LETTING ITEM NO. 04A
NOVEMBER 19, 2023 IDOT LETTING

KA052
TOTAL SHEETS = 38

KANKAKEE VALLEY AIRPORT AUTHORITY
KANKAKEE, ILLINOIS

CONSTRUCTION PLANS
FOR
GREATER KANKAKEE AIRPORT

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

ILLINOIS PROJECT: IKK-4991

SEPTEMBER 22, 2023

811 Know what's **below.** **Call** before you dig.

J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

LICENSED PROFESSIONAL ENGINEER
DAVID K. PEABODY
062-048437
STATE OF ILLINOIS

D Kyle Peabody

LICENSE EXPIRATION
DATE: 11/30/2023
DATE SIGNED: 09/20/2023

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21001660-00

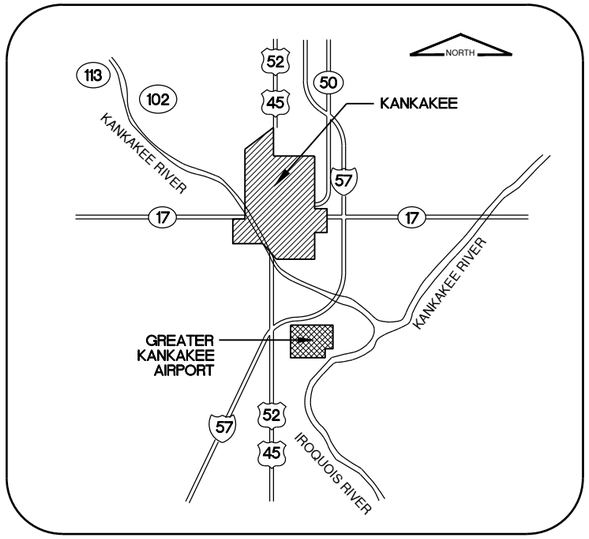
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

SUBMITTED BY D Kyle Peabody
D. KYLE PEABODY, P.E.

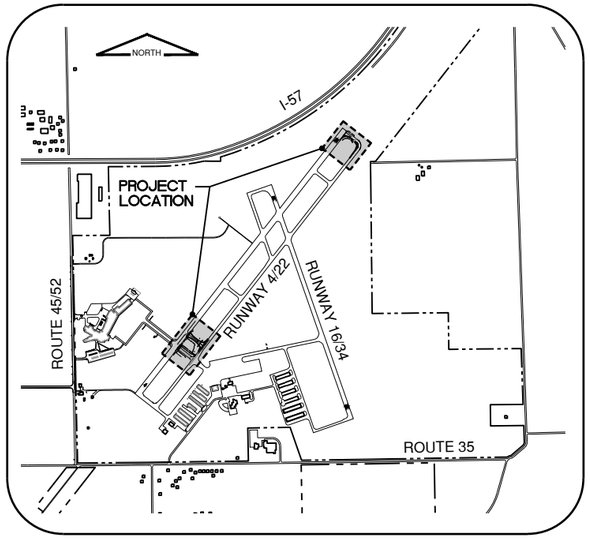
DATE 09/20/2023

APPROVED BY
JEFF BENOIT - AIRPORT MANAGER

DATE 9-21-23



LOCATION MAP



SITE PLAN

RUNWAY 4/22
DESIGN AIRCRAFT APPROACH CATEGORY D

RUNWAY 16/34
DESIGN AIRCRAFT APPROACH CATEGORY B

TAXIWAY A, A2, B, A3, A4, A5, D, H
TAXIWAY DESIGN GROUP 2A AND 2B

KANKAKEE VALLEY AIRPORTY AUTHORITY
GREATER KANKAKEE AIRPORT

SECTION: 21
RANGE: R 12 E
TOWNSHIP: T 30 N

COUNTY: KANKAKEE
TOWNSHIP: OTTO

UNICOM RADIO FREQUENCY - 123.0

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SUMMARY OF QUANTITIES

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR108158	1/C #8 5 KV UG CABLE IN UD	FOOT	5,600	
AR108960	REMOVE CABLE	FOOT	7,000	
AR110502	2-WAY CONCRETE ENCASED DUCT	FOOT	100	
AR110504	4-WAY CONCRETE ENCASED DUCT	FOOT	110	
AR110551	EXTEND DUCT	FOOT	15	
AR110610	ELECTRICAL HANDHOLE	EACH	3	
AR110900	REMOVE DUCT	FOOT	120	
AR125410	MITL-STAKE MOUNTED	EACH	57	
AR125415	MITL-BASE MOUNTED	EACH	8	
AR125443	TAXI GUIDANCE SIGN, 3 CHARACTER	EACH	3	
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	1	
AR125565	SPLICE CAN	EACH	1	
AR125901	REMOVE STAKE MOUNTED LIGHT	EACH	39	
AR125902	REMOVE BASE MOUNTED LIGHT	EACH	4	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	3	
AR125906	REMOVE SPLICE CAN	EACH	2	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	1	
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	7,850	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	3,575	
AR156510	SILT FENCE	FOOT	1,250	
AR156520	INLET PROTECTION	EACH	4	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	1,400	
AR209612	CRUSHED AGG. BASE COURSE - 12"	SQ YD	715	
AR800026	CRUSHED AGG. BASE COURSE - 15"	SQ YD	2,700	
AR401610	BITUMINOUS SURFACE COURSE	TON	780	
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	2,250	
AR403610	BITUMINOUS BASE COURSE	TON	970	
AR602510	BITUMINOUS PRIME COAT	GALLON	1,000	
AR603510	BITUMINOUS TACK COAT	GALLON	700	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	3,550	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	2,950	
AR620900	PAVEMENT MARKING REMOVAL	SQ FT	4,950	
AR701524	24" RCP, CLASS IV	FOOT	540	
AR701900	REMOVE PIPE	FOOT	560	
AR705526	6" PERFORATED UNDERDRAIN W/SOCK	FOOT	780	
AR751540	4' MANHOLE	EACH	2	
AR751560	6' MANHOLE	EACH	1	
AR751900	REMOVE INLET	EACH	1	
AR800003	2 - 1/C #8 5KV UG CABLE IN UD	FOOT	365	
AR901510	SEEDING	ACRE	4.5	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	4.5	

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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INDEX TO SHEETS/ SUMMARY OF QUANTITIES

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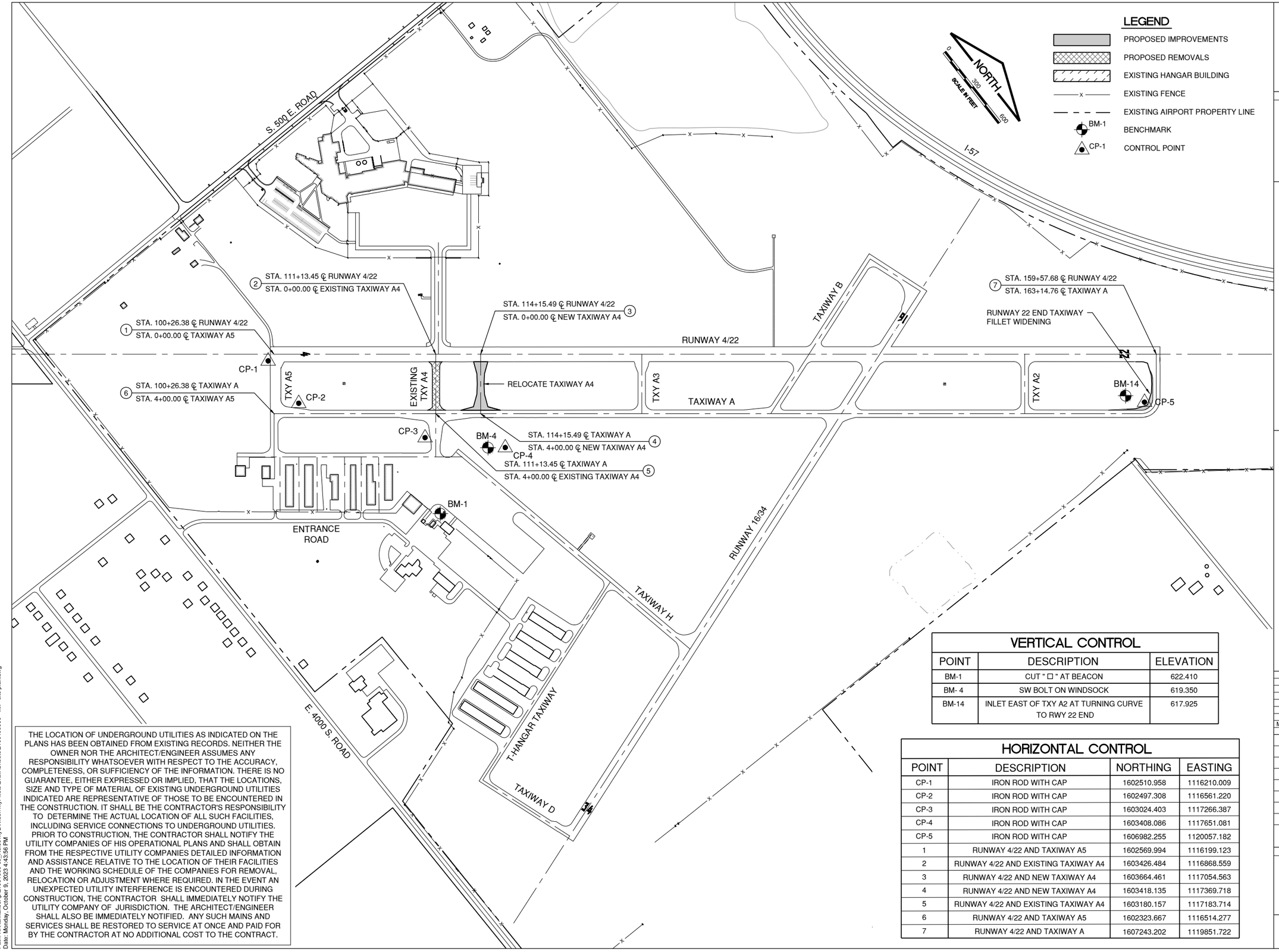

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

VERTICAL CONTROL

POINT	DESCRIPTION	ELEVATION
BM-1	CUT " □ " AT BEACON	622.410
BM- 4	SW BOLT ON WINDSOCK	619.350
BM-14	INLET EAST OF TXY A2 AT TURNING CURVE TO RWY 22 END	617.925

HORIZONTAL CONTROL

POINT	DESCRIPTION	NORTHING	EASTING
CP-1	IRON ROD WITH CAP	1602510.958	1116210.009
CP-2	IRON ROD WITH CAP	1602497.308	1116561.220
CP-3	IRON ROD WITH CAP	1603024.403	1117266.387
CP-4	IRON ROD WITH CAP	1603408.086	1117651.081
CP-5	IRON ROD WITH CAP	1606982.255	1120057.182
1	RUNWAY 4/22 AND TAXIWAY A5	1602569.994	1116199.123
2	RUNWAY 4/22 AND EXISTING TAXIWAY A4	1603426.484	1116868.559
3	RUNWAY 4/22 AND NEW TAXIWAY A4	1603664.461	1117054.563
4	RUNWAY 4/22 AND NEW TAXIWAY A4	1603418.135	1117369.718
5	RUNWAY 4/22 AND EXISTING TAXIWAY A4	1603180.157	1117183.714
6	RUNWAY 4/22 AND TAXIWAY A5	1602323.667	1116514.277
7	RUNWAY 4/22 AND TAXIWAY A	1607243.202	1119851.722

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SITE PLAN AND PROJECT CONTROL PLAN

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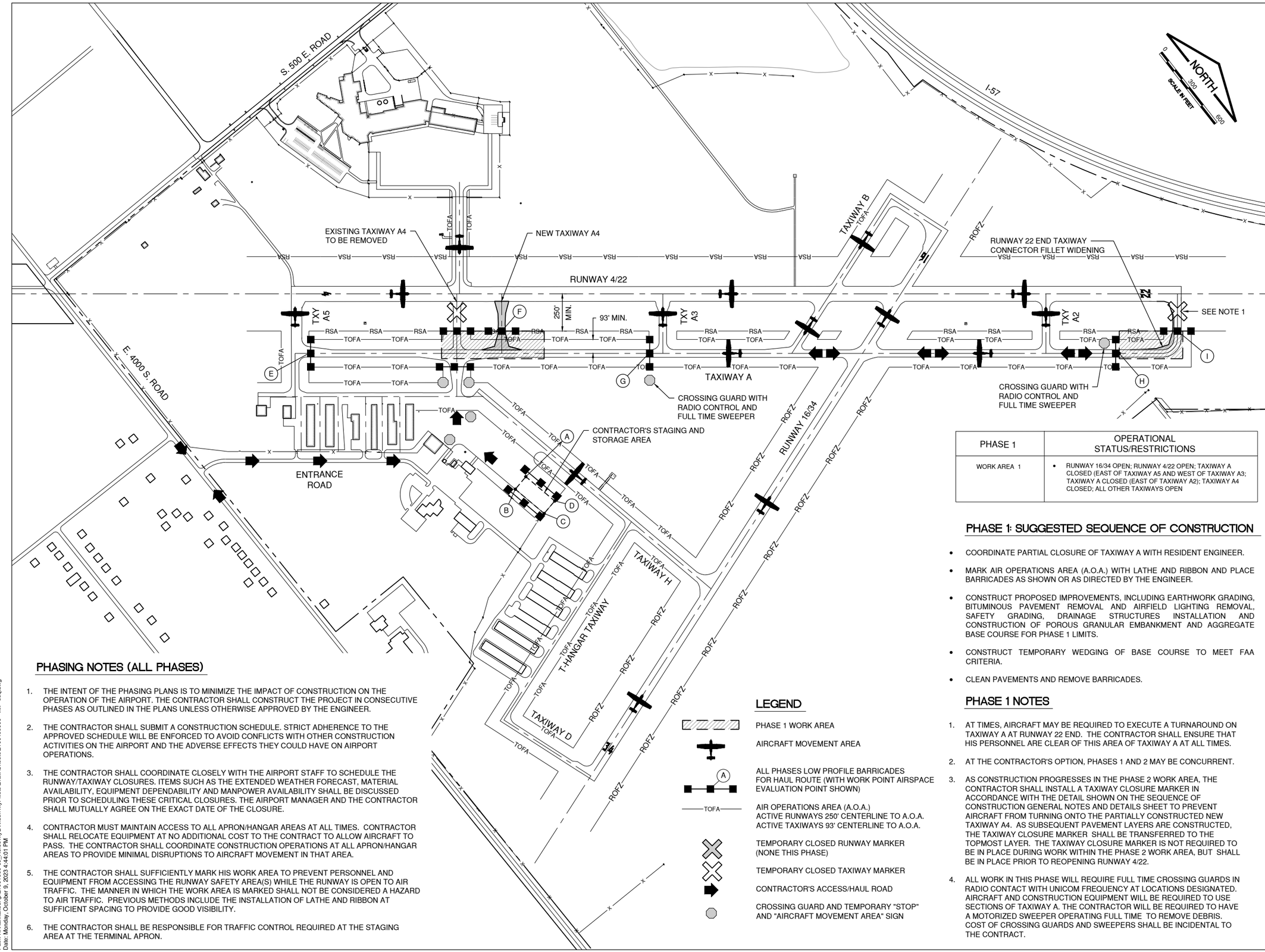

PHASE 1	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 1	• RUNWAY 16/34 OPEN; RUNWAY 4/22 OPEN; TAXIWAY A CLOSED (EAST OF TAXIWAY A5 AND WEST OF TAXIWAY A3; TAXIWAY A CLOSED (EAST OF TAXIWAY A2); TAXIWAY A4 CLOSED; ALL OTHER TAXIWAYS OPEN

PHASE 1: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE PARTIAL CLOSURE OF TAXIWAY A WITH RESIDENT ENGINEER.
- MARK AIR OPERATIONS AREA (A.O.A.) WITH LATHE AND RIBBON AND PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER.
- CONSTRUCT PROPOSED IMPROVEMENTS, INCLUDING EARTHWORK GRADING, BITUMINOUS PAVEMENT REMOVAL AND AIRFIELD LIGHTING REMOVAL, SAFETY GRADING, DRAINAGE STRUCTURES INSTALLATION AND CONSTRUCTION OF POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE COURSE FOR PHASE 1 LIMITS.
- CONSTRUCT TEMPORARY WEDGING OF BASE COURSE TO MEET FAA CRITERIA.
- CLEAN PAVEMENTS AND REMOVE BARRICADES.

PHASE 1 NOTES

1. AT TIMES, AIRCRAFT MAY BE REQUIRED TO EXECUTE A TURNAROUND ON TAXIWAY A AT RUNWAY 22 END. THE CONTRACTOR SHALL ENSURE THAT HIS PERSONNEL ARE CLEAR OF THIS AREA OF TAXIWAY A AT ALL TIMES.
2. AT THE CONTRACTOR'S OPTION, PHASES 1 AND 2 MAY BE CONCURRENT.
3. AS CONSTRUCTION PROGRESSES IN THE PHASE 2 WORK AREA, THE CONTRACTOR SHALL INSTALL A TAXIWAY CLOSURE MARKER IN ACCORDANCE WITH THE DETAIL SHOWN ON THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEET TO PREVENT AIRCRAFT FROM TURNING ONTO THE PARTIALLY CONSTRUCTED NEW TAXIWAY A4. AS SUBSEQUENT PAVEMENT LAYERS ARE CONSTRUCTED, THE TAXIWAY CLOSURE MARKER SHALL BE TRANSFERRED TO THE TOPMOST LAYER. THE TAXIWAY CLOSURE MARKER IS NOT REQUIRED TO BE IN PLACE DURING WORK WITHIN THE PHASE 2 WORK AREA, BUT SHALL BE IN PLACE PRIOR TO REOPENING RUNWAY 4/22.
4. ALL WORK IN THIS PHASE WILL REQUIRE FULL TIME CROSSING GUARDS IN RADIO CONTACT WITH UNICOM FREQUENCY AT LOCATIONS DESIGNATED. AIRCRAFT AND CONSTRUCTION EQUIPMENT WILL BE REQUIRED TO USE SECTIONS OF TAXIWAY A. THE CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER OPERATING FULL TIME TO REMOVE DEBRIS. COST OF CROSSING GUARDS AND SWEEPERS SHALL BE INCIDENTAL TO THE CONTRACT.

PHASING NOTES (ALL PHASES)

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.
3. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THESE CRITICAL CLOSURES. THE AIRPORT MANAGER AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATE OF THE CLOSURE.
4. CONTRACTOR MUST MAINTAIN ACCESS TO ALL APRON/HANGAR AREAS AT ALL TIMES. CONTRACTOR SHALL RELOCATE EQUIPMENT AT NO ADDITIONAL COST TO THE CONTRACT TO ALLOW AIRCRAFT TO PASS. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL APRON/HANGAR AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.
5. THE CONTRACTOR SHALL SUFFICIENTLY MARK HIS WORK AREA TO PREVENT PERSONNEL AND EQUIPMENT FROM ACCESSING THE RUNWAY SAFETY AREA(S) WHILE THE RUNWAY IS OPEN TO AIR TRAFFIC. THE MANNER IN WHICH THE WORK AREA IS MARKED SHALL NOT BE CONSIDERED A HAZARD TO AIR TRAFFIC. PREVIOUS METHODS INCLUDE THE INSTALLATION OF LATHE AND RIBBON AT SUFFICIENT SPACING TO PROVIDE GOOD VISIBILITY.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL REQUIRED AT THE STAGING AREA AT THE TERMINAL APRON.

LEGEND

- PHASE 1 WORK AREA
- AIRCRAFT MOVEMENT AREA
- ALL PHASES LOW PROFILE BARRICADES FOR HAUL ROUTE (WITH WORK POINT AIRSPACE EVALUATION POINT SHOWN)
- TOFA — AIR OPERATIONS AREA (A.O.A.) ACTIVE RUNWAYS 250' CENTERLINE TO A.O.A. ACTIVE TAXIWAYS 93' CENTERLINE TO A.O.A.
- TEMPORARY CLOSED RUNWAY MARKER (NONE THIS PHASE)
- TEMPORARY CLOSED TAXIWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD
- CROSSING GUARD AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN

KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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SHEET TITLE

SEQUENCE OF CONSTRUCTION PHASE 1

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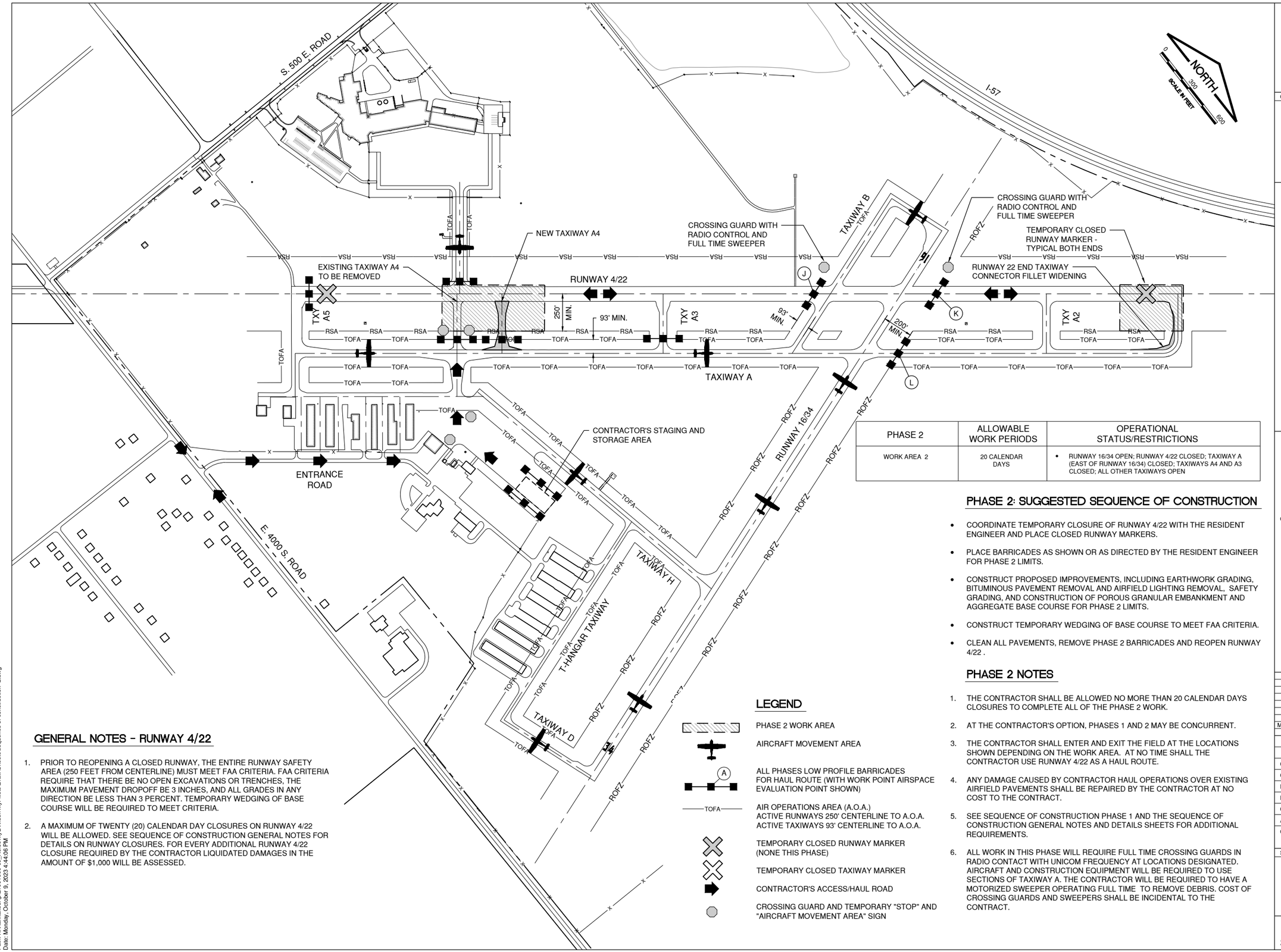

PHASE 2	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 2	20 CALENDAR DAYS	• RUNWAY 16/34 OPEN; RUNWAY 4/22 CLOSED; TAXIWAY A (EAST OF RUNWAY 16/34) CLOSED; TAXIWAYS A4 AND A3 CLOSED; ALL OTHER TAXIWAYS OPEN

PHASE 2: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE TEMPORARY CLOSURE OF RUNWAY 4/22 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER FOR PHASE 2 LIMITS.
- CONSTRUCT PROPOSED IMPROVEMENTS, INCLUDING EARTHWORK GRADING, BITUMINOUS PAVEMENT REMOVAL AND AIRFIELD LIGHTING REMOVAL, SAFETY GRADING, AND CONSTRUCTION OF POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE COURSE FOR PHASE 2 LIMITS.
- CONSTRUCT TEMPORARY WEDGING OF BASE COURSE TO MEET FAA CRITERIA.
- CLEAN ALL PAVEMENTS, REMOVE PHASE 2 BARRICADES AND REOPEN RUNWAY 4/22 .

PHASE 2 NOTES

1. THE CONTRACTOR SHALL BE ALLOWED NO MORE THAN 20 CALENDAR DAYS CLOSURES TO COMPLETE ALL OF THE PHASE 2 WORK.
2. AT THE CONTRACTOR'S OPTION, PHASES 1 AND 2 MAY BE CONCURRENT.
3. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAY 4/22 AS A HAUL ROUTE.
4. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
5. SEE SEQUENCE OF CONSTRUCTION PHASE 1 AND THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
6. ALL WORK IN THIS PHASE WILL REQUIRE FULL TIME CROSSING GUARDS IN RADIO CONTACT WITH UNICOM FREQUENCY AT LOCATIONS DESIGNATED. AIRCRAFT AND CONSTRUCTION EQUIPMENT WILL BE REQUIRED TO USE SECTIONS OF TAXIWAY A. THE CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER OPERATING FULL TIME TO REMOVE DEBRIS. COST OF CROSSING GUARDS AND SWEEPERS SHALL BE INCIDENTAL TO THE CONTRACT.

LEGEND

- PHASE 2 WORK AREA
- AIRCRAFT MOVEMENT AREA
- ALL PHASES LOW PROFILE BARRICADES FOR HAUL ROUTE (WITH WORK POINT AIRSPACE EVALUATION POINT SHOWN)
- TOFA — AIR OPERATIONS AREA (A.O.A.) ACTIVE RUNWAYS 250' CENTERLINE TO A.O.A. ACTIVE TAXIWAYS 93' CENTERLINE TO A.O.A.
- TEMPORARY CLOSED RUNWAY MARKER (NONE THIS PHASE)
- TEMPORARY CLOSED TAXIWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD
- CROSSING GUARD AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN

GENERAL NOTES – RUNWAY 4/22

1. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRE THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES, THE MAXIMUM PAVEMENT DROPOFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 3 PERCENT. TEMPORARY WEDGING OF BASE COURSE WILL BE REQUIRED TO MEET CRITERIA.
2. A MAXIMUM OF TWENTY (20) CALENDAR DAY CLOSURES ON RUNWAY 4/22 WILL BE ALLOWED. SEE SEQUENCE OF CONSTRUCTION GENERAL NOTES FOR DETAILS ON RUNWAY CLOSURES. FOR EVERY ADDITIONAL RUNWAY 4/22 CLOSURE REQUIRED BY THE CONTRACTOR LIQUIDATED DAMAGES IN THE AMOUNT OF $1,000 WILL BE ASSESSED.

KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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SHEET TITLE

SEQUENCE OF CONSTRUCTION PHASE 2

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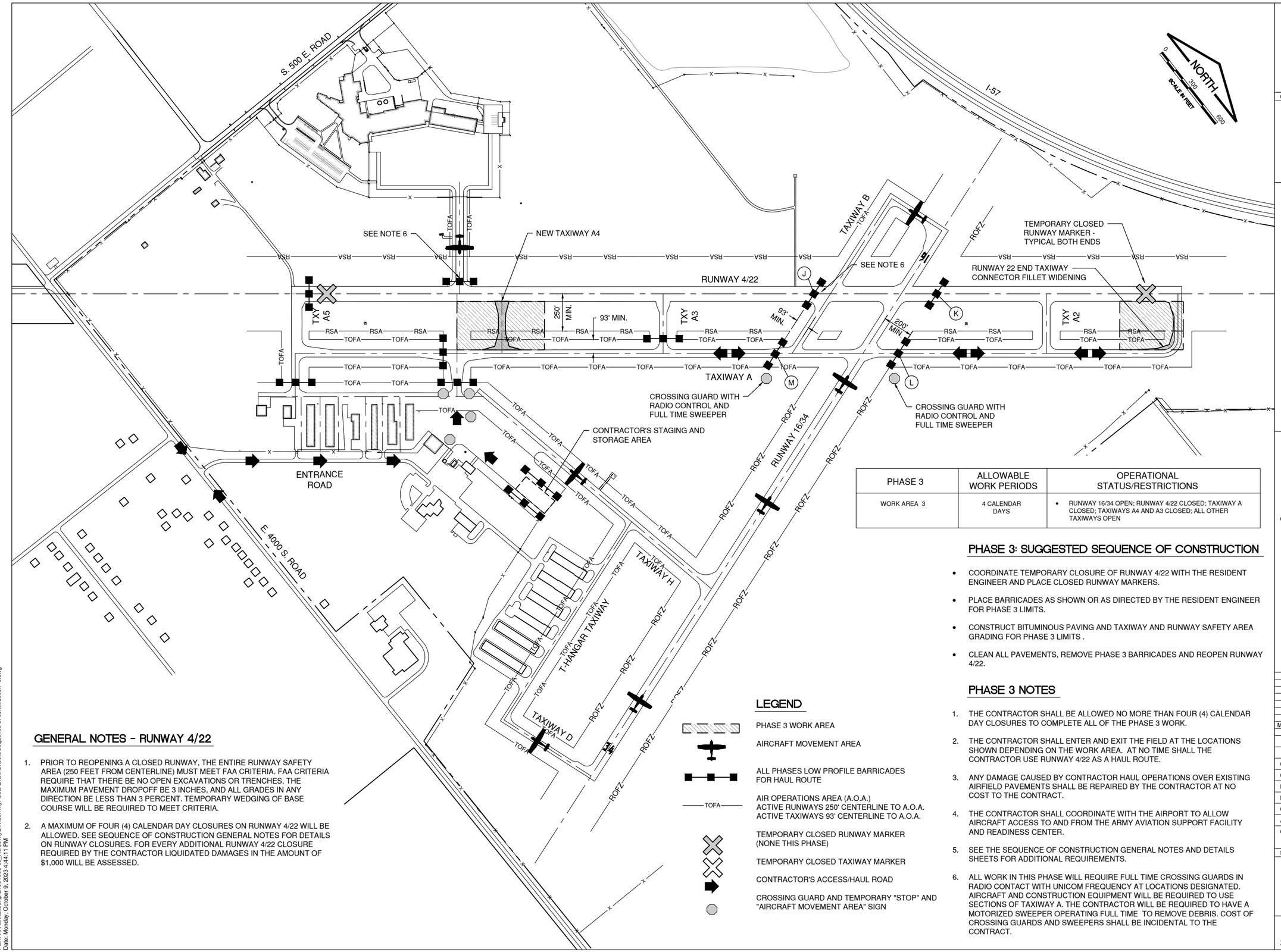

GENERAL NOTES – RUNWAY 4/22

1. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRE THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES, THE MAXIMUM PAVEMENT DROPOFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 3 PERCENT. TEMPORARY WEDGING OF BASE COURSE WILL BE REQUIRED TO MEET CRITERIA.
2. A MAXIMUM OF FOUR (4) CALENDAR DAY CLOSURES ON RUNWAY 4/22 WILL BE ALLOWED. SEE SEQUENCE OF CONSTRUCTION GENERAL NOTES FOR DETAILS ON RUNWAY CLOSURES. FOR EVERY ADDITIONAL RUNWAY 4/22 CLOSURE REQUIRED BY THE CONTRACTOR LIQUIDATED DAMAGES IN THE AMOUNT OF $1,000 WILL BE ASSESSED.

LEGEND

- PHASE 3 WORK AREA
- AIRCRAFT MOVEMENT AREA
- ALL PHASES LOW PROFILE BARRICADES FOR HAUL ROUTE
- TOFA — AIR OPERATIONS AREA (A.O.A.) ACTIVE RUNWAYS 250' CENTERLINE TO A.O.A. ACTIVE TAXIWAYS 93' CENTERLINE TO A.O.A.
- TEMPORARY CLOSED RUNWAY MARKER (NONE THIS PHASE)
- TEMPORARY CLOSED TAXIWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD
- CROSSING GUARD AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN

PHASE 3	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 3	4 CALENDAR DAYS	• RUNWAY 16/34 OPEN; RUNWAY 4/22 CLOSED; TAXIWAY A CLOSED; TAXIWAYS A4 AND A3 CLOSED; ALL OTHER TAXIWAYS OPEN

PHASE 3: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE TEMPORARY CLOSURE OF RUNWAY 4/22 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER FOR PHASE 3 LIMITS.
- CONSTRUCT BITUMINOUS PAVING AND TAXIWAY AND RUNWAY SAFETY AREA GRADING FOR PHASE 3 LIMITS .
- CLEAN ALL PAVEMENTS, REMOVE PHASE 3 BARRICADES AND REOPEN RUNWAY 4/22.

PHASE 3 NOTES

1. THE CONTRACTOR SHALL BE ALLOWED NO MORE THAN FOUR (4) CALENDAR DAY CLOSURES TO COMPLETE ALL OF THE PHASE 3 WORK.
2. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAY 4/22 AS A HAUL ROUTE.
3. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
4. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT TO ALLOW AIRCRAFT ACCESS TO AND FROM THE ARMY AVIATION SUPPORT FACILITY AND READINESS CENTER.
5. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
6. ALL WORK IN THIS PHASE WILL REQUIRE FULL TIME CROSSING GUARDS IN RADIO CONTACT WITH UNICOM FREQUENCY AT LOCATIONS DESIGNATED. AIRCRAFT AND CONSTRUCTION EQUIPMENT WILL BE REQUIRED TO USE SECTIONS OF TAXIWAY A. THE CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER OPERATING FULL TIME TO REMOVE DEBRIS. COST OF CROSSING GUARDS AND SWEEPERS SHALL BE INCIDENTAL TO THE CONTRACT.

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FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

SEQUENCE OF CONSTRUCTION PHASE 3

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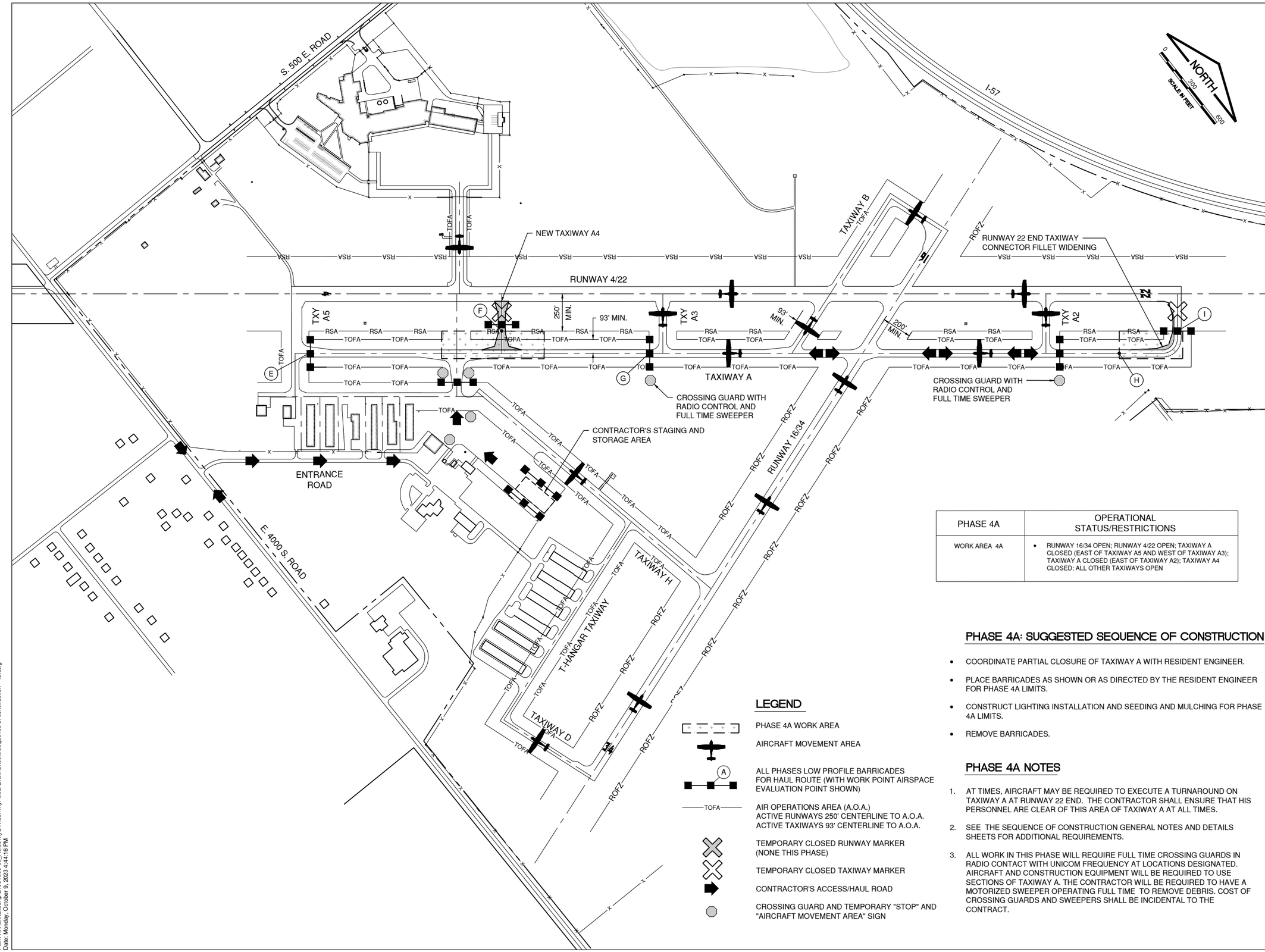
Date: Monday, October 9, 2023 4:44:11 PM



PHASE 4A	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 4A	• RUNWAY 16/34 OPEN; RUNWAY 4/22 OPEN; TAXIWAY A CLOSED (EAST OF TAXIWAY A5 AND WEST OF TAXIWAY A3); TAXIWAY A CLOSED (EAST OF TAXIWAY A2); TAXIWAY A4 CLOSED; ALL OTHER TAXIWAYS OPEN

PHASE 4A: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE PARTIAL CLOSURE OF TAXIWAY A WITH RESIDENT ENGINEER.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER FOR PHASE 4A LIMITS.
- CONSTRUCT LIGHTING INSTALLATION AND SEEDING AND MULCHING FOR PHASE 4A LIMITS.
- REMOVE BARRICADES.

LEGEND

PHASE 4A WORK AREA

AIRCRAFT MOVEMENT AREA

ALL PHASES LOW PROFILE BARRICADES FOR HAUL ROUTE (WITH WORK POINT AIRSPACE EVALUATION POINT SHOWN)

TOFA — AIR OPERATIONS AREA (A.O.A.)
ACTIVE RUNWAYS 250' CENTERLINE TO A.O.A.
ACTIVE TAXIWAYS 93' CENTERLINE TO A.O.A.

TEMPORARY CLOSED RUNWAY MARKER (NONE THIS PHASE)

TEMPORARY CLOSED TAXIWAY MARKER

CONTRACTOR'S ACCESS/HAUL ROAD

CROSSING GUARD AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN

PHASE 4A NOTES

1. AT TIMES, AIRCRAFT MAY BE REQUIRED TO EXECUTE A TURNAROUND ON TAXIWAY A AT RUNWAY 22 END. THE CONTRACTOR SHALL ENSURE THAT HIS PERSONNEL ARE CLEAR OF THIS AREA OF TAXIWAY A AT ALL TIMES.
2. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
3. ALL WORK IN THIS PHASE WILL REQUIRE FULL TIME CROSSING GUARDS IN RADIO CONTACT WITH UNICOM FREQUENCY AT LOCATIONS DESIGNATED. AIRCRAFT AND CONSTRUCTION EQUIPMENT WILL BE REQUIRED TO USE SECTIONS OF TAXIWAY A. THE CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER OPERATING FULL TIME TO REMOVE DEBRIS. COST OF CROSSING GUARDS AND SWEEPERS SHALL BE INCIDENTAL TO THE CONTRACT.

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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SHEET TITLE

SEQUENCE OF CONSTRUCTION PHASE 4A

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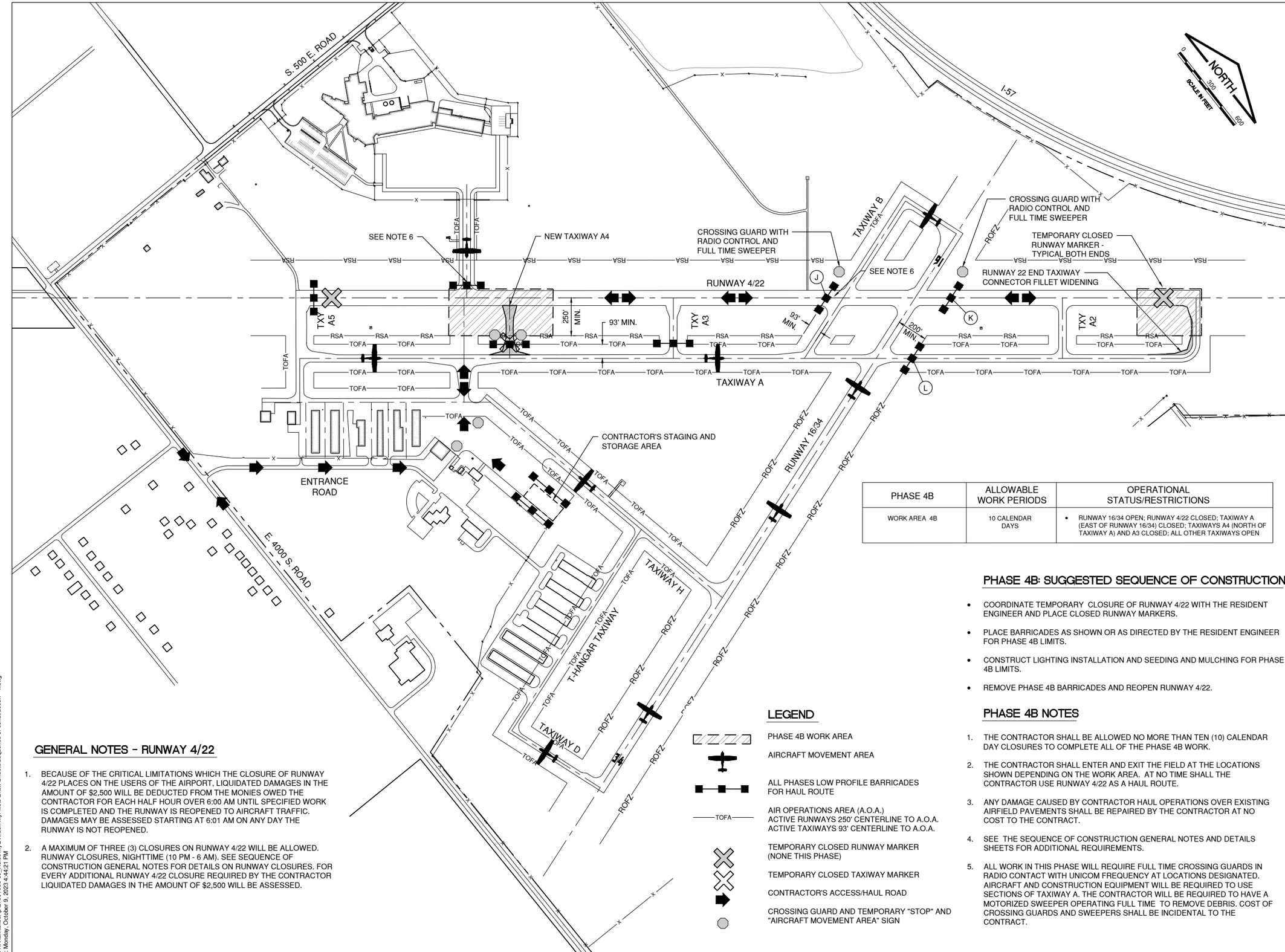

PHASE 4B	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
WORK AREA 4B	10 CALENDAR DAYS	• RUNWAY 16/34 OPEN; RUNWAY 4/22 CLOSED; TAXIWAY A (EAST OF RUNWAY 16/34) CLOSED; TAXIWAYS A4 (NORTH OF TAXIWAY A) AND A3 CLOSED; ALL OTHER TAXIWAYS OPEN

PHASE 4B: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE TEMPORARY CLOSURE OF RUNWAY 4/22 WITH THE RESIDENT ENGINEER AND PLACE CLOSED RUNWAY MARKERS.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE RESIDENT ENGINEER FOR PHASE 4B LIMITS.
- CONSTRUCT LIGHTING INSTALLATION AND SEEDING AND MULCHING FOR PHASE 4B LIMITS.
- REMOVE PHASE 4B BARRICADES AND REOPEN RUNWAY 4/22.

PHASE 4B NOTES

1. THE CONTRACTOR SHALL BE ALLOWED NO MORE THAN TEN (10) CALENDAR DAY CLOSURES TO COMPLETE ALL OF THE PHASE 4B WORK.
2. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAY 4/22 AS A HAUL ROUTE.
3. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
4. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
5. ALL WORK IN THIS PHASE WILL REQUIRE FULL TIME CROSSING GUARDS IN RADIO CONTACT WITH UNICOM FREQUENCY AT LOCATIONS DESIGNATED. AIRCRAFT AND CONSTRUCTION EQUIPMENT WILL BE REQUIRED TO USE SECTIONS OF TAXIWAY A. THE CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER OPERATING FULL TIME TO REMOVE DEBRIS. COST OF CROSSING GUARDS AND SWEEPERS SHALL BE INCIDENTAL TO THE CONTRACT.

GENERAL NOTES – RUNWAY 4/22

1. BECAUSE OF THE CRITICAL LIMITATIONS WHICH THE CLOSURE OF RUNWAY 4/22 PLACES ON THE USERS OF THE AIRPORT, LIQUIDATED DAMAGES IN THE AMOUNT OF $2,500 WILL BE DEDUCTED FROM THE MONIES OWED THE CONTRACTOR FOR EACH HALF HOUR OVER 6:00 AM UNTIL SPECIFIED WORK IS COMPLETED AND THE RUNWAY IS REOPENED TO AIRCRAFT TRAFFIC. DAMAGES MAY BE ASSESSED STARTING AT 6:01 AM ON ANY DAY THE RUNWAY IS NOT REOPENED.
2. A MAXIMUM OF THREE (3) CLOSURES ON RUNWAY 4/22 WILL BE ALLOWED. RUNWAY CLOSURES, NIGHTTIME (10 PM - 6 AM). SEE SEQUENCE OF CONSTRUCTION GENERAL NOTES FOR DETAILS ON RUNWAY CLOSURES. FOR EVERY ADDITIONAL RUNWAY 4/22 CLOSURE REQUIRED BY THE CONTRACTOR LIQUIDATED DAMAGES IN THE AMOUNT OF $2,500 WILL BE ASSESSED.

LEGEND

- PHASE 4B WORK AREA
- AIRCRAFT MOVEMENT AREA
- ALL PHASES LOW PROFILE BARRICADES FOR HAUL ROUTE
- TOFA — AIR OPERATIONS AREA (A.O.A.) ACTIVE RUNWAYS 250' CENTERLINE TO A.O.A. ACTIVE TAXIWAYS 93' CENTERLINE TO A.O.A.
- TEMPORARY CLOSED RUNWAY MARKER (NONE THIS PHASE)
- TEMPORARY CLOSED TAXIWAY MARKER
- CONTRACTOR'S ACCESS/HAUL ROAD
- CROSSING GUARD AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN

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SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

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CAD DWG FILE:	
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DRAWN BY:	JRO
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SEQUENCE OF CONSTRUCTION PHASE 4B

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GENERAL NOTES

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.

2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.

3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.

4. THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.

5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.

6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.

7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.

8. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

9. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.

10. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH TWO ORANGE FLAGS (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.

11. THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.

12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

13. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.

14. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.

15. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.

16. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.

17. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.

18. THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.

19. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.

20. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

21. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

22. COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

23. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.

24. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.

25. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES AND THE RUNWAY'S AIRCRAFT OPERATIONS AREA.

26. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.

27. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

28. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.

29. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

1. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (**$500 PER OCCURENCE**) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.

2. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEPT TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

3. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY'S AIRCRAFT OPERATIONS AREA (AOA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (14) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECCESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

PHASING NOTES (ALL PHASES)

1. THE INTENT OF THE PHASING PLANS IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. THE CONTRACTOR SHALL CONSTRUCT THE PROJECT IN CONSECUTIVE PHASES AS OUTLINED IN THE PLANS UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.

2. PRIOR TO REOPENING A CLOSED RUNWAY, THE ENTIRE (RSA) RUNWAY SAFETY AREA (250 FEET FROM CENTERLINE AND INCLUDING BEYOND THE END OF THE RUNWAY WITHIN AIRPORT PROPERTY) AND (TOFA) TAXIWAY OBJECT FREE AREA MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THE SAFETY AREA(S), THE MAXIMUM PAVEMENT DROP OFF BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 3 PERCENT. STEEL PLATES, TEMPORARY WEDGING OF BASE COURSE AND BITUMINOUS CONCRETE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

3. THE CONTRACTOR SHALL SUBMIT A CONSTRUCTION SCHEDULE. STRICT ADHERENCE TO THE APPROVED SCHEDULE WILL BE ENFORCED TO AVOID CONFLICTS WITH OTHER CONSTRUCTION ACTIVITIES ON THE AIRPORT AND THE ADVERSE EFFECTS THEY COULD HAVE ON AIRPORT OPERATIONS.

4. THE CONTRACTOR SHALL COORDINATE CLOSELY WITH THE AIRPORT STAFF TO SCHEDULE THE RUNWAY/TAXIWAY CLOSURES. ITEMS SUCH AS THE EXTENDED WEATHER FORECAST, MATERIAL AVAILABILITY, EQUIPMENT DEPENDABILITY AND MANPOWER AVAILABILITY SHALL BE DISCUSSED PRIOR TO SCHEDULING THIS CRITICAL CLOSURE. THE AIRPORT EXECUTIVE DIRECTOR AND THE CONTRACTOR SHALL MUTUALLY AGREE ON THE EXACT DATES AND TIMES OF THE CLOSURE(S).

5. CONTRACTOR MUST MAINTAIN ACCESS TO ALL ACTIVE AND OPEN AREAS AT ALL TIMES. CONTRACTOR SHALL RELOCATE EQUIPMENT AT NO ADDITIONAL COST TO CONTRACT TO ALLOW AIRCRAFT TO PASS. CONTRACTOR SHALL COORDINATE CONSTRUCTION OPERATIONS AT ALL ACTIVE AND OPEN AREAS TO PROVIDE MINIMAL DISRUPTIONS TO AIRCRAFT MOVEMENT IN THAT AREA.

6. FAA AND AIRPORT ACCESS ROAD(S) SHALL NOT BE USED AS A HAUL ROAD BY THE CONTRACTOR WITHOUT PRIOR APPROVAL.

7. TO THE EXTENT POSSIBLE THE CONTRACTOR SHALL HAVE ALL EMPLOYEE PARKING OUTSIDE OF AIRPORT FENCE OR AS INDICATED AT THE LOCATION SHOWN.

8. THE AIRPORT RESERVES THE RIGHT TO MODIFY THE SEQUENCE OF CONSTRUCTION INCLUDING BUT NOT LIMITED TO PHASING, WORK AREAS, BARRICADE PLACEMENT, ACCESS AND HAUL ROUTES, AND CONTRACTOR MOVEMENTS AT ANY TIME DURING THE PROJECT WITH FAA, IDA AND ATCT APPROVAL.

9. ALL WORK IN THIS PHASE WILL REQUIRE FULL TIME CROSSING GUARDS IN RADIO CONTACT WITH UNICOM FREQUENCY AT LOCATIONS DESIGNATED. AIRCRAFT AND CONSTRUCTION EQUIPMENT WILL BE REQUIRED TO USE SECTIONS OF TAXIWAY A. THE CONTRACTOR WILL BE REQUIRED TO HAVE A MOTORIZED SWEEPER OPERATING FULL TIME TO REMOVE DEBRIS. COST OF CROSSING GUARDS AND SWEEPERS SHALL BE INCIDENTAL TO THE CONTRACT.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

AIRSPACE ELEVATION POINTS

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	GROUND ELEVATION	TOP ELEVATION
A	RUNWAY 4/22	41°04'01.53"	87°50'52.23"	622.4	647.4
B	RUNWAY 4/22	41°04'00.47"	87°50'51.94"	622.3	647.3
C	RUNWAY 16/34	41°04'00.97"	87°50'48.70"	620.0	645.0
D	RUNWAY 16/34	41°04'02.04"	87°50'48.99"	620.4	645.4
E	RUNWAY 4/22	41°03'55.31"	87°51'12.53"	625.5	650.5
F	RUNWAY 4/22	41°04'06.28"	87°51'03.57"	620.8	645.8
G	RUNWAY 4/22	41°04'13.03"	87°50'54.02"	623.3	648.3
H	RUNWAY 4/22	41°04'37.53"	87°50'28.42"	621.5	646.5
I	RUNWAY 4/22	41°04'41.48"	87°50'26.79"	625.9	650.9
J	RUNWAY 16/34	41°04'24.06"	87°50'49.16"	622.0	647.0
K	RUNWAY 16/34	41°04'30.45"	87°50'42.48"	621.4	646.4
L	RUNWAY 16/34	41°04'26.00"	87°50'40.47"	616.0	641.0
M	RUNWAY 16/34	41°04'19.61"	87°50'47.14"	620.5	645.5

DESIGN AIRCRAFT APPROACH CATEGORY: B AND D

AIRPORT DESIGN GROUP: III
RUNWAY 4/22 SAFETY AREA WIDTH: 250'
RUNWAY 16/34 SAFETY AREA WIDTH: 250'
TAXIWAY CENTERLINE TO OBJECT SEPARATION: 93'
TAXILANE CENTERLINE TO OBJECT SEPARATION: 86.5'

MAXIMUM ANTICIPATED HEIGHT OF EQUIPMENT – 25'

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CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
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SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 1

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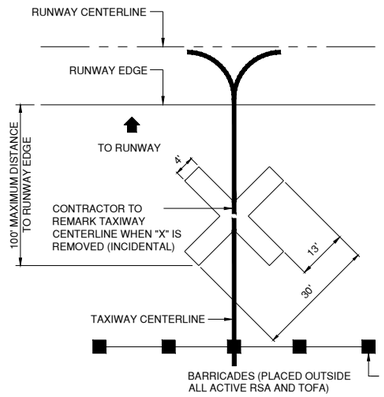
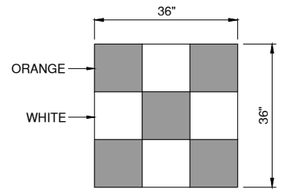


CLOSED TAXIWAY MARKER DETAIL

NOT TO SCALE

CLOSED TAXIWAY MARKER DETAIL NOTES

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED YELLOW WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13 (LATEST EDITION) AND ARE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR CLOSURES EXCEEDING 72 HOURS.
7. AS THE CONSTRUCTION OF THE PORTION OF THE TAXIWAY WITHIN THE RUNWAY SAFETY AREA, PROGRESSES, SHOULD THE NEW TAXIWAY BE CLOSED FOR MORE THAN 72 HOURS, THE CONTRACTOR SHALL INSTALL A CLOSED TAXIWAY MARKER AS SHOWN IN THE DETAIL. THE CLOSED TAXIWAY MARKER SHALL NOT BE REMOVED UNLESS THE TAXIWAY WILL BE OPENED AND REMAIN OPEN WITHIN 72 HOURS OF MARKER REMOVAL.

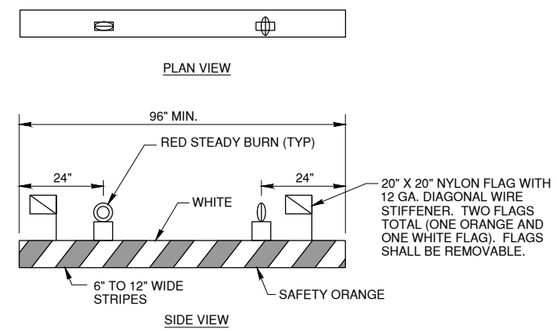


CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

AIRFIELD LIGHTS AND SIGNS NOTES

1. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
2. CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES. CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY, THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

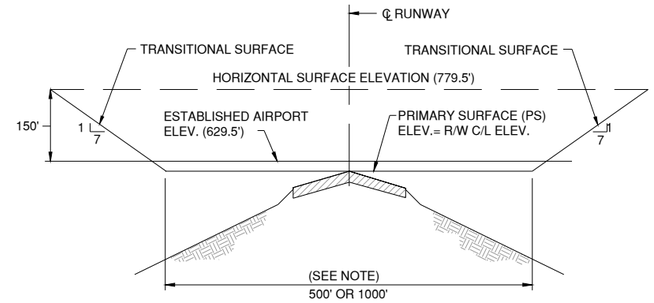


AIRSIDE LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

BARRICADE NOTES

1. STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

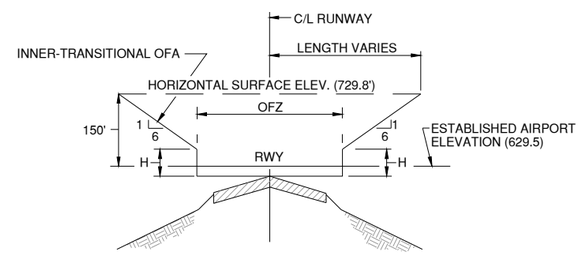


TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

NOTE:

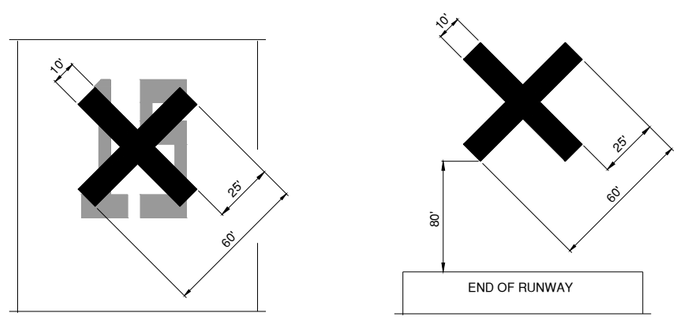
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY



TYPICAL SECTION OBSTACLE FREE ZONE (OFZ)

NO SCALE

RUNWAY	TYPE OF RUNWAY	H (FEET)
16-34	NON-PRECISION	150
4-22	PRECISION	150



ON PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL

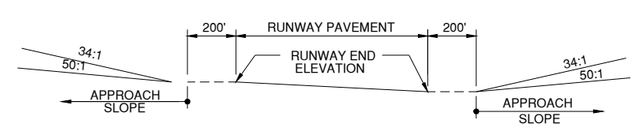
NOT TO SCALE

OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL

NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



TYPICAL PROFILE F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
16	621	34:1
34	617	34:1
4	624	50:1
22	630	34:1

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
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SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS - 2

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

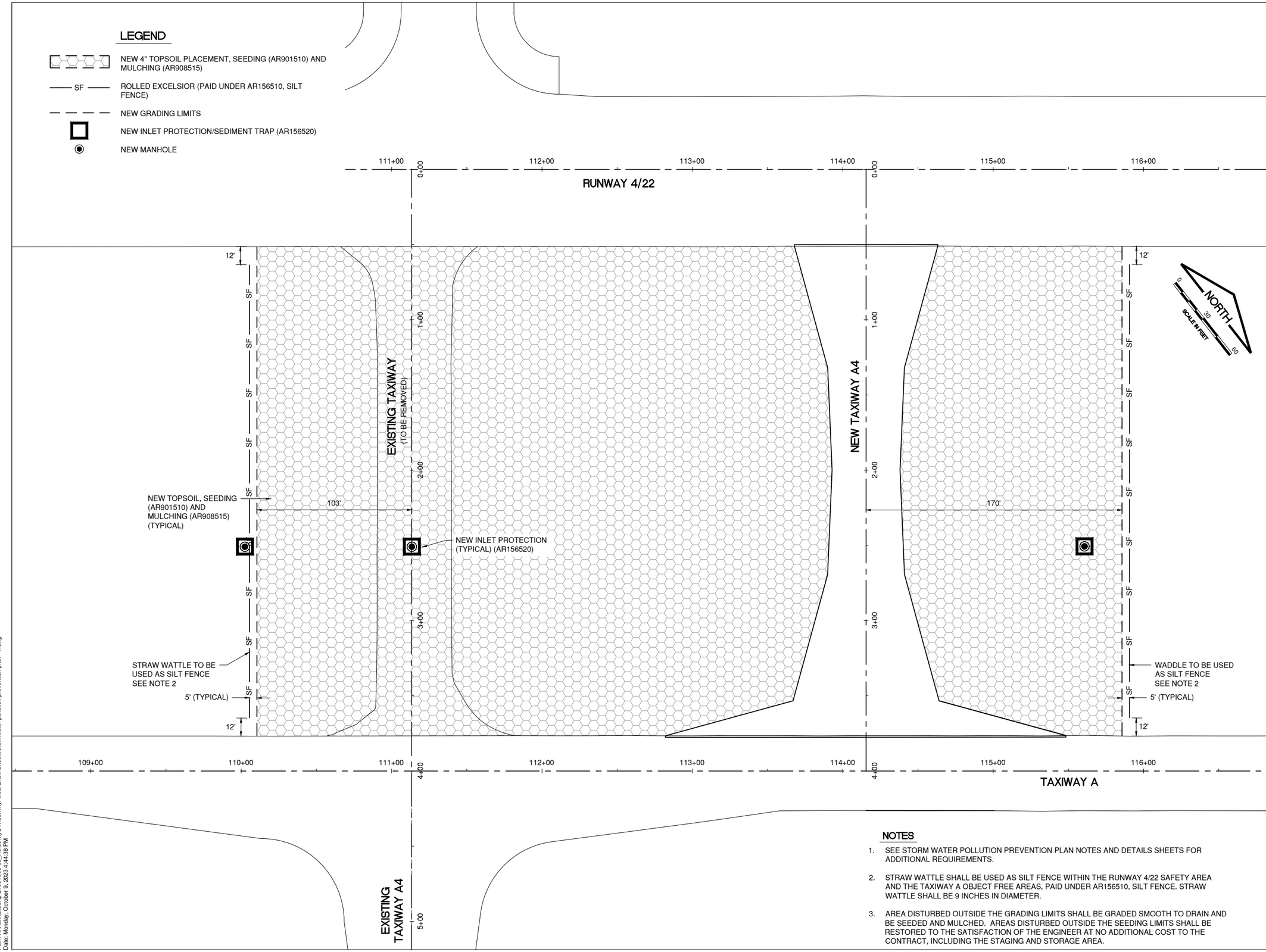
SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

LEGEND

- NEW 4" TOPSOIL PLACEMENT, SEEDING (AR901510) AND MULCHING (AR908515)
- SF — ROLLED EXCELSIOR (PAID UNDER AR156510, SILT FENCE)
- NEW GRADING LIMITS
- NEW INLET PROTECTION/SEDIMENT TRAP (AR156520)
- NEW MANHOLE



NOTES

1. SEE STORM WATER POLLUTION PREVENTION PLAN NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
2. STRAW WATTLE SHALL BE USED AS SILT FENCE WITHIN THE RUNWAY 4/22 SAFETY AREA AND THE TAXIWAY A OBJECT FREE AREAS, PAID UNDER AR156510, SILT FENCE. STRAW WATTLE SHALL BE 9 INCHES IN DIAMETER.
3. AREA DISTURBED OUTSIDE THE GRADING LIMITS SHALL BE GRADED SMOOTH TO DRAIN AND BE SEEDED AND MULCHED. AREAS DISTURBED OUTSIDE THE SEEDING LIMITS SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT, INCLUDING THE STAGING AND STORAGE AREA.

MARK	DATE	DESCRIPTION

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STORMWATER POLLUTION PREVENTION PLAN - 1

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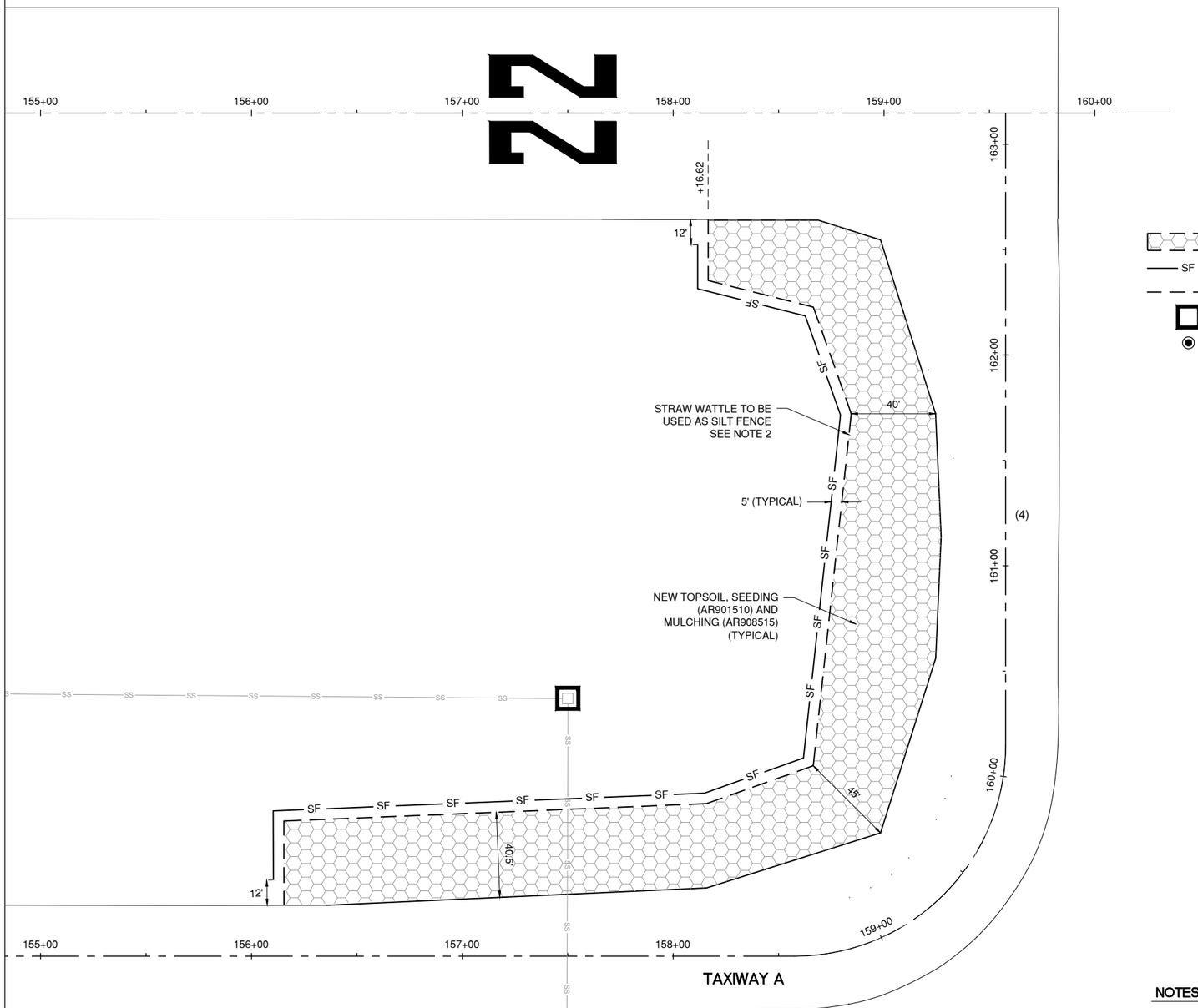

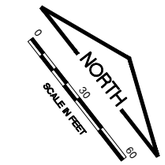

LEGEND

- NEW 4" TOPSOIL PLACEMENT, SEEDING (AR901510) AND MULCHING (AR908515)
- SF — ROLLED EXCELSIOR (PAID UNDER 156510, SILT FENCE)
- NEW GRADING LIMITS
- NEW INLET PROTECTION/SEDIMENT TRAP (AR156520)
- NEW MANHOLE

NOTES

1. SEE STORM WATER POLLUTION PREVENTION PLAN NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
2. STRAW WATTLE SHALL BE USED AS SILT FENCE WITHIN THE RUNWAY 4/22 SAFETY AREA AND THE TAXIWAY A OBJECT FREE AREAS, PAID UNDER AR156510, SILT FENCE. STRAW WATTLE SHALL BE 9 INCHES IN DIAMETER.
3. AREA DISTURBED OUTSIDE THE GRADING LIMITS SHALL BE GRADED SMOOTH TO DRAIN AND BE SEEDED AND MULCHED. AREAS DISTURBED OUTSIDE THE SEEDING LIMITS SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT, INCLUDING THE STAGING AND STORAGE AREA.

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SEPTEMBER 22, 2023

OWNER

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STORMWATER POLLUTION PREVENTION PLAN - 2

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STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT THE GREATER KANKAKEE AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL.

REMOVAL OF EXISTING BITUMINOUS PAVEMENT. CONSTRUCTION OF NEW BITUMINOUS PAVEMENT AND EMBANKMENT.

AIRFIELD LIGHTING, SIGNAGE AND CABLING.

DRAINAGE INFRASTRUCTURE ADJUSTMENTS.

SEEDING AND MULCHING DISTURBED AREAS.

INSTALLATION OF NEW PAVEMENT MARKING.

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

IT IS ESTIMATED THAT MORE THAN 1 ACRE BUT LESS THAN 5 ACRES OF LAND WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KANKAKEE RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCIDENTAL TO THE CONTRACT.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS' REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS"

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT # _____

DATE ISSUED _____

DATE EXPIRED _____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: GREATER KANKAKEE AIRPORT

MARKED: WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SECTION: 16, 20, & 21

PROJECT NUMBER: IKK-4991

COUNTY: KANKAKEE

CONTRACT NUMBER: N/A

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE:_____ DATE:_____

PRINTED NAME:_____ TITLE:_____

NAME OF FIRM:_____

STREET ADDRESS:_____

CITY, STATE, ZIP:_____

PHONE NUMBER:_____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

RECORD OF SITE DISTURBANCE AND STABILIZATION

MAJOR GRADING ACTIVITIES: BEGINNING DATE: _____
LOCATION:_____ COMPLETION DATE:_____

MAJOR GRADING ACTIVITIES: BEGINNING DATE: _____
LOCATION:_____ COMPLETION DATE:_____

SITE STABILIZATION: BEGINNING DATE: _____
LOCATION:_____ COMPLETION DATE:_____

SITE STABILIZATION: BEGINNING DATE: _____
LOCATION:_____ COMPLETION DATE:_____

CONSTRUCTION CEASED: BEGINNING DATE: _____
EXPLANATION:_____ COMPLETION DATE:_____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - 1

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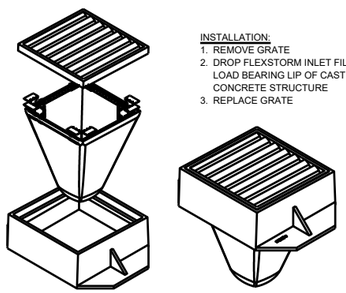
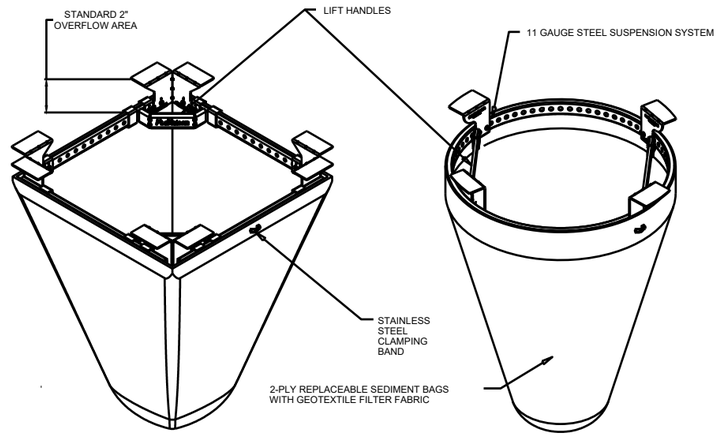
Date: Monday, October 9, 2023 4:44:50 PM

orm Inlet Filter Specifications			
Material Property	*Test Method*	*Value (min ave)*	
> Inner Filter Bag Specs (2 ft³ min vol)		**Non-Woven**	**Woven Mono**
Grab Tensile	ASTM D 4632	100 lbs	200 lbs
Puncture Strength	ASTM D 4833	65 lbs	90 lbs
Trapezoidal Tear	ASTM D 4533	45 lbs	75 lbs
UV Resistance	ASTM D 4355	70% at 500 hrs	90%
App Open Size (AOS)	ASTM D 4751	70 sieve (.212 mm)	40 sieve (.425 mm)
Permittivity	ASTM D 4491	2.0 /sec	2.1/sec
Water Flow Rate	ASTM D 4491	145 gpm/sqft	145 gpm/sqft
> Polyester Outer Reinforcement Bag Specifications			
Weight	ASTM D 3776	4.55 oz/sqyd +/- 15%	
Thickness	ASTM D 1777	.040 +/- .005	
> Frame Construction			
A36 Structural Steel; 11 Guage; Zinc Plated	ASTM A 576	*Tensile Strength > 58,000 psi; Yield Strength > 36,000 psi*	



INSTALLATION:
1. REMOVE GRATE
2. DROP FLEXSTORM INLET FILTER ONTO LOAD BEARING LIP OF CASTING OR CONCRETE STRUCTURE
3. REPLACE GRATE



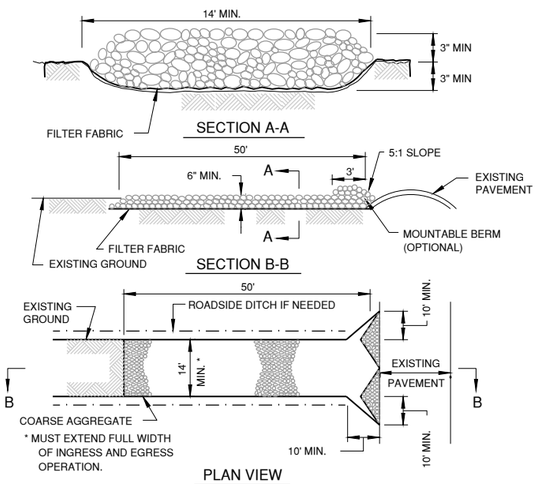
TYPICAL FLAT/RECTANGULAR/ROLLED CURB INLET FILTER

TYPICAL ROUND INLET FILTER

INLET PROTECTION / SEDIMENT TRAP

NOT TO SCALE

1. STORM SEWER INLET PROTECTION SHALL BE INLET FILTERS AS DETAILED HEREIN OR APPROVED EQUAL



1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR AR152540 IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF AERONAUTICS SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. CONTRACTOR HAS THE OPTION TO USE BITUMINOUS PAVEMENT MILLINGS AT NO EXTRA COST TO THE CONTRACT. COMPACTION SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. STABILIZED CONSTRUCTION ENTRANCE(S) SHALL BE INCIDENTAL TO THE CONTRACT.

STABILIZED CONSTRUCTION ENTRANCE

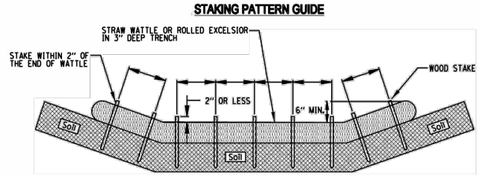
FROM NRCS STANDARD DRAWING NO. IL-630 (MODIFIED)

PROJECT SITE AREA AND STAGING AREA PROTECTION NOTES:

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES WITHIN THE PROJECT LIMITS AND AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE PROJECT SITE AREA AND STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.

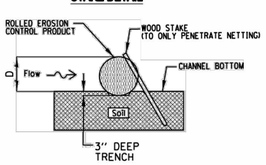
ROLLED EROSION CONTROL PRODUCTS

STAKING PATTERN GUIDE



NOTES:
1. OVERLAP MINIMUM IS THE DIAMETER OF THE ROLL.
2. 4' SPACING FOR WATTLES.
3. 2' SPACING FOR ROLLED EXCELSIOR.
4. OR SPACE ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

STAKE DETAIL



NOTES:
1. DRAWINGS ARE NOT TO SCALE.
2. ENDS OF WATTLES OR ROLLED EXCELSIOR SHALL BE TURNED AT LEAST 6" UPSLOPE.
3. RECOMMENDED STAKES ARE 1 1/8" WIDE × 1 1/8" THICK × 30" LONG.
4. STAKES SHALL NOT EXTEND ABOVE THE STRAW WATTLE MORE THAN 2".
5. SPACING: THE TOE OF THE UPSTREAM DITCH CHECK SHALL CREATE A HORIZONTAL LINE WITH THE TOP OF THE DOWNSTREAM DITCH CHECK.

REFERENCE		STANDARD DWG. NO.
Project		IUM-514
Designed	Date	SHEET 1 OF 1
Checked	Date	DATE 08-2-2019
Approved	Date	

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CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
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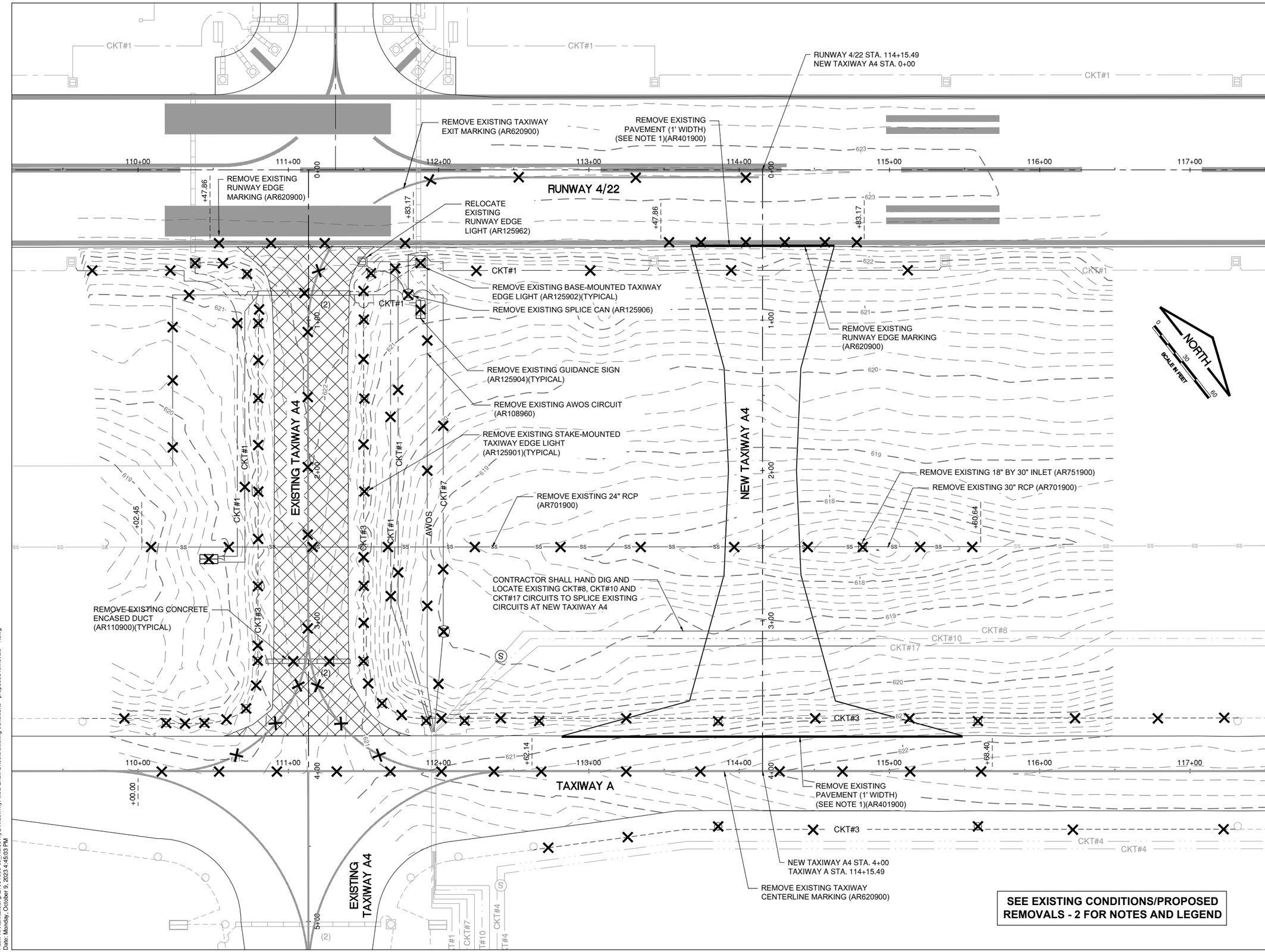
SHEET TITLE

STORMWATER POLLUTION PREVENTION PLAN NOTES AND DETAILS - 2



SEE EXISTING CONDITIONS/PROPOSED REMOVALS - 2 FOR NOTES AND LEGEND

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

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EXISTING CONDITIONS/ PROPOSED REMOVALS - 1

SHEET 15 OF 38

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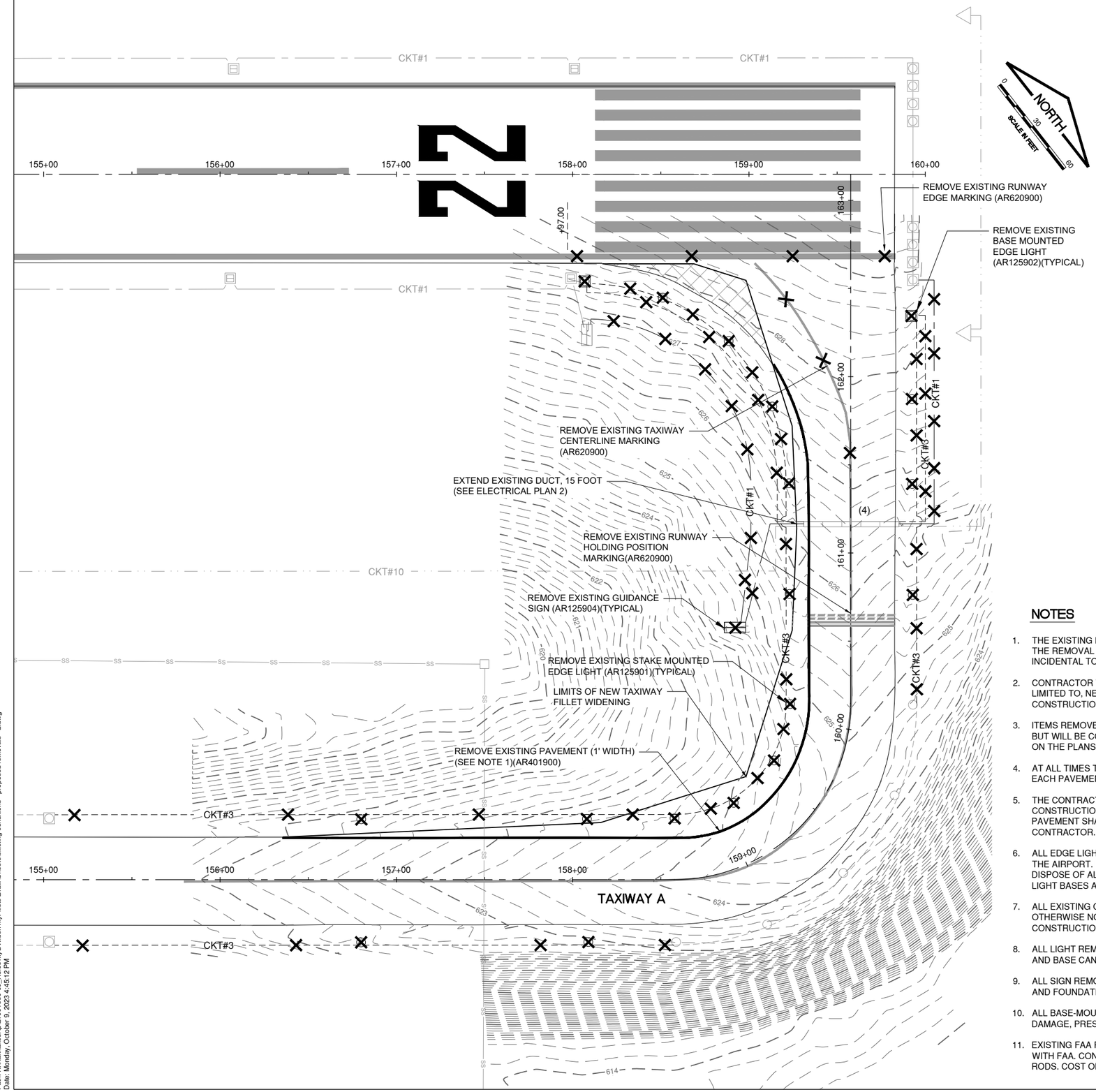
Date: Monday, October 9, 2023 4:45:03 PM



EXISTING CONDITIONS LEGEND

626	EXISTING CONTOUR
	EXISTING CONDUIT/DUCT BANK
ST	EXISTING STORM SEWER
UD	EXISTING UNDERDRAIN
W	EXISTING WATERMAIN
	EXISTING STORM INLET
	EXISTING SLOPE BOX
	EXISTING WATER VALVE
	EXISTING UNDERDRAIN CLEANOUT STRUCTURE
	BASE-MOUNTED TAXIWAY EDGE LIGHT
	STAKE-MOUNTED TAXIWAY EDGE LIGHT
	AIRFIELD GUIDANCE SIGN
S	EXISTING SPLICE CAN
	BASE-MOUNTED RUNWAY LIGHT
	EXISTING ELECTRICAL MANHOLE
	BURIED CONCRETE ENCASED DUCT BANK
CKT#1	EXISTING RUNWAY 4/22 CIRCUIT
CKT#3	EXISTING TAXIWAY A2, A3, A4 AND A5 CIRCUIT
CKT#4	EXISTING TAXIWAY B/BB CIRCUIT
CKT#7	EXISTING ELECTRIC CIRCUIT 7
CKT#8	EXISTING WINDCONE/RUNWAY 22 PAPI
CKT#10	EXISTING RUNWAY 22 REIL
CKT#17	EXISTING RUNWAY 16 PAPI
X	ITEM TO BE REMOVED
	PROPOSED PAVEMENT REMOVALS

NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. CONTRACTOR TO TAKE MEASURES TO PROTECT ALL UNDERGROUND UTILITIES INCLUDING, BUT NOT LIMITED TO, NEW AND EXISTING SHALLOW STORM SEWER PIPE FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
3. ITEMS REMOVED DUE TO PROPOSED PAVEMENT EXCAVATION WILL NOT BE PAID FOR SEPARATELY BUT WILL BE CONSIDERED INCIDENTAL TO UNCLASSIFIED EXCAVATION UNLESS OTHERWISE NOTED ON THE PLANS.
4. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
6. ALL EDGE LIGHTS AND SIGNS THAT ARE TO BE REMOVED SHALL BE SALVAGED AND RETURNED TO THE AIRPORT. IF THE AIRPORT ELECTS NOT TO SALVAGE ANY EQUIPMENT, CONTRACTOR SHALL DISPOSE OF ALL EQUIPMENT OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACT. LIGHT BASES AND SIGN FOUNDATIONS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
7. ALL EXISTING CONDUCTORS BETWEEN LIGHTS AND SIGNS ARE TO BE REMOVED, UNLESS OTHERWISE NOTED. ABANDON CONDUIT IN PLACE IF IT IS NOT IN CONFLICT WITH NEW CONSTRUCTION.
8. ALL LIGHT REMOVALS SHALL INCLUDE COMPLETE REMOVAL OF LIGHT FIXTURES, TRANSFORMERS, AND BASE CANS / STAKE MOUNT UNLESS OTHERWISE NOTED.
9. ALL SIGN REMOVALS SHALL INCLUDE COMPLETE REMOVAL OF SIGNS, ISOLATION TRANSFORMERS AND FOUNDATION, UNLESS OTHERWISE NOTED.
10. ALL BASE-MOUNTED RUNWAY EDGE LIGHTS TO BE REMOVED SHALL BE DONE CAREFULLY TO AVOID DAMAGE, PRESERVED AND RELOCATED DURING NEW CONSTRUCTION.
11. EXISTING FAA POWER AND COMMUNICATIONS CABLES TO BE REMOVED SHALL BE COORDINATED WITH FAA. CONTRACTOR SHALL REMOVE ALL CABLES AND ASSOCIATED GUARD WIRE AND GROUND RODS. COST OF REMOVALS SHALL BE INCIDENTAL TO NEW CABLES.

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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EXISTING CONDITIONS/ PROPOSED REMOVALS - 2

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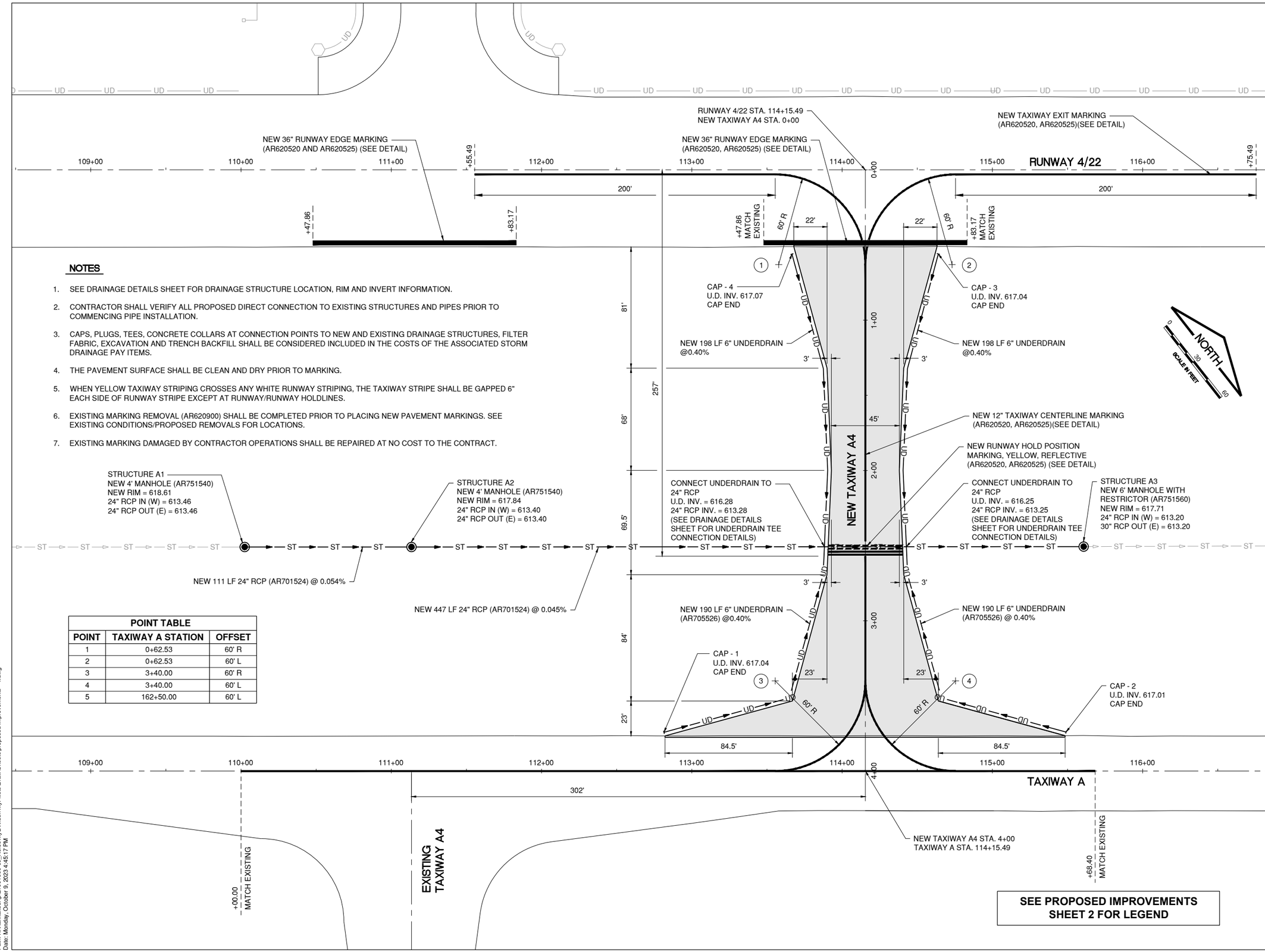

NOTES

1. SEE DRAINAGE DETAILS SHEET FOR DRAINAGE STRUCTURE LOCATION, RIM AND INVERT INFORMATION.
2. CONTRACTOR SHALL VERIFY ALL PROPOSED DIRECT CONNECTION TO EXISTING STRUCTURES AND PIPES PRIOR TO COMMENCING PIPE INSTALLATION.
3. CAPS, PLUGS, TEES, CONCRETE COLLARS AT CONNECTION POINTS TO NEW AND EXISTING DRAINAGE STRUCTURES, FILTER FABRIC, EXCAVATION AND TRENCH BACKFILL SHALL BE CONSIDERED INCLUDED IN THE COSTS OF THE ASSOCIATED STORM DRAINAGE PAY ITEMS.
4. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
5. WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT AT RUNWAY/RUNWAY HOLDLINES.
6. EXISTING MARKING REMOVAL (AR620900) SHALL BE COMPLETED PRIOR TO PLACING NEW PAVEMENT MARKINGS. SEE EXISTING CONDITIONS/PROPOSED REMOVALS FOR LOCATIONS.
7. EXISTING MARKING DAMAGED BY CONTRACTOR OPERATIONS SHALL BE REPAIRED AT NO COST TO THE CONTRACT.

POINT TABLE

POINT	TAXIWAY A STATION	OFFSET
1	0+62.53	60' R
2	0+62.53	60' L
3	3+40.00	60' R
4	3+40.00	60' L
5	162+50.00	60' L

SEE PROPOSED IMPROVEMENTS SHEET 2 FOR LEGEND

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Date: Monday, October 9, 2023 4:45:17 PM

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FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
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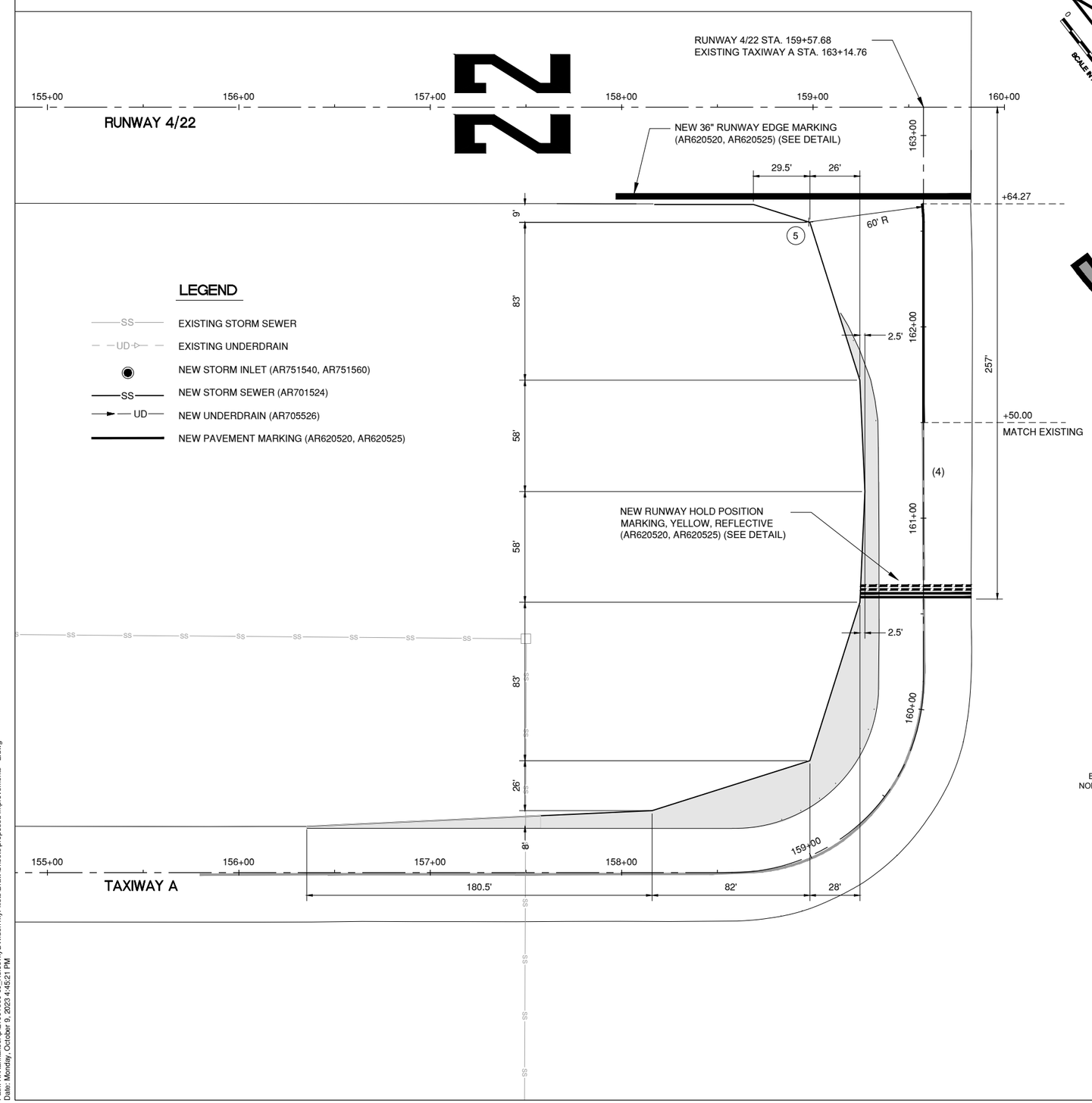
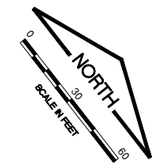
SHEET TITLE

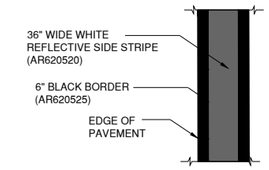
PROPOSED IMPROVEMENTS - 1



LEGEND

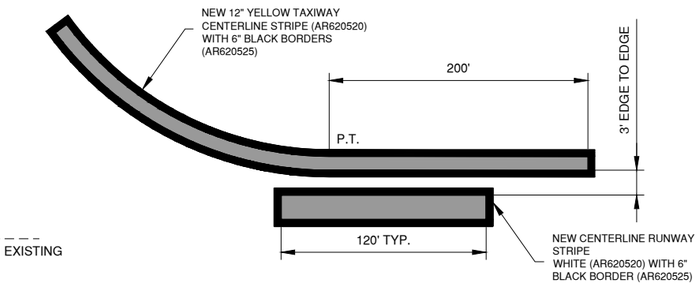
- SS — EXISTING STORM SEWER
- UD — EXISTING UNDERDRAIN
- NEW STORM INLET (AR751540, AR751560)
- SS — NEW STORM SEWER (AR701524)
- UD — NEW UNDERDRAIN (AR705526)
- NEW PAVEMENT MARKING (AR620520, AR620525)





RUNWAY EDGE MARKING CONTINUOUS

NOT TO SCALE



NEW TAXIWAY EXIT MARKING DETAIL

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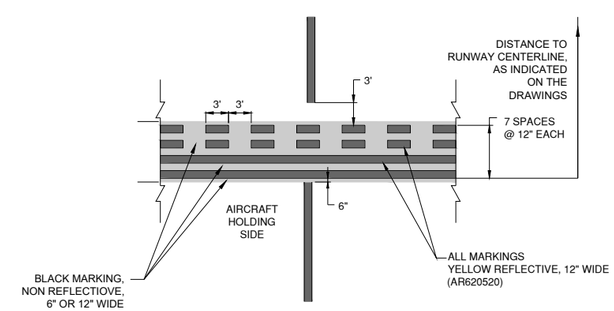

RUNWAY HOLDING POSITION MARKING

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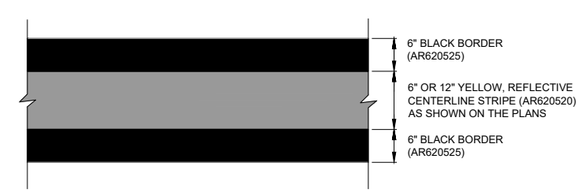

TAXIWAY / TAXILANE CENTERLINE DETAIL

NOT TO SCALE

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

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PROPOSED IMPROVEMENTS - 2

SHEET 18 OF 38

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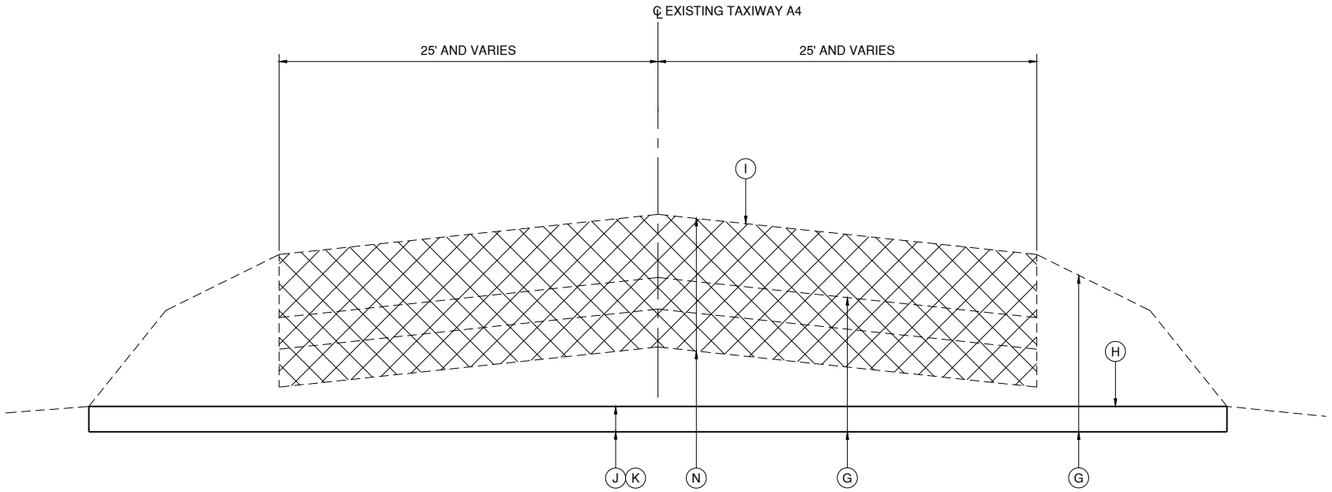
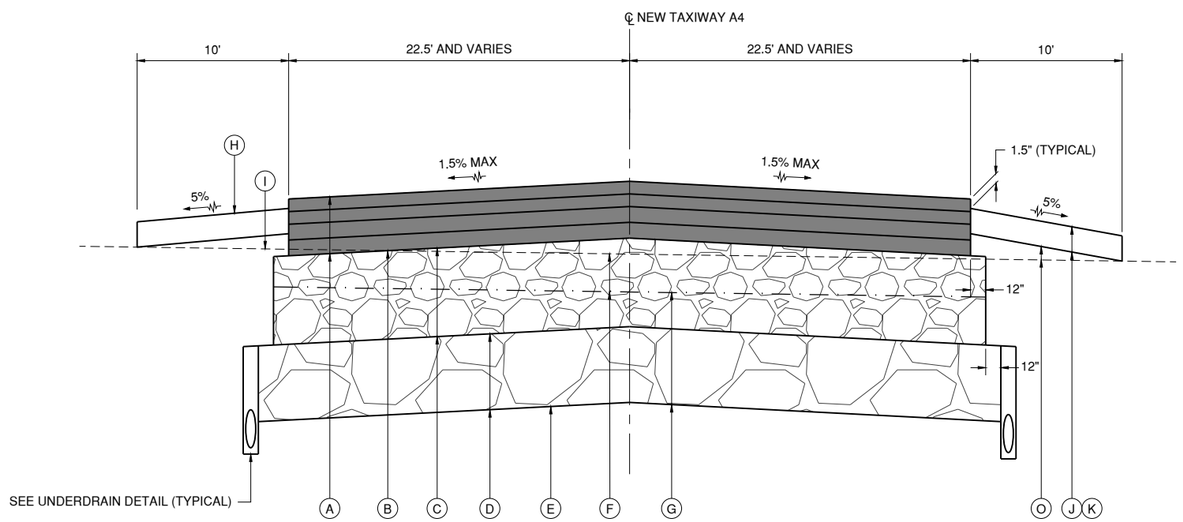


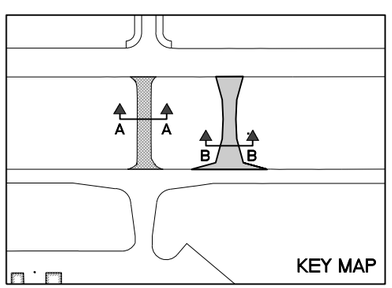
EXISTING TAXIWAY A4 TYPICAL SECTION A-A

NOT TO SCALE



NEW TAXIWAY A4 TYPICAL SECTION B-B (CUT SECTION)

NOT TO SCALE

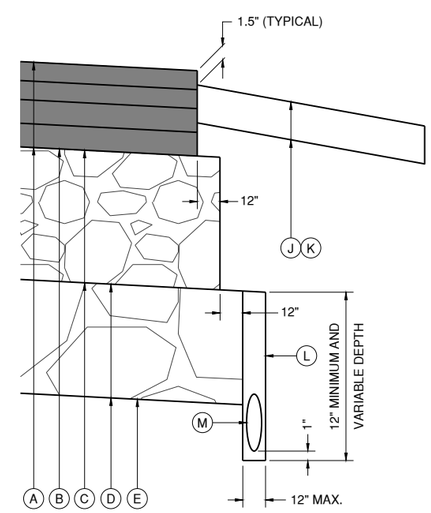


GENERAL NOTES

1. TOPSOIL SHALL BE STRIPPED BELOW NEW PAVEMENT LIMITS PRIOR TO PLACING EMBANKMENT FILL. AVERAGE TOPSOIL DEPTH AT THE PROJECT SITE IS 6-INCHES.
2. A LAYER OF TOPSOIL, MINIMUM 4-INCHES IN THICKNESS, SHALL BE SPREAD OVER THE DISTURBED LIMITS TO PROMOTE THE ESTABLISHMENT OF TURF.
3. AT CUT LOCATIONS OUTSIDE OF NEW PAVEMENT LIMITS, THE CONTRACTOR SHALL OVER-EXCAVATE AS REQUIRED TO ENSURE A 4-INCH MINIMUM THICKNESS LAYER OF TOPSOIL IS ABLE TO BE PLACED.

LEGEND

- (A) NEW 9" BITUMINOUS PAVEMENT WITH TACK COAT BETWEEN EACH LIFT (AR603510)
  - 2" SURFACE COURSE (AR401610)
  - 2" SURFACE COURSE (AR401610)
  - 2-1/2" BASE COURSE (AR403610)
  - 2-1/2" BASE COURSE (AR403610)
- (B) NEW BITUMINOUS PRIME COAT (AR602510)
- (C) NEW 15" CRUSHED AGGREGATE BASE COURSE (AR800026)
- (D) NEW 12" POROUS GRANULAR EMBANKMENT (AR208515)
- (E) NEW SOIL STABILIZATION FABRIC (AR152540)
- (F) NEW 12" (AVG.) TOPSOIL STRIPPING (AR152410)
- (G) NEW UNCLASSIFIED EXCAVATION (AR152410)
- (H) NEW GROUND LINE
- (I) EXISTING GROUND LINE
- (J) NEW 4" TOPSOIL PLACEMENT (905)
- (K) NEW SEEDING AND MULCHING (AR901510 AND AR908515)
- (L) NEW POROUS BACKFILL AND UNDERDRAIN TRENCH ENVELOPE (705)
- (M) NEW 6" PERFORATED PVC UNDERDRAIN PIPE (AR705526)
- (N) REMOVE EXISTING PAVEMENT
  - 10" BITUMINOUS (AR401900)
  - 5" WATERBOUND MACADAM (AR152410)
  - 6" GRANULAR SUBBASE (AR152410)
- (O) NEW SHOULDER FILL (152)



UNDERDRAIN DETAIL
EDGE OF BITUMINOUS PAVEMENT AREAS

NOT TO SCALE

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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SHEET TITLE

TYPICAL SECTIONS - 1

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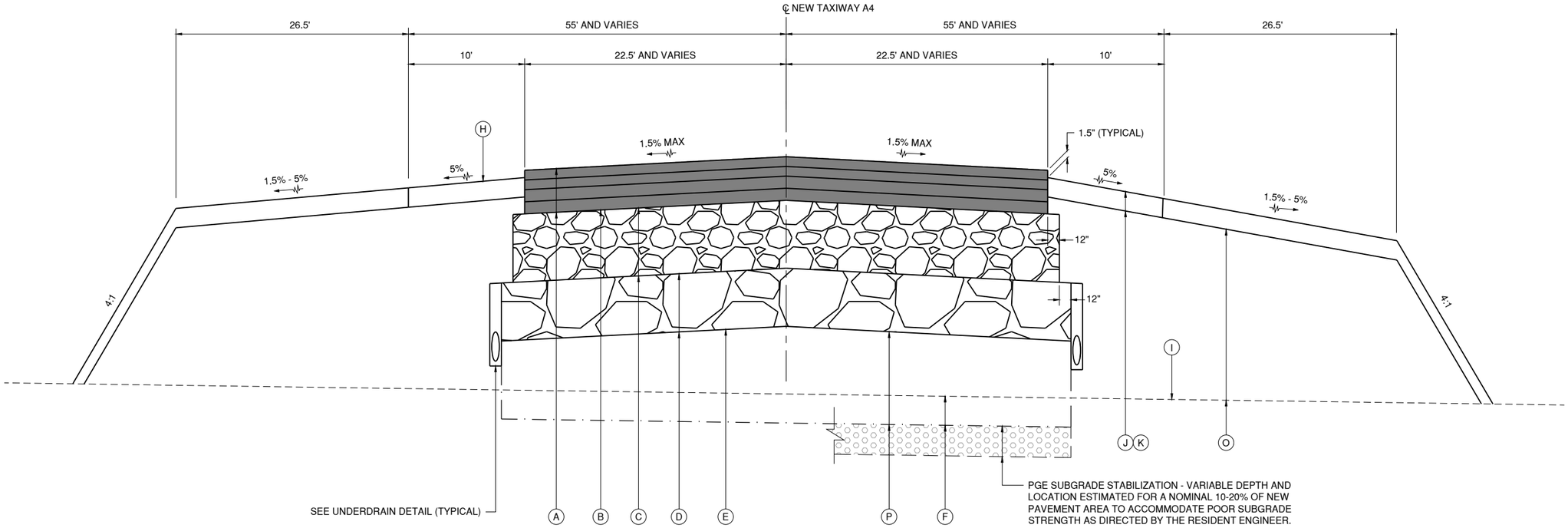
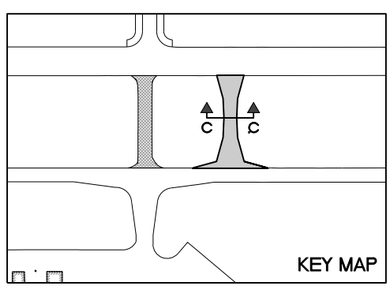


NEW TAXIWAY A4 TYPICAL SECTION C-C (FILL SECTION)

NOT TO SCALE

GENERAL NOTES

1. TOPSOIL SHALL BE STRIPPED BELOW NEW PAVEMENT LIMITS PRIOR TO PLACING EMBANKMENT FILL. AVERAGE TOPSOIL DEPTH AT THE PROJECT SITE IS 12-INCHES.
2. A LAYER OF TOPSOIL, MINIMUM 4-INCHES IN THICKNESS, SHALL BE SPREAD OVER THE DISTURBED LIMITS TO PROMOTE THE ESTABLISHMENT OF TURF.
3. AT CUT LOCATIONS OUTSIDE OF NEW PAVEMENT LIMITS, THE CONTRACTOR SHALL OVER-EXCAVATE AS REQUIRED TO ENSURE A 4-INCH MINIMUM THICKNESS LAYER OF TOPSOIL IS ABLE TO BE PLACED.



LEGEND

- (A) NEW 9" BITUMINOUS PAVEMENT WITH TACK COAT BETWEEN EACH LIFT (AR603510)
  - 2" SURFACE COURSE (AR401610)
  - 2" SURFACE COURSE (AR401610)
  - 2-1/2" BASE COURSE (AR403610)
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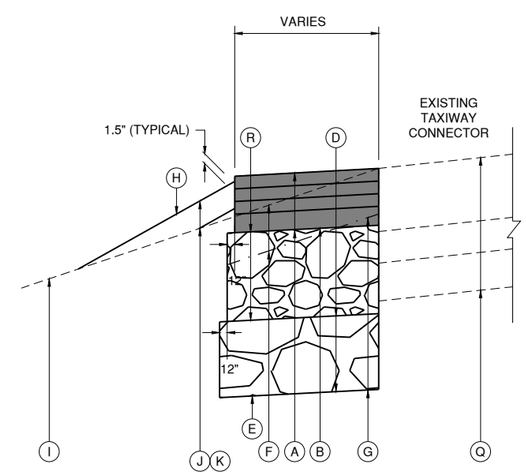
- (F) NEW 12" (AVG.) TOPSOIL STRIPPING (AR152410)
- (G) NEW UNCLASSIFIED EXCAVATION (AR152410)
- (H) NEW GROUND LINE
- (I) EXISTING GROUND LINE
- (J) NEW 4" TOPSOIL PLACEMENT (905)
- (K) NEW SEEDING AND MULCHING (AR901510 AND AR908515)
- (L) NEW POROUS BACKFILL AND UNDERDRAIN TRENCH ENVELOPE (705)
- (M) NEW 6" PERFORATED PVC UNDERDRAIN PIPE (AR705526)
- (N) REMOVE EXISTING PAVEMENT
  - 10" BITUMINOUS (AR401900)
  - 5" WATERBOUND MACADAM (AR152410)
  - 6" GRANULAR SUBBASE (AR152410)
- (O) NEW SHOULDER FILL (152)
- (P) NEW EMBANKMENT FILL (152)
- (Q) EXISTING 9" BITUMINOUS
  - 6" CRUSHED AGGREGATE BASE (209)
  - 6" AGGREGATE BASE (209)
- (R) NEW 12" CRUSHED AGGREGATE BASE COURSE (AR209612)



RUNWAY 22 END TYPICAL SECTION D-D

NOT TO SCALE

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

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CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
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SHEET TITLE

TYPICAL SECTIONS - 2

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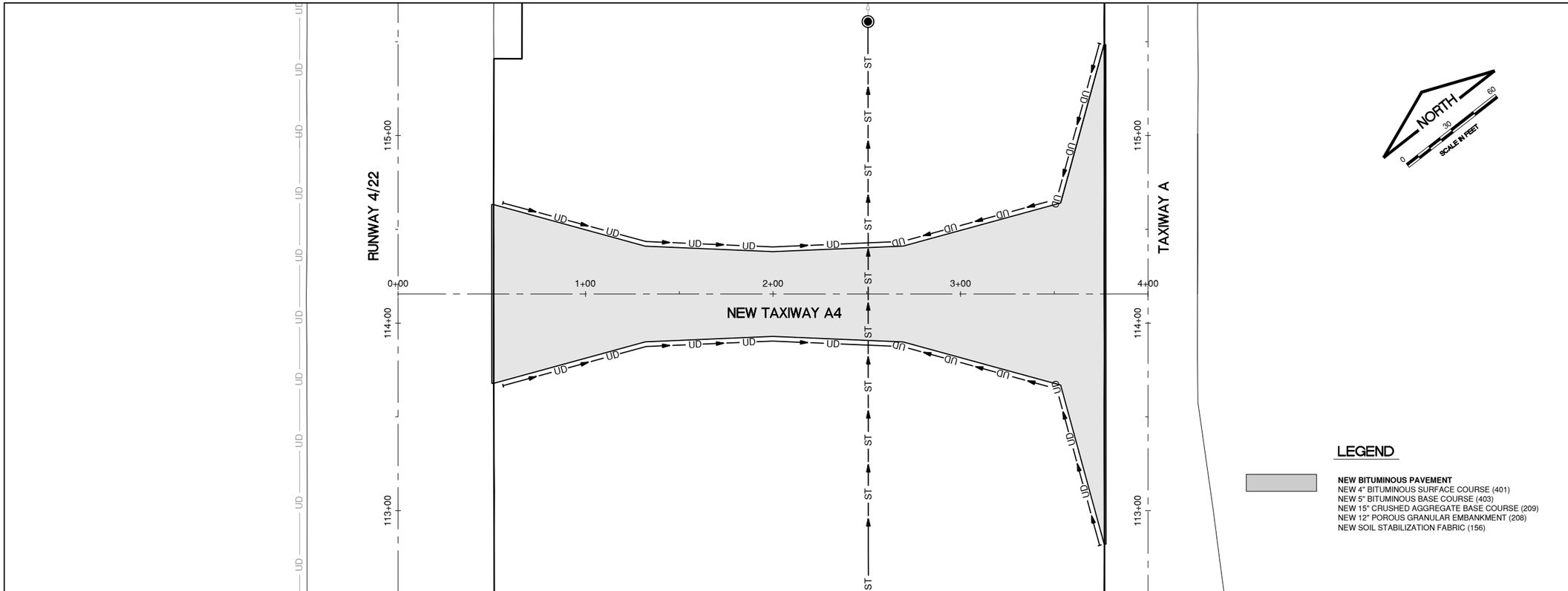

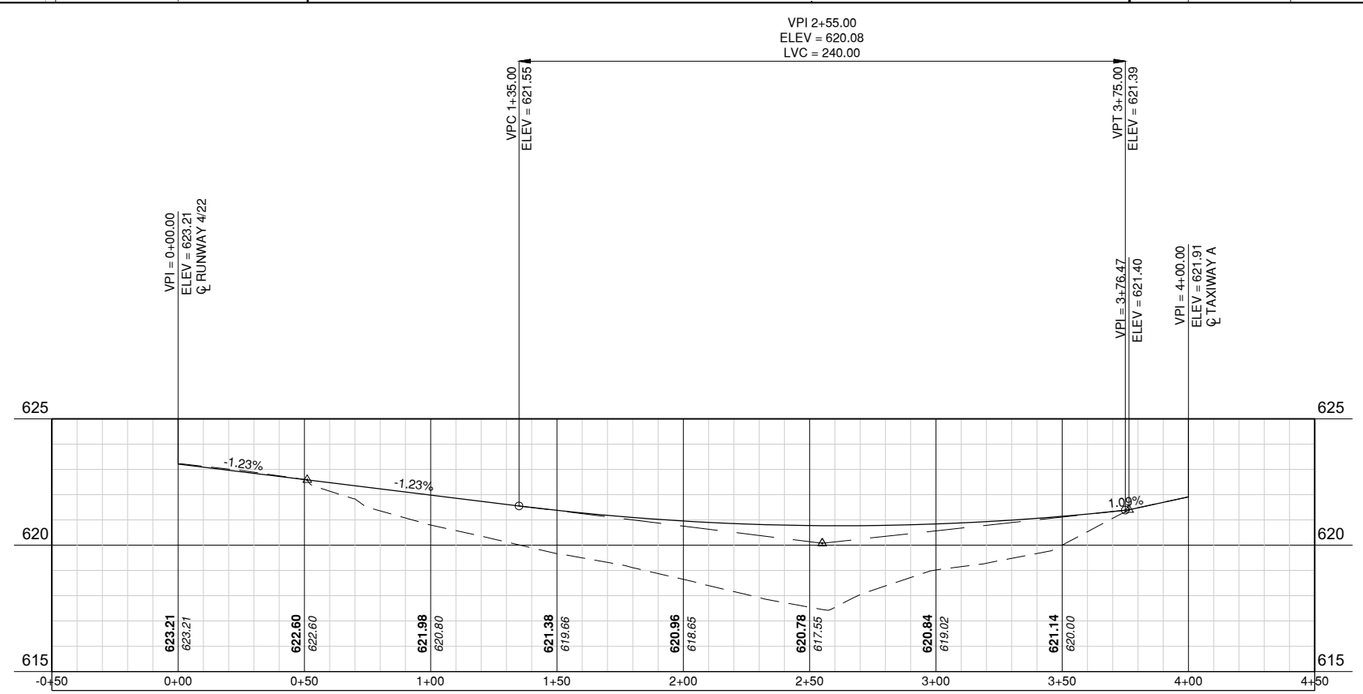

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SEPTEMBER 22, 2023

OWNER

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SHEET TITLE

TAXIWAY A4 PLAN AND PROFILE

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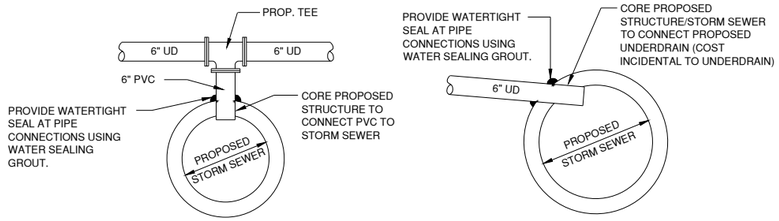


UNDERDRAIN CONNECTION DETAILS

NOT TO SCALE

UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO PROPOSED STRUCTURES AND STORM SEWERS / EXISTING STRUCTURES AND STORM SEWERS, SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED UNDERDRAIN.

STRUCTURE SCHEDULE

STRUCTURE	TYPE	RIM	INVERT	STATION (OFFSET)
A1	NEW 4' MANHOLE WITH TYPE 1 FRAME AND OPEN LID	618.61	EXISTING 24" IN (W) = 613.46 NEW 24" OUT (E) = 613.46	STA. 110+02.45, 250.92' RT BASELINE RUNWAY 4/22
A2	NEW 4' MANHOLE WITH TYPE 1 FRAME AND OPEN LID	617.84	NEW 24" IN (W) = 613.40 NEW 24" OUT (E) = 613.40	STA. 111+13.45, 250.87' RT BASELINE RUNWAY 4/22
A3	NEW 6' RESTRICTOR MANHOLE WITH TYPE 8 GRATE	617.71	NEW 24" IN (W) = 613.20 EXISTING 30" OUT (E) = 613.20	STA. 115+60.64, 250.66' RT BASELINE RUNWAY 4/22

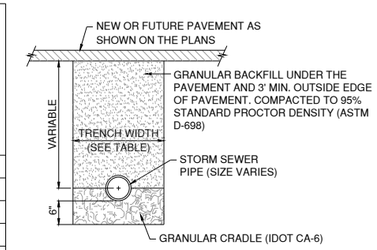
STRUCTURE SCHEDULE NOTES:

1. THE STATION AND OFFSET IS MEASURED TO THE CENTER OF THE STRUCTURE.
2. D/S: DOWNSTREAM.
3. U/S: UPSTREAM.
4. LENGTH OF PIPE FOR MANHOLE TO MANHOLE IS FROM CENTER OF STRUCTURE.
5. RCP: REINFORCED CONCRETE PIPE, CLASS IV.
6. PVC: POLYVINYL CHLORIDE PIPE, SDR 26.
7. MANHOLES SHALL BE IDOT STANDARD 602401-07 AND 602406-11.
8. CONTRACTOR SHALL VERIFY RIM AND INVERT ELEVATIONS ON EXISTING DRAINAGE STRUCTURES/PIPES THAT ARE TO BE CONNECTED TOBEFORE ORDERING MATERIAL (INCIDENTAL TO CONTRACT).
9. FRAME AND LIDS SHALL BE IDOT STANDARD 604001-05 AND 604036-03.

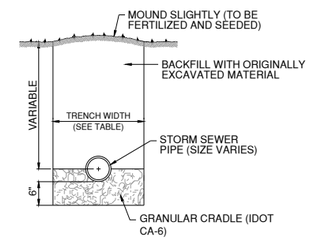
STORM SEWER/UNDERDRAIN NOTES

1. CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
2. ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCLUDED IN THE COST OF THE UNDERDRAIN.
3. UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
4. INSTALL PROPOSED ELECTRICAL DUCTS/CONDUITS TO BE CLEAR OF UNDERDRAIN, COSTS INCLUDED.
5. UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.
6. CORING OF DRAINAGE STRUCTURE AND REMOVAL OF EXISTING STORM SEWER MANHOLE/INLET CONCRETE BENCHES TO FACILITATE CONNECTIONS OF PROPOSED STORM SEWER AND UNDERDRAIN PIPE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PIPE.

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-7"
8	3'-9"
12	4'-2"
15	4'-6"
18	4'-9"
21	5'-0"
24	5'-4"
27	5'-7"
30	5'-11"
36	6'-6"
42	7'-1"
48	7'-8"
54	8'-3"
60	8'-10"
66	9'-5"
72	10'-0"
78	10'-7"
84	11'-2"
90	11'-9"
96	12'-4"
102	12'-11"
108	13'-6"



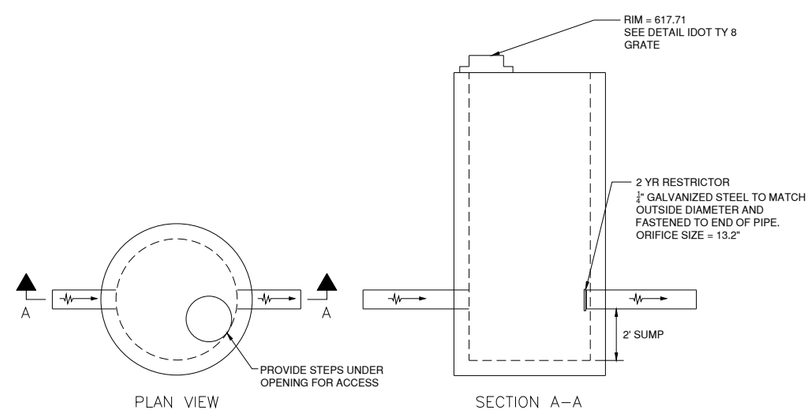
ALL PAVED AREAS



NON-PAVED AREAS

TRENCH DETAILS STORM SEWER

N.T.S.



NOTES:

1. RESTRICTOR MANHOLE IS AN IDOT MODIFIED MANHOLE AS SHOWN.
2. USE SNOUT WITH ANTISIPHON DEVICE FOR RESTRICTORS SMALLER THAN 4" IN DIAMETER.

RESTRICTOR MANHOLE DETAIL

NOT TO SCALE

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SHEET TITLE

DRAINAGE DETAILS - 1

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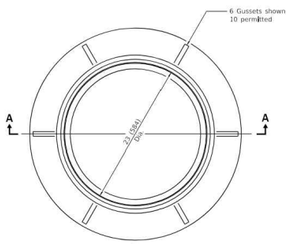
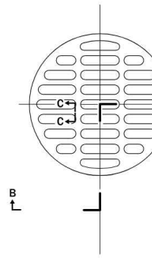
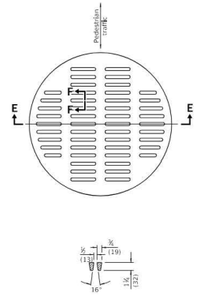
Date: Monday, October 9, 2023 4:45:45 PM



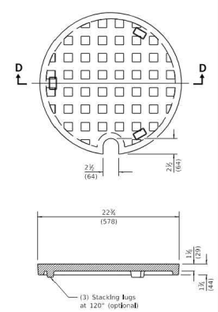
CAST FRAME



SECTION C-C

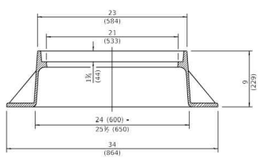


SECTION F-F

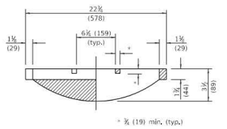


SECTION D-D

CAST CLOSED LID
Gray Iron Lid

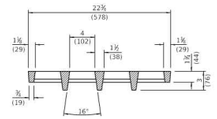


SECTION A-A
Gray Iron



SECTION B-B

CAST OPEN LID



SECTION E-E

ADA COMPLIANT
CAST OPEN LID

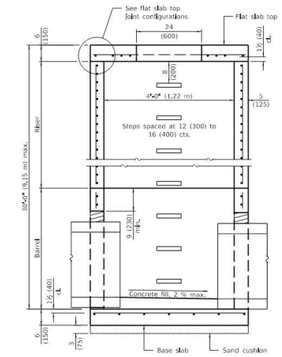
All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation
PASSED January 1, 2020
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2020
ENGINEER OF DESIGN AND ENVIRONMENT
BUREAU OF LR/PD

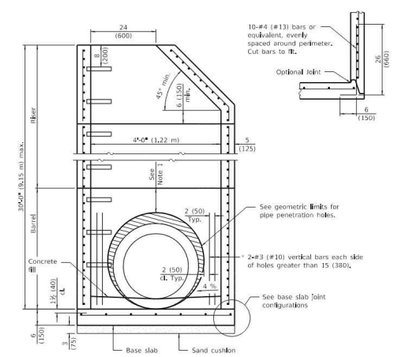
DATE	REVISIONS
1-1-20	Revised dimension in Section B-B of cast open lid.
1-1-15	Revised dimensioning of frame. Added ADA compliant open lid.
1-1-09	Switched units to English (metric).

FRAME AND LIDS TYPE 1

STANDARD 604001-05

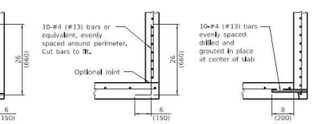


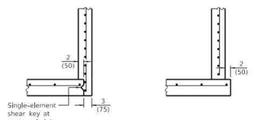
SECTION PARALLEL TO PIPE
(Without conical top riser)



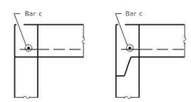
SECTION PERPENDICULAR TO PIPE
(With conical top riser)

* As an alternate, the barrel wall reinforcement may be reduced to riser wall reinforcement with #3 (#10) bars placed around the pipe penetration holes as shown. This option may be utilized when the pipe penetration holes are formed as opposed to cored.





BASE SLAB JOINT CONFIGURATIONS



FLAT SLAB TOP JOINT CONFIGURATIONS
(Shown at access hole)

GEOMETRIC LIMITS FOR PIPE PENETRATION HOLES

Note 1: A minimum of 9 (230) of monolithic reinforced concrete shall be maintained above pipe penetration holes > 24 (600).

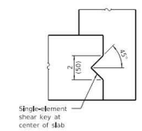
Note 2: A minimum 12 (300) inside arc length of reinforced concrete shall be maintained between pipe penetration holes > 15 (380).

Note 3: A maximum of 60 percent of the inside perimeter of the reinforced concrete manhole walls may be removed.

Note 4: Horizontal joints that intersect pipe penetration holes > 15 (380) shall have one joint splice for every location around the perimeter of the joint where the inside arc length between pipe penetration holes is < 24 (600). See joint splice detail.

Note 5: The recommended pipe penetration hole is equal to the O.D. of the pipe plus 4 (100).

Note 6: Only pipe penetration holes ≤ 15 (380) are allowed in riser sections.



SHEAR KEY GEOMETRY
(Reinforcement not shown for clarity)

GENERAL NOTES

The manufacturer shall ensure that all precast manhole sections are additionally reinforced where required to resist damage from handling, shipping and installation stresses.

Lifting holes shall be located in the sections as per the manufacturer's recommendations.

See Standard 602701 for details of manhole steps.

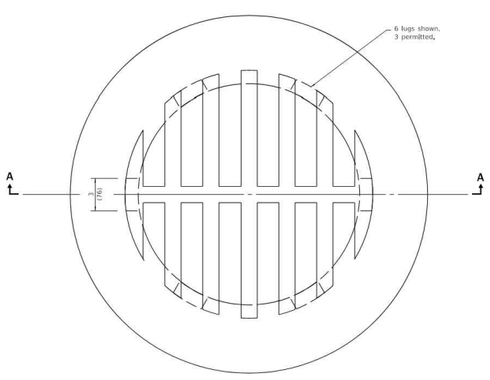
All dimensions are in inches (millimeters) unless otherwise noted.

Illinois Department of Transportation
PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2021
ENGINEER OF DESIGN AND ENVIRONMENT
BUREAU OF LR/PD

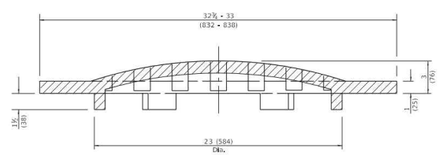
DATE	REVISIONS
1-1-21	Revised Note 1 and lifting hole general note.
3-1-19	Moved wall reinforcement from inside face to middle.

PRECAST MANHOLE TYPE A 4' (1.22 m) DIAMETER
(Sheet 1 of 2)

STANDARD 602401-07



CAST GRATE



SECTION A-A

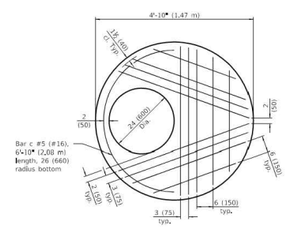
All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation
PASSED January 1, 2015
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2015
ENGINEER OF DESIGN AND ENVIRONMENT
BUREAU OF LR/PD

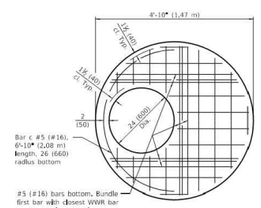
DATE	REVISIONS
1-1-15	Revised dimensions.
1-1-09	Switched units to English (metric).

GRATE TYPE 8

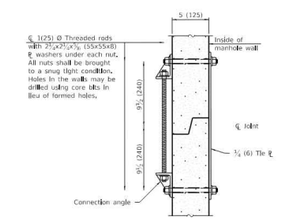
STANDARD 604036-03



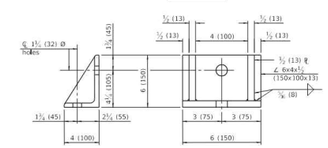
PLAN - FLAT SLAB TOP
(Showing layout of reinforcement bars and c bars)



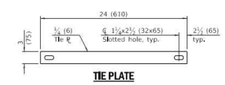
PLAN - FLAT SLAB TOP
(Showing layout of welded wire reinforcement and c bars)



JOINT SPLICE



CONNECTION ANGLE



TIE PLATE

FLAT SLAB TOP REINFORCEMENT

Location	WWR (each direction)		Rebar		
	A_s (min.)	Spacing (max.)	A_s (min.)	Spacing (max.)	Bar Size
Bottom Mat	** 0.62 sq. in./ft. (1312 sq. mm/m)	6 (150)	See plan view for rebar orientation and spacing and this table for bar size		#5 (#16)

** Only one layer of WWR permitted to avoid congestion.

WALL REINFORCEMENT

Location	Orientation	WWR or Rebar	
		A_s (min.)	Spacing (max.)
Riser	Circumferential	0.12 sq. in./ft. (254 sq. mm/m)	6 (150)
	Vertical	0.045 sq. in./ft. (95 sq. mm/m)	8 (200)
Barrel	Circumferential	0.12 sq. in./ft. (254 sq. mm/m)	6 (150)
	Vertical	0.16 sq. in./ft. (339 sq. mm/m)	4 (100)

BASE SLAB REINFORCEMENT

Location	Total Height	WWR or Rebar (each direction)	
		A_s (min.)	Spacing (max.)
Top Mat	≤ 20 ft. (6.10 m)	0.24 sq. in./ft. (508 sq. mm/m)	10 (250)
	> 20 ft. (6.10 m)	0.24 sq. in./ft. (508 sq. mm/m)	10 (250)

Illinois Department of Transportation
PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2021
ENGINEER OF DESIGN AND ENVIRONMENT
BUREAU OF LR/PD

PRECAST MANHOLE TYPE A 4' (1.22 m) DIAMETER
(Sheet 2 of 2)

STANDARD 602401-07

KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

DRAINAGE DETAILS - 2

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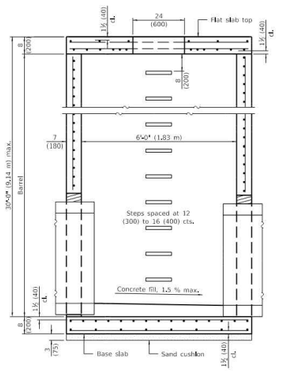
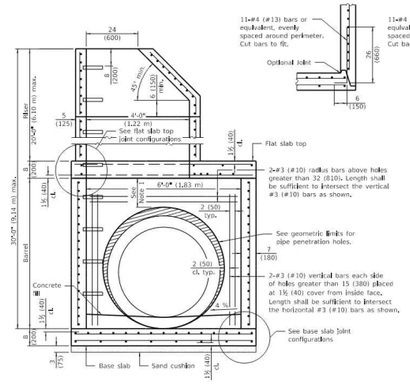
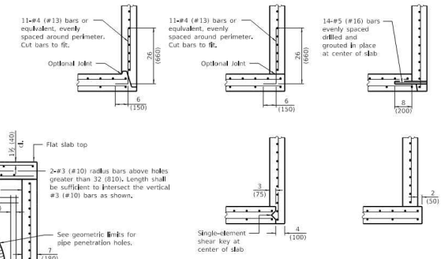


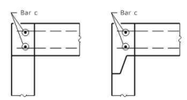
SECTION PARALLEL TO PIPE
(Without conical top riser)



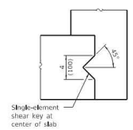
SECTION PERPENDICULAR TO PIPE
(With conical top riser)



BASE SLAB JOINT CONFIGURATIONS



FLAT SLAB TOP JOINT CONFIGURATIONS
(Shown at access hole)



SHEAR KEY GEOMETRY
(Reinforcement not shown for clarity)

GEOMETRIC LIMITS FOR PIPE PENETRATION HOLES

Note 1: A minimum of 9 (230) of monolithic reinforced concrete shall be maintained above pipe penetration holes > 32 (810).

Note 2: A minimum 12 (300) inside arc length of reinforced concrete shall be maintained between pipe penetration holes > 15 (380).

Note 3: A maximum of 60 percent of the inside perimeter of the reinforced concrete manhole walls may be removed.

Note 4: Horizontal joints that intersect pipe penetration holes > 15 (380) shall have one joint splice for every location around the perimeter of the joint where the inside arc length between pipe penetration holes is < 24 (600). See joint splice detail.

Note 5: The recommended pipe penetration hole is equal to the O.D. of the pipe plus 4 (100).

Note 6: Only pipe penetration holes ≥ 15 (380) are allowed in riser sections.

GENERAL NOTES

Pipe holes shall be formed to facilitate proper placement of hole reinforcement.

The manufacturer shall ensure that all precast manhole sections are additionally reinforced where required to resist damage from handling, shipping and installation stresses.

Lifting holes shall be located in the sections as per the manufacturer's recommendations.

See Standard 602701 for details of manhole steps.

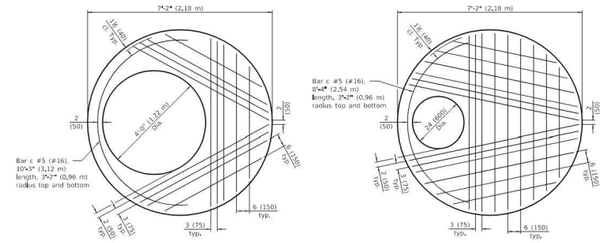
All dimensions are in inches (millimeters) unless otherwise noted.

DATE	REVISIONS
1-1-21	Revised Note 1, Note 2 and lifting hole general note.
3-1-19	Moved wall reinforcement from inside face to middle.

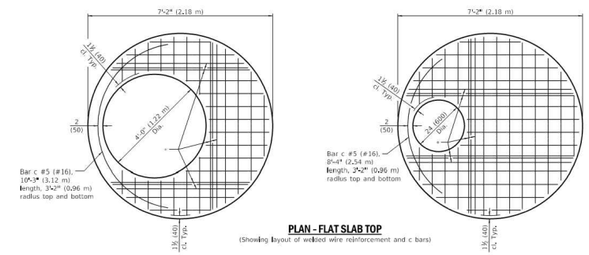
Illinois Department of Transportation
PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2021
ENGINEER OF BUREAU OF ENVIRONMENT
BDE 5400 (Rev. 1-1-21)

PRECAST MANHOLE TYPE A
6' (1.83 m) DIAMETER
(Sheet 1 of 3)

STANDARD 602406-11



PLAN - FLAT SLAB TOP
(Showing layout of bottom reinforcement bars and c bars)



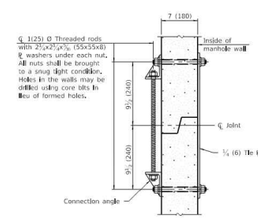
PLAN - FLAT SLAB TOP
(Showing layout of welded wire reinforcement and c bars)

* #5 (#16) bars for risers ≤ 10 ft. (3.05 m) tall or #6 (#19) bars for risers > 10 ft. (3.05 m) tall bottom. Bundle first bar with closest WWR bar to the opening and place second bar ≥3 (75) away.

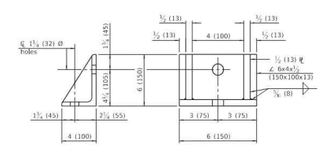
Illinois Department of Transportation
PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2021
ENGINEER OF BUREAU OF ENVIRONMENT
BDE 5400 (Rev. 1-1-21)

PRECAST MANHOLE TYPE A
6' (1.83 m) DIAMETER
(Sheet 2 of 3)

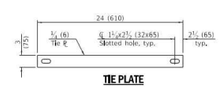
STANDARD 602406-11



JOINT SPLICE



CONNECTION ANGLE



TIE PLATE

FLAT SLAB TOP REINFORCEMENT

Location	Riser Height (RH)	WWR (each direction)		Rebar (each direction except as noted)		
		A_s (min.)	Spacing (max.)	A_s (min.)	Spacing (max.)	Bar Size
Top Mat	All	0.11 sq. in./ft. (233 sq. mm/m)	18 (450)	0.11 sq. in./ft. (233 sq. mm/m)	18 (450)	#3 or #4 (#10) (#13)
Bottom Mat	RH ≤ 10 ft. (3.05 m)	** 0.62 sq. in./ft. (1312 sq. mm/m)	6 (150)	See plan view for rebar orientation and spacing and this table for bar size		#5 (#16)
	RH > 10 ft. (3.05 m)	** 0.88 sq. in./ft. (1863 sq. mm/m)	6 (150)			#6 (#19)

** Only one layer of WWR permitted to avoid congestion.

WALL REINFORCEMENT

Location	Orientation	WWR or Rebar	
		A_s (min.)	Spacing (max.)
4 ft. (1.22 m) Ø Riser	Circumferential	0.12 sq. in./ft. (254 sq. mm/m)	6 (150)
	Vertical	0.045 sq. in./ft. (95 sq. mm/m)	8 (200)
6 ft. (1.83 m) Ø Barrel	Circumferential	0.18 sq. in./ft. (381 sq. mm/m)	6 (150)
	Vertical	0.045 sq. in./ft. (95 sq. mm/m)	8 (200)

BASE SLAB REINFORCEMENT

Location	Riser Height (RH)/ Total Height (TH)	WWR or Rebar (each direction)	
		A_s (min.)	Spacing (max.)
Top Mat	RH ≤ 10 ft. (3.05 m) & TH ≤ 20 ft. (6.10 m)	0.28 sq. in./ft. (593 sq. mm/m)	6 (150)
	RH > 10 ft. (3.05 m) or TH > 20 ft. (6.10 m)	0.40 sq. in./ft. (847 sq. mm/m)	6 (150)
Bottom Mat	All	0.11 sq. in./ft. (233 sq. mm/m)	18 (450)

Illinois Department of Transportation
PASSED January 1, 2021
ENGINEER OF POLICY AND PROCEDURES
APPROVED January 1, 2021
ENGINEER OF BUREAU OF ENVIRONMENT
BDE 5400 (Rev. 1-1-21)

PRECAST MANHOLE TYPE A
6' (1.83 m) DIAMETER
(Sheet 3 of 3)

STANDARD 602406-11

KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

DRAINAGE DETAILS - 3

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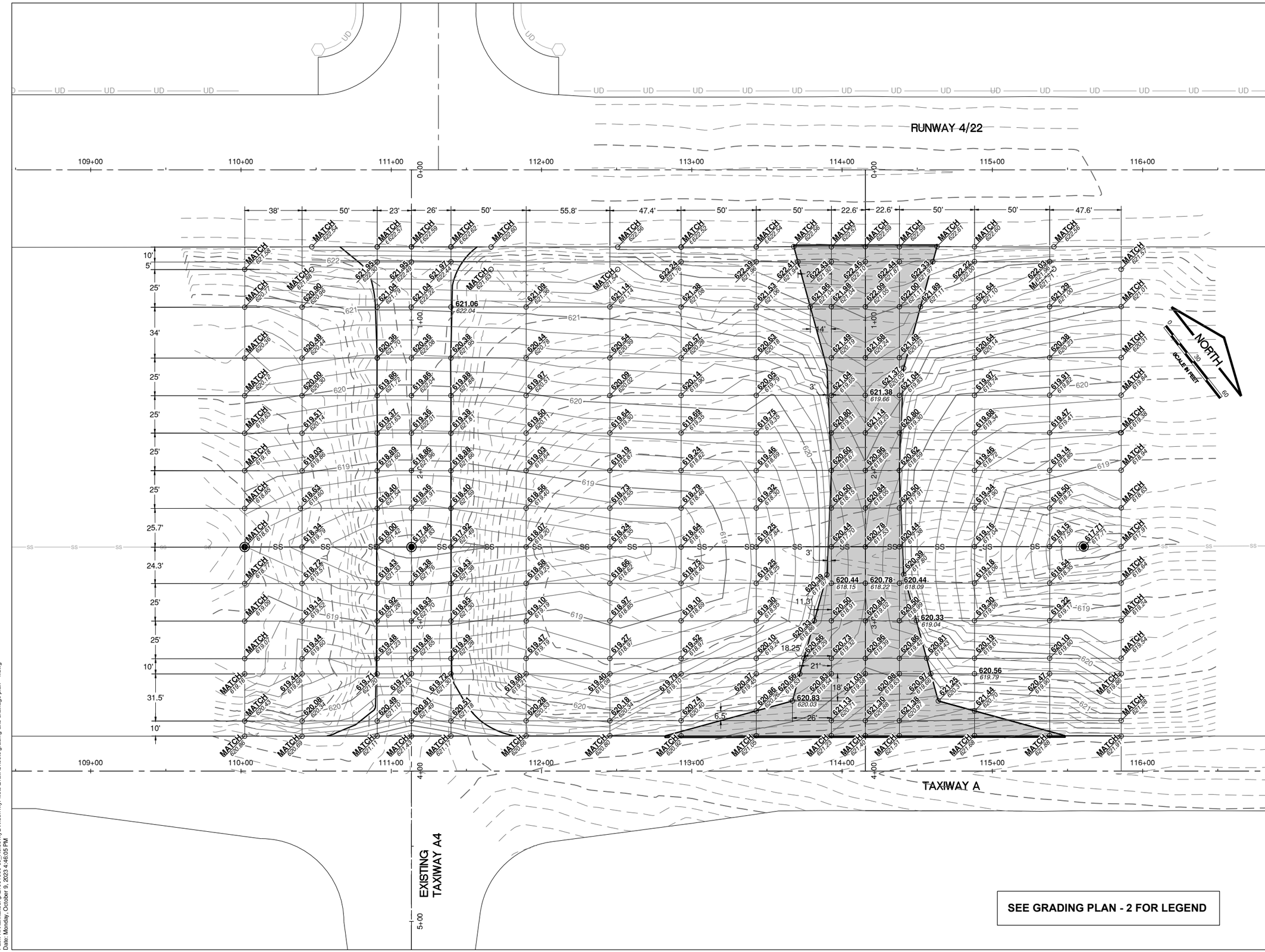

KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

GRADING PLAN - 1

SHEET 25 OF 38

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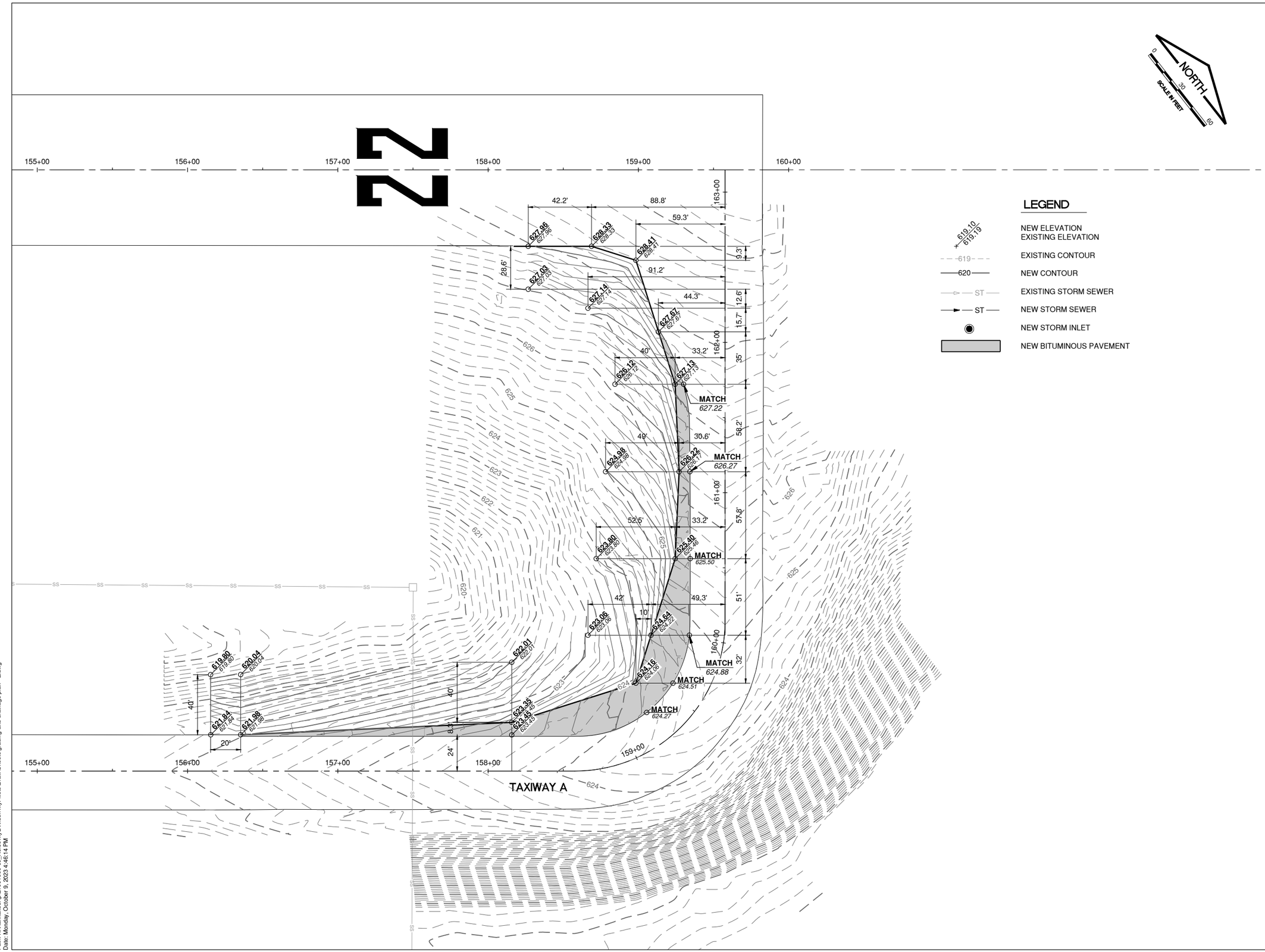

KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

GRADING PLAN - 2

SHEET 26 OF 38

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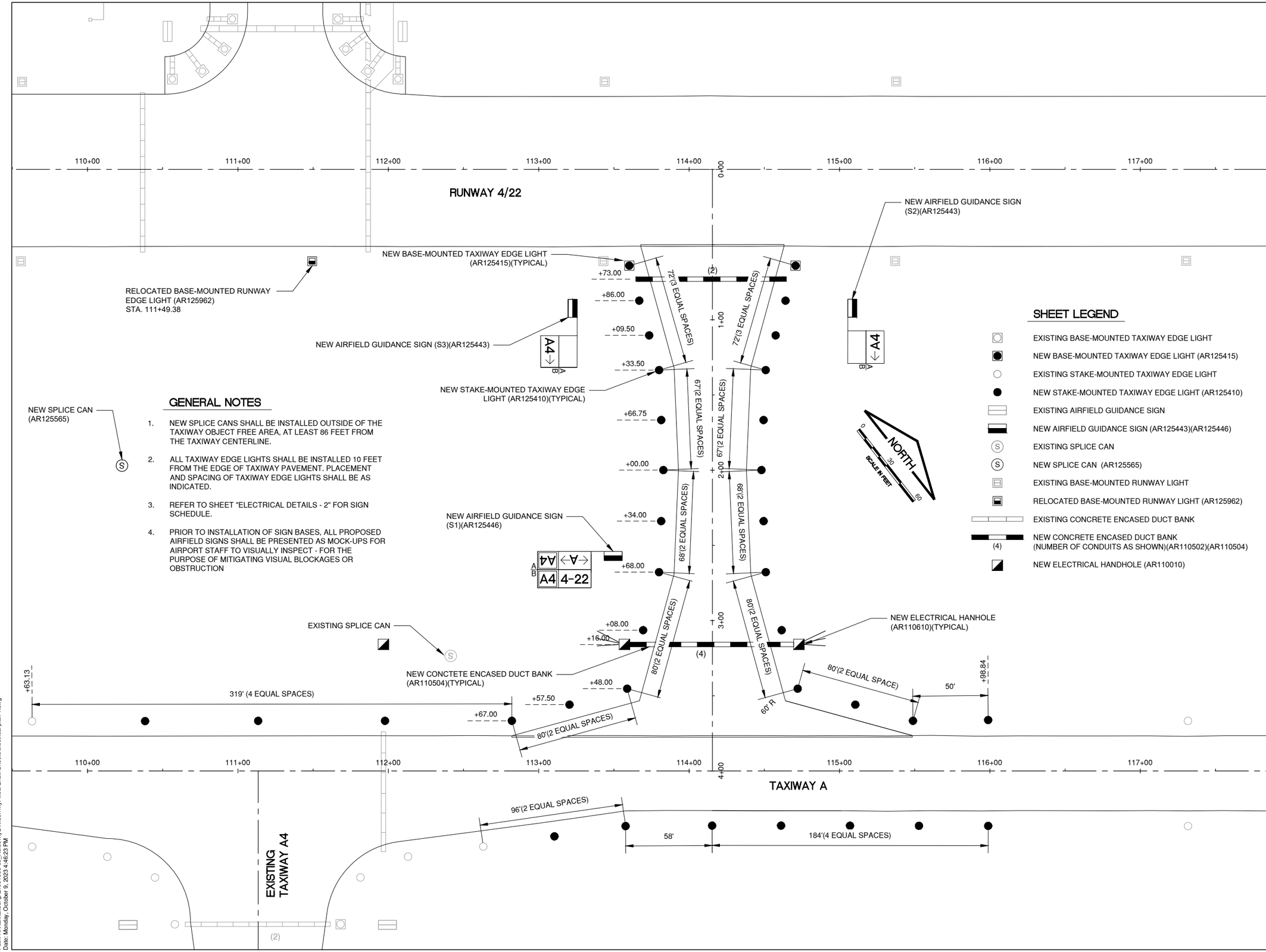

GENERAL NOTES

1. NEW SPLICE CANS SHALL BE INSTALLED OUTSIDE OF THE TAXIWAY OBJECT FREE AREA, AT LEAST 86 FEET FROM THE TAXIWAY CENTERLINE.
2. ALL TAXIWAY EDGE LIGHTS SHALL BE INSTALLED 10 FEET FROM THE EDGE OF TAXIWAY PAVEMENT. PLACEMENT AND SPACING OF TAXIWAY EDGE LIGHTS SHALL BE AS INDICATED.
3. REFER TO SHEET "ELECTRICAL DETAILS - 2" FOR SIGN SCHEDULE.
4. PRIOR TO INSTALLATION OF SIGN BASES, ALL PROPOSED AIRFIELD SIGNS SHALL BE PRESENTED AS MOCK-UPS FOR AIRPORT STAFF TO VISUALLY INSPECT - FOR THE PURPOSE OF MITIGATING VISUAL BLOCKAGES OR OBSTRUCTION

SHEET LEGEND

- EXISTING BASE-MOUNTED TAXIWAY EDGE LIGHT
- NEW BASE-MOUNTED TAXIWAY EDGE LIGHT (AR125415)
- EXISTING STAKE-MOUNTED TAXIWAY EDGE LIGHT
- NEW STAKE-MOUNTED TAXIWAY EDGE LIGHT (AR125410)
- EXISTING AIRFIELD GUIDANCE SIGN
- NEW AIRFIELD GUIDANCE SIGN (AR125443)(AR125446)
- EXISTING SPLICE CAN
- NEW SPLICE CAN (AR125565)
- EXISTING BASE-MOUNTED RUNWAY LIGHT
- RELOCATED BASE-MOUNTED RUNWAY LIGHT (AR125962)
- EXISTING CONCRETE ENCASED DUCT BANK
- (4) NEW CONCRETE ENCASED DUCT BANK (NUMBER OF CONDUITS AS SHOWN)(AR110502)(AR110504)
- NEW ELECTRICAL HANDHOLE (AR110010)

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FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
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SHEET TITLE

ELECTRICAL PLAN 1

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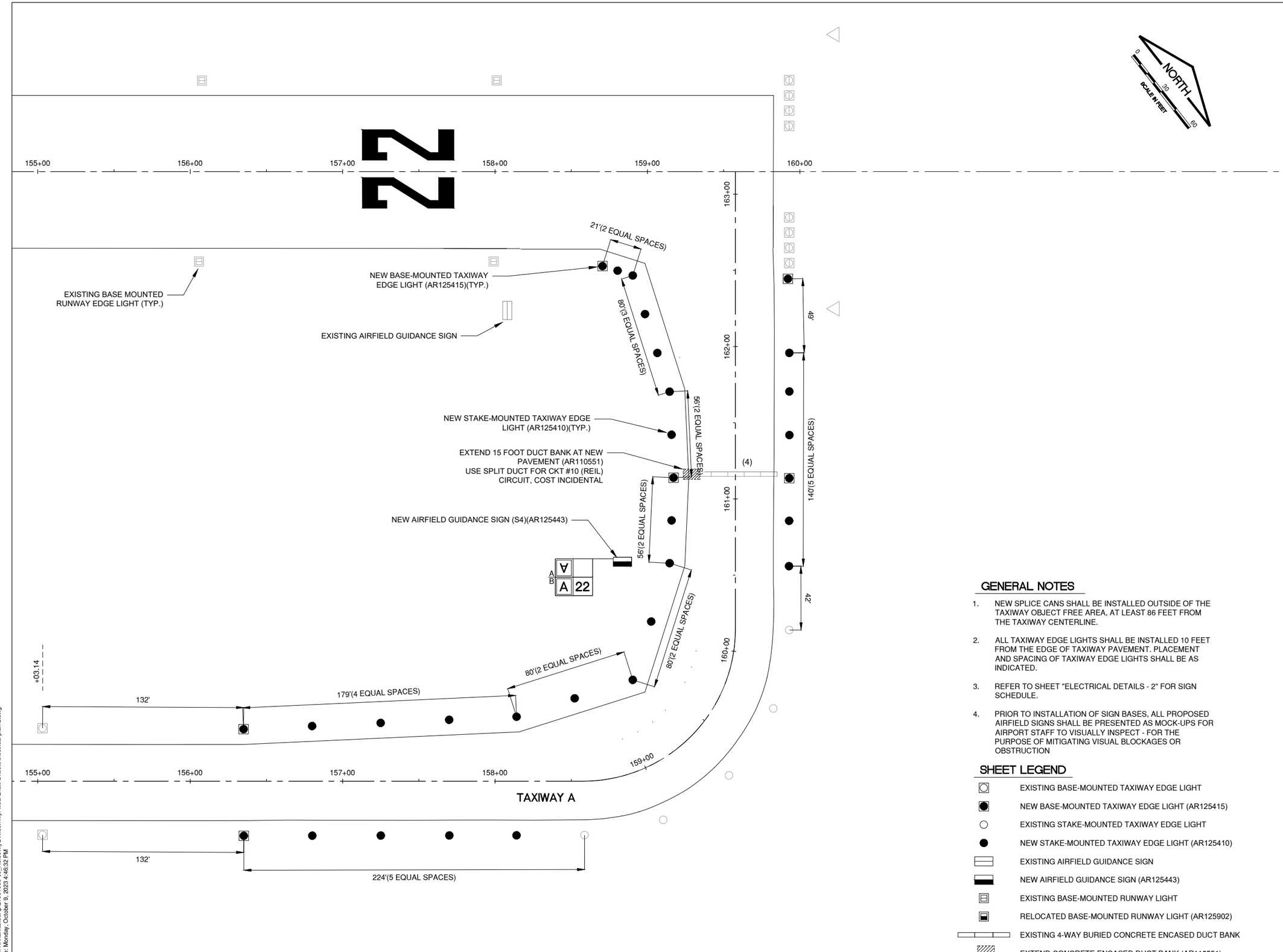

GENERAL NOTES

1. NEW SPLICE CANS SHALL BE INSTALLED OUTSIDE OF THE TAXIWAY OBJECT FREE AREA, AT LEAST 86 FEET FROM THE TAXIWAY CENTERLINE.
2. ALL TAXIWAY EDGE LIGHTS SHALL BE INSTALLED 10 FEET FROM THE EDGE OF TAXIWAY PAVEMENT. PLACEMENT AND SPACING OF TAXIWAY EDGE LIGHTS SHALL BE AS INDICATED.
3. REFER TO SHEET "ELECTRICAL DETAILS - 2" FOR SIGN SCHEDULE.
4. PRIOR TO INSTALLATION OF SIGN BASES, ALL PROPOSED AIRFIELD SIGNS SHALL BE PRESENTED AS MOCK-UPS FOR AIRPORT STAFF TO VISUALLY INSPECT - FOR THE PURPOSE OF MITIGATING VISUAL BLOCKAGES OR OBSTRUCTION

SHEET LEGEND

- EXISTING BASE-MOUNTED TAXIWAY EDGE LIGHT
- NEW BASE-MOUNTED TAXIWAY EDGE LIGHT (AR125415)
- EXISTING STAKE-MOUNTED TAXIWAY EDGE LIGHT
- NEW STAKE-MOUNTED TAXIWAY EDGE LIGHT (AR125410)
- EXISTING AIRFIELD GUIDANCE SIGN
- NEW AIRFIELD GUIDANCE SIGN (AR125443)
- EXISTING BASE-MOUNTED RUNWAY LIGHT
- RELOCATED BASE-MOUNTED RUNWAY LIGHT (AR125902)
- EXISTING 4-WAY BURIED CONCRETE ENCASED DUCT BANK
- EXTEND CONCRETE ENCASED DUCT BANK (AR110551)

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SHEET TITLE

ELECTRICAL PLAN 2

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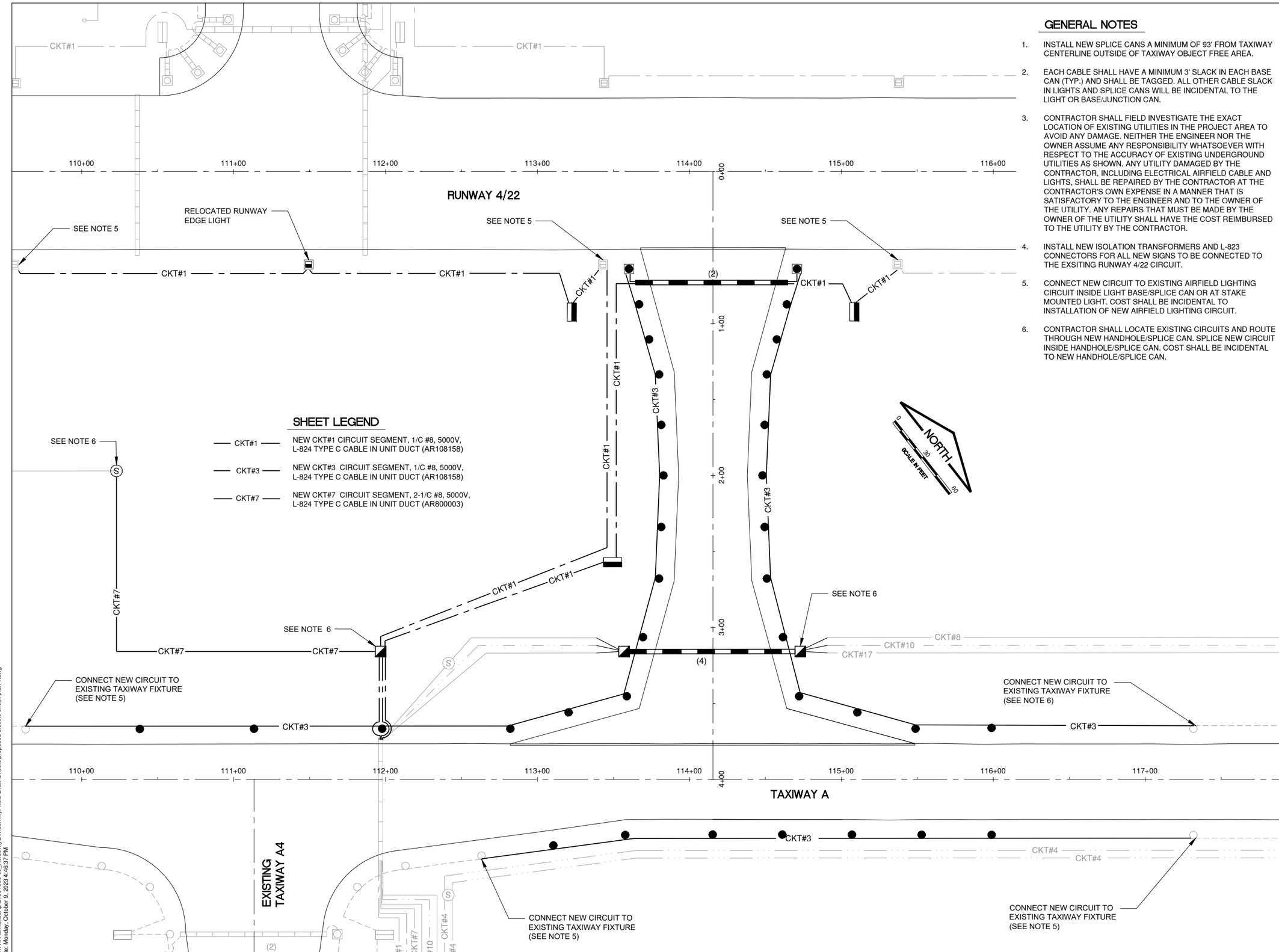

SHEET LEGEND

CKT#1	NEW CKT#1 CIRCUIT SEGMENT, 1/C #8, 5000V, L-824 TYPE C CABLE IN UNIT DUCT (AR108158)
CKT#3	NEW CKT#3 CIRCUIT SEGMENT, 1/C #8, 5000V, L-824 TYPE C CABLE IN UNIT DUCT (AR108158)
CKT#7	NEW CKT#7 CIRCUIT SEGMENT, 2-1/C #8, 5000V, L-824 TYPE C CABLE IN UNIT DUCT (AR800003)

GENERAL NOTES

1. INSTALL NEW SPLICE CANS A MINIMUM OF 93' FROM TAXIWAY CENTERLINE OUTSIDE OF TAXIWAY OBJECT FREE AREA.
2. EACH CABLE SHALL HAVE A MINIMUM 3' SLACK IN EACH BASE CAN (TYP.) AND SHALL BE TAGGED. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR BASE/JUNCTION CAN.
3. CONTRACTOR SHALL FIELD INVESTIGATE THE EXACT LOCATION OF EXISTING UTILITIES IN THE PROJECT AREA TO AVOID ANY DAMAGE. NEITHER THE ENGINEER NOR THE OWNER ASSUME ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY OF EXISTING UNDERGROUND UTILITIES AS SHOWN. ANY UTILITY DAMAGED BY THE CONTRACTOR, INCLUDING ELECTRICAL AIRFIELD CABLE AND LIGHTS, SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE IN A MANNER THAT IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR.
4. INSTALL NEW ISOLATION TRANSFORMERS AND L-823 CONNECTORS FOR ALL NEW SIGNS TO BE CONNECTED TO THE EXSITING RUNWAY 4/22 CIRCUIT.
5. CONNECT NEW CIRCUIT TO EXISTING AIRFIELD LIGHTING CIRCUIT INSIDE LIGHT BASE/SPLICE CAN OR AT STAKE MOUNTED LIGHT. COST SHALL BE INCIDENTAL TO INSTALLATION OF NEW AIRFIELD LIGHTING CIRCUIT.
6. CONTRACTOR SHALL LOCATE EXISTING CIRCUITS AND ROUTE THROUGH NEW HANDHOLE/SPLICE CAN. SPLICE NEW CIRCUIT INSIDE HANDHOLE/SPLICE CAN. COST SHALL BE INCIDENTAL TO NEW HANDHOLE/SPLICE CAN.

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

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PROPOSED ELECTRICAL CIRCUITRY PLAN 1

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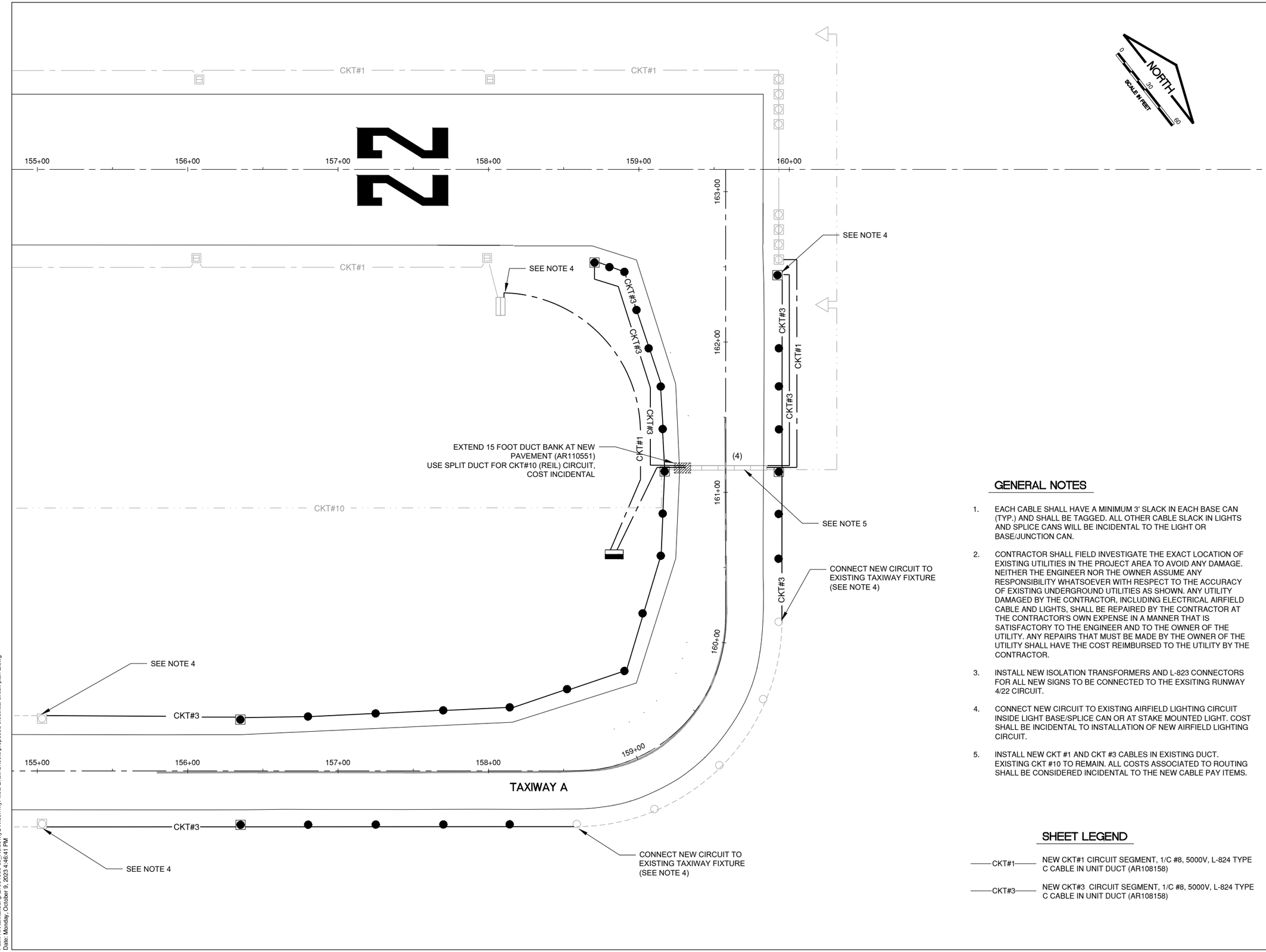

GENERAL NOTES

1. EACH CABLE SHALL HAVE A MINIMUM 3' SLACK IN EACH BASE CAN (TYP.) AND SHALL BE TAGGED. ALL OTHER CABLE SLACK IN LIGHTS AND SPLICE CANS WILL BE INCIDENTAL TO THE LIGHT OR BASE/JUNCTION CAN.
2. CONTRACTOR SHALL FIELD INVESTIGATE THE EXACT LOCATION OF EXISTING UTILITIES IN THE PROJECT AREA TO AVOID ANY DAMAGE. NEITHER THE ENGINEER NOR THE OWNER ASSUME ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY OF EXISTING UNDERGROUND UTILITIES AS SHOWN. ANY UTILITY DAMAGED BY THE CONTRACTOR, INCLUDING ELECTRICAL AIRFIELD CABLE AND LIGHTS, SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE IN A MANNER THAT IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR.
3. INSTALL NEW ISOLATION TRANSFORMERS AND L-823 CONNECTORS FOR ALL NEW SIGNS TO BE CONNECTED TO THE EXSITING RUNWAY 4/22 CIRCUIT.
4. CONNECT NEW CIRCUIT TO EXISTING AIRFIELD LIGHTING CIRCUIT INSIDE LIGHT BASE/SPLICE CAN OR AT STAKE MOUNTED LIGHT. COST SHALL BE INCIDENTAL TO INSTALLATION OF NEW AIRFIELD LIGHTING CIRCUIT.
5. INSTALL NEW CKT #1 AND CKT #3 CABLES IN EXISTING DUCT. EXISTING CKT #10 TO REMAIN. ALL COSTS ASSOCIATED TO ROUTING SHALL BE CONSIDERED INCIDENTAL TO THE NEW CABLE PAY ITEMS.

SHEET LEGEND

CKT#1	NEW CKT#1 CIRCUIT SEGMENT, 1/C #8, 5000V, L-824 TYPE C CABLE IN UNIT DUCT (AR108158)
CKT#3	NEW CKT#3 CIRCUIT SEGMENT, 1/C #8, 5000V, L-824 TYPE C CABLE IN UNIT DUCT (AR108158)

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CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

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CHECKED BY:	STL
APPROVED BY:	DKP
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SHEET TITLE

PROPOSED ELECTRICAL CIRCUITRY PLAN 2

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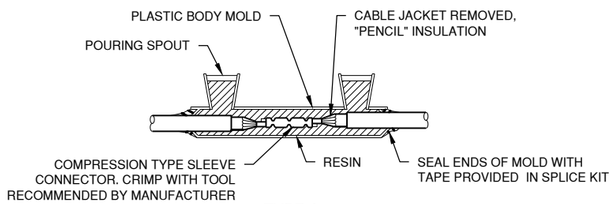
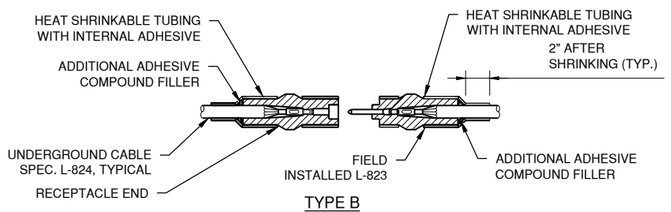


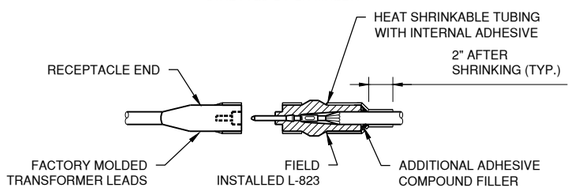
TYPE A

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY



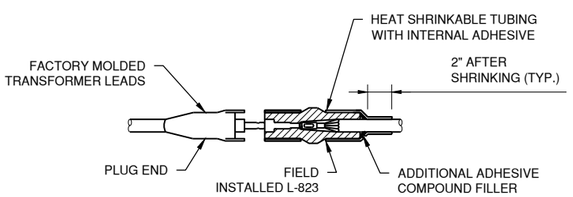
TYPE B

NOT TO BE USED UNLESS DIRECTED BY ENGINEER



TYPE C

FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS

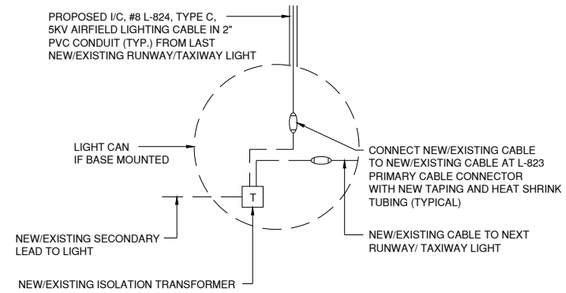


TYPE D

FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS

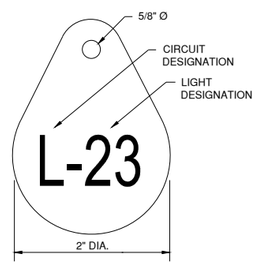
SPLICE NOTES

1. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
2. WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
3. THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
4. THE CONTRACTOR SHALL HAVE A MINIMUM OF FIVE (5) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
5. THE CONTRACTOR MAY ELECT TO USE AN FAA APPROVED "COMPLETE" KIT IN LIEU OF HEAT SHRINK KIT AT NO ADDITIONAL COST.



TAXIWAY/RUNWAY LIGHTING CIRCUIT CONNECTION DETAIL

NOT TO SCALE

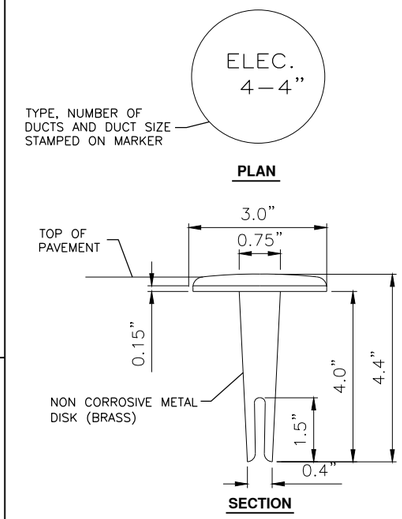


NOTES

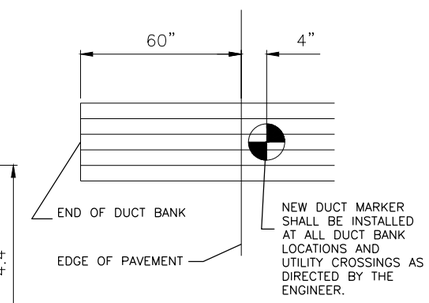
1. INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
2. NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS AND SIGNS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
3. COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. COORDINATE WITH RESIDENT ENGINEER FOR TAXIWAY LIGHTING TAG NUMBERS.

LIGHT IDENTIFICATION DETAIL

NOT TO SCALE



NEW DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT AT ALL NEW DIRECTIONAL BORE, SPLIT DUCT, AND CONCRETE ENCASED DUCT LOCATIONS. COST INCIDENTAL TO THE CONTRACT.

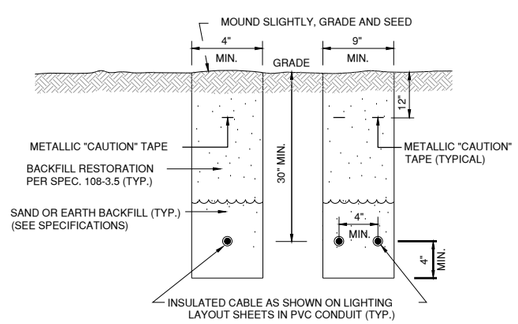


DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.

MARKER PLACEMENT

DUCT MARKER DETAILS

N.T.S.

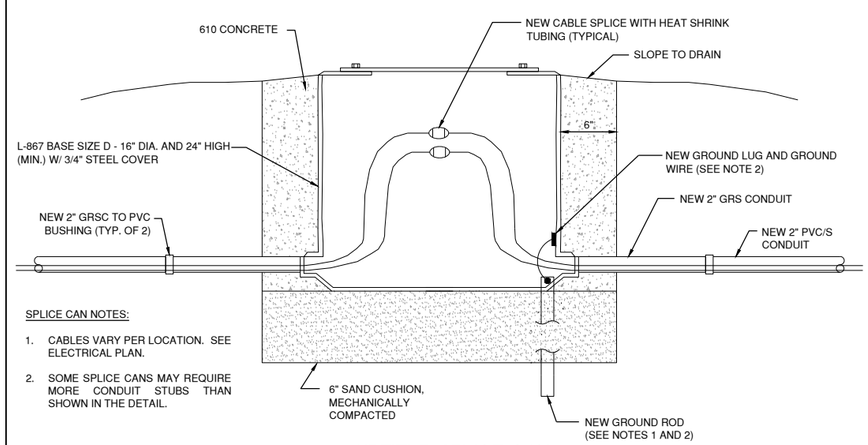


TRENCH DETAIL

NOT TO SCALE

TRENCH NOTES

1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
2. DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.



SPLICE CAN NOTES:

1. CABLES VARY PER LOCATION. SEE ELECTRICAL PLAN.
2. SOME SPLICE CANS MAY REQUIRE MORE CONDUIT STUBS THAN SHOWN IN THE DETAIL.

AIRFIELD SPLICE CAN

NOT TO SCALE

NOTES:

1. INSTALL SAFETY GROUND TO EXISTING SIGNS AND L-867 CANS. ATTACH GROUND LUG TO EXISTING CAN AND INSTALL GROUND ROD AS SHOWN ON PLANS.
2. INSTALL 1/C #6 AWG BARE COPPER GROUND JUMPER CONNECTED TO GROUND LUG INSIDE BASE CAN AND EXOTHERMICALLY WELDED TO GROUND ROD. INSTALL GROUND LUG FOR EXISTING CANS, IF REQUIRED.

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SHEET TITLE

ELECTRICAL DETAILS - 1

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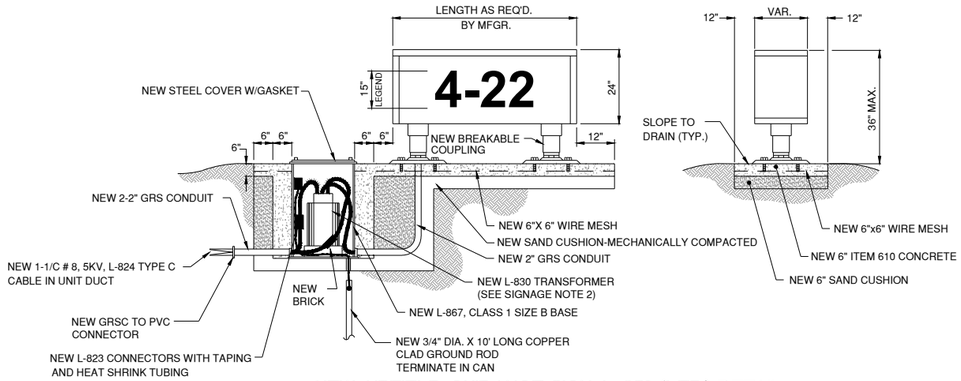


NEW AIRFIELD GUIDANCE SIGN, L-858 (LED) DETAIL

NOT TO SCALE

SIGNAGE NOTES

1. ALL SIGNS ARE 2-SIDED LED SIGNS SIZE 2, CLASS 2 AS MANUFACTURED BY ADB OR APPROVED EQUAL. SIGNS SHALL BE CAPABLE OF BEING CONNECTED TO A 3-STEP OR 5-STEP REGULATOR.
2. TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR LED SIGNS.
3. LIGHTED SIGNS SHALL BE BASE MOUNTED ONLY.
4. THE NUMBER OF MODULES PER SIGN SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
5. CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWINGS INCLUDING SIGN, COLOR, SIZE AND PROPOSED LEGEND, IN ENOUGH DETAIL TO DETERMINE PROPOSED SPACING AND OTHER INFORMATION REQUIRED BY THE SPECIAL PROVISIONS.
6. CONTRACTOR TO VERIFY PROPOSED SIGN LOCATIONS AND ORIENTATIONS WITH RESIDENT ENGINEER PRIOR TO INSTALLATION.

AIRFIELD SIGNAGE SCHEDULE

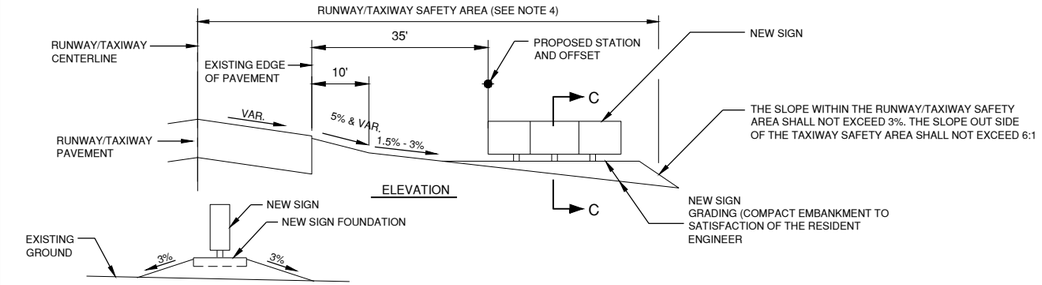
SIGN NUMBER	NEW SIGN FACE	NEW SIGN LEGEND	NEW SIGN TYPE	NEW SIGN LOCATION	NOTES	STATION AND OFFSET
S1	N S	A4 ◄∀► / A4 4-22	2,3 2,1	TAXIWAY A4	NEW 6 CHARACTER SIGN	TAXIWAY A4 -STA. 2+57.00 OFF. 60.00 RT
S2	E W	►A4 / BLANK	3 0	RUNWAY 4/22	NEW 3 CHARACTER SIGN	RUNWAY 4-22 -STA. 115+07.98 OFF. 86.38 RT
S3	E W	BLANK / ►∀	0 3	RUNWAY 4/22	NEW 3 CHARACTER SIGN	RUNWAY 4-22 -STA. 113+22.54 OFF. 86.41 RT
S4	N S	BLANK A / ∀ 22	2,0 2,1	TAXIWAY A	NEW 3 CHARACTER SIGN	TAXIWAY A -STA. 160+57.76 OFF. 68.10 LT

NEW SIGN TYPE LEGEND

0 —— BLANK PANEL - BLACK
1 —— RUNWAY/TAXIWAY HOLDLINE - WHITE LEGEND ON RED BACKGROUND
2 —— LOCATION SIGN - YELLOW LEGEND ON BLACK BACKGROUND
3 —— DIRECTION/INFORMATION SIGN - BLACK LEGEND ON YELLOW BACKGROUND

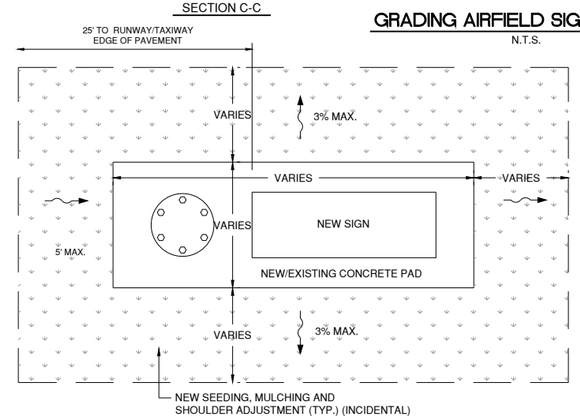
NOTE

1. GUIDANCE SIGN PANEL SIZE WILL BE BASED ON THE MANUFACTURER'S RECOMMENDATION.



GRADING AIRFIELD SIGN DETAILS

N.T.S.

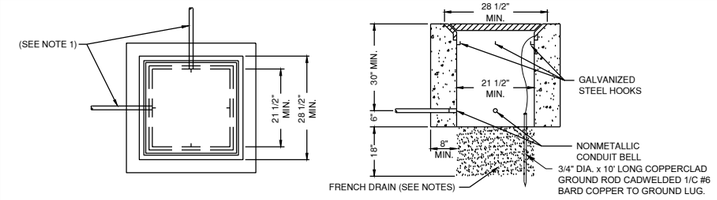


TYPICAL LANDSCAPING FOR NEW SIGN

NOT TO SCALE

NOTES:

1. SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES/SLOPES IN THE FIELD
2. IT IS ESTIMATED THAT 2 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT THE SIGN BASE FOUNDATIONS. ALL MATERIAL USED TO CONSTRUCT THE EMBANKMENT SHALL BE APPROVED BY THE RESIDENT ENGINEER. ALL OFFSITE MATERIAL USED TO CONSTRUCT THE EMBANKMENT SHALL BE CATEGORIZED AS CLEAN CONSTRUCTION OR DEMOLITION DEBRIS IN ACCORDANCE WITH SECTION 905-1.2 OF THE SPECIAL PROVISIONS. COSTS TO CONSTRUCT THE EMBANKMENT SHALL BE INCIDENTAL TO THE SIGN PAY ITEM. ESTIMATED QUANTITY OF EMBANKMENT IS FOR INFORMATION ONLY AND NO ADDITIONAL COMPENSATION SHALL BE MADE FOR ANY ADDITIONAL EMBANKMENT NEEDED.
3. ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
4. DIMENSIONS:
   RUNWAY 4/22 = 250'
   RUNWAY 16/34 = 75'
   ALL TAXIWAYS = 66.5'

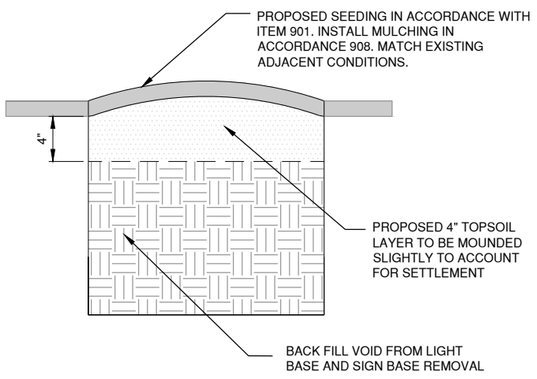


CONCRETE ELECTRICAL HANDHOLE DETAIL- IDOT STANDARD NO. 814001

NOT TO SCALE

NOTES:

1. CONDUIT ENTRANCES AND NUMBER OF CONDUITS SHALL BE AS SHOWN ON SITE PLAN VIEW.
2. COVER SHALL BE STAMPED "ELECTRICAL".
3. FRENCH DRAIN AGGREGATE SHALL BE CA-18 (COST INCIDENTAL TO HANDHOLE).
4. COVER SHALL BE HINGED WITH A SAFETY BAR.



COMPLETED STAKE MOUNTED LIGHT, BASE MOUNTED LIGHT AND SIGN REMOVAL

N.T.S.

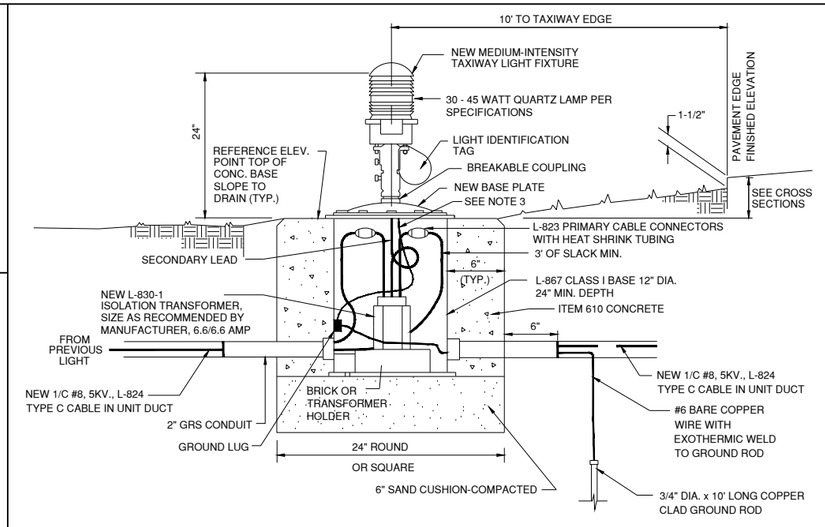
NOTE: COST OF BACKFILL AND RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT.

LIGHTING NOTES

1. TRANSFORMER WATTAGE AS RECOMMENDED BY MANUFACTURER FOR EACH LAMP TYPE.
2. NON-LED FIXTURES SHALL UTILIZE QUARTZ LAMPS.
3. THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
4. LED LIGHT FIXTURES SHALL AS INDICATED ON THE PLANS AND SPECIFICATIONS.
5. AFTER INSTALLATION OF CABLE IN UNIT DUCT, SEAL END OF CONDUIT TO MAKE WATERTIGHT.
6. PRECAST BASE MAY BE USED.

GENERAL NOTES

1. TRANSFORMER HOLDER MAY BE ANY COMMERCIALLY AVAILABLE BRICK.
2. BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
3. ISOLATION TRANSFORMERS SHALL HAVE A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
4. TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
5. AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE CONNECTOR L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL MAY BE USED.
6. LAMP FOR FIXTURES SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS. ISOLATION TRANSFORMERS SHALL BE SIZED PER THE FIXTURE MANUFACTURER, 6.6 AMP.
7. THE CONCRETE BASE FOR BASE MOUNTED LIGHTS AND SIGNS SHALL BE TROWEL FINISHED WITH A 45° BEVELED EDGE AND SLOPED TO DRAIN.



NEW BASE MOUNTED MEDIUM INTESITY TAXIWAY LIGHT

NOT TO SCALE

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
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ELECTRICAL DETAILS - 2

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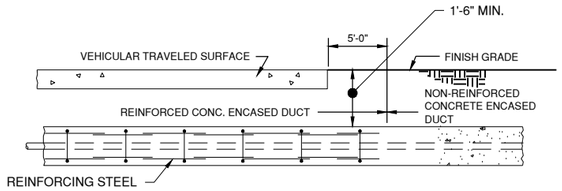
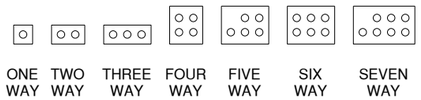
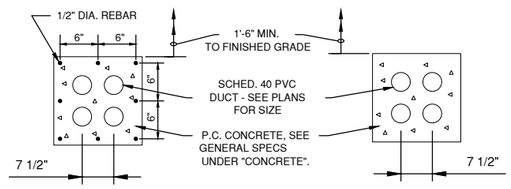


REINFORCED DUCT INSTALLATION



TYPICAL DUCT BANK CONFIGURATIONS

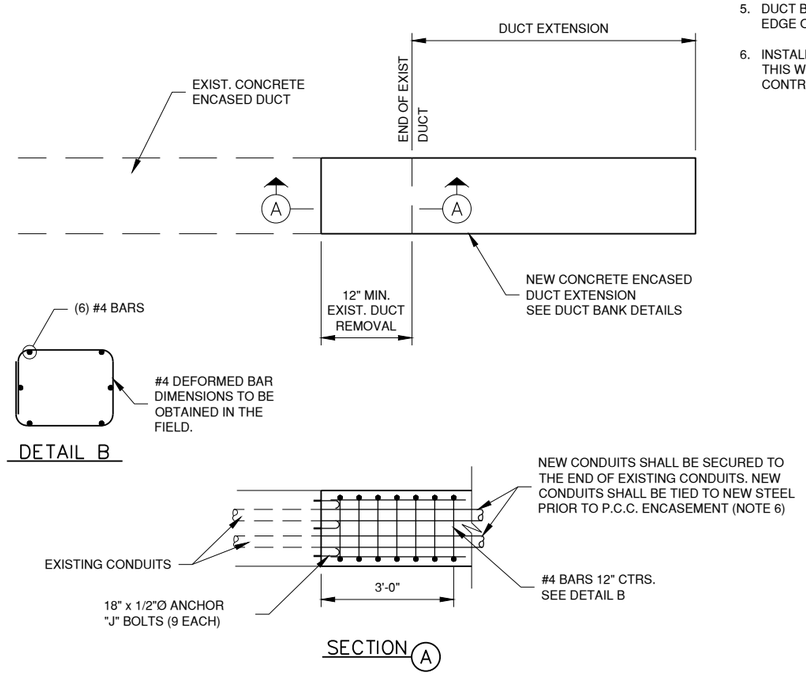


CONCRETE ENCASED DUCT DETAILS (4-WAY SHOWN)

N.T.S.

NOTES

1.) REINFORCED STEEL TO BE MIN. #4 REBAR, MIN. 18" LAP.

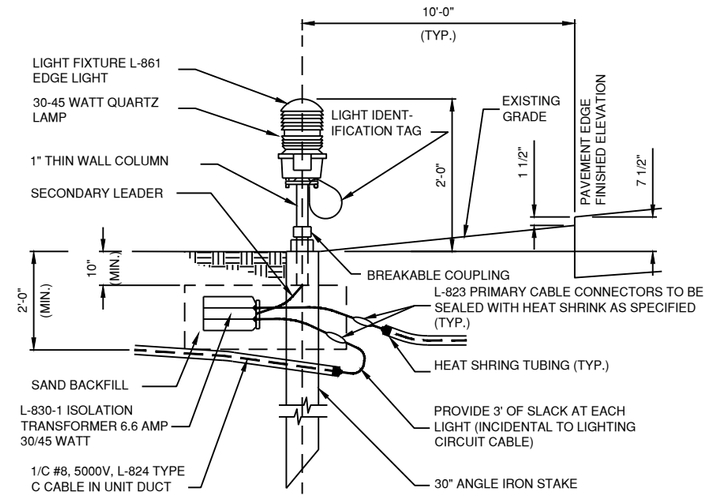


DUCT EXTENSION DETAIL

N.T.S.

DUCT BANK DETAIL NOTES

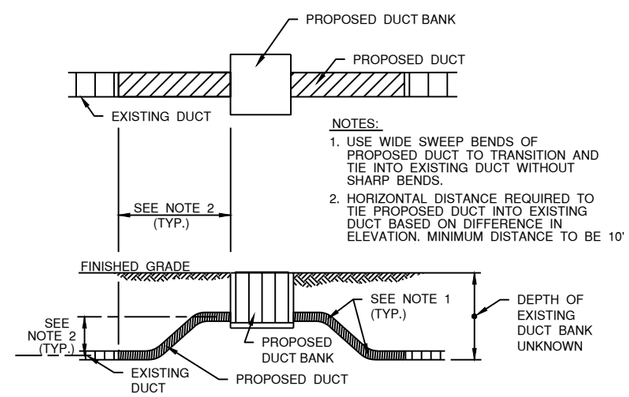
1. DIMENSIONS SHOWN ARE MINIMUM.
2. TOP OF CONCRETE ENCASEMENT TO BE NOT LESS THAN 18" BELOW FINISHED SUBGRADE.
3. DUCT CONCRETE SHALL BE ITEM 610 STRUCTURAL P.C.C. CONCRETE.
4. LOCATIONS SHOWN ARE APPROXIMATE. DUCT BANKS SHALL BE INSTALLED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.
5. DUCT BANKS SHALL EXTEND A MINIMUM OF 5' BEYOND THE NEW EDGE OF PAVEMENT.
6. INSTALL SPLIT DUCT AROUND ALL EXISTING CABLES TO REMAIN. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



UNIT DUCT SHALL BE TERMINATED AT THE L-823 CONNECTORS AND SEALED WITH HEAT SHRINK AS SPECIFIED.

STAKE MOUNTED MEDIUM INTENSITY MARKER LIGHTS

N.T.S.



NOTES:

1. USE WIDE SWEEP BENDS OF PROPOSED DUCT TO TRANSITION AND TIE INTO EXISTING DUCT WITHOUT SHARP BENDS.
2. HORIZONTAL DISTANCE REQUIRED TO TIE PROPOSED DUCT INTO EXISTING DUCT BASED ON DIFFERENCE IN ELEVATION. MINIMUM DISTANCE TO BE 10'

ELECTRICAL DUCT TRANSITION DETAIL PLAN AND SECTION VIEW

NOT TO SCALE

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

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ELECTRICAL DETAILS - 3

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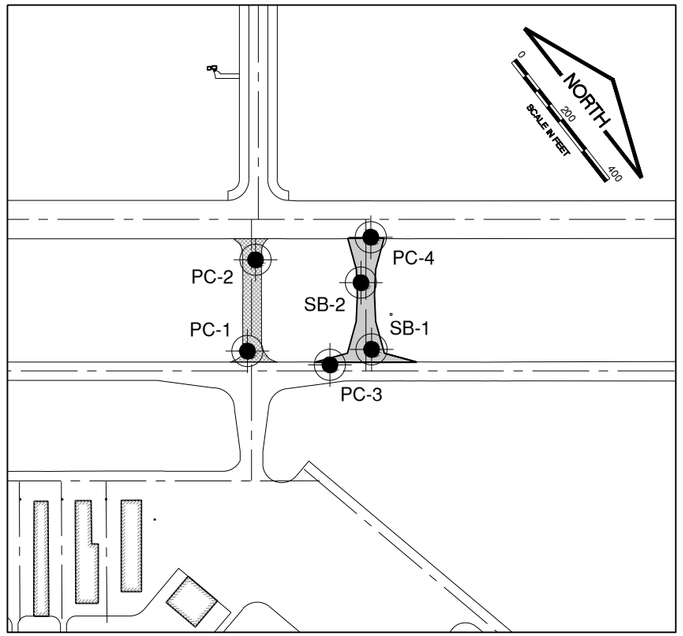

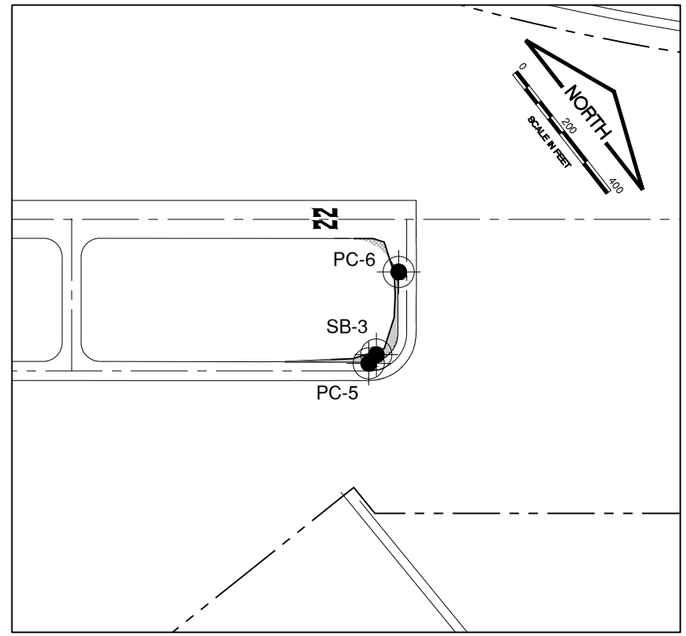

LEGEND

- PROPOSED IMPROVEMENTS
- PROPOSED REMOVALS
- EXISTING HANGAR BUILDING
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- BORING LOCATION (SB-1)
- CORE LOCATION (PC-1)

THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.

GGeo Services, Inc.
Geotechnical, Environmental and Civil Engineering
805 Aspen Drive, Suite 204
Naperville, IL 60565
(630) 355-4366

SOIL BORING LOG

GEO Job No. 23047
Page of 1
Date 6/15/23

PROJECT Kankakee Valley Airport Authority
LOCATION Greater Kankakee Airport, 813 E 4000 S Rd, Kankakee, IL 60901
COUNTY Kankakee DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT
BORING NO. SB-01
Northing 1603465
Easting 1117334
Ground Surface Elev. 619.5 ft

Surface Water Elev. - ft
Stream Bed Elev. - ft
Groundwater Elev.:
First Encounter 613.5 ft
Upon Completion - ft
After - Hrs. - ft

Description	Depth (ft)	Blows (6")	UCS Qu (tsf)	Moist (%)	RQD (pcf)
SANDY TOPSOIL with Gravel-black	618.5			7	
SAND with CLAY & Gravel-black and brown-medium dense(SC)		12			
		13		5	
		6			
POORLY GRADED SAND-brown-loose to medium dense(SP)	616.5				
		4			
		4		11	
		4			
		2			
		5		20	
		6			
WELL GRADED SAND with Gravel-gray-medium dense(SW)	611.5				
		5			
		7		14	
	609.5	7			
End of boring at 10' depth. Backfilled with soil cuttings and bentonite chips. Patched upon completion.	-10				
	-15				
	-20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

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SOIL BORING LOG

GEO Job No. 23047
Page of 1
Date 6/15/23

PROJECT Kankakee Valley Airport Authority
LOCATION Greater Kankakee Airport, 813 E 4000 S Rd, Kankakee, IL 60901
COUNTY Kankakee DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT
BORING NO. SB-02
Northing 1603551
Easting 1117178
Ground Surface Elev. 619.3 ft

Surface Water Elev. - ft
Stream Bed Elev. - ft
Groundwater Elev.:
First Encounter 613.8 ft
Upon Completion - ft
After - Hrs. - ft

Description	Depth (ft)	Blows (6")	UCS Qu (tsf)	Moist (%)	RQD (pcf)
SANDY TOPSOIL	618.3			5	
SILTY CLAYEY SAND-gray-medium dense to loose(SC-SM)		9			
		9		5	
		6			
		4			
		4		8	
	614.3	4			
WELL GRADED SAND with Gravel-gray-medium dense(SW)					
		3			
		6		20	
		5			
WELL GRADED SAND-gray-medium dense(SW)	611.3				
		4			
		6		18	
	609.3	7			
End of boring at 10' depth. Backfilled with soil cuttings and bentonite chips. Patched upon completion.	-10				
	-15				
	-20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

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SOIL BORING LOG

GEO Job No. 23047
Page of 1
Date 6/15/23

PROJECT Kankakee Valley Airport Authority
LOCATION Greater Kankakee Airport, 813 E 4000 S Rd, Kankakee, IL 60901
COUNTY Kankakee DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

CLIENT CMT
BORING NO. SB-03
Northing 1606960
Easting 1120084
Ground Surface Elev. 623.6 ft

Surface Water Elev. - ft
Stream Bed Elev. - ft
Groundwater Elev.:
First Encounter Dry ft
Upon Completion - ft
After - Hrs. - ft

Description	Depth (ft)	Blows (6")	UCS Qu (tsf)	Moist (%)	RQD (pcf)
SILTY TOPSOIL	622.6			7	
SILTY CLAY-gray-hard(CL-ML)		15			
		16	4.00	10	
		14	B		
SILTY CLAY with Gravel-gray-very stiff(CL - ML)	620.6				
		6			
		6	3.00	10	
		8	B		
		8			
		9	3.00	7	
		9	B		
SANDY CLAY-brown and black/gray-loose(SC)	615.6				
		4			
		3	2.00	15	
	613.6	4	B		
End of boring at 10' depth. Backfilled with soil cuttings and bentonite chips. Patched upon completion.	-10				
	-15				
	-20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

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PAVEMENT CORE SUMMARY

Page: 1 of 4

Project: Widen Runway 4/22 End Taxiway Fillets & Taxiway A4 Relocation
Location: Greater Kankakee Airport, 813 E 4000 S Rd, Kankakee, IL 60901
County: Kankakee
Client: CMT

GSI Job No.: 23047
Date: 07/24/2023
Cored By: RICK
Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
PC-1	11	ASPHALT-well consolidated, fine to medium coarse aggregate.
PC-2	10.5	ASPHALT-well consolidated, fine to medium coarse aggregate.
PC-3	10.5	ASPHALT-well consolidated, fine to medium coarse aggregate.
PC-4	12.25	ASPHALT-well consolidated, fine to medium coarse aggregate.
PC-5	10	ASPHALT-well consolidated, fine to medium coarse aggregate.
PC-6	12	ASPHALT-well consolidated, fine to medium coarse aggregate.

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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
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GEOTECHNICAL ENGINEERING INFORMATION

Z:\PROJECTS\2021\21001660 CMT KANKAKEE AIRPORT TWY A4, RW 4/RP\CADD\BORING LOGS\SHEET LOGS.DWG, LOGS.DWG, 7/19/23

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Date: Monday, October 9, 2023 4:47:02 PM

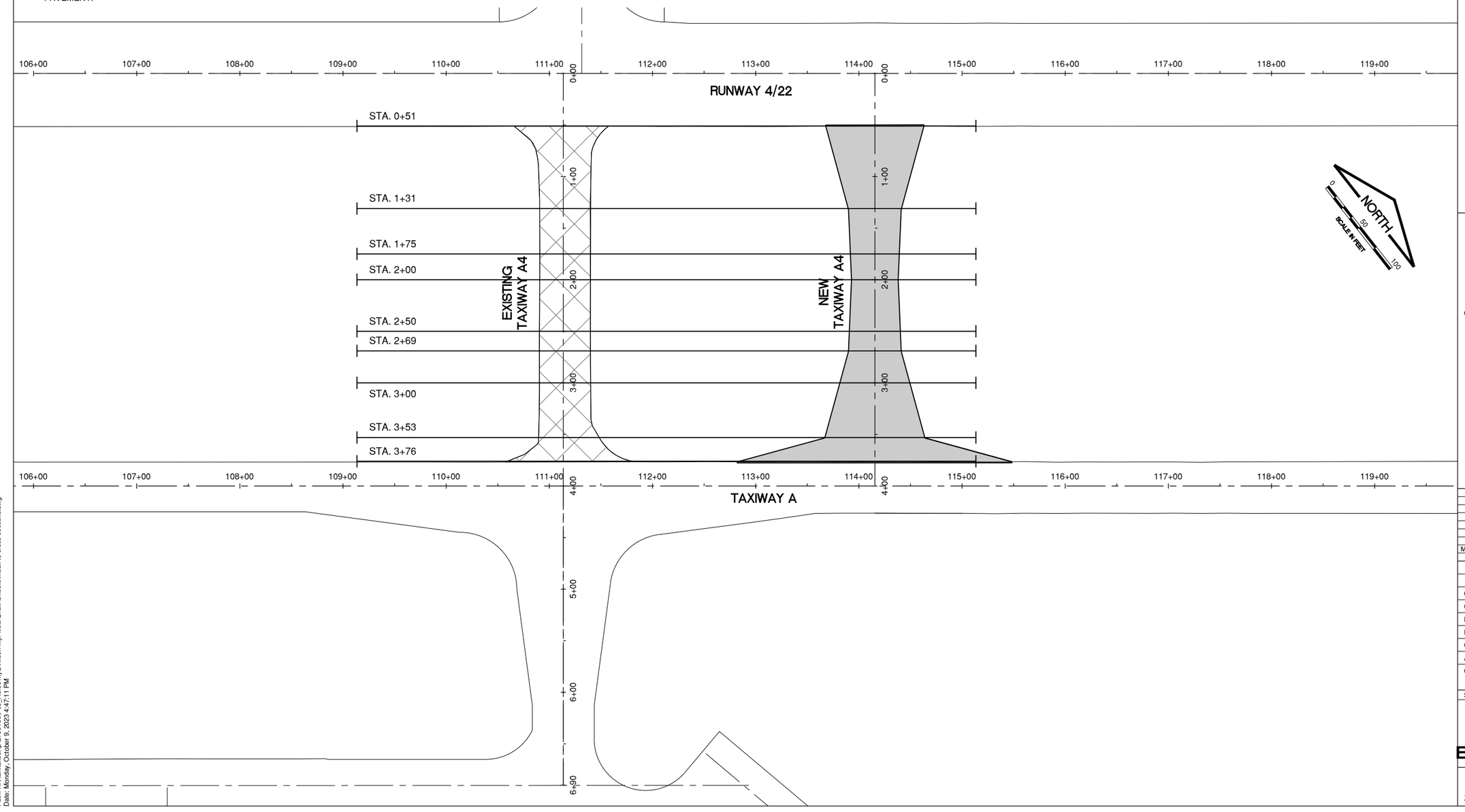
GENERAL EARTHWORK NOTES:

1. ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
2. AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL, UNLESS AUTHORIZED BY THE ENGINEER.
3. PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AREAS. UNCLASSIFIED EXCAVATION AREA ALSO INCLUDES PGE SUBGRADE STABILIZATION AREA AS DIRECTED BY THE RESIDENT ENGINEER.
4. THE CONTRACTOR SHALL ENSURE THAT 4 INCHES OF TOPSOIL CAN BE SPREAD OVER THE LIMITS OF THE GRADED AREA. IN SOME CASES, CONTRACTOR MAY BE REQUIRED TO OVER-EXCAVATE TO PROVIDE THE REQUIRED 4-INCH TOPSOIL LAYER. THE EARTHWORK QUANTITIES SHOWN INCLUDE THE REQUIRED OVER-EXCAVATION AND ARE SHOWN IN THE CROSS SECTIONS.
5. TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL.
6. ANY EXCESS MATERIAL INCLUDING CLAY, UNSUITABLE MATERIAL AND TOPSOIL SHALL BE HAULED OFF AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR. CCDD TESTING REQUIREMENTS SHALL PER THE SPECIAL PROVISIONS SECTION 152.
7. PER THE GEOTECHNICAL INVESTIGATION REPORT, AVERAGE 12 INCHES OF TOPSOIL IS PRESENT AND USED IN QUANTITY ESTIMATION. TOPSOIL STRIPPED SHALL BE INCORPORATED INTO THE REQUIRED TOPSOIL PLACEMENT ON DISTURBED AREAS OUTSIDE OF NEW PAVEMENT.

EARTHWORK SUMMARY TABLE

LOCATION	TOPSOIL STRIPPING INITIAL POSITION (CUBIC YARD)	TOPSOIL PLACEMENT FINAL POSITION (CUBIC YARD)	SHOULDER FILL FINAL POSITION (CUBIC YARD)	UNCLASSIFIED EXCAVATION INITIAL POSITION (CUBIC YARD)	EMBANKMENT FILL FINAL POSITION (CUBIC YARD)
TAXIWAY A4 RELOCATION	3,140	2,340	1,390	3,700	110
TAXIWAY WIDENING AT RUNWAY 22 END	200	240	50	560	0
TOTAL	3,340	2,580	1,440	4,260	110

NOTE: UNCLASSIFIED EXCAVATION QUANTITY INCLUDES 15% NOMINAL NEW PAVEMENT AREA FOR PGE STABILIZATION.



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WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

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INDEX TO CROSS SECTIONS AND EARTHWORK SUMMARY

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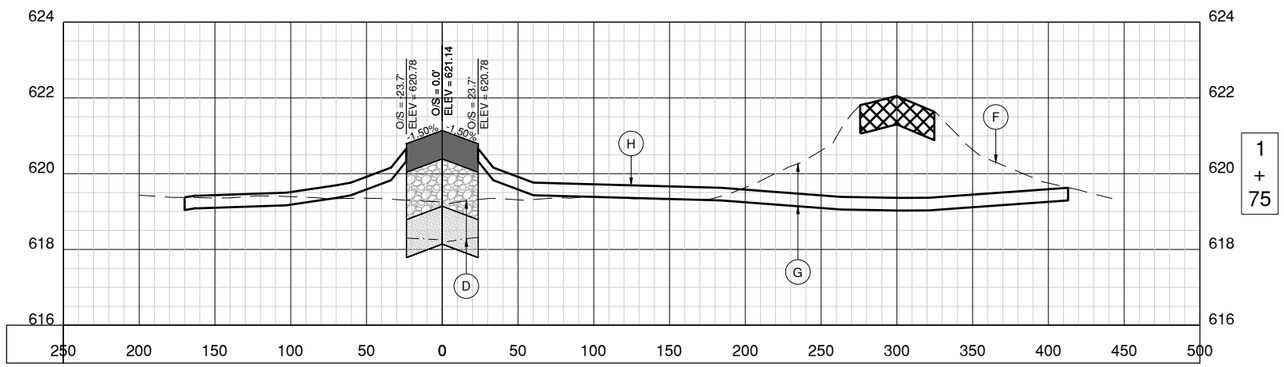

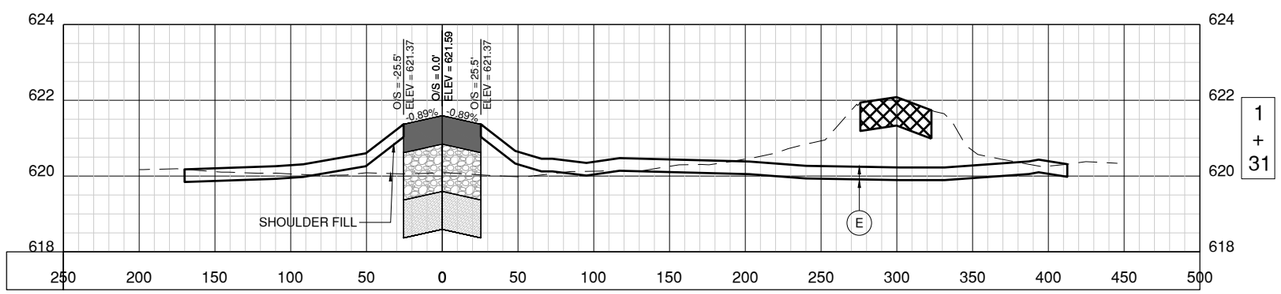

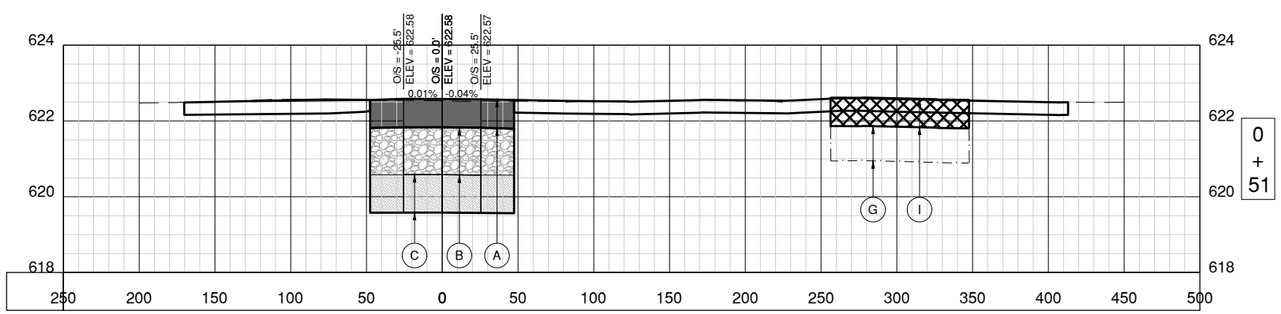

LEGEND

- (A) NEW 9" HMA PAVEMENT (401, 403)
- (B) NEW 15" CRUSHED AGGREGATE BASE COURSE (209)
- (C) NEW 12" POROUS GRANULAR EMABNKMENT (208)
- (D) AVERAGE 12" TOPSOIL STRIPPING (152)
- (E) MINIMUM 4" TOPSOIL PLACEMENT (905)
- (F) EXISTING GROUND LINE
- (G) UNCLASSIFIED EXCAVATION (152)
- (H) NEW GROUND LINE
- (I) EXISTING 9" BITUMINOUS (401) TO BE REMOVED

UE UNCLASSIFIED EXCAVATION
TS TOPSOIL STRIPPING
EF EMBANKMENT FILL
TP TOPSOIL/SHOULDER PLACEMENT

NOTES

1. SEE GRADING PLAN FOR ELEVATIONS.
2. EXISTING AND NEW UTILITIES ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.

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SEPTEMBER 22, 2023

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CROSS SECTIONS - 1

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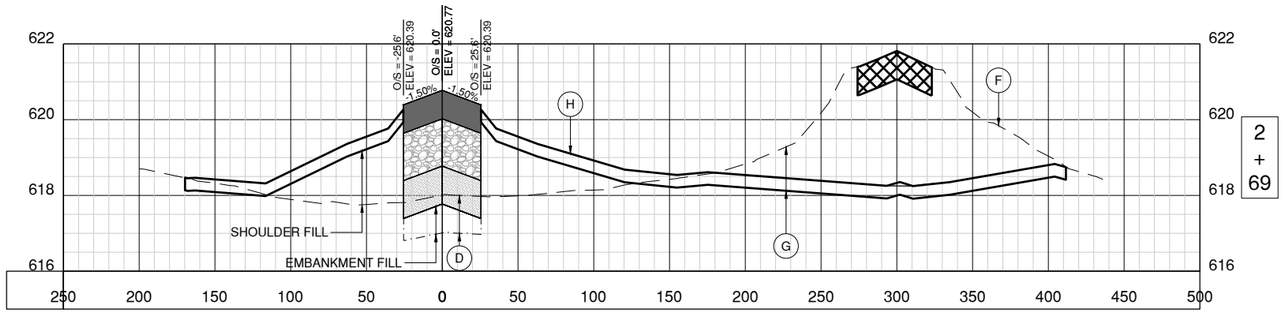

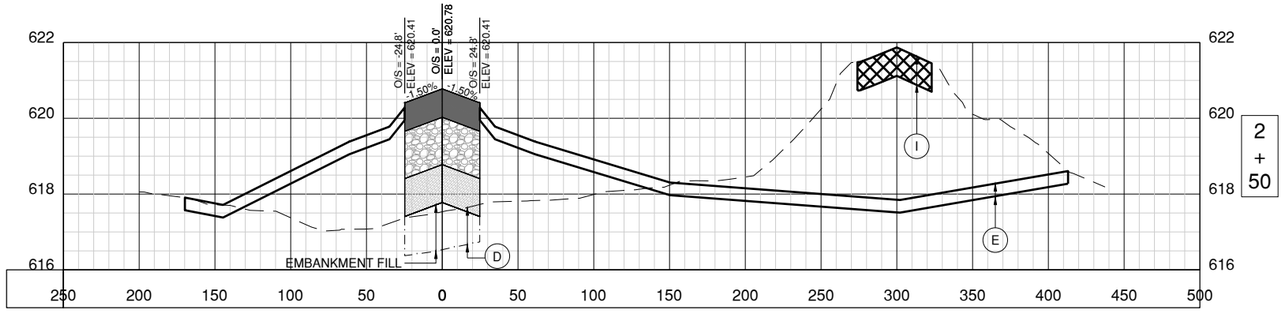

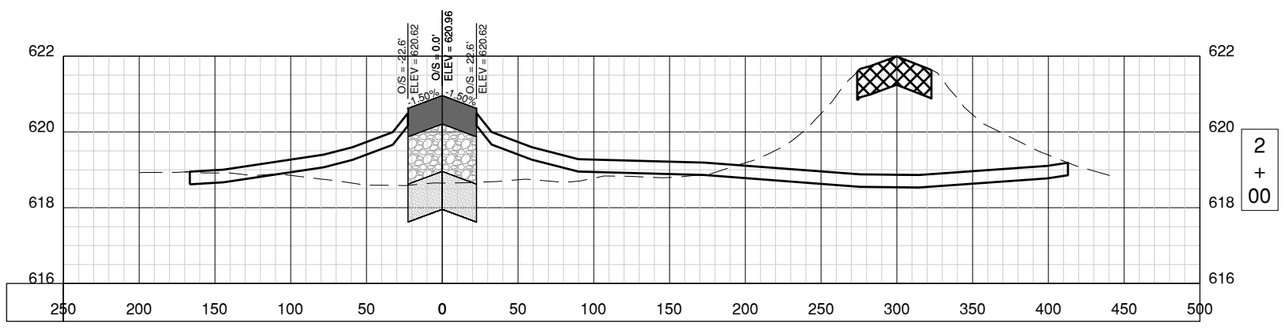

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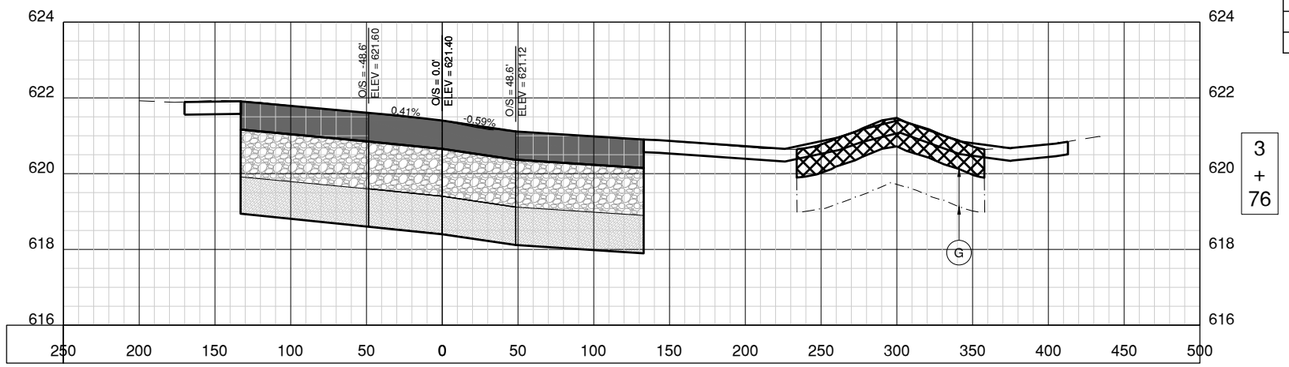
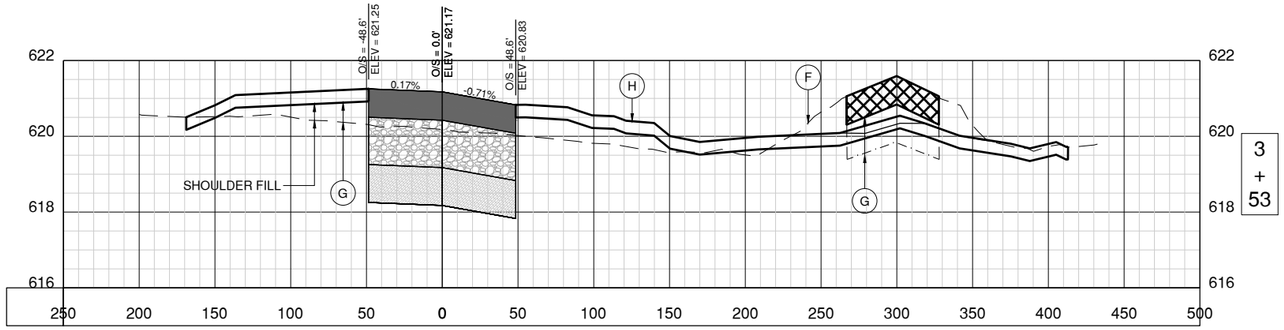
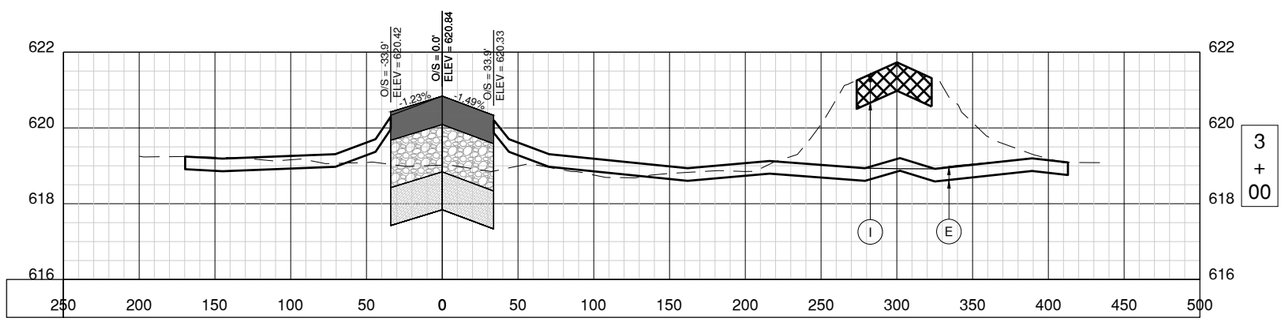
Date: Monday, October 9, 2023 4:47:24 PM

STATION	UNCLASSIFIED VOLUME (C.Y.)	EMBANKMENT FILL VOLUME (C.Y.)	SHOULDER FILL VOLUME (C.Y.)	TOP SOIL STRIPPING VOLUME (C.Y.)	TOP SOIL PLACEMENT VOLUME (C.Y.)
0+51					
1+31	585	0	246	622	508
1+75	245	0	59	330	291
2+00	186	3	64	194	165
2+50	516	56	334	412	329
2+69	214	30	153	166	125
3+00	263	19	117	259	200
3+53	349	0	130	383	328
3+76	1202	0	286	773	391
TOTAL	**3560**	**108**	**1388**	**3139**	**2337**







KA052

CMT

CONSULTANTS

FINAL

WIDEN TAXIWAY FILLET AT RUNWAY 22 END AND RELOCATE TAXIWAY A4

SEPTEMBER 22, 2023

OWNER

KANKAKEE VALLEY KVAA AIRPORT AUTHORITY

MARK	DATE	DESCRIPTION

CMT PROJECT NO:	21001660.00
CAD DWG FILE:	
DESIGNED BY:	STL
DRAWN BY:	JRO
CHECKED BY:	STL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

CROSS SECTIONS - 3

Path: K:\KankakeeAp\21001660-00_RelocTxyDWidenTxyFillets\Draw\Sheets\cross sections - 3.dwg
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