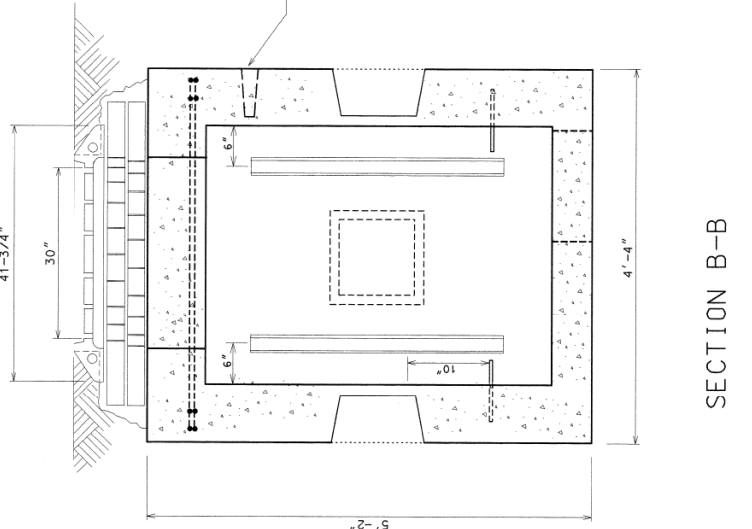
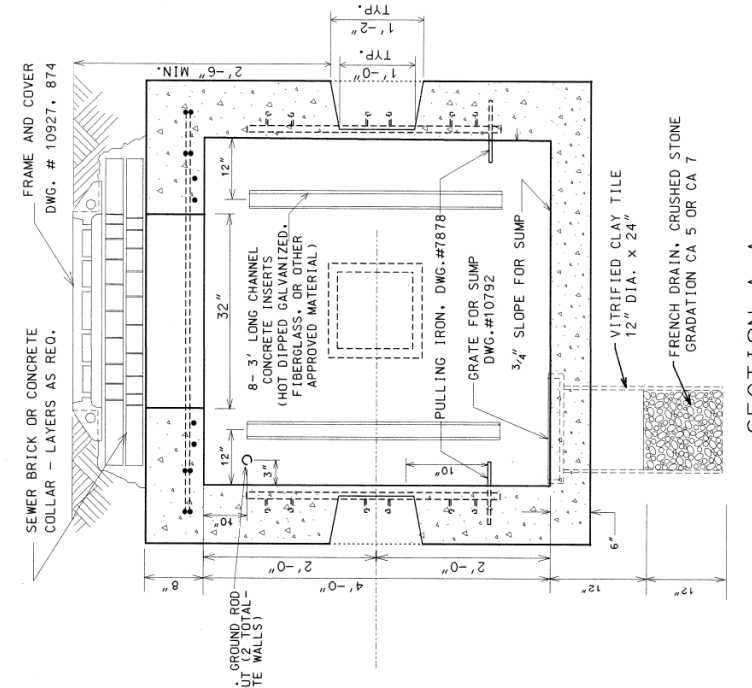
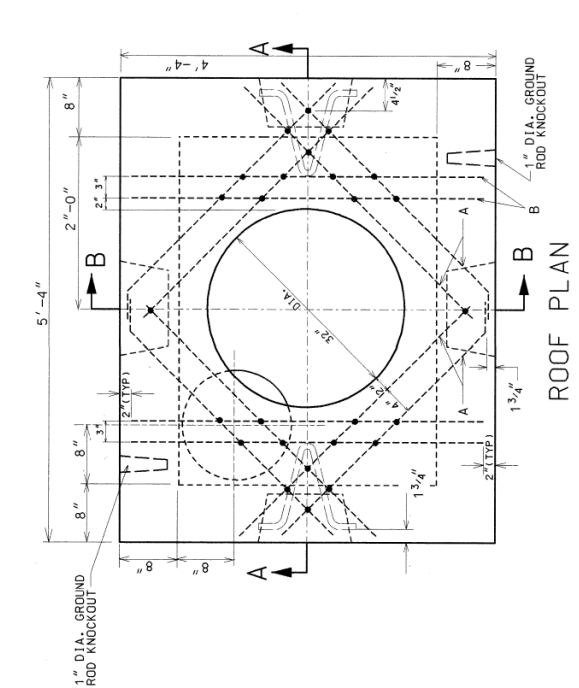
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CDOT DETAILS

SCALE: N.T.S. SHEET 15 OF 16 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 145 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE PATH = p:\617479-PMINT\pccom\line\local\AECOM_D592_NA\Documents\01_Americas\Tr_enporg\station\60269938_Circle\Phase_11\000_CAD\016_Electrical\Sheets\60x95_Contract\0160x95-sht-Light-16



| DRG. | MATERIAL | CODE | SIZE | Qty | Req |
|------|------------------|--------------|----------|-----|-----|
| | FORM FOR MANHOLE | 17-6674-6000 | 3'x4'x4' | 1 | |
| | CONDUIT END BELL | 17-6644-5320 | AS REQ. | | |
| | TILE, SEWER | 39-4036-3200 | 14\"/> | | |

| EXCAVATION (CONSTRUCTION INFORMATION) | |
|---------------------------------------|---------------|
| COMPLETE MANHOLE | 5.0 CU. YDS. |
| NEW ROOF ONLY | 2.0 CU. YDS. |
| SHEETING MANHOLE | 130.0 SQ. FT. |

| #5 REINFORCING BARS | |
|---------------------|-----------------------|
| BAR LENGTH | NO. OF RODS TOTAL FT. |
| A 2'-9" | 8 22'-0" |
| B 4'-0" | 4 16'-0" |

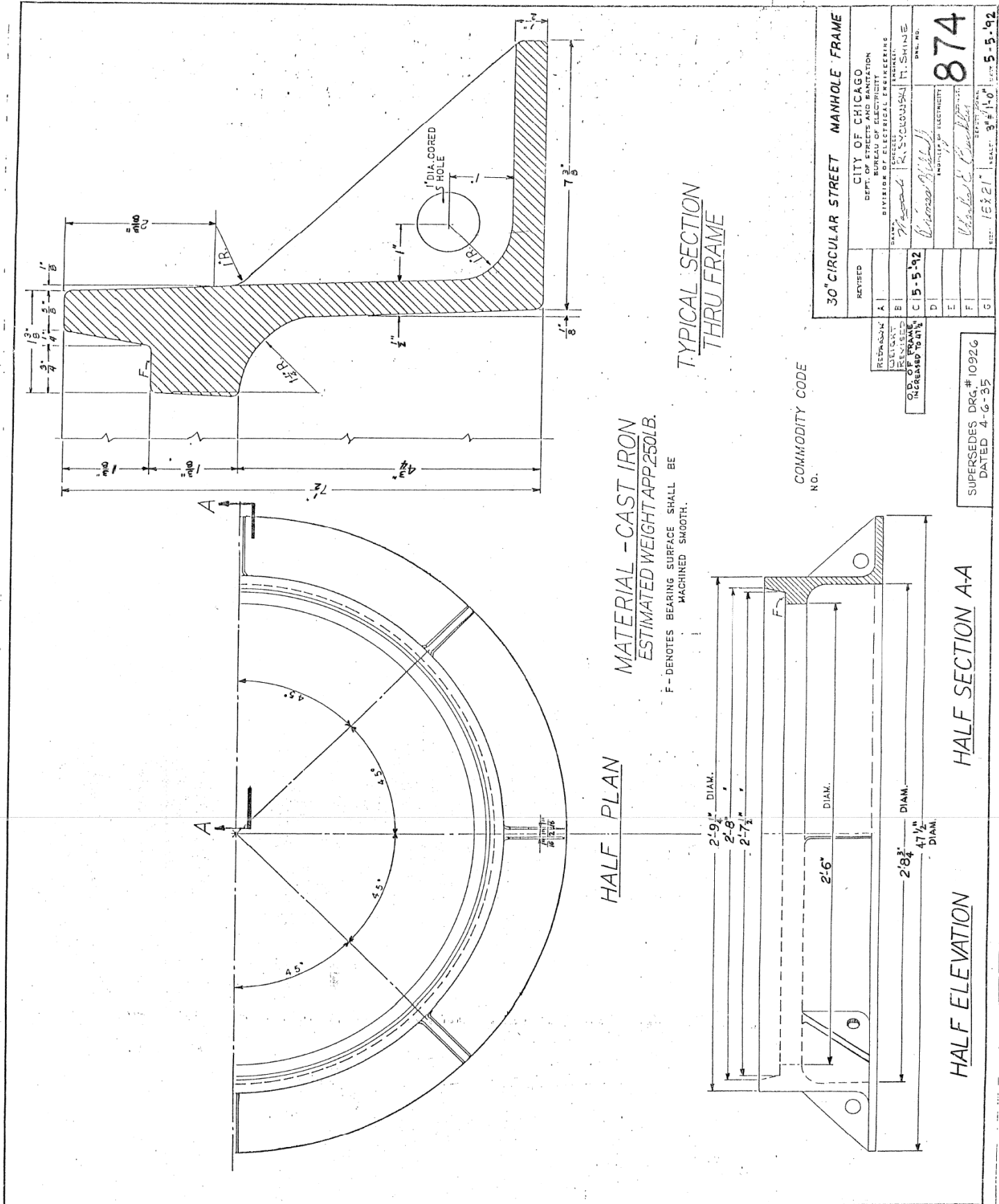
| MATERIALS FOR ROOF ONLY | |
|-------------------------|--------------|
| CONCRETE | 0.5 CU. YDS. |
| REINFORCING BARS #5 | 38' |

NOTES:
 1. PRECAST MANHOLES MUST BE PROVIDED WITH CHANNEL INSERTS, PULLING IRONS, AND CONDUIT KNOCK-OUTS.
 2. ALL CONCRETE MUST BE PORTLAND CEMENT CONCRETE MEETING IDOT REQUIREMENTS FOR CLASS PC CONCRETE FOR PRE-CAST STRUCTURES, OR CLASS S1 CONCRETE FOR CAST-IN-PLACE STRUCTURES.
 3. REINFORCING BARS MUST MEET ASTM A615 GRADE 60.

| DATE | REVISION |
|---------|---|
| 8/21/02 | SUPERSEDES DWG. 729 DATED JAN 12, 1996 |
| 1/12/96 | SUPERSEDES DWG. 729 DATED NOV. 21, 1973 |

3' x 4' x 4'
CONCRETE MANHOLE
WITH 30" FRAME AND COVER

CITY OF CHICAGO
 DEPT. OF STREETS AND SANITATION
 BUREAU OF ELECTRICAL ENGINEERING
 DIVISION OF ELECTRICAL ENGINEERING
 DRAFTSMAN: R. CARTER
 ELECTRICAL DESIGN ENGINEER: B. GARNSEY
 ENGINEER IN CHARGE: B. GARNSEY
 GENERAL SUPERINTENDENT: *Blas S. Alvarez*
 DEPUTY SUPERINTENDENT: *Blas S. Alvarez*
 SCALE: NONE
 SIZE: 11" x 22"
 DRAWING NO. **729**
 DATE: 8/21/02



| REVISED | REVISION |
|---------|----------|
| A | REVISION |
| B | REVISION |
| C | 5-5-92 |
| D | REVISION |
| E | REVISION |
| F | REVISION |
| G | REVISION |

30" CIRCULAR STREET MANHOLE FRAME

CITY OF CHICAGO
 DEPT. OF STREETS AND SANITATION
 BUREAU OF ELECTRICAL ENGINEERING
 DIVISION OF ELECTRICAL ENGINEERING
 DRAFTSMAN: R. CARTER
 ELECTRICAL DESIGN ENGINEER: B. GARNSEY
 ENGINEER IN CHARGE: B. GARNSEY
 GENERAL SUPERINTENDENT: *Blas S. Alvarez*
 DEPUTY SUPERINTENDENT: *Blas S. Alvarez*
 SCALE: NONE
 SIZE: 11" x 22"
 DRAWING NO. **874**
 DATE: 8/21/02



D160X95-sht-Light-16
 USER NAME = myersc
 PLOT SCALE = 2.0000' / in.
 PLOT DATE = 4/26/2017

| | |
|-----------------|-----------|
| DESIGNED - PFD | REVISED - |
| DRAWN - CAM | REVISED - |
| CHECKED - WDS | REVISED - |
| DATE - 05/02/17 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CDOT DETAILS

SCALE: N.T.S. SHEET 16 OF 16 SHEETS STA. TO STA.

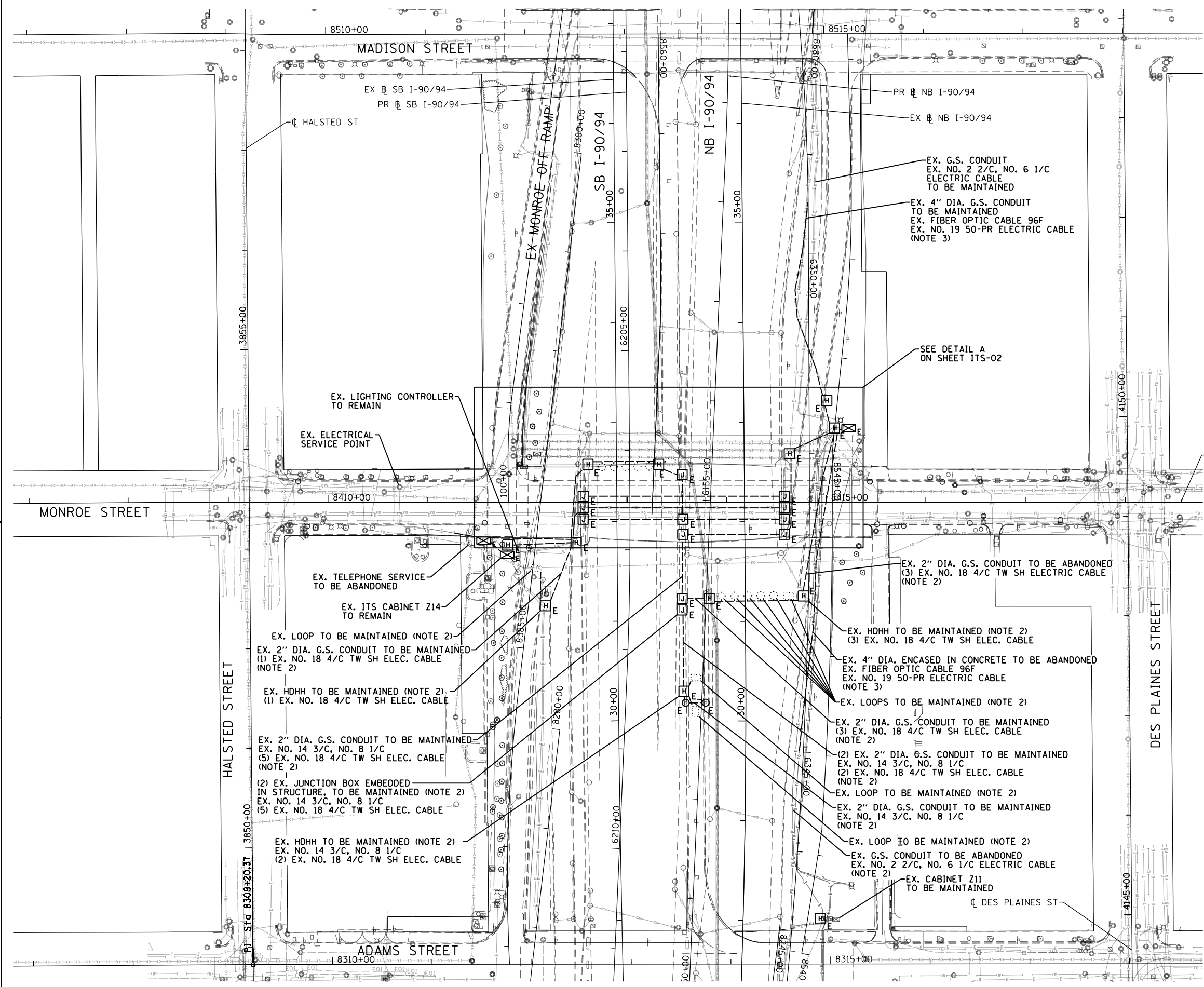
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-------------|--------|--------------|-----------|
| FAI 90/94 | 2014-016R&B | COOK | 378 | 146 |

CONTRACT NO. 60X95

ILLINOIS FED. AID PROJECT

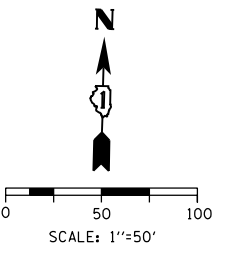
NOTES:

1. LOCATIONS OF EXISTING CONDUIT AND CABLES ARE APPROXIMATE. CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING UTILITIES IN THE FIELD.
2. DISCONNECT ELECTRIC CABLE FROM CABINET, DETECTOR LOOPS, AND RAMP METER SIGNALS AND REMOVE CABLE FROM CONDUIT, HEAVY DUTY HANDHOLES, AND JUNCTION BOXES.
3. FIBER OPTIC CABLE AND NO. 19 50-PAIR ELECTRIC CABLE FROM CABINET Y15 TO THE NORTH TO REMAIN IN PLACE AND BE SPliced TO TEMPORARY AND PROPOSED CABLE AFTER EACH IS INSTALLED (SEE TEMPORARY AND PROPOSED ITS PLAN SHEETS). FIBER OPTIC CABLE AND NO. 19 50-PAIR ELECTRIC CABLE BETWEEN CABINET Y15 AND CABINET Z11 (ADAMS STREET) TO BE REMOVED.
4. ALL EXISTING EQUIPMENT ATTACHED TO OR PART OF AN EXISTING STRUCTURE TO BE DEMOLISHED SHALL BE REMOVED AS PART OF THE STRUCTURAL REMOVAL PAY ITEMS, UNLESS OTHERWISE NOTED.



MONROE ST

DES PLAINES STREET



ITS-01

FILE PATH = p:\6179-PM\IT\pccommon\local\AECOM\0502_NA\Documents\01_Americas\T\engor\station\60269938_Circle\Phase_11\000_CAD\006_Roadway\Sheets\60x95_Contract\0160x95-Sht-11S-01



| |
|------------------------------|
| D160x95-Sht-ITS-01 |
| USER NAME = myersc |
| PLOT SCALE = 100.0000' / in. |
| PLOT DATE = 4/26/2017 |

| | |
|-----------------|-----------|
| DESIGNED - MJL | REVISED - |
| DRAWN - CAM | REVISED - |
| CHECKED - ME | REVISED - |
| DATE - 05/02/17 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

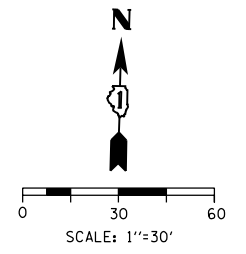
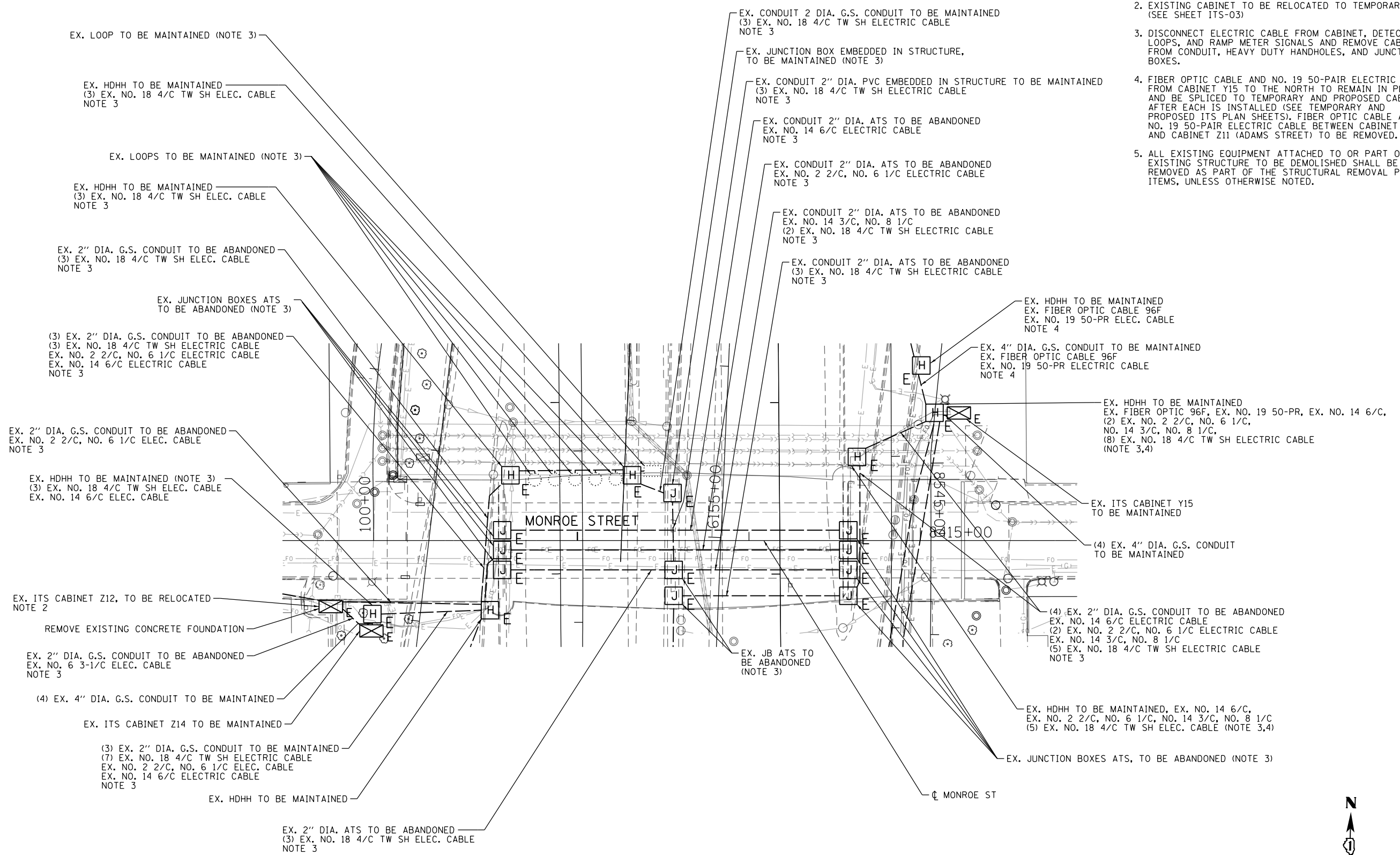
EXISTING/MAINTAINING ITS PLAN MONROE STREET

SCALE: 1"=50' SHEET 1 OF 6 SHEETS STA. 8409+20 TO STA. 8418+00

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 147 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES:

1. LOCATIONS OF EXISTING CONDUIT AND CABLES ARE APPROXIMATE. CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING UTILITIES IN THE FIELD.
2. EXISTING CABINET TO BE RELOCATED TO TEMPORARY POLE (SEE SHEET ITS-03)
3. DISCONNECT ELECTRIC CABLE FROM CABINET, DETECTOR LOOPS, AND RAMP METER SIGNALS AND REMOVE CABLE FROM CONDUIT, HEAVY DUTY HANDHOLES, AND JUNCTION BOXES.
4. FIBER OPTIC CABLE AND NO. 19 50-PAIR ELECTRIC CABLE FROM CABINET Y15 TO THE NORTH TO REMAIN IN PLACE AND BE SPLICED TO TEMPORARY AND PROPOSED CABLE AFTER EACH IS INSTALLED (SEE TEMPORARY AND PROPOSED ITS PLAN SHEETS). FIBER OPTIC CABLE AND NO. 19 50-PAIR ELECTRIC CABLE BETWEEN CABINET Y15 AND CABINET Z11 (ADAMS STREET) TO BE REMOVED.
5. ALL EXISTING EQUIPMENT ATTACHED TO OR PART OF AN EXISTING STRUCTURE TO BE DEMOLISHED SHALL BE REMOVED AS PART OF THE STRUCTURAL REMOVAL PAY ITEMS, UNLESS OTHERWISE NOTED.



ITS-02

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| | | |
|----------------------------|-----------------|-----------|
| D160X95-Sht-ITS-02 | DESIGNED - MJL | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 60.0000' / 1" | CHECKED - ME | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

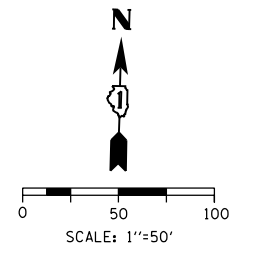
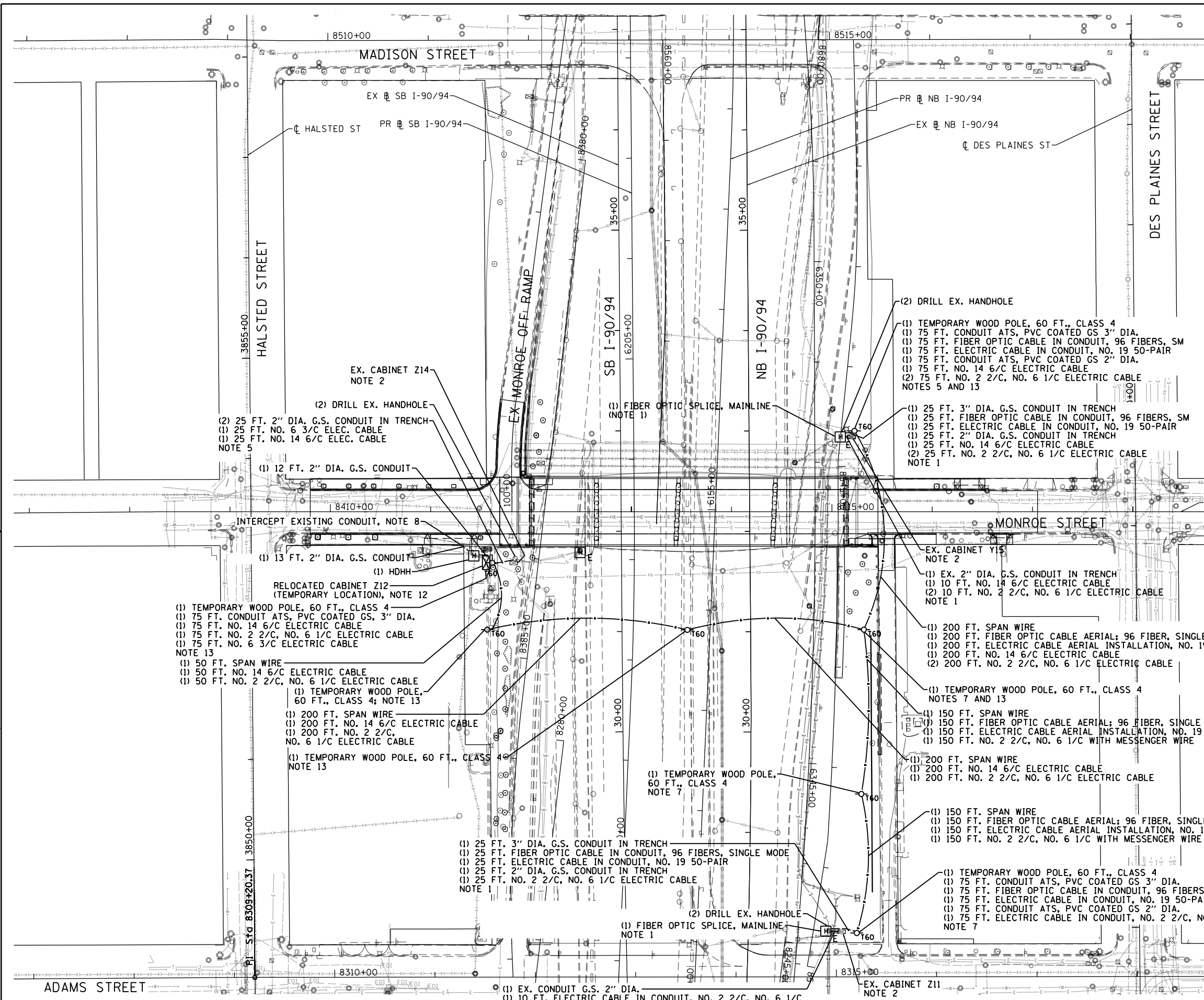
EXISTING/MAINTAINING ITS DETAIL A MONROE STREET

SCALE: 1"=30' SHEET 2 OF 6 SHEETS STA. 8409+20 TO STA. 8418+00

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 148 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES:

1. SPLICE TEMPORARY FIBER OPTIC CABLE AND NO. 18 50-PAIR CABLE TO EXISTING CABLES IN THE HANDHOLE. ONCE PROPOSED CONDUIT IS IN PLACE, REMOVE TEMPORARY CABLES AND SPLICE NEW CABLES TO EXISTING.
2. CONNECT NEW CABLES IN CABINET.
3. THE LOCATIONS OF THE ITS EQUIPMENT ON THE PLANS ARE APPROXIMATIONS. THE FINAL INSTALLATION LOCATIONS OF THE ITS EQUIPMENT SHALL BE STAKED IN THE FIELD AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING ANY WORK.
4. INSTALL 50 FT. OF SLACK OF ALL CABLES ON EACH POLE.
5. INSTALL CONDUIT AND CABLES ATTACHED TO STRUCTURE ON POLE AND UNDERGROUND TO EXISTING HANDHOLE.
6. FIBER OPTIC CABLE ASSIGNMENTS ARE TO BE IDENTIFIED BY THE OAK PARK TSC ELECTRICAL ENGINEER.
7. WOOD POLES TO BE INSTALLED 5 FT FROM THE ALLEY FENCE.
8. AFTER DISCONNECTING EXISTING ELECTRICAL SERVICE INSTALLATION FOR RELOCATION, REMOVE SERVICE CABLE TO THE SERVICE POINT. INTERCEPT EXISTING SERVICE CONDUIT WITH NEW CONDUIT TO THE NEW HANDHOLE AND THEN TO THE TEMPORARY WOOD POLE. REPLACE SERVICE CABLE BETWEEN SERVICE POINT AND RELOCATED ELECTRICAL SERVICE INSTALLATION. THIS WORK SHALL BE PAID FOR AS "ELECTRIC SERVICE INSTALLATION".
9. AERIAL CABLE SAG MUST BE A MINIMUM OF 20 FT ABOVE THE ROADWAY SURFACE.
10. AERIAL CABLE BETWEEN ITS CABINET Z14 AND ITS CABINET Y15 SHALL BE REMOVED AFTER THE PERMANENT INFRASTRUCTURE AND CABLING BETWEEN THE TWO CABINETS IS INSTALLED, TESTED, AND ACCEPTED. TEMPORARY WOOD POLES ON THE WEST SIDE OF THE MAINLINE AND IN THE MEDIUM SHALL BE REMOVED AFTER THE CABLES ARE REMOVED.
11. AERIAL CABLE AND TEMPORARY WOOD POLES BETWEEN ITS CABINET Y15 AND ITS CABINET Z11 (EAST SIDE OF THE MAINLINE) SHALL REMAIN IN PLACE AFTER THE CONCLUSION OF THIS CONTRACT.
12. REMOVE AND SALVAGE ITS CABINET Z12 AND TURN IT OVER TO IDOT AFTER THE NEW ELECTRICAL SERVICE INSTALLATION BECOMES OPERATIONAL.
13. COORDINATE THE FINAL INSTALLATION LOCATION OF THE TEMPORARY ITS WOOD POLES WITH THE LOCATIONS OF THE LIGHTING AND COMCAST TEMPORARY WOOD POLES TO AVOID ANY CONFLICTS.



ITS-03

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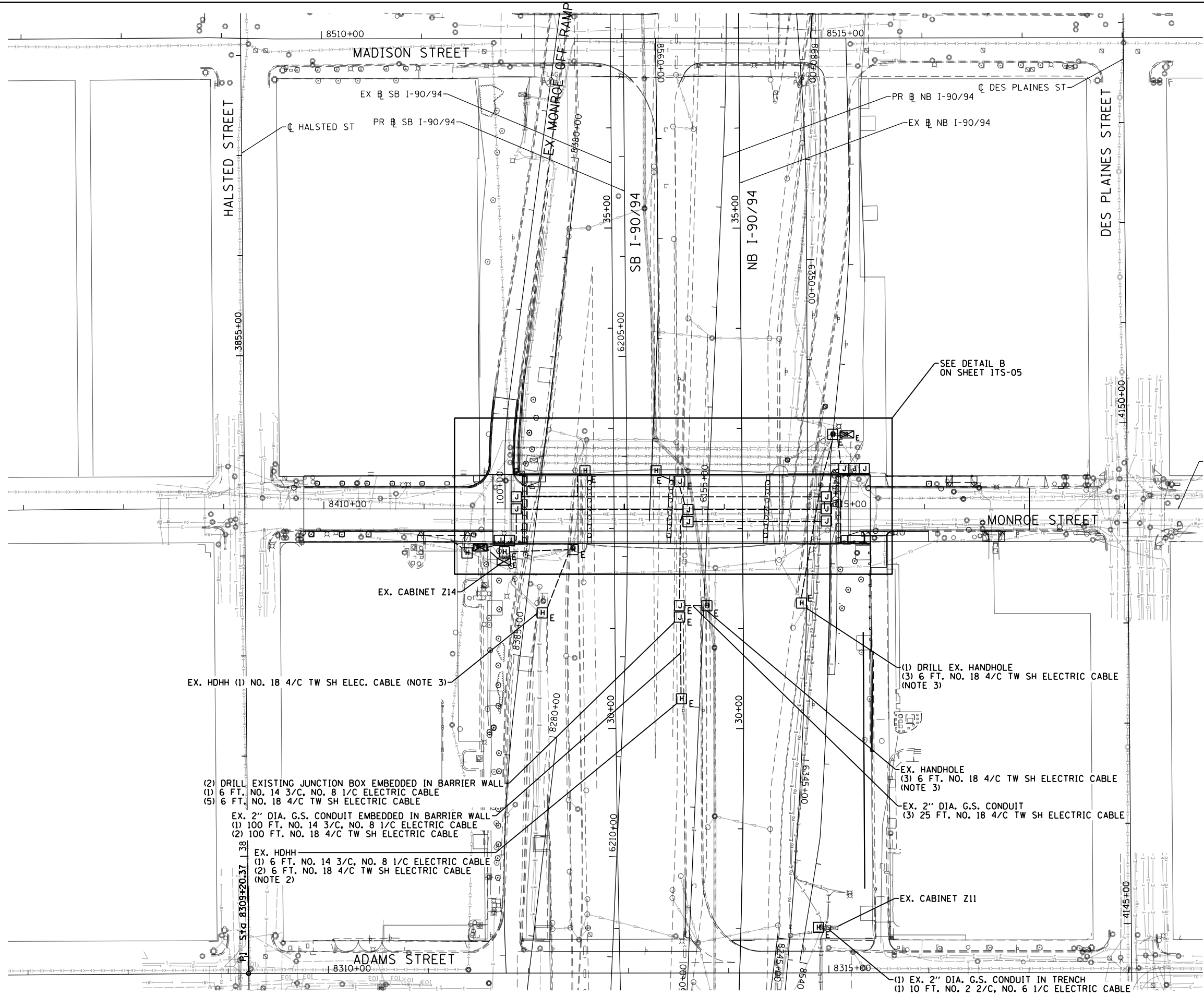
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| D160X95-Sht-ITS-03 | DESIGNED - MJL | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED - ME | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

| | | |
|------------------------------|-----------------|-----------|
| STATE OF ILLINOIS | DESIGNED - MJL | REVISED - |
| DEPARTMENT OF TRANSPORTATION | DRAWN - CAM | REVISED - |
| | CHECKED - ME | REVISED - |
| | DATE - 05/02/17 | REVISED - |

| | | |
|---------------|---------------------|------------------------------|
| SCALE: 1"=50' | SHEET 3 OF 6 SHEETS | STA. 8409+20 TO STA. 8418+00 |
|---------------|---------------------|------------------------------|

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 149 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

FILE PATH = p:\617479-PM\ITR\ascom\line\local\IAC\CDM_D592_NA\Documents\01_Americas\T\engp\station\60269438_Circle\Phase_11\000_CAD\006_Roadway\Sheets\60x95_Contract\0160x95-Sht-115-04



NOTES:

1. THE LOCATIONS OF THE ITS EQUIPMENT SHOWN ON THE PLANS ARE APPROXIMATIONS. THE FINAL INSTALLATION LOCATIONS OF THE ITS EQUIPMENT SHALL BE STAKED IN THE FIELD AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING ANY WORK.
2. CONNECT PERMANENT RAMP METER SIGNAL CABLES TO SIGNALS AND SPLICE RAMP METER DETECTOR CABLES TO DETECTOR LOOPS.
3. SPLICE PERMANENT NEW DETECTOR CABLE TO EXISTING DETECTOR LOOPS.

EX. HDHH (1) NO. 18 4/C TW SH ELEC. CABLE (NOTE 3)

(2) DRILL EXISTING JUNCTION BOX EMBEDDED IN BARRIER WALL
 (1) 6 FT. NO. 14 3/C, NO. 8 1/C ELECTRIC CABLE
 (5) 6 FT. NO. 18 4/C TW SH ELECTRIC CABLE

EX. 2" DIA. G.S. CONDUIT EMBEDDED IN BARRIER WALL
 (1) 100 FT. NO. 14 3/C, NO. 8 1/C ELECTRIC CABLE
 (2) 100 FT. NO. 18 4/C TW SH ELECTRIC CABLE

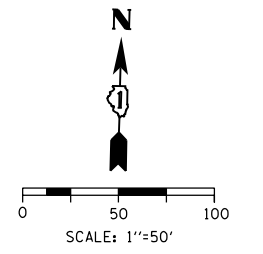
EX. HDHH
 (1) 6 FT. NO. 14 3/C, NO. 8 1/C ELECTRIC CABLE
 (2) 6 FT. NO. 18 4/C TW SH ELECTRIC CABLE
 (NOTE 2)

(1) DRILL EX. HANDHOLE
 (3) 6 FT. NO. 18 4/C TW SH ELECTRIC CABLE
 (NOTE 3)

EX. HANDHOLE
 (3) 6 FT. NO. 18 4/C TW SH ELECTRIC CABLE
 (NOTE 3)

EX. 2" DIA. G.S. CONDUIT
 (3) 25 FT. NO. 18 4/C TW SH ELECTRIC CABLE

(1) EX. 2" DIA. G.S. CONDUIT IN TRENCH
 (1) 10 FT. NO. 2 2/C, NO. 6 1/C ELECTRIC CABLE



ITS-04



D160X95-Sht-ITS-04
 USER NAME = myersc
 PLOT SCALE = 100.0000' / in.
 PLOT DATE = 4/26/2017

| | |
|-----------------|-----------|
| DESIGNED - MJL | REVISED - |
| DRAWN - CAM | REVISED - |
| CHECKED - ME | REVISED - |
| DATE - 05/02/17 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

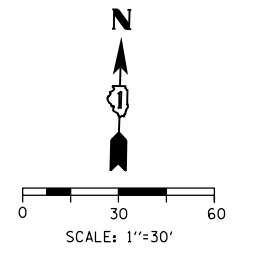
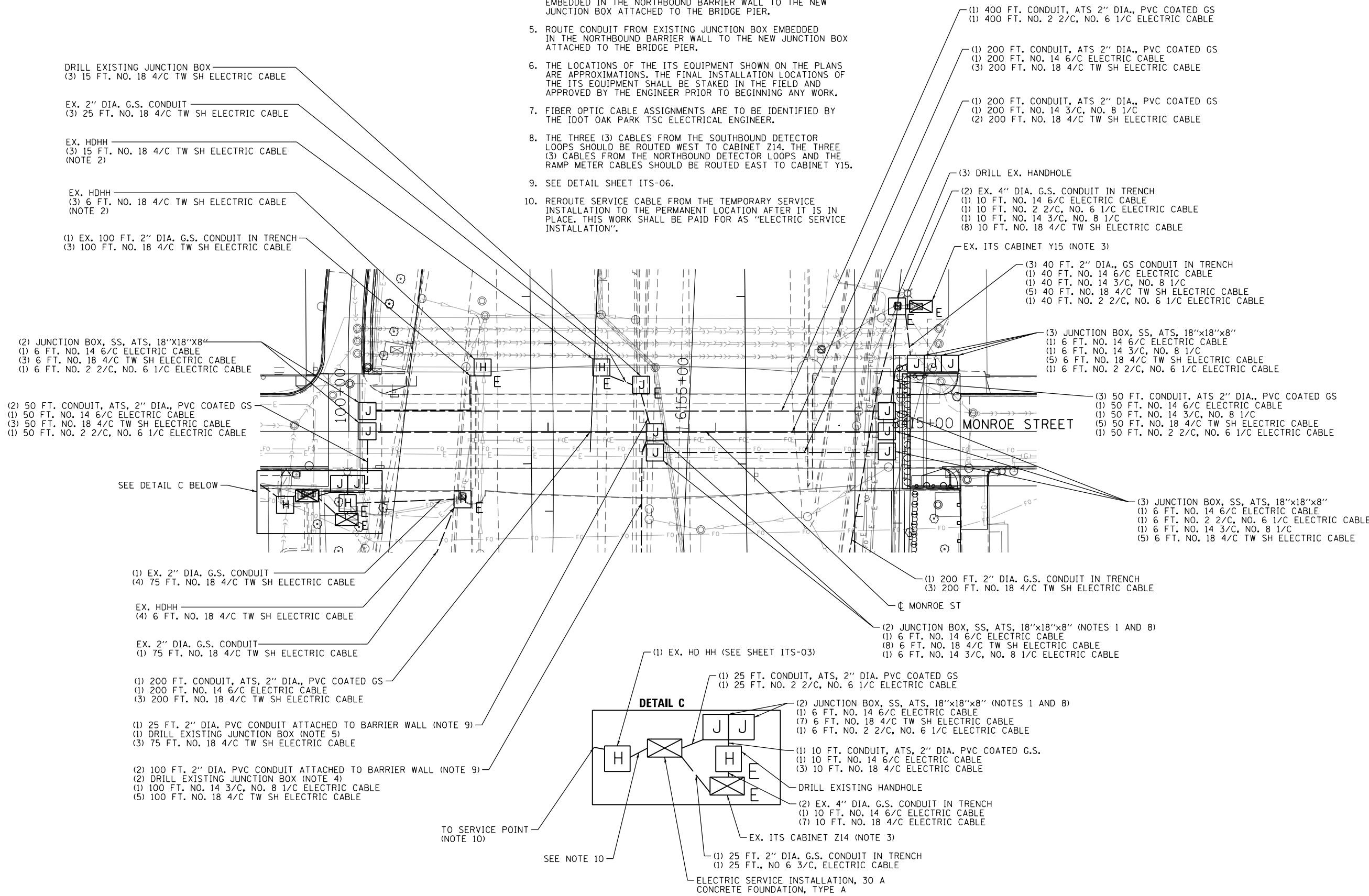
PROPOSED ITS PLAN MONROE STREET

SCALE: 1"=50' SHEET 4 OF 6 SHEETS STA. 8409+20 TO STA. 8418+00

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 150 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES:

1. ATTACH JUNCTION BOXES TO BRIDGE PIER.
2. SPLICE PERMANENT NEW DETECTOR CABLE TO EXISTING DETECTOR LOOPS.
3. CONNECT NEW CABLES IN CABINET.
4. ROUTE ONE CONDUIT FROM EACH EXISTING JUNCTION BOX EMBEDDED IN THE NORTHBOUND BARRIER WALL TO THE NEW JUNCTION BOX ATTACHED TO THE BRIDGE PIER.
5. ROUTE CONDUIT FROM EXISTING JUNCTION BOX EMBEDDED IN THE NORTHBOUND BARRIER WALL TO THE NEW JUNCTION BOX ATTACHED TO THE BRIDGE PIER.
6. THE LOCATIONS OF THE ITS EQUIPMENT SHOWN ON THE PLANS ARE APPROXIMATIONS. THE FINAL INSTALLATION LOCATIONS OF THE ITS EQUIPMENT SHALL BE STAKED IN THE FIELD AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING ANY WORK.
7. FIBER OPTIC CABLE ASSIGNMENTS ARE TO BE IDENTIFIED BY THE IDOT OAK PARK TSC ELECTRICAL ENGINEER.
8. THE THREE (3) CABLES FROM THE SOUTHBOUND DETECTOR LOOPS SHOULD BE ROUTED WEST TO CABINET Z14. THE THREE (3) CABLES FROM THE NORTHBOUND DETECTOR LOOPS AND THE RAMP METER CABLES SHOULD BE ROUTED EAST TO CABINET Y15.
9. SEE DETAIL SHEET ITS-06.
10. REROUTE SERVICE CABLE FROM THE TEMPORARY SERVICE INSTALLATION TO THE PERMANENT LOCATION AFTER IT IS IN PLACE. THIS WORK SHALL BE PAID FOR AS "ELECTRIC SERVICE INSTALLATION".



ITS-05

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| | | |
|-----------------------------|-----------------|-----------|
| D160x95-Sht-ITS-05 | DESIGNED - MJL | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 60.0000' / in. | CHECKED - ME | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

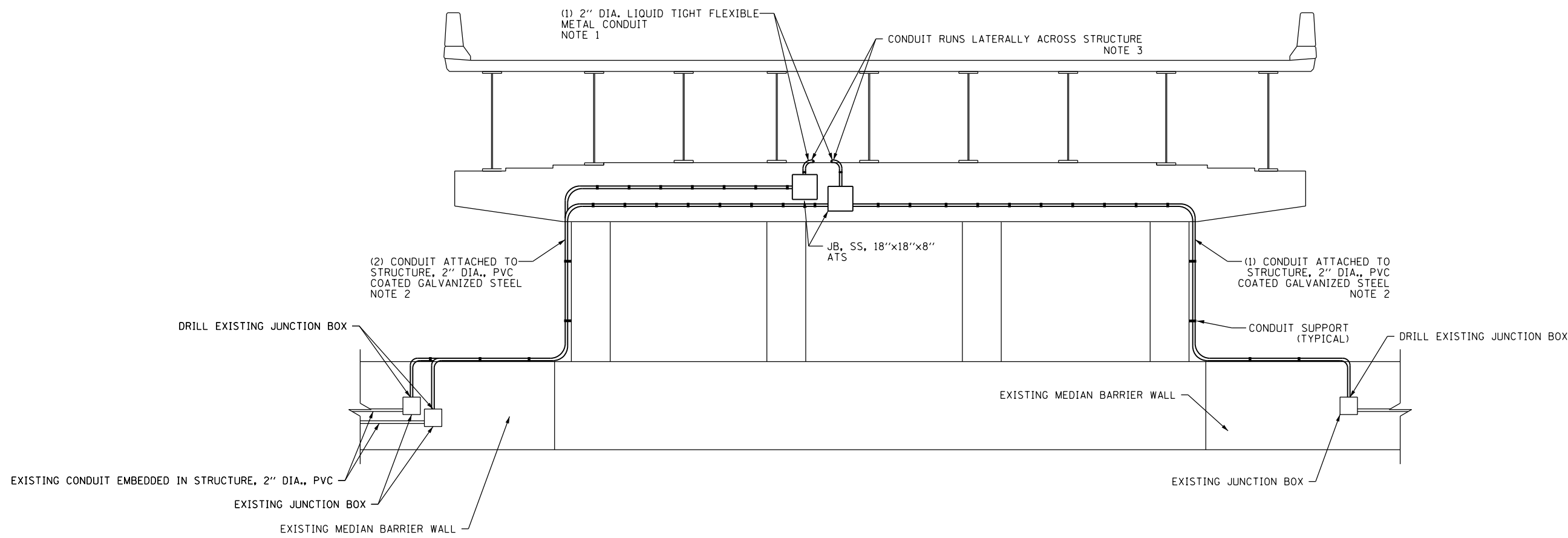
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED ITS DETAIL B MONROE STREET

SCALE: 1"=30' SHEET 5 OF 6 SHEETS STA. 8409+20 TO STA. 8418+00

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. R.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 151 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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CONDUIT ATTACHED TO PIER INSTALLATION DETAIL
(LOOKING WEST)

NOTES:

1. THE FLEXIBLE CONDUIT WILL BE MEASURED AND PAID FOR UNDER THE CONDUIT ATTACHED TO STRUCTURE PAY ITEMS.
2. AT ALL LOCATIONS WHERE 17'-3" MINIMUM CLEARANCE UNDER PIER CAP CAN BE PROVIDED, CONDUIT ATTACHED TO THE STRUCTURE SHALL BE PLACED IN THE REVEAL OF THE PIER AND SHALL NOT CONFLICT WITH DRAINAGE INFRASTRUCTURE.
3. SEE STRUCTURAL PLANS FOR CONDUIT LOCATION.

ITS-06



| | | |
|-----------------------------|-----------------|-----------|
| D160X95-sht-ITS-06 | DESIGNED - MJL | REVISED - |
| USER NAME = myersc | DRAWN - CAM | REVISED - |
| PLOT SCALE = 40.0000' / in. | CHECKED - ME | REVISED - |
| PLOT DATE = 4/26/2017 | DATE - 05/02/17 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ITS DETAILS

SCALE: N.T.S. SHEET 6 OF 6 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| FAI 90/94 | 2014-016R&B | COOK | 378 | 152 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

Bench Mark: Chisel "X" on east side of I-90 ±80' S of Monroe Street on SE corner of Handhole on concrete. Elevation 578.58'.

Existing Structure:
S.N. 016-2054 was originally built as F.A. Route 173, Section S-01012-4B in 1955. The existing structure is a four-span bridge with a reinf. concrete deck supported on W36 beams. The existing structure has an overall length of 341'-2" with span lengths 67'-6", 2 @ 100'-6" and 67'-6" and an out to out deck width 71'-7", carrying four traffic lanes. The existing substructure consists of stub-abutments and multi-column piers founded on timber piles.

Traffic Control: The existing bridge will be closed to traffic and detoured during construction.

No Salvage

SCUPPER LOCATION

| Station | Offset | Station | Offset |
|---------|------------|---------|------------|
| 8412+10 | 22.00' Rt. | 8412+10 | 22.00' Lt. |
| 8412+15 | 22.00' Rt. | 8412+15 | 22.00' Lt. |
| 8412+75 | 22.00' Rt. | 8412+75 | 22.00' Lt. |
| 8412+80 | 22.00' Rt. | 8412+80 | 22.00' Lt. |
| 8414+27 | 22.00' Rt. | 8414+27 | 22.00' Lt. |
| 8414+32 | 22.00' Rt. | 8414+32 | 22.00' Lt. |
| 8415+00 | 22.00' Rt. | 8415+00 | 22.00' Lt. |
| 8415+05 | 22.00' Rt. | 8415+05 | 22.00' Lt. |

DESIGN SPECIFICATIONS
2014 AASHTO LRFD Bridge Design Specifications,
7th Edition with 2015 & 2016 Interims

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS

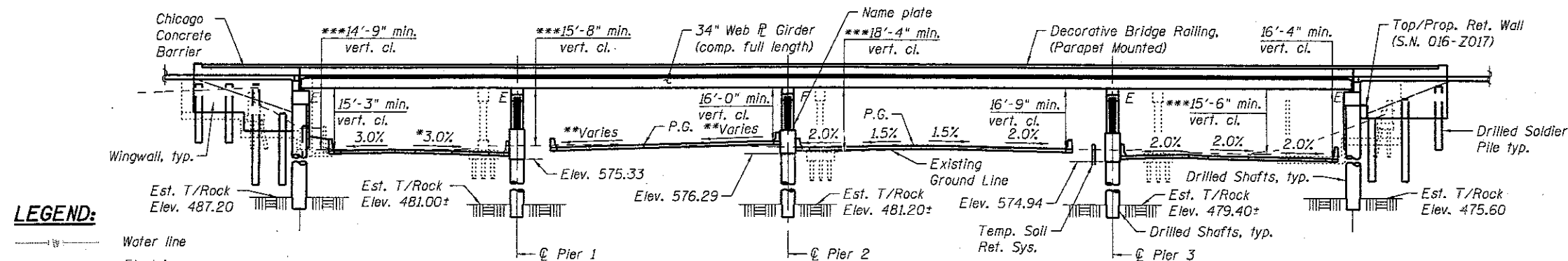
f'c = 3,500 psi
f'c = 4,000 psi (Superstructure Concrete)
fy = 60,000 psi (Reinforcement)
fy = 50,000 psi (M270 Grade 50)

LIGHTWEIGHT CELLULAR CONCRETE FILL

Class II (District I)
Maximum as cast density 30 pcf
f'c = 120 psi

SEISMIC DATA

Seismic Performance Zone (SPZ) = I
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.085g
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.144g
Soil Site Class = D



LEGEND:

- W — Water line
- E — Electric
- T — Telephone line
- TV — Television line
- FO — Fiber Optic line
- CS — Combined Sewer
- SS — Storm Sewer
- ◆ Soil Boring Location

Note:
Roadway cross sections and lane configuration under shown in plan and elevation are part of future contract. For existing cross sections under Monroe Street, see Roadway Plans.

ELEVATION

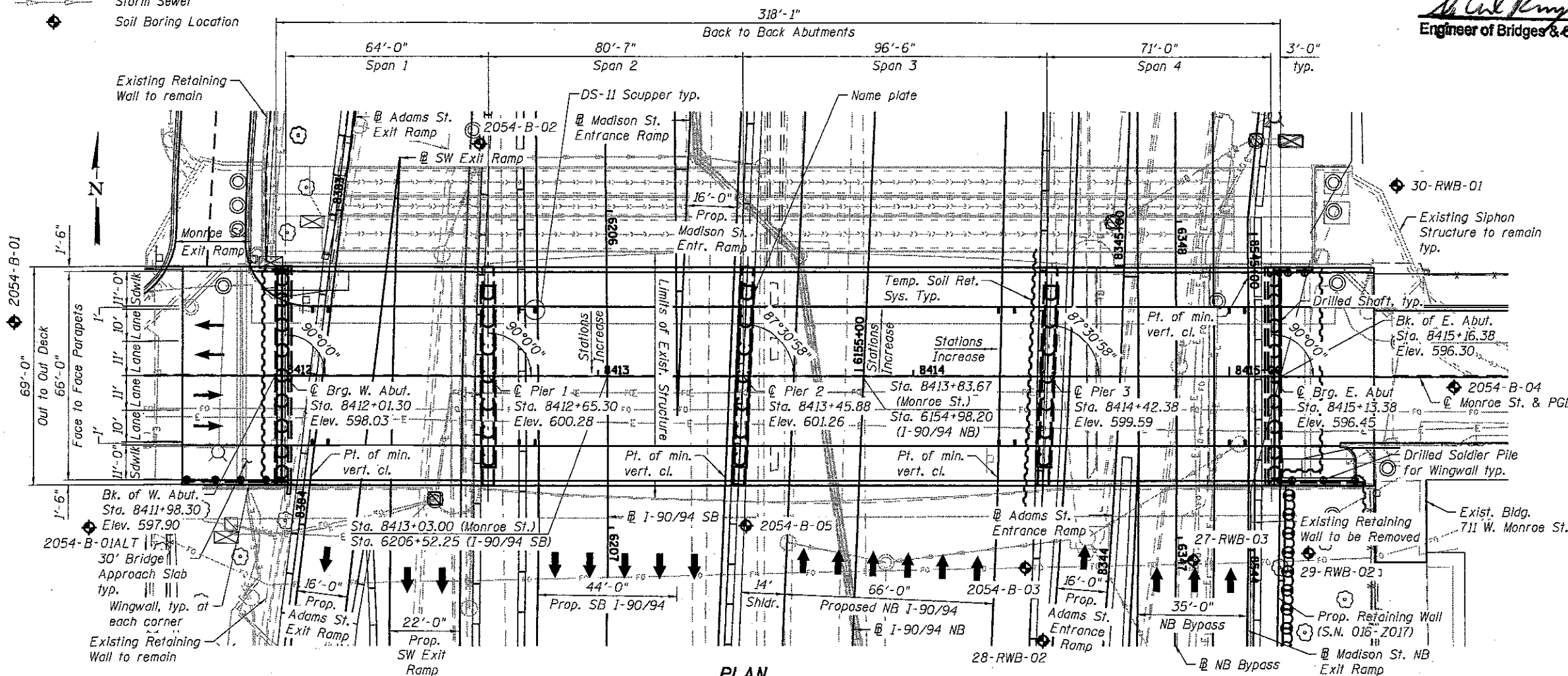
* Varies from 3.0% @ Sta. 1305+28.07 to 2.0% in the opposite direction @ Sta. 1306+28.07
** Varies from 2.0% @ Sta. 6206+14.92 to 4.0% @ Sta. 6207+21.92
*** Interim vertical clearance

APPROVED
For Structural Adequacy Only

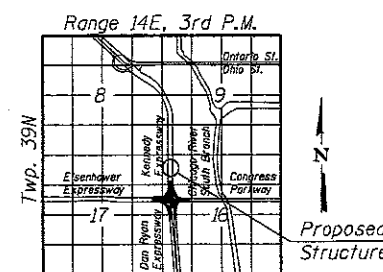
Jamal I. Grainawi
Engineer of Bridges & Structures



Signed *Jamal I. Grainawi*
JAMAL I. GRAINAWI, S.E. IL Lic. No. 081-005161
Expires 11-30-2018
Date July 11, 2017



PLAN



LOCATION SKETCH

GENERAL PLAN & ELEVATION
MONROE ST. OVER I-90/94
(KENNEDY EXPRESSWAY)
F.A.U. ROUTE 1420
SECTION 2014-016R&B
COOK COUNTY
STATION 8413+45.88
STRUCTURE NO. 016-1700

0161700-60X95-S001-QPE.dgn

PARSONS BRINCKERHOFF

| | | |
|-----------------------|------------------|-----------|
| USER NAME = pateld | DESIGNED - MS/JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 7/12/2017 | DRAWN - OCP | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. S1-01 OF S1-60 SHEETS

| | | | | |
|--------------------|---------------------|-------------|------------------|---------------------------|
| F.A.U. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 153 |
| CONTRACT NO. 60X95 | | | | ILLINOIS FED. AID PROJECT |

GENERAL NOTES:

- Fasteners shall be ASTM A325 Type 1, hot dipped galvanized bolts. Bolts 7/8" φ, holes 15/16" φ, unless otherwise noted.
- Calculated weight of Structural Steel = 510,540 pounds (AASHTO M270 Grade 50). Calculated weight of Structural Steel = 45,070 pounds (AASHTO M270 Grade 36).
- All structural steel shall be hot-dip galvanized (see special provisions).
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Bearing seat surfaces shall be constructed or adjusted to the designated elevations within a tolerance of 1/8" (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.
- Concrete Sealer shall be applied to the designated areas of the Piers, Abutments and Wingwalls.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- Slipforming of the parapet is not allowed.
- The contractor shall exercise extreme caution during construction to make certain that construction activities, live load surcharge and other loads applied to the structures will not have detrimental effects on the adjacent building foundations and the existing siphon and other utilities. Any damage during construction shall be repaired by the contractor at his/her expense and no charge to the department.
- Impact driving piles and temporary sheet piling is not allowed.
- Based on the high squeeze potential of the clay soils, the use of temporary casing will be required to Elevation ±549.5 at Pier 1 and Elevation ±547.5 at Pier 2 and Pier 3 in order to properly construct the drilled shaft. Casing may be pulled or left in place, as determined by the Contractor at no cost to the Department.
- The first drilled shaft installed for Piers 1, 2, and 3 shall be considered a demonstration shaft to demonstrate and prove the Contractor's equipment, methods, and proposed procedures for installation of the drilled shaft foundations. CSL testing shall be performed on the demonstration shaft and the results of which shall be submitted to the Engineer for review prior to continuing installation of the remained of drilled shafts on Piers 1, 2 and 3. Any identified inadequacies in the proposed procedures or equipment shall be corrected before proceeding with the Work. If the demonstration shaft cannot be properly constructed using the intended equipment and procedures, or if the Contractor's procedures and equipment are found inadequate, the Contractor shall submit appropriate revisions and remedial measures for the Engineer's approval. The cost of finding suitable equipment and/or procedure will not be paid separately.
- The Contractor shall take all necessary precautions not to contaminate groundwater during the drilled shaft construction operation. Contractor is responsible for the proper containment and disposal of the contaminated groundwater and spoils resulting from Contractor's means and methods. No additional cost will be paid for this effort.

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- SI-1 General Plan and Elevation
- SI-2 General Data I
- SI-3 General Data II
- SI-4 Substructure Layout
- SI-5 Existing Structure Removal Details I
- SI-6 Existing Structure Removal Details II
- SI-7 Temporary Soil Retention System I
- SI-8 Top of Slab Elevation Plan
- SI-9 Top of Slab Elevations I
- SI-10 Top of Slab Elevations II
- SI-11 Top of Slab Elevations III
- SI-12 Top of Slab Elevations IV
- SI-13 Top of West Approach Slab Elevations
- SI-14 Top of East Approach Slab Elevations
- SI-15 Superstructure
- SI-16 Superstructure Details I
- SI-17 Superstructure Details II
- SI-18 Superstructure Details III
- SI-19 Superstructure Details IV
- SI-20 Bridge Approach Slab Details I
- SI-21 Bridge Approach Slab Details II
- SI-22 Bridge Approach Slab Details III
- SI-23 Decorative Railing Parapet Mounted
- SI-24 Expansion Joint Details
- SI-25 Drainage Scupper, DS-II
- SI-26 Drainage System Details
- SI-27 Framing Plan
- SI-28 Superstructure Steel Details I
- SI-29 Superstructure Steel Details II
- SI-30 Bearing Details I
- SI-31 Bearing Details II
- SI-32 West Abutment
- SI-33 West Abutment Details I
- SI-34 West Abutment Details II
- SI-35 West Abutment Details III
- SI-36 West Abutment Architectural Details
- SI-37 East Abutment
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- SI-39 East Abutment Details II
- SI-40 East Abutment Details III
- SI-41 East Abutment Architectural Details
- SI-42 Pier 1
- SI-43 Pier 1 Details
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- SI-45 Pier 2
- SI-46 Pier 2 Details
- SI-47 Pier 2 Architectural Details
- SI-48 Pier 3
- SI-49 Pier 3 Details
- SI-50 Pier 3 Architectural Details
- SI-51 Bar Splicer Assembly and Mechanical Splicer Details
- SI-52 Boring Logs I
- SI-53 Boring Logs II
- SI-54 Boring Logs III
- SI-55 Boring Logs IV
- SI-56 Boring Logs V
- SI-57 Boring Logs VI
- SI-58 Boring Logs VII
- SI-59 Boring Logs VIII
- SI-60 Boring Logs IX

TOTAL BILL OF MATERIAL

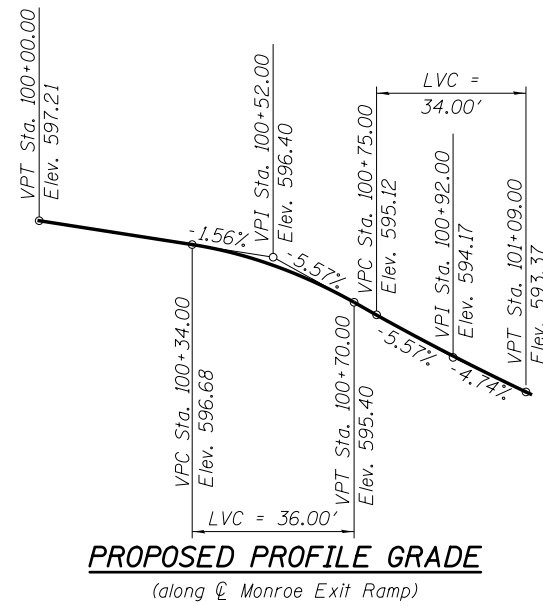
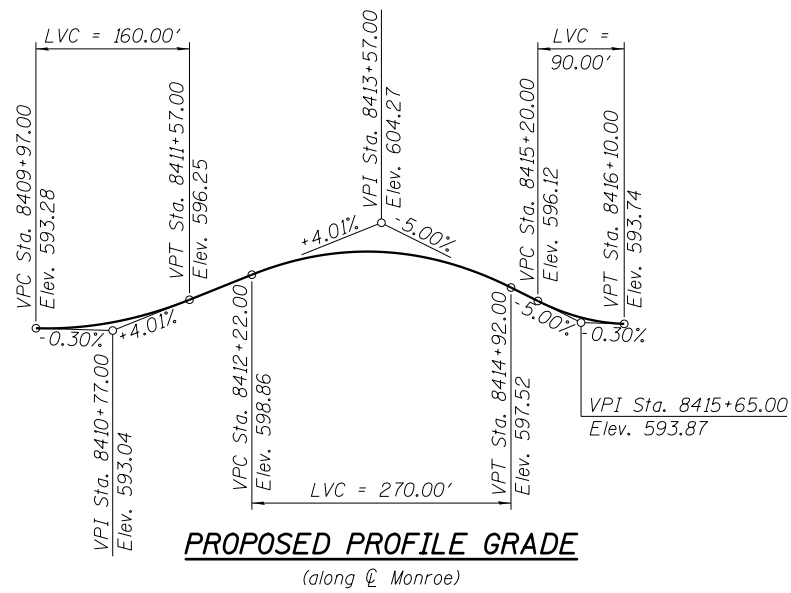
| ITEM | UNIT | SUPER | SUB | TOTAL |
|--|---------|---------|---------|---------|
| Removal of Existing Structures No. 1 | Each | 1 | - | 1 |
| Slope Wall Removal | Sq. Yd. | - | 161 | 161 |
| Protective Shield | Sq. Yd. | 2,714 | - | 2,714 |
| Structure Excavation | Cu. Yd. | - | 1,457 | 1,457 |
| Concrete Structures | Cu. Yd. | - | 706 | 706 |
| Concrete Superstructure | Cu. Yd. | 956 | - | 956 |
| Bridge Deck Grooving | Sq. Yd. | 1,814 | - | 1,814 |
| Form Liner Textured Surface | Sq. Ft. | - | 836 | 836 |
| Rubbed Finish | Sq. Ft. | - | 10,760 | 10,760 |
| Protective Coat | Sq. Yd. | 3,173 | - | 3,173 |
| Concrete Superstructure (Approach Slab) | Cu. Yd. | 197 | - | 197 |
| Furnishing and Erecting Structural Steel | L. Sum | 1.0 | - | 1.0 |
| Stud Shear Connectors | Each | 8,046 | 406 | 8,452 |
| Reinforcement Bars | Pound | - | 349,790 | 349,790 |
| Reinforcement Bars, Epoxy Coated | Pound | 296,340 | 137,890 | 434,230 |
| Bar Splicers | Each | - | 140 | 140 |
| Name Plates | Each | 1 | - | 1 |
| Permanent Casing | Foot | - | 2,006 | 2,006 |
| Drilled Shaft in Soil | Cu. Yd. | - | 1,835 | 1,835 |
| Drilled Shaft in Rock | Cu. Yd. | - | 42.5 | 42.5 |
| Preformed Joint Strip Seal | Foot | 138 | - | 138 |
| Elastomeric Bearing Assembly, Type I | Each | - | 36 | 36 |
| Anchor Bolts, 5/8" | Each | - | 36 | 36 |
| Anchor Bolts, 1" | Each | - | 54 | 54 |
| Temporary Soil Retention System | Sq. Ft. | - | 1,525 | 1,525 |
| Furnishing Soldier Piles (W Section) | Foot | - | 271 | 271 |
| Drilling and Setting Soldier Piles (In Soil) | Cu. Ft. | - | 583 | 583 |
| Concrete Sealer | Sq. Ft. | - | 12,393 | 12,393 |
| Geocomposite Wall Drain | Sq. Yd. | - | 388 | 388 |
| Lightweight Cellular Concrete Fill | Cu. Yd. | - | 1,066 | 1,066 |
| Decorative Railing (Parapet Mounted) | Foot | 724 | - | 724 |
| Crosshole Sonic Logging Access Ducts | Foot | - | 4,051 | 4,051 |
| Crosshole Sonic Logging Testing | Each | - | 10 | 10 |
| Slope Inclinator | Each | - | 2 | 2 |
| Drainage Scuppers, DS-II | Each | 16 | - | 16 |
| Drainage System | L. Sum | 1.0 | - | 1.0 |
| Pipe Underdrains for Structures 4" | Foot | - | 211 | 211 |

STATION 8413+45.88
 BUILT 20-- BY
 STATE OF ILLINOIS
 F.A.U. RT. 1420 SEC. 2014-016R&B
 LOADING HL-93
 STR. NO. 016-1700

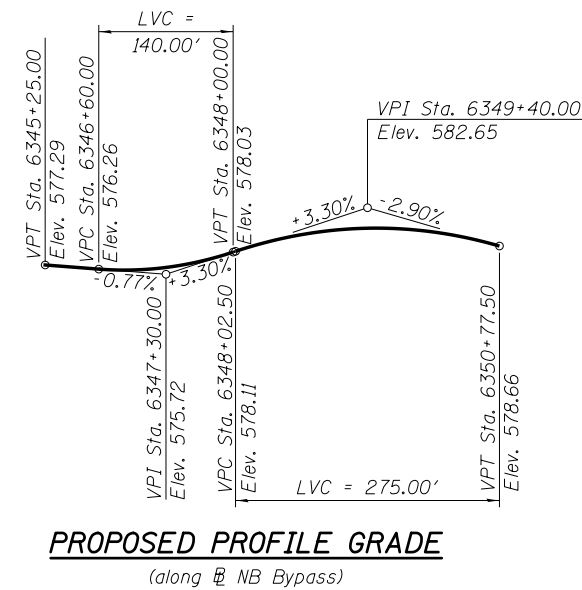
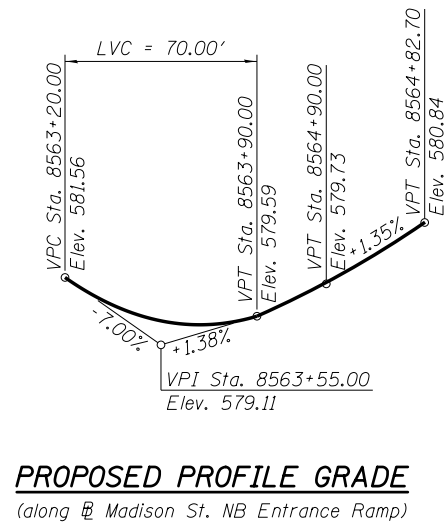
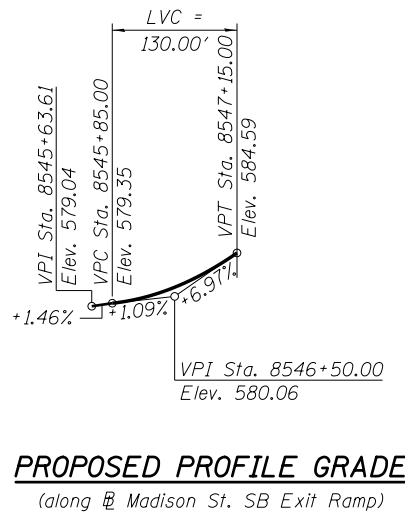
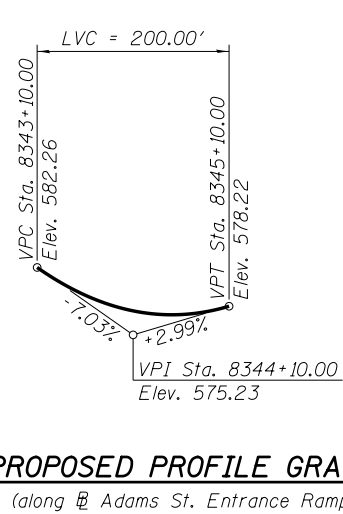
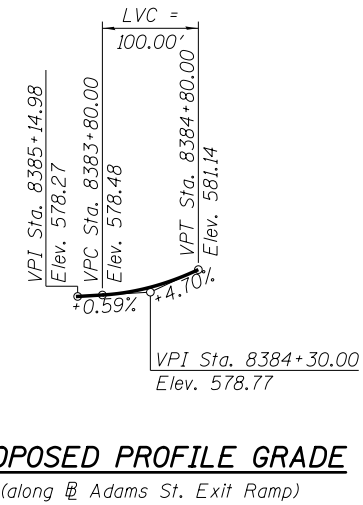
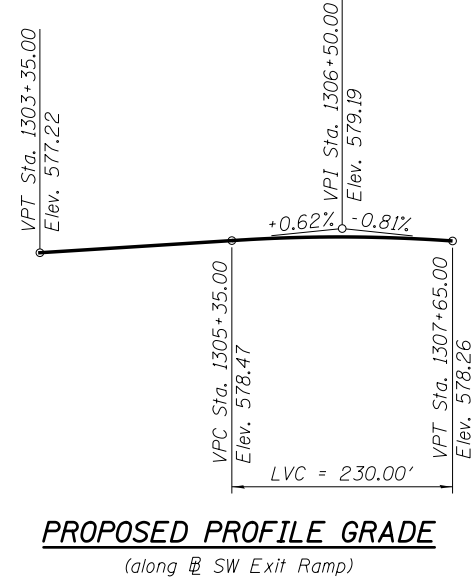
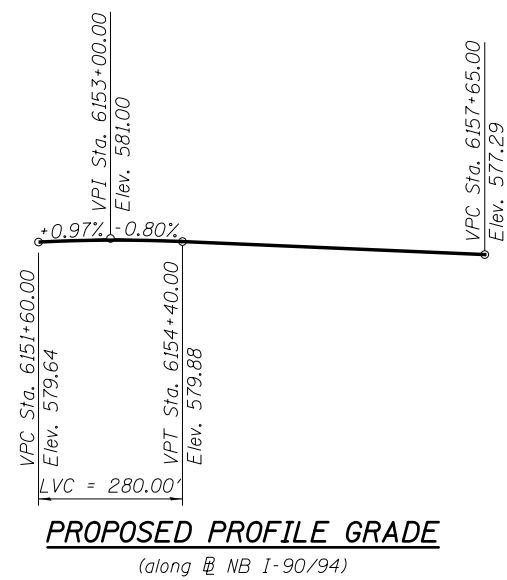
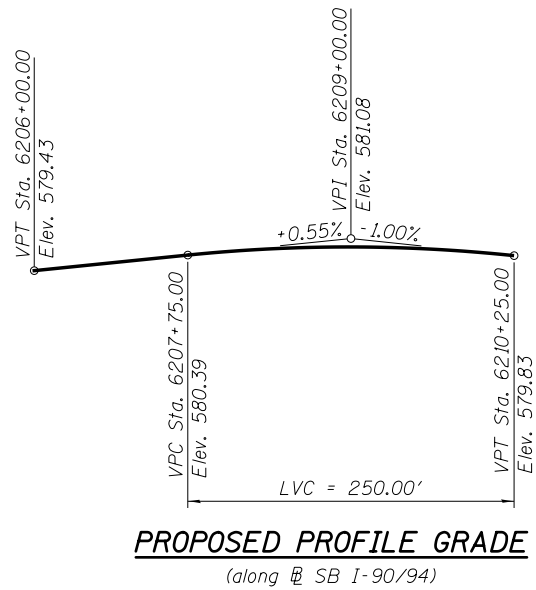
NAME PLATE
 See Std. 515001

0161700-60X95-5002-1NX.dgn

| | | | | | | | | | | |
|---------------------------------|---------------------------|---------------------|---------------------------------|---|--|---------------------------|---------|--------|--------------|-----------|
| PARSONS BRINCKERHOFF | USER NAME = lopezgonzalez | DESIGNED - HA/MS/JZ | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL DATA I STRUCTURE NO. 016-1700 | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | CHECKED - HA | REVISED - | 90/94 | | | 2014-016R&B | COOK | 378 | 154 | |
| | PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - | | | CONTRACT NO. 60X95 | | | | |
| PLOT DATE = 9/1/2017 | CHECKED - JIG | REVISED - | SHEET NO. S1-02 OF S1-60 SHEETS | | | ILLINOIS FED. AID PROJECT | | | | |



For information only,
part of future contract.



0161700-60X95-5003-DET.dgn

**PARSONS
BRINCKERHOFF**

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MA | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

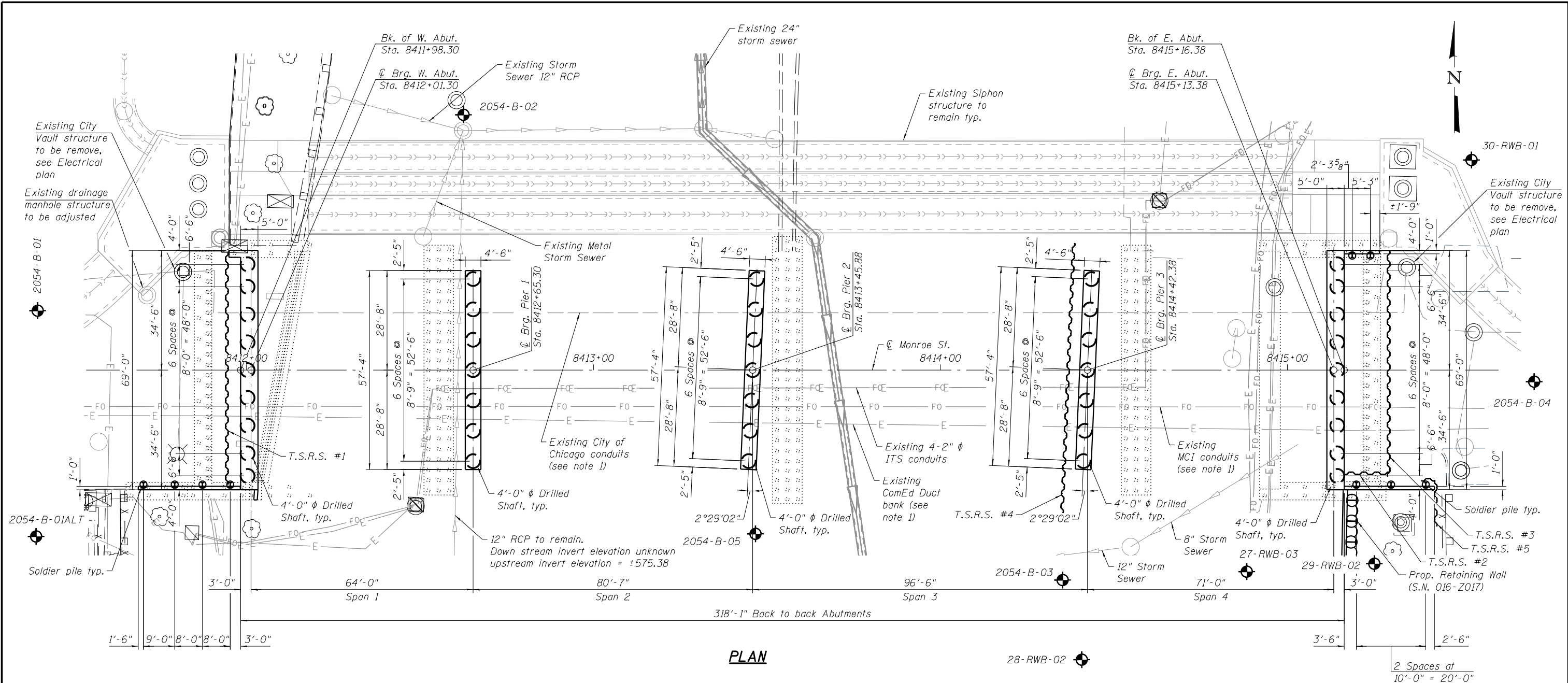
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA II
STRUCTURE NO. 016-1700**

SHEET NO. S1-03 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 155 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT



PLAN

- Notes:
- Existing utility between girders to be removed and/or replaced part of this contract.
 - For Temporary Soil Retention System limits, See sheet S1-7 of S1-60.
 - The contractor shall take precautions to protect existing utilities and foundations during construction of the bridge. The utilities were located based on SUE and utility supplier information available at design.
 - For all drainage structure to be adjusted, see drainage plans.
 - T.S.R.S. denotes Temporary Soil Retention System.

LEGEND:

- |W|— Water line
- E— Electric
- T— Telephone line
- CTV— Television line
- FO— Fiber Optic line
- >>>> Combined Sewer
- >>>> Storm Sewer
- ⊕ Soil Boring

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**PARSONS
BRINCKERHOFF**

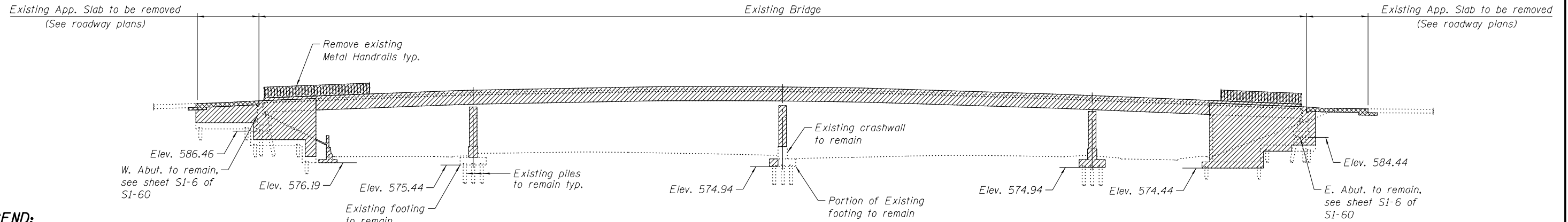
| | | |
|---------------------------|------------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - JZ/MS | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUBSTRUCTURE LAYOUT
STRUCTURE NO. 016-1700**

SHEET NO. S1-04 OF S1-60 SHEETS

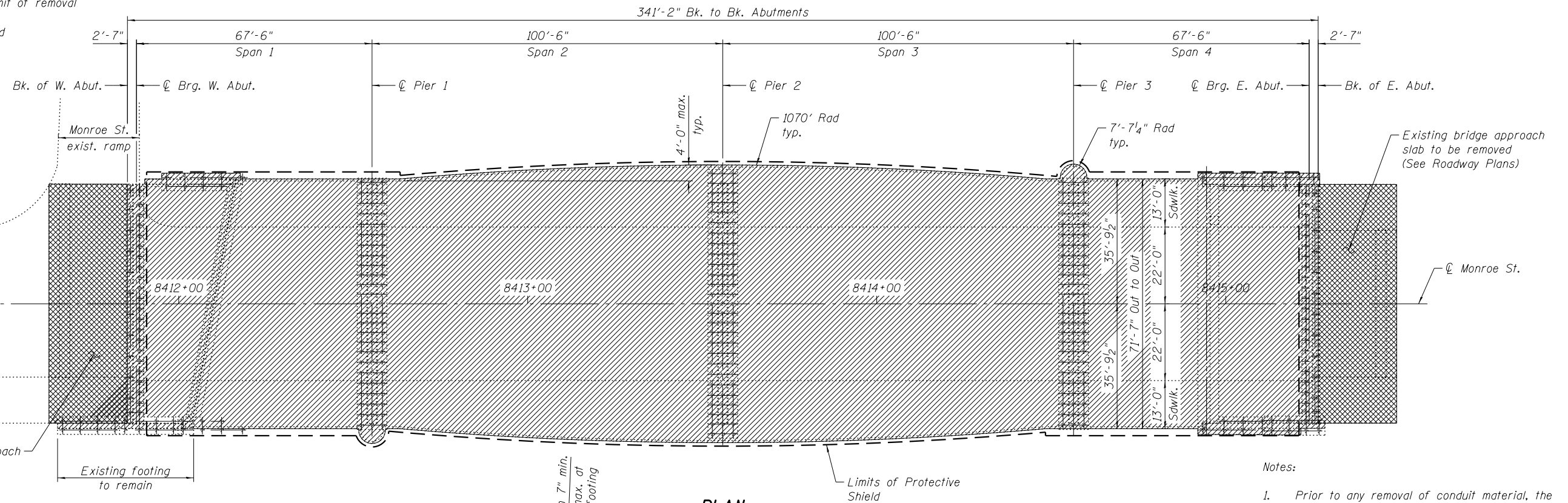
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 156 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



ELEVATION

LEGEND:

- Removal of existing structures, No. 1
- See roadway plans for limit of removal
- Limits of Protective Shield



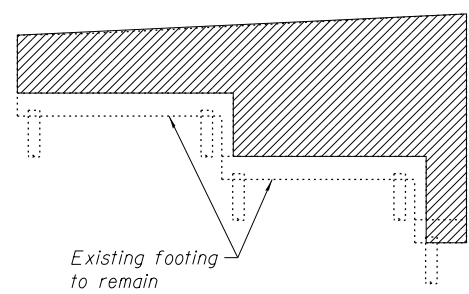
PLAN

Notes:

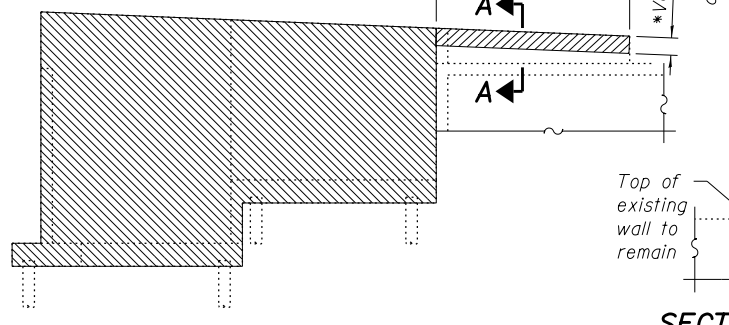
1. Prior to any removal of conduit material, the existing conduit must be tested for the presence of asbestos content. See special provisions for Removal of Existing Structures No. 1 and Removal of Asbestos Cement Conduits.
2. conduits to be removed by others prior to bridge removal if existing conduits contain asbestos. If not removed by others, utility will confirm that no asbestos materials are present and conduit removal will be included as part of Removal of Existing Structures No. 1.
3. For Temporary Soil Retention System limits, See sheet SI-7 of SI-60.
4. See sheet SI-6 of SI-60, for substructure removal.
5. Work this sheet with sheet SI-6 of SI-60.

BILL OF MATERIAL

| Item | Unit | Quantity |
|--------------------------------------|-------|----------|
| Removal of Existing Structures No. 1 | Each | 1 |
| Slope Wall Removal | Sq Yd | 161 |
| Protective Shield | Sq Yd | 2714 |

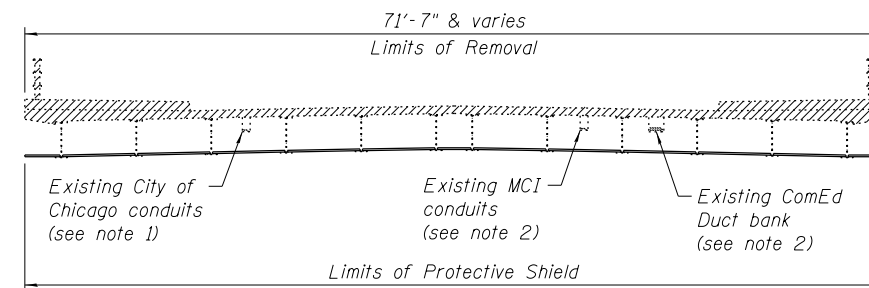


SOUTH WEST WINGWALL REMOVAL



NORTH EAST WINGWALL REMOVAL

SECTION A-A
 * Remove portion of existing wingwall to accommodate approach slab and approach footing construction. Cost included in Removal of Existing Structures, No. 1.



CROSS SECTION OF SUPERSTRUCTURE (Looking East)

0161700-60X95-5005-ESR.dgn

PARSONS BRINCKERHOFF

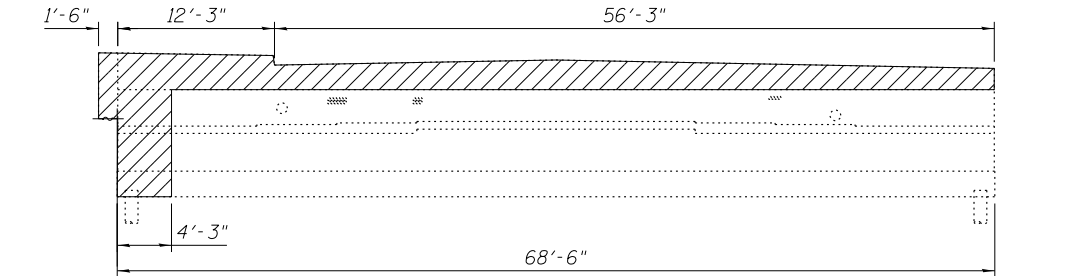
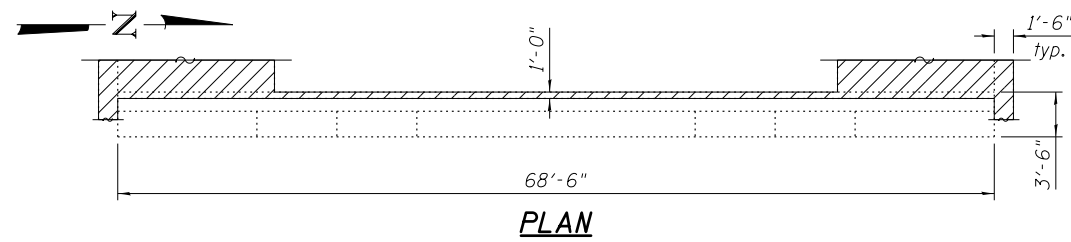
| | | |
|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - HA | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

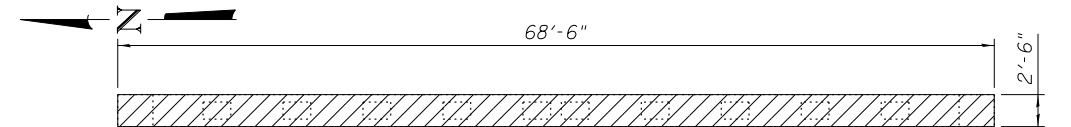
**EXISTING STRUCTURE REMOVAL DETAILS I
 STRUCTURE NO. 016-1700**

SHEET NO. S1-05 OF S1-60 SHEETS

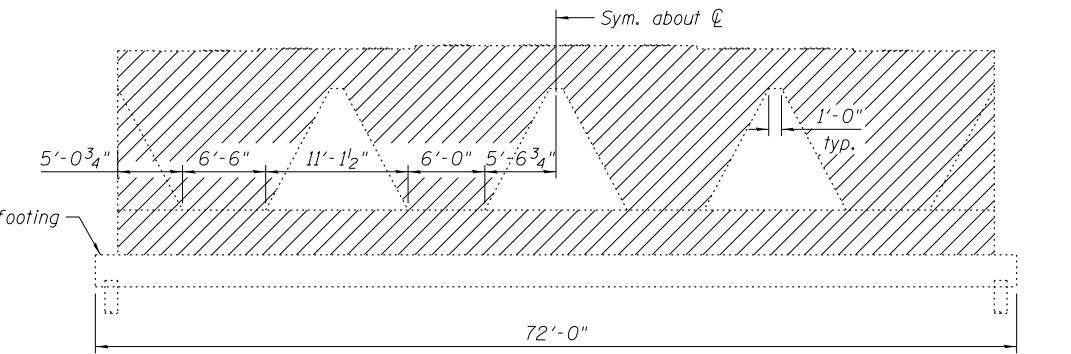
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|---------------------------|---------------------|-------------|------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 157 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



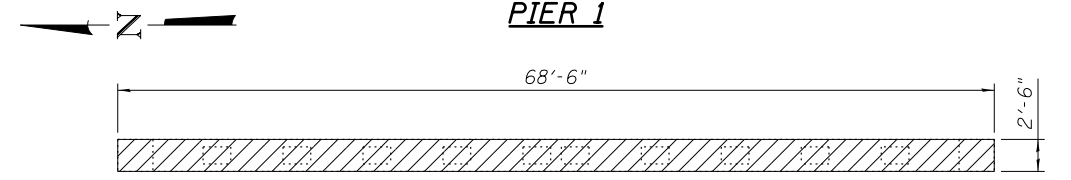
W. ABUTMENT ELEVATION



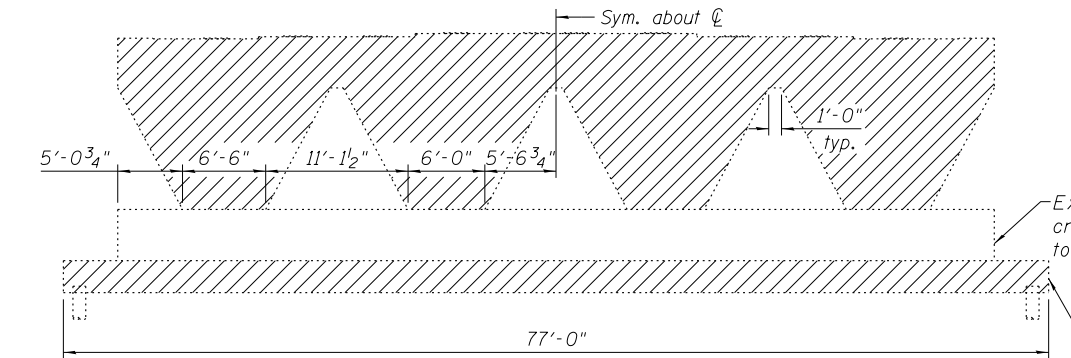
PLAN



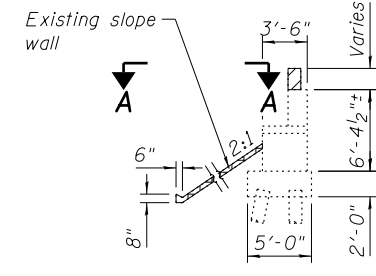
PIER 1



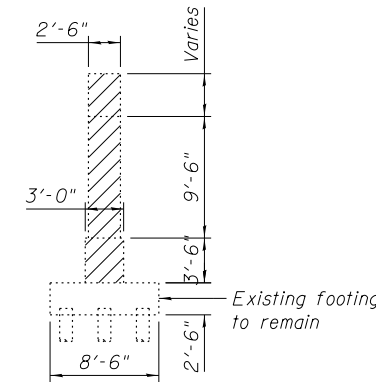
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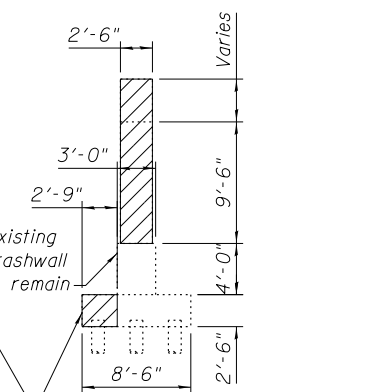
PIER 2



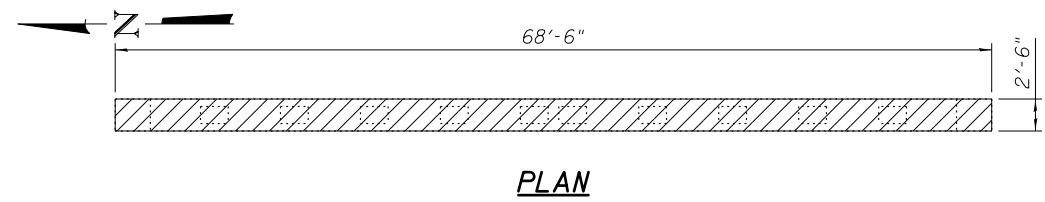
END VIEW



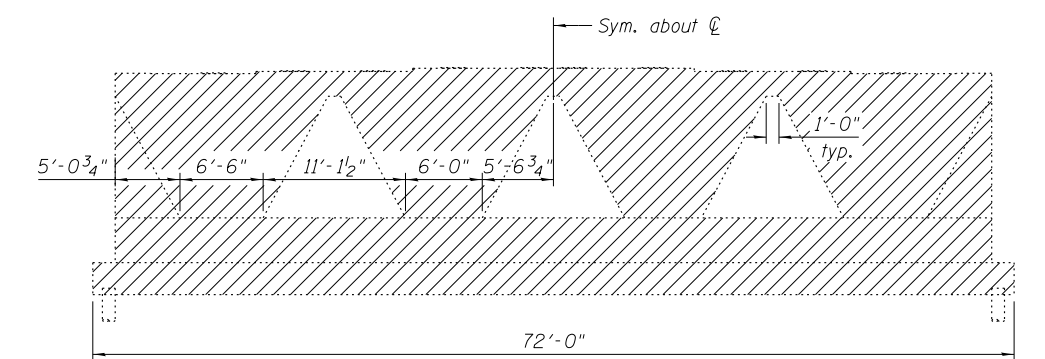
END VIEW



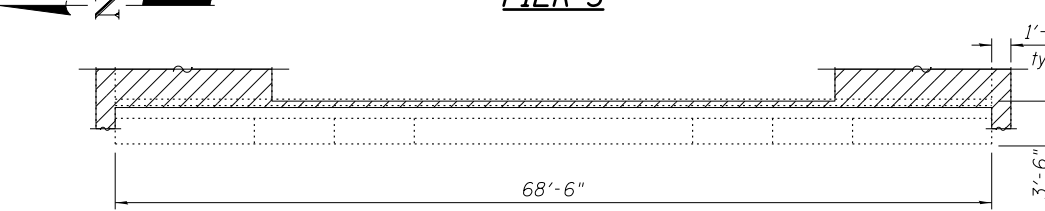
END VIEW



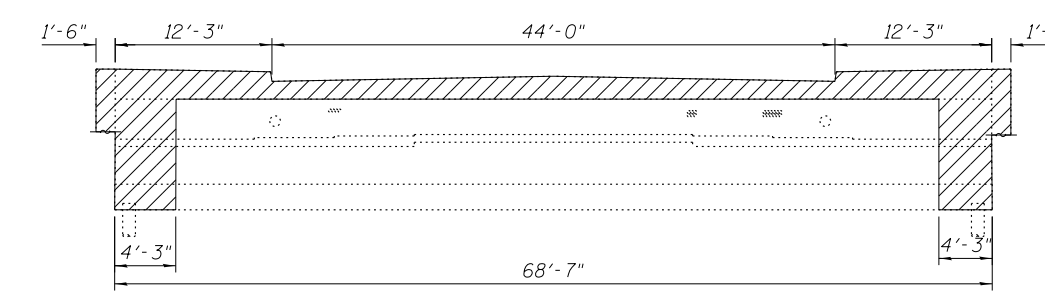
PLAN



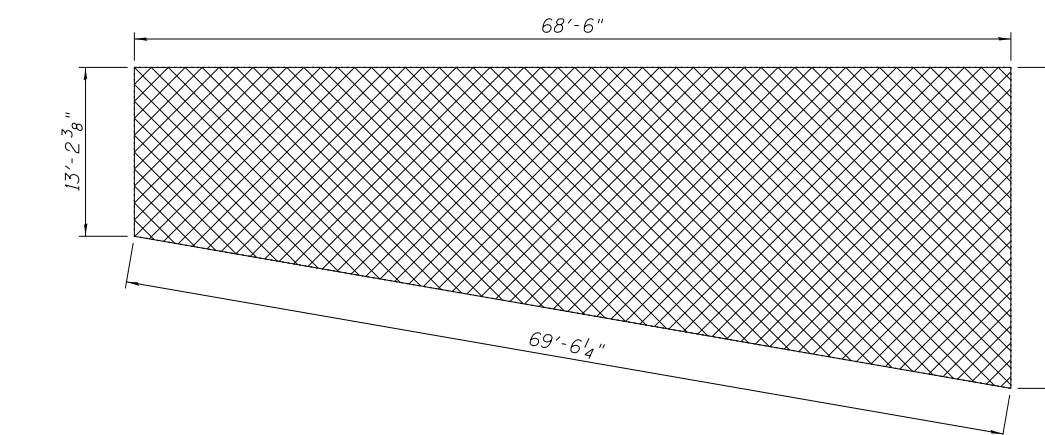
PIER 3



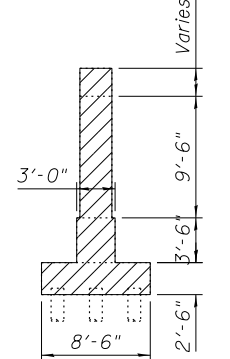
PLAN



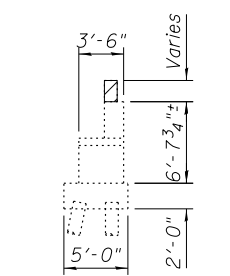
E. ABUTMENT ELEVATION



SECTION A-A



END VIEW



END VIEW

LEGEND:

- Removal of Existing Structures No. 1
- Slope Wall Removal

Note: Work this sheet with sheet S1-5 of S1-60.

0161700-60X95-5006-ESR.dgn

PARSONS BRINCKERHOFF

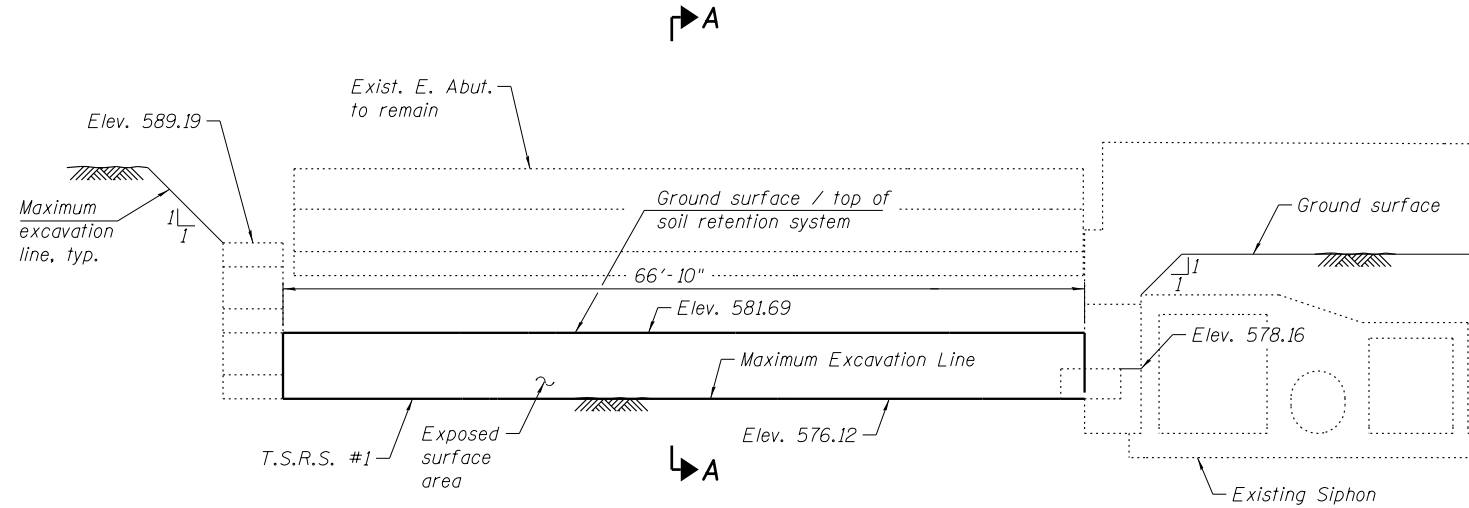
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|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - HA | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

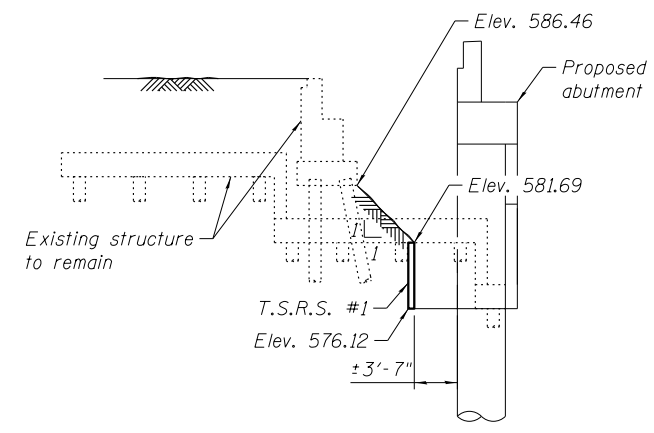
**EXISTING STRUCTURE REMOVAL DETAILS II
STRUCTURE NO. 016-1700**

SHEET NO. S1-06 OF S1-60 SHEETS

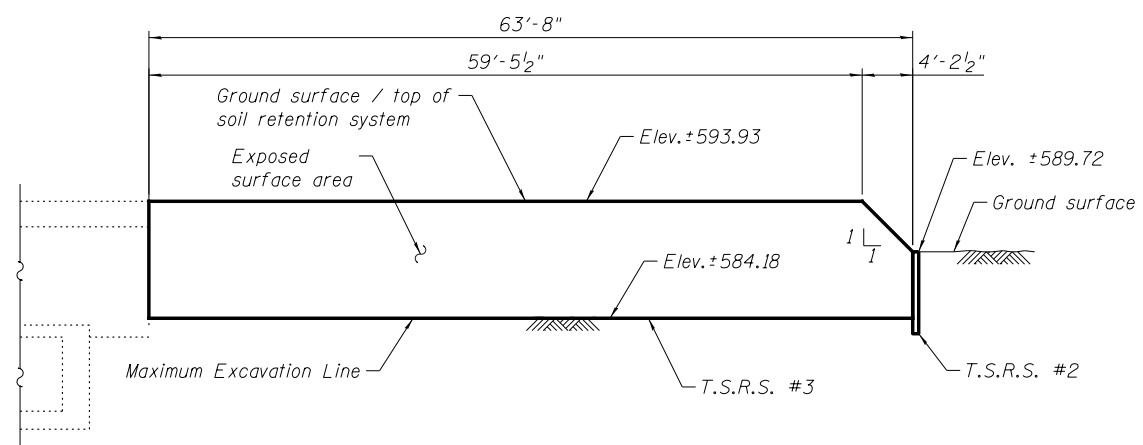
| | | | | |
|---------------------------|---------------------|-------------|------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 158 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



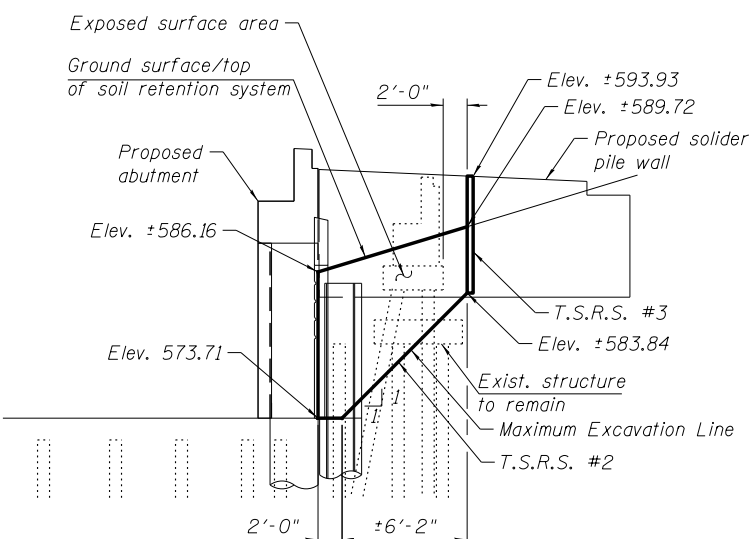
TEMPORARY SOIL RETENTION SYSTEM AT WEST ABUTMENT
(Looking West)



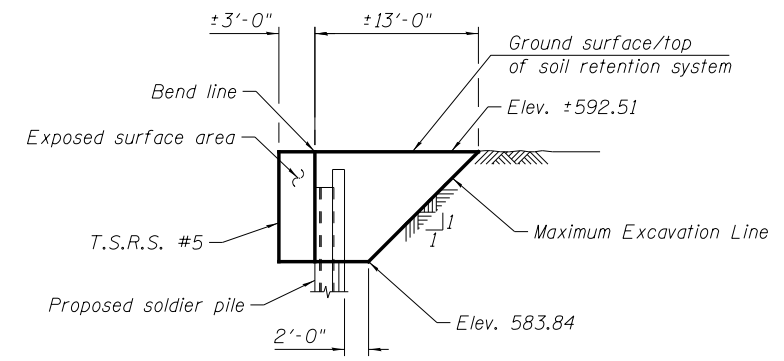
SECTION A-A



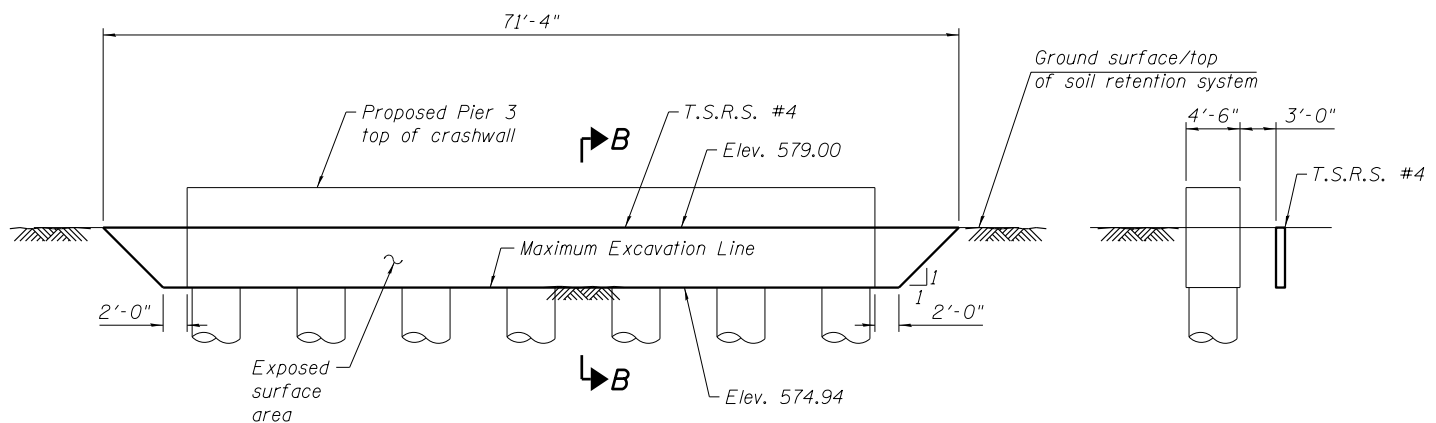
TEMPORARY SOIL RETENTION SYSTEM AT EAST ABUTMENT
(Looking East)



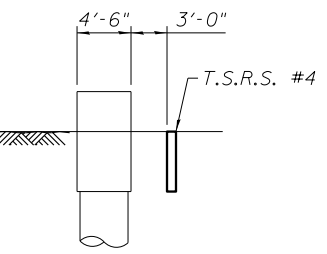
TEMPORARY SOIL RETENTION SYSTEM THRU EAST ABUTMENT
(Looking East)



TEMPORARY SOIL RETENTION SYSTEM AT SOUTH EAST WINGWALL
(Unfolded Elevation, Looking North)



TEMPORARY SOIL RETENTION SYSTEM AT PIER 3
(Looking East)



SECTION B-B

BILL OF MATERIAL

| Item | Unit | Quantity |
|---------------------------------|---------|----------|
| Temporary Soil Retention System | Sq. Ft. | 1,525 |

Notes:

- See sheet S1-5 & S1-6 of S1-60 for existing structure removal details.
- A cantilevered sheet piling design does not appear feasible and additional members or other retention systems may be necessary. The Contractor shall submit a temporary soil retention system design including plan details and calculations for review and acceptance by the Engineer.
- Slope at top of soil retention system to match slope of existing slope wall.
- The maximum allowable excavation slope is 1:1 (V:H).
- T.S.R.S. denotes Temporary Soil Retention System.

0161700-60X95-5007-SRS.dgn

PARSONS BRINCKERHOFF

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

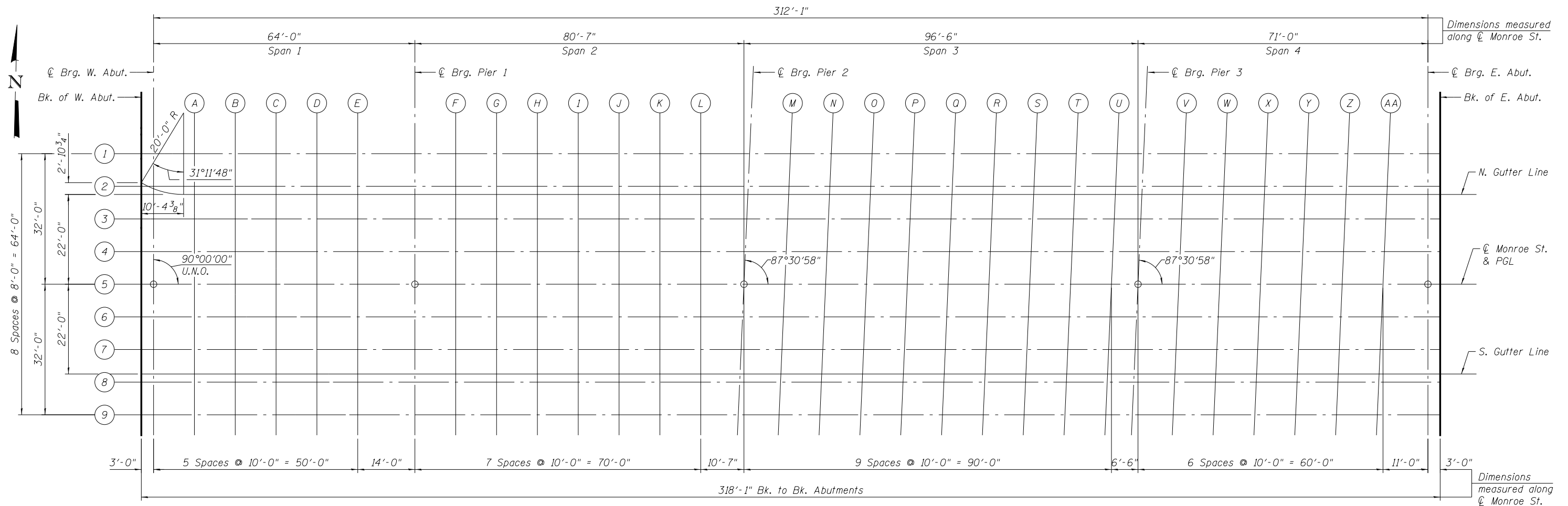
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY SOIL RETENTION SYSTEM
STRUCTURE NO. 016-1700**

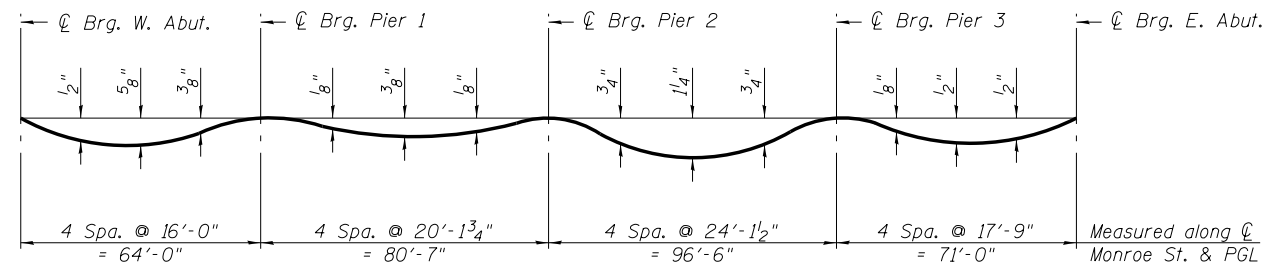
SHEET NO. S1-07 OF S1-60 SHEETS

| | | | | |
|---------------------------|---------------------|-------------|------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 159 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT



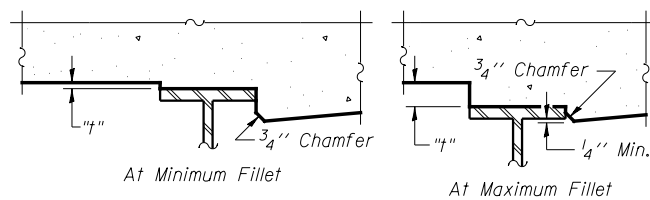
PLAN



DEAD LOAD DEFLECTION DIAGRAM

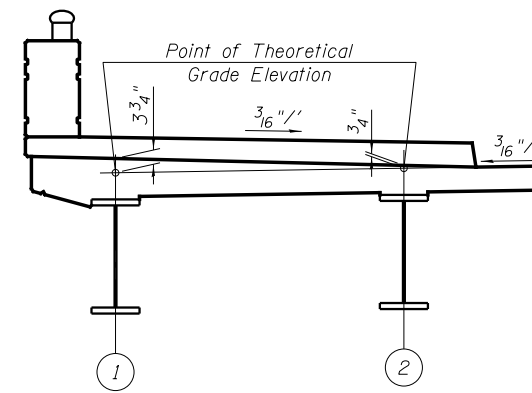
(Includes weight of concrete only.)

Note:
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown in tables. See sheets S1-9 thru S1-12 of S1-60.

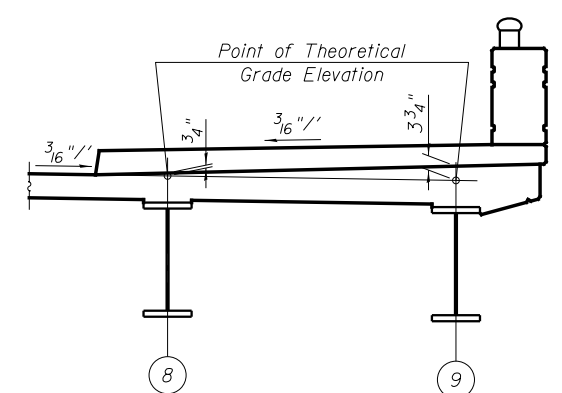


FILLET HEIGHTS

To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals in tables. See sheets S1-9 thru S1-12 of S1-60. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets S1-9 thru S1-12 of S1-60, minus slab thickness, equals the fillet heights "t" above top flange of beams.



LOCATION OF ELEVATION UNDER NORTH SIDEWALK



LOCATION OF ELEVATION UNDER SOUTH SIDEWALK

0161700-60X95-5008-TSE.dgn

PARSONS BRINCKERHOFF

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS PLAN
STRUCTURE NO. 016-1700**

SHEET NO. S1-08 OF S1-60 SHEETS

| | | | | |
|---------------------------|---------------------|-------------|------------------|---------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 160 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

GIRDER 1

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | -32.00 | 597.41 | 597.41 |
| ☉ Brg. W. Abut. | 8412+01.30 | -32.00 | 597.53 | 597.53 |
| A | 8412+11.30 | -32.00 | 597.93 | 597.96 |
| B | 8412+21.30 | -32.00 | 598.33 | 598.38 |
| C | 8412+31.30 | -32.00 | 598.72 | 598.77 |
| D | 8412+41.30 | -32.00 | 599.07 | 599.11 |
| E | 8412+51.30 | -32.00 | 599.39 | 599.41 |
| ☉ Brg. Pier 1 | 8412+65.30 | -32.00 | 599.78 | 599.78 |
| F | 8412+75.30 | -32.00 | 600.02 | 600.02 |
| G | 8412+85.30 | -32.00 | 600.23 | 600.24 |
| H | 8412+95.30 | -32.00 | 600.40 | 600.42 |
| I | 8413+05.30 | -32.00 | 600.54 | 600.57 |
| J | 8413+15.30 | -32.00 | 600.65 | 600.67 |
| K | 8413+25.30 | -32.00 | 600.72 | 600.73 |
| L | 8413+35.30 | -32.00 | 600.76 | 600.76 |
| ☉ Brg. Pier 2 | 8413+47.27 | -32.00 | 600.76 | 600.76 |
| M | 8413+57.27 | -32.00 | 600.73 | 600.75 |
| N | 8413+67.27 | -32.00 | 600.66 | 600.71 |
| O | 8413+77.27 | -32.00 | 600.56 | 600.64 |
| P | 8413+87.27 | -32.00 | 600.43 | 600.52 |
| Q | 8413+97.27 | -32.00 | 600.26 | 600.36 |
| R | 8414+07.27 | -32.00 | 600.06 | 600.15 |
| S | 8414+17.27 | -32.00 | 599.83 | 599.89 |
| T | 8414+27.27 | -32.00 | 599.56 | 599.60 |
| U | 8414+37.27 | -32.00 | 599.26 | 599.27 |
| ☉ Brg. Pier 3 | 8414+43.77 | -32.00 | 599.05 | 599.05 |
| V | 8414+53.77 | -32.00 | 598.69 | 598.69 |
| W | 8414+63.77 | -32.00 | 598.30 | 598.31 |
| X | 8414+73.77 | -32.00 | 597.88 | 597.90 |
| Y | 8414+83.77 | -32.00 | 597.42 | 597.46 |
| Z | 8414+93.77 | -32.00 | 596.93 | 596.97 |
| AA | 8415+03.77 | -32.00 | 596.43 | 596.46 |
| ☉ Brg. E. Abut. | 8415+13.38 | -32.00 | 595.95 | 595.95 |
| Bk. of E. Abut. | 8415+16.38 | -32.00 | 595.80 | 595.80 |

GIRDER 2

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | -24.00 | 597.53 | 597.53 |
| ☉ Brg. W. Abut. | 8412+01.30 | -24.00 | 597.65 | 597.65 |
| A | 8412+11.30 | -24.00 | 598.05 | 598.08 |
| B | 8412+21.30 | -24.00 | 598.45 | 598.50 |
| C | 8412+31.30 | -24.00 | 598.84 | 598.89 |
| D | 8412+41.30 | -24.00 | 599.19 | 599.23 |
| E | 8412+51.30 | -24.00 | 599.51 | 599.54 |
| ☉ Brg. Pier 1 | 8412+65.30 | -24.00 | 599.91 | 599.91 |
| F | 8412+75.30 | -24.00 | 600.15 | 600.15 |
| G | 8412+85.30 | -24.00 | 600.35 | 600.37 |
| H | 8412+95.30 | -24.00 | 600.53 | 600.55 |
| I | 8413+05.30 | -24.00 | 600.67 | 600.69 |
| J | 8413+15.30 | -24.00 | 600.77 | 600.79 |
| K | 8413+25.30 | -24.00 | 600.84 | 600.86 |
| L | 8413+35.30 | -24.00 | 600.88 | 600.89 |
| ☉ Brg. Pier 2 | 8413+46.92 | -24.00 | 600.89 | 600.89 |
| M | 8413+56.92 | -24.00 | 600.86 | 600.87 |
| N | 8413+66.92 | -24.00 | 600.79 | 600.84 |
| O | 8413+76.92 | -24.00 | 600.69 | 600.76 |
| P | 8413+86.92 | -24.00 | 600.56 | 600.65 |
| Q | 8413+96.92 | -24.00 | 600.39 | 600.49 |
| R | 8414+06.92 | -24.00 | 600.19 | 600.28 |
| S | 8414+16.92 | -24.00 | 599.96 | 600.03 |
| T | 8414+26.92 | -24.00 | 599.69 | 599.73 |
| U | 8414+36.92 | -24.00 | 599.39 | 599.41 |
| ☉ Brg. Pier 3 | 8414+43.42 | -24.00 | 599.18 | 599.18 |
| V | 8414+53.42 | -24.00 | 598.83 | 598.83 |
| W | 8414+63.42 | -24.00 | 598.44 | 598.45 |
| X | 8414+73.42 | -24.00 | 598.02 | 598.04 |
| Y | 8414+83.42 | -24.00 | 597.56 | 597.60 |
| Z | 8414+93.42 | -24.00 | 597.08 | 597.12 |
| AA | 8415+03.42 | -24.00 | 596.58 | 596.60 |
| ☉ Brg. E. Abut. | 8415+13.38 | -24.00 | 596.08 | 596.08 |
| Bk. of E. Abut. | 8415+16.38 | -24.00 | 595.93 | 595.93 |

NORTH GUTTER LINE

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | -24.89 | 597.52 | 597.52 |
| ☉ Brg. W. Abut. | 8412+01.30 | -23.40 | 597.66 | 597.66 |
| A | 8412+11.30 | -22.00 | 598.08 | 598.12 |
| B | 8412+21.30 | -22.00 | 598.49 | 598.53 |
| C | 8412+31.30 | -22.00 | 598.87 | 598.92 |
| D | 8412+41.30 | -22.00 | 599.23 | 599.27 |
| E | 8412+51.30 | -22.00 | 599.55 | 599.57 |
| ☉ Brg. Pier 1 | 8412+65.30 | -22.00 | 599.94 | 599.94 |
| F | 8412+75.30 | -22.00 | 600.18 | 600.18 |
| G | 8412+85.30 | -22.00 | 600.38 | 600.40 |
| H | 8412+95.30 | -22.00 | 600.56 | 600.58 |
| I | 8413+05.30 | -22.00 | 600.70 | 600.72 |
| J | 8413+15.30 | -22.00 | 600.80 | 600.82 |
| K | 8413+25.30 | -22.00 | 600.88 | 600.89 |
| L | 8413+35.30 | -22.00 | 600.92 | 600.92 |
| ☉ Brg. Pier 2 | 8413+46.84 | -22.00 | 600.92 | 600.92 |
| M | 8413+56.84 | -22.00 | 600.89 | 600.91 |
| N | 8413+66.84 | -22.00 | 600.82 | 600.87 |
| O | 8413+76.84 | -22.00 | 600.72 | 600.80 |
| P | 8413+86.84 | -22.00 | 600.59 | 600.69 |
| Q | 8413+96.84 | -22.00 | 600.43 | 600.52 |
| R | 8414+06.84 | -22.00 | 600.23 | 600.32 |
| S | 8414+16.84 | -22.00 | 599.99 | 600.06 |
| T | 8414+26.84 | -22.00 | 599.73 | 599.77 |
| U | 8414+36.84 | -22.00 | 599.43 | 599.44 |
| ☉ Brg. Pier 3 | 8414+43.34 | -22.00 | 599.22 | 599.22 |
| V | 8414+53.34 | -22.00 | 598.86 | 598.86 |
| W | 8414+63.34 | -22.00 | 598.47 | 598.49 |
| X | 8414+73.34 | -22.00 | 598.05 | 598.08 |
| Y | 8414+83.34 | -22.00 | 597.60 | 597.64 |
| Z | 8414+93.34 | -22.00 | 597.11 | 597.15 |
| AA | 8415+03.34 | -22.00 | 596.61 | 596.64 |
| ☉ Brg. E. Abut. | 8415+13.38 | -22.00 | 596.11 | 596.11 |
| Bk. of E. Abut. | 8415+16.38 | -22.00 | 595.96 | 595.96 |

0161700-60X95-5009-TSE.dgn

**PARSONS
BRINCKERHOFF**

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS I
STRUCTURE NO. 016-1700**

SHEET NO. S1-09 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 161 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

GIRDER 3

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | -16.00 | 597.66 | 597.66 |
| ☉ Brg. W. Abut. | 8412+01.30 | -16.00 | 597.78 | 597.78 |
| A | 8412+11.30 | -16.00 | 598.18 | 598.21 |
| B | 8412+21.30 | -16.00 | 598.58 | 598.63 |
| C | 8412+31.30 | -16.00 | 598.97 | 599.02 |
| D | 8412+41.30 | -16.00 | 599.32 | 599.36 |
| E | 8412+51.30 | -16.00 | 599.64 | 599.66 |
| ☉ Brg. Pier 1 | 8412+65.30 | -16.00 | 600.03 | 600.03 |
| F | 8412+75.30 | -16.00 | 600.27 | 600.27 |
| G | 8412+85.30 | -16.00 | 600.48 | 600.49 |
| H | 8412+95.30 | -16.00 | 600.65 | 600.67 |
| I | 8413+05.30 | -16.00 | 600.79 | 600.82 |
| J | 8413+15.30 | -16.00 | 600.90 | 600.92 |
| K | 8413+25.30 | -16.00 | 600.97 | 600.98 |
| L | 8413+35.30 | -16.00 | 601.01 | 601.01 |
| ☉ Brg. Pier 2 | 8413+46.58 | -16.00 | 601.01 | 601.01 |
| M | 8413+56.58 | -16.00 | 600.98 | 601.00 |
| N | 8413+66.58 | -16.00 | 600.92 | 600.97 |
| O | 8413+76.58 | -16.00 | 600.82 | 600.89 |
| P | 8413+86.58 | -16.00 | 600.69 | 600.78 |
| Q | 8413+96.58 | -16.00 | 600.52 | 600.62 |
| R | 8414+06.58 | -16.00 | 600.33 | 600.42 |
| S | 8414+16.58 | -16.00 | 600.09 | 600.16 |
| T | 8414+26.58 | -16.00 | 599.83 | 599.87 |
| U | 8414+36.58 | -16.00 | 599.53 | 599.55 |
| ☉ Brg. Pier 3 | 8414+43.08 | -16.00 | 599.32 | 599.32 |
| V | 8414+53.08 | -16.00 | 598.96 | 598.97 |
| W | 8414+63.08 | -16.00 | 598.58 | 598.59 |
| X | 8414+73.08 | -16.00 | 598.16 | 598.18 |
| Y | 8414+83.08 | -16.00 | 597.70 | 597.74 |
| Z | 8414+93.08 | -16.00 | 597.22 | 597.26 |
| AA | 8415+03.08 | -16.00 | 596.72 | 596.74 |
| ☉ Brg. E. Abut. | 8415+13.38 | -16.00 | 596.20 | 596.20 |
| Bk. of E. Abut. | 8415+16.38 | -16.00 | 596.05 | 596.05 |

GIRDER 4

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | -8.00 | 597.78 | 597.78 |
| ☉ Brg. W. Abut. | 8412+01.30 | -8.00 | 597.90 | 597.90 |
| A | 8412+11.30 | -8.00 | 598.30 | 598.34 |
| B | 8412+21.30 | -8.00 | 598.70 | 598.76 |
| C | 8412+31.30 | -8.00 | 599.09 | 599.15 |
| D | 8412+41.30 | -8.00 | 599.44 | 599.49 |
| E | 8412+51.30 | -8.00 | 599.76 | 599.79 |
| ☉ Brg. Pier 1 | 8412+65.30 | -8.00 | 600.16 | 600.16 |
| F | 8412+75.30 | -8.00 | 600.40 | 600.40 |
| G | 8412+85.30 | -8.00 | 600.60 | 600.62 |
| H | 8412+95.30 | -8.00 | 600.78 | 600.80 |
| I | 8413+05.30 | -8.00 | 600.91 | 600.94 |
| J | 8413+15.30 | -8.00 | 601.02 | 601.04 |
| K | 8413+25.30 | -8.00 | 601.09 | 601.10 |
| L | 8413+35.30 | -8.00 | 601.13 | 601.14 |
| ☉ Brg. Pier 2 | 8413+46.23 | -8.00 | 601.14 | 601.14 |
| M | 8413+56.23 | -8.00 | 601.11 | 601.13 |
| N | 8413+66.23 | -8.00 | 601.05 | 601.09 |
| O | 8413+76.23 | -8.00 | 600.95 | 601.02 |
| P | 8413+86.23 | -8.00 | 600.82 | 600.92 |
| Q | 8413+96.23 | -8.00 | 600.65 | 600.76 |
| R | 8414+06.23 | -8.00 | 600.46 | 600.55 |
| S | 8414+16.23 | -8.00 | 600.23 | 600.30 |
| T | 8414+26.23 | -8.00 | 599.96 | 600.00 |
| U | 8414+36.23 | -8.00 | 599.67 | 599.68 |
| ☉ Brg. Pier 3 | 8414+42.73 | -8.00 | 599.45 | 599.45 |
| V | 8414+52.73 | -8.00 | 599.10 | 599.10 |
| W | 8414+62.73 | -8.00 | 598.72 | 598.73 |
| X | 8414+72.73 | -8.00 | 598.30 | 598.33 |
| Y | 8414+82.73 | -8.00 | 597.85 | 597.89 |
| Z | 8414+92.73 | -8.00 | 597.36 | 597.41 |
| AA | 8415+02.73 | -8.00 | 596.86 | 596.89 |
| ☉ Brg. E. Abut. | 8415+13.38 | -8.00 | 596.33 | 596.33 |
| Bk. of E. Abut. | 8415+16.38 | -8.00 | 596.18 | 596.18 |

GIRDER 5, ☉ MONROE STREET & P.G.L.

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | 0.00 | 597.91 | 597.91 |
| ☉ Brg. W. Abut. | 8412+01.30 | 0.00 | 598.03 | 598.03 |
| A | 8412+11.30 | 0.00 | 598.43 | 598.46 |
| B | 8412+21.30 | 0.00 | 598.83 | 598.88 |
| C | 8412+31.30 | 0.00 | 599.22 | 599.27 |
| D | 8412+41.30 | 0.00 | 599.57 | 599.61 |
| E | 8412+51.30 | 0.00 | 599.89 | 599.92 |
| ☉ Brg. Pier 1 | 8412+65.30 | 0.00 | 600.28 | 600.28 |
| F | 8412+75.30 | 0.00 | 600.52 | 600.52 |
| G | 8412+85.30 | 0.00 | 600.73 | 600.74 |
| H | 8412+95.30 | 0.00 | 600.90 | 600.92 |
| I | 8413+05.30 | 0.00 | 601.04 | 601.07 |
| J | 8413+15.30 | 0.00 | 601.15 | 601.17 |
| K | 8413+25.30 | 0.00 | 601.22 | 601.23 |
| L | 8413+35.30 | 0.00 | 601.26 | 601.26 |
| ☉ Brg. Pier 2 | 8413+45.88 | 0.00 | 601.26 | 601.26 |
| M | 8413+55.88 | 0.00 | 601.24 | 601.25 |
| N | 8413+65.88 | 0.00 | 601.17 | 601.22 |
| O | 8413+75.88 | 0.00 | 601.08 | 601.15 |
| P | 8413+85.88 | 0.00 | 600.95 | 601.05 |
| Q | 8413+95.88 | 0.00 | 600.79 | 600.89 |
| R | 8414+05.88 | 0.00 | 600.59 | 600.69 |
| S | 8414+15.88 | 0.00 | 600.36 | 600.43 |
| T | 8414+25.88 | 0.00 | 600.10 | 600.14 |
| U | 8414+35.88 | 0.00 | 599.80 | 599.82 |
| ☉ Brg. Pier 3 | 8414+42.38 | 0.00 | 599.59 | 599.59 |
| V | 8414+52.38 | 0.00 | 599.24 | 599.24 |
| W | 8414+62.38 | 0.00 | 598.86 | 598.88 |
| X | 8414+72.38 | 0.00 | 598.44 | 598.47 |
| Y | 8414+82.38 | 0.00 | 597.99 | 598.03 |
| Z | 8414+92.38 | 0.00 | 597.50 | 597.55 |
| AA | 8415+02.38 | 0.00 | 597.00 | 597.04 |
| ☉ Brg. E. Abut. | 8415+13.38 | 0.00 | 596.45 | 596.45 |
| Bk. of E. Abut. | 8415+16.38 | 0.00 | 596.30 | 596.30 |

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**PARSONS
BRINCKERHOFF**

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS II
STRUCTURE NO. 016-1700**

SHEET NO. S1-10 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 162 |
| | | | CONTRACT NO. 60X95 | |
| ILLINOIS FED. AID PROJECT | | | | |

GIRDER 6

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | 8.00 | 597.78 | 597.78 |
| ⊕ Brg. W. Abut. | 8412+01.30 | 8.00 | 597.90 | 597.90 |
| A | 8412+11.30 | 8.00 | 598.30 | 598.34 |
| B | 8412+21.30 | 8.00 | 598.70 | 598.76 |
| C | 8412+31.30 | 8.00 | 599.09 | 599.15 |
| D | 8412+41.30 | 8.00 | 599.44 | 599.49 |
| E | 8412+51.30 | 8.00 | 599.76 | 599.79 |
| ⊕ Brg. Pier 1 | 8412+65.30 | 8.00 | 600.16 | 600.16 |
| F | 8412+75.30 | 8.00 | 600.40 | 600.40 |
| G | 8412+85.30 | 8.00 | 600.60 | 600.62 |
| H | 8412+95.30 | 8.00 | 600.78 | 600.80 |
| I | 8413+05.30 | 8.00 | 600.91 | 600.94 |
| J | 8413+15.30 | 8.00 | 601.02 | 601.04 |
| K | 8413+25.30 | 8.00 | 601.09 | 601.11 |
| L | 8413+35.30 | 8.00 | 601.13 | 601.14 |
| ⊕ Brg. Pier 2 | 8413+45.54 | 8.00 | 601.14 | 601.14 |
| M | 8413+55.54 | 8.00 | 601.11 | 601.13 |
| N | 8413+65.54 | 8.00 | 601.05 | 601.10 |
| O | 8413+75.54 | 8.00 | 600.96 | 601.03 |
| P | 8413+85.54 | 8.00 | 600.83 | 600.93 |
| Q | 8413+95.54 | 8.00 | 600.67 | 600.77 |
| R | 8414+05.54 | 8.00 | 600.47 | 600.57 |
| S | 8414+15.54 | 8.00 | 600.24 | 600.31 |
| T | 8414+25.54 | 8.00 | 599.98 | 600.02 |
| U | 8414+35.54 | 8.00 | 599.69 | 599.70 |
| ⊕ Brg. Pier 3 | 8414+42.04 | 8.00 | 599.48 | 599.48 |
| V | 8414+52.04 | 8.00 | 599.13 | 599.13 |
| W | 8414+62.04 | 8.00 | 598.74 | 598.77 |
| X | 8414+72.04 | 8.00 | 598.33 | 598.36 |
| Y | 8414+82.04 | 8.00 | 597.88 | 597.93 |
| Z | 8414+92.04 | 8.00 | 597.39 | 597.45 |
| AA | 8415+02.04 | 8.00 | 596.89 | 596.93 |
| ⊕ Brg. E. Abut. | 8415+13.38 | 8.00 | 596.33 | 596.33 |
| Bk. of E. Abut. | 8415+16.38 | 8.00 | 596.18 | 596.18 |

GIRDER 7

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | 16.00 | 597.66 | 597.66 |
| ⊕ Brg. W. Abut. | 8412+01.30 | 16.00 | 597.78 | 597.78 |
| A | 8412+11.30 | 16.00 | 598.18 | 598.21 |
| B | 8412+21.30 | 16.00 | 598.58 | 598.63 |
| C | 8412+31.30 | 16.00 | 598.97 | 599.02 |
| D | 8412+41.30 | 16.00 | 599.32 | 599.36 |
| E | 8412+51.30 | 16.00 | 599.64 | 599.67 |
| ⊕ Brg. Pier 1 | 8412+65.30 | 16.00 | 600.03 | 600.03 |
| F | 8412+75.30 | 16.00 | 600.27 | 600.27 |
| G | 8412+85.30 | 16.00 | 600.48 | 600.49 |
| H | 8412+95.30 | 16.00 | 600.65 | 600.67 |
| I | 8413+05.30 | 16.00 | 600.79 | 600.82 |
| J | 8413+15.30 | 16.00 | 600.90 | 600.92 |
| K | 8413+25.30 | 16.00 | 600.97 | 600.98 |
| L | 8413+35.30 | 16.00 | 601.01 | 601.01 |
| ⊕ Brg. Pier 2 | 8413+45.19 | 16.00 | 601.02 | 601.02 |
| M | 8413+55.19 | 16.00 | 600.99 | 601.01 |
| N | 8413+65.19 | 16.00 | 600.93 | 600.98 |
| O | 8413+75.19 | 16.00 | 600.84 | 600.91 |
| P | 8413+85.19 | 16.00 | 600.71 | 600.80 |
| Q | 8413+95.19 | 16.00 | 600.55 | 600.65 |
| R | 8414+05.19 | 16.00 | 600.35 | 600.44 |
| S | 8414+15.19 | 16.00 | 600.13 | 600.19 |
| T | 8414+25.19 | 16.00 | 599.87 | 599.90 |
| U | 8414+35.19 | 16.00 | 599.57 | 599.59 |
| ⊕ Brg. Pier 3 | 8414+41.69 | 16.00 | 599.36 | 599.36 |
| V | 8414+51.69 | 16.00 | 599.02 | 599.02 |
| W | 8414+61.69 | 16.00 | 598.63 | 598.66 |
| X | 8414+71.69 | 16.00 | 598.22 | 598.25 |
| Y | 8414+81.69 | 16.00 | 597.77 | 597.82 |
| Z | 8414+91.69 | 16.00 | 597.29 | 597.34 |
| AA | 8415+01.69 | 16.00 | 596.79 | 596.82 |
| ⊕ Brg. E. Abut. | 8415+13.38 | 16.00 | 596.20 | 596.20 |
| Bk. of E. Abut. | 8415+16.38 | 16.00 | 596.05 | 596.05 |

SOUTH GUTTER LINE

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | 22.00 | 597.56 | 597.56 |
| ⊕ Brg. W. Abut. | 8412+01.30 | 22.00 | 597.68 | 597.68 |
| A | 8412+11.30 | 22.00 | 598.08 | 598.12 |
| B | 8412+21.30 | 22.00 | 598.49 | 598.54 |
| C | 8412+31.30 | 22.00 | 598.87 | 598.93 |
| D | 8412+41.30 | 22.00 | 599.23 | 599.27 |
| E | 8412+51.30 | 22.00 | 599.55 | 599.57 |
| ⊕ Brg. Pier 1 | 8412+65.30 | 22.00 | 599.94 | 599.94 |
| F | 8412+75.30 | 22.00 | 600.18 | 600.18 |
| G | 8412+85.30 | 22.00 | 600.38 | 600.39 |
| H | 8412+95.30 | 22.00 | 600.56 | 600.58 |
| I | 8413+05.30 | 22.00 | 600.70 | 600.72 |
| J | 8413+15.30 | 22.00 | 600.80 | 600.82 |
| K | 8413+25.30 | 22.00 | 600.88 | 600.88 |
| L | 8413+35.30 | 22.00 | 600.92 | 600.92 |
| ⊕ Brg. Pier 2 | 8413+44.93 | 22.00 | 600.92 | 600.92 |
| M | 8413+54.93 | 22.00 | 600.90 | 600.91 |
| N | 8413+64.93 | 22.00 | 600.84 | 600.88 |
| O | 8413+74.93 | 22.00 | 600.74 | 600.81 |
| P | 8413+84.93 | 22.00 | 600.62 | 600.71 |
| Q | 8413+94.93 | 22.00 | 600.46 | 600.55 |
| R | 8414+04.93 | 22.00 | 600.27 | 600.35 |
| S | 8414+14.93 | 22.00 | 600.04 | 600.10 |
| T | 8414+24.93 | 22.00 | 599.78 | 599.81 |
| U | 8414+34.93 | 22.00 | 599.49 | 599.50 |
| ⊕ Brg. Pier 3 | 8414+41.43 | 22.00 | 599.28 | 599.28 |
| V | 8414+51.43 | 22.00 | 598.93 | 598.94 |
| W | 8414+61.43 | 22.00 | 598.55 | 598.57 |
| X | 8414+71.43 | 22.00 | 598.14 | 598.17 |
| Y | 8414+81.43 | 22.00 | 597.69 | 597.74 |
| Z | 8414+91.43 | 22.00 | 597.21 | 597.26 |
| AA | 8415+01.43 | 22.00 | 596.71 | 596.74 |
| ⊕ Brg. E. Abut. | 8415+13.38 | 22.00 | 596.11 | 596.11 |
| Bk. of E. Abut. | 8415+16.38 | 22.00 | 595.96 | 595.96 |

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| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS III
STRUCTURE NO. 016-1700**

SHEET NO. S1-11 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 163 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

GIRDER 8

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | 24.00 | 597.53 | 597.53 |
| Ⓞ Brg. W. Abut. | 8412+01.30 | 24.00 | 597.65 | 597.65 |
| A | 8412+11.30 | 24.00 | 598.05 | 598.09 |
| B | 8412+21.30 | 24.00 | 598.45 | 598.50 |
| C | 8412+31.30 | 24.00 | 598.84 | 598.89 |
| D | 8412+41.30 | 24.00 | 599.19 | 599.24 |
| E | 8412+51.30 | 24.00 | 599.51 | 599.54 |
| Ⓞ Brg. Pier 1 | 8412+65.30 | 24.00 | 599.91 | 599.91 |
| F | 8412+75.30 | 24.00 | 600.15 | 600.15 |
| G | 8412+85.30 | 24.00 | 600.35 | 600.36 |
| H | 8412+95.30 | 24.00 | 600.53 | 600.54 |
| I | 8413+05.30 | 24.00 | 600.67 | 600.69 |
| J | 8413+15.30 | 24.00 | 600.77 | 600.79 |
| K | 8413+25.30 | 24.00 | 600.84 | 600.85 |
| L | 8413+35.30 | 24.00 | 600.88 | 600.89 |
| Ⓞ Brg. Pier 2 | 8413+44.84 | 24.00 | 600.89 | 600.89 |
| M | 8413+54.84 | 24.00 | 600.87 | 600.88 |
| N | 8413+64.84 | 24.00 | 600.81 | 600.85 |
| O | 8413+74.84 | 24.00 | 600.71 | 600.78 |
| P | 8413+84.84 | 24.00 | 600.59 | 600.68 |
| Q | 8413+94.84 | 24.00 | 600.43 | 600.52 |
| R | 8414+04.84 | 24.00 | 600.24 | 600.32 |
| S | 8414+14.84 | 24.00 | 600.01 | 600.07 |
| T | 8414+24.84 | 24.00 | 599.75 | 599.79 |
| U | 8414+34.84 | 24.00 | 599.46 | 599.47 |
| Ⓞ Brg. Pier 3 | 8414+41.34 | 24.00 | 599.25 | 599.25 |
| V | 8414+51.34 | 24.00 | 598.90 | 598.91 |
| W | 8414+61.34 | 24.00 | 598.52 | 598.55 |
| X | 8414+71.34 | 24.00 | 598.11 | 598.15 |
| Y | 8414+81.34 | 24.00 | 597.66 | 597.71 |
| Z | 8414+91.34 | 24.00 | 597.18 | 597.23 |
| AA | 8415+01.34 | 24.00 | 596.68 | 596.72 |
| Ⓞ Brg. E. Abut. | 8415+13.38 | 24.00 | 596.08 | 596.08 |
| Bk. of E. Abut. | 8415+16.38 | 24.00 | 595.93 | 595.93 |

GIRDER 9

| Location | Station | Offset | Theoretical Grade Elevations | Theoretical Grade Elevations Adjusted For Dead Load Deflection |
|-----------------|------------|--------|------------------------------|--|
| Bk. of W. Abut. | 8411+98.30 | 32.00 | 597.41 | 597.41 |
| Ⓞ Brg. W. Abut. | 8412+01.30 | 32.00 | 597.53 | 597.53 |
| A | 8412+11.30 | 32.00 | 597.93 | 597.96 |
| B | 8412+21.30 | 32.00 | 598.33 | 598.38 |
| C | 8412+31.30 | 32.00 | 598.72 | 598.77 |
| D | 8412+41.30 | 32.00 | 599.07 | 599.11 |
| E | 8412+51.30 | 32.00 | 599.39 | 599.42 |
| Ⓞ Brg. Pier 1 | 8412+65.30 | 32.00 | 599.78 | 599.78 |
| F | 8412+75.30 | 32.00 | 600.02 | 600.02 |
| G | 8412+85.30 | 32.00 | 600.23 | 600.24 |
| H | 8412+95.30 | 32.00 | 600.40 | 600.42 |
| I | 8413+05.30 | 32.00 | 600.54 | 600.56 |
| J | 8413+15.30 | 32.00 | 600.65 | 600.66 |
| K | 8413+25.30 | 32.00 | 600.72 | 600.73 |
| L | 8413+35.30 | 32.00 | 600.76 | 600.76 |
| Ⓞ Brg. Pier 2 | 8413+44.50 | 32.00 | 600.77 | 600.77 |
| M | 8413+54.50 | 32.00 | 600.74 | 600.76 |
| N | 8413+64.50 | 32.00 | 600.68 | 600.73 |
| O | 8413+74.50 | 32.00 | 600.59 | 600.66 |
| P | 8413+84.50 | 32.00 | 600.47 | 600.56 |
| Q | 8413+94.50 | 32.00 | 600.31 | 600.40 |
| R | 8414+04.50 | 32.00 | 600.12 | 600.20 |
| S | 8414+14.50 | 32.00 | 599.89 | 599.95 |
| T | 8414+24.50 | 32.00 | 599.64 | 599.67 |
| U | 8414+34.50 | 32.00 | 599.35 | 599.36 |
| Ⓞ Brg. Pier 3 | 8414+41.00 | 32.00 | 599.14 | 599.14 |
| V | 8414+51.00 | 32.00 | 598.79 | 598.80 |
| W | 8414+61.00 | 32.00 | 598.41 | 598.44 |
| X | 8414+71.00 | 32.00 | 598.00 | 598.04 |
| Y | 8414+81.00 | 32.00 | 597.55 | 597.60 |
| Z | 8414+91.00 | 32.00 | 597.07 | 597.13 |
| AA | 8415+01.00 | 32.00 | 596.57 | 596.61 |
| Ⓞ Brg. E. Abut. | 8415+13.38 | 32.00 | 595.95 | 595.95 |
| Bk. of E. Abut. | 8415+16.38 | 32.00 | 595.80 | 595.80 |

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**PARSONS
BRINCKERHOFF**

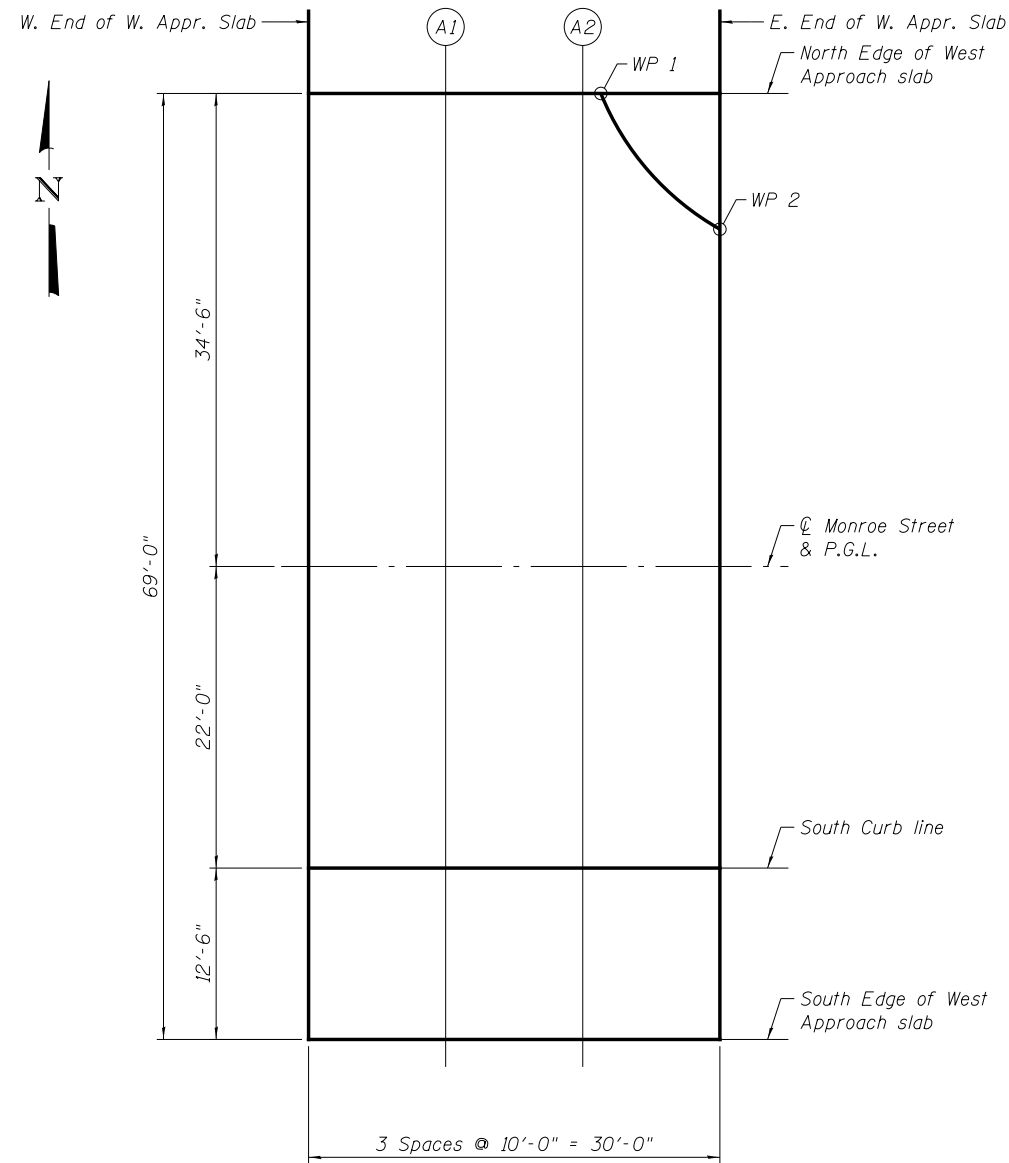
| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF SLAB ELEVATIONS IV
STRUCTURE NO. 016-1700**

SHEET NO. S1-12 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 164 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |



PLAN
(West Approach)

NORTH EDGE OF WEST APPROACH SLAB

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|---------|------------------------------|
| W. End of W. Appr. Slab | 8411+68.80 | - 34.50 | 596.18 |
| A1 | 8411+78.80 | - 34.50 | 596.59 |
| A2 | 8411+88.80 | - 34.50 | 596.99 |
| E. End of W. Appr. Slab | 8411+98.80 | - 34.50 | 597.39 |

SOUTH CURB LINE

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of W. Appr. Slab | 8411+68.80 | 22.00 | 596.38 |
| A1 | 8411+78.80 | 22.00 | 596.78 |
| A2 | 8411+88.80 | 22.00 | 597.18 |
| E. End of W. Appr. Slab | 8411+98.80 | 22.00 | 597.58 |

☉ MONROE STREET & P.G.L.

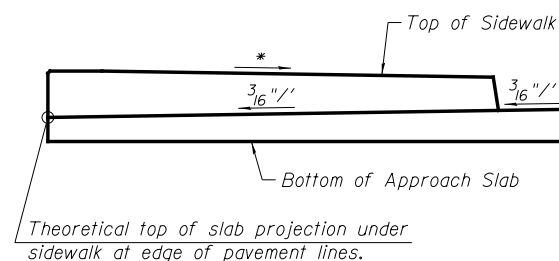
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of W. Appr. Slab | 8411+68.80 | 0.00 | 596.72 |
| A1 | 8411+78.80 | 0.00 | 597.12 |
| A2 | 8411+88.80 | 0.00 | 597.52 |
| E. End of W. Appr. Slab | 8411+98.80 | 0.00 | 597.93 |

SOUTH EDGE OF WEST APPROACH SLAB

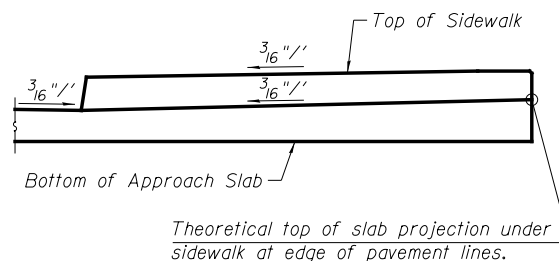
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of W. Appr. Slab | 8411+68.80 | 34.50 | 596.57 |
| A1 | 8411+78.80 | 34.50 | 596.98 |
| A2 | 8411+88.80 | 34.50 | 597.38 |
| E. End of W. Appr. Slab | 8411+98.80 | 34.50 | 597.78 |

WORK POINTS

| Location | Station | Offset | Theoretical Grade Elevations |
|----------|------------|---------|------------------------------|
| WP 1 | 8411+90.12 | - 34.50 | 597.04 |
| WP 2 | 8411+98.80 | - 23.83 | 597.55 |

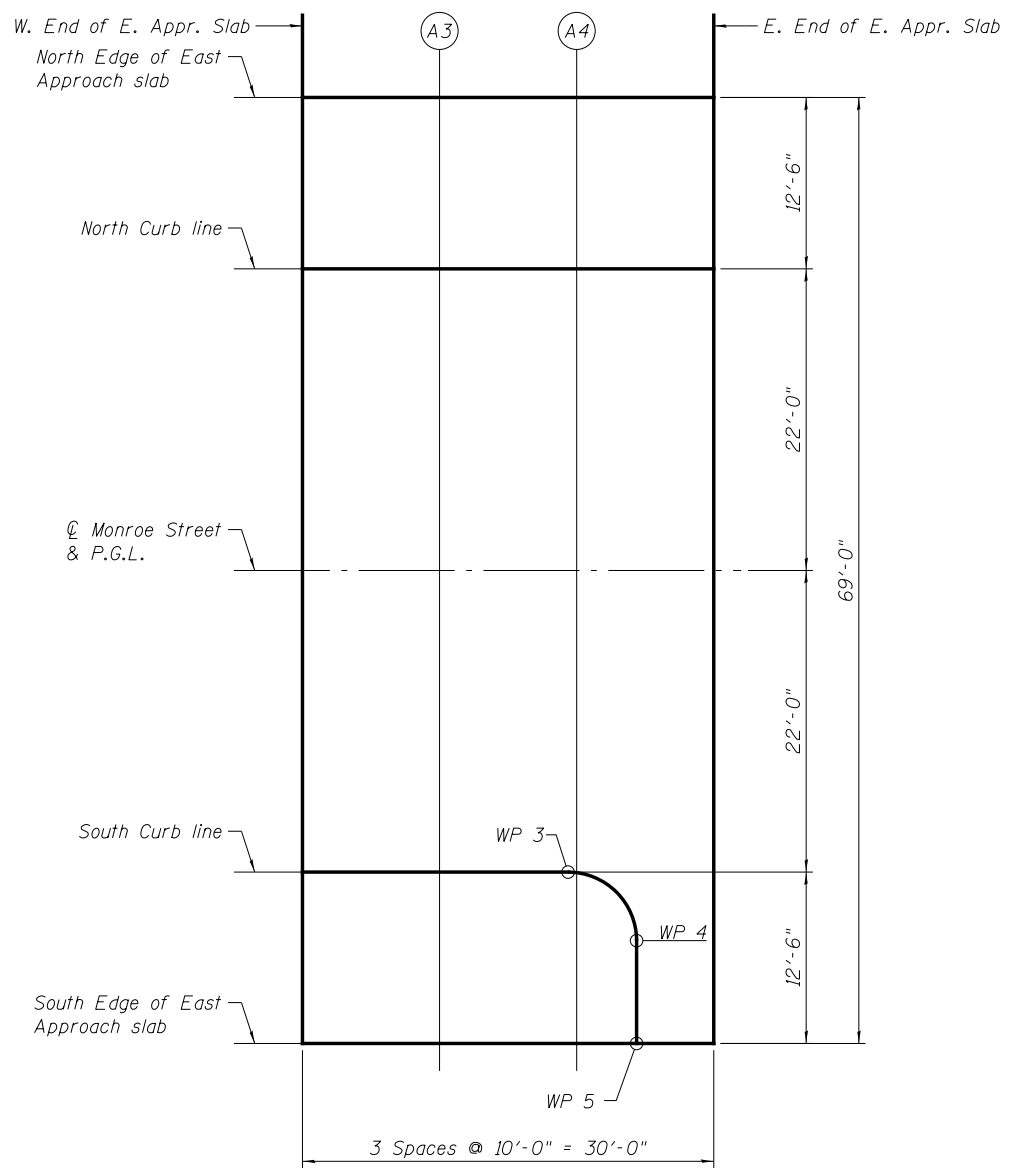


LOCATION OF ELEVATION UNDER NORTH SIDEWALK



LOCATION OF ELEVATION UNDER SOUTH SIDEWALK

* See Roadway plans for cross slope transition



PLAN
(East Approach)

NORTH EDGE OF EAST APPROACH SLAB

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of E. Appr. Slab | 8415+15.88 | -34.50 | 596.18 |
| A3 | 8415+25.88 | -34.50 | 595.69 |
| A4 | 8415+35.88 | -34.50 | 595.24 |
| E. End of E. Appr. Slab | 8415+45.88 | -34.50 | 594.85 |

SOUTH CURB LINE

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of E. Appr. Slab | 8415+15.88 | 22.00 | 595.98 |
| A3 | 8415+25.88 | 22.00 | 595.49 |
| A4 | 8415+35.88 | 22.00 | 595.05 |
| WP 3 | 8415+35.25 | 22.00 | 595.08 |
| WP 4 | 8415+40.25 | 27.00 | 595.34 |
| WP 5 | 8415+40.25 | 34.50 | 595.42 |

NORTH CURB LINE

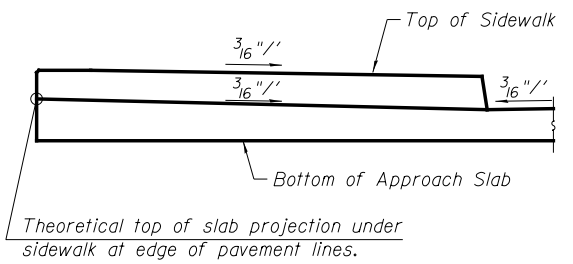
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of E. Appr. Slab | 8415+15.88 | -22.00 | 595.98 |
| A3 | 8415+25.88 | -22.00 | 595.49 |
| A4 | 8415+35.88 | -22.00 | 595.05 |
| E. End of E. Appr. Slab | 8415+45.88 | -22.00 | 594.66 |

SOUTH EDGE OF EAST APPROACH SLAB

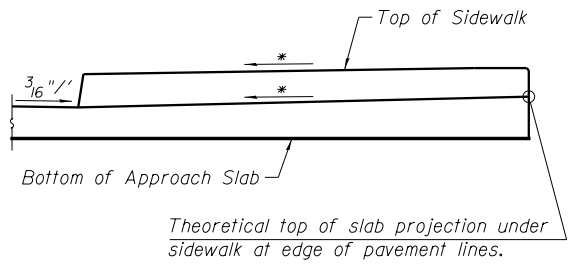
| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of E. Appr. Slab | 8415+15.88 | 34.50 | 596.18 |
| A3 | 8415+25.88 | 34.50 | 595.69 |
| A4 | 8415+35.88 | 34.50 | 595.24 |
| E. End of E. Appr. Slab | 8415+45.88 | 34.50 | 594.79 |

MONROE STREET & P.G.L.

| Location | Station | Offset | Theoretical Grade Elevations |
|-------------------------|------------|--------|------------------------------|
| W. End of E. Appr. Slab | 8415+15.88 | 0.00 | 596.33 |
| A3 | 8415+25.88 | 0.00 | 595.84 |
| A4 | 8415+35.88 | 0.00 | 595.39 |
| E. End of E. Appr. Slab | 8415+45.88 | 0.00 | 595.00 |



LOCATION OF ELEVATION UNDER NORTH SIDEWALK



LOCATION OF ELEVATION UNDER SOUTH SIDEWALK

* See Roadway plans for cross slope transition

0161700-60X95-S014-TAE.dgn

**PARSONS
BRINCKERHOFF**

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

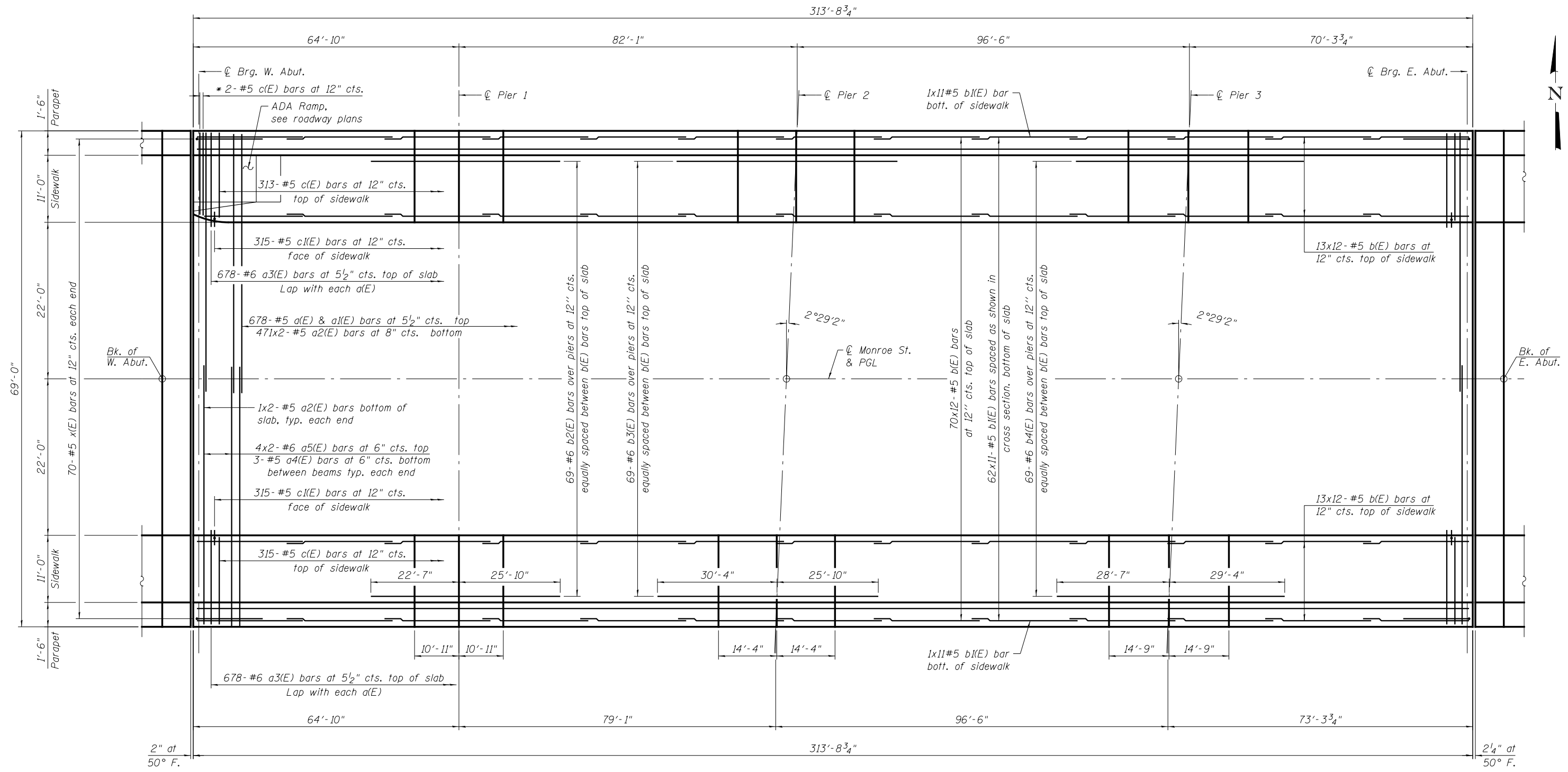
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TOP OF EAST APPROACH SLAB ELEVATION
STRUCTURE NO. 016-1700**

SHEET NO. S1-14 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 166 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT



DECK PLAN

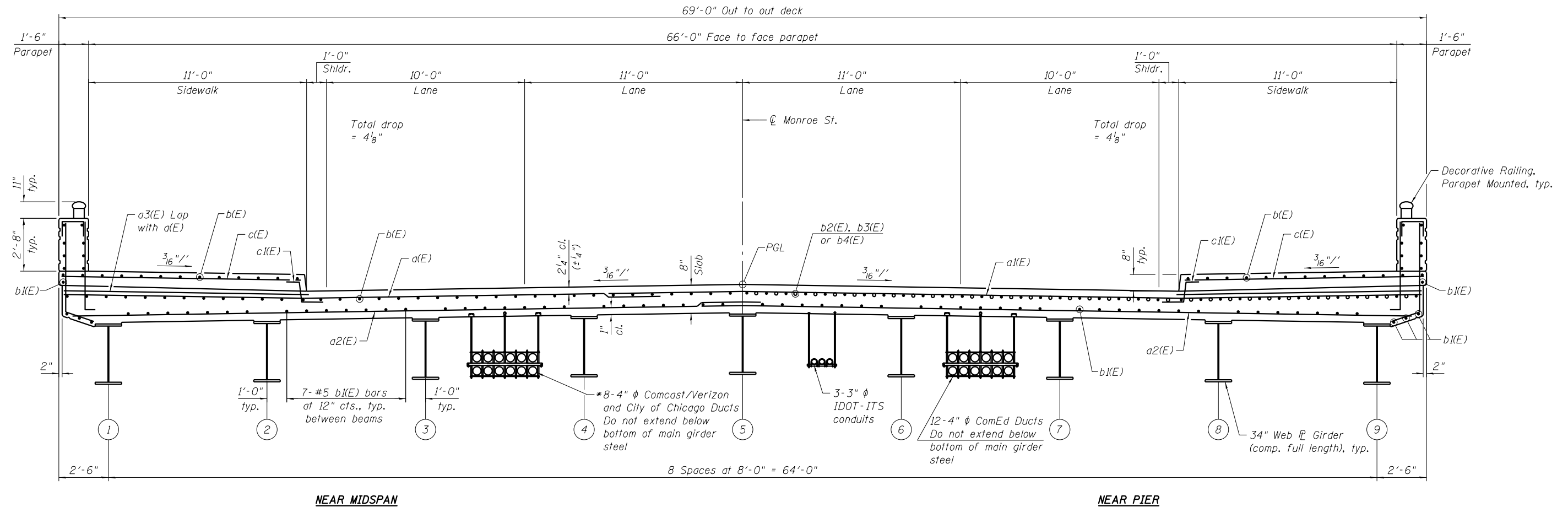
* Cut to fit

MINIMUM BAR LAP
 #5 bar = 3'-6"
 #6 bar = 3'-7"

Notes:
 See sheet S1-16 to S1-19 of S1-60 for superstructure details and Bill of Material.
 Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
 See sheet S1-17 of S1-60 for parapet reinforcement.
 See sheet S1-1 of S1-60 for Scupper location.
 See sheet S1-20 thru S1-23 of S1-60 for Approach slab details.
 Bars in the sidewalk interfering with the ADA ramp construction shall be bent and/or cut to fit based on the sidewalk depth in these areas.
 Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elect to use the welded Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details on S1-24 of S1-60.

0161700-60X95-5015-DEK.dgn

| | | | | | | | | | | |
|---------------------------------|-----------------------|---------------|-----------|---|--|---------------------------|-----------------------|---------------|---------------------------|-----------------|
| PARSONS BRINCKERHOFF | USER NAME = pateld | DESIGNED - MS | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUPERSTRUCTURE STRUCTURE NO. 016-1700 | F.A.I. RTE. = 90/94 | SECTION = 2014-016R&B | COUNTY = COOK | TOTAL SHEETS = 378 | SHEET NO. = 167 |
| | PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - | | | CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |
| | PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - | SHEET NO. S1-15 OF S1-60 SHEETS | | | | | | |



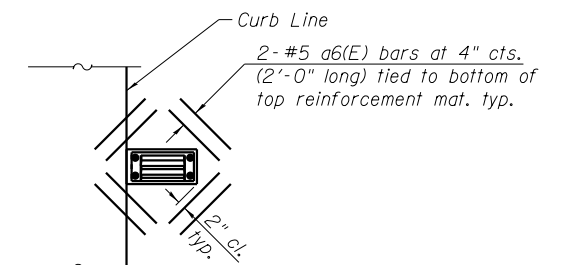
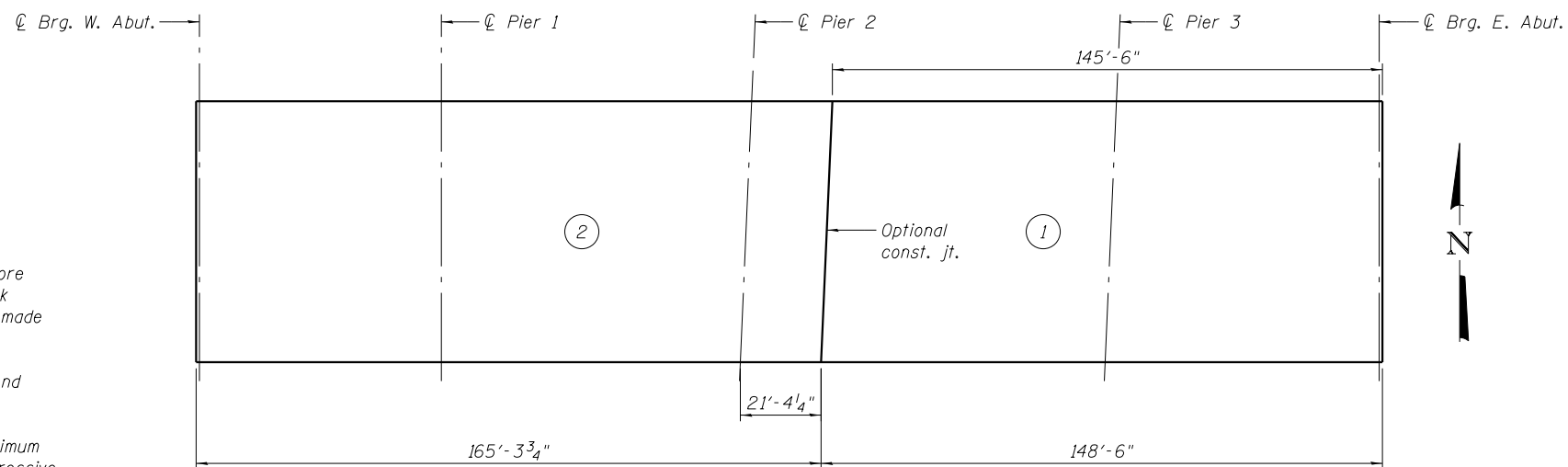
* Duct final arrangement to be determined in the field.
(8 Ducts placed within a window of 12)

DECK POURING SEQUENCE

When the deck pour is stopped for the day at one or more of the transverse Bonded Construction Joints in the Deck Pouring Sequence as shown, the next pour shall not be made until both of the following are met:

1. At least 72 hours shall have elapsed from the end of the previous pour.
2. The concrete strength shall have attained a minimum flexural strength of 650 psi or a minimum compressive strength of 4000 psi.

Concrete pours shall follow the numerical order shown in the pouring sequence. Deviation from this pour sequence requires approval from the Engineer.



Note:
Cut longitudinal reinforcement to clear drainage scuppers.

Notes:
See sheet S1-17 to S1-19 of S1-60 for superstructure details and Bill of Material.
Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
See sheet S1-17 of S1-60 for parapet reinforcement.
Details for the proposed conduit support system for Lighting/ITS are shown on the Lighting/ITS Plans. ComEd ducts and Comcast/Verizon and City of Chicago ducts will be installed by others (The concrete inserts will be provided to the Contractor). The installation location of the concrete inserts will be coordinated and determined in the field by the Contractor. There is no separate payment for the placement of the inserts. Cost of the work involved in providing and installing the inserts included with Concrete Superstructure.

0161700-60X95-5016-DET.dgn

PARSONS BRINCKERHOFF

| | | |
|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - MS | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 9/1/2017 | CHECKED - JIG | REVISED - |

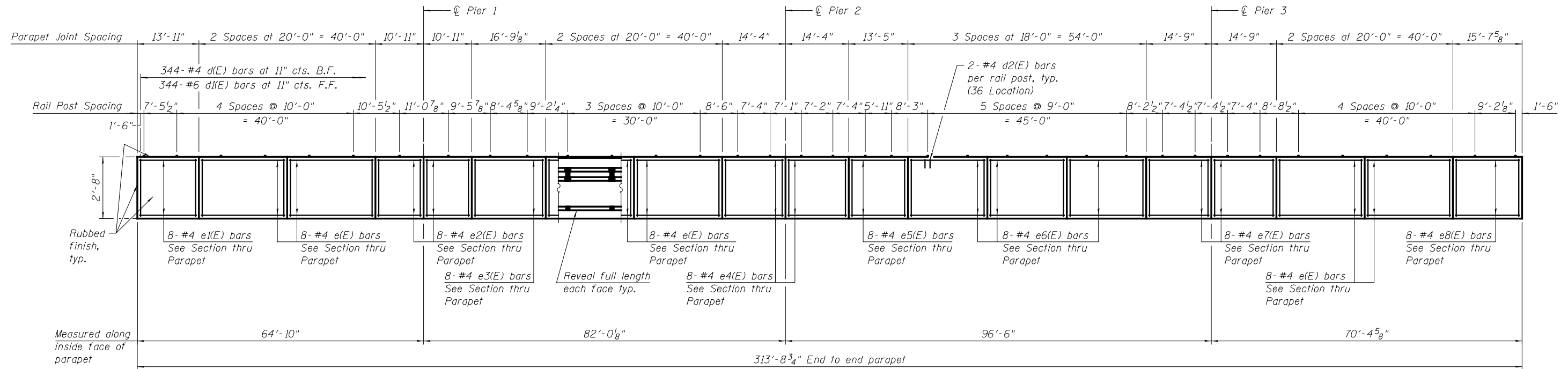
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE DETAILS I
STRUCTURE NO. 016-1700**

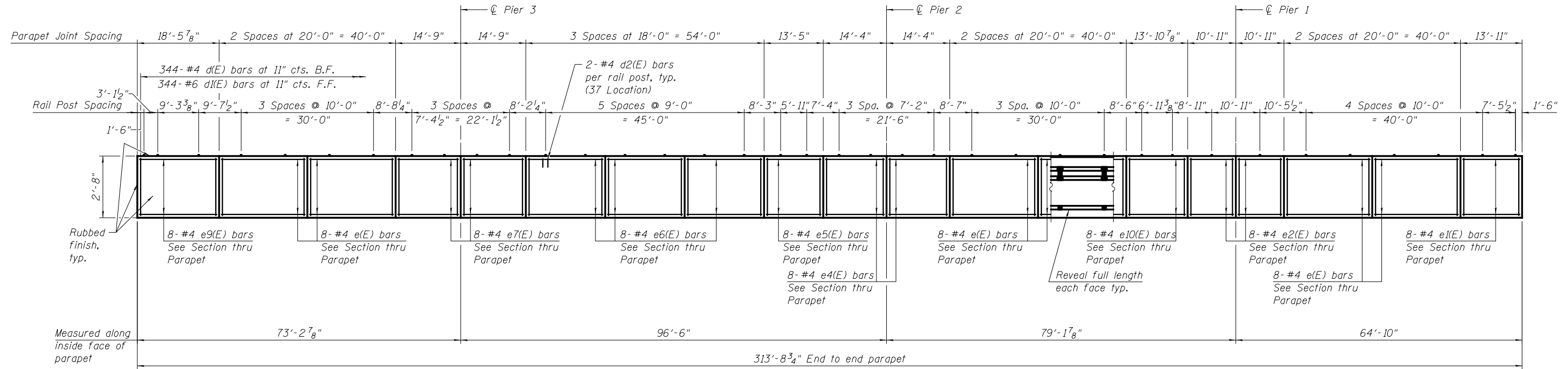
SHEET NO. S1-16 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 168 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT



INSIDE ELEVATION OF NORTH PARAPET
(Looking North)



INSIDE ELEVATION OF SOUTH PARAPET
(Looking South)

Notes:
 See sheet S1-18 of S1-60 for parapet joint details.
 See sheet S1-23 of S1-60 for architectural details on parapets and decorative railing details. All edges shall be chamfered 3/4\"/>

0161700-60X95-S017-DET.dgn

**PARSONS
BRINCKERHOFF**

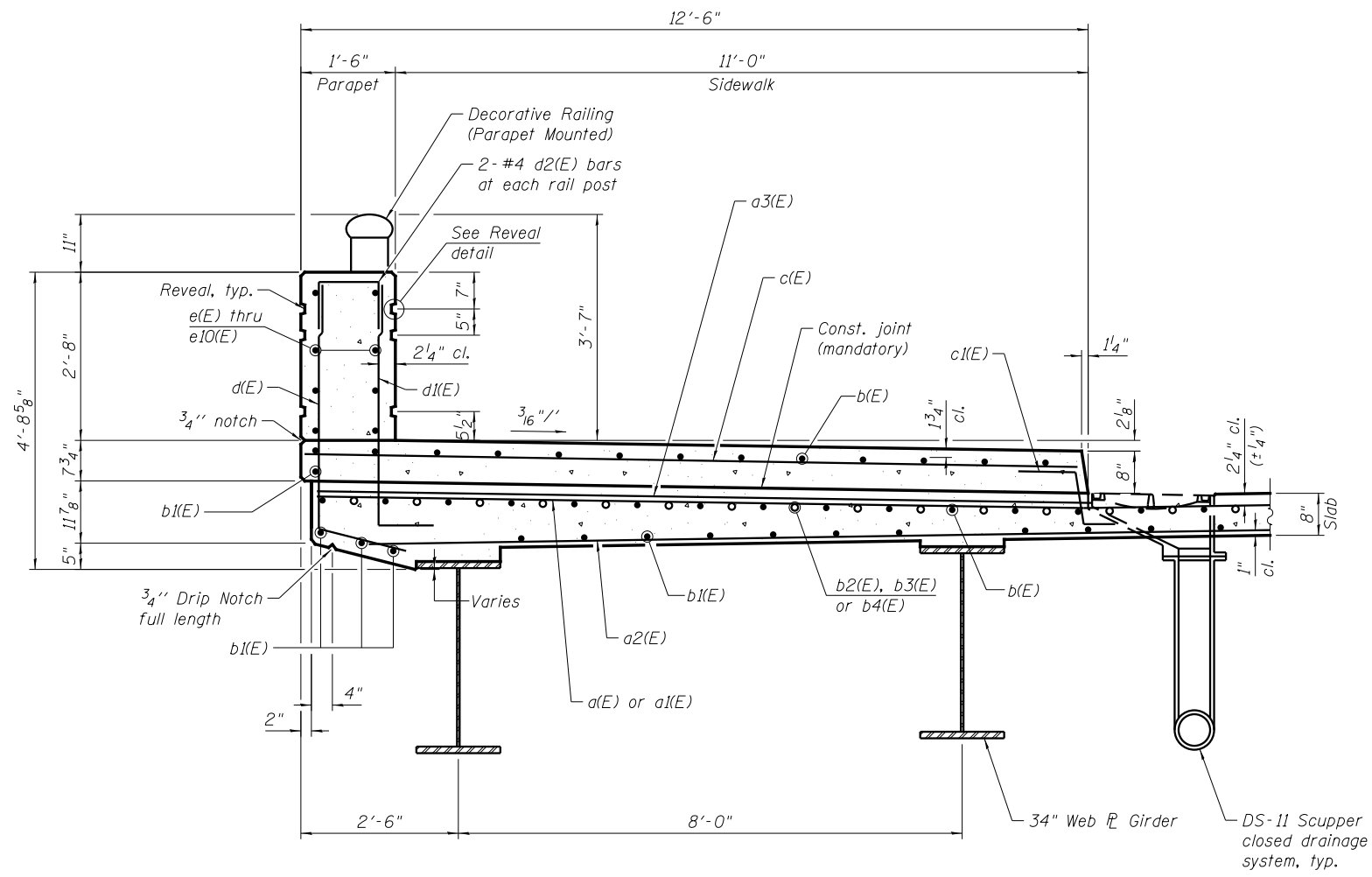
| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MS | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE DETAILS II
STRUCTURE NO. 016-1700**

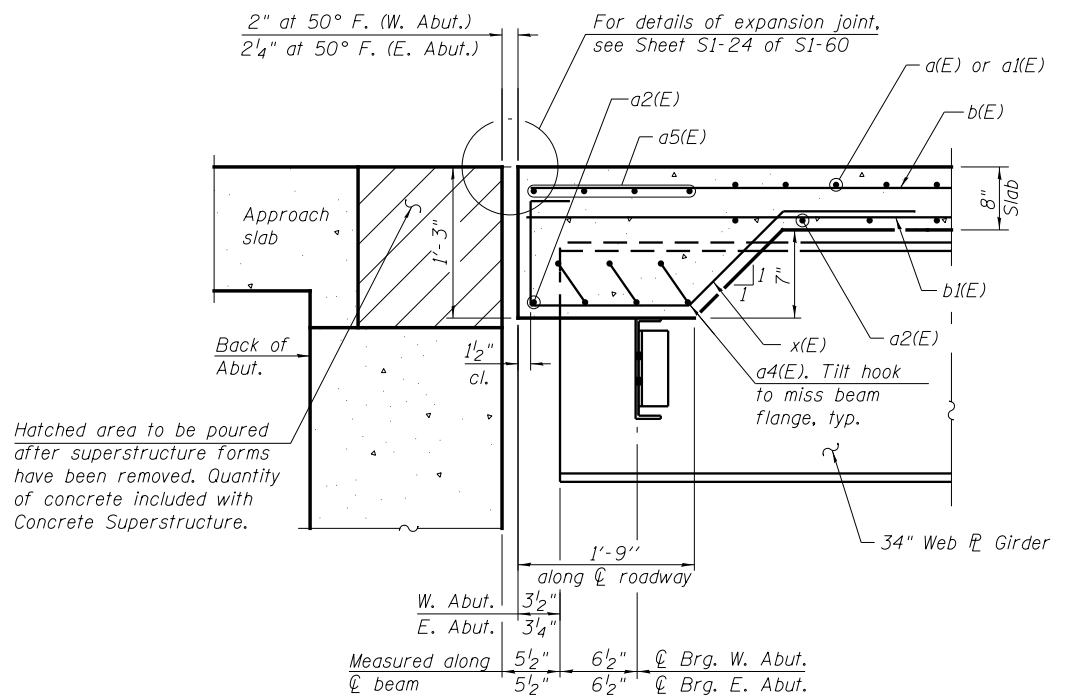
SHEET NO. S1-17 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 169 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



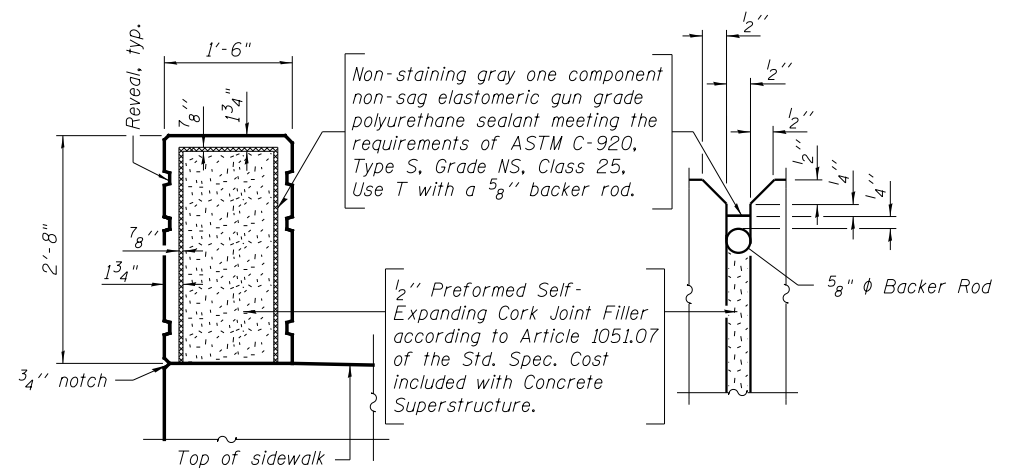
SECTION THRU SIDEWALK

(North sidewalk shown South sidewalk similar, opposite hand)

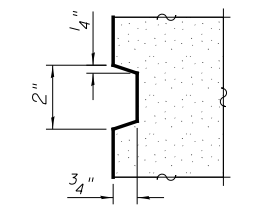


SECTION THROUGH EXPANSION JOINT AT RIGHT ANGLE

(Looking North, West joint shown, East joint similar)



PARAPET JOINT DETAILS



REVEAL DETAIL

0161700-60X95-S018-DET.dgn

**PARSONS
BRINCKERHOFF**

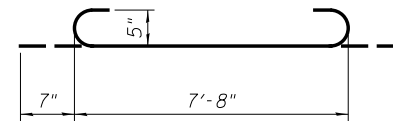
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|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MS | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

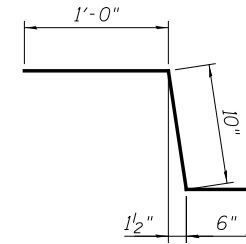
**SUPERSTRUCTURE DETAILS III
STRUCTURE NO. 016-1700**

SHEET NO. S1-18 OF S1-60 SHEETS

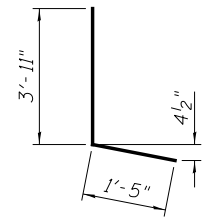
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 170 |
| CONTRACT NO. 60X95 | | | | |
| <small>ILLINOIS FED. AID PROJECT</small> | | | | |



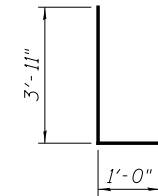
BAR a4(E)



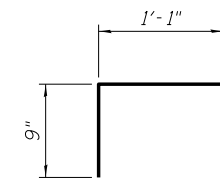
BAR c1(E)



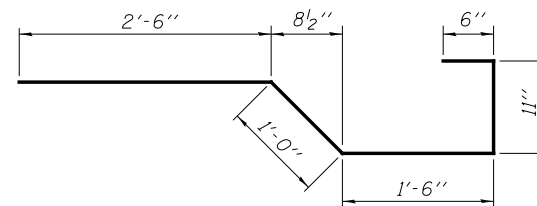
BAR d(E)



BAR d1(E)



BAR d2(E)



BAR x(E)

**SUPERSTRUCTURE
BILL OF MATERIAL**

| Bar | No. | Size | Length | Shape |
|----------------------------------|------|------|---------|---------|
| a(E) | 678 | #5 | 32'-0" | — |
| a1(E) | 678 | #5 | 40'-0" | — |
| a2(E) | 946 | #5 | 36'-0" | — |
| a3(E) | 1356 | #6 | 13'-6" | — |
| a4(E) | 48 | #5 | 8'-10" | U |
| a5(E) | 16 | #6 | 36'-5" | — |
| a6(E) | 96 | #5 | 2'-0" | — |
| b(E) | 1152 | #5 | 29'-5" | — |
| b1(E) | 704 | #5 | 31'-9" | — |
| b2(E) | 69 | #6 | 48'-5" | — |
| b3(E) | 69 | #6 | 56'-2" | — |
| b4(E) | 69 | #6 | 57'-11" | — |
| c(E) | 630 | #5 | 12'-3" | — |
| c1(E) | 630 | #5 | 2'-4" | L |
| d(E) | 688 | #4 | 5'-4" | L |
| d1(E) | 688 | #6 | 4'-11" | L |
| d2(E) | 146 | #4 | 2'-7" | U |
| e(E) | 96 | #4 | 19'-8" | — |
| e1(E) | 16 | #4 | 13'-7" | — |
| e2(E) | 32 | #4 | 10'-7" | — |
| e3(E) | 8 | #4 | 16'-5" | — |
| e4(E) | 32 | #4 | 14'-0" | — |
| e5(E) | 16 | #4 | 13'-1" | — |
| e6(E) | 48 | #4 | 17'-8" | — |
| e7(E) | 32 | #4 | 14'-5" | — |
| e8(E) | 8 | #4 | 15'-4" | — |
| e9(E) | 8 | #4 | 18'-2" | — |
| e10(E) | 8 | #4 | 13'-6" | — |
| x(E) | 140 | #5 | 6'-5" | U |
| Reinforcement Bars, Epoxy Coated | | | Pound | 212,540 |
| Concrete Superstructure | | | Cu. Yd. | 909 |
| Rubbed Finish | | | Sq. Ft. | 2,638 |
| Protective Coat | | | Sq. Yd. | 2,675 |
| Bridge Deck Grooving | | | Sq. Yd. | 1,484 |

Notes:
 Bars indicated thus 1 x 2-#8 etc. indicates 1 line of bars with 2 lengths per line.
 See sheet S1-17 of S1-60 for parapet details.
 See sheet S1-16 of S1-60 for deck pouring sequence.

0161700-60X95-S019-DET.dgn

**PARSONS
BRINCKERHOFF**

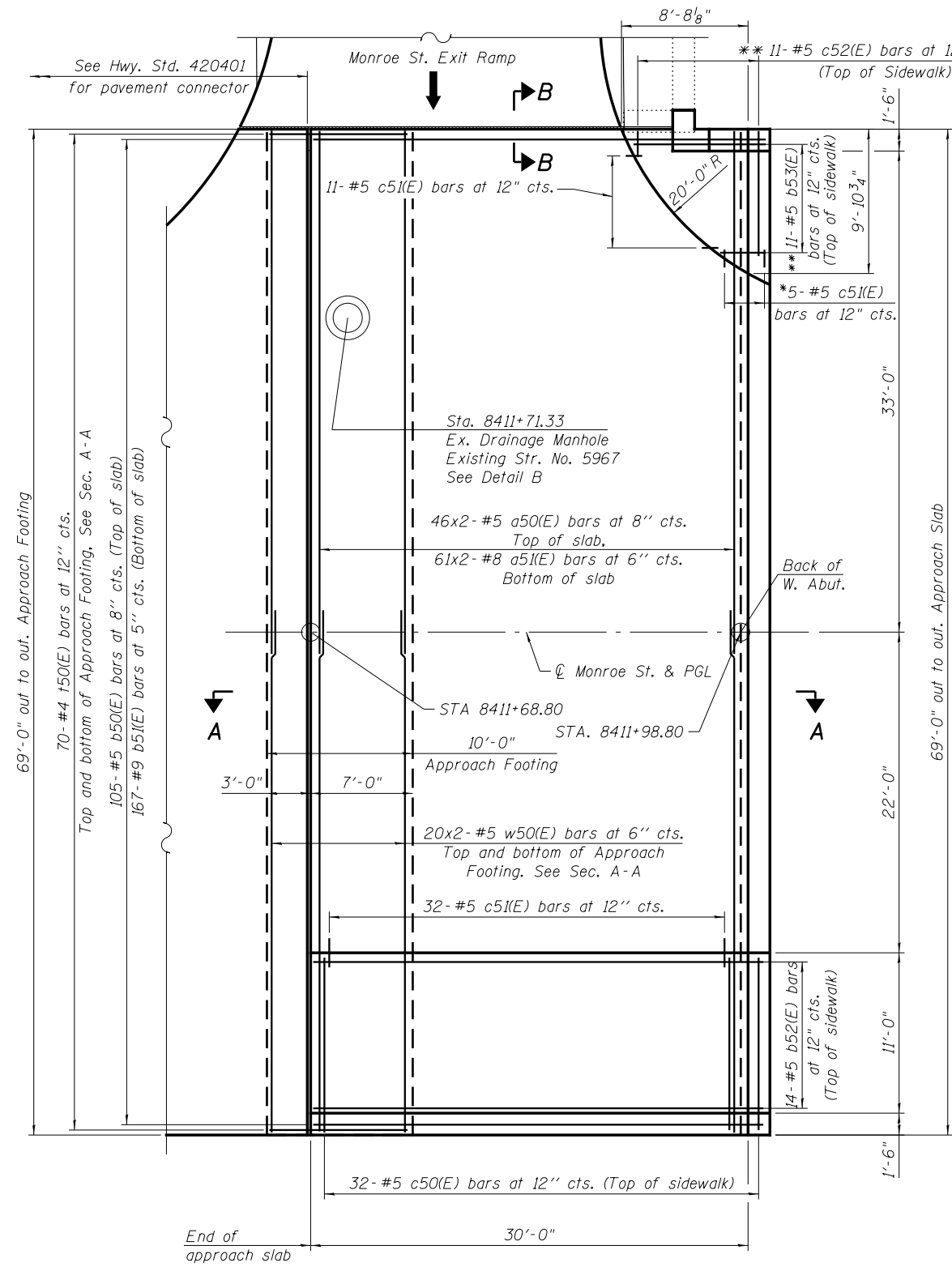
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|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MS | REVISED - |
| | CHECKED - TWP | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUPERSTRUCTURE DETAILS IV
STRUCTURE NO. 016-1700**

SHEET NO. S1-19 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-------------|--------|---------------------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 171 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |

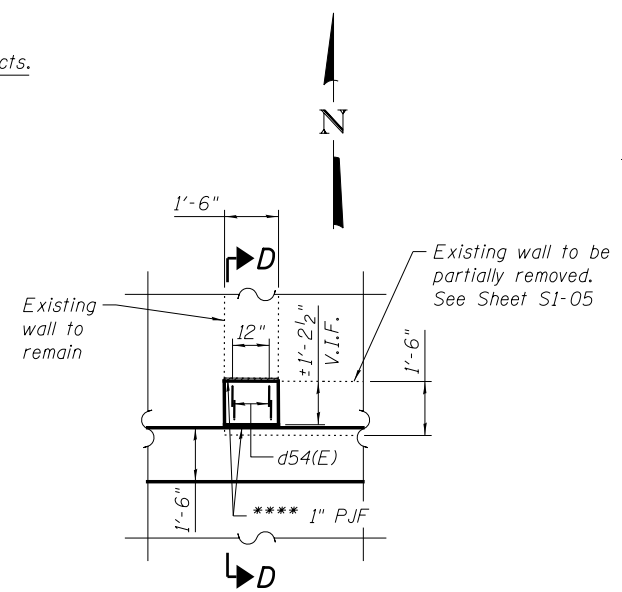


WEST APPROACH PLAN

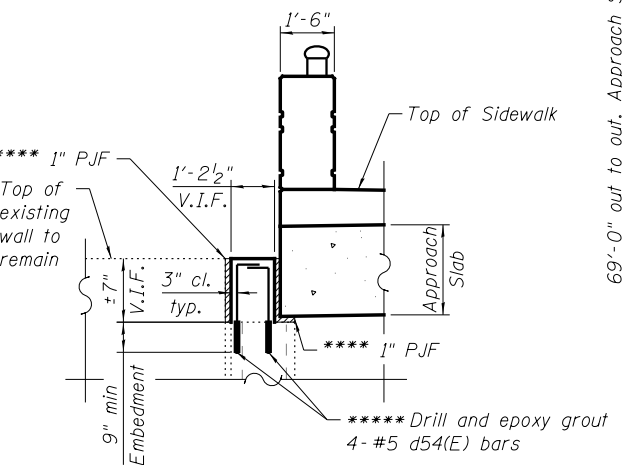
MINIMUM BAR LAP

- #4 bar = 2'-7"
- #5 bar = 3'-3"
- #8 bar = 4'-9"

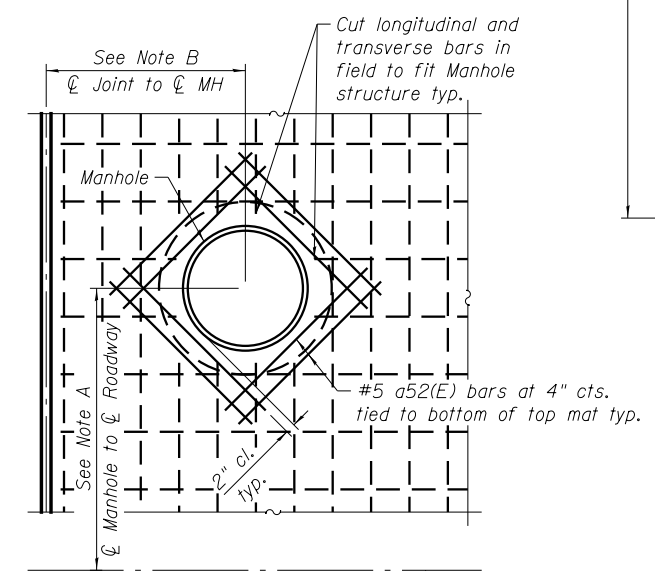
- * c51(E) bars may be omitted where ADA ramp slope prohibits bar placement
- ** Order bars at full length and cut in field to fit
- *** See Roadway Plans
- **** Cost included with Concrete Superstructure (Approach Slab).
- ***** Drill and grout bars according to article 584 of the Std. Specs. with a minimum embedment of 9", place to miss existing reinforcement. Cost included in the cost of Reinforcement Bars, Epoxy Coated.



DETAIL C

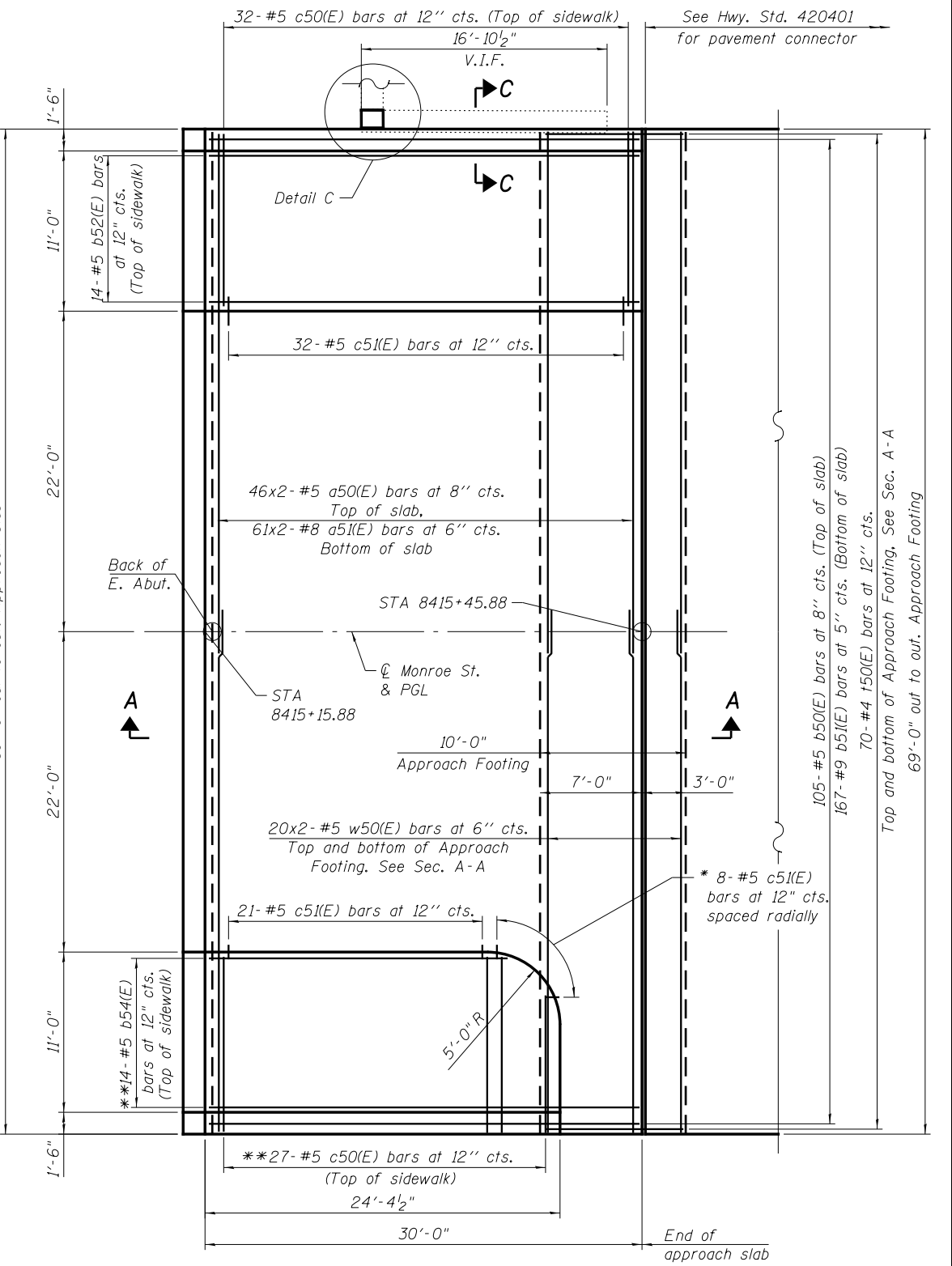


SECTION D-D



DETAIL B

(Applies to Approach Slab and Approach Footing)
Cost of cutting bars included in the cost of Reinforcement Bars, Epoxy Coated



EAST APPROACH PLAN

NOTE A:

For Manhole Existing Structure No. 5967 = 21'-7"

NOTE B:

For Manhole Existing Structure No. 5967 = 2'-8"

Notes:
See sheet S1-21 of S1-60 for Sections A-A and B-B.
See sheet S1-22 of S1-60 for sidewalk Ramp Plans
Bars in the sidewalk interfering with the ADA Ramp Construction shall be bent and/or cut to fit based on the sidewalk depth in these areas.
For ADA ramp locations, layout, elevations, details and top of sidewalk elevations, see Roadway plans.

0161700-60X95-5020-APP.dgn

PARSONS BRINCKERHOFF

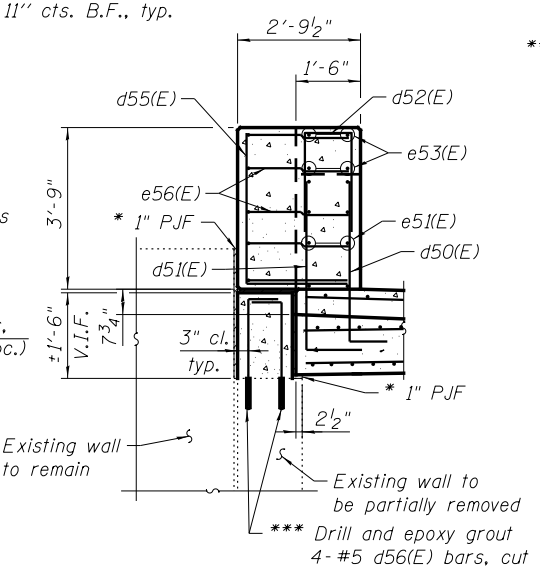
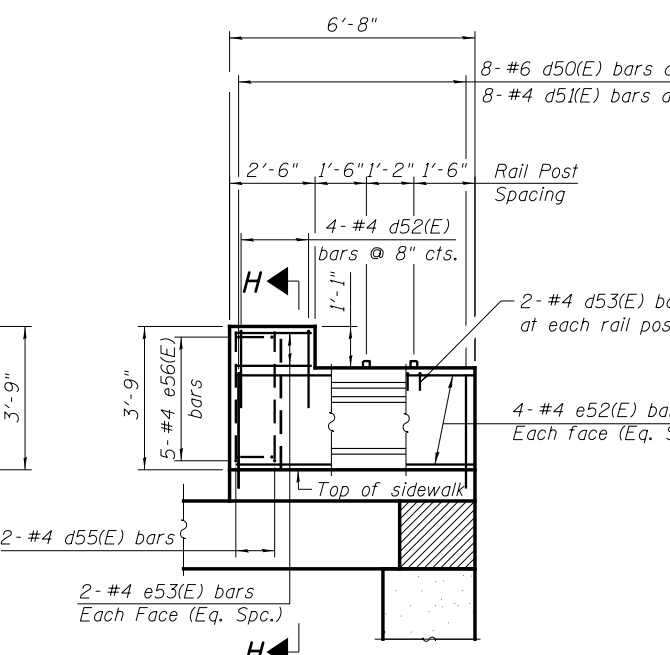
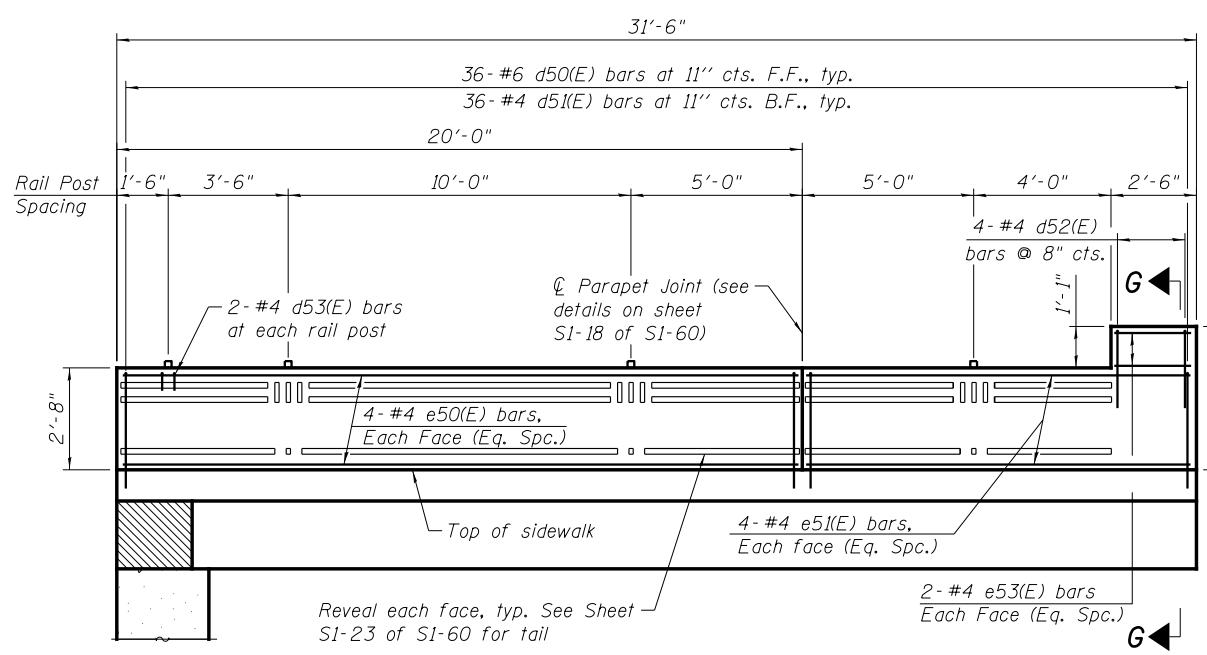
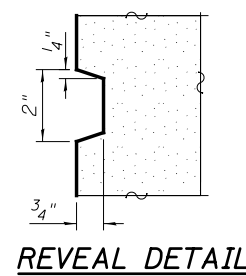
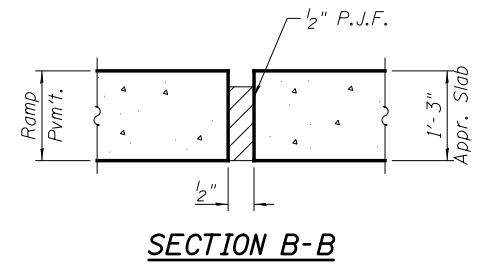
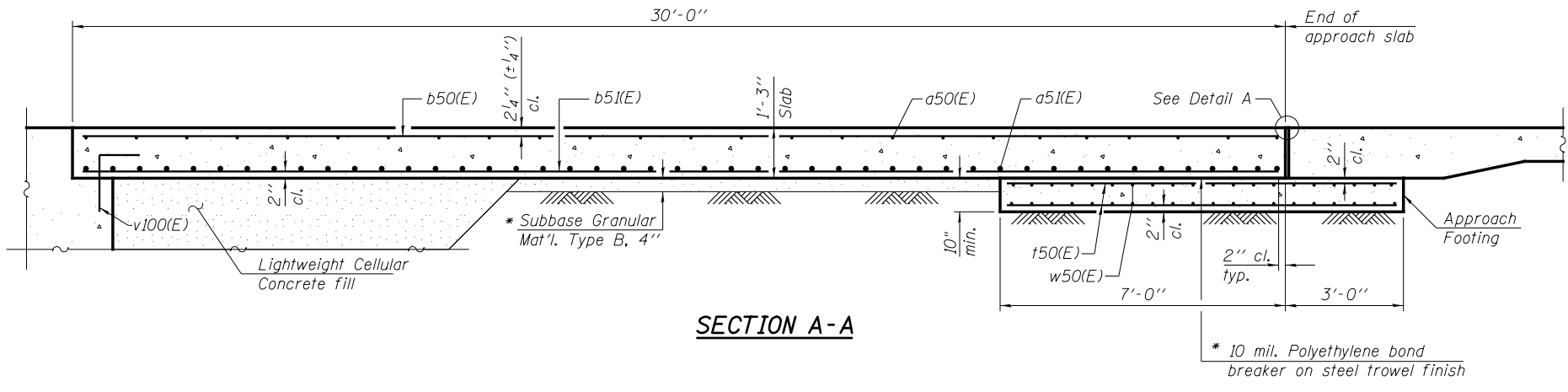
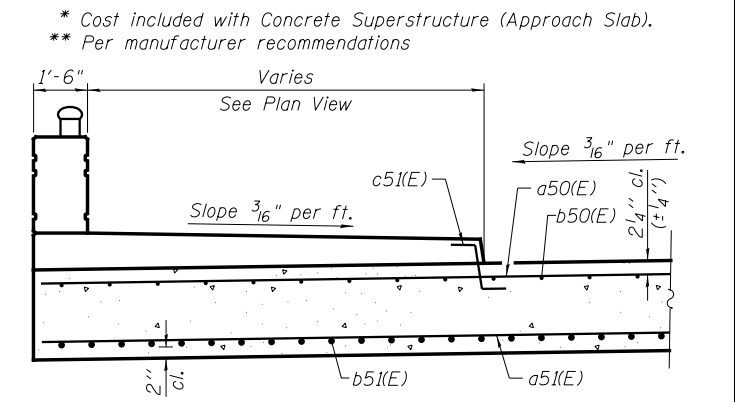
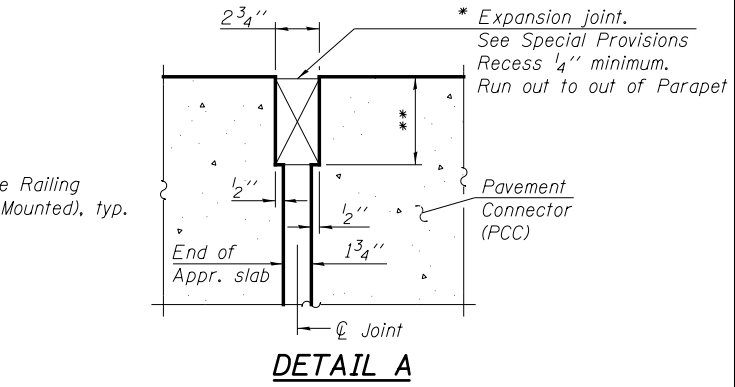
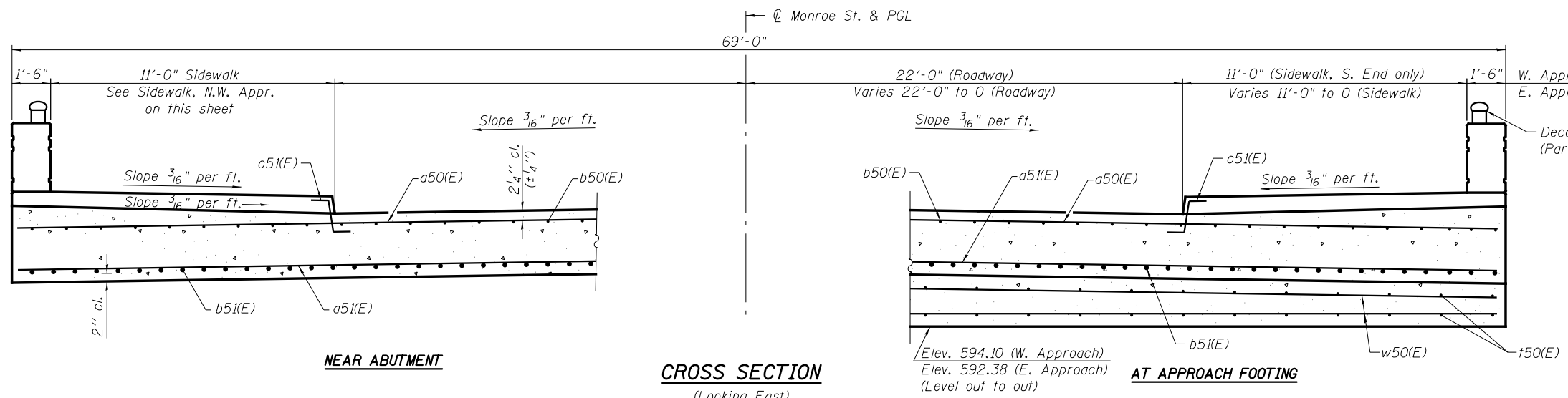
| | | |
|-----------------------|----------------|-----------|
| USER NAME = pateld | DESIGNED - LFB | REVISED - |
| | CHECKED - HA | REVISED - |
| PLOT SCALE = N.T.S. | DRAWN - DCP | REVISED - |
| PLOT DATE = 6/29/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE APPROACH SLAB DETAILS I
STRUCTURE NO. 016-1700**

SHEET NO. S1-20 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 172 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



*** Drill and grout bars according to article 584 of the Std. Specs. with a minimum embedment of 9", place to miss existing reinforcement. Cost included in the cost of Reinforcement Bars, Epoxy Coated.

Notes:
 Approach slab shall be paid for as Concrete Superstructure (Approach Slab).
 Approach footing concrete shall be paid for as Concrete Structures.
 The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
 Cost of excavation for approach footing included with Concrete Structures.
 For Lightweight Cellular Concrete Fill, v100(E) bar and drainage treatment details, see sheet S1-33 thru S1-35 & S1-38 thru S1-40 of S1-60.
 For typical parapet reinforcing section and Section G-G, see sheet S1-22 of S1-60.
 Parapet concrete shall be paid for as Concrete Superstructure.
 The cost of reveal is included in cost of Concrete Superstructure.
 All edges should be chamfered 3/4".

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PARSONS BRINCKERHOFF

| | | |
|-----------------------|----------------|-----------|
| USER NAME = pateld | DESIGNED - LFB | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

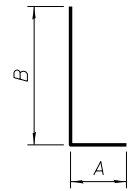
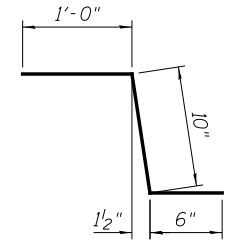
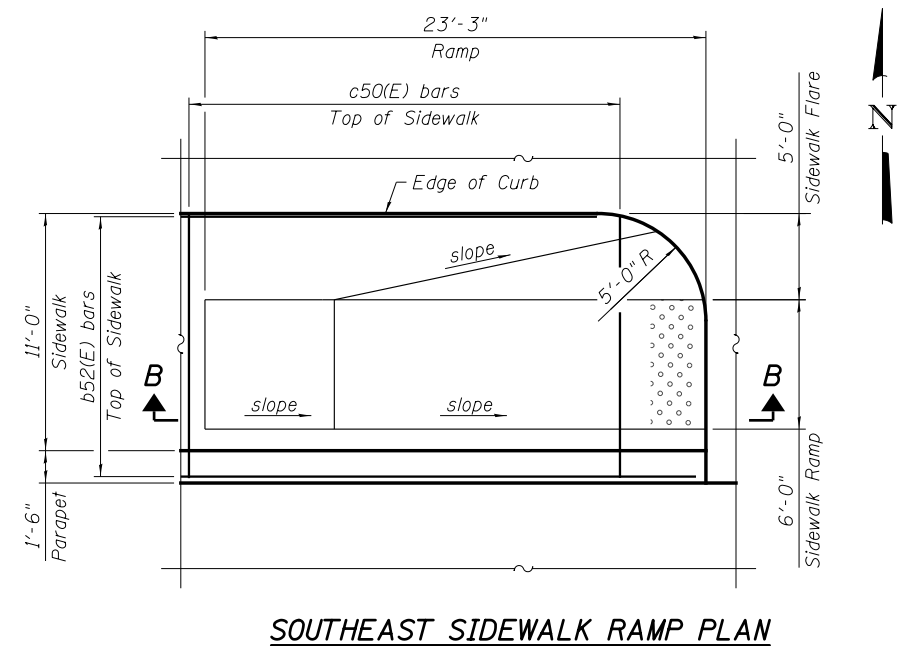
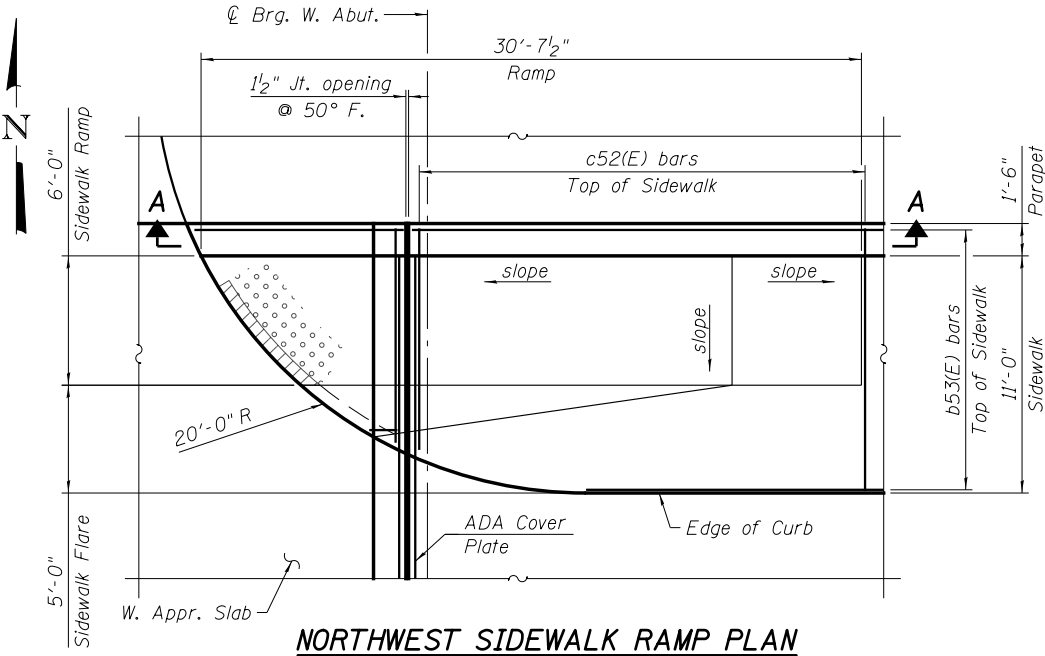
BRIDGE APPROACH SLAB DETAILS II
STRUCTURE NO. 016-1700

SHEET NO. S1-21 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 173 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

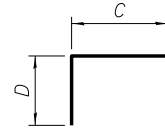
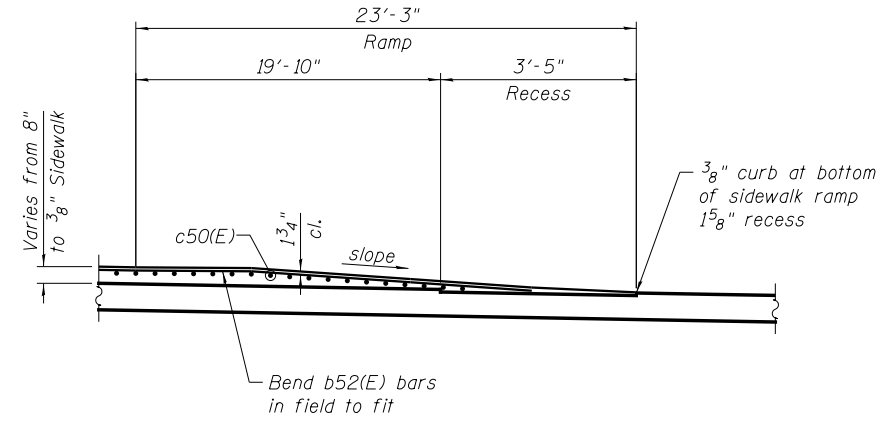
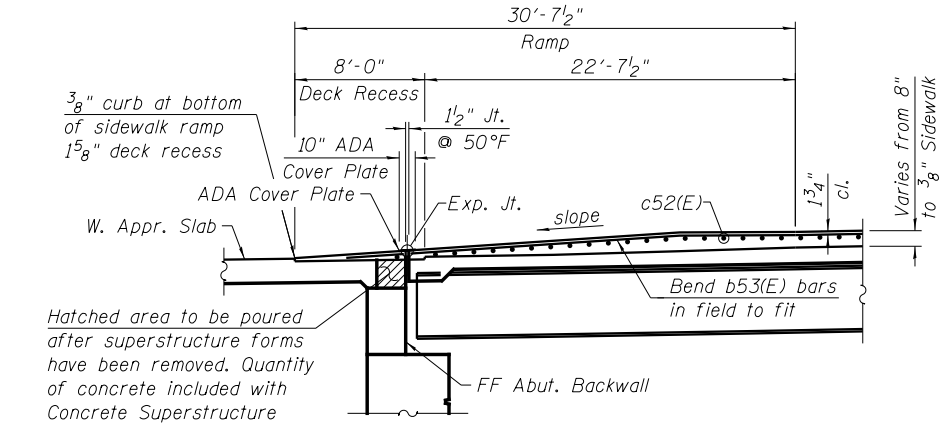
**TWO APPROACHES
BILL OF MATERIAL**

| Bar | No. | Size | Length | Shape |
|---|-----|---------|--------|-------|
| a50(E) | 184 | #5 | 36'-0" | ▬ |
| a51(E) | 244 | #8 | 36'-9" | ▬ |
| a52(E) | 8 | #5 | 5'-4" | ▬ |
| b50(E) | 210 | #5 | 29'-8" | ▬ |
| b51(E) | 334 | #9 | 29'-8" | ▬ |
| b52(E) | 28 | #5 | 31'-2" | ▬ |
| b53(E) | 11 | #5 | 9'-6" | ▬ |
| b54(E) | 14 | #5 | 25'-4" | ▬ |
| c50(E) | 91 | #5 | 12'-2" | ▬ |
| c51(E) | 109 | #5 | 2'-4" | ▬ |
| c52(E) | 11 | #5 | 10'-4" | ▬ |
| d50(E) | 109 | #6 | 5'-3" | ▬ |
| d51(E) | 109 | #4 | 5'-10" | ▬ |
| d52(E) | 16 | #4 | 4'-9" | ▬ |
| d53(E) | 26 | #4 | 2'-7" | ▬ |
| d54(E) | 4 | #5 | 2'-0" | ▬ |
| d55(E) | 2 | #4 | 5'-11" | ▬ |
| d56(E) | 4 | #5 | 3'-0" | ▬ |
| e50(E) | 16 | #4 | 19'-8" | ▬ |
| e51(E) | 16 | #4 | 11'-2" | ▬ |
| e52(E) | 8 | #4 | 6'-4" | ▬ |
| e53(E) | 16 | #4 | 2'-2" | ▬ |
| e54(E) | 8 | #4 | 5'-2" | ▬ |
| e55(E) | 8 | #4 | 20'-1" | ▬ |
| e56(E) | 5 | #4 | 6'-2" | ▬ |
| l50(E) | 280 | #4 | 9'-8" | ▬ |
| w50(E) | 160 | #5 | 36'-0" | ▬ |
| Concrete Superstructure (Approach Slab) | | Cu. Yd. | 197 | |
| Concrete Superstructures | | Cu. Yd. | 47 | |
| Concrete Structures | | Cu. Yd. | 52 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 83,800 | |
| Protective Coat | | Sq. Yd. | 498 | |
| Bridge Deck Grooving | | Sq. Yd. | 330 | |
| Rubbed Finish | | Sq. Ft. | 422 | |



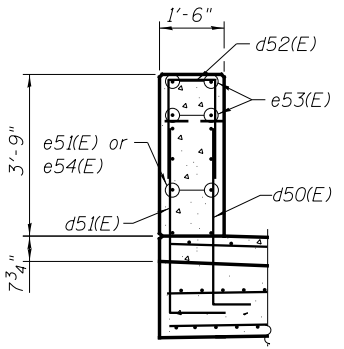
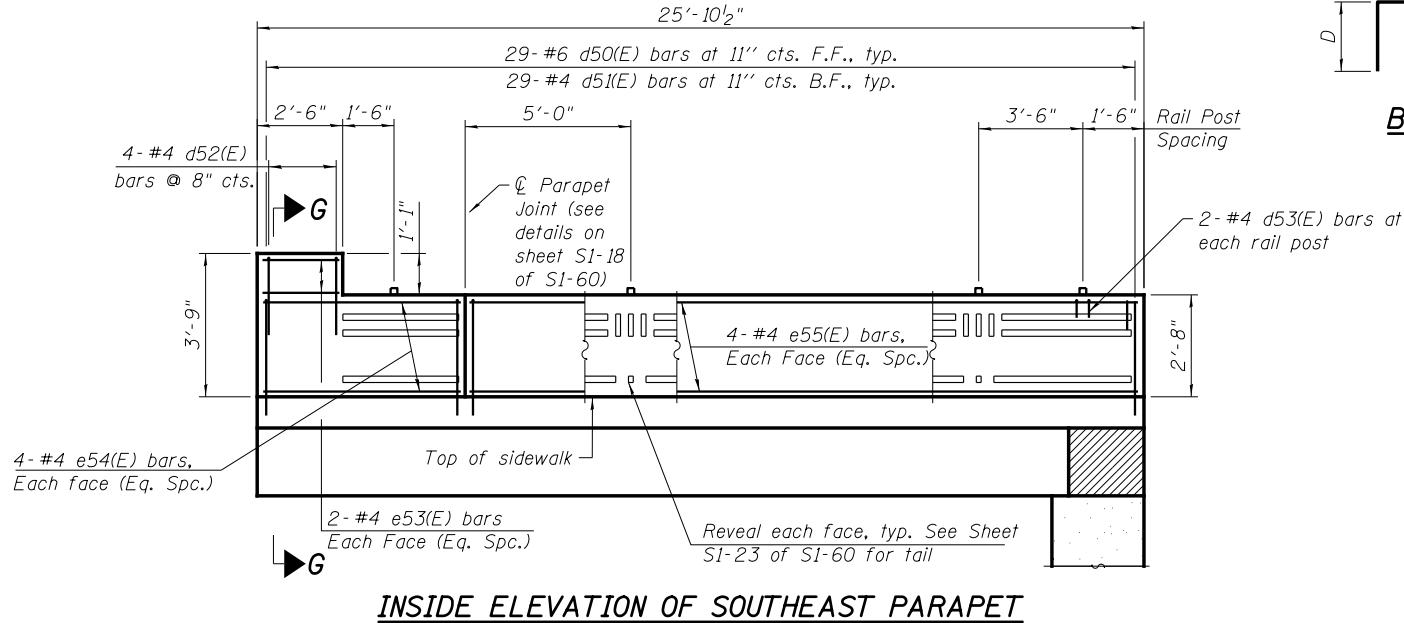
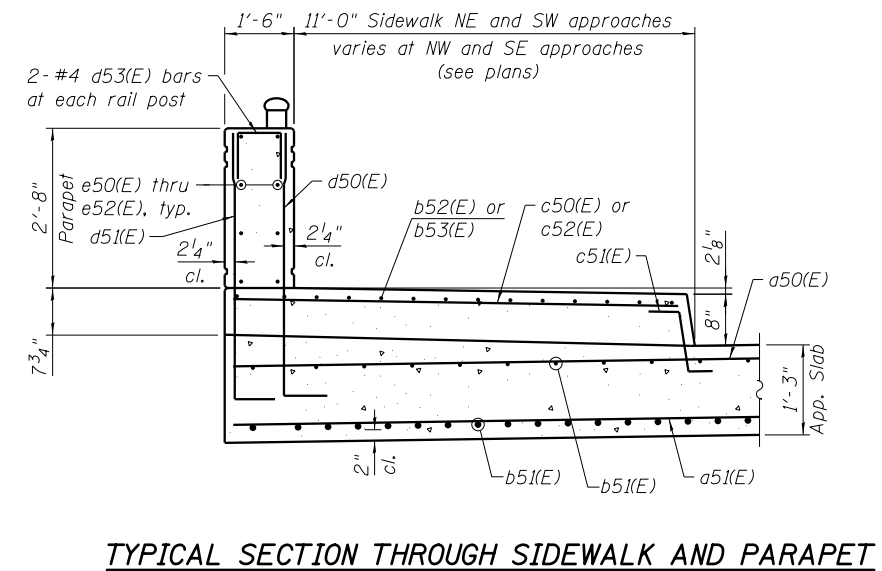
A & B DIMENSIONS

| Bar | A | B |
|--------|-------|-------|
| d50(E) | 11" | 4'-4" |
| d51(E) | 1'-4" | 4'-6" |
| d54(E) | 8" | 1'-4" |
| d55(E) | 2'-6" | 3'-5" |
| d56(E) | 9" | 2'-3" |



C & D DIMENSIONS

| Bar | C | D |
|--------|-------|--------|
| d52(E) | 1'-1" | 1'-10" |
| d53(E) | 1'-1" | 9" |
| e56(E) | 1'-2" | 2'-6" |



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**PARSONS
BRINCKERHOFF**

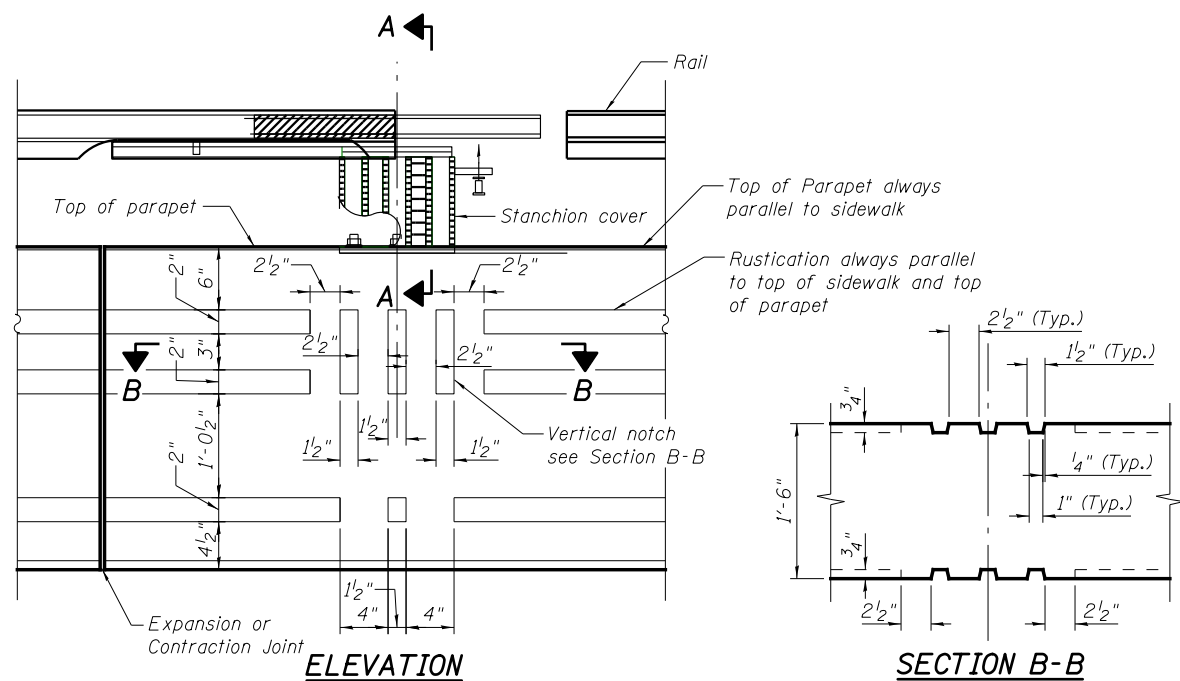
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|-----------------------|----------------|-----------|
| USER NAME = pateld | DESIGNED - LFB | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - HA | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE APPROACH SLAB DETAILS III
STRUCTURE NO. 016-1700**

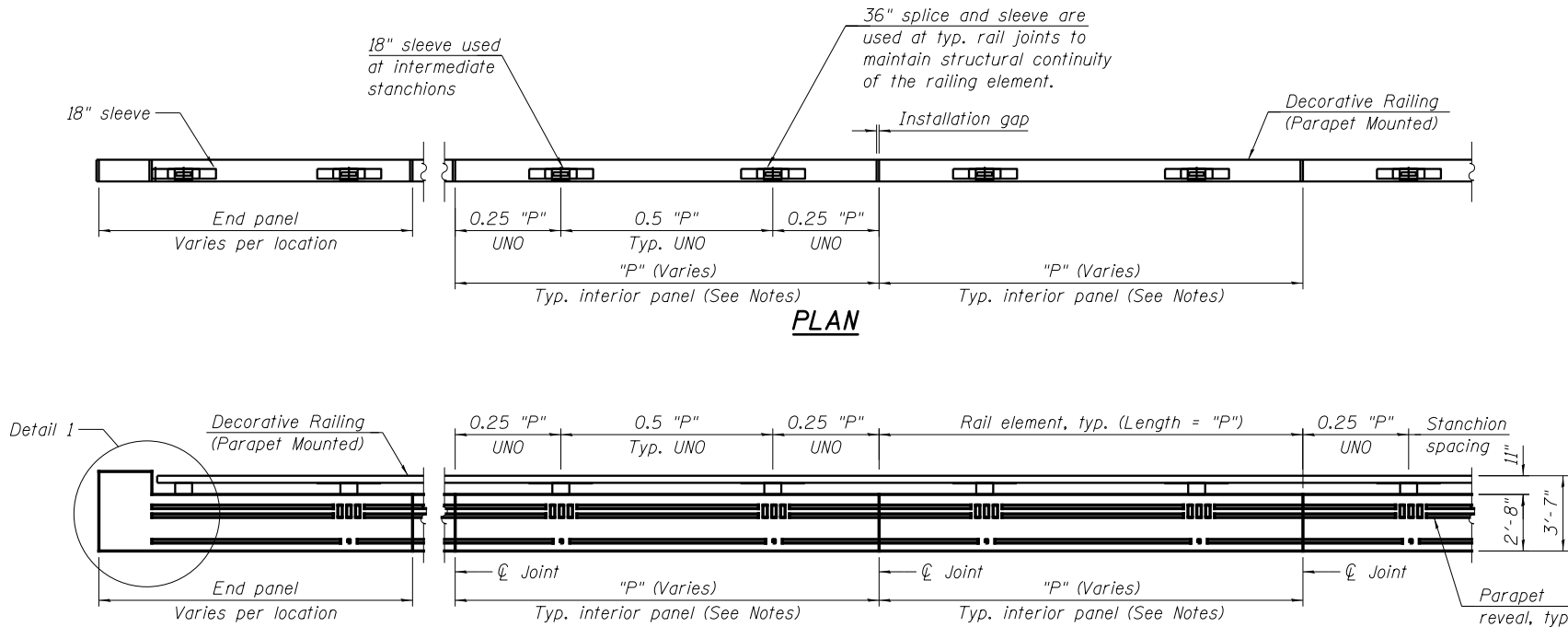
SHEET NO. S1-22 OF S1-60 SHEETS

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 174 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |



ELEVATION

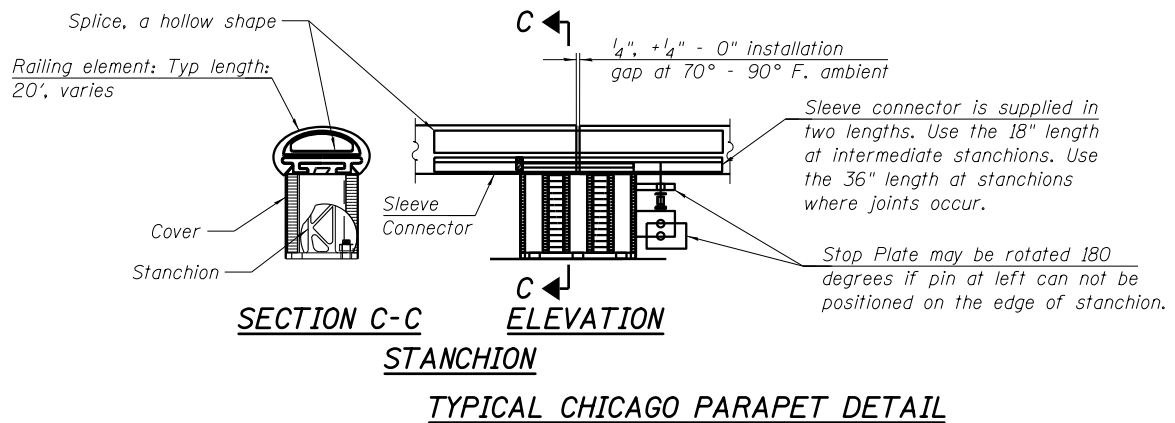
SECTION B-B



PLAN

ELEVATION

RAIL DETAIL - STANCHION LOCATION AND SPACING

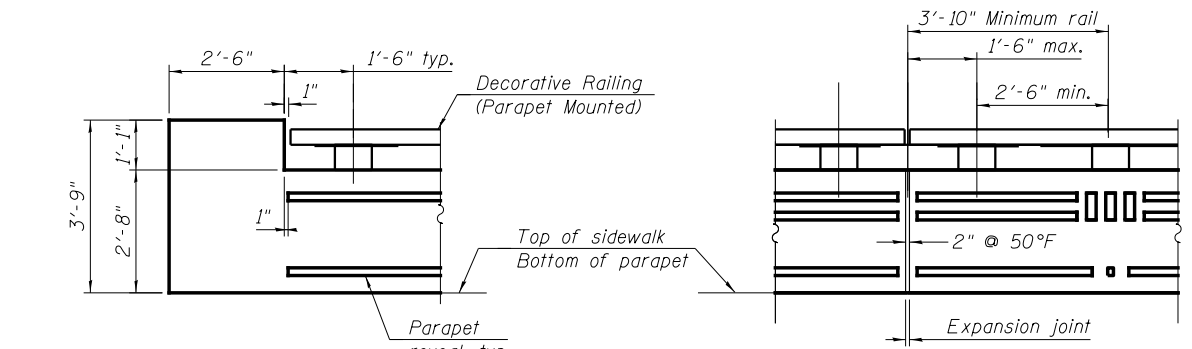


SECTION C-C

ELEVATION

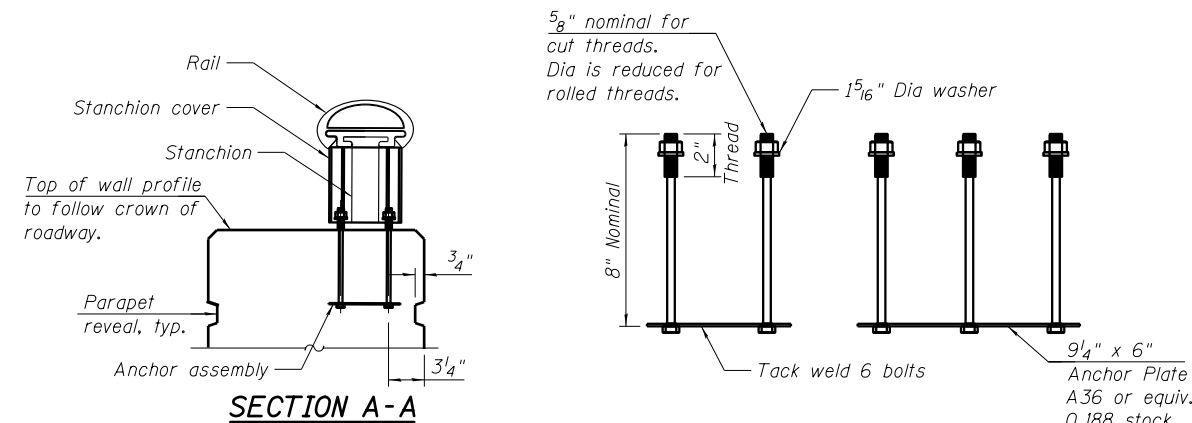
STANCHION

TYPICAL CHICAGO PARAPET DETAIL



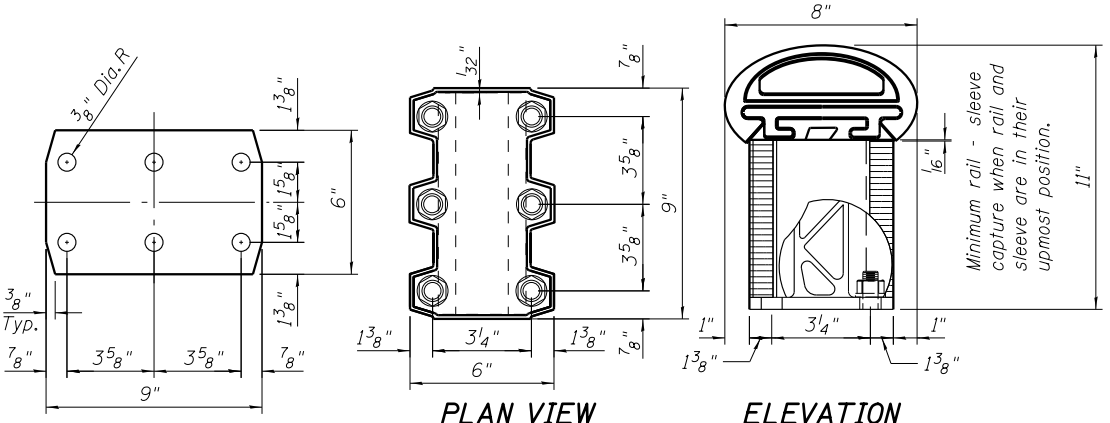
RAIL DETAIL 1 - AT END OF PARAPET

RAIL DETAIL 2 - AT EXPANSION JOINT



SECTION A-A

ANCHOR ASSEMBLY



ELASTOMERIC PAD

PLAN VIEW

ELEVATION

STANCHION COVER

BILL OF MATERIAL

| Item | Unit | Total |
|--------------------------------------|------|-------|
| Decorative Railing (Parapet Mounted) | Foot | 724 |

- NOTES:**
- All fasteners to meet, or exceed, ASTM A307 Grade C strength requirements.
 - Galvanize per Article 509.05 of the Standard Specifications after fabrication.
 - The size and position of parapet reinforcing must be consistent with capture of the anchor assembly. See Sheet S1-17 of S1-60 for rebar details.

- NOTES:**
- Elastomeric pad for stanchion made from 1/16" thick stock.
 - One required per stanchion.

- NOTES:**
- Cover is shown superimposed over stanchion with anchors in place.
 - The stanchion cover is a non-structural element, serving an aesthetic function. It rests on the flange of the stanchion, without fasteners and is captured in place by the rail and stanchion.

Notes:

When walls without rail are adjacent to Chicago wall with rail, their traffic face, or the terminus of their traffic face, must be in the same plane as Chicago wall with rail.

Rustication may vary at terminal ends and is subject to site conditions and site approval. In all other situations, the middle 2.5" x 1.5" rustication is aligned with the center of the stanchion.

Wall details above show that portion of the wall above the gutter break, the substructure is not shown. Note that substructure(s) depth variations could significantly alter the required top of wall profile.

Field cutting of rail elements is acceptable. The cut edge will no longer be anodized. Saw cut only, flame cut not allowed.

End caps shall be used at all rail terminals.

Railing system to be produced using extruded aluminum that can be clear anodized.

Alloy selection is based upon the above color requirement and the engineering sufficiency analysis which must be supplied by the Contractor.

Physical appearance to be equal to "Valentine Urban Systems - Chicago wall with rail".

For panel layout, overall stanchion location, spacing and details, see Sheet S1-17 of S1-60.

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PARSONS BRINCKERHOFF

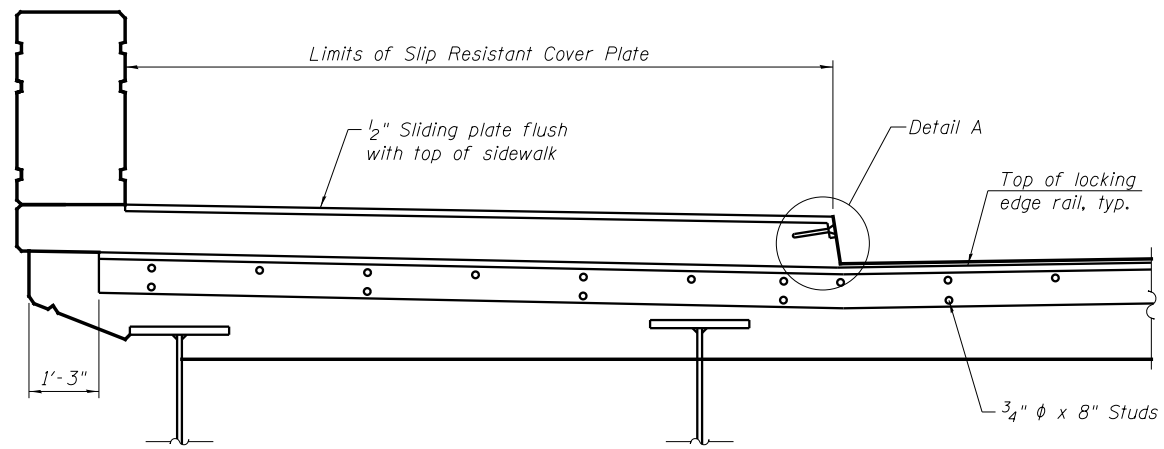
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|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MS | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DECORATIVE RAILING PARAPET MOUNTED
STRUCTURE NO. 016-1700**

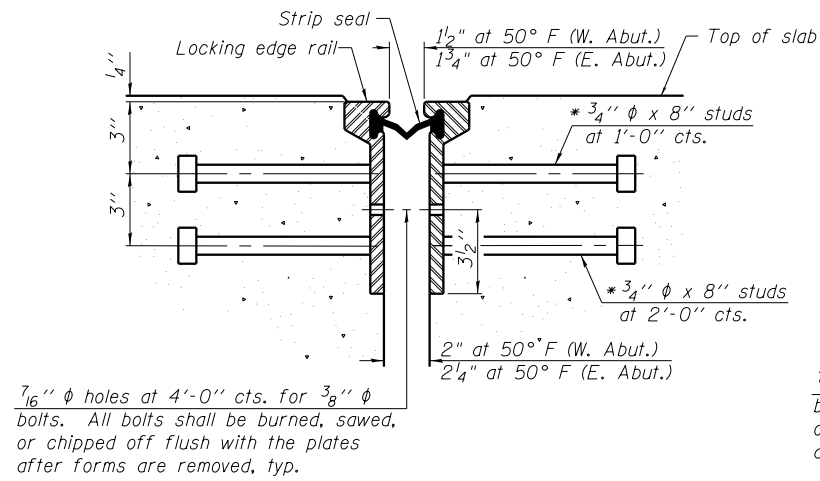
SHEET NO. S1-23 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 175 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



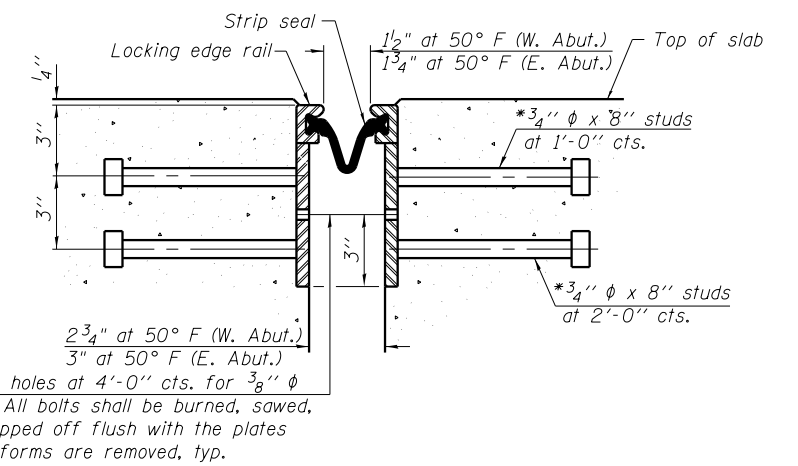
TYPICAL END TREATMENT AT SIDEWALK

Shorter plates with a single row of studs at 12" cts. may be necessary on sidewalks which are shallower than 9". See manufacturer's recommendation.

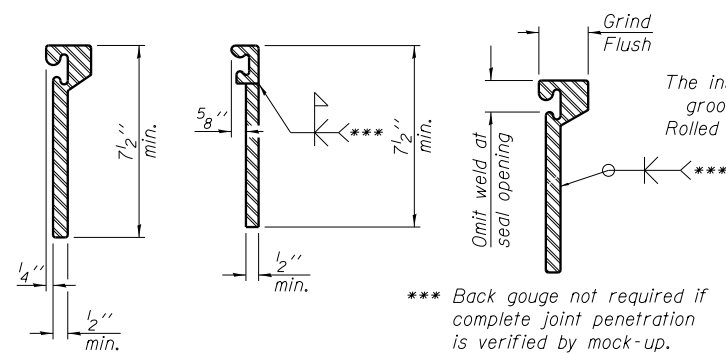


SECTION THRU ROLLED RAIL JOINT AT ROADWAY

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded. Cost shall be included with Preformed Joint Seal.



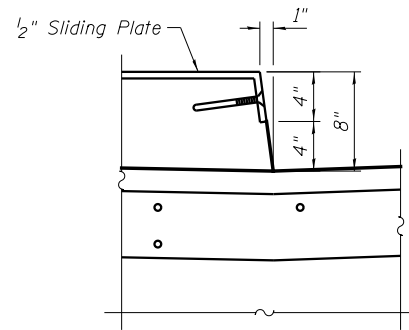
SECTION THRU WELDED RAIL JOINT AT ROADWAY



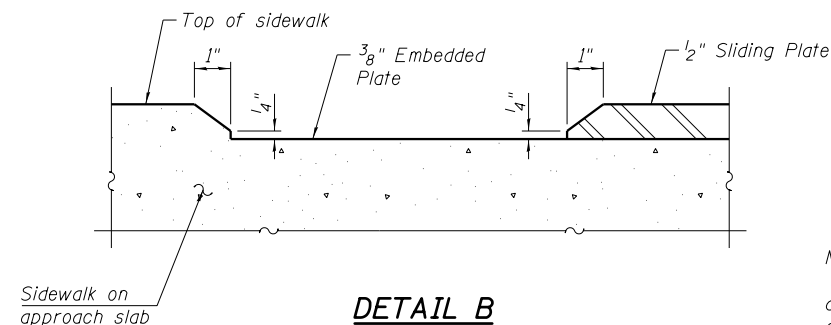
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

*** Back gouge not required if complete joint penetration is verified by mock-up.

ROLLED EXTRUDED RAIL WELDED RAIL LOCKING EDGE RAILS

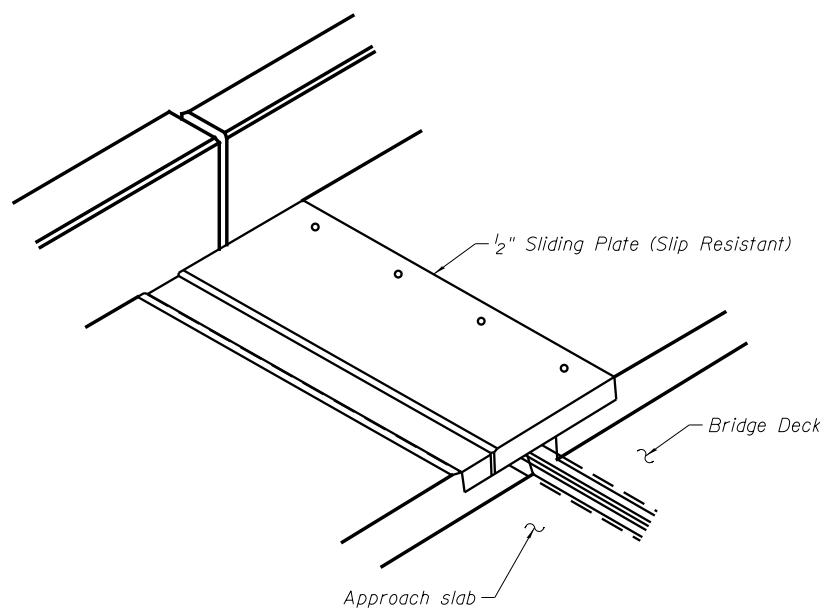


DETAIL A

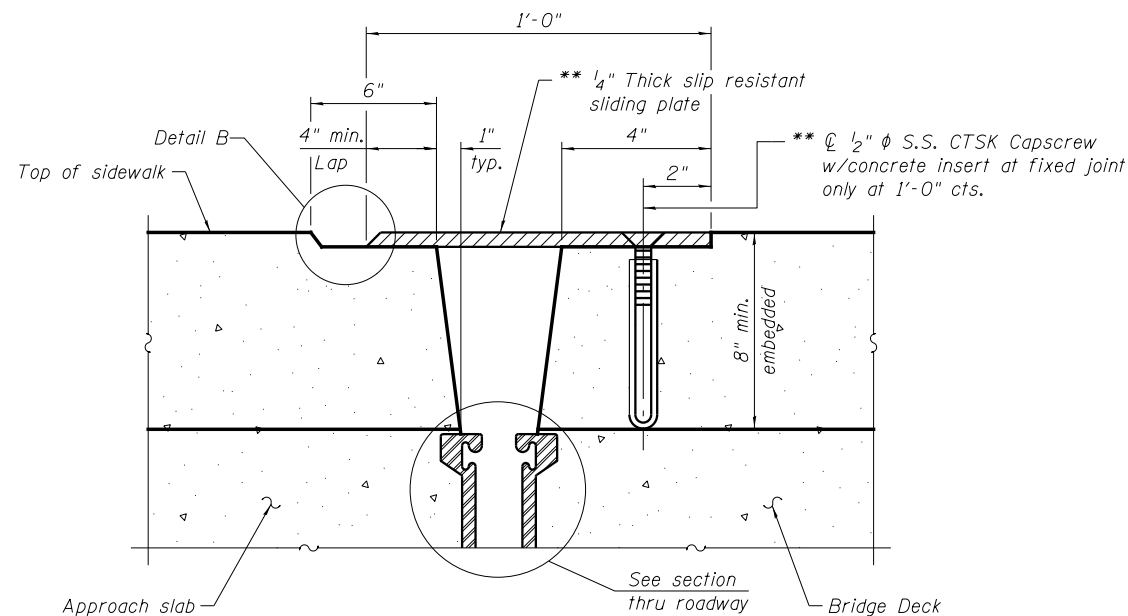


DETAIL B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.
 The manufacturer's recommended installation methods shall be followed.
 The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.
 All steel components (except Expansion Joint plates and attached bars, see General Notes) shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.
 All exposed top surfaces of the sliding plate on the deck shall have a raised pattern according to ASTM A786.



TRIMETRIC VIEW
(Showing Sliding Plate)



SECTION THRU SIDEWALK

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.
 ** Cost shall be included with Preformed Joint Seal.

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 138 |

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PARSONS BRINCKERHOFF

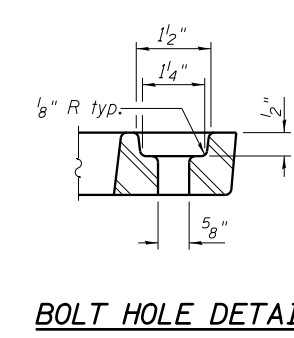
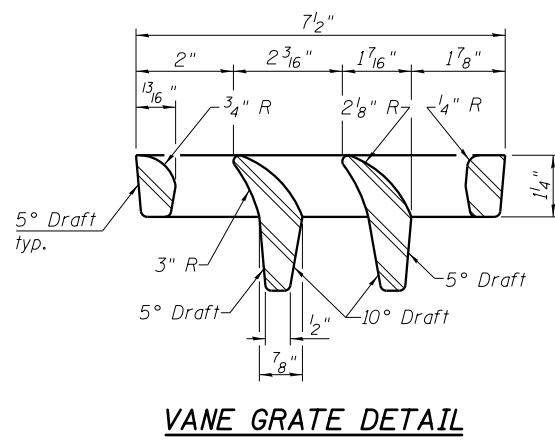
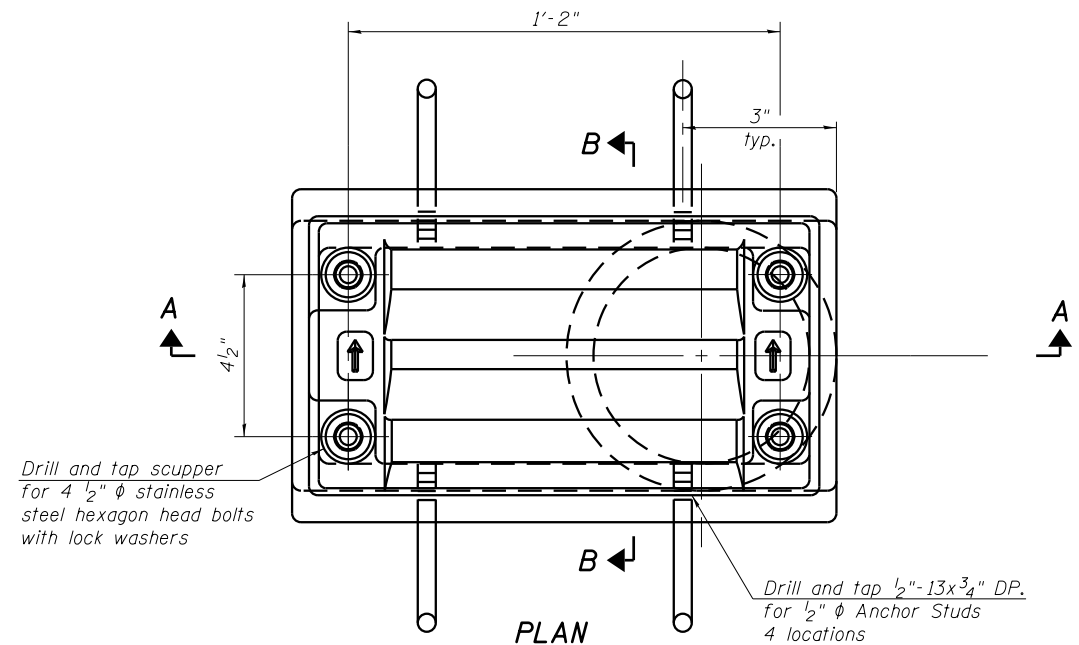
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|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT DETAILS STRUCTURE NO. 016-1700

SHEET NO. S1-24 OF S1-60 SHEETS

| | | | | |
|--------------------|---------------------|-------------|------------------|---------------------------|
| F.A.I. RTE. 90/94 | SECTION 2014-016R&B | COUNTY COOK | TOTAL SHEETS 378 | SHEET NO. 176 |
| CONTRACT NO. 60X95 | | | | ILLINOIS FED. AID PROJECT |



Notes:

All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 35B.

Bolts, anchor studs, washers and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M 232.

Downspouts located on the exterior side of a painted steel fascia beam shall be painted with the finish coat specified for the exterior side of the fascia beam.

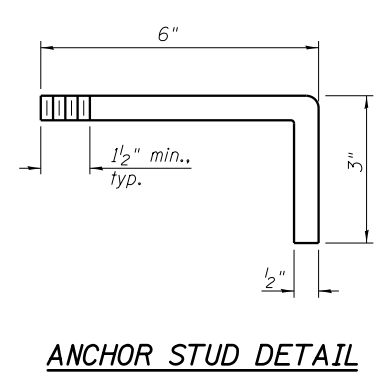
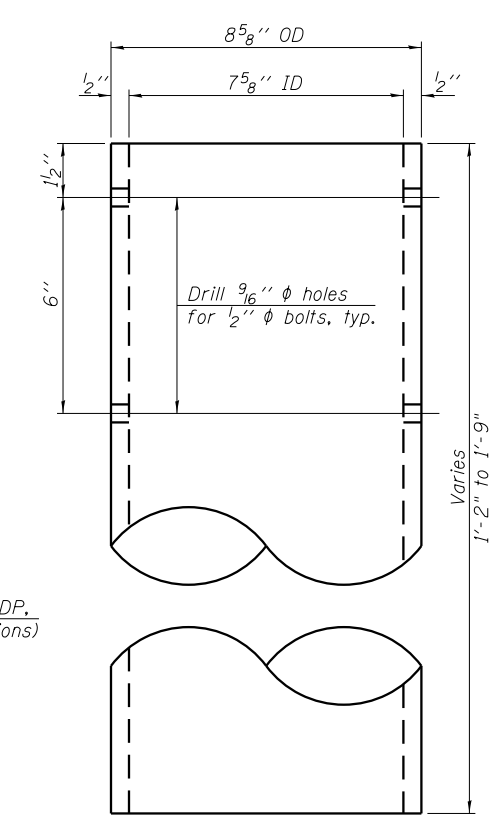
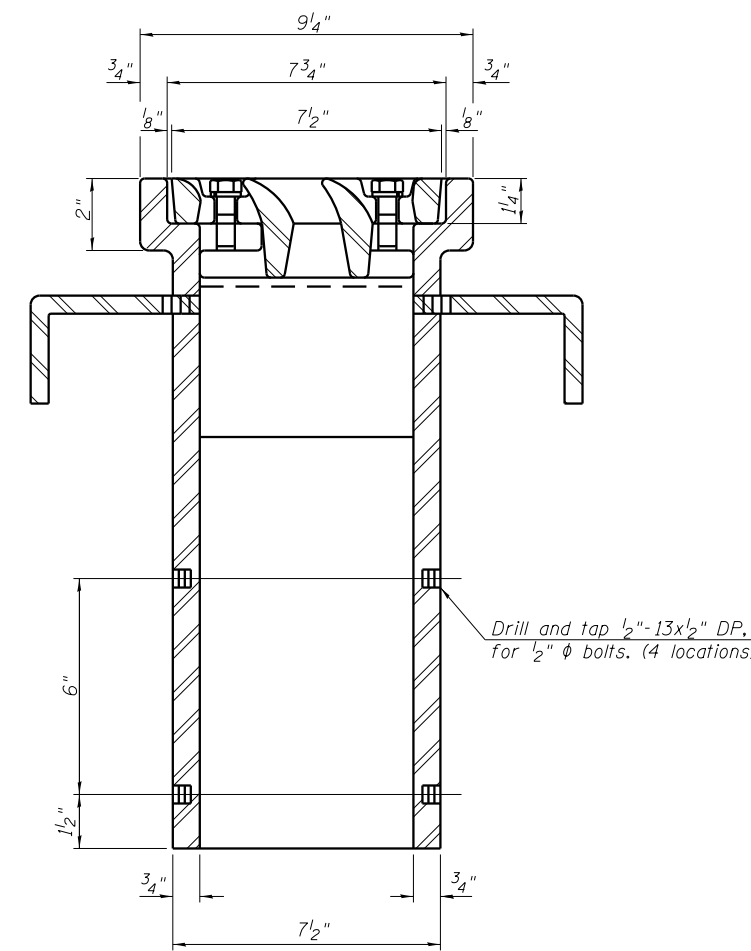
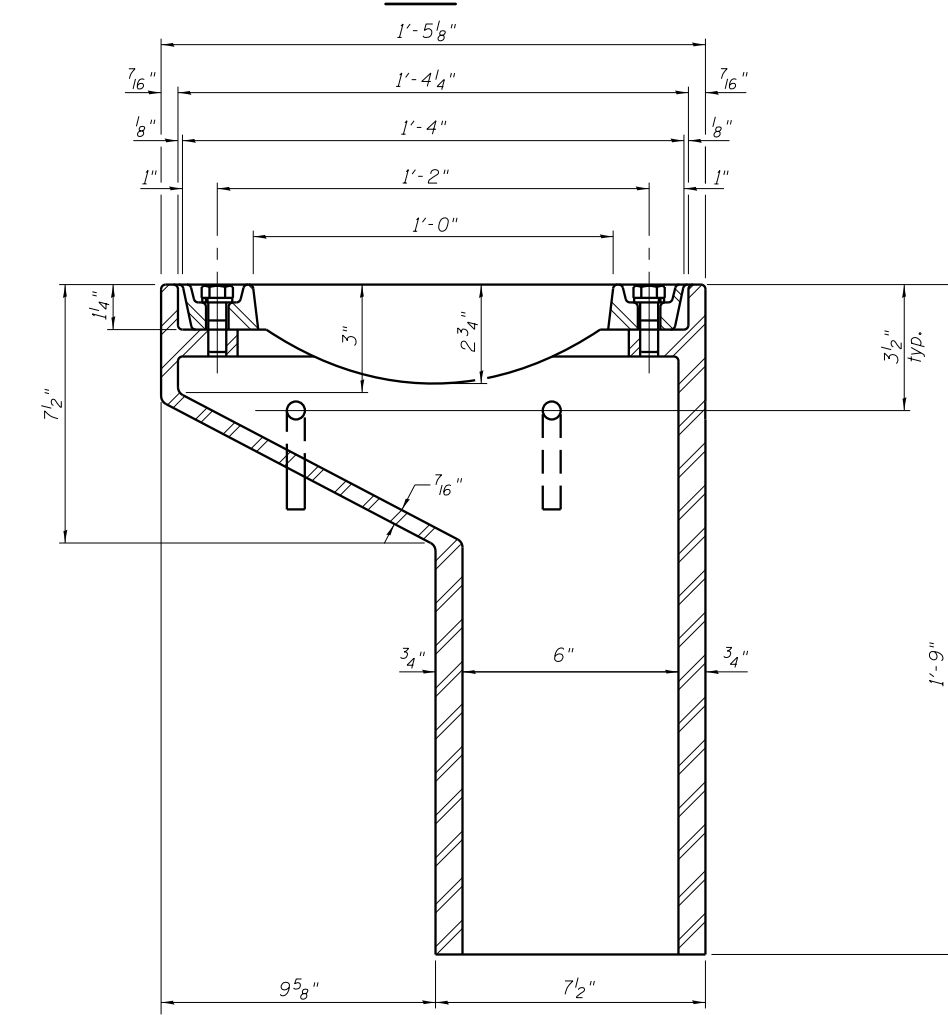
As an alternate, bolts, anchor studs, washers and nuts may be stainless steel according to Article 1006.29(d) of the Standard Specifications.

Structural steel weldments of equal sections and of the same configuration may be substituted for the cast iron scupper frame. Fillet or full penetration welds shall be used for the weldments. Details shall be submitted to the Engineer for approval. Structural steel weldments shall not be substituted for the cast iron scupper grate. Structural steel frames and downspouts shall be galvanized according to AASHTO M111.

The Contractor shall take appropriate measures to assure that Protective Coat is not applied to the scupper.

Cost of the Grate, Frame, Downspout, Anchor Studs, Bolts, Washers and Nuts including complete installation of the scupper shall be paid for at the contract unit price each for Drainage Scupper, DS-11.

Alternate fiberglass downspout conforming to ASTM D 2996 with a short-time rupture strength hoop tensile stress of 30,000 psi min. may be used in lieu of the cast iron or steel equivalent.



See sheet of for scupper location relative to parapet.

BILL OF MATERIAL

| ITEM | UNIT | QUANTITY |
|-------------------------|------|----------|
| Drainage Scupper, DS-11 | Each | 16 |

DS-11

7-1-10

PARSONS BRINCKERHOFF

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - HA | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - AH | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
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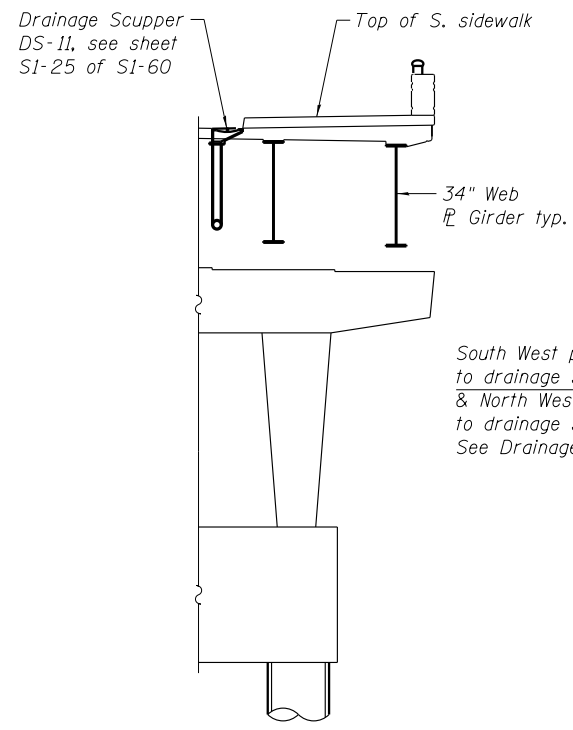
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE SCUPPER, DS-11
STRUCTURE NO. 016-1700

SHEET NO. S1-25 OF S1-60 SHEETS

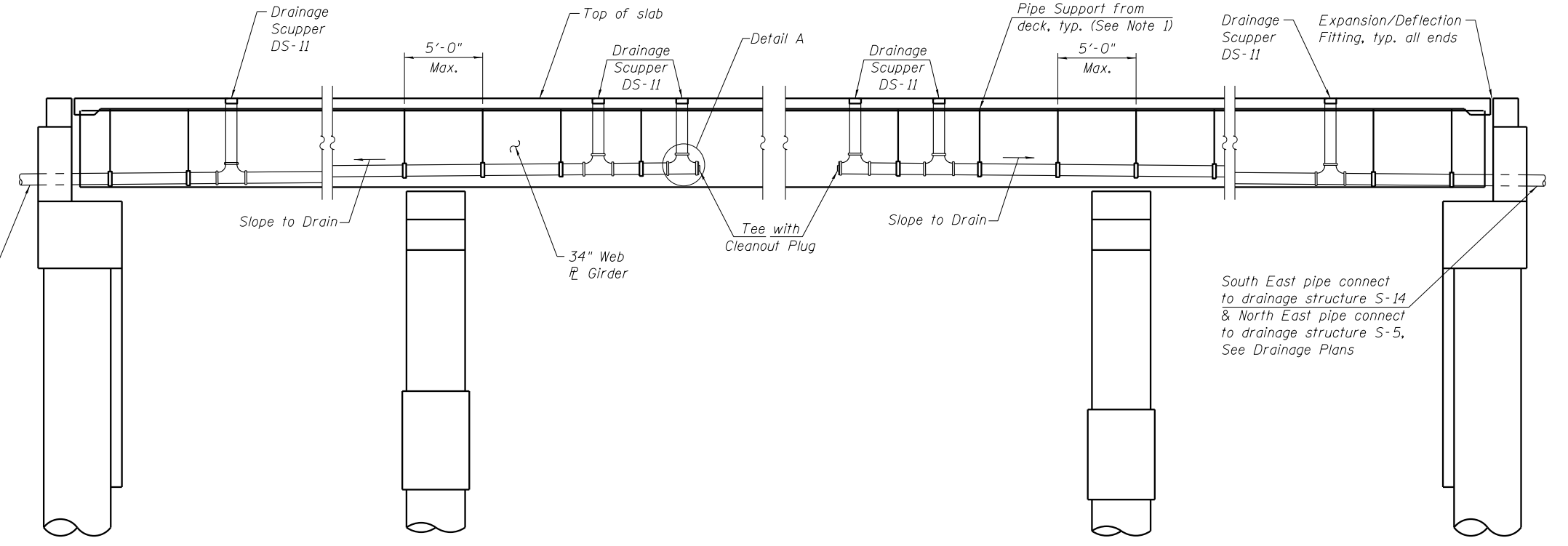
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| F.A.I. RE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 177 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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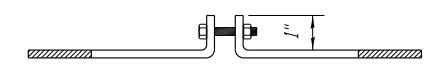
ELEVATION AT PIER 1 & 3
(Looking East at South sidewalk
North sidewalk similar, opposite hand)

South West pipe connect to drainage structure S-3 & North West pipe connect to drainage structure S-18, See Drainage Plans

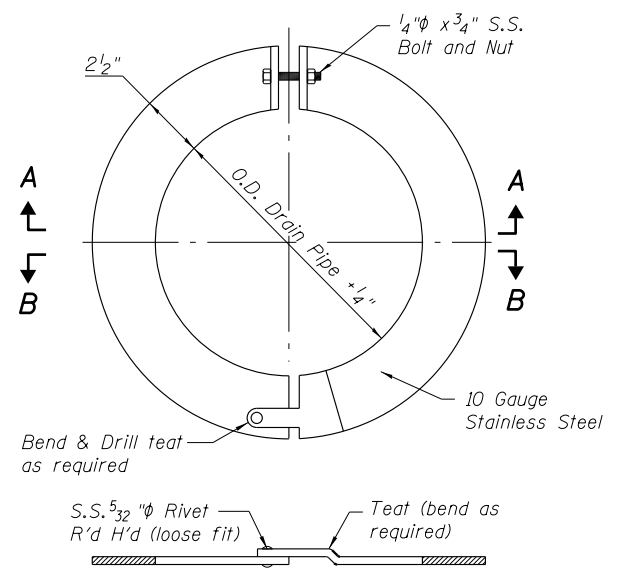


W. ABUTMENT **PIER 1** **PIER 3** **E. ABUTMENT**

ELEVATION
(Looking North)



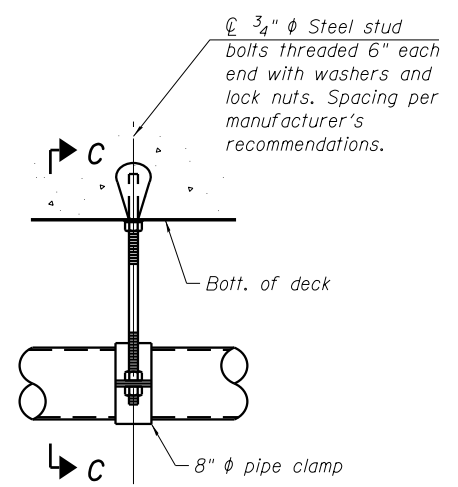
SECTION A-A



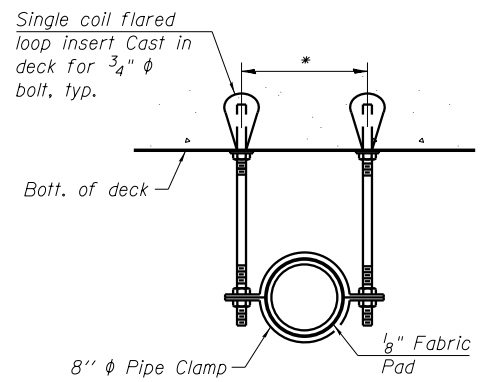
SECTION B-B

DETAIL OF EXPANSION COLLAR

Note:
S.S. denotes Stainless Steel.

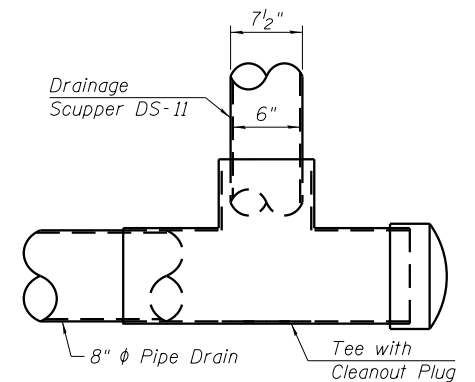


PIPE BRACKET DETAIL



SECTION C-C

* Dimension as required by Pipe Clamp



DETAIL A

Notes:

1. Provide structural support from proposed deck slab for drain pipe per manufacturer's recommendation, not to exceed 6' cts. Cost included with Drainage System.
2. All pipes, pipe fittings and brackets needed shall be included with cost of Drainage System.
3. See abutment details for block outs in backwalls of west and east abutments.
4. The drainage system shall be painted with a finish coat of gray, Munsell No. 5B 7/1. Cost included with Drainage System.

BILL OF MATERIAL

| Item | Unit | Quantity |
|-----------------|--------|----------|
| Drainage System | L. Sum | 1.0 |

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PARSONS BRINCKERHOFF

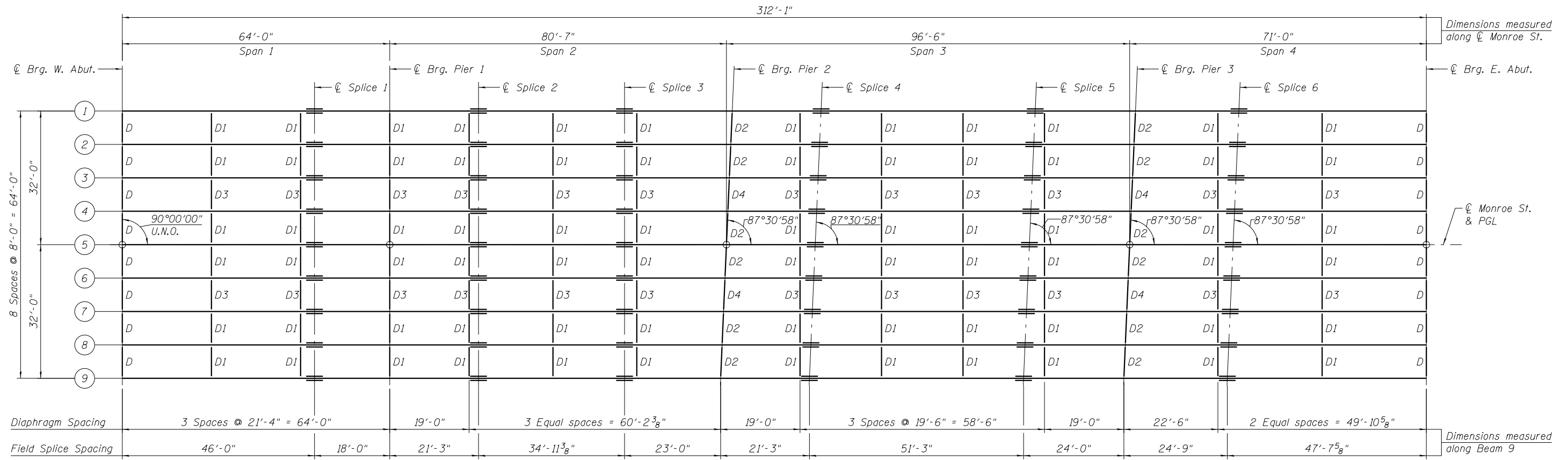
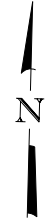
| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - HA | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - RD | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

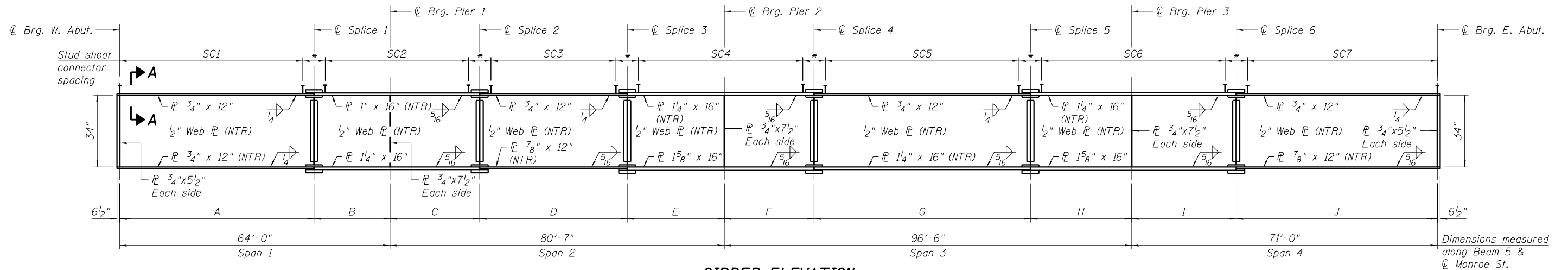
**DRAINAGE SYSTEM DETAILS
STRUCTURE NO. 016-1700**

SHEET NO. S1-26 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 178 |
| | | | CONTRACT NO. 60X95 | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN

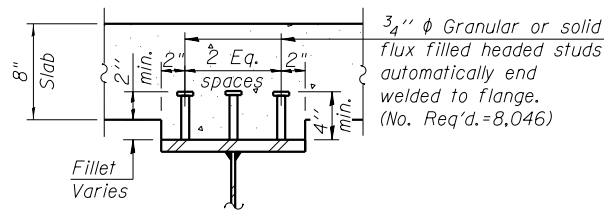


GIRDER ELEVATION

* 4'-6" (No studs)

Notes:

1. Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2.
2. For beam dimensions and Stud shear connector spacing, see tables on sheet S1-28 of S1-60.
3. All steel plates for girders, including bearing stiffeners and splice plates, shall be AASHTO M270, Grade 50.
4. All diaphragms, fill plates and connecting plates may be AASHTO M270, Grade 36.
5. Work this sheet with S1-28 and S1-29.



SECTION A-A

PARSONS BRINCKERHOFF

| | | |
|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - AH | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FRAMING PLAN
STRUCTURE NO. 016-1700**

SHEET NO. S1-27 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 179 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

0161700-60X95-S027-FRM.dgn

| INTERIOR GIRDER MOMENT TABLE | | | | | | | |
|------------------------------------|--------------------|--------|-----------|--------|-----------|--------|-----------|
| | 0.4 Sp. 1 | Pier 1 | 0.5 Sp. 2 | Pier 2 | 0.5 Sp. 3 | Pier 3 | 0.6 Sp. 4 |
| I_s | (in ⁴) | 7073 | 12655 | 7529 | 15914 | 9734 | 15914 |
| $I_c(n)$ | (in ⁴) | 21404 | | 23200 | | 33701 | |
| $I_c(3n)$ | (in ⁴) | 16227 | | 17449 | | 24099 | |
| $I_c(cr)$ | (in ⁴) | | 16972 | | 20544 | | 20544 |
| S_s | (in ³) | 398 | 750 | 439 | 943 | 696 | 943 |
| $S_c(n)$ | (in ³) | 616 | | 673 | | 1036 | |
| $S_c(3n)$ | (in ³) | 564 | | 617 | | 956 | |
| $S_c(cr)$ | (in ³) | | 845 | | 1043 | | 1043 |
| DC1 | (k/') | 1.01 | 1.08 | 1.01 | 1.11 | 1.05 | 1.11 |
| MDC1 | (k) | 260 | 530 | 169 | 749 | 374 | 834 |
| DC2 | (k/') | 0.46 | 0.46 | 0.46 | 0.46 | 0.46 | 0.46 |
| MDC2 | (k) | 138 | 258 | 96 | 349 | 203 | 387 |
| DW | (k/') | 0.24 | 0.24 | 0.24 | 0.24 | 0.24 | 0.24 |
| MDW | (k) | 69 | 130 | 48 | 175 | 100 | 194 |
| $M_L + IM$ | (k) | 807 | 1036 | 835 | 1277 | 1026 | 1275 |
| M_u (Strength I) | (k) | 2013 | 2993 | 1865 | 3870 | 2667 | 4049 |
| $\phi_r M_n$ | (k) | 3073 | | 3477 | | 4864 | |
| f_s DC1 | (ksi) | 7.83 | 8.48 | 4.62 | 9.53 | 6.45 | 10.61 |
| f_s DC2 | (ksi) | 2.94 | 3.66 | 1.87 | 4.01 | 2.55 | 4.45 |
| f_s DW | (ksi) | 1.47 | 1.85 | 0.93 | 2.01 | 1.25 | 2.23 |
| f_s (L+IM) | (ksi) | 15.72 | 14.72 | 14.89 | 14.69 | 11.88 | 14.66 |
| f_s (Service II) | (ksi) | 32.67 | 33.13 | 26.77 | 34.65 | 25.70 | 36.35 |
| 0.95R _h F _{yr} | (ksi) | 47.50 | 47.50 | 47.50 | 47.50 | 47.50 | 47.50 |
| f_s (Total)(Strength I) | (ksi) | 43.17 | 43.71 | 35.56 | 45.65 | 33.92 | 47.83 |
| $\phi_r F_n$ | (ksi) | | 50.00 | | 50.00 | | 50.00 |
| V _r | (k) | | 33.1 | | 36.1 | | 34.5 |

| INTERIOR GIRDER REACTION TABLE | | | | | | |
|--------------------------------|----------|--------|--------|--------|----------|-------|
| | W. Abut. | Pier 1 | Pier 2 | Pier 3 | E. Abut. | |
| R _{DC1} | (k) | 23.9 | 78.4 | 93.3 | 98.8 | 24.3 |
| R _{DC2} | (k) | 11.3 | 37.4 | 43.1 | 45.8 | 11.6 |
| R _{DW} | (k) | 5.8 | 19.1 | 22.0 | 23.3 | 6.0 |
| R _{L + IM} | (k) | 78.5 | 129.9 | 136.8 | 139.2 | 84.7 |
| R _{Total} | (k) | 119.5 | 264.8 | 295.1 | 307.0 | 126.5 |

I_s, S_s : Non-composite moment of inertia and section modulus of the steel section used for computing f_s (Total-Strength I, and Service II) due to non-composite dead loads (in⁴ and in³).

$I_c(n), S_c(n)$: Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing f_s (Total-Strength I, and Service II) in uncracked sections due to short-term composite live loads (in⁴ and in³).

$I_c(3n), S_c(3n)$: Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing f_s (Total-Strength I, and Service II) in uncracked sections, due to long-term composite (superimposed) dead loads (in⁴ and in³).

$I_c(cr), S_c(cr)$: Composite moment of inertia and section modulus of the steel and longitudinal deck reinforcement, used for computing f_s (Total-Strength I and Service II) in cracked sections, due to both short-term composite live loads and long-term composite (superimposed) dead loads (in⁴ and in³).

DC1: Un-factored non-composite dead load (kips/ft.).

MDC1: Un-factored moment due to non-composite dead load (kip-ft.).

DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).

MDC2: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).

DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).

MDW: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).

$M_L + IM$: Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).

M_u (Strength I): Factored design moment (kip-ft.).

$1.25 (MDC1 + MDC2) + 1.5 MDW + 1.75 M_L + IM$

$\phi_r M_n$: Compact composite positive moment capacity computed according to Article 6.10.7.1 or non-slender negative moment capacity according to Article A6.1.1 or A6.1.2 (kip-ft.).

f_s DC1: Un-factored stress at edge of flange for controlling steel flange due to vertical non-composite dead loads as calculated below (ksi).

$MDC1 / S_{nc}$

f_s DC2: Un-factored stress at edge of flange for controlling steel flange due to vertical composite dead loads as calculated below (ksi).

$MDC2 / S_c(3n)$ or $MDC2 / S_c(cr)$ as applicable.

f_s DW: Un-factored stress at edge of flange for controlling steel flange due to vertical composite future wearing surface loads as calculated below (ksi).

$MDW / S_c(3n)$ or $MDW / S_c(cr)$ as applicable.

f_s (L+IM): Un-factored stress at edge of flange for controlling steel flange due to vertical composite live load plus impact loads as calculated below (ksi).

$M_L + IM / S_c(n)$ or $M_L + IM / S_c(cr)$ as applicable.

f_s (Service II): Sum of stresses as computed below (ksi).

$f_{sDC1} + f_{sDC2} + f_{sDW} + 1.3 f_s (L + IM)$

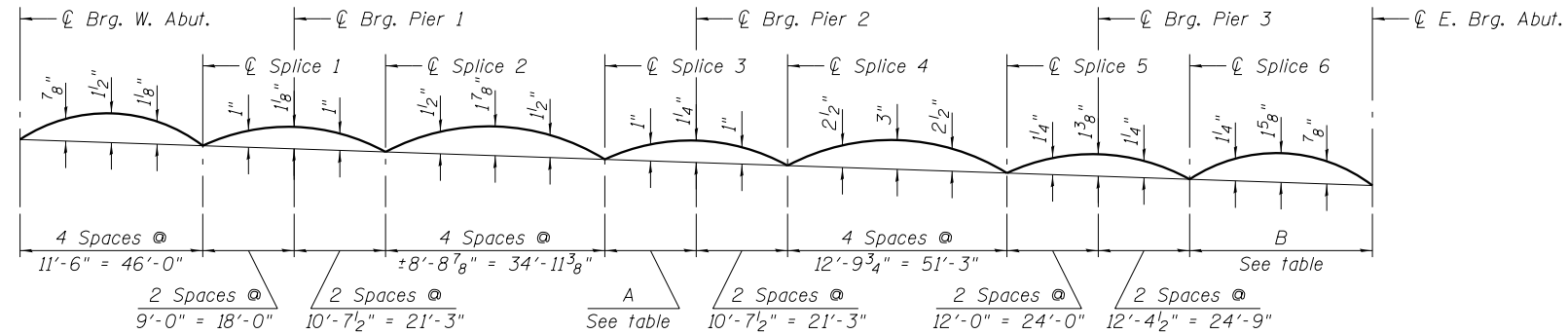
0.95R_hF_{yr}: Composite stress capacity for Service II loading according to Article 6.10.4.2 (ksi).

f_s (Total)(Strength I): Sum of stresses as computed below on non-compact section (ksi).

$1.25 (f_{sDC1} + f_{sDC2}) + 1.5 f_{sDW} + 1.75 f_s (L + IM)$

$\phi_r F_n$: Non-Compact composite positive or negative stress capacity for Strength I loading according to Article 6.10.7 or 6.10.8 (ksi).

V_r: Maximum factored shear range in span computed according to Article 6.10.10.



CAMBER DIAGRAM

TOP OF WEB ELEVATIONS
(For fabrication only)

| | Girder 1 | Girder 2 | Girder 3 | Girder 4 | Girder 5 | Girder 6 | Girder 7 | Girder 8 | Girder 9 |
|-----------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| ℄ Brg. W. Abut. | 596.72 | 596.85 | 596.97 | 597.10 | 597.22 | 597.10 | 596.97 | 596.85 | 596.72 |
| ℄ Splice 1 | 598.42 | 598.54 | 598.67 | 598.79 | 598.92 | 598.79 | 598.67 | 598.54 | 598.42 |
| ℄ Pier 1 | 598.96 | 599.08 | 599.21 | 599.33 | 599.46 | 599.33 | 599.21 | 599.08 | 598.96 |
| ℄ Splice 2 | 599.39 | 599.51 | 599.63 | 599.76 | 599.88 | 599.76 | 599.63 | 599.51 | 599.39 |
| ℄ Splice 3 | 599.81 | 599.93 | 600.06 | 600.18 | 600.31 | 600.18 | 600.06 | 599.93 | 599.81 |
| ℄ Pier 2 | 599.92 | 600.04 | 600.17 | 600.30 | 600.42 | 600.30 | 600.17 | 600.04 | 599.92 |
| ℄ Splice 4 | 599.81 | 599.93 | 600.06 | 600.19 | 600.32 | 600.19 | 600.07 | 599.95 | 599.83 |
| ℄ Splice 5 | 598.93 | 599.06 | 599.19 | 599.33 | 599.46 | 599.35 | 599.23 | 599.11 | 599.00 |
| ℄ Pier 3 | 598.20 | 598.34 | 598.47 | 598.61 | 598.75 | 598.63 | 598.52 | 598.41 | 598.29 |
| ℄ Splice 6 | 597.23 | 597.37 | 597.51 | 597.64 | 597.78 | 597.67 | 597.56 | 597.45 | 597.34 |
| ℄ Brg. E. Abut. | 595.15 | 595.28 | 595.40 | 595.53 | 595.65 | 595.53 | 595.40 | 595.28 | 595.15 |

CAMBER TABLE

| Girder | A | B |
|--------|--|--|
| 1 | 2 Spa. at ± 12'-10 ⁵ / ₈ " = 25'-9 ³ / ₈ " | 4 Spa. at ± 11'-2 ⁵ / ₈ " = 44'-10 ¹ / ₄ " |
| 2 | 2 Spa. at ± 12'-8 ⁵ / ₈ " = 25'-5 ¹ / ₈ " | 4 Spa. at ± 11'-3 ⁵ / ₈ " = 45'-2 ¹ / ₂ " |
| 3 | 2 Spa. at ± 12'-6 ¹ / ₂ " = 25'-1 ¹ / ₄ " | 4 Spa. at ± 11'-4 ⁵ / ₈ " = 45'-6 ⁵ / ₈ " |
| 4 | 2 Spa. at ± 12'-4 ³ / ₈ " = 24'-8 ³ / ₄ " | 4 Spa. at ± 11'-5 ³ / ₄ " = 45'-10 ³ / ₄ " |
| 5 | 2 Spa. at ± 12'-2 ³ / ₈ " = 24'-4 ⁵ / ₈ " | 4 Spa. at ± 11'-6 ³ / ₄ " = 46'-3" |
| 6 | 2 Spa. at ± 12'-0 ¹ / ₄ " = 24'-0 ¹ / ₂ " | 4 Spa. at ± 11'-7 ³ / ₄ " = 46'-7 ¹ / ₈ " |
| 7 | 2 Spa. at ± 11'-10 ¹ / ₈ " = 23'-8 ³ / ₈ " | 4 Spa. at ± 11'-8 ¹ / ₈ " = 46'-11 ¹ / ₄ " |
| 8 | 2 Spa. at ± 11'-8 ¹ / ₈ " = 23'-4 ¹ / ₈ " | 4 Spa. at ± 11'-9 ¹ / ₈ " = 47'-3 ¹ / ₂ " |
| 9 | 2 Spa. at ± 11'-6" = 23'-0" | 4 Spa. at ± 11'-10 ¹ / ₈ " = 47'-7 ⁵ / ₈ " |

BEAM DIMENSIONS

| Girder | A | B | C | D | E | F | G | H | I | J |
|--------|--------|--------|--------|--------------------------------------|-------------------------------------|--------|--------|--------|--------|--------------------------------------|
| 1 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 25'-9 ³ / ₈ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 44'-10 ¹ / ₄ " |
| 2 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 25'-5 ¹ / ₈ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 45'-2 ¹ / ₂ " |
| 3 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 25'-1 ¹ / ₄ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 45'-6 ⁵ / ₈ " |
| 4 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 24'-8 ³ / ₄ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 45'-10 ³ / ₄ " |
| 5 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 24'-4 ⁵ / ₈ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 46'-3" |
| 6 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 24'-0 ¹ / ₂ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 46'-7 ¹ / ₈ " |
| 7 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 23'-8 ³ / ₈ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 46'-11 ¹ / ₄ " |
| 8 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 23'-4 ¹ / ₈ " | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 47'-3 ¹ / ₂ " |
| 9 | 46'-0" | 18'-0" | 21'-3" | 34'-11 ³ / ₈ " | 23'-0" | 21'-3" | 51'-3" | 24'-0" | 24'-9" | 47'-7 ⁵ / ₈ " |

STUD SHEAR CONNECTOR SPACING

| Girder | SC1 | SC2 | SC3 | SC4 | SC5 | SC6 | SC7 |
|--------|--------------------|--------------------|--------------------|--------------------|------------------|--------------------|--------------------|
| 1 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 42 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 43 Spa. at ± 1'-0" |
| 2 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 42 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 43 Spa. at ± 1'-0" |
| 3 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 42 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 43 Spa. at ± 1'-0" |
| 4 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 41 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 44 Spa. at ± 1'-0" |
| 5 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 41 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 44 Spa. at ± 1'-0" |
| 6 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 41 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 44 Spa. at ± 1'-0" |
| 7 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 40 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 45 Spa. at ± 1'-0" |
| 8 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 40 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 45 Spa. at ± 1'-0" |
| 9 | 44 Spa. at ± 1'-0" | 35 Spa. at ± 1'-0" | 33 Spa. at ± 1'-0" | 40 Spa. at ± 1'-0" | 49 Spa. at 1'-0" | 44 Spa. at ± 1'-0" | 45 Spa. at ± 1'-0" |

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PARSONS BRINCKERHOFF

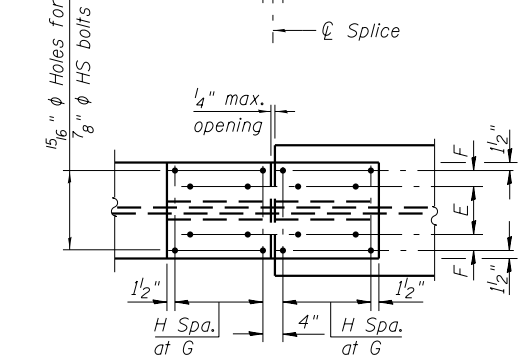
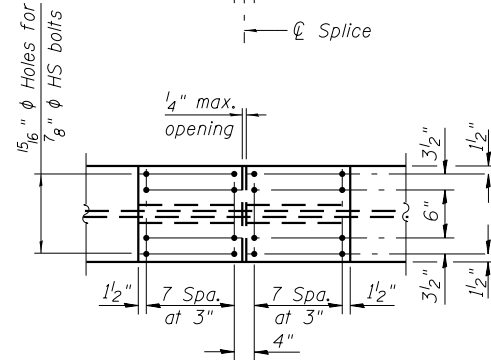
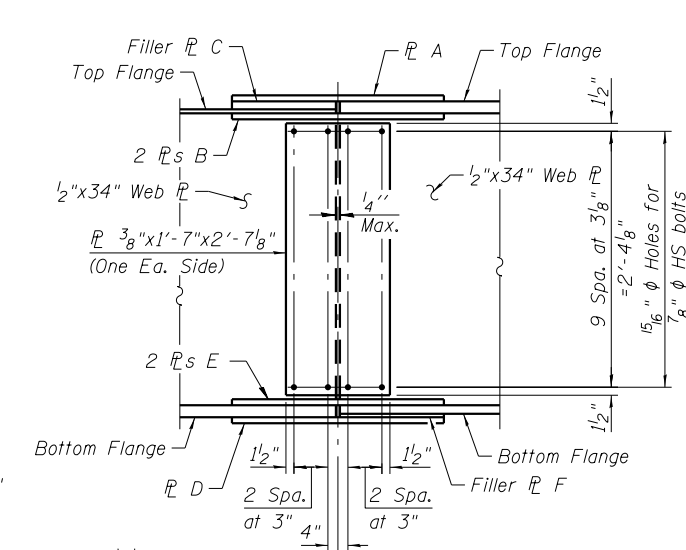
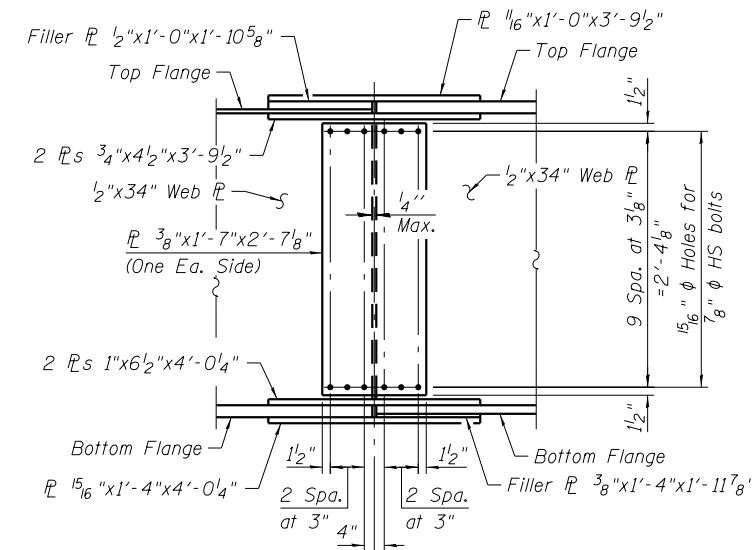
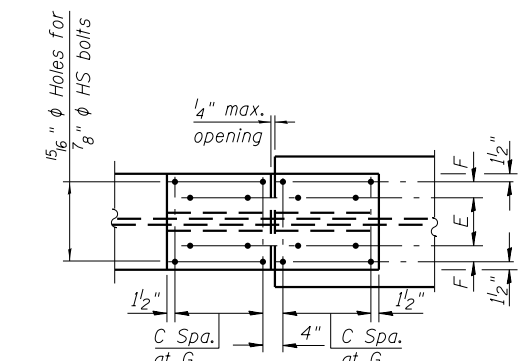
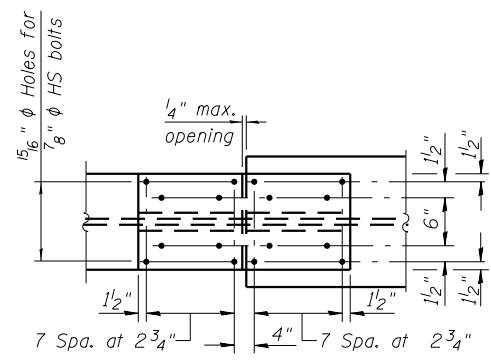
| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - AH | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE STEEL DETAILS I
STRUCTURE NO. 016-1700

SHEET NO. S1-28 OF S1-60 SHEETS

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 180 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |



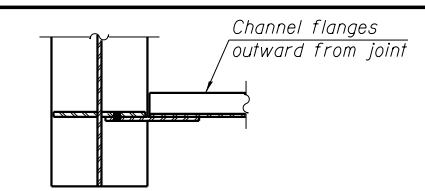
FIELD SPLICE DETAIL (SPLICES 4 & 5)
(18 Required)

FIELD SPLICE DETAIL (SPLICES 1, 2, 3 & 6)
(36 Required)

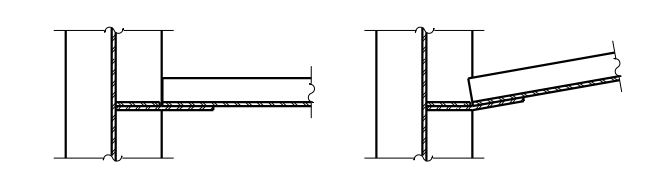
- Notes:
1. Load carrying components designated "NTR" shall conform to the Impact Testing Requirement, Zone 2
 2. All splice plates except filler plates shall meet NTR.
 3. All structural steel shall be AASHTO M 270 Grade 50.
 4. All diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual diaphragms at supports may be temporarily disconnected to install bearing anchor rods.
 5. All structural steel shall be hot dipped galvanized. Cost included with Furnishing and Erecting Structural Steel.

TABLE OF FIELD SPLICE DATA

| Splice | PL A | PL B | Filler PL C | PL D | PL E | Filler PL F | C | E | F | G | H |
|--------|---------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---------------------------|---|----|--------|--------|---|
| 1 | 1/16" x 1'-0" x 3'-9 1/2" | 3/4" x 4 1/2" x 3'-9 1/2" | 1/4" x 1'-0" x 1'-10 5/8" | 1/16" x 1'-0" x 3'-9 1/2" | 3/4" x 4 1/2" x 3'-9 1/2" | 1/2" x 1'-0" x 1'-10 5/8" | 7 | 6" | 1 1/2" | 2 3/4" | 7 |
| 2 | 1/16" x 1'-0" x 3'-9 1/2" | 3/4" x 4 1/2" x 3'-9 1/2" | 1/4" x 1'-0" x 1'-10 5/8" | 3/4" x 1'-0" x 4'-3" | 13/16" x 4 1/2" x 4'-3" | 3/8" x 1'-0" x 2'-13/8" | 7 | 6" | 1 1/2" | 2 3/4" | 8 |
| 3 | 1/16" x 1'-0" x 3'-9 1/2" | 3/4" x 4 1/2" x 3'-9 1/2" | 1/2" x 1'-0" x 1'-10 5/8" | 3/4" x 1'-0" x 4'-3" | 13/16" x 4 1/2" x 4'-3" | 3/4" x 1'-0" x 2'-13/8" | 7 | 6" | 1 1/2" | 2 3/4" | 8 |
| 6 | 1/16" x 1'-0" x 3'-9 1/2" | 3/4" x 4 1/2" x 3'-9 1/2" | 1/2" x 1'-0" x 1'-10 5/8" | 3/4" x 1'-0" x 4'-3" | 13/16" x 4 1/2" x 4'-3" | 3/4" x 1'-0" x 2'-13/8" | 7 | 6" | 1 1/2" | 2 3/4" | 8 |

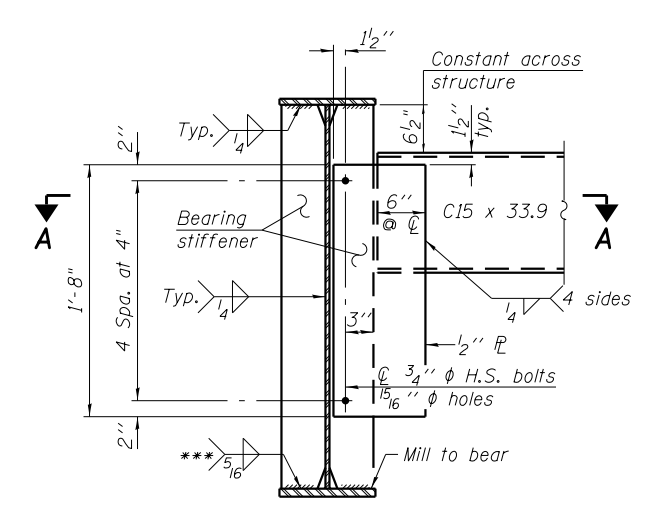


SECTION A-A



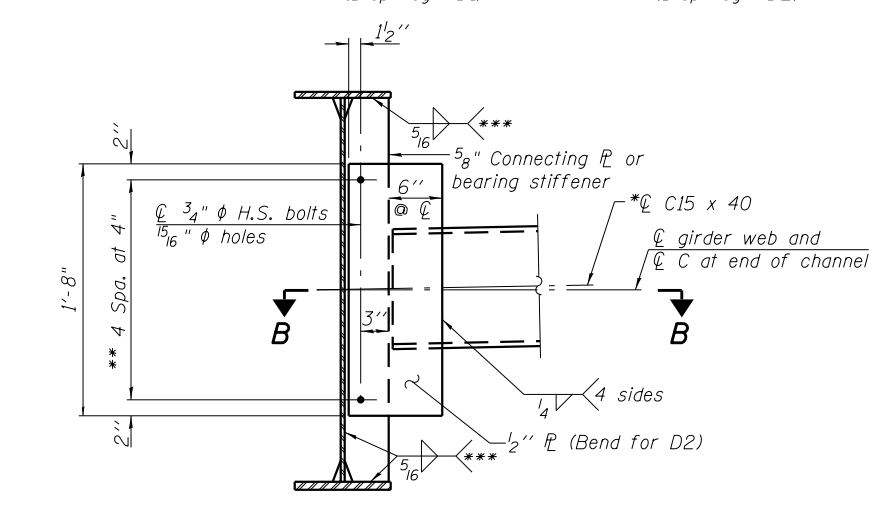
SECTION B-B (Diaphragm D1)

SECTION B-B (Diaphragm D2)



END DIAPHRAGM D
(16 Required)

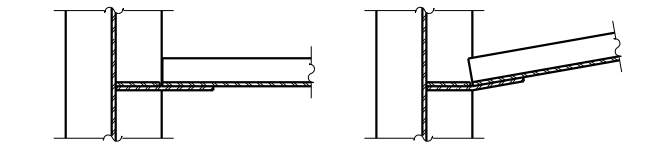
Note: Two hardened washers required for each set of oversized holes.



INTERIOR DIAPHRAGM D1 & D2
(72 Required - D1)
(12 Required - D2)

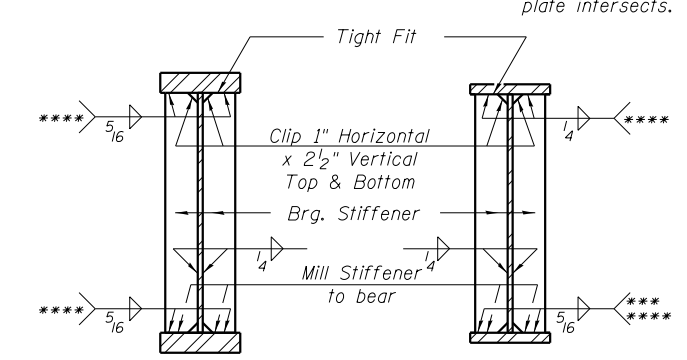
Notes:
Two hardened washers required for each set of oversized holes.
*Alternate channel are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section. The alternate, if utilized, shall be provided at no additional cost to the Department.
**3/4" H.S. bolts, 1/16" holes

*** Use 1/4" weld for location where flange thickness is 3/4".
**** Terminate 1/4" (+1/8") from end of plate intersects.



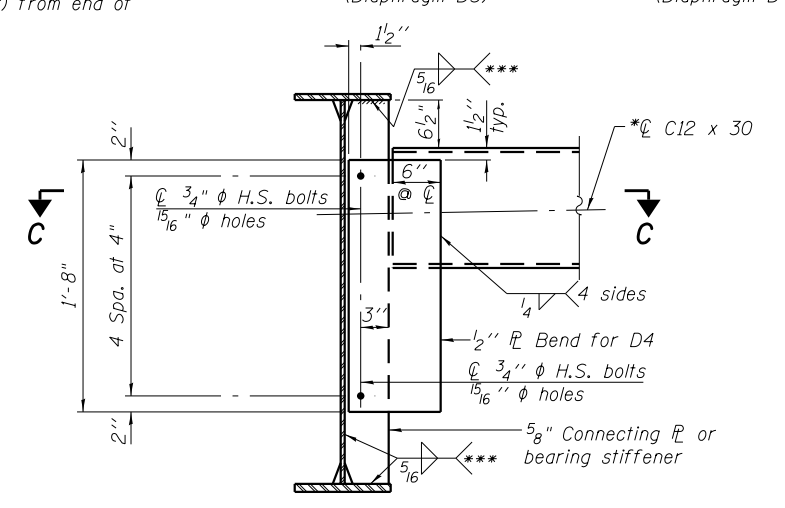
SECTION C-C (Diaphragm D3)

SECTION C-C (Diaphragm D4)



SECTION AT PIER

SECTION AT ABUTMENT



INTERIOR DIAPHRAGM D3 & D4
(24 Required - D3)
(4 Required - D4)

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PARSONS BRINCKERHOFF

USER NAME = lopezgonzalez
PLOT SCALE = N.T.S.
PLOT DATE = 9/1/2017

DESIGNED - AH
CHECKED - CB
DRAWN - DCP
CHECKED - JIG

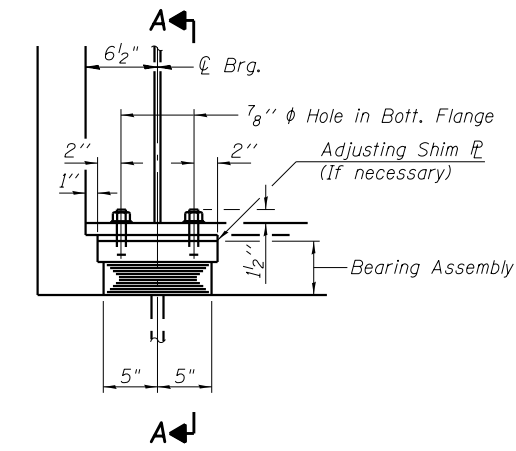
REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

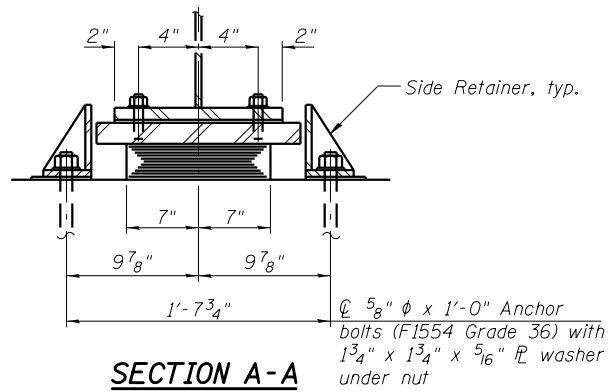
**SUPERSTRUCTURE STEEL DETAILS II
STRUCTURE NO. 016-1700**

SHEET NO. S1-29 OF S1-60 SHEETS

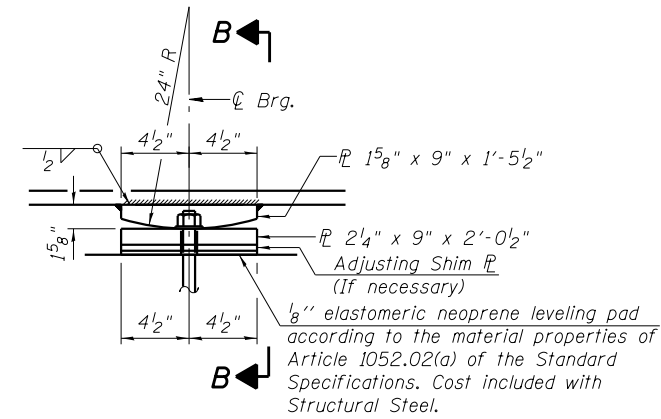
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|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RFE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 181 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



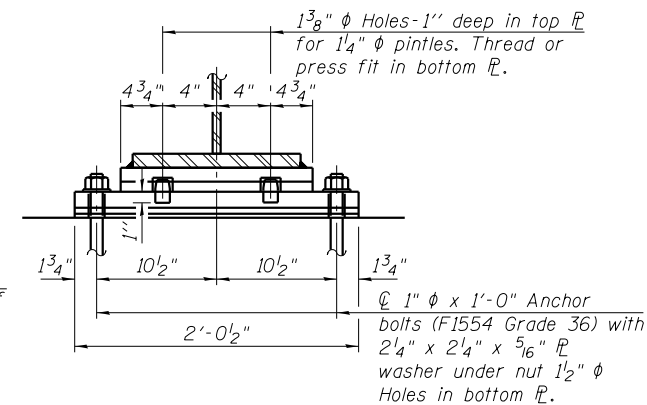
ELEVATION AT ABUT.



SECTION A-A



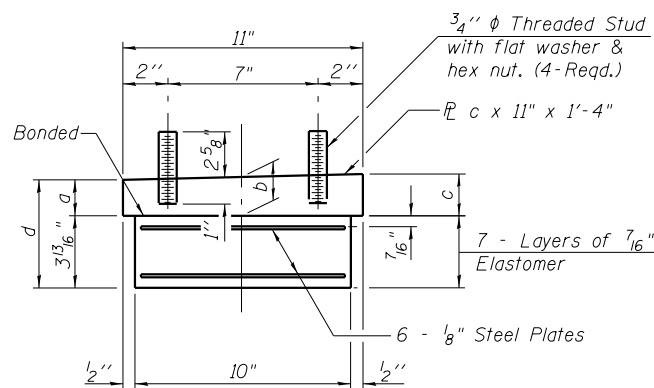
ELEVATION AT PIER 2



SECTION B-B

TYPE I ELASTOMERIC EXP. BRG.

(18 Required)



BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.

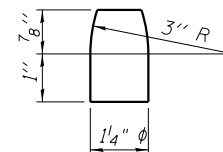
| | a | b | c | d |
|----------|--------|--------|---------|---------|
| W. Abut. | 1 1/2" | 1 3/4" | 1 5/16" | 5 5/16" |
| E. Abut. | 1 1/2" | 1 5/8" | 1 3/4" | 5 5/16" |

Notes:

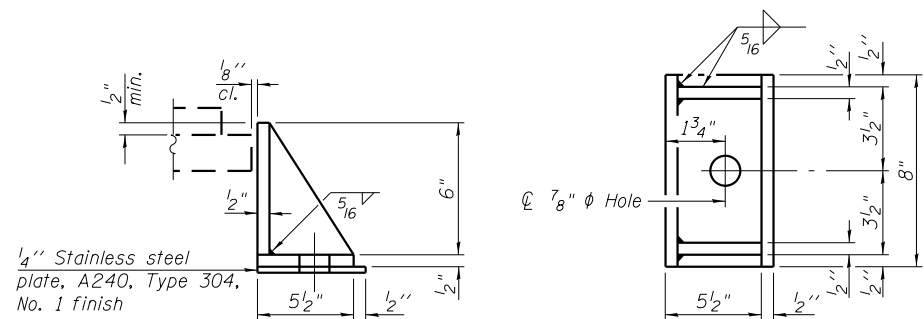
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.
Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
Fixed Bearing Assembly included in "Furnishing and Erecting Structural Steel."
All bearing plates, side retainers, anchor bolts, nuts, washers and pintles shall be galvanized according to AASHTO M111 or M232 as applicable.
The structural steel plates and pintles of the Fixed Bearing and the plates of the Bearing Assembly shall be AASHTO M270, Grade 50.

FIXED BEARING

(9 Required)



PINTLE



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

| Item | Unit | Total |
|-------------------------------------|------|-------|
| Elastomeric Bearing Assembly Type I | Each | 18 |
| Anchor Bolts 1" | Each | 18 |
| Anchor Bolts 5/8" | Each | 36 |

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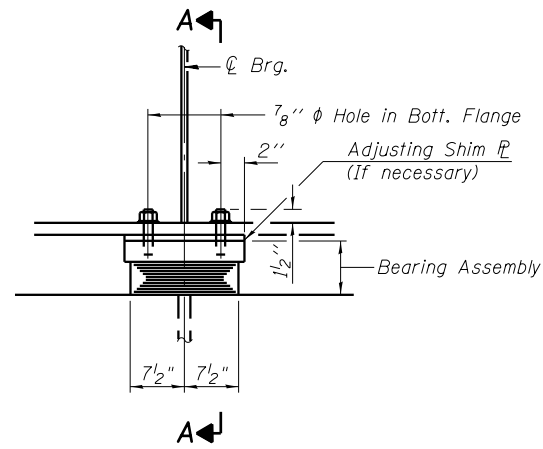
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|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

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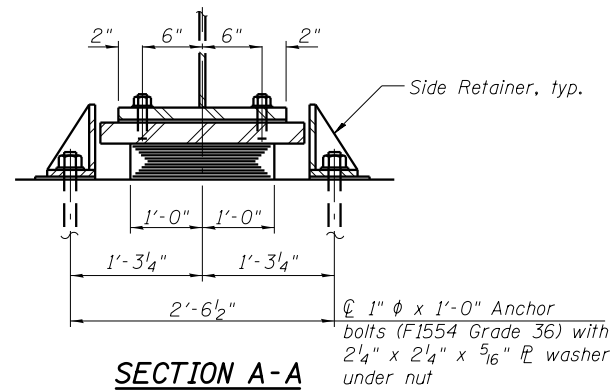
**BEARING DETAILS I
STRUCTURE NO. 016-1700**

SHEET NO. S1-30 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
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| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



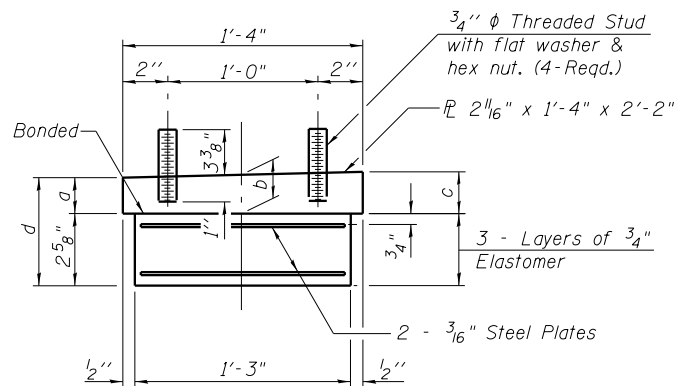
ELEVATION AT PIERS 1 & 3



SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.

(18 Required)



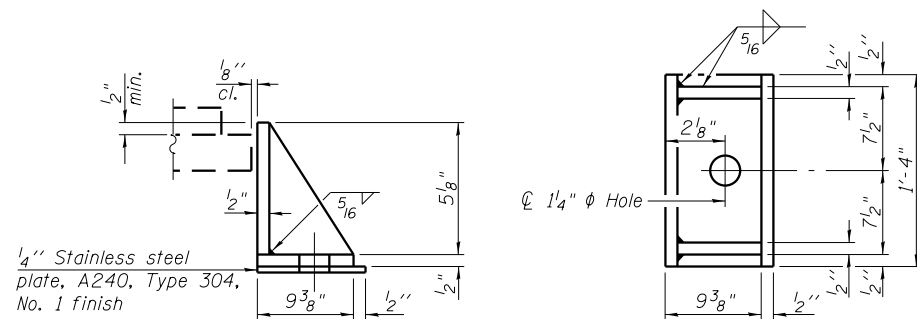
BEARING ASSEMBLY

Note:
Shim plates shall not be placed under Bearing Assembly.

| | a | b | c | d |
|--------|---------------------------------|----------------------------------|-----------------------------------|---------------------------------|
| Pier 1 | 2 ³ / ₈ " | 2 ⁵ / ₈ " | 2 ¹³ / ₁₆ " | 5" |
| Pier 3 | 2 ⁵ / ₈ " | 2 ⁵ / ₁₆ " | 3 ³ / ₁₆ " | 5 ¹ / ₄ " |

Notes:

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.
Side retainers and other steel members required for the elastomeric bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.
Beams shall be braced for stability during erection and remain braced until deck is poured and cured.
Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.
All (embedded and separate) bearing plates, side retainer, anchor bolts, nuts, washers and pintles shall be galvanized according to AASHTO M111 or M232 as applicable.
Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.
The structural steel plates of the bearing assembly shall be AASHTO M270, Grade 50.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

BILL OF MATERIAL

| Item | Unit | Total |
|-------------------------------------|------|-------|
| Elastomeric Bearing Assembly Type I | Each | 18 |
| Anchor Bolts 1" | Each | 36 |

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PARSONS BRINCKERHOFF

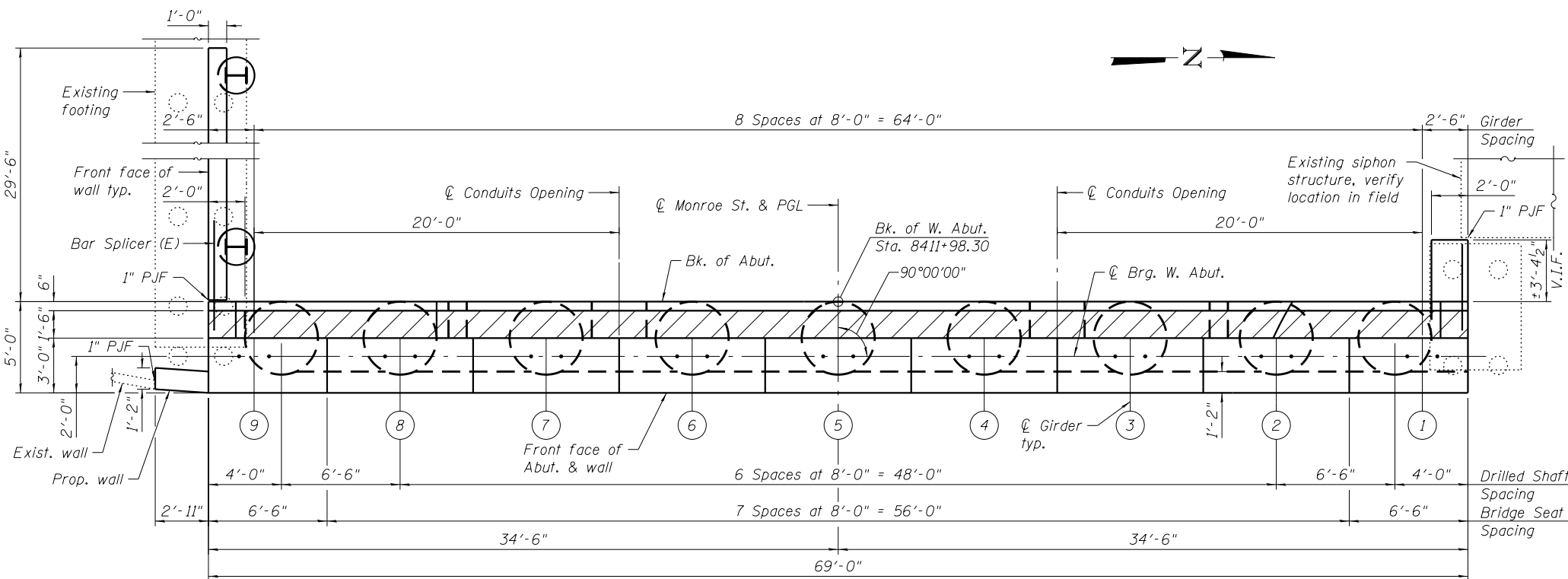
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| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

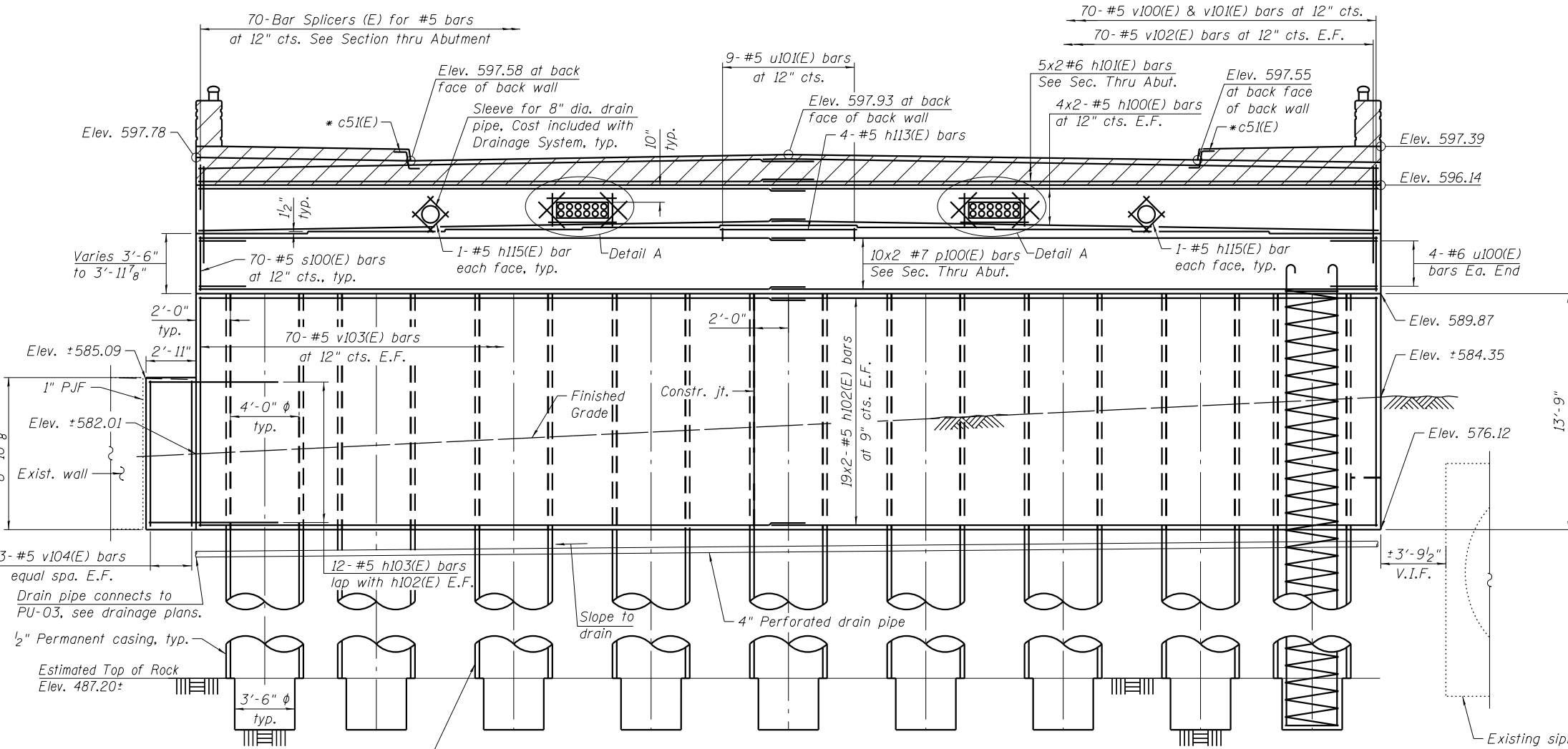
**BEARING DETAILS II
STRUCTURE NO. 016-1700**

SHEET NO. S1-31 OF S1-60 SHEETS

| | | | | |
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



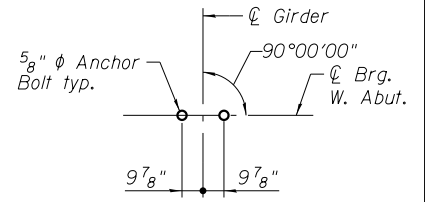
PLAN



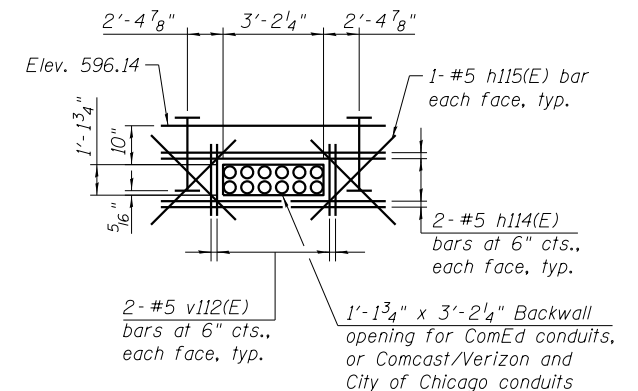
ELEVATION

TOP OF SEAT ELEVATION

| Girder No. | Seat Elevation |
|------------|----------------|
| 1 | 593.37 |
| 2 | 593.49 |
| 3 | 593.62 |
| 4 | 593.74 |
| 5 | 593.86 |
| 6 | 593.74 |
| 7 | 593.62 |
| 8 | 593.49 |
| 9 | 593.37 |



ANCHOR BOLT LAYOUT



DETAIL A

Note:
Conduit provided by Others.
Contractor to coordinate with utility owner for location and size of the utility blockouts. Cost of utility blockouts included in Concrete Structures. (See Electrical Plans.)

MINIMUM BAR LAP

- #4 bar = 2'-11"
- #5 bar = 3'-7"
- #6 bar = 4'-4"
- #7 bar = 5'-0"

* See approach sheet for sidewalk reinforcement

Notes:
Contractor shall adjust pile spacing to miss existing piles as necessary and as allowed by the Engineer. Cut reinforcement in field to miss utility openings. Space reinforcement in cap to miss anchor bolts. Pour steps monolithically with cap. Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure. See sheet S1-33 of S1-60 for Section thru abutment. See sheet S1-34 & S1-35 of S1-60 for wall details and Bill of Materials. Bars noted thus, 3x2-#5 indicates 3 lines of bars with 2 lengths of bars per line. Concrete Sealer shall be applied to abutment backwall bearing seats and exposed faces of abutment cap. Siphon location at the Northwest corner of abutment shall be verified in the field and approved by the Engineer prior to drilled shaft construction. B.F. denotes back face. F.F. denotes front face. E.F. denotes each face.

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PARSONS BRINCKERHOFF

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| USER NAME = lopezgonzalez | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
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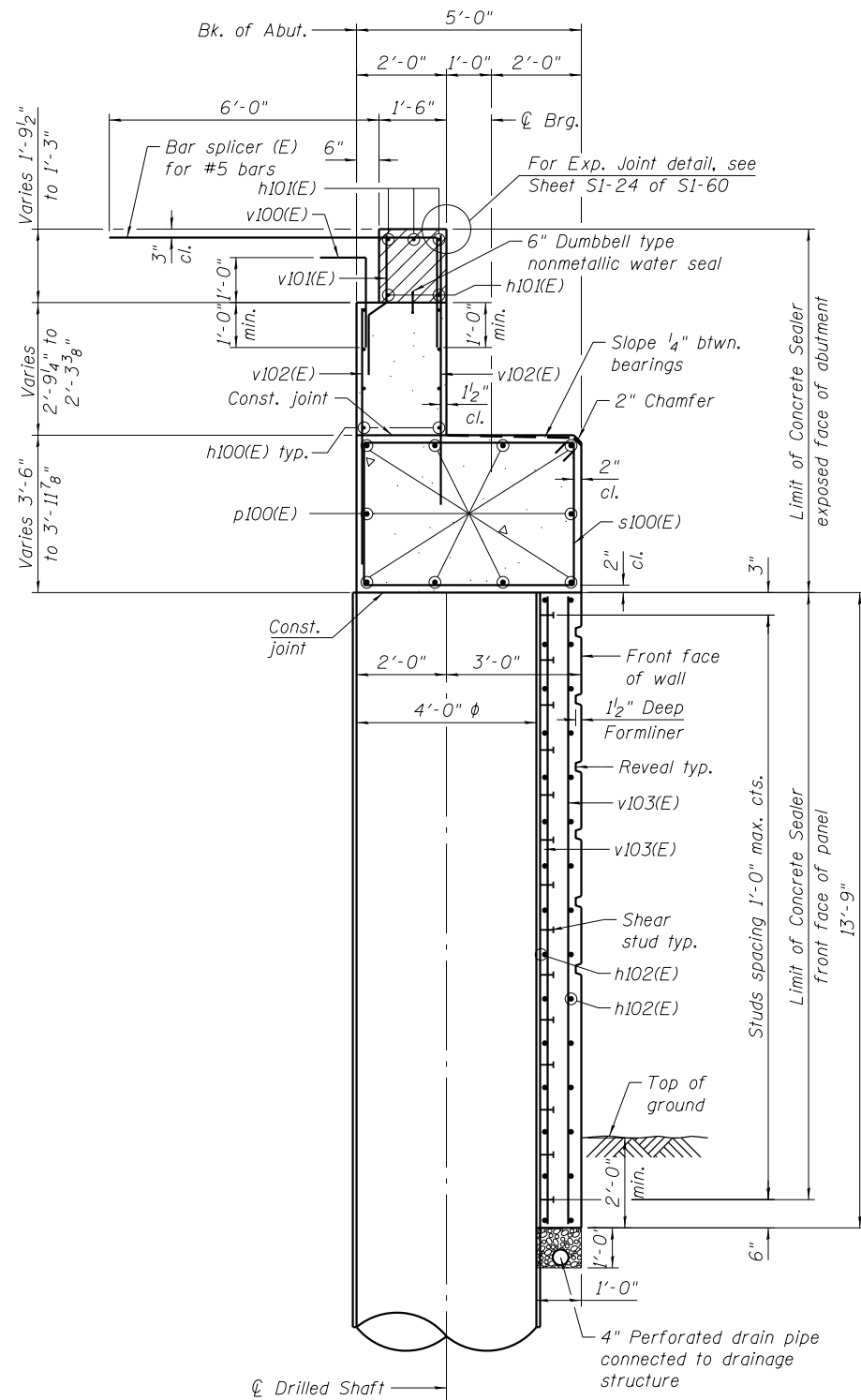
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WEST ABUTMENT
STRUCTURE NO. 016-1700**

SHEET NO. S1-32 OF S1-60 SHEETS

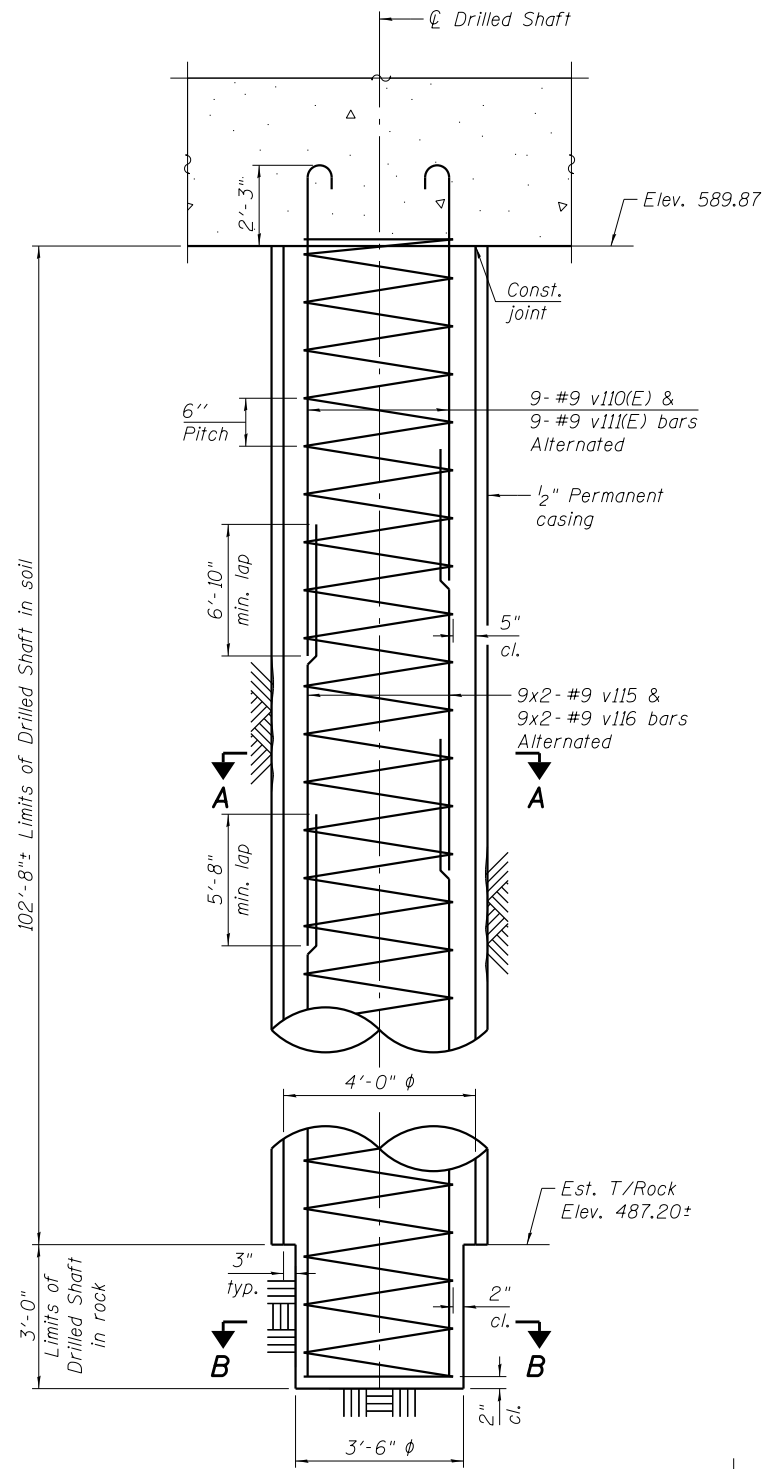
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 184 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT

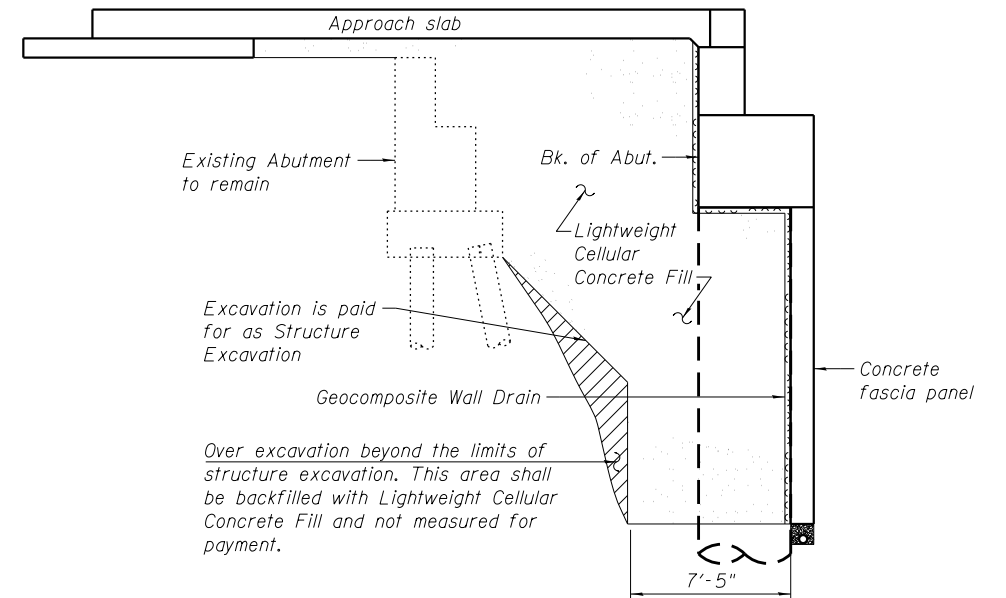
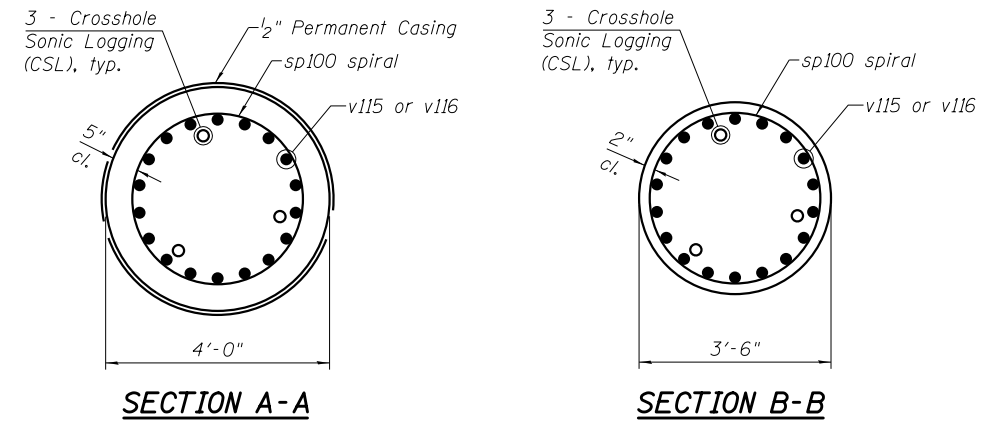


SECTION THRU ABUTMENT

* 3/4" x 6" Granular or solid flux filled headed studs conforming to article 1006.32 of the standard specifications automatically end welded to flange, typ.



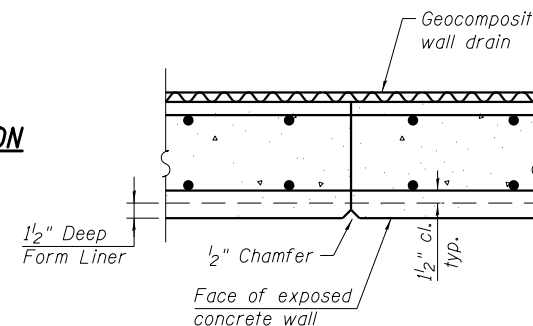
ABUTMENT SHAFT ELEVATION



SECTION THRU ABUTMENT
(Drilled Shaft not shown)

SUGGESTED CONSTRUCTION SEQUENCE

1. Install T.S.R.S. #1 and perform structural excavation.
2. Remove Existing Structures indicated in the removal plan, and portion of existing footing required for earth drilling operation. Cost of footing removal is included in Remove Existing Structures.
3. Construct South West Wingwall, see sheet S1-35 for suggested sequence of soldier pile wall construction.
4. Construct West Abutment.
5. Backfill Lightweight Cellular Concrete Fill and remove T.S.R.S. #1.



CONSTRUCTION JOINT

Notes:

1. See sheet S1-36 of S1-60 for reveal detail.
2. Bars noted thus, 14X2-#9 indicated 14 lines of bars with 2 lengths of bars per line.
3. Bars equally spaced, unless otherwise noted.
4. #4 sp100 spiral, each drilled shaft
 1. Provide 1/2 extra turns, shop welded together per AWS D1.4 top and bottom. Extend spiral 2" into abutment cap. Provide 4-#4 spacers or equivalent.
 2. When splicing spiral reinforcement is necessary, the spiral shall be provided with 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4 or shall both terminate with a 135° standard hook.

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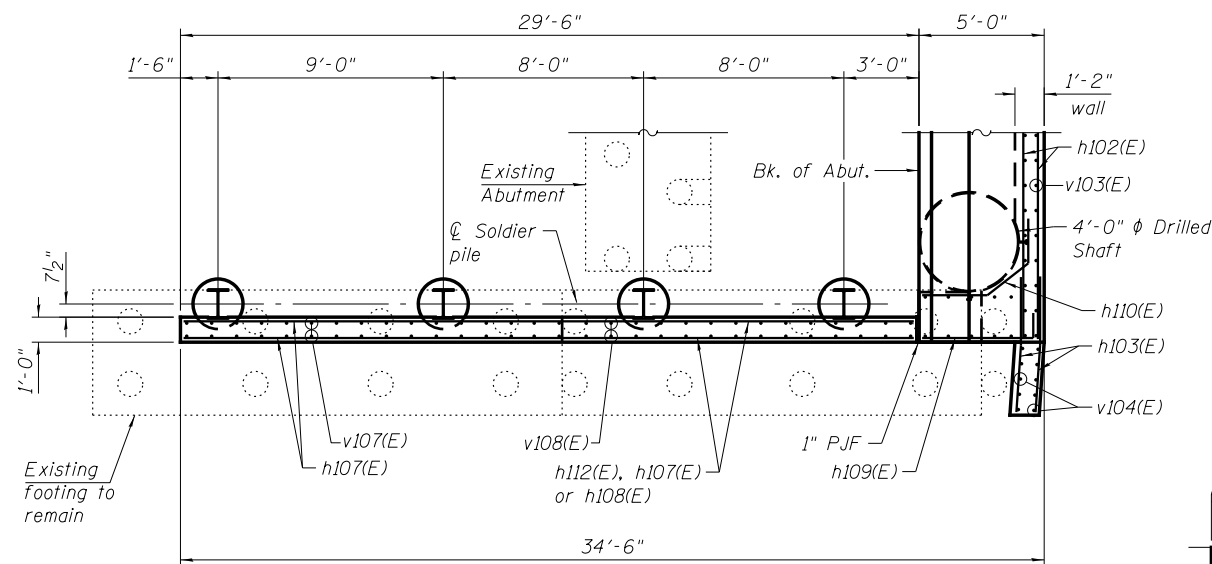
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| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

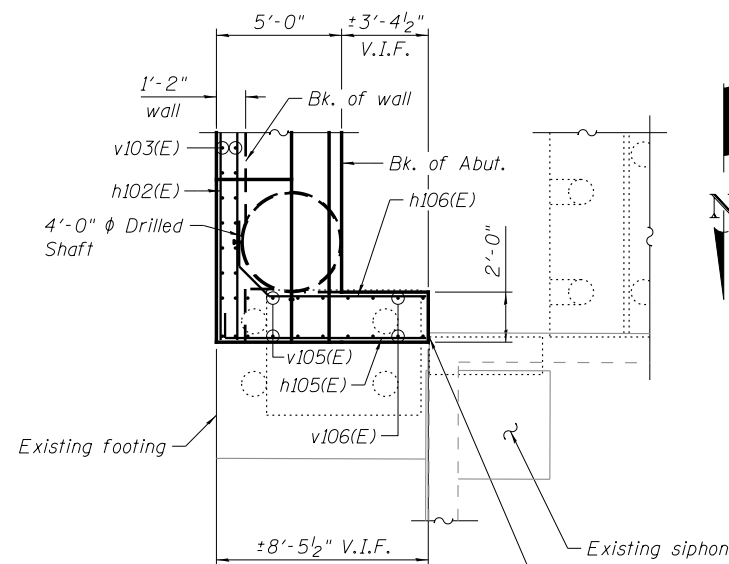
**WEST ABUTMENT DETAILS I
STRUCTURE NO. 016-1700**

SHEET NO. S1-33 OF S1-60 SHEETS

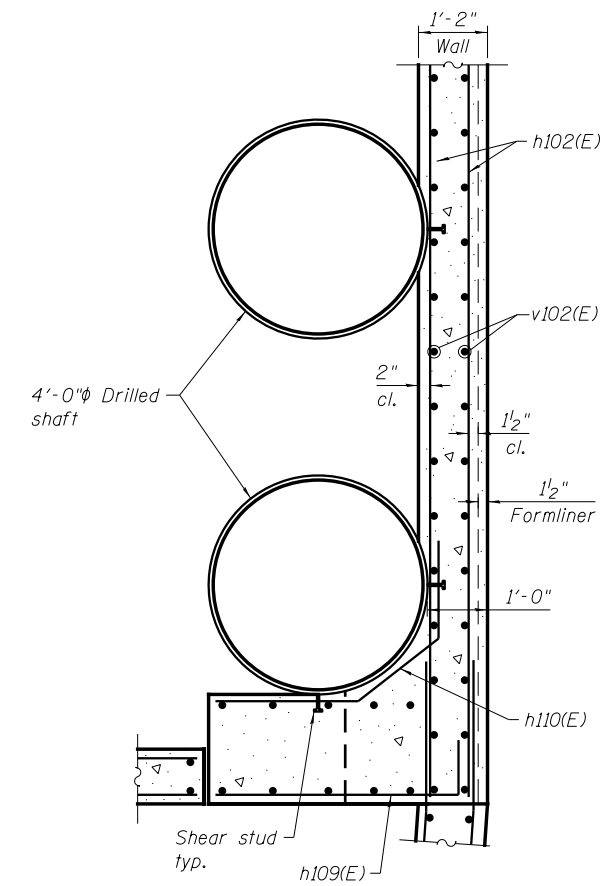
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 185 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



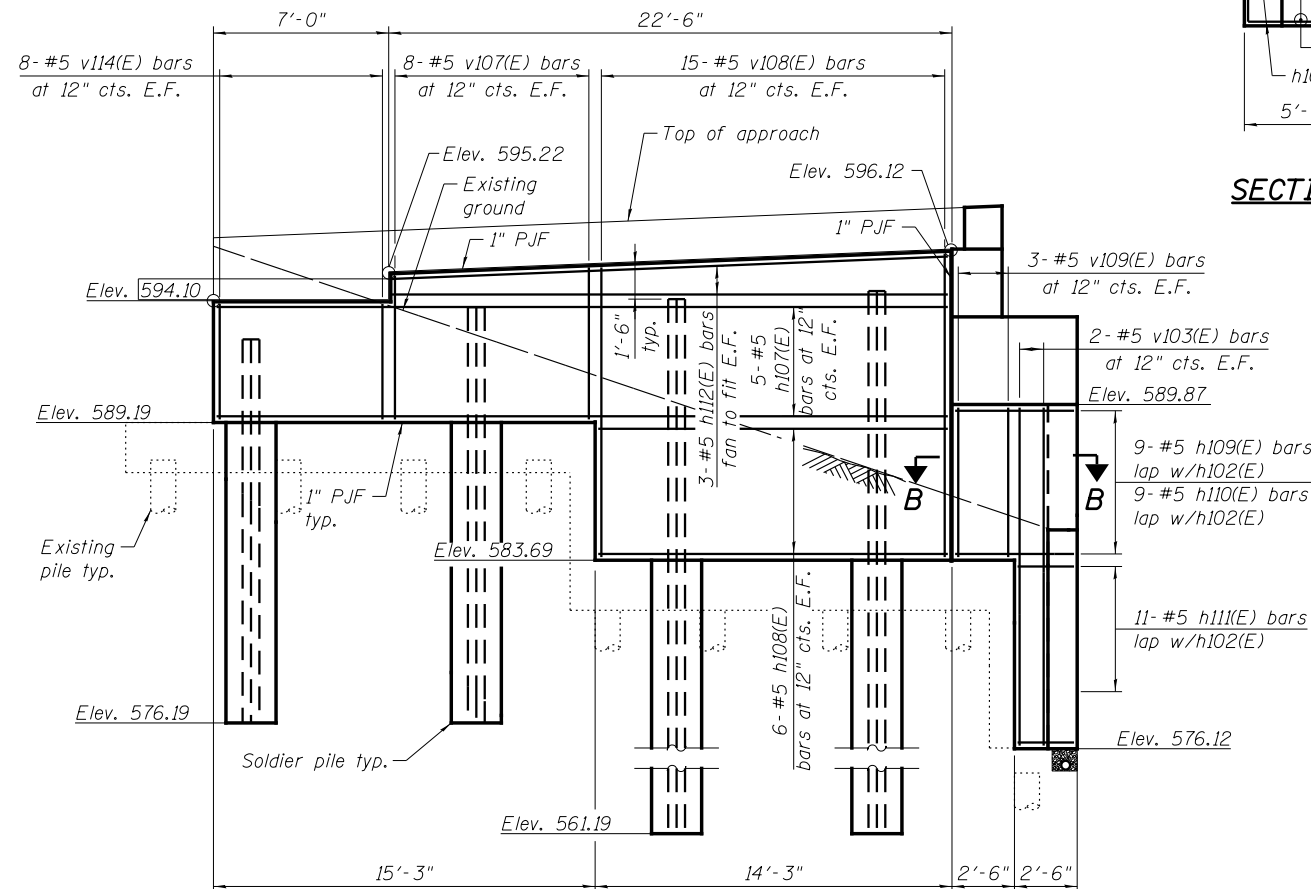
SOUTHWEST SOLDIER PILE WALL PLAN



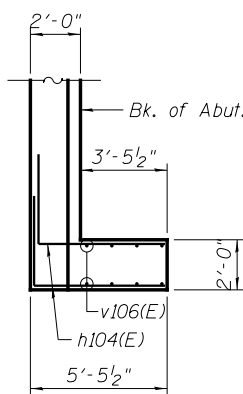
NORTHWEST WALL PLAN



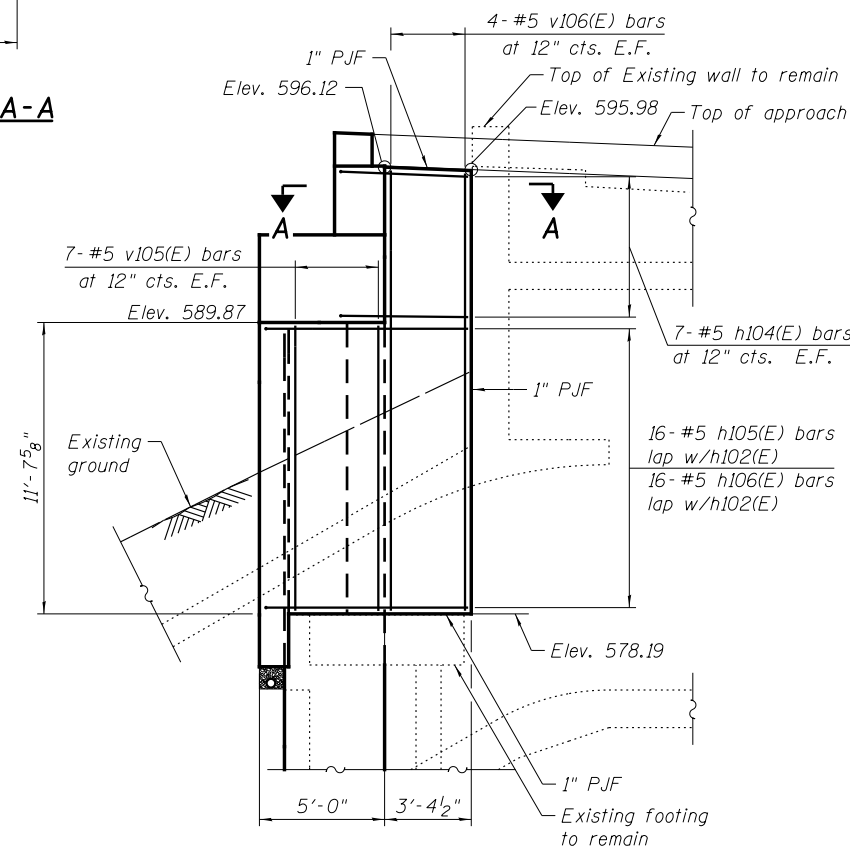
SECTION B-B



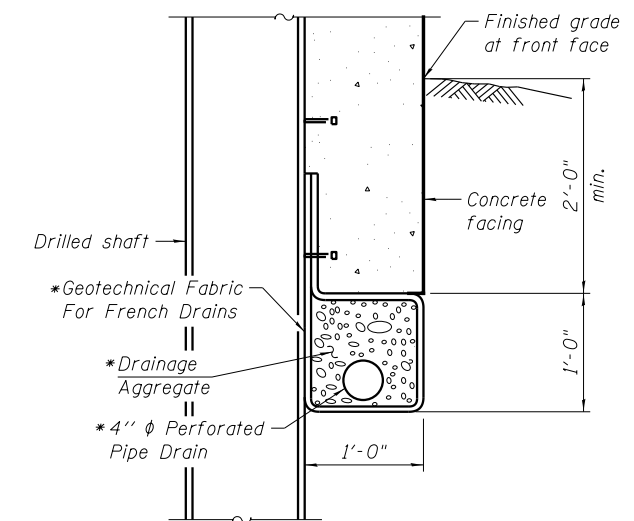
SOUTHWEST SOLDIER PILE WALL ELEVATION



SECTION A-A



NORTHWEST WALL ELEVATION



PIPE UNDERDRAIN DETAIL THRU ABUTMENT

* Included in the cost of Pipe Underdrains for Structures, 4"

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| | | |
|------------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - JZ | REVISED - |
| PLLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

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DEPARTMENT OF TRANSPORTATION**

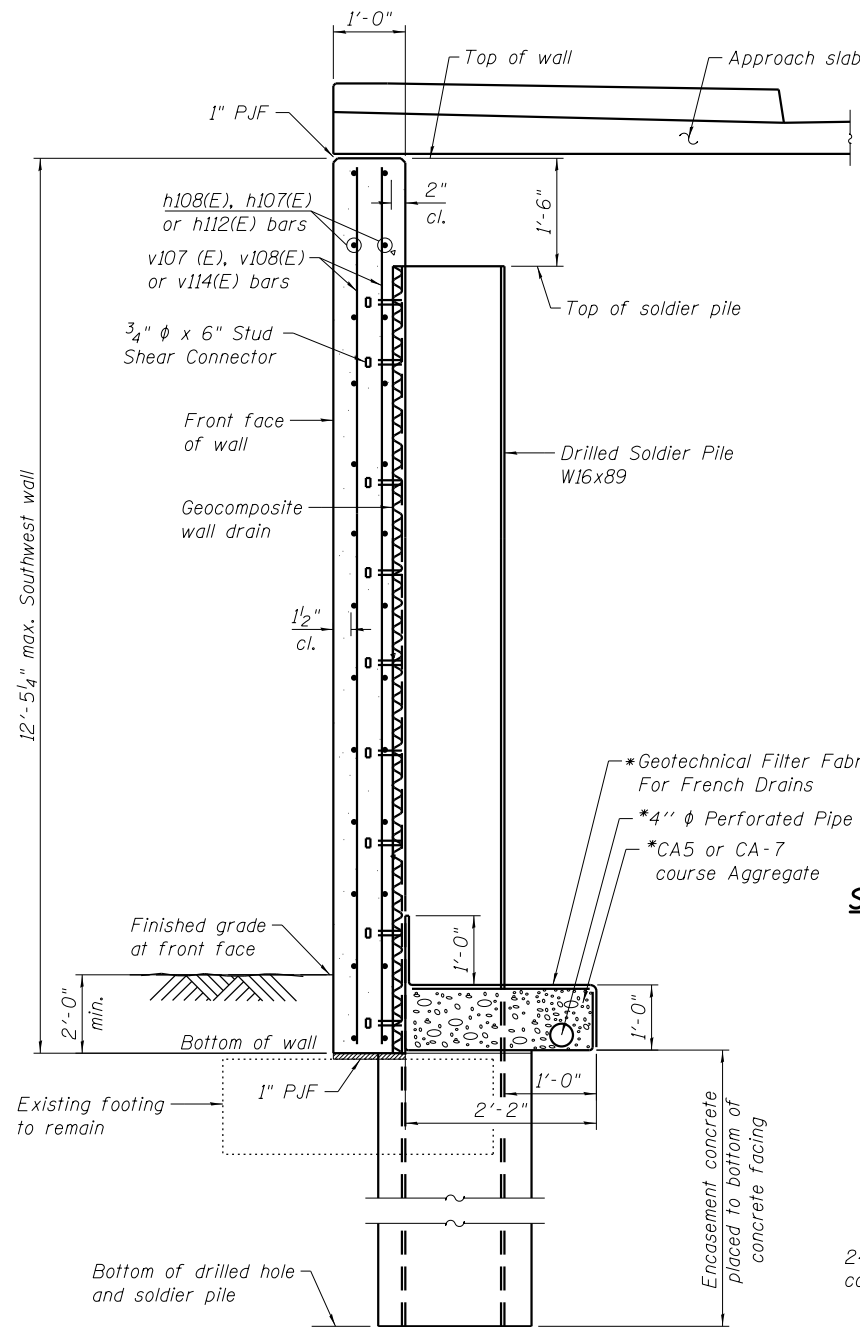
**WEST ABUTMENT DETAILS II
STRUCTURE NO. 016-1700**

SHEET NO. S1-34 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 186 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT

12'-5 1/4" max. Southwest wall



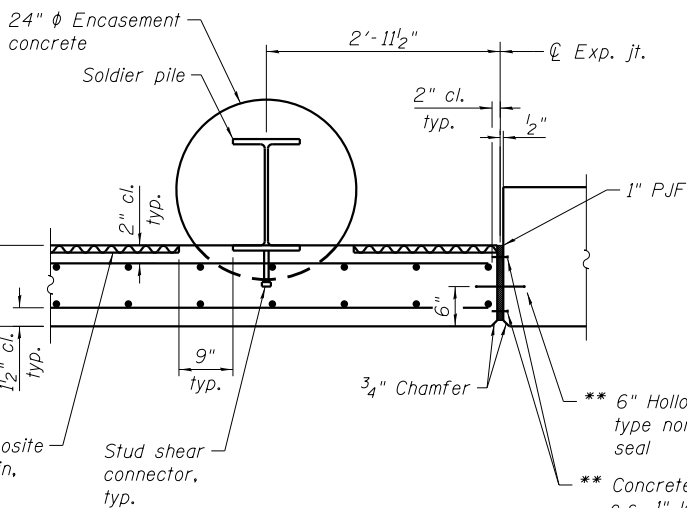
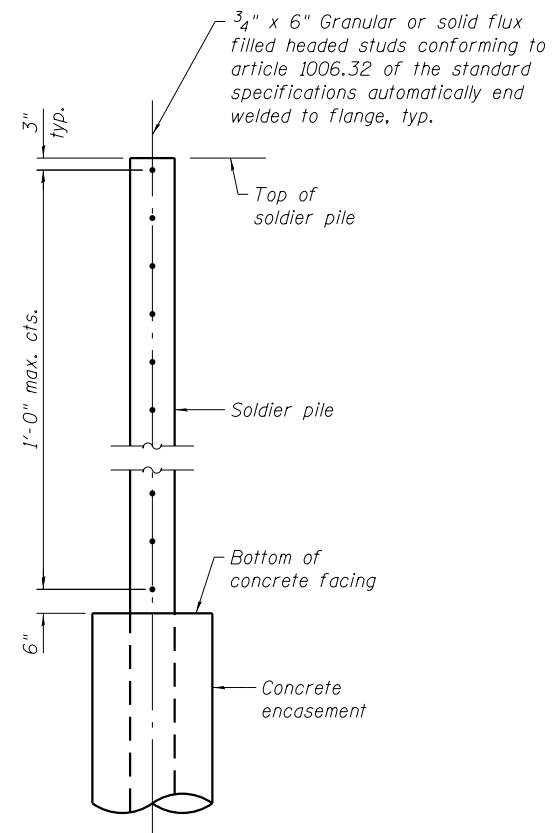
SECTION THRU SOLDIER PILE WALL

* Included in the cost of Pipe Underdrains for Structures.

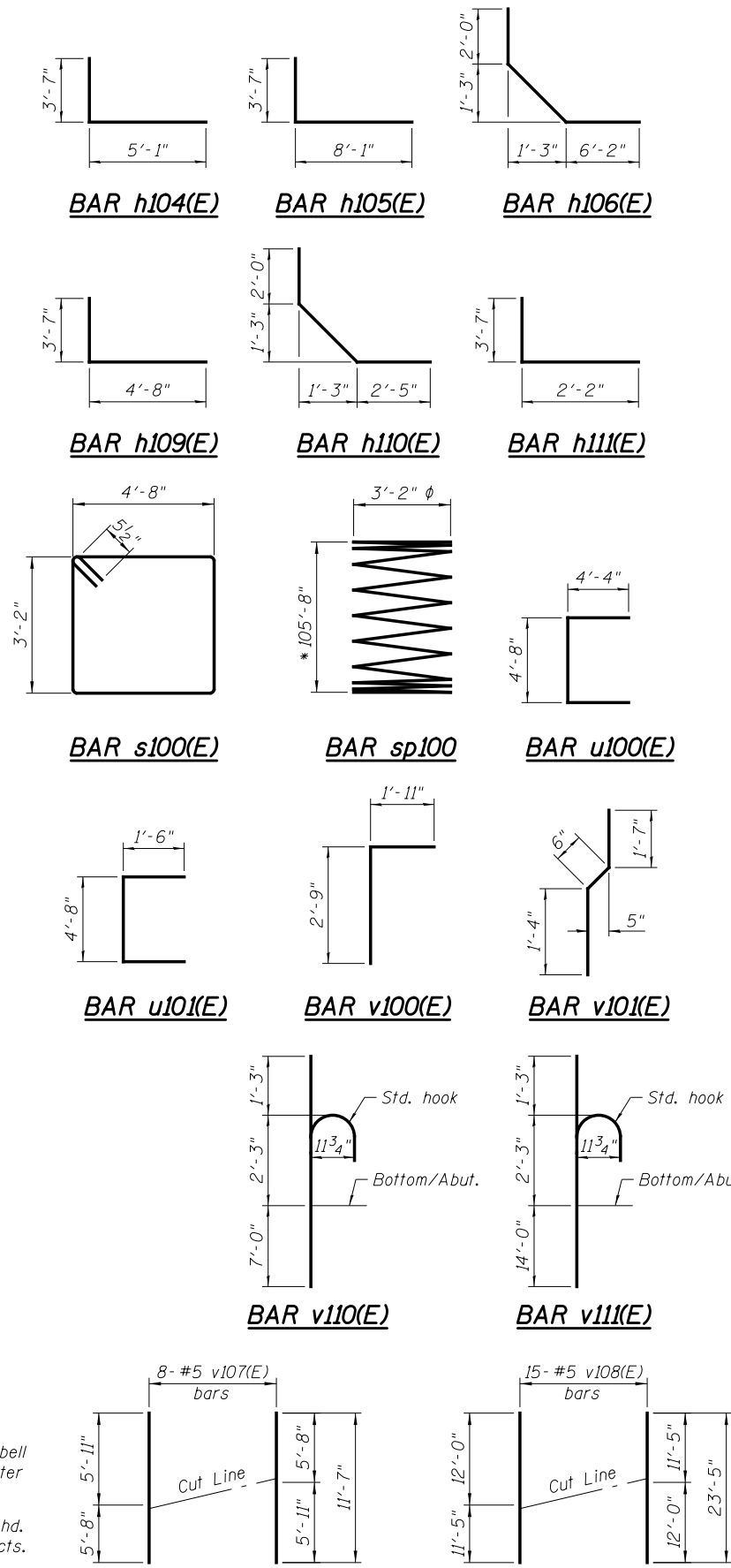
SUGGESTED SEQUENCE OF SOLDIER PILE WALL CONSTRUCTION

1. Drill hole for soldier pile to the tip elevation indicated on the plans.
2. Remove loose material and excess water from hole and set soldier pile in hole, using temporary bracing to maintain correct elevation, clearances, and position during and after placement of concrete.
3. Place encasement concrete around soldier pile to the level indicated on plans.
4. Place formwork and geocomposite wall drain.
5. Place the 4" ϕ perforated pipe drain, geotechnical filter fabric for french drains, and course aggregate.
6. Install stud shear connectors to soldier piles as shown on plan.
7. Construct wall panels.

SHEAR STUD CONNECTOR LAYOUT



EXPANSION JOINT DETAIL



CUTTING DIAGRAM

Order bars full length. Cut as shown. Use the remainder on opposite end.

CUTTING DIAGRAM

Order bars full length. Cut as shown. Use the remainder on opposite end.

BILL OF MATERIAL

| Bar No. | Size | Length | Shape |
|--|---------|---------|-------|
| h100(E) | 8 #5 | 36'-2" | — |
| h101(E) | 10 #6 | 36'-6" | — |
| h102(E) | 76 #5 | 36'-2" | — |
| h103(E) | 24 #5 | 6'-4" | — |
| h104(E) | 14 #5 | 8'-8" | — |
| h105(E) | 16 #5 | 11'-8" | — |
| h106(E) | 16 #5 | 9'-11" | — |
| h107(E) | 10 #5 | 29'-1" | — |
| h108(E) | 12 #5 | 13'-10" | — |
| h109(E) | 9 #5 | 8'-3" | — |
| h110(E) | 9 #5 | 6'-2" | — |
| h111(E) | 11 #5 | 5'-9" | — |
| h112(E) | 6 #5 | 22'-0" | — |
| h113(E) | 4 #5 | 7'-8" | — |
| h114(E) | 16 #5 | 5'-3" | — |
| h115(E) | 32 #5 | 2'-6" | — |
| p100(E) | 20 #7 | 36'-10" | — |
| s100(E) | 70 #5 | 16'-7" | □ |
| sp100 | 9 #5 | 105'-8" | WWW |
| u100(E) | 8 #6 | 13'-4" | □ |
| u101(E) | 9 #5 | 7'-8" | □ |
| v100(E) | 70 #5 | 4'-8" | — |
| v101(E) | 70 #4 | 3'-5" | — |
| v102(E) | 140 #5 | 5'-11" | — |
| v103(E) | 144 #5 | 13'-5" | — |
| v104(E) | 6 #5 | 8'-6" | — |
| v105(E) | 14 #5 | 11'-3" | — |
| v106(E) | 8 #5 | 17'-4" | — |
| v107(E) | 16 #5 | 11'-2" | — |
| v108(E) | 15 #5 | 23'-5" | — |
| v109(E) | 6 #5 | 5'-11" | — |
| v110(E) | 81 #9 | 10'-6" | — |
| v111(E) | 81 #9 | 17'-6" | — |
| v112(E) | 16 #5 | 3'-2" | — |
| v114(E) | 16 #5 | 4'-6" | — |
| v115 | 162 #9 | 45'-10" | — |
| v116 | 162 #9 | 49'-4" | — |
| Structure Excavation | Cu. Yd. | 512 | |
| Concrete Structures | Cu. Yd. | 113 | |
| Stud Shear Connectors | Each | 187 | |
| Reinforcement Bars | Pound | 72,460 | |
| Reinforcement Bars, Epoxy Coated | Pound | 20,500 | |
| Permanent Casing | Foot | 925 | |
| Drilled Shaft in Soil | Cu. Yd. | 431 | |
| Drilled Shaft in Rock | Cu. Yd. | 10 | |
| Furnishing Soldier Piles (W Section) | Foot | 103 | |
| Drilling and Setting Soldier Piles (In Soil) | Cu. Ft. | 224 | |
| Concrete Sealer | Sq. Ft. | 1,963 | |
| Geocomposite Wall Drain | Sq. Yd. | 181 | |
| Lightweight Cellular Concrete Fill | Cu. Yd. | 502 | |
| Pipe Underdrains for Structures 4" | Foot | 102 | |
| Slope Inclinometer | Each | 1 | |
| Crosshole Sonic Logging Access Ducts | Foot | 951 | |
| Crosshole Sonic Logging Testing | Each | 2 | |

* Length is height of spiral

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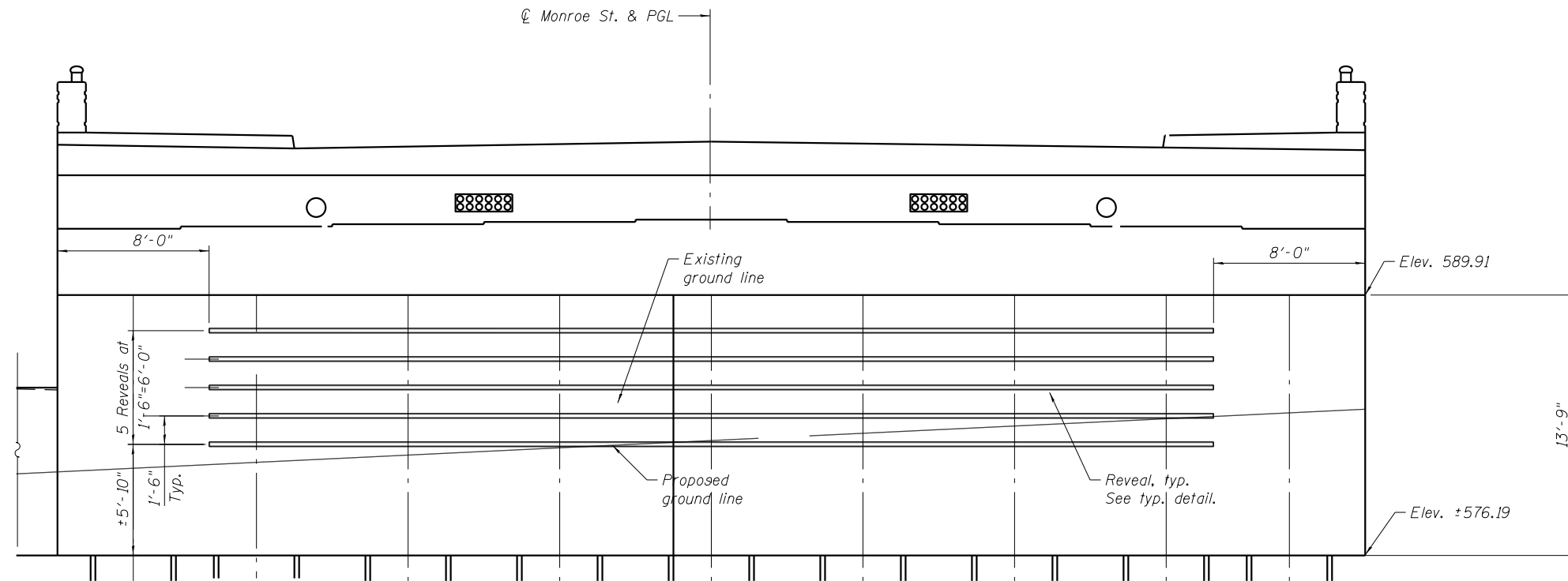
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| USER NAME = lopezgonzalez | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

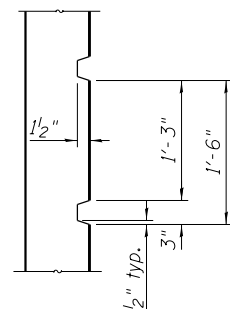
WEST ABUTMENT DETAILS III STRUCTURE NO. 016-1700

SHEET NO. S1-35 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 187 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



ELEVATION



TYPICAL REVEAL DETAIL

1. The 3" x 1/2" reveal in the fascia panel will not be paid separately and shall be included in the cost of the pay item Concrete Structures.

BILL OF MATERIAL

| Item | Unit | Total |
|---------------|---------|-------|
| Rubbed Finish | Sq. Ft. | 750 |

0161700-60X95-5036-ABA.dgn

**PARSONS
BRINCKERHOFF**

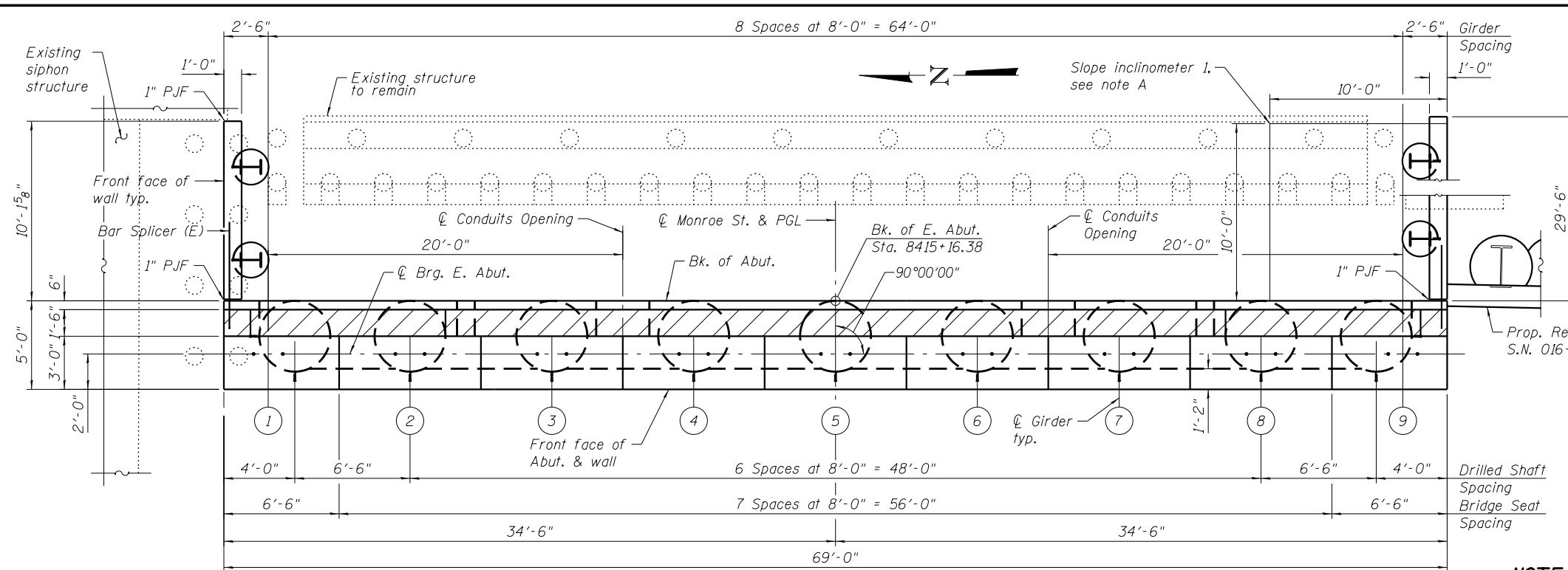
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| USER NAME = lopezgonzalez | DESIGNED - MR | REVISÉD - |
| | CHECKED - AH | REVISÉD - |
| PLOT SCALE = N.T.S. | DRAWN - MR | REVISÉD - |
| PLOT DATE = 9/1/2017 | CHECKED - JIG | REVISÉD - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WEST ABUTMENT ARCHITECTURAL DETAILS
STRUCTURE NO. 016-1700**

SHEET NO. S1-36 OF S1-60 SHEETS

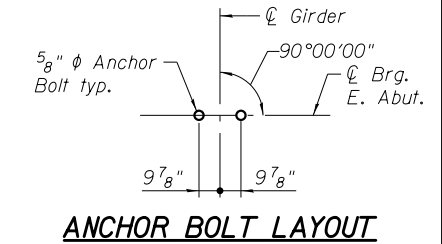
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| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 188 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |



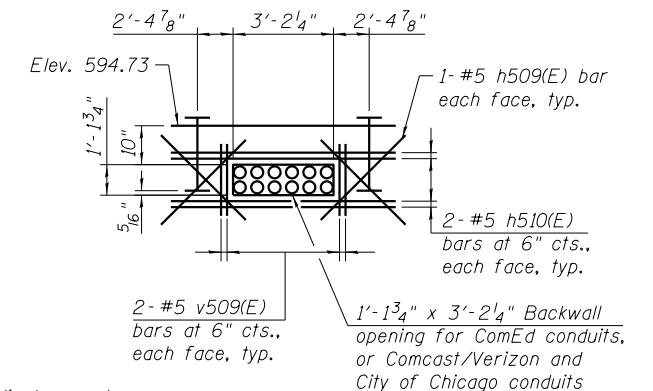
PLAN

TOP OF SEAT ELEVATION

| Girder No. | Seat Elevation |
|------------|----------------|
| 1 | 591.79 |
| 2 | 591.92 |
| 3 | 592.04 |
| 4 | 592.17 |
| 5 | 592.29 |
| 6 | 592.17 |
| 7 | 592.04 |
| 8 | 591.92 |
| 9 | 591.79 |



ANCHOR BOLT LAYOUT

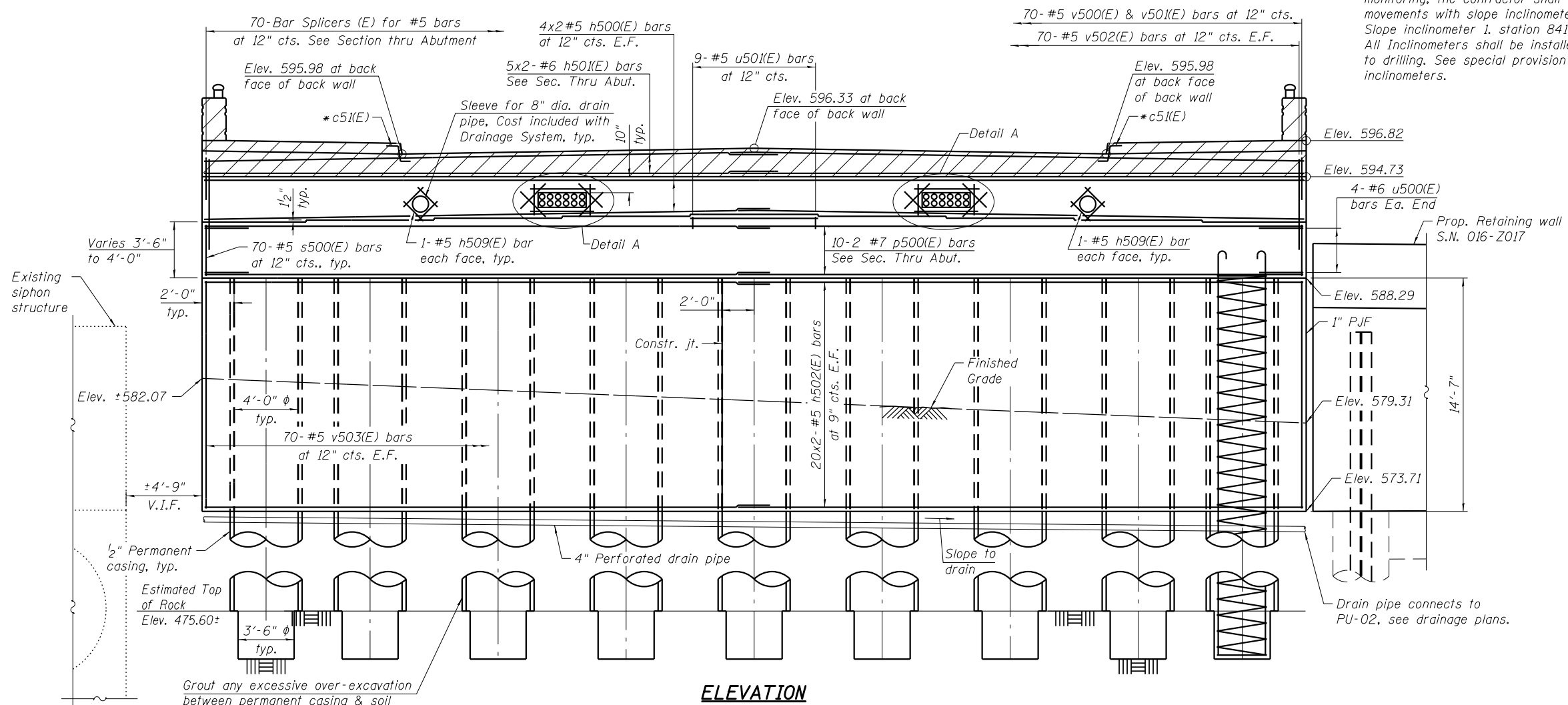


DETAIL A

NOTE A:

In addition to vibration and displacement monitoring, the contractor shall monitor movements with slope inclinometers: Slope inclinometer 1, station 8415+26.38. All inclinometers shall be installed prior to drilling. See special provision for slope inclinometers.

Note:
Conduit provided by Others. Contractor to coordinate with utility owner for location and size of the utility blockouts. Cost of utility blockouts included in Concrete Structures. (see Electrical Plans.)



ELEVATION

MINIMUM BAR LAP

- #4 bar = 2'-11"
- #5 bar = 3'-7"
- #6 bar = 4'-4"
- #7 bar = 5'-0"

* See approach sheet for sidewalk reinforcement

Notes:
Contractor shall adjust pile spacing to miss existing piles as necessary and as allowed by the Engineer. Cut reinforcement in field to miss utility openings. Space reinforcement in cap to miss anchor bolts. Pour steps monolithically with cap. Hatched area to be poured after superstructure false work has been removed. Quantity of concrete included with Concrete Superstructure. See sheet S1-38 of S1-60 for section A-A. See sheet S1-39 & S1-40 of S1-60 for wall details and Bill of Materials. Bars noted thus, 3x2-#5 indicates 3 lines of bars with 2 lengths of bars per line. Concrete Sealer shall be applied to abutment backwall bearing seats and exposed faces of abutment cap. Siphon location at the Northeast corner of abutment shall be verified in the field and approved by the Engineer prior to drilled shaft construction. B.F. denotes back face. F.F. denotes front face. E.F. denotes each face.



| | | |
|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

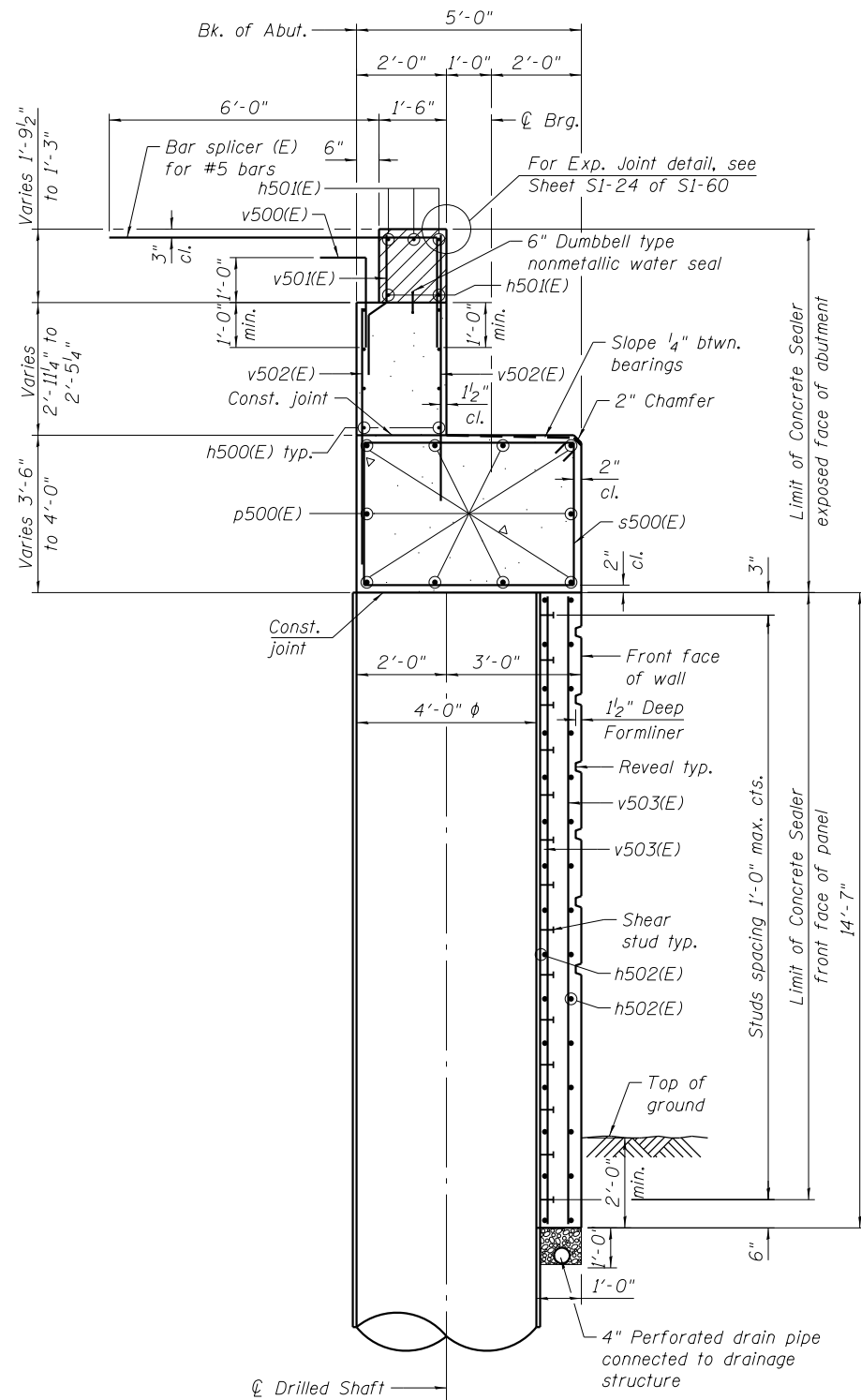
EAST ABUTMENT
STRUCTURE NO. 016-1700

| | | | | |
|--------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 189 |
| CONTRACT NO. 60X95 | | | | |

SHEET NO. S1-37 OF S1-60 SHEETS

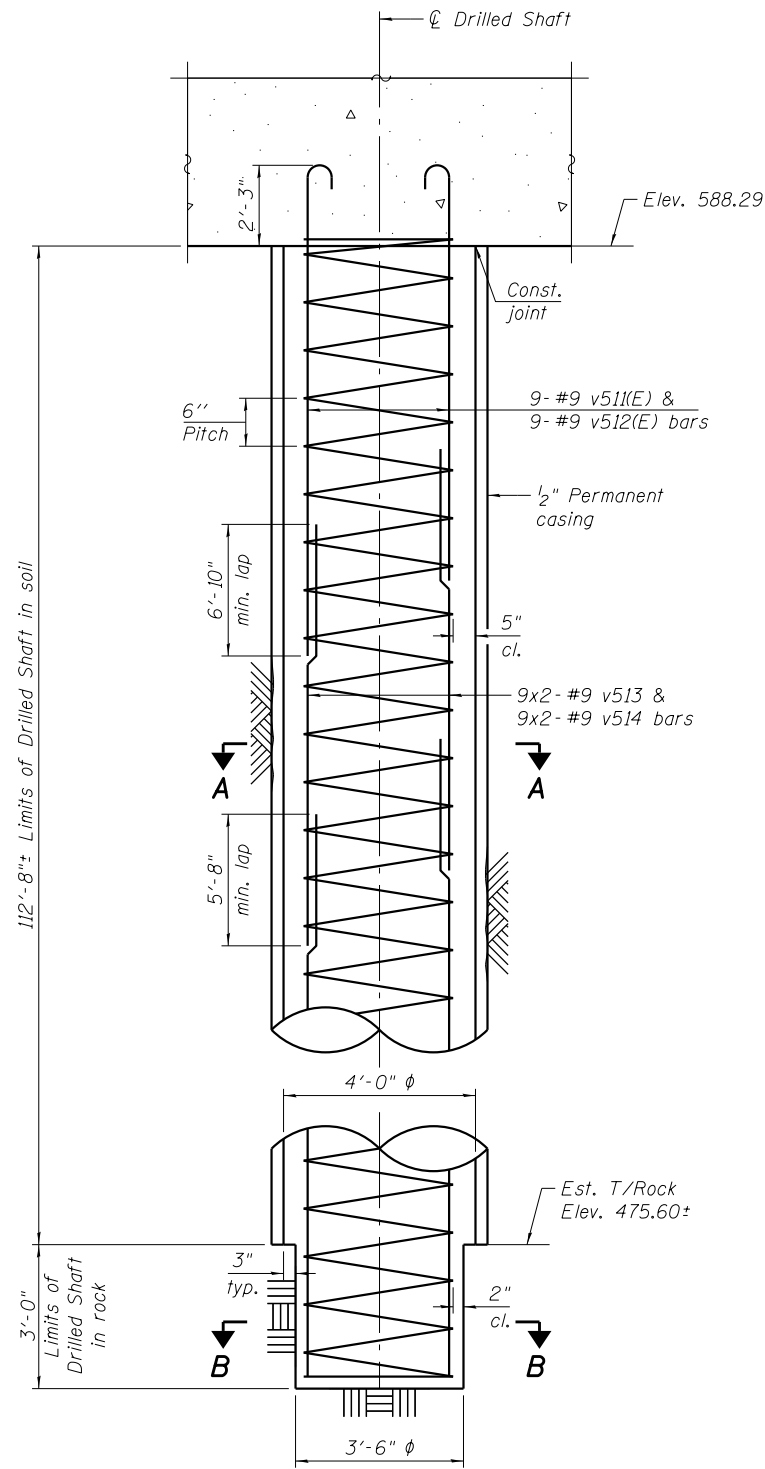
ILLINOIS FED. AID PROJECT

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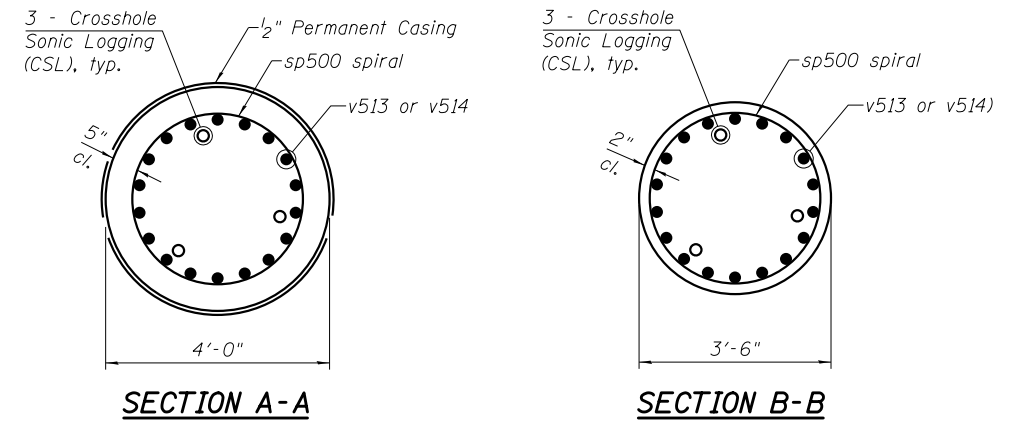


SECTION THRU ABUTMENT

* $\frac{3}{4}$ " x 6" Granular or solid flux filled headed studs conforming to article 1006.32 of the standard specifications automatically end welded to flange, typ.

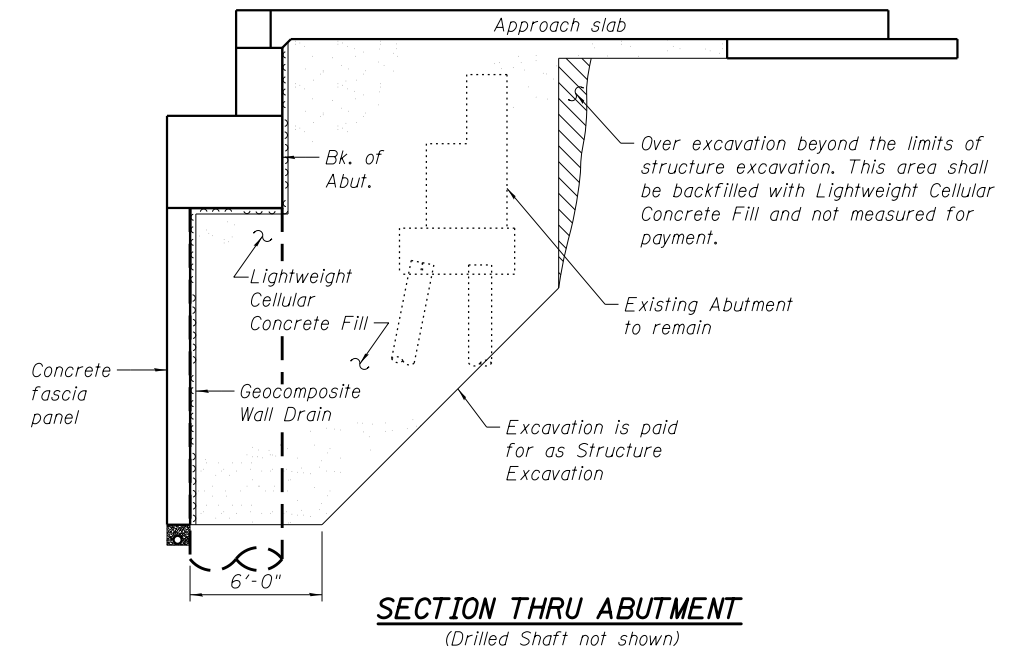


ABUTMENT SHAFT ELEVATION



SECTION A-A

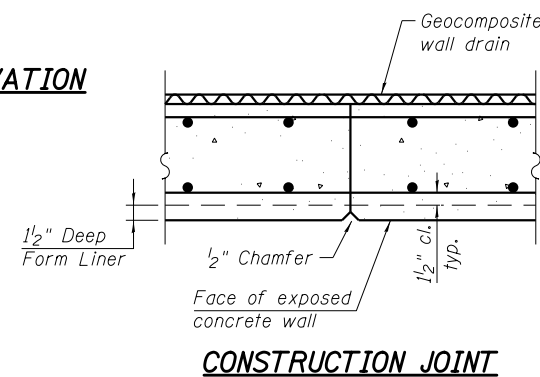
SECTION B-B



SECTION THRU ABUTMENT
(Drilled Shaft not shown)

SUGGESTED CONSTRUCTION SEQUENCE

1. Install T.S.R.S. #3 & #5, and perform structural excavation.
2. Remove part of existing abutment required for T.S.R.S. #2 installation. Cost included in Remove Existing Structures.
3. Install T.S.R.S. #2 and perform structural excavation.
4. Remove Existing Structures indicated on the removal plan.
5. Construct North East and South East Wingwalls, see sheet S1-40 for suggested sequence of soldier pile wall construction.
6. Construct East Abutment.
7. Backfill Lightweight Cellular Concrete Fill and remove T.S.R.S. #2, #3 & #5.



CONSTRUCTION JOINT

Notes:

1. See sheet S1-41 of S1-60 for reveal detail.
2. Bars noted thus, 14X2-#9 indicated 14 lines of bars with 2 lengths of bars per line.
3. Bars equally spaced, unless otherwise noted.
4. #4 sp100 spiral, each drilled shaft.
 1. Provide $\frac{1}{2}$ extra turns, shop welded together per AWS D1.4 top and bottom. Extend spiral 2" into abutment cap. Provide 4-#4 spacers or equivalent.
 2. When splicing spiral reinforcement is necessary, the spiral shall be provided with $\frac{1}{2}$ extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4 or shall both terminate with a 135° standard hook.

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PARSONS BRINCKERHOFF

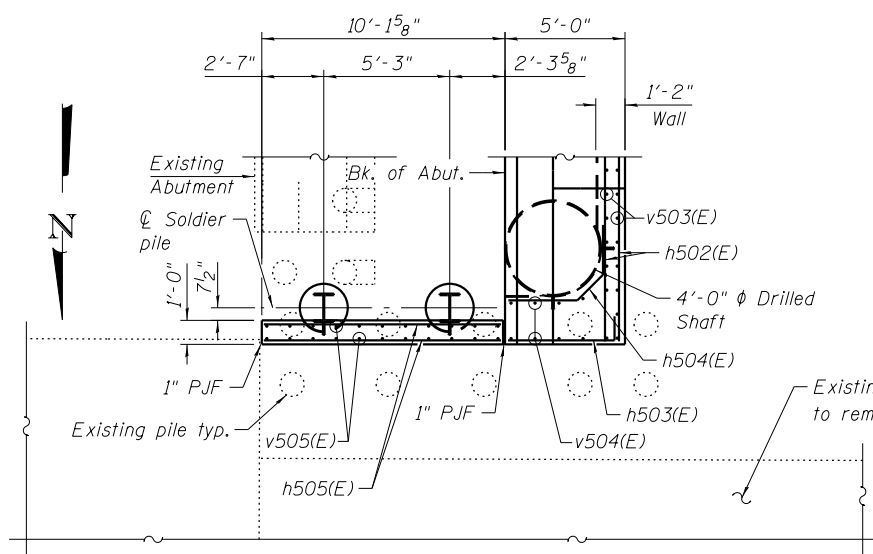
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| USER NAME = | lopezgonzalez | DESIGNED - | JZ | REVISED - | |
| CHECKED - | TWP | REVISIED - | | | |
| PLOT SCALE = | N.T.S. | DRAWN - | DCP | REVISED - | |
| PLOT DATE = | 9/1/2017 | CHECKED - | JIG | REVISED - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

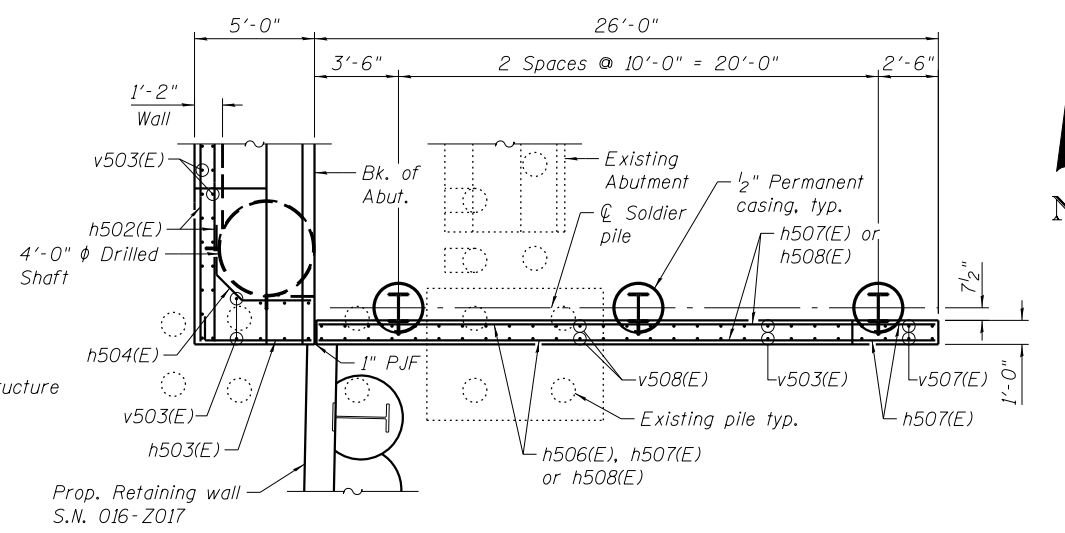
**EAST ABUTMENT DETAILS I
STRUCTURE NO. 016-1700**

SHEET NO. S1-38 OF S1-60 SHEETS

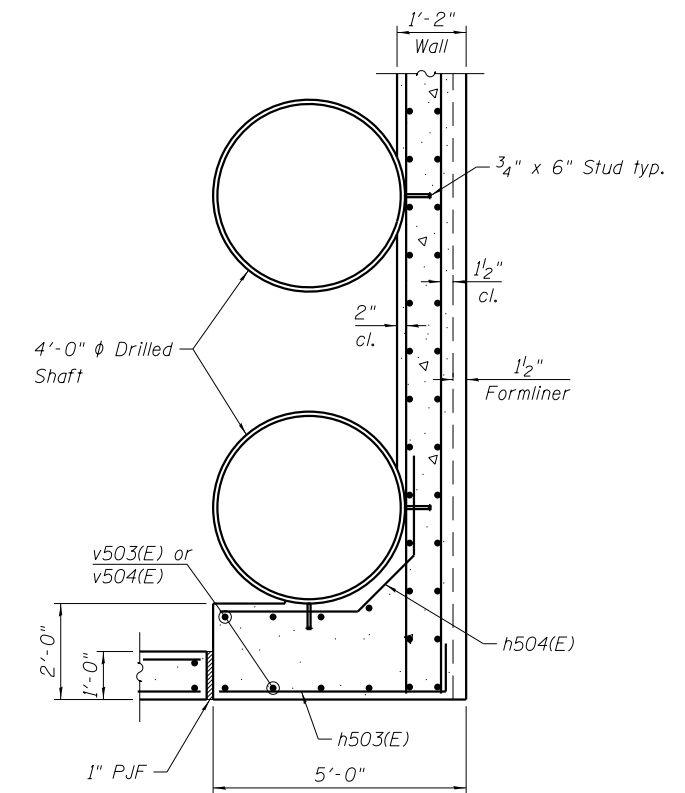
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|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 190 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



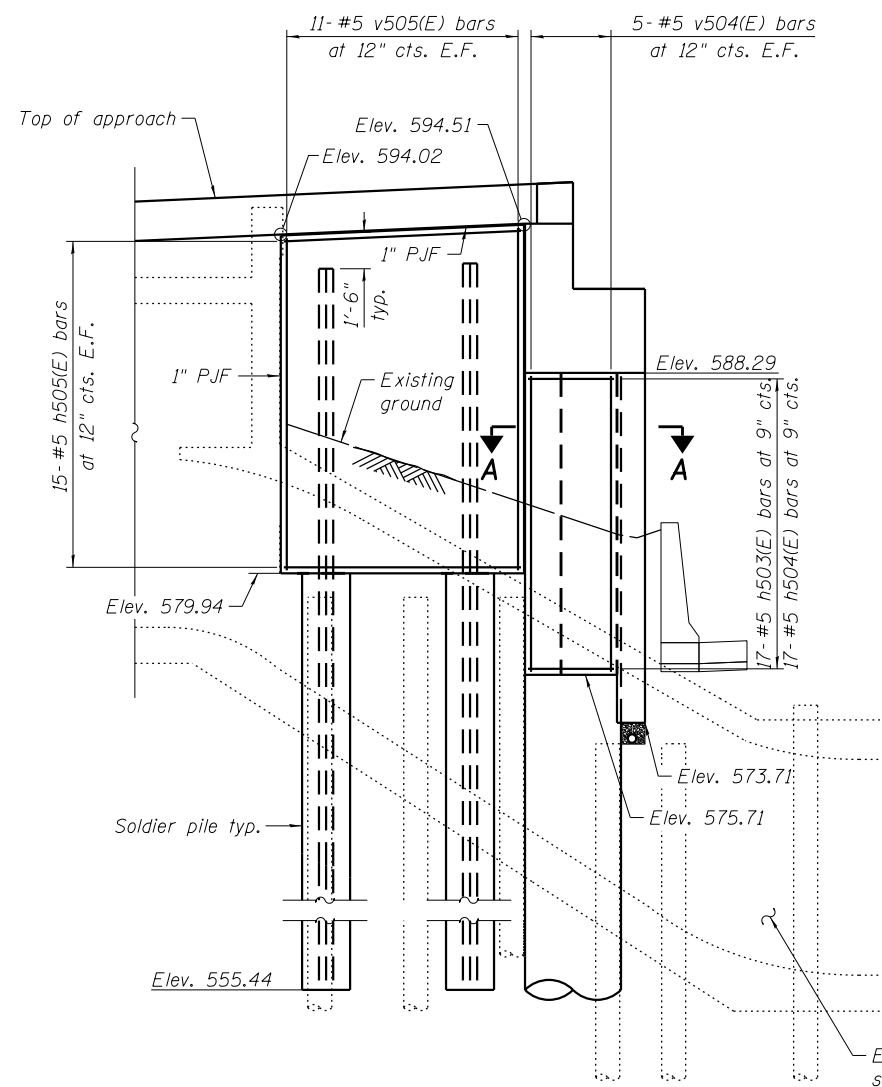
NORTHEAST SOLDIER PILE WALL PLAN



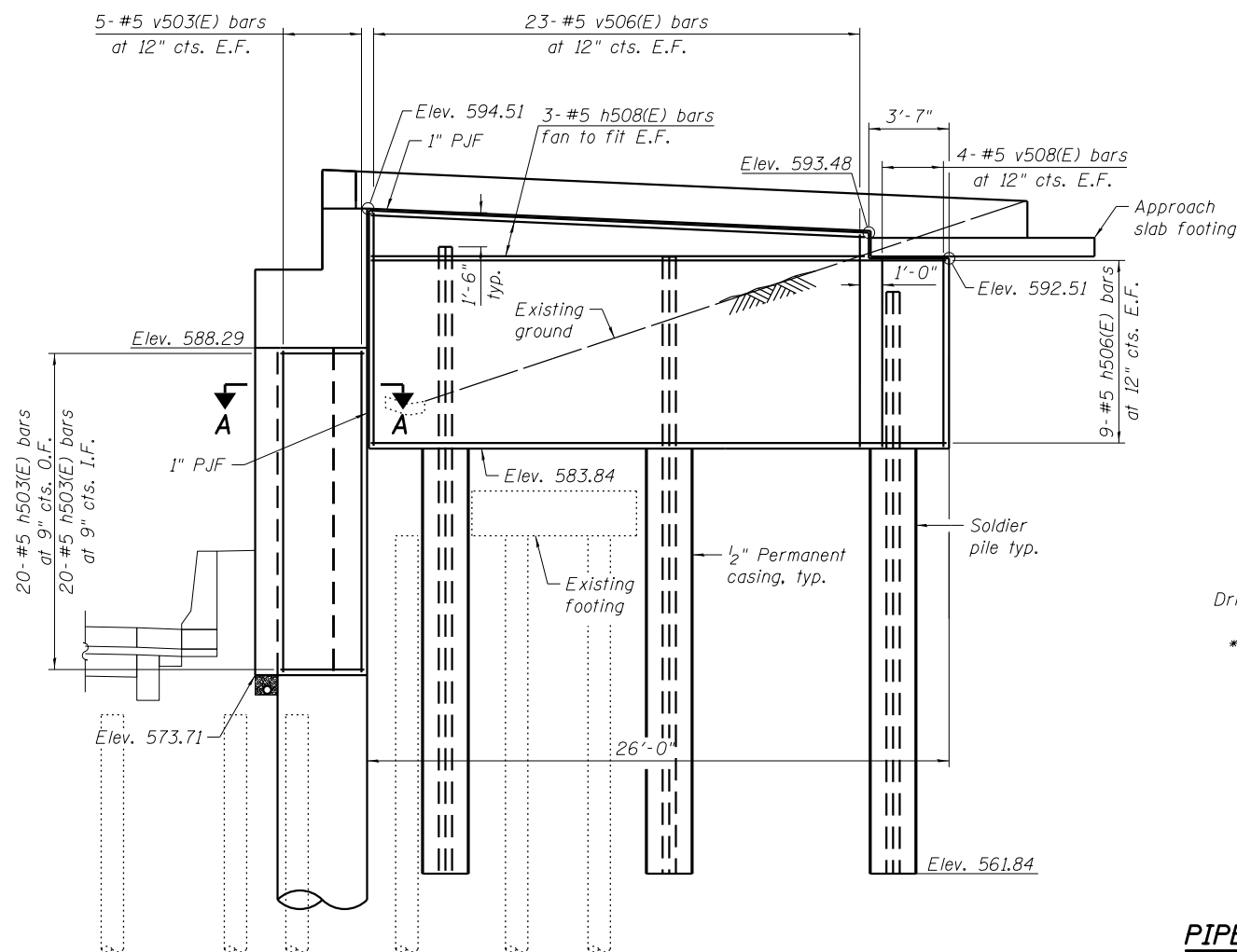
SOUTHEAST SOLDIER PILE WALL PLAN



SECTION A-A
(Northeast corner shown, Southeast corner similar)

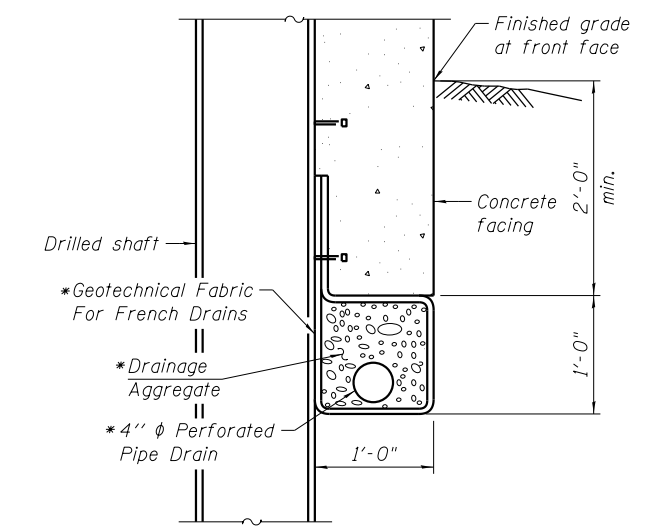


NORTHEAST SOLDIER PILE WALL ELEVATION



SOUTHEAST SOLDIER PILE WALL ELEVATION

Proposed Retaining wall (S.N. 016-Z017) not shown for clarity



PIPE UNDERDRAIN DETAIL THRU ABUTMENT

* Included in the cost of Pipe Underdrains for Structures, 4"

0161700-60X95-5039-DET.dgn

PARSONS BRINCKERHOFF

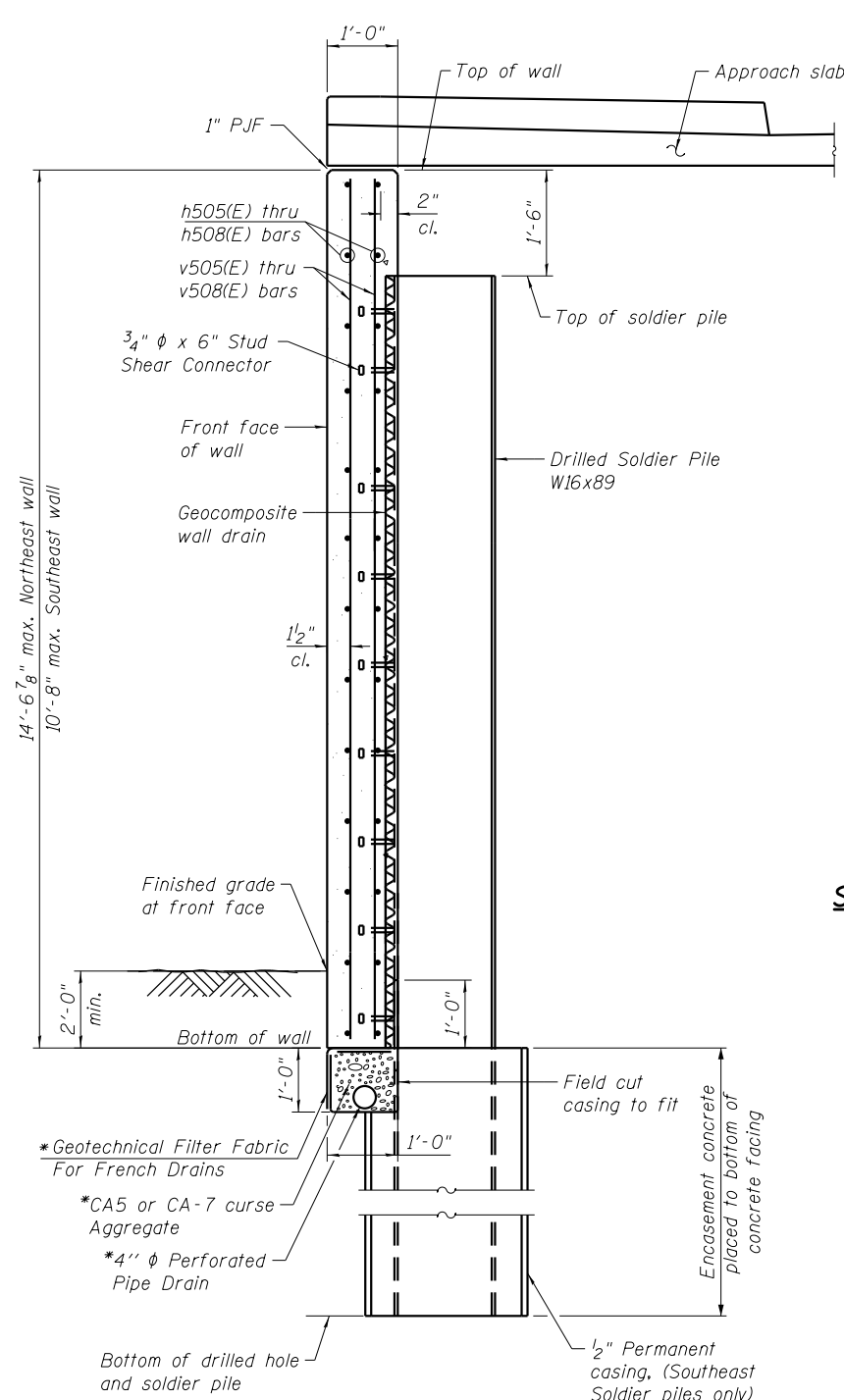
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|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - JZ | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - TWP | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT DETAILS II
STRUCTURE NO. 016-1700**

SHEET NO. S1-39 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 191 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



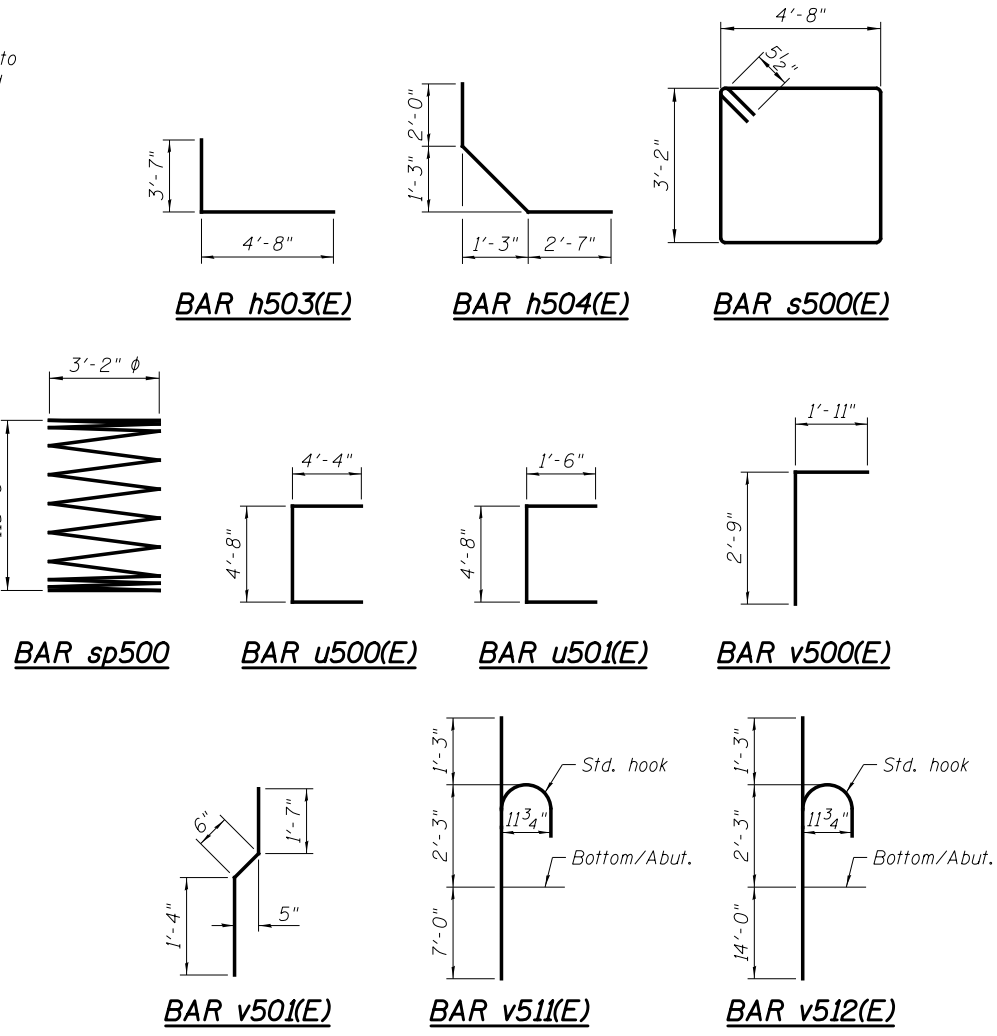
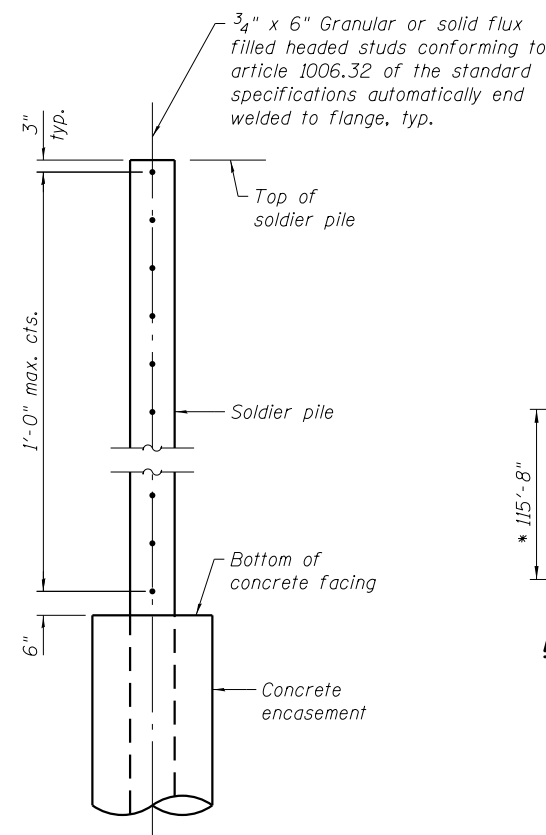
SECTION THRU SOLDIER PILE WALL

* Included in the cost of Pipe Underdrains for Structures.

SUGGESTED SEQUENCE OF SOLDIER PILE WALL CONSTRUCTION

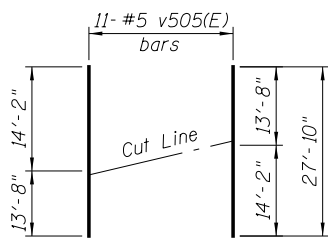
1. Drill hole for soldier pile to the tip elevation indicated on the plans.
2. Remove loose material and excess water from hole and set soldier pile in hole, using temporary bracing to maintain correct elevation, clearances, and position during and after placement of concrete.
3. Place encasement concrete around soldier pile to the level indicated on plans.
4. Place formwork and geocomposite wall drain.
5. Place the 4" ϕ perforated pipe drain, geotechnical filter fabric for french drains, and course aggregate.
6. Install stud shear connectors to soldier piles as shown on plan.
7. Construct wall panels.

SHEAR STUD CONNECTOR LAYOUT



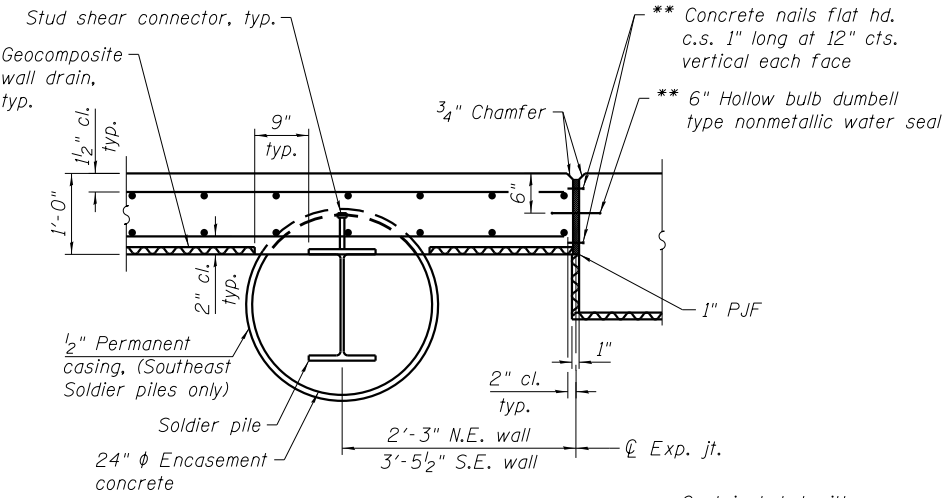
CUTTING DIAGRAM

Order bars full length. Cut as shown. Use the remainder on opposite end.



CUTTING DIAGRAM

Order bars full length. Cut as shown. Use the remainder on opposite end.



EXPANSION JOINT DETAIL

BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|--|-----|---------|---------|-------|
| h500(E) | 8 | #5 | 36'-2" | — |
| h501(E) | 10 | #6 | 36'-6" | — |
| h502(E) | 80 | #5 | 36'-2" | — |
| h503(E) | 37 | #5 | 8'-3" | — |
| h504(E) | 37 | #5 | 6'-4" | — |
| h505(E) | 30 | #5 | 9'-9" | — |
| h506(E) | 18 | #5 | 25'-7" | — |
| h508(E) | 6 | #5 | 22'-0" | — |
| h509(E) | 32 | #5 | 2'-6" | — |
| h510(E) | 16 | #5 | 5'-3" | — |
| p500(E) | 20 | #7 | 36'-10" | — |
| s500(E) | 70 | #5 | 16'-7" | □ |
| sp500 | 9 | #5 | 115'-8" | WWW |
| u500(E) | 8 | #6 | 13'-4" | — |
| u501(E) | 9 | #5 | 7'-8" | — |
| v500(E) | 70 | #5 | 4'-8" | — |
| v501(E) | 70 | #4 | 3'-5" | — |
| v502(E) | 140 | #5 | 5'-11" | — |
| v503(E) | 150 | #5 | 14'-0" | — |
| v504(E) | 10 | #5 | 12'-3" | — |
| v505(E) | 11 | #5 | 27'-10" | — |
| v506(E) | 23 | #5 | 19'-5" | — |
| v508(E) | 8 | #5 | 8'-4" | — |
| v509(E) | 16 | #5 | 3'-2" | — |
| v511(E) | 81 | #9 | 10'-6" | — |
| v512(E) | 81 | #9 | 17'-6" | — |
| v513 | 162 | #9 | 53'-8" | — |
| v514 | 162 | #5 | 57'-2" | — |
| Structure Excavation | | Cu. Yd. | 712 | |
| Concrete Structures | | Cu. Yd. | 120 | |
| Stud Shear Connectors | | Each | 219 | |
| Reinforcement Bars | | Pound | 82,960 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 20,780 | |
| Permanent Casing | | Foot | 1,081 | |
| Drilled Shaft in Soil | | Cu. Yd. | 473 | |
| Drilled Shaft in Rock | | Cu. Yd. | 10 | |
| Furnishing Soldier Piles (W Section) | | Foot | 168 | |
| Drilling and Setting Soldier Piles (In Soil) | | Cu. Ft. | 359 | |
| Concrete Sealer | | Sq. Ft. | 2,074 | |
| Geocomposite Wall Drain | | Sq. Yd. | 207 | |
| Lightweight Cellular Concrete Fill | | Cu. Yd. | 564 | |
| Pipe Underdrains for Structures 4" | | Foot | 109 | |
| Slope Inclinator | | Each | 1 | |
| Crosshole Sonic Logging Access Ducts | | Foot | 1,041 | |
| Crosshole Sonic Logging Testing | | Each | 2 | |

* Length is height of spiral

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PARSONS BRINCKERHOFF

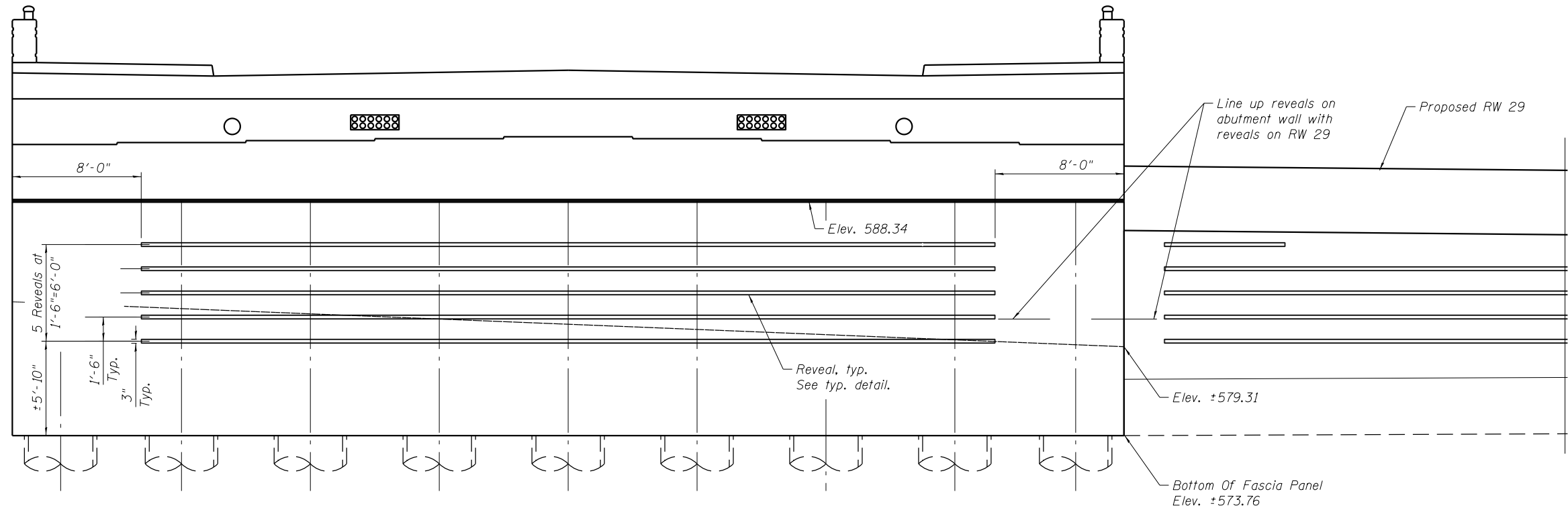
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| USER NAME = | lopezgonzalez | DESIGNED - | JZ | REVISED - | |
| CHECKED - | TWP | REVISOR - | | REVISOR - | |
| PLOT SCALE = | N.T.S. | DRAWN - | DCP | REVISOR - | |
| PLOT DATE = | 9/1/2017 | CHECKED - | JIG | REVISOR - | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

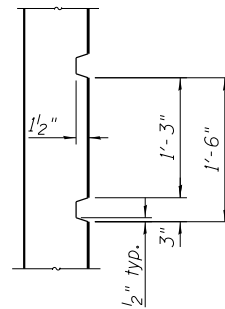
EAST ABUTMENT DETAILS III
STRUCTURE NO. 016-1700

SHEET NO. S1-40 OF S1-60 SHEETS

| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 192 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



ELEVATION



TYPICAL REVEAL DETAIL

NOTES:

- The 3" x 1/2" reveal in the fascia panel will not be paid separately and shall be included in the cost of the pay item Concrete Structures.

BILL OF MATERIAL

| Item | Unit | Total |
|---------------|---------|-------|
| Rubbed Finish | Sq. Ft. | 800 |

0161700-60X95-5041-ABA.dgn

**PARSONS
BRINCKERHOFF**

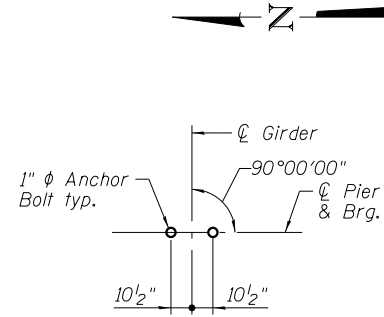
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| PLOT DATE = 9/1/2017 | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

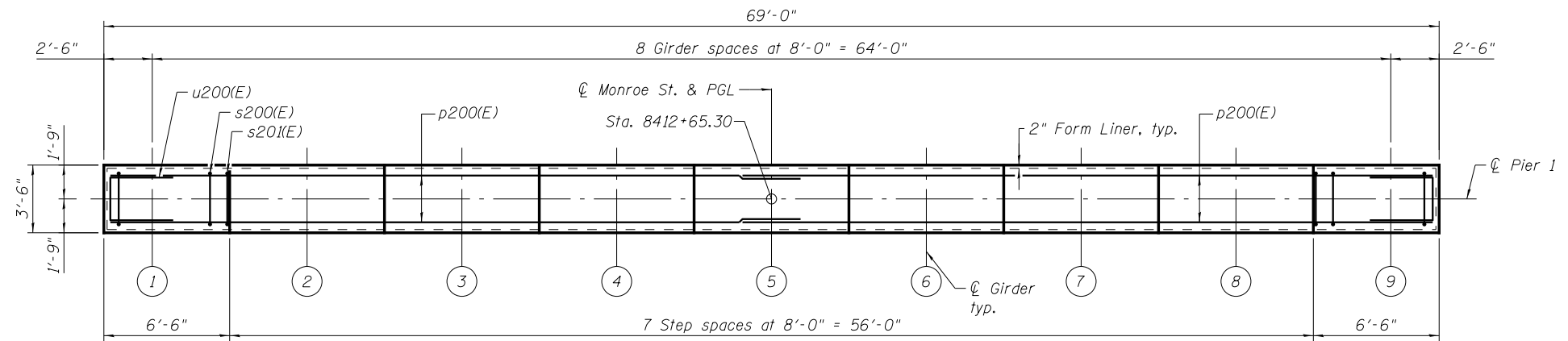
**EAST ABUTMENT ARCHITECTURAL DETAILS
STRUCTURE NO. 016-1700**

SHEET NO. S1-41 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 193 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 60X95 | |



ANCHOR BOLT LAYOUT

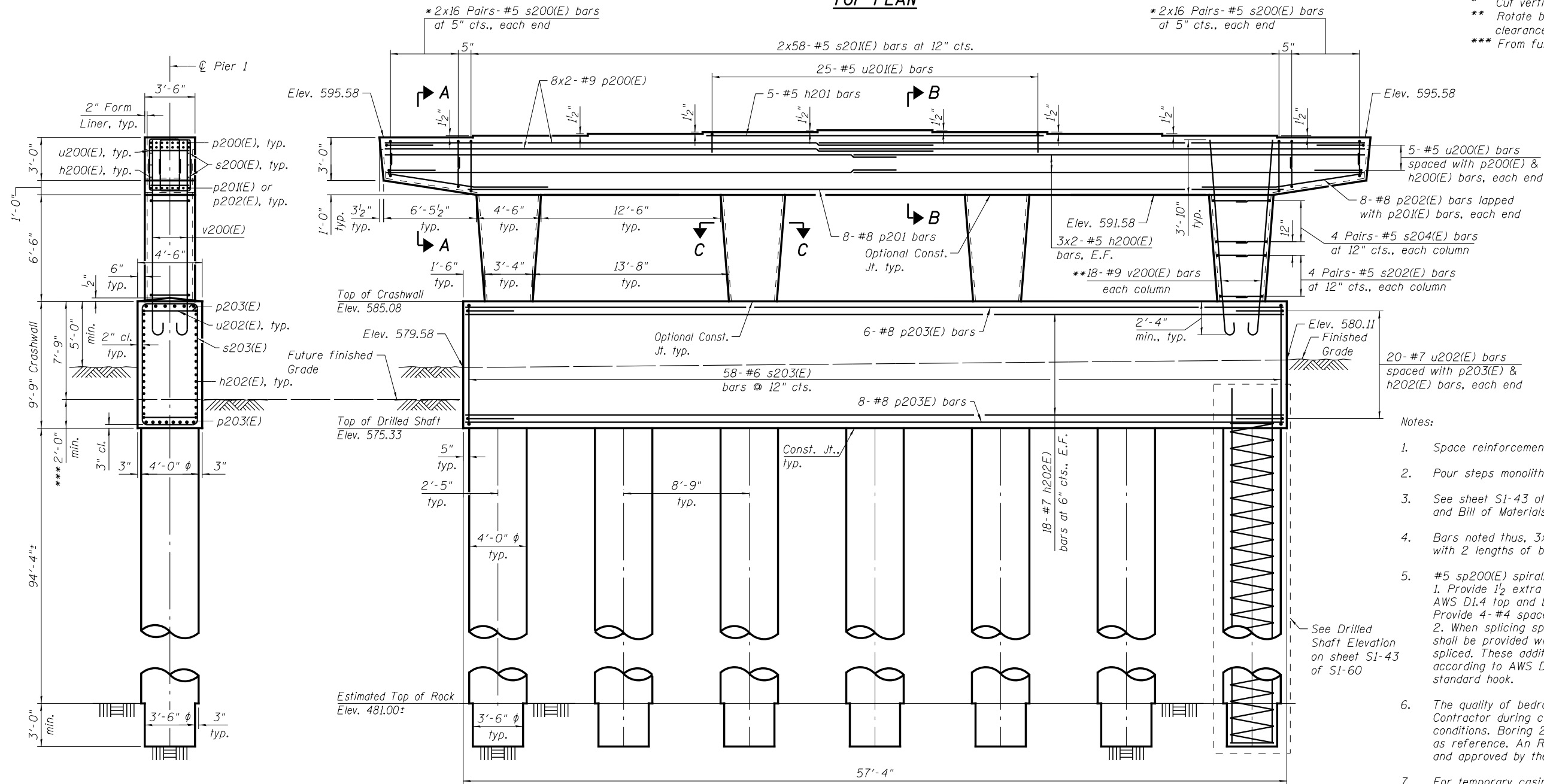


TOP PLAN

TOP OF SEAT ELEVATION

| Girder No. | Seat Elevation |
|------------|----------------|
| 1 | 595.58 |
| 2 | 595.71 |
| 3 | 595.83 |
| 4 | 595.96 |
| 5 | 596.08 |
| 6 | 595.96 |
| 7 | 595.83 |
| 8 | 595.71 |
| 9 | 595.58 |

- * Cut vertical legs of bars to fit. Min. lap is 2'-7".
- ** Rotate bars to maintain minimum clearance.
- *** From future finished grade



ELEVATION
(Looking East)

MINIMUM BAR LAP

- #5 bar = 3'-9"
- #9 bar = 10'-4"
- #10 bar = 6'-4" (Drilled Shaft)

- Notes:
- Space reinforcement in cap to miss anchor bolts.
 - Pour steps monolithically with cap.
 - See sheet S1-43 of S1-60 for section A-A, B-B, C-C, and Bill of Materials.
 - Bars noted thus, 3x2-#5 indicates 3 lines of bars with 2 lengths of bars per line.
 - #5 sp200(E) spiral, each drilled shaft
 - Provide 1/2 extra turns, shop welded together per AWS D1.4 top and bottom. Extend spiral 2" into crashwall. Provide 4-#4 spacers or equivalent.
 - When splicing spiral reinforcement is necessary, the spiral shall be provided with 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4 or shall both terminate with a 135° standard hook.
 - The quality of bedrock at Pier 1 shall be checked by the Contractor during construction to verify the design bedrock conditions. Boring 2054-B-05 information should be used as reference. An RQD of 75% or more should be verified and approved by the Engineer.
 - For temporary casing, see note 13 on sheet S1-02 of S1-60.

END VIEW

Drilled shaft reinforcement not shown for clarity

0161700-60X95-S042-PR1.dgn

PARSONS BRINCKERHOFF

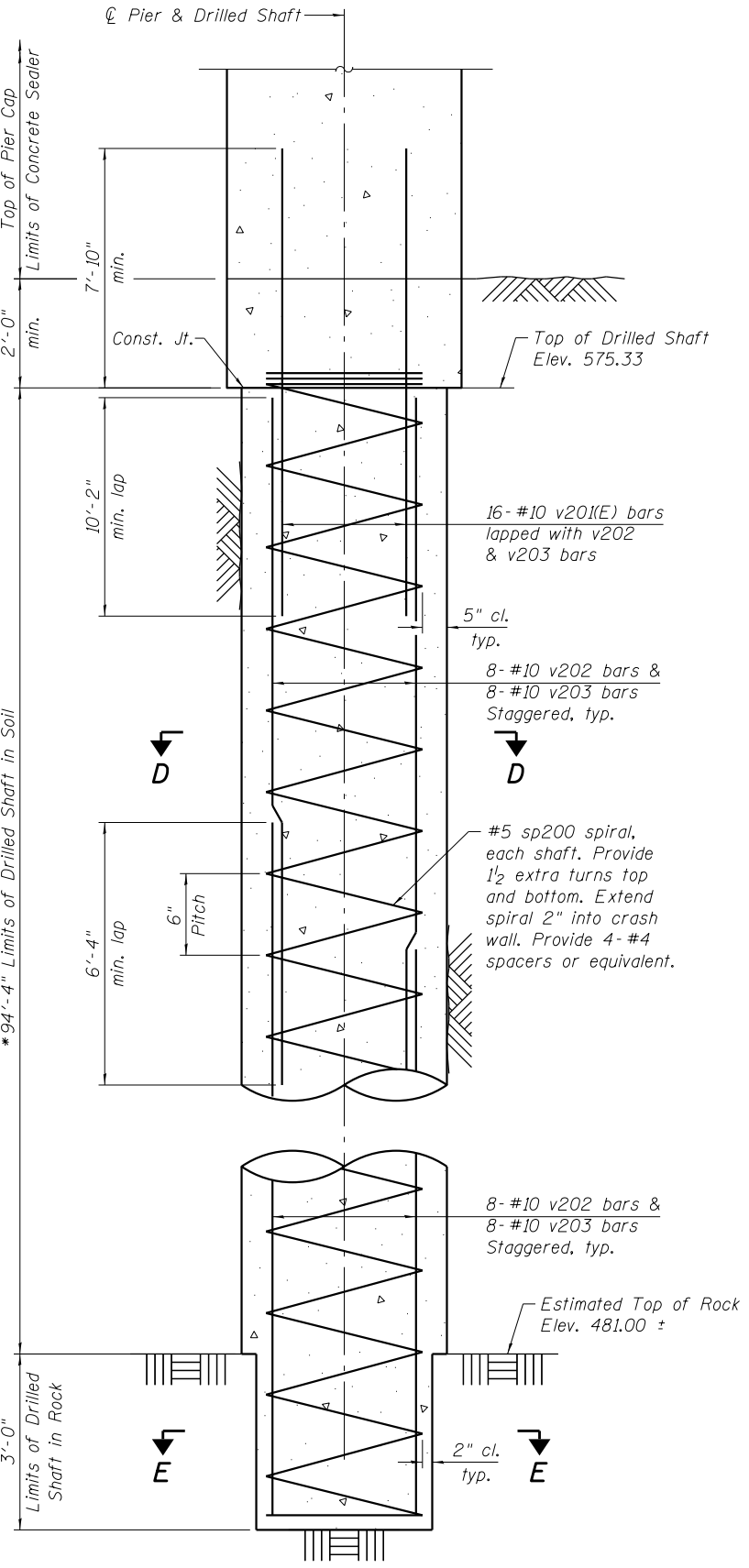
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| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

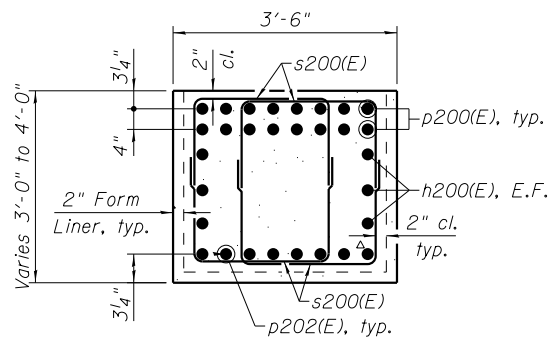
**PIER 1
STRUCTURE NO. 016-1700**

SHEET NO. S1-42 OF S1-60 SHEETS

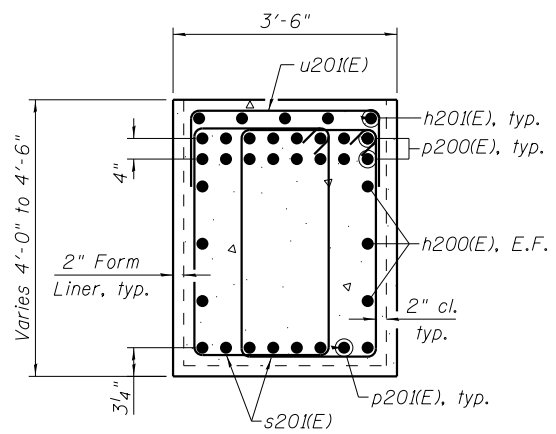
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|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 194 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



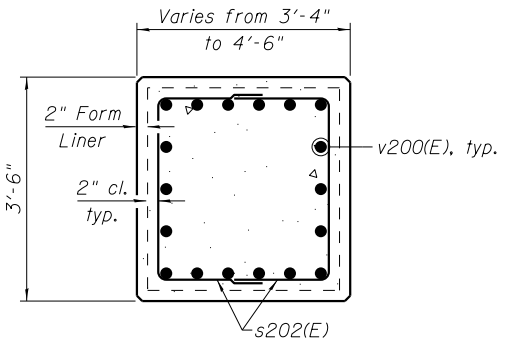
DRILLED SHAFT ELEVATION



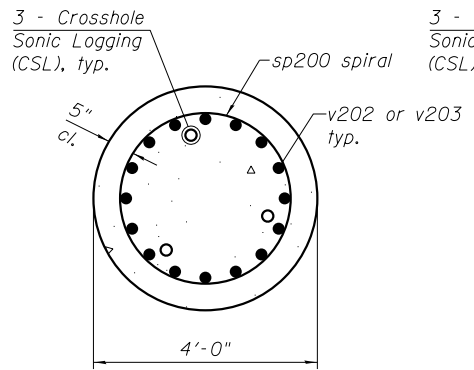
SECTION A-A



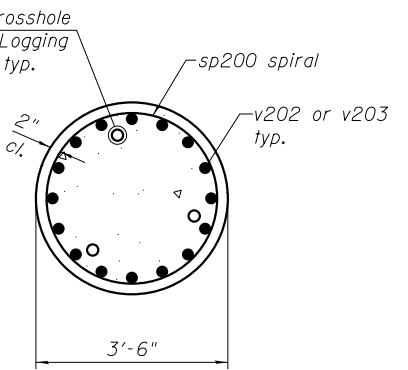
SECTION B-B



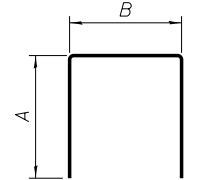
SECTION C-C



SECTION D-D

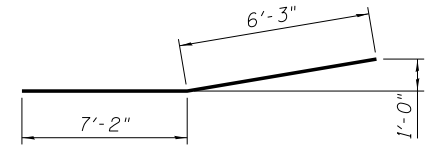


SECTION E-E

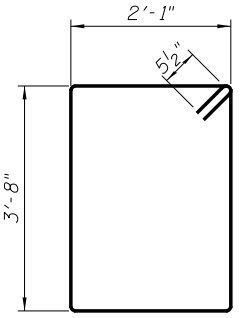


BARS s200(E), s202(E), s204(E), u200(E), u201(E) & u202(E)

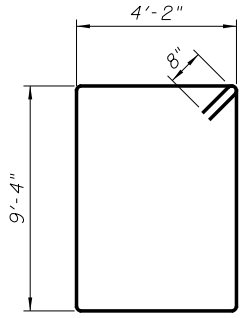
| Bar | A | B |
|---------|-------|--------|
| s200(E) | 3'-3" | 2'-1" |
| s202(E) | 2'-8" | 2'-10" |
| s204(E) | 3'-2" | 2'-10" |
| u200(E) | 3'-9" | 2'-8" |
| u201(E) | 1'-6" | 2'-10" |
| u202(E) | 4'-2" | 4'-0" |



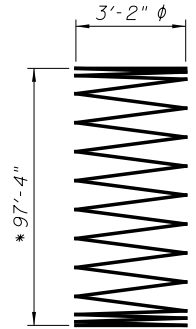
BAR p202(E)



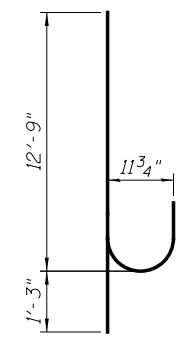
BAR s201(E)



BAR s203(E)



BAR sp200



BAR v200(E)

BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|--------------------------------------|-----|---------|--------|-------|
| h200(E) | 12 | #5 | 36'-3" | — |
| h201(E) | 5 | #5 | 23'-8" | — |
| h202(E) | 36 | #7 | 57'-0" | — |
| p200(E) | 32 | #9 | 39'-6" | — |
| p201(E) | 8 | #8 | 55'-2" | — |
| p202(E) | 16 | #8 | 13'-5" | — |
| p203(E) | 14 | #8 | 57'-0" | — |
| s200(E) | 128 | #5 | 8'-7" | □ |
| s201(E) | 116 | #5 | 12'-5" | □ |
| s202(E) | 32 | #5 | 8'-2" | □ |
| s203(E) | 58 | #6 | 28'-4" | □ |
| s204(E) | 32 | #5 | 9'-2" | □ |
| sp200 | 7 | #5 | 97'-4" | W |
| u200(E) | 10 | #5 | 10'-2" | □ |
| u201(E) | 25 | #5 | 5'-10" | □ |
| u202(E) | 40 | #7 | 12'-4" | □ |
| v200(E) | 72 | #9 | 14'-0" | U |
| v201(E) | 112 | #10 | 18'-2" | — |
| v202 | 112 | #10 | 60'-0" | — |
| v203 | 112 | #10 | 43'-8" | — |
| Structure Excavation | | Cu. Yd. | 92 | |
| Concrete Structures | | Cu. Yd. | 142 | |
| Reinforcement Bars | | Pound | 64,340 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 32,110 | |
| Drilled Shaft in Soil | | Cu. Yd. | 308 | |
| Drilled Shaft in Rock | | Cu. Yd. | 7.5 | |
| Concrete Sealer | | Sq. Ft. | 2,806 | |
| Crosshole Sonic Logging Access Ducts | | Foot | 681 | |
| Crosshole Sonic Logging Testing | | Each | 2 | |

Notes:
 Apply concrete sealer to all exposed concrete surfaces of the pier.
 * The quantities and reinforcement detailing are based on the top of shaft and the estimated top of rock elevations shown and may change based on the actual top of rock encountered at each shaft and the final top of shaft elevation.
 ** Length is height of spiral.
 When splicing spiral reinforcement is necessary, the spirals shall be provided with 1/2" extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4, or shall both terminate in 130° standard hook.
 See sheet S1-42 for location of sections A-A, B-B and C-C.

0161700-60X95-5043-DET.dgn

PARSONS BRINCKERHOFF

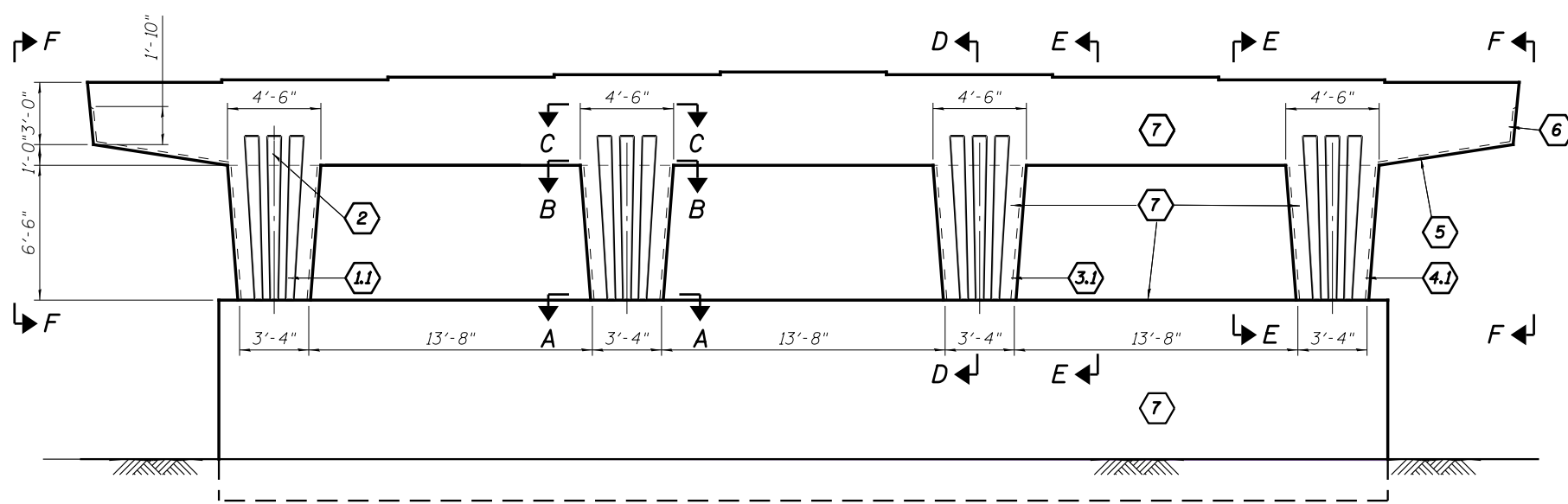
| | | | | | |
|--------------|---------------|------------|-----|-----------|--|
| USER NAME = | lopezgonzalez | DESIGNED - | MS | REVISED - | |
| CHECKED - | CB | REVISOR - | | REVISOR - | |
| PLOT SCALE = | N.T.S. | DRAWN - | DCP | REVISOR - | |
| PLOT DATE = | 9/1/2017 | CHECKED - | JIG | REVISOR - | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

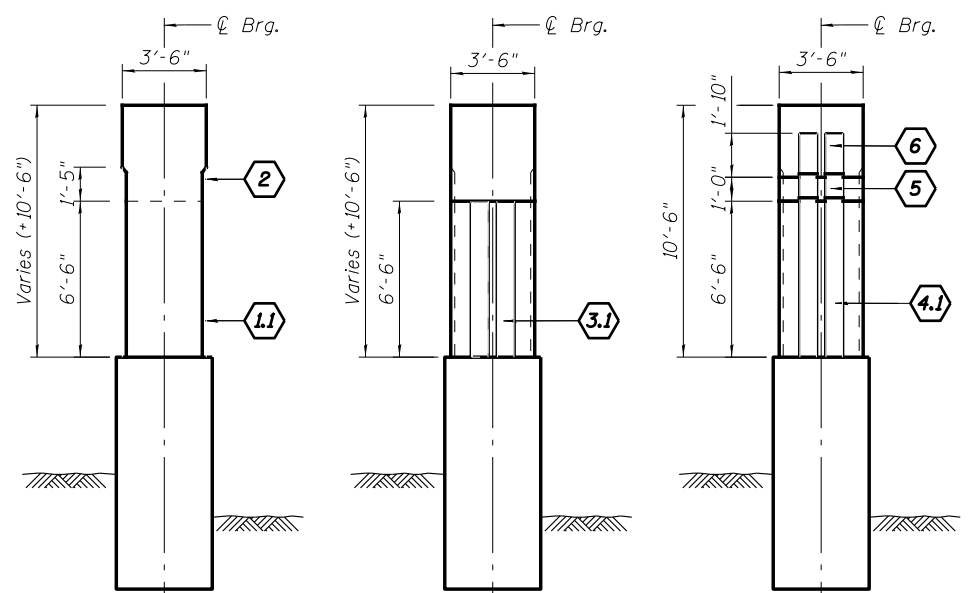
**PIER 1 DETAILS
STRUCTURE NO. 016-1700**

SHEET NO. S1-43 OF S1-60 SHEETS

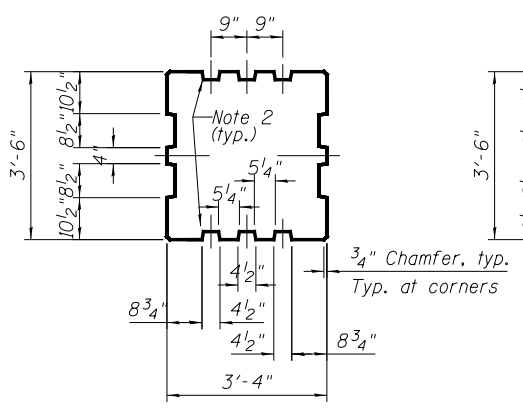
| | | | | |
|---------------------------|-------------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 195 |
| | | | CONTRACT NO. 60X95 | |
| ILLINOIS FED. AID PROJECT | | | | |



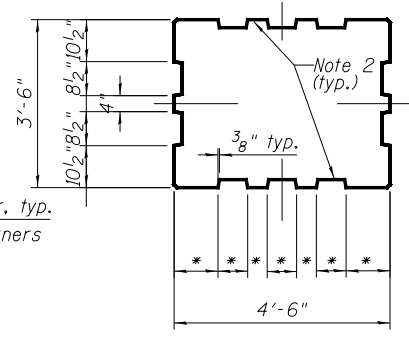
ELEVATION
(Looking East)
(Looking West - Similar)



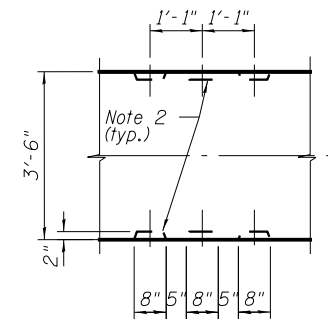
SECTION D-D **SECTION E-E** **PIER END VIEW F**
(Looking South)
(Looking North)



SECTION A
At Bottom of Column

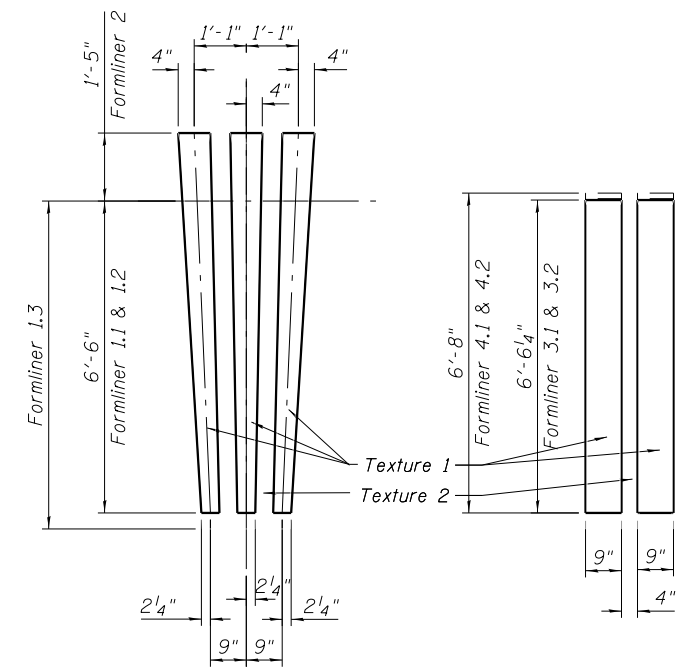


SECTION B
At Top of Column

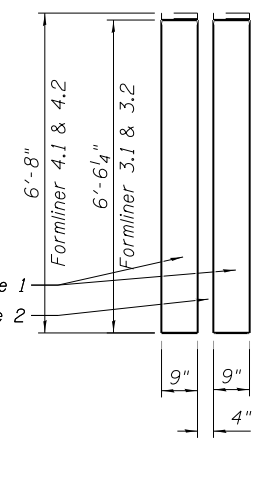


SECTION C
In Cap

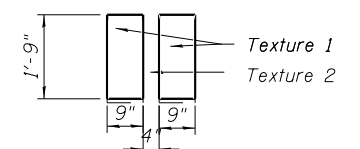
* Dimensions of formliner at top of column (panel 1) to match dimensions of formliner at bottom of pier cap (panel 2).



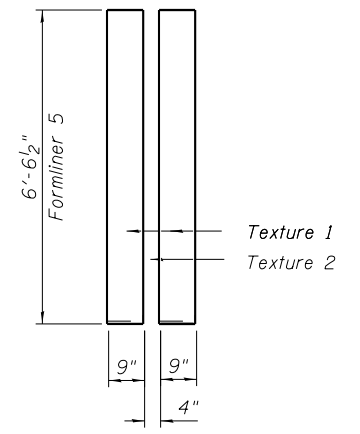
FORM LINER
PANEL 1.1, 1.2 & 2



FORM LINER
PANEL 3.1, 3.2, 4.1 & 4.2



FORM LINER
PANEL 6

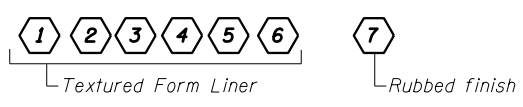


FORM LINER
PANEL 5

NOTES:

- Surface indicated as 7 represents all surface except formliner and shall have smooth rubbed finish. Cost included with pay item Rubbed Finish.
- Tapered fluting - dimensions vary, see elevation profile.
- Form liner panel 2 is continuation of panel 1. Keep adjacent form liners aligned.
- Hand clean and smooth the surface of the construction joint between the pier and cap.
- Texture 1: Light Sandblast, 1/16" depth, as selected from samples provided by the Contractor. Texture 2: Smooth
- Form liner panel 1.3, at Pier 3 is similar to form liner panel 1.1 and 1.2, only longer.
- The three flutes at Form liner panel 1.1, 1.2 and 1.3 have same geometry but are different at the top and bottom.
- Formliner panel 4, 5, and 6 connect to each other. Provide miter edges as required, in order to provide continuous pattern.

LEGEND



BILL OF MATERIAL

| Item | Unit | Total |
|-----------------------------|---------|-------|
| Form Liner Textured Surface | Sq. Ft. | 273 |
| Rubbed Finish | Sq. Ft. | 2043 |

0161700-60X95-S044-PA1.dgn

PARSONS
BRINCKERHOFF

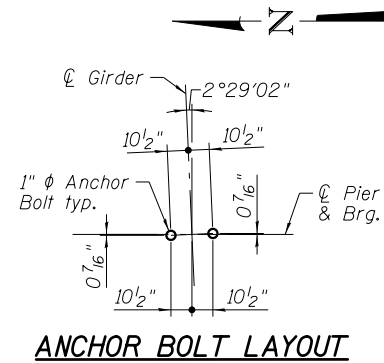
| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MR | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - AH | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - MR | REVISED - |
| | CHECKED - JIG | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

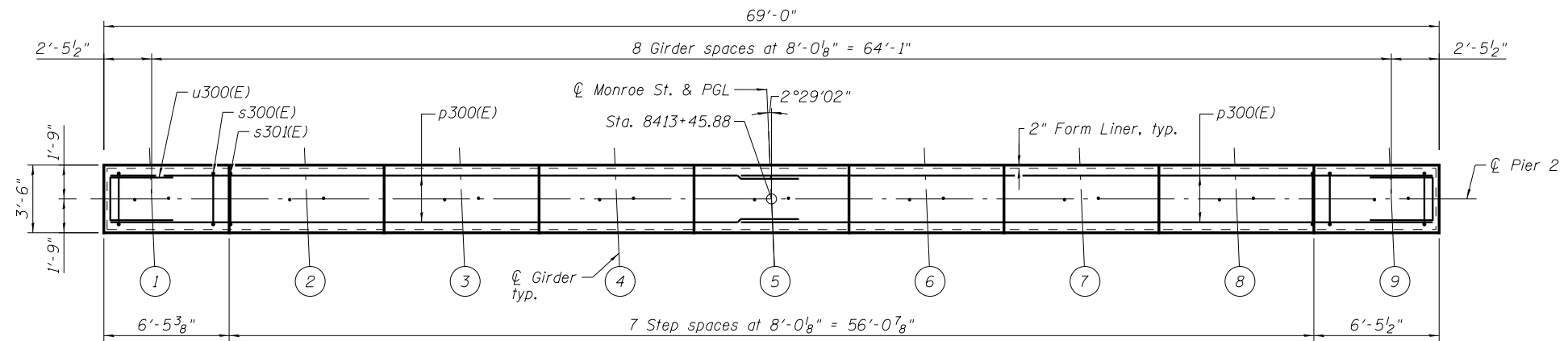
PIER 1 ARCHITECTURAL DETAILS
STRUCTURE NO. 016-1700

SHEET NO. S1-44 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 196 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



ANCHOR BOLT LAYOUT

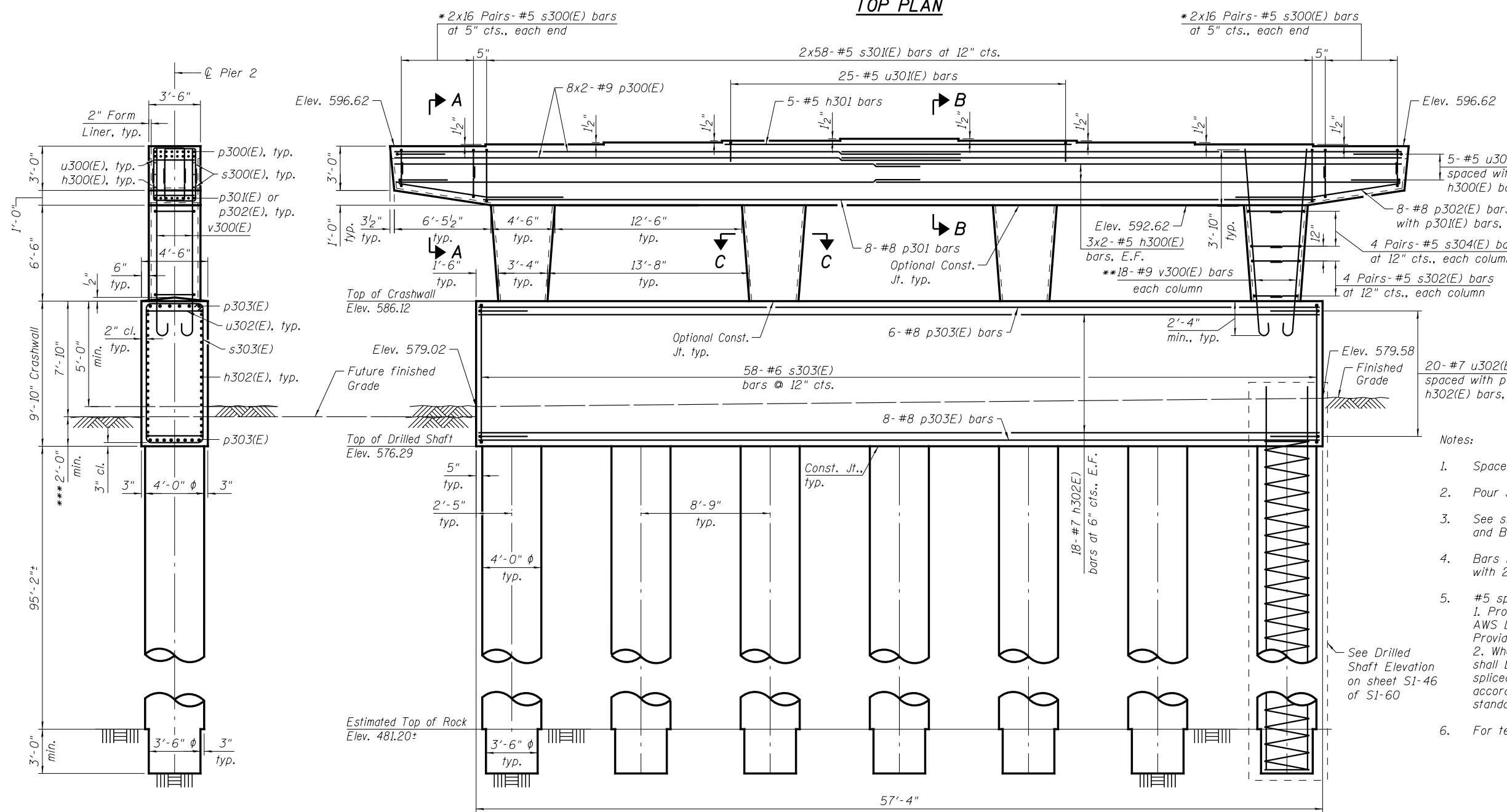


TOP PLAN

TOP OF SEAT ELEVATION

| Girder No. | Seat Elevation |
|------------|----------------|
| 1 | 596.62 |
| 2 | 596.74 |
| 3 | 596.87 |
| 4 | 596.99 |
| 5 | 597.12 |
| 6 | 596.99 |
| 7 | 596.87 |
| 8 | 596.75 |
| 9 | 596.62 |

- * Cut vertical legs of bars to fit. Min. lap is 2'-7".
- ** Rotate bars to maintain minimum clearance.
- *** From future finished grade



ELEVATION
(Looking East)

MINIMUM BAR LAP

- #5 bar = 3'-9"
- #9 bar = 10'-4"
- #10 bar = 6'-4" (Drilled Shaft)

- Notes:
- Space reinforcement in cap to miss anchor bolts.
 - Pour steps monolithically with cap.
 - See sheet S1-46 of S1-60 for section A-A, B-B, C-C, and Bill of Materials.
 - Bars noted thus, 3x2-#5 indicates 3 lines of bars with 2 lengths of bars per line.
 - #5 sp300(E) spiral, each drilled shaft
 - Provide 1/2 extra turns, shop welded together per AWS D1.4 top and bottom. Extend spiral 2" into crashwall. Provide 4-#4 spacers or equivalent.
 - When splicing spiral reinforcement is necessary, the spiral shall be provided with 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4 or shall both terminate with a 135° standard hook.
 - For temporary casing, see note 13 on sheet S1-02 of S1-60.

END VIEW

Drilled shaft reinforcement not shown for clarity

0161700-60X95-5045-PR2.dgn

PARSONS BRINCKERHOFF

USER NAME = pateld
 PLOT SCALE = N.T.S.
 PLOT DATE = 6/29/2017

DESIGNED - MS
 CHECKED - CB
 DRAWN - DCP
 CHECKED - JIG

REVISED -
 REVISED -
 REVISED -
 REVISED -

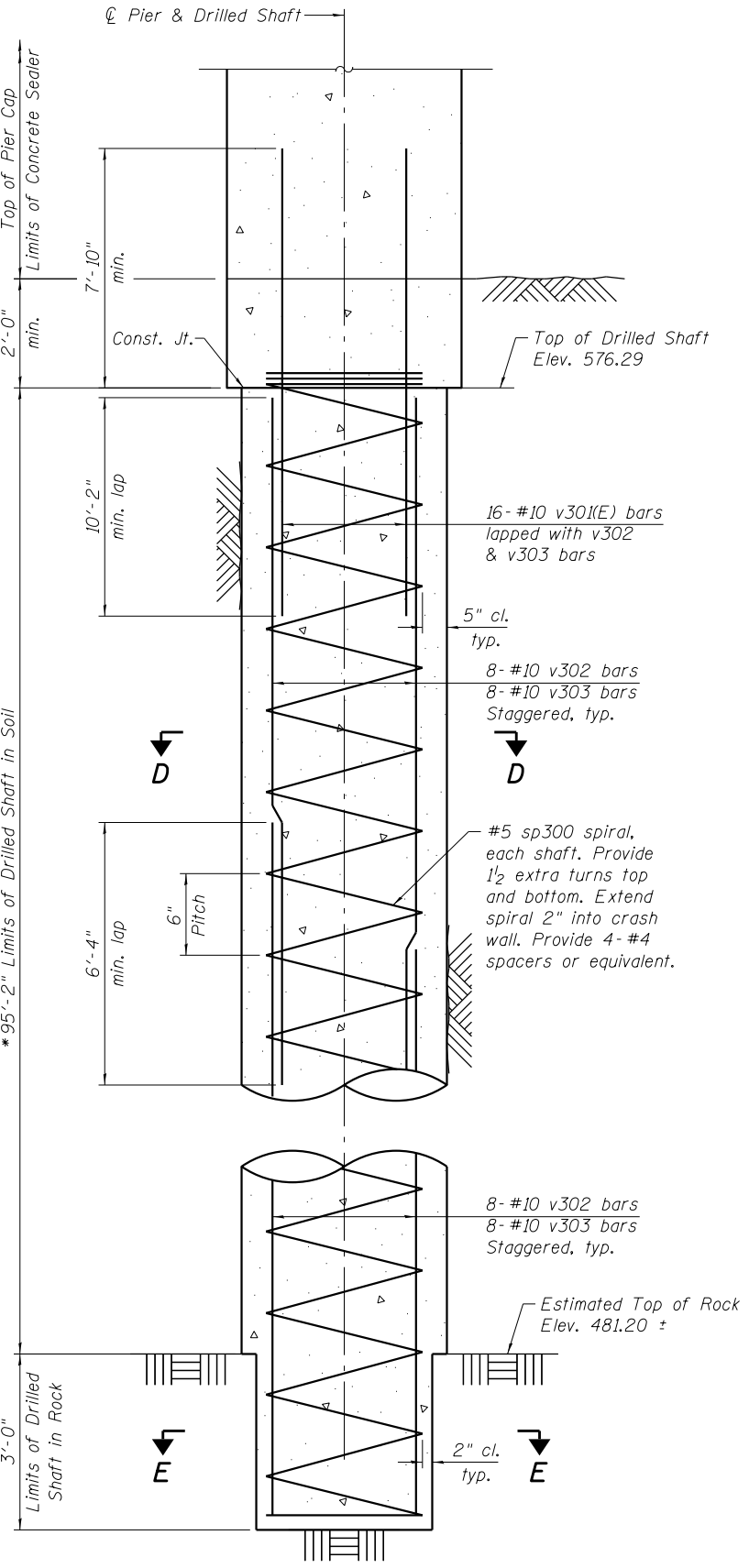
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2
STRUCTURE NO. 016-1700

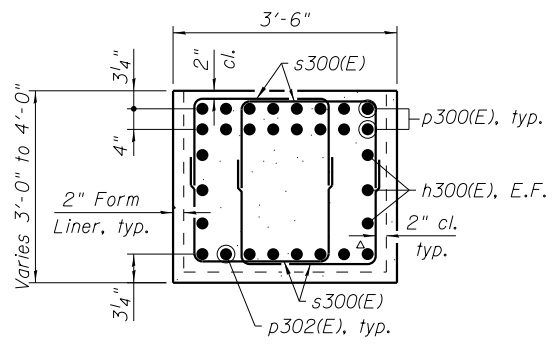
SHEET NO. S1-45 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 197 |
| CONTRACT NO. 60X95 | | | | |

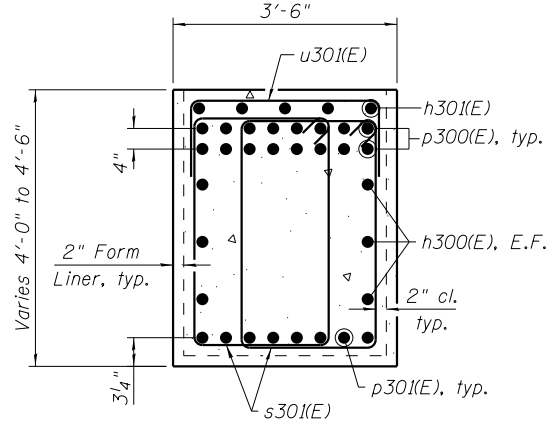
ILLINOIS FED. AID PROJECT



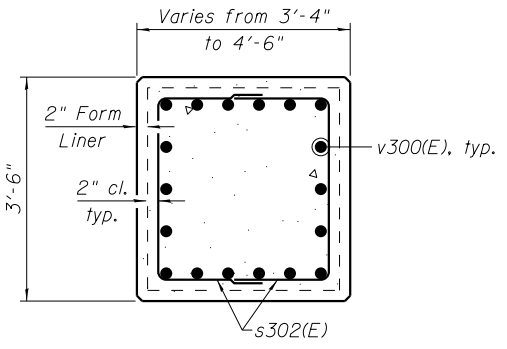
DRILLED SHAFT ELEVATION



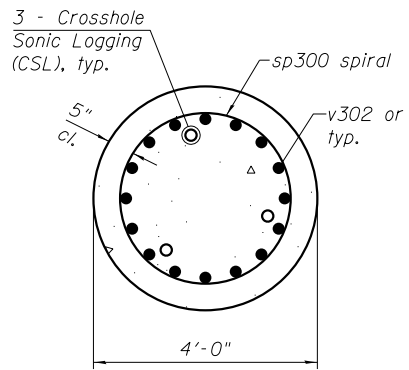
SECTION A-A



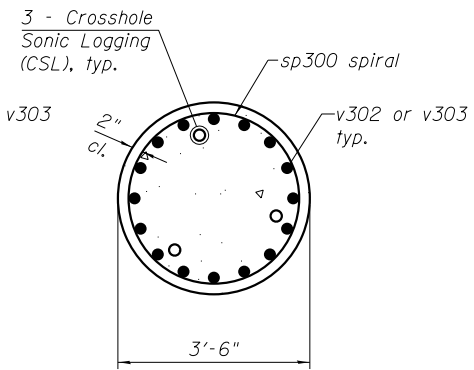
SECTION B-B



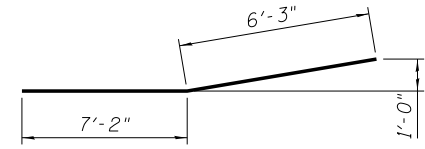
SECTION C-C



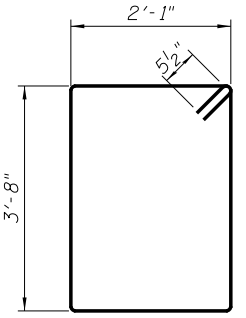
SECTION D-D



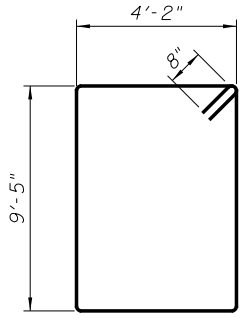
SECTION E-E



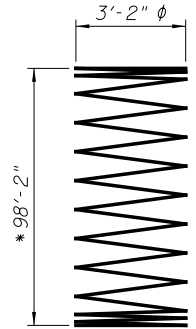
BAR p302(E)



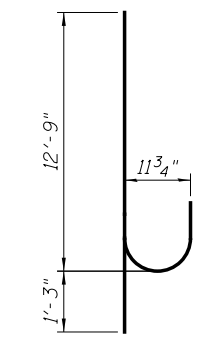
BAR s301(E)



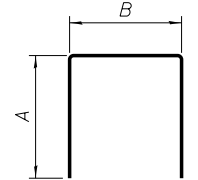
BAR s303(E)



BAR sp300



BAR v300(E)



BARS s300(E), s302(E) s304(E), u300(E), u301(E) & u302(E)

| Bar | A | B |
|---------|-------|--------|
| s300(E) | 3'-3" | 2'-1" |
| s302(E) | 2'-8" | 2'-10" |
| s304(E) | 3'-2" | 2'-10" |
| u300(E) | 3'-9" | 2'-8" |
| u301(E) | 1'-6" | 2'-10" |
| u302(E) | 4'-2" | 4'-0" |

BILL OF MATERIAL

| Bar | No. | Size | Length | Shape |
|--------------------------------------|-----|---------|--------|-------|
| h300(E) | 12 | #5 | 36'-3" | — |
| h301(E) | 5 | #5 | 23'-8" | — |
| h302(E) | 36 | #7 | 57'-0" | — |
| p300(E) | 32 | #9 | 39'-6" | — |
| p301(E) | 8 | #8 | 55'-0" | — |
| p302(E) | 16 | #8 | 13'-5" | — |
| p303(E) | 14 | #8 | 57'-0" | — |
| s300(E) | 128 | #5 | 8'-7" | □ |
| s301(E) | 116 | #5 | 12'-5" | □ |
| s302(E) | 32 | #5 | 8'-2" | □ |
| s303(E) | 58 | #6 | 28'-6" | □ |
| s304(E) | 32 | #5 | 9'-2" | □ |
| ** sp300 | 7 | #5 | 98'-2" | W |
| u300(E) | 10 | #5 | 10'-2" | □ |
| u301(E) | 25 | #5 | 5'-10" | □ |
| u302(E) | 40 | #7 | 12'-4" | □ |
| v300(E) | 72 | #9 | 14'-0" | U |
| v301(E) | 112 | #10 | 18'-2" | — |
| v302 | 112 | #10 | 60'-0" | — |
| v303 | 112 | #10 | 44'-6" | — |
| Structure Excavation | | Cu. Yd. | 63 | |
| Concrete Structures | | Cu. Yd. | 143 | |
| Reinforcement Bars | | Pound | 64,860 | |
| Reinforcement Bars, Epoxy Coated | | Pound | 32,110 | |
| Drilled Shaft in Soil | | Cu. Yd. | 311 | |
| Drilled Shaft in Rock | | Cu. Yd. | 7.5 | |
| Concrete Sealer | | Sq. Ft. | 2817 | |
| Crosshole Sonic Logging Access Ducts | | Foot | 687 | |
| Crosshole Sonic Logging Testing | | Each | 2 | |

Notes:
 Apply concrete sealer to all exposed concrete surfaces of the pier.
 * The quantities and reinforcement detailing are based on the top of shaft and the estimated top of rock elevations shown and may change based on the actual top of rock encountered at each shaft and the final top of shaft elevation.
 ** Length is height of spiral.
 When splicing spiral reinforcement is necessary, the spirals shall be provided with 1/2" extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4, or shall both terminate in 130° standard hook.

0161700-60X95-5046-DET.dgn

PARSONS BRINCKERHOFF

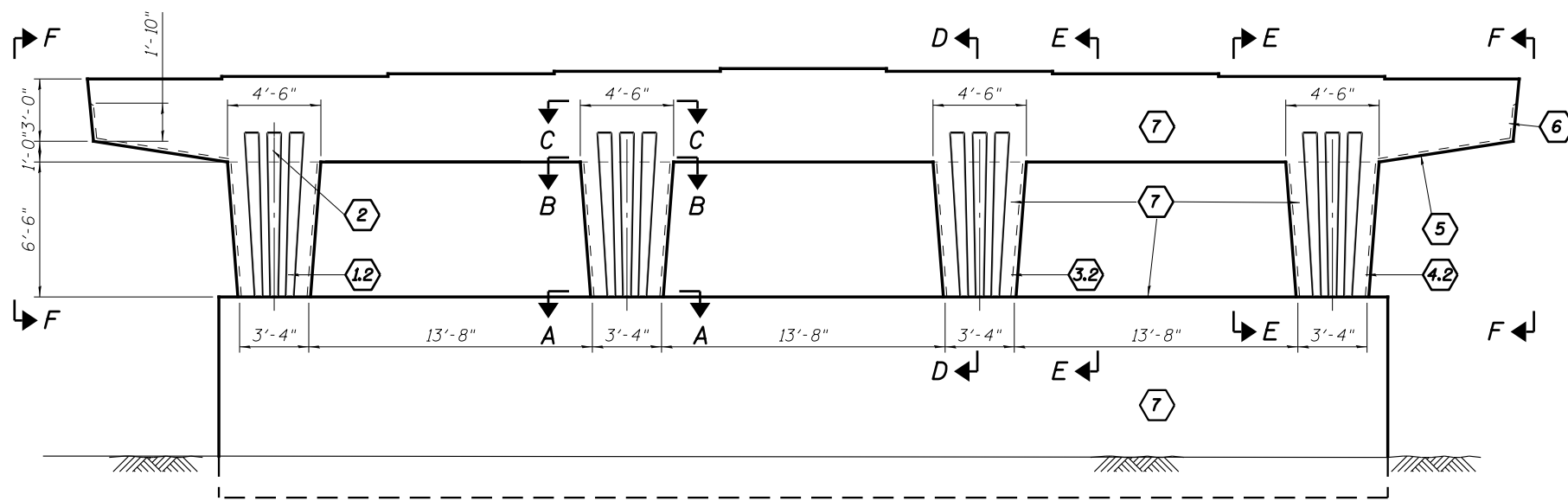
| | | |
|---------------------------|---------------|-----------|
| USER NAME = lopezgonzalez | DESIGNED - MS | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 9/1/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

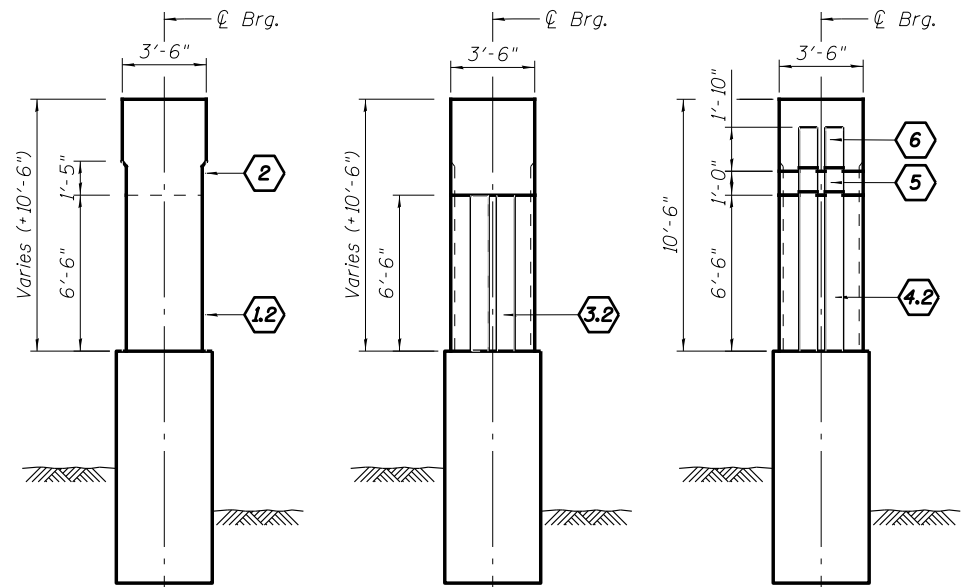
**PIER 2 DETAILS
STRUCTURE NO. 016-1700**

SHEET NO. S1-46 OF S1-60 SHEETS

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 90/94 | 2014-016R&B | COOK | 378 | 198 |
| CONTRACT NO. 60X95 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



ELEVATION
(Looking East)
(Looking West - Similar)

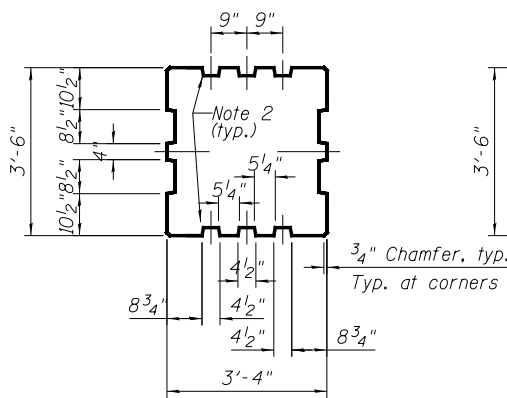


SECTION D-D

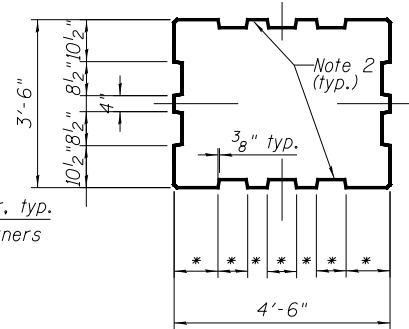
SECTION E-E

PIER END VIEW F

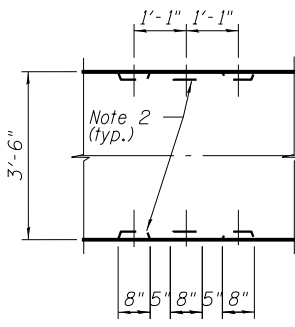
(Looking South)
(Looking North)



SECTION A
At Bottom of Column

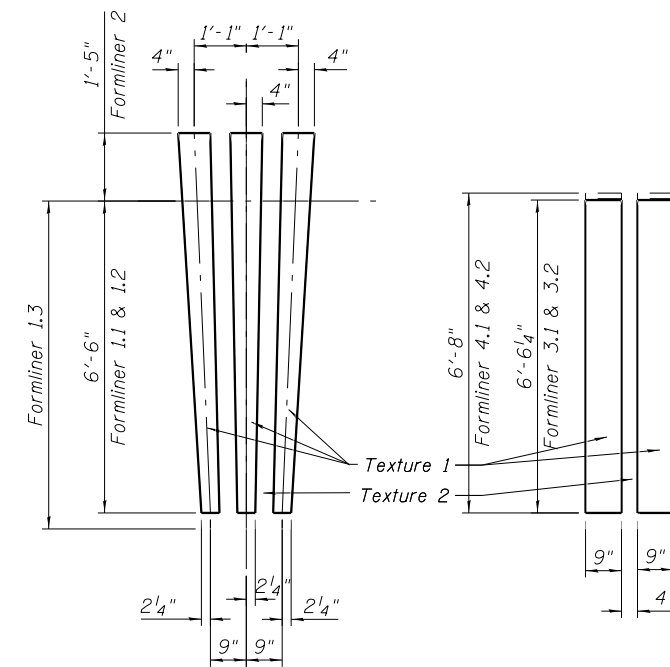


SECTION B
At Top of Column

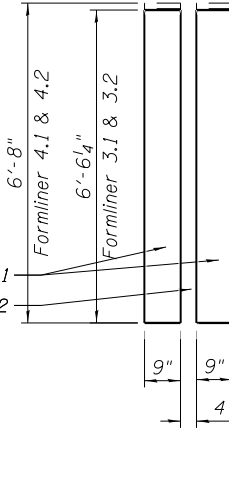


SECTION C
In Cap

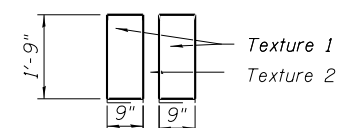
* Dimensions of formliner at top of column (panel 1) to match dimensions of formliner at bottom of pier cap (panel 2).



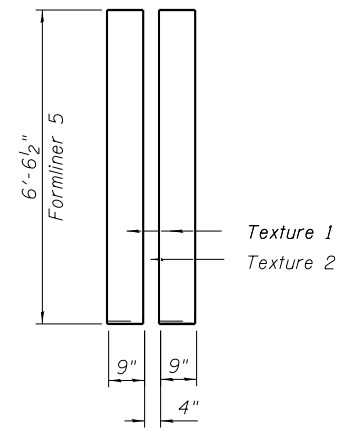
FORM LINER
PANEL 1.1, 1.2 & 2



FORM LINER
PANEL 3.1, 3.2, 4.1 & 4.2



FORM LINER
PANEL 6

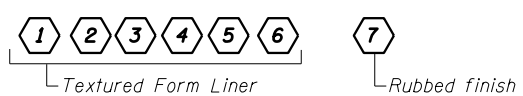


FORM LINER
PANEL 5

NOTES:

1. Surface indicated as 7 represents all surface except formliner and shall have smooth rubbed finish. Cost included with pay item Rubbed Finish.
2. Tapered fluting - dimensions vary, see elevation profile.
3. Form liner panel 2 is continuation of panel 1. Keep adjacent form liners aligned.
4. Hand clean and smooth the surface of the construction joint between the pier and cap.
5. Texture 1: Light Sandblast, 1/16" depth, as selected from samples provided by the Contractor. Texture 2: Smooth
6. Form liner panel 1.3, at Pier 3 is similar to form liner panel 1.1 and 1.2 at Pier 1 & 2, only longer.
7. The three flutes at Form liner panel 1.1, 1.2 and 1.3 have same geometry but are different at the top and bottom.
8. Formliner panel 4, 5, and 6 connect to each other. Provide miter edges as required, in order to provide continuous pattern.

LEGEND



BILL OF MATERIAL

| Item | Unit | Total |
|-----------------------------|---------|-------|
| Form Liner Textured Surface | Sq. Ft. | 273 |
| Rubbed Finish | Sq. Ft. | 2043 |

0161700-60X95-S047-PA2.dgn

PARSONS
BRINCKERHOFF

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MR | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - AH | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - MR | REVISED - |
| | CHECKED - JIG | REVISED - |

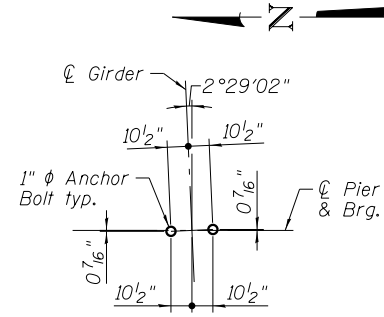
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2 ARCHITECTURAL DETAILS
STRUCTURE NO. 016-1700

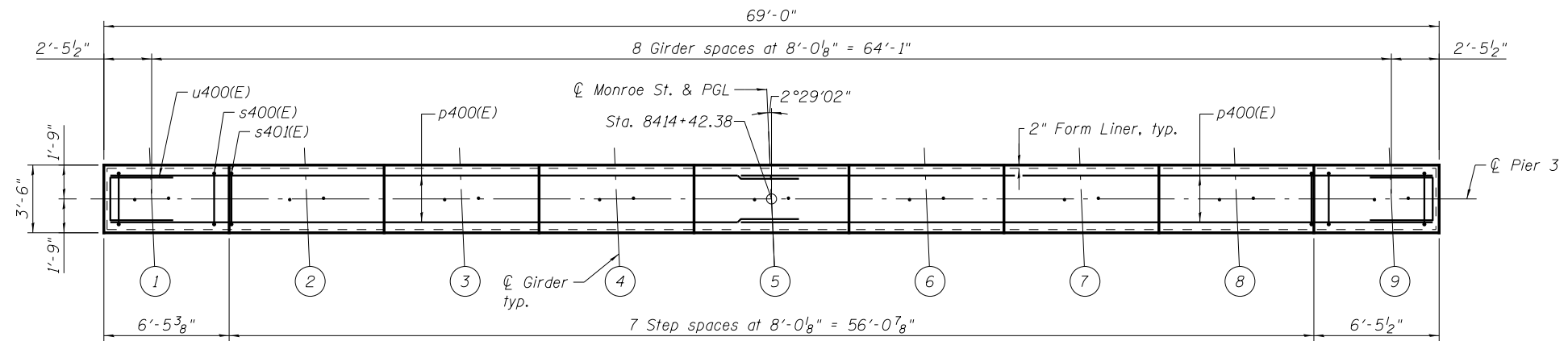
SHEET NO. S1-47 OF S1-60 SHEETS

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.I. R.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 199 |
| CONTRACT NO. 60X95 | | | | |

ILLINOIS FED. AID PROJECT



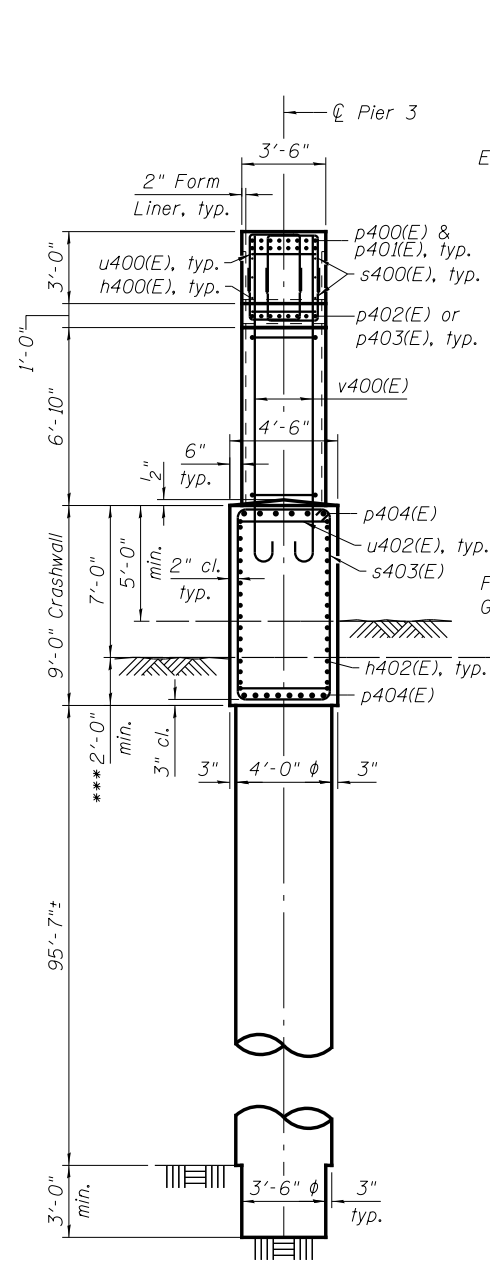
ANCHOR BOLT LAYOUT



TOP PLAN

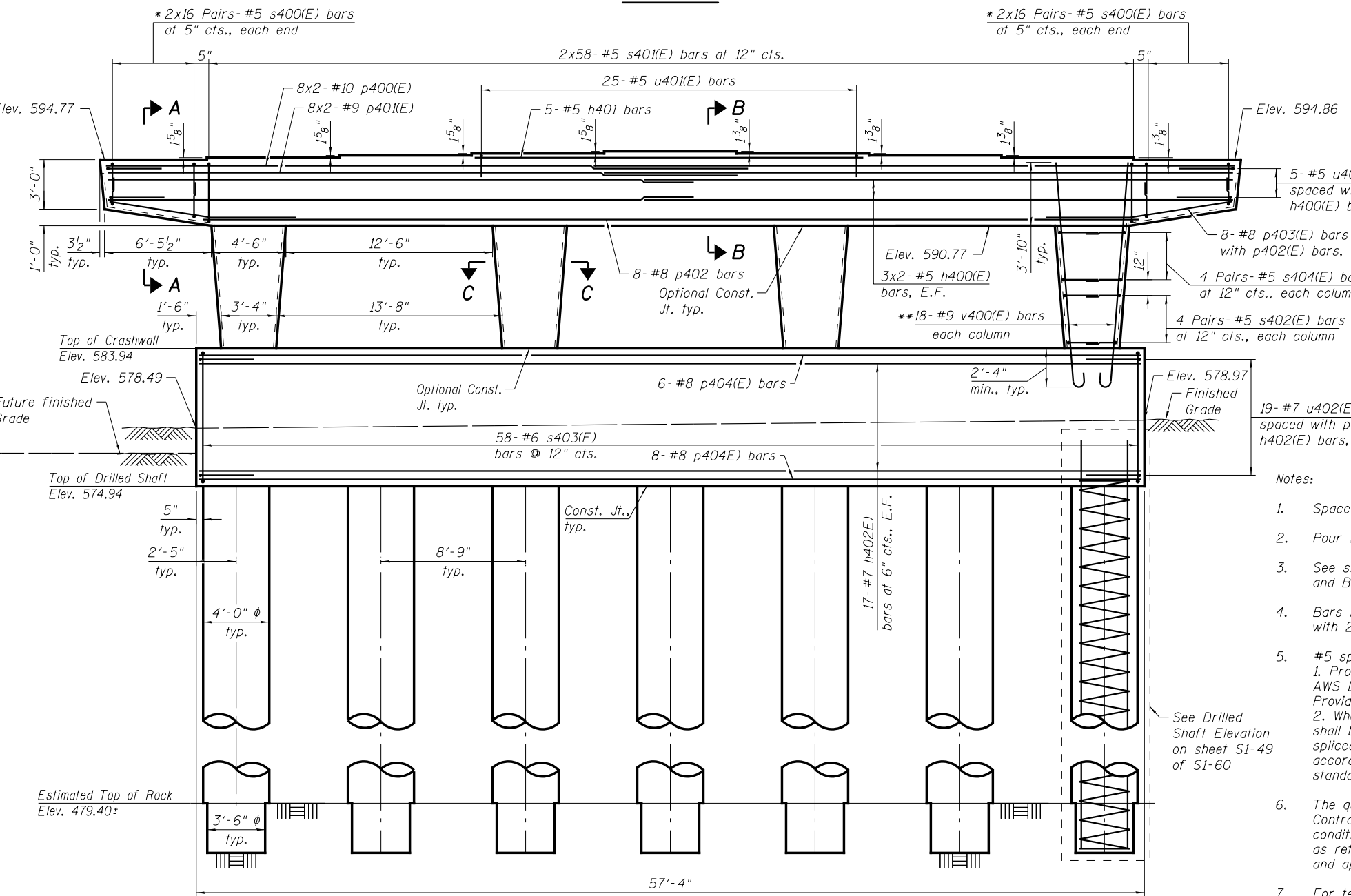
TOP OF SEAT ELEVATION

| Girder No. | Seat Elevation |
|------------|----------------|
| 1 | 594.77 |
| 2 | 594.91 |
| 3 | 595.04 |
| 4 | 595.18 |
| 5 | 595.32 |
| 6 | 595.20 |
| 7 | 595.09 |
| 8 | 594.98 |
| 9 | 594.86 |



END VIEW

Drilled shaft reinforcement not shown for clarity



ELEVATION

(Looking East)

- * Cut vertical legs of bars to fit. Min. lap is 2'-7".
- ** Rotate bars to maintain minimum clearance.
- *** From future finished grade

MINIMUM BAR LAP

- #5 bar = 3'-9"
- #9 bar = 10'-4"
- #10 bar = 6'-4" (Drilled Shaft)
- #10 bar = 11'-6"

- Notes:
- Space reinforcement in cap to miss anchor bolts.
 - Pour steps monolithically with cap.
 - See sheet S1-49 of S1-60 for section A-A, B-B, C-C, and Bill of Materials.
 - Bars noted thus, 3x2-#5 indicates 3 lines of bars with 2 lengths of bars per line.
 - #5 sp400(E) spiral, each drilled shaft
 - Provide 1/2 extra turns, shop welded together per AWS D1.4 top and bottom. Extend spiral 2" into crashwall. Provide 4-#4 spacers or equivalent.
 - When splicing spiral reinforcement is necessary, the spiral shall be provided with 1/2 extra turns at the ends to be spliced. These additional turns shall either be welded together according to AWS D1.4 or shall both terminate with a 135° standard hook.
 - The quality of bedrock at Pier 3 shall be checked by the Contractor during construction to verify the design bedrock conditions. Boring 2054-B-05 information should be used as reference. An RQD of 75% or more should be verified and approved by the Engineer.
 - For temporary casing, see note 13 on sheet S1-02 of S1-60.

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PARSONS BRINCKERHOFF

| | | |
|-----------------------|---------------|-----------|
| USER NAME = pateld | DESIGNED - MS | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - CB | REVISED - |
| PLOT DATE = 6/29/2017 | DRAWN - DCP | REVISED - |
| | CHECKED - JIG | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 3
STRUCTURE NO. 016-1700**

SHEET NO. S1-48 OF S1-60 SHEETS

| | | | | |
|--------------------|-------------|--------|---------------------------|-----------|
| F.A.I. R.T.E. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 90/94 | 2014-016R&B | COOK | 378 | 200 |
| CONTRACT NO. 60X95 | | | ILLINOIS FED. AID PROJECT | |