

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	61 B-BR-1	KANE	37	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO.	62817	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS**

# PROPOSED HIGHWAY PLANS

**FAP ROUTE 573: IL 56  
OVER BLACKBERRY CREEK  
SECTION: 61 B-BR-1  
BRIDGE DECK REPLACEMENT  
STRUCTURE NOS. 045-0026(E.B.) AND 045-0027(W.B.)  
PROJECT: BHF-0573(099)  
KANE COUNTY  
C-91-318-04**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

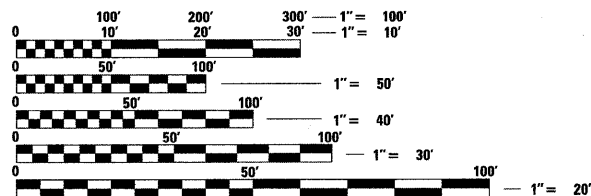
**IMPROVEMENT LOCATED IN  
THE VILLAGE OF SUGAR GROVE**

**TRAFFIC DATA**

2007 ADT = 19,800 VEHICLES PER DAY  
SPEED LIMIT = 55 MPH

**IL 56 OVER BLACKBERRY CREEK**

STRUCTURES NUMBER 045-0026 (EB) & 045-0027 (WB)  
R.C. DECK ON SIMPLY SUPPORTED WIDE FLANGE BEAM  
SPANS. DECK TO BE REMOVED AND REPLACED USING  
STAGE CONSTRUCTION. WORK INCLUDES PIER AND  
ABUTMENT REPAIRS.

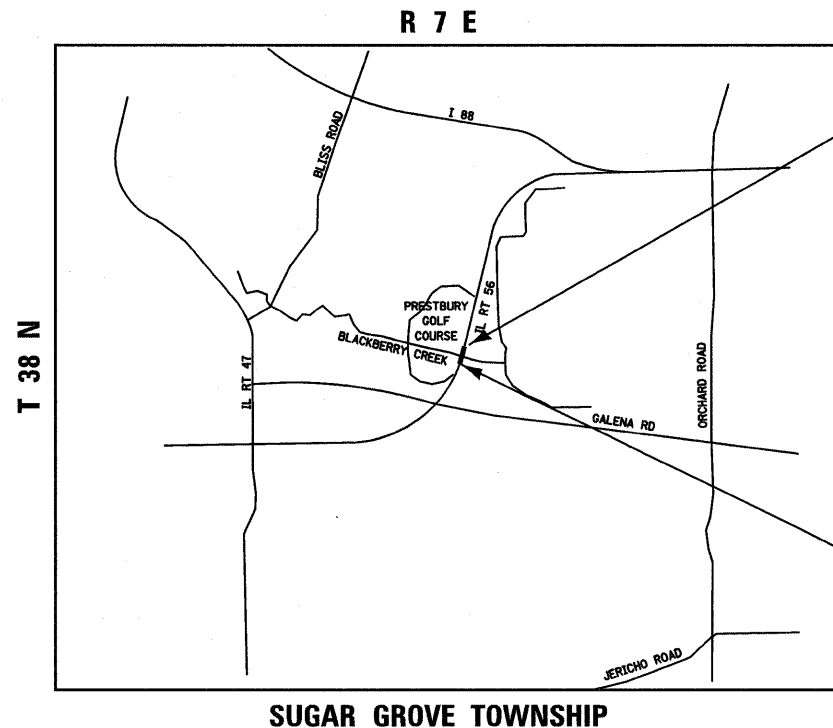


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER MICHELLE AQUINO (847) 705-4606  
PROJECT MANAGER LONG TRAN (847) 705-4232

CONTRACT NO. 62817



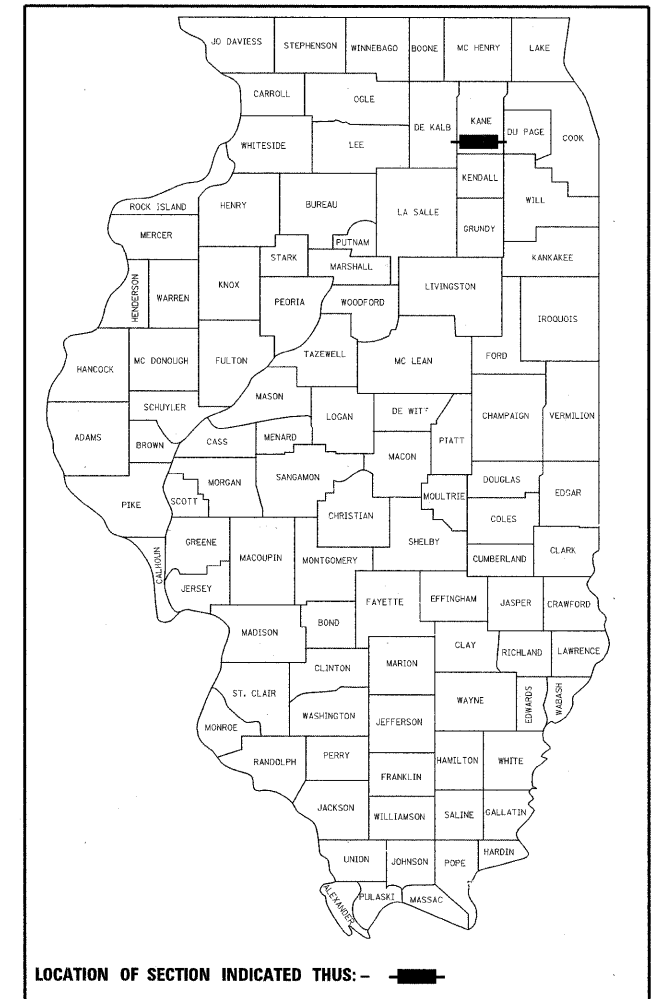
**IMPROVEMENT ENDS  
STA. 204 + 20**



**IMPROVEMENT BEGINS  
STA. 199 + 90**

GROSS & NET LENGTH OF PROJECT = 430 FT = 0.08 MILE

D-91-318-04



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

**rjngroup** 048453  
Excellence through Ownership  
200 West Front Street  
Wheaton, IL 60187  
License # 184-000813

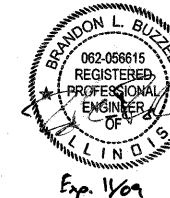
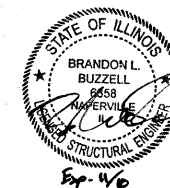
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED MAY 12, 20 09

Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
October 2, 20 09

Charles J. Ingersoll ENGINEER OF DESIGN AND ENVIRONMENT  
October 2, 20 09

Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER



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OF THE STATE OF ILLINOIS**

**STATE STANDARDS**

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-5	TYPICAL SECTIONS
6-7	MAINTENANCE OF TRAFFIC AND STAGING PLANS
8	EXISTING CONDITION (DEMO) PLAN
9	PROPOSED ROADWAY PLAN AND PROFILE
10	EROSION CONTROL PLAN
11	PAVEMENT MARKING PLAN
12	BRIDGE APPROACH SLAB PLAN AND DETAILS
13	GENERAL PLAN AND ELEVATION
14	GENERAL NOTES AND BILL OF MATERIAL
15	STAGE CONSTRUCTION DETAILS
16	CONCRETE REMOVAL PLANS AND DETAILS
17	DECK ELEVATIONS - I
18	DECK ELEVATIONS - II
19	SUPERSTRUCTURE
20	SUPERSTRUCTURE DETAILS
21	DIAPHRAGM DETAILS
22	BRIDGE JOINT SYSTEM - PREFORMED JOINT STRIP SEAL
23	FRAMING PLAN
24	BEARING DETAILS - WEST BOUND PIER
25	BEARING DETAILS - EAST BOUND PIER
26	PIER DETAILS (WEST BOUND STRUCTURE)
27	SUBSTRUCTURE REPAIR
28	BAR SPLICER ASSEMBLY DETAILS
29	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
30	STEEL PLATE BEAM GUARDRAIL ADJACENT TO CURB AND GUTTER AND STABILIZATION AT TBT TY. 1 SPL
31	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
32	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
33	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
34	ARTERIAL ROAD INFORMATION SIGN
35-37	CROSS SECTIONS

STANDARD NO.	DESCRIPTION
420001-07	PAVEMENT JOINTS
482006-03	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
483001-04	PCC SHOULDER
515001-03	NAME PLATE FOR BRIDGES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-08	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
630201-06	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
631026-05	TRAFFIC BARRIER TERMINAL, TYPE 5 & 5A
631031-07	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKERS AND MOUNTING DETAILS
701400-03	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701402-07	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER
701406-05	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701411-05	LANE CLOSURE MULTILANE AT ENTRANCE OR EXIT RAMP FOR SPEEDS > 45 MPH
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS > OR = 45 MPH
701901-01	TRAFFIC CONTROL DEVICES
704001-05	TEMPORARY CONCRETE BARRIER
780001-02	TYPICAL PAVEMENT MARKINGS
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS

(cont.)

5. THE RESIDENT ENGINEER SHALL CONTACT MR. DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT (847)741-9857 AT LEAST 2 WEEKS PRIOR TO INSTALLATION OF PERMANENT PAVEMENT MARKINGS.

**GENERAL NOTES - ROADWAY**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL •J.U.L.I.E.• AT (800)892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED.)
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- SAW CUTTING OF PAVEMENTS, SHOULDERS, ETC., SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN, STRAIGHT EDGE ON THE PORTION REMAINING. THE COST OF SAW CUTTING REMOVAL ITEMS SHALL BE INCLUDED IN THE UNIT PRICES OF THESE ITEMS.
- USE NO. 25 (#8) EPOXY-COATED TIE BARS (OR DOWEL BARS) CONFORMING TO ART. 100610(B)(2) OF THE STANDARD SPECIFICATIONS FOR LONGITUDINAL CONSTRUCTION JOINT GROUTED-IN-PLACE TIE BAR AS SHOWN ON STATE STANDARD 420001. THE TIE BARS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PAVEMENT ITEMS BEING CONSTRUCTED.

**GENERAL NOTES - TRAFFIC CONTROL & PROTECTION**

- STAGING PROCEDURES PRESENTED ARE THE SUGGESTED SEQUENCE OF OPERATIONS. AT HIS OPTION, THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE STAGING PROPOSAL TO THE ENGINEER FOR HIS APPROVAL.
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.
- ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

**GENERAL NOTES - MISCELLANEOUS**

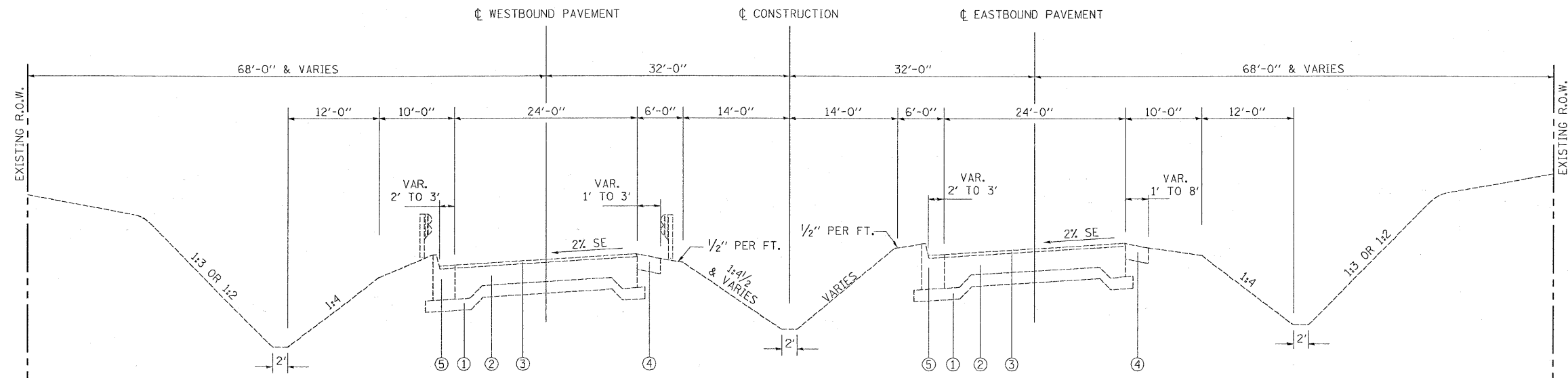
- ALL ELEVATIONS SHOWN ON THESE PLANS ARE ON THE U.S.G.S. DATUM
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

**GENERAL NOTES - EROSION CONTROL**

- ALL EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND OF THE APPLICABLE STATE STANDARDS FOR THE ENTIRE DURATION OF THE CONTRACT, OR UNTIL SUCH TIME AS DIRECTED BY THE ENGINEER.
- ALL AREAS DISTURBED AND RESTORED WITH SEEDING SHALL BE COVERED WITH AN EROSION CONTROL BLANKET IN ACCORDANCE WITH SECTION 251 OF THE STANDARD SPECIFICATIONS.
- IN ACCORDANCE WITH ARTICLE 107.01 OF THE STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL ABIDE AND COMPLY WITH ALL LOCAL ORDINANCES AND REQUIREMENTS AND SHALL COOPERATE WITH THE LOCAL AGENCY(IES) AND MUNICIPALITIES INVOLVED AS DIRECTED BY THE ENGINEER. THE COST FOR COMPLYING WITH THESE REQUIREMENTS SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT PAY ITEMS.

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PLOT SCALE = 1/50	CHECKED - SPF	REVISED -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 62817			
PLOT DATE = 5/6/2009	DATE - APRIL 02, 2009	REVISED -	REVISED -		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT									





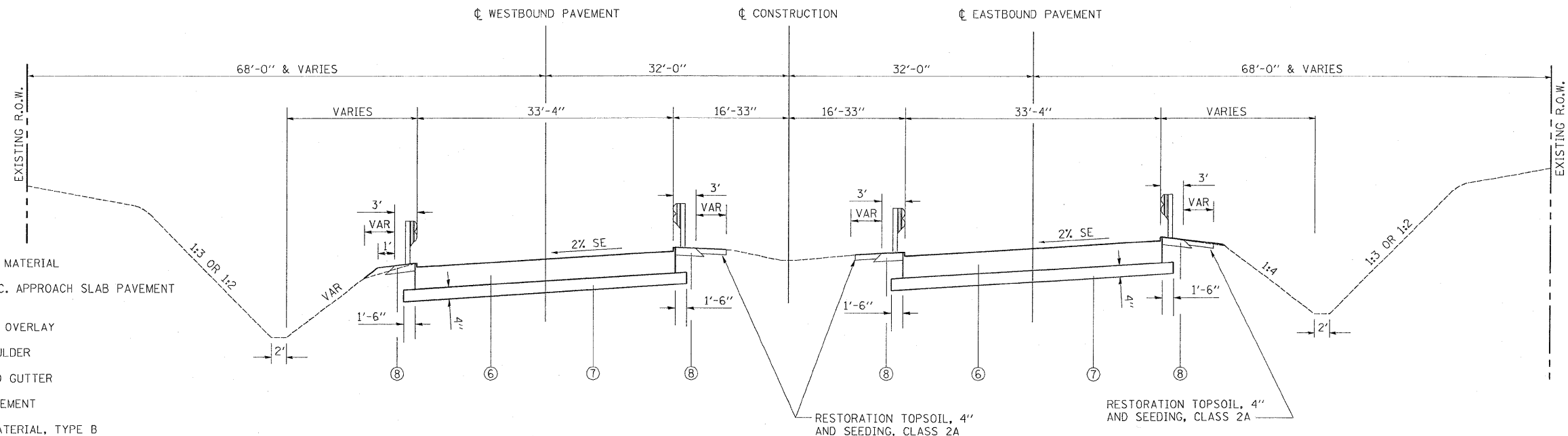
**EXISTING TYPICAL SECTION**

STA 201+23 TO STA 201+65  
BRIDGE OMISSION  
STA 202+55 TO STA 202+97

HMA MIXTURE REQUIREMENTS				
THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT				
PAY ITEM		AC TYPE	VOIDS	SPECIFY THICKNESS
TEMPORARY PAVEMENT	HMA BASE COURSE (HMA BINDER IL-19mm)	PG 64-22*	4% @ 50 GYR	8" (3 LIFTS)
	HMA SURFACE COURSE, MIX "D", N50 (IL-9.5mm)	PG 64-22	4% @ 50 GYR	2"
HOT-MIX ASPHALT SHOULDER		PG 64-22*	2% @ 30 GYR	6" & 8" (2 & 3 LIFTS)

NOTE:  
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.

\*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.



**PROPOSED TYPICAL SECTION  
APPROACH PAVEMENT SECTION**

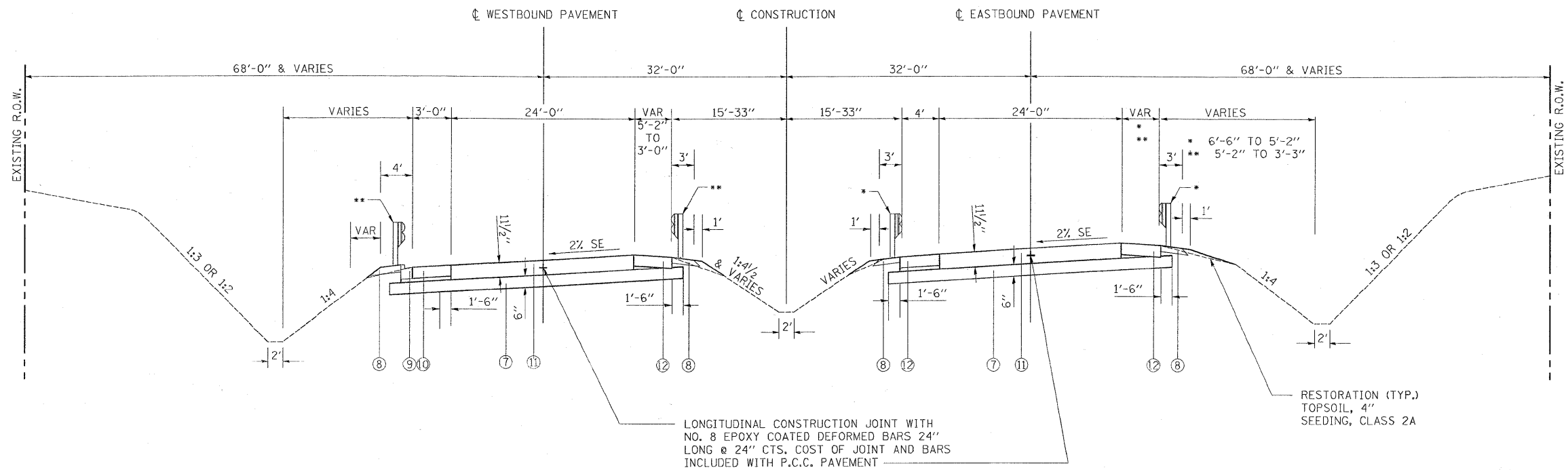
STA 201+35 TO STA 201+65  
BRIDGE OMISSION  
STA 202+55 TO STA 202+85

**LEGEND**

- ① EXISTING 6" SUBBASE GRANULAR MATERIAL
- ② EXISTING 10 1/2" REINFORCED P.C.C. APPROACH SLAB PAVEMENT (IDOT STD 1908-R)
- ③ EXISTING 1 1/2" HOT-MIX ASPHALT OVERLAY
- ④ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑤ EXISTING COMBINATION CURB AND GUTTER
- ⑥ PROPOSED BRIDGE APPROACH PAVEMENT
- ⑦ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B
- ⑧ PROPOSED HOT-MIX ASPHALT SHOULDERS, 6"
- ⑨ PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑩ PROPOSED PORTLAND CEMENT CONCRETE SHOULDERS, 10"
- ⑪ PROPOSED BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)
- ⑫ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"

FILE NAME =	USER NAME = *USER*	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK EXISTING AND PROPOSED TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 6/12/2009		DATE - APRIL 02, 2009	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	



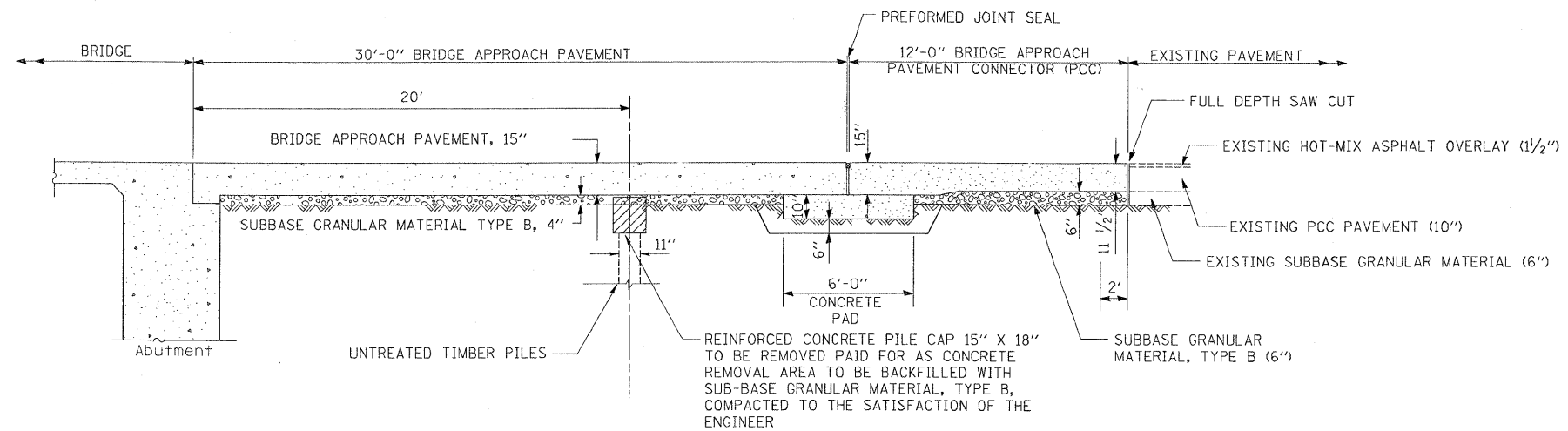


**PROPOSED TYPICAL SECTION  
PAVEMENT REPLACEMENT SECTION**

\* STA 201+23 TO STA 201+35  
\*\* STA 202+85 TO STA 202+97

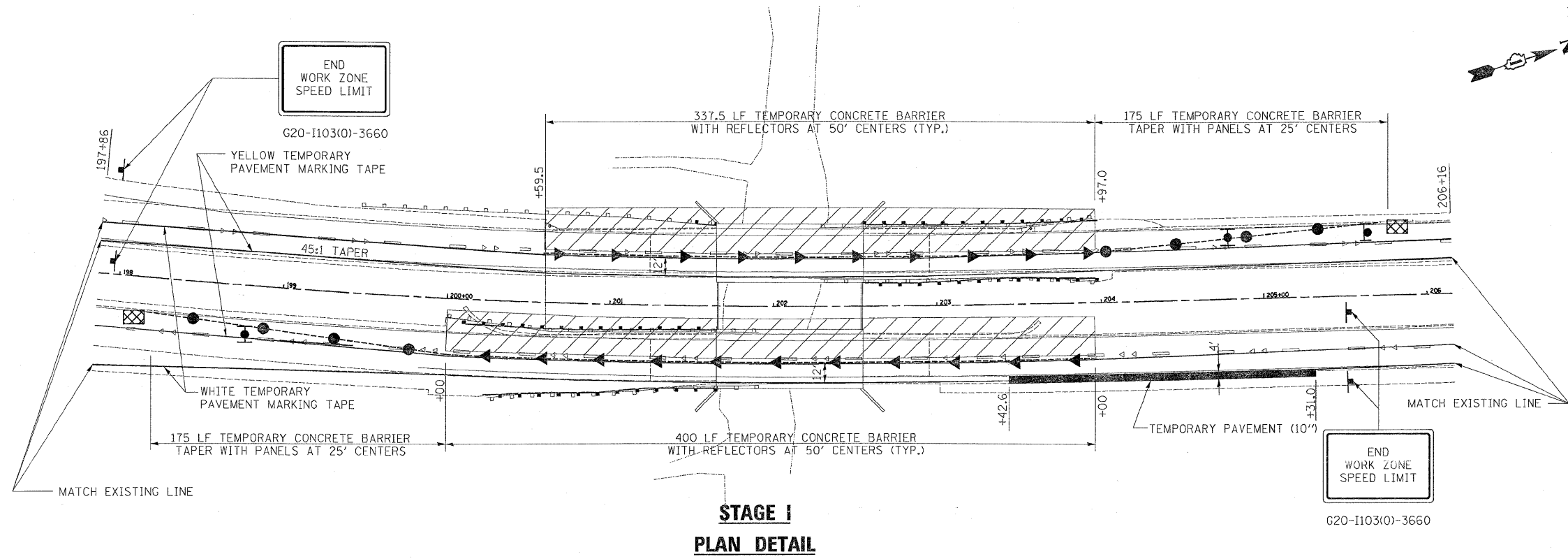
**LEGEND**

- ① EXISTING 6" SUBBASE GRANULAR MATERIAL
- ② EXISTING 10 1/2" REINFORCED P.C.C. APPROACH SLAB PAVEMENT (IDOT STD 1908-R)
- ③ EXISTING 1 1/2" HOT-MIX ASPHALT OVERLAY
- ④ EXISTING HOT-MIX ASPHALT SHOULDER
- ⑤ EXISTING COMBINATION CURB AND GUTTER
- ⑥ PROPOSED BRIDGE APPROACH PAVEMENT
- ⑦ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE B
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- ⑩ PROPOSED PORTLAND CEMENT CONCRETE SHOULDERS, 10"
- ⑪ PROPOSED BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)
- ⑫ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"



**TYPICAL PROFILE VIEW -  
APPROACH ROADWAY WORK**

FILE NAME =	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK EXISTING AND PROPOSED TYPICAL SECTIONS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLDT DATE = 6/12/2009		DATE - APRIL 02, 2009	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT



**STAGE I  
PLAN DETAIL**

**STAGE I**

CONSTRUCT TEMPORARY PAVEMENT ALONG RIGHT SIDE OF EAST BOUND LANES AS NOTED IN THESE PLANS.

WEST BOUND - INSTALL TEMPORARY TRAFFIC CONTROL, CLOSING RIGHT LANE, AS DETAILED IN THESE PLANS AND IN ACCORDANCE WITH TRAFFIC CONTROL STANDARDS 701400 AND 701402.

EAST BOUND - INSTALL TEMPORARY TRAFFIC CONTROL, CLOSING LEFT LANE, AS DETAILED IN THESE PLANS AND IN ACCORDANCE WITH TRAFFIC CONTROL STANDARDS 701400 AND 701402.

REMOVE WEST SIDE OF BRIDGE DECKS, APPROACH PAVEMENTS, ADJACENT CURB AND GUTTER, SHOULDER AND GUARDRAIL.

CONSTRUCT NEW WEST SIDE OF BRIDGE DECKS, APPROACH PAVEMENTS, CURB AND GUTTER ALONG OUTSIDE OF WEST BOUND LANE, NEW SHOULDER AND INSTALL REQUIRED GUARDRAIL IMPROVEMENTS.

CONSTRUCT TEMPORARY PAVEMENT ALONG LEFT SIDE OF EAST BOUND LANES AS NOTED ON THESE PLANS.

**STAGE II**

WEST BOUND - RELOCATE TEMPORARY TRAFFIC CONTROL, CLOSING LEFT LANE, AS DETAILED IN THESE PLANS AND IN ACCORDANCE WITH TRAFFIC CONTROL STANDARDS 701400 AND 701402.

EAST BOUND - RELOCATE TEMPORARY TRAFFIC CONTROL, CLOSING RIGHT LANE, AS DETAILED IN THESE PLANS AND IN ACCORDANCE WITH TRAFFIC CONTROL STANDARDS 701400 AND 701402. FOLLOW STANDARD 701411 (APPLICATION NO. 2) FOR TRAFFIC CONTROL OF ENTRANCE RAMP.

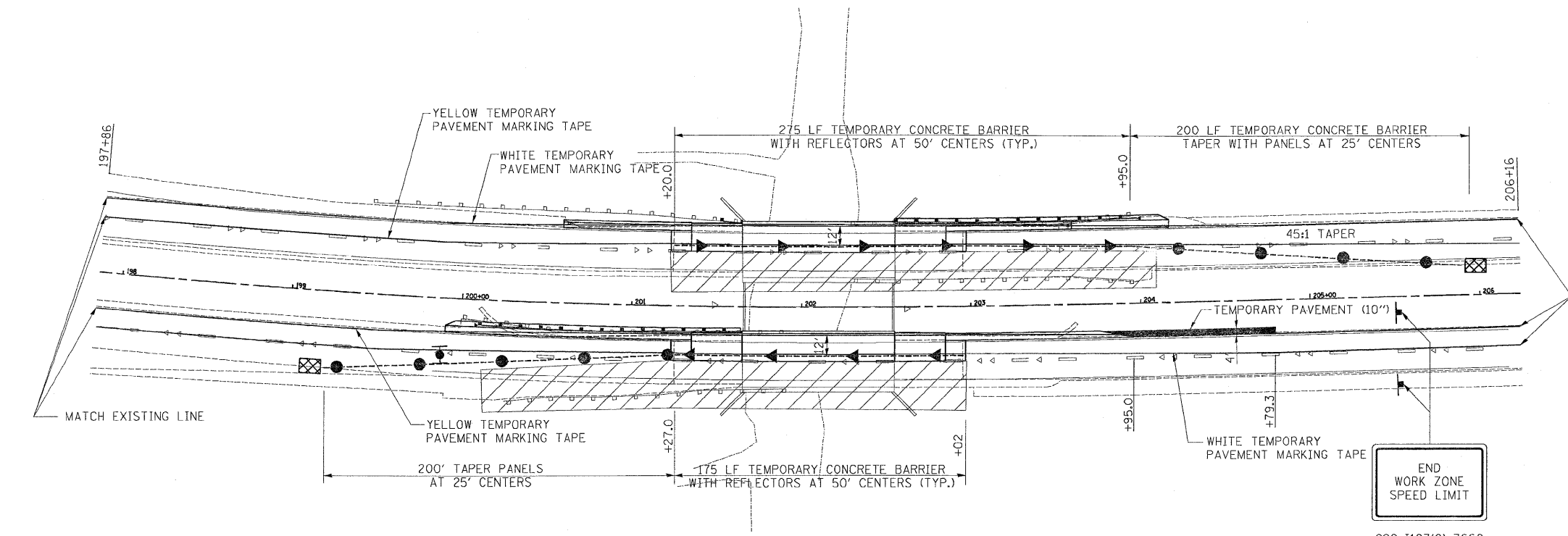
REMOVE EAST SIDE OF BRIDGE DECK, APPROACH PAVEMENTS, ADJACENT SHOULDER AND GUARDRAIL.

CONSTRUCT NEW EAST SIDE OF BRIDGE DECK, APPROACH PAVEMENTS, NEW SHOULDER AND INSTALL REQUIRED GUARDRAIL IMPROVEMENTS.

REMOVE TEMPORARY TRAFFIC CONTROL AND OPEN BOTH LANES OF TRAFFIC.

**STAGE III**

INSTALL PERMANENT PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS USING TRAFFIC CONTROL STANDARDS 701406 AND 701426 FOR DAY LANE CLOSURES.

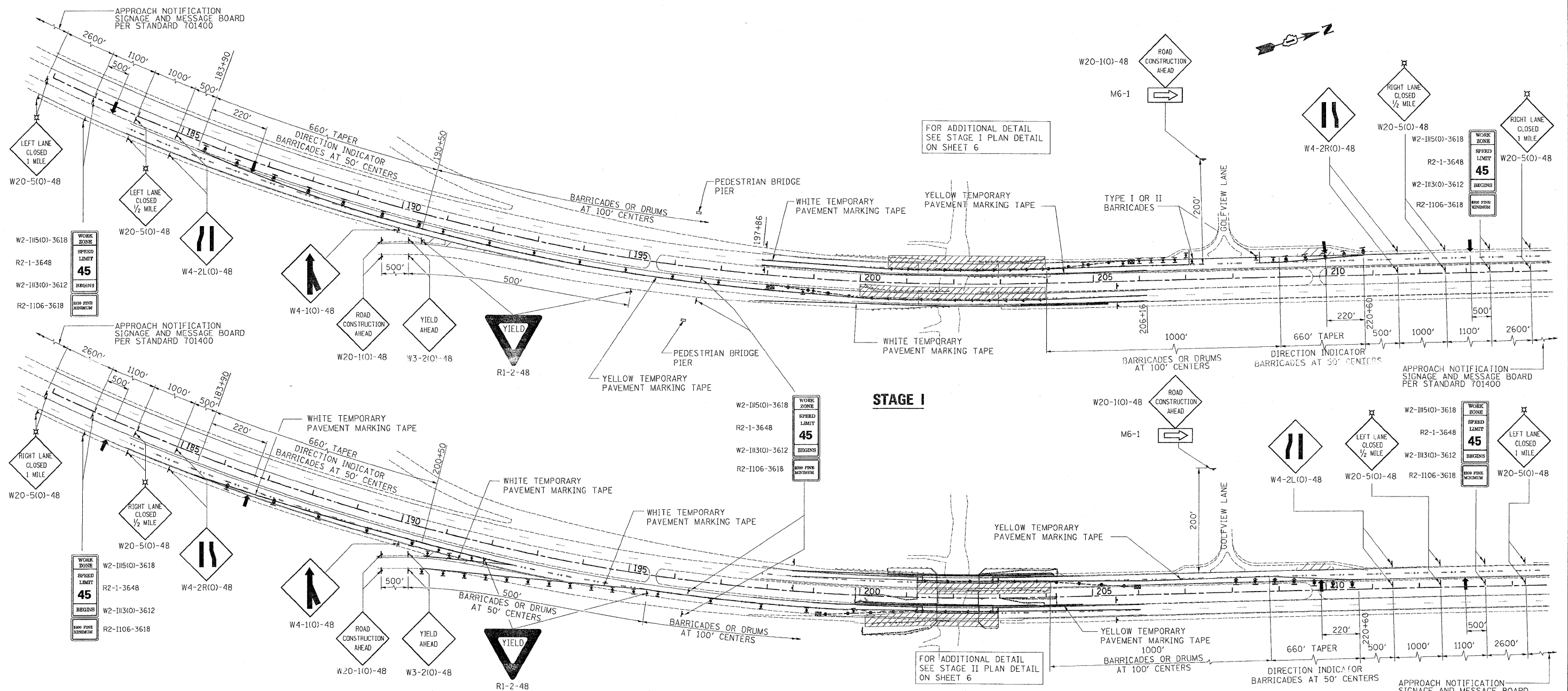


**STAGE II  
PLAN DETAIL**

- Temporary Concrete Barrier
- ↑ Sign
- ↑ Arrow Board
- Type II Barricade, drum, or vertical barricade with steady burn monodirectional light
- ◆ Direction indicator barricade with steady burn monodirectional light
- ▨ Work Area
- ⊠ Impact Attenuator (non redirective) test level 3
- ◀ Type C monodirectional reflector
- Vertical panel with steady burn monodirectional light
- Temporary pavement marking tape

Note: To be Used with  
IDOT Standard 701400

FILE NAME = K:\11182508\cad\design\182508_stage.dwg	USER NAME = \$USER\$ jldgn	DESIGNED - DRAWN - JTT	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK MAINTENANCE OF TRAFFIC NOTES AND STAGING DETAILS</b>	F.A.P. RTE. 573	SECTION 61 B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 6	
PLOT SCALE = 1:40	CHECKED - SPF	REVISED -	REVISED -			SCALE: 1" = 40'	SHEET NO. OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
PLOT DATE = 6/12/2009	DATE - APRIL 02, 2009	REVISED -	REVISED -			CONTRACT NO. 62817					

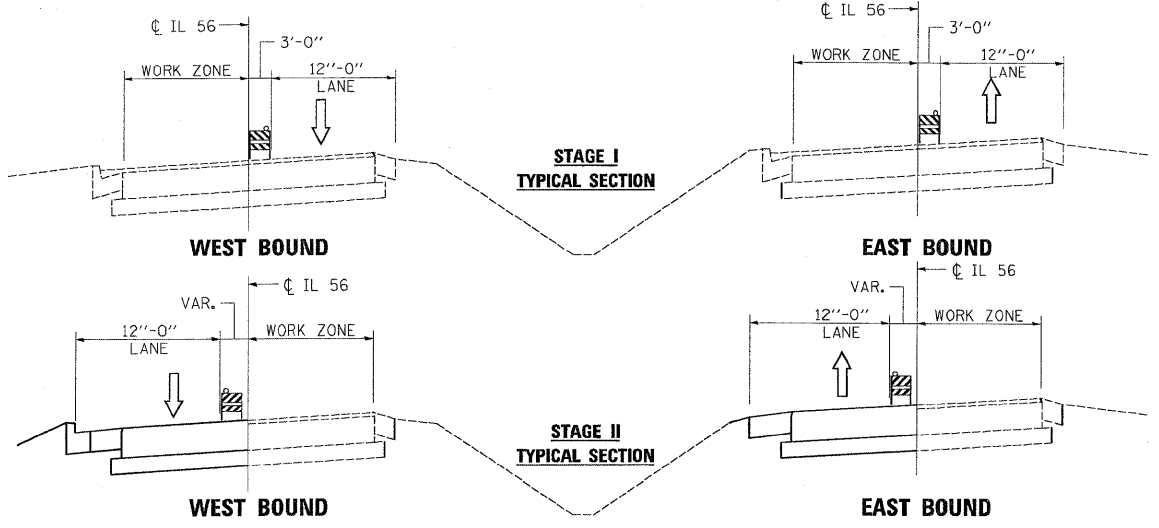


FOR ADDITIONAL DETAIL SEE STAGE I PLAN DETAIL ON SHEET 6

**STAGE I**

FOR ADDITIONAL DETAIL SEE STAGE II PLAN DETAIL ON SHEET 6

**STAGE II**



- Temporary Concrete Barrier
- ↑ Sign
- ↑ Arrow Board
- Type II Barricade, drum, or vertical barricade with steady burn monodirectional light
- ▲ Direction Indicator barricade with steady burn monodirectional light
- ▨ Work Area
- ▣ Impact Attenuator (non redirective) test level 3
- ◀ Type C monodirectional reflector
- Vertical panel with steady burn monodirectional light
- Temporary pavement marking tape

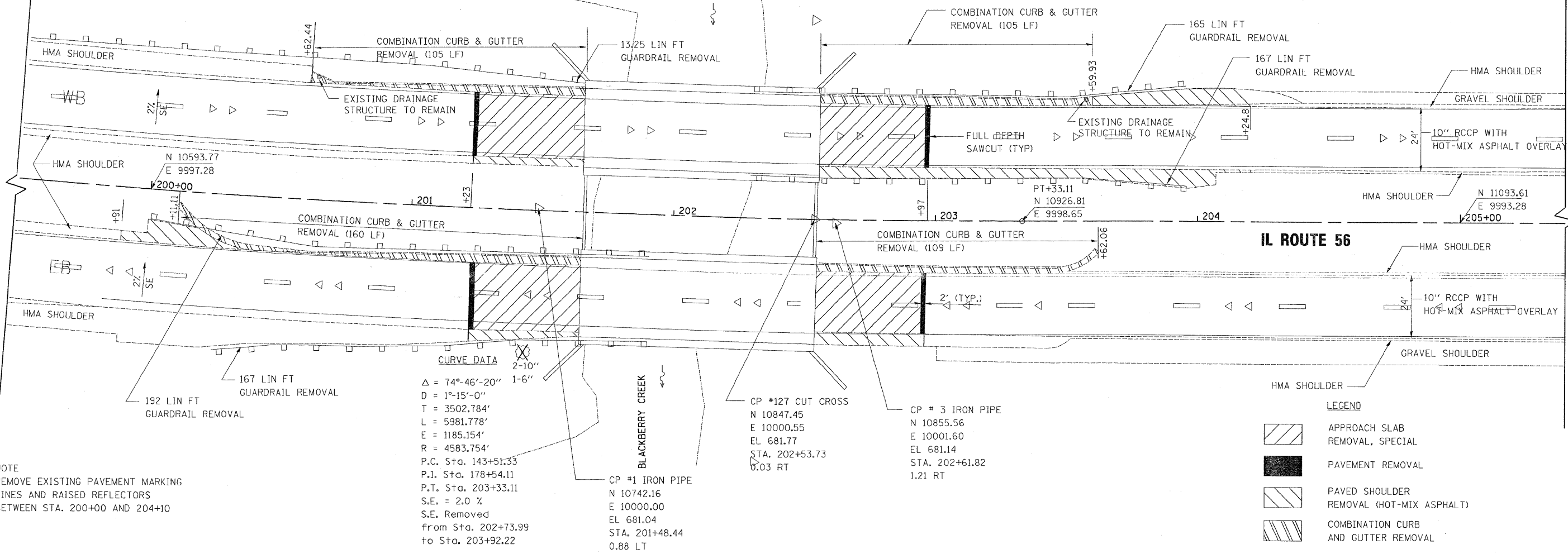
Note: To be Used with IDOT Standard 701400

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PLDT SCALE = 1:107.5	PLDT DATE = 6/12/2009	DRAWN - JTT	REVISED -			SCALE:	SHEET NO. OF SHEETS STA. TO STA.	CONTRACT NO. 62817			
		CHECKED - SPF	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					
		DATE - APRIL 02, 2009	REVISED -								



DATE	BY

DATE	BY



NOTE  
 REMOVE EXISTING PAVEMENT MARKING  
 LINES AND RAISED REFLECTORS  
 BETWEEN STA. 200+00 AND 204+10

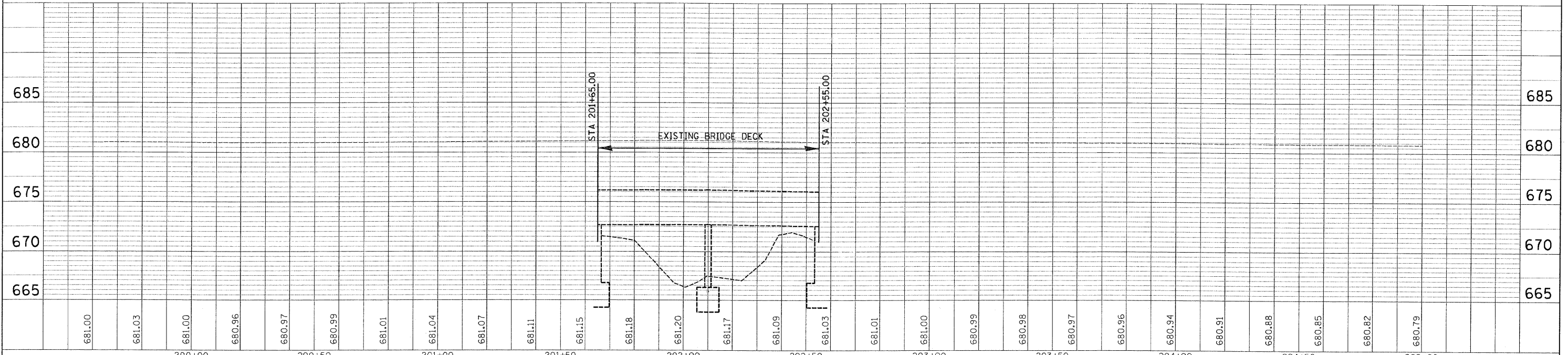
CURVE DATA  
 $\Delta = 74^\circ 46' - 20''$   
 $D = 1^\circ 15' - 0''$   
 $T = 3502.784'$   
 $L = 5981.778'$   
 $E = 1185.154'$   
 $R = 4583.754'$   
 P.C. Sta. 143+51.33  
 P.I. Sta. 178+54.11  
 P.T. Sta. 203+33.11  
 S.E. = 2.0 %  
 S.E. Removed  
 from Sta. 202+73.99  
 to Sta. 203+92.22

BLACKBERRY CREEK  
 CP #1 IRON PIPE  
 N 10742.16  
 E 10000.00  
 EL 681.04  
 STA. 201+48.44  
 0.88 LT

CP #127 CUT CROSS  
 N 10847.45  
 E 10000.55  
 EL 681.77  
 STA. 202+53.73  
 0.03 RT

CP # 3 IRON PIPE  
 N 10855.56  
 E 10001.60  
 EL 681.14  
 STA. 202+61.82  
 1.21 RT

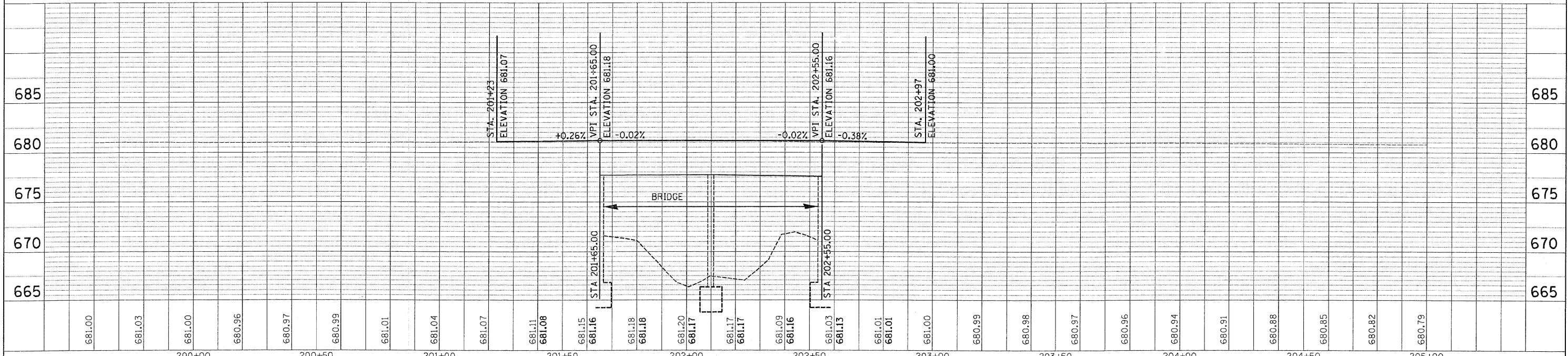
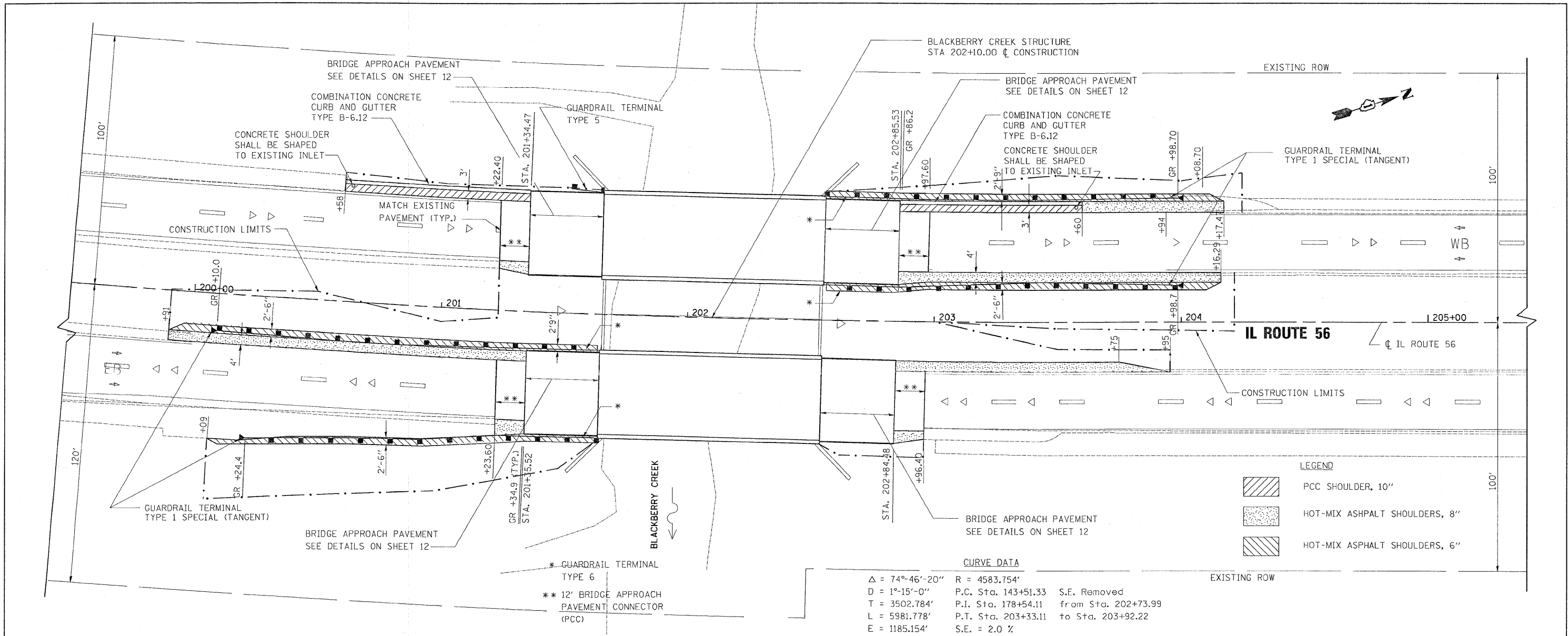
- LEGEND
- APPROACH SLAB REMOVAL, SPECIAL
  - PAVEMENT REMOVAL
  - PAVED SHOULDER REMOVAL (HOT-MIX ASPHALT)
  - COMBINATION CURB AND GUTTER REMOVAL



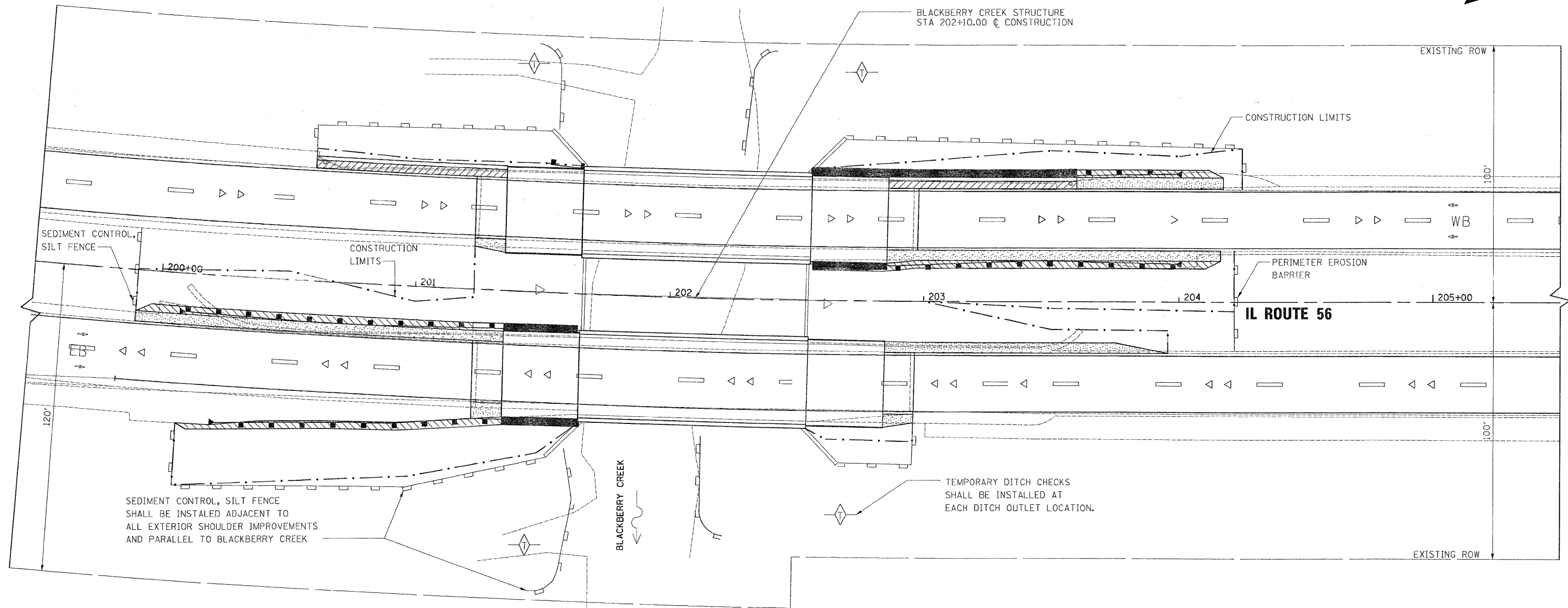
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PLOT SCALE = 1:20	CHECKED - SPF	DESIGNED - JTT	REVISED -			CONTRACT NO. 62817				
PLOT DATE = 6/12/2007	DATE - APRIL 02, 2009	CHECKED -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
						SCALE:	SHEET NO. OF SHEETS STA. TO STA.			

PLAN	SURVEYED	DATE
	BY	
	NO. 1	
	NO. 2	
	NO. 3	
	NO. 4	
	NO. 5	
	NO. 6	
	NO. 7	
	NO. 8	
	NO. 9	
	NO. 10	

PROFILE	SURVEYED	DATE
	BY	
	NO. 1	
	NO. 2	
	NO. 3	
	NO. 4	
	NO. 5	
	NO. 6	
	NO. 7	
	NO. 8	
	NO. 9	
	NO. 10	



FILE NAME = k:\1182508\ced\design\182508.pp_prop.dgn	USER NAME = #USER#	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK PROPOSED ROADWAY PLAN AND PROFILE</b>	F.A.P. RTE. 573	SECTION 61 B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 9	
PLOT SCALE = 1:20	CHECKED - SPF	DRAWN - JTT	REVISED -			CONTRACT NO. 62817					
PLOT DATE = 6/12/2009	DATE - APRIL 02, 2009	REVISIONS	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT					
		DATE	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.					



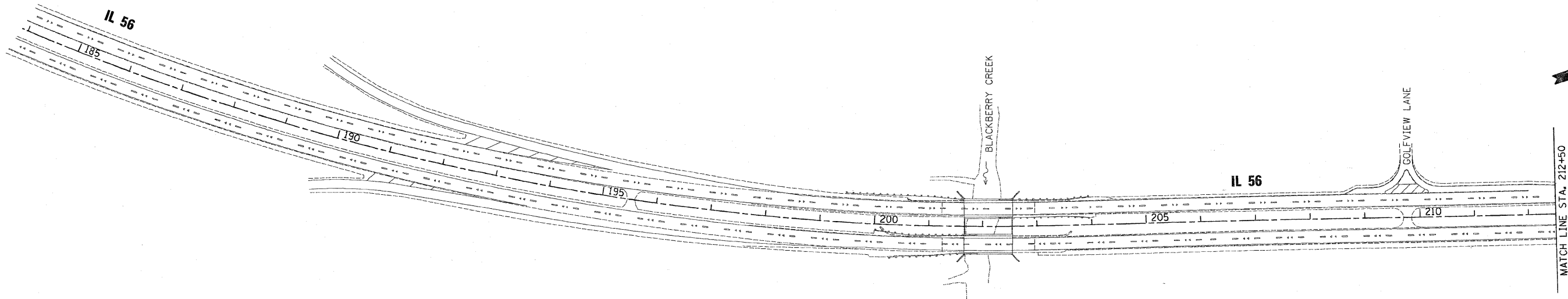
NOTES:

TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED IN ACCORDANCE WITH ARTICLE 280.04 (f) OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

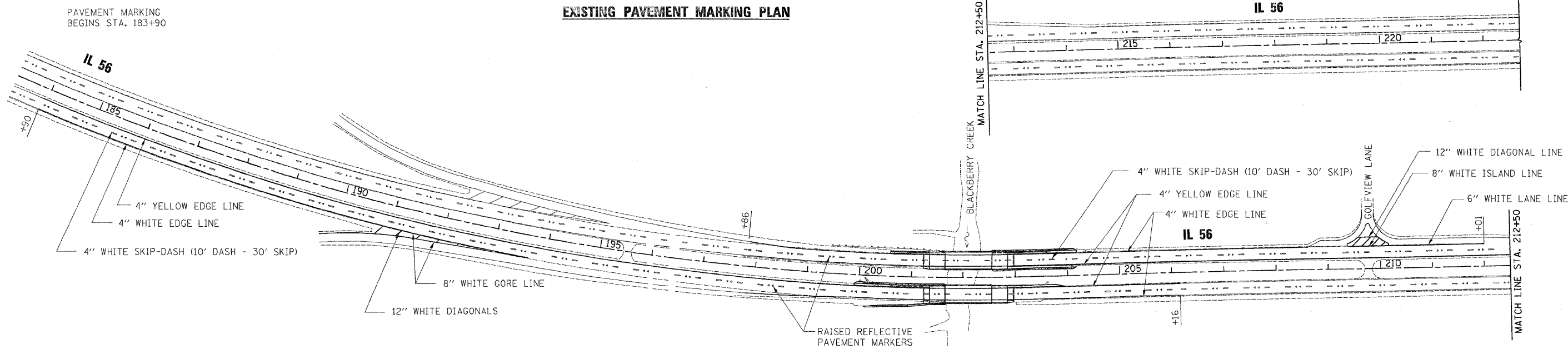
ALL EARTH SURFACES EXPOSED AS A RESULT OF THE CONTRACTORS OPERATIONS, SHALL BE FINAL GRADED, SEEDED AND COVERED WITH EROSION BLANKET AS SOON AS PRACTICAL AFTER CONSTRUCTION ACTIVITIES IN THAT AREA HAVE CONCLUDED.

FILE NAME = k:\11182508\cod\design\182508.ec.dgn	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK EROSION CONTROL PLAN</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLDT SCALE = 1:20	CHECKED - SPF	REVISED -	573			61 B-BR-1	KANE	37	10	
PLDT DATE = 6/12/2009	DATE - APRIL 02, 2009	REVISED -	CONTRACT NO. 62817							
						FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
				SCALE:	SHEET NO. OF SHEETS	STA. TO STA.				





**EXISTING PAVEMENT MARKING PLAN**



**PROPOSED PAVEMENT MARKING PLAN**

NOTES:  
 ALL PAVEMENT MARKING  
 SHALL BE POLYUREA - TYPE I

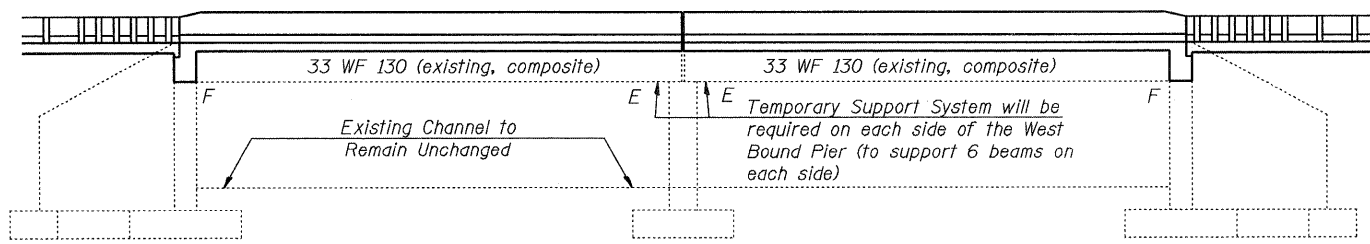
FILE NAME = ks\1182508\cad\design\182508.ctb	USER NAME = \$USER\$	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK EXISTING AND PROPOSED PAVEMENT MARKING PLAN</b>			F.A.P. RTE. 573	SECTION 61 B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 11
PLOT SCALE = 1:100	CHECKED - SPF	DRAWN - JTT	REVISED -		SCALE: 1" = 100'	SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 62817	
PLOT DATE = 6/12/2009	DATE - APRIL 02, 2009	CHECKED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							



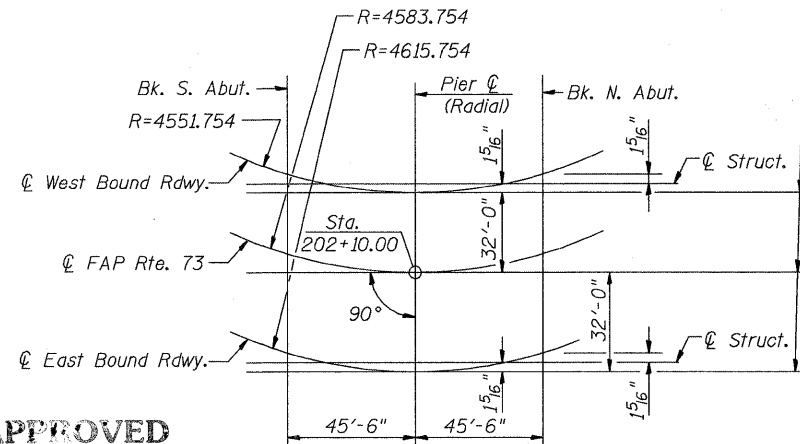
Bench Mark: Chiseled square on northeast wingwall of East Bound Structure.  
Elev. 682.06

Existing Structures: No. 045-0026 (EB) & No. 045-0027 (WB),  
built as FA Rte. 131, Sec. 61 B in 1958.  
The Superstructures consist of a R.C. deck  
91'-0" long by 35'-8" wide supported on two  
simply supported wide flange beam spans.  
Decks to be removed and replaced using  
stage construction.

No Salvage.



**ELEVATION**



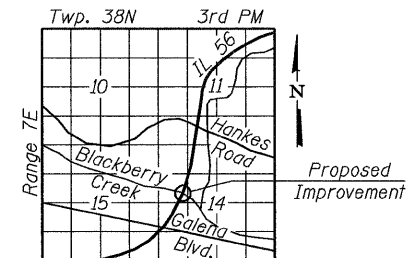
**OFFSET SKETCH**

Local Tangent  
Sta. 202+10.00

**CURVE DATA**  
 $\Delta = 74^\circ-46'-20''$   
 $D = 1^\circ-15'-0''$   
 $T = 3502.784'$   
 $L = 5981.778'$   
 $E = 1185.154'$   
 $R = 4583.754'$   
P.C. Sta. 143+51.33  
P.I. Sta. 178+54.11  
P.T. Sta. 203+33.11  
S.E. = 2.0 %  
S.E. Removed  
from Sta. 202+73.99  
to Sta. 203+92.22

**APPROVED**  
FOR STRUCTURAL ADEQUACY ONLY

Ralph E. Anderson  
ENGINEER OF BRIDGES AND STRUCTURES



**LOCATION SKETCH**

STATION 202+10  
REBUILT 2008 BY  
STATE OF ILLINOIS  
F.A.P. RT. 573 SEC. 61B-BR-1  
LOADING HS20  
STR. NO. 045-0026

**NAME PLATE 1**  
See Std. 515001

STATION 202+10  
REBUILT 2008 BY  
STATE OF ILLINOIS  
F.A.P. RT. 573 SEC. 61B-BR-1  
LOADING HS20  
STR. NO. 045-0027

**NAME PLATE 2**  
See Std. 515001

Note: The existing name plate shall be  
cleaned and relocated adjacent to the  
new name plate. Cost included with  
Name Plates.

**LOADING HS20-44**  
Allow 50#/sq. ft. for future wearing surface.

**DESIGN SPECIFICATIONS**  
2002 AASHTO

**DESIGN STRESSES**

**FIELD UNITS**  
 $f_c = 3,500$  psi  
 $f_y = 60,000$  psi (reinforcement)  
 $f_y = 33,000$  psi (Existing)

**SEISMIC DATA**

Seismic Performance Category (SPC) = A  
Bedrock Acceleration Coefficient (A) = .04g  
Site Coefficient (S) = 1.0

**WATERWAY INFORMATION**

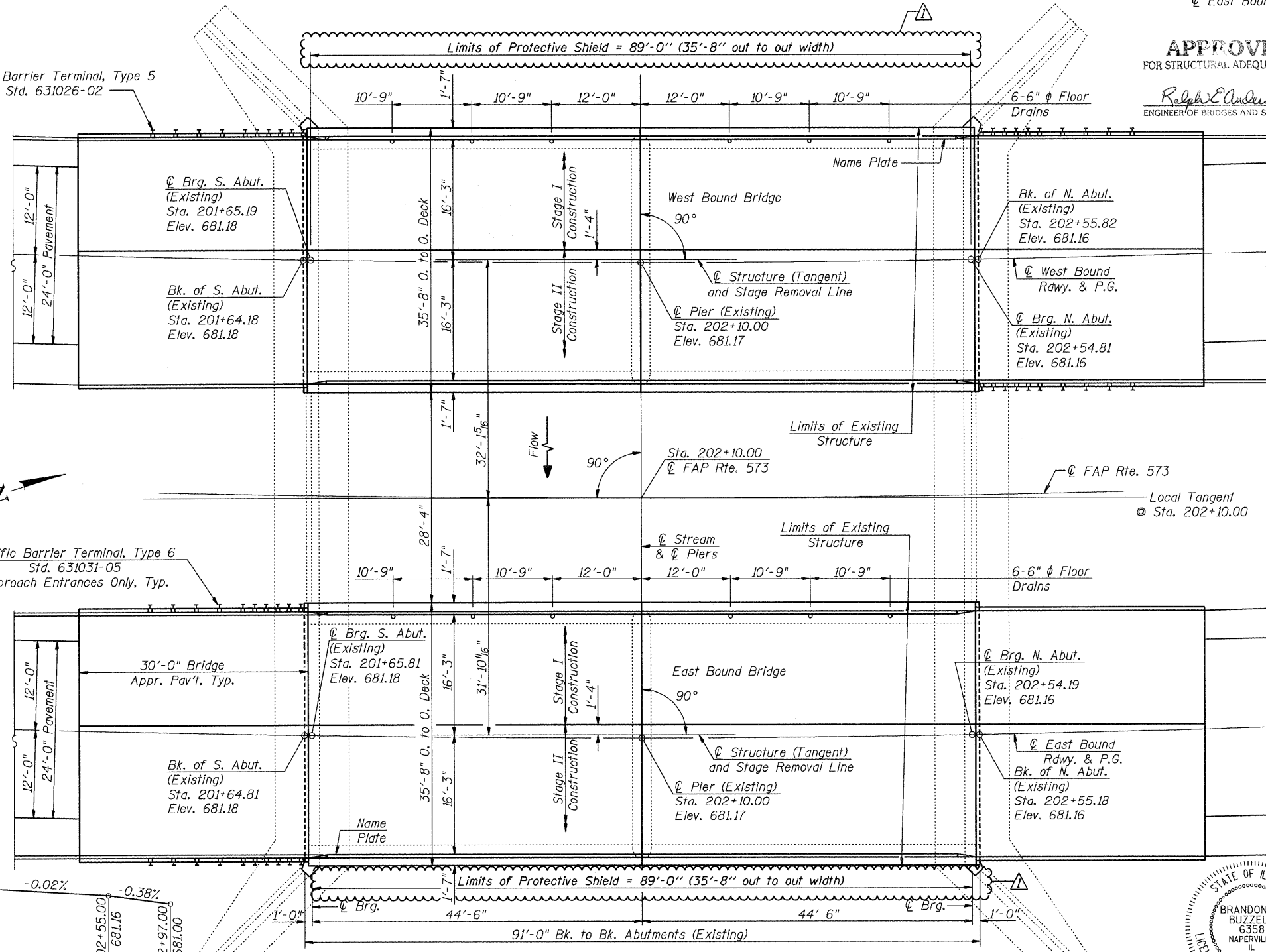
Drainage Area = 31 sq. mi. Low Grade Elev. 676.0' @ Sta. 229+00

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Nat. H.W.E.		Head - Ft.		Headwater El.	
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	Exist.	Prop.
Design	10	1560	668.3	668.3	677.70	0.26	0.26	677.96	677.96	
Base	50	2496	657.4	657.4	678.94	0.36	0.36	679.30	679.30	
Overtopping	100	3120	657.4	657.4	679.50	0.40	0.40	679.90	679.90	
Max. Calc.	1.15	380	379.5	379.5	676.03	0.03	0.03	676.06	676.06	
	500	4305	657.4	657.4	680.71	0.18	0.18	680.89	680.89	

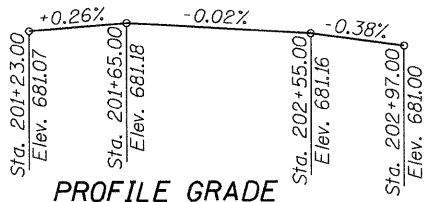
Max. Recorded H.W.E. = +679

Traffic Barrier Terminal, Type 5  
Std. 631026-02

Traffic Barrier Terminal, Type 6  
Std. 631031-05  
Approach Entrances Only, Typ.



**PLAN**



**PROFILE GRADE**  
(along  $\text{\textcircled{C}}$  FAP Rte. 573)

STATE OF ILLINOIS  
BRANDON L. BUTZELL  
6358 NAPERVILLE IL  
LICENSED STRUCTURAL ENGINEER  
Expires 11-30-10

**rjngroup**  
Excellence through Ownership

200 West Front Street  
Wheaton, IL 60187

DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

**GENERAL PLAN AND ELEVATION**  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 1 17 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	573	61B-BR-1	KANE	37	13
CONTRACT NO. 62817					
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

Revised 7/20/09-BLB

7/20/2009 7:20:09 AM Default: K:\118252\Final Plans\VDEN\DUH1-7-20-09.dgn

**GENERAL NOTES**

No field welding is permitted except as specified in the contract documents.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Concrete Sealer shall be applied to the seat area of the West Bound pier.

Cleaning and field painting of structural steel shall be done under a separate painting contract.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

All new structural steel shall be shop painted with an inorganic zinc rich primer per AASHTO M 300, Type 1.

Slipforming of the parapets is not allowed.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	20.8	7.9	28.7
Removal of Existing Concrete Deck	Each	2		2
Floor Drains	Each	12		12
Preformed Joint Strip Seal	Foot	69		69
Concrete Structures	Cu. Yd.		7.6	7.6
Concrete Superstructure	Cu. Yd.	235.1		235.1
Bridge Deck Grooving	Sq. Yd.	607		607
Protective Coat	Sq. Yd.	800		800
Elastomeric Bearing Assembly, Type I	Each	12		12
Furnishing and Erecting Structural Steel	Pound	1,610		1,610
Stud Shear Connectors	Each	3,168		3,168
Reinforcement Bars, Epoxy Coated	Pound	47,400	710	48,110
Name Plates	Each	2		2
Concrete Sealer	Sq. Ft.		83	83
Bar Splacers	Each	700	6	706
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.		20	20
Jack and Remove Existing Bearings	Each	12		12
Temporary Support System	L Sum		1	1
Anchor Bolts, 1"	Each	60		60
Protective Shield	Sq. Yd.	705		705

**INDEX OF SHEETS**

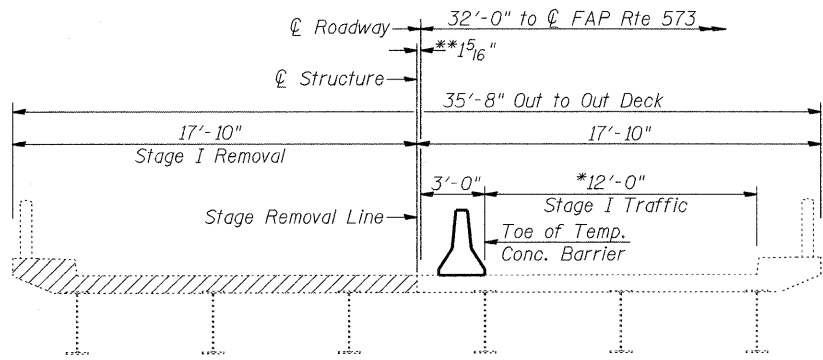
Sheet No.	Description
1	General Plan and Elevation
2	General Notes and Bill of Material
3	Stage Construction Details
4	Concrete Removal Plans and Details
5	Deck Elevations - I
6	Deck Elevations - II
7	Superstructure
8	Superstructure Details
9	Diaphragm Details
10	Preformed Joint Strip Seal
11	Framing Plan
12	Bearing Details - West Bound Pier
13	Bearing Details - East Bound Pier
14	Pier Details (West Bound Structure)
15	Substructure Repair
16	Bar Splicer Assembly Details
17	Temporary Concrete Barrier for Stage Construction

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 200 West Front Street  
 Wheaton, IL 60187  
 DESIGNED: WJV  
 CHECKED: BLB  
 DATE: 04/02/09

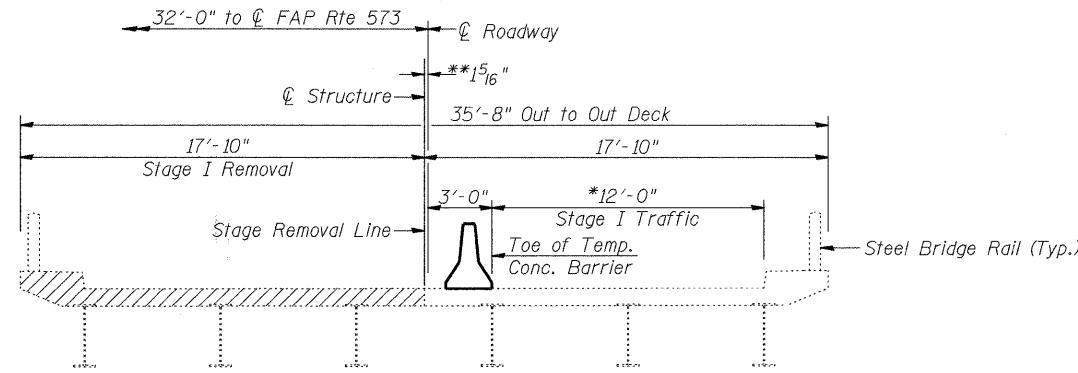
**GENERAL NOTES AND  
 BILL OF MATERIAL  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)**

SHEET NO. 2	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	573	61B-BR-1	KANE	37	14
17 SHEETS	CONTRACT NO. 62817			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

Revised 7/20/09-BLB



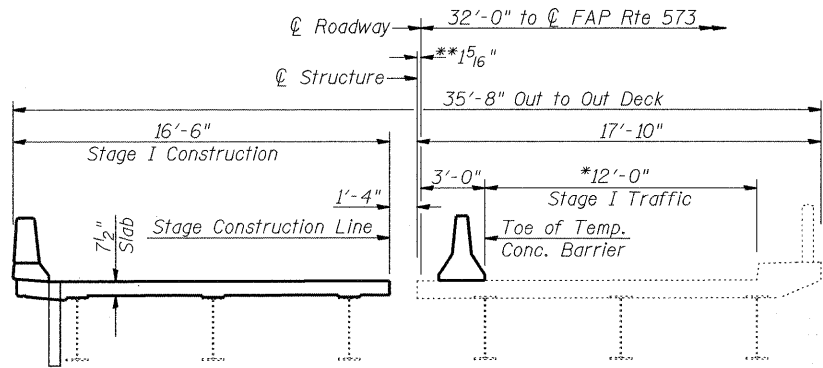
**STAGE I REMOVAL (WEST BOUND LANES)**  
(Looking North)



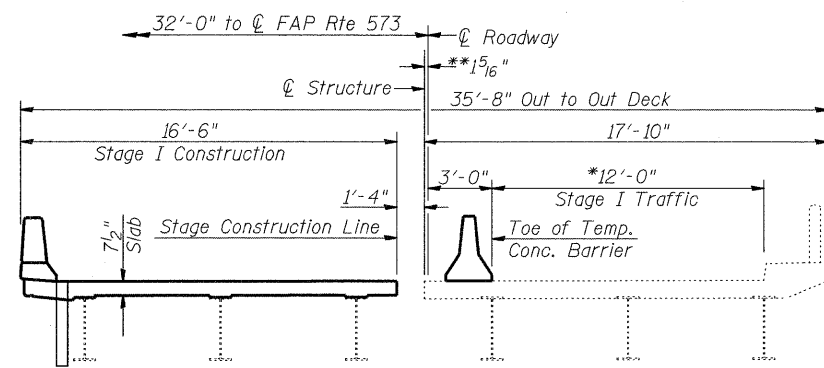
**STAGE I REMOVAL (EAST BOUND LANES)**  
(Looking North)

**LEGEND**

- Denotes "Removal of Existing Concrete Deck"



**STAGE I CONSTRUCTION (WEST BOUND LANES)**  
(Looking North)



**STAGE I CONSTRUCTION (EAST BOUND LANES)**  
(Looking North)

**NOTES**

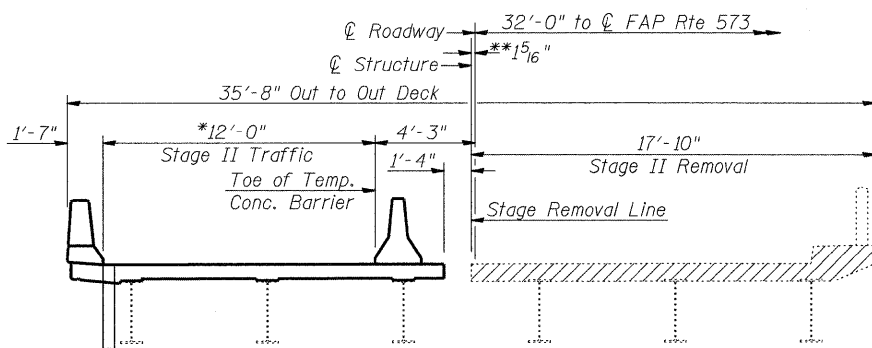
- Dimension lines shown from  $\text{C}$  Roadway are measured radially.
- Removal of existing Steel Bridge Rail is included with Removal of Existing Concrete Deck.
- Pay Item for Temporary Concrete Barrier is included with Roadway Plans.
- See Sheet 17 of 17 for Temporary Concrete Barrier Details.

**SCOPE OF WORK**

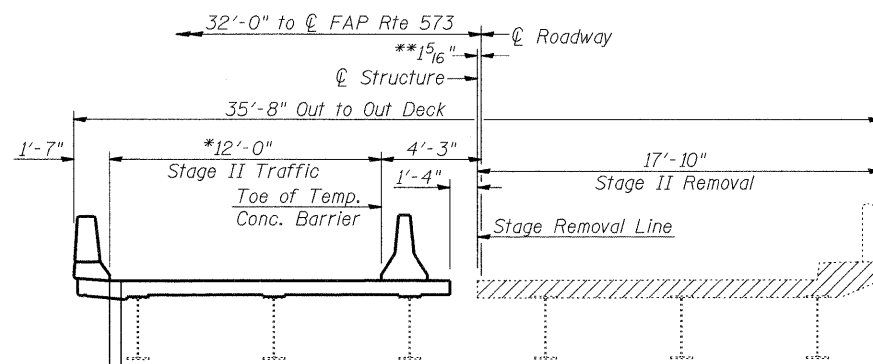
- Remove and replace existing deck utilizing stage construction.
- Remove and replace west bound pier cap.
- Install concrete diaphragms with existing steel diaphragms encased in the concrete at each pier. Also replace the expansion joints at each pier.
- Remove and replace the concrete diaphragms/curtain walls at the abutments.
- Remove expansion bearings and replace with elastomeric bearings at both piers.
- Substructure repairs.
- Install stud shear connectors on existing beams.
- All existing structural steel shall be cleaned where it is going to be encased in concrete.
- Remove and replace Bridge Approach Pavements.

\* Varies  $\pm 1\frac{5}{16}$ "; dimensions to the west of the  $\text{C}$  of Roadway will be an additional  $1\frac{5}{16}$ " at the  $\text{C}$  of the structure and will be  $1\frac{5}{16}$ " less at both ends of the structure. Dimensions to the east of the  $\text{C}$  of Roadway will be an additional  $1\frac{5}{16}$ " at both ends of the structure and will be  $1\frac{5}{16}$ " less at the  $\text{C}$  of the structure.

\*\* Varies from  $1\frac{5}{16}$ " west of  $\text{C}$  Roadway at the  $\text{C}$  of Structure to  $1\frac{5}{16}$ " east of  $\text{C}$  roadway at both ends of the structure.



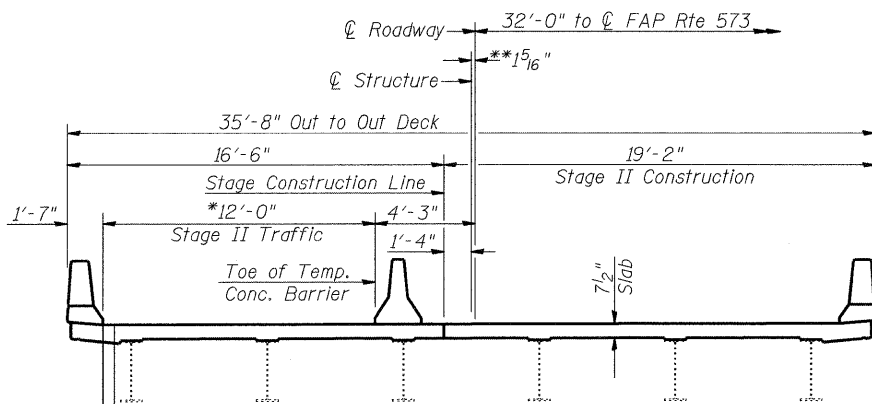
**STAGE II REMOVAL (WEST BOUND LANES)**  
(Looking North)



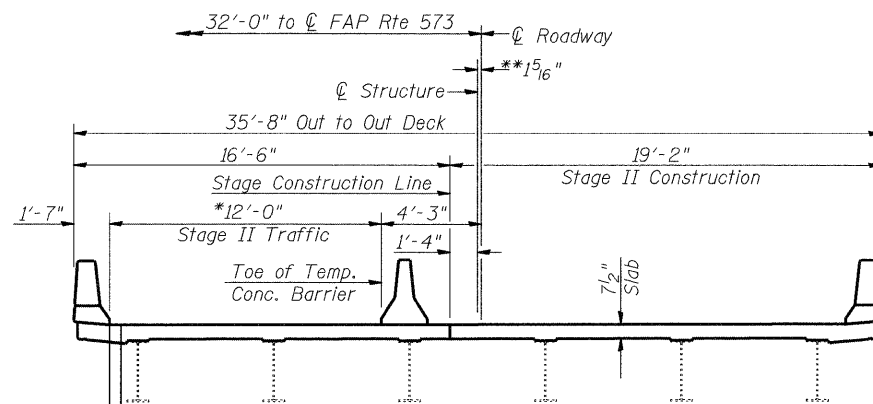
**STAGE II REMOVAL (EAST BOUND LANES)**  
(Looking North)

**BILL OF MATERIAL**

Item	Unit	Total
Removal of Existing Concrete Deck	Each	2



**STAGE II CONSTRUCTION (WEST BOUND LANES)**  
(Looking North)



**STAGE II CONSTRUCTION (EAST BOUND LANES)**  
(Looking North)

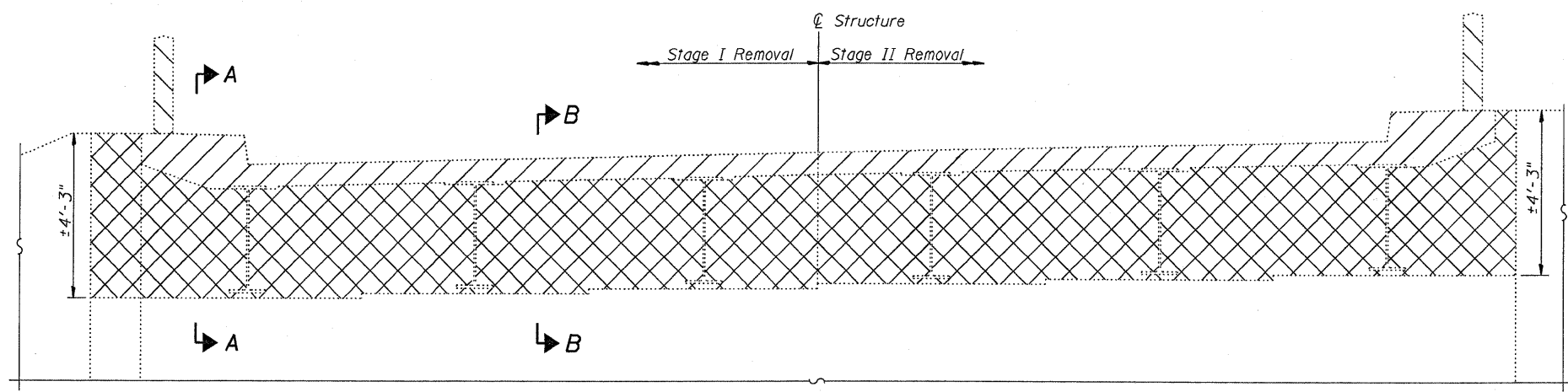
**rjngroup**  
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200 West Front Street  
Wheaton, IL 60187

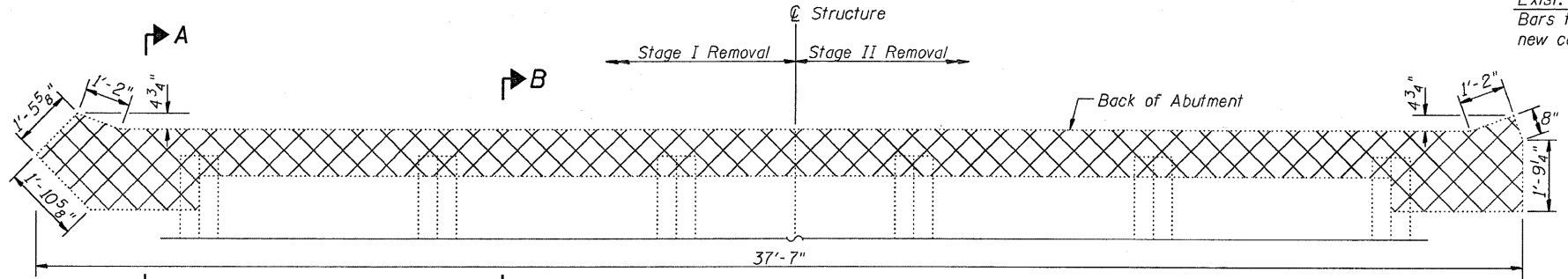
DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

**STAGE CONSTRUCTION DETAILS**  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)

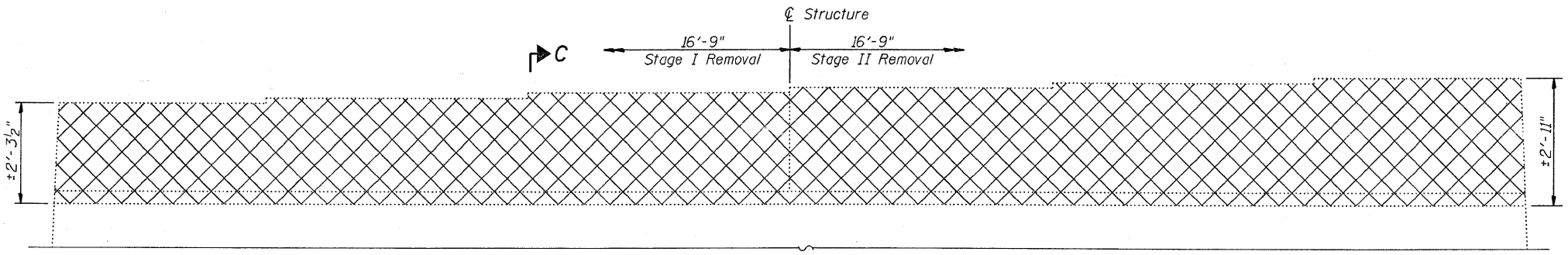
SHEET NO. 3 17 SHEETS	F.A.P. RTE. 573	SECTION 61B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 15
	FED. ROAD DIST. NO. 7 ILLINOIS			CONTRACT NO. 62817 FED. AID PROJECT	



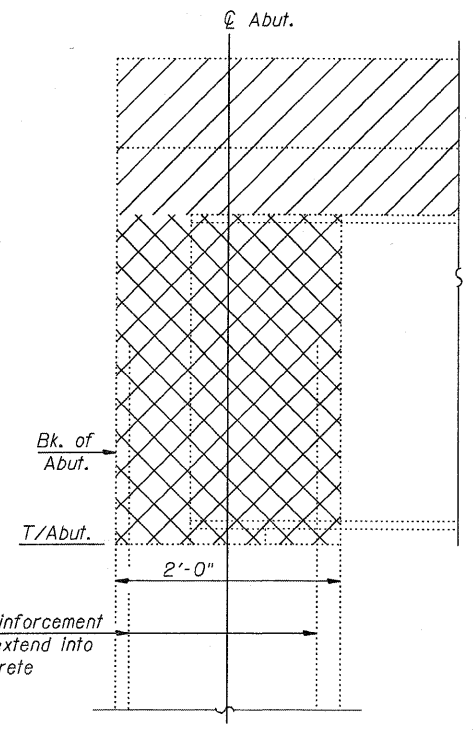
**BACK WALL ELEVATION**  
 North wall (W.B. Structure) shown, South wall (W.B. Structure) same, but mirrored. North wall (E.B. Structure) & South wall (E.B. Structure) same, but opposite hand (4 Total)



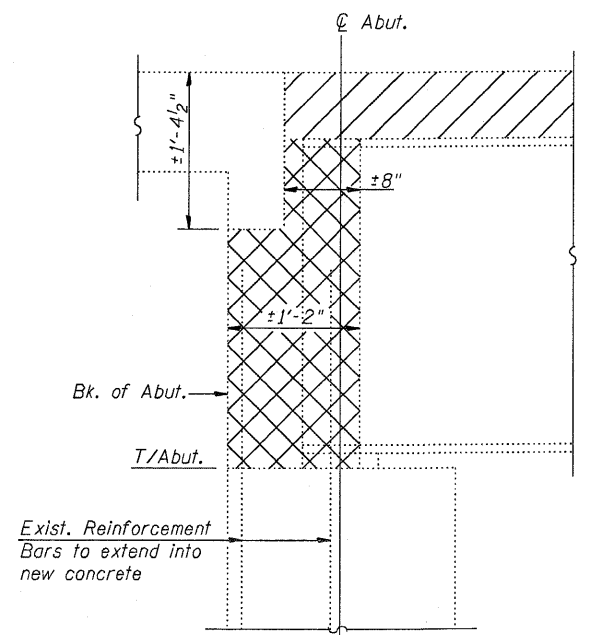
**BACK WALL PLAN**  
 North wall (W.B. Structure) shown, South wall (W.B. Structure) same, but mirrored. North wall (E.B. Structure) & South wall (E.B. Structure) same, but opposite hand (4 Total)



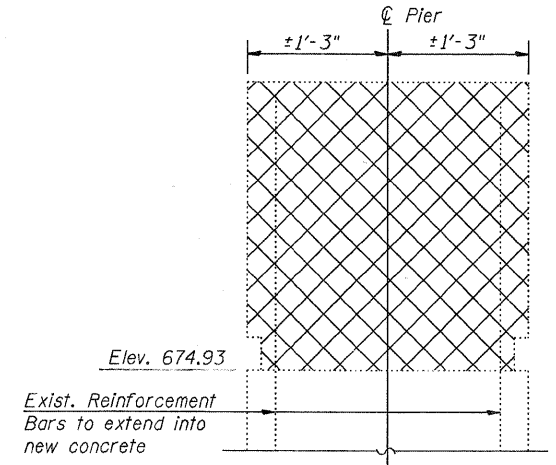
**WEST BOUND PIER ELEVATION**  
 West Bound Pier looking North



**SECTION A-A**



**SECTION B-B**



**SECTION C-C**

**BILL OF MATERIAL**

Item	Unit	Total
Concrete Removal	Cu. Yd.	28.7

- LEGEND**
- Denotes "Concrete Removal".
  - Denotes "Removal of Existing Concrete Deck".
  - Denotes Removal of the Steel Bridge Rail, Cost Included with "Removal of Existing Concrete Deck".

**NOTE:**  
 Steel channels are attached to each end of the pier cap of the westbound pier. These shall be removed after the removal of the existing concrete deck, and before the installation of the Temporary Support System.

Existing steel channels under Stage I traffic shall be left in place until traffic is switched to Stage II and the Stage II bridge deck has been removed.

These steel components shall be removed and disposed of at the Contractor's expense. This work will not be paid for separately, but shall be included in the contract unit price for Concrete Removal.

Existing Reinforcement bars to remain in place and be incorporated into new work shall be blast cleaned to grey metal and straightened. Blast cleaning and straightening shall be included with the cost of "Concrete Removal".

Existing reinforcement bars which have lost 25% or more of their original diameter from either corrosion or construction damage shall be supplemented by new epoxy coated bars, of the same diameter. The bars shall be supplemented by either one of the following two methods:

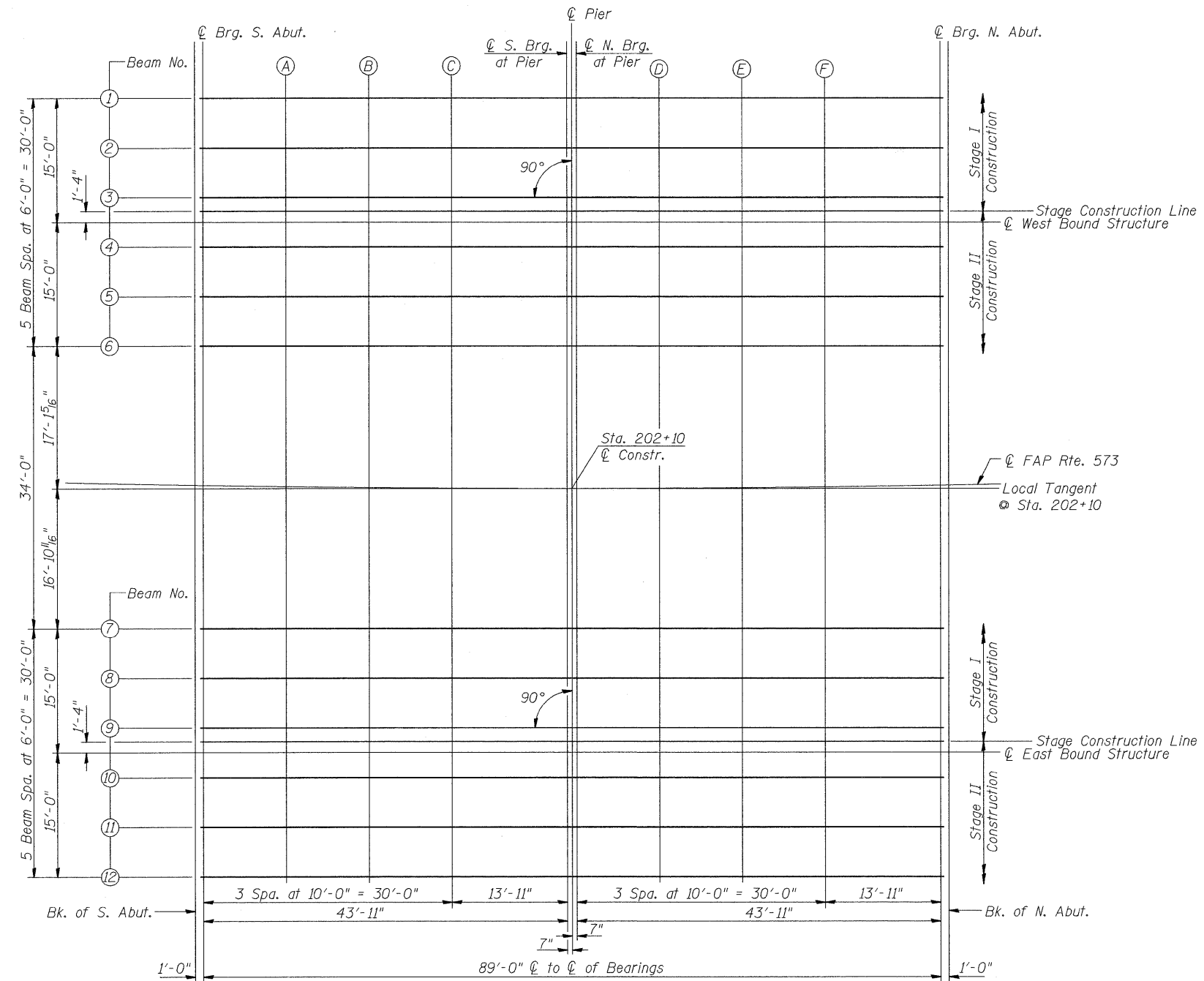
- 1) Drill and grout new bars adjacent to the existing bars according to Article 584 of the Standard Specifications. Drill 1" diameter holes 12" deep into the existing concrete.
- 2) Use of an approved Bar Splicer or Mechanical System. Cost of either method shall be included with the cost of "Concrete Removal".

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 200 West Front Street  
 Wheaton, IL 60187  
 DESIGNED: WJV  
 CHECKED: BLB  
 DATE: 04/02/09

**CONCRETE REMOVAL PLANS AND DETAILS**  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 4	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	573	61B-BR-1	KANE	37	16
17 SHEETS	CONTRACT NO. 62817		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		





**BEAM #1**

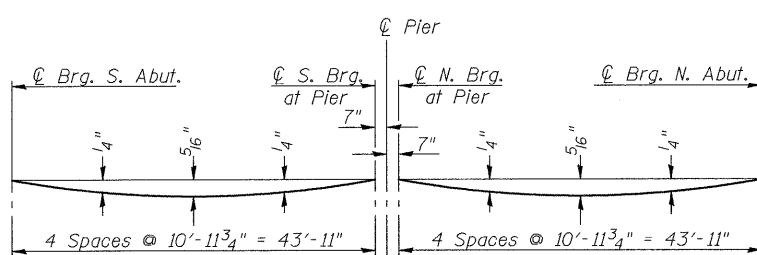
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	201+64.029	-46.882	680.877	680.877
☉ Brg. S. Abut.	201+65.039	-46.892	680.881	680.881
A	201+75.142	-46.979	680.877	680.895
B	201+85.246	-47.044	680.874	680.901
C	201+95.349	-47.087	680.871	680.894
☉ S. Brg. at Pier	202+09.411	-47.110	680.868	680.868
☉ N. Brg. at Pier	202+10.589	-47.110	680.868	680.868
D	202+20.693	-47.098	680.866	680.884
E	202+30.797	-47.063	680.865	680.892
F	202+40.900	-47.007	680.864	680.887
☉ Brg. N. Abut.	202+54.961	-46.892	680.863	680.863
Bk. N. Abut.	202+55.971	-46.882	680.858	680.858

**BEAM #2**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	201+64.090	-40.882	680.998	680.998
☉ Brg. S. Abut.	201+65.099	-40.892	681.001	681.001
A	201+75.188	-40.979	680.997	681.015
B	201+85.279	-41.044	680.994	681.021
C	201+95.369	-41.087	680.991	681.014
☉ S. Brg. at Pier	202+09.412	-41.110	680.988	680.988
☉ N. Brg. at Pier	202+10.588	-41.110	680.988	680.988
D	202+20.679	-41.098	680.986	681.004
E	202+30.769	-41.063	680.985	681.012
F	202+40.859	-41.007	680.984	681.007
☉ Brg. N. Abut.	202+54.901	-40.892	680.983	680.983
Bk. N. Abut.	202+55.910	-40.882	680.978	680.978

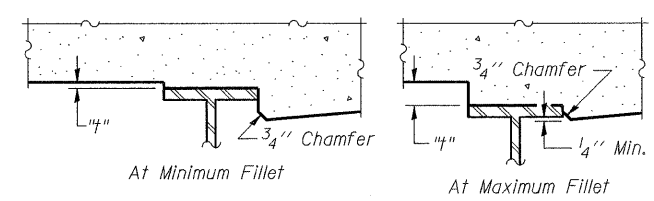
**BEAM #3**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. S. Abut.	201+64.150	-34.882	681.118	681.118
☉ Brg. S. Abut.	201+65.158	-34.892	681.121	681.121
A	201+75.234	-34.979	681.117	681.135
B	201+85.311	-35.044	681.114	681.141
C	201+95.388	-35.087	681.111	681.134
☉ S. Brg. at Pier	202+09.412	-35.110	681.108	681.108
☉ N. Brg. at Pier	202+10.588	-35.110	681.108	681.108
D	202+20.665	-35.098	681.106	681.124
E	202+30.742	-35.063	681.105	681.132
F	202+40.819	-35.007	681.104	681.127
☉ Brg. N. Abut.	202+54.842	-34.892	681.103	681.103
Bk. N. Abut.	202+55.850	-34.882	681.098	681.098



**DEAD LOAD DEFLECTION DIAGRAM**

(Includes weight of concrete only.)  
 Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on this and the following sheet.



**FILLET HEIGHTS**

To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on this and the following sheet, minus slab thickness, equals the fillet heights "t" above top flange of beams.

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 200 West Front Street  
 Wheaton, IL 60187

**DECK ELEVATIONS-I**  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)

DESIGNED: WJV  
 CHECKED: BLB  
 DATE: 04/02/09

SHEET NO. 5	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	573	61B-BR-1	KANE	37	17
17 SHEETS			CONTRACT NO. 62817		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

ks:\1182532\Final\_Planes\Deck Elevations.dgn 5/6/2009 10:47:00 AM

WEST BOUND STAGE CONSTRUCTION LINE

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

C WEST BOUND STRUCTURE

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #4

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #5

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #6

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #7

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #8

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #9

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

EAST BOUND STAGE CONSTRUCTION LINE

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

C EAST BOUND STRUCTURE

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #10

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #11

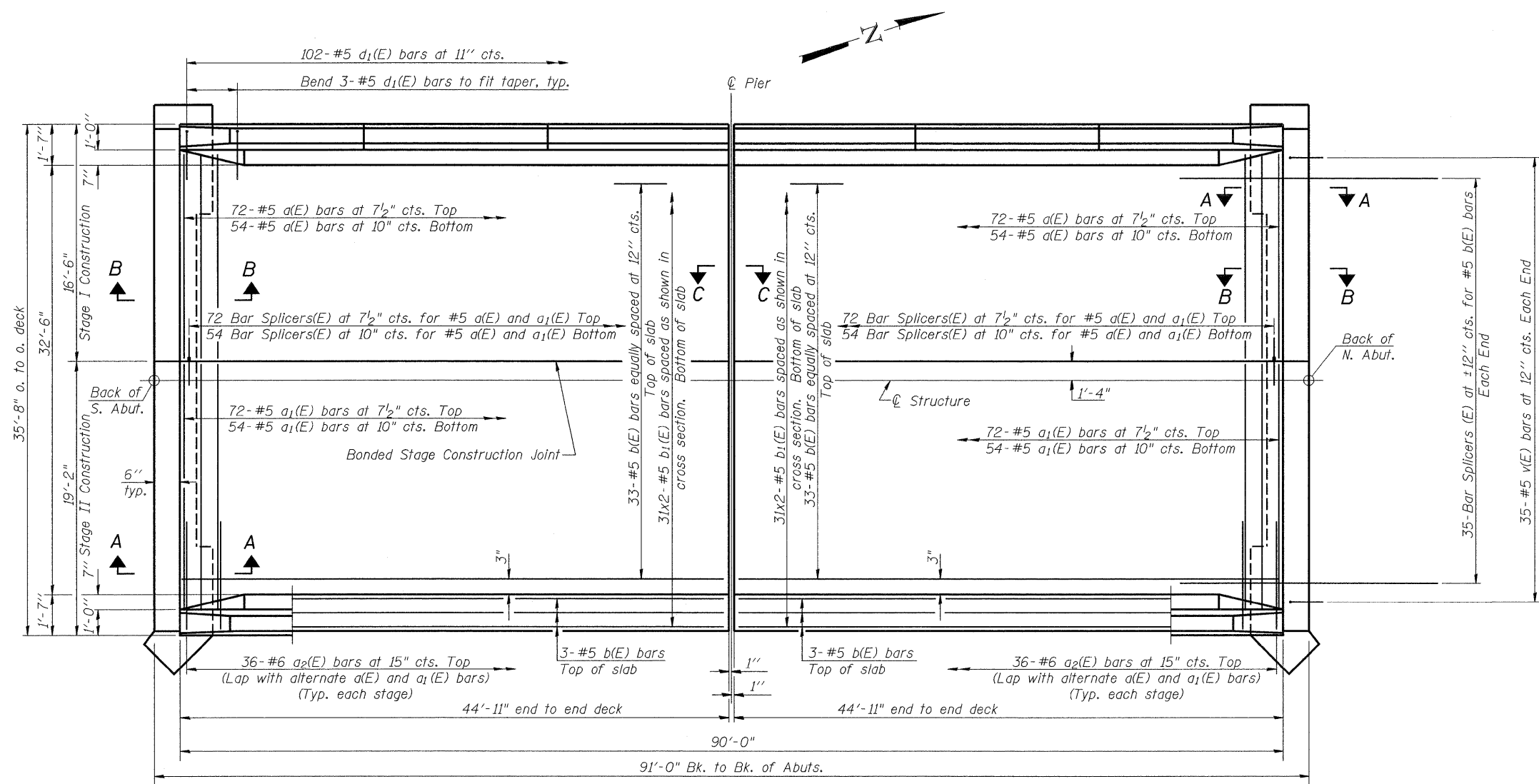
Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

BEAM #12

Table with 5 columns: Location, Station, Offset, Theoretical Grade Elevations, Theoretical Grade Elevations Adjusted For Dead Load Deflection. Rows include Bk. S. Abut., C Brg. S. Abut., C S. Brg. at Pier, C N. Brg. at Pier, C Brg. N. Abut., Bk. N. Abut.

Project information including rjngroup logo, address (200 West Front Street, Wheaton, IL 60187), designer (WJV), checker (BLB), date (04/02/09), sheet number (6 of 17), section (61B-BR-1), county (KANE), total sheets (37), sheet number (18), and contract number (62817).

5/6/2009 11:18:23:21 Final plans\Desk Elevations.dgn

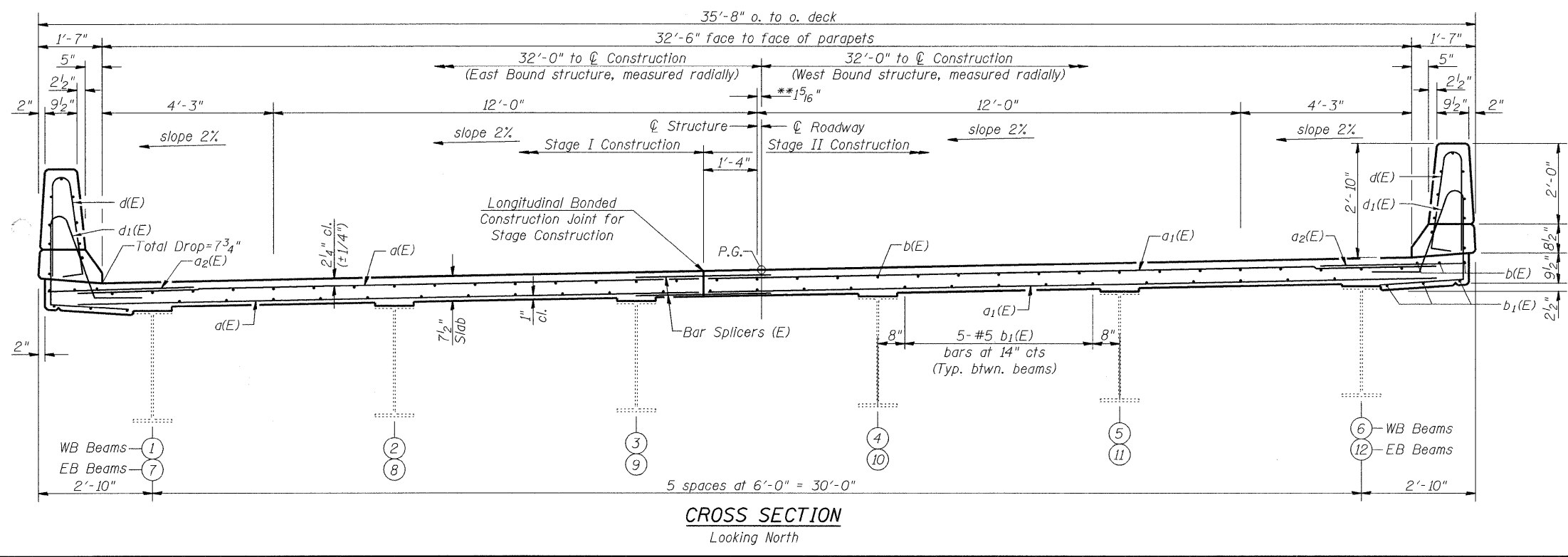


**PLAN**  
 (East Bound Structure shown, West Bound structure is the same, but opposite hand)

**NOTES**

See Sheet 8 of 17 for Superstructure Details, Parapet Reinforcement and Bill of Material.  
 Bars indicated thus 31x2-#5 etc. indicates 31 lines of bars with 2 lengths per line.  
 See Sheet 9 of 17 for Diaphragm Details, Section A-A, Section B-B and Section C-C.  
 See Sheet 16 of 17 for Bar Splicer Details.  
 Dimensions are based on a Rolled Rail Strip Seal Joint. If the Contractor elects to use the Welded Rail Strip Seal Joint, deck dimensions may require adjustments to satisfy the details on sheet 10 of 17.  
 See Sheet 1 of 17 for Floor Drain locations.  
 Cut longitudinal reinforcement to clear floor drains.

**MIN. BAR LAP**  
 #5 bar = 1'-8"



**CROSS SECTION**  
 Looking North

\*\* Varies from 1<sup>5</sup>/<sub>16</sub>" west of  $\phi$  Roadway at the  $\phi$  of Structure to 1<sup>5</sup>/<sub>16</sub>" east of  $\phi$  roadway at both ends of the structure.

**rjngroup**  
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200 West Front Street  
 Wheaton, IL 60187

DESIGNED: WJV  
 CHECKED: BLB  
 DATE: 04/02/09

**SUPERSTRUCTURE**  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)

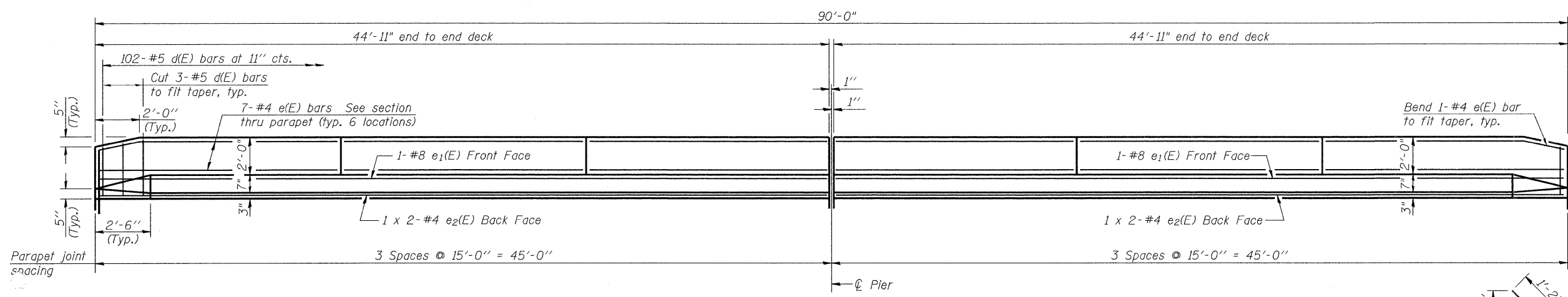
SHEET NO. 7 17 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	573	61B-BR-1	KANE	37	19
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT		
CONTRACT NO. 62817					

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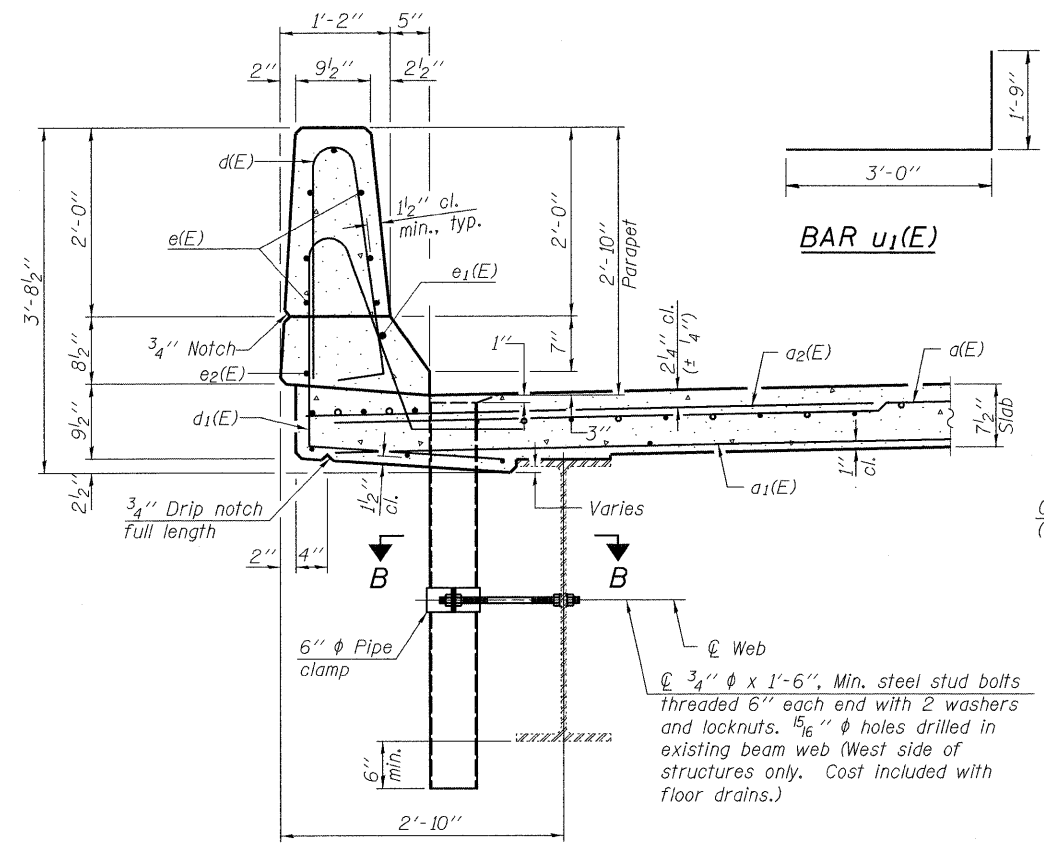
**SUPERSTRUCTURE  
BILL OF MATERIAL  
(E.B. & W.B. STRUCTURES)**

Bar	No.	Size	Length	Shape
a(E)	504	#5	15'-6"	—
a <sub>1</sub> (E)	504	#5	18'-2"	—
a <sub>2</sub> (E)	288	#6	4'-6"	—
b(E)	156	#5	44'-7"	—
b <sub>1</sub> (E)	248	#5	23'-2"	—
d(E)	408	#5	5'-7"	⌋
d <sub>1</sub> (E)	408	#5	7'-7"	⌋
e(E)	168	#4	14'-8"	—
e <sub>1</sub> (E)	8	#8	44'-8"	—
e <sub>2</sub> (E)	16	#4	23'-0"	—
m(E)	40	#6	8'-4"	—
m <sub>1</sub> (E)	16	#6	19'-5"	—
m <sub>2</sub> (E)	16	#6	17'-0"	—
m <sub>3</sub> (E)	8	#6	14'-11"	—
m <sub>4</sub> (E)	8	#6	17'-7"	—
m <sub>5</sub> (E)	96	#6	5'-8"	—
m <sub>6</sub> (E)	24	#6	4'-0"	—
m <sub>7</sub> (E)	8	#6	6'-4"	—
s(E)	116	#5	2'-8"	⌋
s <sub>1</sub> (E)	100	#5	7'-3"	⌋
u(E)	16	#5	5'-5"	⌋
u <sub>1</sub> (E)	16	#5	4'-9"	⌋
v(E)	140	#5	3'-3"	⌋
v <sub>1</sub> (E)	28	#5	3'-6"	⌋
x(E)	216	#5	6'-4"	⌋
Reinforcement Bars, Epoxy Coated		Pound		47,400
Concrete Superstructure		Cu. Yd.		235.1
Bar Splicers		Each		700
Protective Coat		Sq. Yd.		800
Bridge Deck Grooving		Sq. Yd.		607

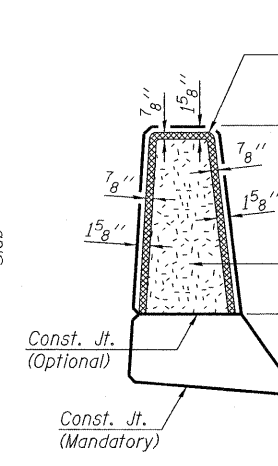
Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.



**INSIDE ELEVATION OF PARAPET  
MIN. BAR LAP  
#4 bar = 1'-4"**



**SECTION THRU PARAPET**



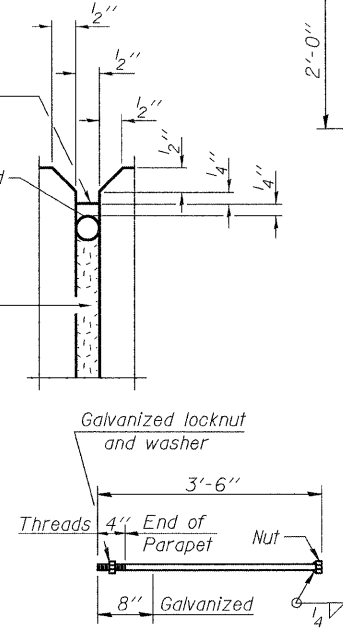
Non-staining gray one component non-sag elastomeric gun grade polyurethane sealant meeting the requirements of ASTM C-920, Type S, Grade NS, Class 25. Use T with a 5/8" backer rod.

1/2" Preformed Self-Expanding Cork Joint Filler according to Article 1051.07 of the Std. Spec. Cost included with Concrete Superstructure.

Const. Jts. at Piers 1/8" Aluminum sheet ASTM B 209 alloy 3003-H14 coated to minimize reaction with wet concrete. Cost included with Concrete Superstructure

**PARAPET JOINT DETAILS**

**Notes:**  
The exterior surfaces of the floor drains shall be painted with the finish coat as specified in the special provisions for Cleaning and Painting New Metal Structures. The exterior surfaces of the drains shall be cleaned according to Society of Protective Coatings Spec. SSPC-SP1 prior to painting.  
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.

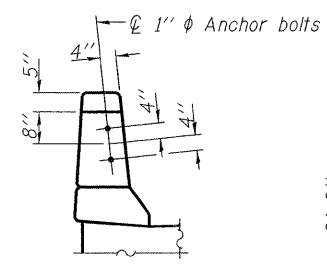


**BAR s(E)**

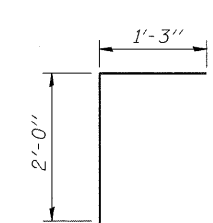
**BAR s1(E)**

**BAR u(E)**

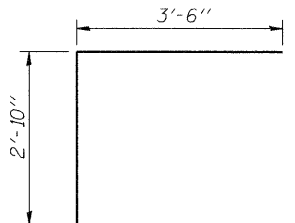
**1" ANCHOR BOLT**  
(Cost included with Concrete Superstructure.)



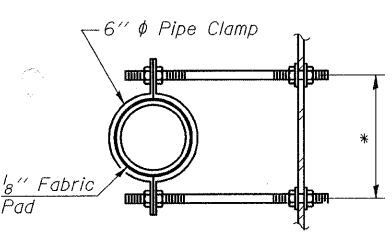
**VIEW C-C**



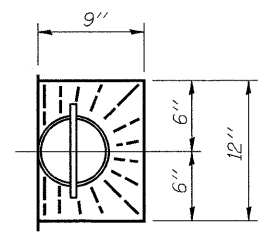
**BAR v(E)**



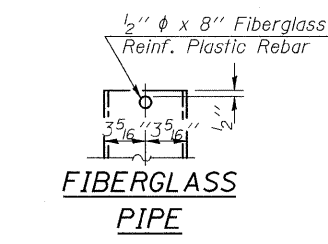
**BAR x(E)**



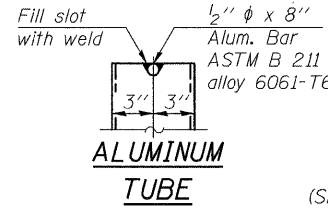
**SECTION B-B**  
\*Dimension as required by Pipe Clamp



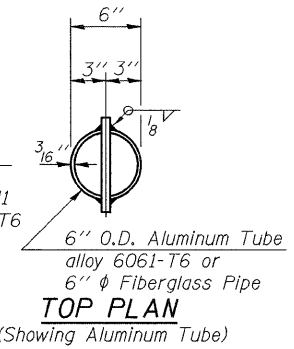
**TOP PLAN**



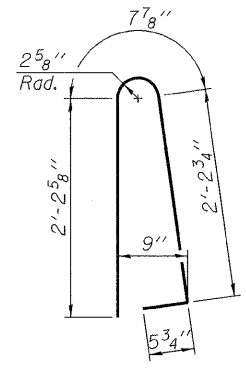
**FIBERGLASS PIPE**



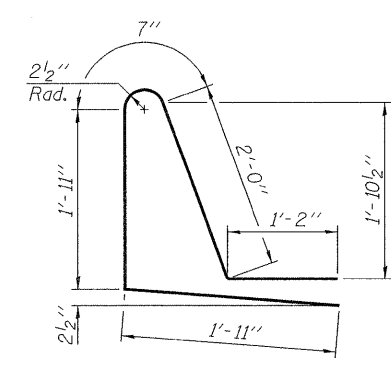
**ALUMINUM TUBE**



**TOP PLAN (Showing Aluminum Tube)**



**BAR d(E)**



**BAR d1(E)**

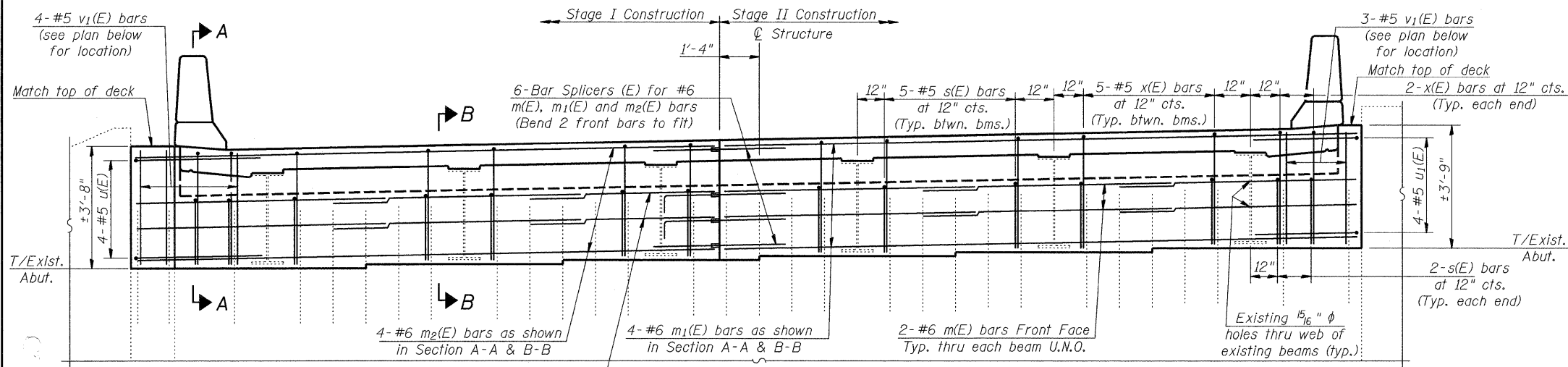
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200 West Front Street  
Wheaton, IL 60187

DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

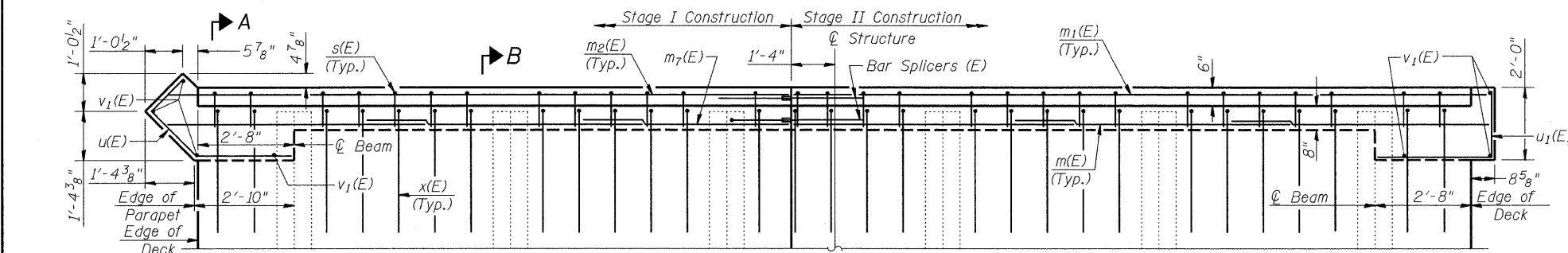
**SUPERSTRUCTURE DETAILS  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)**

SHEET NO. 8	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	573	61B-BR-1	KANE	37	20
17 SHEETS	CONTRACT NO. 62817				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



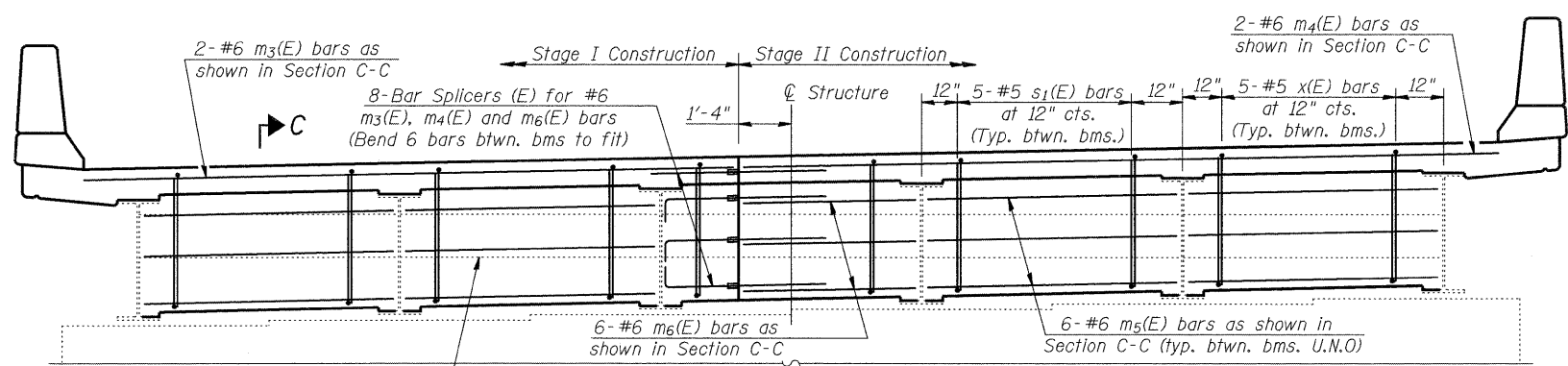
**BACK WALL ELEVATION**

North wall (W.B. Structure) shown, South wall (W.B. Structure) same, but mirrored.  
North wall (E.B. Structure) & South wall (E.B. Structure) same, but opposite hand (4 Total)



**BACK WALL PLAN**

North wall (W.B. Structure) shown, South wall (W.B. Structure) same, but mirrored.  
North wall (E.B. Structure) & South wall (E.B. Structure) same, but opposite hand (4 Total)



**DIAPHRAGM AT PIER ELEVATION**

South Diaphragms shown, looking North.  
North Diaphragms same, but mirrored (4 Total)

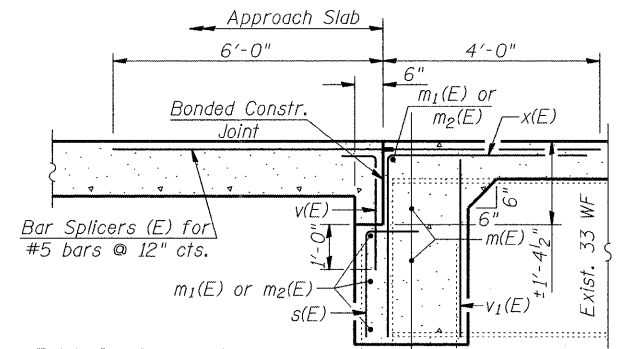
Exist. steel diaphragms to be cleaned according to General Note on sheet 2 of 17, then encased in the concrete diaphragm

**NOTES**

Reinforcement bars in diaphragm are billed with superstructure on sheet 8 of 17.  
Concrete in diaphragm is included with Concrete Superstructure on sheet 8 of 17.  
For details of bars see sheet 8 of 17.  
U.N.O. = Unless Noted Otherwise.  
See Sheet 16 of 17 for Bar Splicer Details.

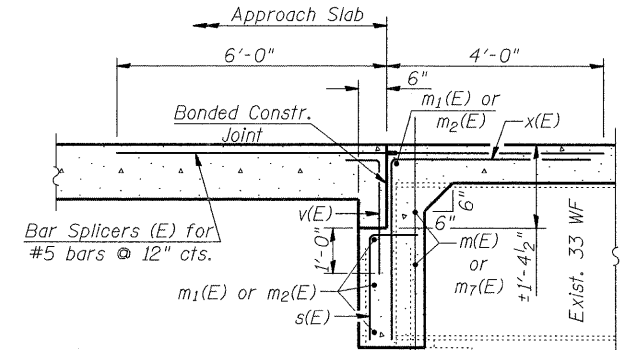
**MIN. BAR LAP**

#6 bar = 2'-9"



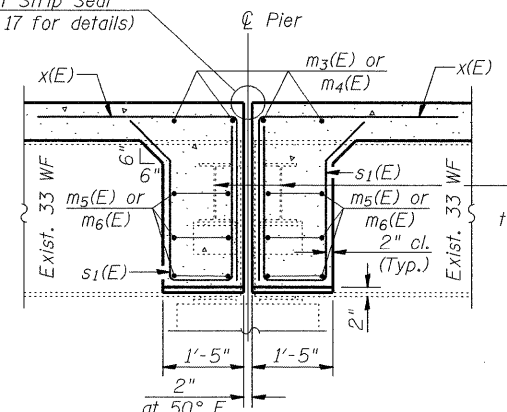
**SECTION A-A**

(Parapet not shown)



**SECTION B-B**

Preformed Joint Strip Seal (See sheet 10 of 17 for details)



**SECTION C-C**

Exist. steel diaphragms to be cleaned according to General Note on sheet 2 of 17, then encased in the concrete diaphragm

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DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

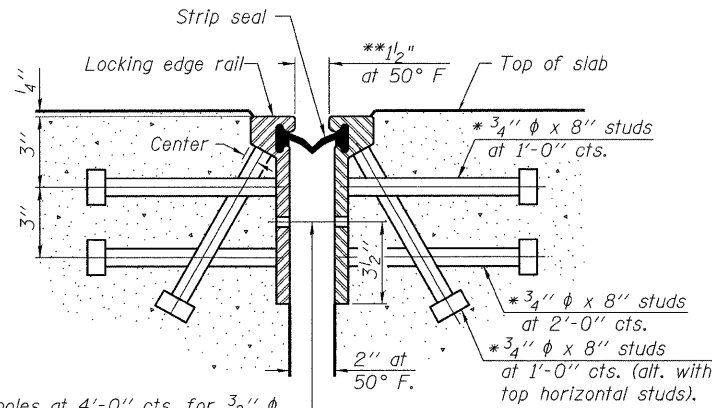
**DIAPHRAGM DETAILS**  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 9 17 SHEETS	F.A.P. RTE. 573	SECTION 61B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 21
	CONTRACT NO. 62817				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

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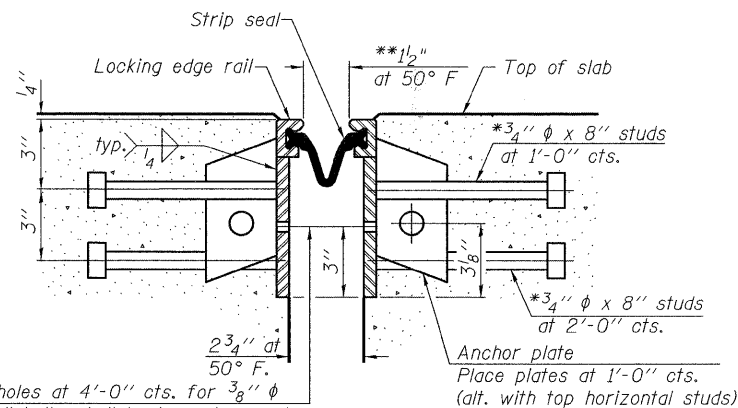
\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

\*\* When joint is fixed, dimension is set at 1 1/2".



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU WELDED RAIL JOINT

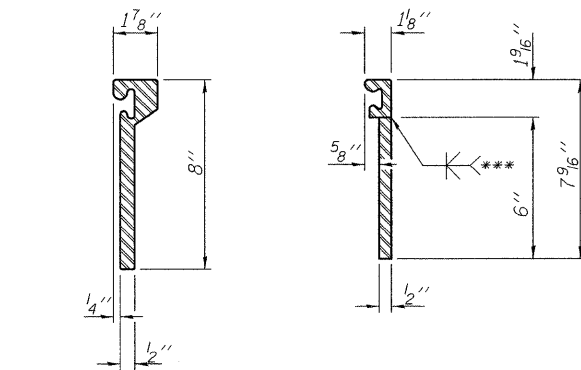
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

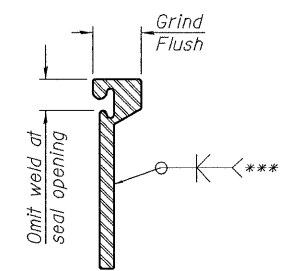
The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



ROLLED EXTRUDED RAIL WELDED RAIL

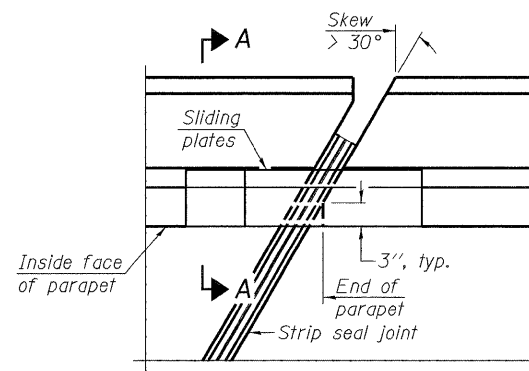


\*\*\*Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL SPLICE

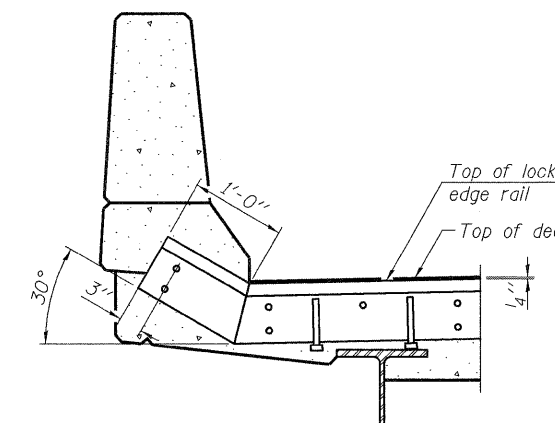
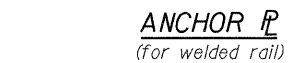
The inside of the locking edge rail groove shall be free of weld residue.

LOCKING EDGE RAILS

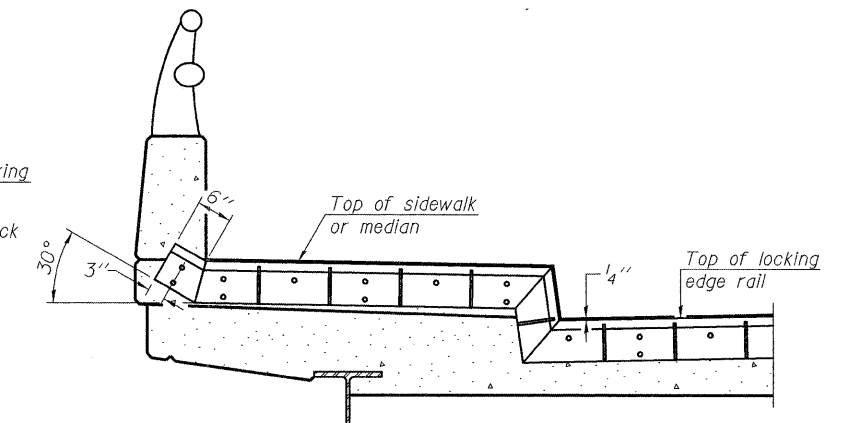


PLAN

ANCHOR PLATE (for welded rail)



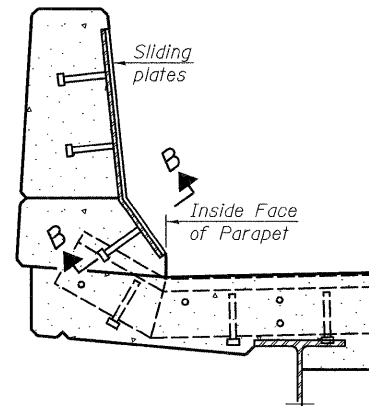
AT PARAPET



AT SIDEWALK OR MEDIAN

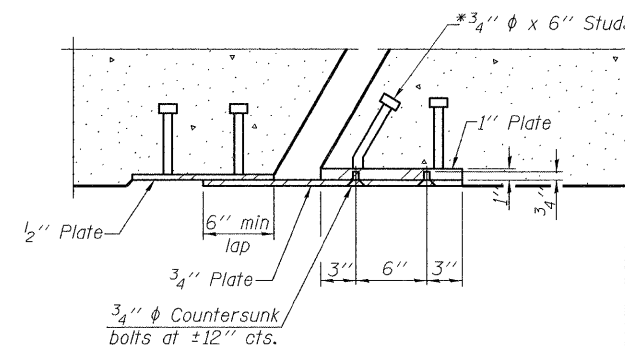
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS



SECTION A-A

POINT BLOCK DETAILS (for skews > 30°)



SECTION B-B

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	69

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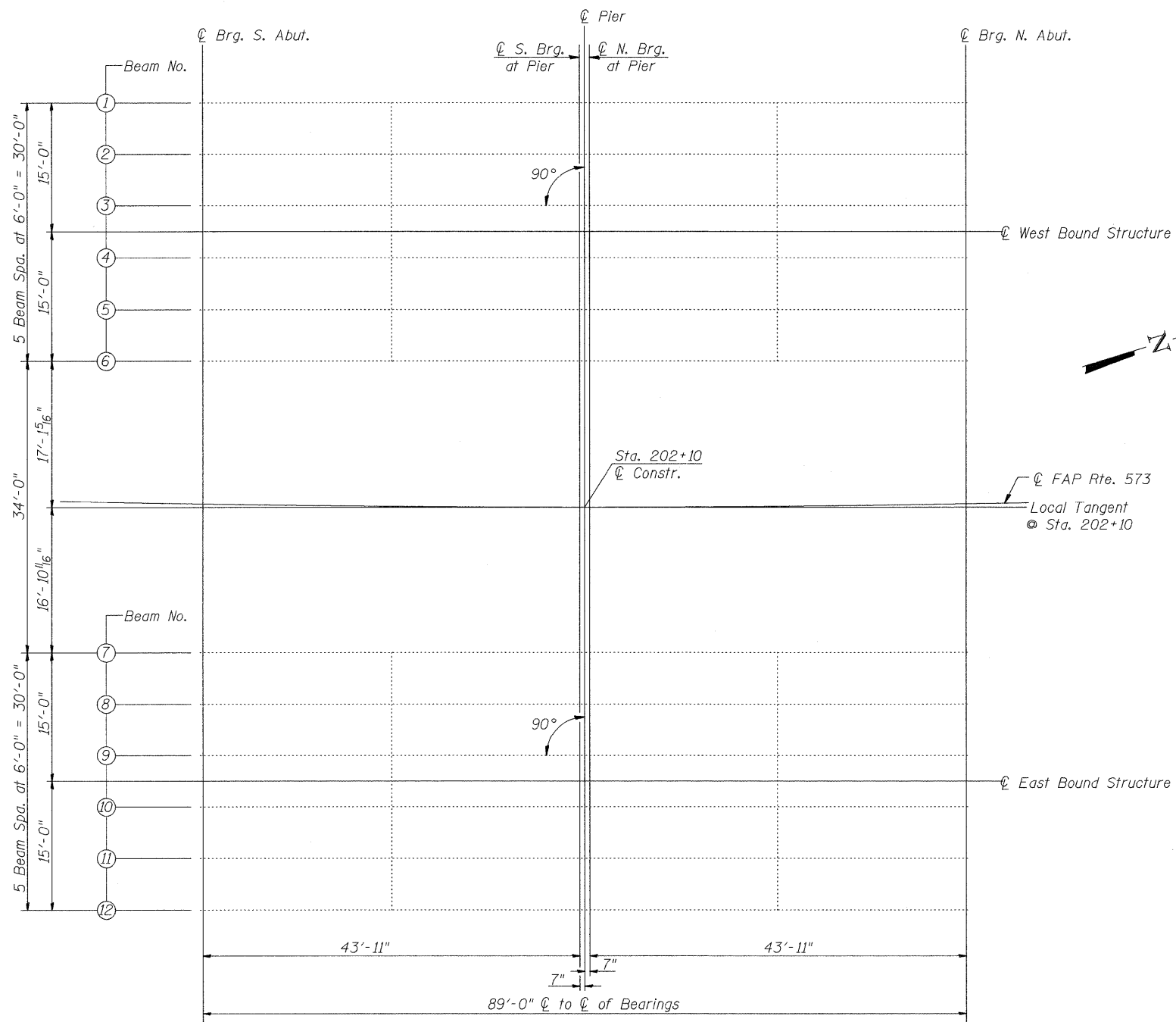
DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

SHEET NO. 10  
17 SHEETS

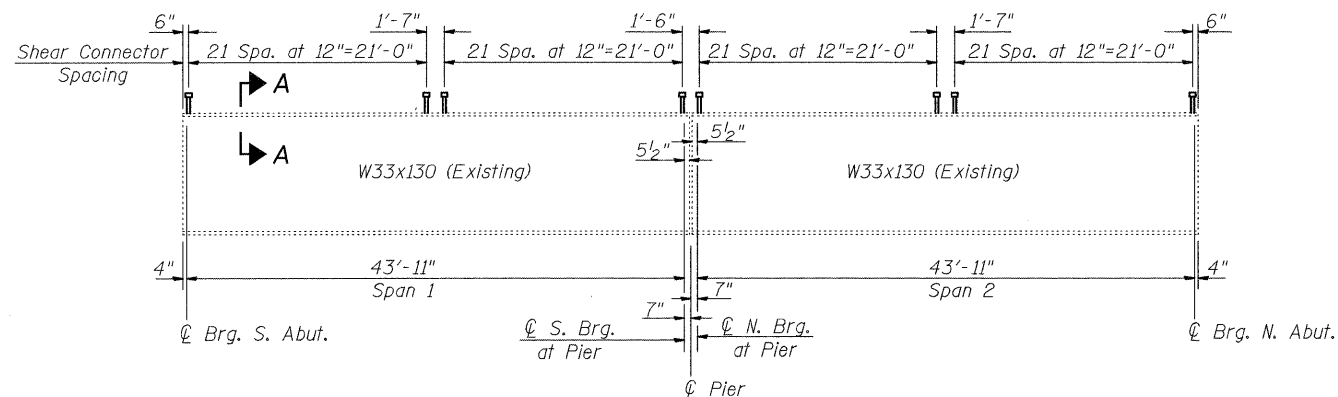
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	61B-BR-1	KANE	37	22
CONTRACT NO. 62817				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

PREFORMED JOINT STRIP SEAL  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)

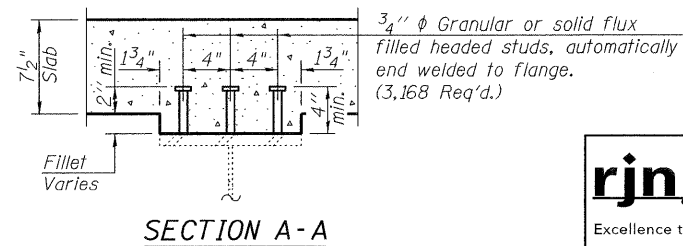




**EXISTING FRAMING PLAN**



**EXISTING BEAM ELEVATION**



**SECTION A-A**

INTERIOR BEAM MOMENT TABLE		
0.5 Sp. 1 & 2		
$I_s$	(in <sup>4</sup> )	6,710
$I_c$ (n)	(in <sup>4</sup> )	17,419
$I_c$ (3n)	(in <sup>4</sup> )	12,637
$S_s$	(in <sup>3</sup> )	406
$S_c$ (n)	(in <sup>3</sup> )	589
$S_c$ (3n)	(in <sup>3</sup> )	529
$Z$	(in <sup>3</sup> )	-
$\bar{p}$	(k/ft.)	0.70
$M\bar{p}$	('k)	169
$s\bar{p}$	(k/ft.)	0.42
$Ms\bar{p}$	('k)	102
$M\bar{t}$	('k)	279
$M$ (Imp)	('k)	82
$^5_3[M\bar{t} + M(\text{Imp})]$	('k)	602
$Ma$	('k)	1135
$Mu$	('k)	2295
$fs\bar{p}$ non-comp	(k.s.i.)	5.0
$fs\bar{p}$ (comp)	(k.s.i.)	2.3
$fs^5_3(\bar{t} + \text{Imp})$	(k.s.i.)	12.3
$fs$ (Overload)	(k.s.i.)	19.6
$fs$ (Total)	(k.s.i.)	-
$VR$	(k)	40.1

INTERIOR BEAM REACTION TABLE		
Abuts. & Pier		
$R\bar{p}$	(k)	24.6
$R\bar{t}$	(k)	34.8
$Imp.$	(k)	10.3
$R$ (Total)	(k)	69.7

$I_s$  and  $S_s$  are the moment of inertia and section modulus of the steel section used in computing  $fs$  (Total & Overload).  
 $I_c(n)$  and  $S_c(n)$  are the moment of inertia and section modulus of the composite section used in computing stresses due to Live Load.  
 $I_c(3n)$  and  $S_c(3n)$  are the moment of inertia and section modulus of the composite section used in computing stresses due to superimposed dead loads. (see AASHTO 10.38)  
 $VR$  is the maximum Live Load + Impact shear range in span.  
 $Z$  is the plastic section modulus used to determine the fully plastic moments in the non-composite areas.  
 $Ma$  (Applied Moment) =  $1.3[M\bar{p} + Ms\bar{p} + ^5_3(M\bar{t} + M(\text{Imp}))]$ .  
The Plastic Moment capacity ( $Mu$ ) is computed according to AASHTO 10.48.1 and 10.50.1.1.  
 $fs$  (Overload) is the sum of the stresses due to  $M\bar{p} + Ms\bar{p} + ^5_3(M\bar{t} + M(\text{Imp}))$ .  
 $fs$  (Total) (Non-compact section) is the sum of the stresses due to  $1.3[M\bar{p} + Ms\bar{p} + ^5_3(M\bar{t} + M(\text{Imp}))]$ .

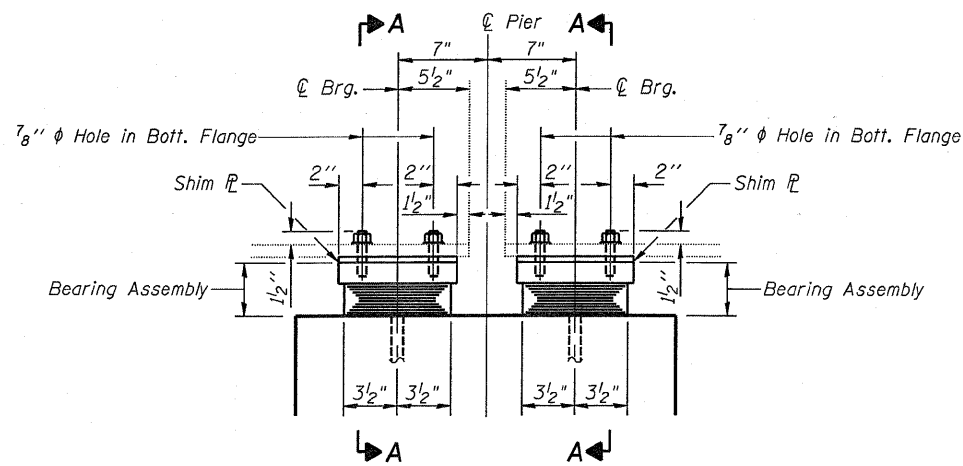
**BILL OF MATERIAL**

Item	Unit	Total
Stud Shear Connectors	Each	3,168

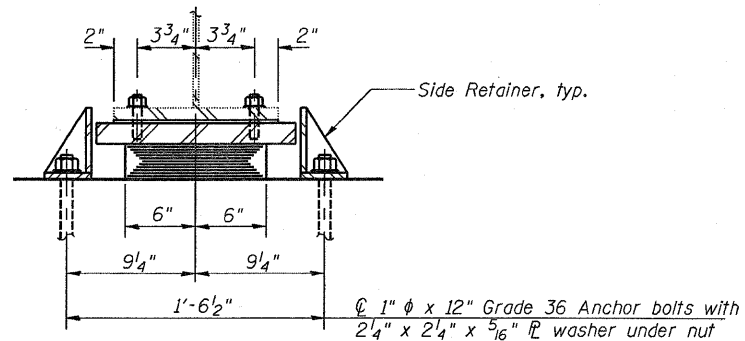
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 Wheaton, IL 60187  
 DESIGNED: WJV  
 CHECKED: BLB  
 DATE: 04/02/09

**FRAMING PLAN**  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)

SHEET NO.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
11	573	61B-BR-1	KANE	37	23
17 SHEETS					
CONTRACT NO. 62817					
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

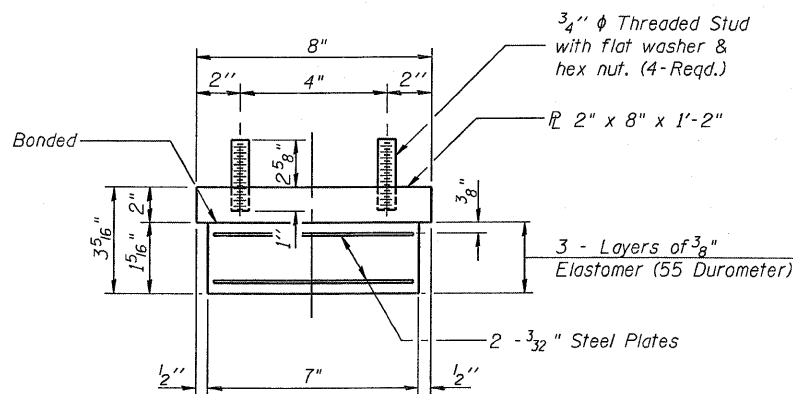


ELEVATION AT PIER (WEST BOUND STRUCTURE)



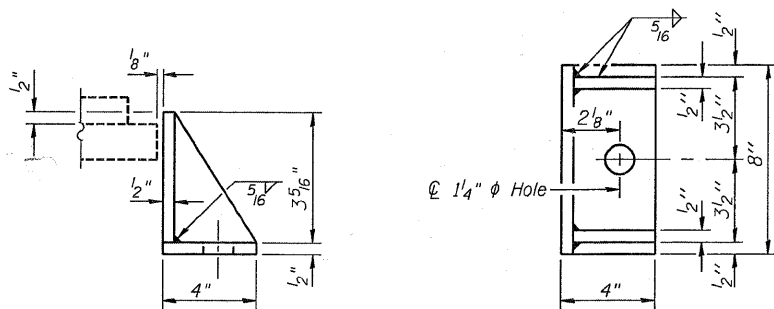
SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.



BEARING ASSEMBLY

Note: Shim plates shall not be placed under Bearing Assembly.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

NOTES

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts for side retainers may be cast in place or installed in holes drilled before or after members are in place.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type I.

7/8" diameter holes in bottom flange for 3/4" studs will be drilled in the field. Cost included with Elastomeric Bearing Assembly, Type I.

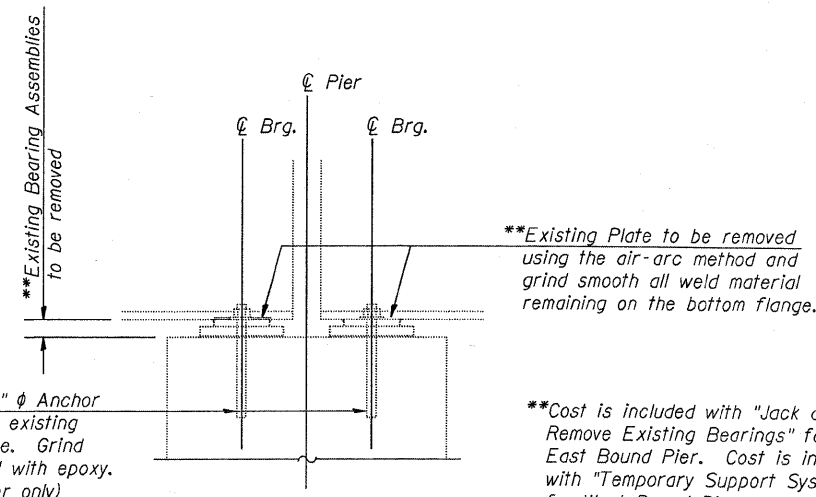
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.

Jacking and Removal of Existing Bearings for the West Bound Bridge shall be included with the Cost for Temporary Support System.

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly, Type I	Each	12
Anchor Bolt, 1"	Each	24
* Temporary Support System	L Sum	1

\*Includes both sides of Westbound Pier.

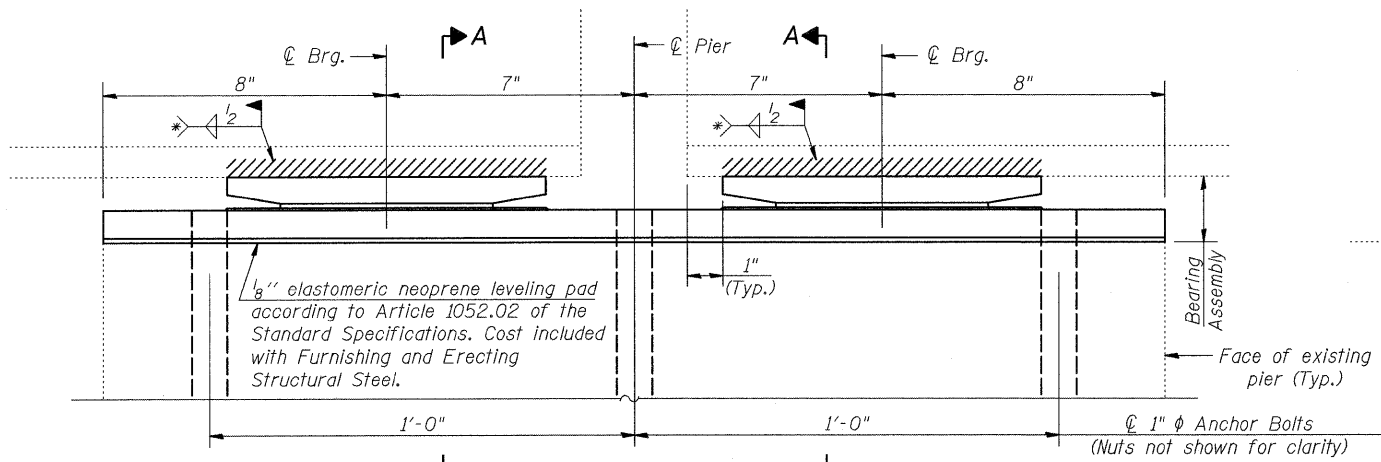


JACK AND REMOVE EXISTING BEARINGS AT PIERS

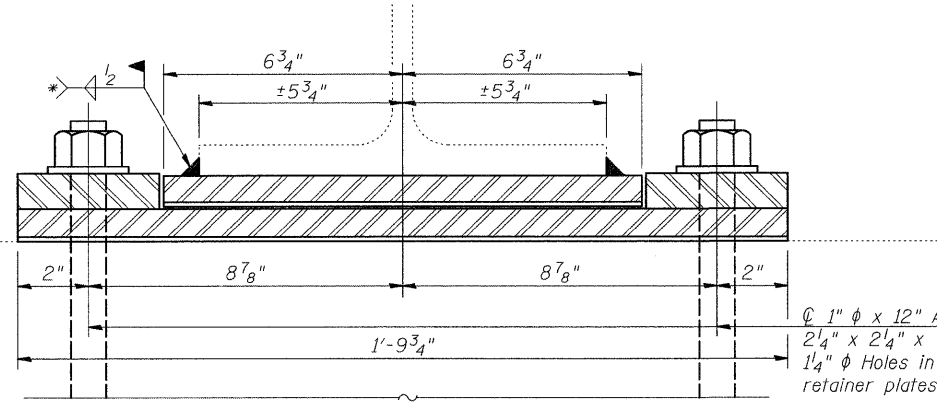
JACKING PROCEDURES

- Prior to commencing any work at the bearings, the Contractor shall submit plans for Jacking for approval by the Engineer.
- Jacking and Removing Existing Bearings shall be done after deck removal is completed and before the new deck is placed.
- Jacking shall be limited so that the maximum lift transversely between adjacent beams is 1/4". See Special Provision for "Jack and Remove Existing Bearings".
- The maximum dead load reaction with the deck removed (per bearing) is 4 kips. Minimum Jack capacity is 8 kips.
- The jacks will be lowered after the top of pier is reconstructed (west bound structure only) and the bearings are installed in place and before the new deck is placed.

<p>Excellence through Ownership</p> <p>200 West Front Street Wheaton, IL 60187</p> <p>DESIGNED: WJV CHECKED: BLB DATE: 04/02/09</p>		<p>BEARING DETAILS - WEST BOUND PIER IL RTE. 56 OVER BLACKBERRY CREEK STATION 202+10.00 STRUCTURE NO. 045-0026 (E.B.) STRUCTURE NO. 045-0027 (W.B.)</p>			
SHEET NO. 12	F.A.P. RTE. 573	SECTION 61B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 24
17 SHEETS	FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 62817			

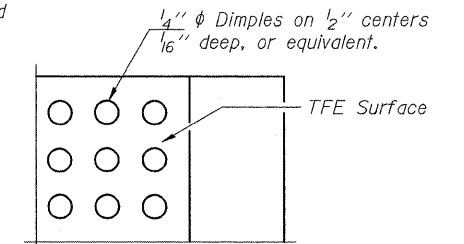


**ELEVATION AT PIER (EAST BOUND STRUCTURE)**  
(Side retainer not shown for clarity)

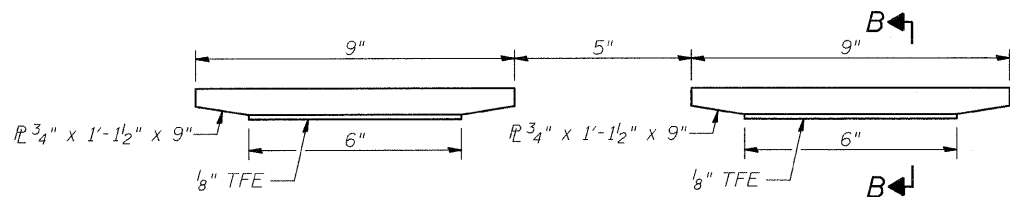


**SECTION A-A**

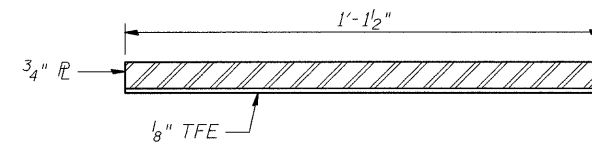
\* Field weld is included with the cost of Jack and Remove Existing Bearings.



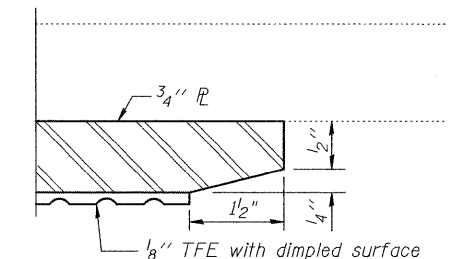
**PLAN-TFE SURFACE**



**ELEVATION**



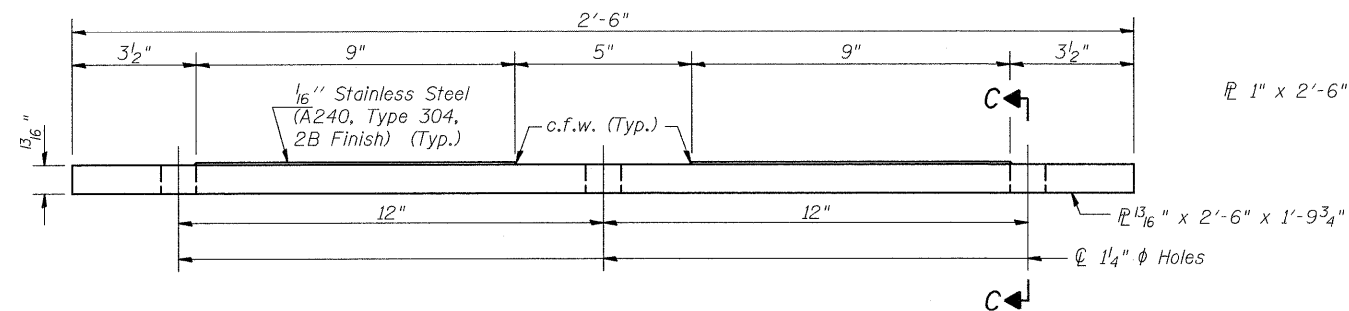
**SECTION B-B**



**SECTION THRU TFE**

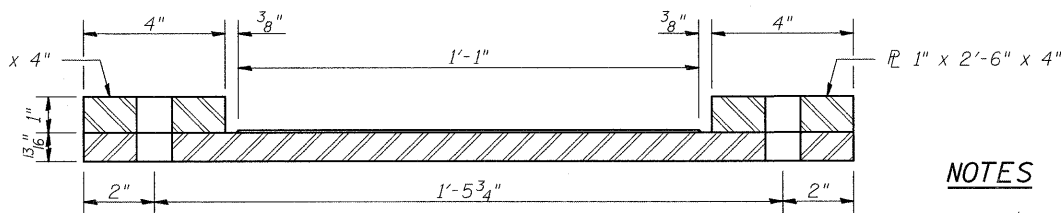
**SPECIAL STEEL BEARING**

**TOP BEARING ASSEMBLY**



**ELEVATION**

Side retainer plates not shown for clarity.



**SECTION C-C**

**BOTTOM BEARING ASSEMBLY**

**NOTES**

The 1/8" TFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.

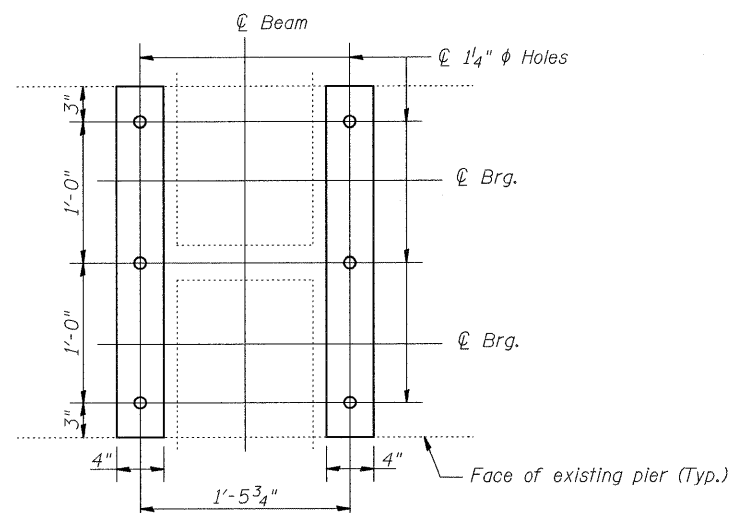
Bearing plates and side retainers shall be included in the weight of Furnishing and Erecting Structural Steel.

1/8" TFE and stainless steel plates will not be paid for separately, but shall be included in the unit bid price for Furnishing and Erecting Structural Steel.

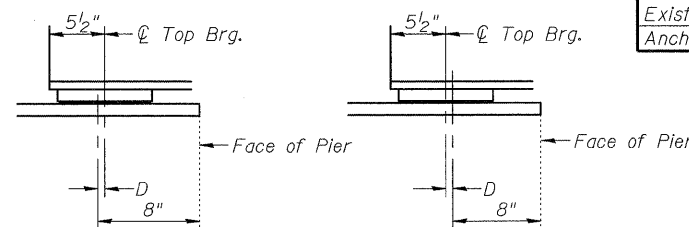
See Sheet 12 of 17 for Jack and Remove Existing Bearings and notes.

**BILL OF MATERIAL**

Item	Unit	Total
Furnishing and Erecting Structural Steel	Pound	1,610
Jack and Remove Existing Bearings	Each	12
Anchor Bolts, 1"	Each	36



**SIDE RETAINERS**



**SETTING ANCHOR BOLTS AT EXP. BRG.**

$D = \frac{1}{8}$ " per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.



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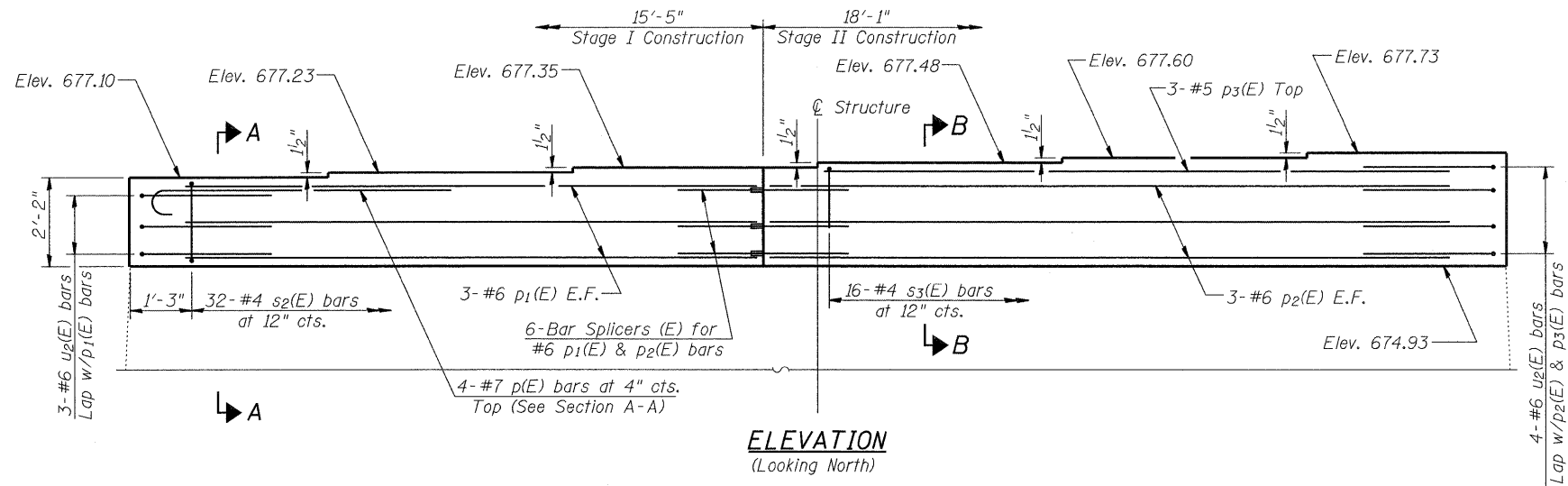
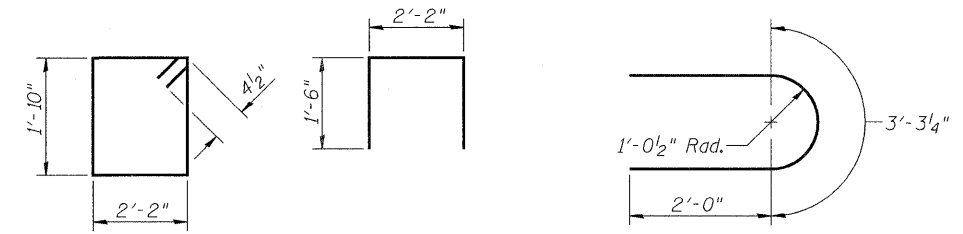
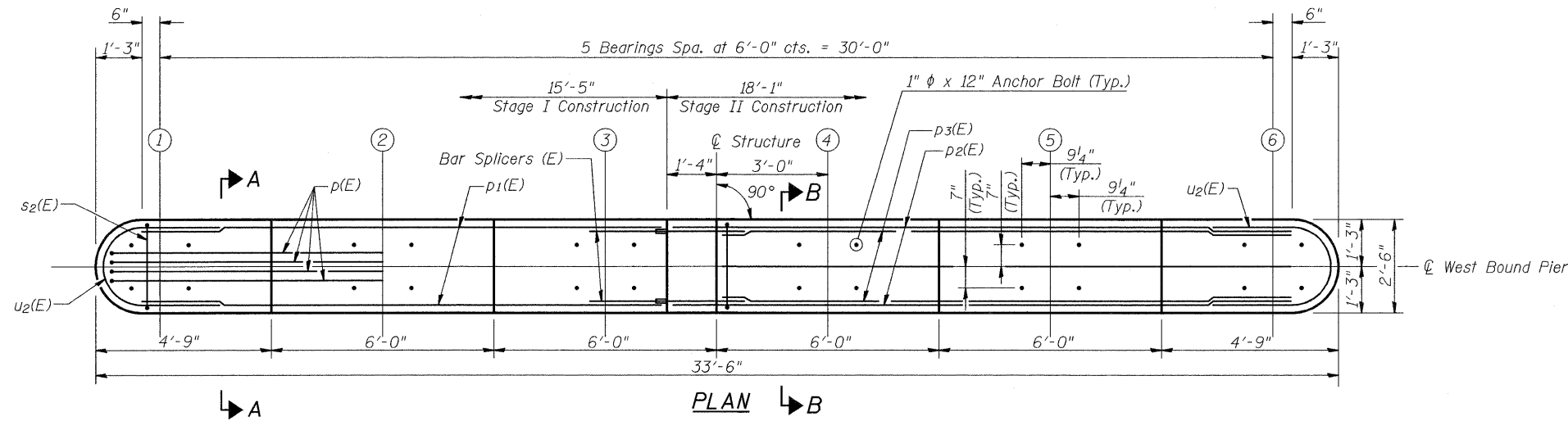
DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

**BEARING DETAILS - EAST BOUND PIER**  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 13	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
17 SHEETS	573	61B-BR-1	KANE	37	25
CONTRACT NO. 62817					
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
573	61B-BR-1	KANE	37	26
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

Contract No.: 62817



**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
p(E)	4	#7	8'-1"	C
p1(E)	6	#6	14'-0"	—
p2(E)	6	#6	16'-8"	—
p3(E)	3	#5	15'-4"	—
s2(E)	32	#4	8'-9"	□
s3(E)	16	#4	5'-2"	□
u2(E)	7	#6	7'-3"	U
Concrete Structures		Cu. Yd.	7.6	
Reinforcement Bars, Epoxy Coated		Pound	710	
Concrete Sealer		Sq. Ft.	83	
Bar Splicers		Each	6	

**MIN. BAR LAP**

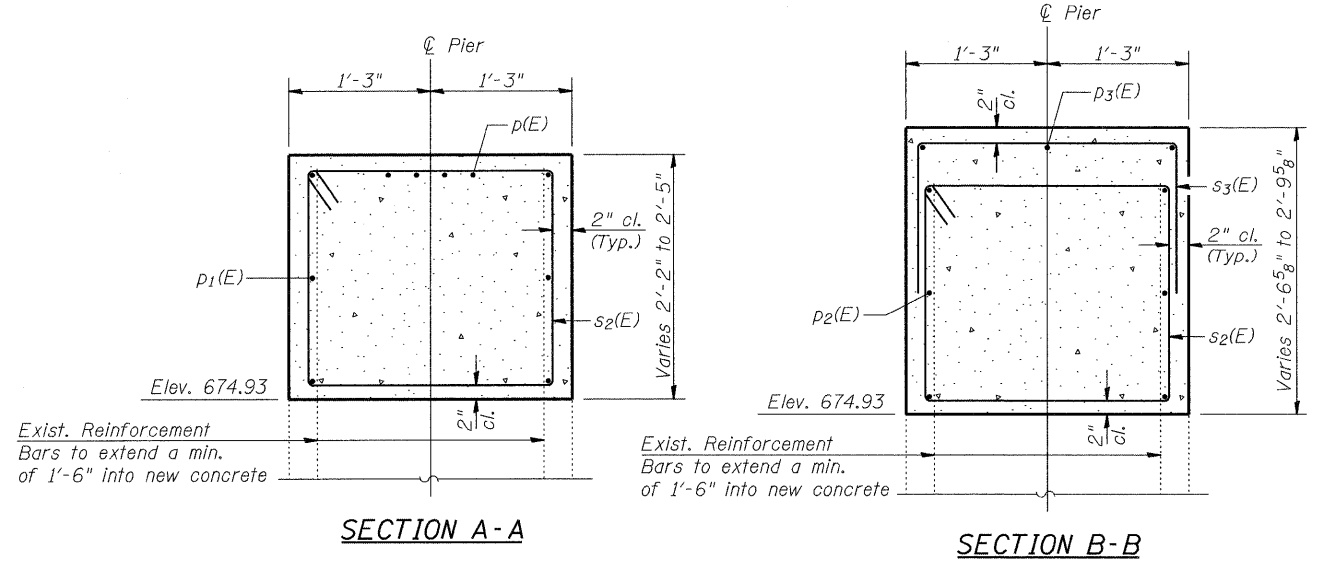
#6 bar = 2'-0"

**LEGEND**

E.F. = Each Face  
 cts. = centers  
 Elev. = Elevation  
 cl. = clear

**NOTES**

For Anchor Bolt and Bearing Details See Sheet 12 and 13 of 17.  
 All edges have standard 3/4" chamfers.  
 Space Reinforcement in cap to miss Anchor Bolts.  
 Pour steps monolithically with cap.



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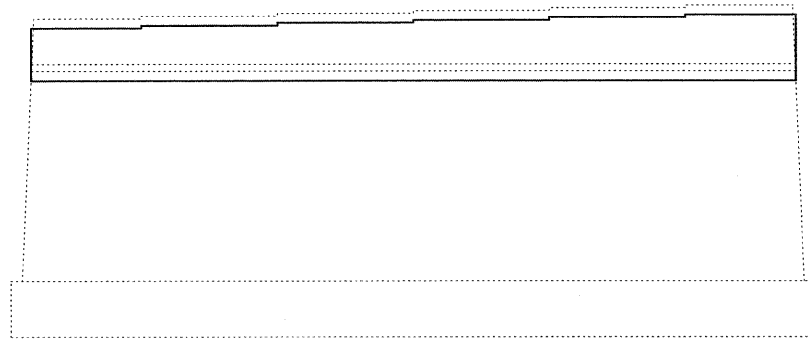
**PIER DETAILS (WEST BOUND STRUCTURE)**  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 14	F.A.P. RTE. 573	SECTION 61B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 26
17 SHEETS	CONTRACT NO. 62817				
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					

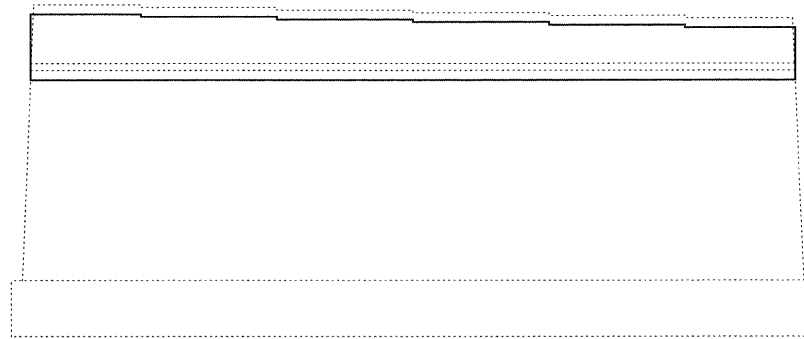
K:\118252\Encl. Plans\pier.dgn 6/3/2009 Default



- Structural Repair of Concrete (Depth Equal to or Less Than 5")



**WEST BOUND PIER**  
(Looking North)



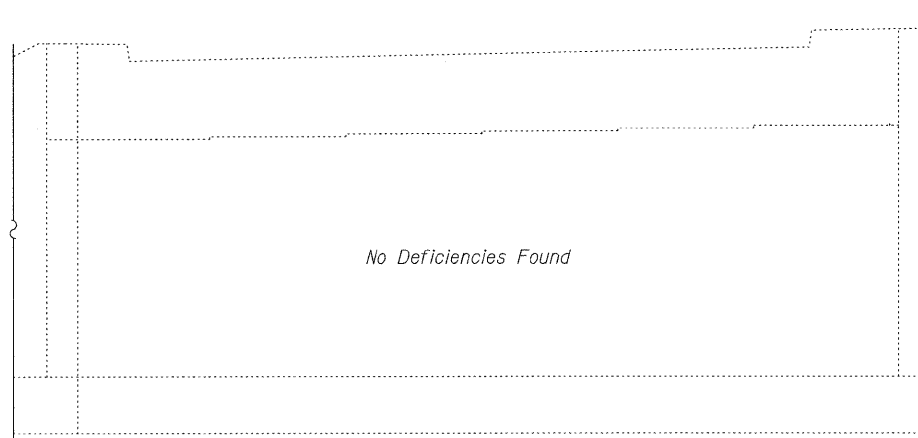
**WEST BOUND PIER**  
(Looking South)



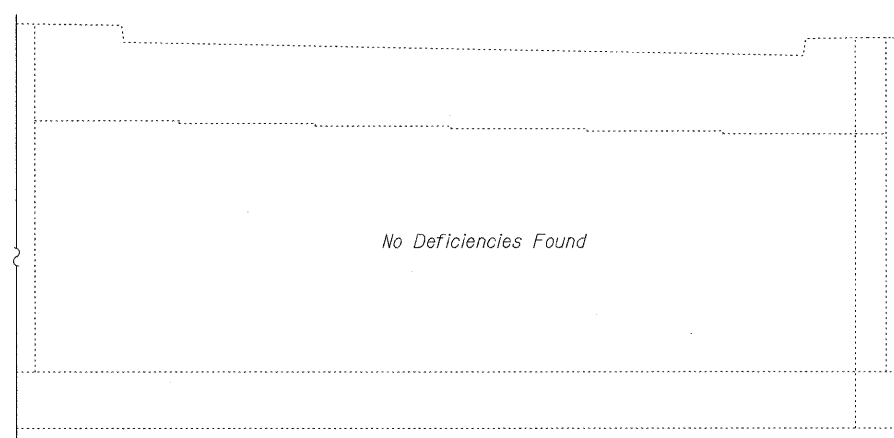
**EAST BOUND PIER**  
(Looking North)



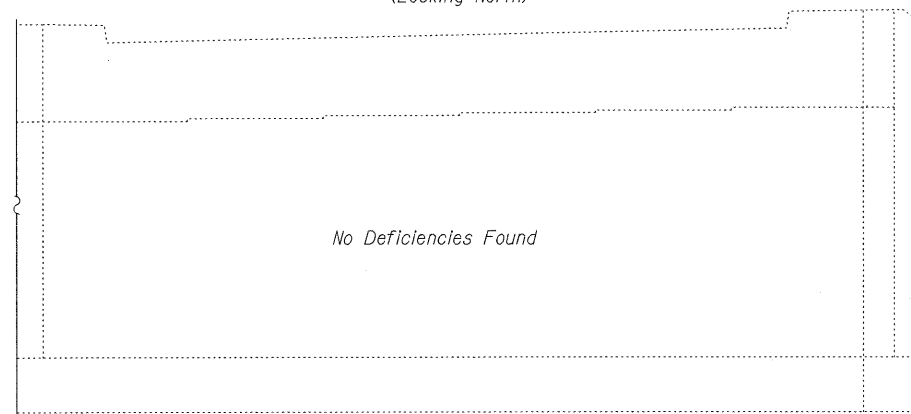
**EAST BOUND PIER**  
(Looking South)



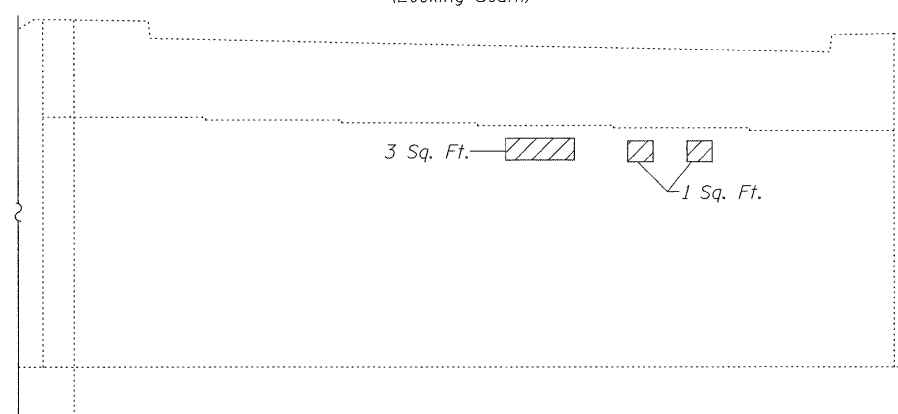
**WEST BOUND NORTH ABUTMENT**  
(Looking North)



**WEST BOUND SOUTH ABUTMENT**  
(Looking South)



**EAST BOUND NORTH ABUTMENT**  
(Looking North)



**EAST BOUND SOUTH ABUTMENT**  
(Looking South)

**BILL OF MATERIAL**

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq. Ft.	20

NOTE: Repair areas shown are from a survey done in 2004. Quantities have been increased to allow for additional needed repairs.

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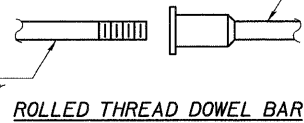
DESIGNED: WJV  
CHECKED: BLB  
DATE: 04/02/09

**SUBSTRUCTURE REPAIR**  
IL RTE. 56 OVER BLACKBERRY CREEK  
STATION 202+10.00  
STRUCTURE NO. 045-0026 (E.B.)  
STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 15	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
17 SHEETS	573	61B-BR-1	KANE	37	27
			CONTRACT NO. 62817		
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					

The diameter of this part is equal or larger than the diameter of bar spliced.

The diameter of this part is the same as the diameter of the bar spliced.



**ROLLED THREAD DOWEL BAR**



**\*\* ONE PIECE**

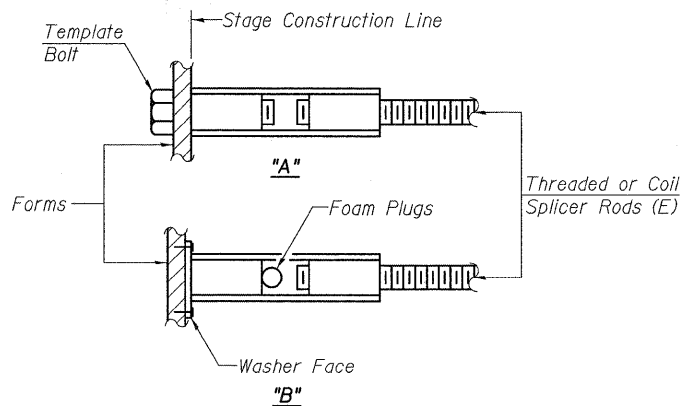
Wire Connector



**WELDED SECTIONS**

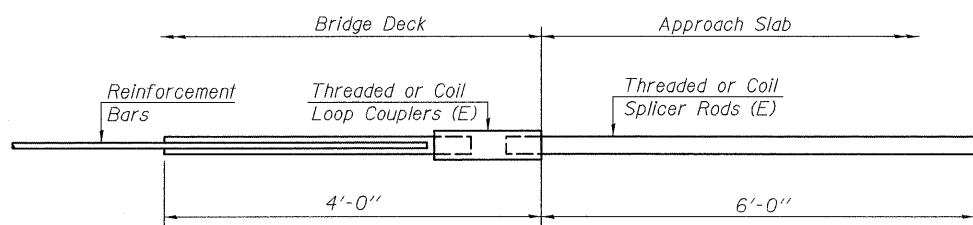
**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



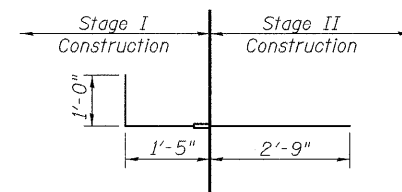
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar	
Min. Capacity = 23.0 kips - tension	
Min. Pull-out Strength = 12.3 kips - tension	
No. Required = 140	



**#6 BAR SPLICER (E) FOR DIAPHRAGMS**

**SPECIAL SPLICER DETAIL**

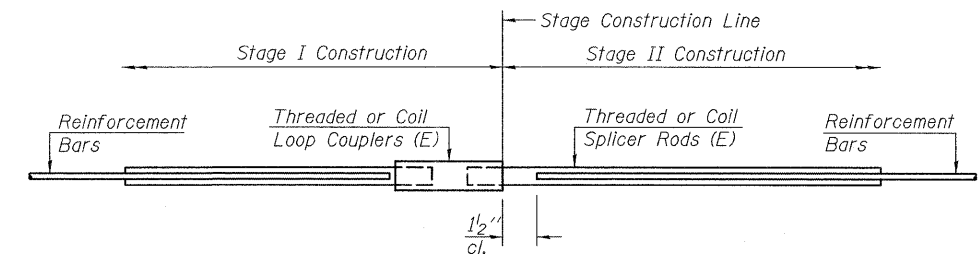
Bar Size	No. Assemblies Required	Location
#6	24	Diaphragms (Pier)
#6	8	Diaphragms (Abutments)

**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
  - ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
 \* = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



**STANDARD**

Bar Size	No. Assemblies Required	Location
#6	6	Pier
#6	8	Diaphragms (Pier)
#6	16	Diaphragms (Abutments)
#5	504	Deck

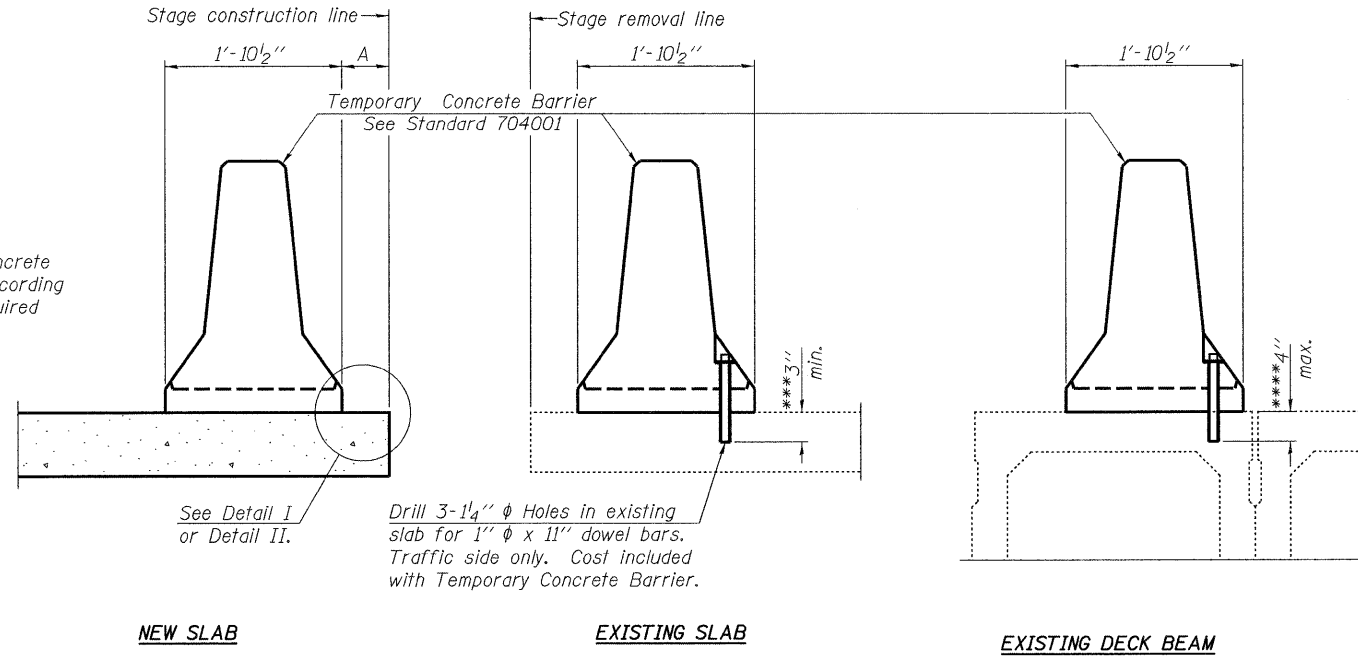
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 Wheaton, IL 60187  
 DESIGNED: WJV  
 CHECKED: BLB  
 DATE: 04/02/09

**BAR SPLICER ASSEMBLY DETAILS**  
 IL RTE. 56 OVER BLACKBERRY CREEK  
 STATION 202+10.00  
 STRUCTURE NO. 045-0026 (E.B.)  
 STRUCTURE NO. 045-0027 (W.B.)

SHEET NO. 16	F.A.P. RTE. 573	SECTION 61B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 28
17 SHEETS					
CONTRACT NO. 62817					
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



SECTIONS THRU SLAB OR DECK BEAM

NOTES

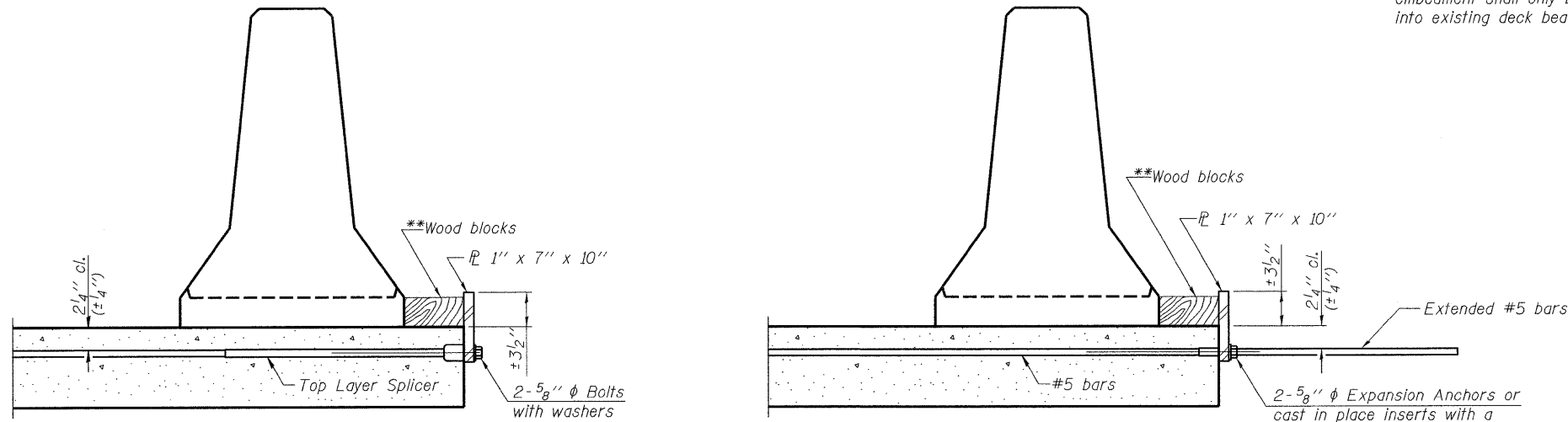
Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1"x7"x10" steel PL to the top layer of couplers with 2-5/8" φ bolts screwed to coupler at approximate CL of each barrier panel.

Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1"x7"x10" steel PL to the concrete slab or concrete wearing surface with 2-5/8" φ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate CL of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

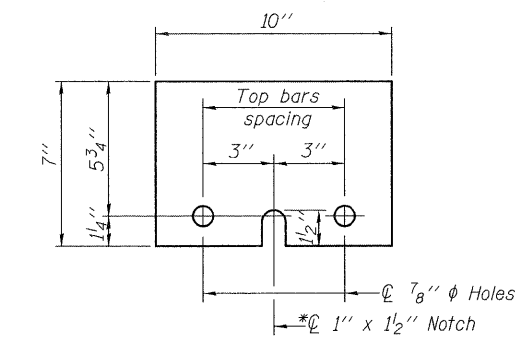
\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I

DETAIL II

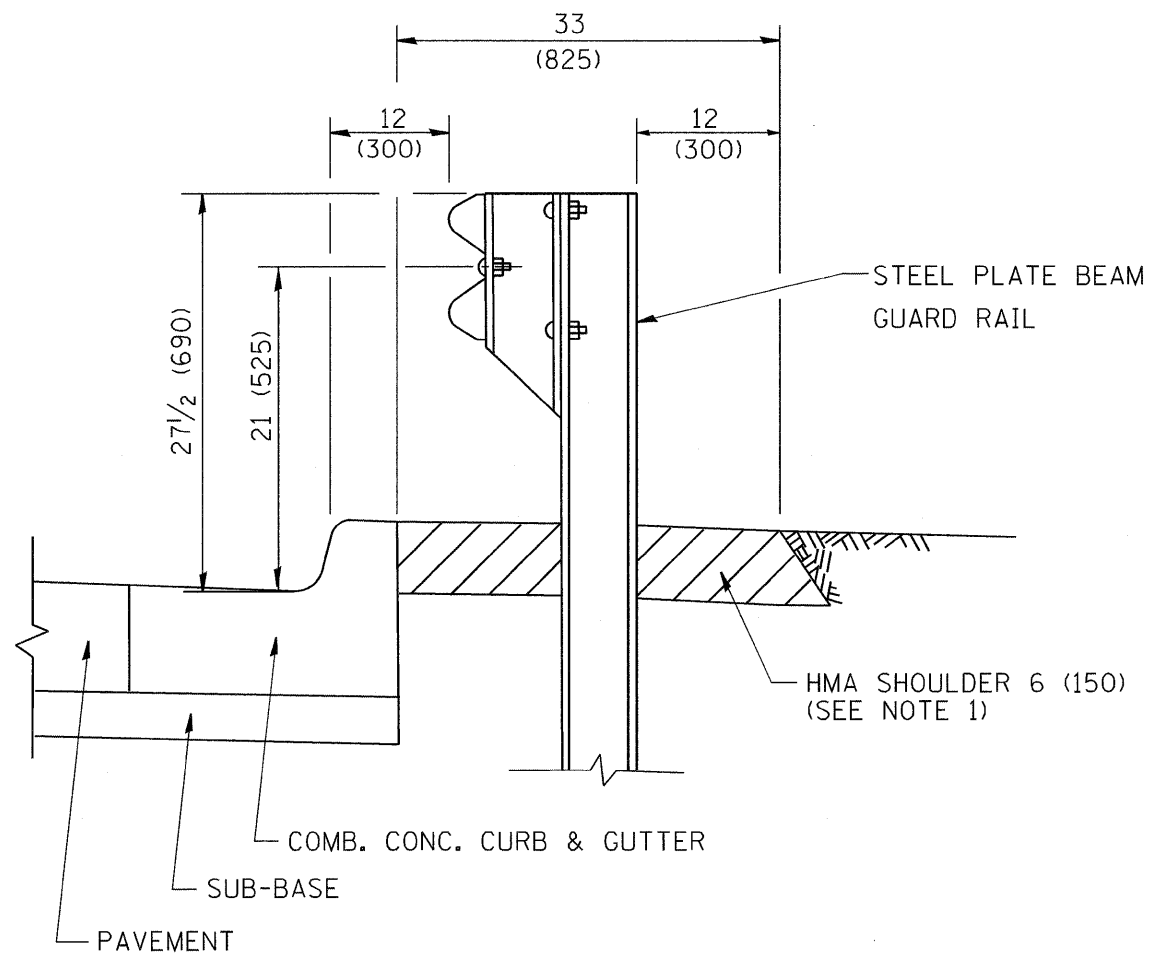
\*\*Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.



STEEL RETAINER PL 1" x 7" x 10"  
\* Required only with Detail II

<b>rjngroup</b> Excellence through Ownership  200 West Front Street Wheaton, IL 60187  DESIGNED: WJV CHECKED: BLB DATE: 04/02/09	TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION IL RTE. 56 OVER BLACKBERRY CREEK STATION 202+10.00 STRUCTURE NO. 045-0026 (E.B.) STRUCTURE NO. 045-0027 (W.B.)					
	SHEET NO. 17  17 SHEETS	F.A.P. RTE. 573	SECTION 61B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 29
	CONTRACT NO. 62817 FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					

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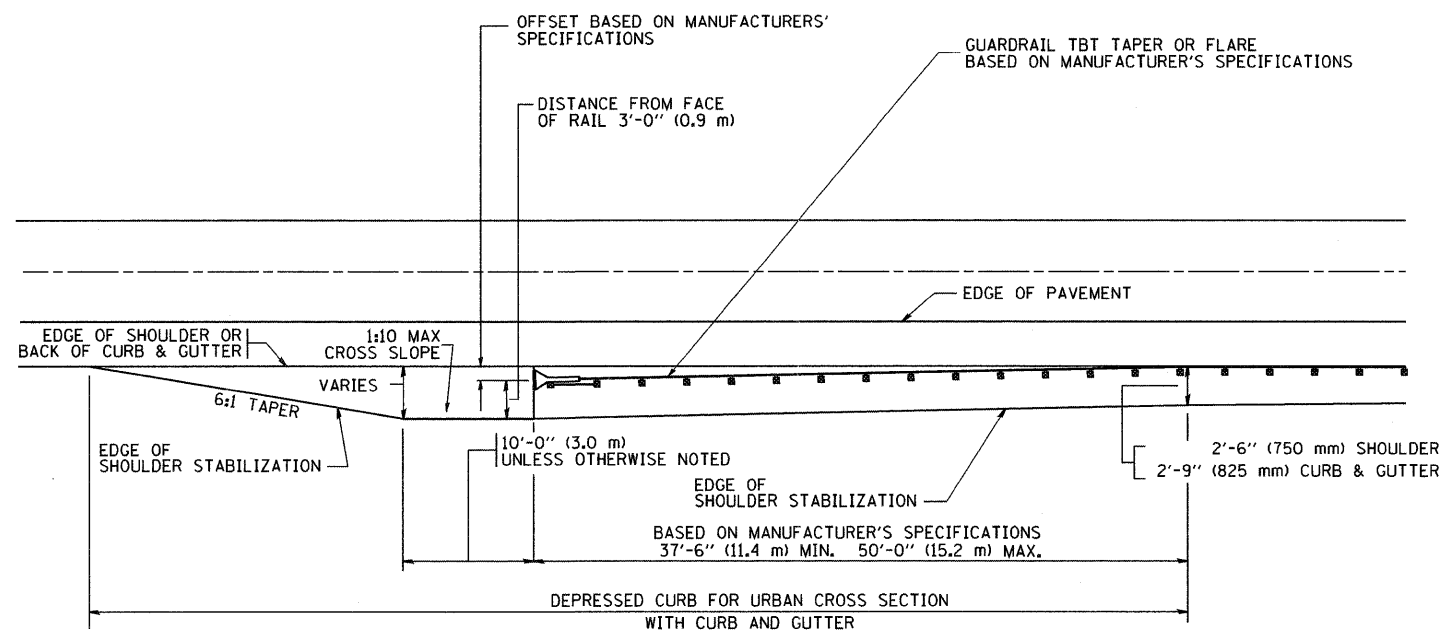


- NOTES: 1. THE HMA SHOULDER SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL
2. GUARD RAIL MAY BE PLACED AT THE BACK OF CURB WHEN DIRECTED BY THE ENGINEER.

BASIS OF PAYMENT: HMA SHOULDER 6 (150) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDER 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

**DETAILS FOR STEEL PLATE BEAM  
GUARD RAIL ADJACENT TO CURB AND GUTTER  
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**STABILIZATION AT TBT TY. 1 SPL.**

TBT = TRAFFIC BARRIER TERMINAL

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = goglienobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 02-23-95
W:\diststd\22x34\bd34.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - E. GOMEZ 08-28-00
	PLOT DATE = 1/4/2008	DATE - 09-22-90	REVISED - R. BORO 01-01-07

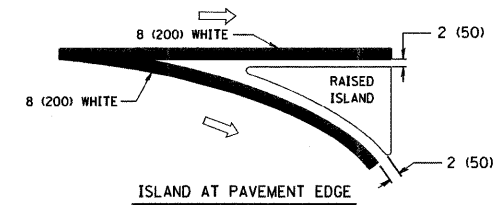
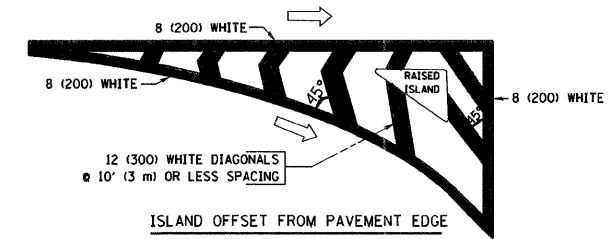
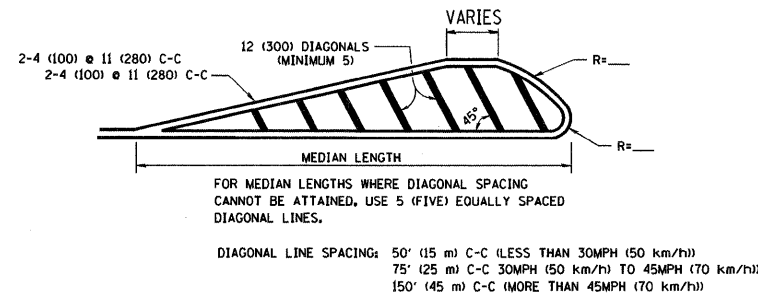
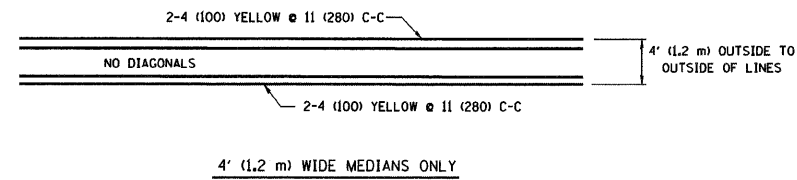
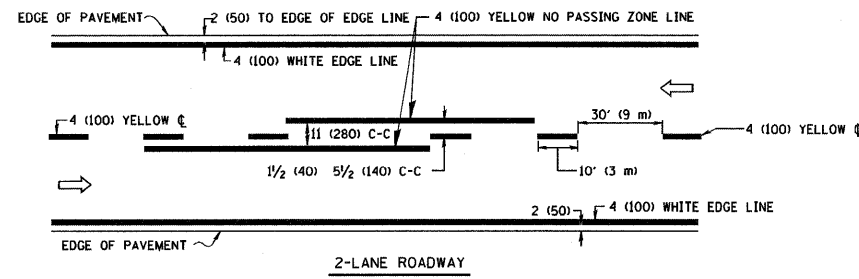
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR STEEL PLATE BEAM GUARD RAIL ADJACENT  
TO CURB AND GUTTER STABILIZATION AT TBT TY 1 SPL.**

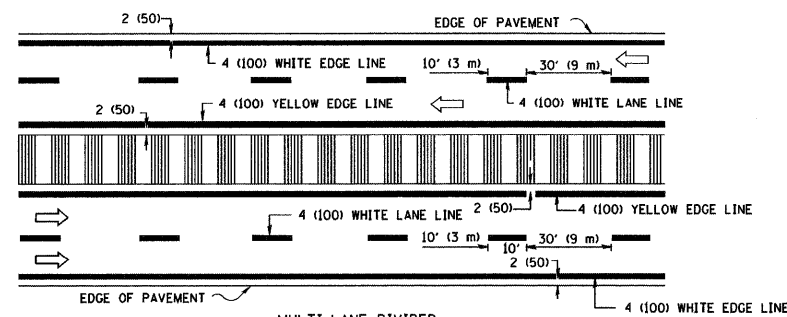
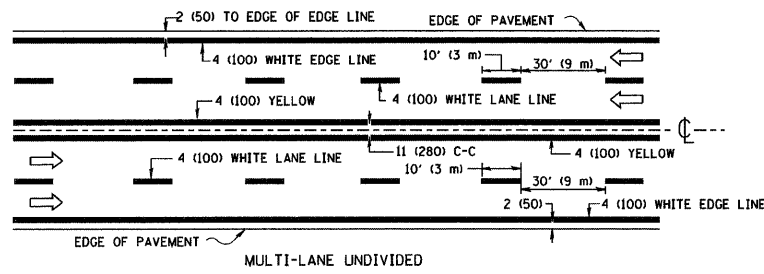
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
537	61 B-BR-1	KANE	37	30
BD600-10 (BD 34)			CONTRACT NO. 62817	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



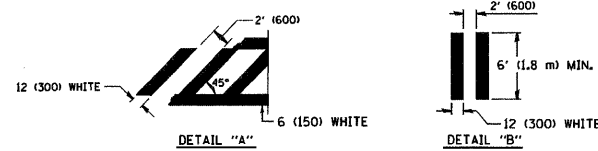
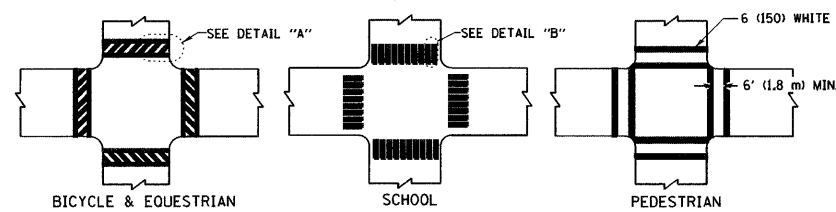


TYPICAL ISLAND MARKING

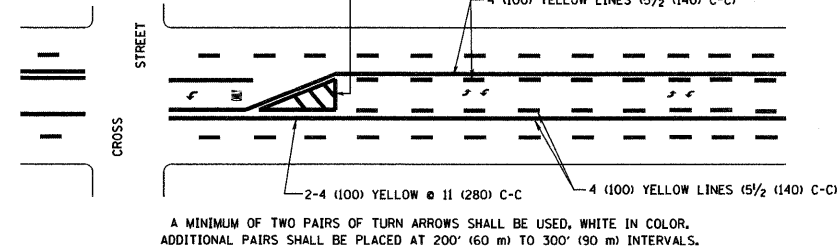


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

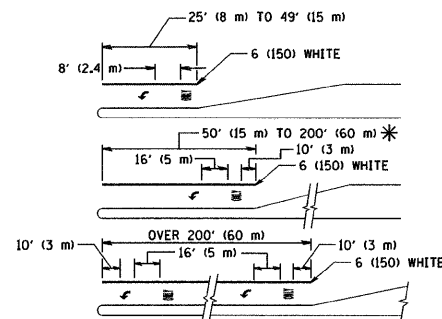


TYPICAL CROSSWALK MARKING



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

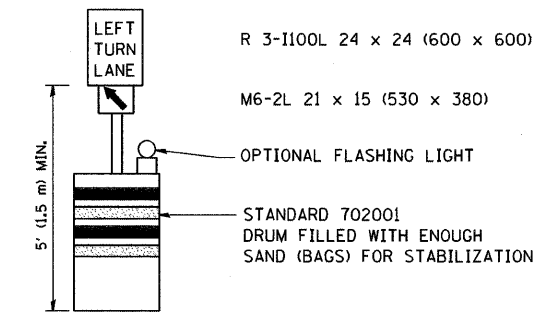
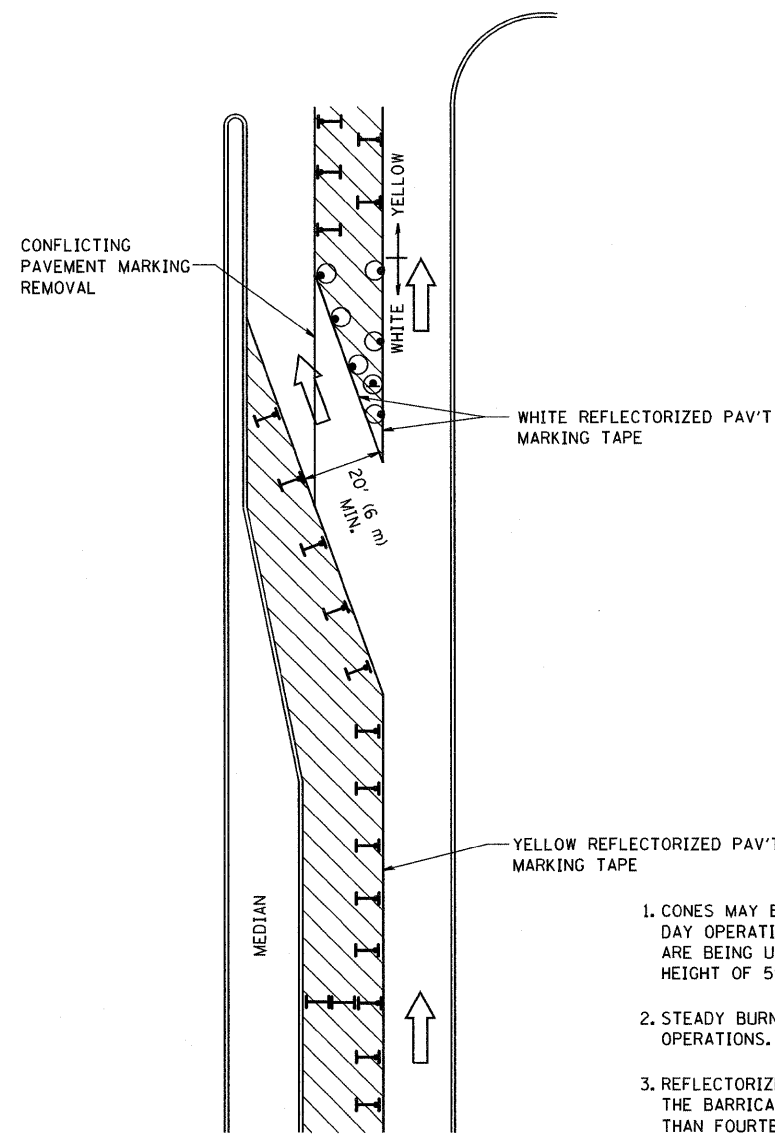
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	PLOT SCALE = 50.000 "/ IN.	CHECKED -	REVISED -A. HOUSEH 10-17-96
	PLOT DATE = 1/4/2008	DATE - 03-19-90	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

F.A.P. RTE. 537	SECTION 61 B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 32
TC-13		CONTRACT NO. 62817		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

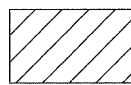







**GENERAL NOTES**

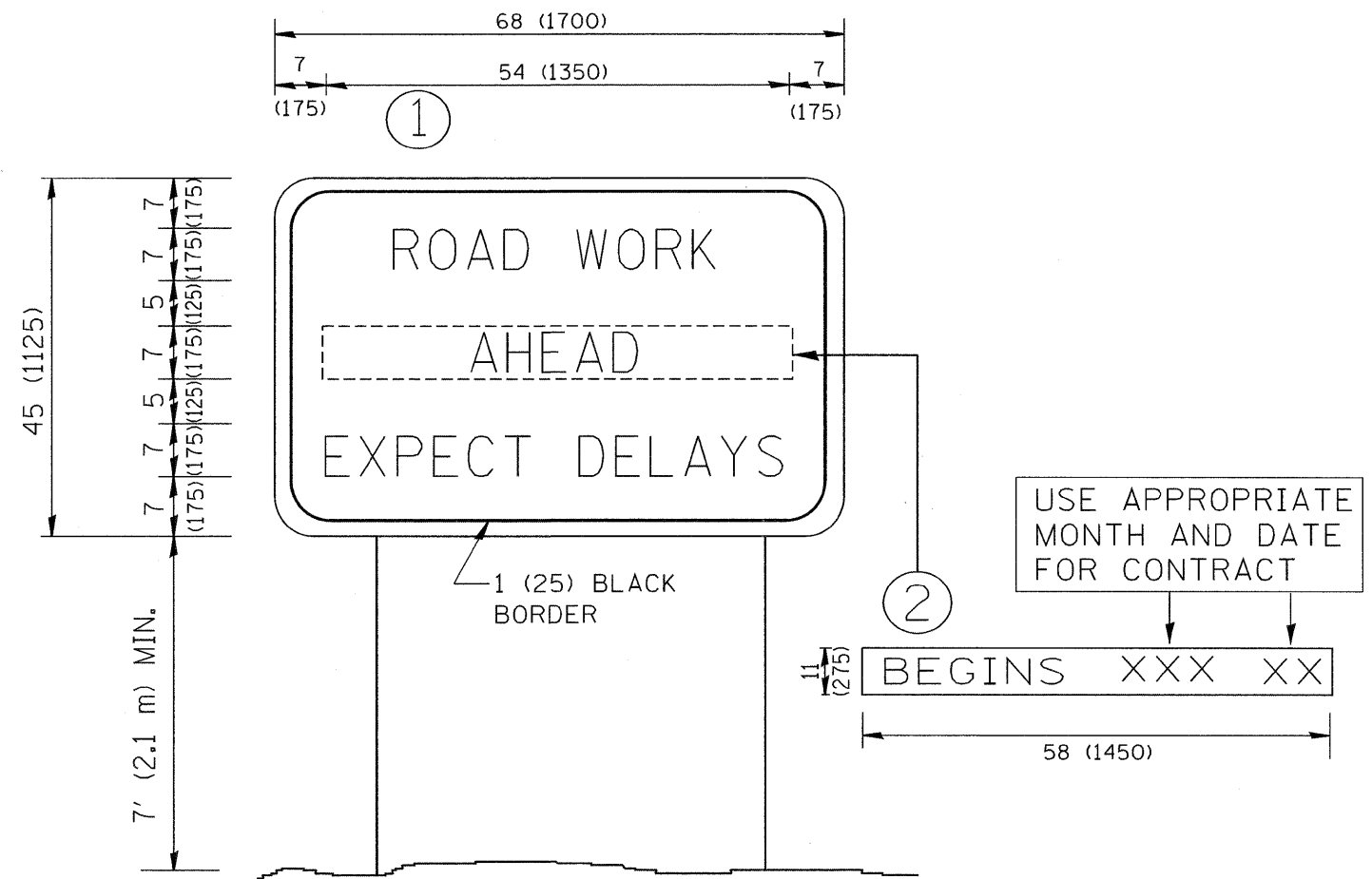
1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME = W:\diststd\22x34\to14.dgn	USER NAME = geglianobt	DESIGNED -	REVISED -T. RAMMACHER 09-08-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50,0000' / IN.	DRAWN -	REVISED - A. HOUSEH 11-07-95					537	61 B-BR-1	KANE	37	33
PLOT DATE = 1/4/2008	CHECKED -	REVISED - A. HOUSEH 10-12-96	REVISED -T. RAMMACHER 01-06-00	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-14		CONTRACT NO. 62817		
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 537	SECTION 61 B-BR-1	COUNTY KANE	TOTAL SHEETS 37	SHEET NO. 34
TC-22			CONTRACT NO. 62817	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

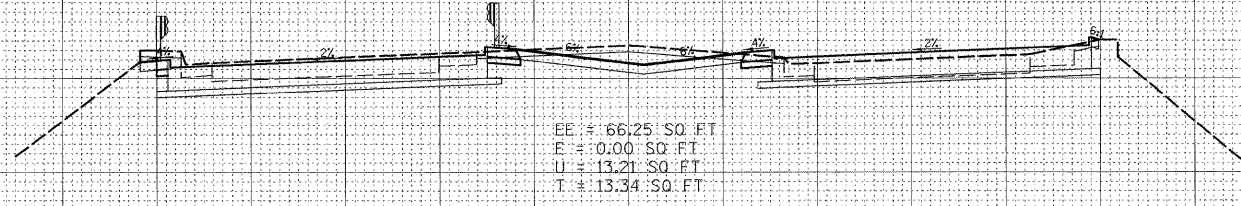




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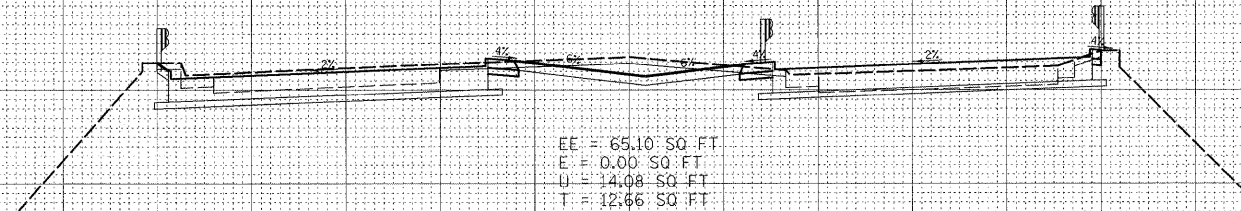


202+54.49

680

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675



201+65.62

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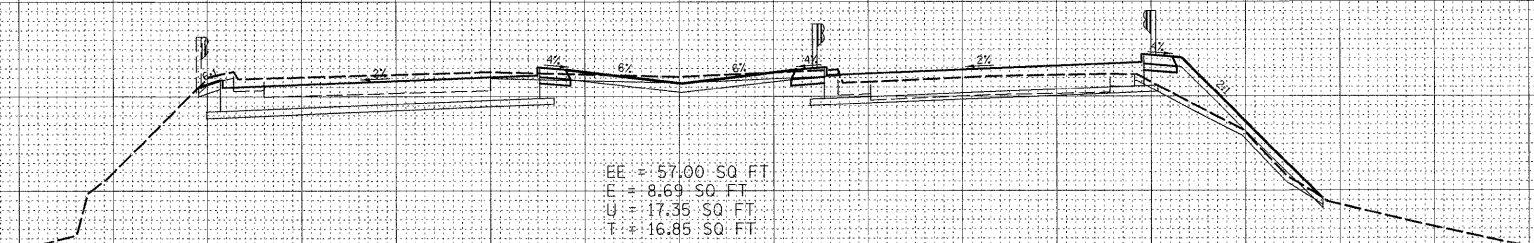
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201+50.00

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FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>IL RTE. 56 OVER BLACKBERRY CREEK</b> <b>CROSS SECTIONS</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#	PLOT SCALE = 1:10	DRAWN - JTT	REVISED -		573	61 B-BR-1	KANE	37	36			
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		DATE - 4/2/09	REVISED -		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT					

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

