

Thickness of shims required under Pier shoes

	P	O	N	M	L
Pier C2	1/8"	9/16"	1/8"	7/8"	-
Pier C3	-	1/16"	1/16"	1/16"	-

BAR SCHEDULE FOR ONE ABUTMENT-2 REQD

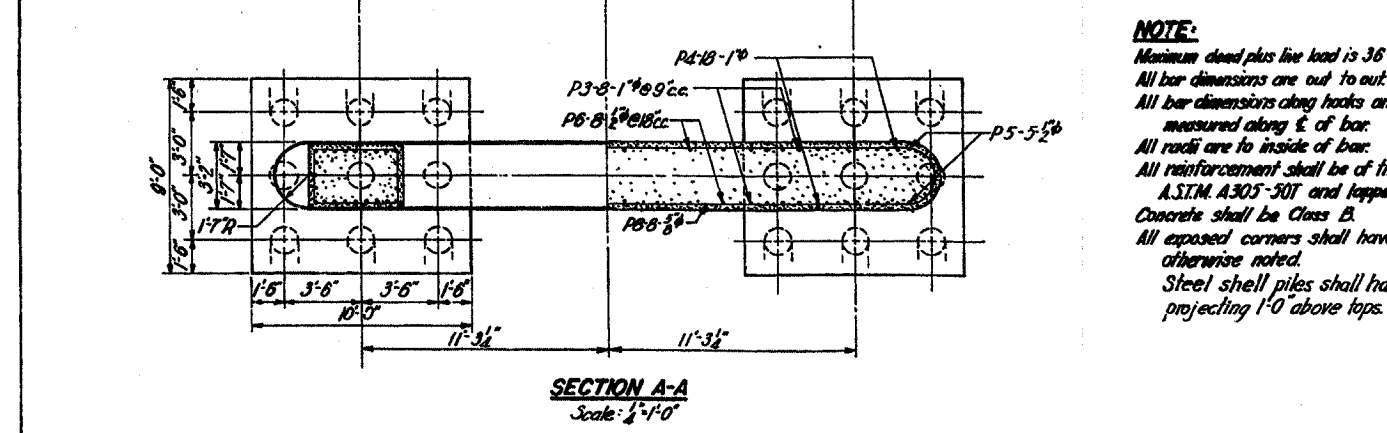
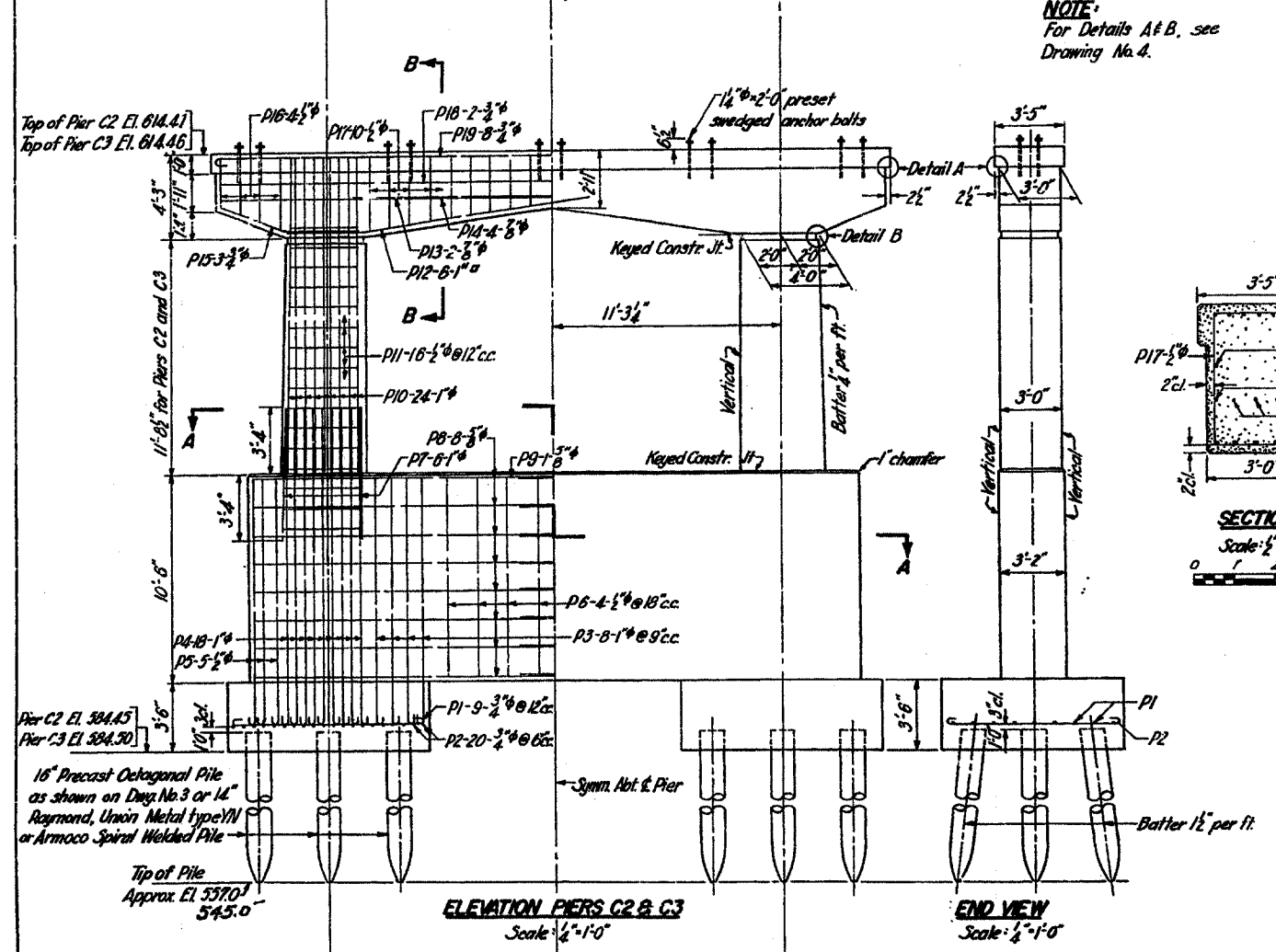
Mark	No	Stock	Bend
A1	41	1/2" φ × 6'-6"	3'-8"
A2	12	1/2" φ × 21'-8"	Straight
A3	39	1/2" φ × 3'-10"	3'-4"
A4	33	1/2" φ × 8'-8"	5'-7"
A5	27	3/8" φ × 3'-7"	Straight
A6	27	3/8" φ × 5'-7"	3'-0"
A7	22	1/2" φ × 10'-2"	3'-2"
A8	6	3/4" φ × 21'-8"	Straight
A9	4	1/2" φ × 8'-4"	7'-0"
A10	6	1/2" φ × 8'-10"	7'-9"
A11	30	1/2" φ × 21'-7"	Straight
A12	4	1/2" φ × 6'-0"	3'-8"

BAR SCHEDULE FOR WINGWALLS OF ONE ABUTMENT-2 REQD

Mark	No	Stock	Bend
W1	24	1/2" φ × 2'-6"	Straight
W2	56	1/2" φ × 7'-6"	Straight
W3	6	1/2" φ × 15'-7"	6'-8"
W4	6	1/2" φ × 13'-8"	5'-8"
W5	6	1/2" φ × 13'-5"	4'-6"
W6	6	1/2" φ × 13'-4"	5'-2"
W7	1	3/4" φ × 12'-11"	4'-10"
W7a	1	3/4" φ × 13'-8"	4'-10"
W8	1	3/4" φ × 12'-10"	4'-10"
W8a	1	3/4" φ × 13'-3"	4'-10"
W9	1	1/2" φ × 9'-7"	4'-10"
W9a	1	1/2" φ × 10'-4"	4'-10"
W10	12	1/2" φ × 10'-7 1/2"	4'-8"
W11	2	1/2" φ × 10'-5 1/2"	4'-8"
W12	2	1/2" φ × 9'-11 1/2"	4'-5 1/2"
W13	2	1/2" φ × 8'-3 1/2"	4'-1 1/2"
W14	2	1/2" φ × 8'-1 1/2"	3'-6"
W15	2	1/2" φ × 5'-8 1/2"	2'-4"
W16	1	1/2" φ × 8'-3"	4'-10"
W16a	1	1/2" φ × 8'-10"	5'-5 1/2" R
W17	1	1/2" φ × 8'-3"	4'-10"
W17a	1	1/2" φ × 8'-10"	5'-5 1/2" R
W18	1	1/2" φ × 9'-7"	4'-10"
W18a	1	1/2" φ × 10'-4"	5'-5 1/2" R
W19	6	1/2" φ × 3'-0"	Straight

BAR SCHEDULE FOR ONE PIER-2 REQD

Mark	No	Stock	Bend
P1	18	3/4" φ × 11'-0"	9'-6"
P2	40	3/4" φ × 10'-0"	8'-6"
P3	16	1" φ × 13'-6"	12'-6"
P4	36	1" φ × 16'-10"	15'-10"
P5	10	1/2" φ × 13'-0"	12'-6"
P6	8	3/4" φ × 25'-10"	10'-0"
P7	12	1" φ × 6'-8"	Straight
P8	16	3/8" φ × 36'-1 1/2"	16'-4 1/2"
P9	1	3/4" φ × 31'-0 3/4"	Straight
P10	48	1" φ × 15'-9 1/2"	Straight
P11	32	1/2" φ × 13'-5"	2 ea. 3'-5" to 3'-11" by 4"
P12	12	1 1/4" φ × 16'-7"	12'-8"
P13	2	3/8" φ × 33'-0"	Straight
P14	4	3/8" φ × 19'-6"	Straight
P15	6	3/4" φ × 7'-3"	2 ea. 2'-7" to 3'-10 1/2" by 3"
P16	8	1/2" φ × 12'-3" avg.	2 ea. 2'-7" to 3'-10 1/2" by 1 1/2"
P17	18	1/2" φ × 12'-4 1/2" avg.	5"
P18	2	3/4" φ × 33'-0"	Straight
P19	8	3/4" φ × 36'-11"	33'-5"



NOTE:
 For Details A & B, see Drawing No. 4.

NOTE:
 Minimum dead plus live load is 36 tons per pile.
 All bar dimensions are out to out.
 All bar dimensions along hooks and bend are measured along ξ of bar.
 All radii are to inside of bar.
 All reinforcement shall be of the deformed type.
 A.S.T.M. A305-50T and lapped 40 dia. at splices.
 Concrete shall be Class B.
 All exposed corners shall have 3/4" chamfers unless otherwise noted.
 Steel shell piles shall have straight bars projecting 1'-0" above tops.

NOTE:
 For hook detail, see drawing No. 4.

APPROVED:
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 FOR MODJESKI & MASTERS

MISSISSIPPI RIVER BRIDGE
 AT CLINTON, IOWA
 CITY OF CLINTON BRIDGE COMMISSION
 PIERS
 G & NW RR. OVERPASS
 SCALES AS NOTED
 CONTRACT NO. 7 DRAWING NO. 9
 MODJESKI & MASTERS, ENGINEERS
 OCTOBER, 1951