

FIELD ENGINEER: MARILIN D. SOLOMON (847) 705-4407
CONSULTANT: JAMES J. BENES & ASSOCIATES, INC. (630) 719-7570

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	-	DUPAGE	25	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 63098				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

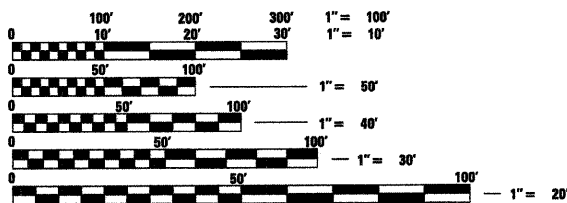
PLAN 1" = 20'
PROFILE HORIZ. 1" = 20'
PROFILE VERT. 1" = 5'
CROSS SECTIONS HORIZ. 1" = 10'
CROSS SECTIONS VERT. 1" = 5'

**F.A.U. ROUTE 2662 (JORIE BOULEVARD)
31st STREET TO 22nd STREET
SECTION NO. 08-00041-00-RS
LAPP ROADWAY RESURFACING
PROJECT NO. M-9003(159)
VILLAGE OF OAK BROOK
DuPAGE COUNTY
JOB NO. C-91-195-09**

TRAFFIC DATA
ADT = 13,200
SPEED LIMIT = 35 MPH

DESIGN DESIGNATION: COLLECTOR

PROJECT LOCATED IN
VILLAGE OF OAK BROOK



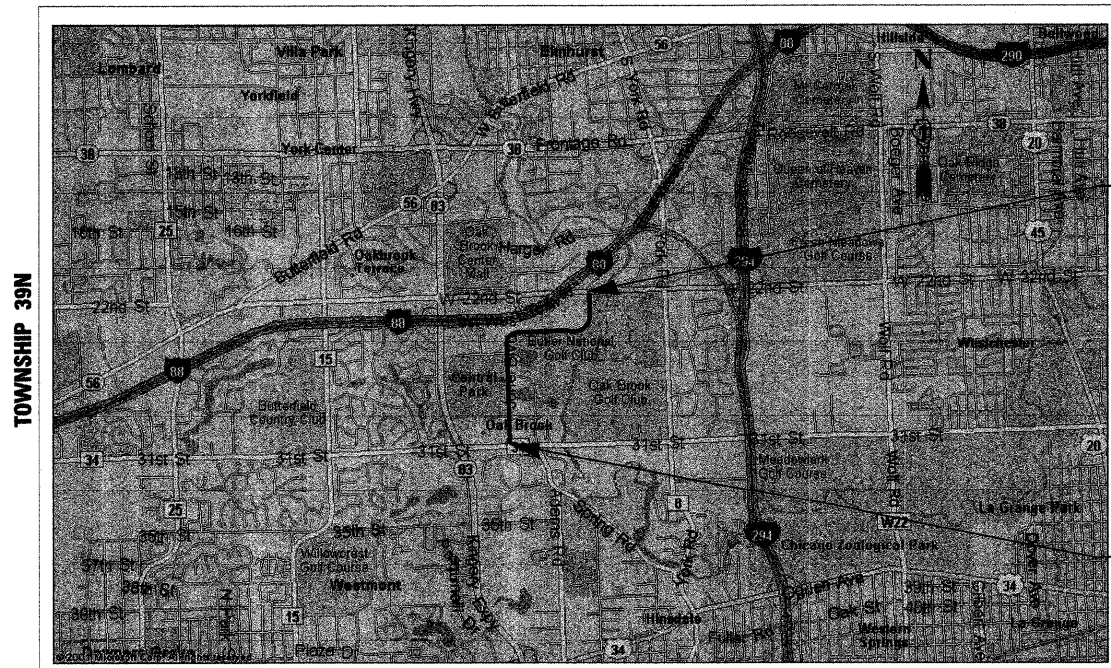
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
811



CONTRACT NO. 63098

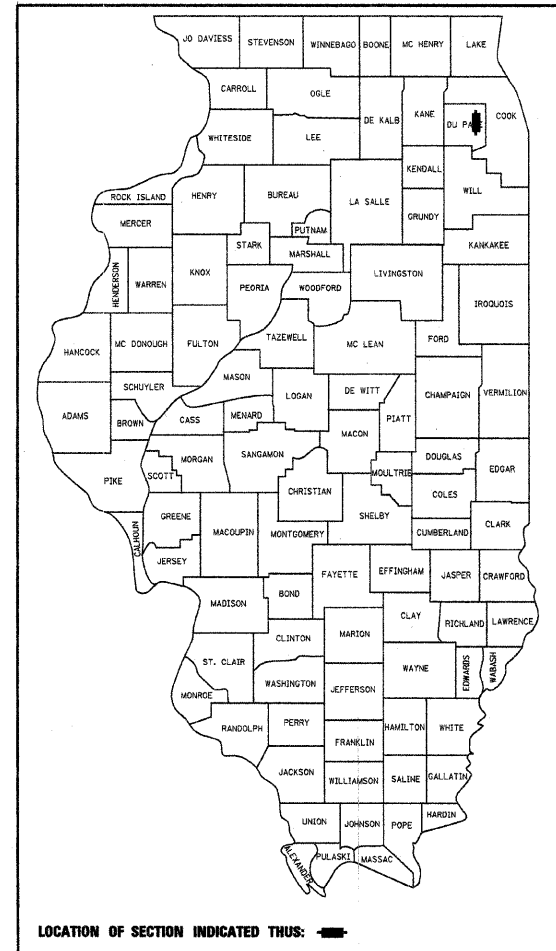
YORK TOWNSHIP, SECTION 26 RANGE 11E



PROJECT ENDS
STA 78+11

PROJECT BEGINS
STA 1+25

SECTION 26, YORK TOWNSHIP 3rd P.M.
GROSS AND NET LENGTH OF PROJECT: 7686 FT (1.456 MILES)
SCALE: NTS
LOCATION MAP



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: AUGUST 25, 2009
Robert J. Benes
Oak Brook, Village Engineer

PASSED: OCTOBER 26, 2009
Cheryl Christensen
District 1 Engineer of Local Roads & Streets

Releasing for Bid
Based on Limited
Review OCTOBER 26, 2009
Diane M. O'Keefe
Deputy Director of Highways, Region 1 Engineer

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JJB
JAMES J. BENES & ASSOCIATES
CONSULTING ENGINEERS
(630) 719-7570
950 WARRENVILLE ROAD, SUITE 101
LISLE, IL 60532
SIGNATURE: *James J. Benes*
DATE: 8-25-09
IL LICENSE NO. 062-039438
EXP. DATE: NOVEMBER 30, 2009
FIELD: JAMES J. BENES AND ASSOCIATES, INC.

GENERAL NOTES

- ACCESS TO LOCAL RESIDENCES AND BUSINESSES SHALL BE MAINTAINED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL GIVE THE MUNICIPALITY AND JAMES J. BENES AND ASSOCIATES, INC. THREE (3) WORKING DAYS NOTICE PRIOR TO THE COMMENCEMENT OF WORK.
JAMES J. BENES AND ASSOCIATES, INC.: (630) 719-7570
- ALL ELEVATIONS ARE ON U.S.G.S. DATUM.
- NEITHER THE ENGINEER, NOR THE OWNER, SHALL ASSUME ANY OF THE RESPONSIBILITIES OF THE CONTRACTOR'S SUPERINTENDENT OR OF SUBCONTRACTORS. ADDITIONALLY, NEITHER THE ENGINEER, NOR THE OWNER, SHALL ADVISE ON, OR ISSUE DIRECTIONS CONCERNING, ASPECTS OF CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, OR SAFETY PRECAUTIONS AND/OR PROGRAMS IN CONNECTION WITH THE WORK.
- THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THEIR ACCURACY IS NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES. THE CONTRACTOR SHALL REPORT ALL ENCOUNTERED DISCREPANCIES TO THE ENGINEER AT ONCE. THE CONTRACTOR SHALL TAKE DUE CARE IN ALL PHASES OF CONSTRUCTION TO PROTECT ALL UTILITIES WHICH MAY BE AFFECTED BY THE WORK. ALL DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL KEEP THE CONSTRUCTION AREA FREE OF DEBRIS AND/OR OBJECTIONABLE MATERIALS DURING CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE FACILITIES DURING CONSTRUCTION AND SHALL REPAIR ANY DRAINAGE FACILITIES DAMAGED DURING CONSTRUCTION. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT AND WILL NOT BE PAID FOR SEPARATELY.
- THE CONTRACTOR SHALL VERIFY THE ELEVATIONS OF EXISTING STORM SEWERS PRIOR TO THE CONSTRUCTION OF PROPOSED STORM SEWER.
- BEFORE STARTING AND EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- UNLESS OTHERWISE SHOWN, TRANSITIONS OF 10' SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTERS AND MEDIANS IN THE FIELD. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE MUNICIPALITY.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I AND TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINTS AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

INDEX OF SHEETS

NO.	TITLE
1	TITLE SHEET
2	INDEX OF SHEETS, GENERAL NOTES, STATE STANDARDS AND BENCH MARKS
3	SUMMARY OF QUANTITIES
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6	SCHEDULES OF QUANTITIES
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19	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
20	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
21	BUTT JOINT AND HMA TAPER DETAILS
22	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
24	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
25	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

BENCH MARKS

- TOP OF THE NORTHWEST BOLT ON THE FIRE HYDRANT AT STA. 4+56, 41' RT. ELEVATION = 687.48
- TOP OF THE ARROW ON THE FLANGE ON THE FIRE HYDRANT AT STA. 19+96, 41' RT. ELEVATION = 666.53
- TOP OF THE EAST BOLT ON THE FIRE HYDRANT AT STA. 36+69, 41' LT. ELEVATION = 669.16
- TOP OF THE ARROW ON THE FLANGE ON THE HYDRANT AT STA. 52+45, 40' LT. ELEVATION = 670.19
- TOP OF THE ARROW ON THE FLANGE ON THE FIRE HYDRANT AT STA. 73+89, 44' LT. ELEVATION = 667.13

LIST OF STATE STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
420001-07	PAVEMENT JOINTS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
602001-01	CATCH BASIN, TYPE A
602011-01	CATCH BASIN, TYPE C
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS TYPE I
604051-03	FRAME AND GRATE TYPE II
604091-02	FRAME AND GRATE TYPE 24
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-03	CORRUGATED PC CONCRETE MEDIAN
701426-03	LANE CLOSURE MULTILANE INTERMITTENT OR MOVING OPERATIONS FOR SPEEDS > 45 MPH
701601-06	URBAN LANE CLOSURE MULTILANE 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701701-06	URBAN LANE CLOSURE MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
814001-02	HANDHOLES
886001-01	DETECTOR LOOP INSTALLATIONS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 2662
JORIE BOULEVARD
INDEX OF SHEETS,
GENERAL NOTES, BENCH MARKS
AND STATE STANDARDS

SCALE: NTS
DATE: 2/06/09

DRAWN BY: SMP
CHECKED BY: JDM

SPECIALTY ITEMS	SPECIAL PROVISION	ITEM Code Number	Description	Unit	TOTAL QUANTITY CODE 1000
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	575
		25000400	NITROGEN FERTILIZER NUTRIENT	POUND	7
		25000500	PHOPHORUS FERTILIZER NUTRIENT	POUND	7
		25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	7
		25200100	SODDING	SQ YD	575
		25200200	SUPPLEMENTAL WATERING	UNIT	10
		28000510	INLET FILTERS	EACH	70
		40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	40
		40600300	AGGREGATE (PRIME COAT)	TON	200
		40600400	MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS	TON	25
		40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	4264
		40600895	CONSTRUCTING TEST STRIP	EACH	1
	SP	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	216
		40601340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4975
		42001300	PROTECTIVE COAT	SQ YD	288
			PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	15
		42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	15
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	200
		42400800	DETECTABLE WARNINGS	SQ FT	100
		44001620	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SQ YD	50751
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	75
		44000600	SIDEWALK REMOVAL	SQ FT	200
		44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	765
	SP	44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	100
	SP	44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	200
	SP	44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	300
	SP	44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	400
		44213200	SAW CUTS	FOOT	265
		60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	4
			MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1
		60257900	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1
		60258200	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1
		60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	12
		60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2
		60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	20
	SP	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1
		67100100	MOBILIZATION	L SUM	1
	SP	70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1
	SP	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
	SP	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
	SP	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	4
		70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4250
		70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1417
		72000100	SIGN PANEL-TYPE 1	SQ FT	28
		72900200	METAL POST-TYPE B	FOOT	56
			THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1097
		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1097
		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3420
		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3818
		78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	225
		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	753
		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	395
		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	166
		88600100	DETECTOR LOOP, TYPE 1	FOOT	1788
		XX002267	MEDIAN REMOVAL AND REPLACEMENT	SQ FT	138
	SP	X4021000	TEMPORARY ACCESS(PRIVATE ENTRANCE)	EACH	11
	SP	Z0068500	STABILIZED DRIVEWAYS, 7"	SQ YD	60
	SP	Z0076600	TRAINEES	HOURS	500

** CODE Y080

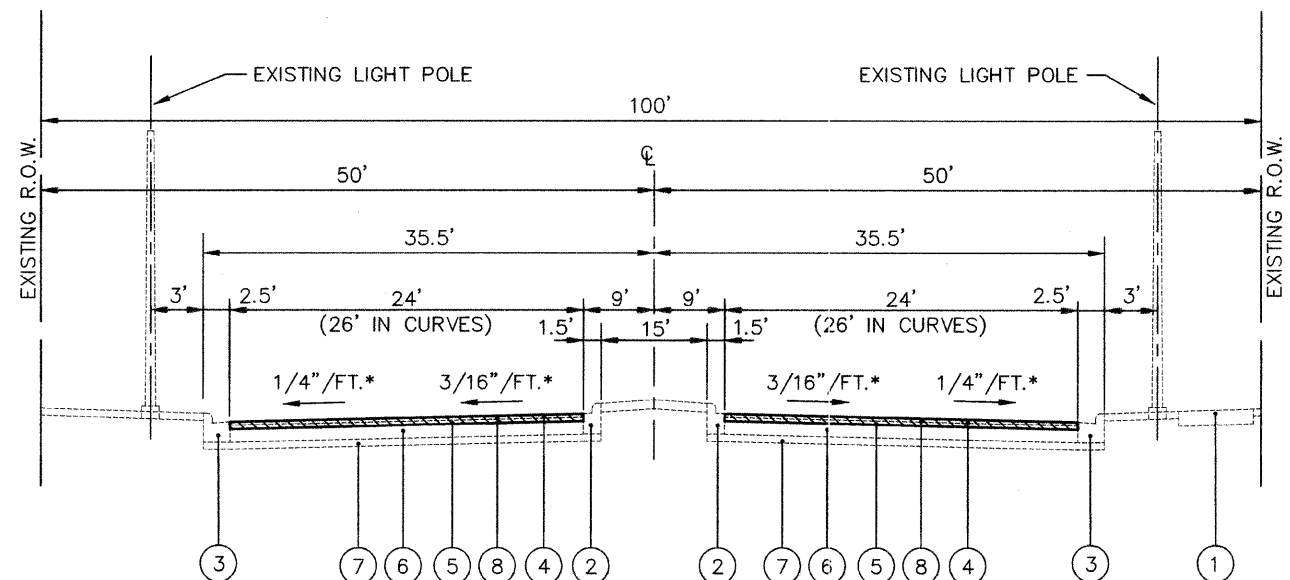
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 2662
JORIE BOULEVARD

SUMMARY OF QUANTITIES

SCALE: N/A
DATE: 2/06/09

DRAWN BY: SMP
CHECKED BY: JDM



EXISTING TYPICAL SECTION
JORIE BOULEVARD
STA 1+25 TO STA 78+11

- LEGEND**
- ① EXISTING BITUMINOUS OR AGGREGATE PATHWAY SURFACE
 - ② EXISTING CURB AND GUTTER, TYPE B-6.12
 - ③ EXISTING CURB AND GUTTER, TYPE B-6.24
 - ④ EXISTING HMA SURFACE COURSE (APPROX. 1.5" THICK)
 - ⑤ EXISTING HMA BINDER COURSE (APPROX. 1.5" THICK)
 - ⑥ EXISTING HMA BASE COURSE (APPROX. 7" THICK)
 - ⑦ EXISTING AGGREGATE BASE (APPROX. 4" THICK)
 - ⑧ PROPOSED HMA SURFACE REMOVAL, 3 1/4 INCHES
 - ⑨ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 1 1/2"
 - ⑩ PROPOSED HMA SURFACE COURSE, MIX D, N70, 1 3/4"
 - ⑪ BITUMINOUS MATERIALS (PRIME COAT)
 - ⑫ AGGREGATE (PRIME COAT)
 - ⑬ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT *
 - ⑭ SODDING *
 - ⑮ MEDIAN REMOVAL AND REPLACEMENT *

* AT LOCATIONS DETERMINED BY THE ENGINEER

PAVEMENT DESIGN CALCULATIONS

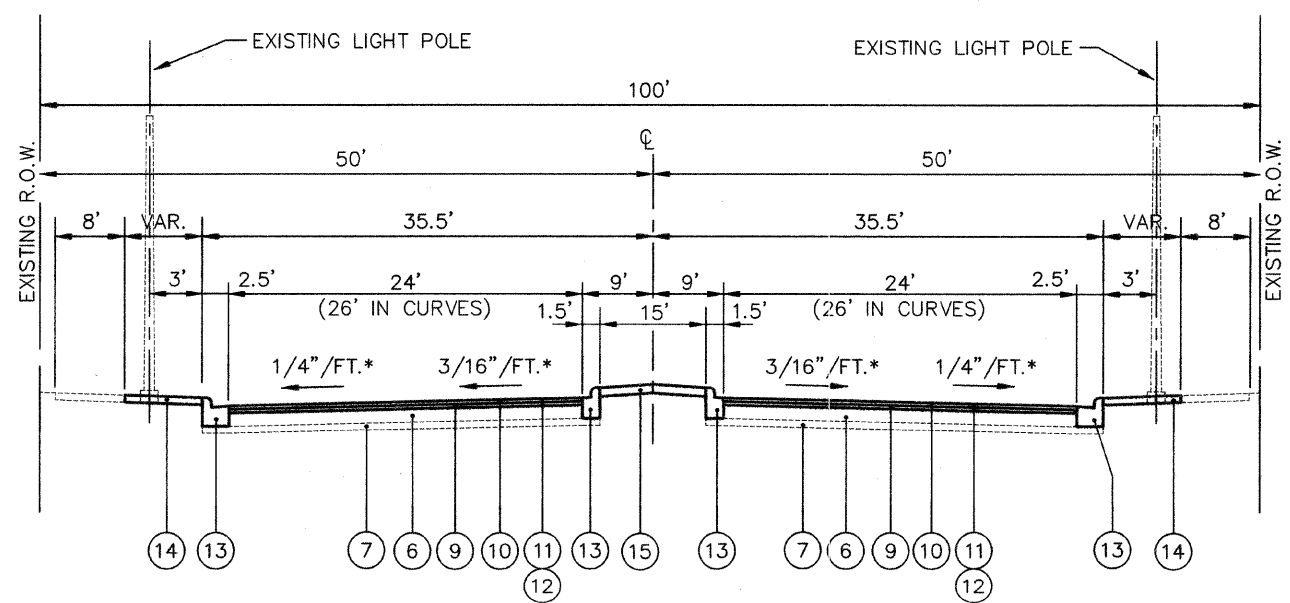
DATE: December 1, 2008
IMPROVEMENT TYPE: FLEXIBLE PAVEMENT CROSS SECTION (RESURFACING)
LOCATION: JORIE BOULEVARD, OAK BROOK

CLASSIFICATION OF ROADWAY: = CLASS I ROADWAY (ADT>3500) - FOUR LANE PAVEMENT
 TRAFFIC FACTOR = $DP((0.15^P \cdot PV) + (132.50 \cdot S \cdot SU) + (482.53 \cdot M \cdot MU)) / 1000000$
 DESIGN LANE VOLUME % OF ADT = 25 % TRUCKS 25 % PASS. VEHICLES
 DESIGN PERIOD, YEARS (DP) = 20 YEARS
 % OF PASSENGER VEHICLES (PV) = 99.00 %
 % OF SINGLE UNIT TRUCKS (SU) = 0.80 %
 % OF MULTI UNIT TRUCKS (MU) = 0.20 %

AVERAGE DAILY TRAFFIC	= 13200	TRAFFIC FACTOR.....	= 0.143455
DESIGN LANE VOLUME	= 3300	IB.R.....	= 3.0
NO. OF PASSENGER VEHICLES	= 3287	STRUCTURAL NUMBER (D _s)	= 3.10
NO. OF SINGLE UNIT TRUCKS	= 26		
NO. OF MULTI UNIT TRUCKS	= 7		

PROPOSED PAVEMENT CROSS SECTION

MATERIAL THICKNESS	STRUCTURAL MATERIAL	COEFFICIENT	D _t
3.25	HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70	0.40	1.30
0.00	EXISTING HOT-MIX ASPHALT BINDER COURSE	0.25	0.00
6.75	EXISTING HOT-MIX ASPHALT BASE COURSE	0.25	1.69
4.00	EXISTING AGGREGATE BASE COURSE	0.10	0.40
TOTAL D_t PROVIDED =			3.39



PROPOSED TYPICAL SECTION
JORIE BOULEVARD
STA 1+25 TO STA 78+11

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, (IL-9.5 mm)	4% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70	4% @ 70 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, (IL-9.5 mm)	4% @ 50 Gyr.
HOT-MIX ASPHALT BASE COURSE (HOT-MIX ASPHALT BINDER IL-19 mm)	4% @ 50 Gyr.
CLASS D PATCHING	
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
 Tel. (630) 719-7570 • Fax (630) 719-7589

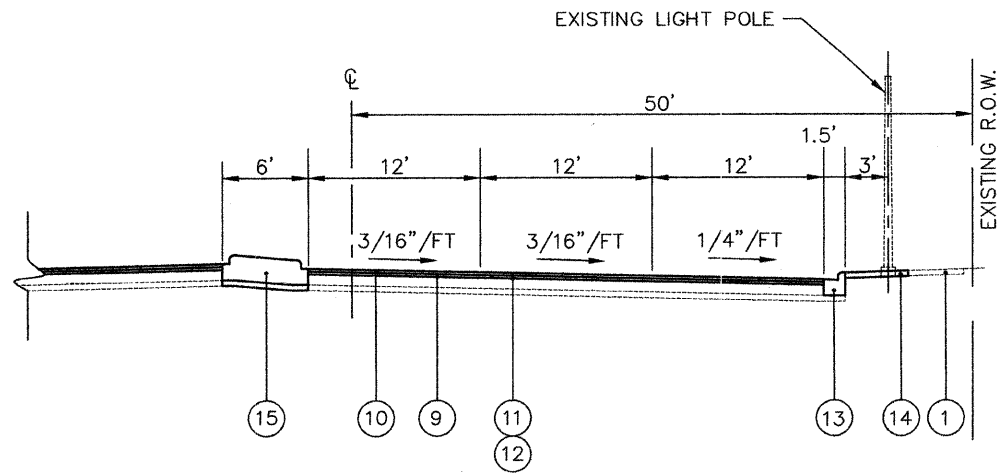
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD

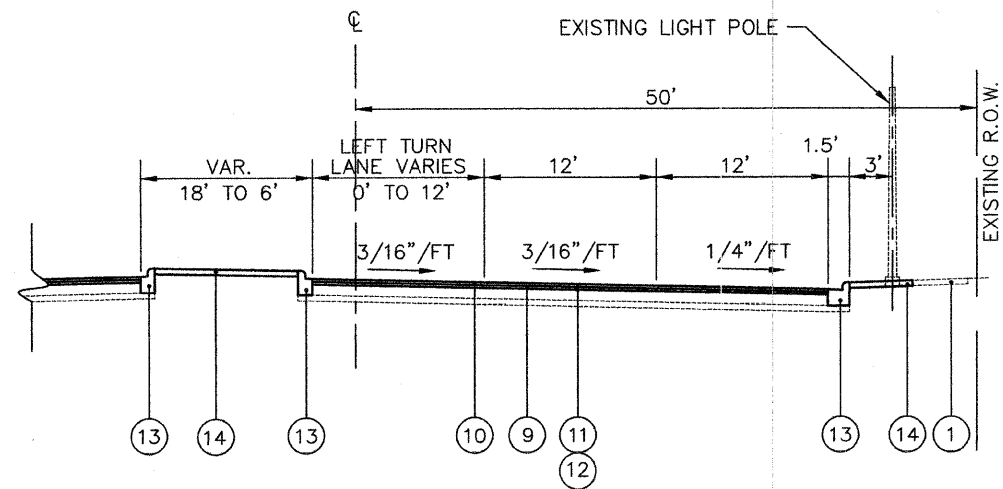
TYPICAL SECTIONS

SCALE: NTS
 DATE: 03/18/08

DRAWN BY: RAJ
 CHECKED BY: JDM



PROPOSED LEFT TURN LANE STORAGE
JORIE BOULEVARD
(LOCATIONS VARY)



PROPOSED LEFT TURN LANE TAPER
JORIE BOULEVARD
(LOCATIONS VARY)

LEGEND

- ① EXISTING BITUMINOUS OR AGGREGATE PATHWAY SURFACE
- ② EXISTING CURB AND GUTTER, TYPE B-6.12
- ③ EXISTING CURB AND GUTTER, TYPE B-6.24
- ④ EXISTING HMA SURFACE COURSE (APPROX. 1.5" THICK)
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- ⑨ PROPOSED LEVELING BINDER (MACHINE METHOD), N70, 1 1/2"
- ⑩ PROPOSED HMA SURFACE COURSE, MIX D, N70, 1 3/4"
- ⑪ BITUMINOUS MATERIALS (PRIME COAT)
- ⑫ AGGREGATE (PRIME COAT)
- ⑬ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT *
- ⑭ SODDING *
- ⑮ MEDIAN REMOVAL AND REPLACEMENT *

* AT LOCATIONS DETERMINED BY THE ENGINEER

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD

TYPICAL SECTIONS

JJB
JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
 Tel. (630) 719-7570 • Fax (630) 719-7589

SCALE: NTS
 DATE: 03/18/08

DRAWN BY: RAJ
 CHECKED BY: JDM

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

STATION	O/S	LT / RT	LENGTH (FT)
1+38	22	LT	4
1+41	41	RT	4
1+43	54	RT	4
1+50	63	LT	4
1+52	69	RT	8
1+61	46	RT	11
1+83	37	LT	4
3+10	33	LT	5
3+81	36	RT	4
3+96	35	RT	4
4+54	34	RT	4
4+70	33	RT	13
4+71	33	LT	14
6+14	33	RT	4
6+35	33	RT	4
6+64	33	LT	4
6+85	34	RT	18
8+65	33	LT	14
11+73	37	LT	13
14+05	33	LT	4
16+30	33	LT	4
16+35	35	RT	4
16+45	33	LT	15
20+73	33	LT	5
20+75	33	RT	9
20+89	33	LT	5
23+28	9	RT	4
23+59	33	LT	4
23+71	33	LT	7
25+78	33	LT	4
25+84	33	RT	29
26+56	33	RT	4
31+07	33	LT	4
31+27	33	LT	4
32+25	33	LT	4
33+25	33	RT	11
33+80	9	LT	7
35+11	33	LT	12
36+96	35	LT	8
37+19	35	RT	5
39+17	35	RT	10
39+21	9	LT	11
40+66	35	RT	4
40+82	35	RT	4
41+62	35	LT	4
41+67	35	RT	10
42+80	35	LT	4
45+34	33	LT	4
45+55	33	RT	8
45+66	33	LT	23
46+49	33	LT	4
47+14	33	RT	4
47+27	33	RT	6
47+29	33	LT	26
47+76	33	LT	4
50+29	33	LT	4
50+42	33	LT	11
50+61	33	LT	15
52+40	33	LT	4
56+18	33	RT	4
56+40	33	RT	8
57+11	33	LT	4
57+70	33	LT	13
58+03	33	LT	29
59+95	33	RT	4
60+21	33	LT	4
60+69	33	RT	4
61+07	33	RT	4
61+28	33	RT	4
62+27	34	LT	4
62+62	9	RT	39
62+66	35	LT	8
63+08	35	RT	5
63+26	9	RT	11
64+28	9	RT	10
65+93	9	RT	4
66+11	35	LT	4
67+80	35	LT	4
68+03	9	RT	4
68+63	35	RT	11
70+88	35	LT	10
70+94	35	RT	4
72+44	45	LT	8
73+05	50	LT	4
73+20	33	RT	7
73+37	33	LT	4
74+31	35	LT	27
75+72	38	LT	5
76+38	39	RT	19
76+73	38	RT	13
76+98	42	LT	4
77+16	38	RT	4
77+33	38	RT	5
77+51	48	LT	5
77+93	11	RT	11
TOTAL =			785

PCC DRIVEWAY PAVEMENT, 7"

STATION	LT / RT	AREA (SY)
3+85	RT	2
37+01	LT	3
42+83	LT	2
45+79	LT	8
TOTAL =		15

STABILIZED DRIVEWAYS, 7"

STATION	LT / RT	AREA (SY)
3+11	LT	1.5
6+17	RT	1.4
6+39	RT	1.4
26+00	RT	9.7
26+59	RT	1.4
41+63	LT	1.4
47+43	LT	8.7
50+49	LT	3.0
50+69	LT	5.0
57+76	LT	4.4
58+18	LT	9.4
62+59	LT	2.7
68+60	RT	3.7
72+50	LT	2.7
73+06	LT	1.4
TOTAL =		60

MEDIAN REMOVAL AND REPLACEMENT

STATION	LT / RT	AREA (SF)
69+33		
TO	6' RT	138
69+56		
TOTAL =		138

SAW CUTS

STATION	LT / RT	LENGTH (FT)
9+95		
TO	RT	125
11+05		
11+65		
TO	RT	140
12+85		
TOTAL =		265

HMA SURFACE REMOVAL - BUTT JOINT

STATION	OFFSET (FT)	LT / RT	AREA (SY)
1+09	0	C/L	46
9+64	46	RT	29
11+33	77	LT	20
11+33	43	RT	27
13+00	45	RT	15
33+33	73	LT	22
78+11	0	C/L	57
TOTAL =			216

STRUCTURE RECONSTRUCTIONS

STA.	O/S	DESCRIPTION	NEW FRAME
56+50	0' C/L	MANHOLES TO BE RECONSTRUCTED	TYPE 1, CL
63+32	9' RT	CATCH BASINS TO BE RECONSTRUCTED	
69+94	13.5' RT	MANHOLES TO BE RECONSTRUCTED	
39+26	9' LT	CATCH BASINS, TYPE A, 4' DIAM.,	
		TYPE 11 FRAME AND GRATE	
39+26	35' RT	CATCH BASINS, TYPE C,	
		TYPE 24 FRAME AND GRATE	
41+73	35' RT	CATCH BASINS, TYPE C,	
		TYPE 24 FRAME AND GRATE	

FRAMES AND LIDS, TYPE 1 OPEN LID

STA.	O/S	DESCRIPTION
70+93	35' LT	FRAMES AND LIDS, TYPE 1 OPEN LID

FRAMES AND LIDS TO BE ADJUSTED

STATION	OFFSET (FT)	LT / RT
1+85	43	RT
70+93	34	LT

FRAMES AND GRATES TO BE ADJUSTED

STATION	OFFSET (FT)	LT / RT
4+79	34	RT
4+79	34	LT
6+93	36	RT
11+73	35	LT
16+50	33	LT
20+81	33	RT
33+31	33	RT
35+18	33	LT
47+35	35	LT
62+87	9.5	RT
64+85	9.5	RT
69+40	9	RT

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

STATION	OFFSET (FT)	LT / RT
1+68	40	RT
1+74	37	RT
2+81	35	RT
7+98	39.5	RT
8+82	36	RT
11+08	40	RT
37+45	0	C/L
41+53	9	LT
50+58	10	LT
50+72	0	C/L
51+68	0	C/L
57+71	0	C/L
58+72	0	C/L
65+32	25	RT
66+77	34	RT
66+68	11	RT
66+75	11	RT
72+94	25.5	RT
73+03	25	RT
74+59	9	RT

REVISIONS	
NAME	DATE

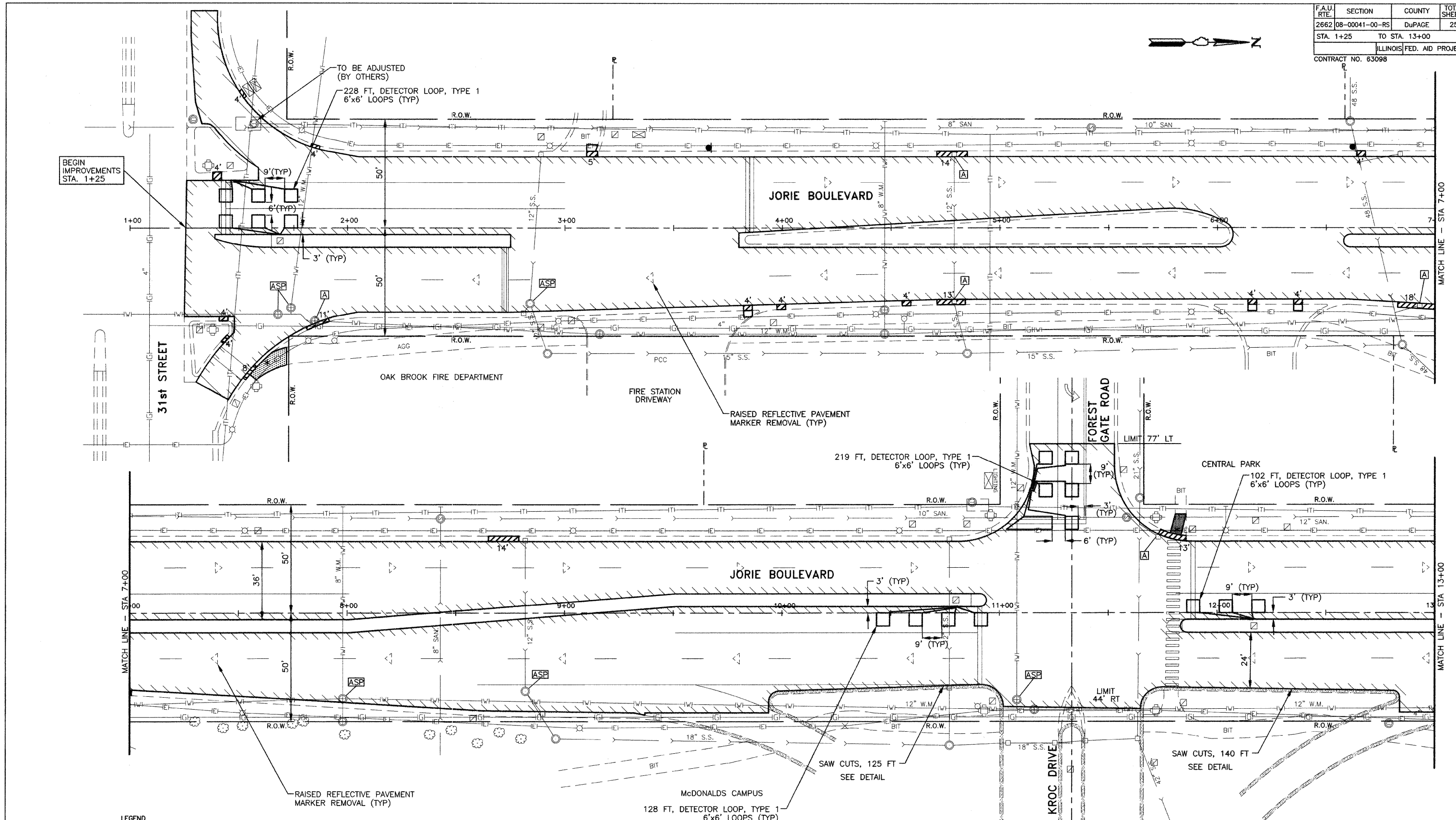
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 2662
JORIE BOULEVARD

SCHEDULES OF QUANTITIES

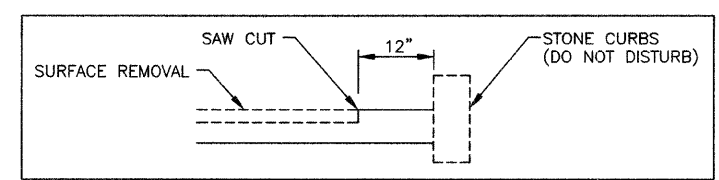
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DATE: 03/25/08

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CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	7
STA. 1+25		TO STA. 13+00		
ILLINOIS		FED. AID PROJECT		
CONTRACT NO. 63098				



- (A) STRUCTURE TO BE ADJUSTED
- (B) STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
- (C) STRUCTURE TO BE RECONSTRUCTED
- (D) STRUCTURE TO BE REMOVED
- (ASP) STRUCTURE TO BE ADJUSTED SPECIAL
- (R) STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
- (RM) STRUCTURE TO BE REMOVED
- HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
- COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
- SIDEWALK REMOVAL
- DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
- * DETECTIBLE WARNINGS

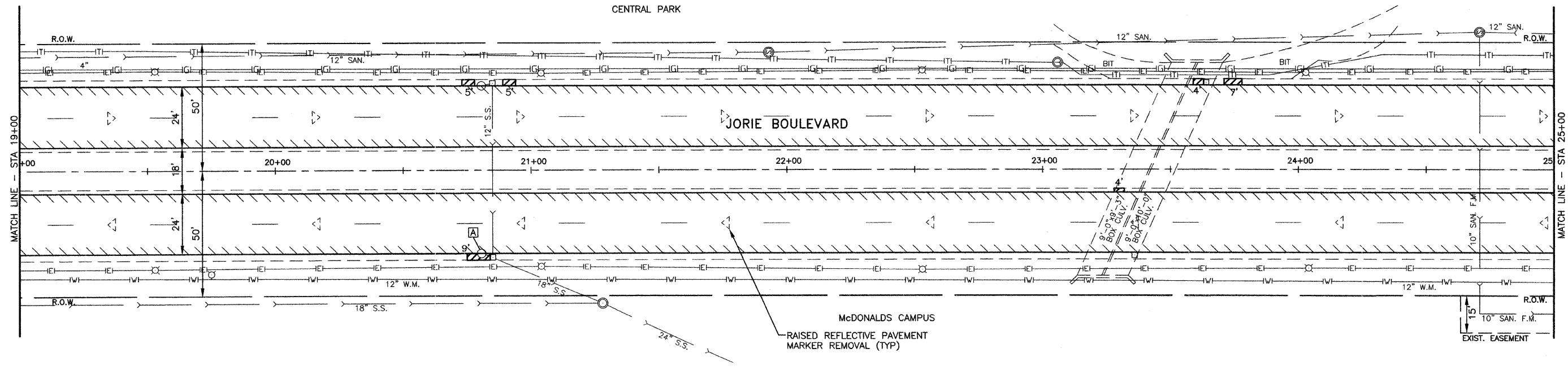
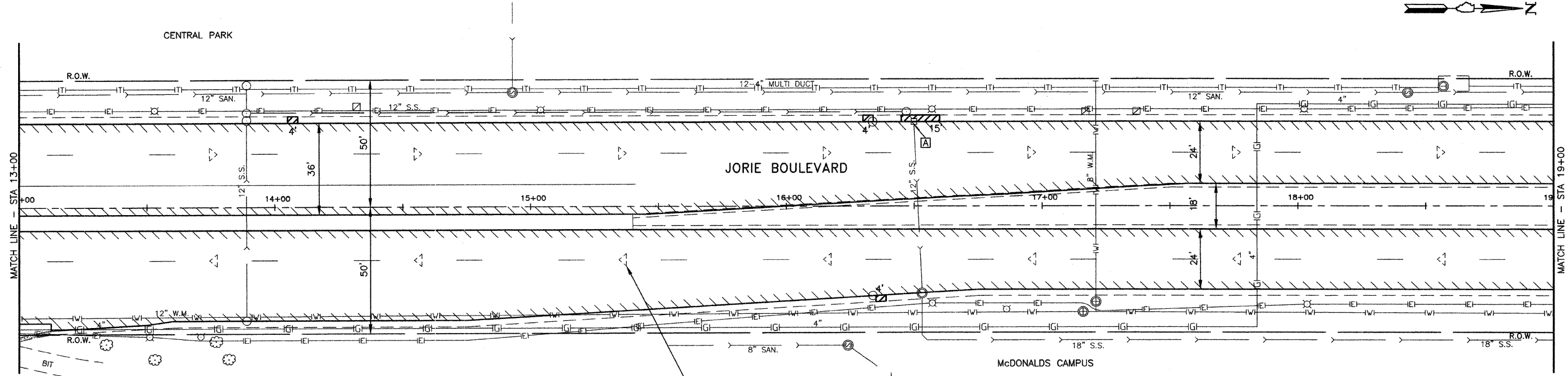
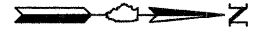


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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD
 PLAN VIEW
 STA 1+25 TO STA 13+00
 SCALE: 1"=20'
 DATE: 03/04/08
 DRAWN BY: BCD
 CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	8
STA. 13+00		TO STA. 25+00		
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 63078				



LEGEND

- | | |
|---|--|
| [A] STRUCTURE TO BE ADJUSTED | [SS] STRUCTURE TO BE ADJUSTED SPECIAL |
| [A] STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID | [R] STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID |
| [R] STRUCTURE TO BE RECONSTRUCTED | [RM] STRUCTURE TO BE REMOVED |
-
- [Hatched Area] HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
 - [Hatched Area] COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
 - [Dotted Area] SIDEWALK REMOVAL
 - [Hatched Area] DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
 - * DETECTIBLE WARNINGS

REVISIONS	
NAME	DATE

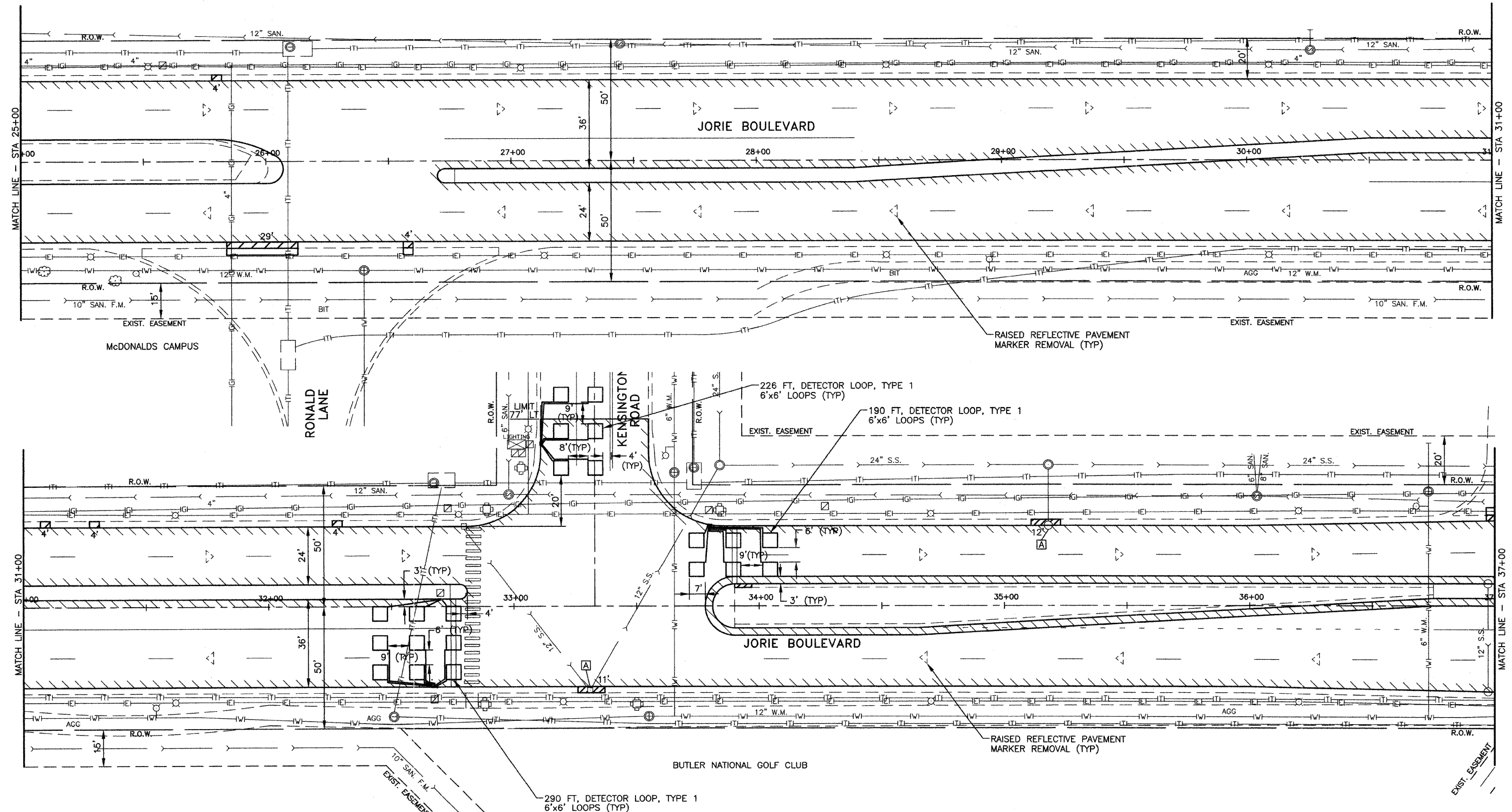
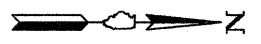
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD

PLAN VIEW
 STA 13+00 TO STA 25+00

SCALE: 1"=20'
 DATE: 03/04/08

DRAWN BY: BCD
 CHECKED BY: JDM

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LEGEND

STRUCTURE TO BE ADJUSTED	STRUCTURE TO BE ADJUSTED SPECIAL
STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
STRUCTURE TO BE RECONSTRUCTED	STRUCTURE TO BE REMOVED

HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
SIDEWALK REMOVAL
DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
* DETECTIBLE WARNINGS

REVISIONS	
NAME	DATE

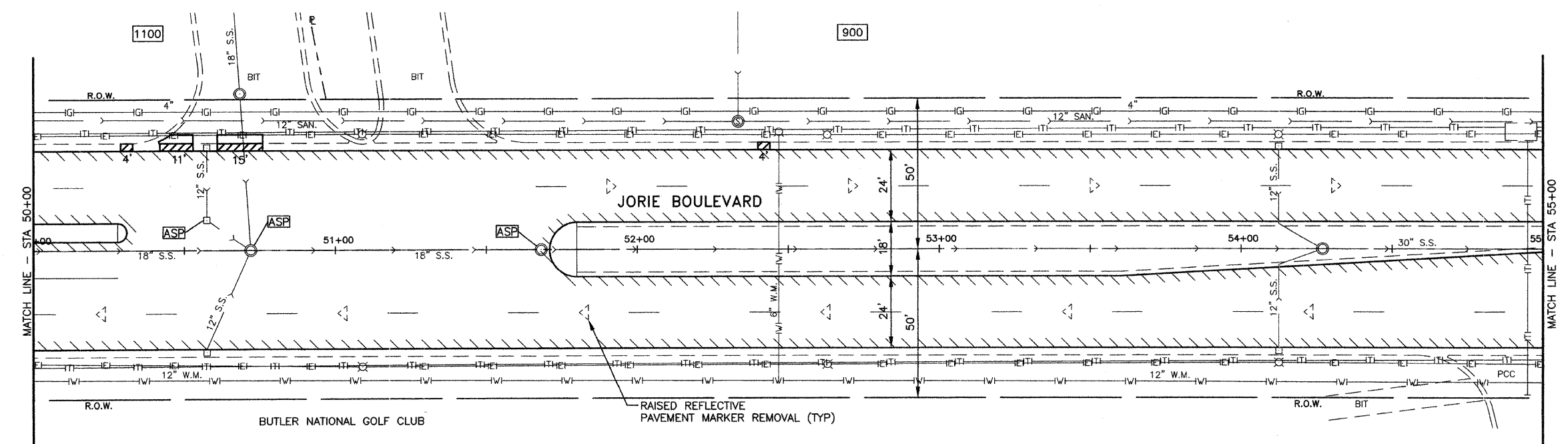
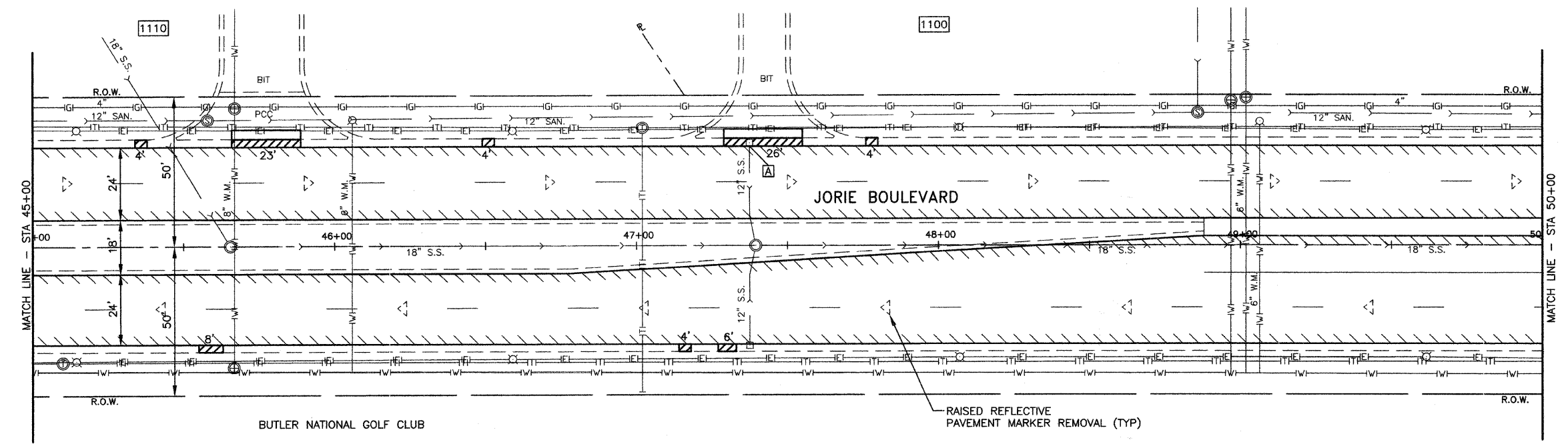
ILLINOIS DEPARTMENT OF TRANSPORTATION
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PLAN VIEW
 STA 25+00 TO STA 37+00

SCALE: 1"=20'
 DATE: 03/04/08

DRAWN BY: BCD
 CHECKED BY: JDM

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LEGEND

- | | |
|---|--|
| Ⓜ STRUCTURE TO BE ADJUSTED | Ⓜ STRUCTURE TO BE ADJUSTED SPECIAL |
| Ⓜ STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID | Ⓜ STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID |
| Ⓜ STRUCTURE TO BE RECONSTRUCTED | Ⓜ STRUCTURE TO BE REMOVED |
-
- ▨ HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
 - ▨ COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
 - ▨ SIDEWALK REMOVAL
 - ▨ DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
 - * DETECTIBLE WARNINGS

REVISIONS	
NAME	DATE

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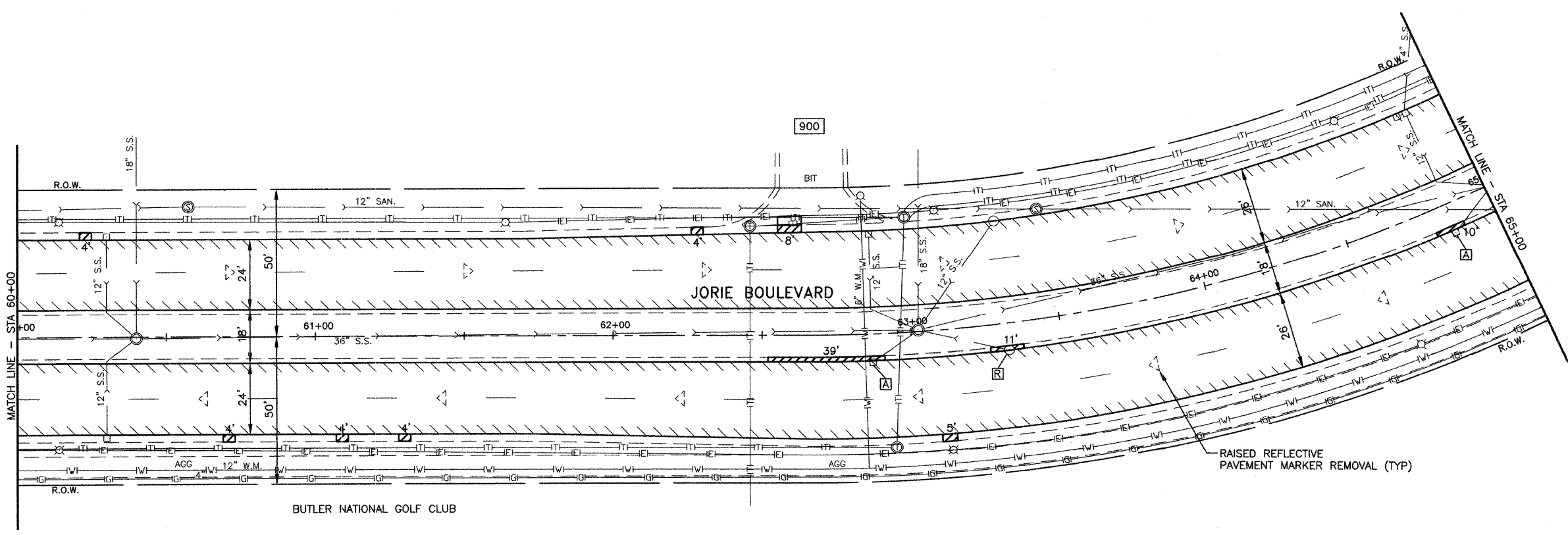
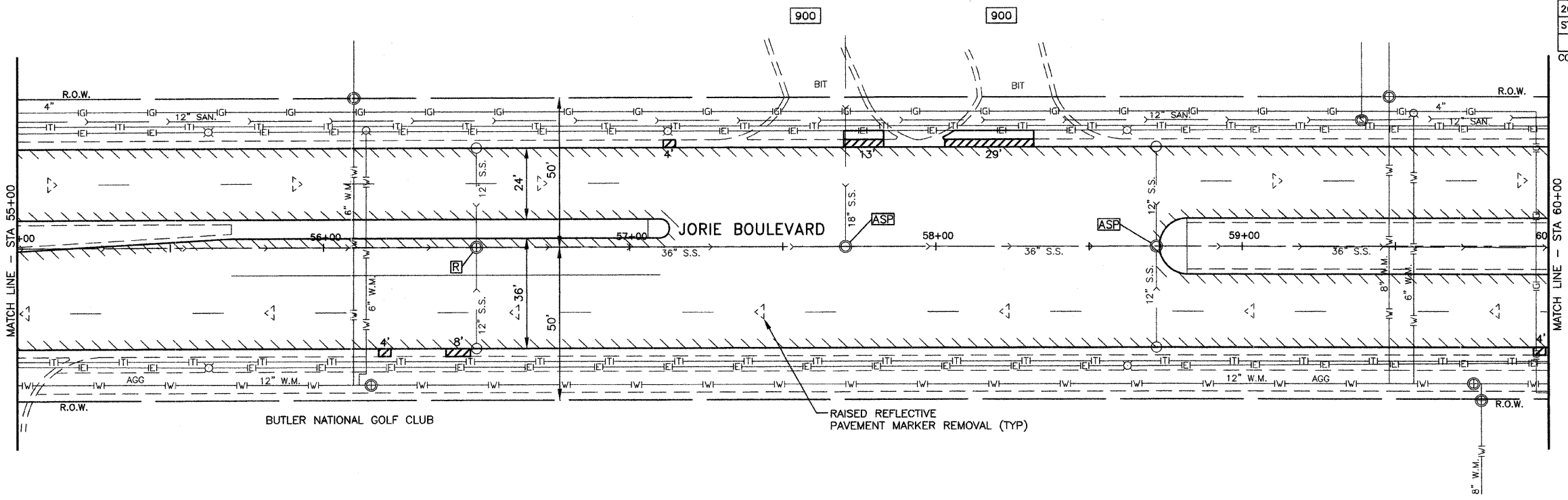
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD

PLAN VIEW
 STA 45+00 TO STA 55+00

SCALE: 1"=20'
 DATE: 03/04/08

DRAWN BY: BCD
 CHECKED BY: JDM

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	12
STA. 55+00		TO STA. 65+00		
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 6309B				



LEGEND

- A STRUCTURE TO BE ADJUSTED
- R STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
- REM STRUCTURE TO BE REMOVED
- ASP STRUCTURE TO BE ADJUSTED SPECIAL
- R STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID
- REM STRUCTURE TO BE REMOVED
- Hatched HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
- Hatched COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
- Dotted SIDEWALK REMOVAL
- Hatched DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
- * DETECTIBLE WARNINGS

REVISIONS	
NAME	DATE

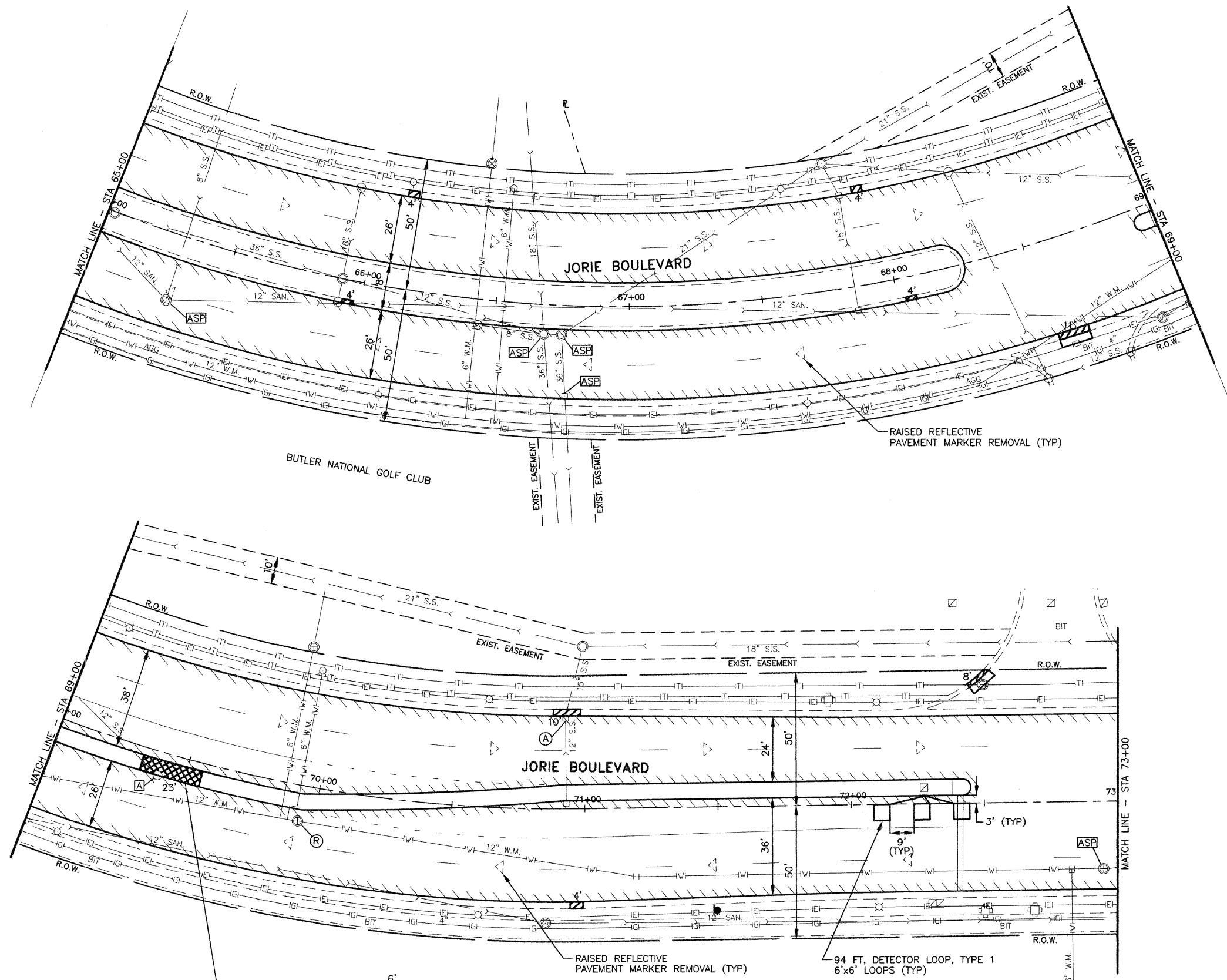
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD

PLAN VIEW
 STA 55+00 TO STA 65+00

SCALE: 1"=20'
 DATE: 03/04/08

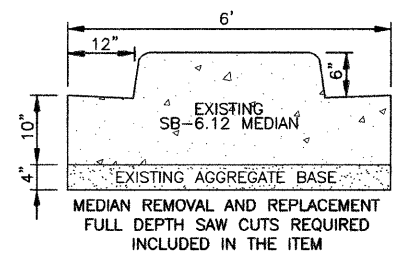
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LEGEND

- | | |
|---|--|
| [A] STRUCTURE TO BE ADJUSTED | [ASP] STRUCTURE TO BE ADJUSTED SPECIAL |
| [A] STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID | [R] STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID |
| [R] STRUCTURE TO BE RECONSTRUCTED | [REM] STRUCTURE TO BE REMOVED |
-
- [---] HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
 - [---] COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
 - [---] SIDEWALK REMOVAL
 - [---] DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
 - * DETECTIBLE WARNINGS



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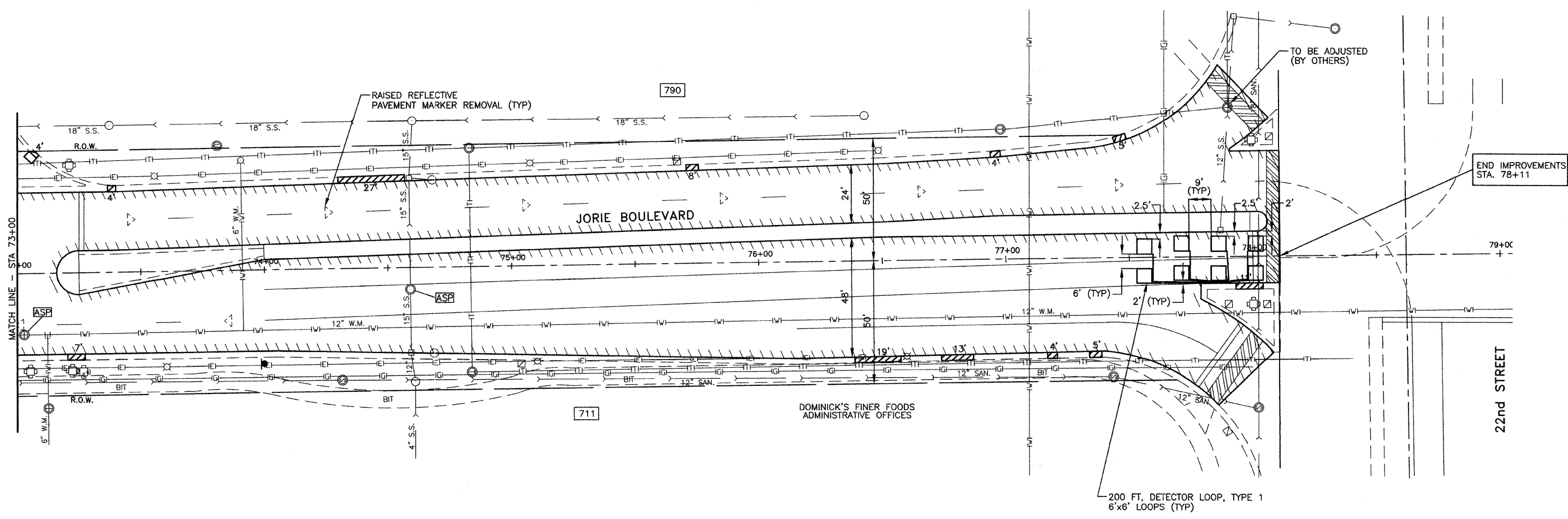
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
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 JORIE BOULEVARD

PLAN VIEW
 STA. 65+00 TO STA. 73+00

SCALE: 1"=20'
 DATE: 03/04/08

DRAWN BY: BCD
 CHECKED BY: JDM



LEGEND

- | | |
|---|--|
| ⓐ STRUCTURE TO BE ADJUSTED | Ⓢ STRUCTURE TO BE ADJUSTED SPECIAL |
| ⓐ STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID | Ⓢ STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID |
| Ⓡ STRUCTURE TO BE RECONSTRUCTED | Ⓢ STRUCTURE TO BE REMOVED |
-
- ▨ HOT-MIX ASPHALT SURFACE REMOVAL LIMIT
 - ▨ COMBINATION CURB AND GUTTER REMOVAL OR CURB REMOVAL
 - ▨ SIDEWALK REMOVAL
 - ▨ DRIVEWAY REMOVAL AND REPLACEMENT AT CURB REPAIRS
 - * DETECTIBLE WARNINGS

REVISIONS	
NAME	DATE

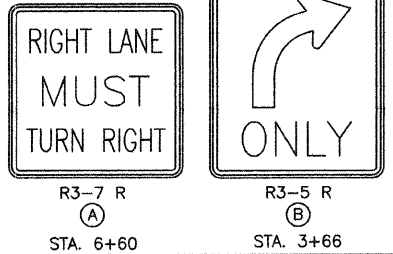
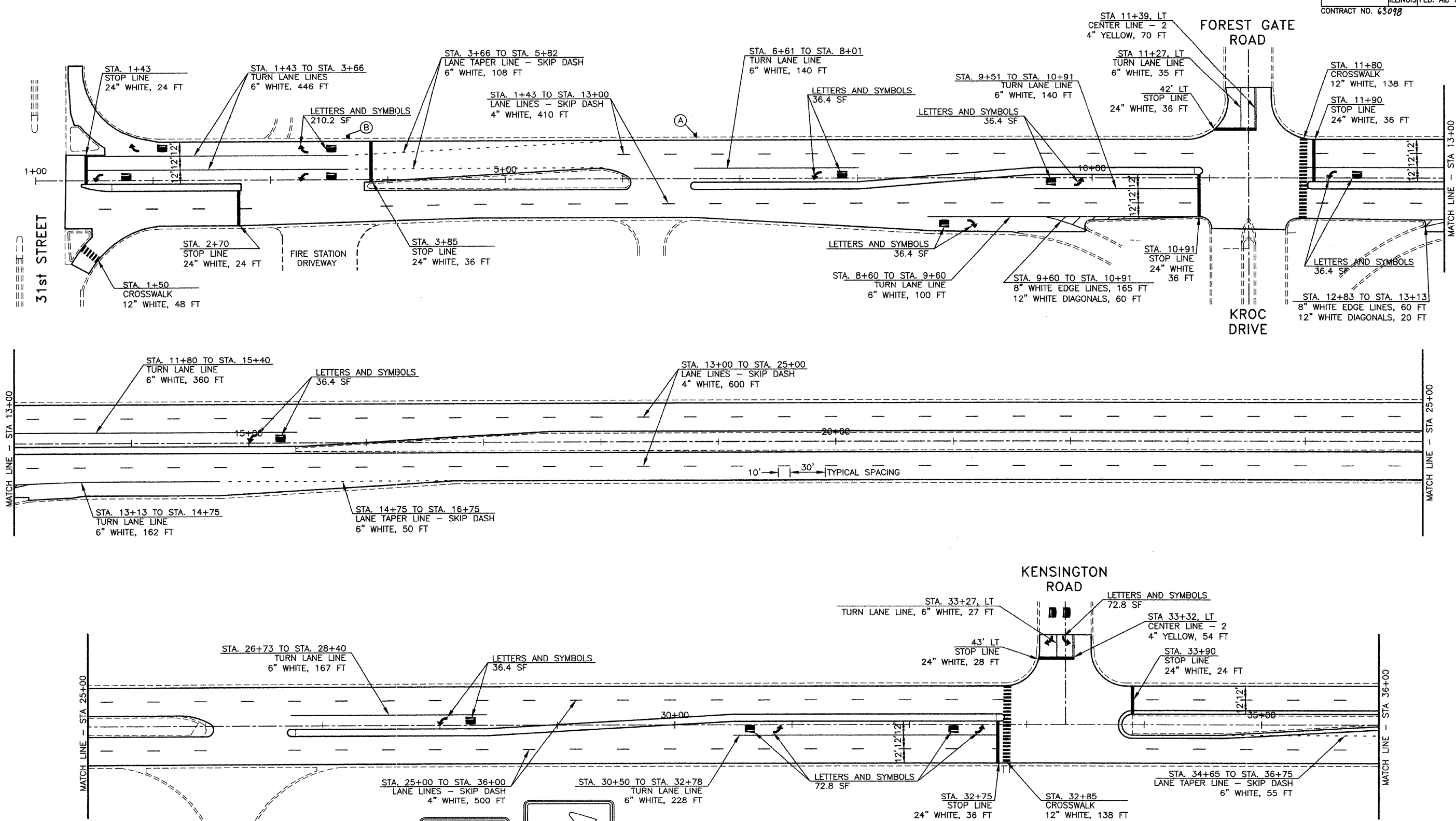
ILLINOIS DEPARTMENT OF TRANSPORTATION
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 JORIE BOULEVARD

PLAN VIEW
 STA. 73+00 TO STA. 78+11

SCALE: 1"=20'
 DATE: 03/04/08

DRAWN BY: BCD
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REVISIONS	
NAME	DATE

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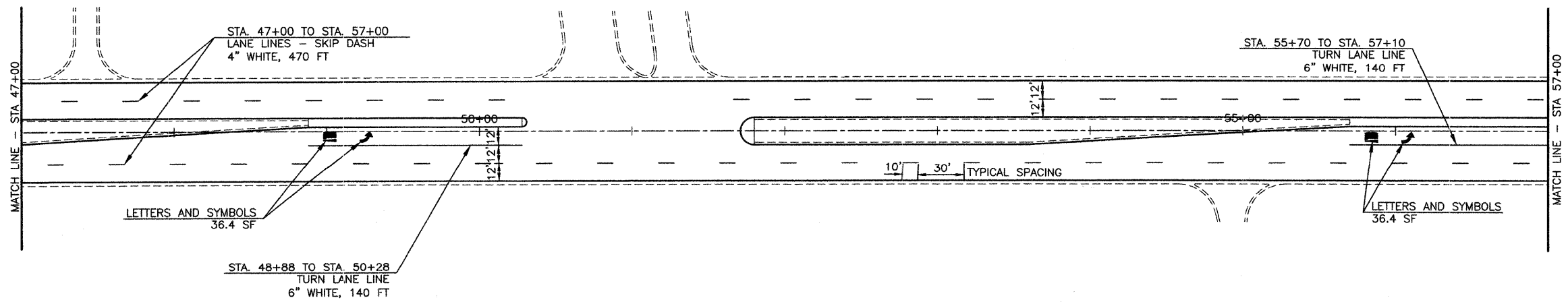
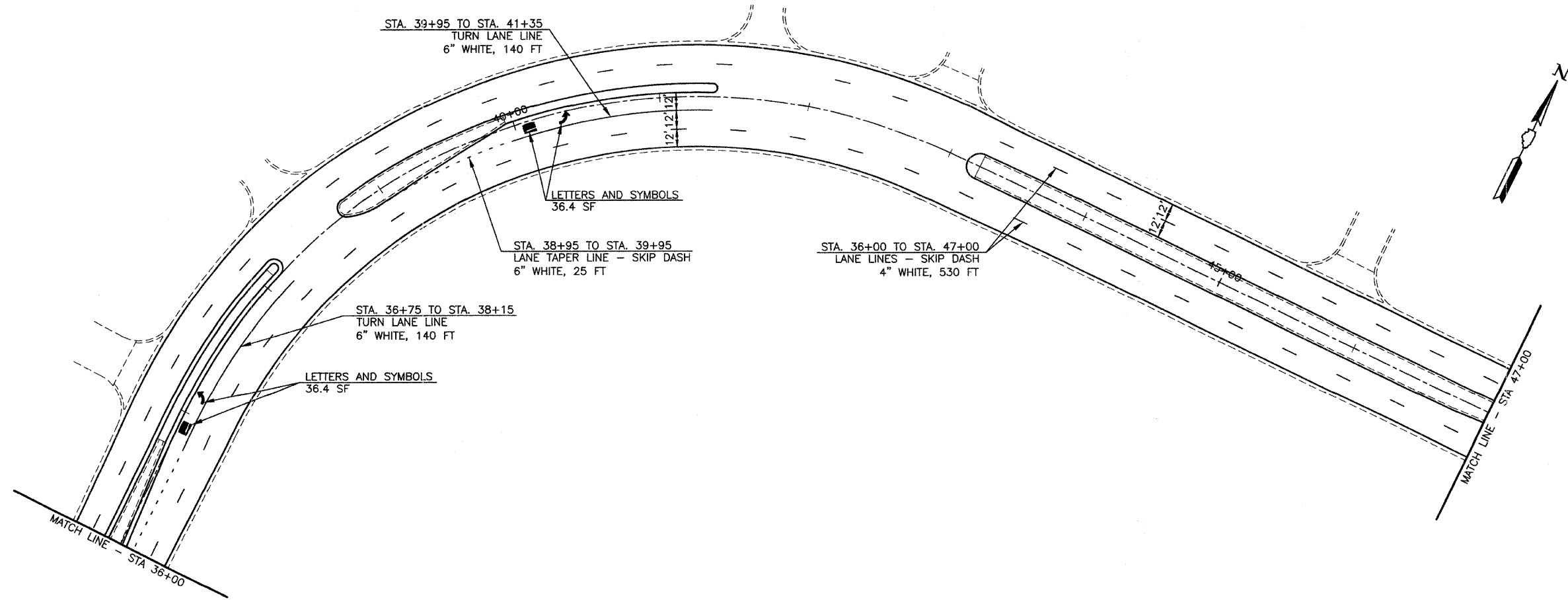
PAVEMENT MARKING PLAN
 STA 1+25 TO STA 36+00

SCALE: 1"=40'
 DATE: _____

DRAWN BY: BCD
 CHECKED BY: JDM

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	16
STA. 36+00		TO STA. 57+00		
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 63098				



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
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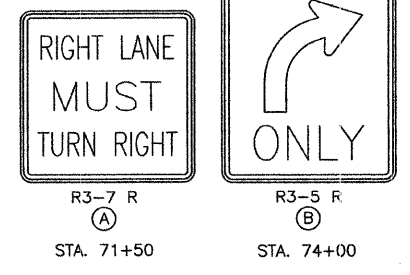
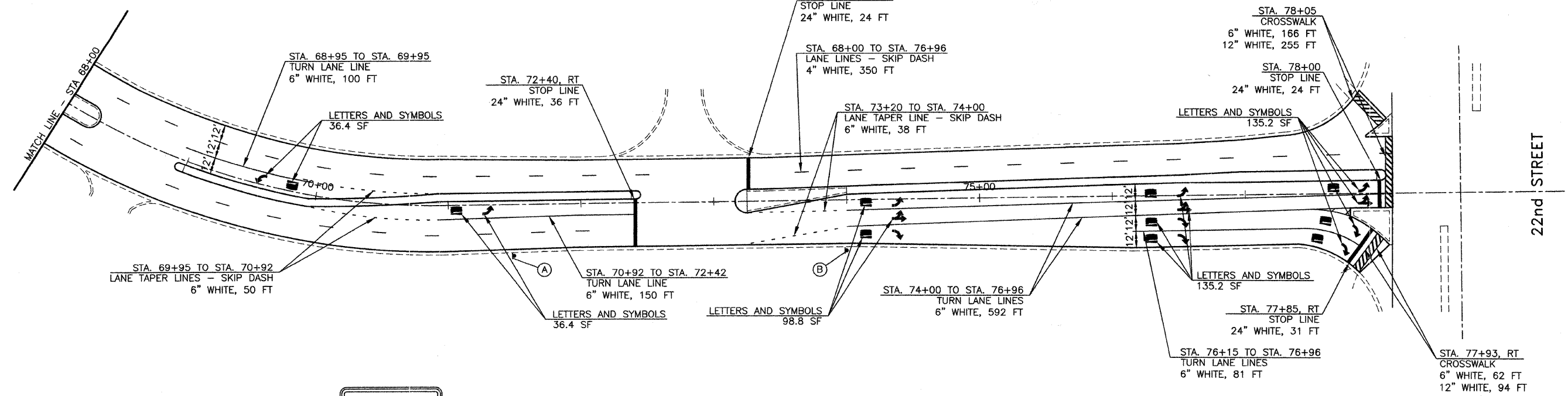
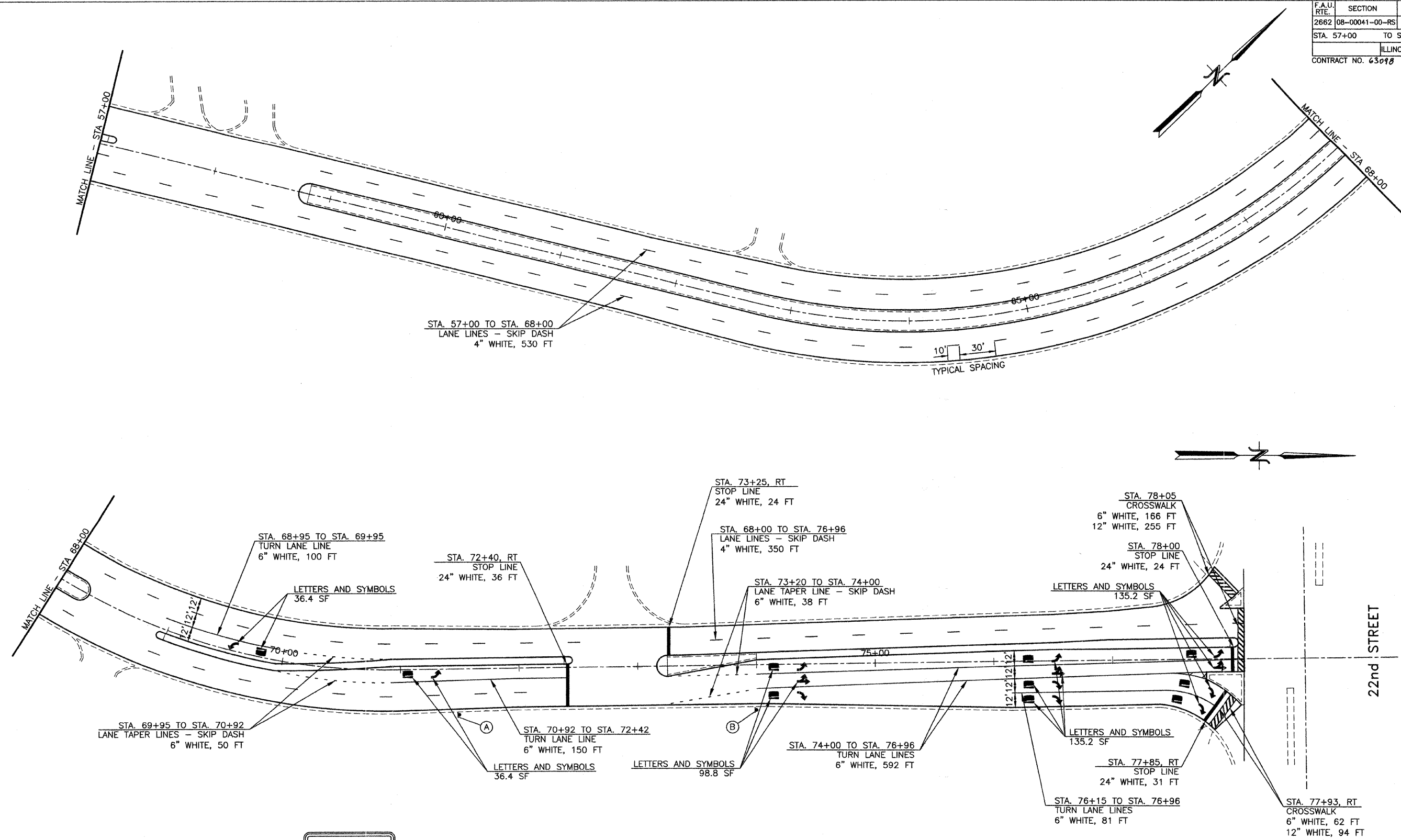
PAVEMENT MARKING PLAN
 STA 36+00 TO STA 57+00

SCALE: 1"=40'
 DATE: _____

DRAWN BY: BCD
 CHECKED BY: JDM

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	17
STA. 57+00		TO STA. 78+11		
ILLINOIS FED. AID PROJECT				
CONTRACT NO. 63098				



REVISIONS	
NAME	DATE

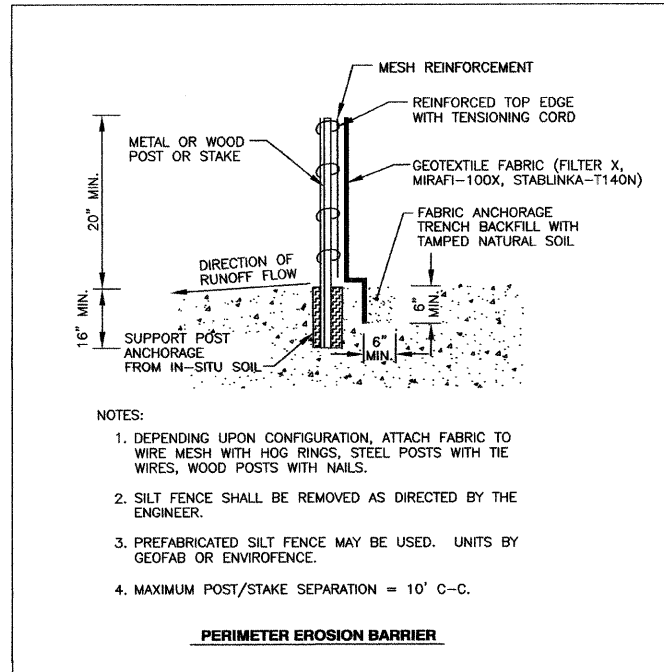
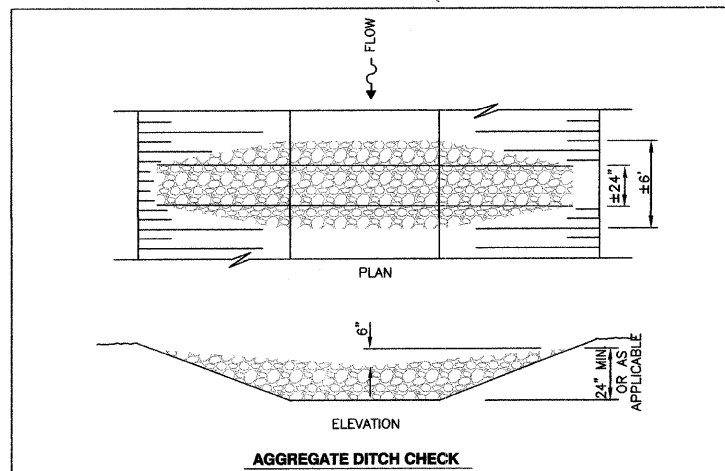
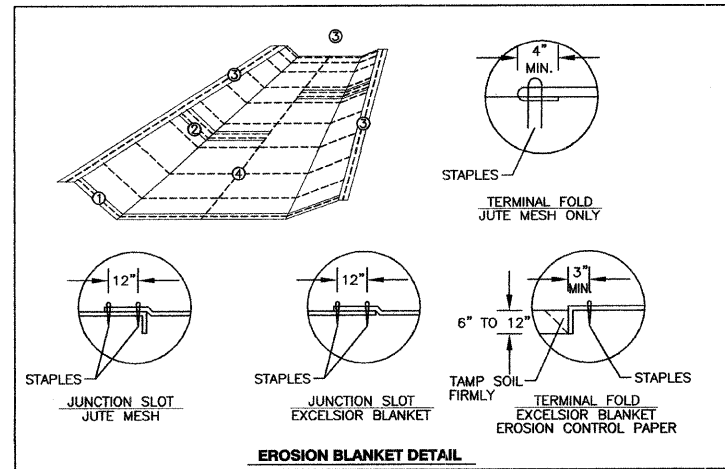
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 2662
 JORIE BOULEVARD

PAVEMENT MARKING PLAN
 STA. 57+00 TO STA. 78+11

SCALE: 1"=40'
 DATE: _____

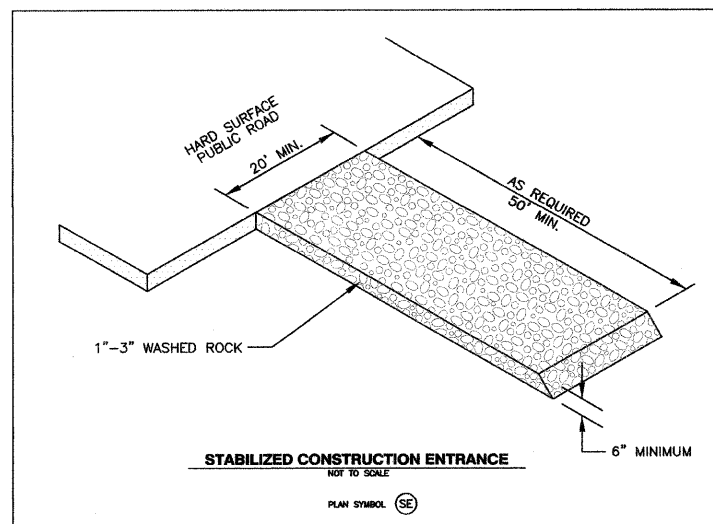
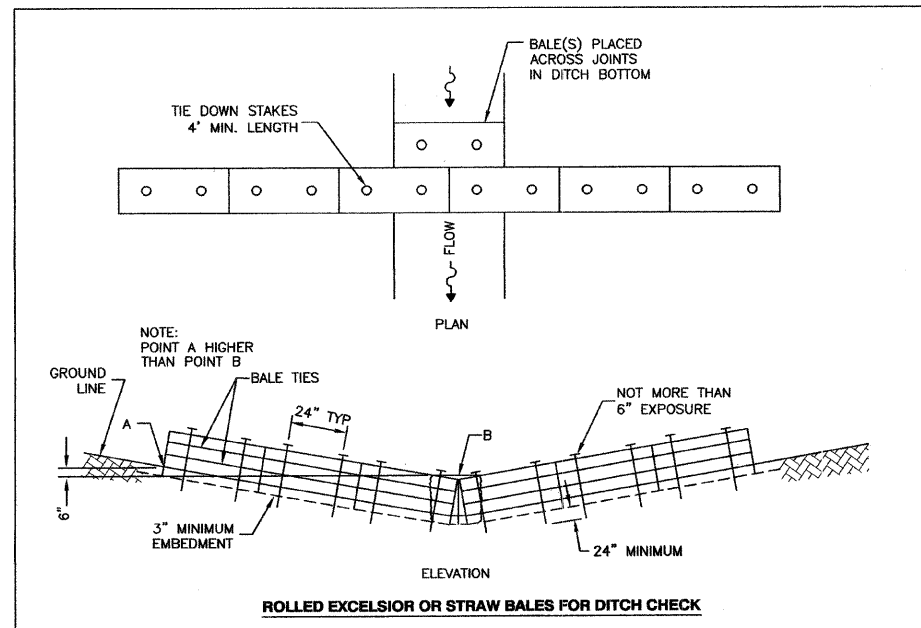
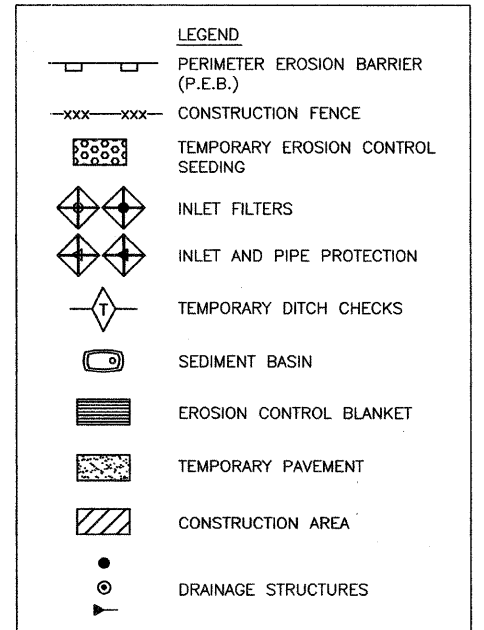
DRAWN BY: BCD
 CHECKED BY: JDM

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TEMPORARY EROSION CONTROL NOTES:

1. SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE LAND IS OTHERWISE DISTURBED ON THE SITE.
2. IF A STOCKPILE IS TO REMAIN IN PLACE FOR MORE THAN 3 DAYS, THEN SEDIMENT AND EROSION CONTROL DEVICES SHALL BE INSTALLED DURING CONSTRUCTION.
3. STORM SEWER INLETS SHALL BE PROTECTED WITH SEDIMENT TRAPPING OR FILTER CONTROL DEVICES DURING CONSTRUCTION.
4. THE QUANTITIES SHOWN FOR INLET AND PIPE PROTECTION AND TEMPORARY DITCH CHECKS ARE SUFFICIENT FOR ONE (1) SETUP AND THREE (3) REPLACEMENTS.
5. THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL MEASURES IN SERVICEABLE CONDITION AT ALL TIMES. EROSION CONTROL MEASURES WILL BE INSPECTED WITHIN 24 HOURS OF ANY STORM EXCEEDING 0.5 INCHES OF PRECIPITATION.
6. ALL CONSTRUCTION ACTIVITIES SHALL BE IN ACCORDANCE WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM STORM WATER PERMIT FOR THIS PROJECT.
7. AS WORK PROGRESSES, ALL SLOPES 1:3 OR GREATER SHALL RECEIVE TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET IMMEDIATELY. ALL FLATTER AREAS THAT DO NOT HAVE A COVER OF VEGETATION, AND WHERE NO FURTHER WORK IS TO OCCUR FOR ONE MONTH OR MORE, SHALL BE TEMPORARILY SEEDED WITHIN SEVEN (7) CALENDAR DAYS UNLESS OTHERWISE DIRECTED BY THE ENGINEER. A SUFFICIENT QUANTITY OF TEMPORARY EROSION CONTROL SEEDING IS INCLUDED TO COVER THE LIMITS OF PERMANENT LANDSCAPING.
8. TEMPORARY DITCH CHECKS SHALL BE PLACED IMMEDIATELY AFTER DITCH GRADING (OR CLEANING AND REGRADING) IS COMPLETED.
9. ALL PROPOSED OPEN LID DRAINAGE STRUCTURES SHALL BE PROTECTED AS DIRECTED BY THE ENGINEER WITH INLET FILTERS, AND THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "INLET FILTERS". ALL OPEN END CULVERTS SHALL BE PROTECTED AS DIRECTED BY THE ENGINEER, WHICH WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "INLET AND PIPE PROTECTION".
10. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED.
11. ANY SOIL, MUD OR DEBRIS WASHED, TRACKED, OR DEPOSITED ONTO THE STREET SHALL BE REMOVED PRIOR TO THE END OF THE DAY.



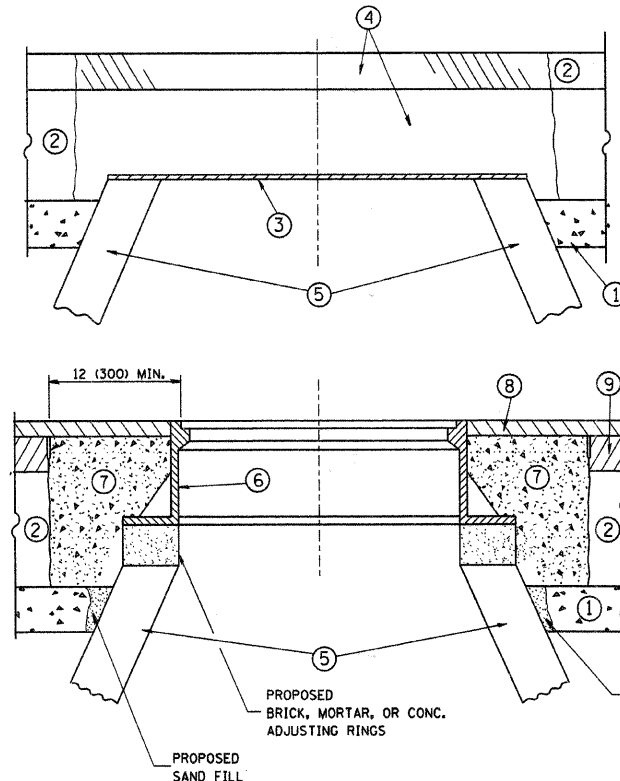
STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY	JUNE	JULY	AUG.	SEP.	OCT.	NOV.	DEC.
PERMANENT SEEDING			+ A			*	*					
DORMANT SEEDING	- B										+ B	
TEMPORARY SEEDING			+ C			+ D						
SODDING			+ E**									
MULCHING	- F											

- A KENTUCKY BLUEGRASS 90 LBS/ACRE MIXED WITH PERENNIAL RYEGRASS 30 LBS/ACRE
- B KENTUCKY BLUEGRASS 135 LBS/ACRE MIXED WITH PERENNIAL RYEGRASS 45 LBS/ACRE + 2 TONS STRAW MULCH/ACRE
- C SPRING OATS 100 LBS/ACRE
- D WHEAT OR CEREAL RYE 150 LBS/ACRE
- E SOD
- F STRAW MULCH 2 TONS/ACRE

* IRRIGATION NEEDED DURING JUNE AND JULY.
 ** IRRIGATION NEEDED FOR 2 TO 3 WEEKS AFTER APPLYING SOD.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.U. 2662 JORIE BOULEVARD
NAME	DATE	
		STORM WATER POLLUTION PREVENTION PLAN
SCALE: NTS		DRAWN BY: RAJ
DATE: 03/25/08		CHECKED BY: JDM

F.A. DIST.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
266208-00041-00-RS	DuPAGE		25	17
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

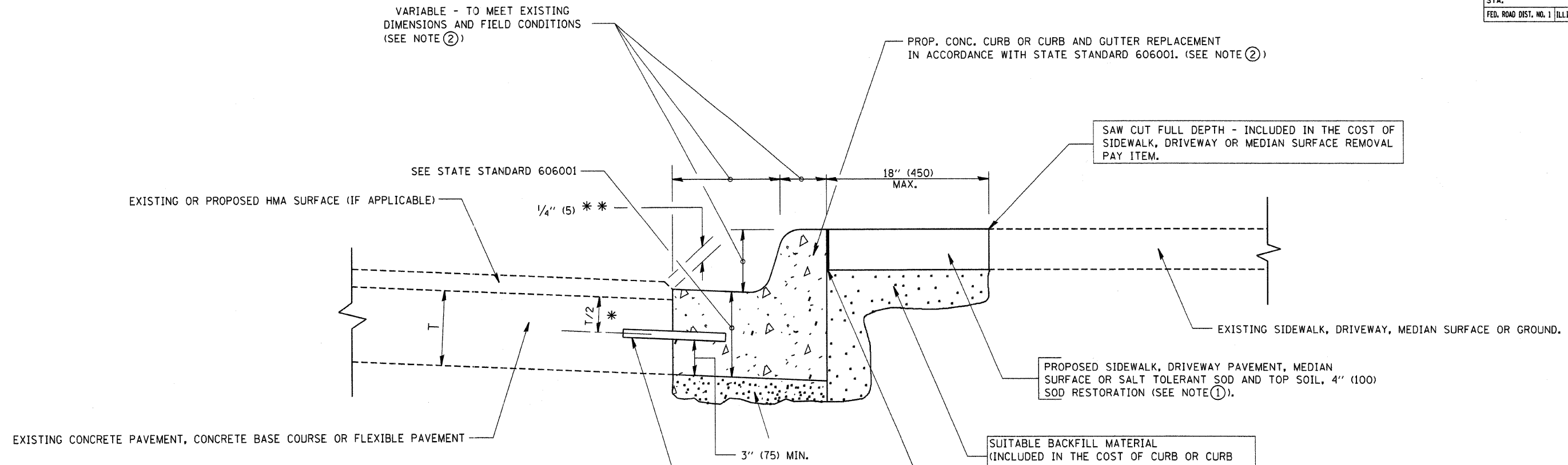
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

BD600-03 (BD-8)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	20
STA.		TO STA.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS		
NAME	DATE	
A. HOUSEH	03/11/94	
R. SHAH	02/24/95	
R. SHAH	03/02/95	
R. SHAH	08/19/96	
R. SHAH	09/12/96	
R. SHAH	09/19/96	
R. SHAH	10/03/96	
A. ABBAS	03/21/97	
M. GOMEZ	01/22/01	
R. BORO	01/01/07	

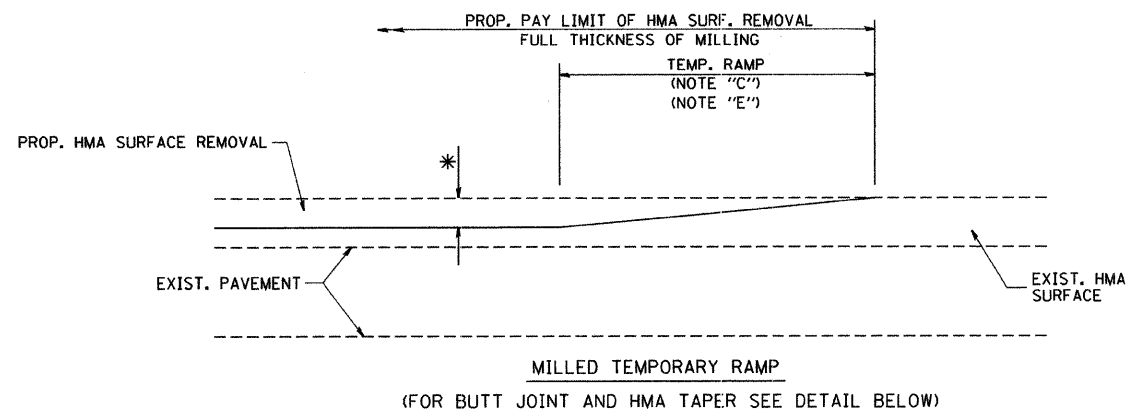
ILLINOIS DEPARTMENT OF TRANSPORTATION
 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
 HORIZ.
 DRAWN BY
 CHECKED BY
 BD600-06 (BD-24)

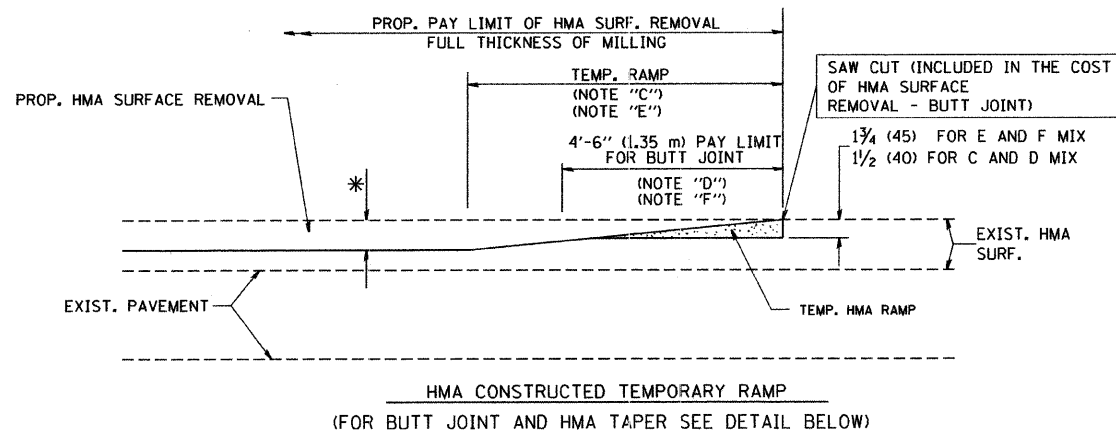
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

PLOT DATE = 3/25/2007
 PLOT SCALE = 800000 / IN.
 USER NAME = bbornd

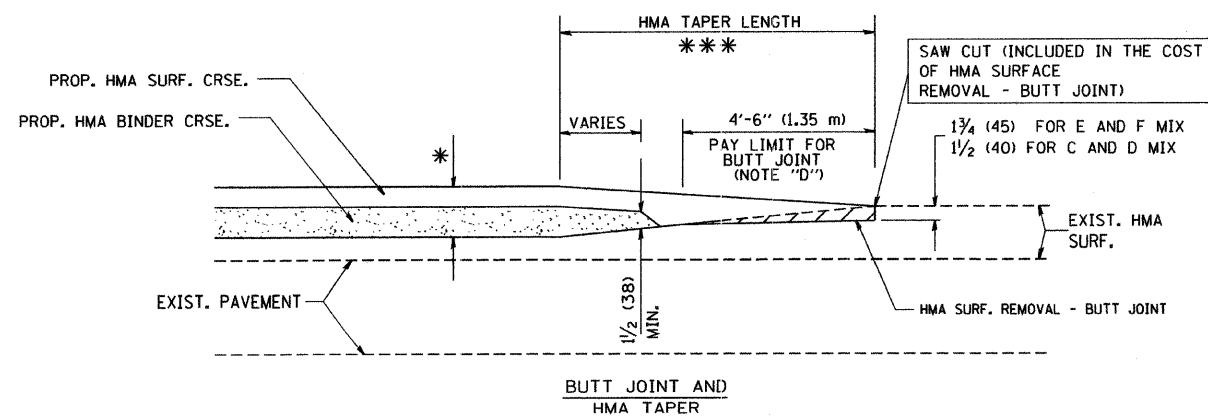
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STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



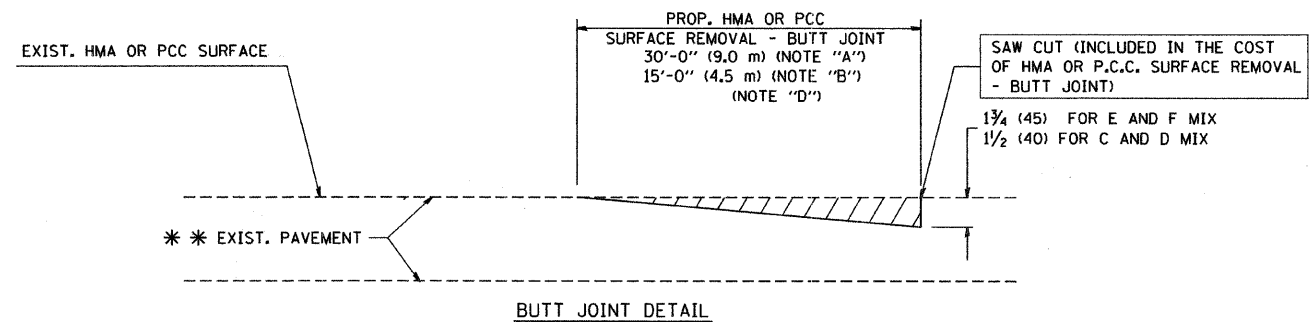
OPTION 1



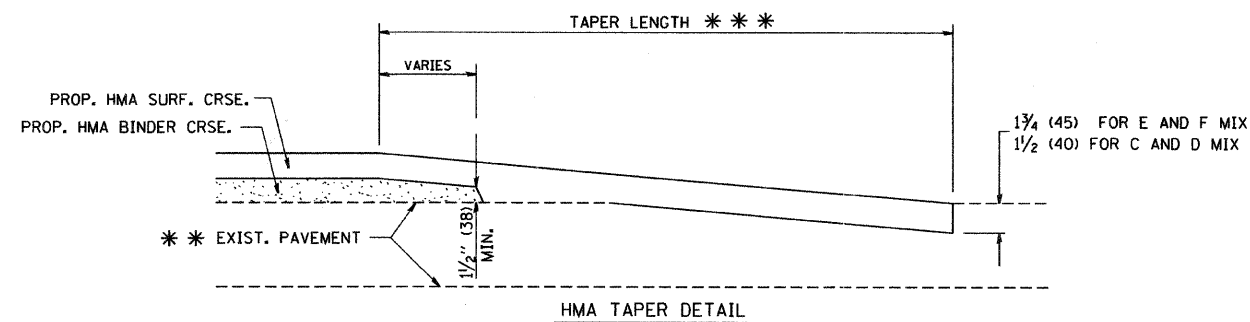
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

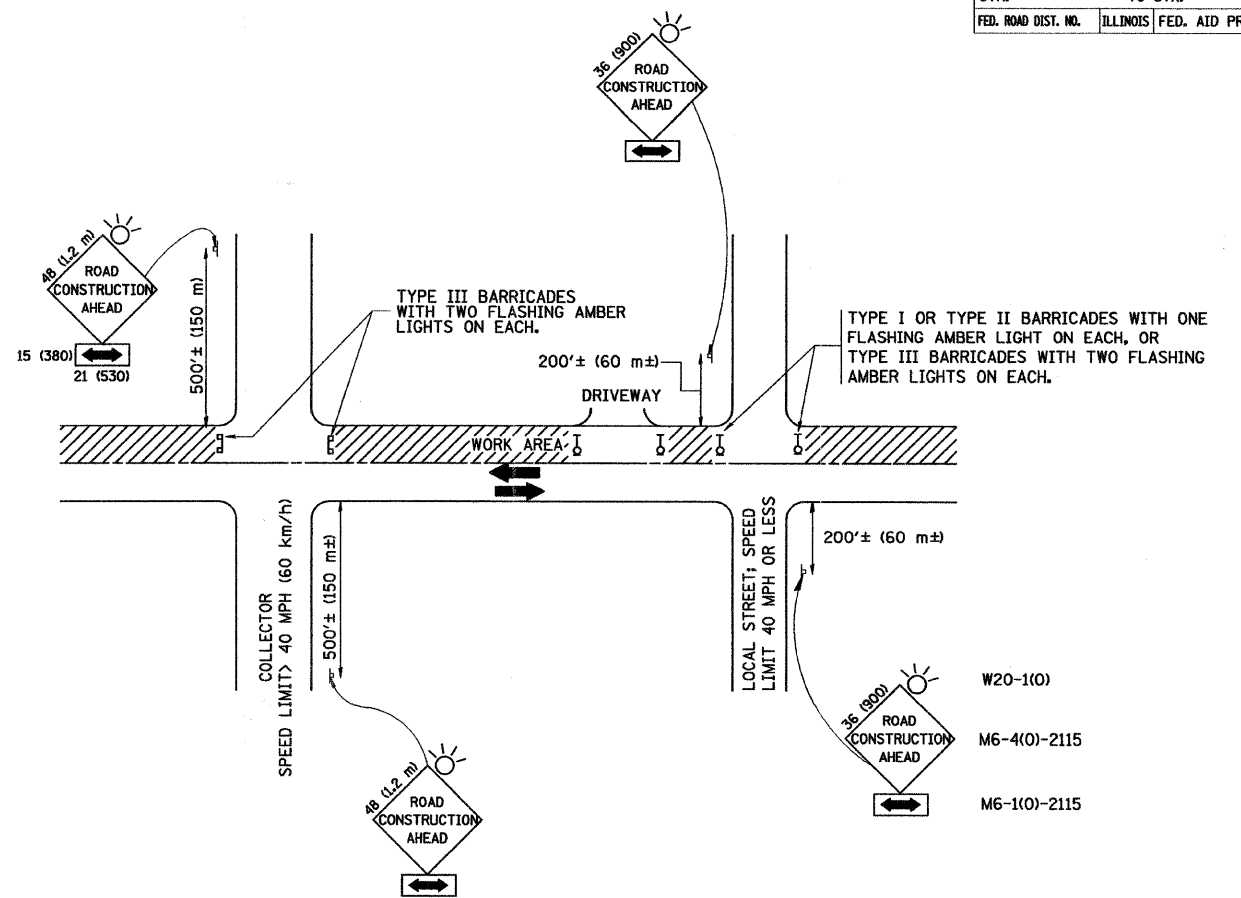
BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

BD400-05 (VI-BD32)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DuPAGE	25	22
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

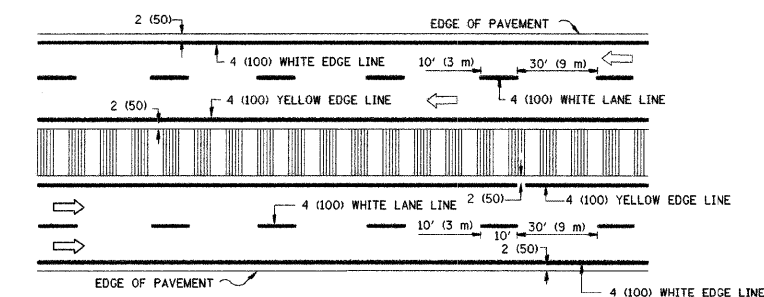
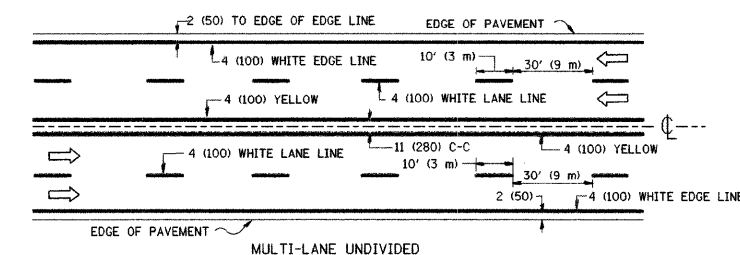
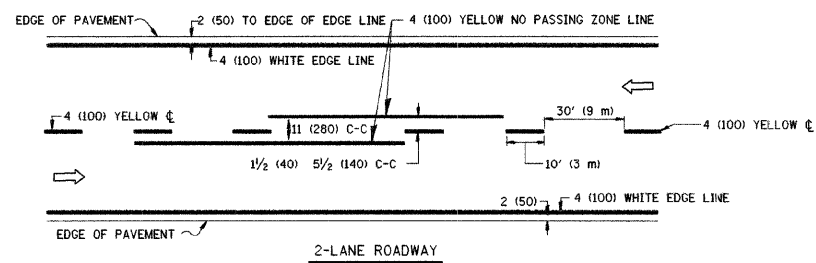
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DRAWN BY

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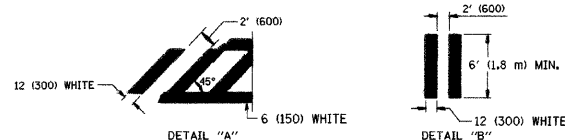
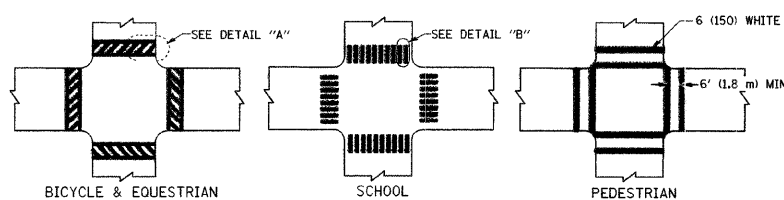
TC-10

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	COOK	25	23
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

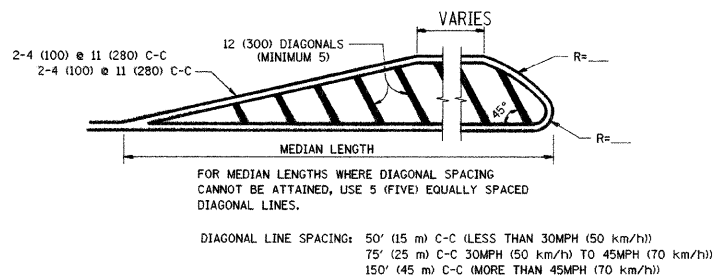
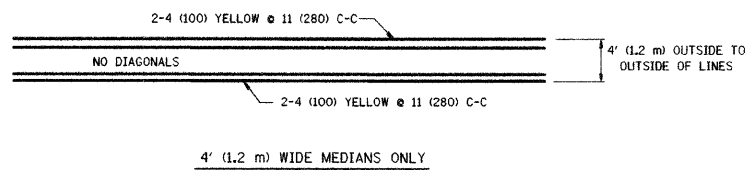


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

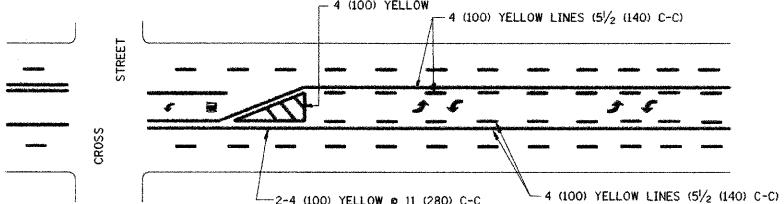


TYPICAL CROSSWALK MARKING



DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

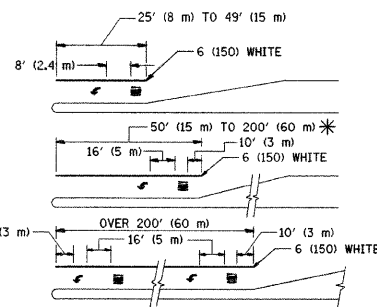


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

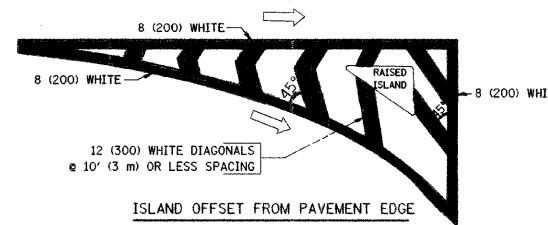


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)

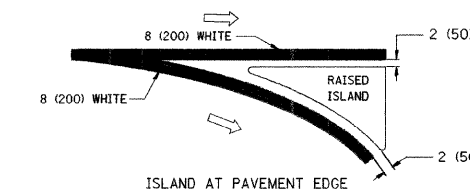
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
C. JUCIUS	09-09-09

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

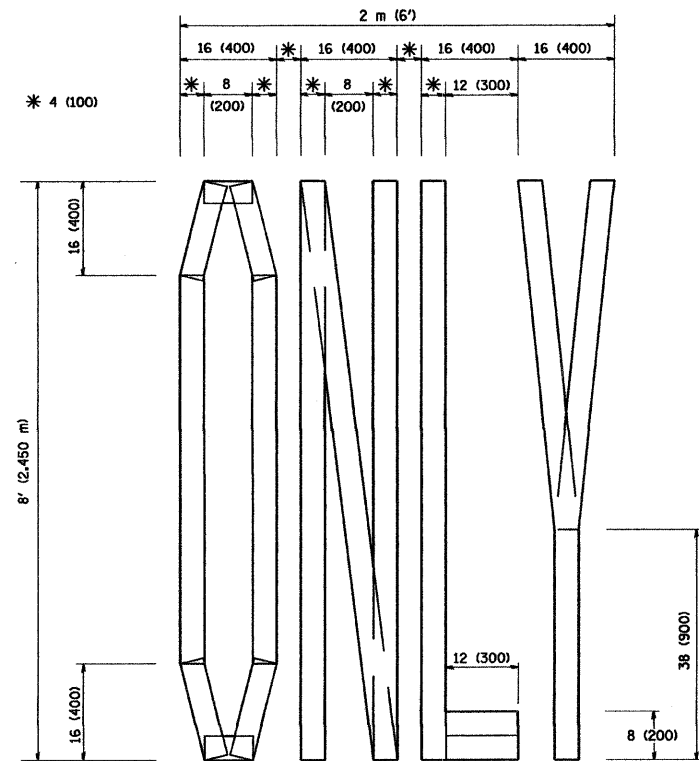
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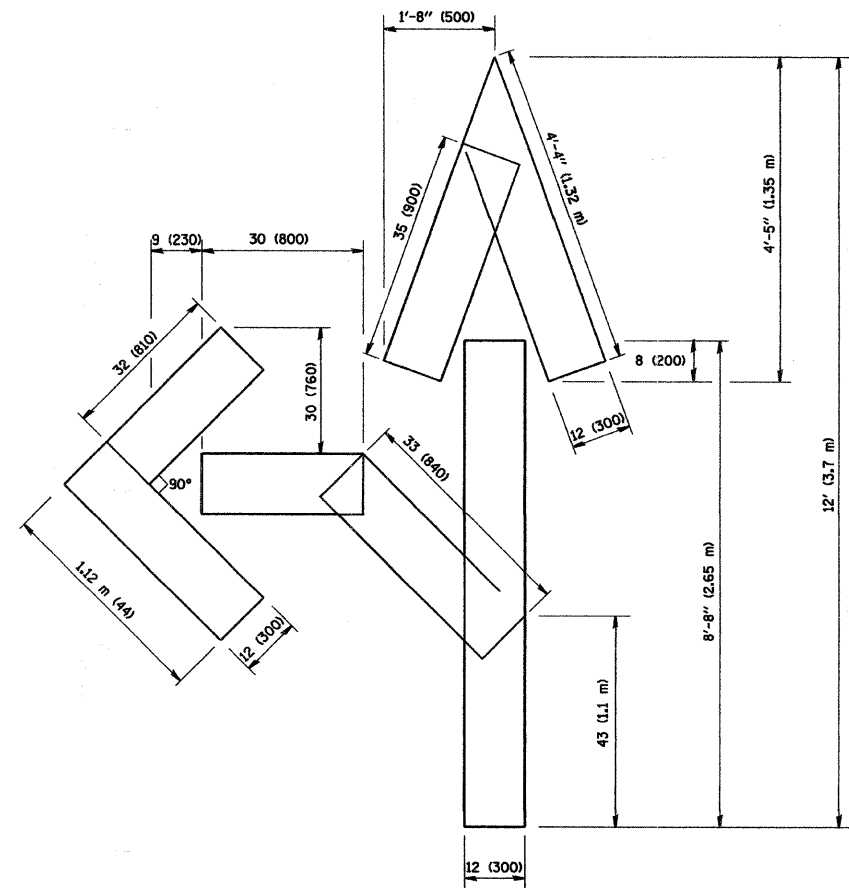
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TC-13

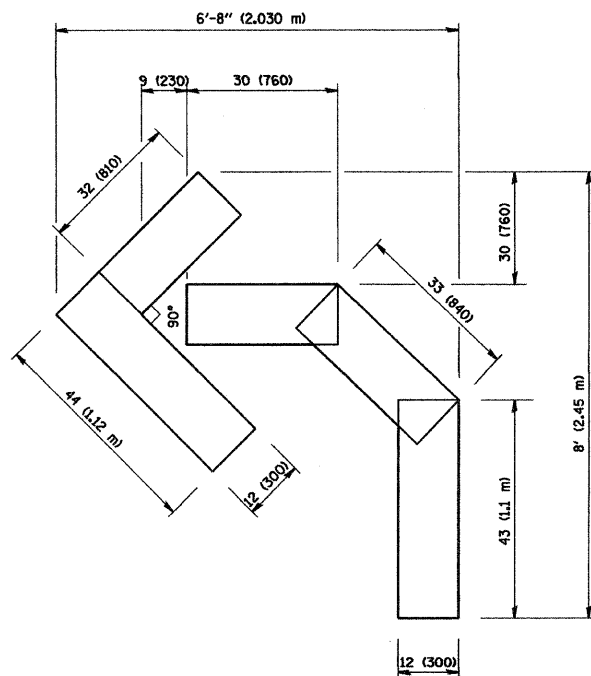
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-00041-00-RS	DUPAGE	25	24
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING**

SCALE: NONE

DRAWN BY CADD

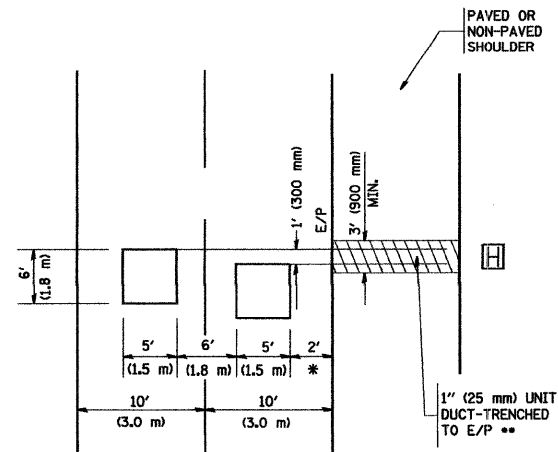
CHECKED BY

TC-16

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2662	08-0041-00-RS	DuPAGE	25	25
STA. TO STA.				
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

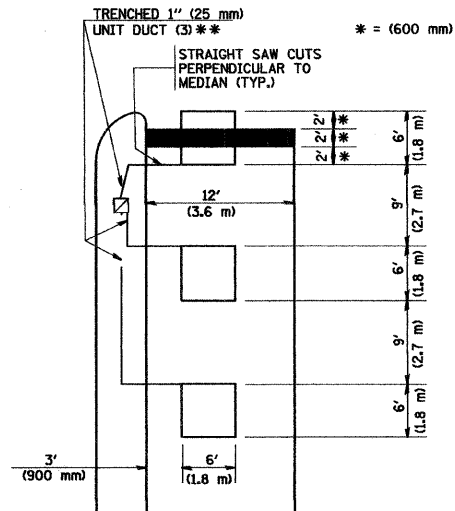


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

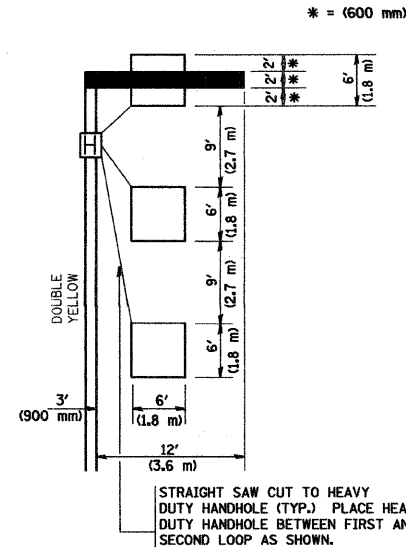
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).

- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

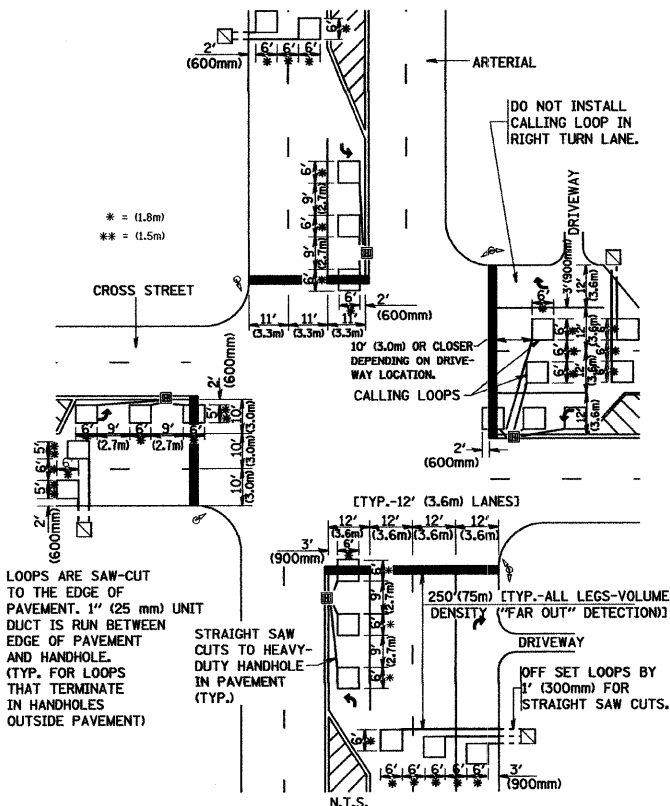
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

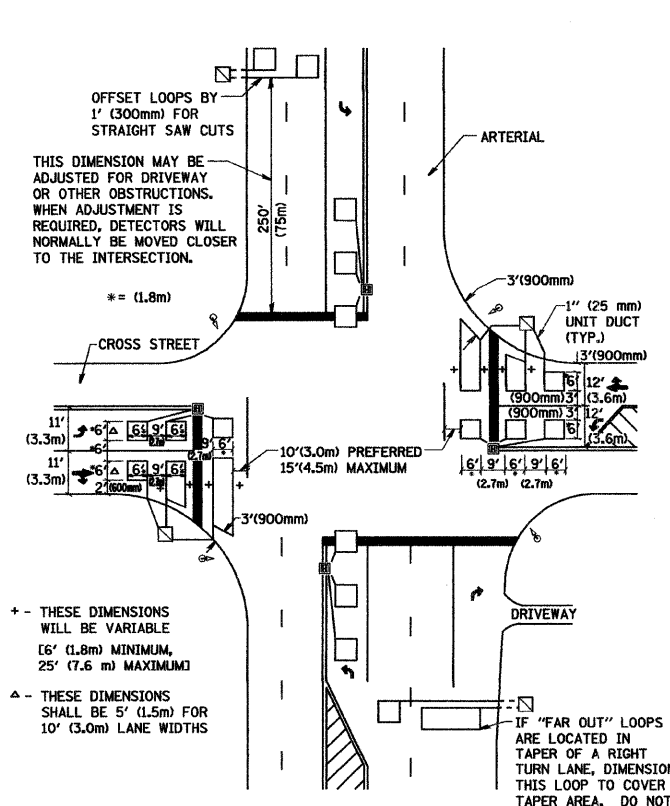
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2 N.T.S.

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

^ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

PLOT DATE = 3/7/2007
FILE NAME = N:\databases\1007.dgn
PLOT SCALE = 1/8"=1'-0"
USER NAME = bauer-d

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**DISTRICT 1
 DETECTOR LOOP
 INSTALLATION DETAILS
 FOR ROADWAY RESURFACING**
 DESIGNED BY
 DRAWN BY CADD
 CHECKED BY R.K.F.
 TS07