

PROJECT LOCATED IN
UNITED CITY OF
YORKVILLE

11-21-14 LETTING ITEM 137

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
F.A.U. 1550**

(GAME FARM ROAD – SOMONAUK STREET)

FROM U.S. ROUTE 34 (VETERANS PARKWAY)
TO 275 FEET WEST OF IL ROUTE 47 (BRIDGE STREET)

SECTION NO.: 03-00031-00-FP

PROJECT NO.: M-8003(810)

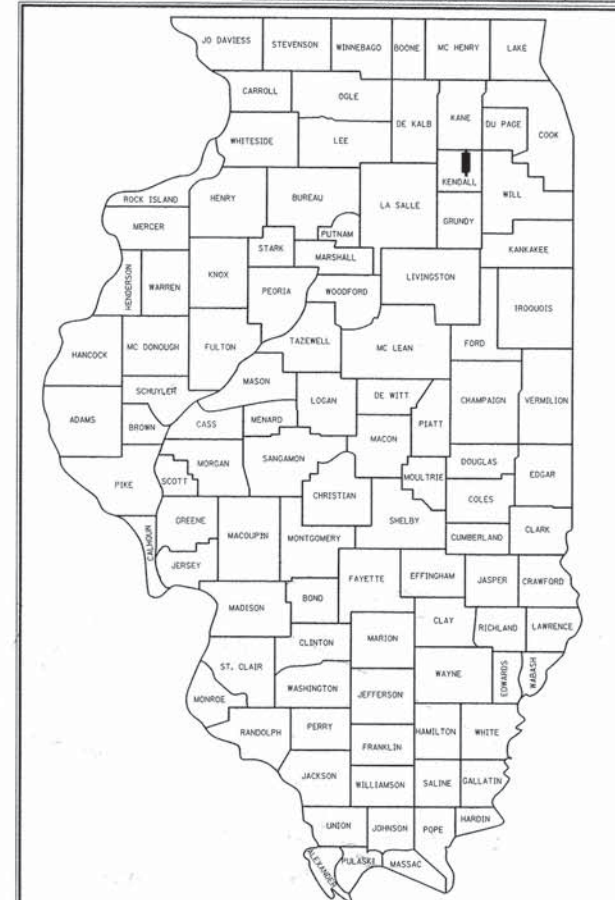
JOB NO. C-93-055-07

RESURFACING, WIDENING, AND RECONSTRUCTION

KENDALL COUNTY, ILLINOIS

UNITED CITY OF YORKVILLE

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|---------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 1 |



LOCATION OF SECTION INDICATED THIS: - [Black bar]

FOR INDEX OF SHEETS, SEE SHEET NO. 2

TRAFFIC DATA:

DESIGN DESIGNATION: MINOR COLLECTOR

ADT: 4,260 VEHICLES PER DAY (2008)
6,390 VEHICLES PER DAY (2018)
PV = 97.5% SU = 2.0% MU = 0.5%

DESIGN SPEED: 30 MPH
POSTED SPEED: 30 MPH
DESIGN VEHICLE: BUS-40

POLICY STATEMENT:
PLANS DESIGNED IN ACCORDANCE WITH THE BUREAU
OF LOCAL ROADS AND STREETS MANUAL.

AGENCY RESPONSIBLE FOR LETTING

APPROVED August 21 20 14
Mary J. Adams
UNITED CITY OF YORKVILLE MAYOR

PASSED 9/5 20 14
David R. E...
DISTRICT 3 LOCAL ROADS AND STREETS ENGINEER

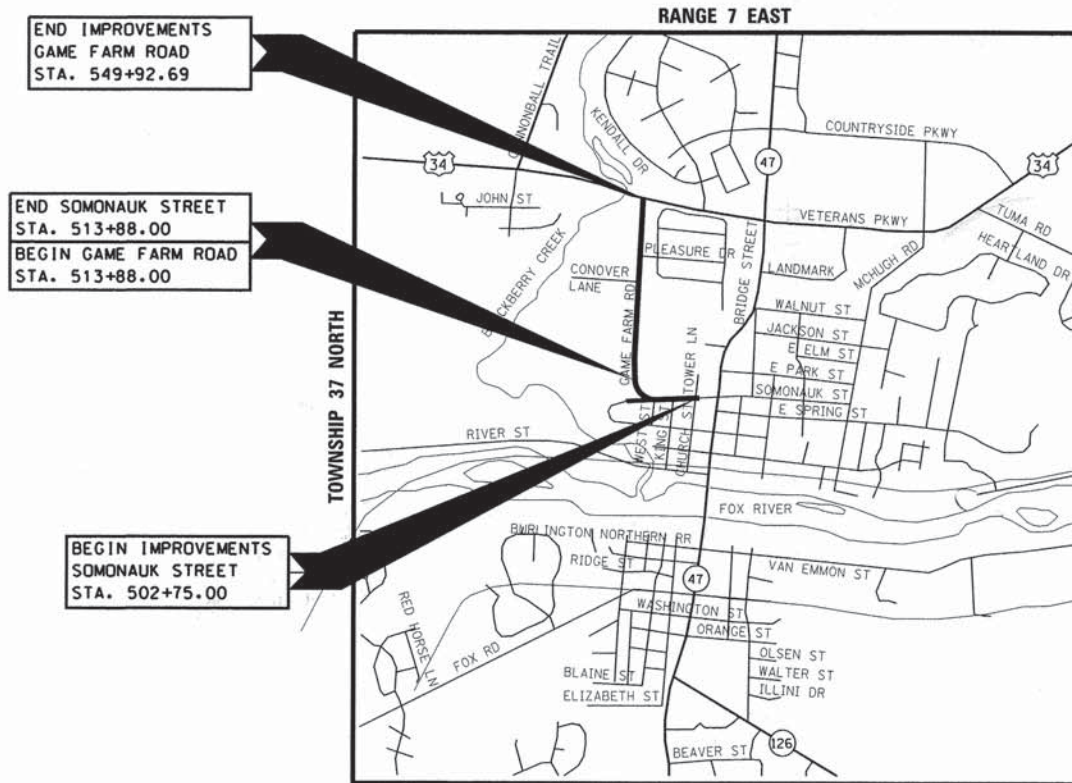
RELEASING FOR BID
BASED ON LIMITED
REVIEW 9/5 20 14
Paul A. L...
DEPUTY DIRECTOR OF HIGHWAYS, REGION 2 ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

Glenn Tredinnick
GLENN TREDINNICK, P.E.
EXPIRES: NOVEMBER 30, 2015



651 PRAIRIE POINTE, SUITE 201 | YORKVILLE, ILLINOIS 60560
Phone: 630.553.7560 | Toll Free: 800.728.7805 | Fax: 630.553.7646 | HRGreen.com
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

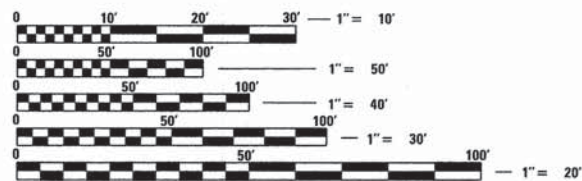


BRISTOL TOWNSHIP
THIRD PRINCIPAL MERIDIAN
LOCATION MAP
1" = 2,000' (APPROX.)

PROJECT GROSS LENGTH = 4,717.69 FEET (0.89 MILE)
PROJECT NET LENGTH = 4,717.69 FEET (0.89 MILE)

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OF THE STATE OF ILLINOIS

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 (CALL 48 HOURS IN ADVANCE)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CONTRACT NO: 87345

| | | | | |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 2 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 87345

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IDOT HIGHWAY STANDARDS

| | | | |
|-----------|---|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS | 701011-04 | OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS | 701101-04 | OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE |
| 420701-02 | PAVEMENT FABRIC | 701106-02 | OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' AWAY |
| 424001-07 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS | 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 424016-01 | MID-BLOCK CURB RAMPS FOR SIDEWALKS | 701427-02 | LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEED ≤ 40 MPH |
| 424011-01 | CORNER PARALLEL CURB RAMPS FOR SIDEWALKS | 701502-06 | URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE |
| 424016-01 | MID-BLOCK CURB RAMPS FOR SIDEWALKS | 701701-09 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 424021-02 | DEPRESSED CORNER FOR SIDEWALKS | 701801-05 | LANE CLOSURE, MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE |
| 424026-01 | ENTRANCE / ALLEY PEDESTRIAN CROSSINGS | 701901-03 | TRAFFIC CONTROL DEVICES |
| 542301-03 | PRECAST REINFORCED CONCRETE FLARED END SECTION | 720001-01 | SIGN PANEL MOUNTING DETAILS |
| 542311-05 | TRANSVERSABLE PIPE GRATE | 720006-04 | SIGN PANEL ERECTION DETAILS |
| 601001-04 | SUB-SURFACE DRAINS | 729001-01 | APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS) |
| 602001-02 | CATCH BASIN, TYPE A | 780001-04 | TYPICAL PAVEMENT MARKINGS |
| 602011-02 | CATCH BASIN, TYPE C | 781001-03 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKINGS |
| 602301-04 | INLET, TYPE A | 857001-01 | STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES |
| 602306-03 | INLET, TYPE B | 873001-02 | TRAFFIC SIGNAL GROUNDING & BONDING |
| 602401-03 | MANHOLE, TYPE A | 878001-09 | CONCRETE FOUNDATION DETAILS |
| 602406-06 | MANHOLE, TYPE A, 6' DIAMETER | 880006-1 | TRAFFIC SIGNAL MOUNTING DETAILS |
| 602501-02 | VALVE VAULT, TYPE A | 886001-01 | DETECTOR LOOP INSTALLATIONS |
| 602701-02 | MANHOLE STEPS | 886006-01 | TYPICAL LAYOUT FOR DETECTION LOOPS |
| 604001-03 | FRAME AND LIDS, TYPE 1 | | |
| 604036-02 | GRATE, TYPE B | | |
| 604051-03 | FRAME AND GRATE, TYPE 11 | | |
| 604091-02 | FRAME AND GRATE, TYPE 24 | | |
| 606001-05 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER | | |
| 664001-02 | CHAIN LINK FENCE | | |
| 701001-02 | OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY | | |
| 701006-05 | OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE | | |

UTILITY CONTACTS

CITY ENGINEER:
UNITED CITY OF YORKVILLE
BRAD SANDERSON
52 WHEELER ROAD
SUGAR GROVE, IL 60554
(630) 466-6700

GAS:
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AURORA, IL 60506
(630) 844-2040

ELECTRIC:
COMMONWEALTH EDISON
2001 AUCUTT ROAD
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(815) 334-3244

SANITARY:
YORKVILLE-BRISTOL SANITARY DISTRICT
KEVIN COLLMAN - EXECUTIVE DIRECTOR
304 RIVER STREET
YORKVILLE, IL 60560
(630) 553-7657

TELEPHONE:
AMERITECH ENGINEERING
65 WEST WEBSTER - FLOOR 3
JOLIET, IL 60432
(815) 727-8196

CABLE:
COMCAST
1303 MARQUETTE ROAD
ROMEDEVILLE, IL 60441
(815) 226-3820

STATE:
DIVISION OF HIGHWAYS
AMY REED
700 EAST NORRIS DRIVE
OTTAWA, IL 6135
(815) 434-6131

| | | |
|-----------|--------------------|------|
| PLAN | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | ALIGNMENT CHECKED | |
| | RT. OF WAY CHECKED | |
| | CADD FILE NAME | |

| | | |
|-----------|--------------------------|------|
| PROFILE | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | GRADES CHECKED | |
| | S.M. NOTED | |
| | STRUCTURE NOTATIONS CHKD | |

COMPANY NAME:
8/27/2014 4:00:08 PM
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
INDEX OF SHEETS /
LIST OF HIGHWAY STANDARDS /
UTILITY CONTACTS
SCALE: VERT. _____ DRAWN BY: MLS
HORIZ. _____ DATE: JULY 2014 CHECKED BY: GT

GENERAL NOTES

- 10 FT TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
- WHERE ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE.
- THE REMOVAL OF EXISTING ENTRANCE CULVERTS SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
- WATER MAIN QUALITY STORM SEWER IS TO BE USED AT LOCATIONS WHERE LATERAL SEPARATION BETWEEN THE SEWER AND WATER MAIN IS LESS THAN 10 FT AND THE WATERMAIN INVERT IS LESS THAN 1.5 FT ABOVE THE STORM SEWER CROWN.
- STORM SEWER, RUBBER GASKET IS TO BE USED AT LOCATIONS WHERE THE WATER MAIN CROSSES BELOW THE SEWER, REGARDLESS OF VERTICAL SEPARATION OR WHERE THE BOTTOM OF THE WATER MAIN IS LESS THEN 1.5 FT ABOVE THE TOP OF THE SEWER.
- THE CONTRACTOR SHALL CALL JULIE AT 800-892-0123, BRAD SANDERSON OF THE UNITED CITY OF YORKVILLE AT 630-466-6700, AND THE CONSTRUCTION ENGINEER AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION FOR FIELD LOCATIONS OF EXISTING UTILITIES. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE UNITED CITY OF YORKVILLE.
- ALL UTILITIES WILL BE MAINTAINED DURING CONSTRUCTION. HAND EXCAVATION IS REQUIRED NEAR THESE FACILITIES TO AVOID DAMAGE.
- SAW CUTTING WILL NOT BE PAID FOR SEPARATELY BUT IS INCLUDED IN THE COST OF THE ITEM BEING REMOVED.
- WATERMAIN FITTINGS WILL BE PAID PER POUND. SEE WEIGHT TABLE SUMMARY IN THE PLANS AND SPECIAL PROVISIONS
- THE HMA SURFACE OF ALL MAILBOX TURNOUTS, PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, AND SIDE ROADS SHALL BE MADE NEATLY, IN A WORKMANLIKE MANNER, AND SHALL ACCURATELY CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. IF REQUIRED BY THE ENGINEER, THE CONTRACTOR SHALL SAW CUT THE HMA SURFACE TO CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. THIS WORK WILL BE INCLUDED IN THE COST OF THE HMA SURFACE.
- EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.
- THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.
- SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.
- FOR STABILIZATION, ALL TYPE III BARRICADES WILL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.
- DETECTABLE WARNING SURFACES SHALL EXTEND 2.0 FT MINIMUM IN THE DIRECTION OF PEDESTRIAN TRAVEL. AT CURB RAMPS AND BLENDED TRANSITIONS, DETECTABLE WARNING SURFACES SHALL EXTEND THE FULL WIDTH OF THE RAMP RUN (EXCLUDING ANY FLARES) OR BLENDED TRANSITIONS. SOME DETECTABLE WARNING PRODUCTS REQUIRE A CONCRETE BORDER FOR PROPER INSTALLATION. THE CONCRETE BORDER SHALL NOT EXCEED 2 INCHES.
- ALL ELEVATIONS REFERRING TO U.S.G.S. MEAN SEA LEVEL DATUM.
- ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

COMMITMENTS

- GAME FARM ROAD AND SOMONAUK STREET WILL BE SIGNED FOR "NO THROUGH TRAFFIC" DURING CONSTRUCTION.
- PARKING RESTRICTIONS WILL BE IN PLACE ALONG THE ENTIRETY OF GAME FARM ROAD. THE SAME PARKING RESTRICTIONS WILL ALSO BE PLACED ALONG SOMONAUK STREET EXCEPT IN THE LOCATION OF THE BUS TURNOUT WHERE SCHOOL BUSES MAY PARK.
- LOCAL TRAFFIC WILL BE MAINTAINED AT ALL TIMES WHILE THROUGH TRAFFIC WILL BE DETOURED ALONG IL ROUTE 47 AND U.S. ROUTE 34.
- THE MIDDLE SECTION IN FRONT OF THE HIGH SCHOOL AND MIDDLE SCHOOL WILL BE CONSTRUCTED DURING THE SUMMER MONTHS WHEN SCHOOL TRAFFIC WILL BE AT A MINIMUM. ACCESS TO ALL SCHOOLS SHALL BE COORDINATED WITH THE SCHOOL DISTRICT.
- THE CONTRACTOR SHALL STAGE THE CONSTRUCTION IN FRONT OF THE MIDDLE SCHOOL SO THAT AT LEAST ONE ENTRANCE TO THE SCHOOL HAS UNRESTRICTED ACCESS. AT NO TIME WILL BOTH ENTRANCES BE CLOSED AT THE SAME TIME.
- THE CONTRACTOR WILL PROVIDE LOCAL ACCESS AND TEMPORARY DRIVEWAY ACCESS TO ALL PROPERTIES, INCLUDING PUBLIC FACILITIES DURING CONSTRUCTION. TEMPORARY AGGREGATE SHALL BE USED.
- STORM WATER DETENTION WILL BE PROVIDED BY USING EXISTING FACILITIES LOCATED AT THE HIGH SCHOOL.
- TEMPORARY FENCES WILL BE PROVIDED ALONG ALL PUBLIC SCHOOL PROPERTIES DURING THE DURATION OF CONSTRUCTION.
- THE 30-INCH DIAMETER TREE LOCATED AT STATION 515+45, LEFT SIDE WILL NOT BE REMOVED. TREE TRUNK PROTECTION AND TEMPORARY FENCING WILL BE PROVIDED BY THE CONTRACTOR.

WATER MAIN CONSTRUCTION

- ALL WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION, AND REVISIONS THERETO, THE NOTES AND ON THE PLANS, AND IN ACCORDANCE WITH CODES AND ORDINANCES OF THE UNITED CITY OF YORKVILLE, ILLINOIS.
- ALL WATER MAIN SHALL BE DUCTILE IRON PIPE CLASS 52 WITH MECHANICAL OR PUSH-ON JOINTS AND SHALL CONFORM TO ANSI A21.51 (AWWA C151), ANSI A21.50 (AWWA C150) AND ANSI A21.11 (AWWA C111). PIPE SHALL BE MANUFACTURED IN THE UNITED STATES.
- ALL FITTINGS SHALL BE DUCTILE IRON AND SHALL CONFORM TO ANSI A21.10 (AWWA C110). FITTINGS SHALL BE MANUFACTURED IN THE UNITED STATES.
- ALL PIPE AND FITTINGS SHALL BE CEMENT LINED IN ACCORDANCE WITH ANSI A21.4 (AWWA C104).
- ALL FITTINGS SHALL BE MECHANICAL JOINT FITTINGS.
- ALL MECHANICAL JOINT FITTINGS, VALVES AND HYDRANTS SHALL BE RESTRAINED WITH RETAINER GLANDS. RETAINER GLANDS SHALL BE EBAA IRON MEGALUG SERIES I100 OR APPROVED EQUAL.
- ALL WATER MAIN AND FITTINGS SHALL BE WRAPPED WITH POLYETHYLENE. POLYETHYLENE SHALL HAVE A THICKNESS OF 8-MIL IN ACCORDANCE WITH ANSI A21.5 (AWWA C105).
- LONG RADIUS CURVES, EITHER HORIZONTAL OR VERTICAL, MAY BE LAID WITH STANDARD PIPE BY DEFLECTIONS AT THE JOINTS. MAXIMUM DEFLECTIONS AT PIPE JOINTS AND LAYING RADIUS FOR THE VARIOUS PIPE LENGTHS SHALL BE IN ACCORDANCE WITH AWWA C600. WHEN RUBBER GASKET PIPE IS LAID ON A CURVE, THE PIPE SHALL BE JOINTED IN A STRAIGHT ALIGNMENT AND THEN DEFLECTED TO THE CURVED ALIGNMENT. TRENCHES SHALL BE MADE WIDER ON CURVES FOR THIS PURPOSE.
- ALL GATE VALVES SHALL BE RESILIENT WEDGE TYPE CONFORMING TO AWWA C509 AND HAVE A NON-RISING STEM, WITH A STANDARD OPERATING NUT AND SHALL OPEN IN A COUNTER-CLOCKWISE DIRECTION. GATE VALVES 10-INCHES OR LARGER SHALL BE IN VALVE VAULTS. GATE VALVES UNDER 10-INCHES SHALL BE IN EITHER A VAULT OR TRENCH ADAPTOR VALVE BOX, VAULTS OR VALVE BOXES SHALL BE LOCATED OUTSIDE OF PAVEMENT OR SIDEWALK, IF AT ALL POSSIBLE.
- ALL VALVES 16-INCHES OR LARGER SHALL BE BUTTERFLY VALVES WITH A NON-RISING STEM, SHALL HAVE A STANDARD OPERATING NUT AND SHALL OPEN IN A COUNTER-CLOCKWISE DIRECTION. BUTTERFLY VALVES SHALL BE CLOW OR AMERICAN FLOW CONTROL (WATEROUS) BUTTERFLY VALVES IN ACCORDANCE WITH AWWA C-504-00. BUTTERFLY VALVES SHALL BE IN VALVE VAULTS.
- ALL VALVE BOXES SHALL BE TRENCH ADAPTER MODEL 6 BY AMERICAN FLOW CONTROL, OR APPROVED EQUAL. LIDS ARE TO BE MARKED WITH "WATER". (VALVE BOX EXTENSIONS IF REQUIRED ARE CONSIDERED INCIDENTAL).
- ALL HYDRANTS SHALL BE IN ACCORDANCE WITH AWWA C502 AND SHALL BE A CLOW F-2545 (MEDALLION) OR A AMERICAN FLOW CONTROL - WATEROUS WB-67-250 WITH ONE 4" STEAMER NOZZLE AND TWO 2" HOSE NOZZLES, WITH NATIONAL STANDARD TREADS, A NATIONAL STANDARD OPERATING NUT, AND ABOVE GROUND BREAK FLANGE. ALL HYDRANTS SHALL HAVE AN AUXILIARY GATE VALVE WITH A TRENCH ADAPTER MODEL 6 VALVE BOX OR C.I VALVE BOX WITH GRIPARMS BY BLR ENTERPRISES.
- REPAIR COUPLINGS SHALL BE SMITH BLAIR (ROCKWELL) D.I. COUPLING TYPE 441 OR EQUAL. COUPLINGS SHALL BE PROVIDED AT LOCATIONS SHOWN ON THE PLANS OR AS REQUIRED TO MAKE PIPE CONNECTIONS.
- ALL TEES, BENDS, FIRE HYDRANTS AND VALVES SHALL BE ADEQUATELY SUPPORTED WITH A CONCRETE BASE, AND SUPPORTED Laterally WITH POURED IN PLACE THRUST BLOCKING AGAINST UNDISTURBED EARTH.
- ALL WATER MAINS SHALL HAVE A MINIMUM DEPTH OF COVER OF 5'-6".
- ALL PRESSURE TAPS TO AN EXISTING CITY MAIN SHALL BE MADE WITH A CLOW OR AMERICAN FLOW CONTROL (WATEROUS) DUCTILE IRON MECHANICAL JOINT TAPPING SLEEVE FOR SAME SIZE TAPS WITH THE MAIN, DISSIMILAR SIZE TAPS AND MAINS SHALL BE MADE WITH STAINLESS STEEL TAPPING SLEEVES AND SHALL BE MUELLER H-304, SMITH BLAIR (ROCKWELL) 662-663 OR 664-665 OR ROMAC SST. A CLOW OR AMERICAN FLOW CONTROL (WATEROUS) RESILIENT TAPPING VALVE AND SHALL BE INSTALLED WITH THE TAPPING SLEEVE. THE TAPPING SLEEVE AND VALVE SHALL BE CONSTRUCTED IN A VALVE VAULT WITH ECCENTRIC CONE.
- NO WATER SERVICE TAPS SHALL BE MADE PRIOR TO THE CITY RECEIVING THE IEPA OPERATING PERMIT.
- ALL TRENCHES CAUSED BY THE CONSTRUCTION OF SEWERS, WATER MAINS, WATER SERVICE PIPES AND THE EXCAVATION AROUND CATCH BASINS, MANHOLES, INLETS AND OTHER APPURTENANCES WHICH OCCUR WITHIN THE LIMITS OF EXISTING OR PROPOSED PAVEMENTS, SIDEWALKS AND CURB AND GUTTERS OR WHERE THE EDGE OF THE TRENCH SHALL BE WITHIN TWO (2) FEET OF SAID IMPROVEMENTS SHALL BE BACKFILLED WITH CA-6 CRUSHED LIMESTONE (IDOT CERTIFIED) AND MECHANICALLY COMPACTED.
- BEFORE ACCEPTANCE BY THE CITY ALL WORK SHALL BE INSPECTED AND APPROVED BY THE CITY OR ITS REPRESENTATIVES.
- WATER MAINS AND WATER SERVICE LINES SHALL BE PROTECTED FROM SANITARY SEWERS, STORM SEWERS, COMBINED SEWERS, HOUSE SEWER SERVICE CONNECTIONS AND DRAINS IN ACCORDANCE WITH TITLE 35: ENVIRONMENTAL PROTECTION AGENCY SUBTITLE F: PUBLIC WATER SUPPLIES, CHAPTER II: ENVIRONMENTAL PROTECTION AGENCY, PARTS 651 654 TECHNICAL POLICY STATEMENTS, SECTION 653.119.
- WHENEVER POSSIBLE, A WATER MAIN MUST BE LAID AT LEAST TEN FEET HORIZONTALLY FROM ANY EXISTING OR PROPOSED DRAIN OR SEWER LINE. SHOULD LOCAL CONDITIONS EXIST WHICH WOULD PREVENT A LATERAL SEPARATION OF TEN FEET, A WATER MAIN MAY BE LAID CLOSER THAN TEN FEET TO A STORM OR SANITARY SEWER PROVIDED THAT THE WATER MAIN INVERT IS AT LEAST EIGHTEEN INCHES ABOVE THE CROWN OF THE SEWER, AND IS EITHER IN A SEPARATE TRENCH OR IN THE SAME TRENCH ON AN UNDISTURBED EARTH SHELF LOCATED TO ONE SIDE OF THE SEWER. IF IT IS IMPOSSIBLE TO OBTAIN PROPER HORIZONTAL OR VERTICAL SEPARATION AS DESCRIBED ABOVE, THEN THE SEWER MUST ALSO BE CONSTRUCTED OF WATER MAIN TYPE MATERIAL (DUCTILE IRON PIPE WITH SLIP-ON OR MECHANICAL JOINTS, PRE-STRESSED REINFORCED CONCRETE PIPE WITH ASTM C-443 JOINTS, ETC.) AND PRESSURE TESTED TO THE MAXIMUM EXPECTED SURCHARGE HEAD TO ASSURE WATER TIGHTNESS BEFORE BACKFILLING..

WATER MAIN CONSTRUCTION (CONT'D)

- WHENEVER WATER MAINS MUST CROSS HOUSE SEWERS, STORM SEWERS OR SANITARY SEWERS, THE WATER MAIN SHALL BE LAID AT SUCH AN ELEVATION THAT THE INVERT OF THE WATER MAIN IS EIGHTEEN INCHES ABOVE THE CROWN OF THE DRAIN OR SEWER. THIS VERTICAL SEPARATION MUST BE MAINTAINED FOR THAT PORTION OF THE WATER MAIN LOCATED WITHIN TEN FEET HORIZONTALLY OF ANY SEWER OR DRAIN CROSSED. THIS MUST BE MEASURED AS THE NORMAL DISTANCE FROM THE WATER MAIN TO THE DRAIN OR SEWER. IF IT IS IMPOSSIBLE TO OBTAIN THE PROPER VERTICAL SEPARATION AS DESCRIBED ABOVE OR IF IT IS NECESSARY FOR THE WATER MAIN TO PASS UNDER A SEWER OR DRAIN, THEN THE SEWER MUST BE CONSTRUCTED OF WATER MAIN TYPE MATERIAL. THIS CONSTRUCTION MUST EXTEND ON EACH SIDE OF THE CROSSING UNTIL THE NORMAL DISTANCE FROM THE WATER MAIN TO THE SEWER OR DRAIN LINE IS AT LEAST TEN FEET. IN MAKING SUCH CROSSINGS, CENTER A LENGTH OF WATER MAIN PIPE OVER/UNDER THE SEWER TO BE CROSSED SO THAT THE JOINTS WILL BE EQUIDISTANT FROM THE SEWER AND AS REMOTE THEREFROM AS POSSIBLE. WHERE A WATER MAIN MUST CROSS UNDER A SEWER, A VERTICAL SEPARATION OF EIGHTEEN INCHES BETWEEN THE INVERT OF THE SEWER AND THE CROWN OF THE WATER MAIN SHALL BE MAINTAINED, ALONG WITH MEANS TO SUPPORT THE SEWER LINE TO PREVENT THEIR SETTling AND BREAKING THE WATER MAIN.
- VALVE VAULTS SHALL BE ADJUSTED WITH PRECAST CONCRETE ADJUSTING RINGS TO A MAXIMUM OF 0'-8".
- HYDROSTATIC TESTS - THE CONTRACTOR SHALL PERFORM HYDROSTATIC TESTS IN ACCORDANCE WITH DIVISION IV, SECTION 41 OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION, AND APPLICABLE PROVISIONS OF AWWA C-600 AND C-605. THE WATER MAINS SHALL MAINTAIN A 150 PSI AVERAGE FOR UP TO 4 HOURS DURING THE TEST. ALLOWABLE LEAKAGE SHALL BE AS SET FORTH IN THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION. THE CITY WATER OPERATOR IN CHARGE OR PERSON AUTHORIZED BY THE CITY WATER OPERATOR IN CHARGE SHALL BE PRESENT DURING ALL TESTING. THE CONTRACTOR SHALL USE A PRESSURE GAGE SUPPLIED BY THE CITY FOR THE TEST.
- DISINFECTION OF THE WATER MAIN - UPON COMPLETION OF THE NEWLY LAID WATER MAINS, THE WATER MAINS SHALL BE DISINFECTED IN ACCORDANCE WITH THE AMERICAN WATER WORKS ASSOCIATION, PROCEDURE DESIGNATION, AWWA C-651. WATER SHALL BE TESTED TO ASSURE THAT 50MG/L OF CL2 IS IN DISINFECTED WATER. THE CITY OPERATOR IN CHARGE OR PERSON AUTHORIZED BY THE CITY SHALL PERFORM SAMPLING AND PROCESSING OF THE TEST SAMPLE AND TEST RESULTS. THE COST OF THE WATER SAMPLING AND TESTING SHALL BE BORNE BY THE CITY. WATER MUST PASS TWO CONSECUTIVE DAYS OF SAMPLING TESTS BY A STATE APPROVED LAB.
- WATER VALVES AND FIRE HYDRANTS SHALL ONLY BE OPERATED BY UNITED CITY OF YORKVILLE WATER DEPARTMENT PERSONNEL. PLEASE CONTACT THE YORKVILLE WATER DEPARTMENT AT 630-553-4372.
- THE DEVELOPER SHALL REIMBURSE THE CITY OF YORKVILLE FOR THE COST OF WATER TO FILL AND TEST NEW WATER MAINS AND ALSO FOR THE COST OF LABORATORY TESTS AFTER CHLORINATION. THE WATER COST SHALL BE AT THE BULK RATE CHARGED BY THE CITY OF YORKVILLE AT THE TIME. THE VOLUME OF WATER SHALL BE CALCULATED AS THE VOLUME OF TWO AND ONE-HALF TIMES THE LENGTHS AND DIAMETERS OF NEW WATER MAINS.
- SERVICES SHALL BE EQUIPPED WITH CORPORATION STOP, CURB STOP AND BUFFALO BOX. THE BUFFALO BOX SHALL BE SET IN THE PARKWAY, ON THE CENTERLINE OF THE PROPERTY, APPROXIMATELY CENTERED BETWEEN THE BACK OF SIDEWALK AND ADJACENT RIGHT OF WAY LINE. SERVICE TRENCHES BENEATH OR WITHIN TWO FEET OF PROPOSED DRIVEWAYS, SIDEWALKS, OR OTHER PAVEMENTS SHALL BE BACKFILLED FULL-DEPTH WITH AGGREGATE. DEPTH OF BURY SHALL BE 5'-6". NO JOINTS SHALL BE ALLOWED BETWEEN THE CORPORATION STOP AND CURB STOP.
- BUFFALO BOXES SHALL NOT BE SET DIRECTLY IN PAVEMENT. THE TOP OF THE BUFFALO BOX SHALL BE SET 3-INCHES BELOW THE PAVEMENT GRADE AND IN AN INSPECTION FRAME, NEENAH R-1973 OR E AST JORDAN 1570 FRAME AND LID.

HMA MIXTURE REQUIREMENTS

| | HMA SURFACE | HMA BINDER COURSE | HMA BASE COURSE |
|---------------------|-------------------|-------------------|-----------------|
| PG GRADE ** | PG64-22 | PG64-22 | PG64-22 |
| DESIGN AIR VOIDS | 4.0% @ N50 | 4.0% @ N50 | 4.0% @ N50 |
| MIXTURE COMPOSITION | IL 12.5 OR IL 9.5 | IL 19.0FG | IL 19.0 |
| FRICTION AGGREGATE | MIXTURE D | | |
| DENSITY TEST METHOD | NUCLEAR/ CORES | NUCLEAR/ CORES | NUCLEAR/ CORES |

MATERIAL SHE BE COMPACTED TO 93.0-97.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY, EXCEPT THAT WHEN PLACED AS FIRST LIFT ON AN UNIMPROVED SUBGRADE THE MINIMUM PERCENT COMPACTION SHALL BE 92.0 PERCENT. THE MAXIMUM THEORETICAL DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE AS SPECIFIED IN THE OC/OA SPECIFICATION.

** WHEN RAP/RAS ABR EXCEEDS 20 PERCENT, THE HIGH AND LOW VIRGIN ASPHALT BINDER GRADES SHALL EACH BE REDUCED BY ONE GRADE (I.E. 25% ABR WOULD REQUIRE A VIRGIN ASPHALT BINDER GRADE OF PG 64-22 TO BE REDUCED TO PG 58-28).

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

| | | |
|-----------------------------|-------|----------------------------|
| GRANULAR MATERIALS | 2.05 | TONS / CU YD |
| BITUMINOUS MAT PRIME COAT | 0.05 | LBS / SO FT (ON HMA) |
| | 0.25 | LBS / SO FT (ON AGGREGATE) |
| AGGREGATE PRIME COAT | 0.002 | TONS / SO YD |
| HMA RESURFACING | 112 | LBS / SO YD / IN |
| SHORT TERM PAVEMENT MARKING | 10 | FT / 100 FT OF APPLICATION |
| SUPPLEMENTAL WATERING | 3 | GAL / SO YD / APPLICATION |

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION

GENERAL NOTES

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014
DRAWN BY: MJF
CHECKED BY: GT

| PLAN | DATE | BY |
|--------------------|------|----|
| SURVEYED | | |
| PLOTTED | | |
| ALIGNMENT CHECKED | | |
| RT. OF WAY CHECKED | | |
| CADD FILE NAME | | |

| PROFILE | DATE | BY |
|--------------------------|------|----|
| SURVEYED | | |
| PLOTTED | | |
| GRADES CHECKED | | |
| S.T.M. NOTED | | |
| STRUCTURE NOTATIONS CHKD | | |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|------------------|--------------|-----------|
| 1550 | 03-00031-00-PP | KENDALL | 111 | 3 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

| | | | | |
|---------------------|----------------|------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 4 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 87345

| CODE NUMBER | PAY ITEM | UNIT | TOTAL | ROADWAY | TRAFFIC SIGNALS | | NON-PARTICIPATING 0043 |
|-------------|--|-------|--------|---------|--------------------|-------------------|------------------------|
| | | | | 0004 | U.S. ROUTE 34 0021 | INTERCONNECT 0021 | |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 296 | 296 | | | |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT | 290 | 290 | | | |
| 20101000 | TEMPORARY FENCE | FOOT | 230 | 230 | | | |
| Δ 20101100 | TREE TRUNK PROTECTION | EACH | 22 | 22 | | | |
| Δ 20101200 | TREE ROOT PRUNING | EACH | 22 | 22 | | | |
| * 20200100 | EARTH EXCAVATION | CU YD | 19,209 | 19,209 | | | |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 4,496 | 4,496 | | | |
| * 20800150 | TRENCH BACKFILL | CU YD | 2,912 | 2,912 | | | |
| * 20900110 | POROUS GRANULAR BACKFILL | CU YD | 37 | 37 | | | |
| 21001000 | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION | SQ YD | 19,181 | 19,181 | | | |
| Δ 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 17,318 | 17,318 | | | |
| Δ 25000210 | SEEDING, CLASS 2A | ACRE | 3.61 | 3.61 | | | |
| Δ 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 325 | 325 | | | |
| Δ 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 325 | 325 | | | |
| Δ 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 325 | 325 | | | |
| Δ 25100630 | EROSION CONTROL BLANKET | SQ YD | 180 | 180 | | | |
| Δ 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 1,083 | 1,083 | | | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 5,399 | 5,399 | | | |
| 28000500 | INLET AND PIPE PROTECTION | EACH | 14 | 14 | | | |
| 28000510 | INLET FILTERS | EACH | 63 | 63 | | | |
| 28100107 | STONE RIPRAP, CLASS A4 | SQ YD | 95 | 95 | | | |
| 28200200 | FILTER FABRIC | SQ YD | 95 | 95 | | | |
| * 30300001 | AGGREGATE SUBGRADE IMPROVEMENT | CU YD | 4,496 | 4,496 | | | |
| * 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SQ YD | 25,400 | 25,400 | | | |
| 31101000 | SUBBASE GRANULAR MATERIAL, TYPE B | TON | 2,822 | 2,822 | | | |
| * 35501306 | HOT-MIX ASPHALT BASE COURSE, 5 1/2" | SQ YD | 21,988 | 21,988 | | | |
| * 40200900 | AGGREGATE SURFACE COURSE, TYPE B | CU YD | 40 | 40 | | | |
| * 40603082 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0 FG, N50 | TON | 2,820 | 2,820 | | | |
| * 40603335 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 | TON | 2,291 | 2,291 | | | |
| 42300200 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH | SQ YD | 595 | 595 | | | |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SQ FT | 11,231 | 11,231 | | | |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 735 | 735 | | | |
| 44000100 | PAVEMENT REMOVAL | SQ YD | 13,657 | 13,657 | | | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 3,489 | 3,489 | | | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 1,781 | 1,781 | | | |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 15,604 | 15,604 | | | |
| * 48101500 | AGGREGATE SHOULDERS, TYPE B 6" | SQ YD | 55 | 55 | | | |
| * 50105220 | PIPE CULVERT REMOVAL | FOOT | 66 | 66 | | | |

* SEE SPECIAL PROVISIONS
Δ SPECIALTY ITEM

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| PLAN | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. _____ | ALIGNMENT CHECKED | |
| | RT. OF WAY CHECKED | |
| | CADD FILE NAME | |

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| PROFILE | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. _____ | GRADES CHECKED | |
| | B.M. NOTED | |
| | STRUCTURE NOTATIONS CHKD | |

COMPANY NAME: #COMPANY_NAME/8/29/2014 10:08:45 AM
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
SUMMARY OF QUANTITIES
(SHEET 1 OF 5)

VERT. SCALE: _____
HORIZ. SCALE: _____
DATE: JULY 2014

DRAWN BY: M.J.F.
CHECKED BY: G.T.

| | | | | |
|---------------------|----------------|----------|------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 5 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |

| CODE NUMBER | PAY ITEM | UNIT | TOTAL | TRAFFIC SIGNALS | | | NON-PARTICIPATING Q043 |
|-------------|---|---|-------|-----------------|---------------|--------------|------------------------|
| | | | | ROADWAY | U.S. ROUTE 34 | INTERCONNECT | |
| | | | | 0004 | 0021 | 0021 | |
| * 54213660 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15" | EACH | 1 | 1 | | | |
| * 54213675 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30" | EACH | 1 | 1 | | | |
| * 54213681 | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36" | EACH | 1 | 1 | | | |
| * 550A0040 | STORM SEWERS, CLASS A, TYPE 1 10" | FOOT | 32 | 32 | | | |
| * 550A0050 | STORM SEWERS, CLASS A, TYPE 1 12" | FOOT | 1835 | 1835 | | | |
| * 550A0070 | STORM SEWERS, CLASS A, TYPE 1 15" | FOOT | 949 | 949 | | | |
| * 550A0090 | STORM SEWERS, CLASS A, TYPE 1 18" | FOOT | 803 | 803 | | | |
| * 550A0110 | STORM SEWERS, CLASS A, TYPE 1 21" | FOOT | 205 | 205 | | | |
| * 550A0120 | STORM SEWERS, CLASS A, TYPE 1 24" | FOOT | 146 | 146 | | | |
| * 550A0130 | STORM SEWERS, CLASS A, TYPE 1 27" | FOOT | 161 | 161 | | | |
| * 550A0140 | STORM SEWERS, CLASS A, TYPE 1 30" | FOOT | 302 | 302 | | | |
| * 550A0160 | STORM SEWERS, CLASS A, TYPE 1 36" | FOOT | 221 | 221 | | | |
| | 55100200 | STORM SEWER REMOVAL 6" | FOOT | 104 | 104 | | |
| | 55100400 | STORM SEWER REMOVAL 10" | FOOT | 279 | 279 | | |
| | 55100500 | STORM SEWER REMOVAL 12" | FOOT | 1868 | 1868 | | |
| | 55100700 | STORM SEWER REMOVAL 15" | FOOT | 185 | 185 | | |
| | 55100900 | STORM SEWER REMOVAL 18" | FOOT | 247 | 247 | | |
| | 55101200 | STORM SEWER REMOVAL 24" | FOOT | 716 | 716 | | |
| Δ * | 56103100 | DUCTILE IRON WATER MAIN 8" | FOOT | 615 | | | 615 |
| Δ * | 56105000 | WATER VALVES 8" | EACH | 3 | | | 3 |
| Δ * | 56105600 | INSERTING VALVES 8" | EACH | 1 | | | 1 |
| Δ * | 56400400 | FIRE HYDRANTS TO BE RELOCATED | EACH | 4 | | | 4 |
| Δ * | 56400500 | FIRE HYDRANTS TO BE REMOVED | EACH | 1 | | | 1 |
| Δ * | 56400820 | FIRE HYDRANT WITH AUXILIARY VALVE AND VALVE BOX | EACH | 1 | | | 1 |
| Δ * | 56500200 | DOMESTIC WATER SERVICE BOXES TO BE MOVED | EACH | 3 | | | 3 |
| * | 60107600 | PIPE UNDERDRAINS 4" | FOOT | 175 | 175 | | |
| | 60200805 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE | EACH | 2 | 2 | | |
| | 60201340 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 4 | 4 | | |
| | 60203905 | CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 2 | 2 | | |
| | 60205040 | CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 1 | 1 | | |
| | 60207000 | CATCH BASINS, TYPE A, TYPE 8 GRATE | EACH | 1 | 1 | | |
| | 60207605 | CATCH BASINS, TYPE C, TYPE 8 GRATE | EACH | 1 | 1 | | |
| | 60208240 | CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE | EACH | 1 | 1 | | |
| | 60218400 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 8 | 8 | | |
| | 60219000 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 8 GRATE | EACH | 1 | 1 | | |
| | 60219540 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 3 | 3 | | |
| | 60221100 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 16 | 16 | | |
| | 60221700 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 8 GRATE | EACH | 1 | 1 | | |
| | 60222240 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 1 | 1 | | |

* SEE SPECIAL PROVISIONS
Δ SPECIALTY ITEM

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| PLAN | SURVEYED | DATE |
| NOTE BOOK | ALIGNED | |
| NO. | RT. OF WAY CHECKED | |
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| PROFILE | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | GRADES CHECKED | |
| | BLK. NOTED | |
| | STRUCTURE NOTATIONS CHECKED | |

COMPANY NAME: (COMPANY NAME)
098-330002.DGN

ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION**
**SUMMARY OF QUANTITIES
(SHEET 2 OF 5)**
SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014
DRAWN BY: M.J.F.
CHECKED BY: GT

| | | | | |
|---------------------|----------------|------------------|--------------|-----------|
| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 6 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |

| CODE NUMBER | PAY ITEM | UNIT | TOTAL | TRAFFIC SIGNALS | | | NON-PARTICIPATING 0043 |
|-------------|--|--------|--------|-----------------|---------------|--------------|---------------------------|
| | | | | ROADWAY | U.S. ROUTE 34 | INTERCONNECT | |
| | | | | 0004 | 0021 | 0021 | |
| 60223800 | MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 2 | 2 | | | |
| 60224039 | MANHOLES, TYPE A, 6'-DIAMETER, TYPE 24 FRAME AND GRATE | EACH | 1 | 1 | | | |
| 60236200 | INLETS, TYPE A, TYPE 8 GRATE | EACH | 6 | 6 | | | |
| 60236800 | INLETS, TYPE A, TYPE 11 FRAME AND GRATE | EACH | 1 | 1 | | | |
| 60237470 | INLETS, TYPE A, TYPE 24 FRAME AND GRATE | EACH | 36 | 36 | | | |
| 60240301 | INLETS, TYPE B, TYPE 8 GRATE | EACH | 1 | 1 | | | |
| 60240328 | INLETS, TYPE B, TYPE 24 FRAME AND GRATE | EACH | 16 | 16 | | | |
| Δ* | 60248900 VALVE VAULTS, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 3 | | | | 3 |
| | 60255500 MANHOLES TO BE ADJUSTED | EACH | 9 | 9 | | | |
| | 60256940 MANHOLES TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE | EACH | 1 | 1 | | | |
| Δ | 60265900 VALVE VAULTS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID | EACH | 5 | | | | 5 |
| | 60500040 REMOVING MANHOLES | EACH | 9 | 9 | | | |
| | 60500060 REMOVING INLETS | EACH | 24 | 24 | | | |
| * | 60603800 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 | FOOT | 313 | 313 | | | |
| * | 60605000 COMBINATION CONCRETE CURB AND GUTTER, TYPE B6.24 | FOOT | 10,093 | 10,093 | | | |
| | 67100100 MOBILIZATION | L SUM | 1 | 1 | | | |
| * | 70106800 CHANGEABLE MESSAGE SIGN | CAL MO | 40 | 40 | | | |
| | 70300100 SHORT-TERM PAVEMENT MARKING | FOOT | 950 | 950 | | | |
| | 70300210 TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 879 | 879 | | | |
| | 70300220 TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 12,211 | 12,211 | | | |
| | 70300240 TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 1,539 | 1,539 | | | |
| | 70300250 TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 2,165 | 2,165 | | | |
| | 70300260 TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 1,109 | 1,109 | | | |
| | 70300280 TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 394 | 394 | | | |
| * | 70301000 WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 317 | 317 | | | |
| Δ | 72000100 SIGN PANEL TYPE 1 | SQ FT | 4 | 4 | | | |
| Δ | 72400500 RELOCATE SIGN PANEL ASSEMBLY - TYPE A | EACH | 2 | 2 | | | |
| Δ | 72900100 METAL POST - TYPE A | FOOT | 51 | 51 | | | |
| *Δ | 73502000 RELOCATE GROUND-MOUNTED SIGN SUPPORT | EACH | 47 | 47 | | | |
| Δ | 78000100 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 911 | 911 | | | |
| Δ | 78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 12,211 | 12,211 | | | |
| Δ | 78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 1,540 | 1,540 | | | |
| Δ | 78000500 THERMOPLASTIC PAVEMENT MARKING - LINE 8" | FOOT | 2,988 | 2,988 | | | |
| Δ | 78000600 THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 876 | 876 | | | |
| Δ | 78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 268 | 268 | | | |
| Δ | 78100100 RAISED REFLECTIVE PAVEMENT MARKER | EACH | 15 | 15 | | | |
| * | 78300100 PAVEMENT MARKING REMOVAL | SQ FT | 694 | 694 | | | |
| | 78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 8 | 8 | | | |
| Δ | 81028320 UNDERGROUND CONDUIT, PVC, 1" DIA. | FOOT | 296 | | 296 | | |

* SEE SPECIAL PROVISIONS
Δ SPECIALTY ITEM

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| PLAN | SURVEYED | BY | DATE |
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| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
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| | CHECKED | | |
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| | STRUCTURE | | |
| | NOTATIONS | | |
| | CHKD | | |

COMPANY NAME: #COMPANY NAME
8/29/2014 10:06:25 AM
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION**
**SUMMARY OF QUANTITIES
(SHEET 3 OF 5)**
SCALE: VERT.
HORIZ.
DATE: JULY 2014
DRAWN BY: MJF
CHECKED BY: GT

| | | | | |
|---------------------|----------------|----------|------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 7 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |

| CODE NUMBER | PAY ITEM | UNIT | TOTAL | TRAFFIC SIGNALS | | NON-PARTICIPATING 0043 |
|-------------|----------|---|-------|-----------------|-----------------------|---------------------------|
| | | | | ROADWAY 0004 | U.S. ROUTE 34 0021 | |
| Δ | 81028360 | UNDERGROUND CONDUIT, PVC, 2 1/2" DIA. | FOOT | 72 | 72 | |
| Δ | 85000200 | MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 | 1 | |
| Δ | 87300010 | GROUNDING EXISTING HANDHOLE FRAME AND COVER | EACH | 3 | 3 | |
| Δ | 87301215 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | FOOT | 200 | 200 | |
| Δ | 87301225 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 693 | 693 | |
| Δ | 87301245 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 1,142 | 1,142 | |
| Δ | 87301255 | ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 173 | 173 | |
| Δ | 87301305 | ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1,090 | 1,090 | |
| Δ | 87301900 | ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 451 | 451 | |
| Δ | 87502450 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 11 FT. | EACH | 1 | 1 | |
| Δ | 87502500 | TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 2 | 2 | |
| Δ | 87800100 | CONCRETE FOUNDATION, TYPE A | FOOT | 9 | 9 | |
| Δ | 87900200 | DRILL EXISTING HANDHOLE | EACH | 16 | 16 | |
| Δ | 88040070 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 4 | 4 | |
| Δ | 88040090 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 2 | 2 | |
| Δ | 88040150 | SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 1 | 1 | |
| Δ | 88102717 | PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER | EACH | 2 | 2 | |
| Δ | 88200410 | TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 7 | 7 | |
| *Δ | 88500100 | INDUCTIVE LOOP DETECTOR | EACH | 9 | 9 | |
| Δ | 88600100 | DETECTOR LOOP, TYPE I | FOOT | 806 | 806 | |
| Δ | 88700200 | LIGHT DETECTOR | EACH | 3 | 3 | |
| Δ | 88700300 | LIGHT DETECTOR AMPLIFIER | EACH | 1 | 1 | |
| Δ | 88800100 | PEDESTRIAN PUSH-BUTTON | EACH | 2 | 2 | |
| Δ | 89502300 | REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 1,862 | 1,862 | |
| Δ | 89502375 | REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 | 1 | |
| Δ | 89502385 | REMOVE EXISTING CONCRETE FOUNDATION | EACH | 1 | 1 | |
| * | X0322936 | REMOVE EXISTING FLARED END SECTION | EACH | 3 | 3 | |
| * | X0323256 | REMOVE AND RELOCATE FLAGPOLE | EACH | 1 | 1 | |
| * | X0323443 | PRECAST MODULAR RETAINING WALL | SQ FT | 786 | 786 | |
| Δ* | X0325207 | TELEVISION INSPECTION OF SEWER | FOOT | 4,705 | 4,576 | 129 |
| Δ* | X0327312 | POLYETHYLENE WRAP, 8" | FOOT | 615 | | 615 |
| * | X2130010 | EXPLORATION TRENCH, SPECIAL | FOOT | 235 | 235 | |
| * | X4021000 | TEMPORARY ACCESS (PRIVATE ENTRANCE) | EACH | 33 | 33 | |
| * | X4022000 | TEMPORARY ACCESS (COMMERCIAL ENTRANCE) | EACH | 6 | 6 | |
| * | X4023000 | TEMPORARY ACCESS (ROAD) | EACH | 1 | 1 | |
| * | 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 74,201 | 74,201 | |
| Δ* | X5610004 | DUCTILE IRON WATER MAIN FITTINGS | POUND | 948 | | 948 |
| Δ* | X5610651 | ABANDON EXISTING WATER MAIN, FILL WITH CLSM | FOOT | 84 | | 84 |
| Δ* | X5610748 | WATER MAIN LINE STOP 8" | EACH | 2 | | 2 |

* SEE SPECIAL PROVISIONS
Δ SPECIALTY ITEM

| | | | |
|------|----------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | CHECKED | | |
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| PROFILE | SURVEYED | BY | DATE |
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| | DATE | | |
| | NOTE BOOK NO. | | |
| | NO. | | |
| | STRUCTURE NOTATION CHKD | | |

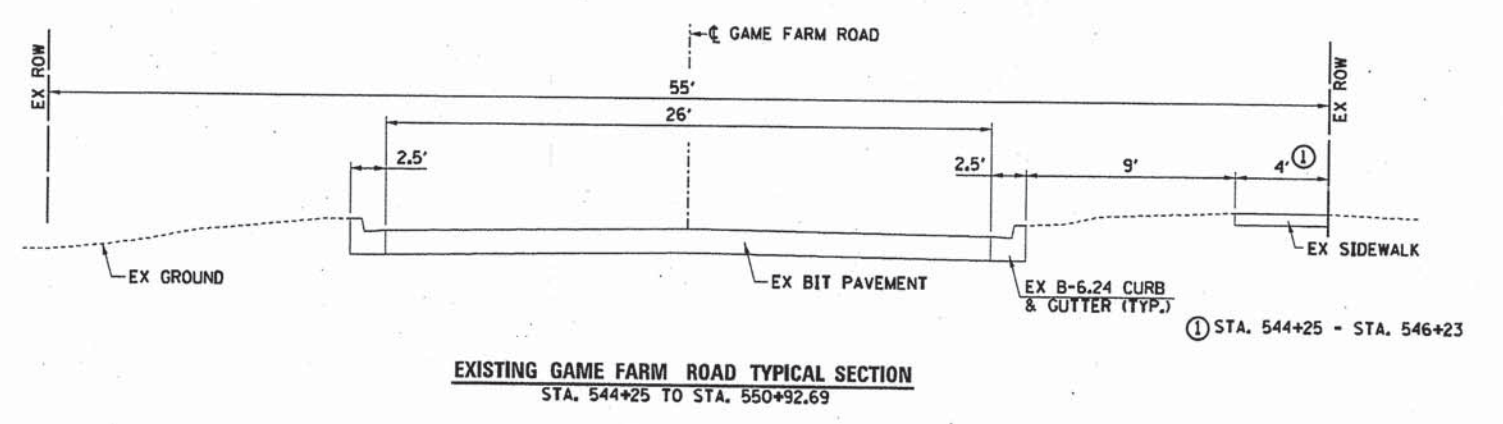
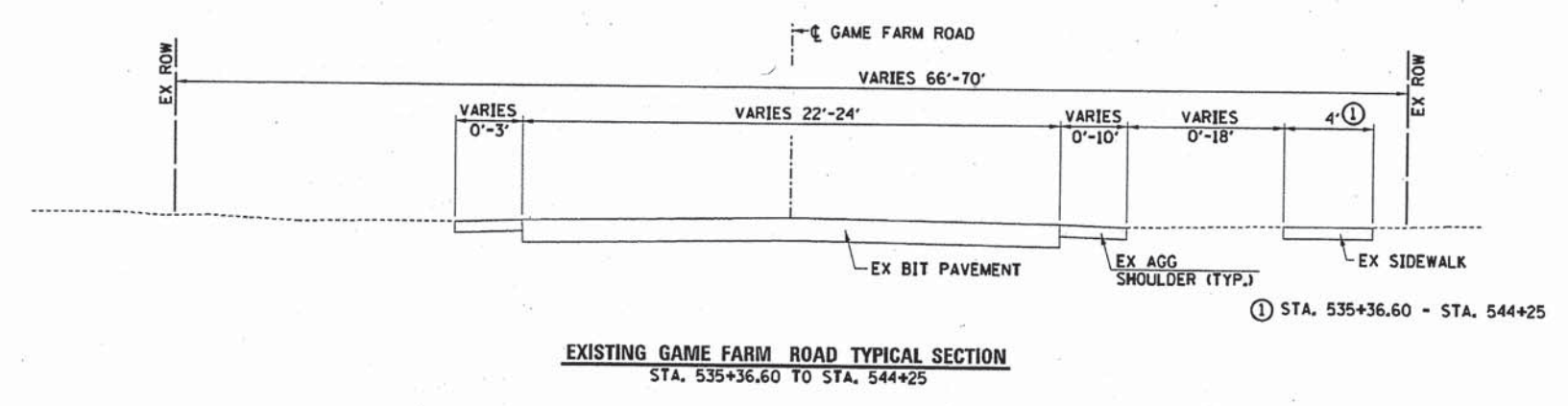
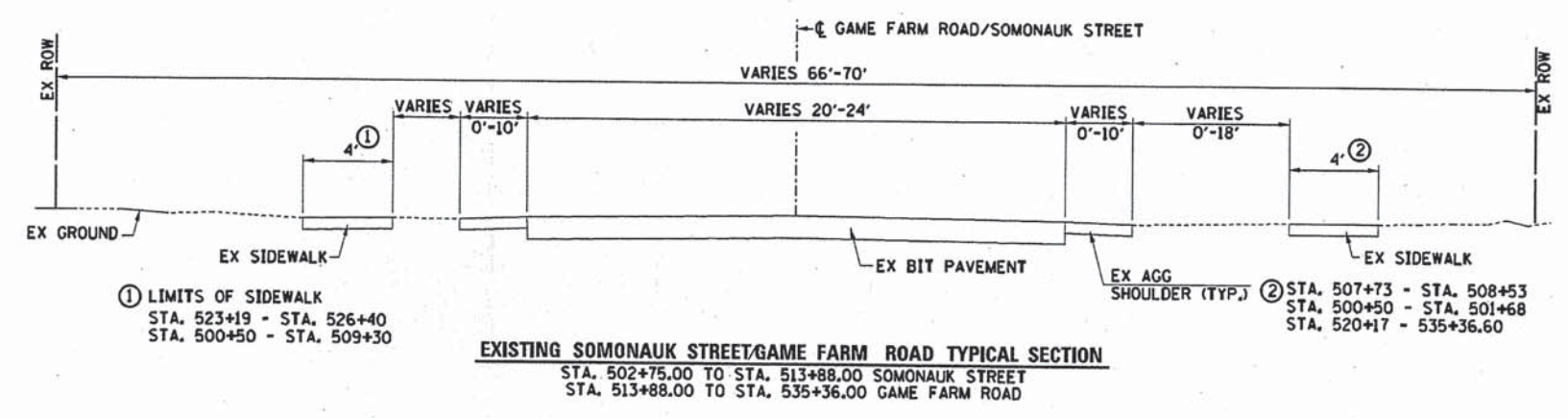
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ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION**
**SUMMARY OF QUANTITIES
(SHEET 4 OF 5)**

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: MJF
CHECKED BY: GT

| | | | | |
|---------------------|----------------|----------|------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 9 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |



| | | | |
|---------------|----------|----|------|
| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK NO. | ALIGNED | | |
| | PLOTTED | | |
| | CHECKED | | |
| | BY | | |
| | DATE | | |

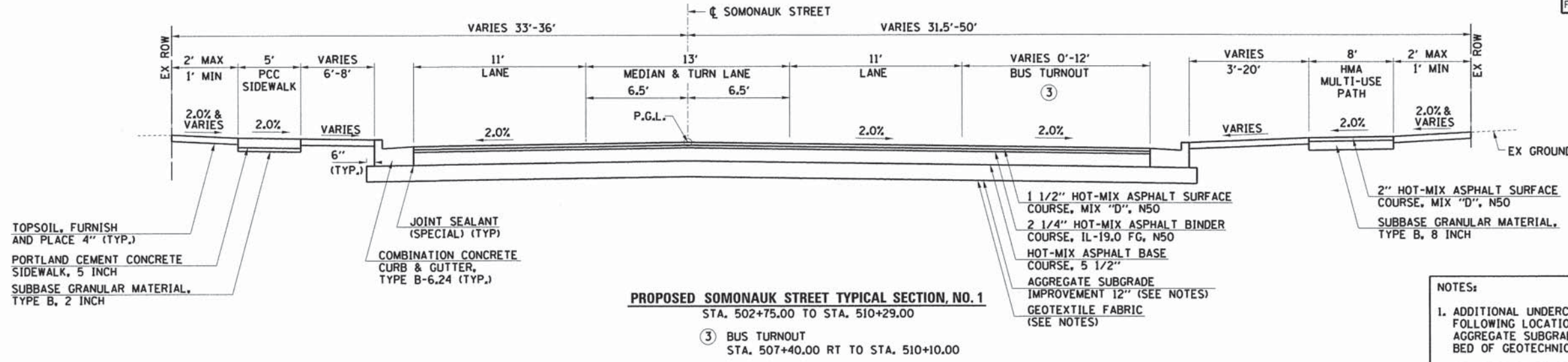
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|---------------|----------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK NO. | GRADES | | |
| | NOTED | | |
| | CHECKED | | |
| | BY | | |
| | DATE | | |

COMPANY NAME:
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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 TYPICAL SECTIONS

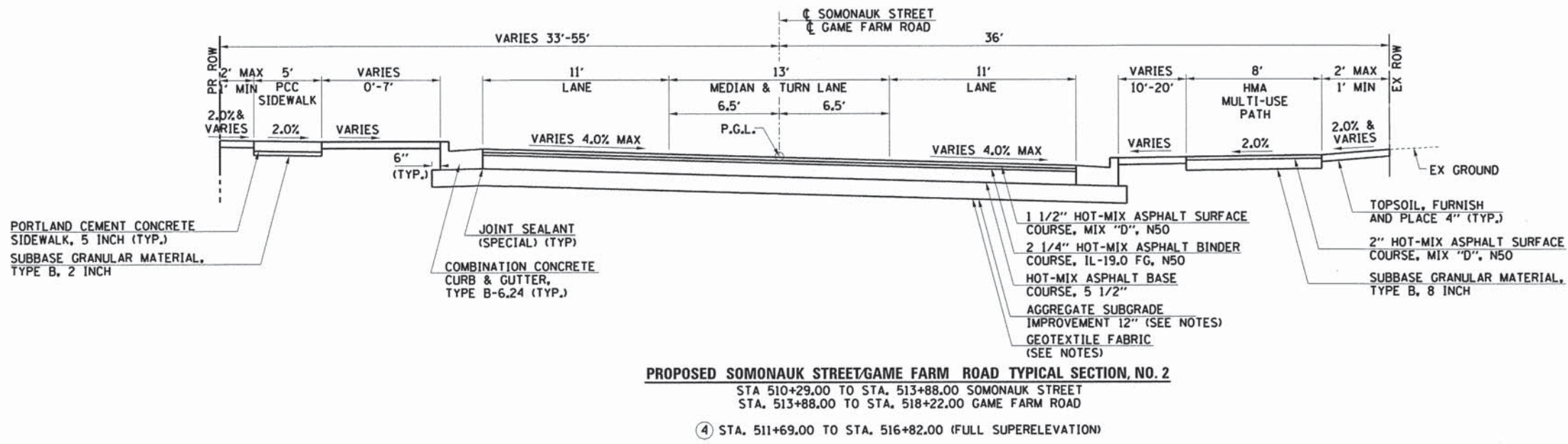
SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: M.J.F.
 CHECKED BY: G.T.



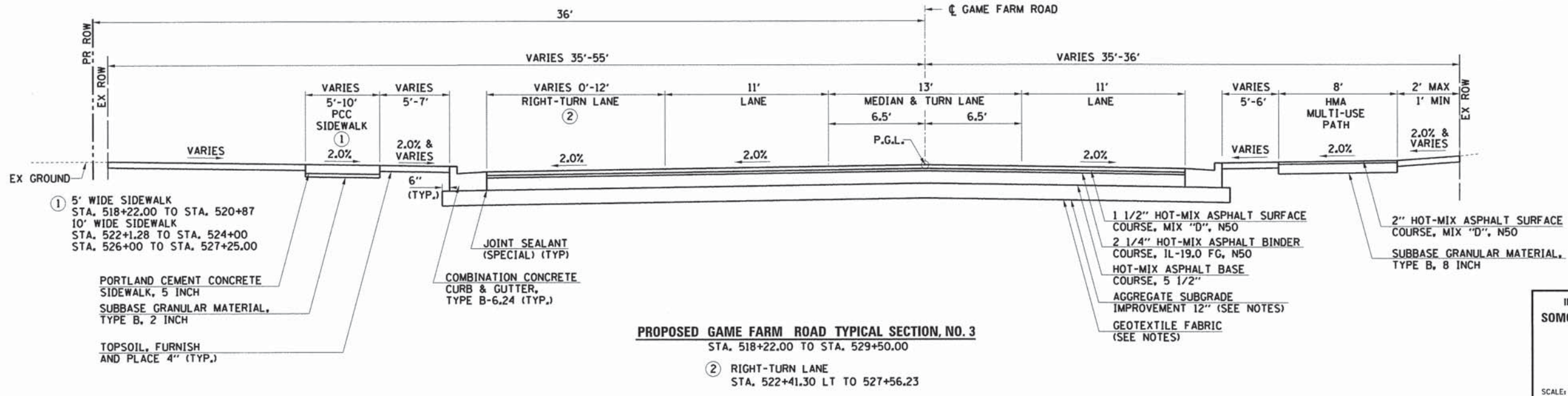
NOTES:

- ADDITIONAL UNDERCUTS SHALL BE PROVIDED IN THE FOLLOWING LOCATIONS AND REPLACED WITH AGGREGATE SUBGRADE IMPROVEMENT (CU YD) ON A BED OF GEOTECHNICAL FABRIC.
 STA. 502+75 TO STA. 507+50 (6")
 STA. 507+50 TO STA. 511+85 (12")
 STA. 519+50 TO STA. 522+50 (6")
 STA. 522+50 TO STA. 525+25 (12")
 STA. 525+25 TO STA. 531+25 (6")
- ALL UNDERCUT AREAS SHALL BE JUSTIFIED AT THE TIME OF CONSTRUCTION BY A QUALIFIED SOILS INSPECTOR. A CONE PENETROMETER SHALL BE USED TO TEST THE SUBGRADE AND THE IDOT SUBGRADE STABILITY MANUAL SHALL BE FOLLOWED. ANY AGGREGATE SUBGRADE IMPROVEMENT NOT NEEDED AT THE TIME OF CONSTRUCTION WILL BE DELETED FROM THE CONTRACT. NO ADJUSTMENTS IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES FROM THE ESTIMATED QUANTITIES SHOWN ON THE PLANS.



④ SUPERELEVATION TRANSITION TABLE

| STATION | LT SLOPE | RT SLOPE |
|-----------|----------|----------|
| 510+66.00 | -2.00% | -2.00% |
| 510+92.00 | +0.00% | -2.00% |
| 511+31.00 | +2.00% | -2.00% |
| 511+44.02 | +2.69% | -2.69% |
| 511+69.00 | +4.00% | -4.00% |
| 516+82.00 | +4.00% | -4.00% |
| 517+06.92 | +2.69% | -2.69% |
| 517+20.00 | +2.00% | -2.00% |
| 517+59.00 | +0.00% | -2.00% |
| 517+85.00 | -2.00% | -2.00% |

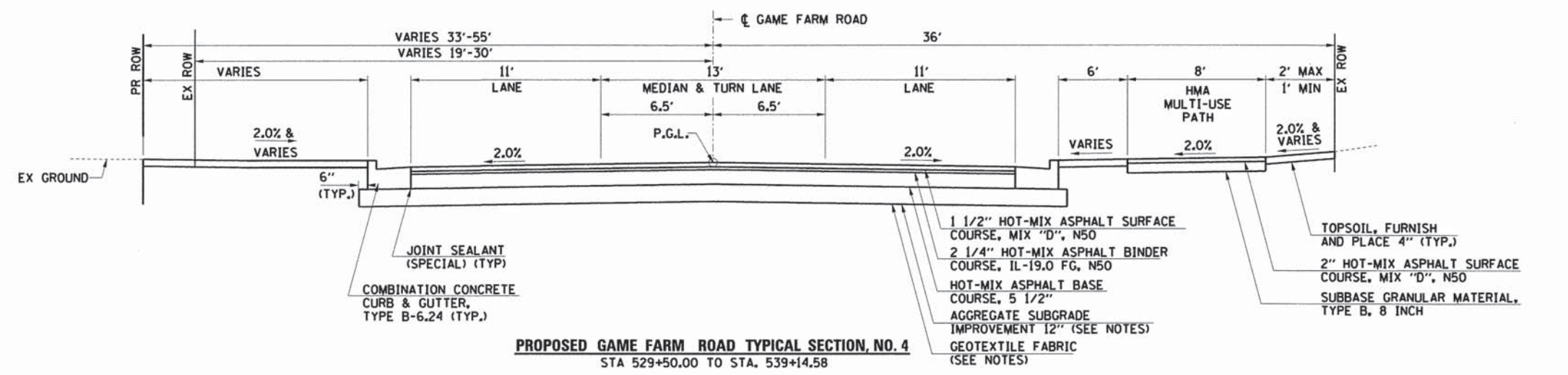


ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
 TYPICAL SECTIONS

| | | | |
|-----------|----------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK | ALIGNED | | |
| NO. | CHECKED | | |
| | RT. OF WAY | | |
| | CHECKED | | |
| | CADD FILE NAME | | |

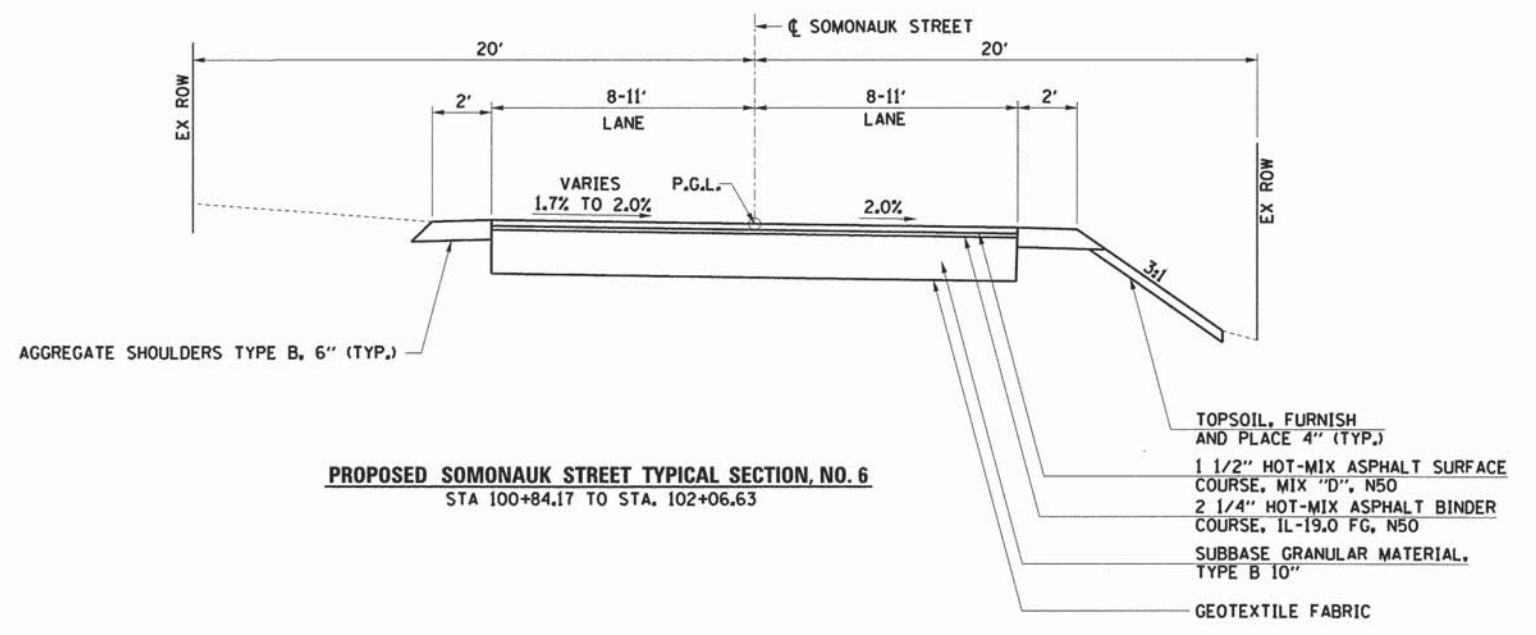
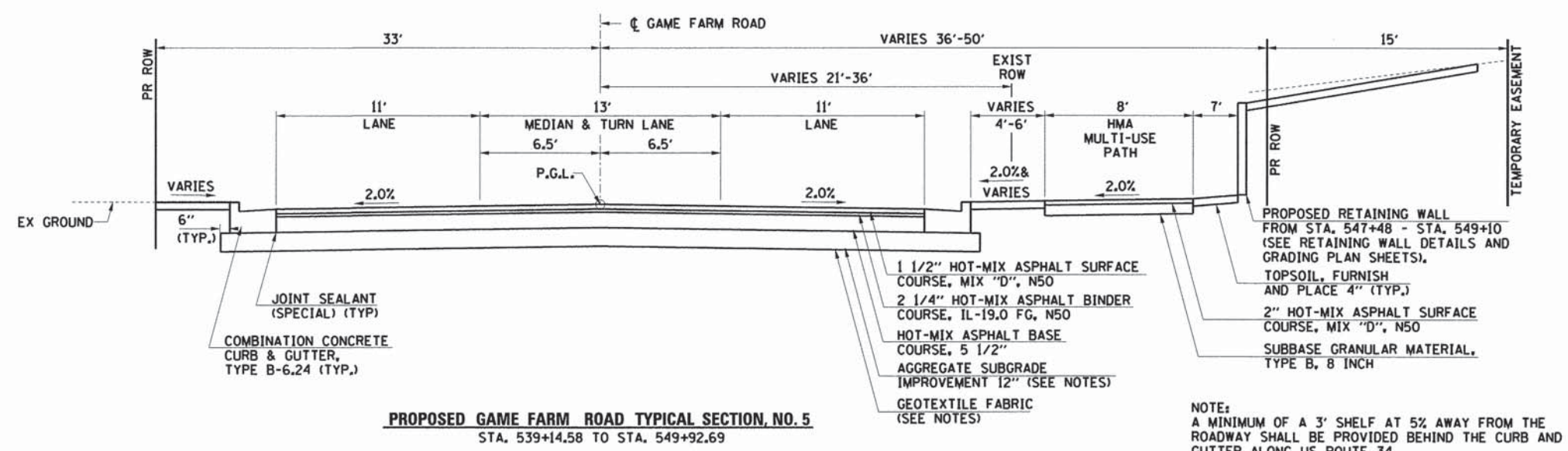
| | | | |
|-----------|-----------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK | CHECKED | | |
| NO. | BY | | |
| | NOTED | | |
| | STRUCTURE | | |
| | NOTATIONS | | |
| | CHFD | | |

| | | | | |
|---------------------|----------------|----------|--------------|------------------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 11 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | | FED. AID PROJECT |
| CONTRACT NO. 87345 | | | | |



NOTES:

- ADDITIONAL UNDERCUTS SHALL BE PROVIDED IN THE FOLLOWING LOCATIONS AND REPLACED WITH AGGREGATE SUBGRADE IMPROVEMENT (CU YD) ON A BED OF GEOTECHNICAL FABRIC.
STA. 525+25 TO STA. 531+25 (6")
STA. 531+25 TO STA. 534+25 (12")
STA. 534+25 TO STA. 539+25 (6")
STA. 542+75 TO STA. 547+75 (12")
- ALL UNDERCUT AREAS SHALL BE JUSTIFIED AT THE TIME OF CONSTRUCTION BY A QUALIFIED SOILS INSPECTOR. A CONE PENETROMETER SHALL BE USED TO TEST THE SUBGRADE AND THE IDOT SUBGRADE STABILITY MANUAL SHALL BE FOLLOWED, ANY AGGREGATE SUBGRADE IMPROVEMENT NOT NEEDED AT THE TIME OF CONSTRUCTION WILL BE DELETED FROM THE CONTRACT. NO ADJUSTMENTS IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES FROM THE ESTIMATED QUANTITIES SHOWN ON THE PLANS.
- THE USE OF RAP IN THE SHOULDER MATERIALS IS PROHIBITED.



ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
TYPICAL SECTIONS

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014
DRAWN BY: M.J.F.
CHECKED BY: G.T.

PLAN

| | |
|--------------------|------|
| SURVEYED | DATE |
| PLOTTED | |
| ALIGNMENT CHECKED | |
| RT. OF WAY CHECKED | |
| CADD FILE NAME | |

PROFILE

| | |
|--------------------------|------|
| SURVEYED | DATE |
| PLOTTED | |
| GRADES CHECKED | |
| B.M. NOTED | |
| STRUCTURE NOTATIONS CHKD | |

COMPANY NAME: #COMPANY_NAME#
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TREE REMOVAL (6 TO 15 UNITS DIAMETER)

| UNITS | LOCATION |
|-------|-------------------------|
| 12 | STA. 503+05.19, 32.4 RT |
| 15 | STA. 503+68.14, 23.2 LT |
| 15 | STA. 508+95.24, 9.5 LT |
| 12 | STA. 509+17.9, 42.2 RT |
| 8 | STA. 512+27.93, 27.3 LT |
| 8 | STA. 512+64.38, 36.5 LT |
| 8 | STA. 512+64.38, 36.5 LT |
| 8 | STA. 512+64.38, 36.5 LT |
| 8 | STA. 512+64.38, 36.5 LT |
| 8 | STA. 512+70.07, 44.5 LT |
| 8 | STA. 513+91.74, 36.4 LT |
| 12 | STA. 513+92.57, 41.7 LT |
| 12 | STA. 513+95.90, 34.8 LT |
| 8 | STA. 513+97.46, 39.8 LT |
| 15 | STA. 514+22.31, 30.1 LT |
| 6 | STA. 522+79.37, 43.9 LT |
| 6 | STA. 534+24.75, 23.4 LT |
| 6 | STA. 534+30.83, 16 LT |
| 10 | STA. 534+33.27, 24.8 LT |
| 6 | STA. 534+97.14, 21.8 LT |
| 12 | STA. 539+84.79, 21.6 LT |
| 12 | STA. 540+16.32, 21.0 LT |
| 12 | STA. 540+69.45, 20.1 LT |
| 12 | STA. 540+97.76, 21.9 LT |
| 14 | STA. 544+14.85, 19.1 LT |
| 14 | STA. 544+24.14, 19.0 LT |
| 14 | STA. 544+34.89, 18.7 LT |
| 15 | STA. 544+91.34, 18.4 LT |

296 TOTAL

TREE REMOVAL (OVER 15 UNITS DIAMETER)

| UNITS | LOCATION |
|-------|-------------------------|
| 42 | STA. 503+35.91, 20.1 LT |
| 24 | STA. 508+32.96, 19.4 LT |
| 24 | STA. 509+32.25, 8.7 LT |
| 24 | STA. 509+86.25, 10.7 LT |
| 20 | STA. 510+61.73, 12.8 LT |
| 18 | STA. 511+01.66, 16.6 LT |
| 18 | STA. 511+59.52, 17.7 LT |
| 20 | STA. 542+83.26, 17.6 LT |
| 18 | STA. 543+10.89, 12.8 LT |
| 42 | STA. 543+26.05, 14.6 LT |
| 40 | STA. 546+06.33, 19.3 LT |

290 TOTAL

TEMPORARY FENCE

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 200 | STA. 515+31, 26.7 LT TO STA. 515+59, 26.7 LT (STAGE 1A) |
| 30 | STA. 547+20, 49 RT TO STA. 549+20 49 RT (STAGE 2A) |

230 TOTAL

TREE TRUNK PROTECTION

| EACH | LOCATION |
|----------------|-------------------------|
| GAME FARM ROAD | |
| 1 | STA. 502+92.41, 32.7 LT |
| 1 | STA. 503+69.40, 30.4 LT |
| 1 | STA. 507+78.43, 41.2 RT |
| 1 | STA. 510+98.00, 37.5 LT |
| 1 | STA. 513+63.43, 38.9 LT |
| 1 | STA. 515+17.41, 31.4 LT |
| 1 | STA. 516+10.74, 33.5 LT |
| 1 | STA. 522+00.00, 64.5 LT |
| 1 | STA. 522+06.25, 69.7 LT |
| 1 | STA. 525+57.90, 52.9 LT |
| 1 | STA. 532+22.27, 30.9 LT |
| 1 | STA. 532+78.69, 31 LT |
| 1 | STA. 535+02.01, 35.4 LT |
| 1 | STA. 535+65.36, 26.7 LT |
| 1 | STA. 536+23.92, 27.9 LT |
| 1 | STA. 536+55.75, 26.2 LT |
| 1 | STA. 536+77.69, 23.0 LT |
| 1 | STA. 540+97.76, 21.9 LT |
| 1 | STA. 541+39.20, 30.7 LT |
| 1 | STA. 541+46.87, 27.4 LT |
| 1 | STA. 542+75.81, 22.5 LT |
| 1 | STA. 542+83.35, 26.3 LT |

22 TOTAL

TREE ROOT PRUNING

| EACH | LOCATION |
|----------------|-------------------------|
| GAME FARM ROAD | |
| 1 | STA. 502+92.41, 32.7 LT |
| 1 | STA. 503+69.40, 30.4 LT |
| 1 | STA. 507+78.43, 41.2 RT |
| 1 | STA. 510+98.00, 37.5 LT |
| 1 | STA. 513+63.43, 38.9 LT |
| 1 | STA. 515+17.41, 31.4 LT |
| 1 | STA. 516+10.74, 33.5 LT |
| 1 | STA. 522+00.00, 64.5 LT |
| 1 | STA. 522+06.25, 69.7 LT |
| 1 | STA. 525+57.90, 52.9 LT |
| 1 | STA. 532+22.27, 30.9 LT |
| 1 | STA. 532+78.69, 31 LT |
| 1 | STA. 535+02.01, 35.4 LT |
| 1 | STA. 535+65.36, 26.7 LT |
| 1 | STA. 536+23.92, 27.9 LT |
| 1 | STA. 536+55.75, 26.2 LT |
| 1 | STA. 536+77.69, 23.0 LT |
| 1 | STA. 540+97.76, 21.9 LT |
| 1 | STA. 541+39.20, 30.7 LT |
| 1 | STA. 541+46.87, 27.4 LT |
| 1 | STA. 542+75.81, 22.5 LT |
| 1 | STA. 542+83.35, 26.3 LT |

22 TOTAL

TRENCH BACKFILL

| CU YD | LOCATION | |
|------------------------------|------------------------------|-------------------------------------|
| GAME FARM ROAD - STORM SEWER | | |
| 4 | 502+85 TO 503+10.5 RT | STORM SEWER 12" |
| 8 | 503+10.5 TO 503+49.38 | STORM SEWER 12" |
| 3 | 503+75 TO 503+75 RT | STORM SEWER 12" |
| 5 | 503+75 LT TO 503+75 RT | STORM SEWER 12" |
| 9 | 503+38 TO 504+04.60 RT | STORM SEWER REMOVAL 6" |
| 4 | 503+75 RT TO 504+96.3 RT | STORM SEWER 12" |
| 1 | 504+21 RT TO 504+22.3 RT | STORM SEWER 15" |
| 5 | 505+25 RT TO 505+25 LT | STORM SEWER 12" |
| 48 | 505+25 RT TO 506+80 RT | STORM SEWER 15" |
| 5 | 506+80 RT TO 506+80 LT | STORM SEWER 12" |
| 34 | 506+80 RT TO 508+15.33 RT | STORM SEWER 15" |
| 5 | 507+86 TO 508+19.26 LT | STORM SEWER 12" |
| 43 | 508+19.26 LT TO 508+13.33 RT | STORM SEWER 15" & INLET REMOVAL |
| | 508+19.26 LT TO 508+15.33 RT | STORM SEWER REMOVAL 12" SAME TRENCH |
| 5 | 508+13.3 RT TO 508+15.3 LT | STORM SEWER 12" & INLET REMOVAL |
| | 508+13.3 RT TO 508+15.3 LT | STORM SEWER REMOVAL 12" SAME TRENCH |
| 13 | 508+15.33 RT TO 508+70.95 RT | STORM SEWER 12" |
| 4 | 508+16.17 RT TO 508+70.95 RT | STORM SEWER REMOVAL 12" |
| 11 | 509+00 TO 509+82 RT | STORM SEWER 12" |
| 6 | 509+82 LT TO 509+82 RT | STORM SEWER 12" |
| 6 | 509+82 TO 510+20 RT | STORM SEWER 12" |
| 5 | 511+00 TO 511+39.19 RT | STORM SEWER 12" |
| 14 | 511+39.19 TO 512+11.7 RT | STORM SEWER 12" |
| 13 | 511+56.06 RT TO 512+11.7 RT | STORM SEWER 12" & INLET REMOVAL |
| 44 | 512+07.11 LT TO 512+11.7 RT | STORM SEWER 15" |
| 3 | 512+45 TO 512+45 RT | STORM SEWER 12" |
| 2 | 512+45 TO 512+45 RT | STORM SEWER 12" |
| 29 | 512+45 TO 513+60 RT | STORM SEWER 12" |
| 26 | 513+60 TO 514+80 RT | STORM SEWER 12" |
| 2 | 513+60 TO 513+60 RT | STORM SEWER 12" |
| 11 | 513+60 LT TO 514+24.16 RT | STORM SEWER 12" |
| 1 | 514+80 TO 514+80 RT | STORM SEWER 12" |
| 21 | 514+80 TO 516+00 RT | STORM SEWER 12" |
| 7 | 516+00 LT TO 516+00 RT | STORM SEWER 12" |
| 4 | 519+20 RT TO 519+20 LT | STORM SEWER 12" |
| 2 | 519+20 RT TO 519+20 RT | STORM SEWER 12" |
| 19 | 519+20 TO 520+50 LT | STORM SEWER 12" |
| 5 | 518+97.48 TO 519+31.65 RT | STORM SEWER 10" & INLET REMOVAL |
| 51 | 519+31.65 TO 520+80.50 RT | STORM SEWER 15" & INLET REMOVAL |
| 1 | 520+43.5 TO 520+50 LT | STORM SEWER 12" |
| 1 | 520+43.5 TO 520+50 RT | STORM SEWER 12" |
| 2 | 520+50 TO 520+50 LT | STORM SEWER 12" |
| 2 | 520+50 LT TO 520+50 RT | STORM SEWER 12" |
| 5 | 520+50 TO 520+77.81 LT | STORM SEWER 15" |
| 24 | 520+77.81 TO 520+78 LT | STORM SEWER 27" |
| 178 | 520+77.81 TO 522+44 LT | STORM SEWER 27" |
| 20 | 520+78.97 LT TO 520+80.5 RT | STORM SEWER 21" REMOVAL |
| 168 | 520+80.50 TO 523+36.08 RT | STORM SEWER 21" REMOVAL |
| 123 | 522+44 TO 523+36.09 LT | STORM SEWER 24" |
| 4 | 522+44 LT TO 522+44 LT | STORM SEWER 12" |
| 2 | 522+44 LT TO 522+44 RT | STORM SEWER 12" |
| 14 | 523+36.09 RT TO 523+36.09 LT | STORM SEWER 21" |
| 209 | 523+36.09 TO 525+00 LT | STORM SEWER 24" |
| 178 | 525+00 TO 526+54.8 LT | STORM SEWER 24" |
| 6 | 526+54.8 TO 526+54 RT | STORM SEWER 12" |
| 87 | 526+54.8 TO 527+54 LT | STORM SEWER 21" |
| 1 | 526+81.48 TO 526+86.18 | STORM SEWER 12" |
| 2 | 526+70.89 TO 527+10.69 RT | STORM SEWER 6" & INLET REMOVAL |
| 2 | 527+10.69 RT TO 527+10.69 LT | STORM SEWER 15" & INLET REMOVAL |
| 6 | 527+10.69 TO 531+11.20 RT | STORM SEWER 12" & INLET REMOVAL |
| 1 | 527+47.5 TO 527+54 RT | STORM SEWER 12" |
| 1 | 527+47.5 TO 527+54 LT | STORM SEWER 12" |
| 1 | 527+54 TO 527+54 LT | STORM SEWER 12" |
| 5 | 527+54 LT TO 527+54 RT | STORM SEWER 12" |
| 93 | 527+54 TO 529+50 LT | STORM SEWER 21" |
| 3 | 527+98.65 TO 528+63.79 LT | CULVERT 12" AND 2 FES REMOVAL |
| 9 | 529+50 TO 529+50 RT | STORM SEWER 15" |
| 30 | 529+50 RT TO 529+50 LT | STORM SEWER 15" |
| 2 | 529+50 TO 529+50 LT | STORM SEWER 15" |
| 56 | 529+50 LT TO 531+00 LT | STORM SEWER 18" |
| 69 | 531+00 TO 533+00 LT | STORM SEWER 18" |
| 2 | 531+11.20 TO 532+18.36 RT | STORM SEWER 12" & INLET REMOVAL |
| 2 | 532+18.36 TO 533+17.25 RT | STORM SEWER 12" & INLET REMOVAL |
| 1 | 533+00 TO 533+07.5 LT | STORM SEWER 12" |
| 5 | 533+00 RT TO 533+00 LT | STORM SEWER 12" |
| 1 | 533+00 TO 533+07.5 LT | STORM SEWER 12" |
| 1 | 533+00 TO 533+00 LT | STORM SEWER 12" |
| 16 | 533+00 TO 534+40 LT | STORM SEWER 15" |

| | | |
|---------------------------------|--------------------------------|---------------------------------|
| 3 | 533+17.25 TO 535+10.01 RT | STORM SEWER 12" & INLET REMOVAL |
| 5 | 534+40 RT TO 534+40 LT | STORM SEWER 12" |
| 1 | 534+40 TO 534+40 LT | STORM SEWER 12" |
| 13 | 534+40 TO 535+50 LT | STORM SEWER 15" |
| 2 | 535+50 TO 535+64 LT | STORM SEWER 12" |
| 5 | 535+64 RT TO 535+64 LT | STORM SEWER 12" |
| 2 | 535+10.01 TO 536+10.22 RT | STORM SEWER 12" & INLET REMOVAL |
| 3 | 536+10.22 TO 538+11.69 RT | STORM SEWER 12" & INLET REMOVAL |
| 5 | 539+11.57 LT TO 539+11.57 RT | STORM SEWER 12" |
| 5 | 539+11.57 TO 539+37 RT | STORM SEWER 12" |
| 5 | 539+37 TO 539+75 RT | STORM SEWER 12" |
| 7 | 539+75 RT TO 539+85 CL | STORM SEWER 12" |
| 35 | 539+85 TO 541+50 | STORM SEWER 12" |
| 3 | 541+50 TO 541+50 LT | STORM SEWER 12" |
| 2 | 541+50 TO 541+50 RT | STORM SEWER 12" |
| 107 | 541+50 TO 543+06 RT | STORM SEWER 15" |
| 2 | 543+06 TO 543+06 LT | STORM SEWER 12" |
| 2 | 543+06 TO 543+06 RT | STORM SEWER 12" |
| 109 | 543+06 TO 544+57 | STORM SEWER 15" |
| 8 | 544+16.24 RT TO 544+16.24 | STORM SEWER 12" & INLET REMOVAL |
| 13 | 544+16.24 LT TO 545+35.30 LT | STORM SEWER 10" & INLET REMOVAL |
| 1 | 544+50.5 TO 544+57 LT | STORM SEWER 12" |
| 2 | 544+57 LT TO 544+57 CL | STORM SEWER 12" |
| 1 | 544+50.5 RT TO 544+57 LT | STORM SEWER 12" |
| 2 | 544+57 RT TO 544+57 CL | STORM SEWER 12" |
| 131 | 544+57 TO 546+12.47 | STORM SEWER 18" |
| 19 | 545+35.30 LT TO 547+29.04 LT | STORM SEWER 10" & INLET REMOVAL |
| 30 | 546+12.47 CL TO 546+12.47 RT | STORM SEWER 10" |
| 49 | 546+12.47 TO 546+67.32 | STORM SEWER 18" |
| 57 | 546+13.90 RT TO 549+76.04 RT | STORM SEWER 10" & INLET REMOVAL |
| 2 | 546+67.32 TO 546+67.32 RT | STORM SEWER 12" |
| 35 | 546+67.32 TO 547+69 | STORM SEWER 18" |
| 2 | 547+29.04 LT TO 547+38.77 | STORM SEWER 10" & INLET REMOVAL |
| 4 | 547+38.77 LT TO 547+39.24 RT | STORM SEWER 10" & INLET REMOVAL |
| 2 | 547+39.24 RT TO 547+44.64 RT | STORM SEWER 10" & INLET REMOVAL |
| 2 | 547+59 TO 547+59 RT | STORM SEWER 12" |
| 2 | 547+59 TO 547+59 LT | STORM SEWER 12" |
| 23 | 547+59 TO 548+51 | STORM SEWER 18" |
| 2 | 548+51 TO 548+51 RT | STORM SEWER 12" |
| 23 | 548+51 TO 549+46.51 LT | STORM SEWER 18" |
| 15 | 549+17.35 LT TO 549+16.54 RT | STORM SEWER 10" & INLET REMOVAL |
| 3 | 549+46.51 RT TO 549+49.55 RT | STORM SEWER 24" |
| 9 | 549+46.51 RT TO 549+46.51 RT | STORM SEWER 24" |
| 8 | 549+46.51 TO 549+80 LT | STORM SEWER 30" |
| 5 | 549+49.55 RT TO 549+50.78 RT | STORM SEWER 24" & INLET REMOVAL |
| 7 | 549+50.78 RT TO 549+70.41 RT | STORM SEWER 24" & INLET REMOVAL |
| 4 | 549+80 LT TO 1908+69.55 RT | STORM SEWER 30" |
| 1 | 549+87.29 TO 549+80 LT | STORM SEWER 12" |
| 2,587 | | |
| U.S. ROUTE 34 | | |
| 30 | 1909+70.12 RT TO 1908+99.98 RT | STORM SEWER 24" REMOVAL |
| 13 | 1908+99.98 RT TO 1908+83.19 RT | STORM SEWER 24" & INLET REMOVAL |
| 29 | 1907+59.90 RT TO 1907+30.40 RT | STORM SEWER 24" REMOVAL |
| 31 | 1907+59.90 RT TO 1907+30.40 RT | STORM SEWER 30" |
| 1 | 1910+2.67 RT TO 1910+2.71 RT | STORM SEWER 12" |
| 104 | | |
| GAME FARM ROAD - WATER MAIN | | |
| 105 | 508+38.08 LT TO 511+77.58 LT | WATERMAIN 8" |
| 27 | 511+69.97 RT TO 511+74.95 LT | WATERMAIN 8" |
| 50 | 518+43.73 TO 523+74.80 RT | WATERMAIN 8" |
| 182 | | |
| GAME FARM ROAD - SANITARY SEWER | | |
| 39 | 513+93.67 LT TO 515+11.41 LT | SANITARY SEWER 8" |
| 39 | | |
| 2,912 | TOTAL | |

POROUS GRANULAR BACKFILL

| CU YD | LOCATION |
|----------------|----------------------------|
| GAME FARM ROAD | |
| 37 | STA. 549+10 TO STA. 547+50 |
| 37 | TOTAL |

PLAN SURVEYED BY DATE
 PLOTTED BY DATE
 ALIGNMENT CHECKED BY DATE
 RT. OF WAY CHECKED BY DATE
 CADD FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY DATE
 GRADES CHECKED BY DATE
 B.M. NOTED BY DATE
 STRUCTURE NOTAT'NS CHKD BY DATE

COMPANY NAME: 8/27/2004 4:00:59 PM
 998_schubert

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|----------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 12 |
| STA. TO STA. | | | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 87345

**ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 SCHEDULE OF QUANTITIES**

SCALE: VERT. 1"=10'
 HORIZ. 1"=40'
 DATE: JULY 2014
 DRAWN BY: JMR
 CHECKED BY: GT

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|----------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 13 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 87345

TOPSOIL FURNISH AND PLACE, 4"

| SQ YD | LOCATION |
|----------------------------------|------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 873 | 502+75 TO 507+50.00 |
| 873 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 2,534 | 507+50.00 TO 513+88.00 |
| 4,395 | 513+88.00 TO 521+74.81 |
| 2,114 | 521+74.81 TO 527+45.00 |
| 9,043 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 1,383 | 527+45.00 TO 531+88.11 |
| 1,627 | 531+88.11 TO 539+55.95 |
| 4,392 | 539+55.95 TO 549+92.69 |
| 7,402 | |
| 17,318 | TOTAL |

SEEDING, CLASS 2A

| ACRE | LOCATION |
|----------------------------------|---------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 0.08 | 502+75 TO 507+50.00 RT |
| 0.11 | 502+75 TO 507+50.00 LT |
| 0.19 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 0.25 | 507+50.00 TO 513+88.00 RT |
| 0.28 | 507+50.00 TO 513+88.00 LT |
| 0.52 | 513+88.00 TO 521+74.81 RT |
| 0.39 | 513+88.00 TO 521+74.81 LT |
| 0.27 | 521+74.81 TO 527+45.00 RT |
| 0.17 | 521+74.81 TO 527+45.00 LT |
| 1.88 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 0.09 | 527+45.00 TO 531+88.11 RT |
| 0.20 | 527+45.00 TO 531+88.11 LT |
| 0.13 | 531+88.11 TO 539+55.95 RT |
| 0.21 | 531+88.11 TO 539+55.95 LT |
| 0.54 | 539+55.95 TO 549+92.69 RT |
| 0.37 | 539+55.95 TO 549+92.69 LT |
| 1.54 | |
| 3.61 | TOTAL |

NITROGEN FERTILIZER NUTRIENT

| POUND | LOCATION |
|----------------------------------|---------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 7 | 502+75 TO 507+50.00 RT |
| 10 | 502+75 TO 507+50.00 LT |
| 17 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 23 | 507+50.00 TO 513+88.00 RT |
| 25 | 507+50.00 TO 513+88.00 LT |
| 47 | 513+88.00 TO 521+74.81 RT |
| 35 | 513+88.00 TO 521+74.81 LT |
| 24 | 521+74.81 TO 527+45.00 RT |
| 15 | 521+74.81 TO 527+45.00 LT |
| 169 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 8 | 527+45.00 TO 531+88.11 RT |
| 18 | 527+45.00 TO 531+88.11 LT |
| 12 | 531+88.11 TO 539+55.95 RT |
| 19 | 531+88.11 TO 539+55.95 LT |
| 49 | 539+55.95 TO 549+92.69 RT |
| 33 | 539+55.95 TO 549+92.69 LT |
| 139 | |
| 325 | TOTAL |

PHOSPHORUS FERTILIZER NUTRIENT

| POUND | LOCATION |
|----------------------------------|---------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 7 | 502+75 TO 507+50.00 RT |
| 10 | 502+75 TO 507+50.00 LT |
| 17 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 23 | 507+50.00 TO 513+88.00 RT |
| 25 | 507+50.00 TO 513+88.00 LT |
| 47 | 513+88.00 TO 521+74.81 RT |
| 35 | 513+88.00 TO 521+74.81 LT |
| 24 | 521+74.81 TO 527+45.00 RT |
| 15 | 521+74.81 TO 527+45.00 LT |
| 169 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 8 | 527+45.00 TO 531+88.11 RT |
| 18 | 527+45.00 TO 531+88.11 LT |
| 12 | 531+88.11 TO 539+55.95 RT |
| 19 | 531+88.11 TO 539+55.95 LT |
| 49 | 539+55.95 TO 549+92.69 RT |
| 33 | 539+55.95 TO 549+92.69 LT |
| 139 | |
| 325 | TOTAL |

POTASSIUM FERTILIZER NUTRIENT

| POUND | LOCATION |
|----------------------------------|---------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 7 | 502+75 TO 507+50.00 RT |
| 10 | 502+75 TO 507+50.00 LT |
| 17 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 23 | 507+50.00 TO 513+88.00 RT |
| 25 | 507+50.00 TO 513+88.00 LT |
| 47 | 513+88.00 TO 521+74.81 RT |
| 35 | 513+88.00 TO 521+74.81 LT |
| 24 | 521+74.81 TO 527+45.00 RT |
| 15 | 521+74.81 TO 527+45.00 LT |
| 169 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 8 | 527+45.00 TO 531+88.11 RT |
| 18 | 527+45.00 TO 531+88.11 LT |
| 12 | 531+88.11 TO 539+55.95 RT |
| 19 | 531+88.11 TO 539+55.95 LT |
| 49 | 539+55.95 TO 549+92.69 RT |
| 33 | 539+55.95 TO 549+92.69 LT |
| 139 | |
| 325 | TOTAL |

EROSION CONTROL BLANKET

| SQ YD | LOCATION |
|----------------------------------|---------------------------|
| GAME FARM ROAD | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 180 | 539+55.95 TO 549+92.69 RT |
| 180 | |
| 180 | TOTAL |

TEMPORARY EROSION CONTROL SEEDING

| POUND | LOCATION |
|----------------------------------|---------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 24 | 502+75 TO 507+50.00 RT |
| 33 | 502+75 TO 507+50.00 LT |
| 57 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 75 | 507+50.00 TO 513+88.00 RT |
| 84 | 507+50.00 TO 513+88.00 LT |
| 156 | 513+88.00 TO 521+74.81 RT |
| 117 | 513+88.00 TO 521+74.81 LT |
| 81 | 521+74.81 TO 527+45.00 RT |
| 51 | 521+74.81 TO 527+45.00 LT |
| 564 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 27 | 527+45.00 TO 531+88.11 RT |
| 60 | 527+45.00 TO 531+88.11 LT |
| 39 | 531+88.11 TO 539+55.95 RT |
| 63 | 531+88.11 TO 539+55.95 LT |
| 162 | 539+55.95 TO 549+92.69 RT |
| 111 | 539+55.95 TO 549+92.69 LT |
| 462 | |
| 1083 | TOTAL |

PERIMETER EROSION BARRIER

| FOOT | LOCATION |
|----------------------------------|-----------------------------|
| GAME FARM ROAD | |
| STA. 502+75 TO STA. 507+50.00 | |
| 94 | 502+75 TO 507+50.00 RT |
| 0 | 502+75 TO 507+50.00 LT |
| 94 | |
| STA. 507+50.00 TO STA. 527+45.00 | |
| 0 | 507+50.00 TO 513+88.00 RT |
| 630 | 507+50.00 TO 513+88.00 LT |
| 266 | 513+88.00 TO 521+74.81 RT |
| 674 | 513+88.00 TO 521+74.81 LT |
| 205 | 521+74.81 TO 527+45.00 RT |
| 400 | 521+74.81 TO 527+45.00 LT |
| 2,175 | |
| STA. 527+45.00 TO STA. 549+92.69 | |
| 468 | 527+45.00 TO 531+88.11 RT |
| 532 | 527+45.00 TO 531+88.11 LT |
| 548 | 531+88.11 TO 539+55.95 RT |
| 620 | 531+88.11 TO 539+55.95 LT |
| 146 | 539+55.95 TO 549+92.69 RT |
| 539 | 539+55.95 TO 549+92.69 LT |
| 277 | 1909+10.12 TO 1906+08.62 RT |
| 3,130 | |
| 5,399 | TOTAL |

INLET AND PIPE PROTECTION

| EACH | LOCATION |
|----------------|---------------------------|
| GAME FARM ROAD | |
| 1 | STA. 503+10.50, 37' RT |
| 1 | STA. 503+75.00, 35.5' RT |
| 1 | STA. 504+21.00, 38' RT |
| 1 | STA. 506+80.00, 26' LT |
| 1 | STA. 508+13.30, 53.5' RT |
| 1 | STA. 508+70.95, 60.3' RT |
| 1 | STA. 510+20.00, 52.2' RT |
| 1 | STA. 512+45.00, 51.6' RT |
| 1 | STA. 516+00.00, 34' LT |
| 1 | STA. 519+20.00, 36.2' RT |
| 1 | STA. 539+37.00, 47' RT |
| 1 | STA. 539+75.00, 48' RT |
| 1 | STA. 549+49.55, 51.5' RT |
| 1 | STA. 1908+69.56, 38.4' RT |
| 14 | TOTAL |

| | | | |
|---------------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | CADD FILE NAME | | |
| NOTE BOOK NO. | | | |

| | | | |
|---------------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| NOTE BOOK NO. | | | |

COMPANY NAME: 8/21/2014 4:00:00 PM 098_sch02.dgn

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
SCHEDULE OF QUANTITIES

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: JMR
 CHECKED BY: GT

INLET FILTERS

| EACH | LOCATION |
|----------------|--------------------------|
| GAME FARM ROAD | |
| 1 | STA. 502+85.00, 17.5' RT |
| 1 | STA. 503+75.00, 17.5' RT |
| 1 | STA. 503+75.00, 18.7' LT |
| 1 | STA. 505+25.00, 17.5' RT |
| 1 | STA. 505+25.00, 17.5' LT |
| 1 | STA. 505+31.00, 17.5' RT |
| 1 | STA. 505+31.00, 17.5' LT |
| 1 | STA. 506+80.00, 17.5' RT |
| 1 | STA. 506+80.00, 17.5' LT |
| 1 | STA. 508+15.33, 29.5' RT |
| 1 | STA. 508+19.26, 30' LT |
| 1 | STA. 509+00.00, 29.5' RT |
| 1 | STA. 509+82.00, 25.3' RT |
| 1 | STA. 509+82.00, 17.5' LT |
| 1 | STA. 511+00.00, 17.5' RT |
| 1 | STA. 511+39.19, 19.6' RT |
| 1 | STA. 512+45.00, 17.5' RT |
| 1 | STA. 513+60.00, 17.5' RT |
| 1 | STA. 514+80.00, 17.5' RT |
| 1 | STA. 516+00.00, 17.5' RT |
| 1 | STA. 519+20.00, 17.5' RT |
| 1 | STA. 519+20.00, 17.5' LT |
| 1 | STA. 520+43.50, 17.5' RT |
| 1 | STA. 520+43.50, 17.5' LT |
| 1 | STA. 520+50.00, 17.5' RT |
| 1 | STA. 520+50.00, 17.5' LT |
| 1 | STA. 522+44.00, 17.5' RT |
| 1 | STA. 522+44.00, 29.5' LT |
| 1 | STA. 523+36.09, 18.3' RT |
| 1 | STA. 526+55.00, 17.5' RT |
| 1 | STA. 526+81.50, 80.1' RT |
| 1 | STA. 527+47.50, 17.5' RT |
| 1 | STA. 527+47.50, 18.2' LT |
| 1 | STA. 527+54.00, 17.5' RT |
| 1 | STA. 527+54.00, 17.7' LT |
| 1 | STA. 529+50.00, 17.5' RT |
| 1 | STA. 529+50.00, 17.5' LT |
| 1 | STA. 533+00.00, 17.5' RT |
| 1 | STA. 533+00.00, 17.5' LT |
| 1 | STA. 533+07.50, 17.5' RT |
| 1 | STA. 533+07.50, 17.5' LT |
| 1 | STA. 534+40.00, 17.5' RT |
| 1 | STA. 534+40.00, 17.5' LT |
| 1 | STA. 535+64.00, 17.5' RT |
| 1 | STA. 535+64.00, 17.5' LT |
| 1 | STA. 539+11.50, 17.5' RT |
| 1 | STA. 539+11.50, 17.5' LT |
| 1 | STA. 541+50.00, 17.5' RT |
| 1 | STA. 541+50.00, 17.5' LT |
| 1 | STA. 543+06.00, 17.5' RT |
| 1 | STA. 543+06.00, 17.5' LT |
| 1 | STA. 544+50.50, 17.5' RT |
| 1 | STA. 544+50.50, 17.5' LT |
| 1 | STA. 544+57.00, 17.5' RT |
| 1 | STA. 544+57.00, 17.5' LT |
| 1 | STA. 546+67.32, 17.5' RT |
| 1 | STA. 547+59.00, 17.5' RT |
| 1 | STA. 547+59.00, 17.5' LT |
| 1 | STA. 548+51.00, 17.5' RT |
| 1 | STA. 548+51.00, 17.5' LT |
| 1 | STA. 549+46.51, 22.4' RT |
| 1 | STA. 549+80.00, 28' LT |
| 1 | STA. 549+87.29, 31.3' RT |
| 1 | STA. 1910+03, 24.8' RT |

63 TOTAL

STONE RIPRAP, CLASS A4

| SQ. YD. | LOCATION |
|---------|---|
| 28 | OUTFALL AT HIGH SCHOOL SWALE STA. 1906+40 RT |
| 67 | OUTFALL AT US ROUTE 34 STA. 800+12 LT |

95 TOTAL

FILTER FABRIC

| SQ. YD. | LOCATION |
|---------|---|
| 28 | OUTFALL AT HIGH SCHOOL SWALE STA. 1906+40 RT |
| 67 | OUTFALL AT US ROUTE 34 STA. 800+12 LT |

95 TOTAL

PIPE CULVERT REMOVAL

| FOOT | LOCATION |
|------|--|
| 66 | GAME FARM ROAD STA. 527+98.55, 21.3' LT - 528+63.79, 21.7' LT |

66 TOTAL

STORM SEWER REMOVAL 6"

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 58 | STA. 503+38.95, 26.8' RT - STA. 503+96.27, 34.3' RT |
| 41 | STA. 526+70.89, 29.2' RT - STA. 527+10.69, 20.6' RT |
| 99 | |

| U.S. ROUTE 34 | |
|---------------|---|
| 5 | STA. 1908+69.33, 33.6' RT - STA. 1908+69.61, 38.3' RT |
| 5 | |

104 TOTAL

STORM SEWER REMOVAL 10"

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 34 | STA. 518+97.48, 27.7' RT - STA. 519+31.67, 27.2' RT |
| 193 | STA. 545+35.73, 12.7' LT - STA. 547+29.04, 26.3' LT |
| 12 | STA. 547+29.04, 26.3' LT - STA. 547+38.75, 19.3' RT |
| 30 | STA. 547+38.75, 19.3' RT - STA. 547+39.24, 10.4' RT |
| 10 | STA. 547+39.24, 10.4' RT - STA. 547+44.64, 18.3' RT |

279 TOTAL

STORM SEWER REMOVAL 12"

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 28 | STA. 508+13.29, 53.5' RT - STA. 508+16.19, 26.0' RT |
| 56 | STA. 508+16.19, 26.0' RT - STA. 508+19.26, 29.8' LT |
| 65 | STA. 508+16.19, 26.0' RT - STA. 508+70.95, 60.3' RT |
| 50 | STA. 511+56.06, 38.3' RT - STA. 512+11.69, 34.4' RT |
| 401 | STA. 527+10.69, 20.6' RT - STA. 531+11.20, 19.5' RT |
| 107 | STA. 531+11.20, 19.5' RT - STA. 532+18.36, 19.4' RT |
| 101 | STA. 532+18.36, 19.4' RT - STA. 533+17.25, 23.8' RT |
| 193 | STA. 533+17.27, 23.8' RT - STA. 535+10.01, 26.9' RT |
| 100 | STA. 535+10.01, 26.9' RT - STA. 536+10.22, 26.7' RT |
| 202 | STA. 536+10.22, 26.7' RT - STA. 538+11.69, 26.3' RT |
| 41 | STA. 544+16.24, 28.8' RT - STA. 544+16.25, 12.0' LT |
| 120 | STA. 544+16.25, 12.0' LT - STA. 545+35.73, 12.7' LT |
| 131 | STA. 546+13.90, 30.1' RT - STA. 547+44.64, 18.3' RT |
| 231 | STA. 547+44.64, 18.3' RT - STA. 549+76.04, 17.6' RT |
| 36 | STA. 549+16.54, 17.8' RT - STA. 549+17.34, 18.3' LT |
| 1,862 | |

| U.S. ROUTE 34 | |
|---------------|---|
| 6 | STA. 1908+68.99, 27.7' RT - STA. 1908+69.33, 33.6' RT |
| 6 | |

1,868 TOTAL

STORM SEWER REMOVAL 15"

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 148 | STA. 519+31.67, 27.2' RT - STA. 520+80.51, 22.2' RT |
| 37 | STA. 527+10.69, 20.6' RT - STA. 527+11.68, 16.8' LT |

185 TOTAL

STORM SEWER REMOVAL 18"

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 247 | STA. 520+78.97, 41.8' LT - STA. 520+77.6, 290.3' LT |

247 TOTAL

STORM SEWER REMOVAL 24"

| FOOT | LOCATION |
|----------------|---|
| GAME FARM ROAD | |
| 64 | STA. 520+80.51, 22.2' RT - STA. 520+78.97, 41.8' LT |
| 254 | STA. 520+80.51, 22.2' RT - STA. 523+36.08, 18.1' RT |
| 318 | |

| U.S. ROUTE 34 | |
|---------------|---|
| 11 | STA. 1909+91.57, 46.4' RT - STA. 1909+81.08, 47.9' RT |
| 21 | STA. 1909+81.08, 47.9' RT - STA. 1909+70.12, 30.6' RT |
| 71 | STA. 1909+70.12, 30.6' RT - STA. 1908+99.98, 27.3' RT |
| 31 | STA. 1908+99.98, 27.3' RT - STA. 1908+68.99, 27.7' RT |
| 264 | STA. 1908+68.99, 27.7' RT - STA. 1906+05.13, 34.2' RT |
| 398 | |

716 TOTAL

DUCTILE IRON WATER MAIN 8"

| FOOT | LOCATION |
|----------------|--|
| GAME FARM ROAD | |
| 350 | STA. 508+38.08, 7.3' LT - STA. 511+77.58, 23.3' LT |
| 100 | STA. 511+66.36, 59.6' RT - STA. 511+72.31, 23.8' LT |
| 165 | STA. 518+01.20, 72.0' RT - STA. 518+06.74, 236.5' RT |

615 TOTAL

WATER VALVES 8"

| EACH | LOCATION |
|----------------|--------------------------|
| GAME FARM ROAD | |
| 1 | STA. 511+69.97, 59.8' RT |
| 1 | STA. 511+74.95, 23.5' LT |
| 1 | STA. 511+78.24, 30.2' LT |

3 TOTAL

INSERTING VALVES 8"

| EACH | LOCATION |
|----------------|---------------------------|
| GAME FARM ROAD | |
| 1 | STA. 515+57.52, 338.5' RT |

1 TOTAL

FIRE HYDRANTS TO BE RELOCATED

| EACH | LOCATION |
|----------------|-----------------------------|
| GAME FARM ROAD | |
| 1 | TO STA. 515+46.56, 22.5' RT |
| 1 | TO STA. 518+82.84, 23.5' LT |
| 1 | TO STA. 521+14.97, 24.3' LT |
| 1 | TO STA. 523+74.63, 34.8' LT |

4 TOTAL

FIRE HYDRANTS TO BE REMOVED

| EACH | LOCATION |
|----------------|-----------------------------|
| GAME FARM ROAD | |
| 1 | TO STA. 508+34.32, 24.3' LT |

1 TOTAL

FIRE HYDRANT WITH AUXILIARY VALVE AND VALVE BOX

| EACH | LOCATION |
|----------------|--------------------------|
| GAME FARM ROAD | |
| 1 | STA. 511+62.50, 35.3' LT |

1 TOTAL

DOMESTIC WATER SERVICE BOXES TO BE MOVED

| EACH | LOCATION |
|----------------|--------------------------|
| GAME FARM ROAD | |
| 1 | STA. 525+13.43, 33.2' LT |
| 1 | STA. 543+05.34, 30.5' RT |
| 1 | STA. 545+62.22, 29.4' RT |

3 TOTAL

VALVE VAULTS, TYPE A, 5' DIAMETER, TYPE 1 FRAME, CLOSED LID

| EACH | LOCATION |
|----------------|---------------------------|
| GAME FARM ROAD | |
| 1 | STA. 515+57.52, 338.5' RT |
| 1 | STA. 518+01.20, 72.0' RT |
| 1 | STA. 518+06.74, 236.5' RT |

3 TOTAL

MANHOLES TO BE ADJUSTED

| EACH | LOCATION |
|----------------|--|
| GAME FARM ROAD | |
| 1 | STA. 503+91.3, 7.6' RT (SANITARY SEWER) |
| 1 | STA. 504+31.2, 32.5' LT (STORM SEWER) |
| 1 | STA. 507+88.6, 8.1' RT (SANITARY SEWER) |
| 1 | STA. 508+19.26, 30' LT (STORM SEWER) |
| 1 | STA. 509+70.71, 25.9' LT (STORM SEWER) |
| 1 | STA. 509+79.61, 23.3' LT (STORM SEWER) |
| 1 | STA. 511+80.0, 16.6' RT (SANITARY SEWER) |
| 1 | STA. 512+07.11, 7.1' LT (STORM SEWER) |
| 1 | STA. 513+93.7, 37.9' LT (SANITARY SEWER) |

9 TOTAL

MANHOLES TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE

| EACH | LOCATION |
|----------------|------------------------|
| GAME FARM ROAD | |
| 1 | STA. 508+19.26, 30' LT |

1 TOTAL

| | | |
|------|-----------------------------|------|
| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | NOTE BOOK ALIGNMENT CHECKED | |
| | RT. OF WAY CHECKED | |
| | CADD FILE NAME | |

| | | |
|---------|-----------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | B.M. NOTED | |
| | STRUCTURE NOTATIONS CHECKED | |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|----------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 15 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |

VALVE VAULTS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 503+54.13, 5.3' RT |
| 1 | STA. 504+22.61, 33.9' LT |
| 1 | STA. 525+13.4, 33.2' LT |
| 1 | STA. 526+97.5, 20.3' LT |
| 1 | STA. 534+81.4, 26' RT |

5 TOTAL

REMOVING MANHOLES

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 509+13.99, 39.0' RT |
| 1 | STA. 508+16.17, 26.0' RT |
| 1 | STA. 512+11.69, 34.4' RT |
| 1 | STA. 520+78.98, 41.8' LT |
| 1 | STA. 523+36.08, 18.1' RT |
| 1 | STA. 544+16.24, 28.6' RT |
| 1 | STA. 546+13.90, 30.1' RT |
| 1 | STA. 549+50.78, 40.9' RT |
| 1 | STA. 549+94.68, 63.5' LT |

9 TOTAL

REMOVING INLETS

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 503+72.01, 28.0' RT |
| 1 | STA. 511+56.06, 38.3' RT |
| 1 | STA. 519+31.66, 27.2' RT |
| 1 | STA. 520+80.51, 22.2' RT |
| 1 | STA. 526+70.89, 29.2' RT |
| 1 | STA. 526+86.18, 83.6' RT |
| 1 | STA. 527+10.70, 20.6' RT |
| 1 | STA. 531+11.21, 19.5' RT |
| 1 | STA. 532+18.36, 19.4' RT |
| 1 | STA. 533+17.26, 23.8' RT |
| 1 | STA. 535+10.01, 26.9' RT |
| 1 | STA. 536+10.22, 26.7' RT |
| 1 | STA. 538+11.69, 26.3' RT |
| 1 | STA. 544+16.25, 12.0' LT |
| 1 | STA. 545+35.73, 12.7' LT |
| 1 | STA. 547+29.04, 26.3' LT |
| 1 | STA. 547+38.75, 19.3' LT |
| 1 | STA. 547+39.24, 10.4' RT |
| 1 | STA. 547+44.64, 18.3' RT |
| 1 | STA. 549+16.68, 11.5' RT |
| 1 | STA. 549+17.35, 18.3' LT |
| 1 | STA. 549+70.42, 34.8' RT |
| 1 | STA. 549+71.52, 69.0' RT |
| 1 | STA. 549+92.35, 32.2' LT |

24 TOTAL

REMOVE EXISTING FLARED END SECTION

| EACH | LOCATION |
|------|---------------------------|
| | GAME FARM ROAD |
| 1 | STA. 527+98.55, 21.3' LT |
| 1 | STA. 528+63.79, 21.7' LT |
| | U.S. ROUTE 34 |
| 1 | STA. 1906+05.13, 34.2' RT |

3 TOTAL

PRECAST MODULAR RETAINING WALL

| SQ FT | LOCATION |
|-------|------------------------------------|
| | GAME FARM ROAD |
| 786 | STA. 547+48, RT TO STA. 549+10, RT |

786 TOTAL

TELEVISION INSPECTION OF SEWER

| FOOT | LOCATION |
|-------|---|
| | GAME FARM ROAD |
| 129 | STA. 513+93.7, 37.9' LT - STA. 515+11.41, 22.5' LT (SANITARY) |
| 4,576 | SEE STORM SEWER SCHEDULE |

4705 TOTAL

POLYETHYLENE WRAP, 8"

| FOOT | LOCATION |
|------|--|
| | GAME FARM ROAD |
| 350 | STA. 508+38.08, 7.3' LT - STA. 511+77.58, 23.3' LT |
| 100 | STA. 511+66.36, 59.6' RT - STA. 511+72.31, 23.8' LT |
| 165 | STA. 518+01.20, 72.0' RT - STA. 518+06.74, 236.5' RT |

615 TOTAL

EXPLORATION TRENCH, SPECIAL

| FOOT | LOCATION |
|------|---------------------------|
| | SANITARY SEWER ADJUSTMENT |
| 50 | STA. 509+59.50 |
| 50 | STA. 510+11.50 |
| | SANITARY SEWER CONNECTION |
| 40 | STA. 514+31.60 |
| 40 | STA. 514+61.00 |
| 55 | STA. 515+11.40 |

235 TOTAL

DUCTILE IRON WATER MAIN FITTINGS

| POUND | LOCATION |
|-------|---|
| | GAME FARM ROAD |
| 408 | STA. 508+38.08, 7.3' LT - STA. 518+01.20, 72' RT |
| 540 | STA. 511+73.58, 60.0' RT - STA. 511+77.91, 23.2' LT |

948 TOTAL

ABANDON EXISTING WATER MAIN, FILL WITH CLSM

| FOOT | LOCATION |
|------|---|
| | GAME FARM ROAD |
| 84 | STA. 511+73.58, 60.0' RT - STA. 511+77.91, 23.2' LT |

84 TOTAL

WATER MAIN LINE STOP 8"

| FOOT | LOCATION |
|------|-----------------------|
| | GAME FARM ROAD |
| 1 | STA. 511+72.6, 29' LT |
| 1 | STA. 511+65.9, 65' RT |

2 TOTAL

WATER SERVICE CONNECTION (SHORT)

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 509+16.60, 33.7' LT |
| 1 | STA. 509+88.60, 33.7' LT |
| 1 | STA. 510+70.90, 32.8' LT |

3 TOTAL

CUT AND CAP EXISTING 8" WATER MAIN

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 511+73.58, 60.0' RT |
| 1 | STA. 511+77.91, 23.2' LT |

2 TOTAL

CONNECTION TO EXISTING WATER MAIN 8"

| EACH | LOCATION |
|------|-------------------------|
| | GAME FARM ROAD |
| 1 | STA. 508+38.08, 7.3' LT |

1 TOTAL

MANHOLES TO BE ADJUSTED WITH FRAME AND GRATE (SPECIAL)

| EACH | LOCATION |
|------|-------------------------|
| | GAME FARM ROAD |
| 1 | STA. 504+31.2, 32.5' LT |

1 TOTAL

SANITARY MANHOLES TO BE RECONSTRUCTED

| EACH | LOCATION |
|------|-------------------------|
| | GAME FARM ROAD |
| 1 | STA. 513+93.7, 37.9' LT |

1 TOTAL

SANITARY SERVICE CONNECTION

| EACH | LOCATION |
|------|-------------------------|
| | GAME FARM ROAD |
| 1 | STA. 514+31.6, 28.0' LT |
| 1 | STA. 514+61.0, 23.7' LT |
| 1 | STA. 515+11.4, 22.5' LT |

3 TOTAL

RELOCATE SIGN AND BASE

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 524+42.37, 43.8' LT |

1 TOTAL

SANITARY SERVICE TO BE ADJUSTED

| EACH | LOCATION |
|------|-------------------------|
| | GAME FARM ROAD |
| 1 | STA. 509+59.5, 12.6' RT |
| 1 | STA. 510+11.5, 14.0' RT |

2 TOTAL

TREE PROTECTION, SPECIAL

| L SUM | LOCATION |
|-------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 515+44.93, 29.9' LT |

1 TOTAL

SANITARY MANHOLE, 4'-DIAMETER

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 515+11.41, 22.5' LT |

1 TOTAL

FENCE (SPECIAL)

| FOOT | LOCATION |
|------|------------------------------------|
| | GAME FARM ROAD |
| 162 | STA. 547+48, RT TO STA. 549+10, RT |

162 TOTAL

TEMPORARY FENCE, 6'-FOOT CHAIN LINK

| FOOT | LOCATION |
|------|---|
| | MOT STAGE 1A |
| 53 | STA. 527+45, 54' LT TO STA. 527+98, 54' LT |
| 58 | STA. 528+55, 54' LT TO STA. 529+12.63, 54' LT |
| | MOT STAGE 2A |
| 117 | STA. 507+51, 34' RT TO STA. 508+52.59, 49' RT |
| 271 | STA. 508+70.45, 49' RT TO STA. 511+41.54, 49' RT |
| 680 | STA. 512+14, 47.45' RT TO STA. 519+50, 34.2' RT |
| 178 | STA. 522+09, 54' LT TO STA. 523+84.62, 54' LT |
| 187 | STA. 524+05.62, 54' LT TO STA. 525+91.88, 54' LT |
| 139 | STA. 526+06.23, 54' LT TO STA. 527+45, 54' LT |
| | MOT STAGE 3 |
| 391 | STA. 504+33.45, 34.9' LT TO STA. 507+65.6, 33.4' LT |

2,074 TOTAL

FENCE REMOVAL

| FOOT | LOCATION |
|------|--|
| | GAME FARM ROAD |
| 471 | STA. 516+15.34, RT TO STA. 520+62.11, RT |
| 314 | STA. 529+05.00, RT TO STA. 531+74.49, RT |

785 TOTAL

PRESSURE CONNECTION 6" X 6"

| EACH | LOCATION |
|------|----------------|
| | WATER MAIN |
| 1 | STA. 301+64.66 |

1 TOTAL

PRESSURE CONNECTION 8" X 8"

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 511+73.58, 60.0' RT |
| 1 | STA. 511+77.91, 23.2' LT |

2 TOTAL

PRESSURE CONNECTION 12" X 8"

| EACH | LOCATION |
|------|--------------------------|
| | GAME FARM ROAD |
| 1 | STA. 518+01.20, 72.0' RT |

1 TOTAL

SANITARY SEWER 8"

| FOOT | LOCATION |
|------|--|
| | GAME FARM ROAD |
| 129 | STA. 513+93.7, 37.9' LT - STA. 515+11.41, 22.5' LT |

129 TOTAL

STONE MASONRY WALL

| FOOT | LOCATION |
|------|--|
| | GAME FARM ROAD |
| 34 | STA. 515+30.17, 30.8' LT TO STA. 515+59.16, 30.6' LT |

34 TOTAL

| | | | |
|------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | NO. _____ | | |
| | CADD FILE NAME | | |

| | | | |
|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | GRADES CHECKED | | |
| | STRUCTURE NOTATIONS CHECKED | | |
| | NO. _____ | | |

COMPANY NAME: 8/21/2014 4:08:18 PM 098_sch04.dgn

**ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
SCHEDULE OF QUANTITIES**

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: JMR
CHECKED BY: GT

DRIVEWAY SCHEDULE

| | | | | |
|---------------------|----------------|----------|------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 16 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |

| DRIVEWAY LOCATION | SUBBASE GRANULAR MATERIAL TYPE B 8" (TON) | SUBBASE GRANULAR MATERIAL TYPE B 10" (TON) | AGGREGATE SURFACE COURSE TYPE B 10" (CU YD) | HMA DRIVEWAY PAVEMENT 3 INCH (SQ YD) | HMA DRIVEWAY PAVEMENT 4 INCH (SQ YD) | PCC DRIVEWAY PAVEMENT 6 INCH (SQ YD) | DRIVEWAY PAVEMENT REMOVAL (SQ YD) | TEMPORARY ACCESS PRIVATE ENTRANCE (EACH) | TEMPORARY ACCESS COMMERCIAL ENTRANCE (EACH) | TEMPORARY ACCESS ROAD (EACH) |
|---------------------------------|---|--|---|--------------------------------------|--------------------------------------|--------------------------------------|-----------------------------------|--|---|------------------------------|
| Game Farm Road/ Somonauk Street | | | | | | | | | | |
| 503+91 RT - PE | 22 | - | - | 47.6 | - | - | - | 1 | - | - |
| 505+65 RT - PE | 7 | - | - | 14.2 | - | - | - | 1 | - | - |
| 506+16 RT - PE | 10 | - | - | 21.4 | - | - | - | 1 | - | - |
| 510+00 LT - PE | 8 | - | - | - | - | 17.4 | 60.2 | 1 | - | - |
| 510+27 LT - PE | 9 | - | - | 20.1 | - | - | 62.2 | 1 | - | - |
| 511+78 RT - PE | - | - | - | - | - | - | 177.2 | 1 | - | - |
| 513+88.00 (Somonauk Street) | | | | | | | | | | |
| 514+85 LT - PE | 16 | - | - | 35.9 | - | - | 97.0 | 1 | - | 1 |
| 515+84 LT - PE | 11 | - | - | 24.8 | - | - | 66.6 | 1 | - | - |
| 517+93 LT - CE | - | 16 | 20 | - | 56.0 | - | 45.0 | - | 1 | - |
| 518+07 RT - CE | 21 | - | - | - | 45.5 | - | - | - | - | - |
| 519+68 RT - PE | - | - | - | - | - | - | 202.4 | 1 | - | - |
| 519+75 LT - CE | - | 38 | 20 | - | 136.0 | - | 130.0 | - | 1 | - |
| 521+75 LT - PE | - | - | - | - | - | - | 300.2 | 1 | - | - |
| 523+02 RT - PE | - | - | - | - | - | - | 123.1 | 1 | - | - |
| 525+36 RT - PE | - | - | - | - | - | - | 133.3 | 1 | - | - |
| 526+58 RT - PE | - | - | - | - | - | - | 236.0 | 1 | - | - |
| 528+35 LT - PE | - | - | - | - | - | - | 307.1 | 1 | - | - |
| 531+88 RT - CE | - | - | - | - | - | - | 146.4 | - | 1 | - |
| 533+25 LT - PE | 11 | - | - | 24.7 | - | - | 68.5 | 1 | - | - |
| 534+92 RT - PE | 19 | - | - | - | - | 41.3 | 32.0 | 1 | - | - |
| 535+23 LT - CE | - | 45 | - | 79.0 | - | - | 142.4 | - | 1 | - |
| 535+83 RT - PE | 15 | - | - | - | - | 33.6 | 27.4 | 1 | - | - |
| 535+95 LT - PE | 20 | - | - | 42.8 | - | - | 84.0 | 1 | - | - |
| 537+05 RT - PE | 16 | - | - | - | - | 34.3 | 25.2 | 1 | - | - |
| 537+19 LT - PE | 10 | - | - | 22.6 | - | - | 65.1 | 1 | - | - |
| 537+46 RT - PE | 16 | - | - | - | - | 34.0 | 20.9 | 1 | - | - |
| 538+09 LT - PE | 110 | - | - | 241.0 | - | - | 280.4 | 1 | - | - |
| 539+37 LT - PE | 10 | - | - | 22.3 | - | - | 28.8 | 1 | - | - |
| 540+72 RT - PE | 17 | - | - | - | - | 37.4 | 27.5 | 1 | - | - |
| 541+04 RT - PE | 8 | - | - | 16.6 | - | - | 13.5 | 1 | - | - |
| 541+83 RT - PE | 20 | - | - | - | - | 44.3 | 36.0 | 1 | - | - |
| 542+13 LT - PE | 5 | - | - | - | - | 11.6 | 56.6 | 1 | - | - |
| 542+89 RT - PE | 21 | - | - | 46.8 | - | - | 38.1 | 1 | - | - |
| 543+48 RT - PE | 17 | - | - | - | - | 37.3 | 23.0 | 1 | - | - |
| 544+45 RT - PE | 15 | - | - | - | - | 33.5 | 21.9 | 1 | - | - |
| 544+52 LT - PE | 10 | - | - | 22.6 | - | - | 39.1 | 1 | - | - |
| 545+47 LT - PE | 12 | - | - | 26.6 | - | - | 39.0 | 1 | - | - |
| 545+96 RT - CE | - | 24 | - | 42.6 | - | - | 32.6 | - | 1 | - |
| 546+41 RT CE | 169 | - | - | - | 371.6 | - | - | - | - | - |
| 547+67 RT CE | - | - | - | - | - | - | 208.5 | - | 1 | - |
| U.S. Route 34 | | | | | | | | | | |
| 1907+45 RT - PE | 41 | - | - | - | - | 90.8 | 91.3 | 1 | - | - |
| SUBTOTAL - DRIVEWAYS | 667 | 123 | 40 | 752 | 609 | 595 | 3,489 | 33 | 6 | 1 |

PLAN SURVEYED PLOTTED DATE
 NOTE BOOK ALIGNMENT CHECKED RT. OF WAY CHECKED
 NO. CADD FILE NAME

PROFILE SURVEYED PLOTTED DATE
 NOTE BOOK GRADES CHECKED B.M. NOTED
 NO. STRUCTURE NOTATIONS CHKD

COMPANY NAME: 8/20/2014 4:02:27 PM
 PSE: acn0505gn

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 DRIVEWAY SCHEDULE

SCALE: VERT. HORIZ.
 DATE: JULY 2014

DRAWN BY: JMR
 CHECKED BY: GT

SIDEWALK SCHEDULE

| LOCATION | SUBBASE GRANULAR MATERIAL TYPE B (8") (TON) | SUBBASE GRANULAR MATERIAL TYPE B (2") (TON) | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (2") (TON) | PORTLAND CEMENT CONCRETE SIDEWALK 5" (SQ FT) | DETECTABLE WARNINGS (SQ FT) | SIDEWALK REMOVAL (SQ FT) | |
|--|---|---|---|--|-----------------------------|--------------------------|-------|
| | | | | | | LEFT | RIGHT |
| GAME FARM ROAD/SOMONAUK STREET | | | | | | | |
| STA. 502+75.00 TO STA. 507+50.00 | | | | | | | |
| 502+75.00 TO 507+50.00 | 156 | 5 | 39 | 321 | 78 | 1,413 | |
| SUB-TOTAL | 156 | 5 | 39 | 321 | 78 | 1,413 | |
| STA. 507+50.00 TO STA. 527+45.00 | | | | | | | |
| 507+50 TO 513+88.00 | 294 | 37 | 73 | 2,856 | 139 | 757 | 1,005 |
| 513+88.00 TO 521+74.81 | 276 | 50 | 68 | 3,943 | 119 | | 753 |
| 521+74.81 TO 527+45.00 | 167 | 48 | 41 | 3,732 | 246 | 1,408 | 3,282 |
| SUB-TOTAL | 737 | 135 | 182 | 10,531 | 504 | 7,205 | |
| STA. 527+45.00 TO STA. 549+92.69.00 | | | | | | | |
| 527+45.00 TO 531+88.11 | 169 | 1 | 42 | 81 | 22 | | 1,696 |
| 531+88.11 TO 539+55.95 | 260 | 2 | 64 | 102 | 44 | | 2,827 |
| 539+55.95 TO 549+92.69 | 337 | 3 | 83 | 196 | 87 | 38 | 2,425 |
| SUB-TOTAL | 766 | 6 | 189 | 379 | 153 | 6,986 | |
| TOTAL | 1659 | 146 | 410 | 11,231 | 735 | 15,604 | |

EARTHWORK SCHEDULE

| LOCATION | EARTH EXCAVATION (CU YD) | EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (25%) (CU YD) | EMBANKMENT (CU YD) | EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD) |
|--|--------------------------|---|--------------------|---|
| STAGE I - STA. 527+45.00 TO STA. 549+92.69 | 8344 | 6258 | 197 | 6061 |
| STAGE II - STA. 507+50.00 TO STA. 527+45.00 | 8,829 | 6622 | 622 | 6000 |
| STAGE II - SOMONAUK STREET | 51 | 38 | 40 | -2 |
| STAGE III - 502+75 TO 507+50.00 | 1,759 | 1319 | 7 | 1312 |
| COLTON STREET | 38 | 29 | 0 | 29 |
| TOWER LANE | 52 | 39 | 0 | 39 |
| CHURCH STREET | 56 | 42 | 0 | 42 |
| WEST LANE | 0 | 0 | 0 | 0 |
| PLEASURE DRIVE | 57 | 43 | 0 | 43 |
| OUTFALL (ROUTE 34) | 23 | 17 | 0 | 17 |
| TOTAL | 19,209 | 14,407 | 866 | 13,541 |

SHRINKAGE FACTORS
EARTH EXCAVATION 25%

NOTE:
CUTS AND FILLS HAVE BEEN ADJUSTED FOR TOPSOIL PLACEMENT.

PLAN SURVEYED PLOTTED
NOTE BOOK ALIGNMENT CHECKED
NO. _____ RT. OF WAY CHECKED
CADD FILE NAME _____

PROFILE SURVEYED PLOTTED
NOTE BOOK GRADES CHECKED
NO. _____ B.M. NOTED
STRUCTURE NOTATIONS CHKD _____

PAVEMENT MARKING SCHEDULE

| LOCATION | SHORT-TERM PAVEMENT MARKING MEDIAN LINE YELLOW (FOOT) | LETTERS AND SYMBOLS (SQ FT) | TEMPORARY PAVEMENT MARKING | | | | | | | | WORK ZONE PAVEMENT MARKING REMOVAL (SQ FT) | THERMOPLASTIC PAVEMENT MARKING | | | | | | | | RAISED REFLECTIVE PAVEMENT MARKER (EACH) | PAVEMENT MARKING REMOVAL (SQ FT) | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL (EACH) | |
|--|---|--------------------------------|---|--|---------------------------------|-----------------------|------------------------------------|---|--|--|---|---|--|---------------------------------|-----------------------|------------------------------------|---|--|--|---|-------------------------------------|---|----------|
| | | | LINE - 4" MEDIAN LINE SOLID YELLOW (FOOT) | LINE - 6" CROSSWALK SOLID WHITE (FOOT) | LINE - 8" LANE LINE | | | LINE - 12" CROSSWALK SOLID WHITE (FOOT) | LINE - 24" DIAGONALS SOLID YELLOW (FOOT) | LINE - 24" STOP BAR WHITE (FOOT) | | LINE - 4" MEDIAN LINE SOLID YELLOW (FOOT) | LINE - 6" CROSSWALK SOLID WHITE (FOOT) | LINE - 8" LANE LINE | | | LINE - 12" CROSSWALK SOLID WHITE (FOOT) | LINE - 24" DIAGONALS SOLID YELLOW (FOOT) | LINE - 24" STOP BAR WHITE (FOOT) | | | | |
| | | | | | 6'-2" SKIP-DASH WHITE (FOOT) | SOLID WHITE (FOOT) | 30'-10" SKIP-DASH YELLOW (FOOT) | | | | | | | 6'-2" SKIP-DASH WHITE (FOOT) | SOLID WHITE (FOOT) | 30'-10" SKIP-DASH YELLOW (FOOT) | | | | | | | |
| STA. 502+75.00 TO STA. 507+50.00 | | | | | | | | | | | | | | | | | | | | | | | |
| 502+75 TO 507+50.00 | 100 | 120 | 1,462 | 491 | 32 | 105 | 80 | | 76 | 27 | 34 | 120 | 1,462 | 243 | 32 | 105 | 80 | | 76 | 27 | | 66 | |
| SUB-TOTAL | 100 | 120 | 1,462 | 243 | 32 | 105 | 105 | | 80 | 0 | 76 | 120 | 1,462 | 243 | 32 | 105 | 80 | | 76 | 27 | | 66 | |
| STA. 507+50.00 TO STA. 527+45.00 | | | | | | | | | | | | | | | | | | | | | | | |
| 507+50 TO 513+88.00 | 128 | 78 | 1,380 | 341 | 31 | 31 | 417 | | | 88 | 43 | 78 | 1,380 | 341 | 31 | 417 | | | 88 | 37 | | | |
| 513+88.00 TO 521+74.81 | 158 | 140 | 1,971 | 219 | 57 | 57 | 150 | | 182 | 66 | 53 | 140 | 1,971 | 219 | 57 | 150 | 182 | | 66 | 40 | | | |
| 521+74.81 TO 527+45.00 | 114 | 109 | 1,130 | 453 | 36 | 33 | 372 | | 238 | 23 | 38 | 109 | 1,130 | 453 | 33 | 372 | 238 | 320 | 23 | 26 | | | |
| SUB-TOTAL | 400 | 327 | 4,481 | 1,013 | 124 | 121 | 939 | | 420 | 177 | 134 | 327 | 4,481 | 1,013 | 121 | 939 | 420 | 320 | 177 | 103 | | | |
| STA. 527+45.00 TO STA. 549+92.69.00 | | | | | | | | | | | | | | | | | | | | | | | |
| 527+45.00 TO 531+88.11 | 88 | 89 | 941 | 34 | 3 | 3 | | | 182 | | 29 | 89 | 941 | 34 | 3 | | 182 | | 21 | 39 | | | |
| 531+88.11 TO 539+55.95 | 154 | 125 | 1,805 | 70 | 30 | 29 | 115 | | 208 | 54 | 51 | 125 | 1,805 | 70 | 29 | 115 | 208 | | 54 | 12 | | | |
| 539+55.95 TO 549+92.69 | 208 | 218 | 2,568 | 49 | 96 | 63 | 321 | | 187 | 32 | 66 | 218 | 2,568 | 104 | 63 | 321 | 187 | 32 | 66 | 47 | | | |
| SUB-TOTAL | 450 | 432 | 5,314 | 208 | 208 | 95 | 436 | | 577 | 32 | 141 | 432 | 5,314 | 208 | 95 | 436 | 577 | 32 | 141 | 98 | | | |
| U.S. ROUTE 34 | | | | | | | | | | | | | | | | | | | | | | | |
| | | | 954 | 75 | | | | | | | | 32 | 954 | 76 | 33 | 150 | | 100 | 30 | 40 | 15 | 628 | 8 |
| TOTAL | 950 | 879 | 12,211 | 1,539 | | 2,165 | | | 1,109 | | 394 | 911 | 12,211 | 1,540 | | 2,988 | | 876 | | 268 | 15 | 694 | 8 |

NOTE:
TEMPORARY PAINT PAVEMENT MARKING SHALL BE APPLIED UPON COMPLETION OF THE BINDER COURSE. IT SHALL MATCH THE PERMANENT PAVEMENT MARKING EXACTLY.

PAVEMENT SCHEDULE

| LOCATION | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU YD) | GEOTECHNICAL FABRIC FOR GROUND STABILIZATION (SQ YD) | AGGREGATE SUBGRADE IMPROVEMENT (CU YD) | AGGREGATE SUBGRADE IMPROVEMENT 12" (SQ YD) | SUBBASE GRANULAR MATERIAL TYPE B (TON) | HOT-MIX ASPHALT BASE COURSE, 5 1/2" (SQ YD) | BITUMINOUS MATERIALS (PRIME COAT) (POUND) | AGGREGATE (PRIME COAT) (TON) | HOT-MIX ASPHALT BINDER COURSE, IL-19.0 FG, N50 (TON) | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (TON) | AGGREGATE SHOULDERS TYPE B 6" (SQ YD) | PAVEMENT REMOVAL (SQ YD) | COMBINATION CURB AND GUTTER REMOVAL (FOOT) | | COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.12 (FOOT) | | COMBINATION CONCRETE CURB AND GUTTER TYPE B-6.24 (FOOT) | | JOINT SEALANT (SPECIAL) (FOOT) |
|---|--|---|---|---|---|--|--|---------------------------------|---|---|--|-----------------------------|---|------------|---|---------------|---|-------|-----------------------------------|
| | | | | | | | | | | | | | LEFT | RIGHT | LEFT | RIGHT | LEFT | RIGHT | |
| STA. 502+75 TO STA. 507+50.00 | | | | | | | | | | | | | | | | | | | |
| 502+75 TO 507+50.00 | 413 | 2,475 | 413 | 2,475 | | 2,141 | 7,224 | 5 | 270 | 180 | | 1,620 | | | | | 511 | 501 | 1,012 |
| SUB-TOTAL | 413 | 2,475 | 413 | 2,475 | | 2,141 | 7,224 | 5 | 270 | 180 | | 1,620 | 0 | 0 | 0 | 0 | 511 | 501 | 1,012 |
| STA. 507+50.00 TO STA. 527+45.00 | | | | | | | | | | | | | | | | | | | |
| 507+50 TO 513+88.00 | 868 | 2,604 | 868 | 3,692 | | 3,256 | 10,987 | 7 | 411 | 274 | | 1,927 | | | | | 707 | 656 | 1,363 |
| 513+88.00 TO 521+74.81 | 237 | 1,419 | 237 | 3,990 | | 3,452 | 11,648 | 7 | 435 | 290 | | 1,950 | 38 | 70 | 37 | | 1,012 | 817 | 1,866 |
| 521+74.81 TO 527+45.00 | 969 | 4,028 | 969 | 4,028 | | 3,599 | 12,146 | 8 | 454 | 303 | | 1,432 | 41 | 233 | | 237 | 607 | 544 | 1,388 |
| SUB-TOTAL | 2,074 | 8,051 | 2,074 | 11,710 | | 10,307 | 34,781 | 22 | 1,300 | 867 | | 5,309 | 79 | 303 | 37 | 237 | 2,326 | 2,017 | 4,617 |
| STA. 527+45.00 TO STA. 549+92.69.00 | | | | | | | | | | | | | | | | | | | |
| 527+45.00 TO 531+88.11 | 468 | 2,446 | 468 | 2,446 | | 2,119 | 7,152 | 5 | 267 | 178 | | 1,099 | 57 | 23 | | | 533 | 459 | 992 |
| 531+88.11 TO 539+55.95 | 781 | 3,533 | 781 | 3,749 | | 3,126 | 10,551 | 7 | 394 | 263 | | 2,022 | | 24 | | | 766 | 801 | 1,567 |
| 539+55.95 TO 549+92.69 | 760 | 2,278 | 760 | 5,020 | | 4,295 | 14,493 | 9 | 542 | 361 | | 3,069 | 606 | 689 | | 39 | 1,069 | 1,110 | 2,218 |
| SUB-TOTAL | 2,009 | 8,257 | 2,009 | 11,215 | | 9,540 | 32,196 | 21 | 1,203 | 802 | | 6,190 | 663 | 736 | | 39 | 2,368 | 2,370 | 4,777 |
| SOMONAUK STREET (100+84.17 TO 102+33.48) | | | | | | | | | | | | | | | | | | | |
| | | 398 | | | 227 | | 1,261 | 1 | 47 | 32 | 55 | 538 | | | | | | | |
| TOTAL | 4,496 | 19,181 | 4,496 | 25,400 | 227 | 21,988 | 74,201 | 49 | 2,820 | 1,881 | 55 | 13,657 | 1,781 | 313 | 10,093 | 10,406 | | | |

PLAN SURVEYED PLOTTED CHECKED
NOTE BOOK ALIGNMENT CHECKED
NO. OF WAY CHECKED
CADD FILE NAME

PROFILE SURVEYED PLOTTED CHECKED
NOTE BOOK GRADES CHECKED
B.M. NOTED
STRUCTURE NOTATIONS CHKD

SIGN SCHEDULE

| EXISTING LOCATION | SIGN PLACEMENT | | STANDARD NUMBER | SIGN PANEL TYPE 1 (SQ FT) | METAL POST TYPE A (FOOT) | RELOCATE GROUND-MOUNTED SIGN SUPPORT (EACH) | RELOCATE SIGN PANEL ASSEMBLY TYPE A (EACH) | REMOVE SIGN COMPLETE (EACH) |
|-------------------|-----------------------|-------------------------------|--------------------------------|------------------------------|-----------------------------|--|---|--------------------------------|
| | PROPOSED LOCATION | HEIGHT ABOVE EDGE OF PAVEMENT | | | | | | |
| GAME FARM ROAD | | | | | | | | |
| STA. 502+85 | 502+85, 22' RT | 7'-0" | S1-1-3636, W16-9p-2412 | | | 1 | | |
| STA. 503+48 | 503+49, 40' RT | 7'-0" | R1-1-3030 | | | 1 | | |
| STA. 503+50 | 503+53, 33' RT | 7'-0" | D3-1 | | | 1 | | |
| STA. 503+82 | 503+86, 39' LT | 7'-0" | R1-1-3030 | | | 1 | | |
| STA. 504+61 | 504+61, 23' RT | 7'-0" | S4-I100-2448 | | | 1 | | |
| STA. 504+84 | 504+84, 23' LT | 7'-0" | R10-7-2430 | | | 1 | | |
| STA. 505+40 | 505+42, 21.5' RT | 7'-0" | PATRIOT'S WAY | | | 1 | | |
| STA. 507+03 | | 7'-0" | R7-1-1218 | | | | | 1 |
| STA. 507+39 | 507+50, 23.5' RT | 7'-0" | S1-1-3636, W16-7p-2412 | | | 1 | | |
| STA. 507+49 | 507+50, 23.5' LT | 7'-0" | S1-1-3636, W16-7p-2412 | | | 1 | | |
| STA. 507+77 | 507+76, 41' LT | 7'-0" | R1-1-3030 | | | 1 | | |
| STA. 508+20 | 508+33, 27' LT | 7'-0" | D3-1 | | | 1 | | |
| STA. 508+24 | 508+21, 35.5' RT | 7'-0" | R7-1-1218 | | 12.0 | | 1.0 | |
| STA. 509+66 | 510+10, 23.5' RT | 7'-0" | W1-2-3030 | | | 1 | | |
| STA. 510+13 | 509+66, 35.5 RT | 7'-0" | R7-1-1218 | | 12.0 | | 1.0 | |
| STA. 511+70 | 511+69, 35' LT | 7'-0" | R1-1-3030 | | | 1 | | |
| STA. 512+03 | 512+15, 24' LT | 7'-0" | D3-1 | | | 1 | | |
| STA. 512+06 | 512+29, 24' RT | 7'-0" | R6-2-2430 | | | 1 | | |
| STA. 512+68 | 513+71, 36.5' LT | 7'-0" | R1-1-3030 | | | 1 | | |
| STA. 512+94 | 512+95, 23.5' RT | 7'-0" | R7-1-1218 | | | 1 | | |
| STA. 513+35 | 513+58, 24' LT | 7'-0" | D3-1 | | | 1 | | |
| STA. 515+28 | 515+28, 23.5' LT | 7'-0" | R7-1-1218 | | | 1 | | |
| STA. 515+80 | | 7'-0" | R7-1-1218 | | | | | 1 |
| STA. 517+51 | STA. 517+51, 23.5' LT | 7'-0" | R7-1-1218 | | | 1 | | |
| STA. 518+05 | STA. 518+05, 23.5 RT | 7'-0" | R7-1-1218 | | | 1 | | |
| STA. 519+41 | 519+41, 23.5 LT | 7'-0" | W1-2-3030 | | | 1 | | |
| STA. 519+50 | 519+45, 39.5 RT | 7'-0" | R5-1-3030, R1-1-3030 | | | 1 | | |
| STA. 519+85 | 520+00, 39.5 RT | 7'-0" | R5-1-3030, R1-1-3030 | | | 1 | | |
| STA. 520+63 | STA. 520+62, 23 LT | 7'-0" | | | | 1 | | |
| STA. 522+09 | 522+31, 36.5 LT | 7'-0" | R6-2-2430 | | | 1 | | |
| STA. 522+57 | 522+57, 23.5 RT | 7'-0" | R6-2-2430 | | | 1 | | |
| STA. 523+32 | 523+31, 24.5' RT | 7'-0" | S1-1-3636, W16-7p-2412 | | | 1 | | |
| STA. 523+32 | 523+31, 35.5' LT | 7'-0" | S1-1-3636, W16-7p-2412 | | | 1 | | |
| STA. 524+31 | 524+31, 35.5' LT | 7'-0" | PATRIOT'S WAY | | | 1 | | |
| STA. 526+75 | 527+25, 39.5 RT | 7'-0" | R1-1-3030 | | | 1 | | |
| | 527+44, 23.5' RT | 7'-0" | S1-1-3636, W16-7p-2412 | 2.0 | 13.5 | | | |
| | 527+44, 23.5' LT | 7'-0" | S1-1-3636, W16-7p-2412 | 2.0 | 13.5 | | | |
| STA. 528+16 | 527+97, 42' LT | 7'-0" | R5-1-3030, R1-1-3030 | | | 1 | | |
| STA. 528+48 | 528+50, 42' LT | 7'-0" | R5-1-3030, R1-1-3030 | | | 1 | | |
| STA. 529+39 | 529+39, 24 LT | 7'-0" | S4-I100-2448 | | | 1 | | |
| STA. 531+29 | 531+29, 23.5 LT | 7'-0" | S1-1-3636, W16-9p-2412 | | | 1 | | |
| STA. 533+09 | 533+10, 23.5 LT | 7'-0" | D3-1 | | | 1 | | |
| STA. 533+78 | 533+78, 23.5 RT | 7'-0" | R2-1 | | | 1 | | |
| STA. 535+00 | 534+96, 24.5 LT | 7'-0" | D3-1 | | | 1 | | |
| STA. 535+00 | 534+92, 24.5 LT | 7'-0" | BRISTOL -KENDALL FIRE DISTRICT | | | 1 | | |
| STA. 535+75 | 535+75, 23.5' LT | 7'-0" | BRISTOL -KENDALL FIRE DISTRICT | | | 1 | | |
| STA. 536+87 | 536+87, 23.5' LT | 7'-0" | BRISTOL -KENDALL FIRE DISTRICT | | | 1 | | |
| STA. 537+40 | 537+40, 23.5' LT | 7'-0" | BRISTOL -KENDALL FIRE DISTRICT | | | 1 | | |
| STA. 537+49 | 537+49, 23.5' LT | 7'-0" | R2-1 | | | 1 | | |
| STA. 539+74 | 539+75, 40' RT | 7'-0" | R1-1-3030 | | | 1 | | |
| STA. 544+39 | 544+35, 23.5' LT | 7'-0" | BRISTOL -KENDALL FIRE DISTRICT | | | 1 | | |
| STA. 546+75 | 546+75, 23.5' LT | 7'-0" | R2-1 | | | 1 | | |
| STA. 548+49 | 546+21, 23.5' RT | 7'-0" | "NO THROUGH TRAFFIC" | | | 1 | | |
| TOTAL | | | | 4.0 | 51.0 | 47 | 2 | 2 |

* SEE PAVEMENT MARKING PLAN FOR LOCATIONS.

PLAN SURVEYED PLOTTED
NOTE BOOK ALIGNMENT CHECKED
NO. OF WAY CHECKED
CADD FILE NAME

PROFILE SURVEYED PLOTTED
NOTE BOOK GRADES CHECKED
B.M. NOTED
STRUCTURE NOTATIONS CHKO

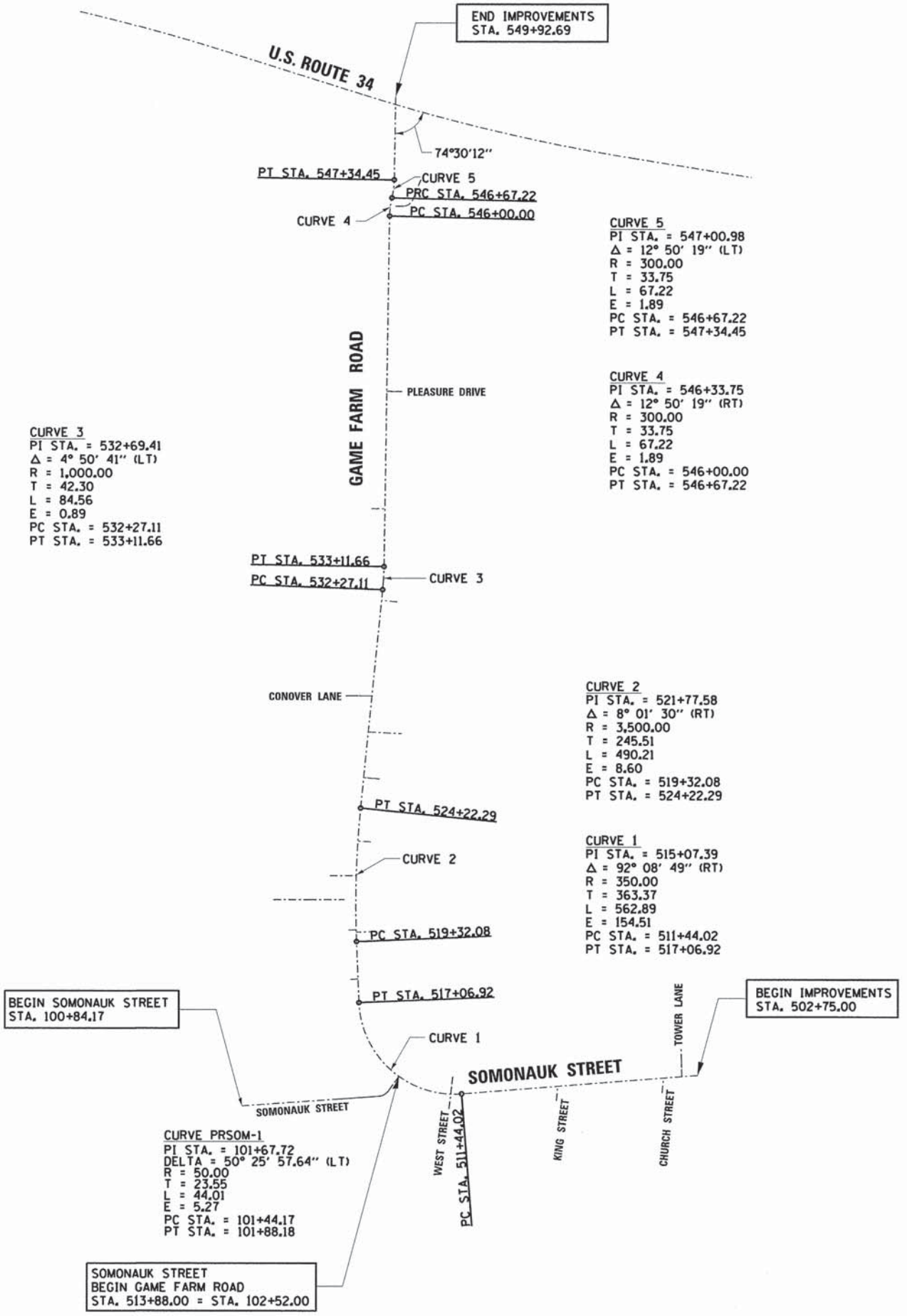
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|---------------------|----------------|---------------------------|--------------|-----------|
| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 22 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



| | | | |
|------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | CADD FILE NAME | | |

| | | | |
|---------|--------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHKD | | |

COMPANY NAME:
8/21/2014 4:08:56 PM
098_TheCity



SOURCE BENCHMARK: PROVIDED BY THE UNITED CITY OF YORKVILLE
DESCRIPTION: A BRASS TABLET IN CONCRETE MONUMENT ON THE EAST SIDE
OF GAME FARM ROAD AT THE SOUTHWEST CORNER OF "UNIT ONE - COUNTRYSIDE
SUBDIVISION"
ELEVATION: 634.71
U.S.G.S. DATUM: (NGVD 29)

SITE BENCHMARKS:
BENCHMARK 1:
CUT SQUARE IN SIDEWALK AT THE SOUTHEAST CORNER OF YORKVILLE GRADESCHOOL
GYMNASIUM BUILDING ALONG THE NORTH SIDE OF SOMONAUK STREET,
DIRECTLY SOUTH OF GAS METER SHUT OFF VALVE
ELEVATION: 627.66

BENCHMARK 2:
CUT SQUARE IN SOUTHEAST CORNER OF CONCRETE SLAB FOR ELECTRICAL TOOL
BOXES - 24' EAST OF THE SOUTHWEST CORNER OF TENNIS COURT FENCE.
ELEVATION: 632.58

BENCHMARK 3:
CUT SQUARE IN SOUTHWEST CORNER OF CONCRETE TRANSFORMER PAD AT THE BEECHER
COMMUNITY BUILDING (908 GAME FARM ROAD)
ELEVATION: 633.57

BENCHMARK 4:
CENTER OF MANHOLE IN CENTERLINE OF PLEASURE DRIVE, 140 FEET (+/-)
EAST OF THE EASTERLY RIGHT OF WAY LINE OF GAME FARM ROAD.
ELEVATION: 636.50

ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION**
ALIGNMENT, TIES, AND BENCHMARKS

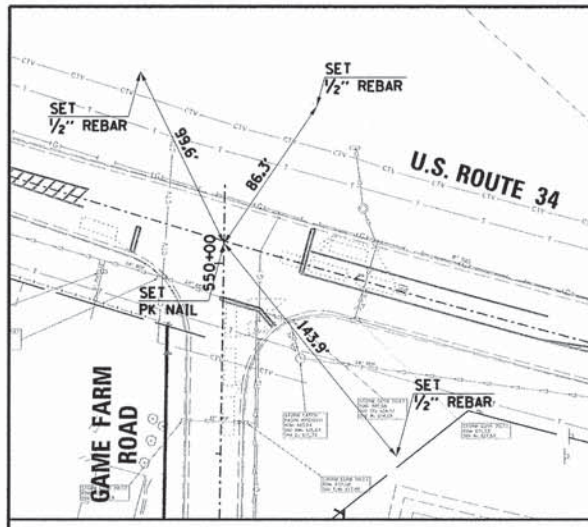
SCALE: VERT. 1"=25'
HORIZ. 1"=250'
DATE: JULY 2014

DRAWN BY: MJF
CHECKED BY: GT

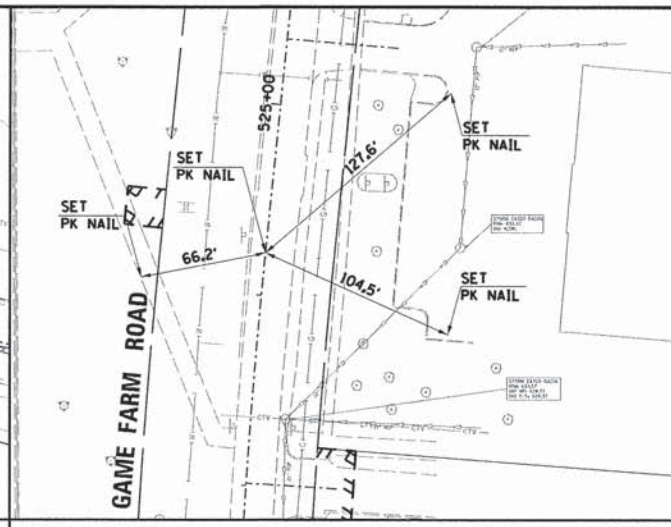
PLAN SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. _____
 ALIGNMENT CHECKED RT. OF WAY CHECKED
 CADD FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. _____
 GRADES CHECKED B.M. NOTED
 STRUCTURE NOTATIONS CHKD

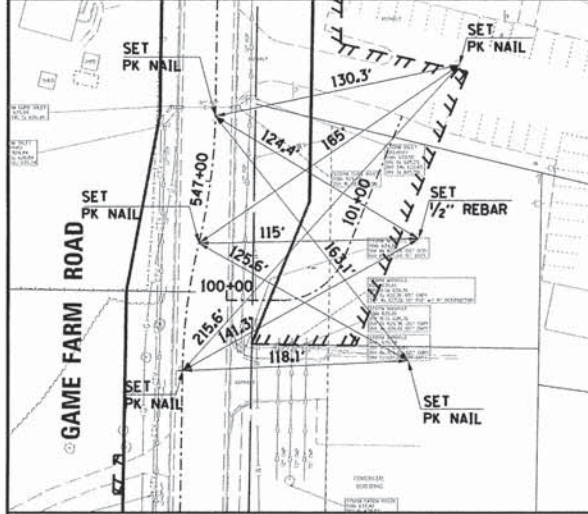
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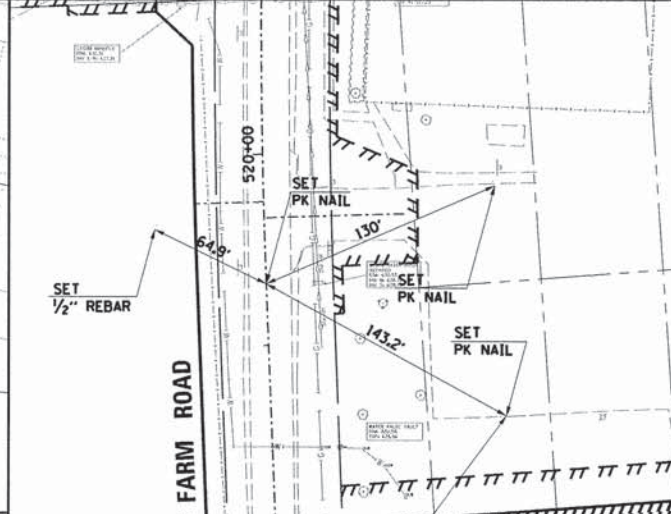
POT STA. 550 + 10.52



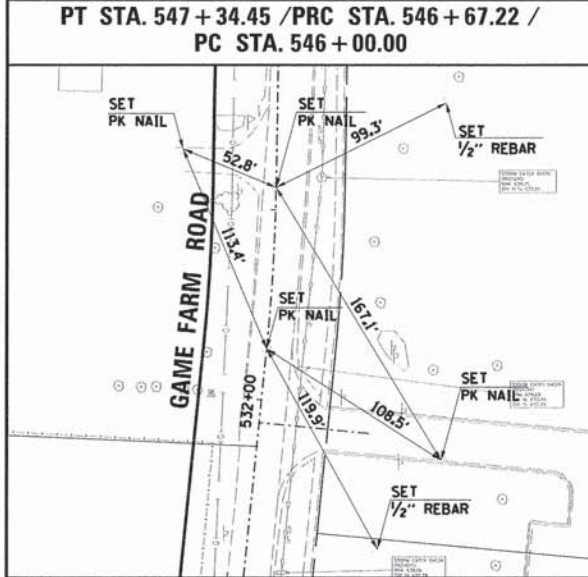
PT STA. 524 + 22.29



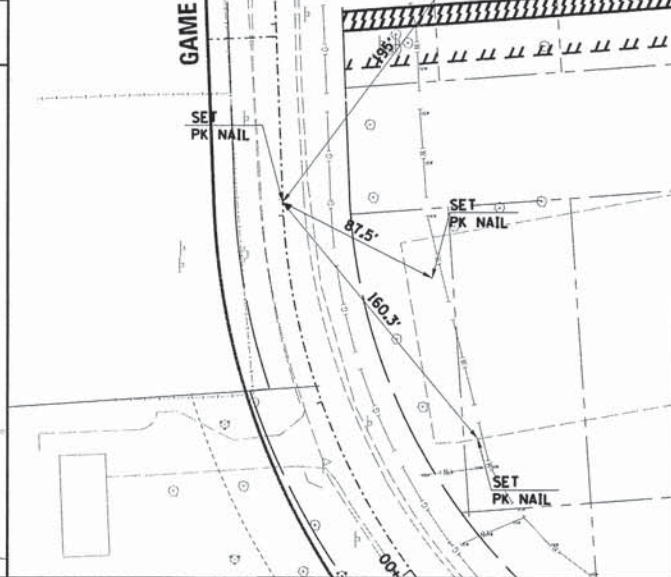
PT STA. 547 + 34.45 / PRC STA. 546 + 67.22 /
 PC STA. 546 + 00.00



PC STA. 519 + 32.07 / PT STA. 517 + 06.92



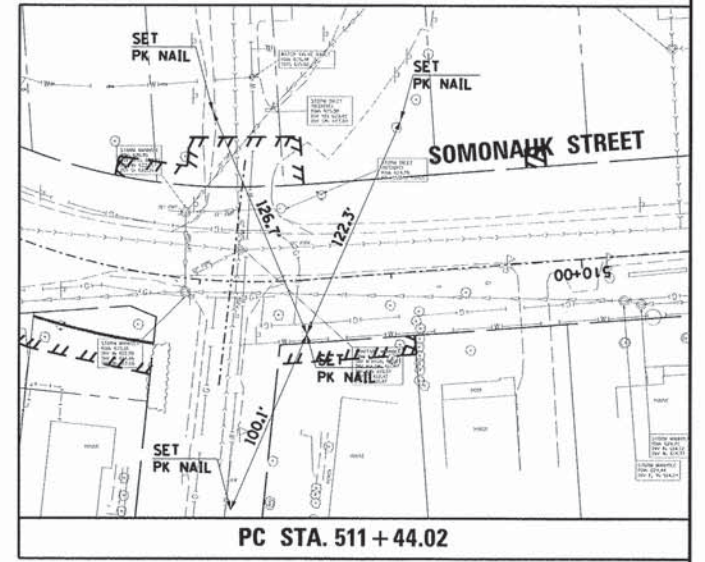
PT STA. 533 + 11.66 / PC STA. 532 + 27.11



| STATION | NORTH | EAST |
|-----------|----------------|--------------|
| 502+75.00 | 1,814,671.9922 | 953,250.7847 |
| 511+44.02 | 1,814,604.1550 | 952,384.4121 |
| 517+06.92 | 1,814,938.8563 | 952,007.3801 |
| 519+32.08 | 1,815,163.8275 | 951,998.2254 |
| 524+22.29 | 1,815,653.4291 | 952,012.6042 |
| 532+27.11 | 1,816,454.2775 | 952,092.4642 |
| 533+11.66 | 1,816,538.6702 | 952,097.2893 |
| 546+67.22 | 1,817,893.4068 | 952,124.8889 |
| 547+34.45 | 1,817,959.9497 | 952,133.3770 |



| | | | | |
|---------------------|---------------------------|---------|--------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 23 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | CONTRACT NO. 87345 | |

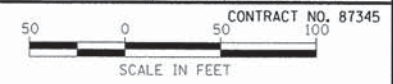


PC STA. 511 + 44.02

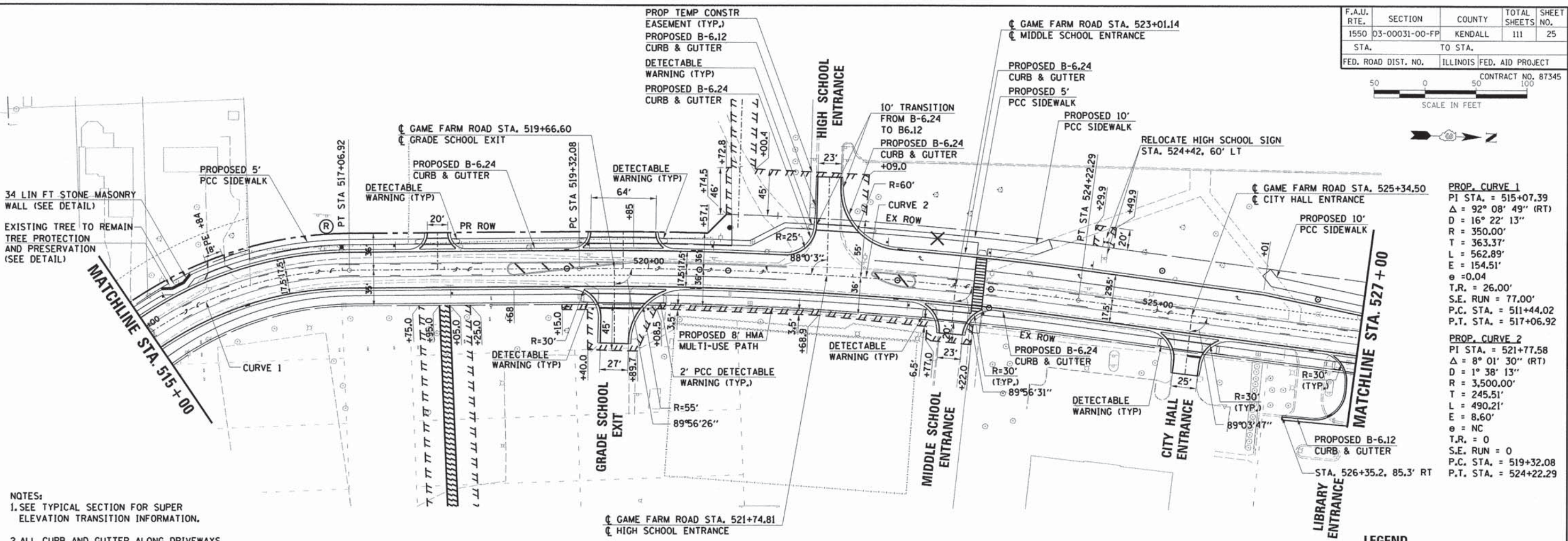
ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 ALIGNMENT, TIES, AND BENCHMARKS

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014
 DRAWN BY: M.J.F.
 CHECKED BY: G.T.

| | | | | |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 25 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



| | | | |
|------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | CADD FILE NAME | | |



- NOTES:
- SEE TYPICAL SECTION FOR SUPER ELEVATION TRANSITION INFORMATION.
 - ALL CURB AND GUTTER ALONG DRIVEWAYS SHALL BE DEPRESSED.

PROP. CURVE 1

PI STA. = 515+07.39
 $\Delta = 92^\circ 08' 49''$ (RT)
 $D = 16^\circ 22' 13''$
 $R = 350.00'$
 $T = 363.37'$
 $L = 562.89'$
 $E = 154.51'$
 $e = 0.04$
 $T.R. = 26.00'$
 $S.E. RUN = 77.00'$
 $P.C. STA. = 511+44.02$
 $P.T. STA. = 517+06.92$

PROP. CURVE 2

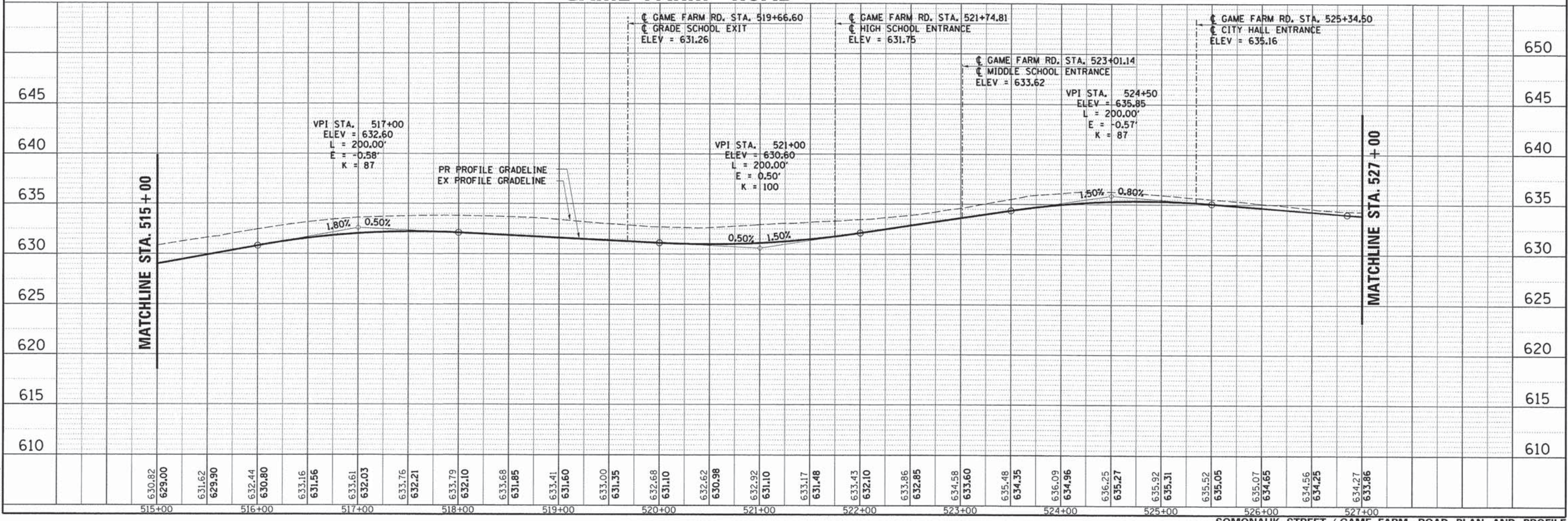
PI STA. = 521+77.58
 $\Delta = 8^\circ 01' 30''$ (RT)
 $D = 1^\circ 38' 13''$
 $R = 3,500.00'$
 $T = 245.51'$
 $L = 490.21'$
 $E = 8.60'$
 $e = NC$
 $T.R. = 0$
 $S.E. RUN = 0$
 $P.C. STA. = 519+32.08$
 $P.T. STA. = 524+22.29$

LEGEND

✕ TREE REMOVAL

GAME FARM ROAD

| | | | |
|---------|--------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
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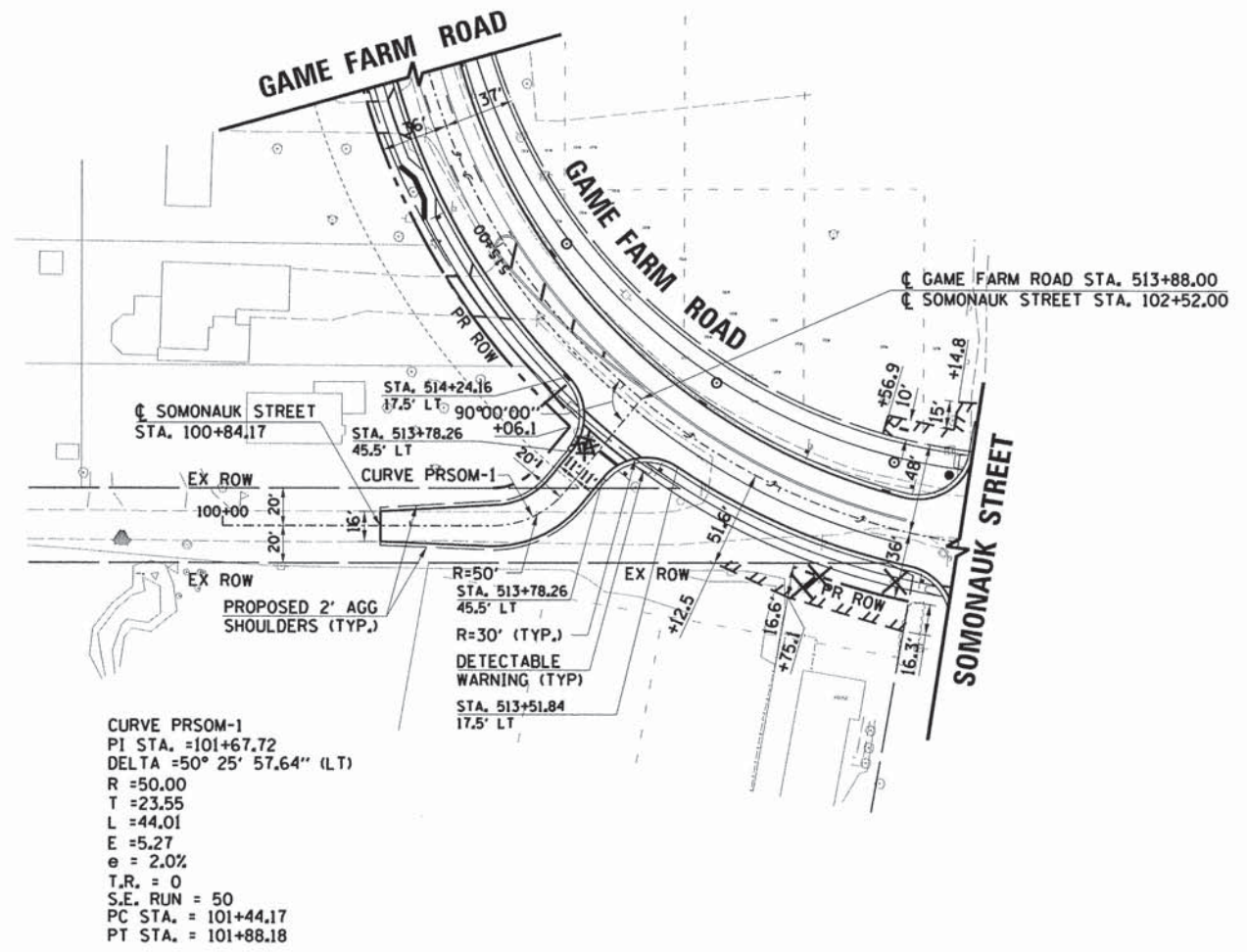
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 28 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |



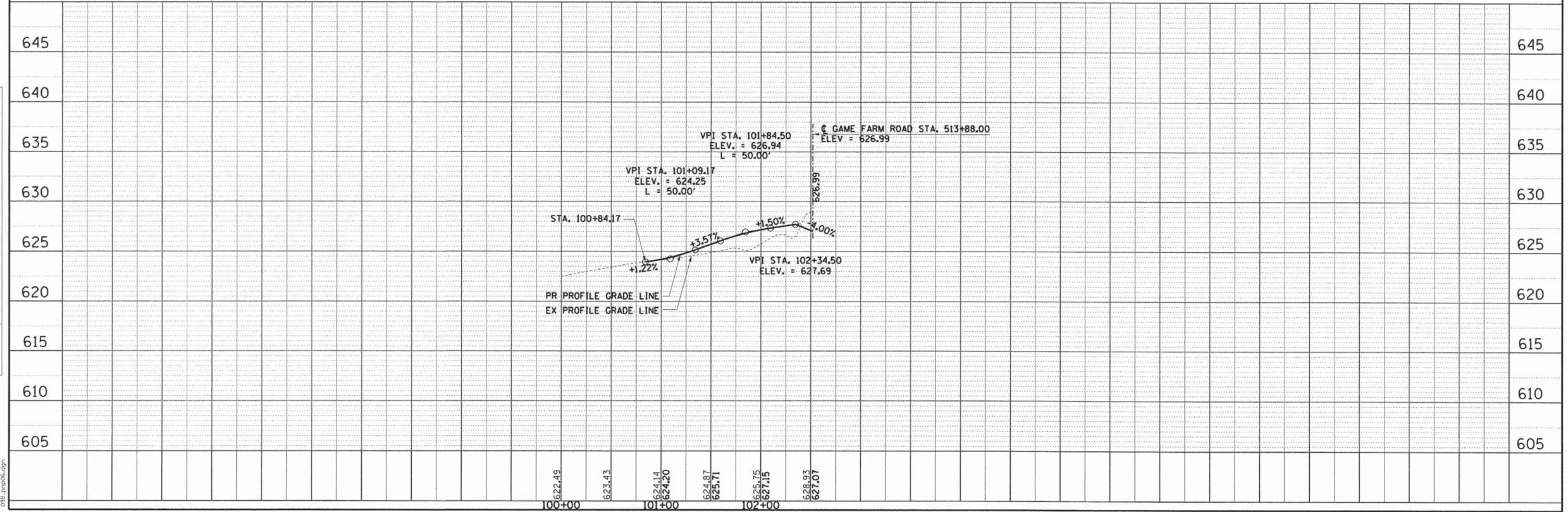
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| | STRUCTURE NOTATIONS CHKD | | |



SOMONAUK STREET CONNECTION

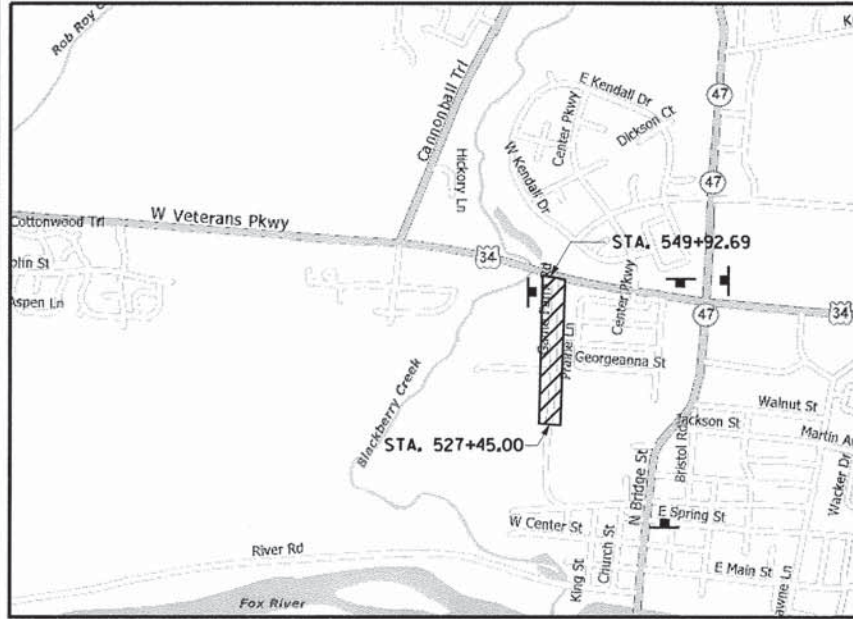
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 29 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |

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| PLAN | SURVEYED | DATE |
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| | RT. OF WAY | |
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STA. 527+45 TO STA 549+92.69
 STAGE I - CONSTRUCTION FROM US ROUTE 34 PAST THE NORTH ENTRANCE OF YORKVILLE HIGH SCHOOL.

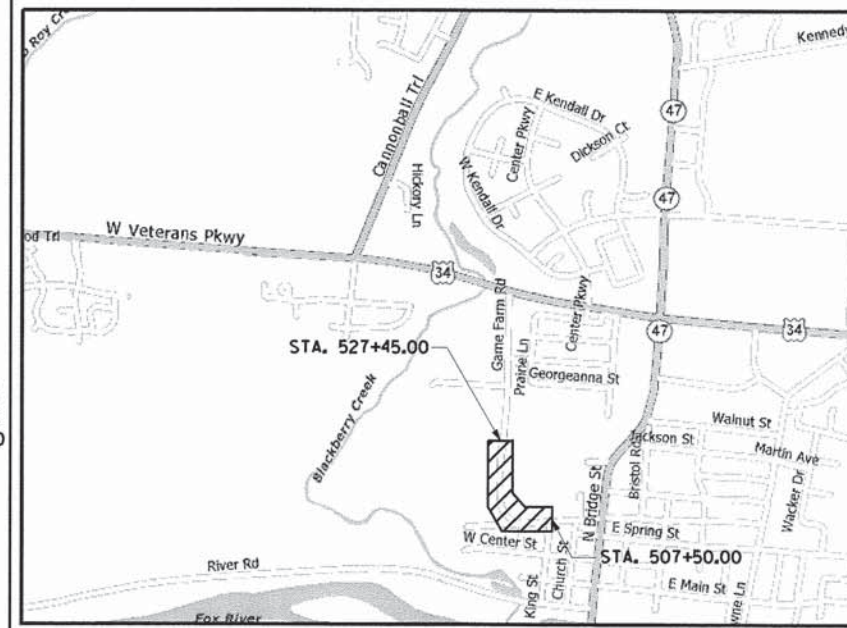
STAGE I

PRE-STAGE

1. INSTALL THROUGH TRAFFIC DETOUR SIGNING FOR GAME FARM ROAD / SOMONAUK.
2. INSTALL CHANGEABLE MESSAGE SIGNS FOR NORTHBOUND AND SOUTHBOUND TRAFFIC ON ILLINOIS ROUTE 47 AT THE SOMONAUK STREET INTERSECTION.
3. INSTALL CHANGEABLE MESSAGE SIGNS FOR EASTBOUND AND WESTBOUND TRAFFIC ON U.S. ROUTE 34 AT THE GAME FARM ROAD INTERSECTION.
4. INSTALL TEMPORARY EROSION CONTROL MEASURES.
5. COORDINATE UTILITY RELOCATIONS.
6. REMOVE TREES AS SHOWN ON THE PLANS.

STAGE I

1. THIS STAGE SHALL NOT BEGIN UNTIL THE EXISTING TRAFFIC SIGNALS AT THE INTERSECTION OF U.S. ROUTE 34 AND GAME FARM ROAD ARE SET TO "FLASHING YELLOW" FOR EASTBOUND AND WESTBOUND TRAFFIC WITHOUT ADVERSELY EFFECTING THE REST OF THE SYSTEM. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION. SEE SECTION 850 OF THE STANDARD SPECIFICATIONS.
2. CONSTRUCT RETAINING WALL NEAR US ROUTE 34.
3. PROVIDE REMOVAL OPERATIONS, PROPOSED STORM SEWER, DRIVEWAYS, AND SIDEWALKS ON GAME FARM ROAD AS SHOWN ON THE PLANS.
4. PROVIDE TEMPORARY AGGREGATE AS NEEDED FOR LOCAL ACCESS TO ENTRANCES.
5. INSTALL STORM SEWER, PAVEMENT, AND CURB AND GUTTER.
6. INSTALL NEW TRAFFIC SIGNAL EQUIPMENT.
7. INSTALL SIDEWALKS AND TEMPORARY PAVEMENT MARKING.
8. ACTIVATE PERMANENT TRAFFIC SIGNALS.
9. INSTALL ALL LANDSCAPING ITEMS WITHIN STAGE I.



STA. 507+50.00 TO STA 527+45.00
 STAGE II - CONSTRUCTION FROM NORTH ENTRANCE OF YORKVILLE HIGH SCHOOL TO EAST OF KING STREET.

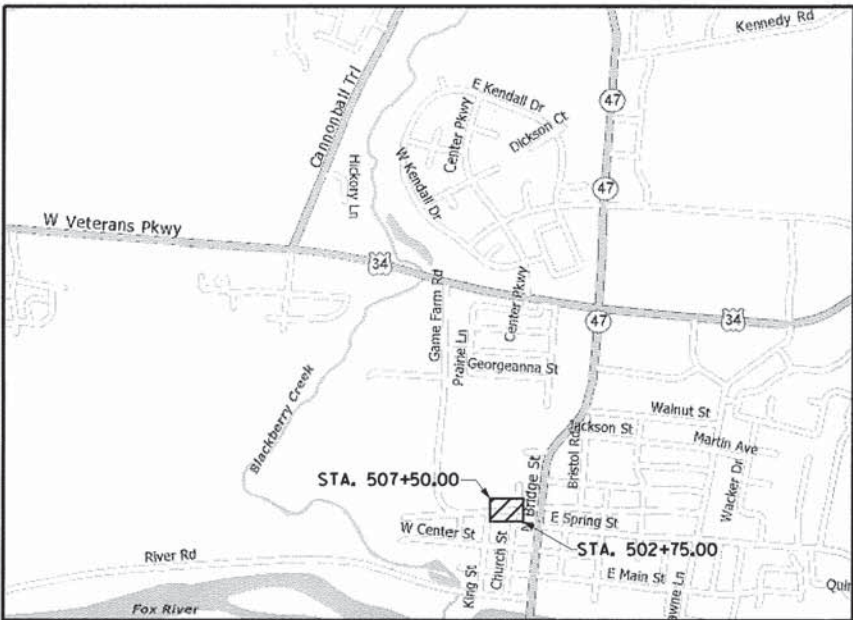
STAGE II

STAGE II

1. THIS STAGE SHALL NOT BEGIN UNTIL AFTER THE LAST DAY OF REGULAR CLASSES OF THE SCHOOL YEAR.
2. THIS STAGE SHALL BE COMPLETED BY THE FIRST DAY OF REGULAR CLASSES FOR THE BEGINNING OF THE SCHOOL YEAR.
3. THIS STAGE SHALL NOT BEGIN UNTIL AFTER STAGE I IS COMPLETED.
4. INSTALL AND MAINTAIN TEMPORARY FENCING ALONG SCHOOL DISTRICT FRONTAGE.
5. PROVIDE REMOVAL OPERATIONS, PROPOSED STORM SEWER, DRIVEWAYS, AND SIDEWALKS ON GAME FARM ROAD AS SHOWN ON THE PLANS.
6. PROVIDE TEMPORARY AGGREGATE AS NEEDED FOR LOCAL ACCESS TO ENTRANCES. ACCESS TO SOMONAUK STREET SHALL BE MAINTAINED.
7. INSTALL PAVEMENT, AND CURB AND GUTTER.
8. INSTALL SIDEWALKS AND TEMPORARY PAVEMENT MARKING.
9. INSTALL ALL LANDSCAPING ITEMS WITHIN STAGE II.

NOTE:
 ALL DRIVEWAYS MUST BE OPEN TO THE PUBLIC FOR JUNE 2015 GRADUATION CEREMONIES.

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| | BY | |
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| | STRUCTURE NOTATIONS CHKD | |
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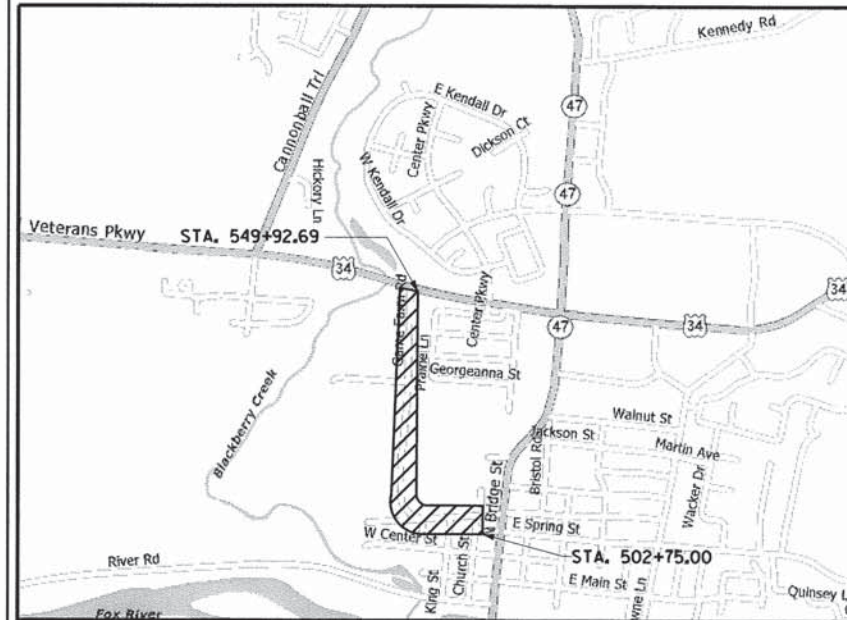


STA. 502+75.00 TO STA 507+50.00
 STAGE III - CONSTRUCTION FROM EAST OF KING STREET TO TOWER LANE.

STAGE III

STAGE III

1. THIS STAGE SHALL NOT BEGIN UNTIL AFTER STAGE II IS COMPLETED.
2. PROVIDE REMOVAL OPERATIONS, PROPOSED STORM SEWER, DRIVEWAYS, AND SIDEWALKS IN WORK ZONE ON SOMONAUK STREET AS SHOWN ON THE PLANS.
3. PROVIDE TEMPORARY AGGREGATE AS NEEDED FOR LOCAL ACCESS TO ALL ENTRANCES.
4. INSTALL PAVEMENT, AND CURB AND GUTTER.
5. INSTALL SIDEWALKS AND TEMPORARY PAVEMENT MARKING. WORK ZONE PAVEMENT MARKING FOR THE MAINTENANCE OF TRAFFIC SHALL CONSIST OF THE FOLLOWING:
 TEMPORARY PAVEMENT MARKING
 SHORT-TERM PAVEMENT MARKING
 (SEE PAVEMENT MARKING SCHEDULE FOR DETAILS).
6. MAINTAIN TEMPORARY FENCING ALONG SCHOOL DISTRICT FRONTAGE.
7. INSTALL ALL LANDSCAPING ITEMS WITHIN STAGE III.



STA. 502+75.00 TO STA 549+92.69
 STAGE IV - CONSTRUCTION ALONG THE ENTIRE PROJECT LENGTH TO INCLUDE BITUMINOUS SURFACE COURSE, AND PAVEMENT MARKINGS.

STAGE IV

STAGE IV

1. PROVIDE SURFACE COURSE PAVEMENT FOR ENTIRE PROJECT LENGTH IN ACCORDANCE WITH STANDARD 701502-02.
2. INSTALL FINAL PAVEMENT MARKINGS.
3. INSTALL ALL LANDSCAPING ITEMS WITHIN STAGE IV.
4. REMOVE THROUGH TRAFFIC DETOUR.
5. REMOVE ALL TEMPORARY TRAFFIC CONTROL MEASURES.
6. REMOVE TEMPORARY EROSION CONTROL MEASURES.

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
 SUGGESTED MAINTENANCE OF TRAFFIC

SCALE: VERT.
 HORIZ.
 DATE: JULY 2014
 DRAWN BY: JMR
 CHECKED BY: GT

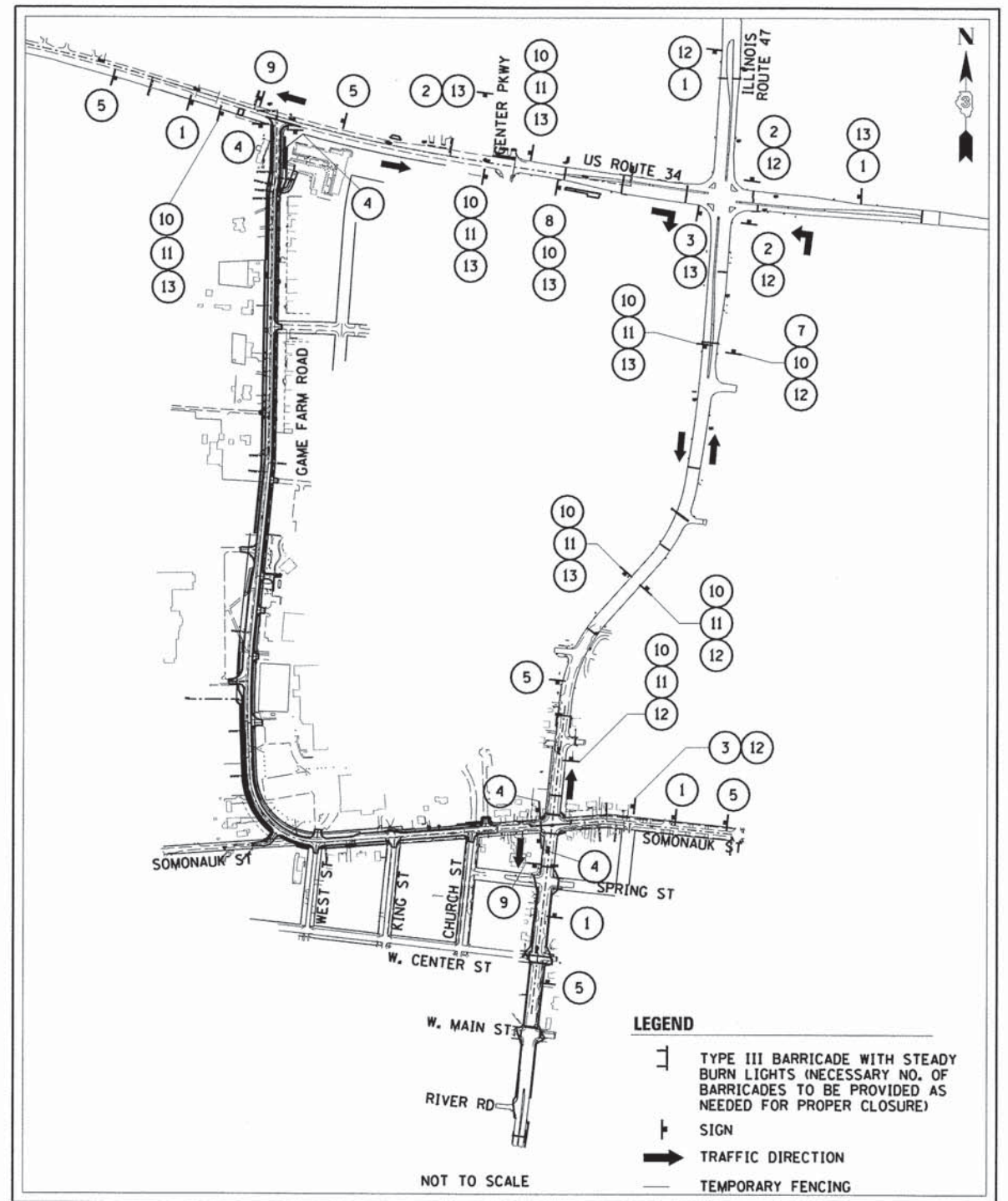
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| PROFILE | SURVEYED | BY | DATE |
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| | GRADES | | |
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| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS | | |
| | CHKD | | |
| | NO. | | |

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SIGN LEGEND

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DETOUR NOTES

- SEE IDOT STANDARD 701901 FOR TRAFFIC CONTROL DEVICES.
- SEE IDOT STANDARD 701502 FOR LANE CLOSURES.
- CONTRACTOR SHALL MAINTAIN TRAFFIC AT ALL SIDE ROAD INTERSECTIONS.
- ALL PAVEMENT MARKINGS REMOVED FOR CONSTRUCTION STAGING SHALL BE REPLACED.
- SIGN LOCATIONS SHOWN ON THE SUGGESTED THROUGH TRAFFIC DETOUR PLAN ARE APPROXIMATE AND SUBJECT TO MODIFICATION AT THE TIME OF CONSTRUCTION.
- SEE IDOT STANDARD 701801 FOR SIDEWALK CLOSURES.
- ALL SIGNING AND BARRIERS SHALL BE INCLUDED IN THE PAY ITEM "DETOUR SIGNING".

SUGGESTED THROUGH TRAFFIC DETOUR SCHEMATIC

- THE CONTRACTOR MUST NOTIFY THE ILLINOIS DEPARTMENT OF TRANSPORTATION BUREAU OF OPERATIONS 14 DAYS PRIOR TO THE IMPLEMENTATION OF DETOURS UTILIZING STATE ROADS.
- THE ILLINOIS DEPARTMENT OF TRANSPORTATION MUST BE GIVEN THE NAME AND PHONE NUMBERS OF A CONTRACTOR'S CONTACT PERSON WHO WILL BE AVAILABLE 24 HOURS A DAY, THAT WILL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL.

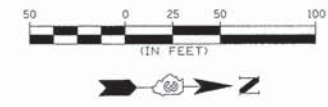
ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION**

SUGGESTED MAINTENANCE OF TRAFFIC

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| SCALE: VERT. | DRAWN BY: JMR |
| HORIZ. | CHECKED BY: GT |
| DATE: JULY 2014 | |

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 31 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

CONTRACT NO. 87345



STAGING GENERAL NOTES

1. THIS ITEM SHALL BE PERFORMED IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 107.14 AND THE APPLICABLE GUIDELINES IN THE DEPARTMENT'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THEIR QUALITY STANDARDS FOR WORK ZONE TRAFFIC CONTROL DEVICES. THIS WORK WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION.
2. WORK SCHEDULED IN STAGE I SHALL NOT BEGIN UNTIL ALL CONFLICTING TRAFFIC CONTROL AND PROTECTION SIGNS AND PAVEMENT MARKINGS HAVE BEEN COVERED OR REMOVED.
3. THE TRAFFIC SIGNALS SHALL BE SET TO "FLASHING YELLOW" FOR U.S. ROUTE 34 TRAFFIC, AND CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED IN BOTH DIRECTIONS ALONG U.S. ROUTE 34 TO PROVIDE AMPLE WARNING.
4. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKINGS THAT CONFLICT WITH THE NEXT STAGE OR ULTIMATE IMPROVEMENT.
5. USE SIGNING IN ACCORDANCE WITH STANDARD 701306 FOR FLAGGING OPERATIONS.
6. FOLLOW REQUIREMENTS OF IDOT STANDARD 702001.
7. TEMPORARY LANE CLOSURE SHALL BE BETWEEN 9:00 AM AND 3:00 PM. THIS WORK SHALL BE CONDUCTED ACCORDING TO STANDARD 701201.
8. MAILBOXES IN CONFLICT WITH STAGED TRAFFIC SHALL BE RELOCATED. THIS WORK WILL BE INCLUDED IN THE COST OF THE CONTRACT (ARTICLE 107.20).
9. TYPE III BARRICADES ARE TO BE LOCATED IN ACCORDANCE WITH IDOT STANDARD 702001.
10. ALL SIGNING AND STRIPING IS TO BE IN ACCORDANCE WITH THE FOLLOWING STATE STANDARDS: 701006-02, 701301.02, 701306.01, 701311-02, 701701-04, AND 702001-05.
11. CONTRACTOR SHALL PROVIDE AND MAINTAIN ACCESS TO ALL SIDE STREETS AND TEMPORARY DRIVEWAY ACCESS TO ALL ABUTTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION.
12. CONTRACTOR WILL BE ALLOWED TO CLOSE THE HIGH SCHOOL EXIT FOR A MAXIMUM OF TEN DAYS TO INSTALL STORM SEWER, CORE OUT EXISTING PAVEMENT, AND PLACEMENT OF AGGREGATE BASE IN FRONT OF THE HIGH SCHOOL EXIT. TEMPORARY SHUTDOWNS WILL BE ALLOWED IN THIS LOCATION TO GRADE, PLACE CURB AND GUTTER, AND PLACE PAVEMENT, BUT CANNOT LAST MORE THAN 24 HOURS IN DURATION. COORDINATE WITH ENGINEER.
13. AFTER PLACEMENT OF BINDER COURSE, CONTRACTOR WILL PLACE A TEMPORARY HMA RAMP AT US ROUTE 34 TO MATCH THE EXISTING PROFILE TO THE NEWLY CONSTRUCTED PROFILE.
14. A SIGN ON THE CHAIN LINK FENCE SHALL BE LOCATED EVERY 100' INDICATING "DANGER KEEP OUT". THE CONTRACTOR SHALL PROVIDE SIGNAGE ON SCHOOL DISTRICT PROPERTY AS REQUIRED TO DETOUR TRAFFIC DURING DRIVEWAY CLOSURES. ADDITIONAL SAFETY MEASURES ALONG THE SCHOOL PROPERTY MAY BE REQUIRED PER THE DIRECTION OF THE CITY ENGINEER.
15. LOCATIONS OF TEMPORARY SIGNS MAY BE ADJUSTED IN THE FIELD FOR MAXIMUM VISIBILITY AS DIRECTED BY THE ENGINEER.

LEGEND

| | |
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| | TEMPORARY CHAIN LINK FENCE |
| | CONSTRUCTION WORK ZONE |
| | TEMPORARY PAVEMENT |
| | TRAFFIC CONTROL SIGNS |
| | BARRICADES OR DRUMS @ 50 FT C-C WITH STEADY BURN LIGHTS |
| | VERTICAL PANELS WITH STEADY BURN LIGHTS @ 50 FT C-C |
| | TYPE III BARRICADE WITH TWO STEADY BURN LIGHTS |

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
 SUGGESTED MAINTENANCE OF TRAFFIC
STAGE I
 SCALE: VERT. 1"=20', HORIZ. 1"=40'
 DATE: JULY 2014
 DRAWN BY: JMR
 CHECKED BY: GT

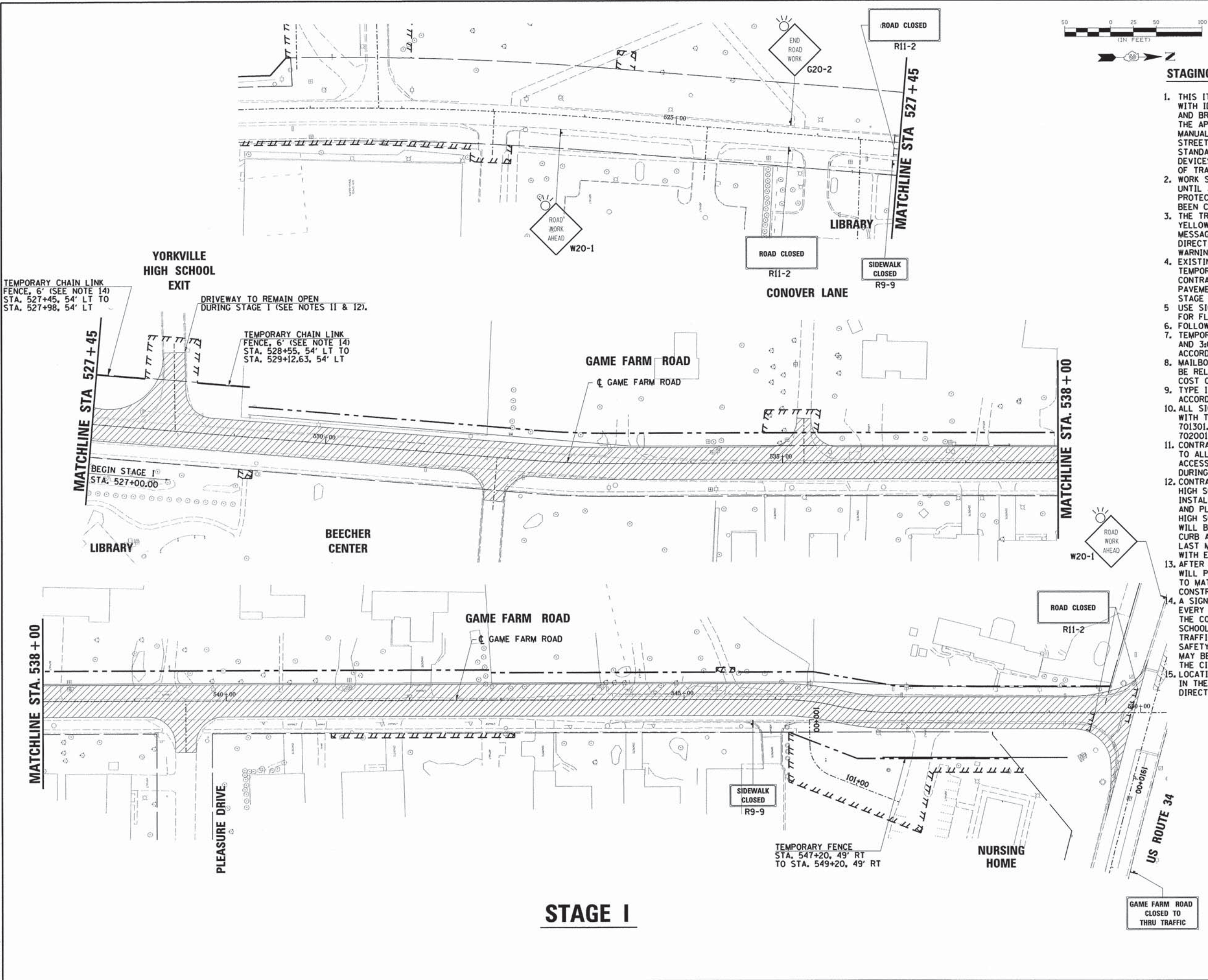
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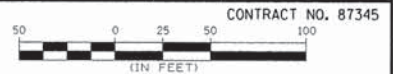
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| STRUCTURE NOTATIONS CHKD | | |

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STAGE I

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 32 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



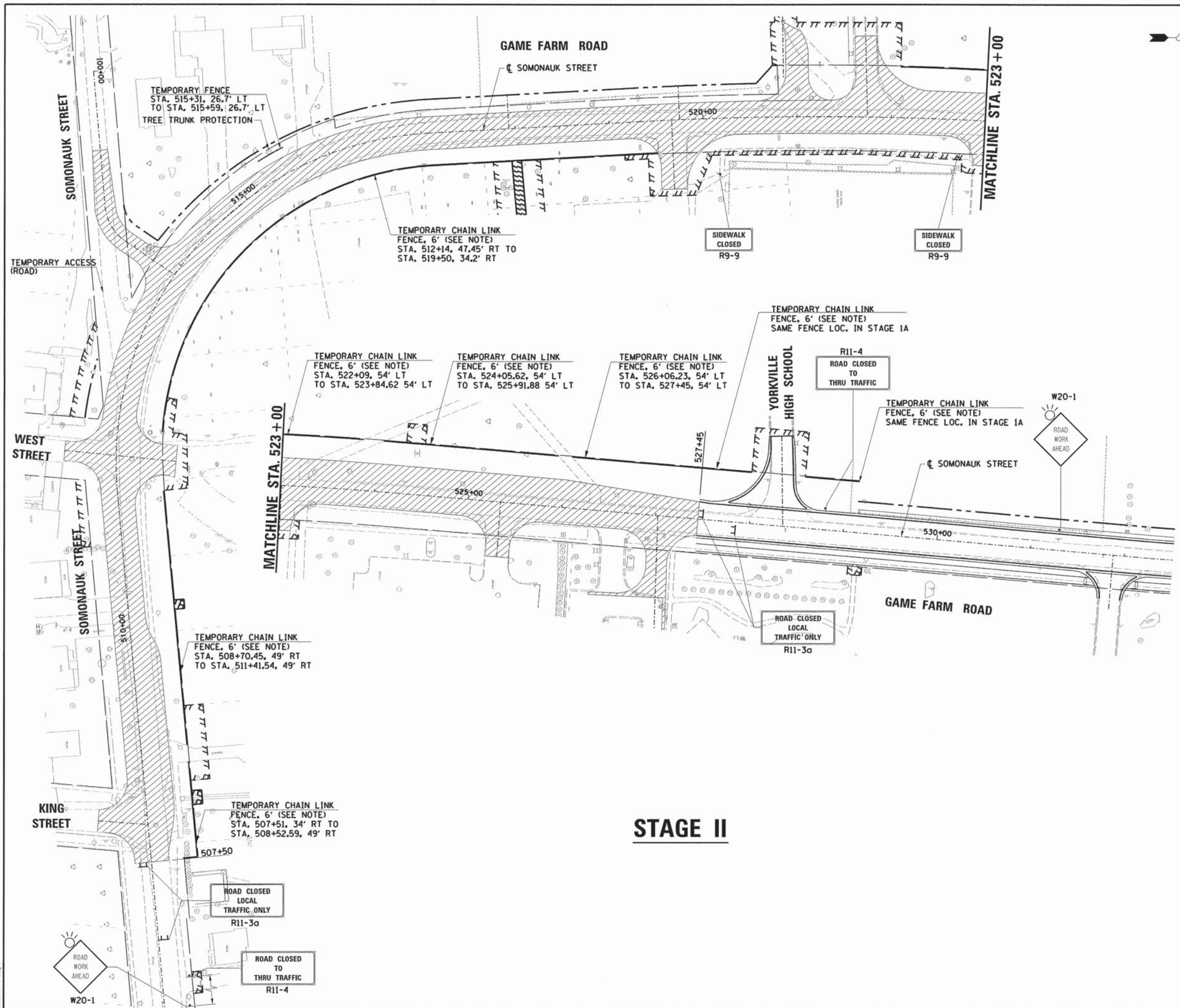
STAGING GENERAL NOTES

1. THIS ITEM SHALL BE PERFORMED IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 107.14 AND THE APPLICABLE GUIDELINES IN THE DEPARTMENT'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THEIR QUALITY STANDARDS FOR WORK ZONE TRAFFIC CONTROL DEVICES. THIS WORK WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION.
2. WORK SCHEDULED IN EACH STAGE SHALL NOT BEGIN UNTIL ALL CONFLICTING TRAFFIC CONTROL AND PROTECTION SIGNS AND PAVEMENT MARKINGS HAVE BEEN COVERED OR REMOVED.
3. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKINGS THAT CONFLICT WITH THE NEXT STAGE OR ULTIMATE IMPROVEMENT.
4. USE SIGNING IN ACCORDANCE WITH STANDARD 701306 FOR FLAGGING OPERATIONS.
5. TEMPORARY LANE CLOSURE SHALL BE BETWEEN 9:00 AM AND 3:00 PM. THIS WORK SHALL BE CONDUCTED ACCORDING TO STANDARD 701201.
6. MAILBOXES IN CONFLICT WITH STAGED TRAFFIC SHALL BE RELOCATED. THIS WORK WILL BE INCLUDED IN THE COST OF THE CONTRACT (ARTICLE 107.20).
7. TYPE III BARRICADES ARE TO BE LOCATED IN ACCORDANCE WITH IDOT STANDARD 702001.
8. ALL SIGNING AND STRIPING IS TO BE IN ACCORDANCE WITH THE FOLLOWING STATE STANDARDS: 701006-02, 701301.02, 701306.01, 701311-02, 701701-04, AND 702001-05.
9. CONTRACTOR SHALL PROVIDE AND MAINTAIN ACCESS TO ALL SIDE STREETS INCLUDING SOMONAUK STREET WEST OF GAME FARM ROAD, AND TEMPORARY DRIVEWAY ACCESS TO ALL ABUTTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION.
10. A SIGN ON THE CHAIN LINK FENCE SHALL BE LOCATED EVERY 100' INDICATING "DANGER KEEP OUT". ADDITIONAL SAFETY MEASURES ALONG THE SCHOOL PROPERTY MAY BE REQUIRED PER THE DIRECTION OF THE CITY ENGINEER.
11. LOCATIONS OF TEMPORARY SIGNS MAY BE ADJUSTED IN THE FIELD FOR MAXIMUM VISIBILITY AS DIRECTED BY THE ENGINEER.

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| | STRUCTURE NOTATIONS CHKD | | |

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STAGE II

LEGEND

- TEMPORARY CHAIN LINK FENCE
- ▨ CONSTRUCTION WORK ZONE
- ▩ TEMPORARY PAVEMENT
- ⊥ TRAFFIC CONTROL SIGNS
- BARRICADES OR DRUMS @ 50 FT C-C WITH STEADY BURN LIGHTS
- ⊥ VERTICAL PANELS WITH STEADY BURN LIGHTS @ 50 FT C-C
- ⊥ TYPE III BARRICADE WITH TWO STEADY BURN LIGHTS

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
 SUGGESTED MAINTENANCE OF TRAFFIC
STAGE II A

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: JMR
 CHECKED BY: GT

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 33 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

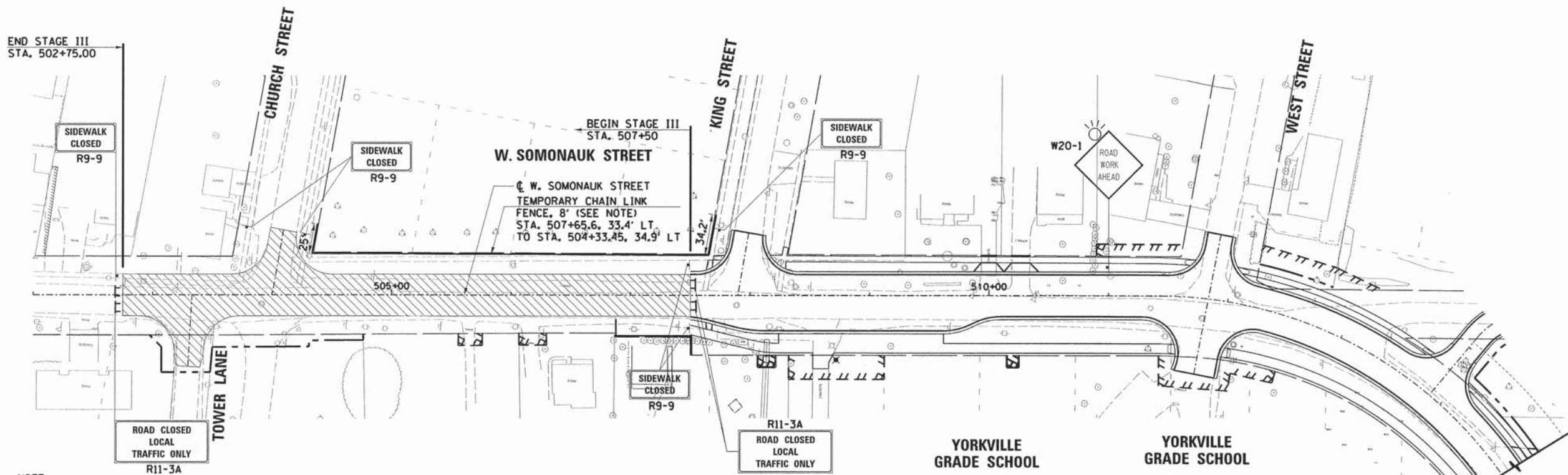
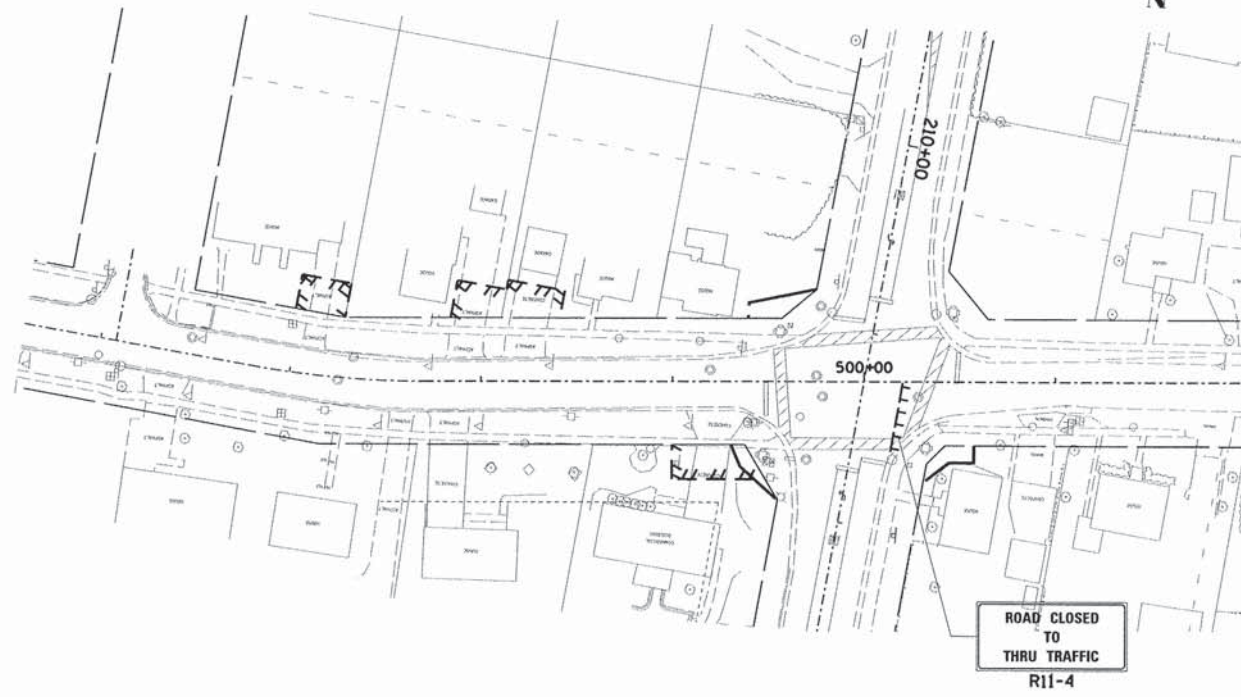
CONTRACT NO. 87345

STAGING GENERAL NOTES

1. THIS ITEM SHALL BE PERFORMED IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 107.14 AND THE APPLICABLE GUIDELINES IN THE DEPARTMENT'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THEIR QUALITY STANDARDS FOR WORK ZONE TRAFFIC CONTROL DEVICES. THIS WORK WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION.
2. WORK SCHEDULED IN EACH STAGE SHALL NOT BEGIN UNTIL ALL CONFLICTING TRAFFIC CONTROL AND PROTECTION SIGNS AND PAVEMENT MARKINGS HAVE BEEN COVERED OR REMOVED.
3. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY PAVEMENT MARKINGS THAT CONFLICT WITH THE NEXT STAGE OR ULTIMATE IMPROVEMENT.
4. USE SIGNING IN ACCORDANCE WITH STANDARD 701306 FOR FLAGGING OPERATIONS.
5. TEMPORARY LANE CLOSURE SHALL BE BETWEEN 9:00 AM AND 3:00 PM. THIS WORK SHALL BE CONDUCTED ACCORDING TO STANDARD 701201.
6. MAILBOXES IN CONFLICT WITH STAGED TRAFFIC SHALL BE RELOCATED. THIS WORK WILL BE INCLUDED IN THE COST OF THE CONTRACT (ARTICLE 107.20).
7. TYPE III BARRICADES ARE TO BE LOCATED IN ACCORDANCE WITH IDOT STANDARD 702001.
8. ALL SIGNING AND STRIPING IS TO BE IN ACCORDANCE WITH THE FOLLOWING STATE STANDARDS: 701006-02, 701301-02, 701306-01, 701311-02, 701701-04, AND 702001-05.
9. CONTRACTOR SHALL PROVIDE AND MAINTAIN ACCESS TO ALL SIDE STREETS AND TEMPORARY DRIVEWAY ACCESS TO ALL ABUTTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION.
10. A SIGN ON THE CHAIN LINK FENCE SHALL BE LOCATED EVERY 100' INDICATING "DANGER KEEP OUT". ADDITIONAL SAFETY MEASURES ALONG THE SCHOOL PROPERTY MAY BE REQUIRED PER THE DIRECTION OF THE CITY ENGINEER.
11. LOCATIONS OF TEMPORARY SIGNS MAY BE ADJUSTED IN THE FIELD FOR MAXIMUM VISIBILITY AS DIRECTED BY THE ENGINEER.

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| | GRADES CHECKED | | |
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| | STRUCTURE NOTATIONS CHKD | | |
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- LEGEND**
- TEMPORARY CHAIN LINK FENCE
 - ▨ CONSTRUCTION WORK ZONE
 - ▩ TEMPORARY PAVEMENT
 - ⊠ TRAFFIC CONTROL SIGNS
 - BARRICADES OR DRUMS @ 50 FT C-C WITH STEADY BURN LIGHTS
 - ⊞ VERTICAL PANELS WITH STEADY BURN LIGHTS @ 50 FT C-C
 - ⊞ TYPE III BARRICADE WITH TWO STEADY BURN LIGHTS

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
 SUGGESTED MAINTENANCE OF TRAFFIC STAGE III

SCALE: VERT. _____ HORIZ. _____ DRAWN BY: JMR
 DATE: JULY 2014 CHECKED BY: GT

NOTE:
 CHAIN LINK SIGN SHALL INCLUDE SIGNS EVERY 100' INDICATING "DANGER, KEEP OUT". ADDITIONAL SAFETY MEASURES ALONG THE SCHOOL PROPERTY MAY BE REQUIRED PER THE DIRECTION OF THE CITY ENGINEER.

STAGE III

COMPANY NAME: 8/27/2014 4:02:35 PM 038_mor106.dgn

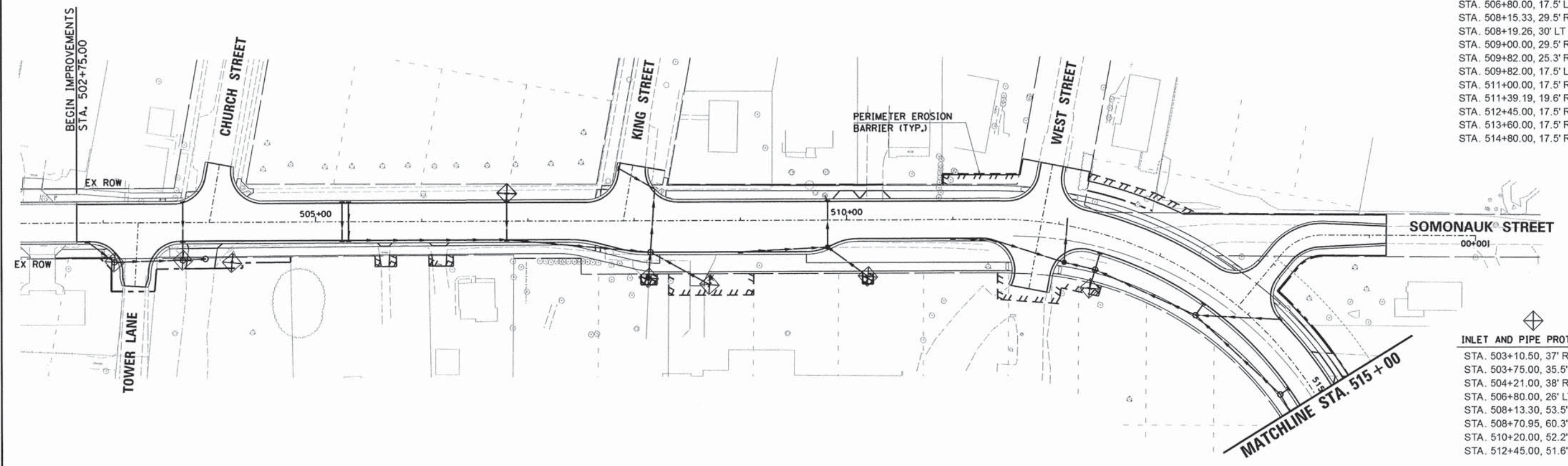
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 34 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |



| PLAN | SURVEYED | BY | DATE |
|-----------|----------------|----|------|
| NOTE BOOK | PLOTTED | | |
| NO. | RT. OF WAY | | |
| | CADD FILE NAME | | |

| PROFILE | SURVEYED | BY | DATE |
|-----------|---------------------|----|------|
| NOTE BOOK | GRADES | | |
| NO. | B.M. NOTED | | |
| | STRUCTURE NOTATIONS | | |

COMPANY NAME: 8/27/2014 4:02:38 PM
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INLET FILTERS

| |
|--------------------------|
| STA. 502+85.00, 17.5' RT |
| STA. 503+75.00, 17.5' RT |
| STA. 503+75.00, 18.7' LT |
| STA. 505+25.00, 17.5' RT |
| STA. 505+25.00, 17.5' LT |
| STA. 505+31.00, 17.5' RT |
| STA. 505+31.00, 17.5' LT |
| STA. 506+80.00, 17.5' RT |
| STA. 506+80.00, 17.5' LT |
| STA. 508+15.33, 29.5' RT |
| STA. 508+19.26, 30' LT |
| STA. 509+00.00, 29.5' RT |
| STA. 509+82.00, 25.3' RT |
| STA. 509+82.00, 17.5' LT |
| STA. 511+00.00, 17.5' RT |
| STA. 511+39.19, 19.6' RT |
| STA. 512+45.00, 17.5' RT |
| STA. 513+60.00, 17.5' RT |
| STA. 514+80.00, 17.5' RT |

INLET AND PIPE PROTECTION

| |
|--------------------------|
| STA. 503+10.50, 37' RT |
| STA. 503+75.00, 35.5' RT |
| STA. 504+21.00, 38' RT |
| STA. 506+80.00, 26' LT |
| STA. 508+13.30, 53.5' RT |
| STA. 508+70.95, 60.3' RT |
| STA. 510+20.00, 52.2' RT |
| STA. 512+45.00, 51.6' RT |

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
EROSION AND SEDIMENT CONTROL
 SCALE: VERT. _____ HORIZ. _____ DRAWN BY: MJF
 DATE: JULY 2014 CHECKED BY: GT

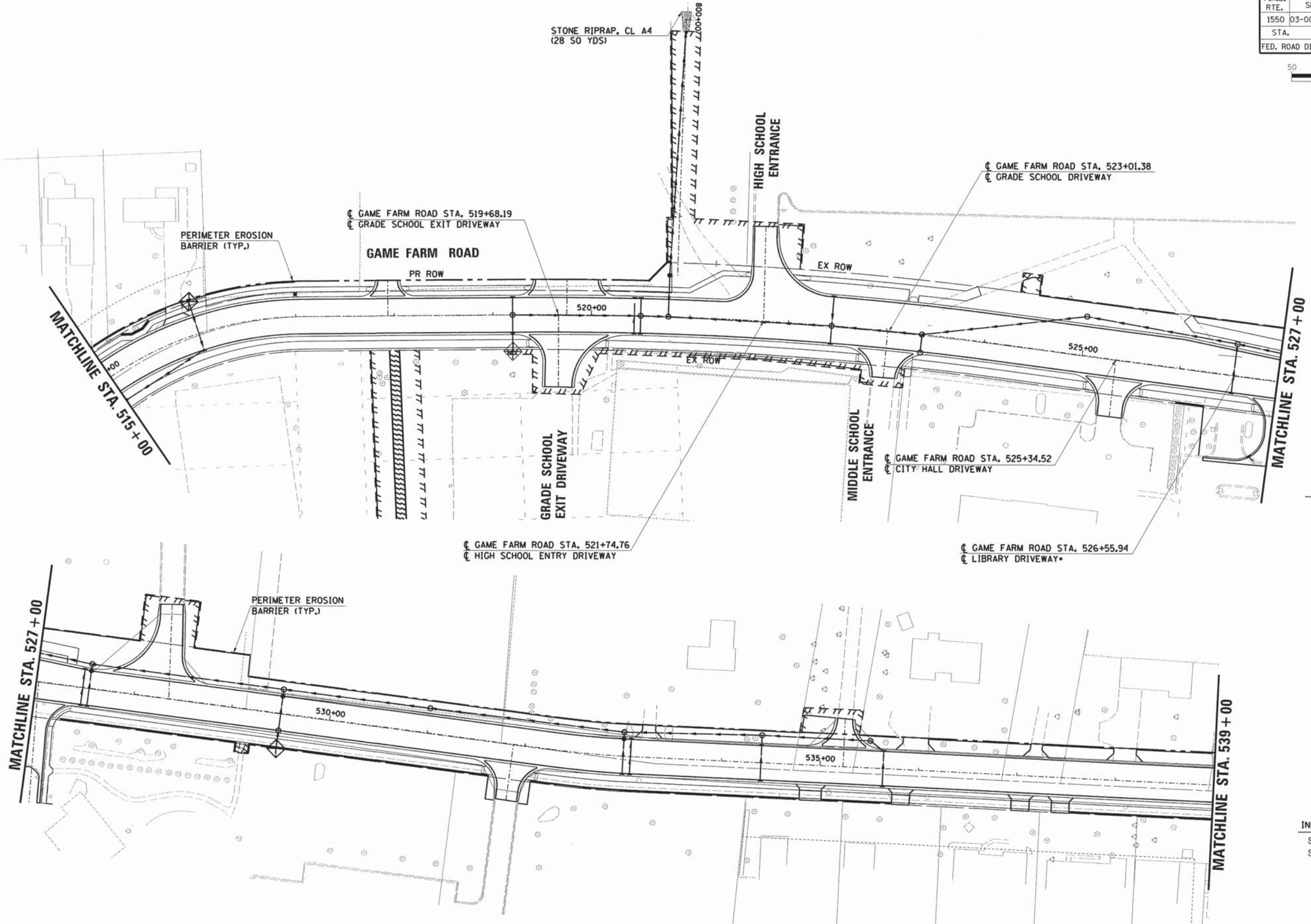
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|---------------------|----------------|------------------|--------------|-----------|
| F.A.J. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 35 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



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|---------------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | CADD FILE NAME | | |

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|---------------|-------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK NO. | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTAT'NS CHKD | | |

COMPANY NAME:
8/21/2014 4:02:42 PM
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INLET FILTERS

| |
|--------------------------|
| STA. 516+00.00, 17.5' RT |
| STA. 519+20.00, 17.5' RT |
| STA. 519+20.00, 17.5' LT |
| STA. 520+43.50, 17.5' RT |
| STA. 520+43.50, 17.5' LT |
| STA. 520+50.00, 17.5' RT |
| STA. 520+50.00, 17.5' LT |
| STA. 522+44.00, 17.5' RT |
| STA. 522+44.00, 29.5' LT |
| STA. 523+36.09, 18.3' RT |
| STA. 526+55.00, 17.5' RT |
| STA. 526+81.50, 80.1' RT |
| STA. 527+47.50, 17.5' RT |
| STA. 527+47.50, 18.2' LT |
| STA. 527+54.00, 17.5' RT |
| STA. 527+54.00, 17.7' LT |
| STA. 529+50.00, 17.5' RT |
| STA. 529+50.00, 17.5' LT |
| STA. 533+00.00, 17.5' RT |
| STA. 533+00.00, 17.5' LT |
| STA. 533+07.50, 17.5' RT |
| STA. 533+07.50, 17.5' LT |
| STA. 534+40.00, 17.5' RT |
| STA. 534+40.00, 17.5' LT |
| STA. 535+64.00, 17.5' RT |
| STA. 535+64.00, 17.5' LT |

INLET AND PIPE PROTECTION

| |
|--------------------------|
| STA. 516+00.00, 34' LT |
| STA. 519+20.00, 36.2' RT |

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
EROSION AND SEDIMENT CONTROL

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: MJF
 CHECKED BY: GT

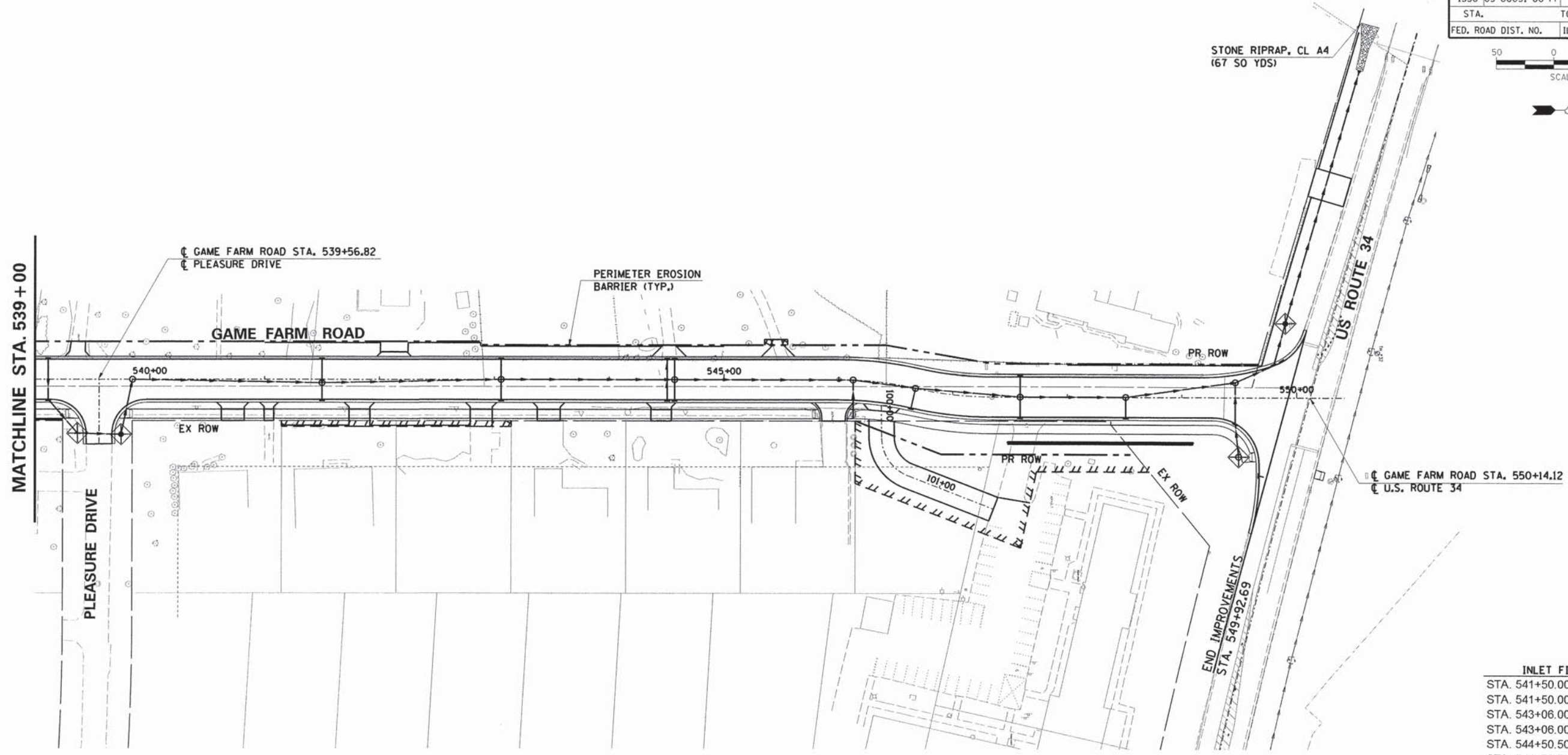
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 36 |
| STA. TO STA. | | ILLINOIS FED. AID PROJECT | | |
| FED. ROAD DIST. NO. | | CONTRACT NO. 87345 | | |



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|------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | CADD FILE NAME | | |

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|---------|-------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTAT'NS CHKD | | |

COMPANY NAME:
9/21/2014 4:02:45 PM
098_ero03.dgn



| INLET FILTERS | |
|--------------------------|--|
| STA. 541+50.00, 17.5' RT | |
| STA. 541+50.00, 17.5' LT | |
| STA. 543+06.00, 17.5' RT | |
| STA. 543+06.00, 17.5' LT | |
| STA. 544+50.50, 17.5' RT | |
| STA. 544+50.50, 17.5' LT | |
| STA. 544+57.00, 17.5' RT | |
| STA. 544+57.00, 17.5' LT | |
| STA. 546+67.32, 17.5' RT | |
| STA. 547+59.00, 17.5' RT | |
| STA. 547+59.00, 17.5' LT | |
| STA. 548+51.00, 17.5' RT | |
| STA. 549+46.51, 22.4' RT | |
| STA. 549+80.00, 28' LT | |
| STA. 549+87.29, 31.3' LT | |
| STA. 1910+03, 24.8' RT | |

| INLET AND PIPE PROTECTION | |
|---------------------------|--|
| STA. 539+37.00, 47' RT | |
| STA. 539+75.00, 48' RT | |
| STA. 549+49.55, 51.5' RT | |
| STA. 1908+69.56, 38.4' RT | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION**

**EROSION AND SEDIMENT
CONTROL**

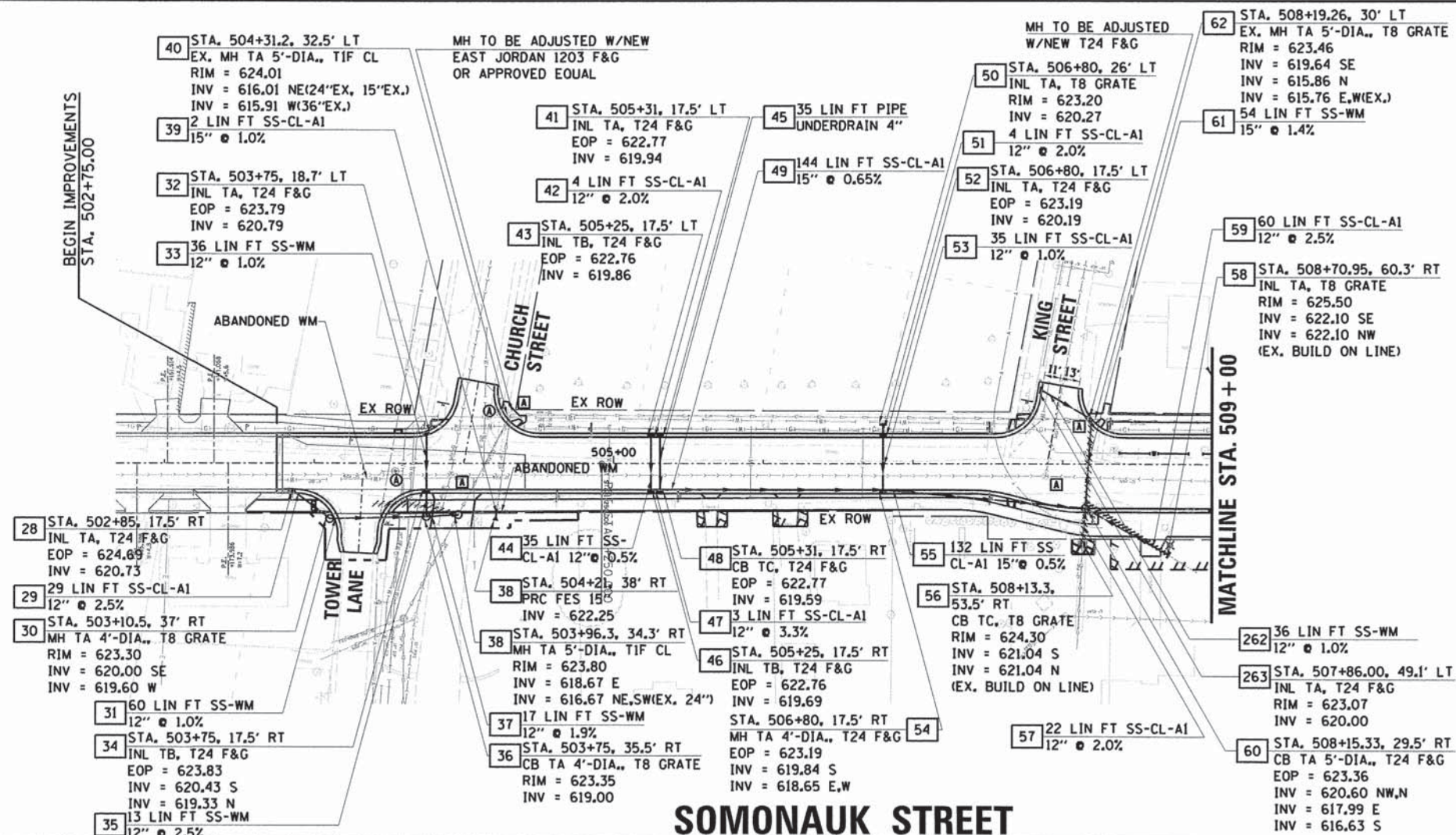
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| SCALE: VERT. HORIZ. DATE: JULY 2014 | DRAWN BY: MJF CHECKED BY: GT |
|---|---------------------------------|

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|---------------------|----------------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 37 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



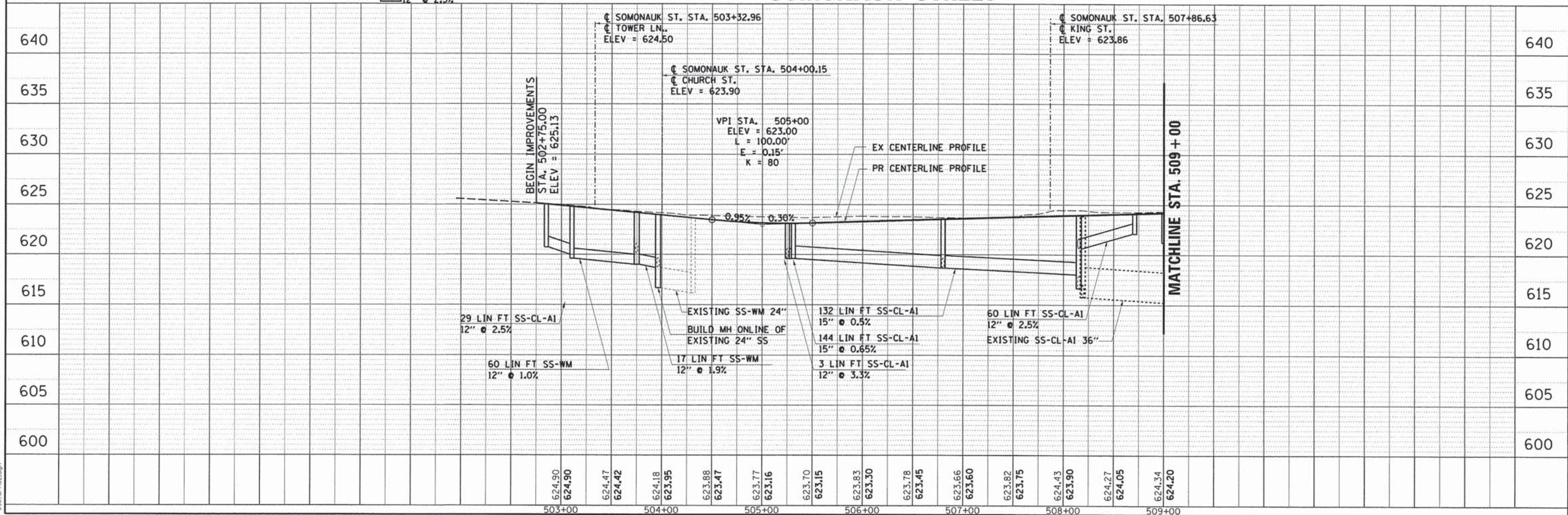
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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | NO. | | |
| | CADD FILE NAME | | |

NOTE FOR MH #38:
 STA. 503+96.3, 34.3' RT
 MH TA 5'-DIA., TIF CL
 BUILD ON EX. SS LINE



LEGEND
 STORM SEWER REMOVAL

| | | | |
|---------|--------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK GRADES CHECKED | | |
| | B.M. NOTED | | |
| | NO. | | |
| | STRUCTURE NOTATIONS CHKO | | |



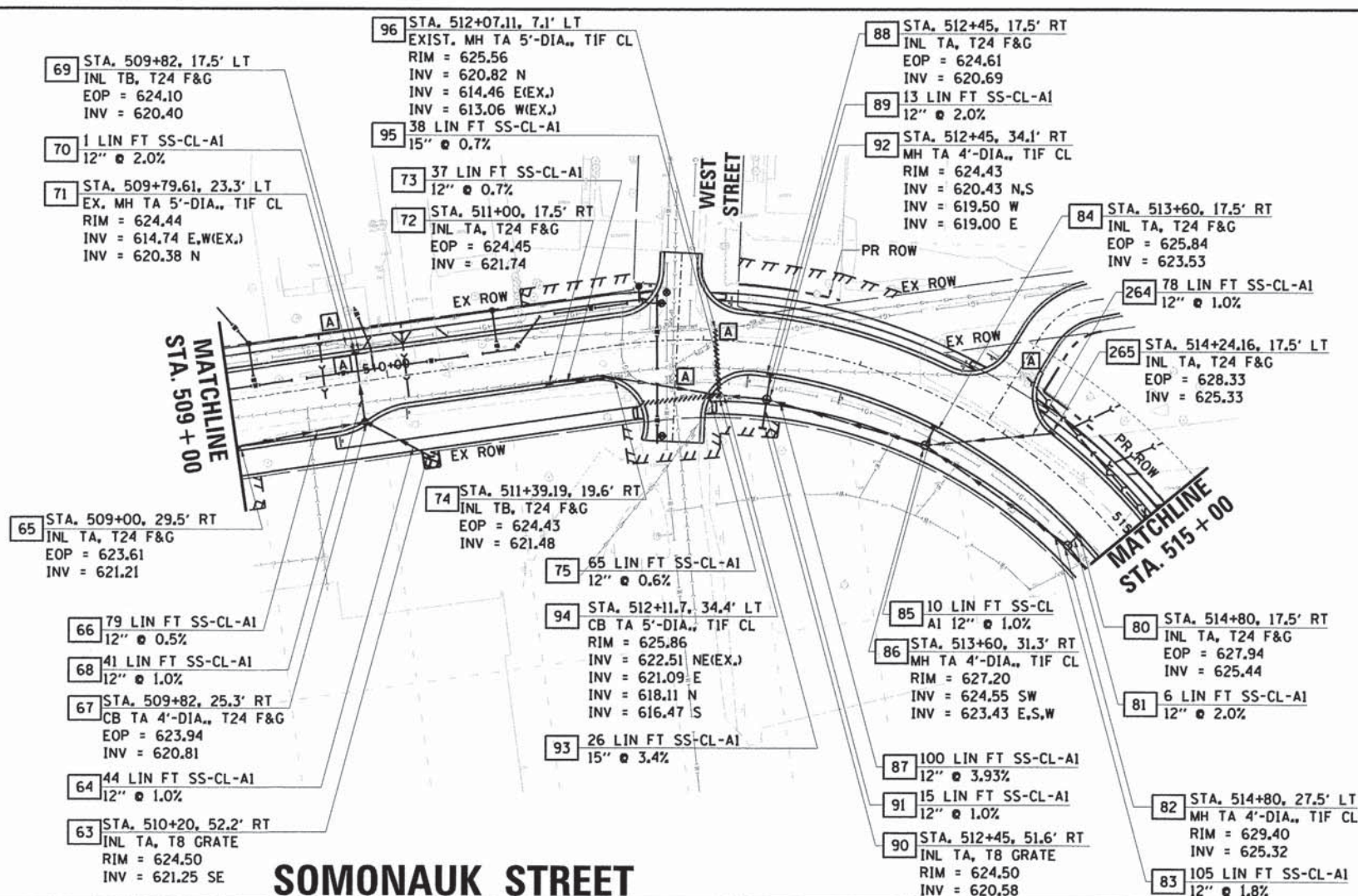
COMPANY NAME:
 8/21/2014 4:02:48 PM
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 38 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



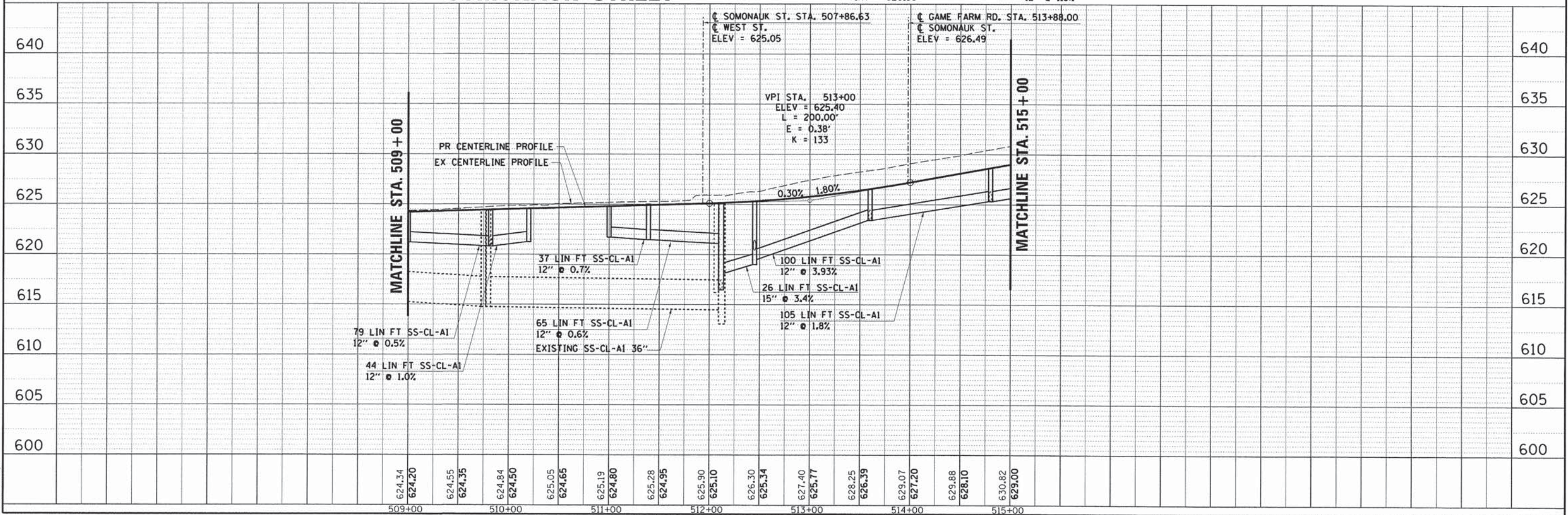
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| PLAN | SURVEYED | DATE |
| NOTE BOOK | PLOTTED | |
| NO. | RT. OF WAY CHECKED | |
| | CADD FILE NAME | |

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|-----------|--------------------------|------|
| PROFILE | SURVEYED | DATE |
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| NO. | GRADES CHECKED | |
| | B.M. NOTED | |
| | STRUCTURE NOTATIONS CHKD | |

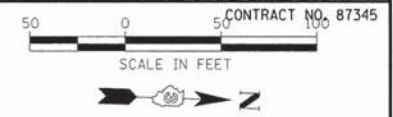


SOMONAUK STREET

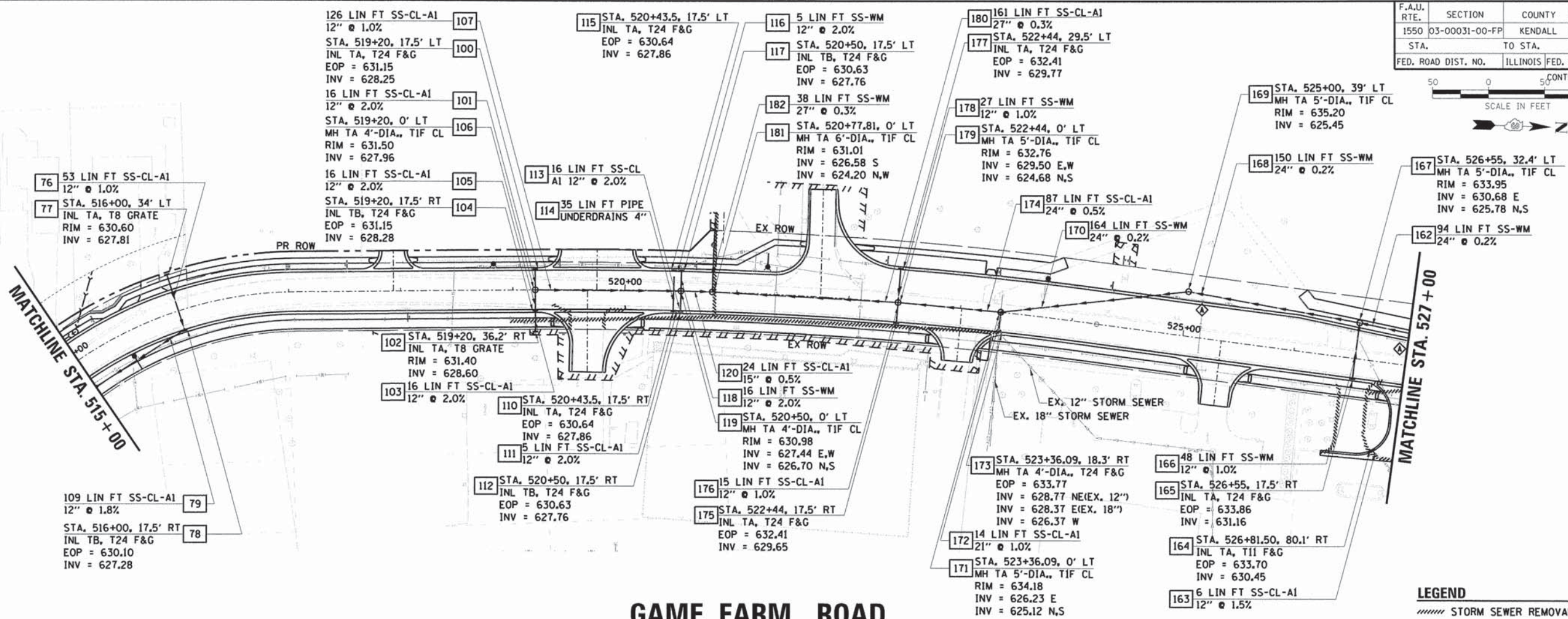
LEGEND
 ===== STORM SEWER REMOVAL



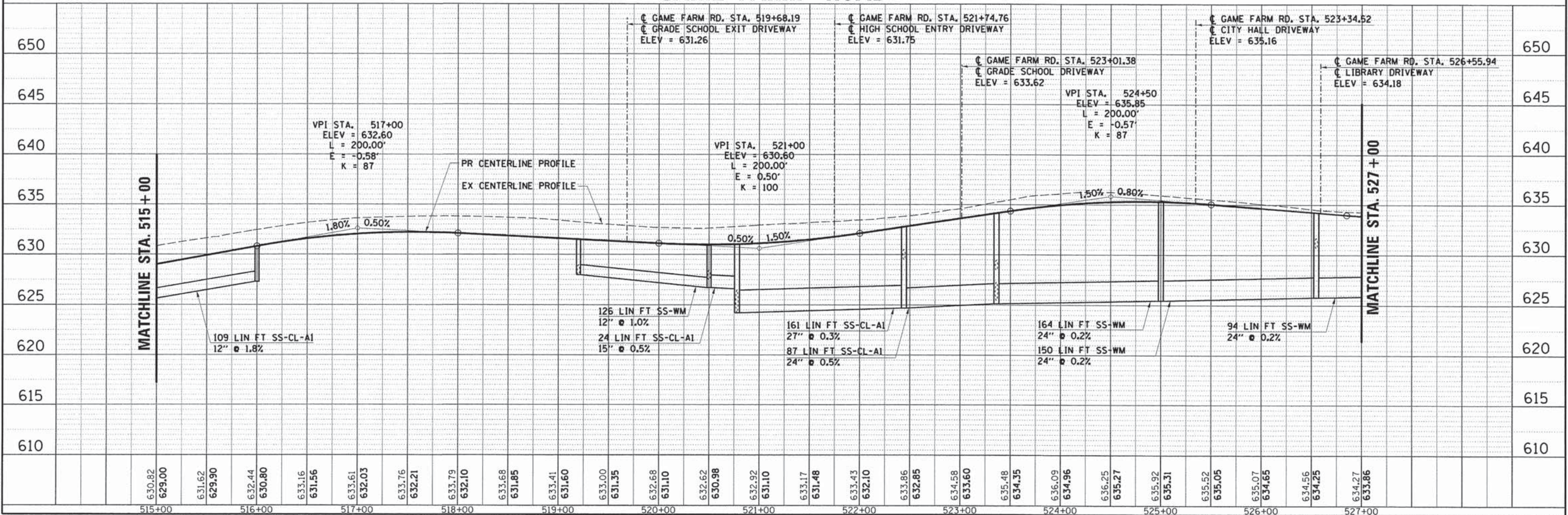
COMPANY NAME: 8/21/2014 4:05:52 PM
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| PLAN | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | RT. OF WAY CHECKED | | |
| | NO. | CAOD FILE NAME | |



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|-----------|--------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK | PLOTTED | | |
| NO. | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHKD | | |



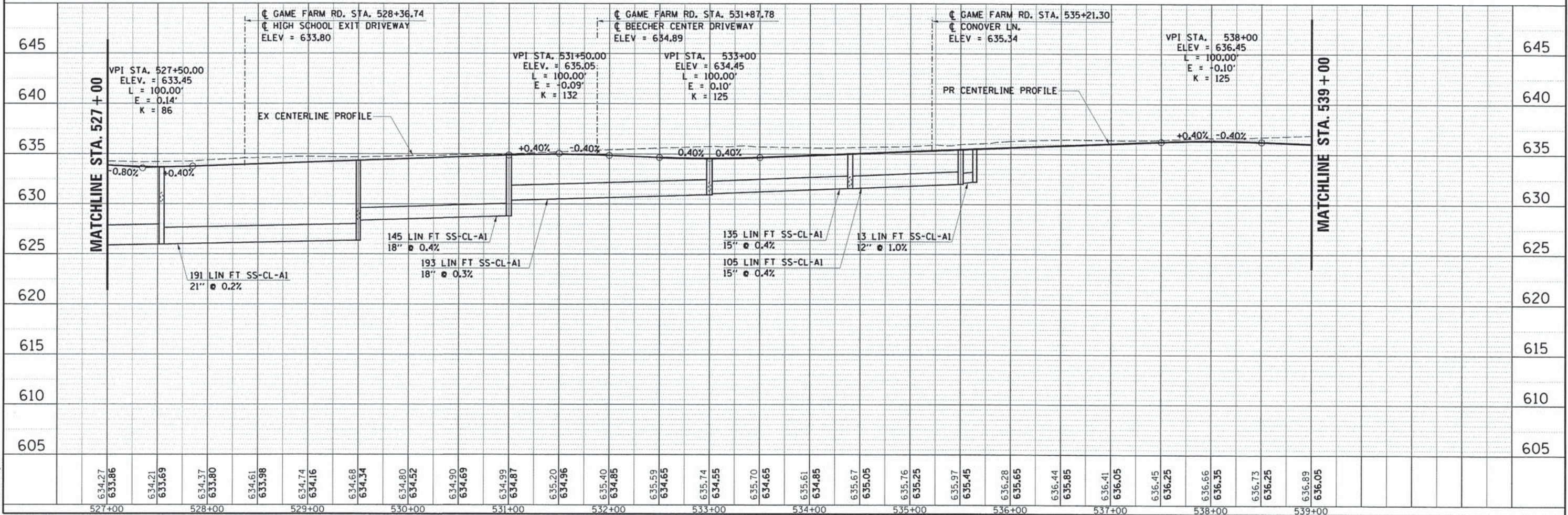
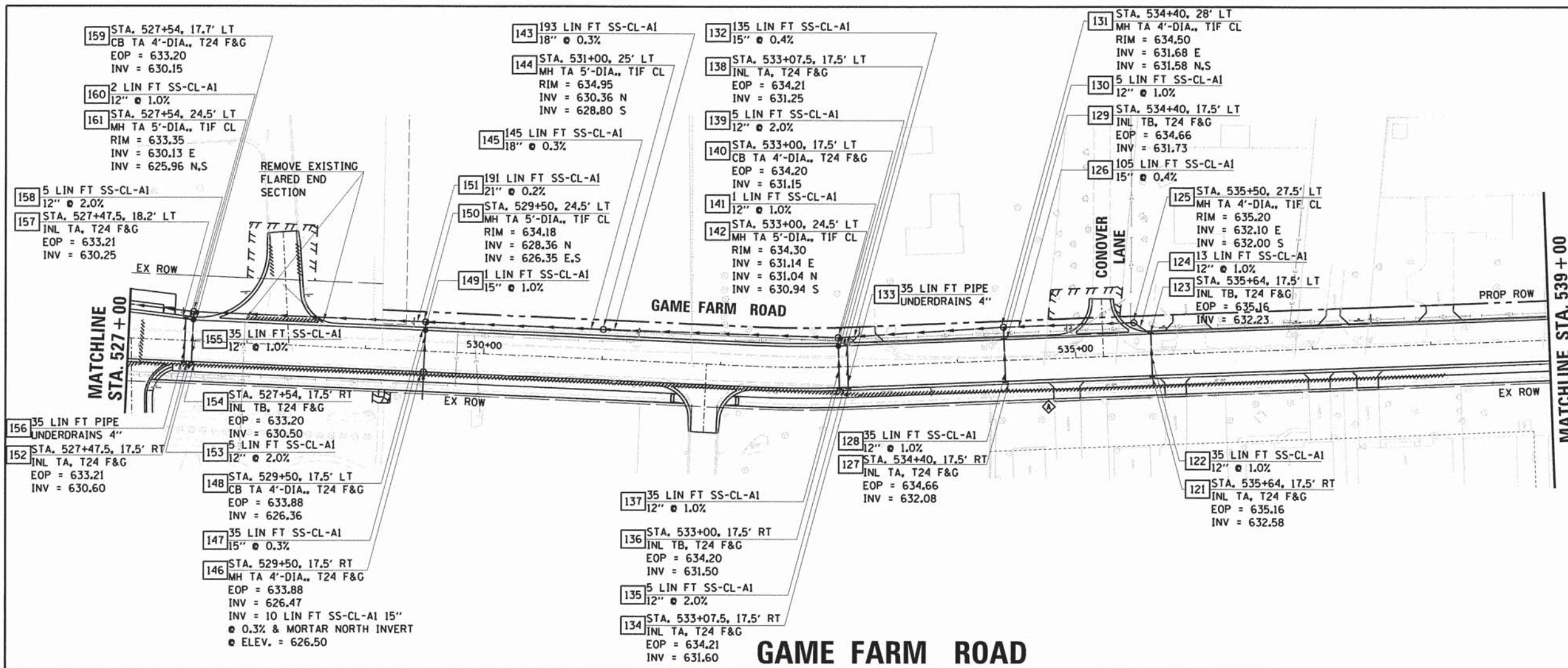
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 40 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



LEGEND
 ===== STORM SEWER REMOVAL

PLAN SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. _____
 ALIGNMENT CHECKED BY _____
 RT. OF WAY CHECKED BY _____
 CADD FILE NAME _____

PROFILE SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. _____
 GRADES CHECKED BY _____
 B.M. NOTED BY _____
 STRUCTURE NOTATIONS CHKD BY _____



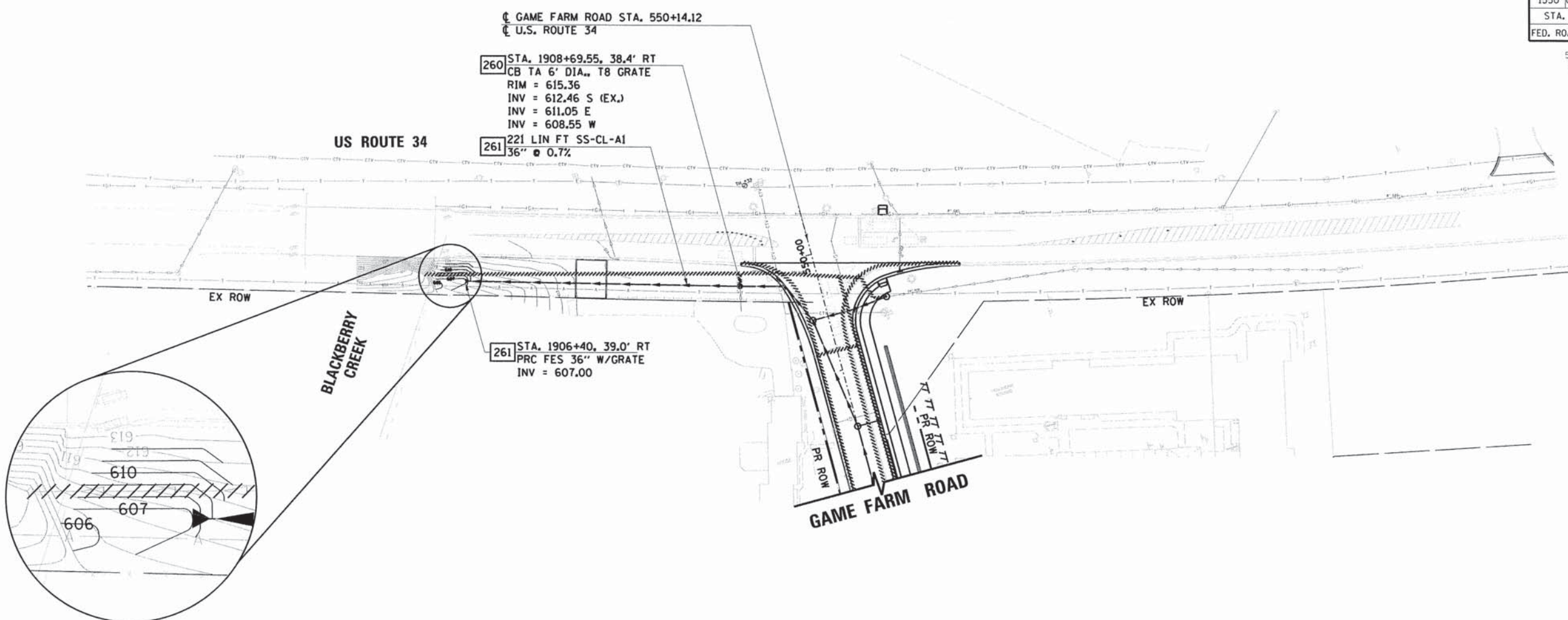
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 42 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| | | CONTRACT NO. | 87345 | |

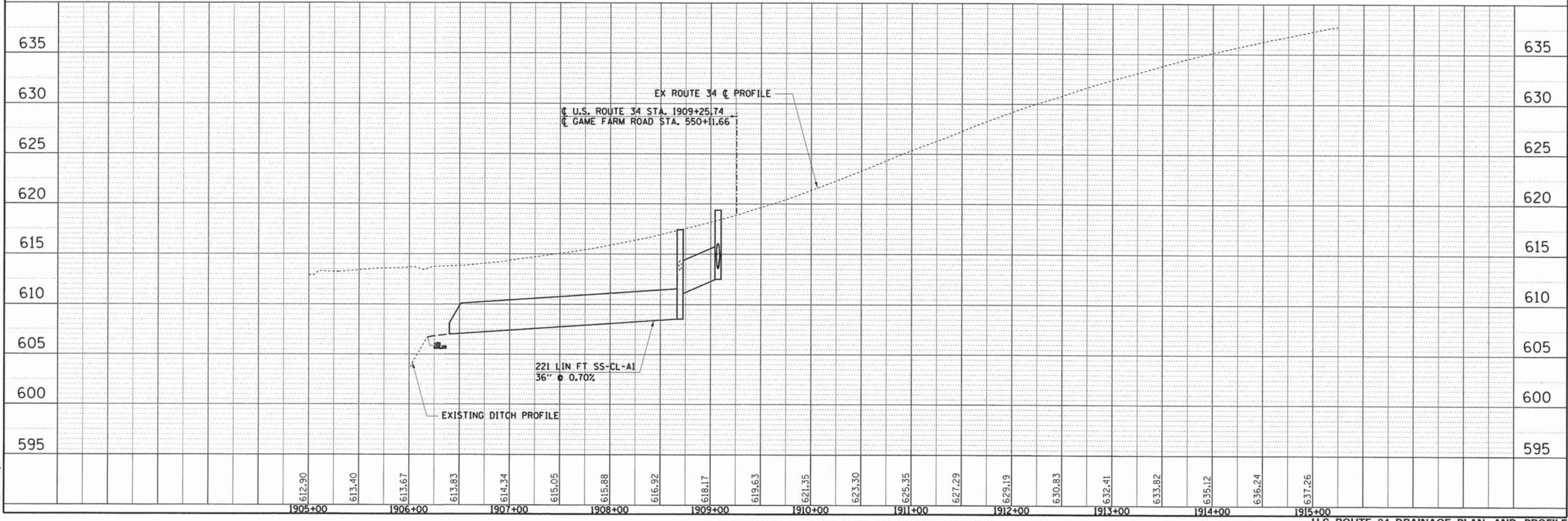


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|------|-----------------------------|----------------|
| PLAN | SURVEYED | DATE |
| | PIOTTED | |
| | NOTE BOOK ALIGNMENT CHECKED | |
| | RT. OF WAY CHECKED | |
| | NO. | CADD FILE NAME |

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|---------|--------------------------|------|
| PROFILE | SURVEYED | DATE |
| | PIOTTED | |
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| | B.M. NOTED | |
| | STRUCTURE NOTATIONS CHKD | |



LEGEND
 //// STORM SEWER REMOVAL



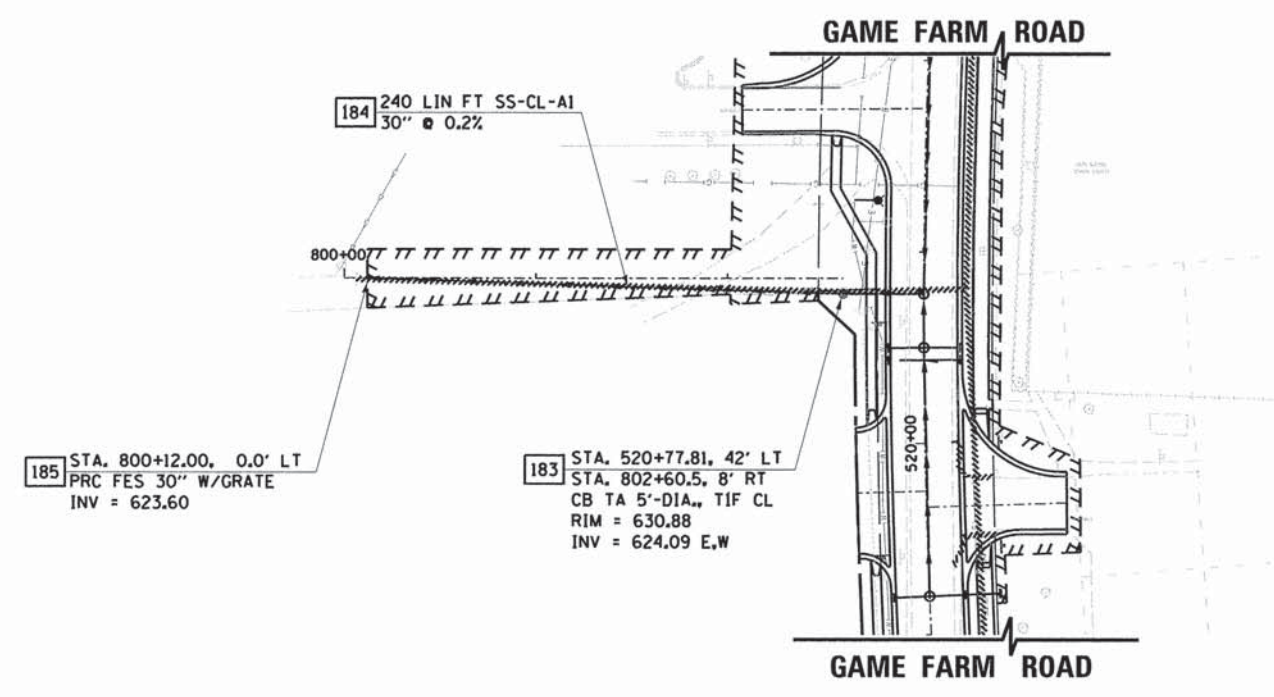
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 43 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



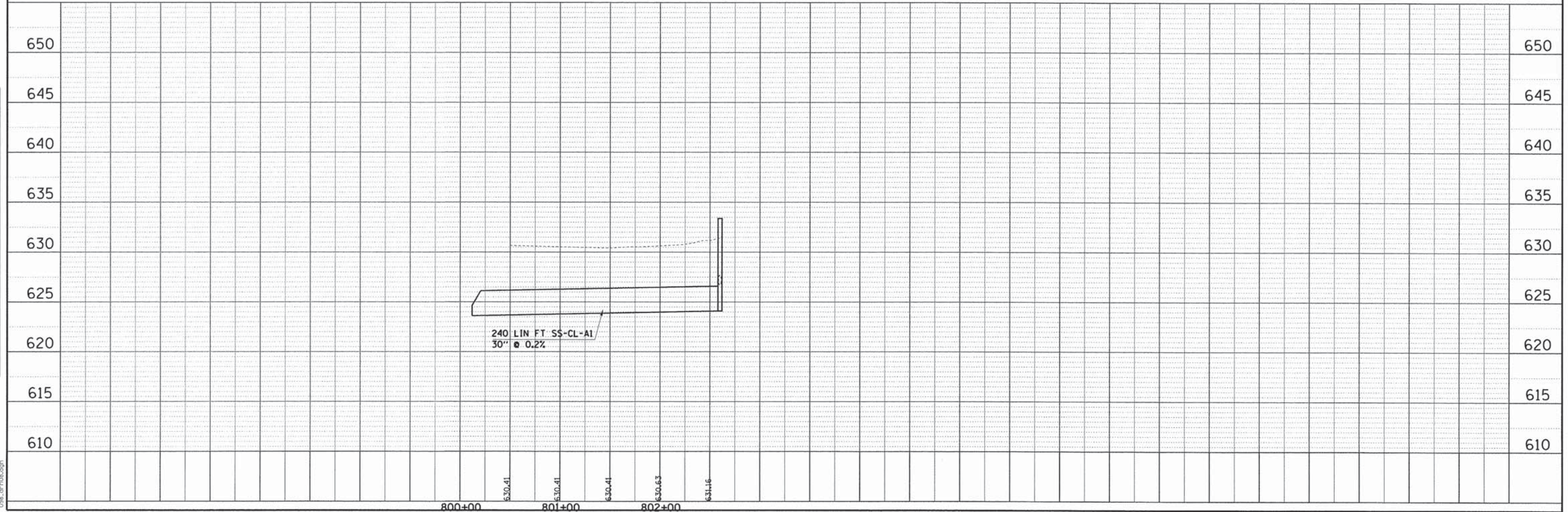
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| NOTE BOOK NO. | ALIGNED | | |
| | CHECKED | | |
| | BY | | |
| | FILE NAME | | |

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| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK NO. | CHECKED | | |
| | BY | | |
| | NOTATIONS | | |



STORM SEWER OUTFALL

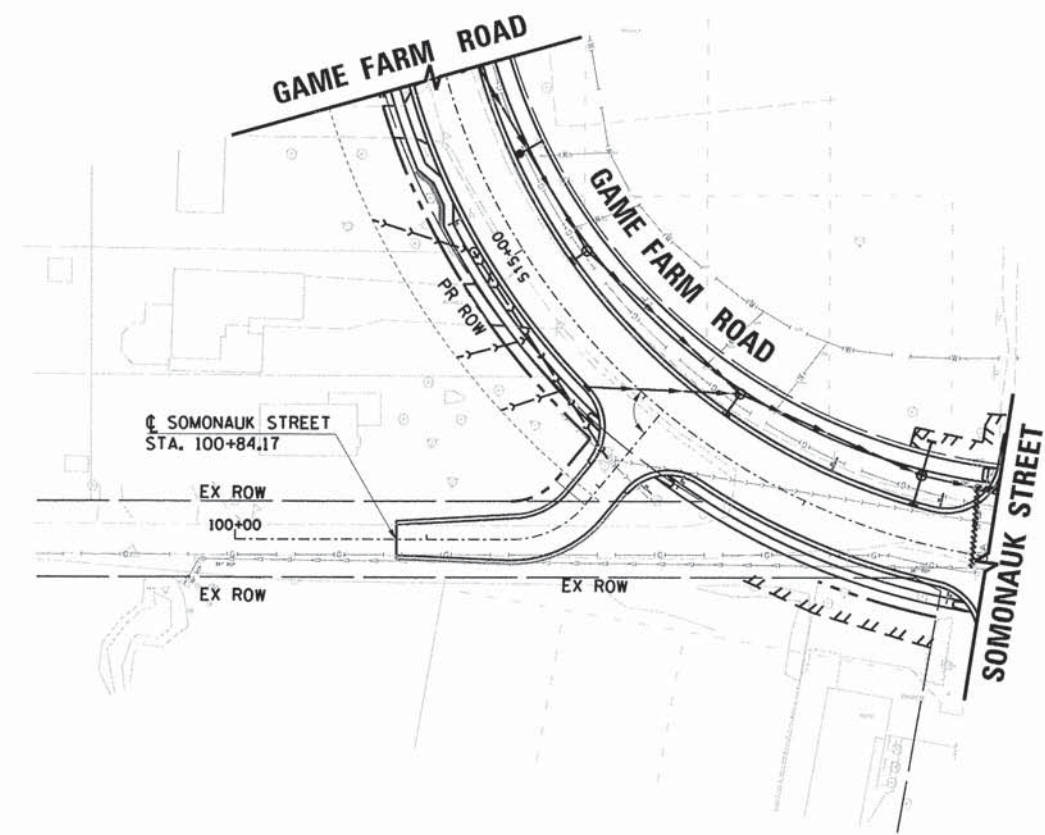
LEGEND
 //// STORM SEWER REMOVAL



COMPANY NAME:
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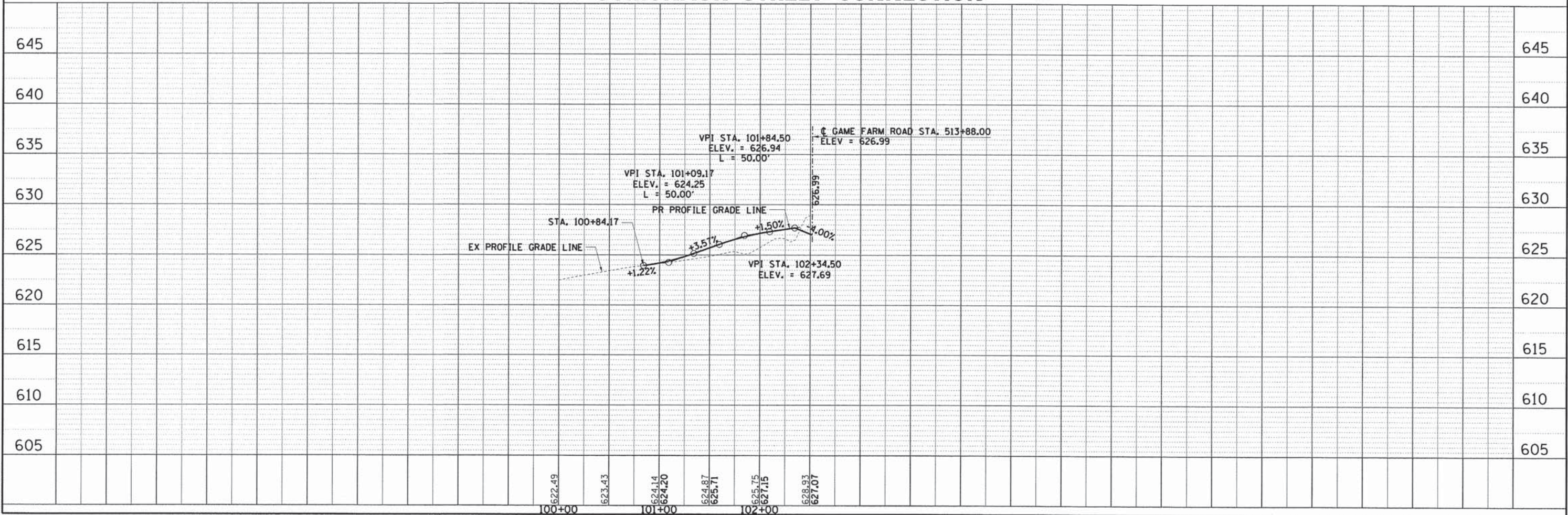
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 44 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

CONTRACT NO. 87345



SOMONAUK STREET CONNECTION

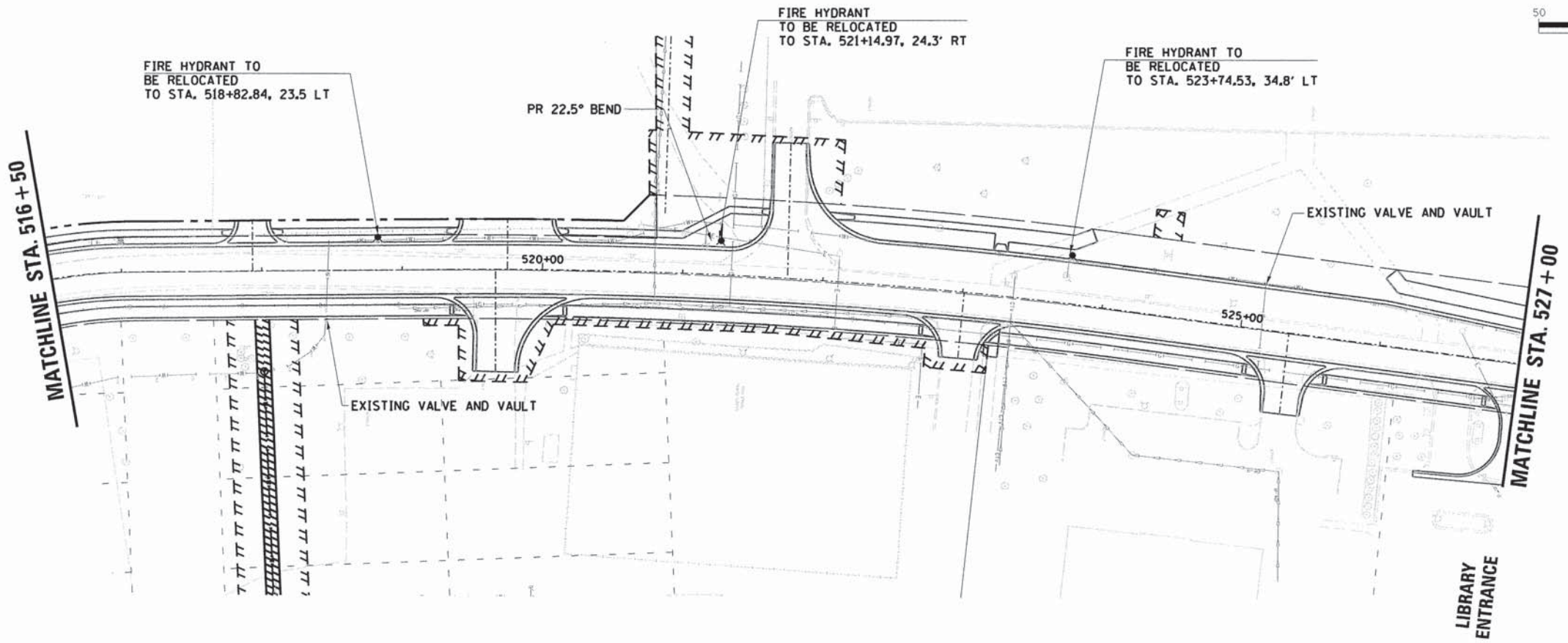
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 — STORM SEWER REMOVAL



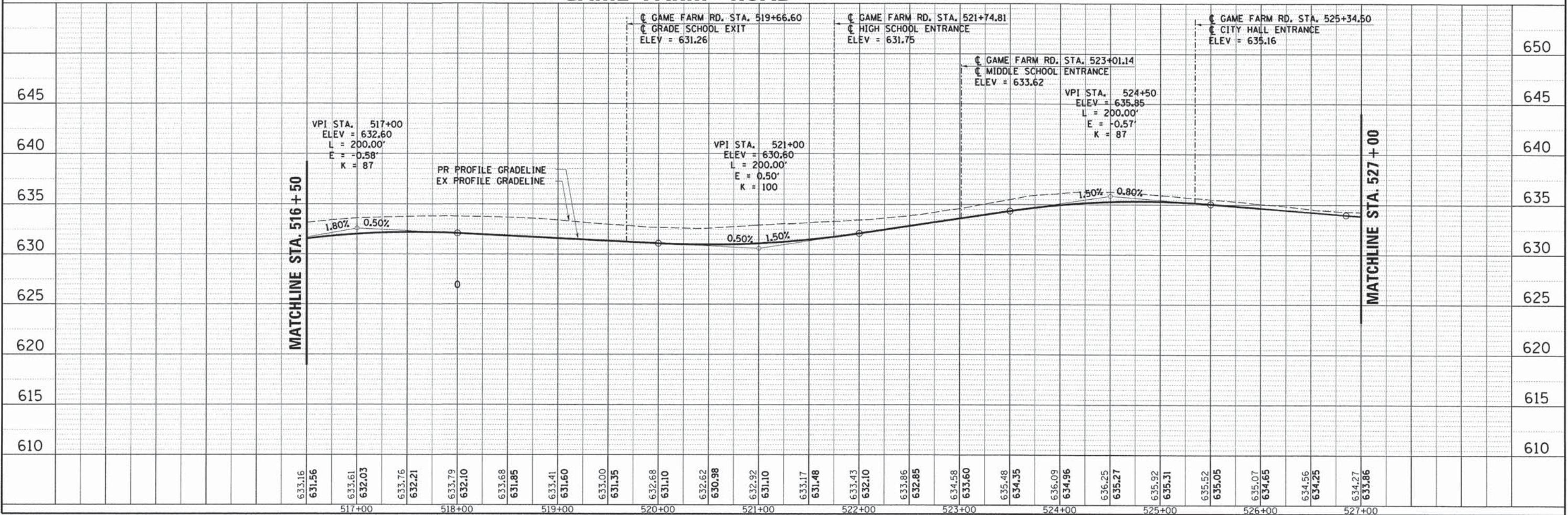
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| PLAN | SURVEYED | BY | DATE |
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| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | CADD FILE NAME | | |

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|---------|-----------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHECKED | | |

COMPANY NAME: 8/27/2014 4:03:12 PM 098_drn09.dgn



GAME FARM ROAD



| | | | |
|------|--------------------|----|------|
| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | NO. | | |
| | CADD FILE NAME | | |

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|---------|--------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHKD | | |
| | NO. | | |

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|----------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 47 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



MATCHLINE STA. 520+00

YORKVILLE MIDDLE SCHOOL

YORKVILLE GRADE SCHOOL

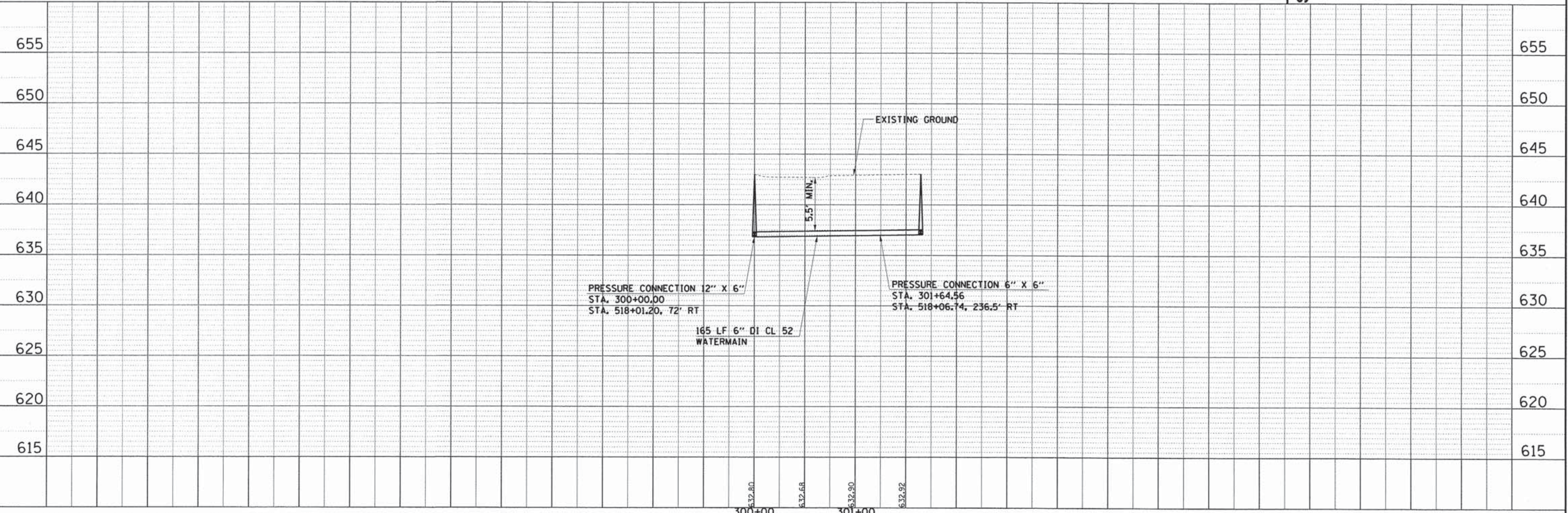
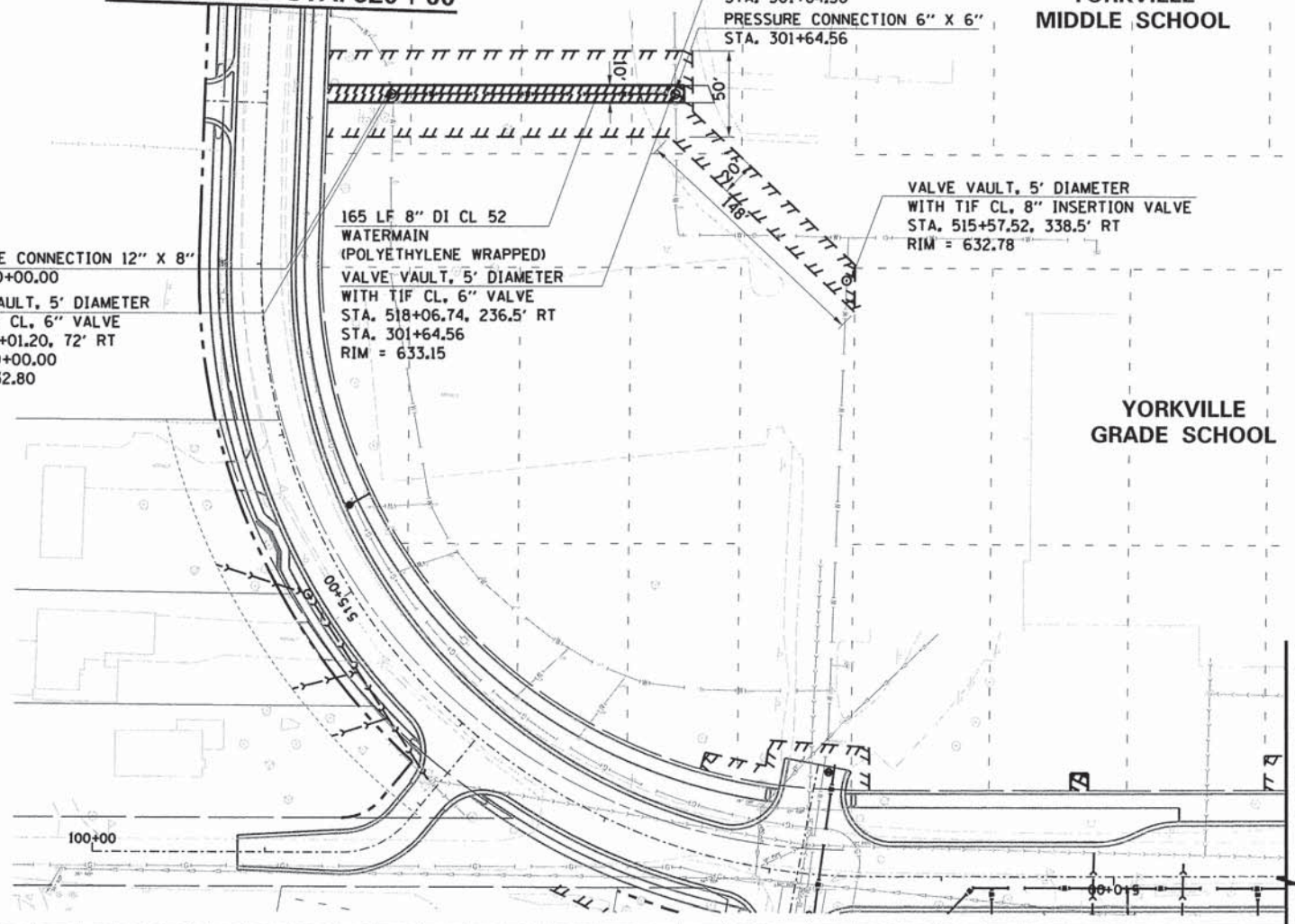
SOMONAUK STREET

PRESSURE CONNECTION 12" X 8"
STA. 300+00.00
VALVE VAULT, 5' DIAMETER
WITH TIF CL. 6" VALVE
STA. 518+01.20, 72' RT
STA. 300+00.00
RIM = 632.80

165 LF 8" DI CL 52
WATERMAIN
(POLYETHYLENE WRAPPED)
VALVE VAULT, 5' DIAMETER
WITH TIF CL. 6" VALVE
STA. 518+06.74, 236.5' RT
STA. 301+64.56
RIM = 633.15

CONNECTION 8" X 6" REDUCER
STA. 301+64.56
PRESSURE CONNECTION 6" X 6"
STA. 301+64.56

VALVE VAULT, 5' DIAMETER
WITH TIF CL. 8" INSERTION VALVE
STA. 515+57.52, 338.5' RT
RIM = 632.78



| PLAN | SURVEYED | BY | DATE |
|-----------|----------------|----|------|
| NOTE BOOK | ALIGNED | | |
| NO. | CHECKED | | |
| | RT. OF WAY | | |
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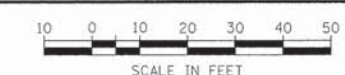
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| NOTE BOOK | GRADES | | |
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| | STRUCTURE NOTATIONS | | |
| | CHKD | | |

COMPANY NAME: 8/20/2018 4:03:51 PM
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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | ALIGNMENT CHECKED | | |
| | RT. OF WAY CHECKED | | |
| | NO. | | |
| | CADD FILE NAME | | |

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|---------|--------------------------|----|------|
| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHKD | | |
| | NO. | | |

COMPANY NAME:
8/21/2014 4:03:24 PM
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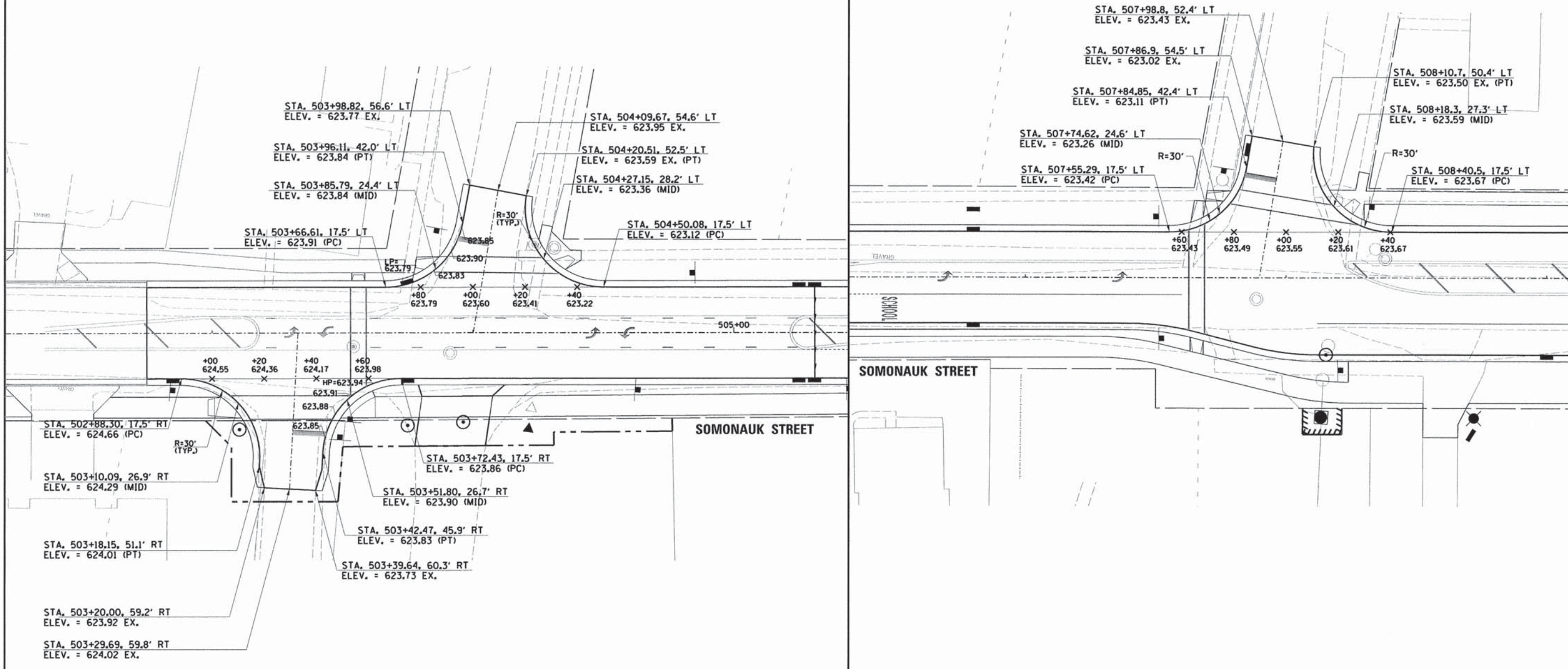
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 48 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 87345 | | | | |



KING STREET = STA. 507 + 89.76

CHURCH STREET = STA. 504 + 00.15

TOWER LANE = STA. 503 + 32.96



ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
INTERSECTION PAVEMENT
ELEVATION PLAN
(SHEET 1 OF 7)

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

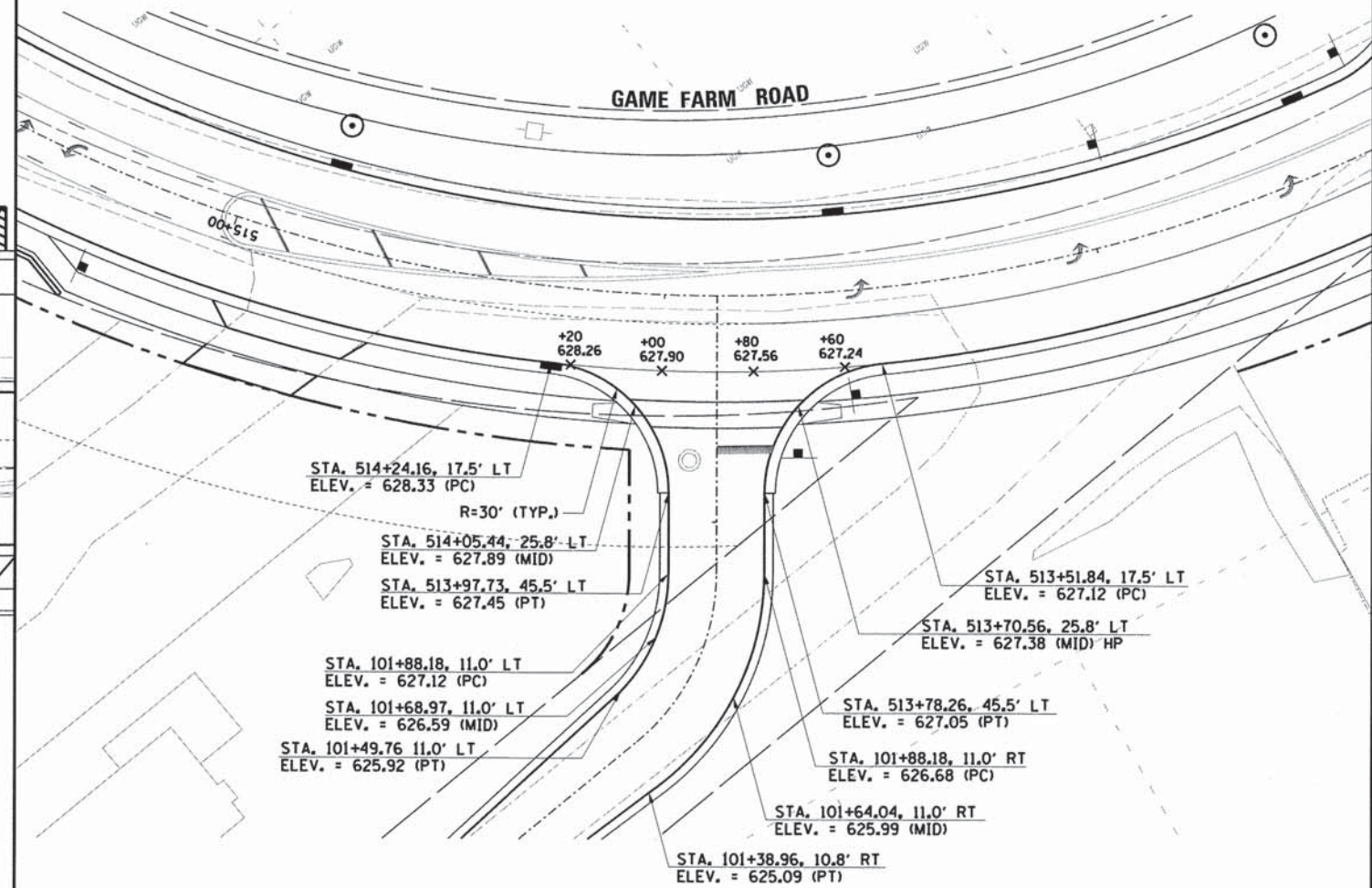
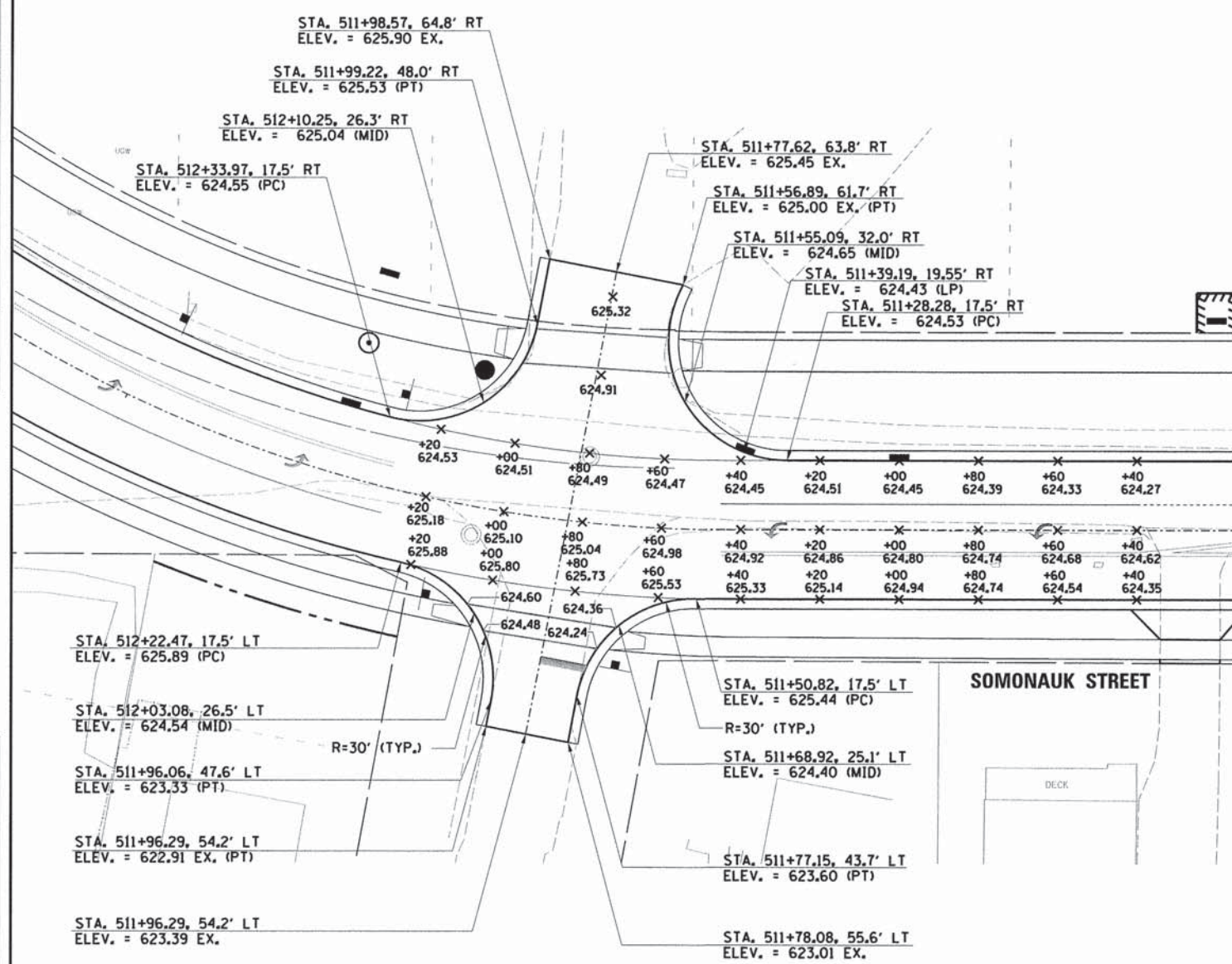
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CHECKED BY: GT

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 49 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |



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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES | | |
| | STRUCTURE NOTATIONS | | |
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| | STRUCTURE NOTATIONS | | |



SOMONAUK STREET = STA. 513 + 88.00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 INTERSECTION PAVEMENT
 ELEVATION PLAN
 (SHEET 2 OF 7)
 SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014
 DRAWN BY: M.J.F.
 CHECKED BY: G.T.

COMPANY NAME: _____
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 50 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

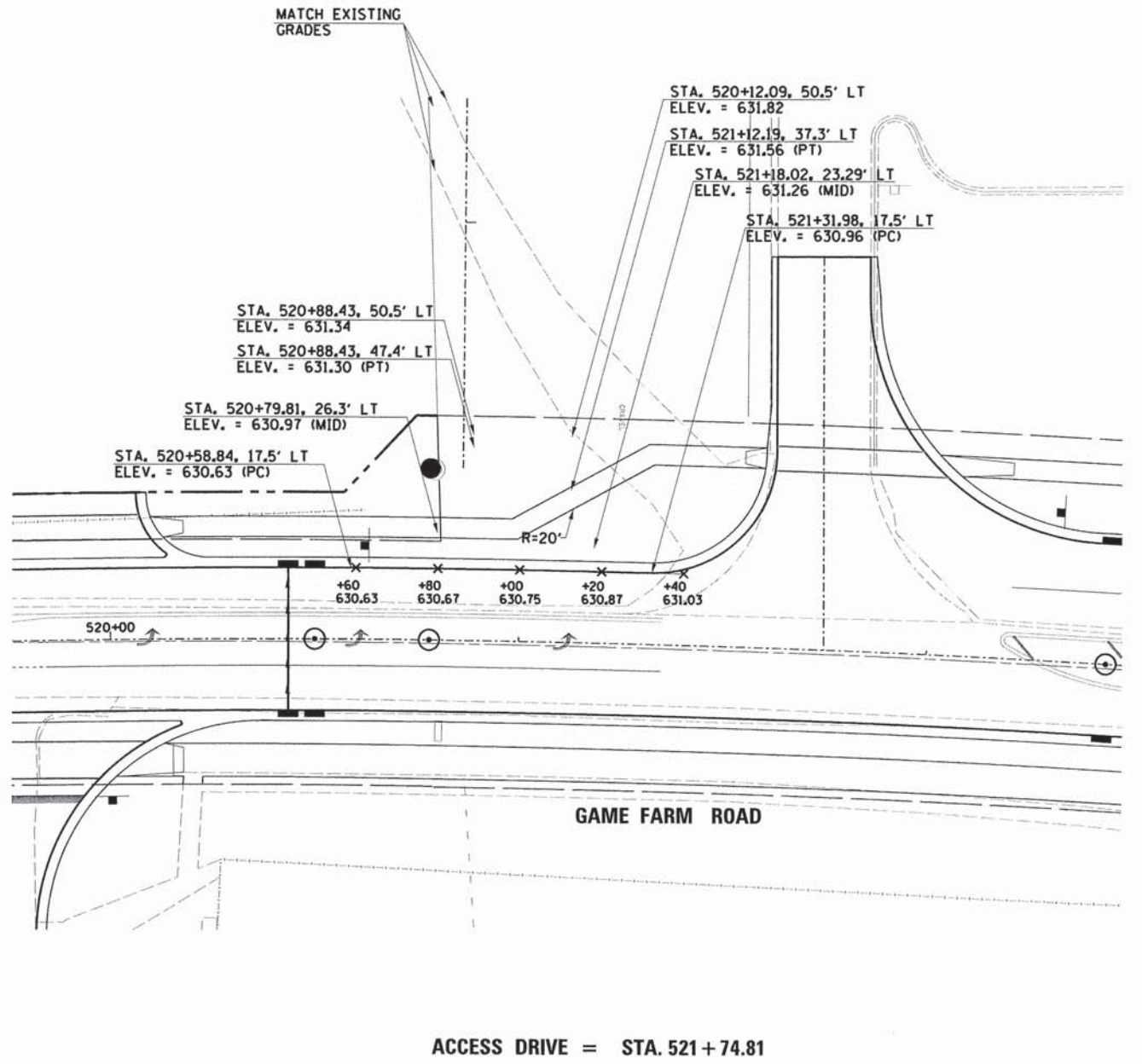
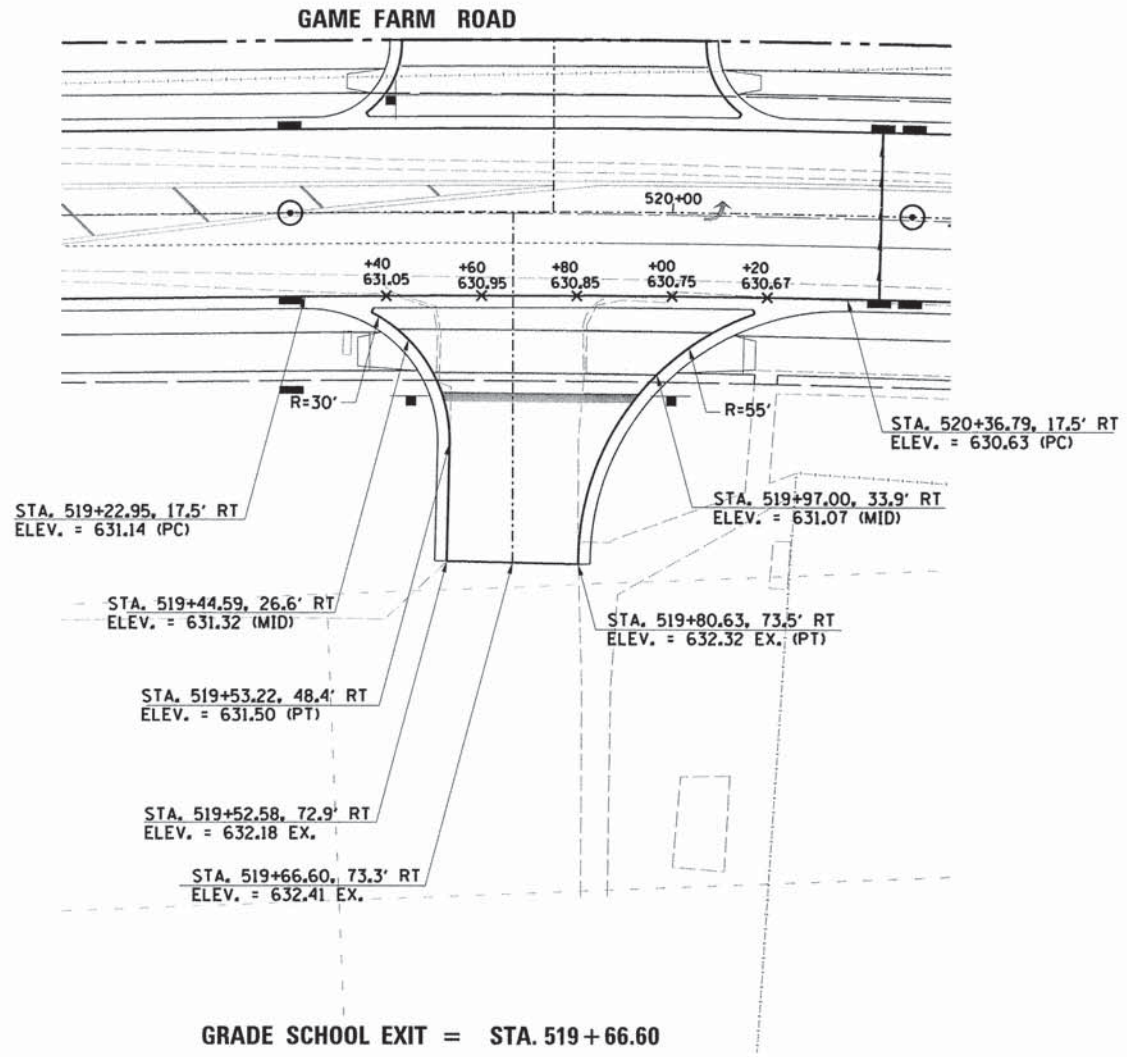


CONTRACT NO. 87345

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| NOTE BOOK NO. | PLOTTED | | |
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| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHKD | | |

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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
INTERSECTION PAVEMENT
ELEVATION PLAN
(SHEET 3 OF 7)

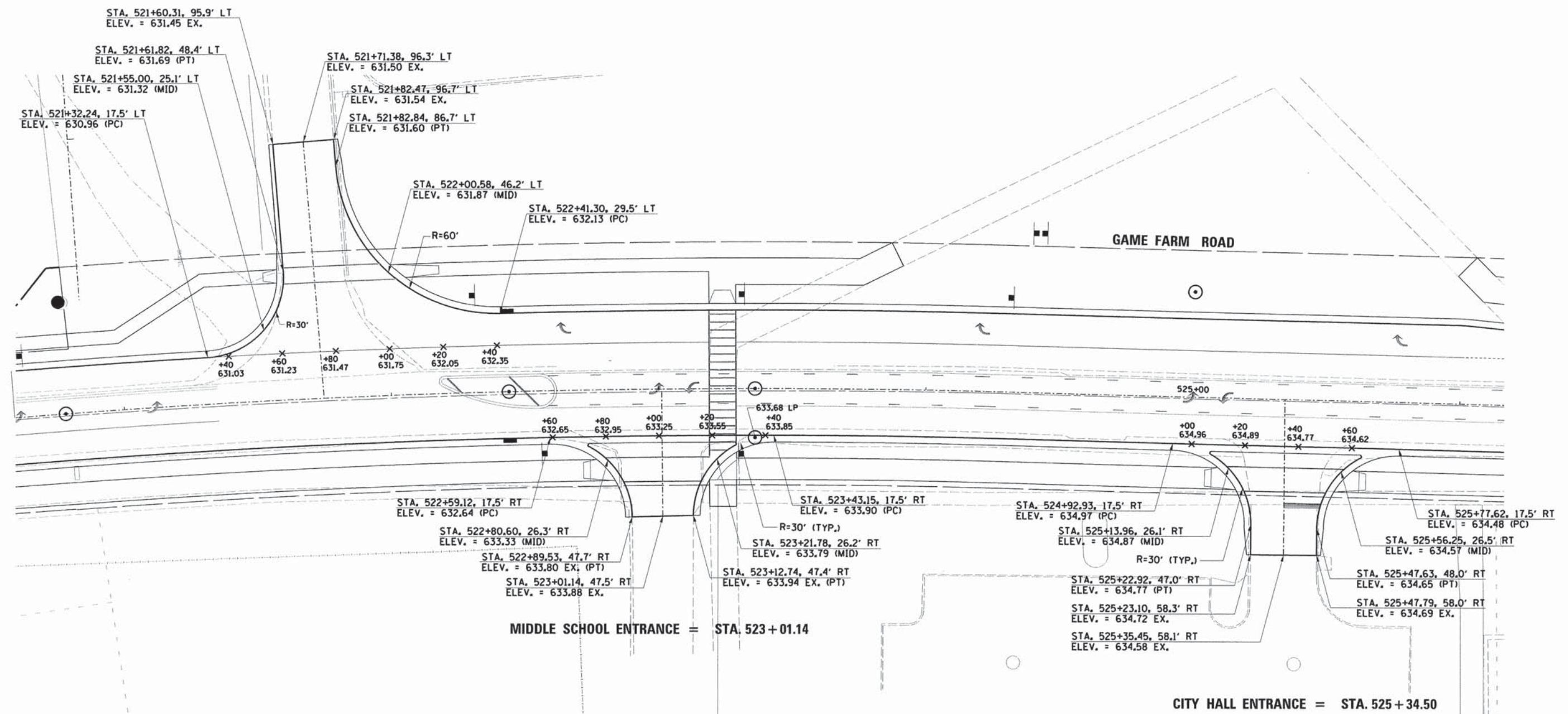
SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: MJF
CHECKED BY: GT

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|---------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 51 |
| STA. _____ | | TO STA. _____ | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |



HIGH SCHOOL ENTRANCE = STA. 521 + 74.81



| PLAN | SURVEYED | DATE |
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| NOTE BOOK NO. _____ | BY _____ | DATE _____ |
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| STRUCTURE NOTATIONS CHRD _____ | | |
| BY _____ | | |

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ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 INTERSECTION PAVEMENT
 ELEVATION PLAN
 (SHEET 4 OF 7)**

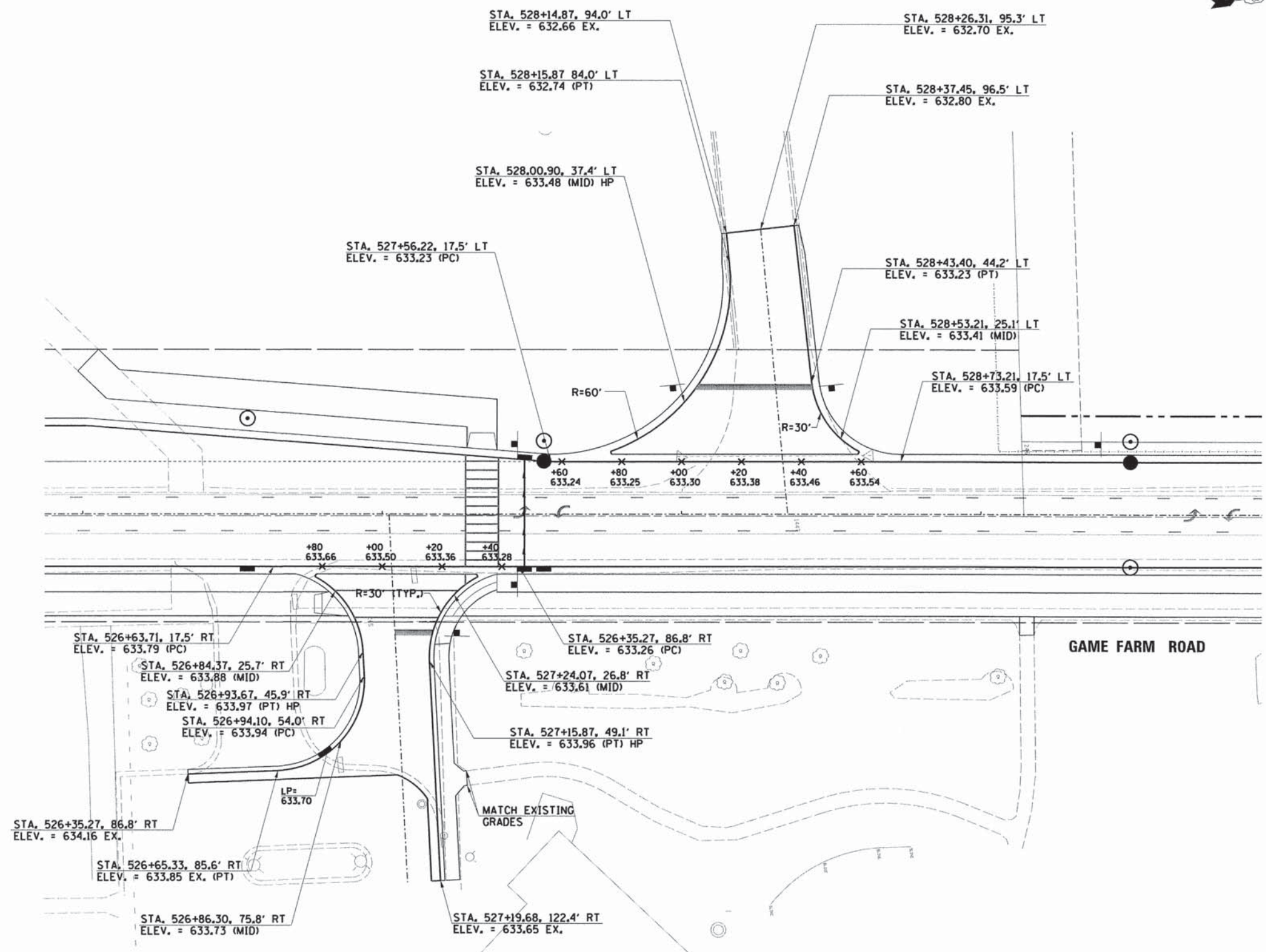
SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: MJF
 CHECKED BY: GT

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| 1550 | 03-00031-00-FP | KENDALL | 111 | 52 |
| STA. _____ TO STA. _____ | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |



HIGH SCHOOL EXIT = STA. 528 + 35.46



LIBRARY ENTRANCE = 525 + 02.27

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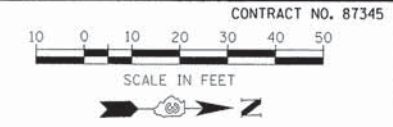
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
INTERSECTION PAVEMENT ELEVATION PLAN
 (SHEET 5 OF 7)

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: M.J.F.
 CHECKED BY: GT

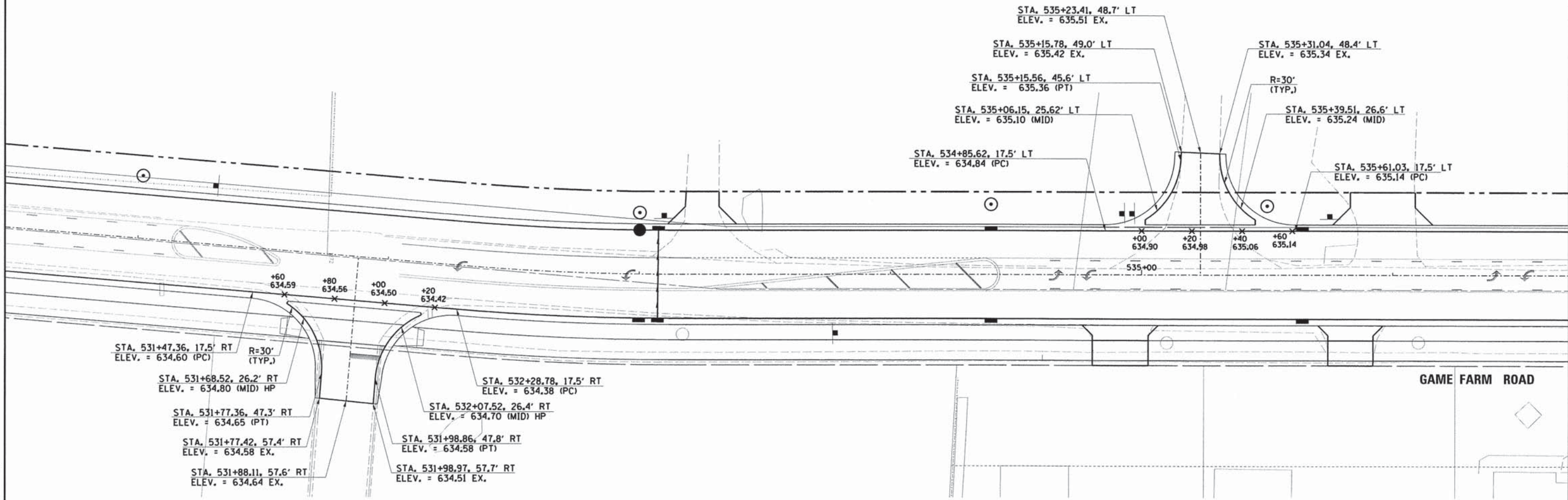
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 53 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



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| | NOTE BOOK GRADES CHECKED | | |
| | B.M. NOTED | | |
| | NO. _____ | STRUCTURE NOTATIONS CHKD | |

CONOVER LANE = STA. 535 + 23.41



BEECHER CENTER ENTRANCE = STA. 531 + 88.11

GAME FARM ROAD

COMPANY NAME:
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
INTERSECTION PAVEMENT
ELEVATION PLAN
(SHEET 6 OF 7)

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

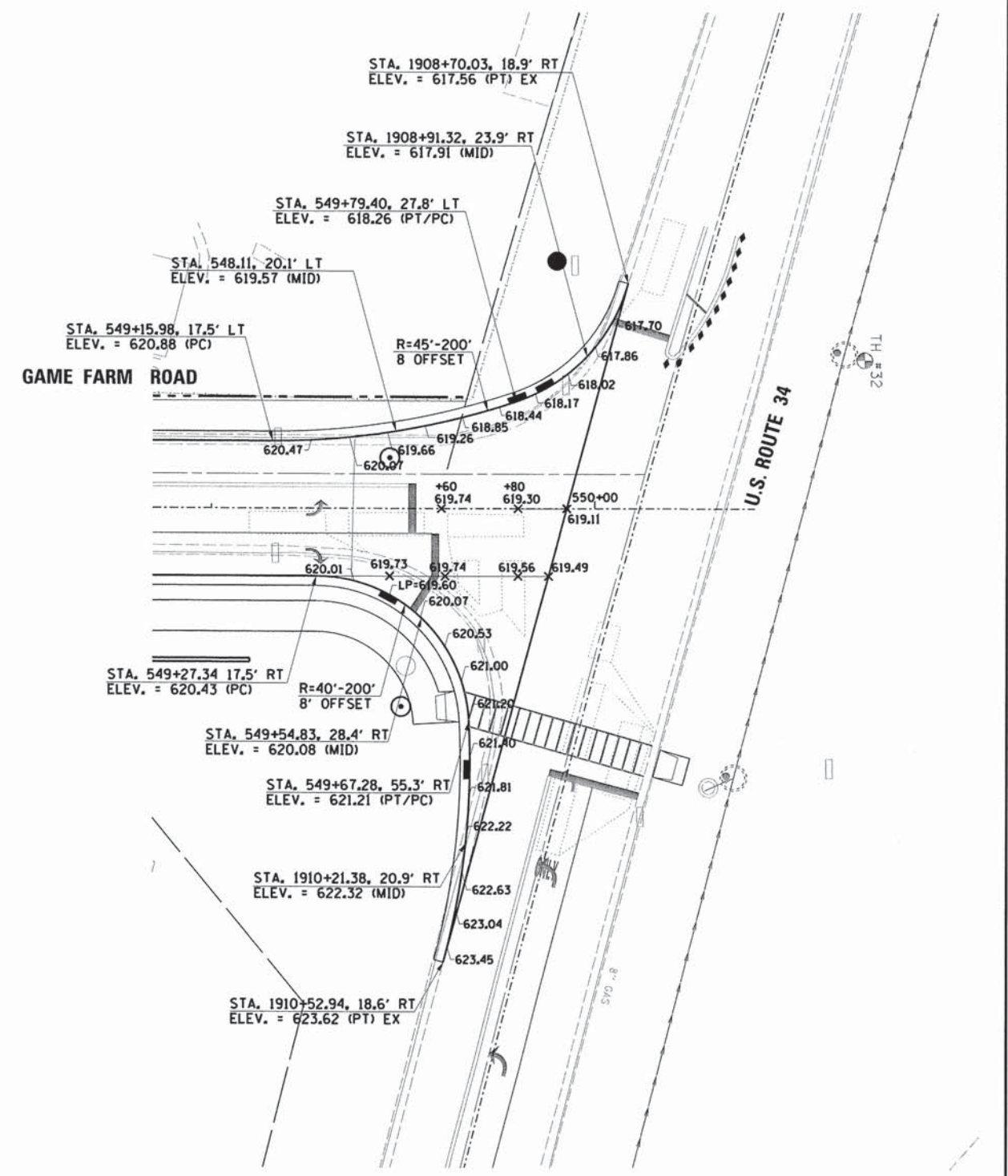
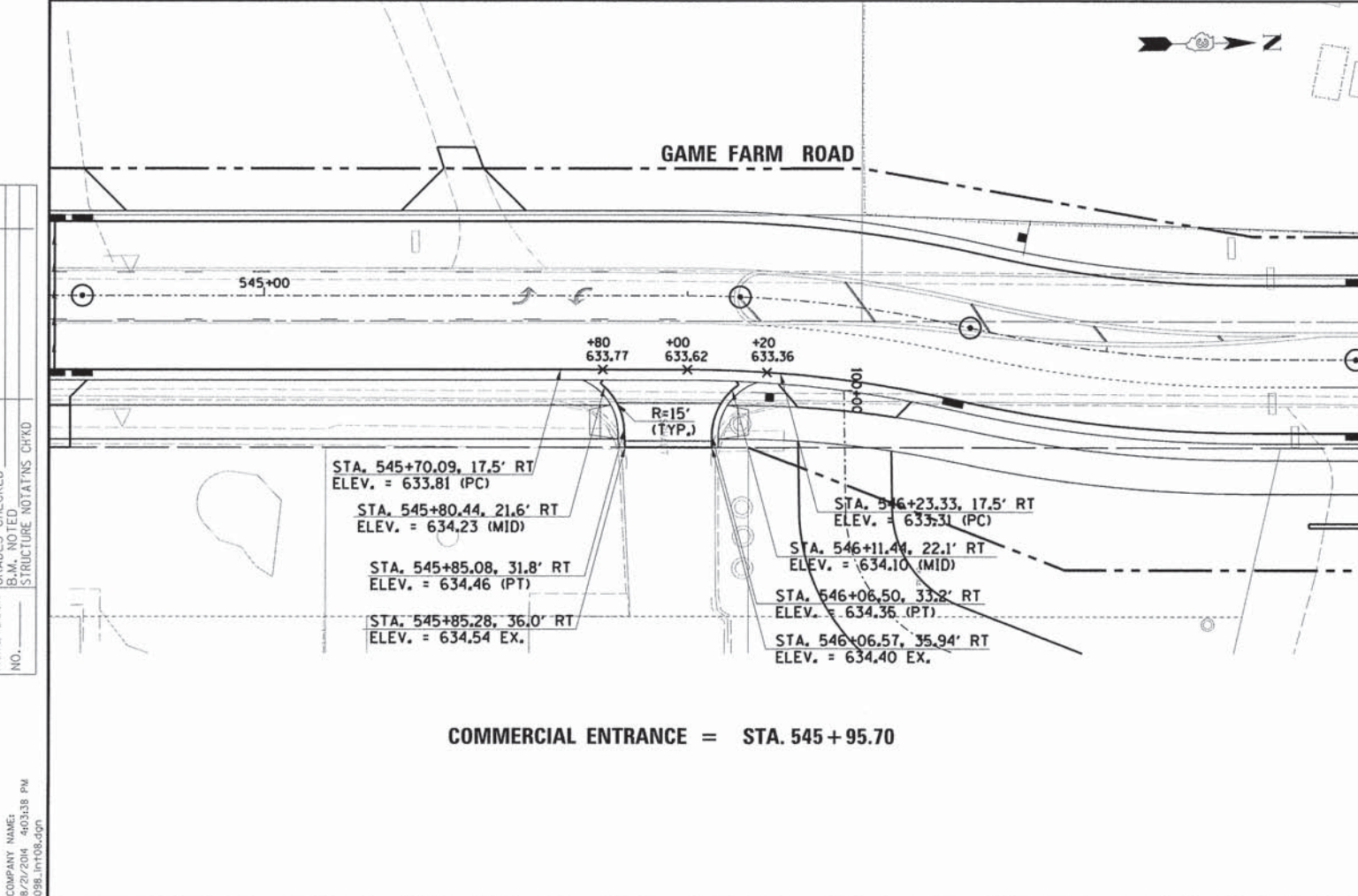
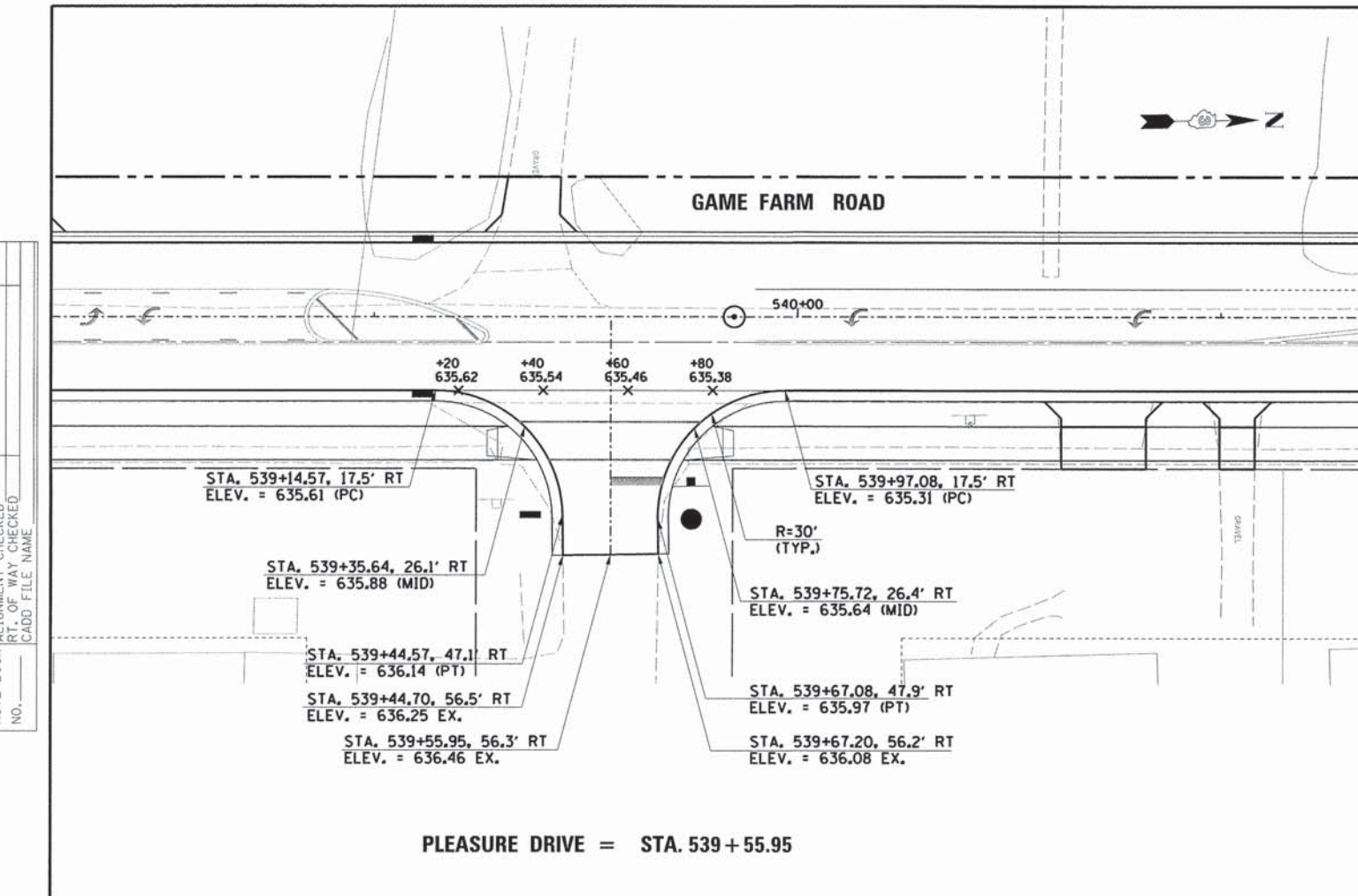
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 54 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



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| | STRUCTURE NOTATIONS CHECKED | | |



ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION**
 INTERSECTION PAVEMENT
 ELEVATION PLAN
 (SHEET 7 OF 7)

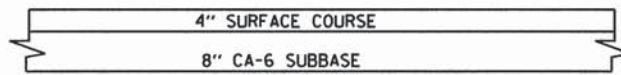
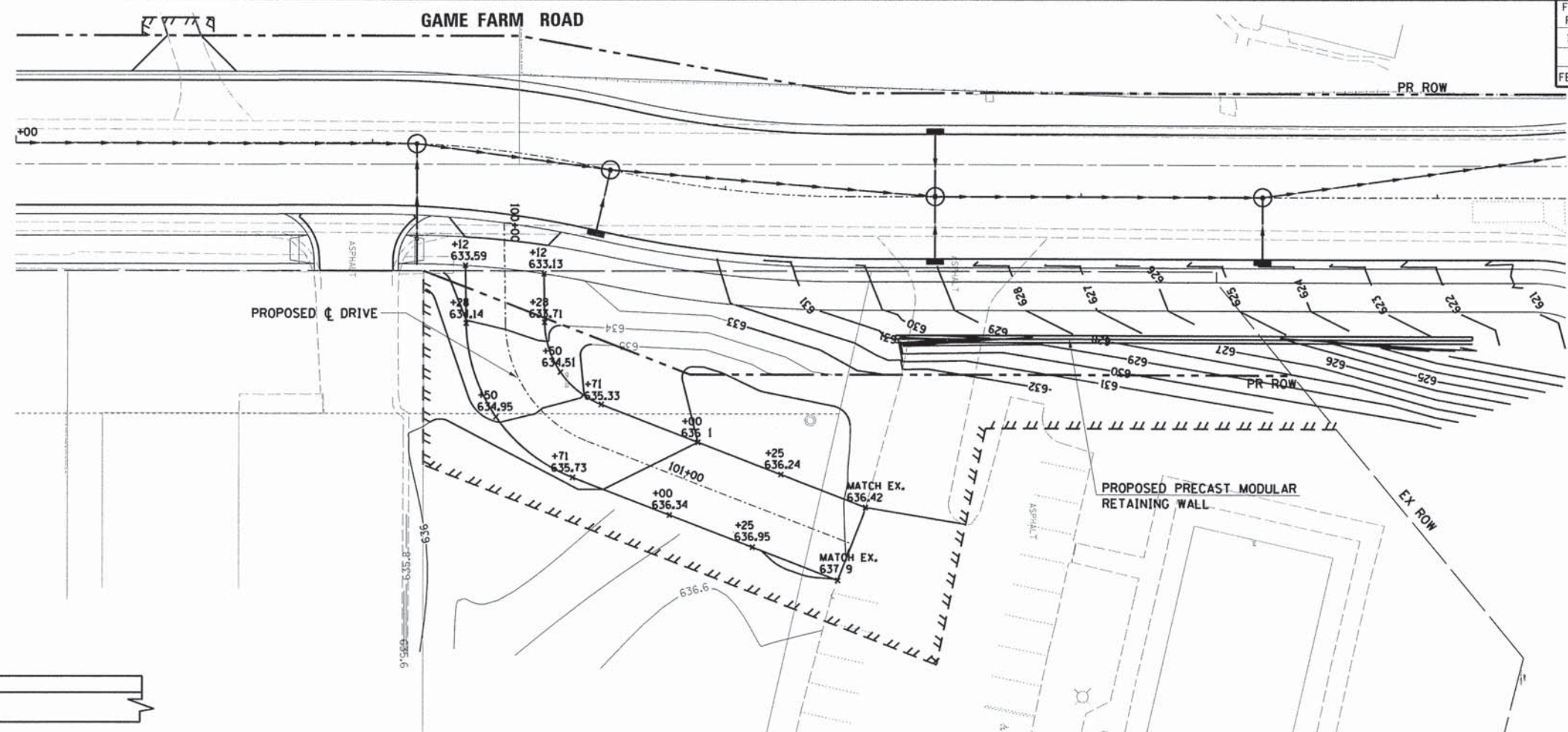
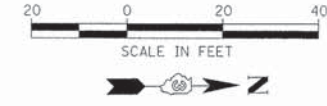
SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 55 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |

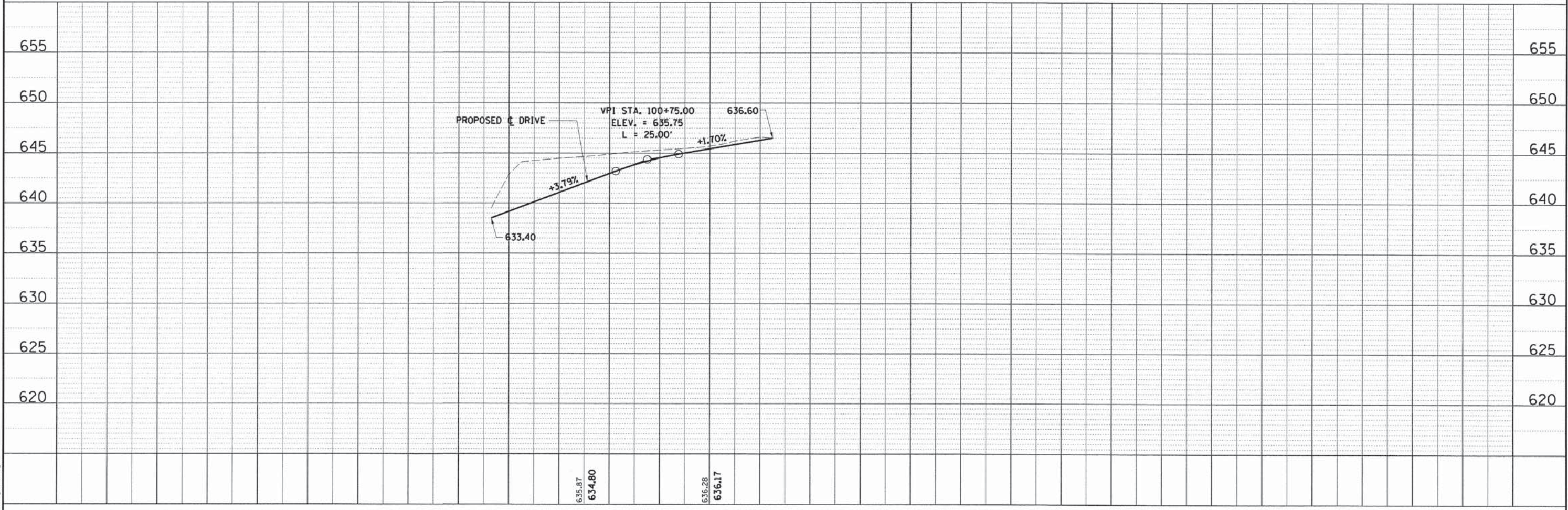
CONTRACT NO. 87345



ENTRANCE TYPICAL SECTION

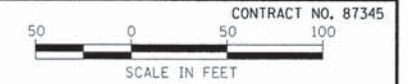
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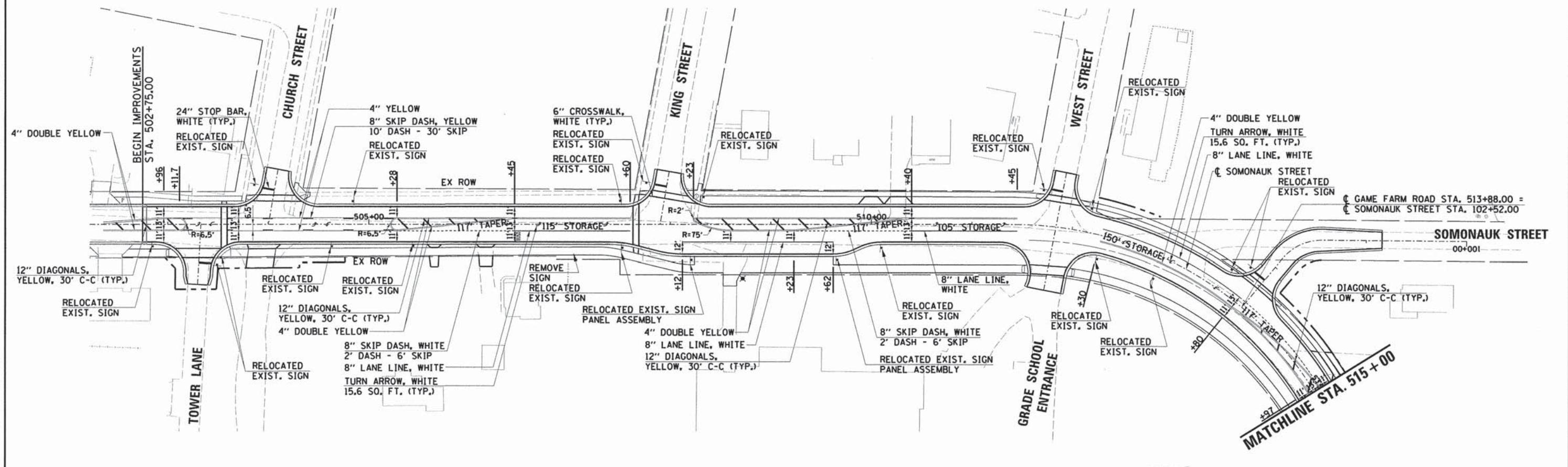
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 56 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
PAVEMENT MARKING

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: JMR
CHECKED BY: GT

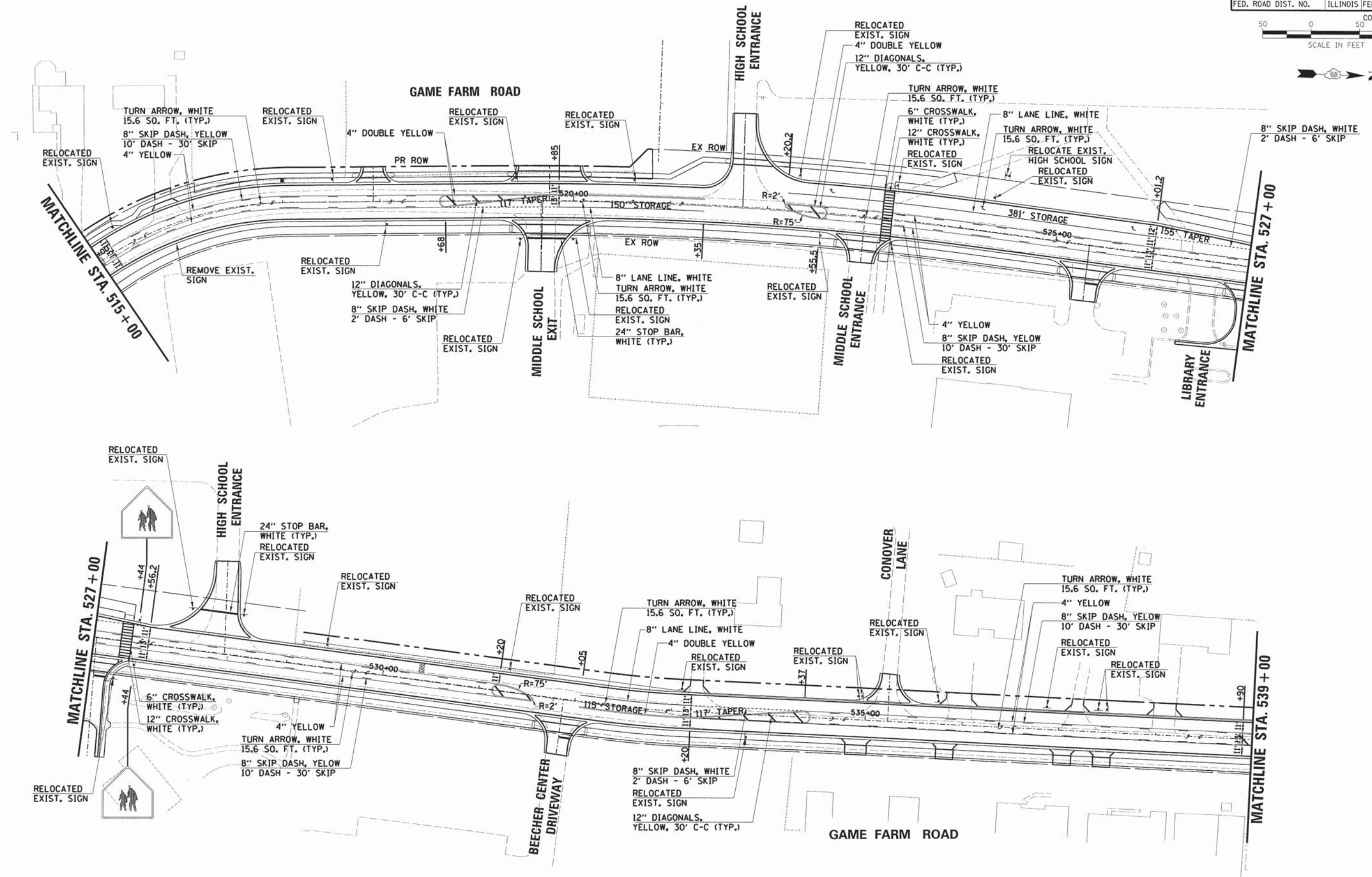
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 57 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



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| | STRUCTURE NOTATIONS CHKD | | |

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ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION
 PAVEMENT MARKING

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: JMR
 CHECKED BY: CT

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 58 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

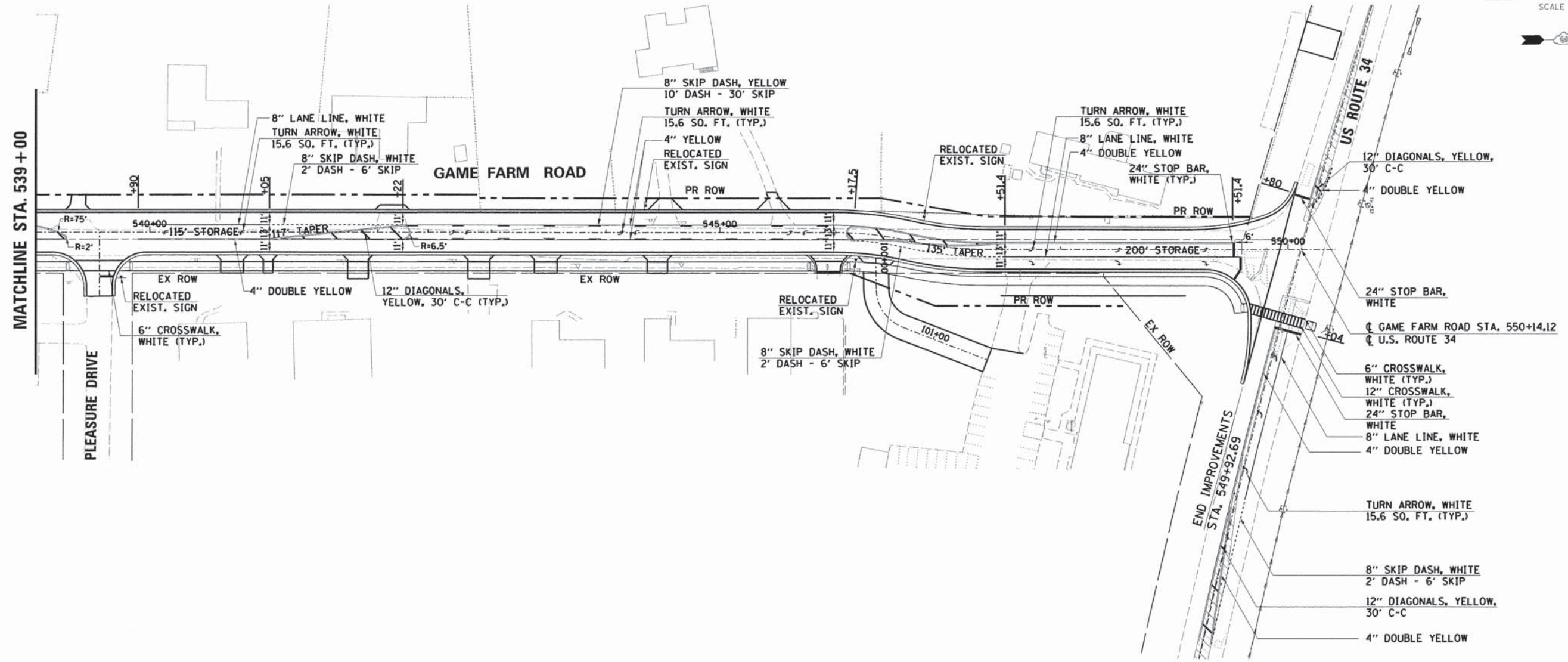
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| | STRUCTURE NOTATIONS CHKD | | |

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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
PAVEMENT MARKING
SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014
DRAWN BY: JMR
CHECKED BY: GT

PLAN SURVEYED BY DATE
 NOTE BOOK NO. _____
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PROFILE SURVEYED BY DATE
 NOTE BOOK NO. _____
 GRADES CHECKED _____
 B.M. NOTED _____
 STRUCTURE NOTATIONS DHRD _____

| ITEM | UNIT | U.S. ROUTE 34 AT GAME FARM ROAD | INTERCONNECT AT U.S. ROUTE 34 |
|---|-------|---------------------------------|-------------------------------|
| UNDERGROUND CONDUIT, PVC, 1" DIA. | FOOT | 296 | |
| UNDERGROUND CONDUIT, PVC, 2 1/2" DIA. | FOOT | 72 | |
| MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 | |
| GROUNDING EXISTING HANDHOLE FRAME AND COVER | EACH | 3 | |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | FOOT | 200 | |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 693 | |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 1,142 | |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 173 | |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1,090 | |
| ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 451 | |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 11 FT. | EACH | 1 | |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 2 | |
| CONCRETE FOUNDATION, TYPE A | FOOT | 9 | |
| DRILL EXISTING HANDHOLE | EACH | 16 | |
| SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 4 | |
| SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 2 | |
| SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 1 | |
| PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER | EACH | 2 | |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 7 | |
| INDUCTIVE LOOP DETECTOR | EACH | 9 | |
| DETECTOR LOOP, TYPE I | FOOT | 807 | |
| LIGHT DETECTOR | EACH | 3 | |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 | |
| PEDESTRIAN PUSH-BUTTON | EACH | 2 | |
| MODIFY EXISTING CONTROLLER | EACH | 1 | |
| REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 1,862 | |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 | |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 1 | |
| ELECTRIC CABLE IN CONDUIT, NO. 20 3C, TWISTED, SHIELDED | FOOT | 489 | |
| LOOP DETECTOR TESTING | EACH | 1 | |
| RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM | L SUM | | 1 |

TRAFFIC SIGNAL GENERAL NOTES:

1. THE CONTRACTOR SHALL CONTACT THE UNITED CITY OF YORKVILLE (630-553-4350) A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK.
2. THE TRAFFIC SIGNAL SECTION AT THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DISTRICT 3 SHALL BE NOTIFIED AT 815-434-8506 AT LEAST 72 HOURS PRIOR TO TURNING ON ANY FLASHER OR CONTROLLER UNITS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK. THE JULIE NUMBER IS 800-892-0123.
4. ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS.
5. ALL TRAFFIC SIGNAL HEADS SHALL BE 12-INCH POLYCARBONATE.
6. TRAFFIC SIGNAL HEADS SHALL BE PROPERLY COVERED PRIOR TO INTERSECTION TURN-ON OR AS DIRECTED BY THE ENGINEER. THIS COST SHALL BE INCLUDED WITH THE COST OF THE ASSOCIATED TRAFFIC SIGNAL PAY ITEMS.
7. A 1/4" DIAMETER CONTINUOUS RODENT RESISTANT NYLON ROPE SHALL BE FURNISHED AND LEFT IN PLACE IN ALL CONDUITS BETWEEN HANDHOLES AND FOUNDATIONS OR CONTROLLER. THIS COST SHALL BE INCLUDED WITH THE COST OF CONDUIT PAY ITEM.
8. THE CONTRACTOR SHALL ARRANGE FOR A FACTORY OR SUPPLIER REPRESENTATIVE TO BE PRESENT AT THE INTERSECTION WHEN THE SIGNAL IS TURNED ON. THIS COST SHALL BE INCLUDED WITH THE TRAFFIC SIGNAL CONTROLLER PAY ITEM.
9. ALL CONDUIT IN TRENCH SHALL BE P.V.C., ALL CONDUIT PUSHED MAY BE P.V.C. OR GALVANIZED STEEL. CONDUIT ATTACHED TO STRUCTURES SHALL BE GALVANIZED STEEL.
10. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR PLACING CONDUIT AT A GREATER THAN 2' MINIMUM DEPTH TO AVOID OBSTACLES SUCH AS UNDERGROUND UTILITIES.
11. THE ELECTRICAL CONDUCTORS FOR ALL TRAFFIC SIGNAL HEADS SHALL BE SOLID, SOFT COPPER.
12. ALL THREADS OF BOLTS USED IN THE ASSEMBLY OF TRAFFIC SIGNAL COMPONENTS SHALL BE COATED WITH A NON-LEAD BASED ANTI-SEIZE COMPOUND, SIMILAR TO LEAD PLATE, PRIOR TO ASSEMBLY.
13. ALL HARDWARE SHALL BE TIGHTENED AND WELL SECURED. CABLES SHALL BE NEATLY WOUND IN HANDHOLES. CABLES SHALL BE NEATLY TRAINED IN THE CONTROLLER CABINET.
14. ALL TRAFFIC SIGNAL WIRING SHALL EXTEND FROM CONTROLLER TO SIGNAL. SPLICES IN JUNCTION BOXES WILL NOT BE ALLOWED.
15. THE CONTROLLER CABINET SHALL BE PLACED SO THAT A TECHNICIAN MAY SEE THE INTERSECTION OVER THE TOP OF THE CABINET WHILE WATCHING THE COMPONENTS IN THE CABINET.
16. THE PROPOSED TRAFFIC SIGNAL CONTROLLER CABINET SHALL BE FURNISHED WITH A MANUAL CONTROL SWITCH AND MANUAL CONTROL CHORD WITHIN THE POLICE DOOR COMPARTMENT. THIS WORK SHALL BE INCLUDED IN THE CONTROLLER CABINET PAY ITEM.
17. THE CONTRACTOR SHALL PROVIDE A SELF-ADHERED PHASE DIAGRAM ON THE INSIDE OF THE CONTROLLER CABINET DOOR.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ELECTRICAL SERVICE FOR THE TRAFFIC SIGNALS. THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY PRIOR TO BEGINNING WORK TO OBTAIN THE UTILITY COMPANY REQUIREMENTS FOR THE SERVICE INSTALLATION.
19. ALL VEHICLE AND PEDESTRIAN SIGNAL HEADS SHALL HAVE POLYCARBONATE BLACK HOUSING AND BLACK BRACKETS.
20. THE ELEVATION OF THE TOP OF THE DOUBLE HANDHOLE SHALL BE LESS THAN THE ELEVATION OF THE TOP OF THE CONTROLLER FOUNDATION.
21. ALL UNINTERRUPTABLE POWER SUPPLIES SHALL BE EQUIPPED WITH ALPHA GUARD MONITORS.
22. ALL GROUNDING MATERIALS FOR THE TRAFFIC SIGNAL CONCRETE FOUNDATIONS SHALL BE IN ACCORDANCE WITH SECTION 806 OF THE STANDARD SPECIFICATIONS.
23. ALL AREAS DISTURBED BY THE CONTRACTOR SHALL BE RESTORED WITH SEED OR SOD TO THE SATISFACTION OF THE ENGINEER. SEEDING OR SODDING SHALL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION.
24. THE FIBER OPTIC CABLE SHALL BE LABELED WITH DIRECTION AND ASSIGNMENT NUMBER.
25. THE SURGE PROTECTOR IN THE CONTROLLER CABINET SHALL HAVE AN INDICATOR LIGHT.
26. THE MAST ARMS SHALL BE LOCATED A MINIMUM OF 6' FROM THE FACE OF CURB OR A MINIMUM OF 18' FROM THE EDGE OF PAVEMENT TO THE FACE OF FOUNDATION WHERE THERE IS NO CURB, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IN CURB AREA, PROVIDE MORE THAN 6' IF POSSIBLE, PROVIDED THE SIGNAL HEADS STILL LINE UP IN THE CENTER OF THE LANES.
27. SEE SUGGESTED MAINTENANCE OF TRAFFIC PLANS FOR INFORMATION REGARDING CHANGEABLE MESSAGE SIGNS.
28. TWO FANS SHALL BE INCLUDED IN EACH CONTROLLER CABINET. THESE ITEMS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL.
29. ONE FAN SHALL BE INCLUDED IN EACH UPS. THESE ITEMS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF UNINTERRUPTABLE POWER SUPPLY.

ILLINOIS DEPARTMENT OF TRANSPORTATION
**SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION**
TRAFFIC SIGNAL SCHEDULE

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014
 DRAWN BY: MJF
 CHECKED BY: APS

| | | | | |
|---------------------|----------------|---------------------------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 60 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |

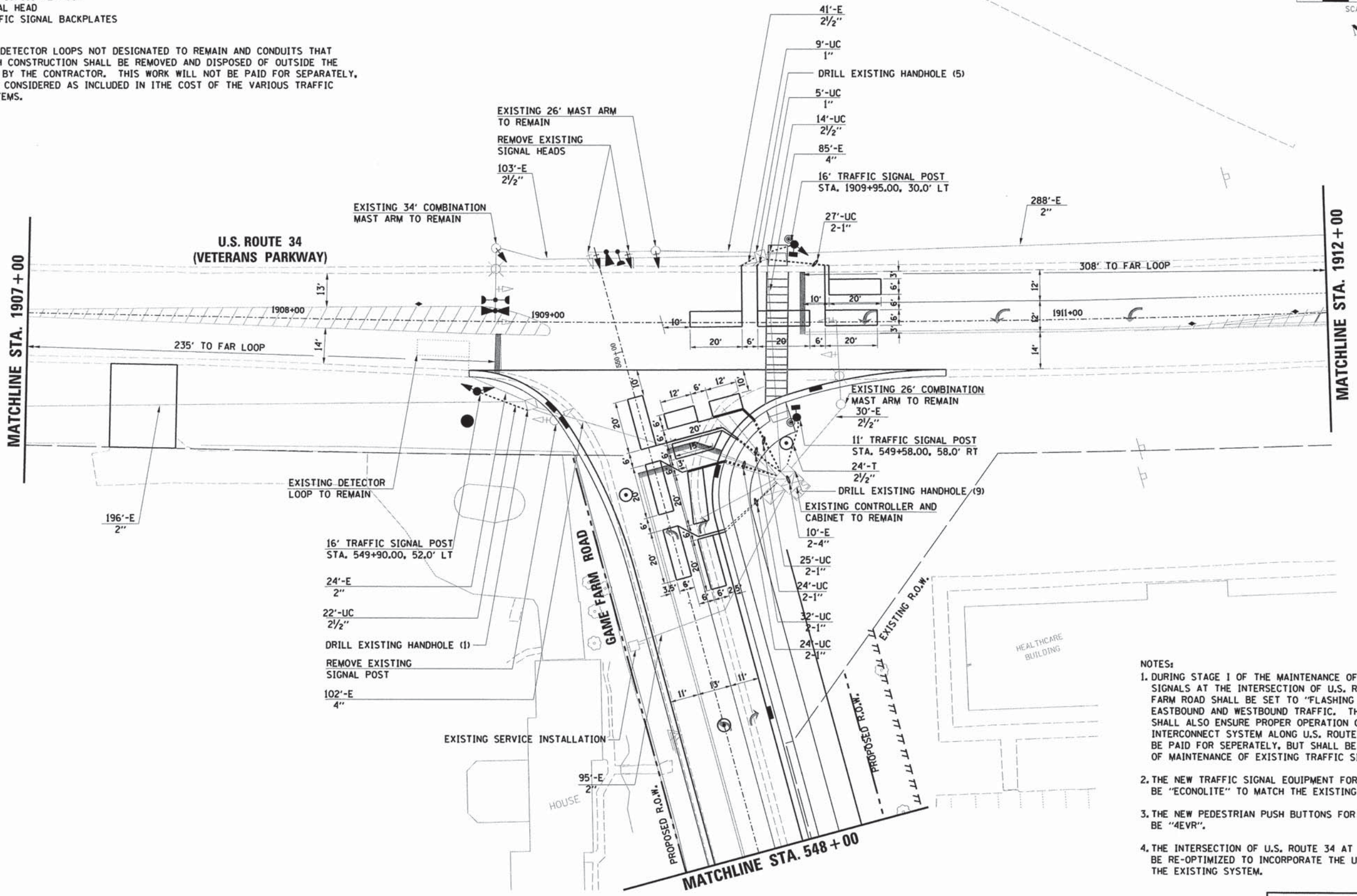
CONTRACT NO. 87345



THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE MAINTENANCE FACILITY DESIGNATED BY THE CITY. THE CONTRACTOR SHALL NOTIFY THE CITY A MINIMUM OF 48 HOURS PRIOR TO DELIVERY. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT.

- 1 EACH TRAFFIC SIGNAL POST
- 4 EACH SIGNAL HEAD
- 4 EACH TRAFFIC SIGNAL BACKPLATES

• THE EXISTING DETECTOR LOOPS NOT DESIGNATED TO REMAIN AND CONDUITS THAT CONFLICT WITH CONSTRUCTION SHALL BE REMOVED AND DISPOSED OF OUTSIDE THE RIGHT-OF-WAY BY THE CONTRACTOR. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE VARIOUS TRAFFIC SIGNAL PAY ITEMS.



- NOTES:
1. DURING STAGE I OF THE MAINTENANCE OF TRAFFIC, THE TRAFFIC SIGNALS AT THE INTERSECTION OF U.S. ROUTE 34 AND GAME FARM ROAD SHALL BE SET TO "FLASHING YELLOW" FOR EASTBOUND AND WESTBOUND TRAFFIC. THE CONTRACTOR SHALL ALSO ENSURE PROPER OPERATION OF THE REMAINDER OF THE INTERCONNECT SYSTEM ALONG U.S. ROUTE 34. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION.
 2. THE NEW TRAFFIC SIGNAL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING SYSTEM.
 3. THE NEW PEDESTRIAN PUSH BUTTONS FOR THIS PROJECT SHALL BE "4EVR".
 4. THE INTERSECTION OF U.S. ROUTE 34 AT GAME FARM ROAD SHALL BE RE-OPTIMIZED TO INCORPORATE THE UPDATED PHASING INTO THE EXISTING SYSTEM.

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
TRAFFIC SIGNAL INSTALLATION PLAN
U.S. ROUTE 34 AT GAME FARM ROAD
(SHEET 1 OF 2)

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: MJF
 CHECKED BY: APS

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| | STRUCTURE NOTATIONS | |
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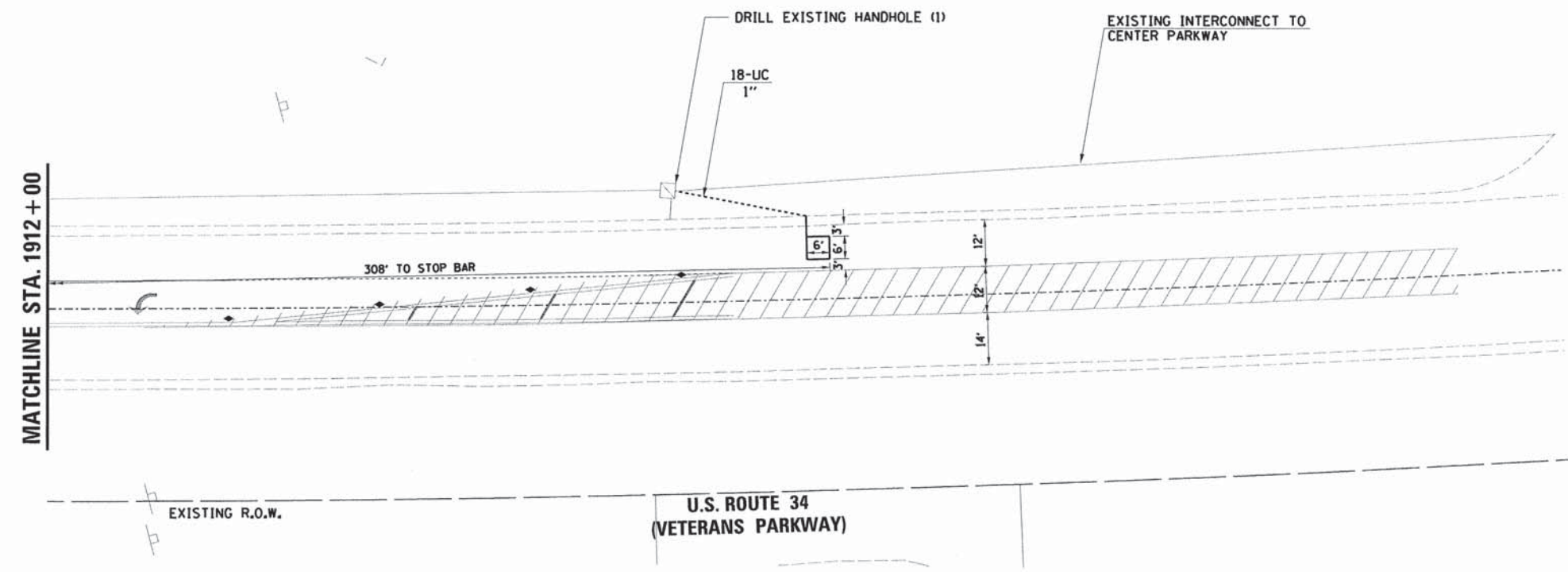
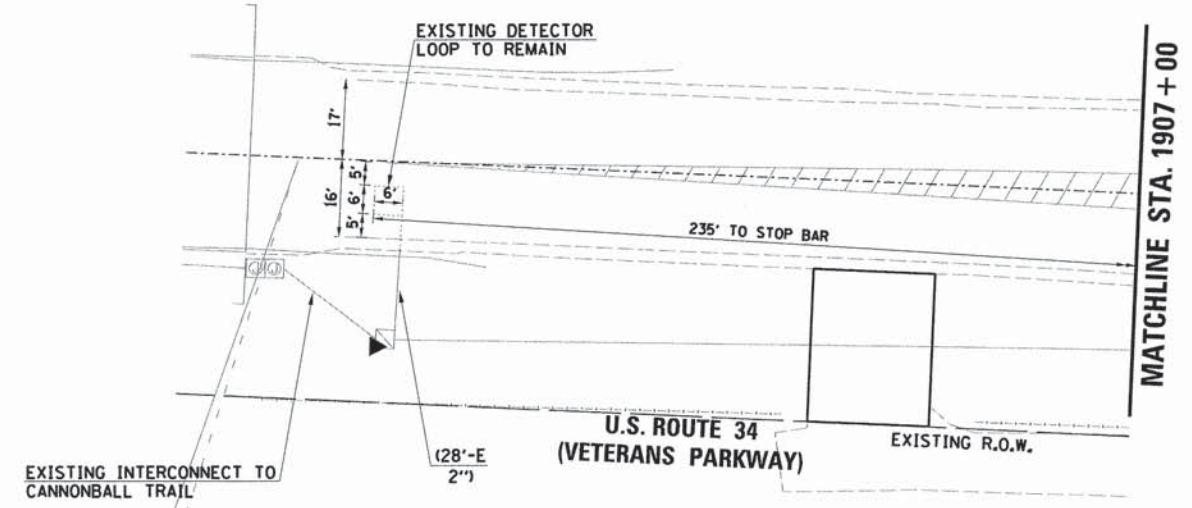
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 61 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

CONTRACT NO. 87345



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| | NOTATIONS | | |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
TRAFFIC SIGNAL INSTALLATION PLAN
U.S. ROUTE 34 AT GAME FARM ROAD
(SHEET 2 OF 2)

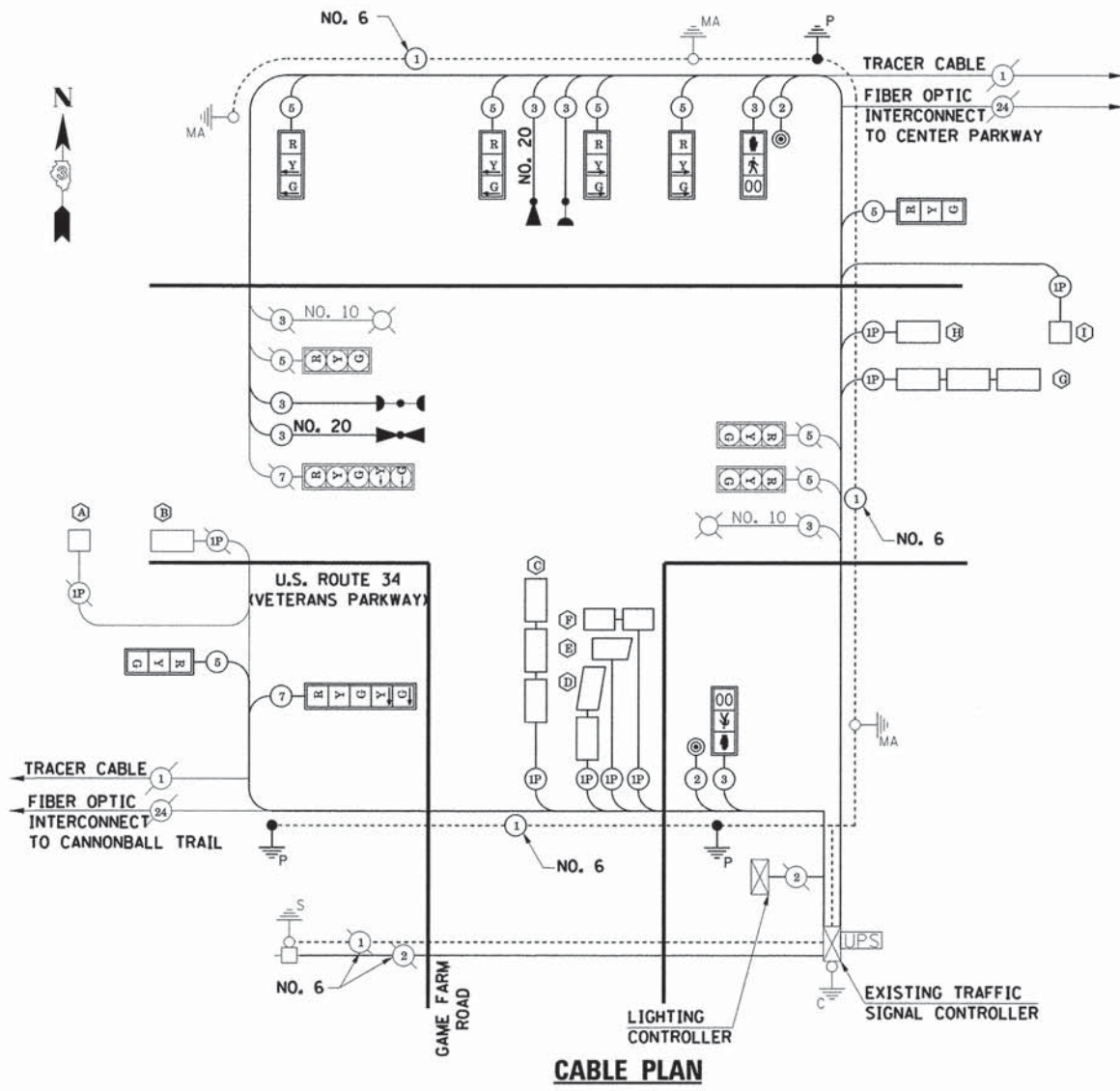
SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: MJF
CHECKED BY: APS

CONTRACT NO. 87345

CABLE DIAGRAM LEGEND

| EXISTING | PROPOSED | DESCRIPTION |
|----------|----------|---|
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| | | DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MMI2F SM12F |
| | | LIGHTING UNIT |
| | | 12" TRAFFIC SIGNAL SECTION |
| | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. |
| | | GROUND CABLE ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| | | GROUND ROD AT POST (P) OR MAST ARM (MA) |
| | | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| | | 12" (300mm) PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER |
| | | PEDESTRIAN PUSH-BUTTON |
| | | ILLUMINATED NO LEFT OR RIGHT-TURN SIGN |



| ITEM | UNIT | U.S. ROUTE 34 AT GAME FARM ROAD |
|---|-------|---------------------------------|
| UNDERGROUND CONDUIT, PVC, 1" DIA. | FOOT | 296 |
| UNDERGROUND CONDUIT, PVC, 2 1/2" DIA. | FOOT | 72 |
| MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 |
| GROUNDING EXISTING HANDHOLE FRAME AND COVER | EACH | 3 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C | FOOT | 200 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C | FOOT | 693 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C | FOOT | 1,142 |
| ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C | FOOT | 173 |
| ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR | FOOT | 1,090 |
| ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C | FOOT | 451 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 11 FT. | EACH | 1 |
| TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT. | EACH | 2 |
| CONCRETE FOUNDATION, TYPE A | FOOT | 9 |
| DRILL EXISTING HANDHOLE | EACH | 16 |
| SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED | EACH | 4 |
| SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED | EACH | 2 |
| SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED | EACH | 1 |
| PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER | EACH | 2 |
| TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC | EACH | 7 |
| INDUCTIVE LOOP DETECTOR | EACH | 9 |
| DETECTOR LOOP, TYPE I | FOOT | 807 |
| LIGHT DETECTOR | EACH | 3 |
| LIGHT DETECTOR AMPLIFIER | EACH | 1 |
| PEDESTRIAN PUSH-BUTTON | EACH | 2 |
| MODIFY EXISTING CONTROLLER | EACH | 1 |
| REMOVE ELECTRIC CABLE FROM CONDUIT | FOOT | 1,862 |
| REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT | EACH | 1 |
| REMOVE EXISTING CONCRETE FOUNDATION | EACH | 1 |
| ELECTRIC CABLE IN CONDUIT, NO. 20 3C, TWISTED, SHIELDED | FOOT | 489 |
| LOOP DETECTOR TESTING | EACH | 1 |
| RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM | L SUM | |

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| PROFILE | SURVEYED | BY | DATE |
| NOTE BOOK | ED | | |
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| | BLM. NOTED | | |
| | STRUCTURE NOTATIONS CHRD | | |

U.S. ROUTE 34 AT GAME FARM ROAD ELECTRICAL LOAD CHART

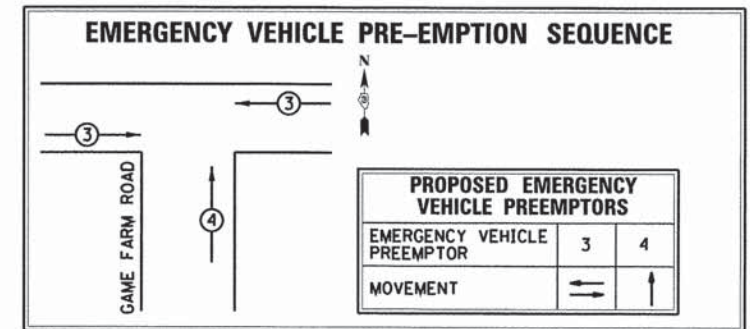
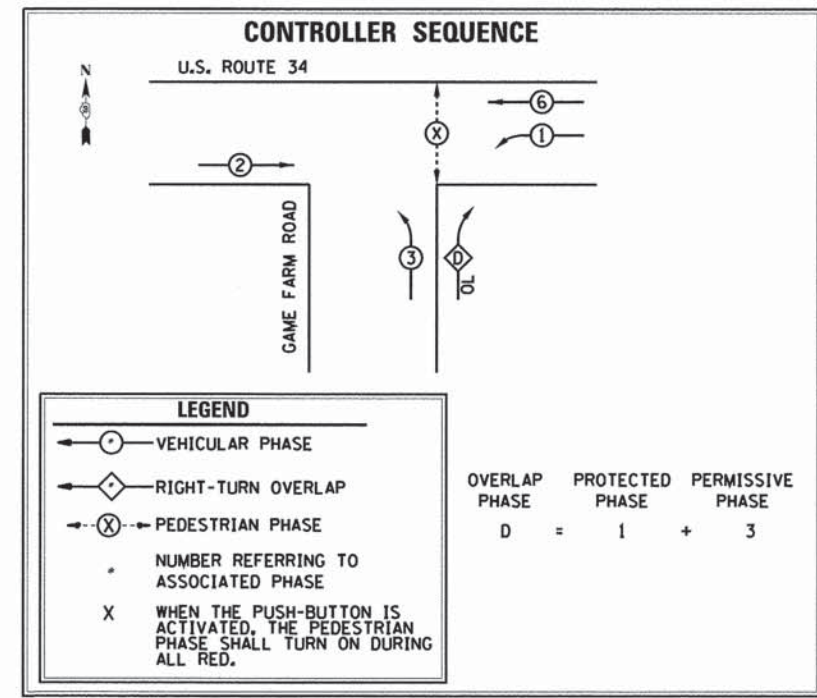
| U.S. ROUTE 34 | | | |
|---------------|--------|--------------|---------------|
| INDICATION | NUMBER | WATTAGE EACH | BURN TIME (%) |
| RED | 7 | 10 | 35 |
| YELLOW | 7 | 22 | 5 |
| GREEN | 7 | 12 | 60 |
| YELLOW ARROW | 2 | 10 | 5 |
| GREEN ARROW | 2 | 5 | 30 |

| GAME FARM ROAD | | | |
|----------------|--------|--------------|---------------|
| INDICATION | NUMBER | WATTAGE EACH | BURN TIME (%) |
| RED | 4 | 10 | 60 |
| YELLOW ARROW | 4 | 10 | 5 |
| GREEN ARROW | 4 | 5 | 30 |
| WALK | 2 | 7 | 87 |
| DON'T WALK | 2 | 5 | 13 |

| TRAFFIC SIGNAL CABINET | | | |
|------------------------|--------|--------------|---------------|
| ITEM | NUMBER | WATTAGE EACH | BURN TIME (%) |
| CONTROLLER | 1 | 6 | 100 |
| LOOP DETECTORS | 9 | 4 | 100 |
| UPS | 1 | 50 | 100 |

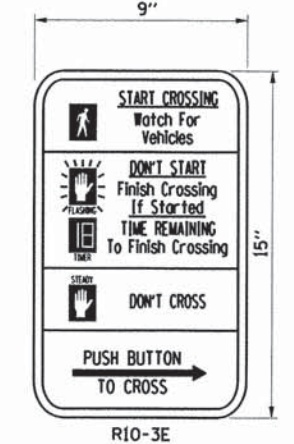
| HIGHWAY LIGHTING | | | |
|------------------|--------|--------------|---------------|
| ITEM | NUMBER | WATTAGE EACH | BURN TIME (%) |
| CONTROLLER | 1 | 6 | 100 |
| LUMINAIRE | 2 | 310 | 360 HRS/MONTH |

AGENCY RESPONSIBLE FOR ENERGY CHARGES: UNITED CITY OF YORKVILLE



U.S. ROUTE 34 AT GAME FARM ROAD DETECTOR LOOP INDUCTANCE CHART

| LOOP SYSTEM | PHASE | LABEL | NO. OF TURNS | INDUCTANCE (MICROHENRIES) | FREQUENCY (HERTZ) | J PIN STATUS |
|-------------|-------|-----------|--------------|---------------------------|-------------------|--------------|
| A | 2 | EB FAR | 6 | 337 | 33,247 | ON |
| B | 2 | EB STBR | 4 | 285 | 36,134 | OFF |
| C | 3 | NB LT | 4 | 785 | 21,781 | ON |
| D | D | NB RT BK | 4 | 525 | 26,627 | ON |
| E | D | NB RT MID | 4 | 251 | 38,558 | ON |
| F | D | NB RT FR | 4 | 365 | 31,925 | ON |
| G | 1 | WB LT | 4 | 804 | 21,521 | ON |
| H | 6 | WB STBR | 4 | 285 | 36,176 | OFF |
| I | 6 | WB FAR | 6 | 341 | 33,043 | ON |



PEDESTRIAN PUSH-BUTTON SIGN DETAIL

THIS SHALL BE MOUNTED ABOVE THE PEDESTRIAN PUSH-BUTTON. THE COST OF THE SIGNS SHALL BE INCLUDED IN THE COST OF THE PUSH-BUTTONS.

TYPE "A" SHEETING REQUIRED

0.94 SQ. FT. EACH

2 REQUIRED

NOTES:

- THE NEW TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE".
- THE NEW PEDESTRIAN PUSH BUTTONS FOR THIS PROJECT SHALL BE "4EVR".

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
CABLE PLAN AND PHASE DESIGNATION DIAGRAM
U.S. ROUTE 34 AT GAME FARM ROAD

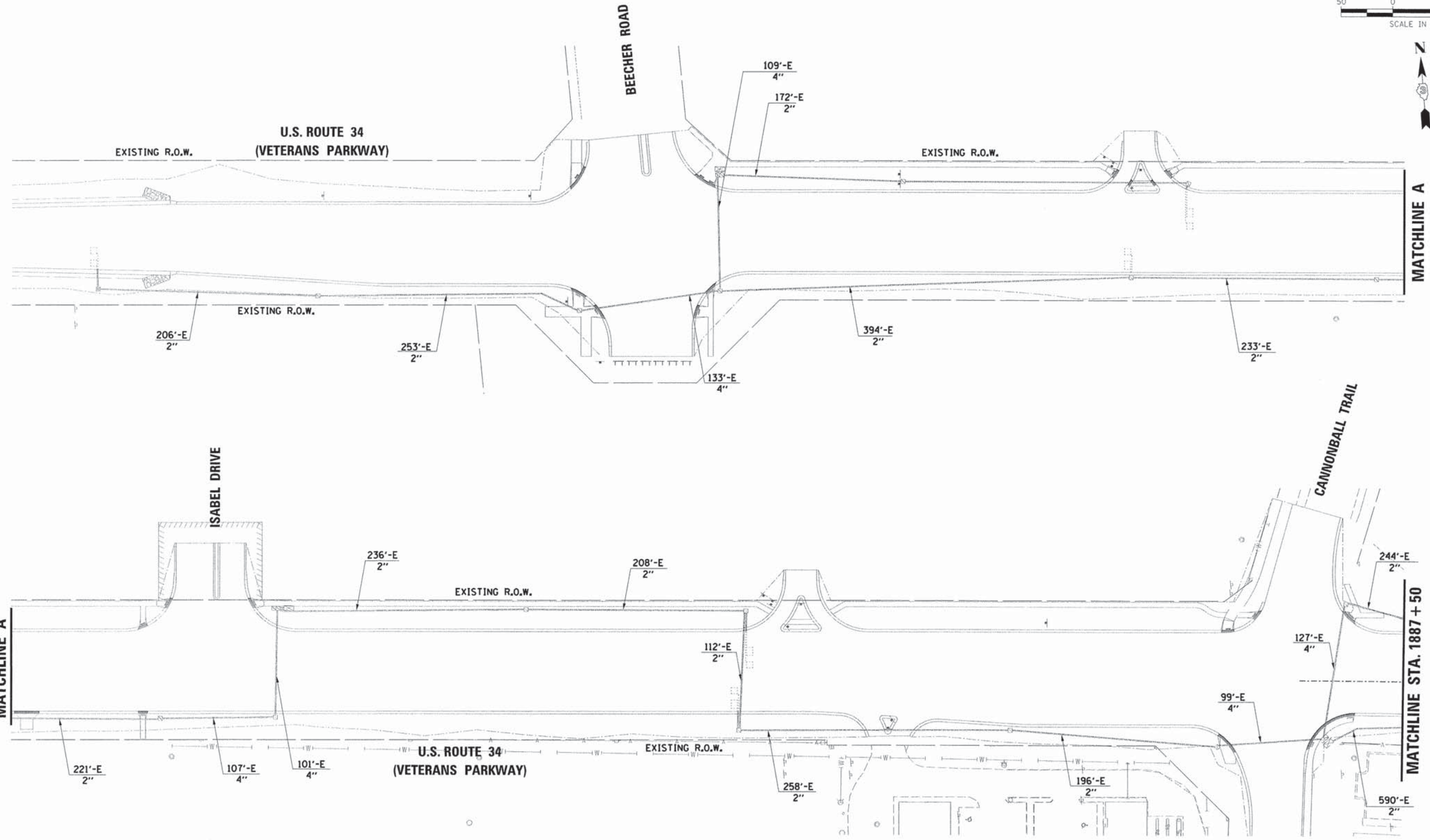
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 DATE: JULY 2014 CHECKED BY: APS

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 63 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |



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| PLAN | SURVEYED | BY | DATE |
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| PROFILE | SURVEYED | BY | DATE |
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| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS | | |



LEGEND

| | EXISTING | PROPOSED |
|--------------------|----------|----------|
| CONTROLLER CABINET | | |
| CONDUIT | | |
| HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| DETECTOR LOOP | | |
| JUNCTION BOX | | |

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
 INTERCONNECT PLAN – U.S. ROUTE 34
 (SHEET 1 OF 3)

SCALE: VERT. _____
 HORIZ. _____
 DATE: JULY 2014

DRAWN BY: M.J.F.
 CHECKED BY: APS

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 64 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

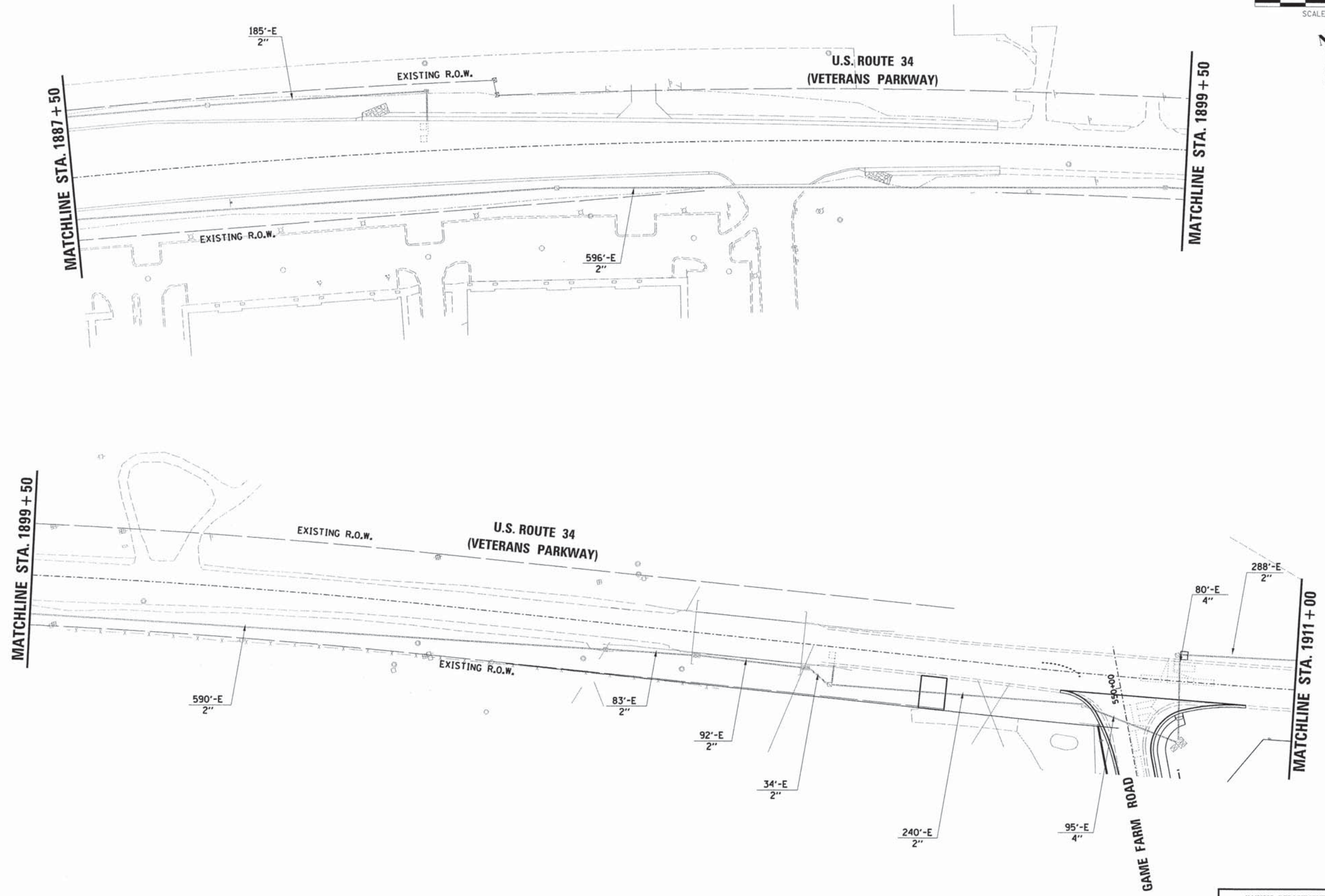
CONTRACT NO. 87345



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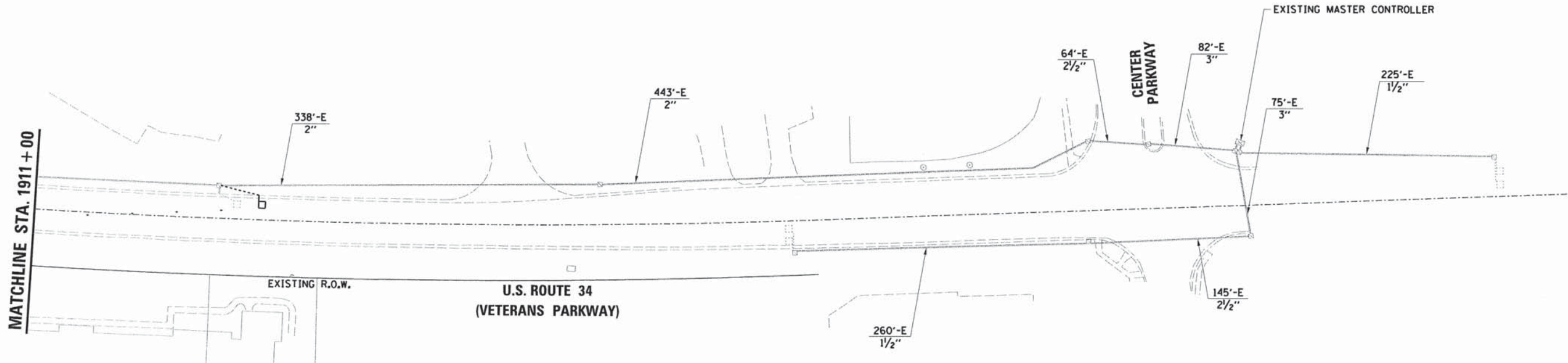
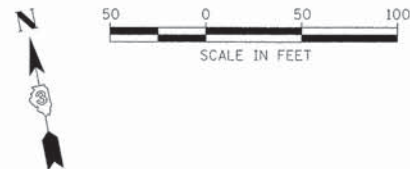
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION

INTERCONNECT PLAN - U.S. ROUTE 34
(SHEET 2 OF 3)

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: M.J.F.
CHECKED BY: A.P.S.

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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 65 |
| STA. | TO STA. | | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |



| ITEM | UNIT | INTERCONNECT AT U.S. ROUTE 34 |
|-----------------------------------|-------|-------------------------------|
| RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM | L SUM | 1 |

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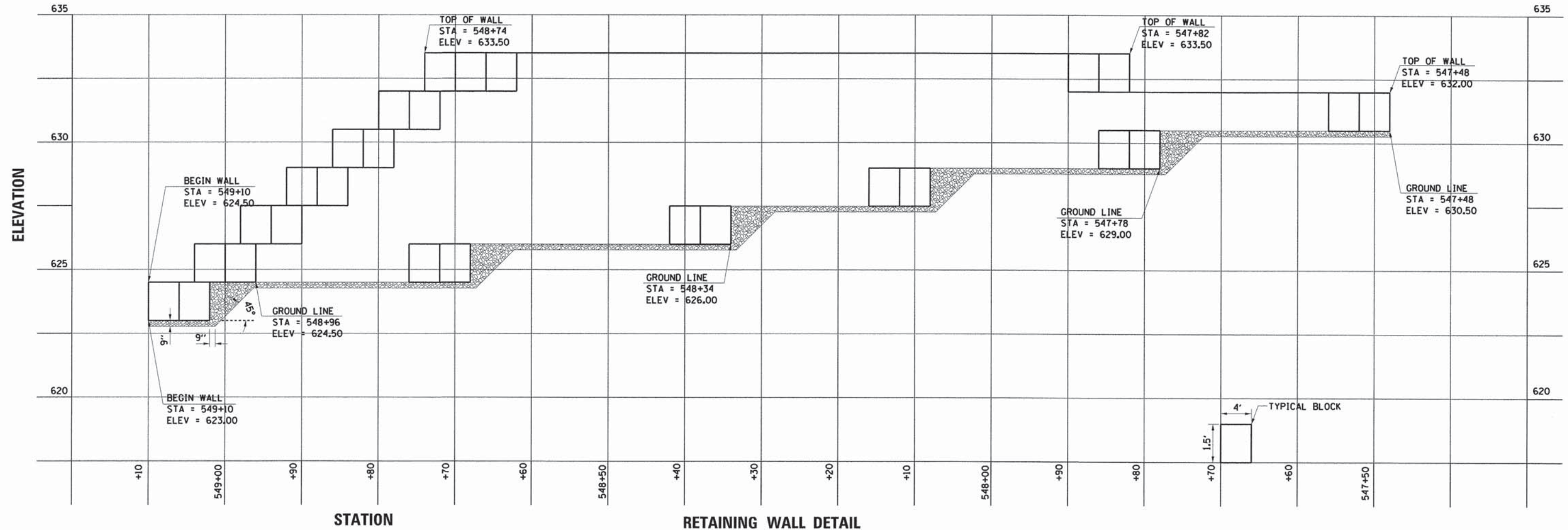
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| | STRUCTURE NOTATIONS CHKD | | |

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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
INTERCONNECT PLAN - U.S. ROUTE 34
(SHEET 3 OF 3)

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: M.J.F.
CHECKED BY: A.P.S.



PLAN SURVEYED BY DATE
 PLOTTED BY DATE
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PROFILE SURVEYED BY DATE
 PLOTTED BY DATE
 NOTE BOOK NO. _____
 GRADES CHECKED BY _____
 S.M. NOTED BY _____
 STRUCTURE NOTATIONS CHECKED BY _____

STATION
RETAINING WALL DETAIL
FRONT VIEW

- NOTES:**
1. A LEVEL AGGREGATE WALL BASE SHALL BE PROVIDED BELOW THE GROUND LINE. SEE DETAILS AND SPECIAL PROVISIONS FOR STEP INFORMATION.
 2. FOR FENCE RAILING, SEE RETAINING WALL DETAIL.

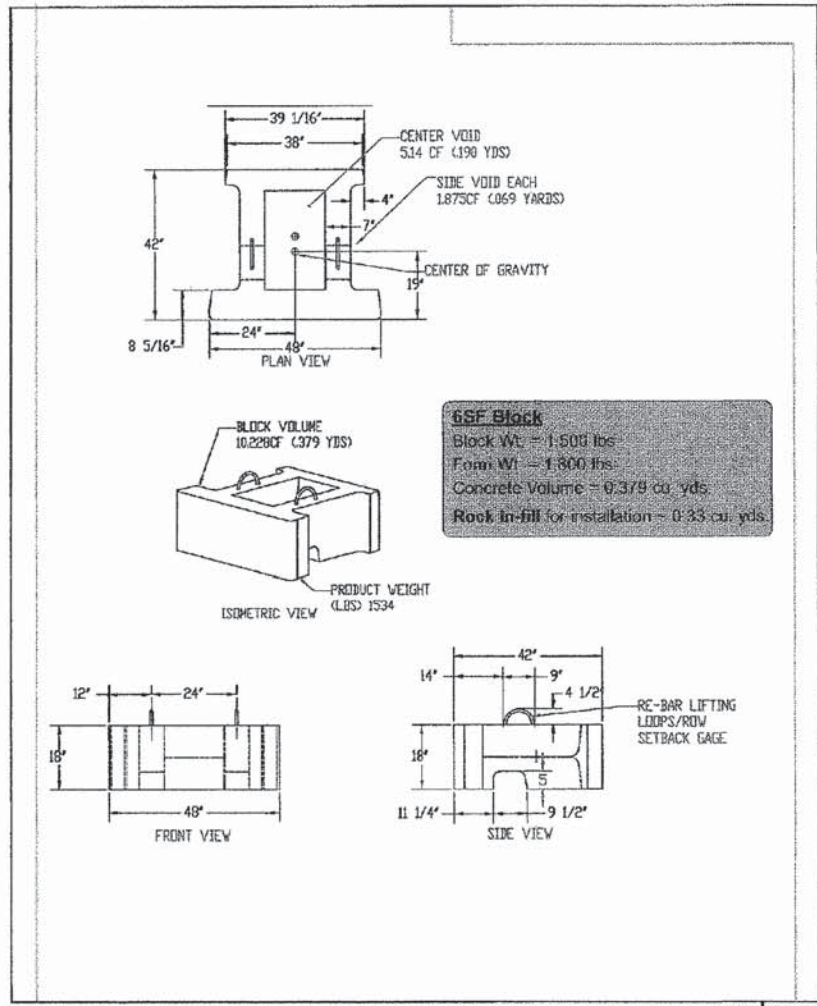
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS FED. AID PROJECT | | | |
| CONTRACT NO. 87345 | | | | |

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| PLAN | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | NOTE BOOK ALIGNMENT CHECKED | | |
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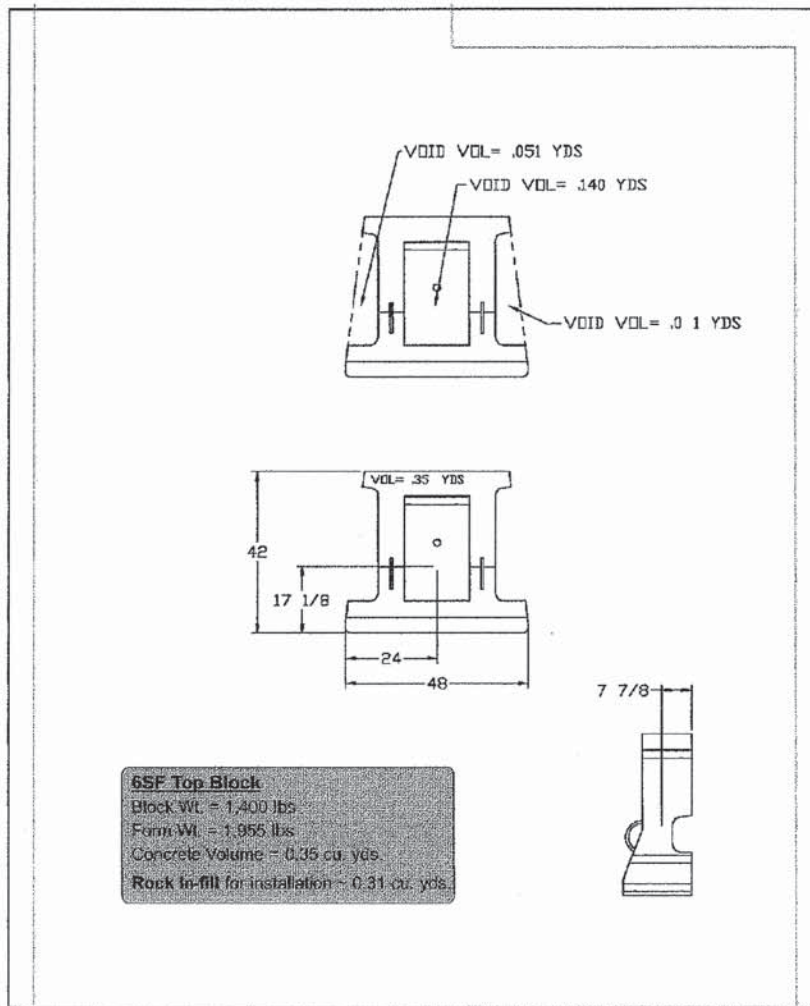
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| | B.M. NOTED | | |
| | NO. | | |
| | STRUCTURE NOTATIONS CHECKED | | |

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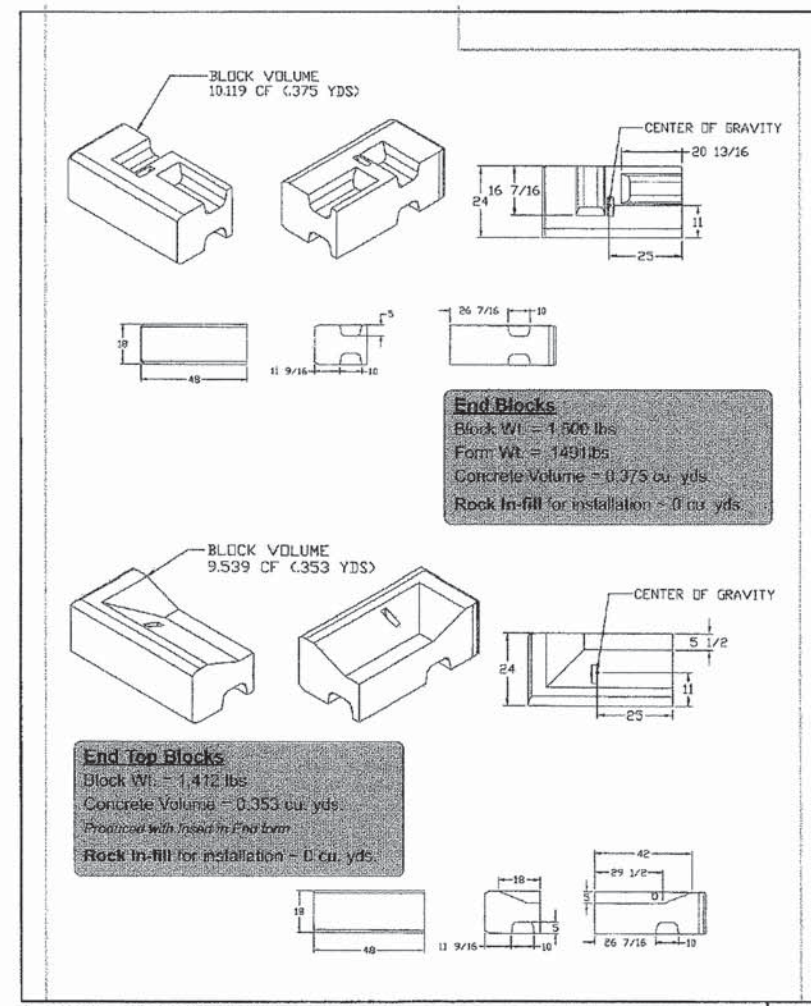
BLOCK DIMENSIONS AND VOLUMES



BLOCK DIMENSIONS AND VOLUMES



BLOCK DIMENSIONS AND VOLUMES



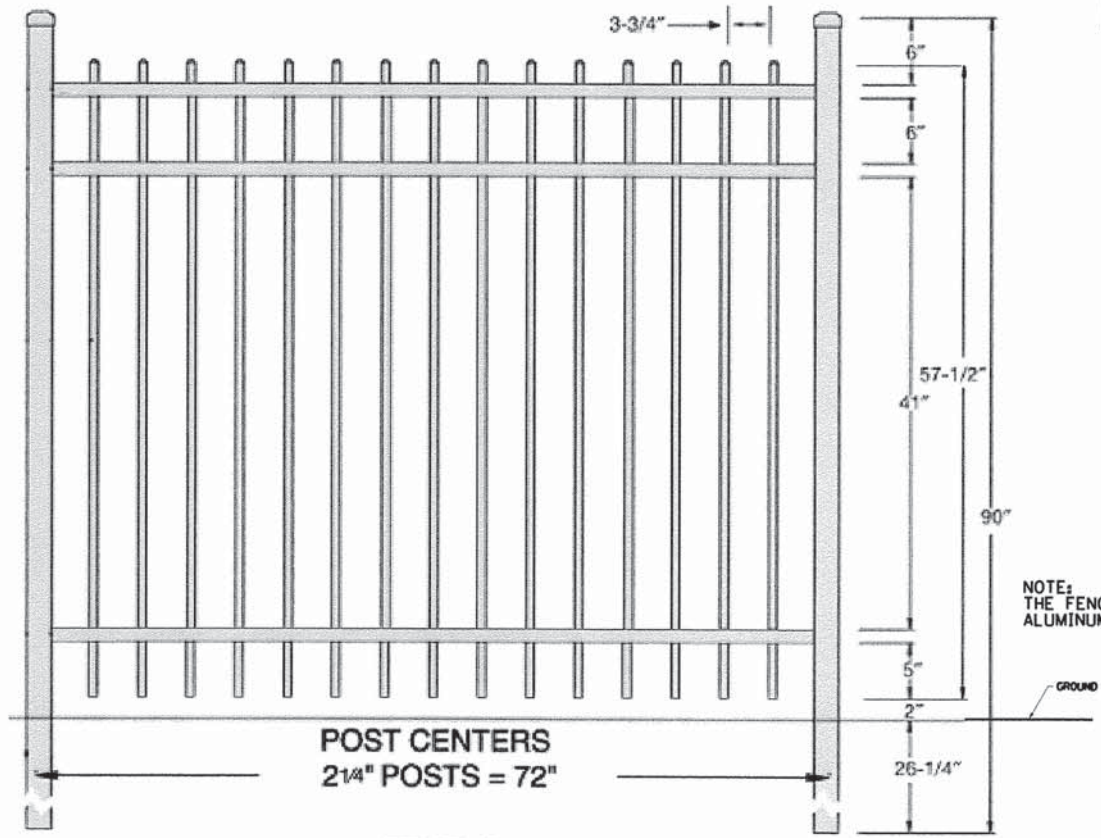
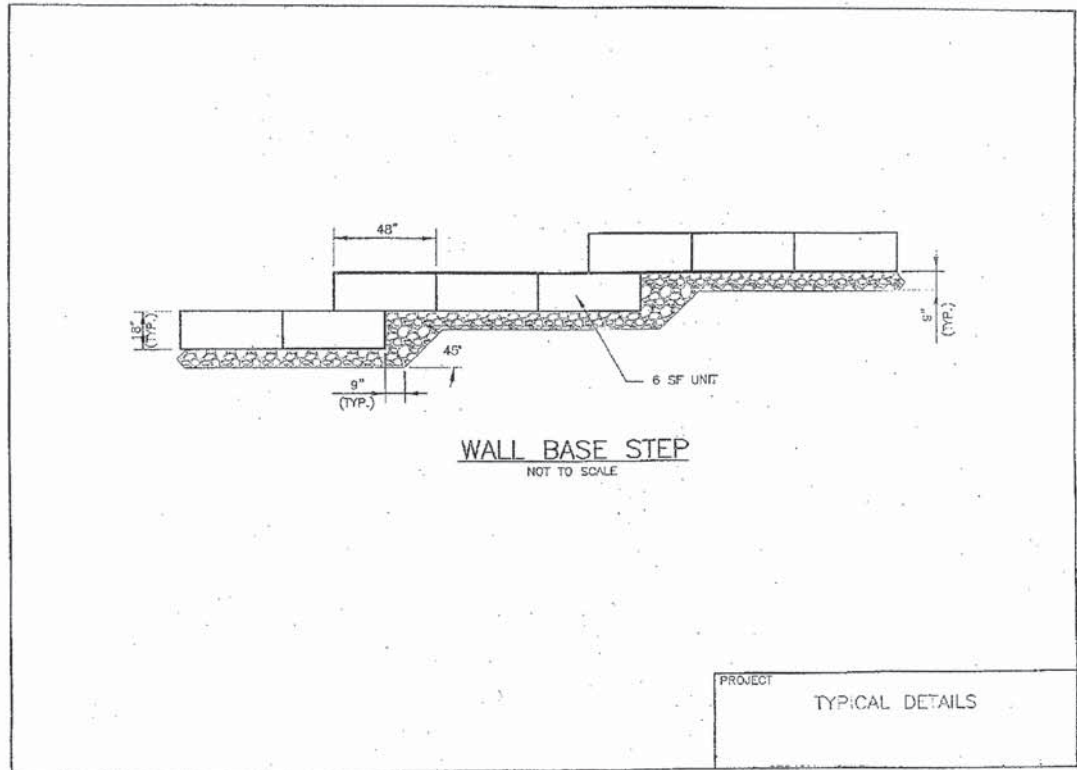
ILLINOIS DEPARTMENT OF TRANSPORTATION
 SOMONAUK STREET / GAME FARM ROAD
 RECONSTRUCTION

RETAINING WALL DETAILS

SCALE: VERT. _____ HORIZ. _____
 DATE: JULY 2014 DRAWN BY: MJF
 CHECKED BY: GT

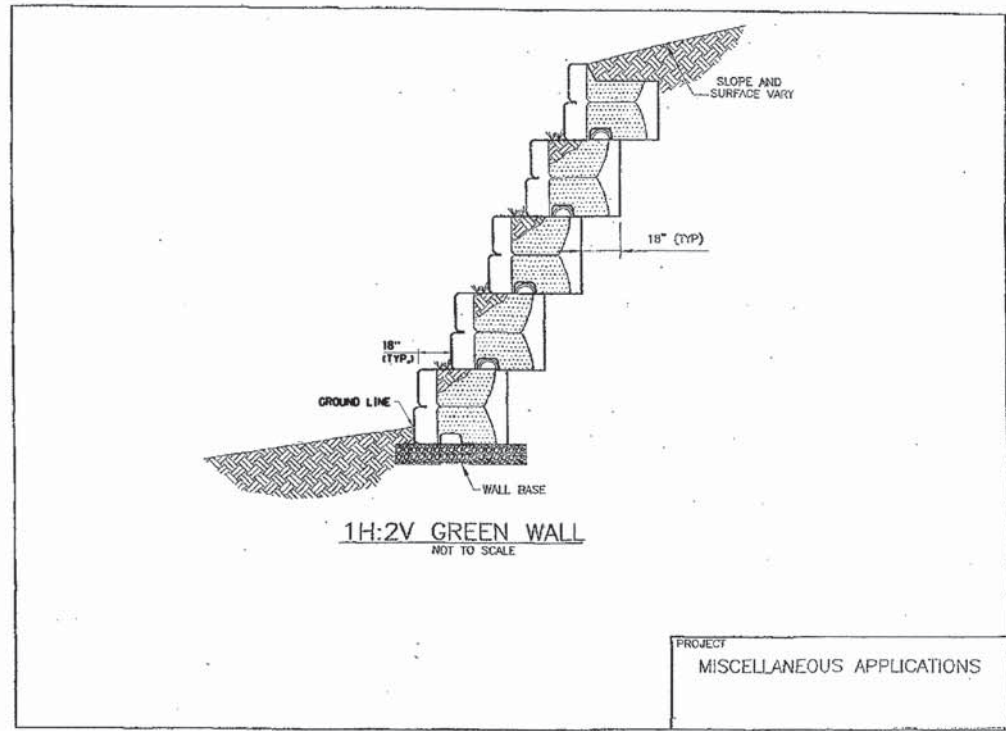
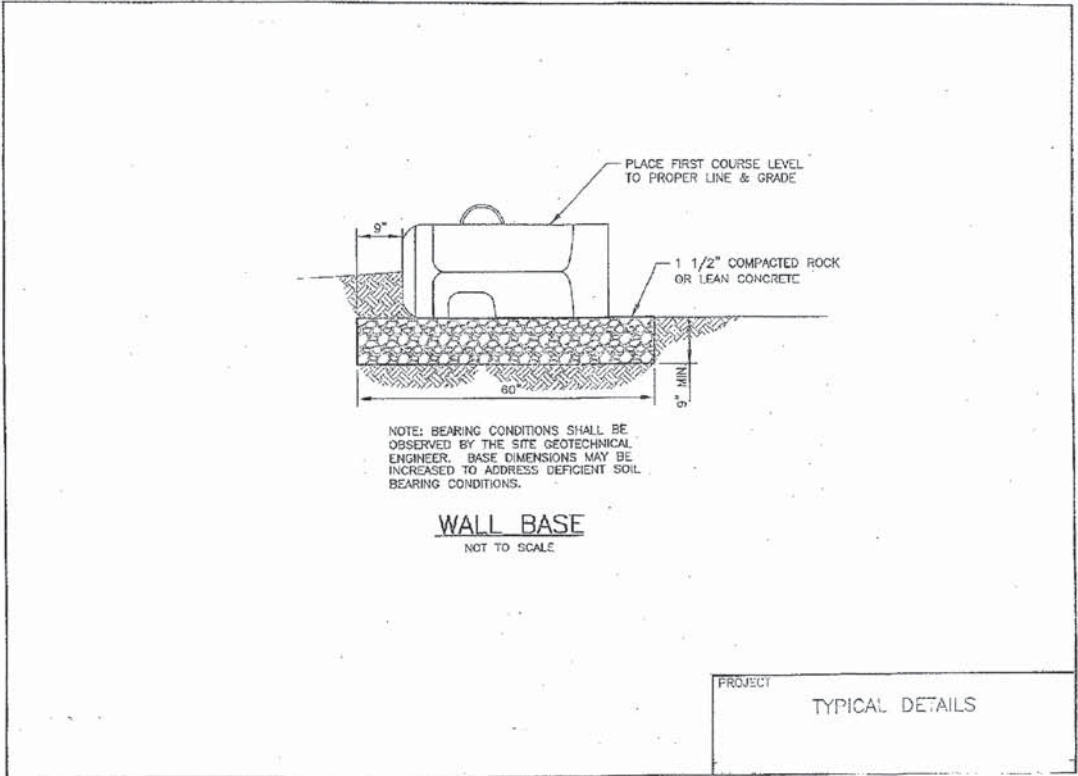
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 68 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |
| CONTRACT NO. 87345 | | | | |

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NOTE:
THE FENCE (SPECIAL) SHALL BE MADE OF ALUMINUM WITH A BLACK FINISH.

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| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
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| | STRUCTURE NOTATIONS CHKD | | |



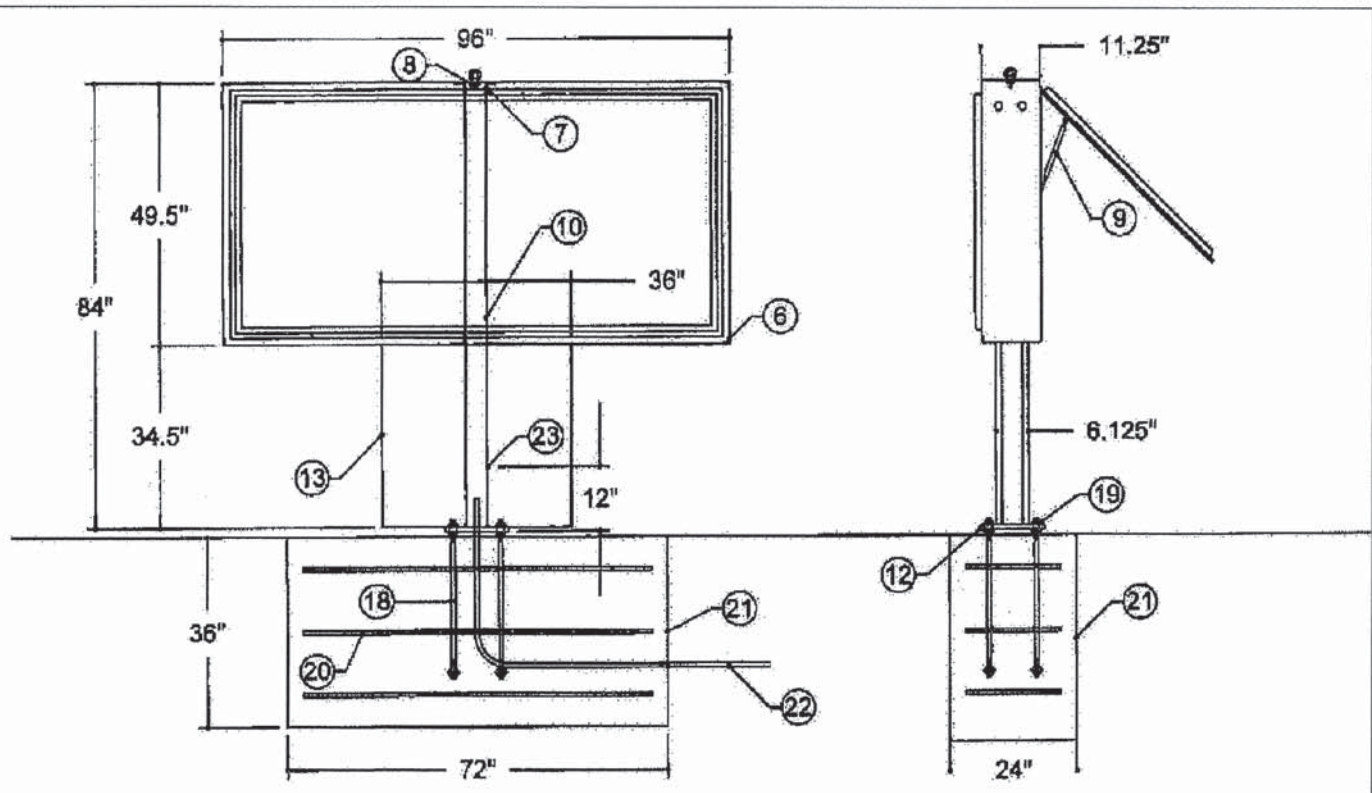
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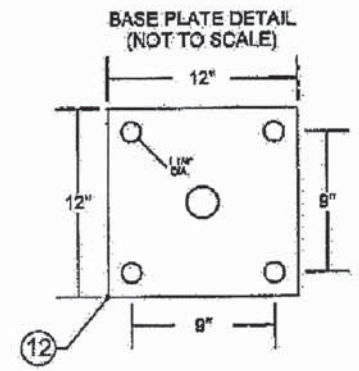
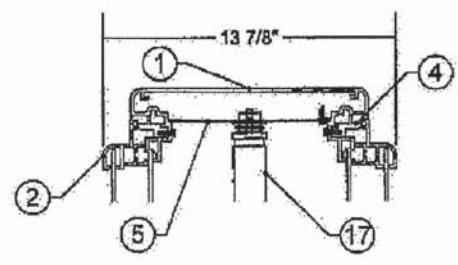
RELOCATE SIGN AND BASE



1. ALLUMINUM FILLER 1 1/4"
2. ALLUMINUM TOP RETAINER
3. ALLUMINUM SIDE AND BOTTOM RETAINER
4. ALLUMINUM HIDDEN ANODIZED HINGE
5. 0.083" ALLUMINUM HAT SECTION
6. ALLUMINUM ANGLE 1 1/2" X 1 1/2" X 3/16"
7. ALLUMINUM ANGLE 2" X 2" X 3/16"
8. 1/2" NUT WELDED FOR EYE BOLT
9. 90 LB GAS STRUT
10. STEEL TUBE 4" X 4" X 3/16"
11. STEEL BASE PLATE 12" X 12" X 3/4" (1) REO'D
12. .083" ALLUMINUM POLE COVER
13. DISCONNECT SWITCH INSIDE CABINET
14. ADVANCE ASB-0820-24-BL BALLAST (2) REO'D
15. 48" COOL WHITE H.O. LAMP (8) REO'D
16. STEEL ANCHOR BOLT 3/4" X 30" (4) REO'D
17. STEEL NUT 3/4" (5) REO'D
18. STEEL RE-BAR #5 GRADE 80 OR BETTER
19. 3000 PSI (MINIMUM) CONCRETE
20. ELECTRICAL CONDUIT RUN UNDER GROUND - MINIMUM DEPTH 24"
21. ELECTRICAL HOOK UP COVER

NOTES

1. YOU MUST USE THE PAPER TEMPLATE THAT ACCOMPANIES THE ANCHOR BOLTS FOR SETTING OF THE ANCHOR BOLTS IN CONCRETE. TAPE THE THREADS OF THE ANCHOR BOLTS FOR PROTECTION AGAINST DIRT, SAND AND CONCRETE.
2. LOCAL FROST LINE REQUIREMENTS MAY SUPERSEDE DRAWN DEPTH DIMENSIONS.
3. 4'-0" DEPTH FOR CONCRETE BASES IN NORTHERN LATITUDES MAY BE REQUIRED.



NOTES:

1. THE EXISTING LARGE INFORMATIONAL SIGN SHOWN ON THIS SHEET IS CURRENTLY LOCATED IN FRONT OF THE YORKVILLE HIGH SCHOOL. THE SIGN SHALL BE RELOCATED OUT FROM THE RIGHT-OF-WAY AND INTO THE ADJACENT PROPOSED DEDICATED EASEMENT, OR AS DIRECTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. THE DETAIL SHOWN ON THIS SHEET IS FOR THE EXISTING HIGH SCHOOL SIGN AND SHALL BE USED FOR INFORMATIONAL PURPOSES ONLY.

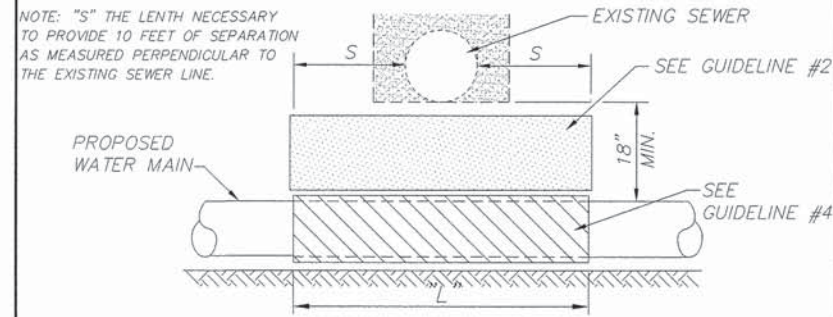
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| | STRUCTURE NOTATIONS | CHKD | |

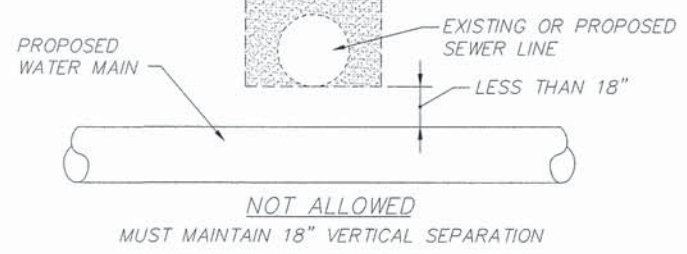
WATER AND SEWER SEPARATION REQUIREMENTS DETAIL

PROPOSED WATER MAIN BELOW EXISTING SEWER LINE WITH 18" MINIMUM SEPARATION.

- GUIDELINES:**
1. OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF WATER MAIN AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT THE LENGTH OF "L".
 2. IF SELECT GRANULAR BACKFILL EXISTS, REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
 3. PROVIDE ADEQUATE SUPPORT FOR EXISTING SEWER LINE TO PREVENT DAMAGE DUE TO SETTLEMENT.
 4. USE "L" FEET OF WATER MAIN MATERIAL FOR CASING OF PROPOSED WATER MAIN AND SEAL ENDS OF CASING.

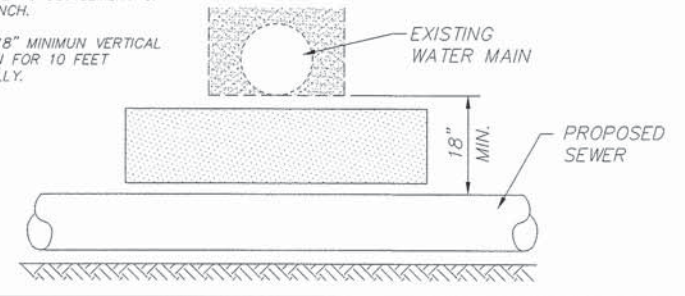


PLACEMENT WATER MAIN BELOW EXISTING OR PROPOSED SEWER LINE WITH LESS THAN 18" MINIMUM SEPARATION.



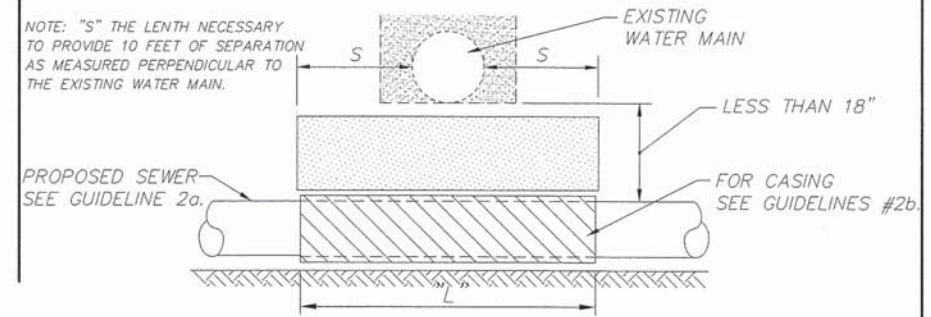
PROPOSED SEWER LINE BELOW EXISTING WATER MAIN WITH 18" MINIMUM SEPARATION.

- GUIDELINES:**
1. PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH.
 2. MAINTAIN 18" MINIMUM VERTICAL SEPARATION FOR 10 FEET HORIZONTALLY.



PROPOSED SEWER LINE BELOW EXISTING WATER MAIN WITH LESS THAN 18" SEPARATION.

- GUIDELINES:**
1. OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF SEWER AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT FOR "S" FEET ON EACH SIDE OF WATER MAIN.
 - 2a. CONSTRUCT "L" FEET OF PROPOSED SEWER OF WATER MAIN MATERIAL AND PRESSURE TEST, OR OR;
 - 2b. USE "L" FEET OF WATER MAIN MATERIAL FOR CASING OF PROPOSED SEWER & SEAL ENDS OF CASING.
 3. PROVIDE ADEQUATE SUPPORT FOR EXISTING WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH.
 4. CLASS IV MATERIAL TO BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY (SEE ARTICLE 20-4).



▲ TAKEN FROM THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, 6th EDITION - SEE PAGES 192, 194, 198 & 200.

| | | | | |
|---------------------|----------------|----------|------------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 71 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

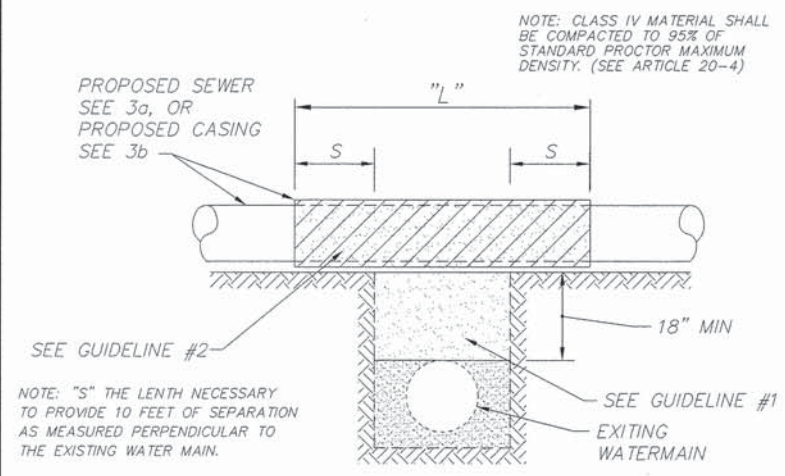
CONTRACT NO. 87345

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| | STRUCTURE NOTATIONS CHECKED | | |
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WATER AND SEWER SEPARATION REQUIREMENTS DETAIL

PROPOSED SEWER LINE WITH 18" MINIMUM VERTICAL SEPARATION ABOVE EXISTING WATER MAIN

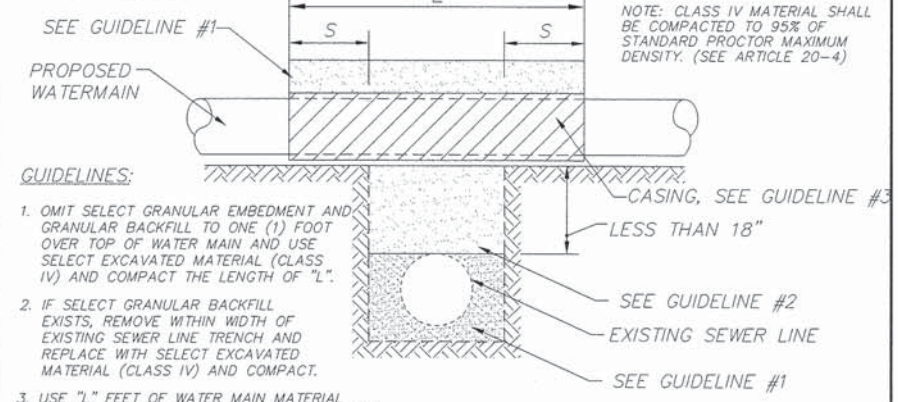


NOTE: "S" THE LENGTH NECESSARY TO PROVIDE 10 FEET OF SEPARATION AS MEASURED PERPENDICULAR TO THE EXISTING WATER MAIN.

NOTES:

1. IF SELECT GRANULAR BACKFILL EXISTS; REMOVE WITHIN WIDTH OF PROPOSED SEWER TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
2. OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF PIPE AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT THE LENGTH OF "L" FEET.
3. a. CONSTRUCT "L" FEET OF PROPOSED SEWER WITH WATER MAIN MATERIAL AND PRESSURE TEST, OR;
b. USE "L" FEET OF WATER MAIN MATERIAL FRO CASING OF PROPOSED SEWER AND SEAL ENDS OF CASING.

PROPOSED WATER MAIN ABOVE EXISTING SEWER LINE WITH LESS THAN 18" SEPARATION.



GUIDELINES:

1. OMIT SELECT GRANULAR EMBEDMENT AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF WATER MAIN AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT THE LENGTH OF "L".
2. IF SELECT GRANULAR BACKFILL EXISTS, REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
3. USE "L" FEET OF WATER MAIN MATERIAL FOR CASING OF PROPOSED WATER MAIN AND SEAL ENDS OF CASING.
4. POINT LOADS SHALL NOT BE ALLOWED BETWEEN WATER MAIN CASING AND SEWER.

NOTE: "S" THE LENGTH NECESSARY TO PROVIDE 10 FEET OF SEPARATION AS MEASURED PERPENDICULAR TO THE EXISTING WATER MAIN.

▲ TAKEN FROM THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, 6th EDITION - SEE PAGE 190 AND 196

COMPANY NAME:
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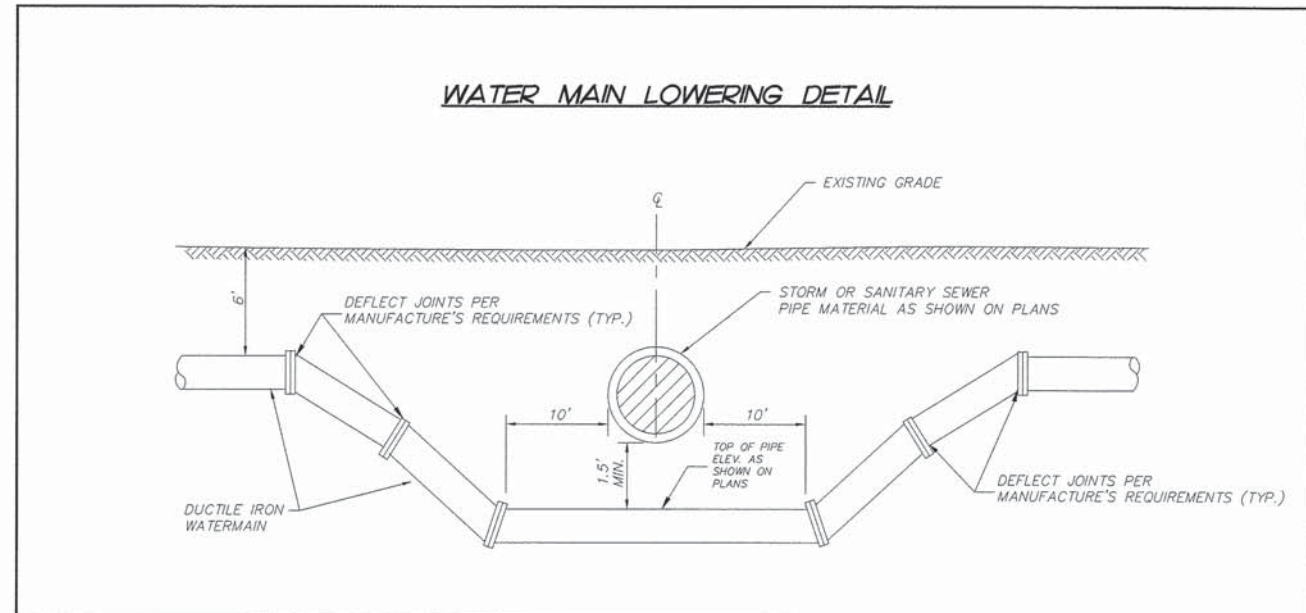
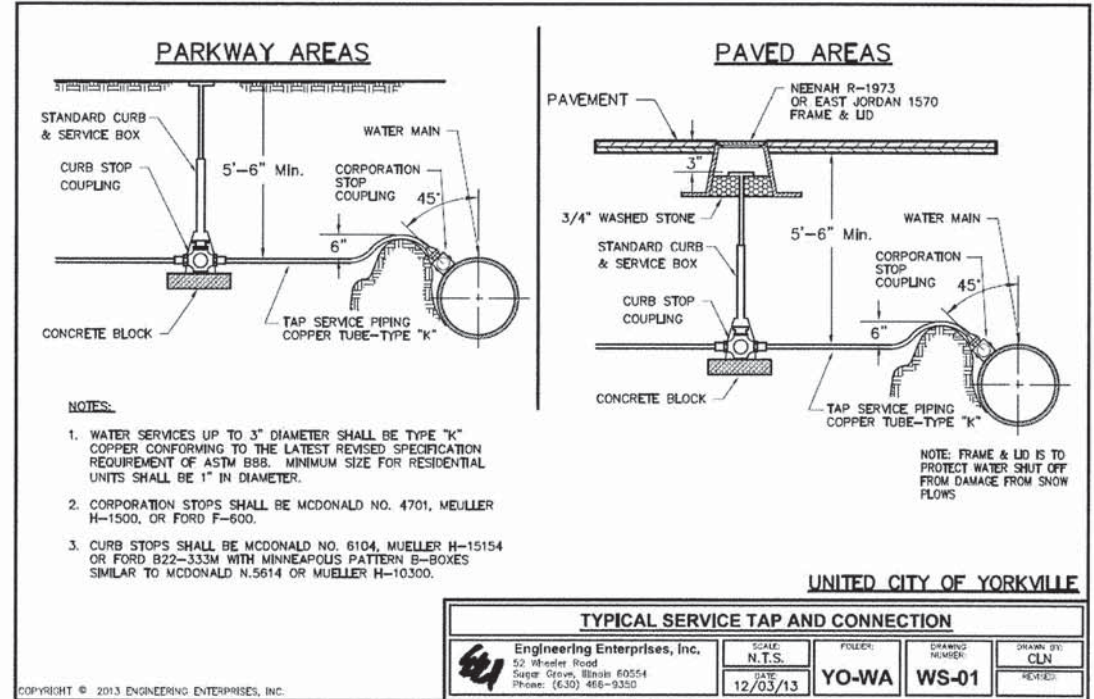
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
MISCELLANEOUS DETAILS

SCALE: VERT.
HORIZ.
DATE: JULY 2014

DRAWN BY: M.J.F.
CHECKED BY: G.T.

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ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
MISCELLANEOUS DETAILS

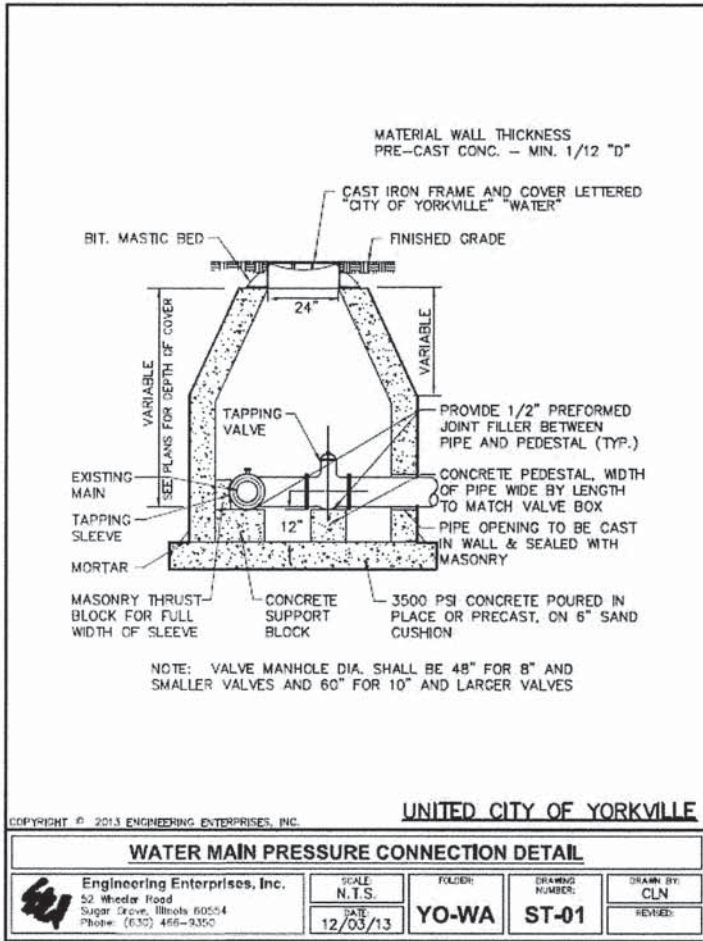
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HORIZ. _____
DATE: JULY 2014

DRAWN BY: MJF
CHECKED BY: GT

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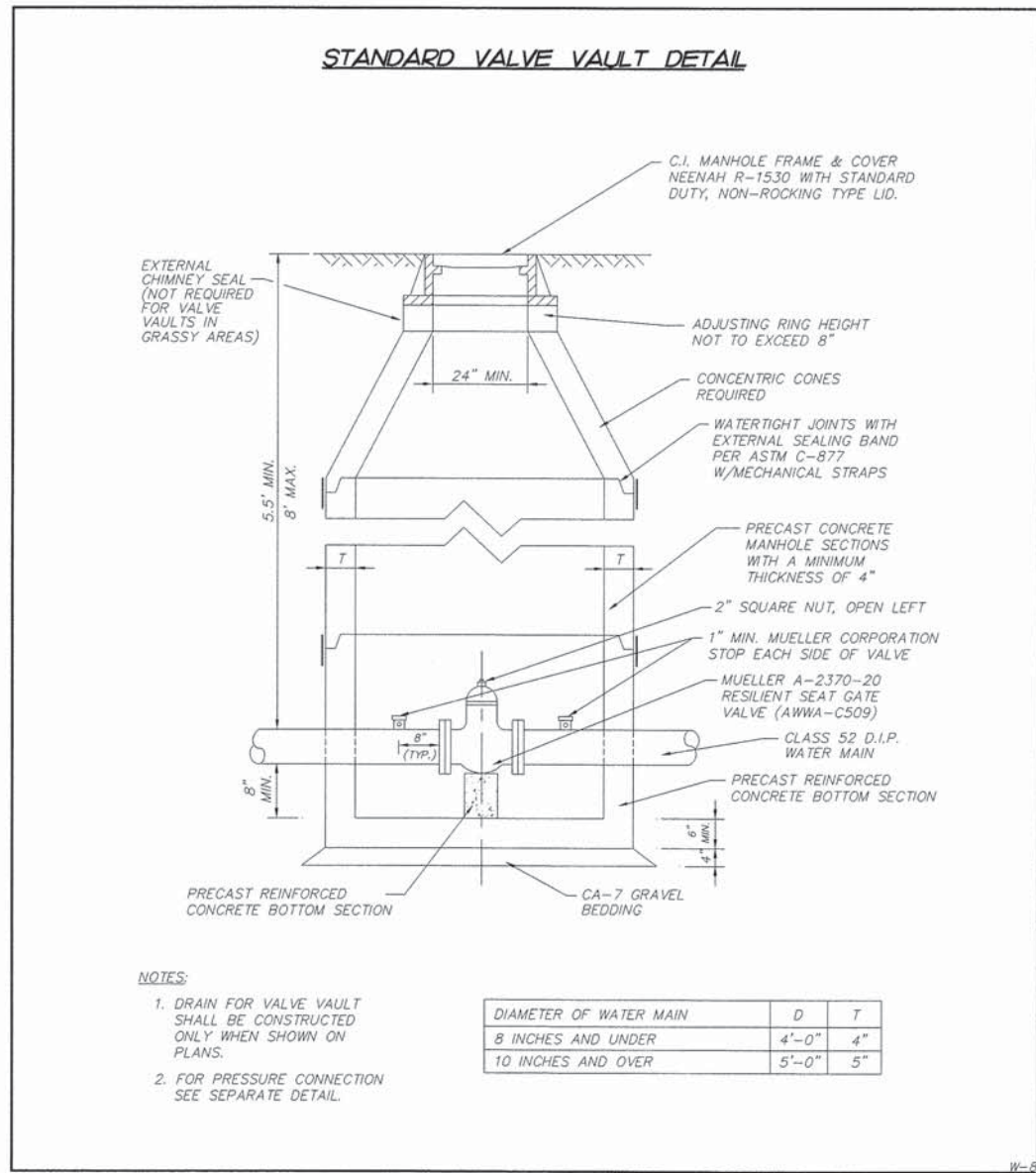
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UNITED CITY OF YORKVILLE

COPYRIGHT © 2013 ENGINEERING ENTERPRISES, INC.

| | | | |
|--|------------------|--------------------|-------------------------|
| WATER MAIN PRESSURE CONNECTION DETAIL | | | |
| <p>Engineering Enterprises, Inc. 52 Wheeler Road Sugar Grove, Illinois 60554 Phone: (630) 466-9350</p> | SCALE N.T.S. | FOLDER YO-WA | DRAWING NUMBER ST-01 |
| | DATE 12/03/13 | DESIGNED BY CLN | CHECKED BY REVISED |



| DIAMETER OF WATER MAIN | D | T |
|------------------------|-------|----|
| 8 INCHES AND UNDER | 4'-0" | 4" |
| 10 INCHES AND OVER | 5'-0" | 5" |

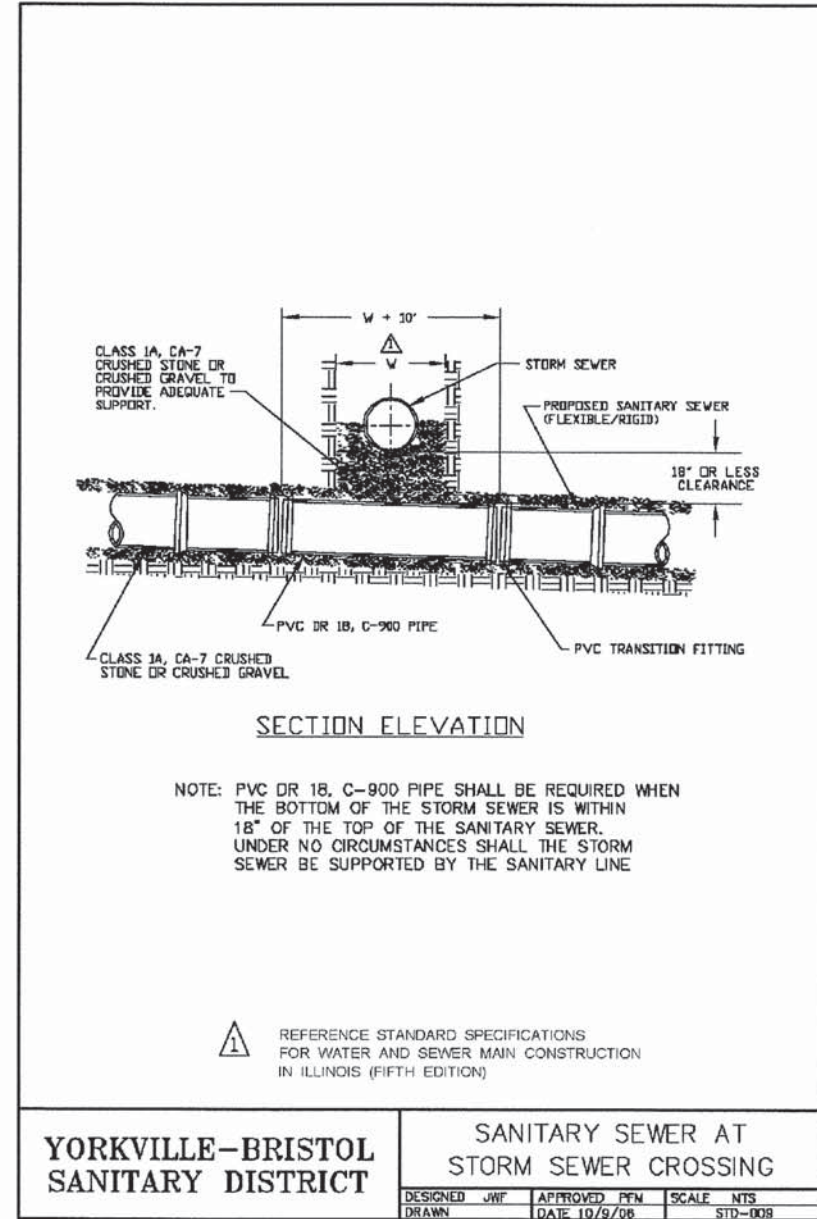
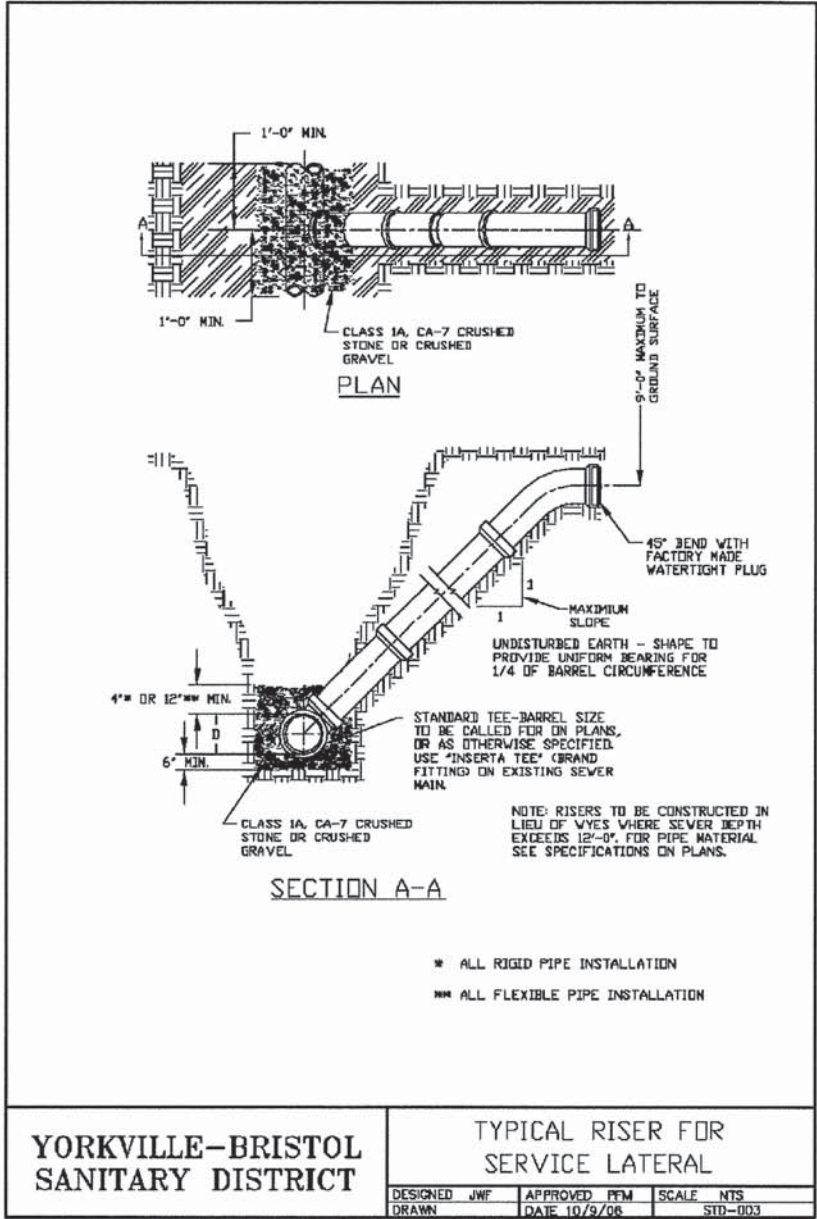
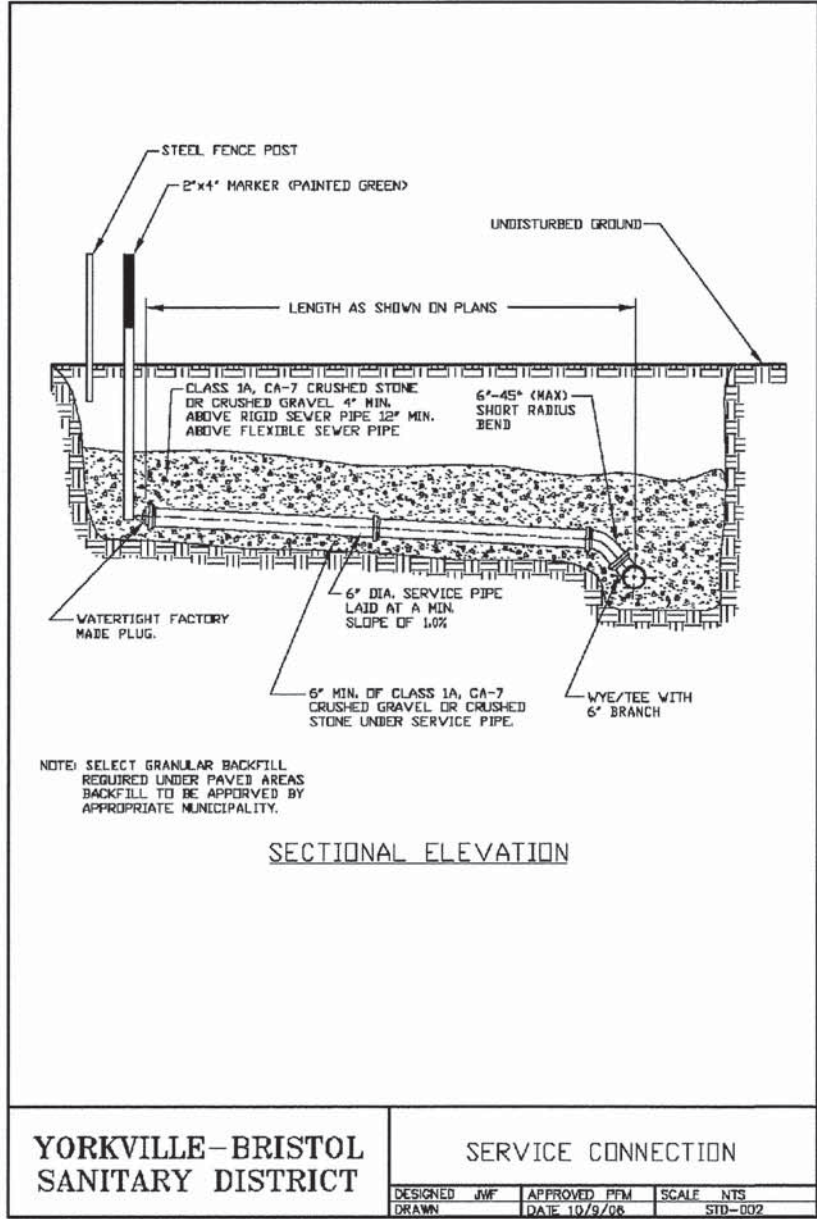
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
MISCELLANEOUS DETAILS

SCALE: VERT. HORIZ. DATE: JULY 2014 DRAWN BY: MJF CHECKED BY: GT

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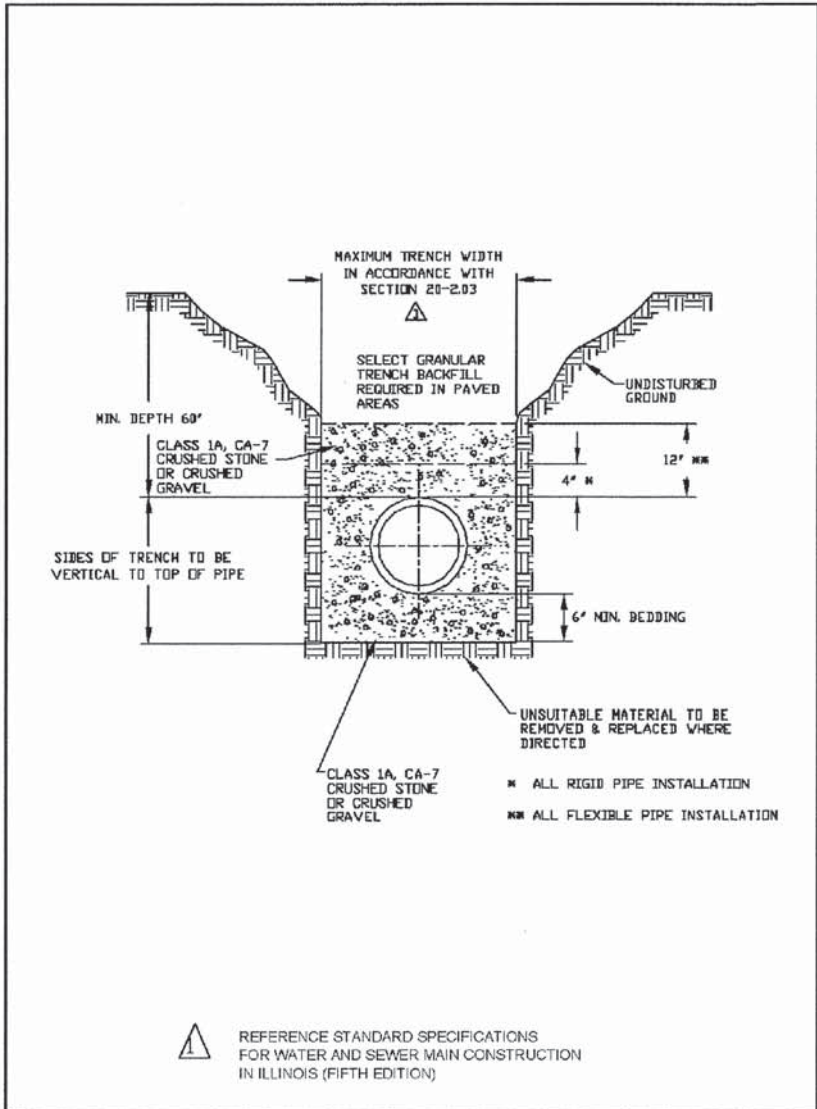
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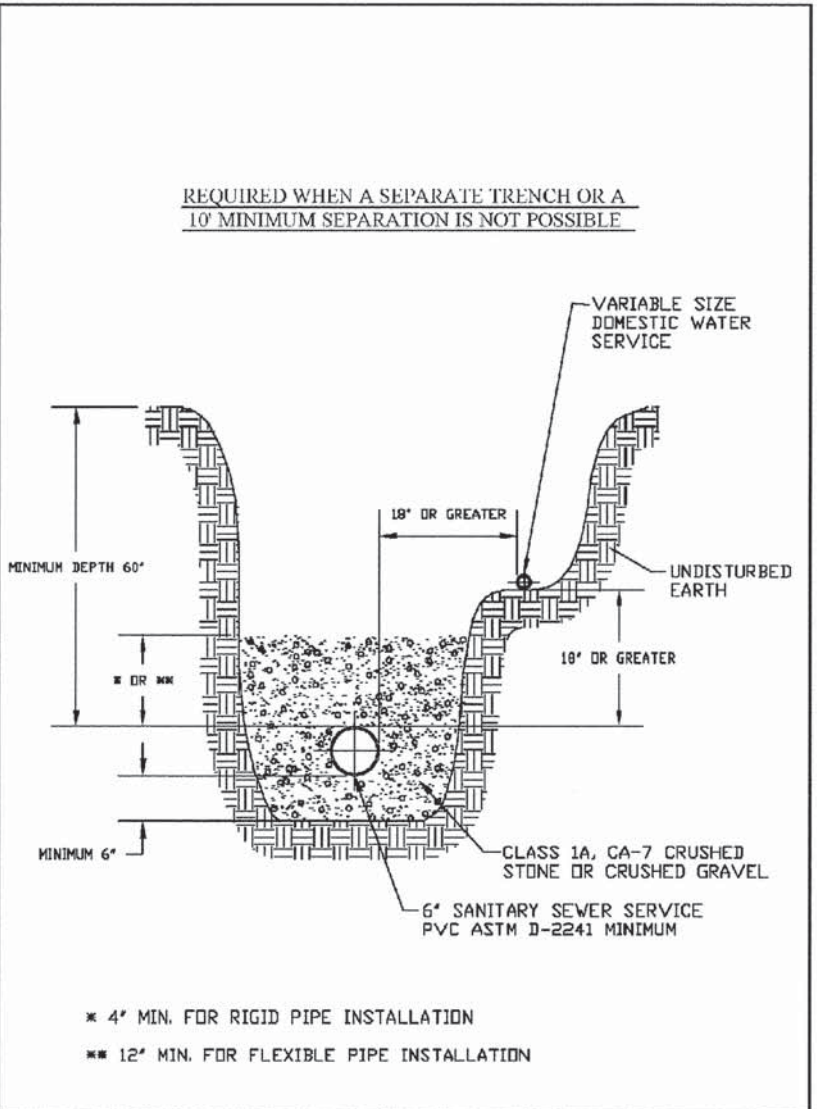
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| | B.M. NOTED | | |
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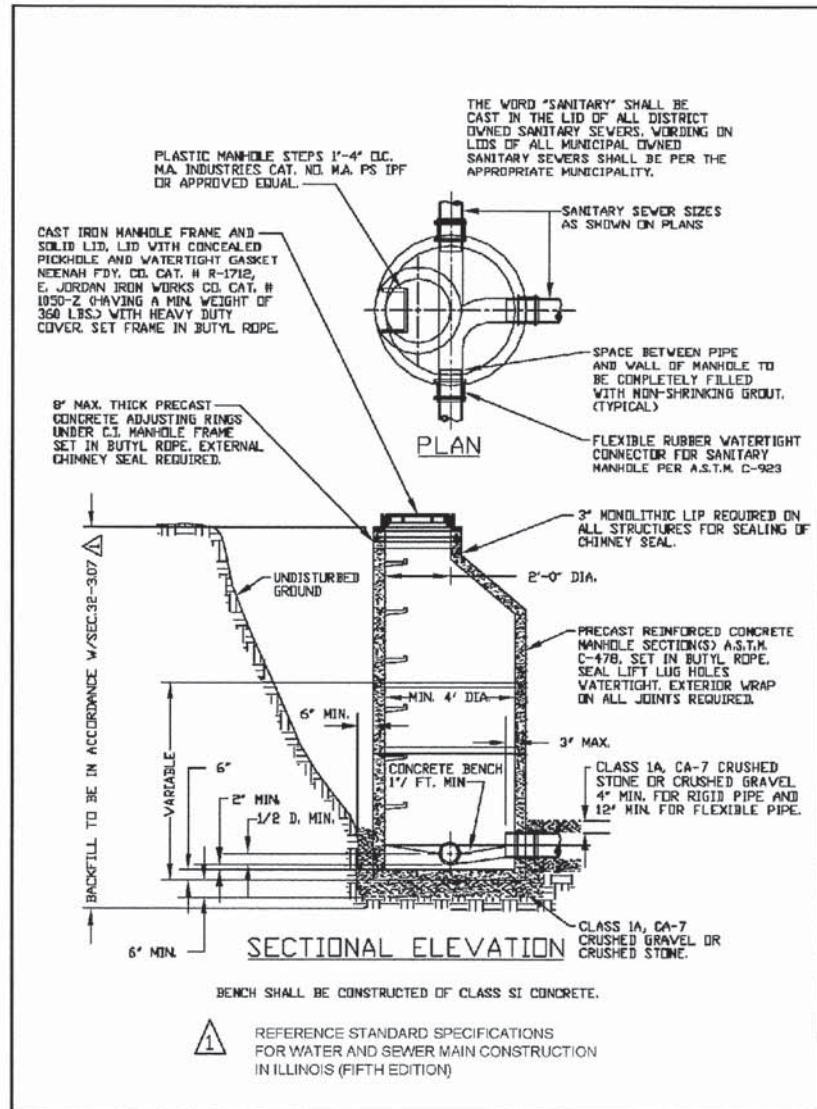
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YORKVILLE-BRISTOL SANITARY DISTRICT
TYPICAL TRENCH DETAIL FOR SANITARY SERVICES & MAINS
 DESIGNED JWF APPROVED PFM SCALE NTS
 DRAWN DATE 10/9/08 STD-001



YORKVILLE-BRISTOL SANITARY DISTRICT
SANITARY SEWER SERVICE & POTABLE WATER SERVICE SEPARATION
 DESIGNED JWF APPROVED PFM SCALE NTS
 DRAWN DATE 10/9/08 STD-010



YORKVILLE-BRISTOL SANITARY DISTRICT
TYPE 'A' MANHOLE
 DESIGNED JWF APPROVED PFM SCALE NTS
 DRAWN DATE 10/9/08 STD-005

4" WATERMAIN FITTING WEIGHTS WITH JOINTS ACCESSORIES

| BENDS: | MJ | MJ-PE |
|--------|--------|--------|
| 90E | 42 lb. | 36 lb. |
| 45E | 38 lb. | 30 lb. |
| 22.5E | 37 lb. | 28 lb. |
| 11.25E | 36 lb. | 27 lb. |

| TEES AND CROSSES: | TEES-MJ | CROSSES-MJ |
|-------------------|---------|------------|
| 4 x 4 | 59 lb. | 74 lb. |
| 4 x 3 | 53 lb. | 64 lb. |

| WYES: | MJ |
|-------|--------|
| 4 x 4 | 69 lb. |
| 4 x 3 | 61 lb. |

| REDUCERS: | MJ | PE | SEMJ-LEPE | LEMJ-SEPE |
|-----------|--------|--------|-----------|-----------|
| 4 x 3 | 31 lb. | 17 lb. | 22 lb. | 26 lb. |

| SLEEVES: | SHORT | LONG |
|----------|--------|--------|
| | 33 lb. | 41 lb. |

| PLUG/CAPS: | PLUG | CAP |
|------------|--------|--------|
| | 10 lb. | 18 lb. |

10" WATERMAIN FITTING WEIGHTS WITH JOINTS ACCESSORIES

| BENDS: | MJ | MJ-PE |
|--------|---------|---------|
| 90E | 147 lb. | 129 lb. |
| 45E | 121 lb. | 103 lb. |
| 22.5E | 107 lb. | 86 lb. |
| 11.25E | 101 lb. | 80 lb. |

| TEES AND CROSSES: | TEES-MJ | CROSSES-MJ |
|-------------------|---------|------------|
| 10 x 10 | 180 lb. | 235 lb. |
| 10 x 8 | 166 lb. | 208 lb. |
| 10 x 6 | 144 lb. | 180 lb. |
| 10 x 4 | 131 lb. | 154 lb. |

| WYES: | MJ |
|---------|---------|
| 10 x 10 | 259 lb. |
| 10 x 8 | 225 lb. |
| 10 x 6 | 187 lb. |
| 10 x 4 | 166 lb. |

| REDUCERS: | MJ | PE | SEMJ-LEPE | LEMJ-SEPE |
|-----------|--------|--------|-----------|-----------|
| 10 x 8 | 89 lb. | 47 lb. | 67 lb. | 72 lb. |
| 10 x 6 | 78 lb. | 48 lb. | 59 lb. | 72 lb. |
| 10 x 4 | 75 lb. | 49 lb. | 54 lb. | 63 lb. |

| SLEEVES: | SHORT | LONG |
|----------|--------|---------|
| | 89 lb. | 104 lb. |

| PLUG/CAPS: | PLUG | CAP |
|------------|--------|--------|
| | 36 lb. | 55 lb. |

6" WATERMAIN FITTING WEIGHTS WITH JOINTS ACCESSORIES

| BENDS: | MJ | MJ-PE |
|--------|--------|--------|
| 90E | 70 lb. | 56 lb. |
| 45E | 60 lb. | 48 lb. |
| 22.5E | 59 lb. | 47 lb. |
| 11.25E | 55 lb. | 43 lb. |

| TEES AND CROSSES: | TEES-MJ | CROSSES-MJ |
|-------------------|---------|------------|
| 6 x 6 | 93 lb. | 124 lb. |
| 6 x 4 | 81 lb. | 100 lb. |
| 6 x 3 | 74 lb. | |

| WYES: | MJ |
|-------|---------|
| 6 x 6 | 126 lb. |
| 6 x 4 | 97 lb. |

| REDUCERS: | MJ | PE | SEMJ-LEPE | LEMJ-SEPE |
|-----------|--------|--------|-----------|-----------|
| 6 x 4 | 46 lb. | 26 lb. | 34 lb. | 38 lb. |
| 6 x 3 | 42 lb. | 20 lb. | 30 lb. | 38 lb. |

| SLEEVES: | SHORT | LONG |
|----------|--------|--------|
| | 50 lb. | 61 lb. |

| PLUG/CAPS: | PLUG | CAP |
|------------|--------|--------|
| | 18 lb. | 28 lb. |

12" WATERMAIN FITTING WEIGHTS WITH JOINTS ACCESSORIES

| BENDS: | MJ | MJ-PE |
|--------|---------|---------|
| 90E | 185 lb. | 157 lb. |
| 45E | 155 lb. | 130 lb. |
| 22.5E | 124 lb. | 99 lb. |
| 11.25E | 123 lb. | 88 lb. |

| TEES AND CROSSES: | TEES-MJ | CROSSES-MJ |
|-------------------|---------|------------|
| 12 x 12 | 244 lb. | 300 lb. |
| 12 x 10 | 217 lb. | 271 lb. |
| 12 x 8 | 182 lb. | 236 lb. |
| 12 x 6 | 170 lb. | 206 lb. |

| WYES: | MJ |
|---------|---------|
| 12 x 12 | 338 lb. |
| 12 x 10 | 287 lb. |
| 12 x 8 | 247 lb. |
| 12 x 6 | 241 lb. |

| REDUCERS: | MJ | PE | SEMJ-LEPE | LEMJ-SEPE |
|-----------|---------|--------|-----------|-----------|
| 12 x 10 | 120 lb. | 59 lb. | 82 lb. | 87 lb. |
| 12 x 8 | 101 lb. | 62 lb. | 77 lb. | 87 lb. |
| 12 x 6 | 100 lb. | 58 lb. | 69 lb. | 88 lb. |
| 12 x 4 | 97 lb. | 60 lb. | 69 lb. | 90 lb. |

| SLEEVES: | SHORT | LONG |
|----------|---------|---------|
| | 100 lb. | 126 lb. |

| PLUG/CAPS: | PLUG | CAP |
|------------|--------|--------|
| | 46 lb. | 66 lb. |

8" WATERMAIN FITTING WEIGHTS WITH JOINTS ACCESSORIES

| BENDS: | MJ | MJ-PE |
|--------|--------|--------|
| 90E | 98 lb. | 80 lb. |
| 45E | 89 lb. | 71 lb. |
| 22.5E | 81 lb. | 65 lb. |
| 11.25E | 78 lb. | 59 lb. |

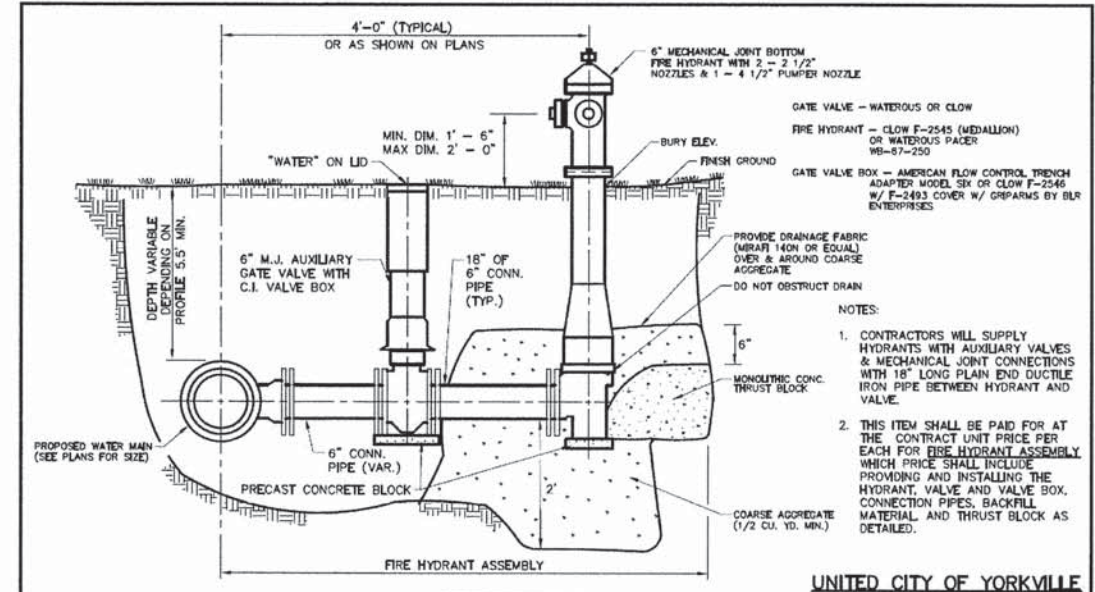
| TEES AND CROSSES: | TEES-MJ | CROSSES-MJ |
|-------------------|---------|------------|
| 8 x 8 | 135 lb. | 180 lb. |
| 8 x 6 | 121 lb. | 160 lb. |
| 8 x 4 | 109 lb. | 130 lb. |

| WYES: | MJ |
|-------|---------|
| 8 x 8 | 181 lb. |
| 8 x 6 | 151 lb. |
| 8 x 4 | 131 lb. |

| REDUCERS: | MJ | PE | SEMJ-LEPE | LEMJ-SEPE |
|-----------|--------|--------|-----------|-----------|
| 8 x 6 | 66 lb. | 36 lb. | 48 lb. | 54 lb. |
| 8 x 4 | 59 lb. | 33 lb. | 42 lb. | 51 lb. |

| SLEEVES: | SHORT | LONG |
|----------|--------|--------|
| | 68 lb. | 83 lb. |

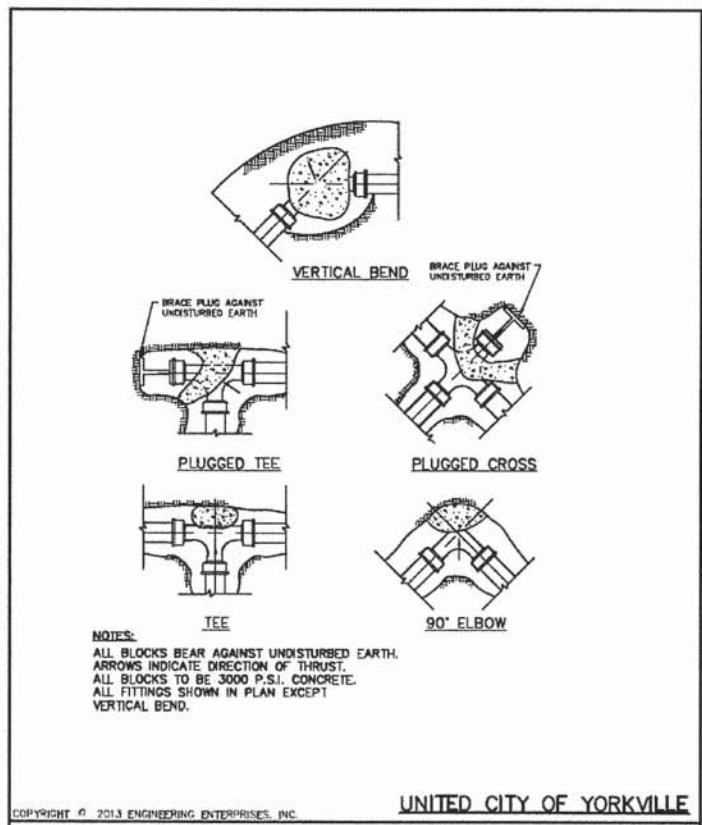
| PLUG/CAPS: | PLUG | CAP |
|------------|--------|--------|
| | 26 lb. | 40 lb. |



UNITED CITY OF YORKVILLE

FIRE HYDRANT ASSEMBLY INSTALLATION

| | | | | | | | | |
|--|--------|----------|----------|-------|-----------------|---------|-----------|-----|
| Engineering Enterprises, Inc. 52 Wheeler Road Sugar Grove, Illinois 60554 Phone: (630) 466-9350 | SCALE: | N.T.S. | FOLDER: | YO-WA | DRAWING NUMBER: | MI-01 | DRAWN BY: | cln |
| | DATE: | 12/03/13 | PROJECT: | YO-WA | HY-01 | REVIEW: | | |



UNITED CITY OF YORKVILLE

TYPICAL THRUST BLOCK INSTALLATION

| | | | | | | | | |
|--|--------|----------|----------|-------|-----------------|---------|-----------|-----|
| Engineering Enterprises, Inc. 52 Wheeler Road Sugar Grove, Illinois 60554 Phone: (630) 466-9350 | SCALE: | N.T.S. | FOLDER: | YO-WA | DRAWING NUMBER: | MI-01 | DRAWN BY: | cln |
| | DATE: | 12/03/13 | PROJECT: | YO-WA | MI-01 | REVIEW: | | |

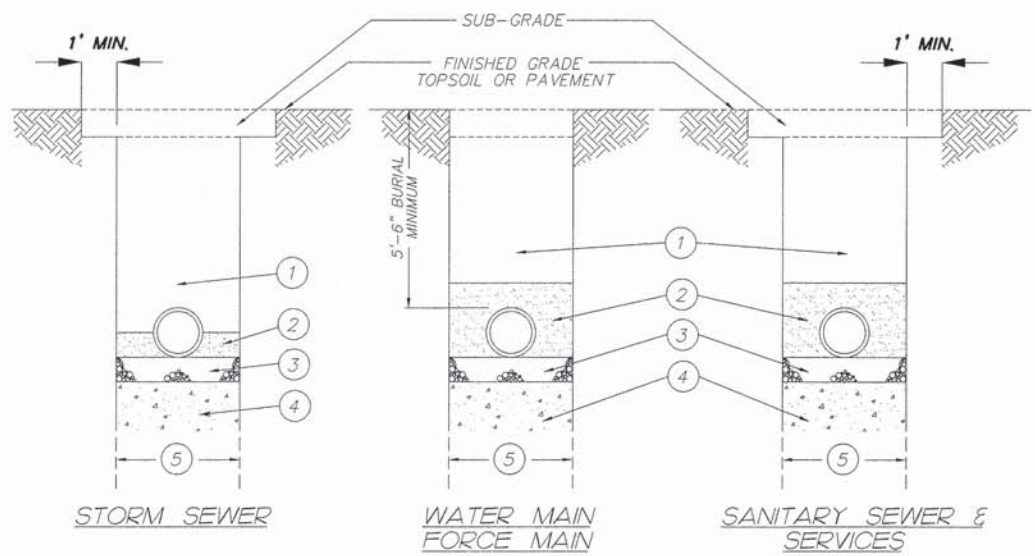
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
MISCELLANEOUS DETAILS

SCALE: VERT. _____ HORIZ. _____ DATE: JULY 2014 DRAWN BY: M.J.F. CHECKED BY: G.T.

PLAN SURVEYED BY: _____ DATE: _____
PLOTTED BY: _____
NOTE BOOK NO. _____
ALIGNMENT CHECKED BY: _____
RT. OF WAY CHECKED BY: _____
CADD FILE NAME: _____

PROFILE SURVEYED BY: _____ DATE: _____
PLOTTED BY: _____
NOTE BOOK NO. _____
GRADES CHECKED BY: _____
B.M. NOTED BY: _____
STRUCTURE NOTATIONS CHKD BY: _____

TRENCH BEDDING/BACKFILLING CROSS SECTION



- ① Trench backfill under pavement, curb and gutter as indicated in road subgrades and within 2 feet of any proposed curb and gutter or sidewalk. Mechanically compacted backfill of excavated materials in other locations if approved by the City Engineer. Refer to trench backfill special provisions for materials and compaction requirements.
- ② SANITARY SEWER (PVC, DIP), WATER MAIN, FORCE MAIN, STORM SEWER (HDPE)
Compacted granular material, CA-6 to 4"-6" above top of pipe.
- ② STORM SEWER (RCP)
Compacted granular backfill material CA-7 to spring line of pipe
- ③ 4" compacted granular bedding, CA-7 gradation.
- ④ Unsuitable material to be removed where directed by Engineer and replaced with suitable material and compacted.
- ⑤ Trench Width - Pipe O.D. + 12" minimum
Pipe O.D. + 18" maximum

NOTES:

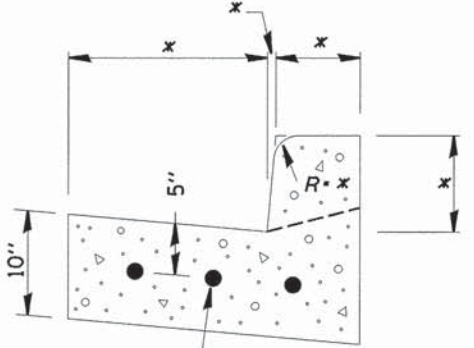
1. PVC pipe conforming to the SDR specified in the plans shall be installed to the latest revised specification requirements of ASTM D-2321 using compacted Class 1 granular embedment materials for bedding, haunching and initial backfill of 12 inches over the top of pipe to provide the necessary support for the pipe so that the maximum deflection does not exceed 5% of the pipe's original internal diameter.
2. CA-7 to be IDOT approved or meet IDOT specifications.

| | | |
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| PLAN | SURVEYED | DATE |
| | PLOTTED | |
| | NOTE BOOK | |
| | ALIGNMENT CHECKED | |
| | RT. OF WAY CHECKED | |
| | NO. _____ | CADD FILE NAME |

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|---------|----------------|--------------------------|
| PROFILE | SURVEYED | DATE |
| | PLOTTED | |
| | GRADES CHECKED | |
| | B.M. NOTED | |
| | NO. _____ | STRUCTURE NOTATIONS CHKD |

COMPANY NAME:
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* VARIES - SEE STANDARD 606001

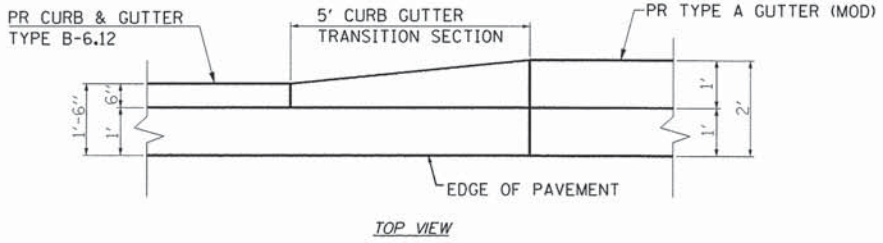
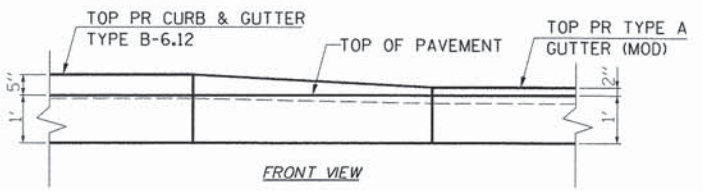


CONTINUOUSLY REINFORCED
WITH 3 - #4 BARS EVENLY SPACED

REINFORCEMENT SHALL NOT BE PAID FOR SEPARATELY BUT
WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR CC&G

REINFORCEMENT DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER

606-4

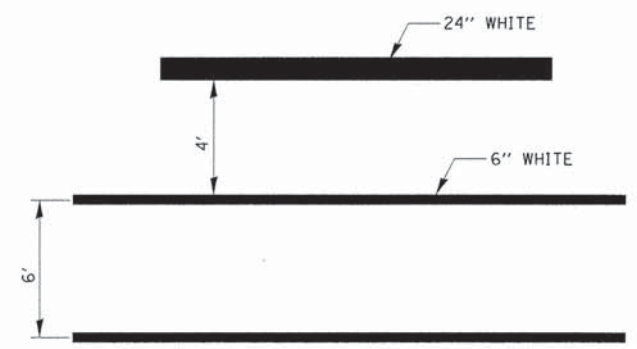


CURB GUTTER TRANSITION DETAIL

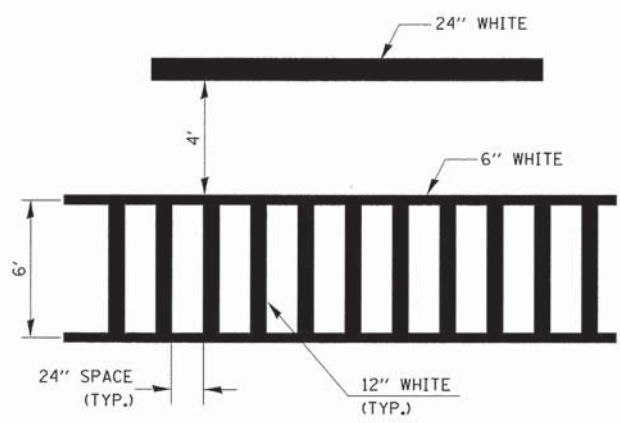
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 78 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |

| | | |
|------|----------------|------|
| PLAN | SURVEYED | DATE |
| | BY | |
| | PLOTTED | |
| | NOTE BOOK | |
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| | CHECKED | |
| | RT. OF WAY | |
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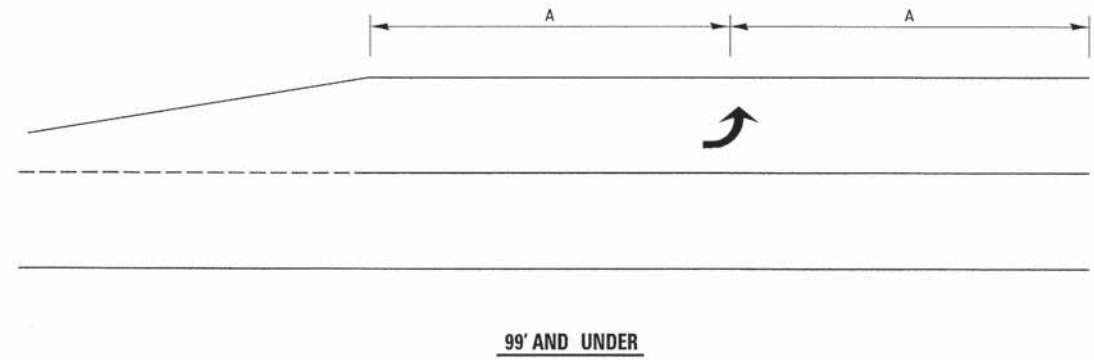
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| | BY | |
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| | STRUCTURE NOTATIONS | |
| | CHKD | |



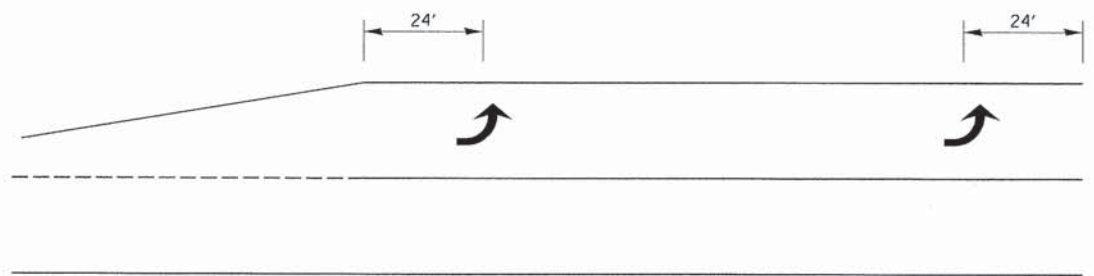
TYPICAL SPACING FOR CROSSWALKS AND STOP BARS



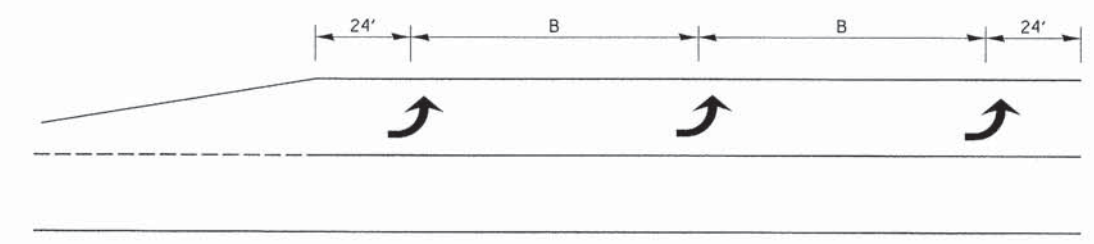
TYPICAL SPACING DETAIL FOR CROSSWALKS AND STOP BARS



99' AND UNDER

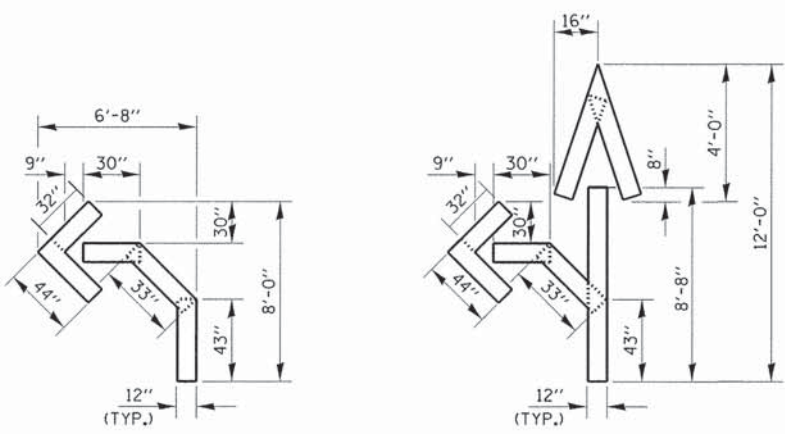


100' TO 149'



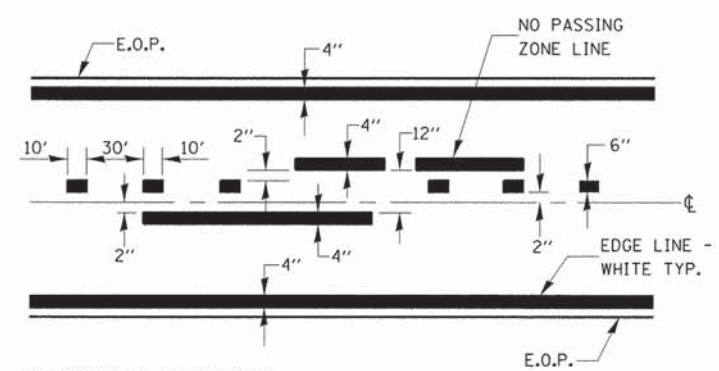
150' AND LONGER

TYPICAL PLACEMENT OF ARROWS IN TURN LANES



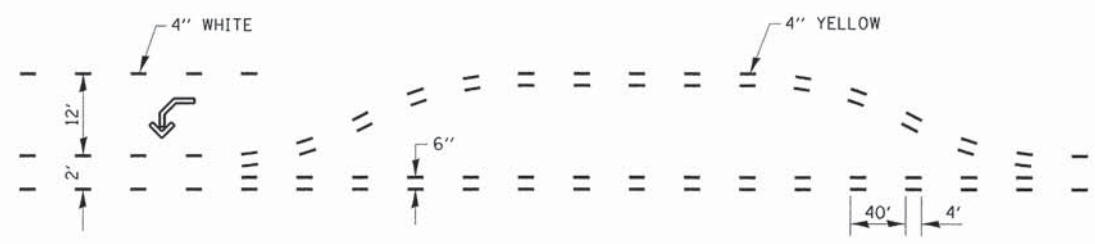
QUANTITY
12" LINE = 16 LIN. FT.
OR 4" LINE = 48 LIN. FT.

QUANTITY
12" LINE = 29 LIN. FT.
OR 4" LINE = 87 LIN. FT.



CENTERLINE & NO PASSING ZONE LINES - YELLOW (SEE TYPICAL SECTIONS)

PAVEMENT MARKING



SHORT-TERM PAVEMENT MARKING FOR MEDIANS AND ARROWS

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD RECONSTRUCTION
MISCELLANEOUS DETAILS

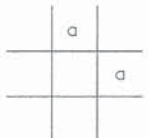
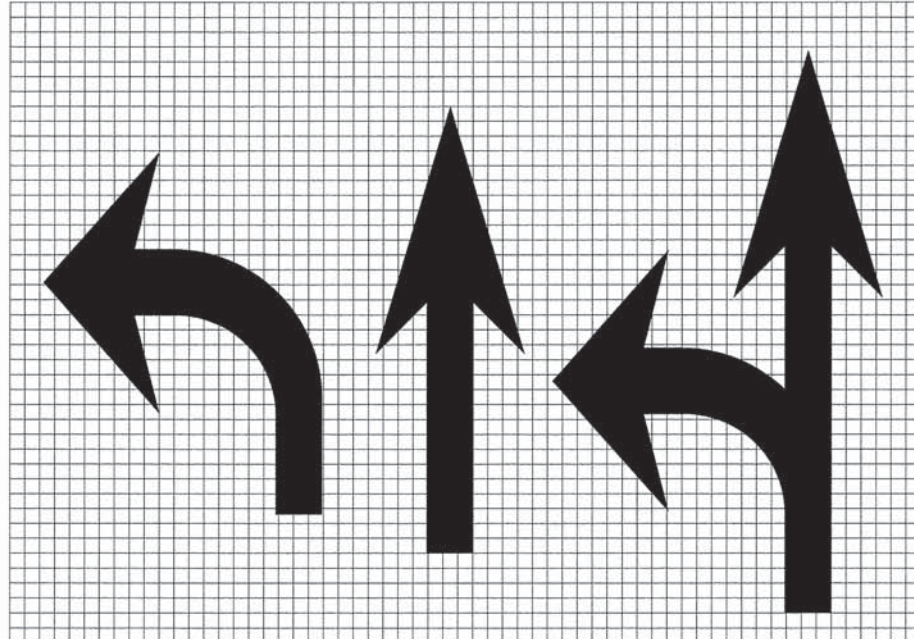
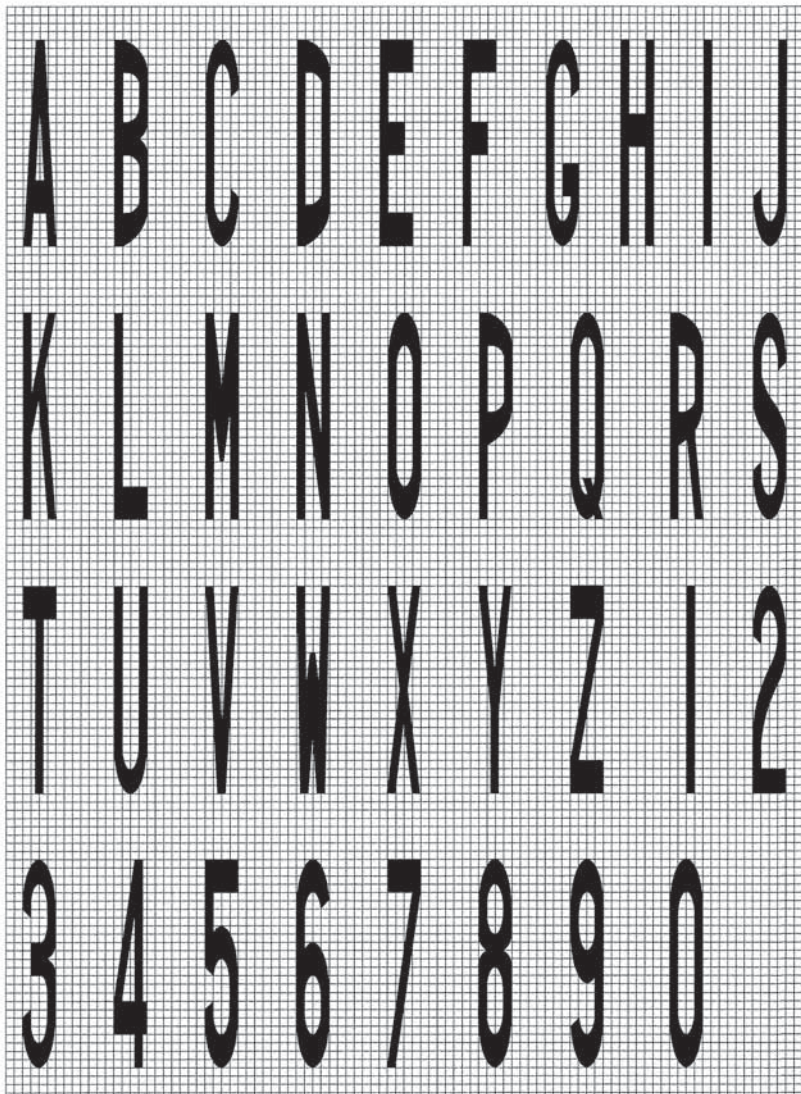
SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: M.J.F.
CHECKED BY: G.T.

COMPANY NAME:
8/21/2014 4:04:56 PM
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PLAN SURVEYED PLOTTED BY DATE
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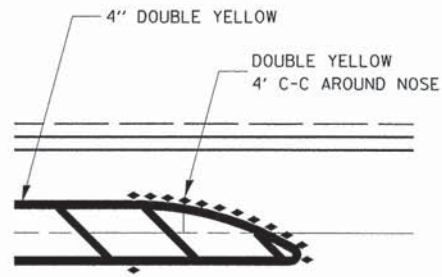
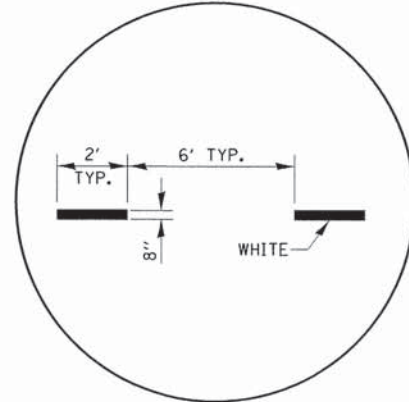
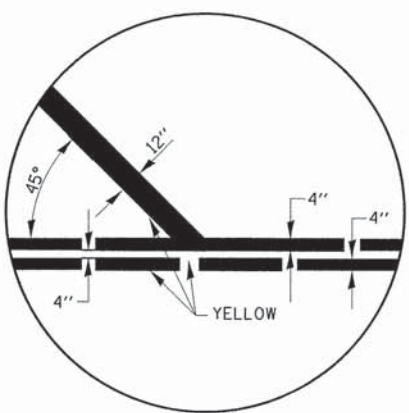
PROFILE SURVEYED PLOTTED BY DATE
 NOTE BOOK GRADES CHECKED
 NO. _____ B.M. NOTED
 STRUCTURE NOTATIONS CRKD



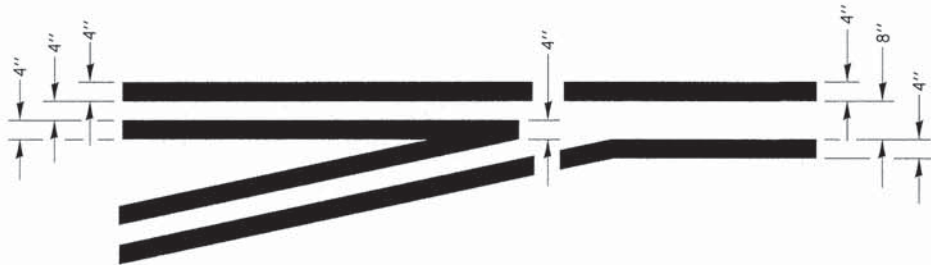
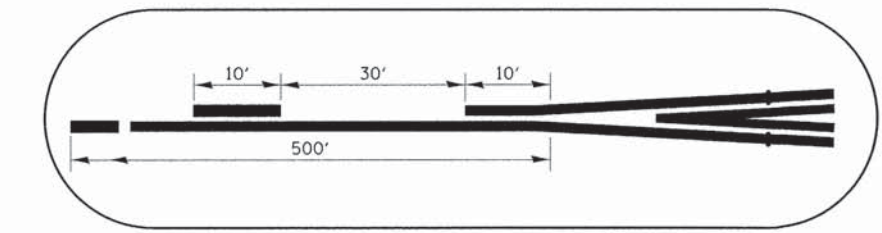
| LEGEND HEIGHT | ARROW SIZE | a |
|---------------|------------|-----|
| 6' | SMALL | 2.9 |
| 8' | LARGE | 3.8 |

THE SPACE BETWEEN ADJACENT LETTERS OR NUMERALS SHOULD BE APPROXIMATELY 3 FOR 6' LEGEND AND 4' FOR 8' LEGEND.

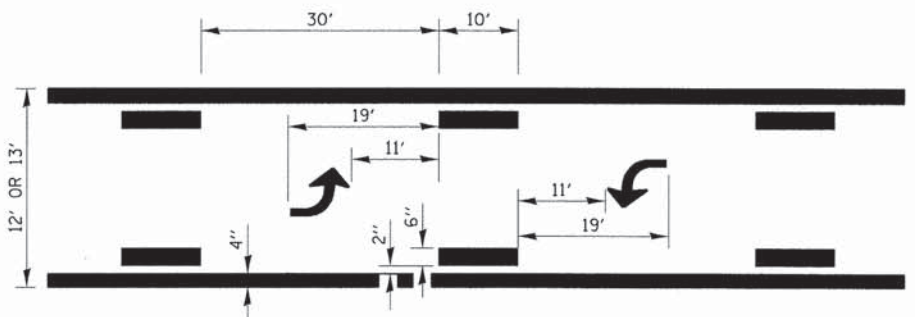
LETTER AND ARROW GRID SCALE



RAISED REFLECTIVE AND PAVEMENT MARKERS



TYPICAL APPLICATION LEFT TURN LANES

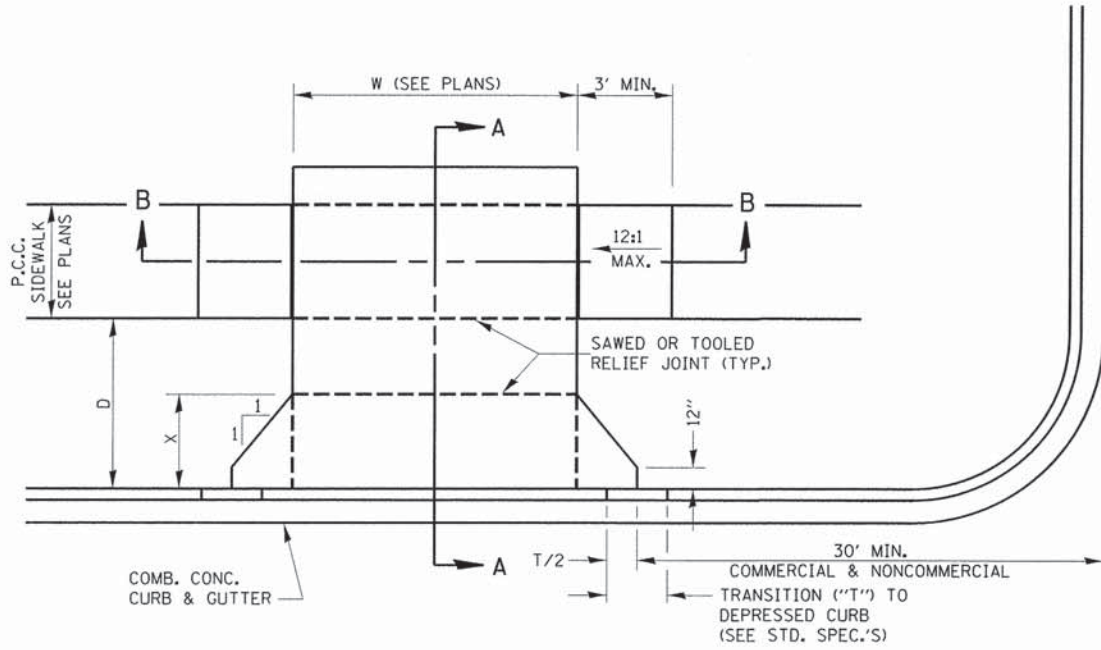


TYPICAL APPLICATION AT BI-DIRECTIONAL TURN LANE

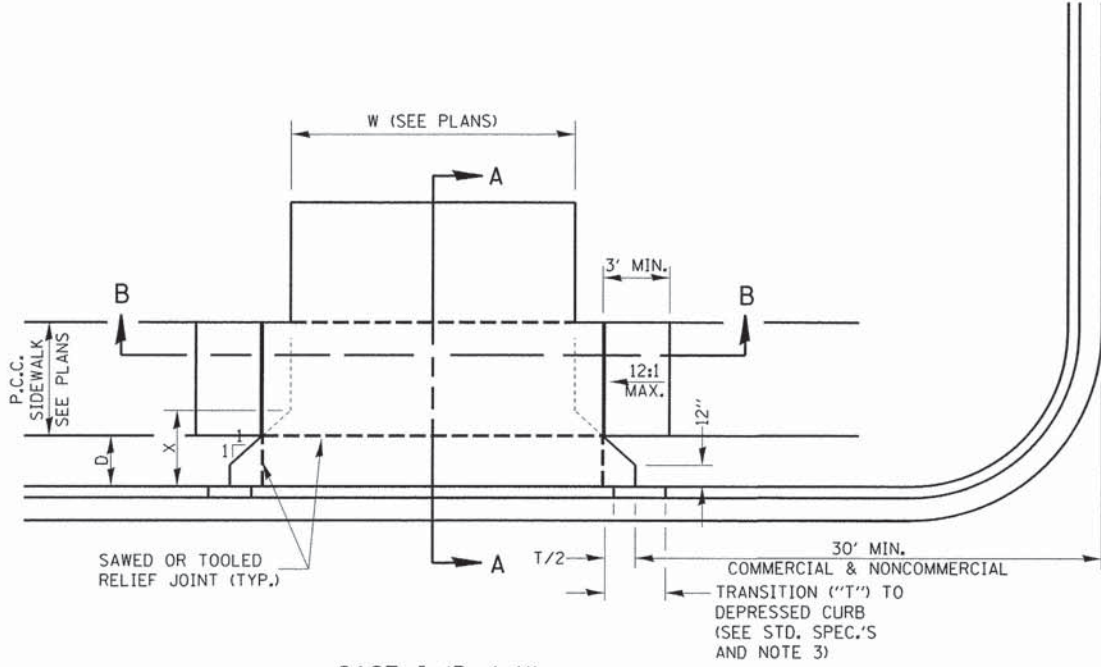
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| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 80 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |
| CONTRACT NO. 87345 | | | | |

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| PLAN | SURVEYED | BY | DATE |
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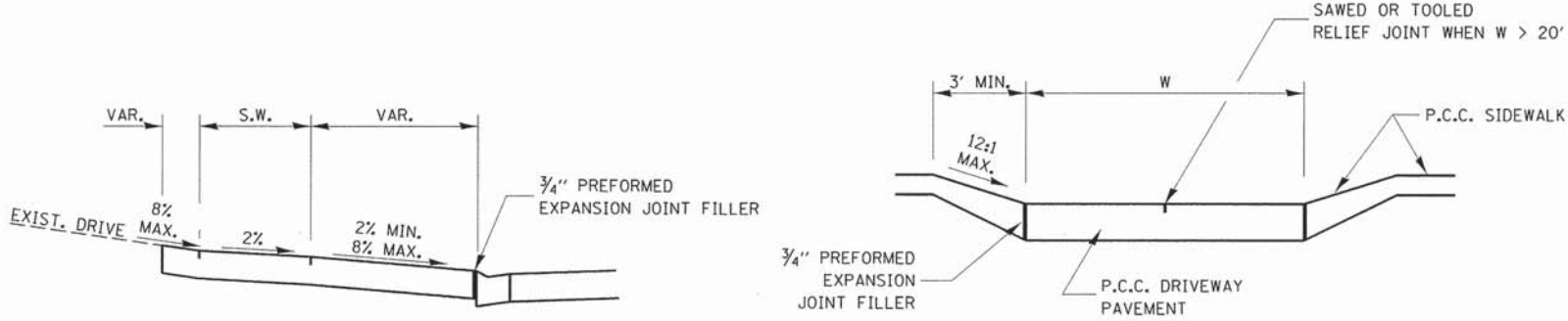
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| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CHKD | | |



CASE I (D ≥ X)



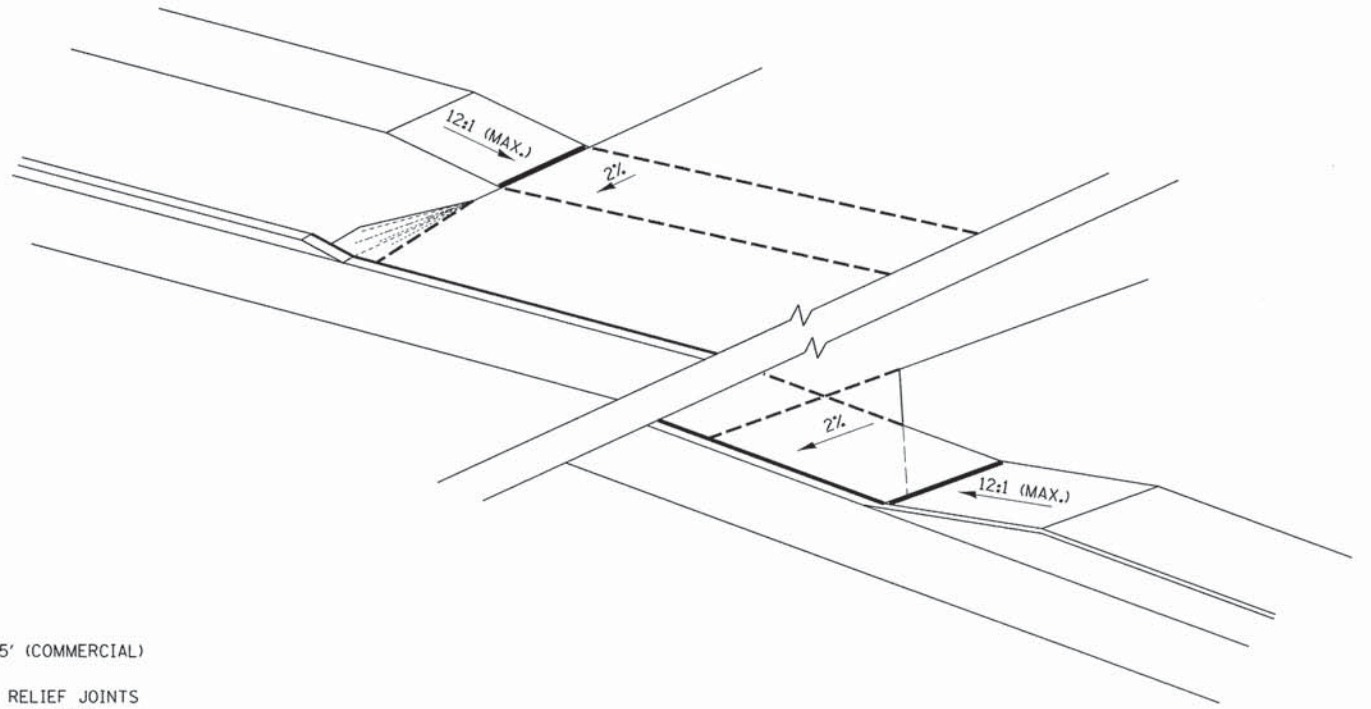
CASE I (D < X)



ELEVATED ENTRANCE * SECTION A-A

SECTION B-B

*(SEE X-SECTIONS FOR ENTRANCE PROFILE.)



- GENERAL NOTES:
- X = 7' (NON-COMMERCIAL) X = 15' (COMMERCIAL)
 - COST OF EXPANSION JOINTS AND RELIEF JOINTS SHALL BE INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT.
 - AS THE DIMENSION "D" APPROACHES ZERO, THE TRANSITION TO DEPRESSED CURB SHALL BE NO STEEPER THAN 12:1

PCC URBAN ENTRANCES

423-1

ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
MISCELLANEOUS DETAILS

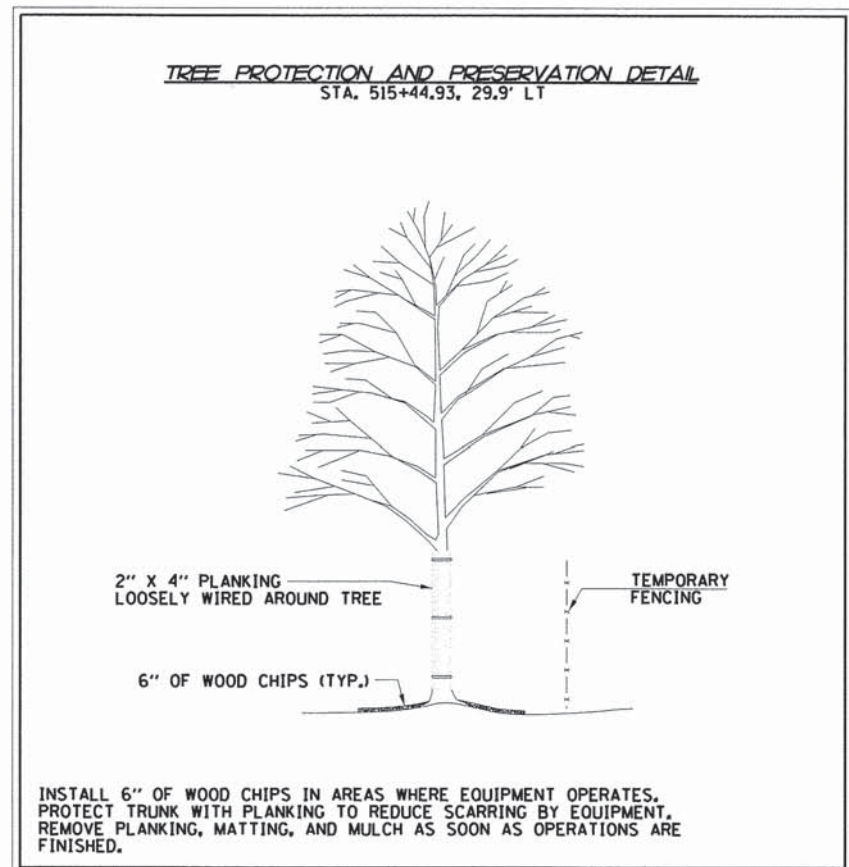
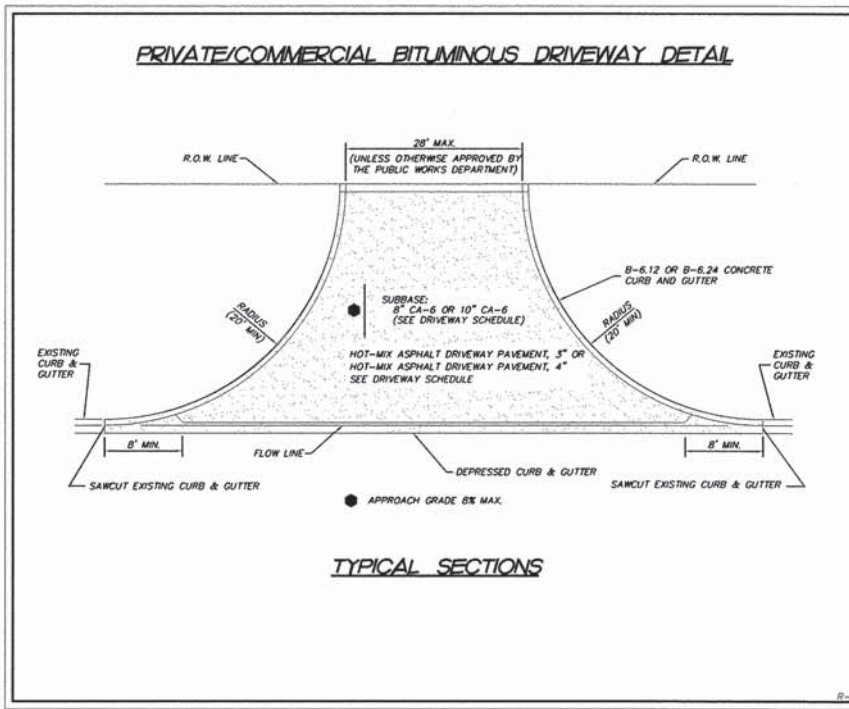
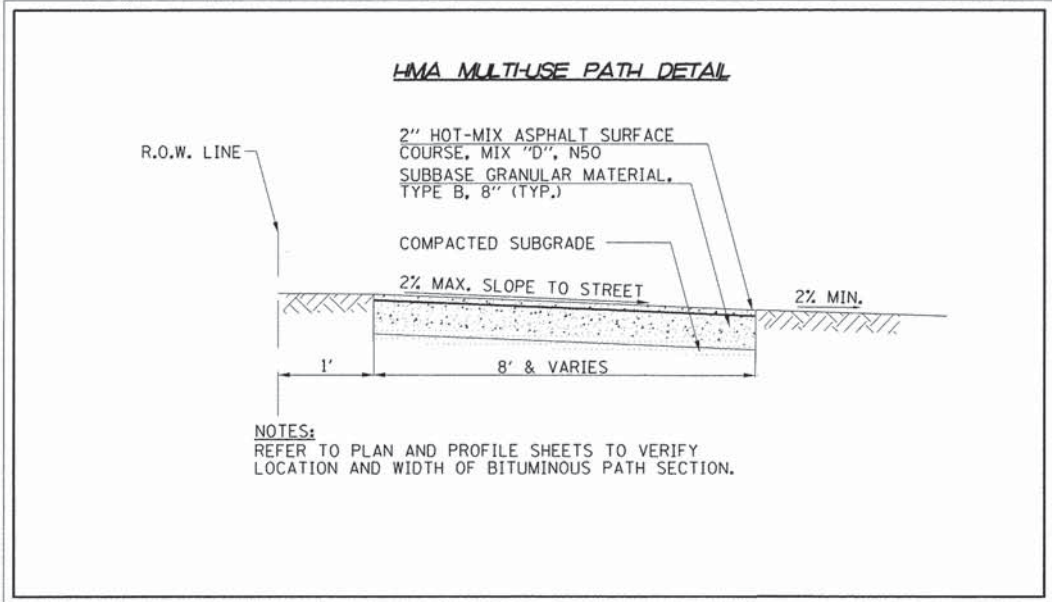
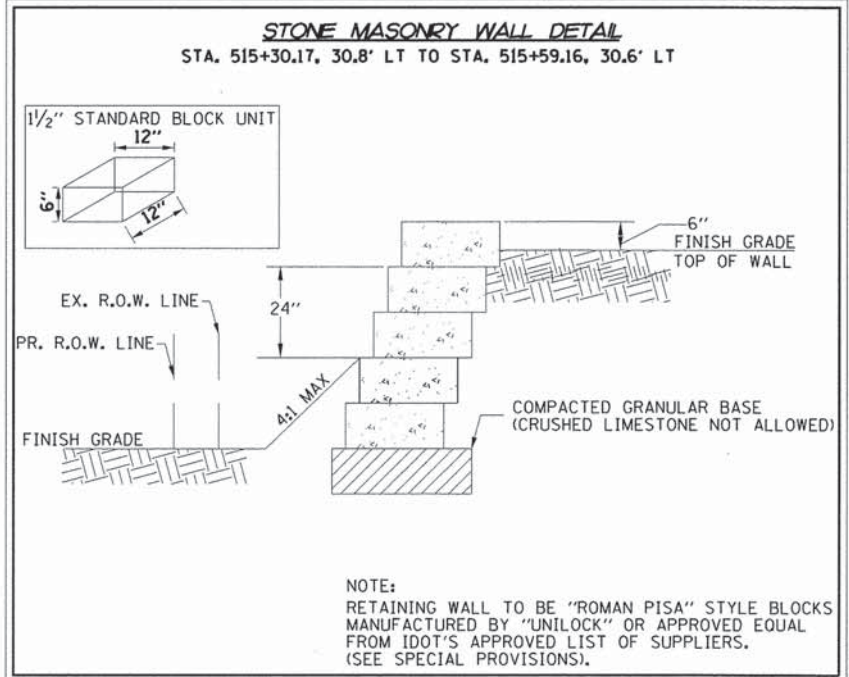
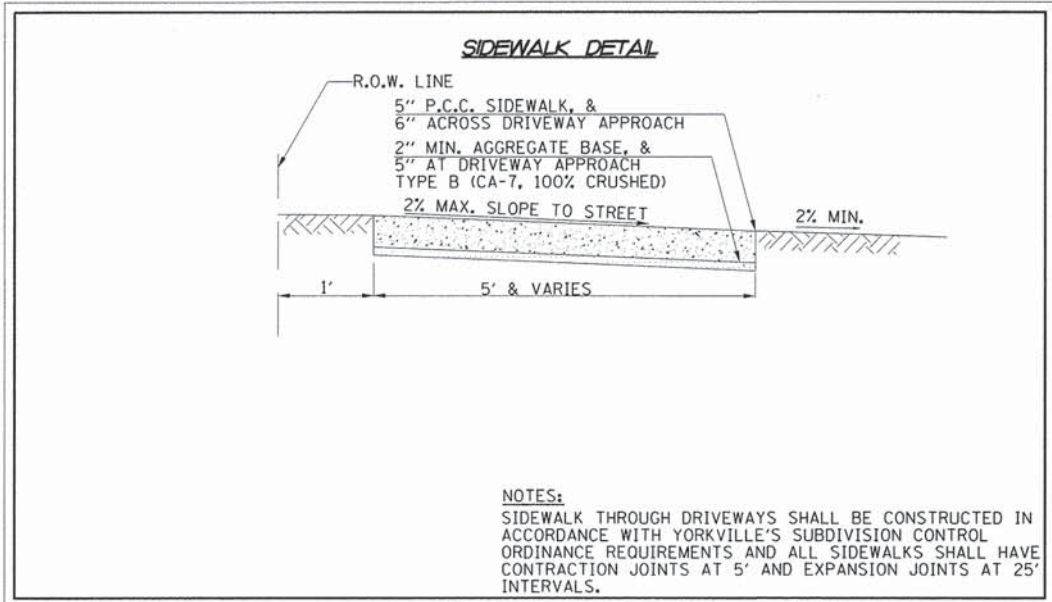
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DATE: JULY 2014 DRAWN BY: MJF
CHECKED BY: GT

COMPANY NAME: 8/21/2014 4:05:00 PM
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| | NO. | | |
| | CADD FILE NAME | | |

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| PROFILE | SURVEYED | BY | DATE |
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| | STRUCTURE NOTATIONS CHKD | | |

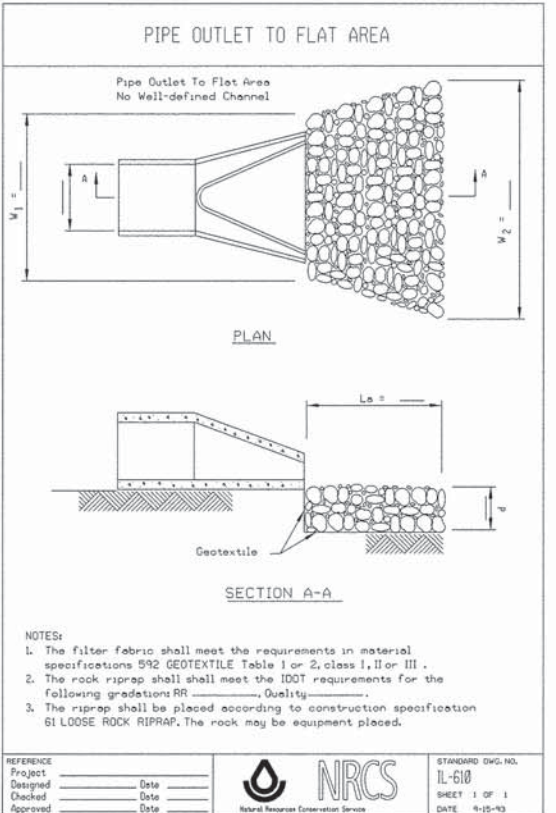
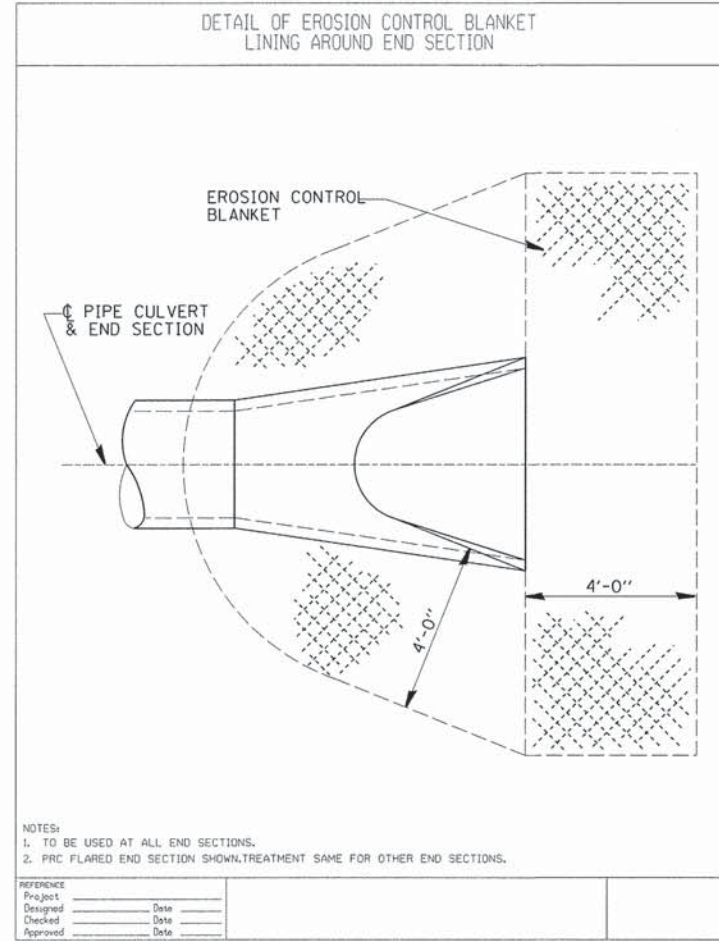
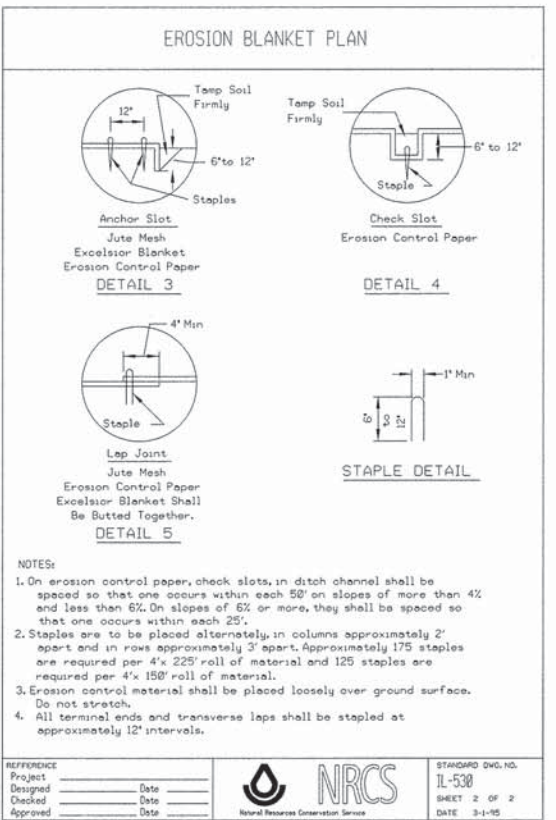
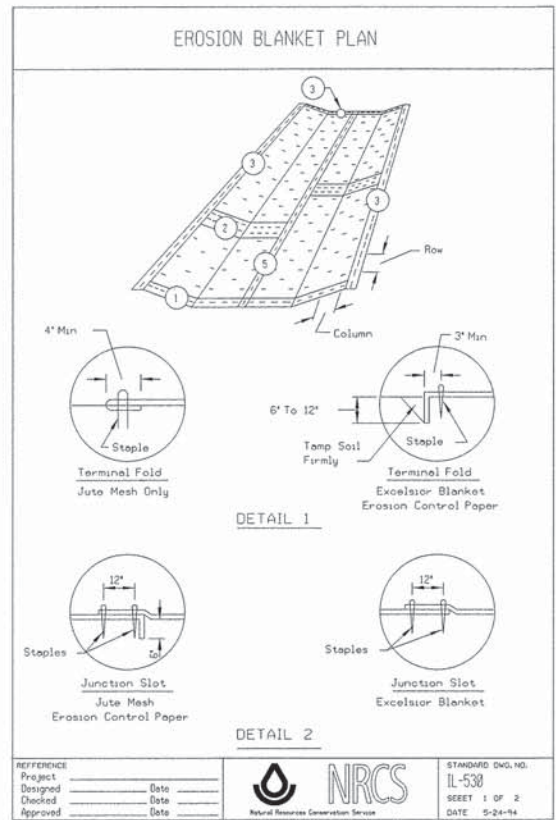
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| PROFILE | SURVEYED | BY | DATE |
| | PLOTTED | | |
| | GRADES CHECKED | | |
| | B.M. NOTED | | |
| | STRUCTURE NOTATIONS CKCD | | |

COMPANY NAME:
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RIPRAP QUANTITY & SIZING CHART

| Flared End Section | Pipe Dia. | Rip-Rap | La | Thickness (d) | Bedding Thickness (d) | W1 | W2 | TOTAL |
|--------------------|-----------|---------|------|---------------|-----------------------|------|-------|----------|
| Station | (in) | Grade | (ft) | (in) | (in) | (ft) | (ft) | (sq. yd) |
| 1906+40 RT | 36 | RR-4 | 40 | 16 | 6 | 7.00 | 15.00 | 67 |
| 800+12 LT | 30 | RR-4 | 22 | 16 | 6 | 7.50 | 11.30 | 28 |

NOTE: ASSUMED MINIMUM TAILWATER CONDITIONS AND MAXIMUM VELOCITY CONDITIONS (10 FPS)

FILTER FABRIC QUANTITY

| Flared End Section | Pipe Dia. | Rip-Rap | La | Thickness (d) | Bedding Thickness (d) | W1 | W2 | FILTER FABRIC |
|--------------------|-----------|---------|------|---------------|-----------------------|------|-------|---------------|
| Station | (in) | Grade | (ft) | (in) | (in) | (ft) | (ft) | (sq. yd) |
| 1906+40 RT | 36 | RR-4 | 40 | 16 | 6 | 7.00 | 15.00 | 67 |
| 800+12 LT | 30 | RR-4 | 22 | 20 | 6 | 7.50 | 11.30 | 28 |

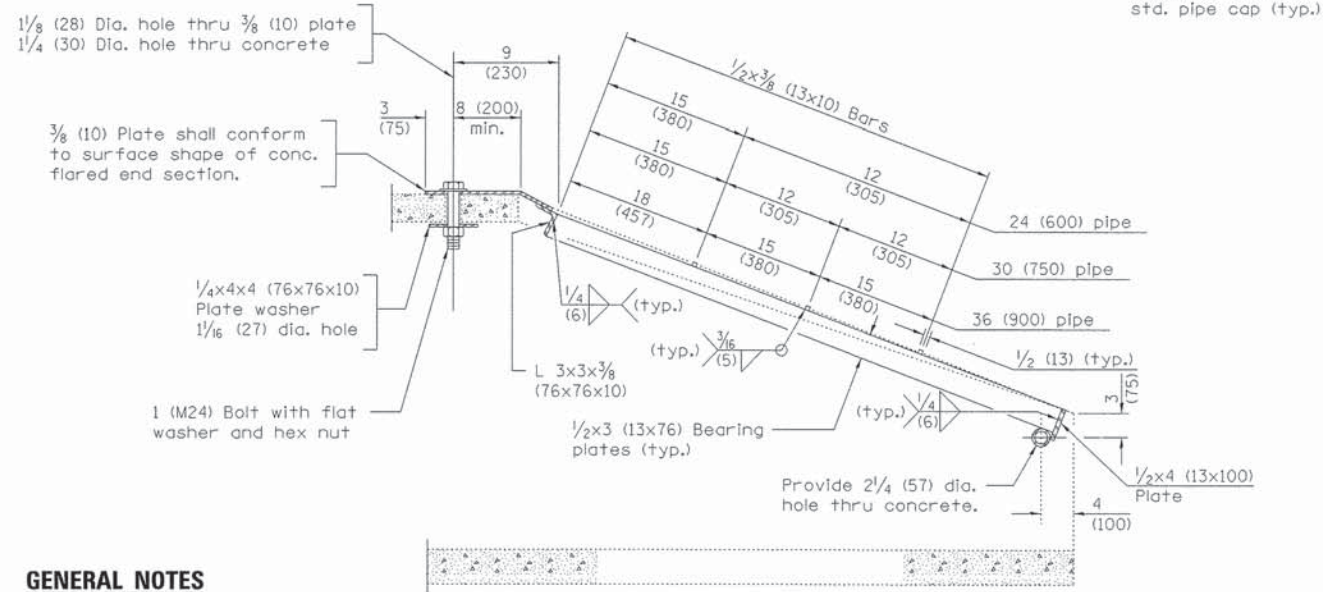
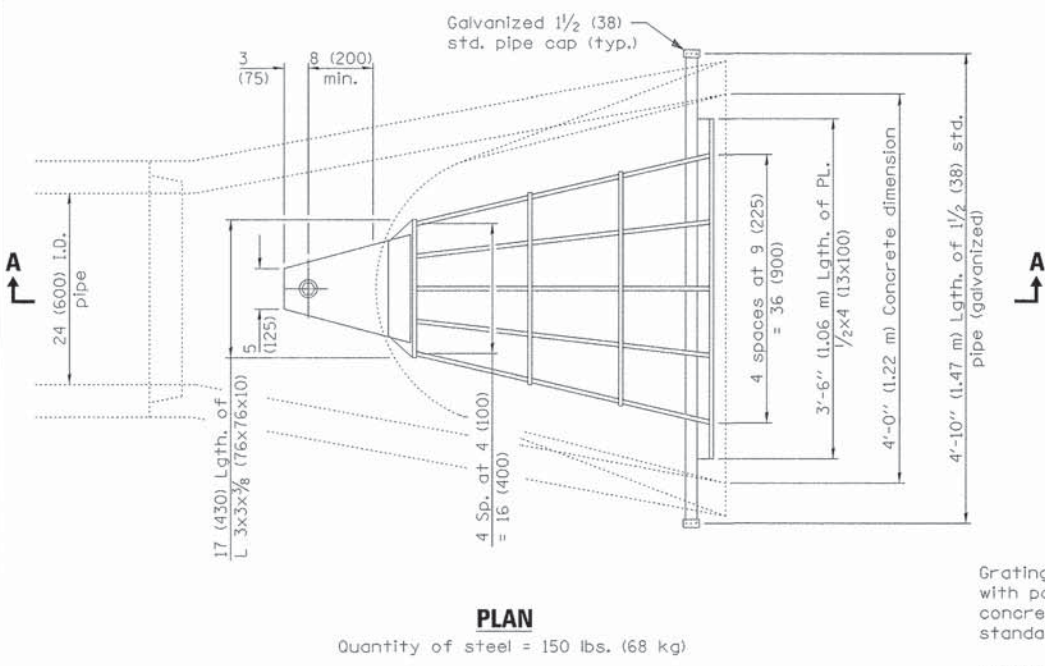
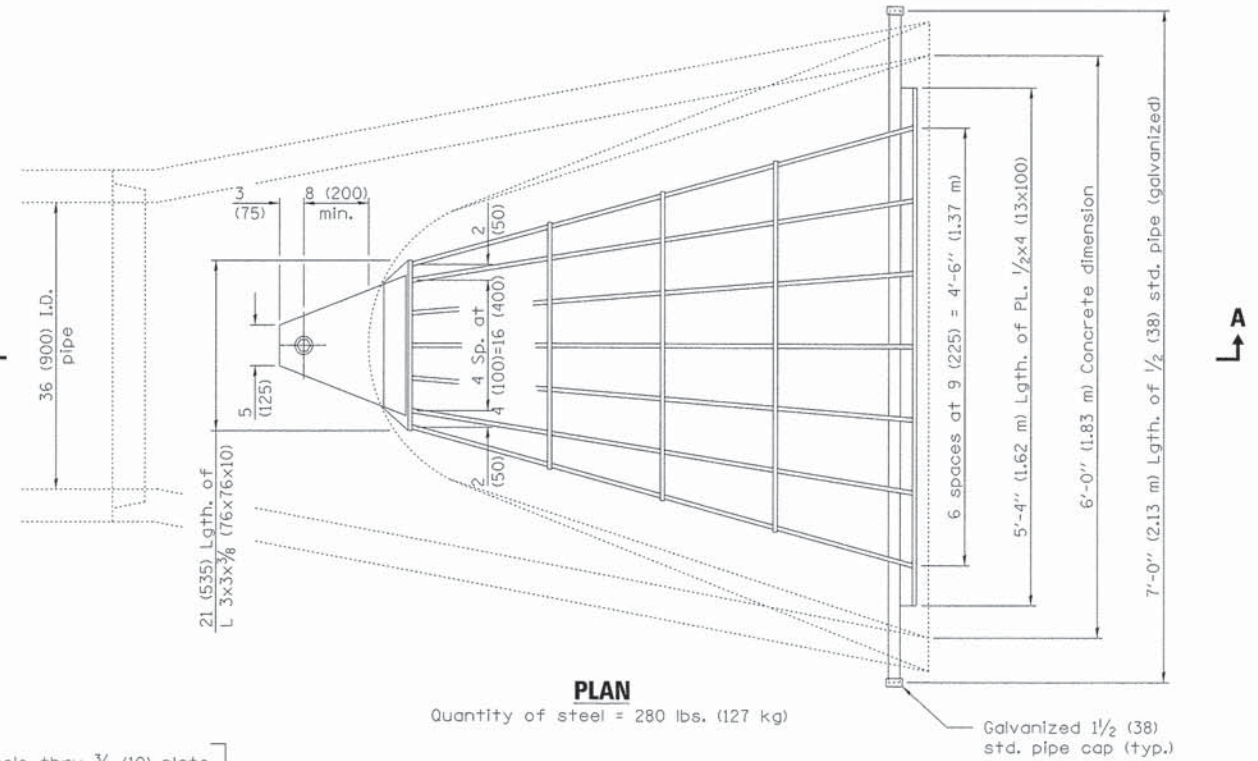
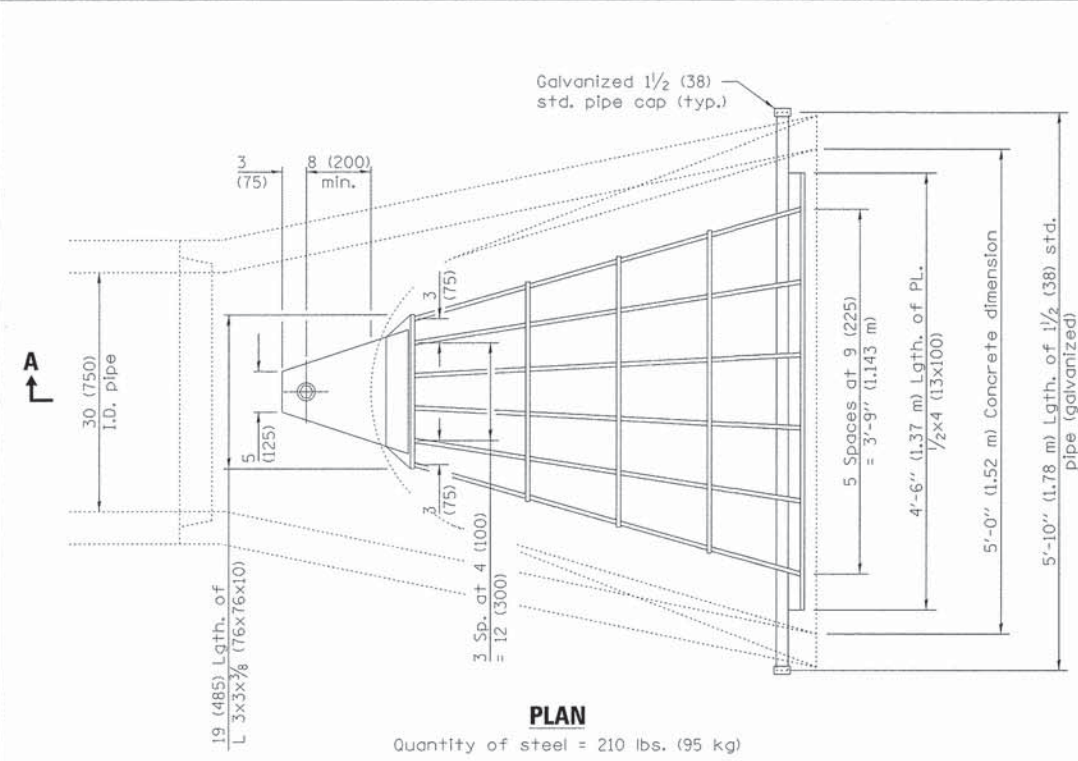
ILLINOIS DEPARTMENT OF TRANSPORTATION
SOMONAUK STREET / GAME FARM ROAD
RECONSTRUCTION
MISCELLANEOUS DETAILS

SCALE: VERT. _____
HORIZ. _____
DATE: JULY 2014

DRAWN BY: MJF
CHECKED BY: GT

PLAN SURVEYED BY DATE
 PLOTTED BY
 NOTE BOOK ALIGNMENT CHECKED RT. OF WAY CHECKED
 NO. CADD FILE NAME

PROFILE SURVEYED BY DATE
 PLOTTED BY
 NOTE BOOK GRADES CHECKED B.M. NOTED
 NO. STRUCTURE NOTATIONS CHKD



GENERAL NOTES

Grating details shown are intended for use with particular sizes of precast reinforced concrete flared end sections as shown on standards 542301 and 542306.

Approximate quantity of steel shown includes total quantity of grating, bolts, nuts, washers and steel pipe.

Holes in the precast concrete flared end sections shall be cored to the diameters noted. If cone-out on the other end of the hole occurs, the hole shall be filled with grout to correct the diameter of the hole.

SECTION A-A

All dimensions are in inches (millimeters) unless otherwise shown.

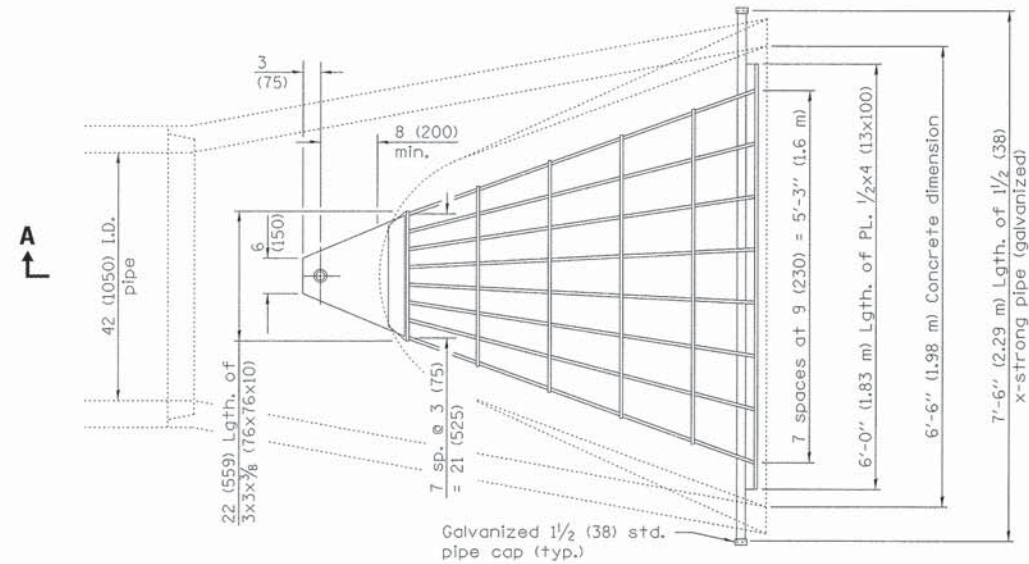
GRATING FOR CONCRETE FLARED END SECTION (FOR 24" (600 mm) THRU 54" (1350 mm) PIPE)

(Sheet 1 of 2)

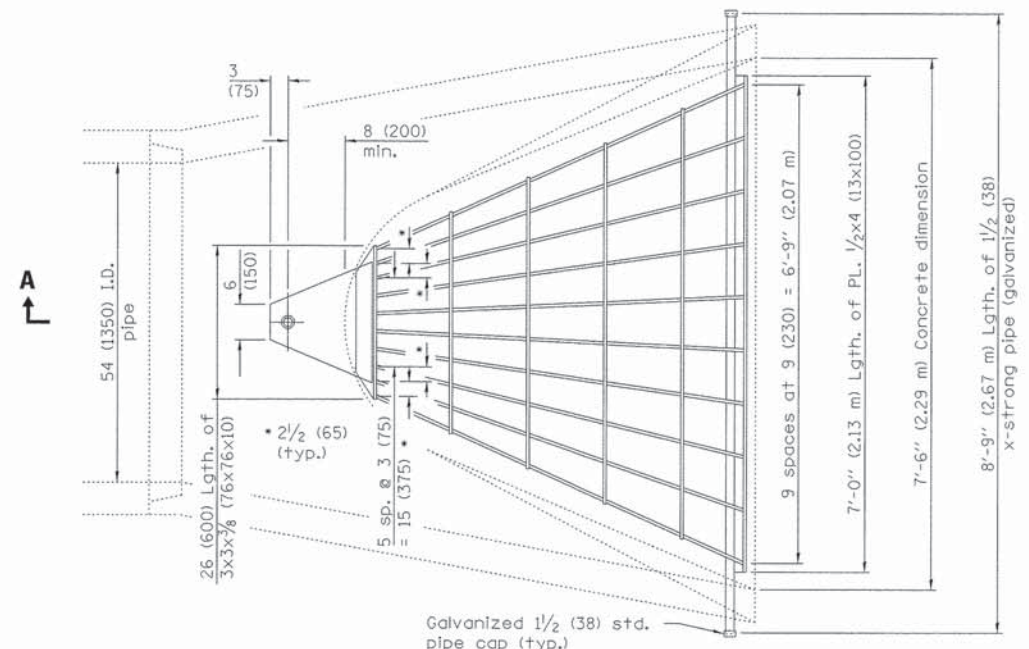
STANDARD 542311-03

PLAN SURVEYED BY DATE
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 NOTE BOOK ALIGNMENT CHECKED
 RT. OF WAY CHECKED
 NO. CADD FILE NAME

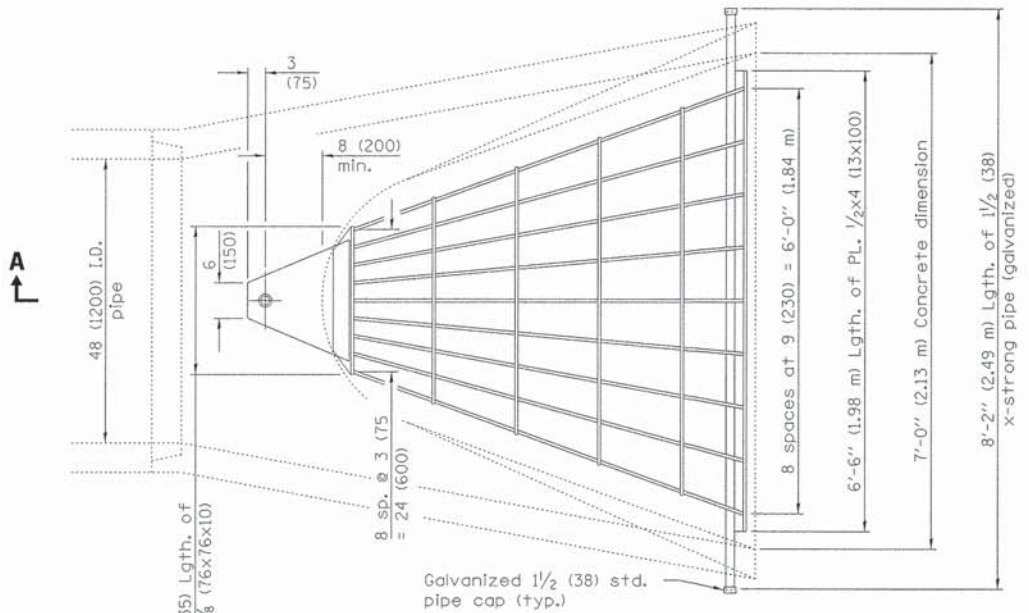
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 B.M. NOTED
 NO. STRUCTURE NOTATIONS CHKD



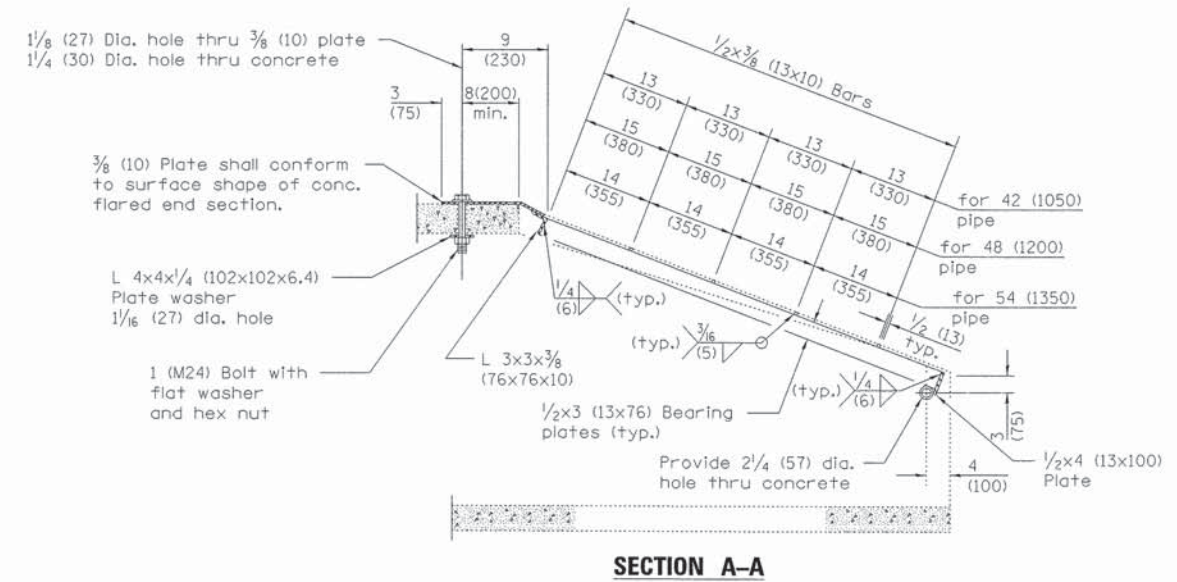
PLAN
 Quantity of steel = 320 lbs. (145 kg)



PLAN
 Quantity of steel = 425 lbs. (193 kg)



PLAN
 Quantity of steel = 400 lbs. (181 kg)

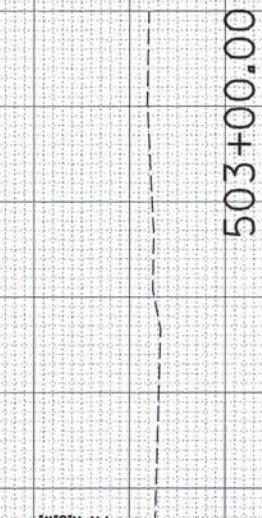


SECTION A-A

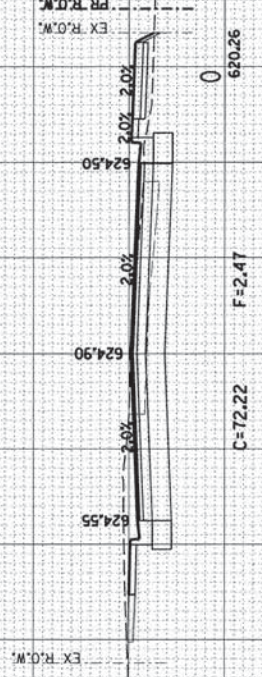
GRATING FOR CONCRETE FLARED END SECTION (FOR 24" (600 mm) THRU 54" (1350 mm) PIPE)
 (Sheet 2 of 2)
STANDARD 542311-03

| | | | | |
|---------------------|----------------|-------------------|--------------|-----------|
| CONTRACT NO. 87345 | | | | |
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1550 | 03-00031-00-FP | KENDALL | 111 | 85 |
| STA. 502+74.00 | | TO STA. 503+00.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

630
625
620



503+00.00

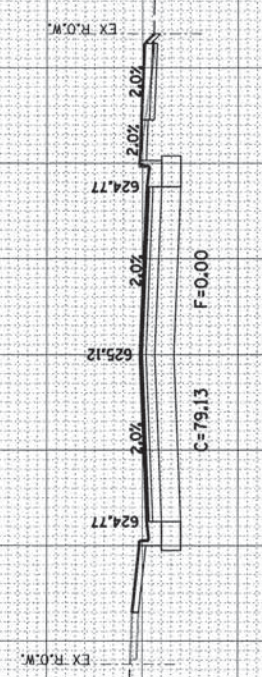


630
625
620

630
625
620



502+76.00



630
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620

630
625
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502+74.00

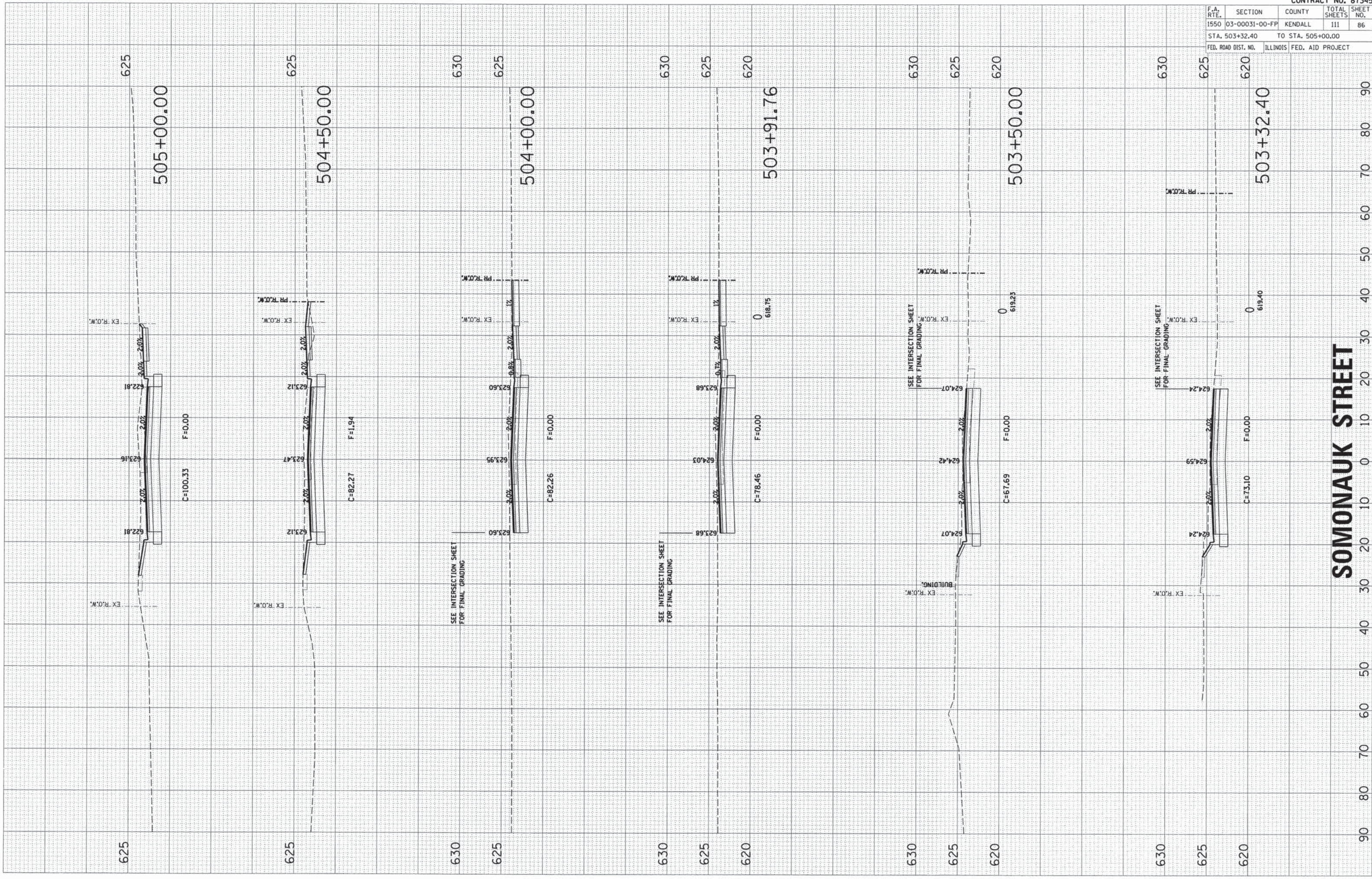


630
625
620

SOMONAUK STREET

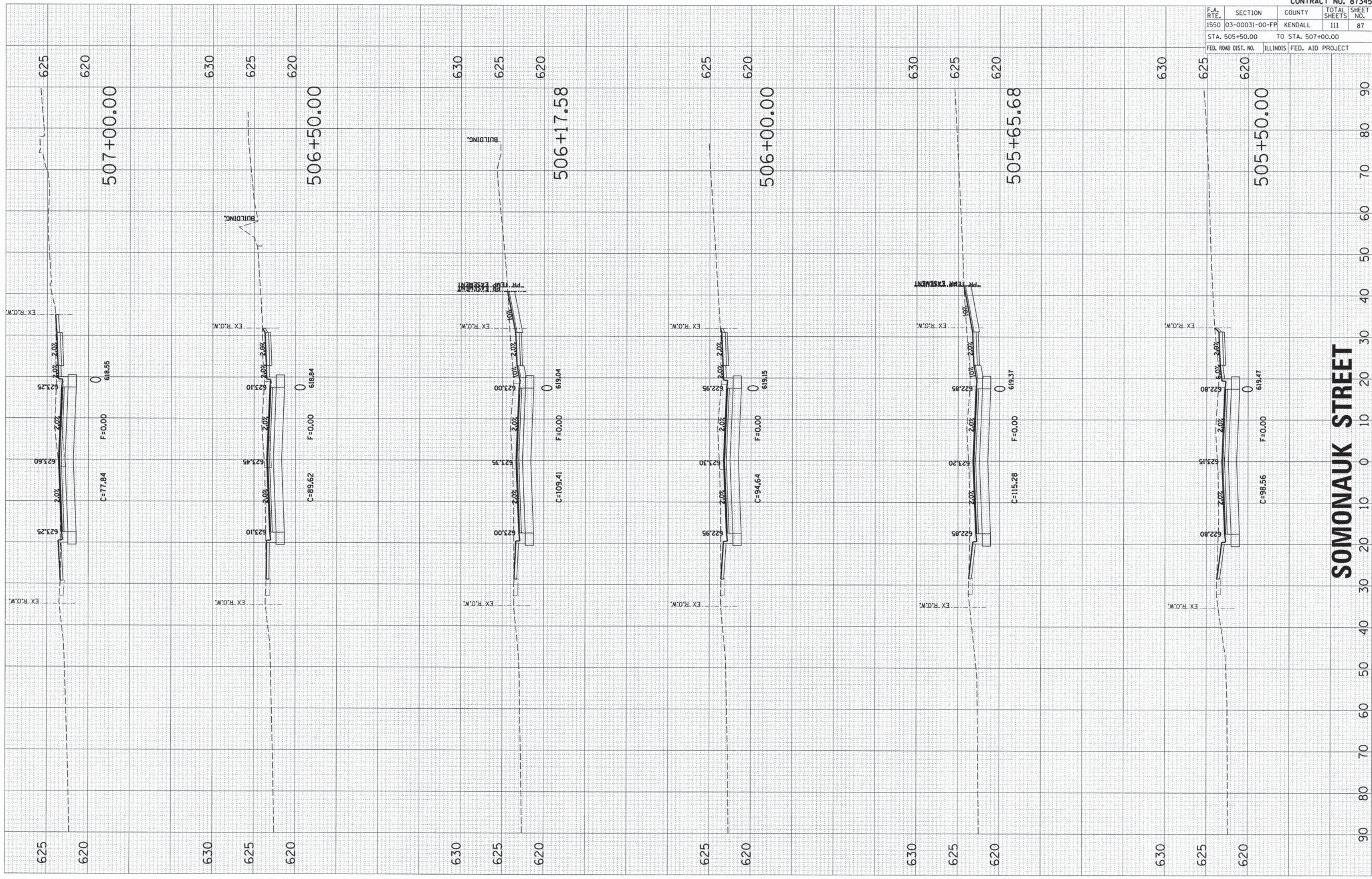
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| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
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| STA. 503+32.40 | | TO STA. 505+00.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



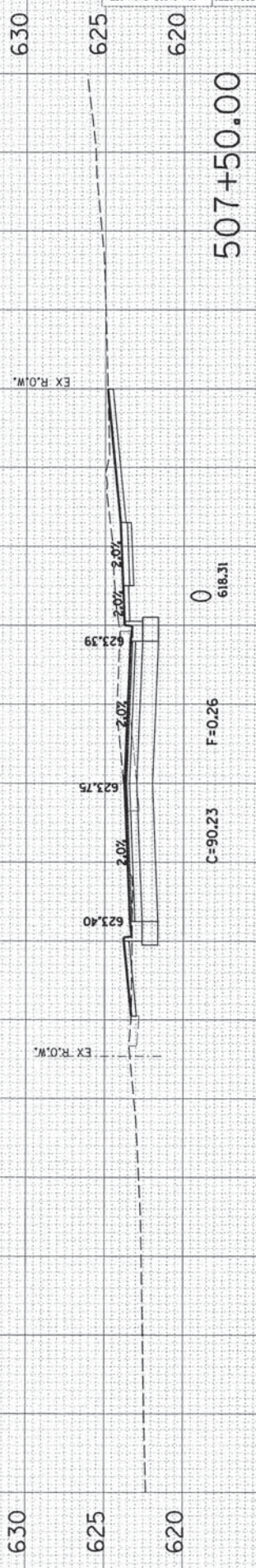
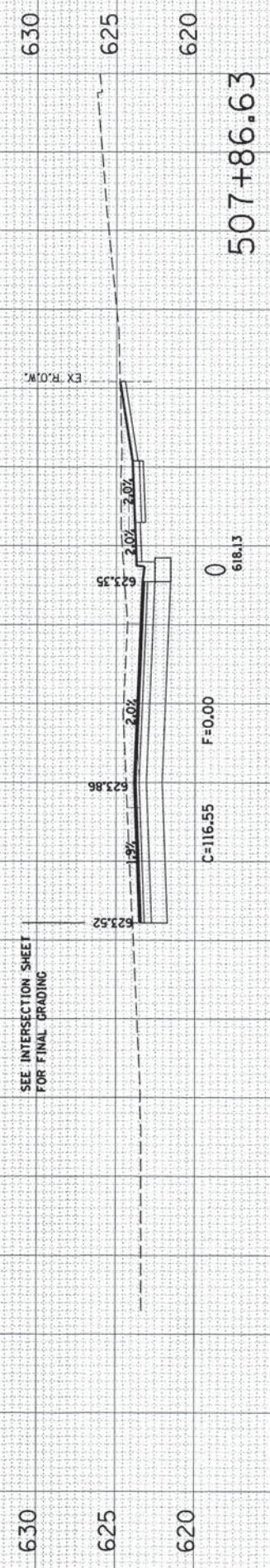
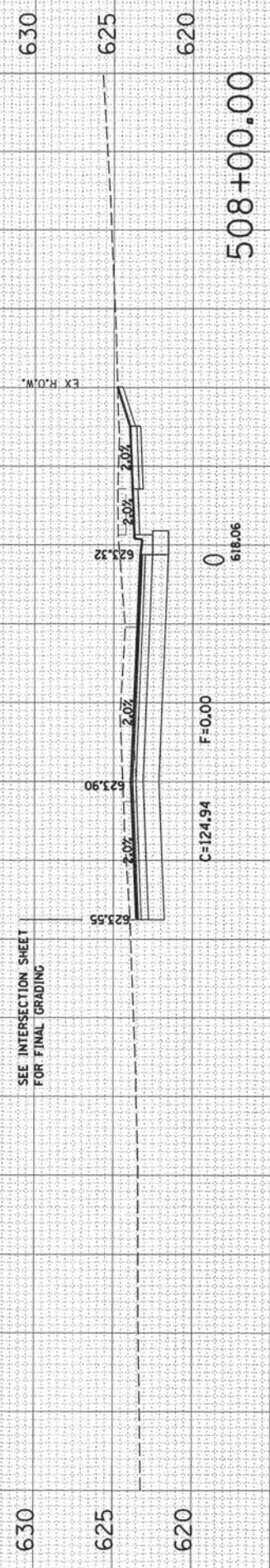
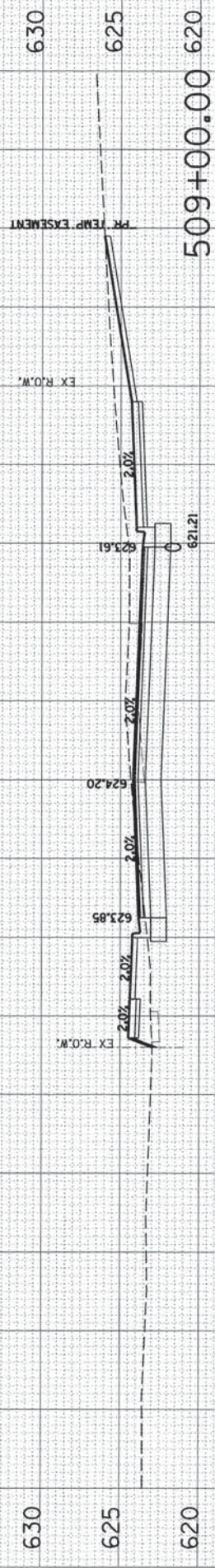
SOMONAUK STREET

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 87 |
| STA. 505+50.00 | | TO STA. 507+00.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

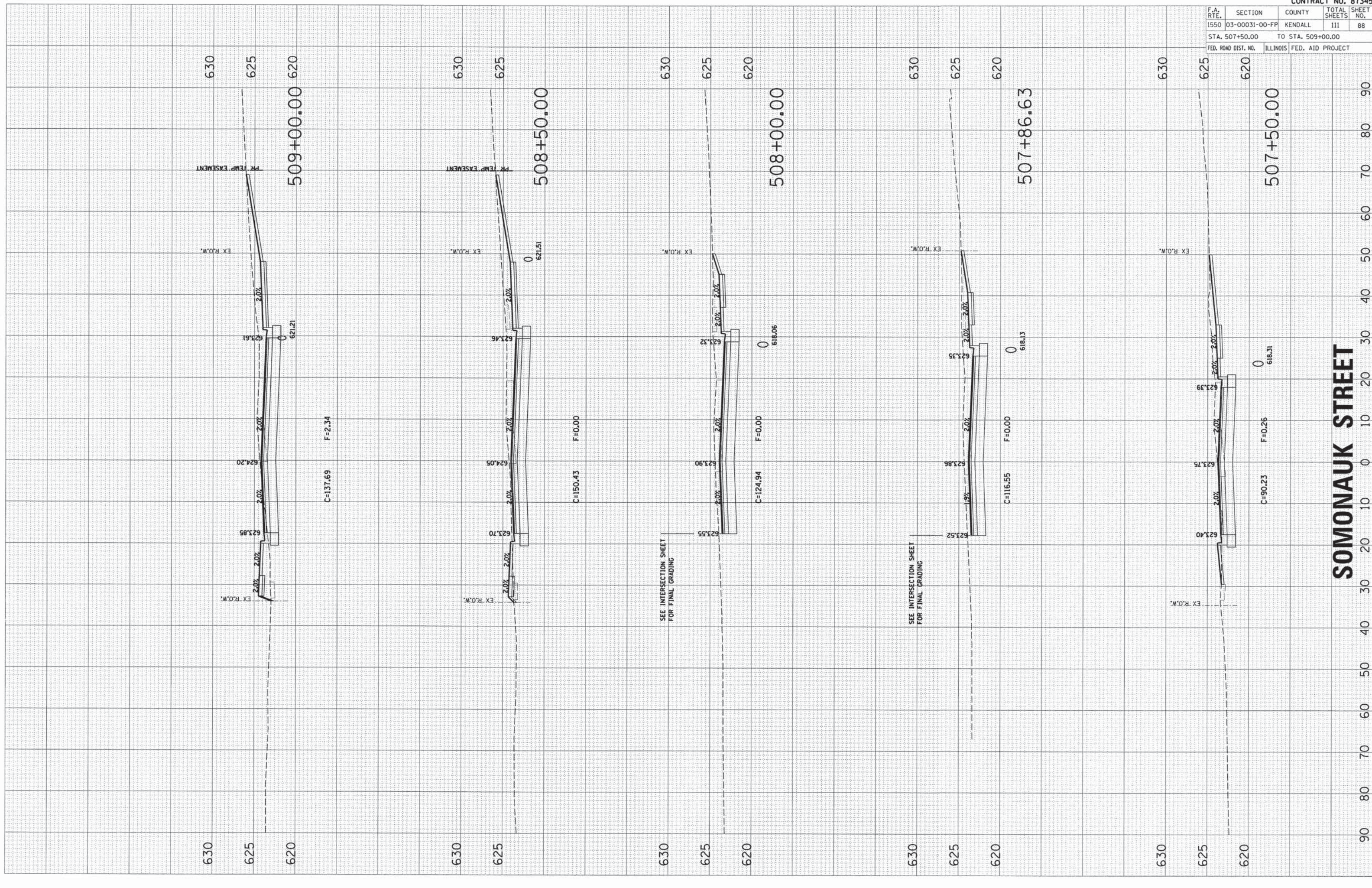


SOMONAUK STREET

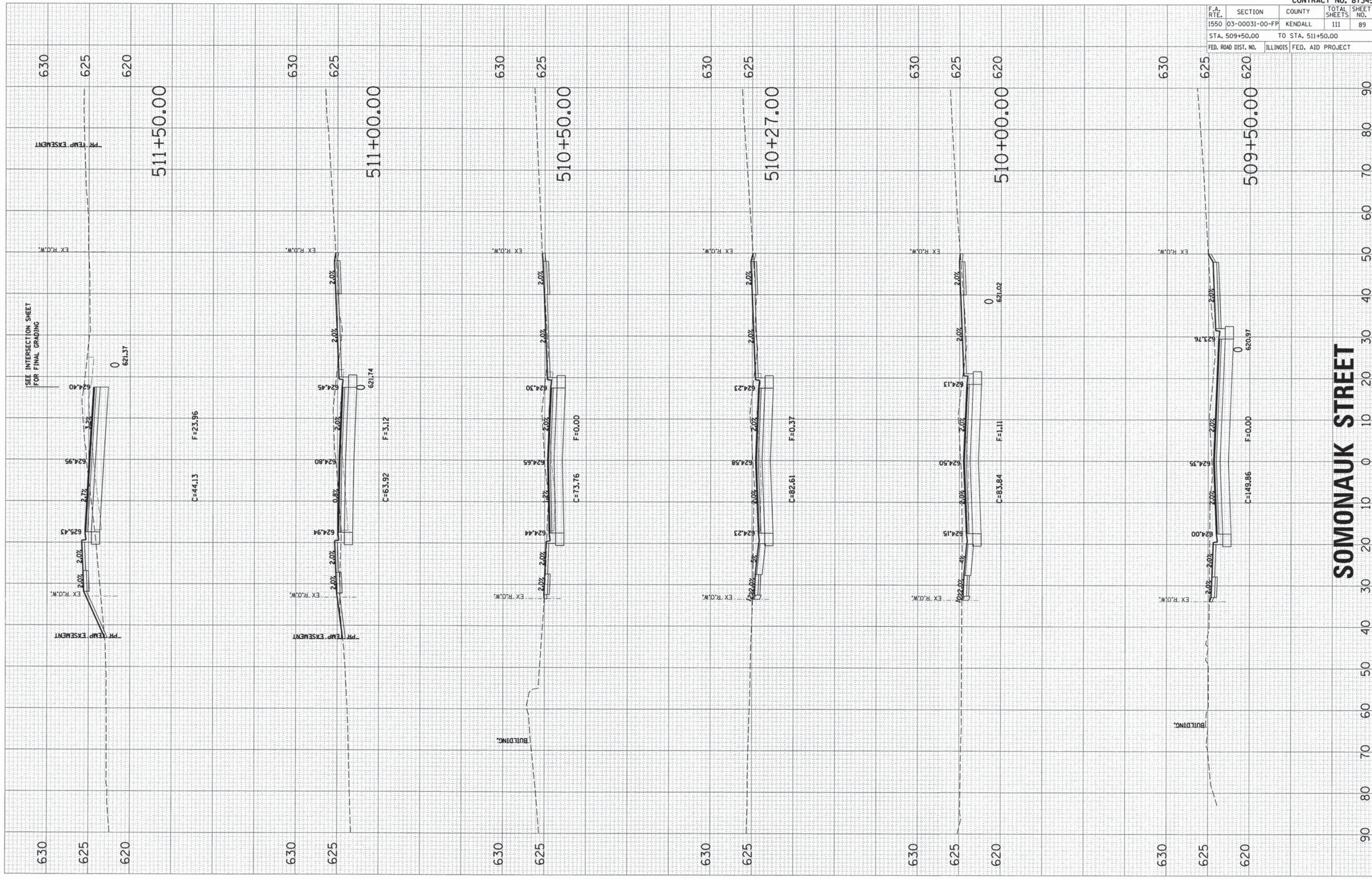
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FF | KENDALL | 111 | 88 |
| STA. 507+50.00 | | TO STA. 509+00.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SOMONAUK STREET

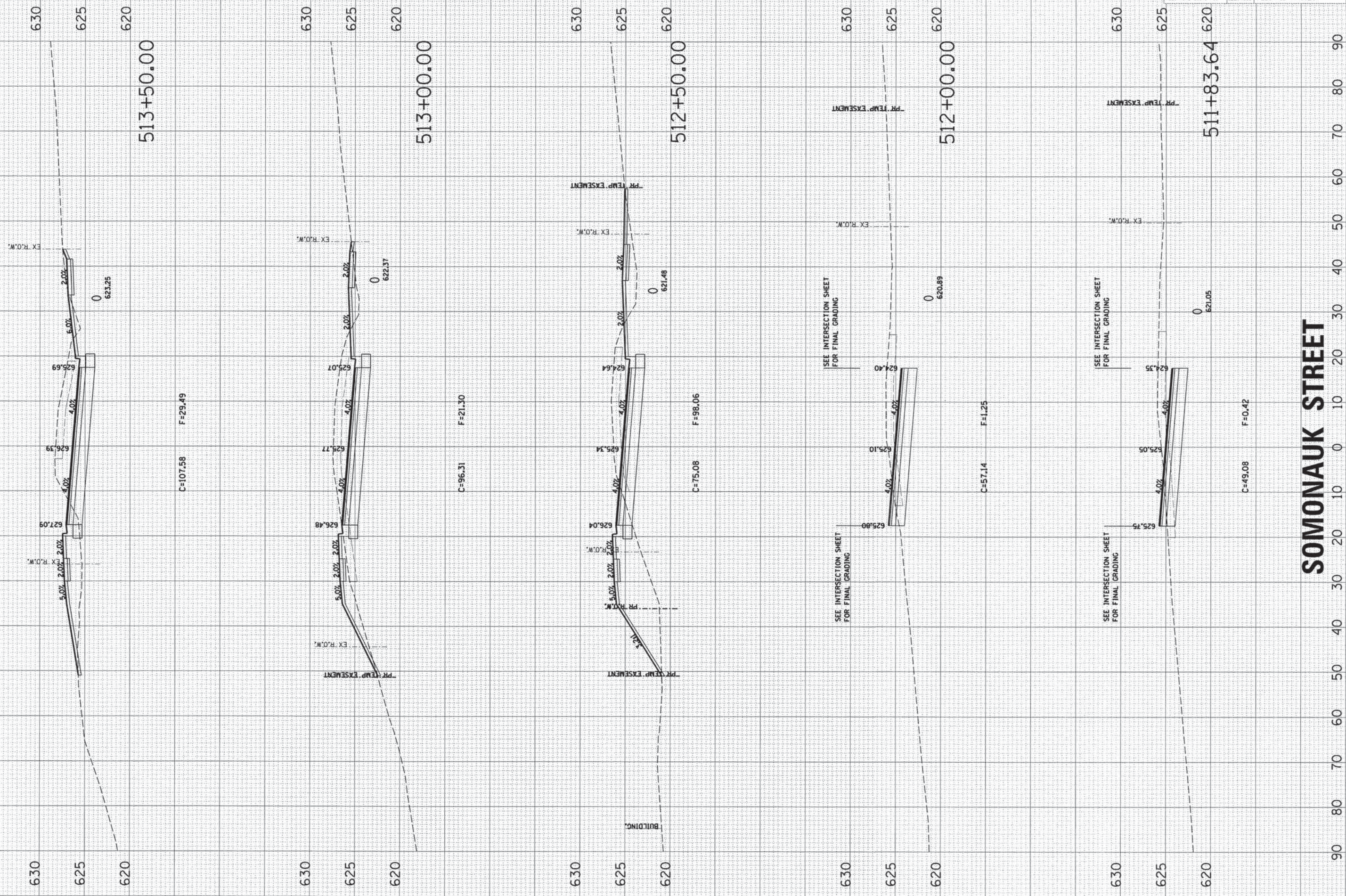


| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 89 |
| STA. 509+50.00 | | TO STA. 511+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SOMONAUK STREET

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 90 |
| STA. 511+83.64 | | TO STA. 513+50.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



SOMONAUK STREET

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 91 |
| STA. 513+88.00 | | TO STA. 515+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

635

630

625

515+00.00



C=132.16 F=0.00

635

630

625

514+85.40



C=136.56 F=0.49

635

630

625

514+50.00



C=116.62 F=6.11

635

630

625

514+00.00



C=105.43 F=1.86

635

630

625

513+88.00



C=103.95 F=1.68

GAME FARM ROAD

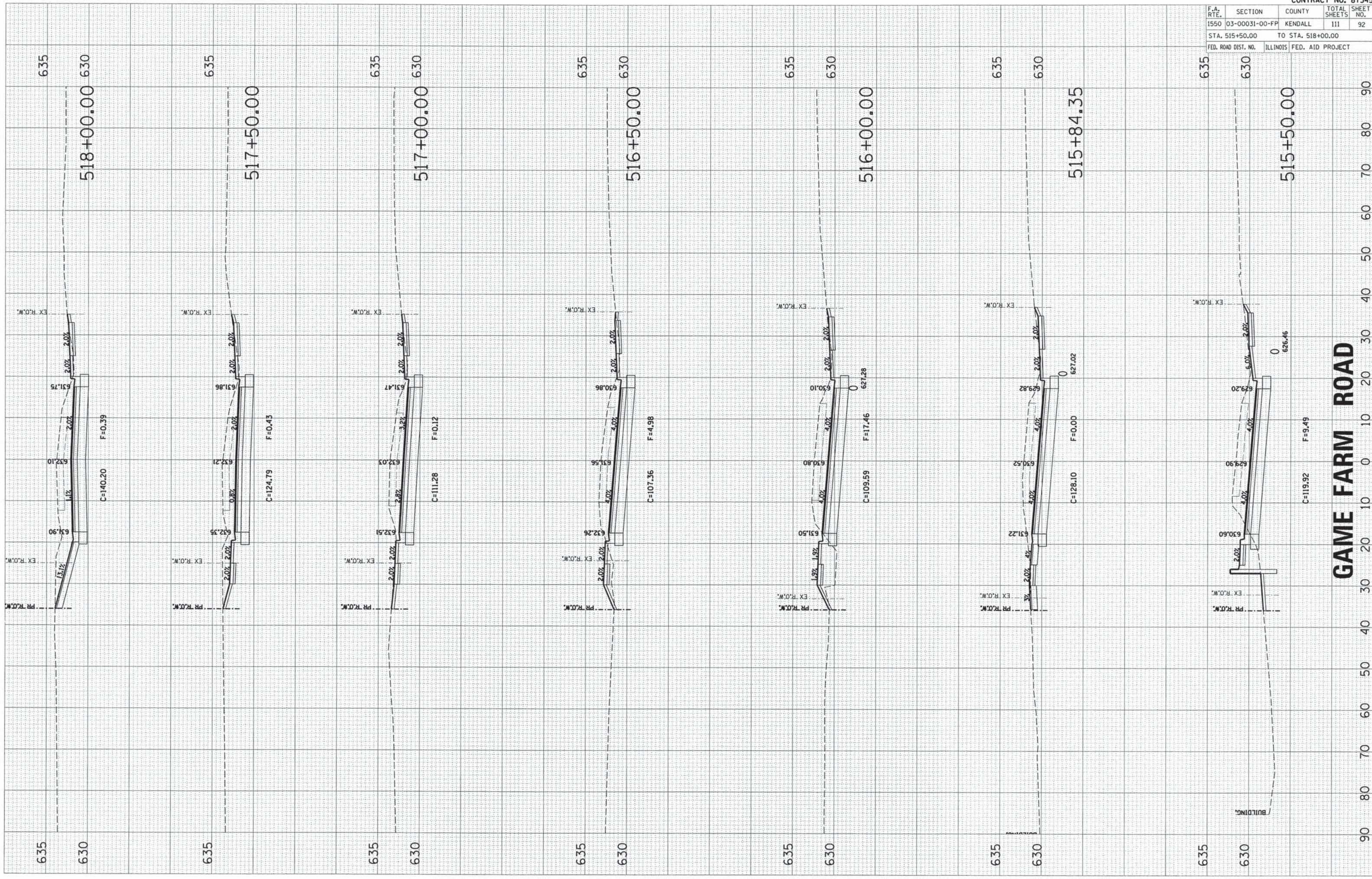
635

630

625

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 92 |
| STA. 515+50.00 | | TO STA. 518+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



GAME FARM ROAD

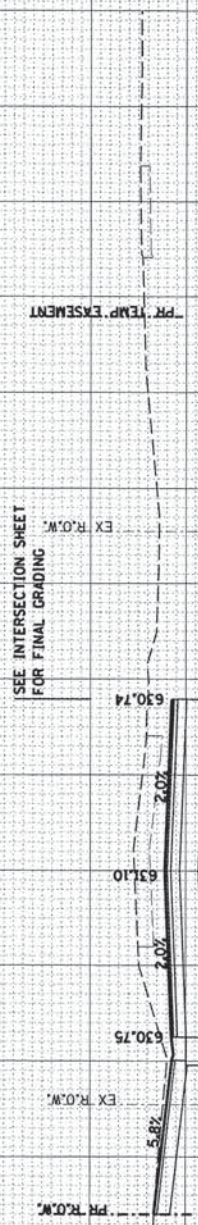
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 93 |
| STA. 518+50.00 | | TO STA. 520+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

635
630



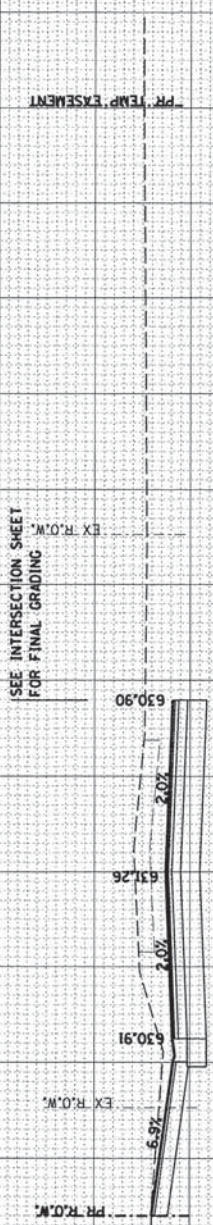
520+50.00

635
630



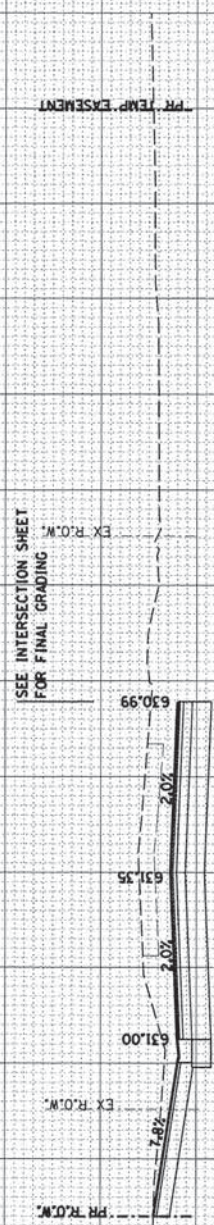
520+00.00

635
630



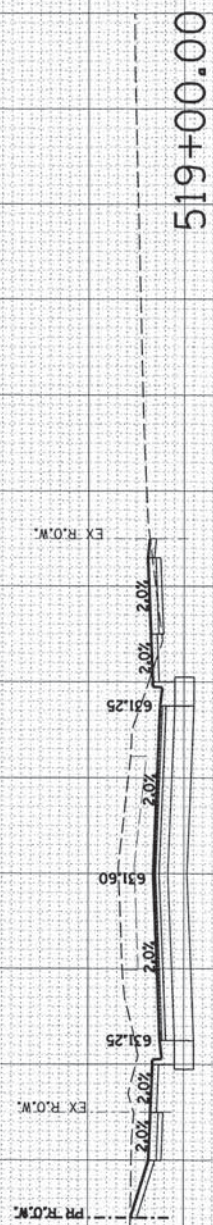
519+68.19

635
630



519+50.00

635
630



519+00.00

635
630

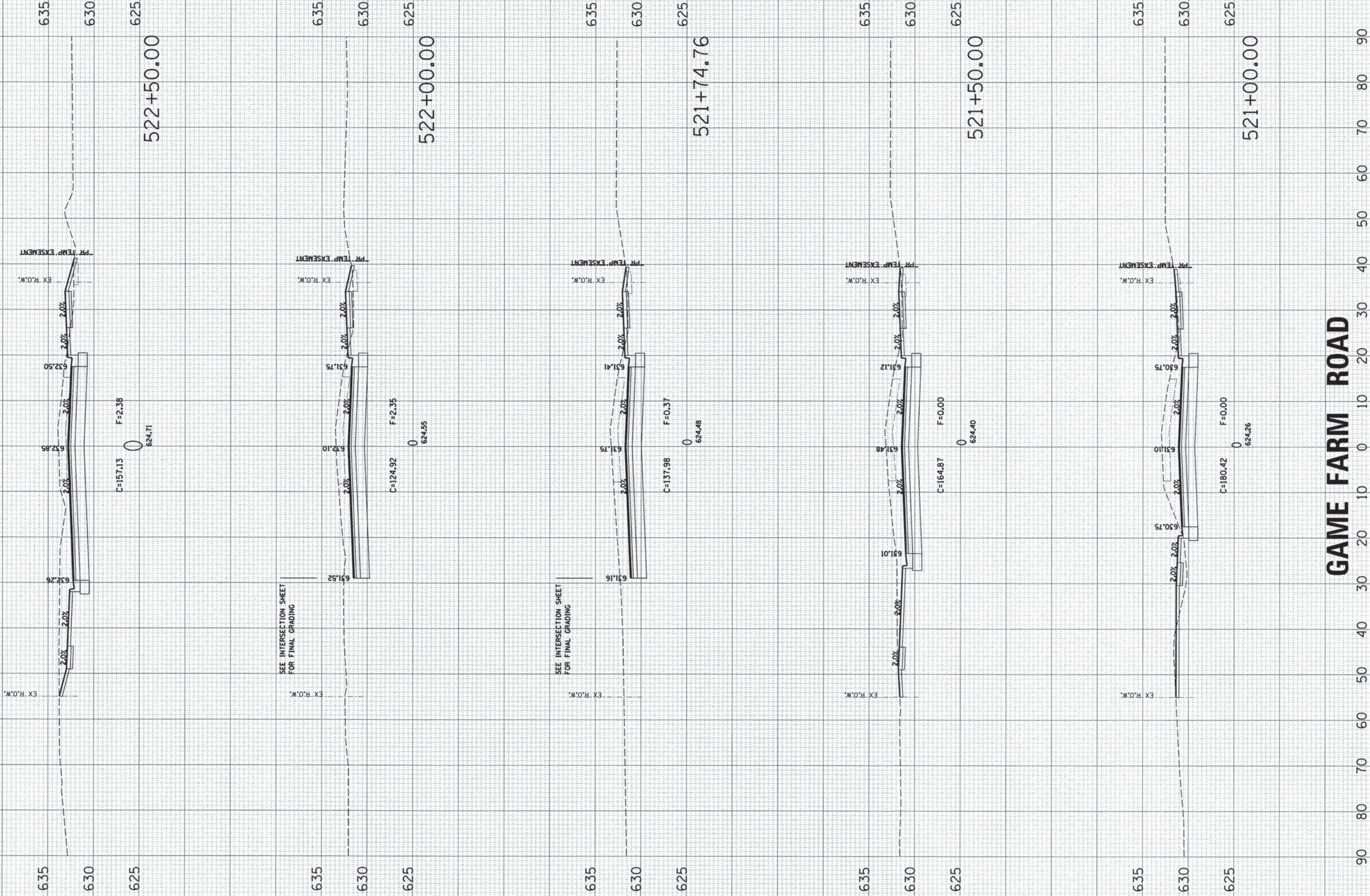


518+50.00

GAME FARM ROAD

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 94 |
| STA. 521+00.00 | | TO STA. 522+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

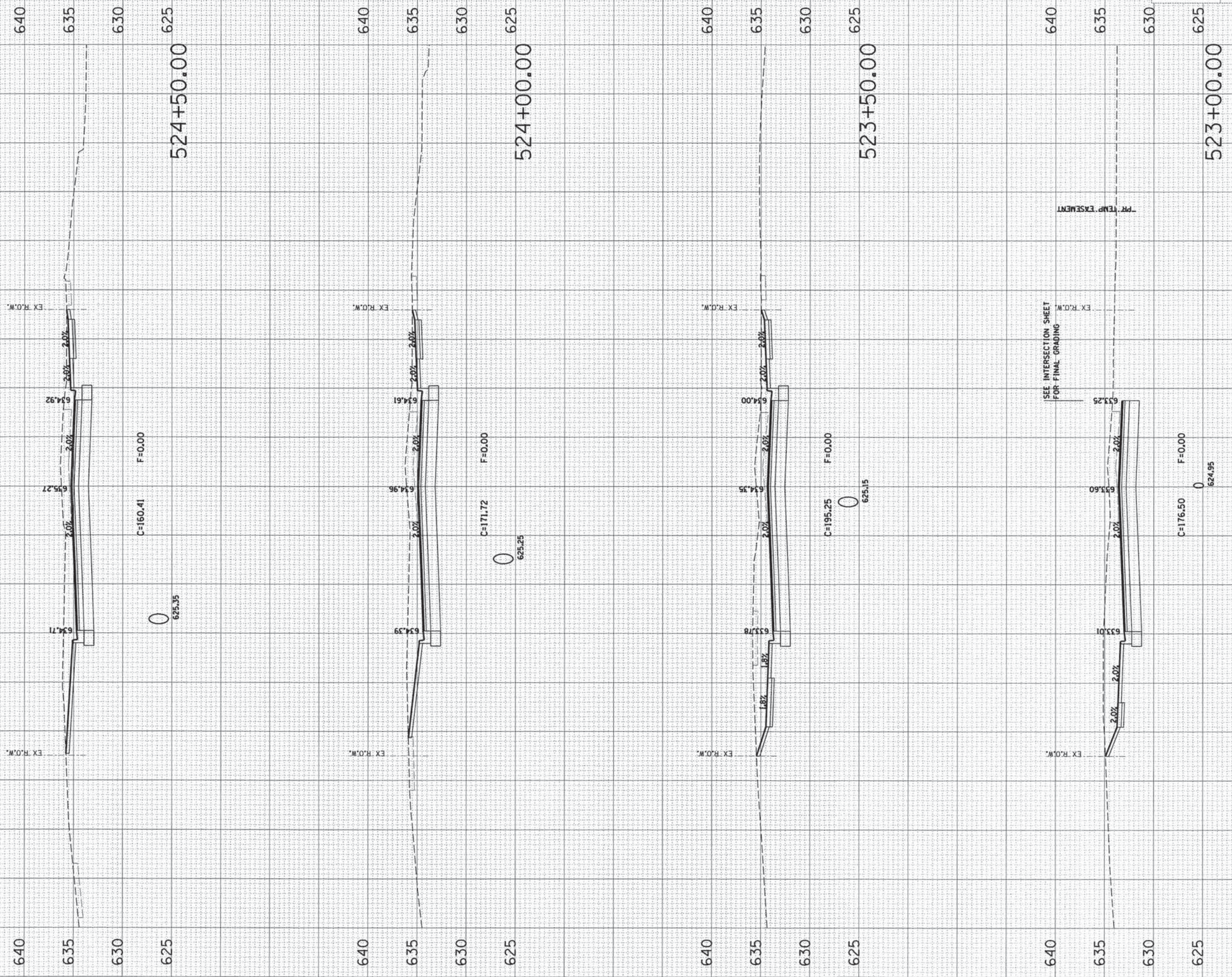


SEE INTERSECTION SHEET FOR FINAL GRADING

SEE INTERSECTION SHEET FOR FINAL GRADING

GAME FARM ROAD

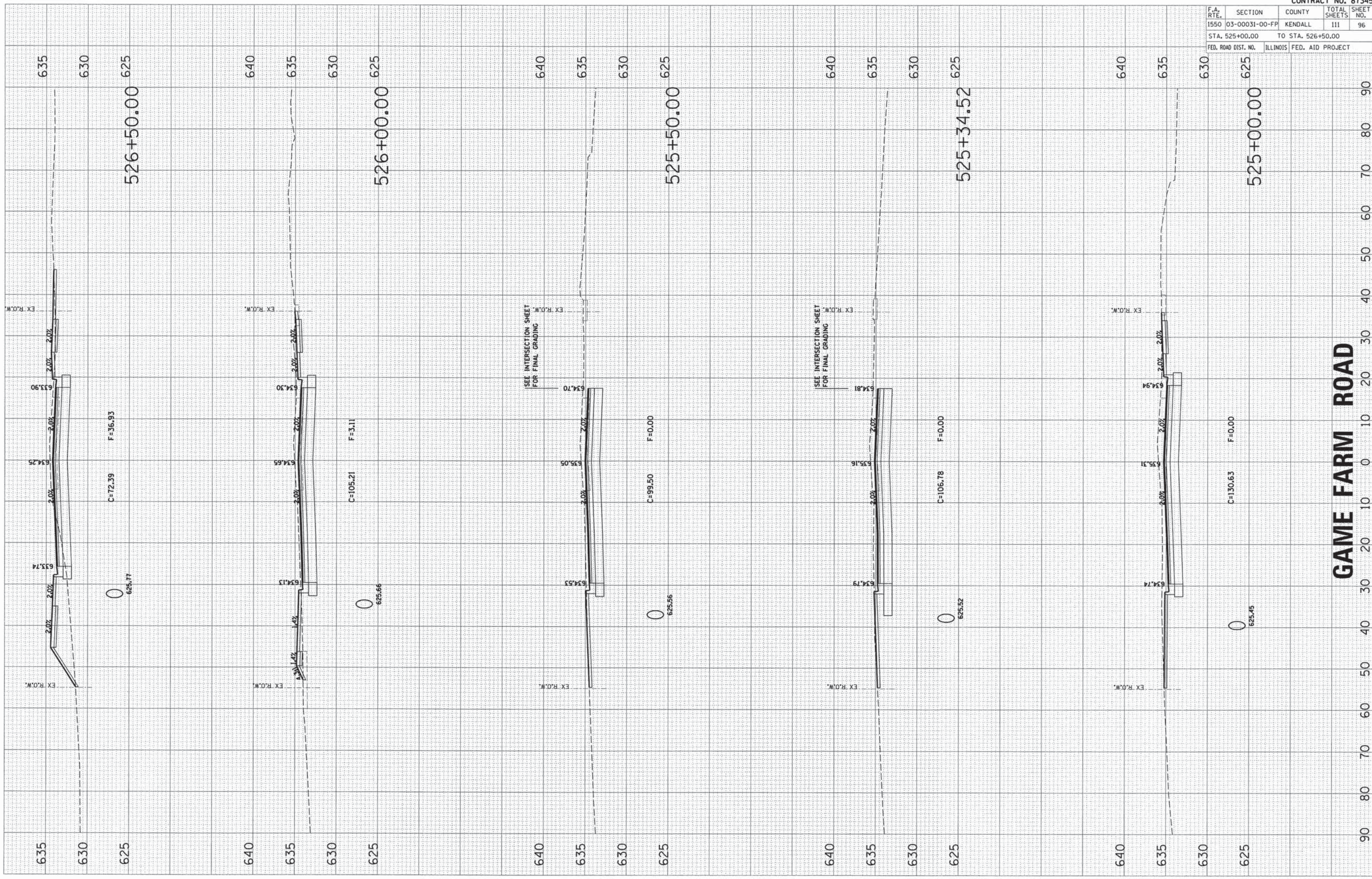
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 95 |
| STA. 523+00.00 | | TO STA. 524+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



GAME FARM ROAD

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 96 |
| STA. 525+00.00 | | TO STA. 526+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

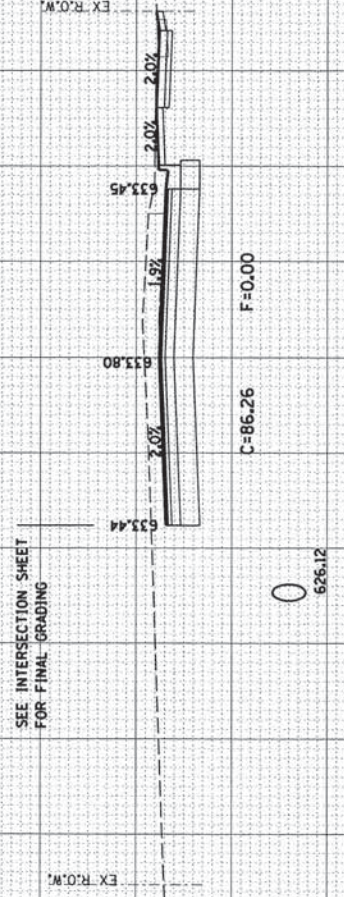


GAME FARM ROAD

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 97 |
| STA. 527+00.00 | | TO STA. 528+36.74 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

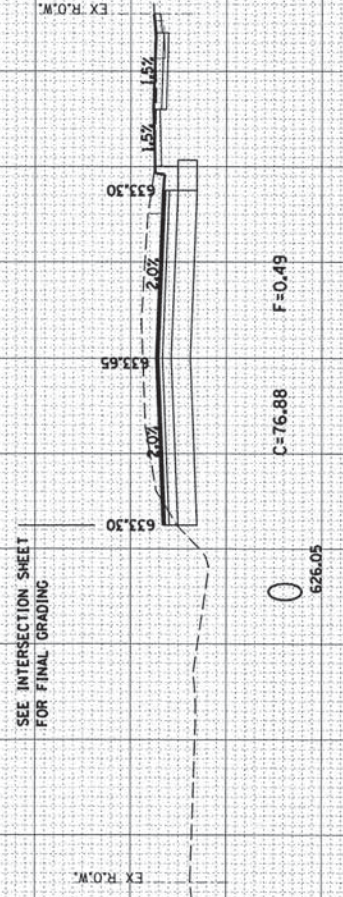
640
635
630
625

528+36.74



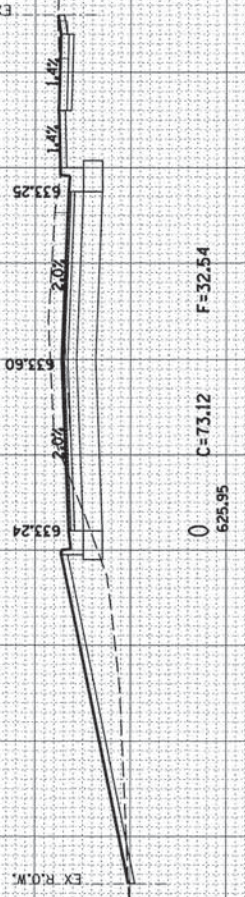
640
635
630
625

528+00.00



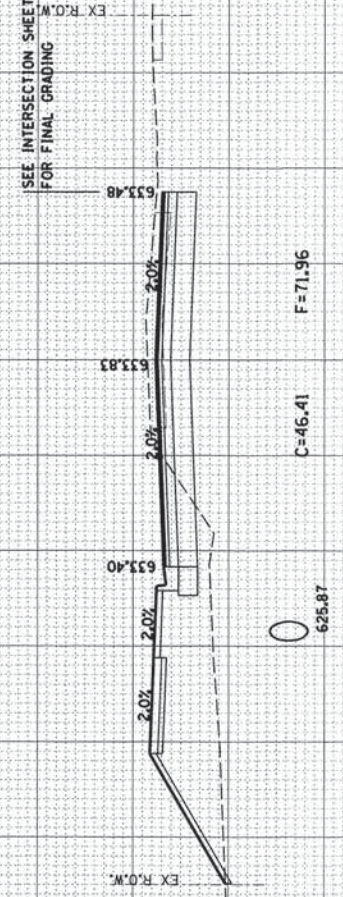
635
630
625

527+50.00



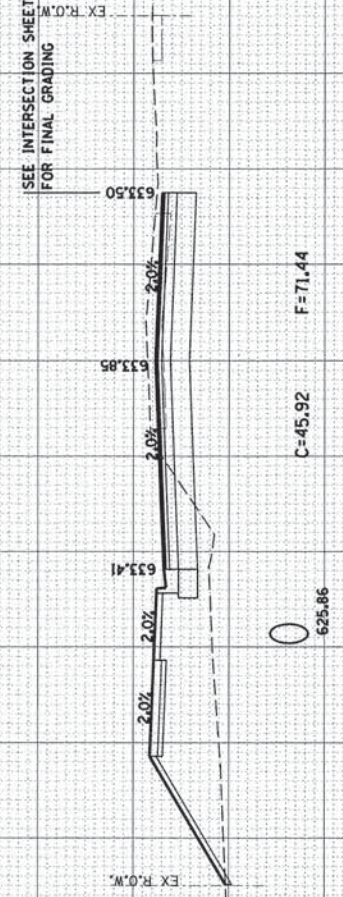
640
635
630
625

527+02.27



640
635
630
625

527+00.00



GAME FARM ROAD

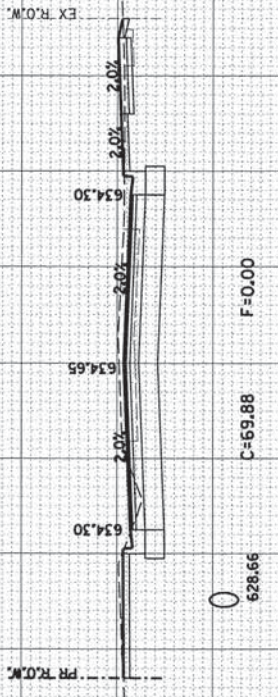
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 98 |
| STA. 528+50.00 | | TO STA. 530+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

640

635

630

530+50.00

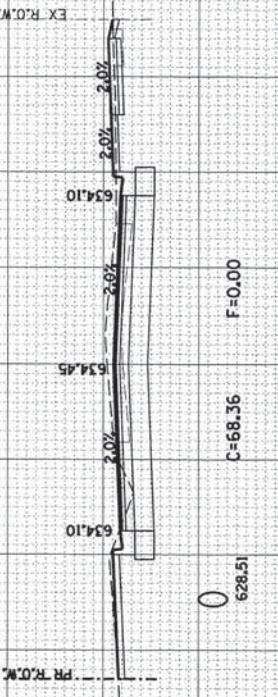


640

635

630

530+00.00

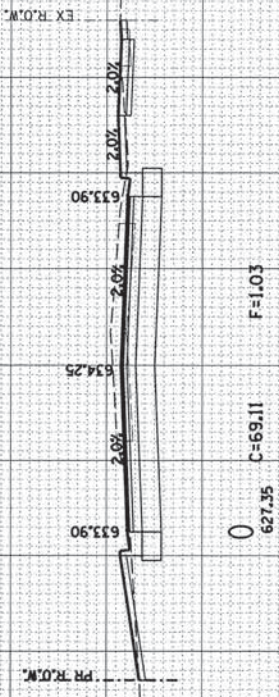


640

635

630

529+50.00

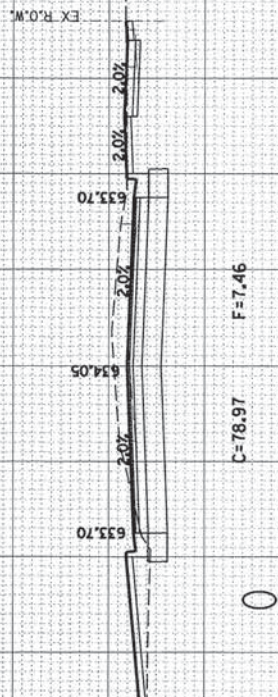


640

635

630

529+00.00



640

635

630

528+50.00



640

635

630

640

635

630

640

635

630

640

635

630

640

635

630

625

GAME FARM ROAD

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

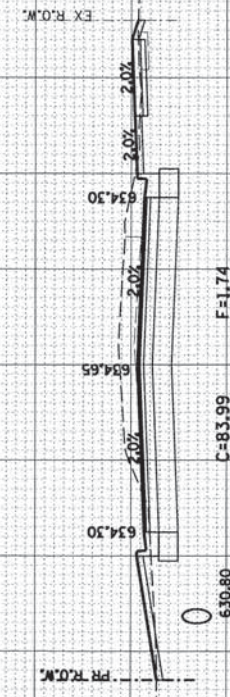
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 99 |
| STA. 531+00.00 | | TO STA. 532+50.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |

640

635

630

532+50.00



640

635

630

532+00.00

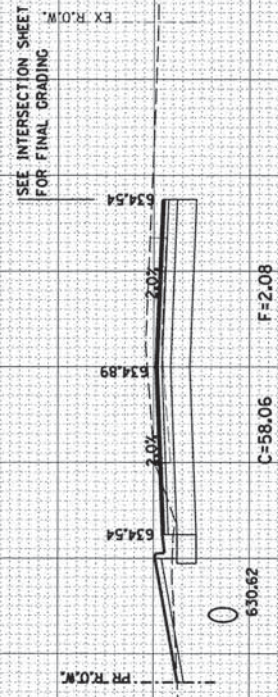


640

635

630

531+88.12

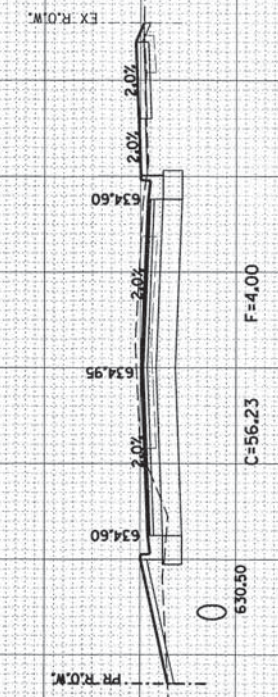


640

635

630

531+50.00

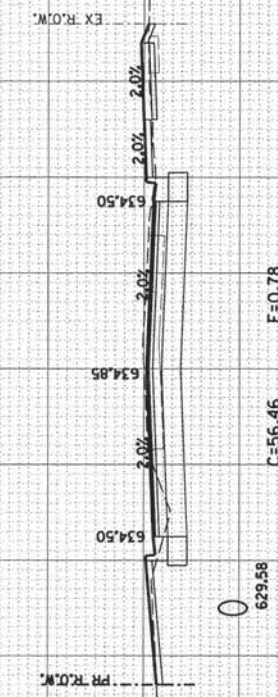


640

635

630

531+00.00



640

635

630

640

635

630

640

635

630

640

635

630

640

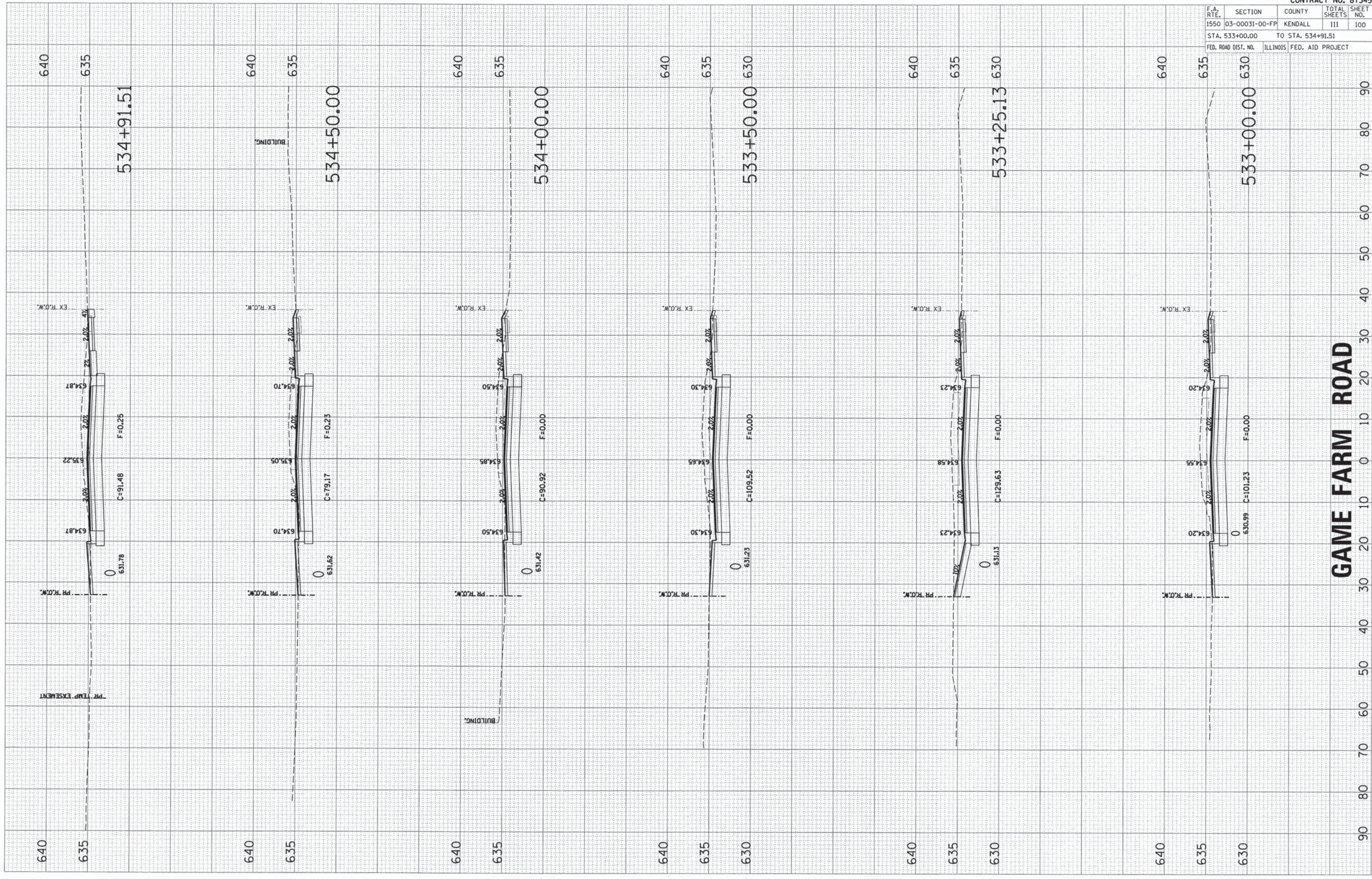
635

630

GAME FARM ROAD

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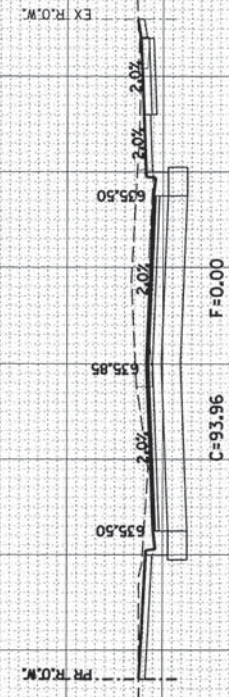
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 100 |
| STA. 533+00.00 | | TO STA. 534+91.51 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



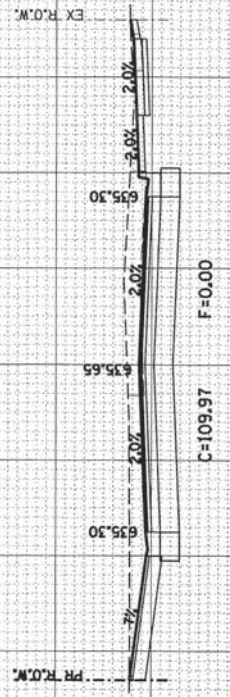
GAME FARM ROAD

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 101 |
| STA. 535+00.00 | | TO STA. 536+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

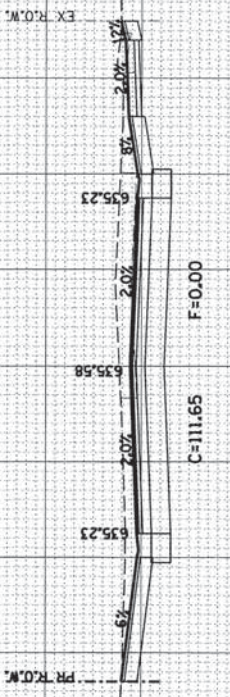
640 635
BUILDING
536+50.00



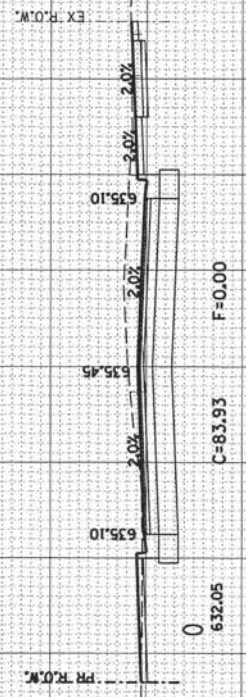
640 635
536+00.00



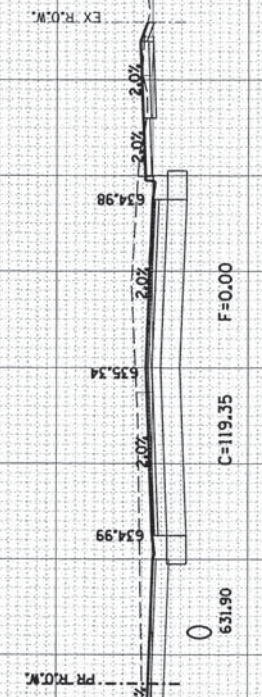
640 635
535+83.21



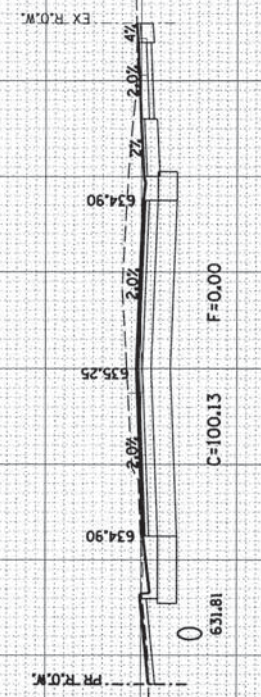
640 635
535+50.00



640 635
535+21.30



640 635
535+00.00



GAME FARM ROAD

640 635
BUILDING

640 635
BUILDING

640 635

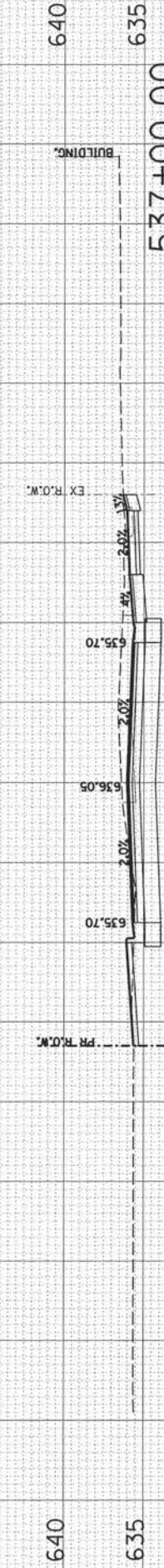
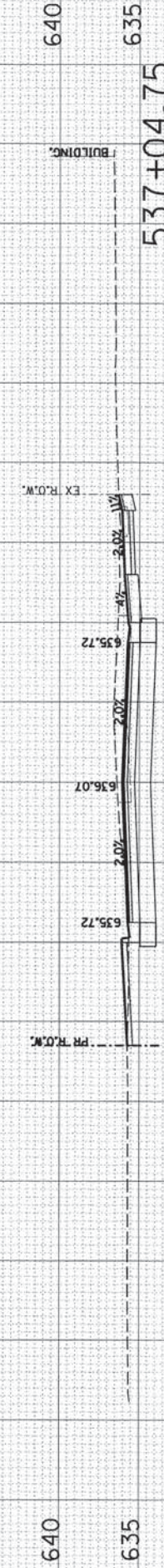
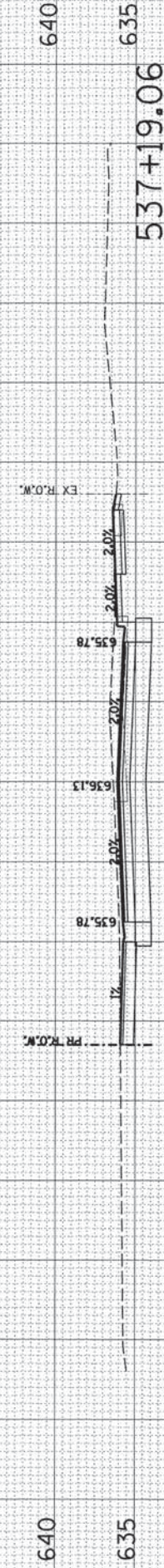
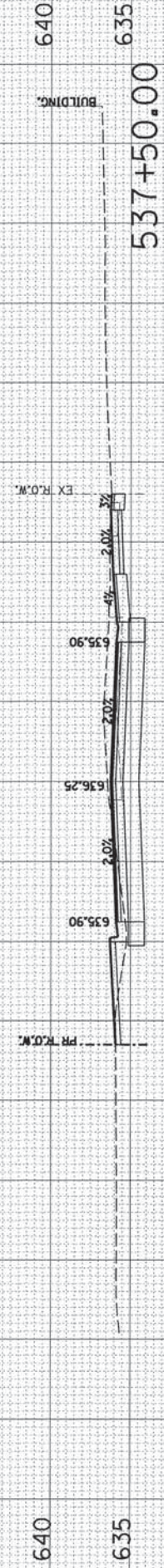
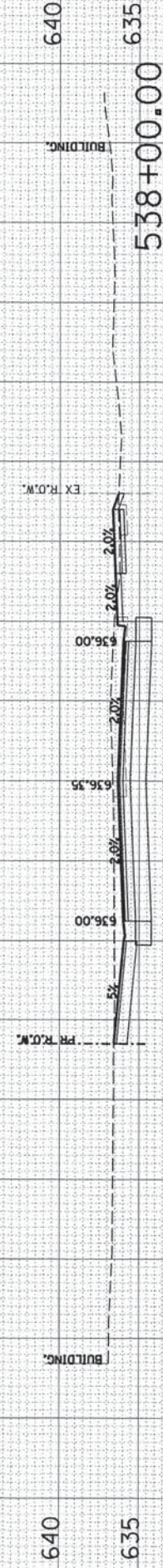
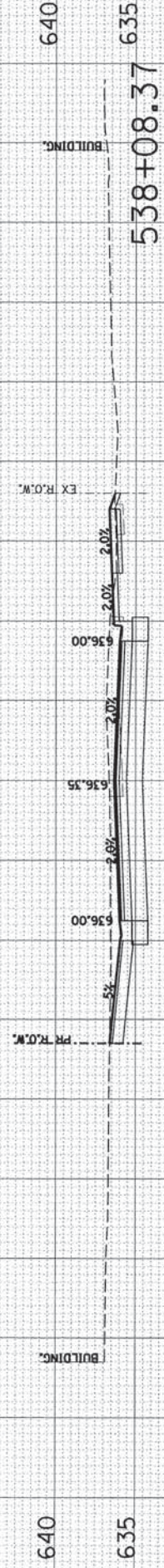
640 635
PR TEMP EXSEMENT

640 635
PR TEMP EXSEMENT

640 635
PR TEMP EXSEMENT

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 102 |
| STA. 537+00.00 | | TO STA. 538+08.37 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



GAME FARM ROAD

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 103 |
| STA. 538+50.00 | | TO STA. 540+00.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

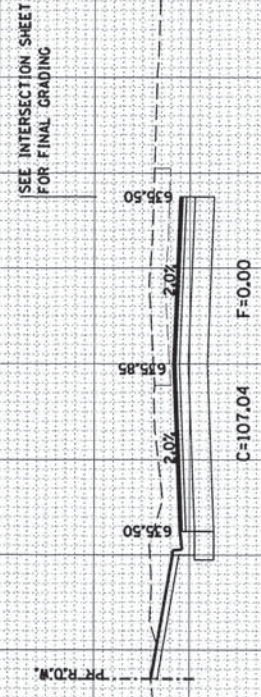
640
635
540+00.00 630



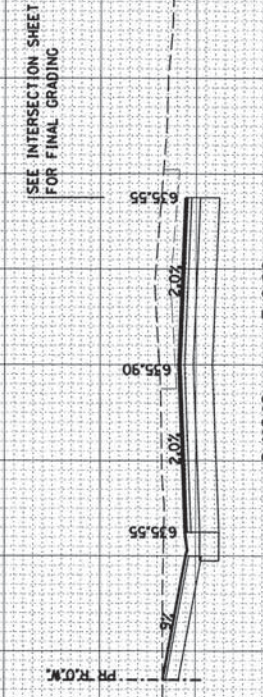
640
635
539+56.77



640
635
539+50.00



640
635
539+36.95



640
635
539+00.00



640
635
538+50.00



640
635
630

640
635

640
635

640
635

640
635

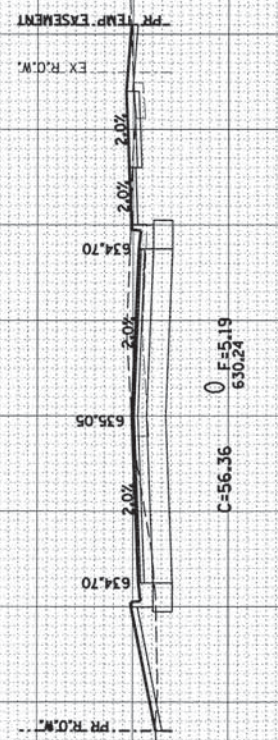
640
635

GAME FARM ROAD

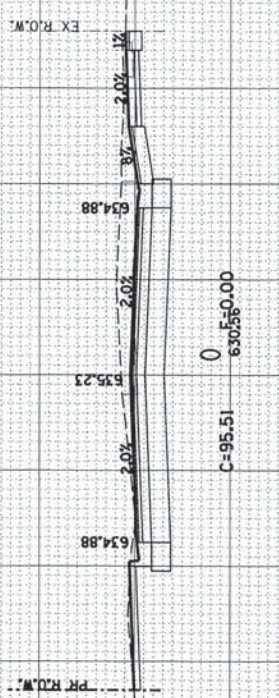
90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 104 |
| STA. 540+50.00 | | TO STA. 541+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

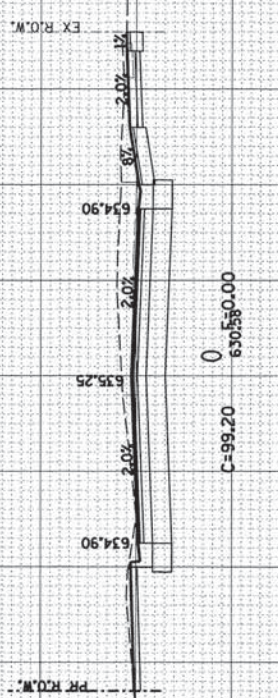
640
635
630
541+50.00



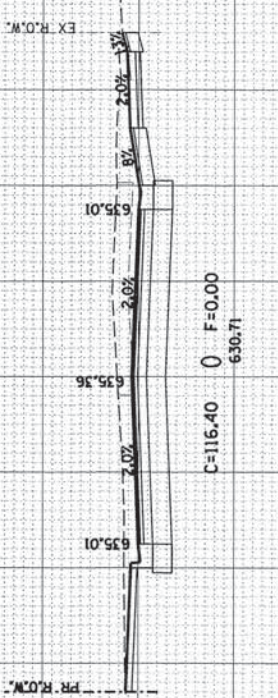
640
635
630
541+03.87



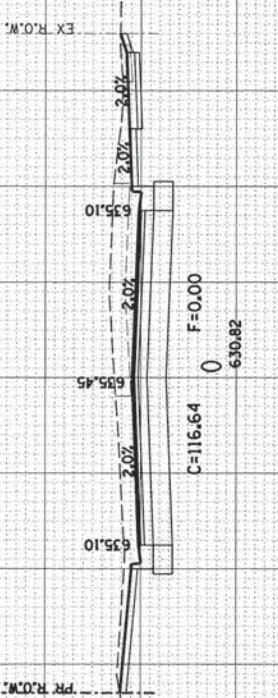
640
635
630
541+00.00



640
635
630
540+72.31



640
635
630
540+50.00



640
635
630

640
635
630

640
635
630

640
635
630

640
635
630

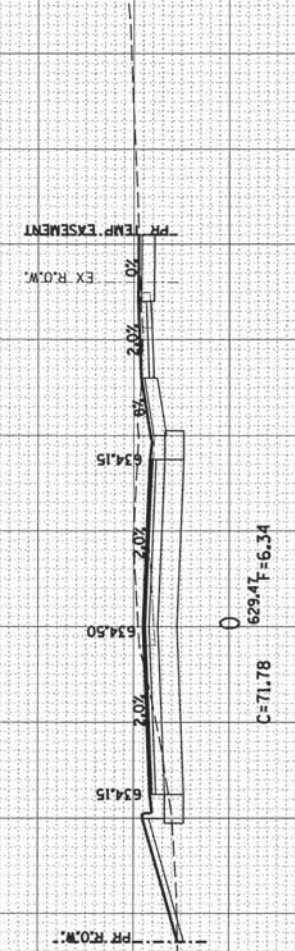
GAME FARM ROAD

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 105 |
| STA. 541+82.68 | | TO STA. 542+88.65 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

640
635
630

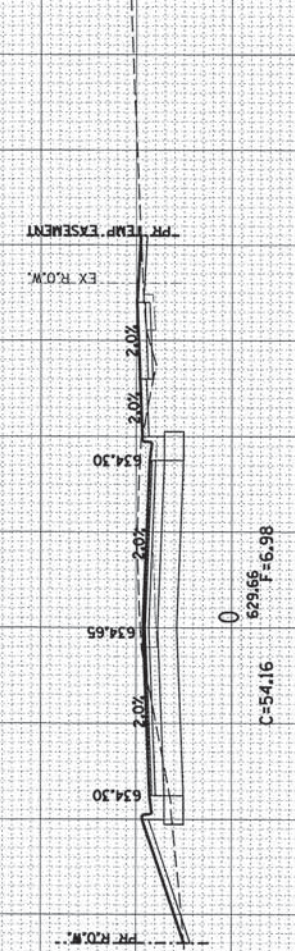
542+88.65



640
635
630

640
635
630

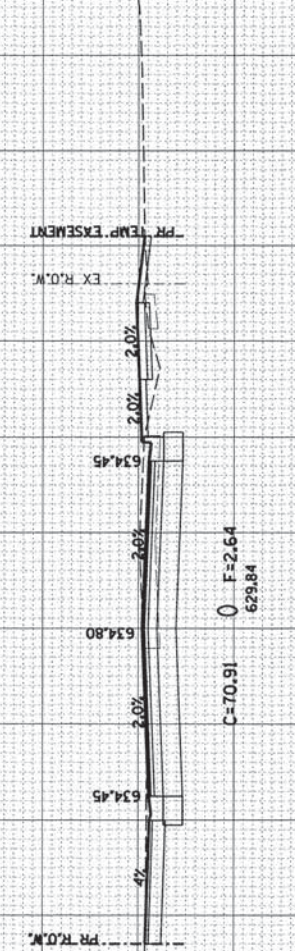
542+50.00



640
635
630

640
635
630

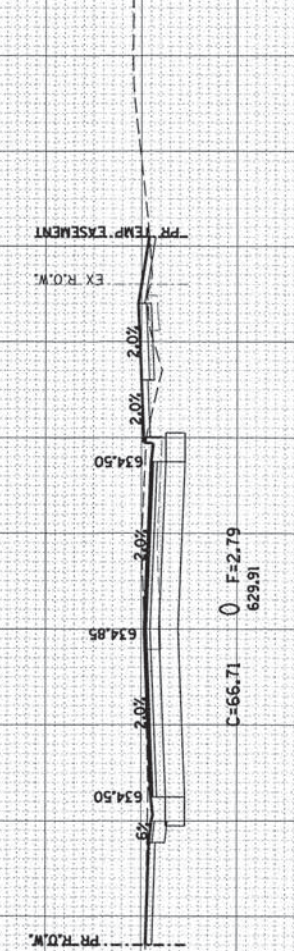
542+12.79



640
635
630

640
635
630

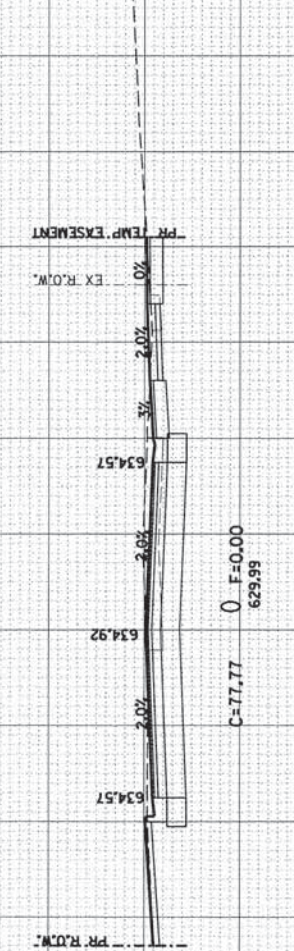
542+00.00



640
635
630

640
635
630

541+82.68



640
635
630

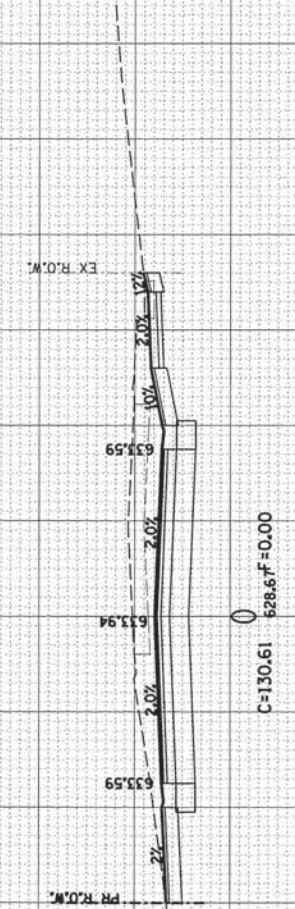
GAME FARM ROAD

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | III | 106 |
| STA. 543+00.00 | | TO STA. 544+50.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

640
635
630

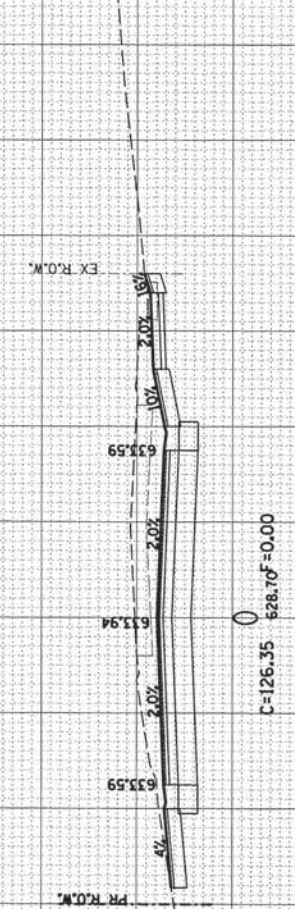
544+50.00



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630

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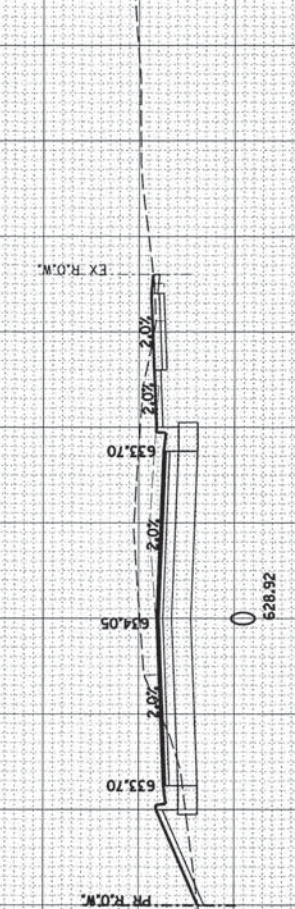
544+45.25



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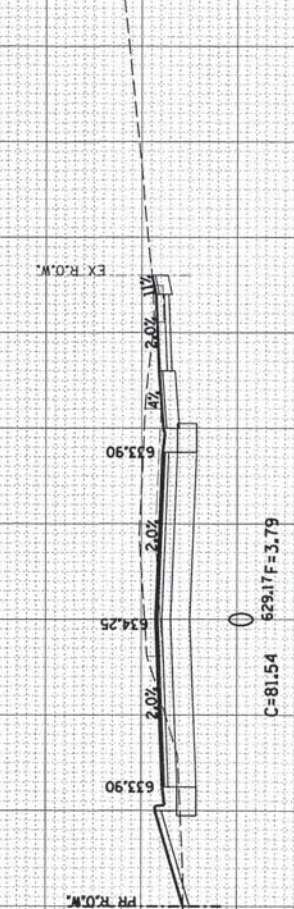
544+00.00



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543+50.00



640
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630

640
635
630

543+00.00

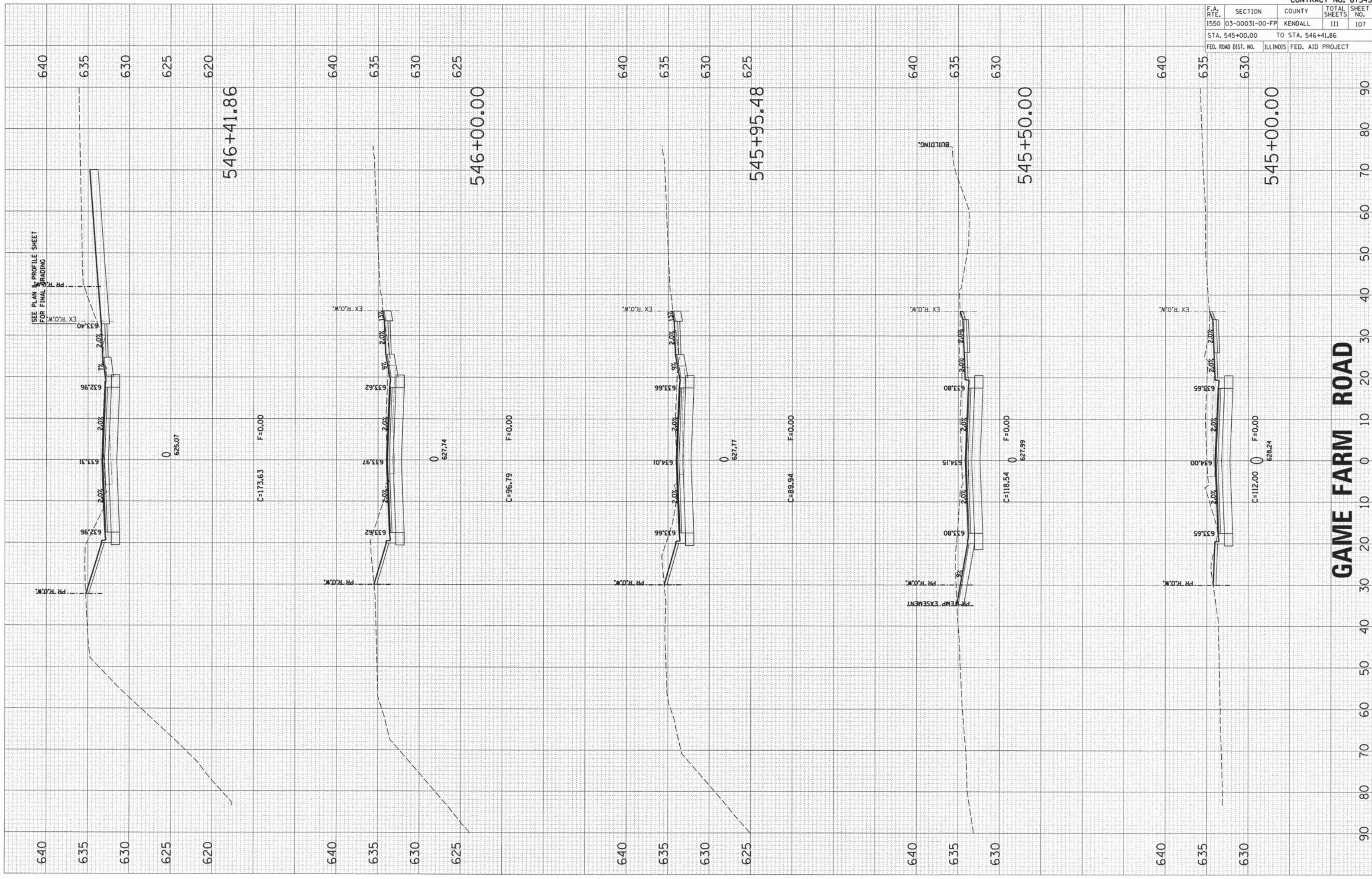


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GAME FARM ROAD

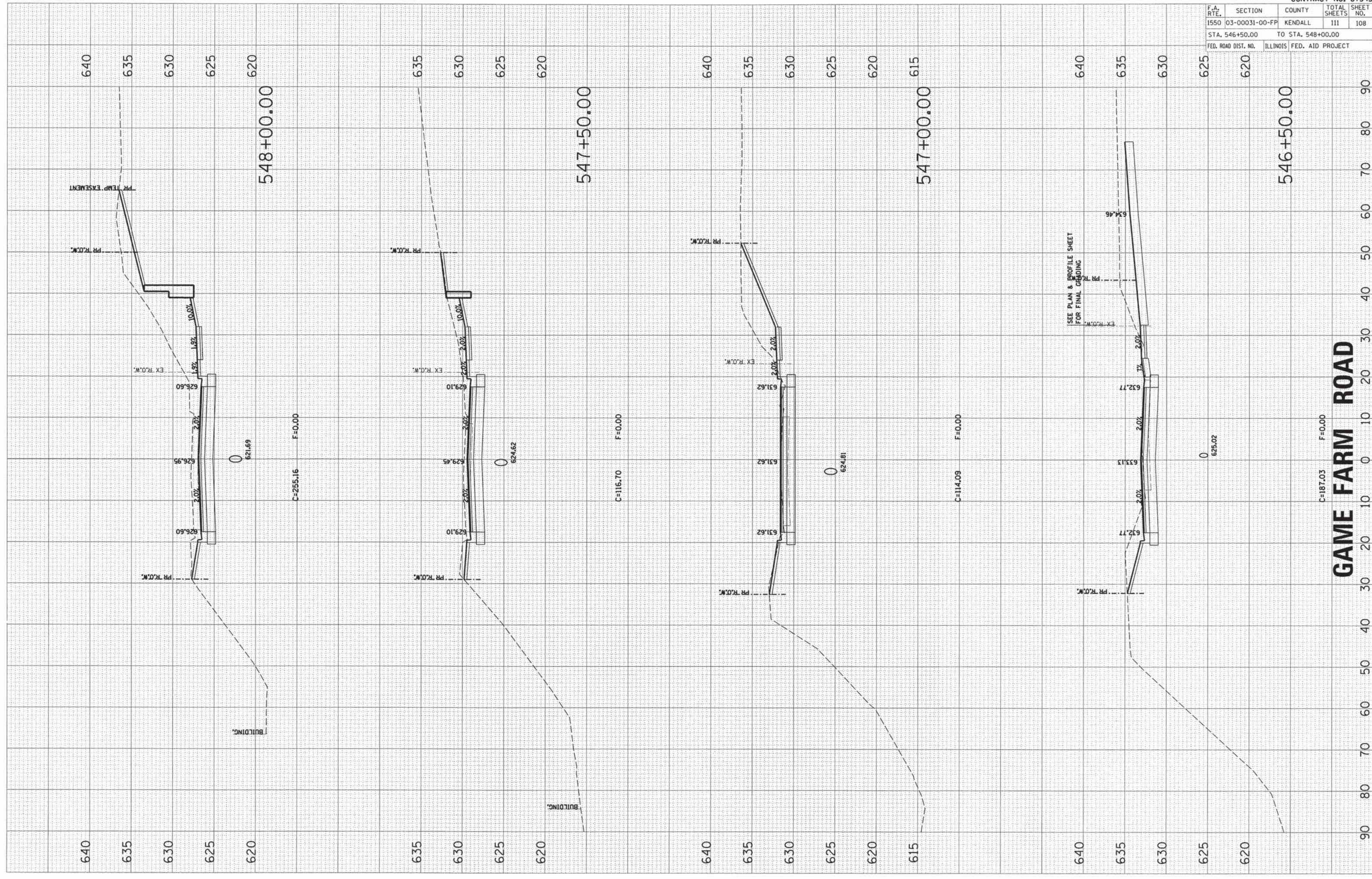
90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 107 |
| STA. 545+00.00 | | TO STA. 546+41.86 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



GAME FARM ROAD

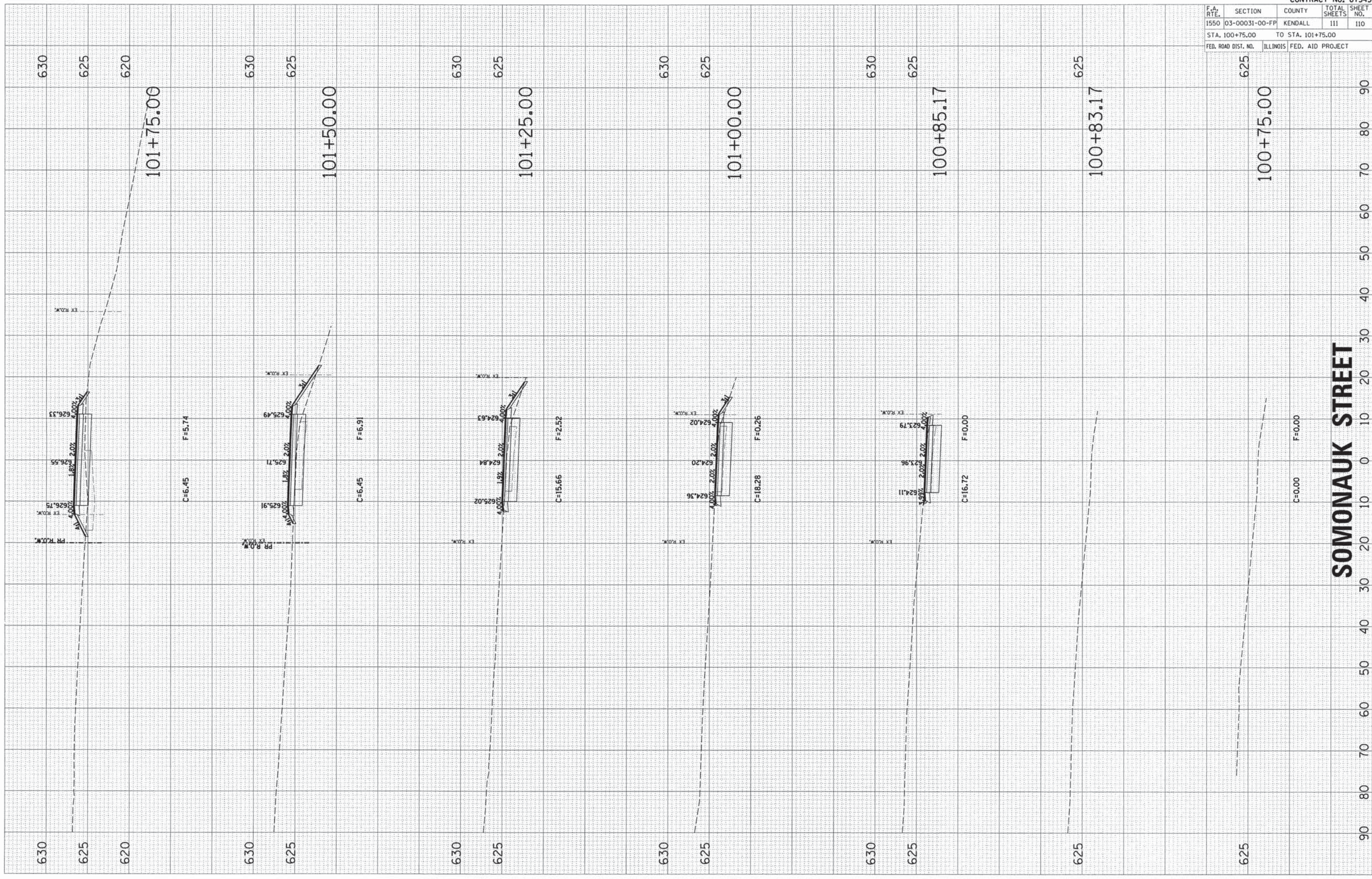
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|------------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 108 |
| STA. 546+50.00 | | TO STA. 548+00.00 | | |
| FED. ROAD DIST. NO. | | ILLINOIS | FED. AID PROJECT | |



SEE PLAN & PROFILE SHEET FOR FINAL GRADING

GAME FARM ROAD

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 110 |
| STA. 100+75.00 | | TO STA. 101+75.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |



SOMONAUK STREET

| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------|----------------|-------------------|--------------|-----------|
| 1550 | 03-00031-00-FP | KENDALL | 111 | 111 |
| STA. 102+00.00 | | TO STA. 102+20.00 | | |
| FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT | | |

630
625

102+20.00

EX R.O.W.

627.36

627.47

627.58

PR R.O.W.

C=6.29 F=57.13

630
625

630
625

102+00.00

626.93

627.15

627.34

PR R.O.W.

C=0.06 F=5.25

630
625

SOMONAUK STREET

90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90