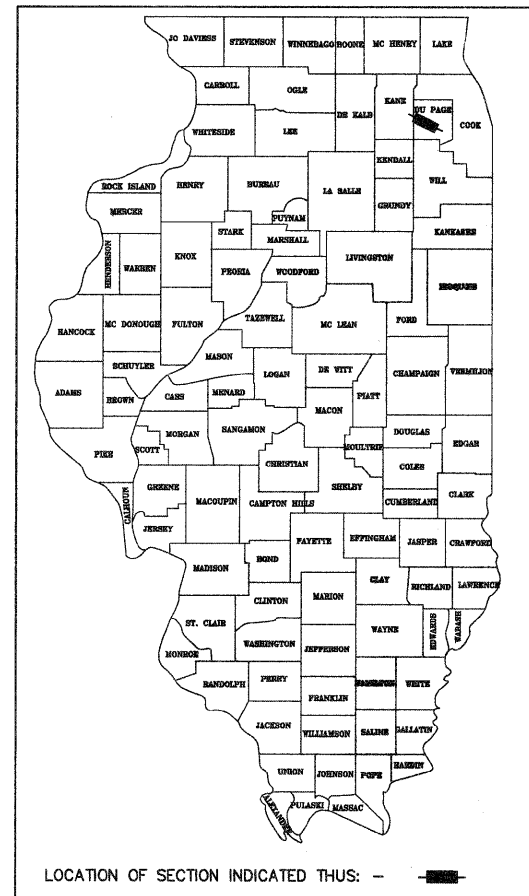


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID PROJECT
LAPP RESURFACING**
BATAVIA ROAD (FAU ROUTE 3553)
FROM IL ROUTE 59
TO IL ROUTE 56
PROJECT NO. ARA-9003(550)
SECTION NO. 09-00029-00-RS
CITY OF WARRENVILLE
DUPAGE COUNTY
JOB NO. C-91-749-09

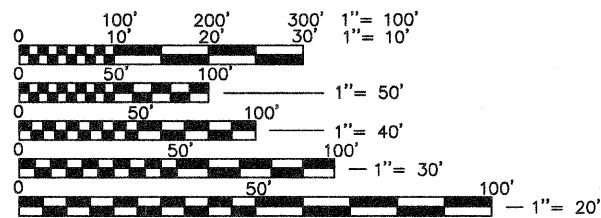
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3553	09-00029-00-RS	DUPAGE	27	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT - LAPP		
CONTRACT NO. 63373				



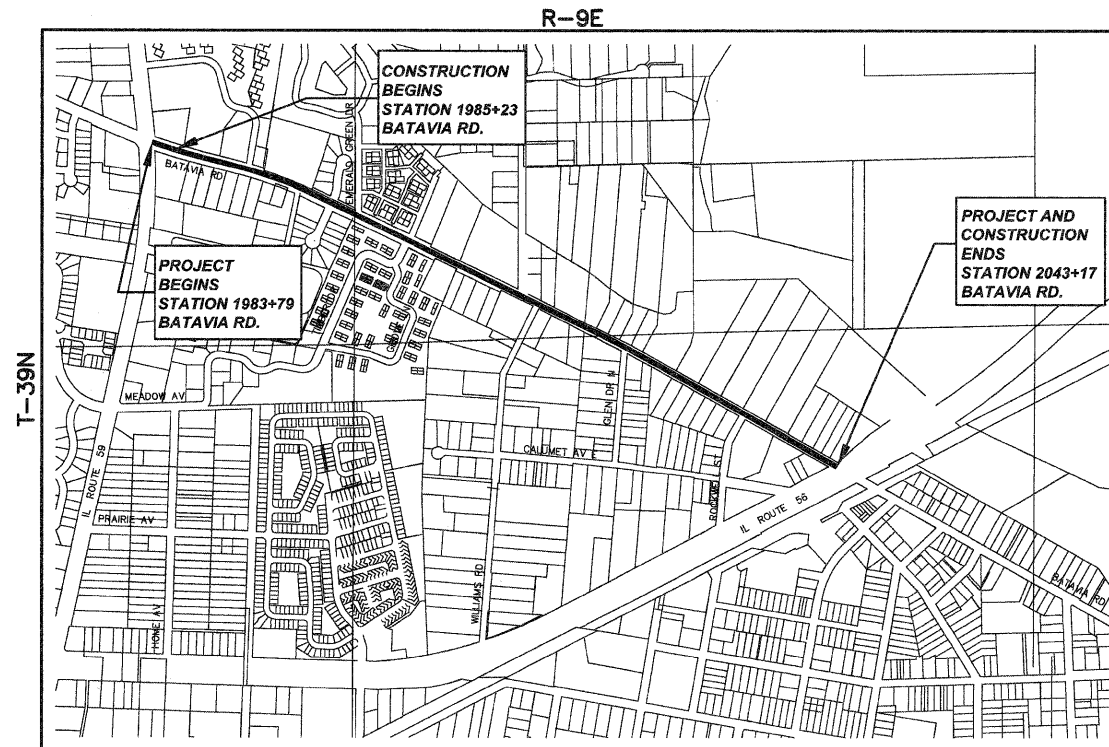
PROJECT LOCATED IN THE CITY OF WARRENVILLE

DESIGN DESIGNATION
BATAVIA ROAD: URBAN MINOR ARTERIAL
DESIGN SPEED = 40 M.P.H.
POSTED SPEED = 35 M.P.H.
ADT(2006) = 8800

FIELD ENGINEER: MARILIN SOLOMON (847) 705-4407
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC.
CONTACT: JASON M. BAUER (630)-466-6700



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.



S 1/2 SEC. 27, NE 1/4 SEC. 34, NW 1/4 SEC 35, T-39N, R-9E 3RD P.M.

LOCATION MAP
N.T.S.

GROSS LENGTH OF PROJECT = 5,938 FEET (1.12 MILES)
NET LENGTH OF PROJECT = 5,794 FEET (1.10 MILES)

TOPOGRAPHICAL INFORMATION FROM SURVEY BY ENGINEERING ENTERPRISES, INC.

JULIE
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811

Know what's below.
Call before you dig.

CONTRACT NO. 63373

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS	
APPROVED	10/16 2009 <i>Michael Smith</i> CITY OF WARRENVILLE, SUPERINTENDENT OF PUBLIC WORKS
PASSED	OCTOBER 26, 2009 <i>Chris Healy</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	OCTOBER 26, 2009 <i>Diana M. O'Keefe</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 10-16-09

BY: *JMB*
JASON M. BAUER

LICENSE EXPIRES: NOVEMBER 30, 2009

SEAL

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
P: 630.466.6700 - W: www.eeiweb.com

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GENERAL NOTES AND CONSTRUCTION SPECIFICATIONS

- THE CONTRACTOR SHALL OBTAIN, ERECT, MAINTAIN AND REMOVE ALL SIGNS, BARRICADES, FLAGGERS AND OTHER TRAFFIC CONTROL DEVICES AS MAY BE NECESSARY FOR THE PURPOSE OF REGULATING, WARNING OR GUIDING TRAFFIC. PLACEMENT AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PARTS OF ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS AND THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.
- LOCATION OF UTILITIES SHOWN ON PLANS ARE APPROXIMATE ONLY, AND ARE NOT NECESSARILY COMPLETE. CONTRACTOR SHALL MAKE HIS OWN INVESTIGATIONS AS TO LOCATION OF ALL EXISTING UNDERGROUND STRUCTURES, CABLES AND PIPE LINES.
- ALL ELEVATIONS ARE BASED ON U.S.G.S. DATUM (NAD 1927).
- ALL ROAD SIGNS, STREET SIGNS AND TRAFFIC SIGNS WHICH NEED TO BE RELOCATED OR MOVED DUE TO CONSTRUCTION SHALL BE TAKEN DOWN AND STORED BY THE CONTRACTOR EXCEPT THOSE THAT ARE NECESSARY FOR PROPER TRAFFIC CONTROL WHICH SHALL BE TEMPORARILY RESET UNTIL COMPLETION OF CONSTRUCTION OPERATIONS. AFTER COMPLETION OF THE WORK, THE CONTRACTOR SHALL RESET ALL SAID SIGNS. THE WORK SHALL BE INCLUDED IN THE TRAFFIC CONTROL PAY ITEMS, AND SHALL NOT BE PAID FOR SEPARATELY.
- SPECIAL ATTENTION IS DRAWN TO THE FACT THAT ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.08.
- ALL REFERENCES TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE DEPARTMENT AS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNER, HIS OR HER AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE UNIT PRICE FOR ALL REMOVAL PAY ITEMS AND BUTT JOINTS SHALL INCLUDE ALL REQUIRED SAW CUTS.
- ALL WORK PERFORMED RELATIVE TO THIS IMPROVEMENT SHALL COMPLY WITH ALL APPLICABLE RULES AND REGULATIONS OF O.S.H.A.
- CONTRACTOR SHALL PROVIDE AND INSTALL TWO WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. (ONE WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.) THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEMS INVOLVED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF WARRENVILLE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- TEMPORARY RAMPS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE THE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

- WHENEVER, DURING CONSTRUCTION OPERATIONS LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT PAY ITEMS.
- EXISTING PUBLIC AND PRIVATE UTILITIES ARE SHOWN ON THE PLANS ACCORDING TO INFORMATION OBTAINED FROM UTILITY COMPANIES, MUNICIPALITIES, AND SURVEYS. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH THE LOCATION OF ALL UTILITIES AND STRUCTURES THAT MAY BE FOUND IN THE VICINITY OF THE CONSTRUCTION. THE CONTRACTOR WILL ALSO ASSUME RESPONSIBILITY FOR ALL UTILITIES WHETHER SHOWN OR NOT, AND MUST REALIZE THAT THE ACTUAL LOCATIONS AND/OR ELEVATIONS OF THE UTILITIES MAY BE DIFFERENT THAN INDICATED.

SHOULD ANY DAMAGES OCCUR DUE TO THE CONTRACTOR'S NEGLIGENCE, THE CONTRACTOR, AT HIS OR HER OWN EXPENSE, SHALL MAKE REPAIRS IN A MANNER ACCEPTABLE TO THE ENGINEER. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES OF HIS OR HER CONSTRUCTION SCHEDULE AND COORDINATE CONSTRUCTION OPERATIONS WITH THE UTILITY COMPANIES SO THAT RELOCATION OF UTILITY LINES AND STRUCTURES MAY PROCEED IN AN ORDERLY MANNER.
- DURING CONSTRUCTION, IF THE CONTRACTOR ENCOUNTERS OR OTHERWISE BECOMES AWARE OF ANY SEWER, UNDERDRAINS OR FIELD DRAINS WITHIN THE RIGHT-OF-WAY OTHER THAN THOSE SHOWN ON THE PLANS, HE OR SHE SHALL SO INFORM THE ENGINEER WHO SHALL DIRECT THE WORK NECESSARY TO MAINTAIN OR TO REPLACE THE FACILITIES IN SERVICE AND TO PROTECT THEM FROM DAMAGE DURING CONSTRUCTION IF TO BE MAINTAINED. EXISTING FACILITIES TO BE MAINTAINED THAT ARE DAMAGED BECAUSE OF NON-COMPLIANCE WITH THIS PROVISION SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
- BATAVIA ROAD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. WHEN NECESSARY TO CLOSE ONE LANE DUE TO CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE-WAY TRAFFIC DURING CONSTRUCTION HOURS WITH THE USE OF SIGNS AND FLAGMEN AS SHOWN ON THE TRAFFIC CONTROL STANDARDS. TWO LANES OF TRAFFIC SHALL BE MAINTAINED AT ALL OTHER TIMES WHEN NO CONSTRUCTION ACTIVITIES ARE BEING CARRIED ON. ACCESS TO ALL ADJOINING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES.
- THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TRAFFIC CONTROL DEVICES.

INDEX OF SHEETS

SHEET NO.	SHEET DESCRIPTION
1.	COVER SHEET AND LOCATION MAP
2.	GENERAL NOTES, STATE STANDARDS, AND INDEX OF SHEETS
3.	SUMMARY OF QUANTITIES
4.	TYPICAL EXISTING AND PROPOSED CROSS SECTIONS
5.	TYPICAL EXISTING AND PROPOSED CROSS SECTIONS
6.	TYPICAL EXISTING AND PROPOSED CROSS SECTIONS
7.	PLAN VIEW - STA 1983+00 TO STA 2000+00
8.	PLAN VIEW - STA 2000+00 TO STA 2018+00
9.	PLAN VIEW - STA 2018+00 TO STA 2036+00
10.	PLAN VIEW - STA 2036+00 TO STA 2043+17
11.	PAVEMENT MARKING DETAILS - STA 1983+00 TO STA 2000+00
12.	PAVEMENT MARKING DETAILS - STA 2000+00 TO STA 2018+00
13.	PAVEMENT MARKING DETAILS - STA 2018+00 TO STA 2036+00
14.	PAVEMENT MARKING DETAILS - STA 2036+00 TO STA 2043+17
15.	RECESSED REFLECTIVE PAVEMENT MARKER

DISTRICT ONE CADD DETAILS

16.	(TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
17.	(TC-11) TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
18.	(TC-13) TYPICAL PAVEMENT MARKINGS
19.	(TC-14) TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
20.	(TC-22) ARTERIAL ROAD INFORMATION SIGN
21.	(BD-01) DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15'(4.5M)
22.	(BD-02) DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB < 15'(4.5M)
23.	(BD-08) DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
24.	(BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
25.	(BD-24) CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
26.	(BD-32) BUTT JOINT AND HMA TAPER DETAILS
27.	(TS-07) DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

STATE STANDARDS

STD. NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-03	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL TURN LANE
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

LEGEND

EXISTING	DESCRIPTION	PROPOSED
	SANITARY SEWER	
	STORM SEWER	
	END SECTION	
	WATER MAIN & SIZE	
	WATER SERVICE & BOX	
	CONTOUR	
	GAS MAIN	
	TELEPHONE CABLE	
	MANHOLE	
	CATCH BASIN	
	INLET	
	HYDRANT	
	VALVE VAULT	
	TREE	
	ELEVATION	
	STREET LIGHT SIGNS	
	FOUND IRON PIPE	
	GUY WIRE	
	FLAG POLE	
	UTILITY POLE	
	UTILITY PEDESTAL	
	HANDHOLE	

Plotted: October 15, 2009 @ 10:20 AM By: Terri Seidler-Johnson - Tab: 02 Notes 22x34

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CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

**GENERAL NOTES,
STATE STANDARDS,
AND INDEX OF SHEETS**

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-CVR
SHEET	2 OF 27

Path: H:\S05KPROJ\W0902\DWG\W0902\DWG\W0902-CVR.DWG

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	CONST. CODE
			1000
			TOTAL QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	2,432
40600300	AGGREGATE (PRIME COAT)	TON	24
40600895	CONSTRUCTING TEST STRIP	EACH	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	245
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	2,798
Δ 42400440	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH, SPECIAL	SQ FT	86
Δ 42400800	DETECTABLE WARNINGS	SQ FT	280
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	24,305
Δ 44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,110
Δ 44004610	SIDEWALK REMOVAL AND REPLACEMENT (SPECIAL)	SQ FT	1,071
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	250
48102100	AGGREGATE WEDGE SHOULDERS, TYPE B	TON	740
60261300	INLETS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	2
Δ 60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13
67100100	MOBILIZATION	L SUM	1
Δ 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
Δ 70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1
Δ 70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
Δ 70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	4,500
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,500
72000100	SIGN PANEL - TYPE 1	SQ FT	8
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	15
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	587.6
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18,090
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2,670
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	50
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,527
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	237
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	214
Δ * 88600600	DETECTOR LOOP REPLACEMENT	FOOT	249
Δ X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	77.1
Δ X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,399
Δ XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	70
Δ * XX006257	RECESSED REFLECTIVE PAVEMENT MARKER	EACH	214
Δ XX006425	RESTORATION	SQ YD	573
Δ XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	1,364
Δ XX007369	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT, 8"	SQ YD	283

Δ SEE SPECIAL PROVISIONS
* SPECIALTY ITEMS

Plotted: October 22, 2009 @ 2:07 PM By: Terri Seidler-Johnson - Tab: 03 Sum of Quant: 22x34

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CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

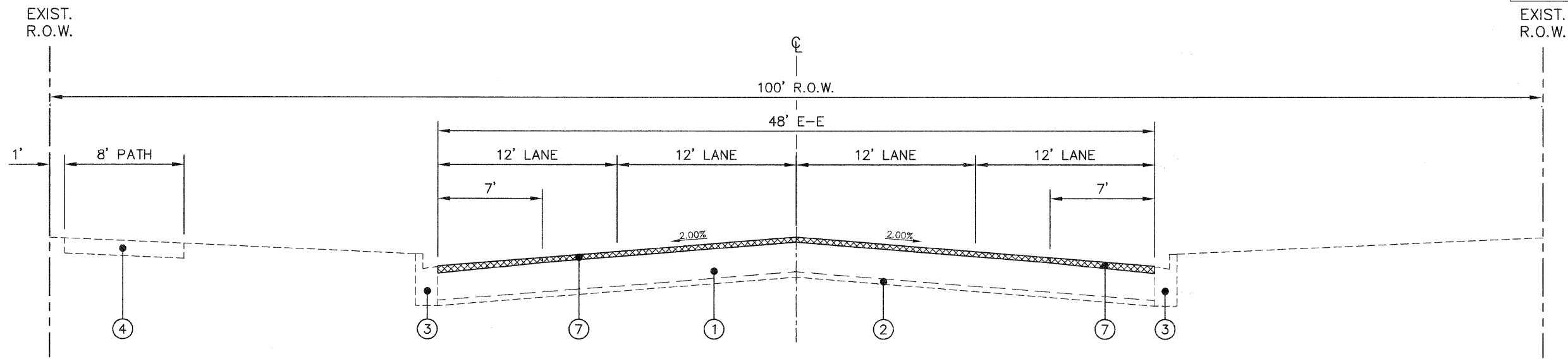
NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

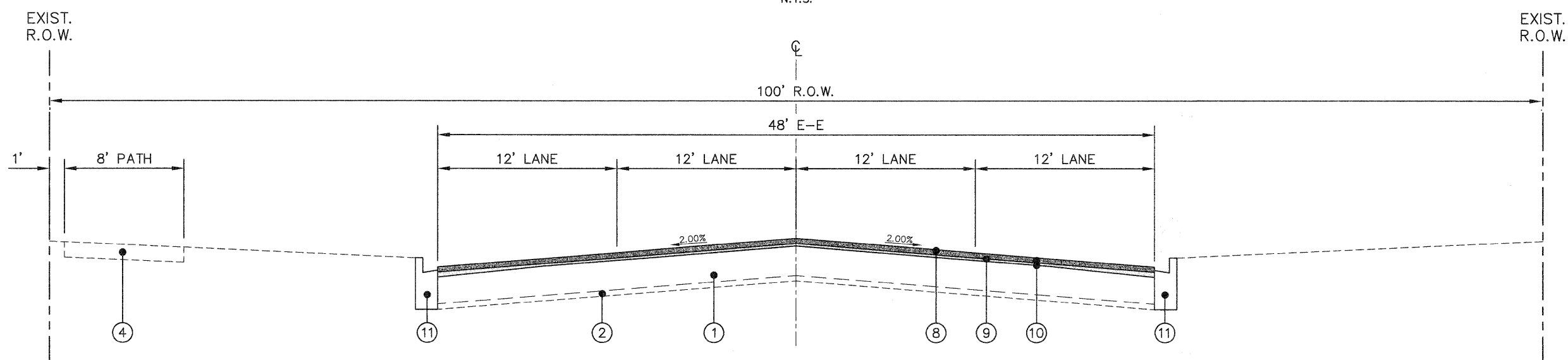
SUMMARY OF QUANTITIES

DATE: OCTOBER 2009
PROJECT NO: WV0902
FILE: WV0902-CVR
SHEET **3** OF **27**

Path: H:\S054PROJ\W0902\DWG\DWG_FINAL_ETG\W0902-CVR



EXISTING TYPICAL SECTION
FROM STATION 1985+23 TO STATION 1991+50, BATAVIA ROAD
N.T.S.



PROPOSED TYPICAL SECTION
FROM STATION 1985+23 TO STATION 1991+50, BATAVIA ROAD
N.T.S.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	4% @ 50 Gyr.
	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm)	4% @ 50 Gyr.
DRIVEWAYS	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	
	HMA SURFACE COURSE, MIX "C", N50 (IL 9.5 mm); 2.0"	4% @ 50 Gyr.
	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT, 8"	
	HMA SURFACE COURSE, MIX "C", N50 (IL 9.5 mm); 2"	4% @ 50 Gyr.
	HMA BASE COURSE (HMA BINDER IL-19 mm); 6" (TO BE PLACED IN TWO LIFTS)	4% @ 50 Gyr.
PATCHING	CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

PATCHING SHALL BE PERFORMED AFTER MILLING OPERATIONS

LEGEND	
① EXISTING 15" FULL-DEPTH ASPHALT PAVEMENT	⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
② EXISTING AGGREGATE BASE COURSE	⑧ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
③ EXISTING 8-6.12 COMBINATION CONCRETE CURB AND GUTTER	⑨ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"
④ EXISTING PORTLAND CEMENT CONCRETE BIKE PATH	⑩ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
⑤ EXISTING AGGREGATE SHOULDER	⑪ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)
⑥ AGGREGATE WEDGE SHOULDER, TYPE "B"	

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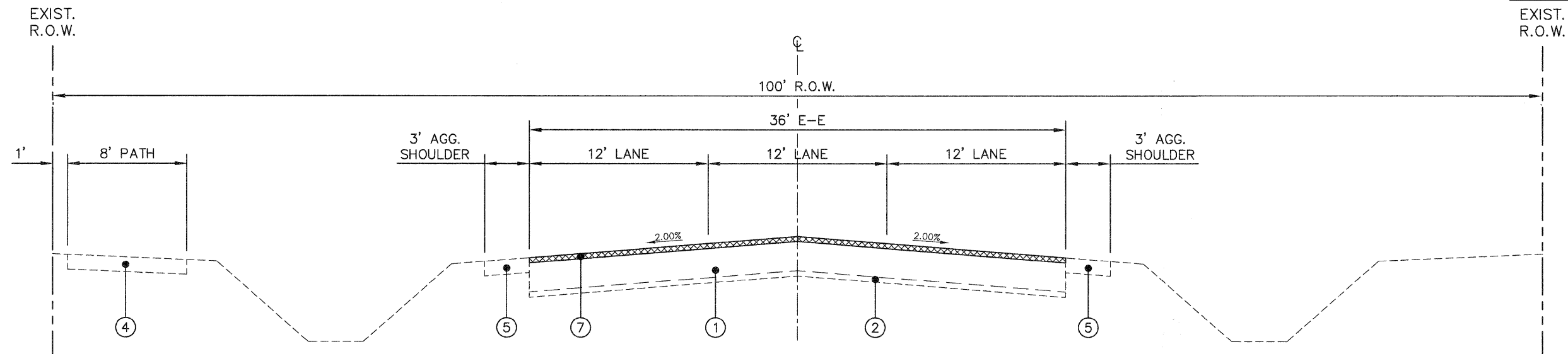
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DUPAGE COUNTY, ILLINOIS

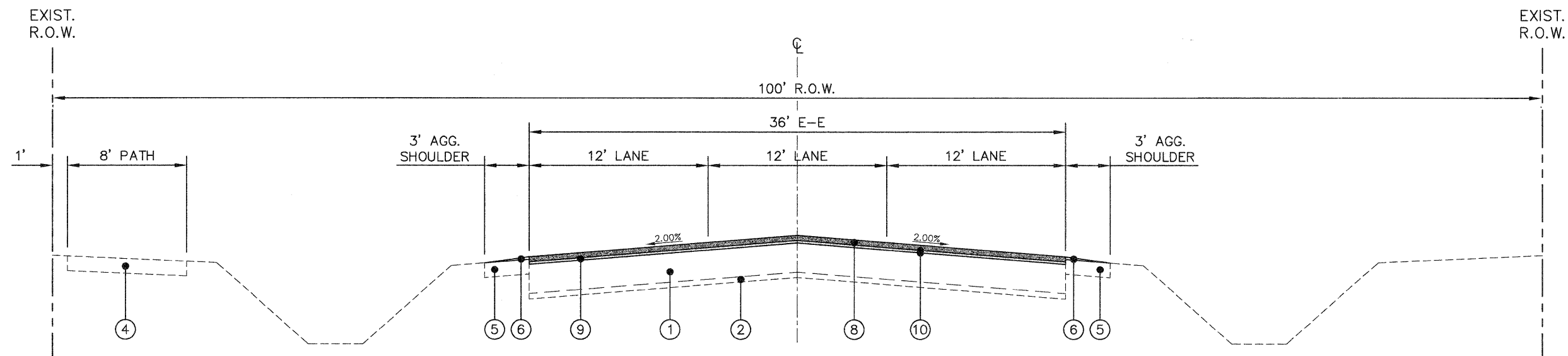
NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

TYPICAL EXISTING AND PROPOSED CROSS SECTIONS



EXISTING TYPICAL SECTION
FROM STATION 1991+50 TO STATION 2015+00, BATAVIA ROAD
N.T.S.



PROPOSED TYPICAL SECTION
FROM STATION 1991+50 TO STATION 2015+00, BATAVIA ROAD
N.T.S.

LEGEND

- | | |
|--|---|
| ① EXISTING 15" FULL-DEPTH ASPHALT PAVEMENT | ⑦ HOT-MIX ASPHALT SURFACE REMOVAL, 3" |
| ② EXISTING AGGREGATE BASE COURSE | ⑧ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0" |
| ③ EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER | ⑨ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0" |
| ④ EXISTING PORTLAND CEMENT CONCRETE BIKE PATH | ⑩ BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT) |
| ⑤ EXISTING AGGREGATE SHOULDER | ⑪ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS) |
| ⑥ AGGREGATE WEDGE SHOULDER, TYPE "B" | |

Plotted: October 15, 2009 @ 10:17 AM By: Terri Seider-Johnson - Tab: 05 X-Sections 22x34

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CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

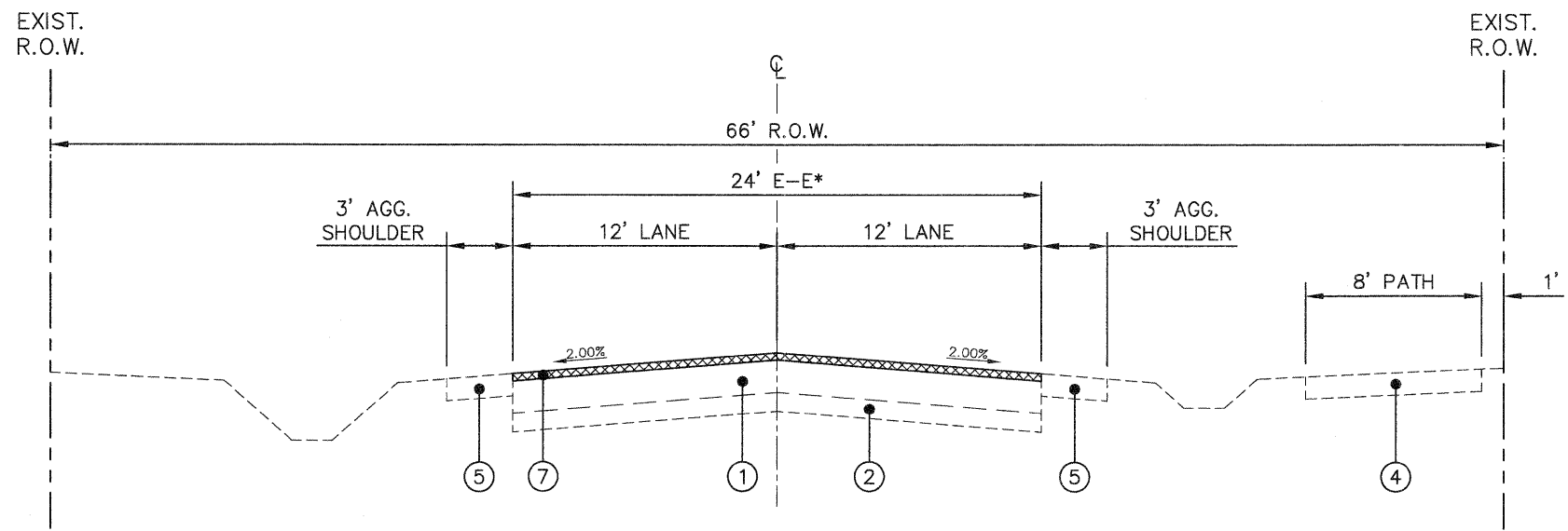
BATAVIA ROAD
LAPP IMPROVEMENTS

TYPICAL EXISTING AND PROPOSED CROSS SECTIONS

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-CVR
SHEET	5 OF 27

Path: \\S05KPR01\W0902\DWG\W0902.DWG

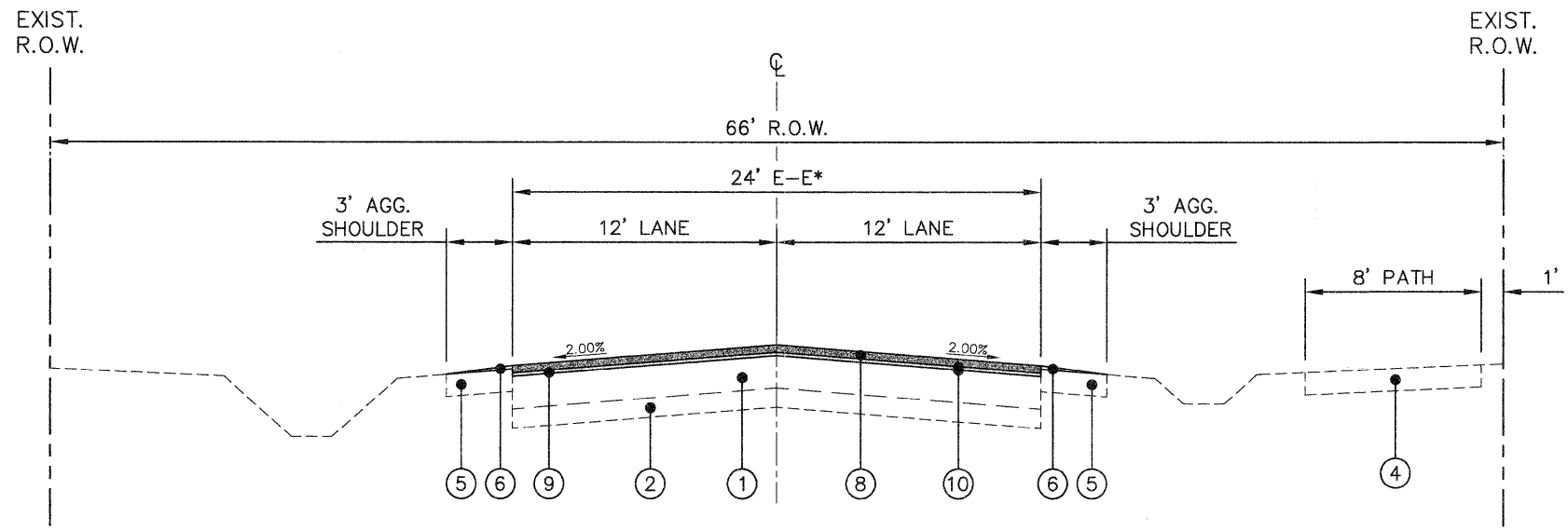
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3553	09-00029-00-RS	DUPAGE	27	6
CONTRACT NO. 63373				



EXISTING TYPICAL SECTION

FROM STATION 2015+00 TO STATION 2043+17, BATAVIA ROAD
N.T.S.

* NOTE:
ROADWAY WIDENS AND TURN
LANES ARE ADDED AT IL 56



PROPOSED TYPICAL SECTION

FROM STATION 2015+00 TO STATION 2043+17, BATAVIA ROAD
N.T.S.

LEGEND	
①	EXISTING 15" FULL-DEPTH ASPHALT PAVEMENT
②	EXISTING AGGREGATE BASE COURSE
③	EXISTING B-6.12 COMBINATION CONCRETE CURB AND GUTTER
④	EXISTING PORTLAND CEMENT CONCRETE BIKE PATH
⑤	EXISTING AGGREGATE SHOULDER
⑥	AGGREGATE WEDGE SHOULDER, TYPE "B"
⑦	HOT-MIX ASPHALT SURFACE REMOVAL, 3"
⑧	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
⑨	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1.0"
⑩	BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
⑪	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AT VARIOUS LOCATIONS)

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DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

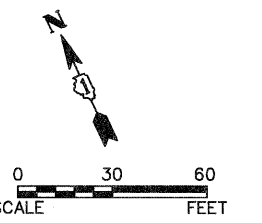
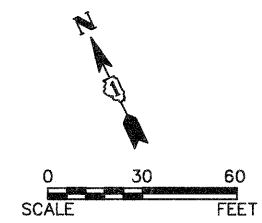
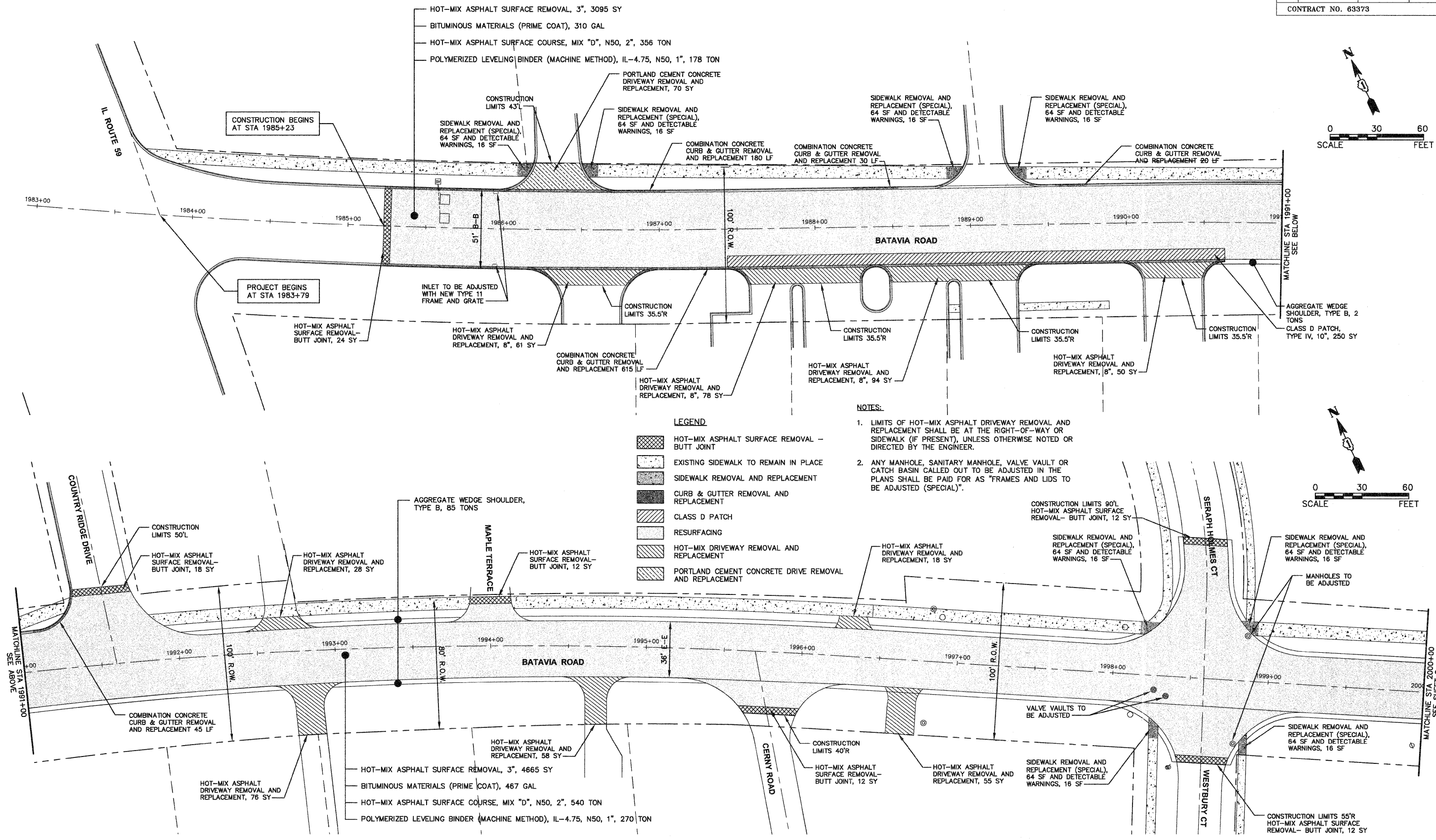
BATAVIA ROAD
LAPP IMPROVEMENTS

TYPICAL EXISTING AND PROPOSED CROSS SECTIONS

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-CVR
SHEET	6 OF 27

Path: H:\SP5K\PROJ\WV0902\DWG\WV0902.DWG FINAL ENG\WV0902-CVR

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3553	09-00029-00-RS	DUPAGE	27	7
CONTRACT NO. 63373				



- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
 - EXISTING SIDEWALK TO REMAIN IN PLACE
 - SIDEWALK REMOVAL AND REPLACEMENT
 - CURB & GUTTER REMOVAL AND REPLACEMENT
 - CLASS D PATCH
 - RESURFACING
 - HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
 - PORTLAND CEMENT CONCRETE DRIVE REMOVAL AND REPLACEMENT

- NOTES:**
- LIMITS OF HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE AT THE RIGHT-OF-WAY OR SIDEWALK (IF PRESENT), UNLESS OTHERWISE NOTED OR DIRECTED BY THE ENGINEER.
 - ANY MANHOLE, SANITARY MANHOLE, VALVE VAULT OR CATCH BASIN CALLED OUT TO BE ADJUSTED IN THE PLANS SHALL BE PAID FOR AS "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)".

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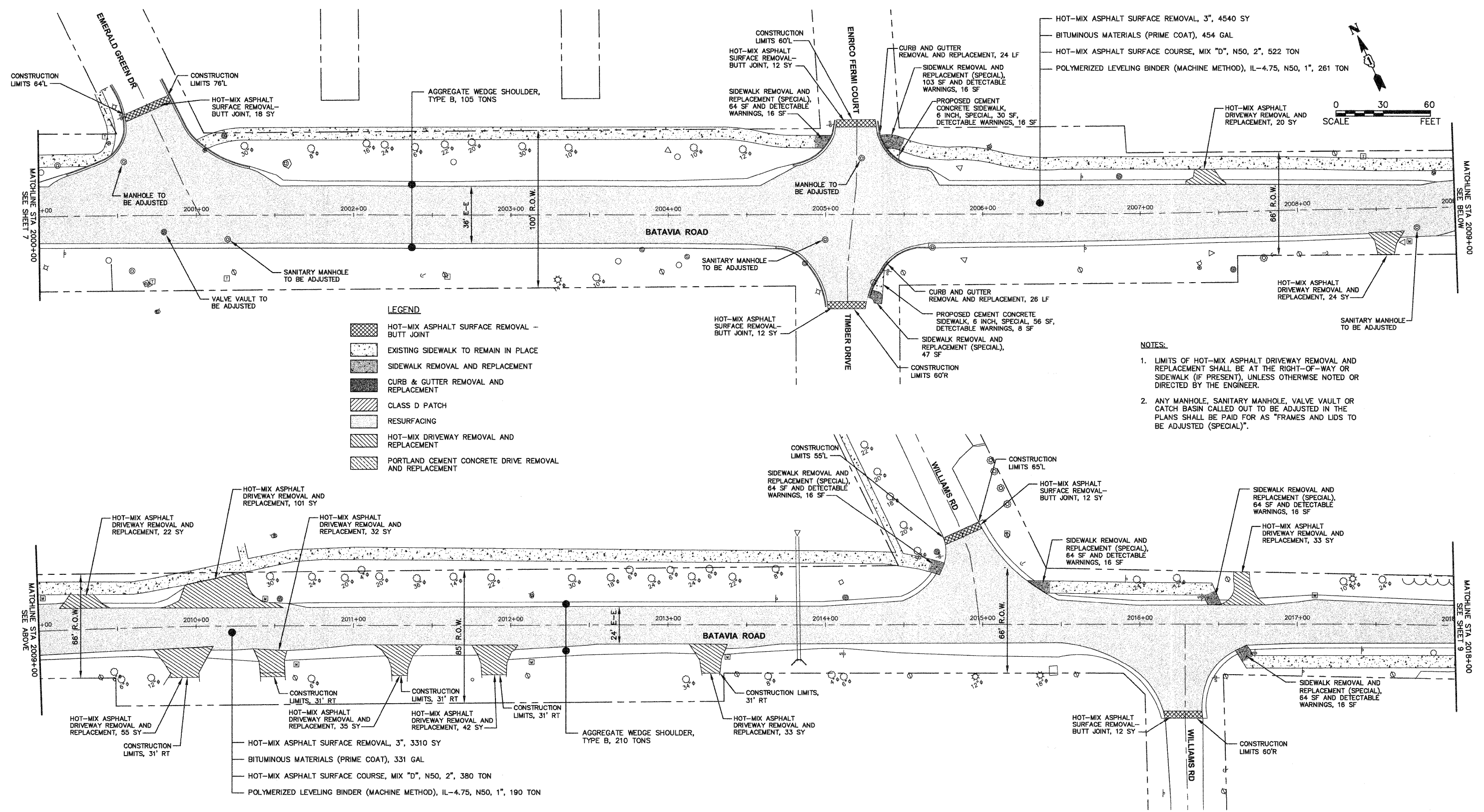
NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

PLAN VIEW
 STA 1983+00 TO STA 2000+00

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-PLAN
 SHEET **7** OF **27**

Path: \\S:\S\PROJ\WV0902\DWG\WV0902.DWG FINAL ENG\WV0902-PLAN



- NOTES:**
- LIMITS OF HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE AT THE RIGHT-OF-WAY OR SIDEWALK (IF PRESENT), UNLESS OTHERWISE NOTED OR DIRECTED BY THE ENGINEER.
 - ANY MANHOLE, SANITARY MANHOLE, VALVE VAULT OR CATCH BASIN CALLED OUT TO BE ADJUSTED IN THE PLANS SHALL BE PAID FOR AS "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)".

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NO.	DATE	REVISIONS

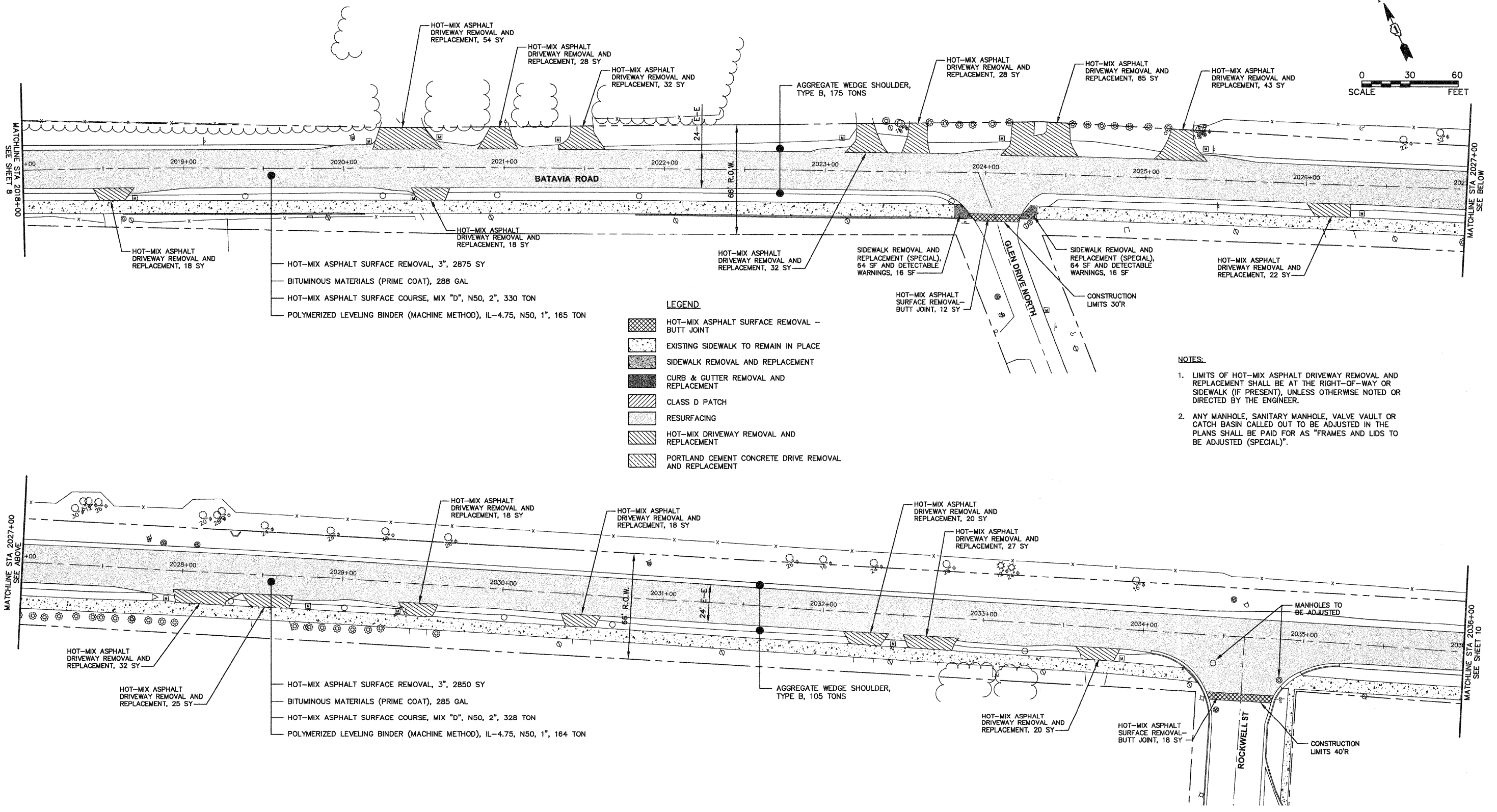
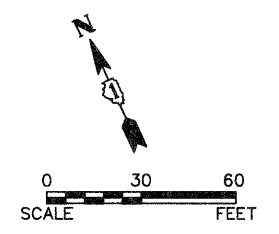
BATAVIA ROAD
LAPP IMPROVEMENTS

PLAN VIEW
STA 2000+00 TO STA 20018+00

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-PLAN
SHEET	8 OF 27

Path: H:\SOS\PROJ\WV0903\DWG\WV0902\DWG\FINAL\ENG\WV0902-PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3553	09-00029-00-RS	DUPAGE	27	9
CONTRACT NO. 63373				



- NOTES:**
- LIMITS OF HOT-MIX ASPHALT REMOVAL AND REPLACEMENT SHALL BE AT THE RIGHT-OF-WAY OR SIDEWALK (IF PRESENT), UNLESS OTHERWISE NOTED OR DIRECTED BY THE ENGINEER.
 - ANY MANHOLE, SANITARY MANHOLE, VALVE VAULT OR CATCH BASIN CALLED OUT TO BE ADJUSTED IN THE PLANS SHALL BE PAID FOR AS "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)".

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 DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

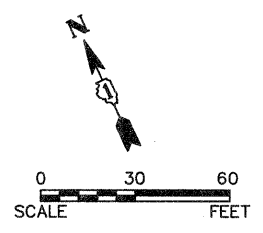
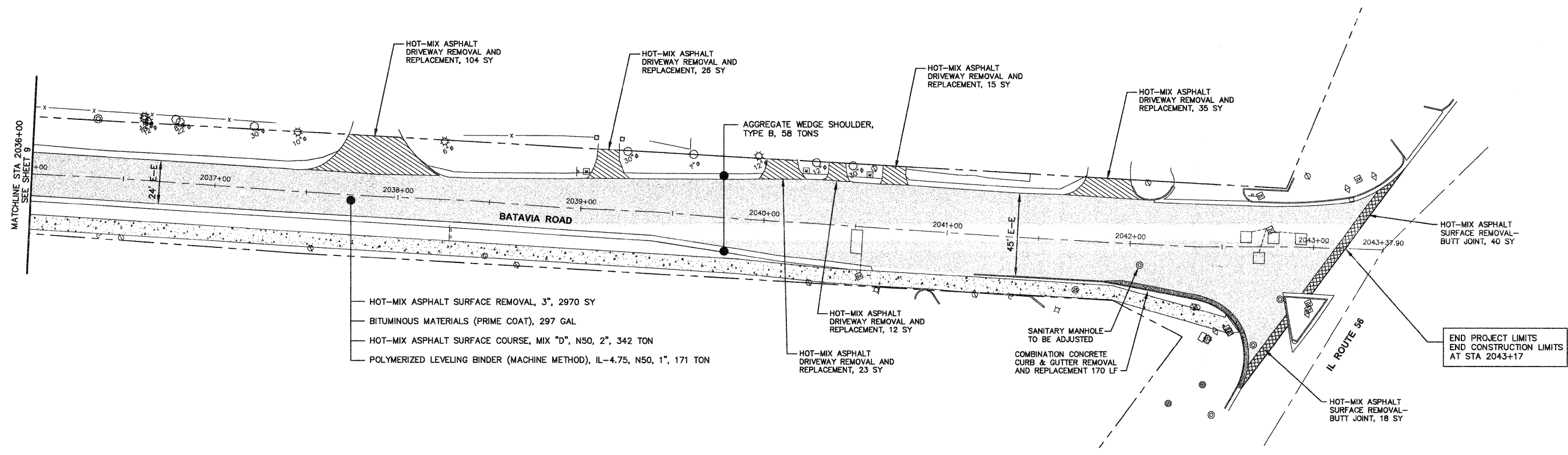
BATAVIA ROAD
LAPP IMPROVEMENTS

PLAN VIEW
STA 2018+00 TO STA 2036+00

DATE:	OCTOBER 2009
PROJECT NO.:	WV0902
FILE:	WV0902-PLAN
SHEET	9 OF 27

Path: H:\S056PROJ\WV0902\DWG\WV0902_Plan.dwg

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3553	09-00029-00-RS	DUPAGE	27	10
CONTRACT NO. 63373				



- HOT-MIX ASPHALT SURFACE REMOVAL, 3", 2970 SY
- BITUMINOUS MATERIALS (PRIME COAT), 297 GAL
- HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2", 342 TON
- POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1", 171 TON

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- EXISTING SIDEWALK TO REMAIN IN PLACE
- SIDEWALK REMOVAL AND REPLACEMENT
- CURB & GUTTER REMOVAL AND REPLACEMENT
- CLASS D PATCH
- RESURFACING
- HOT-MIX DRIVEWAY REMOVAL AND REPLACEMENT
- PORTLAND CEMENT CONCRETE DRIVE REMOVAL AND REPLACEMENT

NOTES:

1. LIMITS OF HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT SHALL BE AT THE RIGHT-OF-WAY OR SIDEWALK (IF PRESENT), UNLESS OTHERWISE NOTED OR DIRECTED BY THE ENGINEER.
2. ANY MANHOLE, SANITARY MANHOLE, VALVE VAULT OR CATCH BASIN CALLED OUT TO BE ADJUSTED IN THE PLANS SHALL BE PAID FOR AS "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)".

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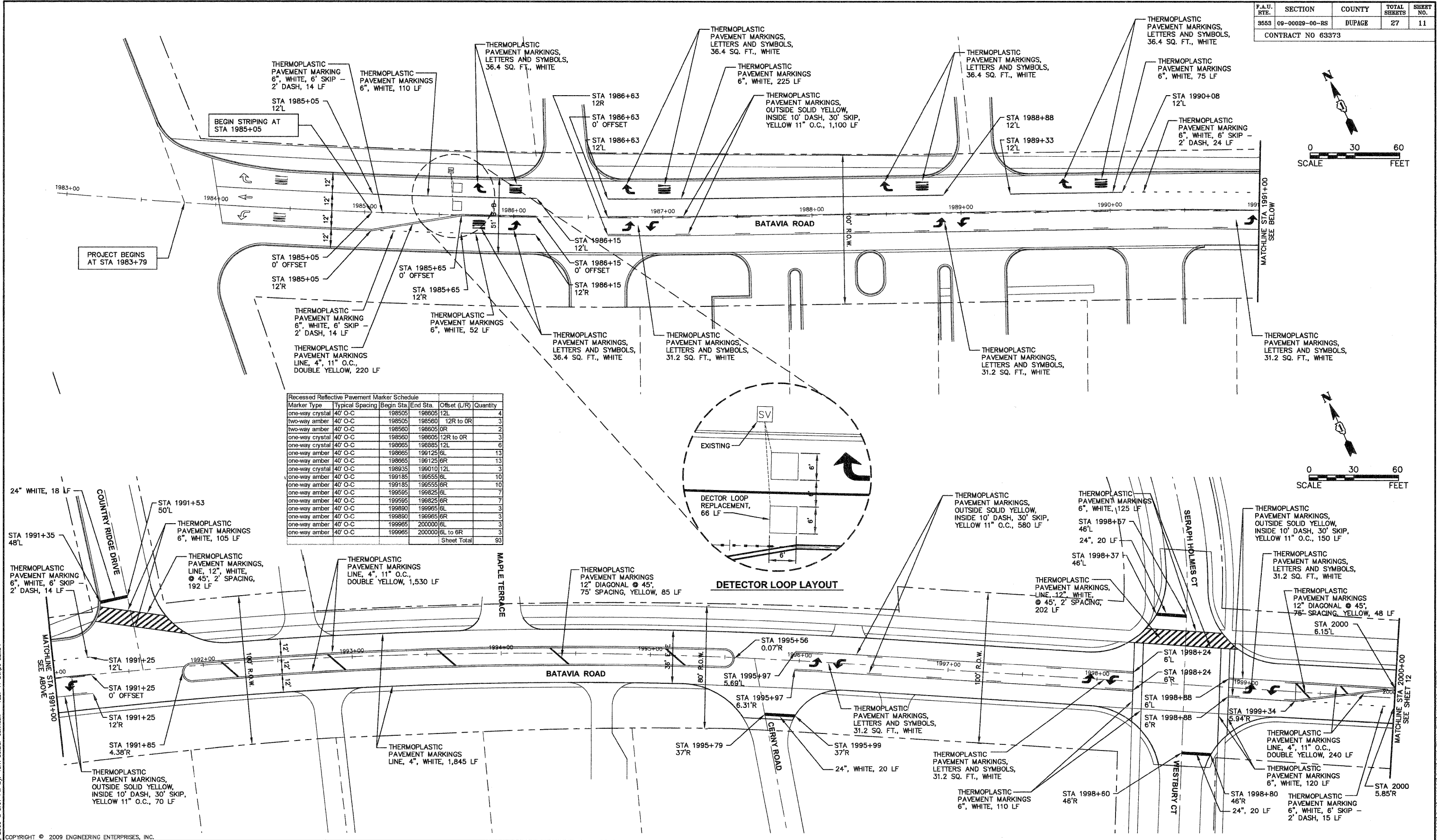
NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

PLAN VIEW
 STA 2036+00 TO STA 2043+17

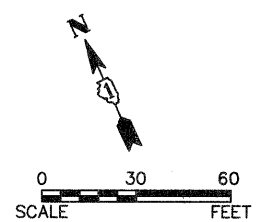
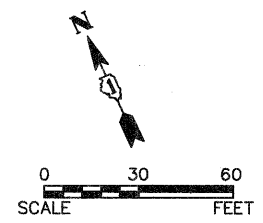
DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-PLAN
SHEET	10 OF 27

Path: H:\S\SR\PROJ\WV0902\DWG\WV0902.DWG FINAL ENG WV0902-PLAN



Recessed Reflective Pavement Marker Schedule

Marker Type	Typical Spacing	Begin Sta	End Sta	Offset (L/R)	Quantity
one-way crystal	40' O.C.	198505	198605	12L	4
two-way amber	40' O.C.	198505	198560	12R to 0R	3
two-way amber	40' O.C.	198560	198605	0R	2
one-way crystal	40' O.C.	198560	198605	12R to 0R	3
one-way crystal	40' O.C.	198665	198885	12L	6
one-way amber	40' O.C.	198665	198125	6L	13
one-way amber	40' O.C.	198665	198125	6R	13
one-way crystal	40' O.C.	198935	199010	12L	3
one-way amber	40' O.C.	199185	198555	6L	10
one-way amber	40' O.C.	199185	198555	6R	10
one-way amber	40' O.C.	199595	198825	6L	7
one-way amber	40' O.C.	199595	198825	6R	7
one-way amber	40' O.C.	199880	199685	6L	3
one-way amber	40' O.C.	199880	199685	6R	3
one-way amber	40' O.C.	199985	200000	6L	3
one-way amber	40' O.C.	199985	200000	6L to 6R	3
Sheet Total					93



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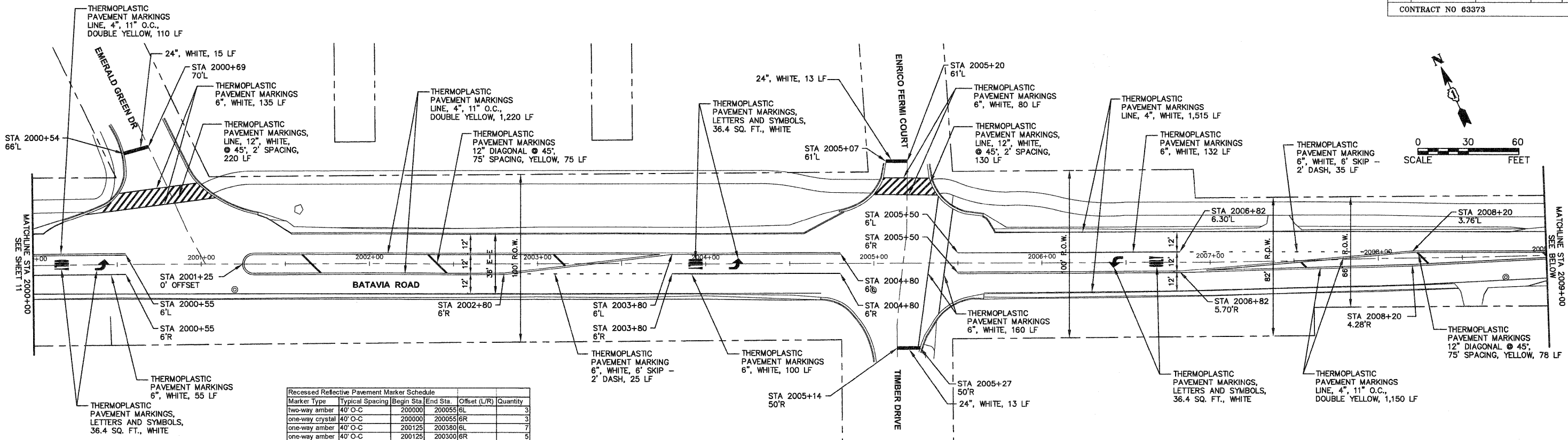
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NO.	DATE	REVISIONS

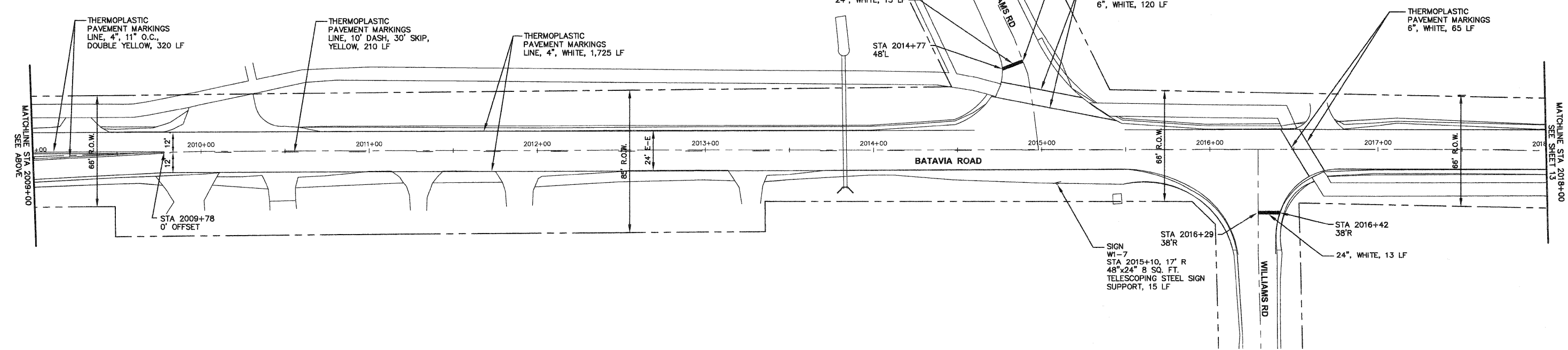
BATAVIA ROAD
 LAPP IMPROVEMENTS

PAVEMENT MARKING DETAILS
 STA 1983+00 TO STA 2000+00

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-STRIPE
 SHEET **11** OF **27**



Marker Type	Typical Spacing	Begin Sta	End Sta	Offset (L/R)	Quantity
two-way amber	40' O-C	200000	200055	6L	3
one-way crystal	40' O-C	200000	200055	6R	3
one-way amber	40' O-C	200125	200380	6L	7
one-way amber	40' O-C	200125	200300	6R	5
one-way amber	40' O-C	200300	200380	6R to 6L	3
one-way crystal	40' O-C	200380	200480	6R	3
two-way amber	40' O-C	200380	200480	6R	3
one-way crystal	40' O-C	200550	200680	6L	4
two-way amber	40' O-C	200550	200680	6R	4
one-way amber	40' O-C	200680	200820	6R	4
one-way amber	40' O-C	200680	200820	6R to 6L	4
one-way amber	40' O-C	200820	200980	6R to 0R	6
one-way amber	40' O-C	200820	200980	6L to 0L	6
two-way amber	80' O-C	200980	201800	0L	12
Sheet Total					67



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NO.	DATE	REVISIONS

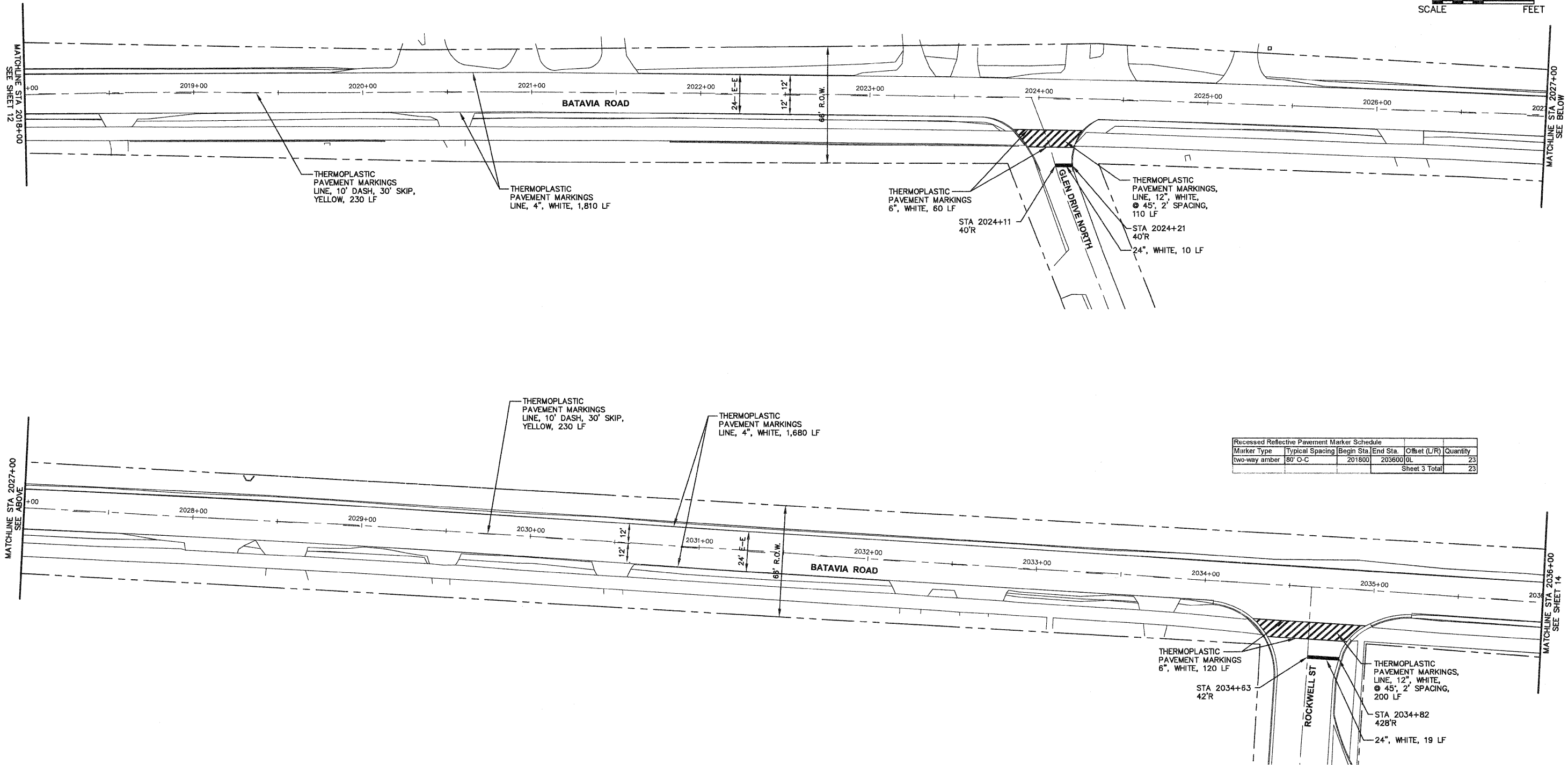
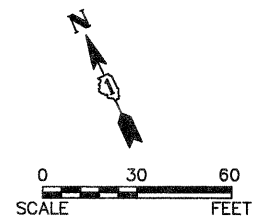
BATAVIA ROAD
 LAPP IMPROVEMENTS

PAVEMENT MARKING DETAILS
 STA 2000+00 TO STA 2008+00

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-STRIPE
 SHEET **12** OF **27**

P:\0903\DWG\WV0902\DWG\ENG\WV0902-STRIPE

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3553	09-00029-00-RS	DUPAGE	27	13
CONTRACT NO 63373				



Marker Type	Typical Spacing	Begin Sta	End Sta	Offset (L/R)	Quantity
two-way amber	80' O-C	201800	203600	0L	23
Sheet 3 Total					23

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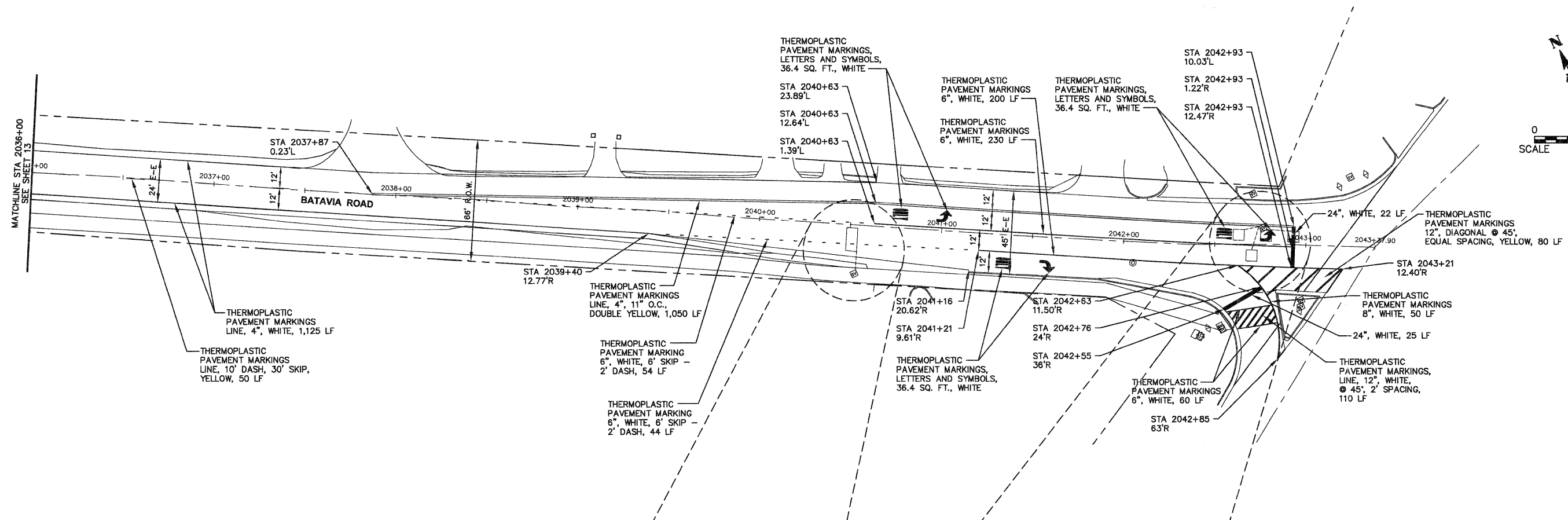
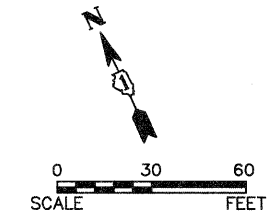
CITY OF WARRENVILLE
 DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

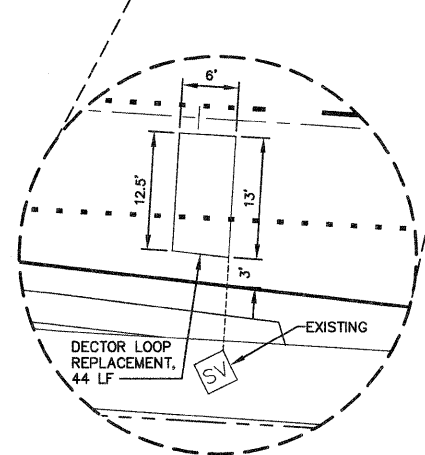
BATAVIA ROAD
LAPP IMPROVEMENTS

PAVEMENT MARKING DETAILS
STA 2018+00 TO STA 2036+00

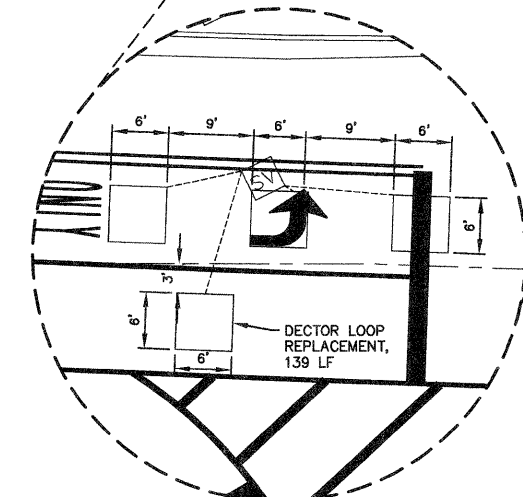
DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-STRIPE
 SHEET **13** OF **27**



Recessed Reflective Pavement Marker Schedule					
Marker Type	Typical Spacing	Begin Sta.	End Sta.	Offset (L/R)	Quantity
two-way amber	80' O-C	203900	203775	0L	2
two-way amber	40' O-C	203775	204070	0L to 12L	9
two-way amber	40' O-C	204070	204290	12L	7
one-way crystal	40' O-C	204070	204290	0L to 12L	7
one-way crystal	40' O-C	204120	204290	10R	6
Sheet 4 Total					31



DETECTOR LOOP LAYOUT



DETECTOR LOOP LAYOUT

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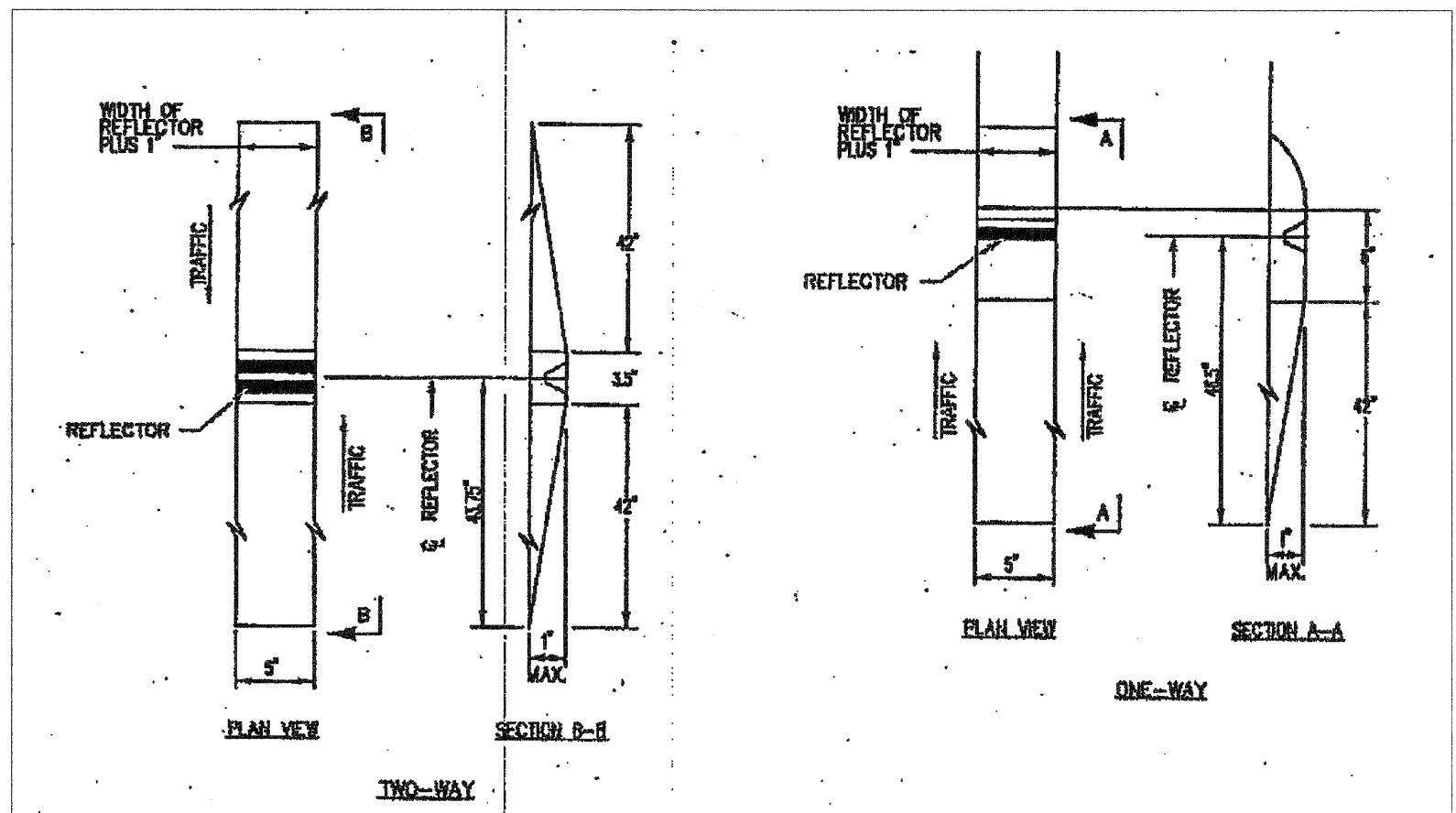
NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

PAVEMENT MARKING DETAILS
 STA 2036+00 TO STA 2043+17

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-STRIPE
 SHEET **14** OF **27**

P:\CH\SS\SS\PROJ\WV0902\DWG\WV0902-DWG-FINAL-ENG\WV0902-STRIPE



RECESSED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES:

1. INSTALLATION SHALL CONFORM TO IDOT DISTRICT ONE DETAIL (TC-11) OR LATEST FOR MARKER REPLACEMENT.
2. IDOT DISTRICT ONE DETAIL (TC-11) SHALL BE MODIFIED TO REFLECT RECESSED PAVEMENT MARKERS INSTEAD OF RAISED PAVEMENT MARKERS.

INSTALLATION NOTES:

1. Saw cut to dimensions shown.
2. Sawcut areas to be dry and free of material that adversely affects the adhesive bond.
3. Install the reflector with an approved two-component epoxy adhesive. Epoxy should not obscure or block the lens.
4. Install top of reflector 1/2 to 1/4 inch below the pavement surface.
5. Reflector shall be 3M Series 190.

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NO.	DATE	REVISIONS

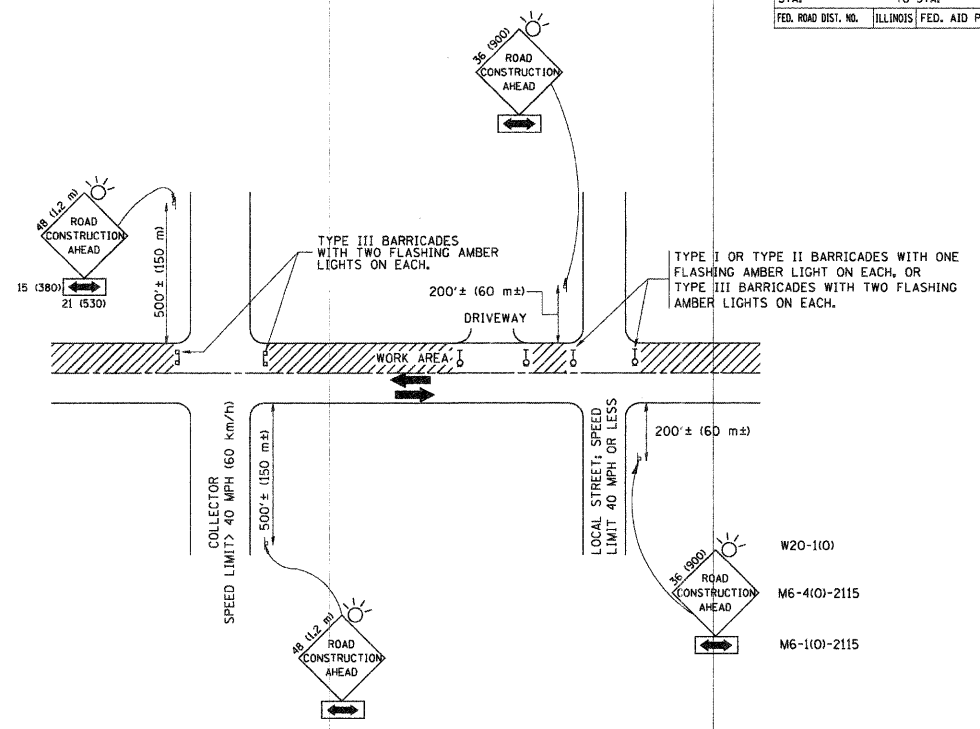
BATAVIA ROAD
 LAPP IMPROVEMENTS

RECESSED REFLECTIVE
 PAVEMENT MARKER

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-CVR
 SHEET **15** OF **27**

Path: H:\S056PROJ\W0902\DWG\W0902\DWG\FINAL_ENC\W0902-CVR

CONTRACT NO.				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAYS:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/7/95
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

SCALE: NONE
 DRAWN BY
 CHECKED BY
 TC-10

PLT DATE: 3/6/2007
 PLT SCALE: 1/8" = 1'-0"
 USER NAME: bward

Plotted: October 9, 2009 @ 8:31 AM By: Terri Seidler-Johnson - Tab: 16 TC-10 22a34

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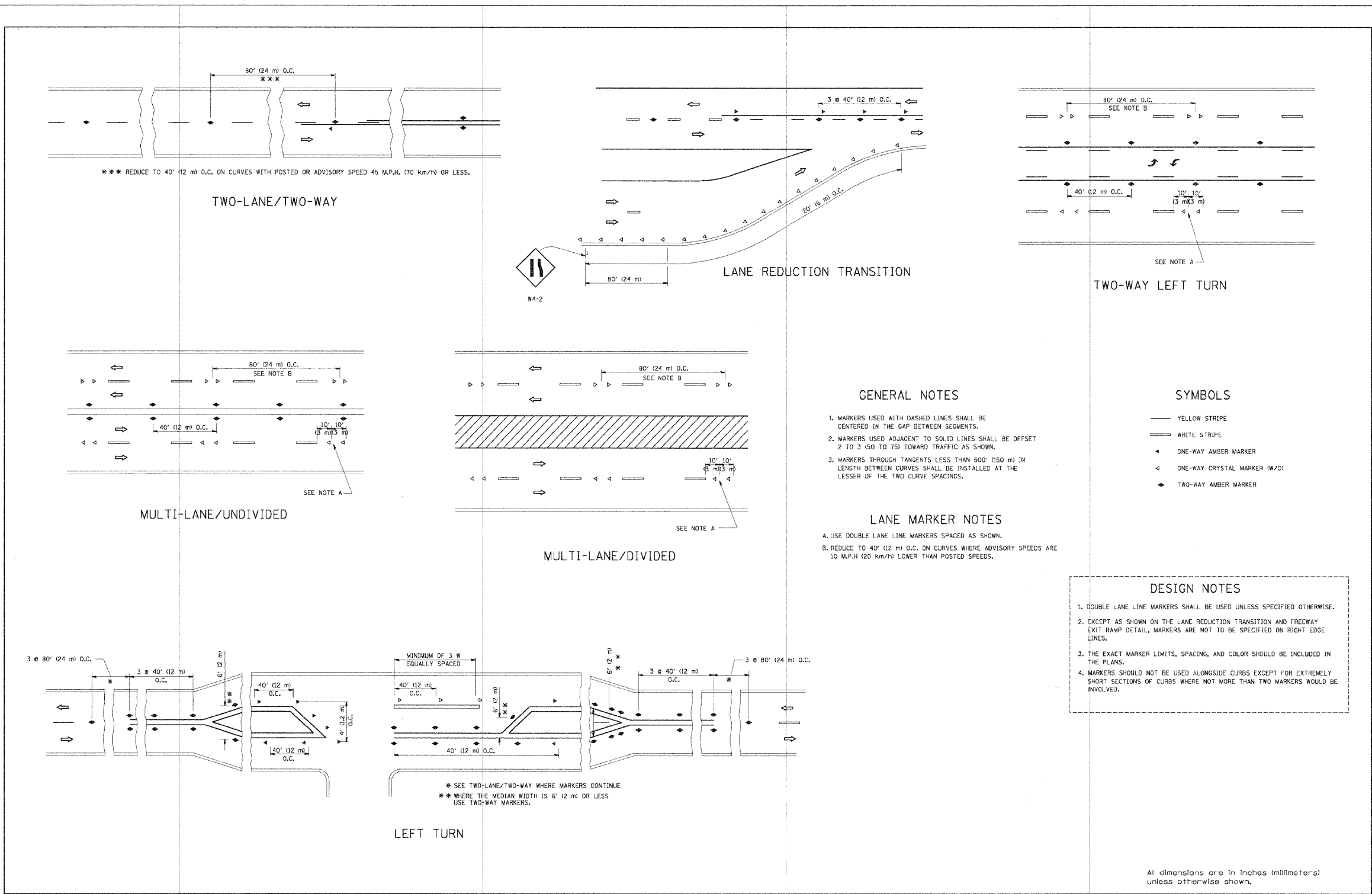
NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

TRAFFIC CONTROL AND PROTECTION
 FOR SIDE ROADS, INTERSECTIONS,
 AND DRIVEWAYS - TC-10

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-CVR
 SHEET **16** OF **27**

Path: H:\SOS\PROJ\WV0902\DWG\WV0902-TC-10.DWG FINAL ENG\WV0902-CVR



GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/D)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drsvkogn	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRW =	DRAWN -	REVISED - T. RAMMACHER 03-12-99									
PLOT SCALE = 5/8" = 1'	CHECKED -	REVISED - T. RAMMACHER 01-06-00									
PLOT DATE = 10/9/2009	DATE -	REVISED - C. JUCIUS 09-09-09									
						SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	
						TC-11 CONTRACT NO. _____ FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT					

Plotted: October 9, 2009 @ 8:27 AM By: Terri Seldier-Johnson - Tab: 17 TC-11 22x34

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CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

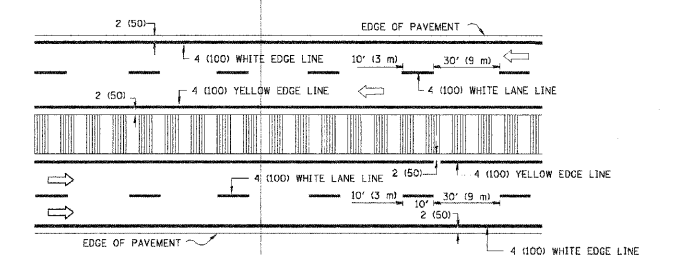
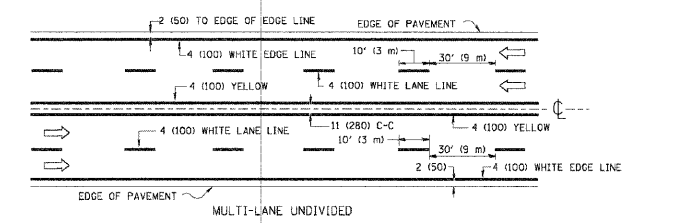
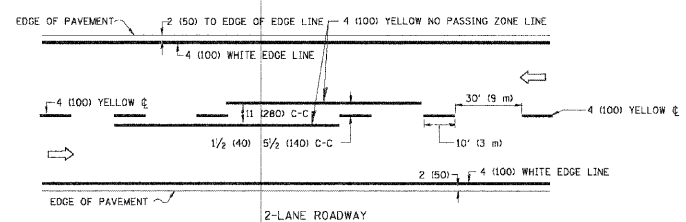
NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
TC-11

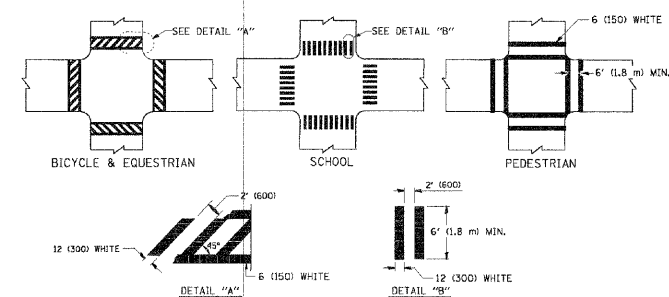
DATE:	OCTOBER 2009
PROJECT NO.:	WV0902
FILE:	WV0902-CVR
SHEET	17 OF 27

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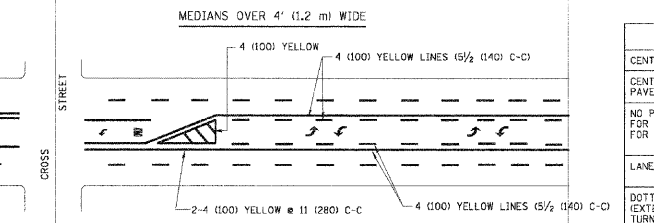
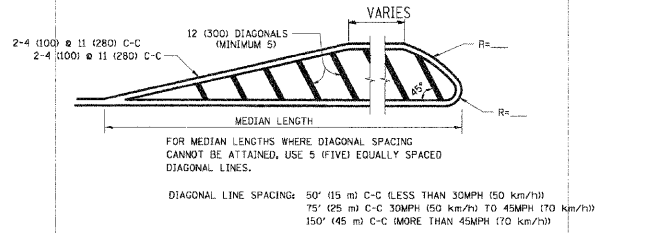
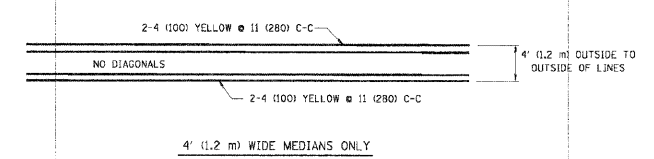


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

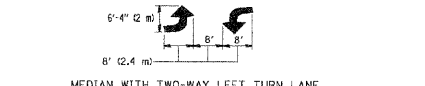
TYPICAL LANE AND EDGE LINE MARKING



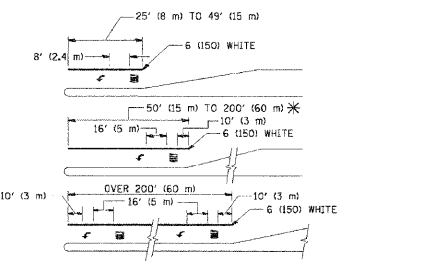
TYPICAL CROSSWALK MARKING



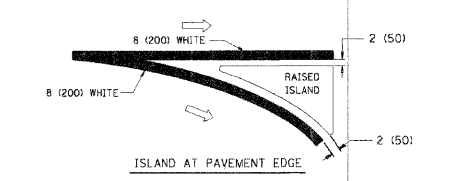
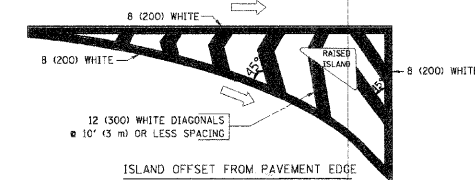
TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/4 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	4 (100)	SKIP-DASH	WHITE	SAME AS LINE BEING EXTENDED
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18" (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/4 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW TWO WAY TRAFFIC WHITE ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "X": "X"=54.0 SQ. FT. (5.0 m²) "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 78000.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE: 50/800 1/4"	CHECKED -	REVISOR - C. JUCIUS 09-09-09	REVISED -
PLOT DATE: 9/11/2009	DATE: 03-19-90	REVISED -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
			TC-13 CONTRACT NO.
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

Plotted: October 9, 2009 @ 8:18 AM By: Terri Seidler-Johnson - Tab: 18 TC-13 22x34

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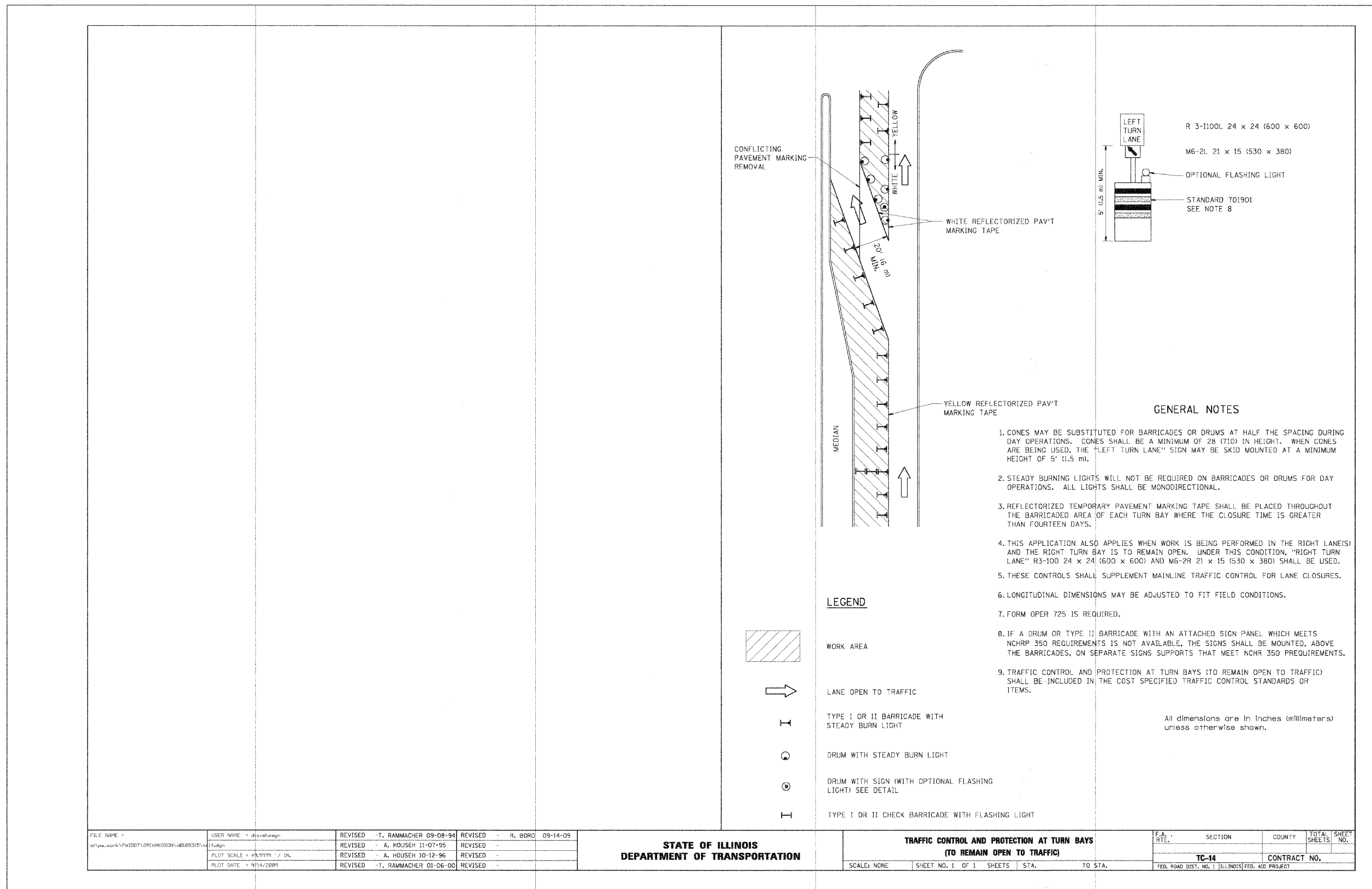
CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS
TC-13

DATE: OCTOBER 2009
PROJECT NO: WV0902
FILE: WV0902-CVR
SHEET 18 OF 27



GENERAL NOTES

- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
- STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
- FORM OPER 725 IS REQUIRED.
- IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME =	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 9/14/2009		REVISED - T. RAMMACHER 01-06-00	REVISED -								
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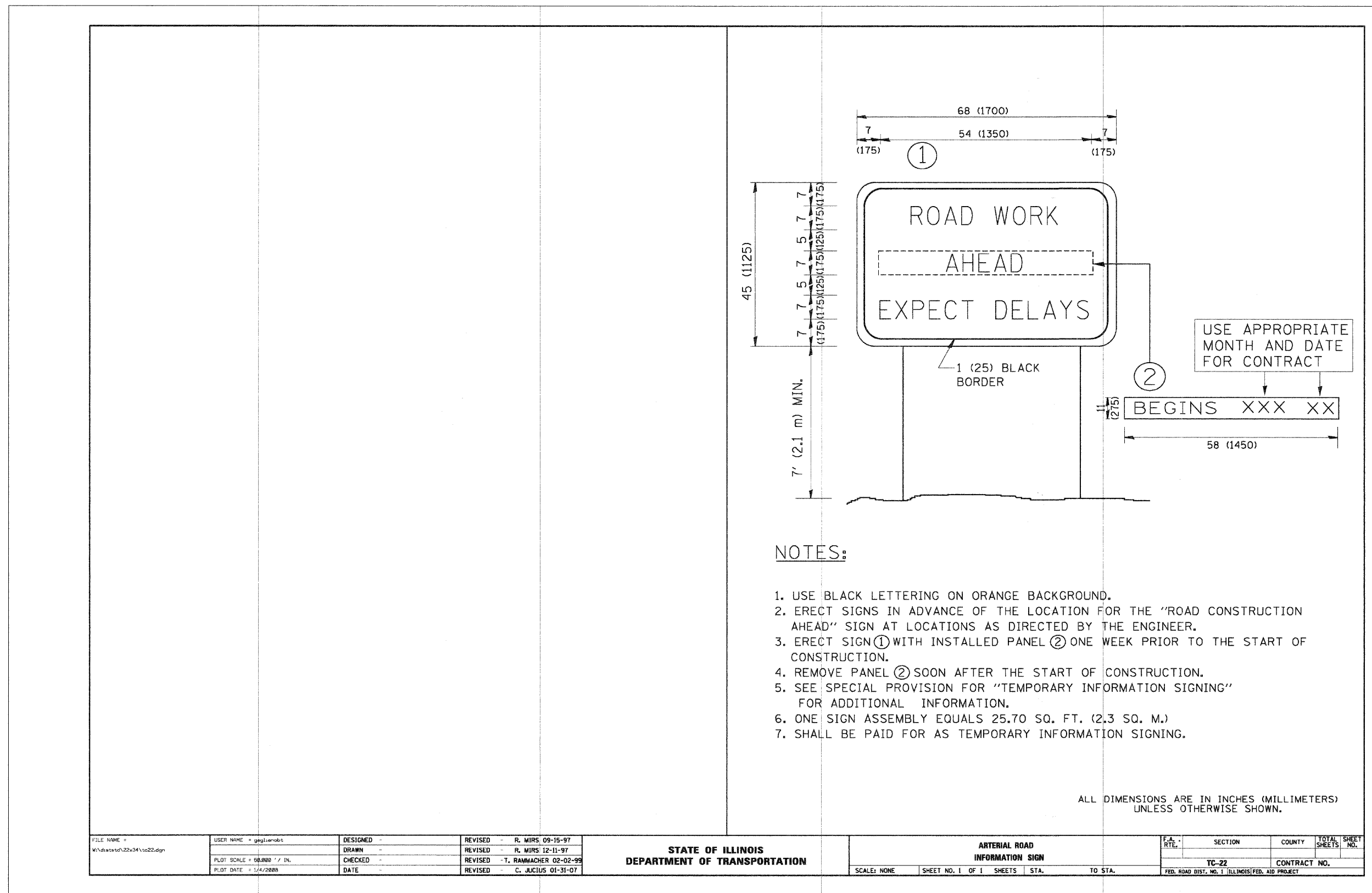
NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
 TC-14

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-CVR
 SHEET **19** OF **27**

Path: H:\S05\PROJ\WV0902\DWG\WV0902-CVR.DWG



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

Plotted: October 9, 2009 @ 8:13 AM By: Terri Seidler-Johnson - Tab: 20 TC-22 22x34

Path: H:\SISK\PROJ\WV0902\DWG\WV0902.DWG FINAL ENG\WV0902-CVR

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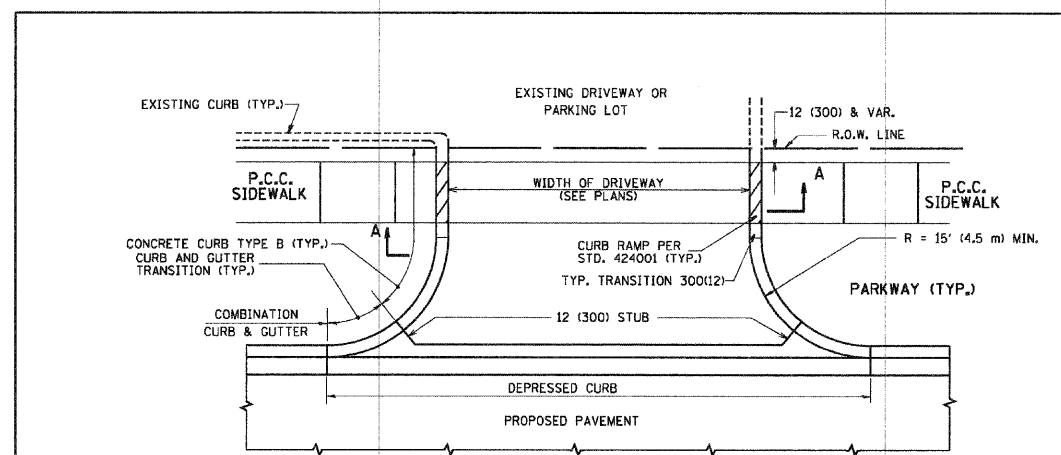
CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

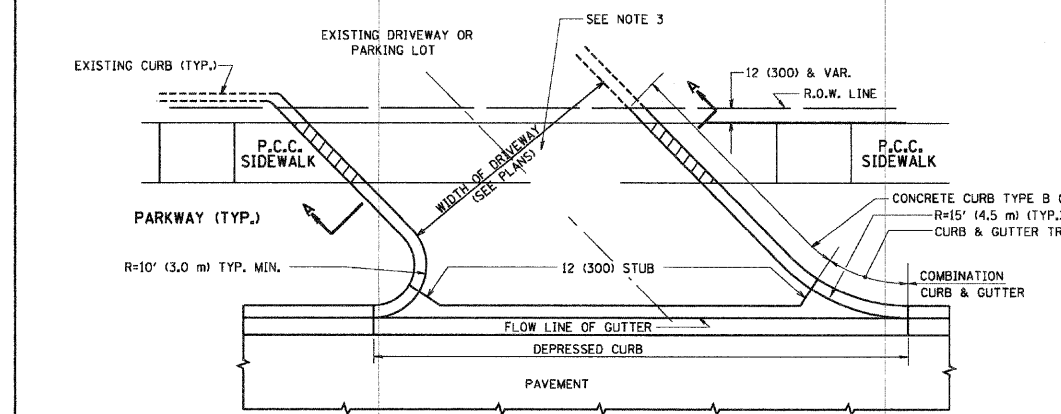
BATAVIA ROAD
LAPP IMPROVEMENTS

ARTERIAL ROAD
INFORMATION SIGN
TC-22

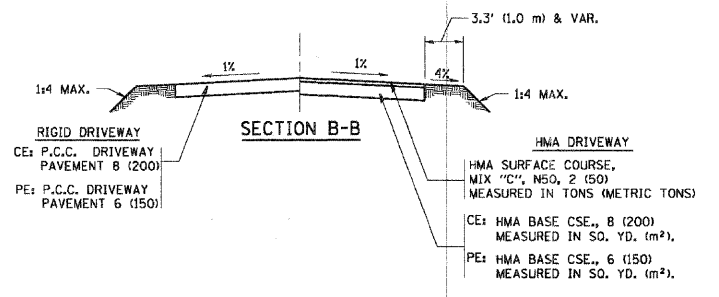
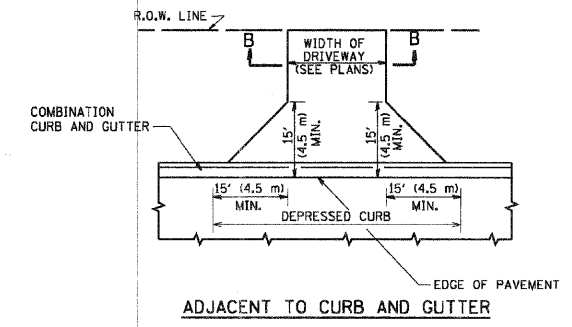
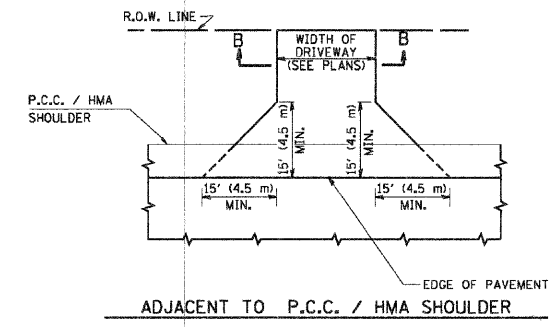
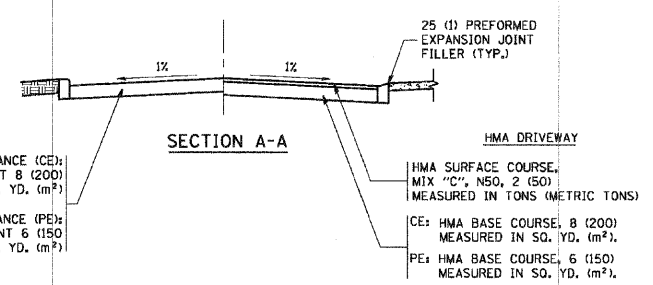
DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-CVR
 SHEET **20** OF **27**



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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PLOT SCALE = 5/8"=1'-0"	CHECKED:	REVISIONS:	REVISIONS:
PLOT DATE = 6/11/2008	DATE: 11-04-95	REVISIONS:	REVISIONS:

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15'(4.5 m)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD0156-07 (BD-01) CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

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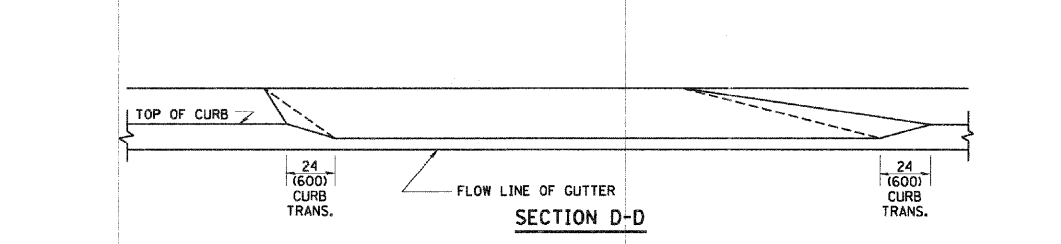
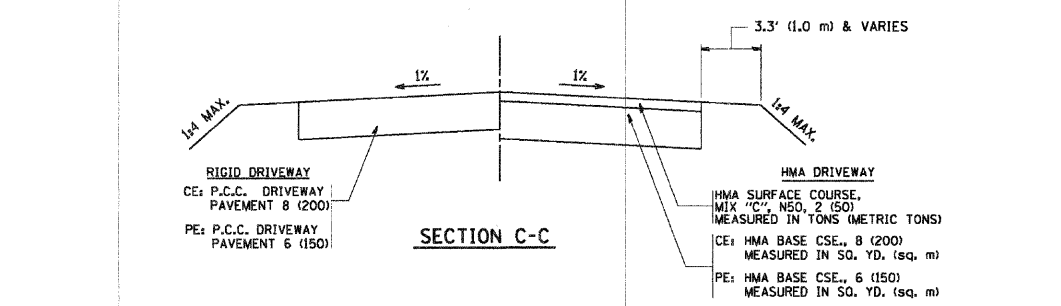
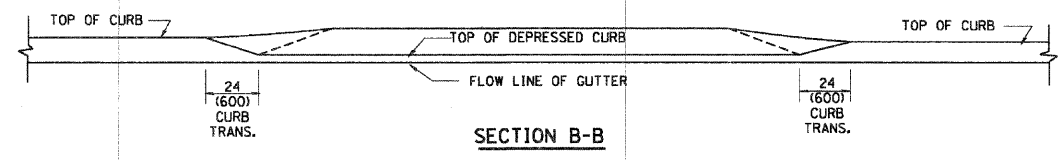
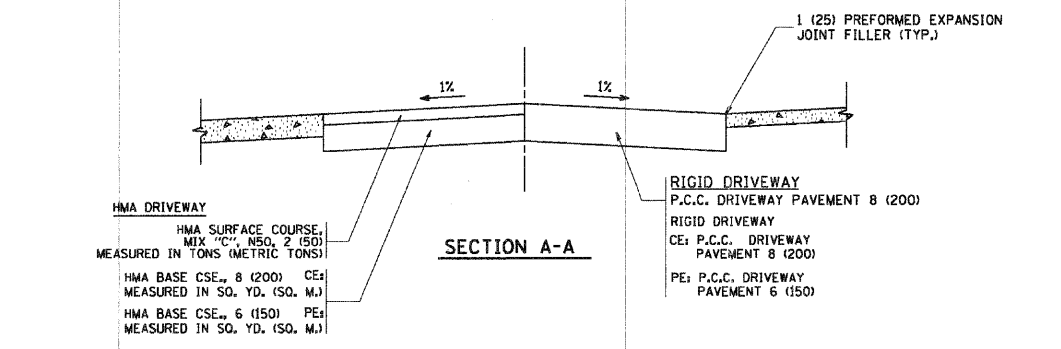
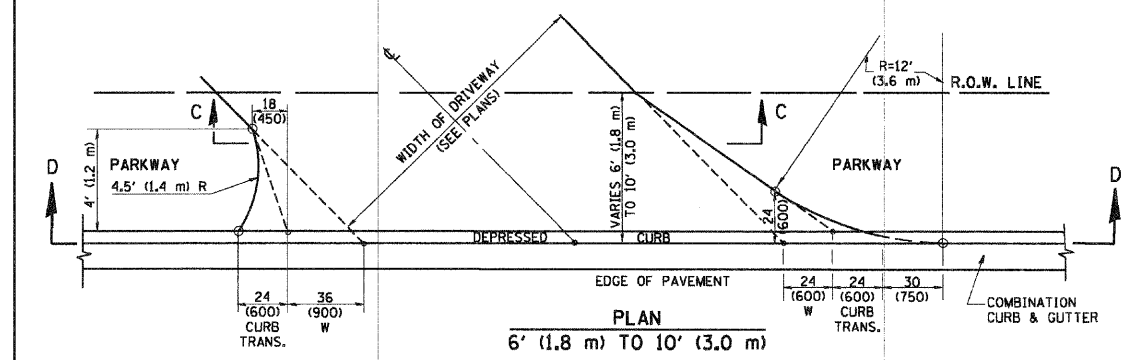
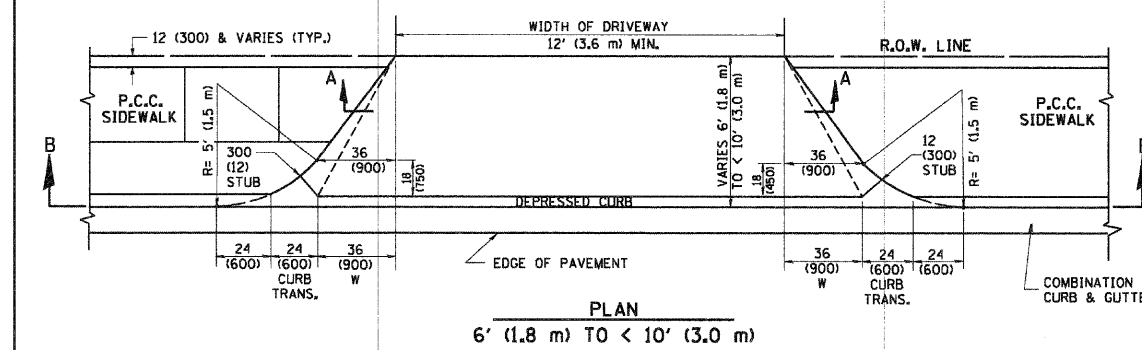
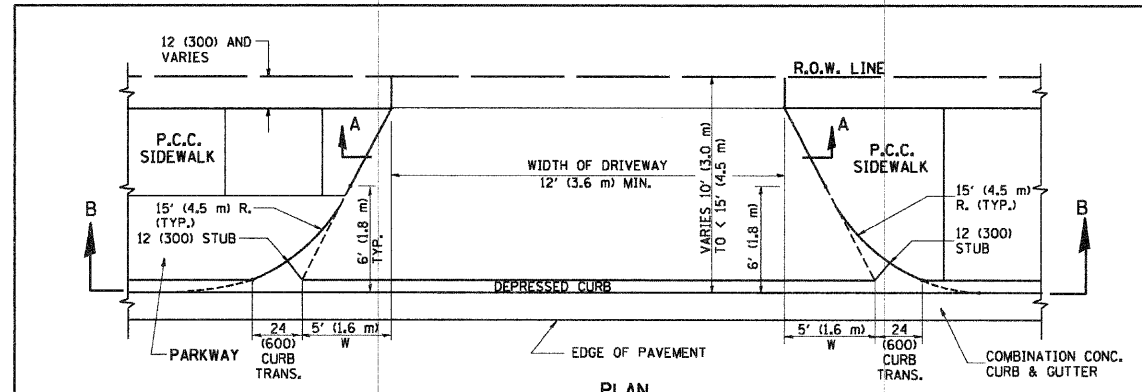
CITY OF WARRENVILLE
 DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15'(4.5M)

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-CVR
 SHEET **21** OF **27**



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = geglernobt	DESIGNED - R. SHAH	REVISED - T. HOLTZ 04-08-97
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	PLOT SCALE = 88.8888 1/32"	CHECKED -	REVISED - P. LOFLEUR 04-15-03
	PLOT DATE = 1/4/2009	DATE - 11-06-95	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	BD400-02 (BD-02)			
CONTRACT NO.				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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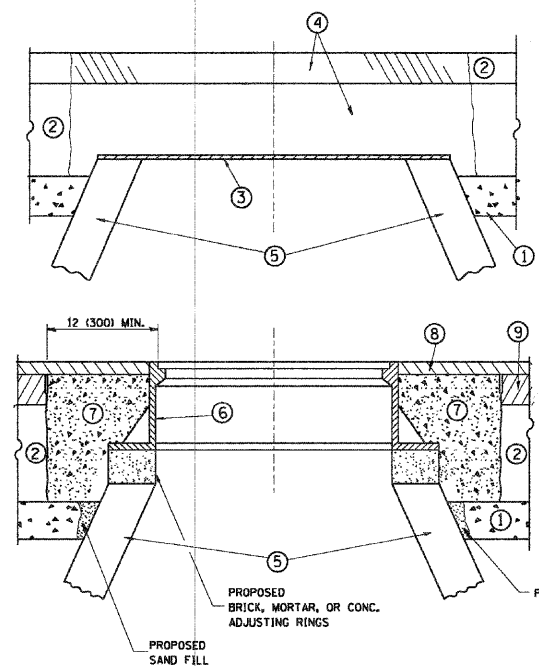
CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

DRIVEWAY DETAILS - DISTANCE
BETWEEN R.O.W. AND FACE OF CURB
< 15'(4.5M) - BD-02

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-CVR
SHEET	22 OF 27



CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- | | |
|--|--|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL" NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 105.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gqgl:arnobt	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
W:\data\stat\22x34\bd08.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 58.0000 1/4" IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/4/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
					BD600-03 (BD-S)		CONTRACT NO.		
					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Plotted: October 9, 2009 @ 7:53 AM By: Terril Seidler-Johnson - Tab: 23 BD-08 22x34

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CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

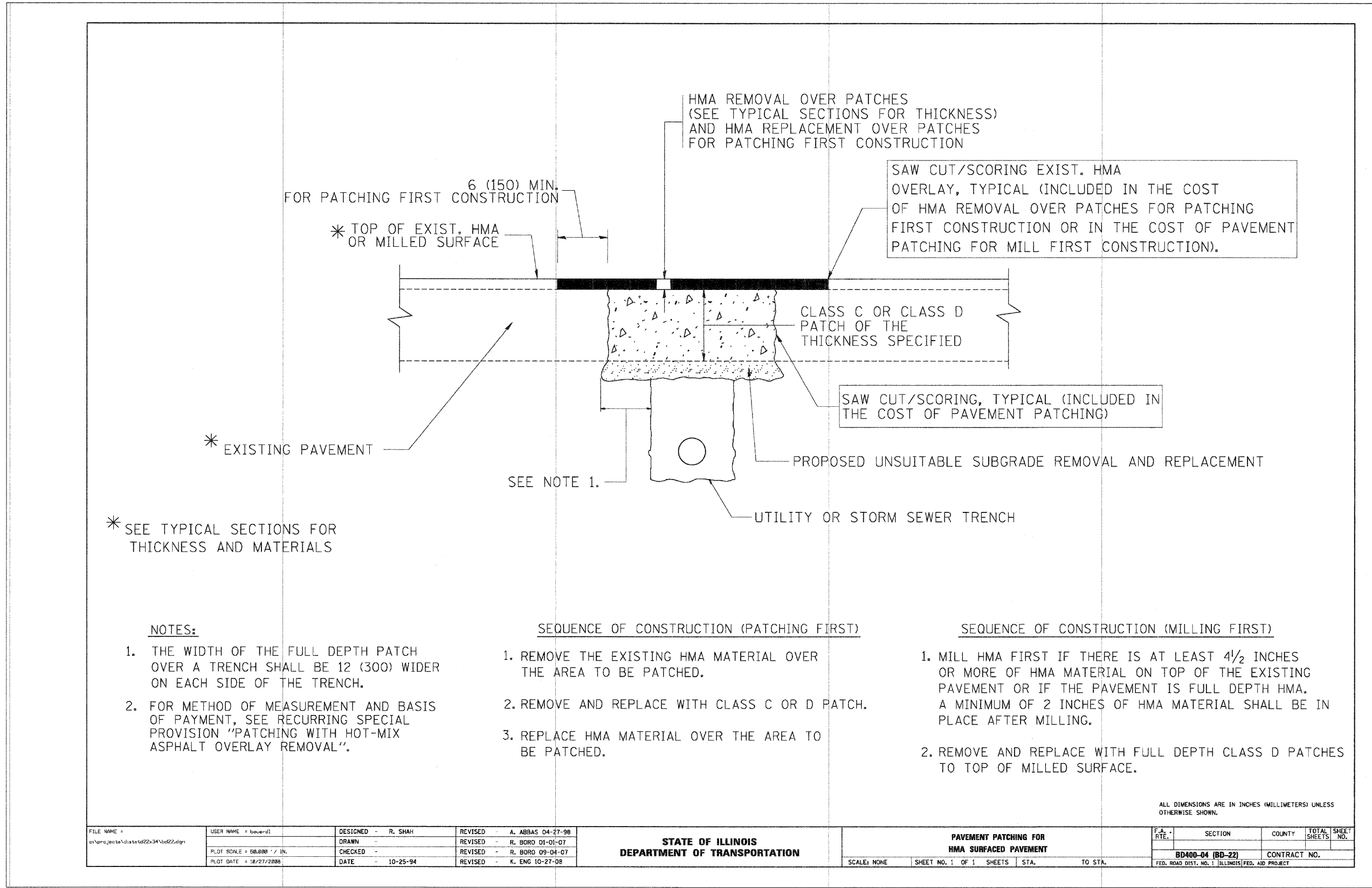
NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-08

DATE: OCTOBER 2009
PROJECT NO: WV0902
FILE: WV0902-CVR
SHEET **23** OF **27**

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* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\proj\acta\distatd22x34\bd22.dgn	USER NAME = beverd	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07	REVISED - R. BORO 09-04-07			BD400-04 (BD-22)	CONTRACT NO.			
PLLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

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**CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS**

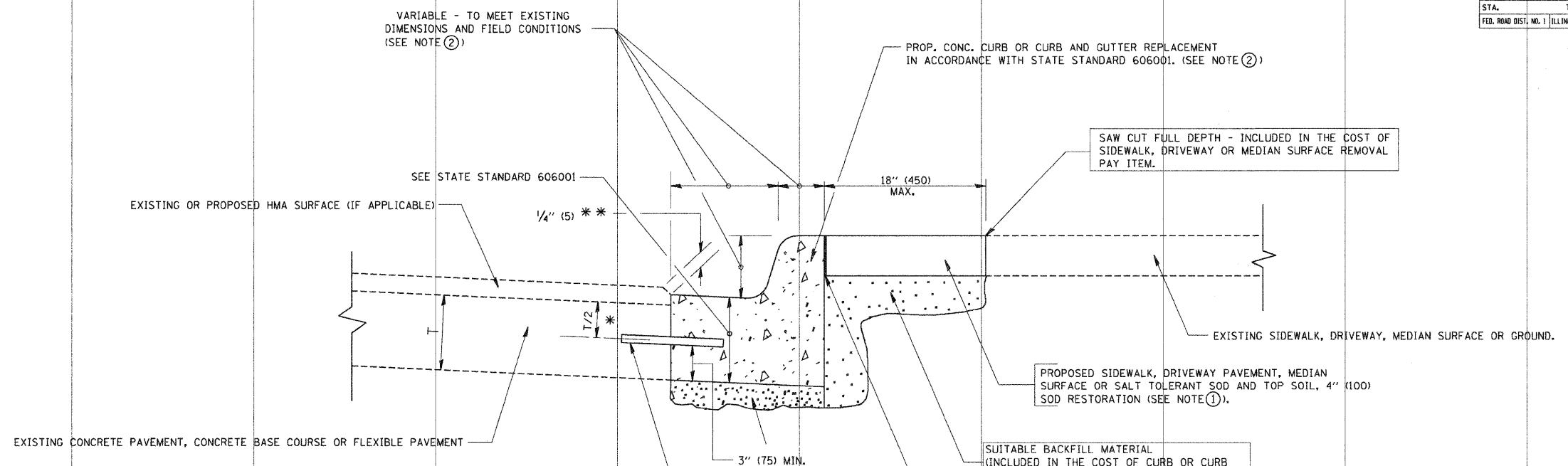
NO.	DATE	REVISIONS

**BATAVIA ROAD
LAPP IMPROVEMENTS**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT
BD-22**

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-CVR
SHEET	24 OF 27

Path: \\S:\S\PROJ\W0902\DWG\W0902.DWG FINAL ENG\W0902-CVR



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- UNDESIRABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
HORIZ. _____

DRAWN BY _____
CHECKED BY _____
BD600-06 (BD-24)

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

PLOT DATE = 10/2/2009
PLOT SCALE = 80000' / 1" = 80000'
USER NAME = bward

Plotted: October 9, 2009 7:45 AM By: Terri Seidler-Johnson - Tab: 25 BD-24, 22x34

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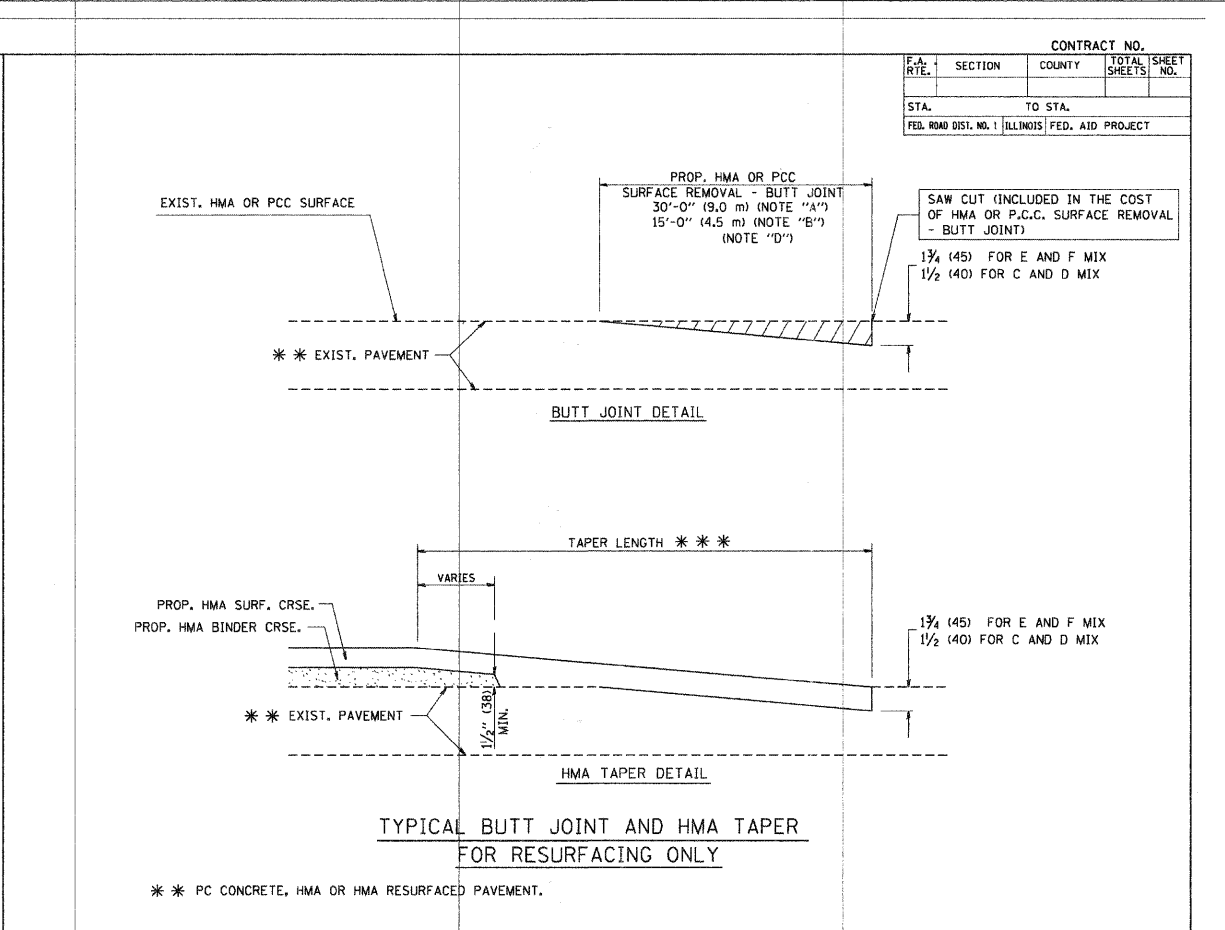
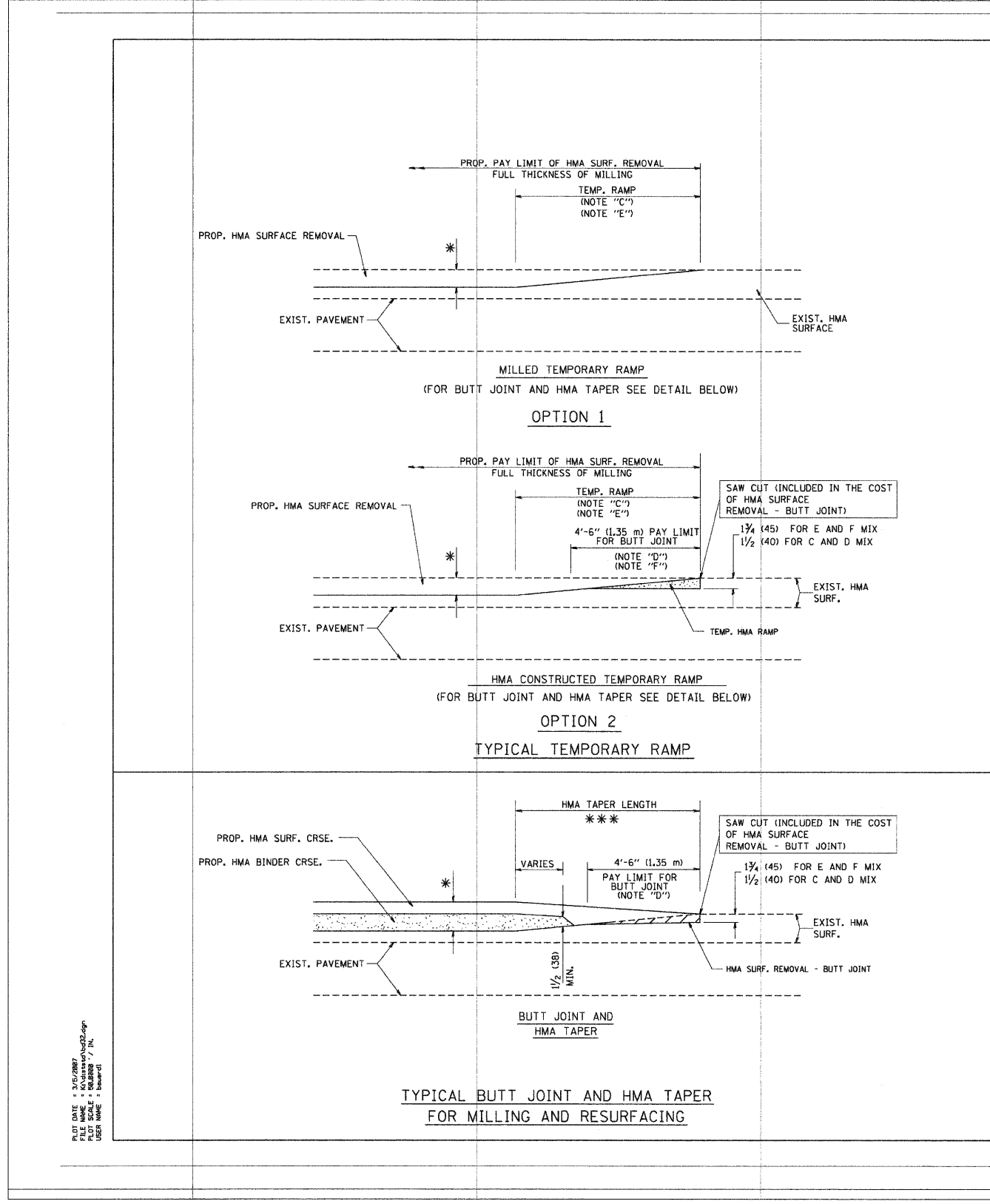
NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT - BD-24

DATE: OCTOBER 2009
PROJECT NO: WV0902
FILE: WV0902-CVR
SHEET **25** OF **27**

Path: \\SBSKPROJ\WV0903\DWG\WV0902\DWG\FINAL\ENG\WV0902-CVR



NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 B: MINOR SIDE ROADS.
 C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
 G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: VERT. NONE
 HORIZ. NONE

DRAWN BY: [Blank]
 CHECKED BY: [Blank]
 BD400-05 (V1-BD32)

PLT DATE: 8/10/07
 PLT SCALE: 1/8" = 1'-0"
 USER: [Blank]

Plotted: October 9, 2009 7:43 AM By: Terri Seidler-Johnson - Tab: 26 BD-32 22X34

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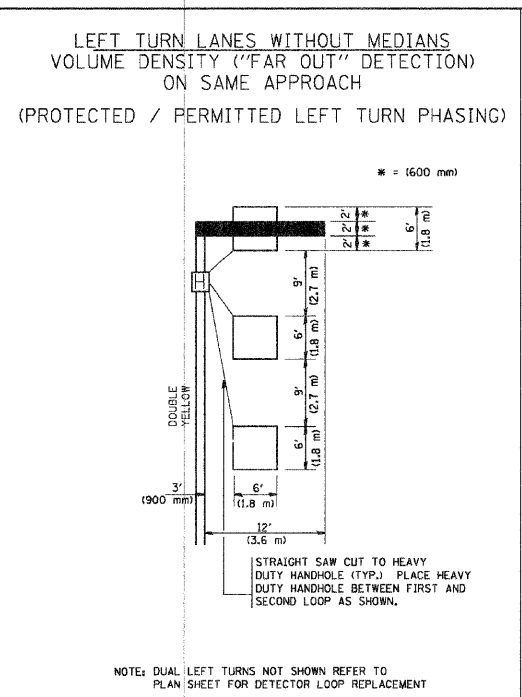
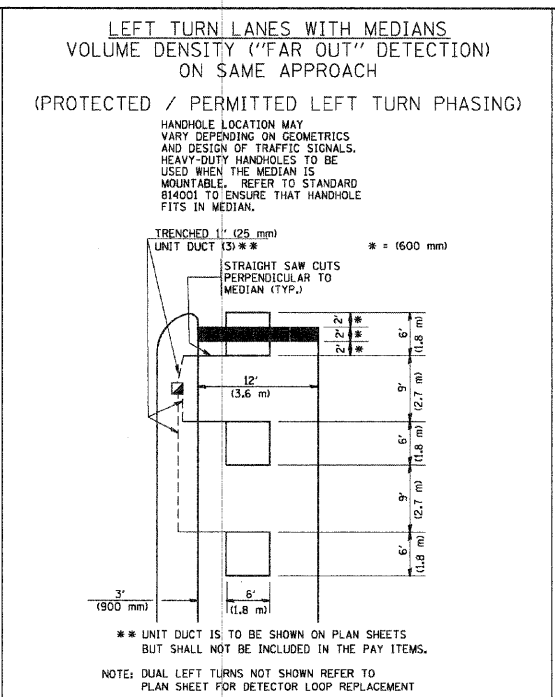
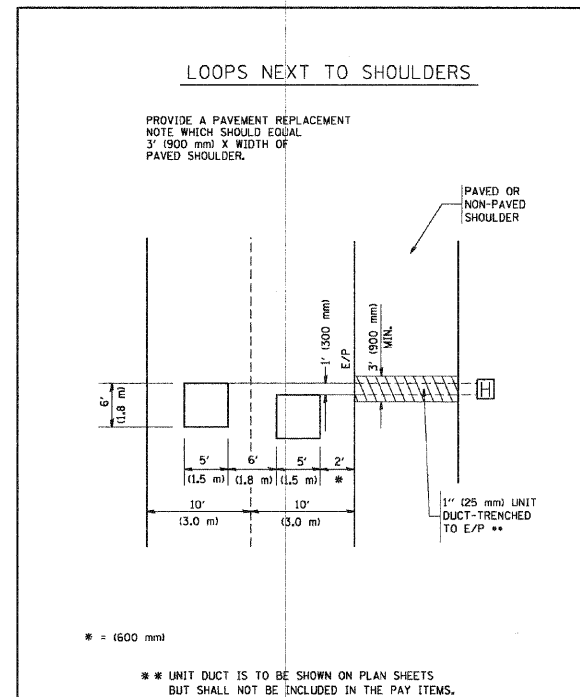
NO.	DATE	REVISIONS

BATAVIA ROAD
 LAPP IMPROVEMENTS

BUTT JOINT AND HMA TAPER DETAILS
 BD-32

DATE: OCTOBER 2009
 PROJECT NO: WV0902
 FILE: WV0902-CVR
 SHEET 26 OF 27

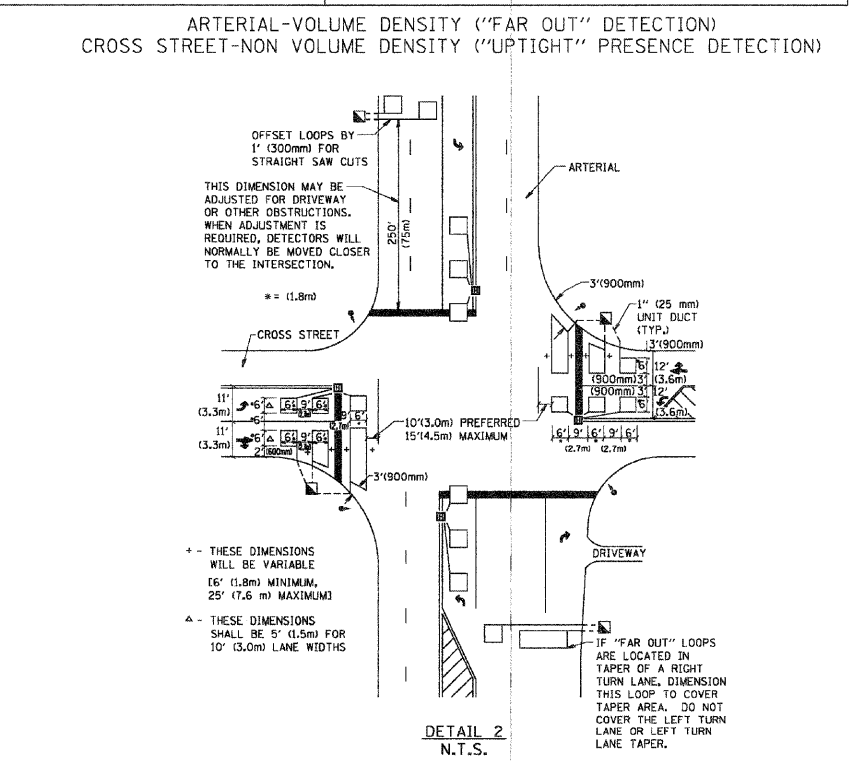
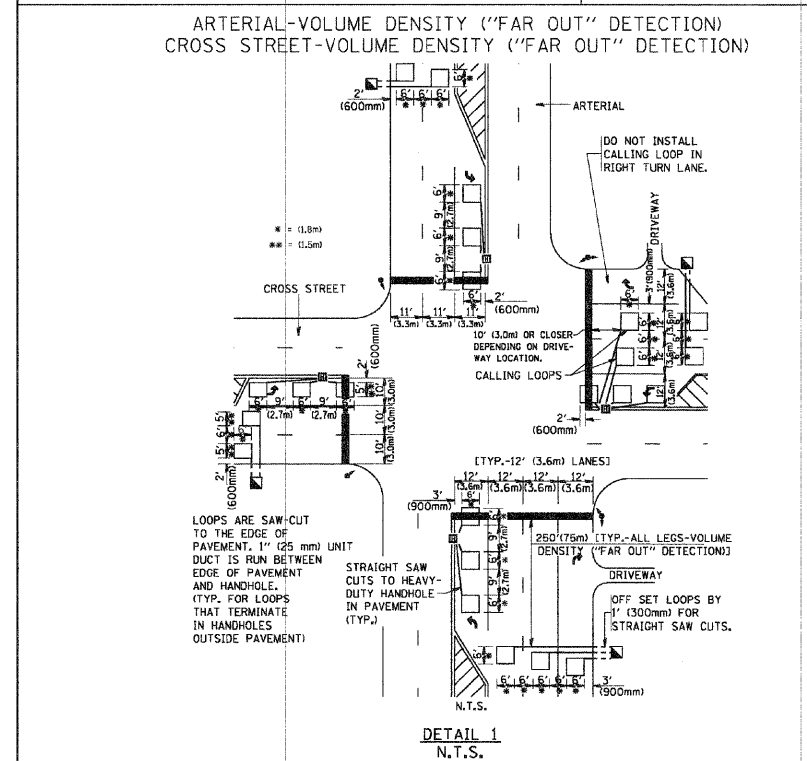
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NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.



PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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PLT SCALE = 50.0000 / IN	CHECKED - R.K.F.	DRAWN -	REVISD -
PLT DATE = 1/4/2009	DATE -		REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION				
DETAILS FOR ROADWAY RESURFACING				
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TS-07			
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. CONTRACT NO. FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

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CITY OF WARRENVILLE
DUPAGE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

BATAVIA ROAD
LAPP IMPROVEMENTS

DISTRICT 1 - DETECTOR LOOP
INSTALLATION DETAILS FOR
ROADWAY RESURFACING - TS-07

DATE:	OCTOBER 2009
PROJECT NO:	WV0902
FILE:	WV0902-CVR
SHEET	27 OF 27