

11-21-14 LETTING ITEM 028

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

TECHNY TRAIL SEGMENT 4
OLD WILLOW RD TO WILLOW RD (FAP 305)
MULTI-USE PATH IMPROVEMENTS
SECTION: 11-00178-00-BT
PROJECT: TE-00D1(846)
VILLAGE OF GLENVIEW
COOK COUNTY
C-91-361-11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	1
FED. ROAD DIST. NO. 1		Job No. C-91-361-11	FED AID PROJECT TE-00D1 (846)	
CONTRACT NO.: 61A68				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

CLAIRE COURT
POSTED SPEED LIMIT = 20 MPH
2014 ADT = 500

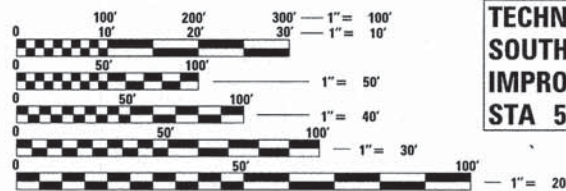
WESTLEIGH DRIVE
POSTED SPEED LIMIT = 20 MPH
2014 ADT = 500

TECHNY TRAIL SEGMENT 4
NORTH LOCATION
IMPROVEMENT BEGINS
STA 59+58

STATION EQUATION
STA 55+14 BK =
STA 35+03 AH

TECHNY TRAIL SEGMENT 4
SOUTH LOCATION
IMPROVEMENT BEGINS
STA 50+29

TECHNY TRAIL
SEGMENT 4
NORTH LOCATION
IMPROVEMENT ENDS
STA 82+67

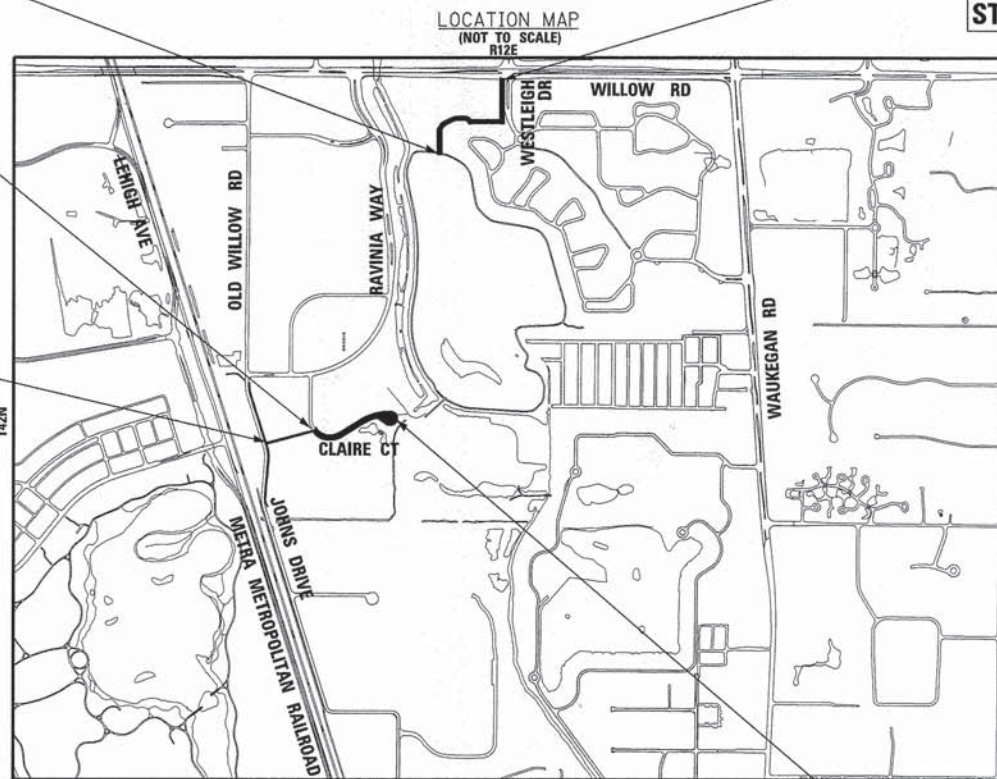


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. A0771242



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:
COUNTY = COOK
CITY-TWNSHP. = GLENVIEW-NORTHFIELD
SEC. & 1/4 SEC. NO. = 22&23
48 HOURS (2 working days) BEFORE YOU DIG



SECTION 22 & 23, T42N, R12E, OF THE THIRD PRINCIPAL MERIDIAN
NORTHFIELD TOWNSHIP
GROSS LENGTH = 2,740 FT = 0.519 MILE
NET LENGTH = 2,740 FT = 0.519 MILE

TECHNY TRAIL
SEGMENT 4
SOUTH LOCATION
IMPROVEMENT ENDS
STA 45+75

Professional Engineer Seal for Todd L. Hoffman, License No. 62-058724, License Expires 11-30-15. Signature of Todd L. Hoffman, Project Manager.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	8-7-2014 <i>Julie</i> VILLAGE OF GLENVIEW
APPROVED	8-7-2014 <i>John T. Brady</i> GLENVIEW PARK DISTRICT
PASSED	August 29, 2014 <i>Chris...</i> DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	August 29, 2014 <i>John F...</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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CONTRACT NO.: 61A68



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 LICENSE NO. 184-0021 - EXPIRES 4/30/2015
 FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., PTDE (847) 705-4021 SCHAUMBURG, IL

COMMITMENTS

THERE ARE NO COMMITMENTS FOR THIS PROJECT.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
202001-01	EARTH MEDIAN DITCH CHECK
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424016-01	MID-BLOCK CURB RAMPS FOR SIDEWALKS
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-04	TYPICAL PAVEMENT MARKINGS

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, BENCHMARKS, AND COMMITMENTS
3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5 - 6	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
7	SCHEDULE OF QUANTITIES
8 - 10	ALIGNMENT AND TIES
11	GEOMETRIC PLAN AND PROFILE: OLD WILLOW EASEMENT
12 - 13	GEOMETRIC PLAN AND PROFILE: CLAIRE COURT
14 - 15	GEOMETRIC PLAN AND PROFILE: WEST FORK PARK
16	GEOMETRIC PLAN AND PROFILE: WESTLEIGH DRIVE
17	MISCELLANEOUS DETAILS
18	DRIVEWAY DETAILS - DISTANCE BETWEEN ROW AND CURB OR EDGE GREATER THAN OR EQUAL TO 15' (BD-01)
19	FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
20	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
21	BUTT JOINTS AND HMA TAPER (BD-32)
22	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
24	CROSS SECTIONS: OLD WILLOW EASEMENT
25 - 27	CROSS SECTIONS: CLAIRE COURT
28	CROSS SECTIONS: WEST FORK PARK
29	CROSS SECTIONS: WESTLEIGH DRIVE

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, HIGHWAY STANDARDS,
BENCHMARKS, AND COMMITMENTS**

SCALE: NONE STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	2
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00018461				

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
- THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, FIBER OPTICS, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS ADMINISTRATOR AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS.
- THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ACCESS TO DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS. UTILIZING CRUSHED STONE OR CRUSHED GRAVEL AS TEMPORARY ACCESS.
- EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
- CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER. SIDE CURBS SHOWN IN IDOT HIGHWAY STANDARDS SHALL NOT BE INSTALLED ON THIS PROJECT.
- PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 8-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES COMMERCIAL DRIVEWAYS.
- A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT DRIVEWAY PAVEMENT.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS OR BUSINESS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS AT LEAST 24 HOURS PRIOR TO PLANNED CLOSURE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED.
- PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES RESIDENTIAL DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 1/2" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS IN BIKE ROUTE SHALL BE PLACED EVERY 10-FEET OR AS DETERMINED BY THE ENGINEER.
- ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND ALLOWED TO CURE FOR 72 HOURS PRIOR TO PLACEMENT OF SURFACE COURSE. CLASS PP CONCRETE SHALL BE USED IF PLACEMENT OF SURFACE COURSE IS PLANNED IN LESS THAN 72 HOURS. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY TO ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT, WHETHER PAID FOR SEPARATELY OR INCLUDED IN OTHER CONTRACT WORK.
- THE DAYS PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
- DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A REPLACEABLE CAST-IN-PLACE 24"x48" NOMINAL PANEL WIDTH. THE PANEL SHALL COMPLY WITH ADA REQUIREMENTS. THE PANEL COLOR SHALL BE BRICK RED UNLESS DETERMINED OTHERWISE BY THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS AND THE APPLICABLE HIGHWAY STANDARD.
- PER THE VILLAGE'S ORDINANCE, THE CONTRACTOR SHALL BE REQUIRED TO CONFINE THE WORK ACTIVITY BETWEEN 7:00 AM-7:00 PM MONDAY THROUGH FRIDAY, 9:00 AM-5:00 PM SATURDAYS. WORK ON SATURDAYS SHALL BE COORDINATED AND APPROVED IN WRITING BY THE ENGINEER AT LEAST 48 HOURS IN ADVANCE. NO WORK WILL BE PERMITTED ON HOLIDAYS WITHOUT THE ENGINEER'S WRITTEN APPROVAL. WORK ACTIVITY, AS INTENDED HEREIN, INCLUDES WARMING/STARTING UP/IDLING OF ANY MACHINERY OR ENGINES.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY AND PRIOR TO ACCEPTANCE OF THE IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL BE INCLUDED IN THE COST OF MOBILIZATION.
- VILLAGE OF GLENVIEW PUBLIC WORKS DEPARTMENT (847) 657-3030
VILLAGE OF GLENVIEW CAPITAL PROJECTS DEPARTMENT (847) 904-4400
- AGGREGATE BASE COURSE, TYPE B - 4" SHALL BE INSTALLED UNDER ALL DRIVEWAY PAVEMENTS CONSTRUCTED ACCORDING TO IDOT DISTRICT ONE DETAIL BD-01. THE AGGREGATE BASE COURSE, TYPE B - 4" WILL BE PAID FOR SEPARATELY.
- IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD AT THE JOB SITE PRIOR TO BIDDING ON THE PROJECT AND PRIOR TO ORDERING MATERIALS AND COMMENCEMENT OF CONSTRUCTION. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK. OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, SPECIALS PROVISIONS, STANDARD SPECIFICATIONS AND/OR DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE MEANING OF THE CONSTRUCTION PLANS AND/OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- THE CURB SHALL BE TAPERED TO THE GUTTER IN A FIVE (5) FOOT LENGTH WHEREVER THE CURB AND GUTTER TERMINATES, WITH AN EXPANSION JOINT PLACED AT THE START OF THE TAPER.
- ALL POSTS, RAILROAD TIES, AND DECORATIVE TIMBER IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND RELOCATED AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS TO PRESERVE THEM FROM HARM. ITEMS NOT RELOCATED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR.
- FURNISHED EXCAVATION FROM AN OFF-SITE LOCATION, IF NECESSARY, SHALL BE INCLUDED IN THE ITEM EARTH EXCAVATION.
- ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGWAYS."
- THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING: THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" GAP WITH CLASS SI CONCRETE TO AN ELEVATION 2-1/2" BELOW THE PROPOSED CURB AND GUTTER FLAG.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/4 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND SHALL NOT EXCEED 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. A MAXIMUM GRADE DIFFERENCE OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H), AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR UTILITY OWNER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH IMPACTED UTILITY OWNERS AND HIS CONSTRUCTION SCHEDULE.
- ALL REQUIRED CONSTRUCTION STAKING (BENCHMARK ESTABLISHMENT, ROW, SIDEWALK, UTILITY LOCATION AND ELEVATIONS) SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR DIMENSIONS AND ELEVATIONS MEASURED FROM CONTRACTOR'S STAKES. THE CONTRACTOR SHALL EXERCISE PROPER CARE IN THE PRESERVATION OF STAKES SET UNTIL THE ENGINEER'S APPROVAL FOR STAKE REMOVAL IS OBTAINED.
- INITIAL HORIZONTAL LAYOUT MAY BE REQUESTED BY THE ENGINEER TO VERIFY FIELD LOCATIONS. HORIZONTAL LAYOUT MAY BE ADJUSTED BY THE ENGINEER DURING CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF CONSTRUCTION LAYOUT.
- ALL TREE PROTECTION (TEMPORARY FENCE INSTALLATION), TREE REMOVAL, TREE PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE WORK OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DETERMINED BY THE ENGINEER.
- TREE REMOVAL NOTICES WILL BE POSTED BY THE VILLAGE ON THE TREES TO BE REMOVED. TREE REMOVAL WORK WILL BE ALLOWED 10 DAYS AFTER POSTING OF THE TREE REMOVAL NOTICE.

EROSION CONTROL NOTES

- ALL AREAS LOCATED DOWNSTREAM FROM DISTURBED AREAS OF CONSTRUCTION SHALL BE PROTECTED FROM POTENTIAL INCREASE OF EROSION AND SEDIMENTATION RESULTING FROM UPSTREAM ACTIVITIES.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF CONSTRUCTION SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SODDING IS PERFORMED.
- SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED AND FUNCTIONAL PRIOR TO THE START OF DISTURBANCE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED; BUT IN NO CASE SHALL THESE MEASURES BE INSTALLED MORE THAN 7 DAYS AFTER THE CONSTRUCTION IN THIS AREA TEMPORARILY OR PERMANENTLY CEASES.
- ALL STORM SEWER STRUCTURES THAT RECEIVE RUNOFF DURING CONSTRUCTION SHALL INCLUDE INLET PROTECTION FILTERS TO PREVENT DEBRIS AND EXCESSIVE SEDIMENT FROM ENTERING THE STORM SEWER SYSTEM. THESE PROTECTIVE MEASURES SHALL BE PROPERLY INSTALLED, MAINTAINED, AND REMOVED IN THEIR ENTIRETY AFTER THE AREA TRIBUTARY TO THE STORM STRUCTURE IS STABILIZED.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PROPERLY STABILIZED OR DISPOSED.
- REPAIR, REPLACE OR MAINTAIN EROSION AND SEDIMENT CONTROL STRUCTURES AFTER A RAINFALL EVENT OF 1/2 INCH OR MORE OVER A 24-HOUR PERIOD AND ON A BI-WEEKLY BASIS AS A MINIMUM.
- MAKE ADJUSTMENTS TO THE SEDIMENTATION AND EROSION CONTROL PLAN AND METHODS, AS NEEDED, TO ACCOMPLISH THE INTENDED PURPOSE.
- ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED WHEN NECESSARY OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL AND/OR DEBRIS UPON OR INTO ANY CHANNEL, DITCH, SWALE OR ANY OTHER TEMPORARY OR PERMANENT LOCATIONS MEANT TO CONVEY SITE DRAINAGE.
- IF THE CONTRACTOR IS NOTIFIED BY THE ENGINEER OF AN EROSION AND SEDIMENT CONTROL DEFICIENCY, THE DEFICIENCY MUST BE CORRECTED WITHIN 24-HOURS OF BEING NOTIFIED.

MAINTENANCE OF TRAFFIC NOTES

- EMERGENCY VEHICLE ACCESS SHALL BE MAINTAINED AT ALL TIMES. ANY SIGNIFICANT CHANGE IN TRAFFIC CONTROL SHALL HAVE PRIOR APPROVAL BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF THREE (3) DAYS NOTICE IN ADVANCE OF CONSTRUCTION ACTIVITIES WHICH IMPACT EMERGENCY SERVICES.
- ALL LANES SHALL BE OPEN TO TRAFFIC AT THE END OF EACH DAY, MAINTAINING A MINIMUM 11' WIDTH PER LANE.
- DAILY LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARDS, EXCEPT LANE CLOSURES WILL NOT BE ALLOWED ON WILLOW ROAD AT ALL.
- THE CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO DRIVEWAYS AND SIDE ROADS DURING CONSTRUCTION, UTILIZING PAY ITEMS TEMPORARY ACCESS (ROAD) AND TEMPORARY ACCESS (COMMERCIAL ENTRANCE).
- ALL COMMERCIAL PROPERTIES SHALL BE PROVIDED AT LEAST ONE INGRESS AND ONE EGRESS AT ALL TIMES. THE CONTRACTOR SHALL COORDINATE DRIVEWAY CLOSURES/MODIFICATIONS WITH THE LOCAL BUSINESSES AND THE ENGINEER. COMMERCIAL DRIVEWAYS WHICH CANNOT BE CLOSED, AS DETERMINED BY THE ENGINEER, SHALL BE CONSTRUCTED IN STAGES OR WILL BE CONSTRUCTED WHEN BUSINESSES ARE NOT OPEN USING HIGH EARLY STRENGTH CONCRETE.
- ALL DRUMS AND VERTICAL PANELS SHALL BE EQUIPPED WITH STEADY BURN BI-DIRECTIONAL LIGHTS. SPACING SHALL BE AT 50 FOOT INTERVALS ALONG WORK AREAS AND 25 FEET WHERE DIRECTION CHANGES AND TAPERS OCCUR, OR AS DIRECTED BY THE ENGINEER.
- CONSTRUCTION VEHICLES AND/OR EQUIPMENT SHALL NOT BE PARKED OVERNIGHT WITHIN VILLAGE RIGHT-OF-WAY IN FRONT OF COMMERCIAL BUSINESS PROPERTIES. THE ENGINEER WILL ASSIST THE CONTRACTOR IN IDENTIFYING CONSTRUCTION STAGING LOCATIONS AT THE START OF CONSTRUCTION.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		305	11-00178-00-BT	COOK	29	3
SCALE: NONE		STA. TO STA.		CONTRACT NO. 61A68		
		FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT TE-00018461		


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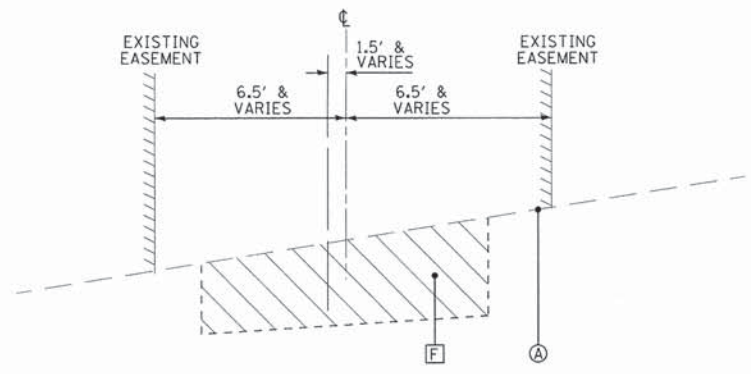
CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				PEDESTRIAN 0028	TOTAL QUANTITY
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	83	83	
20101000	TEMPORARY FENCE	FOOT	2,040	2,040	
20101100	TREE TRUNK PROTECTION	EACH	51	51	
* 20101200	TREE ROOT PRUNING	EACH	51	51	
20200100	EARTH EXCAVATION	CU YD	362	362	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	392	392	
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	2,647	2,647	
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	34	34	
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	34	34	
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	34	34	
* 25100630	EROSION CONTROL BLANKET	SQ YD	2,647	2,647	
* 25200100	SODDING	SQ YD	2,647	2,647	
* 25200200	SUPPLEMENTAL WATERING	UNIT	42	42	
* 28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	56	56	
28000305	TEMPORARY DITCH CHECKS	FOOT	10	10	
28000400	PERIMETER EROSION BARRIER	FOOT	430	430	
28000510	INLET FILTERS	EACH	8	8	
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	2,188	2,188	
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	46	46	
40200900	AGGREGATE SURFACE COURSE, TYPE B	CU YD	279	279	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	21	21	
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	104	104	
42001300	PROTECTIVE COAT	SQ YD	2,256	2,256	
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	42	42	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	18,900	18,900	
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	624	624	
42400800	DETECTABLE WARNINGS	SQ FT	96	96	

* SPECIALTY ITEM

CODE NUMBER	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE	
				PEDESTRIAN 0028	TOTAL QUANTITY
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	849	849	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	181	181	
44000600	SIDEWALK REMOVAL	SQ FT	4,915	4,915	
67100100	MOBILIZATION	L SUM	1	1	
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
72000100	SIGN PANEL - TYPE 1	SQ FT	53	53	
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	2	2	
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	225	225	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	24	24	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	114	114	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	201	201	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	14	14	
* A2002924	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 3" CALIPER, BALLED AND BURLAPPED	EACH	3	3	
* A2006524	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 3" CALIPER, BALLED AND BURLAPPED	EACH	2	2	
* A2006726	TREE, QUERCUS MACROCARPA (BUR OAK), 3" CALIPER, BALLED AND BURLAPPED	EACH	2	2	
* D2001772	EVERGREEN, PICEA ABIES (NORWAY SPRUCE), 6' HEIGHT, BALLED AN BURLAPPED	EACH	8	8	
* K0026610	TRANSPLANTED SALVAGED TREES	EACH	23	23	
* K0026620	TRANSPLANTED SALVAGED SHRUBS	EACH	10	10	
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	203	203	
* Z0013302	SEGMENTAL CONCRETE BLOCK WALL	SQ FT	300	300	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1	
X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	3	3	
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	43	43	
X4400220	CURB REMOVAL AND REPLACEMENT	FOOT	36	36	
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	1	1	
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6	

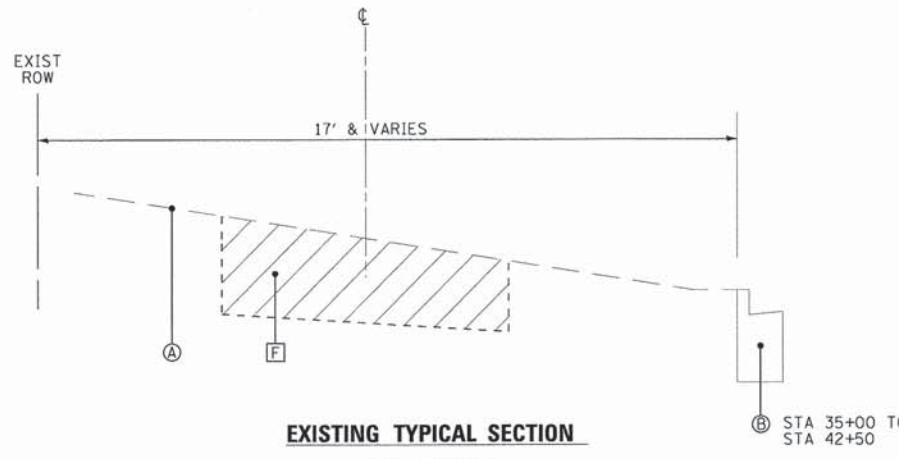
* SPECIALTY ITEM

	DESIGNED - REW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES		F.A.P. RTE. 305	SECTION 11-00178-00-RT	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 4
	DRAWN - MAC/CJC	REVISED -				C-91-361-11		CONTRACT NO. 61A68		
CHECKED - TLH	REVISED -									
DATE - 08/06/14	FILE - 120011-25.S00.sht			SCALE:	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00018461				



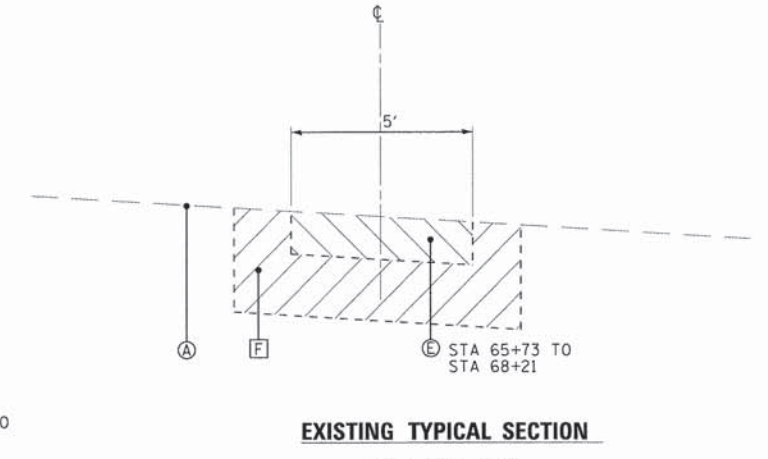
EXISTING TYPICAL SECTION

OLD WILLOW EXISTING EASEMENT
STA 50+29 TO STA 54+79



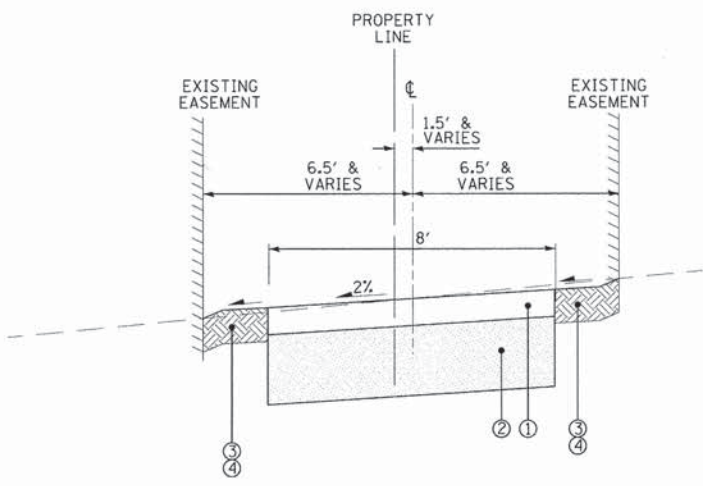
EXISTING TYPICAL SECTION

CLAIRE COURT
STA 35+00 TO STA 45+75



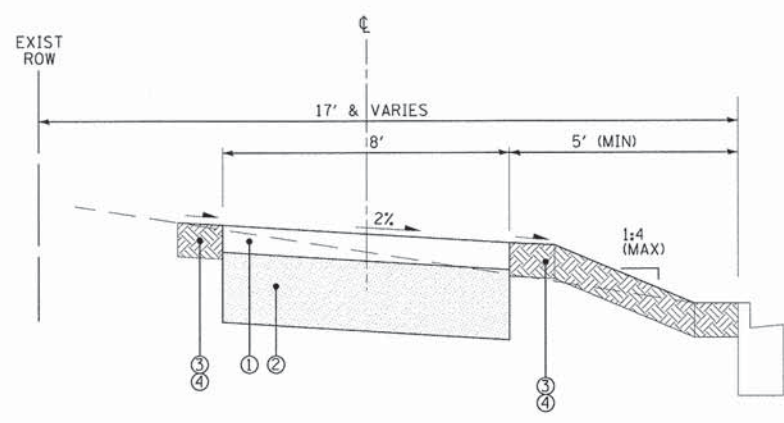
EXISTING TYPICAL SECTION

WEST FORK PARK
STA 59+60 TO STA 61+27
STA 65+73 TO STA 68+21



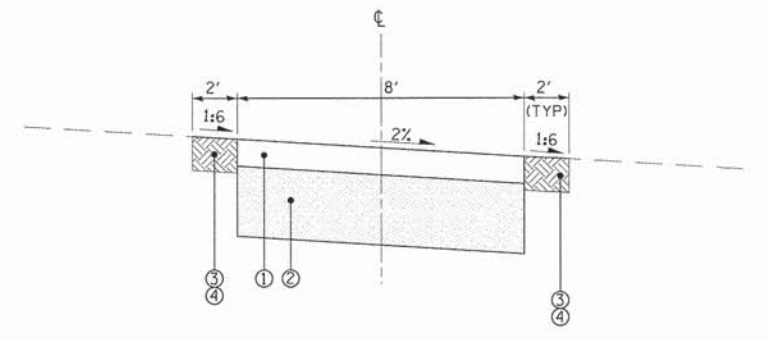
PROPOSED TYPICAL SECTION

OLD WILLOW EXISTING EASEMENT
STA 50+29 TO STA 54+79



PROPOSED TYPICAL SECTION

CLAIRE COURT
STA 35+00 TO STA 45+75



PROPOSED TYPICAL SECTION

WEST FORK PARK
STA 59+60 TO STA 61+27
STA 65+73 TO STA 68+21

EXISTING LEGEND

- AA EXISTING HMA MULTI-USE PATH
- A EXISTING GROUND SURFACE
- B EXISTING CURB AND GUTTER
- C EXISTING CONCRETE SIDEWALK
- D HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- E DISEWALK REMOVAL
- F EARTH EXCAVATION
- ITEM TO BE REMOVED

PROPOSED LEGEND

- 1 PCC SIDEWALK, 5" (THICKEN TO 8" THROUGH COMMERCIAL DRIVEWAYS)
- 2 AGGREGATE BASE COURSE, TYPE B, 4"
- 3 TOPSOIL, FURNISH AND PLACE 4"
- 4 SODDING
- 5 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50-2"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) - 2"	4% @ 50 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm) - COMM - 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm) - COMM - 6"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY THE DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

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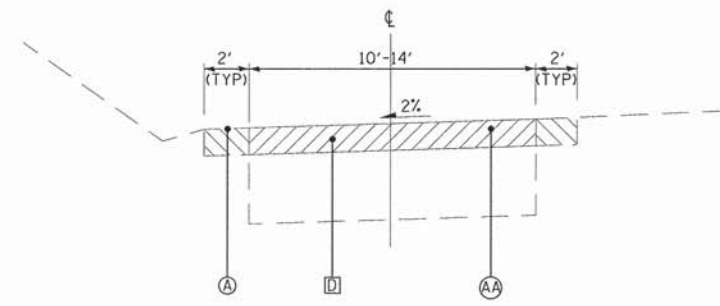
BAXTER & WOODMAN Consulting Engineers	DESIGNED - REW	REVISED - PER IDOT 9/8/14
	DRAWN - MAC/CJC	REVISED -
	CHECKED - TLH	REVISED -
	DATE - 08/06/14	FILE - 120011-25_TypSec.sht

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

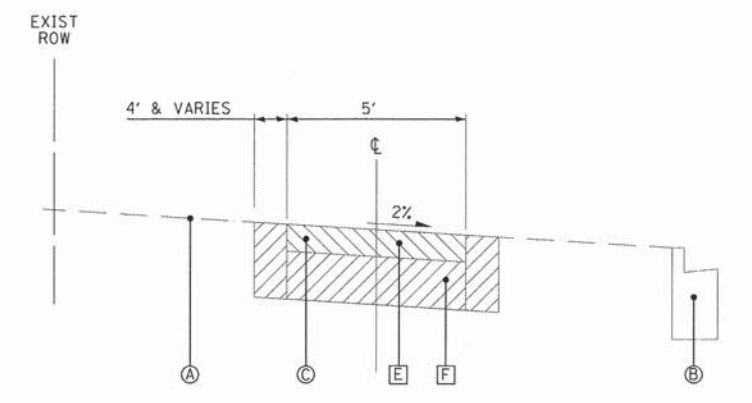
TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
SCALE:	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	5
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00018463				

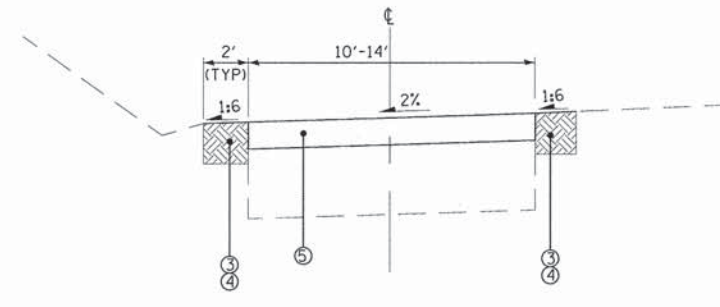
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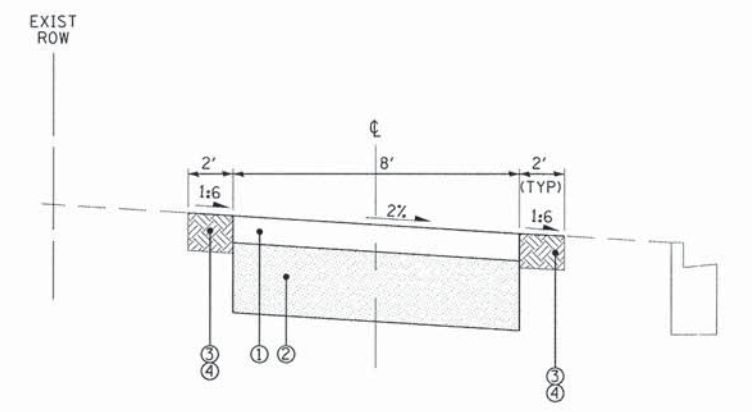
EXISTING TYPICAL SECTION
 WEST FORK PARK
 STA 61+27 TO STA 65+73



EXISTING TYPICAL SECTION
 WESTLEIGH DRIVE
 STA 79+15 TO STA 82+67



PROPOSED TYPICAL SECTION
 WEST FORK PARK
 STA 61+27 TO STA 65+73



PROPOSED TYPICAL SECTION
 WESTLEIGH DRIVE
 STA 79+15 TO STA 82+67

- EXISTING LEGEND**
- AA EXISTING HMA MULTI-USE PATH
 - A EXISTING GROUND SURFACE
 - B EXISTING CURB AND GUTTER
 - C EXISTING CONCRETE SIDEWALK
 - D HOT-MIX ASPHALT SURFACE REMOVAL, 2"
 - E DISEWALK REMOVAL
 - F EARTH EXCAVATION
 - ▨ ITEM TO BE REMOVED

- PROPOSED LEGEND**
- 1 PCC SIDEWALK, 5" (THICKEN TO 8" THROUGH COMMERCIAL DRIVEWAYS)
 - 2 AGGREGATE BASE COURSE, TYPE B, 4"
 - 3 TOPSOIL, FURNISH AND PLACE 4"
 - 4 SODDING
 - 5 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50-2"

BAXTER & WOODMAN Consulting Engineers	DESIGNED - REW	REVISED -
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	CHECKED - TLH	REVISED -
	DATE - 08/06/14	FILE - 120011-25_TypSec.sht

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS AND
 HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

SCALE: STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	6
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001846				

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EARTHWORK SUMMARY	A	B	C = A + B	D	E = D x 0.85	F	G = E - F
LOCATION	UNDERCUT	UNSUITABLE EXCAVATION (TOPSOIL)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
WEST FORK PARK	-	62	62	47	40	3	+37
WESTLEIGH DRIVE	-	49	49	36	31	2	+29
OLD WILLOW EASEMENT	-	100	100	98	83	4	+79
CLAIRE COURT	-	181	181	181	154	38	+116
TOTAL	-	392	392	362	308	47	+261

TREE SCHEDULE					
CODE	PAY ITEM	LABEL	UNIT	TOTAL	
A2002924	TREE, CELTIS OCCIDENTALIS (COMMON HACKBERRY), 3" CALIPER, BALLED AND BURLAPPED	CH	EACH	3	
A2006524	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), 3" CALIPER, BALLED AND BURLAPPED	SWO	EACH	2	
A2006726	TREE, QUERCUS MACROCARPA (BUR OAK), 3" CALIPER, BALLED AND BURLAPPED	BO	EACH	2	
D2001772	EVERGREEN, PICEA ABIES (NORWAY SPRUCE), 6' HEIGHT, BALLED AN BURLAPPED	NS	EACH	8	

BAXTER & WOODMAN Consulting Engineers	DESIGNED - REW	REVISED -
	DRAWN - MAC/CJC	REVISED -
	CHECKED - TLH	REVISED -
	DATE - 08/06/14	FILE - 120011-25_Sch00.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES		
SCALE:	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	7
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-00018461				

Curve Data			
Curve NORTH PARK-1			
P.I. Station	60+58 N	1,980,855.33 E	1,126,622.97
Delta	83° 32' 41.95" (RT)		
Degree	81° 51' 04.01"		
Tangent	62.53		
Length	102.07		
Radius	70.00		
External	23.86		
Long Chord	93.26		
Mid. Ord.	17.79		
P.C. Station	59+96 N	1,980,795.62 E	1,126,604.44
P.T. Station	60+98 N	1,980,843.62 E	1,126,684.40
C.C.	N	1,980,774.86 E	1,126,671.29
Back	= N 17° 14' 53.44" E		
Ahead	= S 79° 12' 24.61" E		
Chord Bear	= N 59° 01' 14.41" E		

Course from PT NORTH PARK-1 to NORTH2 S 79° 12' 24.61" E Dist 35.88
 Point NORTH2 N 1,980,836.90 E 1,126,719.64 Sta 61+34
 Course from NORTH2 to PC NORTH PARK-2 N 10° 08' 14.79" E Dist 82.56

Curve Data			
Curve NORTH PARK-2			
P.I. Station	63+87 N	1,981,085.98 E	1,126,764.17
Delta	79° 29' 14.40" (RT)		
Degree	27° 56' 56.98"		
Tangent	170.46		
Length	284.40		
Radius	205.00		
External	61.61		
Long Chord	262.14		
Mid. Ord.	47.37		
P.C. Station	62+16 N	1,980,918.18 E	1,126,734.17
P.T. Station	65+01 N	1,981,087.09 E	1,126,934.63
C.C.	N	1,980,882.10 E	1,126,935.97
Back	= N 10° 08' 14.79" E		
Ahead	= N 89° 37' 29.19" E		
Chord Bear	= N 49° 52' 51.99" E		

Course from PT NORTH PARK-2 to NORTH3 N 89° 37' 29.19" E Dist 64.72
 Point NORTH3 N 1,981,087.52 E 1,126,999.35 Sta 65+66
 Course from NORTH3 to PC NORTH PARK-3 S 0° 04' 59.69" W Dist 9.18

Curve Data			
Curve NORTH PARK-3			
P.I. Station	66+06 N	1,981,046.77 E	1,126,999.29
Delta	96° 51' 05.51" (LT)		
Degree	204° 37' 40.02"		
Tangent	31.57		
Length	47.33		
Radius	28.00		
External	14.19		
Long Chord	41.89		
Mid. Ord.	9.42		
P.C. Station	65+75 N	1,981,078.33 E	1,126,999.34
P.T. Station	66+22 N	1,981,050.49 E	1,127,030.64
C.C.	N	1,981,078.29 E	1,127,027.34
Back	= S 0° 04' 59.69" W		
Ahead	= N 83° 13' 54.18" E		
Chord Bear	= S 48° 20' 33.06" E		

Course from PT NORTH PARK-3 to PC NORTH PARK-4 N 83° 13' 54.18" E Dist 64.41

CONTROL POINTS	
CP 1, N 1978812.6636, E 1125089.7541, ELEV. 632.91, CP OLD WILLOW RD	
CP 3, N 1981092.3715, E 1127260.8196, ELEV. 636.65 CP WESTLEIGH	
CP 4, N 1981093.4183, E 1126955.6474, ELEV. 637.04 CP PK NAIL IN PATH	

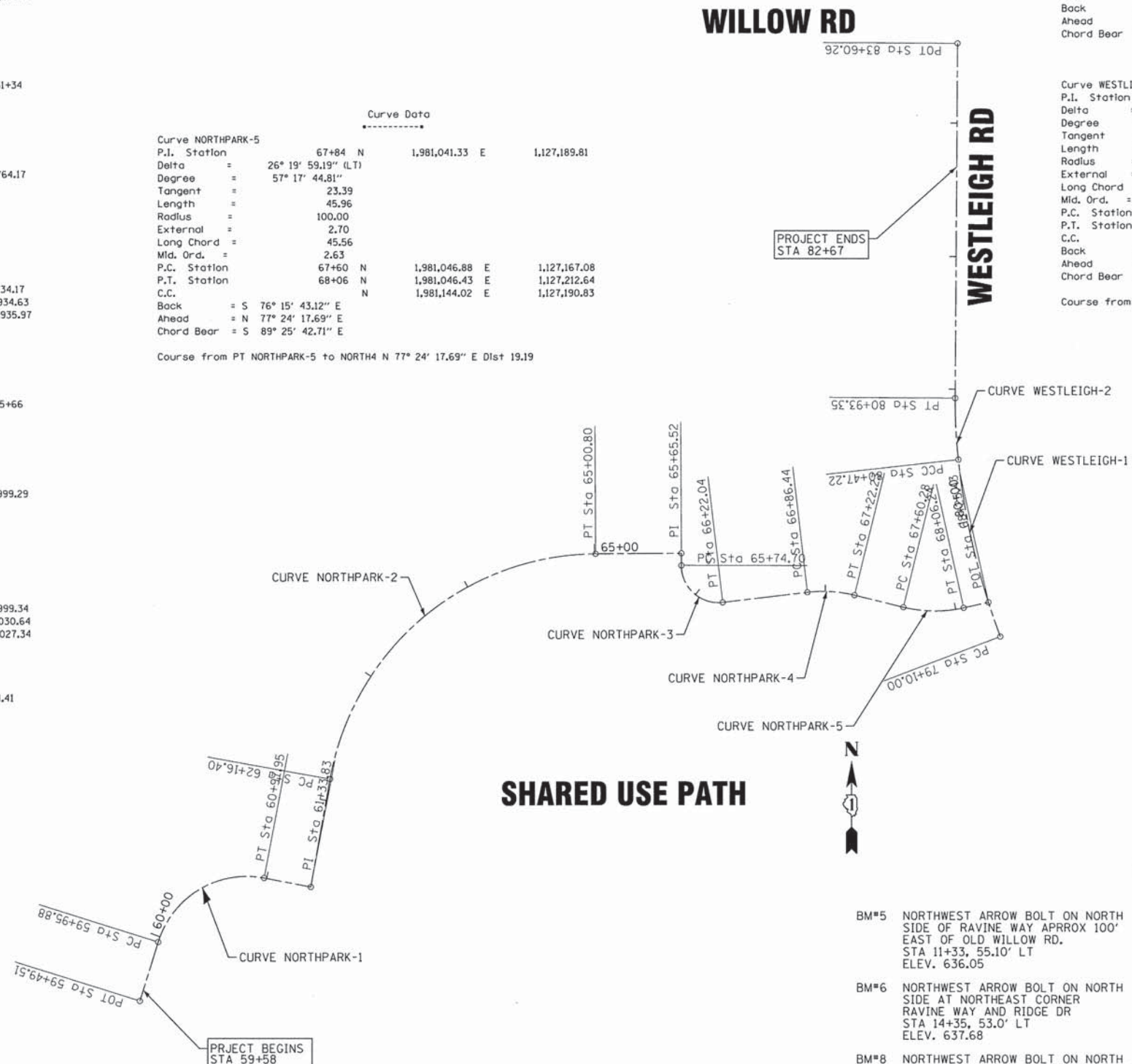
Curve Data			
Curve NORTH PARK-5			
P.I. Station	67+84 N	1,981,041.33 E	1,127,189.81
Delta	26° 19' 59.19" (LT)		
Degree	57° 17' 44.81"		
Tangent	23.39		
Length	45.96		
Radius	100.00		
External	2.70		
Long Chord	45.56		
Mid. Ord.	2.63		
P.C. Station	67+60 N	1,981,046.88 E	1,127,167.08
P.T. Station	68+06 N	1,981,046.43 E	1,127,212.64
C.C.	N	1,981,144.02 E	1,127,190.83
Back	= S 76° 15' 43.12" E		
Ahead	= N 77° 24' 17.69" E		
Chord Bear	= S 89° 25' 42.71" E		

Course from PT NORTH PARK-5 to NORTH4 N 77° 24' 17.69" E Dist 19.19

Curve Data			
Curve WESTLEIGH-1			
P.I. Station	79+79 N	1,981,089.40 E	1,127,216.00
Delta	14° 24' 19.87" (RT)		
Degree	10° 29' 52.95"		
Tangent	68.97		
Length	137.22		
Radius	545.78		
External	4.34		
Long Chord	136.86		
Mid. Ord.	4.31		
P.C. Station	79+10 N	1,981,024.87 E	1,127,240.35
P.T. Station	80+47 N	1,981,157.96 E	1,127,208.46
C.C.	N	1,981,217.61 E	1,127,750.96
Back	= N 20° 40' 47.95" W		
Ahead	= N 6° 16' 28.08" W		
Chord Bear	= N 13° 28' 38.01" W		

Curve Data			
Curve WESTLEIGH-2			
P.I. Station	80+70 N	1,981,180.92 E	1,127,205.93
Delta	6° 36' 30.29" (RT)		
Degree	14° 19' 26.20"		
Tangent	23.09		
Length	46.14		
Radius	400.00		
External	0.67		
Long Chord	46.11		
Mid. Ord.	0.66		
P.C. Station	80+47 N	1,981,157.96 E	1,127,208.46
P.T. Station	80+93 N	1,981,204.01 E	1,127,206.07
C.C.	N	1,981,201.68 E	1,127,606.06
Back	= N 6° 16' 28.08" W		
Ahead	= N 0° 20' 02.22" E		
Chord Bear	= N 13° 58' 12.93" W		

Course from PT WESTLEIGH-2 to WEST2 N 0° 20' 02.22" E Dist 266.91



BENCHMARKS	
DATUM: NAVD 88	
BM#5	NORTHWEST ARROW BOLT ON NORTH SIDE OF RAVINE WAY APPROX 100' EAST OF OLD WILLOW RD. STA 11+33, 55.10' LT ELEV. 636.05
BM#6	NORTHWEST ARROW BOLT ON NORTH SIDE AT NORTHEAST CORNER RAVINE WAY AND RIDGE DR STA 14+35, 53.0' LT ELEV. 637.68
BM#8	NORTHWEST ARROW BOLT ON NORTH SIDE OF CLAIRE CT. ACCROSS FROM SE CORNER OF ADD 2301 STA 34+72, 7.5' LT ELEV. 639.27
BM#10	NORTHWEST ARROW BOLT ON NORTH SIDE OF CLAIRE CT. AT HOSPICE CENTER STA 40+92, 12.5' LT ELEV. 636.20
BM#12	NORTH ARROW BOLT ON FIRE HYDRANT IN PARKING AREA FOR NATURE AREA AT EAST END OF CLAIRE CT NEAR HOSPICE CENTER STA 44+44, 9.72' RT ELEV. 635.58
BM#13	NORTHWEST ARROW BOLT OF FIRE HYDRANT ON SOUTH WEST CORNER OF WILLOW RD AND WESTLEIGH DR STA 42+46, 8.93' RT ELEV. 635.29

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND TIES
WEST FORK PARK/WESTLEIGH DRIVE

SCALE: 1"=50'
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	8
				CONTRACT NO. 61A68
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00018461				

Chain OLDWILLOW contains:
 OLDWILLOW1 CUR OLDWILLOW-1 CUR OLDWILLOW-2 CUR OLDWILLOW-3 CUR OLDWILLOW-4 CUR
 OLDWILLOW-5 OLDWILLOW2

Beginning chain OLDWILLOW description
 =====

Point OLDWILLOW1 N 1,978,085.26 E 1,125,127.14 Sta 50+00

Course from OLDWILLOW1 to PC OLDWILLOW-1 N 84° 05' 13.04" E Dist 65.77

Curve Data

Curve OLDWILLOW-1
 P.I. Station 50+91 N 1,978,094.65 E 1,125,217.79
 Delta = 28° 28' 11.84" (LT)
 Degree = 57° 17' 44.81"
 Tangent = 25.37
 Length = 49.69
 Radius = 100.00
 External = 3.17
 Long Chord = 49.18
 Mid. Ord. = 3.07
 P.C. Station 50+66 N 1,978,092.04 E 1,125,192.56
 P.T. Station 51+15 N 1,978,108.98 E 1,125,238.73
 C.C. N 1,978,191.51 E 1,125,182.26
 Back = N 84° 05' 13.04" E
 Ahead = N 55° 37' 01.20" E
 Chord Bear = N 69° 51' 07.12" E

Course from PT OLDWILLOW-1 to PC OLDWILLOW-2 N 55° 37' 01.20" E Dist 190.01

Curve Data

Curve OLDWILLOW-2
 P.I. Station 53+20 N 1,978,224.60 E 1,125,407.70
 Delta = 16° 45' 51.05" (RT)
 Degree = 57° 17' 44.81"
 Tangent = 14.73
 Length = 29.26
 Radius = 100.00
 External = 1.08
 Long Chord = 29.15
 Mid. Ord. = 1.07
 P.C. Station 53+05 N 1,978,216.28 E 1,125,395.54
 P.T. Station 53+35 N 1,978,229.06 E 1,125,421.74
 C.C. N 1,978,133.75 E 1,125,452.01
 Back = N 55° 37' 01.20" E
 Ahead = N 72° 22' 52.24" E
 Chord Bear = N 63° 59' 56.72" E

Curve OLDWILLOW-3
 P.I. Station 53+49 N 1,978,233.52 E 1,125,435.79
 Delta = 16° 45' 51.05" (LT)
 Degree = 57° 17' 44.73"
 Tangent = 14.73
 Length = 29.26
 Radius = 100.00
 External = 1.08
 Long Chord = 29.15
 Mid. Ord. = 1.07
 P.C. Station 53+35 N 1,978,229.06 E 1,125,421.74
 P.T. Station 53+64 N 1,978,241.84 E 1,125,447.95
 C.C. N 1,978,324.37 E 1,125,391.48
 Back = N 72° 22' 52.24" E
 Ahead = N 55° 37' 01.20" E
 Chord Bear = N 63° 59' 56.72" E

Course from PT OLDWILLOW-3 to PC OLDWILLOW-4 N 55° 37' 01.20" E Dist 31.46

Curve Data

Curve OLDWILLOW-4
 P.I. Station 54+29 N 1,978,278.50 E 1,125,501.52
 Delta = 36° 59' 36.76" (RT)
 Degree = 57° 17' 44.81"
 Tangent = 33.45
 Length = 64.57
 Radius = 100.00
 External = 5.45
 Long Chord = 63.45
 Mid. Ord. = 5.17
 P.C. Station 53+95 N 1,978,259.61 E 1,125,473.91
 P.T. Station 54+60 N 1,978,276.97 E 1,125,534.94
 C.C. N 1,978,177.08 E 1,125,530.38
 Back = N 55° 37' 01.20" E
 Ahead = S 87° 23' 22.05" E
 Chord Bear = N 74° 06' 49.57" E

Curve OLDWILLOW-5
 P.I. Station 54+68 N 1,978,276.61 E 1,125,542.99
 Delta = 43° 55' 50.69" (LT)
 Degree = 286° 28' 44.03"
 Tangent = 8.07
 Length = 15.33
 Radius = 20.00
 External = 1.57
 Long Chord = 14.96
 Mid. Ord. = 1.45
 P.C. Station 54+60 N 1,978,276.97 E 1,125,534.94
 P.T. Station 54+75 N 1,978,281.93 E 1,125,549.05
 C.C. N 1,978,296.95 E 1,125,535.85
 Back = S 87° 23' 22.05" E
 Ahead = N 48° 40' 47.26" E
 Chord Bear = N 70° 38' 42.61" E

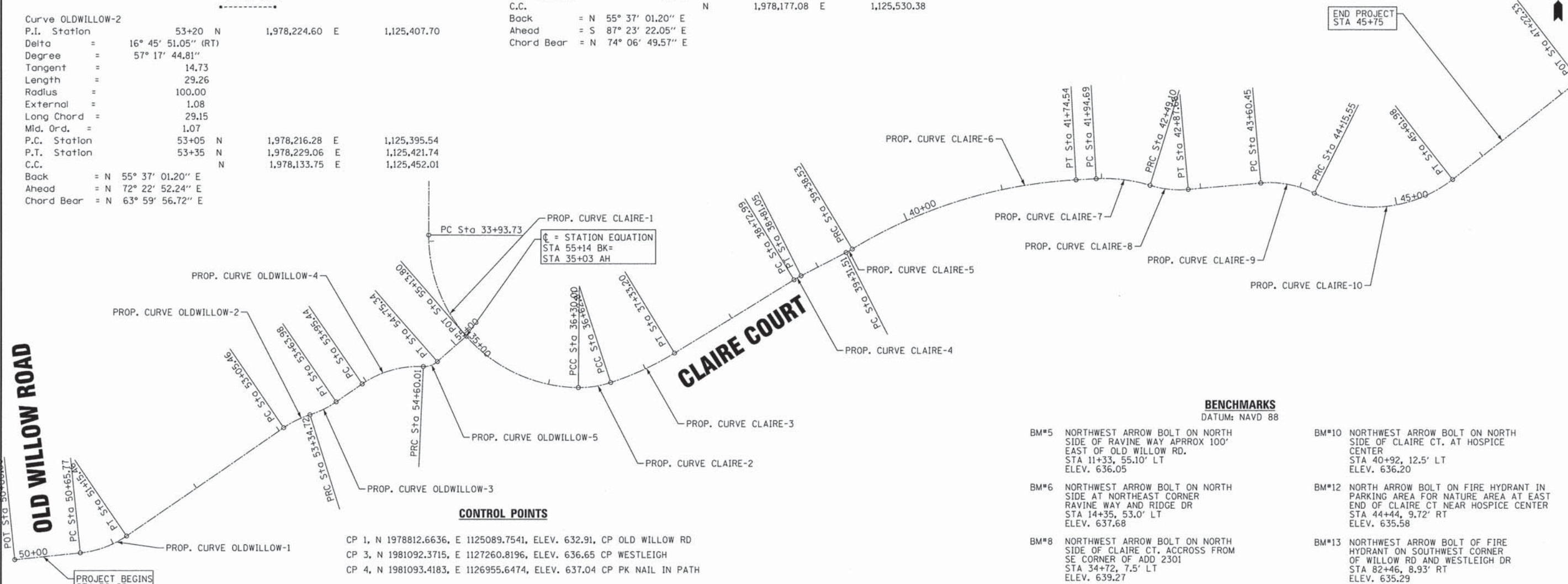
Course from PT OLDWILLOW-5 to OLDWILLOW2 N 48° 40' 47.26" E Dist 38.46

Point OLDWILLOW2 N 1,978,307.33 E 1,125,577.94 Sta 55+14

Ending chain OLDWILLOW description
 =====

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OLD WILLOW ROAD



Q = STATION EQUATION
 STA 55+14 BK=
 STA 35+03 AH

CONTROL POINTS
 CP 1, N 1978812.6636, E 1125089.7541, ELEV. 632.91, CP OLD WILLOW RD
 CP 3, N 1981092.3715, E 1127260.8196, ELEV. 636.65 CP WESTLEIGH
 CP 4, N 1981093.4183, E 1126955.6474, ELEV. 637.04 CP PK NAIL IN PATH

BENCHMARKS
 DATUM: NAVD 88

BM#5	NORTHWEST ARROW BOLT ON NORTH SIDE OF RAVINE WAY APPROX 100' EAST OF OLD WILLOW RD. STA 11+33, 55.10' LT ELEV. 636.05	BM#10	NORTHWEST ARROW BOLT ON NORTH SIDE OF CLAIRE CT. AT HOSPICE CENTER STA 40+92, 12.5' LT ELEV. 636.20
BM#6	NORTHWEST ARROW BOLT ON NORTH SIDE AT NORTHEAST CORNER RAVINE WAY AND RIDGE DR STA 14+35, 53.0' LT ELEV. 637.68	BM#12	NORTH ARROW BOLT ON FIRE HYDRANT IN PARKING AREA FOR NATURE AREA AT EAST END OF CLAIRE CT NEAR HOSPICE CENTER STA 44+44, 8.92' RT ELEV. 635.58
BM#8	NORTHWEST ARROW BOLT ON NORTH SIDE OF CLAIRE CT. ACCROSS FROM SE CORNER OF ADD 2301 STA 34+72, 7.5' LT ELEV. 639.27	BM#13	NORTHWEST ARROW BOLT OF FIRE HYDRANT ON SOUTHWEST CORNER OF WILLOW RD AND WESTLEIGH DR STA 82+46, 8.93' RT ELEV. 635.29

BAXTER & WOODMAN Consulting Engineers	DESIGNED - REW	REVISED -
	DRAWN - MAC/CJC	REVISED -
	CHECKED - TLH	REVISED -
	DATE - 08/06/14	FILE - 120011-25_Align.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND TIES
OLD WILLOW EASEMENT/CLAIRE COURT

SCALE: STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	9
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001946				

<* 1 Describe Chain CLAIRE

Chain CLAIRE contains:
 CLAIRE1 CUR CLAIRE-1 CUR CLAIRE-2 CUR CLAIRE-3 CUR CLAIRE-4 CUR CLAIRE-5 CUR C-
 LAIRE-6 CUR CLAIRE-7 CUR CLAIRE-8 CUR CLAIRE-9 CUR CLAIRE-10 CLAIRE2

Beginning chain CLAIRE description
 =====
 Point CLAIRE1 N 1,978,775.33 E 1,125,541.75 Sta 30+26
 Course from CLAIRE1 to PC CLAIRE-1 S 0° 13' 09.09" W Dist 367.73

Curve Data

Curve CLAIRE-1
 P.I. Station 35+44 N 1,978,257.53 E 1,125,539.77
 Delta = 89° 38' 53.23" (LT)
 Degree = 37° 56' 39.21"
 Tangent = 150.08
 Length = 236.26
 Radius = 151.00
 External = 61.89
 Long Chord = 212.89
 Mid. Ord. = 43.90
 P.C. Station 33+94 N 1,978,407.60 E 1,125,540.34
 P.T. Station 36+30 N 1,978,256.03 E 1,125,689.84
 C.C. N 1,978,407.03 E 1,125,691.34
 Back = S 0° 13' 09.09" W
 Ahead = S 89° 25' 44.14" E
 Chord Bear = S 44° 36' 17.52" E

Curve Data

Curve CLAIRE-2
 P.I. Station 36+47 N 1,978,255.87 E 1,125,706.42
 Delta = 18° 39' 05.76" (LT)
 Degree = 56° 43' 42.58"
 Tangent = 16.59
 Length = 32.88
 Radius = 101.00
 External = 1.35
 Long Chord = 32.73
 Mid. Ord. = 1.33
 P.C. Station 36+30 N 1,978,256.03 E 1,125,689.84
 P.T. Station 36+63 N 1,978,261.02 E 1,125,722.19
 C.C. N 1,978,357.03 E 1,125,690.84
 Back = S 89° 25' 44.14" E
 Ahead = N 71° 55' 10.10" E
 Chord Bear = N 81° 14' 42.98" E

Curve Data

Curve CLAIRE-3
 P.I. Station 36+98 N 1,978,271.98 E 1,125,755.77
 Delta = 13° 23' 11.83" (LT)
 Degree = 19° 02' 06.51"
 Tangent = 35.32
 Length = 70.33
 Radius = 301.00
 External = 2.07
 Long Chord = 70.17
 Mid. Ord. = 2.05
 P.C. Station 36+63 N 1,978,261.02 E 1,125,722.19
 P.T. Station 37+33 N 1,978,290.42 E 1,125,785.90
 C.C. N 1,978,547.15 E 1,125,628.77
 Back = N 71° 55' 10.10" E
 Ahead = N 58° 31' 58.27" E
 Chord Bear = N 65° 13' 34.19" E

Course from PT CLAIRE-3 to PC CLAIRE-4 N 58° 31' 58.27" E Dist 139.79

Curve Data

Curve CLAIRE-4
 P.I. Station 38+77 N 1,978,365.50 E 1,125,908.57
 Delta = 4° 26' 12.42" (RT)
 Degree = 55° 05' 31.54"
 Tangent = 4.03
 Length = 8.05
 Radius = 104.00
 External = 0.08
 Long Chord = 8.05
 Mid. Ord. = 0.08
 P.C. Station 38+73 N 1,978,363.39 E 1,125,905.13
 P.T. Station 38+81 N 1,978,367.33 E 1,125,912.16
 C.C. N 1,978,274.69 E 1,125,959.42
 Back = N 58° 31' 58.27" E
 Ahead = N 62° 58' 10.69" E
 Chord Bear = N 60° 45' 04.48" E

Course from PT CLAIRE-4 to PC CLAIRE-5 N 62° 58' 10.69" E Dist 50.47

Curve Data

Curve CLAIRE-5
 P.I. Station 39+35 N 1,978,391.86 E 1,125,960.24
 Delta = 4° 01' 21.24" (LT)
 Degree = 57° 17' 44.81"
 Tangent = 3.51
 Length = 7.02
 Radius = 100.00
 External = 0.06
 Long Chord = 7.02
 Mid. Ord. = 0.06
 P.C. Station 39+32 N 1,978,390.26 E 1,125,957.11
 P.T. Station 39+39 N 1,978,393.67 E 1,125,963.25
 C.C. N 1,978,479.34 E 1,125,911.66
 Back = N 62° 58' 10.69" E
 Ahead = N 58° 56' 49.45" E
 Chord Bear = N 60° 57' 30.07" E

Curve Data

Curve CLAIRE-6
 P.I. Station 40+59 N 1,978,455.77 E 1,126,066.39
 Delta = 27° 54' 34.49" (RT)
 Degree = 11° 49' 32.72"
 Tangent = 120.39
 Length = 236.01
 Radius = 484.50
 External = 14.73
 Long Chord = 233.68
 Mid. Ord. = 14.30
 P.C. Station 39+39 N 1,978,393.67 E 1,125,963.25
 P.T. Station 41+75 N 1,978,462.37 E 1,126,186.60
 C.C. N 1,977,978.60 E 1,126,213.17
 Back = N 58° 56' 49.45" E
 Ahead = N 86° 51' 23.94" E
 Chord Bear = N 72° 54' 06.69" E

Course from PT CLAIRE-6 to PC CLAIRE-7 N 86° 51' 23.94" E Dist 20.15

Curve Data

Curve CLAIRE-7
 P.I. Station 42+22 N 1,978,464.98 E 1,126,234.17
 Delta = 20° 14' 36.46" (RT)
 Degree = 37° 12' 18.19"
 Tangent = 27.49
 Length = 54.41
 Radius = 154.00
 External = 2.43
 Long Chord = 54.13
 Mid. Ord. = 2.40
 P.C. Station 41+95 N 1,978,463.48 E 1,126,206.72
 P.T. Station 42+49 N 1,978,456.90 E 1,126,260.45
 C.C. N 1,978,309.71 E 1,126,215.17
 Back = N 86° 51' 23.94" E
 Ahead = S 72° 53' 59.61" E
 Chord Bear = S 83° 01' 17.83" E

Curve Data

Curve CLAIRE-8
 P.I. Station 42+69 N 1,978,451.16 E 1,126,279.11
 Delta = 22° 05' 27.67" (LT)
 Degree = 57° 17' 44.81"
 Tangent = 19.52
 Length = 38.56
 Radius = 100.00
 External = 1.89
 Long Chord = 38.32
 Mid. Ord. = 1.85
 P.C. Station 42+49 N 1,978,456.90 E 1,126,260.45
 P.T. Station 42+88 N 1,978,452.86 E 1,126,298.55
 C.C. N 1,978,552.48 E 1,126,289.85
 Back = S 72° 53' 59.61" E
 Ahead = N 85° 00' 32.72" E
 Chord Bear = S 83° 56' 43.44" E

Course from PT CLAIRE-8 to PC CLAIRE-9 N 85° 00' 32.72" E Dist 72.79

Curve Data

Curve CLAIRE-9
 P.I. Station 43+89 N 1,978,461.65 E 1,126,399.23
 Delta = 31° 34' 09.84" (RT)
 Degree = 57° 17' 44.81"
 Tangent = 28.27
 Length = 55.10
 Radius = 100.00
 External = 3.92
 Long Chord = 54.40
 Mid. Ord. = 3.77
 P.C. Station 43+60 N 1,978,459.19 E 1,126,371.07
 P.T. Station 44+16 N 1,978,449.00 E 1,126,424.51
 C.C. N 1,978,359.57 E 1,126,379.77
 Back = N 85° 00' 32.72" E
 Ahead = S 63° 25' 17.44" E
 Chord Bear = S 79° 12' 22.36" E

Curve Data

Curve CLAIRE-10
 P.I. Station 44+98 N 1,978,412.28 E 1,126,497.92
 Delta = 64° 32' 10.69" (LT)
 Degree = 44° 04' 25.24"
 Tangent = 82.08
 Length = 146.43
 Radius = 130.00
 External = 23.74
 Long Chord = 138.81
 Mid. Ord. = 20.08
 P.C. Station 44+16 N 1,978,449.00 E 1,126,424.51
 P.T. Station 45+62 N 1,978,462.76 E 1,126,562.63
 C.C. N 1,978,565.27 E 1,126,482.67
 Back = S 63° 25' 17.44" E
 Ahead = N 52° 02' 31.87" E
 Chord Bear = N 84° 18' 37.21" E

Course from PT CLAIRE-10 to CLAIRE2 N 52° 02' 31.87" E Dist 160.36

Point CLAIRE2 N 1,978,561.40 E 1,126,689.07 Sta 47+22

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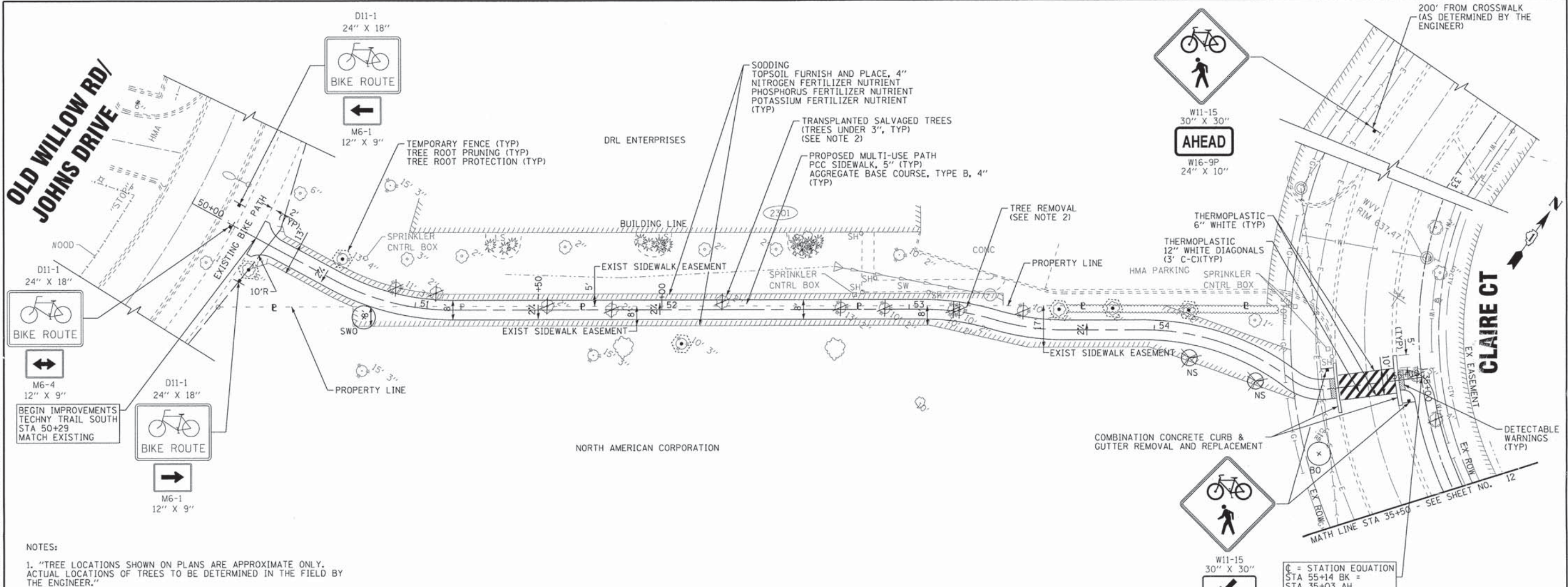
Ending chain CLAIRE description

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	DRAWN - MAC/CJC	REVISED -		SCALE: STA. TO STA.		C-91-361-11		CONTRACT NO. 61A68		FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00018461	
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	DATE - 08/06/14	FILE - I20011-25_Algln.shd									

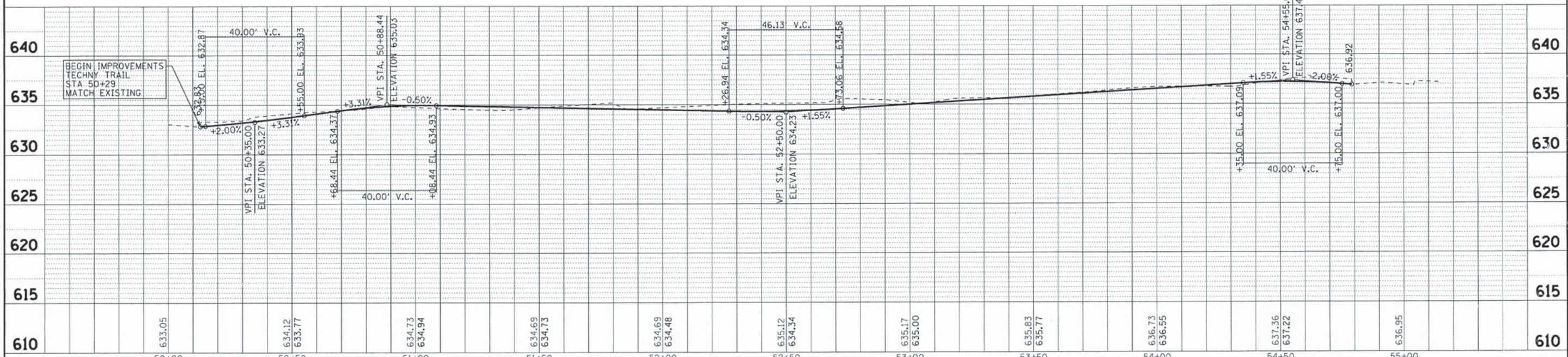
**OLD WILLOW RD/
JOHNS DRIVE**

CLAIRE CT



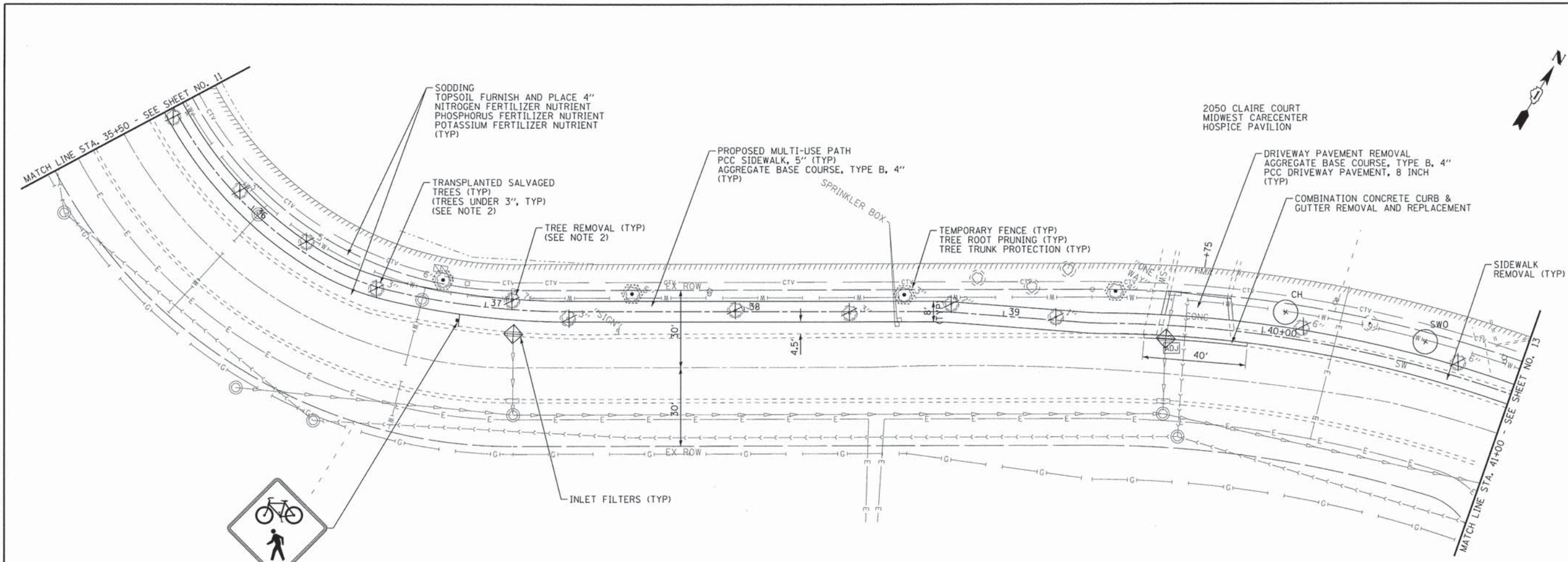
- NOTES:**
- "TREE LOCATIONS SHOWN ON PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS OF TREES TO BE DETERMINED IN THE FIELD BY THE ENGINEER."
 - "TREE REMOVAL AND TREE TRANSPLANTING SHALL NOT COMMENCE UNTIL LOCATIONS ARE APPROVED IN THE FIELD BY THE ENGINEER."

OLD WILLOW EXISTING PERMANENT EASEMENT



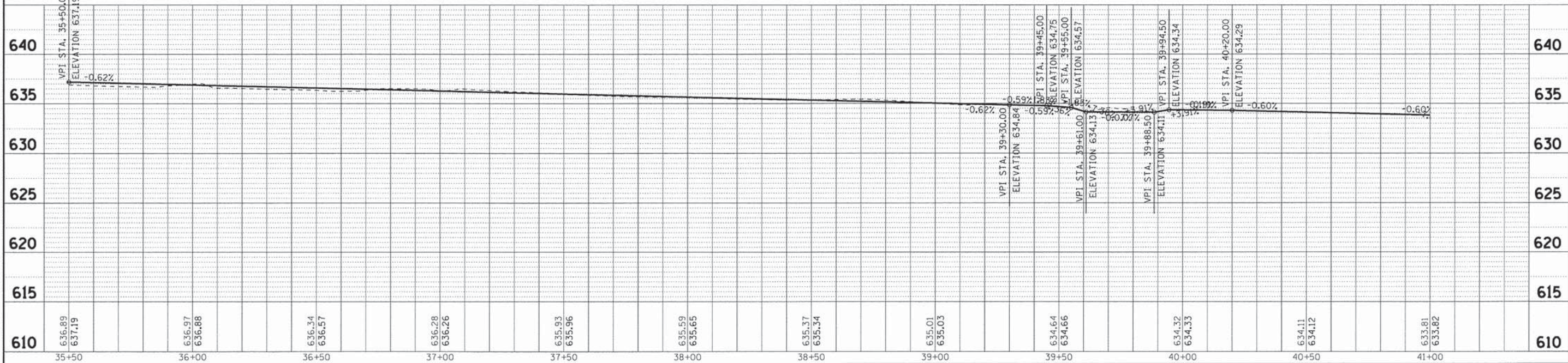
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	DRAWN - MAC/CJC	REVISED -			C-91-361-11		CONTRACT NO. 61A68			
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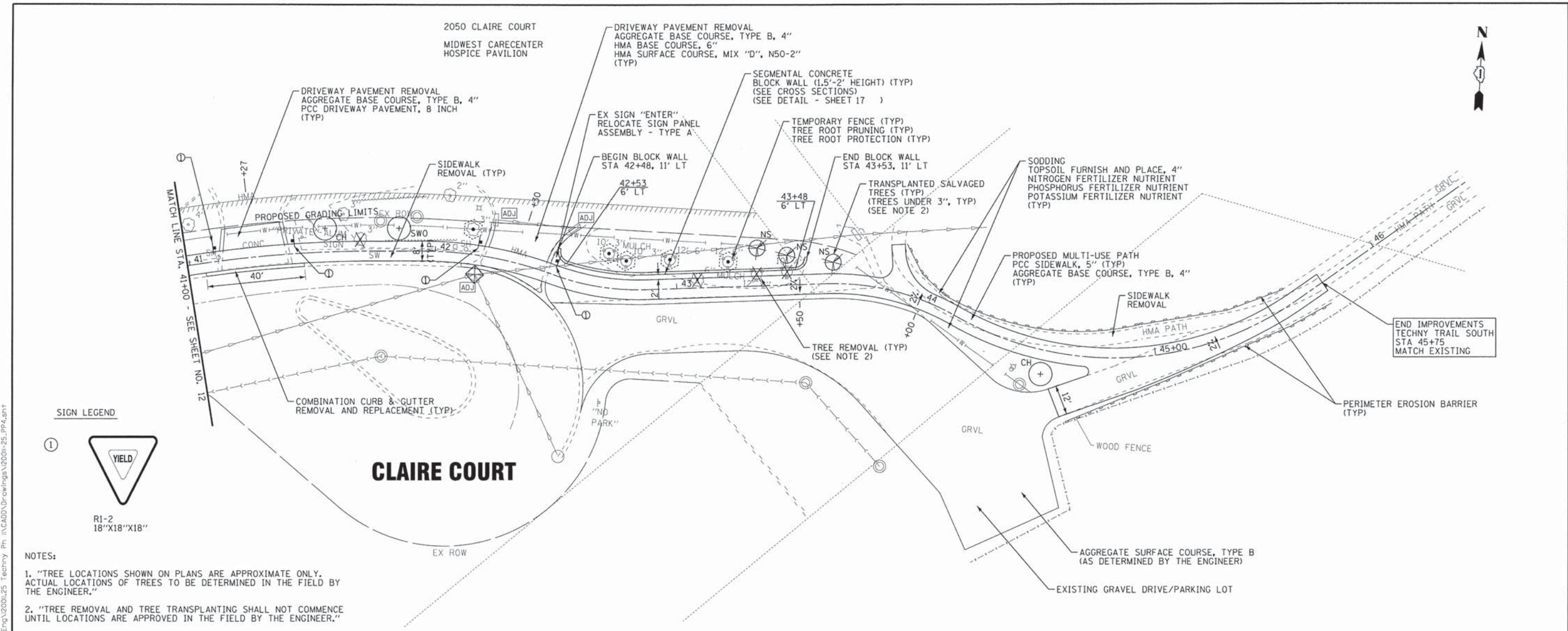
CLAIRE COURT

- NOTES:
- "TREE LOCATIONS SHOWN ON PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS OF TREES TO BE DETERMINED IN THE FIELD BY THE ENGINEER."
 - "TREE REMOVAL AND TREE TRANSPLANTING SHALL NOT COMMENCE UNTIL LOCATIONS ARE APPROVED IN THE FIELD BY THE ENGINEER."

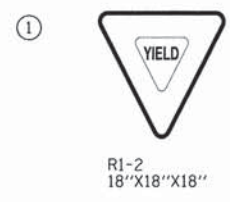


BAXTER & WOODMAN Consulting Engineers	DESIGNED - TLH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN AND PROFILE CLAIRE COURT		F.A.P. RTE. 305	SECTION 11-00178-00-BT	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 12
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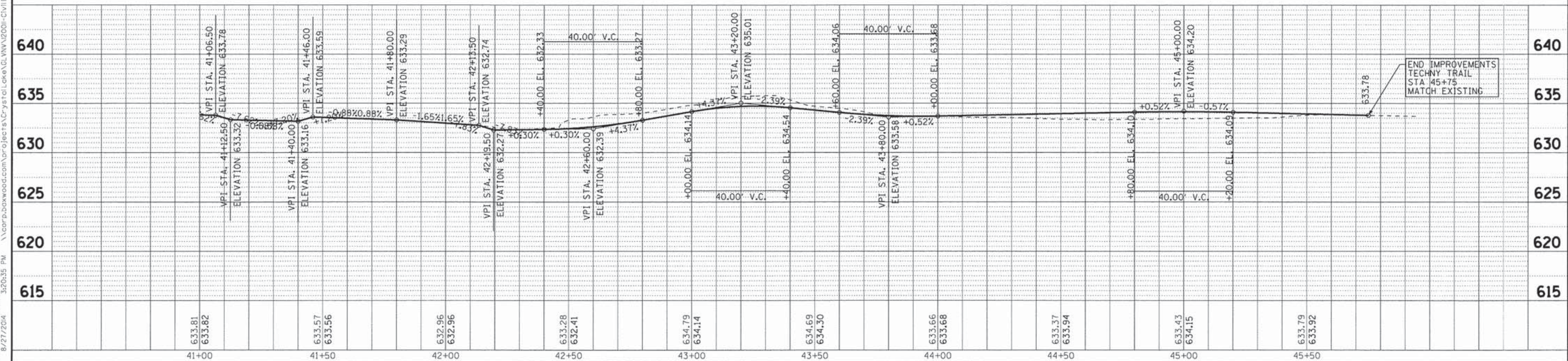
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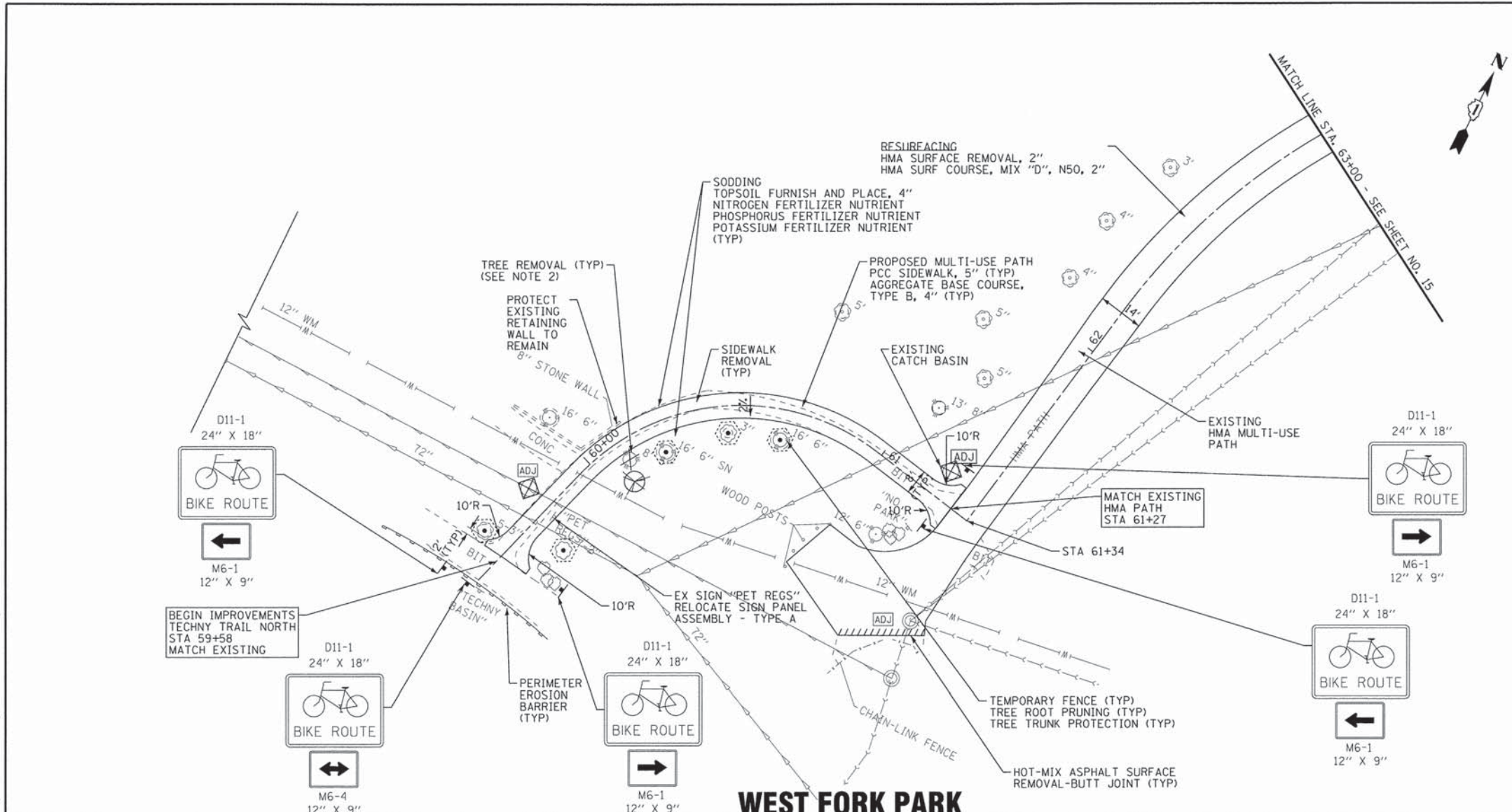


- NOTES:
- "TREE LOCATIONS SHOWN ON PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS OF TREES TO BE DETERMINED IN THE FIELD BY THE ENGINEER."
 - "TREE REMOVAL AND TREE TRANSPLANTING SHALL NOT COMMENCE UNTIL LOCATIONS ARE APPROVED IN THE FIELD BY THE ENGINEER."

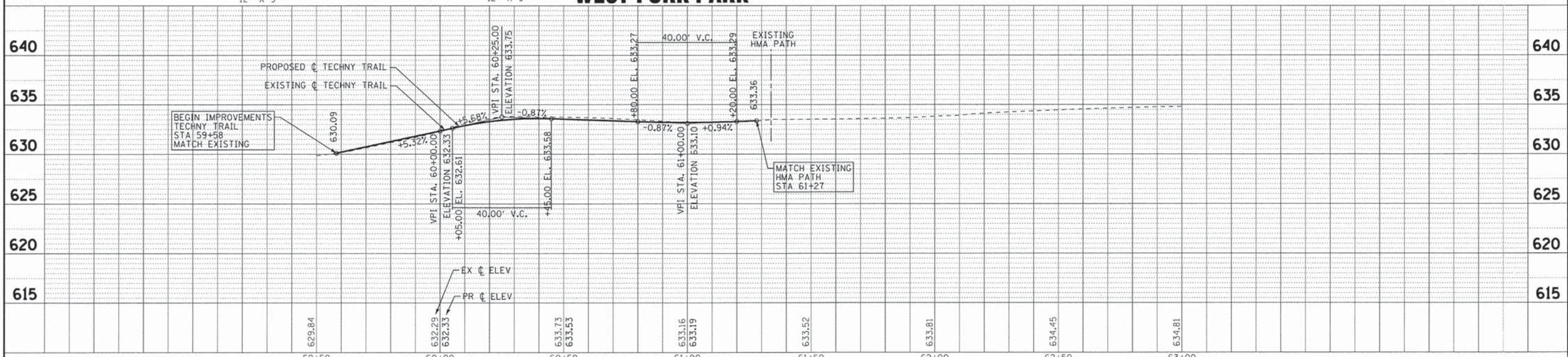


	DESIGNED - TLH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN AND PROFILE CLAIRE COURT		F.A.P. RTE. 305	SECTION 11-00178-00-BT	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 13
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	CHECKED - TLH	REVISED -				FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT TE-00118461		
	DATE - 08/06/14	FILE - 120011-25_PP4.sht				SCALE: H: 1"=20' V: 1"=5'		STA. 41+00 TO STA. 45+79		

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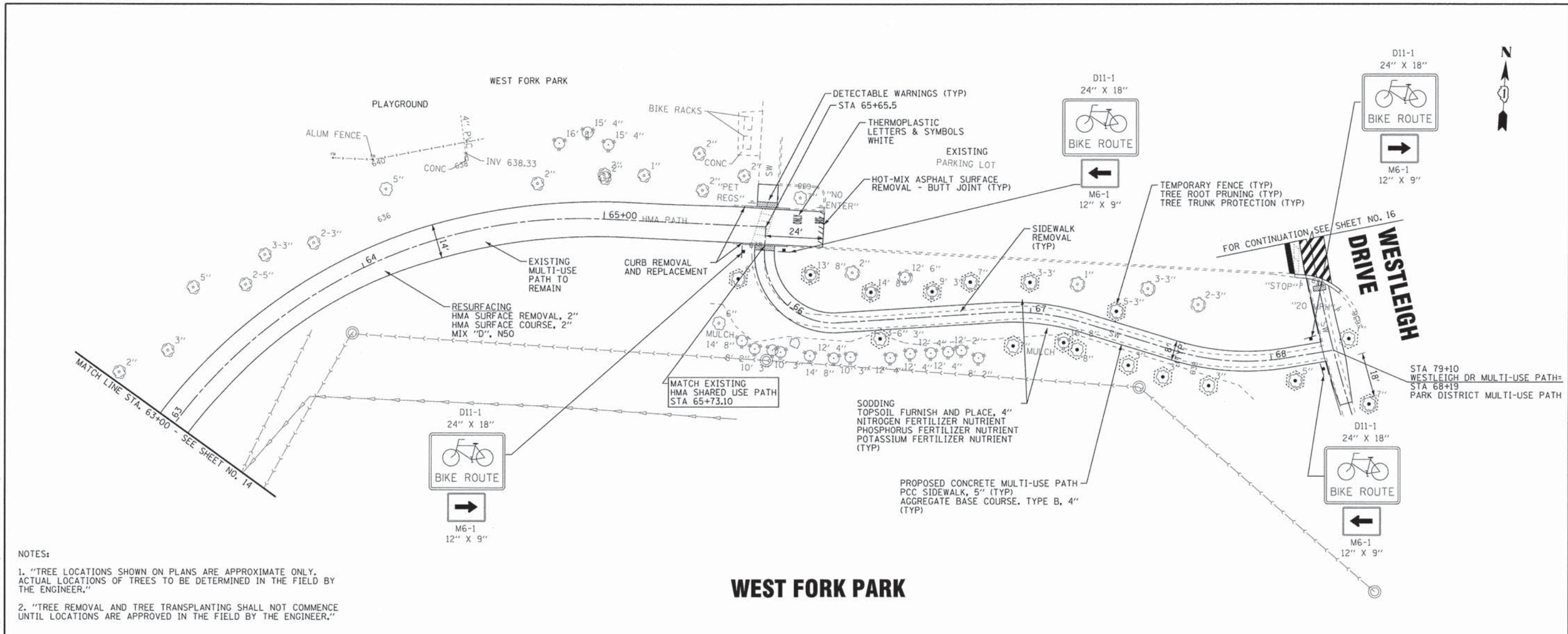


NOTES:
 1. "TREE LOCATIONS SHOWN ON PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS OF TREES TO BE DETERMINED IN THE FIELD BY THE ENGINEER."
 2. "TREE REMOVAL AND TREE TRANSPLANTING SHALL NOT COMMENCE UNTIL LOCATIONS ARE APPROVED IN THE FIELD BY THE ENGINEER."



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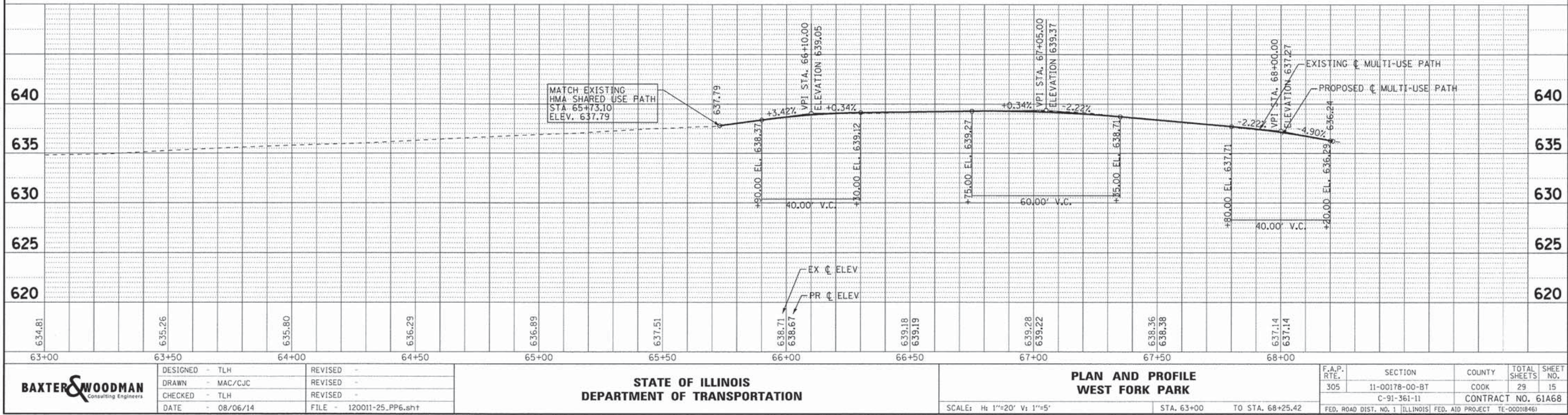
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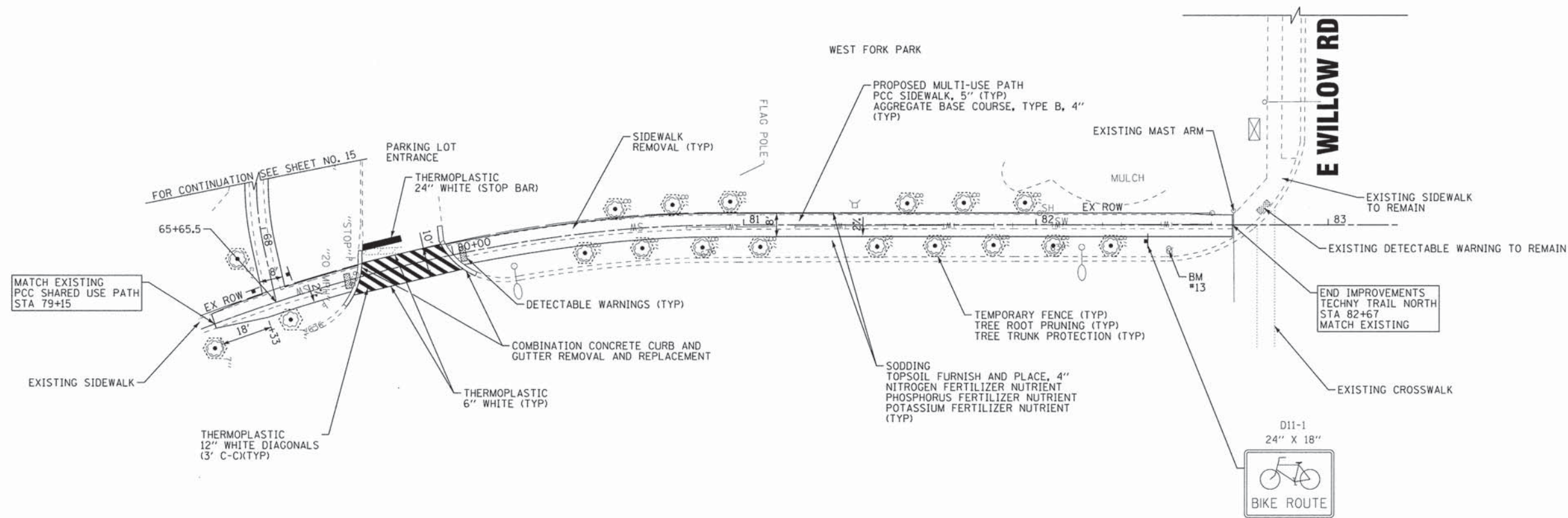
NOTES:

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- "TREE REMOVAL AND TREE TRANSPLANTING SHALL NOT COMMENCE UNTIL LOCATIONS ARE APPROVED IN THE FIELD BY THE ENGINEER."

WEST FORK PARK



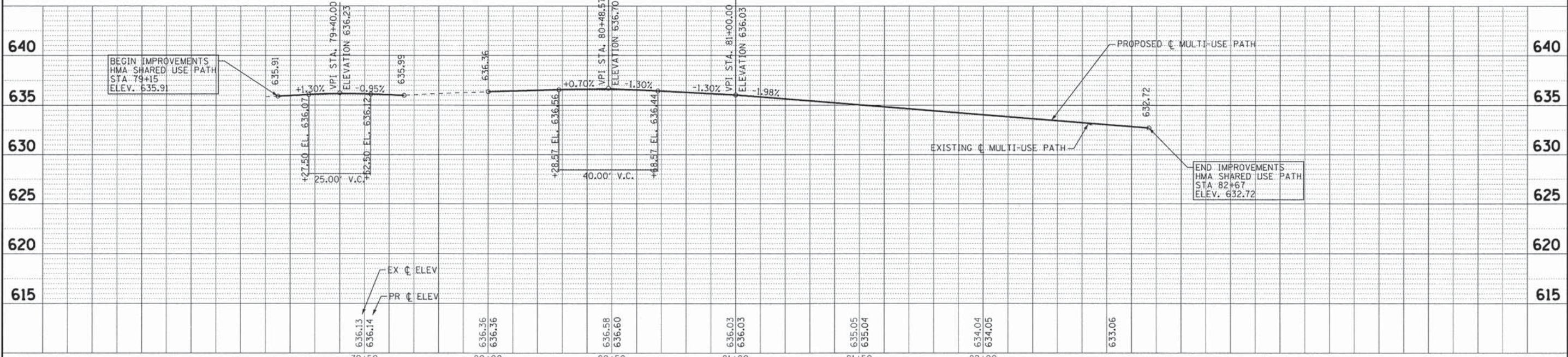
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NOTES:

1. "TREE LOCATIONS SHOWN ON PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS OF TREES TO BE DETERMINED IN THE FIELD BY THE ENGINEER."
2. "TREE REMOVAL AND TREE TRANSPLANTING SHALL NOT COMMENCE UNTIL LOCATIONS ARE APPROVED IN THE FIELD BY THE ENGINEER."

WESTLEIGH DRIVE



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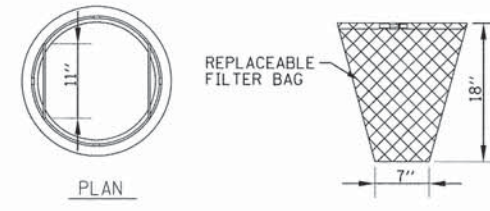
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DRAWN - MAC/CJC	REVISED -
CHECKED - TLH	REVISED -
DATE - 08/06/14	FILE - 120011-25_PP7.shd

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PLAN AND PROFILE
WESTLEIGH DRIVE**
SCALE: H: 1"=20' V: 1"=5'
STA. 79+15 TO STA. 82+58.43

F.A.P. RTE. 305	SECTION 11-00178-00-BT	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 16
C-91-361-11			CONTRACT NO. 61A68	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-00018461				

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 STATE OF ILLINOIS PROFESSIONAL ENGINEERING
 LICENSE NO. 184-00121 - EXPIRES 4/30/2015
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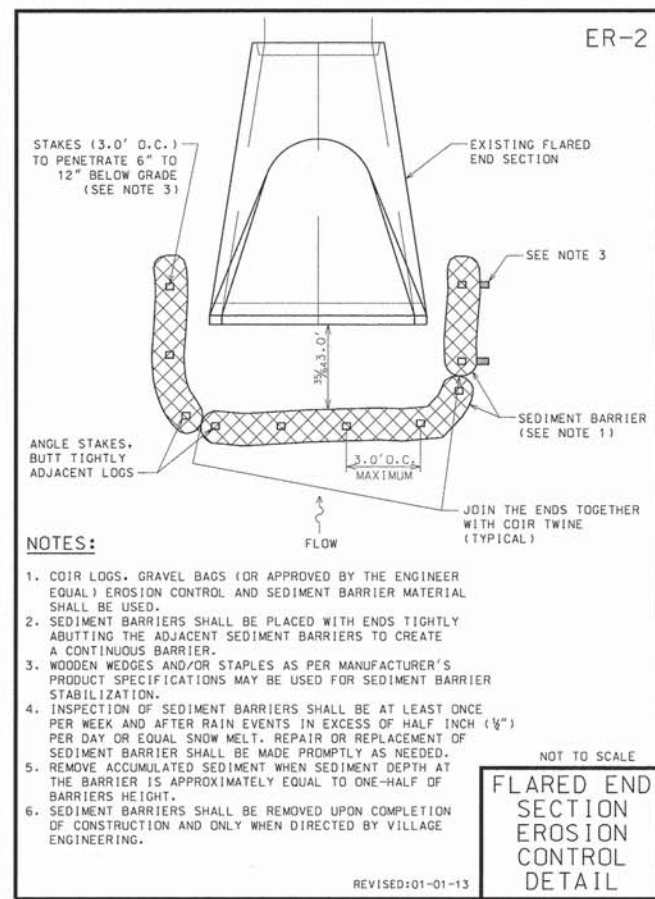


GENERAL NOTES:

FRAME: TOP RING CONSTRUCTED FROM 1 1/4" X 1 1/4" X 1/8" ANGLE. BASE RING CONSTRUCTED OF 1 1/2" X 1/2" X 1/8" CHANNEL. HANDLES & SUSPENSION BRACKETS CONSTRUCTED FROM 1/4" X 1 1/4" FLAT. ALL STEEL CONFORMING TO ASTM-A36.

REPLACEABLE BAG: CONSTRUCTED FROM 4 OZ./SQ. YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH. CONNECTED TO BASE RING WITH STAINLESS STEEL STRAP & LOCK.

INLET FILTER
NO SCALE

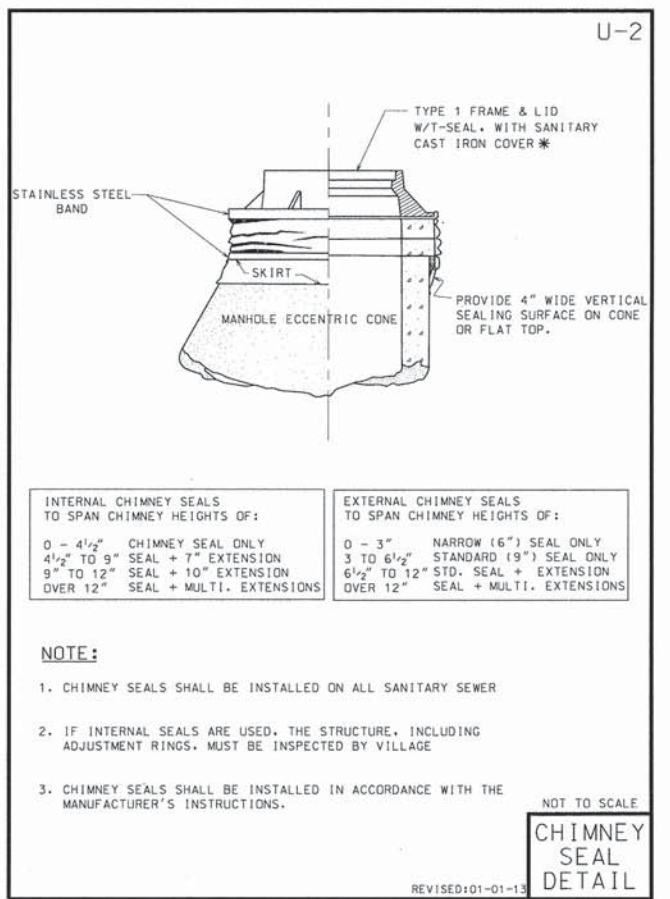


NOTES:

- COIR LOGS, GRAVEL BAGS (OR APPROVED BY THE ENGINEER EQUAL) EROSION CONTROL AND SEDIMENT BARRIER MATERIAL SHALL BE USED.
- SEDIMENT BARRIERS SHALL BE PLACED WITH ENDS TIGHTLY ABUTTING THE ADJACENT SEDIMENT BARRIERS TO CREATE A CONTINUOUS BARRIER.
- WOODEN WEDGES AND/OR STAPLES AS PER MANUFACTURER'S PRODUCT SPECIFICATIONS MAY BE USED FOR SEDIMENT BARRIER STABILIZATION.
- INSPECTION OF SEDIMENT BARRIERS SHALL BE AT LEAST ONCE PER WEEK AND AFTER RAIN EVENTS IN EXCESS OF HALF INCH (1/2") PER DAY OR EQUAL SNOW MELT. REPAIR OR REPLACEMENT OF SEDIMENT BARRIER SHALL BE MADE PROMPTLY AS NEEDED.
- REMOVE ACCUMULATED SEDIMENT WHEN SEDIMENT DEPTH AT THE BARRIER IS APPROXIMATELY EQUAL TO ONE-HALF OF BARRIERS HEIGHT.
- SEDIMENT BARRIERS SHALL BE REMOVED UPON COMPLETION OF CONSTRUCTION AND ONLY WHEN DIRECTED BY VILLAGE ENGINEERING.

NOT TO SCALE
FLARED END SECTION EROSION CONTROL DETAIL

REVISED:01-01-13



INTERNAL CHIMNEY SEALS TO SPAN CHIMNEY HEIGHTS OF:

0 - 4 1/2"	CHIMNEY SEAL ONLY
4 1/2" TO 9"	SEAL + 7" EXTENSION
9" TO 12"	SEAL + 10" EXTENSION
OVER 12"	SEAL + MULTI. EXTENSIONS

EXTERNAL CHIMNEY SEALS TO SPAN CHIMNEY HEIGHTS OF:

0 - 3"	NARROW (6") SEAL ONLY
3 TO 6 1/2"	STANDARD (9") SEAL ONLY
6 1/2" TO 12"	STD. SEAL + EXTENSION
OVER 12"	SEAL + MULTI. EXTENSIONS

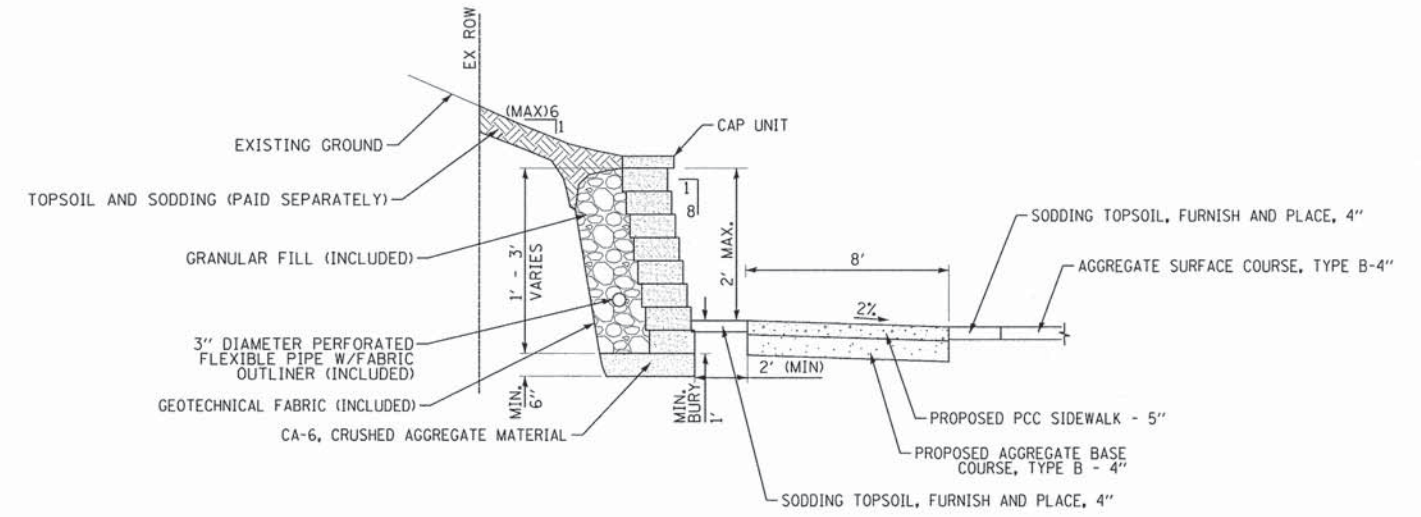
NOTE:

- CHIMNEY SEALS SHALL BE INSTALLED ON ALL SANITARY SEWER
- IF INTERNAL SEALS ARE USED, THE STRUCTURE, INCLUDING ADJUSTMENT RINGS, MUST BE INSPECTED BY VILLAGE
- CHIMNEY SEALS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.

NOT TO SCALE
CHIMNEY SEAL DETAIL

REVISED:01-01-13

*OR APPROVED EQUAL



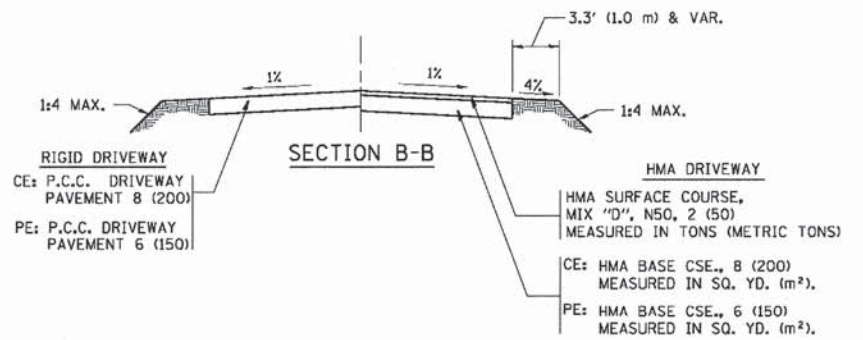
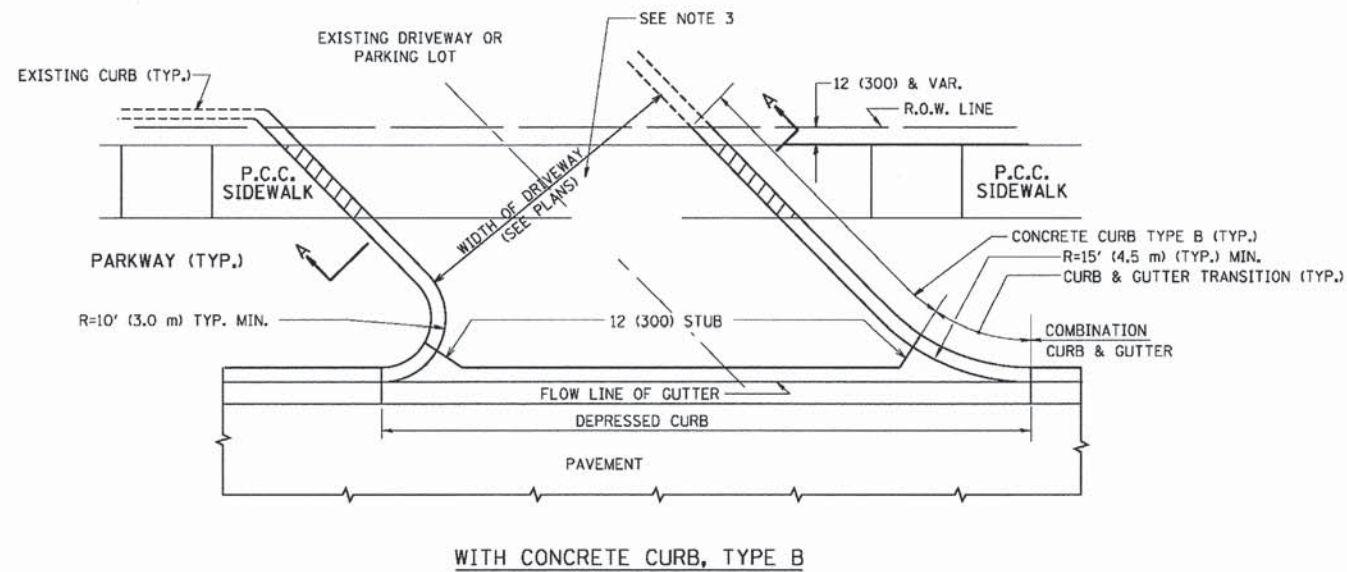
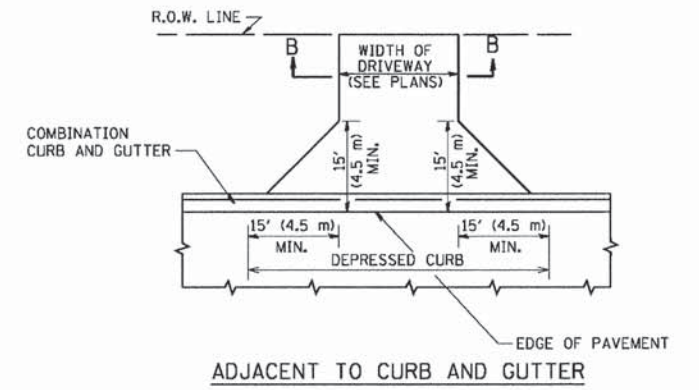
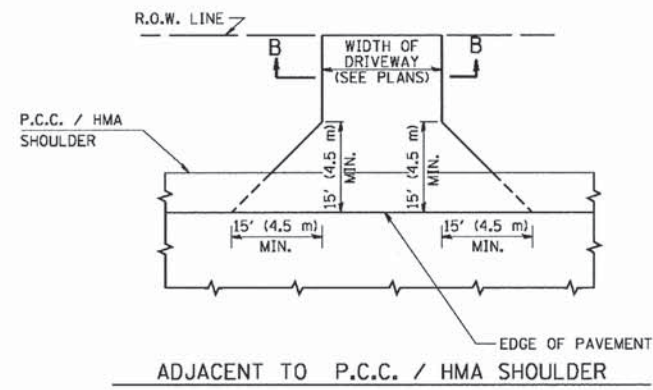
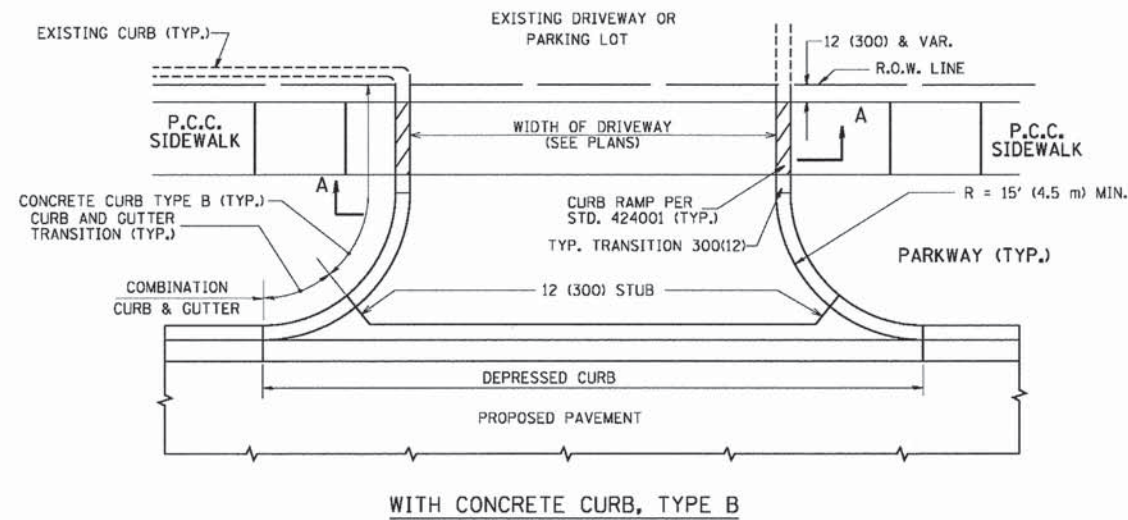
SEGMENTAL CONCRETE BLOCK WALL DETAIL
CLAIRE COURT
STA 42+47 TO STA 43+53 LT
NO SCALE

BAXTER & WOODMAN Consulting Engineers	DESIGNED - REW	REVISED -
	DRAWN - MAC/CJC	REVISED -
	CHECKED - TLH	REVISED -
	DATE - 08/06/14	FILE - 120011-25_Misc_Details.sht

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS	
SCALE:	STA. TO STA.

F.A.P. RTE. 305	SECTION 11-00178-00-BT	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 17
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001846				



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

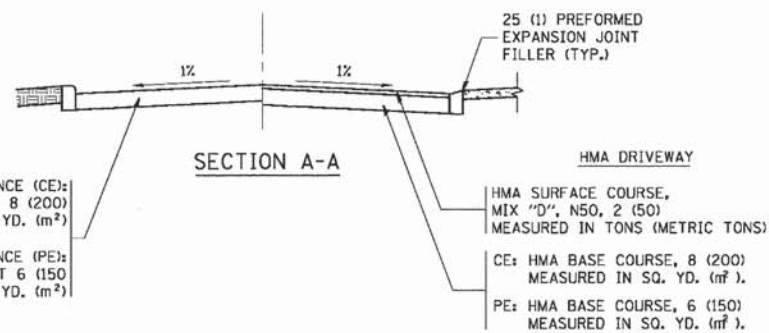
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



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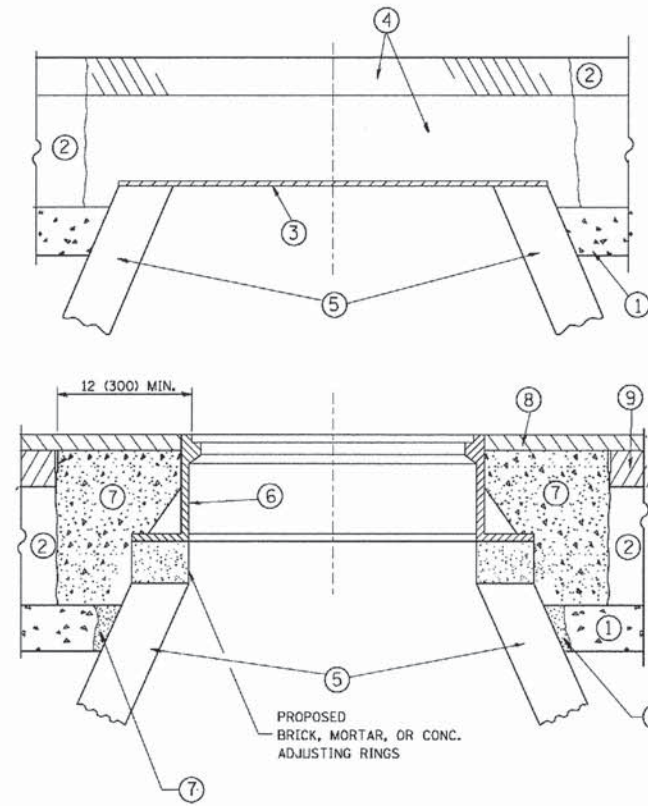
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		DRAWN -	REVISED -
			R. BORO 01-01-07
		CHECKED -	REVISED -
			R. BORO 06-11-08
		DATE -	REVISED -
		11-04-95	R. BORO 09-06-11

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
 AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)**
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	18
BD0156-07 (BD-01)			CONTRACT NO. 61A68	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001(846)				

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NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-1* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

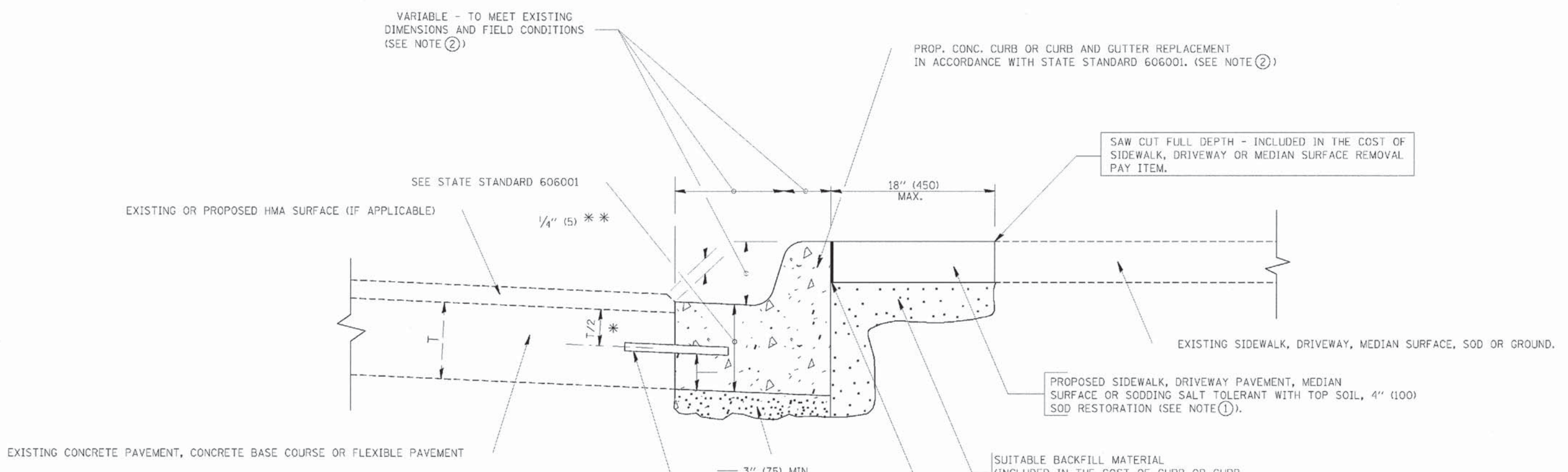
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	PLOT DATE = 12/6/2011	DATE = 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	19
BD600-03 (BD-8)			CONTRACT NO. 61A68	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001(846)				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

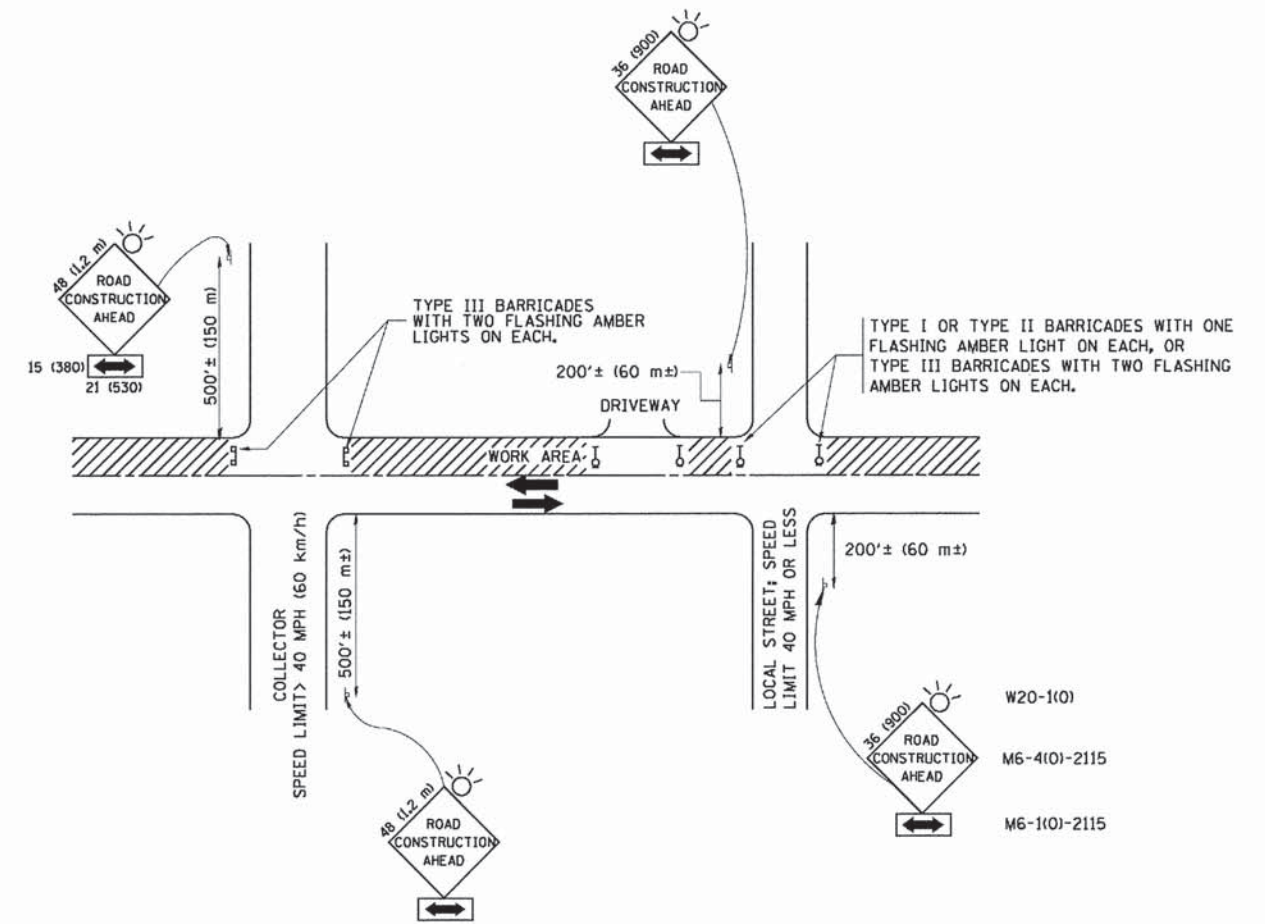
BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
						305	11-00178-00-BT	COOK	29	20	
						BD600-06 (BD-24)		CONTRACT NO. 61A68			
						FED. ROAD DIST. NO. 1 (ILLINOIS) FEB. AID PROJECT TE-0001846					
						SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.					



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

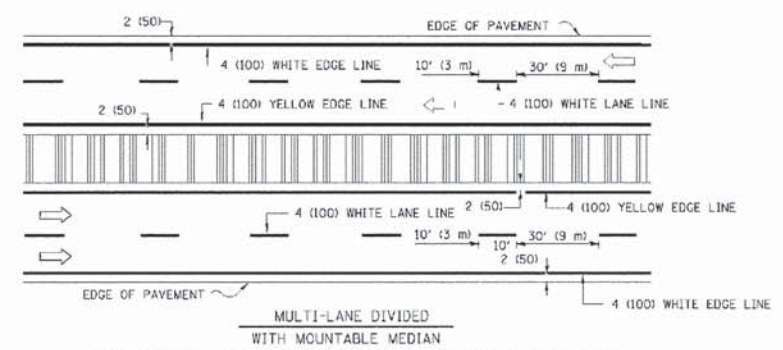
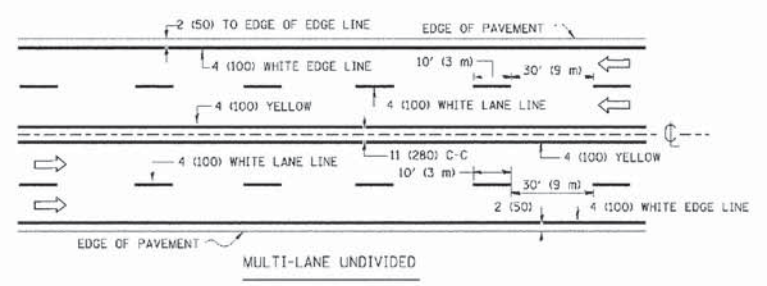
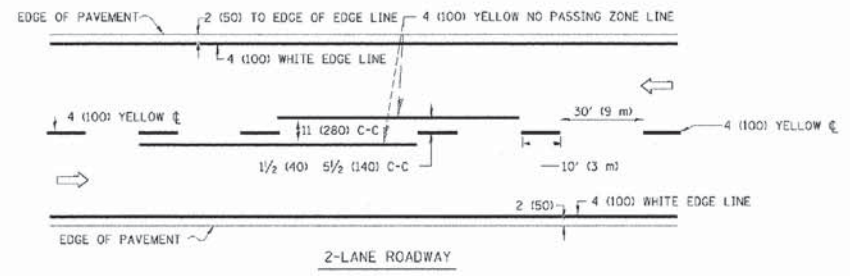
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 DRAWN -
 CHECKED -
 DATE - 06-89
 REVISED - J. OBERLE 10-18-95
 REVISED - A. HOUSEH 03-06-96
 REVISED - A. HOUSEH 10-15-96
 REVISED - T. RAMMACHER 01-06-00

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

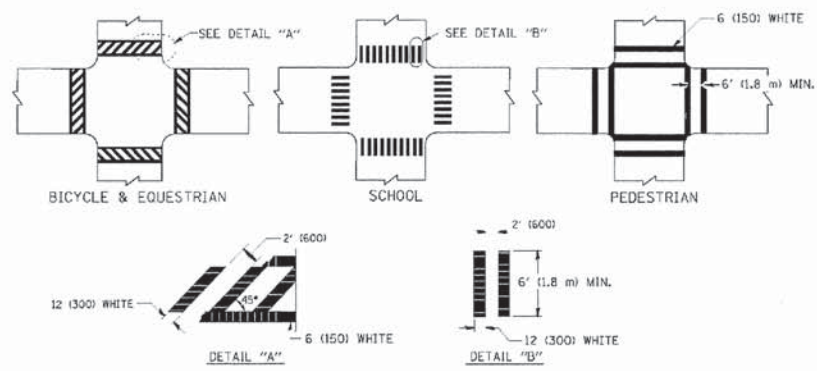
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	22
TC-10			CONTRACT NO. 61A68	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT E-0001(846)				

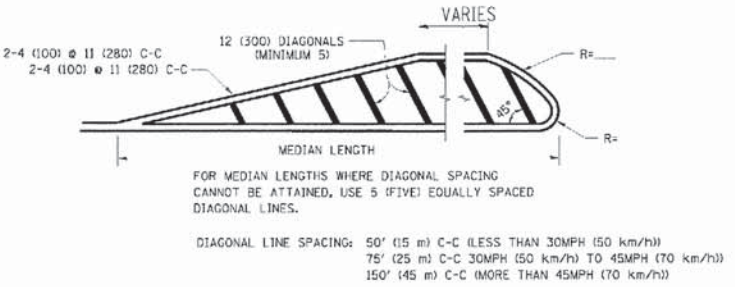
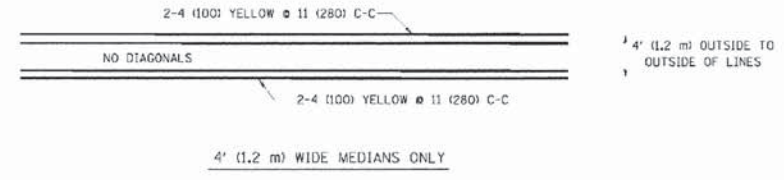


NOTE: MEDIANS WITH BARRIER CURBS DO NOT REQUIRE AN EDGE LINE

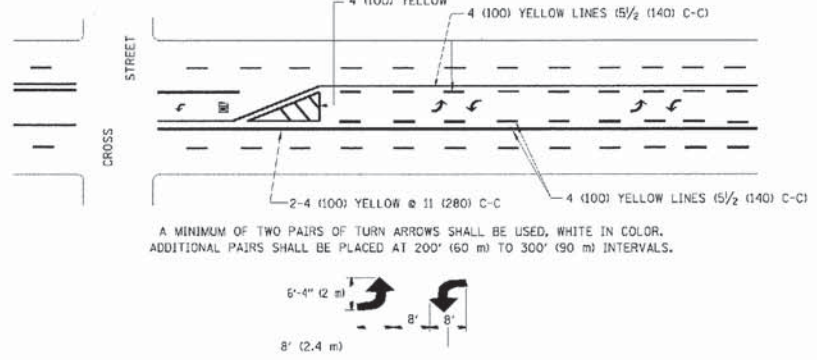
TYPICAL LANE AND EDGE LINE MARKING



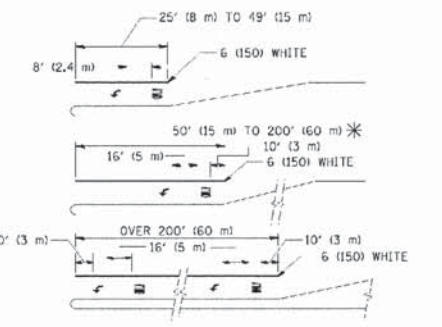
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



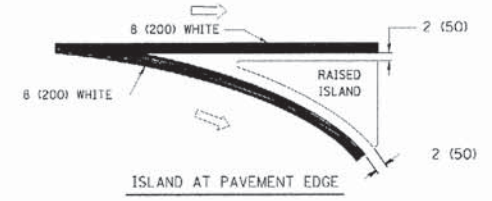
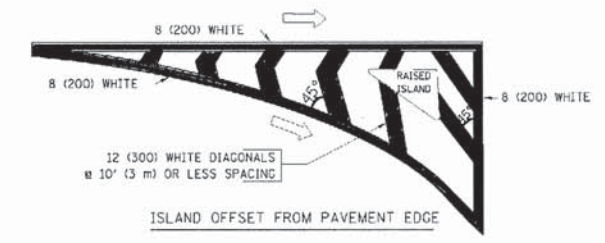
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
DORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (23 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 365.000
 ILLINOIS PROFESSIONAL ENGINEERING BOARD
 01/27/2004
 PROJECT NO. 11-00178-00-BT
 SHEET NO. 23
 DATE 03-19-90
 DESIGNED BY EVERS
 DRAWN BY C. JUCCUS
 CHECKED BY
 DATE 03-19-90
 REVISED BY T. RAMMACHER
 REVISED BY C. JUCCUS
 REVISED BY
 DATE 10-27-94
 DATE 09-09-09

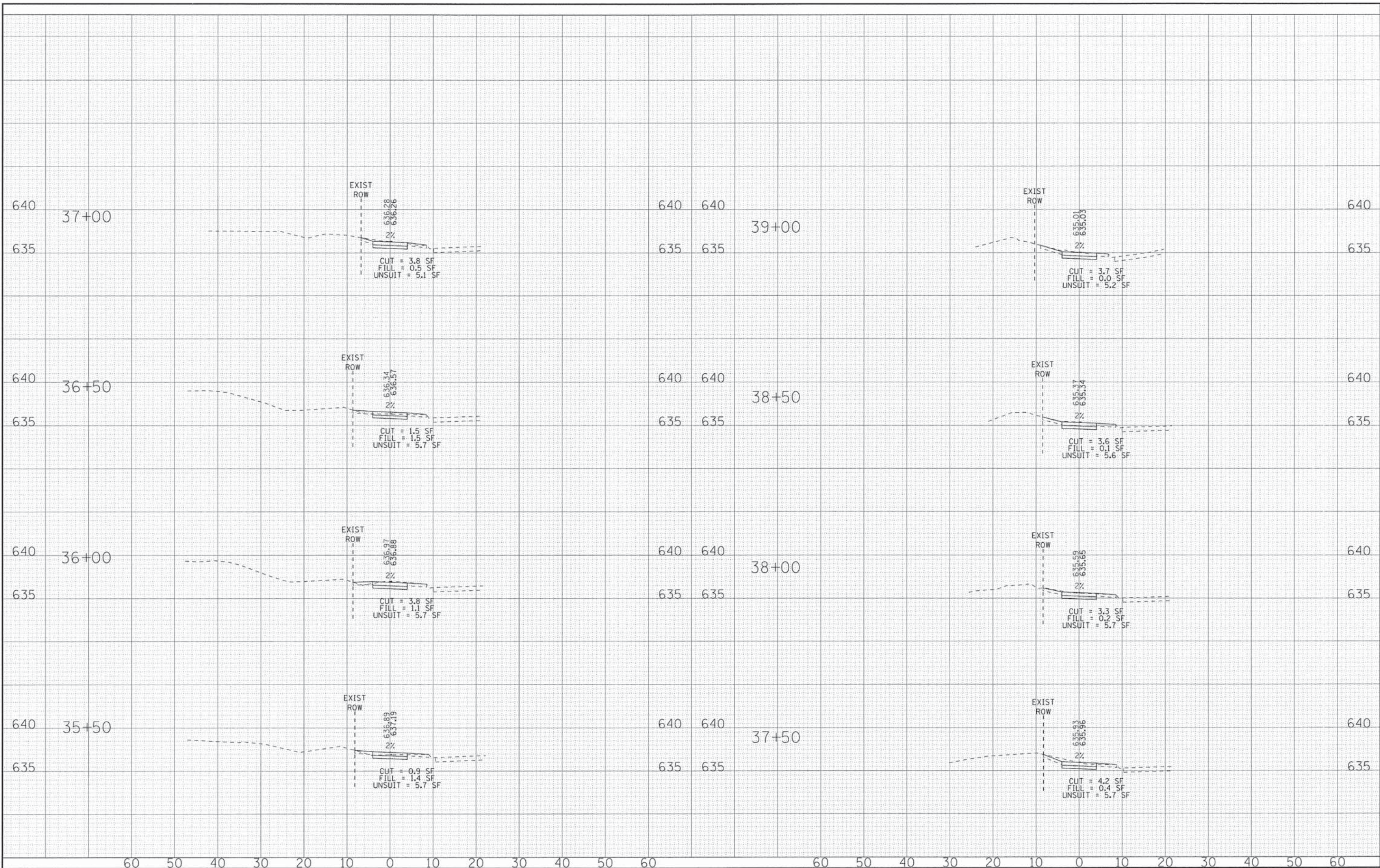
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FILE NAME =	DATE = 03-19-90	DRAWN - C. JUCCUS	REVISED - C. JUCCUS 09-09-09
PLOT SCALE = 50.000' / IN.	CHECKED -	DATE = 03-19-90	REVISED -
PLOT DATE = 03/19/94			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	23
TC-13		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-001(B46)				

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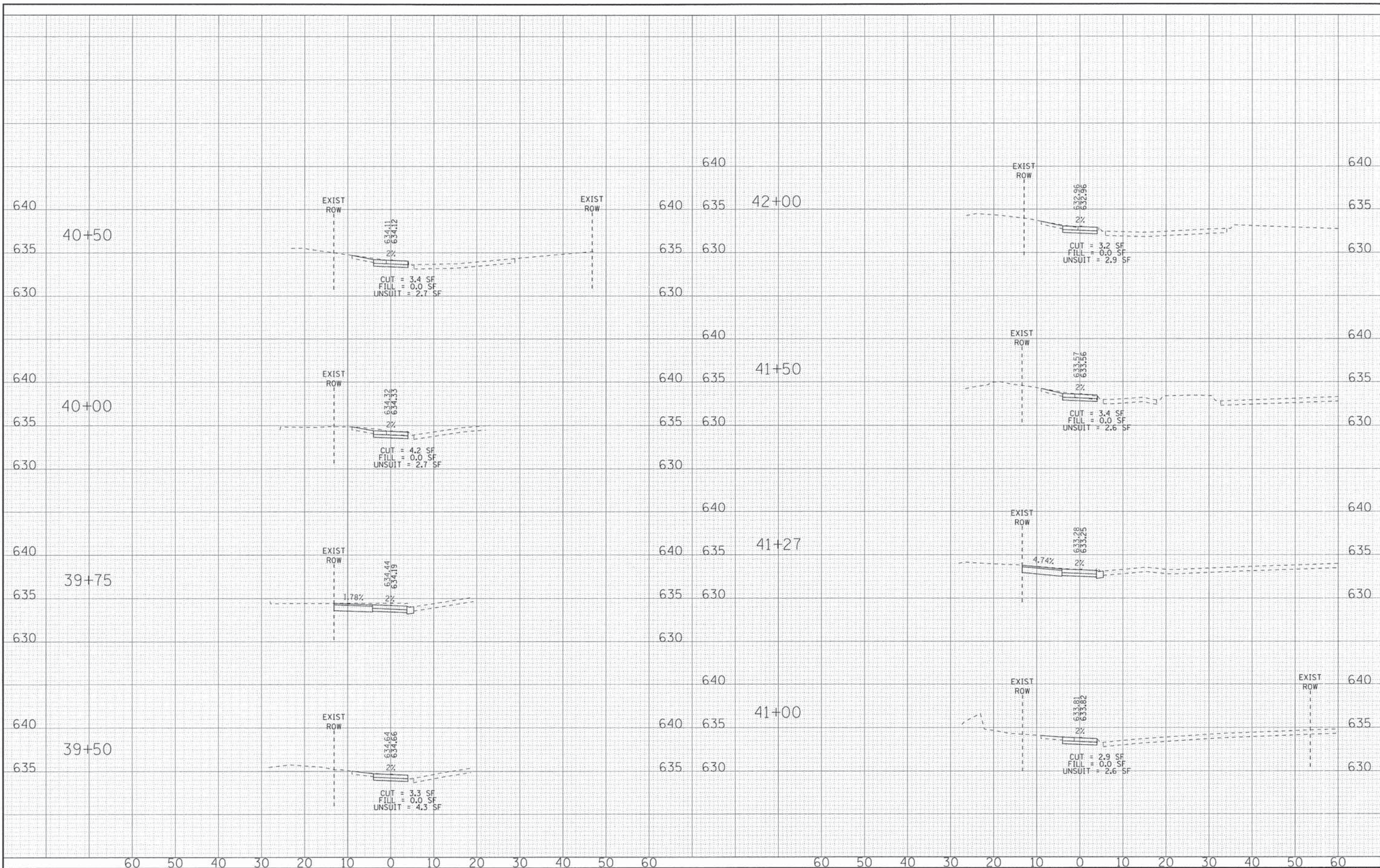
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DATE - 08/06/14	REVISED -

VILLAGE OF GLENVIEW, ILLINOIS
TECHNY TRAIL SEGMENT 4
OLD WILLOW RD/RAVINE WAY TO WILLOW RD (FAP 305)

CROSS SECTIONS
CLAIRE COURT
 SCALE: H: 1"=10' V: 1"=5'
 STA. 35+50 TO STA. 39+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	25
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-0001 18461				

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 LICENSE NO. 181-020123-0000
 PROJECT NO. 11-00178-00-BT
 SHEET NO. 29



BAXTER & WOODMAN
Consulting Engineers

DESIGNED - REW	REVISED -
DRAWN - MAC	REVISED -
CHECKED - TLH	REVISED -
DATE - 08/06/14	REVISED -

VILLAGE OF GLENVIEW, ILLINOIS
TECHNY TRAIL SEGMENT 4
OLD WILLOW RD/RAVINE WAY TO WILLOW RD (FAP 305)

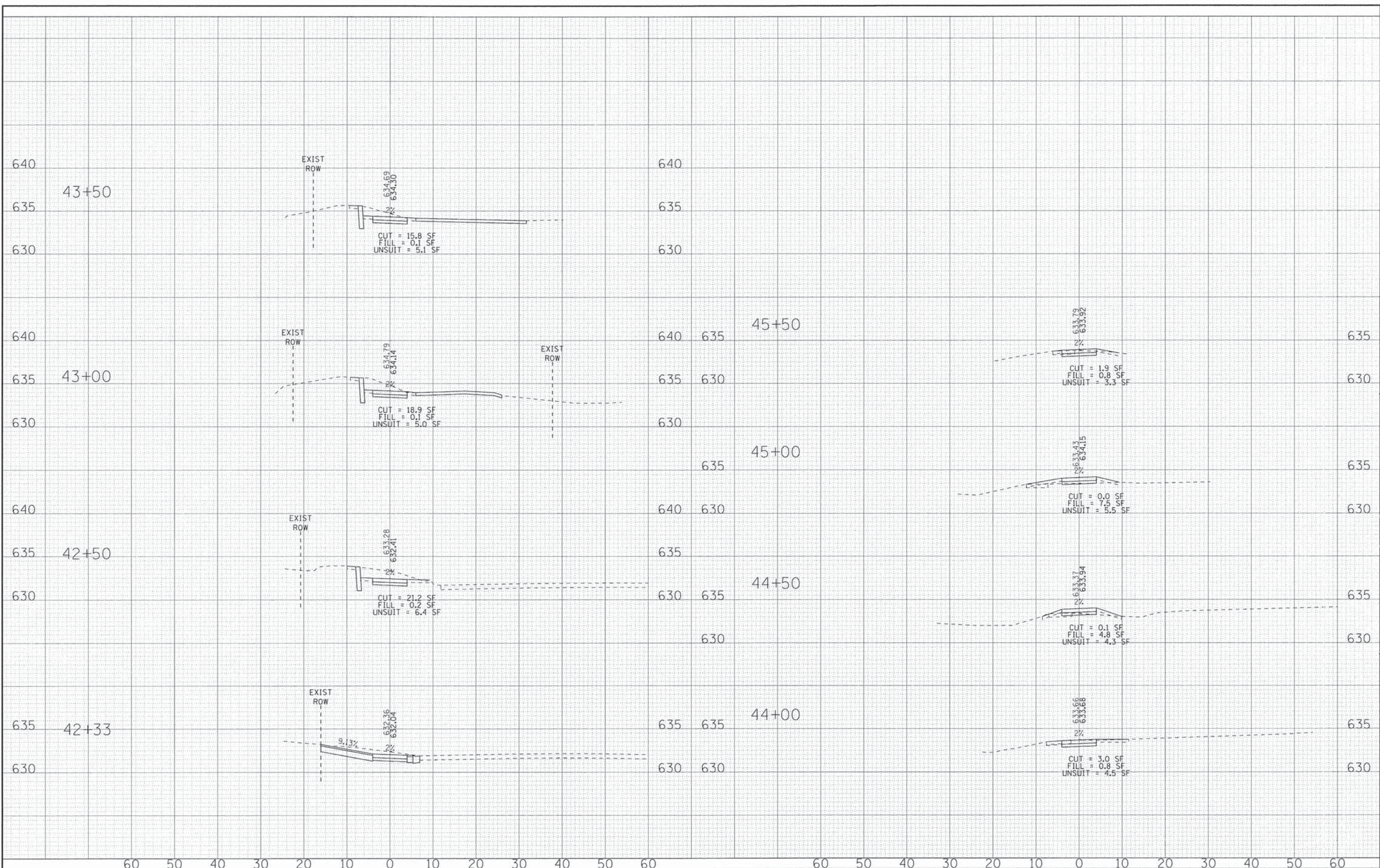
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CROSS SECTIONS
CLAIRE COURT

STA. 39+50 TO STA. 42+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	26
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-001 (846)				

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 BAXTER & WOODMAN, INC. CONSULTING ENGINEERS
 216 S. WASHINGTON ST., SUITE 200, GLENVIEW, IL 60025
 TEL: 847.439.2000 FAX: 847.439.2001



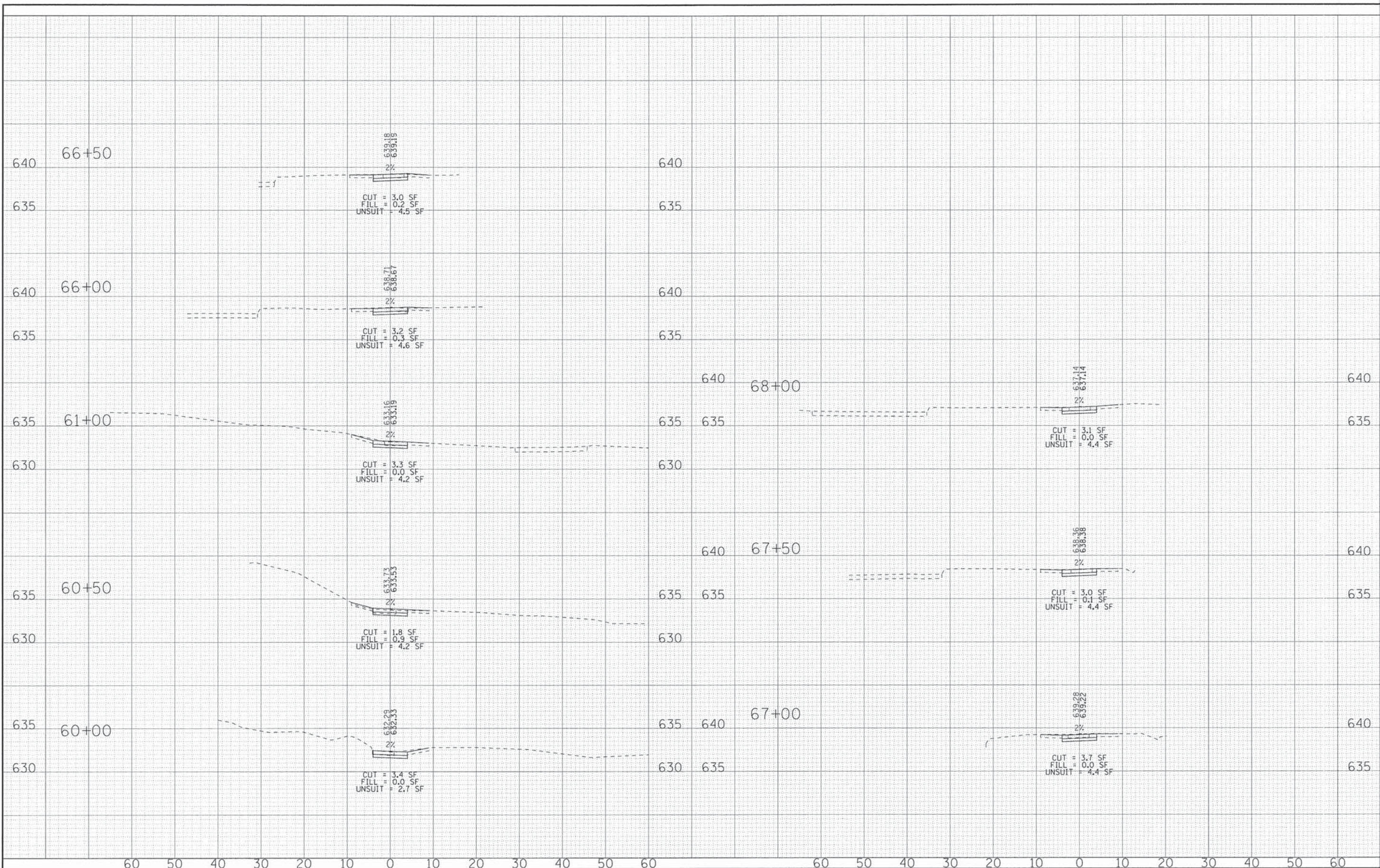
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DRAWN - MAC	REVISED -
CHECKED - TLH	REVISED -
DATE - 08/06/14	REVISED -

VILLAGE OF GLENVIEW, ILLINOIS
TECHNY TRAIL SEGMENT 4
OLD WILLOW RD/RAVINE WAY TO WILLOW RD (FAP 305)

CROSS SECTIONS
CLAIRE COURT
 SCALE: H: 1"=10' V: 1"=5'
 STA. 42+33 TO STA. 45+50

F.A.P. RTE. 305	SECTION 11-00178-00-BT	COUNTY COOK	TOTAL SHEETS 29	SHEET NO. 27
C-91-361-11		CONTRACT NO. 61A68		TE-0001 (846)

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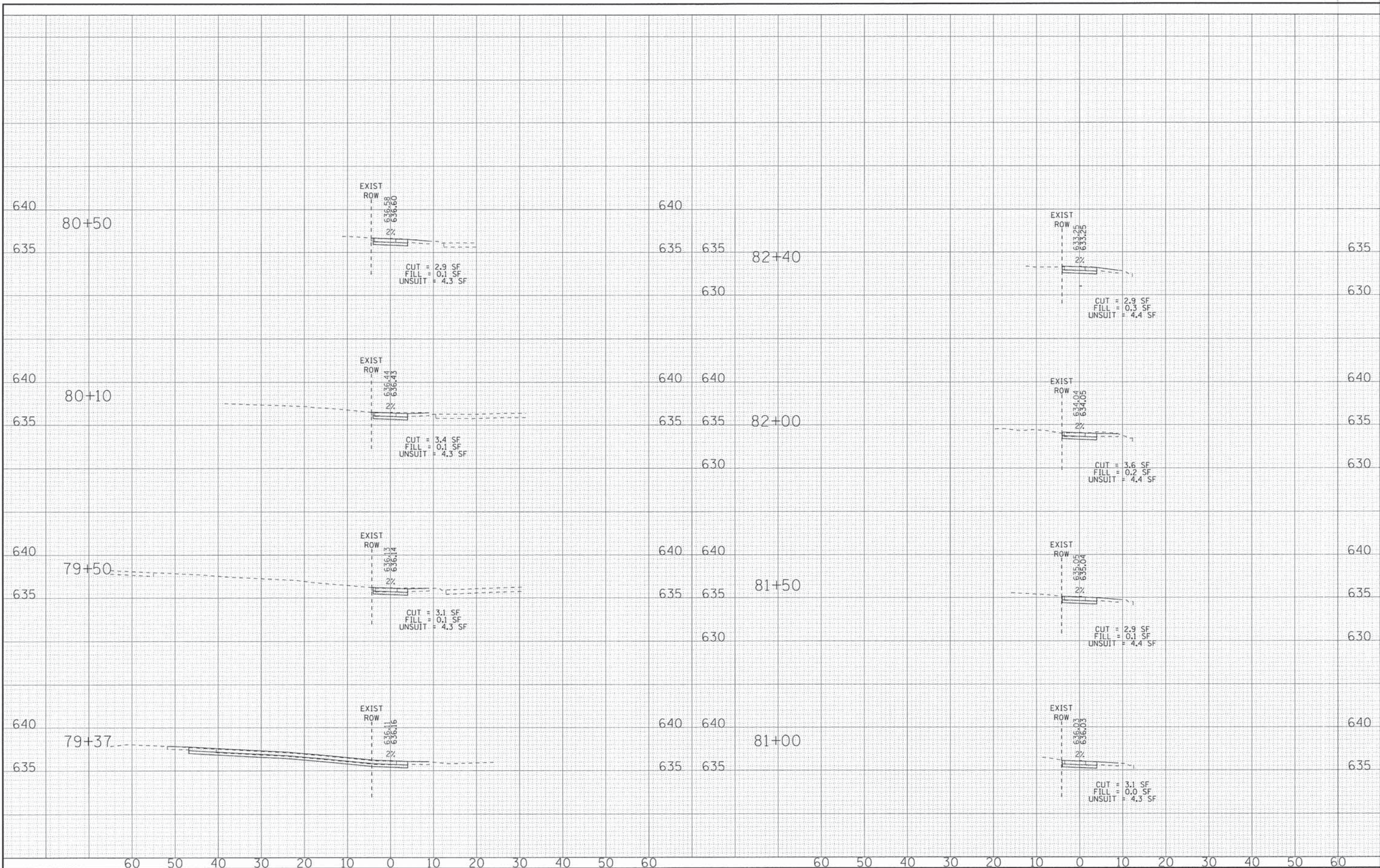
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DRAWN - MAC	REVISED -
CHECKED - TLH	REVISED -
DATE - 08/06/14	REVISED -

VILLAGE OF GLENVIEW, ILLINOIS
TECHNY TRAIL SEGMENT 4
OLD WILLOW RD/RAVINE WAY TO WILLOW RD (FAP 305)

CROSS SECTIONS
WEST FORK PARK
 SCALE: H: 1"=10' V: 1"=5'
 STA. 60+00 TO STA. 68+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	28
	C-91-361-11		CONTRACT NO. 61A68	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT TE-0001 (846)				

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CHECKED - TLH	REVISED -
DATE - 08/06/14	REVISED -

VILLAGE OF GLENVIEW, ILLINOIS
TECHNY TRAIL SEGMENT 4
OLD WILLOW RD/RAVINE WAY TO WILLOW RD (FAP 305)

CROSS SECTIONS
WESTLEIGH DRIVE
 SCALE: H: 1"=10' V: 1"=5'
 STA. 79+37 TO STA. 82+40

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	11-00178-00-BT	COOK	29	29
C-91-361-11		CONTRACT NO. 61A68		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT TE-001 (846)				