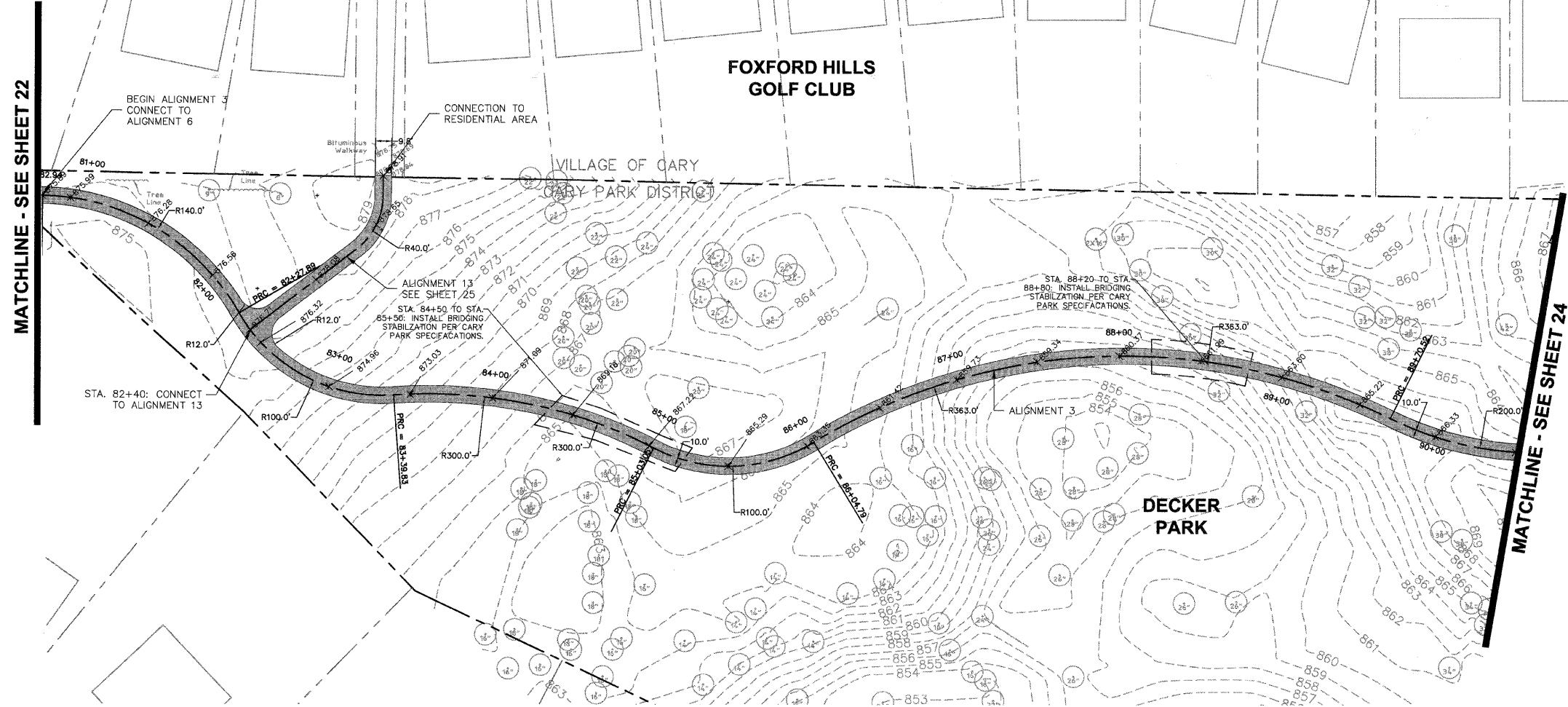
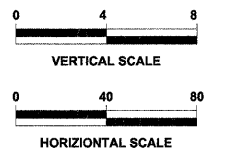
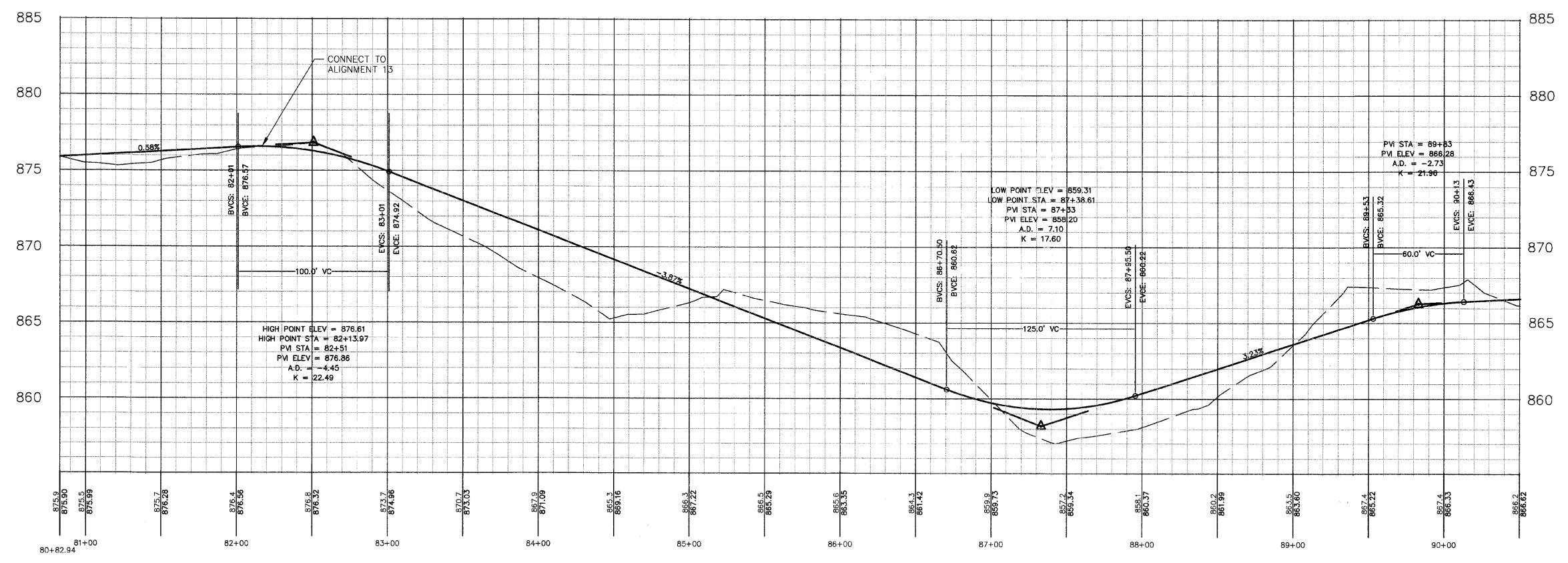
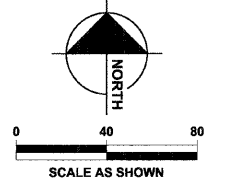


- PLAN LEGEND:**
- PROPOSED SPOT ELEVATIONS
 - PROPOSED BIKE TRAIL
 - PROPOSED AGGREGATE SHOULDER
 - PROPERTY LINE



CARY COMMUNITY TRAIL ALIGNMENT 3 - STA. 80+82.94 TO 90+50



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 9, 2008 ("THE TOPOG") WAS BASED ON MH 48 ON PAGE 25 OF 36 ON THE MANHOLE CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPOG TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 48 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPOG". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK WITH MH 48 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARMA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 48 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 48 USING THIS VERTICAL CONVERSION WOULD BE 861.32 (0.07' HIGHER THAN PUBLISHED MH 48 ELEVATION). THEREFORE DUE TO THE ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPOG" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 48 THE DATUM CONVERSION FOR "THE TOPOG" IS AS FOLLOWS:

NAVD 83 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
NAVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK
DESCRIPTION: MANHOLE #48 FROM THE ASBUILT RECORD DRAWINGS BY MANHOLE CONSULTING LTD. DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY.
LOCATION: 120' NORTHEAST OF THE OGDEN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OGDEN DRIVE.
ELEVATION: 861.45

SITE BENCHMARKS
HE CP #100
DESCRIPTION: IRON PIPE.
LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE CANS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE CANS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
ELEVATION: 854.94

HE CP #1
DESCRIPTION: "P" NAIL IN EDGE OF PAVEMENT.
LOCATION: 592' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MIKE TRAIL, 45' SOUTHWEST OF TRISL 22, 22' SOUTHWEST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
ELEVATION: 865.97

HE CP #8
DESCRIPTION: RAIL ROAD SPIKE.
LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
ELEVATION: 823.23



This document, together with the concrete and design presented herein, is intended only for the specific purpose and district for which it was prepared. Please do not use it for any other purpose without the written consent of the engineer of record. The engineer of record is not responsible for any errors or omissions in this document.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

TRAIL ALIGNMENT
PLAN & PROFILE
STA. 80+82.94 TO STA. 90+50

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	23
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	