

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9-2530	15-07126-00-RS	DUPAGE	15	1

CONTRACT NO. 61C04

**INDEX OF SHEETS**

- 1 COVER SHEET AND INDEX OF SHEETS
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**TRAFFIC DATA**

ADT:  
 POWIS ROAD 7,650 VPD (2012)  
 SCHICK ROAD 6,250 VPD (2012)

**POSTED SPEED**

POWIS ROAD:  
 30 MPH (EXISTING)  
 30 MPH (PROPOSED)

**DESIGN SPEED**

POWIS ROAD:  
 35 MPH (EXISTING)  
 35 MPH (PROPOSED)

SCHICK ROAD:  
 30 MPH (EXISTING)  
 30 MPH (EXISTING)

SCHICK ROAD:  
 35 MPH (EXISTING)  
 35 MPH (EXISTING)

**FUNCTIONAL CLASSIFICATION**

POWIS RD - MINOR ARTERIAL  
 SCHICK RD - MAJOR COLLECTOR

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS  
**PLANS FOR PROPOSED  
 FEDERAL AID HIGHWAY**  
 FAU 2530 (POWIS ROAD)  
 UNION PACIFIC RAILROAD TO FAU 3804 (SMITH ROAD)  
 FAU 1369 (SCHICK ROAD)  
 FAR HILLS DRIVE TO FAU 3800 (PETERSDORF ROAD)  
 RESURFACING  
 SECTION 15-07126-00-RS  
 PROJECT M-4003(544)  
 WAYNE TOWNSHIP  
 DUPAGE COUNTY  
 JOB NO. C-91-412-15

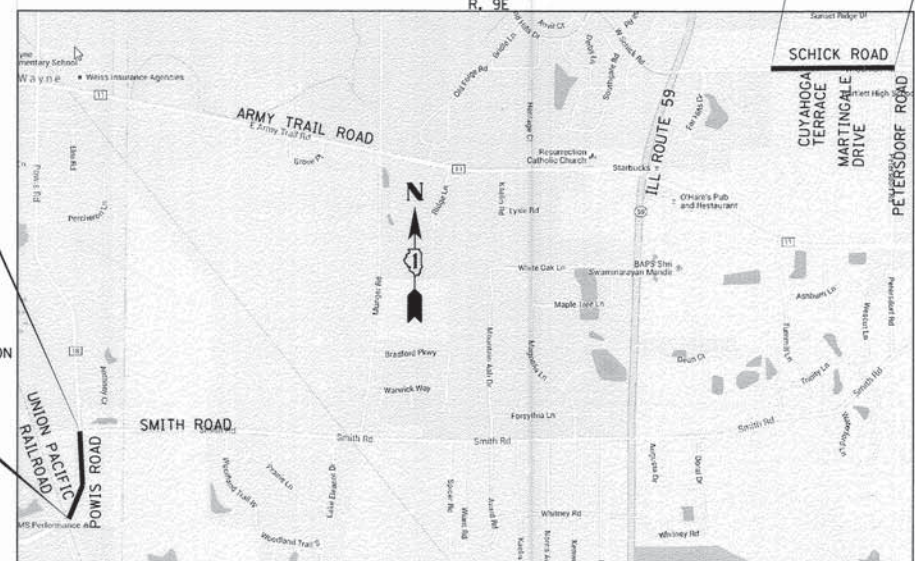


POWIS PROJECT ENDS  
 STATION 114+05

POWIS PROJECT BEGINS  
 STATION 100+08

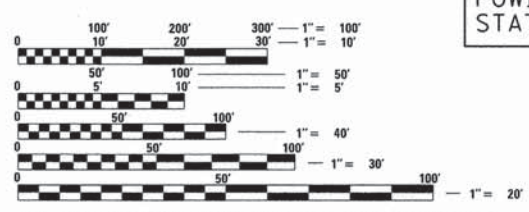
SCHICK PROJECT BEGINS  
 STATION 1000+96

SCHICK PROJECT ENDS  
 STATION 1021+70



**LOCATION MAP**

NOT TO SCALE  
 PROJECT LENGTH (GROSS AND NET)  
 POWIS RD - 1,397 FT (0.27 MILES)  
 SCHICK RD - 2,074 FT (0.39 MILES)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

**CONTRACT NO. 61C04**



DAVID W. BLOCK, P.E.  
 NO. 062-050966  
 EXP. DATE 11/30/15

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

APPROVED October 29<sup>th</sup>, 2015  
*Marty Jermolowski*  
 WAYNE TOWNSHIP, CIVIL ENGINEER

PASSED October 13<sup>th</sup>, 2015  
*Chris Christopher Holt*  
 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
 BASED ON LIMITED  
 REVIEW November 2<sup>nd</sup>, 2015  
*John F. ...*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS**  
 PRE-FINAL

1475 EAST WOODFIELD ROAD, SUITE 600  
 SCHAMLUNBURG, ILLINOIS 60173  
 (847) 605-9600  
 FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., P.TOE, (847) 705-4021, SCHAMLUNBURG, IL  
**Trans Systems**



**GENERAL NOTES**

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR SHALL VERIFY THE ENGINEER'S LINE AND GRADE STAKES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT WITH THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS/HER OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND WAYNE TOWNSHIP AT 630-231-4923 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON TOWNSHIP PROPERTY OR ROW WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THEIR AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- QUANTITIES FOR PATCHING SHALL NOT EXCEED THOSE PROVIDED IN THE SUMMARY OF QUANTITIES UNLESS APPROVED BY THE ENGINEER. THE ENGINEER WILL VERIFY FINAL PATCH LOCATIONS IN THE FIELD, PRIOR TO REMOVAL.
- THE CONTRACTOR IS REQUIRED TO USE A PAVER SKI WHEN PLACING BITUMINOUS LIFTS.
- THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL.
- THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
- THE CONTRACTOR SHALL NOT BE PERMITTED TO STAGE OR OPERATE CONSTRUCTION EQUIPMENT ON THE HMA BIKE PATH.

**SIGNING AND STRIPING**

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL TC-13 AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SIGNS THAT INTERFERE WITH CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.25.

**TRAFFIC CONTROL**

- SEE TRAFFIC CONTROL HIGHWAY STANDARDS CONCERNING TRAFFIC CONTROL AND PROTECTION.
- THE CONTRACTOR SHALL SCHEDULE CONSTRUCTION ACTIVITIES SO THAT TWO-WAY TRAFFIC SHALL REMAIN OPEN AT ALL TIMES.
- "ROAD CONSTRUCTION AHEAD" SIGNS SHALL BE POSTED ON ALL SIDE STREETS FROM BOTH DIRECTIONS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION PAY ITEMS.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF ANY UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN IF NOT SHOWN ON THE PLANS. ALL UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY OWNERS AND THE ENGINEER SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- ALL LOOSE MATERIAL GENERATED FROM CONSTRUCTION WORK/ACTIVITY DEPOSITED IN THE FLOWLINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT IN ACCORDANCE WITH ARTICLE 107.15.
- DRIVEWAY CULVERT PIPES SHALL BE REPLACED ACCORDING TO THE PLANS. THE TOP OF PIPE SHALL BE A MINIMUM OF 10" BELOW THE TOP OF DRIVEWAY PAVEMENT. THE CONTRACTOR SHALL VERIFY THE INVERTS IN THE FIELD.
- THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.

**MISCELLANEOUS**

- MATERIALS RESULTING FROM THE REMOVAL OF CONCRETE SURFACES, UTILITY STRUCTURE ADJUSTMENTS, RESTORATION WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF THE CONTRACTOR DOES NOT REMOVE THESE MATERIALS AT THE REQUEST OF THE ENGINEER, THE ENGINEER WILL HIRE A CONTRACTOR TO HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL BE BILLED (CHARGED) ACCORDINGLY.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS/HER YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO USE OF THE WATER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REGULARLY SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND PAVING HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS AND ALL LOOSE MATERIAL. SWEEPING SHALL BE INCLUDED IN THE PRICE FOR HMA MILLING.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, COMMERCIAL PROPERTY OWNERS, AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE ALTERED DUE TO SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER REPLACEMENT. AT LOCATIONS WHERE THE SIDEWALK, DRIVEWAY, AND/OR CURB AND GUTTER IS SCHEDULED TO BE REMOVED, THE CONTRACTOR SHALL CONTACT THE PROPERTY OWNER 24 HOURS PRIOR TO THEIR REMOVAL. THESE ITEMS SHALL BE REMOVED AND RECONSTRUCTED HALF AT A TIME SUCH THAT THERE ARE NO DRIVEWAY CLOSURES.
- WHEN REMOVING PAVEMENT, CURB AND GUTTER, SIDEWALK, AND/OR ANY OTHER STRUCTURES, THE USE OF ANY TYPE OF CONCRETE BREAKERS WHICH MIGHT DAMAGE UNDERGROUND PUBLIC OR PRIVATE UTILITIES AND BUILDING FOUNDATIONS WILL NOT BE PERMITTED. UNDER NO CIRCUMSTANCES WILL THE USE OF A FROST BALL BE PERMITTED.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.

**COMMITMENTS**

- THE CONTRACTOR SHALL NOT BEGIN WORK BEFORE JUNE 6TH, 2016, AND SHALL COMPLETE ALL WORK TO THE SATISFACTION OF THE ENGINEER BY THE END OF THE DAY ON JULY 31ST, 2016. THIS SCHEDULE COMPLIES WITH THE ELGIN AREA SCHOOL DISTRICT U46 SCHEDULE AND THE ST CHARLES COMMUNITY UNIT SCHOOL DISTRICT 303 SO NO WORK OCCURS WHILE BARTLETT HIGH SCHOOL OR NORTON CREEK ELEMENTARY SCHOOL IS IN SESSION.

**HIGHWAY STANDARDS**

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
542401-01	METAL END SECTION FOR PIPE CULVERTS
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-04	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS

FILE NAME =	USER NAME = .USER_	DESIGNED - BSH	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>POWIS ROAD AND SCHICK ROAD RESURFACING GENERAL NOTES AND HIGHWAY STANDARDS</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Gr\CH14\0092\Road\Sheets\40092-GN.dgn		DRAWN - BSH	REVISED -		9-2530	15-07126-00-RS	DUPAGE	15	2		
		CHECKED - DWB	REVISED -		SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.		CONTRACT NO. 61C04		ILLINOIS FED. AID PROJECT		
#MODELNAME#		DATE - 10/12/15	REVISED -								



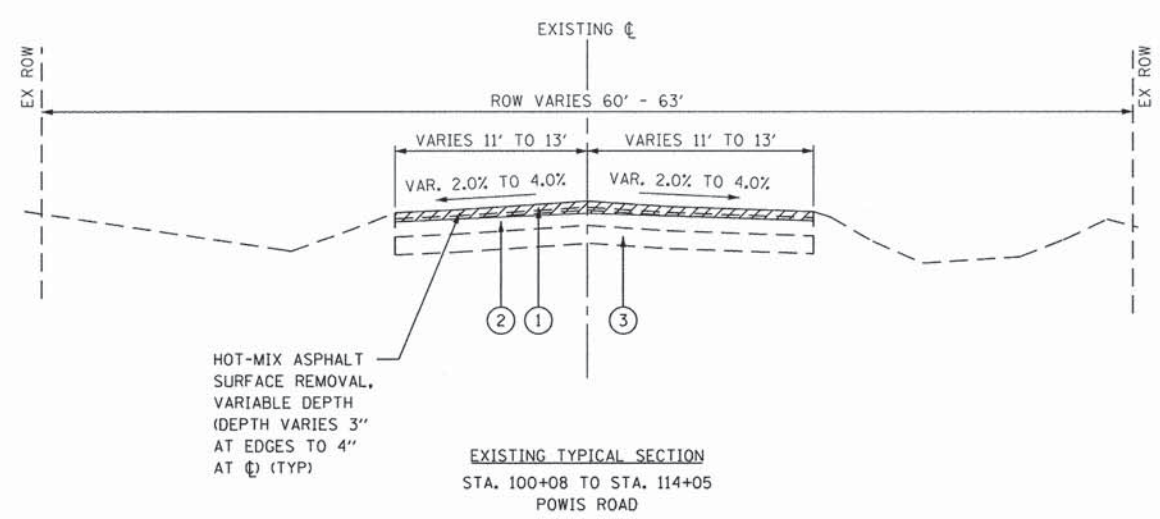
CODE NUMBER	ITEMS	UNIT	TOTAL QUANTITY	0005 ROADWAY 75% STU 25% LA
20200100	EARTH EXCAVATION	CU YD	30	30
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	542	542
20800150	TRENCH BACKFILL	CU YD	26	26
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	1,534	1,534
25000210	SEEDING, CLASS 2A	ACRE	0.3	0.3
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	29	29
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	29	29
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	29	29
25100630	EROSION CONTROL BLANKET	SQ YD	1,534	1,534
25200200	SUPPLEMENTAL WATERING	UNIT	5	5
28000510	INLET FILTERS	EACH	15	15
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	31	31
35800100	PREPARATION OF BASE	SQ YD	927	927
40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	6,237	6,237
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	25	25
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	509	509
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	96	96
40600990	TEMPORARY RAMP	SQ YD	96	96
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,035	1,035
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	288	288
44201701	CLASS D PATCHES, TYPE I, 5 INCH	SQ YD	229	229
44201705	CLASS D PATCHES, TYPE II, 5 INCH	SQ YD	229	229
44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	229	229
44201711	CLASS D PATCHES, TYPE IV, 5 INCH	SQ YD	229	229
48101200	AGGREGATE SHOULDERS, TYPE B	TON	128	128
50105220	PIPE CULVERT REMOVAL	FOOT	200	200

CODE NUMBER	ITEMS	UNIT	TOTAL QUANTITY	0005 ROADWAY 75% STU 25% LA
54215547	METAL END SECTIONS 12"	EACH	14	14
54200217	PIPE CULVERTS, CLASS D, TYPE 1 12"	FOOT	194	194
67100100	MOBILIZATION	LSUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	4	4
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2,897	2,897
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	122	122
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	27,676	27,676
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	470	470
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	678	678
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	144	144
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1,043	1,043
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1
72900200	METAL POST - TYPE B	FOOT	8	8
• 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	62	62
• 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	13,838	13,838
• 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	235	235
• 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	339	339
• 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	72	72
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	9,144	9,144
Z0004522	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6"	SQ YD	462	462
Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	191	191
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1

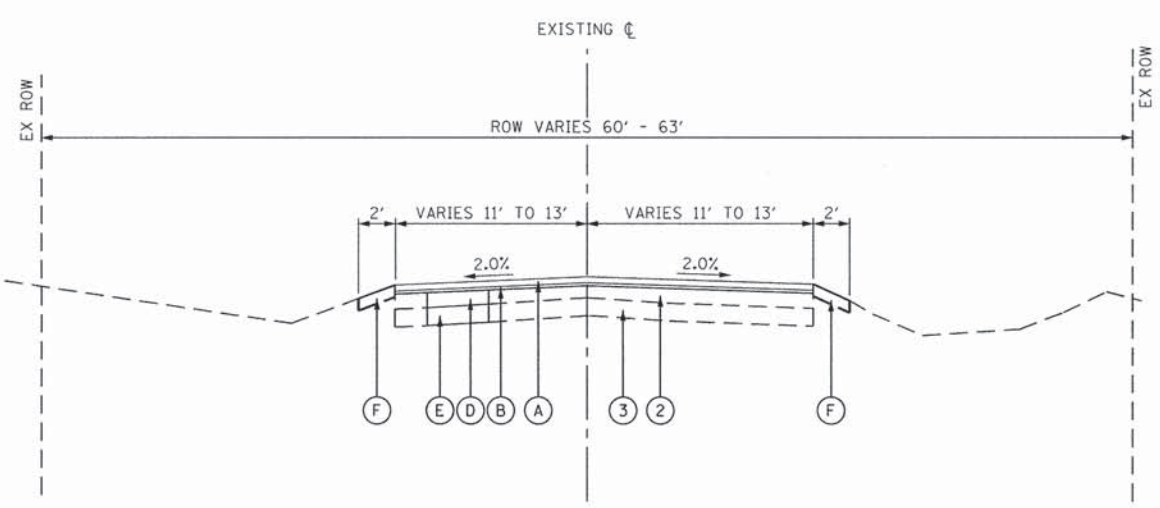
• SPECIALTY ITEM

FILE NAME * G:\CH14\0092\Road\Sheets\40092-500.dgn	USER NAME * USER_	DESIGNED - BSH	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>POWIS ROAD AND SCHICK ROAD RESURFACING SUMMARY OF QUANTITIES</b>	F.A.U. RTE. 9-2530	SECTION 15-07126-00-RS	COUNTY	TOTAL SHEETS 15	SHEET NO. 3	
PLOT SCALE * 50.0000' / 1" =		DRAWN - BSH	REVISED -			SCALE:	SHEET 1 OF 1 SHEETS	STA.	TO STA.	DUPAGE	CONTRACT NO. 61C04
PLOT DATE * 10/28/2015		CHECKED - DWB	REVISED -			ILLINOIS FED. AID PROJECT					
		DATE - 10/12/15	REVISED -								





EXISTING TYPICAL SECTION  
STA. 100+08 TO STA. 114+05  
POWIS ROAD



PROPOSED TYPICAL SECTION  
STA. 100+08 TO STA. 114+05  
POWIS ROAD

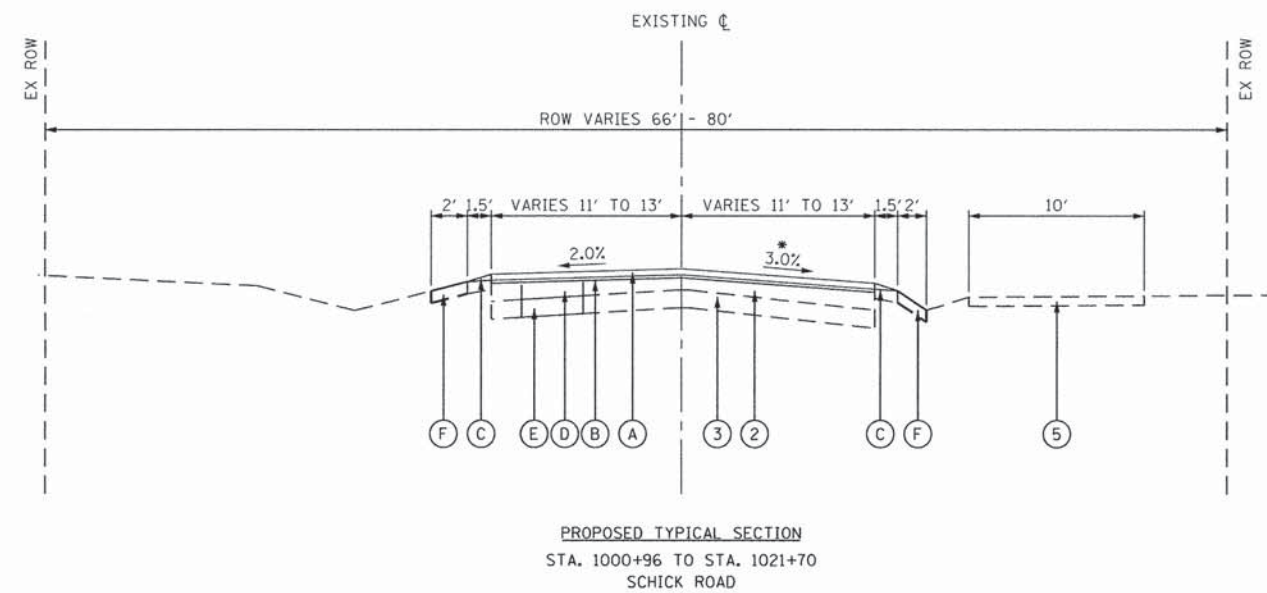
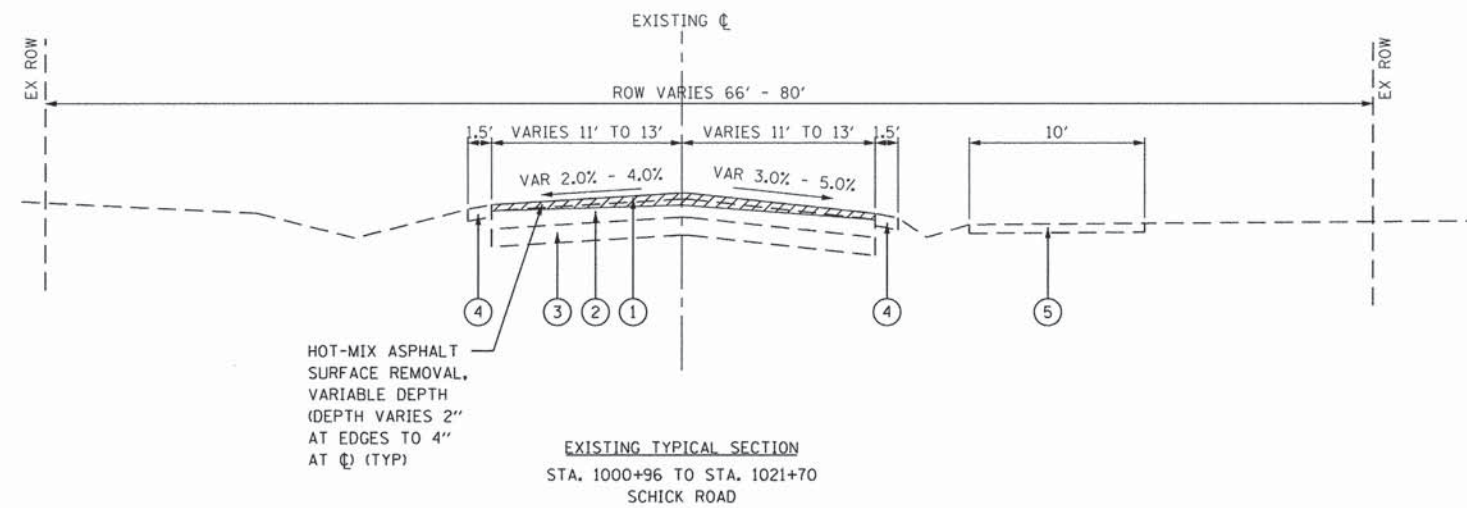
LEGEND

- ① EXISTING HMA SURFACE COURSE, 2"
- ② EXISTING HMA BINDER COURSE, 6"
- ③ EXISTING AGGREGATE BASE COURSE, 6"
- ④ EXISTING AGGREGATE SHOULDERS, 4"
- ⑤ EXISTING HMA MULTI-USE PATH
- Ⓐ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- Ⓑ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- Ⓒ PROPOSED AGGREGATE SHOULDERS, TYPE B, TON
- Ⓓ PROPOSED CLASS D PATCHES, 5" (AS DIRECTED BY ENGINEER)
- Ⓔ PROPOSED REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AGGREGATE SUBGRADE IMPROVEMENT (AT PATCHING LOCATIONS AS DIRECTED BY ENGINEER)
- Ⓕ SEEDING, CLASS 2A
- Ⓖ EROSION CONTROL BLANKET
- Ⓗ TOPSOIL FURNISH AND PLACE, 4" (AS DIRECTED BY ENGINEER)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 2"	4% @ 50 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 MM), 5" (IN 2 LIFTS)	4% @ 70 GYRATIONS
<b>HOT-MIX ASPHALT DRIVEWAYS 6"</b>	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL - 19mm) 4" (IN 2 LIFTS)	4% @ 50 GYRATIONS

- NOTES:
- 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN.
  - 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
  - 3) THE CONTRACTOR SHALL MILL BEFORE PATCHING.



• THE EASTBOUND CROSS SLOPES HAVE BEEN LOWERED FROM UP TO 5% IN EXISTING CONDITIONS TO 3% IN PROPOSED CONDITIONS. CORRECTION OF THE CROSS SLOPE TO 2% WOULD REQUIRE RAISING THE EDGE OF PAVEMENT OR LOWERING THE CENTERLINE ELEVATION. RAISING THE EDGE OF PAVEMENT ELEVATION WOULD SIGNIFICANTLY ALTER THE GRADING BETWEEN THE SHOULDER AND BIKE PATH. LOWERING THE CENTERLINE ELEVATION WOULD SIGNIFICANTLY REDUCE THE EXISTING PAVEMENT DEPTH.

LEGEND

- ① EXISTING HMA SURFACE COURSE, 2"
- ② EXISTING HMA BINDER COURSE, 6"
- ③ EXISTING AGGREGATE BASE COURSE, 6"
- ④ EXISTING AGGREGATE SHOULDERS, 4"
- ⑤ EXISTING HMA MULTI-USE PATH
- Ⓐ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
- Ⓑ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- Ⓒ PROPOSED AGGREGATE SHOULDERS, TYPE B, TON
- Ⓓ PROPOSED CLASS D PATCHES, 5" (AS DIRECTED BY ENGINEER)
- Ⓔ PROPOSED REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AGGREGATE SUBGRADE IMPROVEMENT (AT PATCHING LOCATIONS AS DIRECTED BY ENGINEER)
- Ⓕ SEEDING, CLASS 2A  
EROSION CONTROL BLANKET  
TOPSOIL FURNISH AND PLACE, 4"  
(AS DIRECTED BY ENGINEER)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 2"	4% @ 50 GYRATIONS
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"	3.5% @ 50 GYRATIONS
CLASS D PATCHES (HMA BINDER IL-19 MM), 5" (IN 2 LIFTS)	4% @ 70 GYRATIONS
<b>HOT-MIX ASPHALT DRIVEWAYS 6"</b>	
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 2"	4% @ 50 GYRATIONS
HMA BASE COURSE (HMA BINDER IL - 19mm) 4" (IN 2 LIFTS)	4% @ 50 GYRATIONS

- NOTES:
- 1) THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
  - 2) THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
  - 3) THE CONTRACTOR SHALL MILL BEFORE PATCHING.

FILE NAME =	USER NAME = .USER_	DESIGNED - BSH	REVISED -
G:\CH14\0092\Road\Sheets\40092-TYPSECT\0NS-1.dgn		DRAWN - BSH	REVISED -
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#MODELNAME#	PLOT DATE = 10/28/2015	DATE - 10/12/15	REVISED -

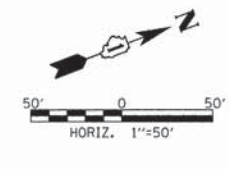
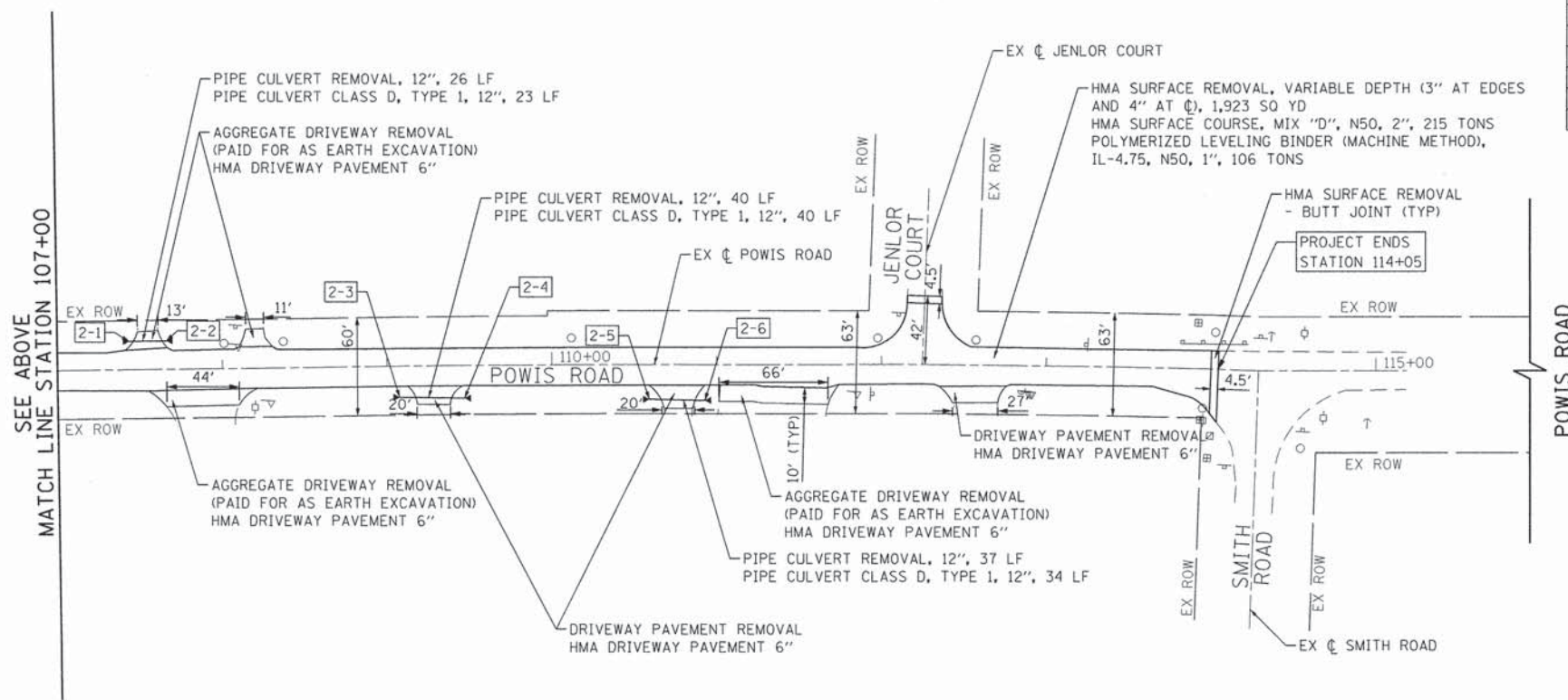
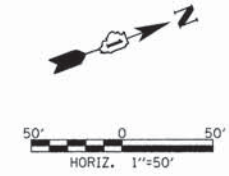
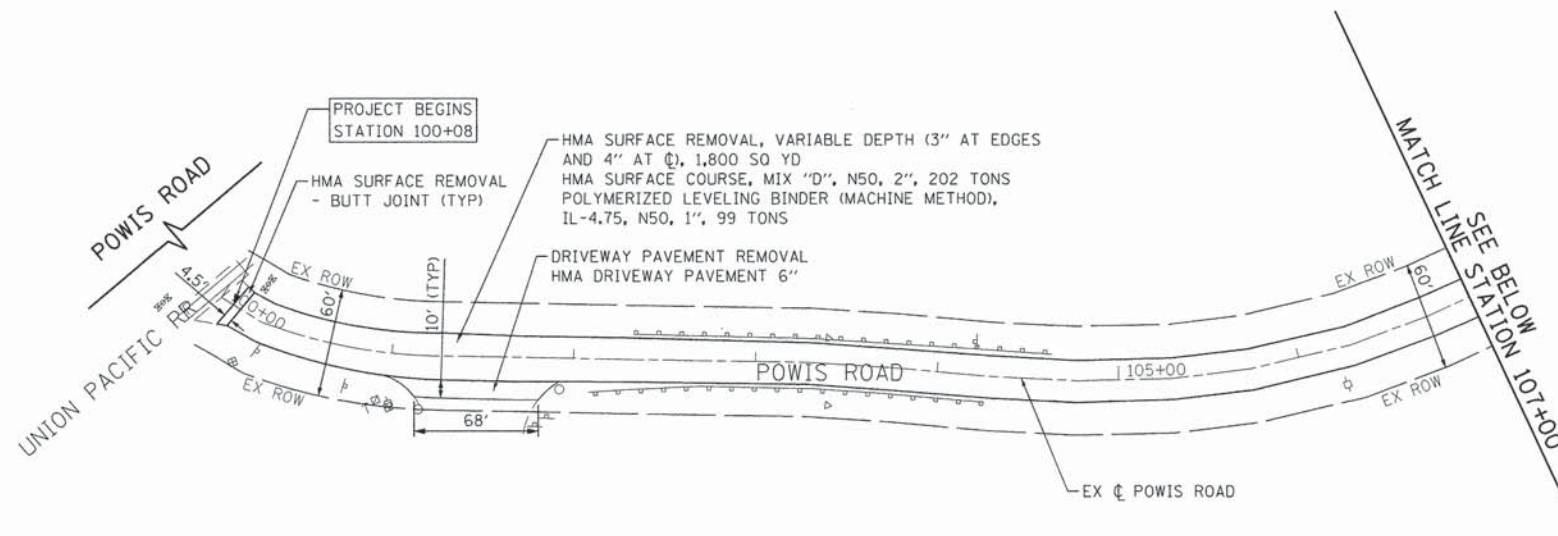
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHICK ROAD RESURFACING  
TYPICAL SECTIONS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9-1369	15-07126-00-R5	DUPAGE	15	5
CONTRACT NO. 61C04				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.



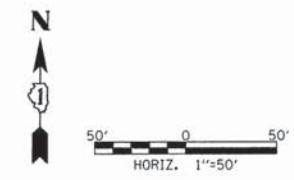
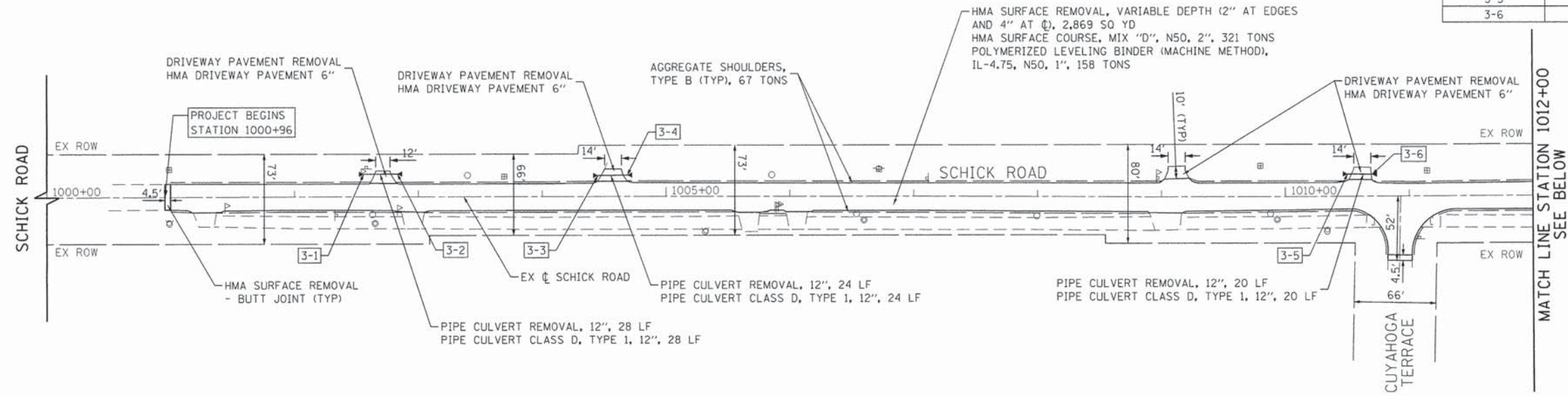


STRUCTURE NUMBER	TYPE	SIZE	STATION	OFFSET
2-1	METAL END SECTION	12"	107+44	17.3' LT
2-2	METAL END SECTION	12"	107+66	17.3' LT
2-3	METAL END SECTION	12"	109+07	18.9' RT
2-4	METAL END SECTION	12"	109+47	20.0' RT
2-5	METAL END SECTION	12"	110+59	21.0' RT
2-6	METAL END SECTION	12"	110+93	21.8' RT

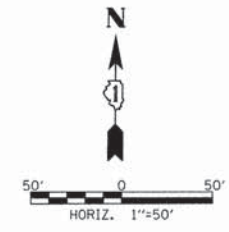
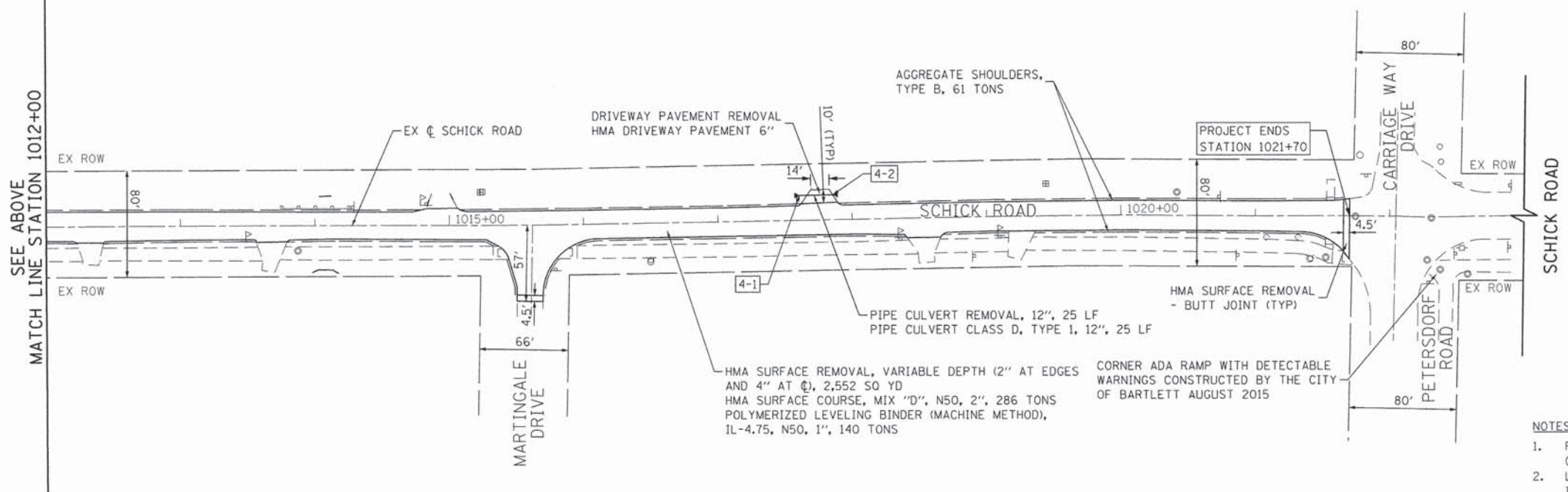
- NOTES:**
1. PIPE CULVERT REMOVAL SHALL INCLUDE THE REMOVAL OF CULVERT END SECTIONS.
  2. LOCATION OF STATIONS AND OFFSETS OF ALL END SECTIONS IS AT THE INVERT OF THE PIPE.
  3. THE CONTRACTOR SHALL VERIFY ALL INVERTS IN THE FIELD BEFORE REPLACEMENT. THE TOP OF DRIVEWAY CULVERT PIPES SHALL BE A MINIMUM OF 10" FROM THE TOP OF DRIVEWAY PAVEMENT.



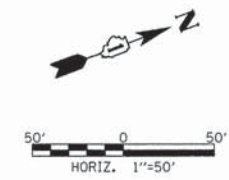
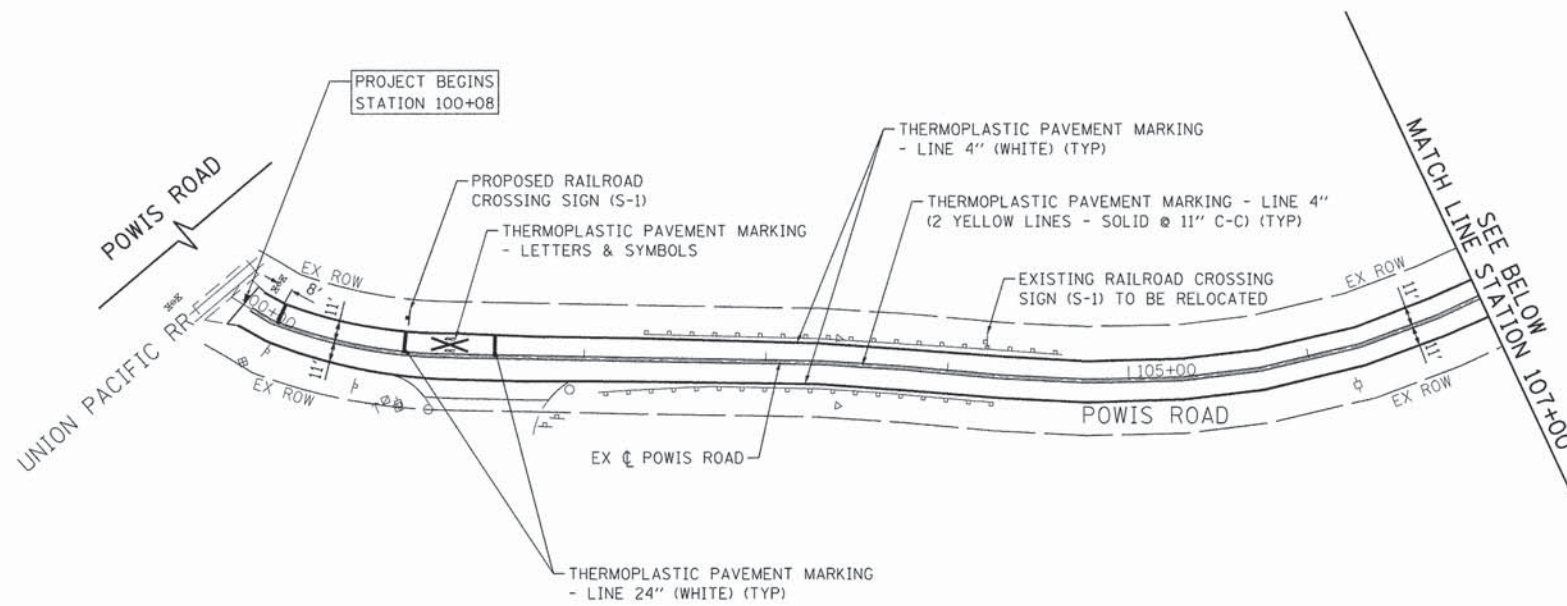
STRUCTURE NUMBER	TYPE	SIZE	STATION	OFFSET
3-1	METAL END SECTION	12"	1002+56	18.5' LT
3-2	METAL END SECTION	12"	1002+83	18.6' LT
3-3	METAL END SECTION	12"	1004+45	17.1' LT
3-4	METAL END SECTION	12"	1004+69	17.4' LT
3-5	METAL END SECTION	12"	1010+50	18.1' LT
3-6	METAL END SECTION	12"	1010+70	17.8' LT



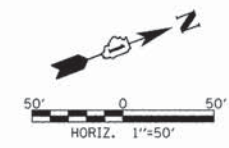
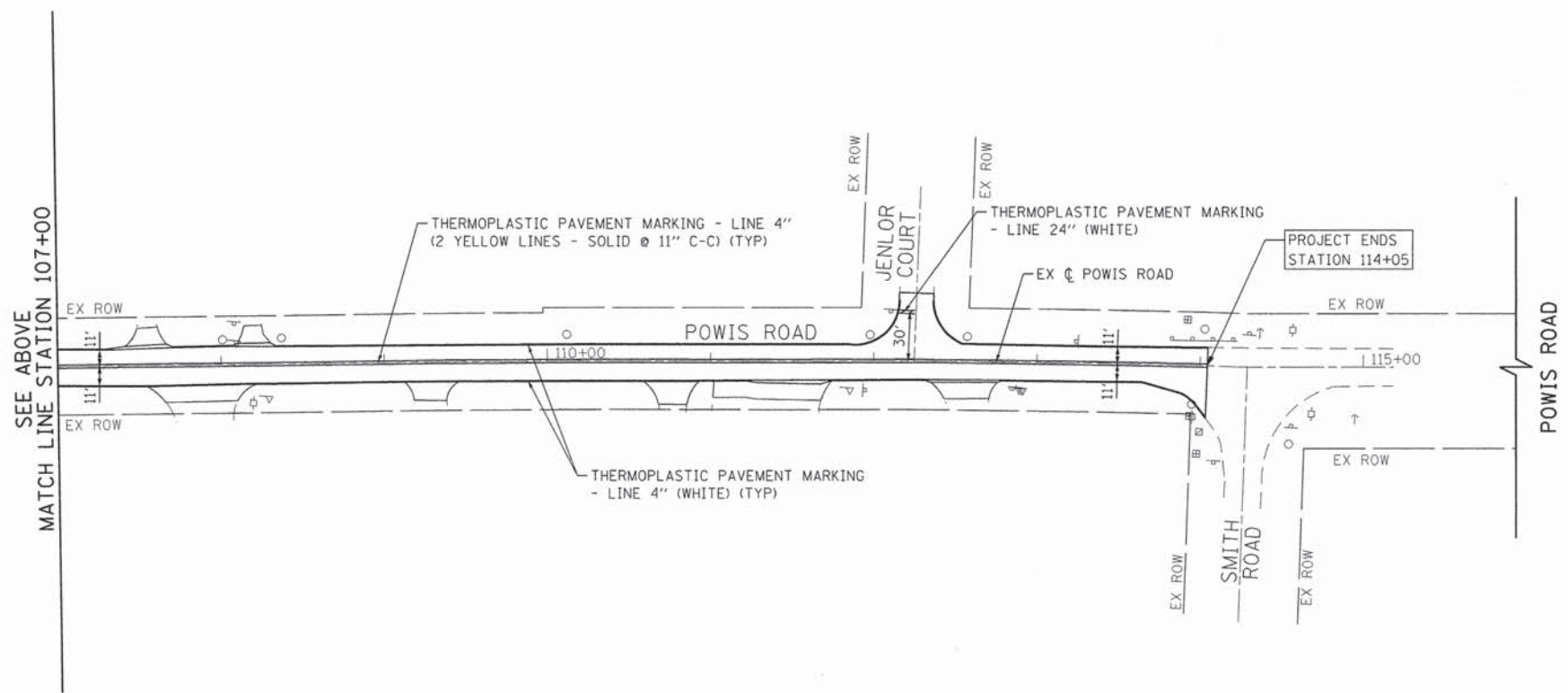
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4-1	METAL END SECTION	12"	1017+61	18.8' LT
4-2	METAL END SECTION	12"	1017+86	18.8' LT



- NOTES:**
- PIPE CULVERT REMOVAL SHALL INCLUDE THE REMOVAL OF CULVERT END SECTIONS.
  - LOCATION OF STATIONS AND OFFSETS OF ALL END SECTIONS IS AT THE INVERT OF THE PIPE.
  - THE CONTRACTOR SHALL VERIFY ALL INVERTS IN THE FIELD BEFORE REPLACEMENT. THE TOP OF DRIVEWAY CULVERT PIPES SHALL BE A MINIMUM OF 10" FROM THE TOP OF DRIVEWAY PAVEMENT.

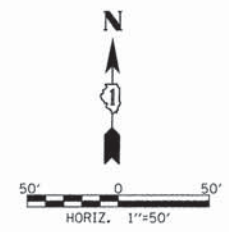
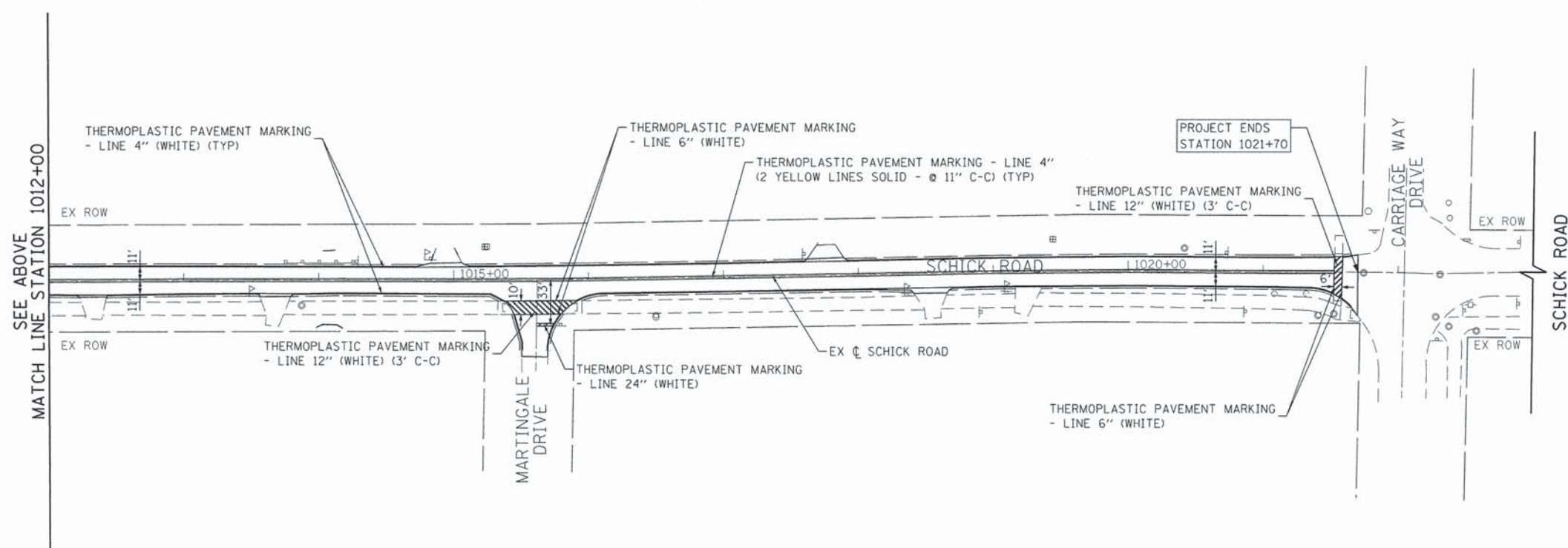
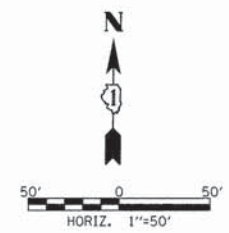
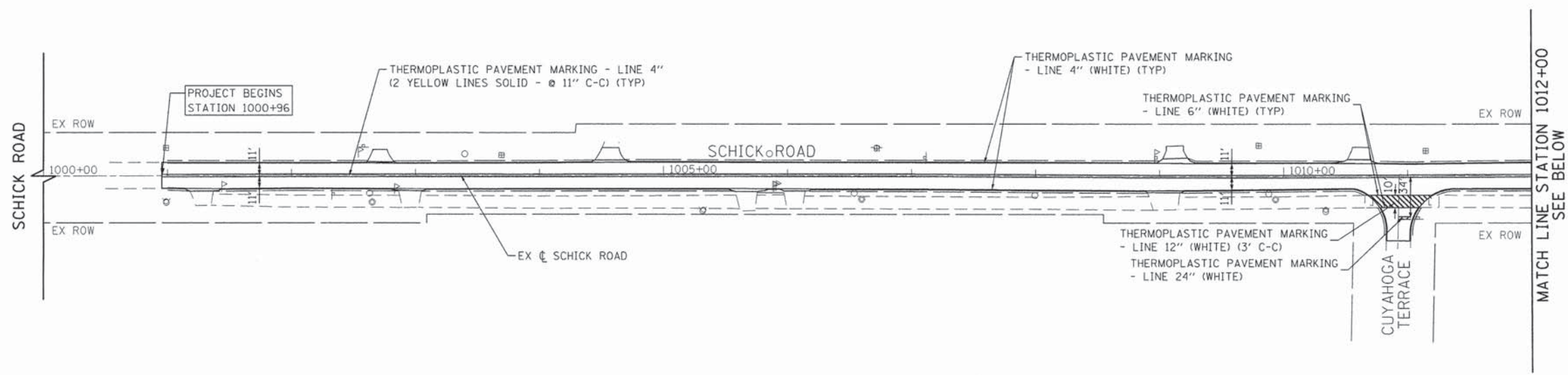


SIGN NUMBER	MUTCD CODE	EXISTING STATION	EXISTING OFFSET	EXISTING WIDTH (IN)	EXISTING HEIGHT (IN)	EXISTING AREA (SQ FT)	RELOCATE SIGN PANEL ASSEMBLY TYPE A	PROPOSED STATION	PROPOSED OFFSET	METAL POST TYPE B (FT)
S-1	W10-1	104+20	18' LT	36	36	9	1	101+00	20' LT	8



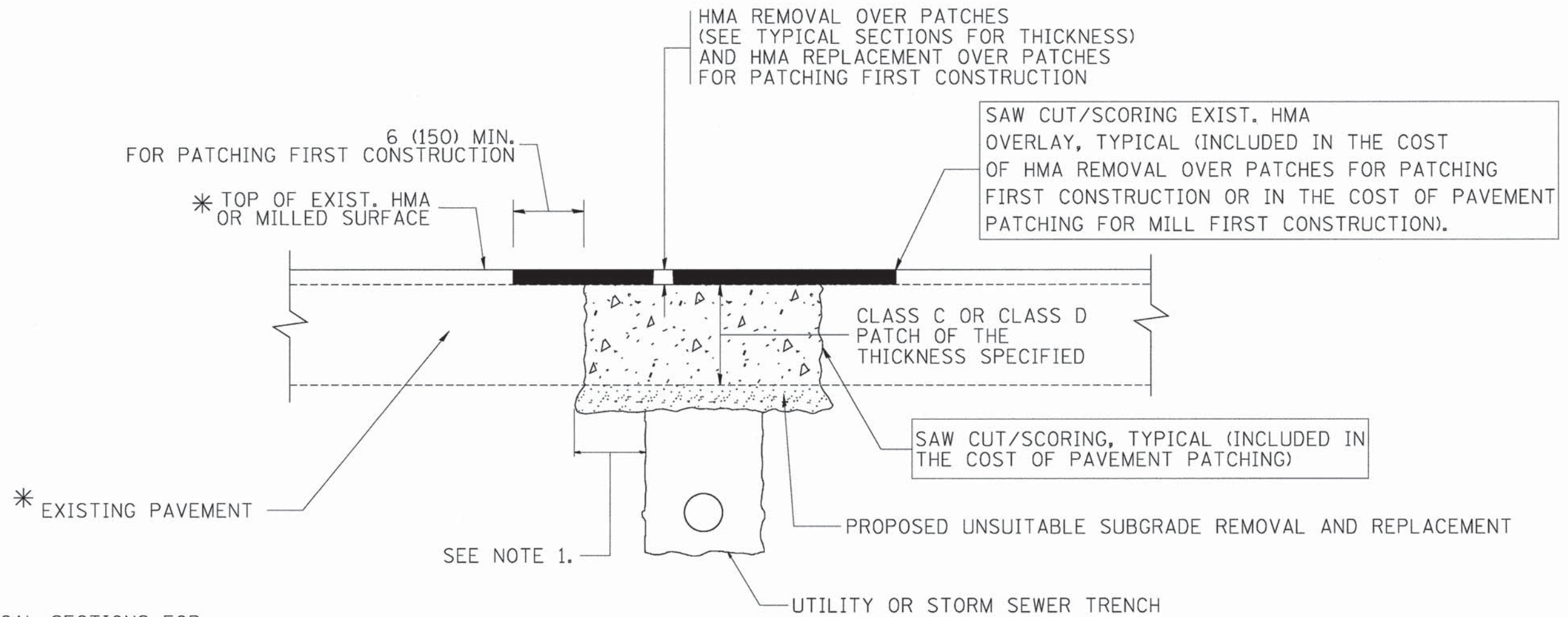
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G:\CH14\0092\Road\Sheets\4092-PM-1-POV.S.dgn	DRAWN - BSH	REVISED -	9-2530					15-07126-00-RS	DUPAGE	15	8	
PLOT SCALE = 50.0000' / in.	CHECKED - DWB	REVISED -	CONTRACT NO. 61C04									
#MODELNAME#	DATE - 10/12/15	REVISED -	ILLINOIS FED. AID PROJECT									
				SCALE: 1"=50'	SHEET 1 OF 2 SHEETS	STA. TO STA.						





FILE NAME =	USER NAME = .USER.	DESIGNED - BSH	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SCHICK ROAD RESURFACING PAVEMENT MARKING PLAN</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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#MODELNAME#		CHECKED - DWB	REVISED -					CONTRACT NO. 61C04				
		DATE - 10/12/15	REVISED -					ILLINOIS FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

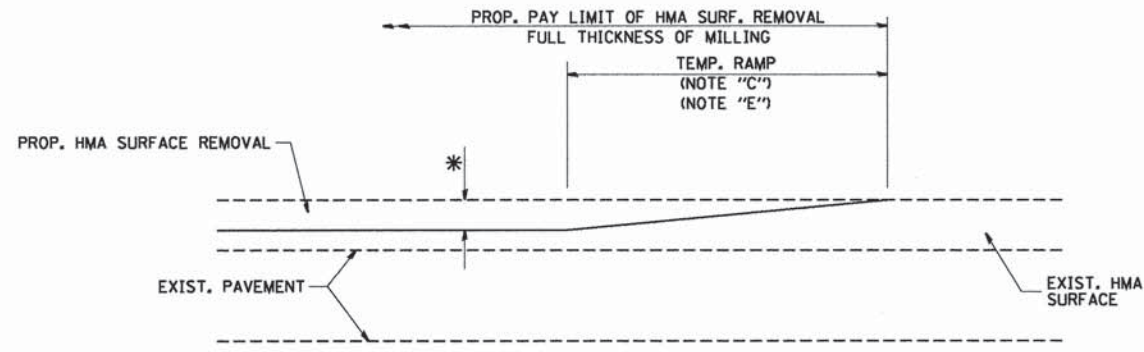
**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

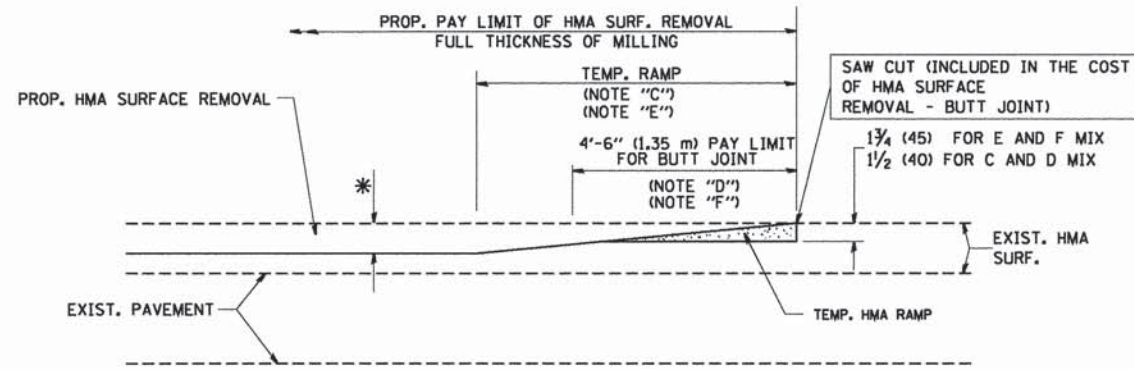
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		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<b>BD400-04 (BD-22)</b> CONTRACT NO. 61C04			
		PLOT SCALE = 5/8" = 1' / IN.	REVISED - R. BORO 09-04-07									
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08									





MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

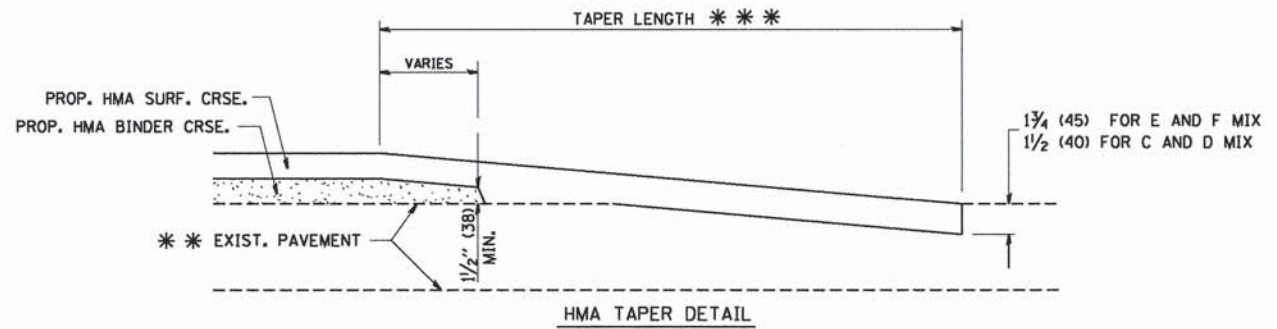
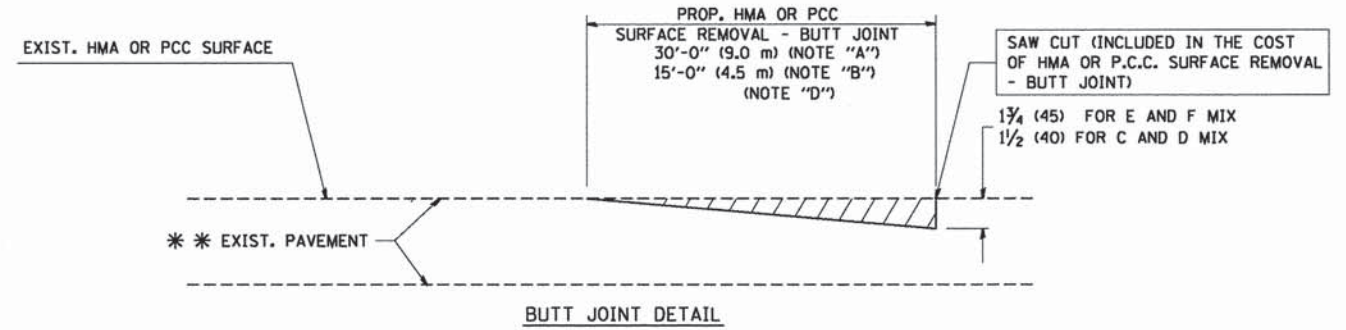
**OPTION 1**



HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

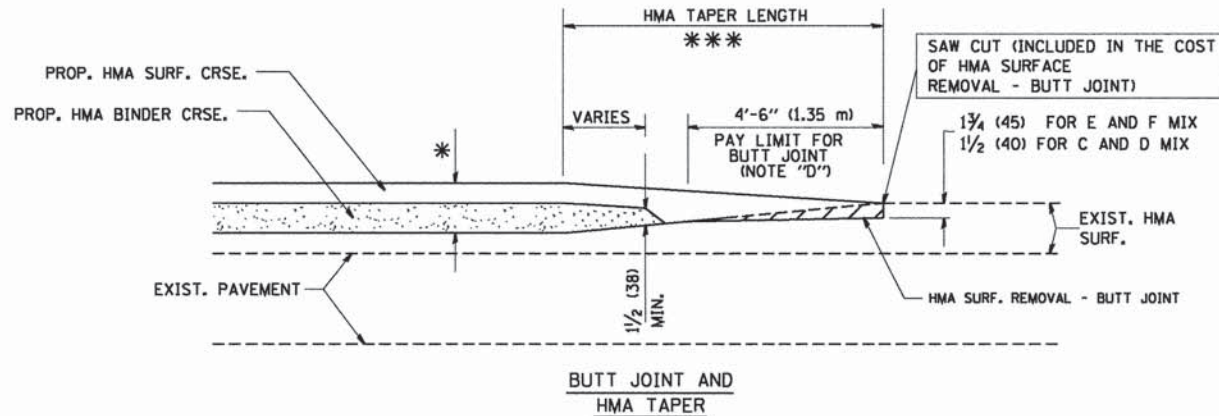
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

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PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG  
DRAWN -  
CHECKED -  
DATE - 06-13-90

REVISED - R. SHAH 10-25-94  
REVISED - A. ABBAS 03-21-97  
REVISED - M. GOMEZ 04-06-01  
REVISED - R. BORO 01-01-07

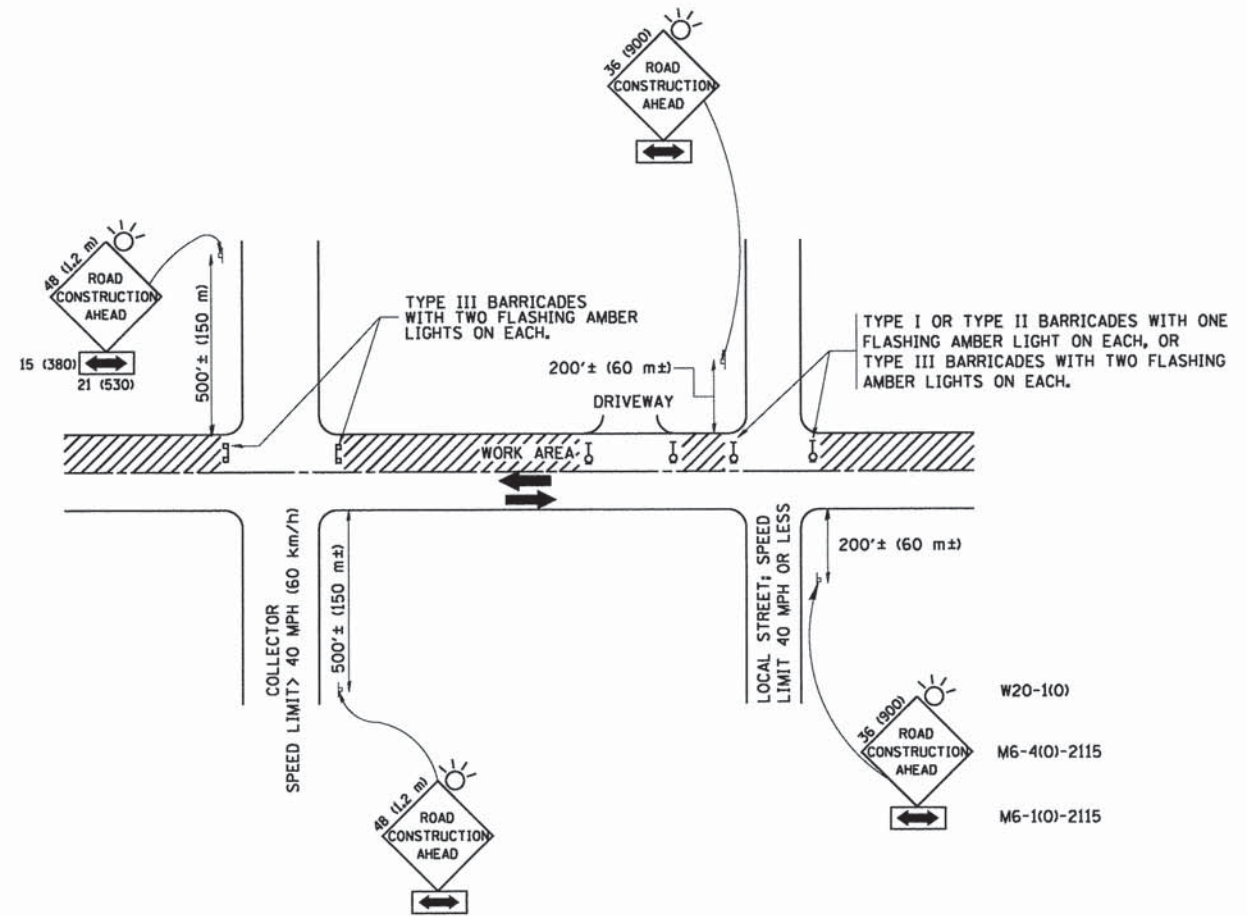
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9-2530	15-07126-00-RS	DUPAGE	15	11
BD400-05 BD32			CONTRACT NO. 61C04	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

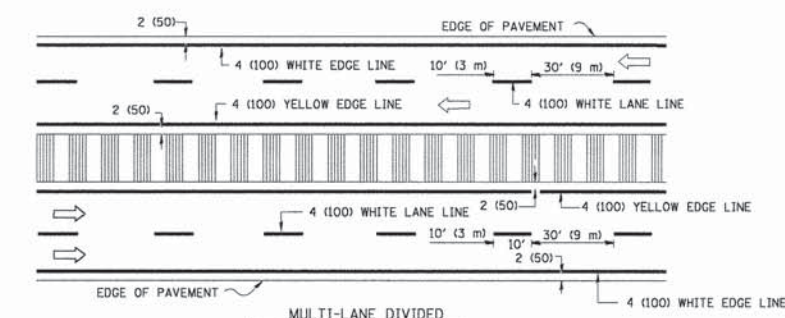
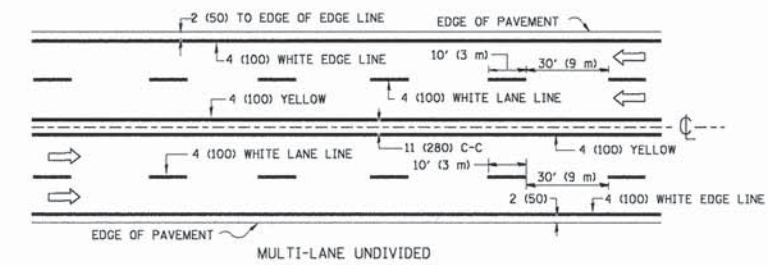
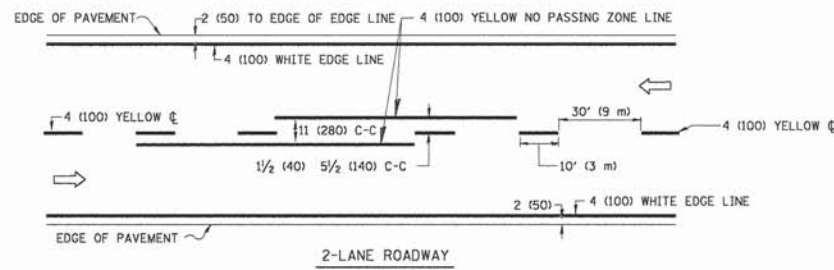
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

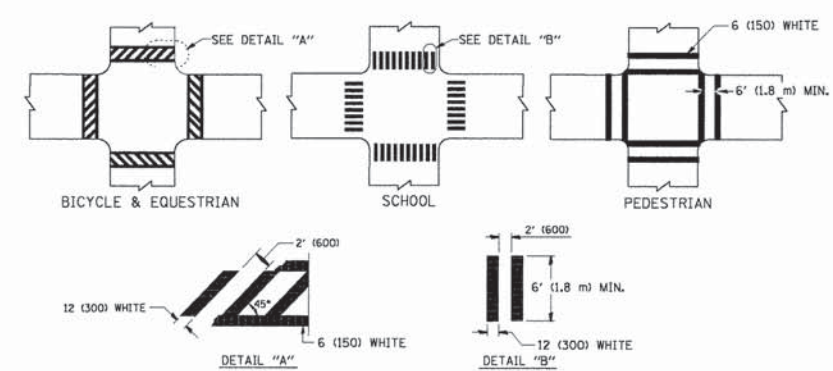
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9-2530	15-07126-00-RS	DUPAGE	15	12
TC-10			CONTRACT NO. 61C04	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

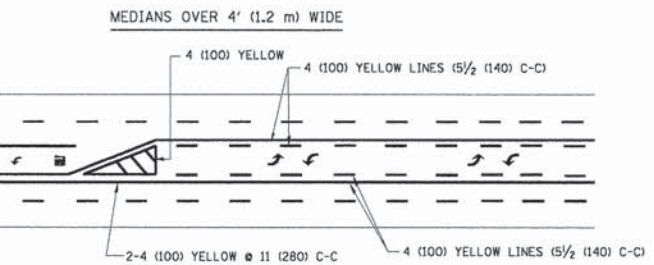
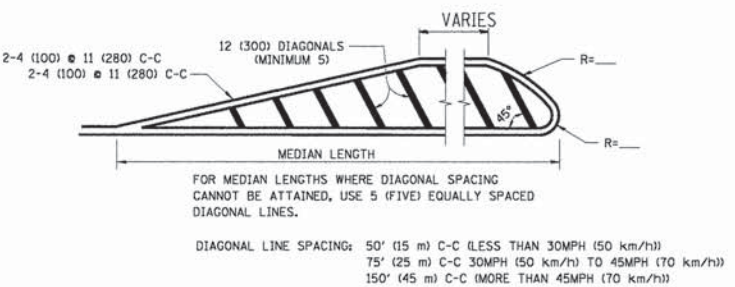
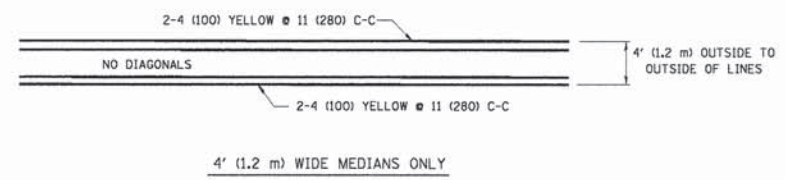




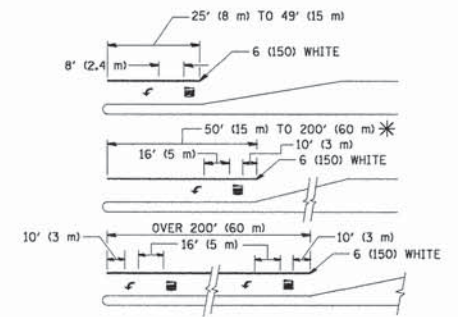
TYPICAL LANE AND EDGE LINE MARKING



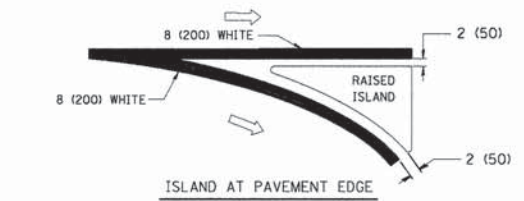
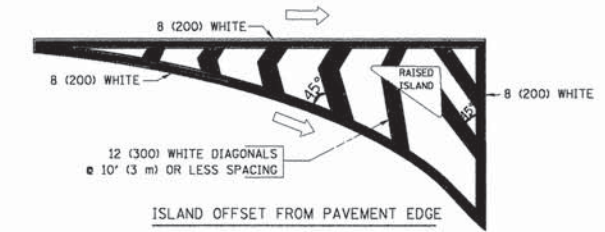
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH CENTERLINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 (3.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

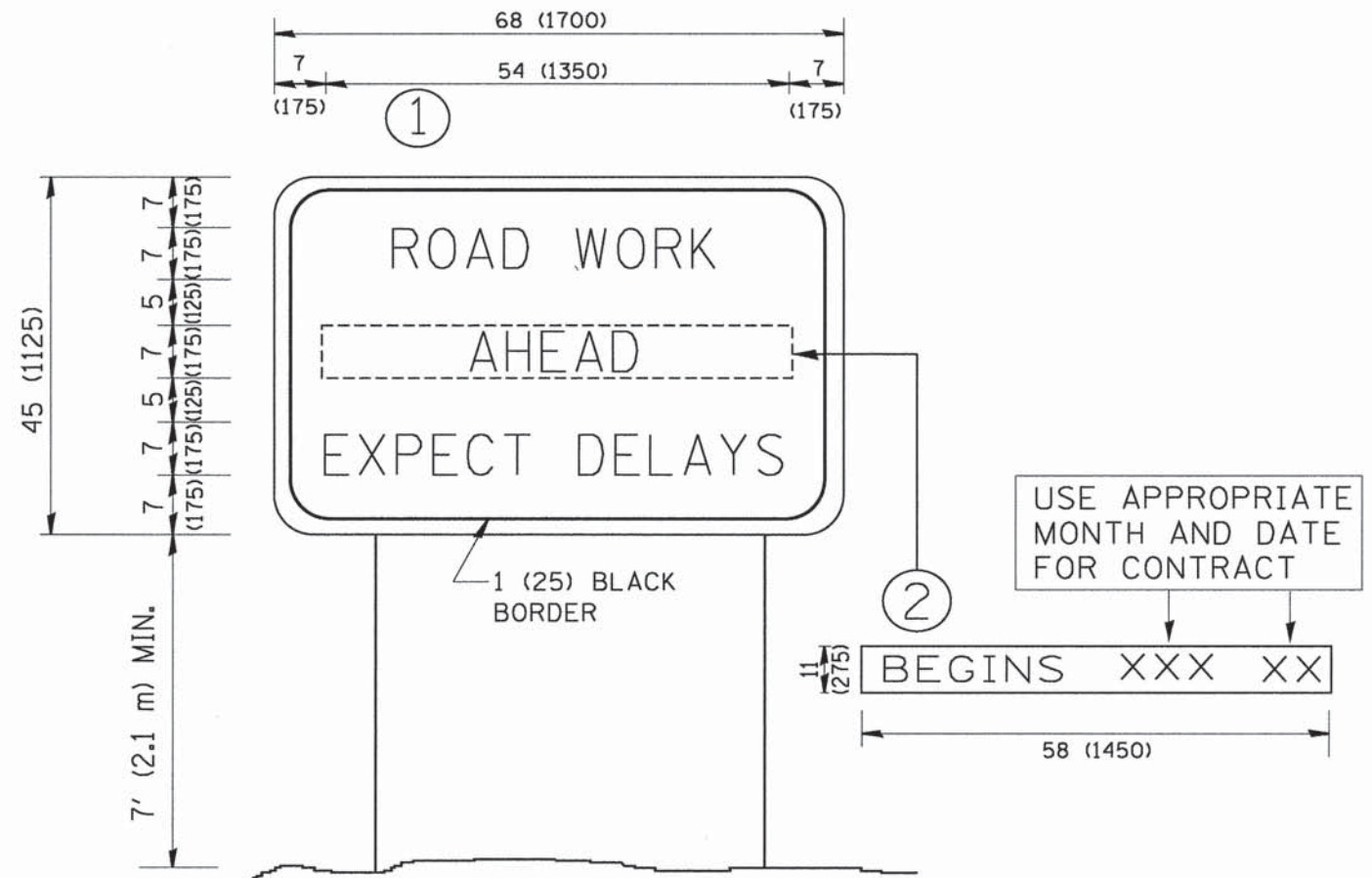
FILE NAME =	USER NAME = drvakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
RT.E	3.dgn	DRAWN -	REVISED - C. JUICIUS 09-09-09
	PLOT SCALE = 5/8" = 1" / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RT.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9-2530	15-07126-00-RS	DUPAGE	15	13
TC-13			CONTRACT NO. 61C04	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\to22.dgn	USER NAME = goglienobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

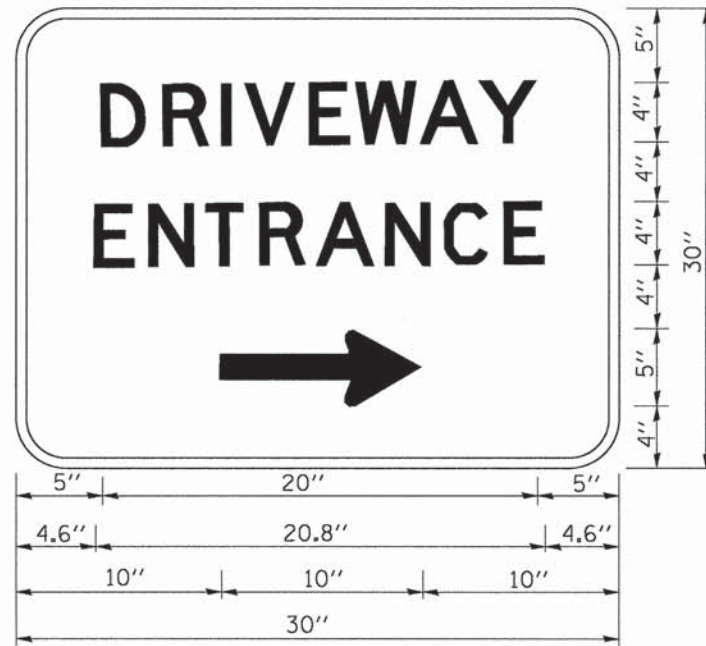
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 9-2530	SECTION 15-07126-00-R5	COUNTY DUPAGE	TOTAL SHEETS 15	SHEET NO. 14
TC-22			CONTRACT NO. 61C04	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gaglienobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
cd\pv_work\p\dot\gaglienobt\dl108315\td	Edgn	DRAWN -	REVISED -
	PLOT SCALE = 50.000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9-2530	15-07126-00-R5	DUPAGE	15	15
<b>TC-26</b>			CONTRACT NO. 61C04	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				