

F.A.I. ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	1
FED. ROAD DIST. NO. 7		ILLINOIS	CONTRACT NO. 74C65	

**11-17-2023 LETTING ITEM 084**

**INDEX OF SHEETS**

**SHEET NO. DESCRIPTION**

- 1. COVER SHEET
- 2. SUMMARY OF QUANTITIES AND MIX DESIGN
- 3-5. SCHEDULE OF QUANTITIES
- 6. ALIGNMENT, TIES, & BENCHMARKS
- 7-10. PLAN & PROFILE
- 11-24. MAINTENANCE OF TRAFFIC
- 25-43. STRUCTURE PLANS S.N. 018-0001/018-0002
- 44-53. STRUCTURE PLANS S.N. 018-0003/018-0004

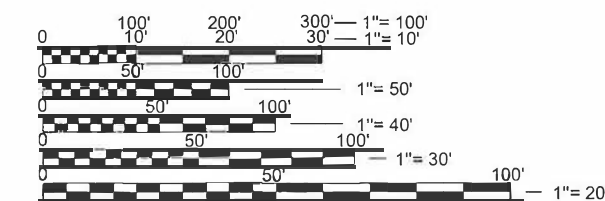
**HIGHWAY STANDARDS:**

000001-08	701411-09	
001001-02	701428-01	
001006	701901-08	
420001-10	704001-08	
642006-01	720001-01	
701400-11	720006-04	
701401-13	720011-01	
701402-12	780001-05	
701406-13		

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**PROPOSED**  
**HIGHWAY PLANS**

**F.A.I. ROUTE 57 (I-57)**  
**SECTION D7 BRIDGE REPAIRS 2023-9**  
**BRIDGE JOINT REPLACE /REPAIR**  
**CUMBERLAND COUNTY**  
**C-97-142-23**

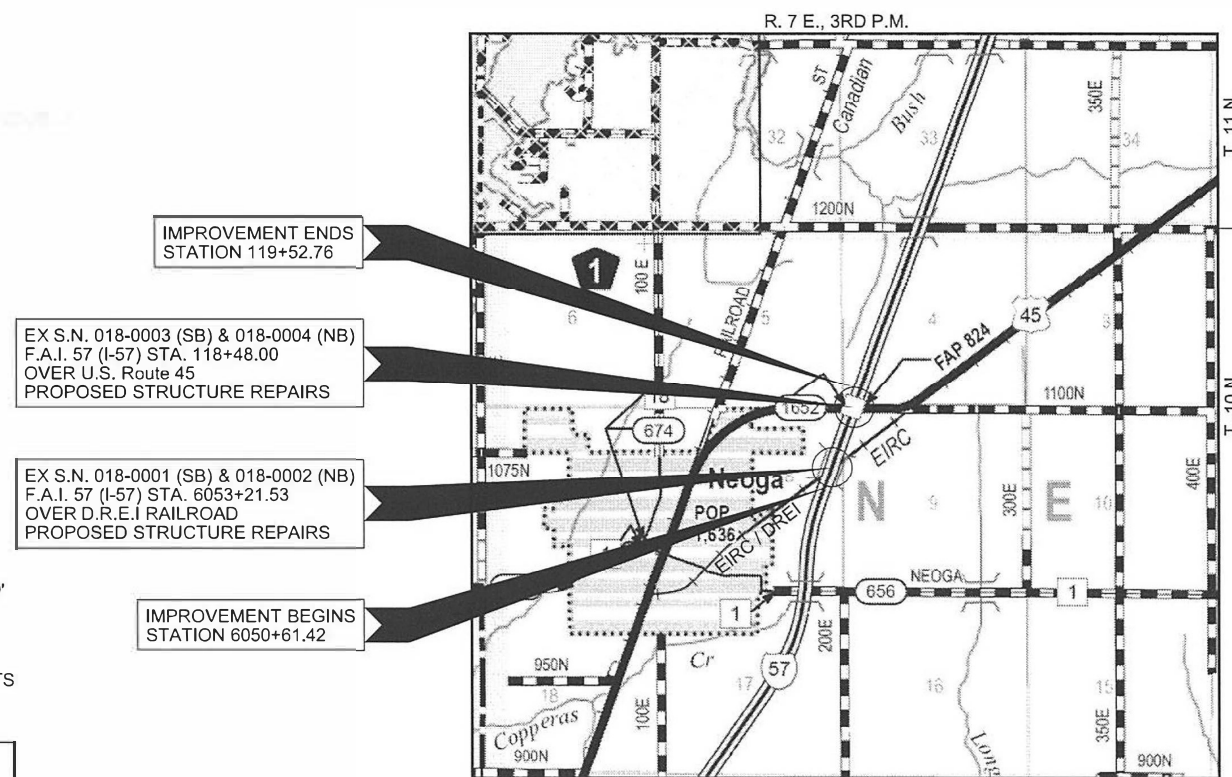
**D-97-111-23**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

**PROJECT ENGINEER: MATT BOWER (217) 342-8359**  
**PROJECT MANAGER: MICHAEL LORENTZ (217) 546-3400**  
**CONTRACT NO. 74C65**



**LOCATION MAP**

APPROXIMATE SCALE: 0 1/2 MILE  
 GROSS LENGTH OF SECTION = 592.1 FEET = 0.112 MILES  
 NET LENGTH OF SECTION = 592.1 FEET = 0.112 MILES



08/29/2023  
 MICHAEL LORENTZ  
 LICENSED PROFESSIONAL ENGINEER  
 STATE OF ILLINOIS  
 062-068792

EXPIRES: 11/30/2023  
 SHEETS: 1-24

08/29/2023  
 STEVEN W. MCGGINSON  
 LICENSED STRUCTURAL ENGINEER  
 STATE OF ILLINOIS  
 081-6464

EXPIRES: 11/30/2024  
 SHEETS: 25-53

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

SUBMITTED August 29 2023

Jeffrey P Myrka  
 REGIONAL ENGINEER

October 13, 2023

Steph M. A. [Signature]  
 ENGINEER OF DESIGN AND ENVIRONMENT

October 13, 2023

Steph M. A. [Signature]  
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY**  
**OF THE STATE OF ILLINOIS**



78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6"											
DESCRIPTION	START STA	END STA	TYPE	COLOR	QUANTITY (FOOT)	DESCRIPTION	START STA	END STA	TYPE	COLOR	QUANTITY (FOOT)
SB 018-0001	6050+61.4	6054+75.9	SOLID	YELLOW	414.5	SB 018-0003	117+43.2	119+20.8	SOLID	YELLOW	177.6
SB 018-0001	6050+61.4	6054+75.9	DASH 10'-30'	WHITE	103.6	SB 018-0003	117+43.2	119+20.8	DASH 10'-30'	WHITE	44.4
SB 018-0001	6050+61.4	6054+75.9	SOLID	WHITE	414.5	SB 018-0003	117+43.2	119+20.8	SOLID	WHITE	177.6
NB 018-0002	6051+95.6	6056+10.1	SOLID	YELLOW	414.5	NB 018-0004	117+75.2	119+52.8	SOLID	YELLOW	177.6
NB 018-0002	6051+95.6	6056+10.1	DASH 10'-30'	WHITE	103.6	NB 018-0004	117+75.2	119+52.8	DASH 10'-30'	WHITE	44.4
NB 018-0002	6051+95.6	6056+10.1	SOLID	WHITE	414.5	NB 018-0004	117+75.2	119+52.8	SOLID	WHITE	177.6
					TOTAL SN 0001 & 0002						1866.0
										TOTAL SN 0003 & 0004	800.0
										PAY ITEM TOTAL	2666.0

64200108 SHOULDER RUMBLE STRIPS, 8 INCH			
DESCRIPTION	START STA	END STA	QUANTITY (FOOT)
SB 018-0001	6049+00.0	6051+00.0	200.0
SB 018-0001	6049+00.0	6051+40.0	240.0
SB 018-0001	6054+40.0	6056+56.0	216.0
NB 018-0002	6050+00.0	6052+31.0	231.0
NB 018-0002	6050+00.0	6052+74.0	274.0
NB 018-0002	6055+32.0	6057+70.0	238.0
SB 018-0003	114+65.0	117+57.0	292.0
SB 018-0003	116+45.0	117+68.0	123.0
SB 018-0003	118+97.0	122+60.0	363.0
SB 018-0003	119+07.0	120+80.0	173.0
NB 018-0004	114+20.0	118+00.0	380.0
NB 018-0004	116+00.0	117+89.0	189.0
NB 018-0004	119+28.0	120+55.0	127.0
NB 018-0004	119+39.0	122+35.0	296.0
TOTAL			3342.0

X0326650 FILLING EXISTING RUMBLE STRIP			
DESCRIPTION	START STA	END STA	QUANTITY (FOOT)
SB 018-0001	6049+00.0	6050+61.0	161.0
SB 018-0001	6049+00.0	6050+96.0	196.0
SB 018-0001	6054+76.0	6056+70.0	194.0
NB 018-0002	6050+00.0	6051+96.0	196.0
NB 018-0002	6050+00.0	6052+01.0	201.0
NB 018-0002	6056+10.0	6057+70.0	160.0
SB 018-0003	114+65.0	117+57.0	292.0
SB 018-0003	116+45.0	117+43.0	98.0
SB 018-0003	118+97.0	122+60.0	363.0
SB 018-0003	119+21.0	120+80.0	159.0
NB 018-0004	114+20.0	118+00.0	380.0
NB 018-0004	116+00.0	117+75.0	175.0
NB 018-0004	119+53.0	120+55.0	102.0
NB 018-0004	119+39.0	122+35.0	296.0
TOTAL			2973.0

MAINTENANCE OF TRAFFIC				ITEM #	70100205	70100207	70100420	70107007	70300150	70300240	70400100			70400125	70400200	70600280	70600370
				NAME	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401*	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402*	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411*	PAVEMENT MARKING BLACKOUT TAPE, 7"	SHORT TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LINE 6"	TEMPORARY CONCRETE BARRIER	PINNING TEMPORARY CONCRETE BARRIER, EACH PIN	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMP. (SEVERE USE, NARROW), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (SEVERE USE, NARROW), TEST LEVEL 3	EACH	EACH
LOCATION	FROM		TO		EACH	EACH	EACH	FOOT	SQ FT	FOOT	FOOT	UNITS	PINNED	3 / UNIT EACH	FOOT	EACH	EACH
	STATION	OFFSET	STATION	OFFSET													
PRE-STAGE																	
NB 0002	6035+00.0		6045+00.0		1												
NB 0002	6053+46.0		105+41.3				1										
SB 0001	6054+00.0		106+37.0				1										
SB 0003	130+00.0		136+00.0				1										
SB 0003	136+00.0		146+00.0		1												
PRE-STAGE TOTAL					2	0	3	0.0	0.0	0.0	0.0	0	0	0.0	0	0	0
STAGE 1																	
NB 0002	6035+00.0		6056+10.0			1											
NB 0002	6056+10.0	48.5' RT	6057+70.0	44.0' RT						160.0							
NB 0002	6056+10.0	60.5' RT	6058+61.0	56.0' RT						62.8							
NB 0002	6049+60.0	56.0' RT	6055+60.0	56.0' RT				600.0	350.0								
NB 0002	6049+84.0	32.0' RT														1	
NB 0002	6049+84.0	32.0' RT	6051+70.0	47.5' RT						187.5	187.5	15	NO	9			
NB 0002	6051+70.0	47.5' RT	6056+10.0	47.5' RT						437.5	437.5	35	YES	108			
SB 0001	6049+00.0	44.0' LT	6050+60.0	48.5' LT						160.0							
SB 0001	6049+00.0	56.0' LT	6050+60.0	60.5' LT						160.0							
SB 0001	6049+00.0	56.0' LT	6054+35.0	56.0' RT				535.0	312.1								
SB 0001	6050+60.0		6057+11.0			1											
SB 0001	104+00.0		109+50.0				1										
SB 0001	106+00.0	56.0' LT	108+00.0	56.0' LT				200.0	116.7								
SB 0001	106+00.0	56.0' LT	108+10.0	60.0' LT				210.0	122.5								
SB 0001	6050+60.0	47.5' LT	6055+01.0	47.5' LT						437.5	437.5	35	YES	108			
SB 0001	6055+01.0	47.5' LT	6056+87.0	32.0' LT						187.5	187.5	15	NO	9			
SB 0001	6056+87.0	32.0' LT														1	
NB 0004	114+20.0		119+55.0			1											
NB 0004	119+55.0	52.0' RT	122+35.0	44.0' RT						280.0							
NB 0004	119+55.0	64.0' RT	122+35.0	56.0' RT						280.0							
NB 0004	114+20.0	56.0' RT	122+35.0	56.0' RT				815.0	475.4								
NB 0004	115+63.0	34.4' RT														1	
NB 0004	115+63.0	34.4' RT	117+50.0	50.0' RT						187.5	187.5	15	NO	9			
NB 0004	117+50.0	50.0' RT	119+53.0	50.0' RT						200.0	200.0	16	YES	51			
SB 0003	114+63.0	44.0' LT	117+43.0	52.0' LT						280.0							
SB 0003	114+63.0	56.0' LT	117+43.0	64.0' LT						280.0							
SB 0003	117+43.0		146+00.0			1											
SB 0003	114+63.0	56.0' LT	122+76.0	56.0' LT				813.0	474.3								
SB 0003	117+43.0	49.5' LT	119+46.0	49.5' LT						200.0	200.0	16	YES	51			
SB 0003	119+46.0	49.5' LT	121+32.0	34.5' LT						187.5	187.5	15	NO	9			
SB 0003	121+32.0	34.5' LT														1	
STAGE 1 TOTAL					0	4	1	3173.0	1850.9	3687.8	2025.0	162		354	0.0	4	0

FILE NAME = 230289-ehi-schedule.dgn	USER NAME = mlbrentz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62776	PLOT SCALE = \$SCALES	CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 164.002959	PLOT DATE = 10/17/2023	DRAWN - G.D.M.	REVISED -
		CHECKED - S.W.M.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

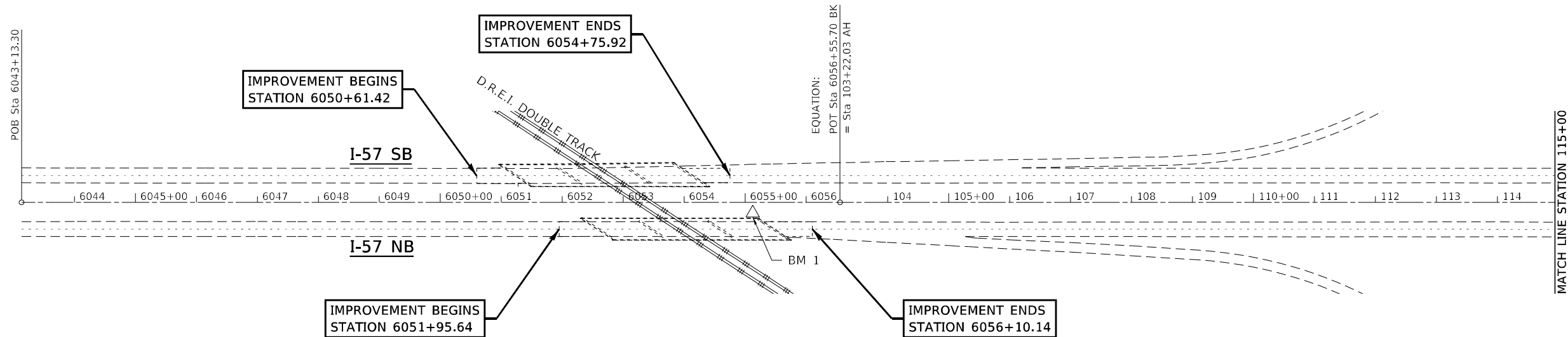
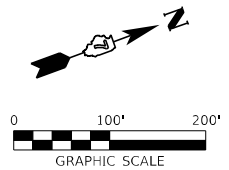
**SCHEDULE OF QUANTITIES**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	4
CONTRACT NO. 74C65				
SHEET NO. 2 OF 3 SHEETS				
ILLINOIS FED. AID PROJECT				



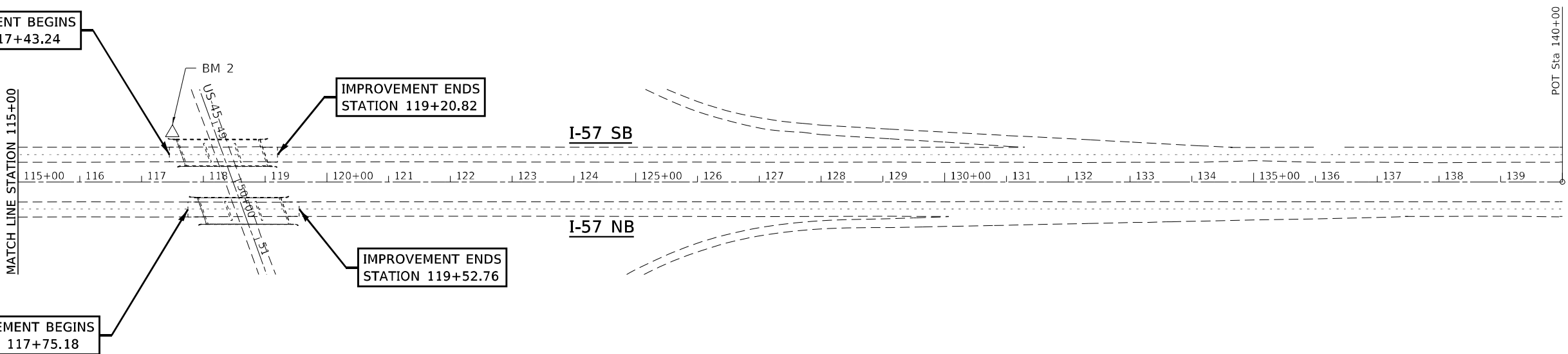
MAINTENANCE OF TRAFFIC					ITEM #	70100205	70100207	70100420	70107007	70300150	70300240	70400100		70400125	70400200	70600280	70600370																	
					NAME	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401*	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402*	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411*	PAVEMENT MARKING BLACKOUT TAPE, 7"	SHORT TERM PAVEMENT MARKING REMOVAL	TEMPORARY PAVEMENT MARKING - LINE 6"	TEMPORARY CONCRETE BARRIER	PINNING TEMPORARY CONCRETE BARRIER, EACH PIN	RELOCATE TEMPORARY CONCRETE BARRIER	IMPACT ATTENUATORS, TEMP. (SEVERE USE, NARROW), TEST LEVEL 3	IMPACT ATTENUATORS, RELOCATE (SEVERE USE, NARROW), TEST LEVEL 3	LOCATIONS	FROM	TO	EACH	EACH	EACH	FOOT	SQ FT	FOOT	FOOT	UNITS	PINNED	3 / UNIT EACH	FOOT	EACH	EACH		
LOCATION	STATION	OFFSET	STATION	OFFSET																														
<b>STAGE 2</b>																																		
NB 0002	6035+00.0		6056+10.0			1																												
NB 0002	6056+10.0	27.6' RT	6057+70.0	32.0' RT							160.0																							
NB 0002	6056+10.0	39.6' RT	6058+61.0	44.0' RT							62.8																							
NB 0002	6049+60.0	32.0' RT	6057+70.0	32.0' RT					810.0	472.5																								
NB 0002	6049+84.0	56.0' RT																														1		
NB 0002	6049+84.0	32.0' RT	6051+70.0	41.0' RT							187.5	15	NO	9	187.5																			
NB 0002	6051+70.0	41.0' RT	6056+10.0	41.0' RT							437.5	35	YES	108	437.5																			
SB 0001	6050+60.0		6057+11.0			1																												
SB 0001	6049+00.0	44.0' LT	6050+60.0	39.6' LT							160.0																							
SB 0001	6049+00.0	32.0' LT	6050+60.0	27.6' LT							160.0																							
SB 0001	6049+00.0	56.0' LT	6057+11.0	56.0' RT					811.0	473.1																								
SB 0001	103+80.0		110+00.0				1																											
SB 0001	6050+60.0	41.0' LT	6055+01.0	41.0' LT							437.5	35	YES	108	437.5																			
SB 0001	6055+01.0	41.0' LT	6056+87.0	56.6' LT							187.5	15	NO	9	187.5																			
SB 0001	6056+87.0	56.6' LT																															1	
NB 0004	116+00.0		119+55.0			1																												
NB 0004	119+55.0	52.0' RT	120+55.0	44.0' RT							100.0																							
NB 0004	119+55.0	64.0' RT	120+55.0	56.0' RT							100.0																							
NB 0004	115+63.0	56.0' RT																															1	
NB 0004	115+63.0	56.0' RT	117+50.0	43.5' RT							187.5	15	NO	9	187.5																			
NB 0004	117+50.0	43.5' RT	119+55.0	43.5' RT							200.0	16	YES	51	200.0																			
SB 0003	117+43.0		146+00.0			1																												
SB 0003	116+43.0	44.0' LT	117+43.0	41.5' LT							100.0																							
SB 0003	116+43.0	32.0' LT	117+43.0	29.5' LT							100.0																							
SB 0003	117+43.0	43.5' LT	119+46.0	43.5' LT							200.0	16	YES	51	200.0																			
SB 0003	119+46.0	43.5' LT	121+32.0								187.5	15	NO	9	187.5																			
SB 0003	121+32.0																																1	
SB 0003	130+00.0		136+00.0				1																											
<b>STAGE 2 TOTAL</b>						0	4	2	1621.0	945.6	2967.8	0.0	162		354	2025.0	0	4																
<b>STAGE 3</b>																																		
NB 0002	6035+00.0		6056+10.0			1																												
SB 0003	136+00.0		146+00.0			1																												
<b>STAGE 3 TOTAL</b>						2	0	0	0.0	0.0	0.0	0.0	0		0	0.0	0	0																
<b>TOTAL STRUCTURES 01 &amp; 02</b>																																		
<b>Structure 01 &amp; 02 TOTAL</b>						2	4	4	3166.0	1847.0	3585.5	1250.0	200		468	1250.0	2	2																
<b>TOTAL STRUCTURES 03 &amp; 04</b>																																		
<b>Structure 03 &amp; 04 TOTAL</b>						2	4	2	1628.0	950.0	3070.5	775.0	124		240	775.0	2	2																
<b>GRAND TOTAL</b>						4*	8*	6*	4794.0	2797.0	6656.0	2025.0	324		708	2025.0	4	4																

NOTE \*: THESE TRAFFIC CONTROL STANDARD ITEMS ARE NOT PAID FOR SEPARATELY BUT SHALL BE INCIDENTAL TO PAY ITEM X7011015 TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).



I-57			
TYPE	STATION	NORTHING	EASTING
POB	6043+13.30	967490.6974	954961.9295
POT	6056+55.70 BK, 103+22.03 AH	968751.8973	955421.7347
POT	140+00	972207.3836	956681.5274

**BENCHMARK #1**  
 CHISELED "□" ON N.W.  
 CORNER OF N.B. S.N. 018-0002  
 STA. 6055+20, 24' RT.  
 ELEV. 692.845

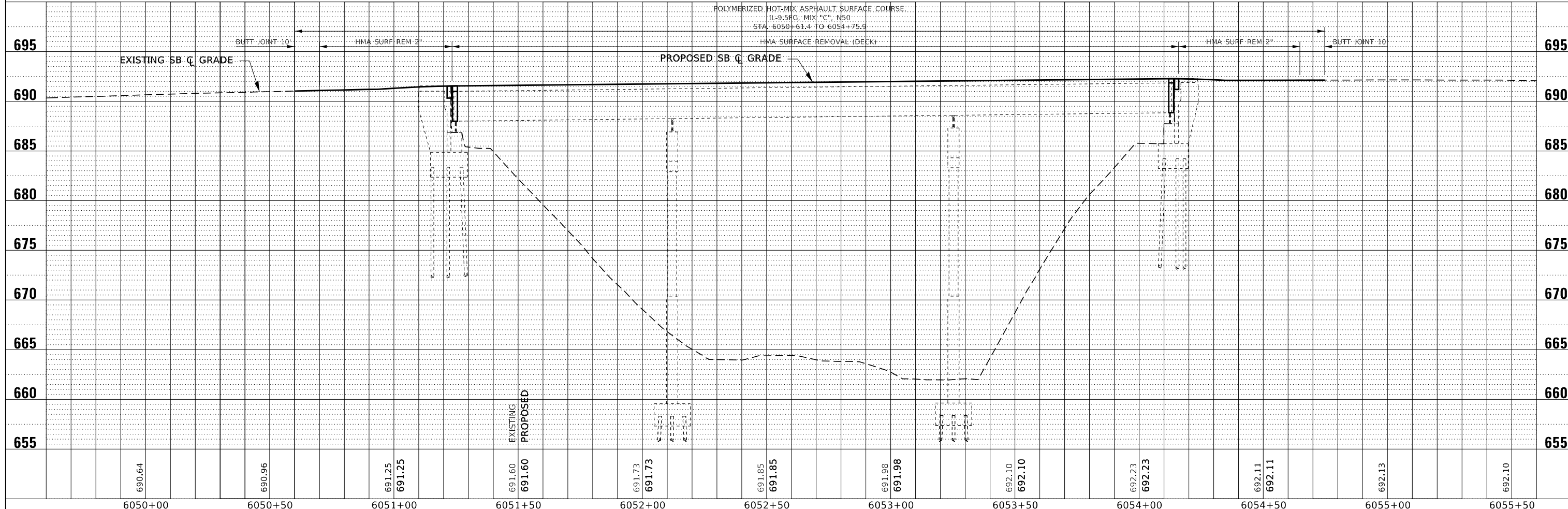
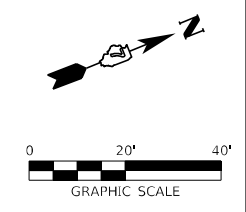
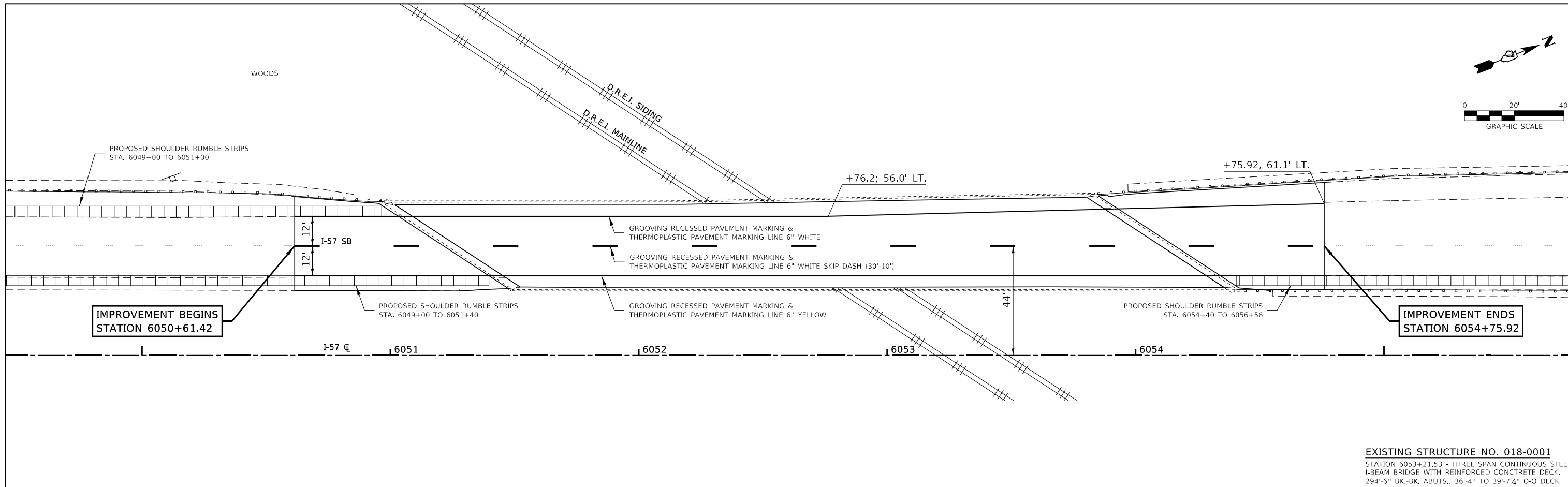


**BENCHMARK #2**  
 CHISELED "□" ON S.W.  
 CORNER OF S.B. S.N. 018-0003  
 STA. 117+55, 68' LT.  
 ELEV. 684.797

FILE NAME = 230269-shl-align lies.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ALIGNMENT, TIES, &amp; BENCHMARKS</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT SCALE =	CHECKED - S.W.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	6
PLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -	SHEET NO. 1 OF 1 SHEETS			CONTRACT NO. 74C65		ILLINOIS   FED. AID PROJECT		

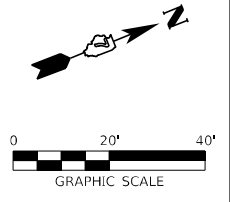
DATE	
BY	
STRIPPED	
PLOTTED	
ALIGNMENT CHECKED	
ASSEMBLY CHECKED	
CADD FILE NAME	
NO.	
PLAN	
NOTE BOOK	
NO.	

DATE	
BY	
STRIPPED	
PLOTTED	
GRADES CHECKED	
STRUCTURE NOTATIONS CHECKED	
NO.	
PROFILE	
NOTE BOOK	
NO.	

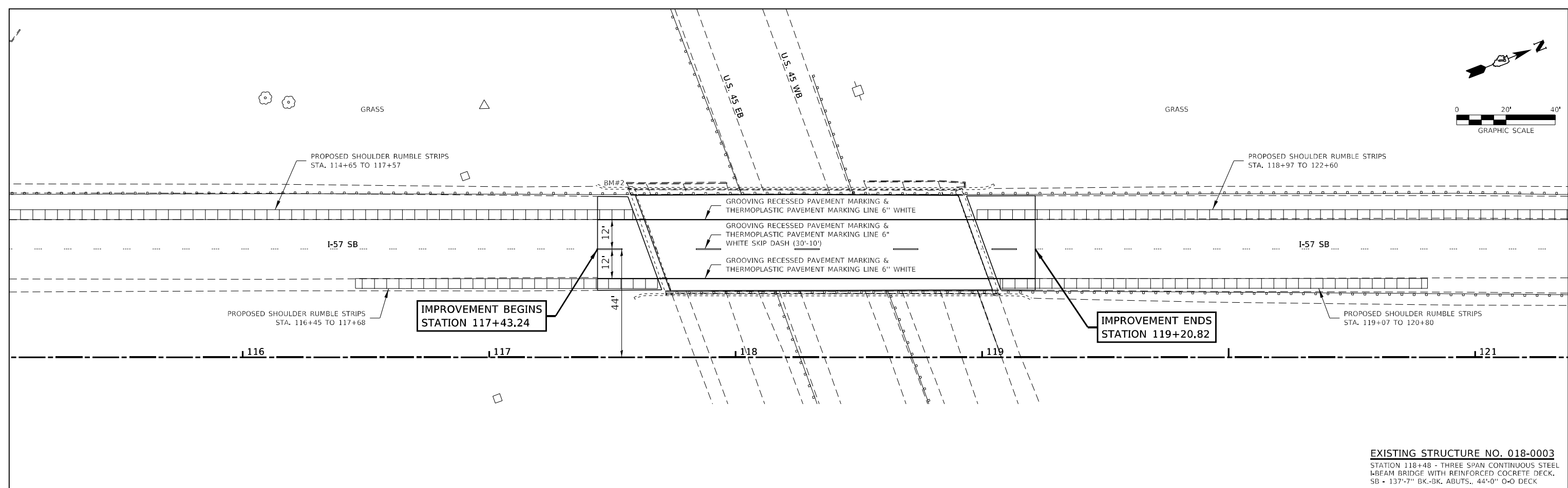


FILE NAME = 230269-shi-plnprf.dgn	USER NAME = mllorenz	DESIGNED - M.A.L.	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN &amp; PROFILE</b> <b>I-57 SB OVER D.R.E.I. - SN 018-0001</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
<b>HAMPTON, LENZINI AND RENWICK, INC.</b> <small>3055 STEVENSON DRIVE, SUITE 201          SPRINGFIELD, IL 62765 472765</small>		DRAWN - G.D.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	7	
PLOT SCALE = 5/8"=1'-0" PLOT DATE = 8/29/2023		CHECKED - S.W.M.	REVISED -			CONTRACT NO. 74C65					
		DATE - 08/07/2023	REVISED -			SCALE: 5V:20H		SHEET NO. 1 OF 4 SHEETS		STA. 6049+00 TO STA. 6056+00	



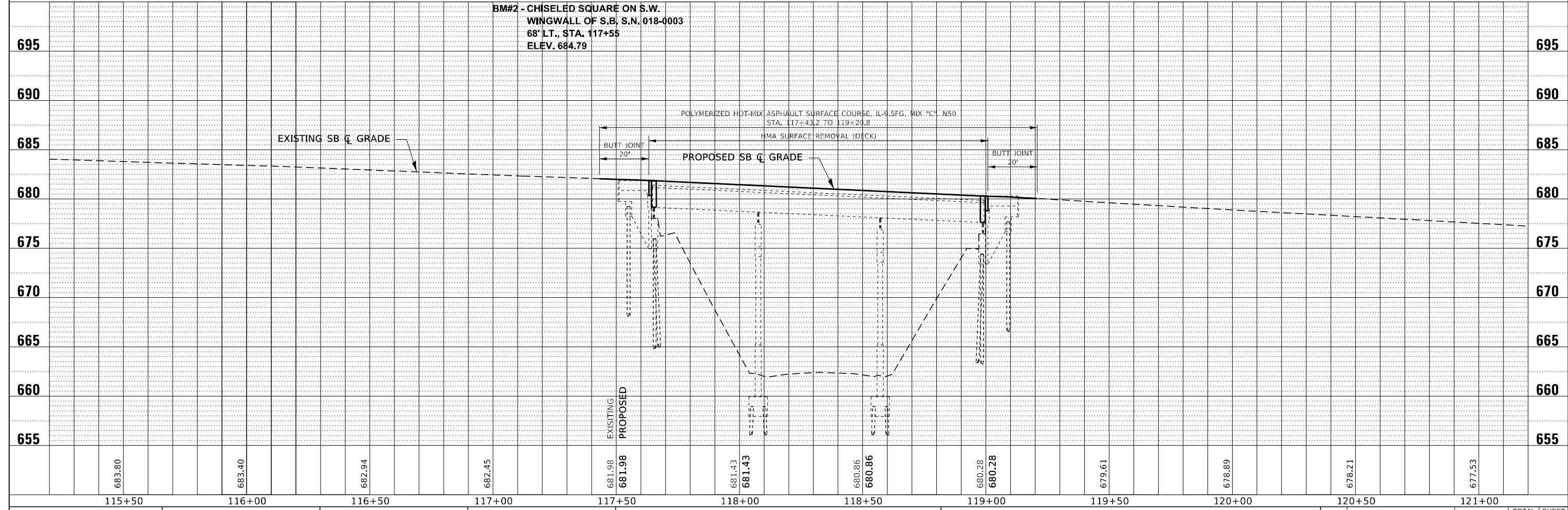


DATE	
BY	
REVIEWED	
PLANNED	
NOTED	
NO.	

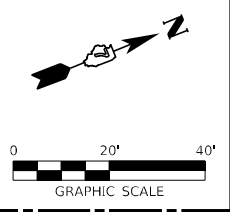


**EXISTING STRUCTURE NO. 018-0003**  
 STATION 118+48 - THREE SPAN CONTINUOUS STEEL I-BEAM BRIDGE WITH REINFORCED CONCRETE DECK.  
 SB - 137'-7" BK.-BK. ABUTS., 44'-0" O-O DECK

DATE	
BY	
REVIEWED	
PLANNED	
NOTED	
NO.	

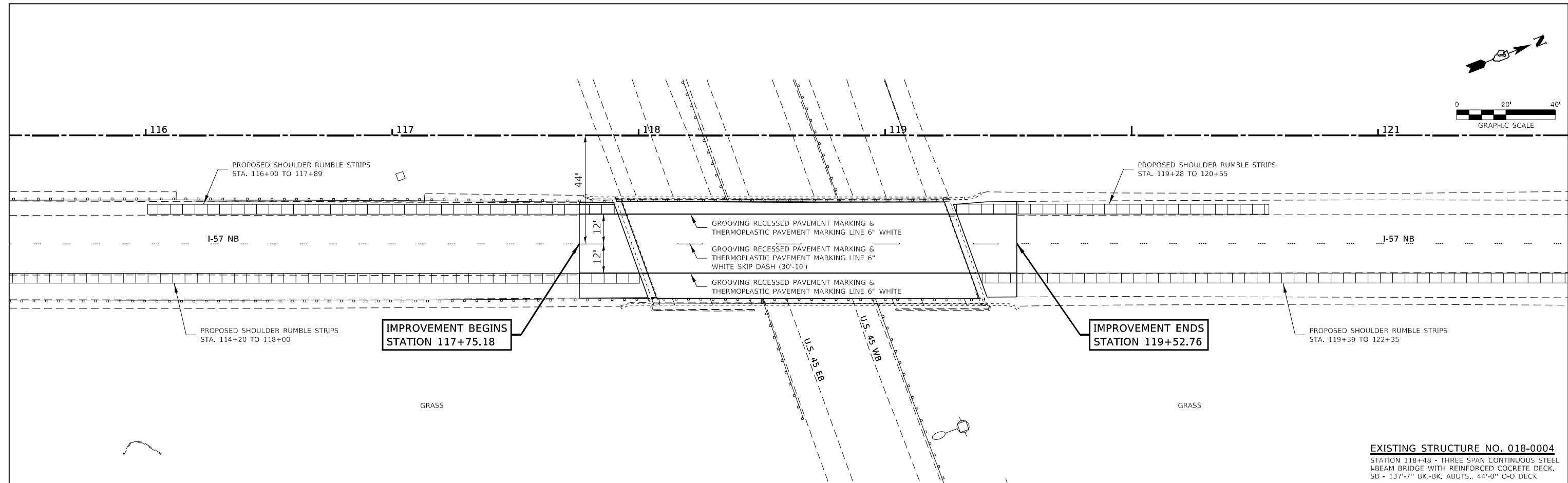


FILE NAME = 230269-shi-plnprf.dgn	USER NAME = mllorenz	DESIGNED - M.A.L.	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b> <b>PLAN &amp; PROFILE</b> <b>I-57 SB OVER U.S. 45 - SN 018-0003</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
HAMPTON, LENZINI AND RENWICK, INC. 3055 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, IL 62765	PLLOT SCALE = 5/8"=1'-0"	DRAWN - G.D.M.	REVISED -		57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	9
BLUNCE PROFESSIONAL DESIGN FIRM L.S. / P.E. / S.E. CORP. 184.000959	PLLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -		CONTRACT NO. 74C65				
		DATE - 08/07/2023	REVISED -		SCALE: 5V:20H	SHEET NO. 3 OF 4 SHEETS	STA. 115+00 TO STA. 122+00	ILLINOIS FED. AID PROJECT	

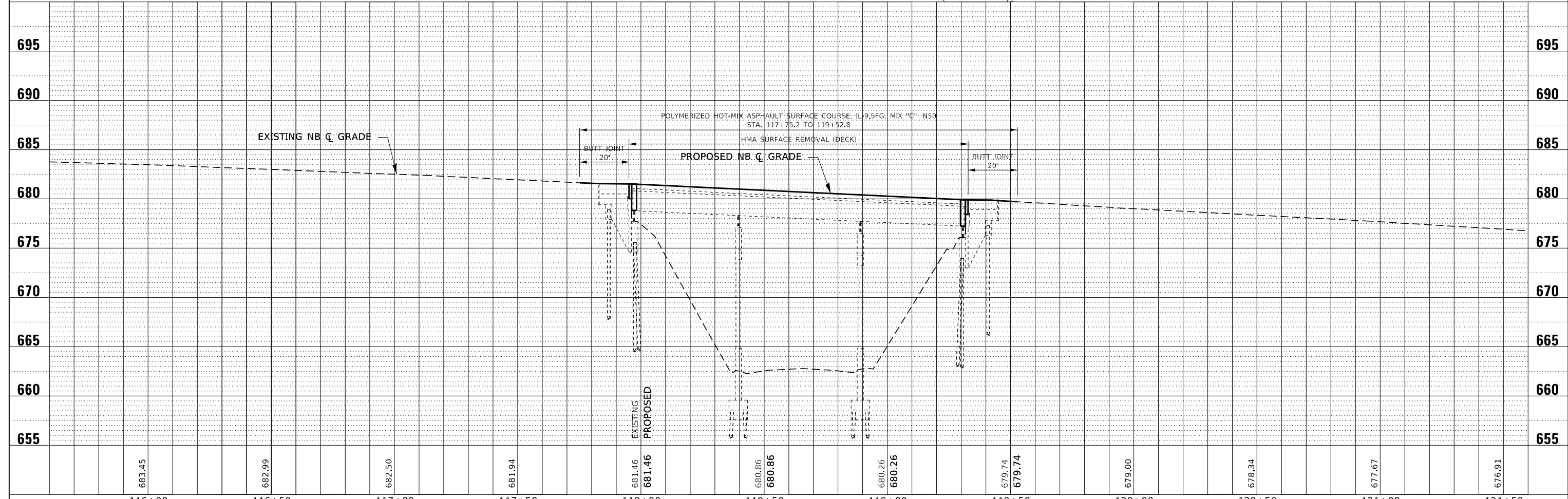


DATE	
BY	
REVIEWED	
PLANNED	
NOTED	
NO.	

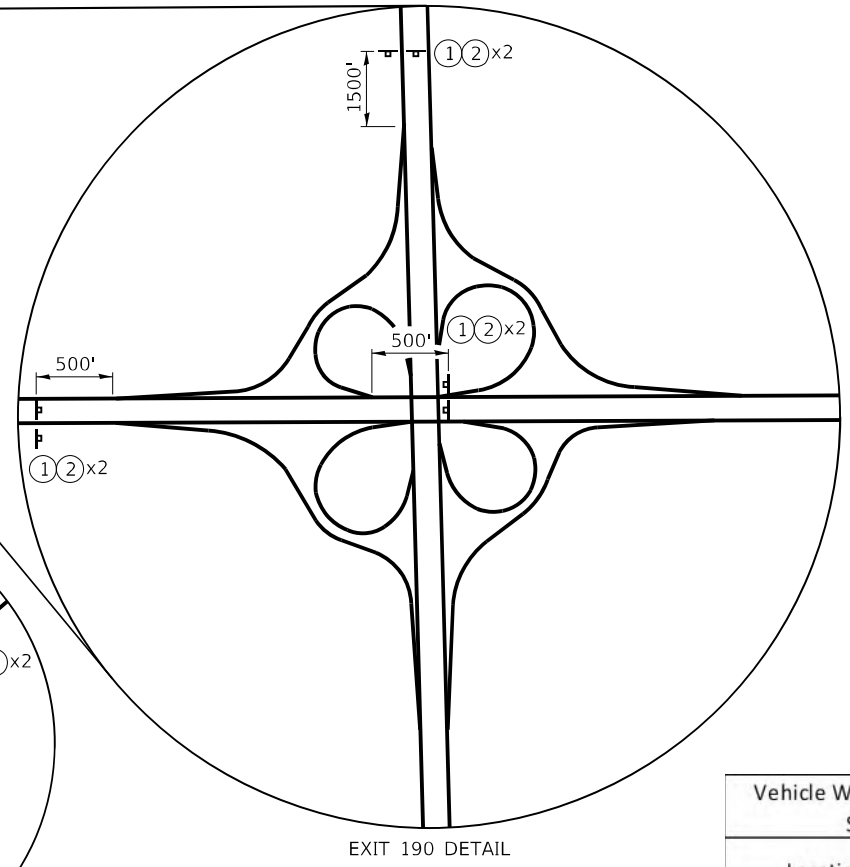
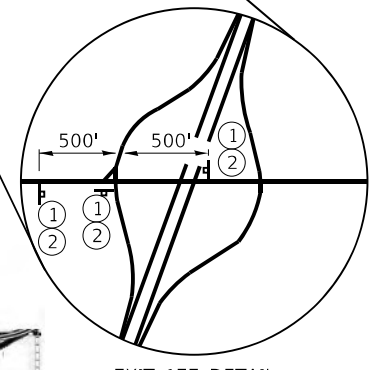
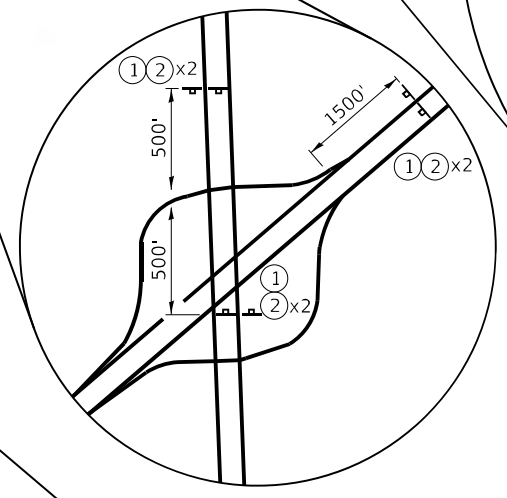
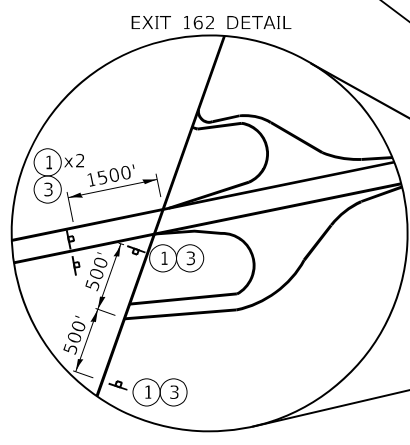
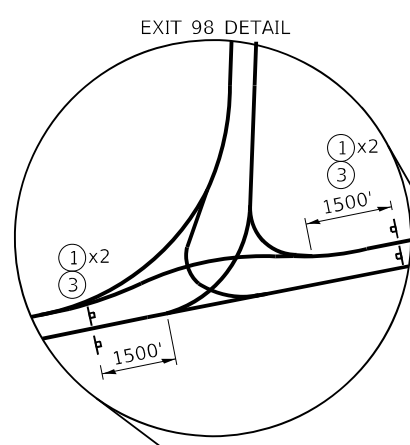
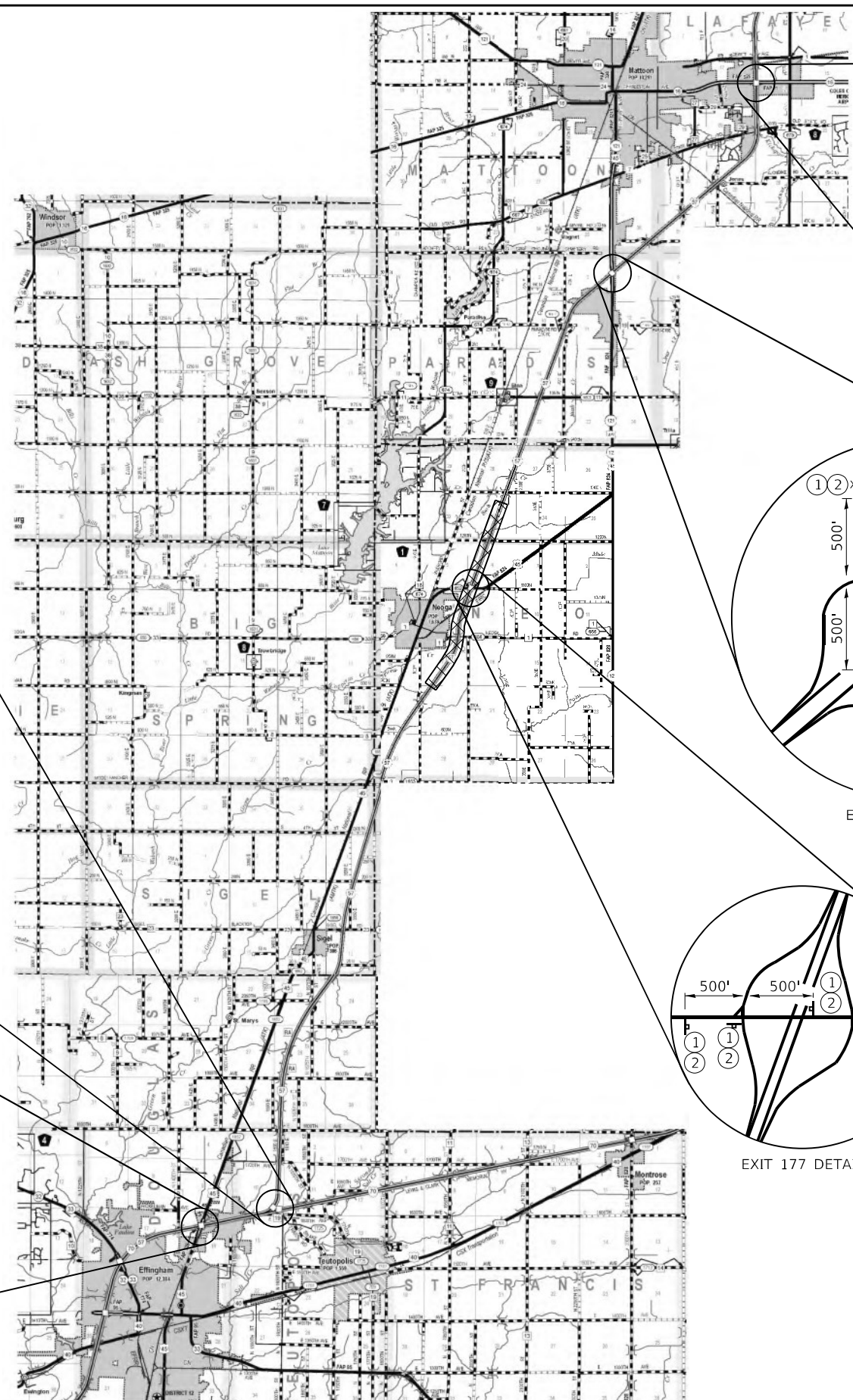
DATE	
BY	
REVIEWED	
PLANNED	
NOTED	
NO.	



**EXISTING STRUCTURE NO. 018-0004**  
 STATION 118+48 - THREE SPAN CONTINUOUS STEEL  
 I-BEAM BRIDGE WITH REINFORCED CONCRETE DECK.  
 SB - 137'-7" BK.-BK. ABUTS., 44'-0" O-O DECK



FILE NAME = 230269-shi-plnprf.dgn	USER NAME = mllorenz	DESIGNED - M.A.L.	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN &amp; PROFILE</b> <b>I-57 NB OVER U.S. 45 - SN 018-0004</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. 3055 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, IL 62776	PLLOT SCALE = 1/8"=1'-0"	DRAWN - G.D.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	10	
BLUNCE PROFESSIONAL DESIGN FIRM L.B. / P.E. / S.E. CORP. 184.000959	PLLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -			CONTRACT NO. 74C65					
		DATE - 08/07/2023	REVISED -			SCALE: 5V:20H	SHEET NO. 4 OF 4 SHEETS	STA. 115+00 TO STA. 122+00	ILLINOIS FED. AID PROJECT		



Vehicle Width Restriction Advanced Signing Schedule		
Location	# Signs	XX Miles Ahead
Exit 190	6	13
Exit 184	6	8
Exit 177	3	0.5
Exit 98	4	13
Exit 162	4	15

**LEGEND**

W12-1103

12''-18''

12''-18''

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT SCALE =	CHECKED - S.W.M.	REVISED -
	PLOT DATE = 8/29/2023	DRAWN - D.M.F.	REVISED -
		CHECKED - S.W.M.	REVISED -

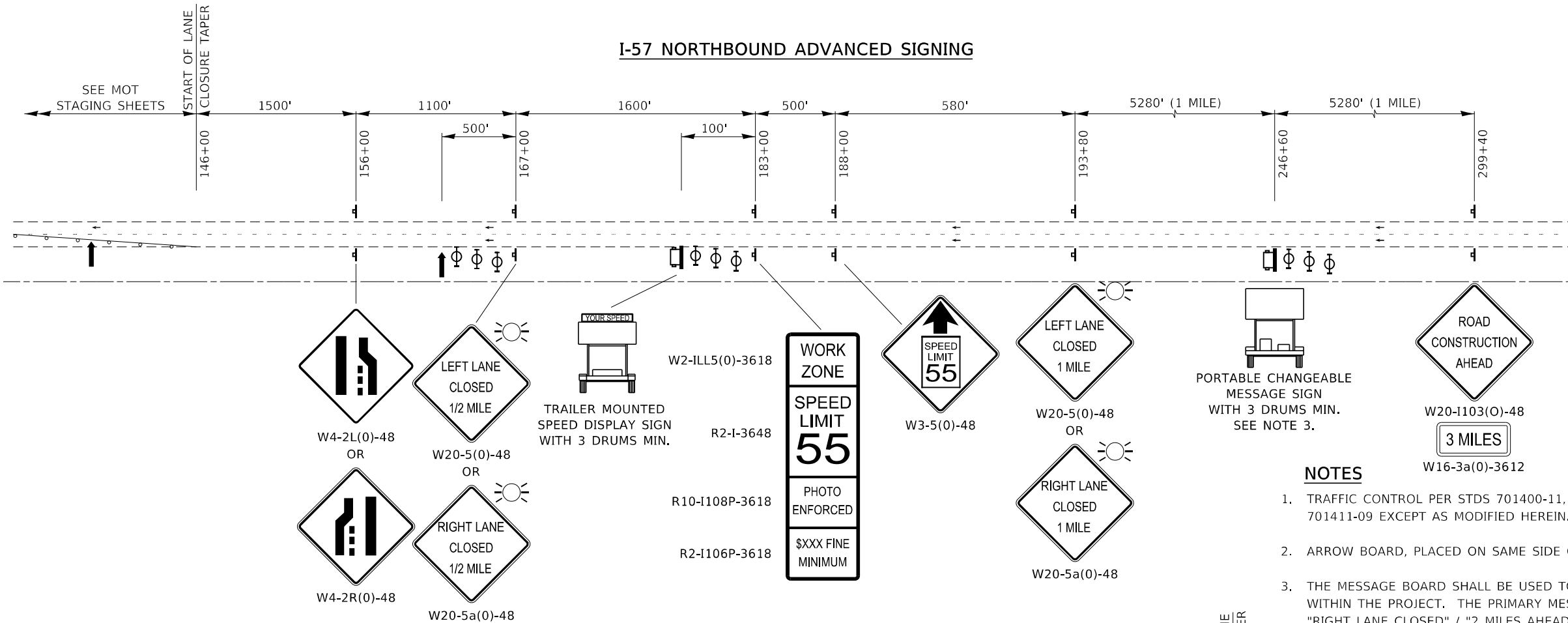
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**VEHICLE WIDTH RESTRICTIONS ADVANCED SIGNING  
MAINTENANCE OF TRAFFIC**

SHEET NO. 1 OF 14 SHEETS

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	11
CONTRACT NO. 74C65				
ILLINOIS   FED. AID PROJECT				

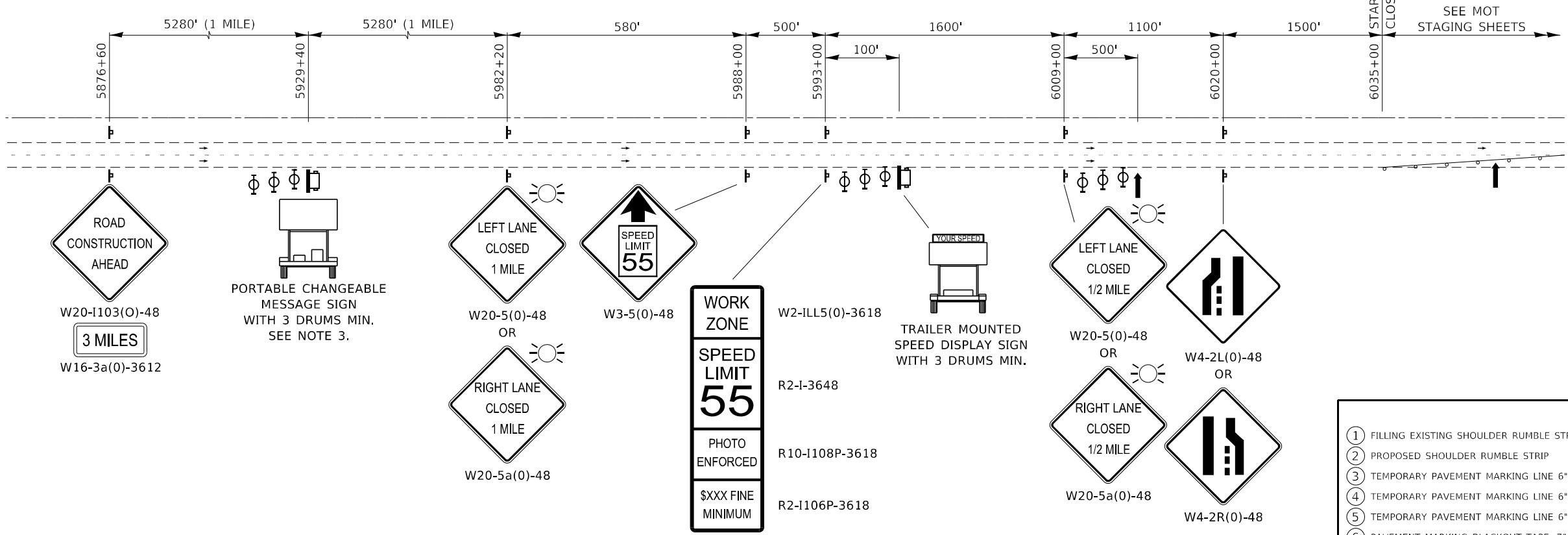
**I-57 NORTHBOUND ADVANCED SIGNING**



**NOTES**

1. TRAFFIC CONTROL PER STDS 701400-11, 701401-13, 701402-12, AND 701411-09 EXCEPT AS MODIFIED HEREIN.
2. ARROW BOARD, PLACED ON SAME SIDE OF ROAD AS LANE CLOSURE.
3. THE MESSAGE BOARD SHALL BE USED TO DISPLAY STATUS OF LANES WITHIN THE PROJECT. THE PRIMARY MESSAGES SHALL BE:  
 "RIGHT LANE CLOSED" / "2 MILES AHEAD"  
 "LEFT LANE CLOSED" / "2 MILES AHEAD"  
 "ALL LANES OPEN"

**I-57 SOUTHBOUND ADVANCED SIGNING**



**LEGEND**

①	FILLING EXISTING SHOULDER RUMBLE STRIP		WORK AREA
②	PROPOSED SHOULDER RUMBLE STRIP		TEMPORARY DRUM BARRIER
③	TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)		CONCRETE BARRIER
④	TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")		P - PINNED OR U - UN-PINNED
⑤	TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)		TRAFFIC CONTROL SIGN
⑥	PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)		ARROW BOARD
			CRASH ATTENUATOR
			DIRECTION OF TRAFFIC
			TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - D.M.F.	REVISED -
		CHECKED - S.W.M.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

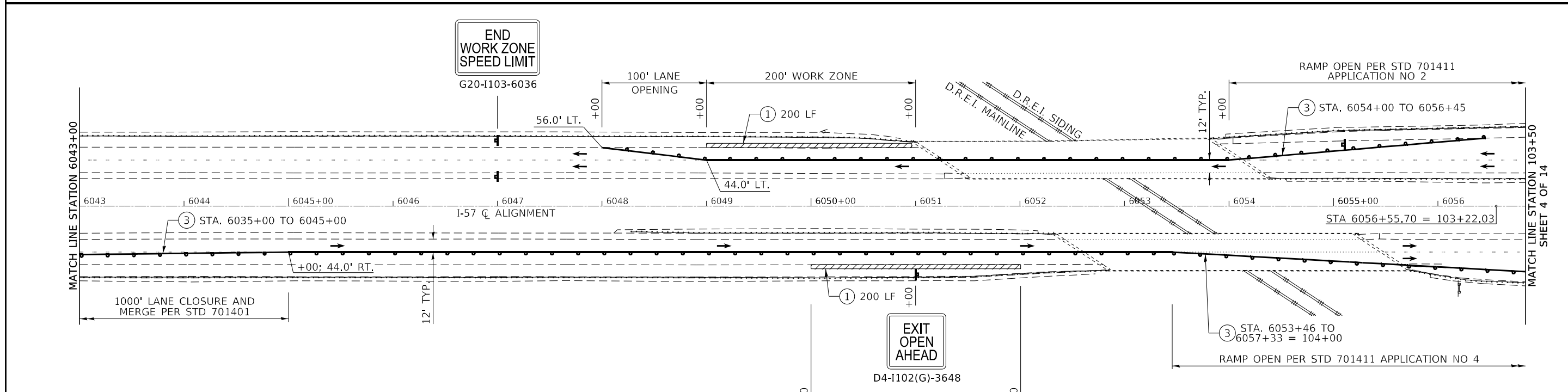
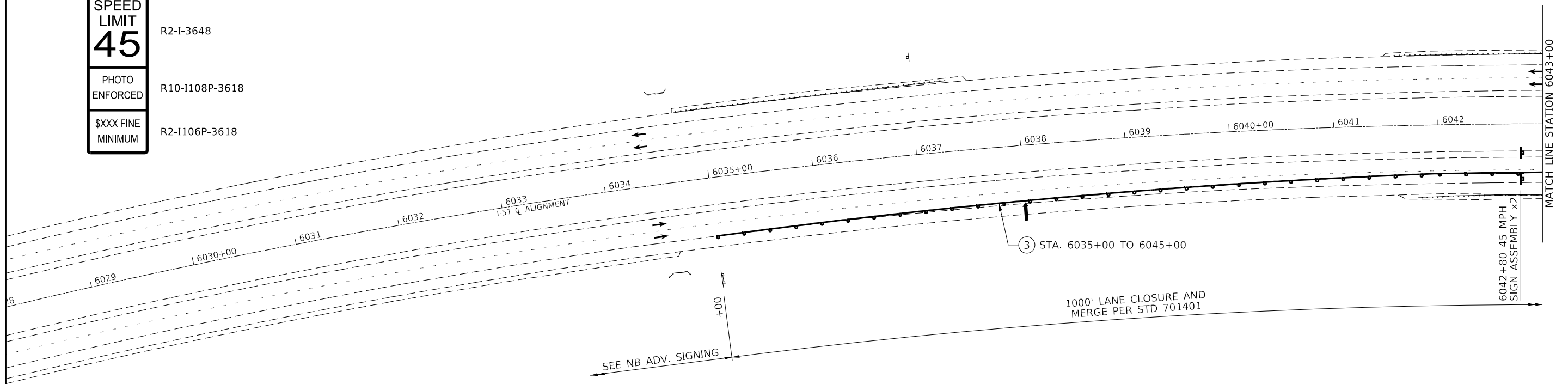
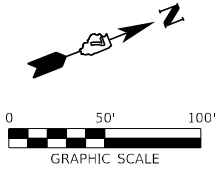
**ADVANCED SIGNING  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	12
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				



45 MPH SIGN ASSEMBLY

WORK ZONE	W2-ILL5(0)-3618
SPEED LIMIT	R2-I-3648
PHOTO ENFORCED	R10-I108P-3618
\$XXX FINE MINIMUM	R2-I106P-3618



**LEGEND**

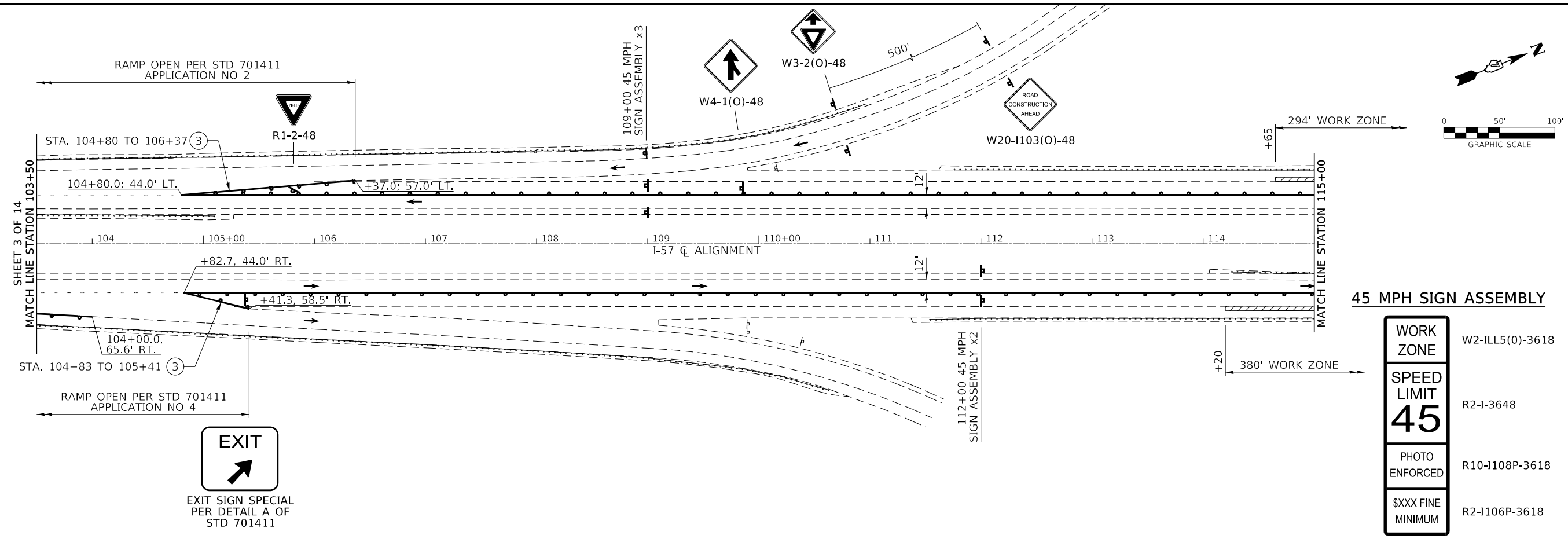
① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	CRASH ATTENUATOR
	DIRECTION OF TRAFFIC
	TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761		CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT SCALE =	DRAWN - D.M.F.	REVISED -
	PLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

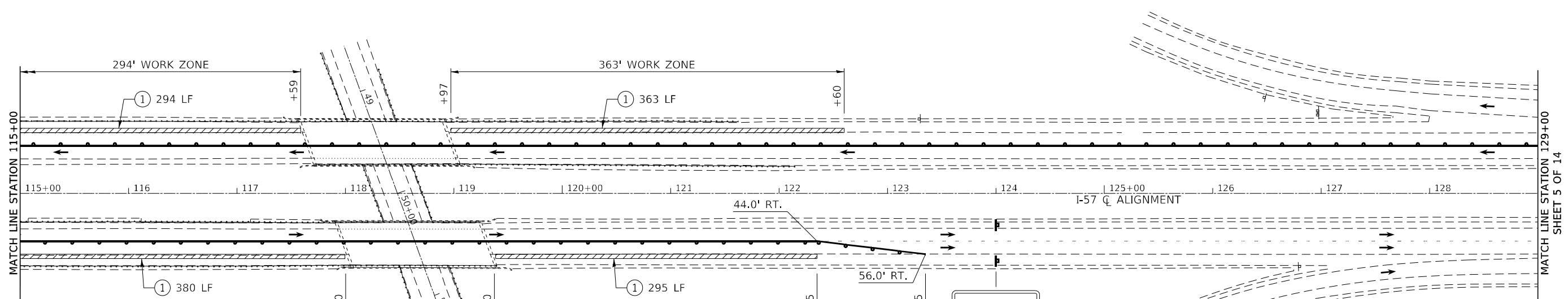
**PRE-STAGE  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	13
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				



**45 MPH SIGN ASSEMBLY**

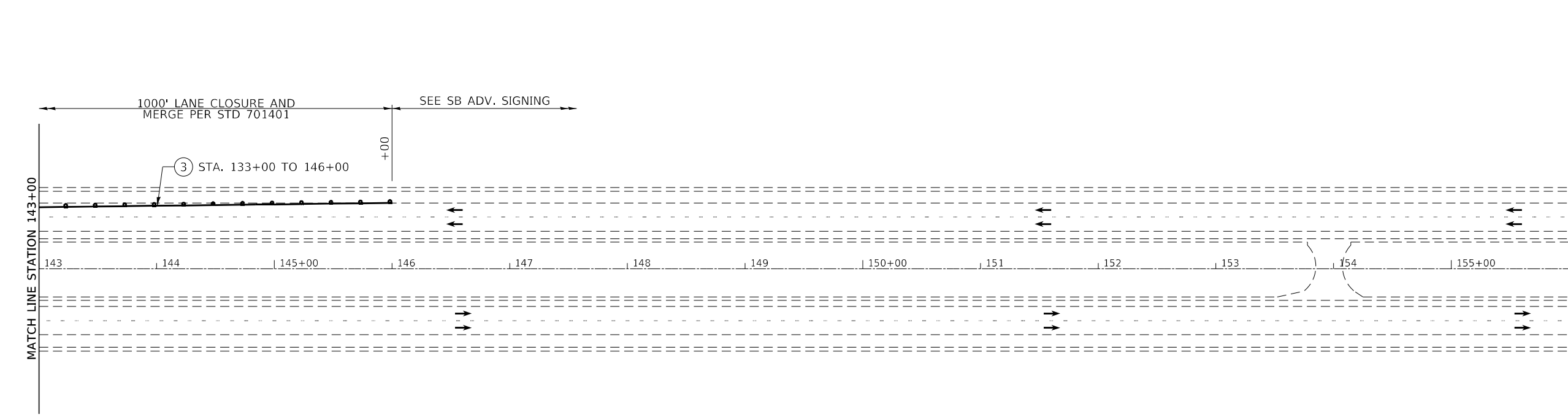
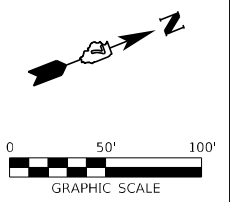
WORK ZONE	W2-ILL5(0)-3618
SPEED LIMIT 45	R2-I-3648
PHOTO ENFORCED	R10-I108P-3618
\$XXX FINE MINIMUM	R2-I106P-3618



**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	CRASH ATTENUATOR
	DIRECTION OF TRAFFIC
	TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PRE-STAGE MAINTENANCE OF TRAFFIC</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.009959	PLOT SCALE =	CHECKED - S.W.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	14
PLOT DATE = 8/29/2023	DRAWN - D.M.F.	CHECKED - S.W.M.	REVISED -			CONTRACT NO. 74C65				
						ILLINOIS FED. AID PROJECT				



**45 MPH SIGN ASSEMBLY**

<b>WORK ZONE</b>	W2-ILL5(0)-3618
<b>SPEED LIMIT 45</b>	R2-I-3648
<b>PHOTO ENFORCED</b>	R10-I108P-3618
<b>\$XXX FINE MINIMUM</b>	R2-I106P-3618

- LEGEND**
- ① FILLING EXISTING SHOULDER RUMBLE STRIP
  - ② PROPOSED SHOULDER RUMBLE STRIP
  - ③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)
  - ④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")
  - ⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)
  - ⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)
  - WORK AREA
  - TEMPORARY DRUM BARRIER
  - CONCRETE BARRIER P - PINNED OR U - UN-PINNED
  - TRAFFIC CONTROL SIGN
  - ARROW BOARD
  - CRASH ATTENUATOR
  - DIRECTION OF TRAFFIC
  - TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.00959		CHECKED - S.W.M.	REVISED -
	PLOT SCALE =	DRAWN - D.M.F.	REVISED -
	PLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -

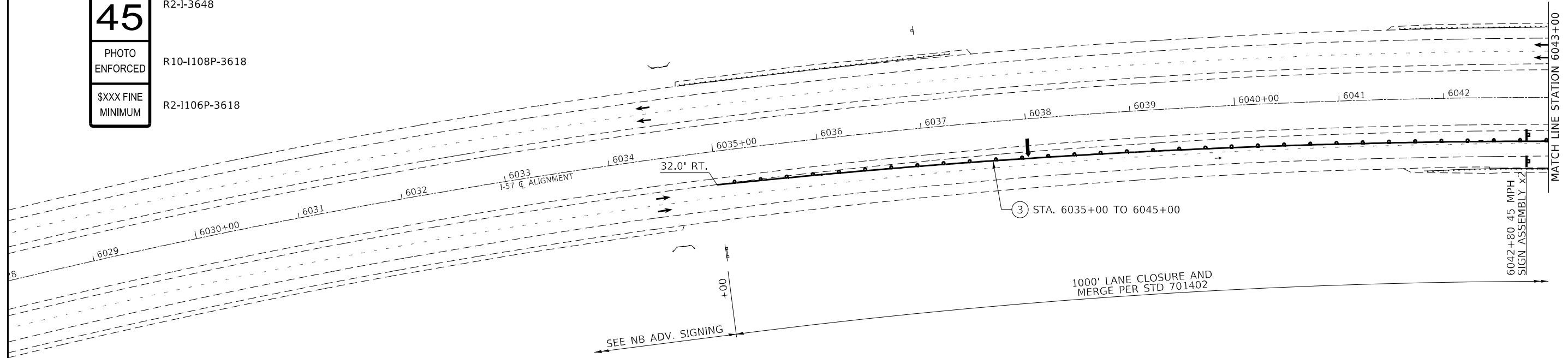
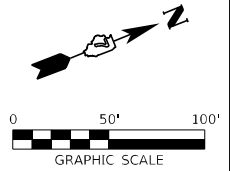
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PRE-STAGE  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	15
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				

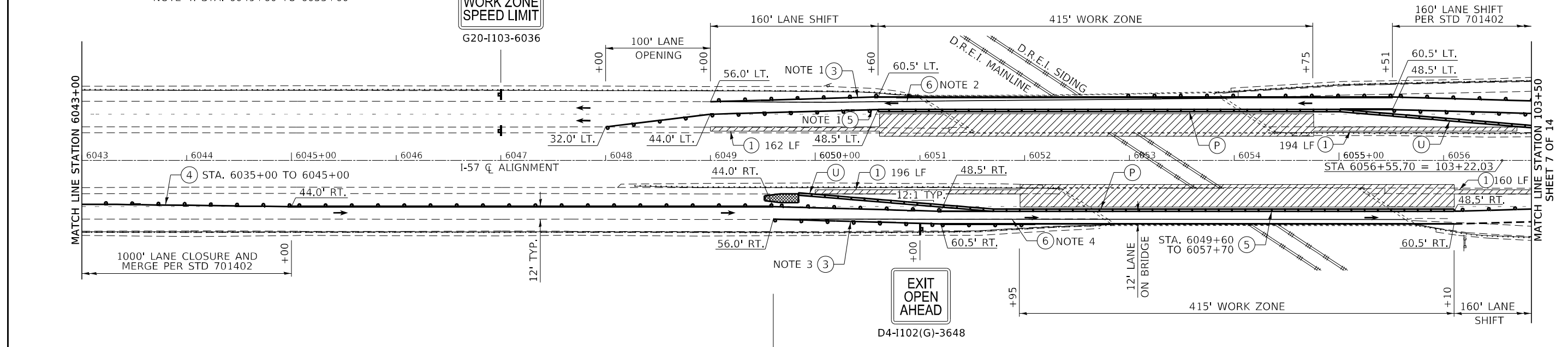
**45 MPH SIGN ASSEMBLY**

<b>WORK ZONE</b>	W2-ILL5(0)-3618
<b>SPEED LIMIT</b>	R2-I-3648
<b>45</b>	
<b>PHOTO ENFORCED</b>	R10-I108P-3618
<b>\$XXX FINE MINIMUM</b>	R2-I106P-3618



NOTE 1: STA. 6049+00 TO 6057+11  
 NOTE 2: STA. 6049+00 TO 6054+35  
 NOTE 3: STA. 6049+60 TO 6056+10  
 NOTE 4: STA. 6049+60 TO 6055+60

**END WORK ZONE SPEED LIMIT**  
 G20-I103-6036



**EXIT OPEN AHEAD**  
 D4-I102(G)-3648

**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	P - PINNED OR U - UN-PINNED
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	TRAFFIC CONTROL SIGN
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	ARROW BOARD
	CRASH ATTENUATOR
	DIRECTION OF TRAFFIC
	TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761		CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959		DRAWN - D.M.F.	REVISED -
		CHECKED - S.W.M.	REVISED -

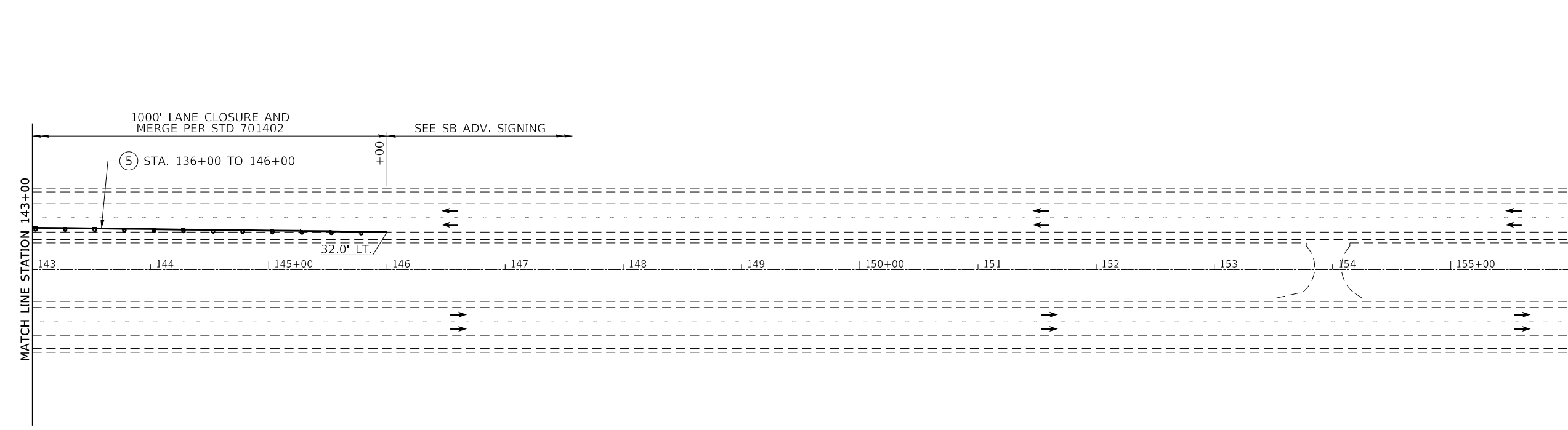
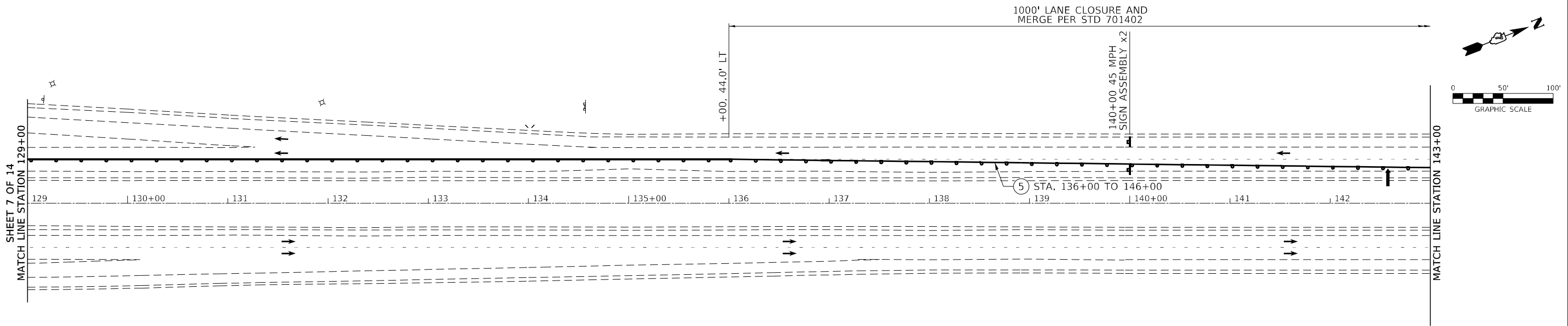
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**STAGE 1  
 MAINTENANCE OF TRAFFIC**

SHEET NO. 6 OF 14 SHEETS

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	16
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				





**45 MPH SIGN ASSEMBLY**

<b>WORK ZONE</b>	W2-ILL5(0)-3618
<b>SPEED LIMIT 45</b>	R2-I-3648
<b>PHOTO ENFORCED</b>	R10-I108P-3618
<b>\$\$\$ FINE MINIMUM</b>	R2-I106P-3618

**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP		WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP		TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)		CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")		TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)		ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)		CRASH ATTENUATOR
		DIRECTION OF TRAFFIC
		TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959		CHECKED - S.W.M.	REVISED -
	PLOT SCALE =	DRAWN - D.M.F.	REVISED -
	PLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -

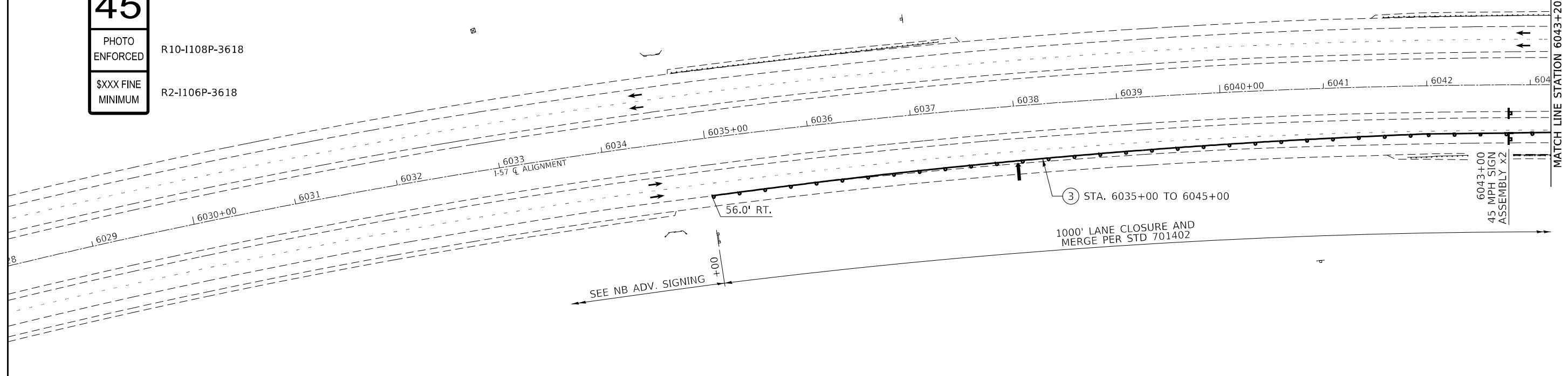
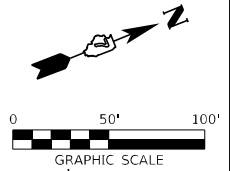
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE 1  
MAINTENANCE OF TRAFFIC**

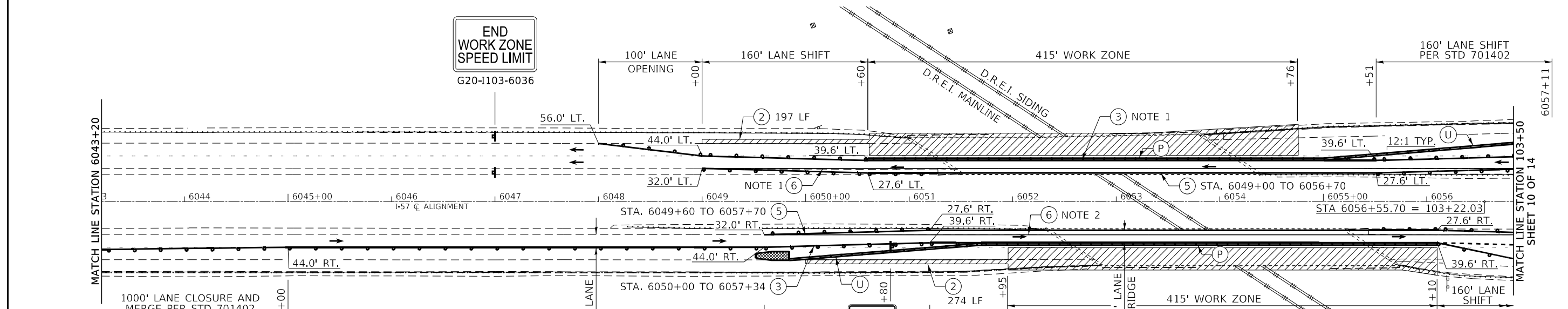
F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	18
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				

**45 MPH SIGN ASSEMBLY**

<b>WORK ZONE</b>	W2-ILL5(0)-3618
<b>SPEED LIMIT</b>	R2-I-3648
<b>45</b>	
<b>PHOTO ENFORCED</b>	R10-1108P-3618
<b>\$XXX FINE MINIMUM</b>	R2-1106P-3618



NOTE 1: STA. 6049+00 TO 6057+11  
 NOTE 2: STA. 6049+60 TO 6057+70



**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	P - PINNED OR U - UN-PINNED
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	TRAFFIC CONTROL SIGN
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	ARROW BOARD
	CRASH ATTENUATOR
	DIRECTION OF TRAFFIC
	TRAILER MOUNTED SIGN

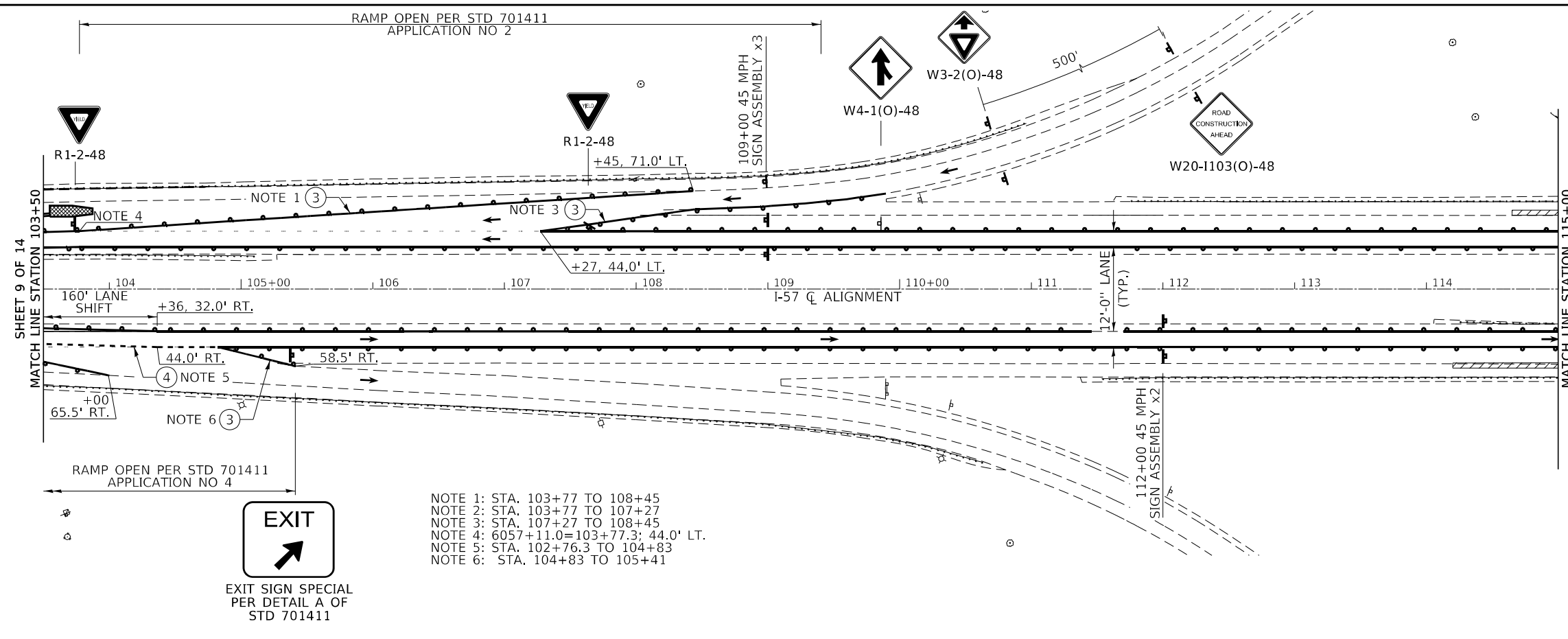
FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.00959	PLOT DATE = 8/29/2023	DRAWN - D.M.F.	REVISED -
		CHECKED - S.W.M.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

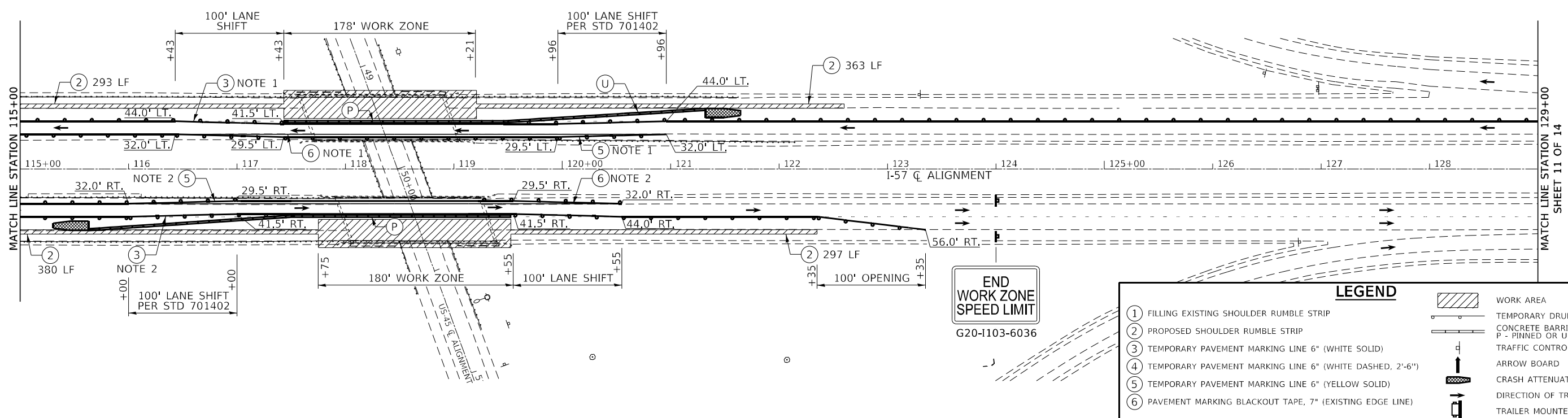
**STAGE 2  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	19
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				

SHEET NO. 9 OF 14 SHEETS

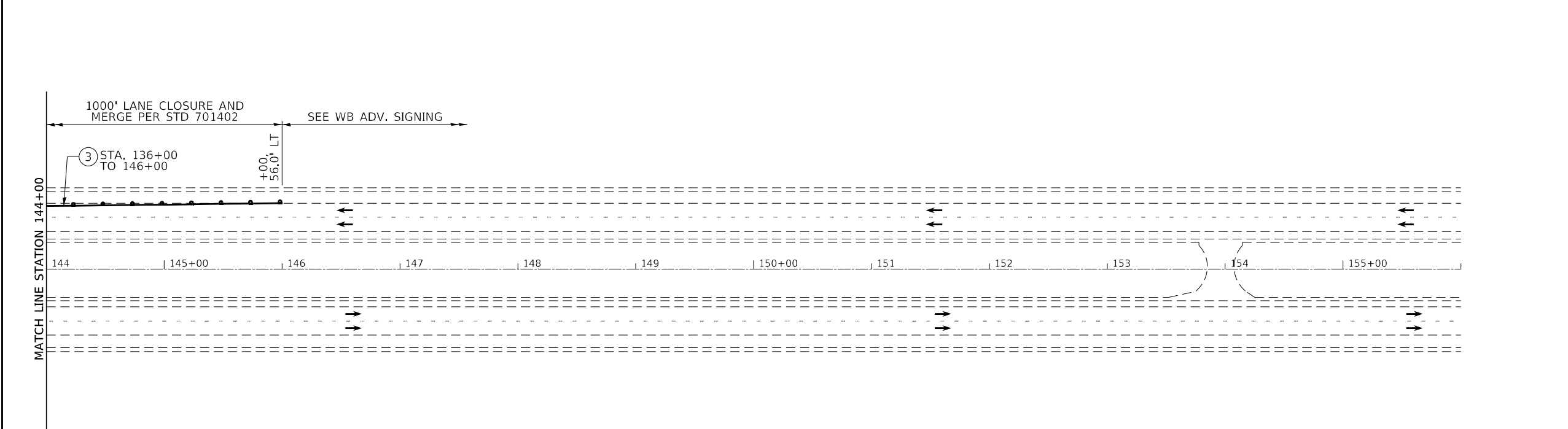
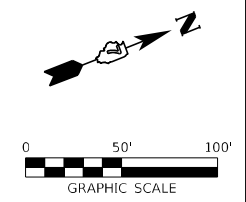
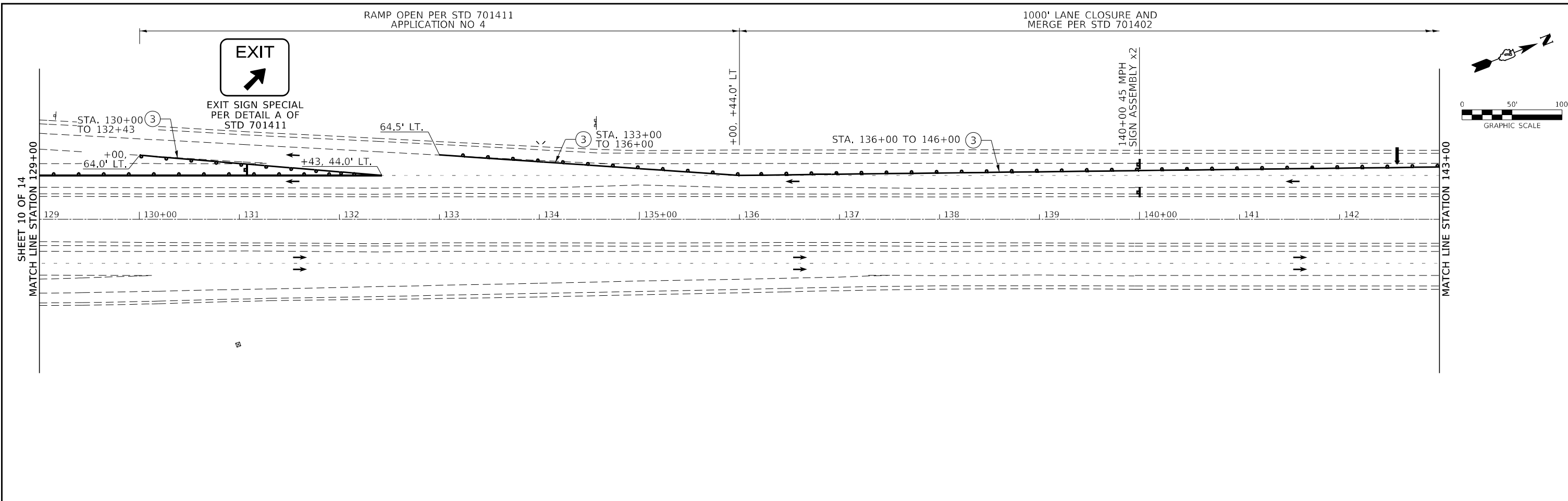


NOTE 1: STA. 116+43 TO 120+96  
 NOTE 2: STA. 116+00 TO 120+55



FILE NAME = 230269-esht-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STAGE 2 MAINTENANCE OF TRAFFIC</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.009959	PLOT SCALE =	CHECKED - S.W.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	20
PLOT DATE = 8/29/2023	DRAWN - D.M.F.	CHECKED - S.W.M.	REVISED -			CONTRACT NO. 74C65				
						ILLINOIS FED. AID PROJECT				





**45 MPH SIGN ASSEMBLY**

<b>WORK ZONE</b>	W2-ILL5(0)-3618
<b>SPEED LIMIT 45</b>	R2-I-3648
<b>PHOTO ENFORCED</b>	R10-I108P-3618
<b>\$XXX FINE MINIMUM</b>	R2-I106P-3618

**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP		WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP		TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)		CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")		TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)		ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)		CRASH ATTENUATOR
		DIRECTION OF TRAFFIC
		TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
<b>HAMPTON, LENZINI AND RENWICK, INC.</b> 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959		CHECKED - S.W.M.	REVISED -
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	PLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -

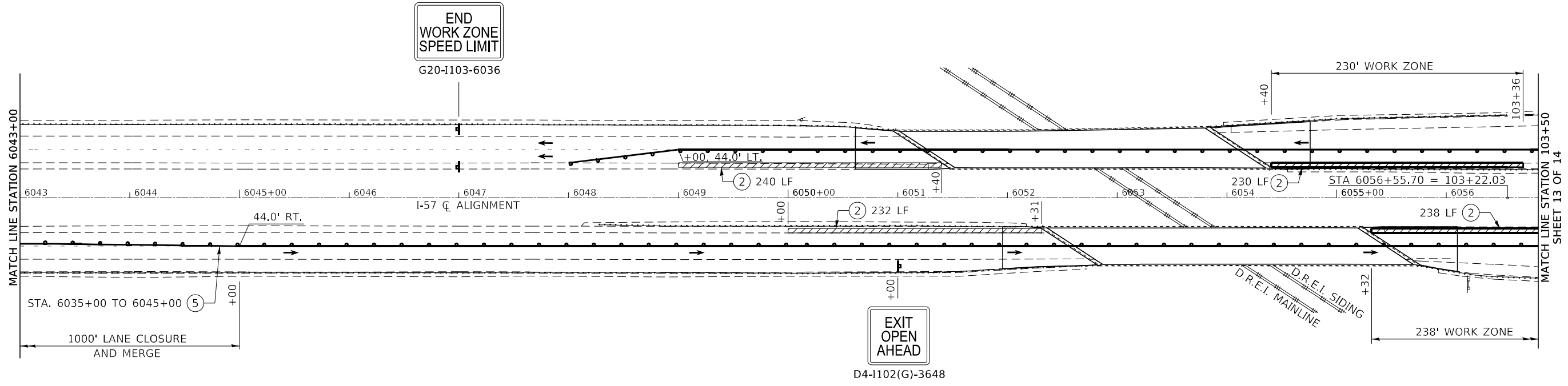
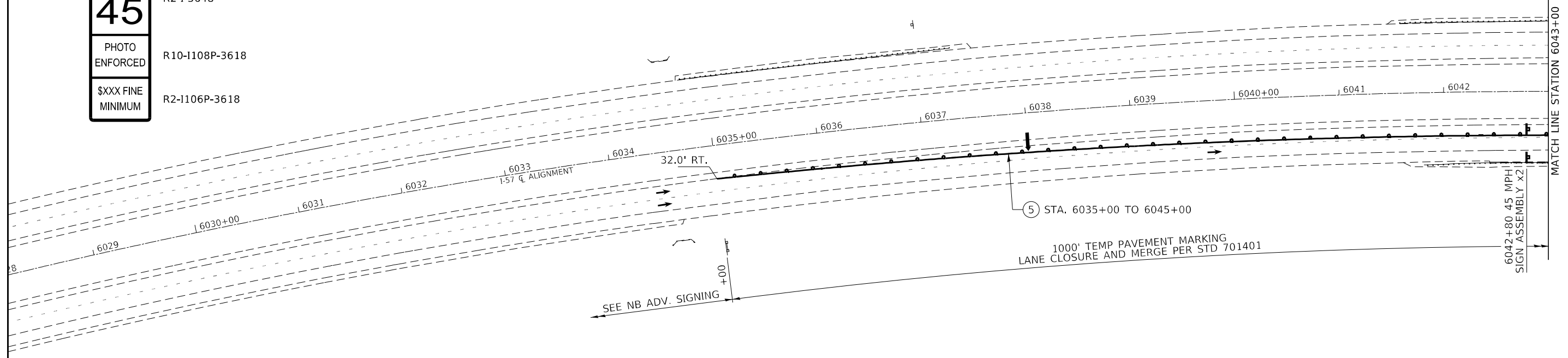
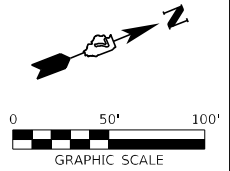
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE 2  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	21
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				

**45 MPH SIGN ASSEMBLY**

WORK ZONE	W2-ILL5(0)-3618
SPEED LIMIT	R2-I-3648
<b>45</b>	
PHOTO ENFORCED	R10-1108P-3618
\$XXX FINE MINIMUM	R2-1106P-3618



**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	CRASH ATTENUATOR
	DIRECTION OF TRAFFIC
	TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761		CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.009959	PLOT SCALE =	DRAWN - D.M.F.	REVISED -
	PLOT DATE = 8/29/2023	CHECKED - S.W.M.	REVISED -

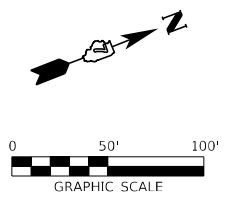
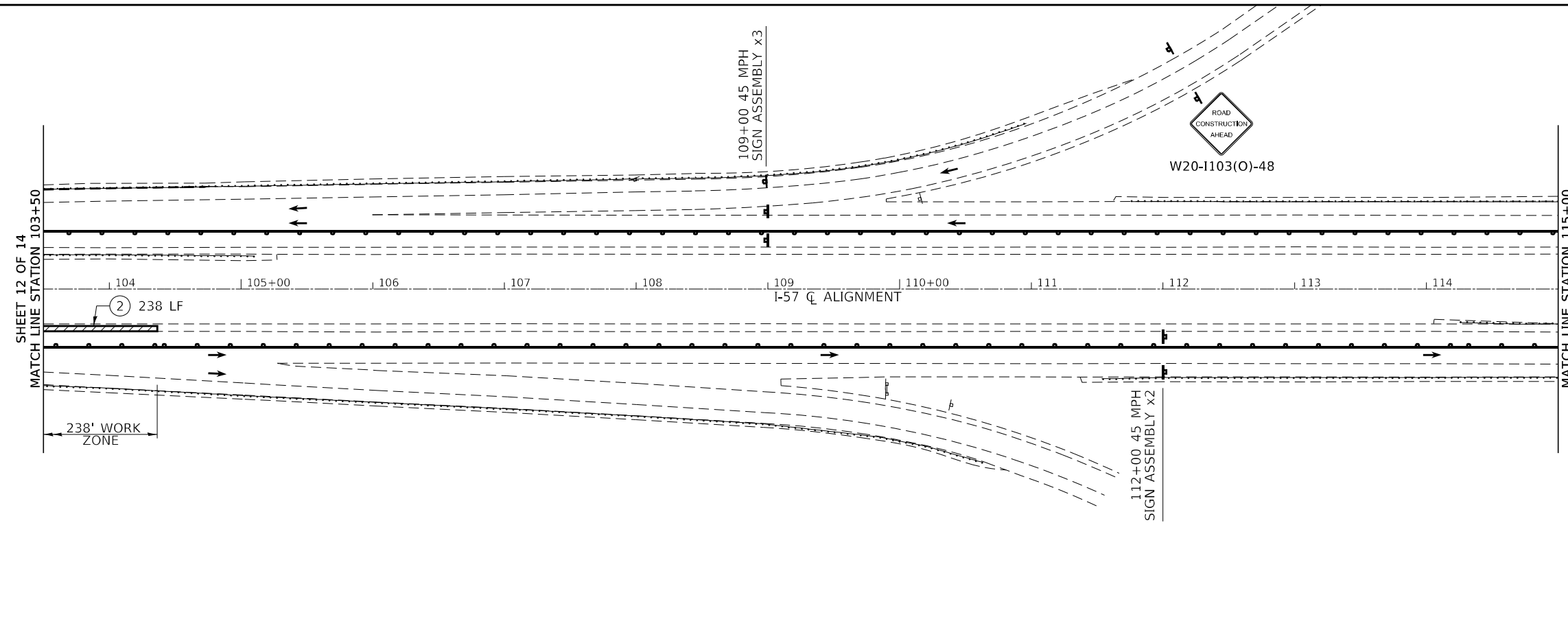
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE 3  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	22
CONTRACT NO. 74C65				

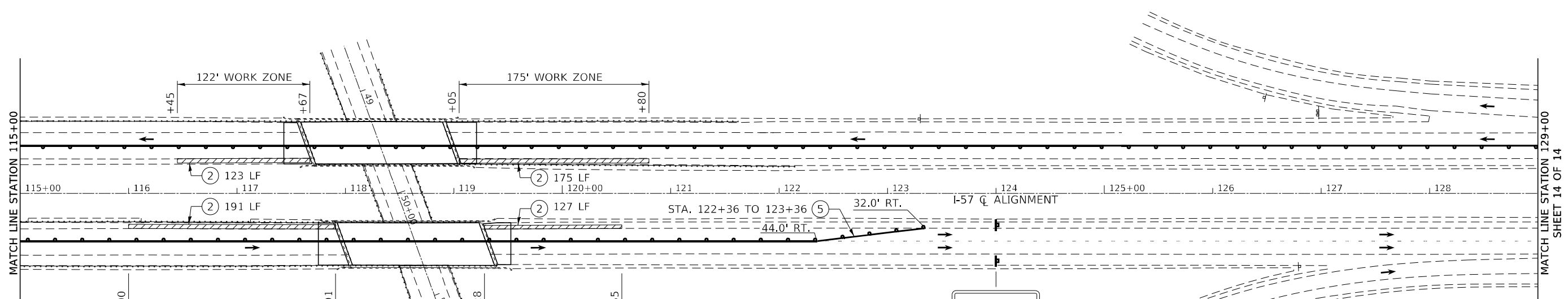
SHEET NO.12 OF 14 SHEETS

ILLINOIS FED. AID PROJECT



**45 MPH SIGN ASSEMBLY**

<b>WORK ZONE</b>	W2-ILL5(0)-3618
<b>SPEED LIMIT</b> <b>45</b>	R2-I-3648
<b>PHOTO ENFORCED</b>	R10-I108P-3618
<b>\$XXX FINE MINIMUM</b>	R2-I106P-3618



**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	CRASH ATTENUATOR
	DIRECTION OF TRAFFIC
	TRAILER MOUNTED SIGN

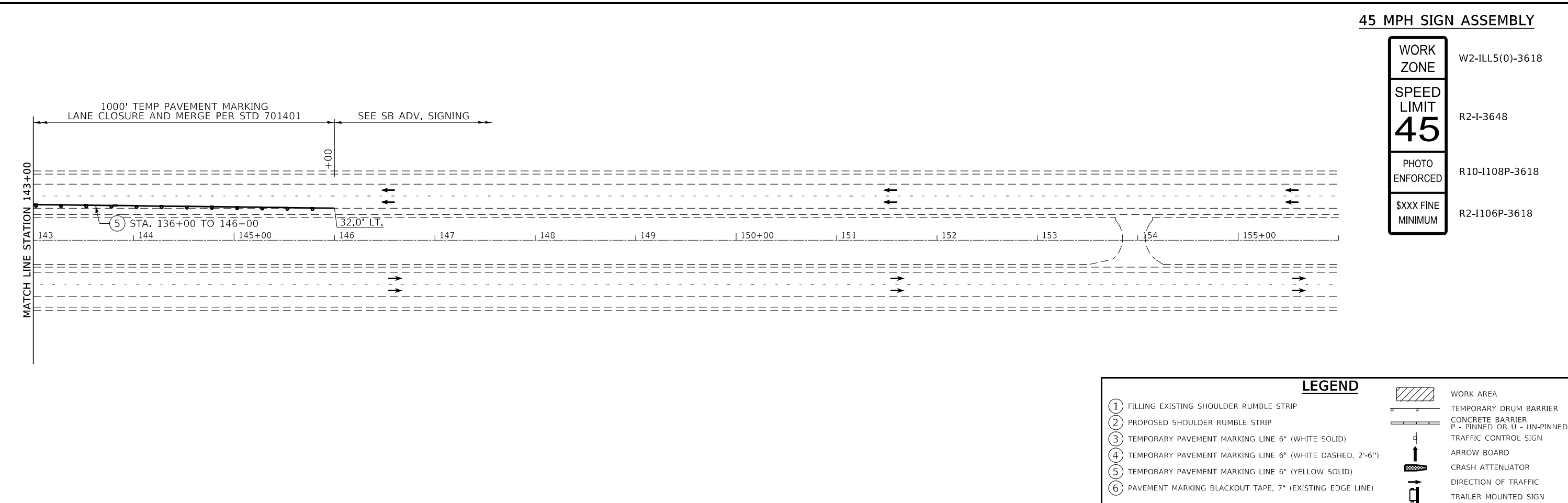
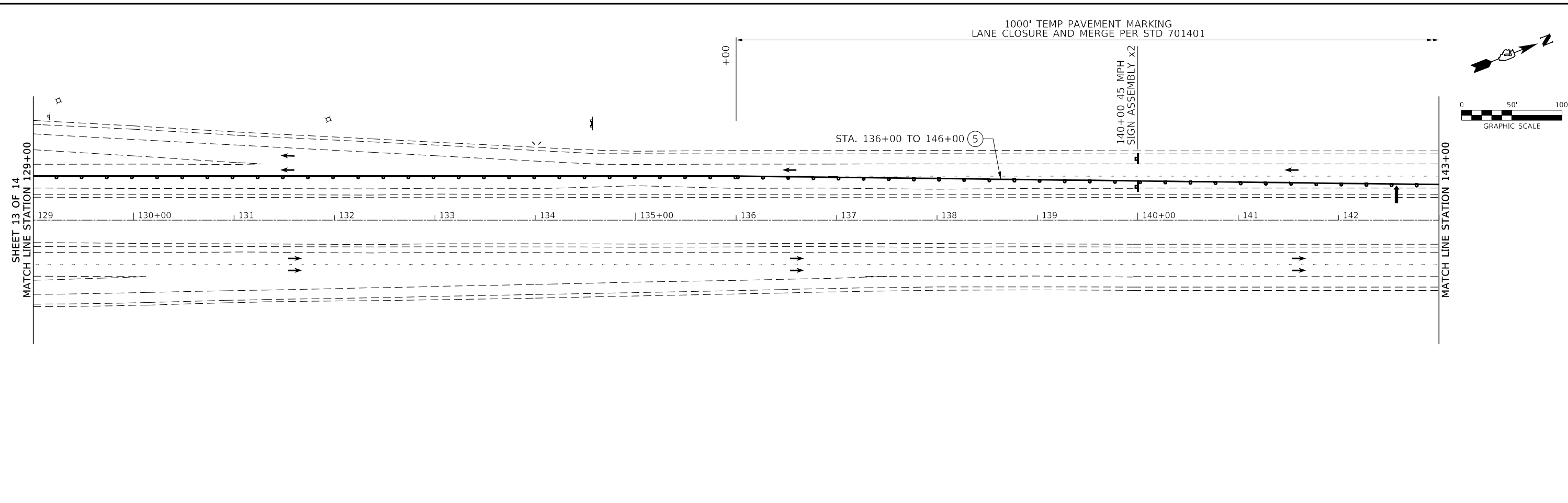
FILE NAME = 230269-shl-mo.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.W.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - D.M.F.	REVISED -
		CHECKED - S.W.M.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE 3  
MAINTENANCE OF TRAFFIC**

SHEET NO.13 OF 14 SHEETS

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	23
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				



**LEGEND**

① FILLING EXISTING SHOULDER RUMBLE STRIP	WORK AREA
② PROPOSED SHOULDER RUMBLE STRIP	TEMPORARY DRUM BARRIER
③ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE SOLID)	CONCRETE BARRIER P - PINNED OR U - UN-PINNED
④ TEMPORARY PAVEMENT MARKING LINE 6" (WHITE DASHED, 2'-6")	TRAFFIC CONTROL SIGN
⑤ TEMPORARY PAVEMENT MARKING LINE 6" (YELLOW SOLID)	ARROW BOARD
⑥ PAVEMENT MARKING BLACKOUT TAPE, 7" (EXISTING EDGE LINE)	CRASH ATTENUATOR
	TRAILER MOUNTED SIGN

FILE NAME = 230269-shl-mol.dgn	USER NAME = mlorenz	DESIGNED - M.A.L.	REVISED -
<b>HAMPTON, LENZINI AND RENWICK, INC.</b> <small>3335 STEVENSON DRIVE, SUITE 201          SPRINGFIELD, ILLINOIS 62761          ILLINOIS PROFESSIONAL DESIGN FIRM          LS / PE / SE CORP. 184.002959</small>		CHECKED - S.W.M.	REVISED -
		DRAWN - D.M.F.	REVISED -
PLOT SCALE =		CHECKED - S.W.M.	REVISED -
PLOT DATE = 8/29/2023			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE 3  
MAINTENANCE OF TRAFFIC**

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	24
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				

BENCHMARK: BM 701 - Chiseled "□" on the N.W. corner of S.N. 018-0002 (NB) bridge. Sta. 6055+20, Rt. 24', Elev. 692.845

EXISTING STRUCTURES: S.N. 018-0001 (SB) and S.N. 018-0002 (NB), Sta. 6053+21.53 (F.A.I. 57)  
 Dual Three span continuous steel I-beam structures originally built in 1963. Rehabilitated with new concrete parapets in 1987. New Hot-Mix Asphalt wearing surface in 2014.  
 29'4"-6" bk.-bk. abuts.; 36'-4" o.-o. deck Northbound.; 36'-4" to 39'-7 1/4" o.-o. deck Southbound.

Traffic maintained utilizing stage construction. Rail traffic to be maintained on both tracks without interruption.

**DESIGN SPECIFICATIONS**

2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

**LOADING HS20 & ALT. (EXIST. CONST.)**

**DESIGN STRESSES**

FIELD UNITS (NEW CONST.)

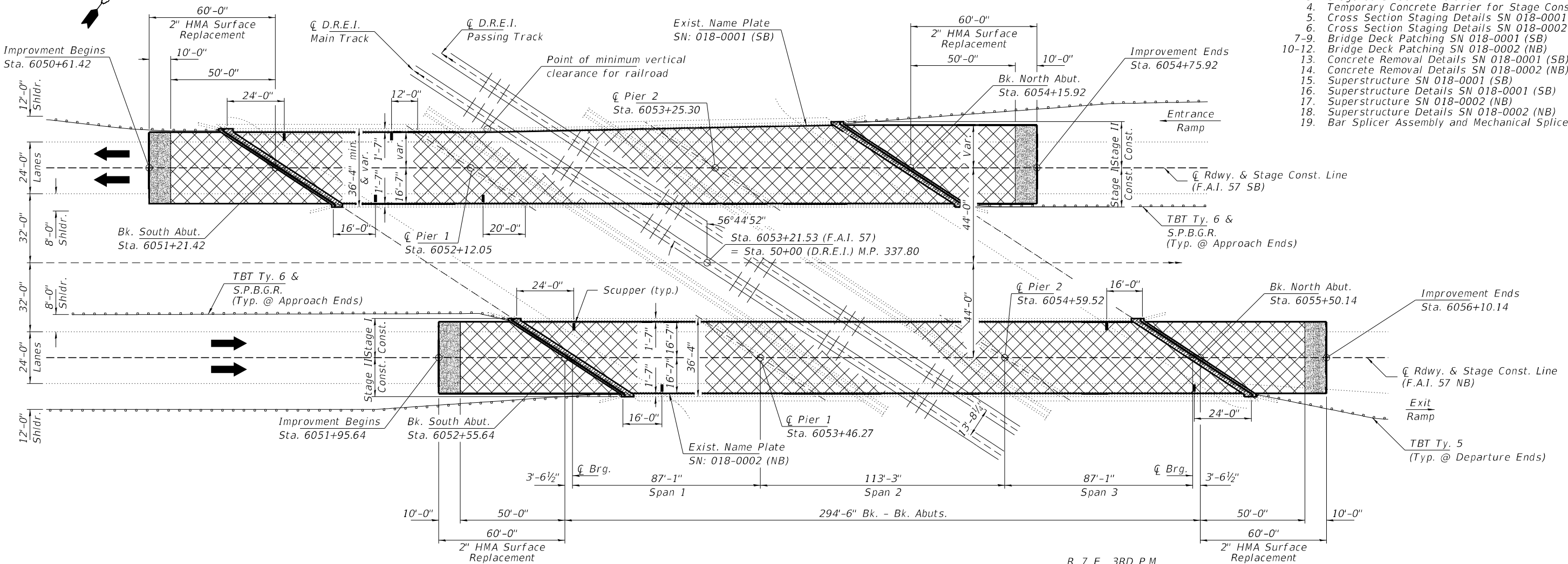
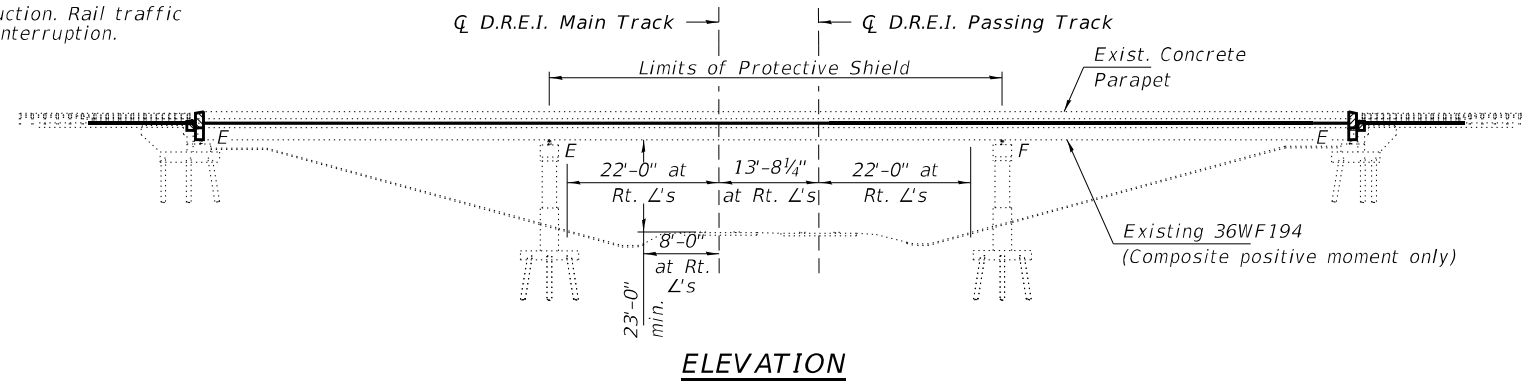
$f'_c = 4,000$  psi (Concrete)  
 $f_y = 60,000$  psi (Reinf.)

EXISTING CONSTRUCTION

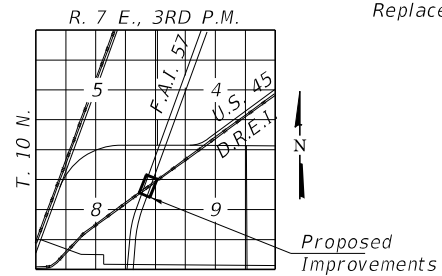
$f'_c = 1,400$  psi (Super and Sub)  
 $f_s = 20,000$  psi (Reinf.)  
 $f_s = 20,000$  psi (Str. Steel)

**INDEX OF STRUCTURE SHEETS**

1. General Plan & Elevation
2. General Data
3. Stage Construction Details
4. Temporary Concrete Barrier for Stage Construction
5. Cross Section Staging Details SN 018-0001 (SB)
6. Cross Section Staging Details SN 018-0002 (NB)
- 7-9. Bridge Deck Patching SN 018-0001 (SB)
- 10-12. Bridge Deck Patching SN 018-0002 (NB)
13. Concrete Removal Details SN 018-0001 (SB)
14. Concrete Removal Details SN 018-0002 (NB)
15. Superstructure SN 018-0001 (SB)
16. Superstructure Details SN 018-0001 (SB)
17. Superstructure SN 018-0002 (NB)
18. Superstructure Details SN 018-0002 (NB)
19. Bar Splicer Assembly and Mechanical Splicer Details

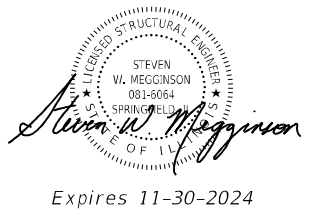


**PLAN**



**LOCATION SKETCH**

- Hatching indicates HMA Surface Removal - Butt Joint
- Hatching indicates 2" HMA Surface Removal & Replacement, or 2" HMA Surface Removal & Replacement (Deck)
- Hatching indicates Concrete Removal



**GENERAL PLAN & ELEVATION**  
**F.A.I. 57 OVER D.R.E.I.**  
**D7 BRIDGE REPAIRS 2023-9**  
**CUMBERLAND COUNTY**  
**STATION 6053+21.53**  
**STRUCTURE NO. 018-0001 (SB)**  
**& STRUCTURE NO. 018-0002 (NB)**

FILE NAME = 230269-12-shl-bridge.dgn	USER NAME = gmetcalf	DESIGNED - S.T.M.	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL PLAN &amp; ELEVATION I-57 OVER D.R.E.I. - SN 018-0001 (SB) SN 018-0002 (NB)</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62703 ILLINOIS PROFESSIONAL DESIGN FIRM L3 / P1 / SE CORP. 164.002959	PLOT SCALE =	CHECKED - S.T.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	25	
PLOT DATE = 9/28/2023	DRAWN - G.D.M.	CHECKED - S.T.M./S.M.S.	REVISED -			CONTRACT NO. 74C65					
						ILLINOIS FED. AID PROJECT					

**GENERAL NOTES**

No field welding is permitted except as specified in the contract documents. Reinforcement bars designated (E) shall be epoxy coated. Prior to pouring the new concrete deck and diaphragms, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete (SSPC - SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4" deep shall be identified and reported to the Bureau of Bridges & Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications. Joint openings shall be adjusted according to article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F. Plan dimensions and details relative to the existing structure have been taken from the existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work. If the existing name plate falls within the area of Concrete Removal it shall be removed, cleaned, and reattached to the new concrete at the same location. Cost included with Concrete Removal. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to address the presence of lead on this project. Protective Coat shall be applied to the areas of exposed Concrete Superstructure consisting of the front face and fascia of diaphragms, new portion of parapets and curbs, top of hatch block and deck replacement and any other portion of Concrete Superstructure that is not covered by HMA surface course. Areas of deck slab repair shown are estimated. The Engineer shall show actual locations of deck slab repairs on as-built plans. Full depth deck slab repairs performed in the exterior bays of the bridge deck (between the parapet walls and the first interior beams) shall be limited to individual lengths no greater than 10'. In these portions of the deck, repair areas longer than 10' shall be divided into segments not greater than 10' in length, and the segments shall be poured in alternating sequence. Subsequent segments repaired in sequence shall not be removed until 72 hours shall have elapsed from the end of the previous, adjacent pour, and the adjacent pour shall have attained a minimum modulus of rupture of 650psi. Removal and reinstallation of guardrail sections will be necessary for removal of the existing expansions joints and reconstruction of improvements. The existing guardrail sections shall be stored, reused, and reattached. Any embedded anchors that are within the concrete removal areas shall be replaced with the approved epoxy grouted anchor bolts with standard washers. This work and all materials shall be included in the contract unit price for Concrete Superstructure. Traffic will be maintained using stage construction.

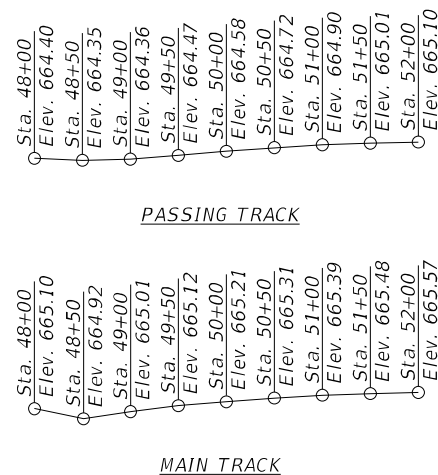
**SCOPE OF WORK**

1. Removal of existing Neoprene Expansion Joints at abutments.
2. Removal of existing HMA deck surfacing and approach surface.
3. Full and partial depth deck patching repair.
4. Structural concrete repair at abutments and superstructure.
5. Resurfacing of approaches and deck.
6. Apply Protective Coat on all new exposed Concrete Superstructure.

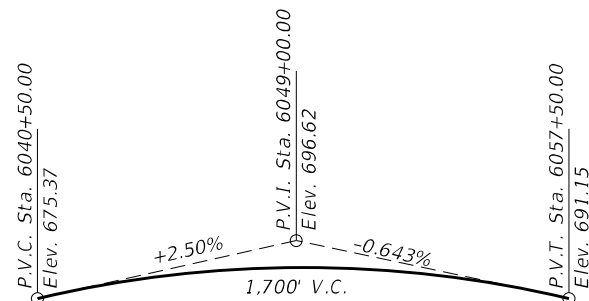
STATION 6053+21.53  
 BUILT 1963  
 STATE OF ILLINOIS  
 F.A.I. RT. 57 SEC. 18-1VB  
 F.A. PROJ. IG-57-4(27)  
 LOADING H20-S16 & ALT.

**EXISTING NAME PLATE**

Existing Name Plate shall be cleaned and reinstalled if required to be removed. Cost included with Concrete Removal.



**TOP OF RAIL ELEVATIONS  
 DECATUR & EASTERN ILLINOIS RAILROAD**

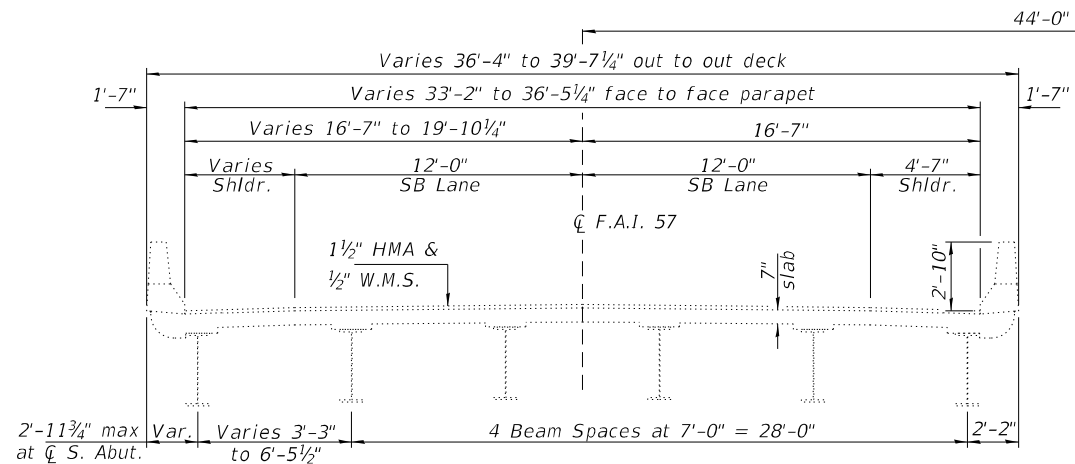


Note:  
 P.G. elevations shown are approximate and are provided as information only. Proposed work shall be constructed to match existing elevations in the field.

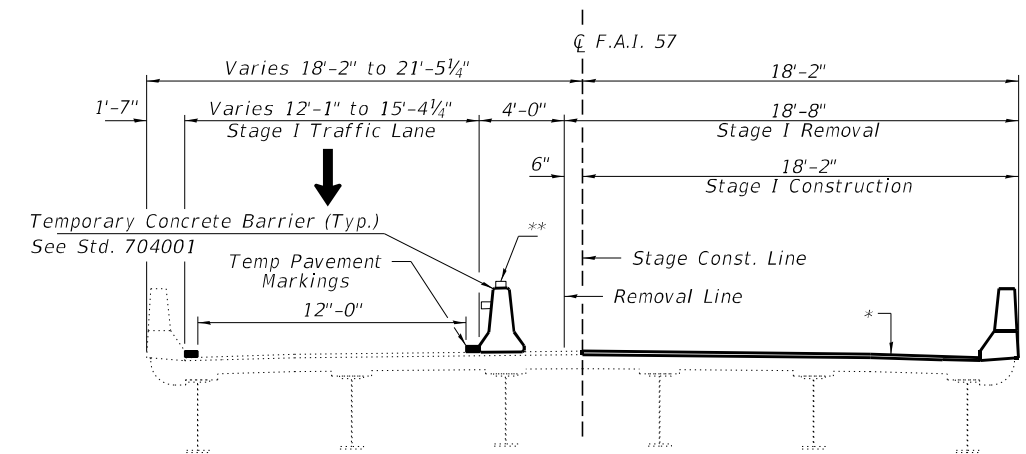
**BILL OF MATERIAL**

Item	Unit	Quantity
Bituminous Materials (Tack Coat)	Pound	1,363
Hot-Mix Asphalt Surface Removal - Butt Joint	Sq. Yd.	151
Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90	Ton	339
Hot-Mix Asphalt Surface Removal, 2"	Sq. Yd.	766
Concrete Removal	Cu. Yd.	34.6
Protective Shield	Sq. Yd.	935
Concrete Superstructure	Cu. Yd.	103.8
Protective Coat	Sq. Yd.	215
Reinforcement Bars, Epoxy Coated	Pound	7,370
Bar Splicers	Each	72
Structural Steel Removal	Pound	8,980
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	2,201
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	57
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	81
Deck Slab Repair (Partial)	Sq. Yd.	140

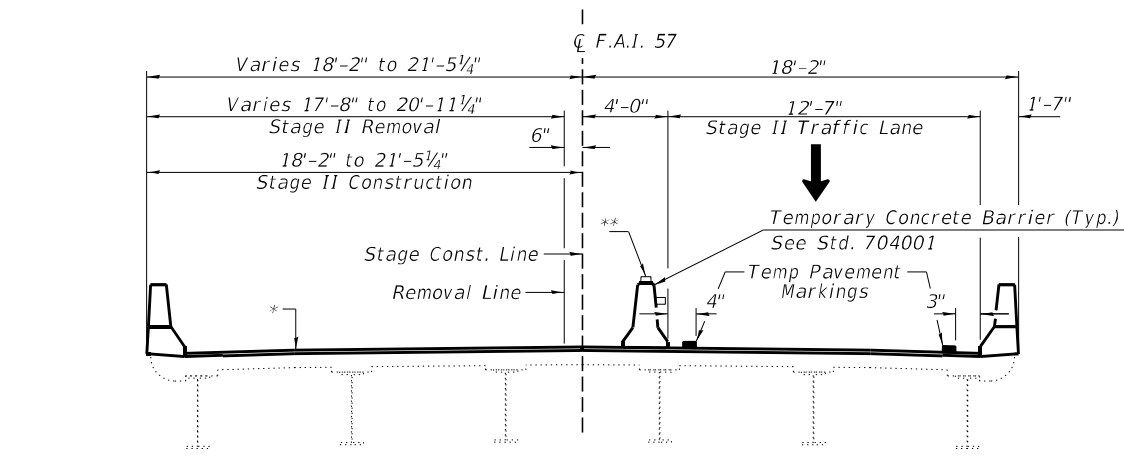
☐ Alignment (F.A.I. 57)



**EXISTING CROSS SECTION**  
(Looking North)

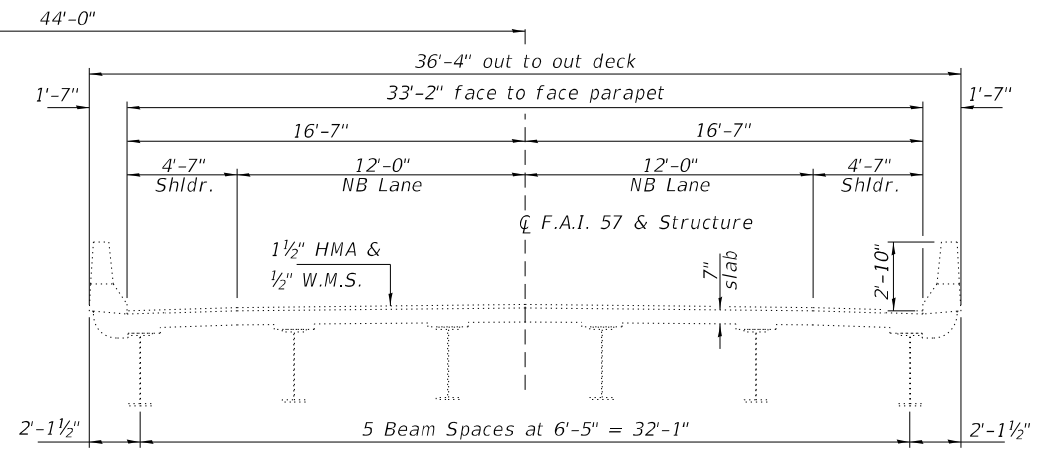


**STAGE I CONSTRUCTION**  
(Looking North)

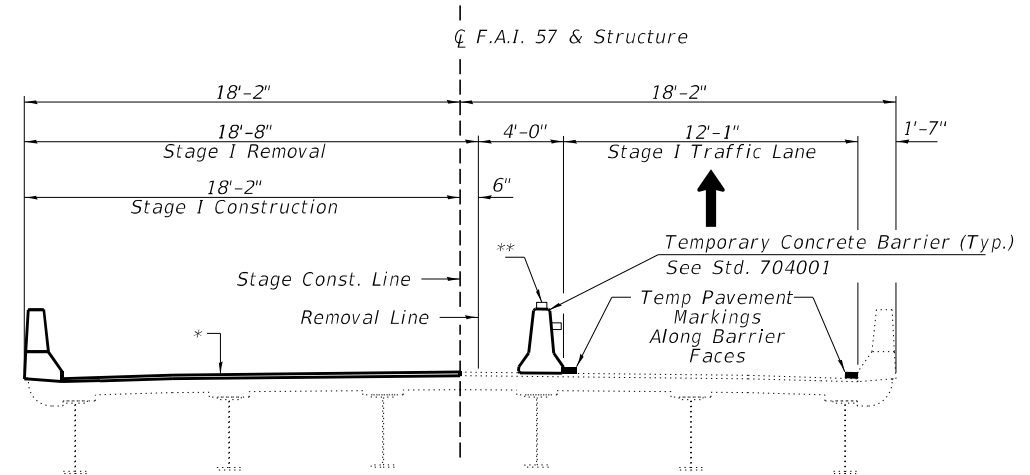


**STAGE II CONSTRUCTION**  
(Looking North)

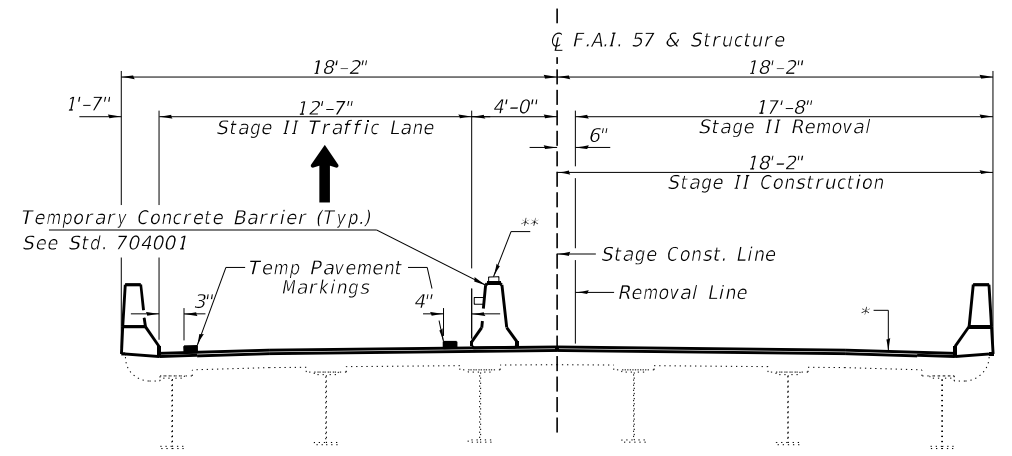
**S.N. 018-0001 SOUTH BOUND LANES**



**EXISTING CROSS SECTION**  
(Looking North)



**STAGE I CONSTRUCTION**  
(Looking North)



**STAGE II CONSTRUCTION**  
(Looking North)

**S.N. 018-0002 NORTH BOUND LANES**

\*2" Hot-Mix Asphalt Surface Removal (Deck)  
Replace with 2" Polymerized Hot-Mix Asphalt Surface Course,  
IL-9.5, Mix "D", N90.

\*\*TCB with Type C reflectors as noted in Standard 704001  
and 782006

Notes:  
See Roadway plans for Maintenance of Traffic Details.  
See Sheet 4 of 19 for details of Temporary Concrete Barrier.  
See Structure Sheets 13 thru 18 of 19 for Concrete Removal  
and Proposed Construction details.

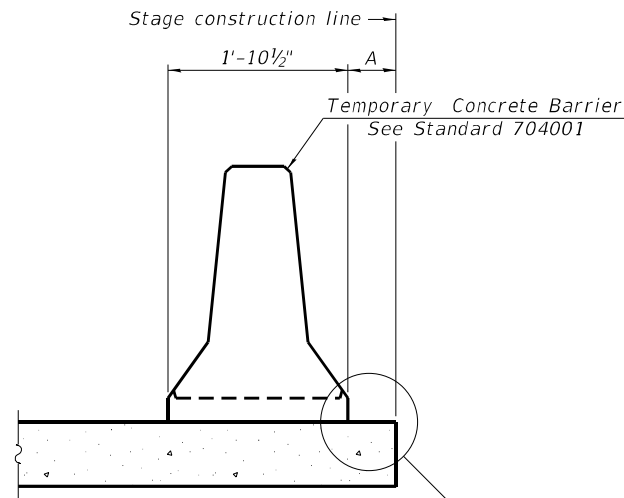
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HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.T.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.009959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISED -
		CHECKED - S.T.M./S.M.S.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS  
I-57 OVER D.R.E.I. - SN 018-0001 (SB) SN 018-0002 (NB)**

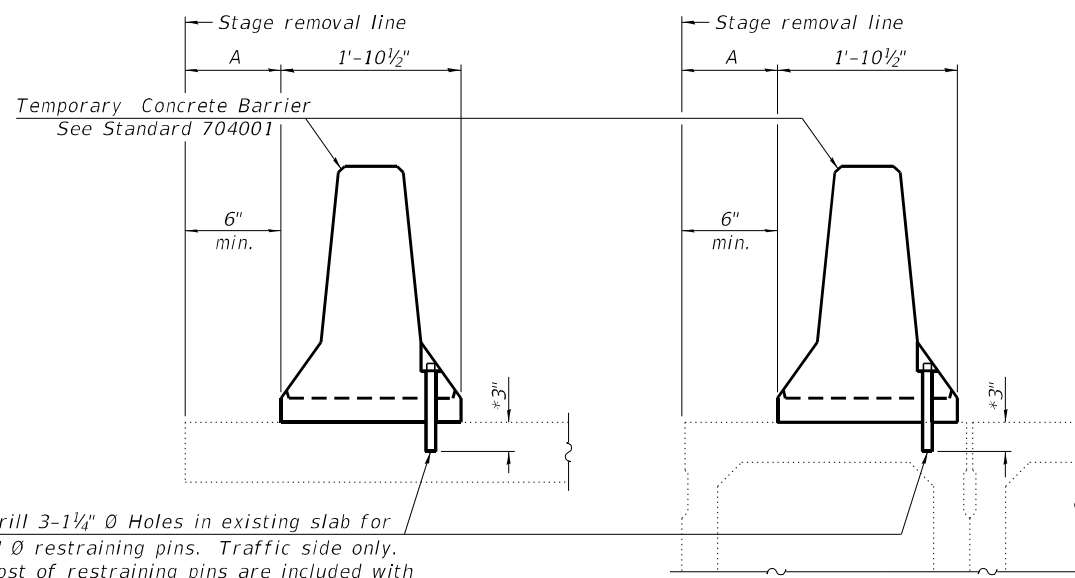
SHEET NO. 3 OF 19 SHEETS

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	27
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



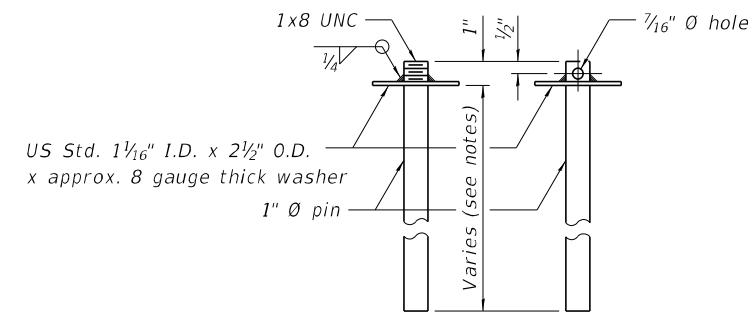
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

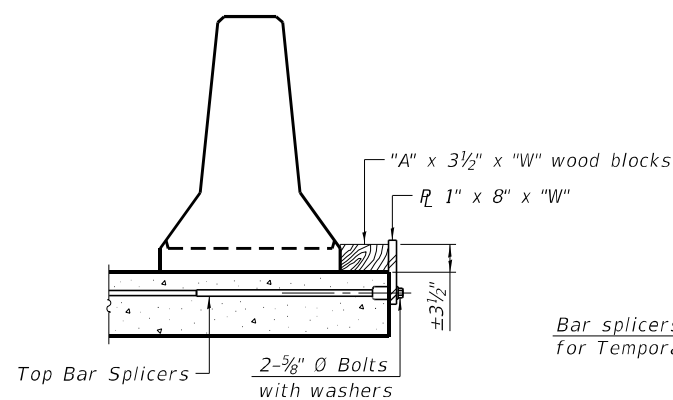
EXISTING SLAB

EXISTING DECK BEAM

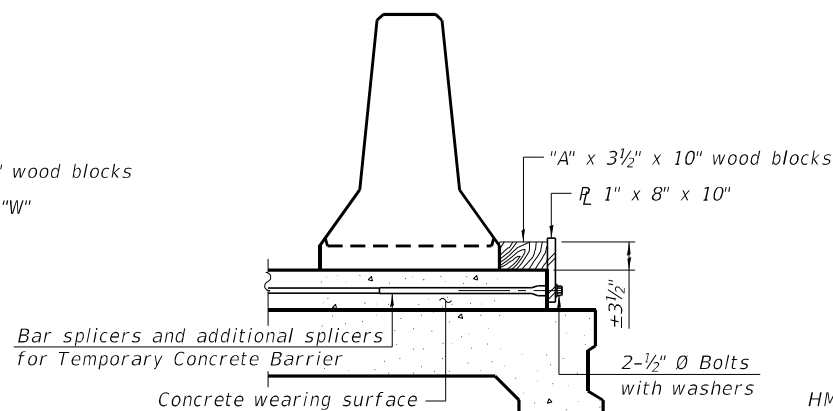
SECTIONS THRU SLAB OR DECK BEAM



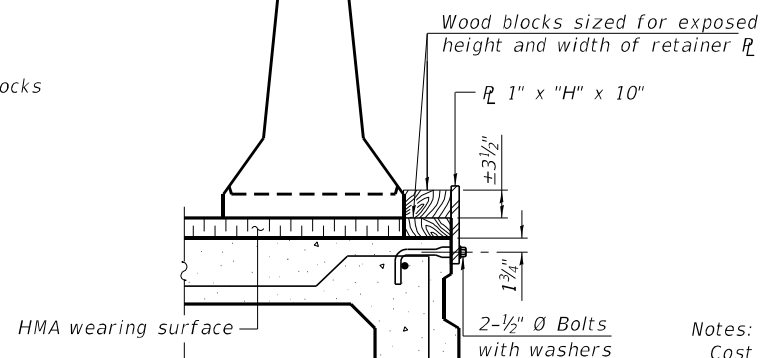
RESTRAINING PIN



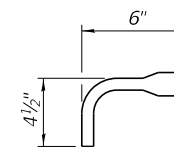
DETAIL I



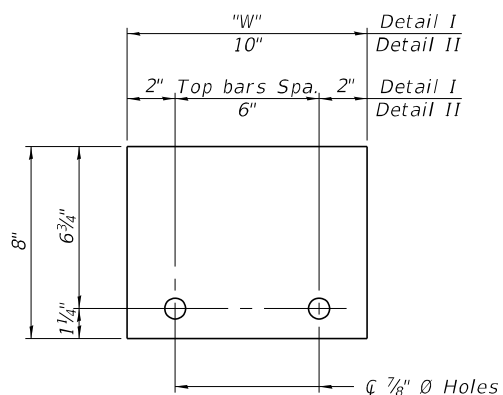
DETAIL II



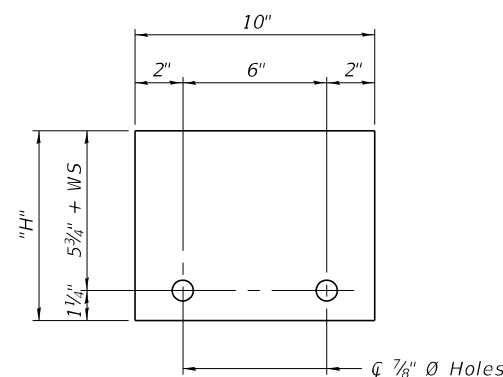
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"  
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"  
(Detail III)

Notes:  
 Cost of retainer assembly is included with Temporary Concrete Barrier.  
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.  
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.  
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.  
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

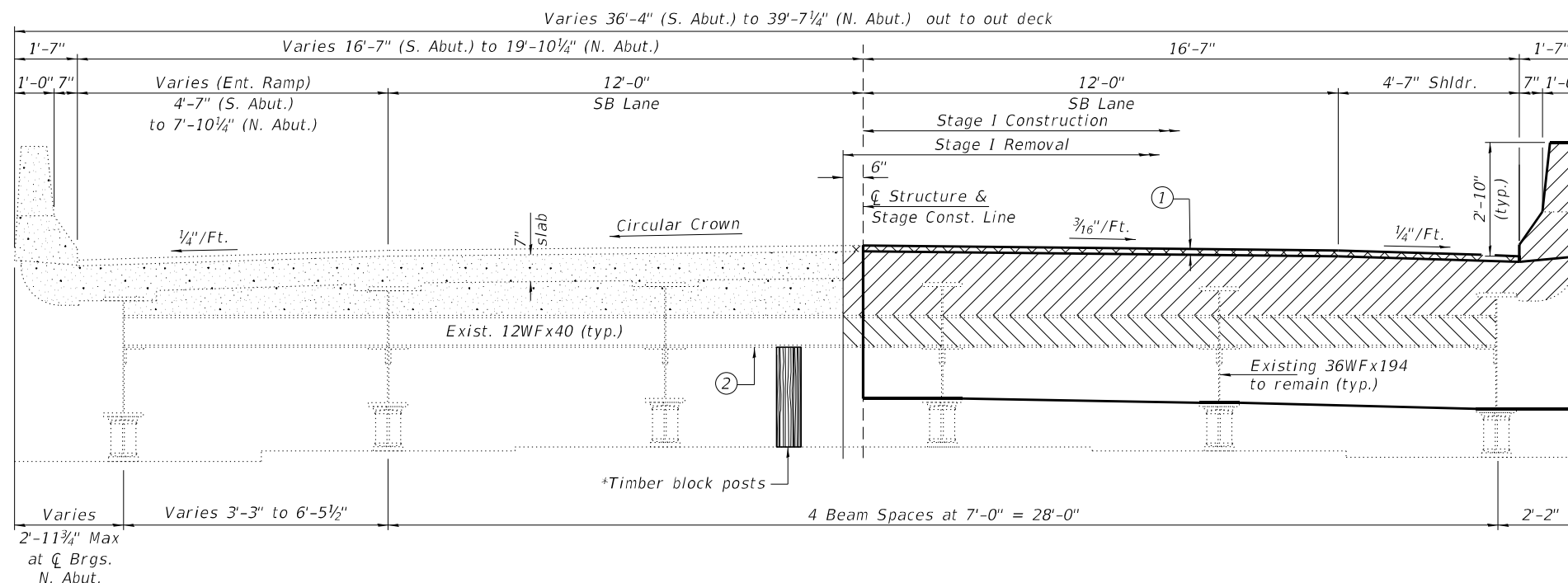
RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021

FILE NAME = 230269-1-2-shl-bridge.dgn	USER NAME = mlkrentz	DESIGNED - S.T.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION I-57 OVER D.R.E.I. - SN 018-0001 (SB) SN 018-0002 (NB)	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.T.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	28	
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISED -			CONTRACT NO. 74C65					
		CHECKED - S.T.M./S.M.S.	REVISED -			SHEET NO. 4 OF 19 SHEETS					

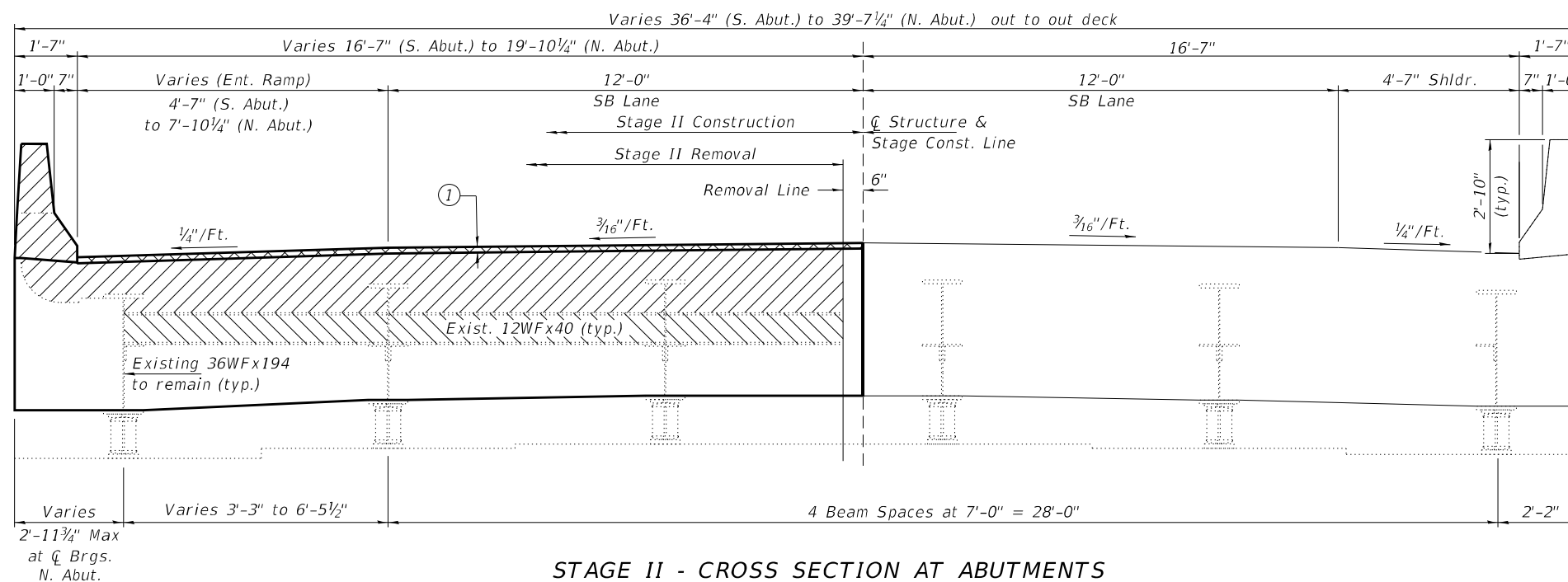




**STAGE I - CROSS SECTION AT ABUTMENTS**




(Looking North)

\*Cost included with Structural Steel Removal

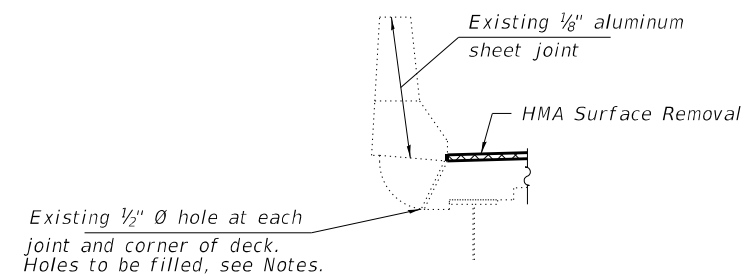


**STAGE II - CROSS SECTION AT ABUTMENTS**

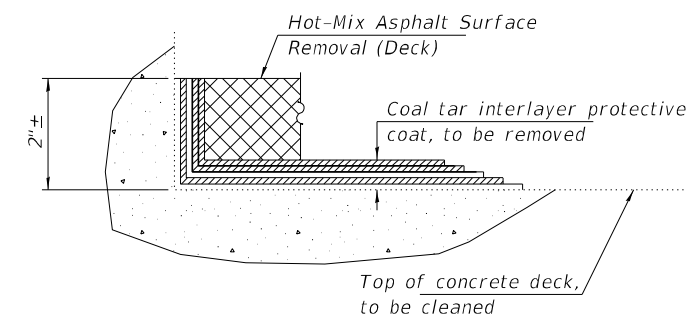
(Looking North)

-  Structural Steel Removal
-  Hatching indicates Concrete Removal
-  Hatching indicates 2" HMA Surface Removal or 2" HMA Surface Removal (Deck)

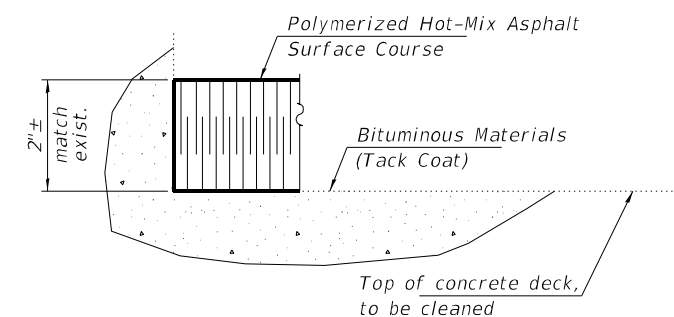
- ① 2" Hot-Mix Asphalt Surface Removal (Deck)  
2" Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90 (Throughout Deck) or Concrete Superstructure at Joint Replacement.
- ② Portion of 12WFx40 end diaphragm to be temporarily supported and remain in place until Stage II removal.



**DETAIL AT ALUMINUM PARAPET JOINTS**



**EXISTING DECK SURFACING**



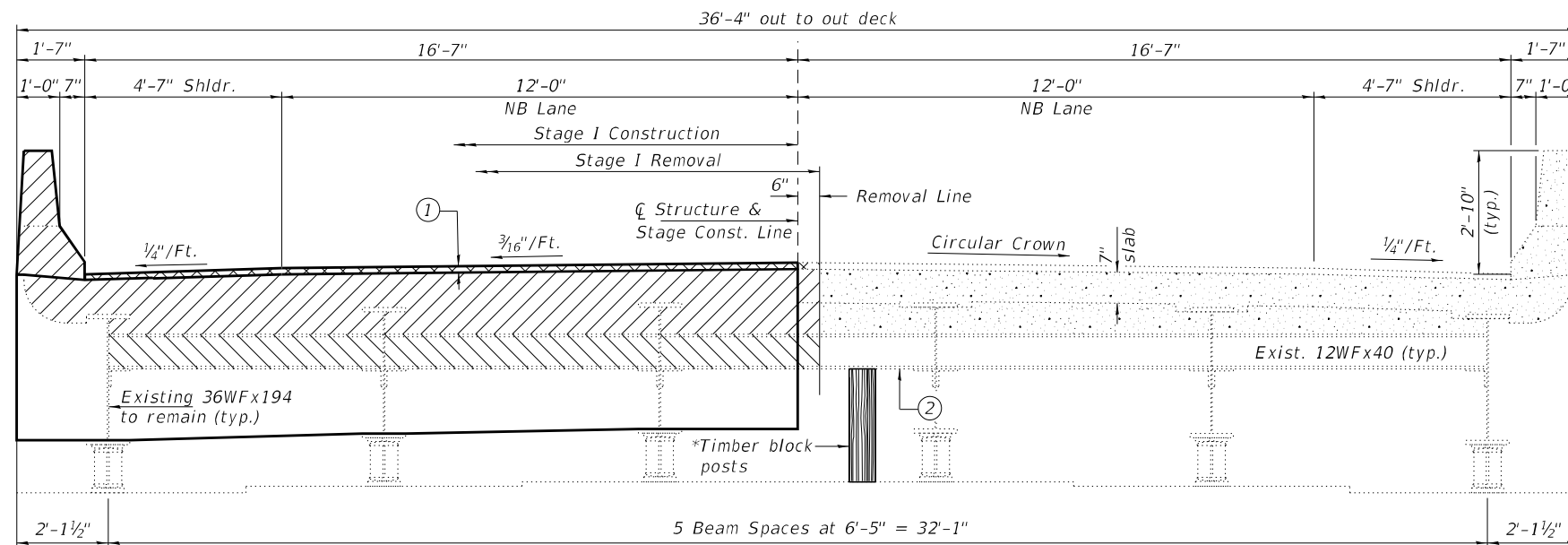
**PROPOSED DECK SURFACING**

**BILL OF MATERIAL**

Item	Unit	Quantity
Bituminous Materials (Tack Coat)	Pound	695
Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90	Ton	173
Structural Steel Removal	Pound	4,545
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	1,116

**Notes:**

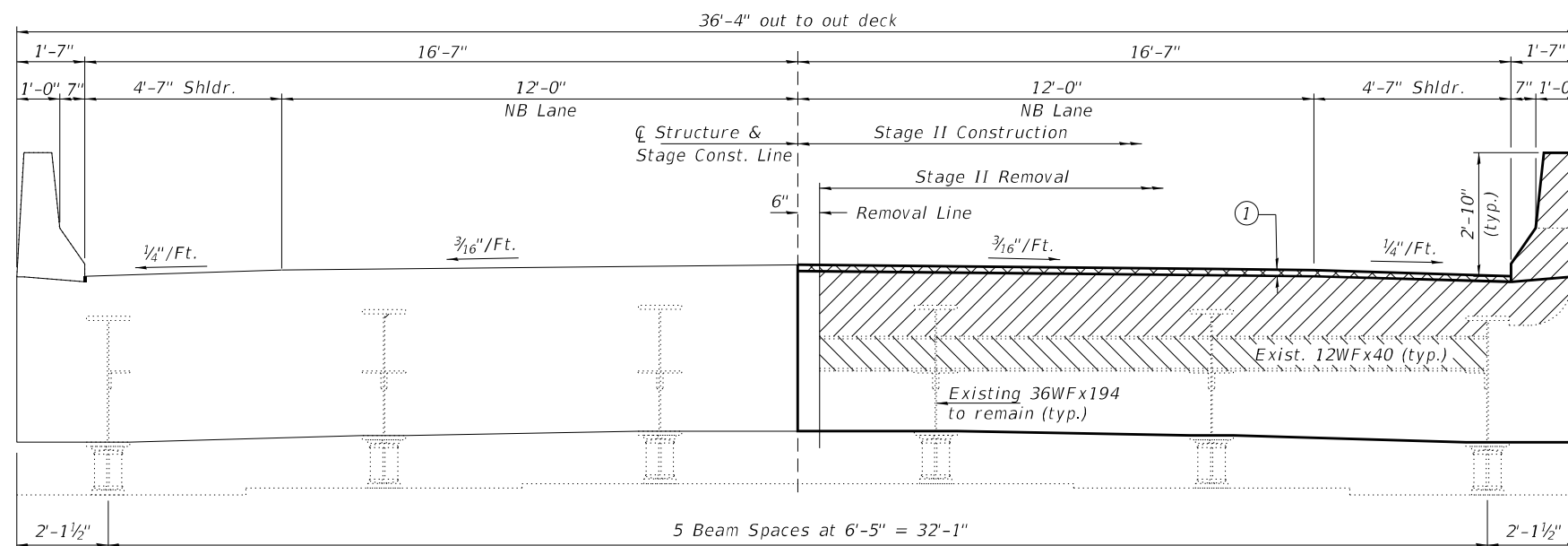
A tack coat, according to Section 406 of the Standard Specifications, with a residual asphalt rate of 0.05 lb./sq. ft. shall be applied to the top of the concrete deck before the hot-mix asphalt surfacing is placed.  
 Prior to applying the tack coat, the top of the existing concrete deck shall be cleaned according to Article 406.05(b)(1) of the Standard Specifications.  
 The quantity for Polymerized Hot-Mix Asphalt Surface Course is calculated based on 2" uniform thickness and a placement factor of 112 lb./sq. yd./inch.  
 All existing 1/2" diameter drain holes at each Aluminum Sheeted Construction Joint and in each corner of the deck shall be filled with polyurethane sealant prior to application of the tack coat. The polyurethane sealant shall be according to Article 1050.04 of the Standard Specifications and the color shall be gray. Cost included with Hot-Mix Asphalt Surface Removal (Deck).  
 All existing end diaphragms shall be removed. Diaphragm connection angles shall remain. End diaphragms at stage removal line shall be temporarily supported and removed in stages.  
 See Sheet 13 of 19 for Concrete Removal details.  
 See Sheets 15 & 16 of 19 for proposed concrete diaphragm, parapet, and deck construction.



**STAGE I - CROSS SECTION AT ABUTMENTS**

(Looking North)

\*Cost included with Structural Steel Removal



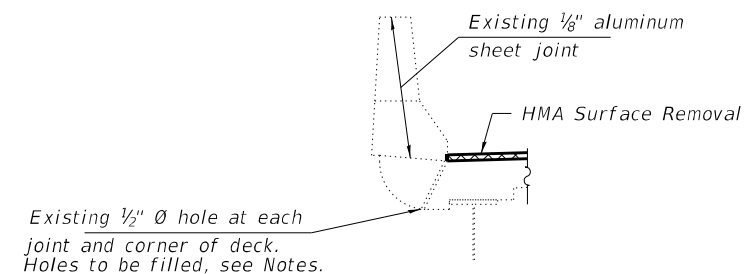
**STAGE II - CROSS SECTION AT ABUTMENTS**

(Looking North)

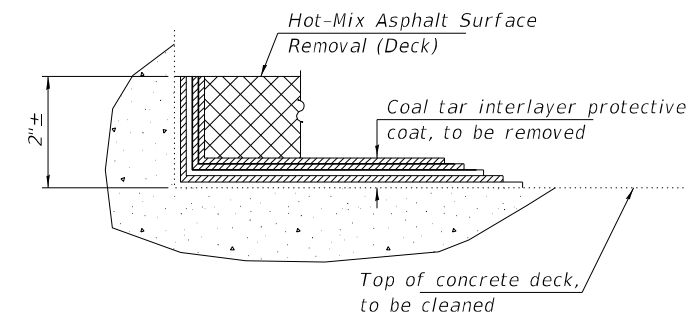
\*Cost included with Structural Steel Removal

- Structural Steel Removal
- Hatching indicates Concrete Removal
- Hatching indicates 2" HMA Surface Removal or 2" HMA Surface Removal (Deck)

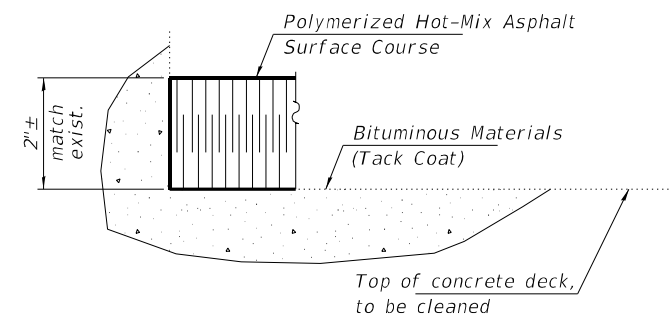
- ① 2" Hot-Mix Asphalt Surface Removal (Deck)  
2" Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90 (Throughout Deck) or Concrete Superstructure at Joint Replacement.
- ② Portion of 12WFx40 end diaphragm to be temporarily supported and remain in place until Stage II removal.



**DETAIL AT ALUMINUM PARAPET JOINTS**



**EXISTING DECK SURFACING**



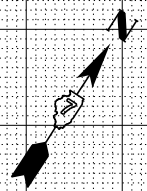
**PROPOSED DECK SURFACING**

**BILL OF MATERIAL**

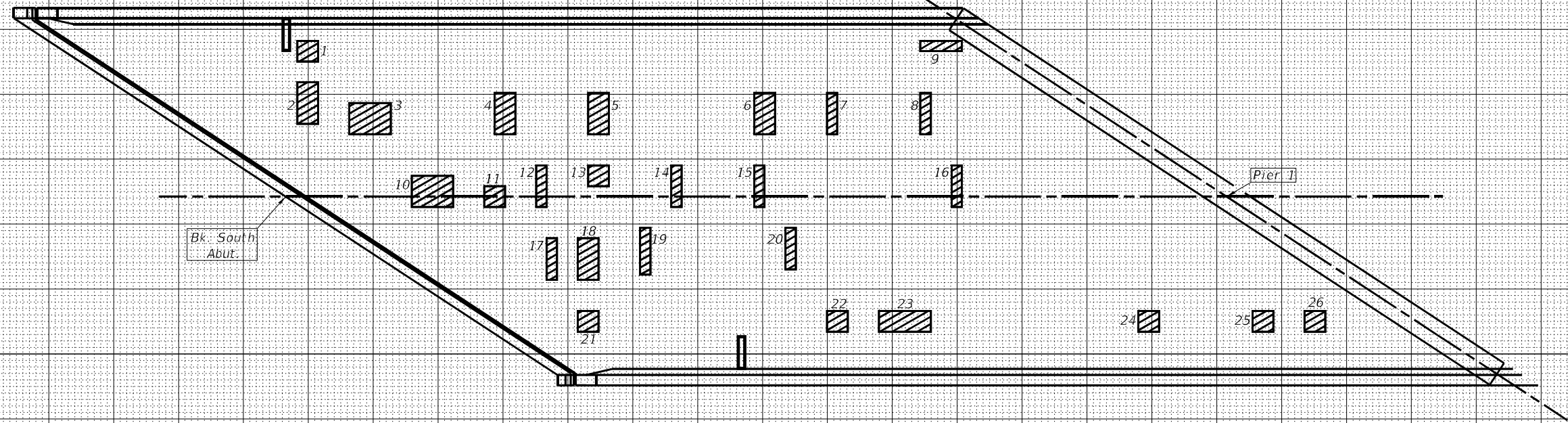
Item	Unit	Quantity
Bituminous Materials (Tack Coat)	Pound	668
Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90	Ton	166
Structural Steel Removal	Pound	4,435
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	1,085

**Notes:**

- A tack coat, according to Section 406 of the Standard Specifications, with a residual asphalt rate of 0.05 lb./sq. ft. shall be applied to the top of the concrete deck before the hot-mix asphalt surfacing is placed.
- Prior to applying the tack coat, the top of the existing concrete deck shall be cleaned according to Article 406.05(b)(1) of the Standard Specifications.
- The quantity for Polymerized Hot-Mix Asphalt Surface Course is calculated based on 2" uniform thickness and a placement factor of 112 lb./sq. yd./inch.
- All existing 1/2" diameter drain holes at each Aluminum Sheeted Construction Joint and in each corner of the deck shall be filled with polyurethane sealant prior to application of the tack coat. The polyurethane sealant shall be according to Article 1050.04 of the Standard Specifications and the color shall be gray. Cost included with Hot-Mix Asphalt Surface Removal (Deck).
- All existing end diaphragms shall be removed. Diaphragm connection angles shall remain. End diaphragms at stage removal line shall be temporarily supported and removed in stages.
- See Sheet 14 of 19 for Concrete Removal details.
- See Sheets 17 & 18 of 19 for proposed diaphragm, parapet, and deck construction.



018-0001 South Span SBL



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
1	2.0	2.0	0.4	
2	2.0	4.0		0.9
3	4.0	3.0		1.3
4	2.0	4.0		0.9
5	2.0	4.0		0.9
6	2.0	4.0		0.9
7	1.0	4.0	0.4	
8	1.0	4.0	0.4	
9	4.0	1.0	0.4	
10	4.0	3.0		1.3
11	2.0	2.0	0.4	
12	1.0	4.0	0.4	
13	2.0	2.0	0.4	
14	1.0	4.0	0.4	
15	1.0	4.0	0.4	
16	1.0	4.0	0.4	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
17	1.0	4.0	0.4	
18	2.0	4.0		0.9
19	1.0	4.5	0.5	
20	1.0	4.0	0.4	
21	2.0	2.0	0.4	
22	2.0	2.0	0.4	
23	4.0	2.0		0.9
24	2.0	2.0	0.4	
25	2.0	2.0	0.4	
26	2.0	2.0	0.4	
TOTAL ROUNDS TO:			9.0	8.0

Deck Slab Repair Notes for SN 018-0001

The southbound structures are planned to be removed first, during Stage I construction of an upcoming bridge replacement project. The deck slab repairs included on this structure are intended to repair the most severely deteriorated areas, to stabilize the bridge deck until the upcoming replacement project.

On the southbound structures only, deck slab repairs within the existing traffic lanes should be prioritized over repairs to existing shoulders.

The following quantity was estimated from overall deck condition:  
Deck Slab Repair (Partial Depth) = 60 Sq Yd

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



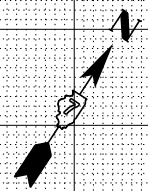
DATE OF SURVEY: 4-10-23  
SURVEY BY: DPM & TMW  
METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES:  
  
DECK SLAB REPAIR (FULL DEPTH TYPE I)  
9.0 SQ YD  
  
DECK SLAB REPAIR (FULL DEPTH TYPE II)  
8.0 SQ YD  
  
DECK SLAB REPAIR (PARTIAL DEPTH)  
60 SQ YD

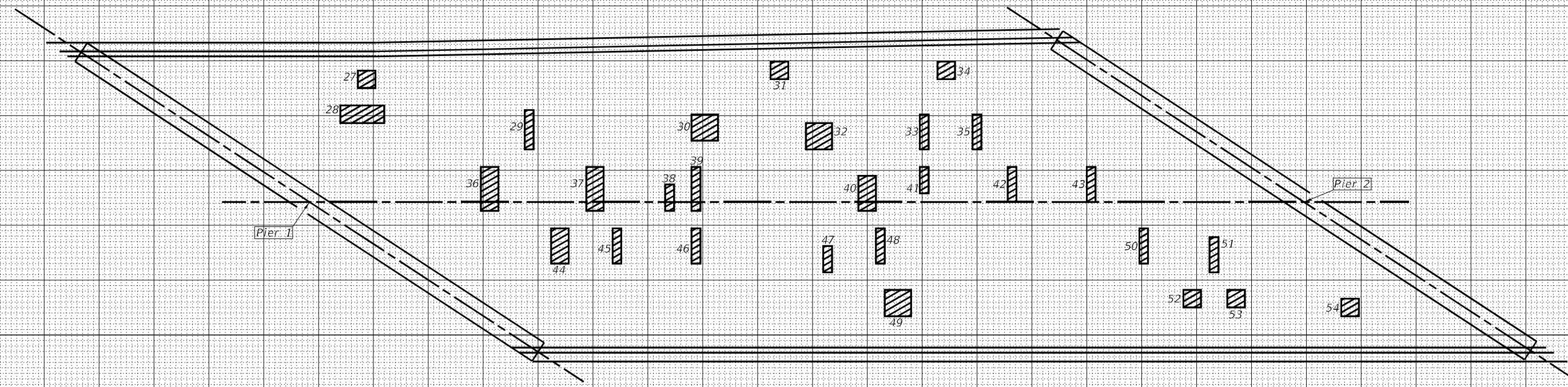
BRIDGE DECK PATCHING (SHEET 1 OF 3)

MODEL: SMOBEL NAMES  
FILE NAME: 230269-1-2-sni-bridge.dgn

FILE NAME = 230269-1-2-sni-bridge.dgn	USER NAME = mlrentz	DESIGNED - T. Walk	REVISED -	<b>STATE OF ILLINOIS</b>	<b>BRIDGE DECK PATCHING, SOUTH SPAN</b>	F.A.I. RTE. = 57	SECTION = D7 BRIDGE REPAIRS 2023-9	COUNTY = CUMBERLAND	TOTAL SHEETS = 53	SHEET NO. = 31
		DRAWN - T. Walk		<b>DEPARTMENT OF TRANSPORTATION</b>		<b>SN 018-0001 (SB)</b>		CONTRACT NO. 74C65		
PLOT SCALE =		CHECKED - D. Macklin		SHEET NO. 7 OF 19 SHEETS		ILLINOIS FED. AID PROJECT				
PLOT DATE = 8/29/2023		DATE - March 2023								



018-0001 Mid Span SBL



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
27	2.0	2.0	0.4	
28	5.0	2.0		1.1
29	1.0	4.5	0.5	
30	3.0	3.0		1.0
31	2.0	2.0	0.4	
32	3.0	3.0		1.0
33	1.0	4.0	0.4	
34	2.0	2.0	0.4	
35	1.0	4.0	0.4	
36	2.0	4.0		0.9
37	2.0	4.0		0.9
38	1.0	3.0	0.3	
39	1.0	4.0	0.4	
40	2.0	4.0		0.9
41	1.0	3.0	0.3	
42	1.0	4.0	0.4	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
43	1.0	4.0	0.4	
44	2.0	4.0		0.9
45	1.0	4.0	0.4	
46	1.0	4.0	0.4	
47	1.0	3.0	0.3	
48	1.0	4.0	0.4	
49	3.0	3.0		1.0
50	1.0	4.0	0.4	
51	1.0	4.0	0.4	
52	2.0	2.0	0.4	
53	2.0	2.0	0.4	
54	2.0	2.0	0.4	
TOTAL ROUNDS TO:			9.0	8.0

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
 SURVEY BY: DPM & TMW  
 METHOD OF SURVEY: VISUAL

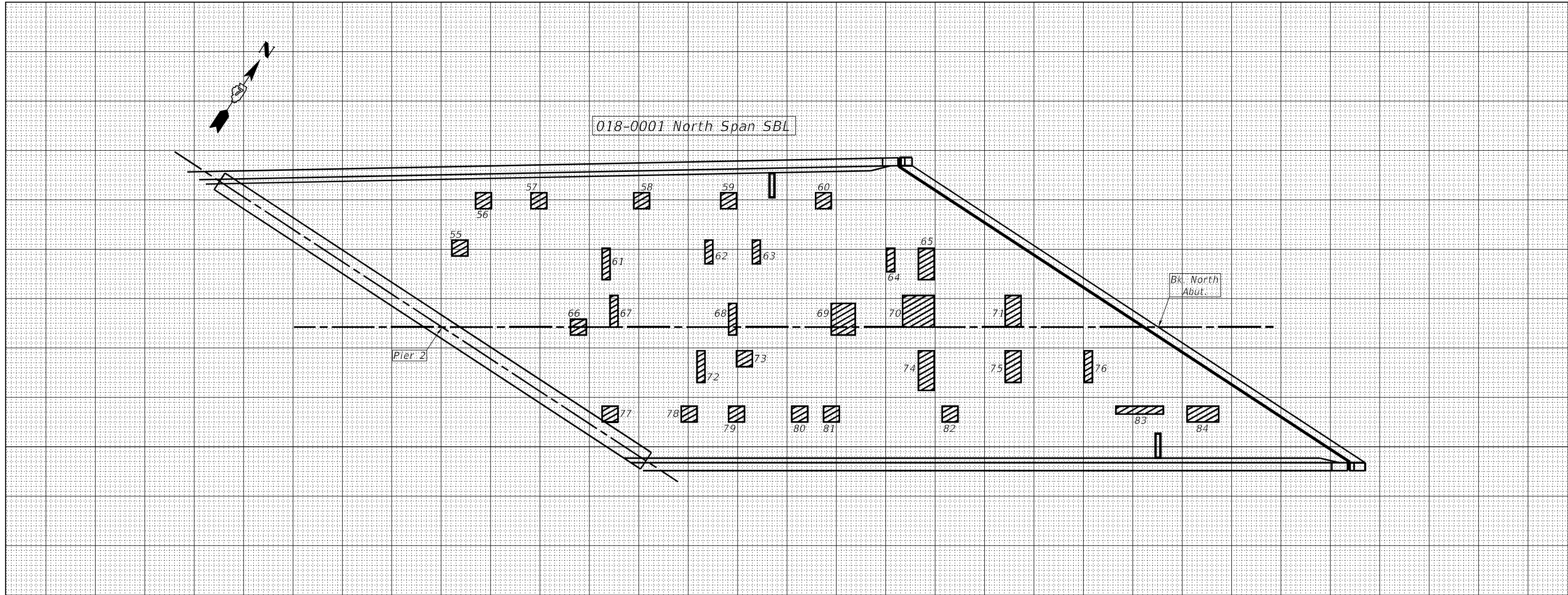
ESTIMATED PAY QUANTITIES:

DECK SLAB REPAIR (FULL DEPTH TYPE I)  
 9.0 SQ YD  
 DECK SLAB REPAIR (FULL DEPTH TYPE II)  
 8.0 SQ YD

BRIDGE DECK PATCHING (SHEET 2 OF 3)

MODEL: S:\MODEL\NAMES  
FILE NAME: 230269-1-2-sb-bridge.dgn

FILE NAME = 230269-1-2-sb-bridge.dgn	USER NAME = mbrentz	DESIGNED - T. Walk	REVISED -	<b>STATE OF ILLINOIS</b>	<b>BRIDGE DECK PATCHING, CENTER SPAN</b>	F.A.I. RTE. = 57	SECTION = D7 BRIDGE REPAIRS 2023-9	COUNTY = CUMBERLAND	TOTAL SHEETS = 53	SHEET NO. = 32
<b>HAMPTON, LENZINI AND RENWICK, INC.</b> <small>3088 STEVENSON DRIVE, SUITE 201          SPRINGFIELD, IL 62776          ILLINOIS PROFESSIONAL DESIGN FIRM          L.S. / P.E. / S.E. CORP. 184.000959</small>		DRAWN - T. Walk		<b>DEPARTMENT OF TRANSPORTATION</b>		<b>SN 018-0001 (SB)</b>		CONTRACT NO. 74C65		
PLOT SCALE =		CHECKED - D. Macklin		SHEET NO. 8 OF 19 SHEETS		ILLINOIS FED. AID PROJECT				
PLOT DATE = 8/29/2023		DATE - March 2023								



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
55	2.0	2.0	0.4	
56	2.0	2.0	0.4	
57	2.0	2.0	0.4	
58	2.0	2.0	0.4	
59	2.0	2.0	0.4	
60	2.0	2.0	0.4	
61	1.0	4.0	0.4	
62	1.0	3.0	0.3	
63	1.0	3.0	0.3	
64	1.0	3.0	0.3	
65	2.0	4.0		0.9
66	2.0	2.0	0.4	
67	1.0	4.0	0.4	
68	1.0	4.0	0.4	
69	3.0	4.0		1.3
70	4.0	4.0		1.8

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
71	2.0	4.0		0.9
72	1.0	4.0	0.4	
73	2.0	2.0	0.4	
74	2.0	5.0		1.1
75	2.0	4.0		0.9
76	1.0	4.0	0.4	
77	2.0	2.0	0.4	
78	2.0	2.0	0.4	
79	2.0	2.0	0.4	
80	2.0	2.0	0.4	
81	2.0	2.0	0.4	
82	2.0	2.0	0.4	
83	6.0	1.0		0.7
84	4.0	2.0		0.9
TOTAL ROUNDS TO:			10.0	9.0

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
 SURVEY BY: DPM & TMW  
 METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES:  
 DECK SLAB REPAIR (FULL DEPTH TYPE I)  
 10.0 SQ YD  
 DECK SLAB REPAIR (FULL DEPTH TYPE II)  
 9.0 SQ YD

BRIDGE DECK PATCHING (SHEET 3 OF 3)

FILE NAME = 230269-1-2-sb-bridge.dgn  
 HAMPTON, LENZINI AND RENWICK, INC.  
 3088 STEVENSON DRIVE, SUITE 201  
 SPRINGFIELD, IL 62761-4710  
 ILLINOIS PROFESSIONAL DESIGN FIRM  
 LB / PE / SE CORP. 184.000959

USER NAME = mbrentz  
 PLOT SCALE =  
 PLOT DATE = 8/29/2023

DESIGNED - T. Walk  
 DRAWN - T. Walk  
 CHECKED - D. Macklin  
 DATE - March 2023

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

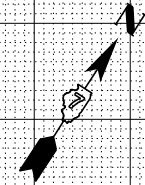
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE DECK PATCHING, NORTH SPAN  
 SN 018-0001 (SB)

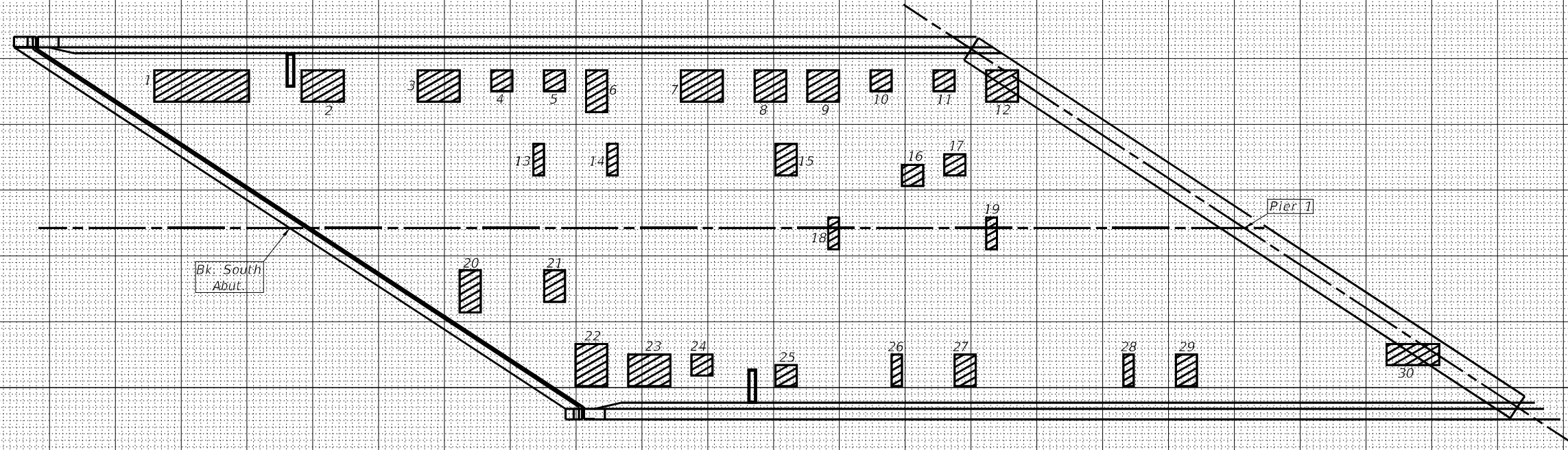
SHEET NO. 9 OF 19 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	33
CONTRACT NO. 74C65				

ILLINOIS FED. AID PROJECT



018-0002 South Span NBL



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
1	9.0	3.0		3.0
2	4.0	3.0		1.3
3	4.0	3.0		1.3
4	2.0	2.0	0.4	
5	2.0	2.0	0.4	
6	2.0	4.0		0.9
7	4.0	3.0		1.3
8	3.0	3.0		1.0
9	3.0	3.0		1.0
10	2.0	2.0	0.4	
11	2.0	2.0	0.4	
12	3.0	3.0		1.0
13	1.0	3.0	0.3	
14	1.0	3.0	0.3	
15	2.0	3.0		0.7
16	2.0	2.0	0.4	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
17	2.0	2.0	0.4	
18	1.0	3.0	0.3	
19	1.0	3.0	0.3	
20	2.0	4.0		0.9
21	2.0	3.0		0.7
22	3.0	4.0		1.3
23	4.0	3.0		1.3
24	2.0	2.0	0.4	
25	2.0	2.0	0.4	
26	1.0	3.0	0.3	
27	2.0	3.0		0.7
28	1.0	3.0	0.3	
29	2.0	3.0		0.7
30	5.0	2.0		1.1
TOTAL ROUNDS TO:			6.0	19.0

Deck Slab Repair Notes for SN 018-0002

The northbound structures are planned to carry Stage I traffic during an upcoming bridge replacement project. The deck slab repairs included on this structure are intended to stabilize the bridge deck until the upcoming replacement project and during Stage I construction of the upcoming project.

On the northbound structures only, deck slab repairs within the traffic lanes and on the shoulders should be prioritized equally. During the upcoming replacement project, the traffic pattern on the existing northbound bridges will be altered for maintenance of traffic during Stage I construction.

The following quantity was estimated from overall deck condition:  
Deck Slab Repair (Partial Depth) = 80 Sq Yd

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
SURVEY BY: DPM & TMW  
METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES:  
  
DECK SLAB REPAIR (FULL DEPTH TYPE I)  
6.0 SQ YD  
  
DECK SLAB REPAIR (FULL DEPTH TYPE II)  
19.0 SQ YD  
  
DECK SLAB REPAIR (PARTIAL DEPTH)  
80 SQ YD

BRIDGE DECK PATCHING (SHEET 1 OF 3)

MODEL: SMOBELNAMES  
FILE NAME: 230269-1-2-sni-bridge.dgn

FILE NAME = 230269-1-2-sni-bridge.dgn  
USER NAME = mlorenz  
**HAMPTON, LENZINI AND RENWICK, INC.**  
3088 STEVENSON DRIVE, SUITE 201  
SPRINGFIELD, IL 62776  
ILLINOIS PROFESSIONAL DESIGN FIRM  
L.S. / P.E. / S.E. CORP. 184.000959

DESIGNED - T. Walk  
DRAWN - T. Walk  
CHECKED - D. Macklin  
DATE - March 2023

REVISOR -  
REVISOR -  
REVISOR -  
REVISOR -

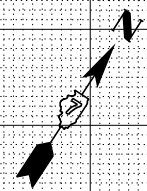
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**BRIDGE DECK PATCHING, SOUTH SPAN**  
**SN 018-0002 (NB)**

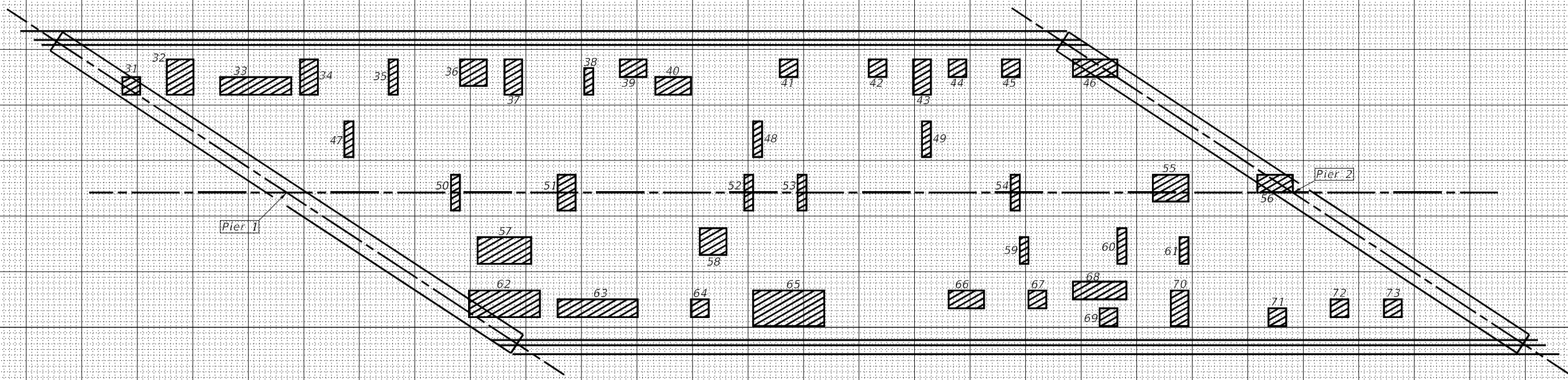
SHEET NO. 10 OF 19 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	34
CONTRACT NO. 74C65				

ILLINOIS FED. AID PROJECT



018-0002 Mid Span NBL



PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
31	2.0	2.0	0.4	
32	3.0	4.0		1.3
33	8.0	2.0		1.8
34	2.0	4.0		0.9
35	1.0	4.0	0.4	
36	3.0	3.0		1.0
37	2.0	4.0		0.9
38	1.0	3.0	0.3	
39	3.0	2.0		0.7
40	4.0	2.0		0.9
41	2.0	2.0	0.4	
42	2.0	2.0	0.4	
43	2.0	4.0		0.9
44	2.0	2.0	0.4	
45	2.0	2.0	0.4	
46	5.0	2.0		1.1

PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
47	1.0	4.0	0.4	
48	1.0	4.0	0.4	
49	1.0	4.0	0.4	
50	1.0	4.0	0.4	
51	2.0	4.0		0.9
52	1.0	4.0	0.4	
53	1.0	4.0	0.4	
54	1.0	4.0	0.4	
55	4.0	3.0		1.3
56	4.0	2.0		0.9
57	6.0	3.0		2.0
58	3.0	3.0		1.0
59	1.0	3.0	0.3	
60	1.0	4.0	0.4	
61	1.0	3.0	0.3	
62	8.0	3.0		2.7

PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
63	9.0	2.0		2.0
64	2.0	2.0	0.4	
65	8.0	4.0		3.6
66	4.0	2.0		0.9
67	2.0	2.0	0.4	
68	6.0	2.0		1.3
69	2.0	2.0	0.4	
70	2.0	4.0		0.9
71	2.0	2.0	0.4	
72	2.0	2.0	0.4	
73	2.0	2.0	0.4	
TOTAL ROUNDS TO:			10.0	27.0

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
 SURVEY BY: DPM & TMW  
 METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES:  
 DECK SLAB REPAIR (FULL DEPTH TYPE I)  
 10.0 SQ YD  
 DECK SLAB REPAIR (FULL DEPTH TYPE II)  
 27.0 SQ YD

BRIDGE DECK PATCHING (SHEET 2 OF 3)

FILE NAME = 230269-1-2-sni-bridge.dgn  
 USER NAME = mbrentz  
 DESIGNED - T. Walk  
 DRAWN - T. Walk  
 CHECKED - D. Macklin  
 DATE - March 2023

REVISOR -  
 REVISOR -  
 REVISOR -  
 REVISOR -

DESIGNED - T. Walk  
 DRAWN - T. Walk  
 CHECKED - D. Macklin  
 DATE - March 2023

REVISOR -  
 REVISOR -  
 REVISOR -  
 REVISOR -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

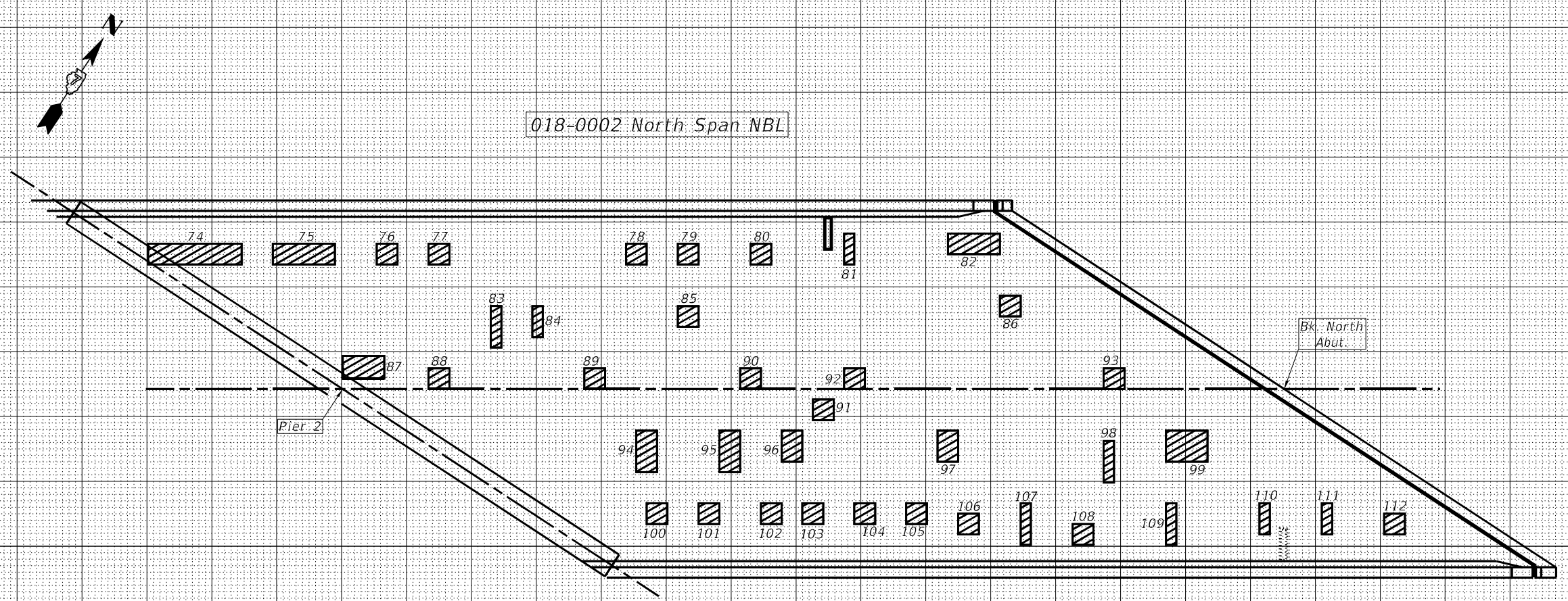
BRIDGE DECK PATCHING, CENTER SPAN  
 SN 018-0002 (NB)

SHEET NO. 11 OF 19 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	35
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				

MODEL: SMOBELNAMES  
FILE NAME: 230269-1-2-sni-bridge.dgn





PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
74	9.0	2.0		2.0
75	6.0	2.0		1.3
76	2.0	2.0	0.4	
77	2.0	2.0	0.4	
78	2.0	2.0	0.4	
79	2.0	2.0	0.4	
80	2.0	2.0	0.4	
81	1.0	3.0	0.3	
82	5.0	2.0		1.1
83	1.0	4.0	0.4	
84	1.0	3.0	0.3	
85	2.0	2.0	0.4	
86	2.0	2.0	0.4	
87	4.0	2.0		0.9
88	2.0	2.0	0.4	
89	2.0	2.0	0.4	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
90	2.0	2.0	0.4	
91	2.0	2.0	0.4	
92	2.0	2.0	0.4	
93	2.0	2.0	0.4	
94	2.0	4.0		0.9
95	2.0	4.0		0.9
96	2.0	3.0		0.7
97	2.0	3.0		0.7
98	1.0	4.0	0.4	
99	4.0	3.0		1.3
100	2.0	2.0	0.4	
101	2.0	2.0	0.4	
102	2.0	2.0	0.4	
103	2.0	2.0	0.4	
104	2.0	2.0	0.4	
105	2.0	2.0	0.4	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
106	2.0	2.0	0.4	
107	1.0	4.0	0.4	
108	2.0	2.0	0.4	
109	1.0	4.0	0.4	
110	1.0	3.0	0.3	
111	1.0	3.0	0.3	
112	2.0	2.0	0.4	
TOTAL ROUNDS TO:			13.0	10.0

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
 SURVEY BY: DPM & TMW  
 METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES:  
 DECK SLAB REPAIR (FULL DEPTH TYPE I)  
 13.0 SQ YD  
 DECK SLAB REPAIR (FULL DEPTH TYPE II)  
 10.0 SQ YD

BRIDGE DECK PATCHING (SHEET 3 OF 3)

FILE NAME = 230269-1-2-sni-bridge.dgn  
 HAMPTON, LENZINI AND RENWICK, INC.  
 3088 STEVENSON DRIVE, SUITE 201  
 SPRINGFIELD, IL 62776  
 ILLINOIS PROFESSIONAL DESIGN FIRM  
 L.S. / P.E. / S.E. / CDRP. 184.000959

USER NAME = mbrentz  
 PLOT SCALE =  
 PLOT DATE = 8/29/2023

DESIGNED - T. Walk  
 DRAWN - T. Walk  
 CHECKED - D. Macklin  
 DATE - March 2023

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

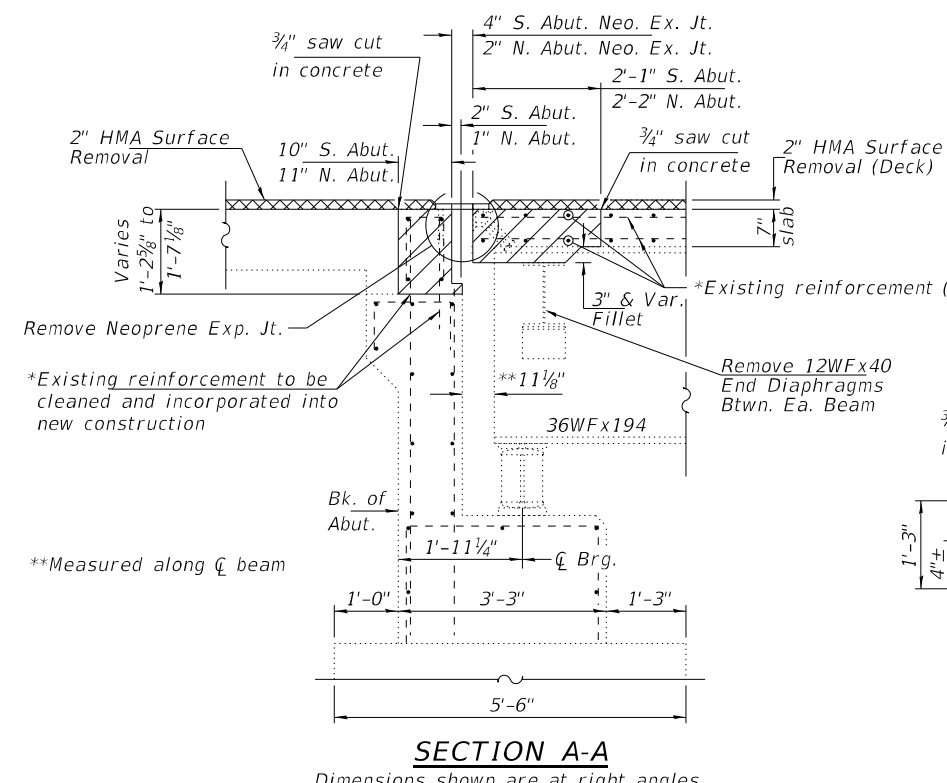
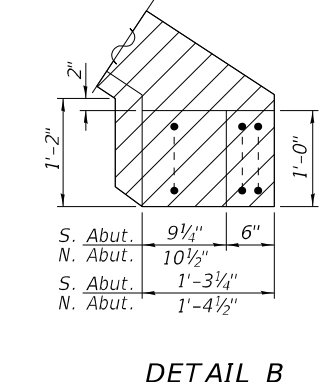
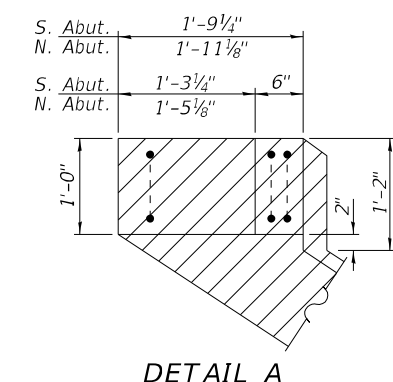
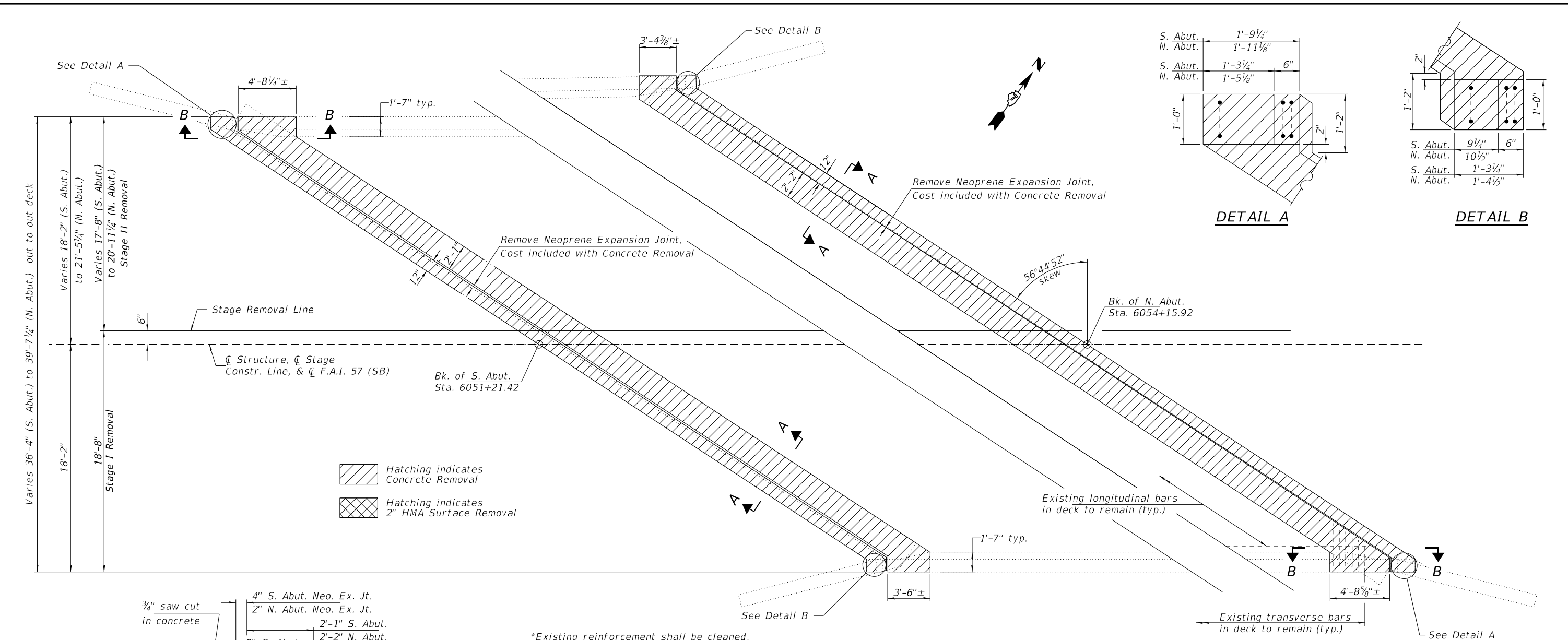
BRIDGE DECK PATCHING, NORTH SPAN  
 SN 018-0002 (NB)

SHEET NO. 12 OF 19 SHEETS

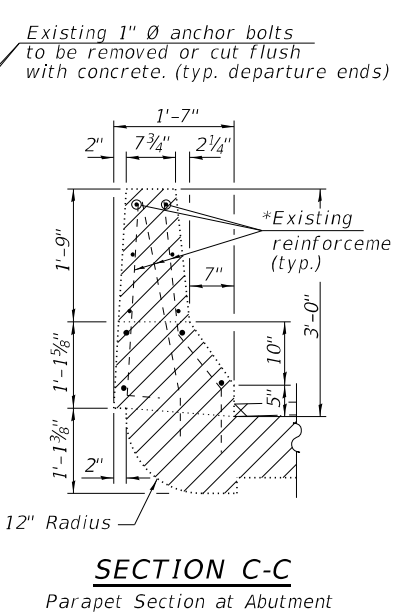
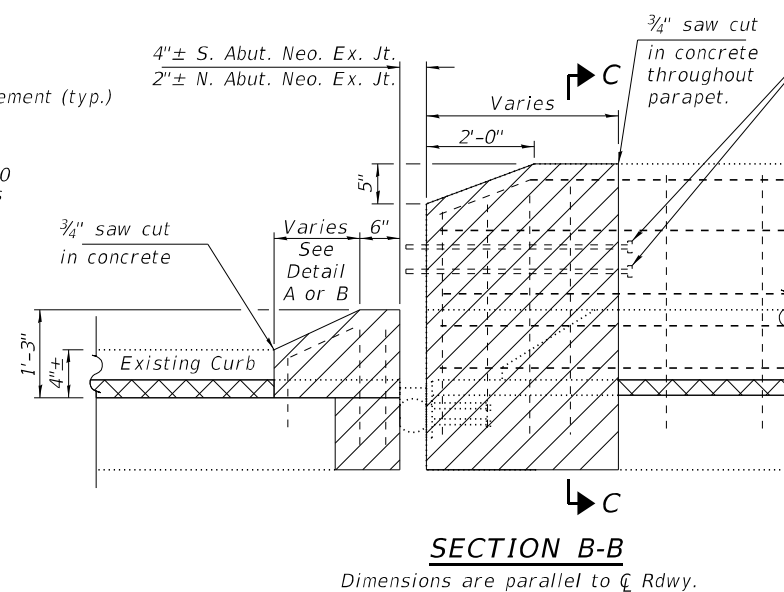
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	36
CONTRACT NO. 74C65				

ILLINOIS FED. AID PROJECT





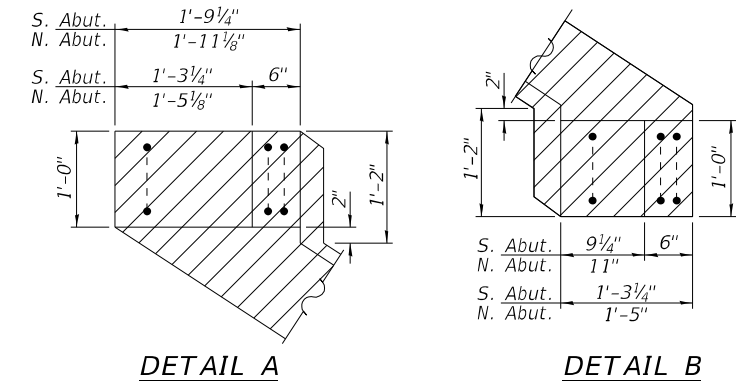
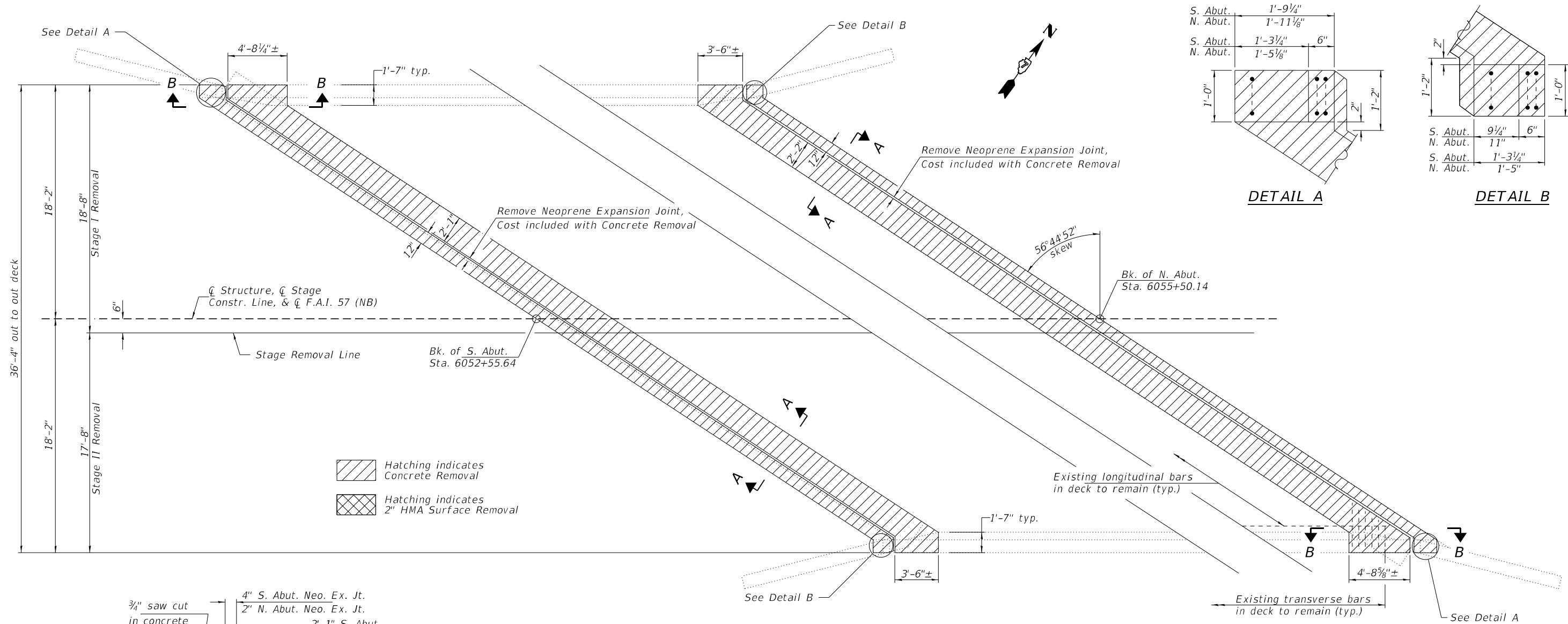
\*Existing reinforcement shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.



**BILL OF MATERIAL**

Item	Unit	Quantity
Concrete Removal	Cu. Yd.	17.8

**Notes:**  
 Removal and disposal of the existing Neoprene Expansion Joint and other materials that fall within the limits of the Concrete Removal, will not be paid for separately, but shall be included in the cost of Concrete Removal.  
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.  
 If the existing Name Plate falls within the limits of the Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost is included with Concrete Superstructure.  
 All existing end diaphragms shall be removed. Diaphragm connection angles shall remain. End diaphragms at stage removal line shall be temporarily supported and removed in stages, see detail on Sheet 5 of 19.

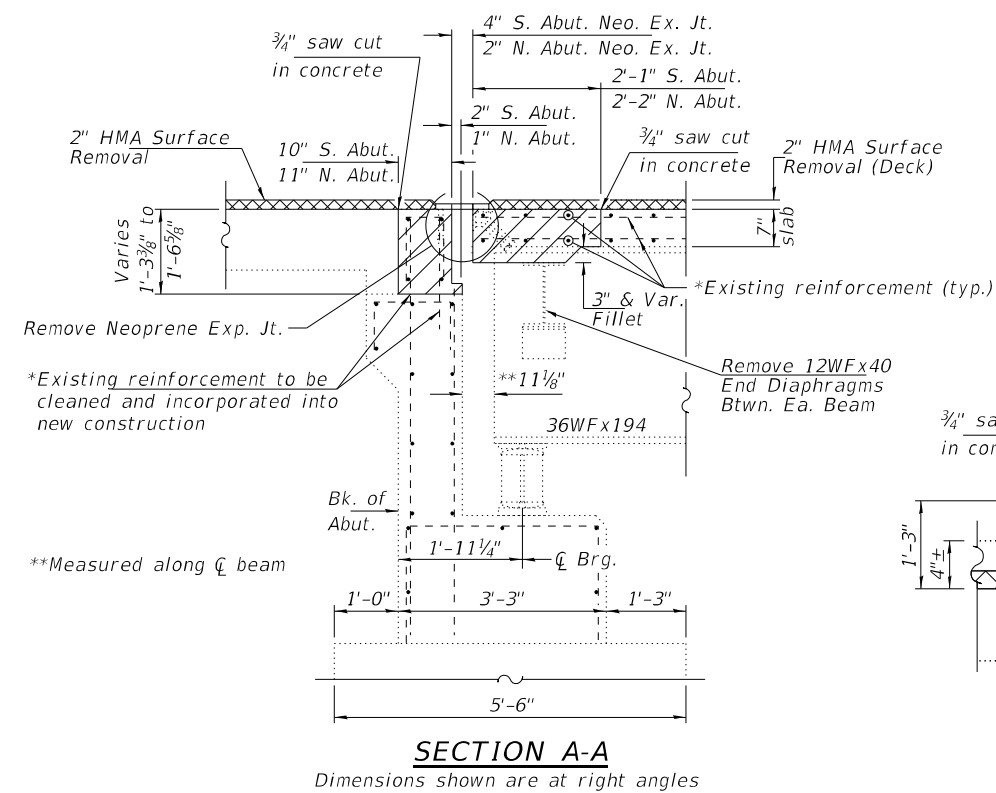


Hatching indicates Concrete Removal  
 Hatching indicates 2" HMA Surface Removal

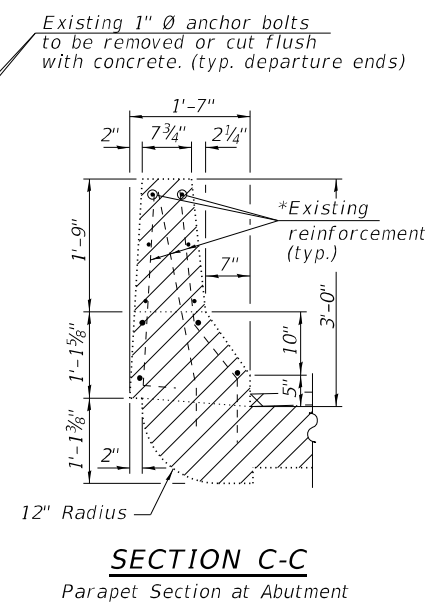
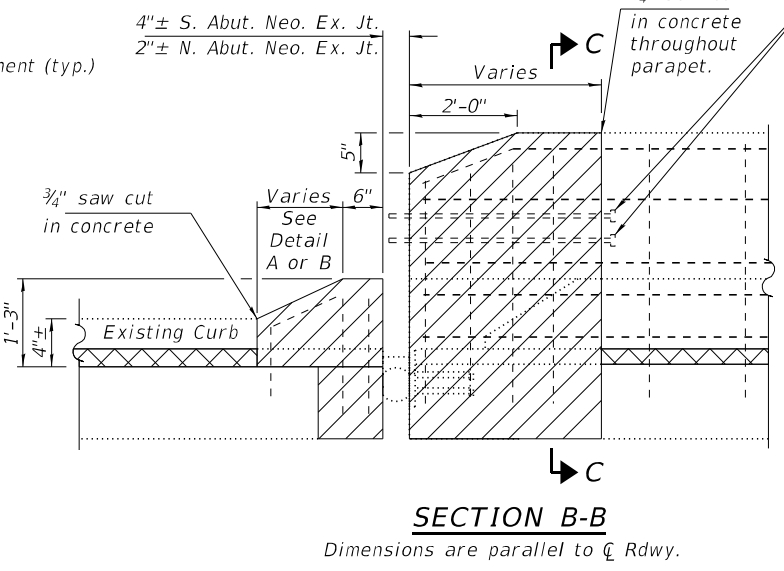
**REMOVAL PLAN**

**BILL OF MATERIAL**

Item	Unit	Quantity
Concrete Removal	Cu. Yd.	16.8

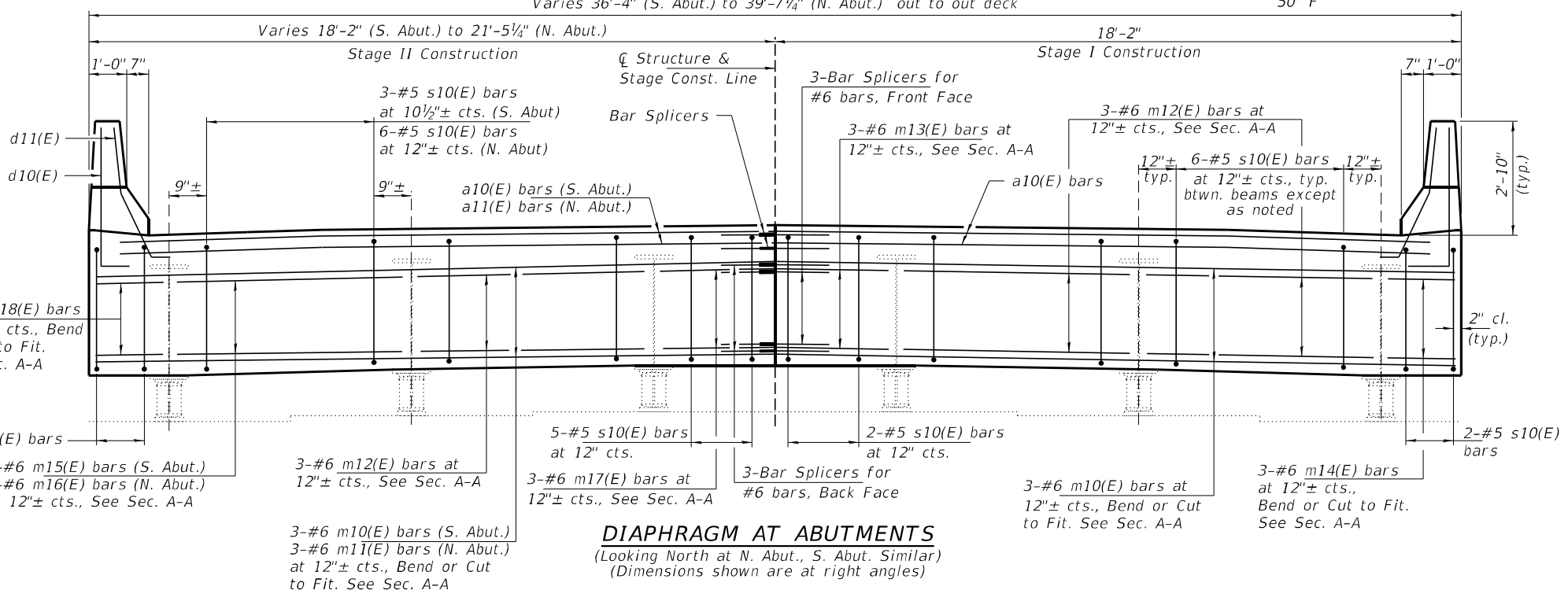
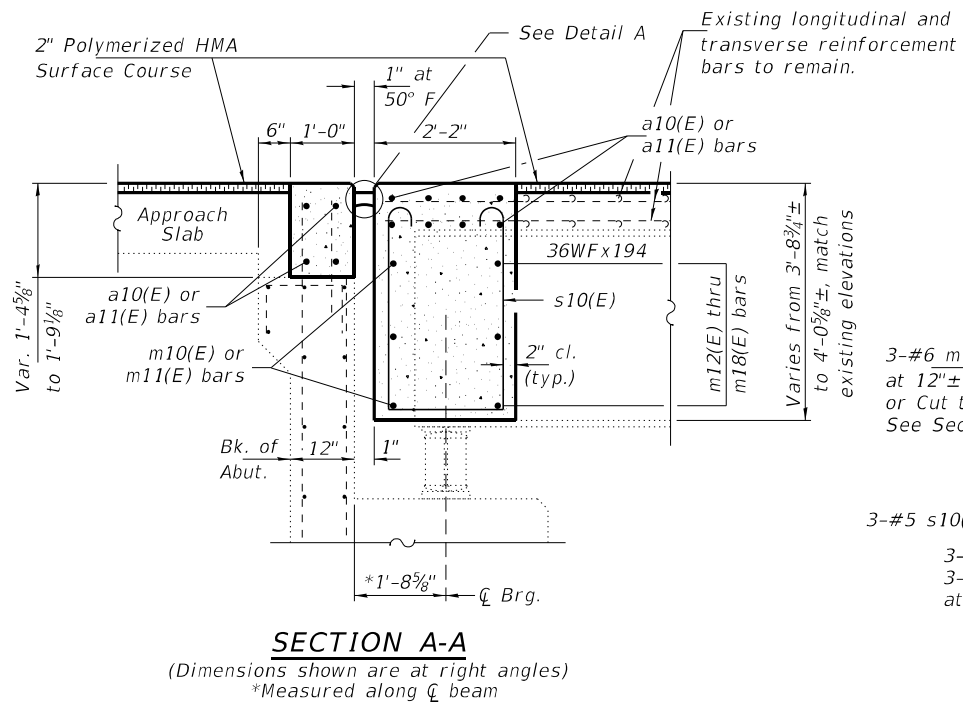
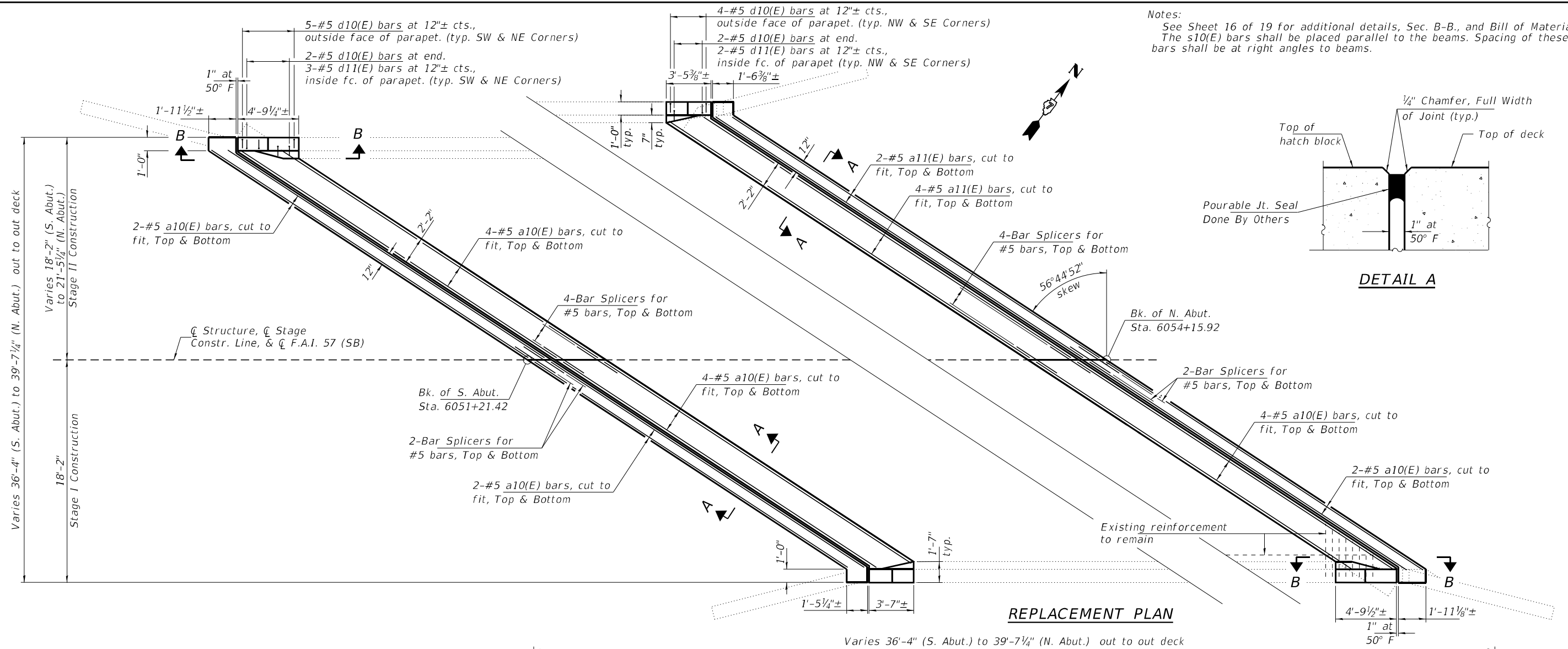


\*Existing reinforcement shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal.

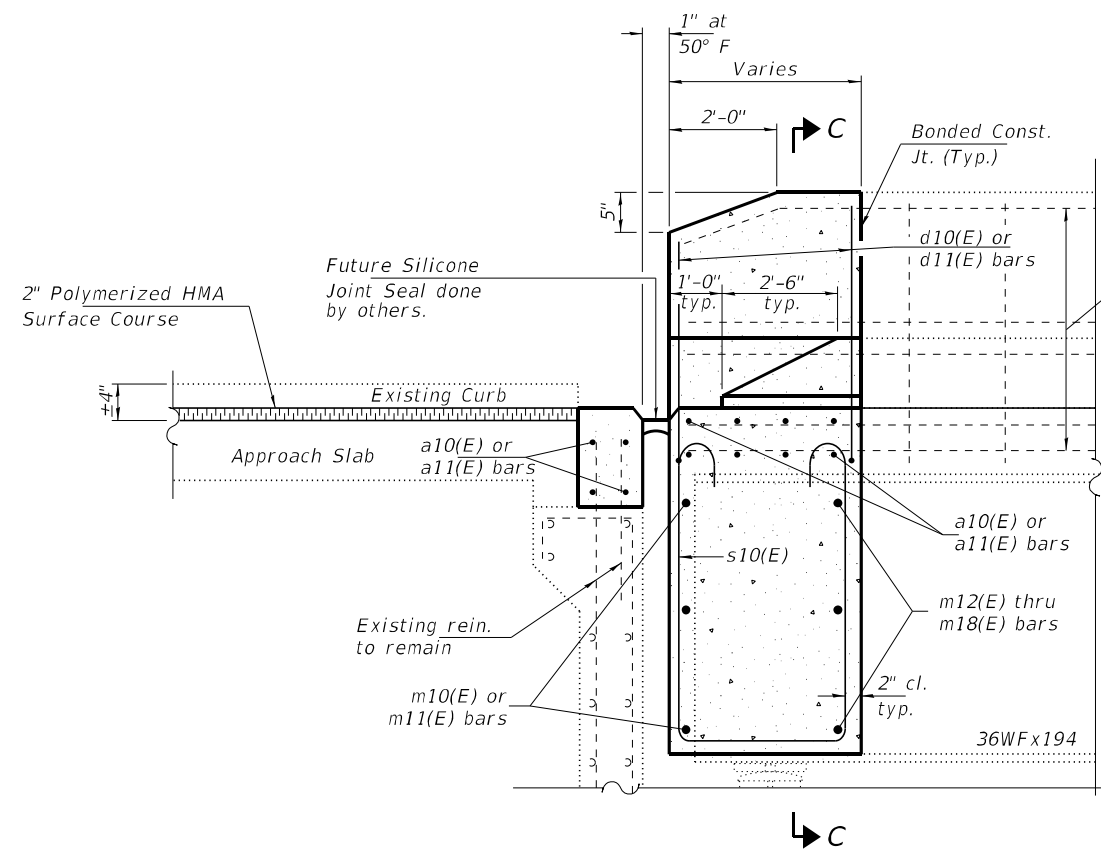


**Notes:**  
 Removal and disposal of the existing Neoprene Expansion Joint and other materials that fall within the limits of the Concrete Removal, will not be paid for separately, but shall be included in the cost of Concrete Removal.  
 Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.  
 If the existing Name Plate falls within the limits of the Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost is included with Concrete Superstructure.  
 All existing end diaphragms shall be removed. Diaphragm connection angles shall remain. End diaphragms at stage removal line shall be temporarily supported and removed in stages, see detail on Sheet 6 of 19.

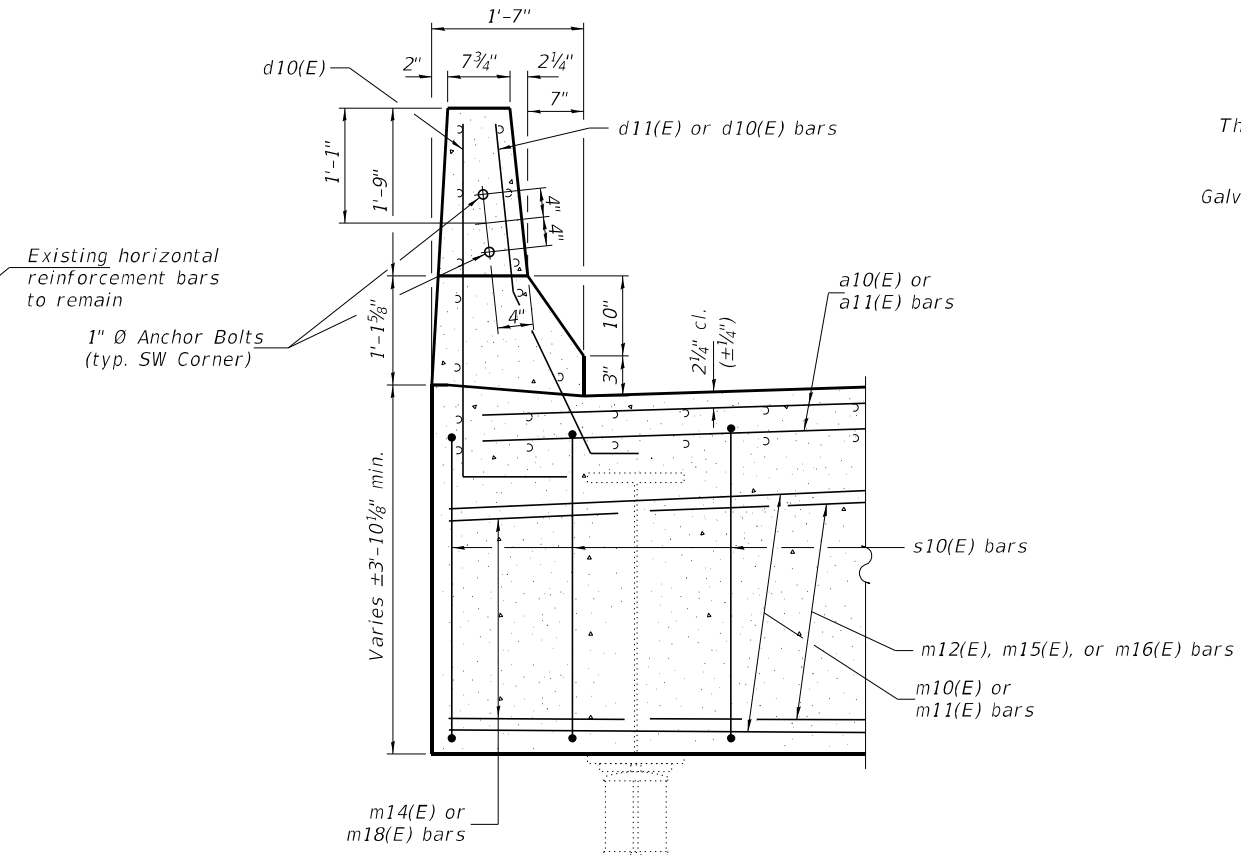
Notes:  
See Sheet 16 of 19 for additional details, Sec. B-B., and Bill of Material.  
The s10(E) bars shall be placed parallel to the beams. Spacing of these bars shall be at right angles to beams.



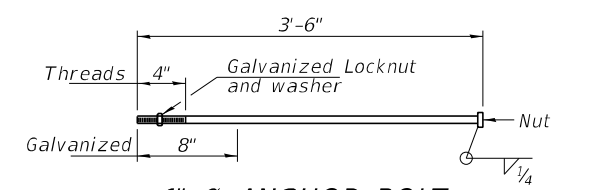
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HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62776	PLOT SCALE =	CHECKED - S.T.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	39	
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISED -			CONTRACT NO. 74C65					
		CHECKED - S.T.M./S.M.S.	REVISED -			ILLINOIS FED. AID PROJECT					



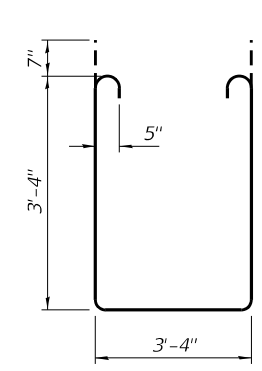
**SECTION B-B**  
(Dimensions shown are parallel to roadway)



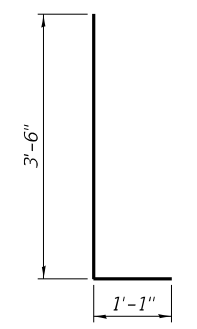
**SECTION C-C**



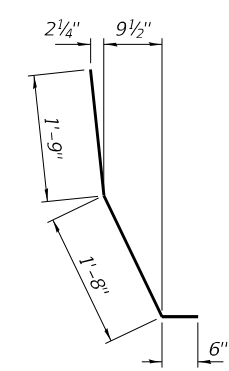
**1" Ø ANCHOR BOLT**  
(Cost of anchor bolts incidental and included into the cost of Concrete Superstructure)



**BAR s10(E)**



**BAR d10(E)**

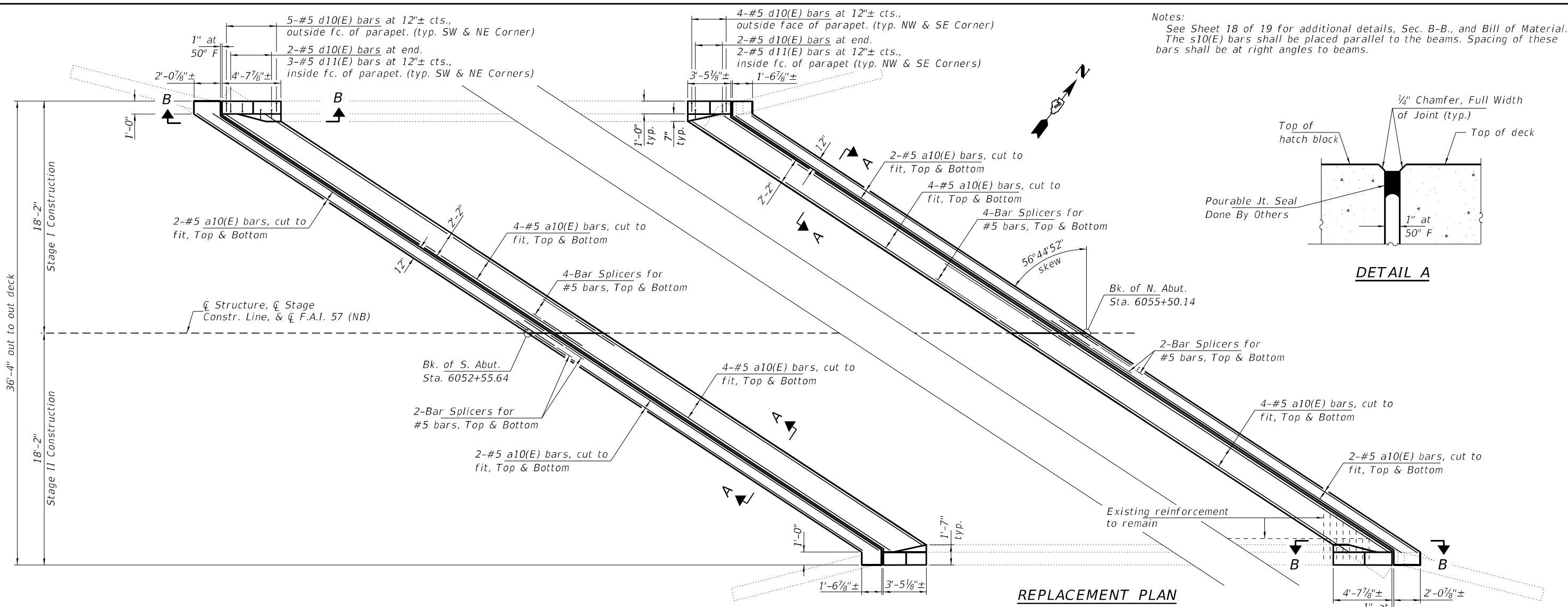


**BAR d11(E)**

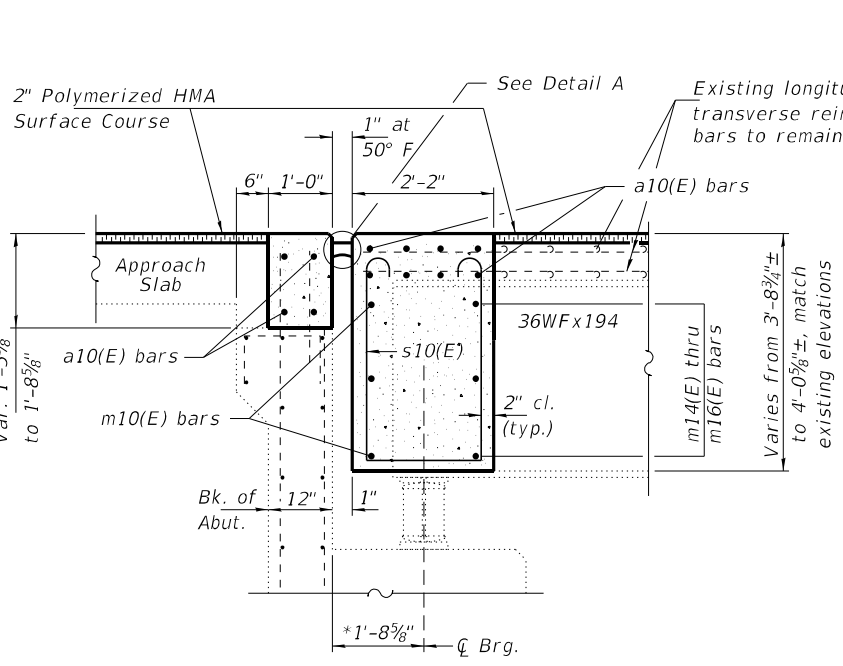
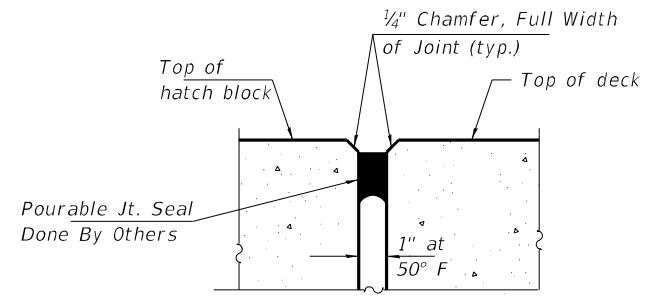
**BILL OF MATERIAL**

BAR	NO.	SIZE	LENGTH	SHAPE
a10(E)	36	#5	30'-11"	—
a11(E)	12	#5	36'-6"	—
d10(E)	26	#5	4'-7"	┌
d11(E)	10	#5	3'-11"	└
m10(E)	9	#6	31'-10"	—
m11(E)	3	#6	37'-6"	—
m12(E)	18	#6	12'-2"	—
m13(E)	6	#6	3'-0"	—
m14(E)	6	#6	2'-3"	—
m15(E)	3	#6	5'-4"	—
m16(E)	3	#6	11'-2"	—
m17(E)	6	#6	8'-6"	—
m18(E)	6	#6	3'-8"	—
s10(E)	69	#5	11'-2"	U
Concrete Superstructure			Cu. Yd.	53.0
Protective Coat			Sq. Yd.	110
Reinforcement Bars, Epoxy Coated			Pound	3,750
Bar Splicers			Each	36

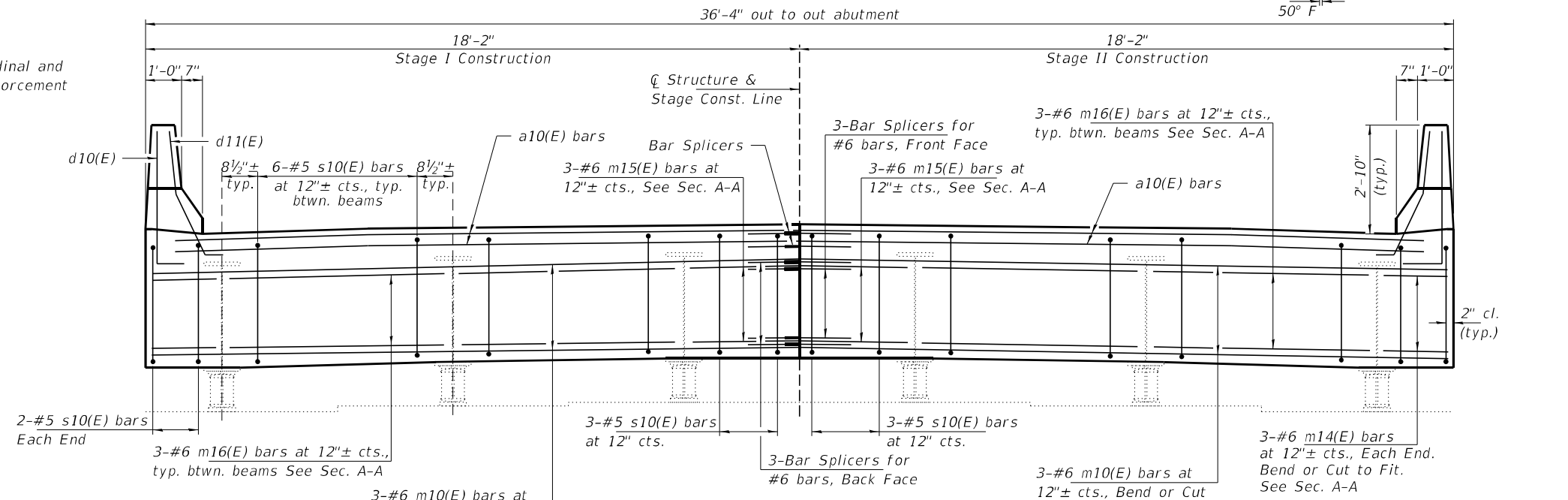
Notes:  
All exposed edges shall have a 3/4" chamfer.



Notes:  
See Sheet 18 of 19 for additional details, Sec. B-B., and Bill of Material.  
The s10(E) bars shall be placed parallel to the beams. Spacing of these bars shall be at right angles to beams.

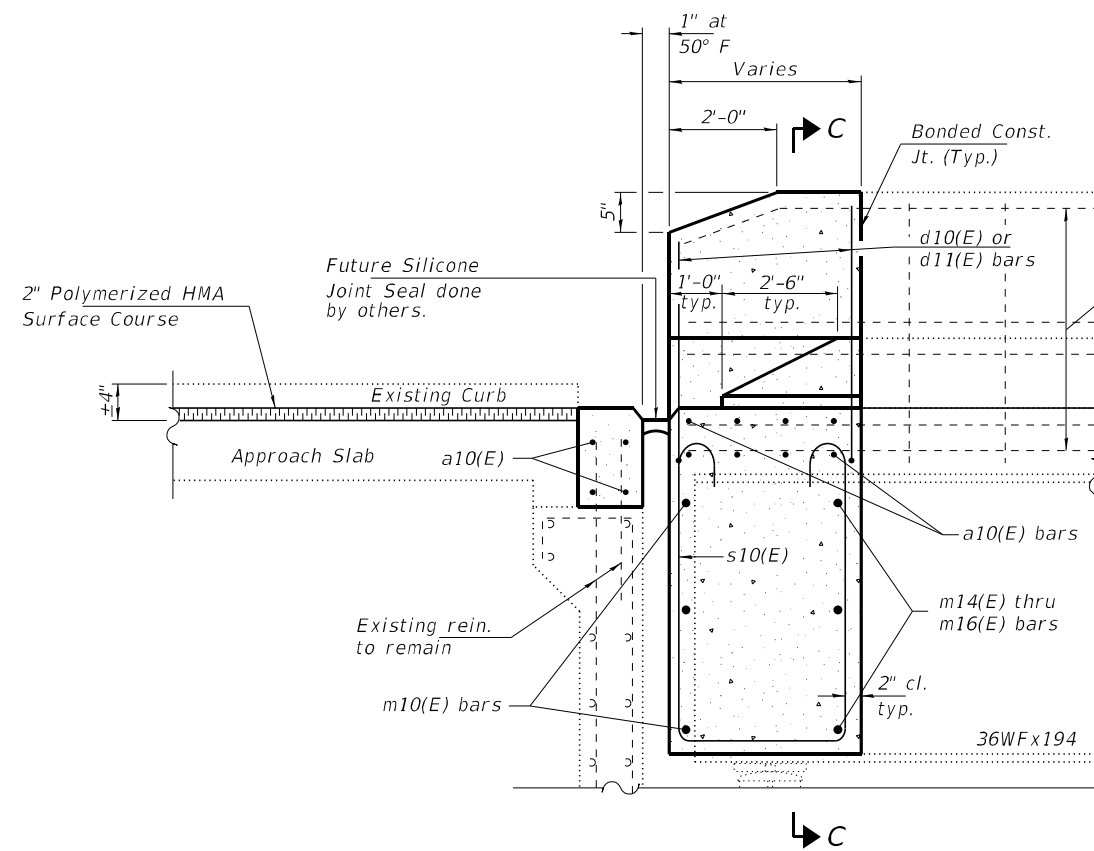


**SECTION A-A**  
(Dimensions shown are at right angles)  
\*Measured along  $\bar{c}$  beam

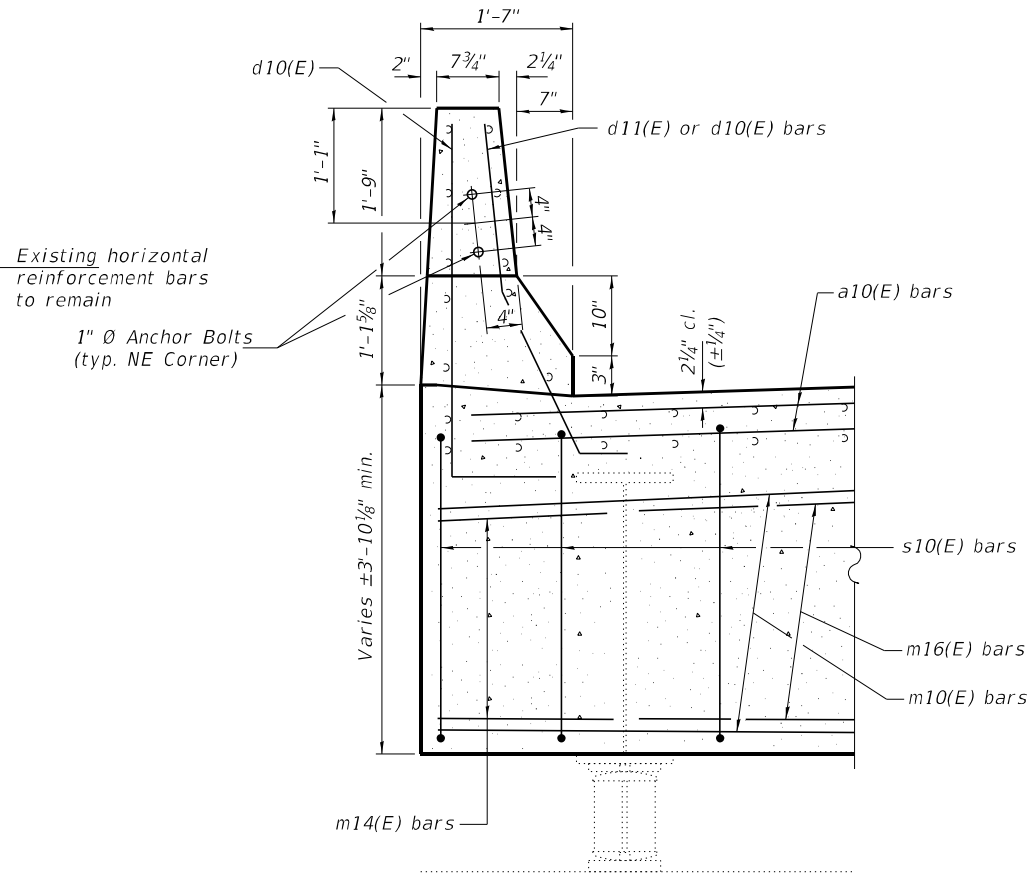


**DIAPHRAGM AT ABUTMENTS**  
(Looking North at North Abutment, South Abutment Similar)  
(Dimensions shown are at right angles)

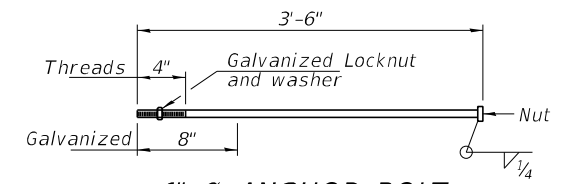
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HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.T.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	41	
ILLINOIS PROFESSIONAL DESIGN FIRM L.S./P.E./S.E. CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISED -			CONTRACT NO. 74C65					
		CHECKED - S.T.M./S.M.S.	REVISED -			ILLINOIS   FED. AID PROJECT					



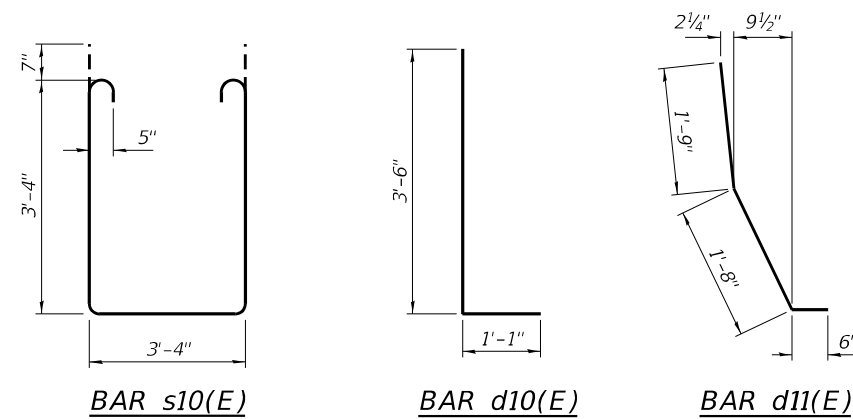
**SECTION B-B**  
(Dimensions shown are parallel to roadway)



**SECTION C-C**



**1" Ø ANCHOR BOLT**  
(Cost of anchor bolts incidental and included into the cost of Concrete Superstructure)



**BILL OF MATERIAL**

BAR	NO.	SIZE	LENGTH	SHAPE
a10(E)	48	#5	30'-11"	—
d10(E)	26	#5	4'-7"	—
d11(E)	10	#5	3'-11"	—
m10(E)	12	#6	31'-10"	—
m14(E)	12	#6	2'-3"	—
m15(E)	12	#6	5'-4"	—
m16(E)	24	#6	11'-2"	—
s10(E)	68	#5	11'-2"	□
Concrete Superstructure			Cu. Yd.	50.8
Protective Coat			Sq. Yd.	105
Reinforcement Bars, Epoxy Coated			Pound	3,620
Bar Splicers			Each	36

Notes:  
All exposed edges shall have a 3/4" chamfer.

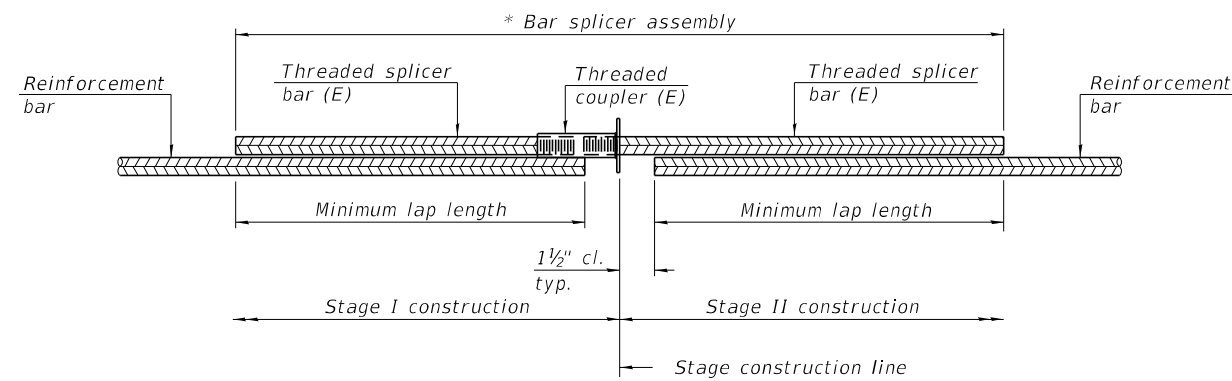
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HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761		CHECKED - S.T.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 164.002959	PLOT SCALE =	DRAWN - G.D.M.	REVISED -
	PLOT DATE = 8/29/2023	CHECKED - S.T.M./S.M.S.	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE DETAILS  
SN 018-0002 (NB)

SHEET NO. 18 OF 19 SHEETS

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	42
CONTRACT NO. 74C65				
ILLINOIS   FED. AID PROJECT				



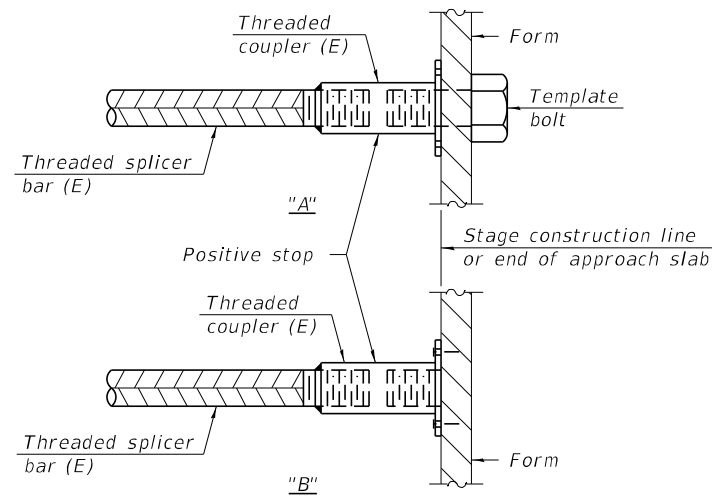
**STANDARD BAR SPLICER ASSEMBLY PLAN**

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Hatch Block	#5	16	3'-6"
Deck	#5	32	3'-6"
Diaphragm, FF	#6	12	3'-6"
Diaphragm, BF	#6	12	4'-5"

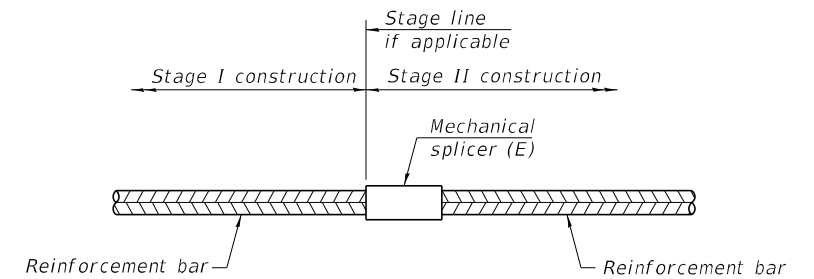


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

Notes:  
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-1-2023

FILE NAME = 230269-1-2-shl-bridge.dgn	USER NAME = mlrentz	DESIGNED - S.T.M.	REVISIONS -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS I-57 OVER D.R.E.I. - SN 018-0001 (SB) SN 018-0002 (NB)</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.T.M.	REVISIONS -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	43	
<b>HLR</b> ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISIONS -			CONTRACT NO. 74C65					
		CHECKED - S.T.M./S.M.S.	REVISIONS -			SHEET NO. 19 OF 19 SHEETS					

BENCHMARK: 4380-2: Chiseled "□" on the S.W. wingwall of S.N. 018-0003 (SB) bridge. Sta. 117+55, Lt. 68', Elev. 684.79

EXISTING STRUCTURES: S.N. 018-0003 (SB) S.N. 018-0004 (NB), Sta. 118+48 (F.A.I. 57) and Sta. 50+00 (U.S. RTE. 45)

Dual Three span continuous steel I-beam structures originally built in 1963.

Rehabilitated with new center span beams in 2012, and new HMA wearing surface in 2014.

137'-7" bk.-bk. abuts.; 44'-0" o.-o. deck.

Traffic maintained utilizing stage construction.

**DESIGN SPECIFICATIONS**

2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

**LOADING HS20 & ALT. (EXIST. CONST.)**

**DESIGN STRESSES**

FIELD UNITS (NEW CONST.)

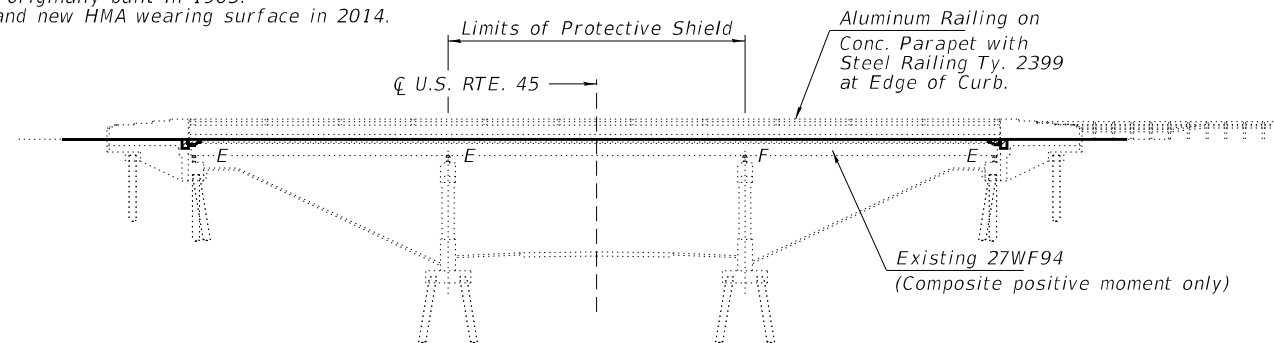
f'c = 4,000 psi (Concrete)  
fy = 60,000 psi (Reinf.)

EXISTING CONSTRUCTION

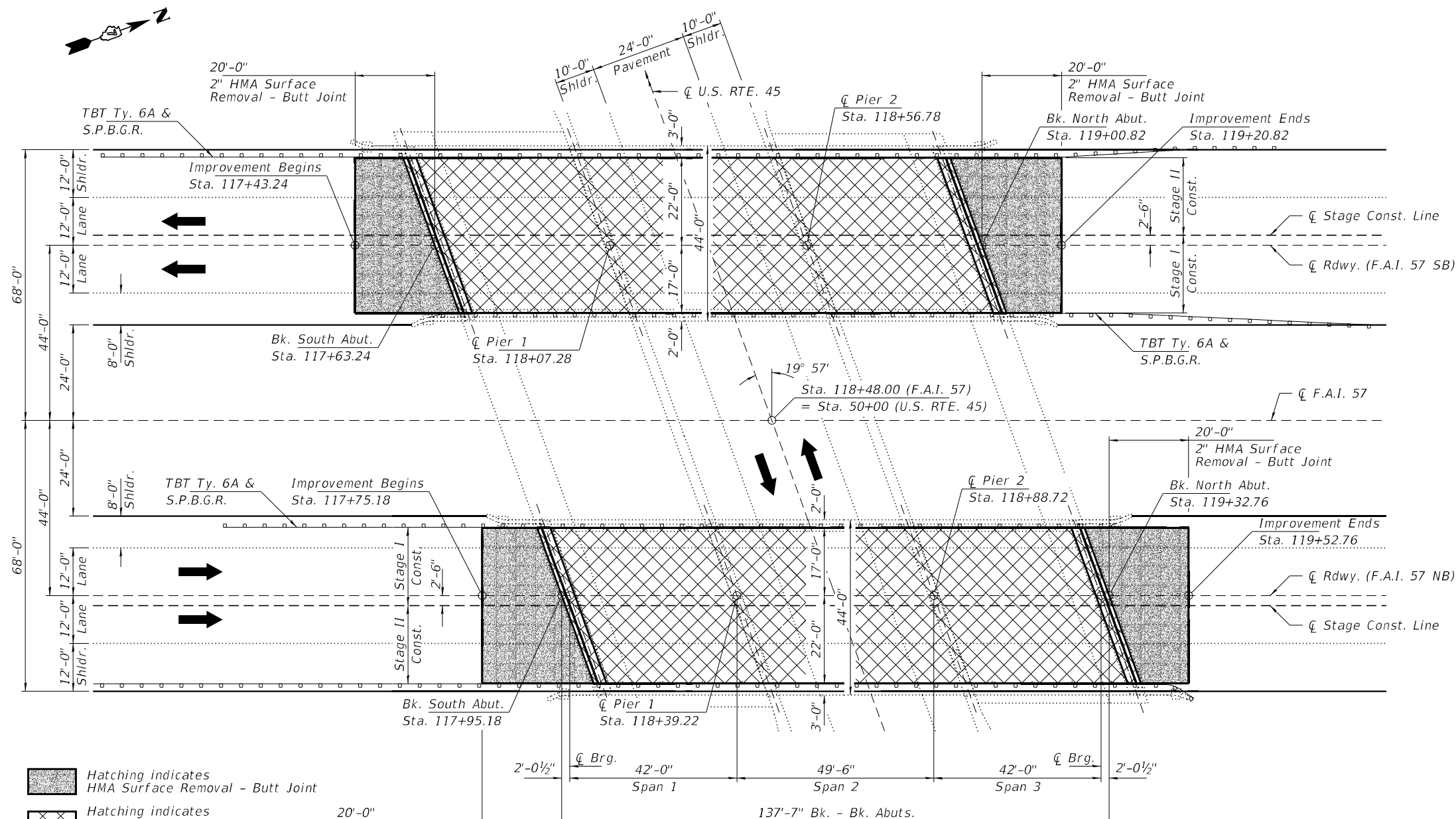
f'c = 1,400 psi (Super and Sub)  
fs = 20,000 psi (Reinf.)  
fs = 20,000 psi (Str. Steel)

**INDEX OF STRUCTURE SHEETS**

1. General Plan & Elevation
2. General Data
3. Stage Construction Details
4. Temporary Concrete Barrier For Stage Construction
5. Cross Section Staging Details SN 018-0003 (SB)
6. Cross Section Staging Details SN 018-0004 (NB)
7. Bridge Deck Patching SN 018-0003 (SB)
8. Bridge Deck Patching SN 018-0004 (NB)
9. Joint Removal & Replacement Details
10. Bar Splicer Assembly and Mechanical Splicer Details

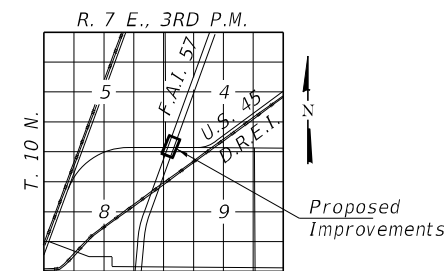


**ELEVATION**

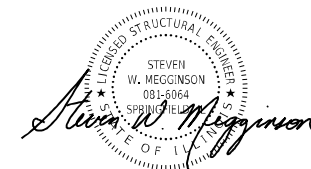


**PLAN**

- Hatching indicates HMA Surface Removal - Butt Joint
- Hatching indicates 2" HMA Surface Removal, or 2" HMA Surface Removal (Deck)
- Hatching indicates Concrete Removal



**LOCATION SKETCH**



Expires 11-30-2024

**GENERAL PLAN & ELEVATION**

**F.A.I. 57 OVER U.S. RTE. 45**

**D7 BRIDGE REPAIRS 2023-9**

**CUMBERLAND COUNTY**

**STATION 118+48.00**

**STRUCTURE NO. 018-0003 (SB)**

**& STRUCTURE NO. 018-0004 (NB)**

FILE NAME = 230269-3-4-shl-bridge.dgn	USER NAME = mlorenz	DESIGNED - S.T.M.	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL PLAN &amp; ELEVATION I-57 OVER US 45 - SN 018-0003 (SB) &amp; 018-0004 (NB)</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761 ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.009959	PLOT SCALE =	CHECKED - S.T.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	44	
PLOT DATE = 8/29/2023		DRAWN - G.D.M.	REVISED -			CONTRACT NO. 74C65					
		CHECKED - S.T.M./S.M.S.	REVISED -			ILLINOIS FED. AID PROJECT					



## GENERAL NOTES

No field welding is permitted except as specified in the contract documents. Reinforcement bars designated (E) shall be epoxy coated. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete (SSPC - SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4" deep shall be identified and reported to the Bureau of Bridges & Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Joint openings shall be adjusted according to article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

Plan dimensions and details relative to the existing structure have been taken from the existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to address the presence of lead on this project.

Areas of deck slab repair shown are estimated. The Engineer shall show actual locations of deck slab repairs on as-built plans.

Full depth deck slab repairs performed in the exterior bays of the bridge deck (between the parapet walls and the first interior beams) shall be limited to individual lengths no greater than 10'. In these portions of the deck, repair areas longer than 10' shall be divided into segments not greater than 10' in length, and the segments shall be poured in alternating sequence. Subsequent segments repaired in sequence shall not be removed until 72 hours shall have elapsed from the end of the previous, adjacent pour, and the adjacent pour shall have attained a minimum modulus of rupture of 650psi.

Protective Coat shall be applied to the top of the new concrete hatch block and concrete deck replacement and any other portion of Concrete Superstructure that is not covered by HMA surface course.

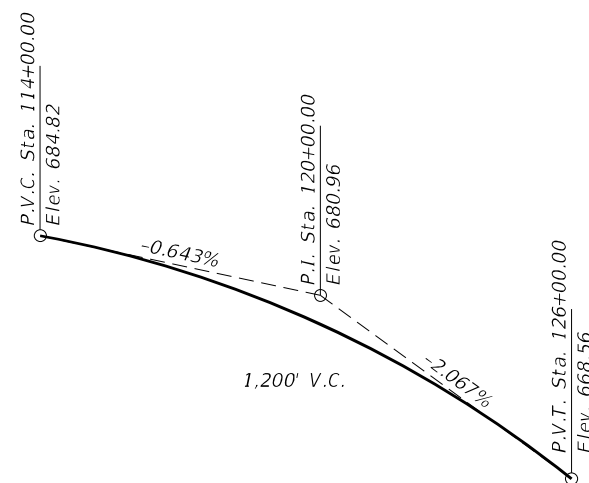
Traffic will be maintained using stage construction.

## SCOPE OF WORK

1. Removal of existing HMA deck surfacing and approach surface.
2. Removal of existing Neoprene Expansion Joints at abutments.
3. Full and partial depth deck patching repair.
4. Structural concrete repair at abutments and superstructure.
5. Resurfacing of approaches and deck.

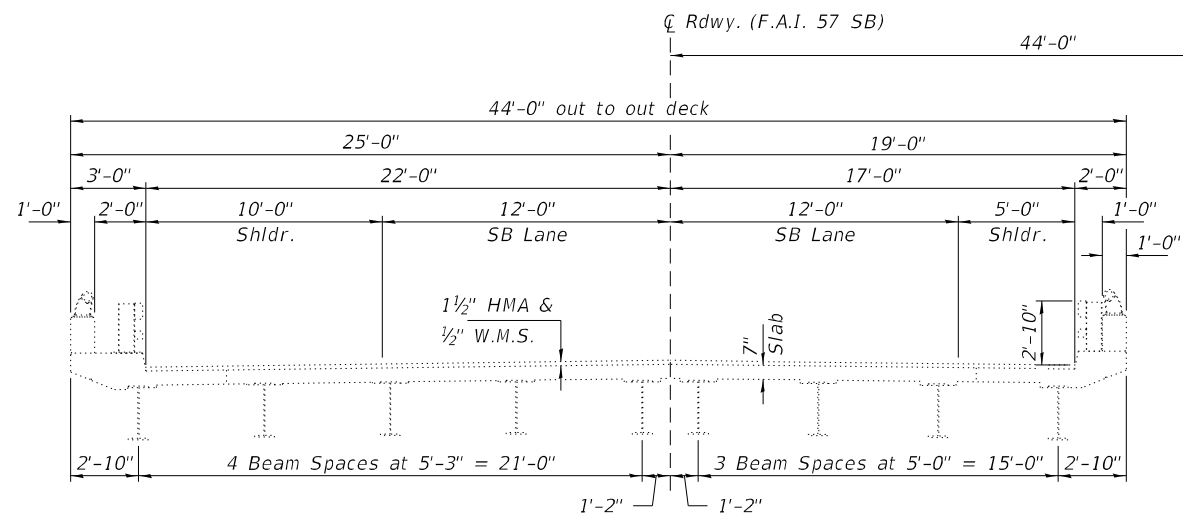
## BILL OF MATERIAL

Item	Unit	Quantity
Bituminous Materials (Tack Coat)	Pound	676
Hot-Mix Asphalt Surface Removal - Butt Joint	Sq. Yd.	363
Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90	Ton	168
Concrete Removal	Cu. Yd.	20.4
Protective Shield	Sq. Yd.	485
Concrete Superstructure	Cu. Yd.	25.0
Protective Coat	Sq. Yd.	55
Reinforcement Bars, Epoxy Coated	Pound	2,040
Bar Splicers	Each	48
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	1,192
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	34
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	15
Deck Slab Repair (Partial)	Sq. Yd.	55

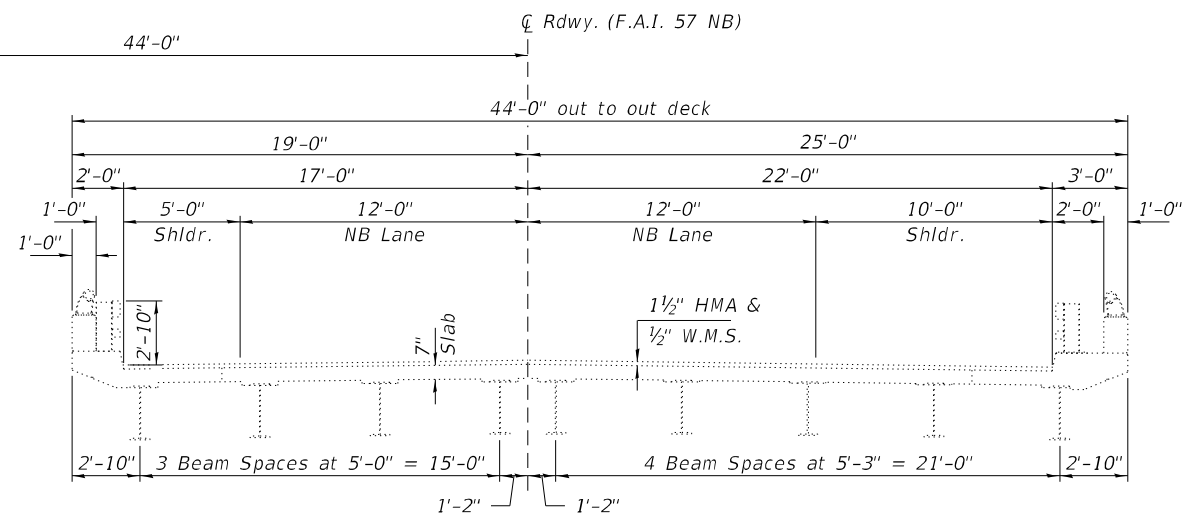


Note:  
P.G. elevations shown are approximate and are provided as information only. Proposed work shall be constructed to match existing elevations in the field.

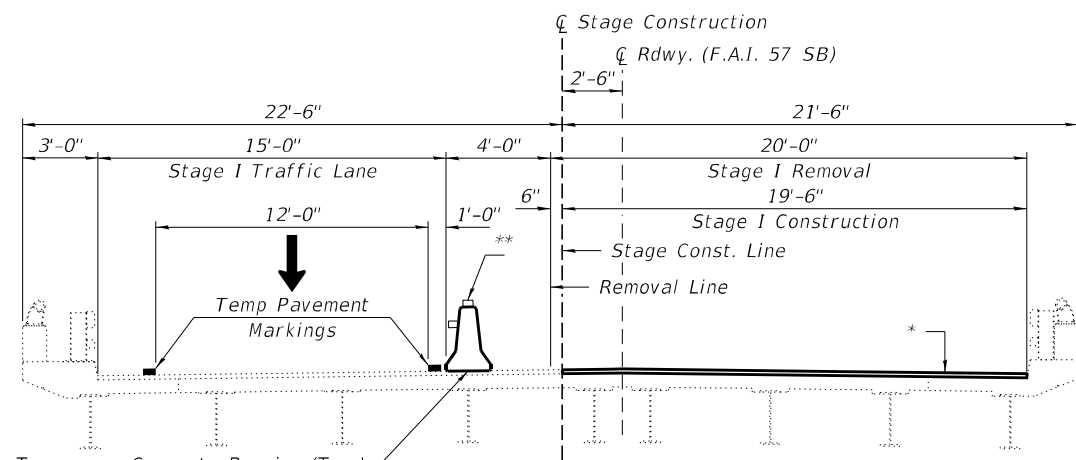
FILE NAME = 230269-3-4-shl-bridge.dgn	USER NAME = mlorenz	DESIGNED - S.T.M.	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL DATA I-57 OVER US 45 - SN 018-0003 (SB) &amp; 018-0004 (NB)</b>	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. <small>3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761</small>	CHECKED - S.T.M.	REVISED -	57			D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	45		
<b>HLR</b> <small>ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959</small>	PLOT SCALE =	DRAWN - G.D.M.	REVISED -			CONTRACT NO. 74C65					
	PLOT DATE = 8/29/2023	CHECKED - S.T.M./S.M.S.	REVISED -			SHEET NO. 2 OF 10 SHEETS					



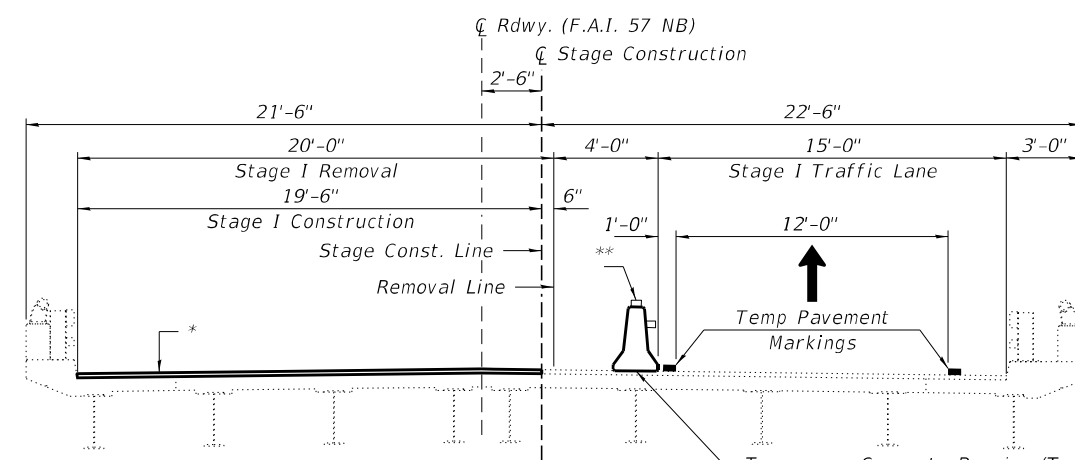
**EXISTING CROSS SECTION**  
(Looking North)



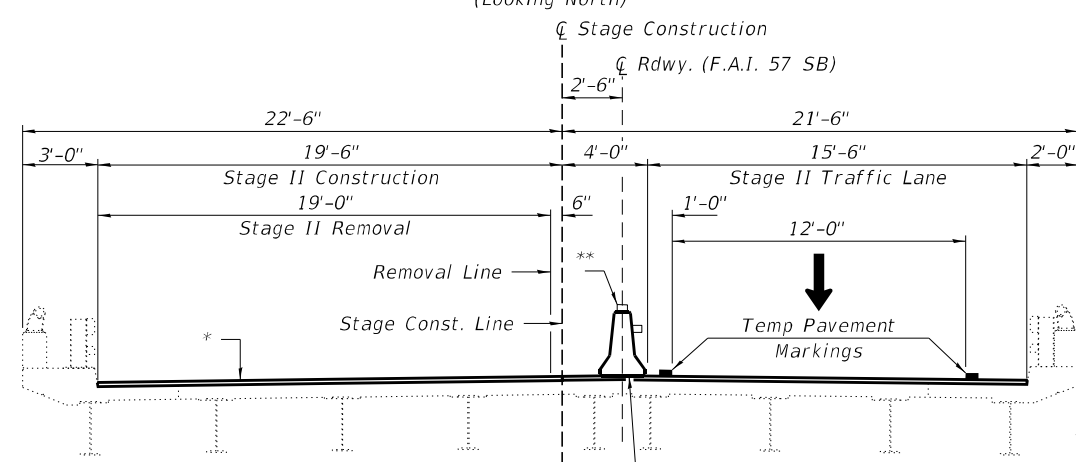
**EXISTING CROSS SECTION**  
(Looking North)



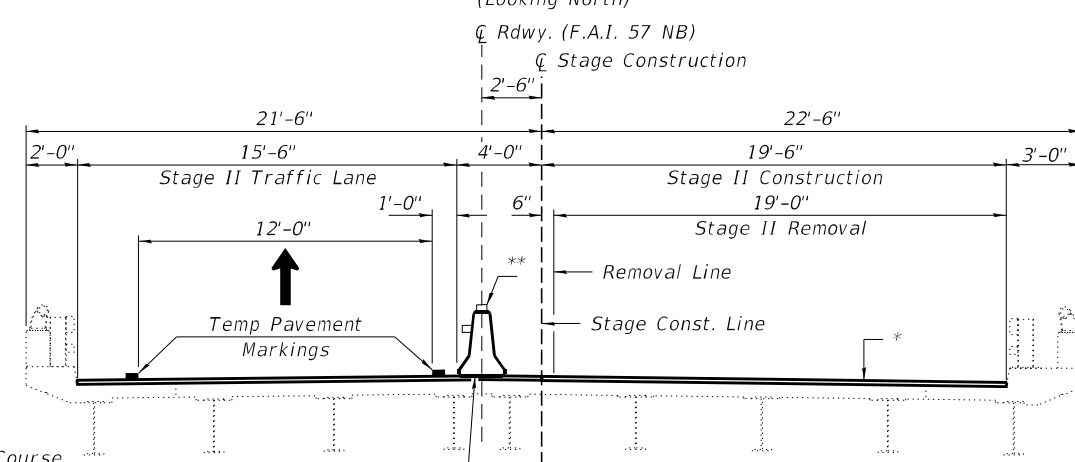
**STAGE I CONSTRUCTION**  
(Looking North)



**STAGE I CONSTRUCTION**  
(Looking North)



**STAGE II CONSTRUCTION**  
(Looking North)



**STAGE II CONSTRUCTION**  
(Looking North)

**S.N. 018-0003 SOUTH BOUND LANES**

**S.N. 018-0004 NORTH BOUND LANES**

\*2" Hot-Mix Asphalt Surface Removal (Deck)  
Replace with 2" Polymerized Hot-Mix Asphalt Surface Course,  
IL-9.5, Mix "D", N90.

\*\* TCB with Type C reflectors as noted in Standard 704001  
and 782006

Notes:  
See Roadway plans for Maintenance of Traffic Details.  
See Sheet 4 of 10 for details of Temporary Concrete Barrier.  
See Structure Sheet 9 of 10 for Concrete Removal and Proposed  
Construction details.

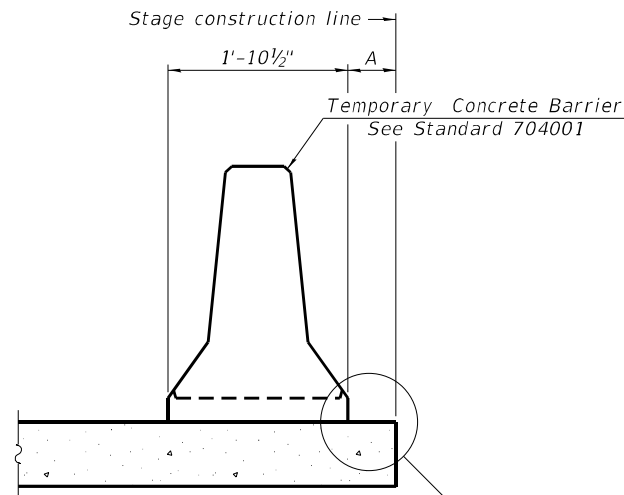
FILE NAME = 230269-3-4-shl-bridge.dgn	USER NAME = mlorenz	DESIGNED - S.T.M.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62763	PLOT SCALE =	CHECKED - S.T.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISED -
		CHECKED - S.T.M./S.M.S.	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS  
I-57 OVER US 45 - SN 018-0003 (SB) & 018-0004 (NB)**

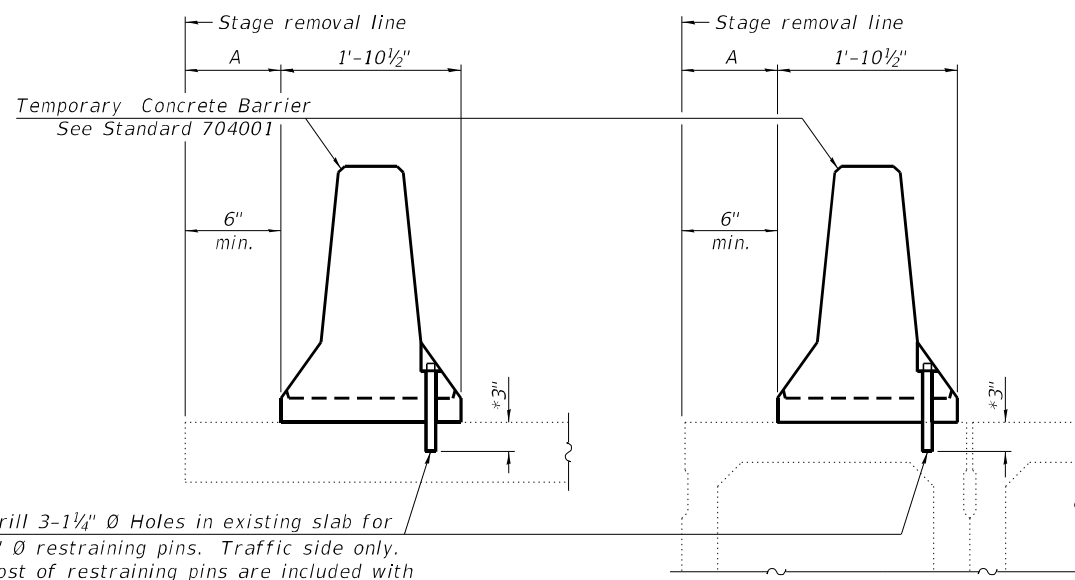
SHEET NO. 3 OF 10 SHEETS

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	46
CONTRACT NO. 74C65				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

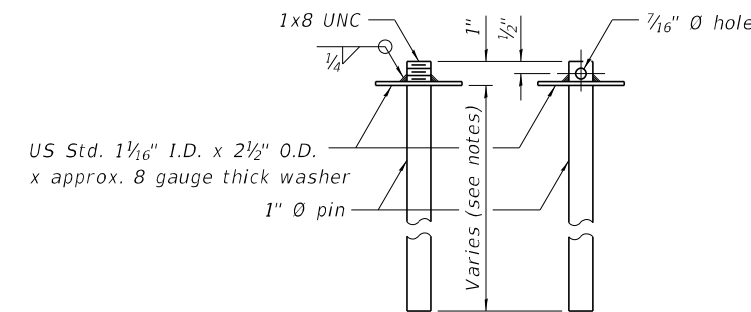


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

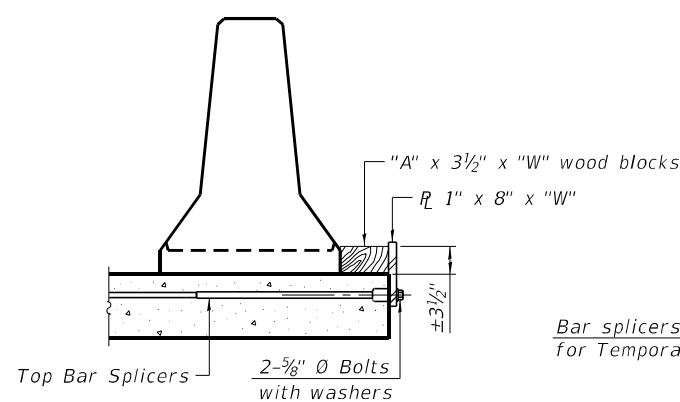
\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

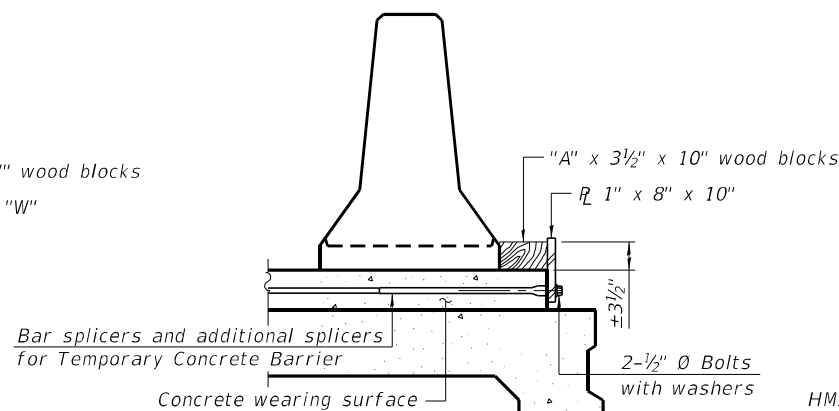


RESTRAINING PIN

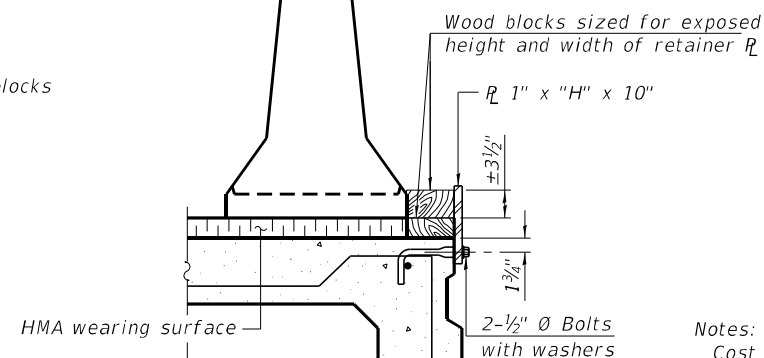
SECTIONS THRU SLAB OR DECK BEAM



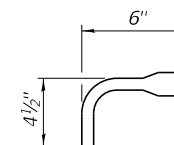
DETAIL I



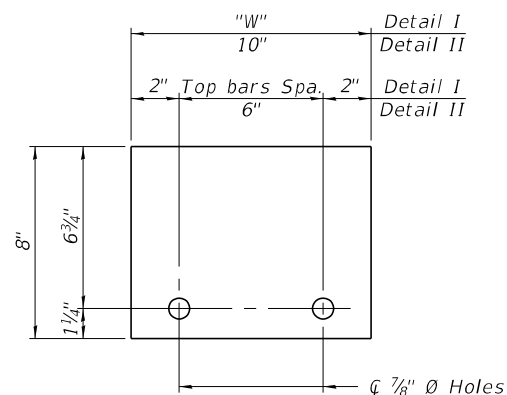
DETAIL II



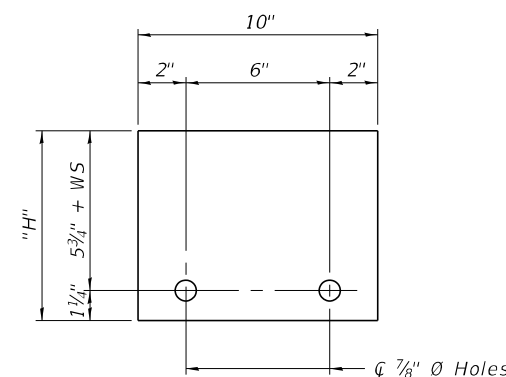
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"  
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"  
(Detail III)

Notes:  
 Cost of retainer assembly is included with Temporary Concrete Barrier.  
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.  
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate.  
 For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

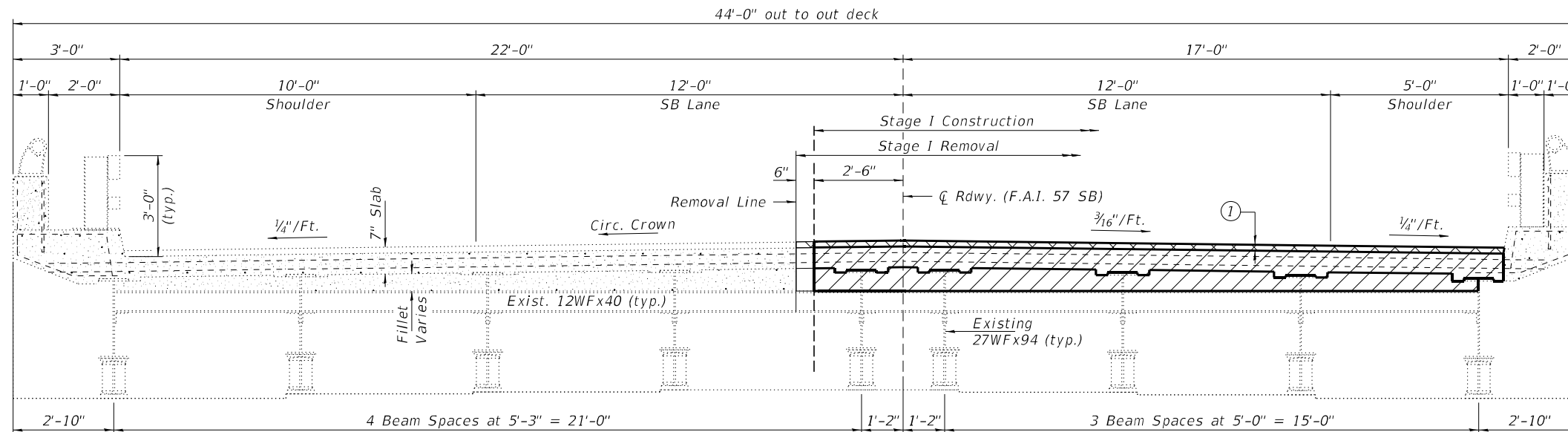
- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

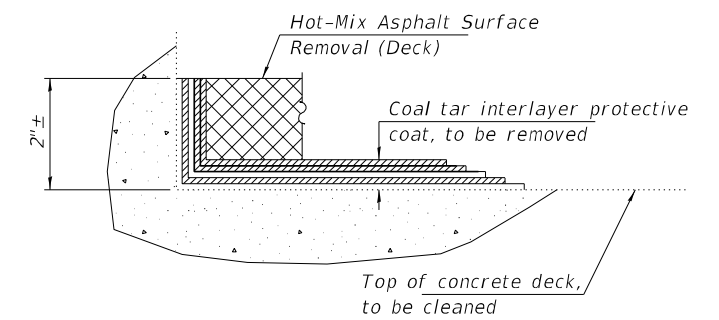
R-27 10-12-2021

FILE NAME = 230269-3-4-shl-bridge.dgn	USER NAME = mlrentz	DESIGNED - S.T.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION I-57 OVER US 45 - SN 018-0003 (SB) & 018-0004 (NB)	F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761	PLOT SCALE =	CHECKED - S.T.M.	REVISED -			57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	47	
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959	PLOT DATE = 8/29/2023	DRAWN - G.D.M.	REVISED -			CONTRACT NO. 74C65					
		CHECKED - S.T.M./S.M.S.	REVISED -			SHEET NO. 4 OF 10 SHEETS					

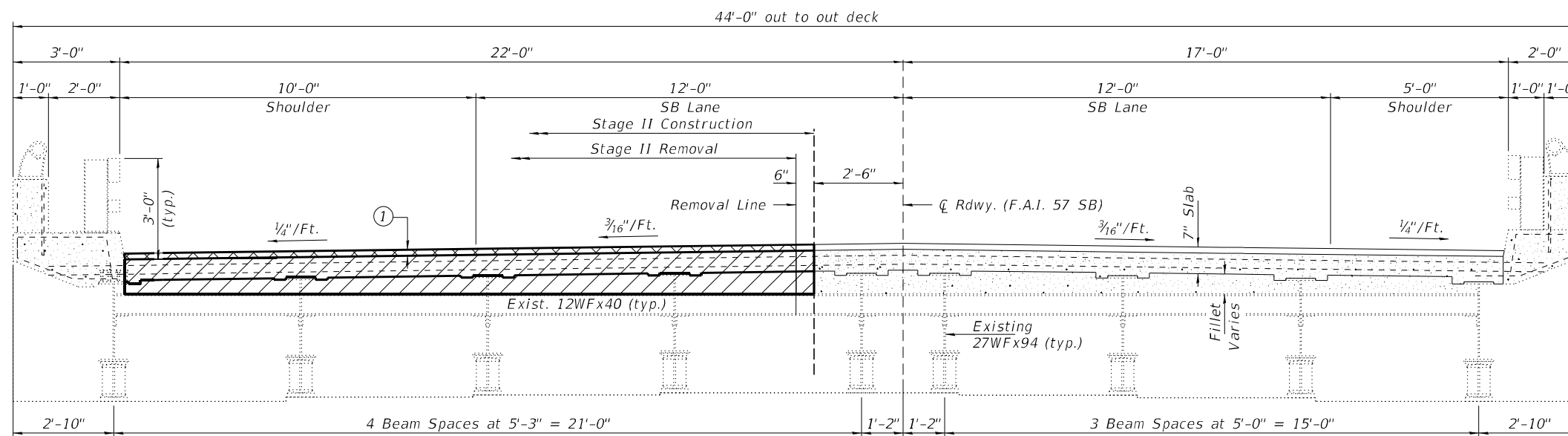


**STAGE I - CROSS SECTION AT ABUTMENTS**

(Looking North)

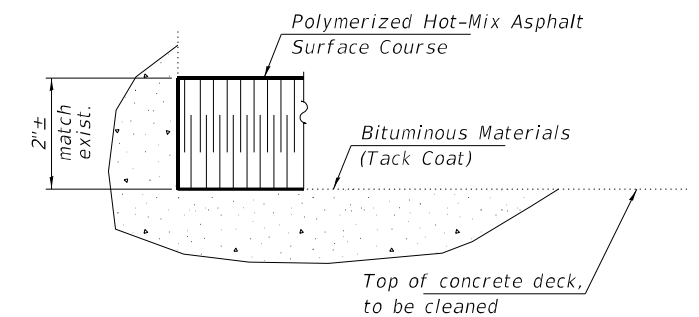


**EXISTING DECK SURFACING**



**STAGE II - CROSS SECTION AT ABUTMENTS**

(Looking North)



**PROPOSED DECK SURFACING**

**BILL OF MATERIAL**

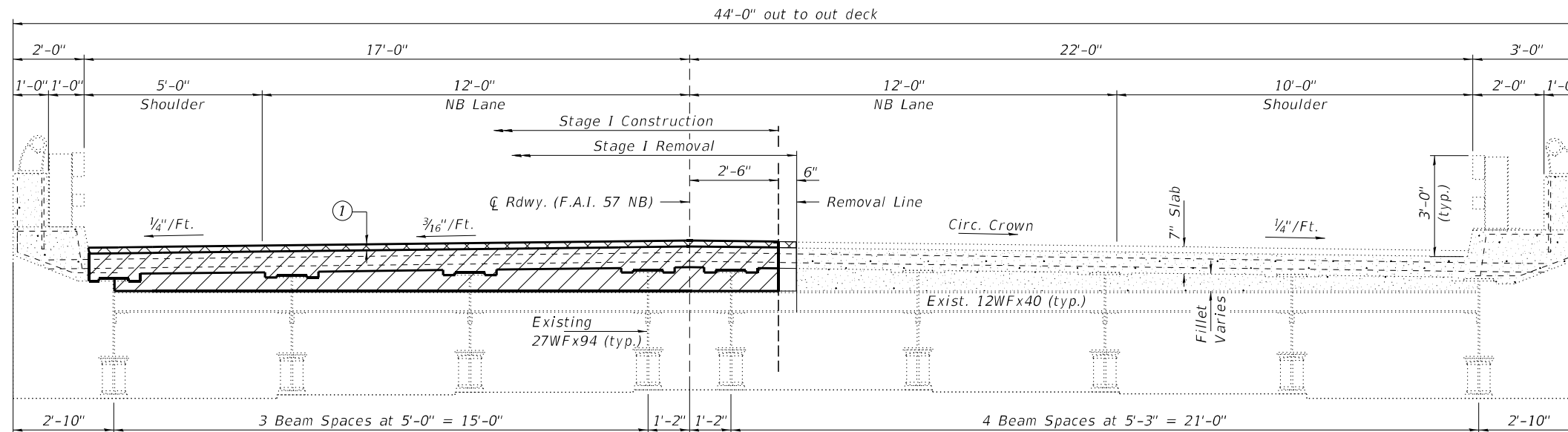
Item	Unit	Quantity
Bituminous Materials (Tack Coat)	Pound	338
Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90	Ton	84
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	596

**Notes:**

A tack coat, according to Section 406 of the Standard Specifications, with a residual asphalt rate of 0.05 lb./sq. ft. shall be applied to the top of the concrete deck before the hot-mix asphalt surfacing is placed. Prior to applying the tack coat, the top of the existing concrete deck shall be cleaned according to Article 406.05(b)(1) of the Standard Specifications. The quantity for Polymerized Hot-Mix Asphalt Surface Course is calculated based on 2" uniform thickness and a placement factor of 112 lb./sq. yd./inch. See Sheet 9 of 10 for removal and replacement of concrete details.

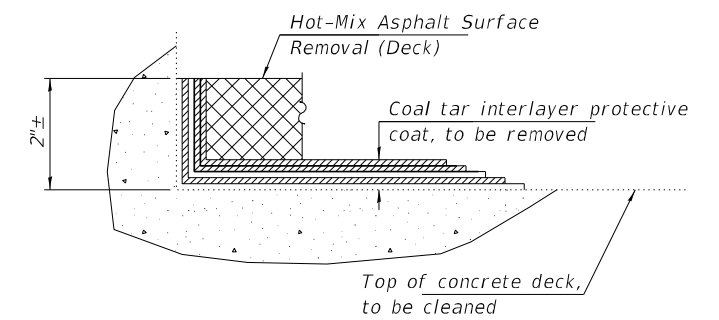
- Hatching indicates 2" HMA Surface Removal (Deck)
- Hatching indicates Concrete Removal

- ① 2" Hot-Mix Asphalt Surface Removal (Deck)
- ② 2" Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90 (Throughout Deck) or Concrete Superstructure at Joint Replacement.

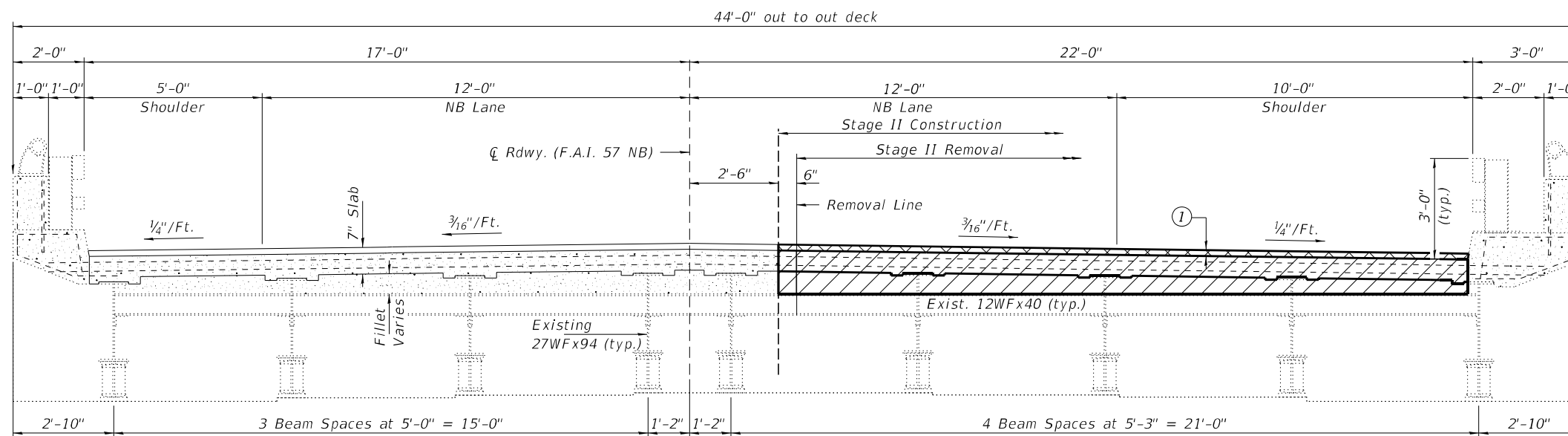


**STAGE I - CROSS SECTION AT ABUTMENTS**

(Looking North)

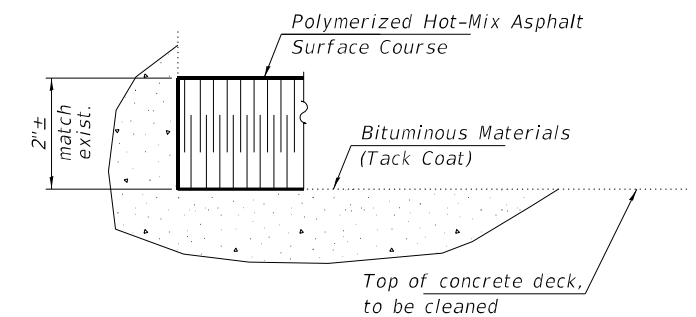


**EXISTING DECK SURFACING**



**STAGE II - CROSS SECTION AT ABUTMENTS**

(Looking North)



**PROPOSED DECK SURFACING**

**BILL OF MATERIAL**

Item	Unit	Quantity
Bituminous Materials (Tack Coat)	Pound	338
Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90	Ton	84
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	596

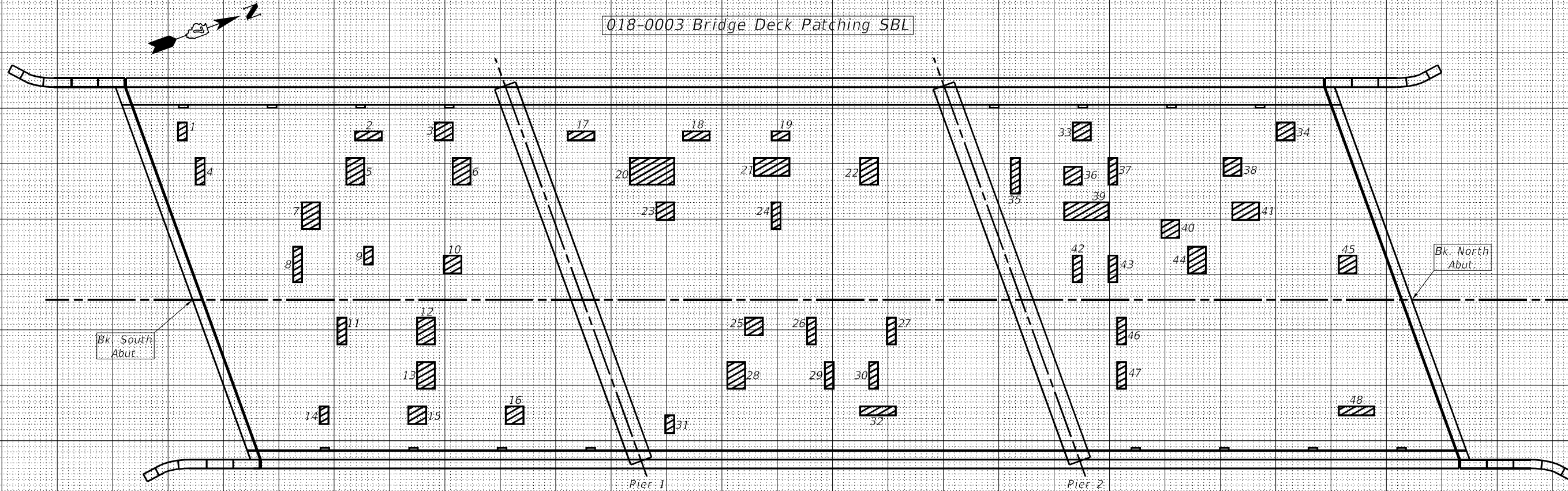
**Notes:**

A tack coat, according to Section 406 of the Standard Specifications, with a residual asphalt rate of 0.05 lb./sq. ft. shall be applied to the top of the concrete deck before the hot-mix asphalt surfacing is placed.  
Prior to applying the tack coat, the top of the existing concrete deck shall be cleaned according to Article 406.05(b)(1) of the Standard Specifications.  
The quantity for Polymerized Hot-Mix Asphalt Surface Course is calculated based on 2" uniform thickness and a placement factor of 112 lb./sq. yd./inch. See Sheet 9 of 10 for removal and replacement of concrete details.

- Hatching indicates 2" HMA Surface Removal & Replacement, or 2" HMA Surface Removal & Replacement (Deck)
- Hatching indicates Concrete Removal

- ① 2" Hot-Mix Asphalt Surface Removal (Deck)
- ② 2" Polymerized Hot-Mix Asphalt Surface Course, IL-9.5, Mix "D", N90 (Throughout Deck) or Concrete Superstructure at Joint Replacement.

018-0003 Bridge Deck Patching SBL



PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
1	1.0	2.0	0.2	
2	3.0	1.0	0.3	
3	2.0	2.0	0.4	
4	1.0	3.0	0.3	
5	2.0	3.0		0.7
6	2.0	3.0		0.7
7	2.0	3.0		0.7
8	1.0	4.0	0.4	
9	1.0	2.0	0.2	
10	2.0	2.0	0.4	
11	1.0	3.0	0.3	
12	2.0	3.0		0.7
13	2.0	3.0		0.7
14	1.0	2.0	0.2	
15	2.0	2.0	0.4	

PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
16	2.0	2.0	0.4	
17	1.0	3.0	0.3	
18	1.0	3.0	0.3	
19	1.0	2.0	0.2	
20	5.0	3.0		1.7
21	4.0	2.0		0.9
22	2.0	3.0		0.7
23	2.0	2.0	0.4	
24	1.0	3.0	0.3	
25	2.0	2.0	0.4	
26	1.0	3.0	0.3	
27	1.0	3.0	0.3	
28	2.0	3.0		0.7
29	1.0	3.0	0.3	
30	1.0	3.0	0.3	

PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
31	1.0	2.0	0.2	
32	4.0	1.0	0.4	
33	2.0	2.0	0.4	
34	2.0	2.0	0.4	
35	1.0	4.0	0.4	
36	2.0	2.0	0.4	
37	1.0	3.0	0.3	
38	2.0	2.0	0.4	
39	5.0	2.0		1.1
40	2.0	2.0	0.4	
41	3.0	2.0		0.7
42	1.0	3.0	0.3	
43	1.0	3.0	0.3	
44	2.0	3.0		0.7
45	2.0	2.0	0.4	

PATCH NO.	LENGTH	WIDTH	DECK SLAB REPAIR (FD TY I) SQ YD	DECK SLAB REPAIR (FD TY II) SQ YD
46	1.0	3.0	0.3	
47	1.0	3.0	0.3	
48	4.0	1.0	0.4	
TOTAL ROUNDS TO:			14.0	10.0

Deck Slab Repair Notes for SN 018-0003

The southbound structures are planned to be removed first, during Stage I construction of an upcoming bridge replacement project. The deck slab repairs included on this structure are intended to repair the most severely deteriorated areas, to stabilize the bridge deck until the upcoming replacement project.

On the southbound structures only, deck slab repairs within the existing traffic lanes should be prioritized over repairs to existing shoulders.

The following quantity was estimated from overall deck condition:  
Deck Slab Repair (Partial Depth) = 25 Sq Yd

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
SURVEY BY: DPM & TMW  
METHOD OF SURVEY: VISUAL

ESTIMATED  
PAY QUANTITIES:

DECK SLAB REPAIR (FULL DEPTH TYPE I)  
14.0 SQ YD

DECK SLAB REPAIR (FULL DEPTH TYPE II)  
10.0 SQ YD

DECK SLAB REPAIR (PARTIAL DEPTH)  
25 SQ YD

BRIDGE DECK PATCHING (SHEET 1 OF 2)

MODEL: SMOBEL NAMES  
FILE NAME: 230205-3-18-018-0003.dwg

USER NAME = mllorenz	DESIGNED - T. Walk	REVISED -
	DRAWN - T. Walk	REVISED -
PLOT SCALE = \$SCALE5	CHECKED - D. Macklin	REVISED -
PLOT DATE = 8/29/2023	DATE - March 2023	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

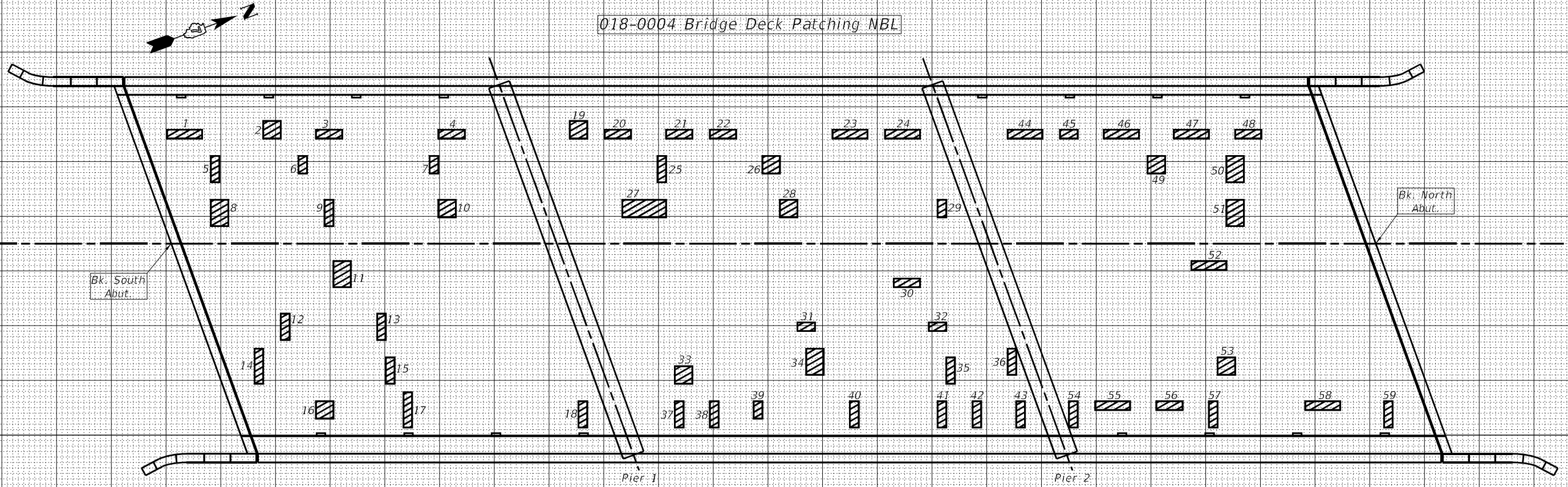
BRIDGE DECK PATCHING  
SN 018-0003 (SB)

SHEET NO. 7 OF 10 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	50
CONTRACT NO. 74C65				

ILLINOIS FED. AID PROJECT

018-0004 Bridge Deck Patching NBL



PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
1	1.0	4.0	0.4	
2	2.0	2.0	0.4	
3	3.0	1.0	0.3	
4	3.0	1.0	0.3	
5	1.0	3.0	0.3	
6	1.0	2.0	0.2	
7	1.0	2.0	0.2	
8	2.0	3.0		0.7
9	1.0	3.0	0.3	
10	2.0	2.0	0.4	
11	2.0	3.0		0.7
12	1.0	3.0	0.3	
13	1.0	3.0	0.3	
14	1.0	4.0	0.4	
15	1.0	3.0	0.3	
16	2.0	2.0	0.4	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
17	1.0	4.0	0.4	
18	1.0	3.0	0.3	
19	2.0	2.0	0.4	
20	3.0	1.0	0.3	
21	3.0	1.0	0.3	
22	3.0	1.0	0.3	
23	4.0	1.0	0.4	
24	4.0	1.0	0.4	
25	1.0	3.0	0.3	
26	2.0	2.0	0.4	
27	5.0	2.0		1.1
28	2.0	2.0	0.4	
29	1.0	2.0	0.2	
30	3.0	1.0	0.3	
31	2.0	1.0	0.2	
32	2.0	1.0	0.2	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
33	2.0	2.0	0.4	
34	2.0	3.0		0.7
35	1.0	3.0	0.3	
36	1.0	3.0	0.3	
37	1.0	3.0	0.3	
38	1.0	3.0	0.3	
39	1.0	2.0	0.2	
40	1.0	3.0	0.3	
41	1.0	3.0	0.3	
42	1.0	3.0	0.3	
43	1.0	3.0	0.3	
44	4.0	1.0	0.4	
45	2.0	1.0	0.2	
46	4.0	1.0	0.4	
47	4.0	1.0	0.4	
48	3.0	1.0	0.3	

PATCH NO.	SIZE		DECK SLAB REPAIR (FD TY I)	DECK SLAB REPAIR (FD TY II)
	LENGTH	WIDTH	SQ YD	SQ YD
49	2.0	2.0	0.4	
50	2.0	3.0		0.7
51	2.0	3.0		0.7
52	4.0	1.0	0.4	
53	2.0	2.0	0.4	
54	1.0	3.0	0.3	
55	4.0	1.0	0.4	
56	3.0	1.0	0.3	
57	1.0	3.0	0.3	
58	4.0	1.0	0.4	
59	1.0	3.0	0.3	
TOTAL ROUNDS TO:			20.0	5.0

Deck Slab Repair Notes for SN 018-0004

The northbound structures are planned to carry Stage I traffic during an upcoming bridge replacement project. The deck slab repairs included on this structure are intended to stabilize the bridge deck until the upcoming replacement project and during Stage I construction of the upcoming project.

On the northbound structures only, deck slab repairs within the traffic lanes and on the shoulders should be prioritized equally. During the upcoming replacement project, the traffic pattern on the existing northbound bridges will be altered for maintenance of traffic during Stage I construction.

The following quantity was estimated from overall deck condition: Deck Slab Repair (Partial Depth) = 30 Sq Yd

THE LOCATIONS AND SIZES SHOWN GRAPHICALLY ABOVE ARE APPROXIMATE. SEE THIS TABLE FOR ACTUAL SIZES.



DATE OF SURVEY: 4-10-23  
SURVEY BY: DPM & TMW  
METHOD OF SURVEY: VISUAL

ESTIMATED PAY QUANTITIES:

DECK SLAB REPAIR (FULL DEPTH TYPE I)  
20.0 SQ YD

DECK SLAB REPAIR (FULL DEPTH TYPE II)  
5.0 SQ YD

DECK SLAB REPAIR (PARTIAL DEPTH)  
30 SQ YD

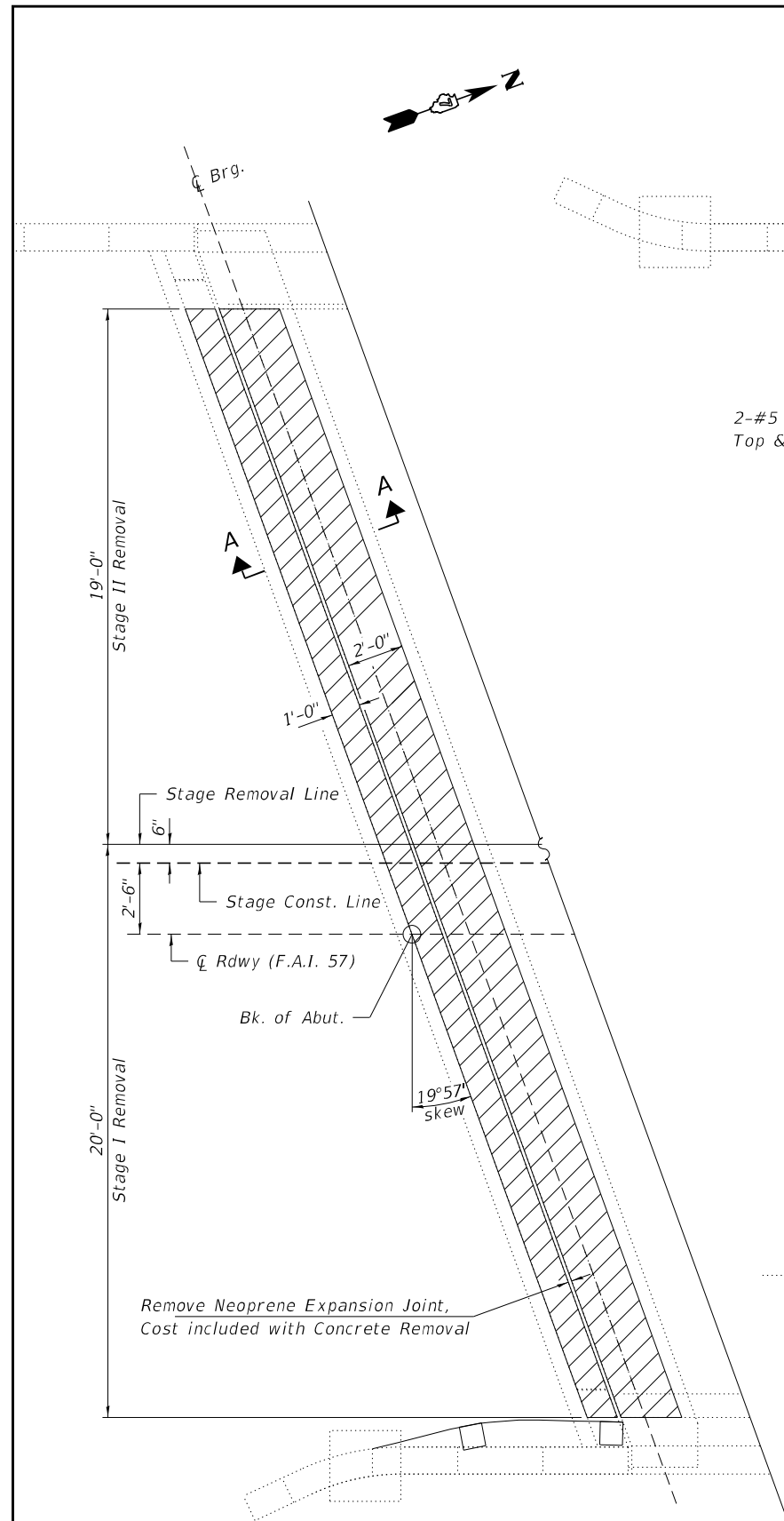
MODEL: SMOBEL NAMES  
FILE NAME: 230205-3-18-018-0004.dgn

USER NAME = mllorenz	DESIGNED - T. Walk	REVISED -
PLOT SCALE = \$SCALE5	DRAWN - T. Walk	REVISED -
PLOT DATE = 8/29/2023	CHECKED - D. Macklin	REVISED -
	DATE - March 2023	REVISED -

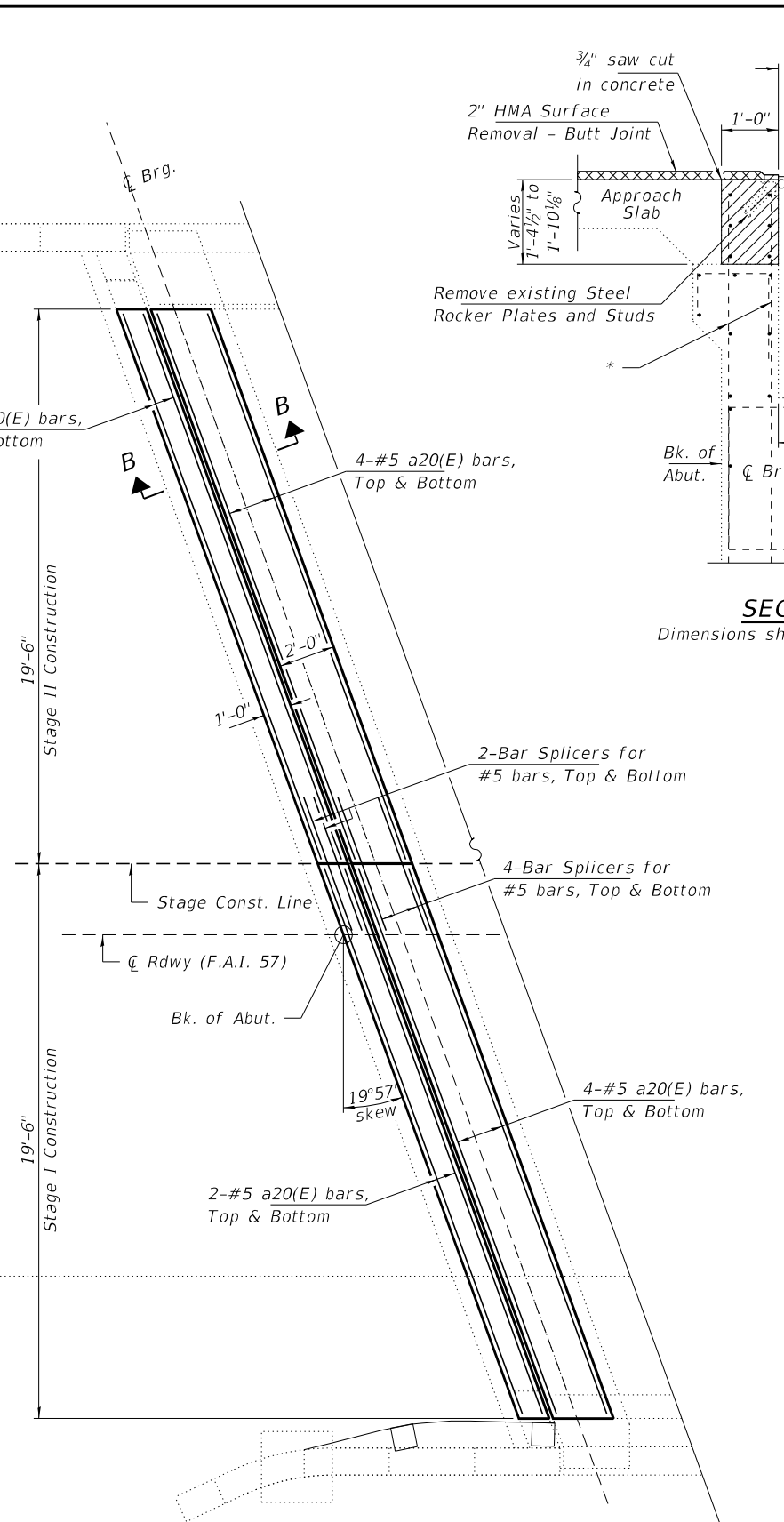
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BRIDGE DECK PATCHING  
SN 018-0004 (NB)

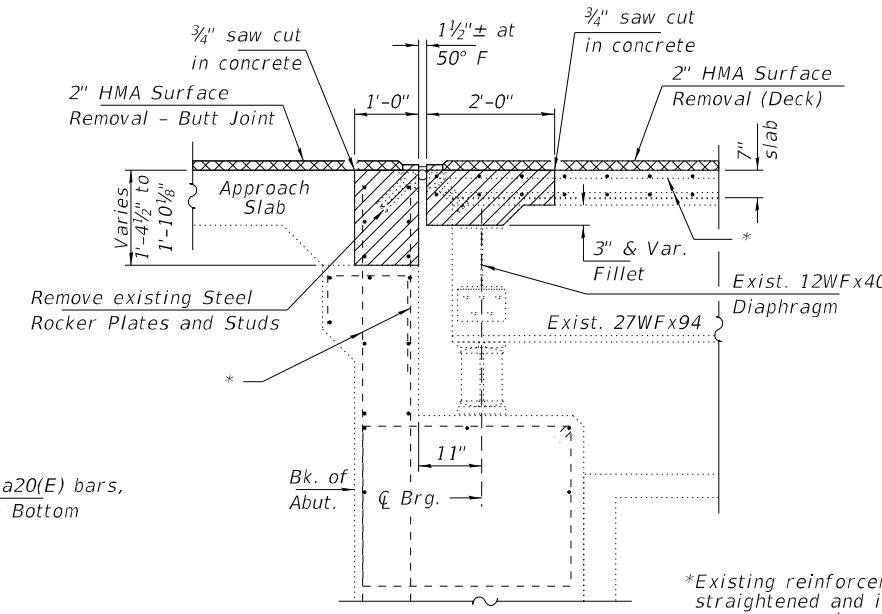
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	51
CONTRACT NO. 74C65				



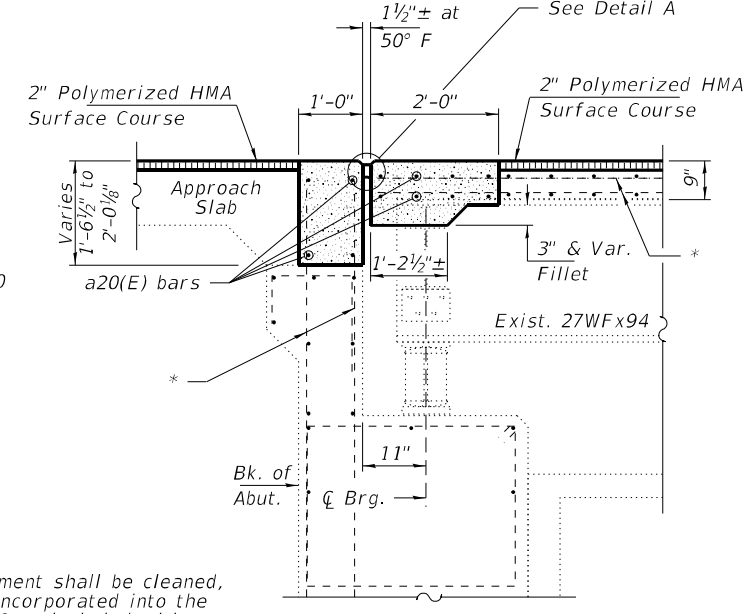
**REMOVAL PLAN**  
S. Abut. Shown, N. Abut. Similar (SN 018-0003 SB)  
SN 018-0004 NB Similar



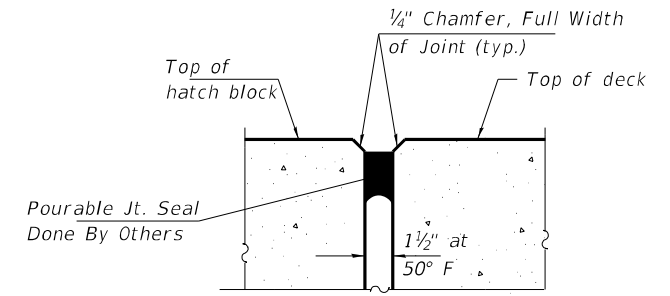
**REPLACEMENT PLAN**  
S. Abut. Shown, N. Abut. Similar (SN 018-0003 SB)  
SN 018-0004 NB Similar



**SECTION A-A**  
Dimensions shown are at right angles



**SECTION B-B**  
Dimensions shown are at right angles



**DETAIL A**

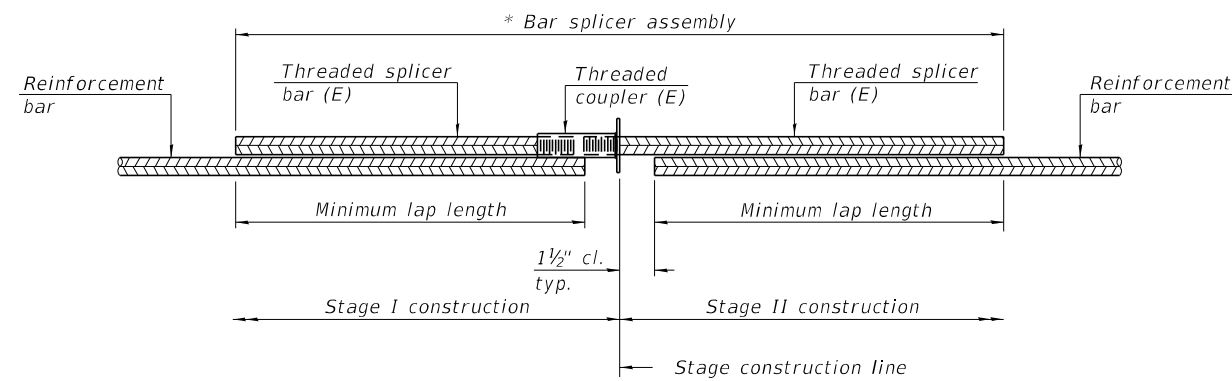
- Hatching indicates HMA Removal
- Hatching indicates Concrete Removal

**BILL OF MATERIAL**

BAR	NO.	SIZE	LENGTH	SHAPE
a20(E)	96	#5	20'-5"	—
Concrete Removal			Cu. Yd.	20.4
Concrete Superstructure			Cu. Yd.	25.0
Protective Coat			Sq. Yd.	55
Reinforcement Bars, Epoxy Coated			Pound	2,040
Bar Splicers			Each	48

Notes:  
Removal and disposal of the existing Neoprene Expansion Joint and other materials that fall within the limits of the Concrete Removal, will not be paid for separately, but shall be included in the cost of Concrete Removal.  
Existing reinforcement bars extending into the removal area shall be cleaned, straightened, and incorporated into the new construction. Any reinforcement bars that are damaged during Concrete Removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.  
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.  
Protective Coat shall be applied to the top of the new concrete hatch block and concrete deck replacement.



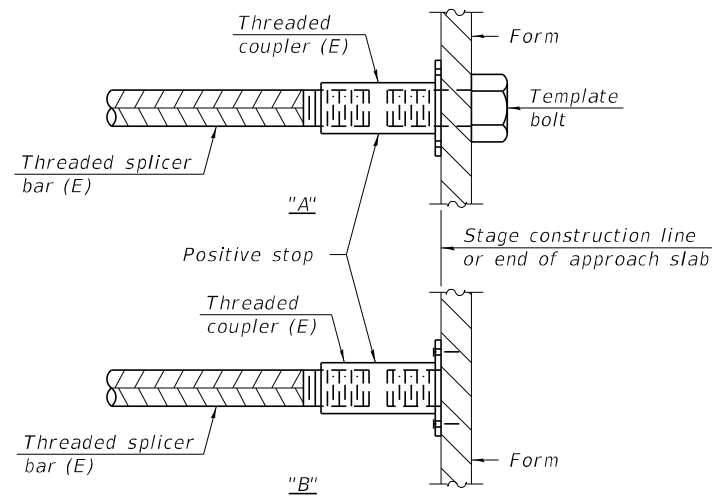


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

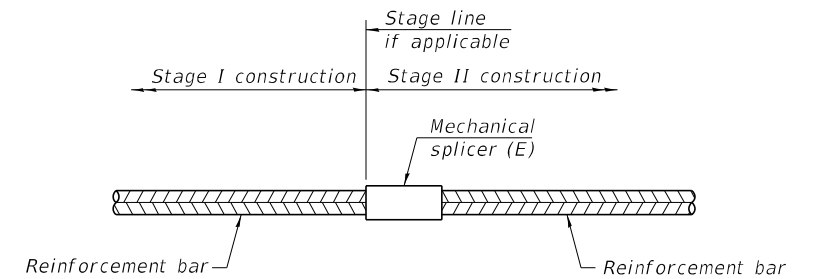
\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Hatch Block	#5	16	3'-6"
Deck	#5	32	3'-6"



**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required

Notes:  
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.  
 All reinforcement shall be lapped and tied to the splicer bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.  
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

1-1-2020

FILE NAME = 230269-3-4-shl-bridge.dgn	USER NAME = mlrentz	DESIGNED - S.T.M.	REVISED -
HAMPTON, LENZINI AND RENWICK, INC. 3335 STEVENSON DRIVE, SUITE 201 SPRINGFIELD, ILLINOIS 62761		CHECKED - S.T.M.	REVISED -
ILLINOIS PROFESSIONAL DESIGN FIRM LS / PE / SE CORP. 184.002959		DRAWN - G.D.M.	REVISED -
	PLOT SCALE =	CHECKED - S.T.M./S.M.S.	REVISED -
	PLOT DATE = 8/29/2023		

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
 I-57 OVER US 45 - SN 018-0003 (SB) & 018-0004 (NB)

F.A.I.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	D7 BRIDGE REPAIRS 2023-9	CUMBERLAND	53	53
CONTRACT NO. 74C65				
ILLINOIS   FED. AID PROJECT				

SHEET NO. 10 OF 10 SHEETS