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ASSOCIATE IDOT FIELD ENGINEER KEVIN STALLWORTH, P.E., 647-705-4141

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	1
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT NO. 63333

INDEX OF SHEETS

DESCRIPTION	SHEET NO.
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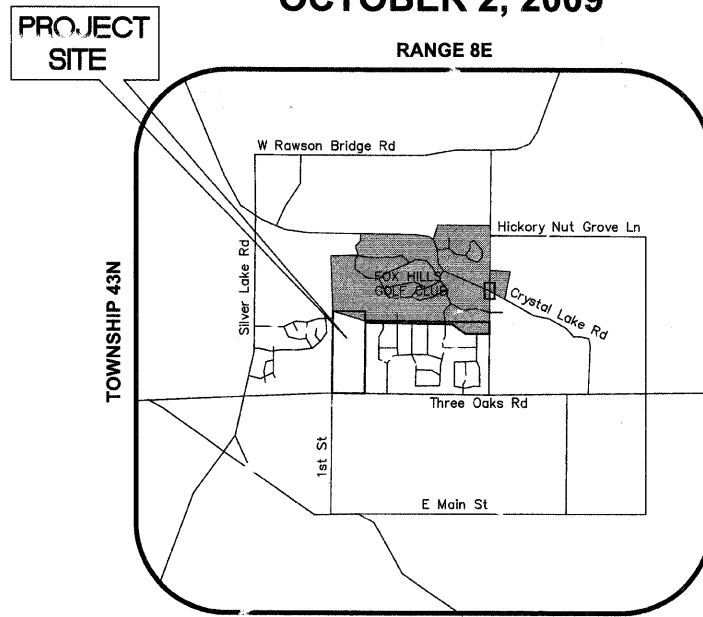
CARY COMMUNITY TRAIL
THREE OAKS ROAD AND SOUTH RAWSON BRIDGE ROAD
CARY PARK DISTRICT
McHENRY COUNTY

SECTION NO. 06-P4001-00-BR
PROJECT NO. ARA-00D1(710)
JOB NUMBER C-91-046-07
COMMUNITY TRAIL
OCTOBER 2, 2009



LEGEND

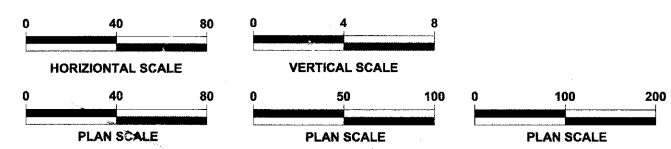
EXISTING	PROPOSED
PROPERTY LINE	PROPOSED EASEMENT
EASEMENT LINE	EXISTING GROUND PROFILE
EXISTING CONTOUR	PROPOSED TRAIL PROFILE
EXISTING SPOT ELEVATION	POINT OF VERTICAL INTERSECTION
EXISTING FIRE HYDRANT	BEGINNING OF VERTICAL CURVE STATION/ELEVATION
EXISTING MANHOLE	END OF VERTICAL CURVE STATION/ELEVATION
EXISTING CATCH BASIN	PROPOSED BIKE TRAIL
EXISTING GAS LINE	PROPOSED SIDEWALK
EXISTING POWER LINE	PROPOSED 2' AGGREGATE SHOULDER
EXISTING UTILITY POLE	GROUND SLOPE
EXISTING TREE	SILT FENCE
EXISTING TREE LINE	PROPOSED SIGN
EXISTING FENCE	PROPOSED CONTOUR
EXISTING LIGHT POLE	PROPOSED SPOT ELEVATION
EXISTING SIGN	PROPOSED PIPE OUTLET
	PROPOSED PERFORATED UNDERDRAIN
	PROPOSED STORM PIPE
	PROPOSED STORM CLEANOUT
	PROPOSED INLET PROTECTION
	PROPOSED RIP RAP



ALGONQUIN TOWNSHIP **3RD PM**
LOCATION MAP
 NOT TO SCALE
GROSS LENGTH OF PROJECT = 9,557 LINEAL FEET (1.81 MILES)
NET LENGTH OF PROJECT = 9,557 LINEAL FEET (1.81 MILES)

PEDESTRIAN TRAFFIC DATA

POSTED: 20 MPH
DESIGN: 20 MPH
ADT: 100 PEDESTRIAN USERS PER DAY
FUNCTIONAL CLASSIFICATION: SHARED USE PATH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES.

Kimley-Horn and Associates, Inc.
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Tel. No. (312) 726 9445 Fax No. (312) 726 9449



Date of Signature: 10-02-09
 Date of Expiration: 11-30-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: *J. Raica* 10/02/09
DAVID J. RAICA
 DIRECTOR OF PLANNING AND DEVELOPMENT, CARY PARK DISTRICT

PASSED: *October 27* 20 09
Christopher Holt
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW: *October 29,* 20 09
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

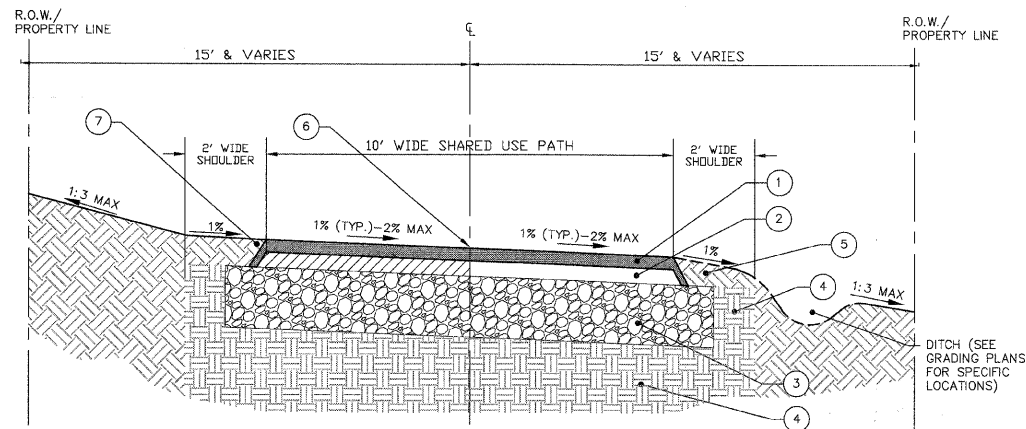
DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC, DATED FEB. 2, 2008 (THE TOPO) WAS BASED ON MH 46 ON PAGE 29 OF 38 ON THE MANHART CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON NAD83 COMPANY'S RTK CORES REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE COMPARISON OF THE TOPO TO NAVD 29 ELEVATION FOR MH 46 USING NAD 83 VERTICAL CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR THE TOPO, WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTRIE INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

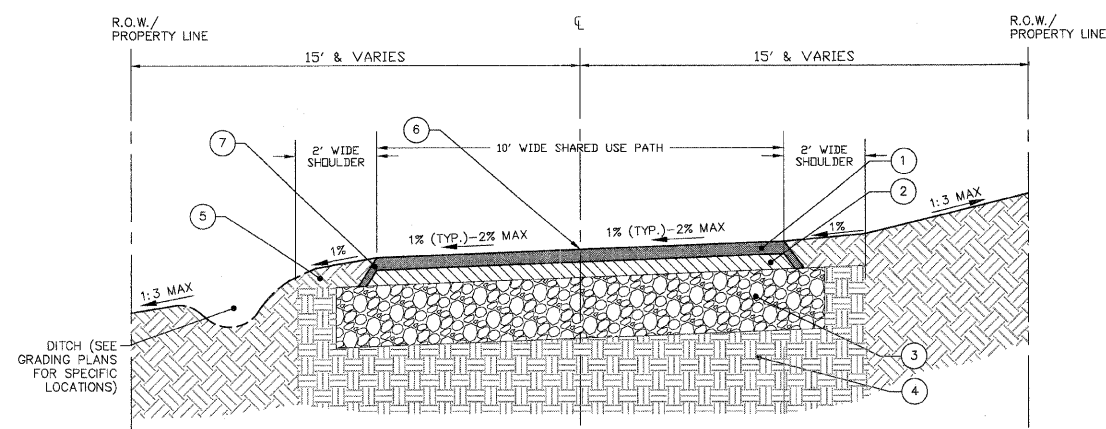
BASED ON HAEGER ENGINEERING, LLC. GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR THE TOPO IS AS FOLLOWS:
 NAVD 83 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

J.U.L.I.E
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
8-1-1 OR 1-800-892-0123

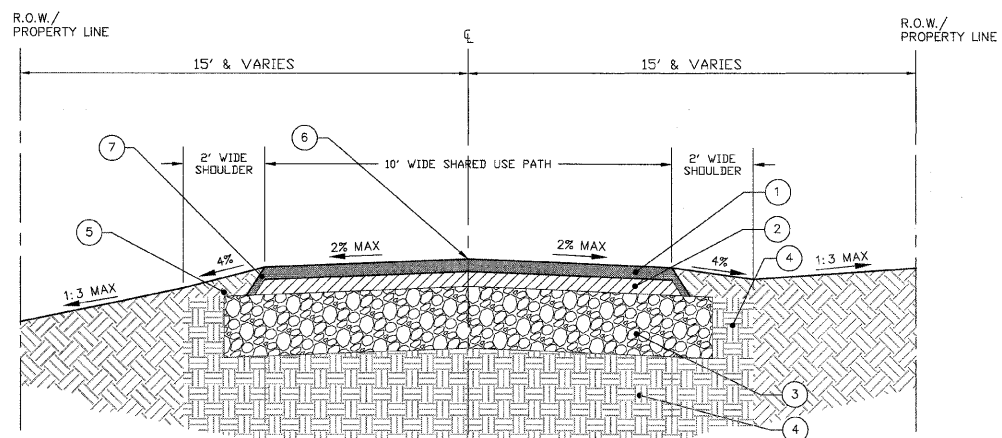
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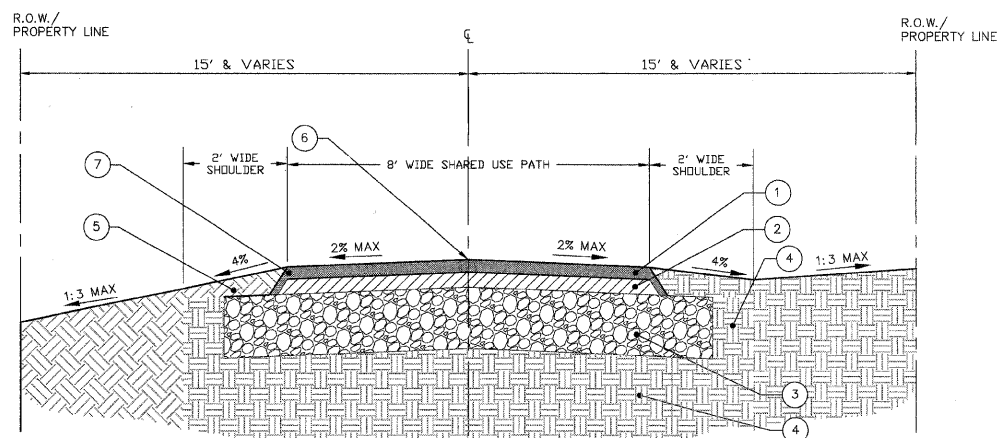
① TYPICAL 10' SUPERELEVATED CROSS SECTION (SLOPE RIGHT)
NOT TO SCALE
STA. 10+00 TO 44+50, 50+00 TO 52+00, 78+00 TO 90+50,



② TYPICAL 10' SUPERELEVATED CROSS SECTION (SLOPE LEFT)
NOT TO SCALE
STA. 45+00 TO 48+89.55, 200+00 TO 202+24.64, 52+50 TO 73+80, 91+00 TO 94+76.57, 100+50 TO 102+21.12



③ TYPICAL 10' CROWNED PATH CROSS SECTION
NOT TO SCALE
STA. 74+30 TO 76+00, 76+40 TO 77+50



④ TYPICAL 8' CROWNED PATH CROSS SECTION
NOT TO SCALE
STA. 500+00 TO 503+69.25

TRAIL CROSS SLOPE TRANSITION SEGMENTS

44+50 TO 45+00, 52+00 TO 52+50, 73+80 TO 74+30,
77+50 TO 78+00, 90+50 TO 91+00, 100+00 TO 100+50,
111+00 TO 111+82.06

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
ITEM #	AIR VOIDS
SHARED USE TRAIL	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25"	4% @ 50 Gyr.
PATCHING	
HOT-MIX ASPHALT PATCH, CLASS D, TYPE III, 12"	4% @ 50 Gyr.
NOTES:	
• THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES 112 LBS/SY/IN PER IDOT.	
• FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.	

LEGEND

- ① PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2" THICKNESS
- ② PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25" THICKNESS
- ③ PROPOSED AGGREGATE BASE, TYPE B - 8" THICKNESS (12 FT WIDTH)
- ④ COMPACTED SUBGRADE
- ⑤ PROPOSED SHOULDER: AGGREGATE SHOULDER, TYPE B OR OR SEEDING CLASS 1, OR SEEDING CLASS 1B, OR SEEDING CLASS 4A (NOTE: SEE SHEETS 15-25 FOR SHOULDERS WITH AGGREGATE SHOULDER, TYPE B AND SEE SHEETS 54-58 FOR SHOULDERS WITH SEEDING)
- ⑥ PROPOSED THERMOPLASTIC PAVEMENT MARKING - LINE 4" (NOTE: SEE PLANS FOR SPECIFIC LOCATIONS)
- ⑦ HAND TAMPED TAPER - SEE SHEET 63 FOR CARY PARK DISTRICT DETAIL 13-01

NOTES

- 1. FINAL PAVING CROSS SECTION, MATERIALS AND METHODS WILL BE PER GEOTECHNICAL REPORT BY PSI DATED FEBRUARY 22, 2008, SECTION 406 OF THE IDOT STANDARD SPECIFICATIONS AND CARY PARK DISTRICT SPECIFICATIONS.
- 2. SEE GRADING PLANS ON PAGES 54-58 AND CROSS SECTIONS ON PAGES 26-48 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
- 3. FINAL AGGREGATE BASE WIDTH TO BE INSTALLED 12 FEET WIDE PER CARY PARK DISTRICT.

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DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

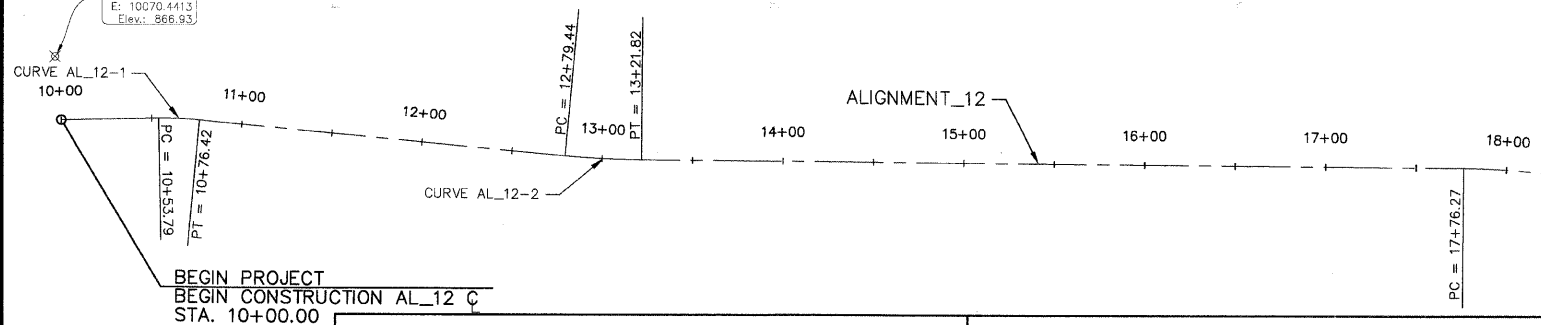
TYPICAL SECTIONS

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	3
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



1-800-892-0123

CP# 15050
Type: CP
N: 25828.4034
E: 10070.4413
Elev.: 666.93



HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.
ALIGNMENT: ALIGNMENT_12
DESC: CARY-GROVE PARK TRAIL

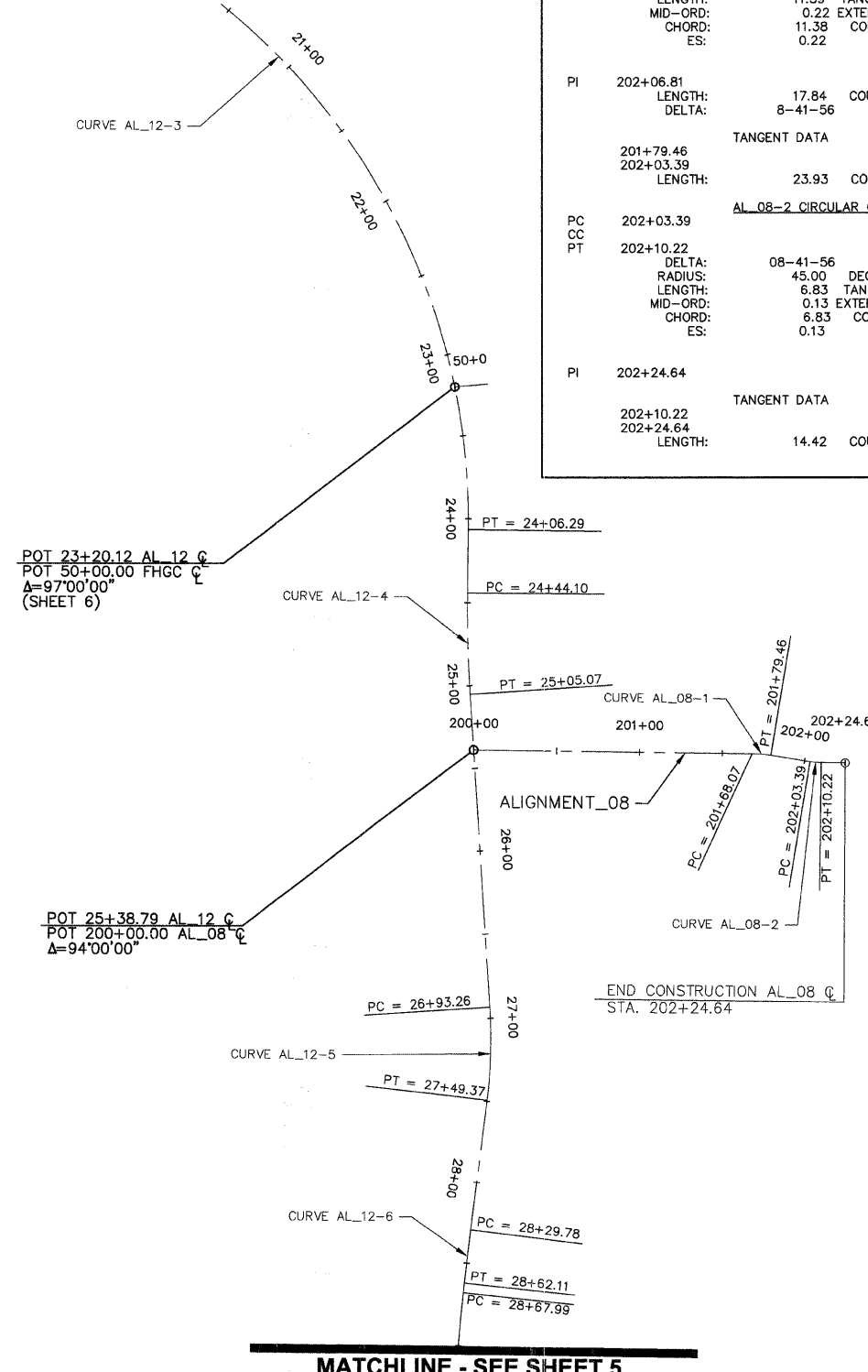
DESC.	STATION	SPIRAL/CURVE DATA	NORTHING	EASTING
PI	10+00	LENGTH: 65.12 COURSE: 25793.7698 N 88-50-31 E	10073.8950	
PI	10+65.12	LENGTH: 235.55 COURSE: 25795.0859 S 84-40-37 E	10139.0002	
TANGENT DATA				
	10+00	25793.7698	10073.8950	
	10+65.12	25794.8571	10127.6787	
AL_12-1 CIRCULAR CURVE DATA				
PC	10+53.79	25794.8571	10127.6787	
CC	10+76.42	25594.8979	10131.7210	
PT	10+76.42	25794.0354	10150.2753	
	DELTA:	06-28-52	TYPE: RIGHT	
	RADIUS:	200.00	DEGREE: 28-38-52	
	LENGTH:	22.82	TANGENT: 11.32	
	MID-ORD:	0.32	EXTERNAL: 0.32	
	CHORD:	22.61	COURSE: S 87-55-03 E	
	ES:	0.32		
PI	13+00.64	LENGTH: 877.36 COURSE: 25773.2340 S 89-31-58 E	10373.5304	
	DELTA:	4-51-22		
TANGENT DATA				
	10+76.42	25794.0354	10150.2753	
	12+79.44	25775.2008	10352.4209	
AL_12-2 CIRCULAR CURVE DATA				
PC	12+79.44	25775.2008	10352.4209	
CC	13+21.82	26273.0445	10398.8067	
PT	13+21.82	25773.0611	10394.7306	
	DELTA:	04-51-22	TYPE: LEFT	
	RADIUS:	500.00	DEGREE: 11-27-33	
	LENGTH:	42.38	TANGENT: 21.20	
	MID-ORD:	0.45	EXTERNAL: 0.45	
	CHORD:	42.36	COURSE: S 87-06-18 E	
	ES:	0.45		
PI	21+77.98	LENGTH: 470.02 COURSE: 25766.0817 S 00-42-41 W	11250.8628	
	DELTA:	90-14-40		
TANGENT DATA				
	13+21.82	25773.0611	10394.7306	
	17+76.27	25769.3564	10849.1661	
AL_12-3 CIRCULAR CURVE DATA				
PC	17+76.27	25769.3564	10849.1661	
CC	24+06.29	25369.3697	10845.9053	
PT	24+06.29	25364.4026	11245.8744	
	DELTA:	90-14-40	TYPE: RIGHT	
	RADIUS:	400.00	DEGREE: 14-19-26	
	LENGTH:	630.02	TANGENT: 401.71	
	MID-ORD:	117.76	EXTERNAL: 166.90	
	CHORD:	566.89	COURSE: S 44-24-39 E	
	ES:	166.90		
PI	24+74.60	LENGTH: 246.83 COURSE: 25296.1008 S 03-39-17 E	11245.0262	
	DELTA:	4-21-58		
TANGENT DATA				
	24+06.29	25364.4026	11245.8744	
	24+44.10	25326.5950	11245.4049	
AL_12-4 CIRCULAR CURVE DATA				
PC	24+44.10	25326.5950	11245.4049	
CC	25+05.07	25318.6608	12045.3432	
PT	25+05.07	25265.6663	11246.9701	
	DELTA:	04-21-58	TYPE: LEFT	
	RADIUS:	800.00	DEGREE: 07-09-43	
	LENGTH:	60.96	TANGENT: 30.50	
	MID-ORD:	0.58	EXTERNAL: 0.58	
	CHORD:	60.95	COURSE: S 01-28-18 E	
	ES:	0.58		

PI	27+21.40	LENGTH: 124.71 COURSE: 25049.7760 S 07-03-44 W	11260.7597	
	DELTA:	10-43-01		
TANGENT DATA				
	25+05.07	25265.6663	11246.9701	
	26+93.26	25077.8577	11258.9661	
AL_12-5 CIRCULAR CURVE DATA				
PC	26+93.26	25077.8577	11258.9661	
CC	27+49.37	25058.7348	10959.5762	
PT	27+49.37	25021.8505	11257.3001	
	DELTA:	10-43-01	TYPE: RIGHT	
	RADIUS:	300.00	DEGREE: 19-05-55	
	LENGTH:	56.11	TANGENT: 28.14	
	MID-ORD:	1.31	EXTERNAL: 1.32	
	CHORD:	56.03	COURSE: S 01-42-14 W	
	ES:	1.32		
PI	28+45.95	LENGTH: 73.11 COURSE: 24926.0081 S 06-08-09 W	11245.4264	
	DELTA:	0-55-35		
TANGENT DATA				
	27+49.37	25021.8505	11257.3001	
	28+29.78	24942.0538	11247.4143	
AL_12-6 CIRCULAR CURVE DATA				
PC	28+29.78	24942.0538	11247.4143	
CC	28+62.11	24696.1583	13232.2406	
PT	28+62.11	24909.9324	11243.6982	
	DELTA:	00-55-35	TYPE: LEFT	
	RADIUS:	2000.00	DEGREE: 02-51-53	
	LENGTH:	32.34	TANGENT: 16.17	
	MID-ORD:	0.07	EXTERNAL: 0.07	
	CHORD:	32.34	COURSE: S 06-35-57 W	
	ES:	0.07		
PI	29+19.05	LENGTH: 134.77 COURSE: 24853.3183 S 01-15-46 W	11237.6121	
	DELTA:	4-52-24		
TANGENT DATA				
	28+62.11	24909.9324	11243.6982	
	28+67.99	24904.0889	11243.0700	

CARY-GROVE PARK

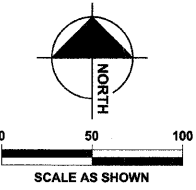
HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.
ALIGNMENT: ALIGNMENT_08
DESC: MINK TRAIL CONNECTION

DESC.	STATION	SPIRAL/CURVE DATA	NORTHING	EASTING
PI	200+00	LENGTH: 173.77 COURSE: 25232.0152 S 89-17-19 E	11249.1195	
PI	201+73.77	LENGTH: 33.06 COURSE: 25229.8573 S 80-35-23 E	11422.8795	
	DELTA:	8-41-56		
TANGENT DATA				
	200+00	25232.0152	11249.1195	
	201+68.07	25229.9282	11417.1755	
AL_08-1 CIRCULAR CURVE DATA				
PC	201+68.07	25229.9282	11417.1755	
CC	201+79.46	25154.9339	11416.2442	
PT	201+79.46	25228.9246	11428.5071	
	DELTA:	08-41-56	TYPE: RIGHT	
	RADIUS:	75.00	DEGREE: 76-23-40	
	LENGTH:	11.39	TANGENT: 5.70	
	MID-ORD:	0.22	EXTERNAL: 0.22	
	CHORD:	11.38	COURSE: S 84-56-21 E	
	ES:	0.22		
PI	202+06.81	LENGTH: 17.84 COURSE: 25224.4519 S 89-17-19 E	11455.4942	
	DELTA:	8-41-56		
TANGENT DATA				
	201+79.46	25228.9246	11428.5071	
	202+03.39	25225.0115	11452.1176	
AL_08-2 CIRCULAR CURVE DATA				
PC	202+03.39	25225.0115	11452.1176	
CC	202+10.22	25269.4059	11459.4754	
PT	202+10.22	25224.4094	11458.9166	
	DELTA:	08-41-56	TYPE: LEFT	
	RADIUS:	45.00	DEGREE: 127-19-26	
	LENGTH:	6.83	TANGENT: 3.42	
	MID-ORD:	0.13	EXTERNAL: 0.13	
	CHORD:	6.83	COURSE: S 84-56-21 E	
	ES:	0.13		
PI	202+24.64	LENGTH: 14.42 COURSE: 25224.2304 S 89-17-19 E	11473.3316	
TANGENT DATA				
	202+10.22	25224.4094	11458.9166	
	202+24.64	25224.2304	11473.3316	



ALIGNMENT DATA LEGEND

- TRAIL CENTER LINE
- CC COMPOUND CURVE
- PC POINT OF CURVATURE
- PI POINT OF INTERSECTION
- PT POINT OF TANGENCY
- POT POINT OF TERMINATION



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DESIGNED	JWH	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REVISED
DATE	10/02/2009	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

ALIGNMENT DATA
STA. 10+00 TO STA. 29+00
STA. 200+00 TO STA. 202+24.64

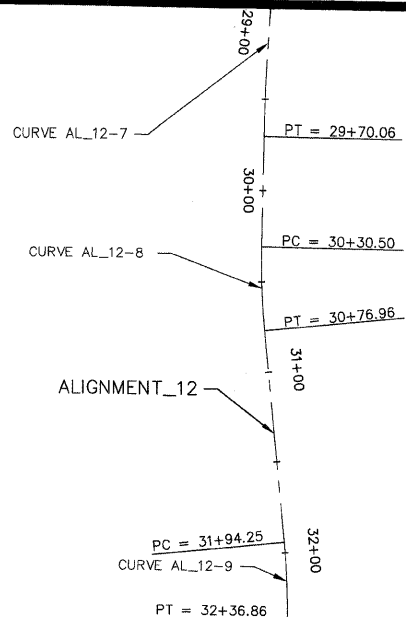
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	4
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

ALIGNMENT DATA LEGEND

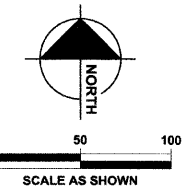
- TRAIL CENTER LINE
- CC COMPOUND CURVE
- PC POINT OF CURVATURE
- PI POINT OF INTERSECTION
- PT POINT OF TANGENCY
- POT POINT OF TERMINATION

CARY-GROVE PARK

MATCHLINE - SEE SHEET 4



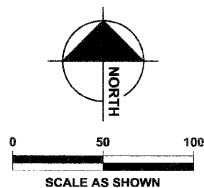
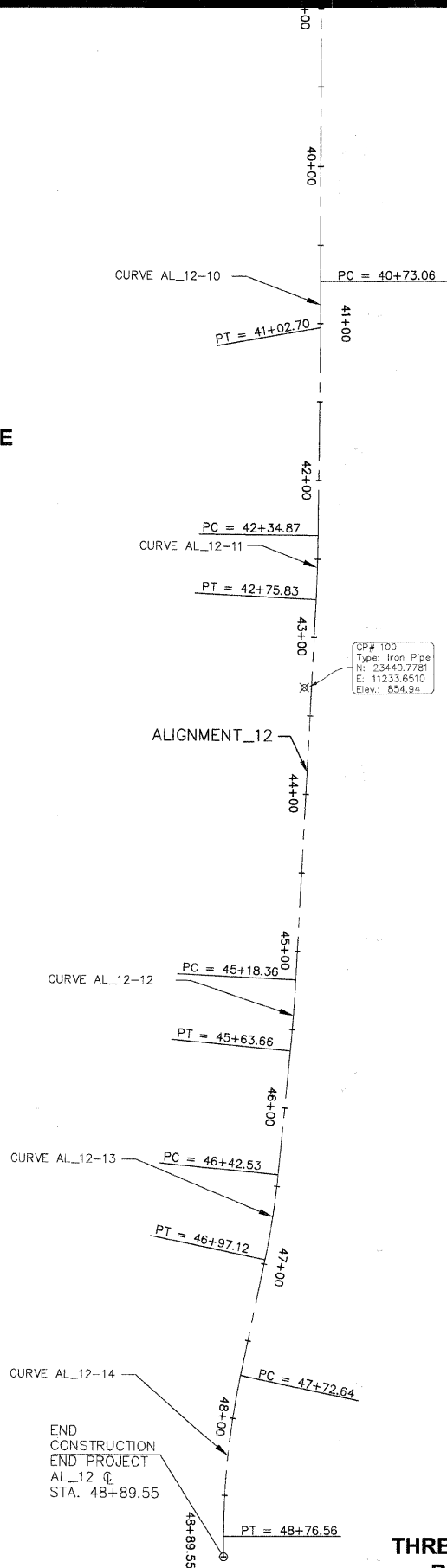
AL_12-7 CIRCULAR CURVE DATA			
PC	28+67.99	24904.0889	11243.0700
CC		24775.8244	12436.1954
PT	29+70.06	24802.2676	11236.4868
DELTA:	04-52-24	TYPE:	LEFT
RADIUS:	1200.00	DEGREE:	04-46-29
LENGTH:	102.06	TANGENT:	51.06
MID-ORD:	1.08	EXTERNAL:	1.09
CHORD:	102.03	COURSE:	S 03-41-57 W
ES:	1.09		
PI	30+53.76	24718.5845	11234.6423
LENGTH:	161.87	COURSE:	S 05-23-32 E
DELTA:	6-39-18		
TANGENT DATA			
	29+70.06	24802.2676	11236.4868
	30+30.50	24741.8350	11235.1548
LENGTH:	60.45	COURSE:	S 01-15-46 W
AL_12-8 CIRCULAR CURVE DATA			
PC	30+30.50	24741.8350	11235.1548
CC		24733.0206	11635.0577
PT	30+76.96	24695.4314	11236.8278
DELTA:	06-39-18	TYPE:	LEFT
RADIUS:	400.00	DEGREE:	14-19-26
LENGTH:	46.46	TANGENT:	23.26
MID-ORD:	0.67	EXTERNAL:	0.68
CHORD:	46.43	COURSE:	S 02-03-53 E
ES:	0.68		
PI	32+15.58	24557.4291	11249.8539
LENGTH:	362.15	COURSE:	S 00-42-41 W
DELTA:	6-06-13		
TANGENT DATA			
	29+70.06	24802.2676	11236.4868
	30+30.50	24741.8350	11235.1548
LENGTH:	117.29	COURSE:	S 05-23-32 E
AL_12-9 CIRCULAR CURVE DATA			
PC	31+94.25	24578.6609	11247.8498
CC		24541.0717	10849.6199
PT	32+36.86	24536.1046	11249.5891
DELTA:	06-06-13	TYPE:	RIGHT
RADIUS:	400.00	DEGREE:	14-19-26
LENGTH:	42.61	TANGENT:	21.33
MID-ORD:	0.57	EXTERNAL:	0.57
CHORD:	42.59	COURSE:	S 02-20-25 E
ES:	0.57		
PI	35+77.69	24195.3069	11245.3568
LENGTH:	510.19	COURSE:	S 00-11-02 W
DELTA:	0-31-40		
TANGENT DATA			
	32+36.86	24536.1046	11249.5891
	35+77.69	24195.3069	11245.3568
LENGTH:	340.82	COURSE:	S 00-42-41 W



MATCHLINE - SEE THIS SHEET

MATCHLINE - SEE THIS SHEET

CARY-GROVE PARK



THREE OAKS ROAD

PI	40+87.88	14.82	COURSE:	23685.1186	11243.7204
LENGTH:		0-38-09		S 00-27-07 E	
DELTA:					
TANGENT DATA					
	35+77.69	24195.3069		11245.3568	
	40+73.06	23699.9361		11243.7680	
LENGTH:	495.37	COURSE:	S 00-11-02 W		
AL_12-10 CIRCULAR CURVE DATA					
PC	40+73.06	23699.9361		11243.7680	
CC		23691.3701		13914.4268	
PT	41+02.70	23670.3015		11243.8373	
DELTA:	00-38-09	TYPE:	LEFT		
RADIUS:	2670.67	DEGREE:	02-08-43		
LENGTH:	29.63	TANGENT:	14.82		
MID-ORD:	0.04	EXTERNAL:	0.04		
CHORD:	29.63	COURSE:	S 00-08-03 E		
ES:	0.04				
PI	41+02.70	23670.3015		11243.8373	
LENGTH:	152.66	COURSE:	S 00-42-41 W		
DELTA:	1-09-49				
TANGENT DATA					
	41+02.70	23670.3015		11243.8373	
	42+34.87	23538.1419		11242.1961	
LENGTH:	132.17	COURSE:	S 00-42-41 W		
AL_12-11 CIRCULAR CURVE DATA					
PC	42+34.87	23538.1419		11242.1961	
CC		23550.5597		10242.2732	
PT	42+75.83	23497.2000		11240.8485	
DELTA:	02-20-50	TYPE:	RIGHT		
RADIUS:	1000.00	DEGREE:	05-43-46		
LENGTH:	40.97	TANGENT:	20.49		
MID-ORD:	0.21	EXTERNAL:	0.21		
CHORD:	40.96	COURSE:	S 01-53-06 W		
ES:	0.21				
PI	45+41.01	23232.3960		11226.6985	
LENGTH:	128.84	COURSE:	S 05-39-15 W		
DELTA:	2-35-43				
TANGENT DATA					
	42+75.83	23497.2000		11240.8485	
	45+18.36	23255.0161		11227.9073	
LENGTH:	242.53	COURSE:	S 03-03-31 W		
AL_12-12 CIRCULAR CURVE DATA					
PC	45+18.36	23255.0161		11227.9073	
CC		23308.3758		10229.3319	
PT	45+63.66	23209.8538		11224.4668	
DELTA:	02-35-43	TYPE:	RIGHT		
RADIUS:	1000.00	DEGREE:	05-43-46		
LENGTH:	45.30	TANGENT:	22.65		
MID-ORD:	0.26	EXTERNAL:	0.26		
CHORD:	45.29	COURSE:	S 04-21-23 W		
ES:	0.26				
PI	46+69.85	23104.1794		11214.0046	
LENGTH:	154.99	COURSE:	S 11-54-33 W		
DELTA:	6-15-18				
TANGENT DATA					
	45+63.66	23209.8538		11224.4668	
	46+42.53	23131.3661		11216.6962	
LENGTH:	78.87	COURSE:	S 05-39-15 W		
AL_12-13 CIRCULAR CURVE DATA					
PC	46+42.53	23131.3661		11216.6962	
CC		23180.6271		10719.1288	
PT	46+97.12	23077.4478		11208.3670	
DELTA:	06-15-18	TYPE:	RIGHT		
RADIUS:	500.00	DEGREE:	11-27-33		
LENGTH:	54.59	TANGENT:	27.32		
MID-ORD:	0.74	EXTERNAL:	0.75		
CHORD:	54.56	COURSE:	S 08-46-54 W		
ES:	0.75				
PI	48+24.79	22952.5217		11182.0203	
LENGTH:	65.14	COURSE:	S 00-00-00 E		
DELTA:	11-54-33				
TANGENT DATA					
	46+97.12	23077.4478		11208.3670	
	47+72.64	23003.5502		11192.7821	
LENGTH:	75.52	COURSE:	S 11-54-33 W		
AL_12-14 CIRCULAR CURVE DATA					
PC	47+72.64	23003.5502		11192.7821	
CC		22900.3708		11682.0203	
PT	48+76.56	22900.3708		11182.0203	
DELTA:	11-54-33	TYPE:	LEFT		
RADIUS:	500.00	DEGREE:	11-27-33		
LENGTH:	103.93	TANGENT:	52.15		
MID-ORD:	2.70	EXTERNAL:	2.71		
CHORD:	103.74	COURSE:	S 05-57-16 W		
ES:	2.71				
PI	48+89.55	22887.3847		11182.0203	
TANGENT DATA					
	48+76.56	22900.3708		11182.0203	
	48+89.55	22887.3847		11182.0203	
LENGTH:	12.99	COURSE:	S 00-00-00 W		



1-800-892-0123

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

ALIGNMENT DATA
STA. 29+00 TO STA. 39+00
STA. 39+00 TO 48+89.55

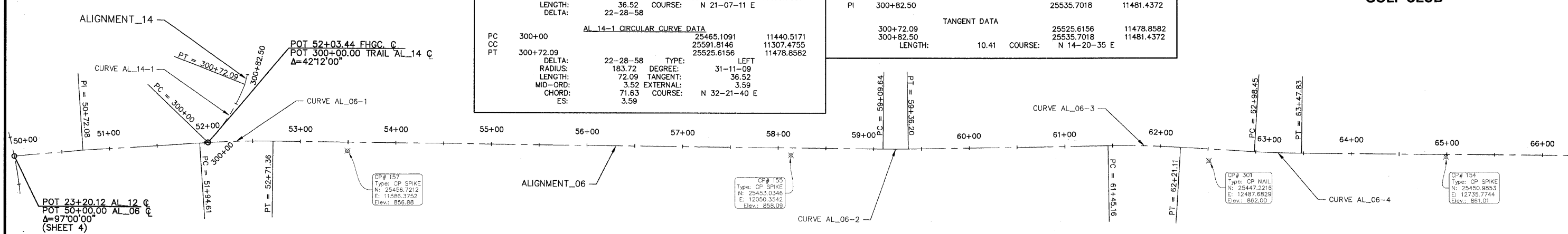
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	5
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

FOXFORD HILLS GOLF CLUB

HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.
ALIGNMENT: ALIGNMENT_14
DESC: FOXFORD HILLS GOLF CLUB CONNECTION

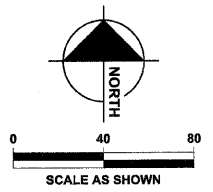
DESC.	STATION	SPIRAL/CURVE DATA	NORTHING	EASTING
PI	300+00	LENGTH: 36.52 COURSE: N 43-36-10 E	25465.1091	11440.5171
PI	300+36.52	LENGTH: 36.52 COURSE: N 21-07-11 E	25491.5521	11465.7007
PC	300+00	AL 14-1 CIRCULAR CURVE DATA		
CC		DELTA: 22-28-58 TYPE: LEFT	25465.1091	11440.5171
PT	300+72.09	RADIUS: 183.72 DEGREE: 31-11-09	25591.8146	11307.4755
		LENGTH: 72.09 TANGENT: 36.52	25525.6156	11478.8582
		MID-ORD: 3.52 EXTERNAL: 3.59		
		CHORD: 71.63 COURSE: N 32-21-40 E		
		ES: 3.59		

PI	300+72.09	LENGTH: 10.41 COURSE: N 14-20-35 E	25525.6156	11478.8582
		DELTA: 6-46-36		
PI	300+82.50		25535.7018	11481.4372
		TANGENT DATA		
	300+72.09		25525.6156	11478.8582
	300+82.50	LENGTH: 10.41 COURSE: N 14-20-35 E	25535.7018	11481.4372



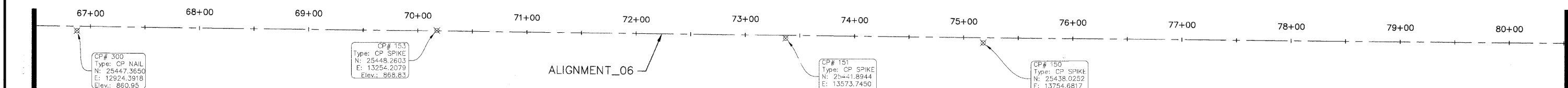
ALIGNMENT DATA LEGEND

- TRAIL CENTER LINE
- CC COMPOUND CURVE
- PC POINT OF CURVATURE
- PI POINT OF INTERSECTION
- PT POINT OF TANGENCY
- POT POINT OF TERMINATION



MATCHLINE - SEE THIS SHEET

FOXFORD HILLS GOLF CLUB



HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.
ALIGNMENT: ALIGNMENT_06
DESC: BERM CORRIDOR

DESC.	STATION	SPIRAL/CURVE DATA	NORTHING	EASTING
PI	50+00	LENGTH: 72.08 COURSE: N 84-53-19 E	25450.0169	11237.6894
PI	50+72.08	LENGTH: 180.93 COURSE: N 86-12-51 E	25456.4385	11309.4818
		DELTA: 1-19-32		
	50+00	TANGENT DATA		
	50+72.08	LENGTH: 72.08 COURSE: N 84-53-19 E	25450.0169	11237.6894
			25456.4385	11309.4818
	50+72.08	TANGENT DATA		
	51+94.61	LENGTH: 122.53 COURSE: N 86-12-51 E	25456.4385	11309.4818
			25464.5289	11431.7488
PI	52+33.01	LENGTH: 689.95 COURSE: S 89-23-18 E	25467.0639	11470.0595
		DELTA: 4-23-51		
	51+52.54	TANGENT DATA		
	51+94.61	LENGTH: 42.07 COURSE: N 86-12-51 E	25461.7512	11389.7706
			25464.5289	11431.7488

AL_06-1 CIRCULAR CURVE DATA

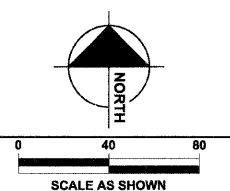
PC	51+94.61	25464.5289	11431.7488
CC		24466.7110	11497.7742
PT	52+71.36	25466.6540	11508.4517
	DELTA: 04-23-51 TYPE: RIGHT		
	RADIUS: 1000.00 DEGREE: 05-43-46		
	LENGTH: 76.75 TANGENT: 38.39		
	MID-ORD: 0.74 EXTERNAL: 0.74		
	CHORD: 76.73 COURSE: N 88-24-47 E		
	ES: 0.74		
PI	59+22.92	25459.6969	12159.9697
	LENGTH: 260.24 COURSE: N 89-05-23 E		
	DELTA: 1-31-20		
	52+71.36	TANGENT DATA	
	59+09.64	LENGTH: 638.27 COURSE: S 89-23-18 E	25466.6540 11508.4517
			25459.8388 12146.6865
PC	59+09.64	25459.8388	12146.6865
CC		26459.7818	12157.3641
PT	59+36.20	25459.9080	12173.2520
	DELTA: 01-31-20 TYPE: LEFT		
	RADIUS: 1000.00 DEGREE: 05-43-46		
	LENGTH: 26.57 TANGENT: 13.28		
	MID-ORD: 0.09 EXTERNAL: 0.09		
	CHORD: 26.57 COURSE: N 89-51-03 E		
	ES: 0.09		
PI	61+83.15	25463.8315	12420.1729
	LENGTH: 140.03 COURSE: S 86-33-31 E		
	DELTA: 4-21-07		

TANGENT DATA

	59+36.20	25459.9080	12173.2520
	61+45.16	25463.2279	12382.1827
	LENGTH: 208.96 COURSE: N 89-05-23 E		
PC	61+45.16	25463.2279	12382.1827
CC		24463.3541	12398.0706
PT	62+21.11	25461.5508	12458.0994
	DELTA: 04-21-07 TYPE: RIGHT		
	RADIUS: 1000.00 DEGREE: 05-43-46		
	LENGTH: 75.95 TANGENT: 38.00		
	MID-ORD: 0.72 EXTERNAL: 0.72		
	CHORD: 75.94 COURSE: S 88-44-04 E		
	ES: 0.72		
PI	63+23.14	25455.4259	12559.9469
	LENGTH: 1759.77 COURSE: S 89-23-18 E		
	DELTA: 2-49-47		
	62+21.11	TANGENT DATA	
	62+98.45	LENGTH: 77.33 COURSE: S 86-33-31 E	25461.5508 12458.0994
			25456.9086 12535.2928

AL_06-4 CIRCULAR CURVE DATA

PC	62+98.45	25456.9086	12535.2928
CC		26455.1052	12595.3216
PT	63+47.83	25455.1622	12584.6441
	DELTA: 02-49-47 TYPE: LEFT		
	RADIUS: 1000.00 DEGREE: 05-43-46		
	LENGTH: 49.39 TANGENT: 24.70		
	MID-ORD: 0.30 EXTERNAL: 0.30		
	CHORD: 49.38 COURSE: S 87-58-24 E		
	ES: 0.30		
PI	80+82.91	25436.6358	14319.6187
	LENGTH: 1735.07 COURSE: S 89-23-18 E		
	63+47.83	TANGENT DATA	
	80+82.91	LENGTH: 1735.07 COURSE: S 89-23-18 E	25455.1622 12584.6441
			25436.6358 14319.6187



MATCHLINE - SEE SHEET 7

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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

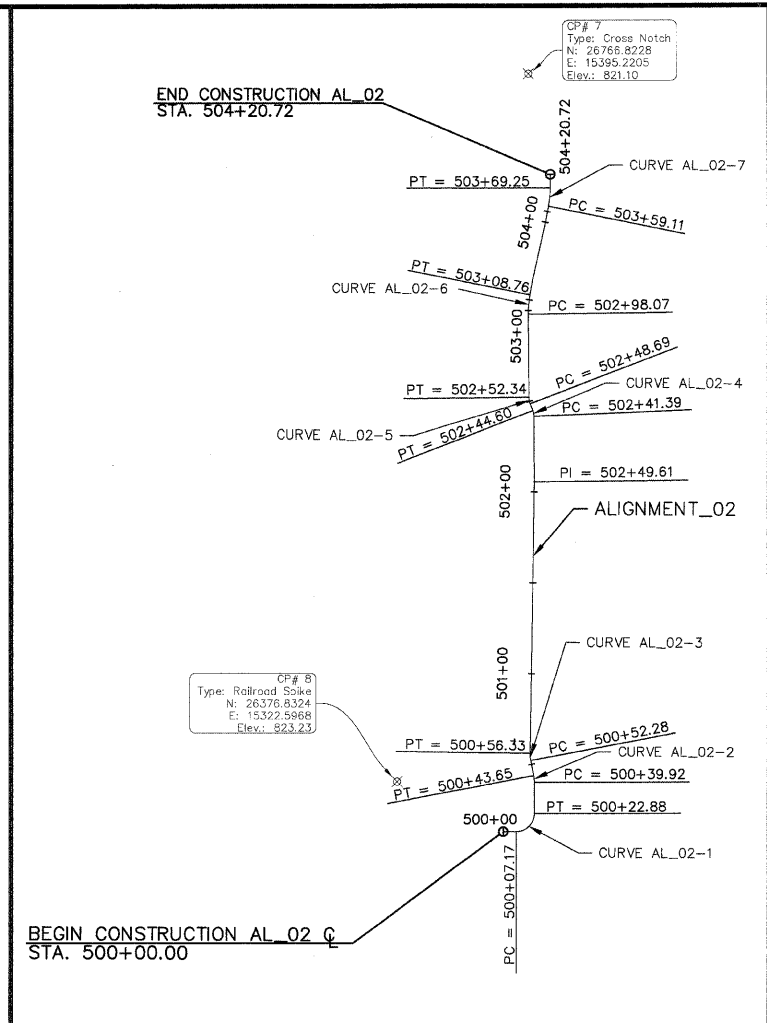
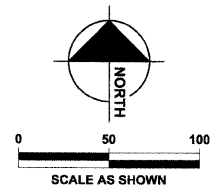
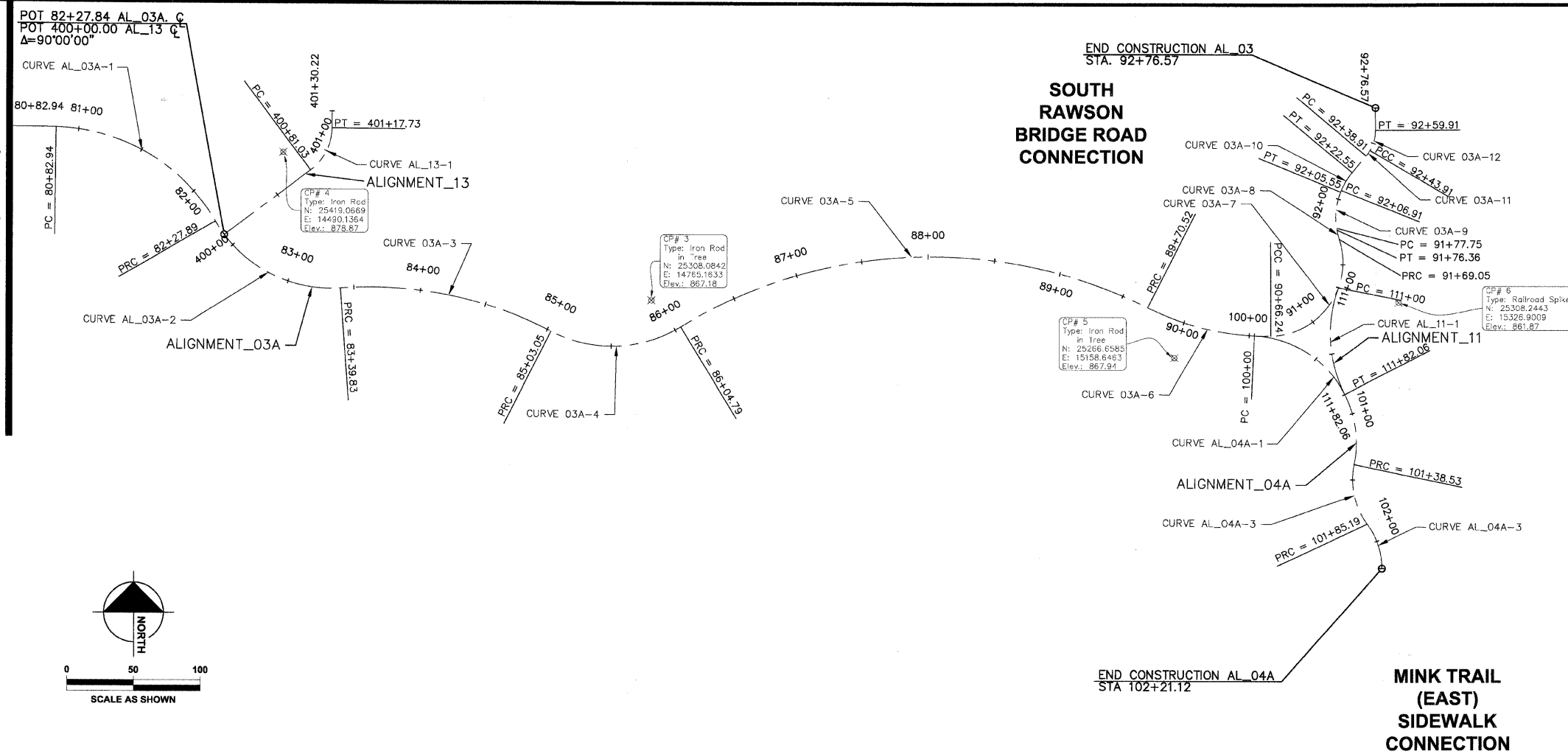
**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**ALIGNMENT DATA
STA. 50+00 TO STA. 66+50
STA. 66+50 TO STA. 80+50
STA. 300+00 TO STA. 300+82.50**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	6
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



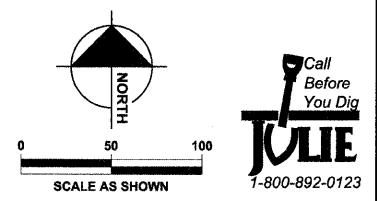
MATCHLINE - SEE SHEET 6



CRYSTAL LAKE AVENUE CONNECTION

ALIGNMENT DATA LEGEND

- TRAIL CENTER LINE
- CC COMPOUND CURVE
- PC POINT OF CURVATURE
- PI POINT OF INTERSECTION
- PT POINT OF TANGENCY
- POT POINT OF TERMINATION



SEE SHEET 08 FOR ADDITIONAL ALIGNMENT DATA

HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.
ALIGNMENT: ALIGNMENT_13
DESC: NEW CASTLE DRIVE CONNECTION

DESC.	STATION	SPIRAL/CURVE DATA	NORTHING	EASTING
PI	400+00	LENGTH: 100.78	25356.7339	14445.9449
		COURSE: N 53-01-07 E		
PI	401+00.78	LENGTH: 32.25	25417.3595	14526.4520
		DELTA: 52-34-06		
		TANGENT DATA		
	400+00		25356.7339	14445.9449
	400+81.03	LENGTH: 81.03	25405.4755	14510.6707
		COURSE: N 53-01-07 E		
		AL_13-1 CIRCULAR CURVE DATA		
PC	400+81.03		25405.4755	14510.6707
CC			25437.4287	14486.6084
PT	401+17.73		25437.1144	14526.6072
		DELTA: 52-34-06		
		RADIUS: 40.00	DEGREE: 143-14-22	LEFT
		LENGTH: 36.70	TANGENT: 19.76	
		MID-ORD: 4.14	EXTERNAL: 4.61	
		CHORD: 35.43	COURSE: N 26-44-04 E	
		ES: 4.61		
PI	401+30.22		25449.6087	14526.7054
		TANGENT DATA		
	401+17.73		25437.1144	14526.6072
	401+30.22	LENGTH: 12.49	25449.6087	14526.7054
		COURSE: N 00-27-00 E		

HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.
ALIGNMENT: ALIGNMENT_11
DESC: ALIGNMENT 03 / 04 CONNECTOR

DESC.	STATION	SPIRAL/CURVE DATA	NORTHING	EASTING
PI	111+00	LENGTH: 43.50	25319.4190	15281.8516
		COURSE: S 20-04-51 W		
PI	111+43.50	LENGTH: 43.50	25278.5664	15266.9170
		DELTA: 47-00-54		
		AL_11A-1 CIRCULAR CURVE DATA		
PC	111+00		25319.4190	15281.8516
CC			25285.0843	15375.7724
PT	111+82.06		25239.7878	15286.6196
		DELTA: 47-00-54		
		RADIUS: 100.00	DEGREE: 57-17-45	LEFT
		LENGTH: 82.06	TANGENT: 43.50	
		MID-ORD: 8.30	EXTERNAL: 9.05	
		CHORD: 79.77	COURSE: S 03-25-36 E	
		ES: 9.05		
PI	111+82.06			

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

ALIGNMENT DATA
STA. 80+50 TO 92+76.57
STA. 100+00 TO 102+21.12
STA. 111+00 TO 111+82.06
STA. 400+00 TO STA. 401+30.22; STA. 500+00 TO 504+20.72

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	7
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

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HORIZONTAL ALIGNMENT STATION AND CURVE REPORT.

Table with columns: STATION, SPIRAL / CURVE DATA, NORTHING, EASTING. Includes curve data for AL_03A-1 through AL_03A-7.

Table with columns: STATION, SPIRAL / CURVE DATA, NORTHING, EASTING. Includes curve data for AL_03A-8 through AL_03A-12.

Table with columns: STATION, SPIRAL / CURVE DATA, NORTHING, EASTING. Includes curve data for AL_04A-1 through AL_04A-3.

Table with columns: STATION, SPIRAL / CURVE DATA, NORTHING, EASTING. Includes curve data for AL_02-3 through AL_02-7.

ALIGNMENT DATA LEGEND

- TRAIL CENTER LINE
CC COMPOUND CURVE
PC POINT OF CURVATURE
PI POINT OF INTERSECTION
PT POINT OF TANGENCY
POT POINT OF TERMINATION

Vertical text on the left margin containing project details and dates.

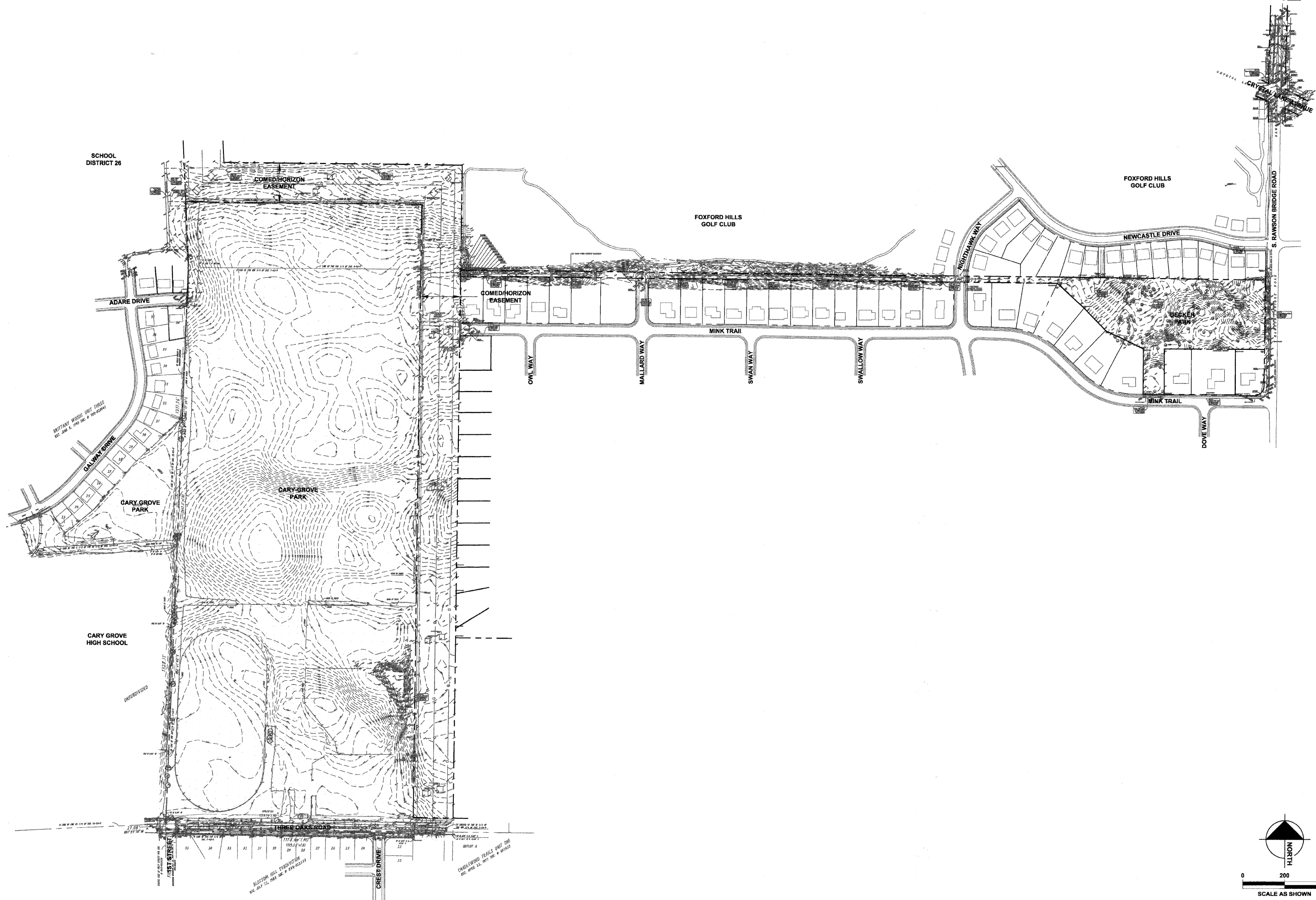
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT CARY COMMUNITY TRAIL

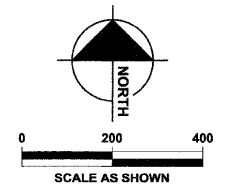
ALIGNMENT DATA STA. 80+00 TO 92+76.57 STA. 100+00 TO 102+21.12 STA. 111+00 TO 111+82.06

Table with columns: DESIGNED, DRAWN, CHECKED, DATE, REVISED, SECTION, COUNTY, TOTAL SHEETS, SHEET NO., FED. ROAD DIST. NO. 1, ILLINOIS, CONTRACT NO. 63333.





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DESIGNED	- JWH
DRAWN	- LES
CHECKED	- JMM
DATE	- 10/02/2009

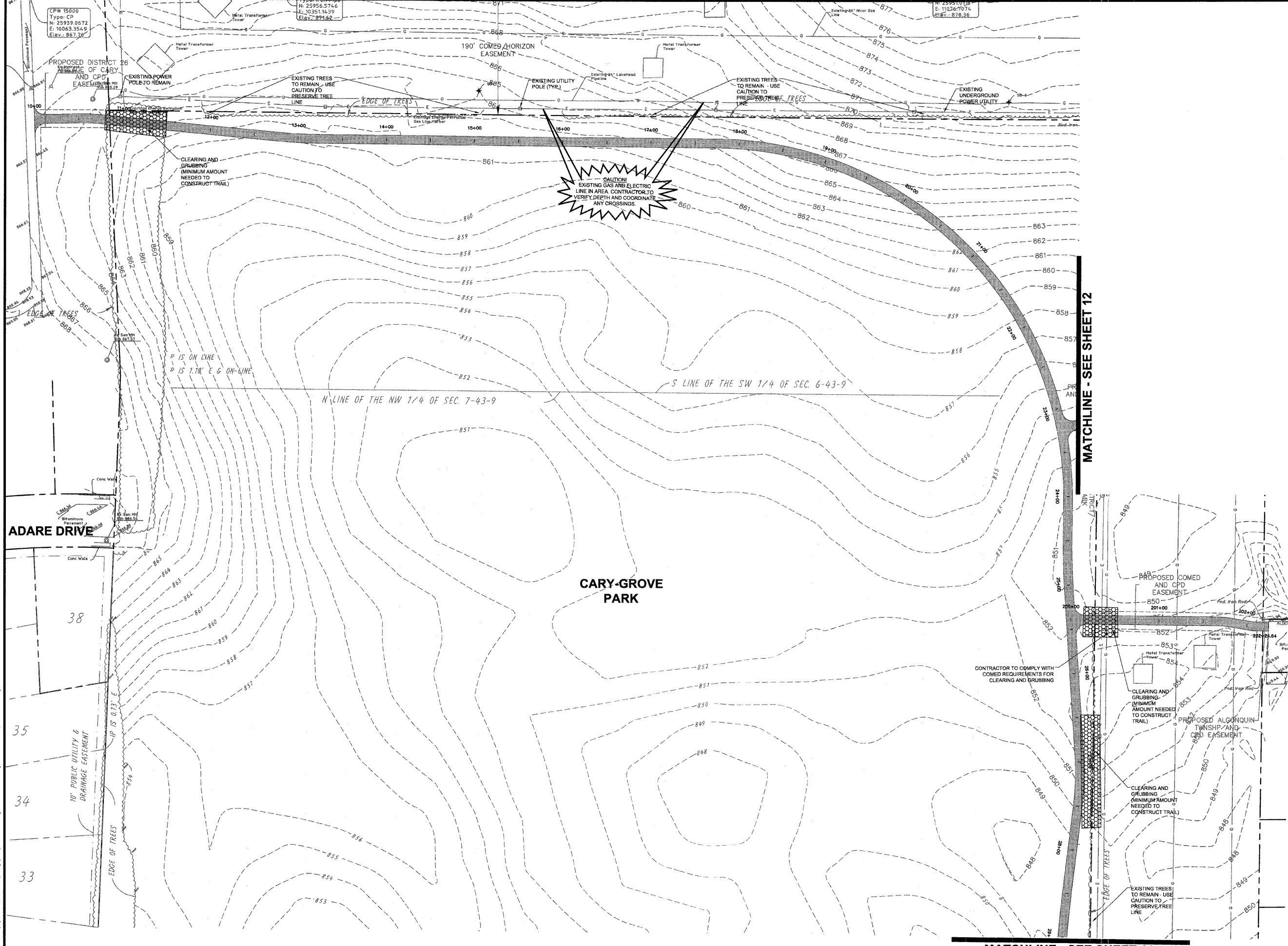
REVISED	
REVISED	
REVISED	
REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

OVERALL
EXISTING CONDITIONS

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	9
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



DEMOLITION LEGEND:

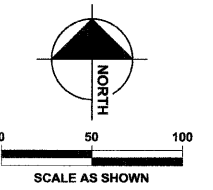
- ASPHALT PAVEMENT AND CONCRETE REMOVAL
- CLEARING AND GRUBBING
- TREE REMOVAL
- TREE PROTECTION, SEE DETAIL SHEET 55
- CURB AND GUTTER REMOVAL
- PAVEMENT MARKING REMOVAL
- PROPOSED TRAIL
- EXISTING TREE LINE

DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 38 ON THE MANHOLE CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PRODUCTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT, DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASUREMENT THE CONVERSION TO NAVD 83 ELEVATION FOR MH 46 USING NAD 83 VERTICAL CONVERSION WOULD BE 861.32 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:
 NAVD 83 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 83-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

- SOURCE BENCHMARK**
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHOLE CONSULTING LTD DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY.
 ELEVATION: 861.45
- SITE BENCHMARKS**
- HE OF #10
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 844' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 28' NORTHWEST OF POWER POLE.
 ELEVATION: 854.64
- HE OF #1
 DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT
 LOCATION: 562' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHWEST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97
- HE OF #8
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 825.23



This document, together with the concepts and designs presented herein, is intended only for the specific purpose and site for which it was prepared. It is not to be used for any other purpose without the express written consent of the engineer. The engineer's responsibility is limited to the work shown on these drawings. The contractor is responsible for obtaining all necessary permits and for complying with all applicable laws, codes, and regulations. The contractor shall verify the accuracy of all data and conditions shown on these drawings. The contractor shall be responsible for any and all errors and omissions. The contractor shall be responsible for any and all damages, claims, and liabilities arising out of or in connection with the performance of the work shown on these drawings. The contractor shall be responsible for any and all costs and expenses incurred in connection with the performance of the work shown on these drawings. The contractor shall be responsible for any and all delays and interruptions in the performance of the work shown on these drawings. The contractor shall be responsible for any and all safety and health hazards arising out of or in connection with the performance of the work shown on these drawings. The contractor shall be responsible for any and all environmental impacts arising out of or in connection with the performance of the work shown on these drawings. The contractor shall be responsible for any and all other matters relating to the performance of the work shown on these drawings.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

EXISTING CONDITIONS AND DEMOLITION PLAN
 STA. 10+00 TO STA. 29+00
 STA. 200+00 TO STA. 202+24.64

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	10
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

MATCHLINE - SEE SHEET 10

MATCHLINE - SEE THIS SHEET

DEMOLITION LEGEND:

- ASPHALT PAVEMENT AND CONCRETE REMOVAL
- CLEARING AND GRUBBING
- TREE REMOVAL
- TREE PROTECTION, SEE DETAIL SHEET 55
- CURB AND GUTTER REMOVAL
- PAVEMENT MARKING REMOVAL
- PROPOSED TRAIL
- EXISTING TREE LINE

DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

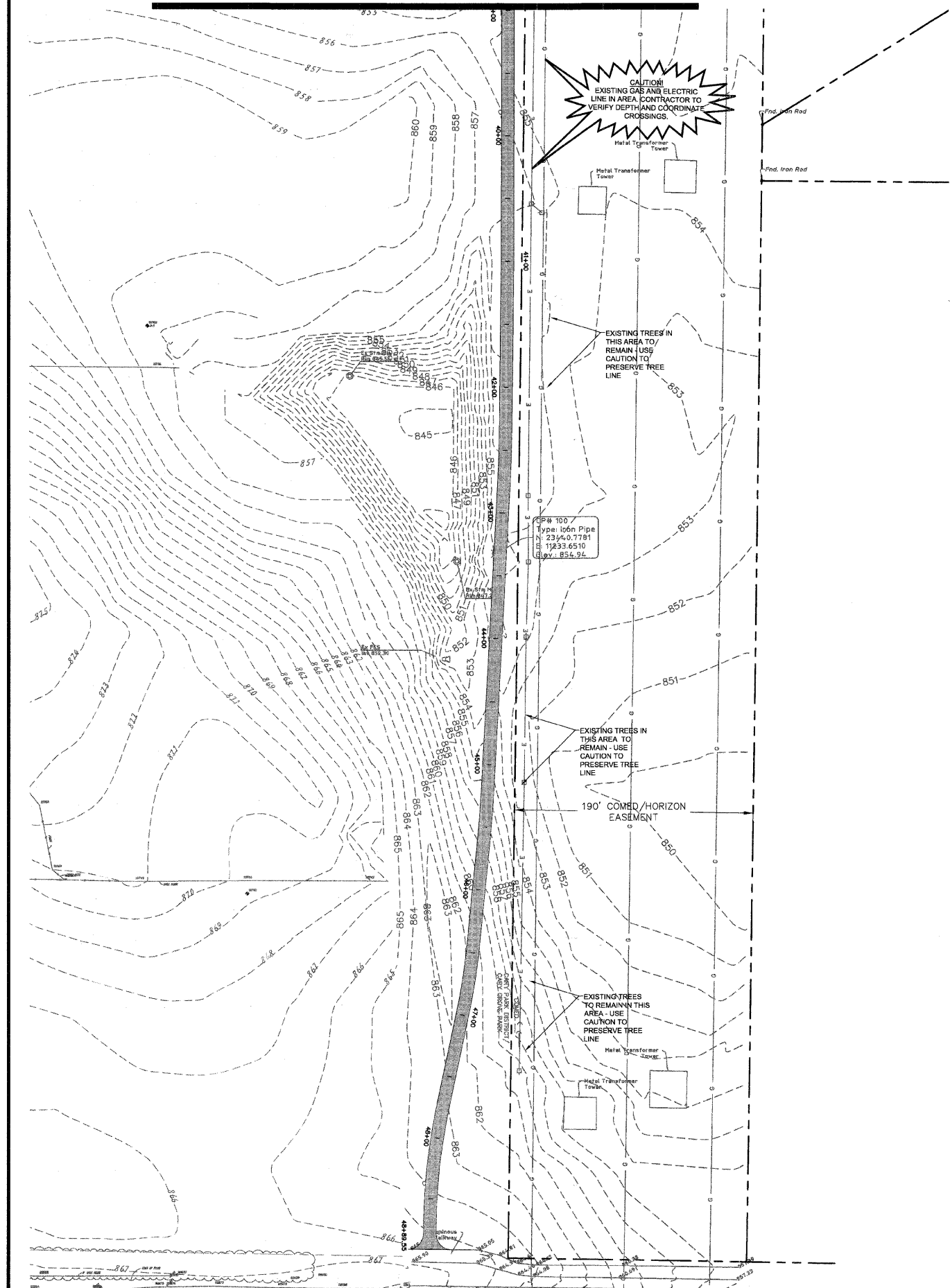
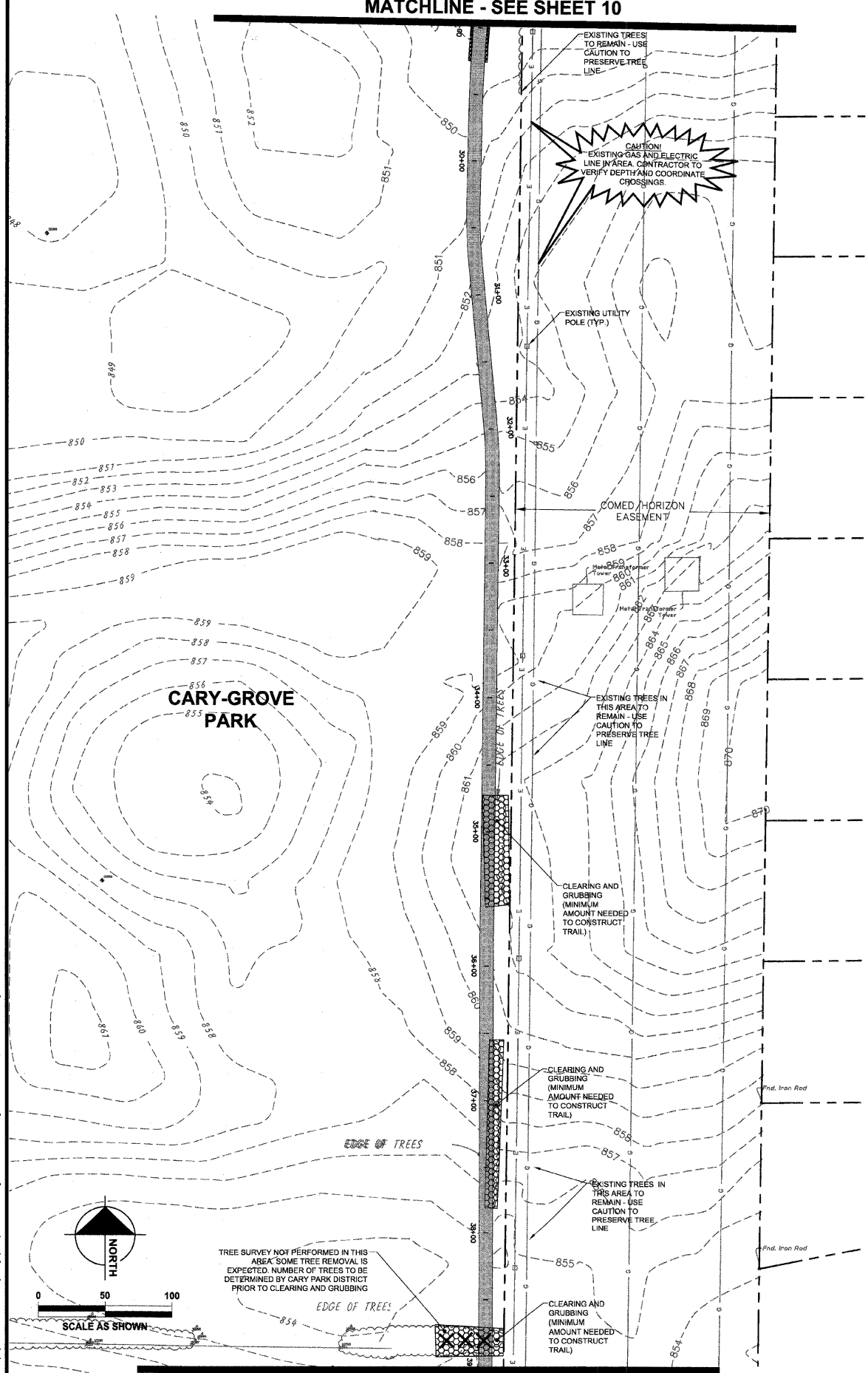
THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2009 ("THE TOPO") WAS BASED ON MH 48 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 4, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 48 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 48 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 48 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 48 USING NAVD HORIZONTAL CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 48 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 48 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 83 (GEOID 03) - LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29 - RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

- SOURCE BENCHMARK**
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION - PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 851.45
- SITE BENCHMARKS**
 HE OF #100
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94
- HE OF #1
 DESCRIPTION: "M" NAIL IN EDGE OF PAVEMENT
 LOCATION: 592' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MAIN TRAIL, 45' SOUTHWEST OF TREE 27 SOUTHEAST OF IRON ROD, AND 17' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97
- HE OF #8
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23



MATCHLINE - SEE THIS SHEET

MATCHLINE - SEE THIS SHEET

DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
 CARY COMMUNITY TRAIL

EXISTING CONDITIONS AND DEMOLITION
 STA. 29+00 TO STA. 39+00
 STA. 39+00 TO 48+89.55

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
76-P4001-00-BR	MCHENRY	64	11
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

This document, together with the complete set of design plans and specifications, is intended only for the specific purpose and client for which it was prepared. It is not to be used for any other purpose without the written consent of the engineer. The engineer's responsibility is limited to the design and construction of the project as shown on these drawings. The contractor is responsible for the construction of the project in accordance with the design and specifications. The engineer does not warrant the accuracy of the information provided herein.



MATCHLINE - SEE SHEET 10

MATCHLINE - SEE THIS SHEET

MATCHLINE - SEE THIS SHEET

MATCHLINE - SEE SHEET 13

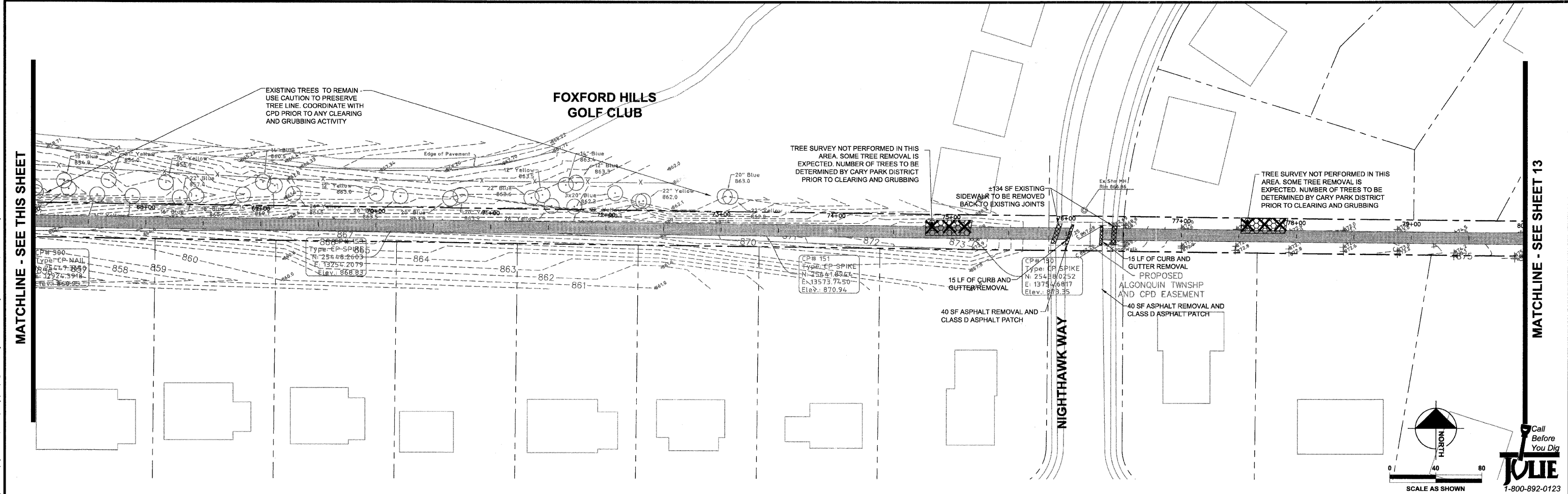
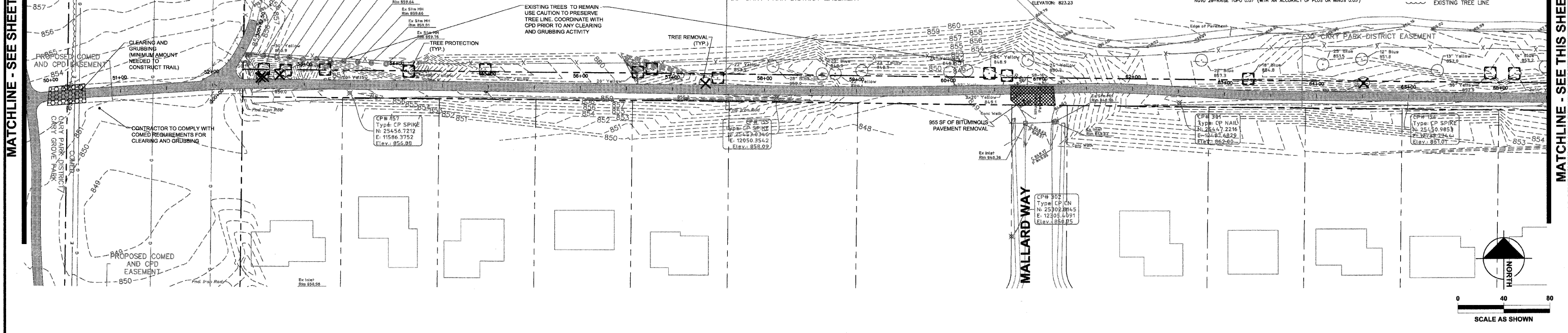
FOXFORD HILLS GOLF CLUB

FOXFORD HILLS GOLF CLUB

BENCHMARKS
 SOURCE BENCHMARK: MANHOLE #48 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE. ELEVATION: 851.45
 SITE BENCHMARKS
 HE CP #102
 DESCRIPTION: IRON PIPE
 LOCATION: 100' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 564' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 20' NORTHEAST OF POWER POLE. ELEVATION: 854.94
 HE CP #1
 DESCRIPTION: "PH" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 302' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MAIN TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK. ELEVATION: 852.97
 HE CP #2
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHEAST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT. ELEVATION: 853.23

DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY
 THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT, DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KAMA COMPANY'S RTK CROSS REFERENCE NETWORK USING NAVD 86 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 86 (GEOID 03) AS THE REFERENCE DATUM BASED ON THE GPS MEASURED HEIGHT. THE CONVERSION TO NGVD 29 ELEVATION FOR MH 46 USING NOS VERTICAL CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:
 NAVD 86 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NGVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

- DEMOLITION LEGEND:**
- ASPHALT PAVEMENT AND CONCRETE REMOVAL
 - CLEARING AND GRUBBING
 - TREE REMOVAL
 - TREE PROTECTION, SEE DETAIL SHEET 55
 - CURB AND GUTTER REMOVAL
 - PAVEMENT MARKING REMOVAL
 - PROPOSED TRAIL
 - EXISTING TREE LINE



This document, together with the concept and design presented herein, is intended only for the specific purpose and client for which it was prepared. Review drawings were made by the Designer/Engineer/Architect/Planner/Interior Designer/Contractor/Manufacturer/Supplier/Installer/Constructor/Operator/Maintainer/Owner/End User. No liability is assumed by the Designer/Engineer/Architect/Planner/Interior Designer/Contractor/Manufacturer/Supplier/Installer/Constructor/Operator/Maintainer/Owner/End User for any use of this document for any other purpose.

DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

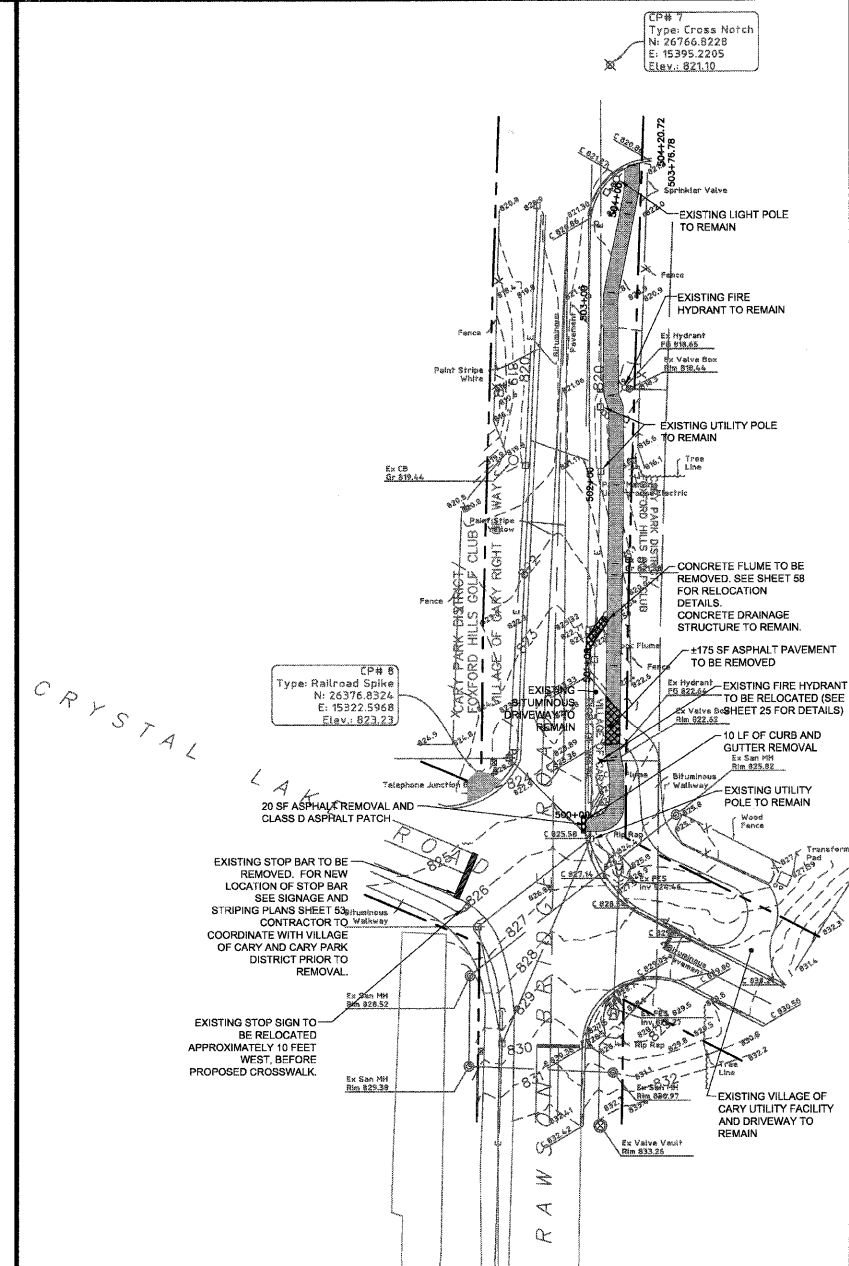
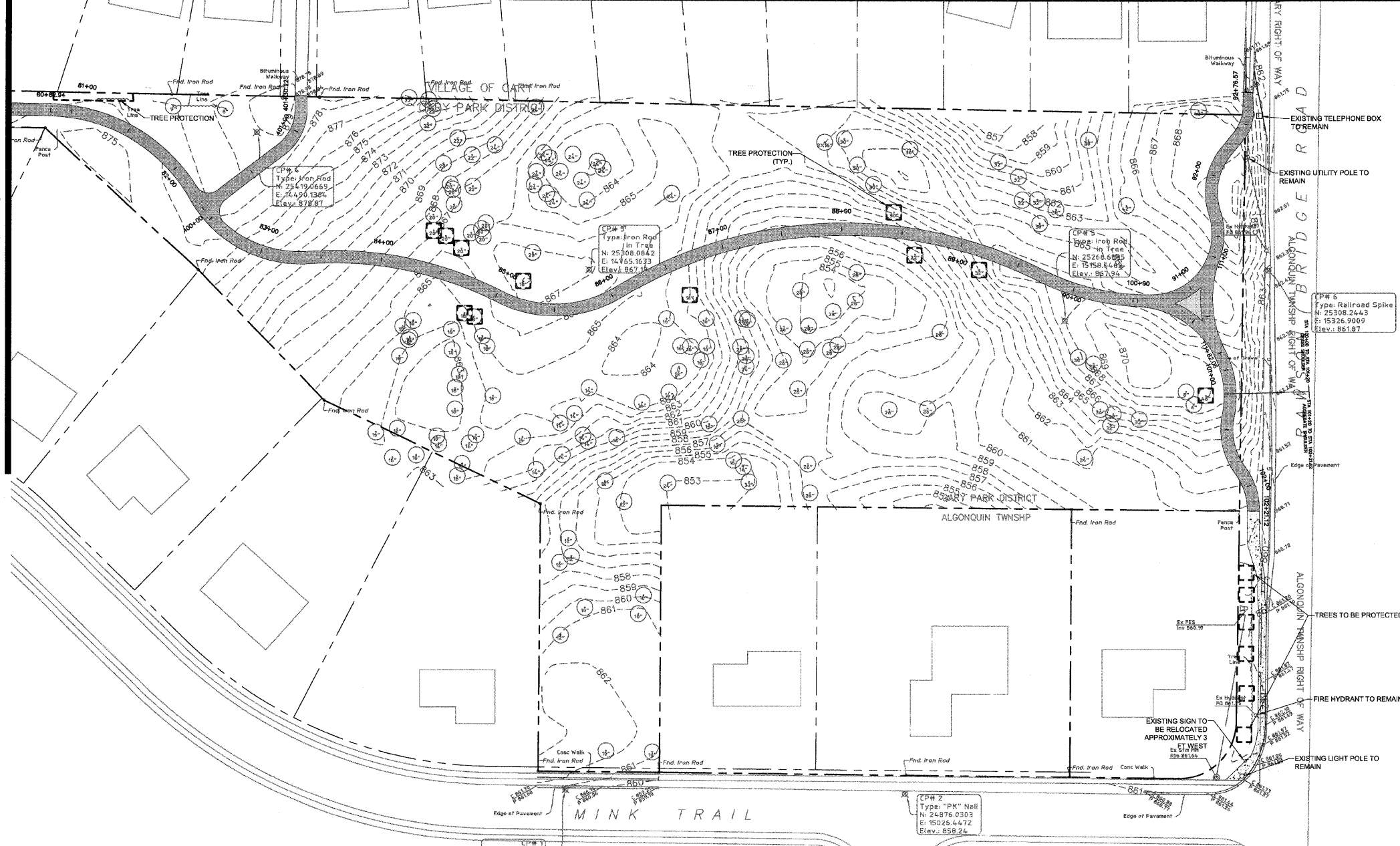
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

EXISTING CONDITIONS AND DEMOLITION PLAN
STA. 50+00 TO STA. 66+50
STA. 66+50 TO STA. 80+50
STA. 300+00 TO STA. 300+82.50

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	12
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



MATCHLINE - SEE SHEET 12



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC, DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARL COMPANY'S RTK CORE REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 46 USING NOS VERTCON CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

BASED ON HAEGER ENGINEERING, LLC, GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 88 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK:
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

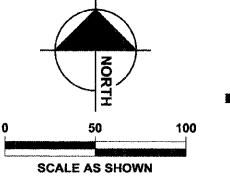
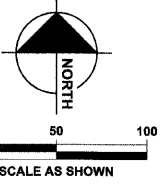
SITE BENCHMARKS:
 HE CP #100
 DESCRIPTION: IRON PIPE.
 LOCATION: 100' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 564' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

HE CP #1
 DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 582' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 42' SOUTHWEST OF TREE 2, 22' SOUTHWEST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

HE CP #6
 DESCRIPTION: RAIL ROAD SPIKE.
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 16' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23

DEMOLITION LEGEND:

- ASPHALT PAVEMENT AND CONCRETE REMOVAL
- CLEARING AND GRUBBING
- TREE REMOVAL
- TREE PROTECTION, SEE DETAIL SHEET 55
- CURB AND GUTTER REMOVAL
- PAVEMENT MARKING REMOVAL
- PROPOSED TRAIL
- EXISTING TREE LINE
- PROPOSED SIDEWALK



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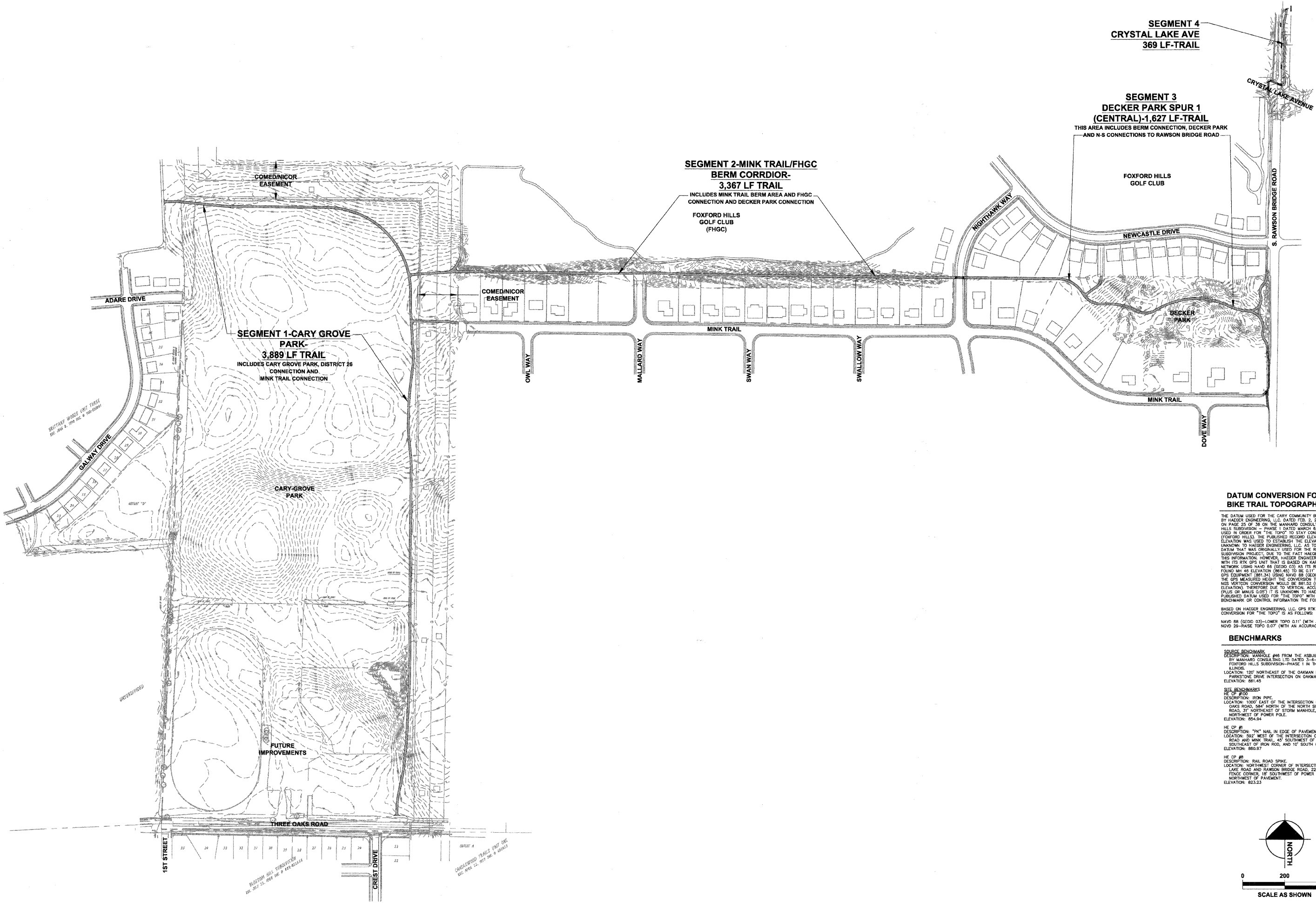
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

EXISTING CONDITIONS AND DEMOLITION PLAN
 STA. 80+50 TO 92+76.57
 STA. 100+00 TO 102+21.12
 STA. 111+00 TO 111+82.06
 STA. 400+00 TO STA. 401+30.22; STA. 500+00 TO 504+20.72

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	13
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63333	



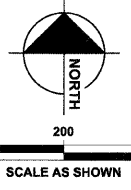
DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2009 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 38 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 881.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARA COMPANY'S RTK CORE REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (881.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 46 USING NAD 29 VERTIC CONVERSION WOULD BE 881.22 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:
 NAVD 88 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

- SOURCE BENCHMARK**
 DESCRIPTION: MANHOLE #46 FROM THE AS-BUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD. DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE DAMIAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON DAMIAN DRIVE.
 ELEVATION: 881.45
- SITE BENCHMARKS**
- HE CP #10**
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 884' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94
- HE CP #1**
 DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 502' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 880.87
- HE CP #8**
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHEAST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23



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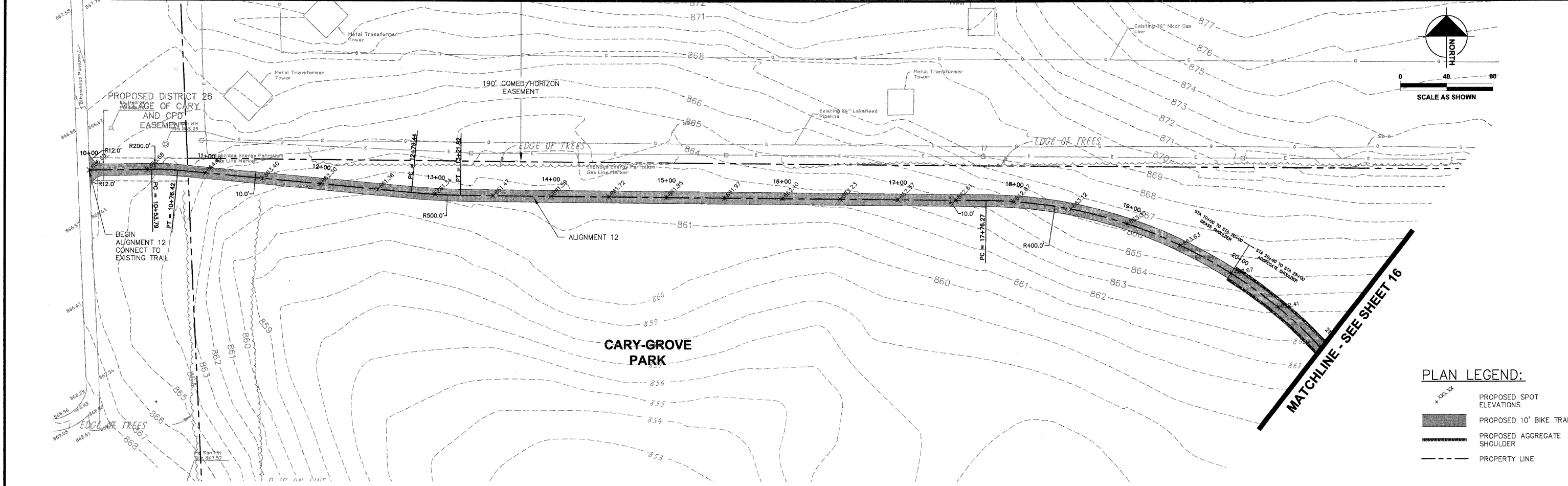
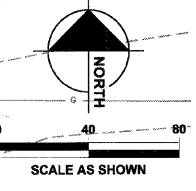
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

OVERALL TRAIL
ALIGNMENT PLAN

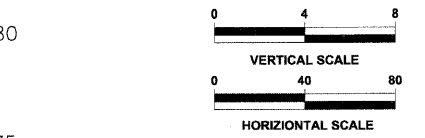
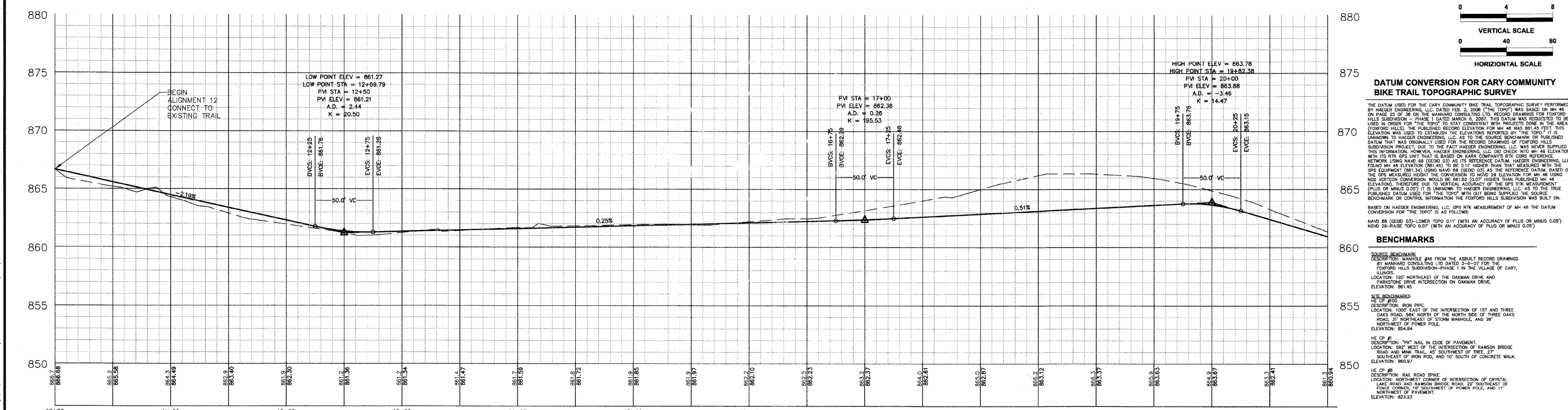
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	14
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



CARY COMMUNITY TRAIL ALIGNMENT 12 - STA. 10+00 TO 21+00

PLAN LEGEND:

+XXXXX	PROPOSED SPOT ELEVATIONS
	PROPOSED 10' BIKE TRAIL
	PROPOSED AGGREGATE SHOULDER
	PROPERTY LINE



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 48 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 48 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT, DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAEGER ENGINEERING, LLC DOES CHECK INTO MH 48 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARLA COMPANY'S RTK CORS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 48 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 48 USING NOS VERTICAL CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 48 ELEVATION). THEREFORE DUE TO NEGLIGIBLE ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR THE TOPO WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 48 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:
 NAVD 83 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #48 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS

HE CP #100
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 364' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.64

HE CP #1
 DESCRIPTION: "M" NAIL IN EDGE OF PAVEMENT
 LOCATION: 582' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MNK TRAIL, 45' SOUTHWEST OF TREE, 2' SOUTHWEST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

HE CP #8
 DESCRIPTION: SAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23

This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse or modification of this document for any other project without the written consent of the engineer is prohibited. The engineer and its consultants shall not be responsible for any errors or omissions in this document. The user of this document shall be responsible for obtaining all necessary permits and approvals. The user of this document shall be responsible for obtaining all necessary permits and approvals. The user of this document shall be responsible for obtaining all necessary permits and approvals.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

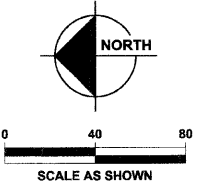
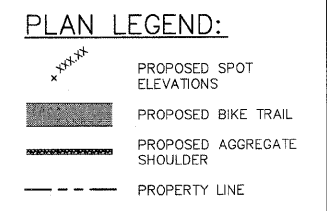
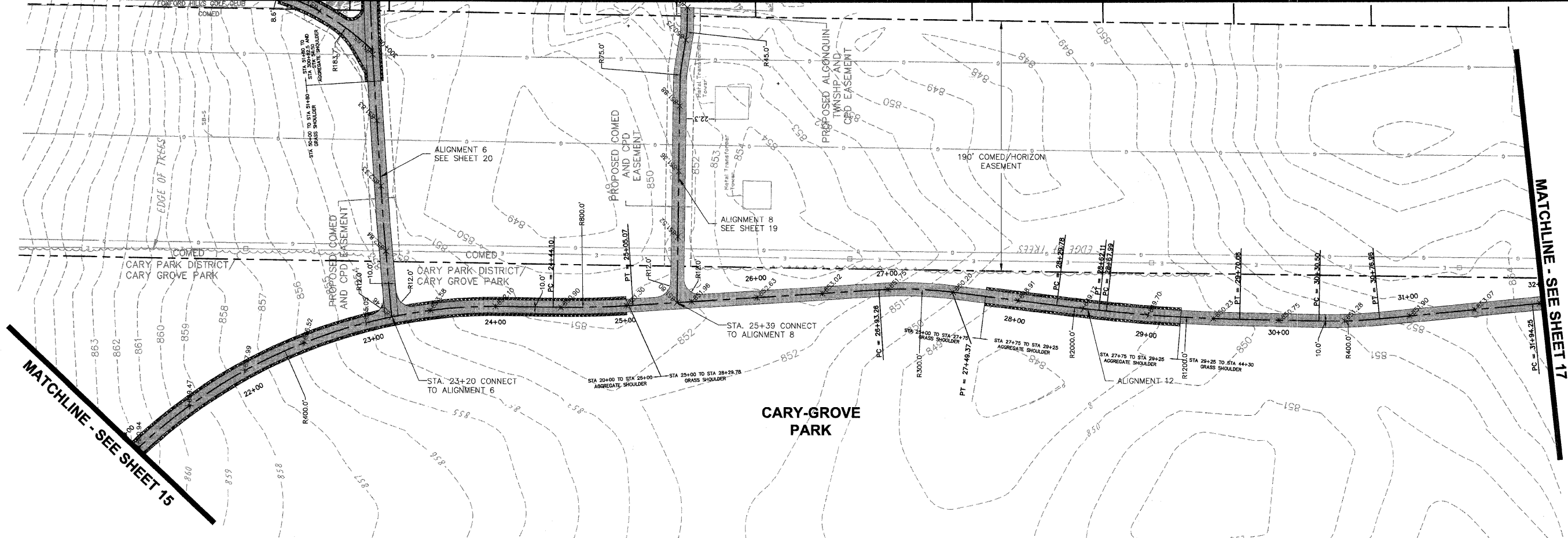
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

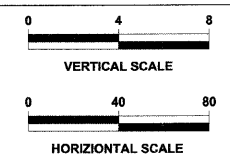
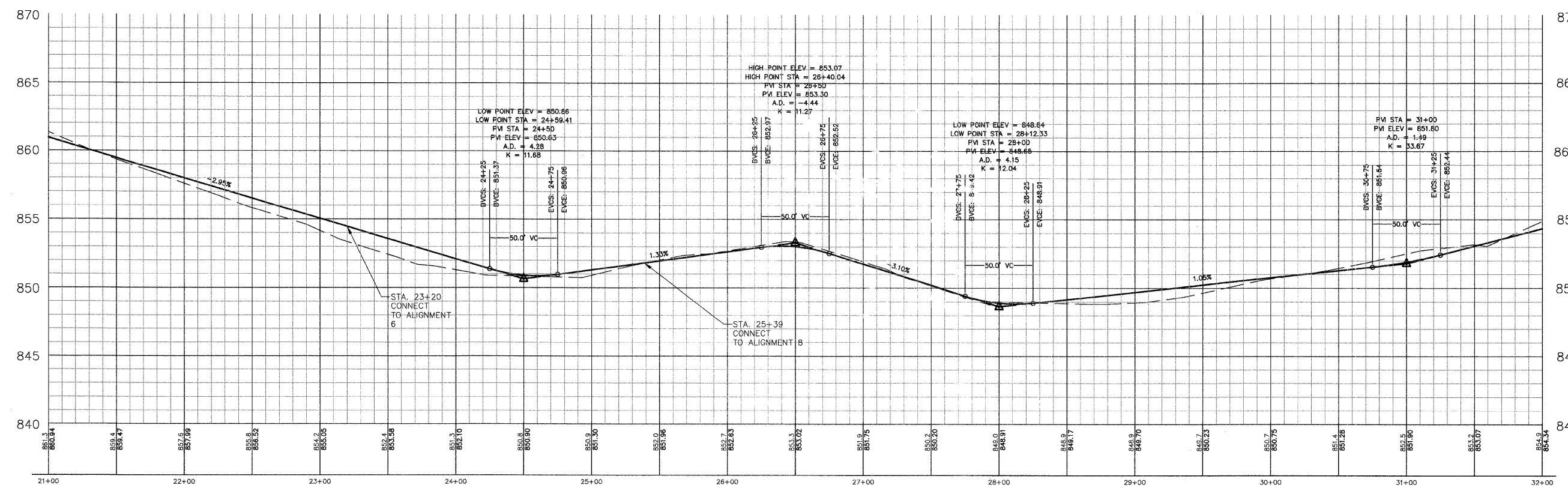
TRAIL ALIGNMENT
PLAN & PROFILE
STA. 10+00 TO STA. 21+00

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	15
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	





CARY COMMUNITY TRAIL ALIGNMENT 12 - STA. 21+00 TO 32+00



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2009 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 26 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA OF FOXFORD HILLS. THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO." IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED WITH ITS RTK GPS UNIT THAT IS BASED ON NAD 83 COMPANYS RTK GPS REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11" HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 88 ELEVATION FOR MH 46 USING NAD 83 VERTICAL CONVERSION WOULD BE 861.32 (0.07" HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05") IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 88 (GEOID 03)-LOWER TOPO 0.11" (WITH AN ACCURACY OF PLUS OR MINUS 0.05")
 NAVD 88-RAISE TOPO 0.07" (WITH AN ACCURACY OF PLUS OR MINUS 0.05")

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #46 FROM THE ASSULT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARSONS DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS
 #1 OF #1
 DESCRIPTION: IRON PIPE
 LOCATION: 1800' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 28' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

#2 OF #1
 DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 592' WEST OF THE INTERSECTION OF RANSON BRIDGE ROAD AND MARK TRAIL 40' SOUTHWEST OF TREE 27
 SOUTHWEST OF IRON ROD AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

#3 OF #1
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RANSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 862.23

This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of this document for any other project without the express written consent of the engineer is prohibited. The engineer and its staff shall not be held responsible for any errors or omissions in this document. Date: 10/02/2009 10:00 AM.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

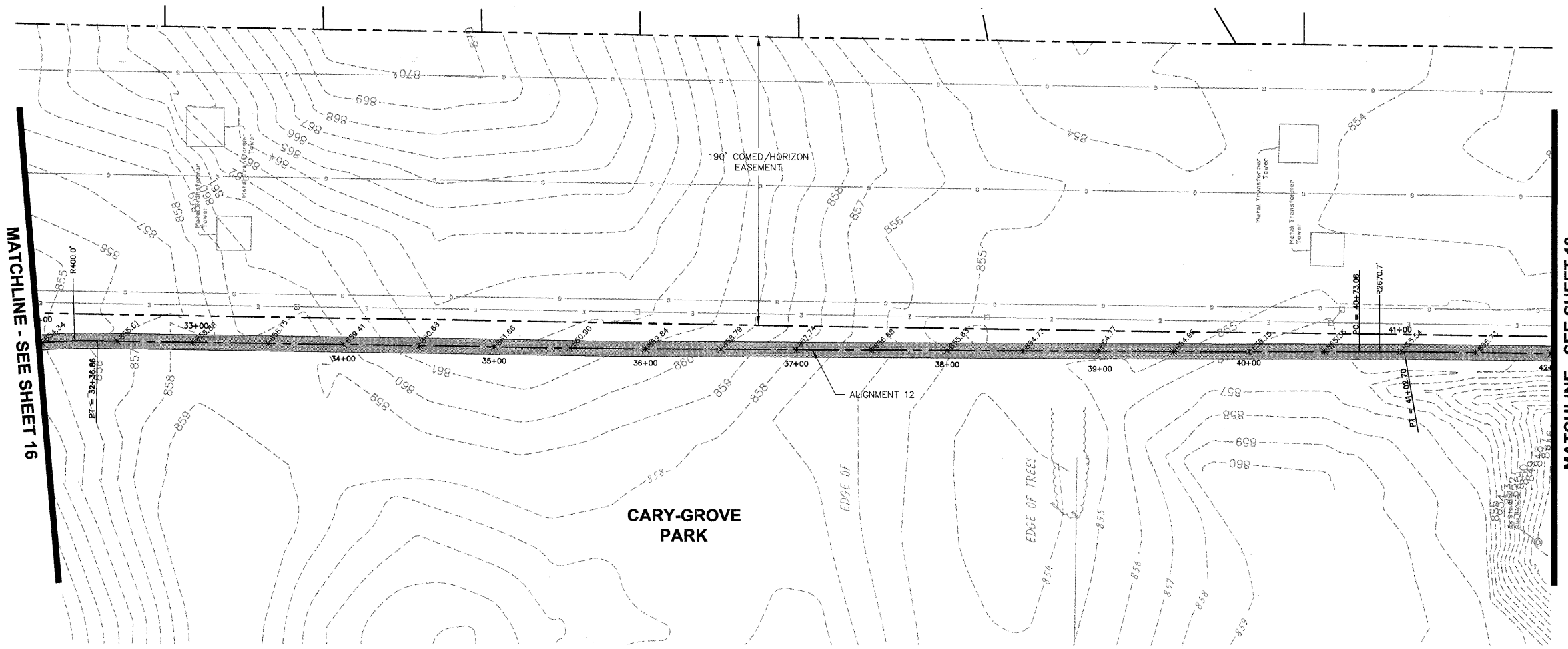
**TRAIL ALIGNMENT
 PLAN & PROFILE
 STA. 21+00 TO STA. 32+00**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	16
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

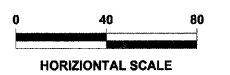
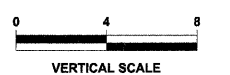
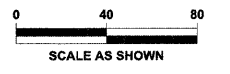
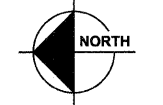


PLAN LEGEND:

- XXXXXX PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE



CARY COMMUNITY TRAIL ALIGNMENT 12 - STA. 32+00 TO 42+00



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAZGER ENGINEERING, LLC DATED FEB. 2, 2009 (THE TOPO) WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAZGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAZGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAZGER ENGINEERING, LLC CHECKED INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAZGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THIS INFORMATION CONVERSION WOULD BE 861.32 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE, DUE TO THE ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAZGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR THE TOPO WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAZGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR THE TOPO IS AS FOLLOWS:

NAVD 88 (GEOID 03) - LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 88 - RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

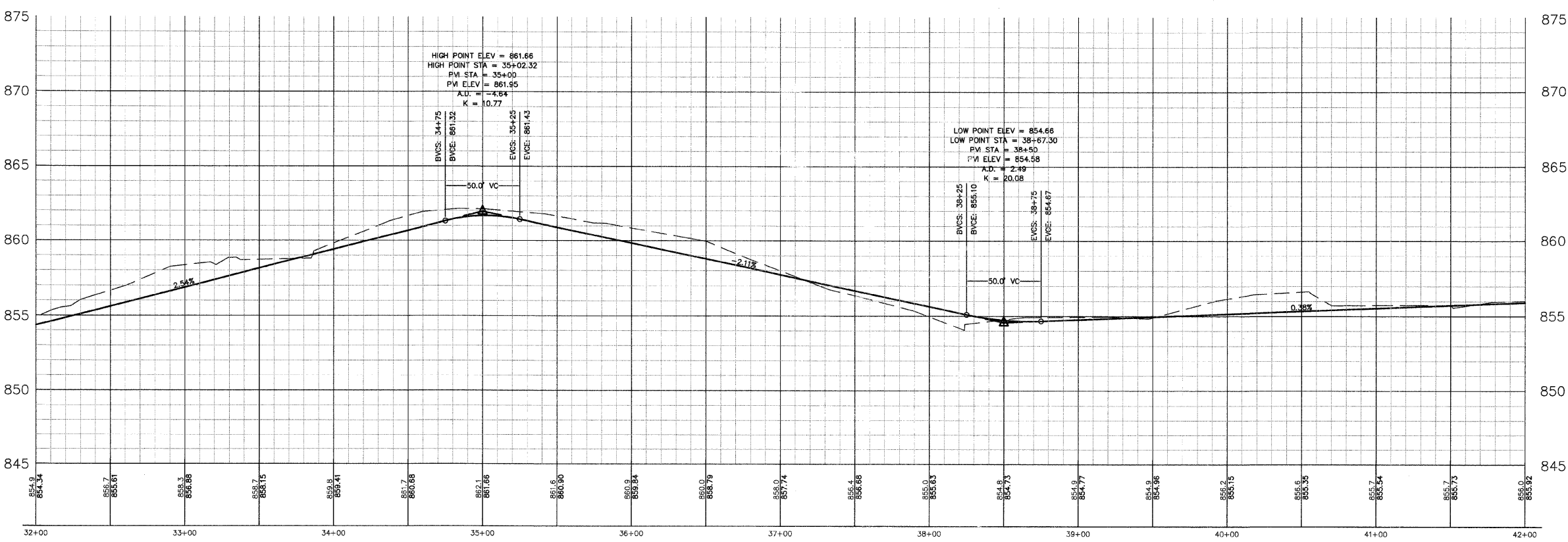
BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #46 FROM THE ASPHALT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSONS DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS
 HE CP #100
 DESCRIPTION: IRON PIPE.
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 864' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

HE CP #1
 DESCRIPTION: "TM" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 592' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND BIKI TRAIL, 45' SOUTHWEST OF TREE 27 SOUTHWEST OF THE INTERSECTION OF BIKI TRAIL AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

HE CP #6
 DESCRIPTION: RAIL ROAD SPIKE.
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 853.23



This document, together with the contract and design presented herein, is intended only for the specific purpose and site for which it was prepared. Reuse of this document for any other project without the written consent of the engineer is prohibited. The engineer and its staff shall not be held responsible for any errors or omissions in this document. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

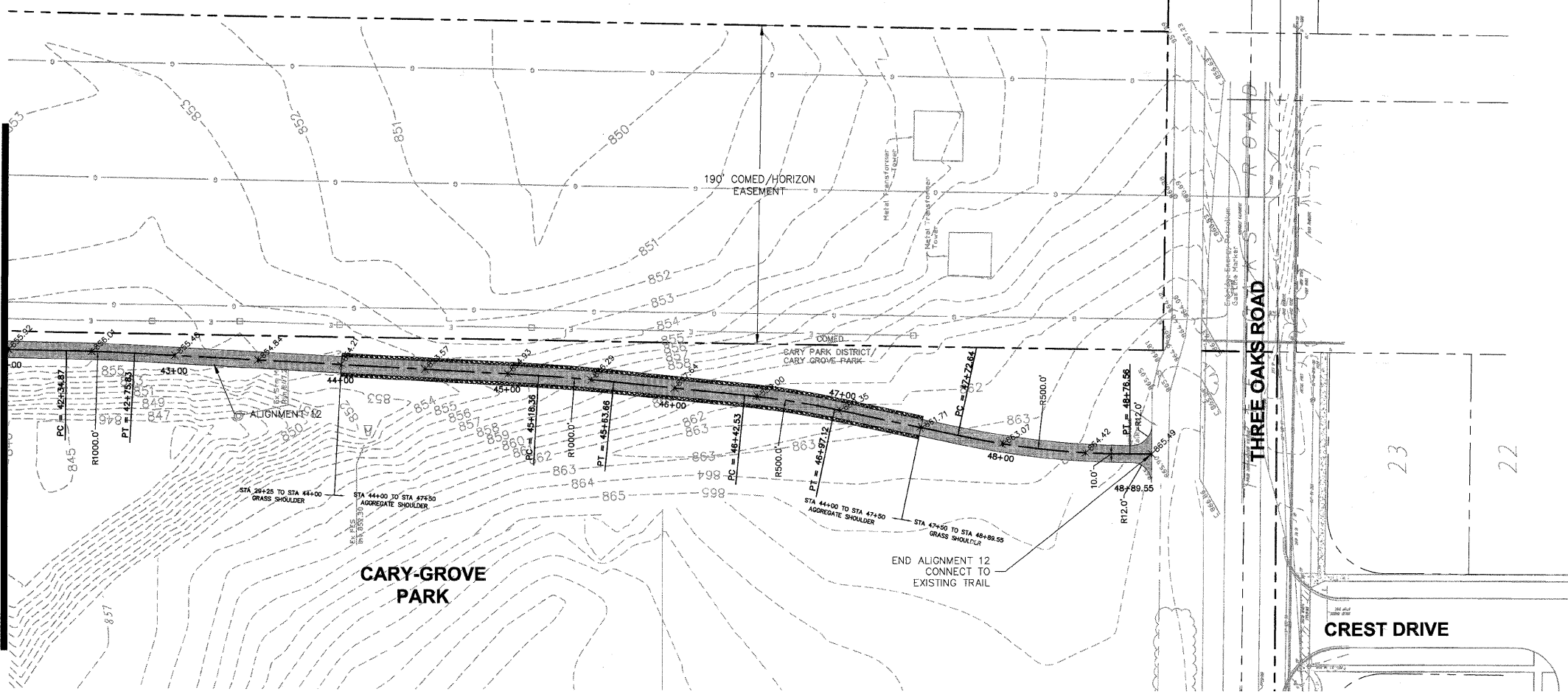
**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**TRAIL ALIGNMENT
PLAN & PROFILE**
STA. 32+00 TO STA. 42+00

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	17
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

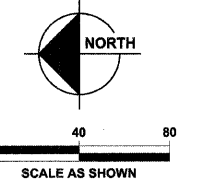


MATCHLINE - SEE SHEET 17

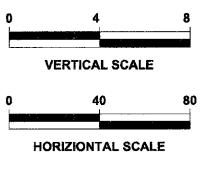
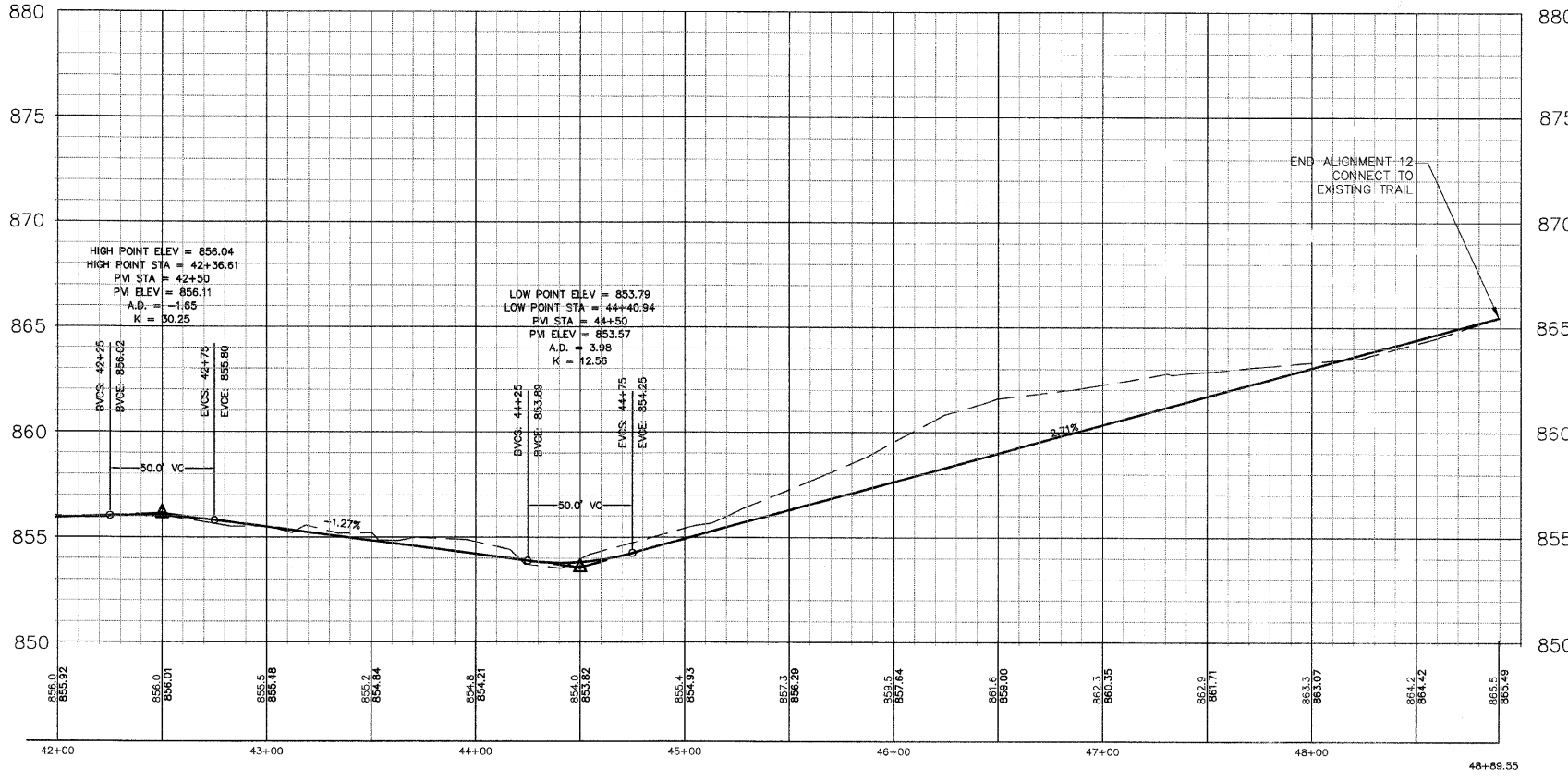


PLAN LEGEND:

- PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE



CARY COMMUNITY TRAIL ALIGNMENT 12 - STA. 42+00 TO 48+89.55



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 4, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK TO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARA COMPANY'S RTK CORS REFERENCE NETWORK USING HAND 46 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING HAND 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NGVD 29 ELEVATION FOR MH 46 USING NSD VERTICION CONVERSION WOULD BE 861.52 (LOFT) HIGHER THAN PUBLISHED MH 46 ELEVATION. THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO", WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

HAND 88 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

NGVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #48 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITL BENCHMARKS
 HE CP #100
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 644' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 25' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

HE CP #1
 DESCRIPTION: "T" MARK IN EDGE OF PAVEMENT.
 LOCATION: 592' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MANK TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHEAST OF IRON ROD, AND 11' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

HE CP #2
 DESCRIPTION: RAIL ROAD SPIKE.
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 15' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23

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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

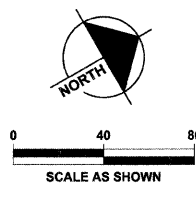
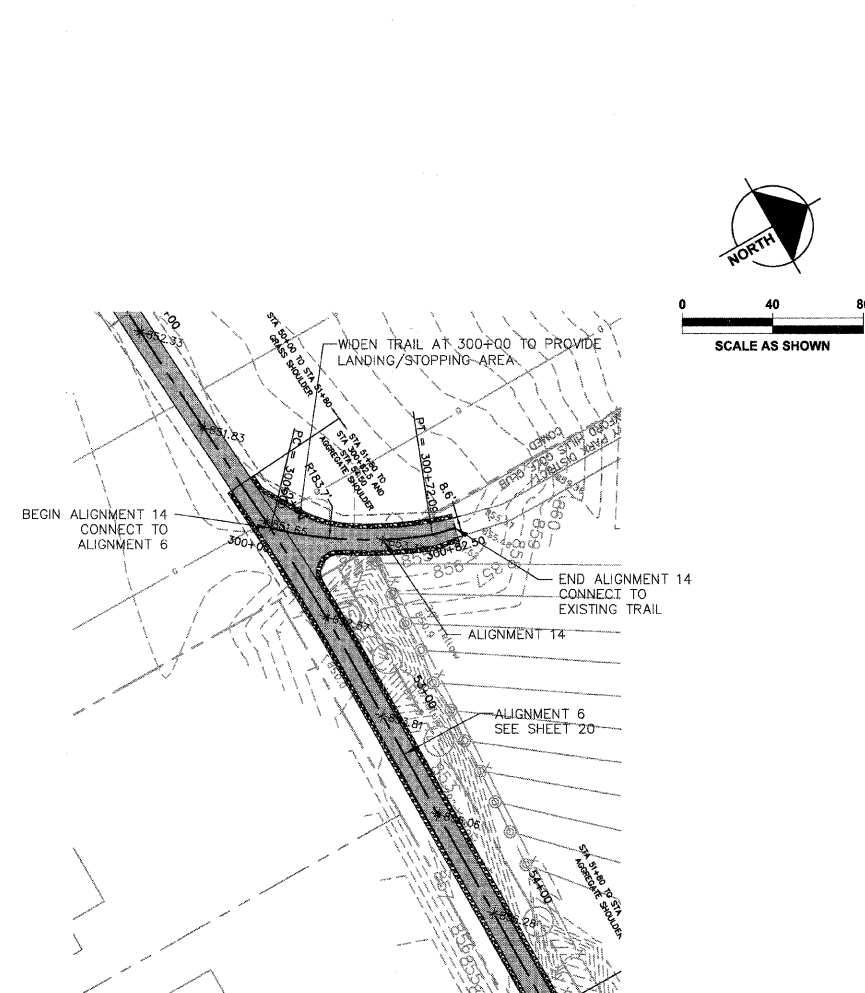
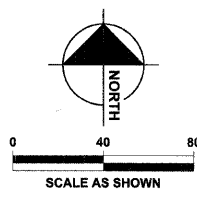
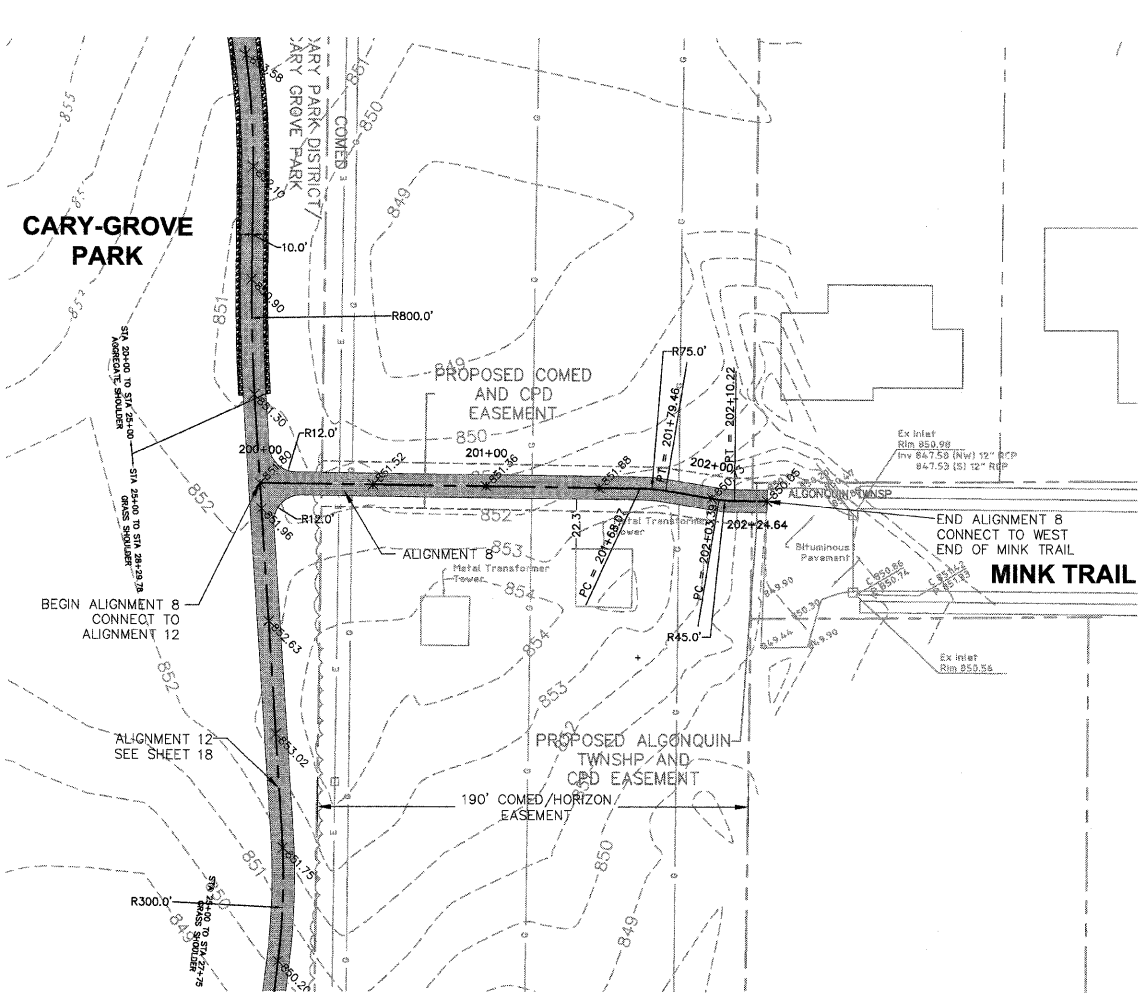
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

**TRAIL ALIGNMENT
 PLAN & PROFILE
 STA. 42+00 TO STA. 48+89.55**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	18
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



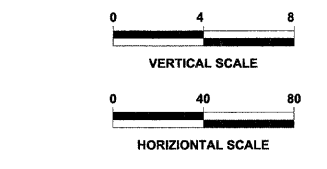
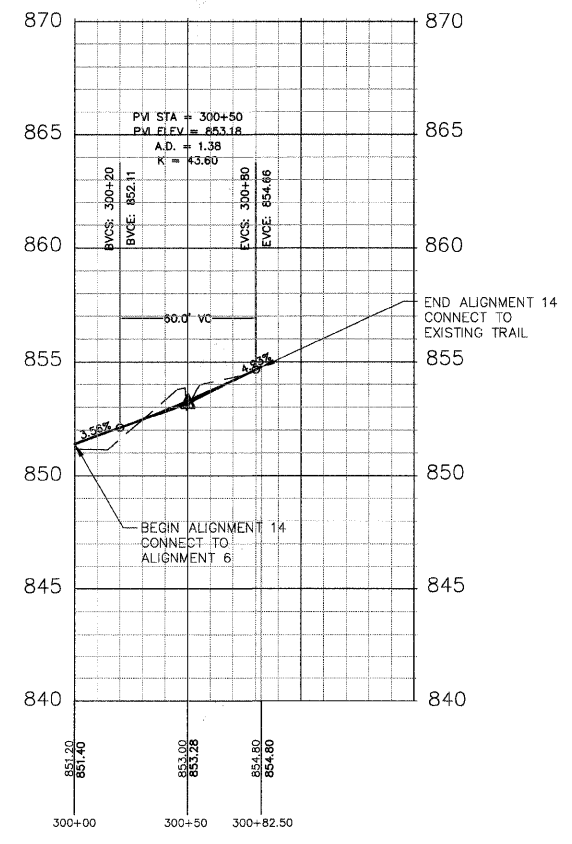
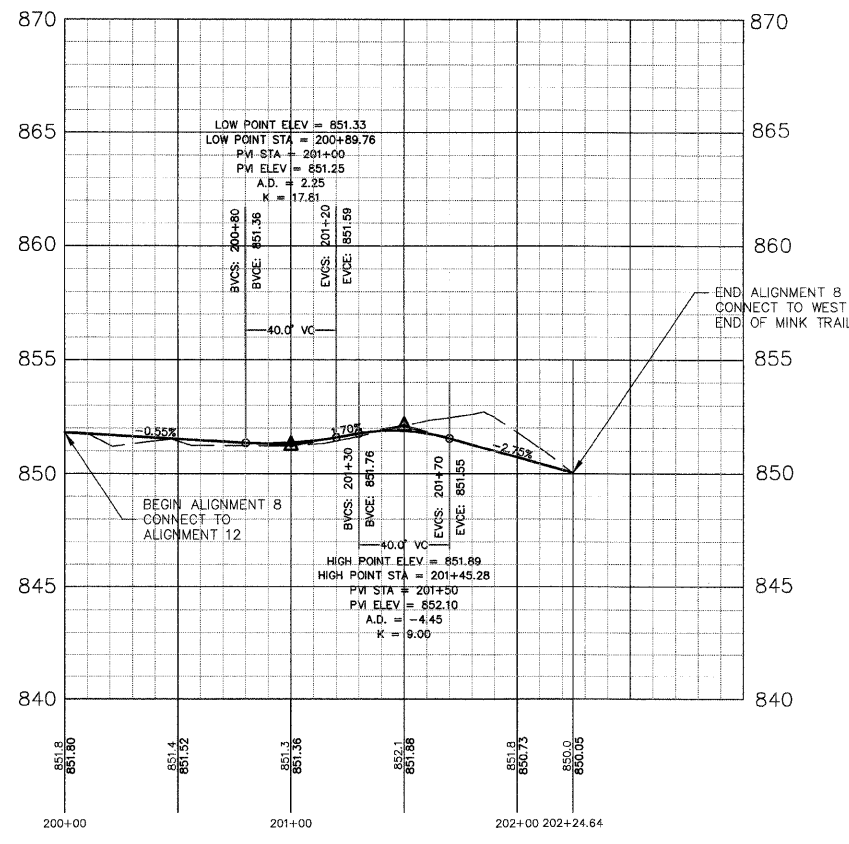


PLAN LEGEND:

- XXXXXX PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE

CARY COMMUNITY TRAIL ALIGNMENT 8 - STA. 200+00 TO STA. 202+24.64

CARY COMMUNITY TRAIL ALIGNMENT 14 - STA. 300+00 TO STA. 300+82.50



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEDER ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO." IT IS UNKNOWN TO HAEDER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT DUE TO THE FACT HAEDER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAEDER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON NAD83 COMPANYS RTK GPS REFERENCE NETWORK USING NAVD 86 (GEOID 03) AS ITS REFERENCE DATUM. HAEDER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 86 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT CONVERSION TO NVD 28 ELEVATION FOR MH 46 USING NAD 83 (GEOID 03) IT IS UNKNOWN TO HAEDER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

BASED ON HAEDER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 86 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NVD 28-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK:
 DESCRIPTION: MANHOLE #46 FROM THE ASSULT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS

HE CP #1
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 25' NORTHWEST OF POWER POLE.
 ELEVATION: 854.84

HE CP #2
 DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 592' WEST OF THE INTERSECTION OF RANSON BRIDGE ROAD AND MINK TRAIL, 40' SOUTHWEST OF TREE 27 SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

HE CP #3
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RANSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 16' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23



This document, together with the concepts and design presented herein, is intended only for the specific purpose and site for which it was prepared. Reuse of this document for any other project without the express written consent of the engineer is prohibited. The engineer shall not be responsible for any errors or omissions in this document.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

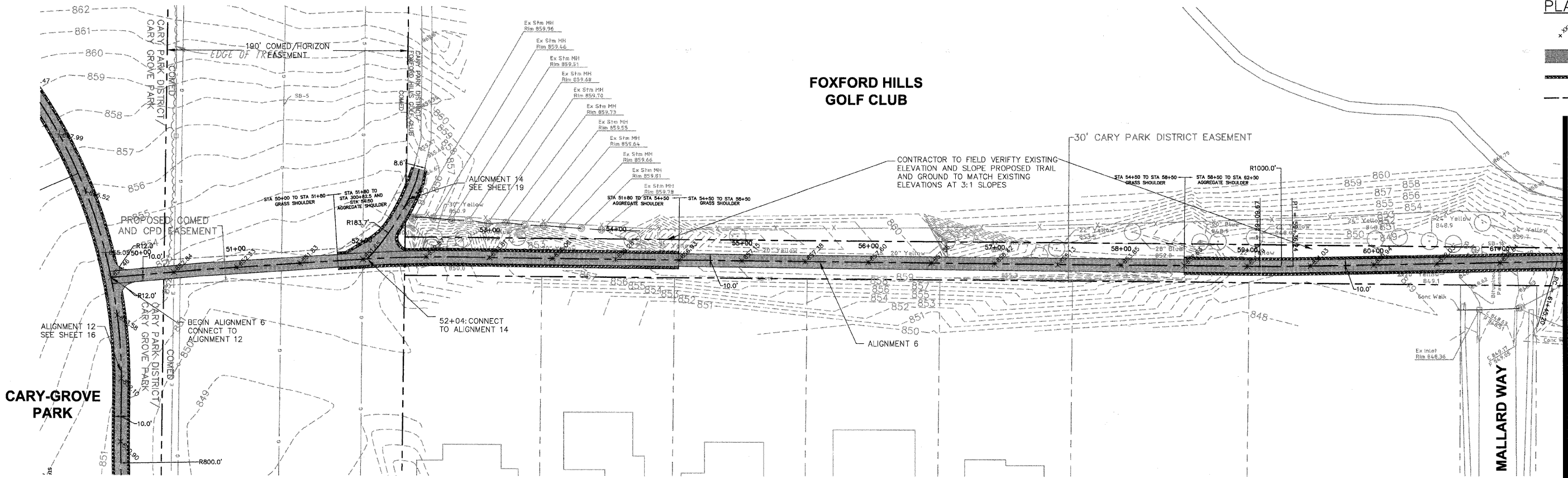
TRAIL ALIGNMENT
PLAN & PROFILE
STA. 200+00 TO STA. 202+24.64
STA. 300+00 TO STA. 300+82.50

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	19
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

PLAN LEGEND:

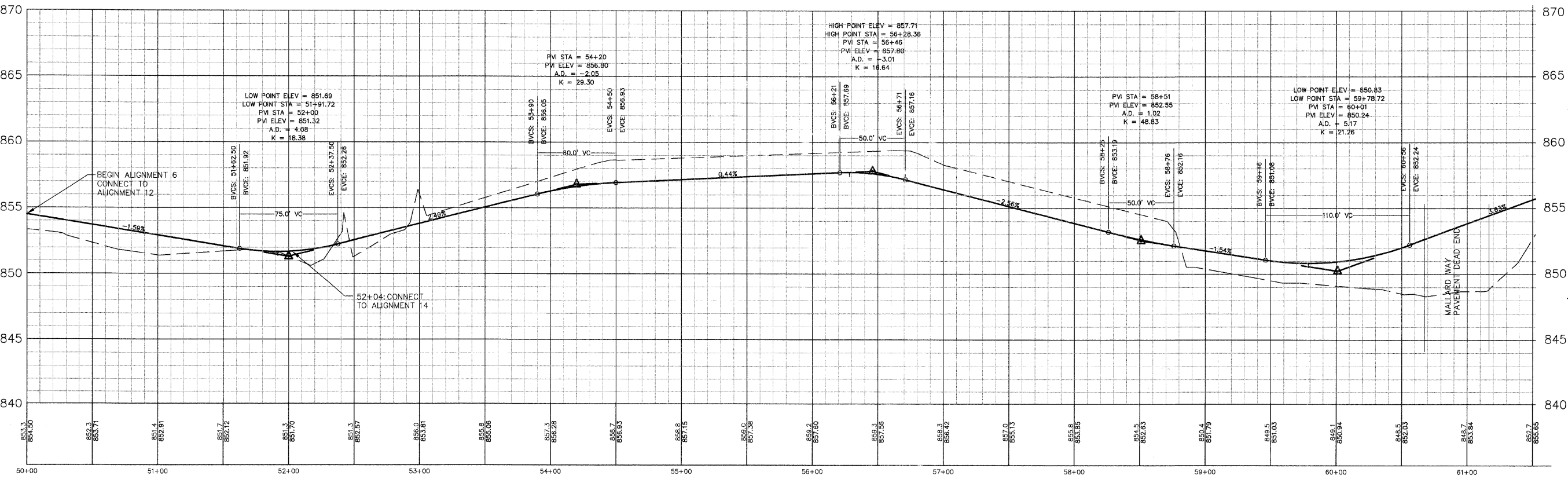
- PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE

FOXFORD HILLS GOLF CLUB



MATCHLINE - SEE SHEET 21

CARY COMMUNITY TRAIL ALIGNMENT 6 - STA. 50+00 TO 61+50



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC, DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MALLARD CONSULTING, LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK WITH MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KAMA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11 HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASUREMENT REPORT THE CONVERSION TO NAVD 28 ELEVATION FOR MH 46 USING NAVS VERTICAL CONVERSION WOULD BE 861.82 (0.07 HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05) IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR THE TOPO WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR THE TOPO IS AS FOLLOWS:
 NAVD 83 (GEOID 03)-LOWER TOPO 0.11" (WITH AN ACCURACY OF PLUS OR MINUS 0.05)
 NAVD 28-RAISE TOPO 0.07" (WITH AN ACCURACY OF PLUS OR MINUS 0.05)

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANNING #48 FROM THE ASSULT RECORD DRAWINGS BY MALLARD CONSULTING LTD DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS
 HE CP #10
 DESCRIPTION: IRON PIPE.
 LOCATION: 100' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF 3" IRON MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

HE CP #1
 DESCRIPTION: "2" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 522' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND WINK TRAIL, 45' SOUTHWEST OF TREE 27 SOUTHWEST OF BRON ROAD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.57

HE CP #8
 DESCRIPTION: RAIL ROAD SPIKE.
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23



1-800-892-0123

DESIGNED	JWH
DRAWN	LES
CHECKED	JMM
DATE	10/02/2009

REVISED	
REVISED	
REVISED	
REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

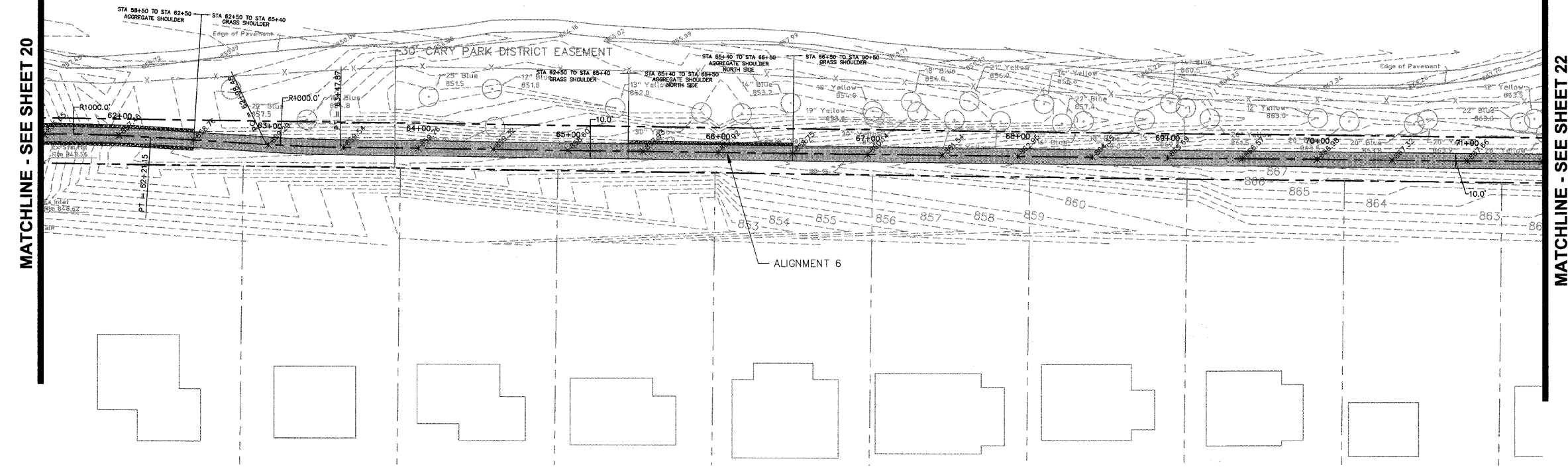
**TRAIL ALIGNMENT
PLAN & PROFILE
STA. 50+00 TO STA. 61+50**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	20
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

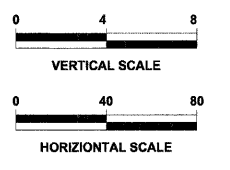
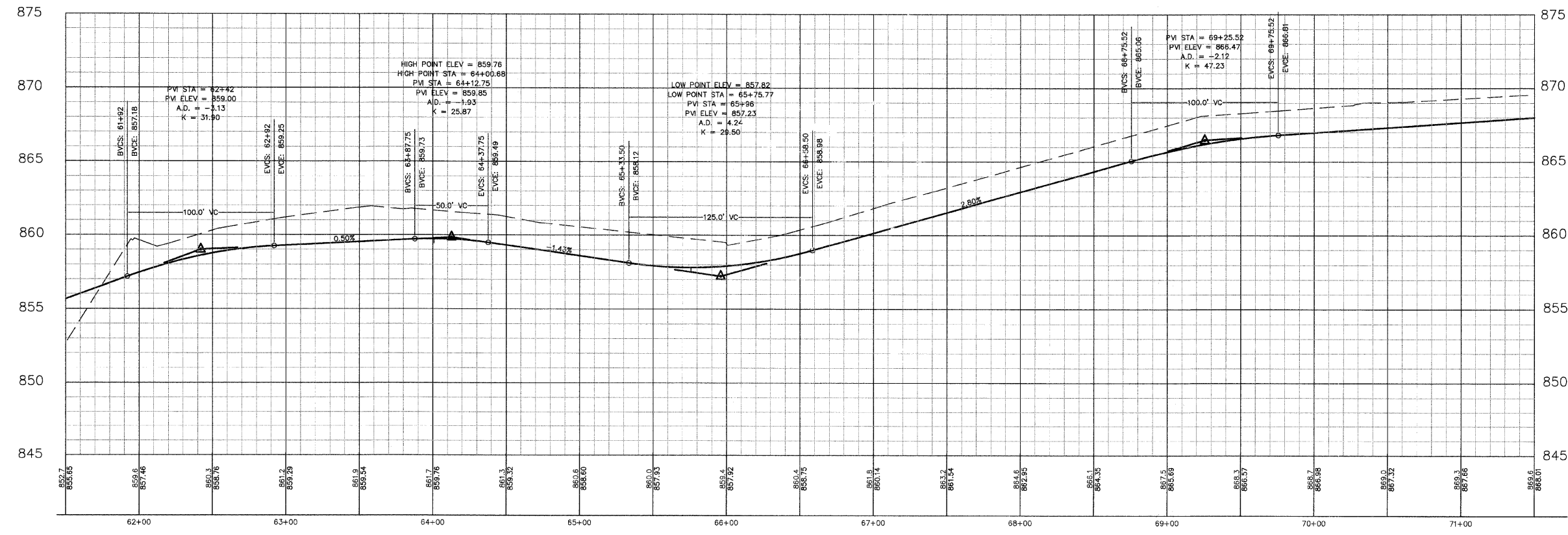
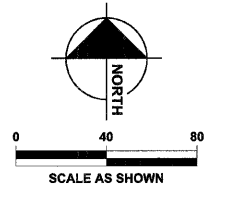
PLAN LEGEND:

	PROPOSED SPOT ELEVATIONS
	PROPOSED BIKE TRAIL
	PROPOSED AGGREGATE SHOULDER
	PROPERTY LINE

FOXFORD HILLS GOLF CLUB



CARY COMMUNITY TRAIL ALIGNMENT 6 - STA. 61+50 TO 71+50



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAZZER ENGINEERING, LLC, DATED FEB. 2, 2008 (THE TOPO) WAS BASED ON MH 48 ON PAGE 25 OF 38 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 48 WAS 861.45 FEET. THE ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAZZER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT, DUE TO THE FACT HAZZER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAZZER ENGINEERING, LLC DID CHECK INTO MH 48 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARA COMPANY'S RTK CORS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAZZER ENGINEERING, LLC FOUND MH 48 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 48 USING NAVD VERTCON CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 48 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAZZER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON.

BASED ON HAZZER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 48 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 83 (GEOID 03) - LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29 - RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD. DATED 3-1-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS

HE CP #1
 DESCRIPTION: IRON PIPE
 LOCATION: 100' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 264' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

HE CP #2
 DESCRIPTION: "PIV" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 582' WEST OF THE INTERSECTION OF RAMSON BRIDGE ROAD AND MAIN TRAIL, 45' SOUTHWEST OF TREE 57 SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

HE CP #3
 DESCRIPTION: RAIL ROAD SPIKE.
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAMSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23

This document, together with the concept and design presented herein, is intended solely for the specific purpose and client for which it was prepared. Reuse or modification of this document for any other project without the written consent of HAZZER ENGINEERING, LLC is strictly prohibited. HAZZER ENGINEERING, LLC shall not be responsible for any errors or omissions in this document.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

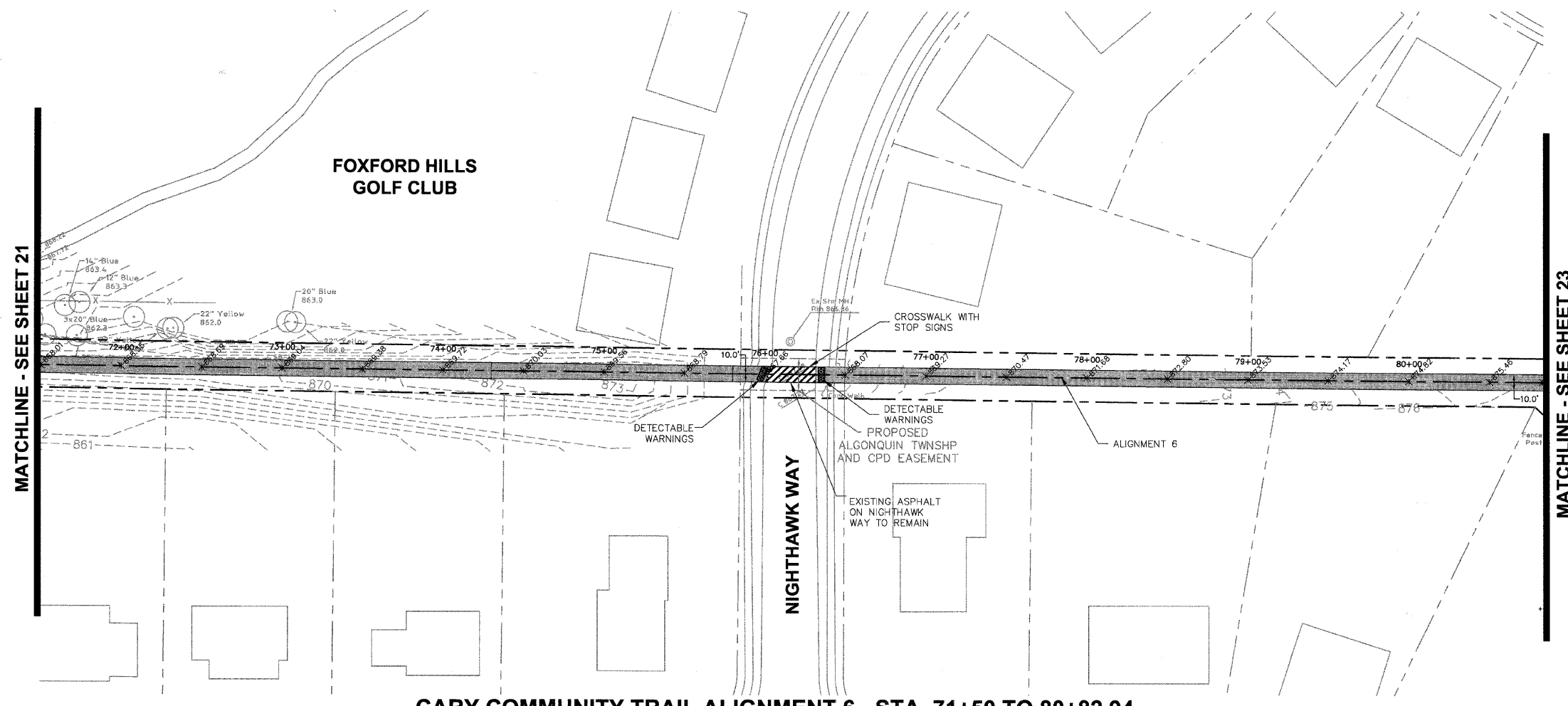
**TRAIL ALIGNMENT
PLAN & PROFILE**
STA. 61+50 TO STA. 71+50

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	21
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

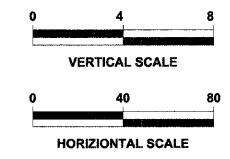
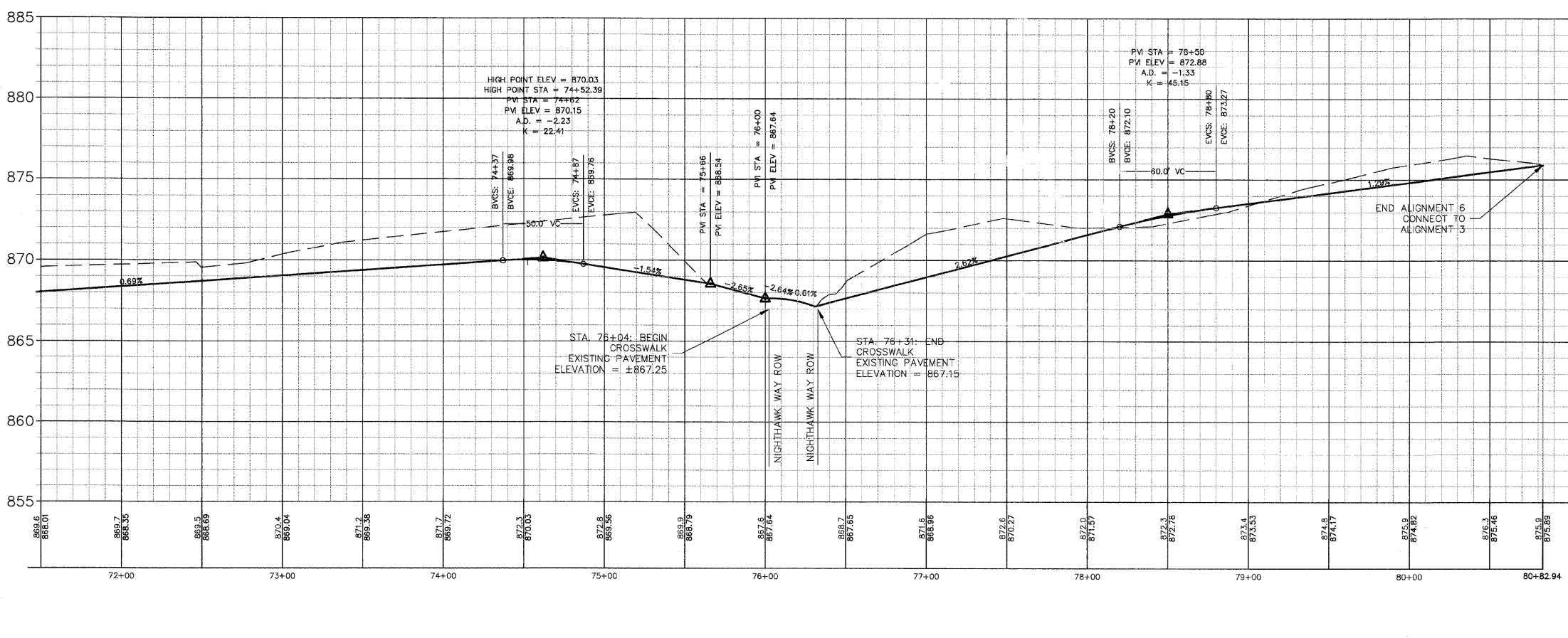
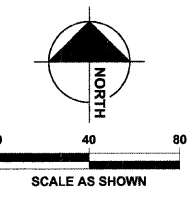


PLAN LEGEND:

- PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE



CARY COMMUNITY TRAIL ALIGNMENT 6 - STA. 71+50 TO 80+82.94



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAZGER ENGINEERING, LLC, DATED FEB. 2, 2006 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 38 ON THE HANNAH CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 801.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAZGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAZGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAZGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KANSA COMPANYS RTK CORS REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAZGER ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 23 ELEVATION FOR MH 46 USING NAD 83 VERTICAL CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.02') IT IS UNKNOWN TO HAZGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR THE TOPO WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAZGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 88 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 23-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

- BENCHMARKS**
- SOURCE BENCHMARK**
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY HANNAH CONSULTING LTD DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45
- SITE BENCHMARKS**
- HC CP #00
 DESCRIPTION: IRON PIPE
 LOCATION: 100' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 564' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 20' NORTHEAST OF POWER POLE.
 ELEVATION: 854.94
- HC CP #1
 DESCRIPTION: "P" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 582' WEST OF THE INTERSECTION OF RAMSON BRIDGE ROAD AND MINN TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 856.87
- HC CP #6
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAMSON BRIDGE ROAD, 22' SOUTHEAST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23

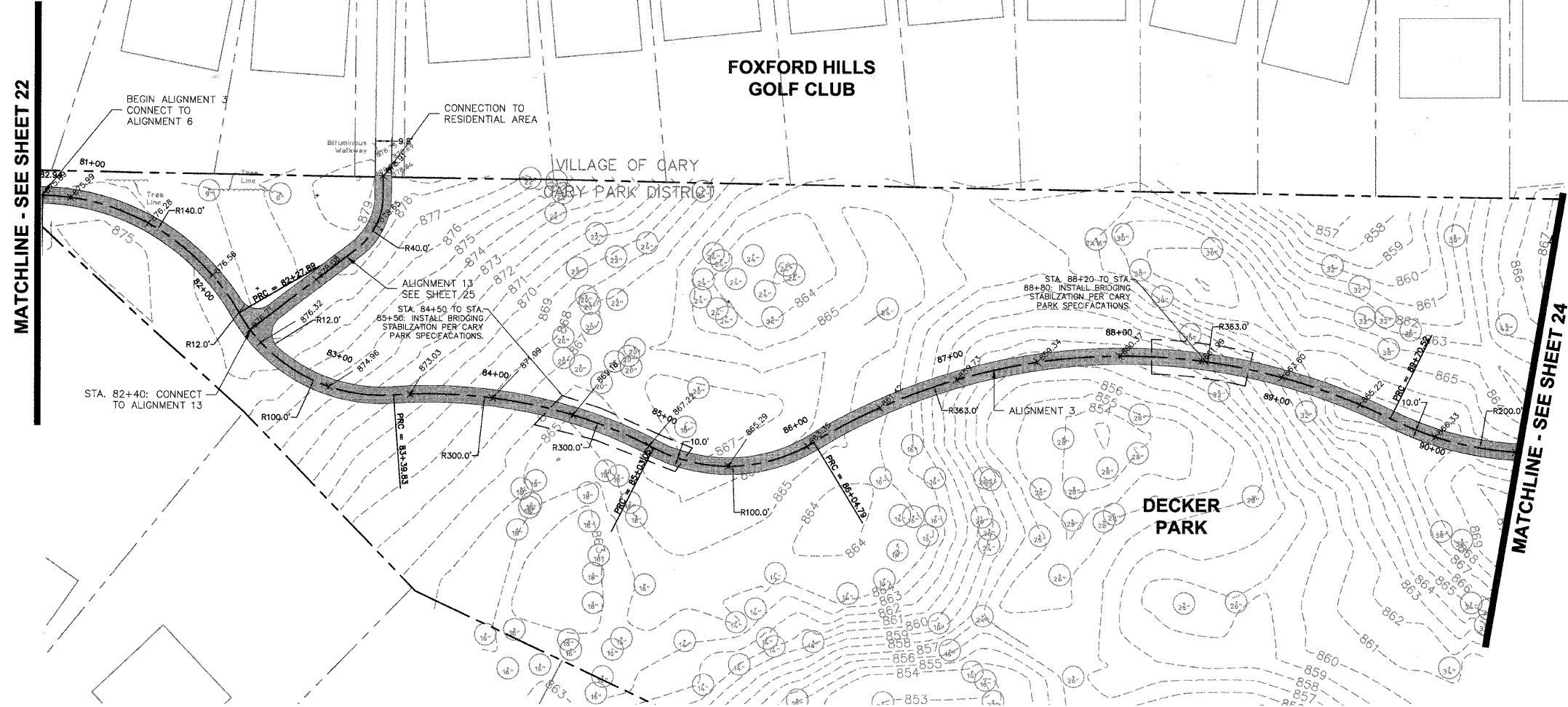


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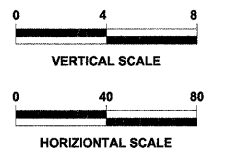
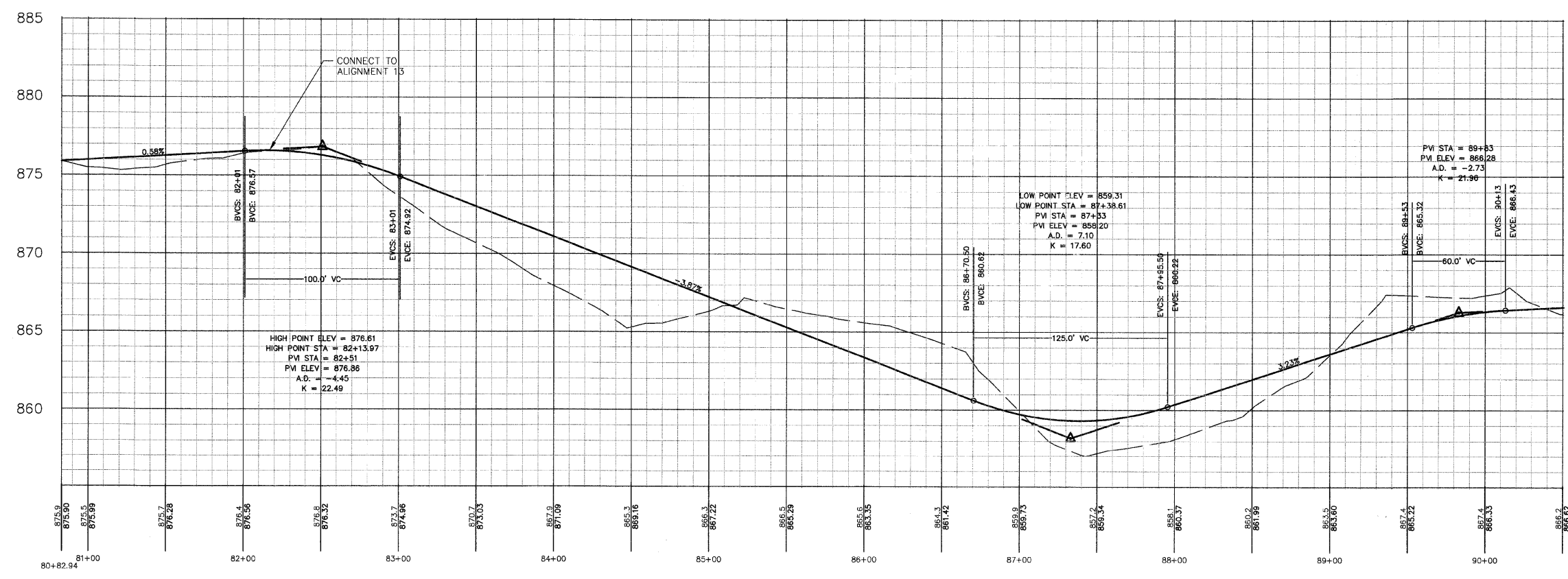
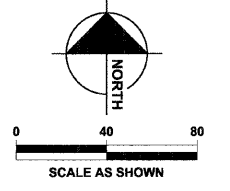
DESIGNED	JWH	REVISED		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CARY PARK DISTRICT CARY COMMUNITY TRAIL	TRAIL ALIGNMENT PLAN & PROFILE	
DRAWN	LES	REVISED		STA. 71+50 TO STA. 80+82.94			
CHECKED	JMM	REVISED		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DATE	10/02/2009	REVISED		06-P4001-00-BR	MCHENRY	64	22
				FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

PLAN LEGEND:

	PROPOSED SPOT ELEVATIONS
	PROPOSED BIKE TRAIL
	PROPOSED AGGREGATE SHOULDER
	PROPERTY LINE



CARY COMMUNITY TRAIL ALIGNMENT 3 - STA. 80+82.94 TO 90+50



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 9, 2008 ("THE TOPOG") WAS BASED ON MH 48 ON PAGE 25 OF 36 ON THE MANHOLE CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION PHASE 1 DATED MARCH 8, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPOG TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 48 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPOG". IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK WITH MH 48 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARMA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 48 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 83 (GEOID 03) AS ITS REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 48 USING NAVS VERTICAL CONVERSION WOULD BE 861.23 (NOT HIGHER THAN PUBLISHED MH 48 ELEVATION). THEREFORE DUE TO THE ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPOG" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 48 THE DATUM CONVERSION FOR "THE TOPOG" IS AS FOLLOWS:

NAVD 83 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')
 NAVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #48 FROM THE ASBUILT RECORD DRAWINGS BY MANHOLE CONSULTING LTD. DATED 3-8-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY.
 LOCATION: 120' NORTHEAST OF THE OGDEN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OGDEN DRIVE.
 ELEVATION: 861.45

SITE BENCHMARKS
 BE CP #100
 DESCRIPTION: IRON PIPE.
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE CREEK ROAD, 564' NORTH OF THE NORTH SIDE OF THREE CREEK ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

BE CP #8
 DESCRIPTION: "X" NAIL IN EDGE OF PAVEMENT.
 LOCATION: 592' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MIKE TRAIL, 45' SOUTHWEST OF TRIS, 22' SOUTHWEST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 865.97

BE CP #8
 DESCRIPTION: RAIL ROAD SPIKE.
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23



This document, together with the concrete and design presented herein, is intended only for the specific purpose and project for which it was prepared. Please refer to the contract documents for a complete and accurate description of the project. No responsibility is assumed by the engineer for any use of this document for any other purpose.

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

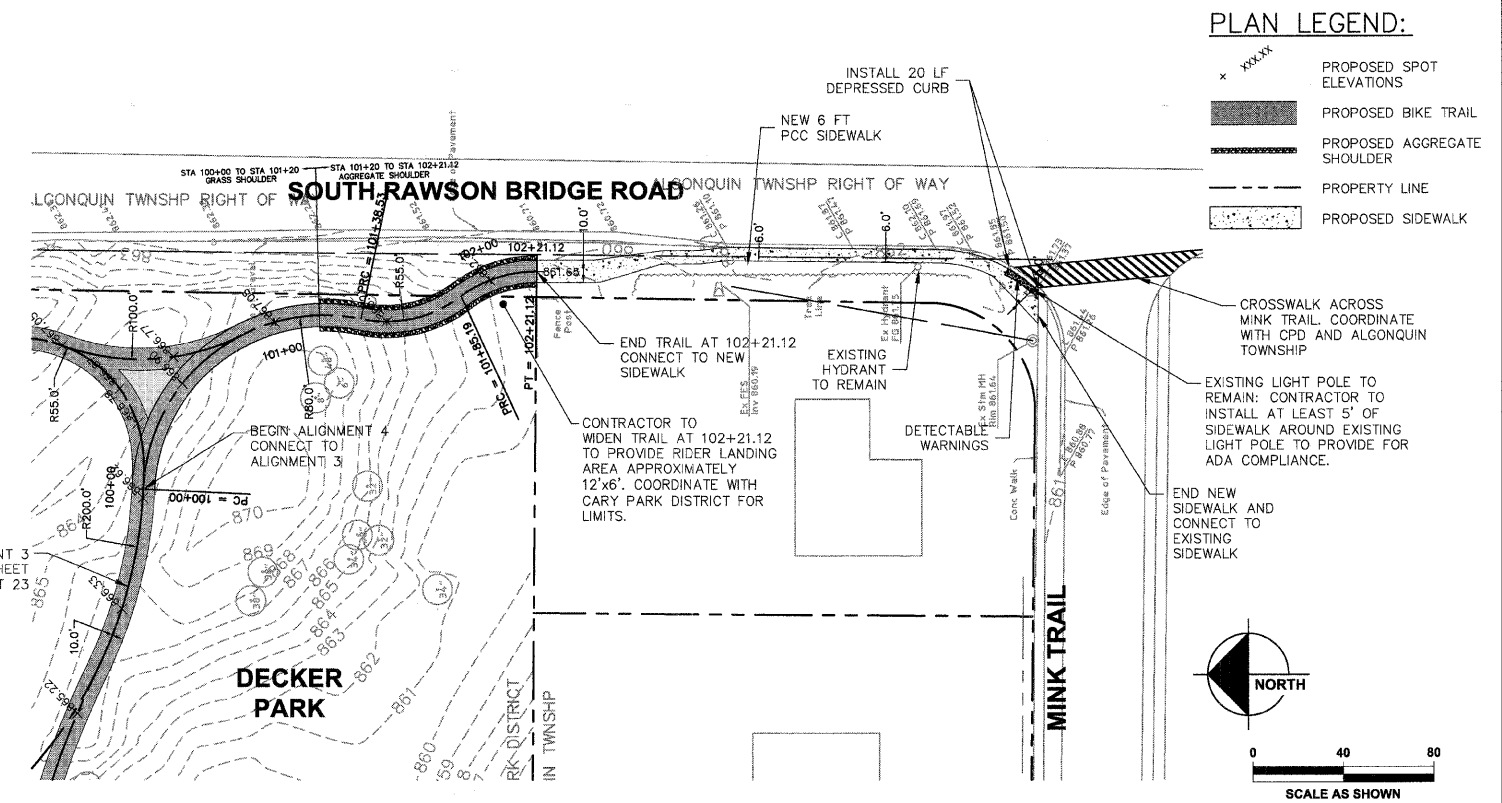
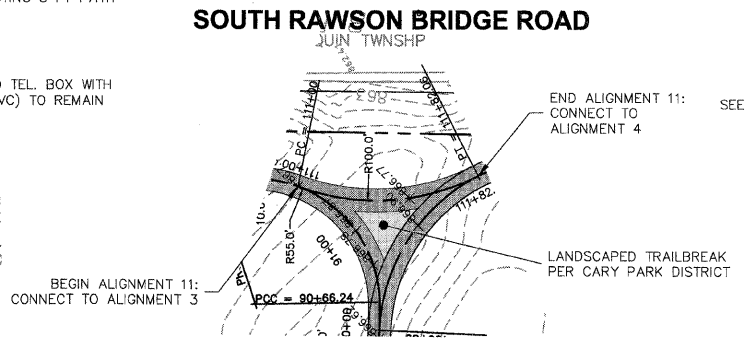
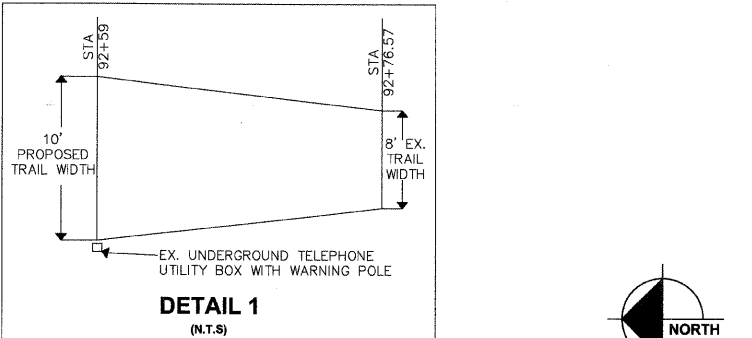
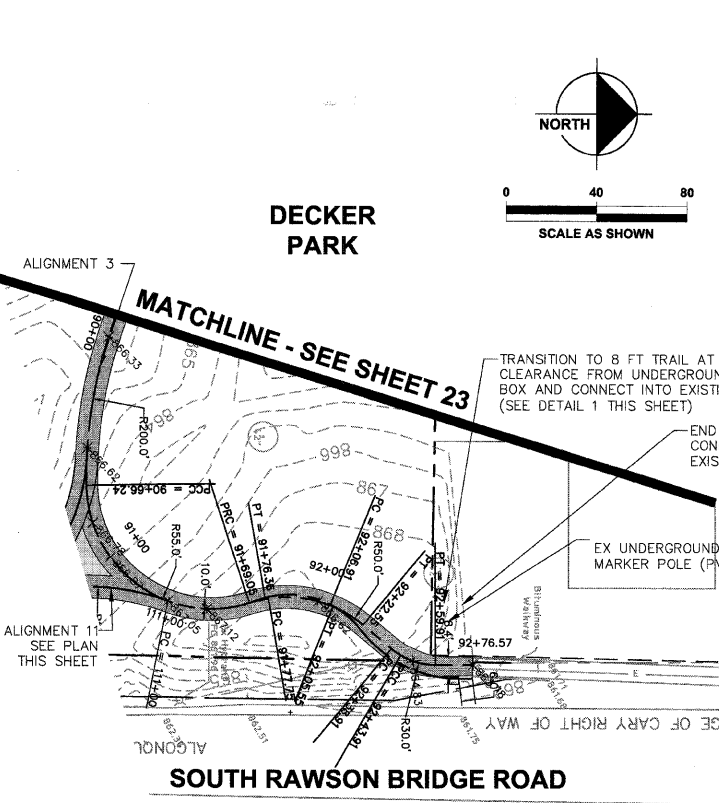
**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**TRAIL ALIGNMENT
PLAN & PROFILE
STA. 80+82.94 TO STA. 90+50**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	23
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

PLAN LEGEND:

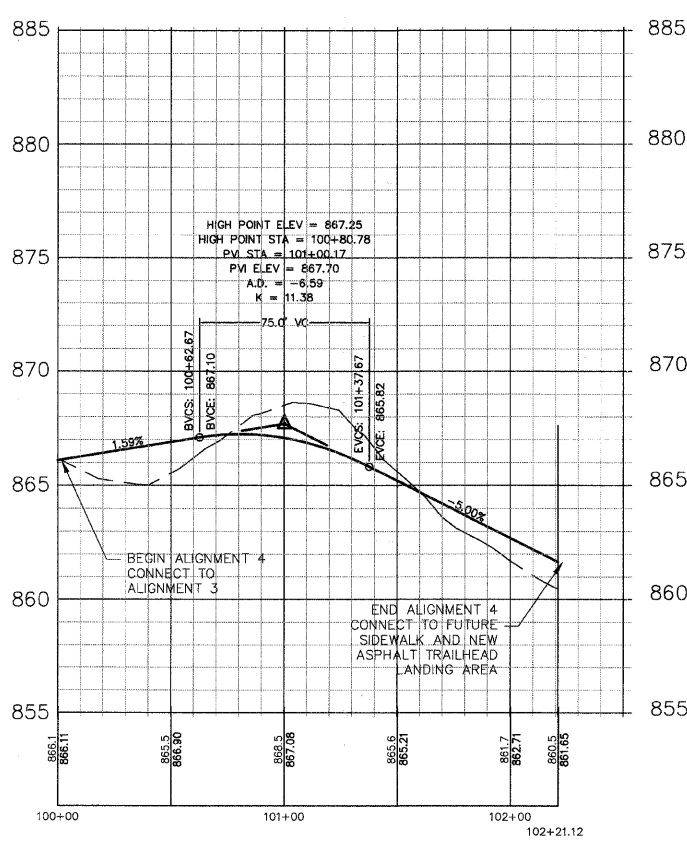
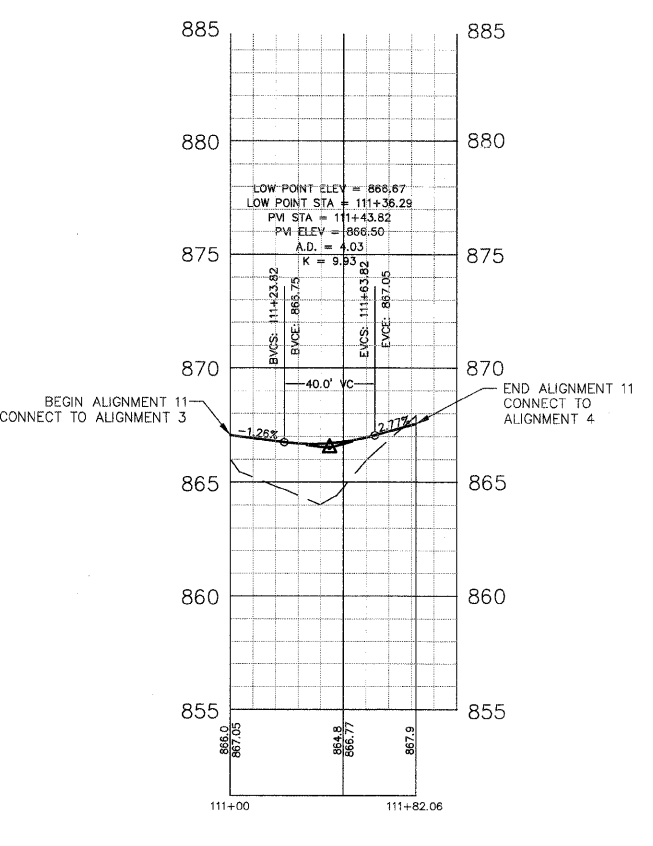
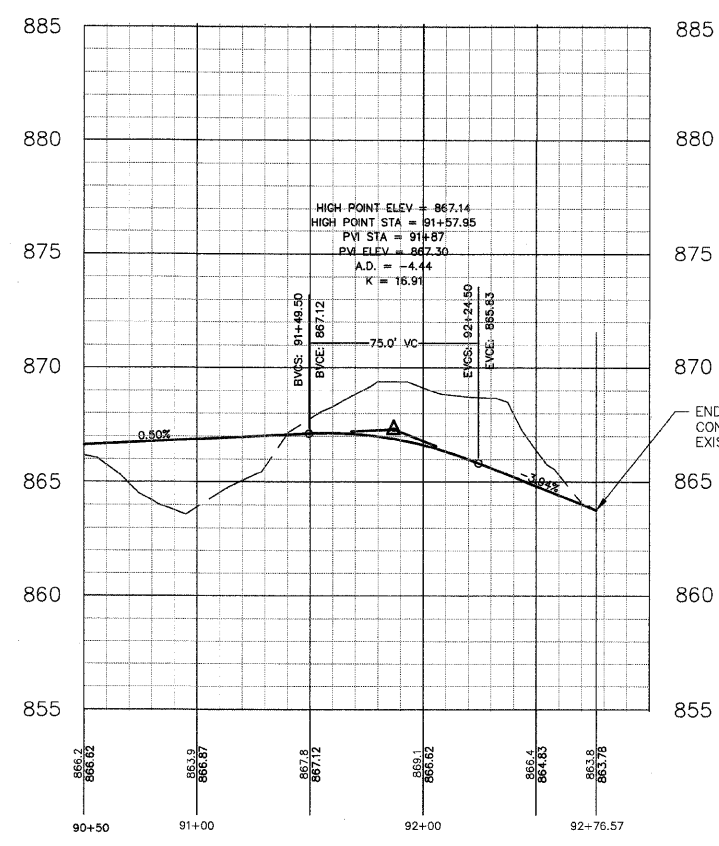
- PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE
- PROPOSED SIDEWALK



**CARY COMMUNITY TRAIL ALIGNMENT 3 CONTINUED
STA. 90+50 TO 92+76.57**

**CARY COMMUNITY TRAIL ALIGNMENT 11
STA. 111+00 TO 111+82.06**

**CARY COMMUNITY TRAIL ALIGNMENT 4
STA. 100+00 TO 102+21.12**



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAZEGR ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 28 OF 38 ON THE MANHARD CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR "THE TOPO" TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 861.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE TOPO". IT IS UNKNOWN TO HAZEGR ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAZEGR ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION HOWEVER, HAZEGR ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KAMA COMPANY'S RTK GPS REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAZEGR ENGINEERING, LLC FOUND MH 46 ELEVATION (861.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (861.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NAVD 29 ELEVATION FOR MH 46 USING NGS VERTCON CONVERSION WOULD BE 861.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (PLUS OR MINUS 0.03') IT IS UNKNOWN TO HAZEGR ENGINEERING, LLC AS TO THE TRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAZEGR ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

BENCHMARKS

SOURCE BENCHMARK
DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
LOCATION: 20' NORTHEAST OF THE GAMMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON GAMMAN DRIVE.
ELEVATION: 861.45

SITE BENCHMARKS
HE CP #10
DESCRIPTION: IRON PIPE.
LOCATION: 1000' EAST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 23' SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALL.
ELEVATION: 866.17

HE CP #8
DESCRIPTION: RAIL ROAD SPIKE.
LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF POWER POLE.
ELEVATION: 823.23

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**TRAIL ALIGNMENT
PLAN & PROFILE
STA. 90+50 TO STA. 92+76.57
STA. 111+00 TO STA. 111+82.06
STA. 100+00 TO STA. 102+21.12**

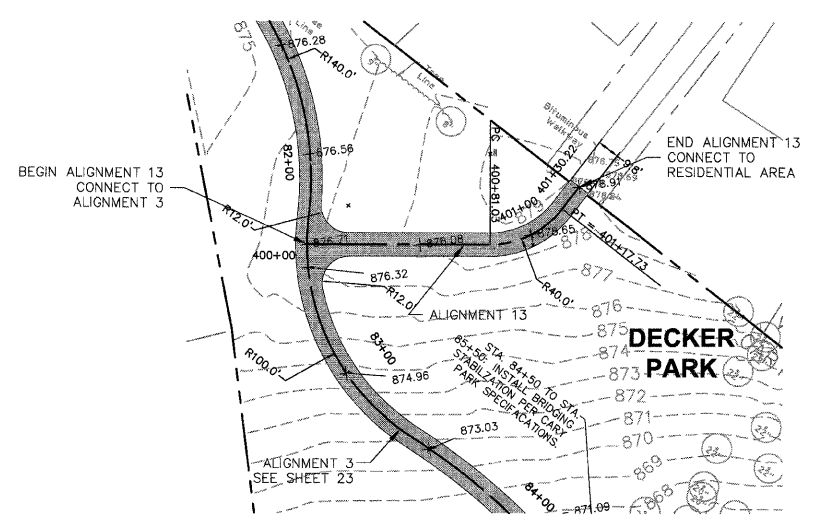
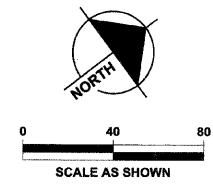
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	24
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



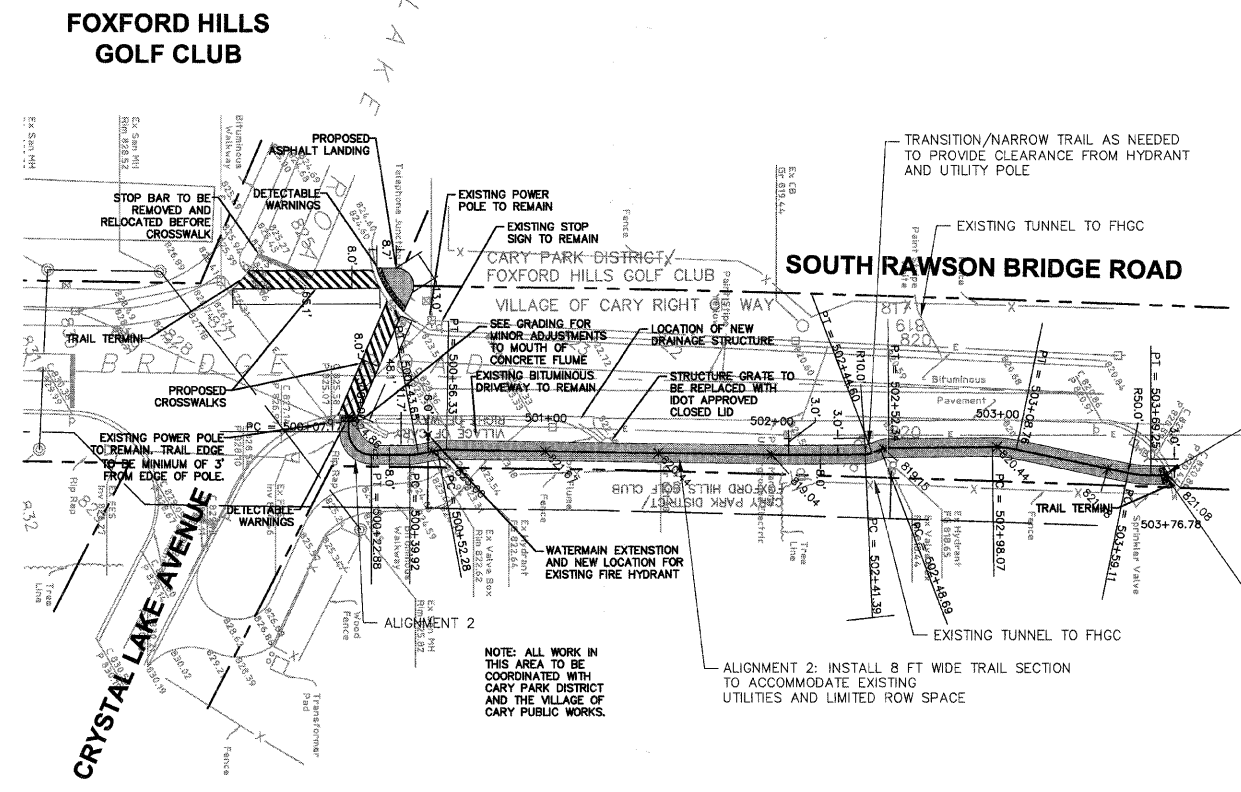
This document, together with the conceptual and preliminary plans, is for informational purposes only. It is not intended to be a contract. The contractor shall be responsible for all field measurements and for any errors or omissions. The contractor shall be responsible for all field measurements and for any errors or omissions. The contractor shall be responsible for all field measurements and for any errors or omissions.

PLAN LEGEND:

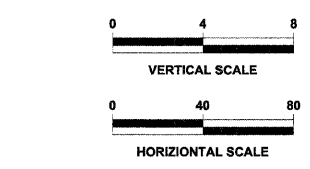
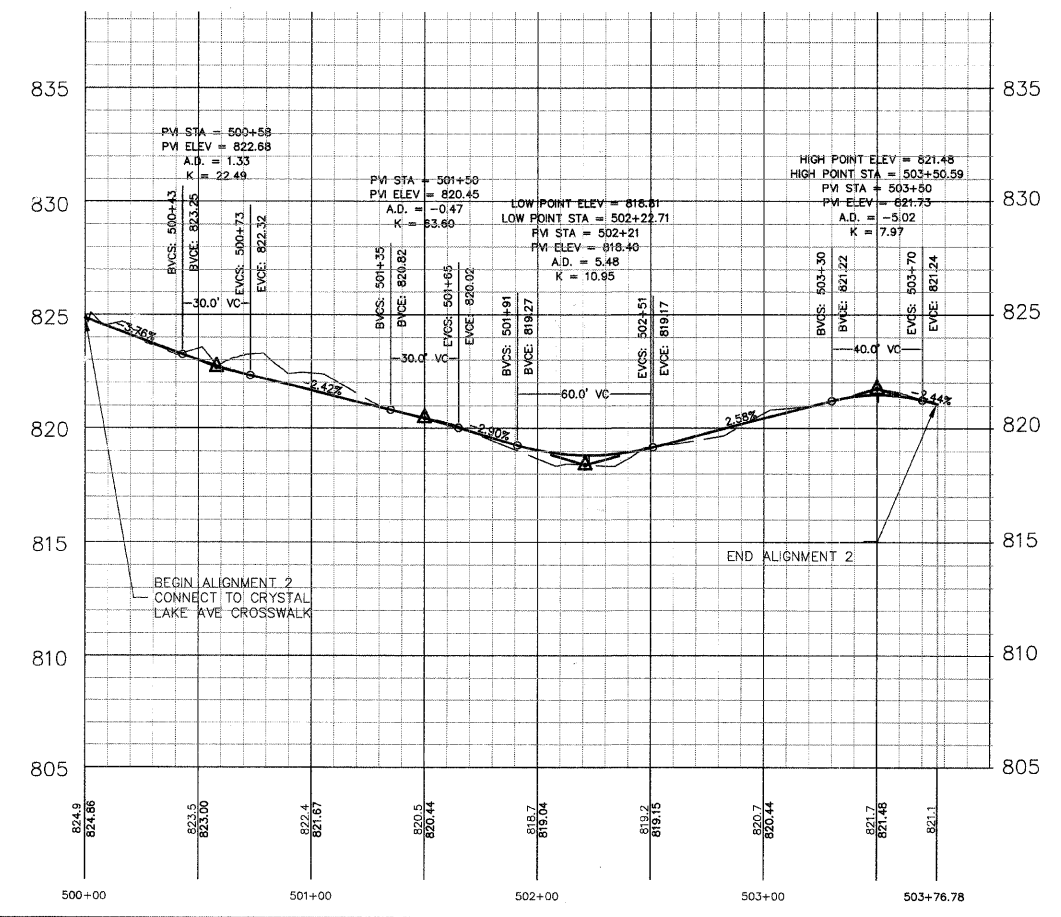
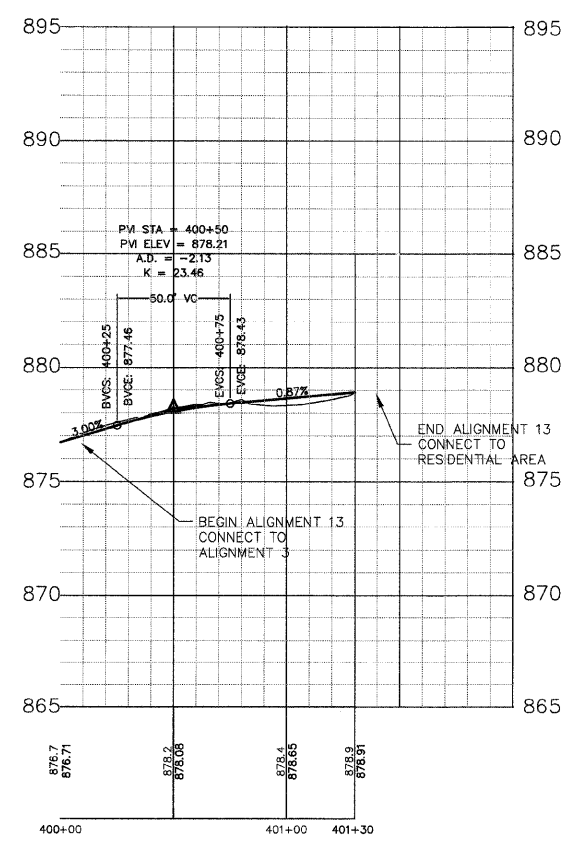
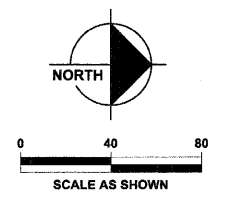
- PROPOSED SPOT ELEVATIONS
- PROPOSED BIKE TRAIL
- PROPOSED AGGREGATE SHOULDER
- PROPERTY LINE



CARY COMMUNITY TRAIL ALIGNMENT 13 - STA. 400+00 TO STA. 401+30.22



CARY COMMUNITY TRAIL ALIGNMENT 2 - STA. 500+00 TO STA. 504+20.72



DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HAEGER ENGINEERING, LLC DATED FEB. 2, 2008 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 38 ON THE MANHOLE CONSULTING LTD. RECORD DRAWINGS FOR FOXFORD HILLS SUBDIVISION - PHASE 1 DATED MARCH 6, 2007. THIS DATUM WAS REQUESTED TO BE USED IN ORDER FOR THE TOPO TO STAY CONSISTENT WITH PROJECTS DONE IN THE AREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS 881.45 FEET. THIS ELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY THE TOPO. IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWINGS OF FOXFORD HILLS SUBDIVISION PROJECT. DUE TO THE FACT HAEGER ENGINEERING, LLC WAS NEVER SUPPLIED THIS INFORMATION, HOWEVER, HAEGER ENGINEERING, LLC DID CHECK INTO MH 46 ELEVATION WITH ITS RTK GPS UNIT THAT IS BASED ON KARLA COMPANY'S RTK CORRS REFERENCE NETWORK USING NAVD 88 (GEOID 03) AS ITS REFERENCE DATUM. HAEGER ENGINEERING, LLC FOUND MH 46 ELEVATION (881.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (881.34) USING NAVD 88 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT, THE CONVERSION TO NAVD 88 ELEVATION FOR MH 46 USING NAD83 VERTCON WOULD BE 881.52 (0.07' HIGHER THAN PUBLISHED MH 46 ELEVATION). THEREFORE DUE TO THE VERTICAL ACCURACY OF THE GPS RTK MEASUREMENT (+/- 0.05' MINUS 0.05') IT IS UNKNOWN TO HAEGER ENGINEERING, LLC AS TO THE TRUE BENCHMARK OR CONTROL INFORMATION THE FOXFORD HILLS SUBDIVISION WAS BUILT ON. BASED ON HAEGER ENGINEERING, LLC GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

BENCHMARKS

SOURCE BENCHMARK
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHOLE CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 881.45

SITE BENCHMARKS
 BE CP #100
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 564' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

BE CP #1
 DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT
 LOCATION: 322' WEST OF THE INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHWEST OF IRON ROD, AND 10' SOUTH OF CONCRETE WALK.
 ELEVATION: 860.97

BE CP #8
 DESCRIPTION: RAIL ROAD SPIKE
 LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHWEST OF FENCE CORNER, 15' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
 ELEVATION: 823.23

This document, together with the documents and designs prepared herein, is submitted only for the specific purpose and use intended. It is not to be construed as a contract or as a warranty of any kind. The user of this document shall be responsible for its proper use and interpretation. The user of this document shall be responsible for its proper use and interpretation. The user of this document shall be responsible for its proper use and interpretation.

DESIGNED	JWH	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REVISED
DATE	10/02/2009	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

**TRAIL ALIGNMENT
 PLAN & PROFILE
 STA. 400+00 TO STA. 401+30.22
 STA. 500+00 TO STA. 504+20.72**

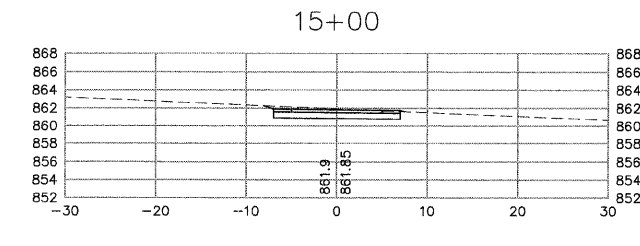
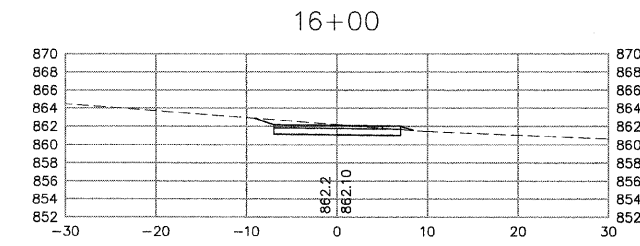
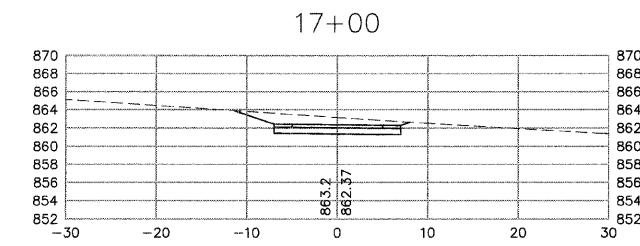
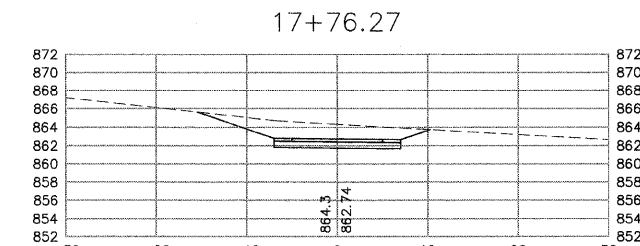
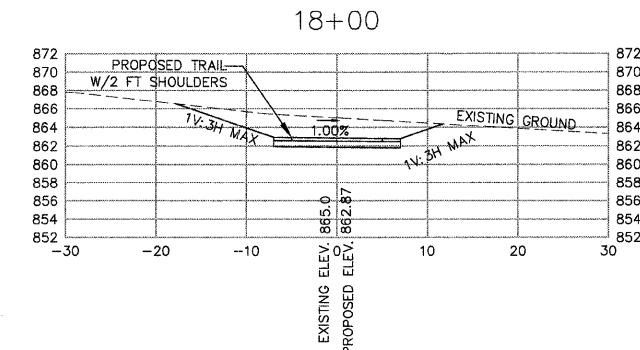
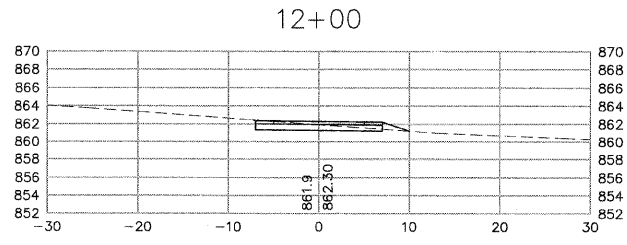
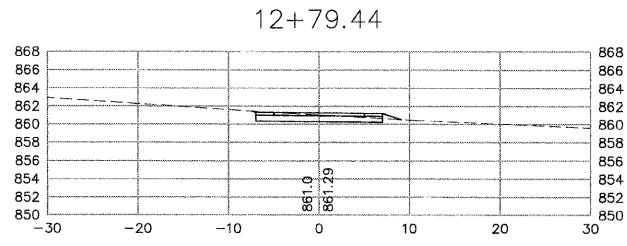
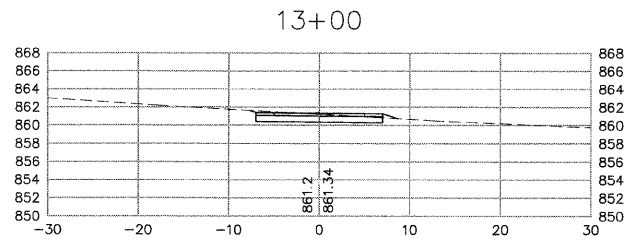
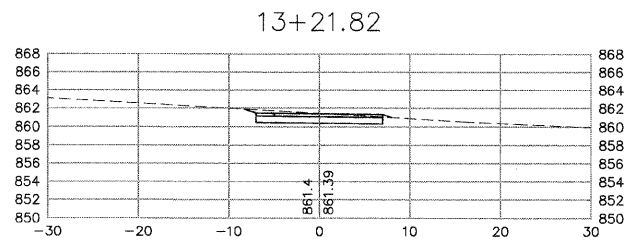
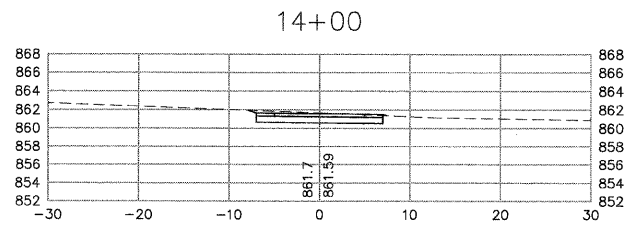
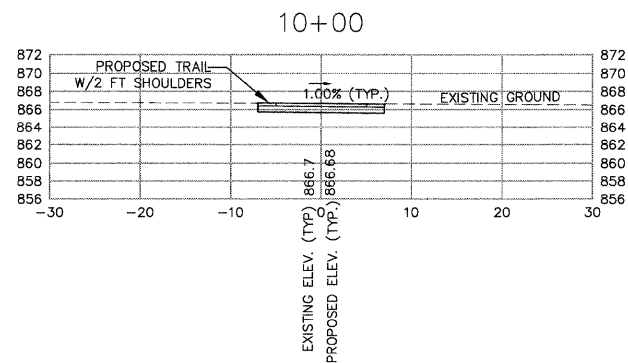
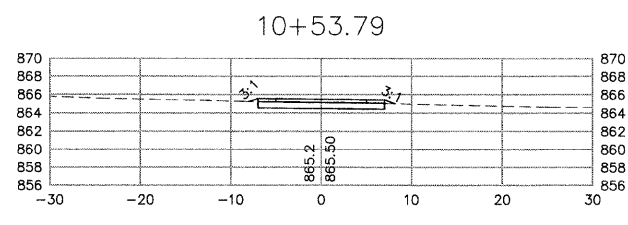
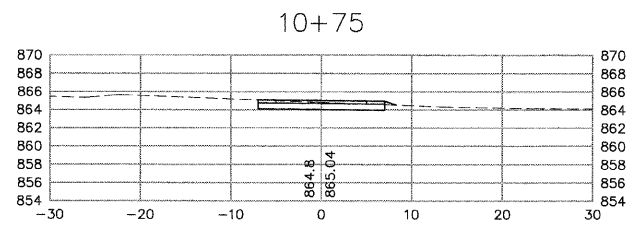
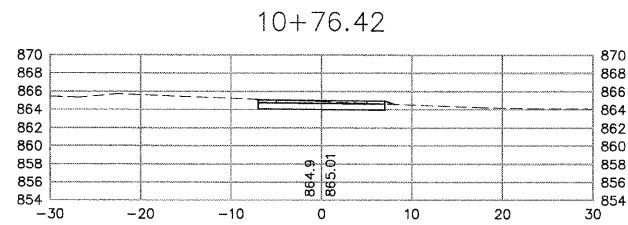
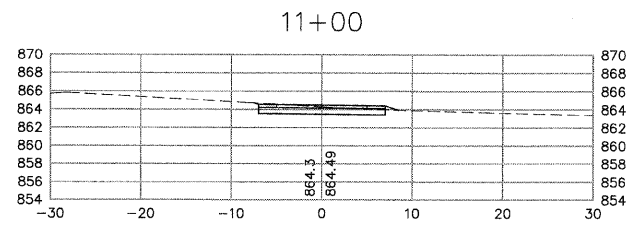
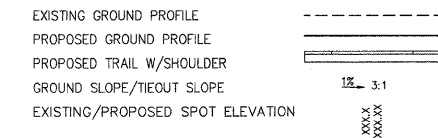
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	25
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



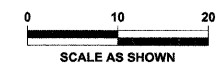
GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



This document, together with the contract and design, is to be used for the specific purpose and object for which it was prepared. It is not to be used for any other purpose without the written consent of the engineer. The engineer shall not be responsible for any errors or omissions in this document.



1-800-892-0123

DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

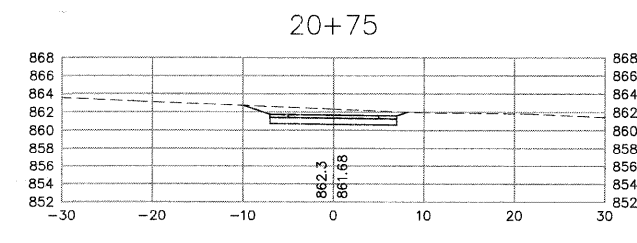
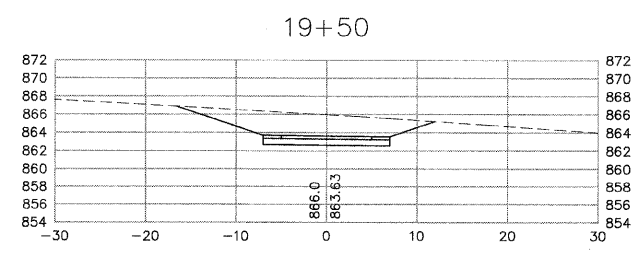
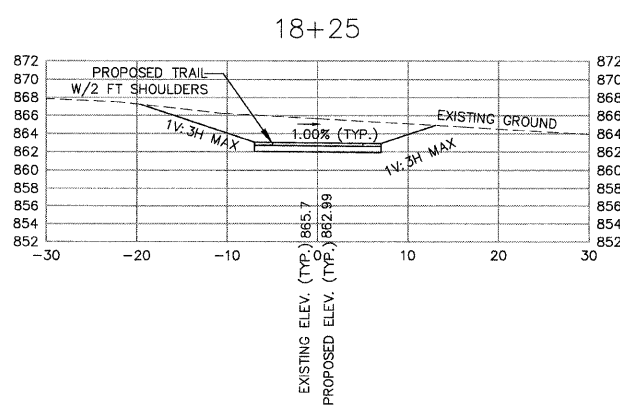
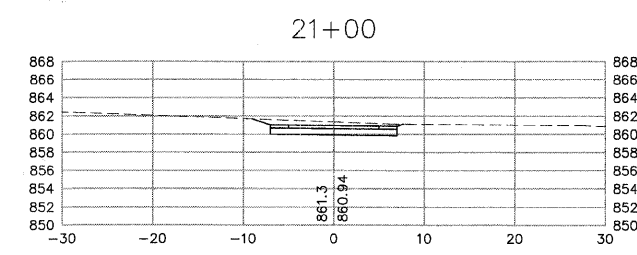
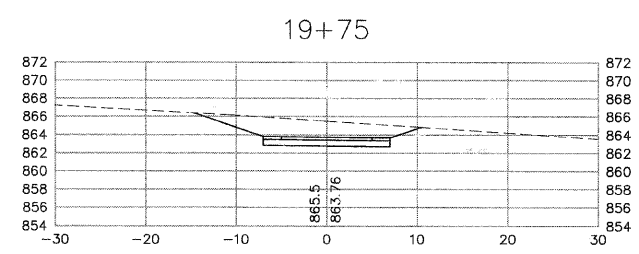
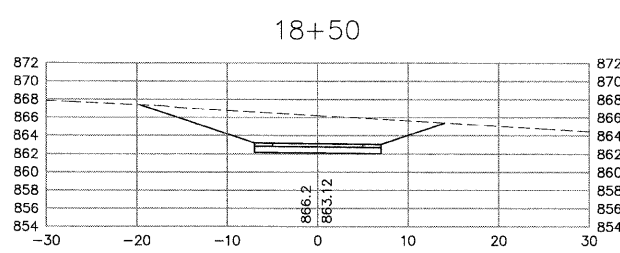
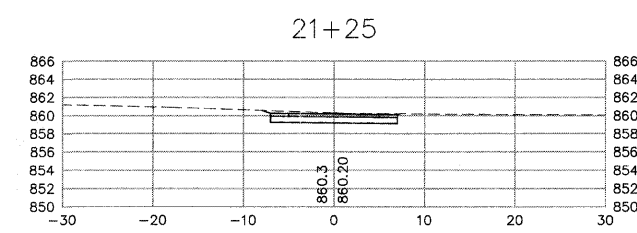
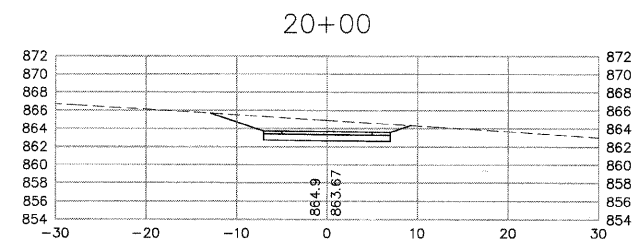
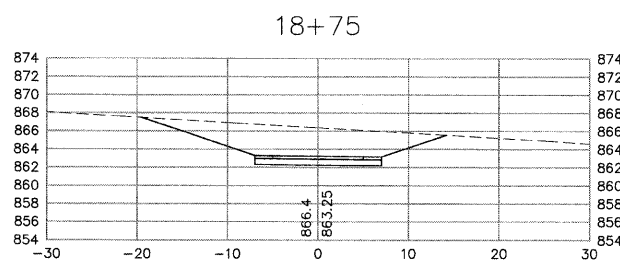
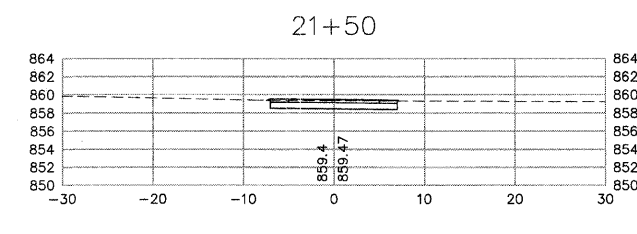
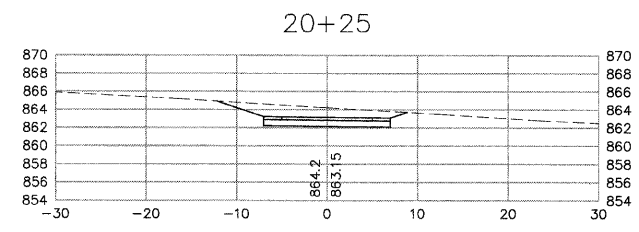
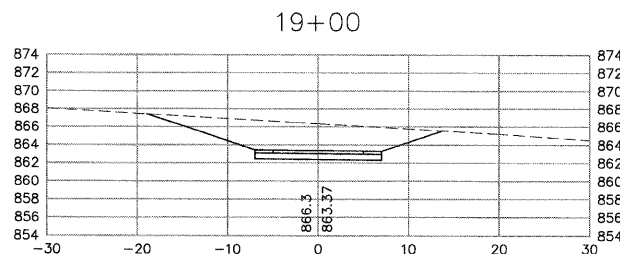
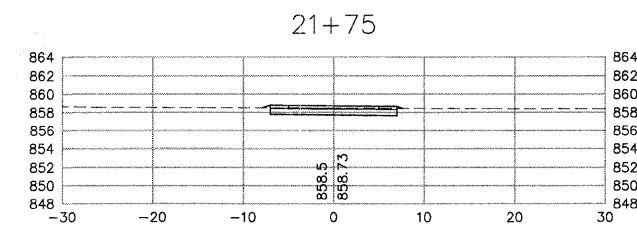
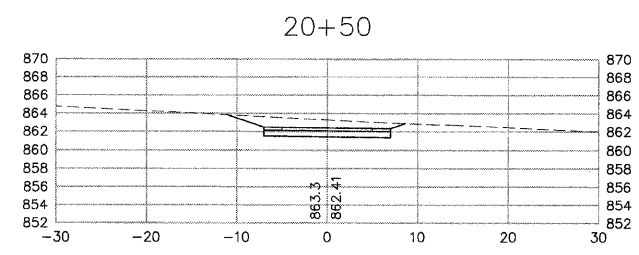
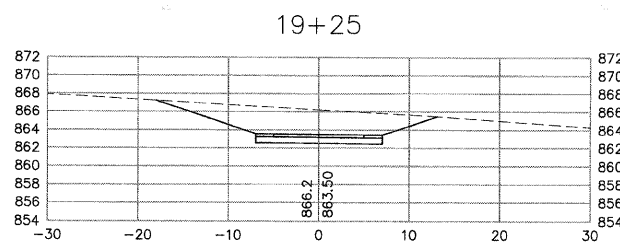
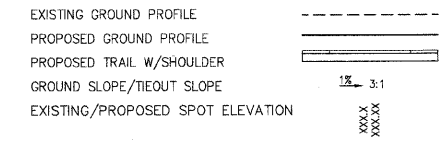
**CROSS SECTIONS
STA. 10+00 TO STA 18+00**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	26
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

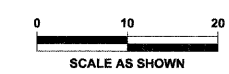
GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



This document, together with the accompanying drawings, is to be used for the construction of the project for which it was prepared. It is the responsibility of the contractor to verify the accuracy of the information provided herein. The user of this document is advised that the user assumes all liability for any and all consequences arising from the use of this document. The user of this document is advised that the user assumes all liability for any and all consequences arising from the use of this document.



DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

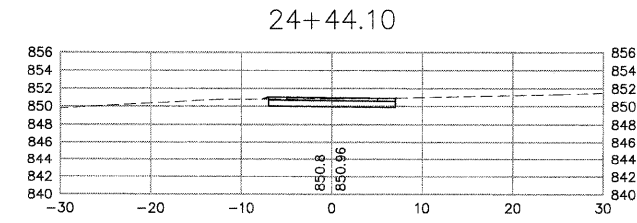
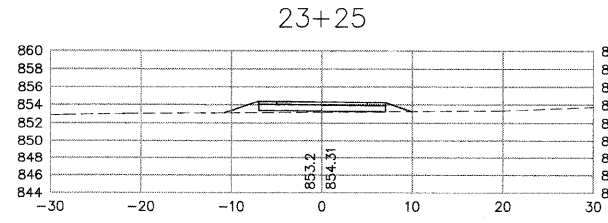
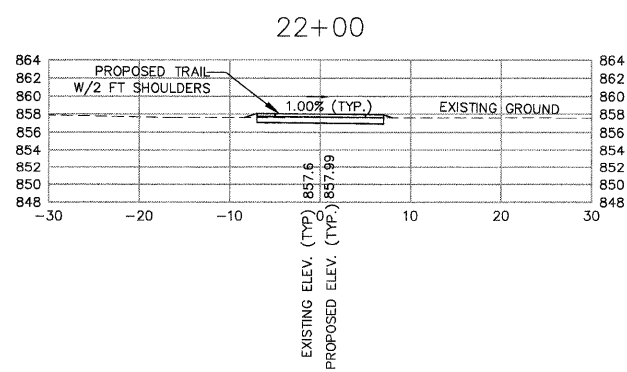
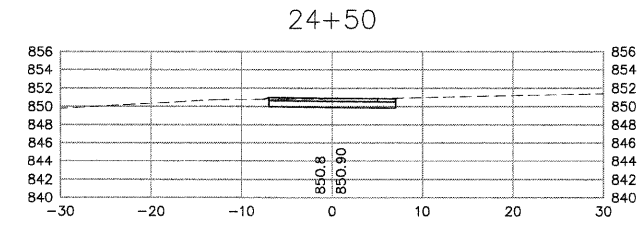
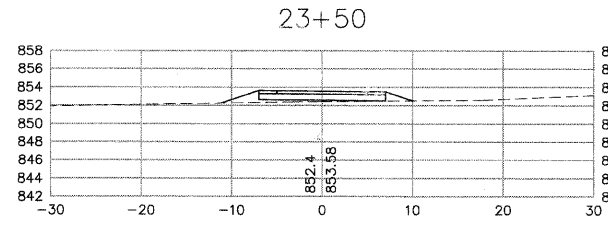
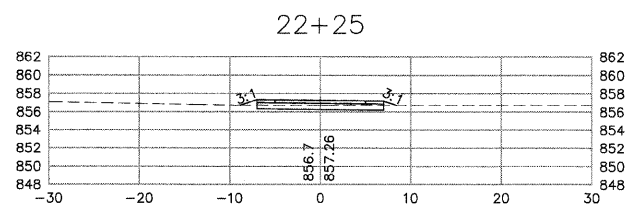
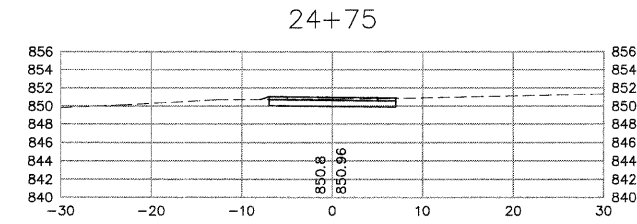
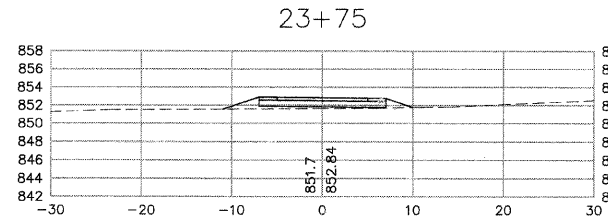
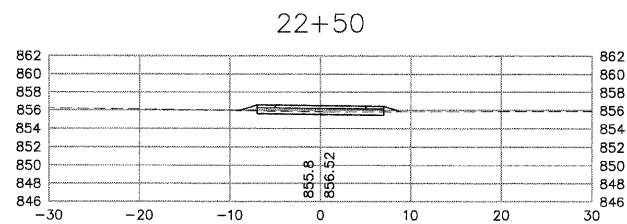
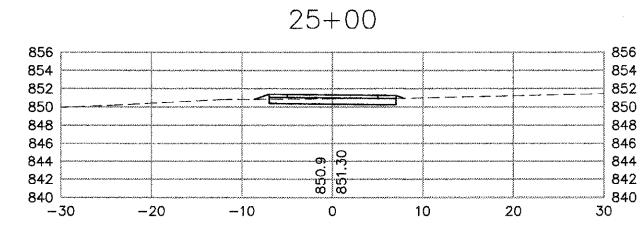
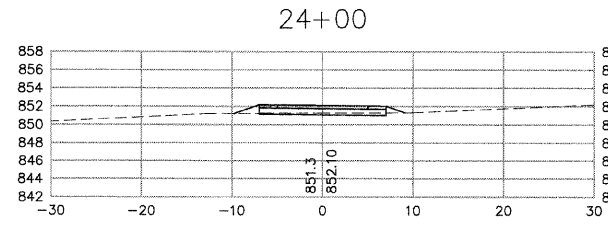
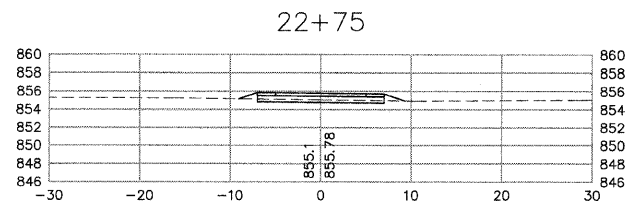
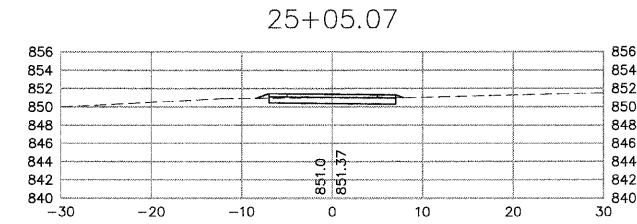
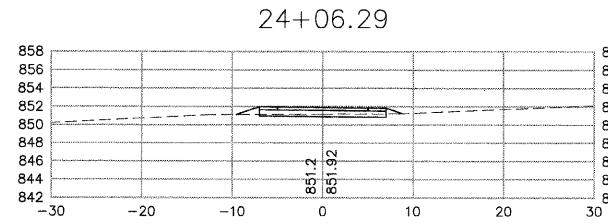
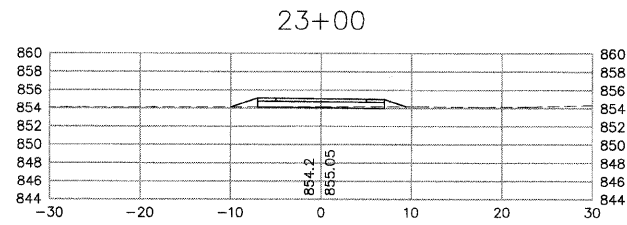
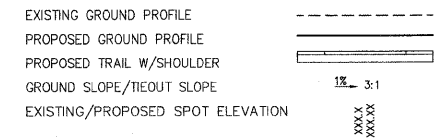
CROSS SECTIONS
STA. 18+25 TO STA 21+75

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	27
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

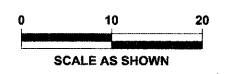
GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

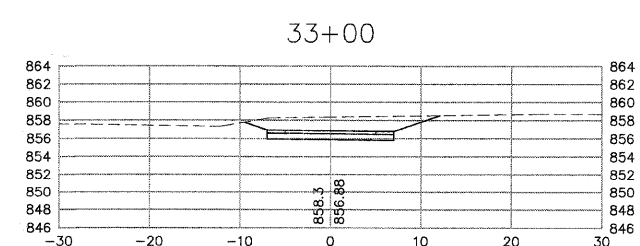
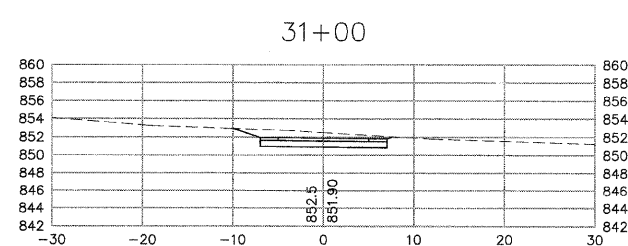
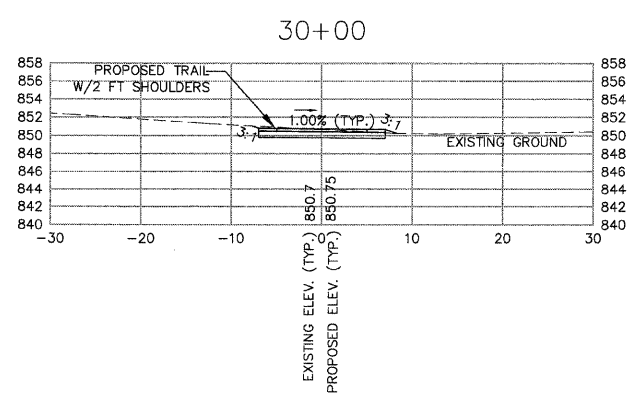
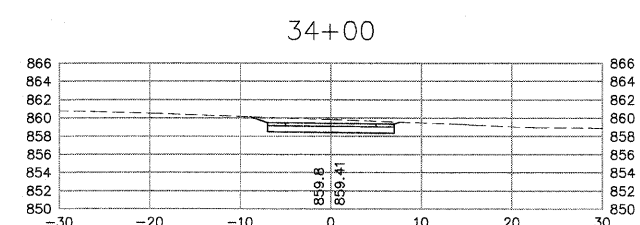
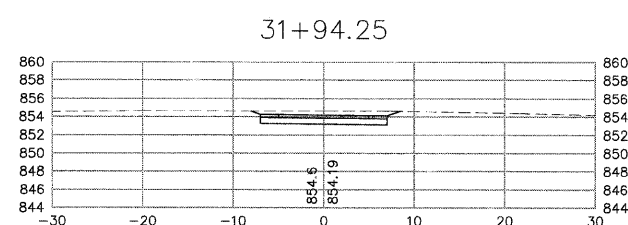
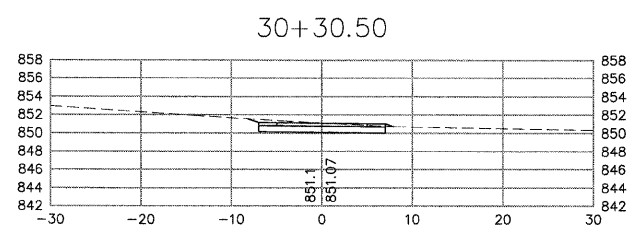
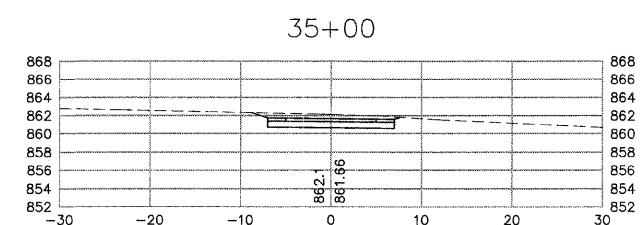
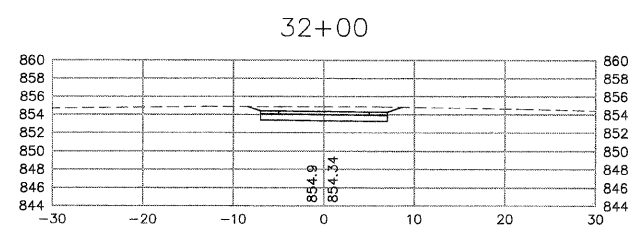
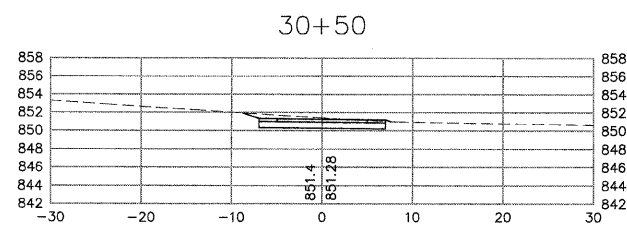
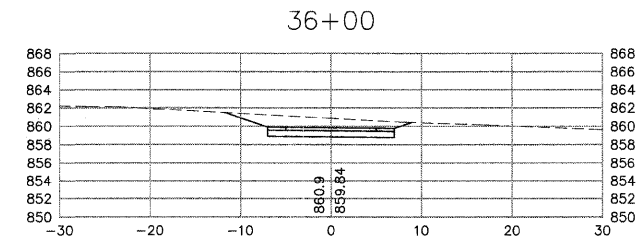
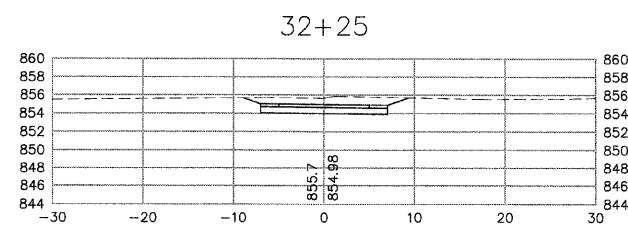
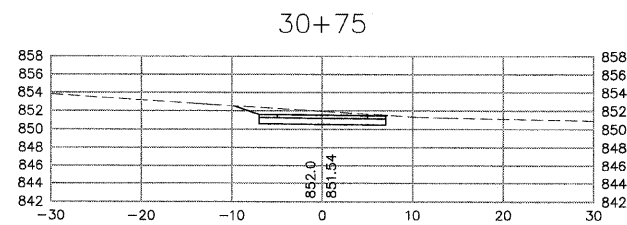
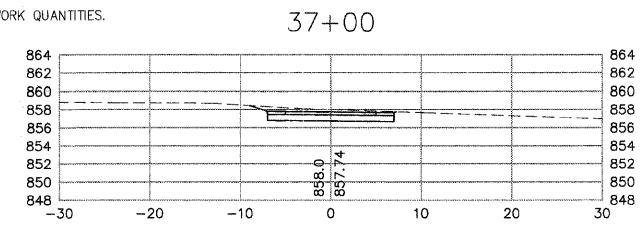
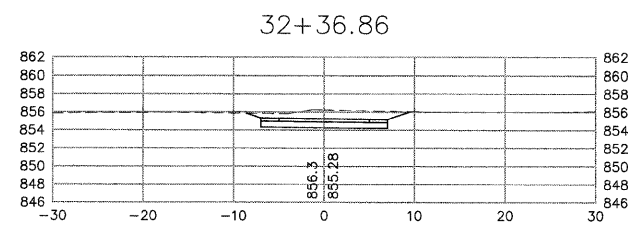
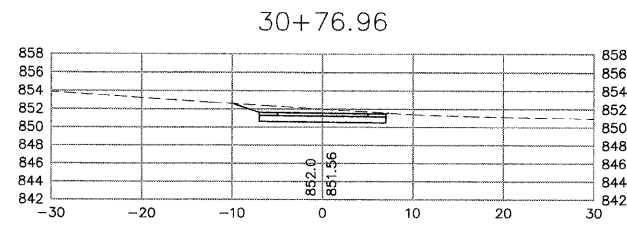
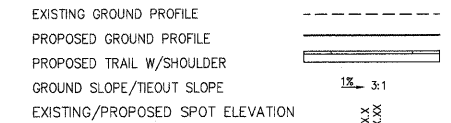
CROSS SECTIONS
STA. 22+00 TO STA 25+05.07

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	28
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

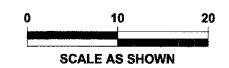
GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX. AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
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3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

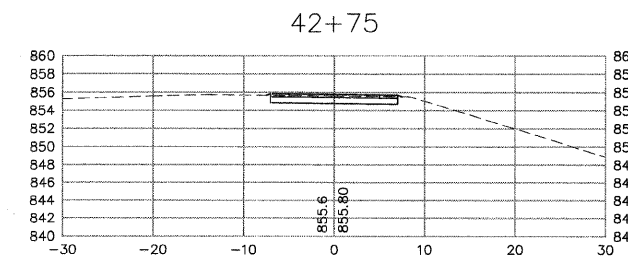
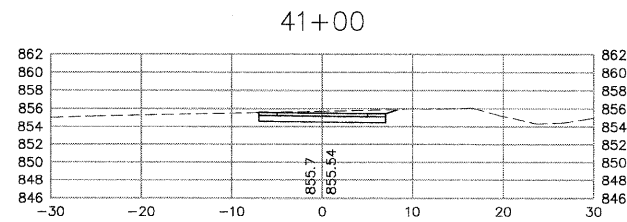
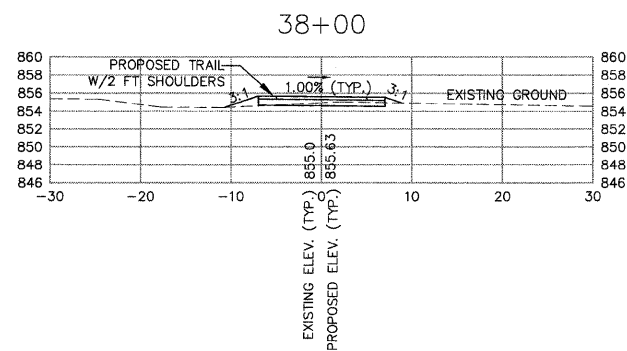
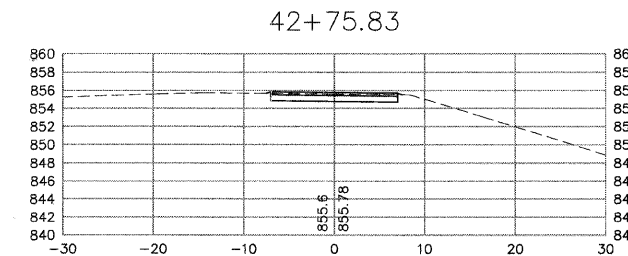
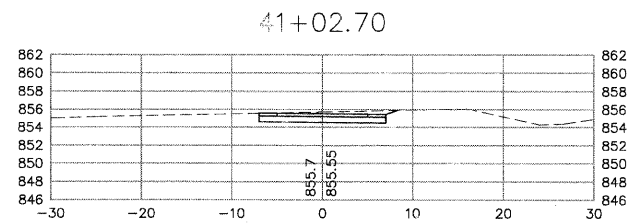
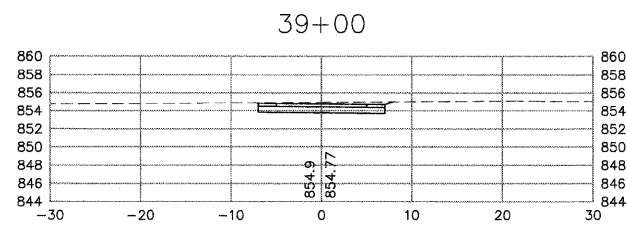
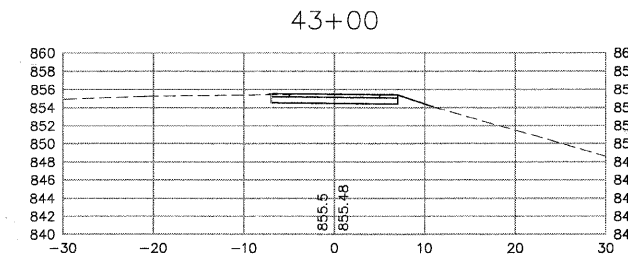
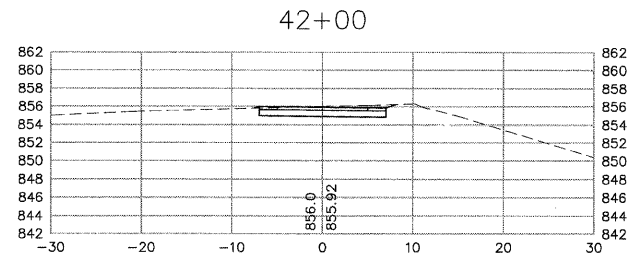
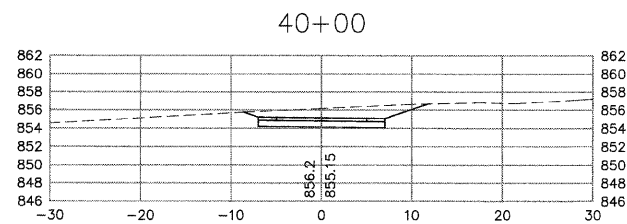
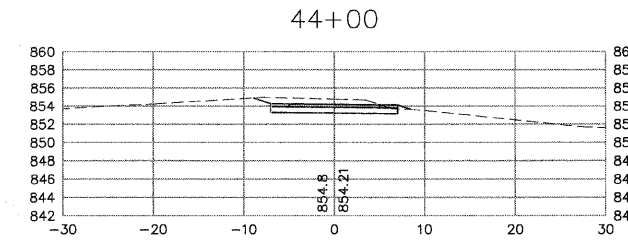
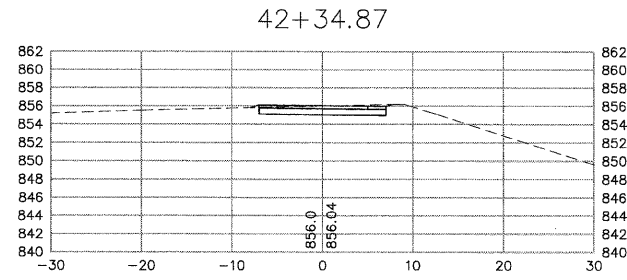
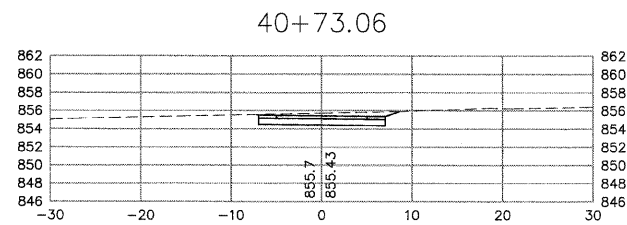
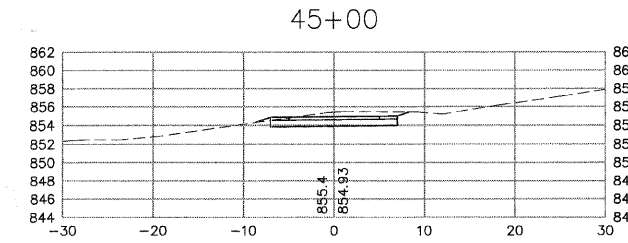
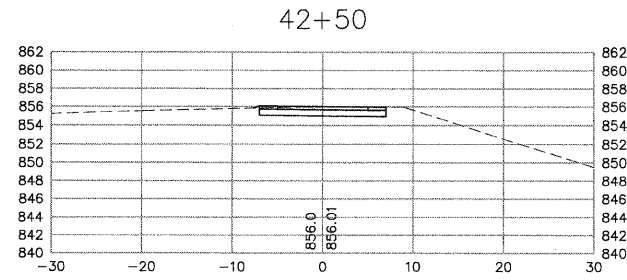
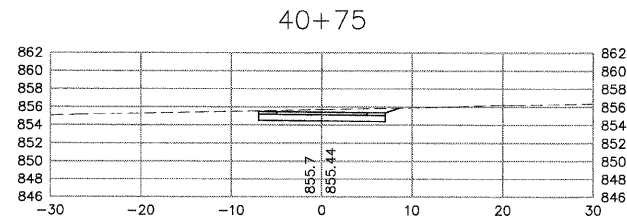
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		CARY PARK DISTRICT CARY COMMUNITY TRAIL		CROSS SECTIONS STA. 30+00 TO STA 37+00		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						06-P4001-00-BR	MCHENRY	64	30
FED. ROAD DIST. NO. 1		ILLINOIS		CONTRACT NO. 63333					

GENERAL NOTES:

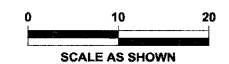
1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 56+00, 58+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SLOPE AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:

- EXISTING GROUND PROFILE
- PROPOSED GROUND PROFILE
- PROPOSED TRAIL W/SHOULDER
- GROUND SLOPE/TIEOUT SLOPE
- EXISTING/PROPOSED SPOT ELEVATION



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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

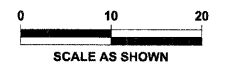
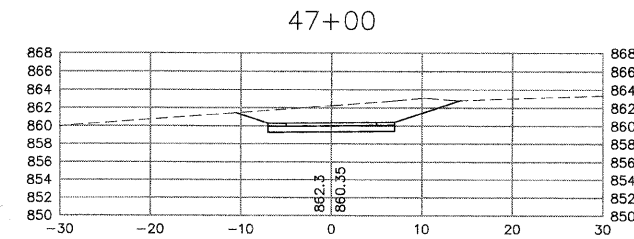
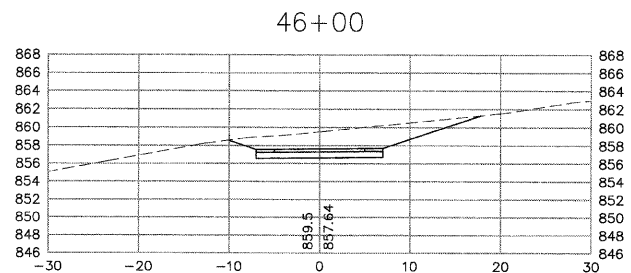
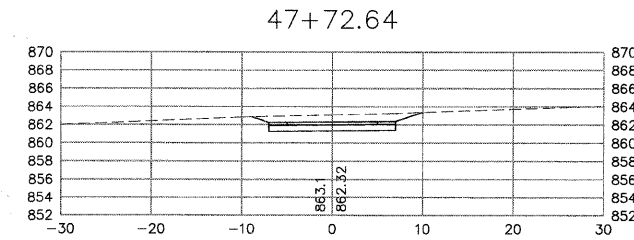
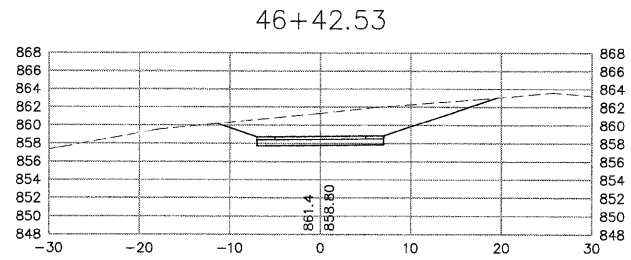
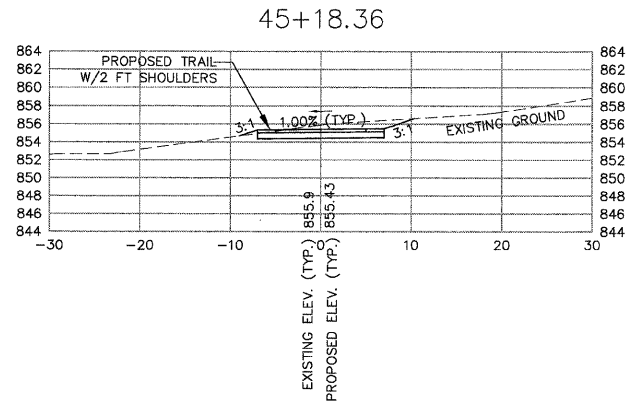
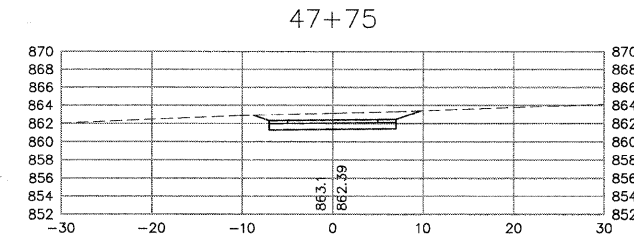
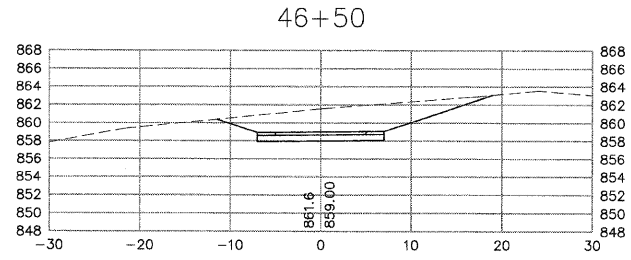
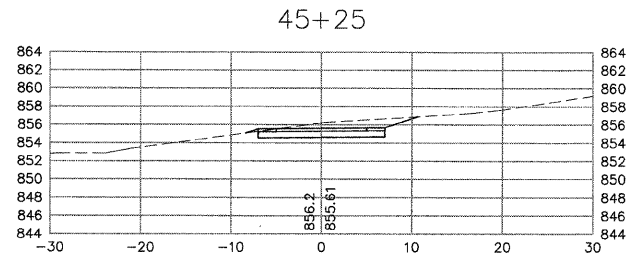
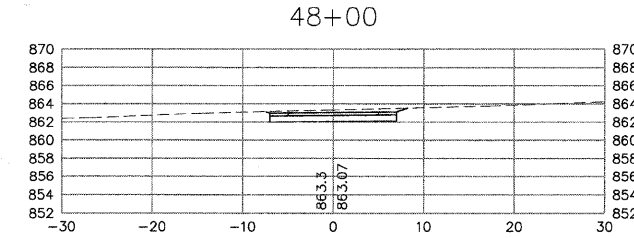
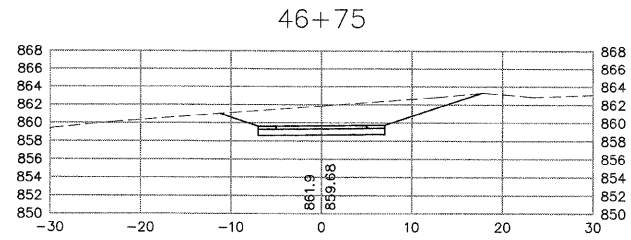
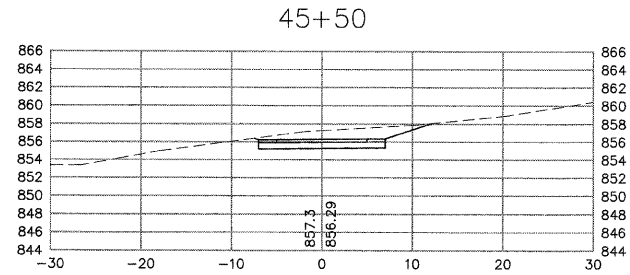
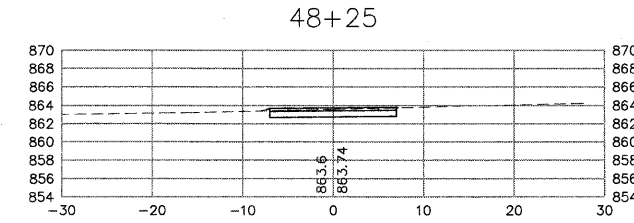
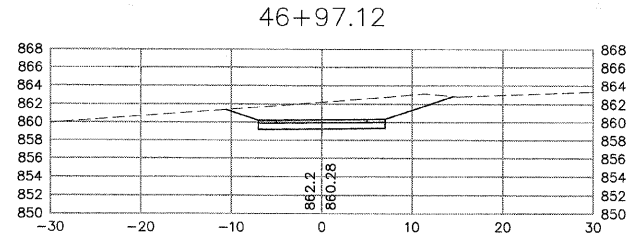
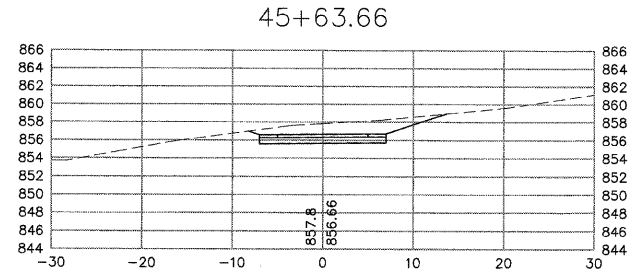
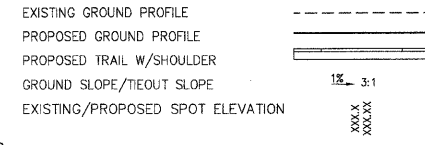
**CROSS SECTIONS
STA. 38+00 TO STA 45+00**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	31
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

1. THE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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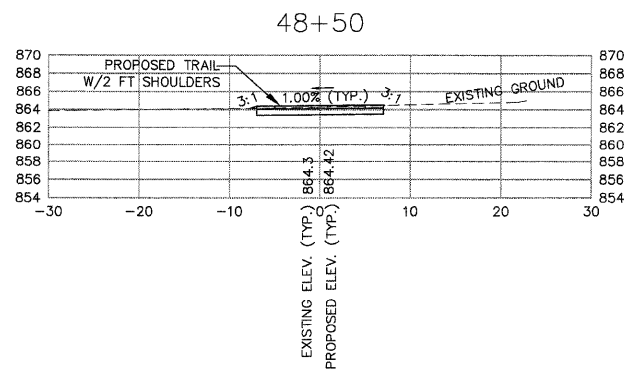
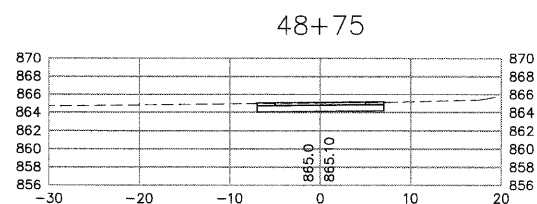
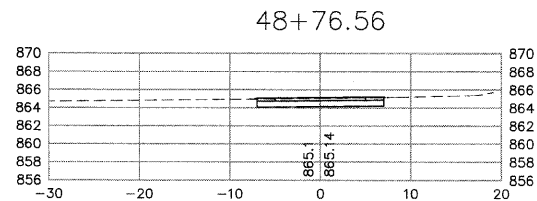
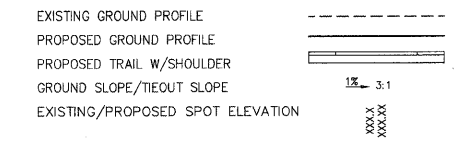
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		CARY PARK DISTRICT CARY COMMUNITY TRAIL		CROSS SECTIONS STA. 45+18.36 TO STA 48+25	
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
06-P4001-00-BR	MCHENRY	64	32		
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63333			

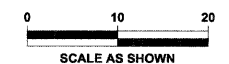
GENERAL NOTES:

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2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



This document, together with the complete and original project plans, is intended only for the specific purpose and use for which it was prepared. It is not to be used for any other purpose without the written consent of the engineer. The engineer's responsibility is limited to the design and construction of the project as shown on the plans. The contractor is responsible for the construction of the project in accordance with the plans and specifications. The engineer's office is located at 1000 N. Dearborn Street, Chicago, IL 60610.



DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

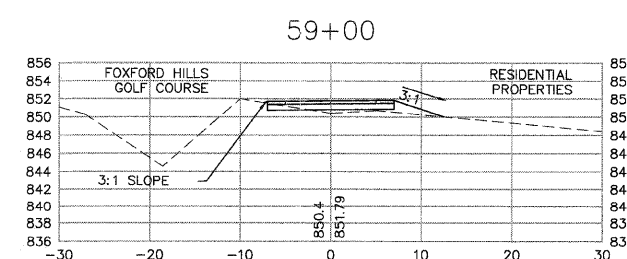
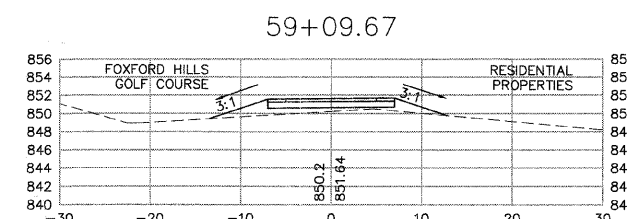
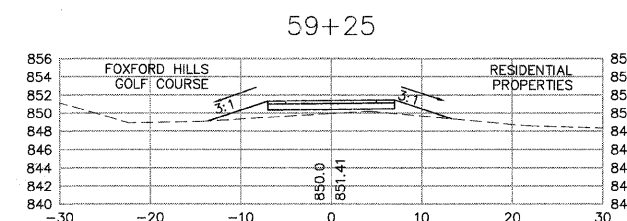
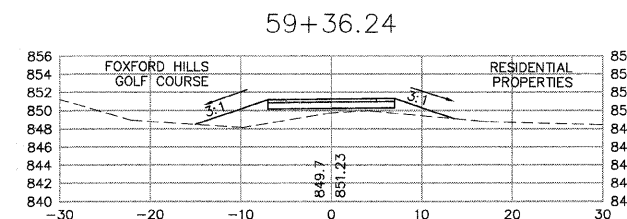
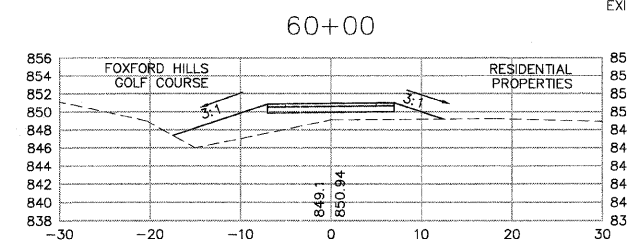
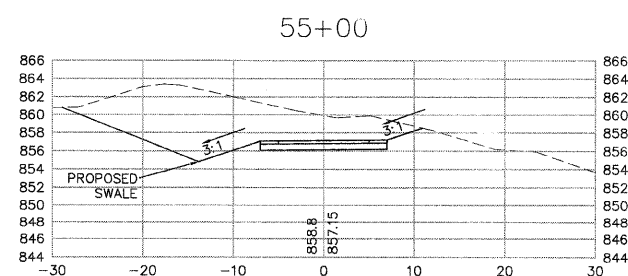
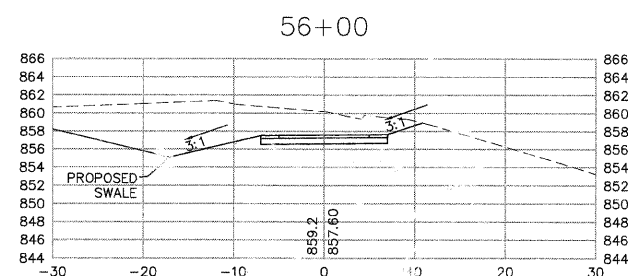
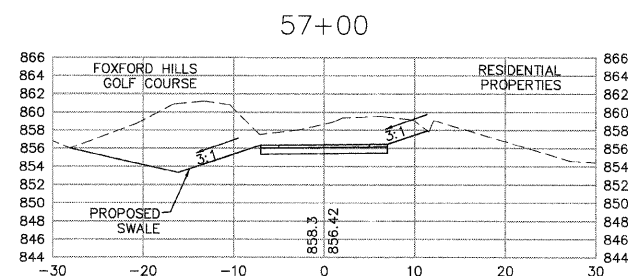
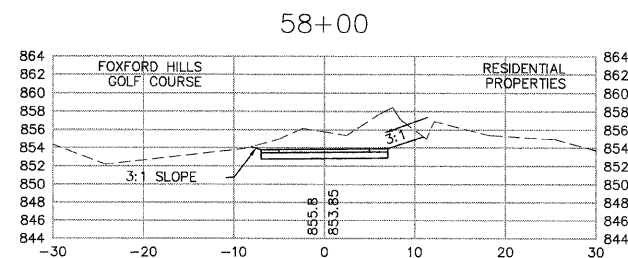
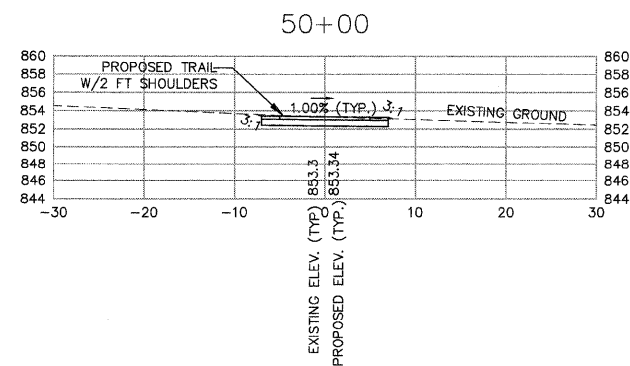
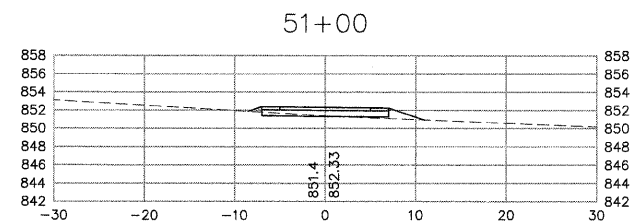
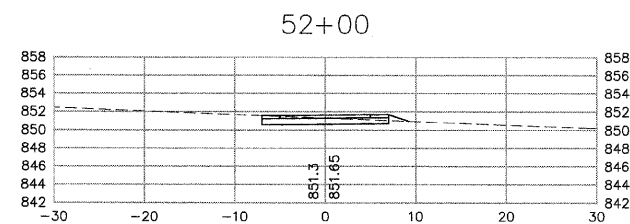
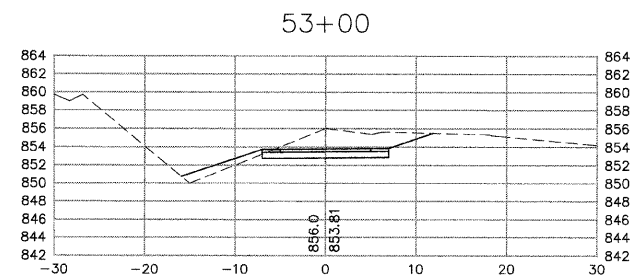
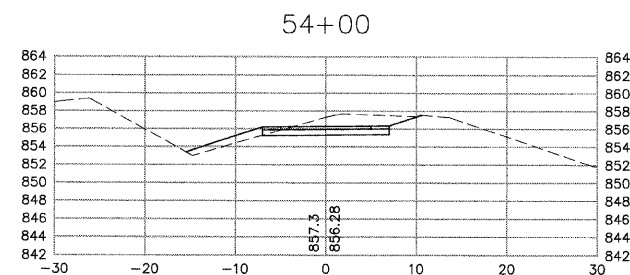
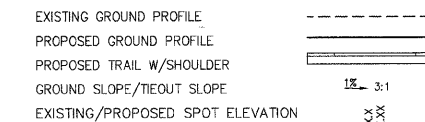
CROSS SECTIONS
STA. 48+50 TO STA 48+76.56

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	33
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.
5. EXISTING TOPO BETWEEN 54+00 AND 59+00 IS APPROXIMATE. CONTRACTOR TO CONFIRM EXISTING GRADES IN THIS AREA AND COORDINATE WITH CARY PARK DISTRICT PRIOR TO STARTING WORK.

CROSS SECTION LEGEND:



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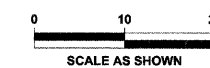
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**CROSS SECTIONS
STA. 50+00 TO STA 60+00**

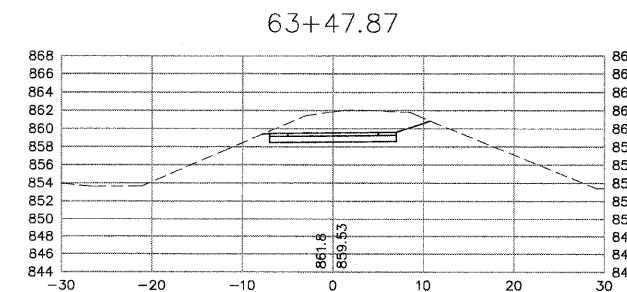
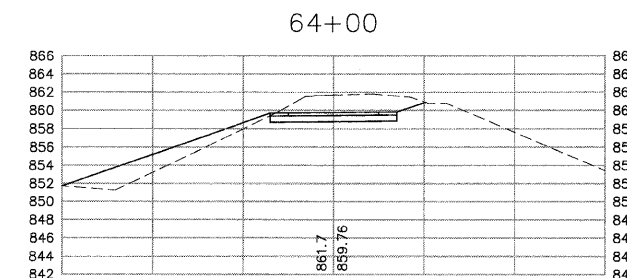
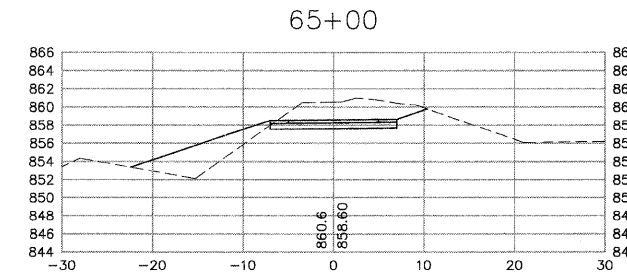
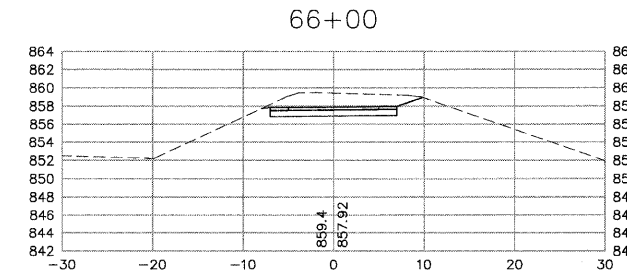
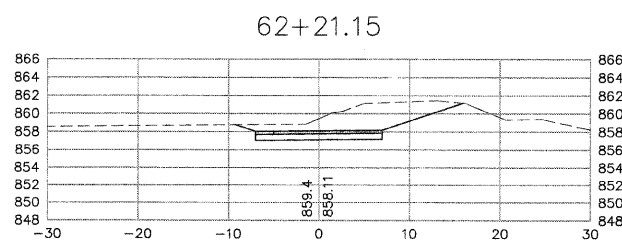
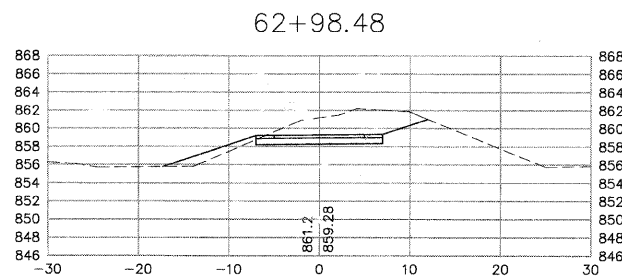
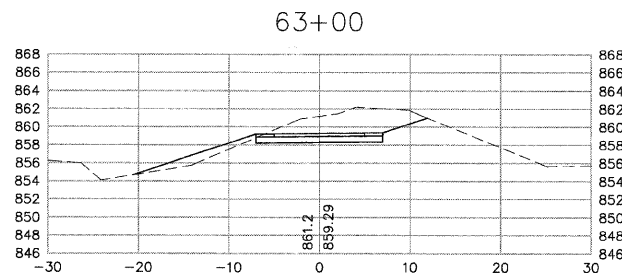
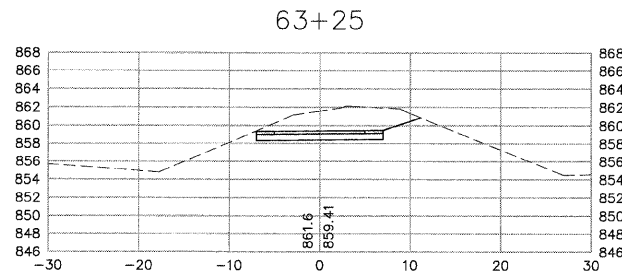
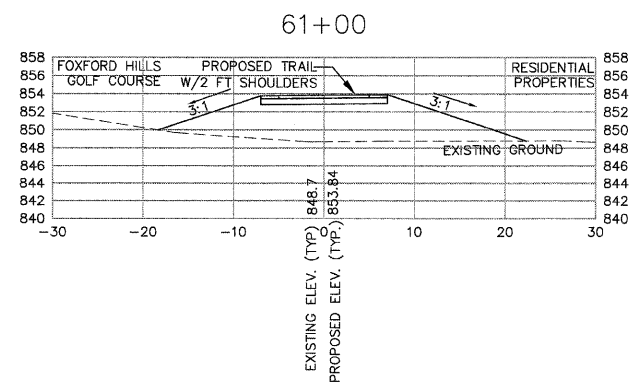
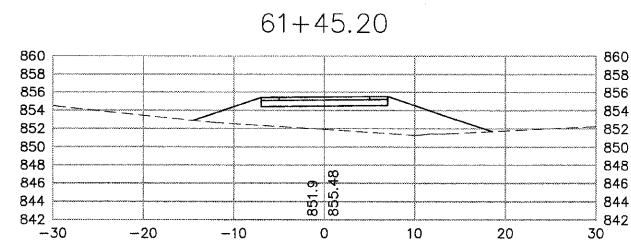
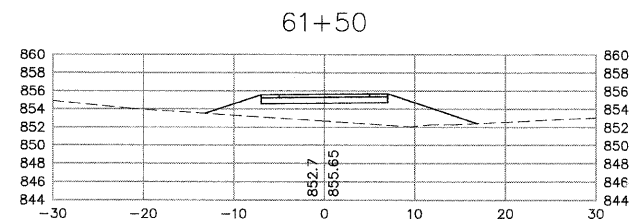
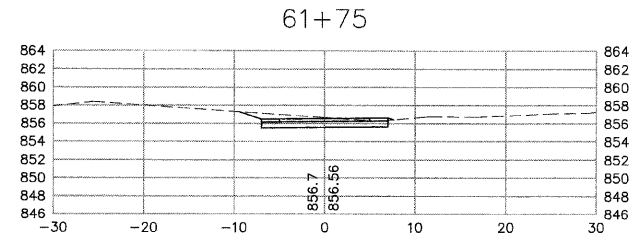
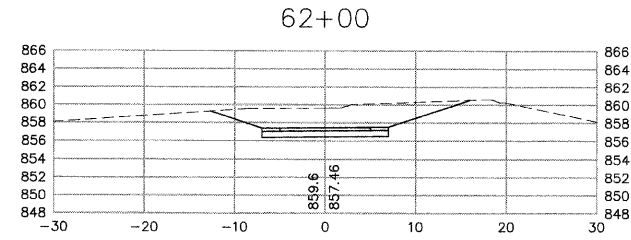
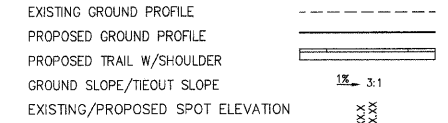
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	34
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



This document, together with the design and engineering provided herein, is intended only for the specific project and site for which it was prepared. It is not to be used for any other project or site without the express written consent of the engineer of record. The engineer of record shall not be responsible for any errors or omissions in this document or for any consequences arising from its use. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The user of this document shall also be responsible for obtaining all necessary insurance and bonding. The user of this document shall also be responsible for obtaining all necessary approvals from the appropriate authorities. The user of this document shall also be responsible for obtaining all necessary approvals from the appropriate authorities.

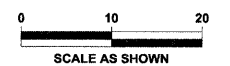
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**CROSS SECTIONS
STA. 61+00 TO STA 66+00**

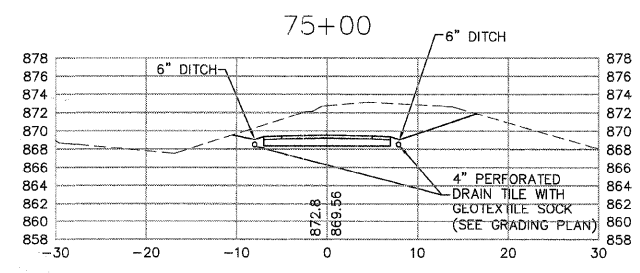
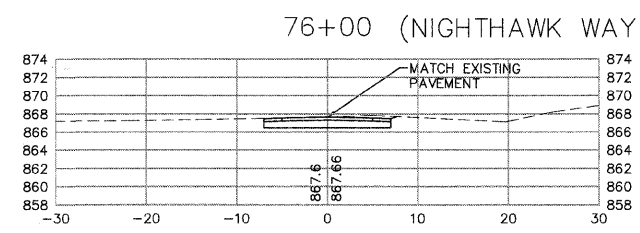
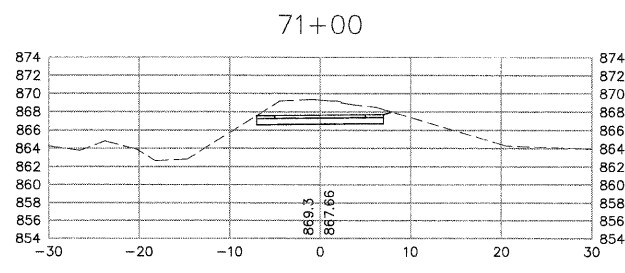
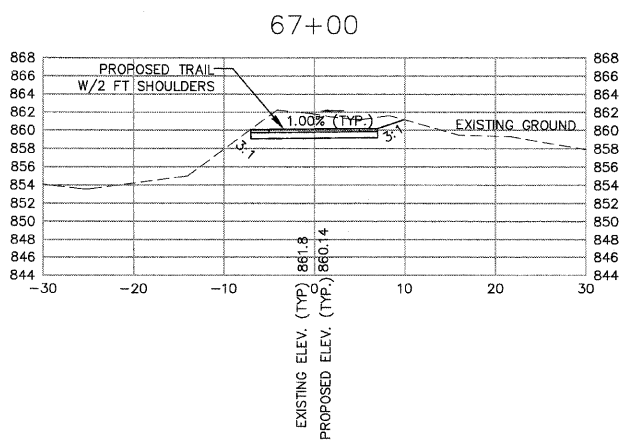
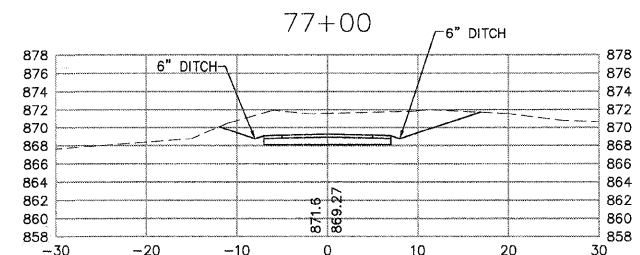
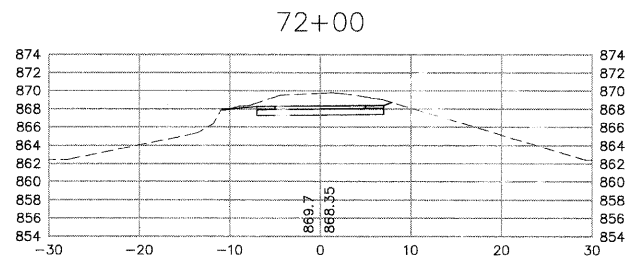
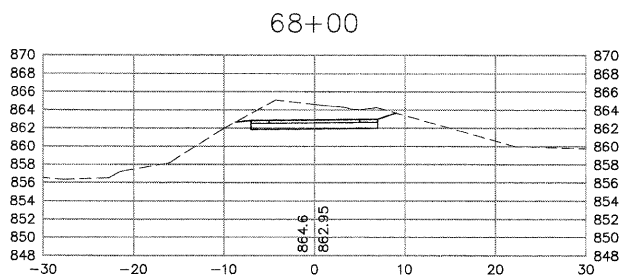
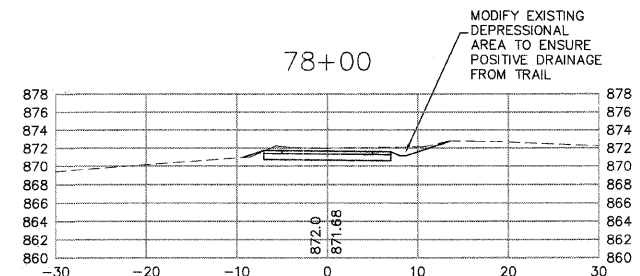
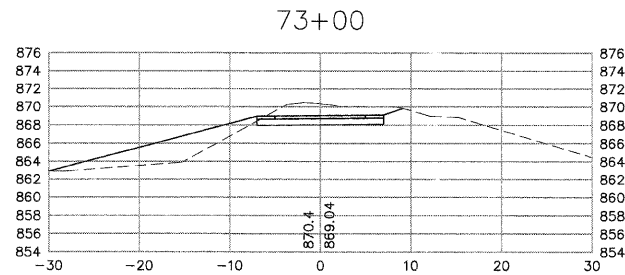
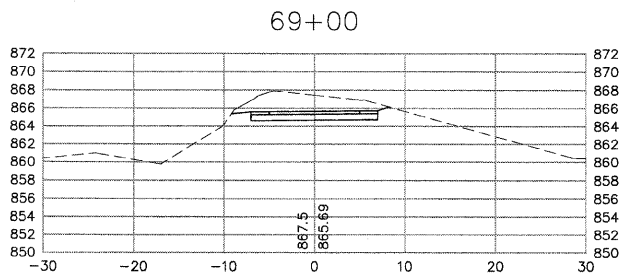
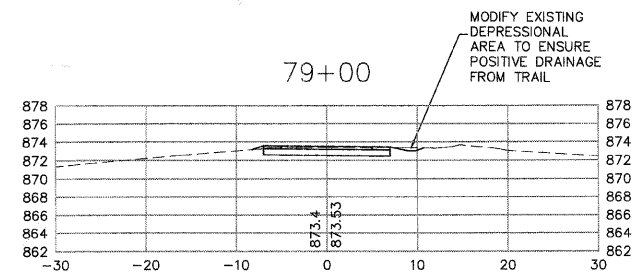
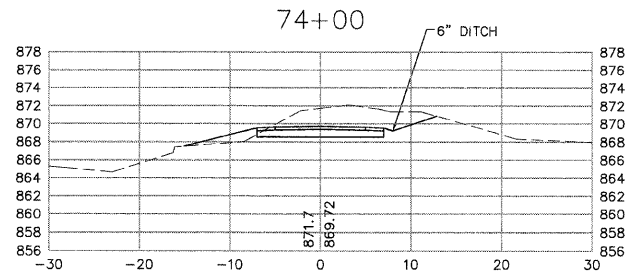
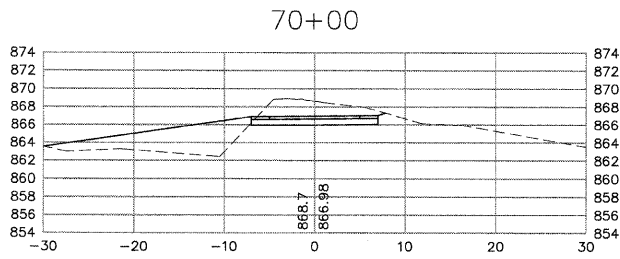
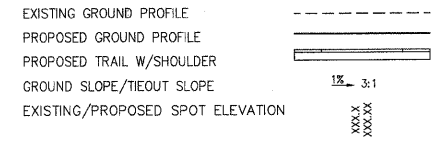
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	35
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, 71+00, 72+00, 73+00, 74+00, 75+00, 76+00, 77+00, 78+00, 79+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



This document, together with the concepts and designs presented herein, is submitted as advisory only for the specific purpose stated for which it was prepared. It is not to be used for any other purpose without the written consent of the engineer. The engineer's liability is limited to the work shown on this drawing. No other work shall be done by reference to this drawing.

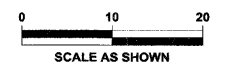
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**CROSS SECTIONS
STA. 67+00 TO STA 79+00**

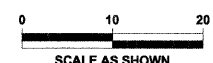
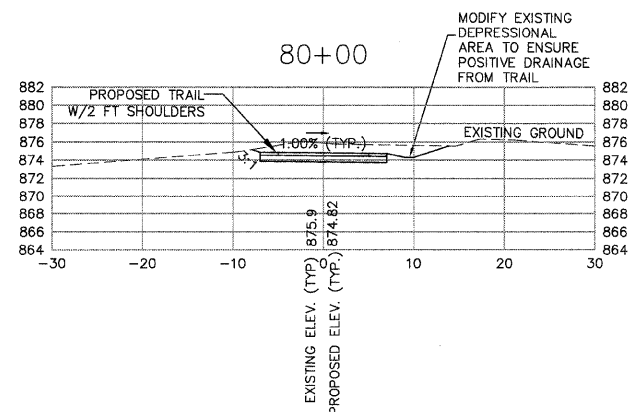
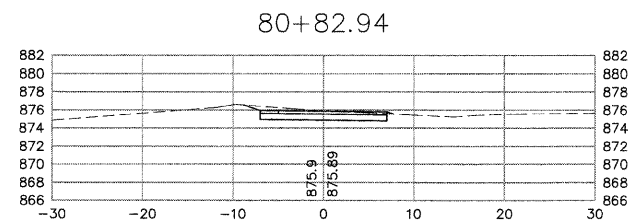
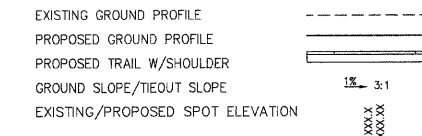
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	36
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



GENERAL NOTES:

1. TIE CUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

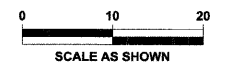
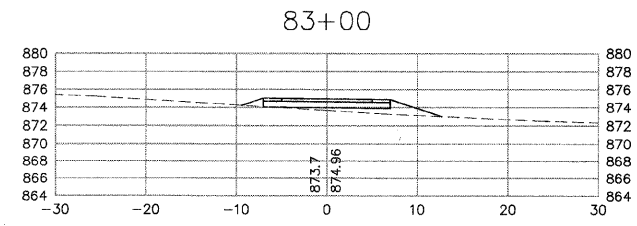
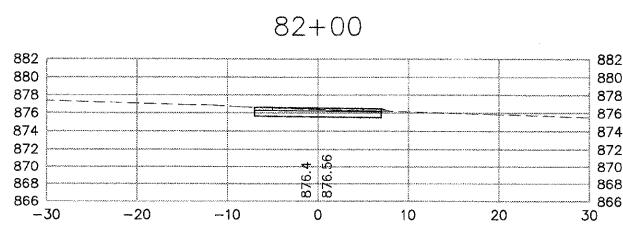
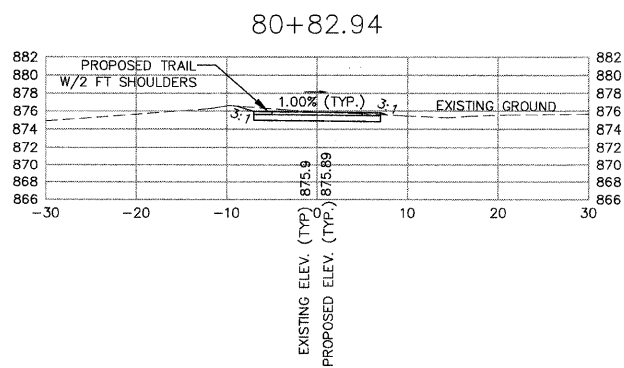
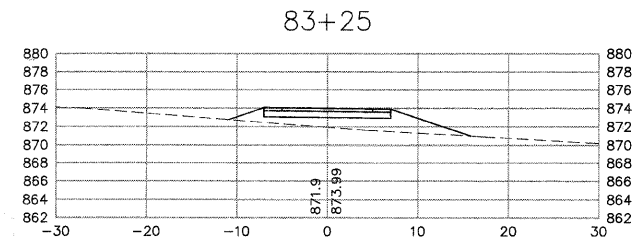
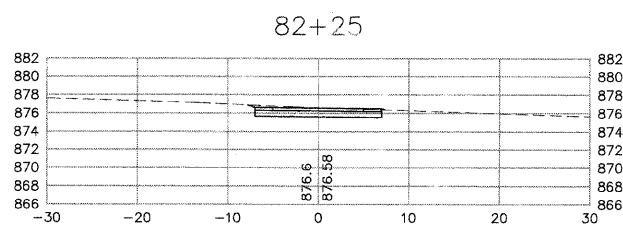
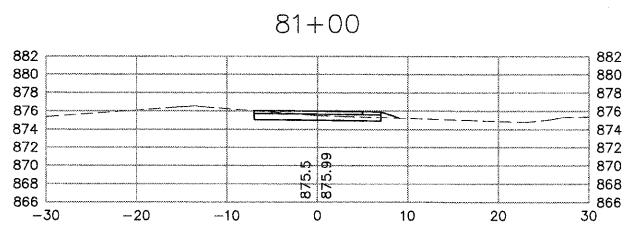
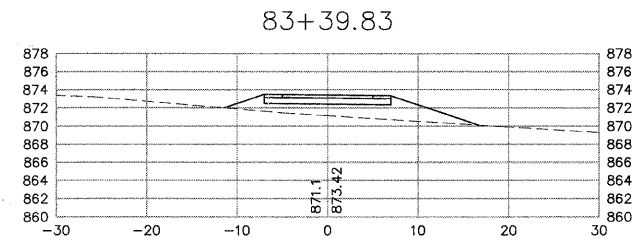
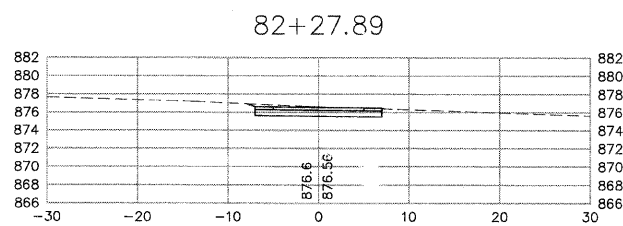
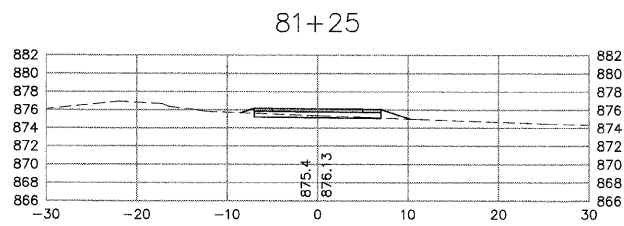
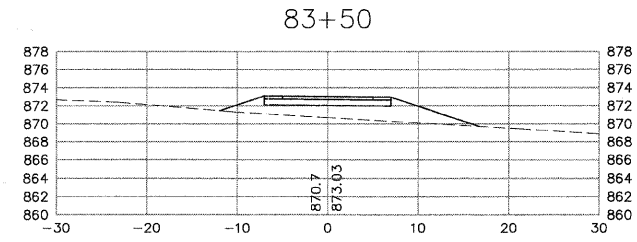
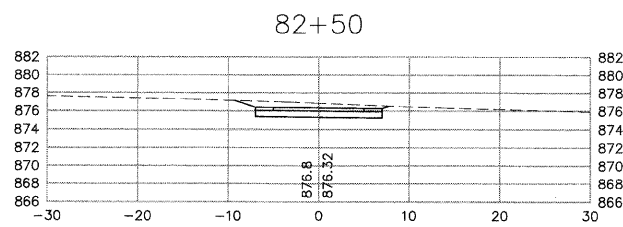
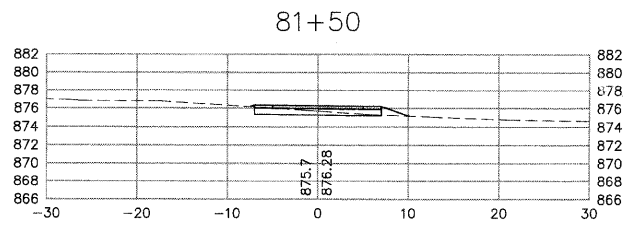
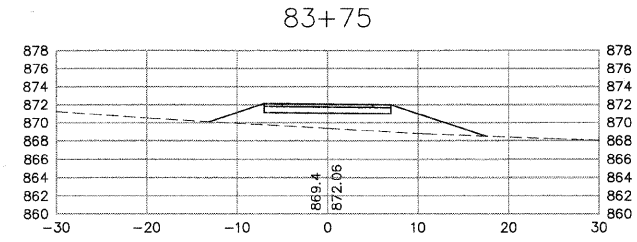
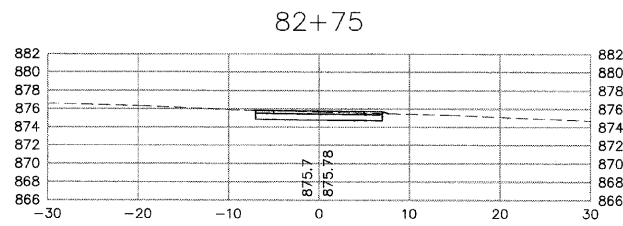
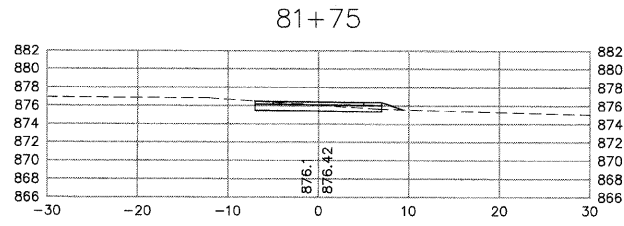
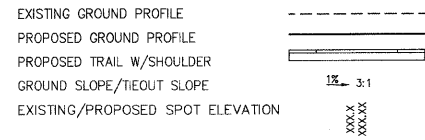
CROSS SECTIONS
STA 80+00 TO STA 80+82.94

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	37
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

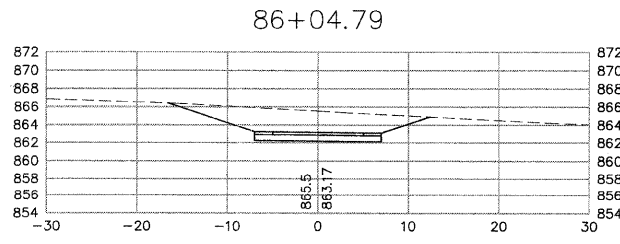
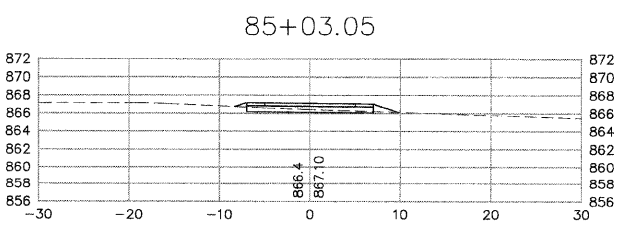
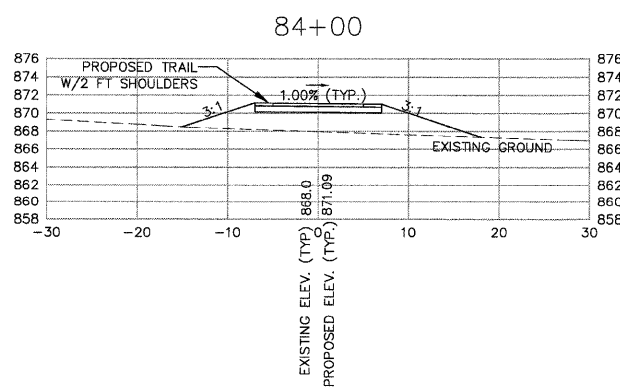
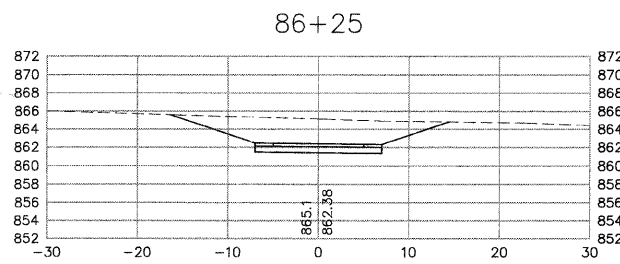
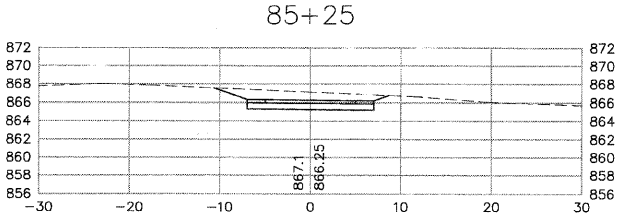
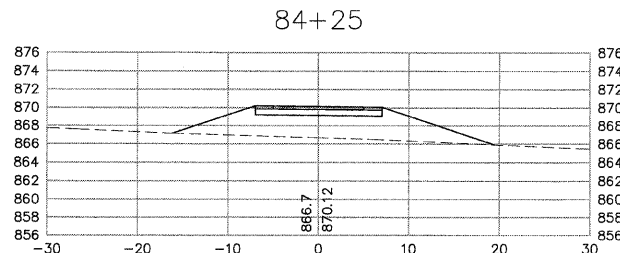
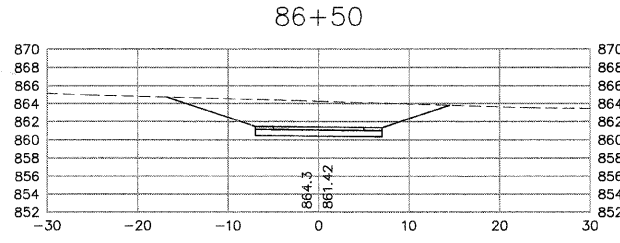
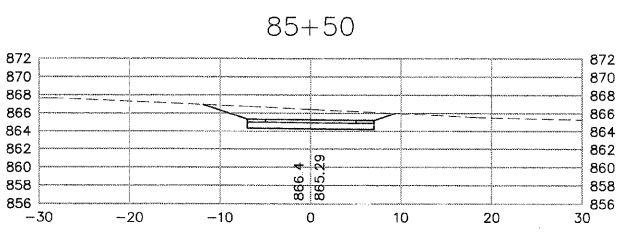
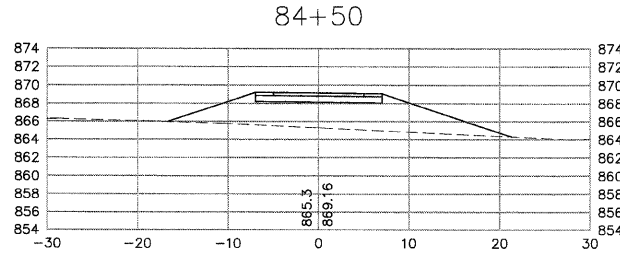
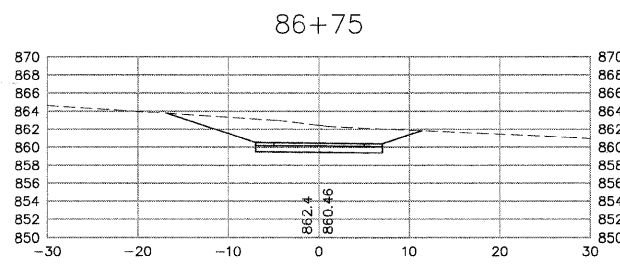
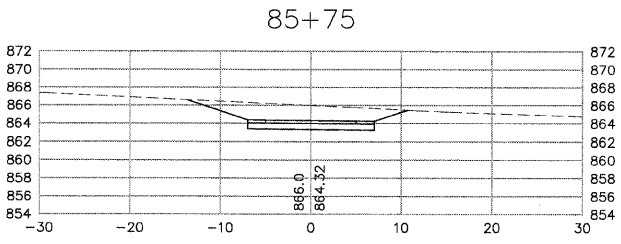
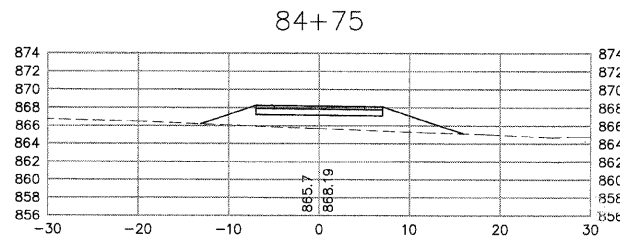
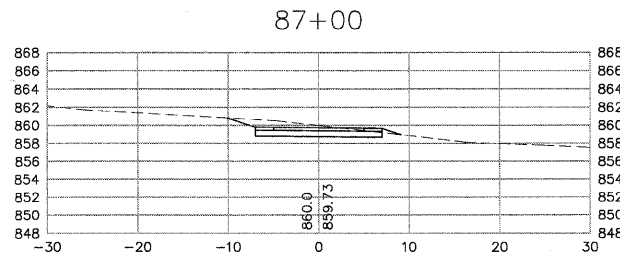
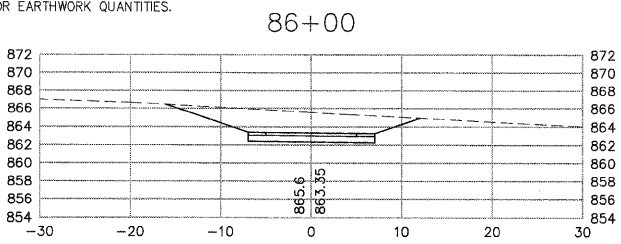
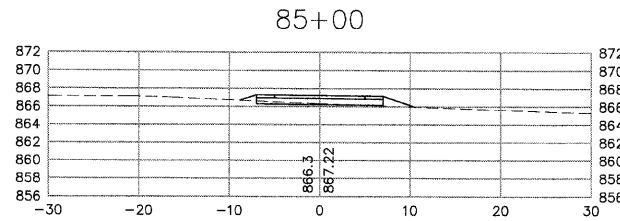
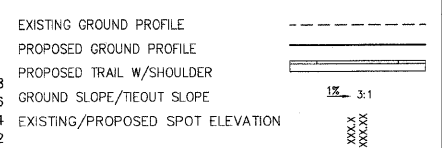
CROSS SECTIONS
STA. 80+82.94 TO STA. 83+75

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	38
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
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CROSS SECTION LEGEND:



The information contained herein is the property of the State of Illinois and is loaned to the contractor for the project only. It is not to be used for any other purpose without the written consent of the State of Illinois. The contractor shall be responsible for the accuracy of the information contained herein. The contractor shall be responsible for the accuracy of the information contained herein. The contractor shall be responsible for the accuracy of the information contained herein.

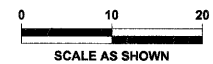
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DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

**CROSS SECTIONS
 STA. 84+00 TO STA. 87+00**

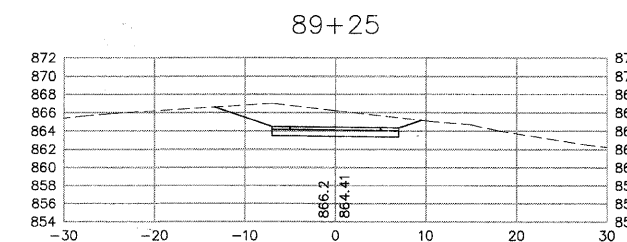
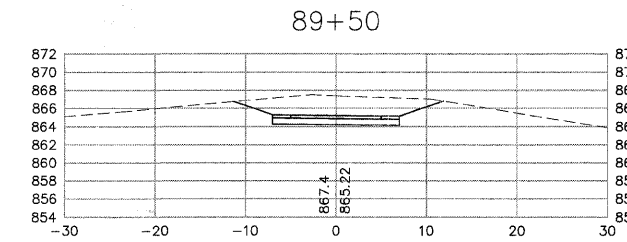
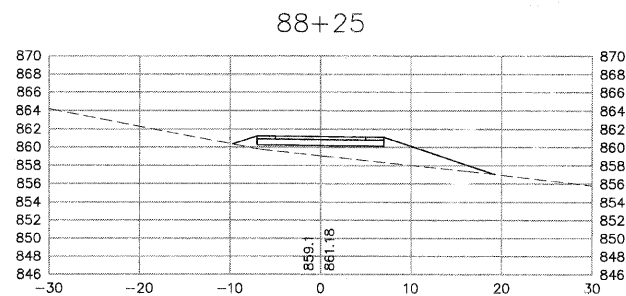
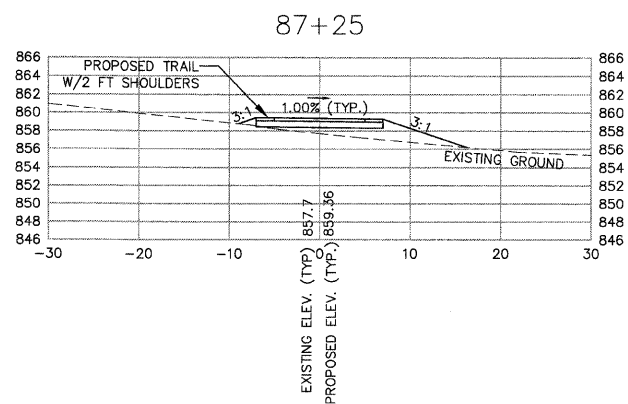
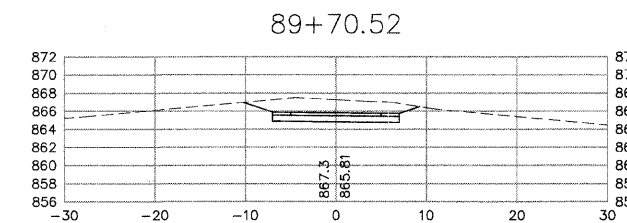
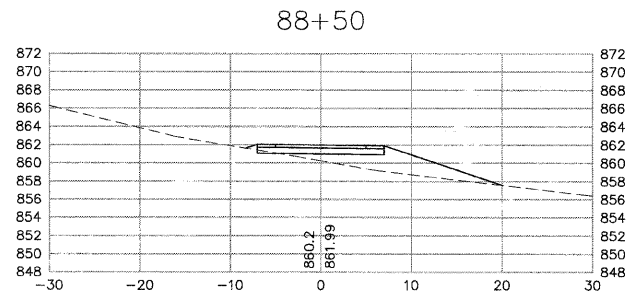
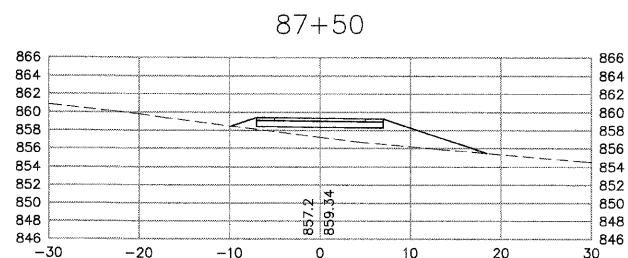
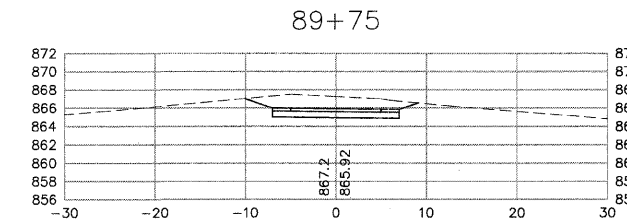
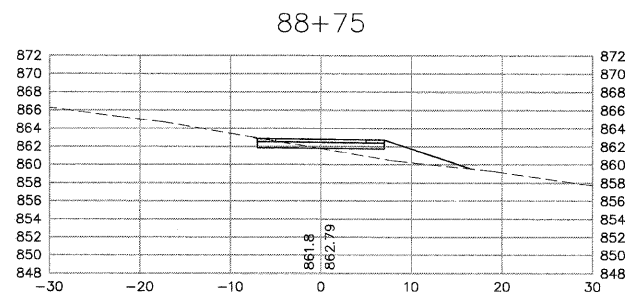
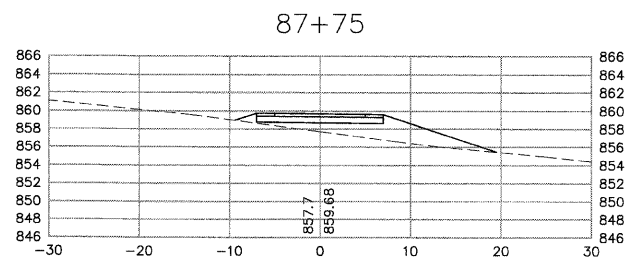
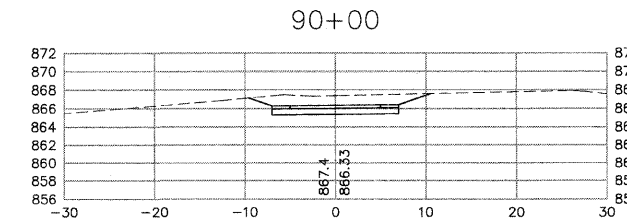
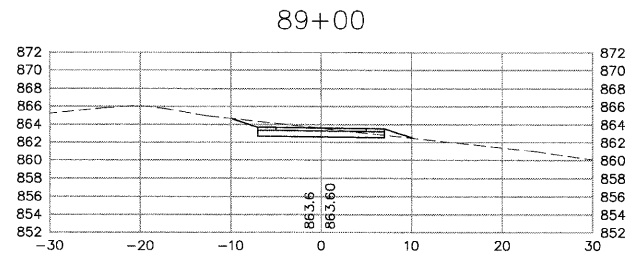
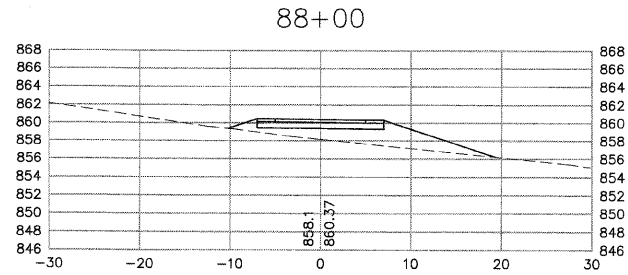
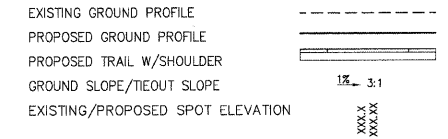
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	39
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



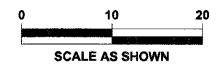
GENERAL NOTES:

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CROSS SECTION LEGEND:



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DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

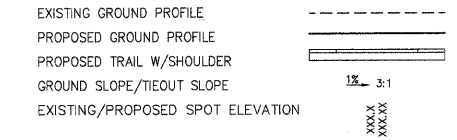
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

CROSS SECTIONS
STA. 87+25 TO STA. 90+00

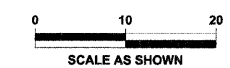
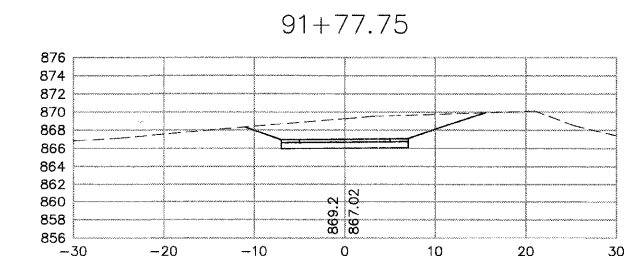
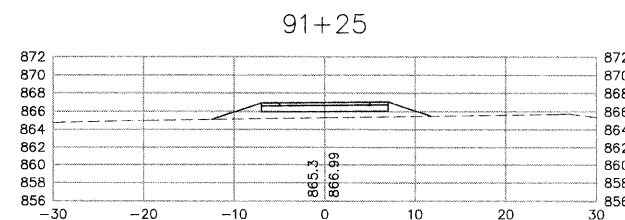
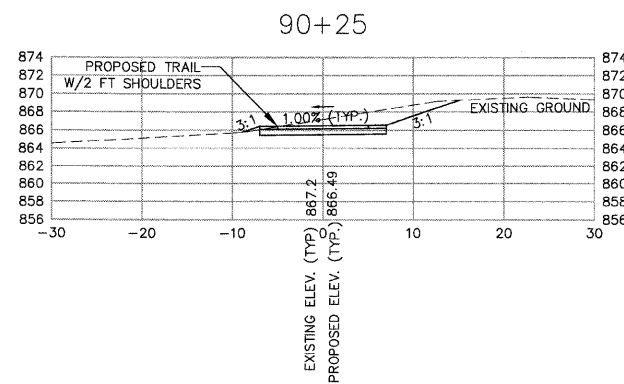
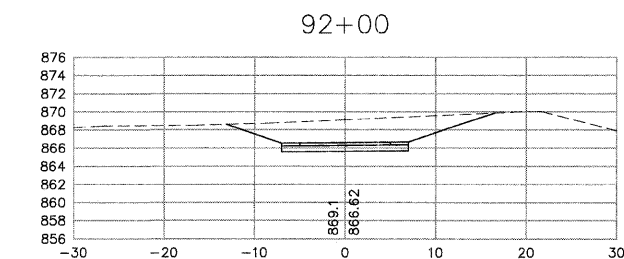
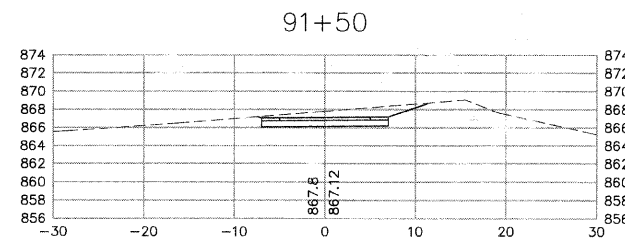
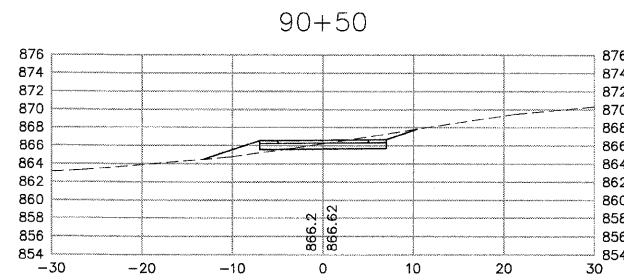
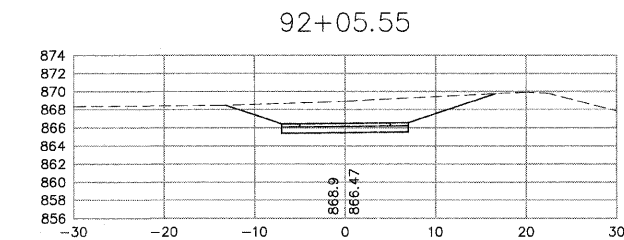
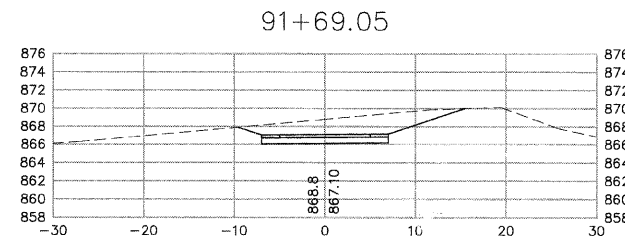
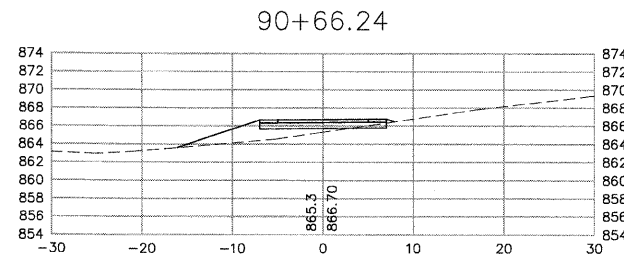
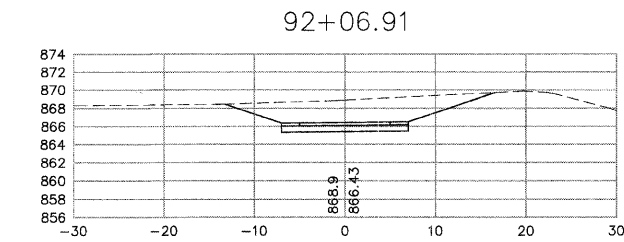
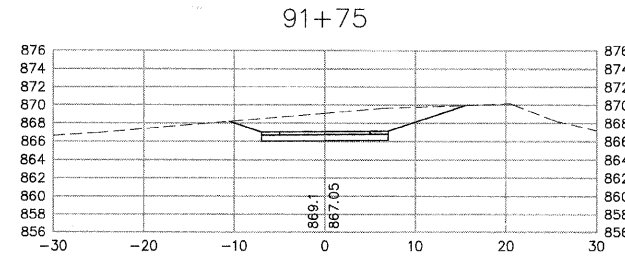
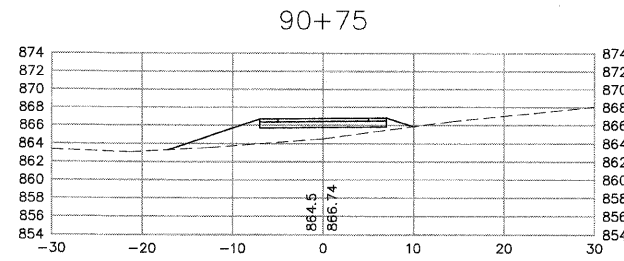
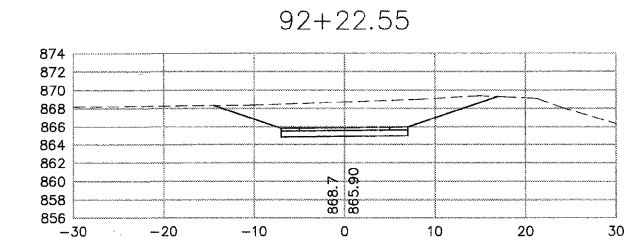
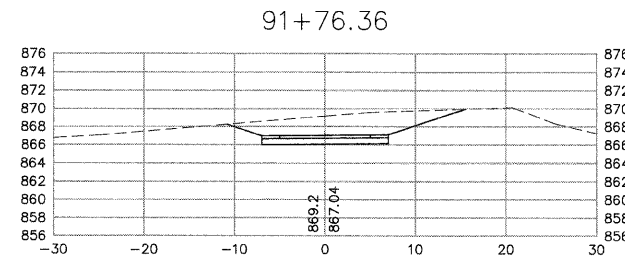
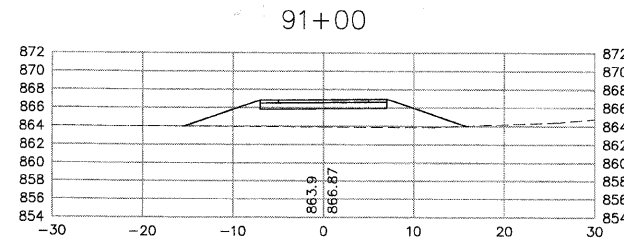
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	40
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

CROSS SECTION LEGEND:



GENERAL NOTES:

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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

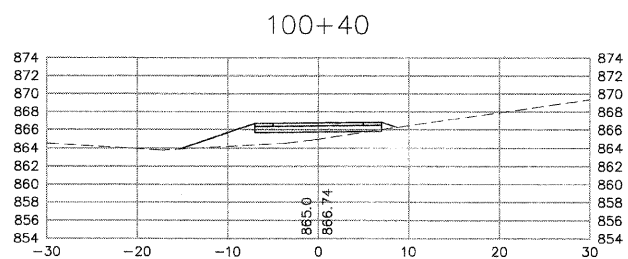
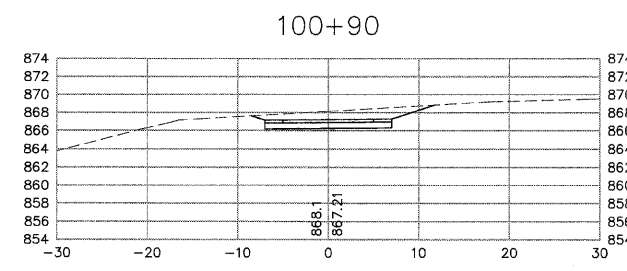
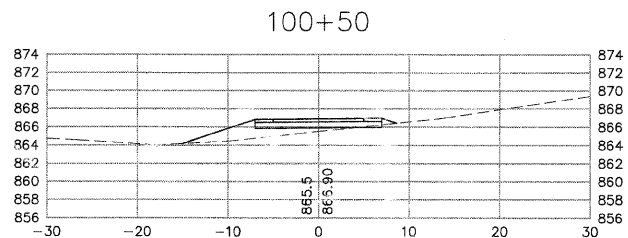
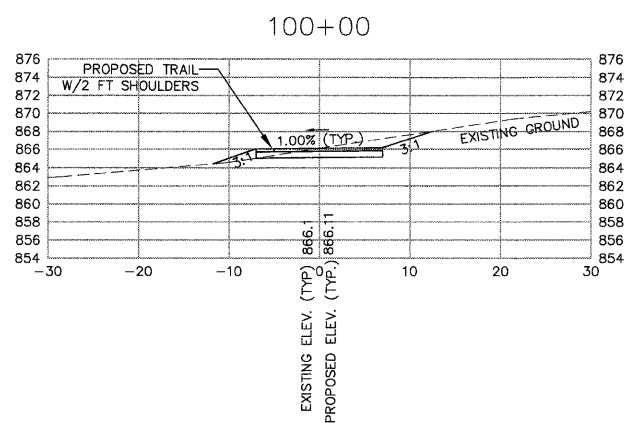
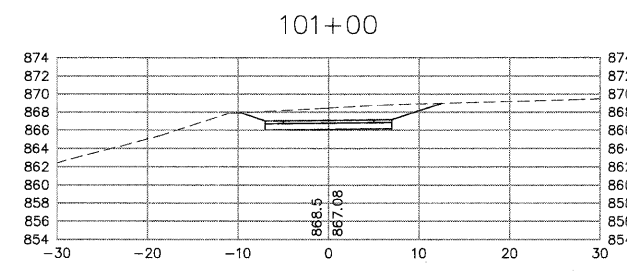
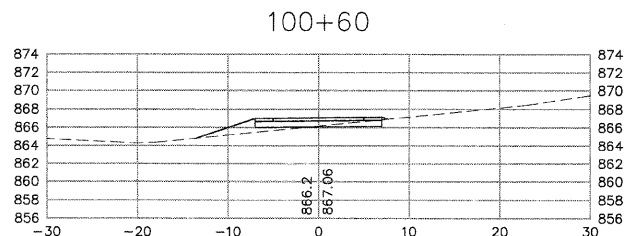
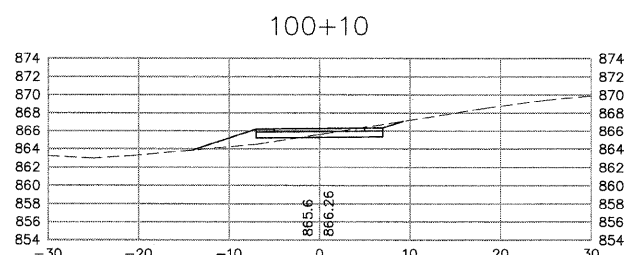
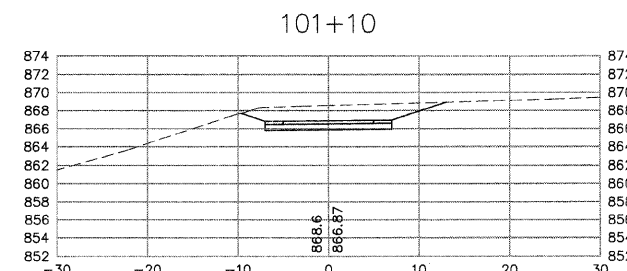
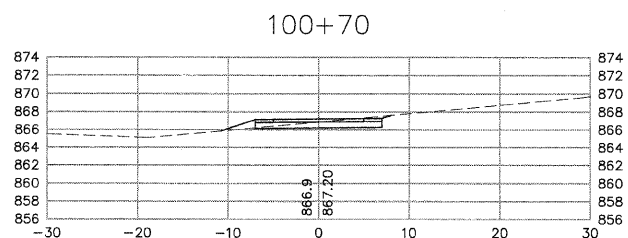
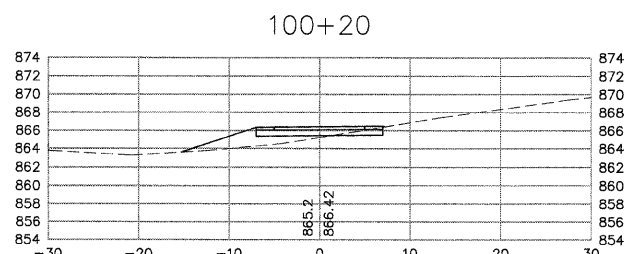
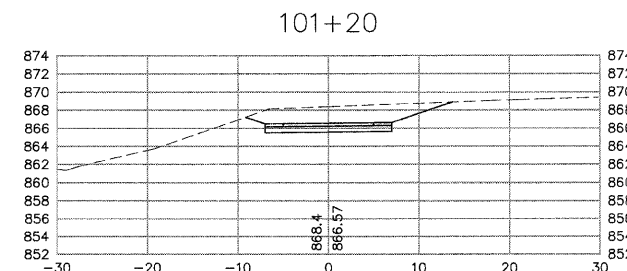
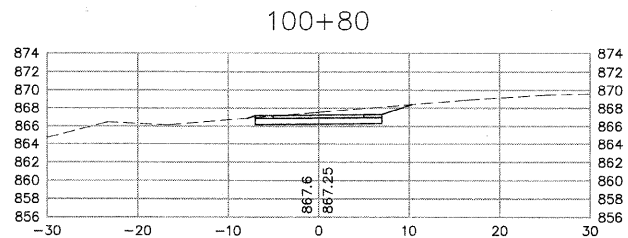
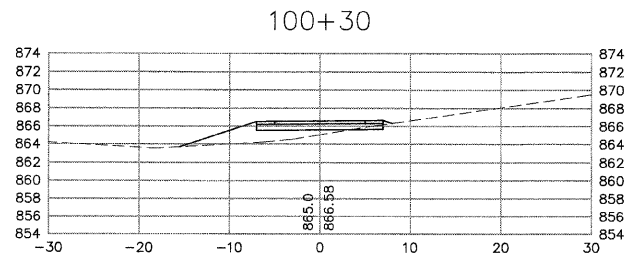
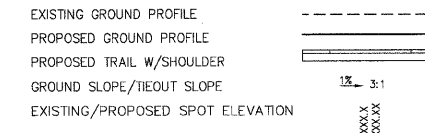
**CROSS SECTIONS
STA. 90+25 TO STA. 92+22.55**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	41
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

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CROSS SECTION LEGEND:



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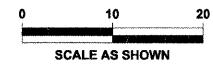
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**CROSS SECTIONS
STA. 100+00 TO STA. 101+20**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	42
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

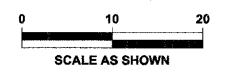
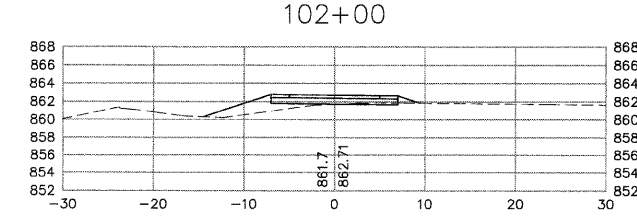
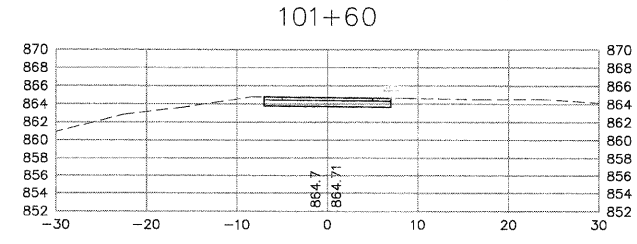
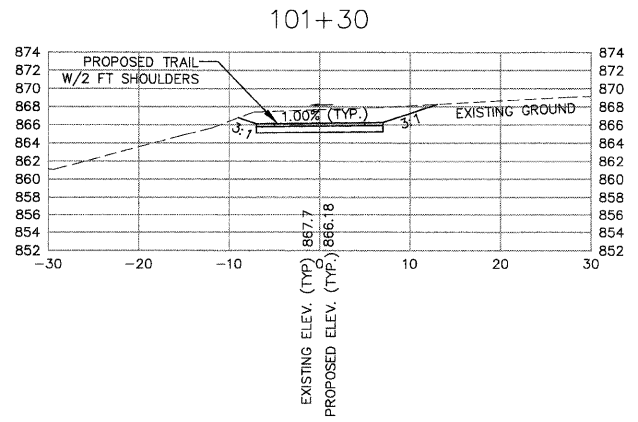
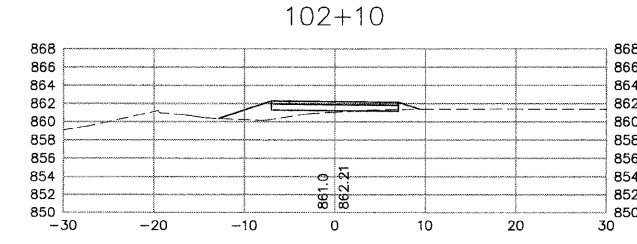
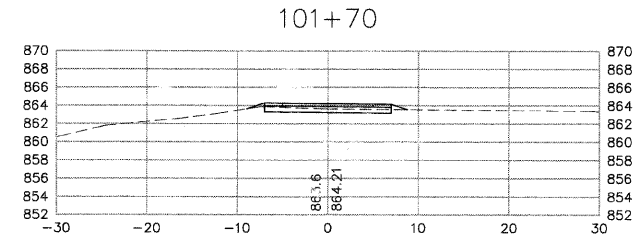
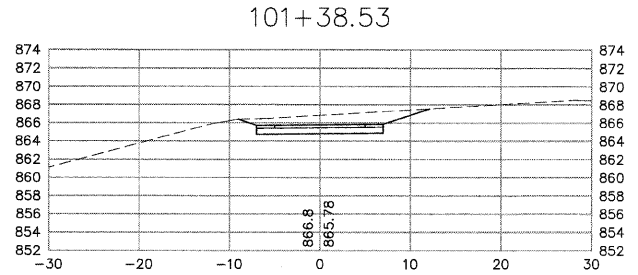
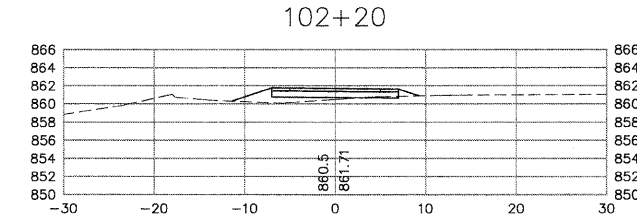
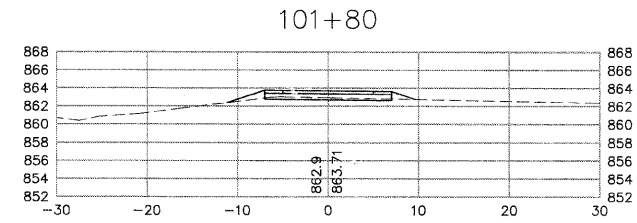
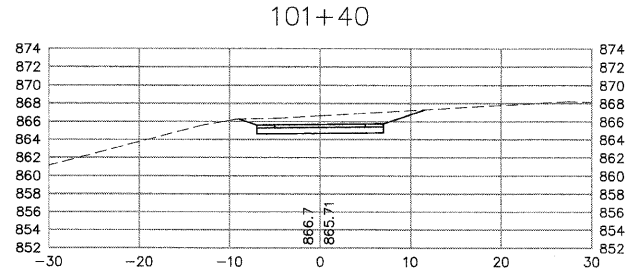
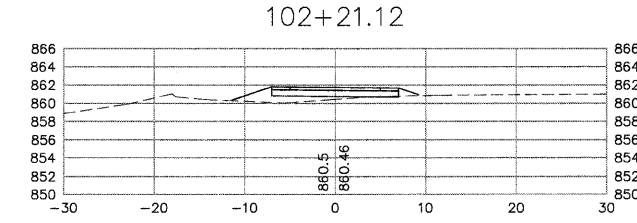
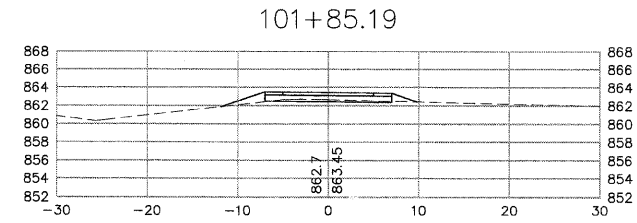
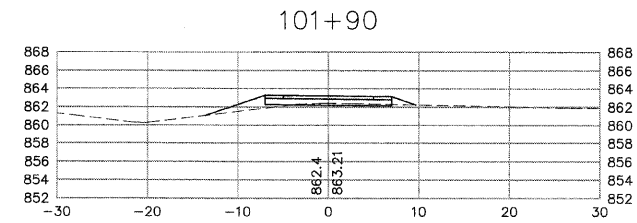
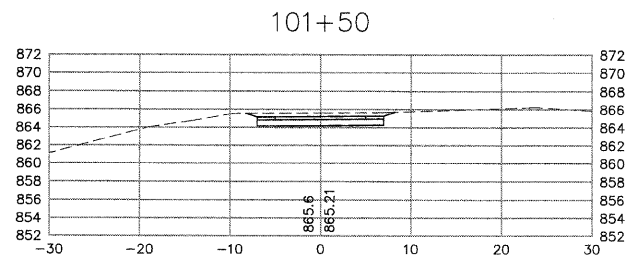
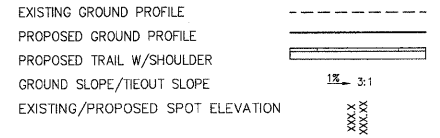


This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Please refer to the contract documents for a complete description of the project. No liability is assumed for any use of this document for any other purpose.

GENERAL NOTES:

- TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT B) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
- PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SLOPES AND TRANSITIONS.
- PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

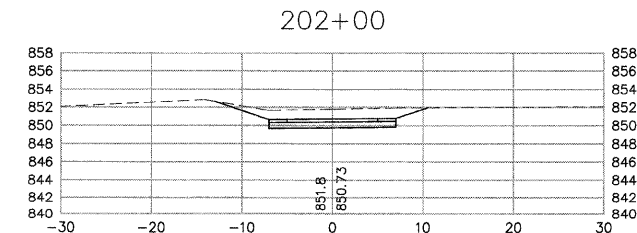
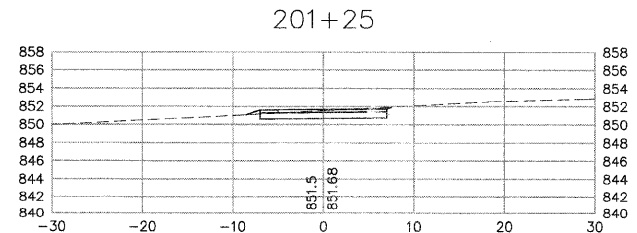
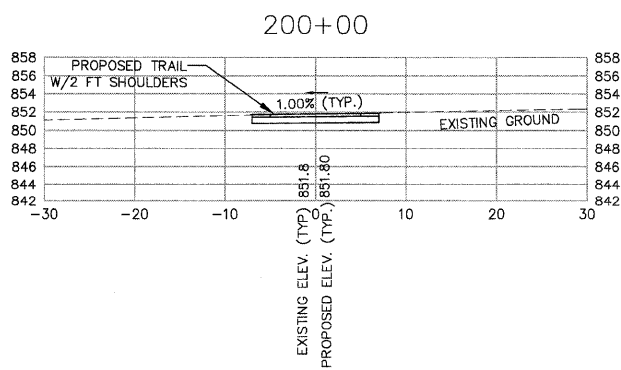
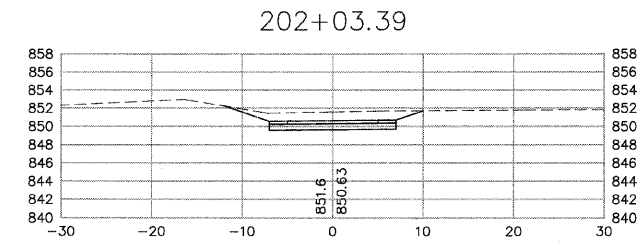
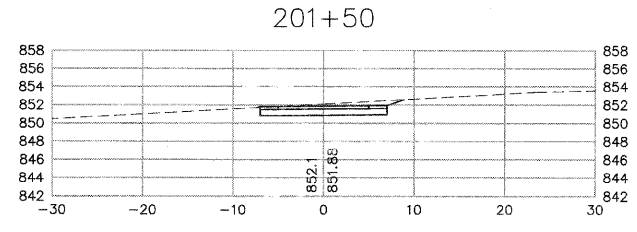
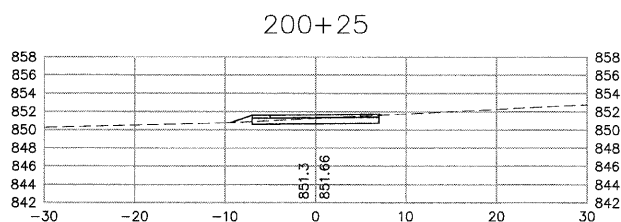
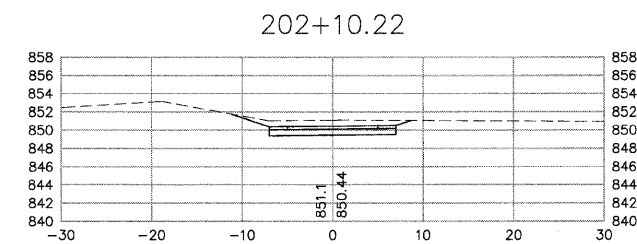
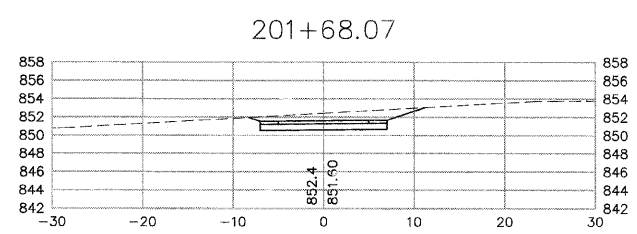
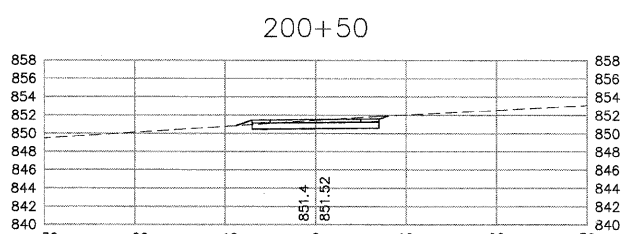
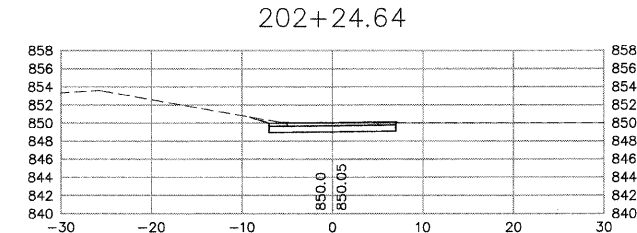
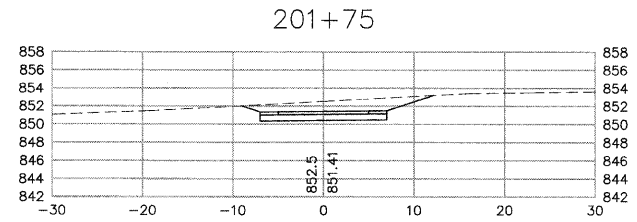
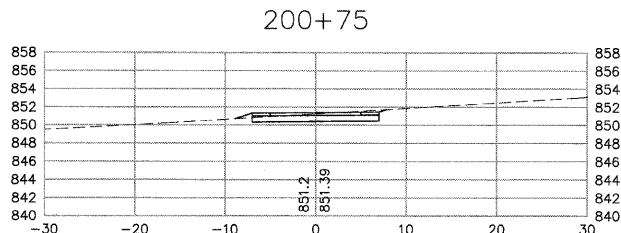
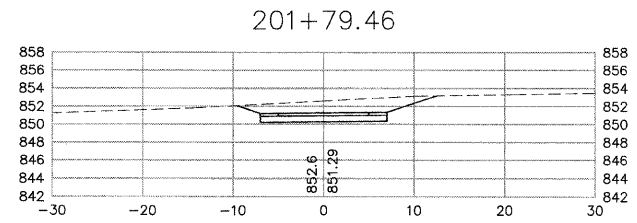
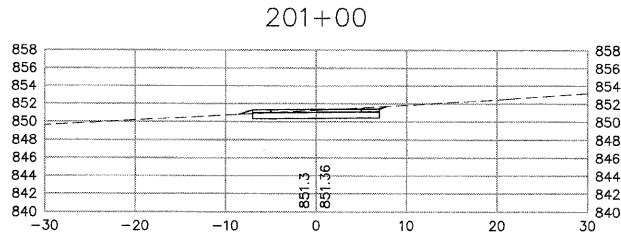
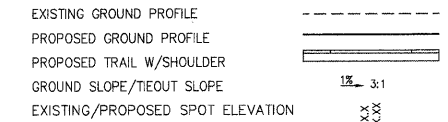
CROSS SECTIONS
STA. 101+30 TO STA. 102+21.12

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	43
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

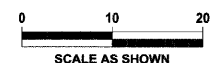
GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



This document, together with the complete and original proposed plans, is intended only for the specific purpose and client for which it was prepared. Please refer to the contract documents for the project name, location, and any other information. No responsibility is assumed for any errors or omissions.



DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

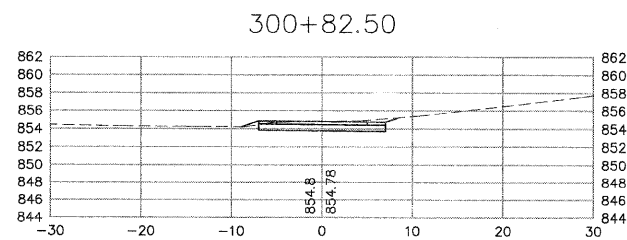
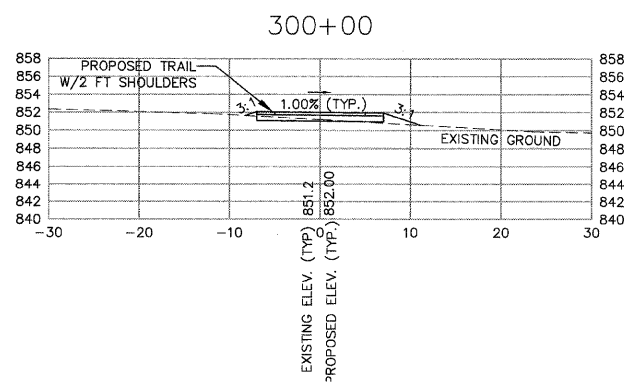
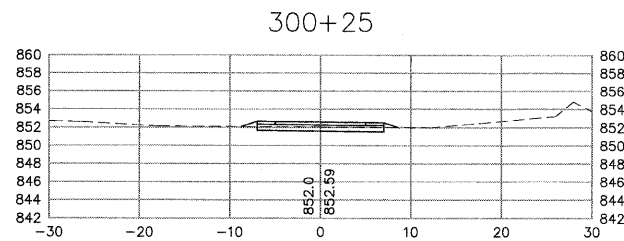
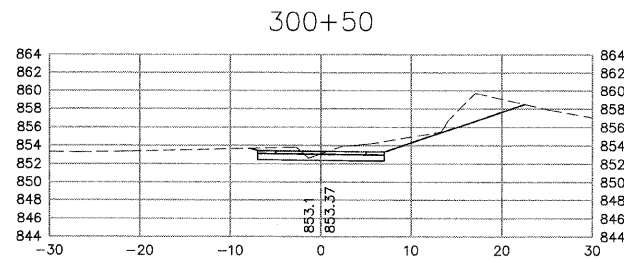
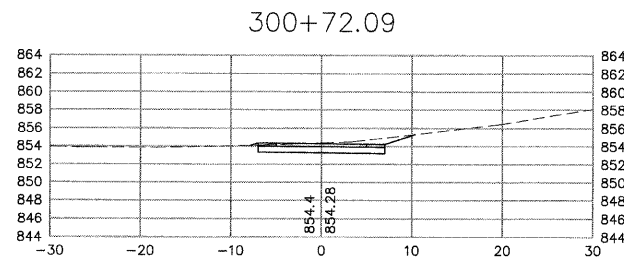
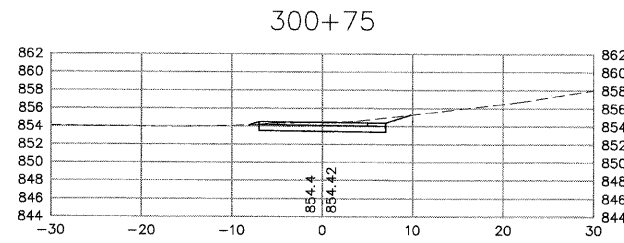
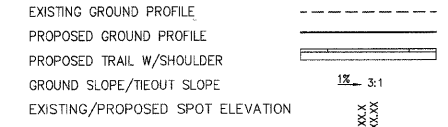
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SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	44
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

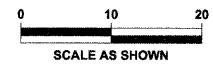
GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

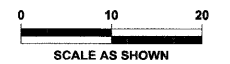
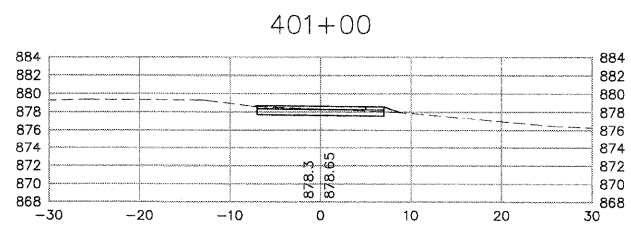
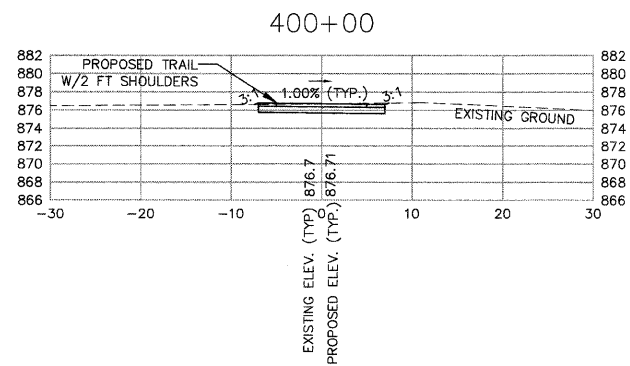
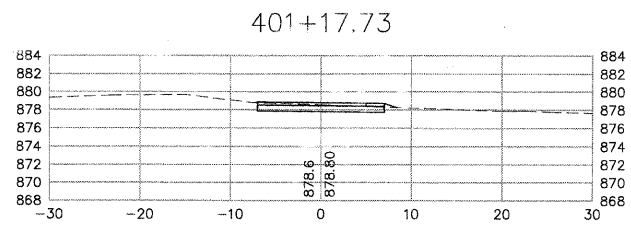
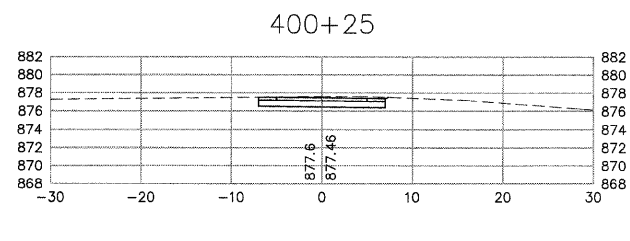
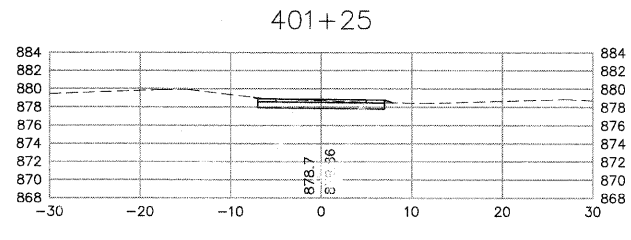
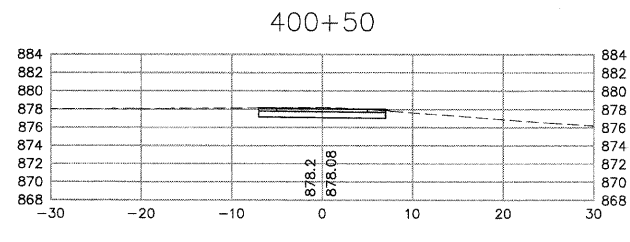
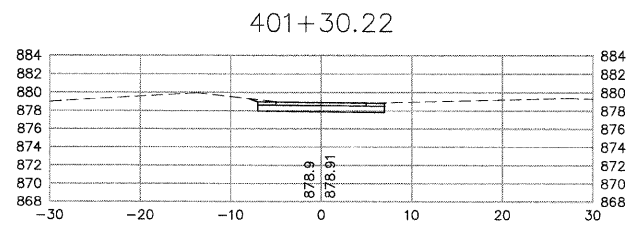
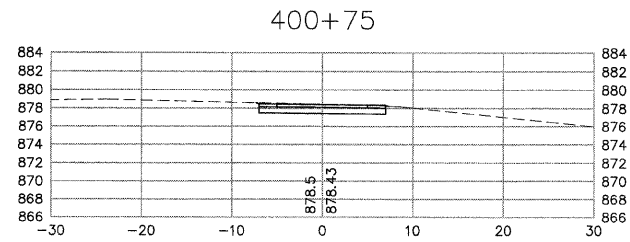
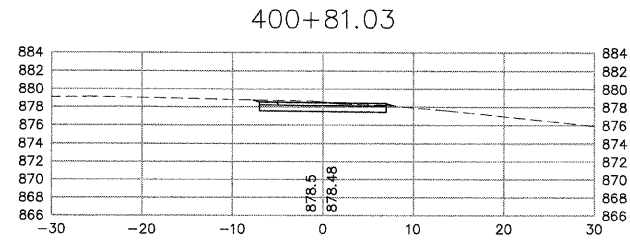
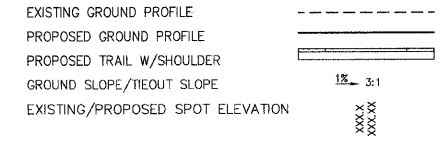
CROSS SECTIONS
STA. 300+00 TO STA. 300+82.50

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	45
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

1. THE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-56 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

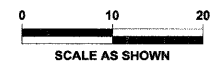
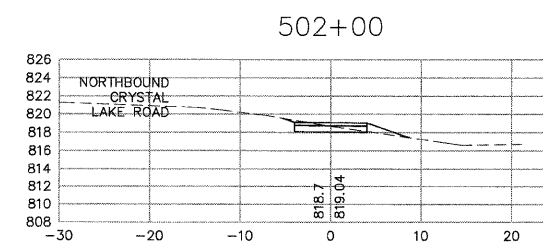
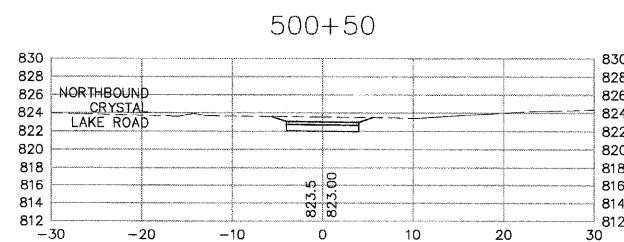
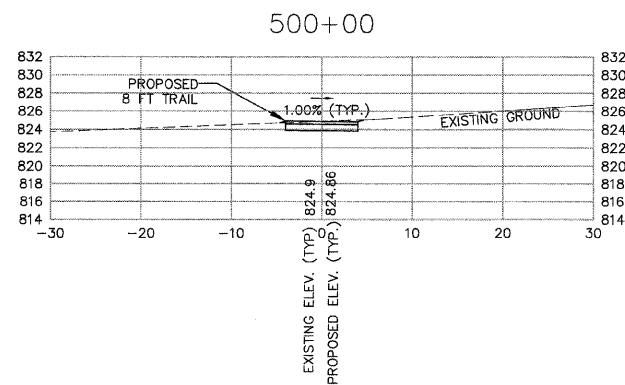
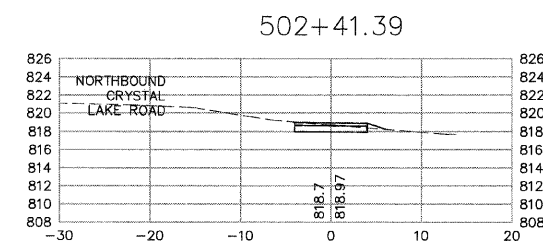
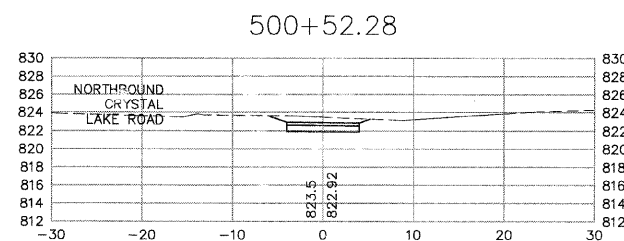
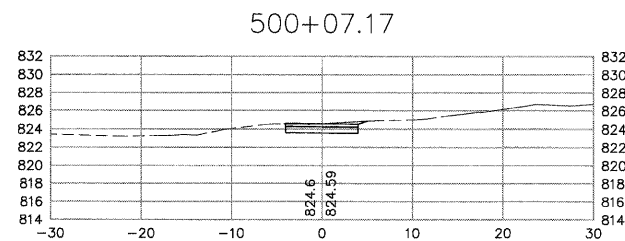
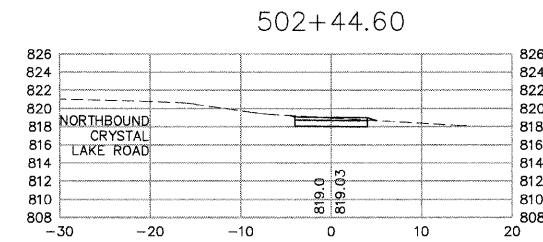
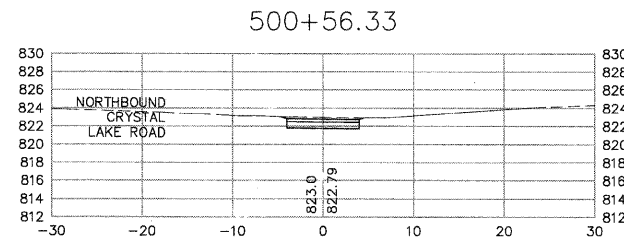
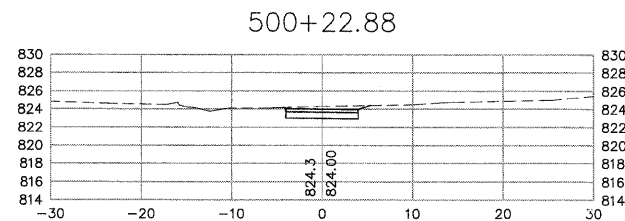
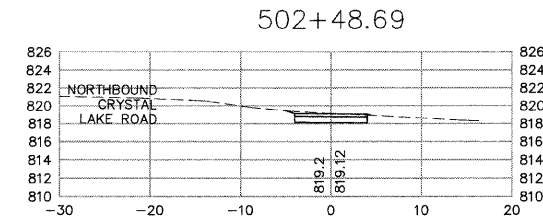
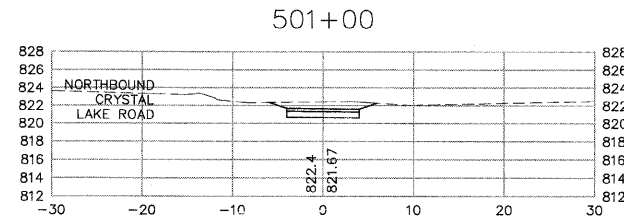
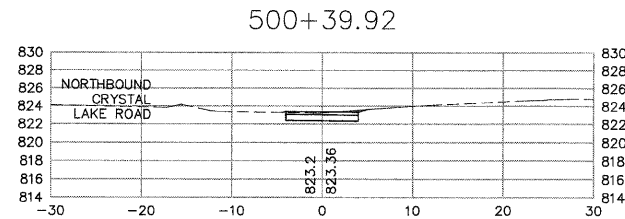
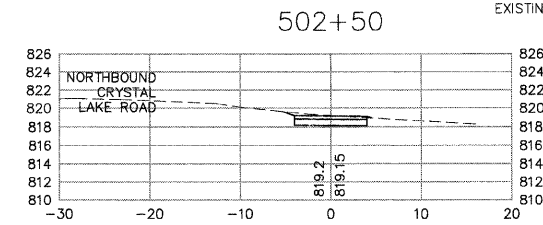
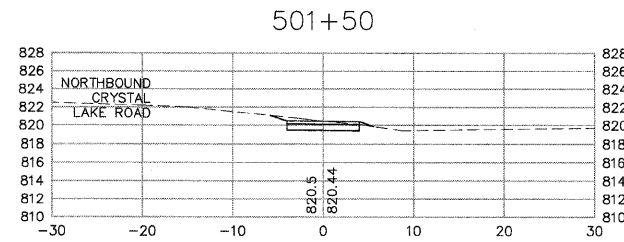
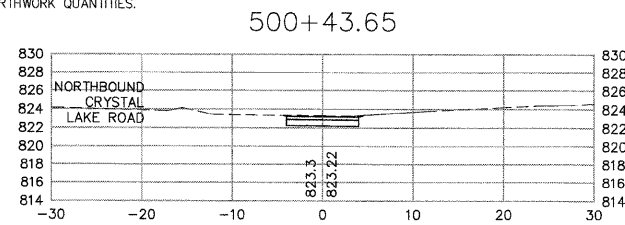
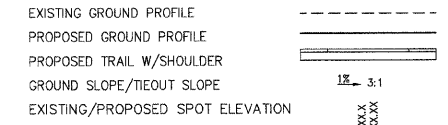
CROSS SECTIONS
STA. 400+00 TO STA. 401+30.22

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	46
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS: 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00, 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:



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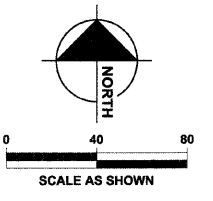
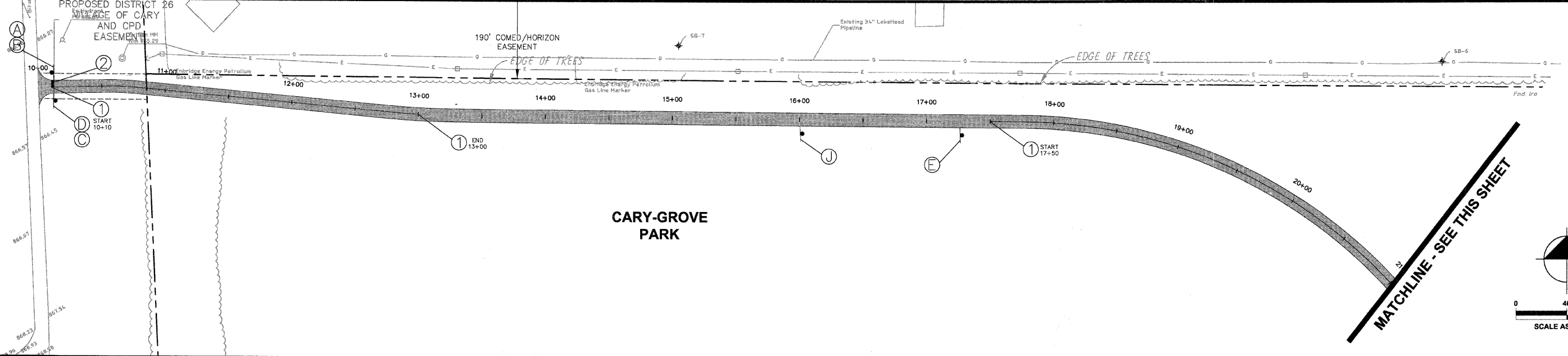
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**CROSS SECTIONS
STA. 500+00 TO STA. 502+50**

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	47
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



SIGNING NOTES:

1. ALL PROPOSED SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MOST CURRENT EDITION OF EACH OF THE FOLLOWING DOCUMENTS AND ANY REVISION THERETO:
 - A. MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
 - B. ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
2. PROPOSED SIGN LOCATIONS ARE APPROXIMATE AND SHALL BE MODIFIED IN THE FIELD TO AVOID CONFLICT WITH UNDERGROUND UTILITIES OR OTHER OBSTRUCTIONS AND TO COMPLY WITH THE STANDARDS REFERENCED IN NOTE 1 ABOVE. SIGNS SHALL BE INSTALLED ONLY AFTER APPROVAL OF FINAL LOCATIONS BY CARY PARK DISTRICT.
3. CROSSWALK LONGITUDINAL LINES TO BE 6" WIDE. CROSSWALK DIAGONAL LINES TO BE 12" WIDE WITH 2' SPACING BETWEEN LINES. STOP BARS SHALL BE PLACED A MINIMUM OF 4' BEHIND CROSSWALKS.
4. SIGNS LISTED AND LABELED IN THE SIGN LEGEND ARE UNIQUE TO THE ALIGNMENTS SHOWN ON THIS SHEET.
5. PLACE SIGN A MINIMUM OF 3 FEET AND MAXIMUM OF 6 FEET FROM EDGE OF TRAIL.

SIGN LEGEND

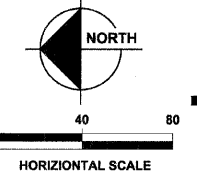
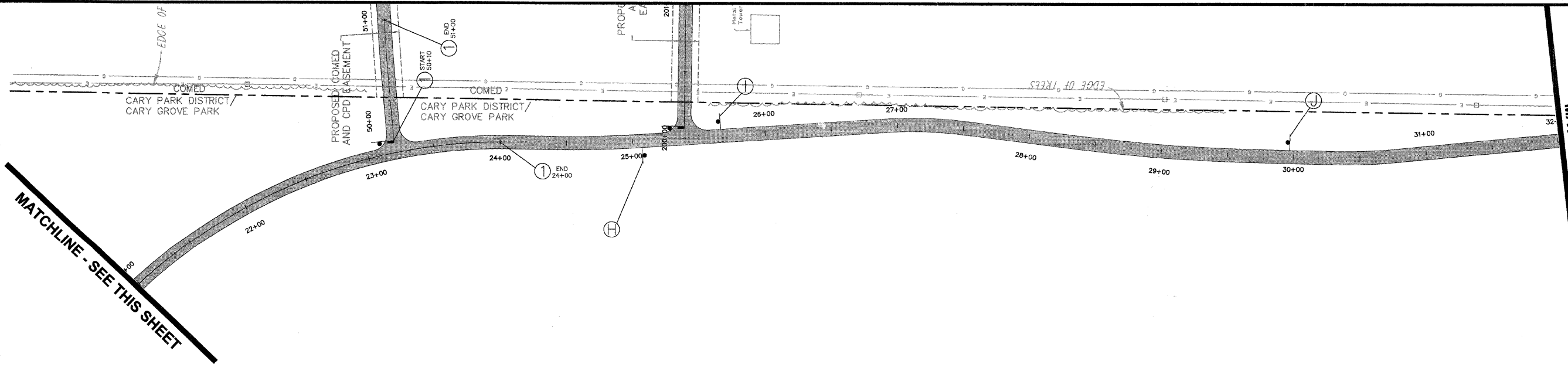
SIGN LETTER	(A)	(B)	(C)	(D)	(E)	(F)	(H)	(I)	(J)
SIGN									
NO.	R1-1	D1 - 1b	D11-1	M4-11	W1-2	R1-2	D1 - 1b	D1 - 1b	D1-1c
SIGN DIM.	18" x 18"	24" x 12" D4 LETTER SIZE	24" x 18"	12" x 4"	18" x 18"	18" x 18" x 18"	24" x 6"	24" x 6"	24" x 18"

SIGNING LEGEND

- PLACE NEW SIGN
- DETECTABLE WARNINGS

PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING, 4" (100 mm) WHITE
- ② THERMOPLASTIC PAVEMENT MARKING, 24" (600 mm) WHITE
- ③ THERMOPLASTIC PAVEMENT MARKING, 6" (150 mm) WHITE
- ④ THERMOPLASTIC PAVEMENT MARKING, 12" (300 mm) WHITE



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DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

SIGNAGE AND STRIPING PLAN
STA. 10+00 TO STA. 21+00
STA. 21+00 TO 32+00

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	49
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

SIGNING NOTES:

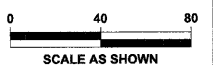
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5. PLACE SIGN A MINIMUM OF 3 FEET AND MAXIMUM OF 6 FEET FROM EDGE OF TRAIL.

SIGNING LEGEND

- (X) PLACE NEW SIGN
- DETECTABLE WARNINGS

PAVEMENT MARKING LEGEND

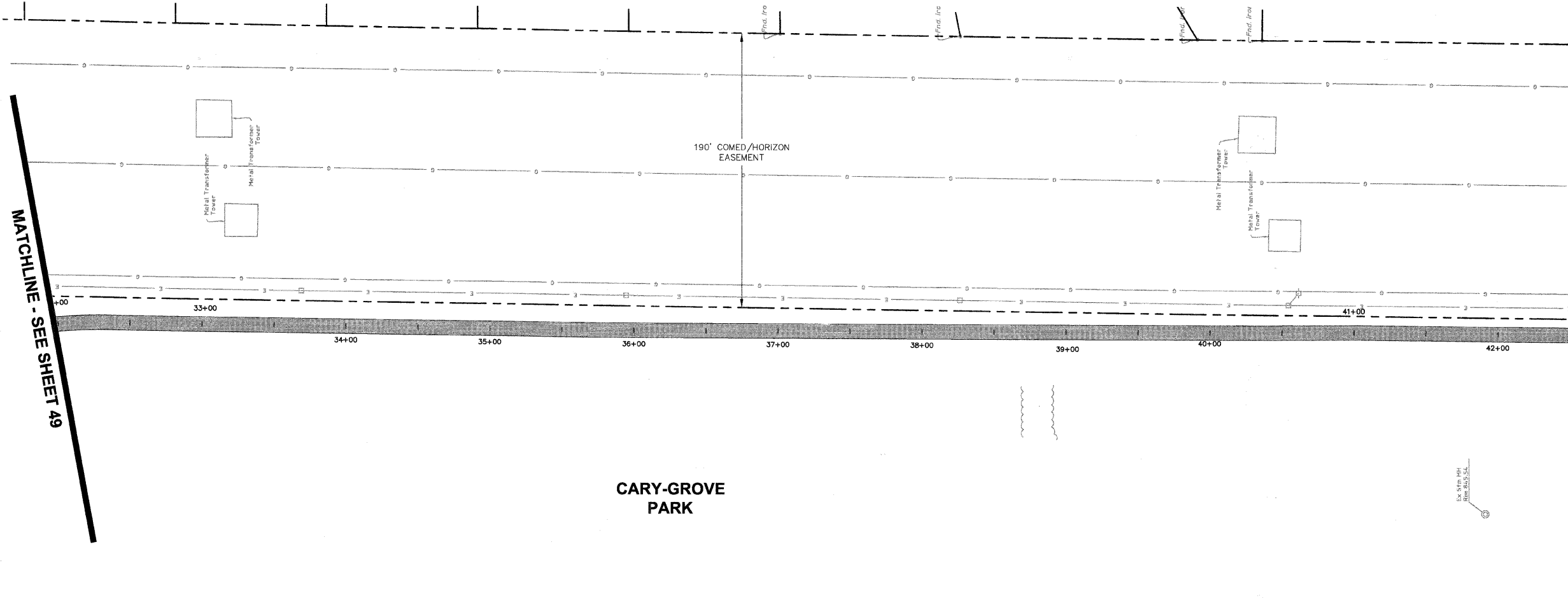
- ① THERMOPLASTIC PAVEMENT MARKING, 4" (100 mm) WHITE
- ② THERMOPLASTIC PAVEMENT MARKING, 24" (600 mm) WHITE
- ③ THERMOPLASTIC PAVEMENT MARKING, 6" (150 mm) WHITE
- ④ THERMOPLASTIC PAVEMENT MARKING, 12" (300 mm) WHITE



MATCHLINE - SEE SHEET 49

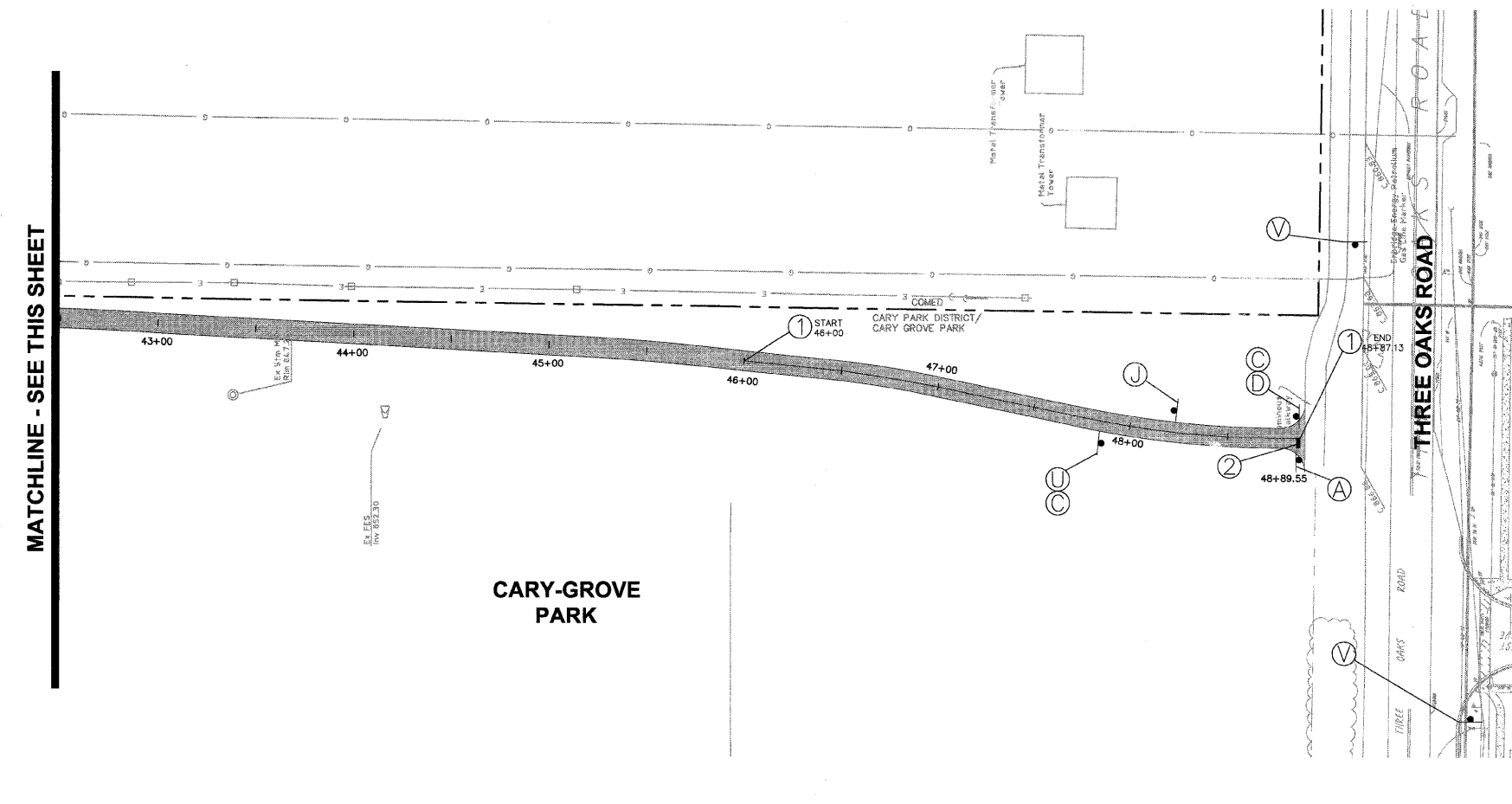
MATCHLINE - SEE THIS SHEET

CARY-GROVE PARK



MATCHLINE - SEE THIS SHEET

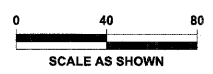
CARY-GROVE PARK



SIGN LEGEND

SIGN LETTER	(A)	(C)	(D)	(J)	(U)
SIGN					
NO.	R1-1	D11-1	M4-11	D1-1c	M4-12
SIGN DIM.	18" x 18"	24" x 18"	12" x 4"	24" x 18"	12" x 4"

SIGN LETTER	(V)
SIGN	
NO.	W21-1104
SIGN DIM.	18" x 18"



This document, together with the concepts and design presented herein, is an instrument of service, to be used only for the specific purpose and client for which it was prepared. Return drawings to: K. O. LEE, Inc., 1000 N. Lincoln Ave., Suite 100, Chicago, IL 60610. Date: 10/02/2009. No other use or reproduction is permitted without the written consent of K. O. LEE, Inc.

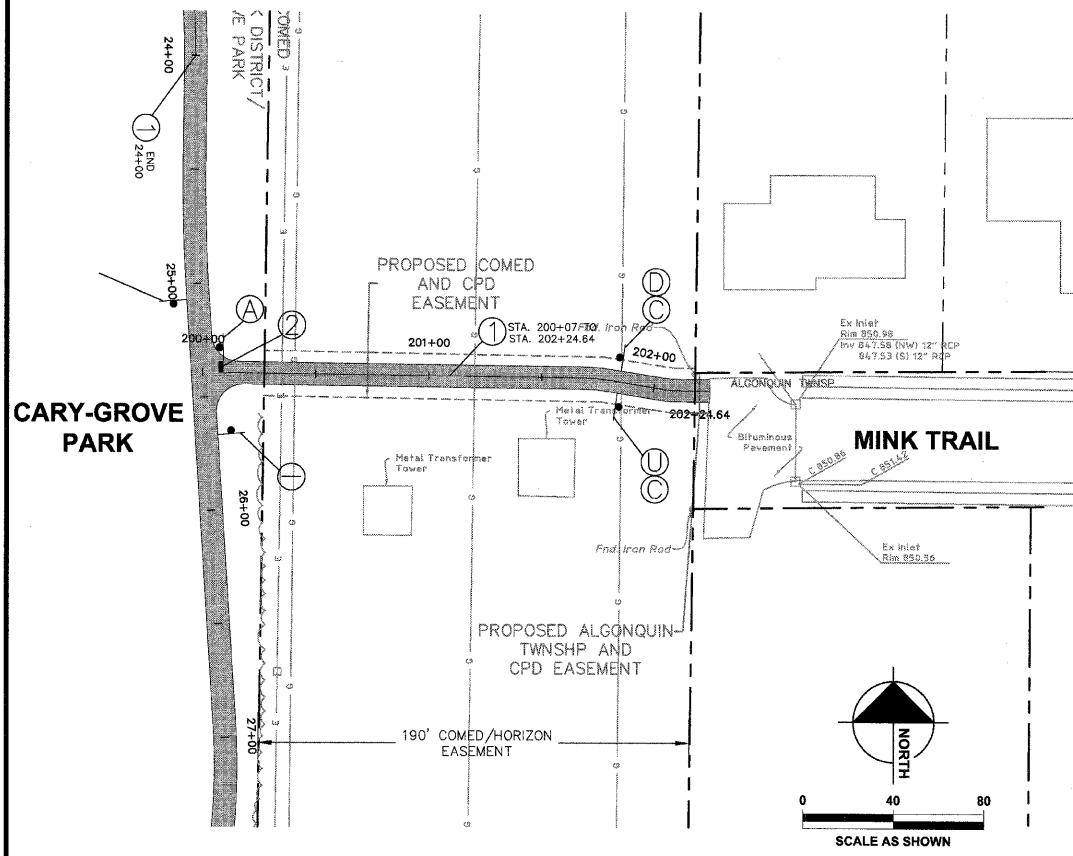
DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

SIGNAGE AND STRIPING PLAN
STA. 32+00 TO STA. 42+50
STA. 42+50 TO 48+89.55

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	50
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



SIGN LEGEND

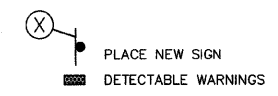
SIGN LETTER	(A)	(C)	(D)	(G)
SIGN				
NO.	R1-1	D11-1	M4-11	X4-11
SIGN DIM.	18" x 18"	24" x 18"	12" x 4"	18" x 18"

SIGN LETTER	(F)	(K)	(L)	(U)	(W)
SIGN					
NO.	R1-2	W3-2a	D1-1c	M4-12	W14-12
SIGN DIM.	18" x 18" x 18"	18" x 18"	24" x 6" D4 LETTER SIZE	12" x 4"	30" x 30"

SIGNING NOTES:

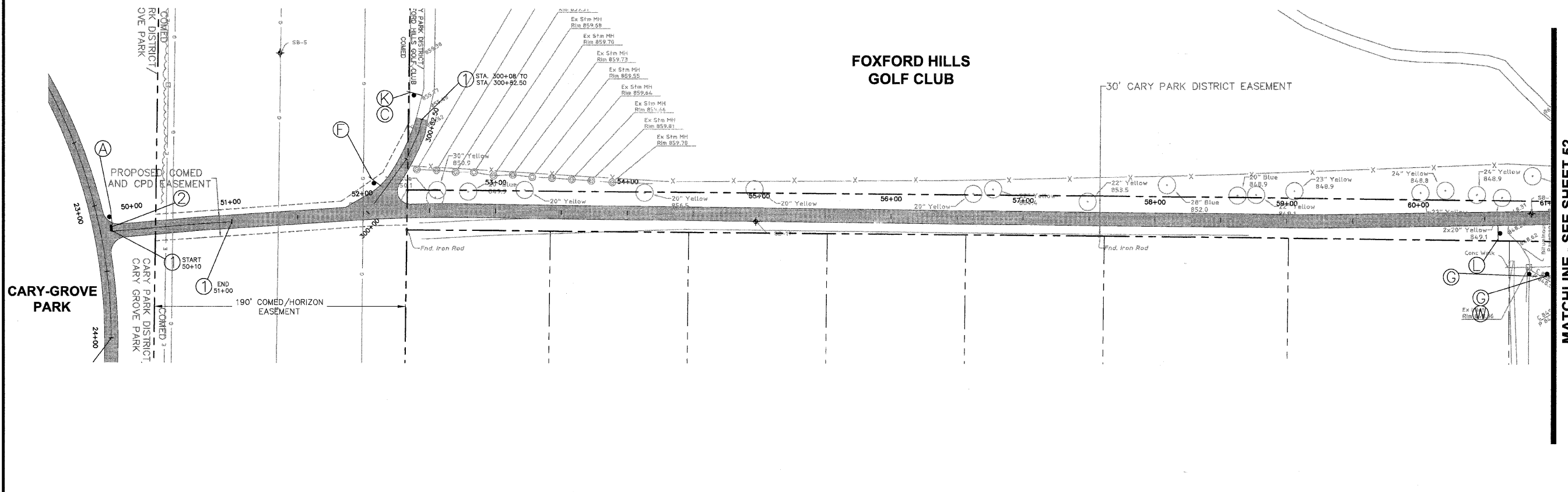
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SIGNING LEGEND

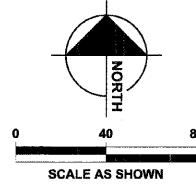


PAVEMENT MARKING LEGEND

- THERMOPLASTIC PAVEMENT MARKING, 4" (100 mm) WHITE
- THERMOPLASTIC PAVEMENT MARKING, 24" (600 mm) WHITE
- THERMOPLASTIC PAVEMENT MARKING, 6" (150 mm) WHITE
- THERMOPLASTIC PAVEMENT MARKING, 12" (300 mm) WHITE



MATCHLINE - SEE SHEET 52



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DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

SIGNAGE AND STRIPING PLAN
STA. 200+00 TO STA. 202+24.64
STA. 300+00 TO STA. 300+82.50
STA. 50+00 TO STA. 61+00

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	51
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

SIGNING NOTES:

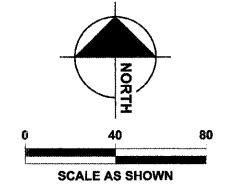
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SIGNING LEGEND

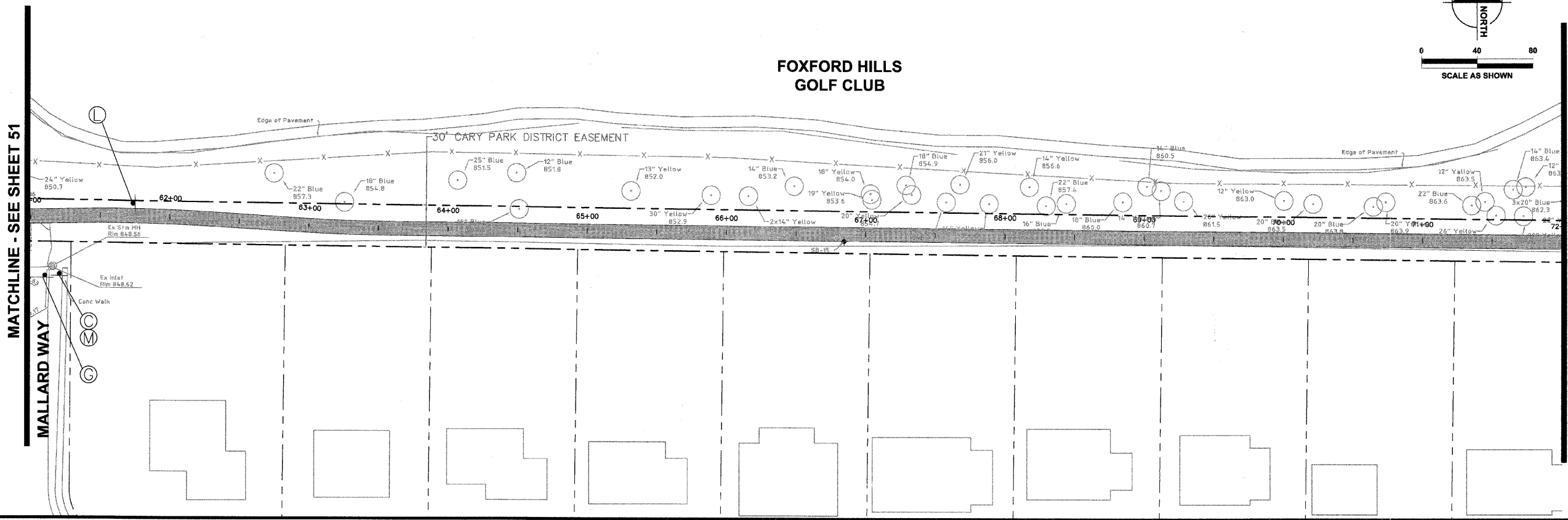


PAVEMENT MARKING LEGEND

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- THERMOPLASTIC PAVEMENT MARKING, 24" (600 mm) WHITE
- THERMOPLASTIC PAVEMENT MARKING, 6" (150 mm) WHITE
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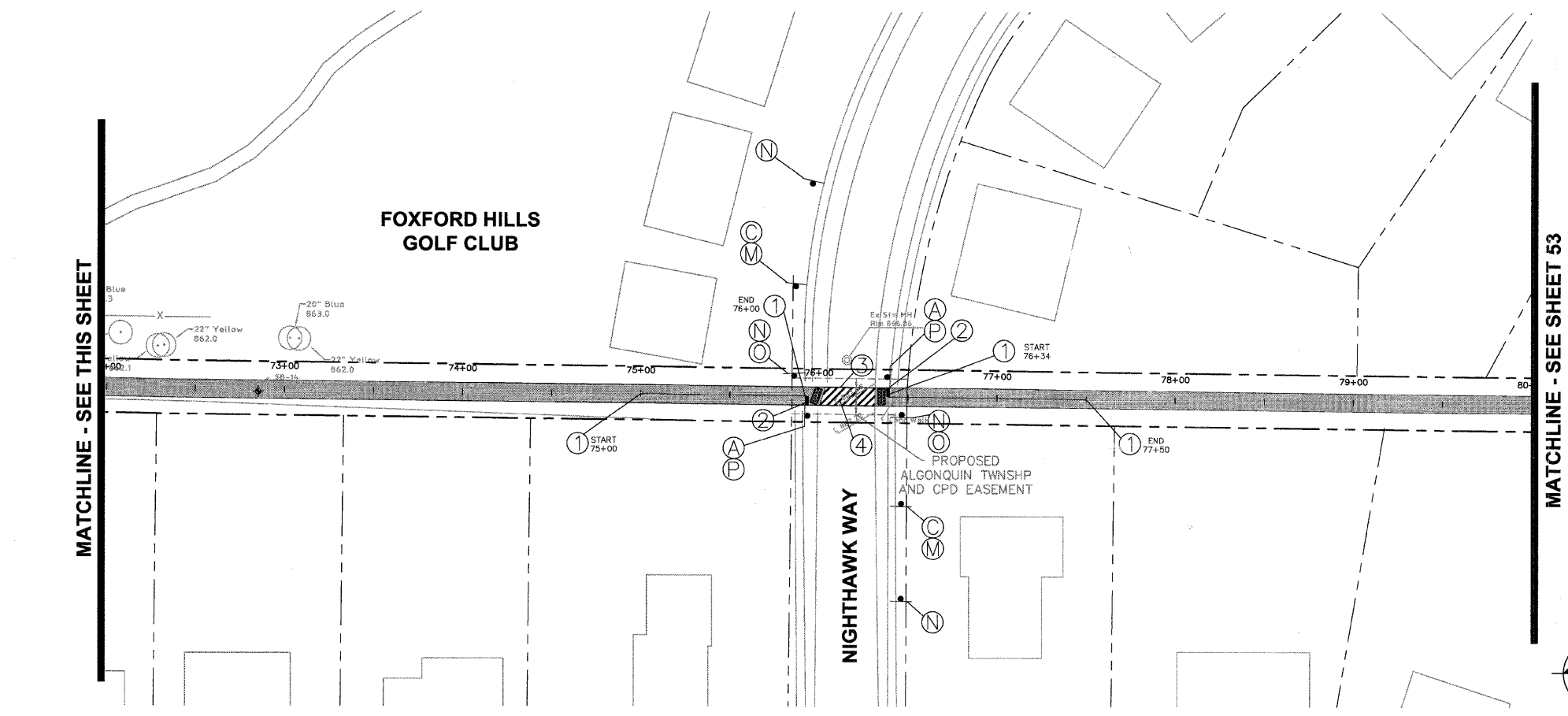
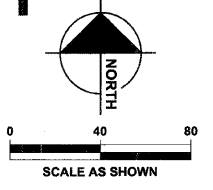
FOXFORD HILLS GOLF CLUB



SIGN LEGEND

SIGN LETTER	A	C	F	G	L
SIGN					
NO.	R1-1	D11-1	R1-2	X4-11	D1-1c
SIGN DIM.	18" x 18"	24" x 18"	18" x 18" x 18"	18" x 18"	24" x 6" D4 LETTER SIZE

SIGN LETTER	M	N	O	P
SIGN				
NO.	M7-5	W11-1	W16-7	D1-1c
SIGN DIM.	12" x 9"	18" x 18"	24" x 12"	24" x 6" D4 LETTER SIZE



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DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

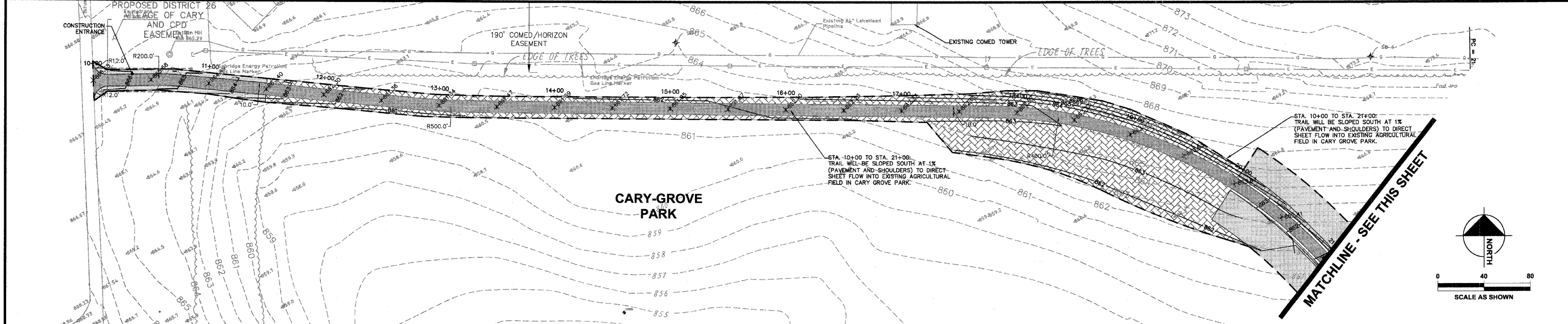
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

SIGNAGE AND STRIPING PLAN
STA. 61+00 TO STA. 72+00
STA. 72+00 TO STA. 80+00

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	52
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	





GRADING LEGEND:

- PROPOSED SPOT ELEVATIONS
- PROPOSED CONTOUR
- FLOW ARROW
- EXISTING CONTOUR
- EXISTING STORM SEWER
- PIPE OUTLET/CULVERT
- 4" PERFORATED EDGE DRAIN
- 12" PVC STORM PIPE
- EDGE DRAIN CLEANOUT
- IDOT CLASS 1 SEEDING*
- IDOT CLASS 1B SEEDING*
- IDOT CLASS 4A SEEDING*
- EXCELSIOR/WOOD BLANKET
- INLET PROTECTION
- SILT FENCE
- RIP RAP
- TREE PROTECTION
- PROPERTY LINE
- LIMITS OF DISTURBANCE (8.96 ACRES)

NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER SEEDING WHERE THERE IS NO EXCELSIOR BLANKET
 * SEE SHEET 2 FOR SEEDING CHARTS

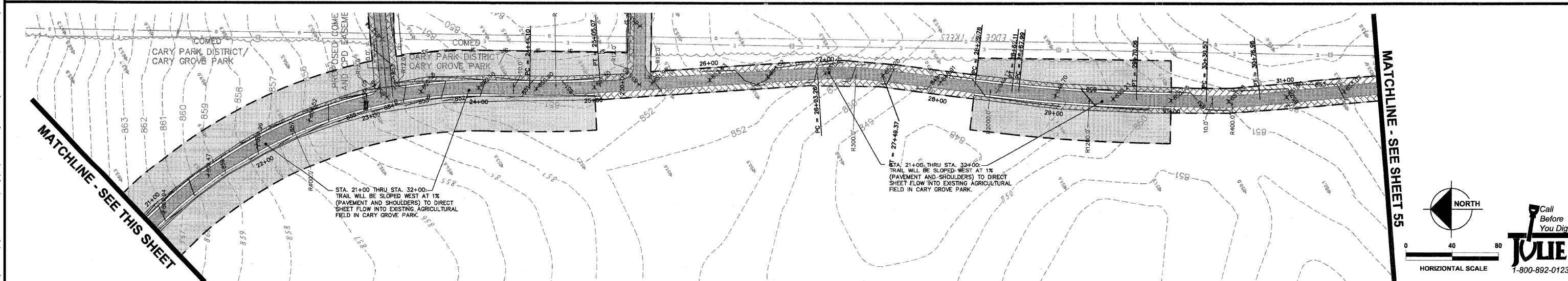
BENCHMARKS:

- SOURCE BENCHMARK**
 DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
 LOCATION: 120' NORTHEAST OF THE OAKMAN DRIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
 ELEVATION: 861.45
- SITE BENCHMARK**
 HE CP#100
 DESCRIPTION: IRON PIPE
 LOCATION: 1000' EAST OF INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
 ELEVATION: 854.94

GRADING NOTES:

1. PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION.
2. ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
3. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
4. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING GRADES SHOWN ON THESE PLANS ARE BASED OFF A TOPOGRAPHIC SURVEY BY HAEGER ENGINEERING DATED 02/07/2008, A TOPOGRAPHIC SURVEY AND FUTURE GRADING IMPROVEMENT PLAN PERFORMED BY HITCHCOCK ENGINEERING DATED 10/04/2004 AND A FOXFORD HILLS SUBDIVISION AS BUILT SURVEY BY MANHARD DATED 03/28/07.
5. CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
6. REFER TO GEOTECHNICAL REPORT FOR SPECIFIC SITE SOIL CONDITIONS AND CONSIDERATIONS.
7. CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING, AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
8. MULCH METHOD TO BE USED TO PROTECT ALL PERMANENT SEEDING WHEREVER THE EXCELSIOR BLANKET HAS NOT BEEN SPECIFIED.
9. WHERE APPLICABLE, CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP SEDIMENT FROM ENTERING ADJACENT RESIDENTIAL PROPERTIES.
10. CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO AVOID DISTURBING AREA OUTSIDE OF EXISTING CARY PARK DISTRICT PROPERTY, EASEMENTS, AND APPROVED CONSTRUCTION EASEMENTS WITH ADJACENT STAKE HOLDERS.
11. CONTRACTOR SHALL RESTORE ANY DISTURBED AREAS OUTSIDE OF CONSTRUCTION LIMITS TO PRE-EXISTING CONDITIONS AT NO ADDITIONAL COST TO THE OWNER.
12. ALL GRADING AND DRAINAGE IMPROVEMENTS WILL BE PERFORMED IN ACCORDANCE WITH MCHENRY COUNTY STORMWATER ORDINANCE AND IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
13. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE IN ACCORDANCE WITH THE APPROVED SWPPP, MCHENRY COUNTY, AND THE MCHENRY COUNTY SOIL CONSERVATION DISTRICT.
14. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.
15. PLEASE REFER TO STANDARDS AND DETAILS LOCATED ON SHEET 54-55 FOR CONSTRUCTION DETAILS.
16. SEE PLAN AND PROFILES ON PAGES 10-20 AND CROSS SECTIONS ON PAGES 21-43 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
17. PRIOR TO INSTALLING SOIL STABILIZATION MEASURES, CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT TO VERIFY STABILIZATION TYPE (STRAW MULCH OR EXCELSIOR BLANKET) TO BE USED.
18. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
19. PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.

20. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
21. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
22. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
23. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
24. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
25. A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
26. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
27. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
28. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
29. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.



DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

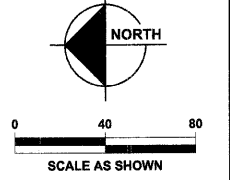
**CARY PARK DISTRICT
 CARY COMMUNITY TRAIL**

EROSION CONTROL AND GRADING PLAN
 STA. 10+00 TO STA. 21+00
 STA. 21+00 TO 32+00

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	54
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

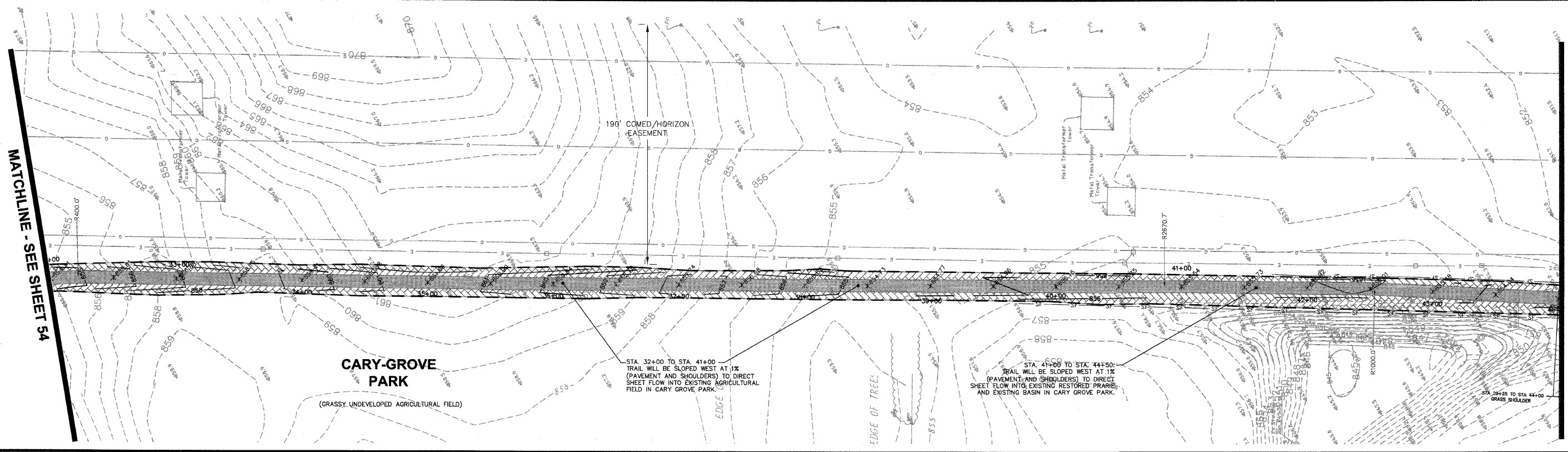
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MATCHLINE - SEE SHEET 54

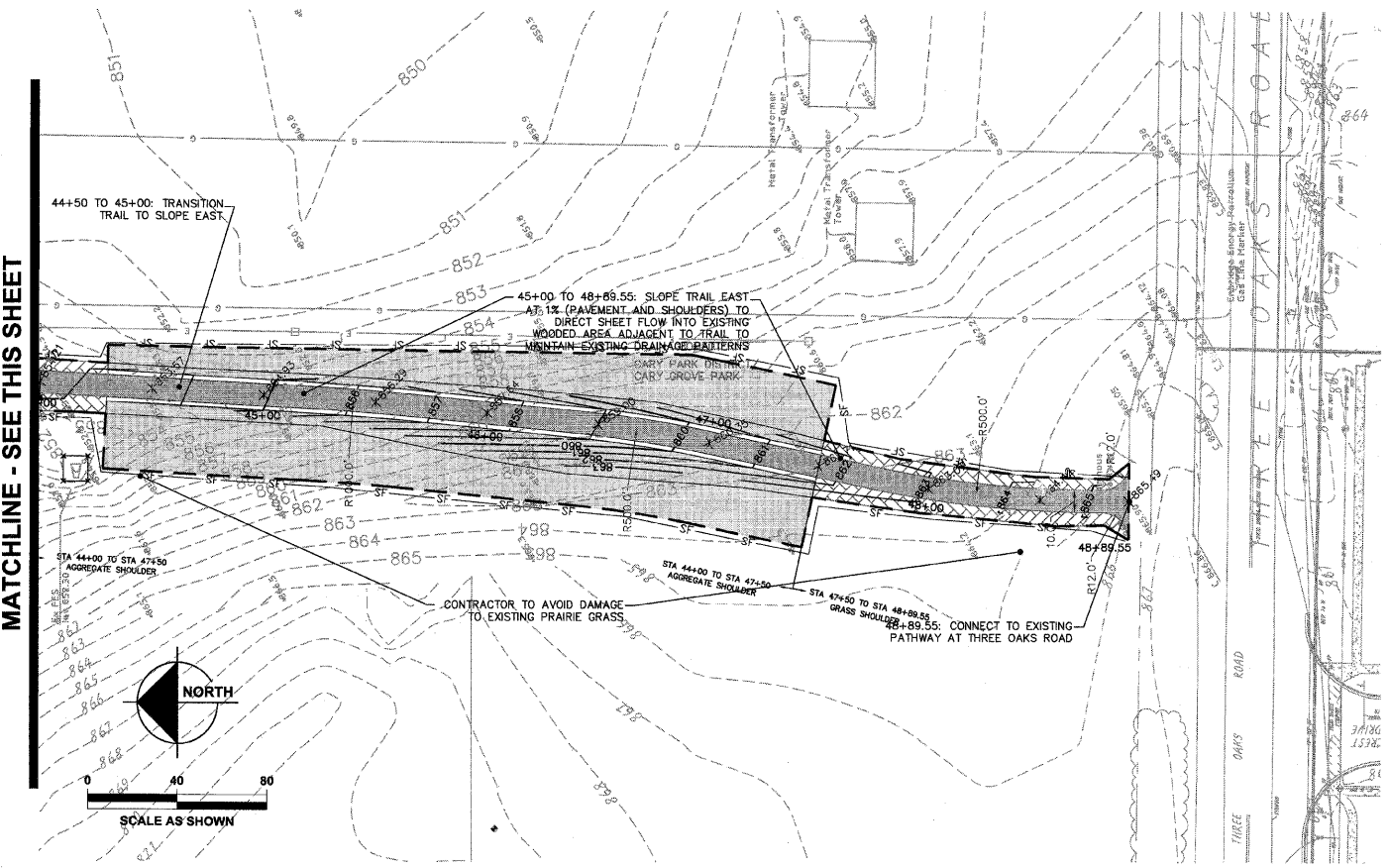
MATCHLINE - SEE THIS SHEET



CARY-GROVE PARK
(GRASSY UNDEVELOPED AGRICULTURAL FIELD)

STA. 32+00 TO STA. 41+00
TRAIL WILL BE SLOPED WEST AT 1%
(PAVEMENT AND SHOULDERS) TO DIRECT
SHEET FLOW INTO EXISTING RESTORED PRAIRIE
FIELD IN CARY GROVE PARK.

STA. 41+00 TO STA. 44+00
TRAIL WILL BE SLOPED WEST AT 1%
(PAVEMENT AND SHOULDERS) TO DIRECT
SHEET FLOW INTO EXISTING RESTORED PRAIRIE
AND EXISTING BASIN IN CARY GROVE PARK.



GRADING LEGEND:

- PROPOSED SPOT ELEVATIONS
- PROPOSED CONTOUR
- FLOW ARROW
- EXISTING CONTOUR
- EXISTING STORM SEWER
- PIPE OUTLET/CULVERT
- 4" PERFORATED EDGE DRAIN
- 12" PVC STORM PIPE
- EDGE DRAIN CLEANOUT
- IDOT CLASS 1 SEEDING*
- IDOT CLASS 1B SEEDING*
- IDOT CLASS 4A SEEDING*
- EXCELSIOR/WOOD BLANKET
- INLET PROTECTION
- SILT FENCE
- RIP RAP
- TREE PROTECTION
- PROPERTY LINE
- LIMITS OF DISTURBANCE (8.96 ACRES)

NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER SEEDING WHERE THERE IS NO EXCELSIOR BLANKET
* SEE SHEET 2 FOR SEEDING CHARTS

GRADING NOTES:

1. PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION.
2. ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEEDED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
3. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.

4. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING GRADES SHOWN ON THESE PLANS ARE BASED OFF A TOPOGRAPHIC SURVEY BY HAEGER ENGINEERING DATED 02/07/2008, A TOPOGRAPHIC SURVEY AND FUTURE GRADING IMPROVEMENT PLAN PERFORMED BY HITCHCOCK ENGINEERING DATED 10/04/2004 AND A FOXFORD HILLS SUBDIVISION AS BUILT SURVEY BY MANHARD DATED 03/28/07.
5. CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
6. REFER TO GEOTECHNICAL REPORT FOR SPECIFIC SITE SOIL CONDITIONS AND CONSIDERATIONS.
7. CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING, AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
8. WHERE APPLICABLE, CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP SEDIMENT FROM ENTERING ADJACENT RESIDENTIAL PROPERTIES.
9. CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO AVOID DISTURBING AREA OUTSIDE OF EXISTING CARY PARK DISTRICT PROPERTY, EASEMENTS AND APPROVED CONSTRUCTION EASEMENTS WITH ADJACENT STAKE HOLDERS.
10. CONTRACTOR SHALL RESTORE ANY DISTURBED AREAS OUTSIDE OF CONSTRUCTION LIMITS TO PRE-EXISTING CONDITIONS AT NO ADDITIONAL COST TO THE OWNER.
11. ALL GRADING AND DRAINAGE IMPROVEMENTS WILL BE PERFORMED IN ACCORDANCE WITH MCHENRY COUNTY STORMWATER ORDINANCE AND IDOT STANDARDS AND SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
12. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE IN ACCORDANCE WITH THE APPROVED SWPPP, MCHENRY COUNTY, AND THE MCHENRY COUNTY SOIL CONSERVATION DISTRICT.
13. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.
14. PLEASE REFER TO STANDARDS AND DETAILS LOCATED ON SHEET 59-63 FOR CONSTRUCTION DETAILS.
15. SEE PLAN AND PROFILES ON PAGES 15-25 AND CROSS SECTIONS ON PAGES 26-48 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
16. PRIOR TO INSTALLING SOIL STABILIZATION MEASURES, CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT TO VERIFY STABILIZATION TYPE (STRAW MULCH OR EXCELSIOR BLANKET) TO BE USED.
17. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.

18. PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.
19. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
20. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
21. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
22. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
23. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (3) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
24. A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
25. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
26. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
27. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
28. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.
29. MULCH METHOD TO BE USED TO PROTECT ALL PERMANENT SEEDING WHEREVER THE EXCELSIOR BLANKET HAS NOT BEEN SPECIFIED.

BENCHMARKS:

SOURCE BENCHMARK
DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
LOCATION: 120' NORTHEAST OF THE OAKMAN DIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
ELEVATION: 861.45

SITE BENCHMARK
HE CP#100
DESCRIPTION: IRON PIPE
LOCATION: 1000' EAST OF INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.
ELEVATION: 854.94

This document, together with the contracts and design specifications, is intended only for the specific purpose and site for which it was prepared. It is not to be used for any other purpose without the written consent of the engineer. The engineer and his firm shall not be held responsible for any errors or omissions in this document. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities. The user of this document shall be responsible for obtaining all necessary permits and approvals from the appropriate authorities.

DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

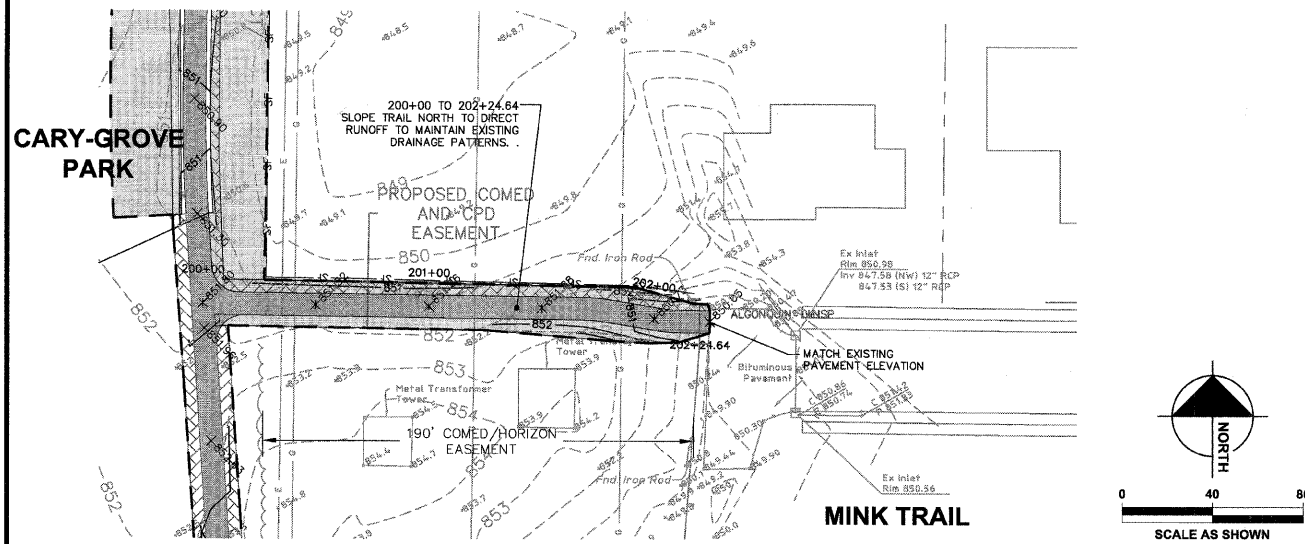
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

EROSION CONTROL AND GRADING PLAN
STA. 32+00 TO STA. 44+00
STA. 44+00 TO 48+89.55

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	55
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	





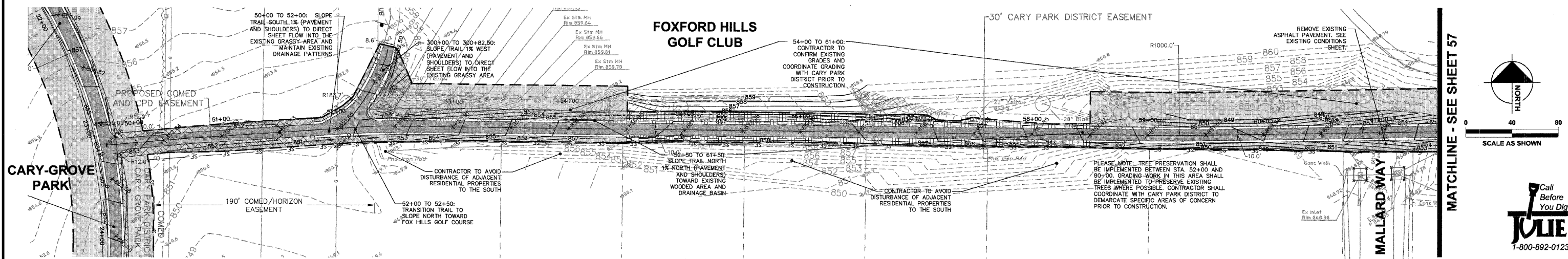
GRADING LEGEND:

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- PROPOSED CONTOUR
- FLOW ARROW
- EXISTING CONTOUR
- EXISTING STORM SEWER
- PIPE OUTLET/CULVERT
- 4" PERFORATED EDGE DRAIN
- 12" PVC STORM PIPE
- EDGE DRAIN CLEANOUT
- IDOT CLASS 1 SEEDING*
- IDOT CLASS 1B SEEDING*
- IDOT CLASS 4A SEEDING*
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- INLET PROTECTION
- SILT FENCE
- RIP RAP
- TREE PROTECTION
- PROPERTY LINE
- LIMITS OF DISTURBANCE (8.96 ACRES)

NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER SEEDING WHERE THERE IS NO EXCELSIOR BLANKET
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29. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.



DESIGNED	- JWH	REVISED	
DRAWN	- LES	REVISED	
CHECKED	- JMM	REVISED	
DATE	- 10/02/2009	REVISED	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
 CARY COMMUNITY TRAIL

EROSION CONTROL AND GRADING PLAN
 STA. 200+00 TO STA. 202+24.64
 STA. 300+00 TO STA. 300+82.5
 STA. 50+00 TO STA. 61+50

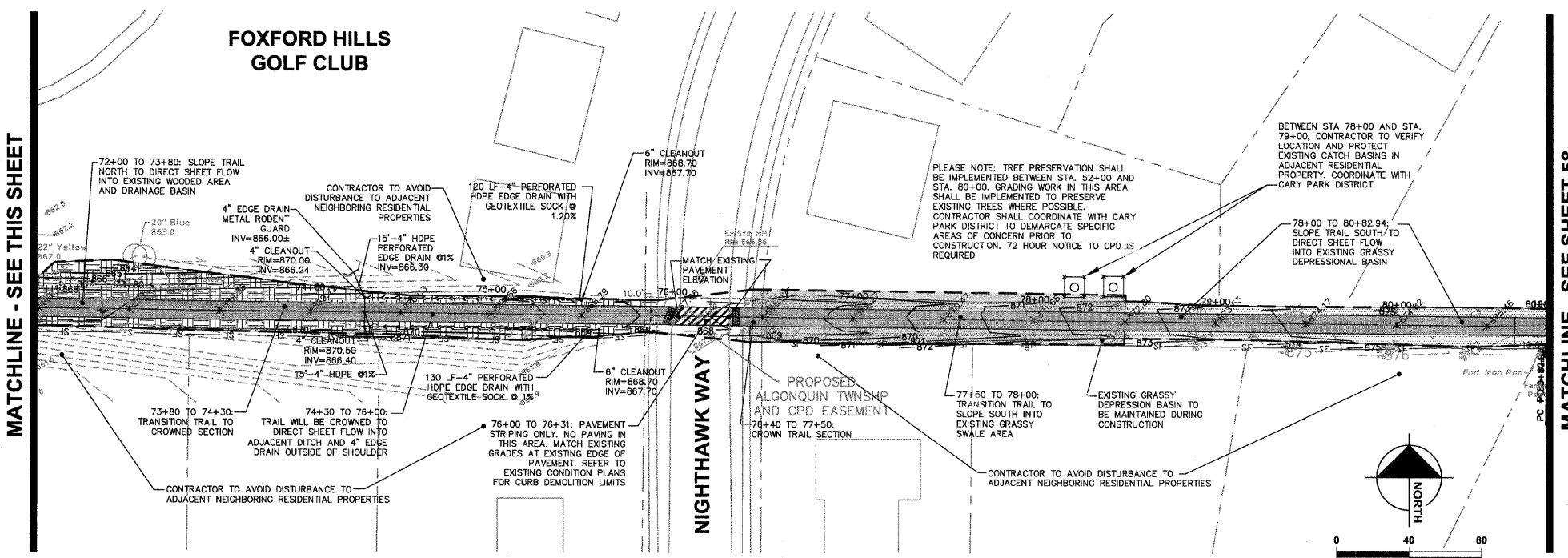
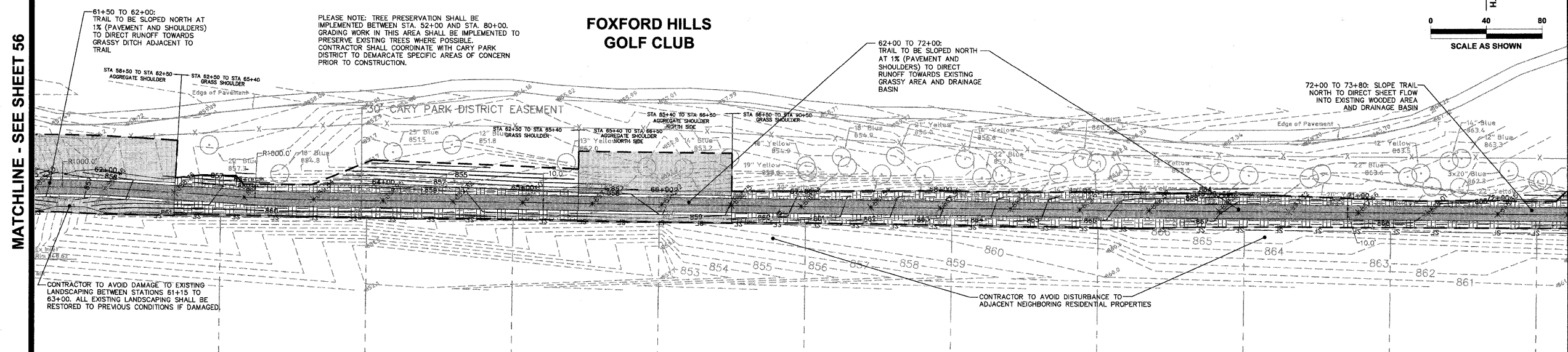
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	56
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse or modification of this document for any other project without the written consent of the engineer is strictly prohibited.

MATCHLINE - SEE SHEET 57



- GRADING LEGEND:**
- X---X---X--- PROPOSED SPOT ELEVATIONS
 - 100--- PROPOSED CONTOUR
 - 100--- EXISTING CONTOUR
 - >--- FLOW ARROW
 - >--- EXISTING STORM SEWER
 - >--- PIPE OUTLET/CULVERT
 - >--- 4" PERFORATED EDGE DRAIN
 - >--- 12" PVC STORM PIPE
 - EDGE DRAIN CLEANOUT
 - [--- IDOT CLASS 1 SEEDING*
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 - [--- RIP RAP
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 - [--- PROPERTY LINE
 - [--- LIMITS OF DISTURBANCE (8.96 ACRES)
- NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER SEEDING WHERE THERE IS NO EXCELSIOR BLANKET
- * SEE SHEET 2 FOR SEEDING CHARTS



12. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE IN ACCORDANCE WITH THE APPROVED SWPPP, MCHENRY COUNTY, AND THE MCHENRY COUNTY SOIL CONSERVATION DISTRICT.
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22. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
23. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (3) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
24. A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
25. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
26. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
27. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
28. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.
29. ALL EDGE DRAIN PIPE USED SHALL MEET IDOT SECTION 1040.

GRADING NOTES:

1. PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION.
2. ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
3. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
4. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING GRADES SHOWN ON THESE PLANS ARE BASED OFF A TOPOGRAPHIC SURVEY BY HAEGE ENGINEERING DATED 02/07/2008, A TOPOGRAPHIC SURVEY AND FUTURE GRADING IMPROVEMENT PLAN PERFORMED BY HITCHCOCK ENGINEERING DATED 10/04/2004 AND A FOXFORD HILLS SUBDIVISION AS BUILT SURVEY BY MANHARD DATED 03/28/07.
5. CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
6. REFER TO GEOTECHNICAL REPORT FOR SPECIFIC SITE SOIL CONDITIONS AND CONSIDERATIONS.
7. CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING, AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
8. WHERE APPLICABLE, CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP SEDIMENT FROM ENTERING ADJACENT RESIDENTIAL PROPERTIES.
9. CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO AVOID DISTURBING AREA OUTSIDE OF EXISTING CARY PARK DISTRICT PROPERTY, EASEMENTS, AND APPROVED CONSTRUCTION EASEMENTS WITH ADJACENT STAKE HOLDERS.
10. CONTRACTOR SHALL RESTORE ANY DISTURBED AREAS OUTSIDE OF CONSTRUCTION LIMITS TO PRE-EXISTING CONDITIONS AT NO ADDITIONAL COST TO THE OWNER.
11. ALL GRADING AND DRAINAGE IMPROVEMENTS WILL BE PERFORMED IN ACCORDANCE WITH MCHENRY COUNTY STORMWATER ORDINANCE AND IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

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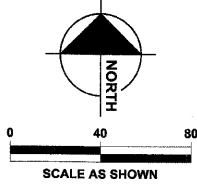
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DRAWN -- LES	REVISED
CHECKED -- JMM	REVISED
DATE -- 10/02/2009	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

EROSION CONTROL AND GRADING PLAN
STA. 60+50 TO STA. 72+50
STA. 71+50 TO STA. 80+82.94

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	57
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	





GRADING LEGEND:

- XXX.XX PROPOSED SPOT ELEVATIONS
- 100 PROPOSED CONTOUR
- FLOW ARROW
- 100 EXISTING CONTOUR
- EXISTING STORM SEWER
- ⌋ PIPE OUTLET/CULVERT
- 4" PERFORATED EDGE DRAIN
- 12" PVC STORM PIPE
- EDGE DRAIN CLEANOUT
- ▨ IDOT CLASS 1 SEEDING*
- ▨ IDOT CLASS 1B SEEDING*
- ▨ IDOT CLASS 4A SEEDING*
- ▨ EXCELSIOR/WOOD BLANKET
- INLET PROTECTION
- ST SILT FENCE
- ⊗ RIP RAP
- ⊙ TREE PROTECTION
- PROPERTY LINE
- LIMITS OF DISTURBANCE (8.96 ACRES)

NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER SEEDING WHERE THERE IS NO EXCELSIOR BLANKET
* SEE SHEET 2 FOR SEEDING CHARTS

BENCHMARKS:

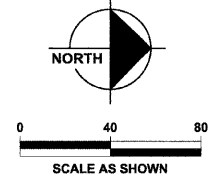
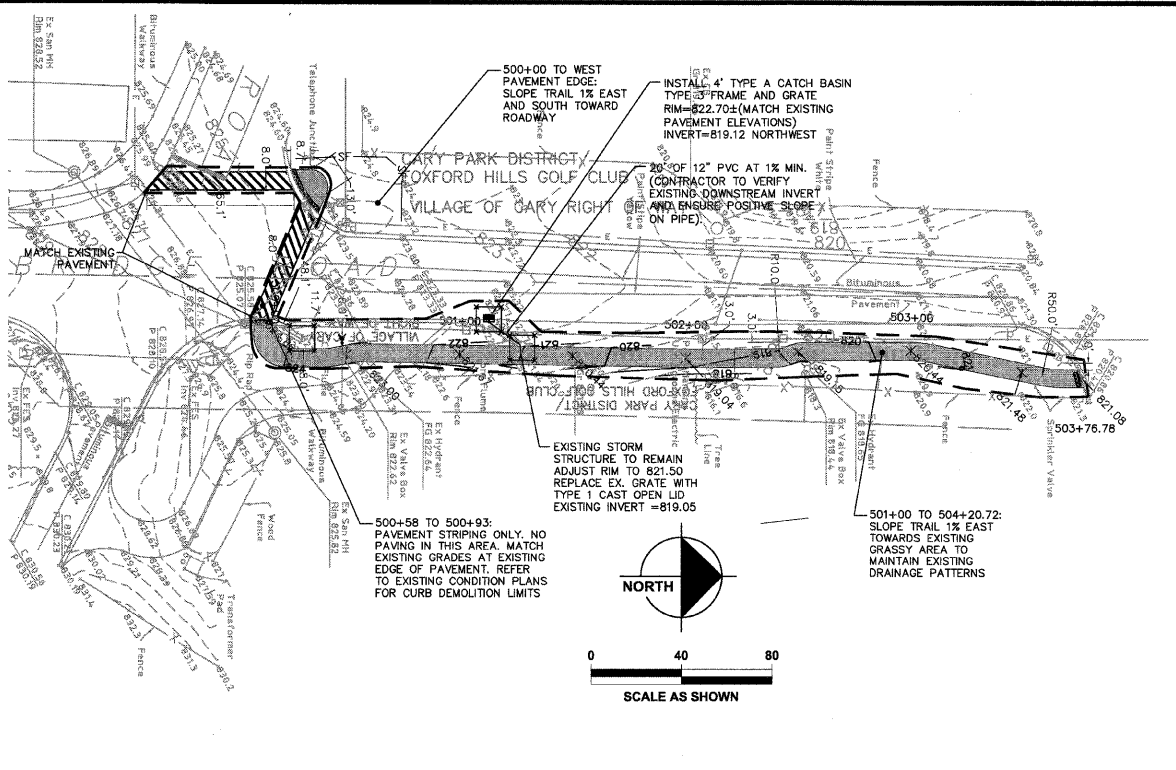
SOURCE BENCHMARK
DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.
LOCATION: 120' NORTHEAST OF THE OAKMAN DIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.
ELEVATION: 861.45

SITE BENCHMARK
HE CP #1
DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT LOCATION: 592' WEST OF INTERSECTION OF RAWSON BRIDGE ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 27' SOUTHEAST OF IRON ROD, AND 10' SOUTH OF CONCRETE SIDEWALK
ELEVATION: 860.97

SITE BENCHMARK
HE CP #8
DESCRIPTION: RAIL ROAD SPIKE LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHEAST OF FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND 11' NORTHWEST OF PAVEMENT.
ELEVATION: 823.23

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12. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE IN ACCORDANCE WITH THE APPROVED SWPPP, MCHENRY COUNTY, AND THE MCHENRY COUNTY SOIL CONSERVATION DISTRICT.
13. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.
14. PLEASE REFER TO STANDARDS AND DETAILS LOCATED ON SHEET 54-55 FOR CONSTRUCTION DETAILS.
15. SEE PLAN AND PROFILES ON PAGES 10-20 AND CROSS SECTIONS ON PAGES 21-43 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS
16. PRIOR TO INSTALLING SOIL STABILIZATION MEASURES, CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT TO VERIFY STABILIZATION TYPE (STRAW MULCH OR EXCELSIOR BLANKET) TO BE USED.
17. NOTES ON BRIDGING STABILIZATION METHODOLOGY:
 - REMOVE/SCRAPE 1-2" EXISTING GROUND
 - PLACE GEOTEXTILE FABRIC AS DIRECTED BY CARY PARK DISTRICT
 - INSTALL STONE IN APPROVED LIFTS TO BRING SECTION UP TO PROPOSED SUBGRADE ELEVATION.
 - INSTALL PAVEMENT SECTION UP TO FINAL GRADE. SEE PLAN/PROFILE FOR ELEVATIONS
 CONTRACTOR IS TO COORDINATE WITH CARY PARK DISTRICT PRIOR TO INSTALLATION OF THESE MATERIALS.
18. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
19. PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.
20. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
21. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
22. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
23. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
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DESIGNED	- JWH	REVISED	
DRAWN	- LES	REVISED	
CHECKED	- JMM	REVISED	
DATE	- 10/02/2009	REVISED	

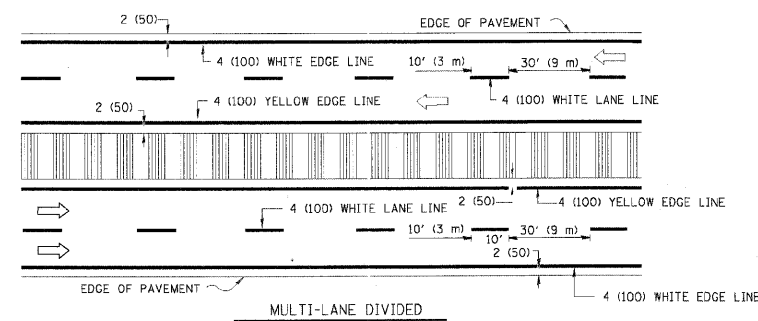
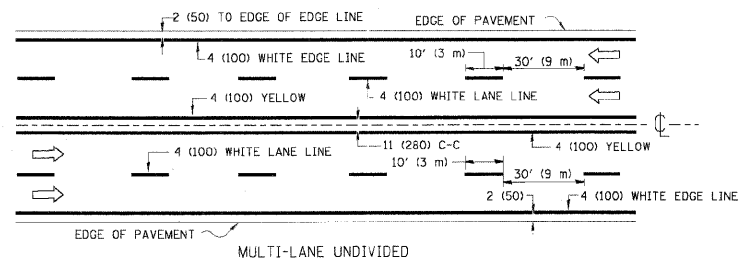
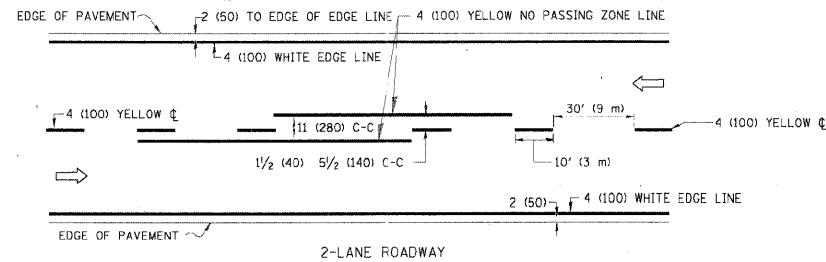
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

**EROSION CONTROL AND GRADING PLAN
STA. 80+82.94 TO STA. 92+76.57
STA. 111+00 TO 111+82.06
STA. 100+00 TO 102+21.12
STA. 500+00 TO 504+20.72**

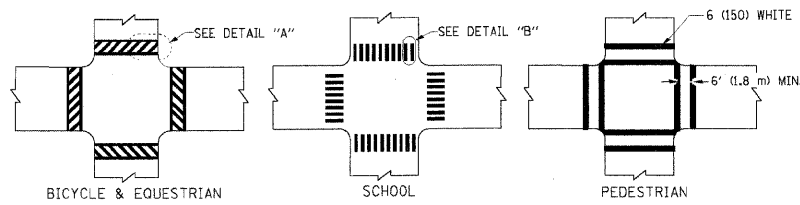
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	58
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



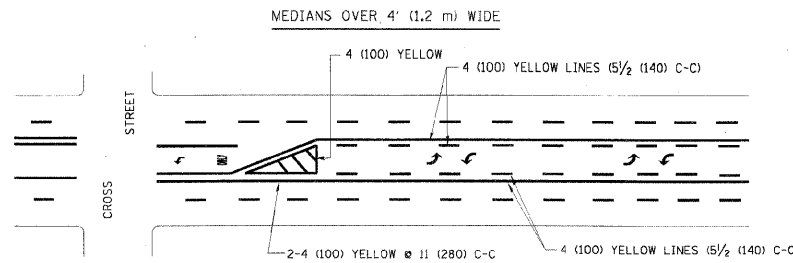
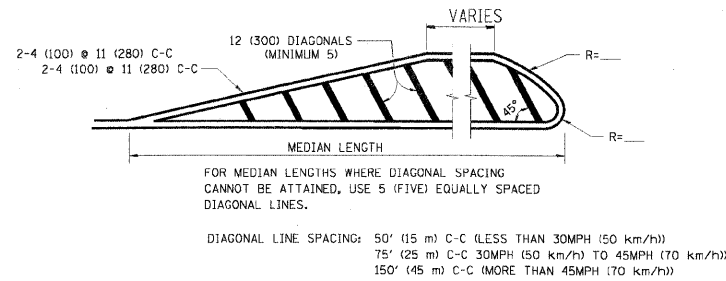
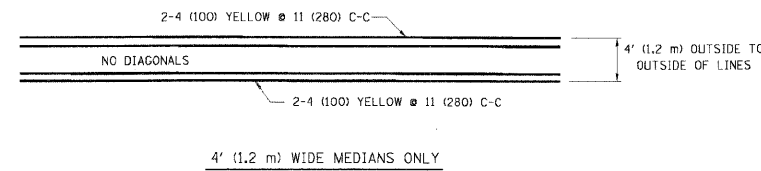


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

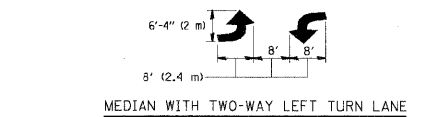
TYPICAL LANE AND EDGE LINE MARKING



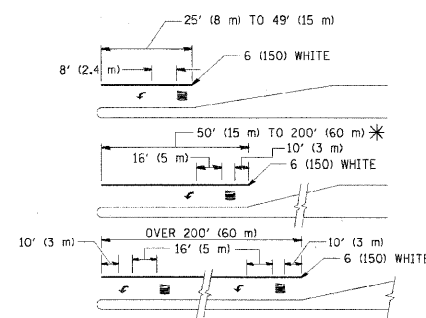
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

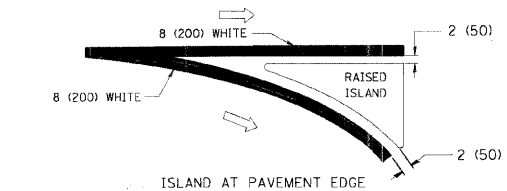
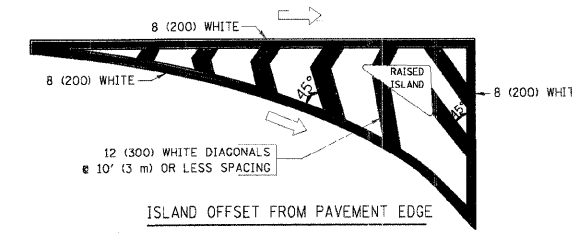


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in Inches (millimeters) unless otherwise shown.

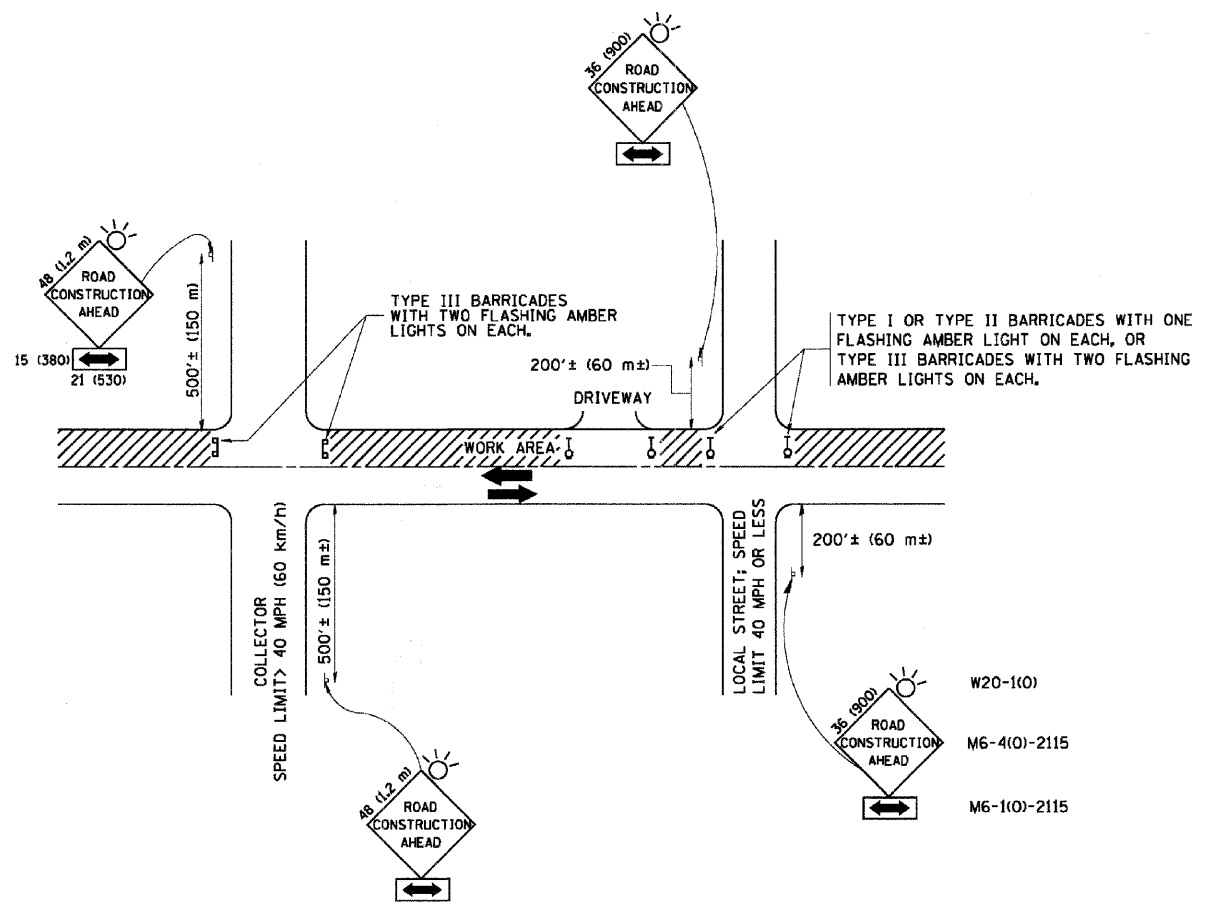
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PLT SCALE = 50.000 1/2" = 1"	PLT DATE = 9/9/2009	DRAWN -	REVISED - C. JUCIUS 09-09-09					SCALE: NONE				SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	TC-13 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
DATE - 03-19-90	REVISOR -	CHECKED -	REVISOR -																	

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DESIGNED - JWH	REVISED	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				CARY PARK DISTRICT CARY COMMUNITY TRAIL				DETAILS				SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN - LES	REVISED													06-P4001-00-BR	MCHENRY	64	59
CHECKED - JMM	REVISED									FED. ROAD DIST. NO. 1 ILLINOIS CONTRACT NO. 63333							
DATE - 10/02/2009	REVISED																



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TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\dists\d\22x34\tcl0.dgn	USER NAME = gaglianob	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN - LES	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 1 / IN.	CHECKED - JMM	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. _____	TO STA. _____

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-10			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO.	

DESIGNED -	JWH	REVISED	
DRAWN -	LES	REVISED	
CHECKED -	JMM	REVISED	
DATE -	10/02/2009	REVISED	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

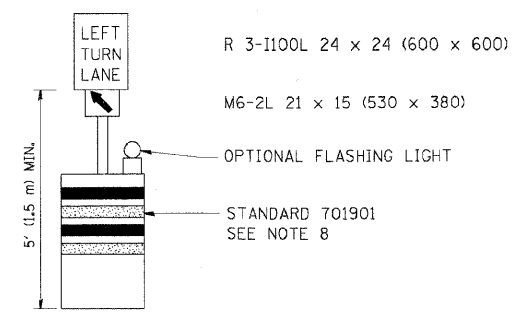
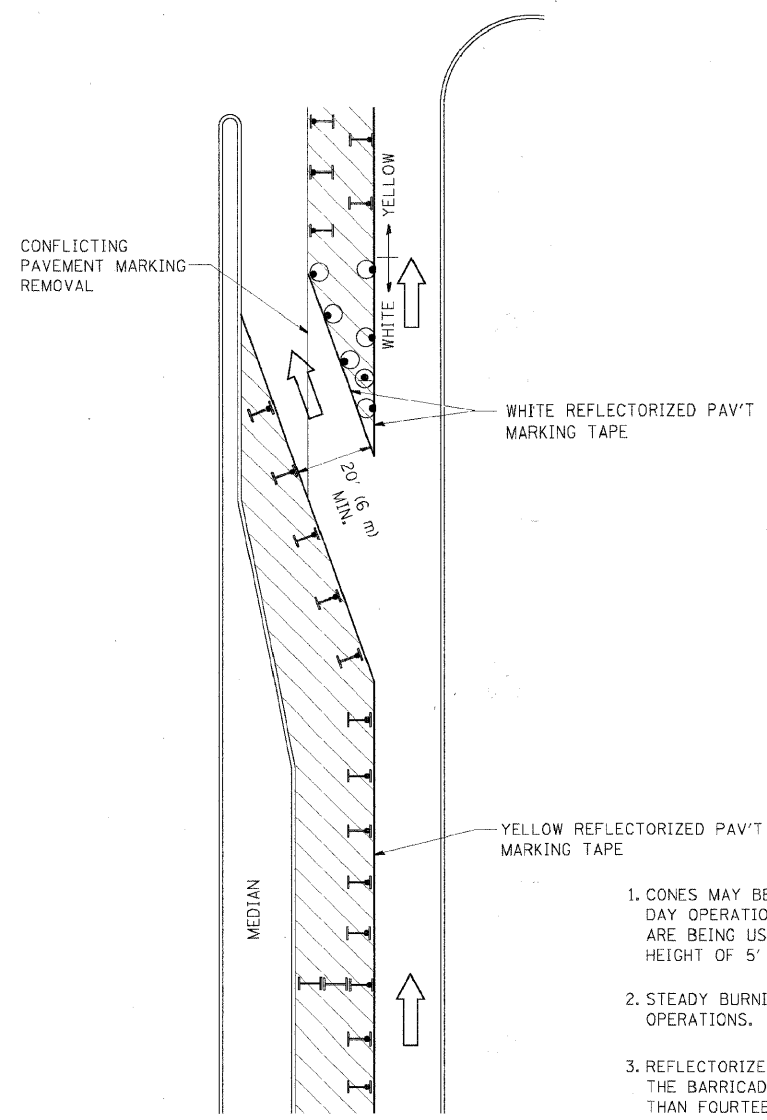
**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

DETAILS

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	60
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63333	



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GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = drivakosgn	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pwork\pwork\DRIVAKOSGN\0109315\14.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	
		PL0" SCALE = 49,999% 1/2" IN.	REVISED - A. HOUSEH 10-12-96	REVISED -						
		PLOT DATE = 9/14/2009	REVISED - T. RAMMACHER 01-06-00	REVISED -						
						TC-14		CONTRACT NO.		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REVISED

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

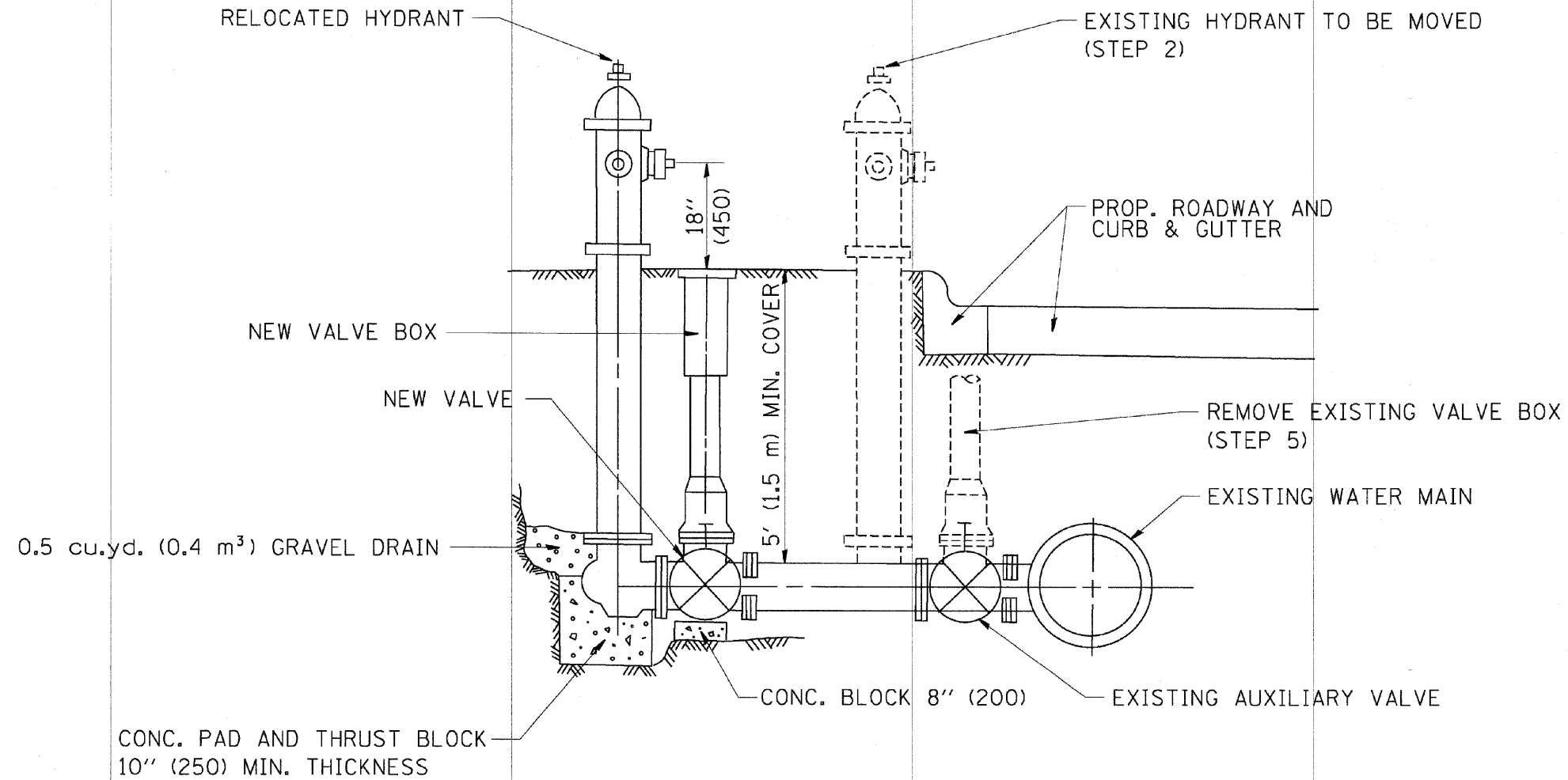
**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

DETAILS

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	61
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 63333	



1-800-892-0123



SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

FIRE HYDRANT TO BE MOVED

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dist\td\22x34\bd36.dgn	USER NAME = geglianobc	DESIGNED - DRAWN -	REVISOR - R. SHAH 09-09-94	REVISOR - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		FIRE HYDRANT TO BE MOVED		F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 68.8888' / 1" IN.	CHECKED - DATE -	REVISOR - DATE -	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS					STA.	TO STA.	BD-36	CONTRACT NO.	FED. ROAD DIST. NO. 1

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DESIGNED - JWH	REVISOR
DRAWN - LES	REVISOR
CHECKED - JMM	REVISOR
DATE - 10/02/2009	REVISOR

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

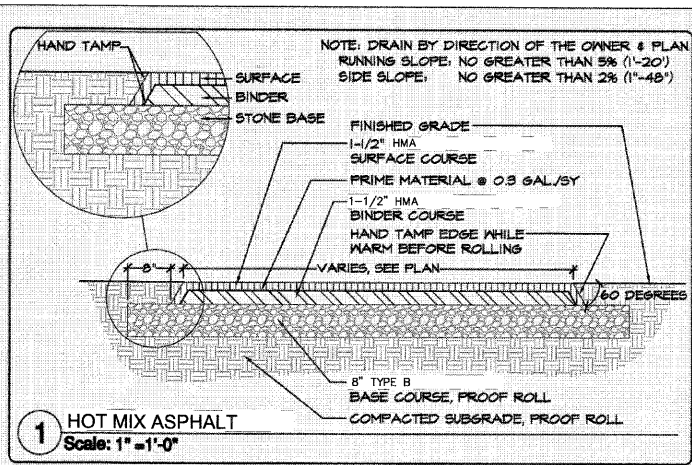
**CARY PARK DISTRICT
CARY COMMUNITY TRAIL**

DETAILS

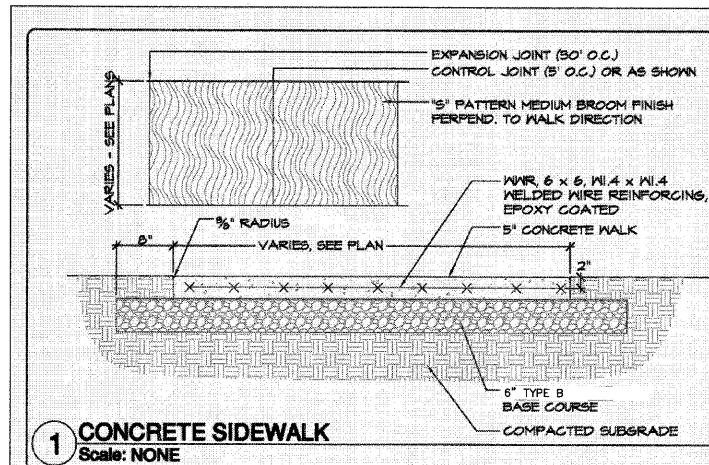
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	62
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



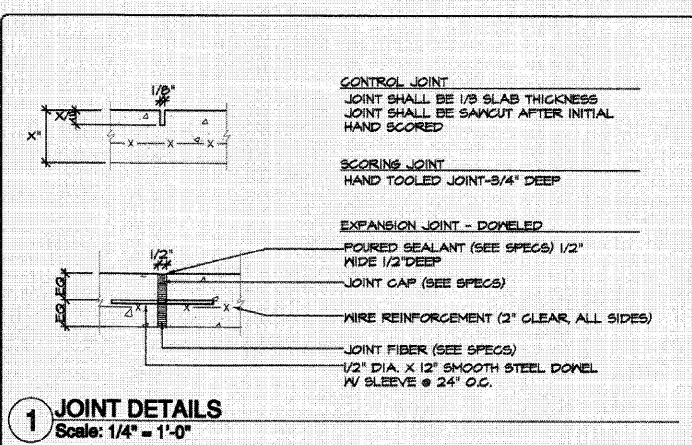
1-800-892-0123



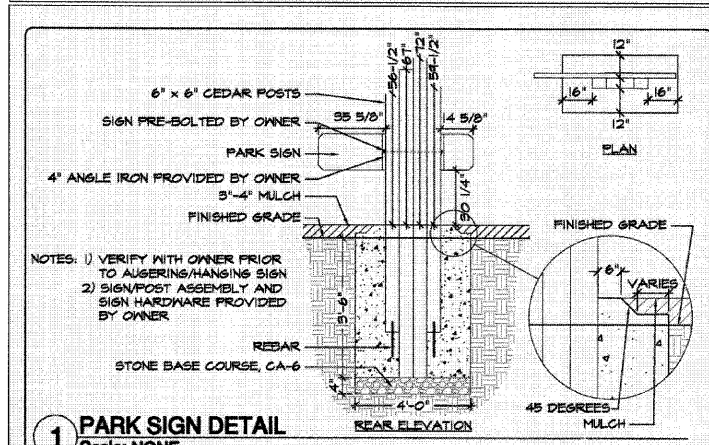
1 HOT MIX ASPHALT
Scale: 1" = 1'-0"
Cary Park District
255 Briargate Rd
Cary, Illinois 60013
(847) 639-4801
Construction Detail 13-01



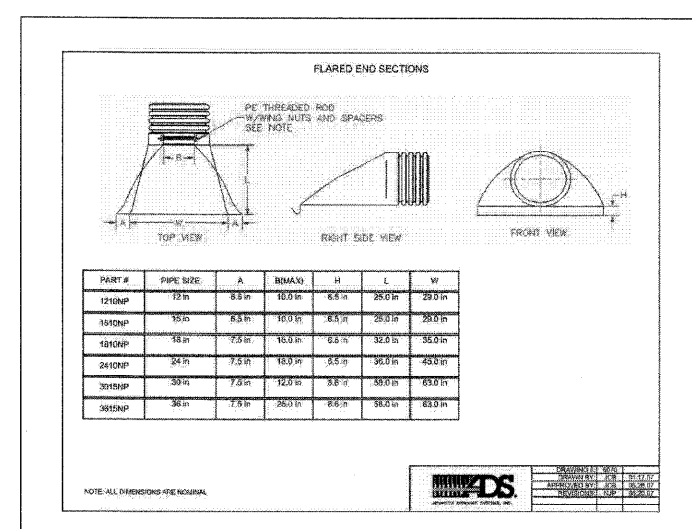
1 CONCRETE SIDEWALK
Scale: NONE
Cary Park District
255 Briargate Rd
Cary, Illinois 60013
(847) 639-4801
Construction Detail 13-02



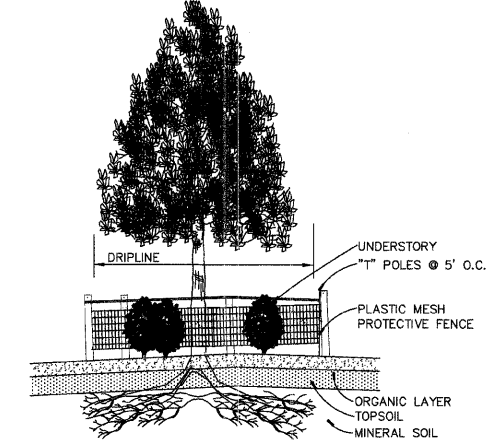
1 JOINT DETAILS
Scale: 1/4" = 1'-0"
Cary Park District
255 Briargate Rd
Cary, Illinois 60013
(847) 639-4801
Construction Detail 13-03



1 PARK SIGN DETAIL
Scale: NONE
Cary Park District
255 Briargate Rd
Cary, Illinois 60013
(847) 639-4801
Construction Detail 14-02

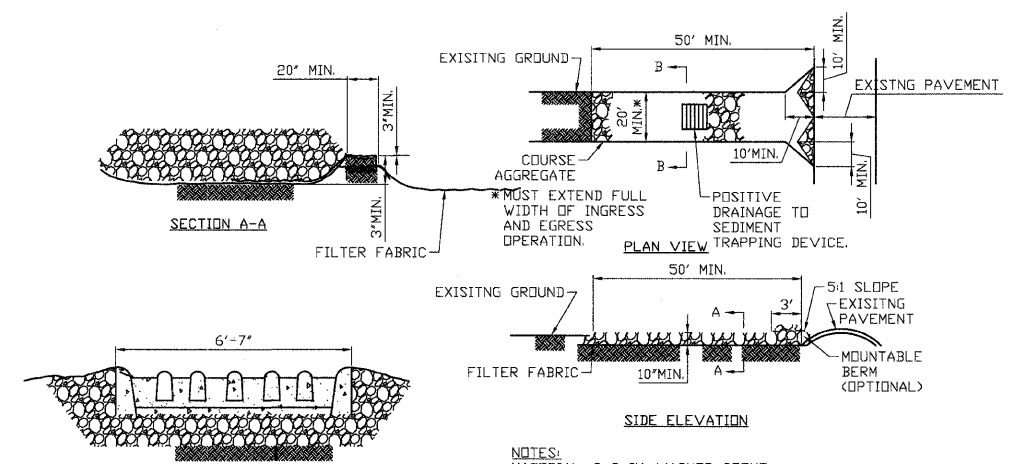


FLARED END SECTION
NOT TO SCALE
MUNDO DS
DESIGN SERVICES, INC.



- EITHER PLASTIC OR WOOD ORANGE SNOW FENCING SHALL BE INSTALLED AT OR BEYOND THE DRIPLINE, UNLESS MORE SUBSTANTIAL FENCING IS REQUIRED.
- STAKES SHALL BE METAL "T" POLES SPACED NO FURTHER APART THAN 5' ON CENTER.
- FENCING SHALL NOT BE INSTALLED CLOSER TO THE TREE THAN THE DRIPLINE OF THOSE TREES TO BE SAVED. SPECIAL CIRCUMSTANCES SHALL BE REVIEWED BY THE CARY PARK DISTRICT.
- FENCING SHALL BE ERECTED PRIOR TO CONSTRUCTION. THE CARY PARK DISTRICT SHALL BE NOTIFIED ONCE THE FENCING IS INSTALLED FOR INSPECTION.
- UNDER NO CIRCUMSTANCE SHALL THE PROTECTIVE FENCING BE REMOVED WITHOUT PROPER APPROVAL FROM THE CARY PARK DISTRICT.
- NO PERSON SHALL CONDUCT ANY ACTIVITY WITHIN THE AREAS PROPOSED TO REMAIN. THIS SHALL INCLUDE, BUT NOT LIMITED TO:
 - NO SOLVENTS OR CHEMICALS WITHIN THE PROTECTED AREAS.
 - NO BUILDING MATERIALS OR CONSTRUCTION EQUIPMENT WITHIN THE PROTECTED AREAS.
 - NO GRADE CHANGES, INCLUDING FILL, WITHIN THE PROTECTED AREAS.
 - NO REMOVAL OR VEGETATION FROM THE GROUND, WITHOUT PERMISSION FROM THE PROPER REVIEWING AUTHORITY.
 - ANY REQUIRED SWALE NEEDS TO BE DIRECTED AROUND THE PROTECTED AREAS. IN INSTANCES WHERE SWALES ARE APPROVED THROUGH A PROTECTED AREA, THE SWALES NEED TO BE HAND DUG. MACHINERY OF ANY KIND IS PROHIBITED.

TREE PROTECTION DETAIL
NOT TO SCALE



- NOTES:**
MATERIAL: 2-3 IN. WASHED STONE
THICKNESS: 10 IN MINIMUM
WIDTH: 20 FT MINIMUM OR FULL WIDTH OF ENTRANCE/EXIT ROADWAY
LENGTH: 50 FT MINIMUM. THE LENGTH CAN BE SHORTER FOR SMALL SITES SUCH AS FOR AN INDIVIDUAL HOME.
WASHING FACILITY (OPTIONAL): LEVEL AREA WITH 3 IN WASHED STONE MINIMUM OR A COMMERCIAL RACK, AND WASTE WATER DIVERTED TO A SEDIMENT TRAP OR BASIN.
GEOTEXTILE FABRIC UNDERLINER: MAY BE USED UNDER WET CONDITIONS OR FOR SOLIDS WITHIN A HIGH SEASONAL WATER TABLE TO PROVIDE GREATER BEARING STRENGTH.

CONSTRUCTION ENTRANCE
NOT TO SCALE

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DESIGNED --	JWH	REVISED	
DRAWN --	LES	REVISED	
CHECKED --	JMM	REVISED	
DATE --	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

DETAILS

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	63
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	



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DESIGNED --	JWH	REVISED	
DRAWN --	LES	REVISED	
CHECKED --	JMM	REVISED	
DATE --	10/02/2009	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

TRAIL CONSTRUCTION PHASING PLAN
PER CARY PARK DISTRICT STAFF

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY	64	64
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63333	

