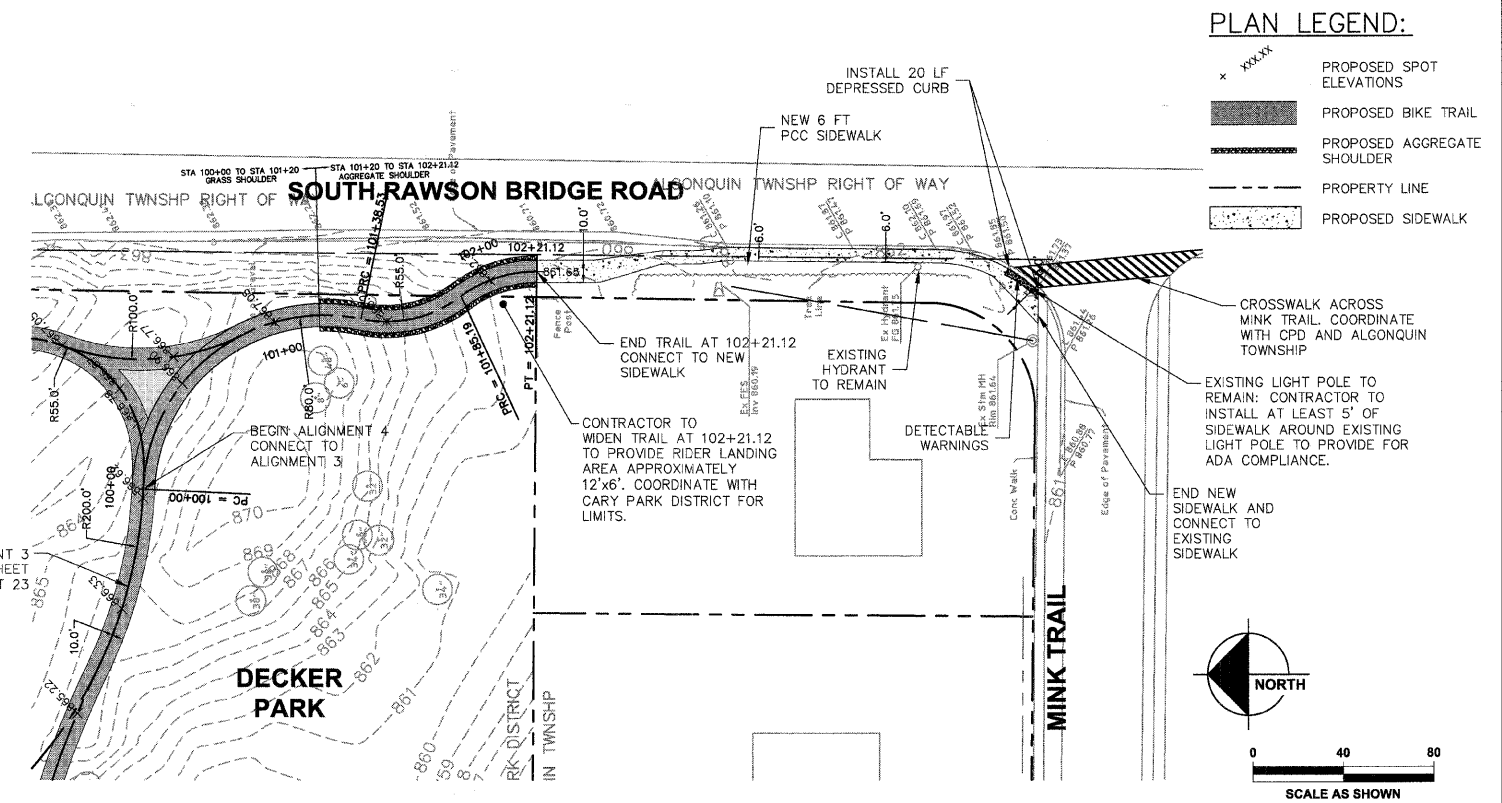
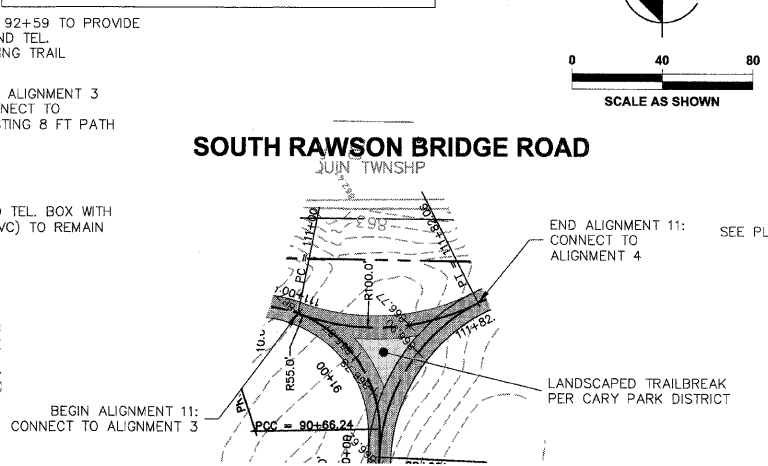
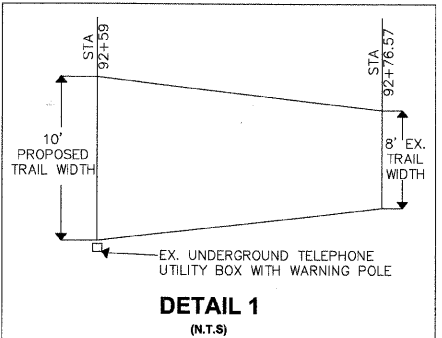
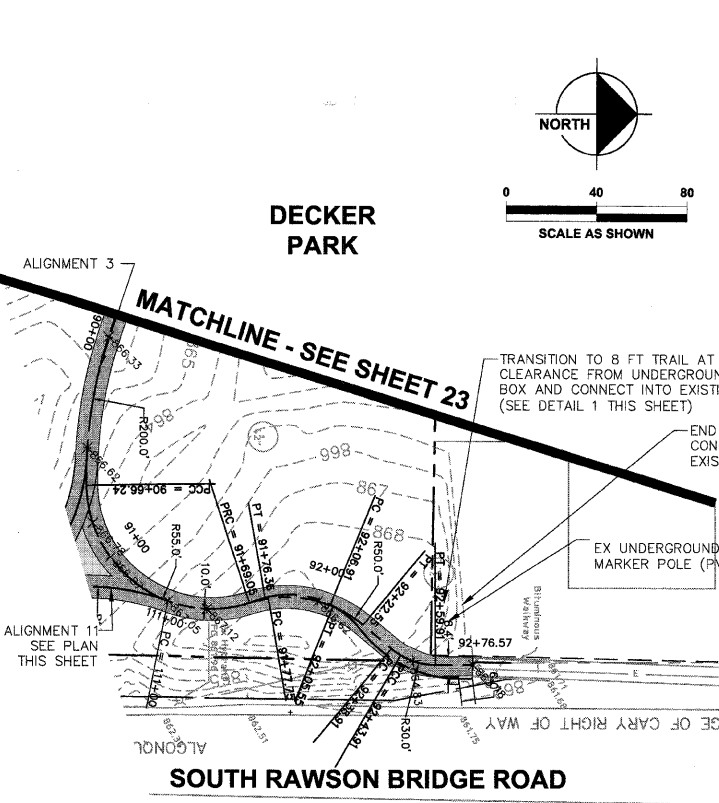


PLAN LEGEND:

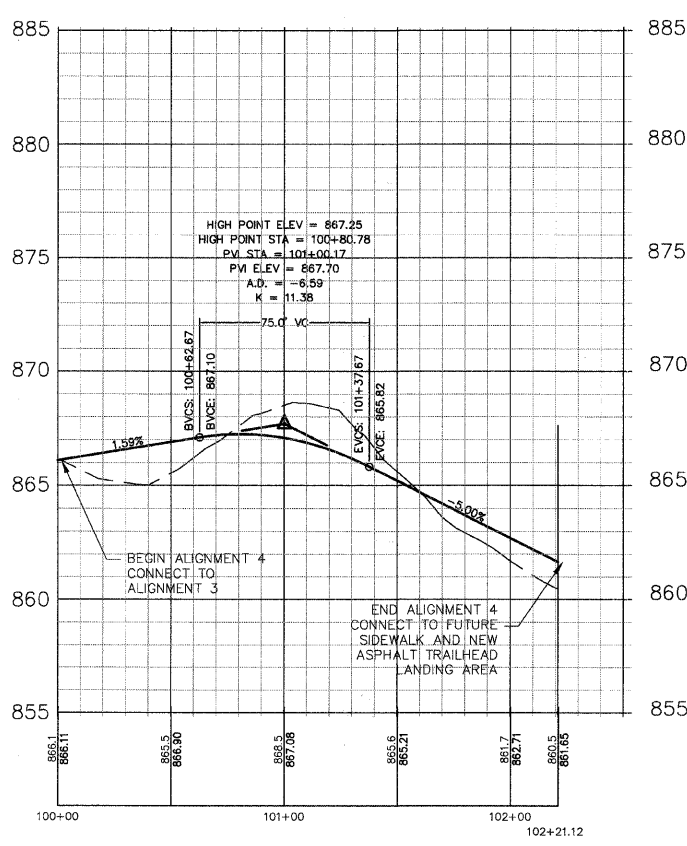
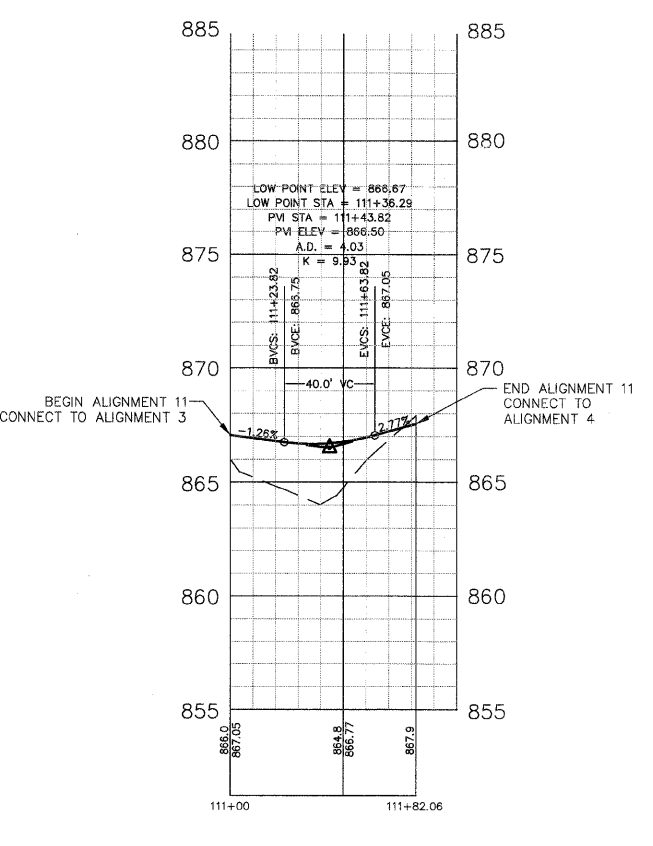
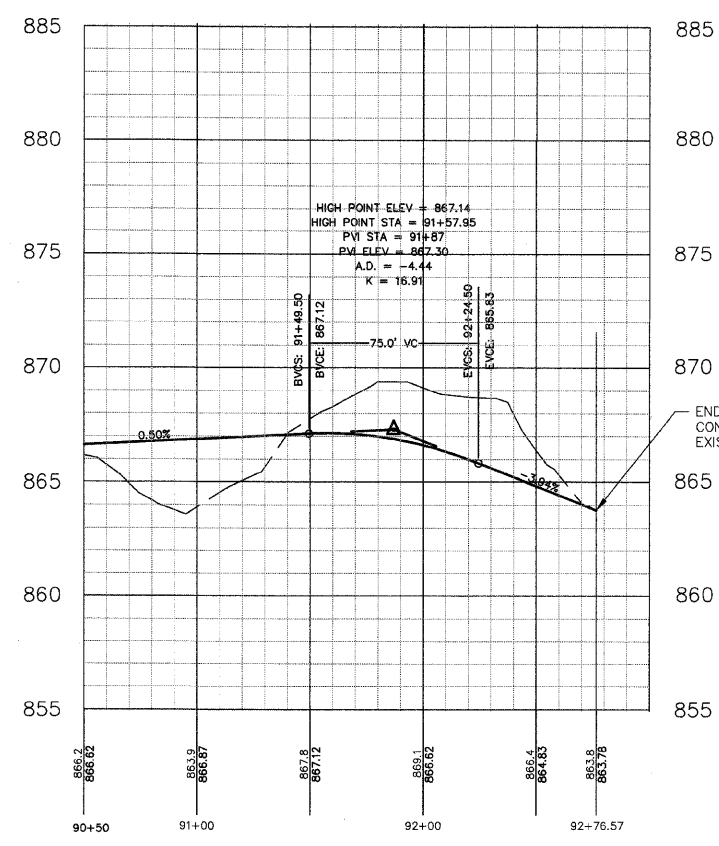
- PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE
- PROPOSED SIDEWALK



CARY COMMUNITY TRAIL ALIGNMENT 3 CONTINUED
STA. 90+50 TO 92+76.57

CARY COMMUNITY TRAIL ALIGNMENT 11
STA. 111+00 TO 111+82.06

CARY COMMUNITY TRAIL ALIGNMENT 4
STA. 100+00 TO 102+21.12



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

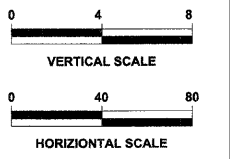
THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAZEGR ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 28 OF 38 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAZEGR ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAZEGR ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION HOWEVER, HAZEGR ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KAMA COMPANY'S RTK GPS REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAZEGR ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 46 USING NGS VERTCON CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.03') IT IS UNKNOWN TO HAZEGR ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAZEGR ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

BENCHMARKS

SOURCE BENCHMARK
DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
LOCATION: 20' NORTHEAST OF THE GANMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON GANMAN DRIVE.
ELEVATION: 861.45

SITE BENCHMARKS
HE CP #10
DESCRIPTION: IRON PIPE.
LOCATION: 1000' EAST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 23' SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALL.
ELEVATION: 866.17

HE CP #8
DESCRIPTION: RAIL ROAD SPIKE.
LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF POWER POLE.
ELEVATION: 823.23



DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

TRAIL ALIGNMENT
PLAN & PROFILE
STA. 90+50 TO STA. 92+76.57
STA. 111+00 TO STA. 111+82.06
STA. 100+00 TO STA. 102+21.12

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	24
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



This document, together with the conceptual and preliminary plans, is for informational purposes only. It is not intended to be a contract. The contractor shall be responsible for all field measurements and for any errors or omissions. The contractor shall be responsible for all field measurements and for any errors or omissions. The contractor shall be responsible for all field measurements and for any errors or omissions.