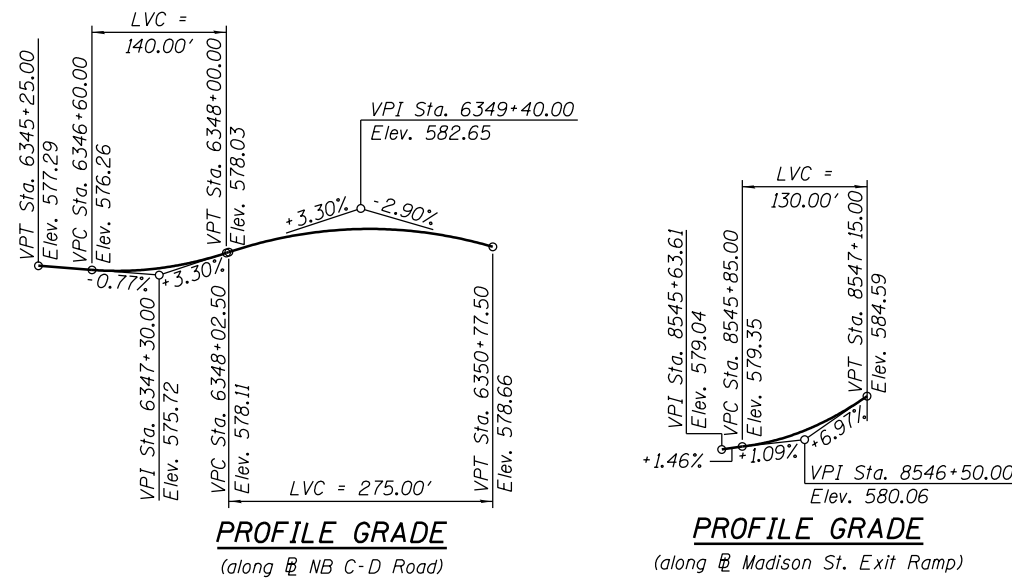


GENERAL NOTES:

- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in price of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Reinforcement bars designated "(E)" shall be epoxy coated.
- Protective Coat shall be applied to exposed surfaces of the parapet and concrete sealer shall be applied to exposed surfaces of the facing.
- Existing utilities in conflict with retaining wall construction shall be protected or relocated according to directions given on the roadway plans.
- The Contractor shall exercise extreme caution during wall construction to make certain that construction activities, live load surcharge and other loads applied to the wall will not have detrimental effects on the adjacent structures, buildings and utilities. See Special Provision for Construction Vibration Monitoring.
- The contractor shall provide vibration and displacement monitoring at the locations specified in the Special Provision for Construction Vibration Monitoring, to ensure that removal/construction activities in the vicinity of the structures do not have detrimental effects on building foundations. No additional compensation shall be provided to the Contractor for alternative means and methods, or additional precautionary measures, required during removal/construction activities to satisfy these requirements. See Contract Special Provisions for details.
- Slipforming of parapet is not allowed.
- The Contractor shall take all necessary precautions not to contaminate groundwater during the drilled shaft construction operation. Contractor is responsible for the proper containment and disposal of the contaminated groundwater and spoils resulting from Contractor's means and methods. No additional cost will be paid for this effort.
- Limited groundwater elevation data is available in the boring logs. In addition, groundwater may also be present in deeper granular layers. The groundwater may rise in the shafts to an elevation above the top of granular layers. The Contractor shall consider this information when choosing construction methods. The contractor will not be compensated for issues related to the groundwater elevation.
- The Contractor shall provide a method to assure the soldier piles achieve at least the plan tip elevations. The soldier pile locations and elevations shall meet the tolerances provided in the Standard Specifications. Any additional measures required to satisfy the construction tolerances will not be paid for separately but shall be included in Drilling and Setting Soldier Piles (In Soil).
- Any storage of construction equipment and material behind the wall is not allowed.
- Contractor shall determine the appropriate curing time prior to initiating drilling for soldier piles to minimize any cracking and damage to the Secondary CLSM Shafts. Contractor shall be responsible for repairing or shoring any damaged areas encountered during excavation. Contractor shall not begin excavation until Secondary CLSM Shafts attains min. compressive strength of 200 psi.
- Secondary CLSM shafts shall be filled with CLSM as soon as the shaft excavation and cleaning have been completed. Secondary CLSM Shafts shall not be permitted to remain open overnight without being filled with CLSM.
- Earth excavation in front of wall shall be gradual and no more than 4'-0" of earth shall be excavated at a time in front of the wall. The elevation difference between adjacent excavated areas in front of wall shall not vary by more than 6'-0" over a distance of 50'-0" as measured along the length of the wall.
- Temporary and permanent steel casing(s) shall be placed through the upper granular soils (sand and gravel) by twisting, screwing and pushing the casing into the underlying soft clay formation a sufficient distance to provide a watertight seal. Once this seal has been achieved, the soil and water inside the casing can be removed. The casing can then be extended to the required depth by twisting, screwing and pushing in conjunction with excavation inside of the casing. Extend the casing as necessary to control the inflow of groundwater. The drilled excavation shall not advance below the bottom of the casing during this installation. For Secondary CLSM Shafts, removal of temporary casing and CLSM placement must be properly coordinated to prevent contamination of the CLSM.
- During excavation for drilled Soldier Piles and Secondary CLSM Shafts Contractor may encounter existing abandoned 12" ϕ watermain. Contractor shall remove existing abandoned watermain as necessary and as directed by the Engineer. This work shall not be paid separately but shall be included with Removal of Existing Structure No. 2.
- Contractor shall coordinate proposed wall construction with construction of Monroe St bridge East Abutment.
- Concrete for Drilled Soldier Pile Encasement shall be in accordance with Section 522 of Standard Specifications, except that the mix design of concrete shall attain a compressive strength of 7,000 psi at 14 days. Soldier Pile Encasement shall extend from top of pile elevation to bottom of pile tip. Cost included with Drilling and Setting Soldier Piles (In Soil).

SEQUENCE OF CONSTRUCTION:

- Locate existing utilities that are to remain. Contractor to coordinate any required improvements to, or removals of, existing utilities with utility owner.
- Construct Secondary CLSM Shafts.
- Drill for Soldier Pile shafts, install Permanent Casing, soldier piles and concrete encasement.
- Excavate earth in front of wall and chip Secondary CLSM shafts as necessary. Drill weep holes into Secondary CLSM shafts and install geocomposite wall drain.
- Install timber lagging between the beginning of the wall and P-1, and between P-25 and proposed Monroe St. Bridge Abutment. While excavating, remove existing wall stem in front of the proposed wall as necessary.
- Construct concrete fascia and concrete gutter behind the wall as shown in the plans.
- Complete removal of stem, footing and piles of existing wall in front of the proposed wall.



CURVE DATA

(NB C-D Road)
 Curve: P-NCD-NX-6
 PI Sta. = 6345+36.95
 $\Delta = 5^\circ 12' 37''$ (LT)
 $D = 1^\circ 05' 35''$
 $R = 5,242.00'$
 $T = 238.51'$
 $L = 476.70'$
 $E = 5.42'$
 $e = NC$
 $T.R. = NA$
 $S.E. Run = NA$
 $P.C. Sta. = 6342+98.44$
 $P.T. Sta. = 6347+75.14$

INDEX OF SHEETS:

S2-01	General Plan and Elevation
S2-02	General Notes, Index of Sheets, Total Bill of Material
S2-03	Soldier Pile Wall Typical Cross Sections and Details
S2-04	Plan and Elevation I
S2-05	Plan and Elevation II
S2-06	Soldier Pile Wall Sections, Details and Bill of Material
S2-07	Secondary CLSM Shaft Details
S2-08	Architectural Details
S2-09	Boring Logs I
S2-10	Boring Logs II
S2-11	Boring Logs III
S2-12	Boring Logs IV
S2-13	Boring Logs V
S2-14	Boring Logs VI

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Removal Of Existing Structures No. 2	Each	1
Structure Excavation	Cu Yd	323.1
Concrete Structures	Cu Yd	102.6
Concrete Superstructure	Cu Yd	33.2
Protective Coat	Sq Yd	154
Stud Shear Connectors	Each	287
Reinforcement Bars, Epoxy Coated	Pound	14600
Name Plates	Each	1
Permanent Casing	Foot	1826
Secant Lagging	Cu Ft	5611
Furnishing Soldier Piles (W Section)	Foot	1826
Drilling And Setting Soldier Piles (In Soil)	Cu Ft	30702
Untreated Timber Lagging	Sq Ft	32
Concrete Sealer	Sq Ft	1738
Geocomposite Wall Drain	Sq Yd	167
Pipe Underdrains For Structures 4"	Foot	150
Slope Inclinometer	Each	1

STATION 6345+67.55 TO 6347+16.62
 BUILT 20__ BY
 STATE OF ILLINOIS
 F.A.I. RTE. 90/94 SEC 2014-016R&B
 LOADING HL-93
 STRUCTURE NO. 016-2017

NAME PLATE

See Std. 515001

ELEVATIONS TABLE

STATION	OFFSET	ELEVATION A	ELEVATION B	ELEVATION C	ELEVATION D	ELEVATION E	ELEVATION F
6345+67.55	33'-0 7/8" Rt.	576.43	580.05	582.39	589.23	574.43	581.76
6345+97.36	32'-8 5/8" Rt.	576.20	579.82	585.30	589.01	574.20	579.73
6346+27.18	32'-6 3/8" Rt.	575.97	579.59	585.60	589.38	573.97	579.40
6346+56.99	32'-6 1/4" Rt.	575.74	579.36	585.90	588.88	573.74	579.12
6346+86.81	32'-8 1/8" Rt.	575.62	579.24	586.20	589.07	573.52	579.02
6347+16.62	33'-0" Rt.	575.76	579.38	586.50	591.15	573.76	578.89

Elevation A: Future Edge of Shoulder at Face of Proposed Roadway Barrier.
 Elevation B: Future Proposed Grade at Front Face of Wall.
 Elevation C: Top of Wall Elevation.
 Elevation D: Existing Grade at Front Face of Wall (at intersection of line projected from FF of fascia panel and existing ground line).
 Elevation E: Bottom of Fascia Panel.
 Elevation F: Interim Grade at Front Face of Wall.

0162017-W60X95-SHT-02-GenNote.dgn



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	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

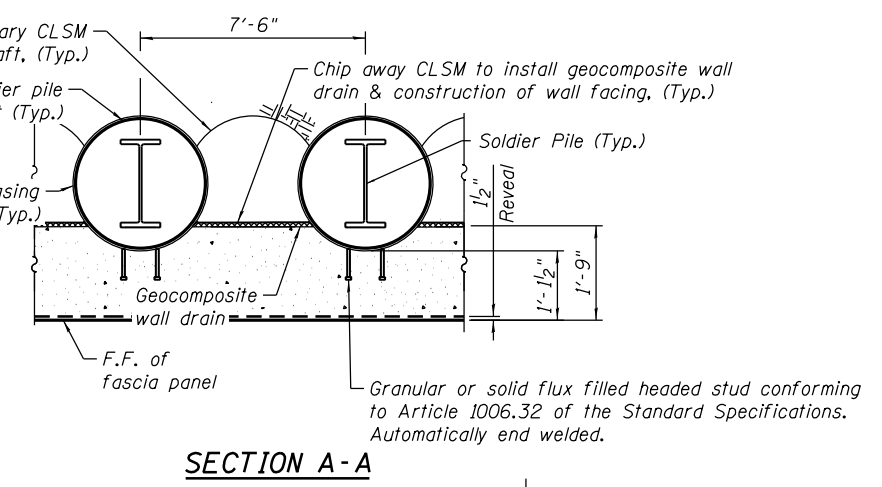
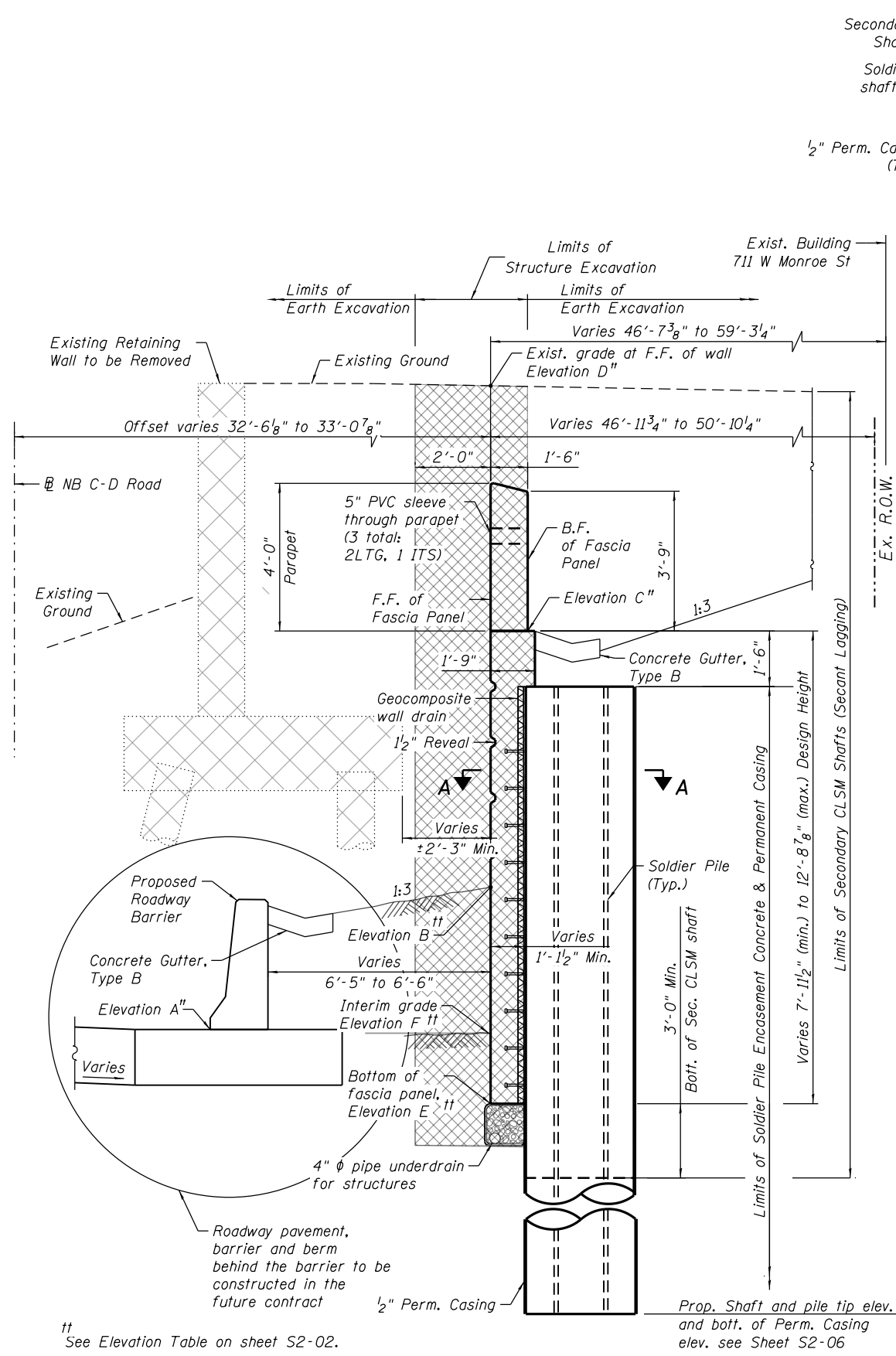
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, INDEX OF SHEETS, TOTAL BILL OF MATERIAL
 RETAINING WALL 29 (STRUCTURE NO. 016-2017)

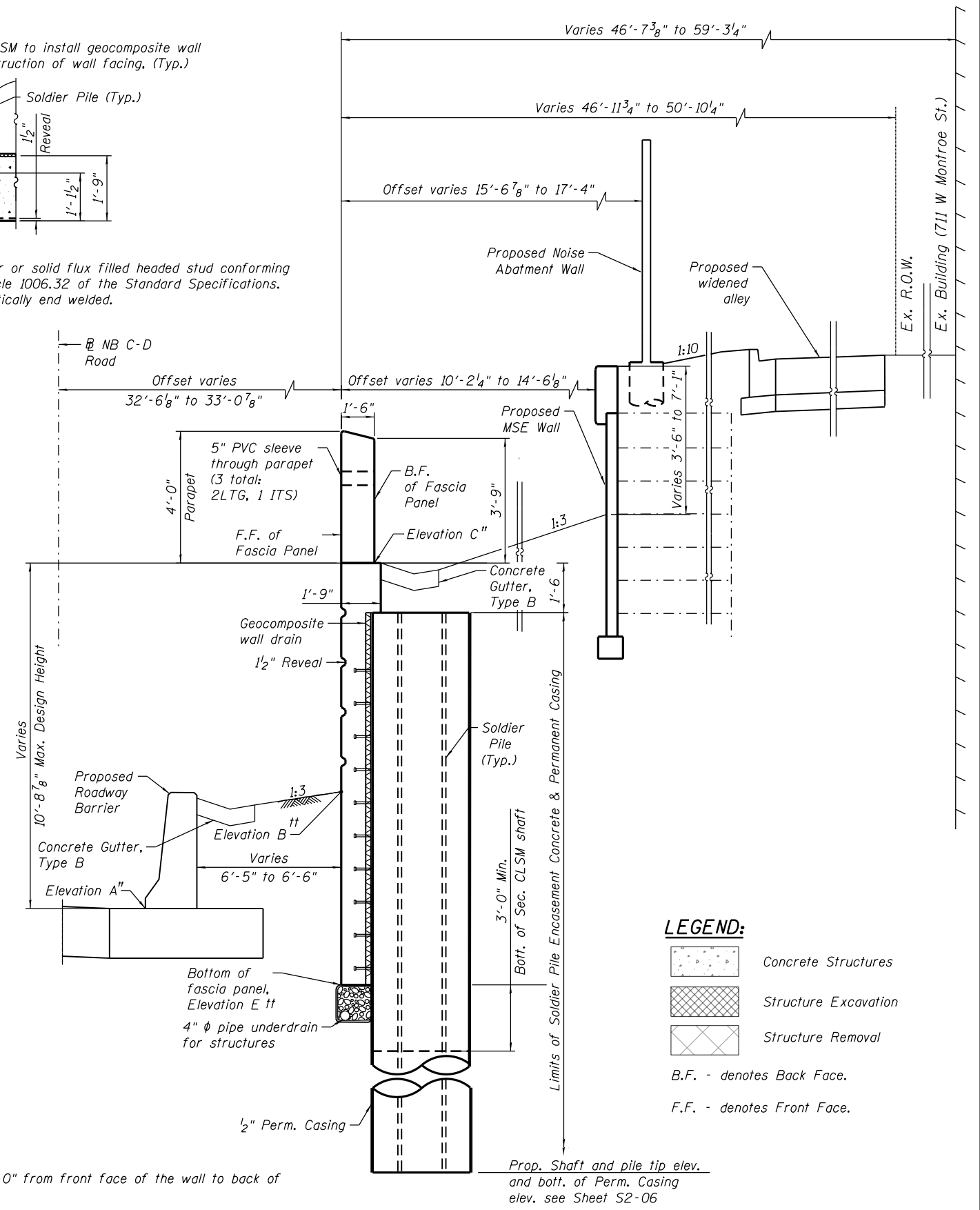
SHEET NO. S2-02 OF 14 SHEETS

F.A.I. RTE. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 301
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

016-2017-W60X95-SHT-03-Typical Section.dgn



SECTION A-A



FUTURE CONTRACT - TYPICAL SECTION

LEGEND:

- Concrete Structures
- Structure Excavation
- Structure Removal

B.F. - denotes Back Face.
F.F. - denotes Front Face.

- NOTES:**
- Structure Excavation is measured 2'-0" from front face of the wall to back of soldier pile concrete fascia panel.
 - See sheet S2-05 for joint details and pipe underdrain details.
 - See sheet S2-06 for stud shear connector detail and parapet joint detail.

CONTRACT 60X95 - INTERIM TYPICAL SECTION
(Looking North)



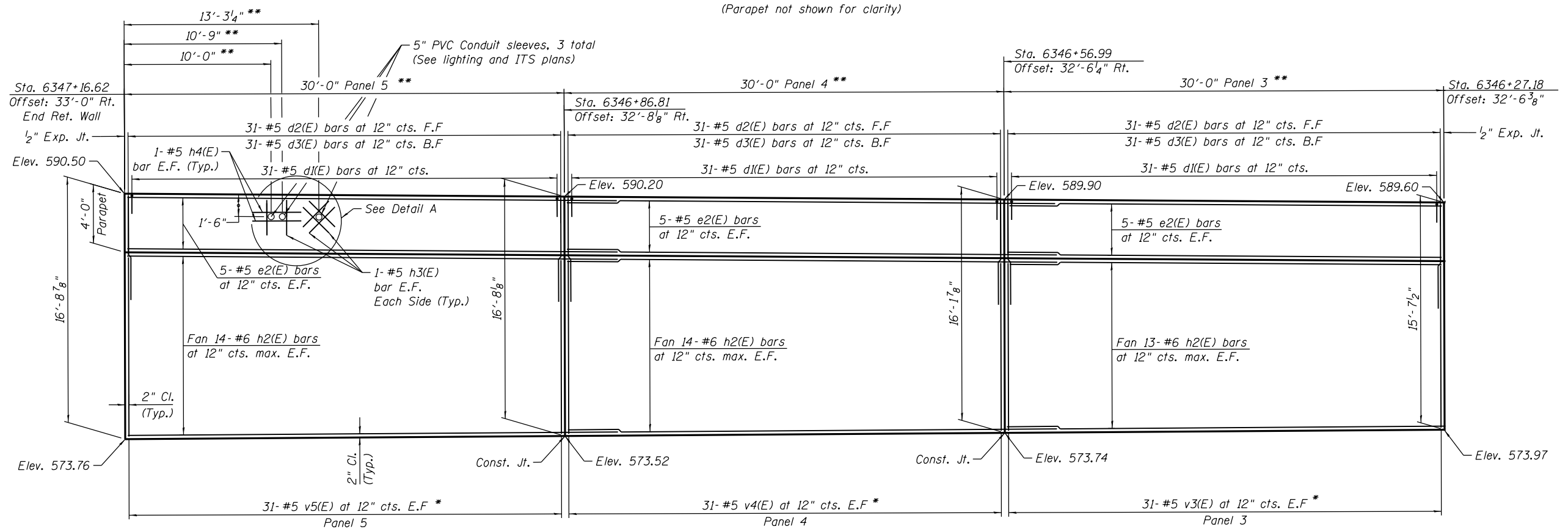
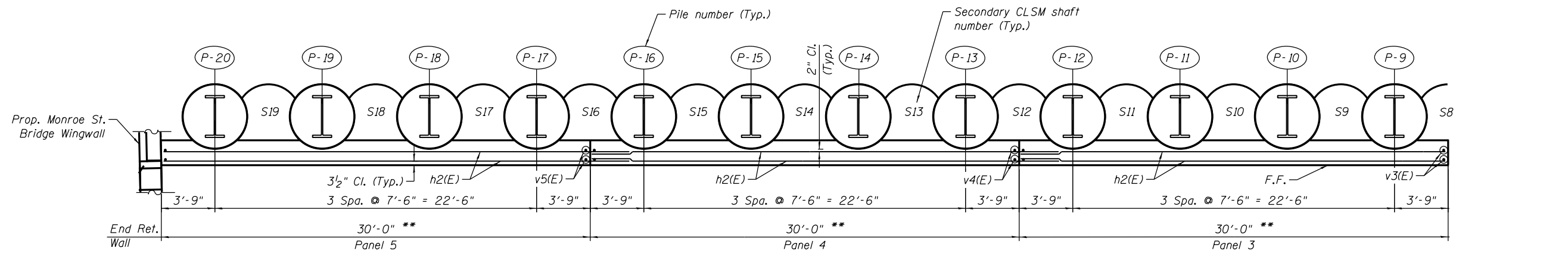
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	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOLDIER PILE WALL TYPICAL CROSS SECTION AND DETAILS
RETAINING WALL 29 (STRUCTURE NO. 016-2017)

SHEET NO. S2-03 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	302
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				



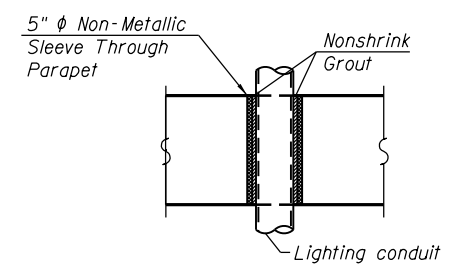
NOTES:

1. For typical wall cross sections and details, see sheet S2-03.
2. For soldier pile layout, min bar laps, sections, details and Bill of Material, see sheet S2-06.
3. See sheet S2-07 for Secondary CLSM Shaft Details.
4. Station and offset are measured along F.F. of the wall from NB C-D Road.

LEGEND:

- B.F. - denotes Back Face.
- E.F. - denotes Each Face.
- F.F. - denotes Front Face.

* See Field Cutting Diagram on sheet S2-06.
** Measured along F.F. of the wall.



DETAIL A
SLEEVE THRU PARAPET
Furnishing and installing Non-Metallic Sleeve and Grout is included in the cost of Concrete Superstructure. See Lighting Plans for additional details.

016Z017-W60X95-SHT-04-P&E-1

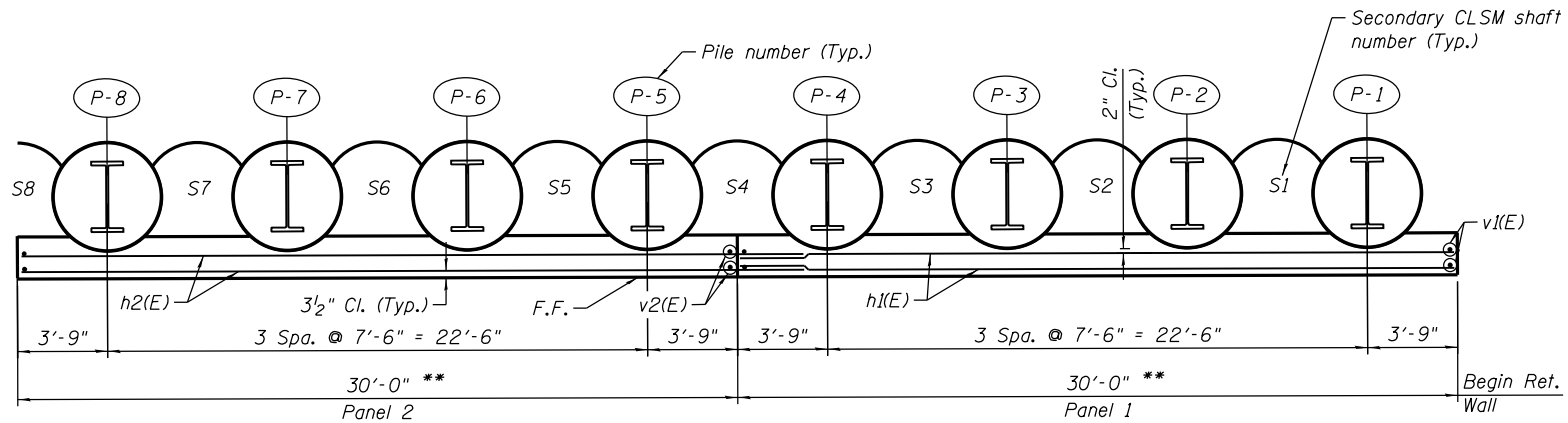


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	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND ELEVATION I
RETAINING WALL 29 (STRUCTURE NO. 016-2017)
SHEET NO. S2-04 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				

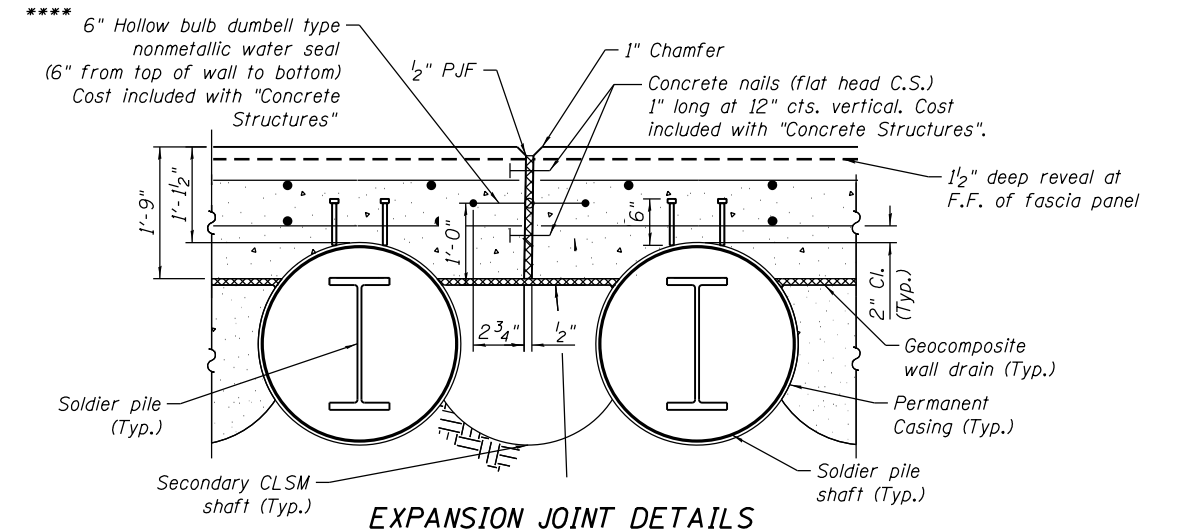


PLAN

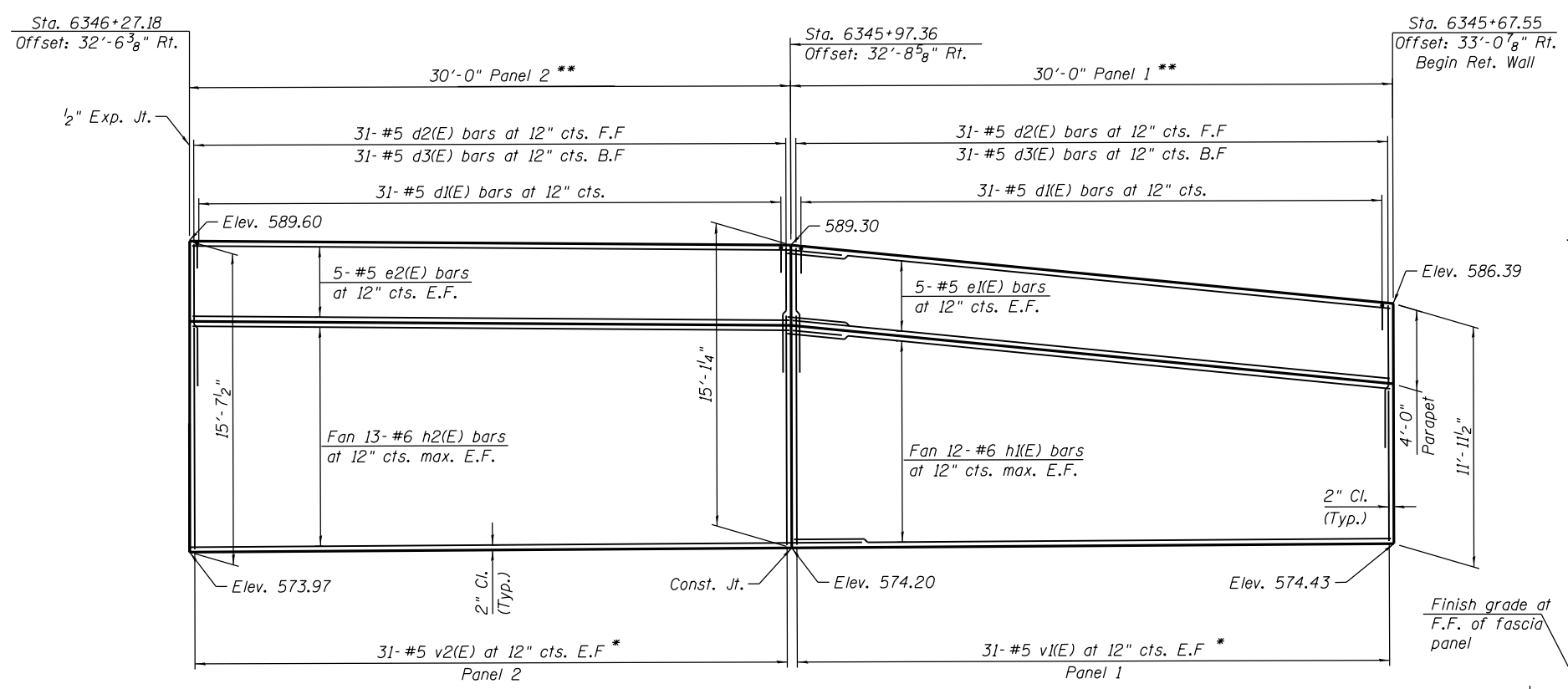
(Parapet not shown for clarity)

* See Field Cutting Diagram on sheet S2-06

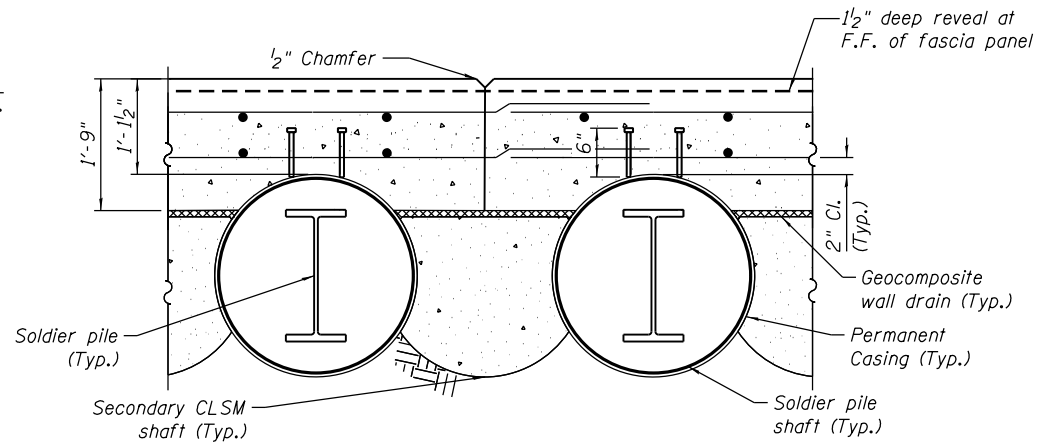
** Measured along F.F. of the wall



EXPANSION JOINT DETAILS



ELEVATION



CONSTRUCTION JOINT DETAILS

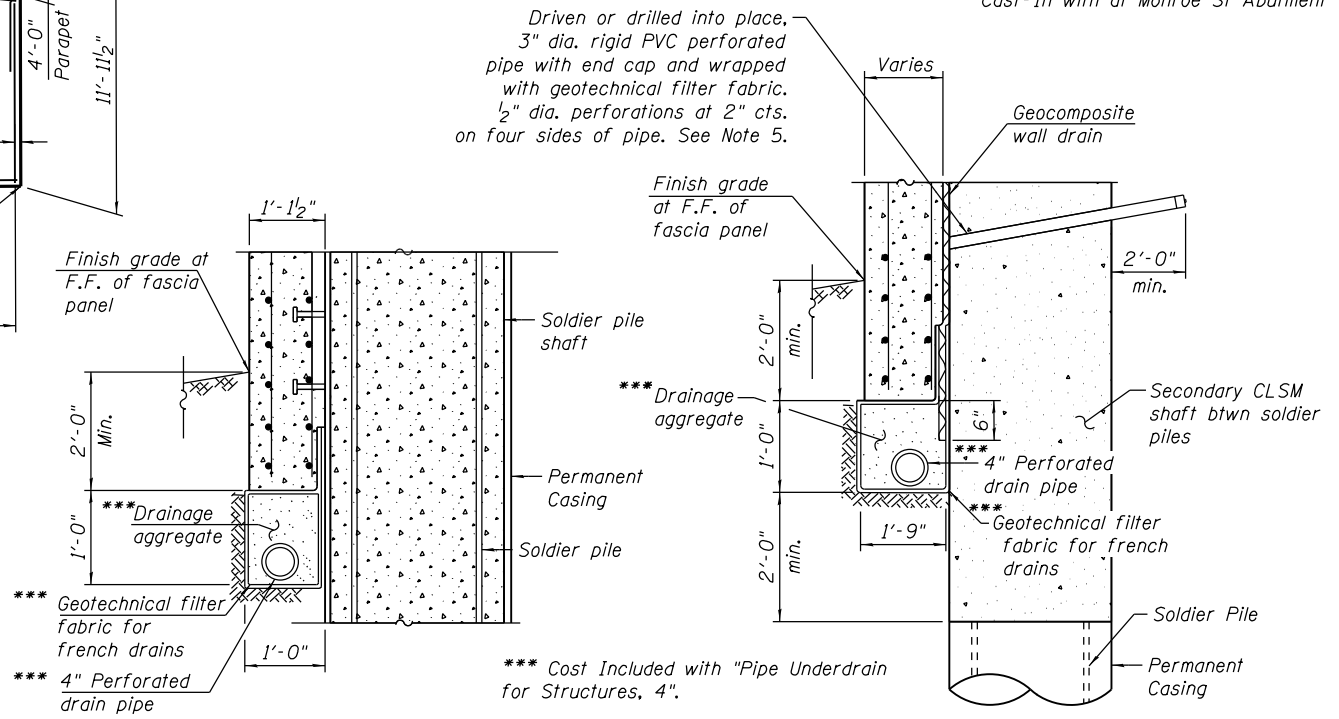
**** Installed in Saw Cut Groove or Cast-In with at Monroe St Abutment

NOTES:

1. For typical wall cross sections and details, see sheet S2-03.
2. For soldier pile layout, min bar laps, sections, details and Bill of Material, see sheet S2-06.
3. See sheet S2-07 for Secondary CLSM Shaft Details.
4. Stations and offsets are measured along F.F. of the wall from NB C-D Road.
5. Prior to placement of geocomposite wall drain & concrete for the wall fascia, Contractor shall drill or drive 3/2" ϕ weep holes for 3" ϕ perforated PVC pipe at 4'-0" (\pm) cts. along the height of the Secondary CLSM Shafts. The location of the weep holes will be submitted to the Engineer for approval prior to installation. Contractor shall clean the PVC pipe after installation. Cost shall be included with Concrete Structures.

LEGEND:

- B.F. - denotes Back Face.
- E.F. - denotes Each Face.
- F.F. - denotes Front Face.



PIPE UNDERDRAIN DETAIL AT SOLDIER PILE

PIPE UNDERDRAIN DETAIL BETWEEN SOLDIER PILES



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PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND ELEVATION II
RETAINING WALL 29 (STRUCTURE NO. 016-2017)

SHEET NO. S2-05 OF 14 SHEETS

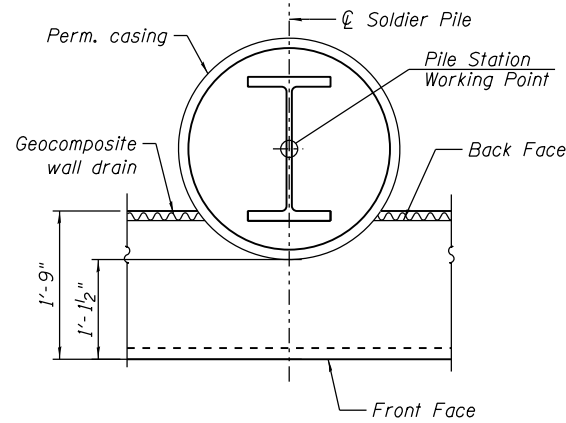
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	304
CONTRACT NO. 60X95				

ILLINOIS FED. AID PROJECT

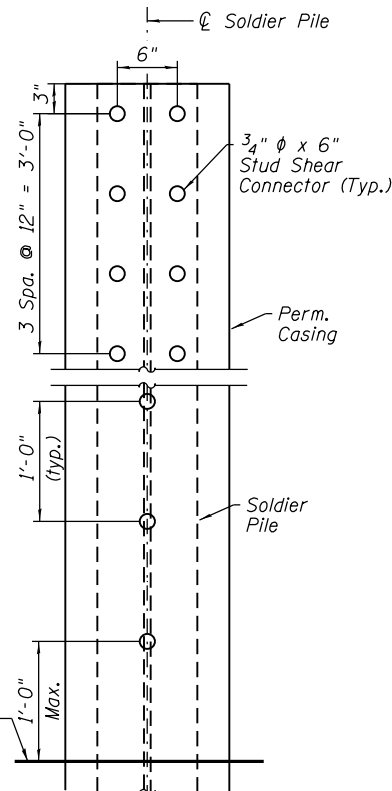
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SOLDIER PILE LAYOUT

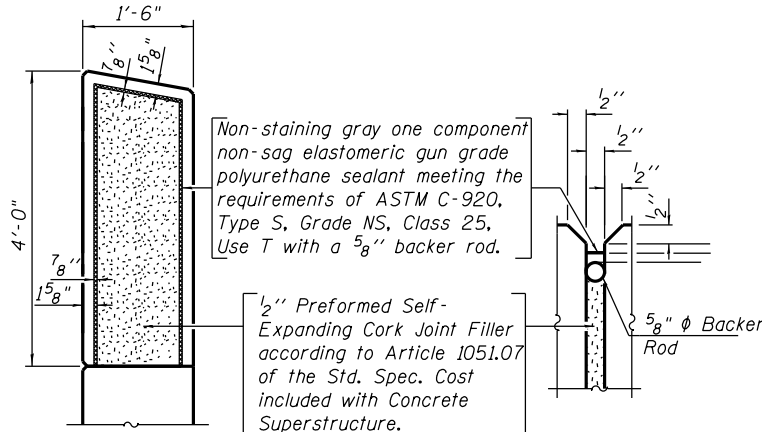
Pile	Station at Working Point	Offset	Top of Parapet Elev.	Top of Pile Elev. & Top of Perm. Casing Elev.	Bottom of Wall Elev.	Section	Shaft Diameter	Pile Tip Elev.	Pile Length	Bottom of Perm. Casing Elev.	Perm. Casing Length
P-01	6345+71.32	36'-5 1/4" Rt.	587.08	581.58	574.32	W33x318	4'-6"	493.32	88'-6"	493.32	88'-6"
P-02	6345+78.77	36'-4 1/8" Rt.	587.51	582.01	574.28	W33x318	4'-6"	493.28	89'-0"	493.28	89'-0"
P-03	6345+86.22	36'-3" Rt.	587.92	582.42	574.24	W33x318	4'-6"	493.24	89'-6"	493.24	89'-6"
P-04	6345+93.67	36'-2 1/8" Rt.	588.29	582.79	574.20	W33x318	4'-6"	493.20	90'-0"	493.20	90'-0"
P-05	6346+01.12	36'-1 1/4" Rt.	588.63	583.13	574.16	W33x318	4'-6"	493.16	90'-0"	493.16	90'-0"
P-06	6346+08.57	36'-0 5/8" Rt.	588.94	583.44	574.12	W33x318	4'-6"	493.12	90'-6"	493.12	90'-6"
P-07	6346+16.01	36'-0" Rt.	589.23	583.73	574.08	W33x318	4'-6"	493.08	91'-0"	493.08	91'-0"
P-08	6346+23.46	35'-11 5/8" Rt.	589.48	583.98	574.04	W33x318	4'-6"	493.04	91'-0"	493.04	91'-0"
P-09	6346+30.91	35'-11 1/4" Rt.	589.71	584.21	574.00	W33x318	4'-6"	493.00	91'-6"	493.00	91'-6"
P-10	6346+38.36	35'-11 1/8" Rt.	589.91	584.41	573.96	W33x318	4'-6"	492.96	91'-6"	492.96	91'-6"
P-11	6346+45.81	35'-11 1/8" Rt.	590.08	584.58	573.92	W33x318	4'-6"	492.92	92'-0"	492.92	92'-0"
P-12	6346+53.26	35'-11 1/8" Rt.	590.21	584.71	573.88	W33x318	4'-6"	492.88	92'-0"	492.88	92'-0"
P-13	6346+60.71	35'-11 3/4" Rt.	590.32	584.82	573.84	W33x318	4'-6"	492.84	92'-0"	492.84	92'-0"
P-14	6346+68.16	35'-11 3/4" Rt.	590.40	584.90	573.80	W33x318	4'-6"	492.80	92'-6"	492.80	92'-6"
P-15	6346+75.60	36'-0 1/8" Rt.	590.46	584.96	573.76	W33x318	4'-6"	492.76	92'-6"	492.76	92'-6"
P-16	6346+83.05	36'-0 3/4" Rt.	590.48	584.98	573.72	W33x318	4'-6"	492.72	92'-6"	492.72	92'-6"
P-17	6346+90.50	36'-1 1/2" Rt.	590.47	584.97	573.68	W33x318	4'-6"	492.68	92'-6"	492.68	92'-6"
P-18	6346+97.95	36'-2 3/8" Rt.	590.44	584.94	573.64	W33x318	4'-6"	492.64	92'-6"	492.64	92'-6"
P-19	6347+05.40	36'-3 1/4" Rt.	590.37	584.87	573.60	W33x318	4'-6"	492.60	92'-6"	492.60	92'-6"
P-20	6347+12.85	36'-4 3/8" Rt.	590.28	584.78	573.56	W33x318	4'-6"	492.56	92'-6"	492.56	92'-6"



SOLDIER PILE WORKING POINT



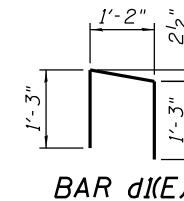
STUD SHEAR CONNECTOR DETAIL



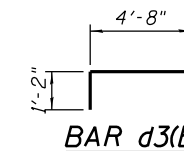
PARAPET JOINT DETAILS

BAR TABLE SCHEDULE

Bar	No. of Sets Req'd	No. of Bars Per Set	A	B	C
v1(E)	1	31	10'-10"	7'-8"	18'-6"
v2(E)	1	31	11'-4"	10'-10"	22'-2"
v3(E)	1	31	11'-10"	11'-4"	23'-2"
v4(E)	1	31	12'-5"	11'-10"	24'-3"
v5(E)	1	31	12'-5"	12'-5"	24'-10"



BAR d1(E)



BAR d3(E)

BILL OF MATERIAL

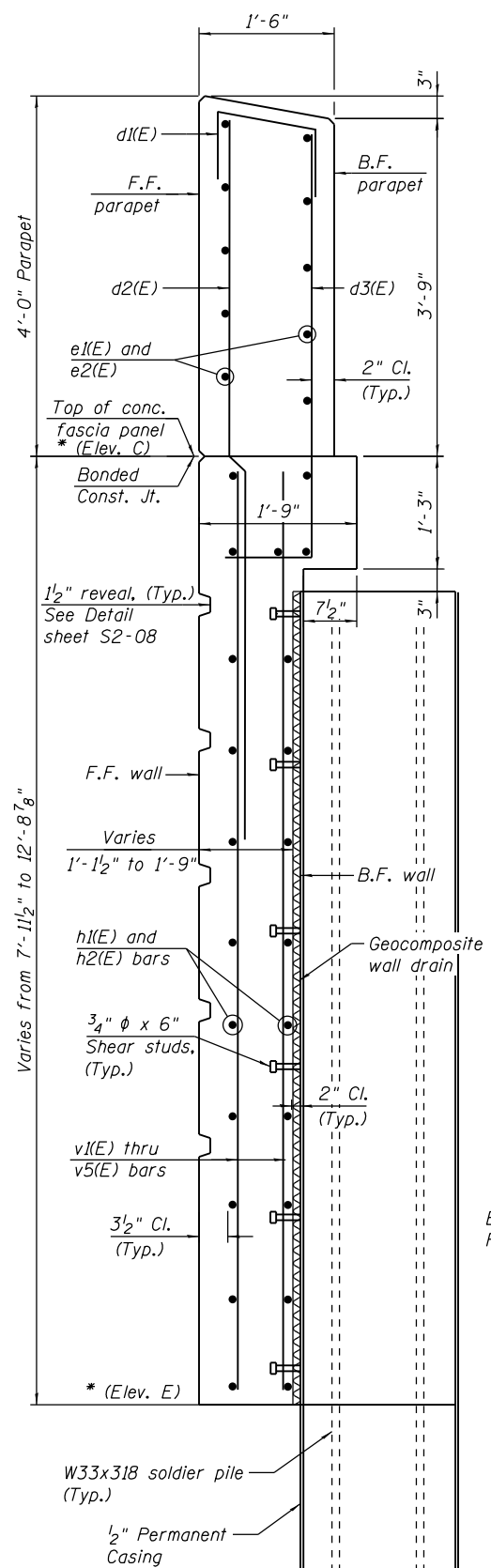
Bar	No.	Size	Length	Shape
d1(E)	155	#5	3'-8"	
d2(E)	155	#5	7'-2"	
d3(E)	155	#5	5'-10"	
e1(E)	10	#5	33'-2"	
e2(E)	40	#5	32'-0"	
h1(E)	24	#6	33'-10"	
h2(E)	108	#6	32'-8"	
h3(E)	12	#5	2'-6"	
h4(E)	4	#5	3'-6"	
v1(E)	31	#5	18'-6"	
v2(E)	31	#5	22'-2"	
v3(E)	31	#5	23'-2"	
v4(E)	31	#5	24'-3"	
v5(E)	31	#5	24'-10"	
Structure Excavation		Cu Yd	323.1	
Concrete Structures		Cu Yd	102.6	
Concrete Superstructure		Cu Yd	33.2	
Protective Coat		Sq Yd	154	
Stud Shear Connectors		Each	287	
Reinforcement Bars, Epoxy Coated		Pound	14600	
Permanent Casing		Foot	1826	
Secant Lagging		Cu Ft	5611	
Furnishing Soldier Piles (W Section)		Foot	1826	
Drilling And Setting Soldier Piles (In Soil)		Cu Ft	30702	
Untreated Timber Lagging		Sq Ft	32	
Concrete Sealer		Sq Ft	1738	
Geocomposite Wall Drain		Sq Yd	167	
Pipe Underdrains For Structures 4"		Foot	150	

NOTES:

- Soldier pile encasement concrete extends from top of pile elevation to bottom of pile tip. Cost shall be included with Drilling and Setting Soldier Piles (In Soil).
- The geocomposite wall drain shall be constructed according to Section 591 of the Standard Specifications.
- Stud shear connectors shall be 3/4" x 6" granular or solid flux filled headed studs, automatically end welded to the permanent casing.
- See sheet S2-05 for Pipe Underdrain details.
- See sheet S2-08 for Architectural Reveals pattern and details. Reveals shall not be paid separately and shall be considered included with the cost of Concrete Structures.
- 1/2" Preformed Flexible Foam Expansion Joint Filler shall be applied per Section 1051.09 of the Standard Specifications. Cost of joint treatments shall be included with the cost of Concrete Structures.
- For Secondary CLSM Shaft Layout Table, see sheet S2-07.
- For Wall Elevations Table, see sheet S2-02.
- Stations and offsets are measured along NB C-D Road.

LEGEND:

B.F. - denotes Back Face.
F.F. - denotes Front Face.

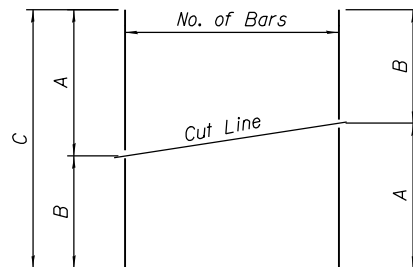


TYPICAL SOLDIER PILE WALL CROSS SECTION

* See Elevation Table on sheet S2-02.

MIN. BAR LAPS

Bar	Lap
#5	3'-2"
#6	3'-10"



BAR CUTTING DIAGRAM

Order v1 thru v5 bars full length. Cut as shown and use remainder of bars in opposite face of wall. See table for dimensions.

016-2017-W60X95-SHT-06-Soldier Pile.dgn



USER NAME = keserovicm	DESIGNED - MK	REVISED
	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

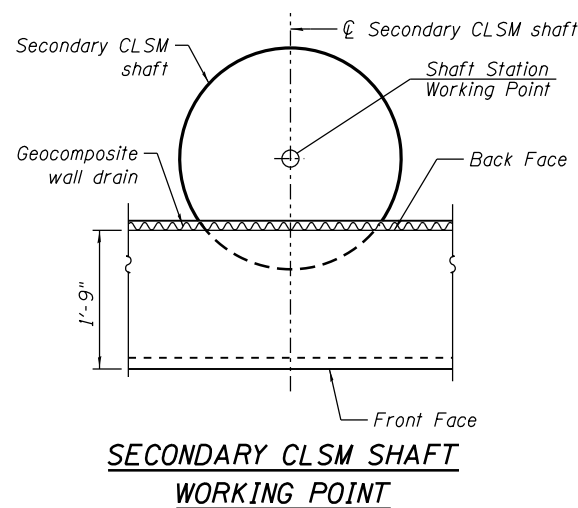
SOLDIER PILE WALL SECTIONS, DETAILS AND BILL OF MATERIAL
RETAINING WALL 29 (STRUCTURE NO. 016-2017)

SHEET NO. S2-06 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	305
				CONTRACT NO. 60X95
ILLINOIS FED. AID PROJECT				

SECONDARY CLSM SHAFT LAYOUT

Shaft	Station at Working Point	Offset	Top of Shaft Elev.	Bottom of Wall Elev.	Shaft Diameter	Bottom of Shaft Elev.	Shaft Length
S1	6345+75.05	36'-4 5/8" Rt.	581.80	574.30	4'-6"	571.30	10'- 6"
S2	6345+82.50	36'-3 1/2" Rt.	582.22	574.26	4'-6"	571.26	10'- 11 1/2"
S3	6345+89.94	36'-2 1/2" Rt.	582.60	574.22	4'-6"	571.22	11'- 4 1/2"
S4	6345+97.39	36'-1 5/8" Rt.	582.96	574.18	4'-6"	571.18	11'- 9 1/2"
S5	6346+04.84	36'-0 7/8" Rt.	583.29	574.14	4'-6"	571.14	12'- 2"
S6	6346+12.29	36'-0 1/4" Rt.	583.59	574.10	4'-6"	571.10	12'- 6"
S7	6346+19.74	35'-11 3/4" Rt.	583.86	574.06	4'-6"	571.06	12'- 9 1/2"
S8	6346+27.19	35'-11 3/8" Rt.	584.10	574.02	4'-6"	571.02	13'- 1"
S9	6346+34.64	35'-11 1/8" Rt.	584.31	573.98	4'-6"	570.98	13'- 4"
S10	6346+42.08	35'-11 1/8" Rt.	584.49	573.94	4'-6"	570.94	13'- 6 1/2"
S11	6346+49.53	35'-11 1/8" Rt.	584.65	573.90	4'-6"	570.90	13'- 9"
S12	6346+56.98	35'-11 1/4" Rt.	584.77	573.86	4'-6"	570.86	13'- 11"
S13	6346+64.43	35'-11 1/2" Rt.	584.87	573.82	4'-6"	570.82	14'- 0 1/2"
S14	6346+71.88	35'-11 7/8" Rt.	584.93	573.78	4'-6"	570.78	14'- 2"
S15	6346+79.33	36'-0 3/8" Rt.	584.97	573.74	4'-6"	570.74	14'- 3"
S16	6346+86.78	36'-1 1/8" Rt.	584.98	573.70	4'-6"	570.70	14'- 3 1/2"
S17	6346+94.23	36'-1 7/8" Rt.	584.96	573.66	4'-6"	570.66	14'- 3 1/2"
S18	6347+01.67	36'-2 3/4" Rt.	584.91	573.62	4'-6"	570.62	14'- 3 1/2"
S19	6347+09.12	36'-3 7/8" Rt.	584.83	573.58	4'-6"	570.58	14'- 3"



NOTES - CLSM MATERIAL FOR SECONDARY CLSM SHAFTS:

The CLSM shall be according to Section 1019, except where modified as follows:

- The mix design shall be proposed by the Contractor. The mix design shall meet the following criteria.

Parameter	Value
Flow	≥ 7 in. (178mm)
Air Content	15-25%
Compressive Strength at 28 days	≥ 200 psi (1380 kPa)

- Test data submitted with the proposed mix design shall indicate compressive strength at 1, 2, 3, 7, 14, and 28 days. Test data indicating strength at 180 days is optional.
- The proposed mix design will be verified by the Engineer with a trial batch and mockup. The Contractor shall notify the Engineer and the Engineer of Record (EOR) the schedule of the trial batch and mockup construction.
- The trial batch shall be of a volume sufficient to construct the mockup and accommodate all required testing. The Engineer waives the right to require a 180 day compressive strength test.
- The mockup shall be constructed according to a typical section of CLSM secondary shaft as detailed in the contract plans. The mockup construction shall be completed with the Contractor's equipment and methods intended for construction.
- The Contractor shall determine when the mockup is to be evaluated for its suitability to drilling for permanent anchoring and cutting for adjacent piling. The Contractor's determination shall be based on strength or other measures approved by the Engineer. If more than two trial batches and mockups are required by the Engineer to satisfactorily assess constructability, the additional trial batches and mockups will be paid for under Article 109.04.
- Verification of a mix design and mockup shall in no manner be construed as acceptance of any mixture produced or work done. Tests performed at the jobsite will determine if a mix design can meet specifications, and the work will be accepted as specified in the contract plans and specifications.

NOTES:

- Construction of the Secondary CLSM Shafts shall be in accordance with Section 522 of the Standard Specifications. Cost of constructing Secondary CLSM Shafts and chipping CLSM for the construction of wall facing, geocomposite wall drain, concrete gutter & parapet shall be included with Secant Lagging.
- Contractor shall determine the appropriate curing time prior to initiating drilling for soldier piles to minimize any cracking and damage to the Secondary CLSM Shafts. Contractor shall be responsible for repairing or shoring any damaged areas encountered during excavation. Contractor shall not begin excavation until Secondary CLSM Shafts attains min. compressive strength of 200 psi.
- Contractor shall maintain structural integrity of Secondary CLSM Shafts and shall take precautions not to damage Secondary CLSM Shafts during the construction. If structural integrity of the Secondary CLSM shaft is compromised then Contractor shall make necessary repairs immediately. Cost shall be included with Secant Lagging.

016-2017-W60X95-SHT-07-Secondary Shaft.dgn



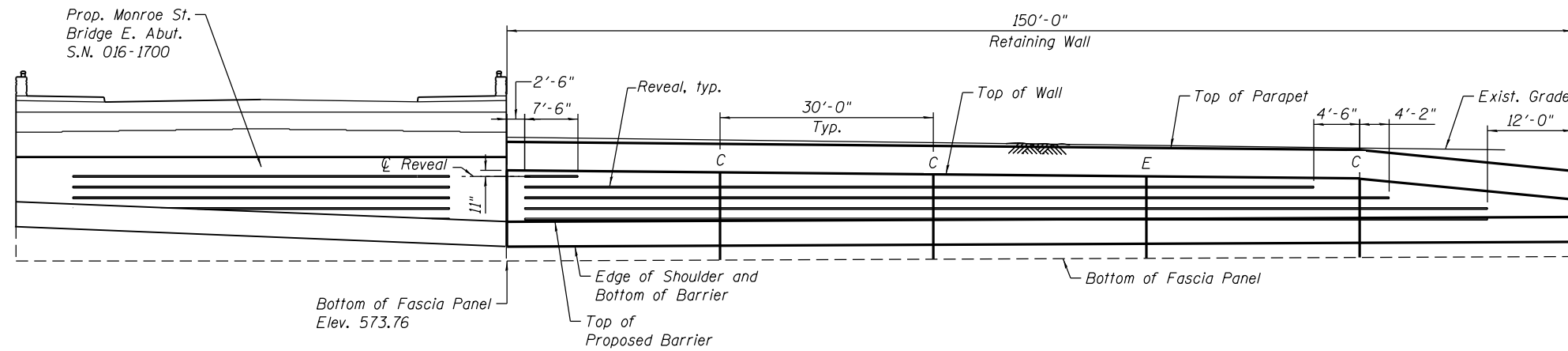
USER NAME = keserovic	DESIGNED - MK	REVISED
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PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

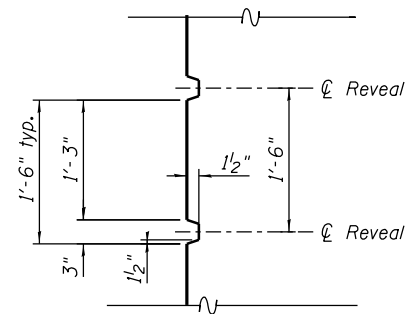
**SECONDARY CLSM SHAFT DETAILS
RETAINING WALL 29 (STRUCTURE NO. 016-2017)**

SHEET NO. S2-07 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	306
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				



ELEVATION



TYPICAL REVEAL DETAIL

NOTES:

1. Reveals shall not be paid separately and shall be considered included with the cost of Concrete Structures.
2. All lengths are measured along the F.F. of Fascia Panel.

016-2017-W60X95-SHT-08-arch.dgn



USER NAME = keserovicm	DESIGNED - MK	REVISED
	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARCHITECTURAL DETAILS
RETAINING WALL 29 (STRUCTURE NO. 016-2017)**

SHEET NO. S2-08 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	307
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X95	



wangeng@wangeng.com
1145 N Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

BORING LOG 29-RWB-01

WEI Job No.: 1100-04-01

Client: AECOM
Project: Circle Interchange Reconstruction
Location: Section 17, T39N, R14E of 3rd PM

Datum: NAVD 88
Elevation: 591.82 ft
North: 1899679.29 ft
East: 1171674.90 ft
Station: 6346+29.02
Offset: 46.47 RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
599.8	Black SANDY LOAM, trace slag --FILL--														
	Dense to very dense, brown SANDY GRAVEL --FILL--	1		8	16	NP	12			9		0	0	0.25	24
		2		4	13	NP	8			10		0	0	0.66	25
		3		5	24	NP	10			11		0	0	0.41	26
		4		1	2		16			12		0	1	0.25	26
		5		0	0		27			13		0	0	0.33	27
		6		0	0		27			14		0	0	0.41	27
		7		0	0		27								
		8		0	0		27								

--LL%=34, PL%=17--
--%Gravel=10.0--
--%Sand=13.8--
--%Silt=47.8--
--%Clay=28.5--
--A-6 (11)--

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	06-17-2014	Complete Drilling	06-17-2014
Drilling Contractor	Wang Testing Services	Drill Rig	D-50 TMR [78%]
Driller	R&J	Logger	S. Woods
Checked by	C. Marin	Depth to Water	NA
Drilling Method	2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion	While Drilling	Rotary wash
		At Completion of Drilling	mud in the borehole
		Time After Drilling	NA
		Depth to Water	NA

The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.



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BORING LOG 29-RWB-01

WEI Job No.: 1100-04-01

Client: AECOM
Project: Circle Interchange Reconstruction
Location: Section 17, T39N, R14E of 3rd PM

Datum: NAVD 88
Elevation: 591.82 ft
North: 1899679.29 ft
East: 1171674.90 ft
Station: 6346+29.02
Offset: 46.47 RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
	--%Clay=17.6-- --A-4 (5)--														
530.1	Very stiff, gray SILTY CLAY to SILTY CLAY LOAM, trace gravel														
		15		0	2		23			19		4	7	2.30	18
		16		2	3		27								
		17		3	4		21								
		18		4	9		16								
		19		0	0		27								
		20		0	0		27								
		21		0	0		27								
		22		0	0		27								
		23		0	0		27								
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		104		0	0		27								
		105		0	0		27								
		106		0	0		27								
		107		0	0		27								
		108		0	0		27								
		109		0	0										

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
593.2	5-inch thick, black LOAM --TOPSOIL-- Medium dense, gray and black SANDY LOAM, trace slag --FILL--	0-5	1	5 8 8	NP	14							
590.6	Loose to very dense, brown SANDY GRAVEL --FILL--	5-10	2	9 13 31	NP	10							
	--HARD DRILLING--	10-15	3	7 8 50/4"	NP	11							
583.1	Very soft to medium stiff, gray CLAY to SILTY CLAY, trace gravel	15-20	4	25 5 4	NP	6							
		20-25	5	0 0 0	0.41 B	26							
		25-30											
		30-35											
		35-40											
		40-45											
		45-50											
		50-55											
		55-60											
		60-65											
		65-70											
		70-75											
		75-80											
		80-85											
		85-90											
		90-95											
		95-100											

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling . . . 06-16-2014 . . .	Complete Drilling . . . 06-16-2014 . . .	While Drilling . . . Rotary wash . . .	
Drilling Contractor . . . Wang Testing Services . . .	Drill Rig B-57 TMR [100%]	At Completion of Drilling . . . mud in the borehole . . .	
Driller . . . N&K . . .	Logger . . . A. Happel . . .	Time After Drilling . . . NA . . .	
Drilling Method . . . 3.25" HSA to 11' . . .	mud rotary thereafter, boring . . .	Depth to Water . . . NA . . .	
. . . backfilled upon completion . . .		The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
593.2	5-inch thick, black LOAM --TOPSOIL-- Medium dense, gray and black SANDY LOAM, trace slag --FILL--	0-5	1	5 8 8	NP	14							
590.6	Loose to very dense, brown SANDY GRAVEL --FILL--	5-10	2	9 13 31	NP	10							
	--HARD DRILLING--	10-15	3	7 8 50/4"	NP	11							
583.1	Very soft to medium stiff, gray CLAY to SILTY CLAY, trace gravel	15-20	4	25 5 4	NP	6							
		20-25	5	0 0 0	0.41 B	26							
		25-30											
		30-35											
		35-40											
		40-45											
		45-50											
		50-55											
		55-60											
		60-65											
		65-70											
		70-75											
		75-80											
		80-85											
		85-90											
		90-95											
		95-100											

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling . . . 06-16-2014 . . .	Complete Drilling . . . 06-16-2014 . . .	While Drilling . . . Rotary wash . . .	
Drilling Contractor . . . Wang Testing Services . . .	Drill Rig B-57 TMR [100%]	At Completion of Drilling . . . mud in the borehole . . .	
Driller . . . N&K . . .	Logger . . . A. Happel . . .	Time After Drilling . . . NA . . .	
Drilling Method . . . 3.25" HSA to 11' . . .	mud rotary thereafter, boring . . .	Depth to Water . . . NA . . .	
. . . backfilled upon completion . . .		The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

NOTE:
 1. Station and offset are measured along @ NB C-D Road.

016-Z017-W60X95-SHT-10-Boring



USER NAME = keserovic	DESIGNED - MK	REVISED
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PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BORING LOGS - II
 RETAINING WALL 29 (STRUCTURE NO. 016-2017)

SHEET NO. S2-10 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	309
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				

Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)	Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)
		Medium dense, gray GRAVELLY SAND; dry --FILL--			1	14 11 6	NP	5						9	1 1 2	0.33 B	26
	576.6	Very soft to medium stiff, gray CLAY to SILTY CLAY, trace gravel			2	2 2 2	0.41 B	23						10	3 3 4	0.41 B	25
					3	1 1 1	0.41 B	26						11	1 2 2	0.41 B	26
					4	1 1 2	0.41 B	25						12	1 2 3	0.41 B	26
					5	1 1 2	0.25 B	25									
					6	1 1 2	0.41 B	25						13	2 2 2	0.16 B	26
					7	1 1 1	0.41 B	17									
					8	0 1 2	0.33 B	26						14	3 4 4	0.90 B	22

GENERAL NOTES

Begin Drilling 06-24-2014 Complete Drilling 06-24-2014
 Drilling Contractor Wang Testing Services Drill Rig B-57 TMR [100%]
 Driller N&K Logger A. Happel Checked by C. Marin
 Drilling Method 2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion

WATER LEVEL DATA

While Drilling 62.00 ft
 At Completion of Drilling mud in the borehole
 Time After Drilling NA
 Depth to Water NA
 The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)	Profile	Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blows/6 in)	Qu (tsf)	Moisture Content (%)
		--%Clay=46.5-- --A-7-6 (23)--															
	537.9	Stiff to very stiff, gray SILTY CLAY, trace gravel			15	3 4 6	2.87 B	17						19	8 21 21	NP	13
		Dense, gray SANDY LOAM, trace gravel; moist															
		Boring terminated at 65.00 ft															
					16	5 5 7	1.56 B	22									
					17	6 8 11	3.53 B	21									
					18	3 4 4	1.23 B	41									
	522.9	Stiff, gray CLAY to SILTY CLAY, trace gravel --LL%=41, PL%=19-- --%Gravel=0.3-- --%Sand=1.6-- --%Silt=51.6--															

GENERAL NOTES

Begin Drilling 06-24-2014 Complete Drilling 06-24-2014
 Drilling Contractor Wang Testing Services Drill Rig B-57 TMR [100%]
 Driller N&K Logger A. Happel Checked by C. Marin
 Drilling Method 2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion

WATER LEVEL DATA

While Drilling 62.00 ft
 At Completion of Drilling mud in the borehole
 Time After Drilling NA
 Depth to Water NA
 The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.

NOTE:
 1. Station and offset are measured along @ NB C-D Road.

016-Z017-W60X95-SHT-11-Boring



USER NAME = keserovic	DESIGNED - MK	REVISED
	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BORING LOGS - III
RETAINING WALL 29 (STRUCTURE NO. 016-2017)
 SHEET NO. S2-11 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	310
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	Sample No. recovery	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	Sample No. recovery	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	
578.1	6-inch thick ASPHALT --PAVEMENT--															
577.1	12-inch thick CONCRETE --PAVEMENT--															
	Dense, gray and white CRUSHED STONE --BASE COURSE--		1	22	20	10	NP				9	1	1	1	0.16	27
575.4	Medium stiff, brown and gray SILTY CLAY LOAM, trace gravel		2	2	1	2	0.74	22			10	1	2	2	0.25	26
			5	1	1	2										
573.1	Very soft to medium stiff, gray CLAY to SILTY CLAY, trace gravel		3	1	1	2	0.33	25			11	1	2	1	0.16	26
570.3	--Possible Wet SAND lens--		4	1	1	3	0.33	20			12	1	2	3	0.33	26
			5	2	2	2	0.66	15								
			6	1	1	2	0.41	24			13	1	2	3	0.49	20
			7	1	1	2	0.41	25								
			8	1	1	2	0.25	25			14	3	5	6	0.57	22

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	06-25-2014	Complete Drilling	06-25-2014
Drilling Contractor	Wang Testing Services	Drill Rig	B-57 TMR [100%]
Driller	N&K	Logger	A. Happel
Checked by	C. Marin	Time After Drilling	NA
Drilling Method	2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion	Depth to Water	NA
		The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	Sample No. recovery	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample No.	Sample No. recovery	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	
516.9	Dense, brown and gray SANDY LOAM, trace gravel --Moist--															
			15	4	5	6	0.33	20			19	16	24	25	NP	9
			45	1	1	2										
			45	1	1	2										
			50	5	8	12	3.44	21								
			55	4	6	7	2.38	25								
			60	3	4	4	0.98	41								

GENERAL NOTES		WATER LEVEL DATA	
Begin Drilling	06-25-2014	Complete Drilling	06-25-2014
Drilling Contractor	Wang Testing Services	Drill Rig	B-57 TMR [100%]
Driller	N&K	Logger	A. Happel
Checked by	C. Marin	Time After Drilling	NA
Drilling Method	2.25" SSA to 10', mud rotary thereafter, boring backfilled upon completion	Depth to Water	NA
		The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

NOTE:
 1. Station and offset are measured along @ NB C-D Road.

016-Z017-W60X95-SHT-12-Boring



USER NAME = keserovic	DESIGNED - MK	REVISED
	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BORING LOGS - IV
RETAINING WALL 29 (STRUCTURE NO. 016-2017)

SHEET NO. S2-12 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	311
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	



wangeng@wangeng.com
1145 N Main Street
Lombard, IL 60148
Telephone: 630 953-9928
Fax: 630 953-9938

BORING LOG 2054-B-04

WEI Job No.: 1100-04-01

Client: AECOM
Project: Circle Interchange Reconstruction
Location: Section 17, T39N, R14E of 3rd PM

Datum: NAVD 88
Elevation: 593.64 ft
North: 1899800.05 ft
East: 1171715.00 ft
Station: 6347+47.80
Offset: 87.93 RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
593.0	7-inch thick CONCRETE --PAVEMENT-- Construction debris --hard drilling, 1 to 12 feet-- --possible cobbles--	0								0					
	Drilled without sampling	5								5					
	Drilled without sampling	10								10					
581.6	Drilled without sampling	15								15					
	Drilled without sampling	20								20					

GENERAL NOTES				WATER LEVEL DATA			
Begin Drilling	08-24-2015	Complete Drilling	08-25-2015	While Drilling	Rotary wash		
Drilling Contractor	Wang Testing Services	Drill Rig	CME-55 TMR [85%]	At Completion of Drilling	Mud at 12 ft		
Driller	R&N	Logger	F. Bozga	Checked by	C. Marin		
Drilling Method	2.25" IDA HSA to 18', mud rotary thereafter, boring backfilled upon completion			Time After Drilling	NA		
		Depth to Water	NA			The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	



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BORING LOG 2054-B-04

WEI Job No.: 1100-04-01

Client: AECOM
Project: Circle Interchange Reconstruction
Location: Section 17, T39N, R14E of 3rd PM

Datum: NAVD 88
Elevation: 593.64 ft
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Station: 6347+47.80
Offset: 87.93 RT

Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)	Profile Elevation (ft)	SOIL AND ROCK DESCRIPTION	Depth (ft)	Sample Type	Sample No.	SPT Values (blw/6 in)	Qu (tsf)	Moisture Content (%)
530.1	Stiff to very stiff, gray SILTY CLAY LOAM, trace gravel	45		1	6 7 11	1.89 B		530.1		45		1	6 7 11	1.89 B	17
	Drilled without sampling	50		2	12 11 12	3.28 B		524.6 524.1	Gray GRAVELLY SAND; saturated Stiff, gray SILTY CLAY	50		2	12 11 12	3.28 B	16
	Drilled without sampling	55		3	4 4 6	1.31 B				55		3	4 4 6	1.31 B	28
	Drilled without sampling	60		4	16 25 45	NP		516.9	Very dense, gray, fine SAND, interbedded silt; wet	60		4	16 25 45	NP	14

GENERAL NOTES				WATER LEVEL DATA			
Begin Drilling	08-24-2015	Complete Drilling	08-25-2015	While Drilling	Rotary wash		
Drilling Contractor	Wang Testing Services	Drill Rig	CME-55 TMR [85%]	At Completion of Drilling	Mud at 12 ft		
Driller	R&N	Logger	F. Bozga	Checked by	C. Marin		
Drilling Method	2.25" IDA HSA to 18', mud rotary thereafter, boring backfilled upon completion			Time After Drilling	NA		
		Depth to Water	NA			The stratification lines represent the approximate boundary between soil types; the actual transition may be gradual.	

NOTE:

1. Station and offset are measured along @ NB C-D Road.



USER NAME = keserovic	DESIGNED - MK	REVISED
	CHECKED - ATB	REVISED
PLOT SCALE = N.T.S.	DRAWN - MK	REVISED
PLOT DATE = 7/11/2017	CHECKED - ATB	REVISED

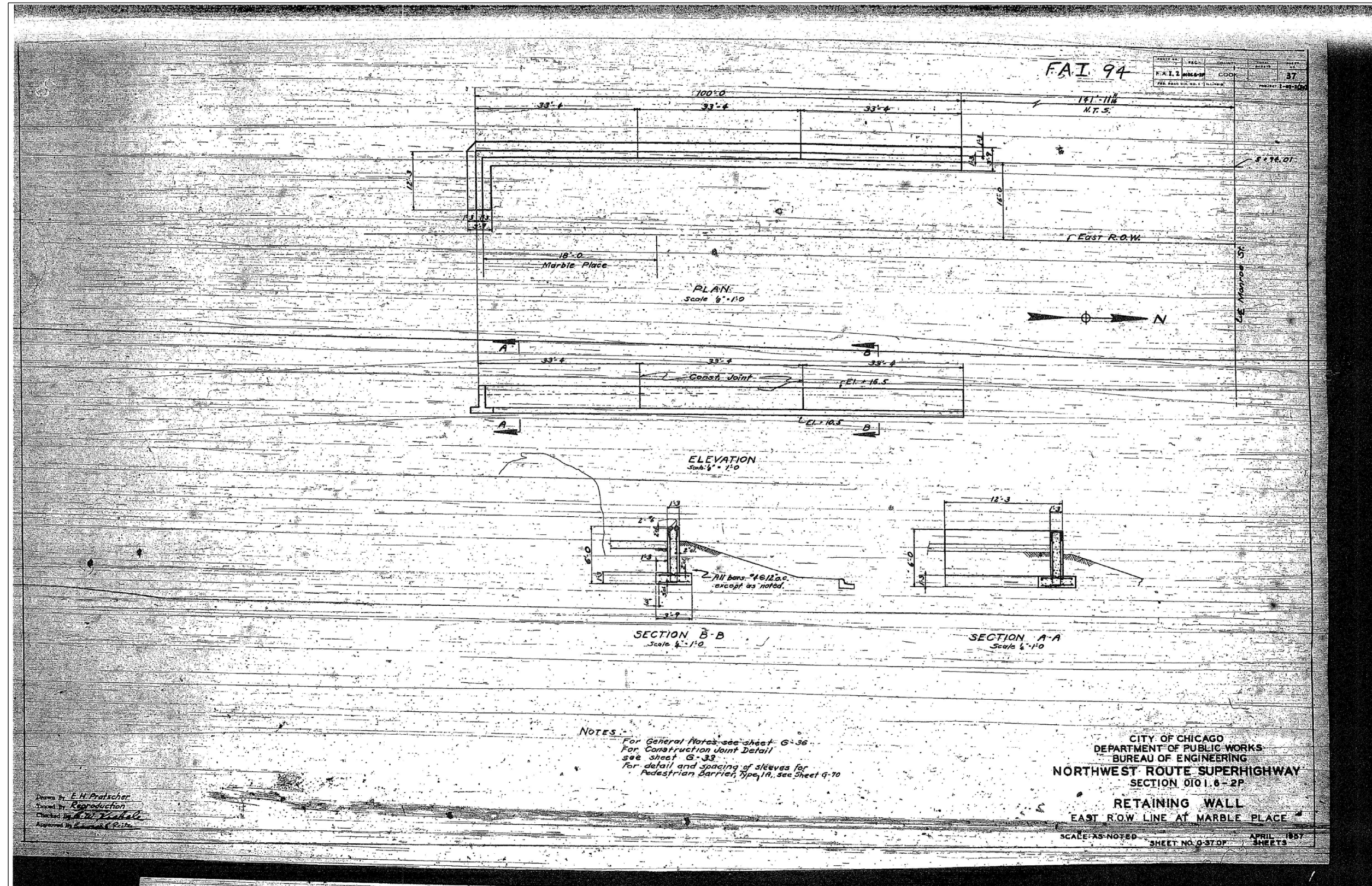
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS - V
RETAINING WALL 29 (STRUCTURE NO. 016-2017)

SHEET NO. S2-13 OF 14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	312
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				

FOR INFORMATION ONLY



Drawn by E.H. Pratscher
 Traced by Reproduction
 Checked by K.W. Kishel
 Approved by [Signature]

NOTES:
 For General Notes, see sheet G-36
 For Construction Joint Detail
 see sheet G-33
 For detail and spacing of sleeves for
 Pedestrian Barriers, Type 1A, see Sheet G-70

CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
 NORTHWEST ROUTE SUPERHIGHWAY
 SECTION 0101.6-2P
 RETAINING WALL
 EAST R.O.W. LINE AT MARBLE PLACE
 SCALE AS NOTED
 SHEET NO. 0-37 OF 37 SHEETS
 APRIL 1957

016Z017-W60X95-ExistingPlans-01.dgn



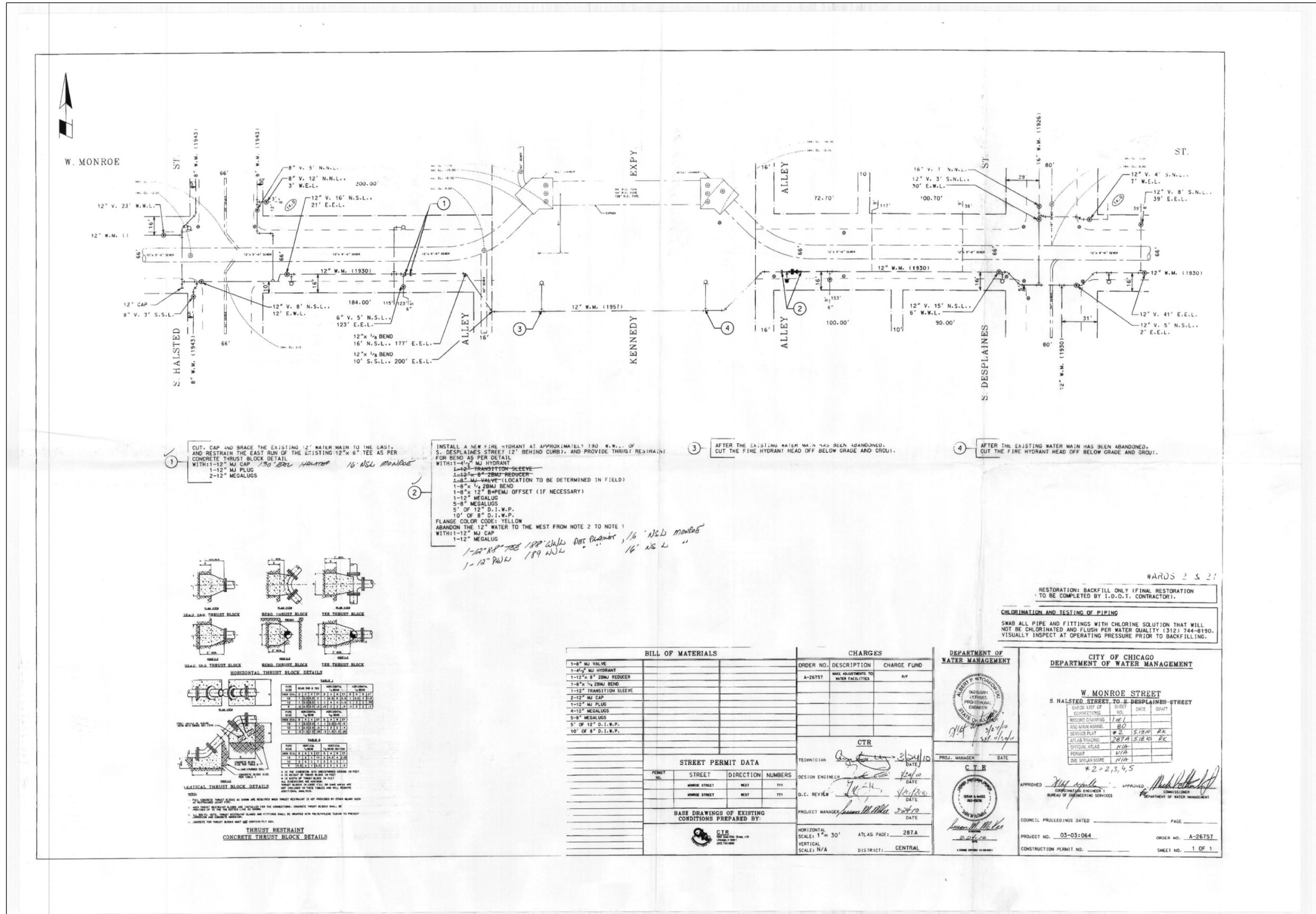
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PLOT DATE = 4/26/2017	CHECKED - ATB	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

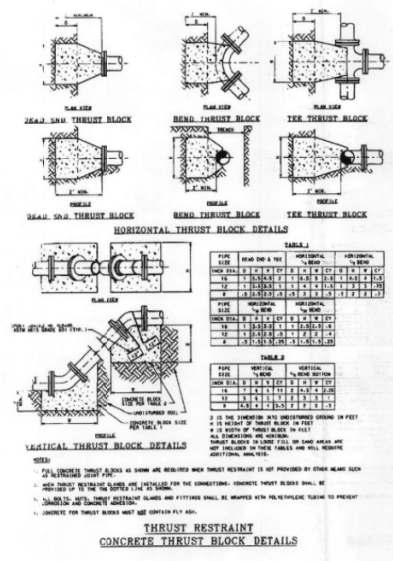
EXISTING PLANS I
 RETAINING WALL 29 (016-2017)
 SHEET NO. 1 OF 3 SHEETS

F.A.I. RTE. 90/94/290	SECTION	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 314
				CONTRACT NO. 60X95
ILLINOIS FED. AID PROJECT				

FOR INFORMATION ONLY



- 1 CUT, CAP AND BRACE THE EXISTING 12" WATER MAIN TO THE EAST, AND RESTRAIN THE EAST RUN OF THE EXISTING 12"x6" TEE AS PER CONCRETE THRUST BLOCK DETAIL WITH: 1-12" MJ CAP 1-12" MJ PLUG 2-12" MEGALUGS
- 2 INSTALL A NEW FIRE HYDRANT AT APPROXIMATELY 130' W.W.L. OF S. DESPLAINES STREET (2' BEHIND CURB), AND PROVIDE THRUST RESTRAINT FOR BEND AS PER DETAIL WITH: 1-4" MJ HYDRANT 1-12" TRANSITION SLEEVE 1-8" 2BMS REDUCER 1-8" MJ VALVE (LOCATION TO BE DETERMINED IN FIELD) 1-8"x 1/2" ZBMJ BEND 1-8"x 12" S-WPEMJ OFFSET (IF NECESSARY) 1-12" MEGALUG 5-8" MEGALUGS 5' OF 12" D.I.W.P. 10' OF 8" D.I.W.P. FLANGE COLOR CODE: YELLOW ABANDON THE 12" WATER TO THE WEST FROM NOTE 2 TO NOTE 1 WITH: 1-12" MJ CAP 1-12" MEGALUG
- 3 AFTER THE EXISTING WATER MAIN HAS BEEN ABANDONED, CUT THE FIRE HYDRANT HEAD OFF BELOW GRADE AND GROUT.
- 4 AFTER THE EXISTING WATER MAIN HAS BEEN ABANDONED, CUT THE FIRE HYDRANT HEAD OFF BELOW GRADE AND GROUT.



BILL OF MATERIALS		CHARGES	
1-8" MJ VALVE		ORDER NO.	CHARGE FUND
1-4" MJ HYDRANT		A-2675T	WATER FACILITIES
1-12" 8" ZBMJ REDUCER			
1-8" x 1/2" ZBMJ BEND			
1-12" TRANSITION SLEEVE			
2-12" MJ CAP			
1-12" MJ PLUG			
4-12" MEGALUGS			
5-8" MEGALUGS			
5' OF 12" D.I.W.P.			
10' OF 8" D.I.W.P.			

STREET PERMIT DATA	
PERMIT NO.	711
STREET	MONROE STREET
DIRECTION	WEST
NUMBERS	711

DEPARTMENT OF WATER MANAGEMENT

PROJ. MANAGER DATE

DESIGN ENGINEER DATE

D.C. REVIEWER DATE

PROJECT MANAGER DATE

HORIZONTAL SCALE: 1" = 30'

VERTICAL SCALE: N/A

ATLAS PAGE: 28TA

DISTRICT: CENTRAL

CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT

W. MONROE STREET S. HALSTED STREET TO S. DESPLAINES STREET

CHECK LIST OF SHEET DATE

RECORD DRAWING 1 of 1

ASSESSMENT 80

SERVICE PLAN 2 5/19/10

ATLAS TRACING 28TA 5/18/10

COPYRIGHT N/A

PERMIT N/A

DISTRICT N/A

* 2 = 2, 3, 4, 5

APPROVED [Signature] DATE

CONSTRUCTION PERMIT NO. A-2675T

SHEET NO. 1 OF 1

0162017-W60X95-ExistingPlans-02.dgn



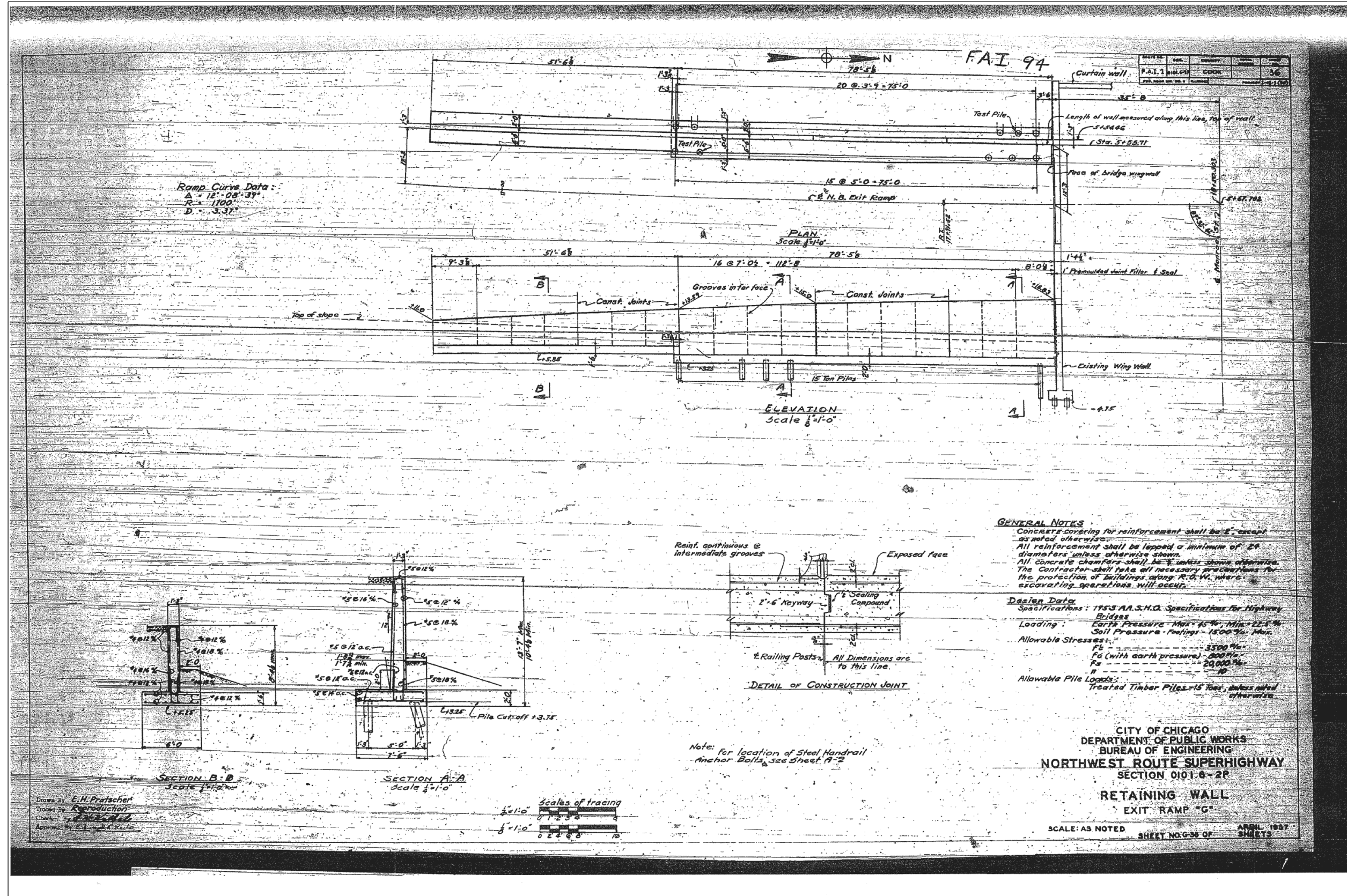
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PLOT DATE = 4/26/2017	DRAWN - MRK	REVISED
	CHECKED - ATB	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS II
RETAINING WALL 29 (016-2017)
SHEET NO. 2 OF 3 SHEETS

F.A.I. RTE. 90/94/290	SECTION	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 315
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X95	

FOR INFORMATION ONLY



USER NAME = floresg
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 CHECKED - ATB
 PLOT SCALE = N.T.S.
 DRAWN - MRK
 PLOT DATE = 4/26/2017
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 CHECKED - ATB

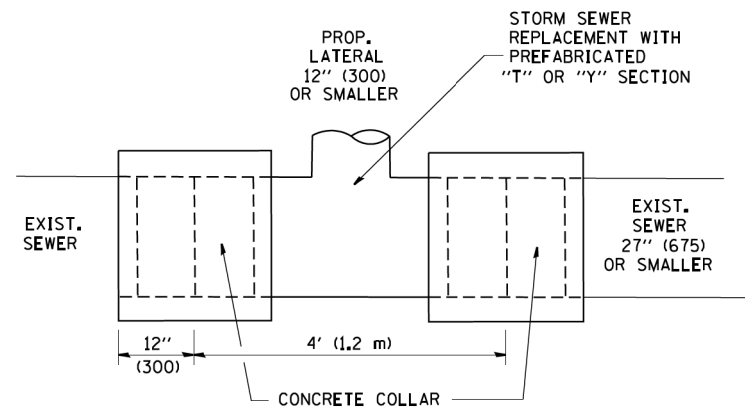
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS III
 RETAINING WALL 29 (016-2017)

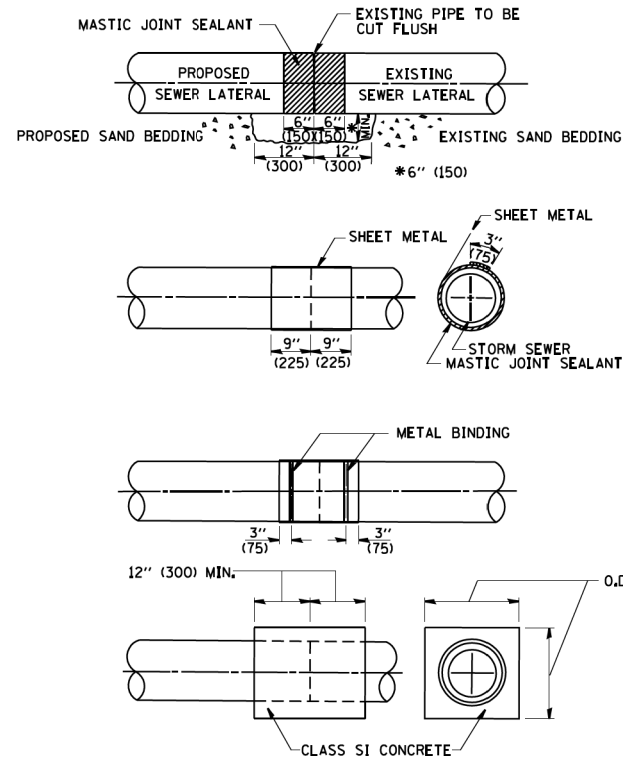
SHEET NO. 3 OF 3 SHEETS

F.A.I. RTE. 90/94/290	SECTION	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 316
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

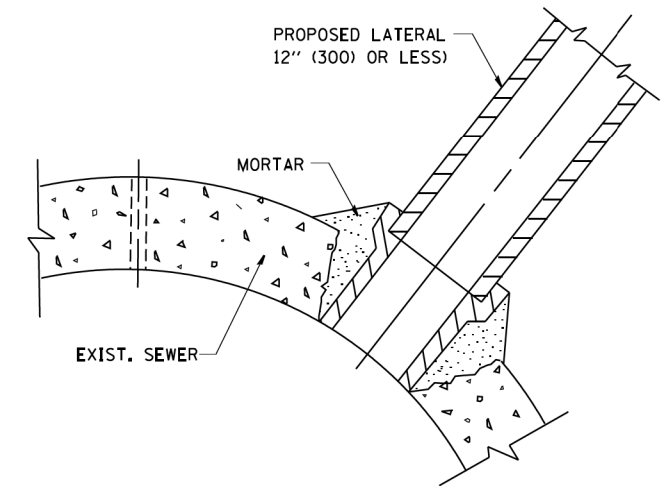


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

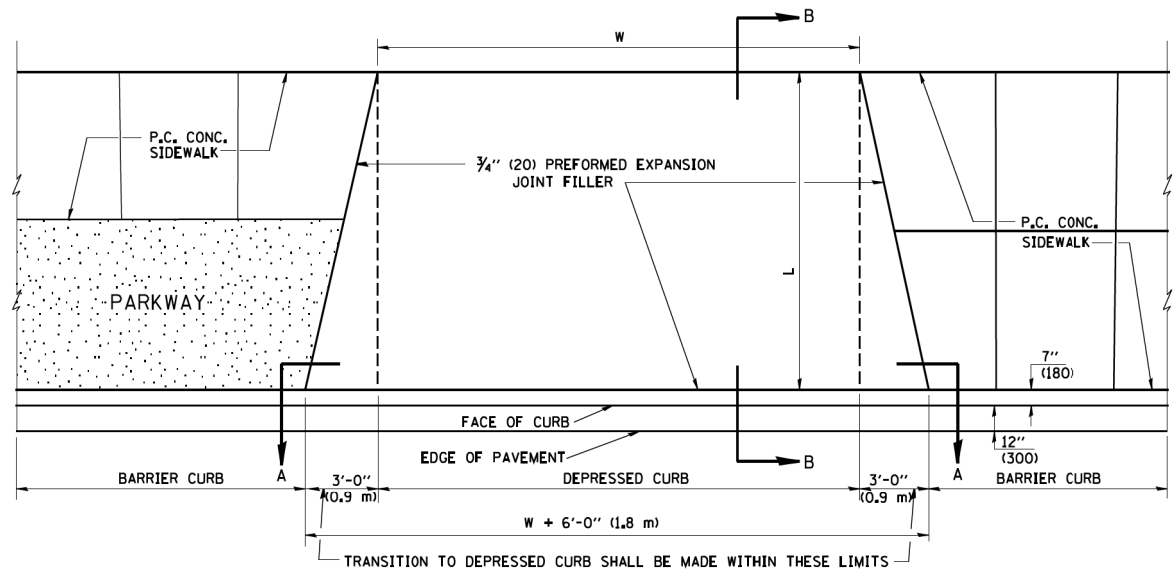
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

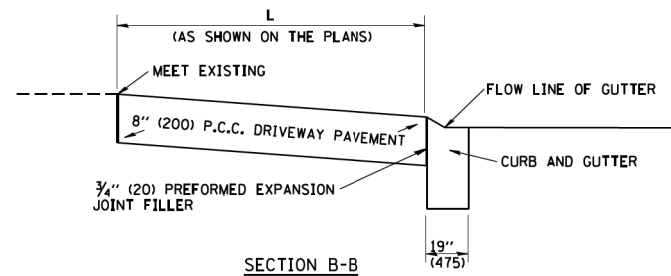
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	CONTRACT NO. 60X95		
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - R. SHAH 10-25-94		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								
		DATE - 07-25-90	REVISED - R. SHAH 06-12-96										

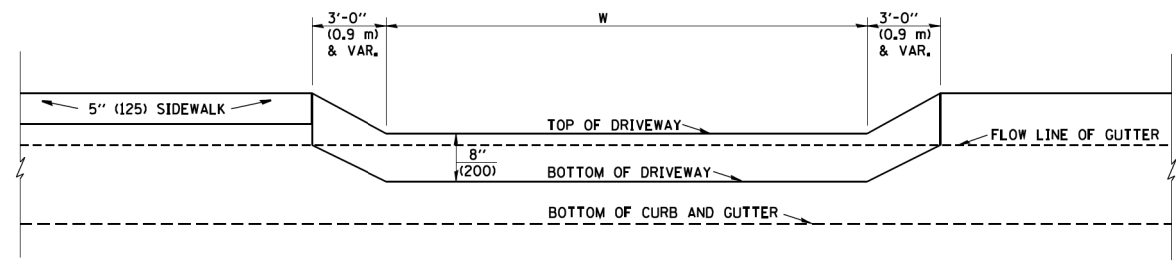


PLAN VIEW

- NOTES:
1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
 2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
 3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
 4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
 5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

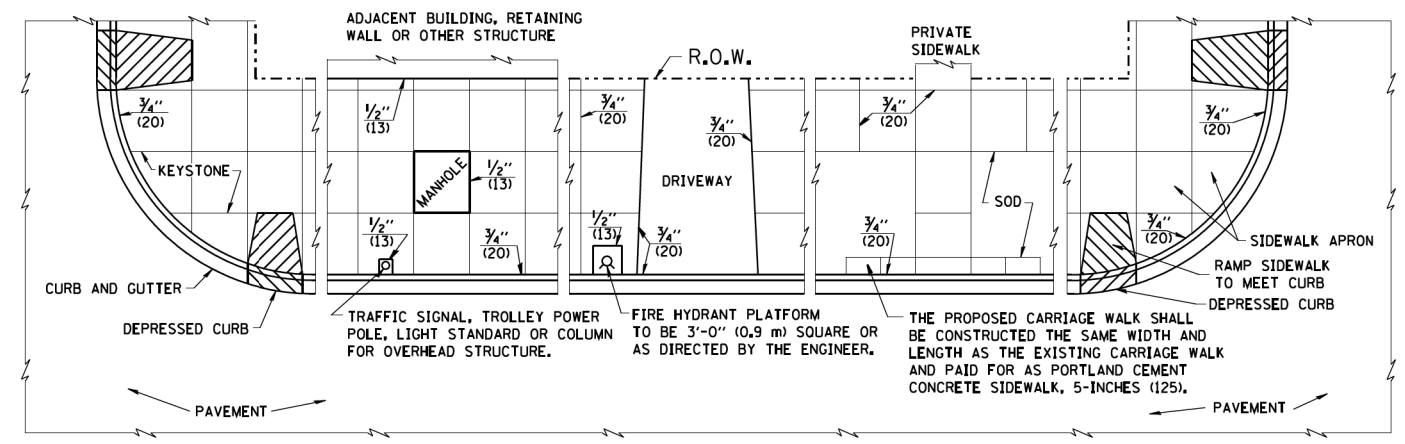


SECTION B-B



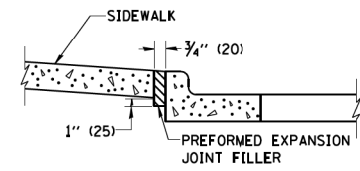
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



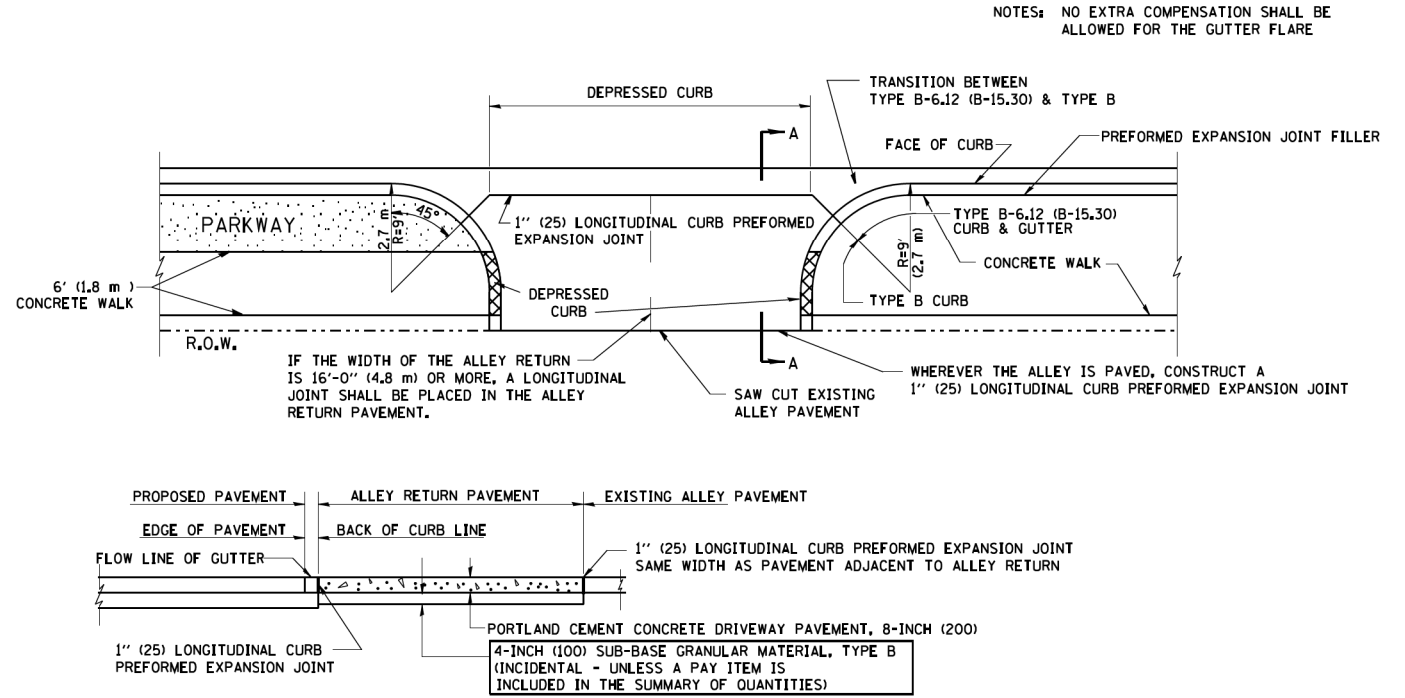
NOTES:

1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK, WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

PORTLAND CEMENT CONCRETE SIDEWALK DETAILS



SECTION A-A

ALLEY RETURN DETAIL

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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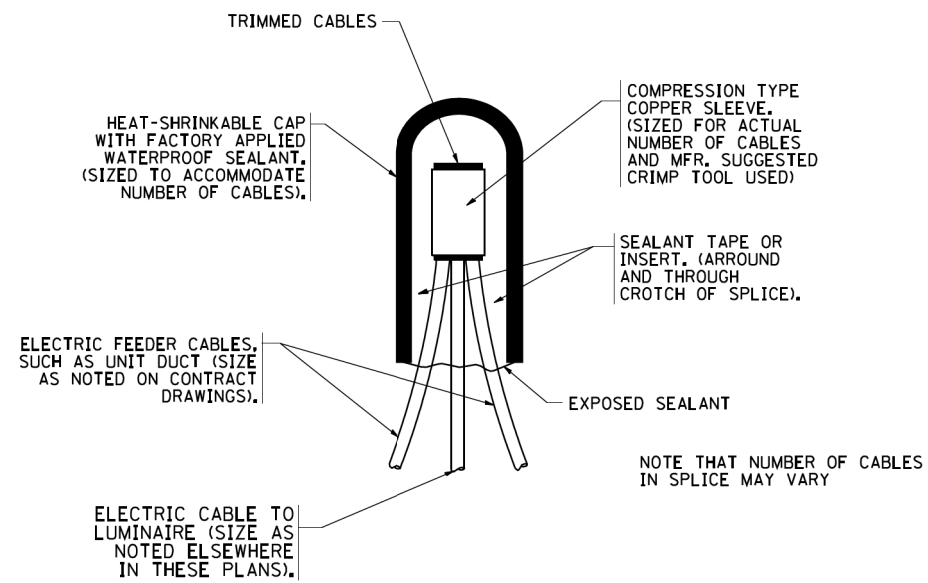
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DRAWN -
CHECKED -
DATE - 06-13-90

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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

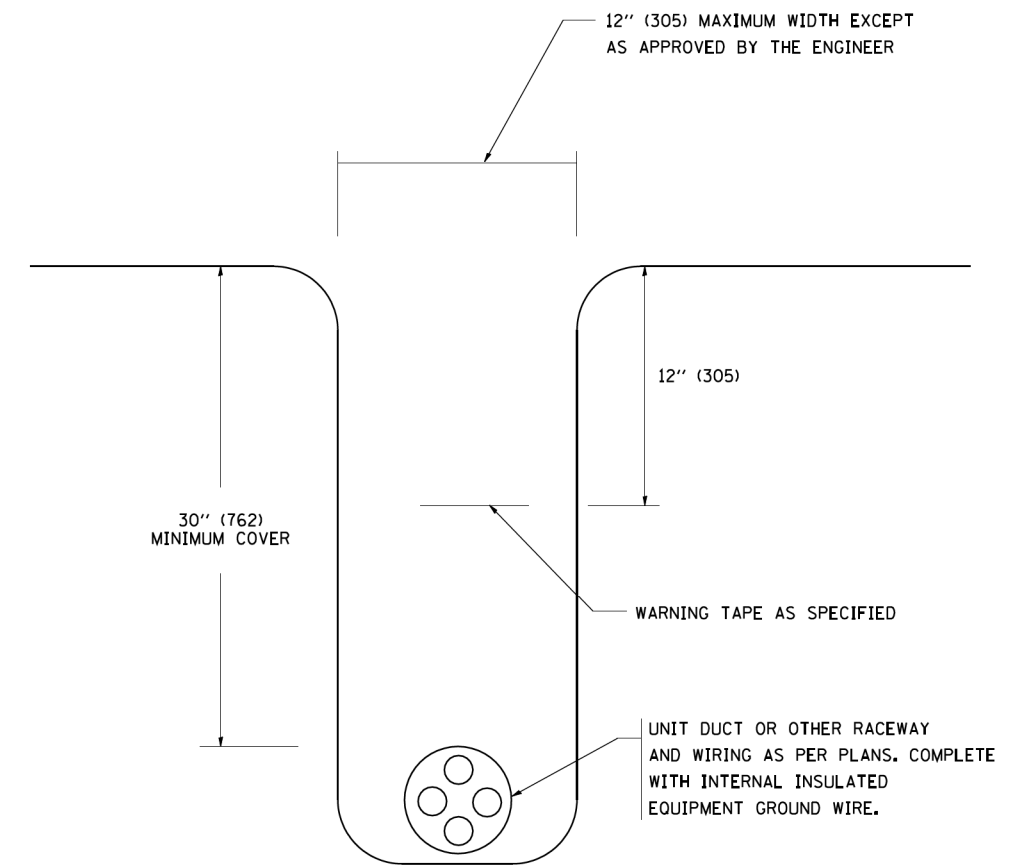
CITY OF CHICAGO
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. R.T.E. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 318
BD400-03 (BD-17)		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

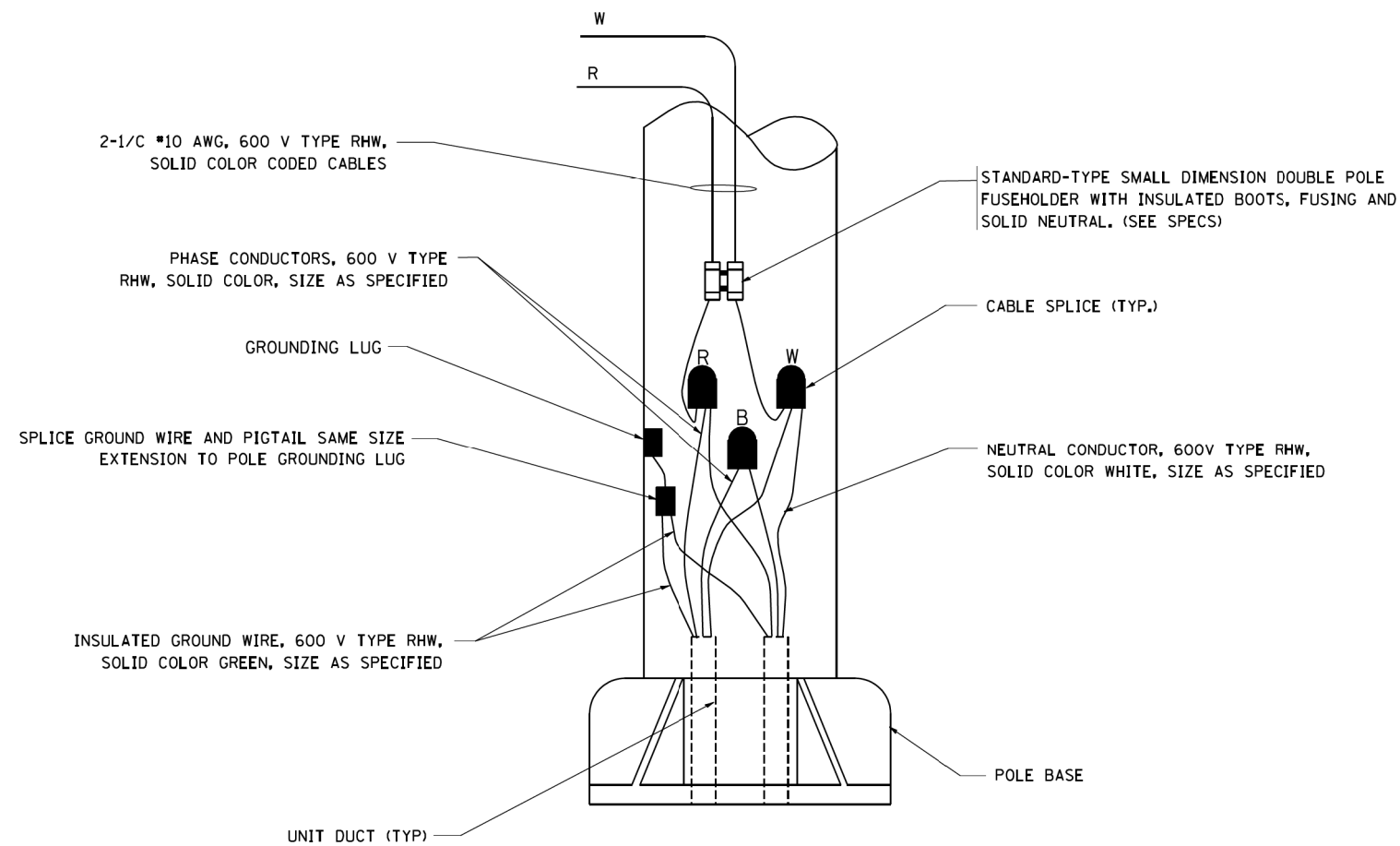


TYPICAL SPLICE DETAIL
N.T.S.

NOTE THAT NUMBER OF CABLES IN SPLICE MAY VARY



TYPICAL WIRING IN TRENCH DETAIL
N.T.S.



POLE WIRING DETAIL
N.T.S.

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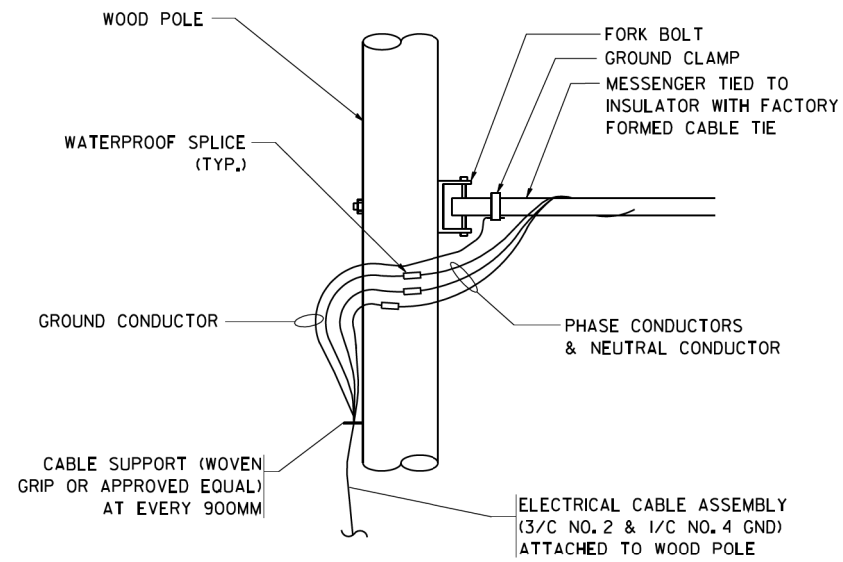
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DATE -

REVISED - 08-08-03
REVISED -
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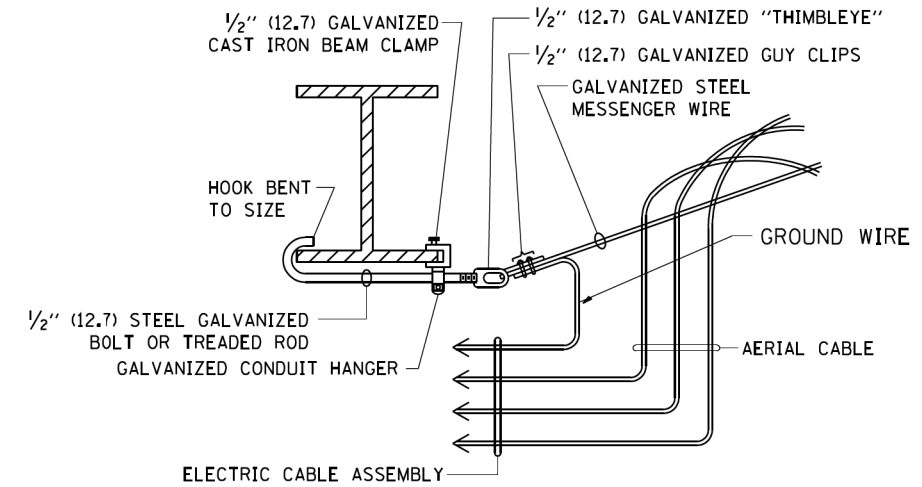
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISC. ELECTRICAL DETAILS
SHEET A
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	320
BE-702		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



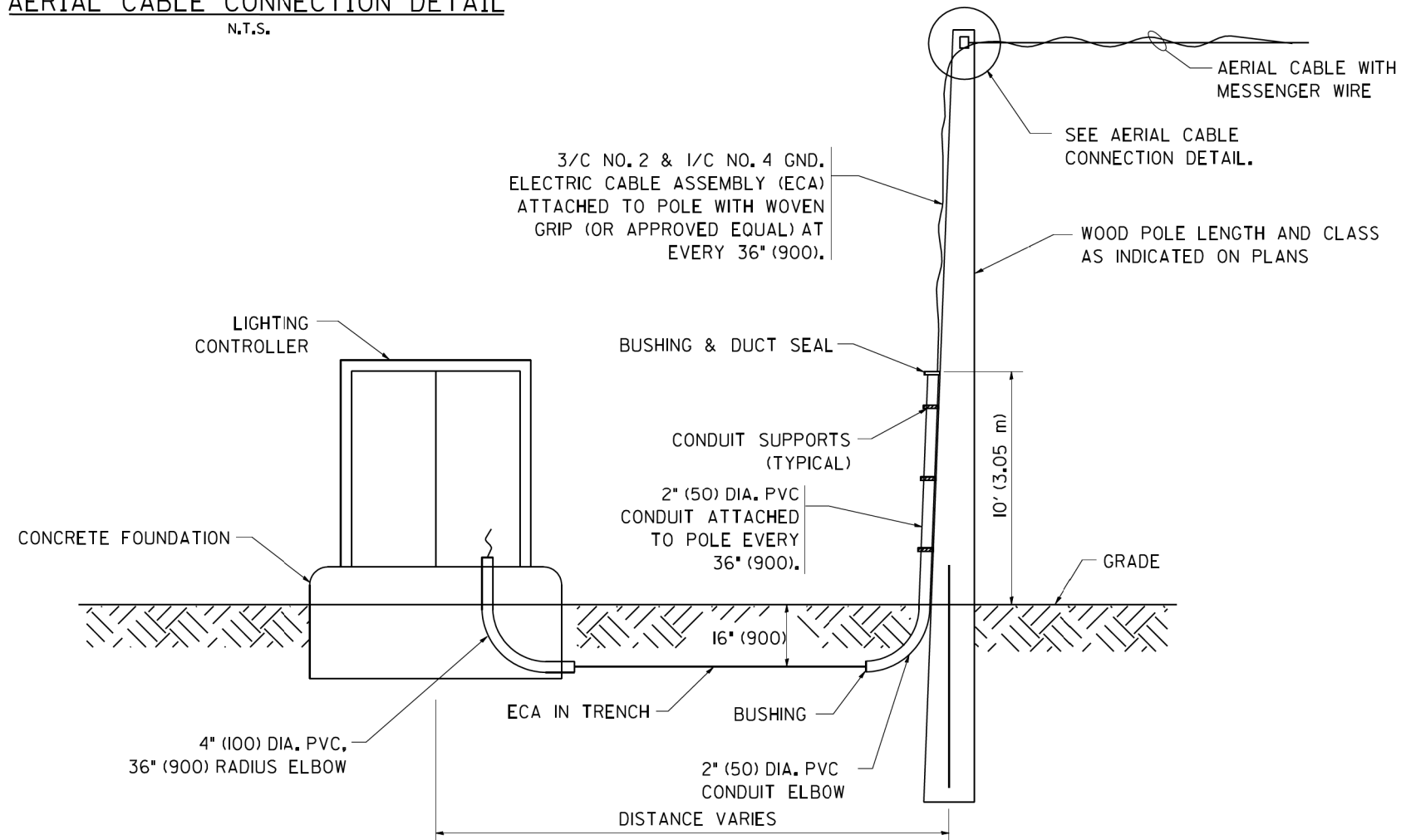
AERIAL CABLE CONNECTION DETAIL
N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE
NOT TO SCALE

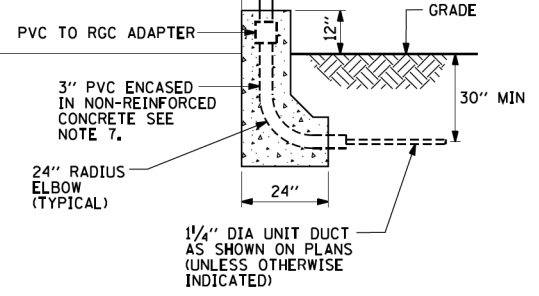
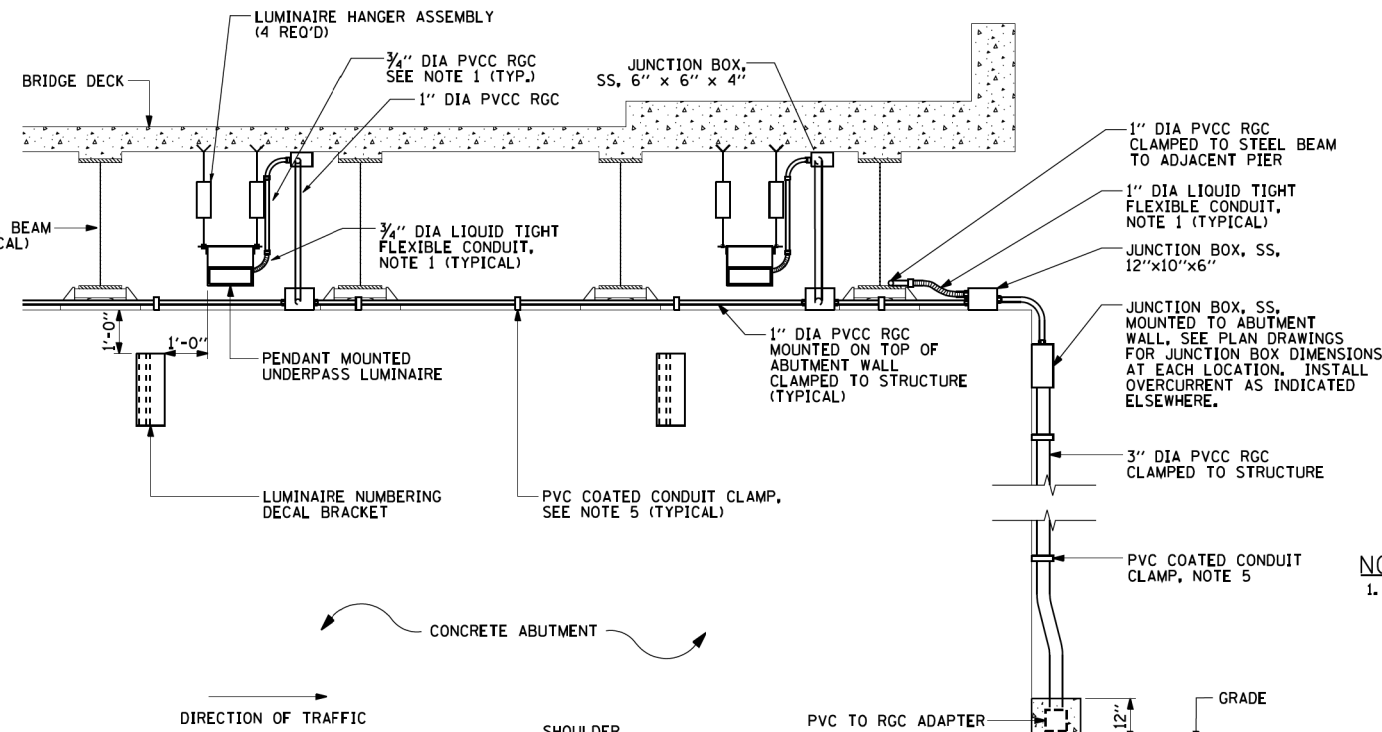
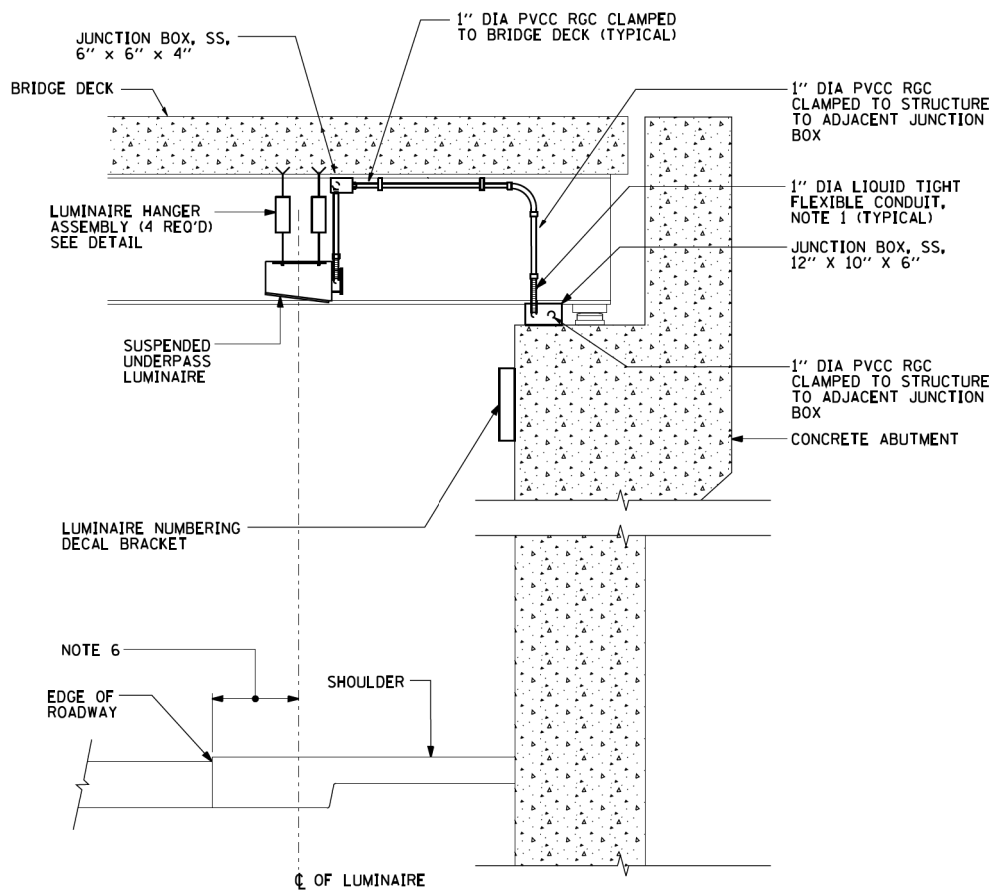
NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.

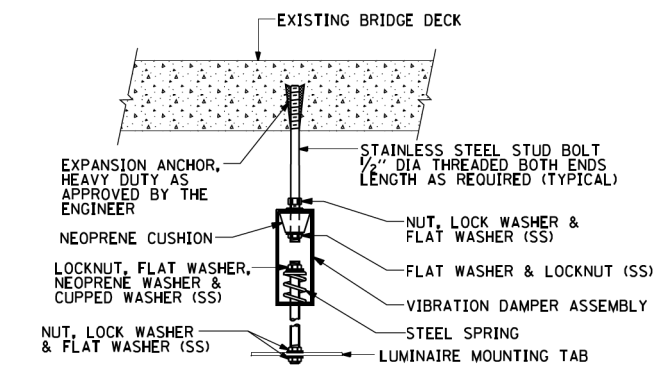


WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL
N.T.S.

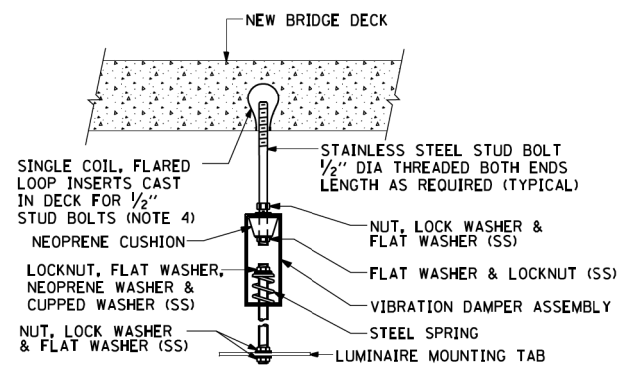
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	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 60X95			
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									



- NOTES:**
- LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN, PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT, LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT 3/4" DIA. CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE COST OF UNDERPASS LUMINAIRE INSTALLATION.
 - SEE UNDERPASS LIGHTING PLANS FOR INSTALLATION LOCATION OF UNDERPASS LIGHTING LUMINAIRES.
 - THE CONTRACTOR SHALL USE APPROVED SINGLE COIL FLARED LOOP INSERTS WHEN SUSPENDED MOUNTING AN UNDERPASS LUMINAIRE TO A NEW BRIDGE DECK. THE FLARED LOOP INSERTS MUST BE CAST INTO THE CONCRETE DECK. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND COORDINATING THE INSERT LOCATIONS FOR MOUNTING THE UNDERPASS LIGHTING SYSTEM AS SHOWN ON THE PLANS WITH THE BRIDGE DECK CONTRACTOR. SEE DETAIL.
 - THE UNDERPASS LUMINAIRE HANGER ASSEMBLY COMPLETE WITH HEAVY DUTY ANCHORS/INSERTS AND ALL APPLICABLE HARDWARE SHALL BE INCLUDED IN THE COST OF THE UNDERPASS LUMINAIRE PAY ITEM.
 - SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
 - ALL UNDERPASS LUMINAIRES MUST BE CENTERED IN THE BEAM SPACE AS INDICATED ON THE PLANS UNLESS OTHERWISE DIRECTED BY THE ENGR. LUMINAIRE SETBACK SHALL BE AS INDICATED IN PLANS FOR EACH SPECIFIC UNDERPASS
 - THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
 - ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

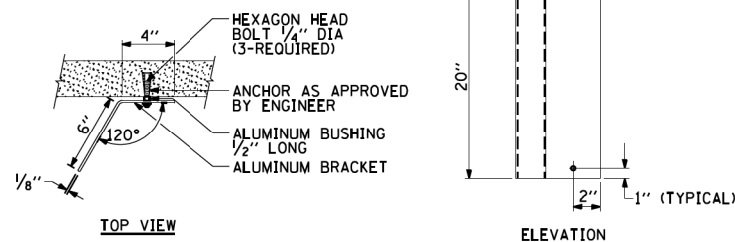


EXISTING BRIDGE DECK INSTALLATION



NEW BRIDGE DECK INSTALLATION

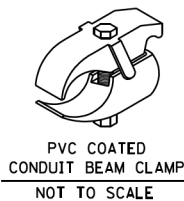
TYPICAL LUMINAIRE HANGER ASSEMBLY DETAILS



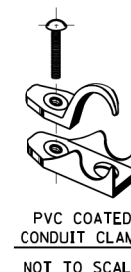
TOP VIEW

ELEVATION

LUMINAIRE NUMBERING DECAL BRACKET NOT TO SCALE



PVC COATED CONDUIT BEAM CLAMP NOT TO SCALE



PVC COATED CONDUIT CLAMP NOT TO SCALE

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 PLOT DATE = 1/4/2008

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

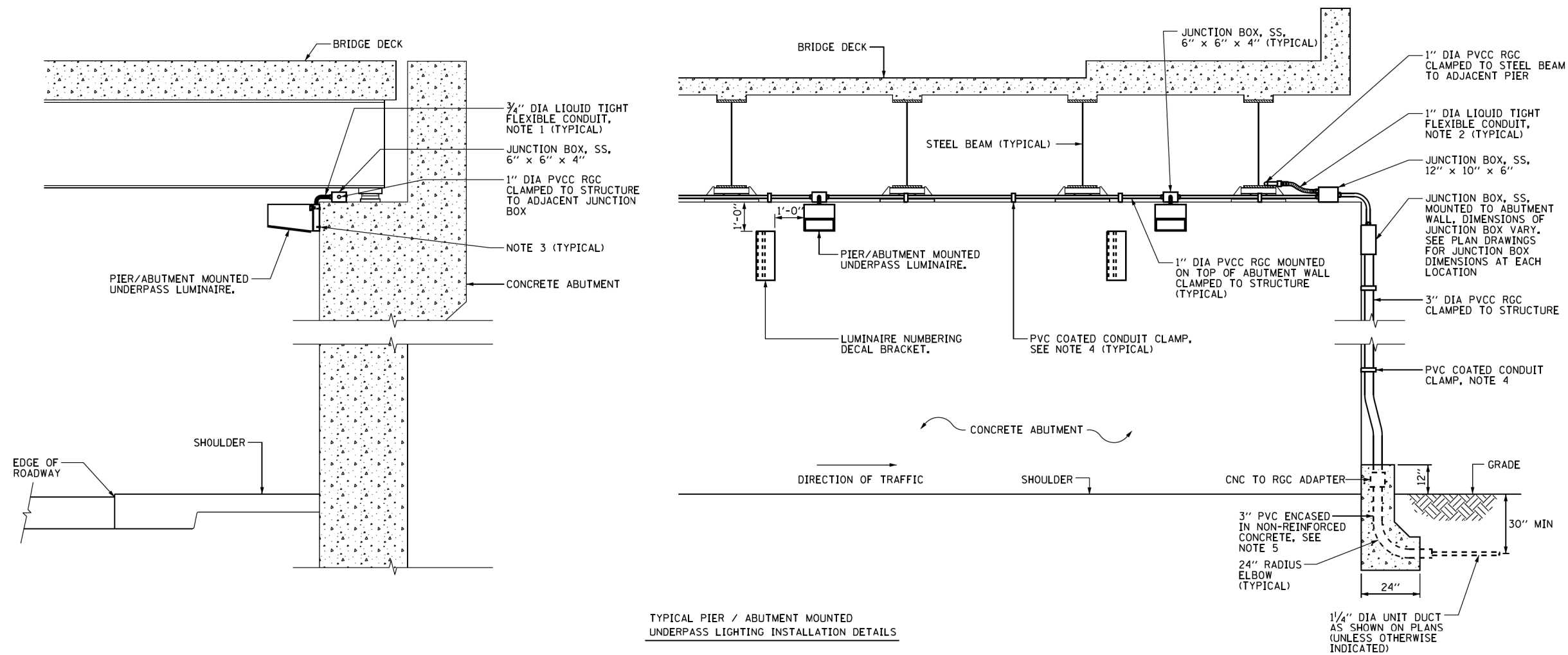
REVISED - 12-12-05
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

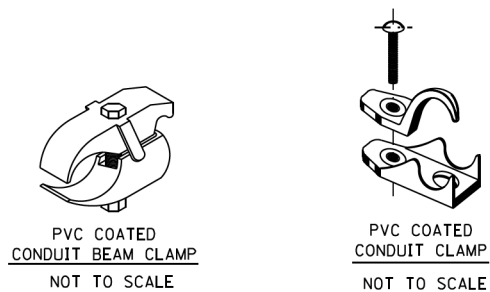
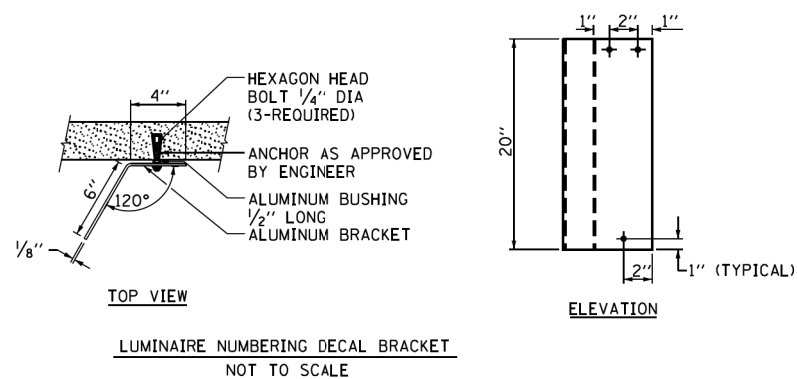
SUSPENDED MOUNT UNDERPASS
 LUMINAIRE INSTALLATION DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION 2014-016R&B BE-900	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 322
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		CONTRACT NO. 60X95		



TYPICAL PIER / ABUTMENT MOUNTED UNDERPASS LIGHTING INSTALLATION DETAILS



NOTES:

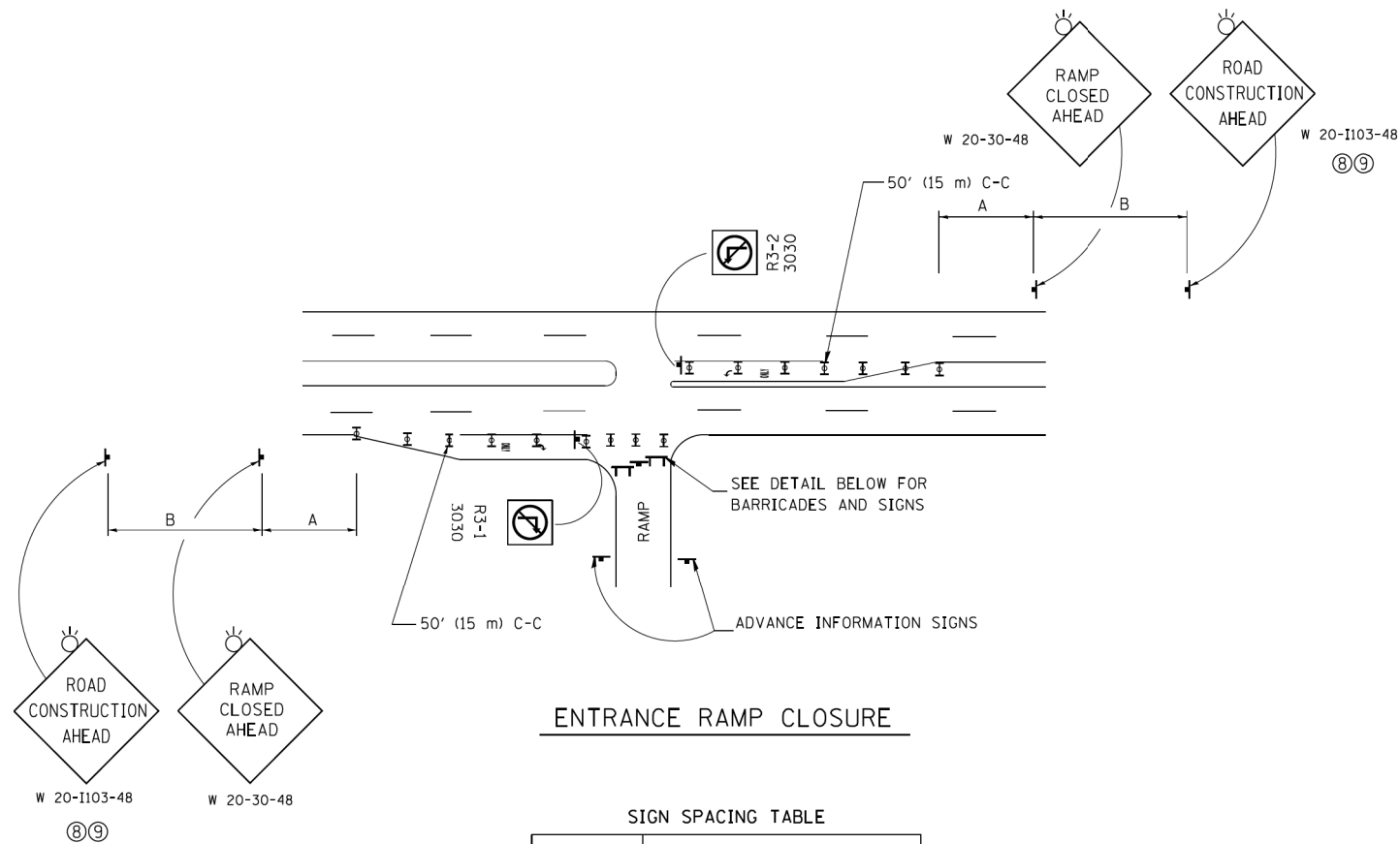
1. LIQUID TIGHT FLEXIBLE METAL CONDUIT, MAXIMUM LENGTH 6'-0", TYPICAL FOR EACH INSTANCE AS SHOWN, PROVIDE PVC COATED RIGID GALVANIZED STEEL CONDUIT AS REQUIRED NOT TO EXCEED 6'-0" OF FLEXIBLE LIQUID TIGHT METAL CONDUIT. LIQUID TIGHT FLEXIBLE METAL CONDUIT WILL BE INCLUDED IN THE COST OF THE CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED PAY ITEM EXCEPT THAT THE COST OF THE 3/4" DIA. RIGID STEEL CONDUIT AND 3/4" DIA. FLEXIBLE CONDUIT SHALL BE INCLUDED IN THE LUMINAIRE INSTALLATION.
2. UNDERPASS LUMINAIRE MOUNTED TO FACE OF PIER OR ABUTMENT WALL. MOUNTING HEIGHT OF 1" BELOW THE TOP OF PIER OR ABUTMENT WALL TYPICAL FOR ALL PIER/ABUTMENT MOUNTED UNDERPASS LUMINAIRES UNLESS OTHERWISE NOTED.
3. EXPANSION ANCHOR, POWDER ACTUATED FASTENERS WILL NOT BE ALLOWED. EXPANSION ANCHOR MUST BE SIZED IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
4. SECURE THE CONDUIT WITH PVC COATED CONDUIT CLAMPS OR CONDUIT BEAM CLAMPS AS SHOWN AT 5'-0" INTERVALS FOR LATERALS AND WITHIN 2'-0" MAXIMUM FROM ANY JUNCTION BOX, FLEXIBLE CONDUIT, OR CHANGE IN DIRECTION. ALL PVC COATED CONDUIT CLAMPS OR BEAM CLAMPS SHALL BE INCLUDED WITH THE COST OF THE "CONDUIT ATTACHED TO STRUCTURE, OF THE CORRESPONDING DIA., GALVANIZED STEEL, PVC COATED" PAY ITEM.
5. THE CONCRETE ENCASED CONDUIT TRANSITION SHALL BE INCLUDED IN THE COST OF THE GALVANIZED RIGID STEEL CONDUIT PAY ITEMS.
6. ALL CONDUIT ATTACHED TO STRUCTURE SHALL BE PVC COATED RIGID STEEL CONDUIT (PVCC RGC) TYPICAL.

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Default	PLOT SCALE = 100.000 / 1in	CHECKED -	REVISED -
	PLOT DATE = 11/17/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PIER / ABUTMENT MOUNTED UNDERPASS LUMINAIRE INSTALLATION DETAILS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.1 RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	323
BE-902		CONTRACT NO. 60X95		
ILLINOIS FED. AID PROJECT				

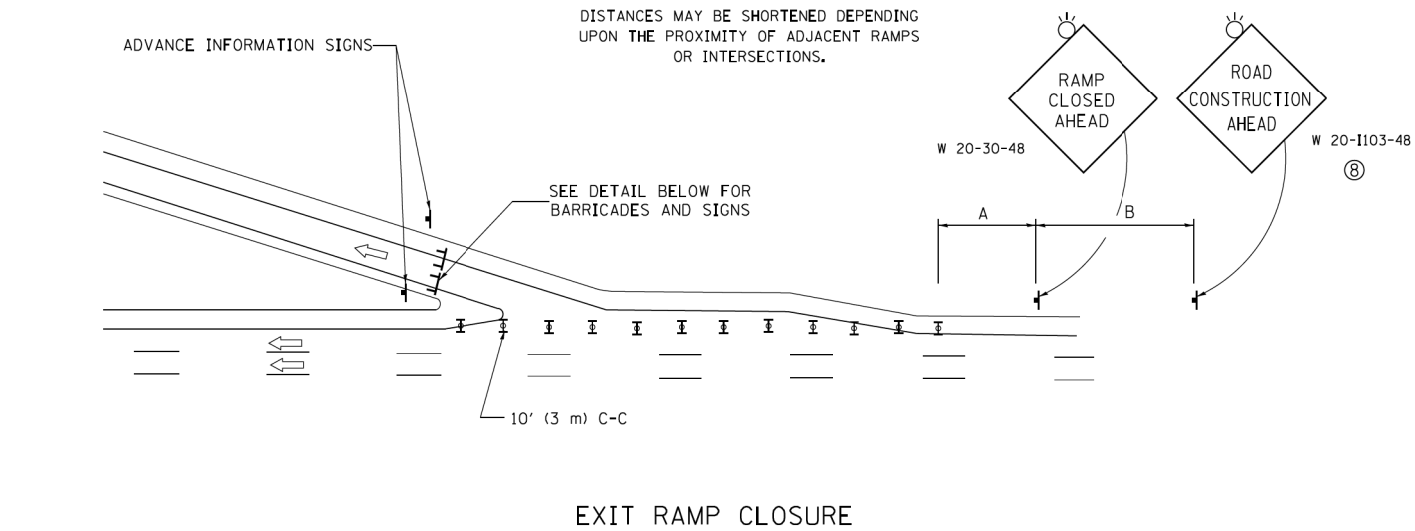


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

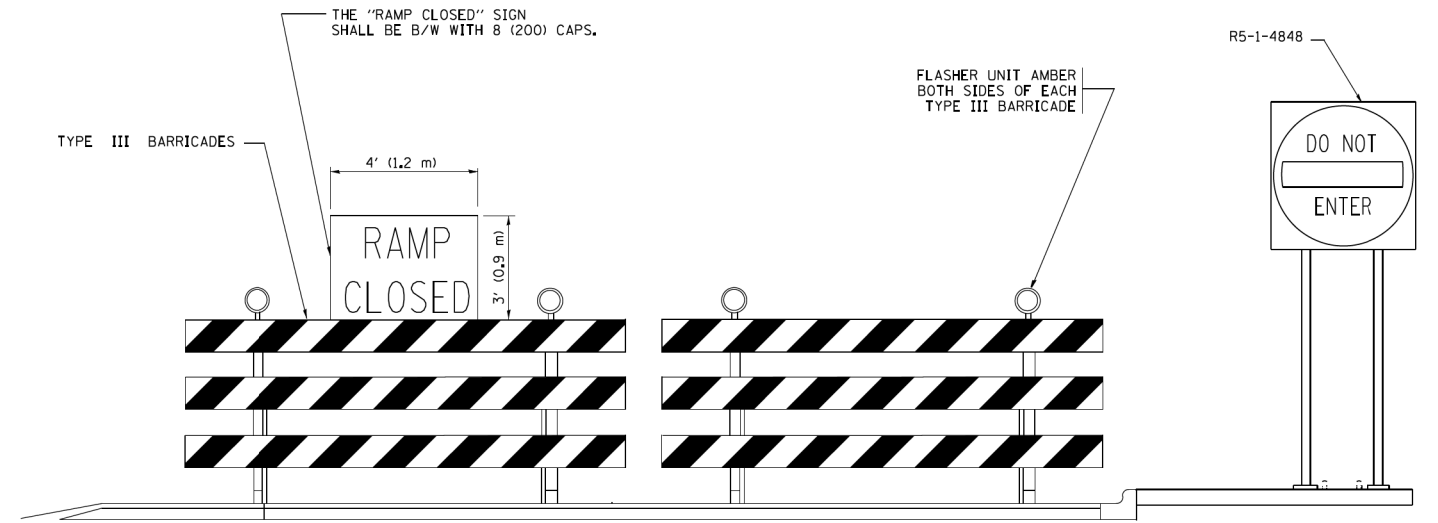
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

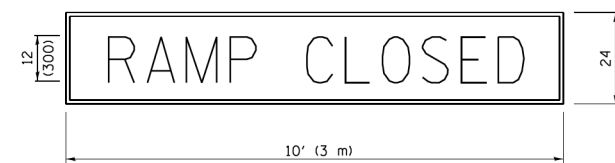
SYMBOLS

- ▬ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ▬ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

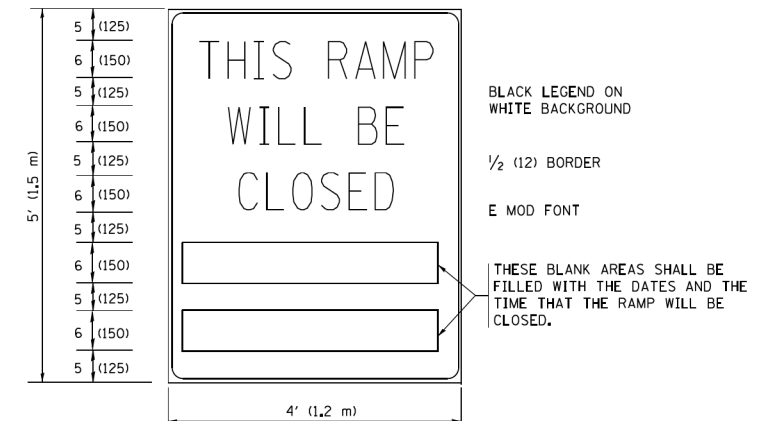
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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ca:\pwwork\pwwork\footemj\00100315\ca00.dgn		DRAWN -	REVISED - SPB 01-07
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	PLOT DATE = 7/8/2013	DATE - 02-83	REVISED - MD 06-13

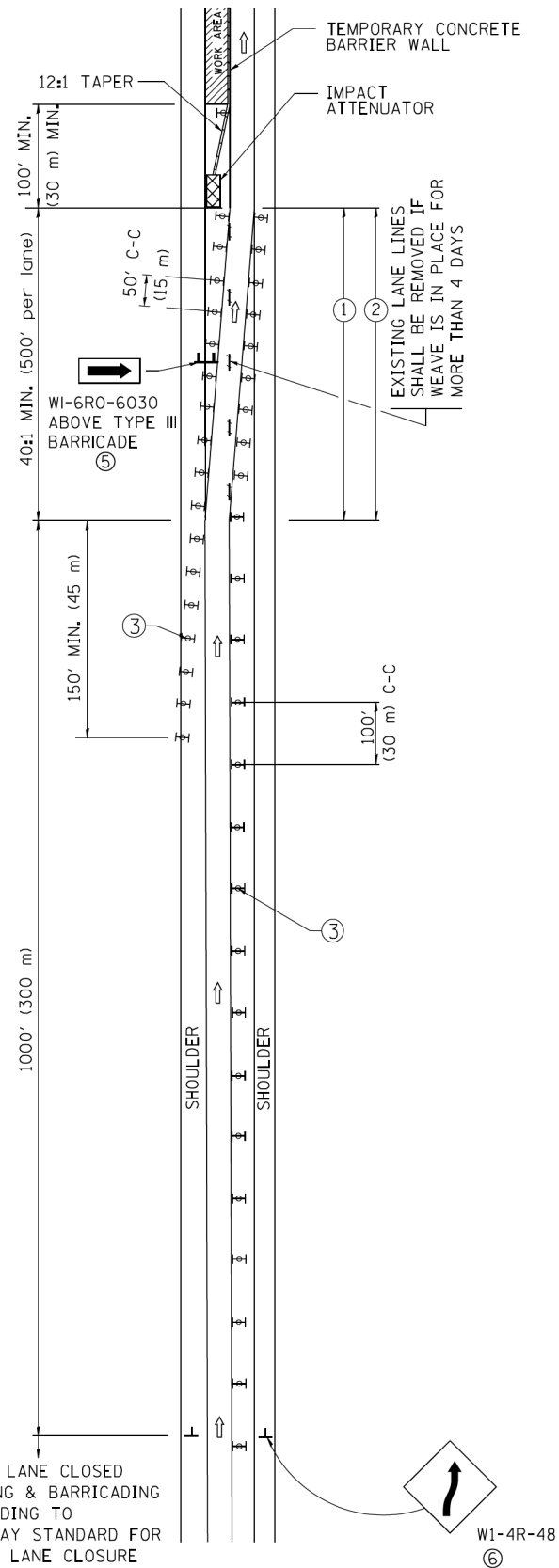
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

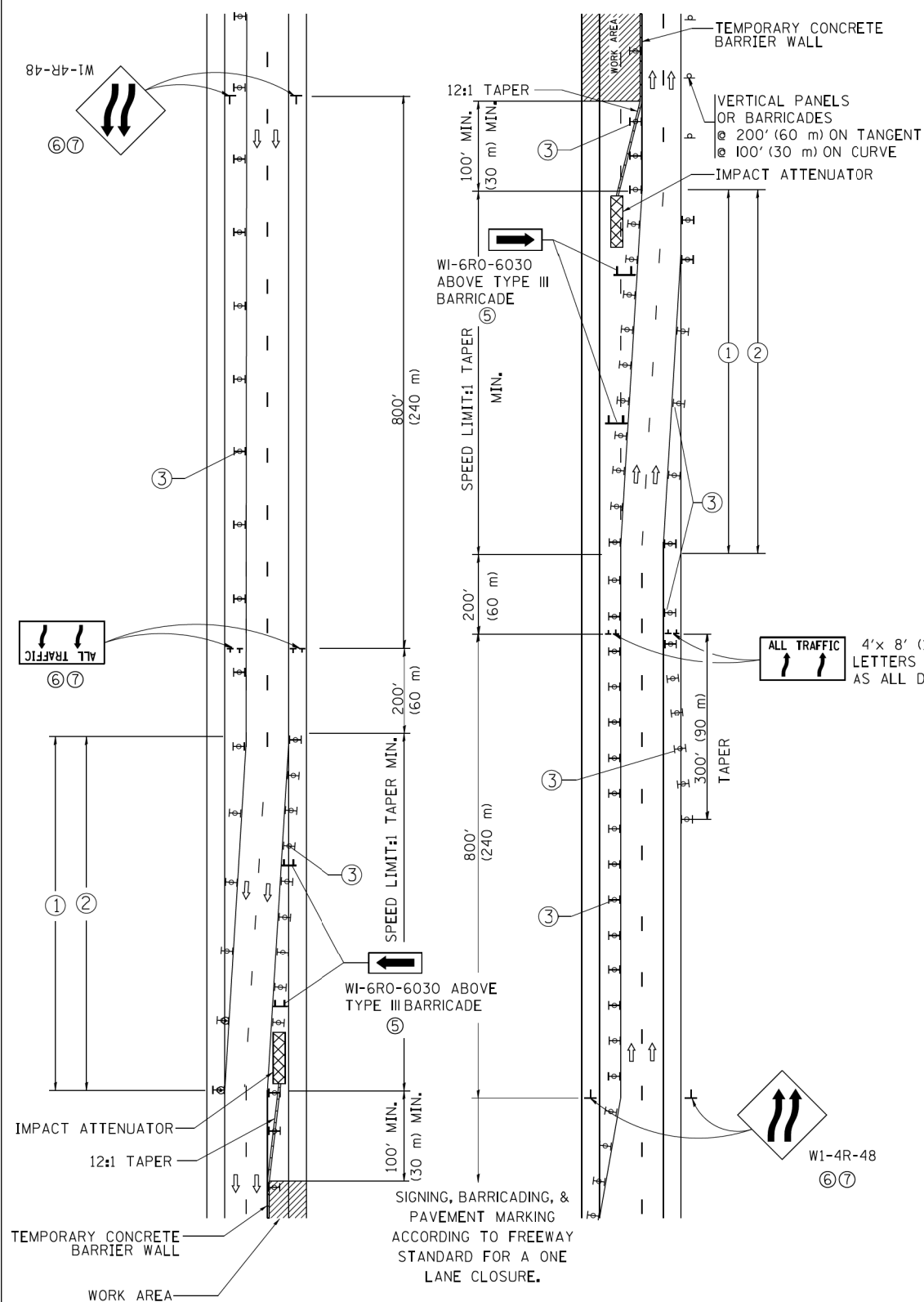
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F.A.I. RTE. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 324
TC-08		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

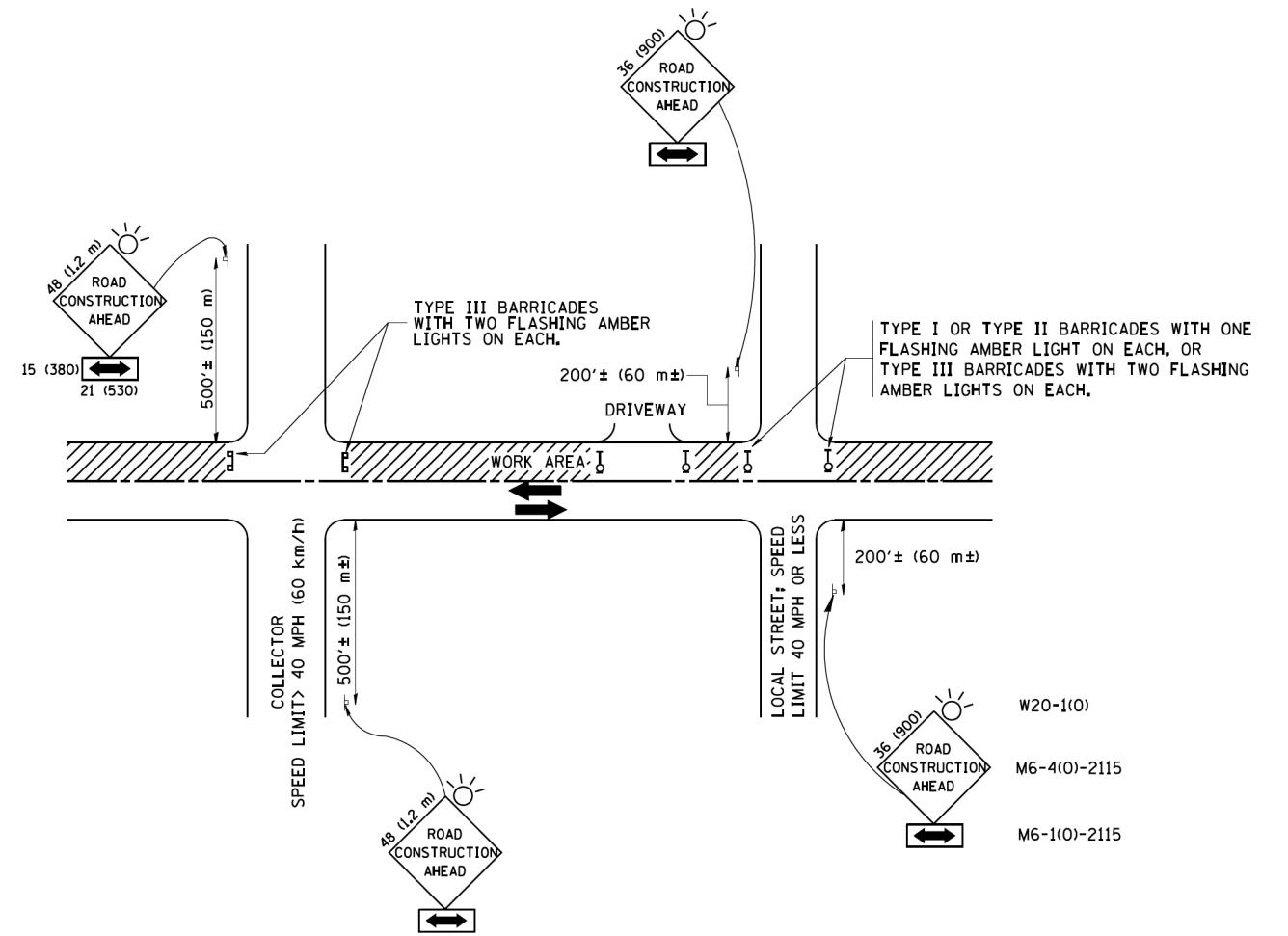
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED - DWS	REVISED - JAF 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A.I. RTE. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 325
DRAWN -					REVISED - SPB 01-07	SCALE: NONE			TC-09		CONTRACT NO. 60X95	
PLOT SCALE = 50.000' / in.					REVISED - SPB 12-09	SHEET NO. 1 OF 1 SHEETS			FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT	
PLOT DATE = 7/1/2013					REVISED - MD 06-13	STA. TO STA.						



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

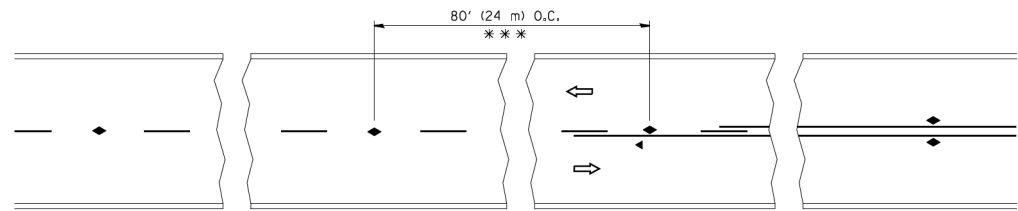
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACH 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

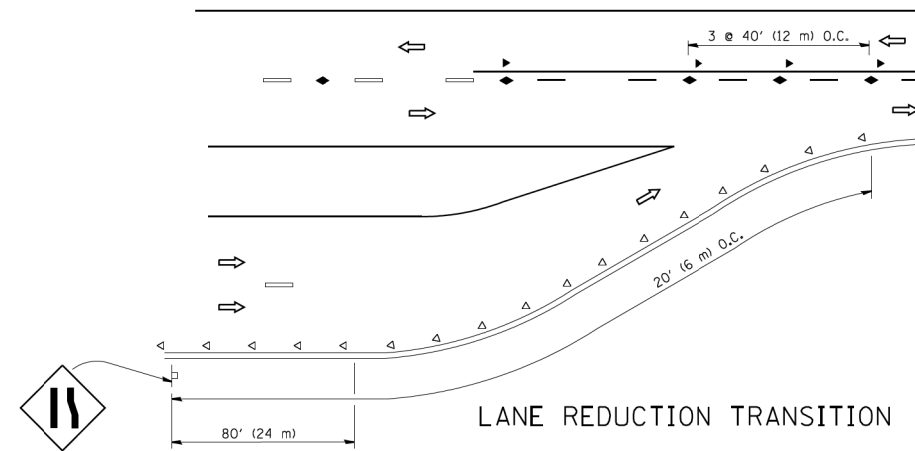
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 60X95	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

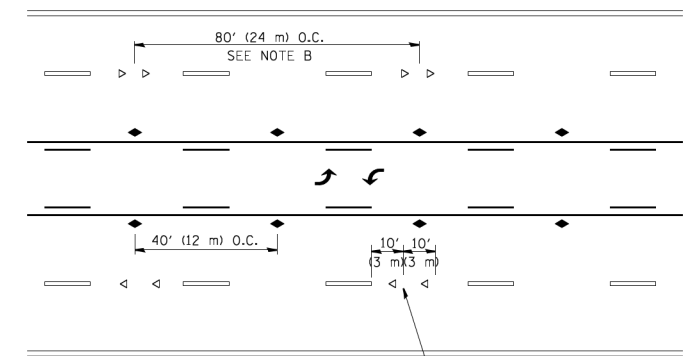


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

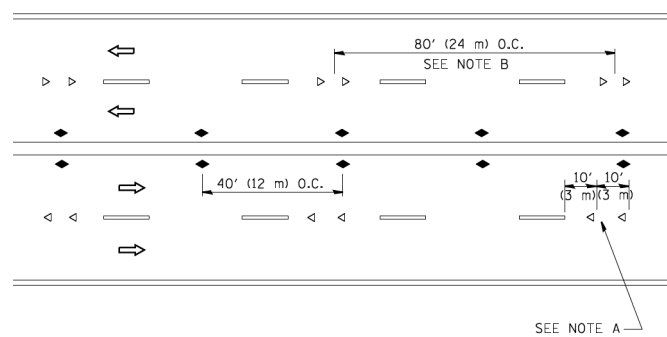
TWO-LANE/TWO-WAY



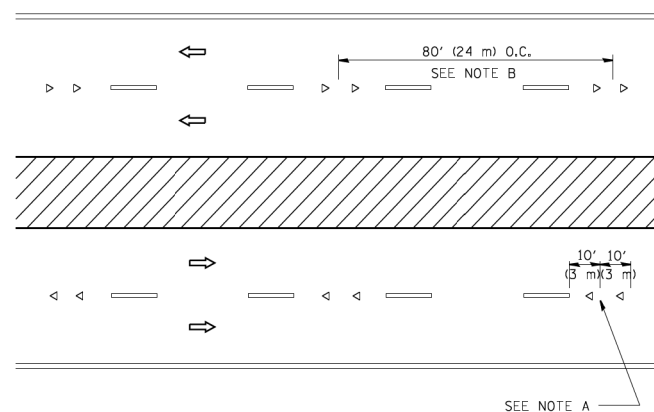
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

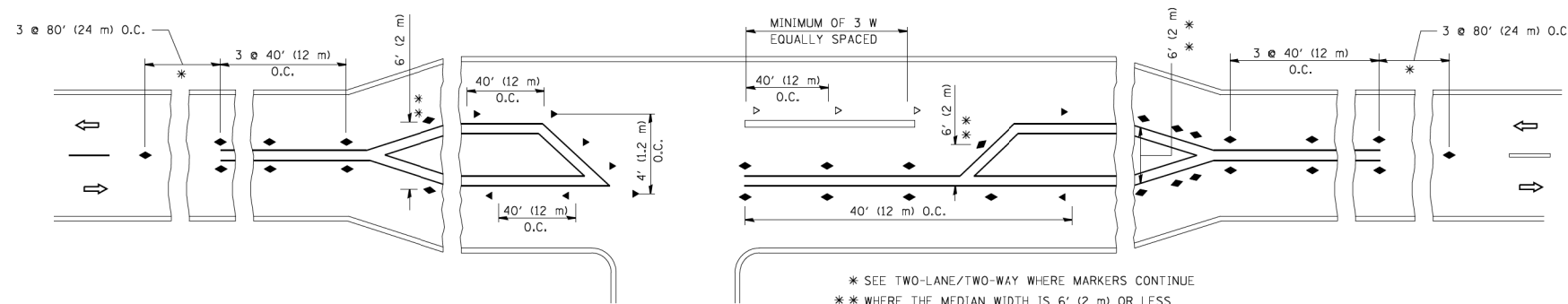
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

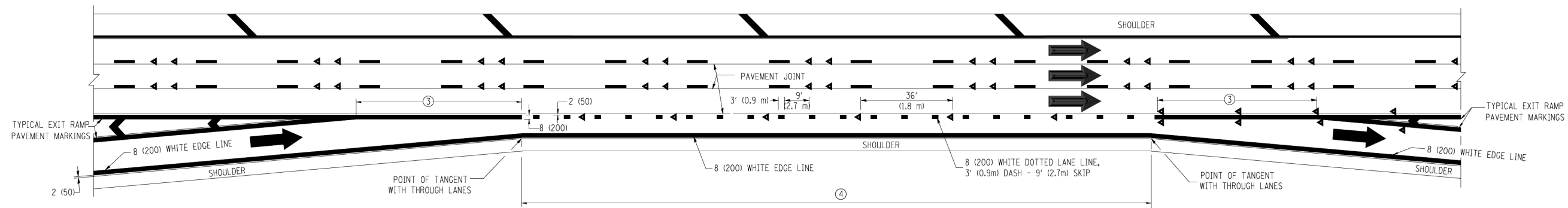
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

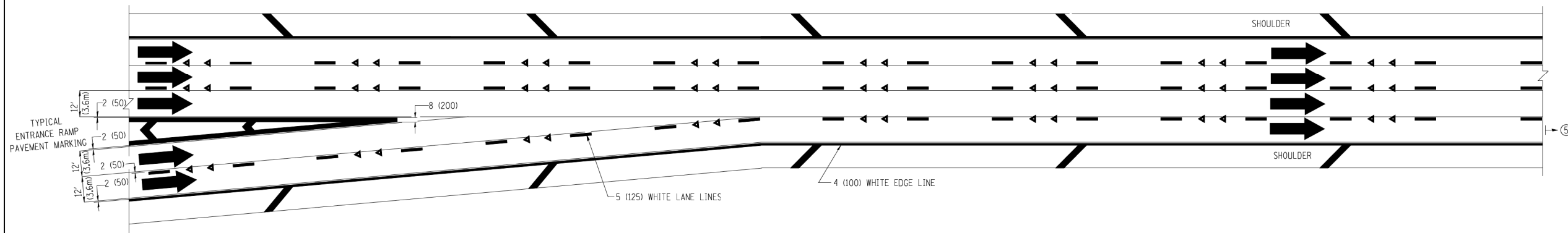
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

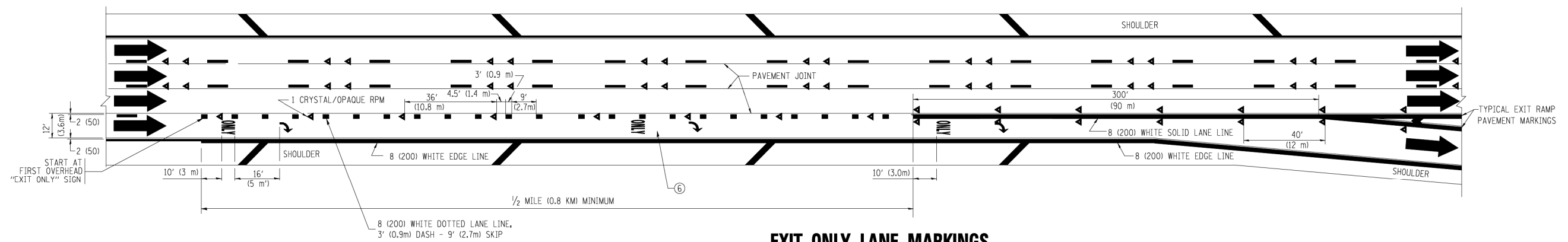
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	327
TC-11			CONTRACT NO. 60X95	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



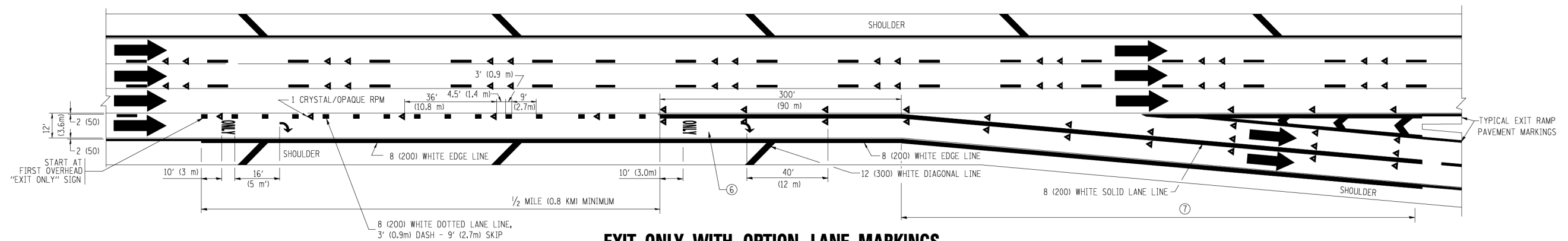
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

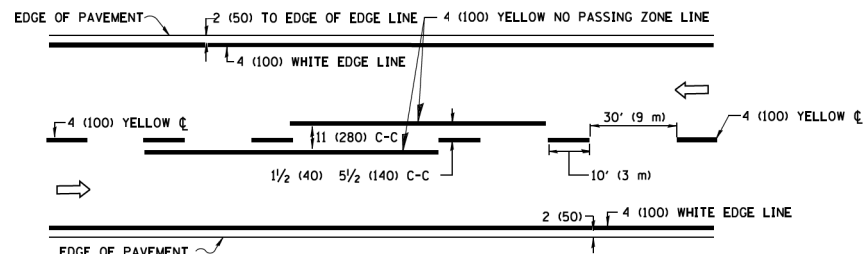
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = l1e9aa	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
u:\pwwork\p\p1001\LCYSA\100315\w12.dwg		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/22/2010	DATE - 01-90	REVISED - S.P.B. 01-10

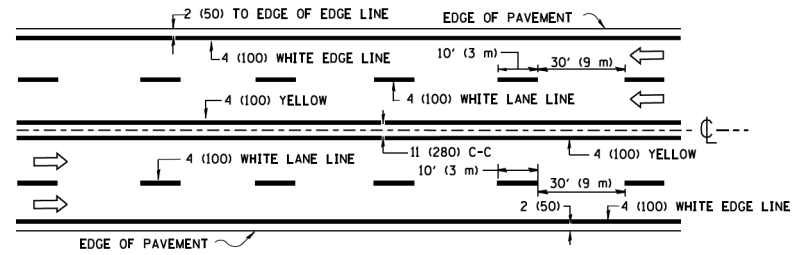
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
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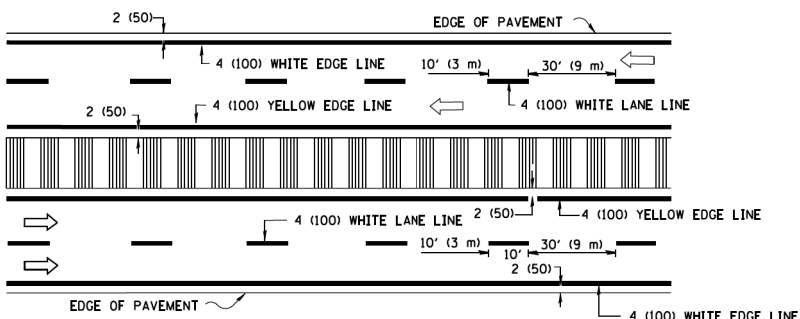
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	329
TC-12		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

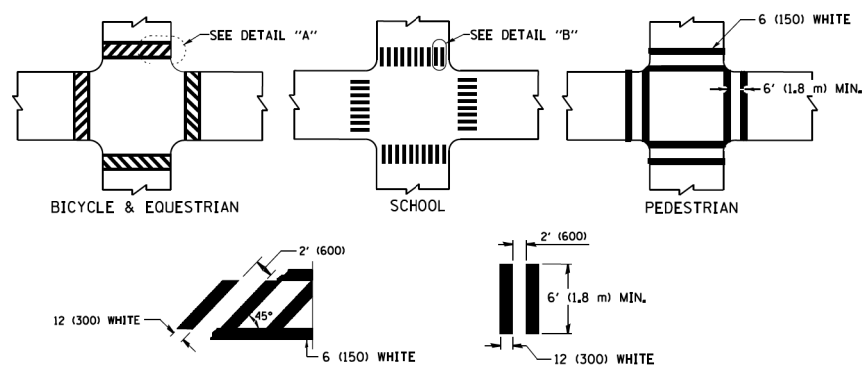


MULTI-LANE UNDIVIDED



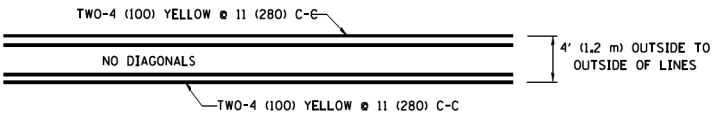
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

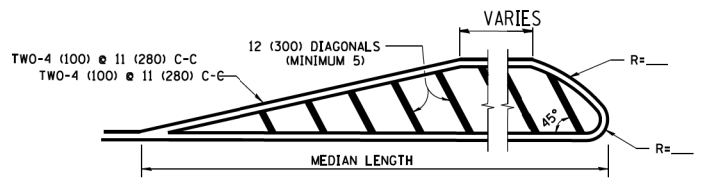


TYPICAL CROSSWALK MARKING

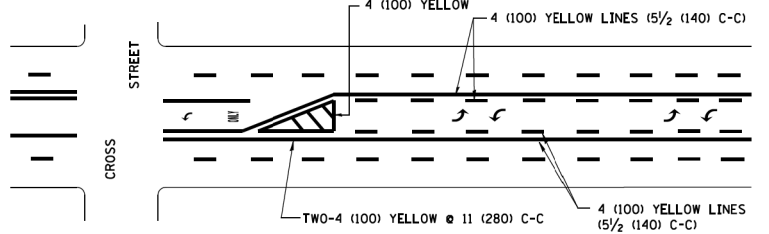
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



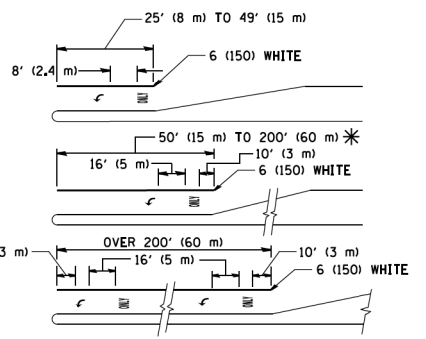
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



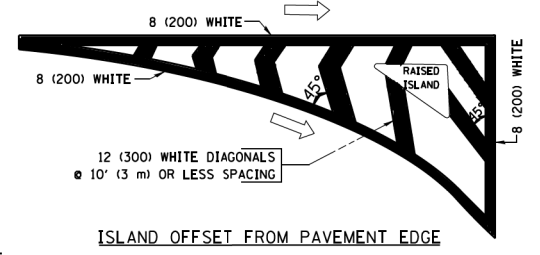
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



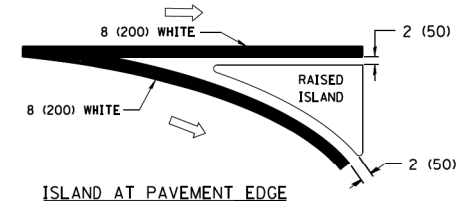
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

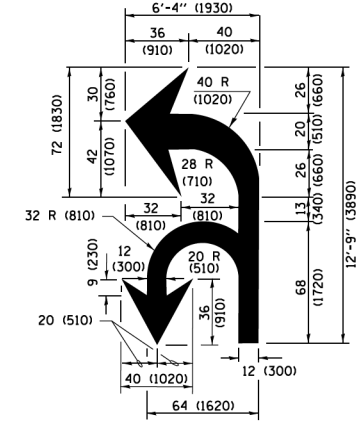
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



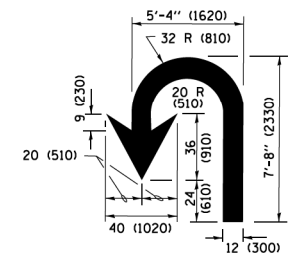
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = lszekrf	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
PROJECT =	PROJECT =	DRAWN - CAD	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 50,000' / 1" =	CHECKED -	DATE - 03-19-90	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 12/21/2015	DATE -	REVISED - C. JUCIUS 12-21-15	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE		F.A.1	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS		90/94	2014-016R&B	COOK	378	330
SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	ILLINOIS FED. AID PROJECT	

TC-13		CONTRACT NO. 60X95
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TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

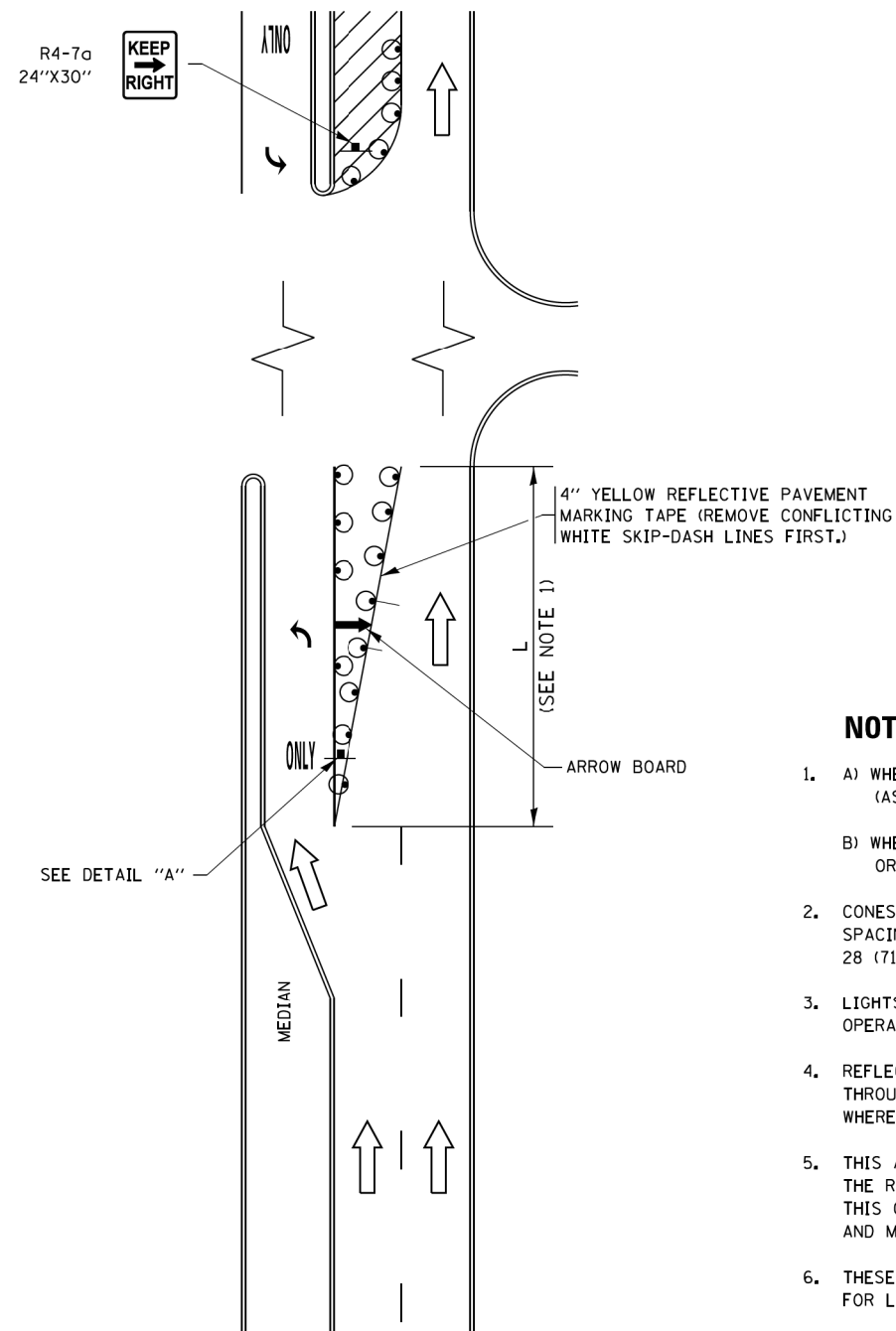


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

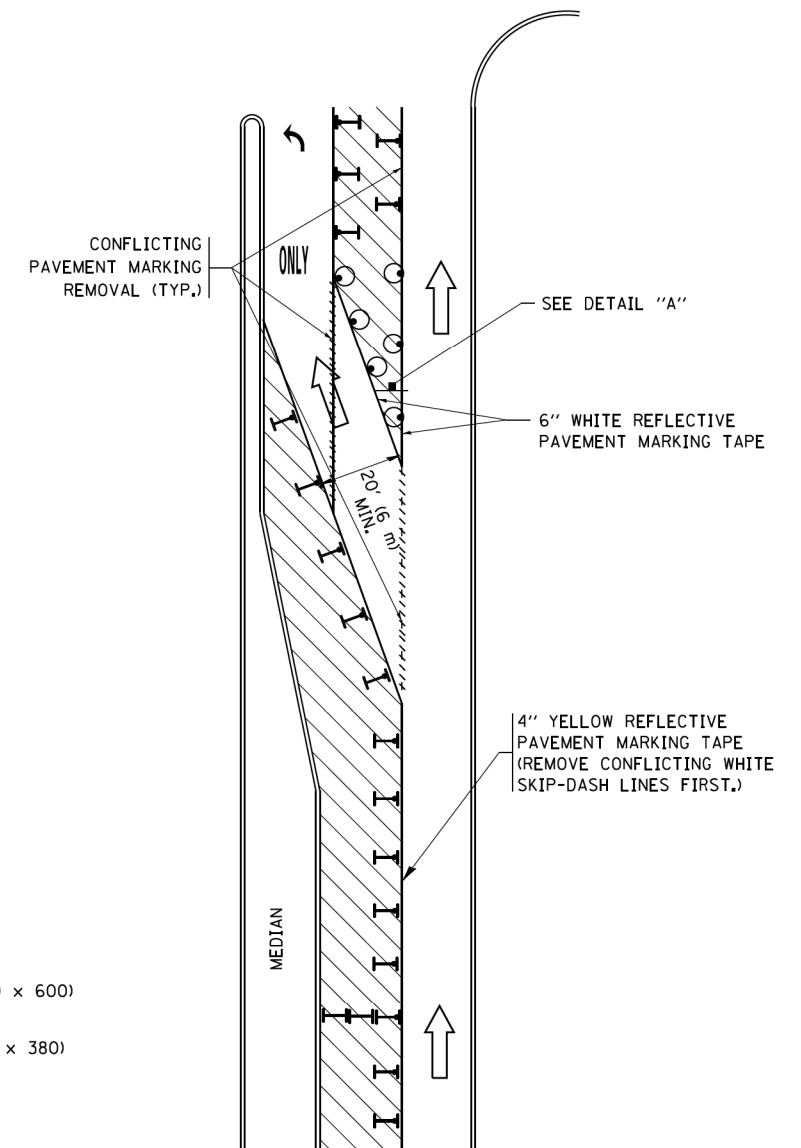


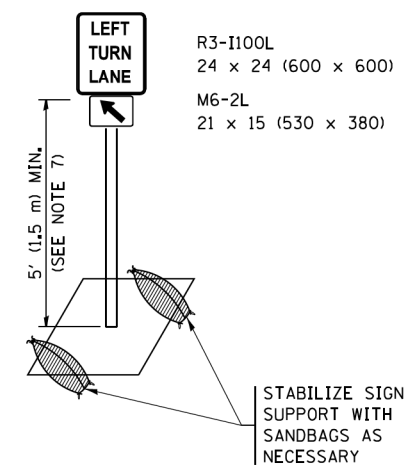
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

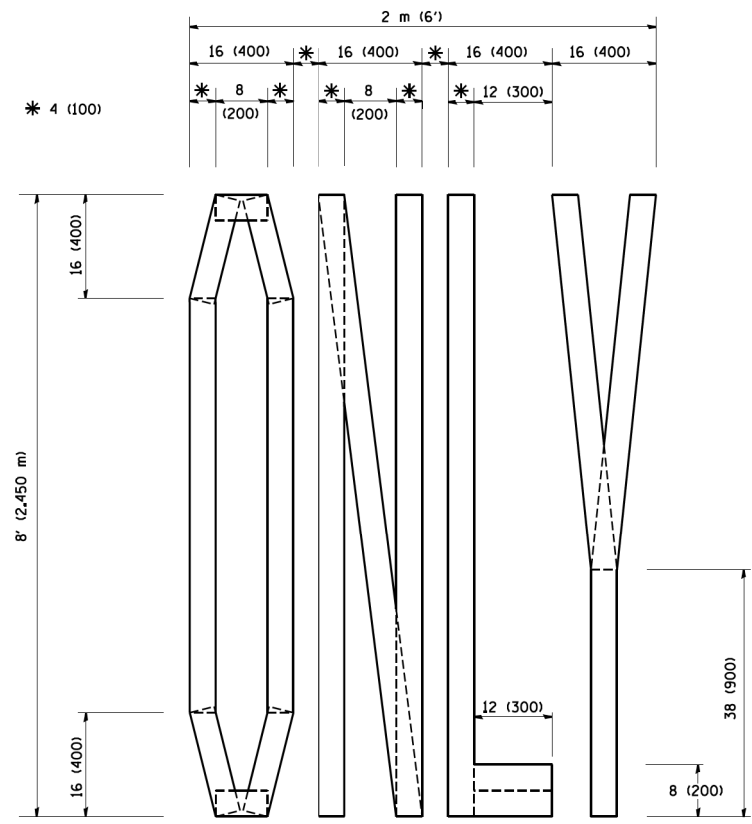
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



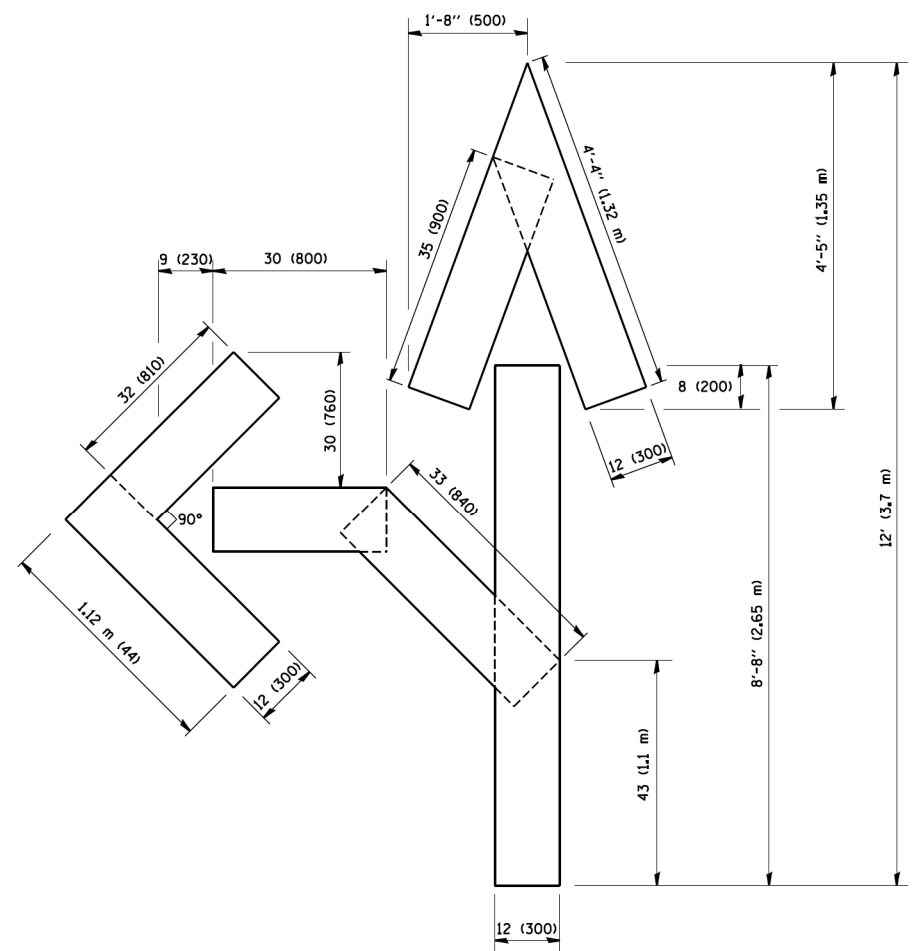
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

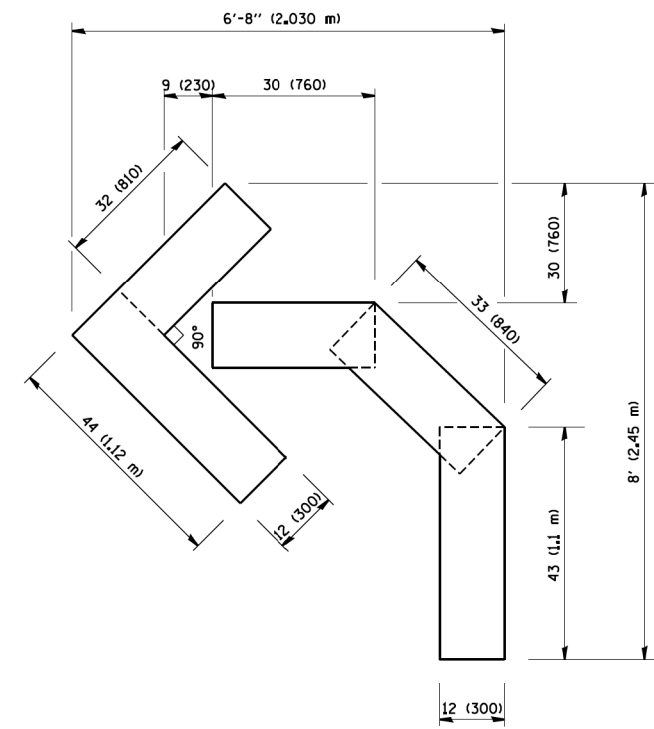
FILE NAME =	USER NAME = footemj	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT DATE = 9/15/2016	REVISED - A. HOUSEH 10-07-95	REVISED - A. SCHUETZE 07-01-13					90/94	2014-016R&B	COOK	378	331
		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO. 60X95				
		REVISED - T. RAMMACHER 01-06-00	REVISED -		ILLINOIS FED. AID PROJECT							



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\d\st\td\22x34\td16.dgn	USER NAME = gegl:anobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/4/2008	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

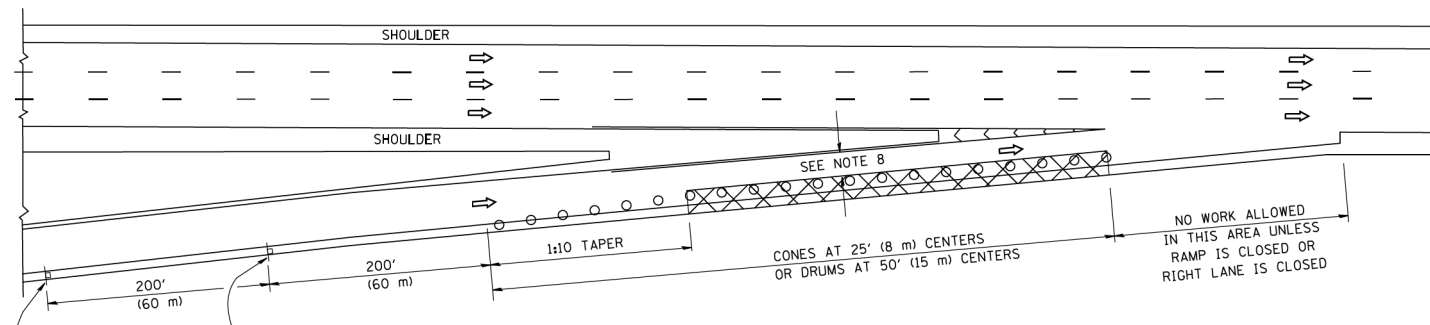
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

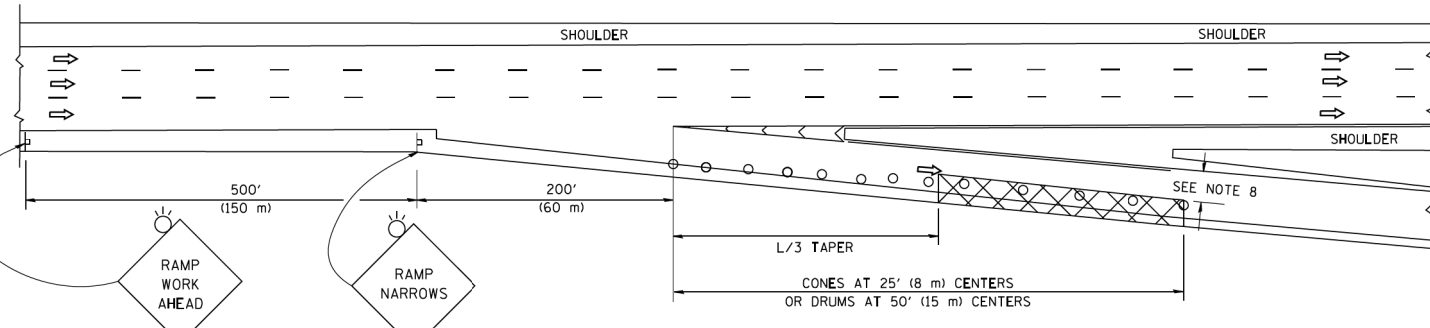
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	332
TC-16		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PARTIAL RAMP CLOSURE DETAILS

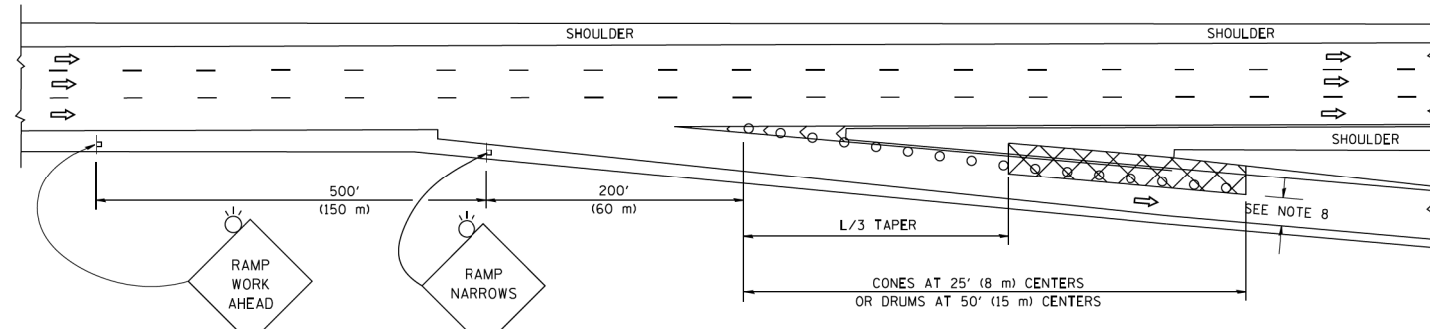
SHOULDER CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

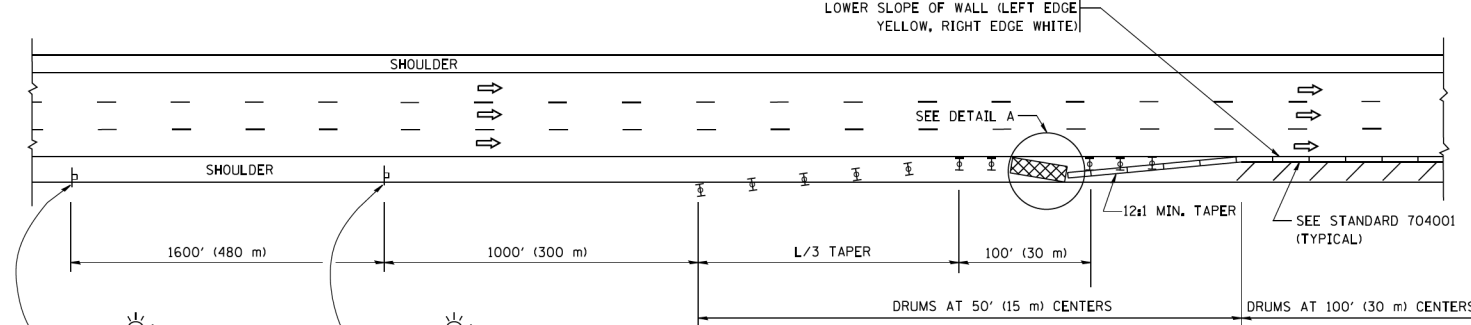
SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

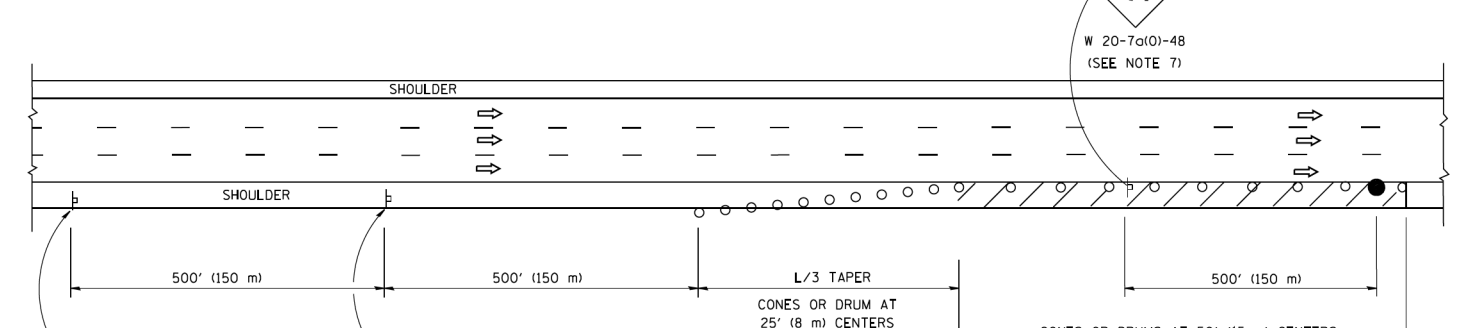
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
	W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.



PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350/MASH COMPLIANT.

DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

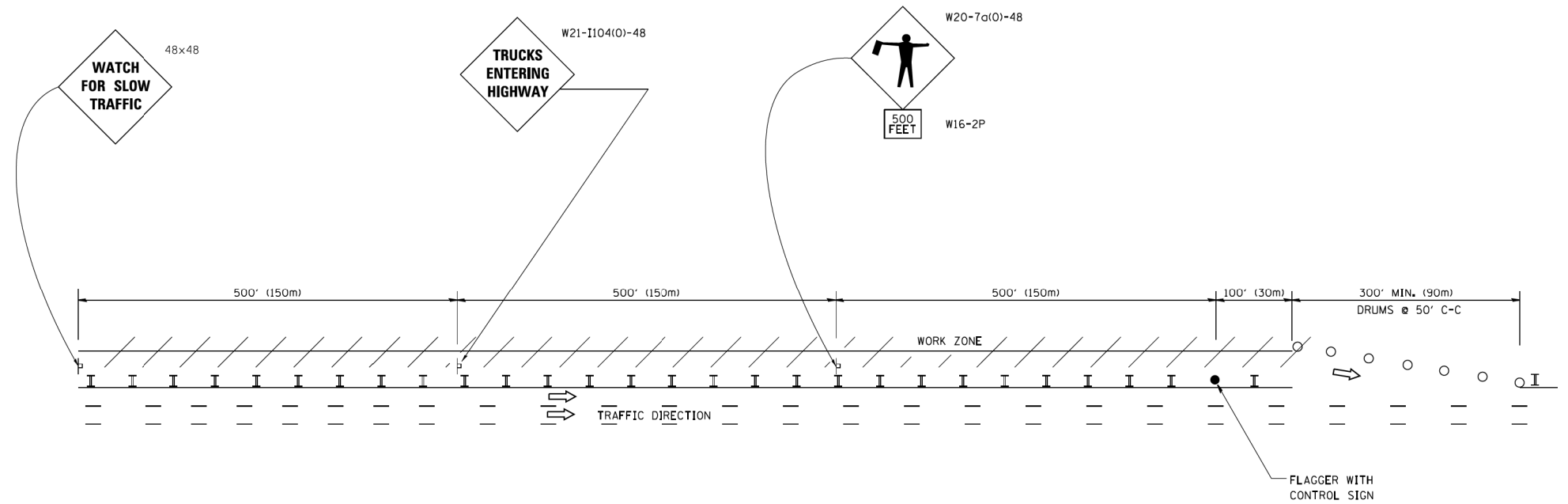
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12" MIN. WIDTH TANGENT SECTION
16" MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

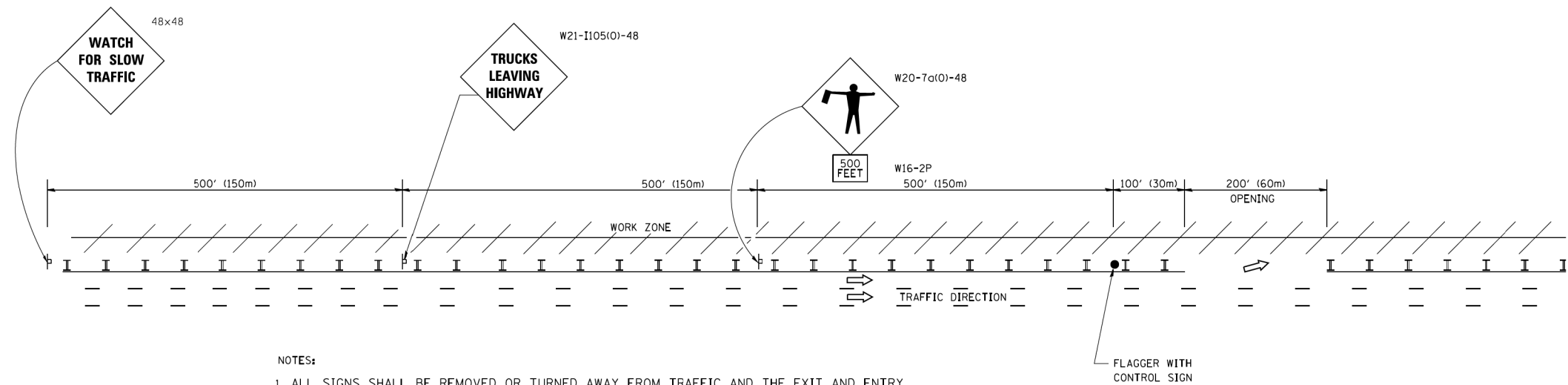
FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - J.A.F. 12-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			F.A.I. RTE. 90/94	SECTION 2014-016R&B TC-17	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 333
		DRAWN - D.W.S.	REVISED - S.P.B. 01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT		
		CHECKED -	REVISED - S.P.B. 12-09									
		DATE - 11-96	REVISED - M.D. 06-13									

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMP.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - J.A.F. 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FREEWAY/EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS/EXPRESSWAYS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwwork\pwwork\footemj\00100315\td10.dgn		DRAWN -	REVISED - S.P.B. 01-07			90/94	2014-016R&B	COOK	378	334	
PLOT SCALE = 50.000' / in.		CHECKED -	REVISED - S.P.B. 12-09			TC-18		CONTRACT NO. 60X95			
PLOT DATE = 7/8/2013		DATE -	REVISED - M.D. 06-13			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

ROUTE MARKERS

FOR U.S. ROUTES
M1-40-2424

FOR ILLINOIS ROUTES
M1-50-2424

R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

M5-1L-2115

M5-1R-2115

M6-1-2115

M6-2-2115

M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

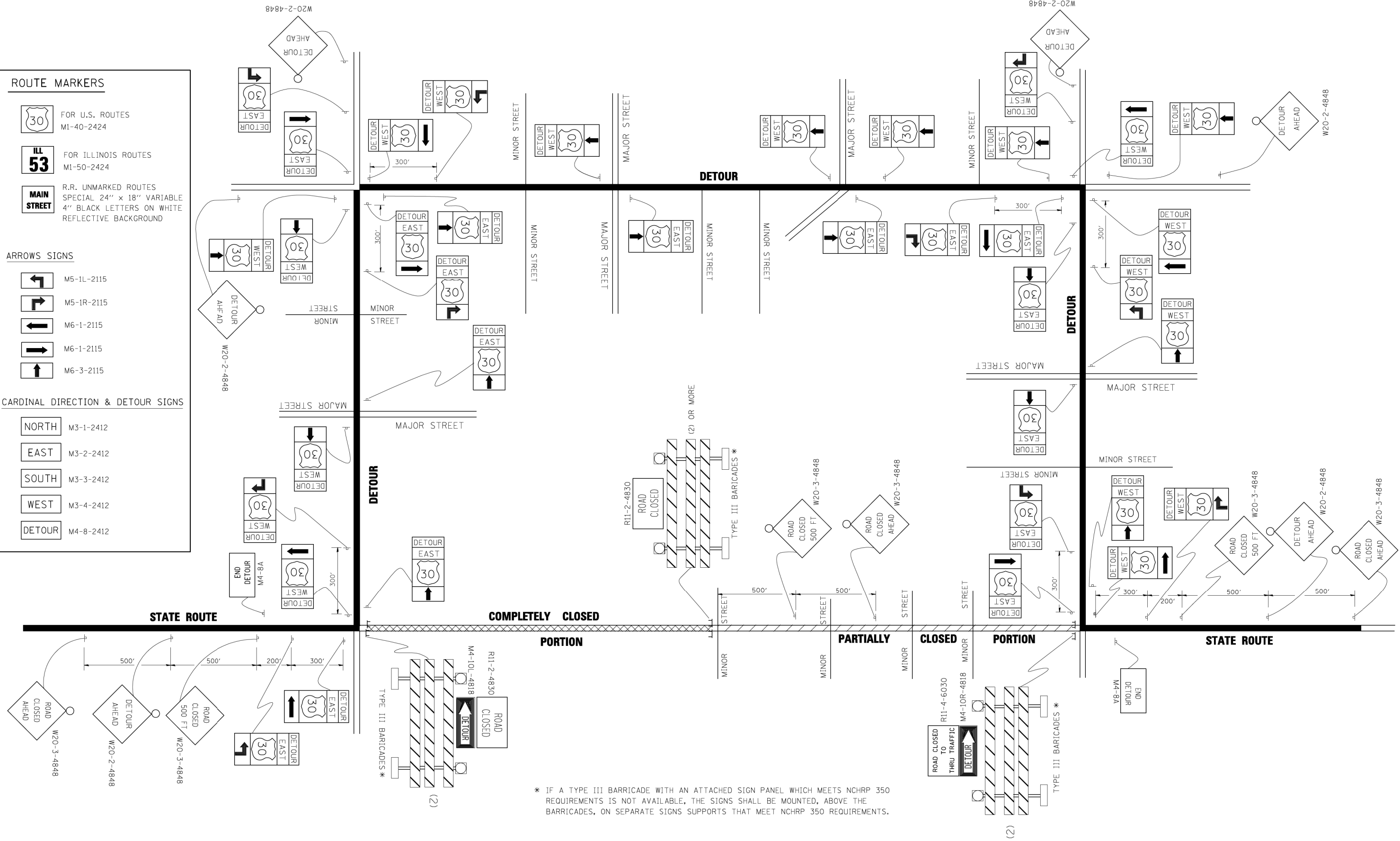
NORTH M3-1-2412

EAST M3-2-2412

SOUTH M3-3-2412

WEST M3-4-2412

DETOUR M4-8-2412



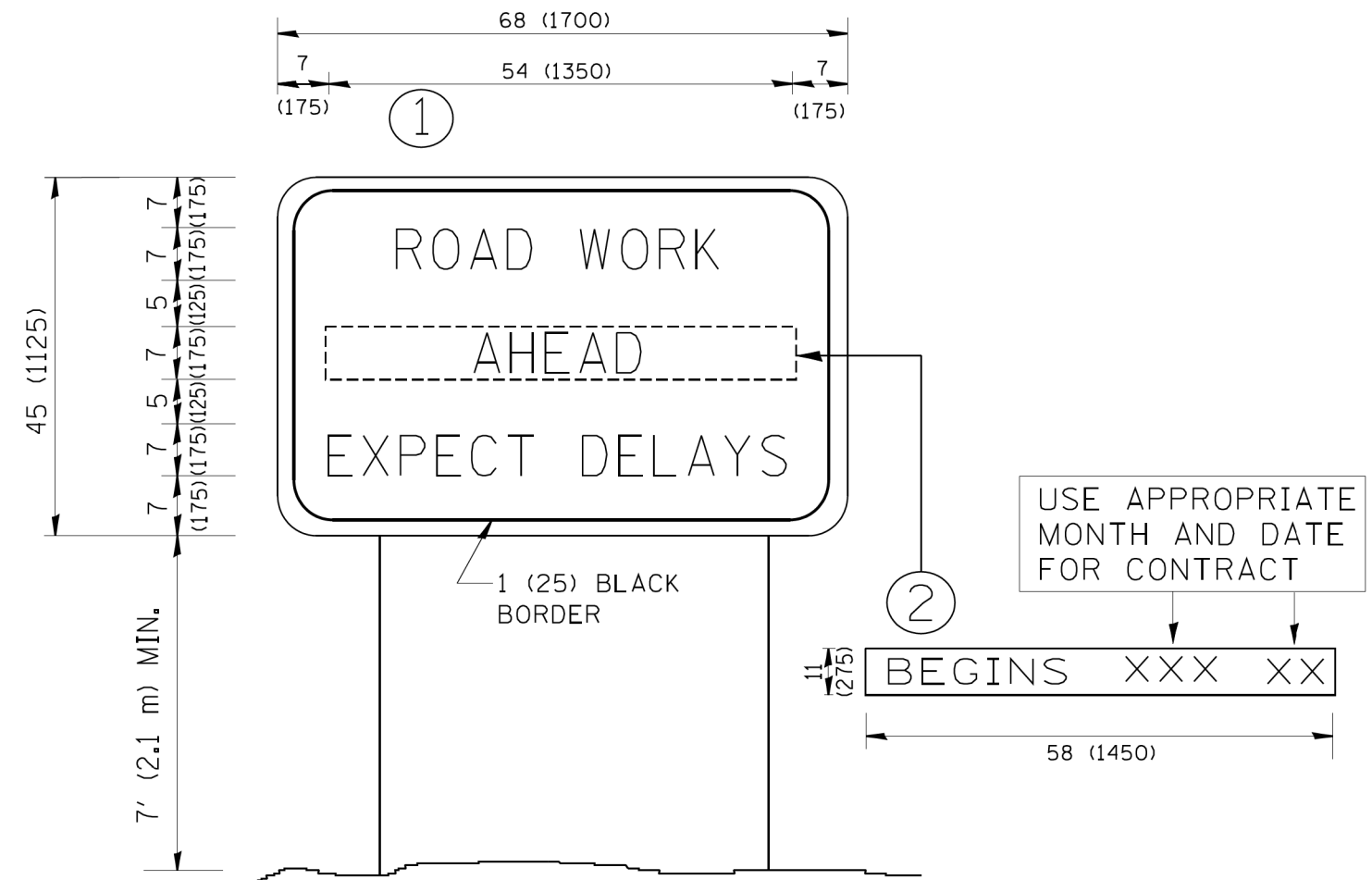
* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - 10-18-02
u:\pwwork\p\w\id\0\DRIVAKOSGN\0100315\1421.dwg		DRAWN -	REVISED - R. DORO 09-14-09
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. R.T.E. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 335
TC-21		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

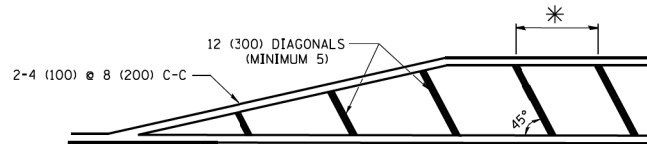
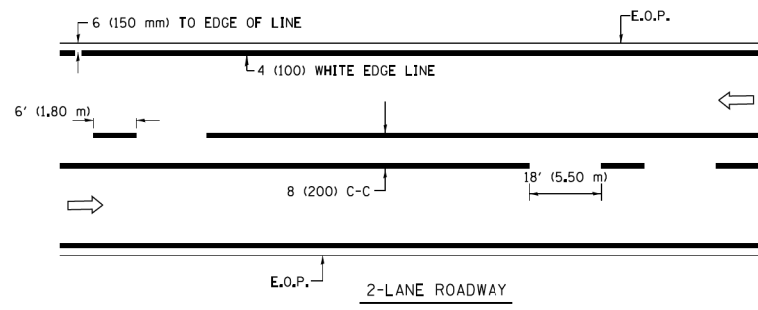
FILE NAME = W:\d\15\15\22x34\1522.dgn	USER NAME = gegl1enobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

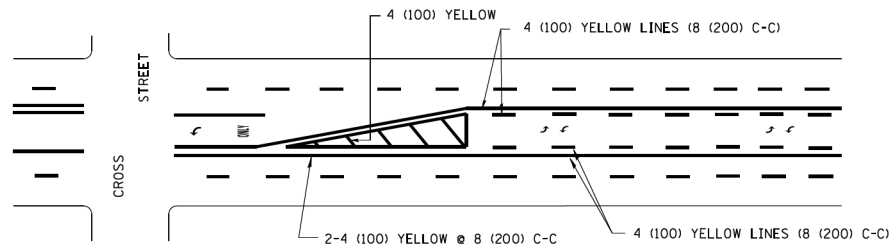
F.A.I. RTE. 90/94	SECTION 2014-016R&B TC-22	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 336
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X95	



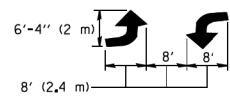
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

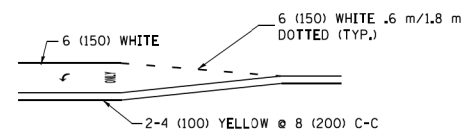


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR, ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

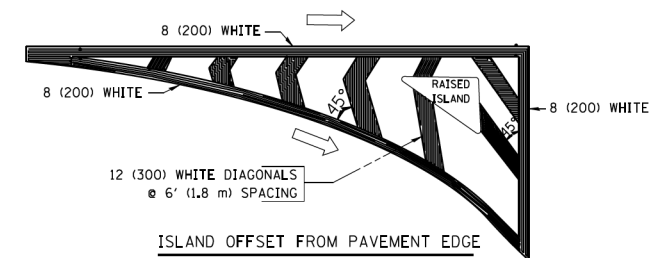


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

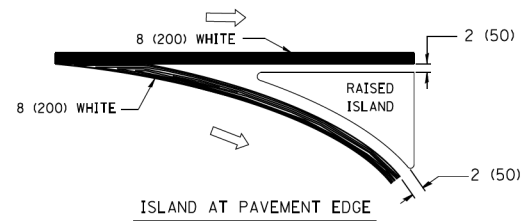
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



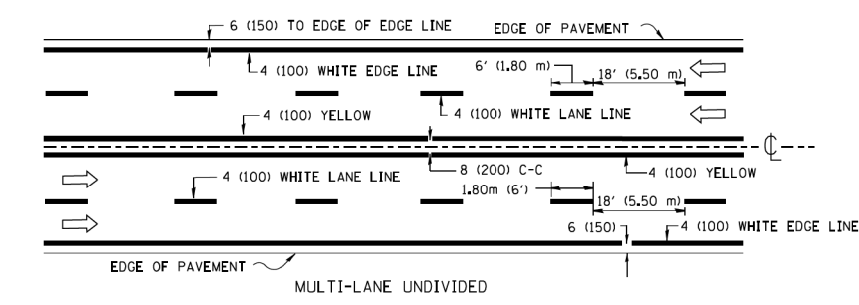
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

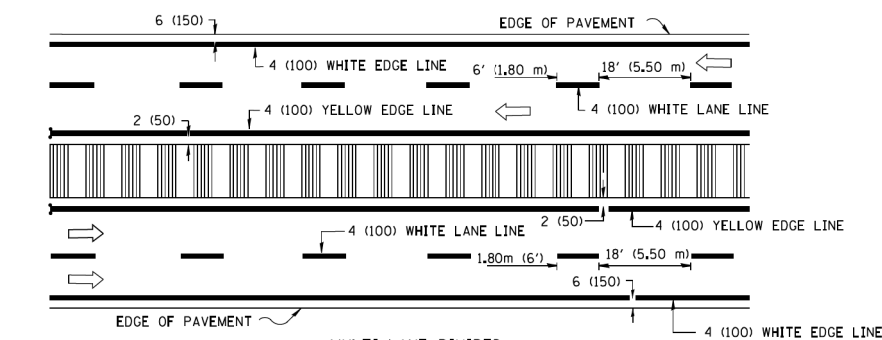
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4 m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



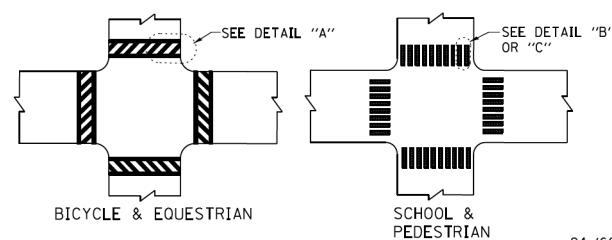
MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



BICYCLE & EQUESTRIAN

SCHOOL & PEDESTRIAN

DETAIL "A"

DETAIL "B"

DETAIL "C"

TYPICAL CROSSWALK MARKING

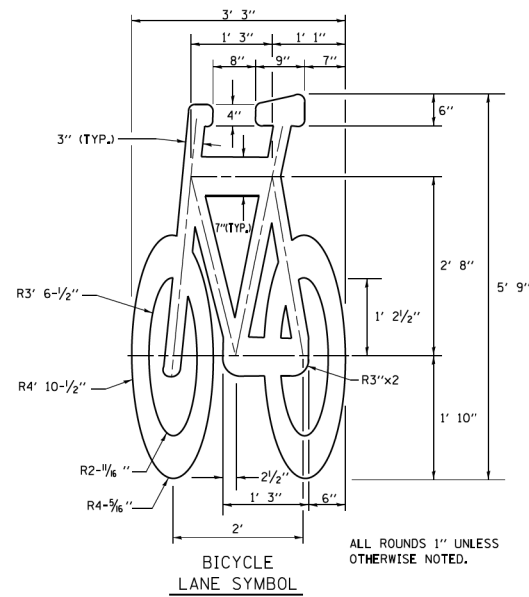
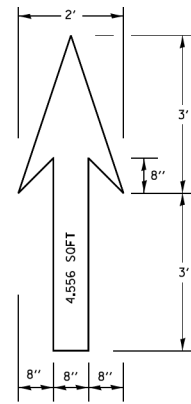
FILE NAME =	USER NAME = drsvakosgn	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
ca:\pwwork\pavado\drsvakosgn\0100315\12-24.dgn		DRAWN -	REVISED - K. ENG 02-28-12
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/1/2012	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS**

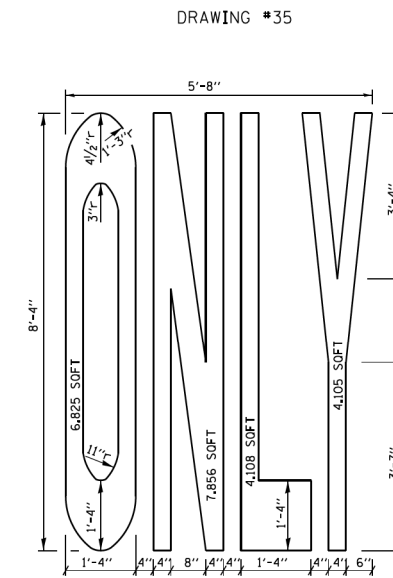
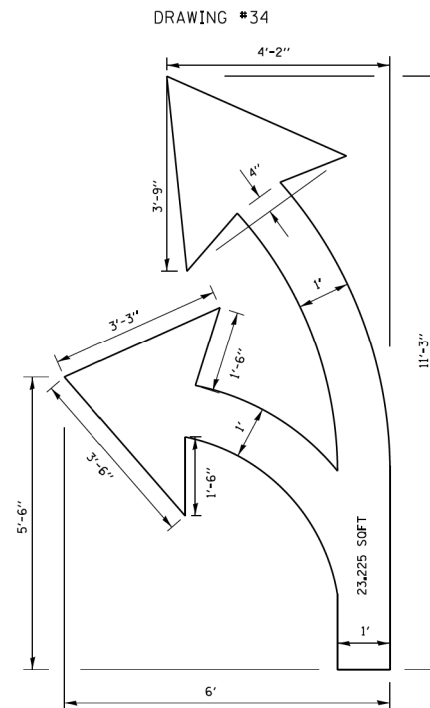
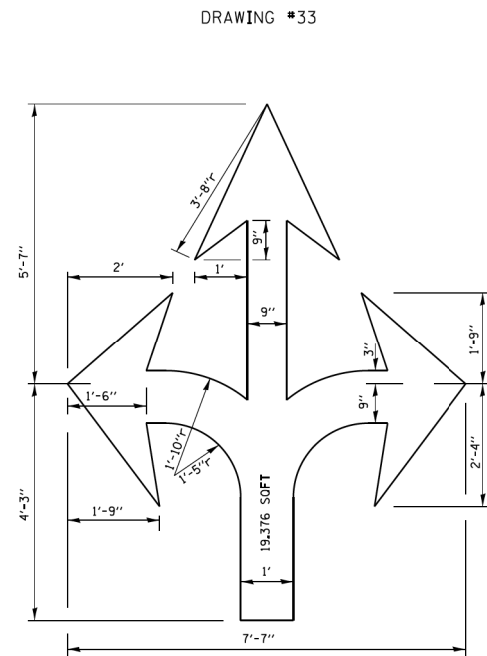
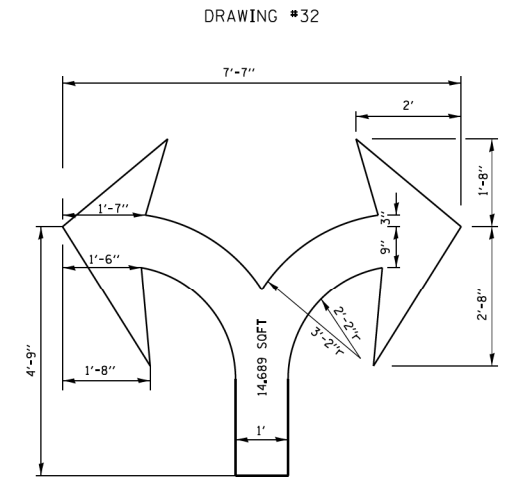
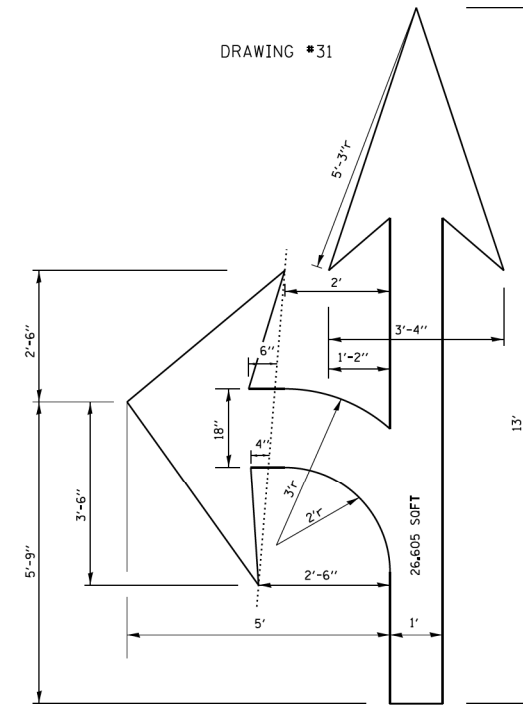
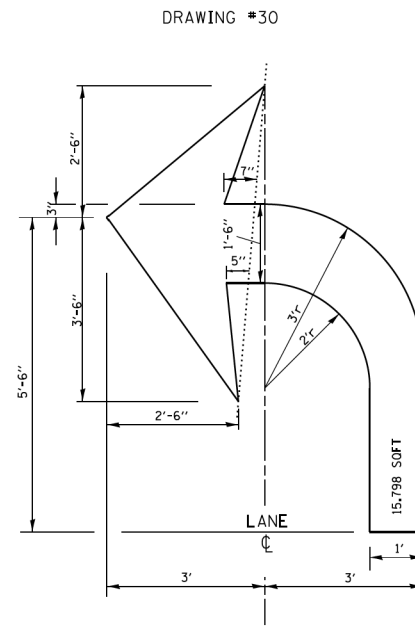
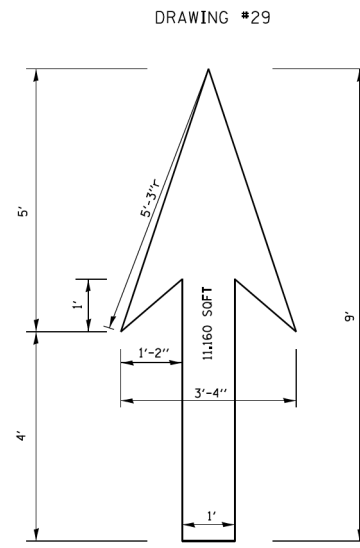
SCALE: NONE SHEET NO. 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 337
TC-24		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTE:**
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



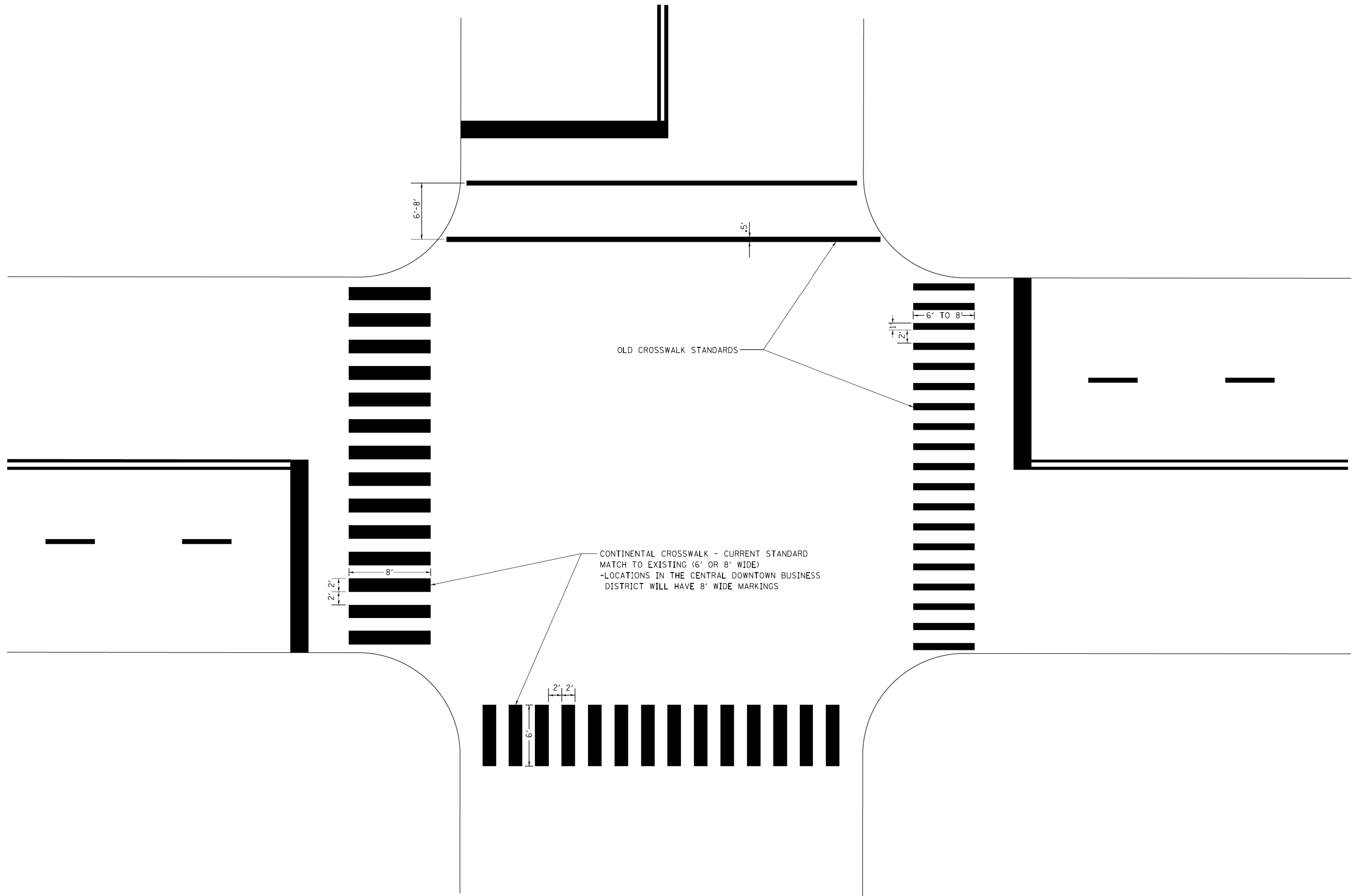
NOTE: ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = drsvakosgn	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
ca:\pwwork\pwwork\drsvakosgn\0100315\024.dgn		DRAWN -	REVISED - K. ENG 02-20-12
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/29/2012	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO	
TYPICAL PAVEMENT MARKINGS	
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	338
TC-24		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



OLD CROSSWALK STANDARDS

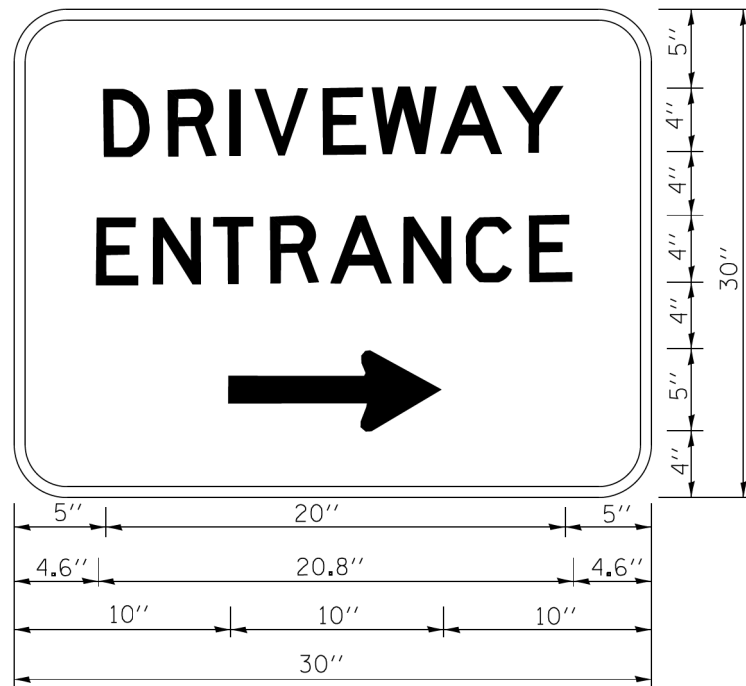
CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

FILE NAME =	USER NAME = drivakosgn	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
ca:\pwork\pwork\drivakosgn\0100315\124.dgn		DRAWN -	REVISED - K. ENG 02-28-12
	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/29/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	339
TC-24		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gagl1anob1	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ca:\pwwork\pwwork\gagl1anob1\0100315\1026.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

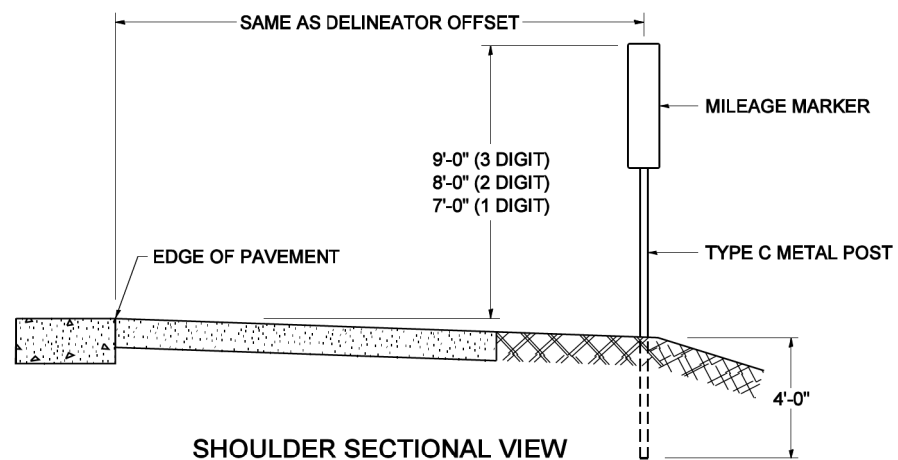
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

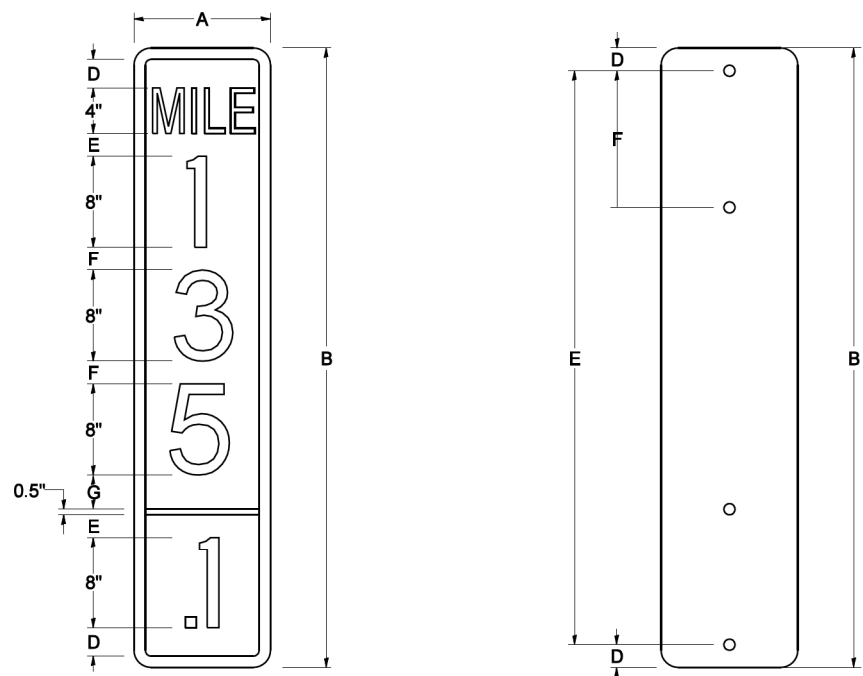
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	340
TC-26		CONTRACT NO. 60X95		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STANDARD DESIGN FOR MILE POST



SHOULDER SECTIONAL VIEW

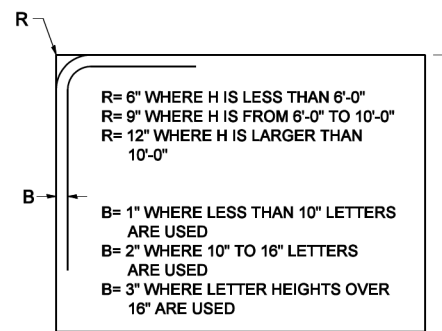


SIGN SIZE	DIMENSIONS							DIGIT
	A	B	C	D	E	F	G	
12 x 24	12.0	24.0	1.5	1.5	1.5	N/A	1.5	1
12 x 36	12.0	36.0	1.5	2.0	2.0	2.0	1.5	2
12 x 48	12.0	48.0	1.5	2.5	2.0	2.0	2.5	3

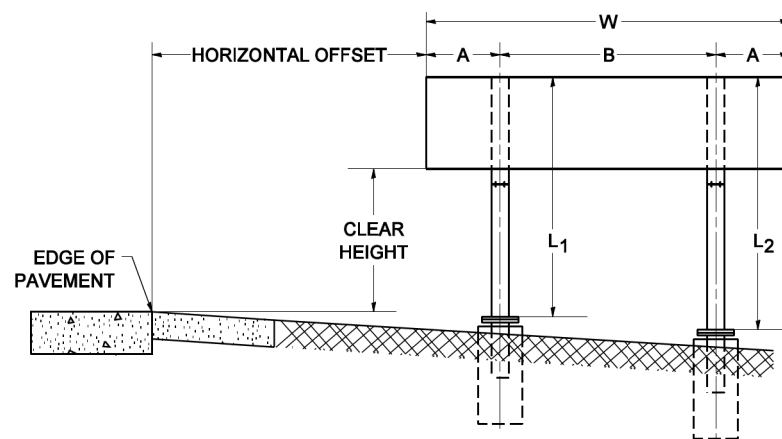
BLANK	A	B	C	D	E	F
B9-1224	12.0	24.0	1.5	2.0	20.0	N/A
B9-1236	12.0	36.0	1.5	2.0	32.0	12.0
B9-1248	12.0	48.0	1.5	2.0	44.0	12.0

SIGN SIZE	SERIES LINES					BORDER	BLANK STD.
	1	2	3	4	5		
12 x 24	4C	8D	4C	N/A	N/A	0.5	B9-1224
12 x 36	4C	8D	8D	4C	N/A	0.5	B9-1236
12 x 48	4C	8D	8D	8D	4C	0.5	B9-1248

BORDER AND RADIUS LAYOUT



MAJOR GUIDE SIGN LAYOUT

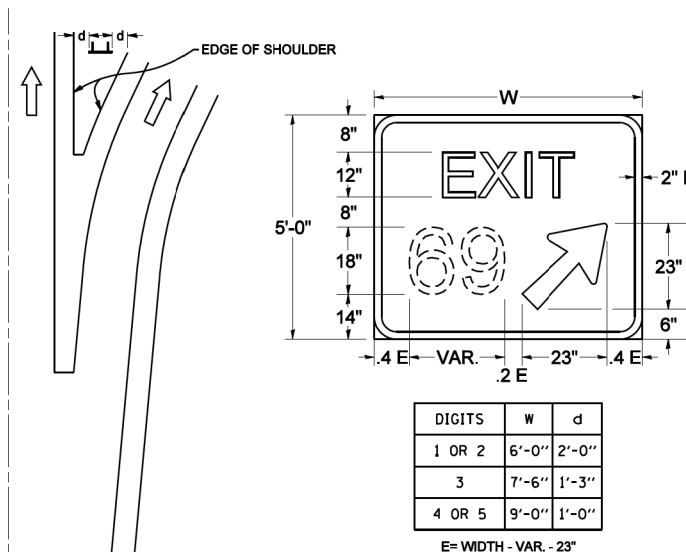


NUMBER OF STEEL SUPPORTS	A	B
2	.2 W	.6 W
3	.15 W	.35 W
4	.125 W	.25 W
5	.1 W	.2 W

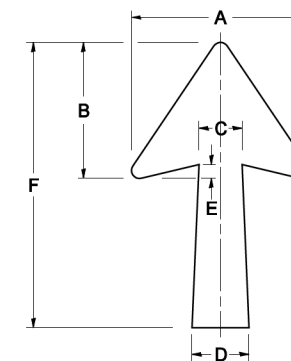
L₁ IS THE LENGTH OF SUPPORT, NOT INCLUDING THE STUB PROJECTION, CLOSEST TO THE EDGE OF THE PAVEMENT.

"A" IS THE DISTANCE FROM THE SIGN EDGE TO THE CENTERLINE OF THE NEAREST SUPPORT. "B" IS THE DISTANCE BETWEEN CENTERLINES OF SUPPORTS.

GORE SIGNS

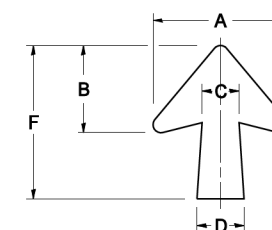


STANDARD ARROWS FOR INTERSTATE GUIDE SIGNS



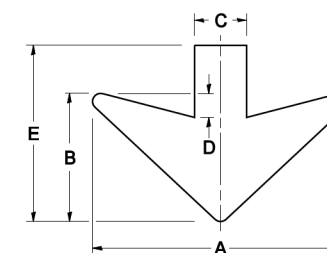
ARROW SYMBOL	A	B	C	D	E	F	R
24 1/4 x 15 1/8	15 1/8	11 1/8	3 3/4	5	1 1/8	2 1/4	5/8
29 1/4 x 18 1/4	18 1/4	14	4 1/2	6	1 1/2	2 9/4	3/4
35 5/8 x 22 1/4	22 1/4	17	5 3/8	7 1/8	1 3/4	35 5/8	1
18 1/4 x 11 1/4	11 1/4	8 3/4	3 1/8	3 3/8		18 1/4	

NOTE: D & F ARE RECOMMENDED DIMENSIONS. TAPER SHOULD BE HELD CONSTANT FOR LONGER OR SHORTER SHAFT LENGTHS



ARROW SYMBOL	A	B	C	D	E	F	R
17 1/4 x 14 1/4	14 1/4	9 3/8	3 3/8	4 1/2	5/8	17 1/4	3/4
20 1/4 x 17 1/4	17 1/4	11 3/4	4 3/8	5 5/8	1 1/2	20 1/4	
25 x 21 1/8	21 1/8	14 1/4	5	6 3/4	1 3/4	25	1
9 5/8 x 8 1/8	8 1/8	5 3/8	2 3/8	2 3/8		9 5/8	1/2

DOWN ARROWS

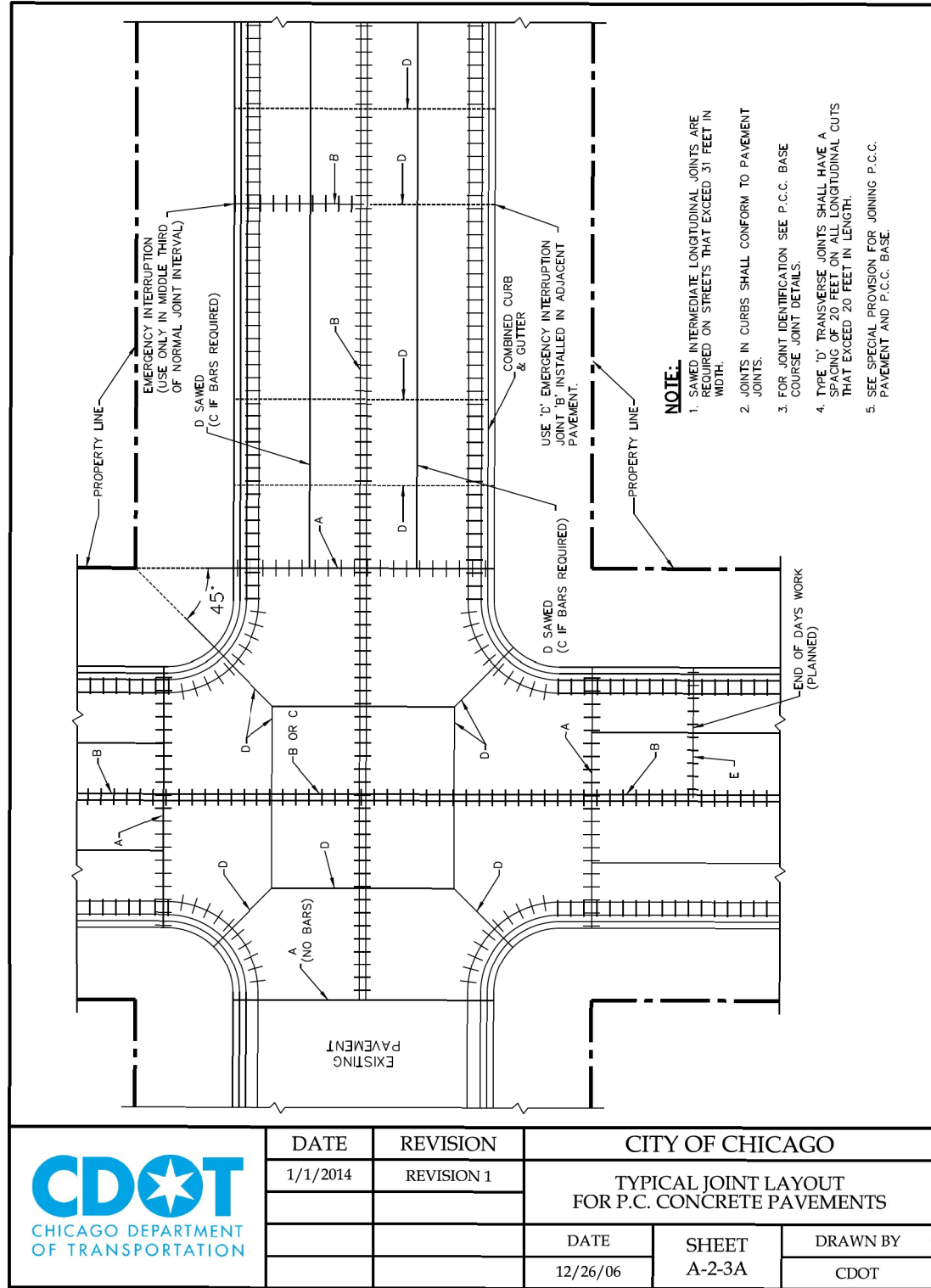


ARROW SYMBOL	A	B	C	D	E	R
16 1/2 x 24	24	12	5	1 1/2	16 1/2	3/4
22 x 32	32	16	6 1/2	3	22	1

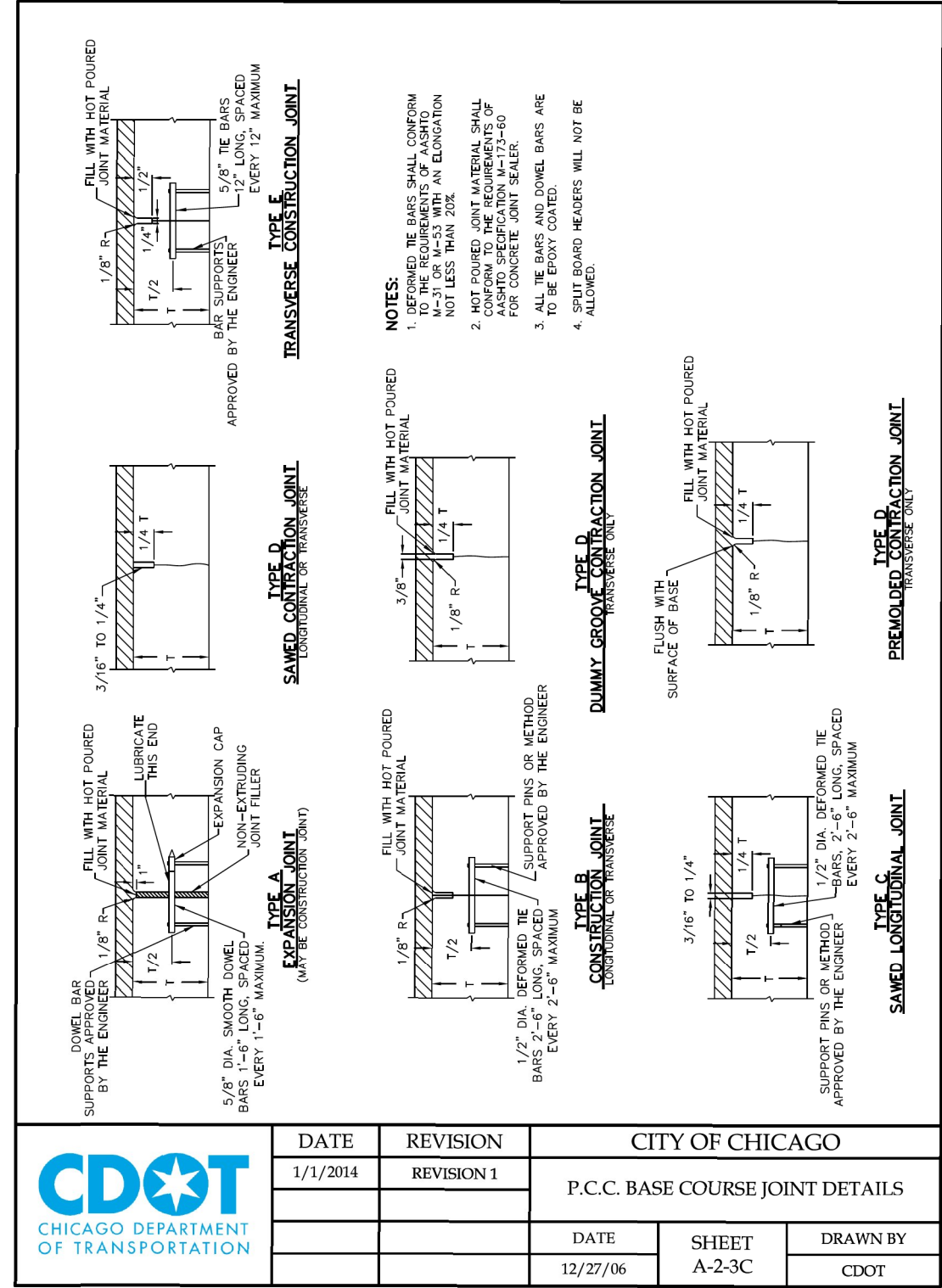
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	FILED		
	NO. _____		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS OK'D		
	NO. _____		

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	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	TYPICAL JOINT LAYOUT FOR P.C. CONCRETE PAVEMENTS		
	DATE	SHEET	DRAWN BY		
	12/26/06	A-2-3A	CDOT		



	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	P.C.C. BASE COURSE JOINT DETAILS		
	DATE	SHEET	DRAWN BY		
	12/27/06	A-2-3C	CDOT		

PARSONS BRINCKERHOFF

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	DRAWN - MCC	REVISED -
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PLOT DATE = 4/18/2017	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

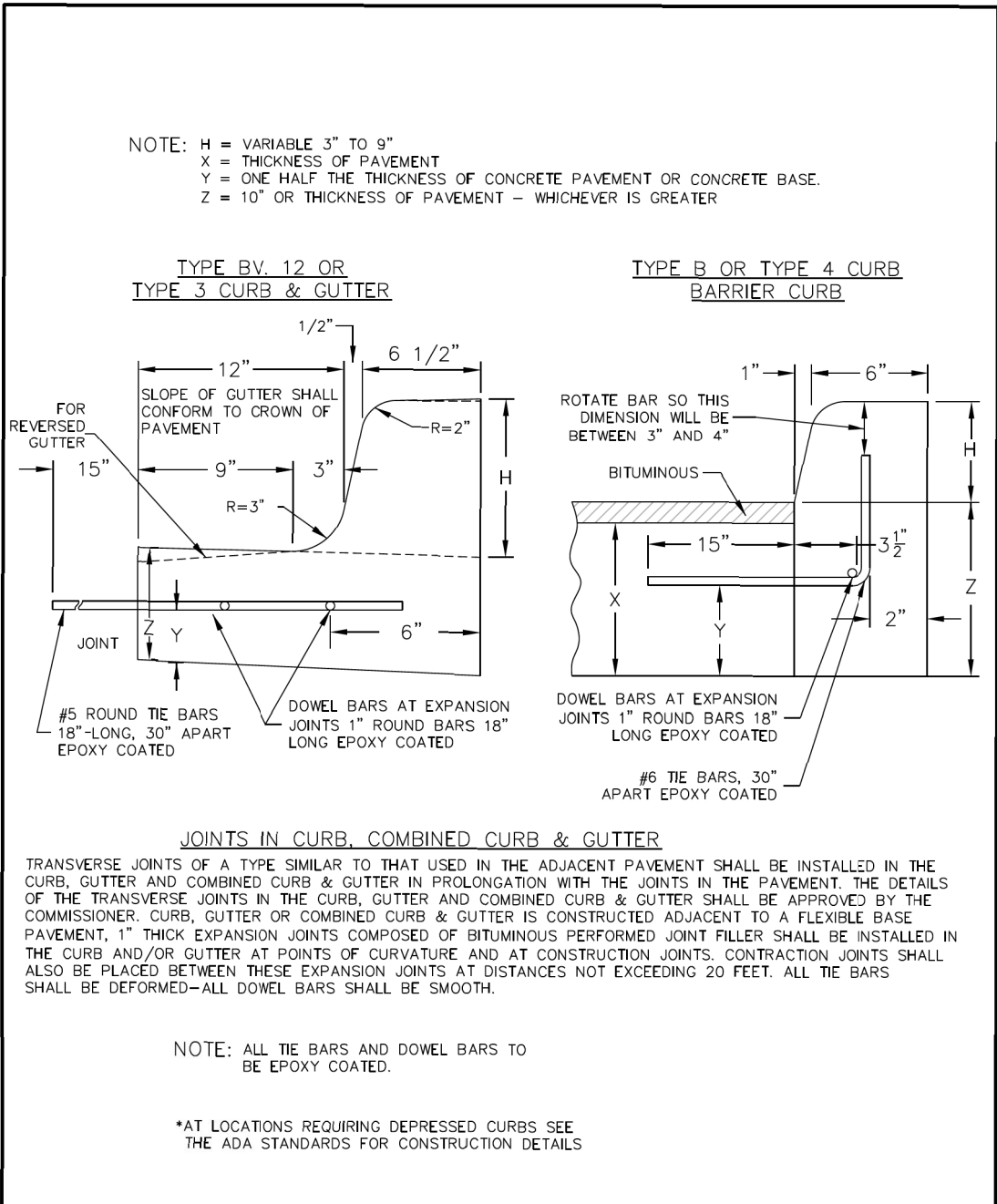
CHICAGO DEPARTMENT OF TRANSPORTATION
STANDARDS

SCALE: N.T.S. SHEET NO. 1 OF 7 SHEETS STA. TO STA.

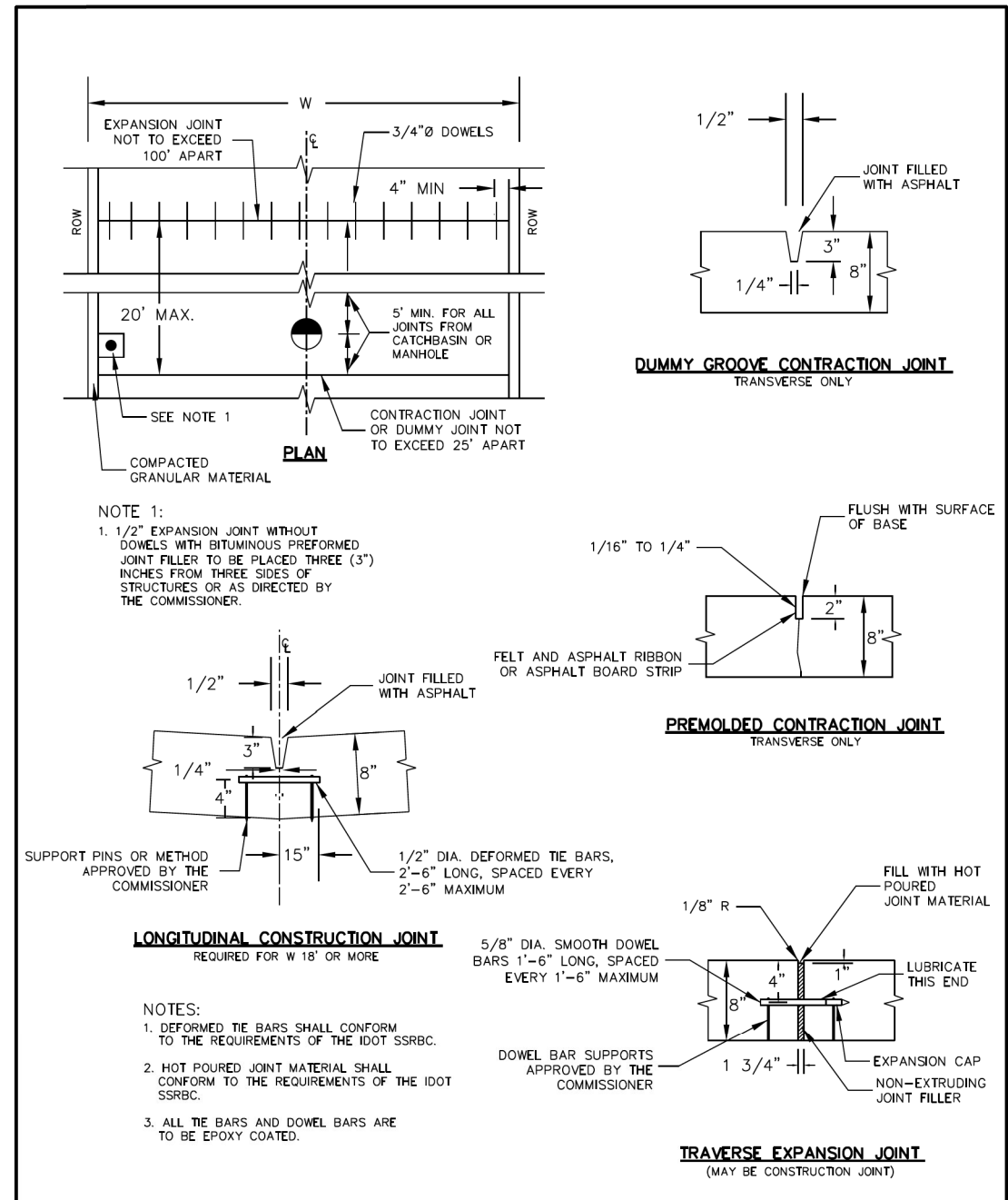
F.A.I. RTE. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 342
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

DATE	
BY	
PLAN	
DATE	
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NOTE BOOK	
NO.	
DATE	
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NOTE BOOK	
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DATE	
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PROFILE	
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NOTE BOOK	
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DATE	
BY	
NOTE BOOK	
NO.	



	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	CONCRETE CURB & GUTTER DETAIL		
	DATE	SHEET	DRAWN BY		
	12/12/06	A-2-6	CDOT		



	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	PORTLAND CEMENT CONCRETE ALLEY		
	DATE	SHEET	DRAWN BY		
	12/21/06	A-2-9	CDOT		

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PARSONS BRINCKERHOFF

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	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

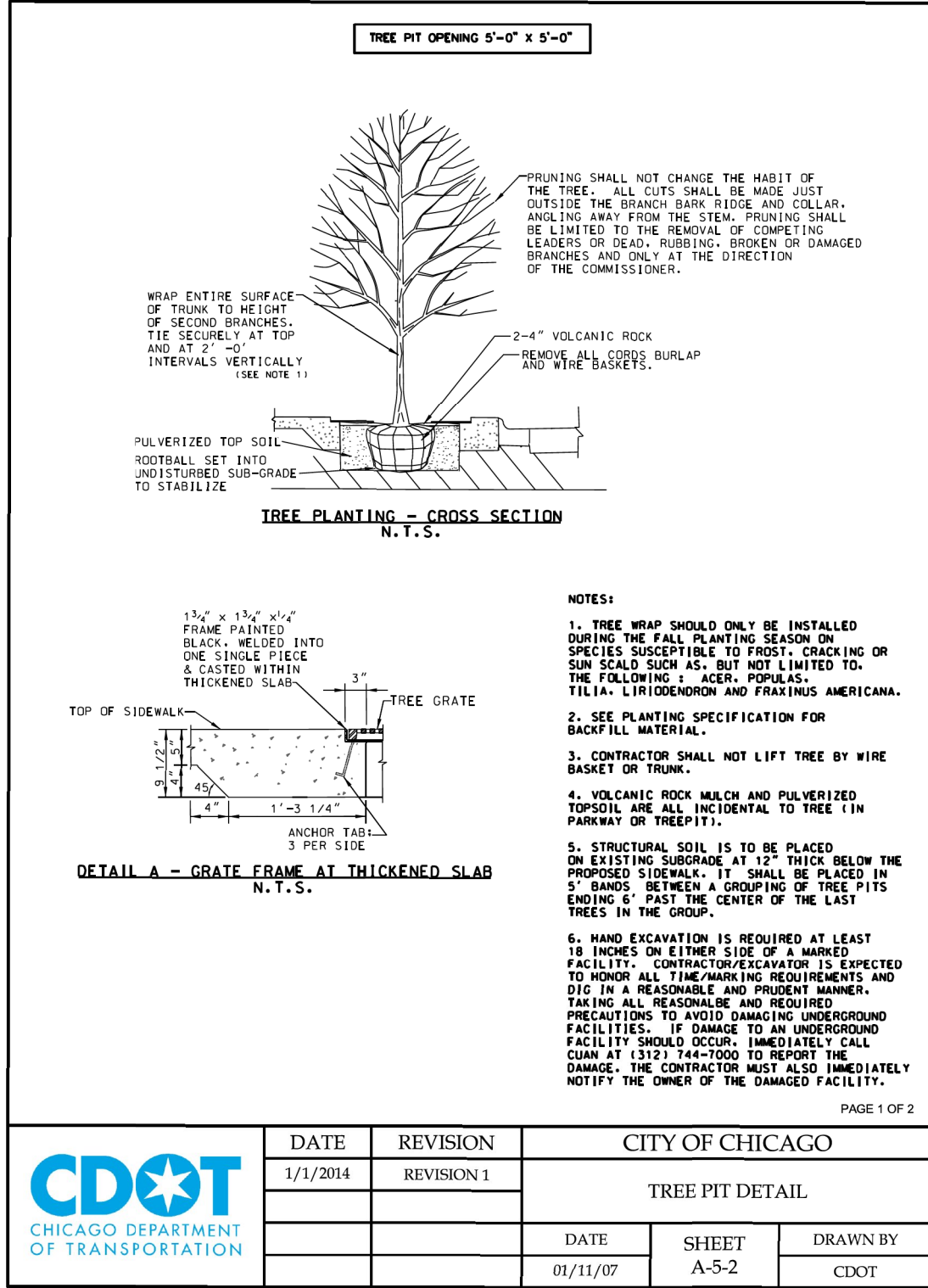
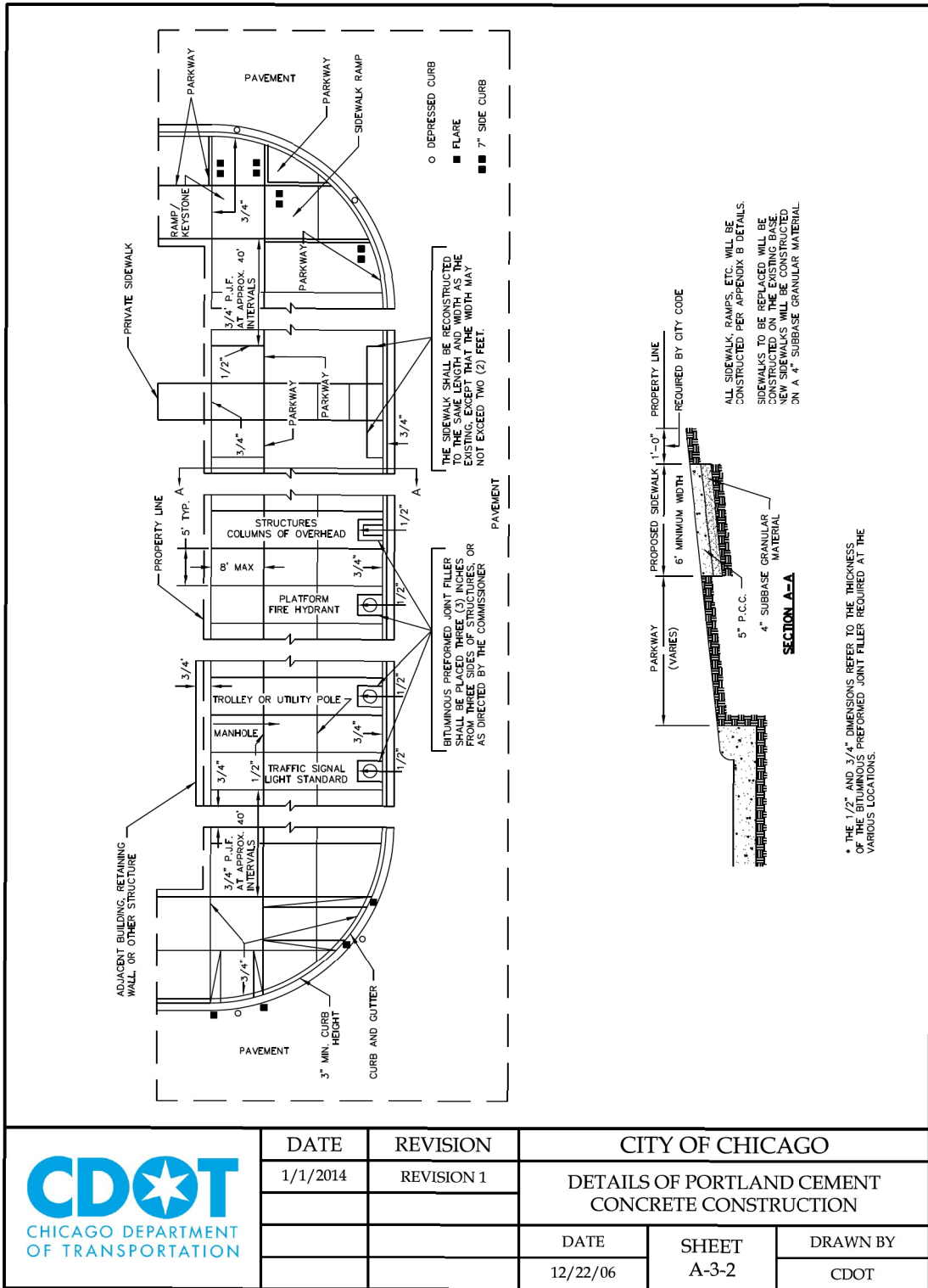
CHICAGO DEPARTMENT OF TRANSPORTATION STANDARDS			
SCALE: N.T.S.	SHEET NO. 2 OF 7 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	2014-016R&B	COOK	378	343
CONTRACT NO. 60X95				ILLINOIS FED. AID PROJECT

PLAN	SUBMITTED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	NO. _____		
	FILE NAME		

PROFILE	SUBMITTED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS OK'D		
	NO. _____		
	FILE NAME		

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- NOTES:**
- TREE WRAP SHOULD ONLY BE INSTALLED DURING THE FALL PLANTING SEASON ON SPECIES SUSCEPTIBLE TO FROST, CRACKING OR SUN SCALD SUCH AS, BUT NOT LIMITED TO, THE FOLLOWING: ACER, POPULAS, TILIA, LIRIODENDRON AND FRAXINUS AMERICANA.
 - SEE PLANTING SPECIFICATION FOR BACKFILL MATERIAL.
 - CONTRACTOR SHALL NOT LIFT TREE BY WIRE BASKET OR TRUNK.
 - VOLCANIC ROCK MULCH AND PULVERIZED TOPSOIL ARE ALL INCIDENTAL TO TREE (IN PARKWAY OR TREETPIT).
 - STRUCTURAL SOIL IS TO BE PLACED ON EXISTING SUBGRADE AT 12" THICK BELOW THE PROPOSED SIDEWALK. IT SHALL BE PLACED IN 5' BANDS BETWEEN A GROUPING OF TREE PITS ENDING 6" PAST THE CENTER OF THE LAST TREES IN THE GROUP.
 - HAND EXCAVATION IS REQUIRED AT LEAST 18 INCHES ON EITHER SIDE OF A MARKED FACILITY. CONTRACTOR/EXCAVATOR IS EXPECTED TO HONOR ALL TIME/MARKING REQUIREMENTS AND DIG IN A REASONABLE AND PRUDENT MANNER, TAKING ALL REASONABLE AND REQUIRED PRECAUTIONS TO AVOID DAMAGING UNDERGROUND FACILITIES. IF DAMAGE TO AN UNDERGROUND FACILITY SHOULD OCCUR, IMMEDIATELY CALL 312) 744-7000 TO REPORT THE DAMAGE. THE CONTRACTOR MUST ALSO IMMEDIATELY NOTIFY THE OWNER OF THE DAMAGED FACILITY.

PAGE 1 OF 2

	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	DETAILS OF PORTLAND CEMENT CONCRETE CONSTRUCTION		
	DATE	SHEET	DRAWN BY		
	12/22/06	A-3-2	CDOT		

	DATE	REVISION	CITY OF CHICAGO		
	1/1/2014	REVISION 1	TREE PIT DETAIL		
	DATE	SHEET	DRAWN BY		
	01/11/07	A-5-2	CDOT		

PARSONS BRINCKERHOFF

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PLOT DATE = 4/18/2017	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION STANDARDS

SCALE: N.T.S. SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	345
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

PLAN	SURVEYED	BY	DATE
	PLOTTED		
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	PLOTTED		
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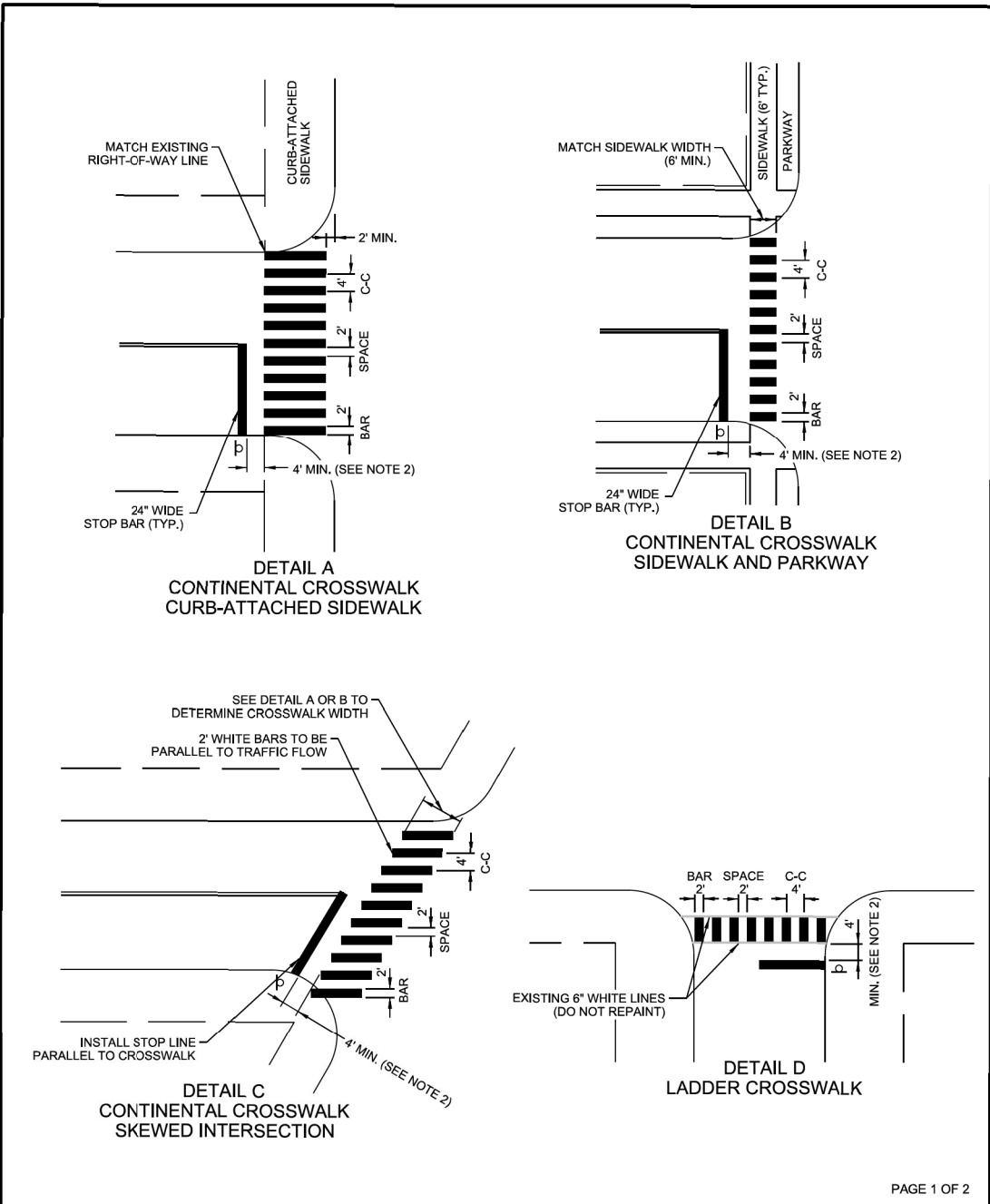
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USER NAME = Kucharski	DESIGNED - MCC	REVISED -
	DRAWN - MCC	REVISED -
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PLOT DATE = 4/18/2017	DATE - 05/02/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CHICAGO DEPARTMENT OF TRANSPORTATION STANDARDS	
SCALE: N.T.S.	SHEET NO. 6 OF 7 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	347
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

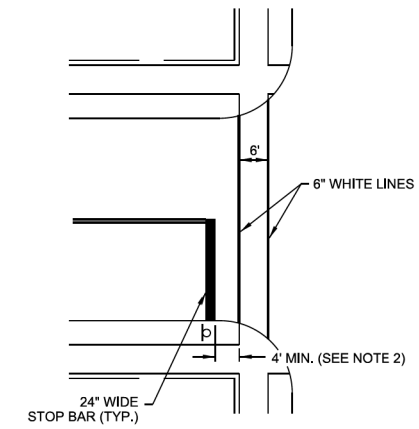


CDOT CHICAGO DEPARTMENT OF TRANSPORTATION	DATE	REVISION	CITY OF CHICAGO	
	1/1/2014	REVISION 1	CROSSWALK MARKING DETAIL	
	DATE	SHEET	DRAWN BY	
	06/25/12	A-7-1A	CDOT	

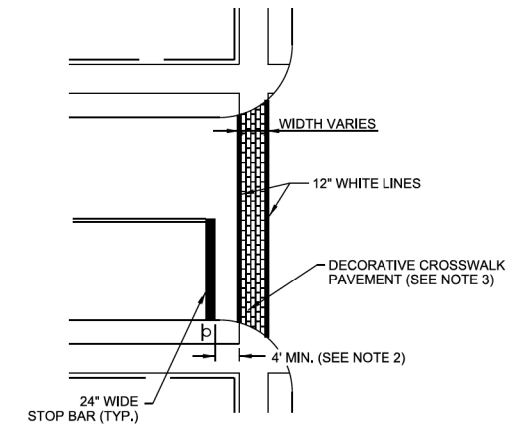
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	PLOTTED		
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	GRADES CHECKED		
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DETAIL E
TRANSVERSE LINE CRSSWALK



DETAIL F
DECORATIVE CRSSWALK

GENERAL NOTES:

- DO NOT INSTALL NEW CROSSWALKS ACROSS ARTERIAL OR COLLECTOR STREETS WITHOUT COORDINATING WITH CDOT TRAFFIC ENGINEERS.
- STOP LINE SHOULD BE PLACED A MINIMUM OF 4' IN ADVANCE OF A CROSSWALK. AT THE ENGINEER'S DISCRETION, THE STOP BAR MAY BE MORE THAN 4' FROM THE CROSSWALK TO AVOID ENCROACHMENT BY TURNING VEHICLES.
- ALL RESTORATION OF DECORATIVE CROSSWALK PAVEMENT SHALL BE COORDINATED WITH CDOT PROJECT DEVELOPMENT
- STRIPING SHOULD BE PLACED TO AVOID WHEEL PATHS FOR LONGEVITY.

PAGE 2 OF 2



DATE	REVISION	CITY OF CHICAGO		
1/1/2014	REVISION 1	CROSSWALK MARKING DETAIL		
		DATE	SHEET	DRAWN BY
		06/25/12	A-7-1B	CDOT

**PARSONS
BRINCKERHOFF**

USER NAME = Kucharski	DESIGNED - MCC	REVISED -
	DRAWN - MCC	REVISED -
PLOT SCALE = 1"=50'	CHECKED - RT	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

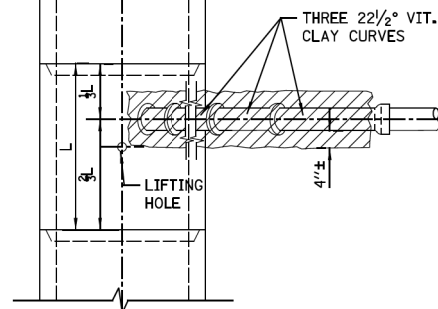
CHICAGO DEPARTMENT OF TRANSPORTATION
STANDARDS

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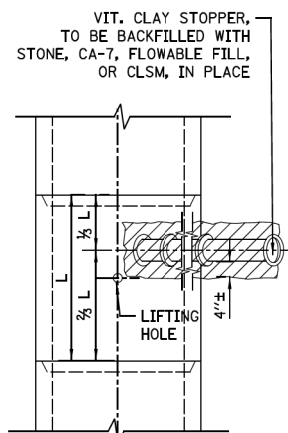
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FAI 90/94	2014-016R&B	COOK	378	348
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60X95	

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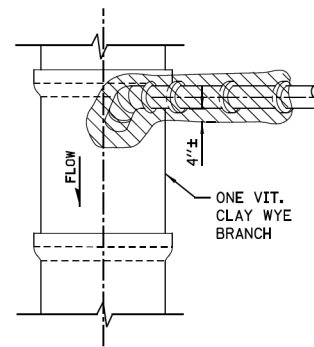
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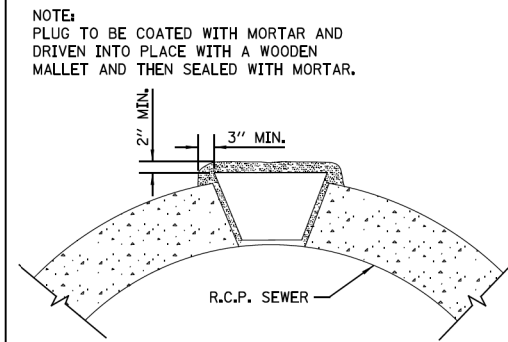
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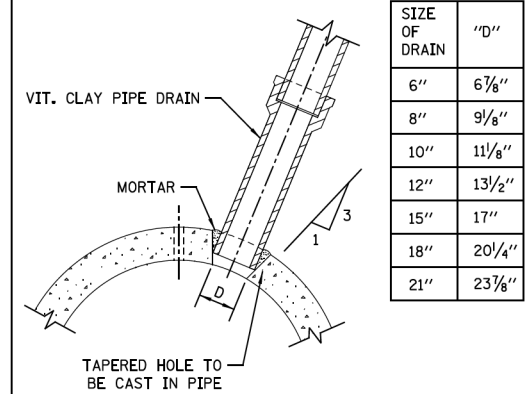
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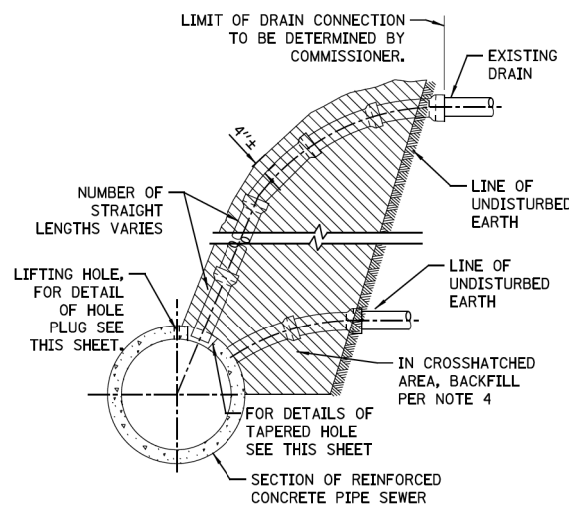
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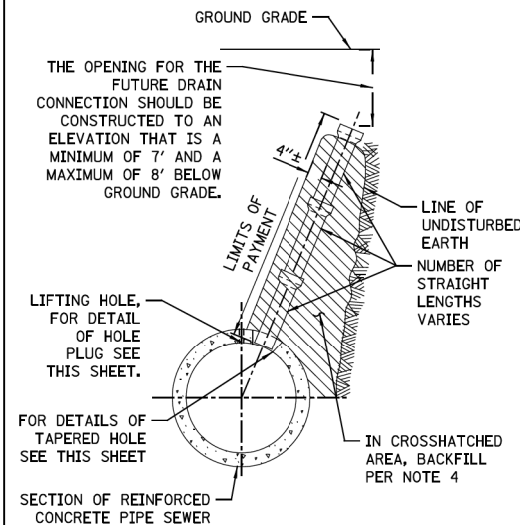
LIFTING HOLE PLUG DETAIL FOR CONCRETE PIPE



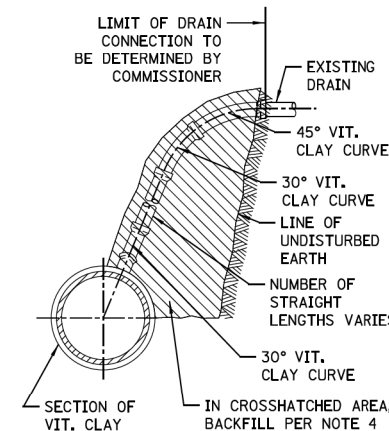
DETAIL OF TAPERED HOLE



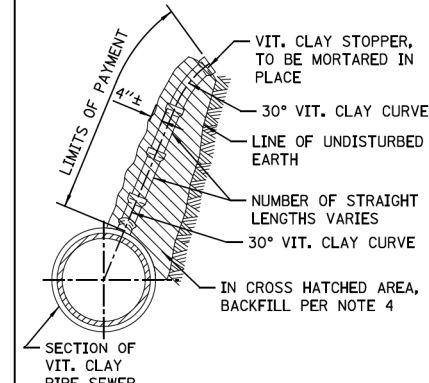
TYPICAL DRAIN CONNECTIONS FOR EXISTING DRAINS



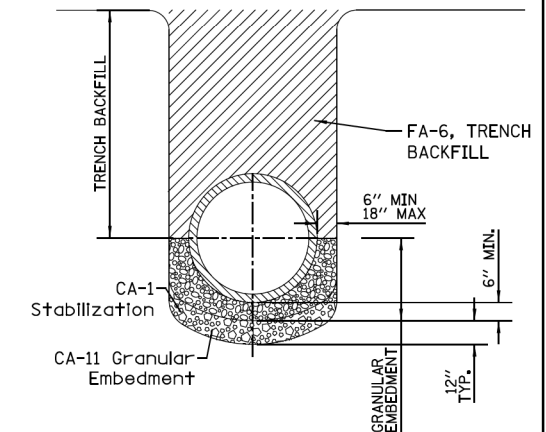
TYPICAL DRAIN STACKS FOR FUTURE USE



TYPICAL DRAIN CONNECTIONS FOR EXISTING DRAINS



TYPICAL DRAIN STACKS FOR FUTURE USE



NOTE:
 1. FOR TRENCH BACKFILL, USE FA-6 SAND, CRUSHED CONCRETE SAND OR STONE SAND.
 2. FOR GRANULAR EMBEDMENT, USE CA-11, CRUSHED GRAVEL, CRUSHED STONE, OR CRUSHED CONCRETE.
 3. 12" OF CA-1 STONE IS ONLY REQUIRED WHEN UNSTABLE MATERIAL IS ENCOUNTERED AT TRENCH BOTTOM.

SEWER TRENCH DETAIL

NOTES:

1. ALL DRAIN CONNECTION JOINTS MUST BE MADE AS SPECIFIED IN SPECIFICATIONS..
2. FOR DUCTILE IRON PIPE DRAIN CONNECTIONS SEE SHEET NO. A.2.
3. FOR ALL GRANULAR EMBANKMENT, USE CA-7 OR CA-11
4. FOR BACKFILL OF HATCHED SUPPORT AREAS, USE CONCRETE, CA-11, FLOWABLE FILL, OR CLSM.

\$FILE\$
\$DATE\$

STANDARD REVISIONS	
DATE	DESCRIPTION
2/24/15	APPROVED PLAN

PERCENT COMPLETE	DATE
30	
60	
75	
90	
100	
BULLETIN	

CITY OF CHICAGO
DEPARTMENT OF WATER MANAGEMENT
BUREAU OF ENGINEERING SERVICES

DRAWN	SIZE
CHECKED	NO.
NO. OF SD	
REVISIONS	
	A.1
	OF
	PN

VITRIFIED CLAY PIPE DRAIN CONNECTIONS

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PARSONS BRINCKERHOFF

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	DRAWN - MCC	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
WATER MANAGEMENT STANDARD DETAILS

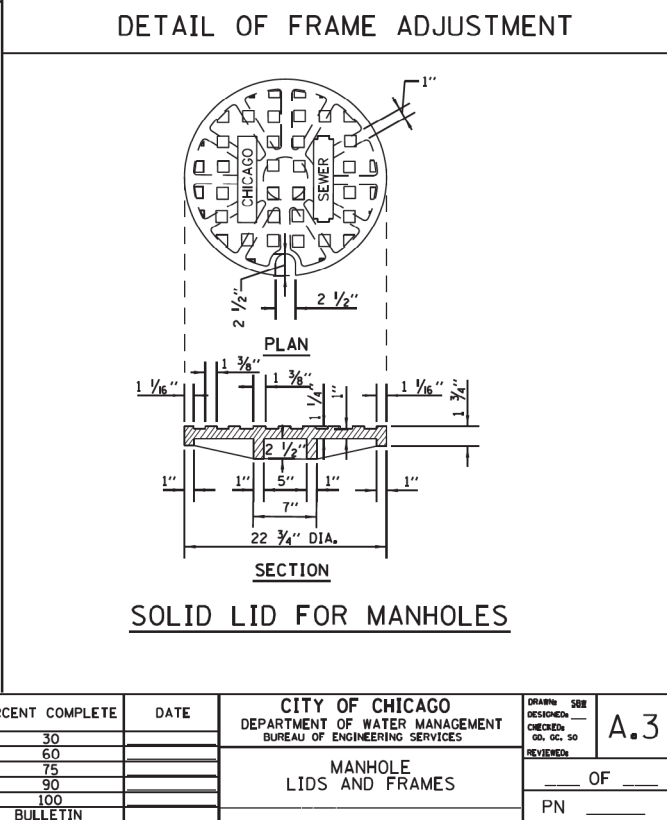
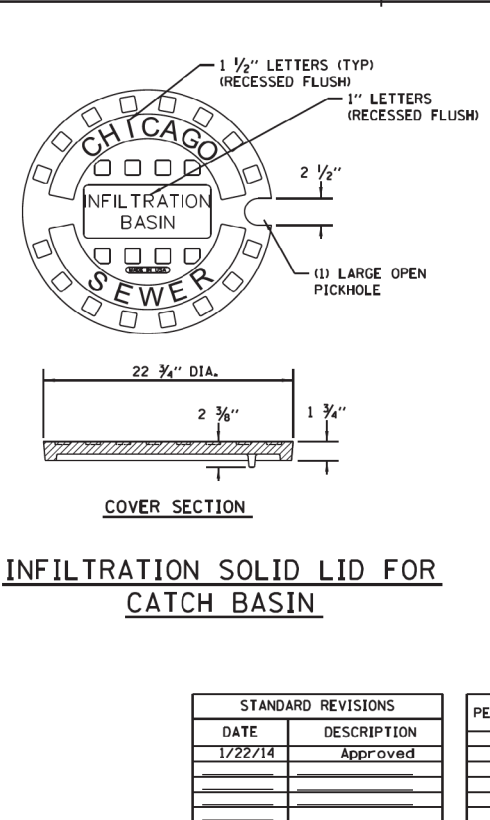
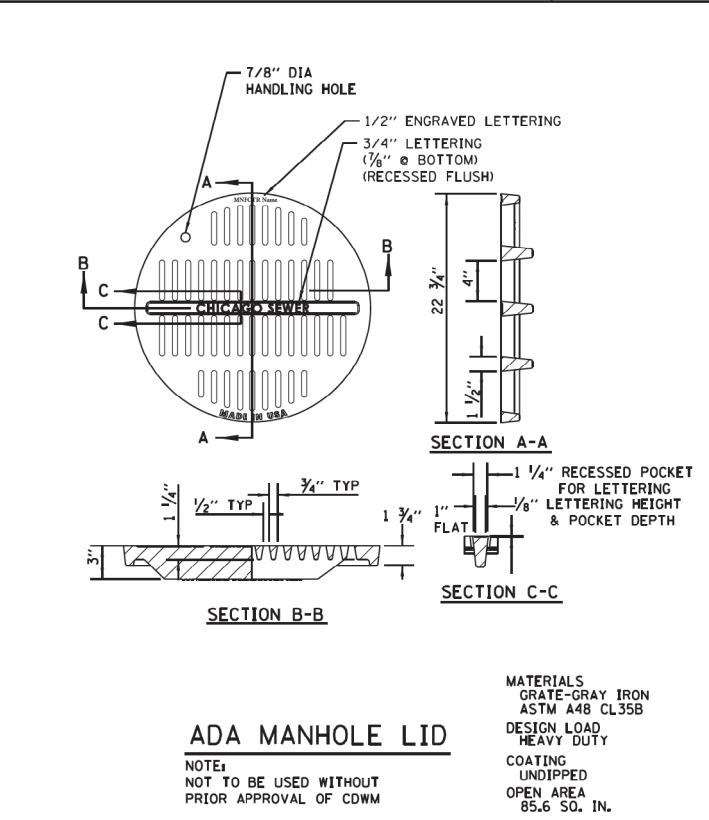
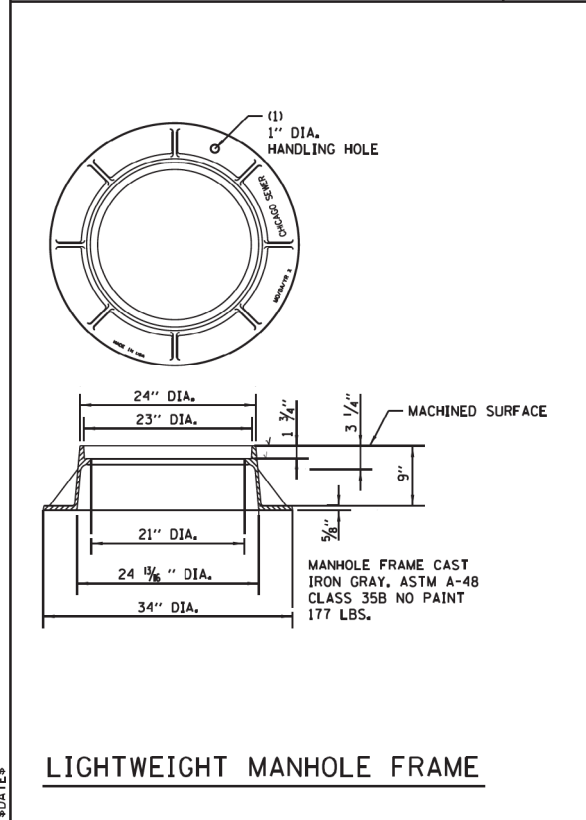
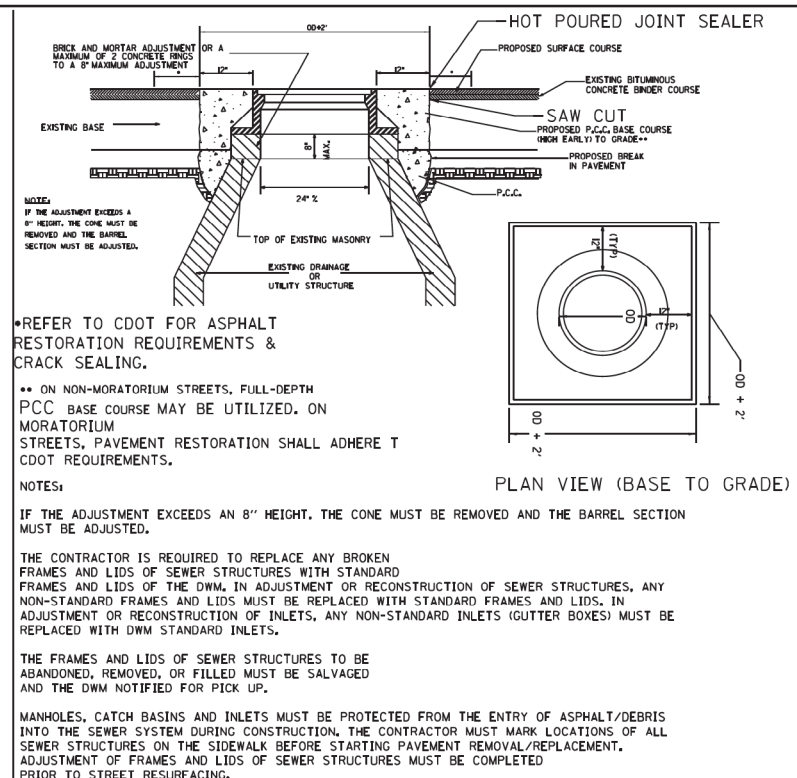
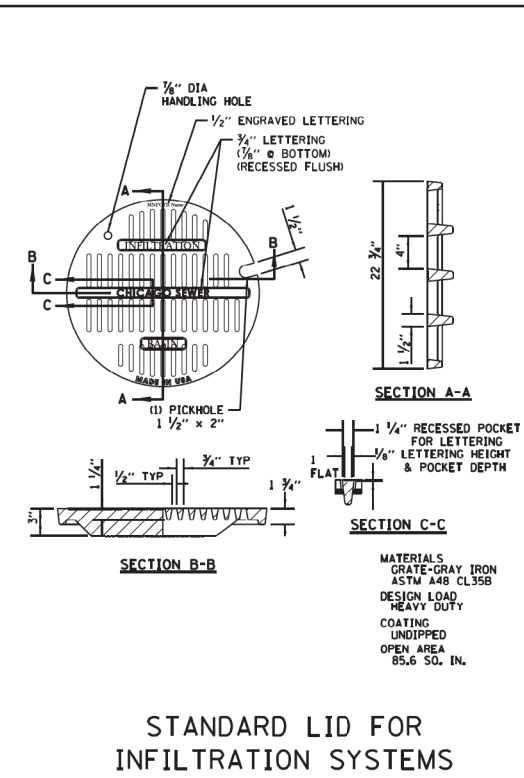
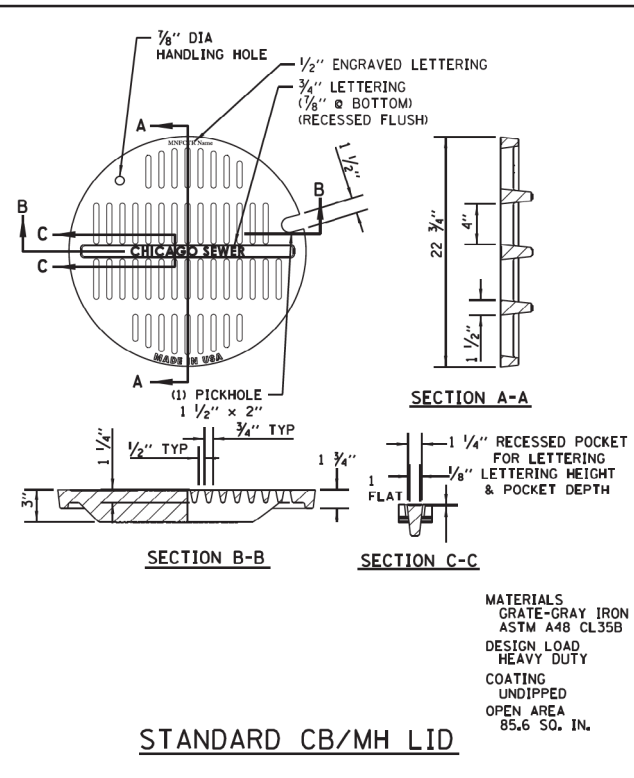
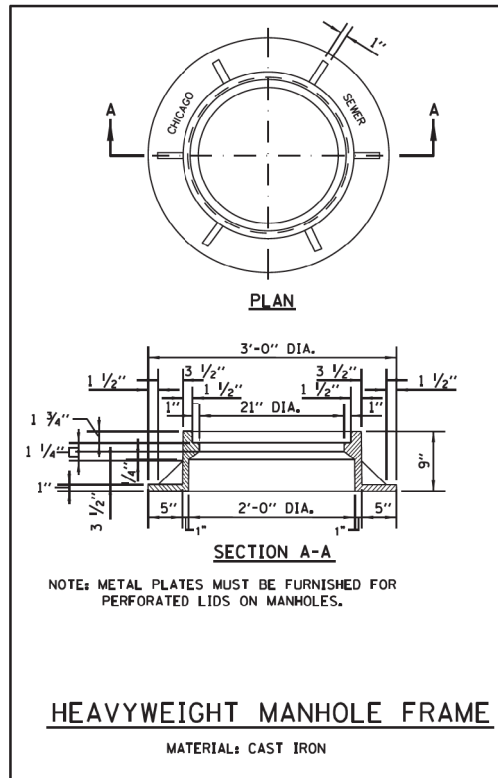
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	349
				CONTRACT NO. 60X95
				ILLINOIS FED. AID PROJECT

DATE	
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PLAN	SURVEYED
	PLOTTED
	GRADES CHECKED
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	FILED
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PROFILE	SURVEYED
	PLOTTED
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STANDARD REVISIONS	
DATE	DESCRIPTION
1/22/14	Approved

PERCENT COMPLETE	DATE
30	
60	
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BULLETIN	

CITY OF CHICAGO	
DEPARTMENT OF WATER MANAGEMENT	
BUREAU OF ENGINEERING SERVICES	
MANHOLE LIDS AND FRAMES	

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PARSONS BRINCKERHOFF

USER NAME = Kucharski	DESIGNED - MCC	REVISED -
	DRAWN - MCC	REVISED -
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PLOT DATE = 4/18/2017	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION			
WATER MANAGEMENT STANDARD DETAILS			
SCALE: N.T.S.	SHEET NO. 2 OF 4 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				

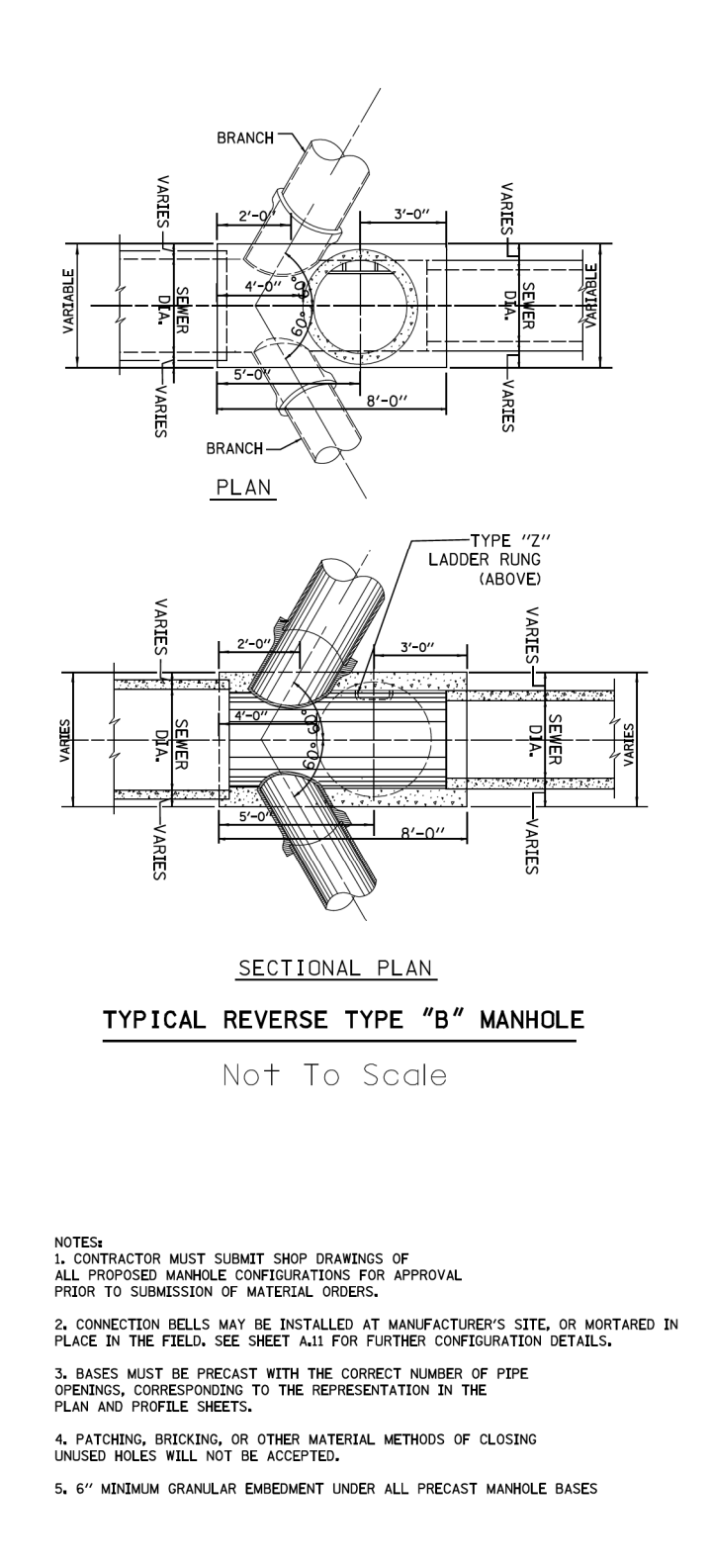
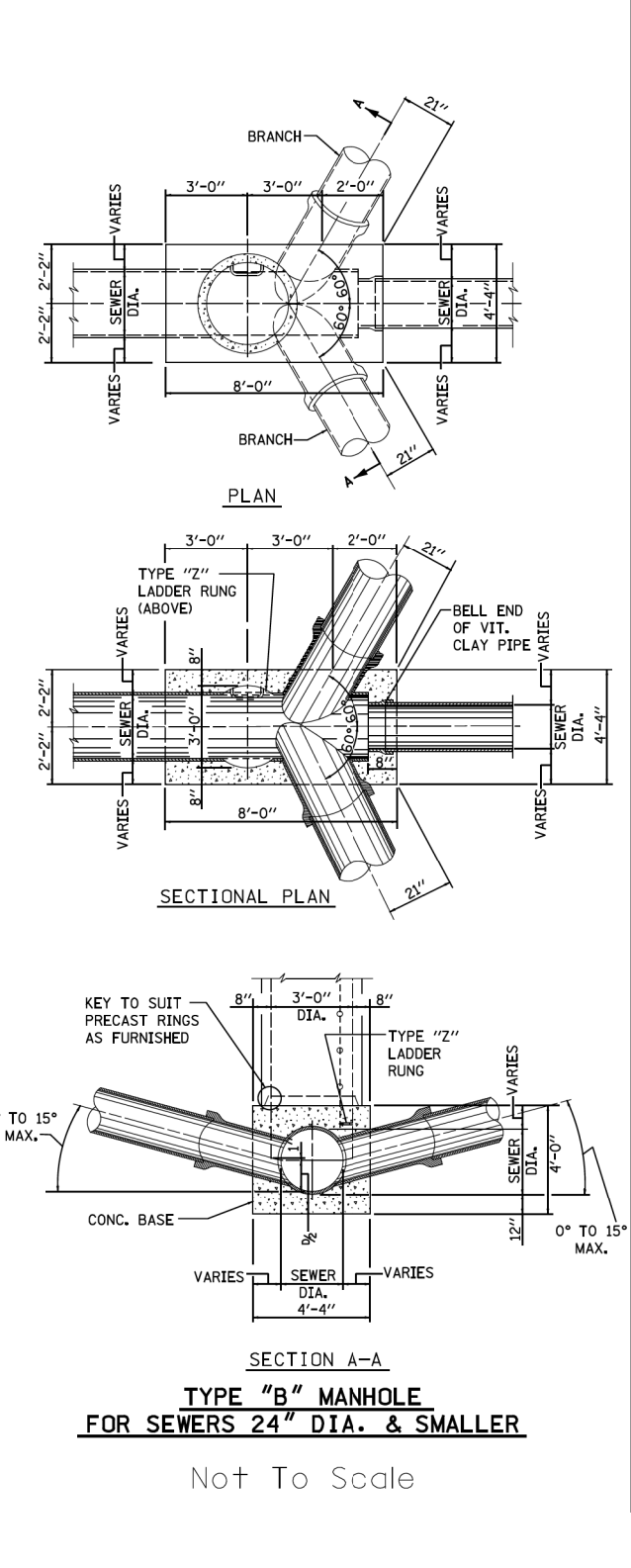
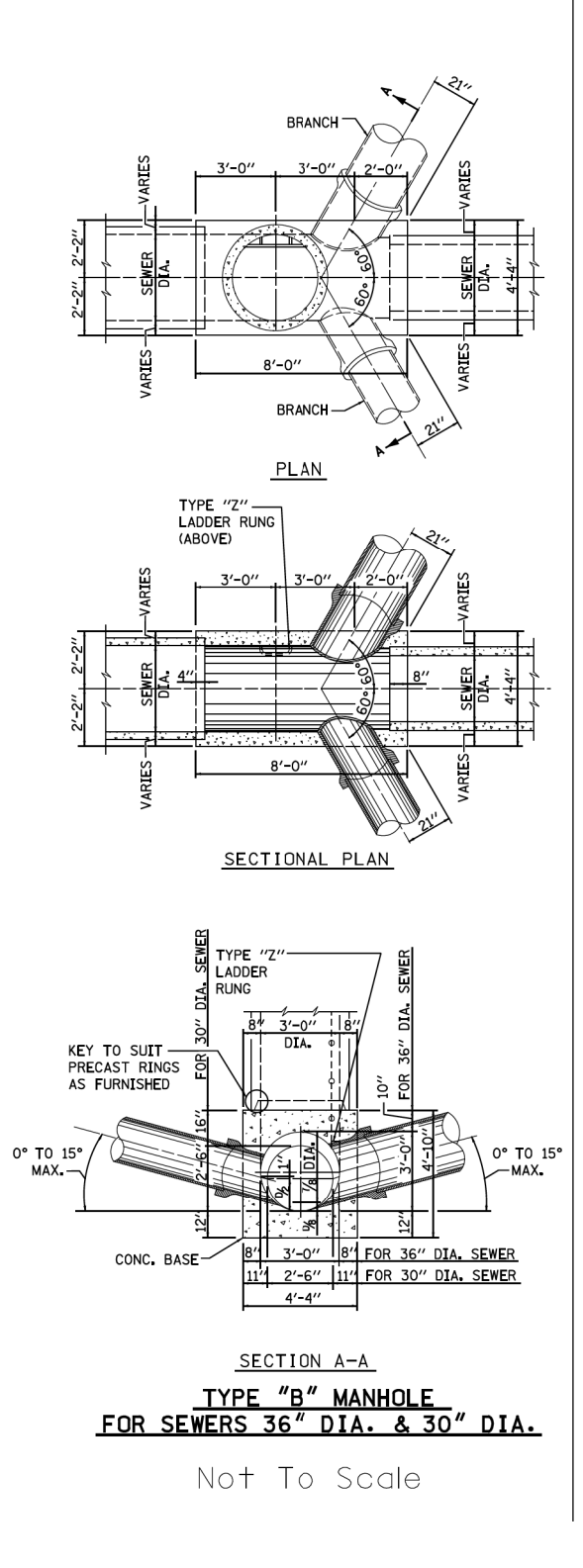
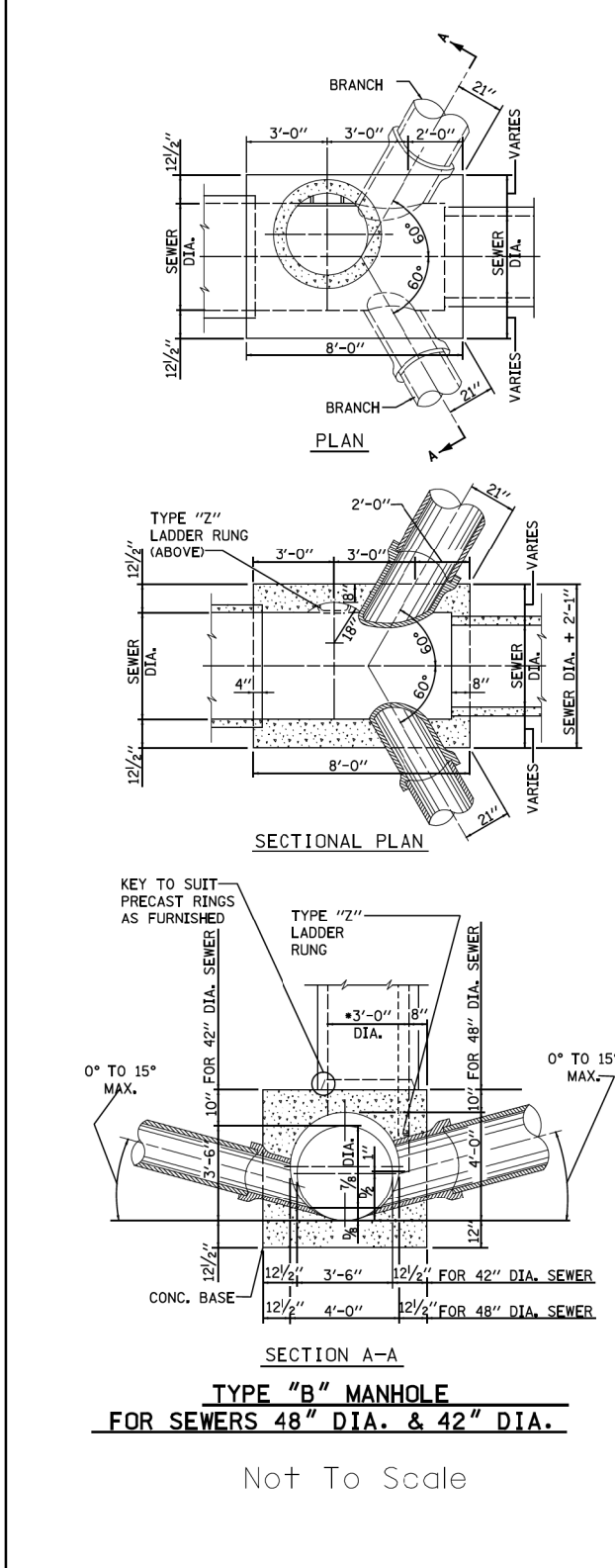
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- NOTES:
1. CONTRACTOR MUST SUBMIT SHOP DRAWINGS OF ALL PROPOSED MANHOLE CONFIGURATIONS FOR APPROVAL PRIOR TO SUBMISSION OF MATERIAL ORDERS.
 2. CONNECTION BELLS MAY BE INSTALLED AT MANUFACTURER'S SITE, OR MORTARED IN PLACE IN THE FIELD. SEE SHEET A.11 FOR FURTHER CONFIGURATION DETAILS.
 3. BASES MUST BE PRECAST WITH THE CORRECT NUMBER OF PIPE OPENINGS, CORRESPONDING TO THE REPRESENTATION IN THE PLAN AND PROFILE SHEETS.
 4. PATCHING, BRICKING, OR OTHER MATERIAL METHODS OF CLOSING UNUSED HOLES WILL NOT BE ACCEPTED.
 5. 6" MINIMUM GRANULAR EMBEDMENT UNDER ALL PRECAST MANHOLE BASES

STANDARD REVISIONS		PERCENT COMPLETE	DATE	CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES	DRAWN DESIGNED CHECKED REVIEWED	SCALE A.9
DATE	DESCRIPTION					
1/5/15	APPROVED PLAN	30		TYPE "B" MANHOLE	OF	PN
		60				
		75				
		90				
		100				
		BULLETIN				

PARSONS
BRINCKERHOFF

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	DRAWN - MCC	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

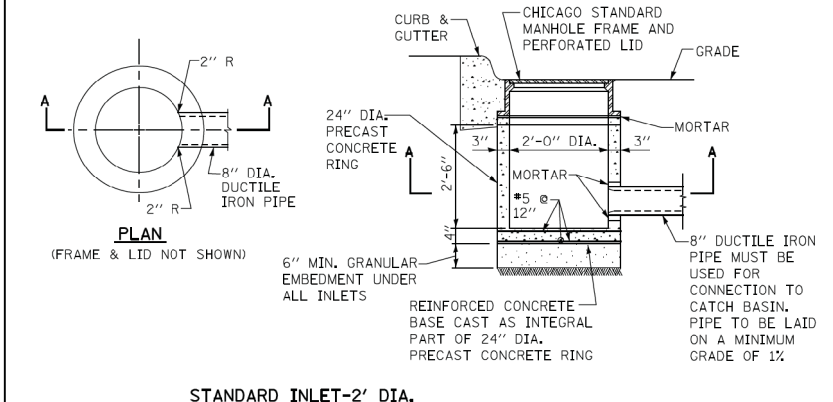
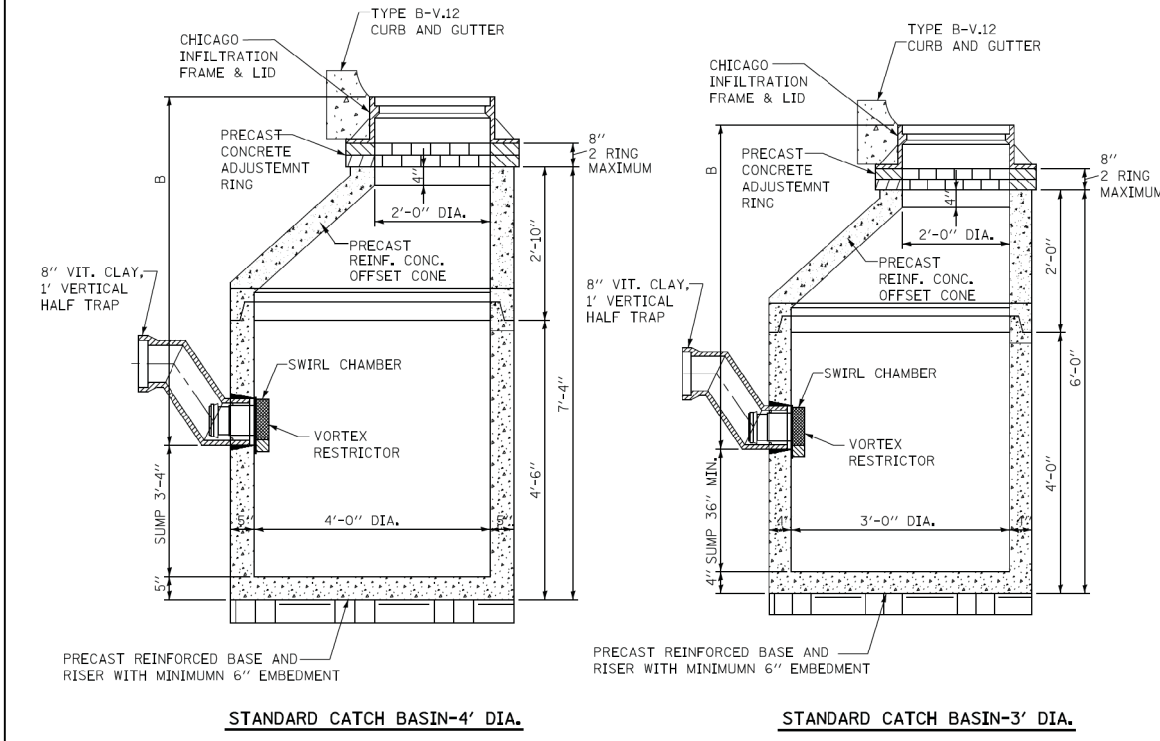
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WATER MANAGEMENT STANDARD DETAILS		FAI 90/94	2014-016R&B	COOK	378	350b
SCALE: N.T.S.	SHEET NO. 2d OF 4 SHEETS	STA.	TO STA.		CONTRACT NO. 60X95	
				ILLINOIS FED. AID PROJECT		

DATE	
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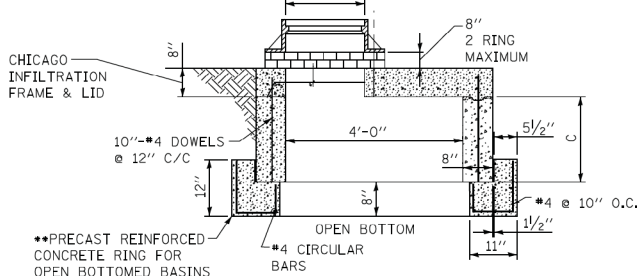
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STANDARD DRAINAGE STRUCTURES FOR PUBLIC STREETS



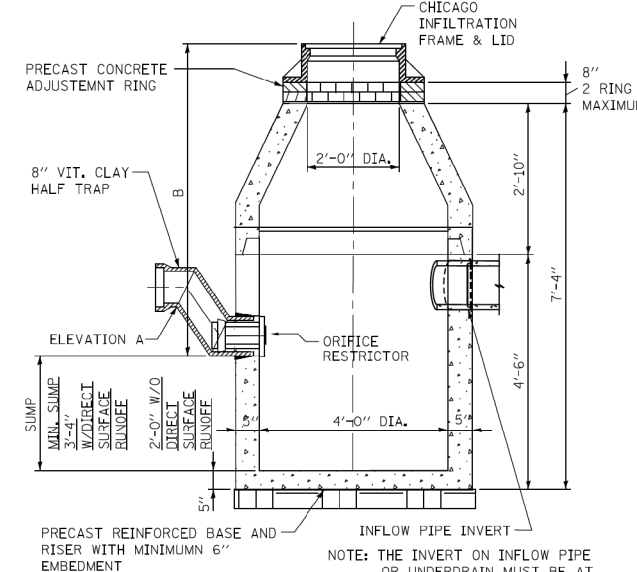
NOTE:
INLETS AND 3' DIAMETER CATCH BASINS ARE TO BE USED ONLY WITH PRIOR APPROVAL OF DWM SEWER ENGINEER OR FIELD INSPECTOR.

STANDARD DRAINAGE STRUCTURES FOR PUBLIC ALLEYS



GREEN ALLEY OPEN BOTTOM CATCH BASIN
TO BE USED IF STANDARD CONE WITH REQUIRED SUMP DEPTH INSTALLATION IS IMPOSSIBLE.

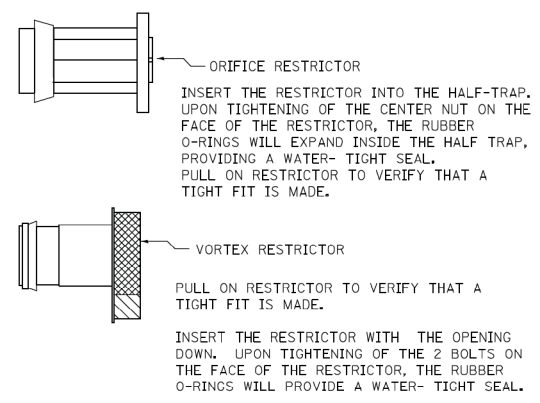
- IF FLAT TOP SLAB CATCH BASIN IS USED IN AN OPEN BOTTOM APPLICATION, THE FOLLOWING CRITERIA MUST BE FOLLOWED:
- "C" MUST BE A MINIMUM OF 3 FEET.
- PROVIDE A MINIMUM AGGREGATE BASE OF 1.0 FEET BELOW PRECAST REINFORCED CONCRETE RING.
- GEOTECH FABRIC MUST BE PLACED ON SIDES AND BOTTOM OF AGGREGATE SURROUNDING BASIN.
- AGGREGATE BASE MUST PROVIDE ADEQUATE STORMWATER STORAGE CAPACITY PER THE APPROVED/PERMITTED PLANS.



CATCH BASIN-ORIFICE RESTRICTOR
N.T.S.

- NOTES:
1. FOR ANY DRAIN TILE CONNECTION, THE WATER TABLE MUST BE AT LEAST 3.5 FEET BELOW ELEVATION A.
 2. PVC UNDERDRAINS MUST BE 4" MINIMUM DIAMETER, (6" RECOMMENDED) AND MUST BE WRAPPED IN GEOTECH FABRIC.

DRAINAGE STRUCTURES RESTRICTORS



- GENERAL NOTES:**
1. CATCH BASIN TO CATCH BASIN CONNECTIONS ARE ALLOWED IN PRIVATE SITES & ALLEYS. ONLY THE DOWNSTREAM CATCH BASIN IS REQUIRED TO HAVE A HALF-TRAP.
 2. IF B < 4 FEET, THEN USE A DUCTILE IRON PIPE HALF TRAP AND FLAT TOP SLAB CATCH BASIN AS NECESSARY.
 3. INLETS AND 3' DIAMETER CATCH BASINS ARE TO BE USED ONLY WITH PRIOR APPROVAL OF DWM FIELD INSPECTOR.

RESTRICTOR NOTES:
THE DWM'S RAIN BLOCKER RESTRICTOR PROGRAM MUST BE MAINTAINED WITH ANY ROADWAY IMPROVEMENT.
THE DESIGN OF ANY ROADWAY IMPROVEMENT MUST CONSIDER LIMITING THE NUMBER OF CATCH BASINS TO THE EXTENT PRACTICAL. THE NUMBER OF EXISTING STRUCTURES SHOULD NOT BE INCREASED.

THE RESTRICTORS CAN BE OBTAINED FROM DWM CENTRAL DISTRICT AT 3901 S. ASHLAND AVE. THE CONTRACTOR SHOULD ARRANGE FOR PICK UP BY CONTACTING 312-747-1177 (7AM TO 3PM, M-F)

FLOW RESTRICTORS MUST BE INSTALLED IN ALL CATCH BASINS OUTSIDE OF THE CENTRAL BUSINESS DISTRICT. RESTRICTORS MUST NOT BE INSTALLED IN CATCH BASINS IN CLOSE PROXIMITY TO VIADUCT AREAS, BUS STOPS, OR EMERGENCY ENTRANCES. THE DWM MUST APPROVE THE NON-INSTALLATION OR REMOVAL OF ANY RESTRICTOR. REQUIREMENTS FOR RESTRICTOR INSTALLATION ARE AS FOLLOWS:

- ARTERIAL STREETS: 3-INCH ORIFICE RESTRICTOR
- BUS ROUTES: 3-INCH ORIFICE RESTRICTOR
- RESIDENTIAL STREETS: 3-INCH VORTEX RESTRICTOR
- ALLEYS: 3-INCH ORIFICE RESTRICTOR IN THE LAST CB.
- CLOSED LIDS ARE REQUIRED ON ALL MANHOLES EXCEPT AT INTERSECTIONS WHERE A PERFORATED LID SHALL BE USED.

STANDARD REVISIONS		PERCENT COMPLETE	DATE	CITY OF CHICAGO		DATE	REVISION
DATE	DESCRIPTION			DEPARTMENT OF WATER MANAGEMENT			
				BUREAU OF ENGINEERING SERVICES			
1/22/14	APPROVED	30		DRAINAGE STRUCTURE DETAILS			A.15
		60					
		75					
		90					
		100					
		BULLETIN					

PARSONS BRINCKERHOFF

USER NAME = Kucharski	DESIGNED - MCC	REVISED -
	DRAWN - MCC	REVISED -
PLOT SCALE = 1"=50'	CHECKED - RT	REVISED -
PLOT DATE = 4/18/2017	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

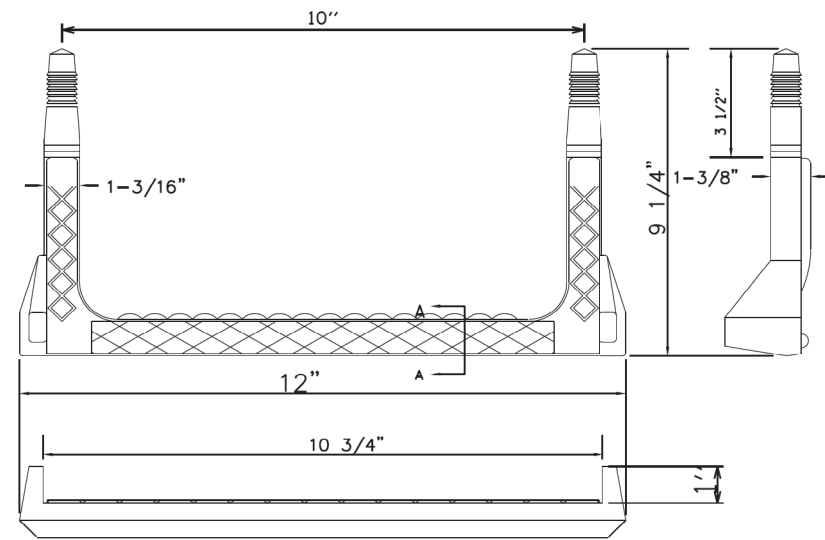
CHICAGO DEPARTMENT OF TRANSPORTATION
WATER MANAGEMENT STANDARD DETAILS
SCALE: N.T.S. SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I. RT. 90/94	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 351
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

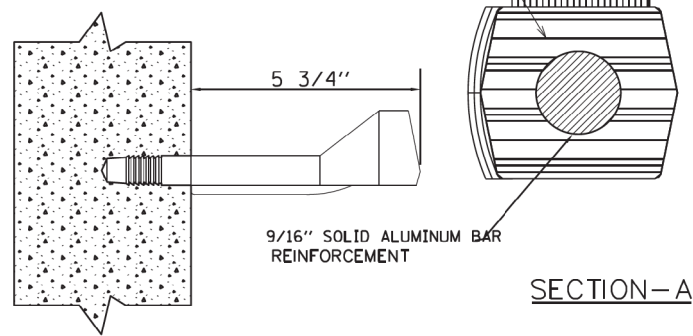
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	PLOTTED	
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	CHECKED	
	BY	
	NO.	
	FILE NAME	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
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	FILE NAME	

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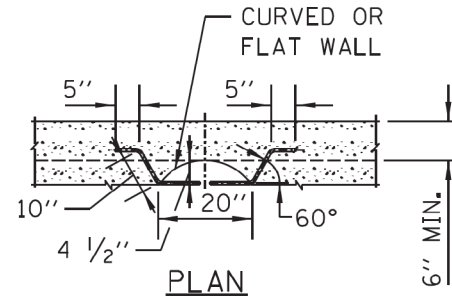


COPOLYMER POLYPROPYLENE PLASTIC

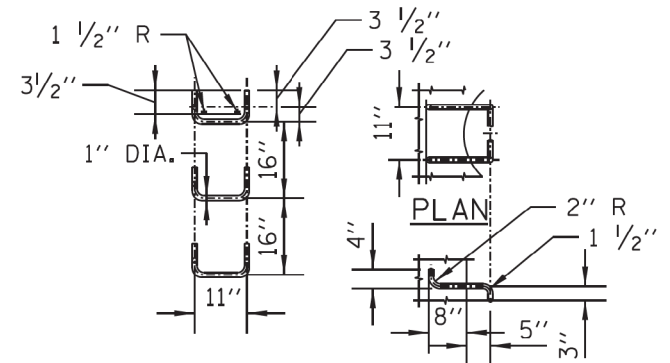


STEP LADDER RUNG

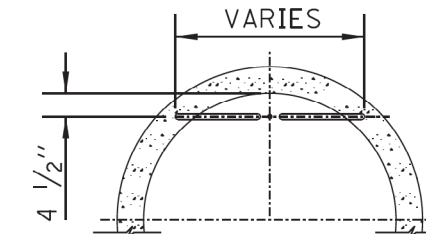
- NOTES:
1. VERTICAL SPACING = 16" O.C., ON VERTICAL WALL ONLY.
 2. STEPS TO PROTRUDE MINIMUM 5" FROM FACE OF STRUCTURE WALL.
 3. STEPS SHALL MEET THE REQUIREMENTS OF ASTM C478 IN ADDITION TO A HORIZONTAL PULL-OUT LOAD OF 1000 LBS. WHEN INSTALLED.
 4. ALL STEPS SHALL BE VERTICALLY ALIGNED IN A STRAIGHT LINE.
 5. NO STEPS LOCATED INSIDE MANHOLE CHIMNEY.
 6. MINIMUM CONCRETE STRENGTH MUST BE 3000 PSI
 7. HOLES- PREFORMED/DRILLED
 - A. HOLES MUST BE PARALLEL
 - B. HOLES MUST BE 10" CENTERED, 1" DIAMETER
 - C. MINIMUM DEPTH- 3 1/2" TO 3 3/4"



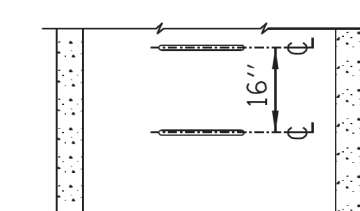
ELEVATION TYPE X



SPACING ELEVATION HANDHOLD-TYPE Z RUNG



PLAN



ELEVATION TYPE Y

PERCENT COMPLETE	DATE	CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT BUREAU OF ENGINEERING SERVICES	DESIGNED	SRB
30	1/22/14		CHECKED	
60			REVIEWED	
75				
90				
100		LADDER RUNGS		OF
BULLETIN				A.17

PARSONS BRINCKERHOFF

USER NAME = Kucharski	DESIGNED - MCC	REVISED -
	DRAWN - MCC	REVISED -
PLOT SCALE = 1"=50'	CHECKED - RT	REVISED -
PLOT DATE = 4/18/2017	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION
WATER MANAGEMENT STANDARD DETAILS

SCALE: N.T.S. SHEET NO. 4 OF 4 SHEETS STA. TO STA.

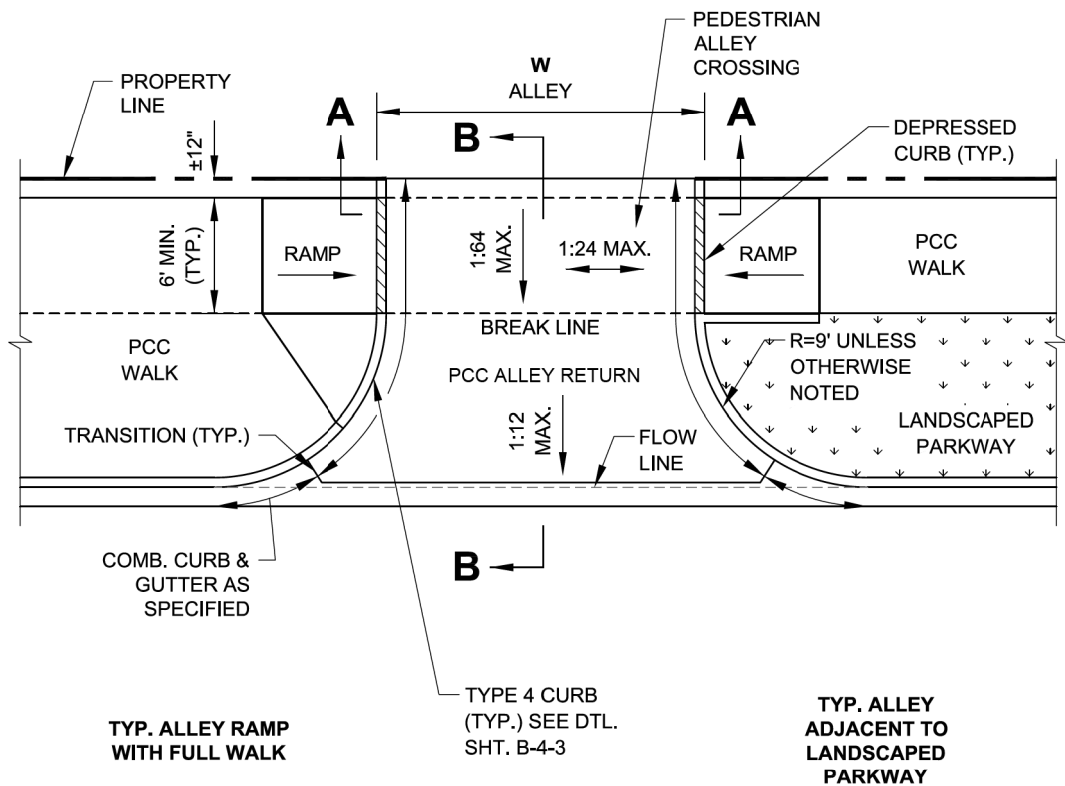
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	352
			CONTRACT NO. 60X95	
ILLINOIS FED. AID PROJECT				

DATE	
BY	
PLAN	SURVEYED
	NOTED
	CHECKED
	FILED
	NO.

DATE	
BY	
PROFILE	SURVEYED
	NOTED
	CHECKED
	FILED
	NO.

NOTES:

- A. DETECTABLE WARNING IS NOT REQUIRED AT ALLEY RETURNS.
- B. RAMP DETAILS ARE DEPENDENT UPON SITE SPECIFIC CONDITIONS (WHERE RAMPS ARE NECESSARY). SEE APPROPRIATE RAMP SHEET AS REQUIRED (TYP.).
- C. SEE DETAIL B-2-5 FOR REDUCED WIDTH (4' MIN.) PEDESTRIAN ACCESSIBLE ROUTE ACROSS ALLEY / DRIVEWAY (TO ALLOW FOR ADDITIONAL SLOPE AT ALLEY / DRIVEWAY).



PLAN VIEW

NOTE:
WORK THIS SHEET WITH SHEET B-2-2.

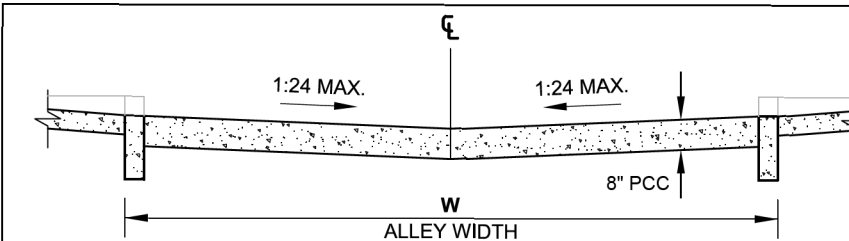


DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5
01/01/14	REVISION 6

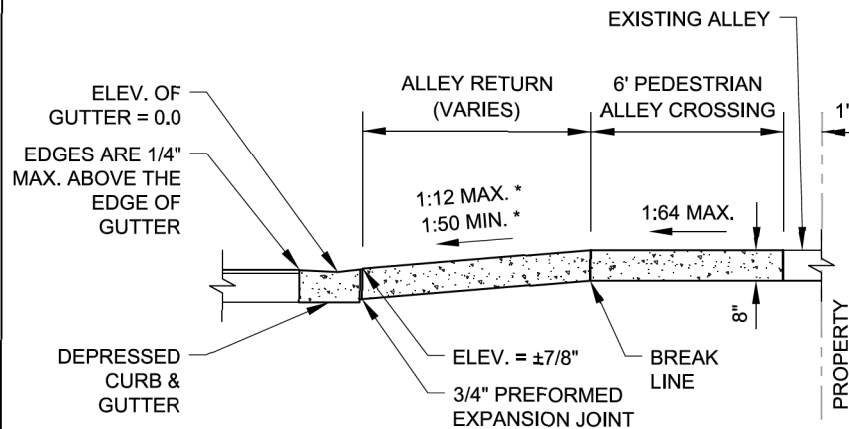
CITY OF CHICAGO
ALLEY RETURN PLAN VIEW
SHEET B-2-1

SCALE: NOT TO SCALE
DATE: 10/23/2006

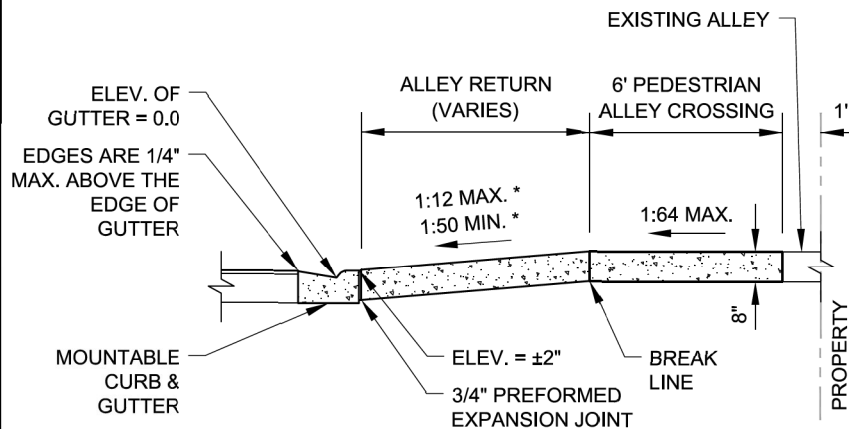
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CHECKED BY: GK



SECTION A-A: CROSS SECTION AT PROPERTY LINE



SECTION B-B: LONGITUDINAL SECTION SHOWING DEPRESSED CURB & GUTTER



SECTION B-B: LONGITUDINAL SECTION SHOWING MOUNTABLE CURB & GUTTER

ALLEY RETURN NOTES:

- A. WHEN A PORTION OF AN EXISTING PAVED ALLEY IS REMOVED TO ADJUST THE ALLEY PAVEMENT TO THE NEW GRADE, 3/4" PREFORMED EXPANSION JOINT MATERIAL SHALL BE PLACED ON THE PROPERTY LINE OTHERWISE THE JOINT IS OMITTED. THE COST OF FURNISHING AND PLACING THE 3/4" PREFORMED EXPANSION JOINT MATERIAL SHALL BE INCLUDED IN THE COST OF THE PORTLAND CEMENT CONCRETE ALLEY RETURN.
 - B. SAWED JOINTS SHALL BE SEALED WITH A POURED MATERIAL MEETING THE REQUIREMENTS OF SECTION 1050 OF THE SSRBC.
- * SLOPE VARIES AND IS NOT GOVERNED BY ADA
- ** THE SLOPE OF THE ALLEY IN THE 1' SPACE ADJACENT TO THE PROPERTY LINE MAY BE ALTERED UP TO 10% TO MEET EXISTING CONDITIONS

NOTE:
WORK THIS SHEET WITH SHEET B-2-1.



DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5
01/01/14	REVISION 6

CITY OF CHICAGO
ALLEY RETURN SECTIONS
SHEET B-2-2

SCALE: NOT TO SCALE
DATE: 10/23/2006

DRAWN BY: CDOT
CHECKED BY: GK

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USER NAME = Kucharski	DESIGNED - MCC	REVISED -
PLOT SCALE = 1"=50'	DRAWN - MCC	REVISED -
PLOT DATE = 4/18/2017	CHECKED - RT	REVISED -
	DATE - 05/02/2017	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CHICAGO DEPARTMENT OF TRANSPORTATION ADA STANDARDS	
SCALE: N.T.S.	SHEET NO. 1 OF 4 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	353
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

PLAN	SUBMITTED	DATE
NOTE BOOK	PLOTTED	BY
NO.	ALIGNMENT CHECKED	
	FILE NAME	

PROFILE	SUBMITTED	DATE
NOTE BOOK	PLOTTED	BY
NO.	GRADES CHECKED	
	STRUCTURE NOTATIONS CHECKED	

GENERAL NOTES:

1. THE DETECTABLE WARNING USED SHALL BE CHOSEN FROM THE CHICAGO DEPARTMENT OF TRANSPORTATION LIST OF APPROVED DETECTABLE WARNING PRODUCTS (AVAILABLE ON THE CITY OF CHICAGO WEBSITE). IT IS NOT ACCEPTABLE TO INSTALL TWO DIFFERENT DETECTABLE WARNING PRODUCTS ADJACENT TO ONE ANOTHER AT ANY LOCATION. IN THE CENTRAL BUSINESS DISTRICT, GRANITE OR OTHER SPECIALTY PAVING MATERIALS MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.
2. THE DETECTABLE WARNING MUST BE INSTALLED A MAXIMUM OF 8" OR LESS FROM FACE OF CURB (SEE DETAIL SHEET B-4-2).
3. THE DETECTABLE WARNING MUST COVER FULL WIDTH OF RAMP EXCLUDING SIDE FLARES FOR A MINIMUM UNOBSTRUCTED DEPTH OF 24". THE DETECTABLE WARNING LOCATED ON THE SURFACES OF RAMPS IS TYPICALLY ORIENTED PERPENDICULAR TO THE RUN OF THE RAMP UNLESS SPECIAL CIRCUMSTANCES OCCUR (SEE DETAIL SHEET B-1-5). THE DETECTABLE WARNING MUST BE PROVIDED FOR A MINIMUM DEPTH OF 24" FOR THE ENTIRE LENGTH OF THE SIDEWALK WHERE THE SIDEWALK IS FLUSH WITH THE STREET (DEPRESSED CURB OR FLUSH TRANSITION). IF IT IS NECESSARY TO CUT A UNIT(S) IN THE PROVISION OF A COMPLIANT RAMP OR SIDEWALK WITH 24" MINIMUM DEPTH OF DETECTABLE WARNING, THE UNITS SHALL BE CUT IN A NEAT AND WORKMAN LIKE MANNER PER MANUFACTURER'S REQUIREMENTS WITH A MINIMUM OF THREE PINS OR ANCHOR POINTS (WHERE APPLICABLE). THE UNITS SHALL BE ARRANGED SO THAT THE CUT UNITS ARE LARGE ENOUGH TO BE PROPERLY AND ADEQUATELY SECURED. CUT UNITS SHALL NOT BE USED UNLESS ALL OTHER DESIGN OPTIONS HAVE BEEN EXHAUSTED. THE USE OF SALVAGE PIECES FROM UNITS THAT ARE CUT WILL NOT BE PERMITTED WITHOUT WRITTEN APPROVAL OF THE COMMISSIONER. CUT UNIT SALVAGE PIECES NOT APPROVED FOR USE MUST BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY.
4. WHERE APPLICABLE, A COMBINATION OF STRAIGHT AND RADIAL DETECTABLE WARNING UNITS MAY BE USED ON COMPOUND AND LARGE RADII. CONTRACTOR MUST MAKE THIS DETERMINATION AND VERIFY IN FIELD.
5. THE DETECTABLE WARNING MUST CONTRAST WITH ADJACENT PAVEMENT. IF LIGHT COLORED PAVEMENT IS USED THE DETECTABLE WARNING COLOR SHALL BE RED. IF A DARK COLORED PAVEMENT IS USED THE DETECTABLE WARNING COLOR SHALL BE YELLOW. CONTRACTOR TO VERIFY THAT PROPER CONTRAST IS OBTAINED.
6. PRIOR TO PLACING CONCRETE FOR DEPRESSED CURBS, RAMPS, OR SIDEWALKS THE CONTRACTOR SHALL VERIFY THAT LAYOUT OR DESIGN COMPLIES WITH THE REQUIREMENTS OF THE CDOT ADA STANDARDS.
7. RAMP WIDTH MUST BE A MINIMUM OF 6'-0" AND IN INCREMENTS OF 1'-0", EXCEPT WHEN USING THE PERPENDICULAR RAMP AT CORNER (OR OTHER SPECIAL CDOT APPROVED CONDITIONS), WHICH HAS A MINIMUM WIDTH OF 4'-0".
8. THE MAXIMUM ALLOWABLE RAMP RUNNING SLOPE IS 1:14, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED. GRADE BREAKS AT THE TOP AND BOTTOM OF RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF RAMP RUN.
9. THE MAXIMUM ALLOWABLE RAMP CROSS SLOPE IS 1:64, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED.
10. THE MAXIMUM ALLOWABLE RAMP LANDING SLOPE IS 1:64, MEASURED AT ANY LOCATION AND IN ANY DIRECTION ON THE LANDING. THE RAMP LANDING WIDTH SHALL MATCH THE FULL WIDTH OF THE RAMP FOR A MINIMUM UNOBSTRUCTED DEPTH OF 4'-0". RAMP LANDINGS SHALL BE PROVIDED AT THE TOP AND/OR BOTTOM OF RAMPS WHERE TURNING IS REQUIRED.
11. RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK. RAMP SIDE FLARES ARE NOT REQUIRED WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS LANDSCAPED OR IS OCCUPIED BY A BARRIER THAT BLOCKS PEDESTRIAN ACCESS. EXCEPTIONS TO THIS RULE MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.



DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5

SCALE: NOT TO SCALE
DATE: 10/23/2006

**CITY OF CHICAGO
GENERAL NOTES
SHEET B-3-2**

DRAWN BY: CDOT
CHECKED BY: LCM

GENERAL NOTES (CONTINUED):

12. UTILITIES, SUCH AS LIGHT POLES, TRAFFIC POLES AND HYDRANTS, MAY BE LOCATED IN THE FLARE OF THE RAMP BUT ARE NOT ALLOWED ON THE RAMP SURFACE OR LANDING AREAS. EXISTING UTILITY STRUCTURE LIDS MAY REMAIN WITHIN THE FLARE OR ON THE SURFACE OF THE RAMP IF THE REQUIREMENTS OF GENERAL NOTE #19 ARE MET.
13. ALL LOCATIONS WITH TYPE 4 OR TYPE B CURB (EXCEPT ALLEY APRONS) SHALL BE CONSTRUCTED AS CURB AND GUTTER TYPE BV.12 THROUGH THE LIMITS OF THE CORNER AND THE CURB RAMPS.
14. ALTERATIONS SHALL NOT DECREASE THE ACCESSIBILITY TO EXISTING FACILITIES, SIDEWALKS LEADING TO EXISTING FACILITIES, OR DOOR OR GATE ACCESS POINTS TO FACILITIES. THE ELEVATION AT THE EXISTING PROPERTY LINE OR FACILITY ACCESS POINT SHALL BE MAINTAINED AT A MINIMUM. ANY ALTERATIONS ADJACENT TO OR AFFECTING A FACILITY ACCESS POINT SHALL RESULT IN IMPROVED ACCESS OR AT A MINIMUM A REPLICATION OF EXISTING CONDITIONS, INCLUDING SIDEWALK SLOPES AND SURFACE CONDITIONS. FACILITIES INCLUDE, BUT ARE NOT LIMITED TO PRIVATE BUSINESSES, PUBLIC BUILDINGS, RESIDENCES, BUS STOPS, PUBLIC BENCHES, PAY PHONES, AND PARKING METERS.
15. THE MINIMUM CROSSWALK WIDTH IS 6'-0". CROSSWALKS SHALL BE LOCATED AS SHOWN IN THE PLAN SHEETS DEPENDING ON THE TYPE OF CURB RAMP USED. BEYOND THE CURB FACE AT THE BASE OF CURB RAMPS, A CLEAR SPACE OF 4'-0" BY 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE STRIPES OF THE CROSSWALK (WHERE PROVIDED).
16. IF SIDEWALK AND ALLEY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED. IF SIDEWALK AND DRIVEWAY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED BUT DETECTABLE WARNING UNITS ARE STILL REQUIRED IF THE DRIVEWAY HAS TRAFFIC CONTROL DEVICES (I.E. TRAFFIC SIGNALS).
17. MAIN LINE SIDEWALK SHALL HAVE A MAXIMUM CROSS SLOPE NOT TO EXCEED 1:64 FOR THE FULL WIDTH OF WALK UNLESS OTHERWISE APPROVED BY THE COMMISSIONER. WHERE TURNING IS REQUIRED AND WHERE SIDEWALKS INTERSECT, THE SLOPE OF THE SIDEWALK SHALL NOT EXCEED 1:64 IN ANY DIRECTION.
18. MAIN LINE SIDEWALK RUNNING SLOPES SHALL NOT EXCEED 1:24 OR THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT STREET, WHICH EVER IS HIGHER.
19. THERE SHALL BE NO VERTICAL LEVEL DIFFERENCES BETWEEN SURFACES GREATER THAN 1/4" ON THE MAIN LINE SIDEWALK. THERE SHALL BE NO HORIZONTAL GAPS OR OPENINGS GREATER THAN 1/2" ON THE MAIN LINE SIDEWALK.
20. WHERE OBSTRUCTIONS EXIST ON THE MAINLINE SIDEWALK, THE CLEAR WIDTH OF USEABLE SIDEWALK SHALL NOT BE LESS THAN 4'-0". OBSTRUCTIONS INCLUDE, BUT ARE NOT LIMITED TO SIDEWALK BENCHES, FIRE HYDRANTS, SIGNAL OR LIGHT POLES, NEWSPAPER DISPENSERS, TRASH RECEPTACLES, AND UTILITY PEDESTALS.
21. CURB RAMPS AND LANDING (KEystone) TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIAL STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE TO BE USED.
22. DEPRESSED CURB, RAMP, OR SIDEWALK DESIGNS OR LAYOUTS SHALL MAINTAIN OR IMPROVE EXISTING DRAINAGE AND THE EXISTING INTERSECTION GEOMETRY SHALL NOT BE MODIFIED WITHOUT CDOT APPROVAL.
23. ALL CONSTRUCTION DOCUMENTS MUST BE STAMPED BY A LICENSED ARCHITECT/LANDSCAPE ARCHITECT/ ENGINEER TO CERTIFY THAT THEY ARE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND ALL CODES AND BUILDING ORDINANCES OF THE CITY OF CHICAGO AND THE STATE OF ILLINOIS.
24. NO DEVIATIONS FROM THESE STANDARDS ARE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.



DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3
11/02/09	REVISION 4
08/10/12	REVISION 5
01/01/14	REVISION 6

SCALE: NOT TO SCALE
DATE: 10/23/2006

**CITY OF CHICAGO
GENERAL NOTES (CONTINUED)
SHEET B-3-3**

DRAWN BY: CDOT
CHECKED BY: GK

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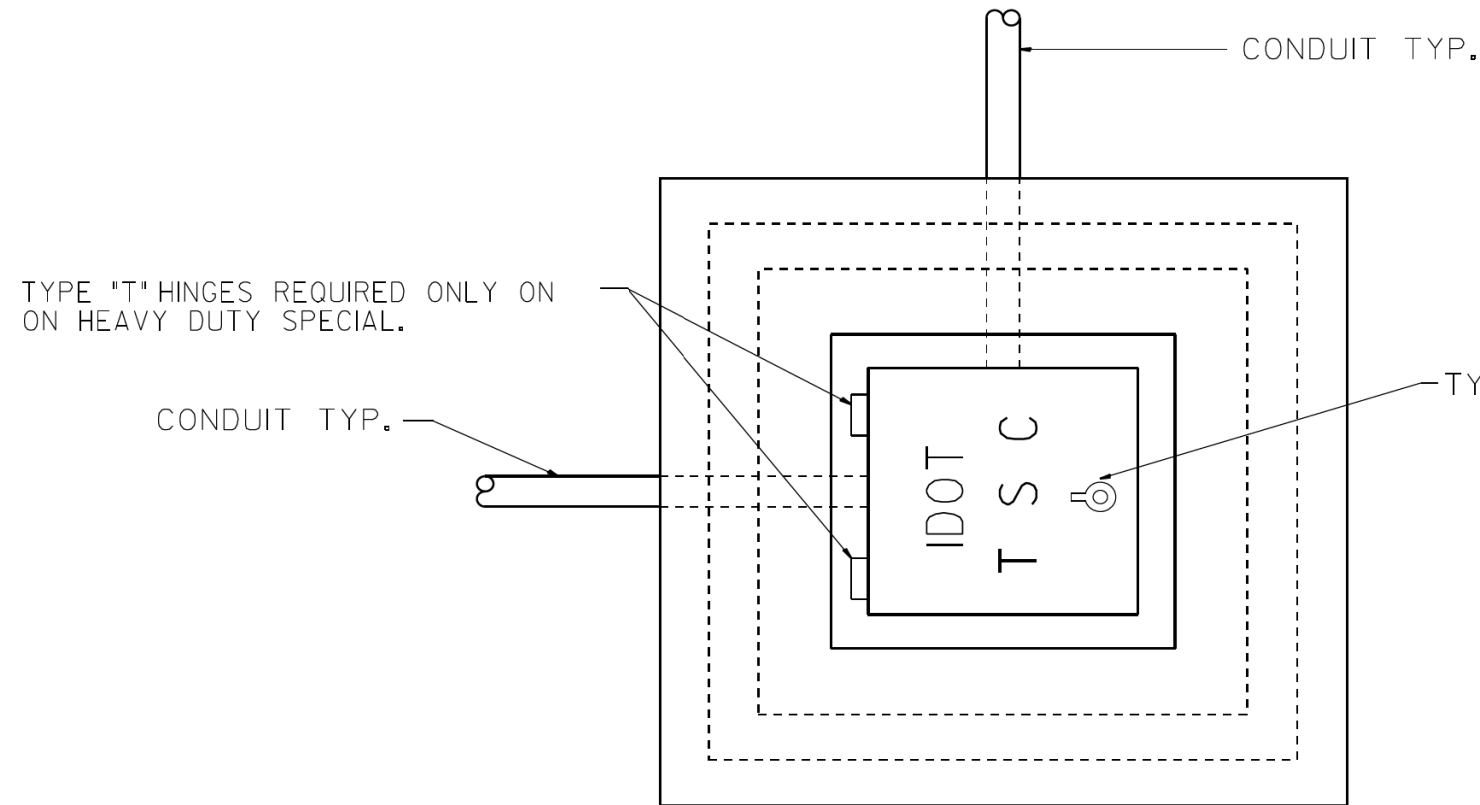
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PLOT DATE = 4/18/2017	CHECKED - RT	REVISED -
	DATE - 05/02/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CHICAGO DEPARTMENT OF TRANSPORTATION
ADA STANDARDS**

SCALE: N.T.S. SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	355
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	



PLAN

HEAVY DUTY HANDHOLE MINIMUM DIMENSIONS (UNHINGED)

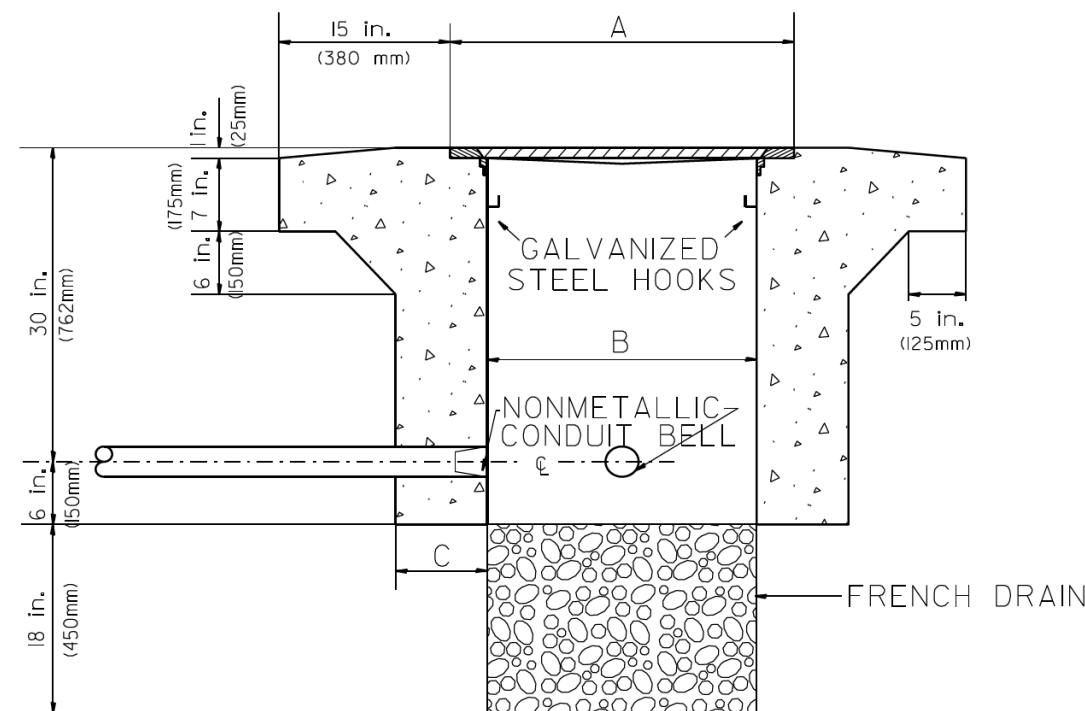
A	28" (711 mm)
B	22" (559 mm)
C	8" (200 mm)

(FRAME AND COVER 260 LBS. (118 Kg.) MIN.)

HEAVY DUTY HANDHOLE SPECIAL MINIMUM DIMENSIONS

A	31.5" (800 mm)
B	30.0" (762 mm)
C	10.0" (250 mm)

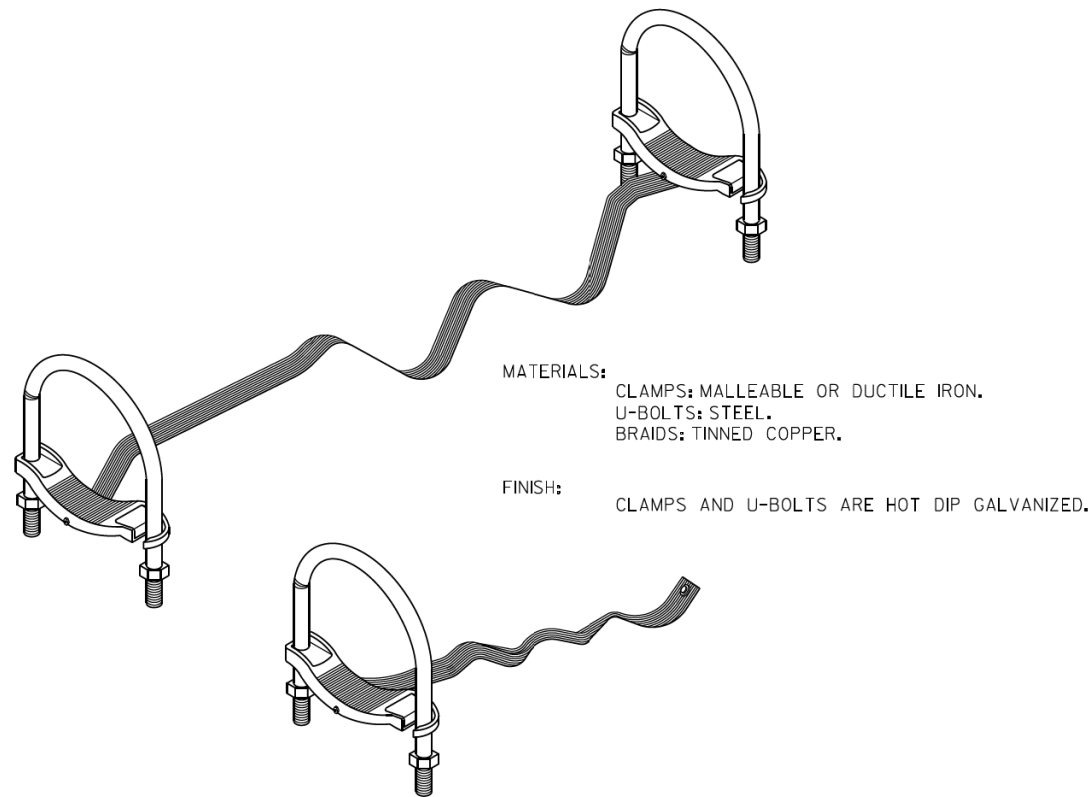
(FRAME AND COVER 405 LBS. (184 Kg. (405))



ELEVATION

PC CONCRETE - HEAVY DUTY HAND HOLE

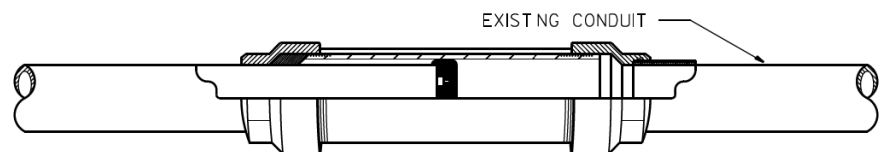
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		DATE - 09/11/96	REVISED -								FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT



MATERIALS:
 CLAMPS: MALLEABLE OR DUCTILE IRON.
 U-BOLTS: STEEL.
 BRAIDS: TINNED COPPER.

FINISH:
 CLAMPS AND U-BOLTS ARE HOT DIP GALVANIZED.

GROUNDING & BONDING JUMPERS FOR RIGID STEEL, IMC & EMT



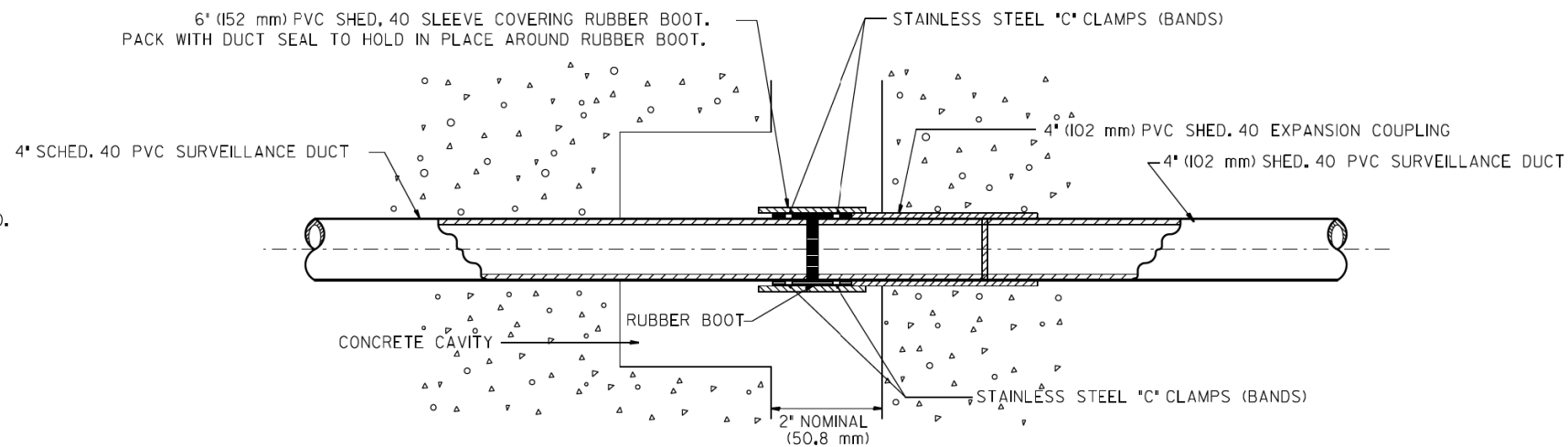
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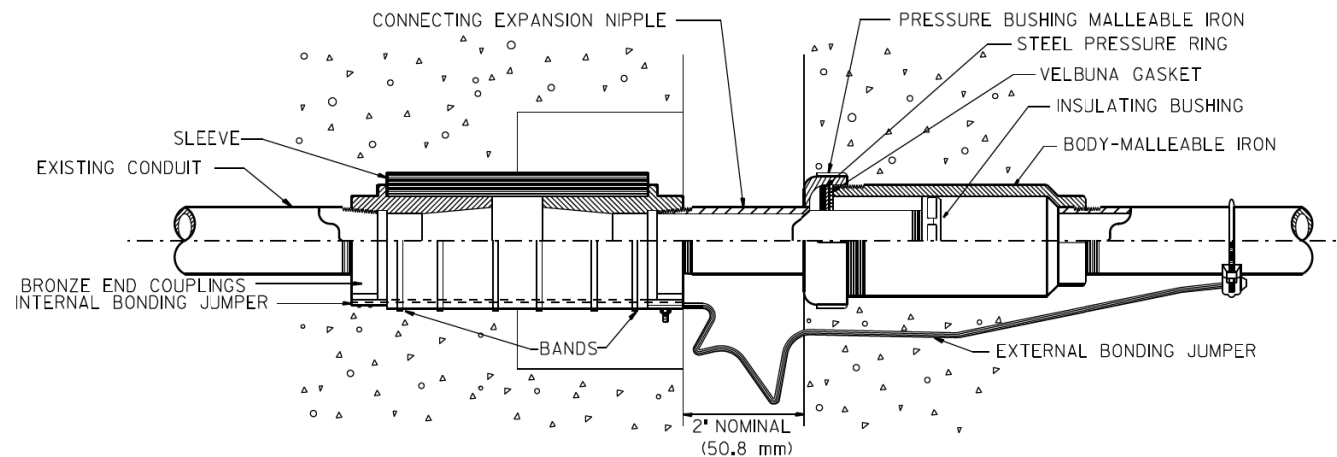
BONDING EXPANSION FITTINGS

MATERIALS:
 HEAD: MALLEABLE OR DUCTILE IRON.
 SLEEVE: STEEL.
 INSULATING BUSHING: PHENOLIC.

FINISH:
 HOT DIP GALVANIZED.



EXPANSION/DEFLECTION FITTING



COMBINATION DEFLECTION/EXPANSION FITTINGS FOR RIGID METAL CONDUIT & IMC

FITTING CAN BE USED EXPOSED OR EMBEDDED IN CONCRETE.

MATERIALS:
 SLEEVE: NEOPRENE.
 END COUPLINGS: BRONZE.
 BONDING JUMPER: TINNED COPPER BRAIDS.
 BANDS: STAINLESS STEEL.

FINISH:
 ALL MALLEABLE, DUCTILE IRON OR STEEL PARTS
 ARE HOT DIP GALVANIZED.

FILE NAME =	USER NAME = #USER#	DESIGNED - R.L.	REVISED - 03/99
#FILEL#		DRAWN - G.M.	REVISED -
	PLOT SCALE = #SCALE#	CHECKED - R.L.	REVISED -
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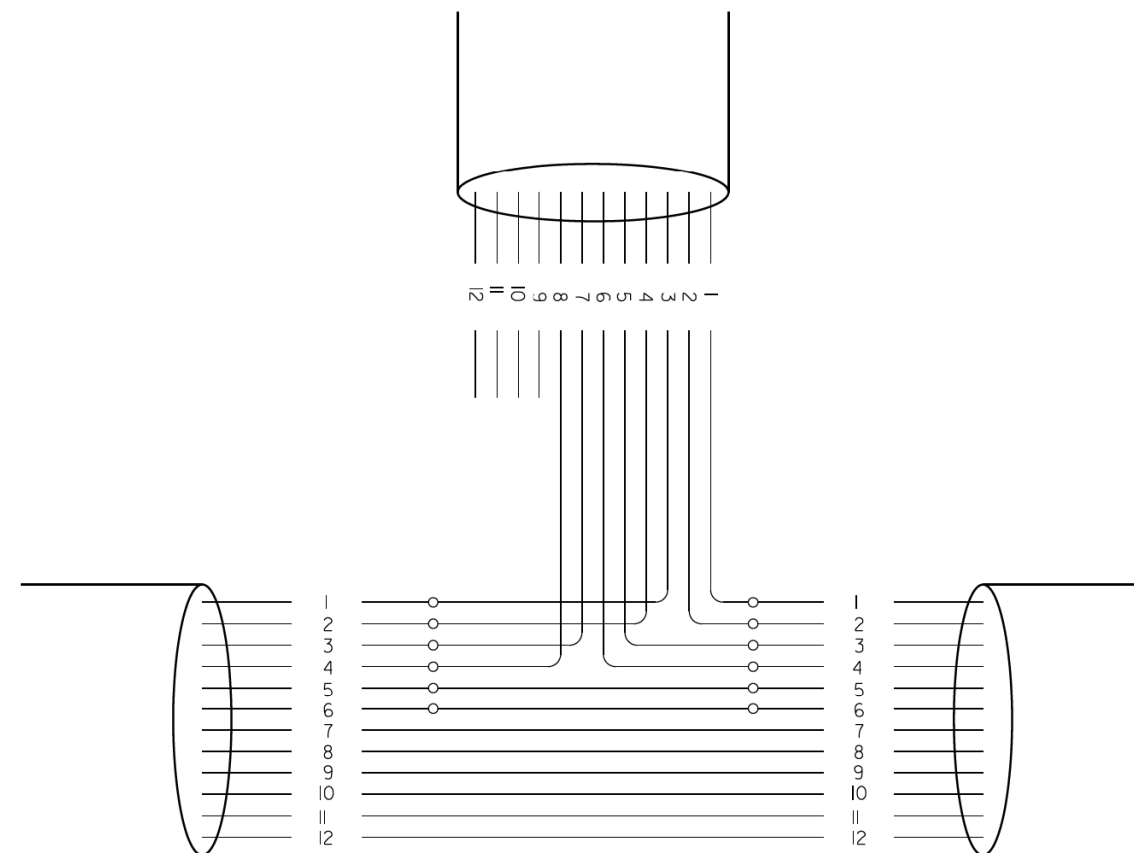
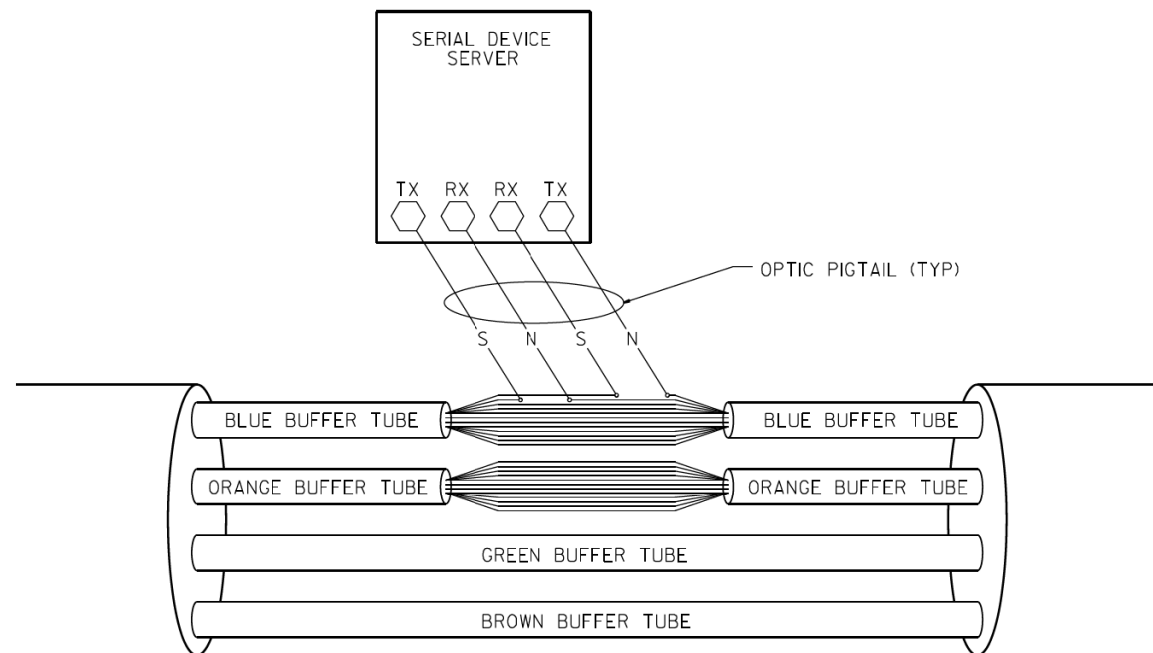
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 TRAFFIC SYSTEMS CENTER

EXPANSION FITTING DETAIL SHEET

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			378	358
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SYSTEMS CENTER (TY-ITSC-400#18)



PHYSICAL SPLICE DETAILS (TYP)
(NOT TO SCALE)

FIBERS		FUNCTION	APPLICATION DESCRIPTION
FIBER NO.	COLOR CODE		
1	BLUE	IN TX	DATA CIRCUIT
2	ORANGE	IN RX	
3	GREEN	OUT RX	
4	BROWN	OUT TX	
5	SLATE	IN TX	DMS
6	WHITE	IN RX	
7	RED	OUT RX	
8	BLACK	OUT TX	
9	YELLOW	IN TX	CCTV
10	VIOLET	IN TX	
11	ROSE	OUT RX	
12	AQUA	OUT TX	

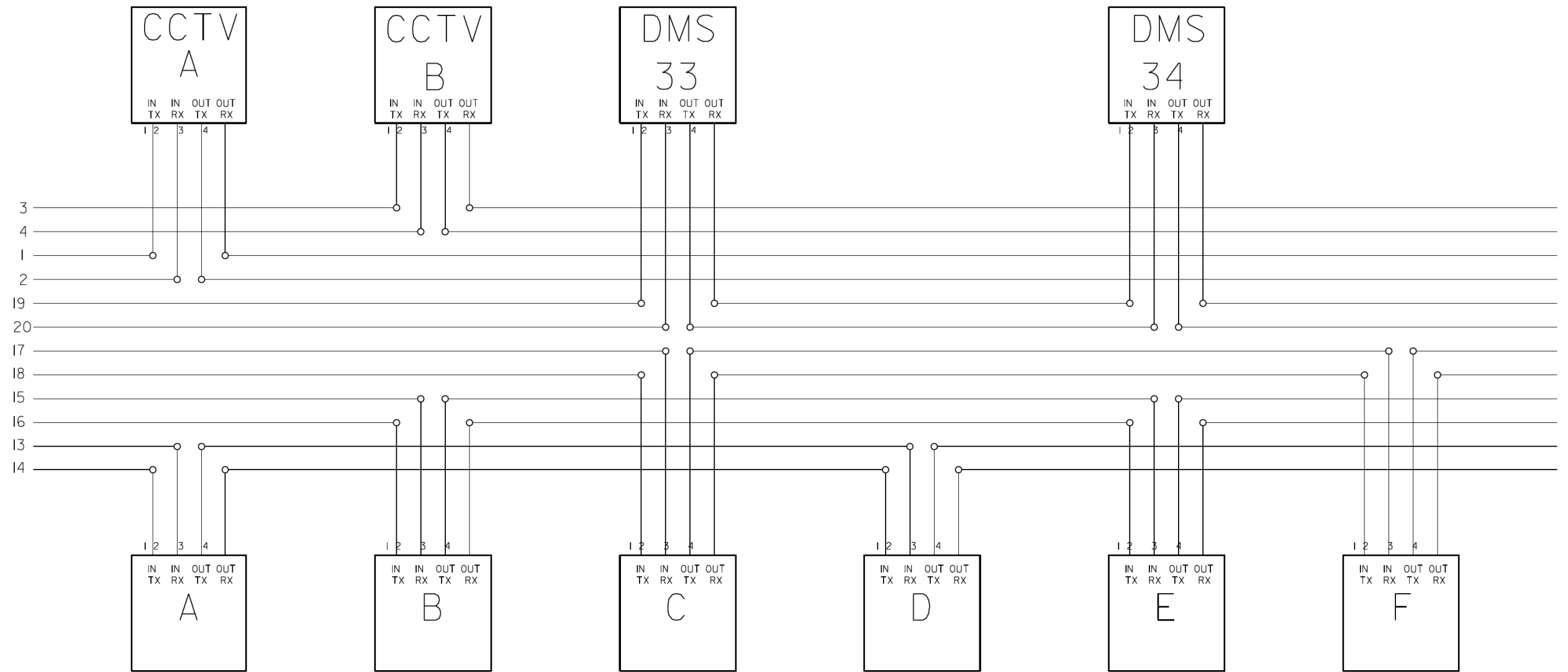
NOTE:

1.- THIS DIAGRAM IS PROVIDED FOR ILLUSTRATION PURPOSES ONLY AND DEPICTS A TYPICAL FIBER OPTIC SPLICE.

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#FILEL#		DRAWN - G.M.	REVISED -		SCALE: NONE	SHEET	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO.
#MODELNAME#		CHECKED - J.G.	REVISED -									
		DATE - 12/29/09	REVISED -									

FIBERS

CABINETS



FIBERS 13 AND 14 CABINETS 77, 83, 94, 96, 95, 100A, 105, 104, 108, 115, 119, 125, 131, 137, 143, 155, 149, 112, 165, 171.

FIBERS 15 AND 16 CABINETS 79, 85, 89, 98, 97, 101, 107, 109, 111, 110, 121, 127, 133, 139, 145, 151, 157, 167.

FIBERS 17 AND 18 CABINETS 81, 87, 91, 93, 100, 103, 102, 106, 113, 117, 123, 129, 135, 141, 147, 153, 169, 159, 161, 163.

FIBERS 19 AND 20 DMS 33, DMS 34.

FIBERS 3 AND 4 CCTV 50, 51, 52, 49, 47B, 47, 45, 43A, 42, 40, 38, 37, 35A, 34, 32, 30A, 29.

FIBERS 5 AND 6 CCTV 51, 49A, 48, 47A, 46, 44, 43, 40A, 39, 37A, 36, 36A, 35, 32A, 31, 30.

NOTE:

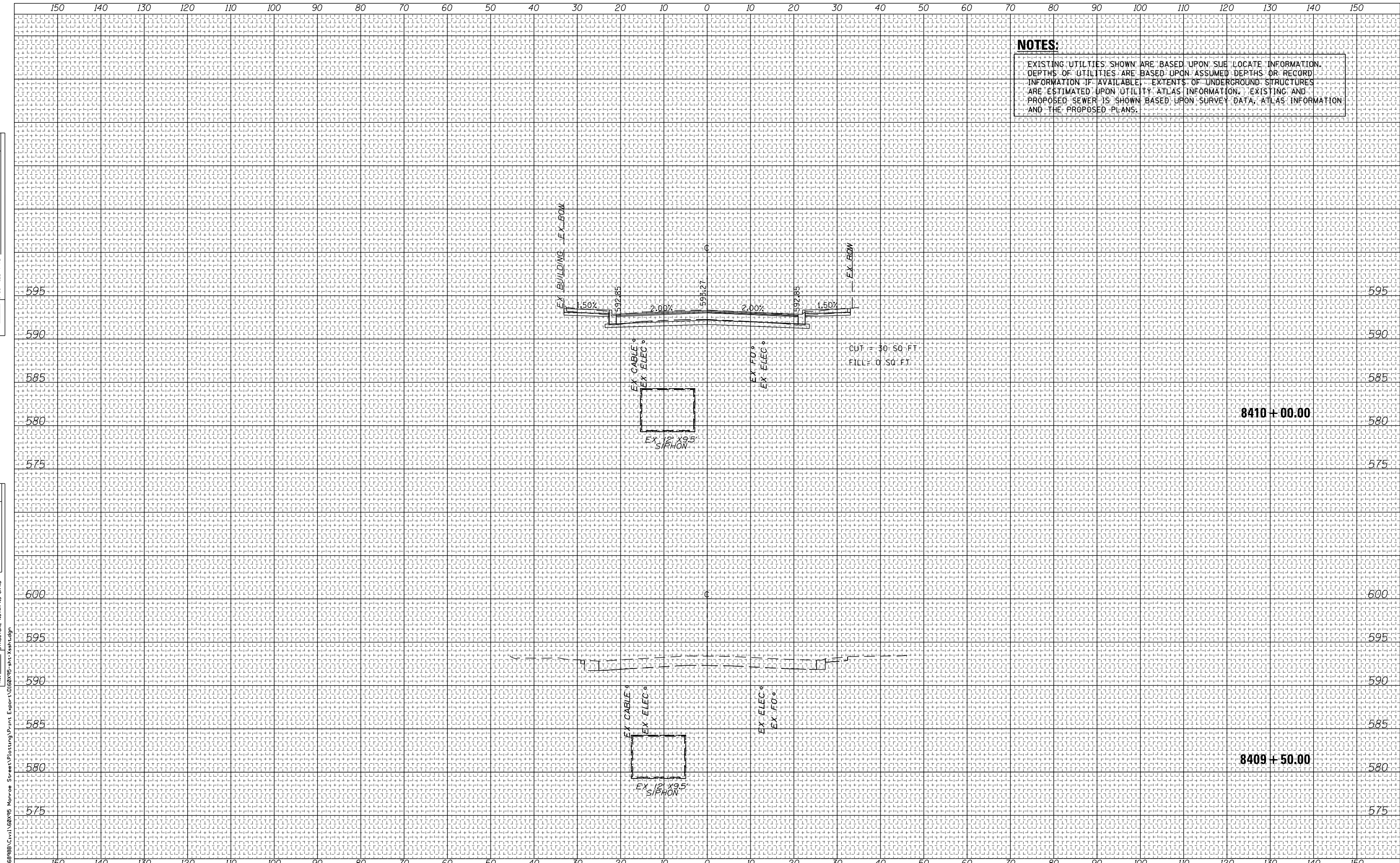
1.- CABINETS ARE SPSSRVDS, SO THEY DO NOT GET ATTACHED TO THE FIBER.

2.- CCTV 49 AND 49A ARE IN THE ARSENAL RD. CONTRACT.

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		DATE - 12/29/09	REVISED - 04/19/2012 R.T.									

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NOTE BOOK	PLOTTED	BY
NO.	REVISIONS	
	AT	
	OR	
	FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	REVISIONS	
	AT	
	OR	
	FILE NAME	



**PARSONS
BRINCKERHOFF**

USER NAME =	Kucherski
PLOT SCALE =	
PLOT DATE	

DESIGNED -	MMA	REVISED -	
DRAWN -	MMA	REVISED -	
CHECKED -	RT	REVISED -	
DATE -	05/02/2017	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

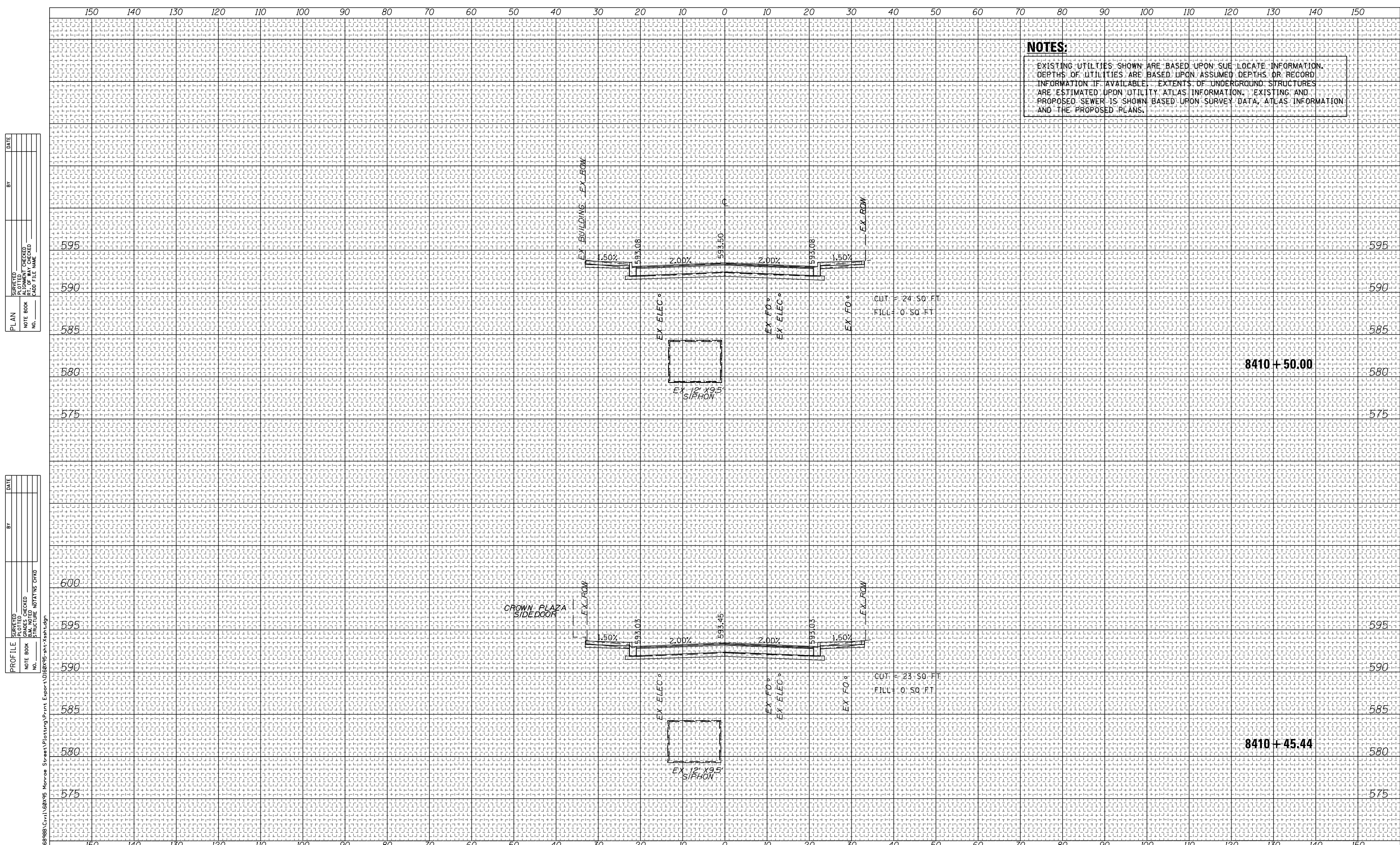
**CROSS SECTIONS
MONROE STREET**

SCALE: 10H=5V SHEET NO. 1 OF 10 SHEETS STA. 8409+50.00 TO STA. 8410+00.00

F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	361
			CONTRACT NO. 60X95	

ILLINOIS FED. AID PROJECT

FILE NAME = I:\168983C\1168983 Monroes Streets\Plotting\Print Export\10168983-1168983.dgn



NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.

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NOTE BOOK	PLOTTED	
NO.	REVISIONS	
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	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	REVISIONS	
	AT 0% MAX CHECKED	
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**PARSONS
BRINCKERHOFF**

USER NAME = Kucherski	DESIGNED - MMA	REVISED -
PLOT SCALE =	DRAWN - MMA	REVISED -
PLOT DATE	CHECKED - RT	REVISED -
	DATE - 05/02/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

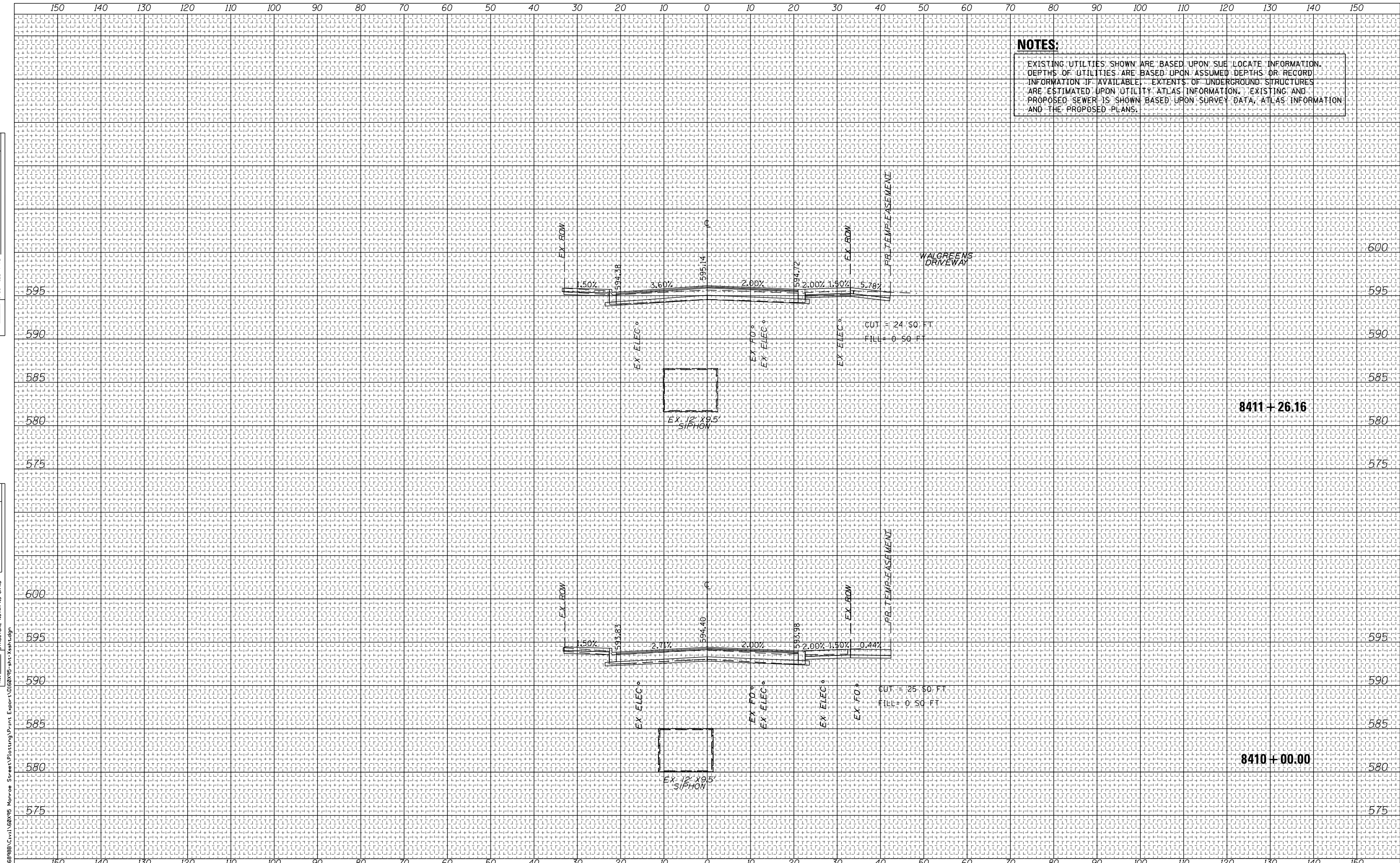
**CROSS SECTIONS
MONROE STREET**

SCALE: 10H= 5V SHEET NO. 2 OF 10 SHEETS STA. 8410+45.44 TO STA. 8410+50.00

F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	362
			CONTRACT NO. 60X95	
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	REVISIONS	
	AT 0% MAY CHECKED	
	CADD FILE NAME	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	GRADES CHECKED	
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	FILE NAME	



NOTES:
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PARSONS BRINCKERHOFF

USER NAME = Kucherski	DESIGNED - MMA	REVISED -
PLOT SCALE =	DRAWN - MMA	REVISED -
PLOT DATE	CHECKED - RT	REVISED -
	DATE - 05/02/2017	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

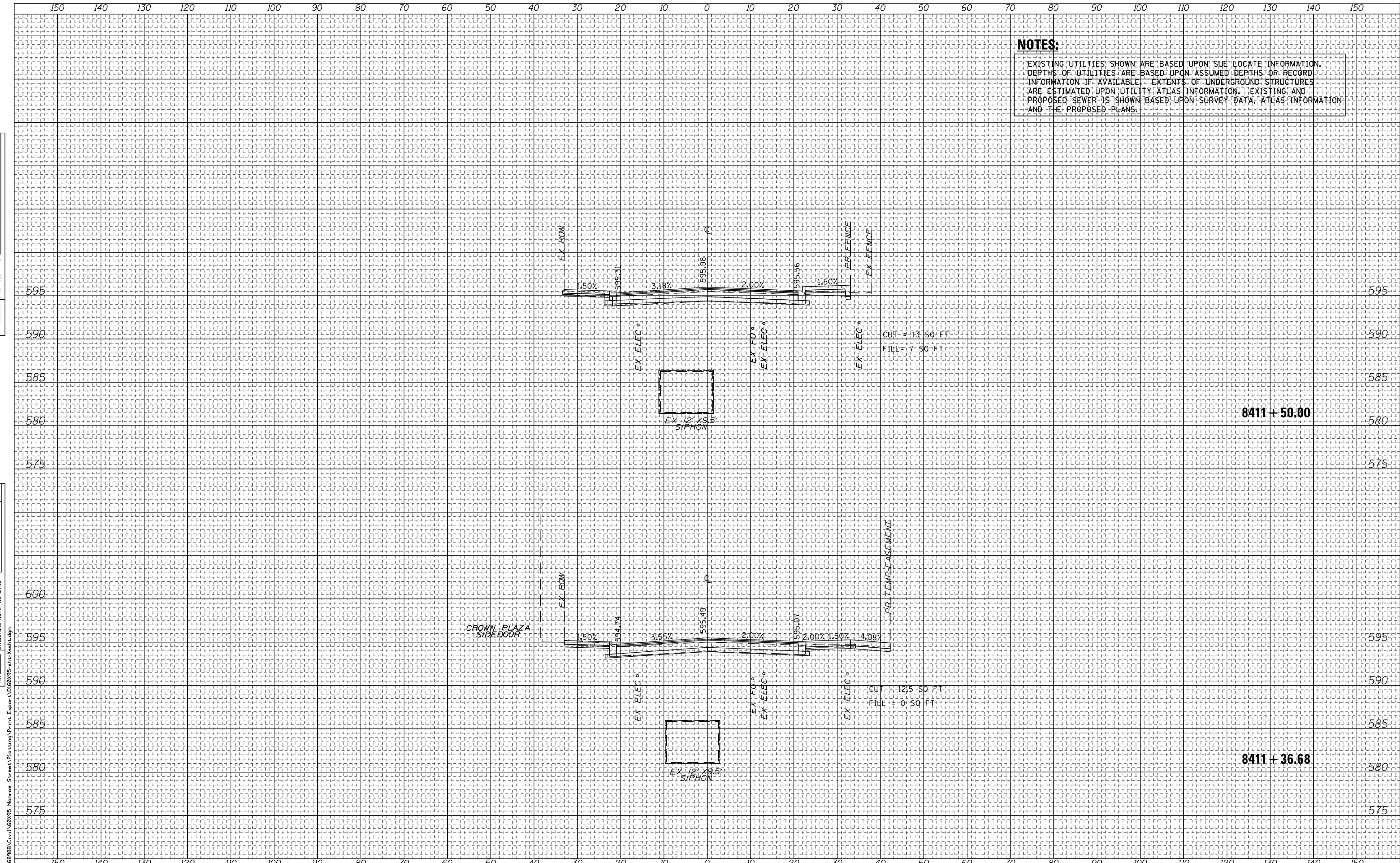
**CROSS SECTIONS
 MONROE STREET**

SCALE: 10'H 5:V SHEET NO. 3 OF 10 SHEETS STA. 8410+00.00 TO STA. 8411+26.16

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	363
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	AT		
	OR		
	MA		
	Y		
	NO.		
	NO.		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES		
	CHECKED		
	STRUCTURE		
	NOTATIONS		
	CHFD		
	NO.		
	NO.		
	NO.		



NOTES:
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PARSONS BRINCKERHOFF

USER NAME =	Kucherski	DESIGNED -	MMA	REVISED -	
PLOT SCALE =		DRAWN -	MMA	REVISED -	
PLOT DATE		CHECKED -	RT	REVISED -	
		DATE -	05/02/2017	REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 MONROE STREET**

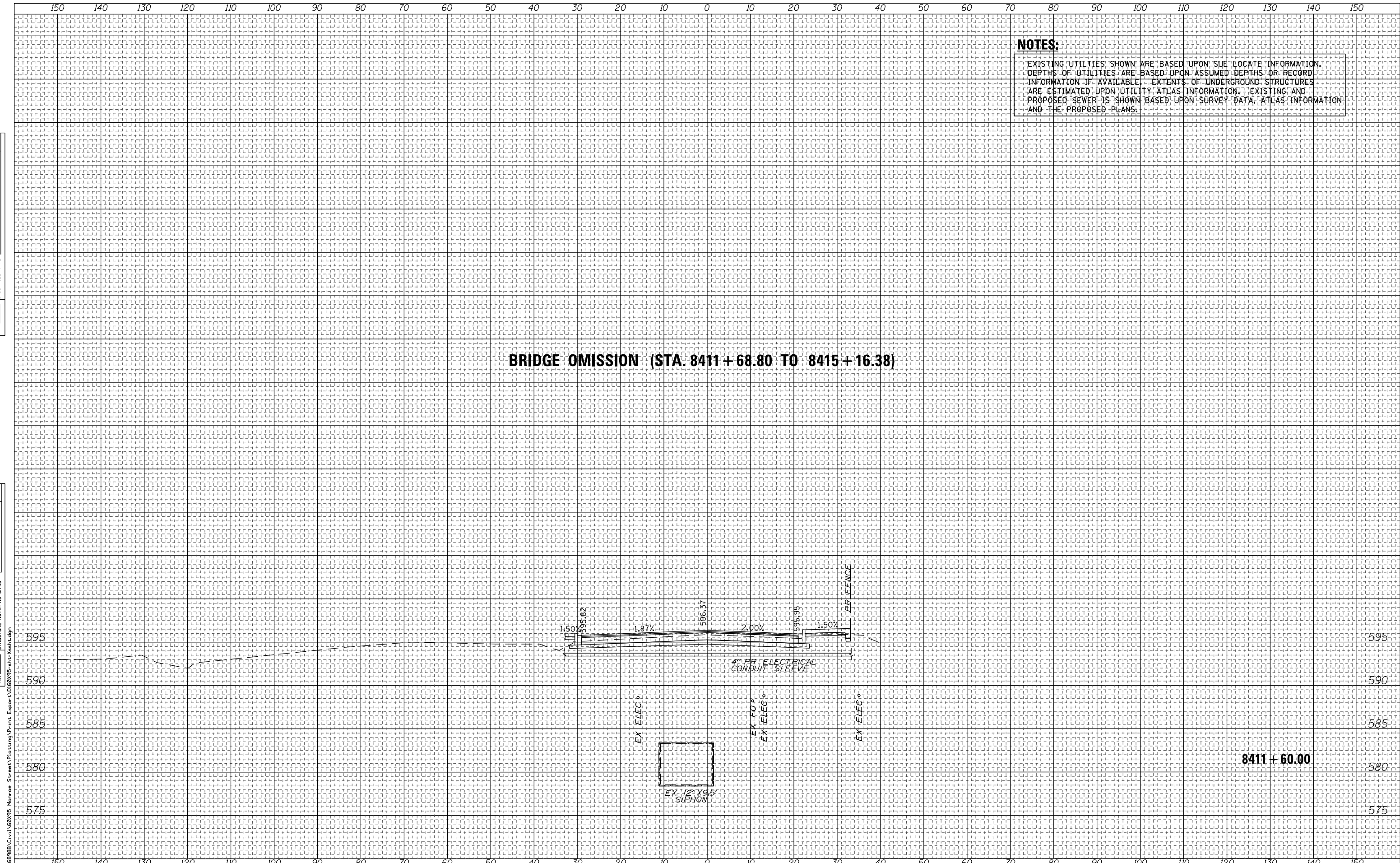
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F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	364
			CONTRACT NO. 60X95	
ILLINOIS FED. AID PROJECT				

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NOTE BOOK	PLOTTED	
NO.	BY	
	NO. OF	
	FILE	

PROFILE	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	BY	
	NO. OF	
	FILE	



NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION.
 DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD
 INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES
 ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND
 PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION
 AND THE PROPOSED PLANS.

BRIDGE OMISSION (STA. 8411+68.80 TO 8415+16.38)

**PARSONS
BRINCKERHOFF**

USER NAME = Kucherski	DESIGNED - MMA	REVISED -
PLOT SCALE =	DRAWN - MMA	REVISED -
PLOT DATE	CHECKED - RT	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
MONROE STREET**

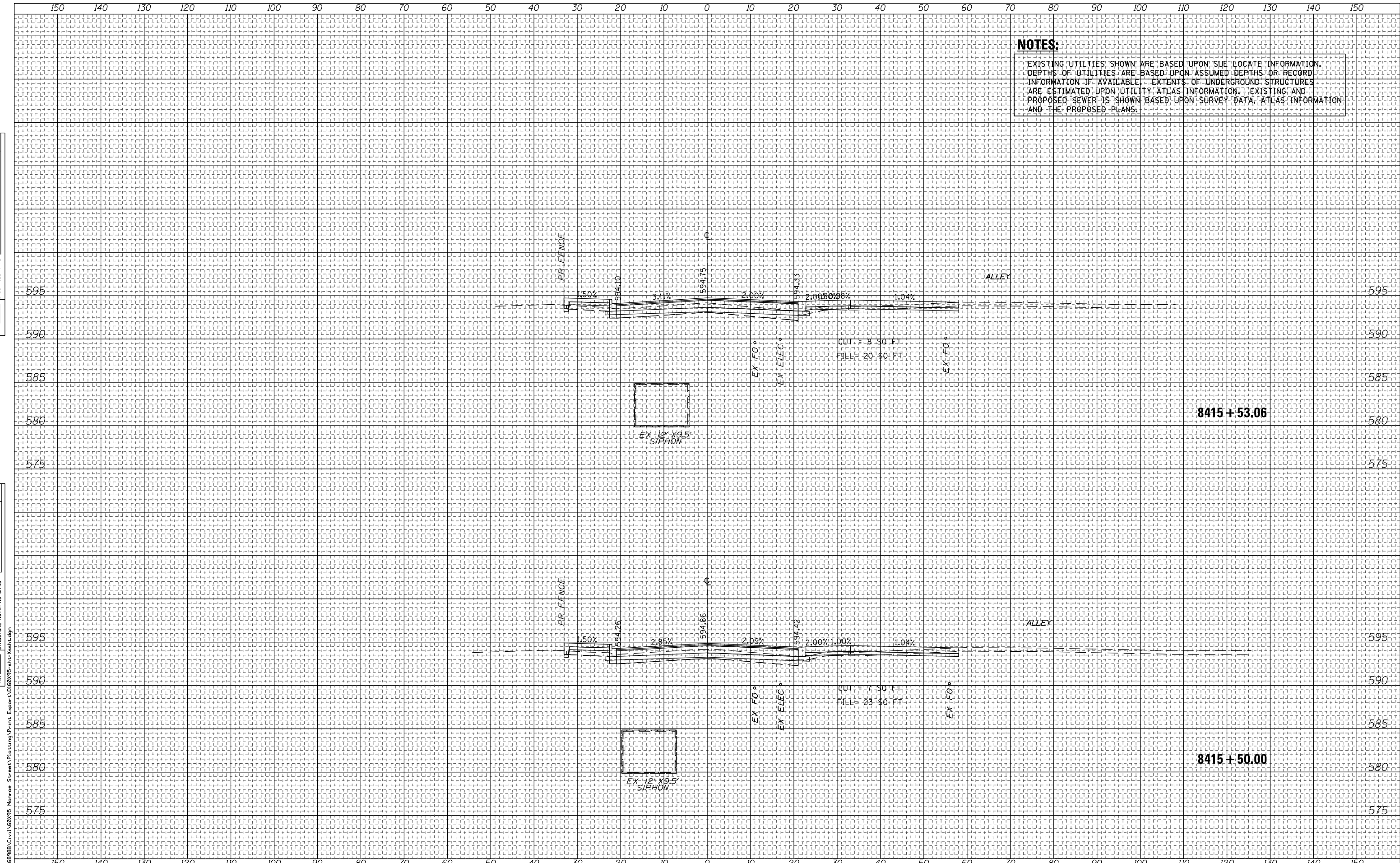
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	365
			CONTRACT NO. 60X95	
ILLINOIS FED. AID PROJECT				

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	OR		
	FILE		
	NAME		

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NOTE BOOK	GRADES CHECKED		
NO.	STRUCTURE		
	NOTATIONS		
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NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.

PARSONS BRINCKERHOFF

USER NAME =	Kucherski	DESIGNED -	MMA	REVISED -	
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		DATE	05/02/2017	REVISED -	

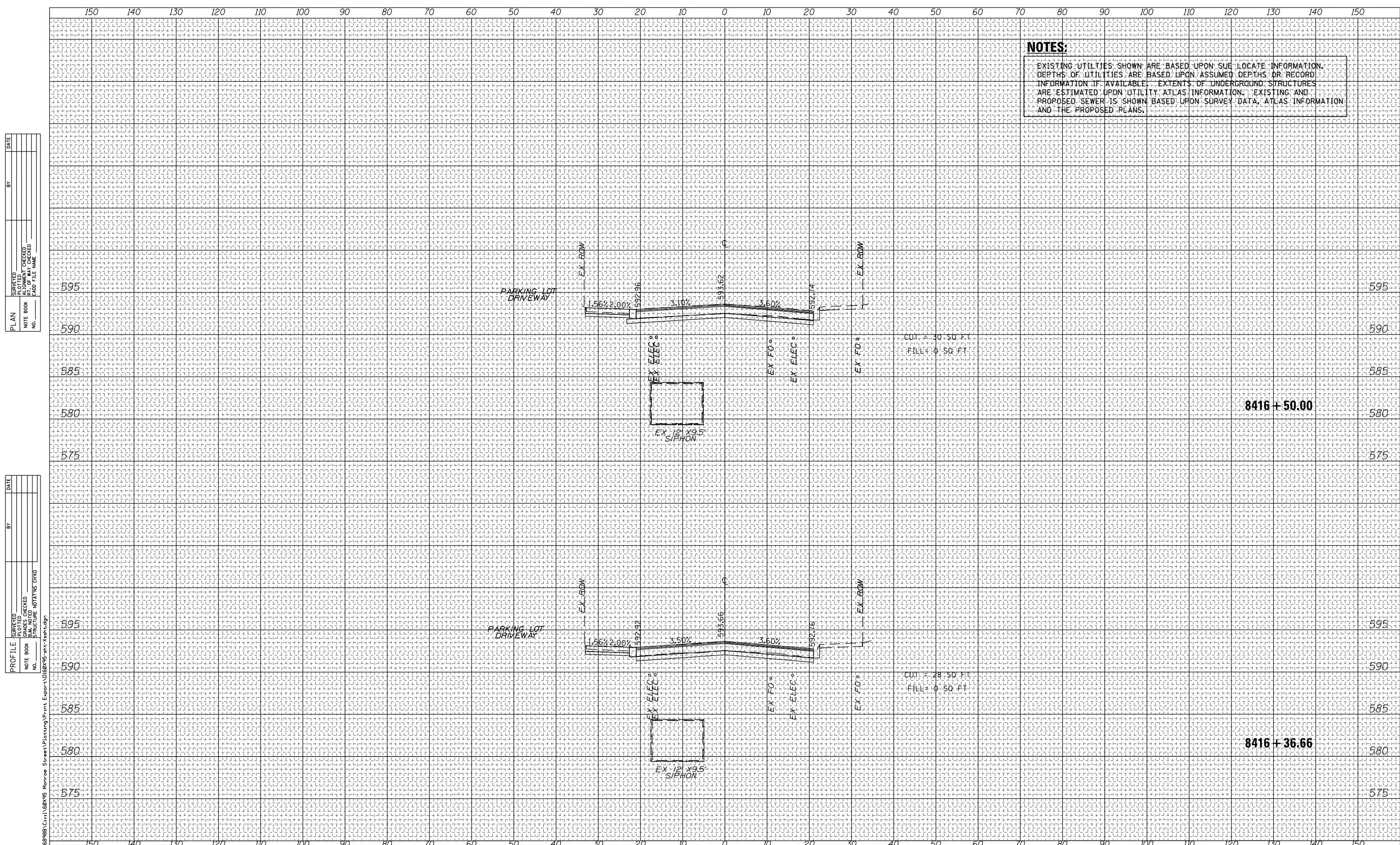
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 MONROE STREET

SCALE: 10H= 5V SHEET NO. 6 OF 10 SHEETS STA. 8409+53.06 TO STA. 8416+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	366
			CONTRACT NO. 60X95	
ILLINOIS FED. AID PROJECT				

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NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.

DATE	
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PLAN	
NO. _____	
NOTE BOOK	
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PLOTTED	
GRADES CHECKED	
STRUCTURE NOTATIONS CHECKED	
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DATE	
BY	
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PLOTTED	
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STRUCTURE NOTATIONS CHECKED	
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**PARSONS
BRINCKERHOFF**

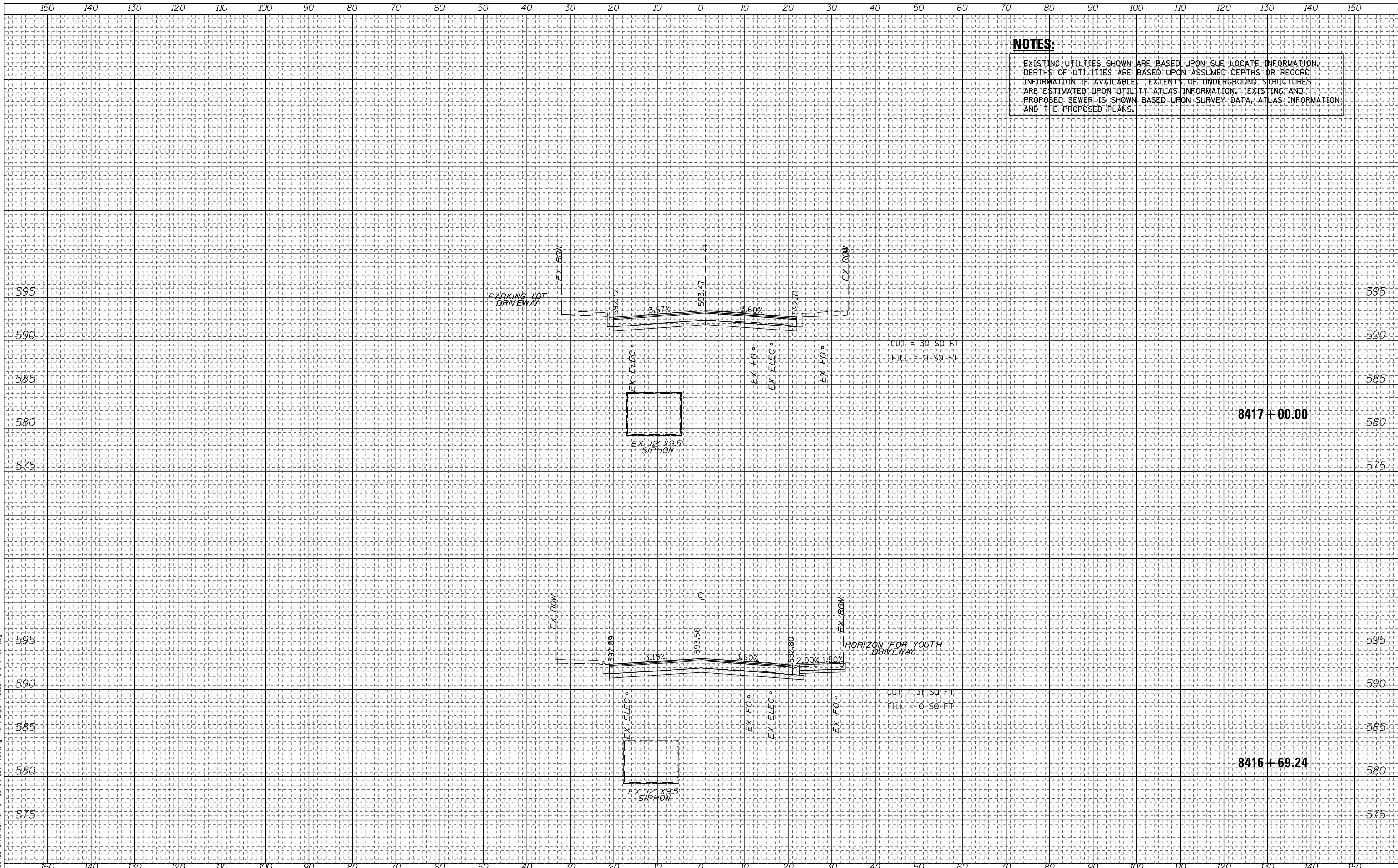
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CHECKED - RT	REVISIED -
DATE - 05/02/2017	REVISIED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
MONROE STREET**

SCALE: 10H= 5V SHEET NO. 8 OF 10 SHEETS STA. 8416+50.00 TO STA. 8416+69.24

F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	368
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	



NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.

DATE	
BY	
PLAN SURVEYED	
PILOTED	
NOTED	
REVISIONS	
AT OR MAY CHECKED	
NO. CAD FILE NAME	

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GRADES CHECKED	
NOTE BOOK	
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**PARSONS
BRINCKERHOFF**

USER NAME = Kucherski	DESIGNED - MMA	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

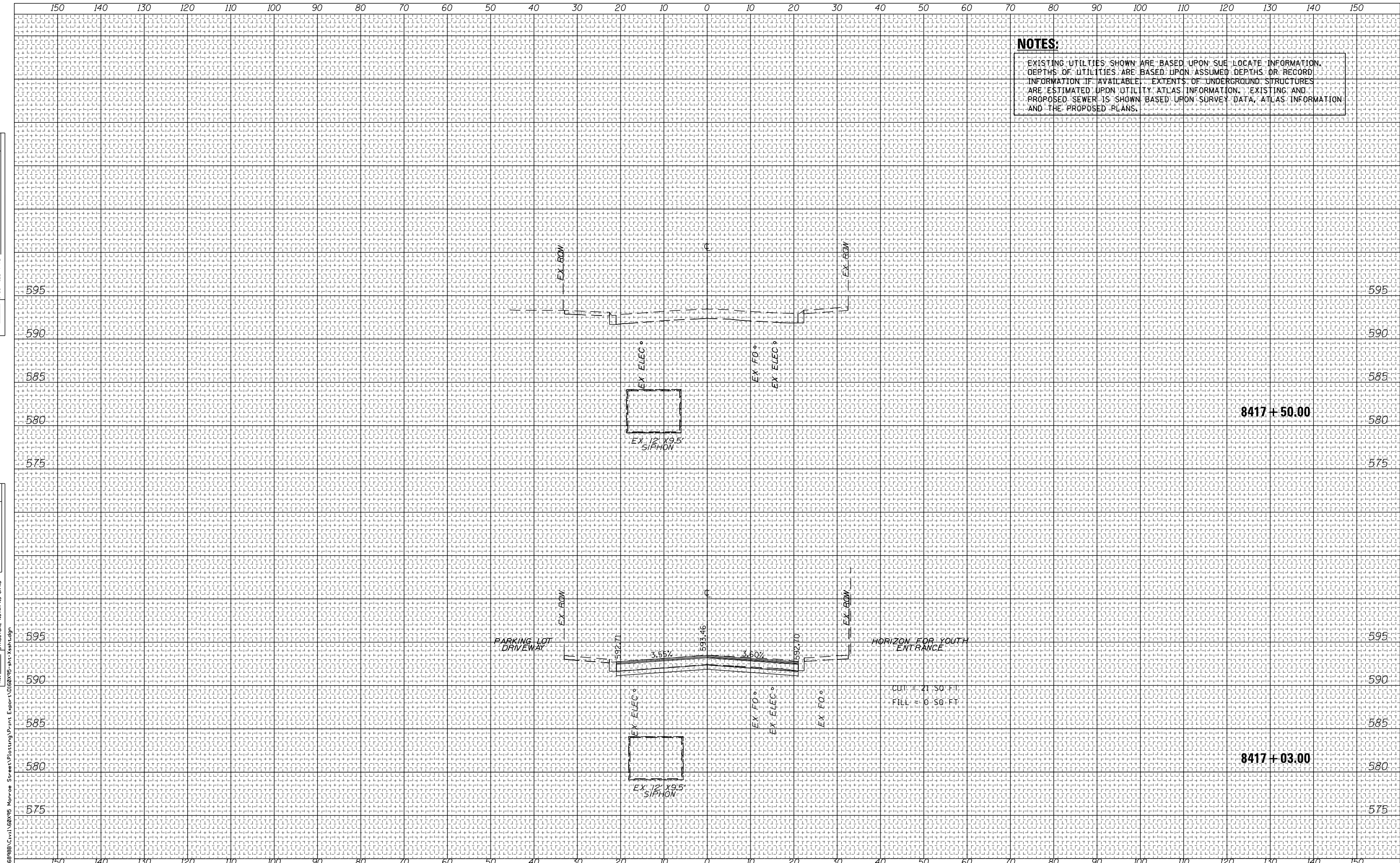
**CROSS SECTIONS
MONROE STREET**

SCALE: 10H 5V SHEET NO. 9 OF 10 SHEETS STA. 8417+00.00 TO STA. 8417+03.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	369
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

PLAN	SURVEYED	BY	DATE
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	FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	FILE NAME		



NOTES:
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PARSONS BRINCKERHOFF

USER NAME = Kucharski	DESIGNED - MMA	REVISED -
PLOT SCALE =	DRAWN - MMA	REVISED -
PLOT DATE	CHECKED - RT	REVISED -
	DATE - 05/02/2017	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 MONROE STREET**

SCALE: 10H+ 5V SHEET NO. 10 OF 10 SHEETS STA. 8417+50.00 TO STA. 8417+50.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 90/94	2014-016R&B	COOK	378	370
CONTRACT NO. 60X95			ILLINOIS FED. AID PROJECT	

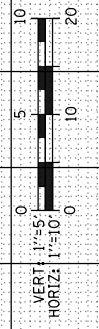
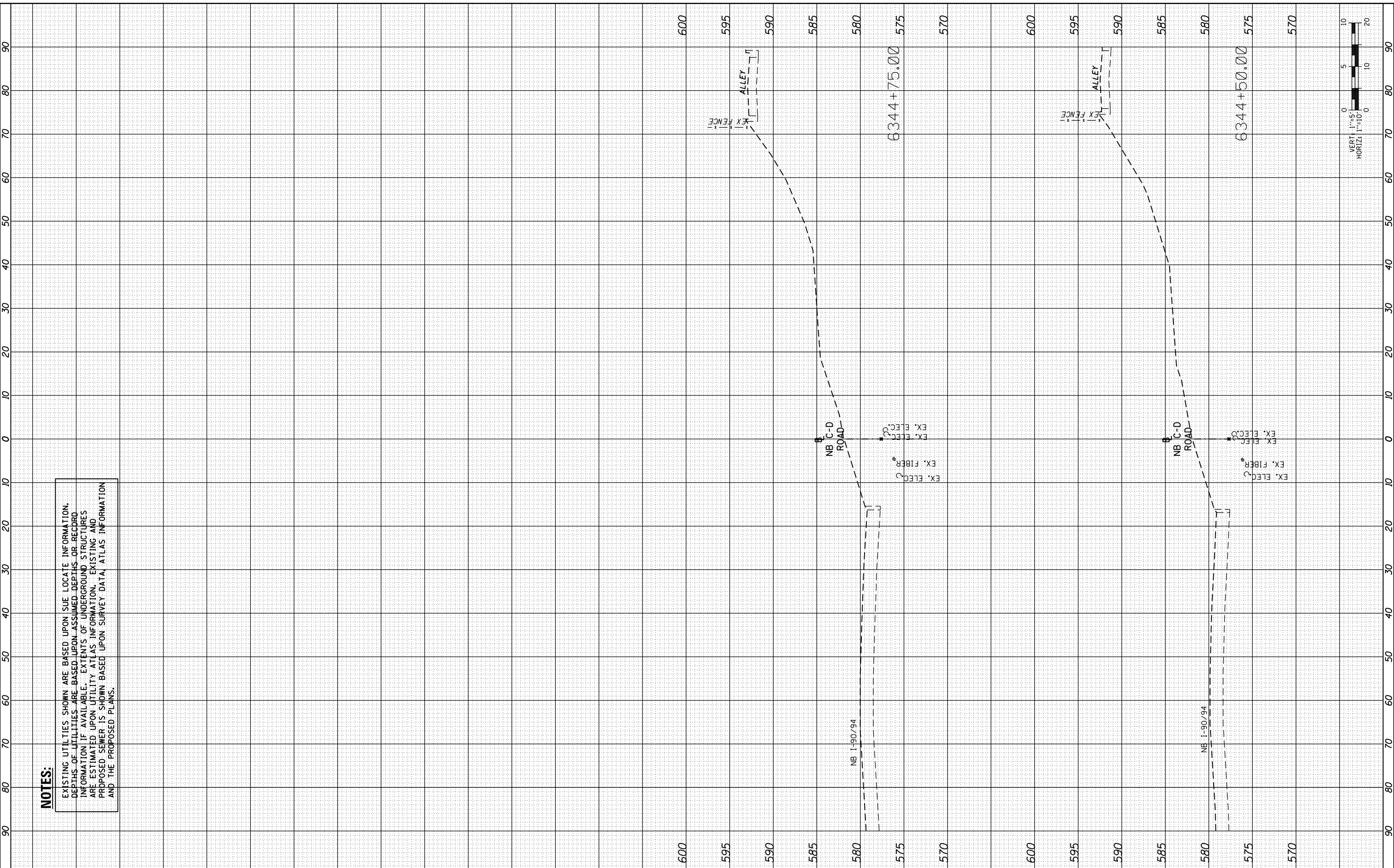
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NOTE BOOK	PLOTTED		
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	AREAS		
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NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.



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 PLOT SCALE = 20.0000' / in.
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 DRAWN - SDH
 CHECKED -
 DATE - 7/14/2017

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAINLINE CROSS SECTIONS
 RETAINING WALL 29 (SN 016-2017)

SCALE: 1:5V, 1:10H SHEET 02 OF 8 SHEETS STA. 6344+50.00 TO STA. 6345+00.00

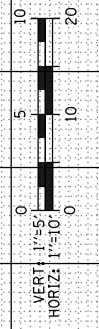
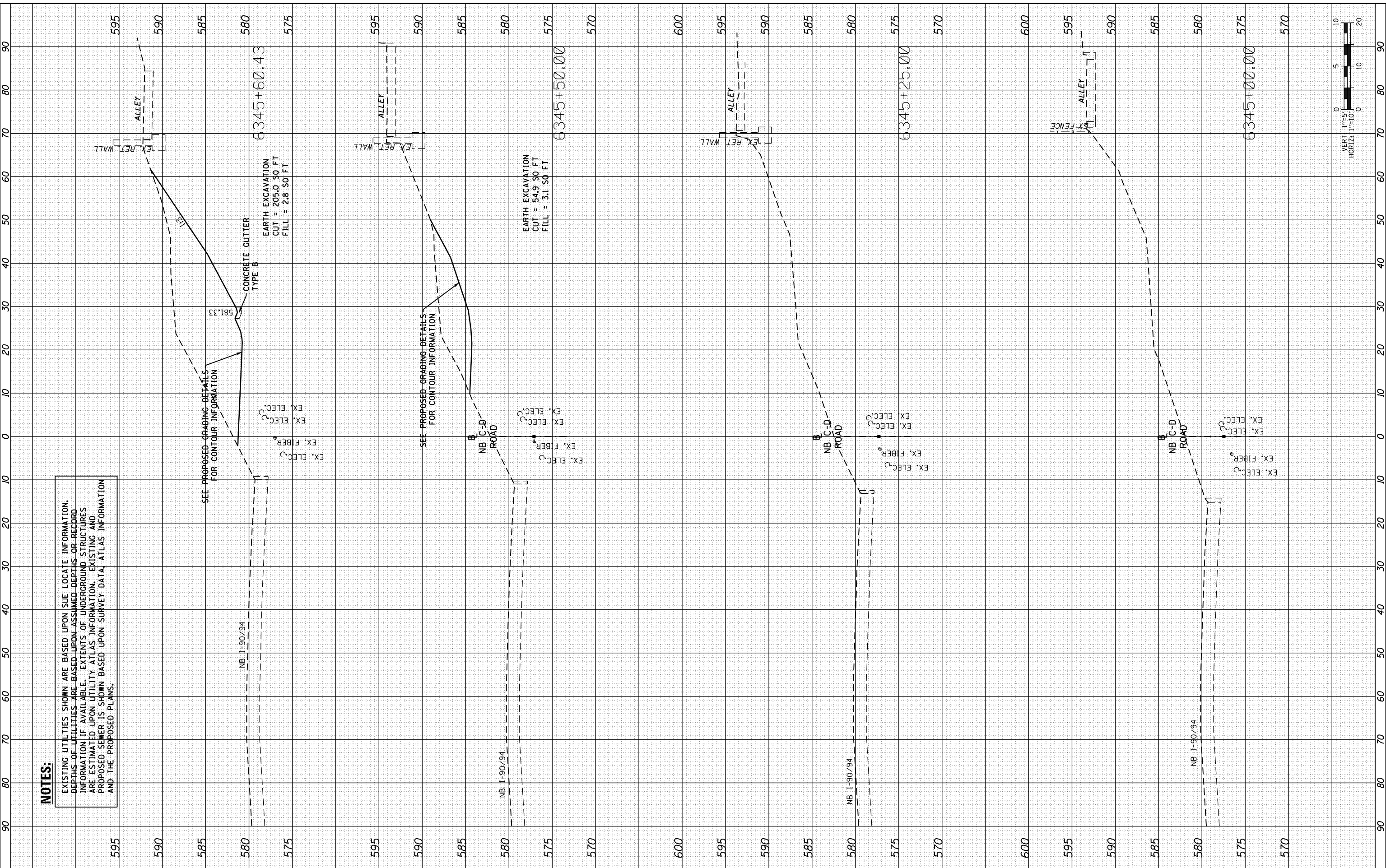
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1420	2014-016R&B	COOK	378	372
				CONTRACT NO. 60X95
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

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NOTES:
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PLOT DATE = 7/13/2017

DESIGNED - SDH	REVISED -
DRAWN - SDH	REVISED -
CHECKED -	REVISED -
DATE - 7/14/2017	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINLINE CROSS SECTIONS
 RETAINING WALL 29 (SN 016-2017)**

SCALE: 1:5V, 1:10H SHEET 03 OF 8 SHEETS STA. 6345+00.00 TO STA. 6345+60.43

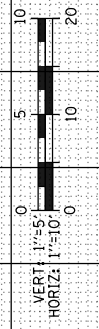
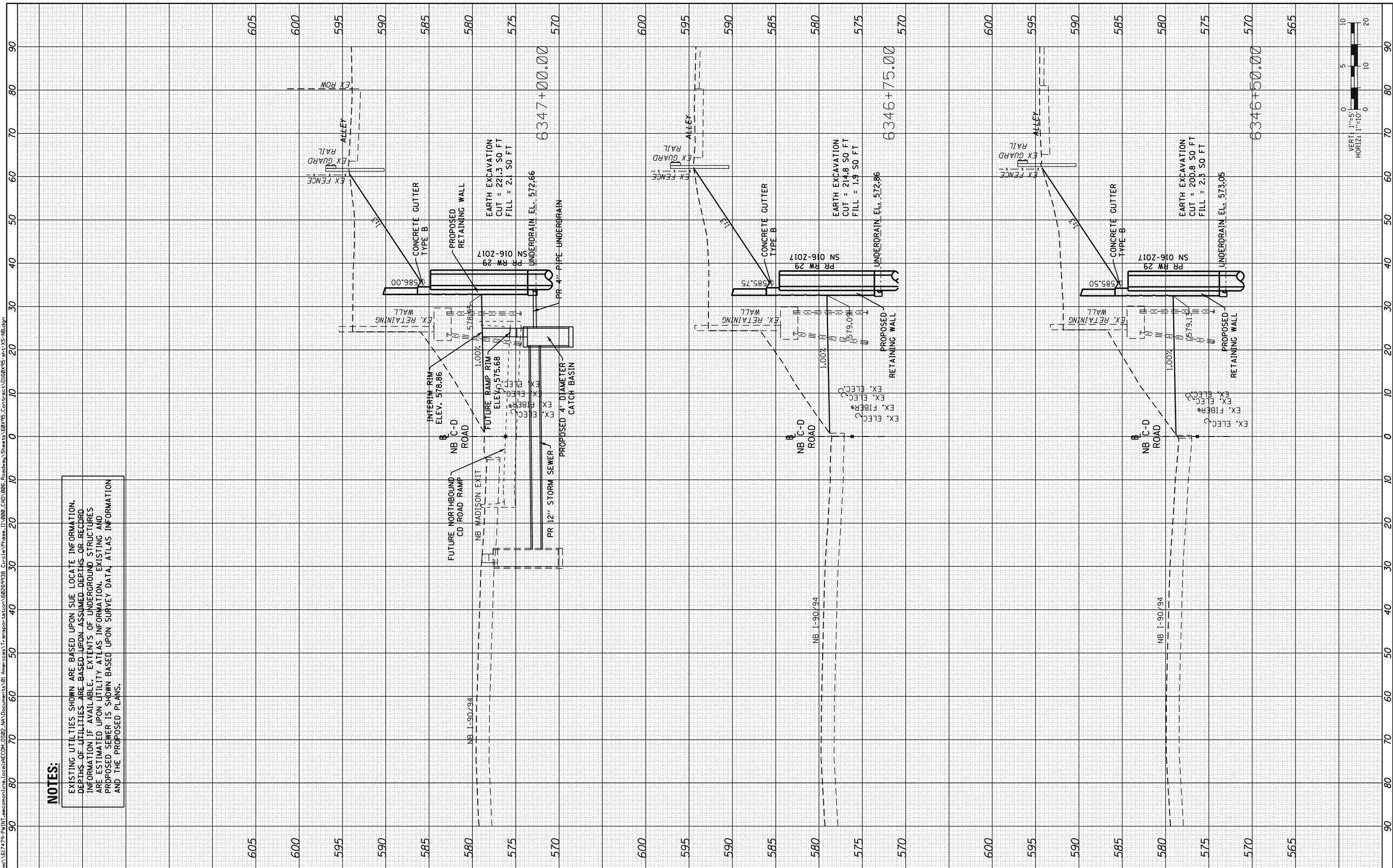
F.A.I. RTE. 1420	SECTION 2014-016R&B	COUNTY COOK	TOTAL SHEETS 378	SHEET NO. 373
CONTRACT NO. 60X95				ILLINOIS FED. AID PROJECT

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
NO.	AREAS CHECKED		

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NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.



D160X95-sh1-XS-NB.dgn	DESIGNED - SDH	REVISED -
USER NAME = hollisd	DRAWN - SDH	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 7/13/2017	DATE - 7/14/2017	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MAINLINE CROSS SECTIONS
 RETAINING WALL 29 (SN 016-2017)**

SCALE: 1:5V, 1:10H SHEET 05 OF 8 SHEETS STA. 6346+50.00 TO STA. 6347+00.00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1420	2014-016R&B	COOK	378	375
				CONTRACT NO. 60X95

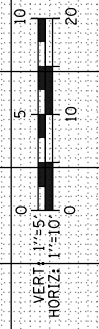
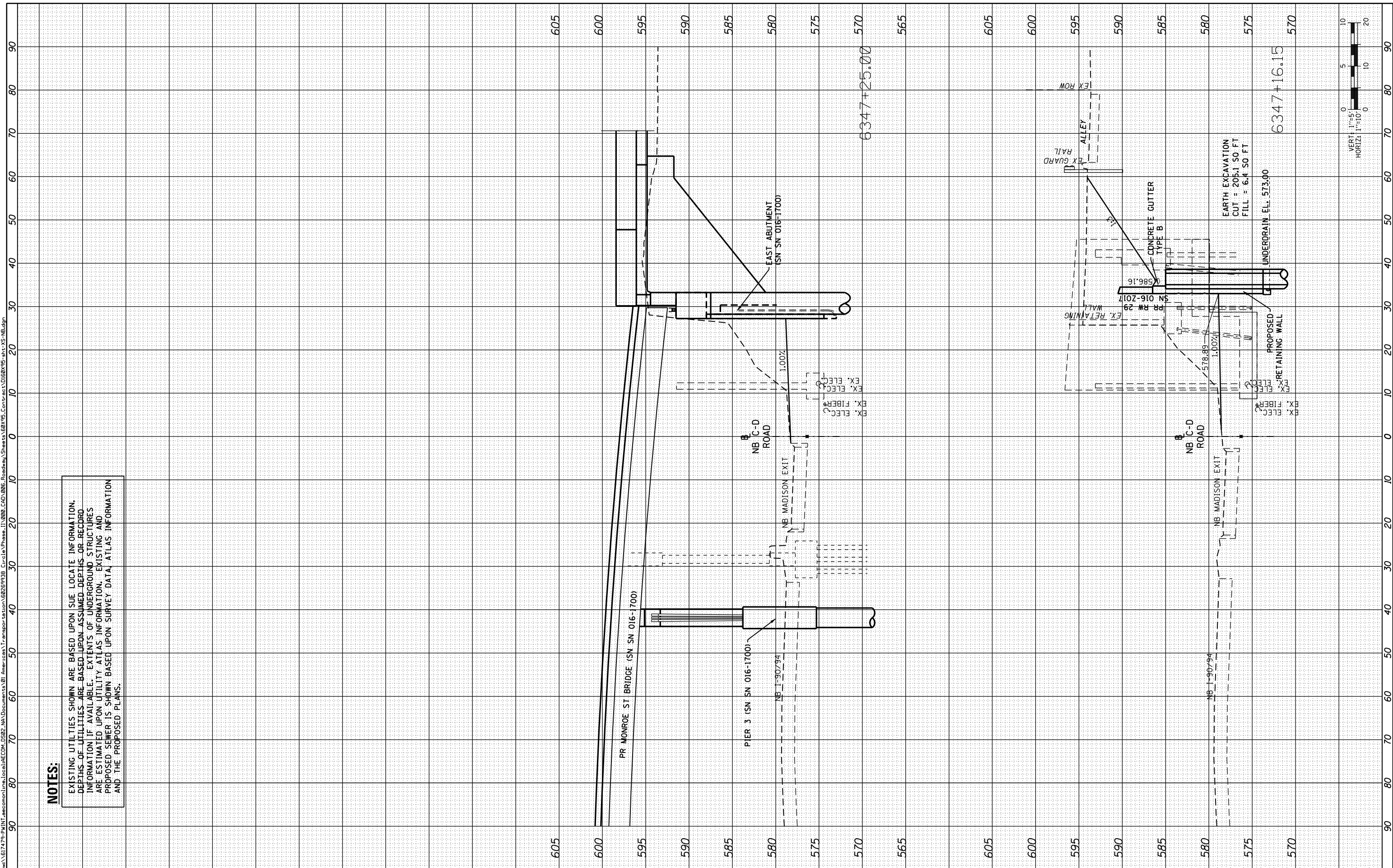
ILLINOIS FED. AID PROJECT

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS		

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NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.



D160X95-shr-XS-NB.dgn	DESIGNED - SDH	REVISED -
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PLOT SCALE = 20.0000 / in.	CHECKED -	REVISED -
PLOT DATE = 7/13/2017	DATE - 7/14/2017	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAINLINE CROSS SECTIONS
 RETAINING WALL 29 (SN 016-2017)

SCALE: 1:5V, 1:10H SHEET 06 OF 8 SHEETS STA. 6347+16.15 TO STA. 6347+25.00

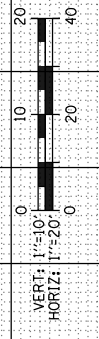
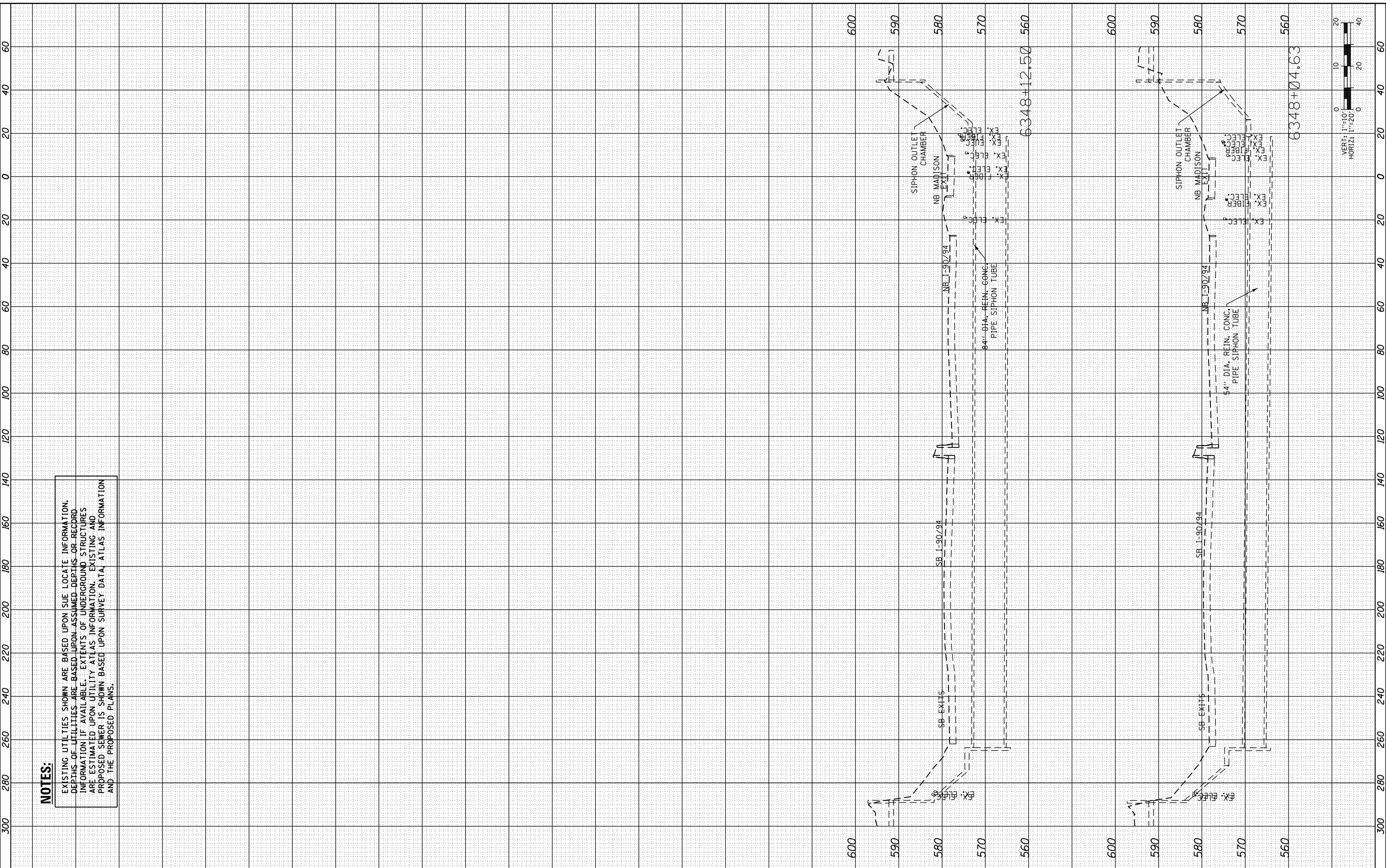
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1420	2014-016R&B	COOK	378	376
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
NO.	AREAS	CHECKED	

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
NO.	AREAS	CHECKED	

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NOTES:
 EXISTING UTILITIES SHOWN ARE BASED UPON SUE LOCATE INFORMATION. DEPTHS OF UTILITIES ARE BASED UPON ASSUMED DEPTHS OR RECORD INFORMATION IF AVAILABLE. EXTENTS OF UNDERGROUND STRUCTURES ARE ESTIMATED UPON UTILITY ATLAS INFORMATION. EXISTING AND PROPOSED SEWER IS SHOWN BASED UPON SURVEY DATA, ATLAS INFORMATION AND THE PROPOSED PLANS.



D160X95-sh1-XS-NB.dgn	DESIGNED - SDH	REVISED -
USER NAME = hollisd	DRAWN - SDH	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 7/13/2017	DATE - 7/14/2017	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

MAINLINE MAINLINE CROSS SECTIONS
 RETAINING WALL 29 (SN 016-2017)

SCALE: 1:10V, 1:20H SHEET 08 OF 8 SHEETS STA. 6348+04.63 TO STA. 6348+12.50

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1420	2014-016R&B	COOK	378	378
CONTRACT NO. 60X95				
ILLINOIS FED. AID PROJECT				