

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET 2

TRAFFIC DATA

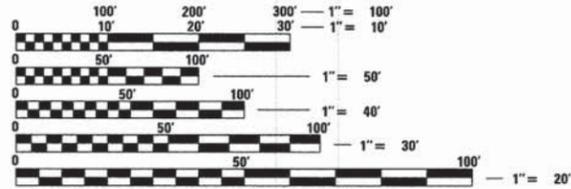
ADT: HILLTOP DRIVE: 1,000 VPD (2012) 1,500 VPD (2040)

HILLTOP DRIVE POSTED SPEED DESIGN SPEED
PROJECT LIMITS 25 MPH (EXISTING/ PROPOSED) 25 MPH

DESIGN DESIGNATION

FAU 4036 (HILLTOP DRIVE) - COLLECTOR (URBAN)

PROJECT LOCATED IN THE VILLAGE OF LAKE IN THE HILLS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811



STRUCTURE INFORMATION:

EXISTING SN 056-6402 THIS EXISTING STRUCTURE WAS BUILT IN 1984 BY MCHENRY COUNTY AS A SINGLE SPAN PRECAST, PRESTRESSED CONCRETE DECK BEAM BRIDGE SUPPORTED ON CLOSED ABUTMENTS ON SPREAD FOOTINGS. THE DESIGN LOADING USED WAS HS20. ROAD TO BE CLOSED DURING CONSTRUCTION. TRAFFIC TO BE MAINTAINED WITH TEMPORARY DETOUR ROUTE DURING CONSTRUCTION. THE STRUCTURE IS TO BE REHABILITATED AS SHOWN WITH NO SALVAGE.

PROFESSIONAL ENGINEER'S SIGN & SEAL EXCLUDING SHEET(S): S-1 TO S-8

Signature of Kevin M. Arft, P.E. KEVIN M. ARFT, P.E. EXPIRES: 11-30-15



STRUCTURAL ENGINEER'S SIGN & SEAL FOR STRUCTURAL SHEET(S): S-1 TO S-8

Signature of Robert G. Davies, P.E., S.E. ROBERT G. DAVIES, P.E., S.E. EXPIRES: 11-30-14



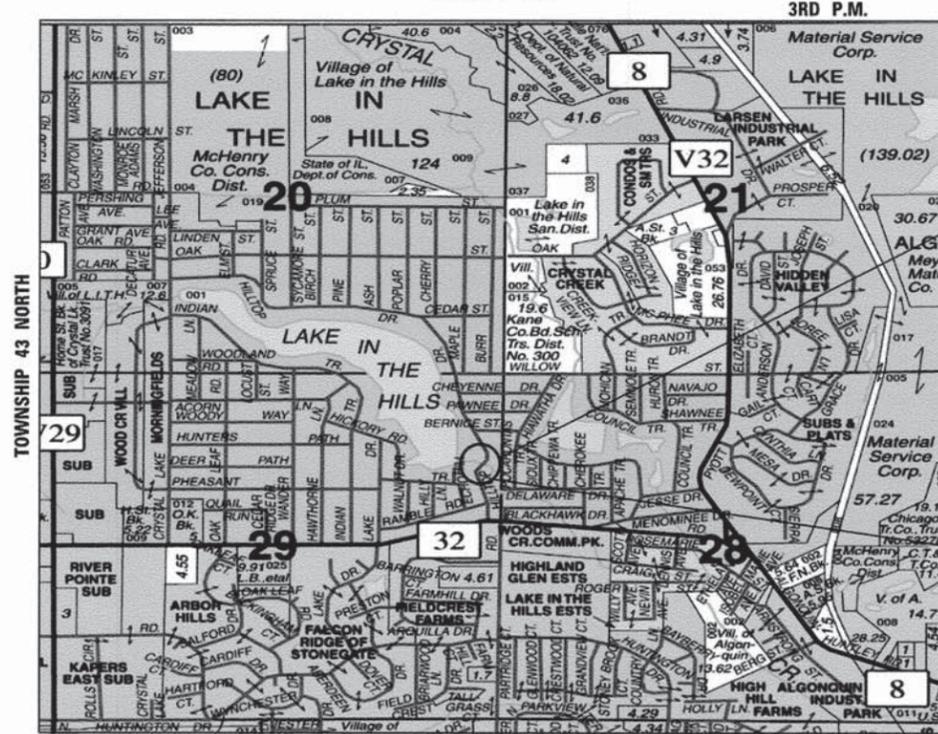
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS

1-17-14 LETTING ITEM 097

PLANS FOR PROPOSED FEDERAL-AID HIGHWAY

FAU 4036 (HILLTOP DRIVE) OVER WOODS CREEK BRIDGE REHABILITATION SECTION: 11-00036-00-BR PROJECT: BHM-9003(892) MCHENRY COUNTY JOB: C-91-155-12

PROJECT LOCATION MAP ALGONQUIN TOWNSHIP N.T.S. RANGE 8 EAST



PROJECT ENDS STA. 10+27.00

PROJECT BEGINS STA. 8+50.00

PROJECT LENGTH

GROSS LENGTH OF IMPROVEMENT (HILLTOP DRIVE) = 177 FT (0.03 MI)

Table with columns: F.A.U. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., FED. ROAD DIST. NO., ILLINOIS, CONTRACT NO.



Approval section containing signatures and dates for the State of Illinois Department of Transportation, District One Engineer of Local Roads and Streets, and HRGreen firm.

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PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAMBURG, IL

COMPANY NAME, PROJECT CONTACT, CLIENT, DATE PLOTTED, FILE NAME, PLOT DRIVER, PEA TABLE

PROJECT ENGINEER: K. ARFT PROJECT MANAGER: R. DAVIES CONTRACT NO. 63857

HRGreen logo and contact information: 420 NORTH FRONT STREET, SUITE 100 | MCHENRY, ILLINOIS 60050

INDEX OF SHEETS

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STATE STANDARDS

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|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEM |
| 515001-03 | NAME PLATE FOR BRIDGES |
| 630001-10 | STEEL PLATE BEAM GUARDRAIL |
| 635006-03 | REFLECTOR AND TERMINAL MARKER PLACEMENT |
| 635011-02 | REFLECTOR MARKER AND MOUNTING DETAILS |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701311-03 | LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701901-03 | TRAFFIC CONTROL DEVICES |
| 720001-01 | SIGN PANEL MOUNTING DETAILS |
| 720006-04 | SIGN PANEL ERECTION DETAILS |
| 720011-01 | METAL POSTS FOR SIGNS, MARKERS & DELINEATORS |
| 728001-01 | TELESCOPING STEEL SIGN SUPPORT |
| 729001-01 | APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS) |
| 780001-04 | TYPICAL PAVEMENT MARKINGS |
| BLR 23-4 | TRAFFIC BARRIER TERMINAL, TYPE 1 |
| BLR 27-1 | TRAFFIC BARRIER TERMINAL, TYPE 5A |

CONTACTS

| | | |
|--|----------------|----------------|
| COMED | NORA FERNANDEZ | (815) 490-7260 |
| NICOR | CONSTANCE LANE | (630) 388-3830 |
| AT&T | HECTOR SUAREZ | (847) 888-6864 |
| COMCAST | TED WYMAN | (630) 600-6349 |
| VILLAGE OF LAKE IN THE HILLS | FRED MULLARD | (847) 860-7500 |
| McHENRY COUNTY DIVISION OF TRANSPORTATION | RAY BEETS | (815) 334-4960 |
| U.S. ARMY CORPS OF ENGINEERS | SOREN HALL | (312) 846-5532 |
| McHENRY - LAKE COUNTY SOIL & WATER CONSERVATION DISTRICT | ED WESKERNA | (815) 338-0099 |
| LAKE IN THE HILLS SANITARY DISTRICT | RICK FORNER | (847) 658-5122 |

STANDARD ABBREVIATIONS

| | |
|--------|-------------------------------------|
| B-B | - BACK TO BACK OF CURB |
| B.C. | - BACK OF CURB |
| B.O.C. | - BACK OF CURB |
| B.S.L. | - BUILDING SETBACK LINE |
| C.B. | - STORM CATCH BASIN |
| C.E. | - COMMONWEALTH EDISON CO. |
| D.E. | - DRAINAGE EASEMENT |
| E-E | - EDGE TO EDGE OF PAVEMENT |
| E.O.P. | - EDGE OF PAVEMENT |
| E.O.S. | - EDGE OF SHOULDER |
| E.P. | - EDGE OF PAVEMENT |
| E.S. | - EDGE OF SHOULDER |
| F.E.S. | - FLARED END SECTION |
| I.B.T. | - ILLINOIS BELL TELEPHONE CO. |
| L.E. | - LANDSCAPE EASEMENT |
| M.H. | - MANHOLE (TYPE SPECIFIED ON PLANS) |
| R.O.W. | - RIGHT OF WAY |
| T.B.F. | - TRENCH BACKFILL |
| T.C. | - TOP OF CURB |
| T.C.E. | - TEMPORARY CONSTRUCTION EASEMENT |
| T.O.B. | - TOP OF BERM |
| T.O.C. | - TOP OF CURB |
| U.E. | - UTILITY EASEMENT |
| U.O.N. | - UNLESS OTHERWISE NOTED |

COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 CLIENT: City of Aurora
 DATE PLOTTED: 10/17/2013 9:55:05 AM
 FILE PLACED: 10/17/2013 9:55:05 AM
 PLOT ORIGIN: def:DEF-11.dgn
 PEN TABLE: standard-trans.tbl



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| FILE NAME = 86120386-gen-01.dgn | DRAWN - WJH | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - RGD | REVISED - |
| PLOT DATE = 10/17/2013 | DATE - 10/17/13 | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS AND LIST OF STATE STANDARDS
 HILLTOP DRIVE OVER WOODS CREEK**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|---------|--------------|-----------|
| 4036 | 11-00036-00-BR | McHENRY | 24 | 2 |
| CONTRACT NO. | | | | 63857 |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |

GENERAL NOTES

ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2012

ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.

PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT THEM TO THE ENGINEER BEFORE DOING ANY WORK. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT. FORTY EIGHT HOURS BEFORE STARTING EXCAVATION THE CONTRACTOR SHALL CALL J.U.L.I.E. (1-800-892-0123) TO HAVE THE LOCATION OF EXISTING UTILITIES STAKED.

THE CONTRACTOR SHALL CONTACT THE VILLAGE OF LAKE IN THE HILLS AND THE McHENRY COUNTY DIVISION OF TRANSPORTATION AT LEAST 72 HOURS IN ADVANCE OF BEGINNING ANY WORK.

THE GENERAL CONTRACTOR SHALL CONTACT THE ARMY CORPS OF ENGINEERS AND THE MCHENRY-LAKE SOIL AND WATER CONSERVATION DISTRICT TWO (2) WEEKS PRIOR TO CONSTRUCTION. SEE EROSION CONTROL PLANS AND BRIDGE PLANS.

OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.

THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.1 GALLONS / SQ YD.

SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.

THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE NOTED.

THE RESIDENT ENGINEER SHALL CONTACT THE QA MATERIALS TESTING CONSULTANT 48 HOURS IN ADVANCE OF PLACING MATERIALS.

THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS EXCAVATION, UNSUITABLE AND UNUSABLE MATERIALS OFFSITE AND AT AN APPROVED LOCATION IN A MANNER THAT PUBLIC OR PRIVATE PROPERTY WILL NOT BE DAMAGED OR ENDANGERED. THIS WORK IS CONSIDERED INCLUDED IN THE COST OF THE REMOVAL ITEMS.

THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ANY REQUIRED VILLAGE-OWNED UTILITY INSPECTIONS WITH THE VILLAGE OF LAKE IN THE HILLS

SPECIAL ATTENTION IS DRAWN TO THE FACT THAT ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 107.07.

THE ENGINEER AND VILLAGE ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF HIS WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO THE USE OF THE WATER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION.

THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.

THE LOCATION OF THE EXISTING UTILITIES, AS SHOWN ON THE DRAWINGS, REPRESENTS DATA RECEIVED FROM VARIOUS SOURCES. IT IS NOT GUARANTEED TO BE CORRECT OR ALL INCLUSIVE. THE CONTRACTOR SHALL CONDUCT HIS OWN INVESTIGATIONS INTO THE LOCATION, SIZE, DEPTH, AND NATURE OF ANY AND ALL EXISTING UTILITIES WHICH MAY INTERFERE WITH THE WORK UNDER THIS CONTRACT. ANY EXISTING UTILITIES WHICH ARE TO REMAIN IN SERVICE SHALL BE FULLY PROTECTED BY THE CONTRACTOR AND ANY DAMAGE CAUSED BY THE CONSTRUCTION SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST.

THE THICKNESSES OF HOT MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE HOT MIX ASPHALT MIXTURES ARE TO BE PLACED.

WHERE PROPOSED WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION, NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.

THE CONTRACTOR WILL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH BARRICADE USED OR BARRELS TO BE DOUBLE WEIGHTED. (TYPE I OR TYPE II ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.) ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF EIGHT (8) SANDBAGS PER BARRICADE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE VILLAGE OF LAKE IN THE HILLS. THE CONTRACTOR SHALL RESTORE THE FIELD OFFICE SITE TO LIKE ORIGINAL CONDITION.

THE CONTRACTOR SHALL ENSURE THAT ALL STORM SEWER MANHOLES, SANITARY SEWER MANHOLES, WATERMAIN VALVES, AND FIRE HYDRANTS REMAIN READILY ACCESSIBLE TO THE VILLAGE AND THE LAKE IN THE HILLS SANITARY DISTRICT AT ALL TIMES.

THE WORK UNDER THIS CONTRACT SHALL CONFORM TO ALL REGULATIONS GIVEN IN THE ARMY CORPS OF ENGINEERS PERMIT ISSUED FOR THE PROJECT AND THE IDOT FLOODWAY PERMIT ISSUED FOR THE PROJECT.

THE CONTRACTOR SHALL NOT ALLOW CONCRETE OR OTHER CONSTRUCTION DEBRIS TO FALL INTO THE CREEK DURING DEMOLITION OR CONSTRUCTION.

EXISTING SIGNS IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED, STORED, AND RE-INSTALLED AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A LOG OF EXISTING SIGNS TO THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."

ADDITIONAL "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL" AND "AGGREGATE SUBGRADE IMPROVEMENT, 12" NOT INCLUDED IN THE EARTHWORK SCHEDULE HAVE BEEN ADDED FOR USE IF UNSUITABLE MATERIAL IS ENCOUNTERED IN THE FIELD, AS DETERMINED BY THE ENGINEER. IF UNSUITABLE AND/OR UNSTABLE MATERIALS IS NOT ENCOUNTERED, THEN THE QUANTITIES SHALL BE DEDUCTED AND NO COMPENSATION WILL BE DUE TO THE CONTRACTOR.

ADDITIONAL QUANTITIES:
REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL: 120 CU YD
AGGREGATE SUBGRADE IMPROVEMENT 12": 348 SQ YD

THE LENGTH OF "GUARDRAIL REMOVAL" INCLUDES THE LENGTH OF THE TRAFFIC BARRIER TERMINALS.

EXISTING FIELD TILES ENCOUNTERED DURING CONSTRUCTION SHALL BE MAINTAINED IN ACCORDANCE WITH SECTION 611 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE ITEM "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL."

THIS PROJECT MAY REQUIRE UP TO 4 CHANGEABLE MESSAGE SIGNS.

SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.

THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH HIS CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

ALL SIGNS SHALL BE INSTALLED OR RELOCATED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. COST FOR THIS WORK SHALL BE INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)"

ALL REMOVED SIGNS SHALL BE RETURNED TO THE VILLAGE OF LAKE IN THE HILLS AT 9010 HALIGUS ROAD, LAKE IN THE HILLS, IL 60156 COST FOR THIS WORK SHALL BE INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."

LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE ITEM "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."

TRAFFIC CONTROL

SEE TEMPORARY DETOUR PLAN FOR GENERAL NOTES CONCERNING TRAFFIC CONTROL AND PROTECTION.

EROSION CONTROL

SEE EROSION CONTROL PLANS FOR GENERAL NOTES CONCERNING EROSION CONTROL.

IN ADDITION TO THE REQUIREMENTS OF SECTION 107.01 OF THE STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROPER REMOVAL AND DISPOSAL OF EXCAVATED MATERIALS FROM THE PROJECT SITE. THE CONTRACTOR WILL MEET ALL REQUIREMENTS SET FORTH BY THE IEPA AND PUBLIC ACT 96-1416 IN REGARDS TO CLEAN CONSTRUCTION AND DEMOLITION DEBRIS WHICH MAY INCLUDE, BUT NOT LIMITED TO, FIELD AND LABORATORY ANALYSES, CERTIFICATION FROM A LICENSED PROFESSIONAL ENGINEER, DUMPING FEES AND DOCUMENTATION. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE COST OF THE CONTRACT. NO ADDITIONAL COMPENSATION WILL BE PROVIDED.

SUGGESTED SEQUENCE OF CONSTRUCTION

1. POST DETOUR AND CLOSE ROAD TO THROUGH TRAFFIC.
2. INSTALL EROSION CONTROL MEASURES INDICATED ON EROSION CONTROLS PLANS AND DETAILS.
3. EXCAVATION BEHIND ABUTMENTS
4. REMOVE EXISTING BRIDGE SUPERSTRUCTURE.
5. DEWATERING FOR RIPRAP INSTALLATION
6. INSTALL PROPOSED STONE RIPRAP.
7. SET BEAMS AND CONSTRUCT BRIDGE SUPERSTRUCTURE.
8. BACKFILL BEHIND ABUTMENTS
9. PERFORM APPROACH ROADWAY IMPROVEMENTS AND FINAL RESTORATION.

SUMMARY OF PHASE I COMMITMENTS

1. DETOUR COORDINATION WITH McHENRY COUNTY DIVISION OF TRANSPORTATION

COMPANY NAME: Kevin M. Arft
PROJECT CONTACT: City of Aurora
CLIENT: 11/1/2013 10:27:30 AM
DATE PLOTTED: 86120386-gen-01.dgn
FILE NAME: pcf.dwg
PLOT DRIVER: stender@trans.tbi
PEN TABLE:



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| FILE NAME = 86120386-gen-01.dgn | DRAWN - WJH | REVISED - |
| PLOT SCALE = N.T.S. | CHECKED - RGD | REVISED - |
| PLOT DATE = 11/1/2013 | DATE - 11/1/13 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
HILLTOP DRIVE OVER WOODS CREEK**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|----------------|---------|---------------------------|-----------|
| 4036 | 11-00036-00-BR | McHENRY | 24 | 3 |
| CONTRACT NO. 63857 | | | ILLINOIS FED. AID PROJECT | |

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | |
|-----------------------|---|--------|----------------|---|--|
| PAY ITEM NUMBER | PAY ITEMS | UNITS | TOTAL QUANTITY | ROADWAY 80% FEDERAL 20% LOCAL 0004 | BRIDGE 80% FEDERAL 20% LOCAL 0014 |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 255 | 255 | |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SO YD | 460 | 460 | |
| 25000210 | SEEDING, CLASS 2A | ACRE | 0.1 | 0.1 | |
| 25000310 | SEEDING, CLASS 4 | ACRE | 0.1 | 0.1 | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 9 | 9 | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 9 | 9 | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 9 | 9 | |
| 25100635 | HEAVY DUTY EROSION CONTROL BLANKET | SO YD | 920 | 920 | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 60 | 60 | |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 80 | 80 | |
| 28000400 | PERIMETER EROSION BARRIER | FOOT | 395 | 395 | |
| 28100109 | STONE RIPRAP, CLASS A5 | SO YD | 105 | | 105 |
| 28200200 | FILTER FABRIC | SO YD | 75 | | 75 |
| 30300001 | AGGREGATE SUBGRADE IMPROVEMENT | CU YD | 120 | 120 | |
| 30300112 | AGGREGATE SUBGRADE IMPROVEMENT 12" | SO YD | 365 | 365 | |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 35 | 35 | |
| 40701921 | HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 12" | SO YD | 350 | 350 | |
| 44000100 | PAVEMENT REMOVAL | SO YD | 365 | 365 | |
| 44000200 | DRIVEWAY PAVEMENT REMOVAL | SO YD | 20 | 20 | |
| 48101500 | AGGREGATE SHOULDERS, TYPE B 6" | SO YD | 100 | 100 | |
| 50101500 | REMOVAL OF EXISTING SUPERSTRUCTURES | EACH | 1 | | 1 |
| 50200100 | STRUCTURE EXCAVATION | CU YD | 99.8 | | 99.8 |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 3.7 | | 3.7 |
| 50300260 | BRIDGE DECK GROOVING | SO YD | 187 | | 187 |
| 50300300 | PROTECTIVE COAT | SO YD | 200 | | 200 |
| 50400405 | PRECAST PRESTRESSED CONCRETE DECK BEAMS (21" DEPTH) | SO FT | 1,792 | | 1,792 |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 3,140 | | 3,140 |
| 50901050 | STEEL RAILING, TYPE SM | FOOT | 112 | | 112 |
| 51500100 | NAME PLATES | EACH | 1 | | 1 |
| 58700300 | CONCRETE SEALER | SO FT | 78 | | 78 |
| 59100100 | GEOCOMPOSITE WALL DRAIN | SO YD | 63 | | 63 |
| 60603800 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B - 6.12 | FOOT | 60 | 60 | |
| 63000003 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | FOOT | 75 | 75 | |
| 63100075 | TRAFFIC BARRIER TERMINAL, TYPE 5A | EACH | 4 | 4 | |

| SUMMARY OF QUANTITIES | | | | CONSTRUCTION TYPE CODE | |
|-----------------------|--|--------|----------------|---|--|
| PAY ITEM NUMBER | PAY ITEMS | UNITS | TOTAL QUANTITY | ROADWAY 80% FEDERAL 20% LOCAL 0004 | BRIDGE 80% FEDERAL 20% LOCAL 0014 |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 199 | 199 | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 3 | 3 | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 12 | 12 | |
| 78005110 | EPOXY PAVEMENT MARKING - LINE 4" | FOOT | 710 | 710 | |
| 78200420 | GUARDRAIL MARKERS, TYPE B | EACH | 8 | 8 | |
| 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 3 | 3 | |
| LR631020 | TRAFFIC BARRIER TERMINAL, TYPE 1 | EACH | 3 | 3 | |
| X0301852 | DEWATERING STRUCTURE NO. 1 | EACH | 2 | | 2 |
| X5030305 | CONCRETE WEARING SURFACE, 5" | SO YD | 200 | | 200 |
| X5860110 | GRANULAR BACKFILL FOR STRUCTURES | CU YD | 99.8 | | 99.8 |
| X7010216 | TRAFFIC CONTROL AND PROTECTION (SPECIAL) | L SUM | 1 | 1 | |
| Z0004522 | HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 6" | SO YD | 20 | 20 | |
| Z0004556 | HOT-MIX ASPHALT SURFACE REMOVAL (DECK) | SO YD | 200 | | 200 |
| Z0012754 | STRUCTURAL REPAIR OF CONCRETE (DEPTH LESS THAN OR EQUAL TO 5 INCHES) | SO FT | 36 | | 36 |
| Z0012755 | STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES) | SO FT | 72 | | 72 |
| Z0013798 | CONSTRUCTION LAYOUT | L SUM | 1 | 1 | |
| Z0030850 | TEMPORARY INFORMATION SIGNING | SO FT | 100 | 100 | |

*SPECIALTY ITEM

COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 DATE PLOTTED: 11/1/2013 10:21:53 AM
 FILE NAME: 86120386-500-01.dgn
 PLOT DRIVER: pdfLdt
 PEN TABLE: standard-11rns.tbl

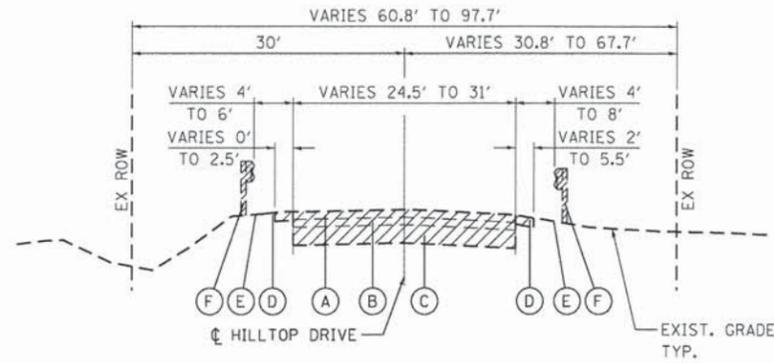


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| FILE NAME = 86120386-500-01.dgn | DRAWN - WJH | REVISED - |
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| PLOT DATE = 11/1/2013 | DATE - 11/1/13 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

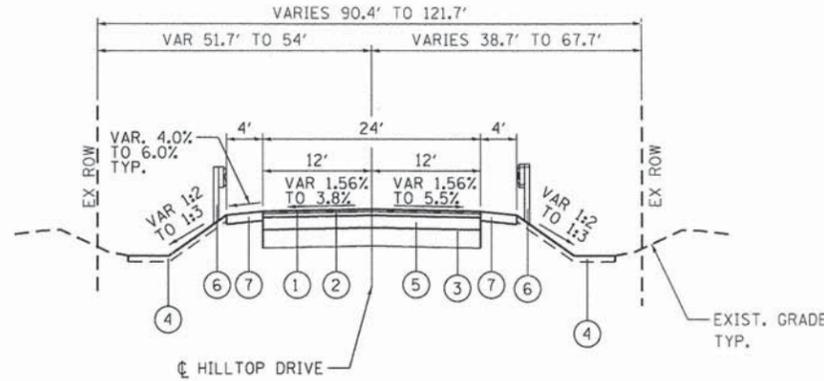
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| SUMMARY OF QUANTITIES HILLTOP DRIVE OVER WOODS CREEK | | | |
| SCALE: N.T.S. | SHEET NO. OF | SHEETS | STA. TO STA. |

| | | | | |
|--------------------|------------------------|----------------|-----------------|---|
| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY McHENRY | TOTAL SHEETS 24 | SHEET NO. 4 |
| CONTRACT NO. 63857 | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT |



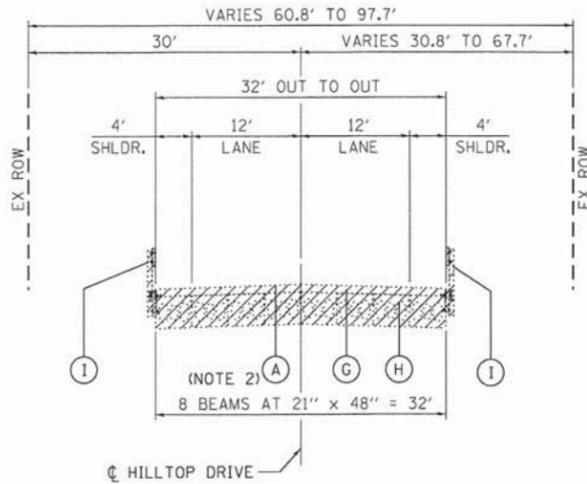
EXISTING APPROACH ROADWAY TYPICAL SECTION

STA. 8+50.00 TO STA. 9+30.00
STA. 9+86.00 TO STA. 10+27.00



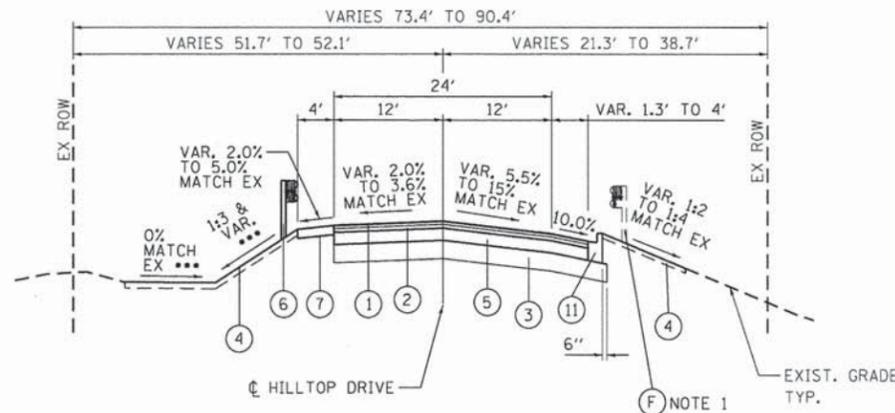
PROPOSED APPROACH ROADWAY TYPICAL SECTION

STA. 9+10.62 TO STA. 9+30.00
STA. 9+86.00 TO STA. 10+27.00



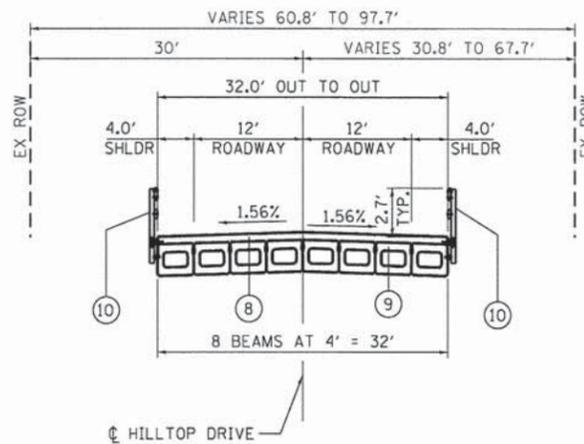
EXISTING BRIDGE CROSS SECTION

STA. 9+30.00 TO STA. 9+86.00



PROPOSED APPROACH ROADWAY TYPICAL SECTION

STA. 8+50.00 TO STA. 9+10.62
*** SEE CROSS SECTIONS FOR ADDITIONAL INFORMATION



PROPOSED BRIDGE CROSS SECTION

STA. 9+30.00 TO STA. 9+86.00

EXISTING LEGEND

- (A) EX HMA SURFACE (±5 1/4" THICKNESS)
- (B) EX HMA BINDER (THICKNESS VARIES 3" TO 5")
- (C) EX AGGREGATE BASE/SUBBASE (±7" THICKNESS)
- (D) EX AGGREGATE SHOULDER
- (E) EX GRASS SHOULDER
- (F) EX STEEL PLATE BEAM GUARDRAIL
- (G) EX HMA BRIDGE WEARING SURFACE (THICKNESS VARIES 3 1/2" TO 9")
- (H) EX PPC DECK BEAMS (21" DEPTH)
- (I) EX STEEL RAILING, TYPE S-1

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE MIX "D" N50 @ 2"
- (2) HOT-MIX ASPHALT BINDER COURSE IL - 19.0, N50 @ 2 1/2"
- (3) AGGREGATE SUBGRADE IMPROVEMENT 12"
- (4) TOPSOIL FURNISH & PLACE 4" WITH SEEDING CLASS 2A
- (5) HOT-MIX ASPHALT BASE COURSE, 7 1/2"
- (6) STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS
- (7) AGGREGATE SHOULDERS, TYPE B 6"
- (8) CONCRETE WEARING SURFACE, 5"
- (9) PPC DECK BEAMS (21" DEPTH)
- (10) STEEL RAILING, TYPE SM
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- PAID FOR UNDER HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 12"
- REMOVAL PAID FOR UNDER REMOVAL OF EXISTING SUPERSTRUCTURE

INDICATES ITEMS TO BE REMOVED

NOTES:

1. EX TO REMAIN. STA. 8+50.00 TO STA. 8+78.81
2. REMOVAL PAID FOR UNDER HOT-MIX ASPHALT SURFACE REMOVAL (DECK)

| HOT-MIX ASPHALT REQUIREMENTS | |
|--|------------------------------|
| MIXTURE TYPE | AIR VOIDS @ N _{DES} |
| FULL DEPTH PAVEMENT | |
| HOT-MIX ASPHALT SURFACE COURSE MIX "D" N50 (IL-9.5 mm) (2") | 4% @ 50 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 (2 1/2") | 4% @ 50 Gyr. |
| HOT-MIX ASPHALT BASE COURSE, 7 1/2" (HMA BINDER IL-19 mm) (IN 3 LIFTS) | 4% @ 50 Gyr. |
| DRIVEWAY PAVEMENT | |
| HOT-MIX ASPHALT SURFACE COURSE MIX "D" N50 (IL-9.5 mm) (2") | 4% @ 50 Gyr. |

- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SO YD/IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

COMPANY NAME: Kevin M. Arff
PROJECT CONTACT: City of Aurora
CLIENT: 10/17/2013 9:54 AM
DATE PLOTTED: 10/17/2013 9:54 AM
PLOT DRIVER: standard-trans.tbl
PEN TABLE:



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| PLOT SCALE = N.T.S. | CHECKED - RGD | REVISED - |
| PLOT DATE = 10/17/2013 | DATE - 10/17/13 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| TYPICAL SECTIONS | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|--|---------------------|----------------|--------------|--------------------|-----------|
| HILLTOP DRIVE OVER WOODS CREEK | | 4036 | 11-00036-00-BR | McHENRY | 24 | 5 |
| SCALE: N.T.S. | | SHEET NO. OF SHEETS | | STA. TO STA. | CONTRACT NO. 63857 | |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | | | |

PROP. CURVE HILLTOP-1
 PI STA. = 7+76.23
 $\Delta = 49^\circ 58' 59''$ (RT)
 $D = 25^\circ 27' 53''$
 $R = 225.00'$
 $T = 104.88'$
 $L = 196.28'$
 $E = 23.24'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. \text{ RUN} = \text{---}$
 $P.C. \text{ STA} = 6+78.09$
 $P.T. \text{ STA} = 8+74.37$

PROP. CURVE HILLTOP-2
 PI STA. = 11+33.86
 $\Delta = 50^\circ 50' 55''$ (LT)
 $D = 28^\circ 38' 52''$
 $R = 200.00'$
 $T = 95.07'$
 $L = 177.50'$
 $E = 21.45'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. \text{ RUN} = \text{---}$
 $P.C. \text{ STA} = 10+45.12$
 $P.T. \text{ STA} = 12+22.61$

LINE L1
 LENGTH = 170.75
 BEARING = $N26^\circ 09' 21'' E$
 BEGIN STA. = 8+74.37
 END STA. = 10+45.12

SET PK NAIL
 PC STA. 8+00.00
 N: 2007711.566
 E: 988720.620

SET PK NAIL
 PC STA. 9+00.00
 N: 2007805.486
 E: 988753.174

SET PK NAIL
 PC STA. 10+00.00
 N: 2007895.246
 E: 988797.255

SET PK NAIL
 PC STA. 11+00.00
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 E: 988834.317

SET PK NAIL
 PCC STA. 12+22.61
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 E: 988819.335

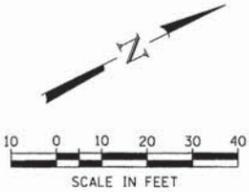
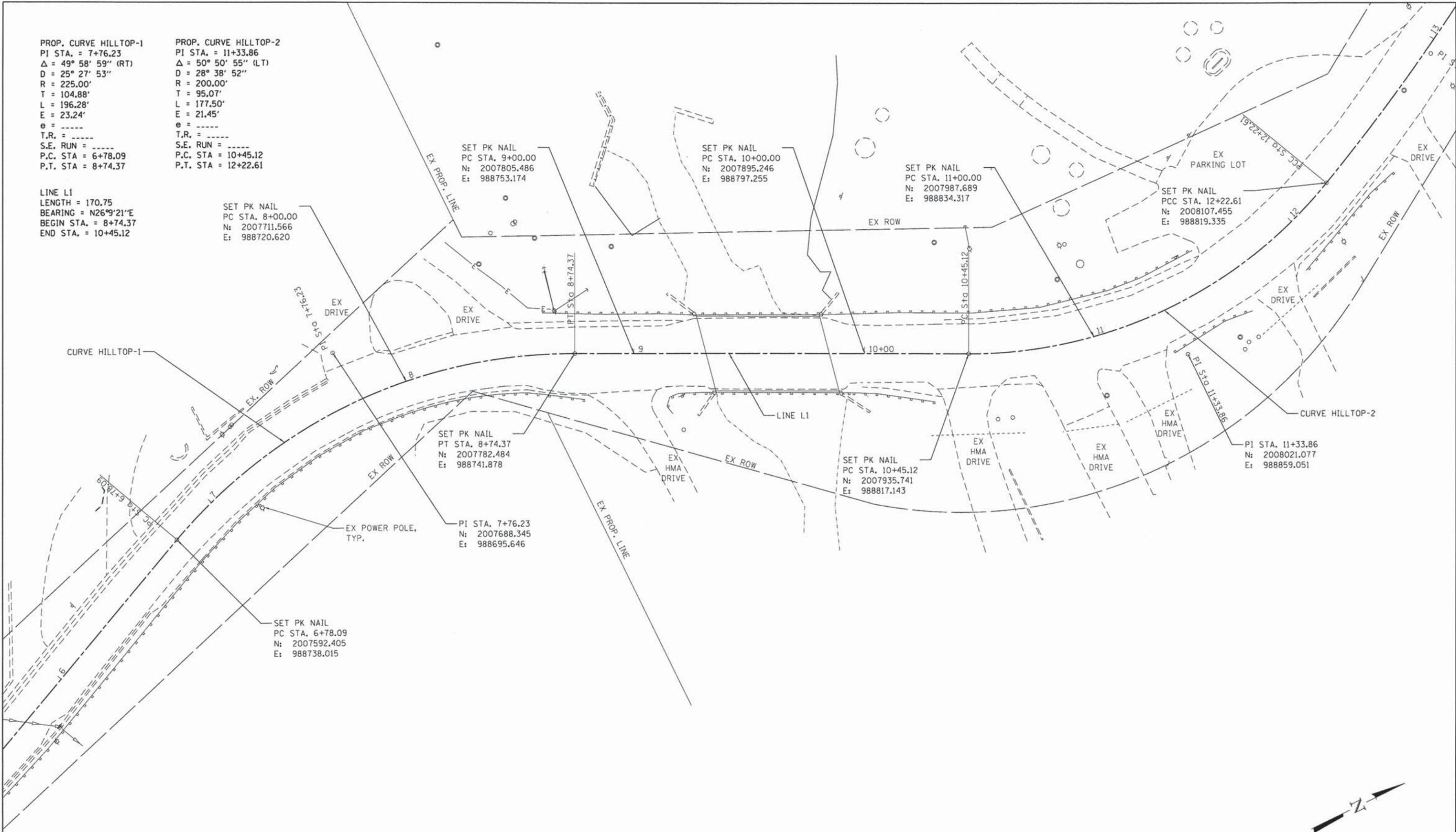
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 PT STA. 8+74.37
 N: 2007782.484
 E: 988741.878

SET PK NAIL
 PC STA. 10+45.12
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 E: 988817.143

PI STA. 7+76.23
 N: 2007688.345
 E: 988695.646

PI STA. 11+33.86
 N: 2008021.077
 E: 988859.051

SET PK NAIL
 PC STA. 6+78.09
 N: 2007592.405
 E: 988738.015



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: Kevin M. Arff
 CLIENT: City of Aurora
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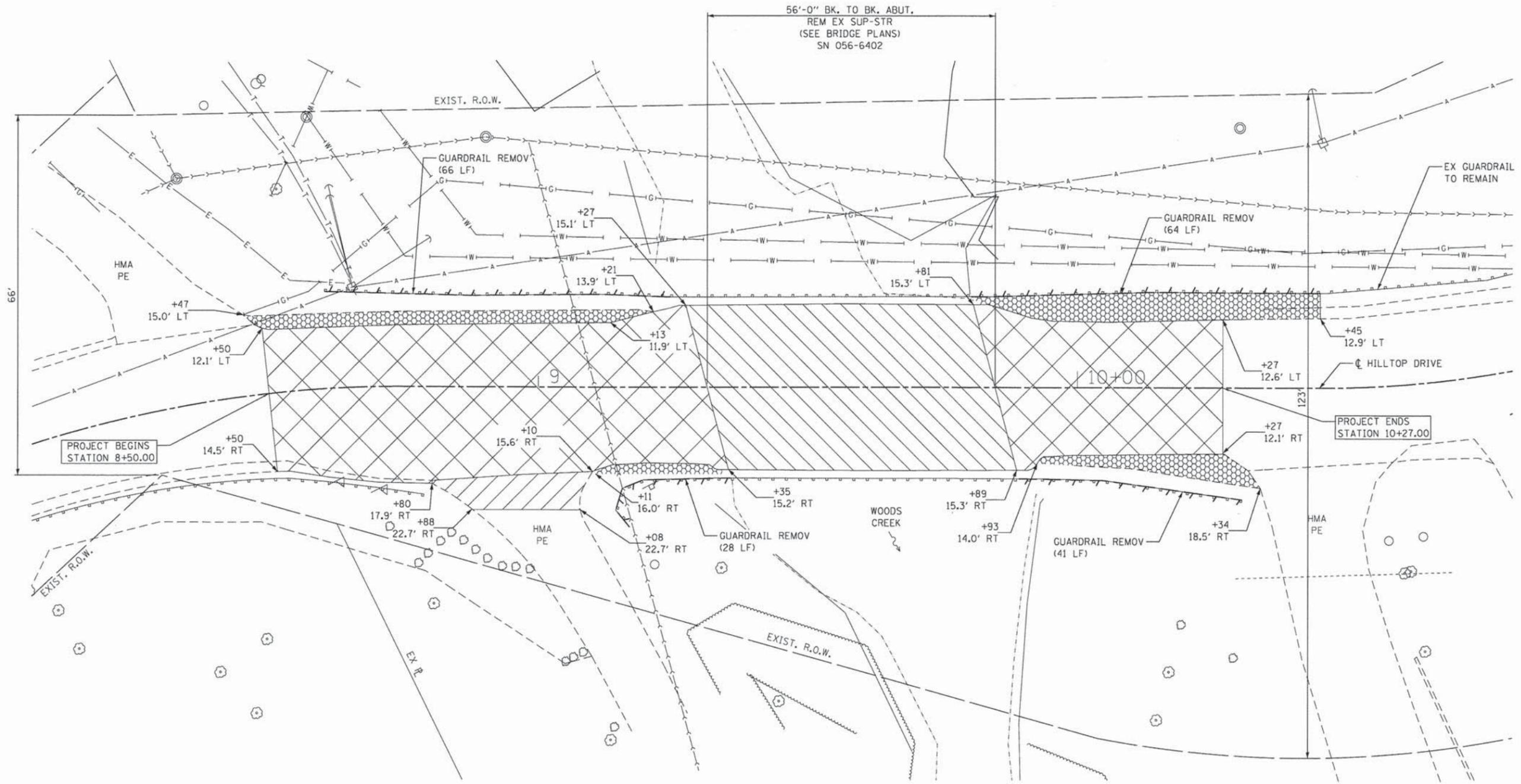


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| PLOT DATE = 10/17/2013 | DATE - 10/17/13 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

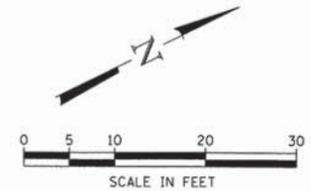
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| ALIGNMENT DETAIL | | | |
| HILLTOP DRIVE OVER WOODS CREEK | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. TO STA. |

| | | | | |
|---|------------------------|----------------|-----------------|--------------------|
| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY McHENRY | TOTAL SHEETS 24 | SHEET NO. 6 |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | CONTRACT NO. 63857 |



LEGEND

- PAVEMENT REMOVAL
- AGGREGATE OR GRASS SHOULDER REMOVAL (PAID AS REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL)
- DRIVEWAY PAVEMENT REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL (DECK)
- LINEAR REMOVAL ITEMS (GUARDRAIL REMOV)



COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 CLIENT: ID/17/2013 9:55:49 AM
 DATE PLOTTED: 86120386-Rem-01.dgn
 FILE NAME: pdf.DET-11f.dwg
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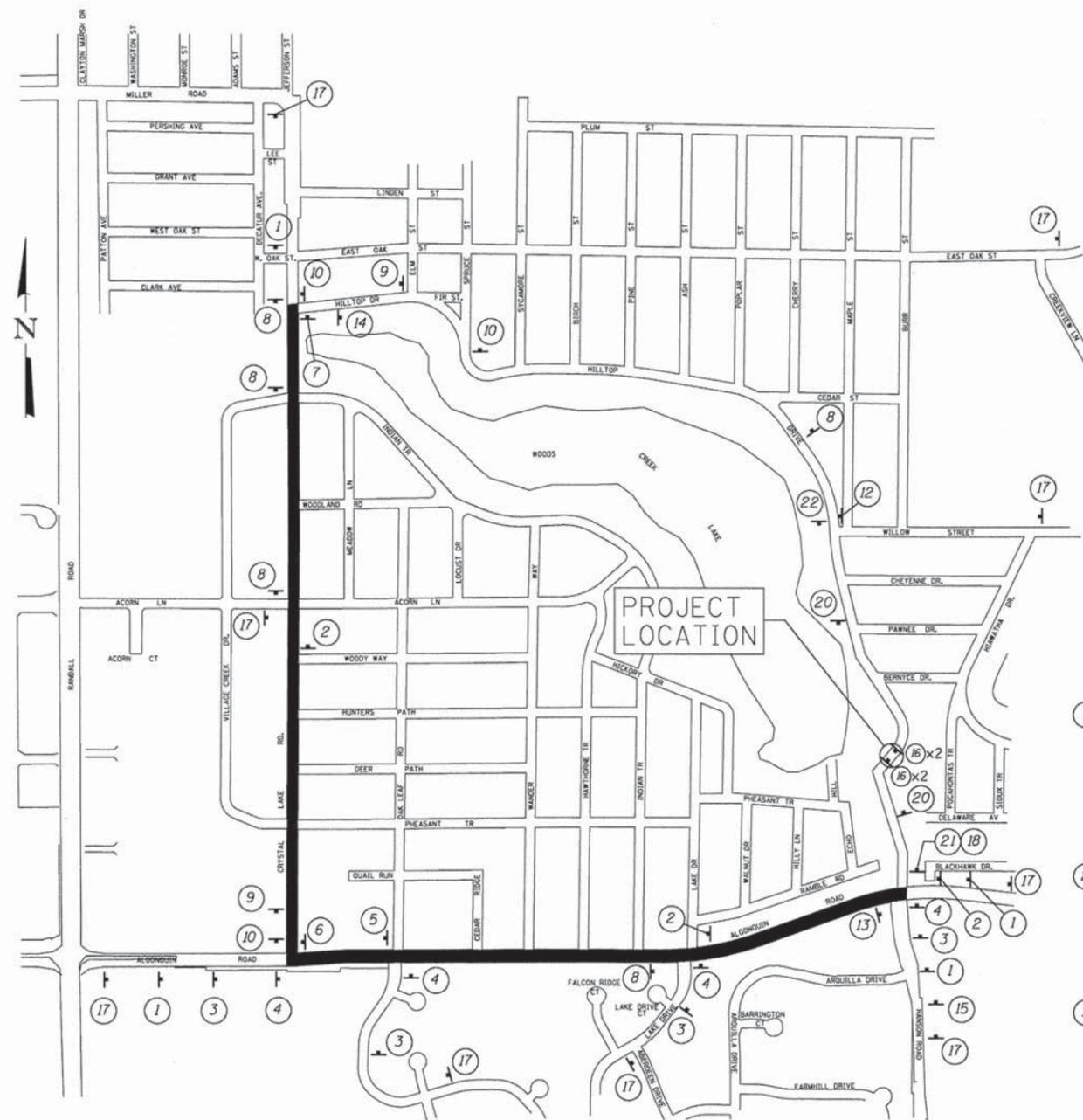
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | |
|--|---|
| REMOVAL PLAN HILLTOP DRIVE OVER WOODS CREEK | |
| SCALE: 1"=10' | SHEET NO. 1 OF 1 SHEETS STA. 8+05 TO STA. 10+81 |

| | | | | |
|---|------------------------|----------------|-----------------|-------------|
| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY MCHENRY | TOTAL SHEETS 24 | SHEET NO. 7 |
| CONTRACT NO. 63857 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

DETOUR GENERAL NOTES

1. THE ROAD SHALL NOT BE CLOSED UNTIL ALL SIGNING HAS BEEN ERECTED IN ACCORDANCE WITH THE DETOUR PLAN.
2. THE ENGINEER, VILLAGE OF LAKE IN THE HILLS POLICE DEPARTMENT, AND THE MCHENRY COUNTY DIVISION OF TRANSPORTATION SHALL BE NOTIFIED IN WRITING AT LEAST TWO WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT.
3. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND AND BRIDGE CONSTRUCTION" ADOPTED JANUARY 1, 2012. THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," AND AS DIRECTED BY THE ENGINEER.
4. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL CONFORM TO "CONVENTIONAL ROAD" AS REQUIRED BY THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE.
5. THIS IS THE MINIMUM REQUIRED TRAFFIC CONTROL. ADDITIONAL SIGNING AND/OR BARRICADES DEEMED NECESSARY BY THE ENGINEER DUE TO CONTRACTOR OPERATIONS SHALL BE PROVIDED AND INSTALLED AND INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."
6. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE, (INCLUDING A 24-HR EMERGENCY NUMBER) AND HIS REPRESENTATIVES SHALL BE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK AT THE PRECONSTRUCTION CONFERENCE.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR MAY REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
8. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WITH THE DETOUR IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.
9. ALL DETOUR SIGNING SHALL BE POST MOUNTED.
10. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1084.02 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR IN LIKE-NEW CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY, INCLUDING SUNDAYS AND HOLIDAYS.
12. THE "ROAD CLOSED" (R11-2 SIGN SHALL BE MOUNTED ABOVE THE TOPS OF THE TYPE III BARRICADES. ALL TYPE III BARRICADES SHALL HAVE TWO AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINES OF THE SUPPORTS.
13. THE TYPE III BARRICADES USED AT POINTS OF CLOSURE TO THRU TRAFFIC ONLY SHALL NOT EXCEED 8 FEET IN WIDTH EACH FOR A SINGLE APPROACH LANE. ALL BARRICADES AT THESE LOCATIONS SHALL HAVE REFLECTORIZED STRIPING ON THE BACK SIDES OF THE BARRICADES.
14. CONSTRUCTION EQUIPMENT SHALL NOT BE PARKED IMMEDIATELY BEHIND THE TYPE III BARRICADES DURING NON-WORKING HOURS. IN ANY EVENT ARTICLE 701.04 OF THE STANDARDS SPECIFICATIONS SHALL APPLY.
15. DURING NON-WORKING HOURS THE CONTRACTOR SHALL PROVIDE A MEANS TO RESTRAIN THE TYPE III BARRICADES FROM EASY MOVEMENT BY VANDALS. THE CHOSEN METHOD SHALL BE APPROVED BY THE ENGINEER.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNS, INCLUDING CUTTING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER. COST INCLUDED UNDER "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)."
17. THE ENGINEER SHALL BE NOTIFIED AT LEAST 24 HOURS BEFORE THE ROAD IS TO BE REOPENED TO TRAFFIC. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
18. ALL WORK REQUIRED FOR DETOUR SIGNING SHALL BE PAID FOR UNDER "TRAFFIC CONTROL AND PROTECTION."
19. THE CONTRACTOR IS MADE AWARE THAT ALGONQUIN ROAD IS UNDER THE JURISDICTION OF THE MCHENRY COUNTY DIVISION OF TRANSPORTATION (MCDOT). THE CONTRACTOR SHALL CONTACT THE MCDOT AT LEAST 48 HOURS IN ADVANCE OF PLACING ANY CONSTRUCTION SIGNAGE WITHIN MCDOT RIGHT-OF-WAY. DETOUR SIGNAGE SHALL NOT BE INSTALLED ON ANY MCDOT STREET LIGHT POLES OR SIGNAL POLES. DETOUR SIGNAGE SHALL NOT OBSTRUCT ANY MCDOT SIGNS AND CANNOT USE THE SAME SIGN POSTS. SIGNS ON MCDOT ROADS MUST BE INSTALLED AT LEAST 12 FEET FROM THE EDGE OF THE MAINLINE ROADWAY (EDGE LINE).



SIGN LEGEND

| | | | |
|----|--|----|--|
| 1 | | 6 | |
| 2 | | 7 | |
| 3 | | 8 | |
| 4 | | 9 | |
| 5 | | 10 | |
| 11 | | 12 | |
| 13 | | 14 | |
| 15 | | 16 | |
| 17 | | 18 | |
| 19 | | 20 | |
| 21 | | 22 | |
| 23 | | 24 | |

LEGEND

PROPOSED DETOUR ROUTE

COMPANY NAME: Kevin M. Arft
 PROJECT CONTACT: City of Aurora
 CLIENT: 11/1/2013 10:27:35 AM
 DATE PLOTTED: 86120386-01r.dgn
 FILE NAME: pefcpl
 PLOT DRIVER: standard-trans.tbl
 PEN TABLE:

HRGreen.com
 Illinois Professional Design Firm
 #184-001322

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| PLOT SCALE = N.T.S. | CHECKED - RGD | REVISED - |
| PLOT DATE = 11/1/2013 | DATE - 11/1/13 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY DETOUR PLAN
 HILLTOP DRIVE OVER WOODS CREEK

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|------------------------|----------------|--------------------|-------------|
| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY McHENRY | TOTAL SHEETS 24 | SHEET NO. 9 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 63857 | |

EROSION CONTROL NOTES

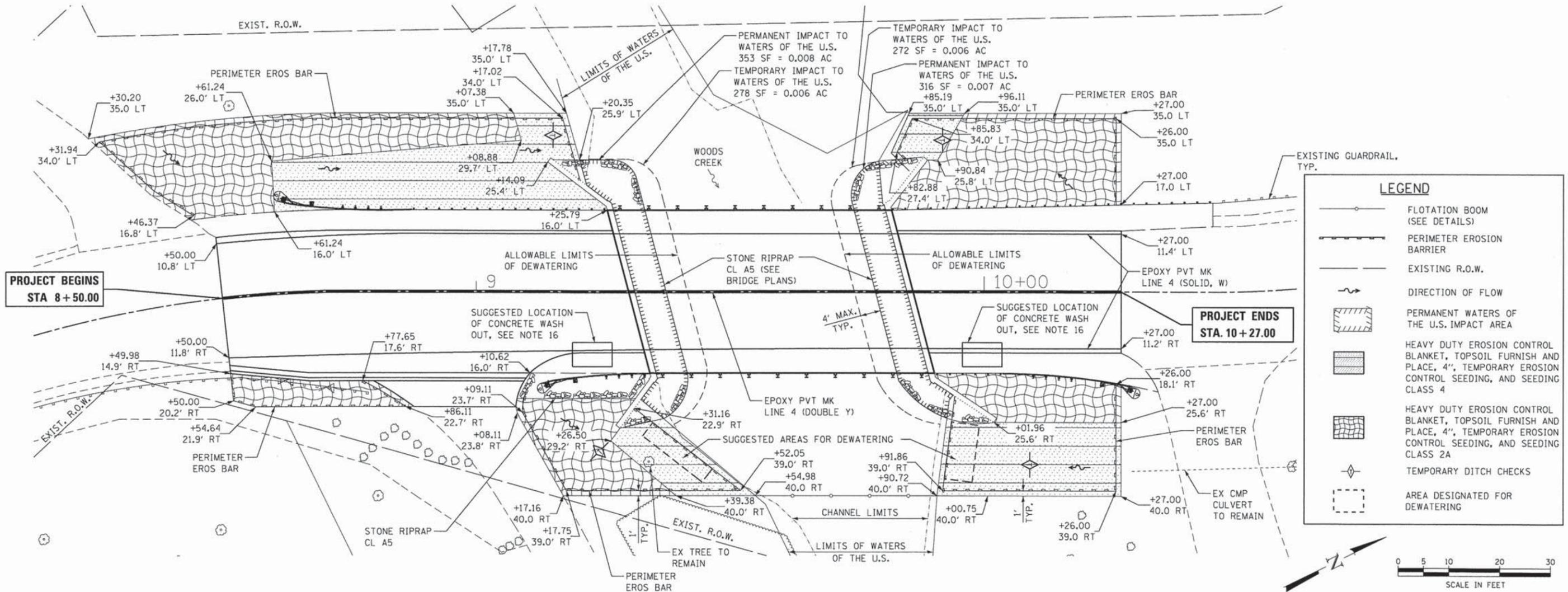
1. THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN EROSION CONTROL MEASURES PRIOR TO EARTHWORK.
2. STOCKPILES OF SOIL AND OTHER BUILDING MATERIALS TO REMAIN IN PLACE MORE THAN SEVEN (7) DAYS SHALL BE FURNISHED WITH EROSION AND SEDIMENT CONTROL MEASURES (I.E. PERIMETER SILT FENCE). STOCKPILES TO REMAIN IN PLACE FOR 14 DAYS OR MORE SHALL RECEIVE TEMPORARY SEEDING.
3. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PREVENT POLLUTION OF STORM WATER AND SHALL FOLLOW IEPA & IDOT CONSTRUCTION MEMORANDUM NO. 06-60.
4. STABILIZATION MEASURES SHALL BE INITIATED WITHIN 7 DAYS OF CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASING IN AREAS WHERE IT WILL NOT OCCUR FOR A PERIOD OF 14 OR MORE CALENDAR DAYS.
5. THE CONTRACTOR SHALL APPLY TEMPORARY EROSION CONTROL SEEDING TO ALL ERODIBLE BARE EARTH AREAS WITHIN THE CONTRACT LIMITS EACH WEEK, REGARDLESS OF WEATHER CONDITIONS OR PROGRESS OF THE WORK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ERODIBLE EMBANKMENT AND EXCAVATION AREAS WHERE WORK IS IN PROGRESS SHALL BE INCLUDED ON THE AREAS TO BE SEEDDED. SEE BDE SPECIAL PROVISION FOR TEMPORARY EROSION CONTROL.
6. THE SOIL AND WATER CONSERVATION DISTRICT IS RESPONSIBLE FOR CONDUCTING SITE VISITS AND VERIFYING THAT THE PRACTICES ARE WORKING PROPERLY AND DETERMINE IF ADDITIONAL PRACTICES ARE NEEDED FOR BETTER SOIL EROSION AND SEDIMENT CONTROL. IF ADDITIONAL PRACTICES ARE DEEMED NECESSARY BY THE ENGINEER OR THE SWCD THE CONTRACTOR WILL IMPLEMENT THE PRACTICES IN A TIMELY MANNER.
7. THE MCHENRY-LAKE SOIL AND WATER CONSERVATION DISTRICT MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES, AND TWO WEEKS PRIOR TO FINAL INSPECTION.

8. THE SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSPECTED WEEKLY AND AFTER 1/2 INCH OF RAIN OR MORE BY THE INDIVIDUAL ON SITE IN CHARGE OF SOIL EROSION AND SEDIMENT CONTROL DURING THE CONSTRUCTION OF THE PROJECT. THE ENGINEER WILL BE RESPONSIBLE FOR THE EROSION AND SEDIMENT CONTROL INSPECTIONS.
9. HEAVY DUTY EROSION CONTROL BLANKET SHALL BE INSTALLED TO ALL DISTURBED AREAS WITH SLOPES EQUAL TO OR GREATER THAN 5H:1V AND IN CRITICAL AREAS (I.E. DETENTION BASIN PERIMETERS, STREAMBANKS, BERMS, ETC.) IMMEDIATELY UPON FINAL GRADING.
10. PERIMETER EROSION BARRIER SHALL BE INSTALLED PRIOR TO EARTH WORK AND WILL REMAIN IN PLACE UNTIL THE CONTRIBUTING AREA IS STABILIZED.
11. ALL ADJACENT STREETS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY AND CLEANED WHEN NECESSARY OR DIRECTED BY THE ENGINEER.
12. ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL.
13. ALL MATERIALS USED FOR TEMPORARY CONSTRUCTION ACTIVITIES WILL BE REMOVED TO UPLAND AREAS IMMEDIATELY FOLLOWING COMPLETION OF THE CONSTRUCTION ACTIVITY EACH DAY.
14. A STAMPED AND SIGNED COPY OF THE APPROVED SOIL EROSION AND SEDIMENT CONTROL PLAN SHALL BE MAINTAINED ON THE SITE AT ALL TIMES AND BE PRESENTED WHEN REQUESTED BY MCHENRY-LAKE SWCD, U.S. ARMY CORP OF ENGINEERS OR ANY OTHER AUTHORIZED AGENCY.
15. SEEDING MIXTURES USED SHALL BE CLASS 2A AND CLASS 4 AS DETAILED IN SECTION 250 OF THE STANDARD SPECIFICATIONS. PLANTING TIMES ARE LIMITED TO APRIL 1 TO JUNE 15 AND AUGUST 1 TO NOVEMBER 1. FOR CLASS 2A; MAY 15 TO JUNE 30 AND OCTOBER 15 TO DECEMBER 1 FOR CLASS 4.
16. THE CONTRACTOR WILL BE REQUIRED TO HAVE A DESIGNATED CONCRETE WASH OUT AREA DURING ALL CONCRETE POURS. LOCATION TO BE COORDINATED BETWEEN THE CONTRACTOR AND ENGINEER.
17. THE COST FOR DEWATERING, FILTERING AND AGENCY COORDINATION SHALL BE INCLUDED IN THE COST OF "DEWATERING, STRUCTURE NO. 1."

18. OTHER THAN TO INSTALL AND REMOVE THE METHOD OF DIVERSION/DEWATERING, NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR WOODS CREEK SHALL BE ISOLATED FROM CREEK FLOWS AND DEWATERED PRIOR TO THE COMMENCEMENT OF WORK. THE DIVERSION/ISOLATION OF THE CREEK FLOWS MUST BE CONSTRUCTED FROM NON-ERODIBLE MATERIALS (STEEL SHEETS, AQUA BARRIERS, ETC.). EARTHEN DIVERSIONS ARE NOT ALLOWED.
19. THE MCHENRY-LAKE COUNTY SOIL AND WATER CONSERVATION DISTRICT AND THE ARMY CORPS OF ENGINEERS MUST BE IN AGREEMENT WITH METHOD OF DIVERSION/ISOLATION, DEWATERING AND FILTERING PRIOR TO THE COMMENCEMENT OF CONSTRUCTION, ONCE WORK IN THIS AREA BEGINS, PRIORITY SHALL BE GIVEN TO THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF ALL DISTURBED AREAS.
20. IF BYPASS PUMPING IS NECESSARY, THE PUMP SHALL BE PLACED ON A STABLE SURFACE OR FLOATED TO PREVENT SEDIMENT FROM BEING SUCKED INTO THE HOSE. THE OUTLET SHALL BE PLACED ON A NON-ERODIBLE, ENERGY DISSIPATING SURFACE PRIOR TO REJOINING THE STREAM FLOW AND SHALL NOT CAUSE EROSION OF DOWNSTREAM AREAS. CLEANING OR FILTERING OF BYPASS WATER IS NOT NECESSARY UNLESS OTHERWISE REQUIRED.
21. WHEN DEWATERING THE CONSTRUCTION AREA, ALL WATER MUST BE FILTERED PRIOR TO REJOINING THE STREAM FLOW. DEWATERING METHODS SHALL BE CHOSEN BASED ON SITE CONDITIONS, CONSTRAINTS AND SEDIMENT LOADS. POSSIBLE OPTIONS FOR SEDIMENT REMOVAL INCLUDE BAFFLE SYSTEMS, ANIONIC POLYMERS, DEWATERING BAGS, OR OTHER METHODS APPROVED BY THE ENGINEER. DISCHARGE WATER IS CONSIDERED CLEAN IF IT DOES NOT RESULT IN A VISUALLY IDENTIFIABLE DEGRADATION OF WATER QUALITY TO THE SATISFACTION OF THE ENGINEER.
22. THE SIDE SLOPES MUST BE RESEEDED AND STABILIZED WITH AN APPROPRIATE HEAVY DUTY EROSION CONTROL BLANKET PRIOR TO ACCEPTING FLOWS. THE SUBSTRATE SHALL BE STABLE ENOUGH TO ACCEPT FLOWS AND SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER.
23. NO EQUIPMENT IS ALLOWED TO OPERATE IN THE WATER OR ON THE STREAMBED.

TOTAL PERMANENT IMPACT TO THE WATERS OF THE U.S. = 0.015 AC

TOTAL TEMPORARY IMPACT TO THE WATERS OF THE U.S. = 0.012 AC



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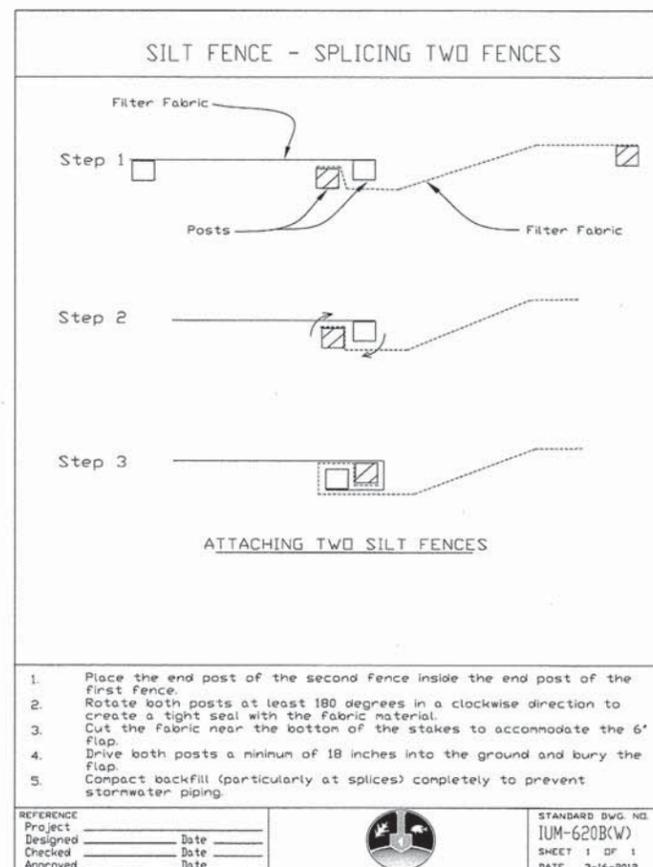
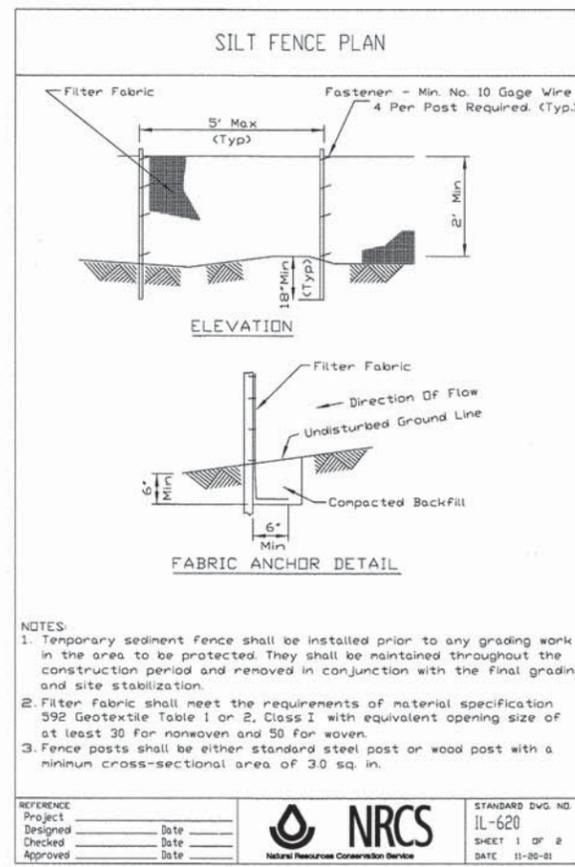
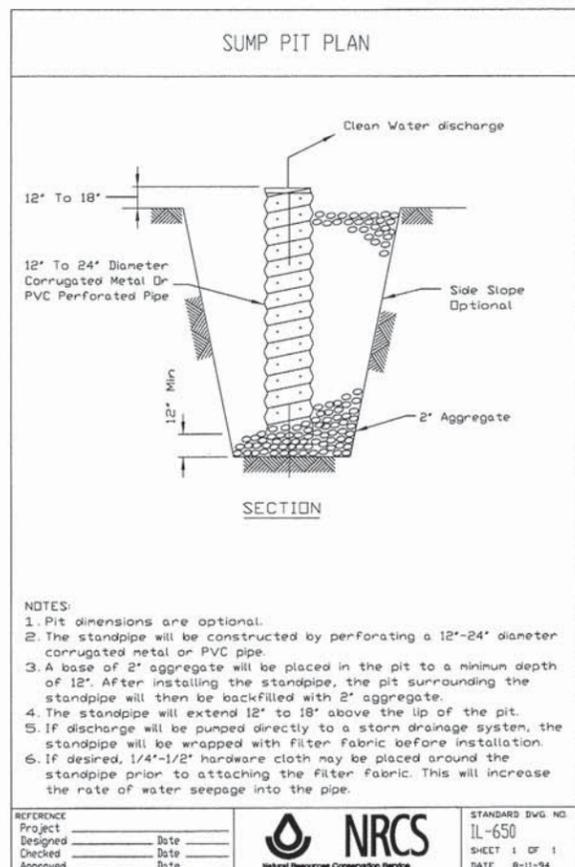
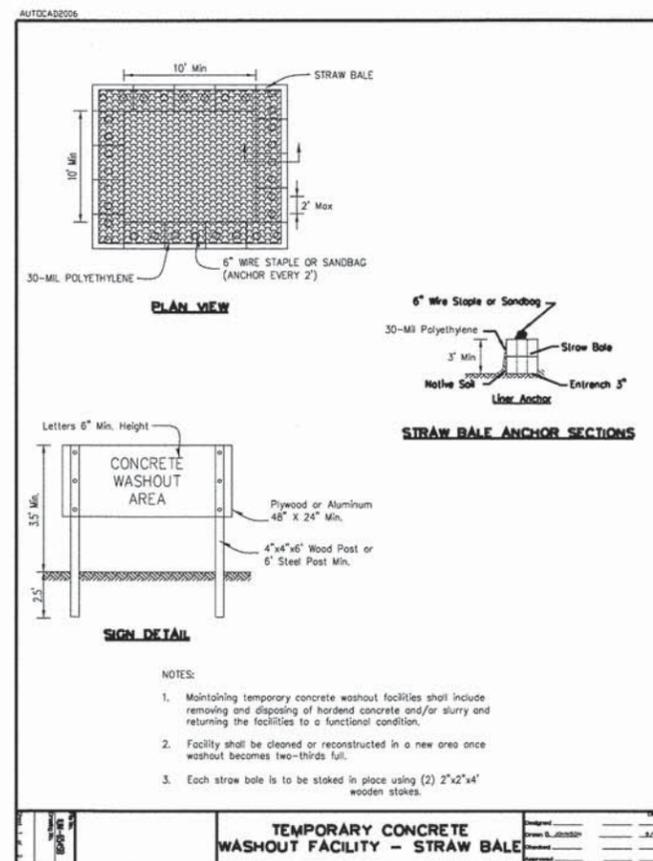
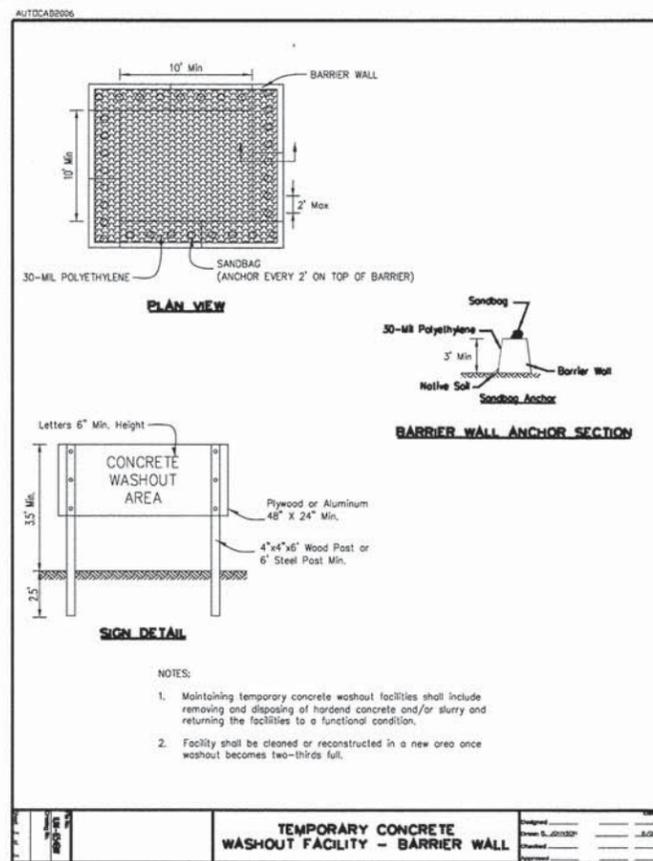
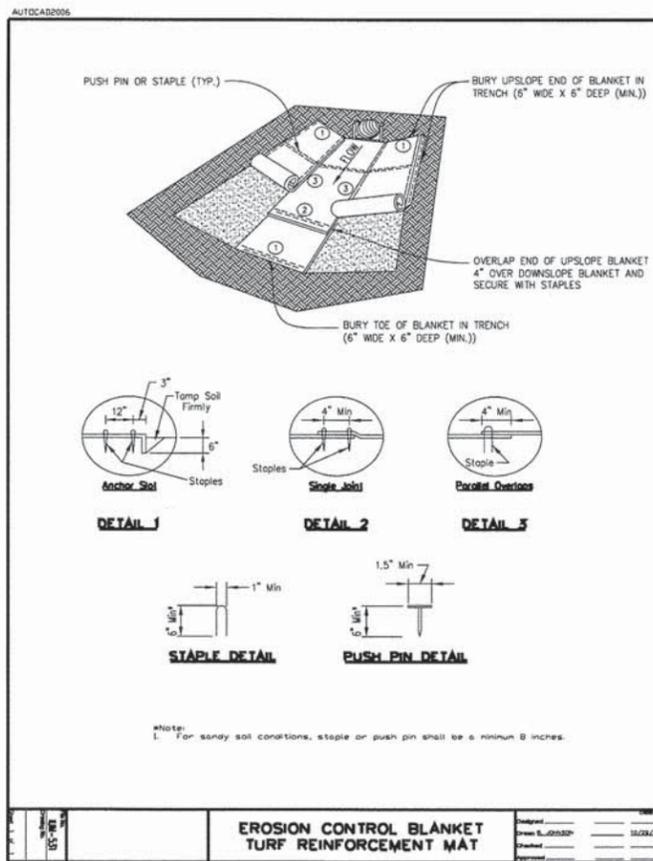
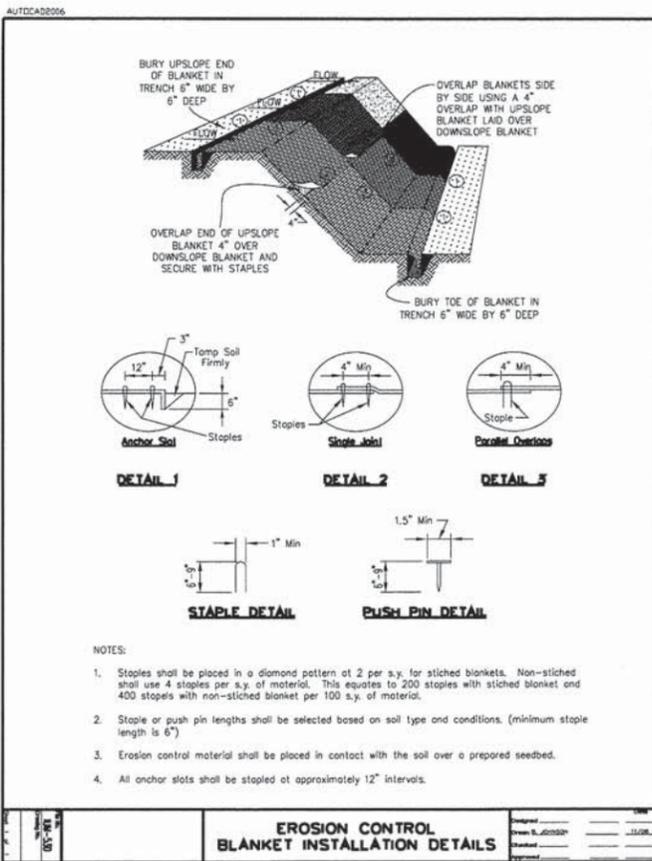
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EROSION CONTROL, PAVEMENT MARKING AND RESTORATION PLAN
 HILLTOP DRIVE OVER WOODS CREEK

SCALE: 1"=10' SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|------------------------|----------------|--------------------|--------------|
| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY McHENRY | TOTAL SHEETS 24 | SHEET NO. 10 |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | CONTRACT NO. 63857 | |



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 #184-001322

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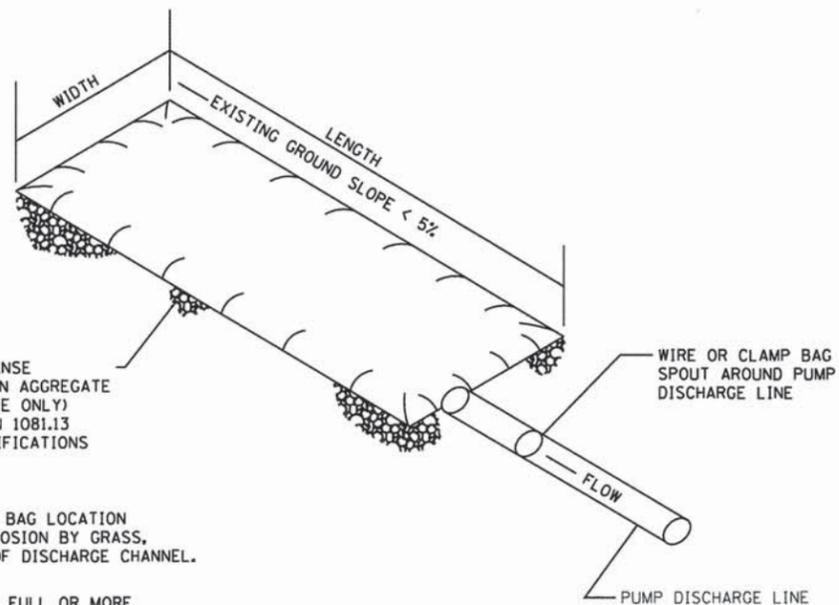
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EROSION CONTROL DETAILS
 HILLTOP DRIVE OVER WOODS CREEK

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

| | | | | |
|---|----------------|---------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 4036 | 11-00036-00-BR | McHENRY | 24 | 11 |
| CONTRACT NO. 63857 | | | | |
| FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT | | | | |



PLACE FILTERBAG ON DENSE MATURE GRASS OR ON AN AGGREGATE BASE (COARSE AGGREGATE ONLY) CONFORMING TO SECTION 1081.13 OF THE STANDARD SPECIFICATIONS

ENSURE THAT RUNOFF FROM THE BAG LOCATION IS PREVENTED FROM CAUSING EROSION BY GRASS, STONE, OR GEOTEXTILE LINING OF DISCHARGE CHANNEL.

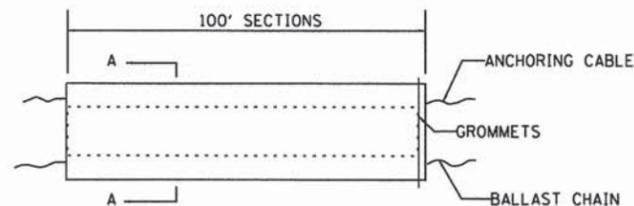
REMOVE SILT WHEN BAG IS HALF FULL OR MORE OFTEN IF RECOMMENDED BY THE MAUFACTURER. DISPOSE OF SILT BY TRANSPORTING THE FILLED BAG TO DISPOSAL AREA. SLIT THE BAG, BLEND SILT INTO EXISTING TOPOGRAPHY, SEED AND MULCH

MAINTAIN EXTRA FILTER BAGS ON SITE. REPLACE DAMAGED BAGS PROMPTLY.

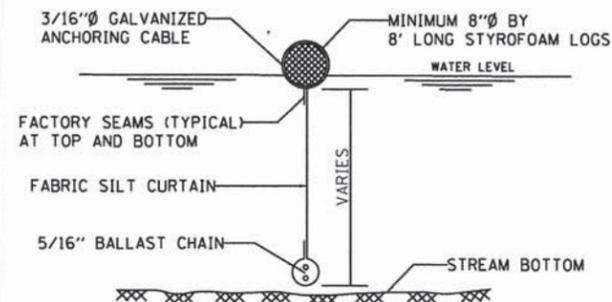
TYPICAL FLOW RATE: 10 GPM x W x L

NOTE: DEWATERING SYSTEM DETAILS SHALL BE USED IN CONJUNCTION WITH THE PROJECT SPECIAL PROVISIONS.

FIGURE S1. SEDIMENT FILTER BAG



ELEVATION



SECTION A-A

FLOTATION BOOM

NOTES:

1. FLOTATION BOOM SHALL BE ANCHORED TO PREVENT DRIFT SHOREWARD OR DOWNSTREAM. ANCHORAGES SHALL BE INSTALLED ON BOTH SHORE AND STREAM SIDE.
2. SHORE ANCHORS SHALL CONSIST OF A POST WITH DEADMAN OR APPROVED EQUAL. STREAM ANCHORS SHALL BE SUFFICIENT SIZE TO STABILIZE THE BARRIER WITH NUMBER AND SPACING DEPENDENT ON WATERWAY VELOCITIES.
3. FABRIC SECTIONS SHALL BE CONNECTED END TO END WITH MINIMUM 5/8" DIAMETER POLYPROPYLENE ROPE
4. DESIGN OF BOOM AND ANCHORAGE SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMENDATIONS. BOTTOM OF BOOM SHALL REACH BOTTOM OF WATERWAY USING ONE OR TWO VERTICAL SECTIONS AS REQUIRED.
5. MAINTENANCE SHALL BE PERFORMED AS NEEDED. CONTRACTOR SHALL REMOVE THE BOOM AT COMPLETION OF WORK IN A MANNER THAT WILL PREVENT SILTATION OF THE WATERWAY.
6. COST OF FLOTATION BOOM INCLUDED IN THE ITEM "DEWATERING - STRUCTURE NO. 1."

APPLICATION: FLOTATION BOOMS TO BE USED TO CONTROL EROSION, TURBIDITY AND DEBRIS WHEN WORKING IN WATERWAYS.

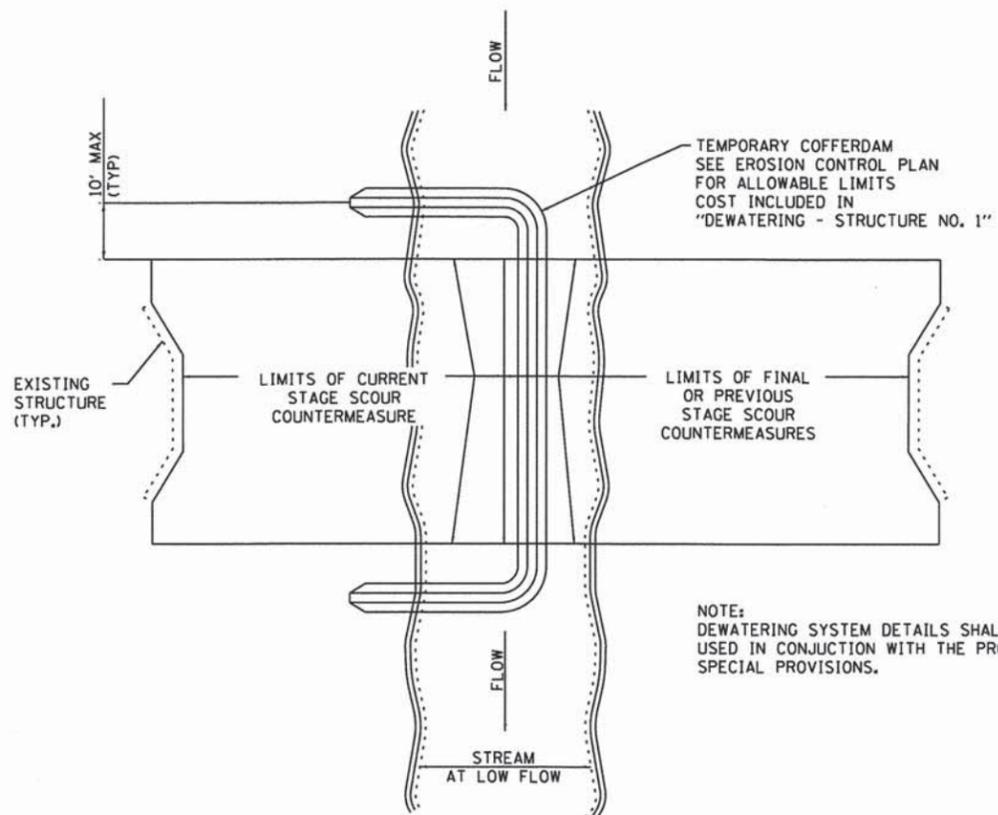


FIGURE D2. STAGED IN-STREAM COFFERDAM DIVISION

COMPANY NAME: HRGreen
 PROJECT CONTACT: Kevin M. Arff
 CITY: Chicago, IL
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
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| PLOT DATE = 10/17/2013 | DATE - 10/17/13 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

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|---|--|---------------------|----------------|--------------|--------------|--------------------|
| EROSION CONTROL DETAILS | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| HILLTOP DRIVE OVER WOODS CREEK | | 4036 | 11-00036-00-BR | McHENRY | 24 | 12 |
| SCALE: N.T.S. | | SHEET NO. OF SHEETS | | STA. TO STA. | | CONTRACT NO. 63857 |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | | | |

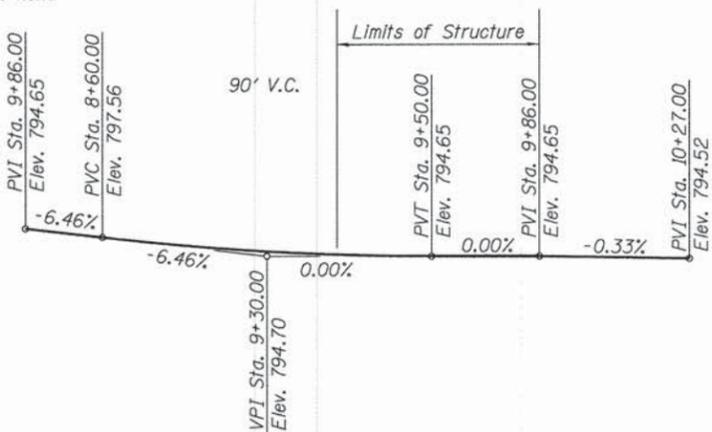
Benchmark 1:
Arrow bolt on top of hydrant located at the southwest corner of Hilltop Drive and Bernyce Drive. Elev. 799.71 (NAVD88)

Benchmark 2:
Chiseled square on southeast wing wall of the Hilltop Drive Bridge over Woods Creek. Elev. 794.19 (NAVD88)

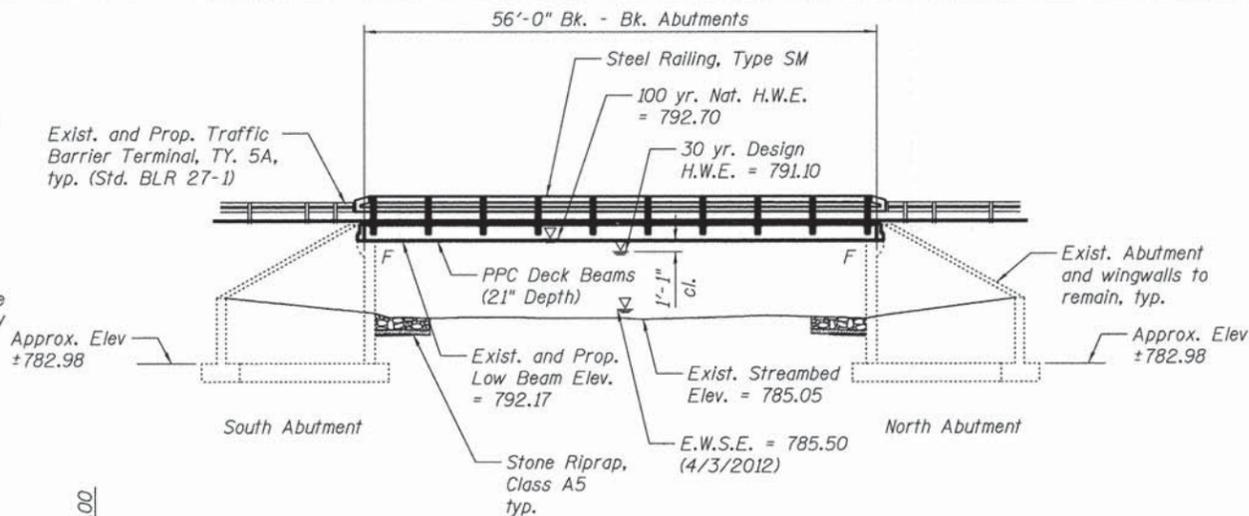
Existing SN: 056-6402

The existing structure built in 1984 by McHenry County consists of a single span precast, prestressed concrete deck beam bridge supported on closed abutments on spread footings with an out to out width of 32'-0" and a total length of 56'-0" along the centerline of the roadway. The design loading was HS20. Superstructure to be removed and replaced as shown. Road to be closed during construction. Traffic to be maintained with temporary detour route during construction.

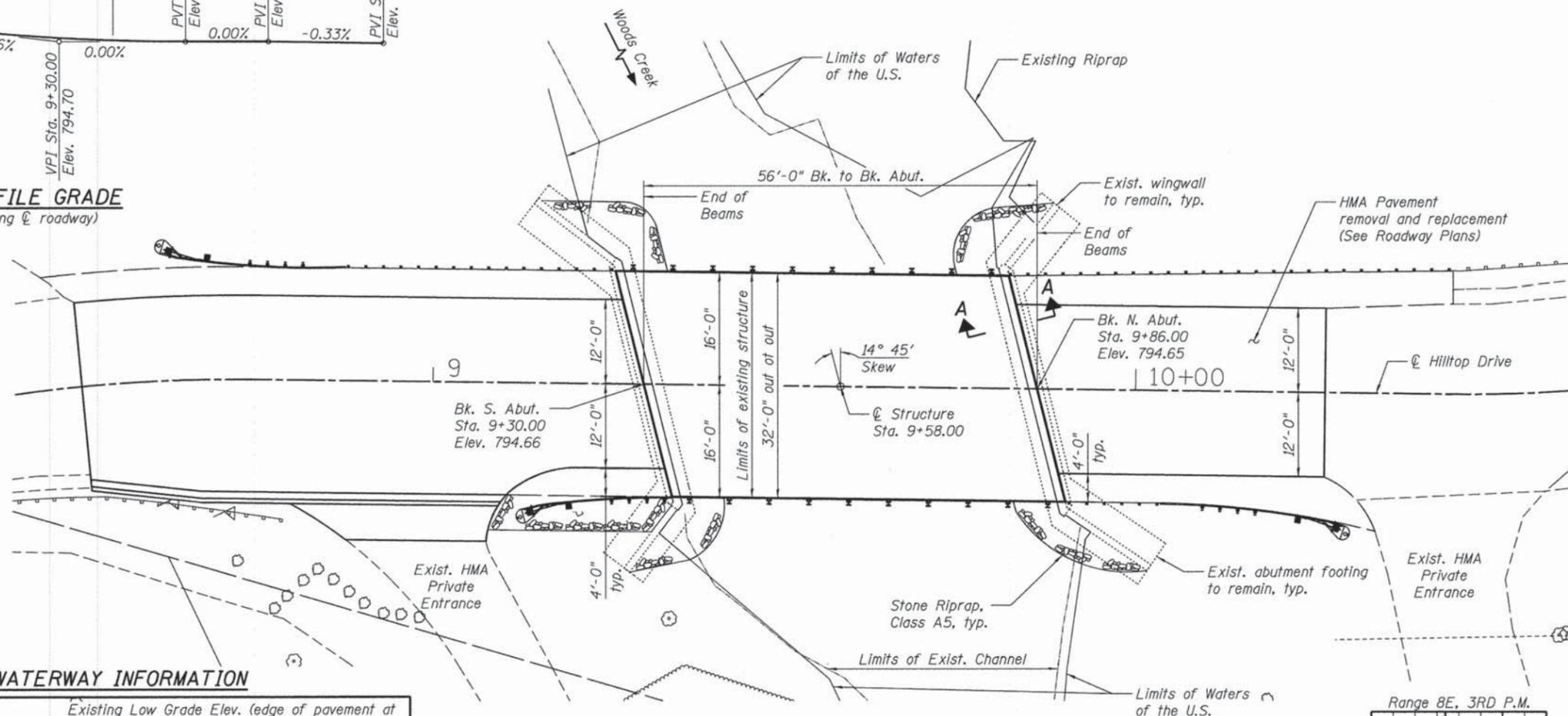
Salvage: None



PROFILE GRADE
(along centerline roadway)



ELEVATION



PLAN

WATERWAY INFORMATION

Existing Low Grade Elev. (edge of pavement at local sag) @ Sta. 11+84 = 792.71
Proposed Low Grade Elev. (edge of pavement at local sag) @ Sta. 11+84 = 792.71

| Flood | Freq. Yr. | Q C.F.S. | Opening Sq. Ft. | | Nat. H.W.E. | Head - Ft. | | Headwater El. | |
|-------------|-----------|----------|-----------------|-------|-------------|------------|-------|---------------|-------|
| | | | Exist. | Prop. | | Exist. | Prop. | Exist. | Prop. |
| Design | 10 | 736 | 262 | 262 | 789.8 | 1.3 | 1.3 | 791.1 | 791.1 |
| | 30 | 1,310 | 268 | 268 | 791.1 | 0.3 | 0.3 | 791.4 | 791.4 |
| | 50 | 1,589 | 295 | 295 | 791.7 | 0.4 | 0.4 | 792.1 | 792.1 |
| Overtopping | | | | | | | | 793.5 | 793.5 |
| Base | 100 | 2,141 | 322 | 322 | 792.7 | 0.8 | 0.8 | 793.5 | 793.5 |
| Max. Calc. | 500 | 3,802 | 322 | 322 | 795.3 | 1.0 | 1.0 | 796.3 | 796.3 |

Note:
See sheet S-2 for Section A-A and Index of Sheets



LOCATION SKETCH

DESIGN SPECIFICATIONS
2002 AASHTO Standard Specifications,
for Highway Bridges, 17th Edition

DESIGN STRESSES
FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

PRECAST PRESTRESSED UNITS

f'ci = 5,000 psi
f'c = 6,000 psi
fpu = 270,000 psi (1/2" φ low lax strands)
fpbt = 201,960 psi (1/2" φ low lax strands)

LOADING HS20-44

No future wearing surface allowed

SEISMIC DATA

Seismic Performance Category (SPC) = A
Bedrock Acceleration Coefficient = 4%
Site Coefficient = 1.0



To the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current "AASHTO Standard Specifications for Highway Bridges".

Robert G. Davies
Structural Engineer Expires: 11/30/2014
HR Green, Inc.

GENERAL PLAN AND ELEVATION
HILLTOP DRIVE OVER
WOODS CREEK
SECTION NO. 11-00036-00-BR
F.A.U. RTE 4036
McHENRY COUNTY
STATION 9+58.00
SN: 056-6402

COMPANY NAME: Kevin M. Artt
PROJECT CONTACT: City of Aurora
CLIENT: City of Aurora
DATE PLOTTED: 10/17/2013 9:16:02 AM
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| | CHECKED - 10/17/13 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. S-1 OF S-8 SHEETS

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|----------------|---------|---------------------------|-----------|
| 4036 | 11-00036-00-BR | McHENRY | 24 | 13 |
| CONTRACT NO. 63857 | | | ILLINOIS FED. AID PROJECT | |

GENERAL NOTES

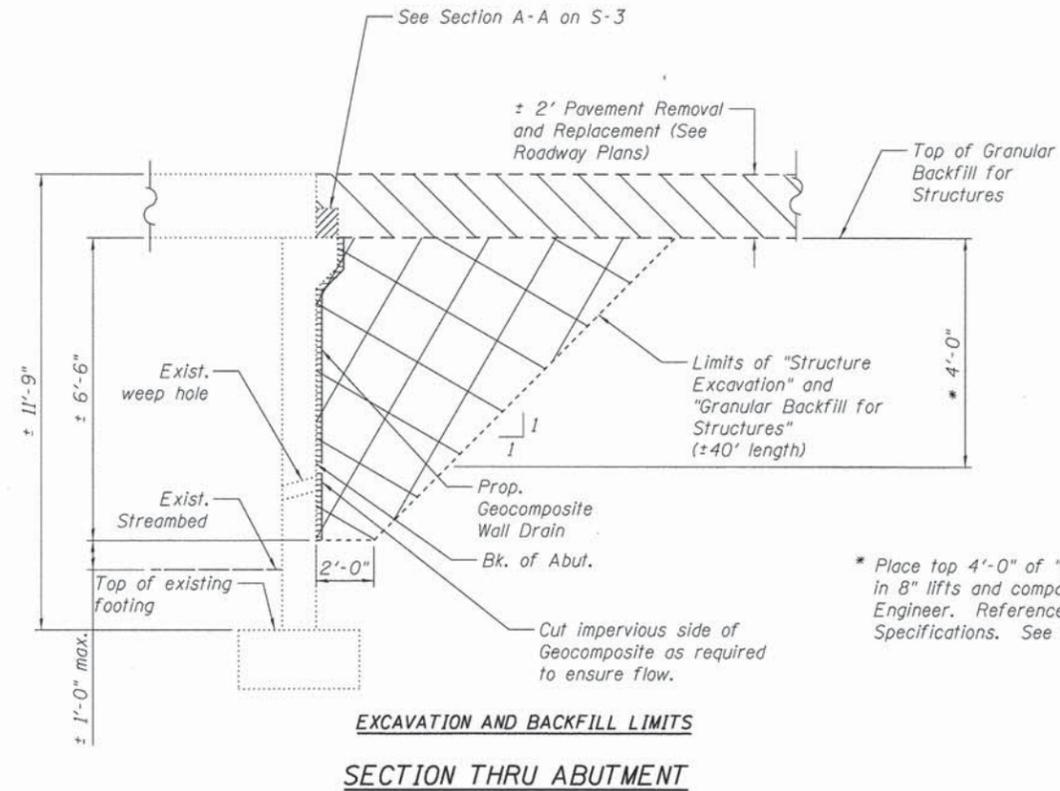
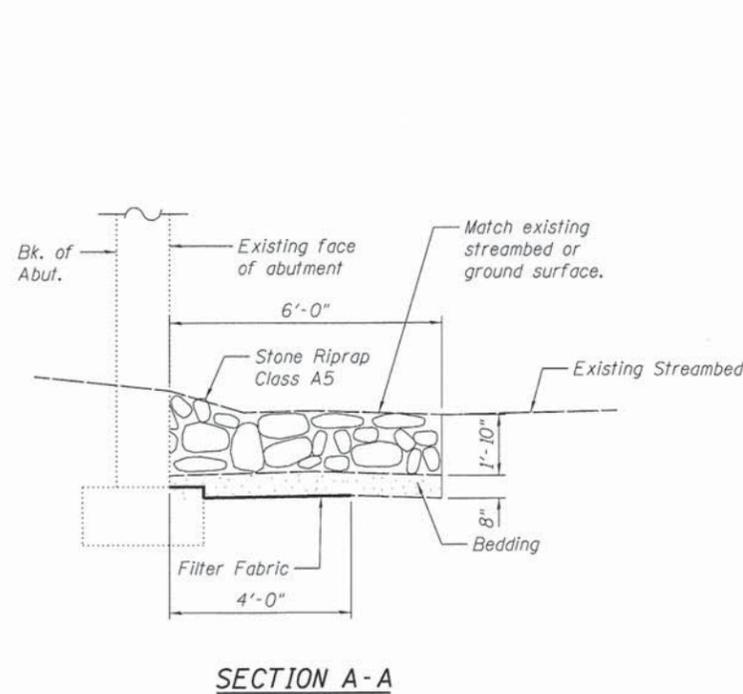
1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
4. Excavation behind existing abutment walls shall be performed to balance front and back soil pressure before removing the existing superstructure (See Section Thru Abutment this sheet).
5. Backfill shall be placed behind the abutment after the Concrete Wearing Surface and Concrete Back Wall have been poured and have achieved minimum compressive strengths of 4,000 psi and 3,500 psi respectively. See Article 502.10 of the Standard Specifications.
6. See Special Provisions for permits. See Special Provisions for original bridge contract plans (not "As-Built").

INDEX OF SHEETS

- S-1 General Plan and Elevation
- S-2 Bill of Materials and General Notes
- S-3 Superstructure
- S-4 Superstructure Detail
- S-5 21" x 48" PPC Deck Beam
- S-6 21" x 48" PPC Deck Beam Details
- S-7 Steel Railing, Type SM with Concrete Wearing Surface
- S-8 Abutment Repair Details

TOTAL BILL OF MATERIAL

| ITEM | UNIT | SUPER | SUB | TOTAL |
|--|----------|-------|------|-------|
| Stone Riprap, Class A5 | Sq. Yds. | - | 105 | 105 |
| Filter Fabric | Sq. Yds. | - | 75 | 75 |
| Removal of Existing Superstructures | Each | 1 | - | 1 |
| Structure Excavation | Cu. Yds. | - | 99.8 | 99.8 |
| Concrete Superstructure | Cu. Yds. | 3.7 | - | 3.7 |
| Bridge Deck Grooving | Sq. Yds. | 187 | - | 187 |
| Protective Coat | Sq. Yds. | 200 | - | 200 |
| Precast, Prestressed Concrete Deck Beams, 21 Inch | Sq. Ft. | 1,792 | - | 1,792 |
| Reinforcement Bars, Epoxy Coated | Pounds | 3,140 | - | 3,140 |
| Steel Railing, Type SM | Foot | 112 | - | 112 |
| Name Plates | Each | 1 | - | 1 |
| Concrete Sealer | Sq. Ft. | - | 78 | 78 |
| Geocomposite Wall Drain | Sq. Yds. | - | 63 | 63 |
| Dewatering Structure No. 1 | Each | - | 2 | 2 |
| Concrete Wearing Surface, 5" | Sq. Yds. | 200 | - | 200 |
| Granular Backfill for Structures | Cu. Yds. | - | 99.8 | 99.8 |
| Hot-Mix Asphalt Surface Removal (Deck) | Sq. Yds. | 200 | - | 200 |
| Structural Repair of Concrete (Depth Less than or Equal to 5") | Sq. Ft. | - | 36 | 36 |
| Structural Repair of Concrete (Depth Greater than 5") | Sq. Ft. | - | 72 | 72 |



* Place top 4'-0" of "Granular Backfill for Structures" in 8" lifts and compact to the satisfaction of the Engineer. Reference Article 502.10 of the Standard Specifications. See General Note 5 this sheet.

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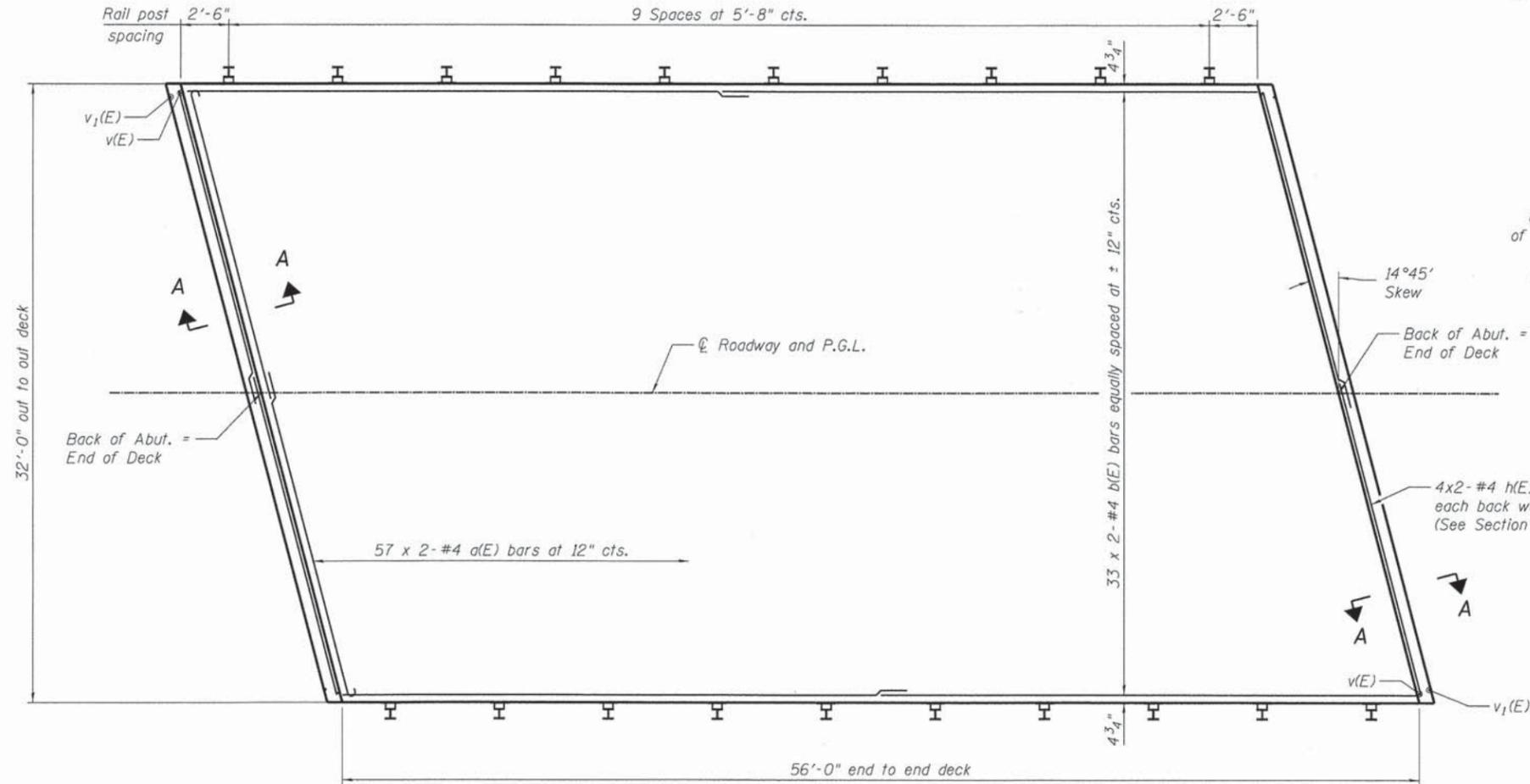
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

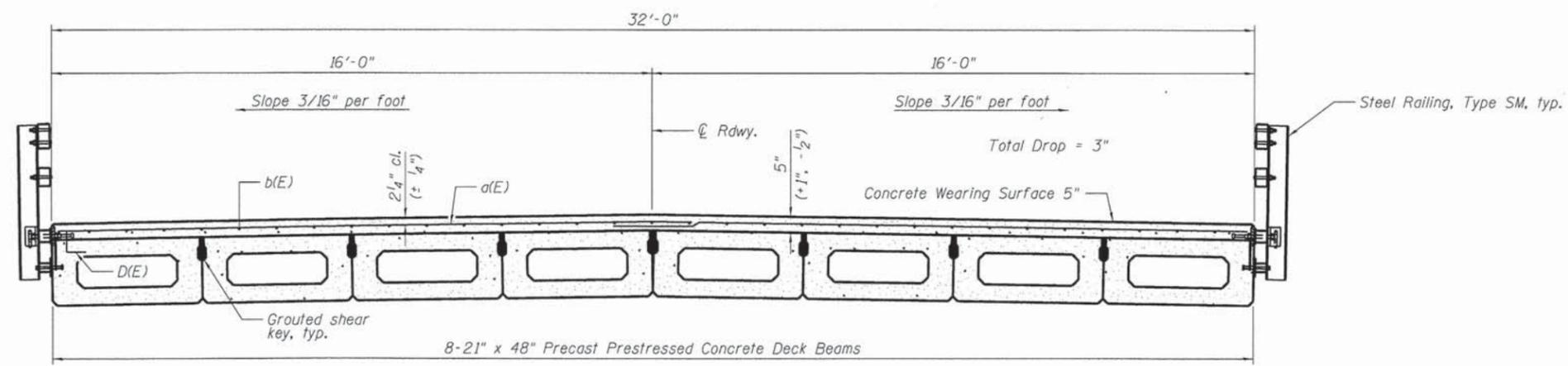
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 STRUCTURE NO. 056-6402

SHEET NO. S-2 OF S-8 SHEETS

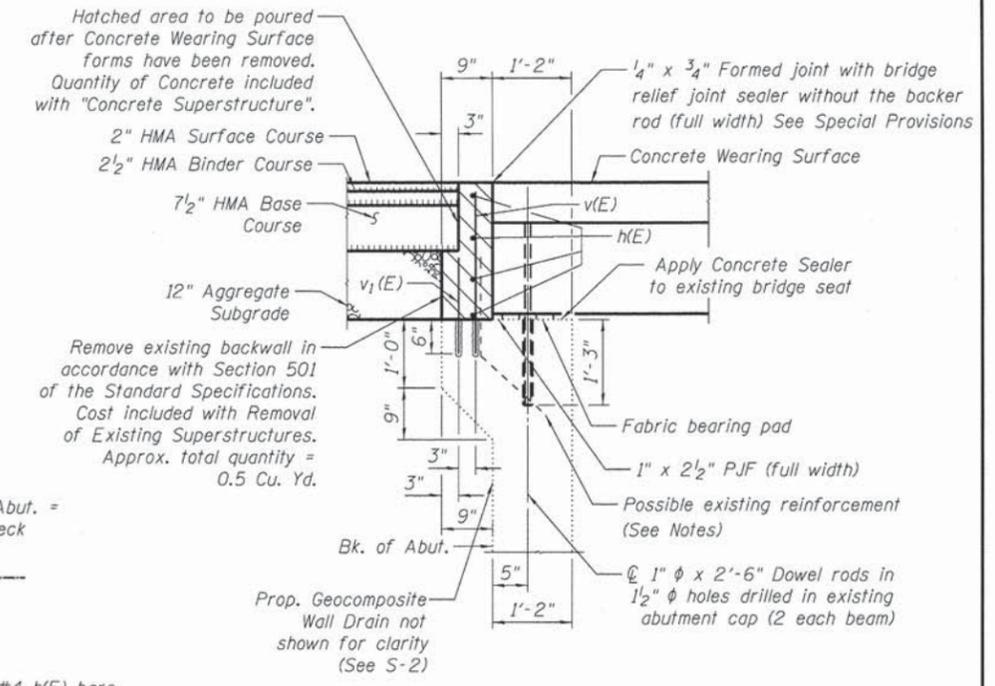
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| CONTRACT NO. 63857 | | | ILLINOIS FED. AID PROJECT | |



PLAN



CROSS SECTION
(Looking North)



SECTION A-A

Notes:

See sheet S-4 for Superstructure Details and Bill of Materials.

Bars indicated thus 20 x 2-#4 etc. indicates 20 lines of bars with 2 lengths per line.

Spacing of a(E) bars shall be measured along the ϕ of structure.

See Sheet S-6 for fabric bearing pad details.

See Sheet S-8 for additional details of hatched area in Section A-A.

Existing reinforcement shall be cleaned and incorporated into the new construction. Cost included with Removal of Existing Superstructures.

MINIMUM BAR LAP
#4 bar = 2'-7"

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 PROJECT CONTACT: Kevin M. Arfitt
 CLIENT: City of Aurora
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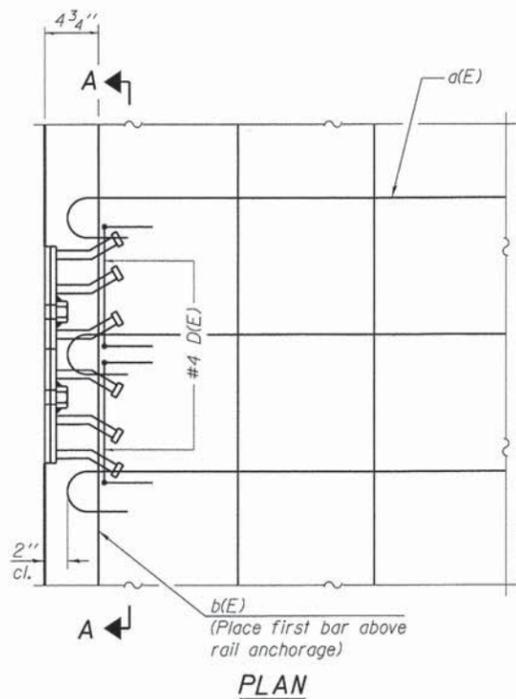


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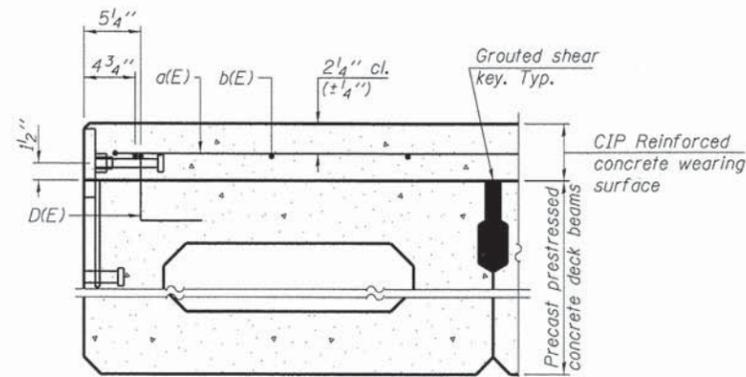
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
STRUCTURE NO. 056-6402
SHEET NO. S-3 OF S-8 SHEETS

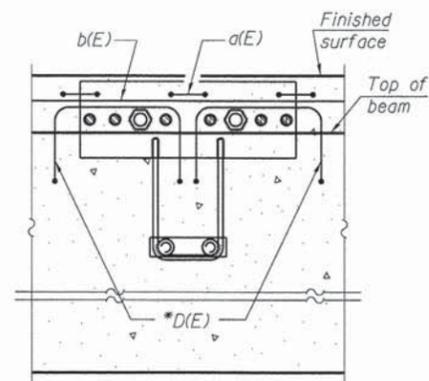
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| ILLINOIS FED. AID PROJECT | | | | |



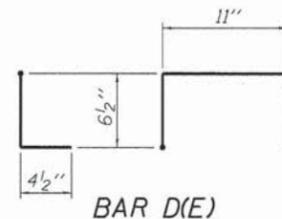
Notes:
Formwork necessary for the wearing surface may be secured utilizing the bottom rail anchorage inserts and/or additional inserts cast into the beam.



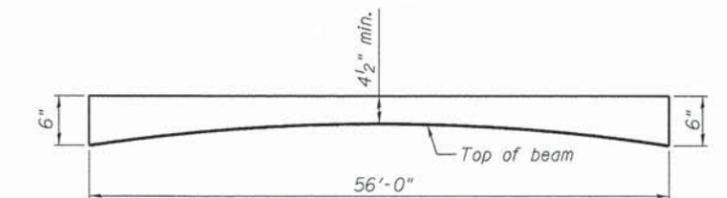
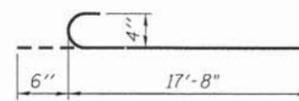
SECTION THRU FASCIA BEAM



SECTION A-A



* Place 2-#4 D(E) bars in beam at each post location as shown. D(E) bar included in cost of beam.



**SUPERSTRUCTURE
BILL OF MATERIAL**

| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|------|---------|-------|
| a(E) | 114 | #4 | 18'-2" | ⌋ |
| b(E) | 66 | #4 | 29'-2" | — |
| h(E) | 16 | #4 | 17'-8" | — |
| v(E) | 66 | #5 | 2'-6" | — |
| v ₁ (E) | 64 | #5 | 1'-7" | — |
| Reinforcement Bars, Epoxy Coated | | | Pound | 3,140 |
| Concrete Wearing Surface, 5" | | | Sq. Yd. | 200 |

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.

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CLIENT: City of Aurora
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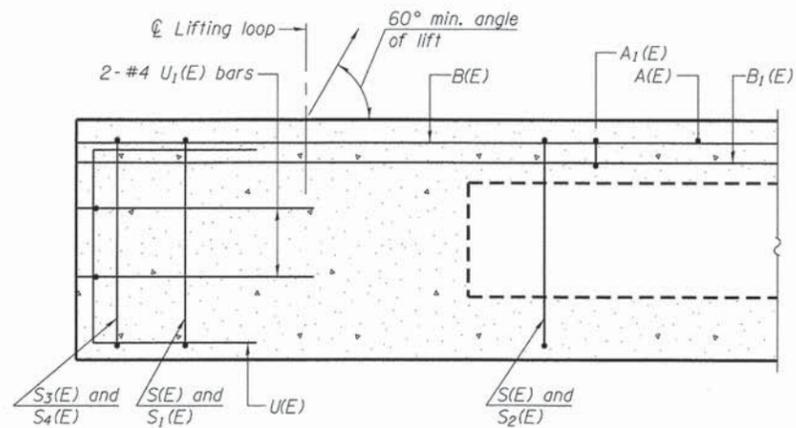
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| USER NAME = whood | DESIGNED - KMA | REVISED - |
| PLOT SCALE = | CHECKED - RGD | REVISED - |
| PLOT DATE = 10/17/2013 | DRAWN - WJH | REVISED - |
| | CHECKED - 10/17/13 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

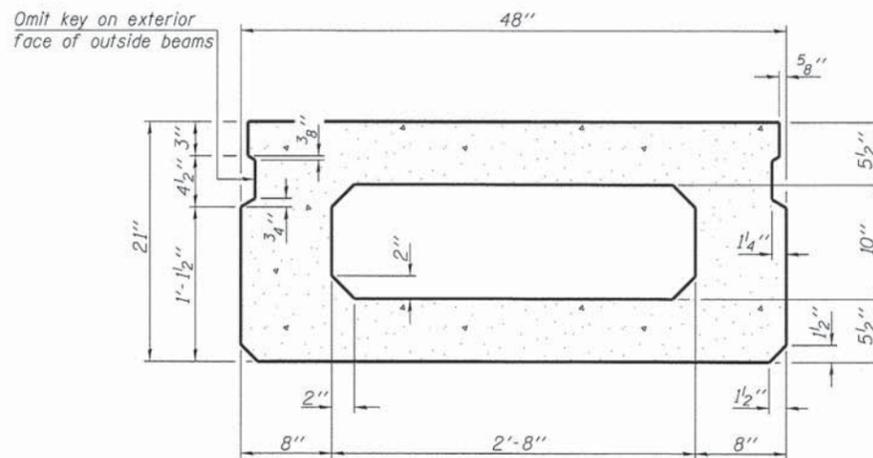
SUPERSTRUCTURE DETAIL
STRUCTURE NO. 056-6402

SHEET NO. S-4 OF S-8 SHEETS

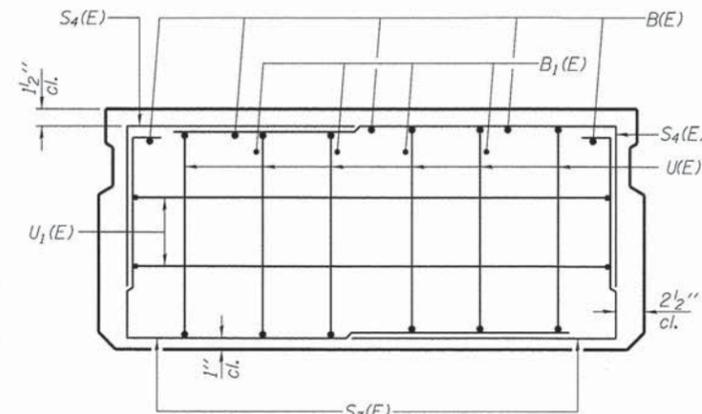
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| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY McHENRY | TOTAL SHEETS 24 | SHEET NO. 16 |
| | | | | CONTRACT NO. 63857 |
| ILLINOIS FED. AID PROJECT | | | | |



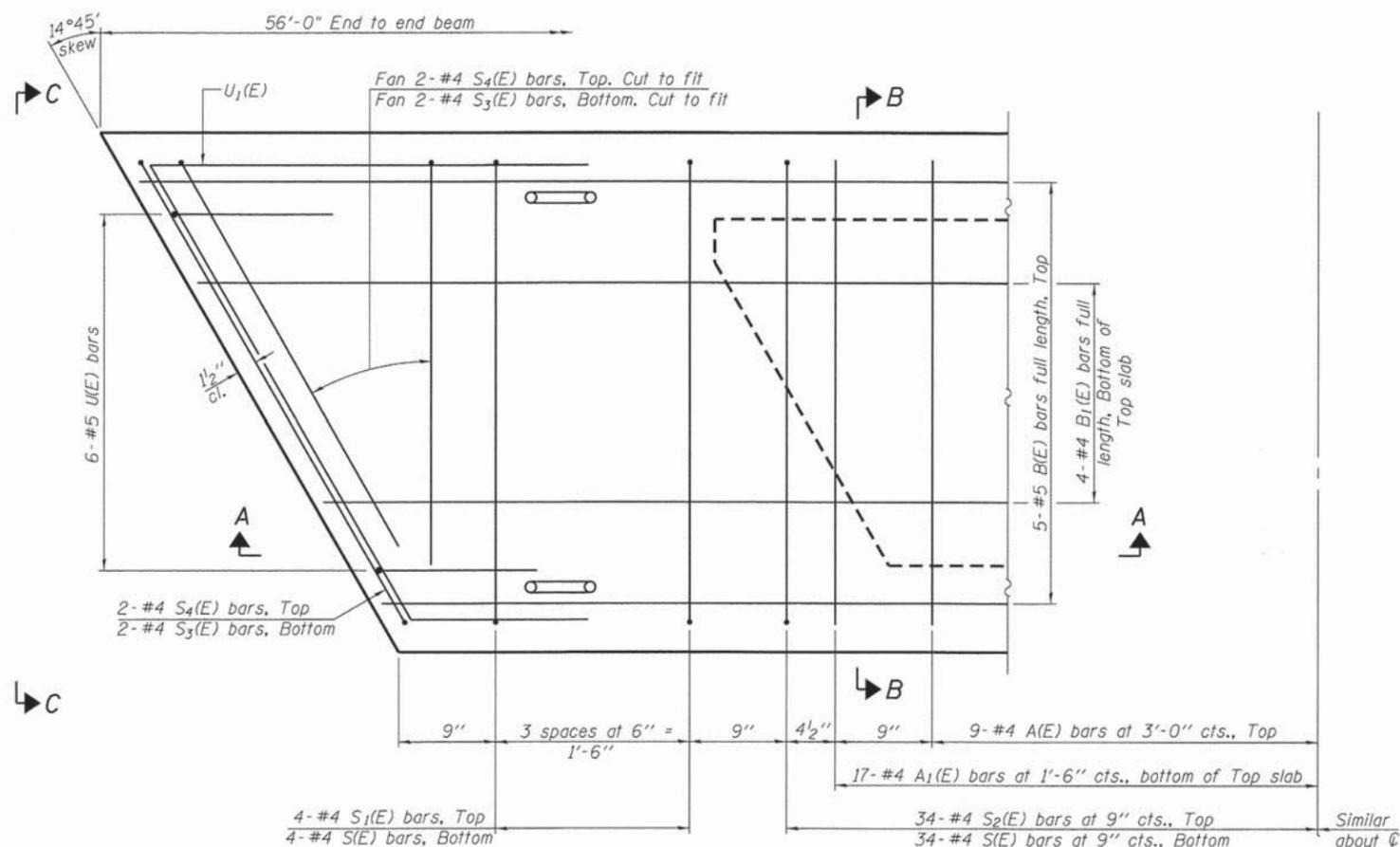
SECTION A-A



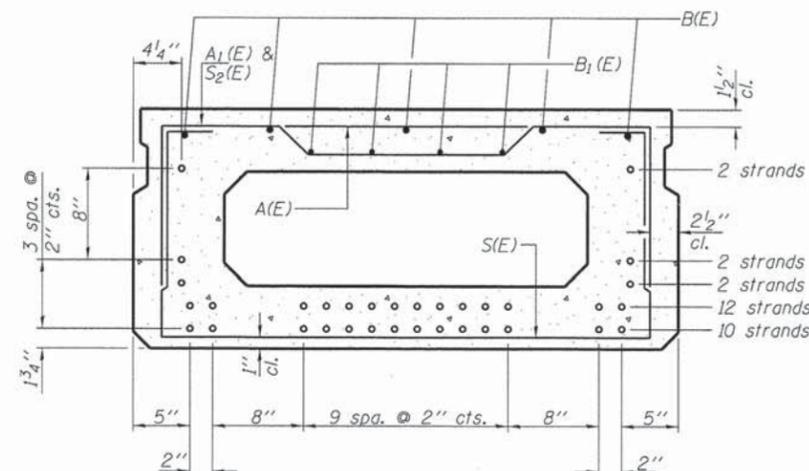
SECTION B-B
(Showing dimensions)



VIEW C-C



PLAN VIEW



SECTION B-B

(Showing reinforcement and permissible strand locations)

Note: Place the number of strands specified in each row symmetrically about the centerline of beam in the permissible strand locations shown.

BAR LIST
ONE BEAM ONLY
(For information only)

| Bar | No. | Size | Length | Shape |
|-------|-----|------|--------|-------|
| A(E) | 18 | #4 | 3'-7" | — |
| A1(E) | 34 | #4 | 3'-10" | — |
| B(E) | 5 | #5 | 55'-9" | — |
| B1(E) | 4 | #4 | 55'-9" | — |
| D(E) | 20 | #4 | 2'-9" | ⌊ |
| S(E) | 76 | #4 | 7'-5" | ⌊ |
| S1(E) | 8 | #4 | 5'-11" | ⌊ |
| S2(E) | 68 | #4 | 6'-2" | ⌊ |
| S3(E) | 8 | #4 | 5'-9" | ⌊ |
| S4(E) | 8 | #4 | 5'-0" | ⌊ |
| U(E) | 12 | #5 | 4'-0" | ⌊ |
| U1(E) | 4 | #4 | 7'-0" | ⌊ |

Note: See sheet of for additional details and Bill of Material.
* At exterior beams only

MINIMUM BAR LAP

#4 bar = 2'-0"
#5 bar = 2'-6"

Note: Spacing of S(E) and S2(E) bars may be adjusted up to 4" in the immediate area of the transverse tie diaphragms to miss the block outs for the transverse ties.

COMPANY NAME: HRGreen
PROJECT CONTACT: Kevin M. Arff
CITY OF AURORA
CLIENT OFFICE: 4810 S. STATE ST., SUITE 202, AM
FILE NAME: 11-00036-00-01.dgn
PLOT DRIVER: pdfLJET-111.dwt
PEN TABLE: standard-trans.tbl



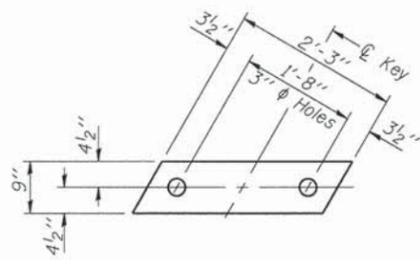
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| USER NAME = whoad | DESIGNED - KMA | REVISED - |
| PLOT SCALE = | CHECKED - RGD | REVISED - |
| PLOT DATE = 10/17/2013 | DRAWN - WJH | REVISED - |
| | CHECKED - 10/17/13 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

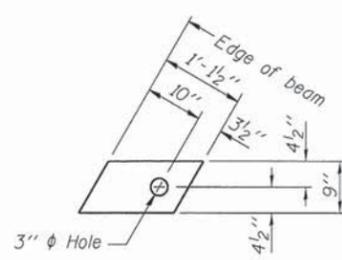
21" X 48" PPC DECK BEAM
STRUCTURE NO. 056-6402

SHEET NO. S-5 OF S-8 SHEETS

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------------|----------------|---------|--------------|-----------|
| 4036 | 11-00036-00-BR | McHENRY | 24 | 17 |
| CONTRACT NO. 63857 | | | | |
| [ILLINOIS] FED. AID PROJECT | | | | |



FABRIC BEARING PAD
(Interior)

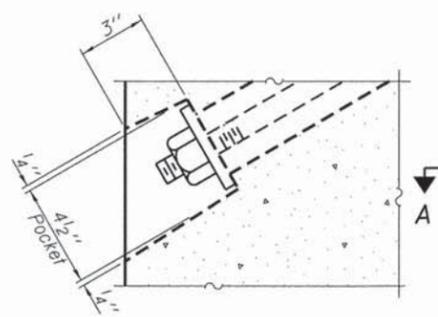


FABRIC BEARING PAD
(Exterior)

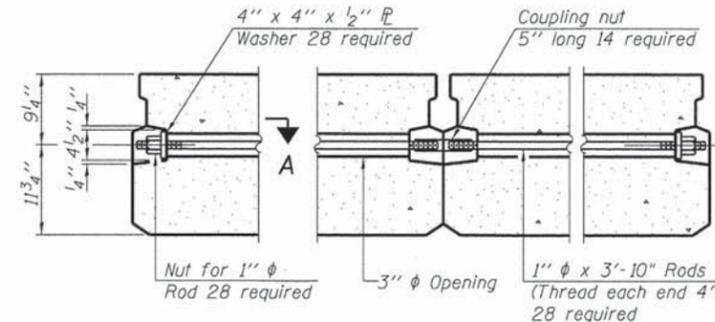
FIXED

Notes:

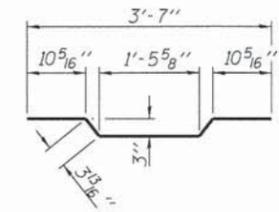
All bearing pads shall be 1" thick.
Omit holes when using expansion bearings.
Expansion bearing pad shall be bonded to the substructure.



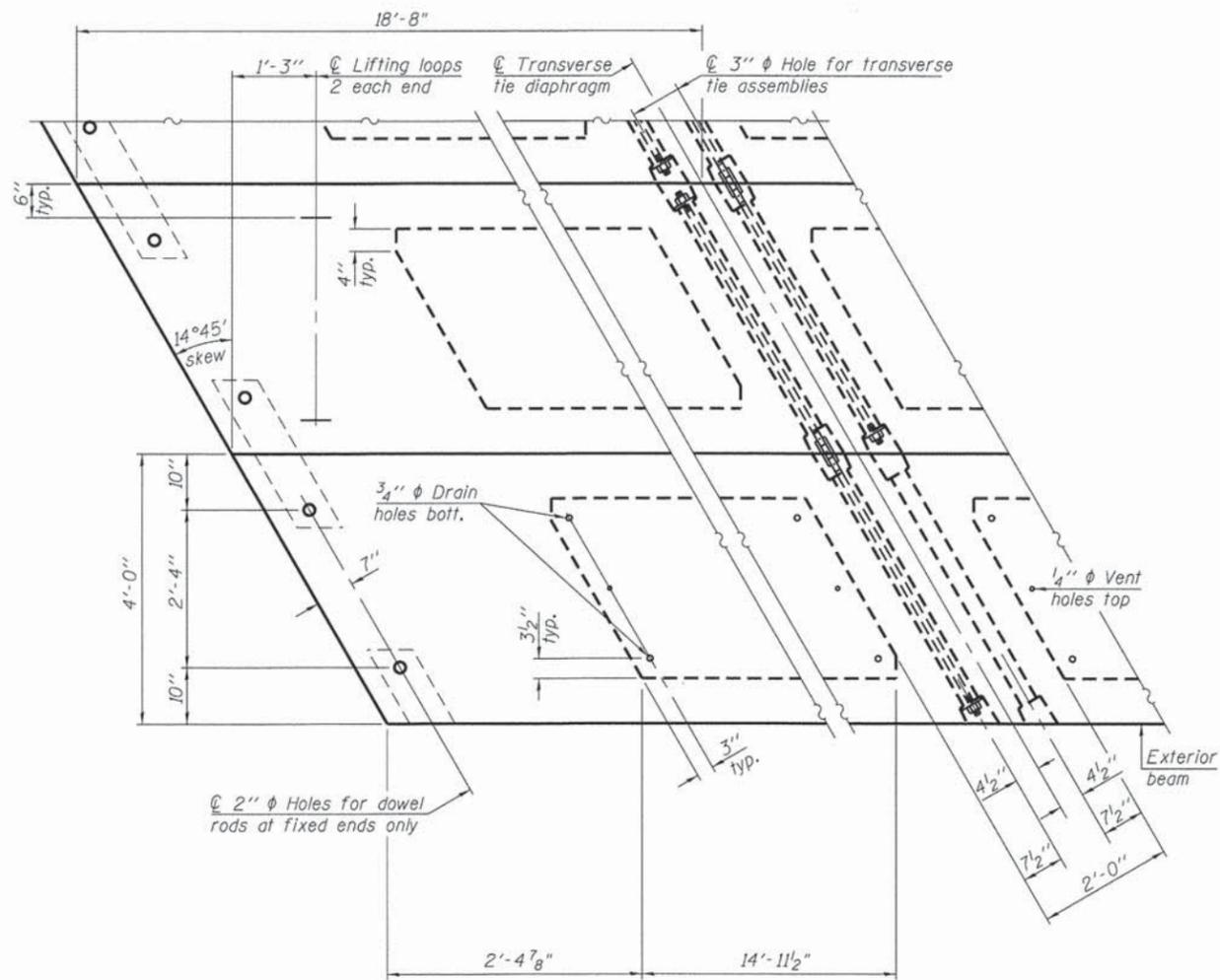
SECTION A-A



TYPICAL TRANSVERSE TIE ASSEMBLY

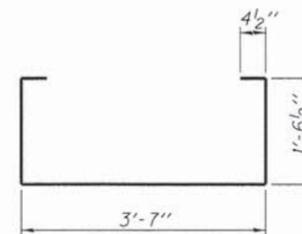


BAR A₁(E)

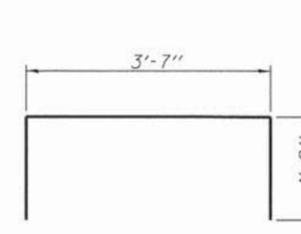


PLAN VIEW

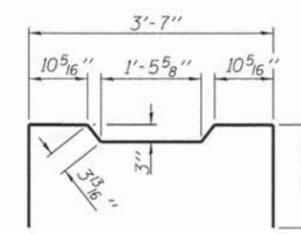
Note: Connect beams in pairs with the transverse tie configuration shown.



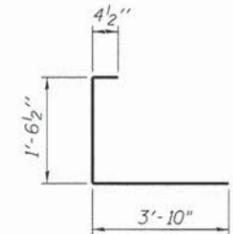
BAR S(E)



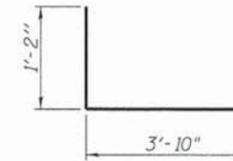
BAR S₁(E)



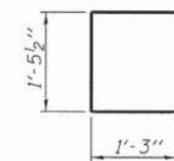
BAR S₂(E)



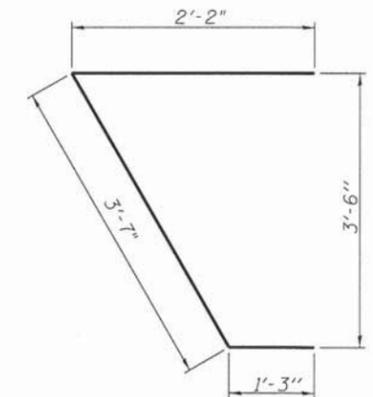
BAR S₃(E)



BAR S₄(E)



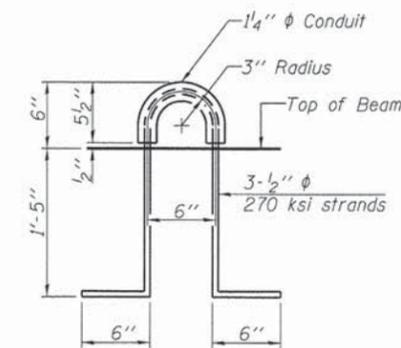
BAR U(E)



BAR U₁(E)

NOTES

Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153 sq. in. The 1" φ rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets on exterior faces of bridge shall be filled with grout after transverse tie assembly is in place.
Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions).
Two 1/8" fabric adjusting shims of the dimensions of the exterior bearing pad shall be provided for each bearing pad location.
A minimum 2 1/2" φ lifting pin shall be used to engage the lifting loops during handling.
Corrosion Inhibitor, per Article 1020.05(b)(12) and 1021.06 of the Standard Specifications, shall be used in the concrete for precast prestressed concrete deck beams.
Compressive strength of prestressed concrete, f'c, shall be 6000 psi.
Compressive strength of prestressed concrete at release, f'ci, shall be 5000 psi.



LIFTING LOOP DETAIL

BILL OF MATERIAL

| | | |
|---|---------|------|
| Precast Prestressed Conc. Deck Bms. (21" depth) | Sq. Ft. | 1792 |
|---|---------|------|

COMPANY NAME: HRGreen
PROJECT CONTACT: Kevin M. Arff
CLIENT: City of Aurora
DATE PLOTTED: 10/17/2013 9:16:24 AM
FILE NAME: 0620386-08-02.dgn
PEN TABLE: pen.tbl



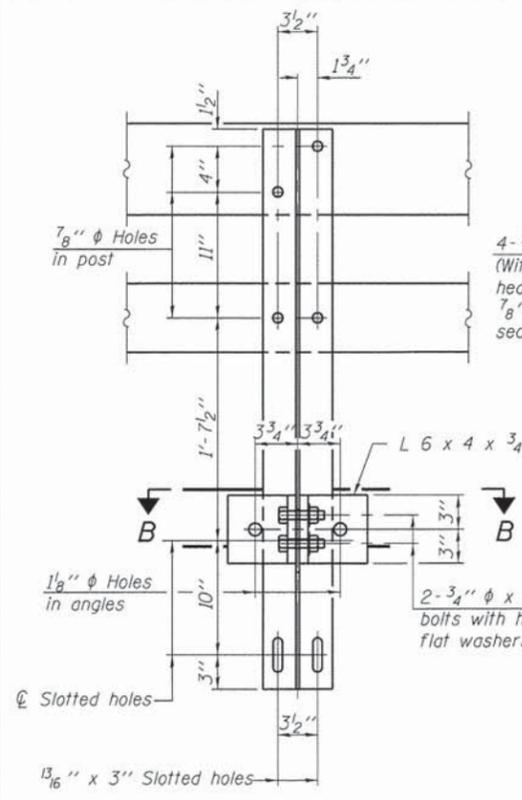
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|------------------------|--------------------|-----------|
| USER NAME = whood | DESIGNED - KMA | REVISED - |
| PLOT SCALE = | CHECKED - RGD | REVISED - |
| PLOT DATE = 10/17/2013 | DRAWN - WJH | REVISED - |
| | CHECKED - 10/17/13 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

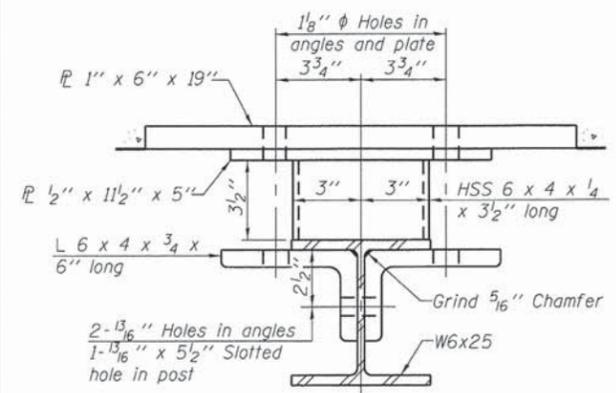
**21" X 48" PPC DECK BEAM DETAILS
STRUCTURE NO. 056-6402**

SHEET NO. S-6 OF S-8 SHEETS

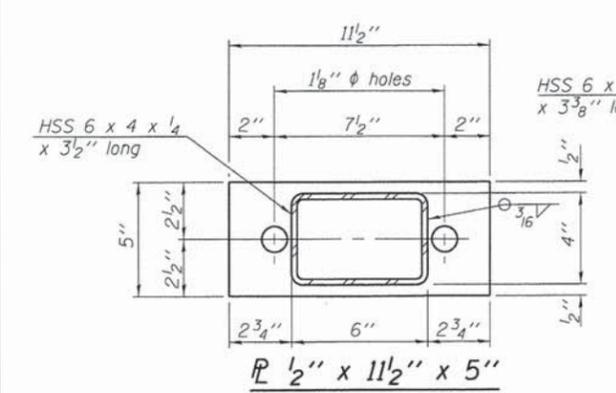
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|--------------------|------------------------|----------------|---------------------------|--------------|
| F.A.U. RTE. 4036 | SECTION 11-00036-00-BR | COUNTY McHENRY | TOTAL SHEETS 24 | SHEET NO. 18 |
| CONTRACT NO. 63857 | | | ILLINOIS FED. AID PROJECT | |



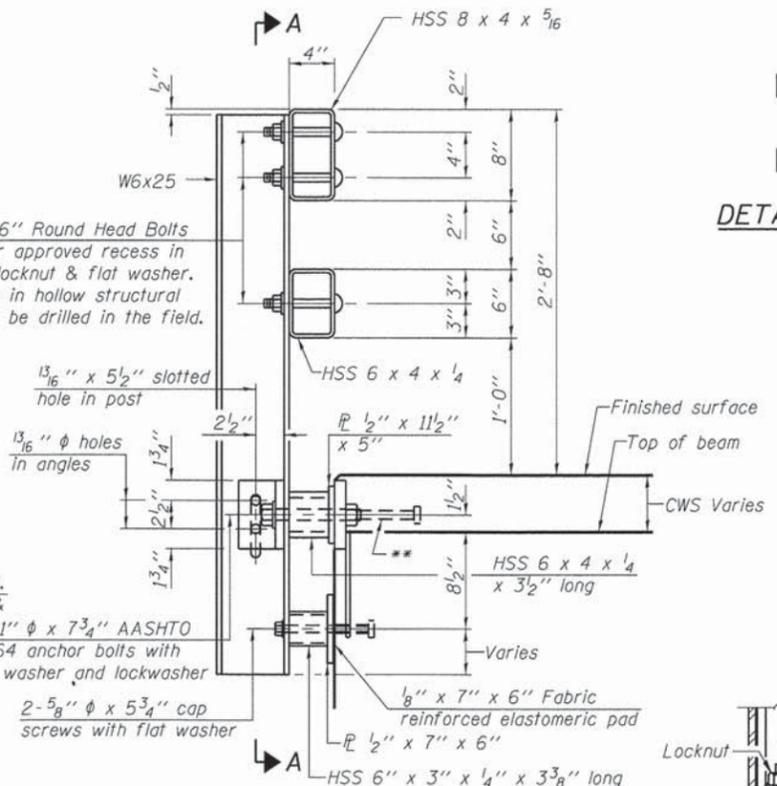
SECTION A-A



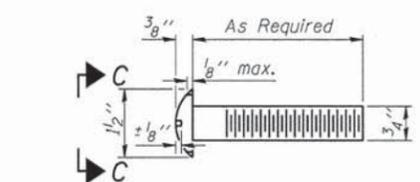
SECTION B-B



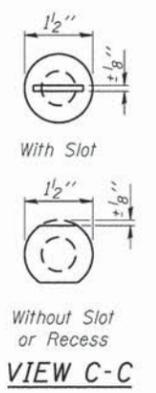
SECTION C-C



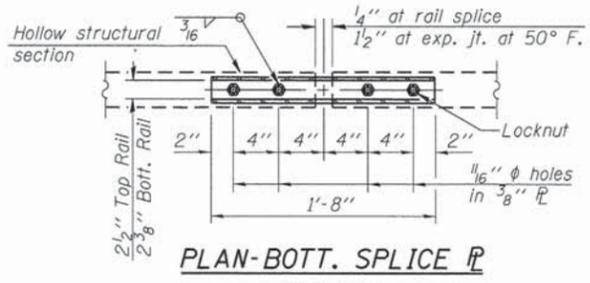
SECTION AT RAIL POST



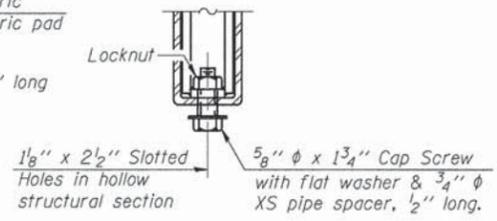
DETAIL OF 3/4" ROUND HEAD BOLT



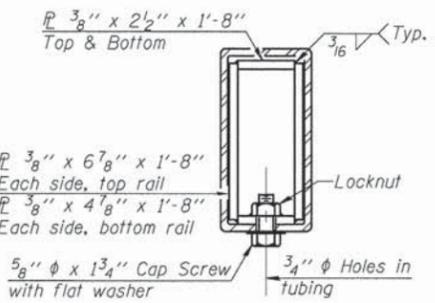
VIEW C-C



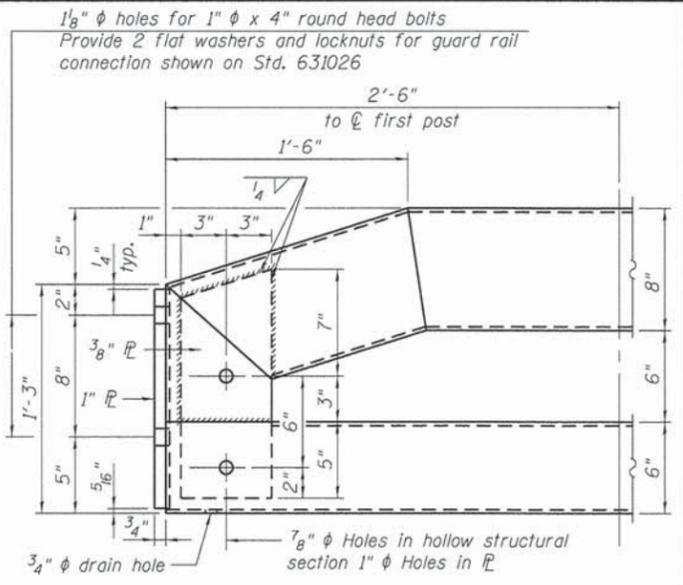
PLAN-BOTT. SPLICE TYPICAL



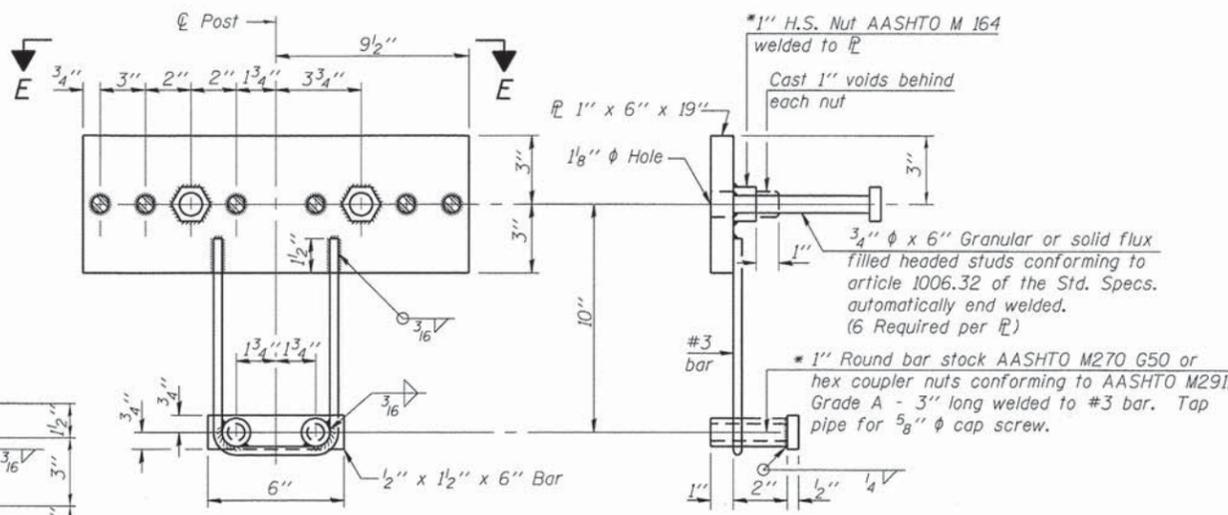
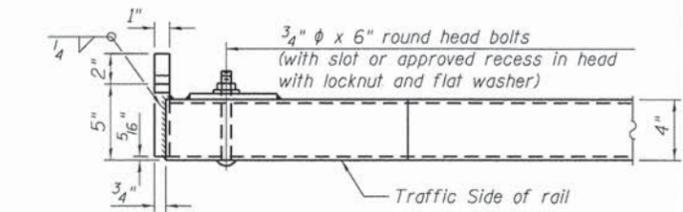
RAIL SPLICE CONNECTION AT EXPANSION JT.



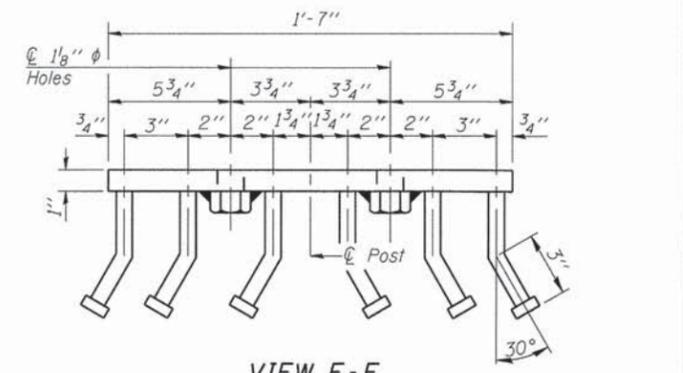
SECTION AT RAIL SPLICE



END OF RAIL DETAILS



ANCHOR DEVICE



VIEW E-E

Notes:
 All field drilled holes shall be coated with an approved zinc rich paint before erection.
 For multi-span bridges, sufficient 1/4 inch x 6 inch x 1-2 inch galvanized steel shims shall be provided to align rail between adjacent spans. Cost included with Steel Railing, Type SM.
 Steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.
 ** The studs of the anchor devices shall be placed below the top reinforcement bars and the outermost longitudinal reinforcement bar shall be placed directly above the studs of the rail post anchor device.

BILL OF MATERIAL

| Item | Unit | Quantity |
|------------------------|------|----------|
| Steel Railing, Type SM | Foot | 112 |

COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 CLIENT: 10/17/2013 9:16:26 AM
 DATE PLOTTED: 06/20/2016-01:01:00
 FILE NAME: pdt_DET-T11.dwg
 PLOT DRIVER: standard-trans.tbl
 PLOT TABLE:



USER NAME = whood
 DESIGNED - KMA
 CHECKED - RGD
 DRAWN - WJH
 CHECKED - 10/17/13

REVISOR -
 REVISION -
 REVISION -
 REVISION -

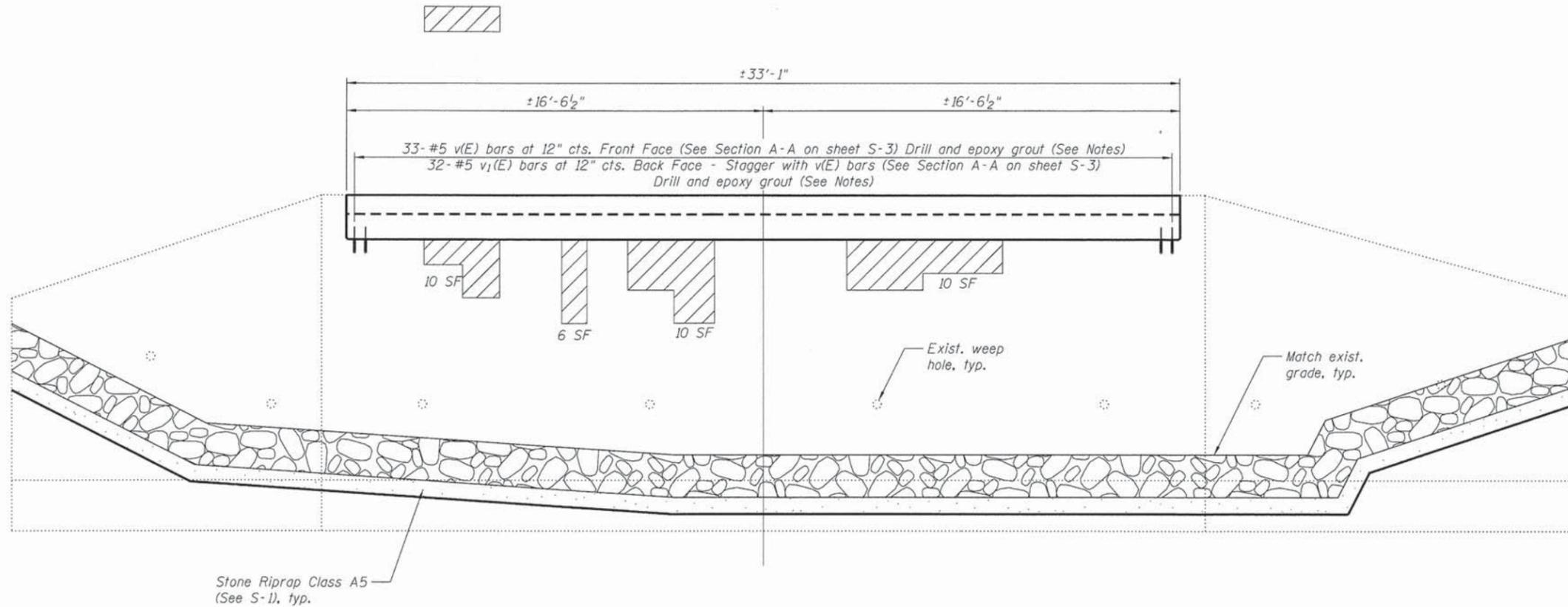
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STEEL RAILING, TYPE SM WITH CONCRETE WEARING SURFACE
 STRUCTURE NO. 056-6402

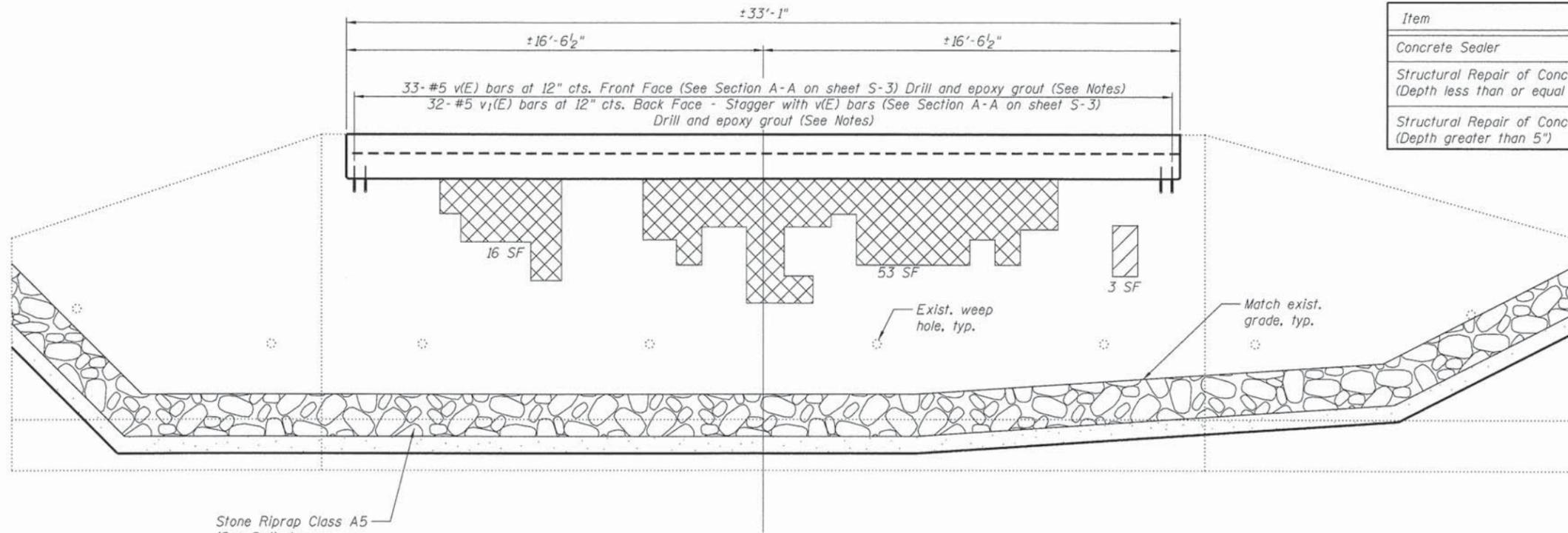
SHEET NO. S-7 OF S-8 SHEETS

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|---------|--------------|--------------------|
| 4036 | 11-00036-00-BR | McHENRY | 24 | 19 |
| | | | | CONTRACT NO. 63857 |

ILLINOIS FED. AID PROJECT



NORTH ABUTMENT ELEVATION



SOUTH ABUTMENT ELEVATION

BILL OF MATERIAL

| Item | Unit | Quantity |
|--|---------|----------|
| Concrete Sealer | Sq. Ft. | 78 |
| Structural Repair of Concrete (Depth less than or equal to 5") | Sq. Ft. | 36 |
| Structural Repair of Concrete (Depth greater than 5") | Sq. Ft. | 72 |

LEGEND

- Structural Repair of Concrete (Depth less than or equal to 5")
- Structural Repair of Concrete (Depth greater than 5")

Notes:
 1. Drilling and epoxy grouting shall conform to Article 509.06 of the Standard Specifications. Cost included in "Reinforcement Bars, Epoxy Coated."

COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 CLIENT: 10/17/2013 9:16:29 AM
 DATE PLOTTED: 10/17/2013 9:16:29 AM
 PLOT DRIVER: standard-trans.tbl
 PEN TABLE:



| | | |
|------------------------|--------------------|-----------|
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| | CHECKED - RGD | REVISED - |
| PLOT SCALE = | DRAWN - WJH | REVISED - |
| PLOT DATE = 10/17/2013 | CHECKED - 10/17/13 | REVISED - |

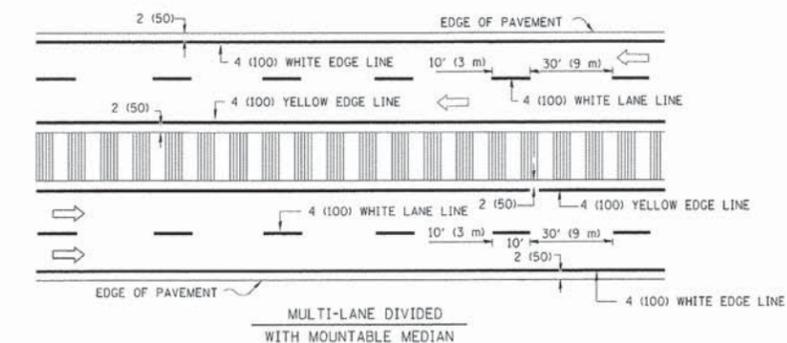
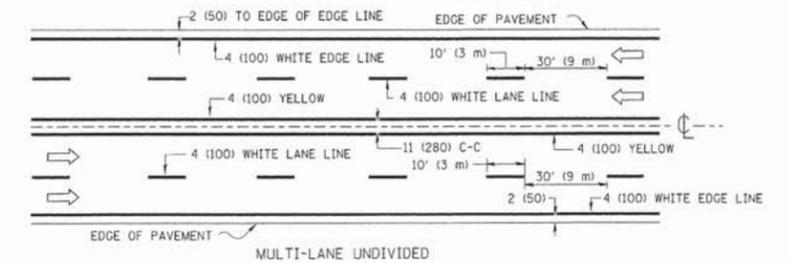
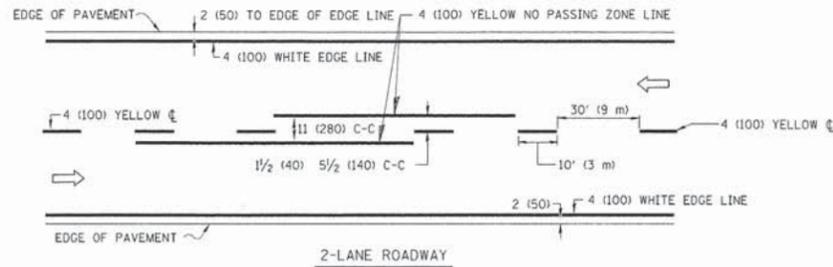
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ABUTMENT REPAIR DETAILS
 STRUCTURE NO. 056-6402**

SHEET NO. S-8 OF S-8 SHEETS

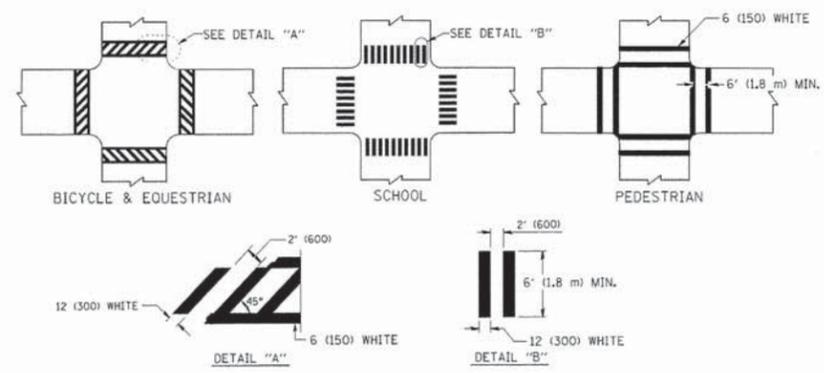
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|---------|--------------|---------------------------|
| 4036 | 11-00036-00-BR | McHENRY | 24 | 20 |
| | | | | CONTRACT NO. 63857 |

ILLINOIS FED. AID PROJECT

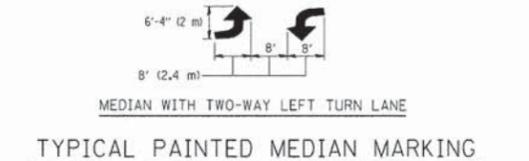
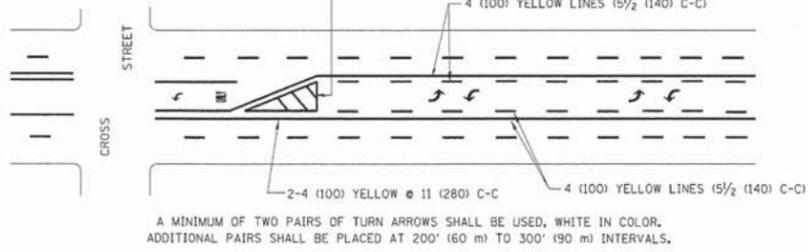
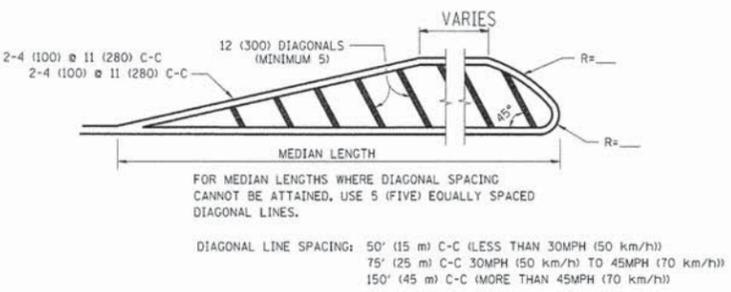
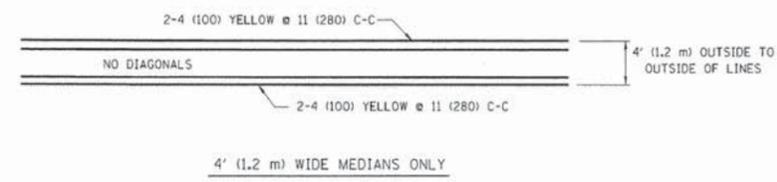


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

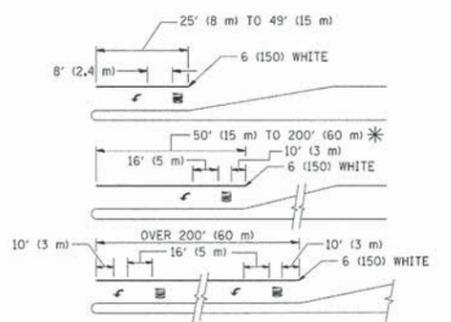
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



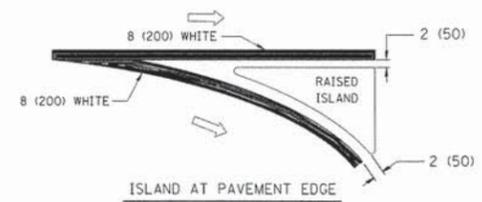
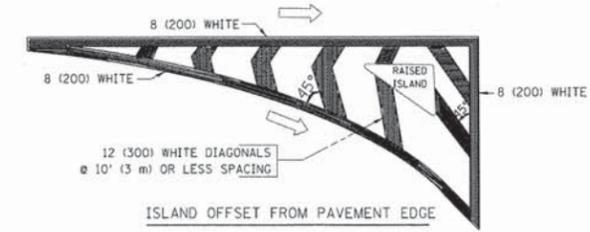
TYPICAL TURN LANE MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" 15 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SO. FT. (1.03 m ²) EACH "X"=54.0 SO. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

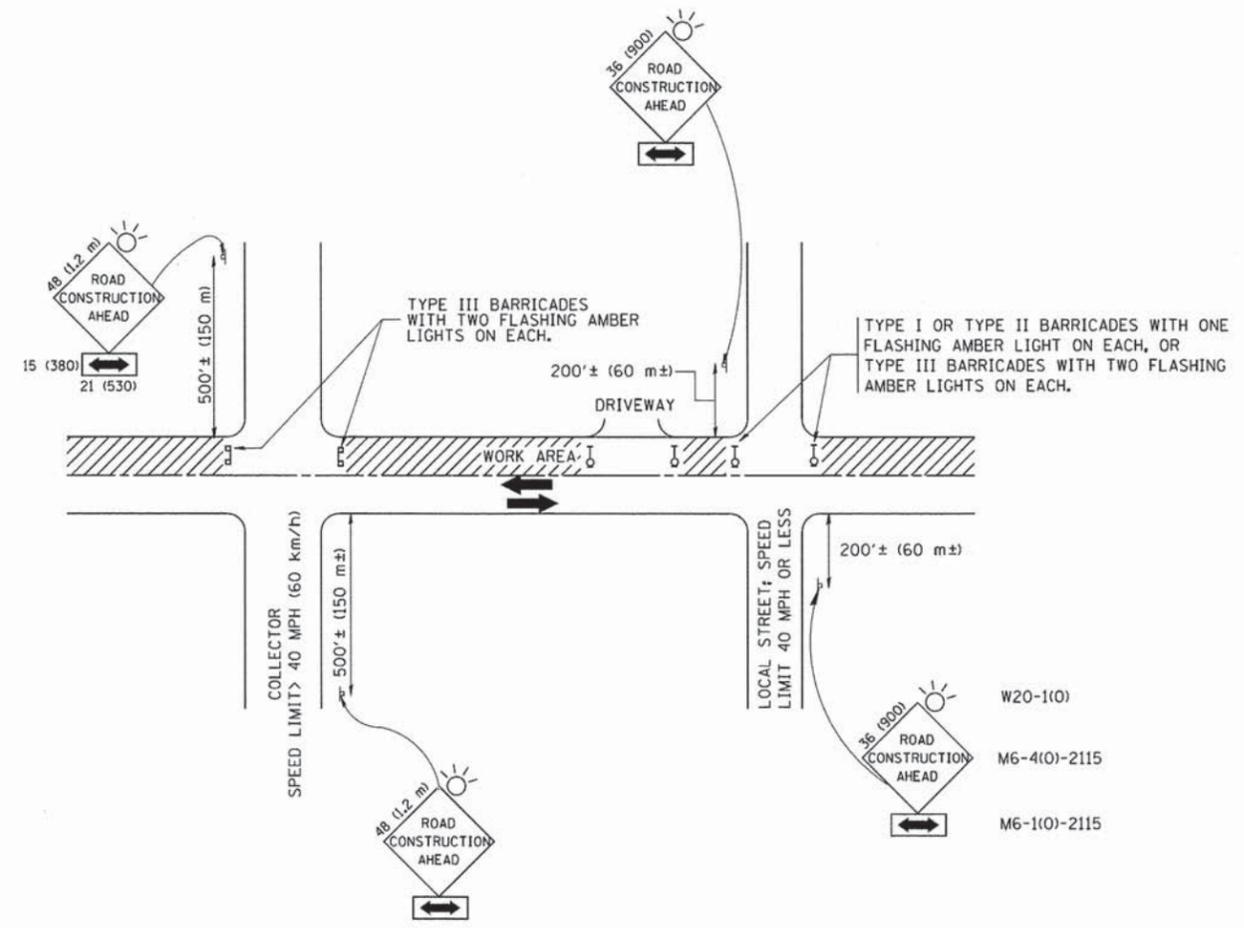
COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 CLIENT: 10/17/2013 9:16:37 AM
 DATE PLOTTED: 86/20086-0E7-01.dgn
 FILE NAME: pdf.DET-11F.dgn
 PLOT DRIVER: standard-trans.dtl

| | | | |
|--|----------------------------|------------------|--------------------------------|
| FILE NAME = | USER NAME = drvakosgn | DESIGNED - EVERS | REVISED -T, RAMMACHER 10-27-94 |
| c:\pwwork\pwwork\drvakosgn\10109315\to | 3.dgn | DRAWN - | REVISED -C, JUCIUS 09-09-09 |
| | PLOT SCALE = 50.000 1/ IN. | CHECKED - | REVISED - |
| | PLOT DATE = 9/9/2009 | DATE - 03-19-90 | REVISED - |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

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|---------------------------|-----------|-----------|------|---------|
| DISTRICT ONE | | | | |
| TYPICAL PAVEMENT MARKINGS | | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |

| | | | | |
|---|----------------|---------|--------------------|-----------|
| M.S. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 4036 | 11-00036-00-BR | McHENRY | 24 | 21 |
| TC-13 | | | CONTRACT NO. 63857 | |
| FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT | | | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

COMPANY NAME: Kevin M. Arff
 PROJECT CONTACT: City of Aurora
 CLIENT: 10/17/2003 9:26:42 AM
 DATE PLOTTED: 8620086-0E7-02.dgn
 FILE NAME: pdr_DET-TFF.dgn
 PLOT DRIVER: standard-trans.tbl
 PEN TABLE:

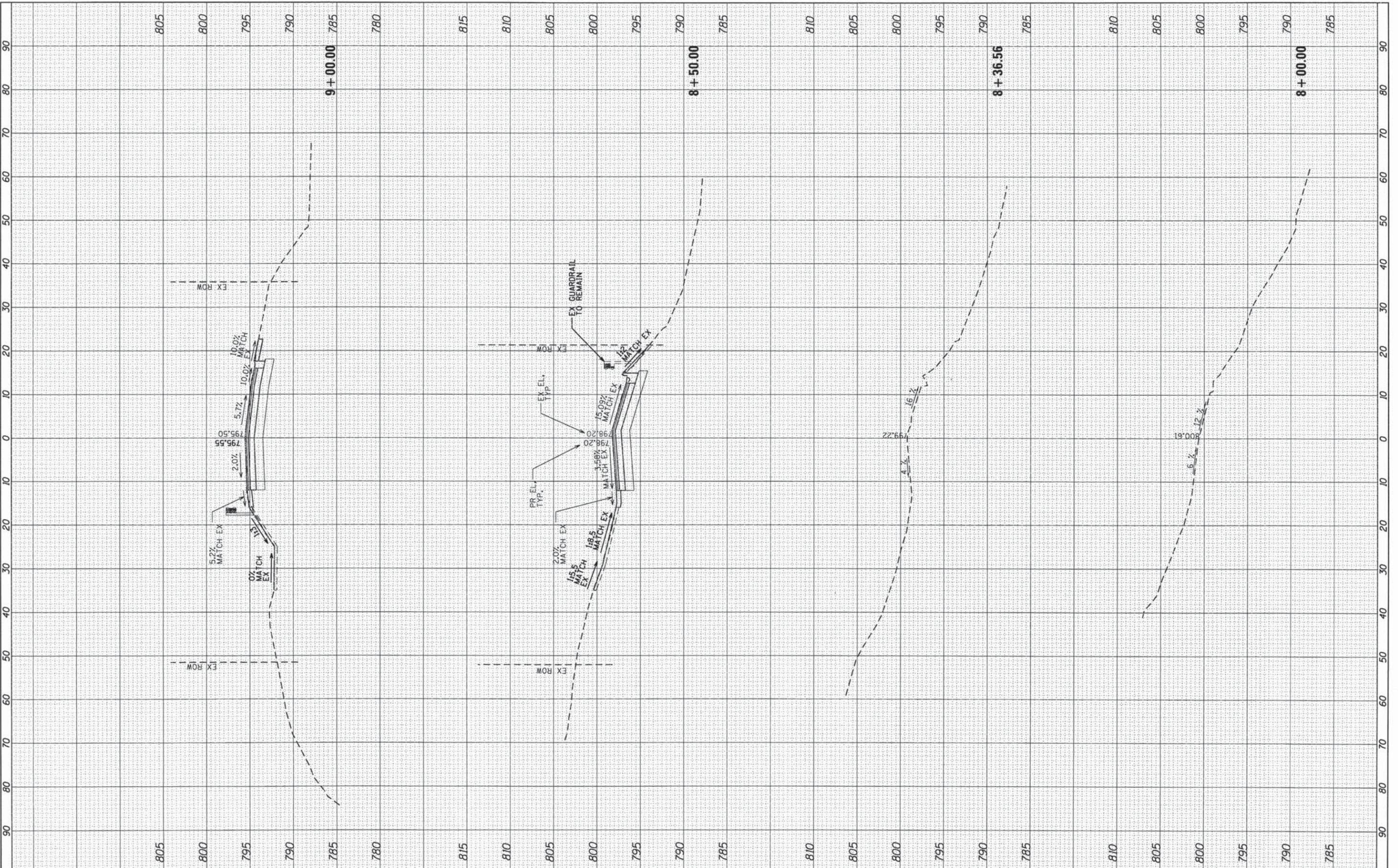
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| FILE NAME = | USER NAME = geglencbt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| W:\dststd\22x34\tc10.dgn | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | | PLOT SCALE = 50,000 ' / IN. | REVISED - A. HOUSEH 10-15-96 |
| | | PLOT DATE = 1/4/2008 | REVISED - T. RAMMACHER 01-06-00 |
| | | DATE - 06-89 | |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

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|--|-----------|-----------|------|---------|
| TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | | | | |
| SCALE: | SHEET NO. | OF SHEETS | STA. | TO STA. |

| | | | | |
|---|----------------|---------|--------------------|-----------|
| M.S. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 4036 | 11-00036-00-BR | McHENRY | 24 | 22 |
| TC-10 | | | CONTRACT NO. 63857 | |
| FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT | | | | |

COMPANY NAME: Kevin M. Artt
 PROJECT CONTACT: City of Aurora
 CLIENT: 11/1/2013 10:27:38 AM
 DATE PLOTTED: 86120386-xsec.dgn
 FILE NAME: pdr.plt
 PLOT DRIVER: s:\fordor-dr-frans.tbl



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|--|--|
| | HRGreen.com Illinois Professional Design Firm # 184-001322 |
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|-------------------------------|----------------|-----------|
| USER NAME = whood | DESIGNED - KMA | REVISED - |
| FILE NAME = 86120386-xsec.dgn | DRAWN - WJH | REVISED - |
| PLOT SCALE = 10H:5V | CHECKED - RGD | REVISED - |
| PLOT DATE = 11/1/2013 | DATE - 11/1/13 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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|---------------------------------------|--|
| CROSS SECTIONS | |
| HILLTOP DRIVE OVER WOODS CREEK | |
| SCALE: 10H:5V | SHEET NO. OF SHEETS STA. 8+00.00 TO STA. 9+00.00 |

| F.A.U. RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|---------|--------------|-----------|
| 4036 | 11-00036-00-BR | MCHENRY | 24 | 23 |
| CONTRACT NO. 63857 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

