

IDOT FEDERAL AID DESIGN ENGINEER: MARILIN SOLOMON, PE (847) 705-4407

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1472	05-00072-00-RS	COOK	12	1
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		
CONTRACT NO: 63334				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

F.A.U. ROUTE 1472 (RIDGEWOOD ROAD)
GOLF ROAD TO F.A.P. 372 (IL 171 / FIRST AVENUE)

ROADWAY RESURFACING
SECTION 05-00072-00-RS
PROJECT: ARA-8003(516)
VILLAGE OF RIVERSIDE
COOK COUNTY
C-91-200-05

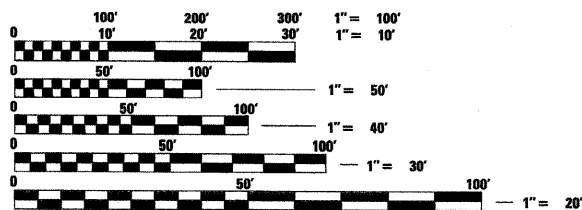
FOR INDEX OF SHEETS, SEE SHEET NO. 2

DESIGN DESIGNATION:
COLLECTOR

TRAFFIC DATA
ADT: 7,000 (2030)

SPEED LIMIT:
25 MPH (POSTED)
30 MPH (DESIGN)

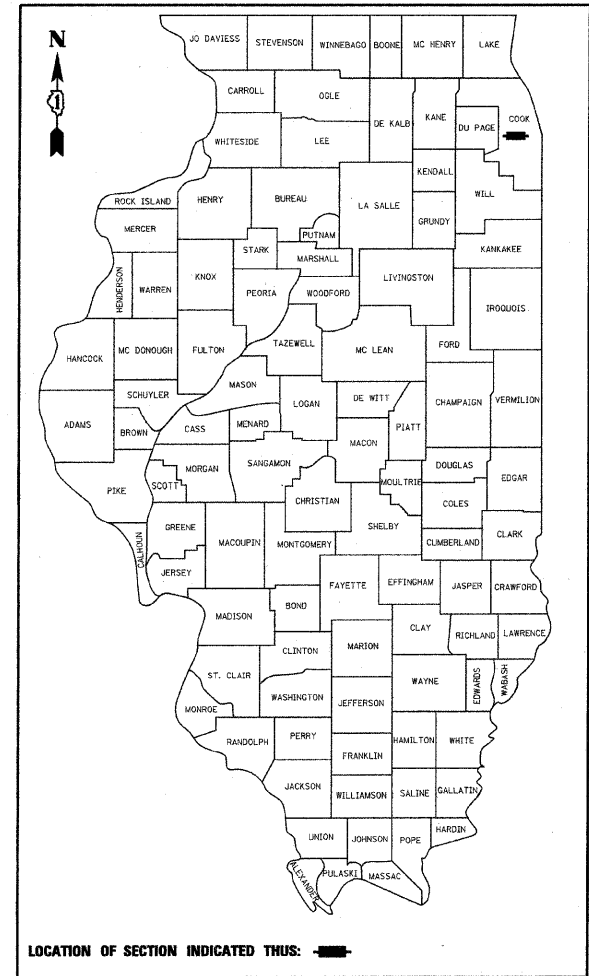
PROJECT LOCATED IN
VILLAGE OF RIVERSIDE



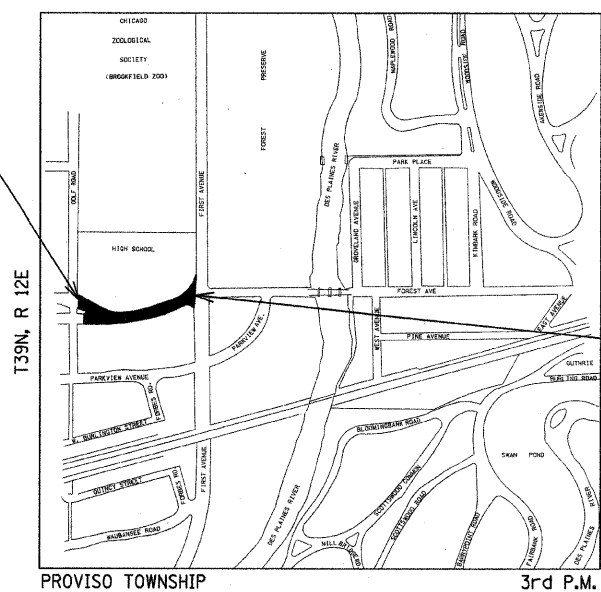
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811

CONTRACT NO. 63334



PROJECT BEGINS-RIDGEWOOD ROAD
AT GOLF ROAD
STA 0+27



PROJECT ENDS-RIDGEWOOD ROAD
AT FIRST AVENUE (IL 171)
STA 8+57

LOCATION MAP
GROSS LENGTH OF PROJECT: 830 FT. (0.157 MI.)
NET LENGTH OF PROJECT: 830 FT. (0.157 MI.)
39N12E PROVISO TOWNSHIP SECTION 35
SCALE: NTS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: September 30, 2009

Doug Cook
Riverside, Director of Public Works

PASSED: October 27, 2009

Christy Hebert
District 1 Engineer of Local Roads & Streets

Releasing for Bid
Based on Limited
Review October 29, 2009

Deann M. O'Keefe
Deputy Director of Highways, Region 1 Engineer

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JJB
JAMES J. BENES & ASSOCIATES
CONSULTING ENGINEERS
950 WARRENVILLE ROAD, SUITE 101
LISLE, IL 60532
(630) 719-7570
SIGNATURE: *James J. Benes*
DATE: October 9, 2009
IL LICENSE NO: 062-060441
EXP. DATE: NOVEMBER 30, 2009
FIELD: JAMES J. BENES AND ASSOCIATES, INC.
CIVIL ENGINEERING



GENERAL NOTES

- ACCESS TO LOCAL RESIDENCES AND BUSINESSES SHALL BE MAINTAINED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL GIVE THE ENGINEER AND JAMES J. BENES AND ASSOCIATES, INC. THREE (3) WORKING DAYS NOTICE PRIOR TO THE COMMENCEMENT OF WORK. (JAMES J. BENES AND ASSOCIATES, INC.: (630) 719-7570)
- ALL ELEVATIONS ARE ON U.S.G.S. DATUM. (1927)
- NEITHER THE ENGINEER, NOR THE OWNER, SHALL ASSUME ANY OF THE RESPONSIBILITIES OF THE CONTRACTOR'S SUPERINTENDENT OR OF SUBCONTRACTORS. ADDITIONALLY, NEITHER THE ENGINEER, NOR THE OWNER, SHALL ADVISE ON, OR ISSUE DIRECTIONS CONCERNING, ASPECTS OF CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, OR SAFETY PRECAUTIONS AND/OR PROGRAMS IN CONNECTION WITH THE WORK.
- THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THEIR ACCURACY IS NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION AND ELEVATION OF ALL UTILITIES. THE CONTRACTOR SHALL REPORT ANY ENCOUNTERED DISCREPANCIES TO THE ENGINEER AT ONCE. THE CONTRACTOR SHALL TAKE DUE CARE IN ALL PHASES OF CONSTRUCTION TO PROTECT ANY UTILITIES WHICH MAY BE AFFECTED BY THE WORK. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL KEEP THE CONSTRUCTION AREA FREE OF DEBRIS AND/OR OBJECTIONABLE MATERIALS DURING CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN ALL EXISTING DRAINAGE FACILITIES DURING CONSTRUCTION AND SHALL REPAIR ANY DRAINAGE FACILITIES DAMAGED DURING CONSTRUCTION. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT AND WILL NOT BE PAID FOR SEPARATELY.
- THE CONTRACTOR SHALL VERIFY THE ELEVATIONS OF EXISTING STORM SEWERS PRIOR TO THE CONSTRUCTION OF PROPOSED STORM SEWER.
- BEFORE STARTING AND EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- UNLESS OTHERWISE SHOWN, TRANSITIONS OF 10' SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURB AND GUTTERS AND MEDIANS IN THE FIELD. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF RIVERSIDE.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I AND TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH THE "BUTT JOINTS AND BITUMINOUS TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- CONSTRUCTION SHALL NOT BEGIN UNTIL AFTER THE RIVERSIDE BROOKFIELD HIGH SCHOOL SPRING SEMESTER IS COMPLETE IN JUNE OF 2010.

MWRD GENERAL NOTES

- THE MWRD LOCAL SEWER PERMIT SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF WORK (CALL 708-588-4055.)
- ELEVATION DATUM IS U.S.G.S. (1927)
- NO FLOOR DRAINS.
- NO FOOTING DRAINS AND DOWNSPOUTS.
- ALL SANITARY SEWER PIPE MATERIALS AND JOINTS (AND STORM SEWER PIPE MATERIALS AND JOINTS IN A COMBINED SEWER AREA) SHALL CONFORM TO:

<u>Pipe Material Spec.</u>	<u>Joint Spec.</u>	<u>Pipe Material Spec.</u>	<u>Joint Spec.</u>
<u>Vitrified Clay Pipe</u>		<u>PVC Gravity Sewer Pipe</u>	
VCP C-700	C-425	6"-15" dia. SDR 26	
VCP (No-Bell) C-700		D-3034	D-3212 or D-2855
Joint Collar	C-425 D-1784		
<u>Concrete Pipe C-14</u>		18"-27" dia. F/dy=46	
RCP C-76	C-443	F-679	D-3212 or D-2855
ACP C-428	D-1869		
<u>ABS Sewer Pipe</u>		CISP A-74	C-564
Solid Wall 6" dia. SDR 23.5		DIP A-21.51	A-21.11
ABS D-2751	D-2751		
<u>ABS Composite/Truss Pipe</u>			
8"-15" dia.			
ABS D-2680	D-2680		
- ALL SANITARY SEWER CONSTRUCTION, (AND STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS), REQUIRES STONE BEDDING WITH STONE 1/4" TO 1" IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL TO 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE WHEN USING PVC.
- "BAND-SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPES OF DISSIMILAR MATERIALS.
- WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
 - CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS ("SEWER-TAP" MACHINE OR SIMILAR) AND PROPER INSTALLATION OF HUB-WYE SADDLE OR HUB-TEE SADDLE.
 - REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
 - WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING "BAND-SEAL" OR SIMILAR COUPLINGS TO HOLD IT FIRMLY IN PLACE.
- WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATERMANS SHALL BE MAINTAINED UNLESS: THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18 INCH VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CAN NOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATERMAIN, THE SEWER SHALL BE CONSTRUCTED TO WATERMAIN STANDARDS.
- ALL EXISTING SEPTIC SYSTEMS SHALL BE ABANDONED. ABANDONED TANKS SHALL BE FILLED WITH GRANULAR MATERIAL OR REMOVED.
- ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE CAST IN PLACE OR PRE-CAST REINFORCED CONCRETE.

INDEX OF DRAWINGS

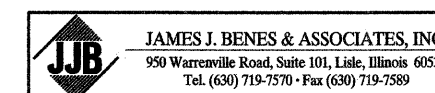
- TITLE SHEET
- INDEX OF SHEETS, BENCH MARKS, LIST OF STATE STANDARDS AND GENERAL NOTES
- SUMMARY OF QUANTITIES
- TYPICAL SECTIONS
- PLAN SHEET
- DETECTOR LOOP DETAILS AND SCHEDULES
- DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
- CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT DETAILS
- BUTT JOINT AND BITUMINOUS TAPER DETAILS
- TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
- DISTRICT ONE TYPICAL PAVEMENT MARKINGS
- PAVEMENT MARKINGS LETTERS AND SYMBOLS FOR TRAFFIC STAGING

BENCH MARKS

#	DESCRIPTION	USGS ELEVATION
1)	NORTH FLANGE BOLT OF THE FIRE HYDRANT AT THE INTERSECTION OF WOODSIDE DRIVE AND LONGCOMMON ROAD	617.65
2)	NORTH BONNET BOLT OF THE FIRE HYDRANT AT THE NORTHWEST CORNER OF FIRST AVENUE AND RIDGEWOOD ROAD	617.31

IDOT HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-03	FRAMES AND LIDS, TYPE 1
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1472
RIDGEWOOD ROAD
INDEX OF DRAWINGS, BENCH MARKS,
GENERAL NOTES AND
IDOT STANDARD DRAWINGS

SCALE: NTS
DATE: 9/15/09
DRAWN BY: SMP
CHECKED BY: JDS

SUMMARY OF QUANTITIES

ITEM CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
			I-000
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	84
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	84
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	506
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	208
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	3
25000500	PHOSPORUS FERTILIZER NUTRIENT	POUND	3
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	3
25200100	SODDING	SQ YD	208
25200200	SUPPLEMENTAL WATERING	UNIT	5
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	3
40600300	AGGREGATE (PRIME COAT)	TON	16
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	166
40600895	CONSTRUCTING TEST STRIP	EACH	1
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	64
40600990	TEMPORARY RAMP	SQ YD	127
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	498
42001300	PROTECTIVE COAT	SQ YD	286
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	93
42400800	DETECTABLE WARNINGS	SQ FT	68
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	3946
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	81
44000600	SIDEWALK REMOVAL	SQ FT	846
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	442

SUMMARY OF QUANTITIES

ITEM CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
			I-000
44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	98
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	98
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	118
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	276
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	5810
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	3
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2
60262700	INLETS TO BE RECONSTRUCTED	EACH	2
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	4
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	6
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	830
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	276
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	41
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2018
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	137
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	247
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	79
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	438
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	9
XX004005	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL (PEA GRAVEL)	SQ FT	846
Z0066500	STABILIZED DRIVEWAYS 7"	SQ YD	10

LEGEND

* - DENOTES SPECIALTY ITEMS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1472
RIDGWOOD ROAD

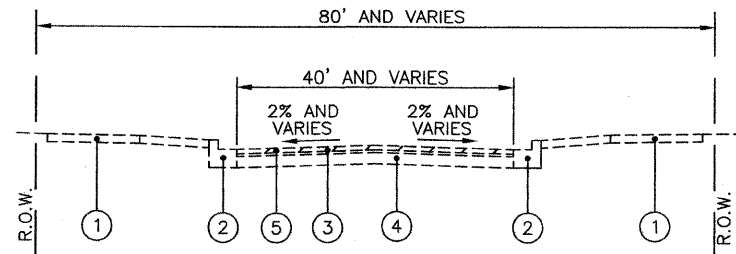
SUMMARY OF QUANTITIES

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DRAWN BY: SMP
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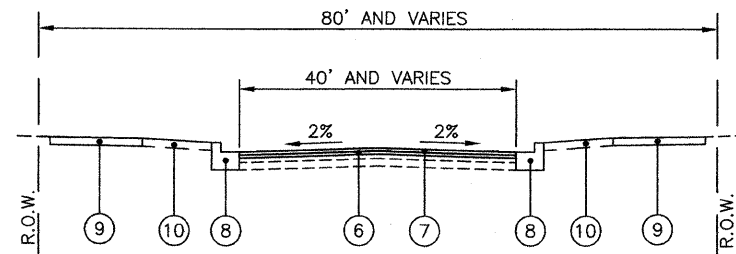


JAMES J. BENES & ASSOCIATES, INC.
950 Warrenville Road, Suite 101, Lisle, Illinois 60532
Tel. (630) 719-7570 • Fax (630) 719-7589



EXISTING TYPICAL SECTION
RIDGEWOOD ROAD

GOLF ROAD TO FIRST AVENUE (IL 171)
STA. 0+27 TO STA 8+57



PROPOSED TYPICAL SECTION
RIDGEWOOD ROAD

GOLF ROAD TO FIRST AVENUE (IL 171)
STA. 0+27 TO STA 8+57

LEGEND

- ① EXISTING SIDEWALK
- ② EXISTING CURB AND GUTTER, TYPE B-6.12
- ③ EXISTING HOT-MIX ASPHALT CONCRETE SURFACE COURSE
- ④ EXISTING BASE COURSE
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑥ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 (3/4")
- ⑦ PROPOSED HOT-MIX ASPHALT CONCRETE SURFACE COURSE, MIX D, N50 (2-1/4")
- ⑧ PROPOSED COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)
- ⑨ PROPOSED PCC SIDEWALK, 5 INCH (PEA GRAVEL MIX)
NOTE: MINIMUM 2 INCH CA-6 SUBBASE REQUIRED
(COST INCLUDED WITH SIDEWALK)
- ⑩ TOPSOIL FURNISH AND PLACE 4" AND SODDING (AS DIRECTED BY THE ENGINEER)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ N _{des}
LEVELING BINDER (MACHINE METHOD), N50 (IL 9.5mm)	4% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N50 (IL 9.5mm)	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70 (CLASS D PATCHES, 8")	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N50 (5" DRIVEWAY BASE)	4% @ 50 GYR
HOT-MIX ASPHALT SURFACE COURSE, MIX C, N50 (IL 9.5mm)(2" DRIVEWAY SURFACE)	4% @ 50 GYR

UNIT WEIGHT FOR ALL HOT-MIX ASPHALT SURFACE MIX IS 112 LBS/SY/IN

FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT 1 SPECIAL PROVISIONS

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1472
RIDGEWOOD ROAD

TYPICAL SECTIONS

JJB JAMES J. BENES & ASSOCIATES, INC.
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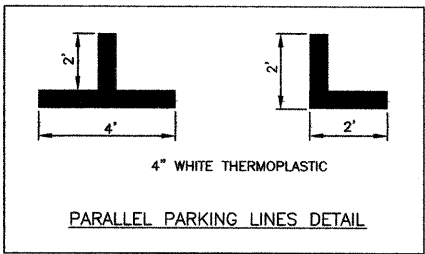
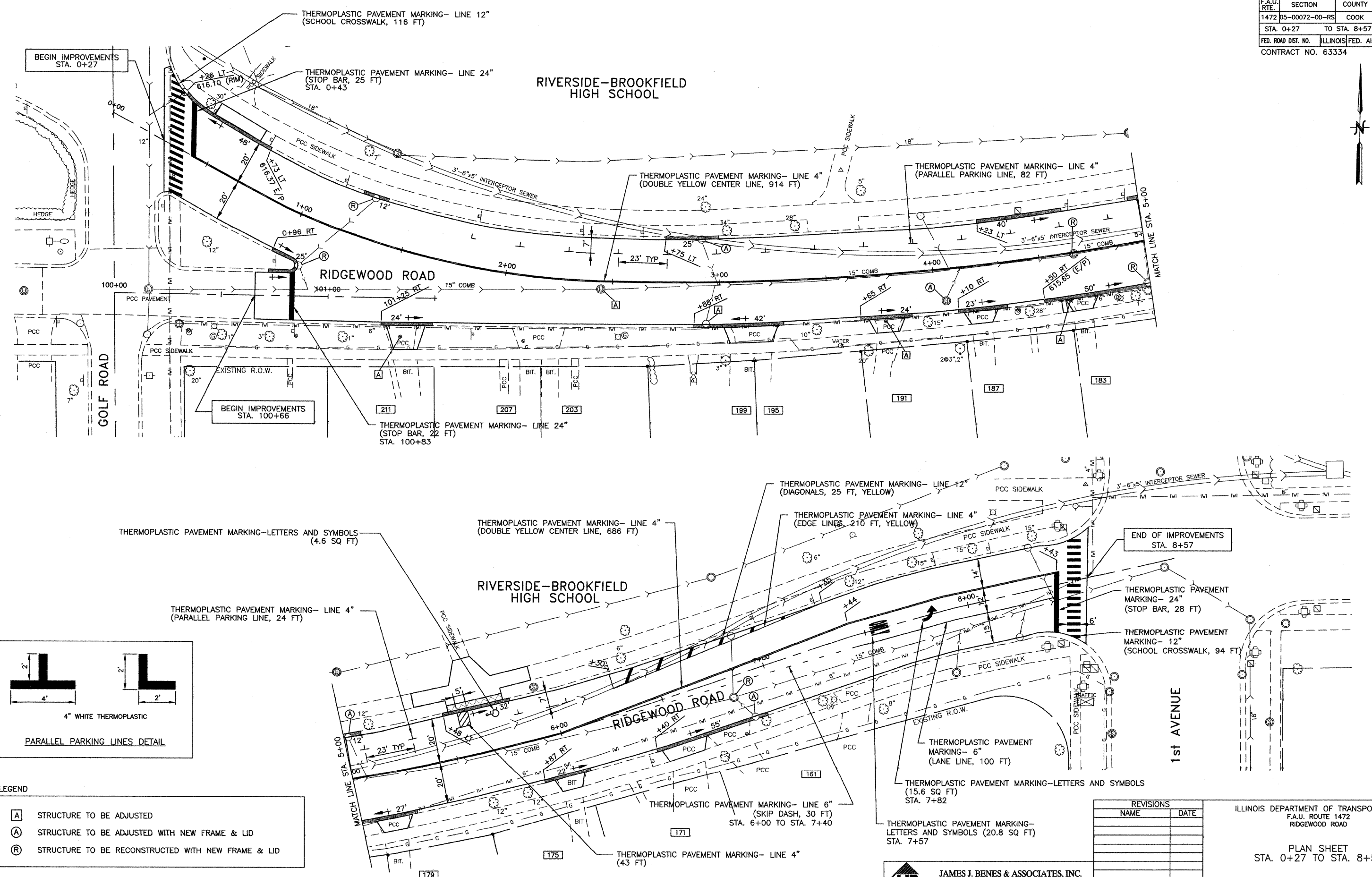
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STA. 0+27		TO STA. 8+57		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 63334				



RIVERSIDE-BROOKFIELD HIGH SCHOOL



LEGEND

[A]	STRUCTURE TO BE ADJUSTED
[A]	STRUCTURE TO BE ADJUSTED WITH NEW FRAME & LID
[R]	STRUCTURE TO BE RECONSTRUCTED WITH NEW FRAME & LID

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.U. ROUTE 1472
 RIDGEWOOD ROAD

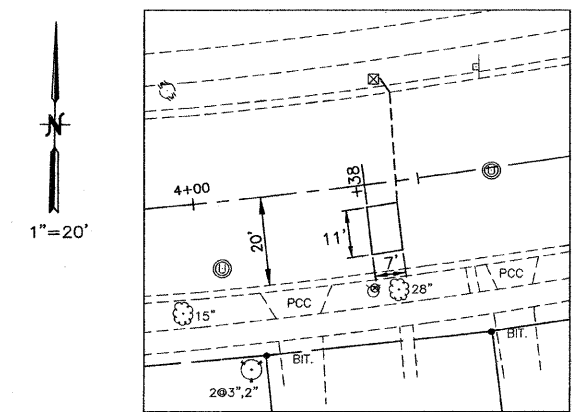
PLAN SHEET
 STA. 0+27 TO STA. 8+57

SCALE: 1"=20'
 DATE: 9/15/09

DRAWN BY: SMP
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JJB JAMES J. BENES & ASSOCIATES, INC.
 950 Warrenville Road, Suite 101, Lisle, Illinois 60532
 Tel. (630) 719-7570 • Fax (630) 719-7589

STRUCTURE RECONSTRUCTION SCHEDULE				
STATION	OFFSET (FOOT)	LT/RT	DESCRIPTION	TYPE 1 FRAME
1+09	23	RT	CATCH BASINS TO BE RECONSTRUCTED	OPEN LID
1+31	20	LT	INLETS TO BE RECONSTRUCTED	OPEN LID
4+66	1	RT	MANHOLES TO BE RECONSTRUCTED	CLOSED LID
4+99	20	RT	INLETS TO BE RECONSTRUCTED	OPEN LID
6+82	10	RT	MANHOLES TO BE RECONSTRUCTED	CLOSED LID

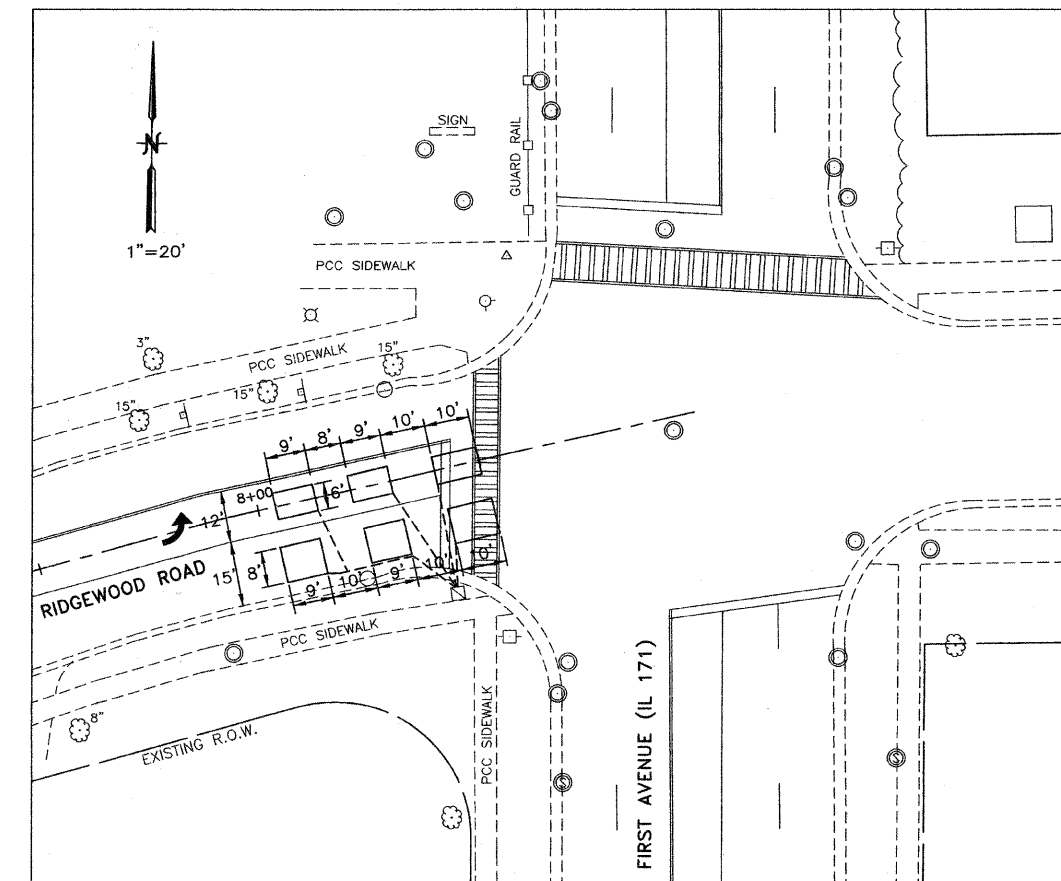


DETECTOR LOOP REPLACEMENT DETAIL
RIDGEWOOD ROAD AT HIGH SCHOOL

STRUCTURE ADJUSTMENT SCHEDULE				
STATION	OFFSET (FOOT)	LT/RT	DESCRIPTION	TYPE 1 FRAME
2+45	4	RT	FRAME AND LID TO BE ADJUSTED (SPECIAL)	-
2+93	20	LT	FRAME AND LID TO BE ADJUSTED	OPEN LID
2+94	20	RT	FRAME AND LID TO BE ADJUSTED	-
4+05	15	RT	FRAME AND LID TO BE ADJUSTED (SPECIAL)	CLOSED LID
5+02	20	LT	FRAME AND LID TO BE ADJUSTED	OPEN LID
6+89	22	RT	FRAME AND LID TO BE ADJUSTED	OPEN LID

DRIVEWAY SCHEDULE							
STATION	OFFSET (L/R/T)	ADDRESS	PROPOSED WIDTH AT SIDEWALK (FOOT)	PROPOSED WIDTH AT BACK / CURB (FOOT)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	STABILIZED DRIVEWAYS 7" (SQ YD)	P.C.C. DRIVEWAY PVMT, 7" (SQ YD)
101+36	RT	# 211	11	17	14.4	-	17.1
3+13	RT	# 199/195	19	25	16.7	-	25.7
3+77	RT	# 191	11	17	8.0	-	10.1
4+21	RT	# 187	10	16	6.0	-	8.7
4+67	RT	# 183	10	16	6.8	-	8.7
5+15	RT	# 179	11	17	9.3	-	9.3
5+98	RT	# 175	10	16	8.7	9.4	-
6+54	RT	# 171	17	23	11.3	-	13.3
TOTALS:					81	9	93

* ANY INCIDENTAL EARTH EXCAVATION REQUIRED TO REPLACE DRIVEWAY APRON AS PROPOSED WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THIS ITEM.



DETECTOR LOOP REPLACEMENT DETAIL
RIDGEWOOD ROAD AND FIRST AVENUE (IL 171)

REVISIONS	
NAME	DATE

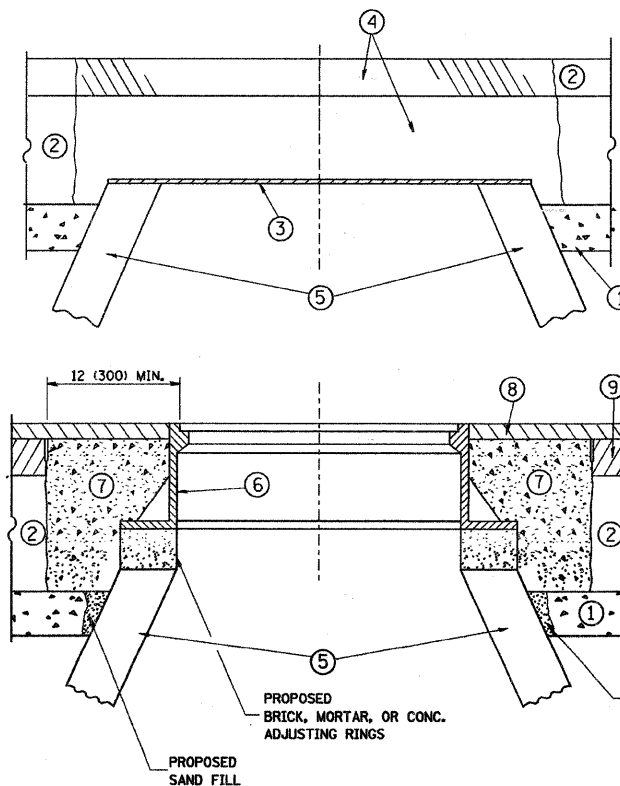
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.U. 1472
RIDGEWOOD ROAD

DETECTOR LOOP DETAILS
AND SCHEDULES

SCALE: N.T.S.
DATE: 9/15/09
DRAWN BY: SMP
CHECKED BY: JDS

JJB
JAMES J. BENES & ASSOCIATES, INC.
950 Warrenville Road, Suite 101, Lisle, Illinois 60532
Tel. (630) 719-7570 • Fax (630) 719-7589

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1472	05-00072-00-RS	COOK	12	7
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS. TRACT NO. 63334

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

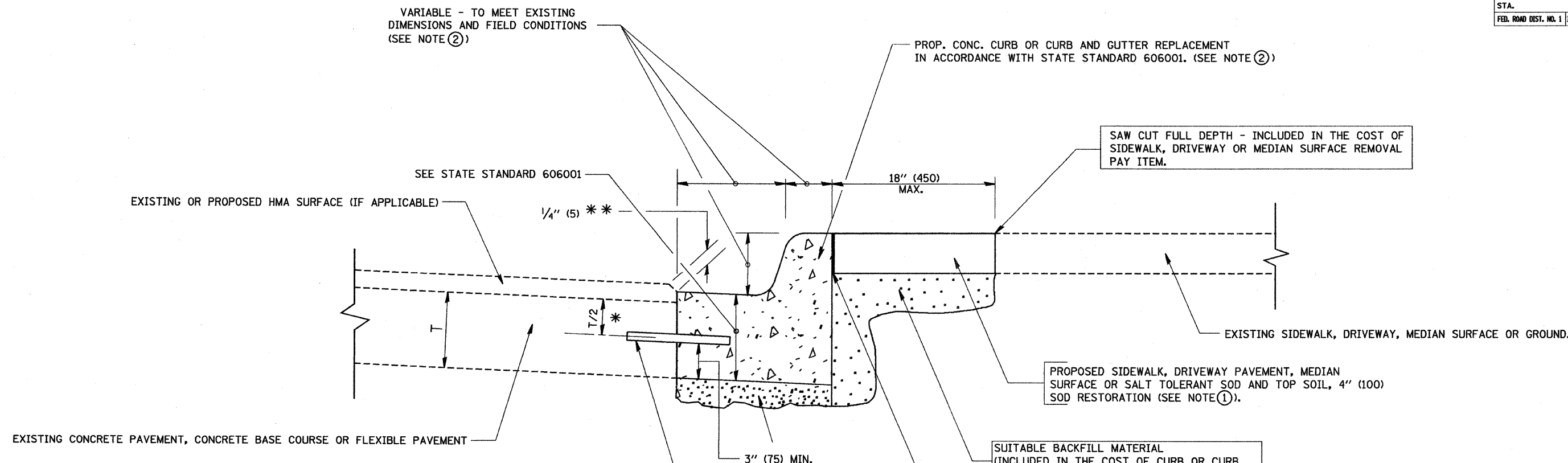
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HORIZ.

DRAWN BY

CHECKED BY

BD600-03 (BD-8)

F.A.O. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1472	05-00072-00-RS	COOK	12	8
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

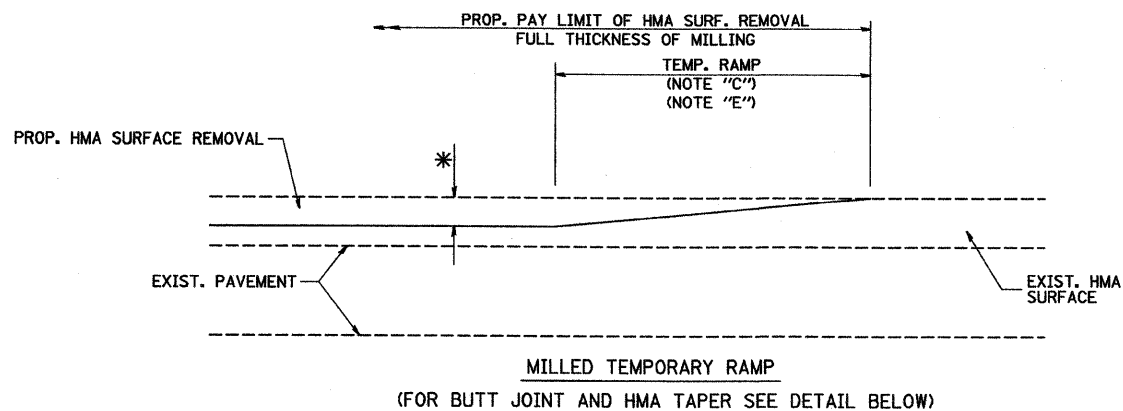
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DRAWN BY
CHECKED BY
BD600-06 (BD-24)

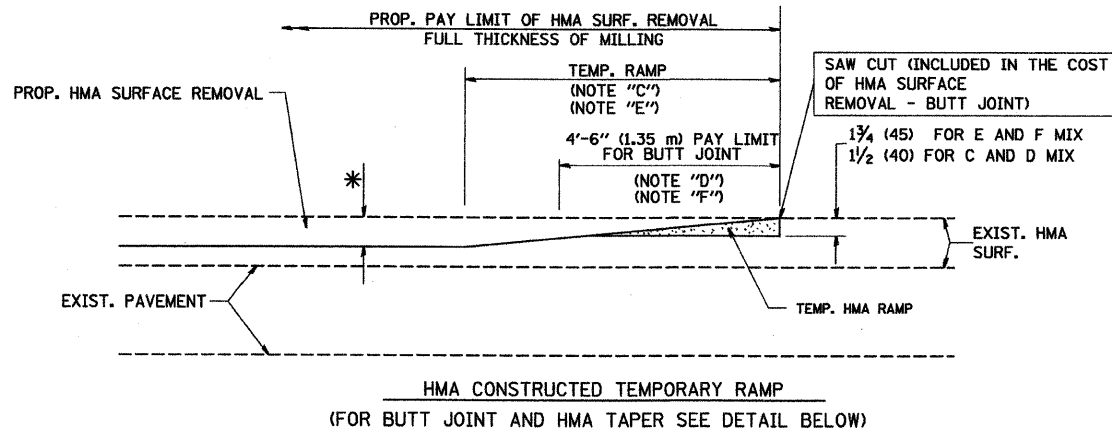
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F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

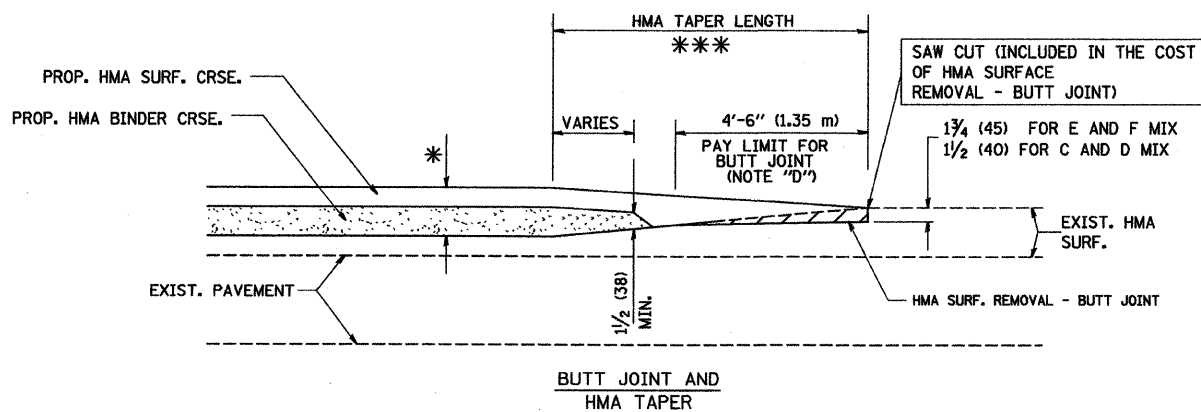


OPTION 1

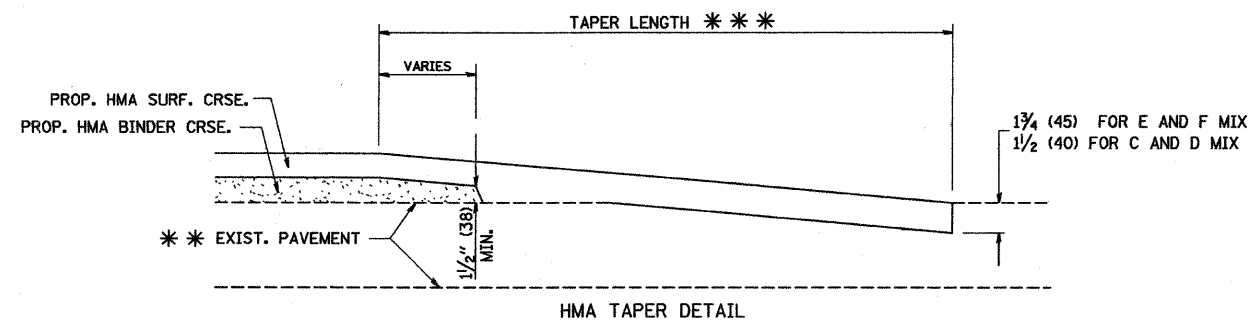
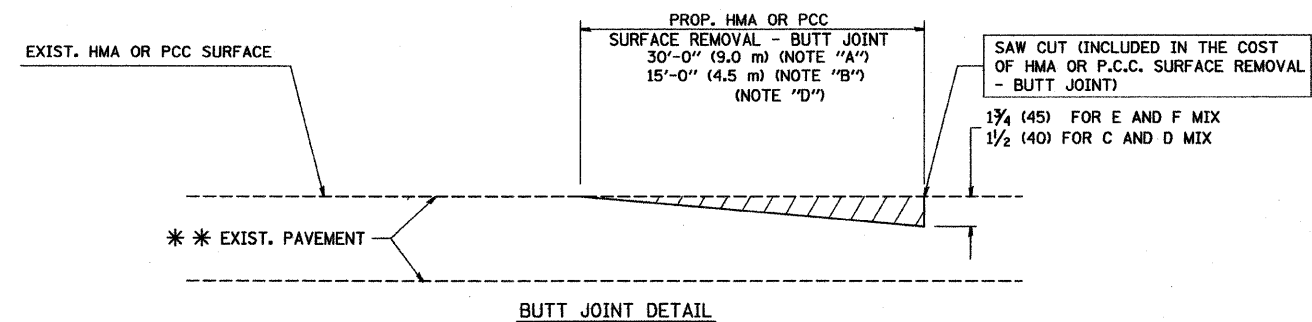


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

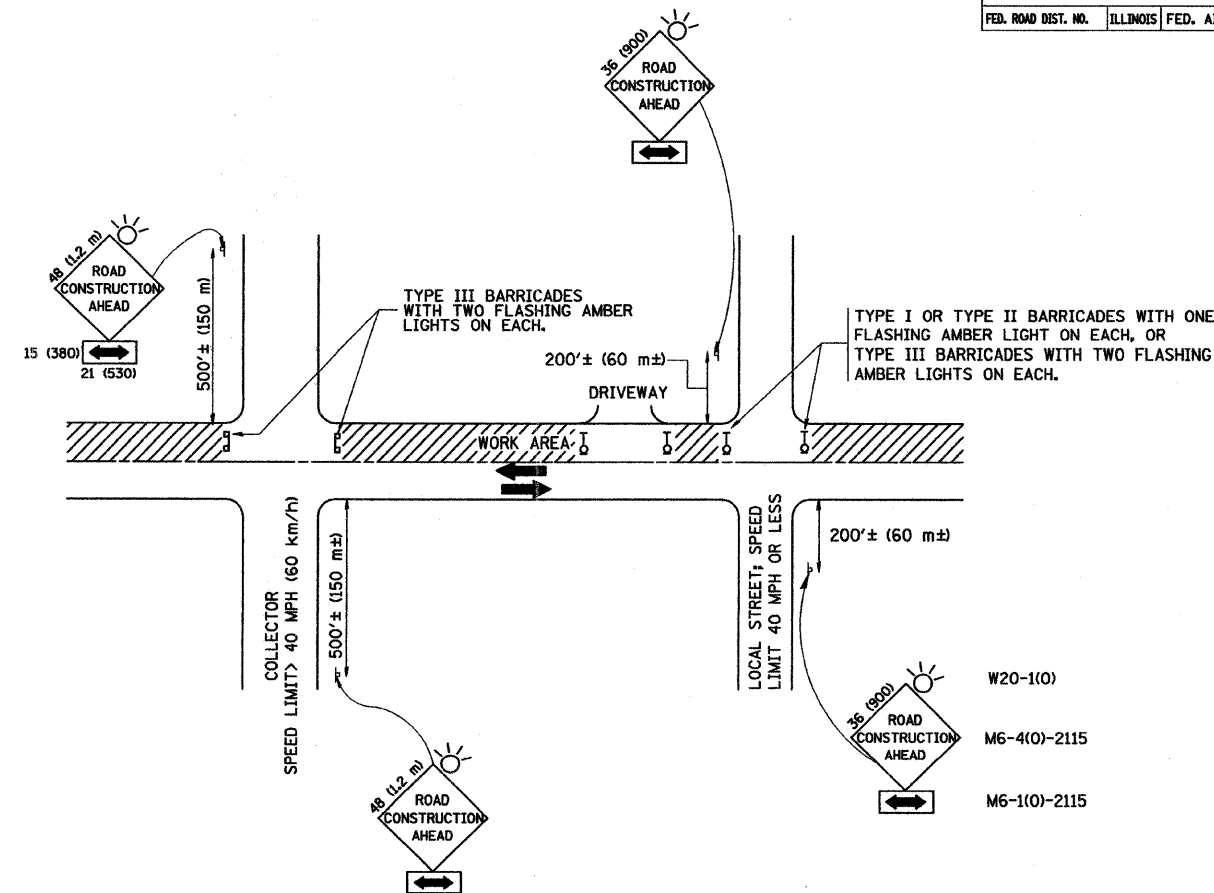
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HORIZ. NONE

DRAWN BY
CHECKED BY

BD400-05 (VI-BD32)

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PLT SCALE = 80/8000 / IN.
USER NAME = bmaur-d

F.A.I.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1472	05-00072-00-RS	COOK	12	10
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

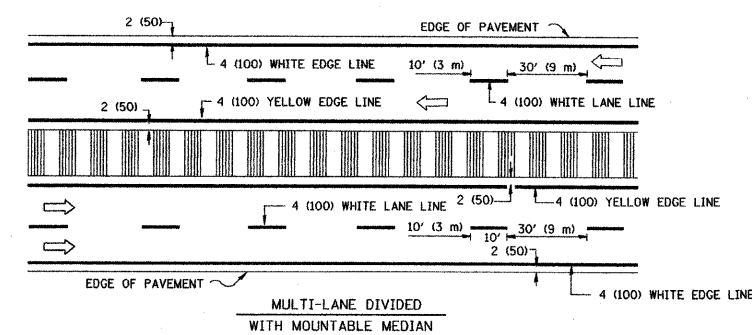
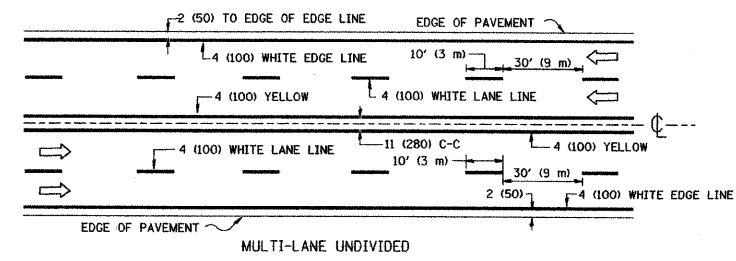
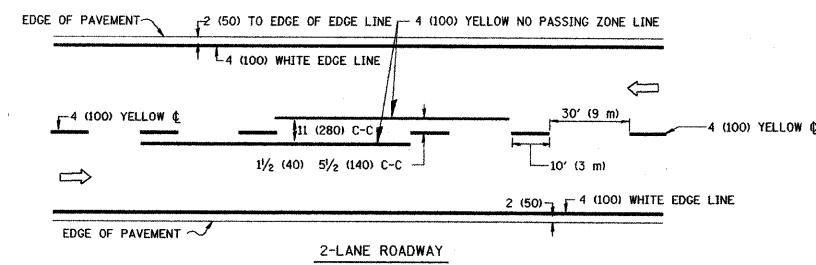
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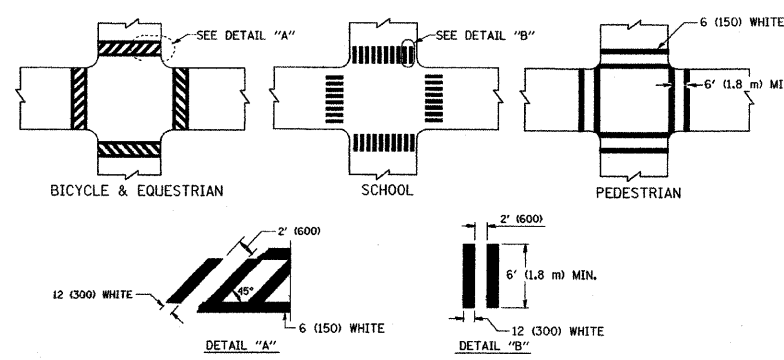
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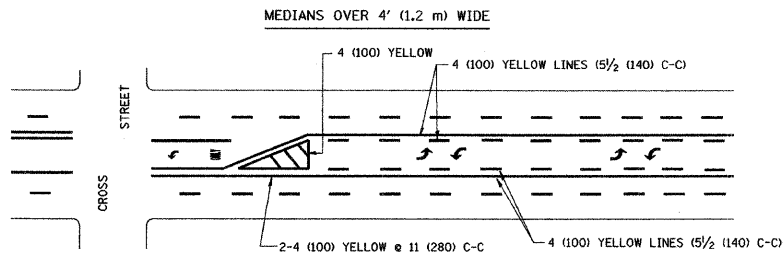
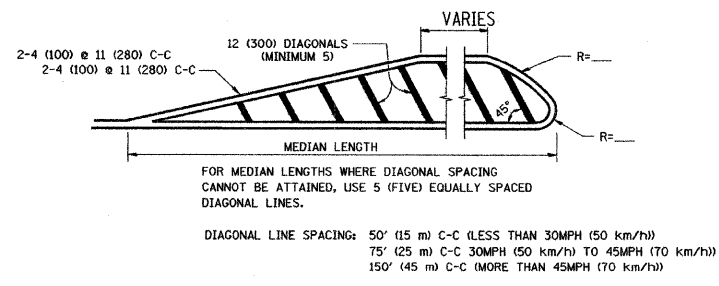
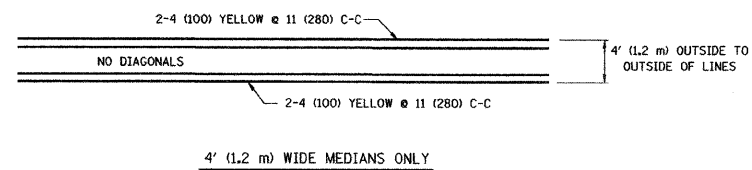


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

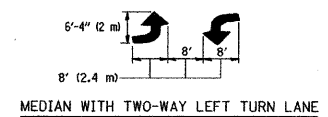
TYPICAL LANE AND EDGE LINE MARKING



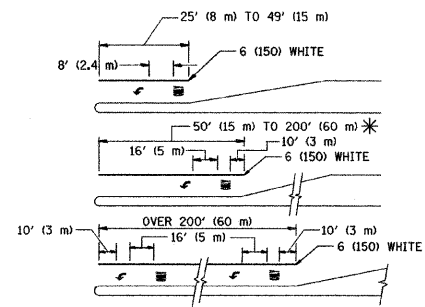
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



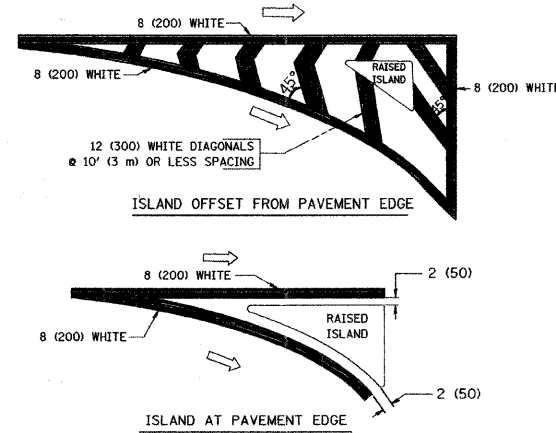
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

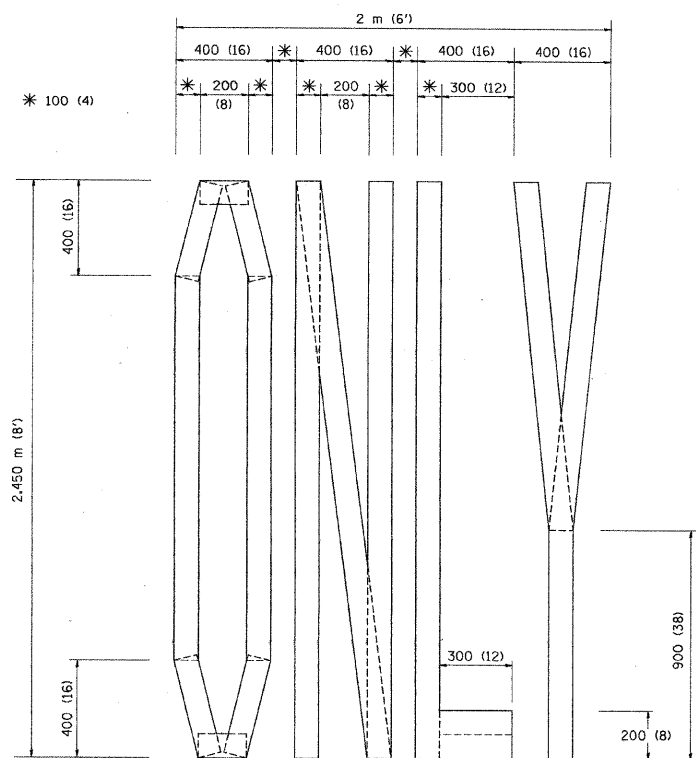
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

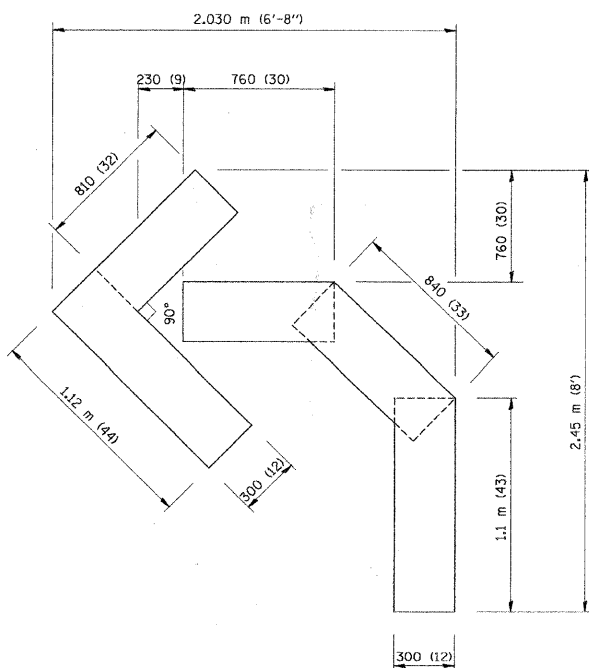
REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
C. JUCIUS	09-09-09

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT ONE
 TYPICAL PAVEMENT MARKINGS
 SCALE: NONE
 DRAWN BY CADD
 CHECKED BY
 TC-13

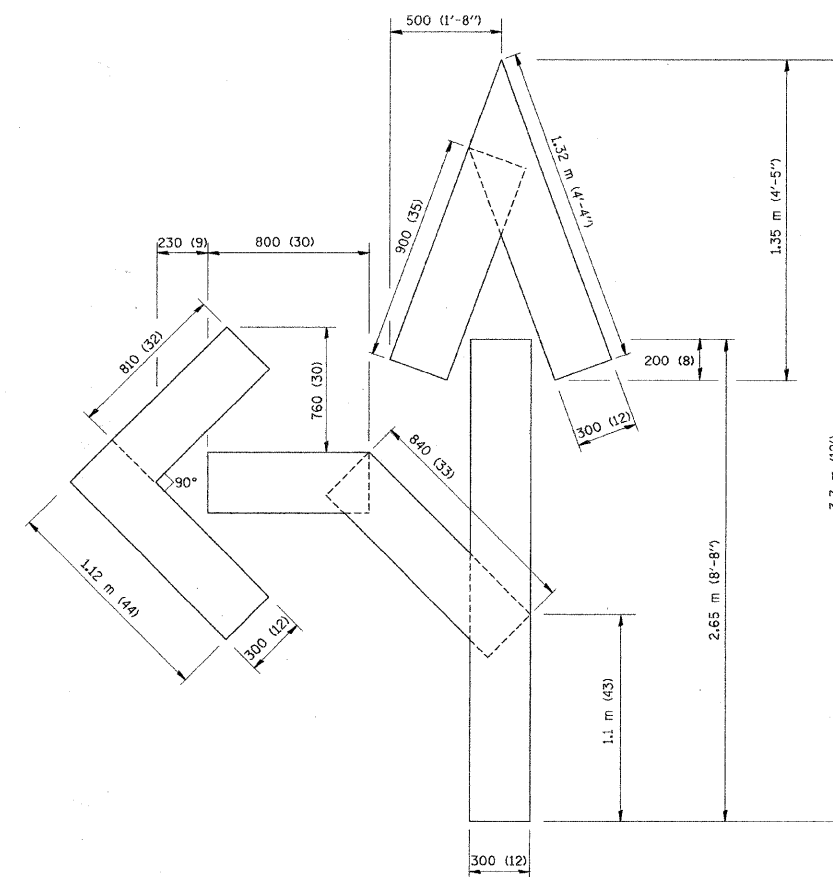
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QUANTITY
 100 (4) LINE = 19.7 m (64.1 ft.)
 1.97 sq. m (21.1 sq. ft.)



QUANTITY
 100 (4) LINE = 13.9 m (45.5 ft.)
 1.39 sq. m (15.2 sq. ft.)



QUANTITY
 100 (4) LINE = 25.3 m (82.5 ft.)
 2.53 sq. m (27.5 sq. ft.)

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

SCALE: NONE
 DATE 10/18/2002

DRAWN BY CADD
 CHECKED BY
 TC-16

REVISION DATE: 08/28/00