

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004-RS	WILL	34	1
ILLINOIS			CONTRACT NO. 80B34	

\* 34 + 1 = 35 TOTAL SHEETS

D-91-196-25

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE IMPROVEMENT IS LOCATED IN THE  
CITY OF LOCKPORT

TRAFFIC DATA

IL 7 (159th ST) 2023 ADT = 22,200  
POSTED SPEED LIMIT = 45 MPH

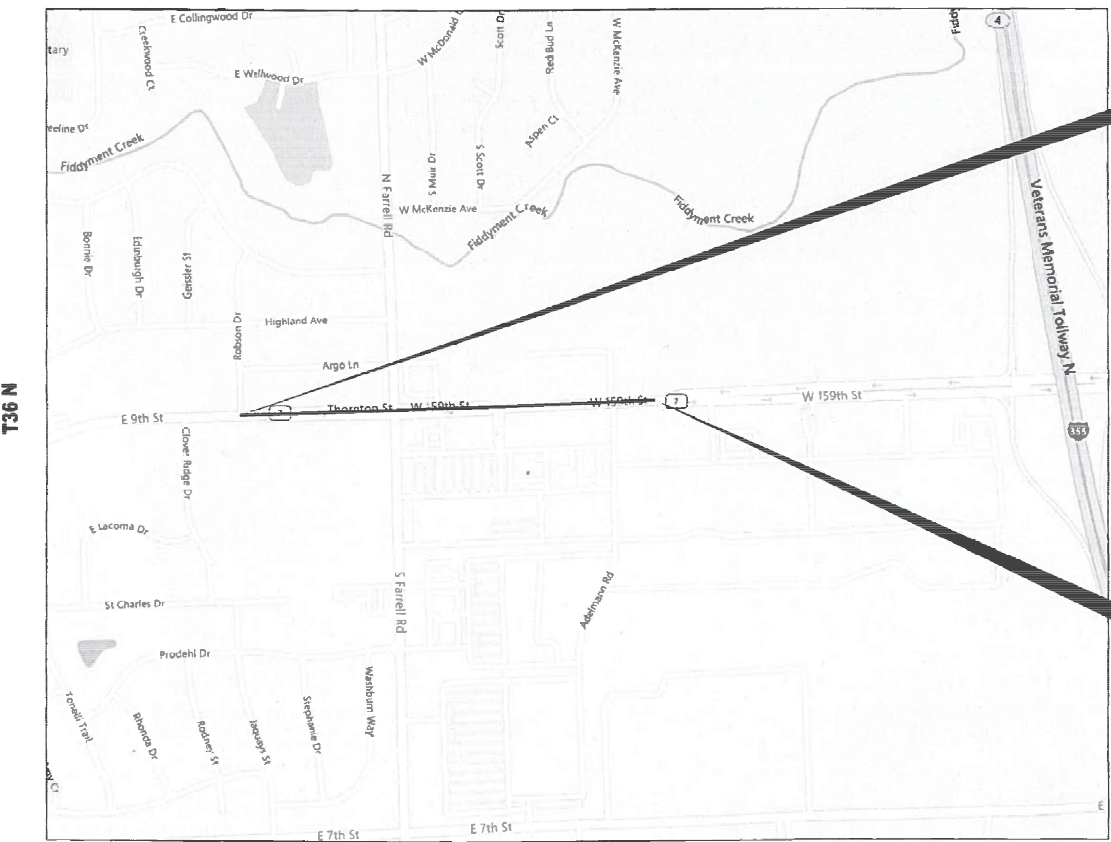
PROPOSED  
HIGHWAY PLANS

FAP ROUTE 351: IL 7(159TH ST.)  
FROM ROBSON DR. TO ADELLMAN DR.  
SECTION 2025-2004 RS  
PROJECT NHPP-TY86(450)

STANDARD OVERLAY, HMA SHOULDERS & ADA IMPROVEMENTS  
WILL COUNTY

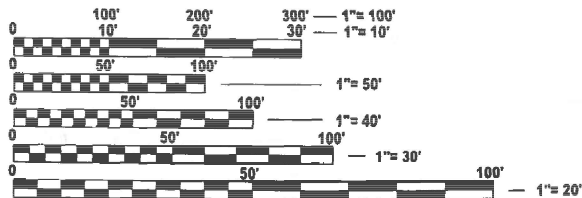
C-91-270-25

R 10 E



PROJECT BEGINS  
STA. 20+61

PROJECT ENDS  
STA. 53+02



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER : LUKASZ POCIECHA (847-705-4255)  
PROJECT MANAGER: VESELIN VELICHKOV

CONTRACT NO. 80B34

GROSS LENGTH = 3241.00 FT. = 0.6 MILE = NET LENGTH



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED Oct. 15 2025  
for review IR  
December 5 2025  
See E.A. etc  
ENGINEER OF DESIGN AND ENVIRONMENT  
December 5 2025  
Harry  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

MODEL: GenNotesH01 [Sheet]  
FILE NAME: c:\pw\_work\tdot\illinois.gov\4112420\BD119625-shit-gennote.dgn

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.		DESCRIPTION	STANDARD NO.	DESCRIPTION								
1	TITLE SHEET		000001-09	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS	<div>1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).</div> <div>2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, VILLAGE OF TINLEY PARK AND COOK COUNTY.</div> <div>3. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.</div> <div>4. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.</div> <div>5. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.</div> <div>6. THE RESIDENT ENGINEER SHALL CONTACT ERIC CAMPOS AREA TRAFFIC FIELD ENGINEER, AT ERIC.CAMPOS@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.</div> <div>7. ANY DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.</div> <div>8. ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.</div> <div>9. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE PROJECT LIMITS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.</div> <div>10. THE CONTRACTOR SHALL CONTACT THE DISTRICT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.</div> <div>11. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM AND FOUNDATIONS AND VERIFYING THE MAST ARM LENGHTS.</div> <div>12. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.</div> <div>13. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.</div> <div>14. AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH AGGREGATE SUBGRADE IMPROVEMENT (CUYD) WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND THE IDOT SUBGRADE STABILITY MANUAL.</div> <div>15. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT CONTRACTOR'S EXPENSE.</div> <div>16. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINING OF WORK..</div> <div>17. PAVEMENT MARKING TAPE TYPE IV SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES. ALL MILLED SURFACES SHALL BE A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES.ANY DEVIATIONS SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.</div> <div>18. ALL PAVEMENT PATCHING LOCATIONS SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION.</div> <div>19. THE AGGREGATE GRADATION FOR THE LOWER 9" OF AGGREGATE SUBGRADE IMPROVEMENT 12" SHALL BE CS 1 OR RR 1.</div> <div>20. WHEN SEVERELY DETORIATED SECTIONS OF STABILIZED HMA MEDIAN SURFACE ARE ENCOUNTERED DURING THE MEDIAN MILLING AND RESURFACING,THE SECTIONS SHALL BE PATCHED WITH CLASS D PATCH,10INCH. THE TYPE AND LOCATION SHALL BE COORDINATED WITH THE RESIDENT ENGINEER.</div>							
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES		280001-07	TEMPORARY EROSION CONTROL SYSTEM								
3-4	SUMMARY OF QUANTITIES		482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT								
5-6	EXISTING AND PROPOSED TYPICAL SECTIONS		604001-05	FRAME AND LIDS TYPE 1								
7-8	PROPOSED PAVEMENT MARKING PLANS		701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE								
9-11	PROPOSED SIDEWALK RAMP DETAILS		701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY								
12-15	DETECTOR LOOP PLANS		701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS								
16	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING(BD-08)		701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH								
17	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT(BD-22)		701311-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY								
18	BUTT JOINT AND HMA TAPER DETAILS (BD-32)		701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH								
19	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTION AND DRIVEWAYS (TC-10)		701336-07	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > 45 MPH								
20	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)		701501-06	URBAN LANE CLOSURE, 2L, UNDIVIDED								
21	DISTRICT ONE TYPICAL PAVEMENT MARKING (TC-13)		701502-09	URBAN LANE CLOSURE, 2L, WITH BIDIRECTIONAL LEFT TURN LANE								
22	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC (TC-14)		701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN								
23	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)		701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN								
24	ARTERIAL ROAD INFORMATION SIGN (TC-22)		701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION								
25	DRIVEWAY ENTRANCE SIGNING (TC-26)		701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE								
26	DISTRICT 1 - MAST ARM MOUNTED STREET NAME SIGNS (TS-02)		701901-11	TRAFFIC CONTOL DEVICES								
27-29	DISTRICT 1 - STANDARD SIGNAL DESIGN DETAILS (TS-05)		781001-04	TYPICAL APPLIOICATION RAISED REFLECTIVE PAVEMENT MARKERS								
30	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)		886001-01	DETECTOR LOOP INSTALLATIONS								
31	PROJECT DETAIL FOR SINGLEE PERPENDICULAR CURB RAMPS(PD-01)		886006-01	TYPICAL LAYOUTS FOR DETECTOR LOOP								
32	PROJECT DETAIL FOR SINGLEE PERPENDICULAR CURB RAMPS(PD-02)											
33	PROJECT DETAIL FOR SINGLEE PERPENDICULAR CURB RAMPS WITH TURNING SPACE(PD-04)											
34	PROJECT DETAIL FOR PARALLEL CURB RAMPS(PD-06)											
GENERAL NOTES CONTINUE ON NEXT SHEET												
	USER NAME = Merin,Jose	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES AND INDEX OF SHEETS IL 7 (159TH ST.) (ROBSON DR. TO ADELMANN DR.)	F A P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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		CHECKED -	REVISED -			CONTRACT NO. 80B34						
	PLOT DATE = 11/19/2025	DATE -	REVISED -									
						SCALE:	SHEET 1 OF 1 SHEETS	STA.	TO STA.			
						ILLINOIS					FED. AID PROJECT	

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SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	DRAINAGE	SIGNAL			
					80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0043	0021			
	20200100	EARTH EXCAVATION	CU YD	150	150					
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	221	221					
	21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	67	67					
	25200110	SODDING, SALT TOLERANT	SQ YD	67	67					
	25200200	SUPPLEMENTAL WATERING	UNIT	0.67	0.67					
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	19	19					
	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	221	221					
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15124	15124					
	40600370	LONGITUDINAL JOINT SEALANT	FOOT	8642	8642					
	40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	34	34					
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	149	149					
	40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	919	919					
	40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	47	47					
	40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	2183	2183					
	42001300	PROTECTIVE COAT	SQ YD	788	788					
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1464	1464					
	42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	528	528					
	42400800	DETECTABLE WARNINGS	SQ FT	175	175					
	44000100	PAVEMENT REMOVAL	SQ YD	202	202					
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	411	411					
	44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	22269	22269					
	44000600	SIDEWALK REMOVAL	SQ FT	1464	1464					
* SPECIALTY ITEM										

SUMMARY OF QUANTITIES					TYPE CODE					
					URBAN	URBAN	URBAN	URBAN	URBAN	URBAN
					ROADWAY	DRAINAGE	SIGNAL			
					80% FED 20% STATE	80% FED 20% STATE	80% FED 20% STATE			
	Code No.	Item	Unit	Total Quantity	0005	0043	0021			
	44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	10	10					
	44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	17	17					
	44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	216	216					
	48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	247	247					
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	4	4					
	60920012	PIPE CULVERTS TO BE CLEANED 12"	FOOT	90	90					
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	145	145					
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1					
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1					
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	4	4					
	67100100	MOBILIZATION	L SUM	1	1					
	70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1					
	70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1					
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					
	70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1					
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
	70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1	1					
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	16703	16703					

				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITY IL 7 (159TH ST.) ROBSON DR. TO E OF ADALMANN DR.				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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												CONTRACT NO. 80B34				
												ILLINOIS FED. AID PROJECT				
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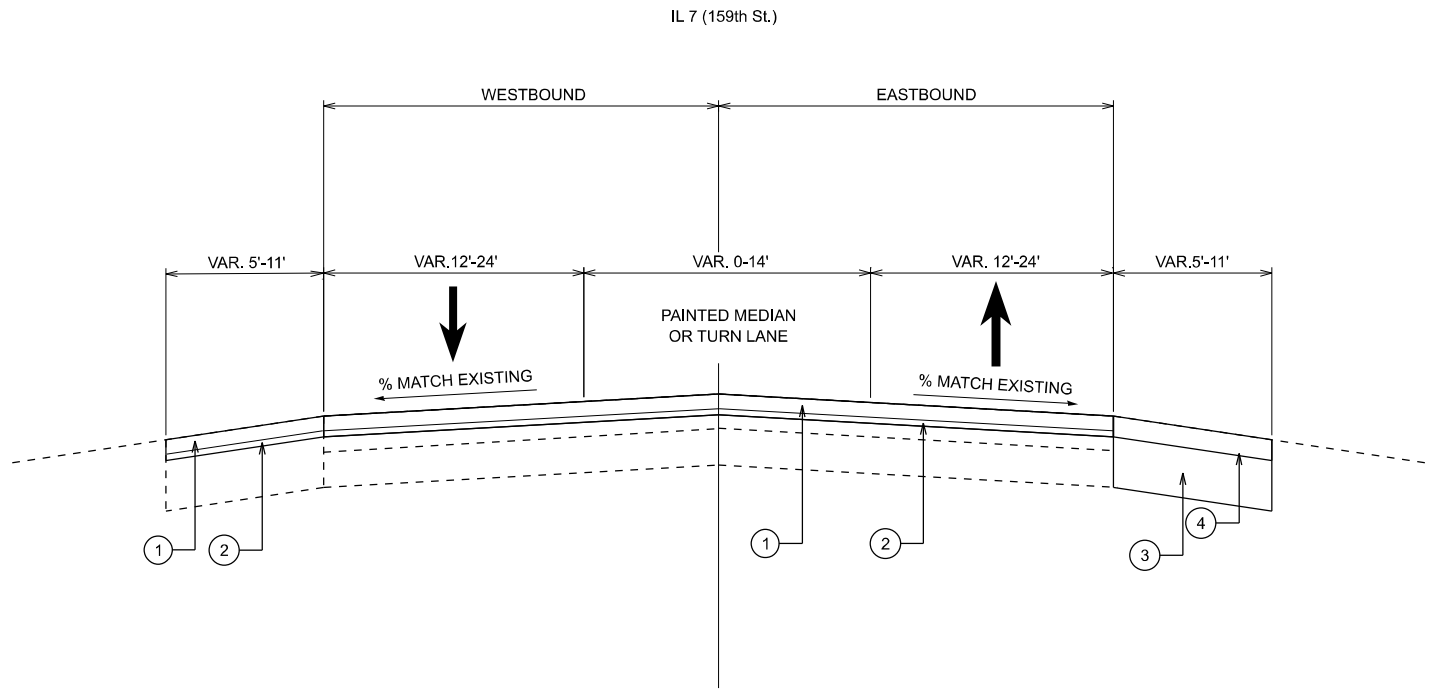
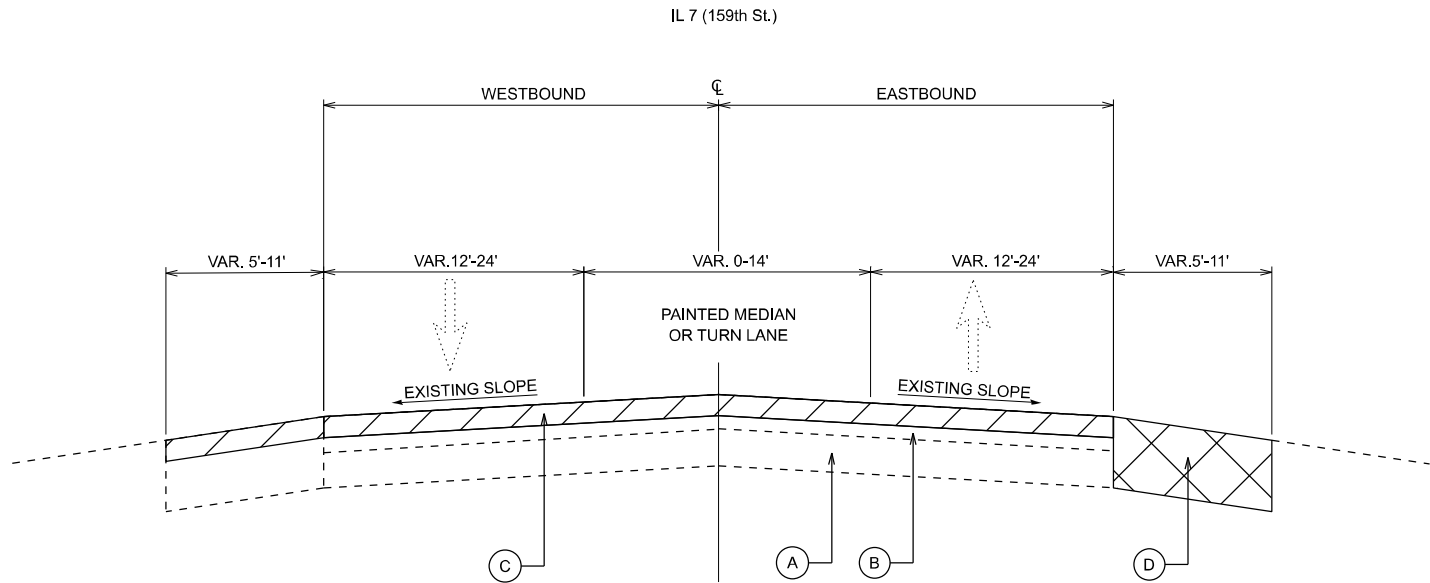
\* SPECIALTY ITEM

## SUMMARY OF QUANTITIES

**SUMMARY OF QUANTITY**  
**IL 7 (159TH ST.) ROBSON DR. TO E OF ADALMANN DR.**



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## EXISTING LEGEND

- (A) — PCC PAVEMENT,  $\pm$  7"
- (B) — HMA SURFACE COURSE,  $\pm$  6"
- (C) — HMA SURFACE REMOVAL, 2 1/2"
- (D) — EARTH EXCAVATION, 24" (AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER)

## PROPOSED LEGEND

- (1) — POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT 9.5, MIX "F", N80, 1 3/4"
- (2) — POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- (3) — AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (4) — HOT-MIX ASPHALT SHOULDERS, 8"

### NOTES:

- SAWCUT THE EXISTING HMA SHOULDER 1' FROM THE OUTSIDE EDGE FOR A CLEAN LINE FOR THE HMA SHOULDER WIDENING. WHERE THE EXISTING HMA SHOULDER IS 1' OR LESS, SAWCUT ON THE 12' LANE.
- THE CONTRACTOR SHALL MILL BEFORE PATCHING PER BD-22
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLYMERIZED HMA BINDER COURSE IL-4.75 N50

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP
MIXTURE TYPE	AIR VOIDS @ Ndesign	
MAINLINE RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT 9.5, MIX "F", N80, 1 3/4"	3.5% @ 80 Gyr.	QCP
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 4-75, N50 3/4"	3.5% @ 50 Gyr.	QC/QA
HOT MIX ASPHALT SHOULDERS, 8"		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT 9.5, MIX "F", N80, 1 3/4"	3.5% @ 80 Gyr.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70 6 1/4"	4% @ 70 Gyr.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 Gyr.	QC/QA
MEDIAN SURFACE		
HOT MIX ASPHALT SURFACE COURSE, MIX "D", IL 9.5, N50, 2"	4% @ 50 Gyr.	QC/QA
QMP Designations: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)		

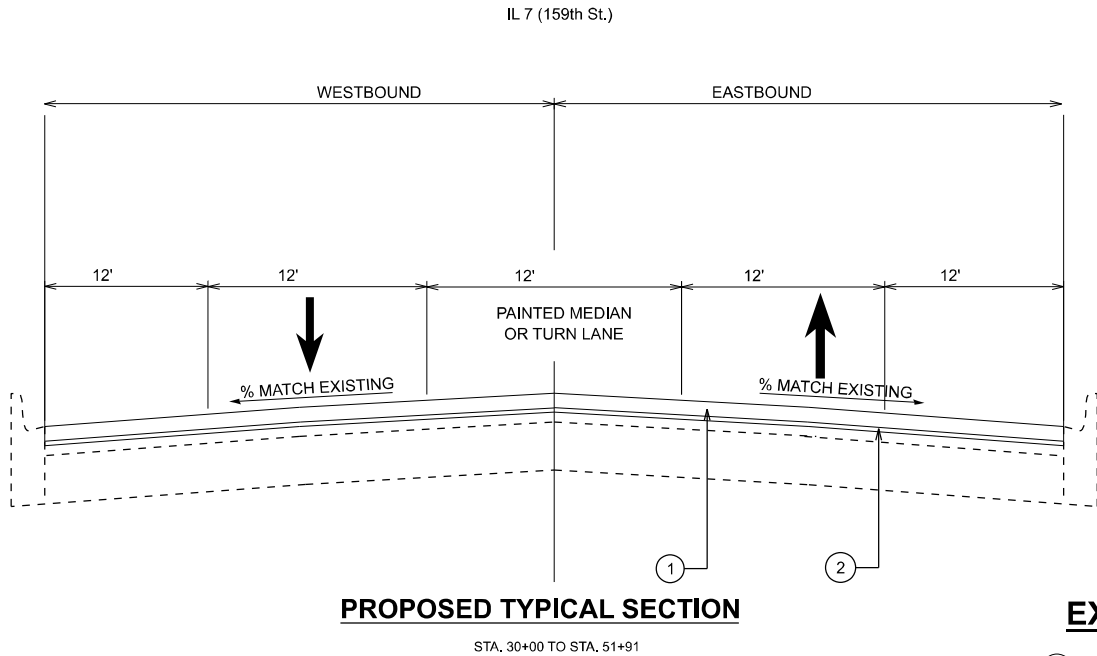
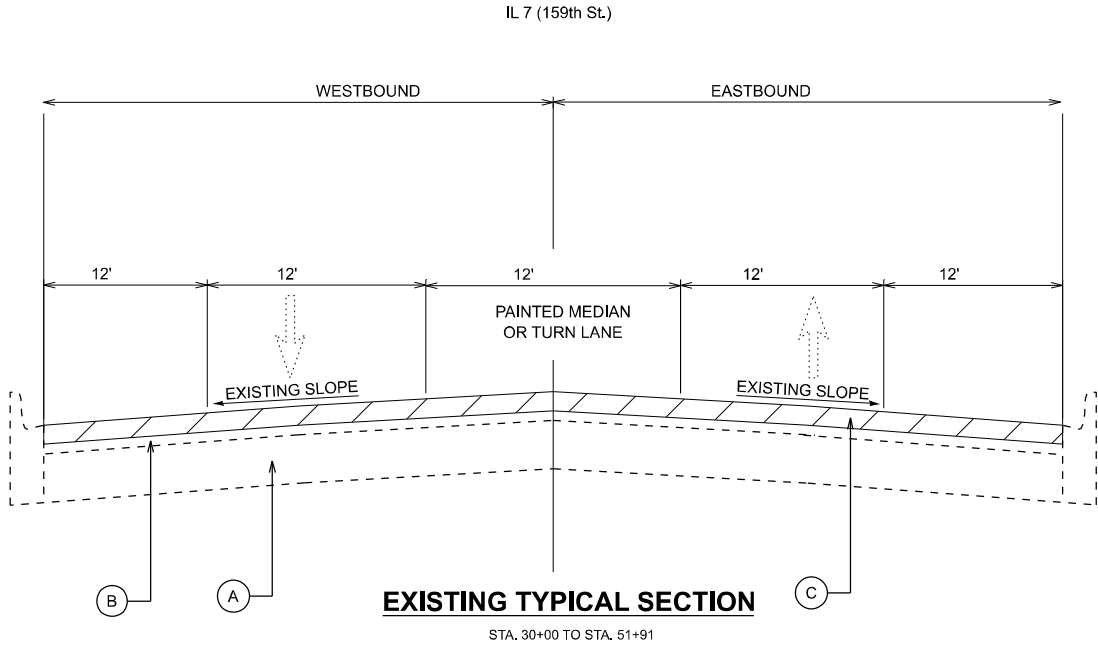
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

WHEN SHOULDER RESURFACING OF 6 FEET OR LESS IS ALLOWED TO BE PLACED SIMULTANEOUSLY WITH THE ADJACENT TRAFFIC LANE, THE MAINLINE QUALITY MANAGEMENT PROGRAM WILL BE ENFORCED FOR THE MAINLINE AND SHOULDER.

MODEL: Typical section-1 (Sheet) FILE NAME: c:\paw_work\paw\illinois.gov_merlin.jose@illinois.gov\1124205\119625-sh+typical.dgn	USER NAME = merlin.jose	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION IL ROUTE 7 (159TH ST.) ROBSON DR. TO E OF ADELMANN DR.			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					351	2025-2004 RS	WILL	34	5
		CHECKED -	REVISED -					CONTRACT NO. 80B34				
	PLOT DATE = 11/20/2025	DATE -	REVISED -		SCALE:	SHEET 1	OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

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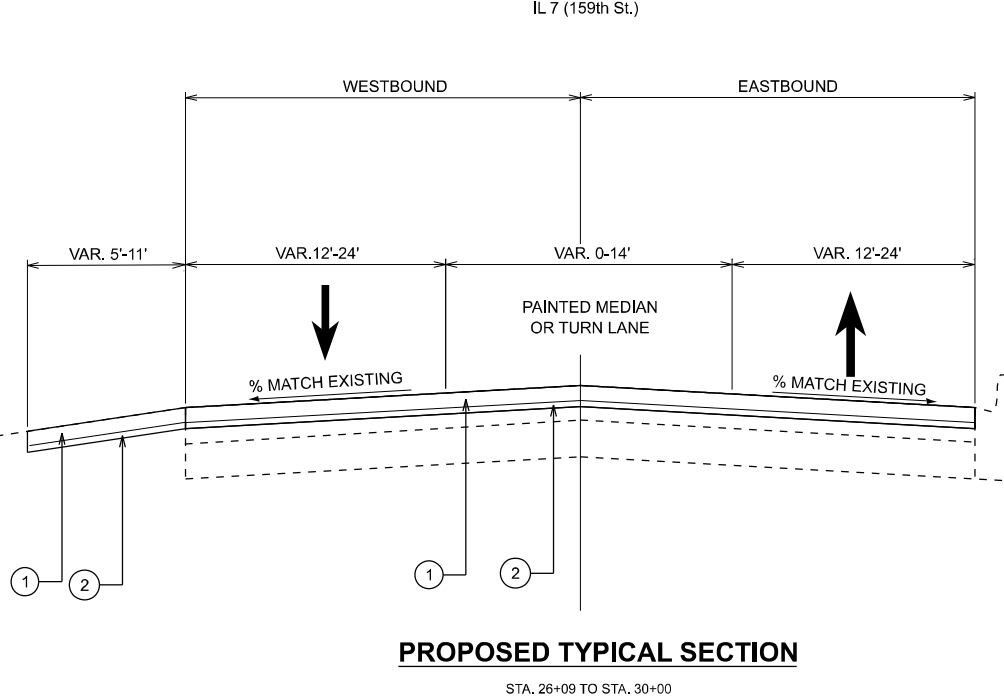
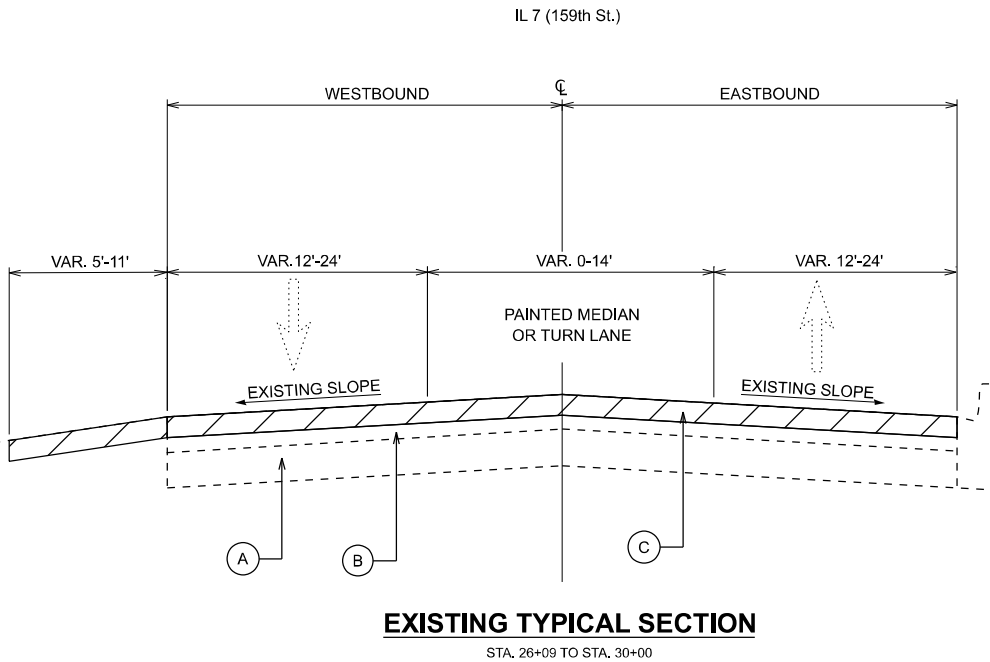
- A — PCC PAVEMENT,  $\pm 7"$
- B — HMA SURFACE COURSE,  $\pm 6"$
- C — HMA SURFACE REMOVAL, 2 1/2"
- D — EARTH EXCAVATION, 24" (AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER)

### PROPOSED LEGEND

- 1 — POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT 9.5, MIX "F", N80, 1 3/4"
- 2 — POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50, 3/4"
- 3 — AGGREGATE SUBGRADE IMPROVEMENT, 12"
- 4 — HOT-MIX ASPHALT SHOULDERS, 8"

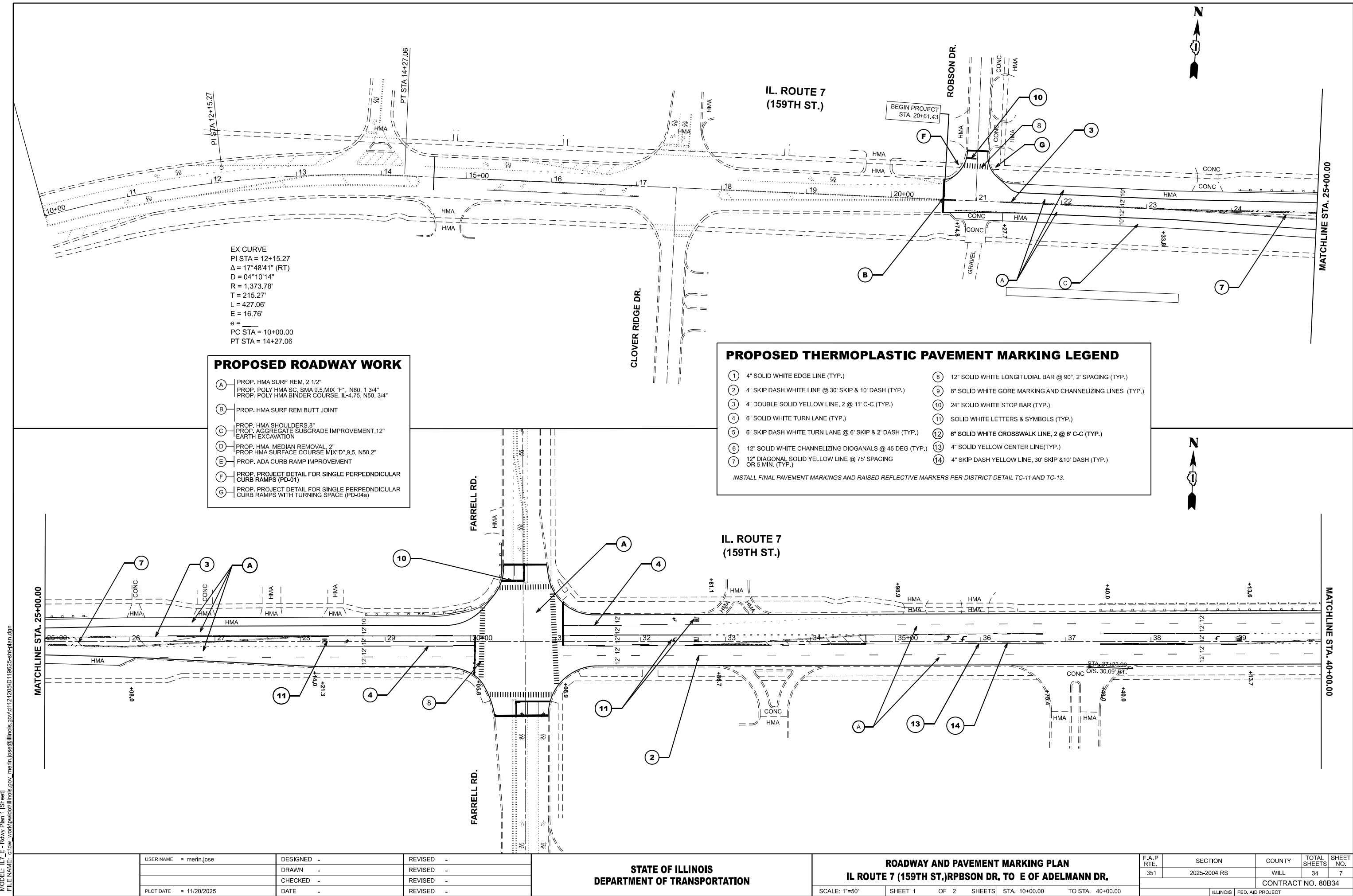
#### NOTES:

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- THE CONTRACTOR SHALL MILL BEFORE PATCHING PER BD-22
- THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED ON THE POLYMERIZED HMA BINDER COURSE IL-4.75 N50



	USER NAME = merin,jose	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION IL ROUTE 7 (159TH ST) ROBSON DR. TO E OF ADELMANN DR.			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					351	2025-2004 RS	WILL	34	6
		CHECKED -	REVISED -					CONTRACT NO. 80B34				
	PLOT DATE = 11/20/2025	DATE -	REVISED -		SCALE:	SHEET 2	OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

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	CHECKED -	REVISED -
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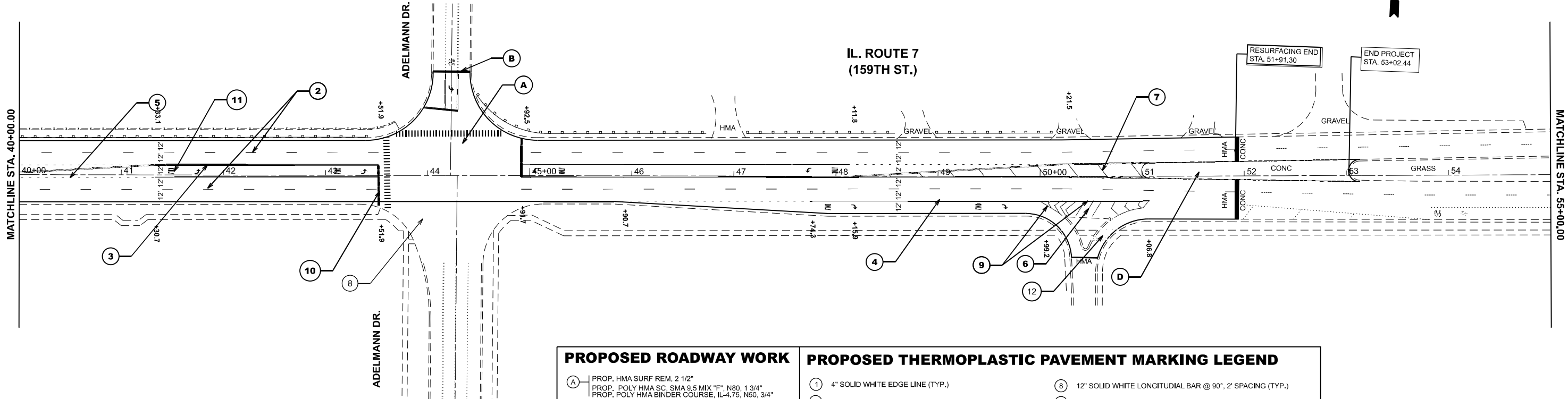
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN  
IL ROUTE 7 (159TH ST.)RBPSON DR. TO E OF ADELMANN DR.

SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 10+00.00 TO STA. 40+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	7
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

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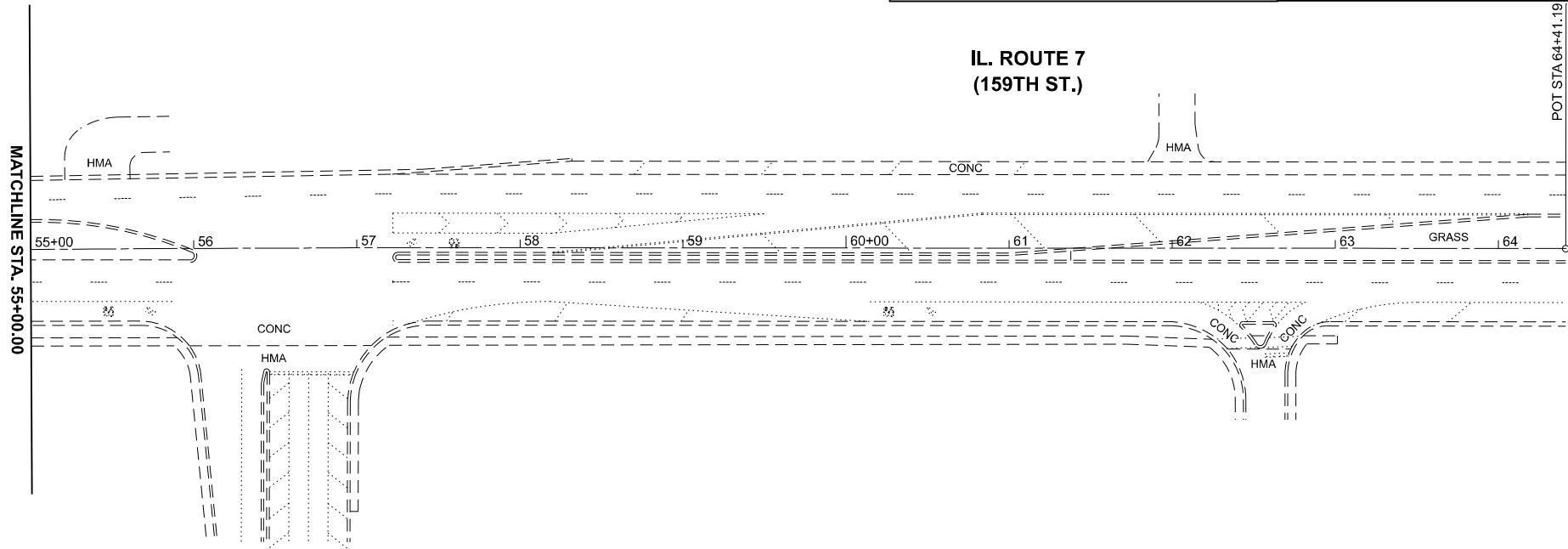
#### PROPOSED ROADWAY WORK

- (A) PROP. HMA SURF REM, 2 1/2"  
PROP. POLY HMA SC, SMA 9.5 MIX "F", N80, 1 3/4"  
PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
- (B) PROP. HMA SURF REM BUTT JOINT
- (C) PROP. HMA SHOULDER, 8"  
PROP. AGGREGATE SUBGRADE IMPROVEMENT, 12"  
EARTH EXCAVATION
- (D) PROP. HMA MEDIAN REMOVAL, 2"  
PROP HMA SURFACE COURSE MIX "D", 9.5, N50, 2"
- (E) PROP. ADA CURB RAMP IMPROVEMENT
- (F) PROP. PROJECT DETAIL FOR SINGLE PERPEDNDICULAR CURB RAMPS (PD-01)
- (G) PROP. PROJECT DETAIL FOR SINGLE PERPEDNDICULAR CURB RAMPS WITH TURNING SPACE (PD-04)

#### PROPOSED THERMOPLASTIC PAVEMENT MARKING LEGEND

- (1) 4" SOLID WHITE EDGE LINE (TYP.)
- (2) 4" SKIP DASH WHITE LINE @ 30' SKIP & 10' DASH (TYP.)
- (3) 4" DOUBLE SOLID YELLOW LINE, 2 @ 11' C-C (TYP.)
- (4) 6" SOLID WHITE TURN LANE (TYP.)
- (5) 6" SKIP DASH WHITE TURN LANE @ 6' SKIP & 2' DASH (TYP.)
- (6) 12" SOLID WHITE CHANNELIZING DIOGANALS @ 45 DEG (TYP.)
- (7) 12" DIAGONAL SOLID YELLOW LINE @ 75' SPACING
- (8) 12" SOLID WHITE LONGITUDIAL BAR @ 90°, 2' SPACING (TYP.)
- (9) 8" SOLID WHITE GORE MARKING AND CHANNELIZING LINES (TYP.)
- (10) 24" SOLID WHITE STOP BAR (TYP.)
- (11) SOLID WHITE LETTERS & SYMBOLS (TYP.)
- (12) 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
- (13) 4" SOLID YELLOW CENTER LINE (TYP.)
- (14) 4" SKIP DASH YELLOW LINE, 30' SKIP & 10' DASH (TYP.)

INSTALL FINAL PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS PER DISTRICT DETAIL TC-11 AND TC-13.



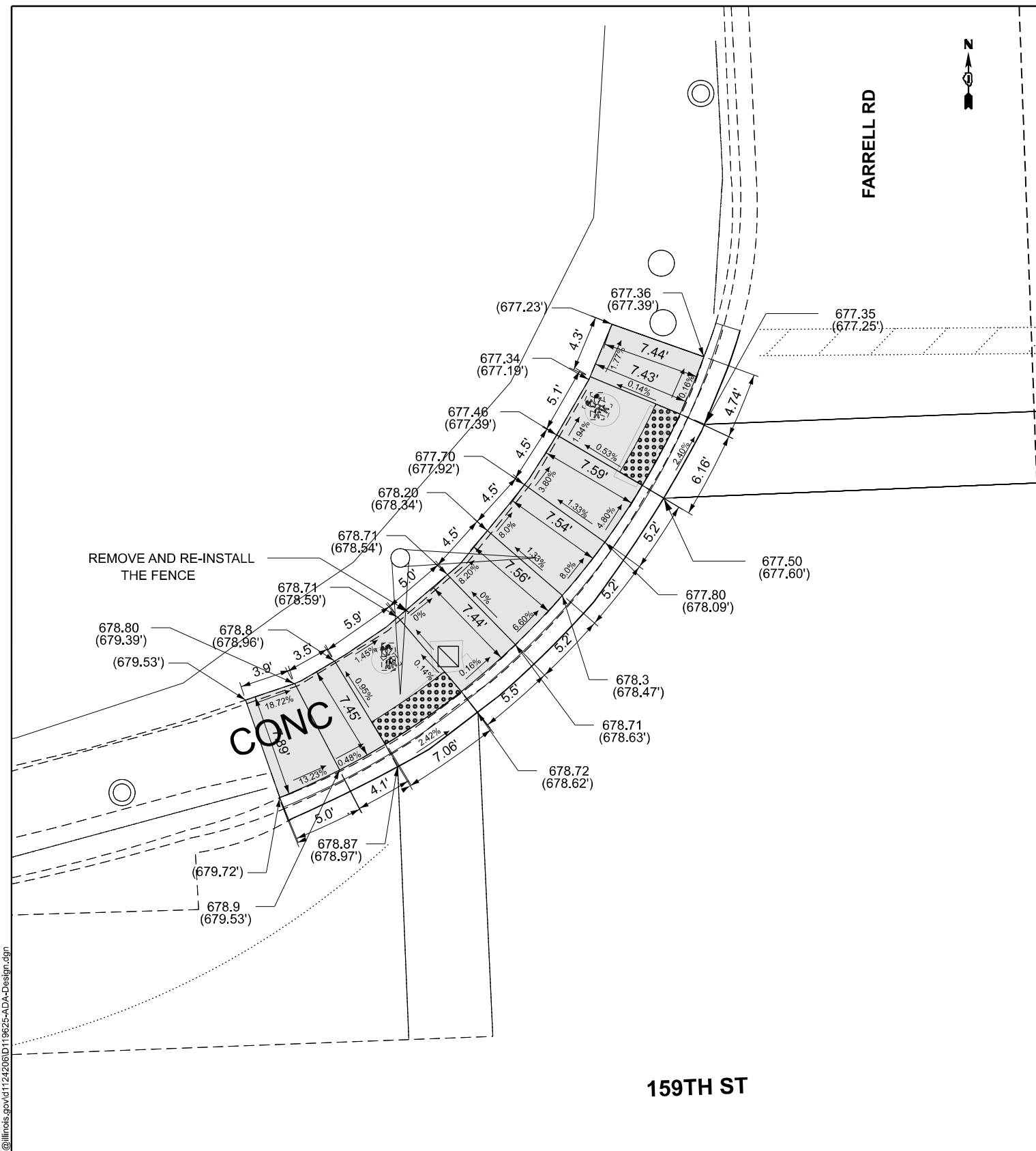
	USER NAME = merin,jose	DESIGNED -	REVISED -
		DRAWN -	REVISED -
		CHECKED -	REVISED -
	PLOT DATE = 11/20/2025	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLAN  
IL ROUTE 7 (159TH ST.) ROBSON DR. TO E OF ADELMANN DR.

SCALE: 1"=50' SHEET 2 OF 2 SHEETS STA. 40+00.00 TO STA. 70+00.00

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	31	8
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				



**FARRELL RD**

**159TH ST**

## LEGEND

XX.XX<sup>1</sup>

**EXISTING LENGTH**

===== PROPOSED SIDE CURB



## PROPOSED SIDEWALK

## DETECTABLE WARNINGS

USER NAME = merin.jose	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/16/2025	DATE -	REVISED -

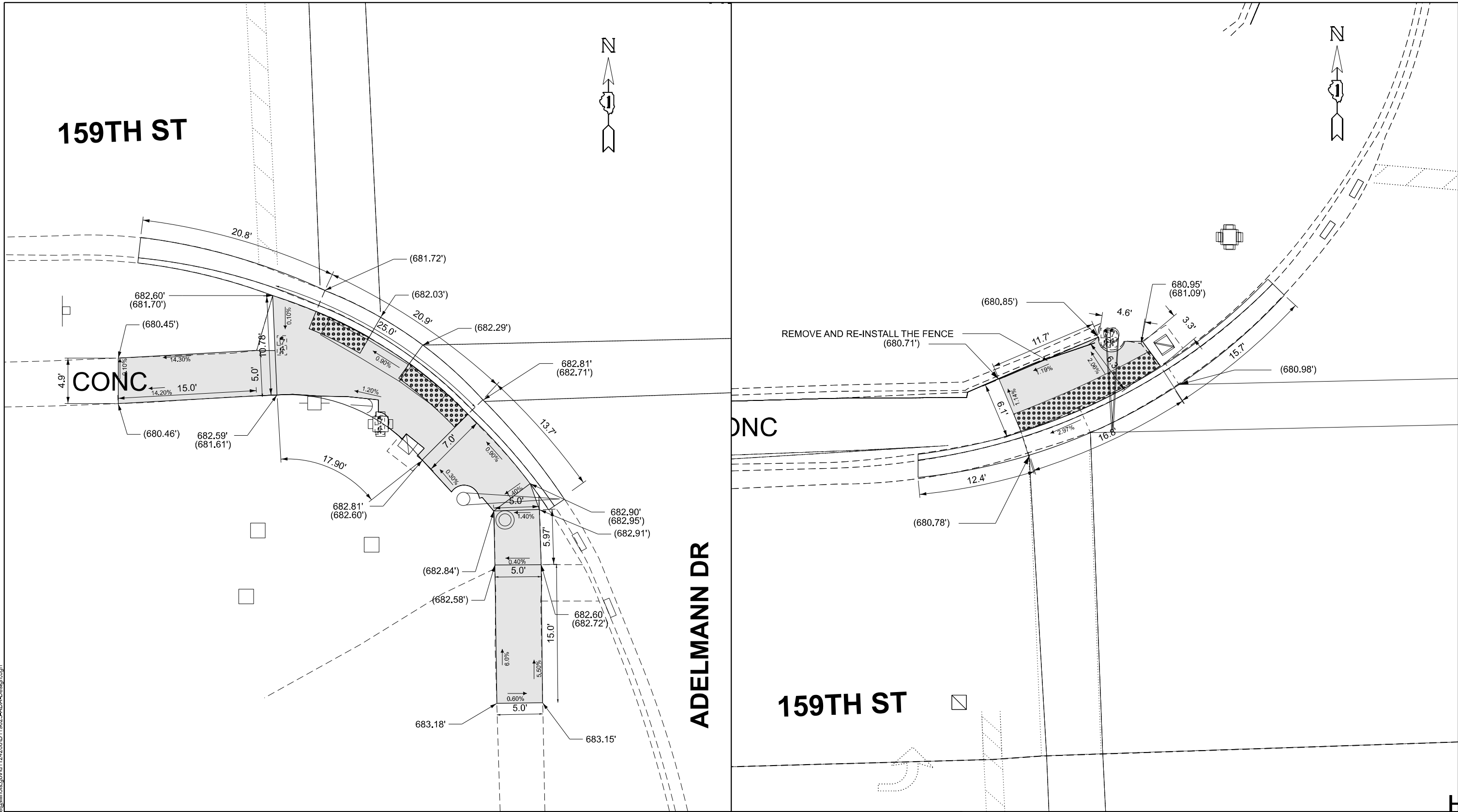
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN CURB RAMP DETAILS**  
**IL 7 (159TH ST.) AT FARREL RD.**

SCALE:	SHEET 1	OF 7	SHEETS	STA. 0+00.00	TO STA. 0+00.00
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	9
		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

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**LEGEND**

XX.XX'

EXISTING LENGTH

PROPOSED SIDE CURB



PROPOSED SIDEWALK



DETECTABLE WARNINGS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

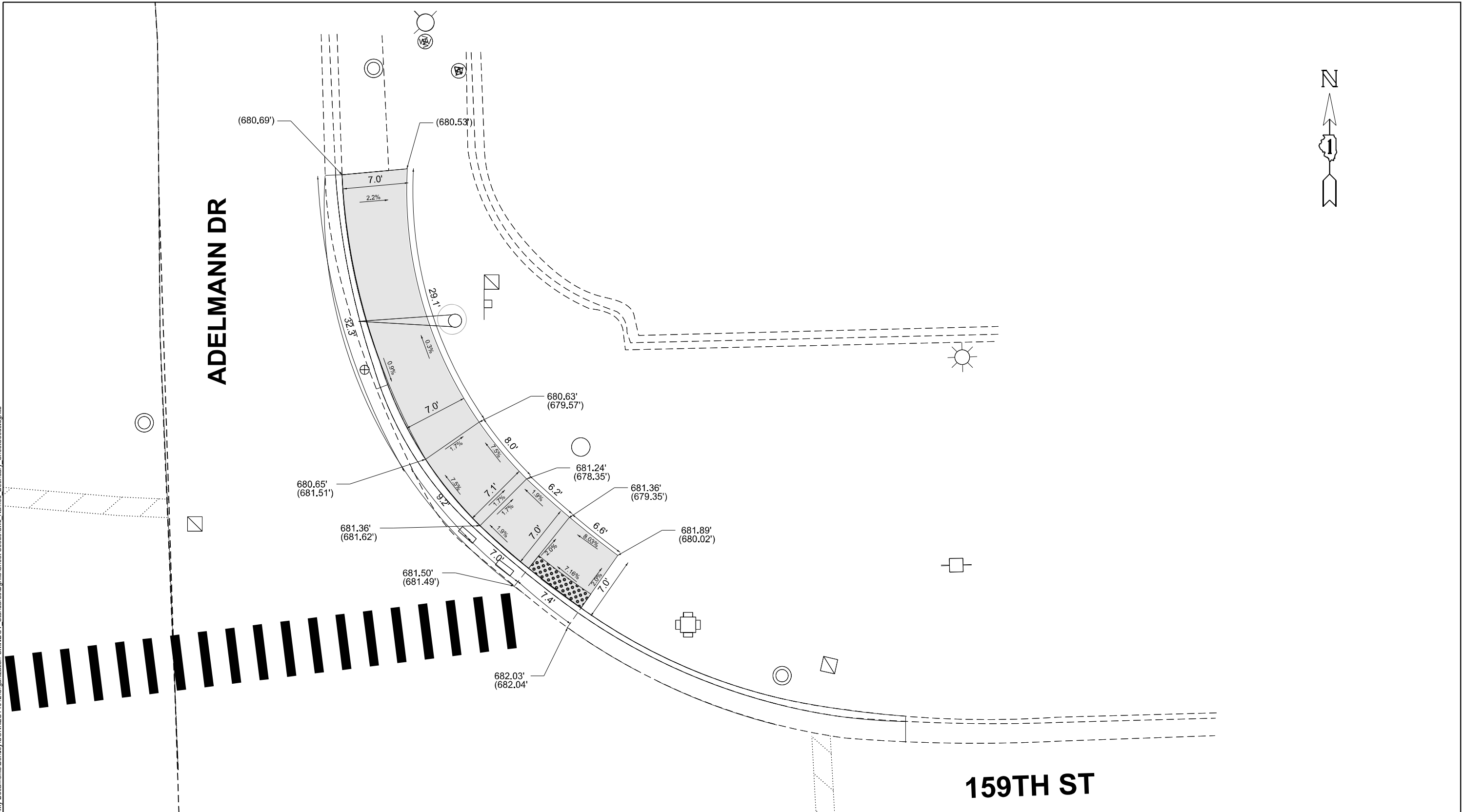
PEDESTRIAN CURB RAMP DETAILS  
IL 7(159TH ST.)AT ADELMANN DR.

SCALE: SHEET 2 OF 7 SHEETS STA. 0+00.00 TO STA. 0+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	10
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				



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**LEGEND**

XX.XX'

EXISTING LENGTH

PROPOSED SIDE CURB

( )

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN CURB RAMP DETAILS  
IL 7 (159TH ST. AT ADELMANN DR.)**

SCALE: \_\_\_\_\_ SHEET 000 OF \_ SHEETS STA. \_\_\_\_\_ TO STA. \_\_\_\_\_

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	010A
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

159TH ST



CONC

CONC

12"

LEGEND

xx.xx'

EXISTING LENGTH

PROPOSED SIDE CURB

( ) EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PEDESTRIAN CURB RAMP DETAILS  
IL 7 (159TH ST.) AT DRIVEWAY(E OF ADALMANN DR.)

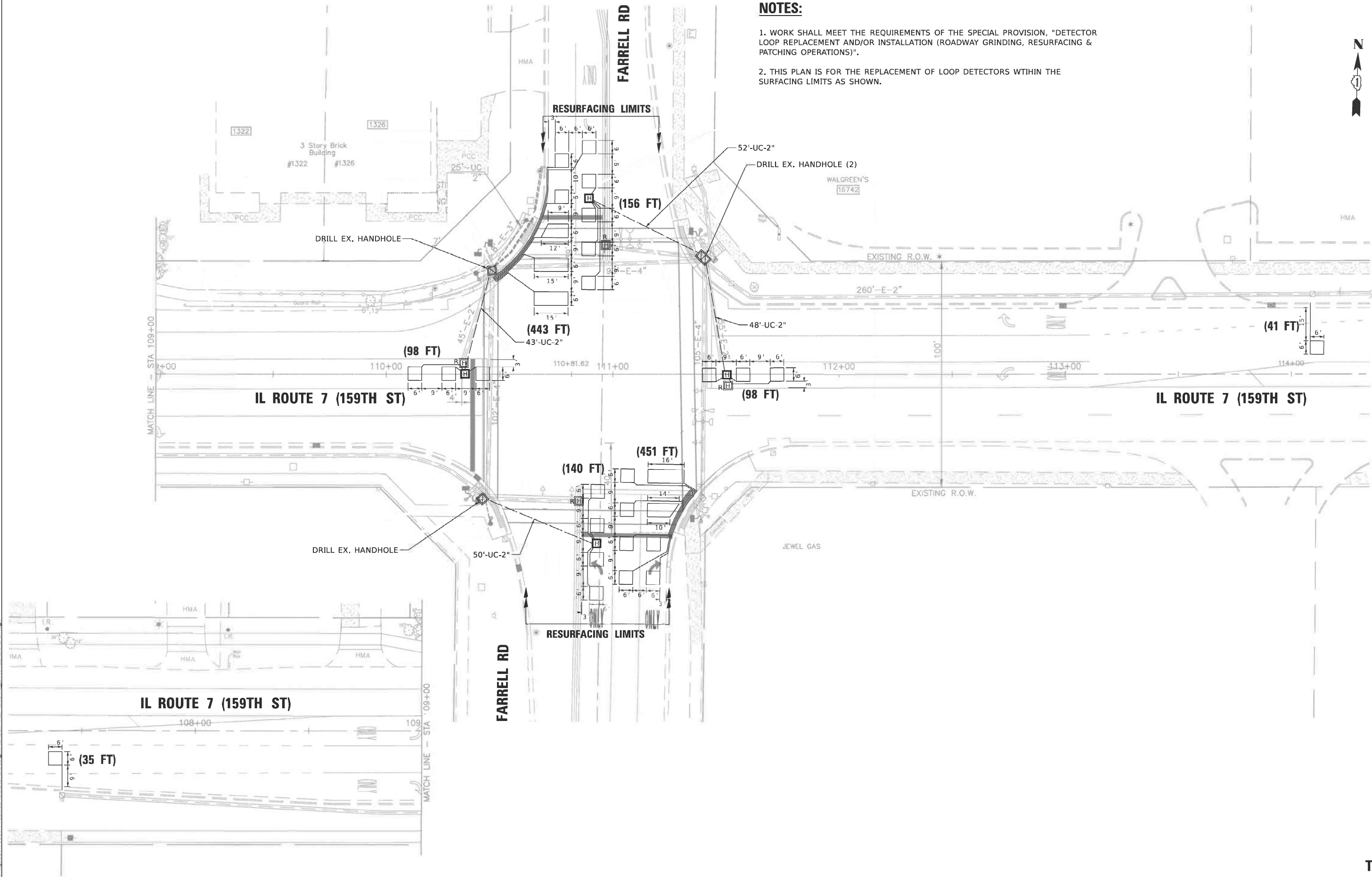
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

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USER NAME = merlin.jose	DESIGNED -	REVISED -
	DRAWN -	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/16/2025	DATE -	REVISED -

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FILE NAME: T:\Projects\17000\17180\TechData\CADD\Design\Plans\CI\FinalPlanCI13\_D117128-sh11-ts-01.dgn



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS)".
2. THIS PLAN IS FOR THE REPLACEMENT OF LOOP DETECTORS WITHIN THE SURFACING LIMITS AS SHOWN.



**TS 7526**

USER NAME	DESIGNED	KJS	REVISED	
	DRAWN	EL	REVISED	
	CHECKED	KJS	REVISED	
	DATE	11/05/2025	REVISED	

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

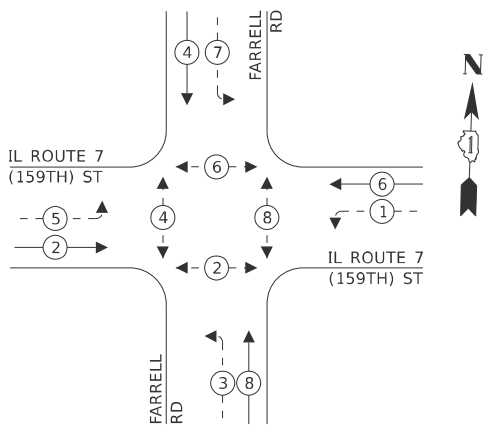
**TRAFFIC SIGNAL MODERNIZATION PLAN  
IL ROUTE 7 (159TH ST) AT FARRELL RD**

SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA. TO STA.

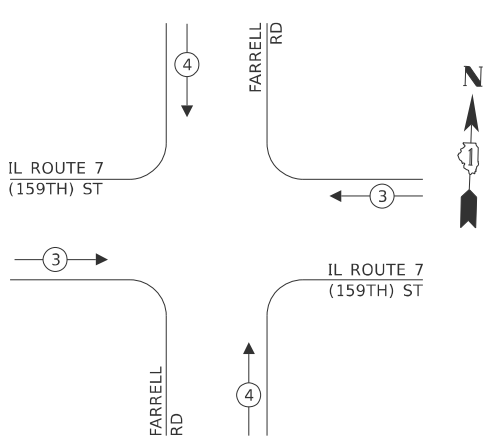
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351	2025-2004 RS	WILL	34	12
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

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EXISTING AND PRROPOSED  
CONTROLLER SEQUENCE



EXISTING AND PROPOSED EMERGENCY  
VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	4	11	44
4-SECTION	-	14	-
5-SECTION	8	13	104
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	8	15	120
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			443
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1,048

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPARTMENT  
PHONE: (886) 629-3532  
COMPANY: COMED  
ACCOUNT NUMBER: ---  
METER NUMBER: ---

CABLE PLAN  
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	193
HEAVY-DUTY HANDHOLE	EACH	4
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	619
DRILL EXISTING HANDHOLE	EACH	4
DETECTOR LOOP, TYPE I	FOOT	492
REMOVE EXISTING HANDHOLE	EACH	4
DETECTOR LOOP REPLACEMENT	FOOT	970

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

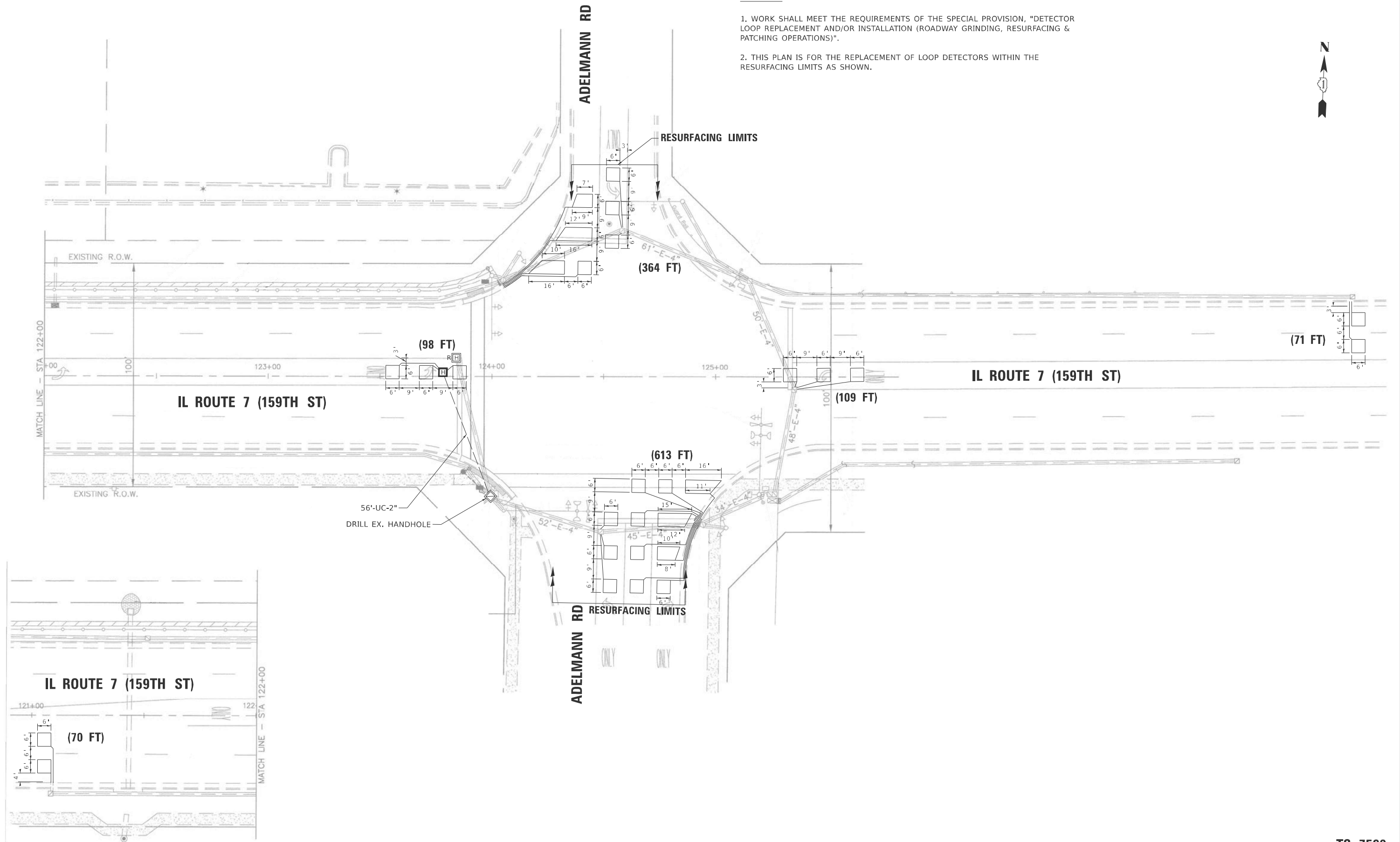
CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE  
PREEMPTION SEQUENCE, AND SCHEDULE OF QUANTITES  
IL ROUTE 7 (159TH ST) AND FARRELL RD

SCALE: N.T.S. SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	13
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

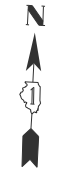
TS 7526

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**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS)".
2. THIS PLAN IS FOR THE REPLACEMENT OF LOOP DETECTORS WITHIN THE RESURFACING LIMITS AS SHOWN.



**TS 7529**

USER NAME =	DESIGNED -	KJS	REVISED -	
	DRAWN -	EL	REVISED -	
	CHECKED -	KJS	REVISED -	
	DATE -	11/05/2025	REVISED -	

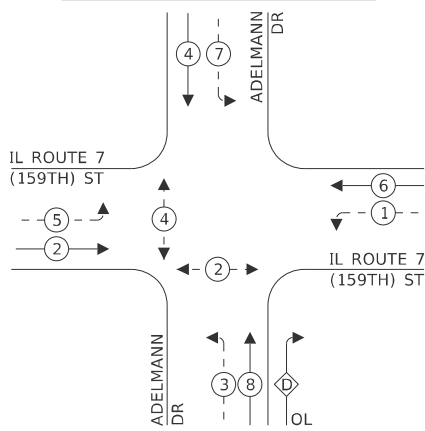
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION PLAN  
IL ROUTE 7 (159TH ST) AT ADELMANN DR**

SCALE: 1"=20'    SHEET 1    OF 2    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351		WILL	34	14
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

EXISTING AND PRROPOSED  
CONTROLLER SEQUENCE



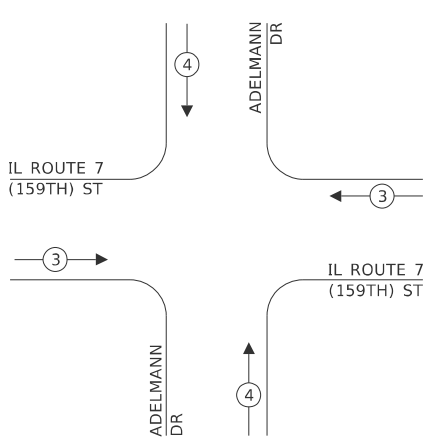
LEGEND:

- PROTECTED PHASE
- PROTECTED/PERMITTED PHASE
- PEDESTRIAN PHASE
- OVERLAP

RIGHT TURN OVERLAP  
PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
D	= 8	+ 1

EXISTING AND PROPOSED EMERGENCY  
VEHICLE PREEMPTION SEQUENCE



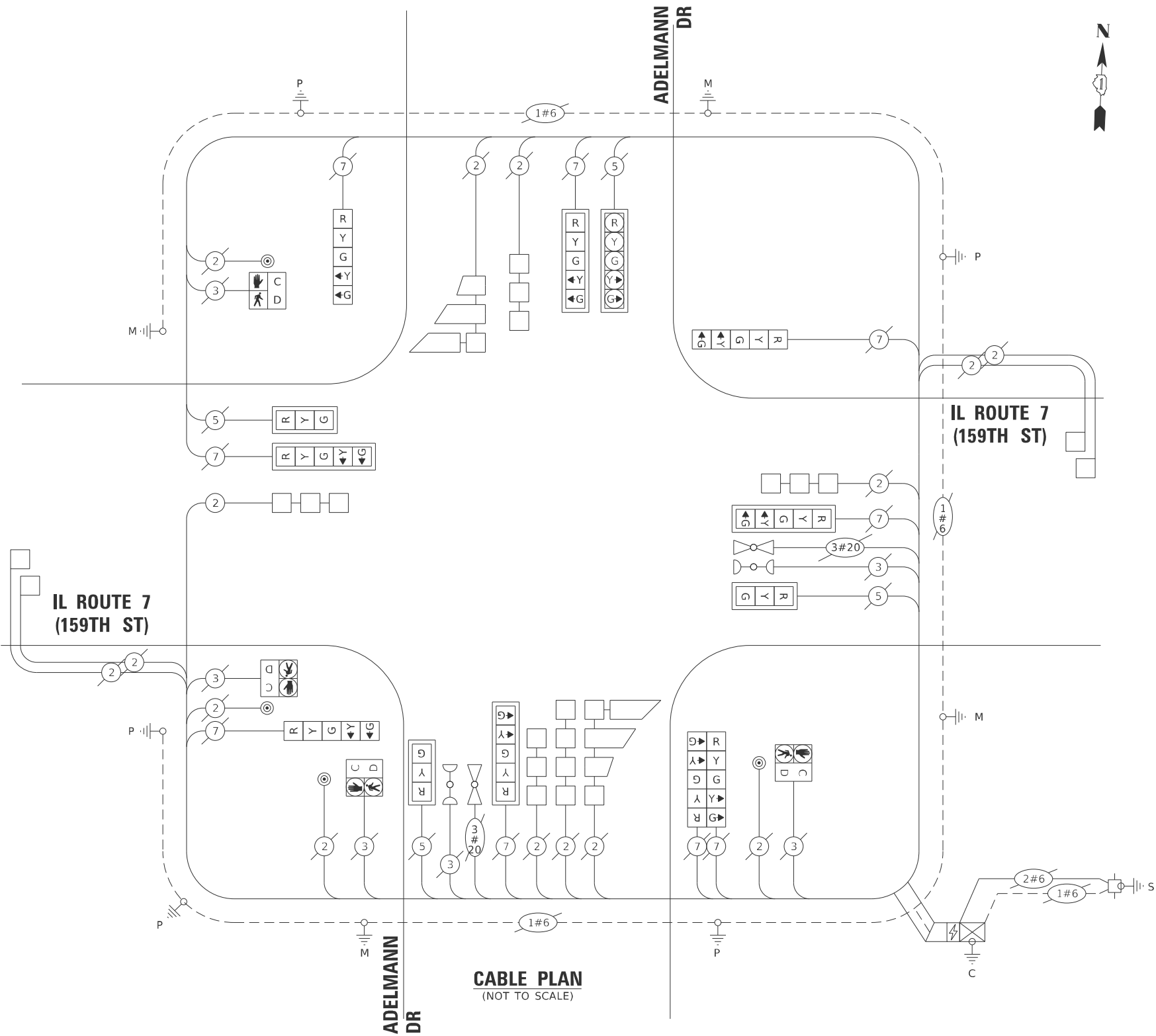
TRAFFIC SIGNAL  
ELECTRICAL SERVICE REQUIREMENTS

EQUIPMENT TYPE	QUANTITY	UNIT WATTAGE	TOTAL WATTAGE
SIGNAL HEAD 1 OR 3-SECTION	3	11	33
4-SECTION	-	14	-
5-SECTION	10	13	130
PROGRAMMABLE 3-SECTION	-	22	-
4-SECTION	-	32	-
5-SECTION	-	28	-
PEDESTRIAN SIGNAL	4	15	60
CONTROLLER	1	150	150
MASTER CONTROLLER	-	100	-
UPS	1	25	25
DETECTION RADAR OR VIDEO	-	20	-
BLANK-OUT SIGN	-	25	-
NETWORK SWITCH II OR III	-	35	-
CELLULAR MODEM	-	15	-
TOTAL UPS SIZING			398
UPS CHARGING	1	225	225
BATTERY HEATER MAT	1	180	180
CABINET HEATER	1	200	200
FLASHER	-	15	-
LED STREET NAME SIGN	-	120	-
LUMINAIRE	-	240	-
TOTAL SERVICE WIRE SIZING			1,003

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION  
201 WEST CENTER COURT  
SCHAUMBURG, ILLINOIS 60196

ENERGY SUPPLY: CONTACT: NEW BUSINESS DEPARTMENT  
PHONE: (866) 639-3532  
COMPANY: COMED  
ACCOUNT NUMBER: ---  
METER NUMBER: ---



SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	56
HEAVY-DUTY HANDHOLE	EACH	1
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	229
DRILL EXISTING HANDHOLE	EACH	1
DETECTOR LOOP, TYPE I	FOOT	98
REMOVE EXISTING HANDHOLE	EACH	1
DETECTOR LOOP REPLACEMENT	FOOT	1227

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE  
PREEMPTION SEQUENCE, AND SCHEDULE OF QUANTITIES  
IL ROUTE 7 (159TH ST) AND ADELMANN DR

SCALE: N.T.S. SHEET 2 OF 2 SHEETS STA. TO STA.

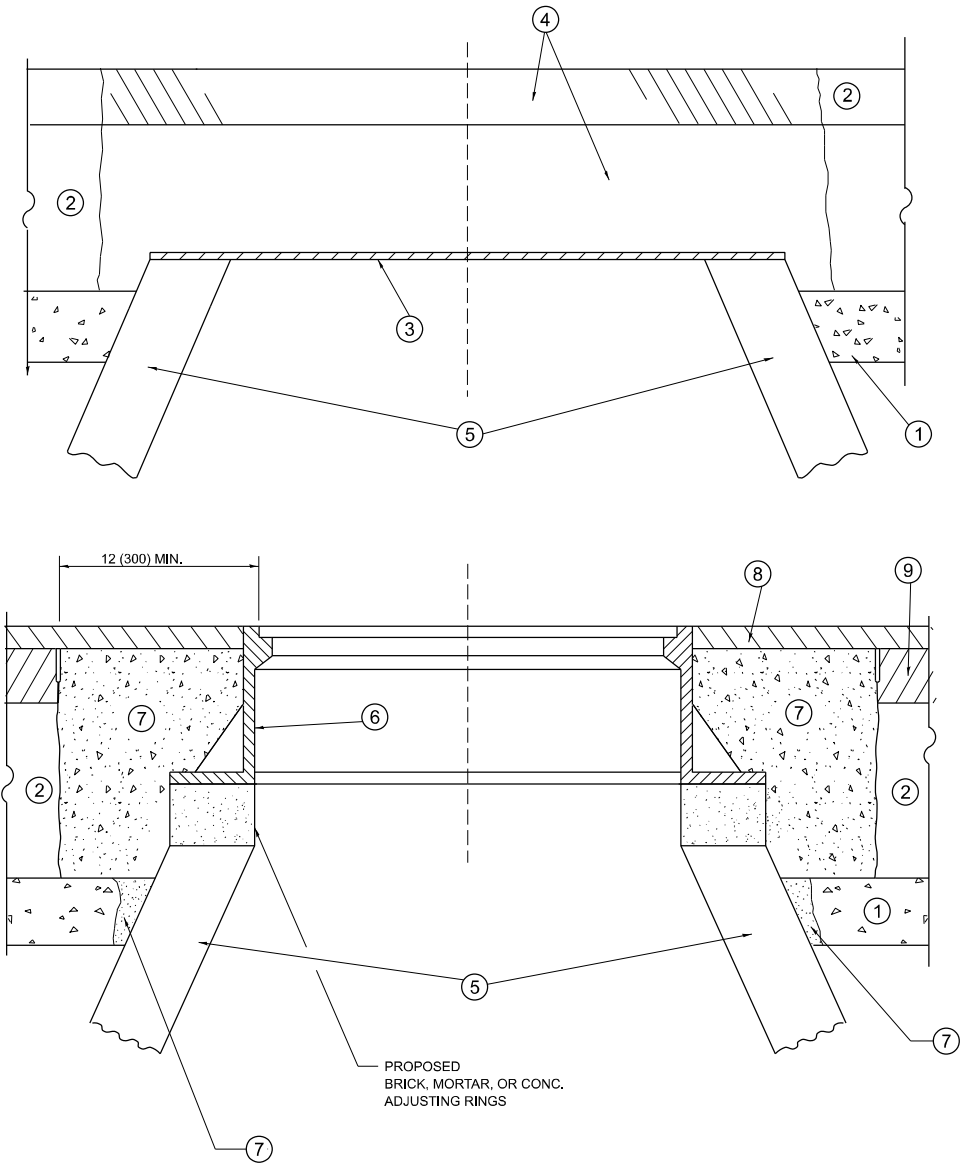
TS 7529

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	15
CONTRACT NO. 80B34				
ILLINOIS FED. AID PROJECT				

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**DETAILS FOR FRAMES AND LIDS ADJUSTMENT  
WITH MILLING**

**NOTES**

1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
4. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
5. THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

**CONSTRUCTION PROCEDURES**

- STAGE 1** (BEFORE PAVEMENT MILLING)
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

- STAGE 2** (AFTER PAVEMENT MILLING)
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- |  |                               |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL                 | ⑥ FRAME AND LID (SEE NOTES)   |
| ② EXISTING PAVEMENT                          | ⑦ CLASS PP-2* CONCRETE        |
| ③ 36 (900) DIAMETER METAL PLATE              | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE  |
| ⑤ EXISTING STRUCTURE                         |                               |

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

1. REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
2. THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
3. NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
4. WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

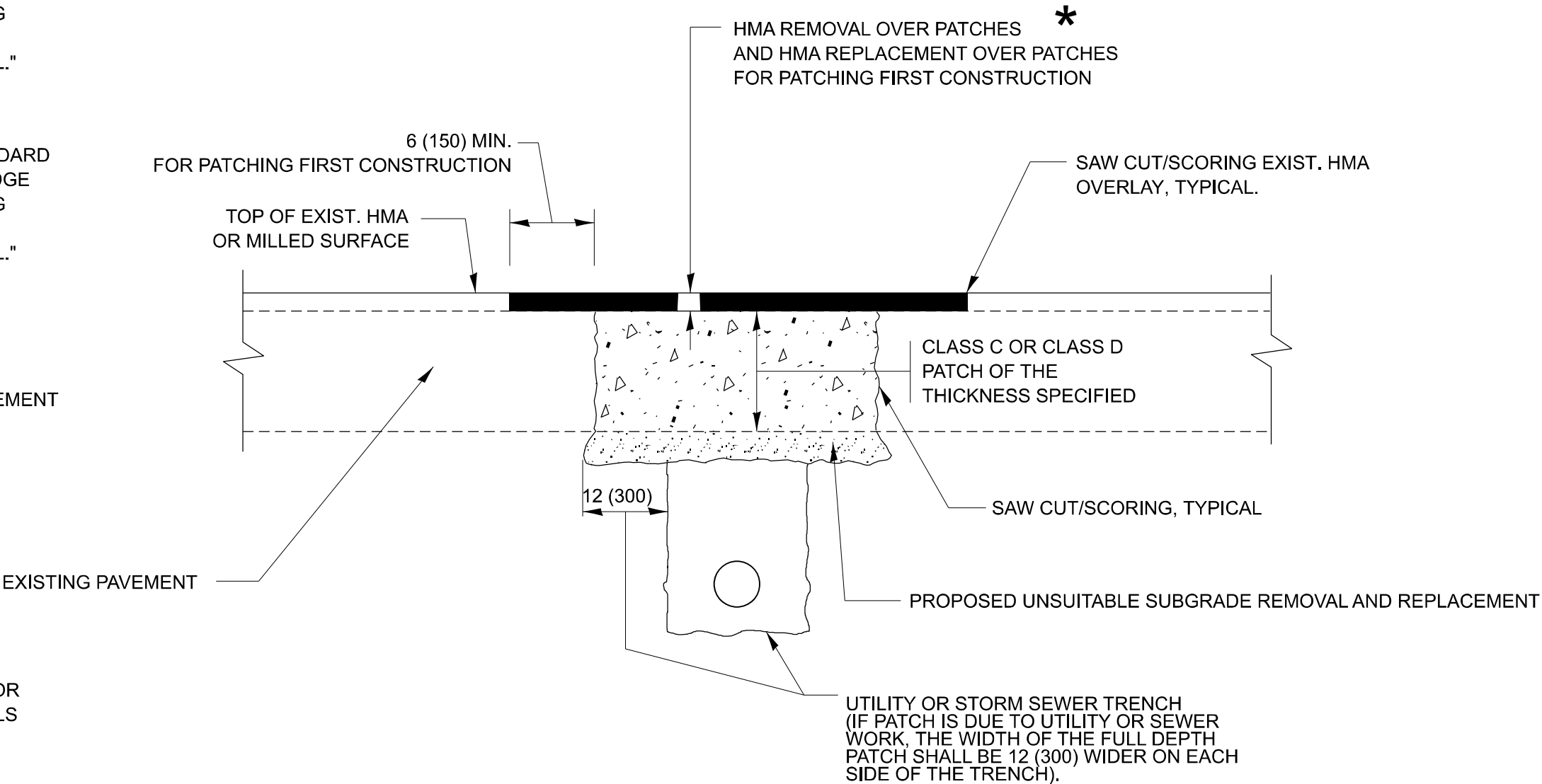
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		DRAWN -	REVISED - R. BORO 12-06-11					351	2025-2004 RS	WILL	34	16
		CHECKED -	REVISED - K. SMITH 11-18-22		BD600-03 (BD-08)			CONTRACT NO. 80B34				
	PLOT DATE = 10/16/2025	DATE - 10-25-94	REVISED - K. SMITH 09-15-23		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS   FED. AID PROJECT		

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

- 1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
- 2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
- 3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

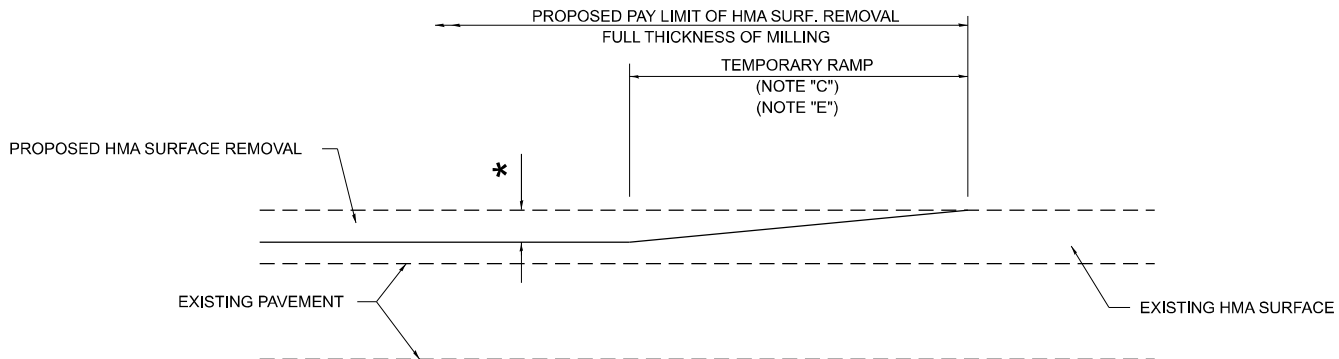
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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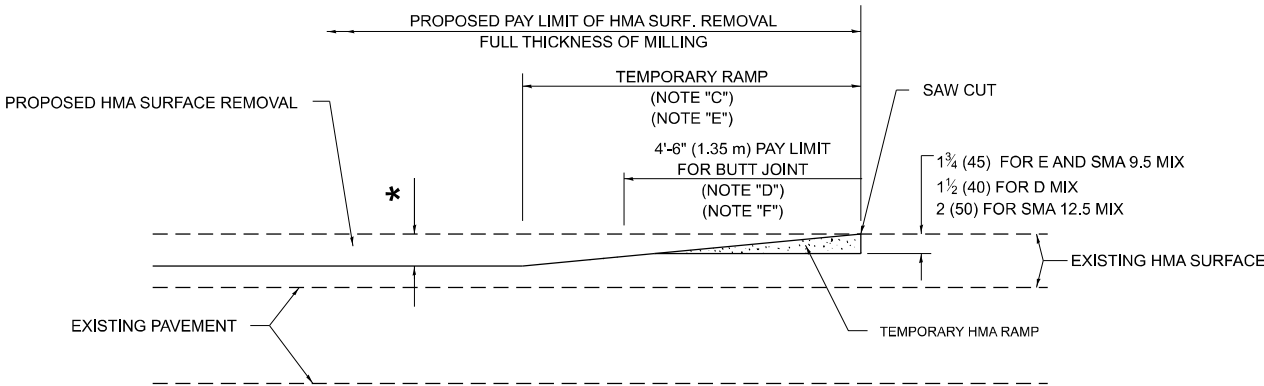
	USER NAME = merin,jose	DESIGNED - R. SHAH	REVISED - R. BORO 01-01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. BORO 09-04-07						351	2025-2004 RS	WILL	34	17	
		CHECKED -	REVISED - K. ENG 10-27-08						BD400-04 (BD-22)					CONTRACT NO. 80B34
	PLOT DATE = 10/16/2025	DATE - 10-25-94	REVISED - K. SMITH 11-18-22		SCALE: NONE		SHEET 1	OF 1	SHEETS	STA.	TO STA.			
							ILLINOIS FED. AID PROJECT							



**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

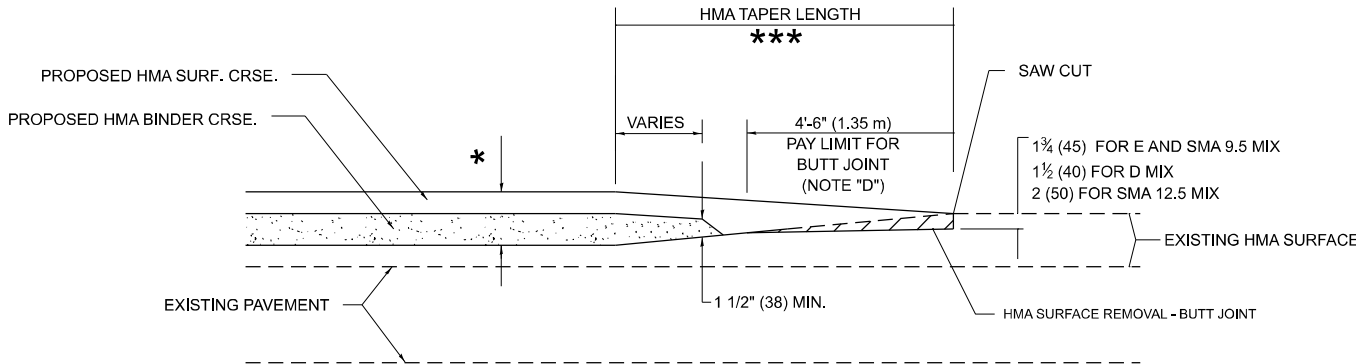


**HMA CONSTRUCTED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

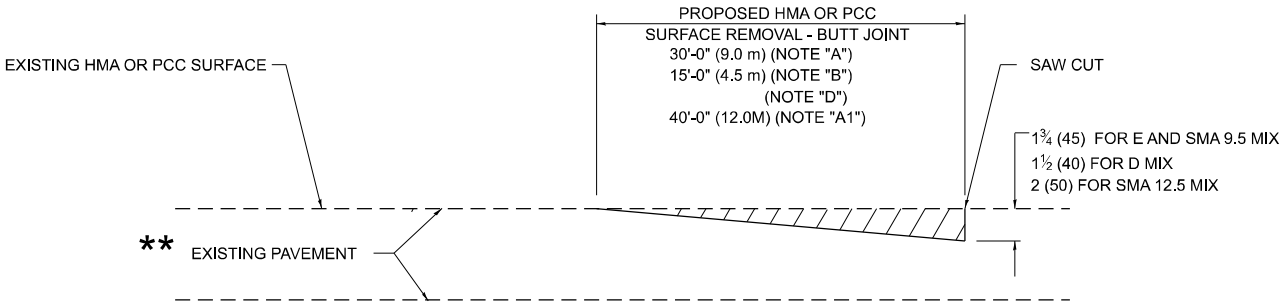
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

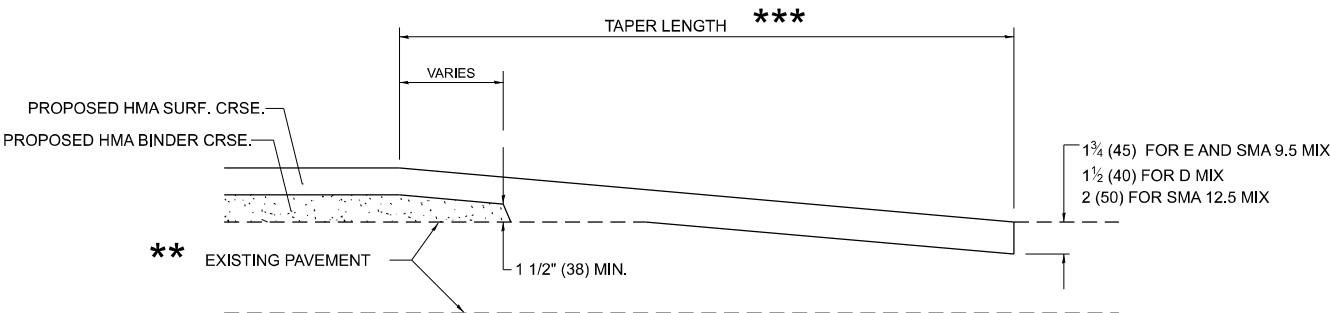


**BUTT JOINT AND  
HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY**

\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\*  
20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: BD-32 [Sheet]  
FILE NAME: c:\pav\_work\pav\illinois.gov\112420\BD119625-shr-DistStd.sgn

USER NAME = merin,jose	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 10/16/2025	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

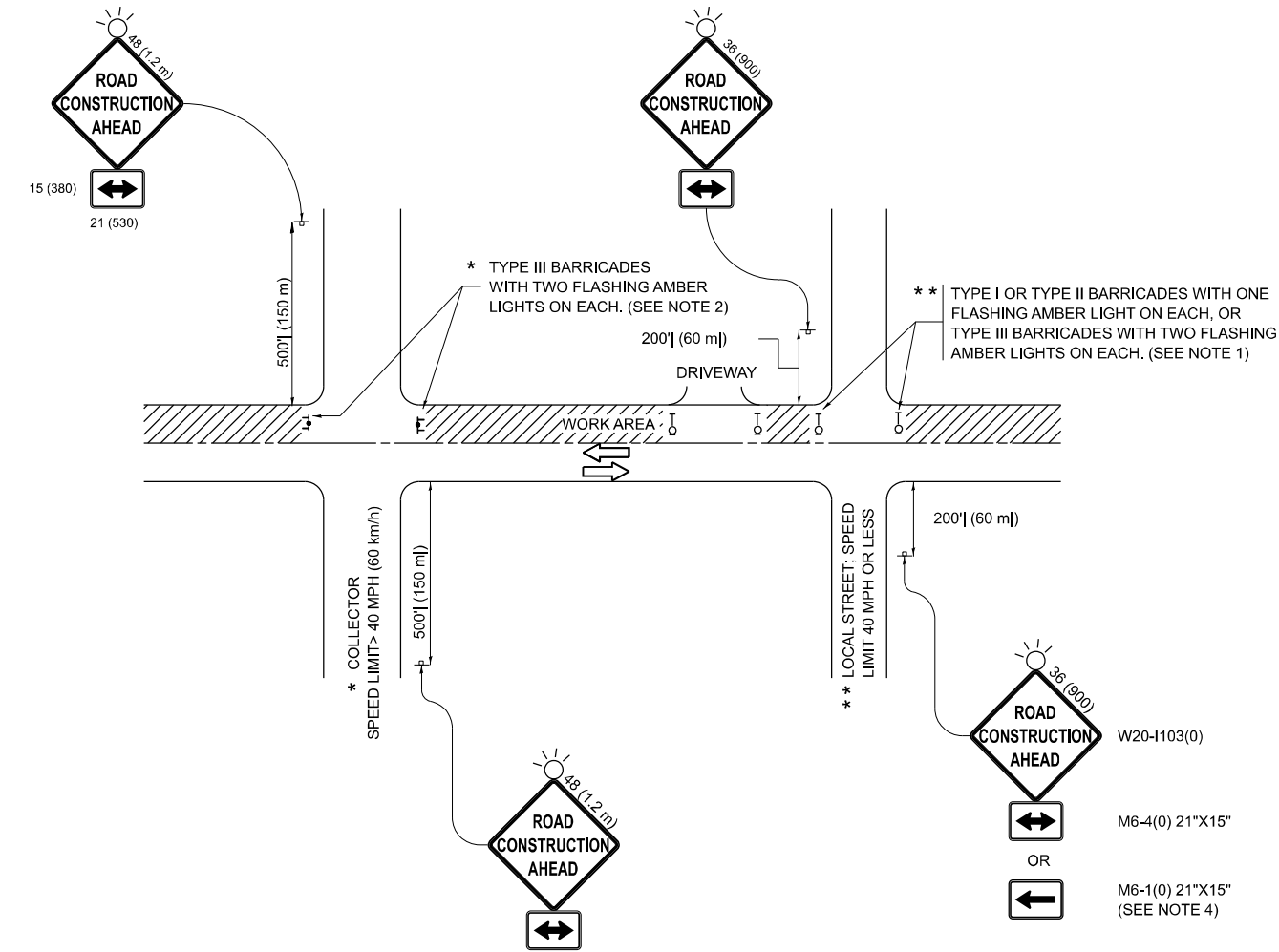
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	18
BD400-05 BD-32		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

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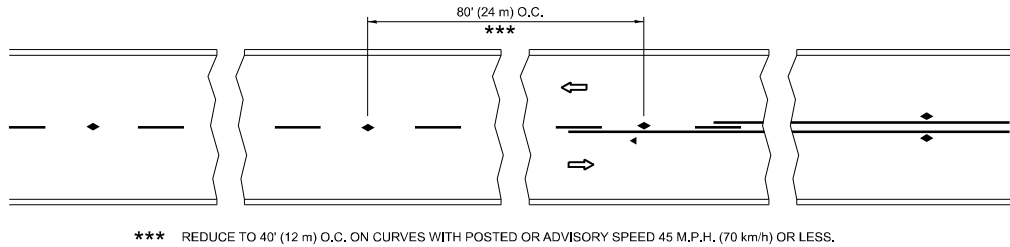
NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

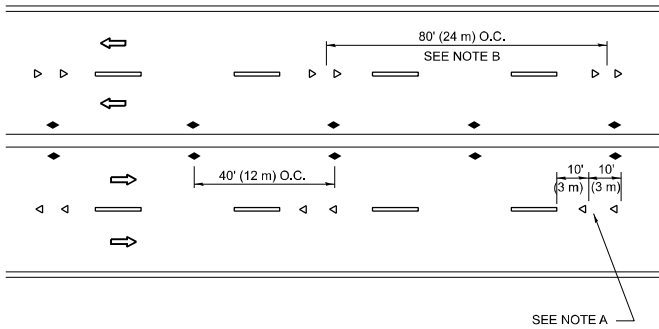
All dimensions are in inches (millimeters)  
unless otherwise shown.

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		CHECKED -	REVISED - A. SCHUETZE 09-15-06									TC-10		CONTRACT NO. 80B34		
	PLOT DATE = 10/16/2025	DATE - 06-89	REVISED - D. SENDERAK 05-03-24									ILLINOIS FED. AID PROJECT				

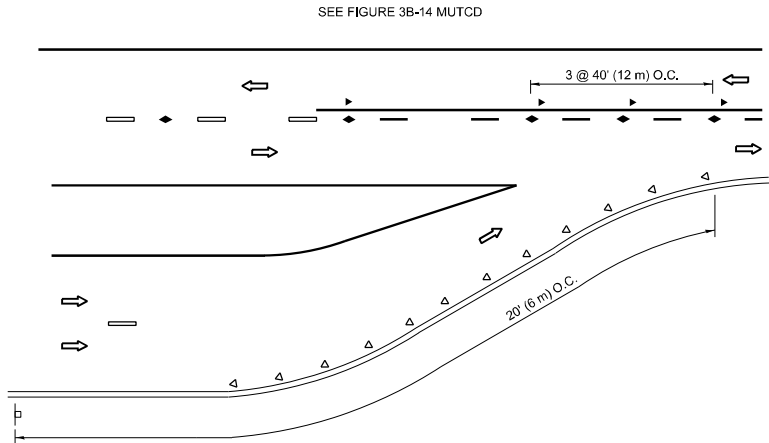
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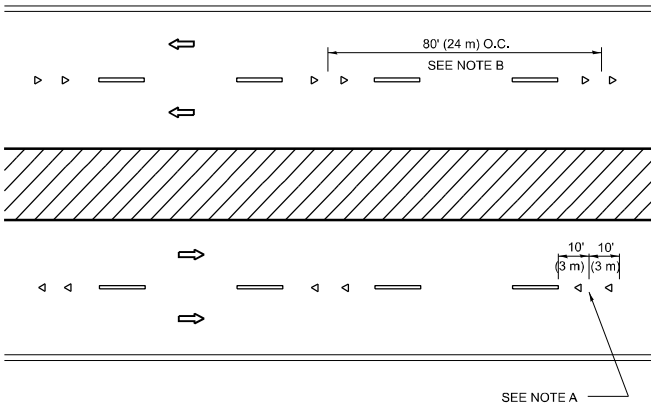
## TWO-LANE/TWO-WAY



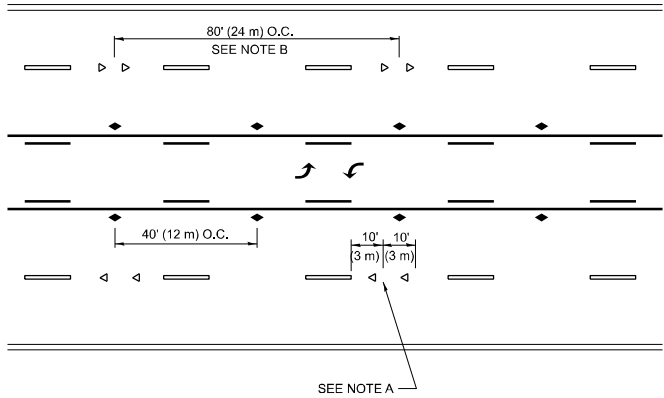
## MULTI-LANE/UNDIVIDED



## LANE REDUCTION TRANSITION



## MULTI-LANE/DIVIDED



## TWO-WAY LEFT TURN

### GENERAL NOTES

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

### LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

### SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

### DESIGN NOTES

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

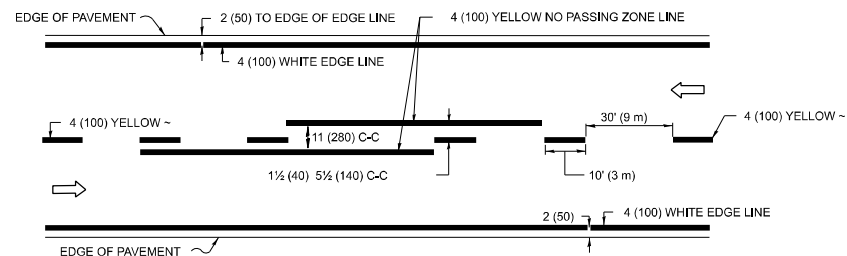
All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = merin,jose	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 10/16/2025	DATE -	REVISED - C. JUCIUS 07-01-13

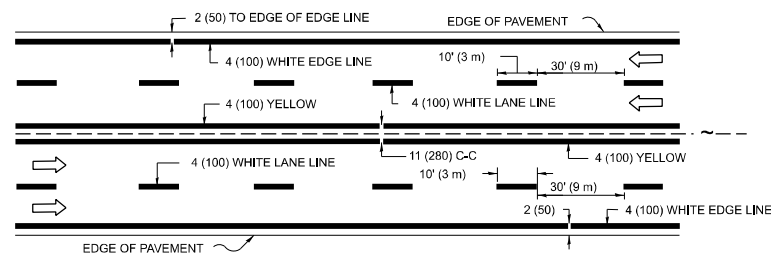
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

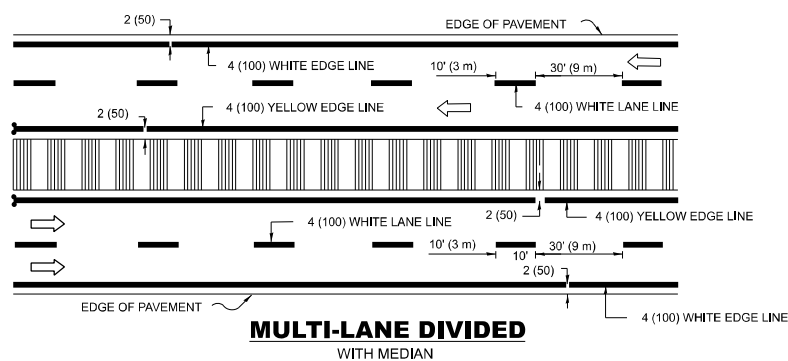
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		



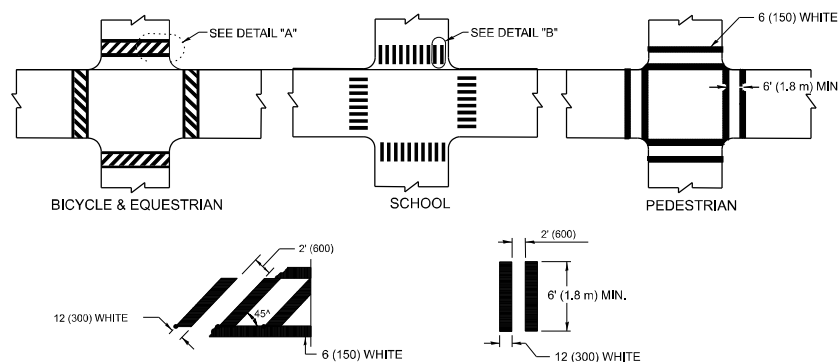
## 2-LANE ROADWAY



## MULTI-LANE UNDIVIDED



## TYPICAL LANE AND EDGE LINE MARKING

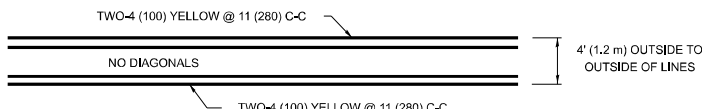


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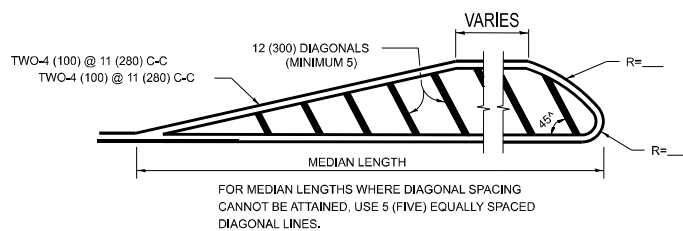
### DETAIL "B"

## TYPICAL CROSSWALK MARKING

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

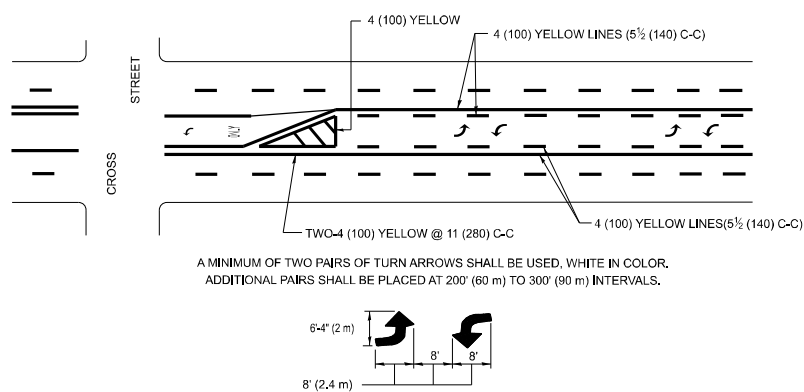


**4' (1.2 m) WIDE MEDIANS ONLY**



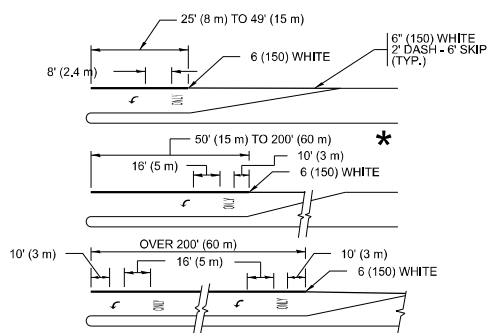
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**



### MEDIAN WITH TWO-WAY LEFT TURN LANE

## TYPICAL PAINTED MEDIAN MARKING

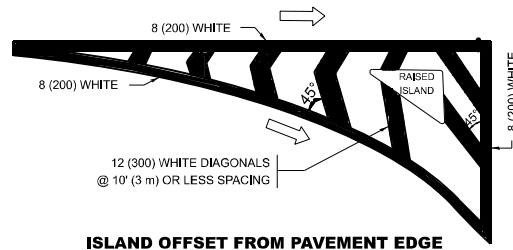


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.

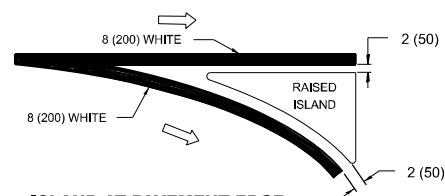
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

### TYPICAL LEFT (OR RIGHT) TURN LANE

## TYPICAL TURN LANE MARKING

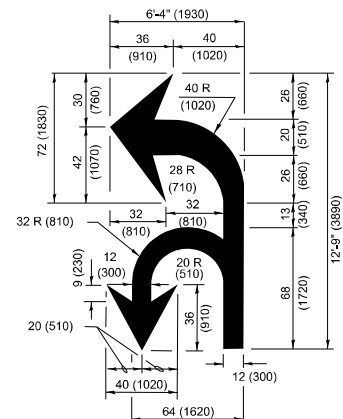


## ISLAND OFFSET FROM PAVEMENT EDGE

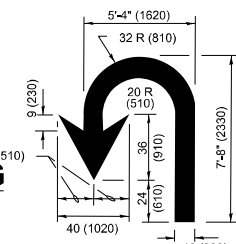


### ISLAND AT PAVEMENT EDGE

## 20 (5) TYPICAL ISLAND MARKING



## COMBINATION LEFT AND U-TURN



## U-TURN

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55



## LANE REDUCTION TRANSITION

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION  8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW  WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8' )	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO  
STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE  
CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = merin.jose	DESIGNED = EVERS	REVISED = C. JUCIUS 09-09-09
	DRAWN =	REVISED = C. JUCIUS 07-01-13
	CHECKED =	REVISED = C. JUCIUS 12-21-15
PLOT DATE = 10/16/2025	DATE = 03-19-90	REVISED = C. JUCIUS 04-12-16

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>				
<b>TYPICAL PAVEMENT MARKINGS</b>				
SCALE: NONE	SHEET 1	OF 1	SHEETS	STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	21
<b>TC-13</b>		CONTRACT NO. 80B34		
		ILLINOIS	FED. AID PROJECT	

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TURN BAY ENTRANCE AT START  
OF LANE CLOSURE TAPER

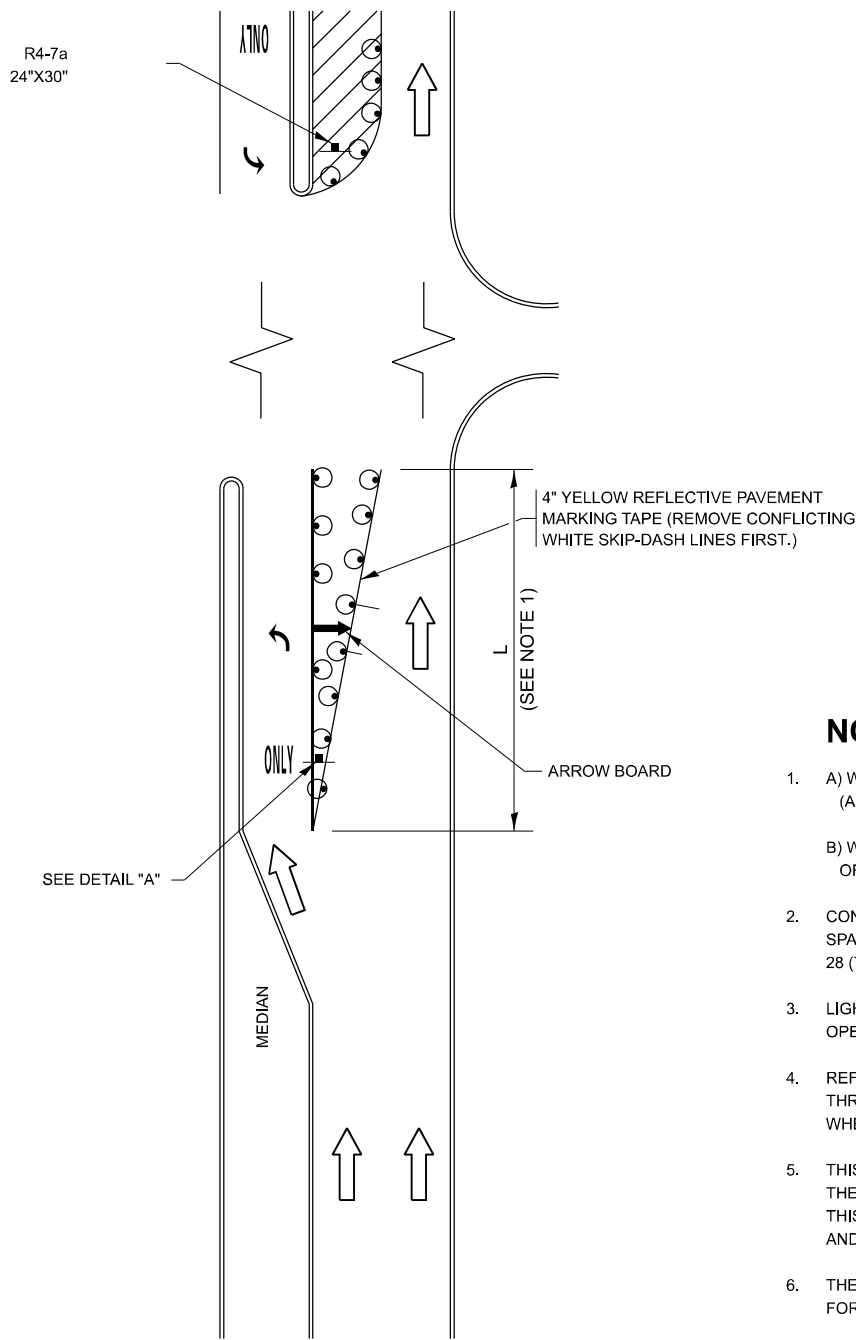
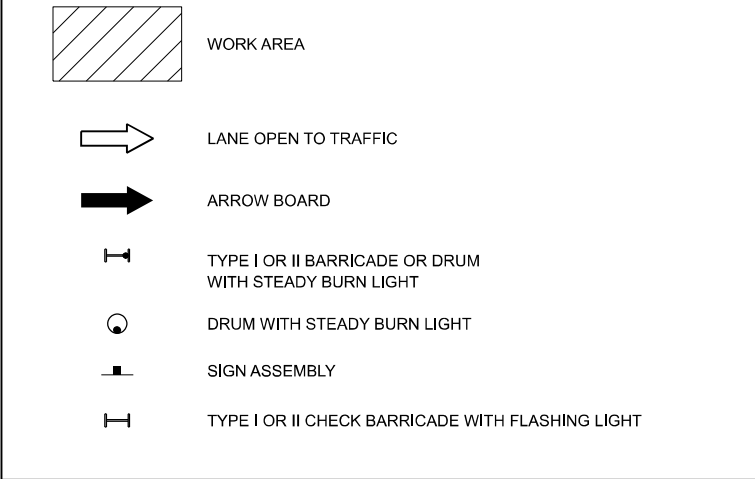


FIGURE 1

LEGEND



NOTES:

- A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
- REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
- THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
- THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
- THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
- TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE  
WITHIN A LANE CLOSURE

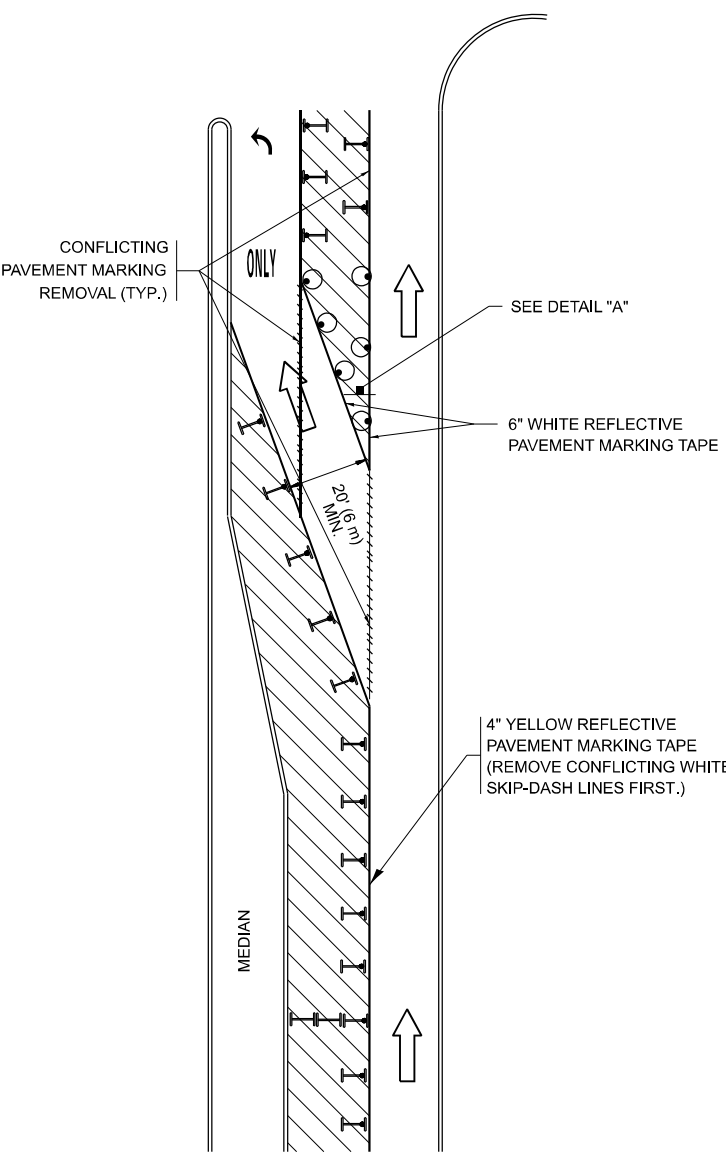
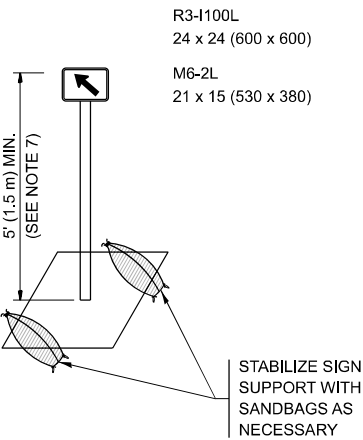


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters)  
unless otherwise shown.

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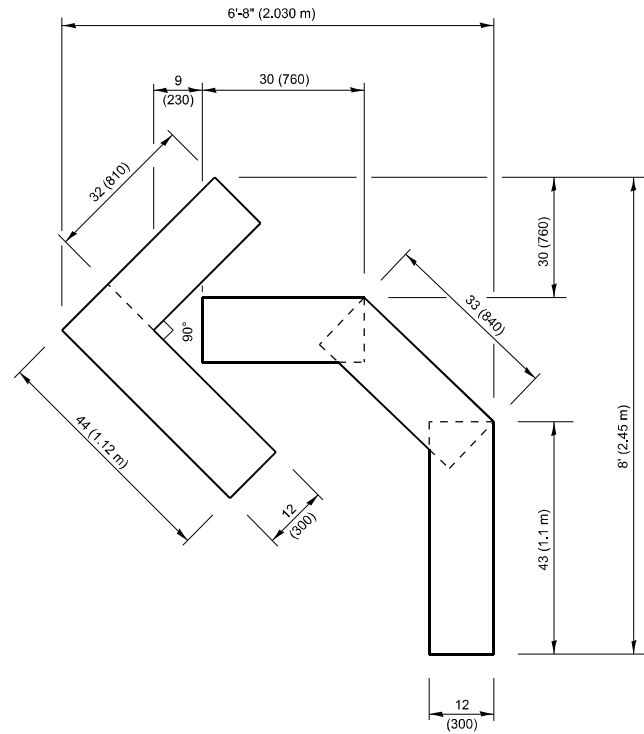
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 10/16/2025	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)

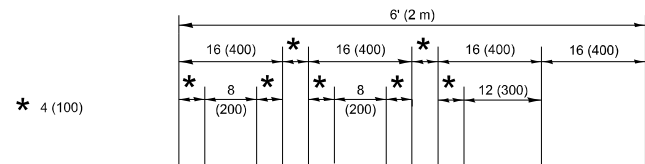
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	22
TC-14		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		



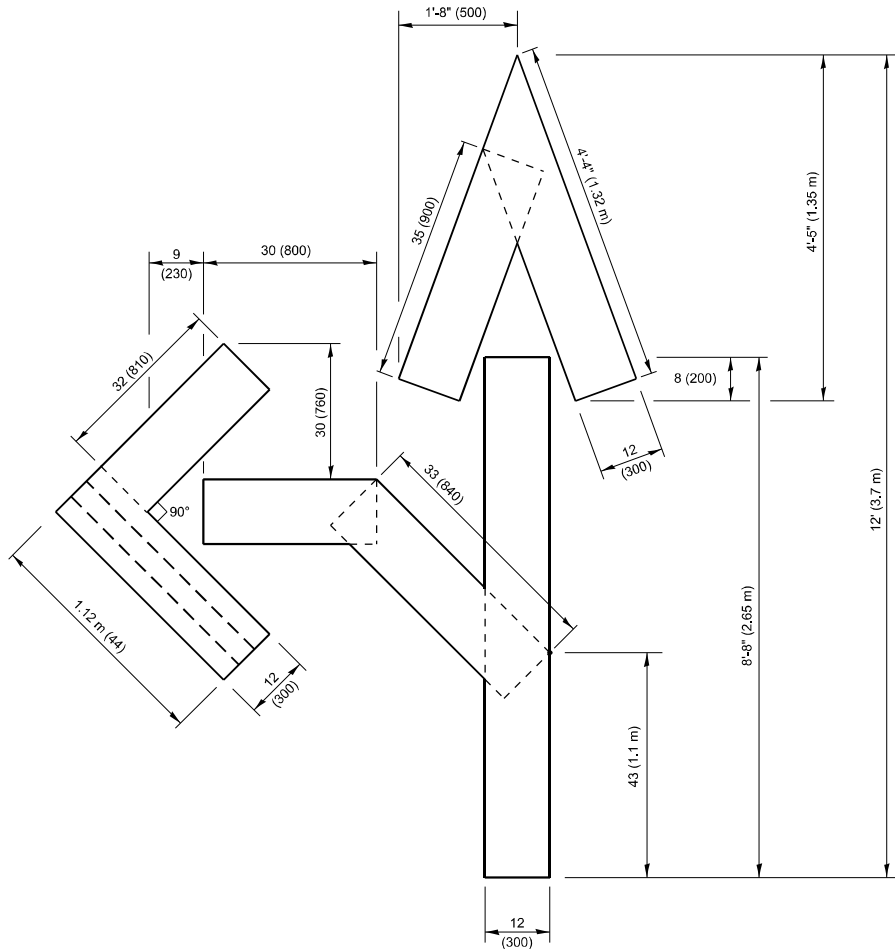
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

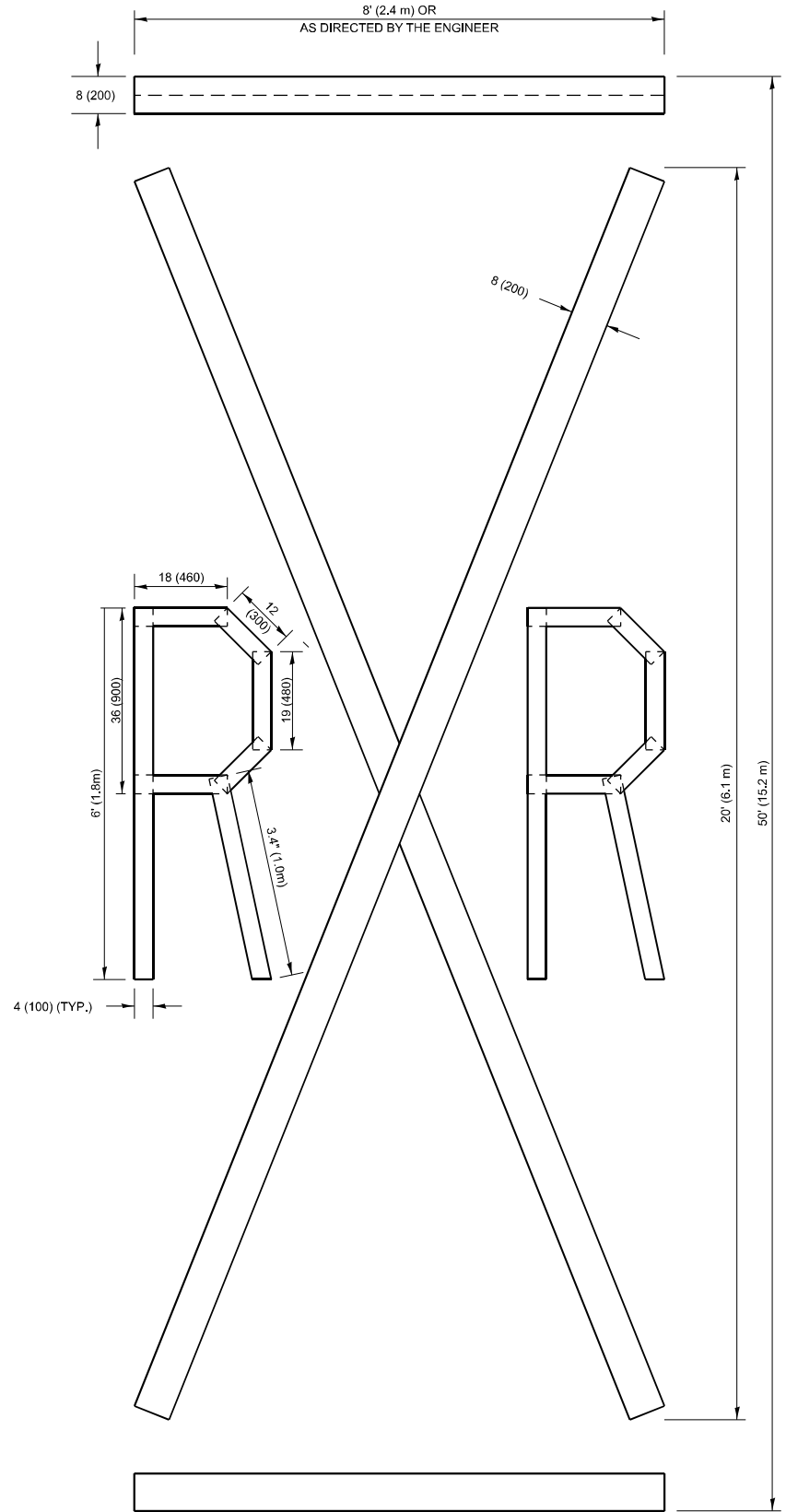


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED  
IN LINEAR FEET OF 4" LINES TO MATCH THE  
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS  
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)  
unless otherwise shown.

MODEL: TC-16 (Sheet)  
FILE NAME: c:\pav\_work\pav\illinois.gov\merin.jose\illinois.gov\1124205D119625-shl-DistStds.dgn

USER NAME = merin.jose	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 10/16/2025	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

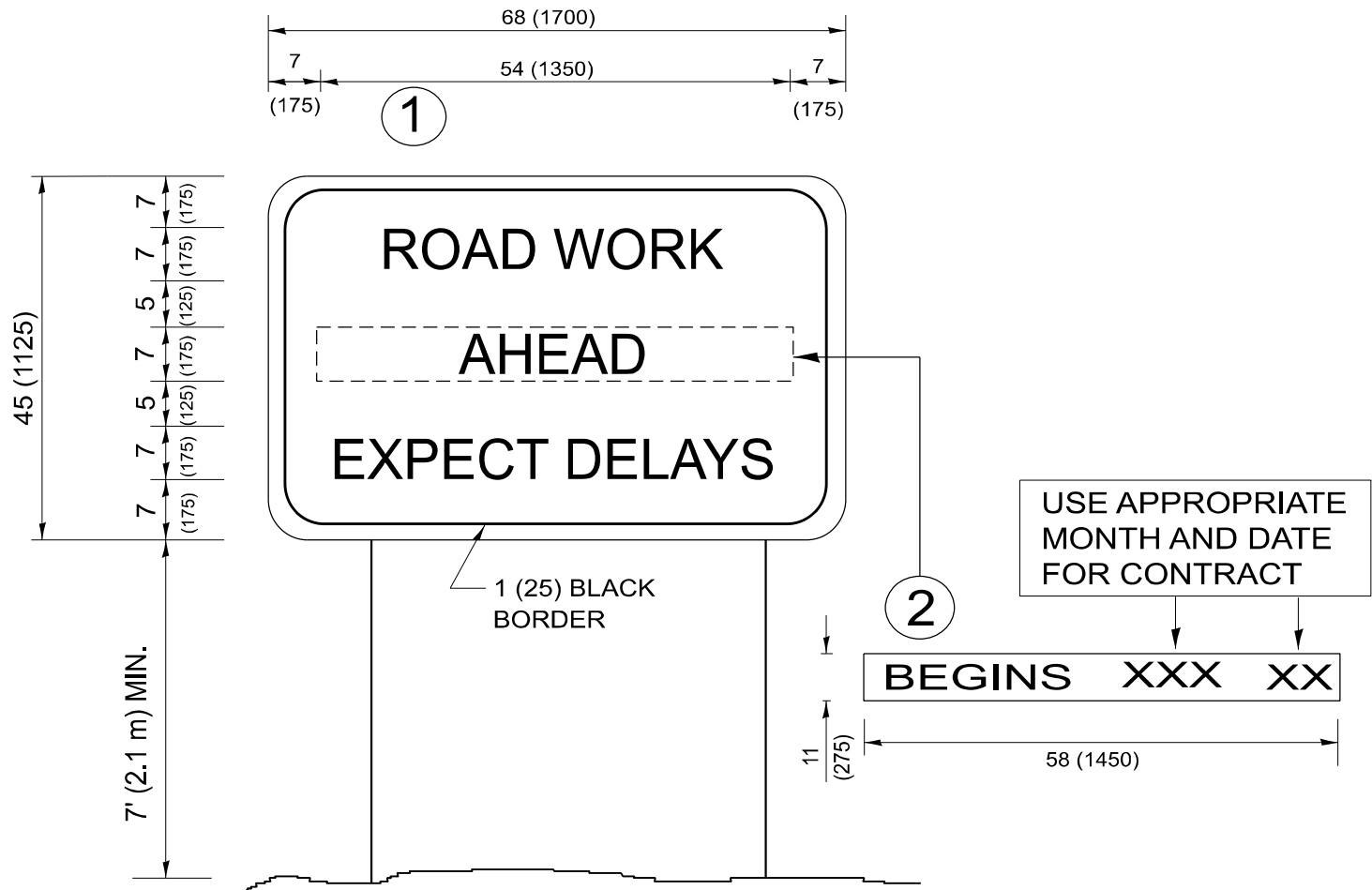
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	23
TC-16		CONTRACT NO. 80B34		
		ILLINOIS	FED. AID PROJECT	

MODEL: TC-22 [Sheet]  
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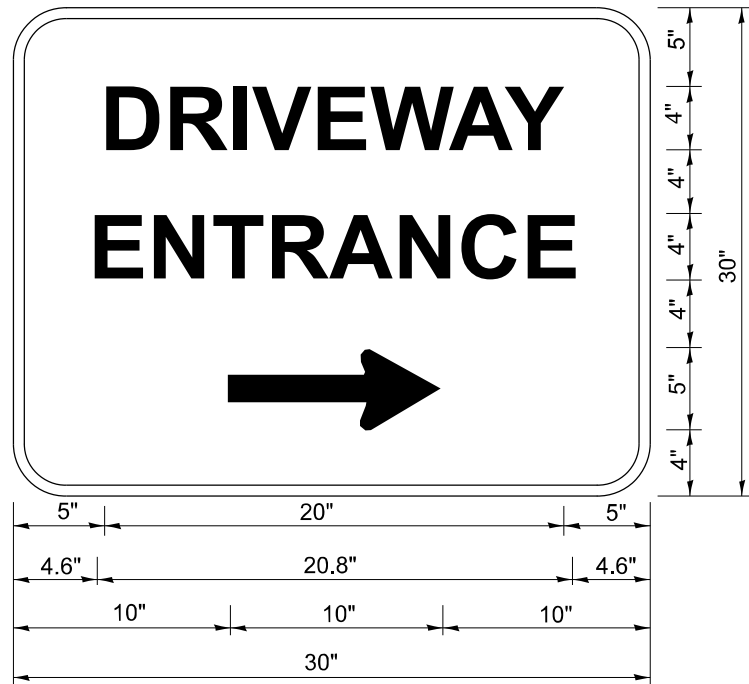


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)  
UNLESS OTHERWISE SHOWN.

	USER NAME   = merin,jose	DESIGNED   -	REVISED   -   R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
		DRAWN    -	REVISED   -   R. MIRS 12-11-97						351	2025-2004 RS	WILL	34	24		
		CHECKED   -	REVISED   - T. RAMMACHER 02-02-99						TC-22					CONTRACT NO. 80B34	
	PLOT DATE   = 10/16/2025	DATE       -	REVISED   -   C. JUCIUS 01-31-07		SCALE: NONE				SHEET   1	OF   1	SHEETS	STA.	TO STA.		
							ILLINOIS   FED. AID PROJECT								



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

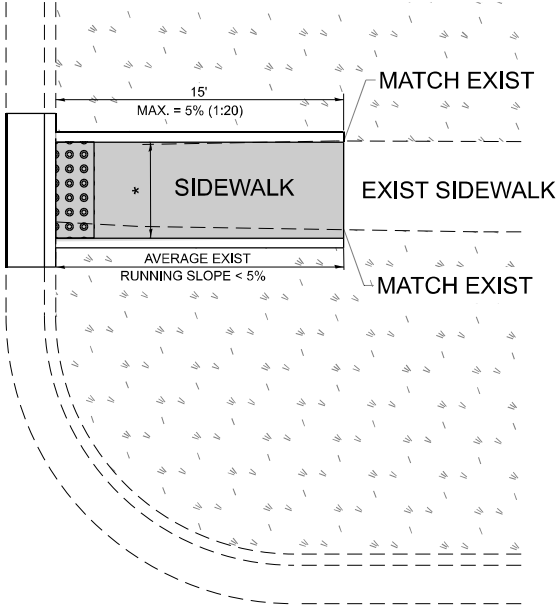
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)  
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: TC-26 (Sheet)  
FILE NAME: c:\p\work\p\work\illinois.gov\_merlin.jose\119625-sht-DistStd.dgn

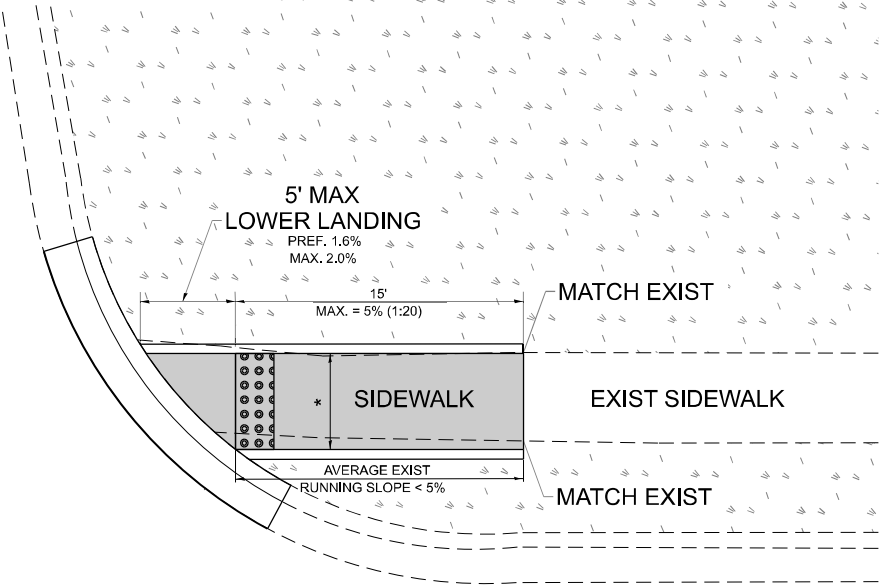
	USER NAME = merlin.jose	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -					351	2025-2004 RS	WILL	34	25
		CHECKED -	REVISED -		TC-26			CONTRACT NO. 80B34				
	PLOT DATE = 10/16/2025	DATE -	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

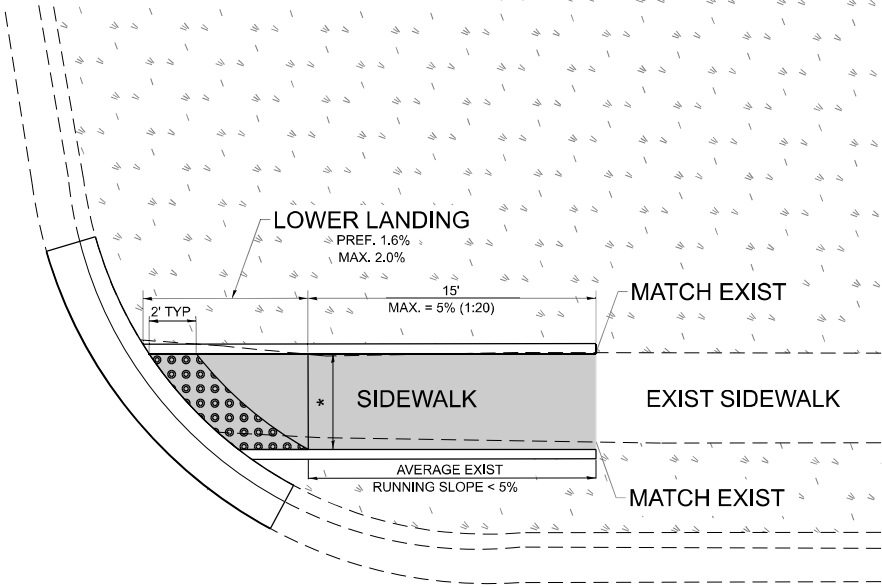
PD-01A



PD-01B



PD-01C



LEGEND

- EXIST. GRASS
- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: PD-01 [Sheet]  
FILE NAME: c:\pwwork\illinois.gov\_merinjose\119625-sh-DistStd.dgn

USER NAME	= merinjose	DESIGNED	-	REVISED	-
		DRAWN	-	REVISED	-
		CHECKED	-	REVISED	-
PLOT DATE	= 10/16/2025	DATE	-	REVISED	-

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

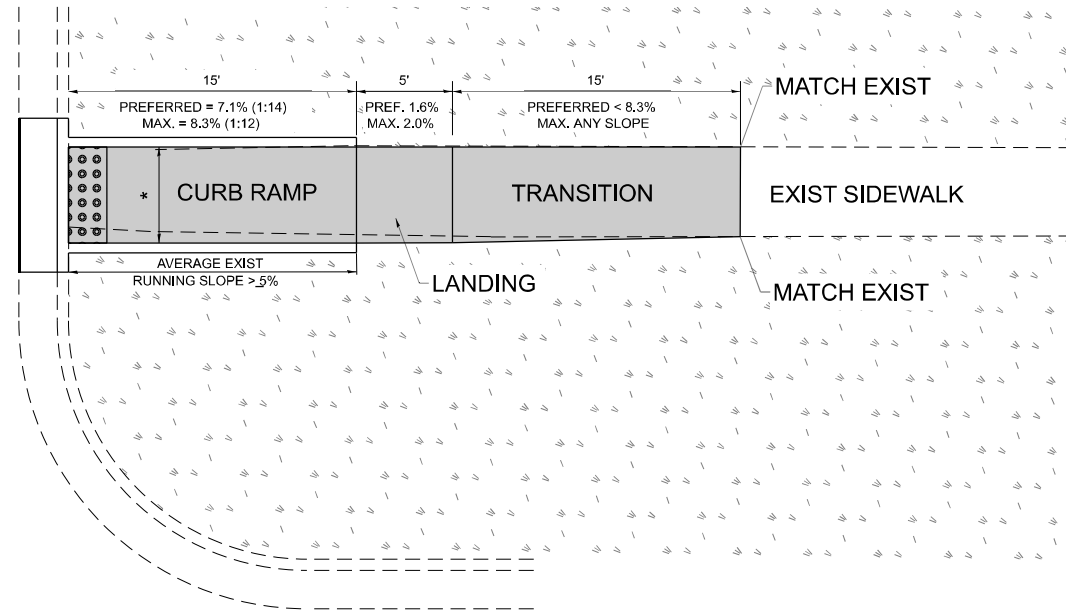
PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-01)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

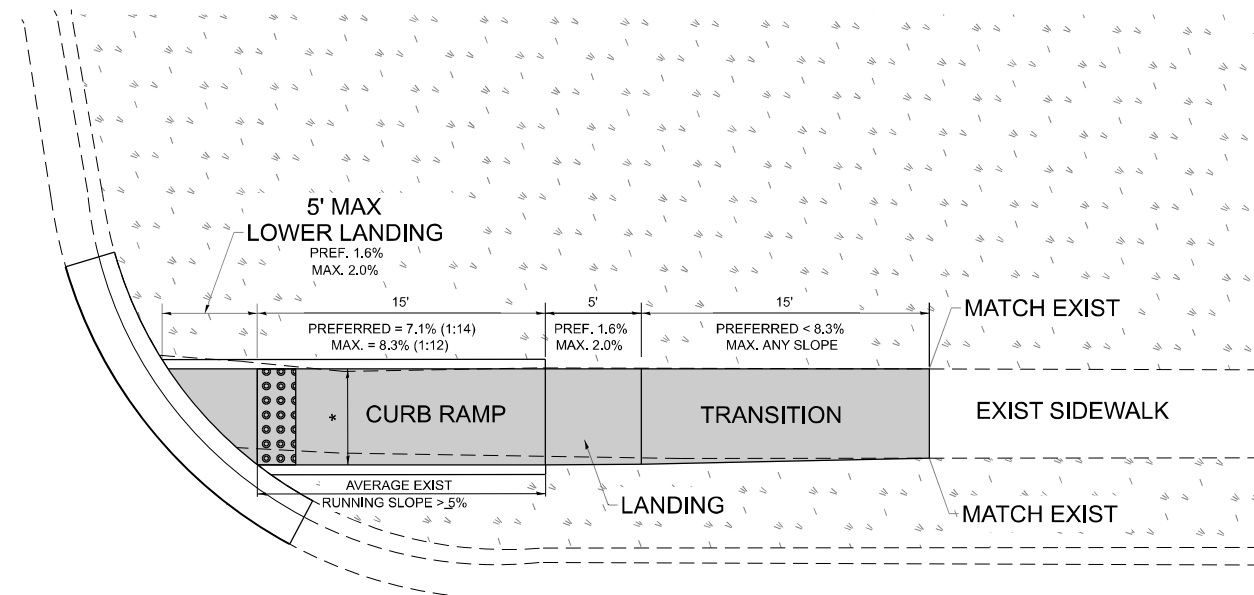
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	26
PD-01		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

**ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR GREATER RUN. SLOPE**

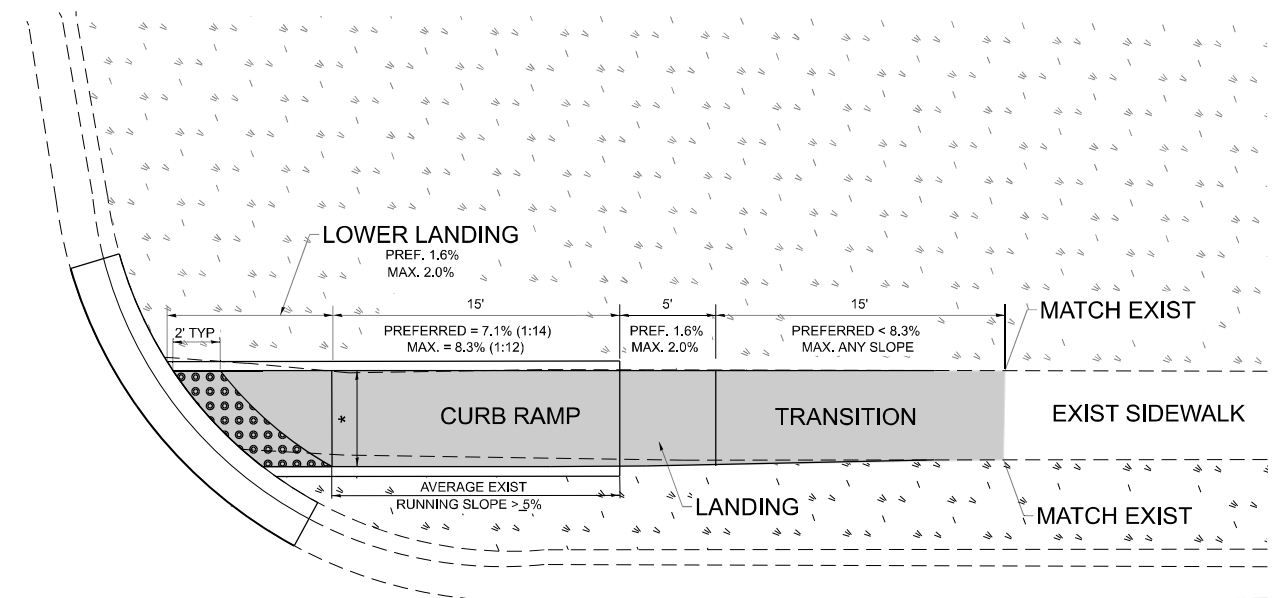
**PD-02A**



**PD-02B**



**PD-02C**



### LEGEND



EXIST. GRASS



PROPOSED SIDEWALK



## DETECTABLE WARNINGS

===== PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

USER NAME = merin.jose	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/16/2025	DATE = 10/02/2019	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

## PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS (PD-02)

SCALE NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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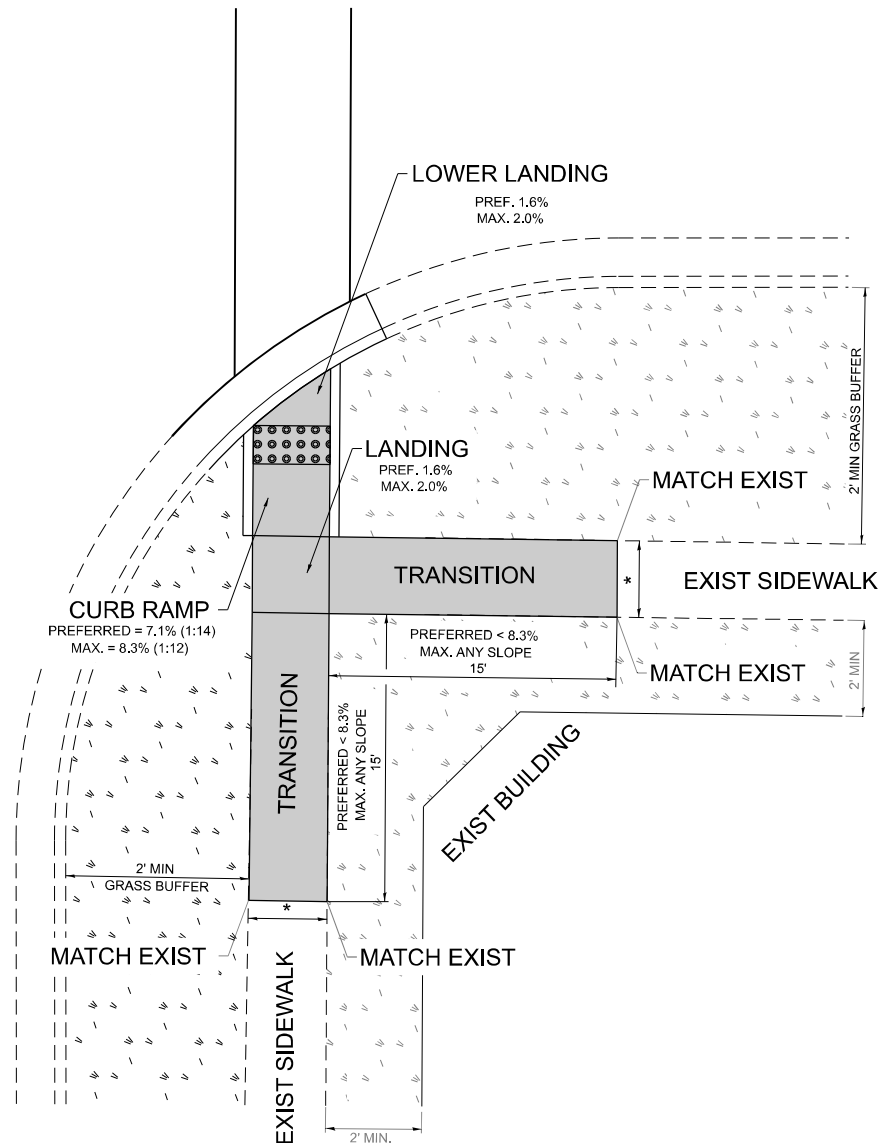
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	27
PD-02		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

MODEL: PD-02 [Sheet]  
FILE NAME: c:\pw\_world\worldtillinois.gov\_medin\_jose@illinois.gov\d1124205\D119625-sh1-Dis1Sids.dgn

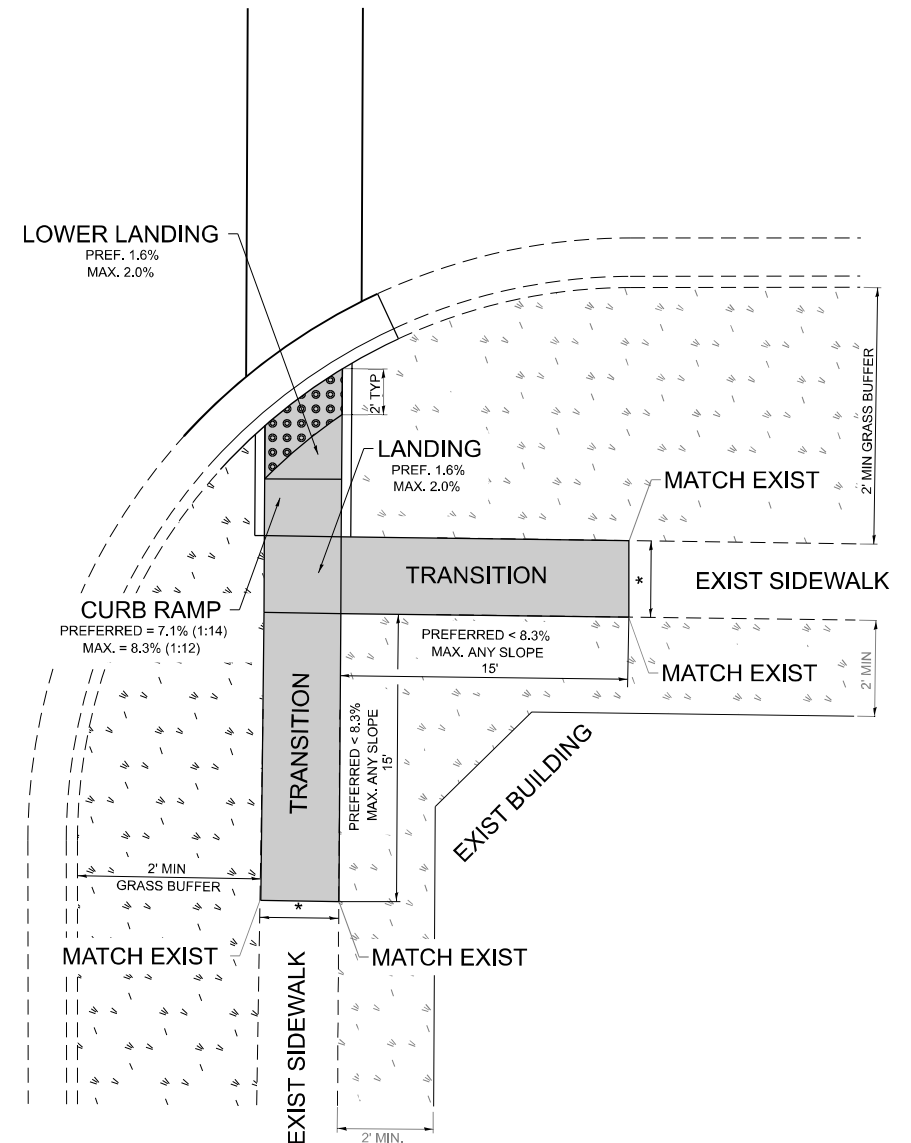


## **ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ TURNING SPACE**

**PD-04A**



**PD-04B**



### LEGEND



EXIST. GRASS



PROPOSED SIDEWALK



## DETECTABLE WARNINGS

===== PROPOSED SIDE CURB

**CONSTRUCTION NOTES:**

1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK

\* MATCH EXISTING SIDEWALK WIDTH

USER NAME = merin.jose	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
	CHECKED -	REVISED -
PLOT DATE = 10/16/2025	DATE = 10/02/2019	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

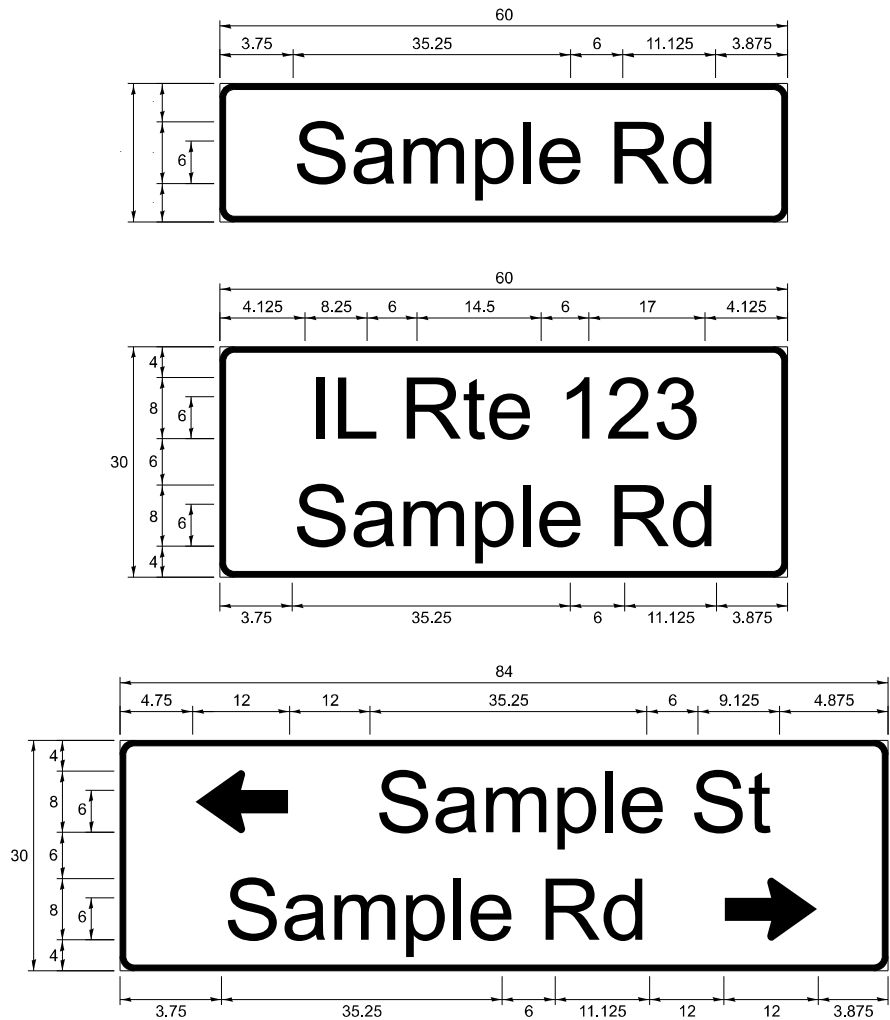
**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS WITH TURNING SPACE (PD-04)**

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
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F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	28
PD-04		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

MODEL: PD-04 [Sheet]  
FILE NAME: c:\pw\work\pidwtot\illinois.gov\_merin.jose@illinois.gov\d1124205ID119625-sht-DistSids.dgn

SIGN PANEL - TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

- J.O. HERBERT COMPANY, INC  
MIDLOTHIAN, VA

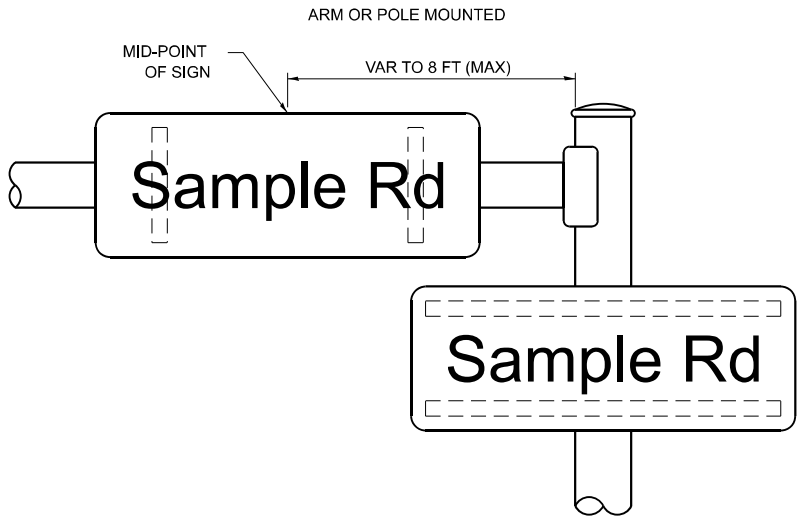
- WESTERN REMAC, INC.  
WOODRIDGE, IL

PARTS LISTING:

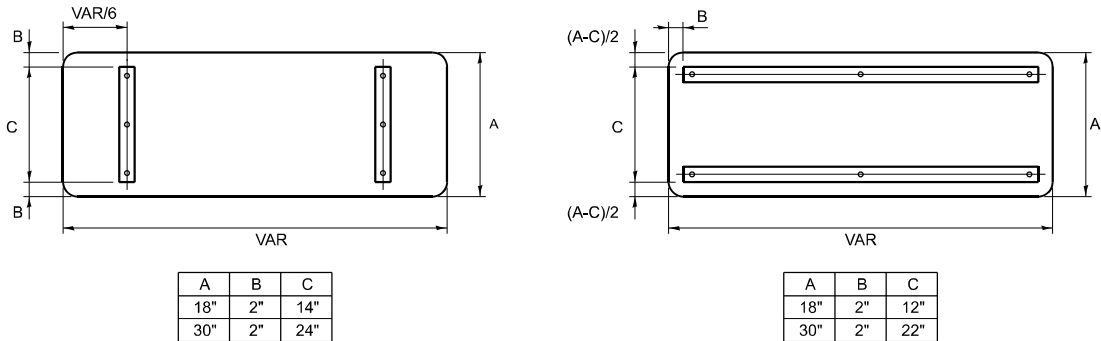
SIGN CHANNEL  
SIGN SCREWS  
PART #HPN053 (MED. CHANNEL)  
1/4" x 14 x 1" H.W.H. #3  
SELF TAPPING WITH NEOPRENE WASHER  
PART #HPN034 (UNIVERSAL)  
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

MODEL: TS-02 [Sheet]  
FILE NAME: c:\p\work\illinois.gov\1124205D\119625-sh-DisStds.dgn

PLOT DATE = 10/16/2025	USER NAME = merin,jose	DESIGNED - LP/IP	REVISED - LP 07/01/2015
		DRAWN - LP	REVISED -
		CHECKED - IP	REVISED -
	DATE - 10/01/2014	REVISED -	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
MAST ARM MOUNTED STREET NAME SIGNS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	29
TS-02		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE			SIGNAL HEAD		
COMMUNICATION CABINET			-SQUARE			- (P) PROGRAMMABLE SIGNAL HEAD		
MASTER CONTROLLER			-ROUND					
MASTER MASTER CONTROLLER			HEAVY DUTY HANDHOLE					
UNINTERRUPTABLE POWER SUPPLY			-SQUARE					
SERVICE INSTALLATION			-ROUND					
- (P) POLE MOUNTED			DOUBLE HANDHOLE			SIGNAL HEAD WITH BACKPLATE		
SERVICE INSTALLATION			JUNCTION BOX			- (P) PROGRAMMABLE SIGNAL HEAD		
- (G) GROUND MOUNTED			RAILROAD CANTILEVER MAST ARM			- (RB) RETROREFLECTIVE BACKPLATE		
- (GM) GROUND MOUNTED METERED			RAILROAD FLASHING SIGNAL					
TELEPHONE CONNECTION			RAILROAD CROSSING GATE			PEDESTRIAN SIGNAL HEAD		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CROSSBUCK			AT RAILROAD INTERSECTIONS		
ALUMINUM MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			PEDESTRIAN SIGNAL HEAD		
STEEL COMBINATION MAST ARM			UNDERGROUND CONDUIT (UC),			WITH COUNTDOWN TIMER		
ASSEMBLY AND POLE WITH LUMINAIRE			GALVANIZED STEEL					
SIGNAL POST			TEMPORARY SPAN WIRE,			ILLUMINATED SIGN		
- (BM) BARREL MOUNTED - TEMPORARY			TETHER WIRE, AND CABLE			"NO LEFT TURN"/"NO RIGHT TURN"		
WOOD POLE			SYSTEM ITEM			NUMBER OF CONDUCTORS, ELECTRIC		
GUY WIRE			INTERSECTION ITEM			CABLE NO. 14, UNLESS NOTED OTHERWISE.		
SIGNAL HEAD			REMOVE ITEM			ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SIGNAL HEAD WITH BACKPLATE			RELOCATE ITEM			GROUND CABLE IN CONDUIT,		
SIGNAL HEAD OPTICALLY PROGRAMMED			ABANDON ITEM			NO. 6 SOLID COPPER (GREEN)		
FLASHER INSTALLATION			CONTROLLER CABINET AND			ELECTRIC CABLE IN CONDUIT, TRACER		
- (FS) SOLAR POWERED			FOUNDATION TO BE REMOVED			NO. 14 1/C		
PEDESTRIAN SIGNAL HEAD			MAST ARM POLE AND			COAXIAL CABLE		
PEDESTRIAN PUSH BUTTON			FOUNDATION TO BE REMOVED			VENDOR CABLE		
- (APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			SIGNAL POST AND			COPPER INTERCONNECT CABLE,		
RADAR DETECTION SENSOR			FOUNDATION TO BE REMOVED			NO. 18, 3 PAIR TWISTED, SHIELDED		
VIDEO DETECTION CAMERA			DETECTOR LOOP, TYPE I			FIBER OPTIC CABLE		
RADAR/VIDEO DETECTION ZONE			PREFORMED DETECTOR LOOP			-NO. 62.5/125, MM12F		
PAN, TILT, ZOOM (PTZ) CAMERA			SAMPLING (SYSTEM) DETECTOR			-NO. 62.5/125, MM12F SM12F		
EMERGENCY VEHICLE LIGHT DETECTOR			INTERSECTION AND SAMPLING			-NO. 62.5/125, MM12F SM24F		
CONFIMATION BEACON			(SYSTEM) DETECTOR					
WIRELESS INTERCONNECT			QUEUE AND SAMPLING			GROUND ROD		
WIRELESS INTERCONNECT RADIO REPEATER			(SYSTEM) DETECTOR			- (C) CONTROLLER		
			WIRELESS DETECTOR SENSOR			- (M) MAST ARM		
			WIRELESS ACCESS POINT			- (P) POST		
						- (S) SERVICE		

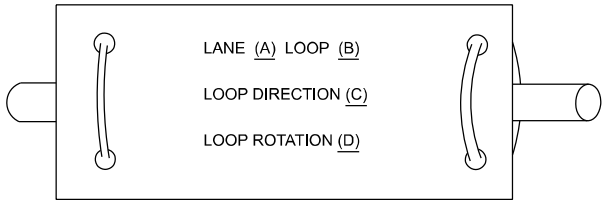
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		DRAWN - IP	REVISED -					351	2025-2004 RS	WILL	34	30
		CHECKED - LP	REVISED -					TS-05				CONTRACT NO. 80B34
	PLOT DATE = 10/16/2025	DATE - 9/29/2016	REVISED -					ILLINOIS   FED. AID PROJECT				
SCALE:		NONE	SHEET	OPI	SHEETS	STA.	TO STA.					

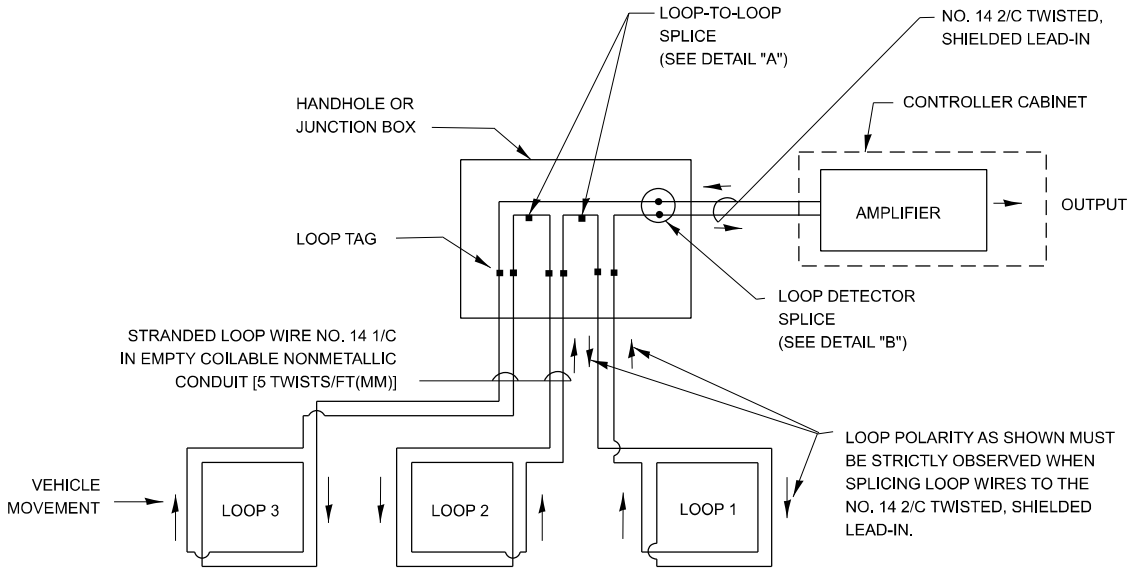
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

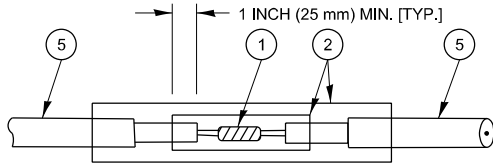


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

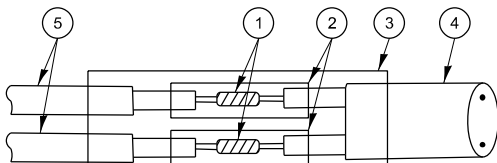


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES. SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE.
- THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

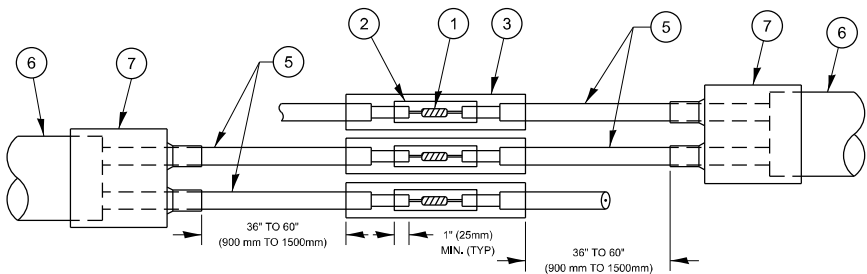


DETAIL "A"  
LOOP-TO-LOOP SPLICE

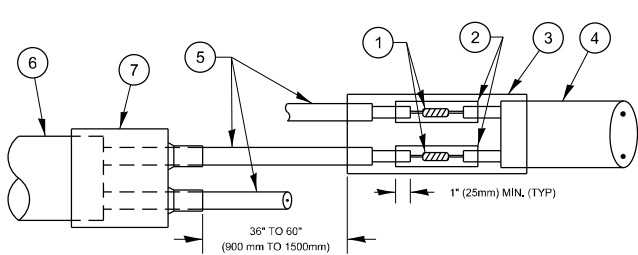


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

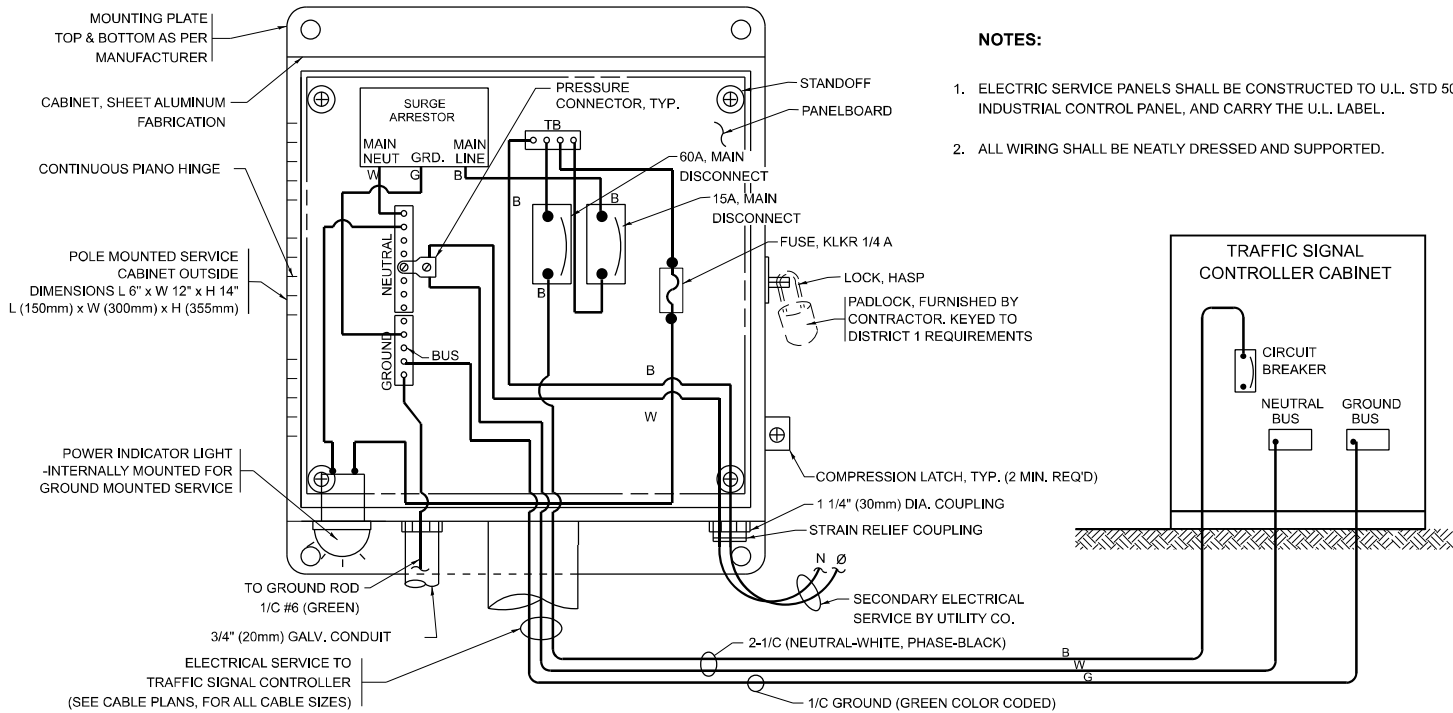
LOOP DETECTOR SPLICE

1. WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
2. WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
3. WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
4. NO. 14 2/C TWISTED, SHIELDED CABLE.
5. LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE. PRE-FORMED LOOP
6. XL POLYOLEFIN 2 CONDUCTOR
7. BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

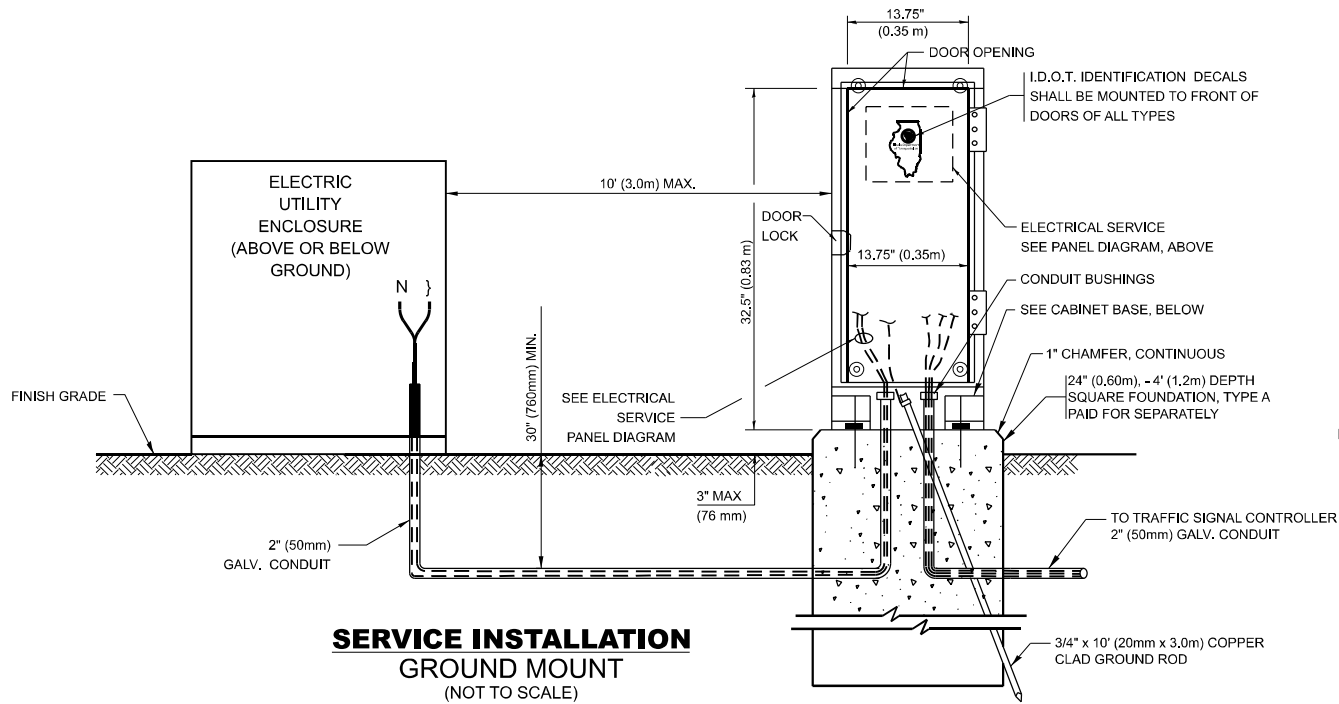
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		DRAWN    -	REVISED   -						351	2025-2004 RS	WILL	34	31
		CHECKED   -	REVISED   -		TS-05				CONTRACT NO. 80B34				
	PLOT DATE   = 10/16/2025	DATE       -	REVISED   -		SCALE	NONE	SHEET	2	OF 7	SHEETS	STA.	TO STA.	ILLINOIS

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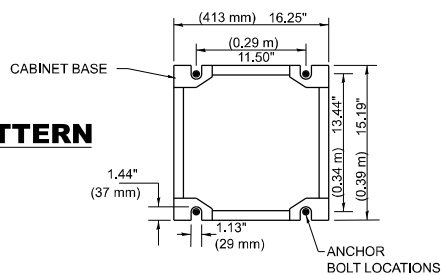


**ELECTRICAL SERVICE - PANEL DIAGRAM**  
**(TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
SERVICE INSTALLATION POLE MOUNT (SHOWN)  
(NOT TO SCALE)



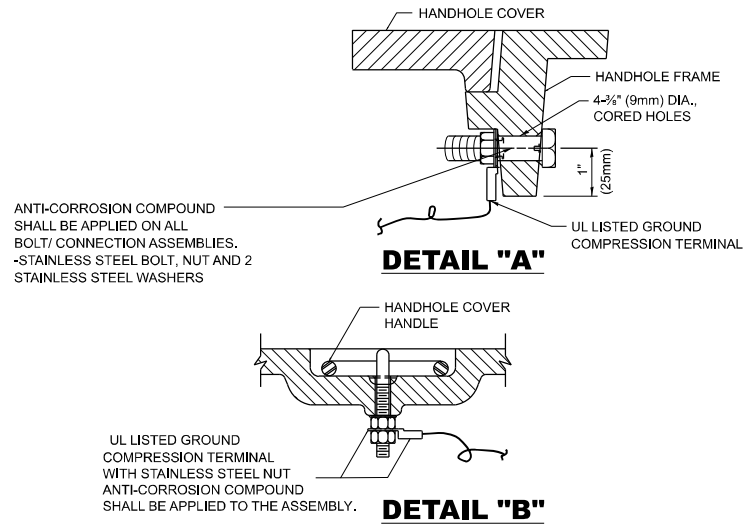
**SERVICE INSTALLATION**  
**GROUND MOUNT**  
(NOT TO SCALE)

**CABINET - BASE BOLT PATTERN**  
(NOT TO SCALE)

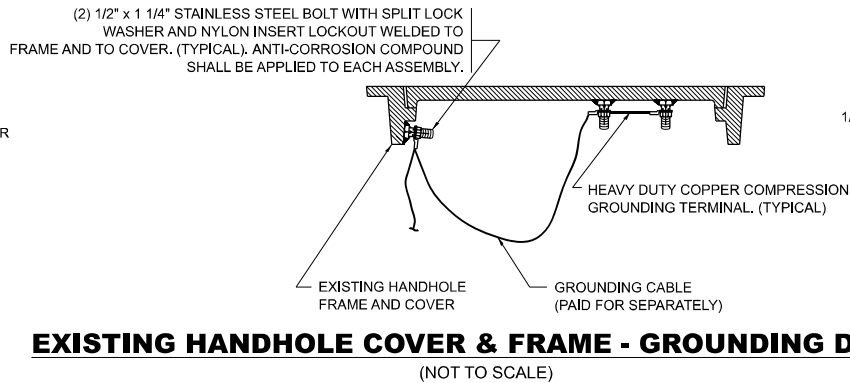


**NOTES:**

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



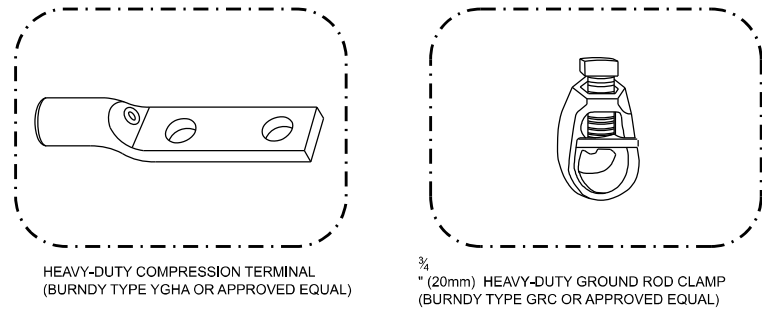
**HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
(NOT TO SCALE)



**EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL**  
(NOT TO SCALE)

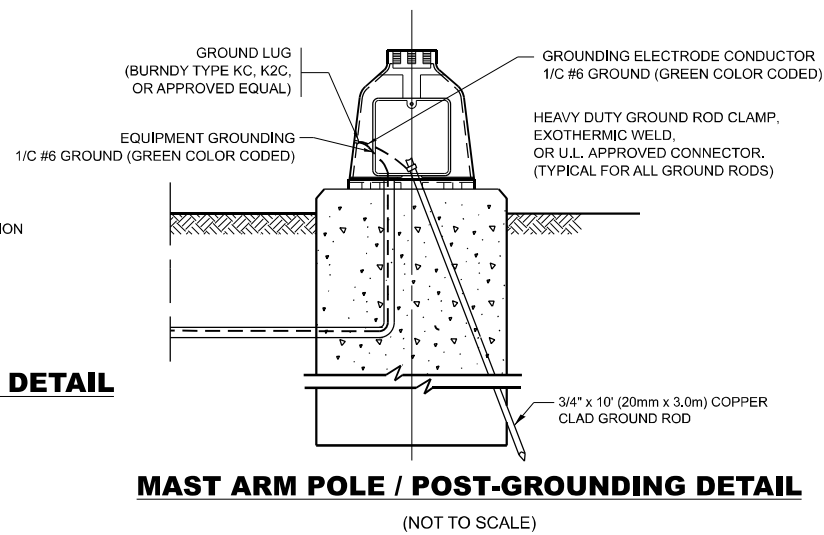
**NOTES:**  
**GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4\"/>
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



**NOTES:**

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES. 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES. 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE / POST-GROUNDING DETAIL**  
(NOT TO SCALE)

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

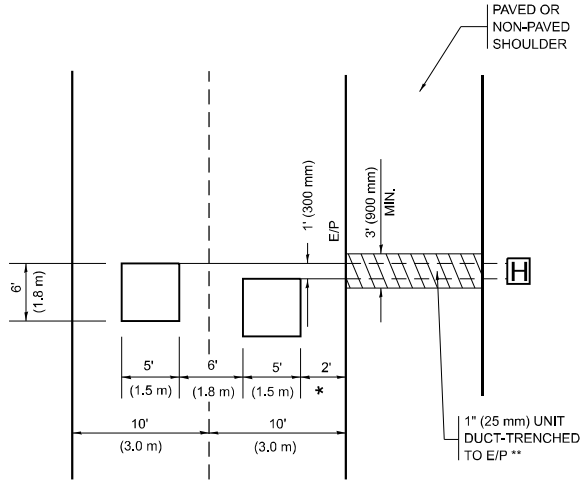
**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 4 OF 7 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	32
TS-05		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT  
NOTE WHICH SHOULD EQUAL  
3' (900 mm) X WIDTH OF  
PAVED SHOULDER.

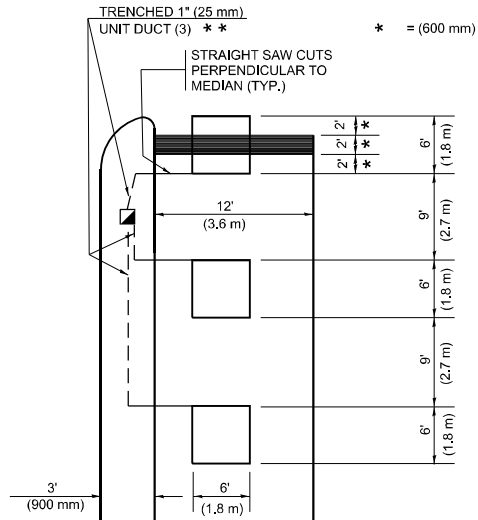


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

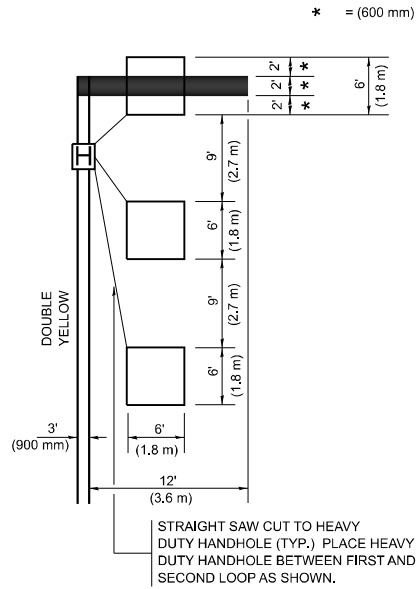
HANDHOLE LOCATION MAY  
VARY DEPENDING ON GEOMETRICS  
AND DESIGN OF TRAFFIC SIGNALS.  
HEAVY-DUTY HANDHOLES TO BE  
USED WHEN THE MEDIAN IS  
MOUNTABLE. REFER TO STANDARD  
814001 TO ENSURE THAT HANDHOLE  
FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS  
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO  
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

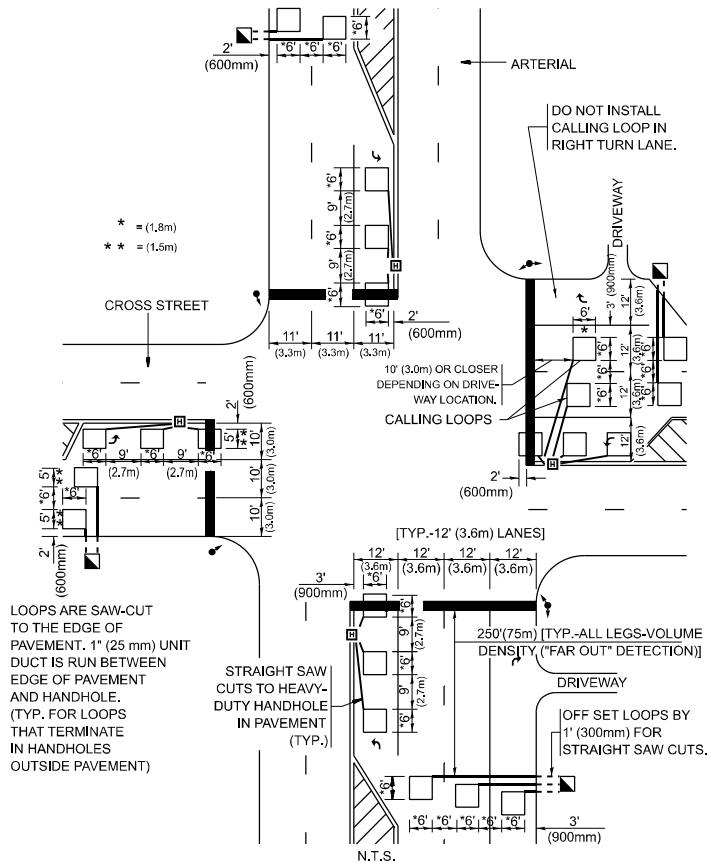
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

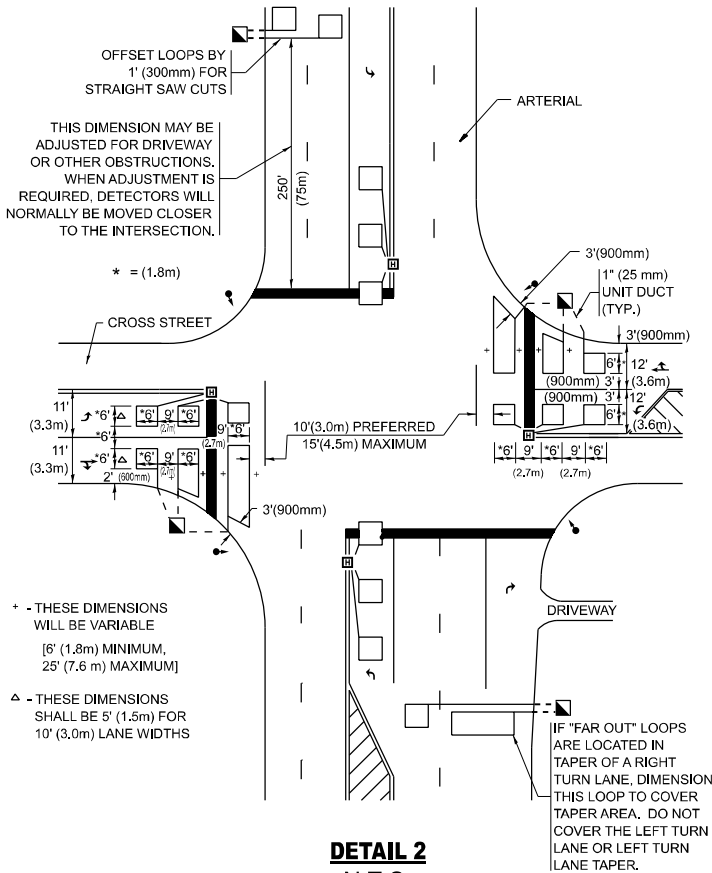
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

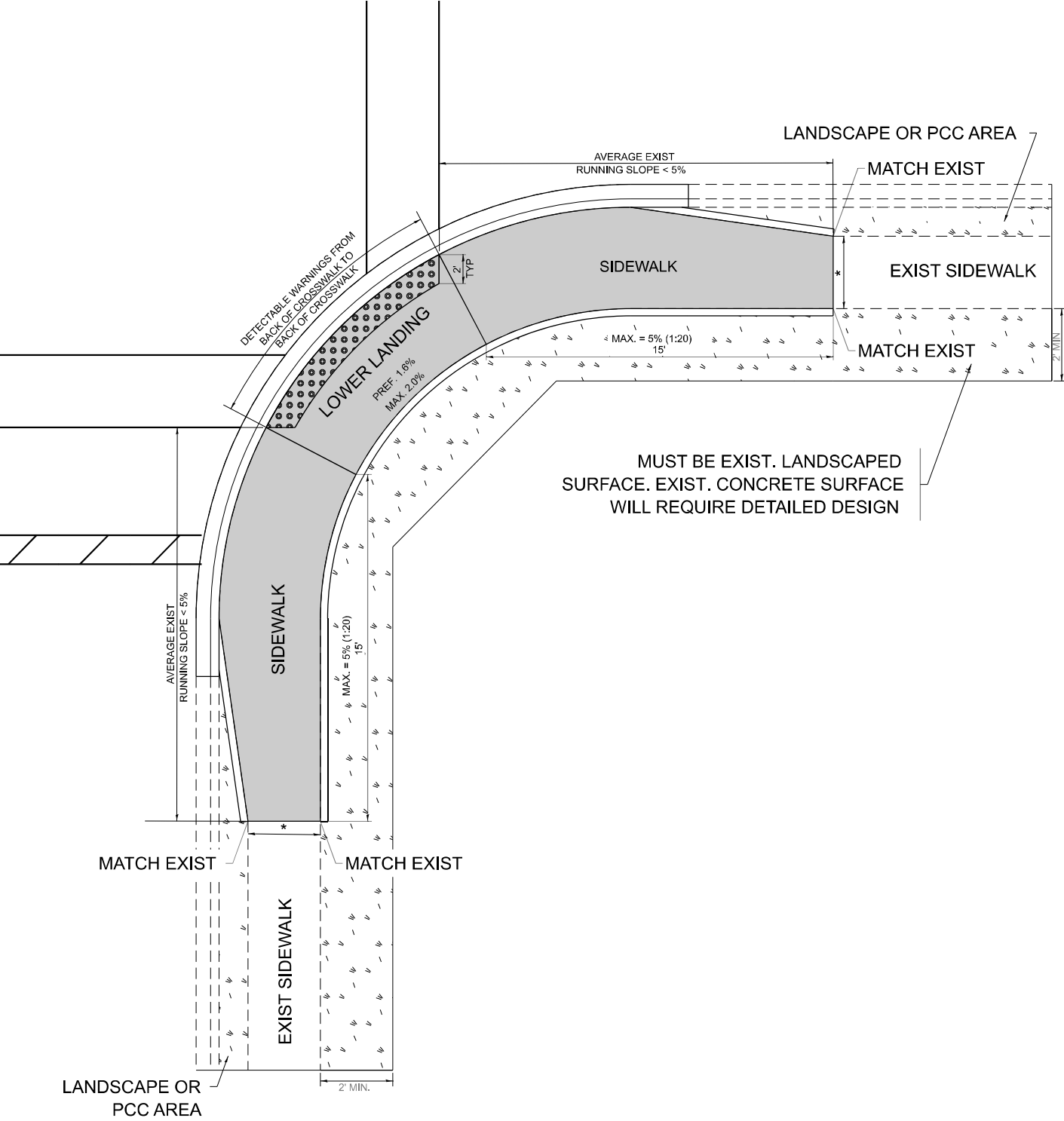
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	DRAWN -	REVISED -
	CHECKED - R.K.F.	REVISED -
PLOT DATE = 10/16/2025	DATE -	REVISED -

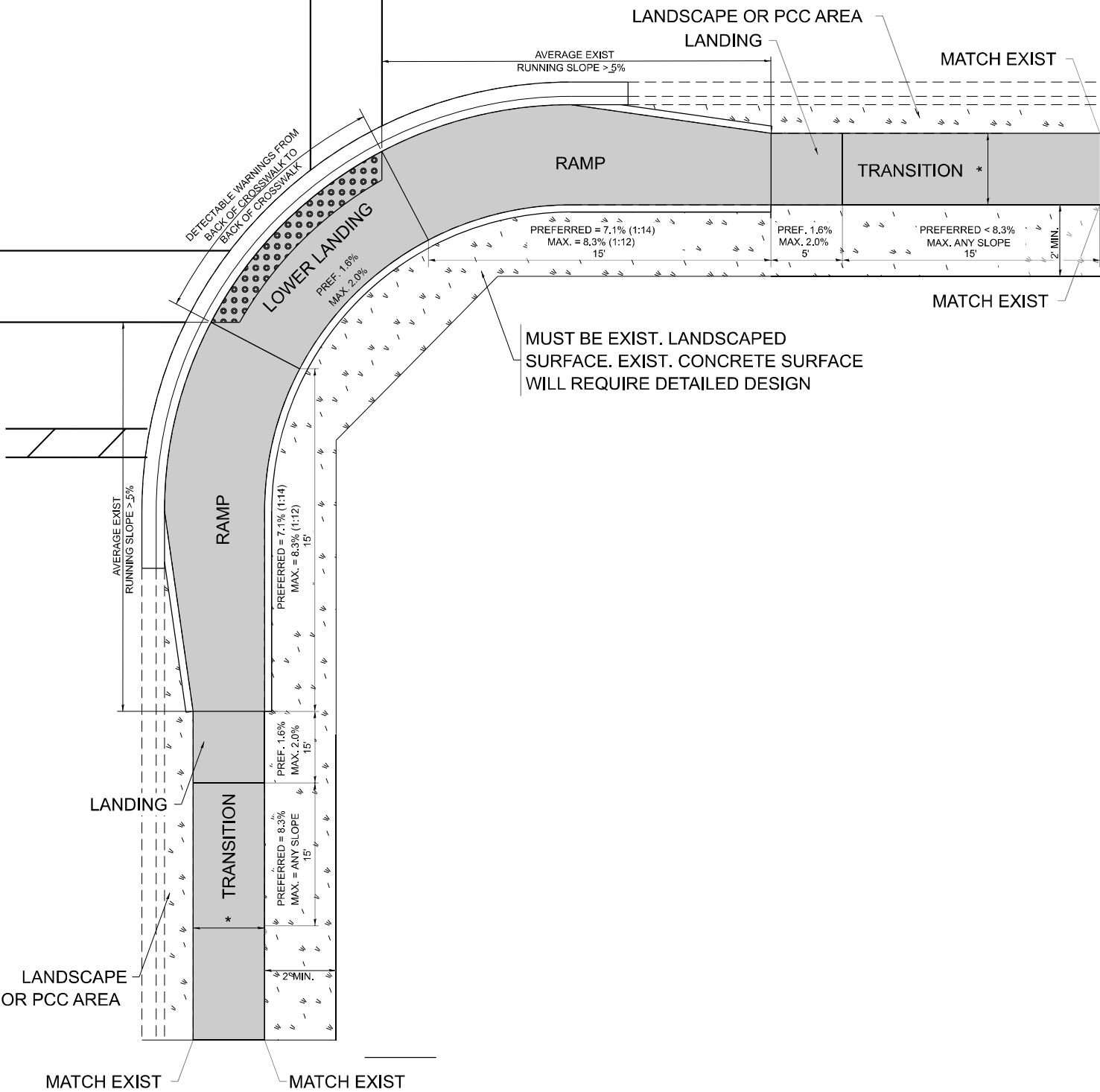
F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	33
TS-07		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		

ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

PD-06A



PD-06B



LEGEND

PROPOSED SIDE CURB

- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

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		DRAWN - R. LEDEZMA	REVISED -
		CHECKED -	REVISED -
	DATE - 10/02/2019		REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PROJECT DETAIL FOR PARALLEL CURB RAMPS  
(PD-06)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
351	2025-2004 RS	WILL	34	34
PD-06		CONTRACT NO. 80B34		
ILLINOIS		FED. AID PROJECT		