

1-17-14 LETTING ITEM 028

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

F.A.U. ROUTE 1039 (HAPP ROAD)  
SUNSET RIDGE RD TO VILLAGE LIMITS

SECTION 13-00159-00-RS

PROJECT NO. M-4003(158)

VILLAGE OF NORTHBROOK

COOK COUNTY

C-91-205-13

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	1

CONTRACT: 63883

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- 701301-04 LANE CLOSURE, 2L, 2W SHORT TIME OPERATIONS
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- 720006-04 SIGN PANEL ERECTION DETAILS
- 720011-01 METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
- 729001-01 APPLICATION OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
- 781001-04 TYPICAL PAVEMENT MARKINGS

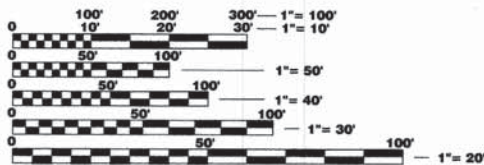
**TRAFFIC DATA:**

ADT 2,950 VPD (2006)

POSTED SPEED LIMIT - 30 MPH

DESIGN SPEED LIMIT - 35 MPH

CLASSIFICATION: URBAN COLLECTOR

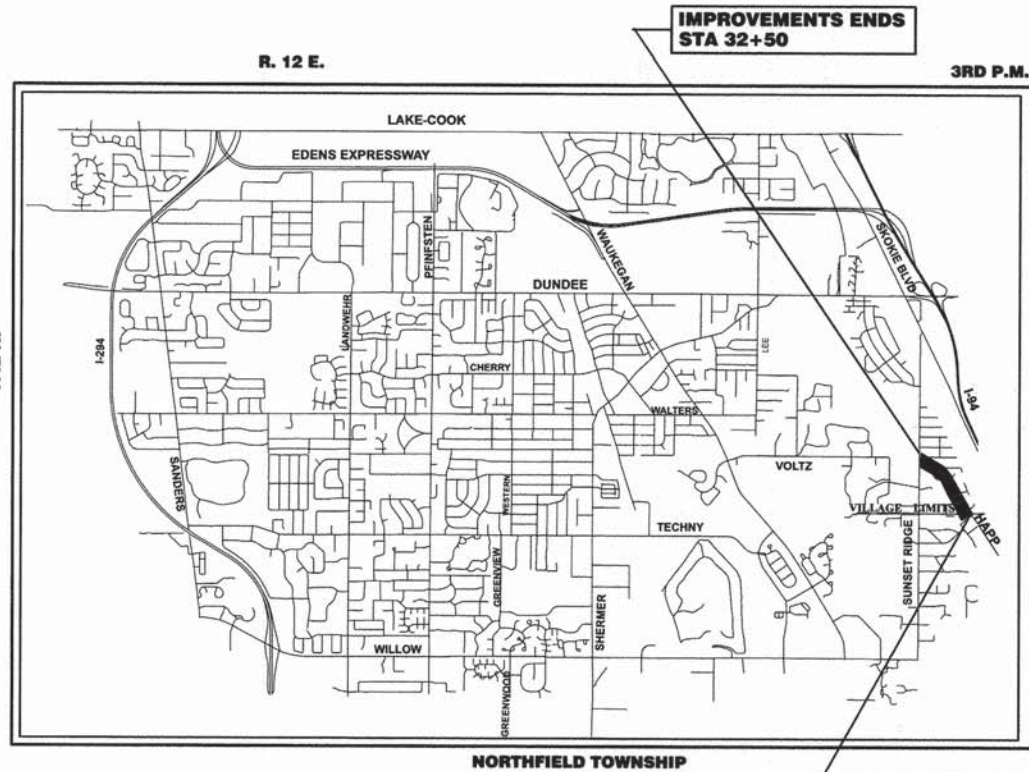


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 63883

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847) 705-4406, SCHAMBERG, IL



GROSS LENGTH OF PROJECT: 2,250 L.F. = 0.426 MILES  
NET LENGTH OF PROJECT: 2,250 L.F. = 0.426 MILES



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED *October 4, 2013*  
*PK*  
NORTHBROOK VILLAGE ENGINEER

PASSED *OCTOBER 22, 2013*  
*C. J. Holt*  
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
BASED ON LIMITED REVIEW *October 28, 2013*  
*John F. ...*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEERS SIGN & SEAL  
EXPIRES: *11/30/2015* *PK*

**PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS**



**GENERAL NOTES:**

1. THE CONTRACTOR SHALL PERFORM ALL WORK IN THIS CONTRACT IN A MANNER THAT CONFORMS WITH THE NORTHBROOK MUNICIPAL CODE. THE CONTRACTOR'S ATTENTION IS DRAWN SPECIFICALLY TO PROVISIONS OF THE MUNICIPAL CODE REGARDING CONSTRUCTION EQUIPMENT TRACKING MUD ON PUBLIC STREETS (SEC. 18-1) AND DUST CONTROL (SEC. 18-25).
2. "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO HEREAFTER AS "STANDARD SPECIFICATIONS"), AND ITS SUPPLEMENTAL REFERENCES, PUBLISHED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND THE "STANDARDS AND SPECIFICATIONS FOR PUBLIC AND PRIVATE IMPROVEMENTS", PUBLISHED BY THE VILLAGE OF NORTHBROOK, SHALL BE INCORPORATED BY REFERENCE INTO THIS CONTRACT. THE MOST RESTRICTIVE STANDARD SHALL APPLY AND ANY CONTRADICTION BETWEEN THESE REFERENCES UNRESOLVED BY THE SPECIAL PROVISIONS SHALL BE INTERPRETED BY THE ENGINEER.
3. THE ASPHALT SURFACE SHALL BE MILLED IN A MANNER THAT WILL LEAVE THE EXISTING SURFACE 2 INCHES BELOW THE EDGE OF THE GUTTER FLAG WITH A CROSS SLOPE BETWEEN 1.5% AND 2.5%.
4. THE CONTRACTOR SHALL REMOVE AND DISPOSE OF THE SHORT TERM PAVEMENT MARKING FROM THE FINAL SURFACE JUST PRIOR TO OR JUST FOLLOWING THE INSTALLATION OF THE PERMANENT PAVEMENT MARKING. THE COST OF THE REMOVAL SHALL BE INCLUDED IN THE COST OF "SHORT TERM PAVEMENT MARKING".
5. COMBINATION CONCRETE CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS DRIVEWAYS AND TRANSITIONS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THE CURB AND GUTTER TRANSITION .
6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THIS PROJECT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR FAILURE TO VERIFY EXISTING DIMENSIONS OR CONDITIONS.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR OWNER. THIS WORK SHALL BE DONE AT THE CONTRACTOR'S EXPENSE.
8. ONLY PRECAST CONCRETE ADJUSTMENT RINGS SHALL BE ALLOWED FOR THE ADJUSTMENT OF CATCH BASINS, MANHOLES, VALVE VAULTS AND INLETS.
9. CRUSHED CONCRETE SHALL NOT BE INSTALLED ON THIS CONTRACT, EXCEPT FOR TEMPORARY PURPOSES WHERE IT WILL BE COMPLETELY REMOVED FROM THE SITE.
10. THE CONTRACTOR SHALL ERECT "FRESH OIL" SIGNS PRIOR TO THE PLACEMENT OF THE PRIME COAT AND COMMENCEMENT OF PAVING OPERATIONS.
11. THE CONTRACTOR SHALL BE REQUIRED TO DISPOSE OF ALL SIDEWALKS, CURB AND GUTTER, PAVEMENT AND ALL OTHER MATERIALS EXCAVATED OR REMOVED DUE TO THE PROPOSED IMPROVEMENTS. ALL EXCESS EXCAVATED SHALL BE REMOVED FROM THE WORKSITE ON THE DAY OF ITS EXCAVATION. NO ADDITIONAL PAYMENT SHALL BE MADE FOR HAULING THESE MATERIALS OUTSIDE THE PROJECT LIMITS.
12. THE ACTUAL QUANTITIES FOR: C&G REM & REP; AND CLASS D PATCHES WILL NOT EXCEED THE QUANTITY LISTED IN THE SUMMARY OF QUANTITIES.
13. IF PAVEMENT PATCHING OPERATIONS PRECEDE ASPHALT MILLING OPERATIONS, THE CONTRACTOR SHALL PROVIDE A SMOOTH, DRIVEABLE SURFACE FREE OF ABRUPT GRADE CHANGES AT HIS/HER OWN EXPENSE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
14. ALL WORK NECESSARY TO RESTORE DISTURBED AREAS WILL NOT BE MEASURED FOR PAYMENT, BUT THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM CAUSING THE DISTURBANCE.
15. THE CONTRACTOR SHALL CONSIDER AFTER-HOURS USE OF THE STREET BY THE RESIDENTS WHEN PARKING EQUIPMENT AT THE END OF THE WORKDAY. THE ENGINEER SHALL APPROVE EQUIPMENT AND MATERIAL STORAGE LOCATION.

SUMMARY OF QUANTITIES				
SPECIALTY ITEM	ITEM NO.	DESCRIPTION	UNIT	CONSTRUCTION CODE TYPE 0005
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	940
	40600300	AGGREGATE (PRIME COAT)	TON	20
	40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL4.75, N50	TON	410
	40600982	HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	100
	40603335	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	820
	42400200	PORTLAND CEMET CONCRETE SIDEWALK, 5"	SQ FT	100
	42400800	DETECTABLE WARNINGS	SQ FT	40
	44000157	HOT MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	9,400
	44000600	SIDEWALK REMOVAL	SQ FT	100
	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	120
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	120
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	120
	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	120
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	9
	67100100	MOBILIZATION	L SUM	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
	70300100	SHORT TERM PAVEMENT MARKINGS	FOOT	2,000
X	72000100	SIGN PANEL - TYPE 1	SQ FT	50
X	72900100	METAL POST - TYPE A	FOOT	90
X	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS & SYMBOLS	SQ FT	500
X	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE, 4"	FOOT	4,000
X	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE, 6"	FOOT	4,000
X	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE, 12"	FOOT	150
X	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE, 24"	FOOT	100
	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	4
	XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	120
	XX007259	SIGN AND POST REMOVAL	EACH	10
	Z0004562	COMBINATION CONCRETE CURB & GUTTER REMOVAL & REPLACEMENT	FOOT	700
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	121

FILE NAME - :K/huff/mft/happ

USER NAME - huff	DESIGNED - J. HUFF	REVISED -
	DRAWN - J. HUFF	REVISED -
PLOT SCALE 30.000/ in	CHECKED - M. BUKOLT	REVISED -
PLOT DATE - 7-23-13	DATE - 5-9-13	REVISED -

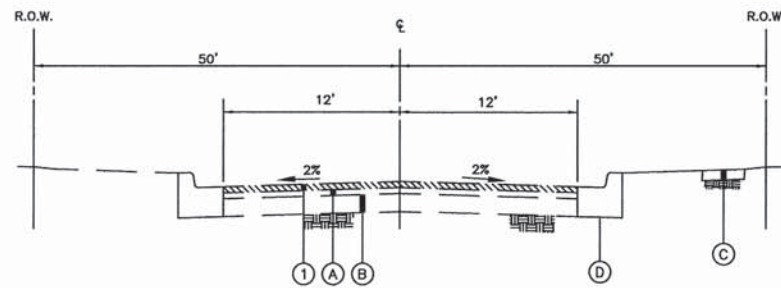
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**HAPP ROAD RESURFACING  
GENERAL NOTES AND SUMMARY OF QUANTITIES**

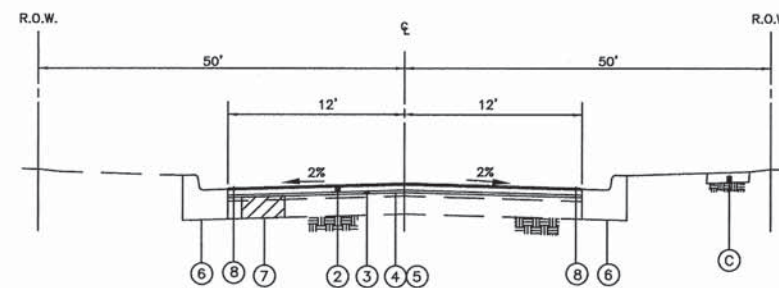
SCALE: 30 SHEET NO. 1 OF 19 SHEETS STA. 10+00 TO STA. 32+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	2
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63883	
			FED. AID PROJECT	

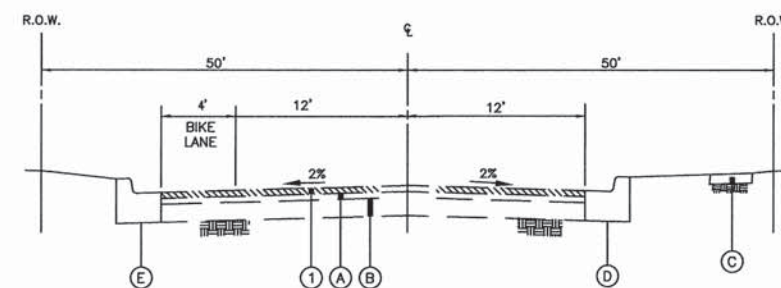




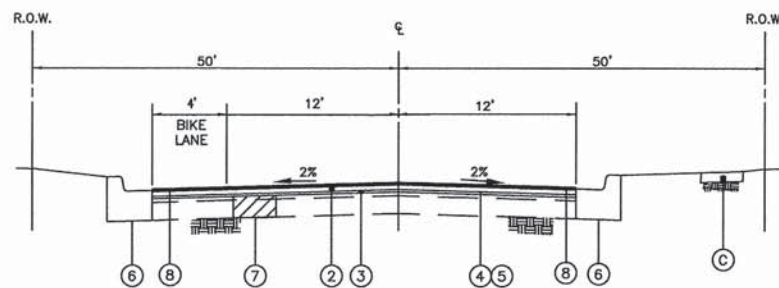
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STA. 10+00 TO 12+00, HAPP ROAD



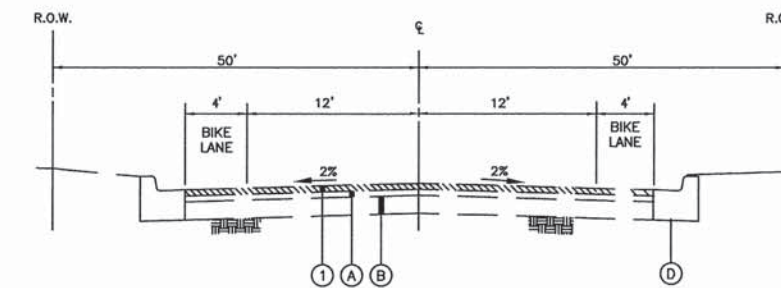
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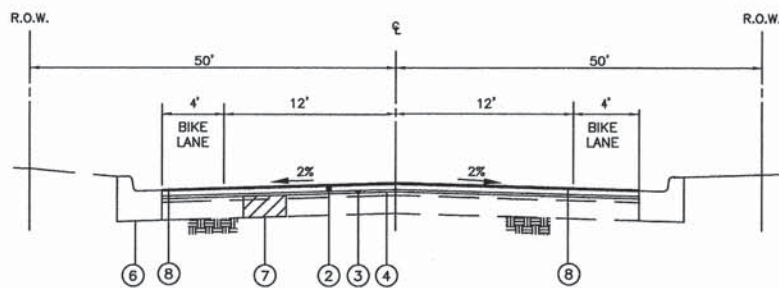
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STA. 12+00 TO 15+00, HAPP ROAD



PROPOSED TYPICAL SECTION  
STA. 12+00 TO 15+00, HAPP ROAD



EXISTING TYPICAL SECTION  
STA. 15+00 TO 31+50, HAPP ROAD



PROPOSED TYPICAL SECTION  
STA. 15+00 TO 31+50, HAPP ROAD

- (A) EXISTING ASPHALT SURFACE COURSE, 3"
- (B) EXISTING ASPHALT BASE COURSE, 7"
- (C) EXISTING P.C.C. SIDEWALK
- (D) COMB. CONC. CURB & GUTTER, TYPE B-6.24
- (1) HOT MIX ASPHALT SURFACE REMOVAL, 2"
- (2) HOT MIX ASPHALT SURFACE COURESE, MIX D, N50, IL-9.5MM, (1-1/2")
- (3) POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75MM, N50 (3/4")
- (4) BITUMINIOUS MATERIALS (PRIME COAT)
- (5) AGGREGATE (PRIME COAT)
- (6) COMB. CONC. CURB & GUTTER REMOVAL AND REPLACEMENT, AS DIRECTED BY THE ENGINEER
- (7) CLASS D PATCHES, BINDER IL-19MM, 6", AS SHOWN ON PLANS AND DIRECTED BY THE ENGINEER
- (8) HMA SURFACE COURSE TO BE 1/4" ABOVE GUTTER FLAG, (TYP.)

NOTE:  
\*\* "CONTRACTOR SHALL MILL BEFORE PATCHING".

HOT MIX ASPHALT MIXTURE TABLE		
OPERATION	MIXTURE USE	DESIGN AIR VOIDS
SURFACE COURSE	HOT MIX ASPHALT SURFACE COURESE, MIX D, N50, IL-9.5MM, (1-1/2")	4% @ 50 GYR.
BINDER COURSE	POLY LEVELING BINDER IL-4.75 (MM), N50, (3/4")	3.5% @ 50 GYR.
PATCHING	CLASS D PATCHES, TYPE I-IV, 6 IN., (TWO LIFTS)	4% @ 70 GYR.
DRIVEWAY PATCHING	HMA SURF. CSE., MIX D, IL-9.5MM, N50, 2"	4% @ 50 GYR.

- NOTE:
1. THE UNIT WEIGHT USED TO CALCULATE ALL ASPHALT MIXTURES IS 112 LBS./SQ.YD./IN.
  2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS."
  3. "FOR PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS."

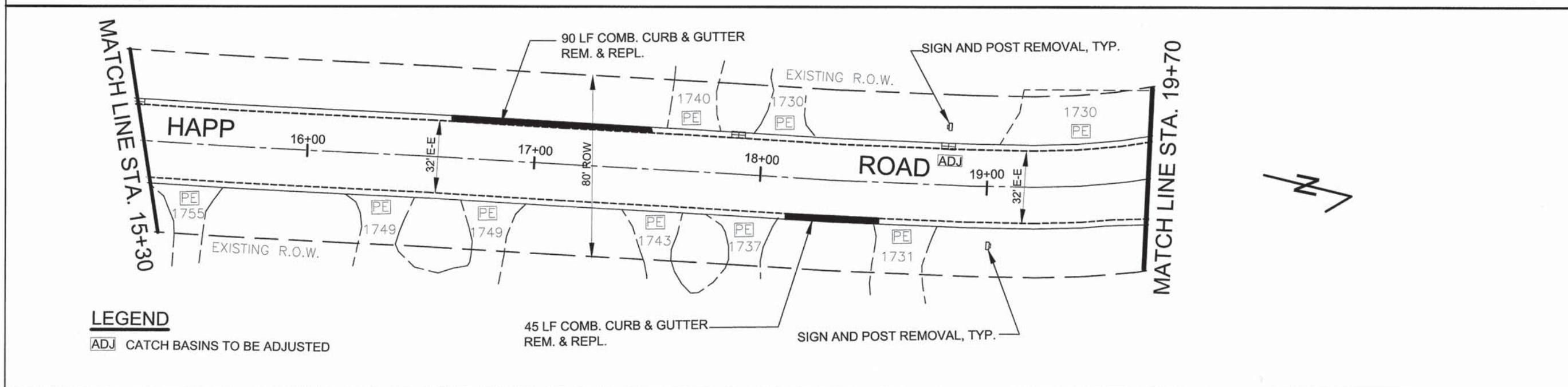
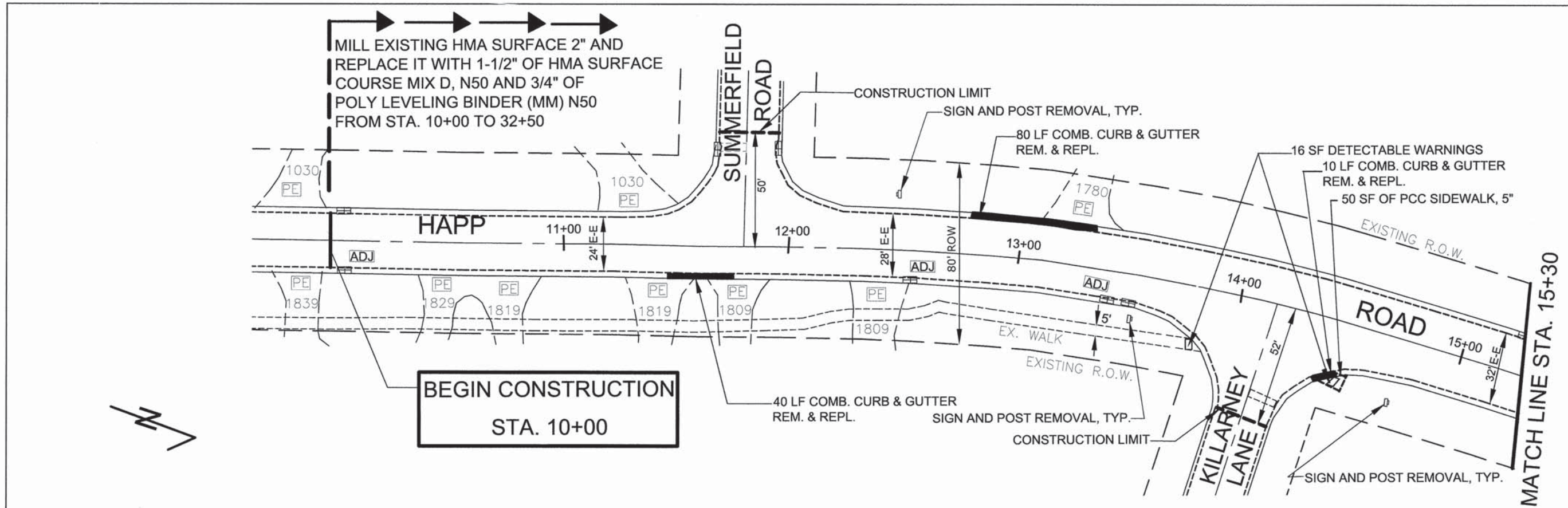
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		DRAWN - J. HUFF	REVISED -
	PLOT SCALE 30.000/in	CHECKED - M. BUKOLT	REVISED -
	PLOT DATE - 7-23-13	DATE - 5-9-13	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>HAPP ROAD RESURFACING TYPICAL SECTION</b>		
SCALE: 30	SHEET NO. 1 OF 19 SHEETS	STA. 10+00 TO STA. 32+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	3
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63883 FED. AID PROJECT	

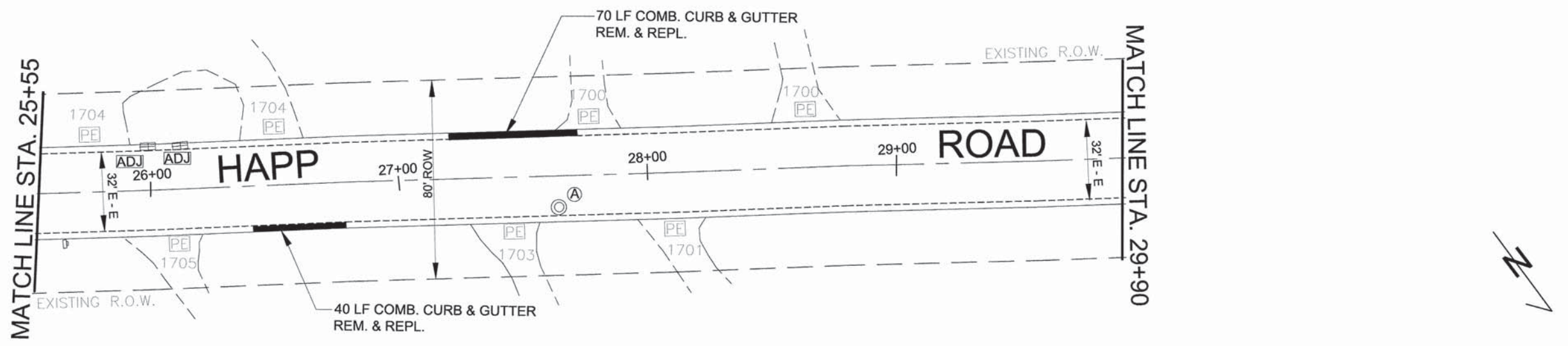
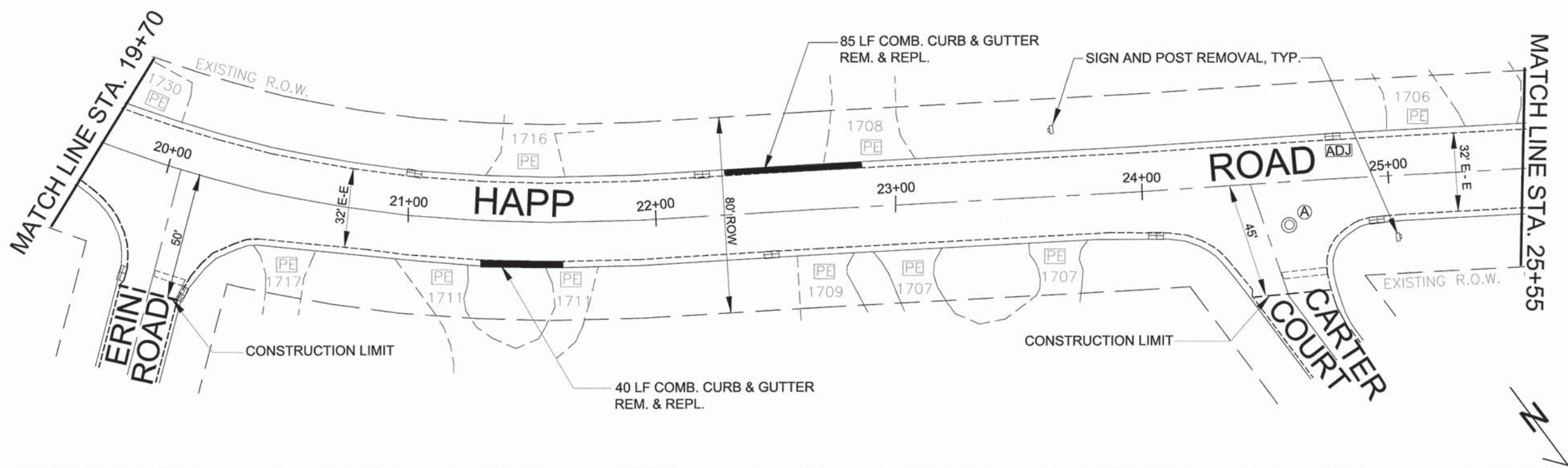




**LEGEND**  
 [ADJ] CATCH BASINS TO BE ADJUSTED

FILE NAME - :K/huff/ml/happ	USER NAME - huff	DESIGNED - J. HUFF	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTAION</b>	<b>HAPP ROAD RESURFACING EXISTING AND PROPOSED PLAN</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE 30.000/ in	DRAWN - J. HUFF	REVISED -				1039	13-00159-00-RS	COOK	19	4
	PLOT DATE - 7-23-13	CHECKED - M. BUKOLT	REVISED -				CONTRACT NO. 63883				
	DATE - 5-9-13	REVISED -		SCALE: 30	SHEET NO. 1 OF 19 SHEETS	STA. 10+00 TO STA. 19+70	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				





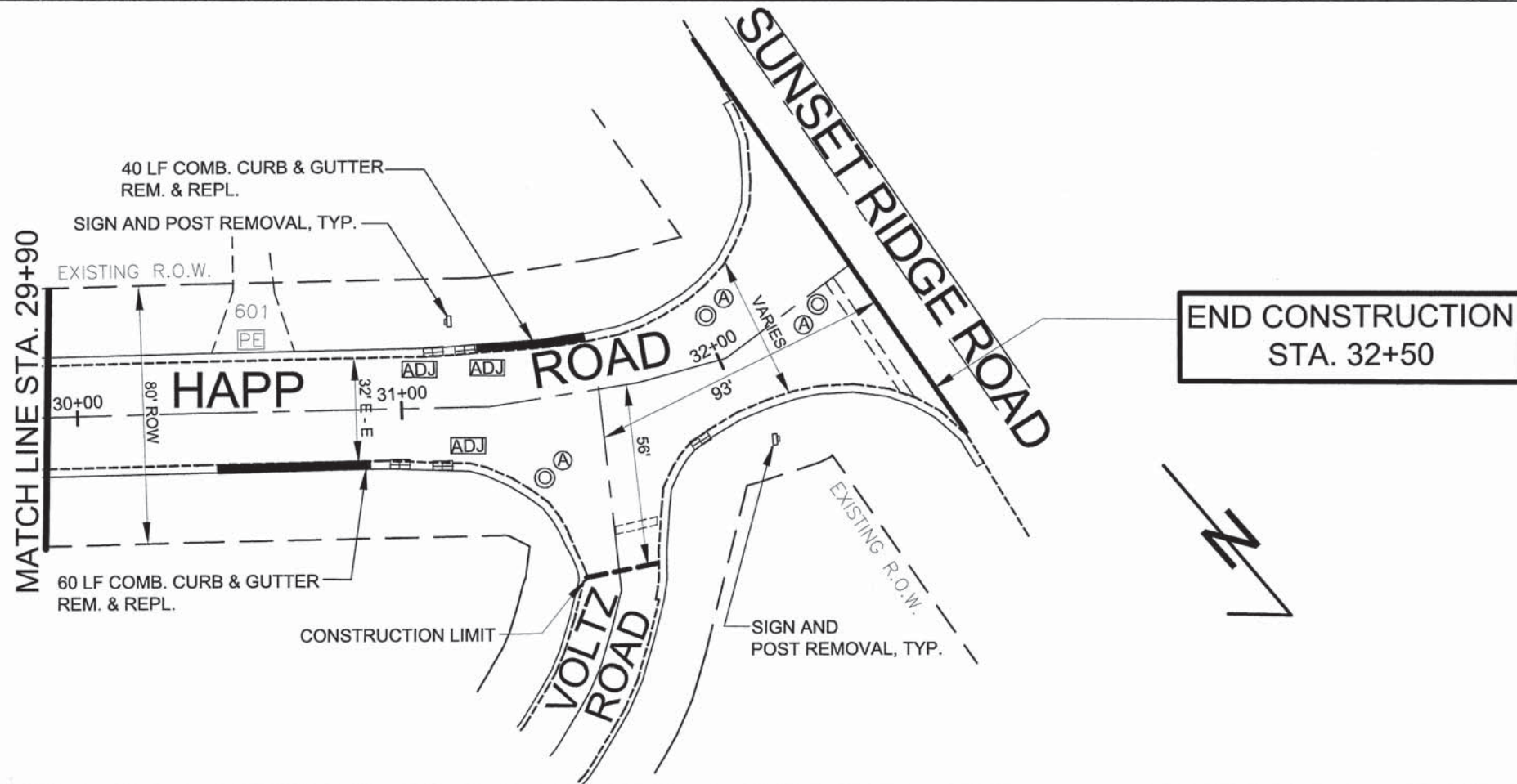
- LEGEND**
- (A) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
  - (ADJ) CATCH BASINS TO BE ADJUSTED

FILE NAME - :K:huff/mh/happ	USER NAME - huff	DESIGNED - J. HUFF	REVISED -
		DRAWN - J. HUFF	REVISED -
	PLOT SCALE 30.000/ in	CHECKED - M. BUKOLT	REVISED -
	PLOT DATE - 7-23-13	DATE - 5-9-13	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTAION**

<b>HAPP ROAD RESURFACING EXISTING AND PROPOSED PLAN</b>		
SCALE: 30	SHEET NO. 1 OF 19 SHEETS	STA. 19+70 TO STA. 29+90

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	5
CONTRACT NO. 63883			FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	



**LEGEND**

- (A) FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- (ADJ) CATCH BASINS TO BE ADJUSTED

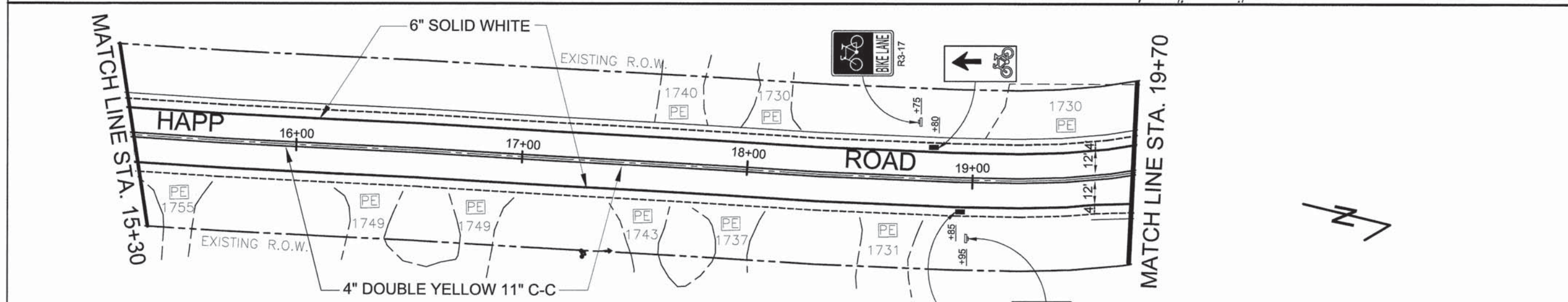
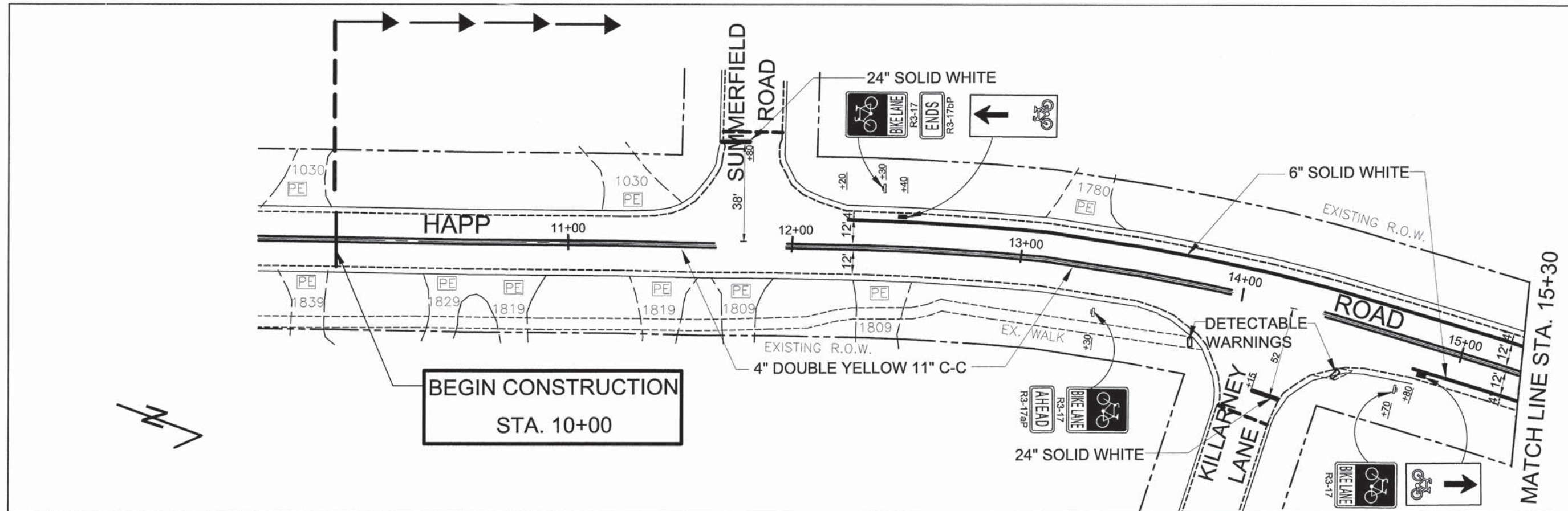
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	PLOT DATE - 7-23-13	DATE - 5-9-13	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>HAPP ROAD RESURFACING EXISTING AND PROPOSED PLAN</b>		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		1039	13-00159-00-RS	COOK	19	6
SCALE: 30	SHEET NO. 1 OF 20 SHEETS	STA. 29+90 TO STA. 32+50	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			

<b>CONTRACT NO. 63883</b>	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	

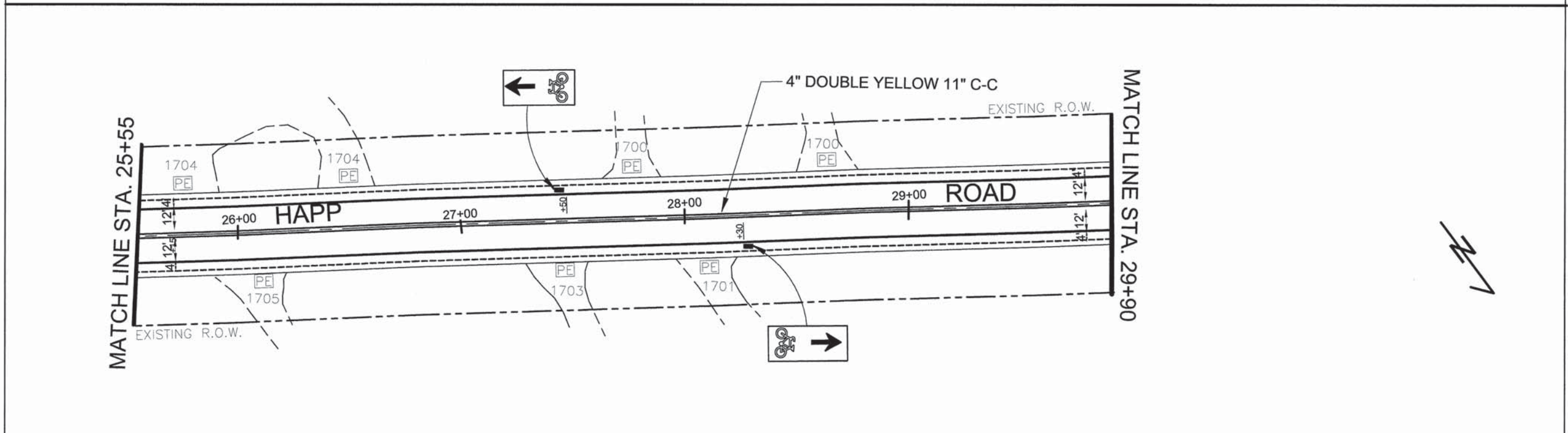
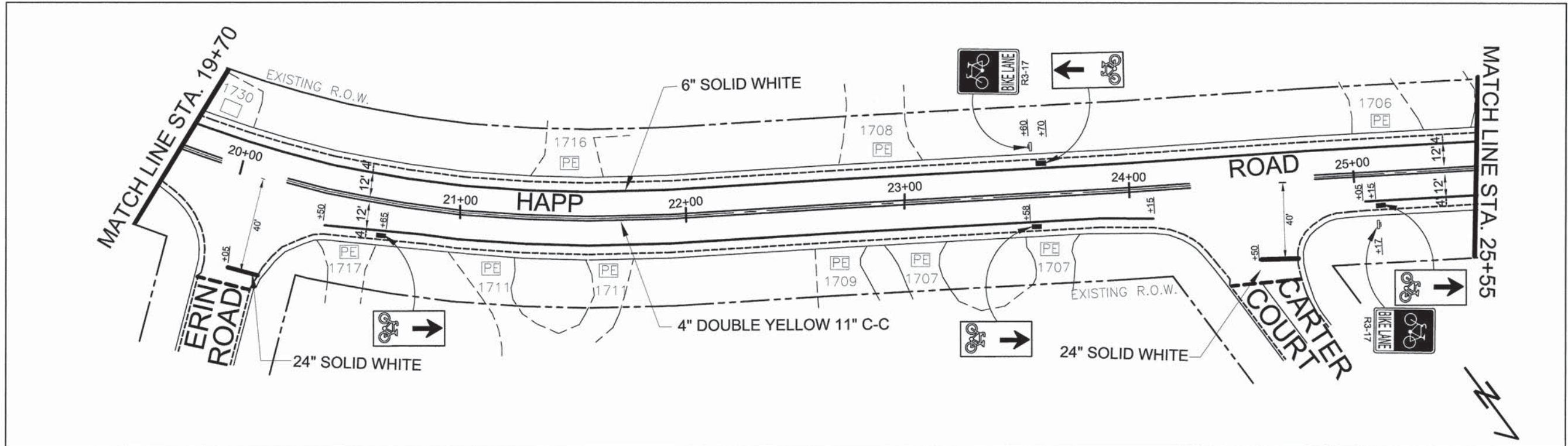




**NOTES:**  
 1. ALL PAVEMENT MARKING MUST BE THERMOPLASTIC

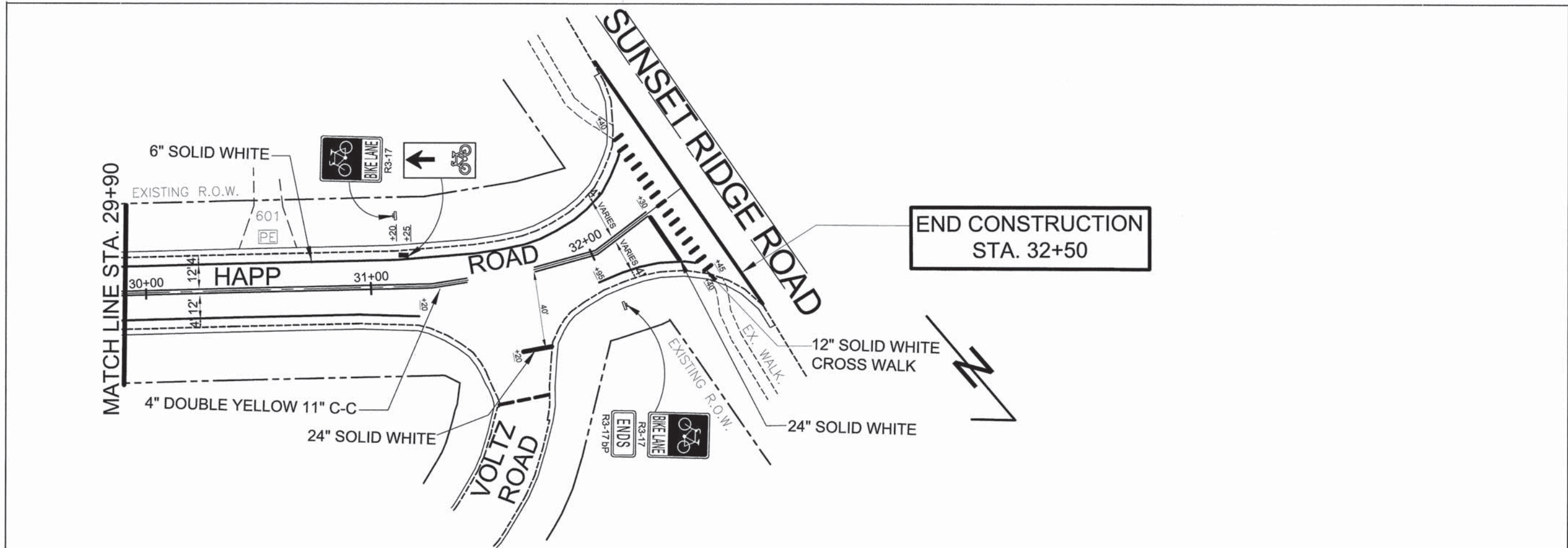
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	PLOT SCALE 30,000/ in	DRAWN - J. HUFF	REVISED -		1039	13-00159-00-RS	COOK	19	7			
	PLOT DATE - 7-23-13	CHECKED - M. BUKOLT	REVISED -		SCALE: 30    SHEET NO. 1 OF 19 SHEETS    STA. 10+00 TO STA. 19+70			CONTRACT NO. 63883				
	DATE - 5-9-13	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT			





FILE NAME - :K/huff/m/happ	USER NAME - huff	DESIGNED - J. HUFF	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>HAPP ROAD RESURFACING EXISTING AND PAVEMENT MARKINGS</b>			FAU. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE 30.000/ in	DRAWN - J. HUFF	REVISED -		1039	13-00159-00-RS	COOK	19	8			
	PLOT DATE - 7-23-13	CHECKED - M. BUKOLT	REVISED -		SCALE: 30      SHEET NO. 1 OF 19 SHEETS      STA. 19+70 TO STA. 29+90			CONTRACT NO. 63883				
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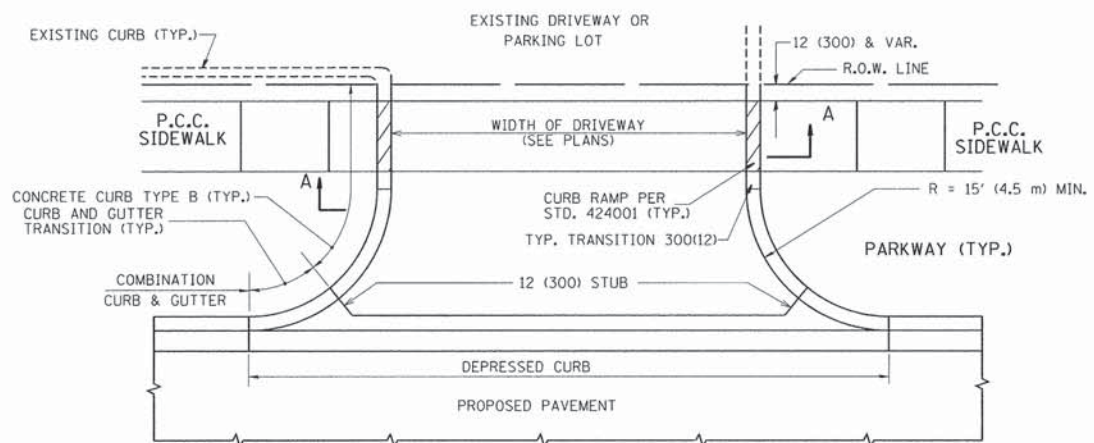
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTAION**

**HAPP ROAD RESURFACING  
EXISTING AND PAVEMENT MARKINGS**

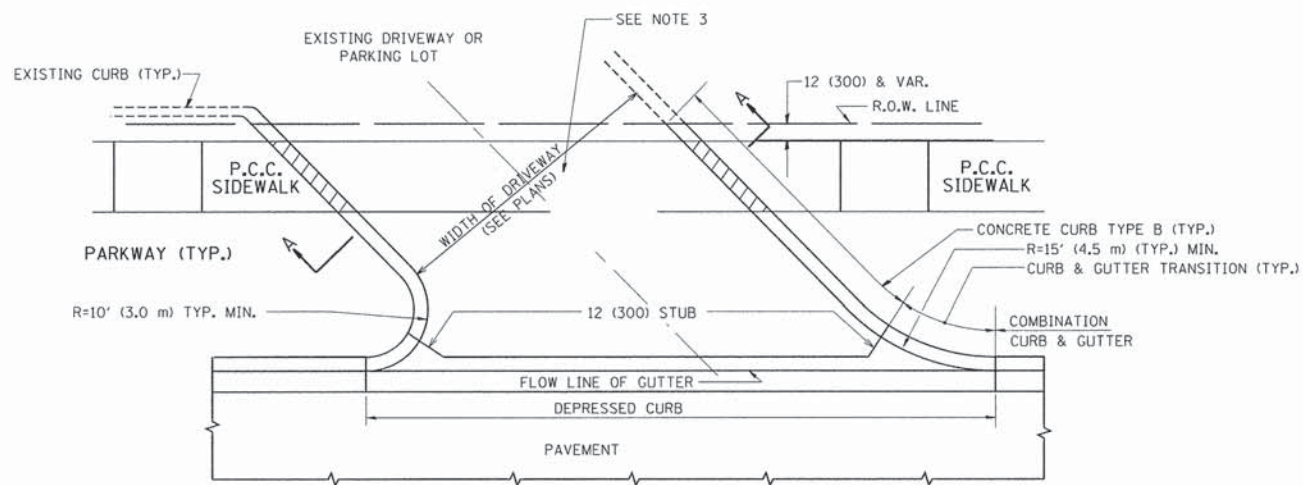
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FAU. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	9
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

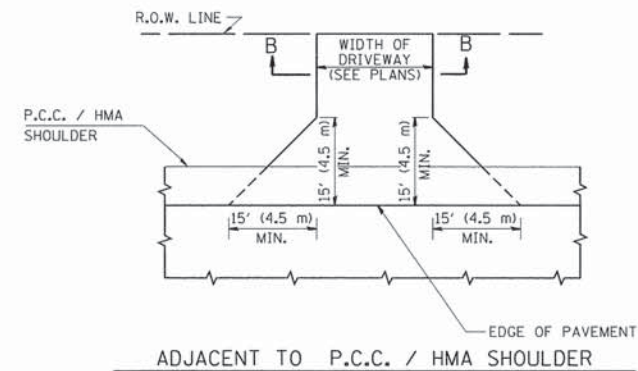
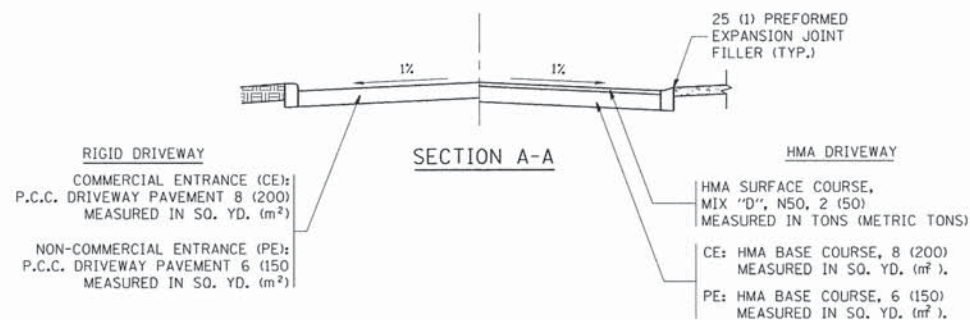




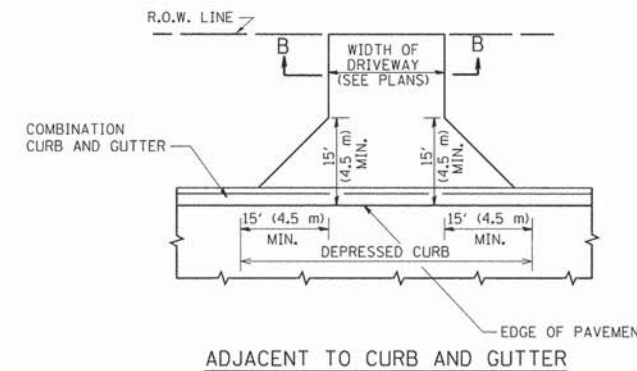
WITH CONCRETE CURB, TYPE B



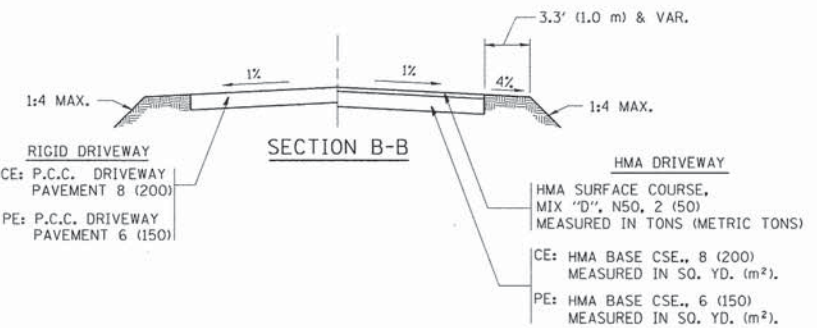
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)  
 HMA SURFACE COURSE,  
 MIX "D", N50, 2 (50)  
 MEASURED IN TONS (METRIC TONS)  
 AGGREGATE BASE CSE., TYPE B, 8 (200)  
 MEASURED IN SO. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

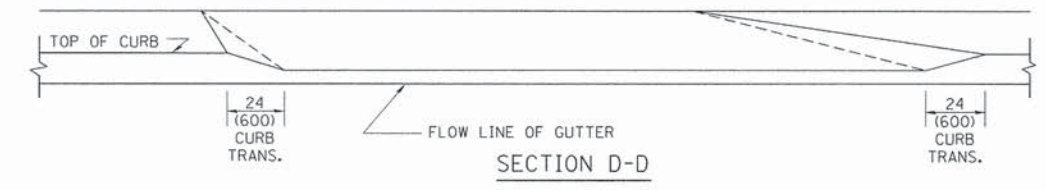
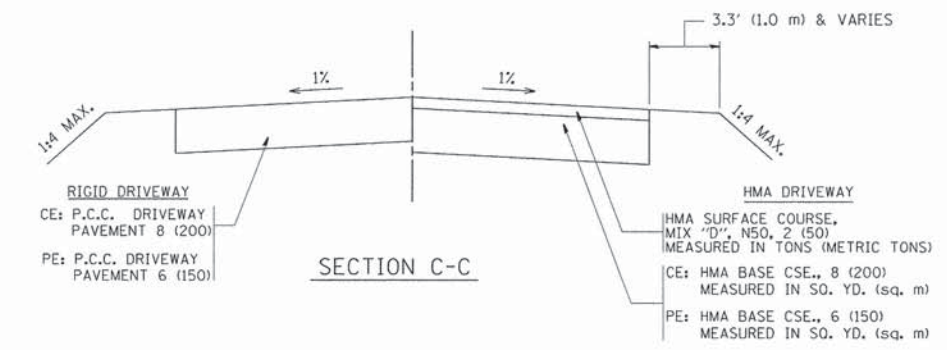
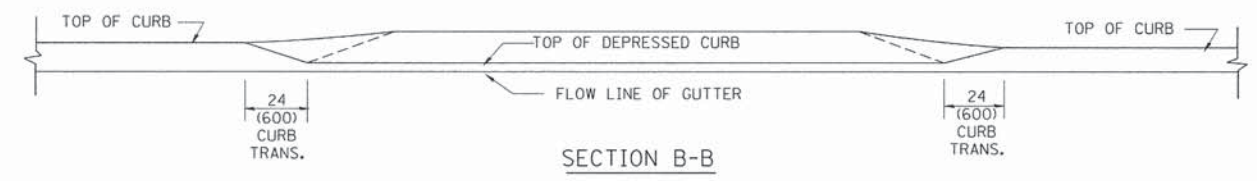
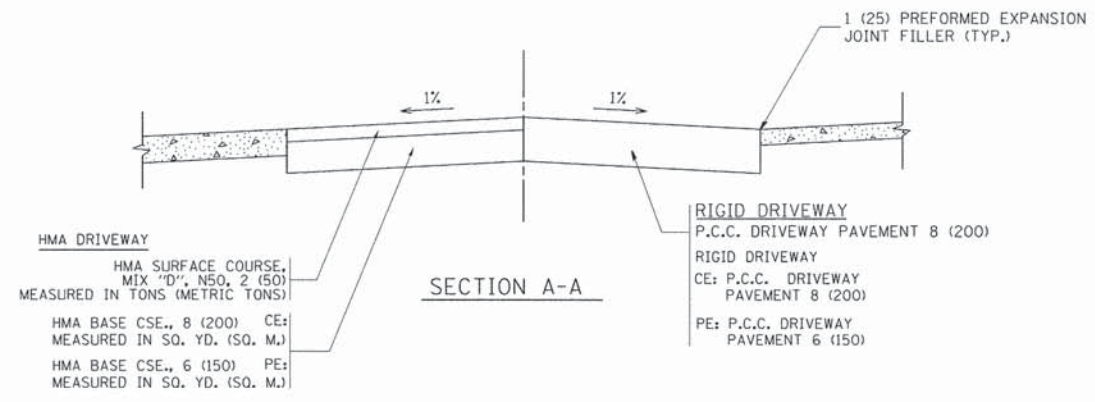
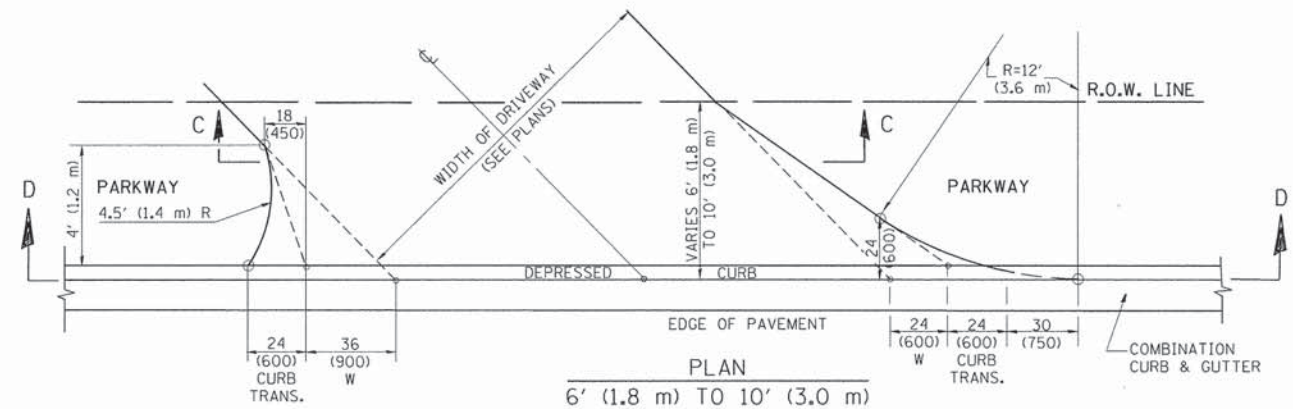
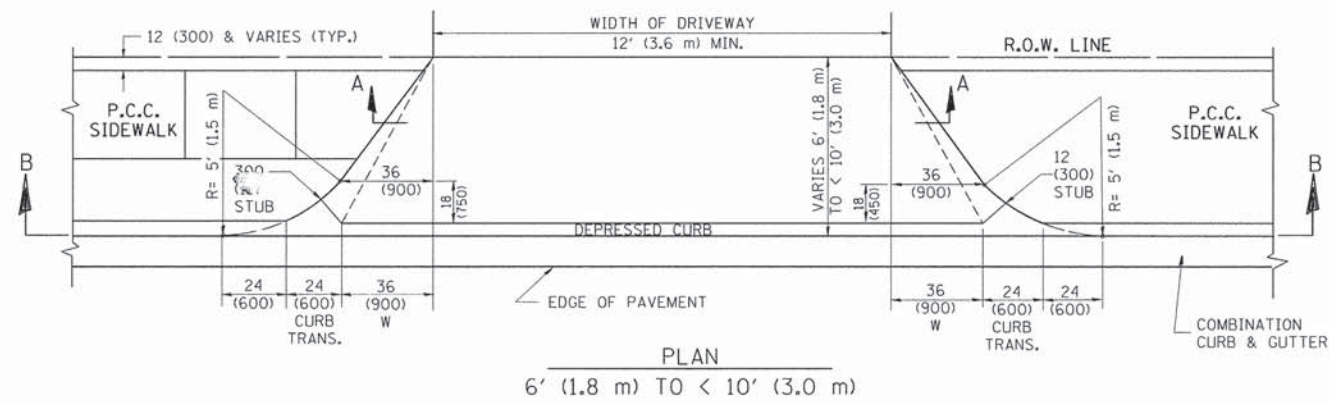
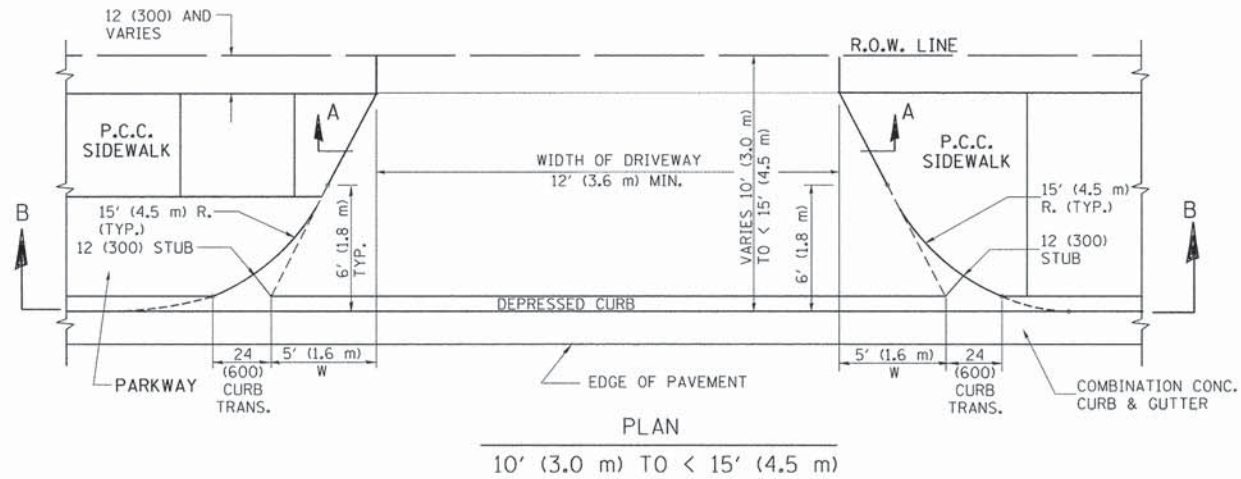
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	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 9/6/2011	DATE - 11-04-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	10
BD0156-07 (BD-01)		CONTRACT NO. 63883		
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				





GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

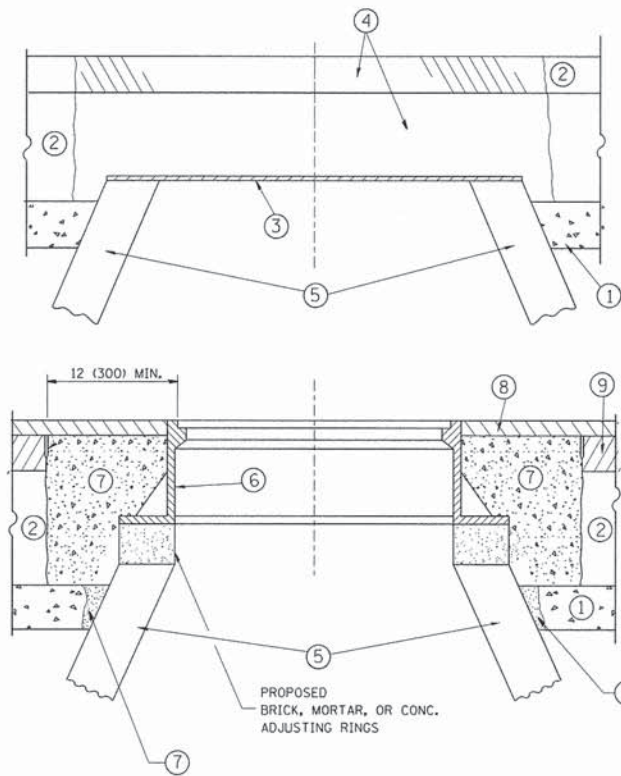
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PLOT SCALE = 50.0000' / 1"	DATE - 11-06-95	REVISED - R. BORO 09-06-11	
PLOT DATE = 10/28/2011			

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	11
BD400-02 (BD-02)			CONTRACT NO. 63883	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

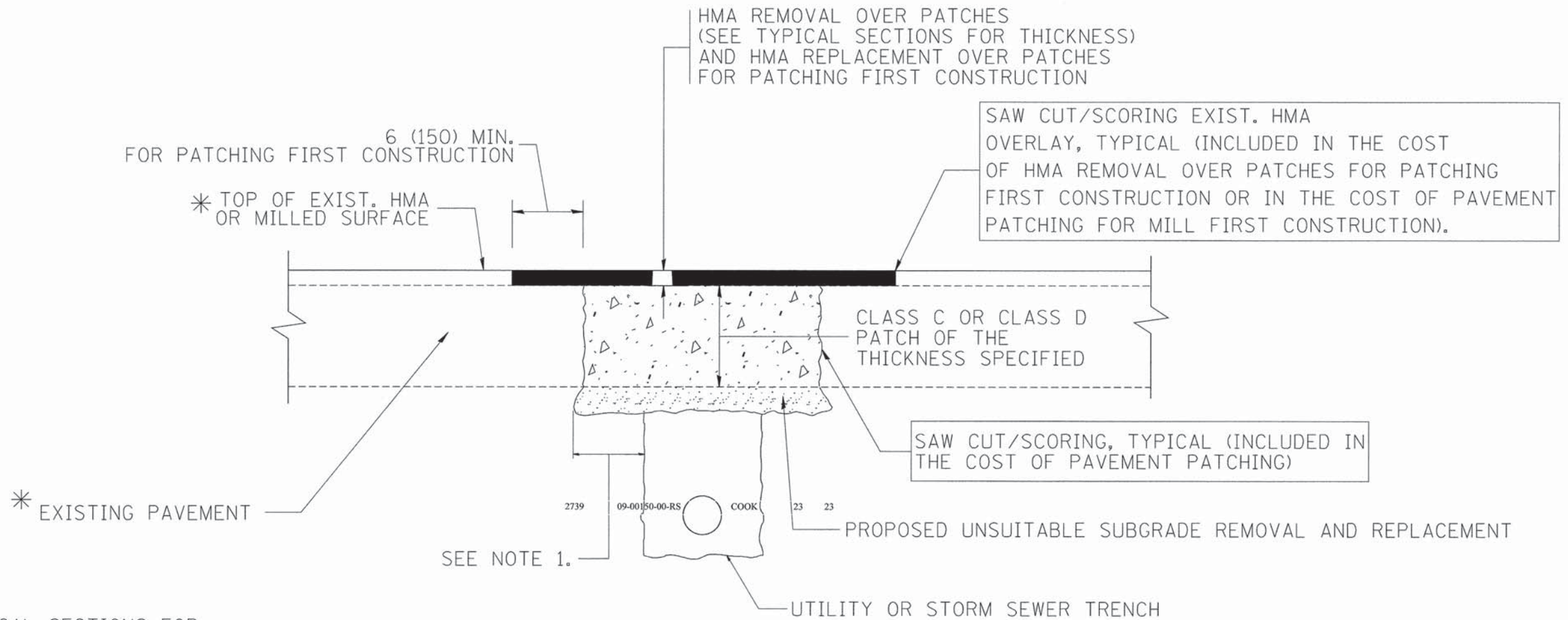
WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			F.A.M. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 1/648.5000" = 1" = 60'		CHECKED -	REVISED - R. BORO 03-09-11		<b>BD600-03 (BD-8)</b>			<b>CONTRACT NO. 63883</b>				
PLOT DATE = 12/6/2011		DATE - 10-25-94	REVISED - R. BORO 12-06-11		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

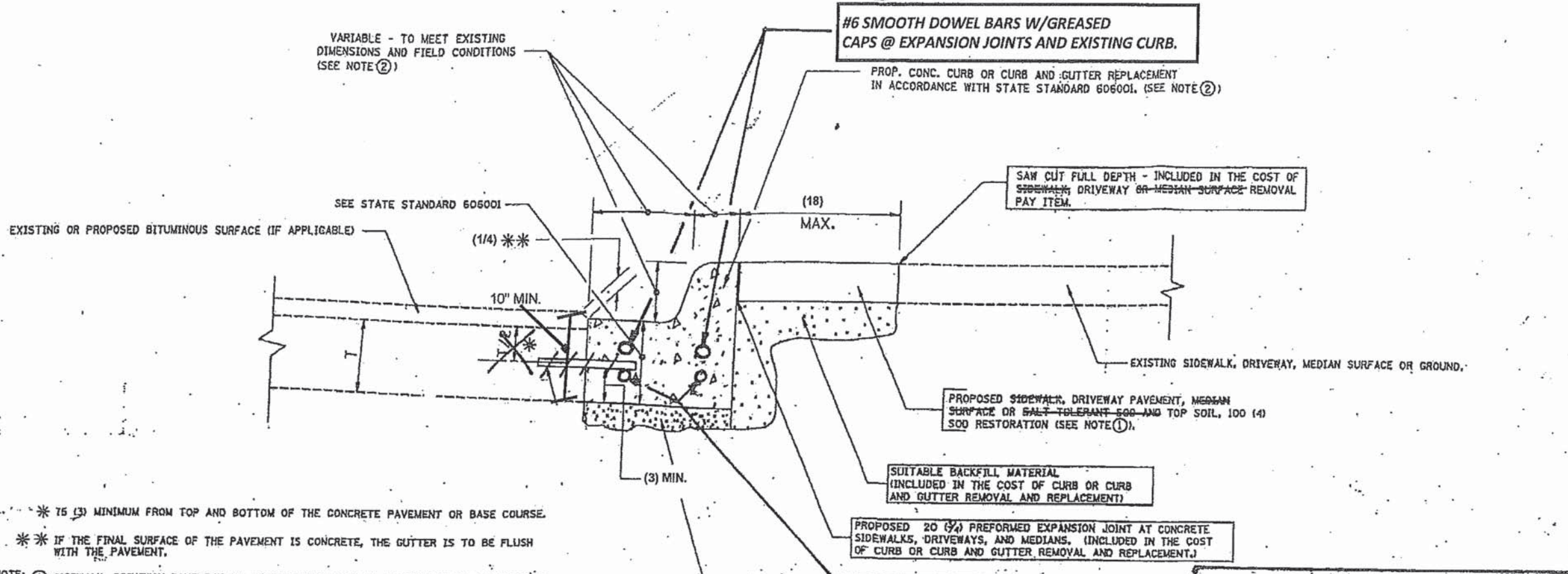
**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>		F.A.M. RTE. 1039	SECTION 13-00159-00-RS	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 13	
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	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
			REVISED - K. ENG 10-27-08									





VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE 2)

#6 SMOOTH DOWEL BARS W/GREASED CAPS @ EXPANSION JOINTS AND EXISTING CURB.

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE 2)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT-TOLERANT 500 AND TOP SOIL, 100 (4) SOD RESTORATION (SEE NOTE 1).

SUIABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 20 (3/4) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

**SUBBASE:**  
ALL CURB AND GUTTER TO BE REMOVED AND REPLACED AT LOCATIONS TO BE DETERMINED BY THE ENGINEER MUST HAVE 3-INCHES OF GRANULAR CA-6 SUBBASE. ALL EXCAVATION WORK FOR THE ADDITIONAL DEPTH AND PLACEMENT OF THE MECHANICALLY COMPACTED SUBBASE SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

2- CONTINUOUS #4 STEEL REINFORCING BARS INTERRUPTED AT EXPANSION JOINTS ONLY. (EPOXY COATED)

EXPANSION JOINTS AT 60' INTERVALS. CONTRACTION JOINTS AT 15' INTERVALS. USE CLASS SI STATE APPROVED P.C. CONC.

THE COST OF THE #6 SMOOTH DOWEL BARS & CONTINUOUS #4 EPOXY-COATED STEEL REINFORCING BARS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT"

EXISTING OR PROPOSED BITUMINOUS SURFACE (IF APPLICABLE)

SEE STATE STANDARD 606001

(1/4) \*\*

10" MIN.

(18) MAX.

(3) MIN.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

- \*\* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT-TOLERANT 500 AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

FILE NAME - :k/huff/mft/hopp	USER NAME - huff	DESIGNED - J. HUFF	REVISED -
		DRAWN - J. HUFF	REVISED -
	PLOT SCALE 30.000/in	CHECKED - M. BUKOLT	REVISED -
	PLOT DATE - 7-23-13	DATE - 5-9-13	REVISED -

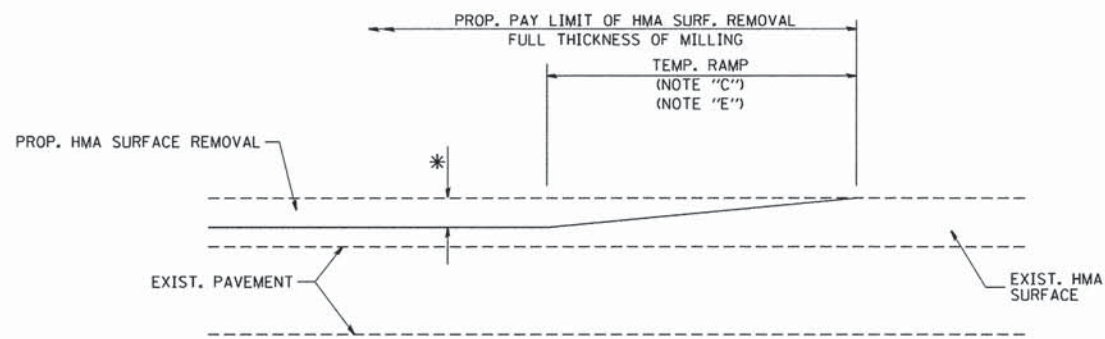
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

HAPP ROAD RESURFACING  
CURB AND GUTTER DETAIL

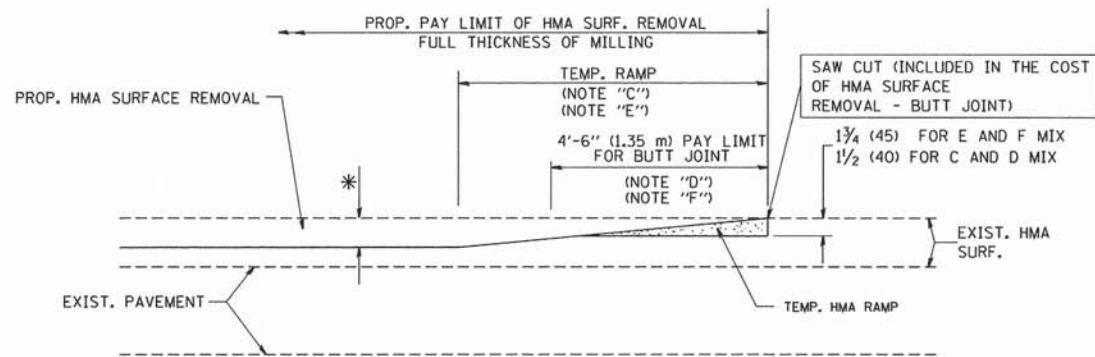
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 63883	
			FED. AID PROJECT	



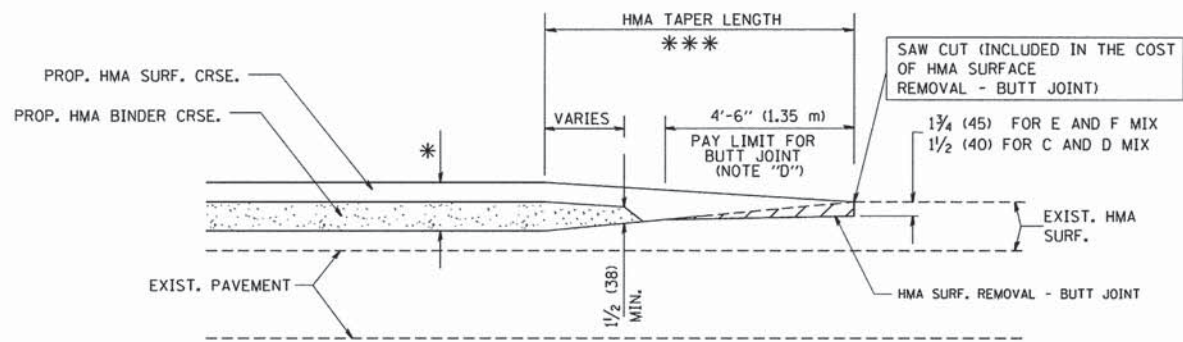


OPTION 1



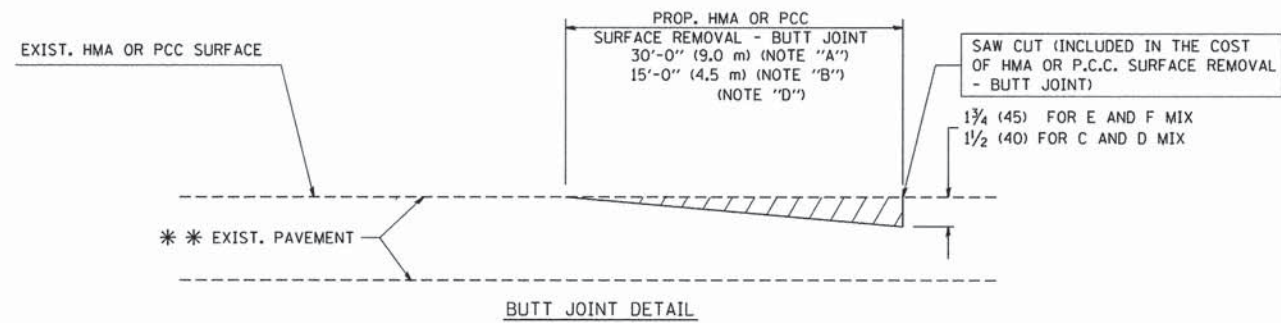
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2  
TYPICAL TEMPORARY RAMP

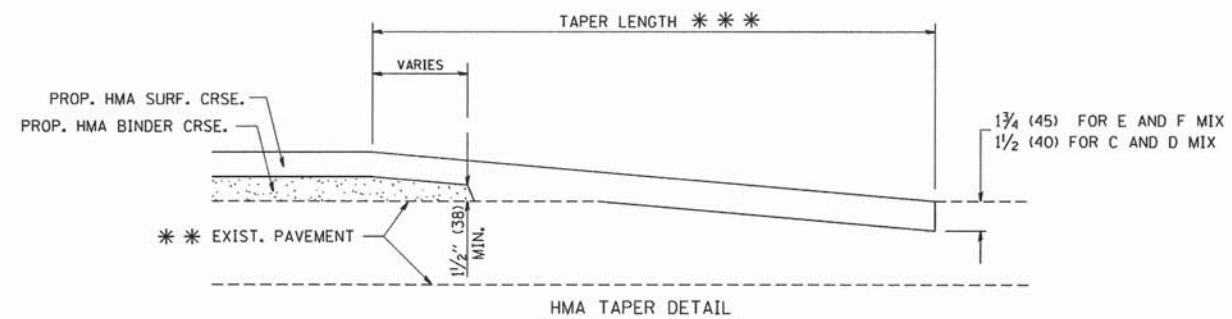


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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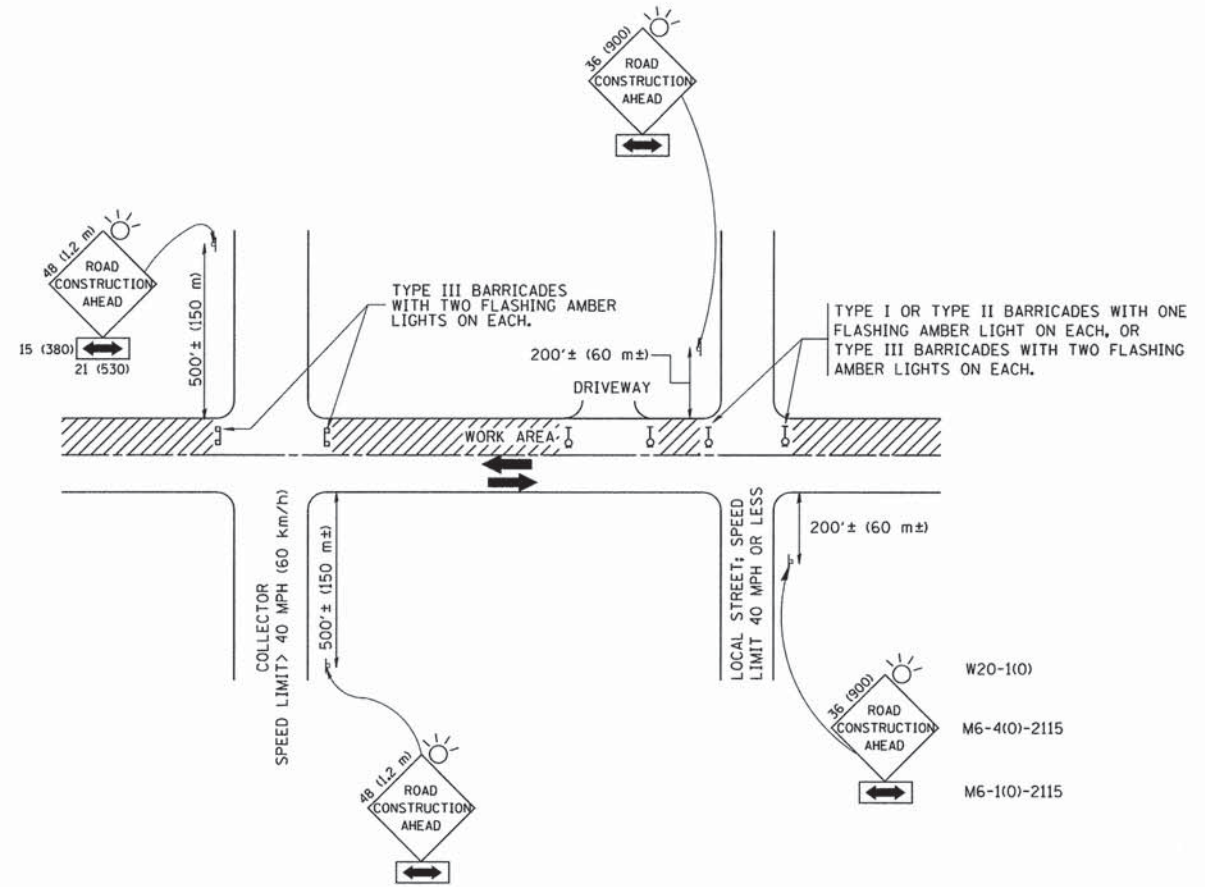
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PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
		REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.	
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	15
BD400-05 BD32		CONTRACT NO. 63883		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





### TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

#### NOTES:

##### A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

##### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\td18.dgn	USER NAME = geglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50,000 % / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

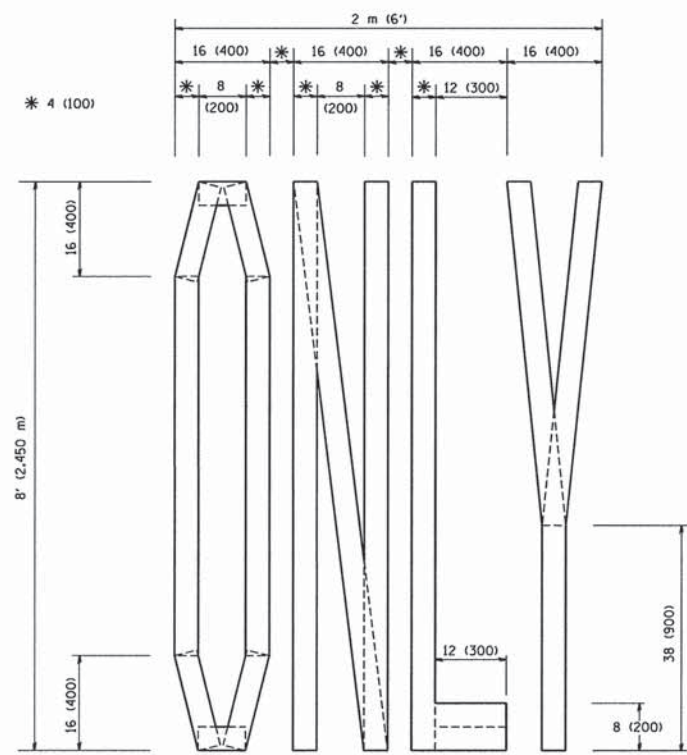
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1039	SECTION 13-00159-00-RS	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 16
TC-10			CONTRACT NO. 63883	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

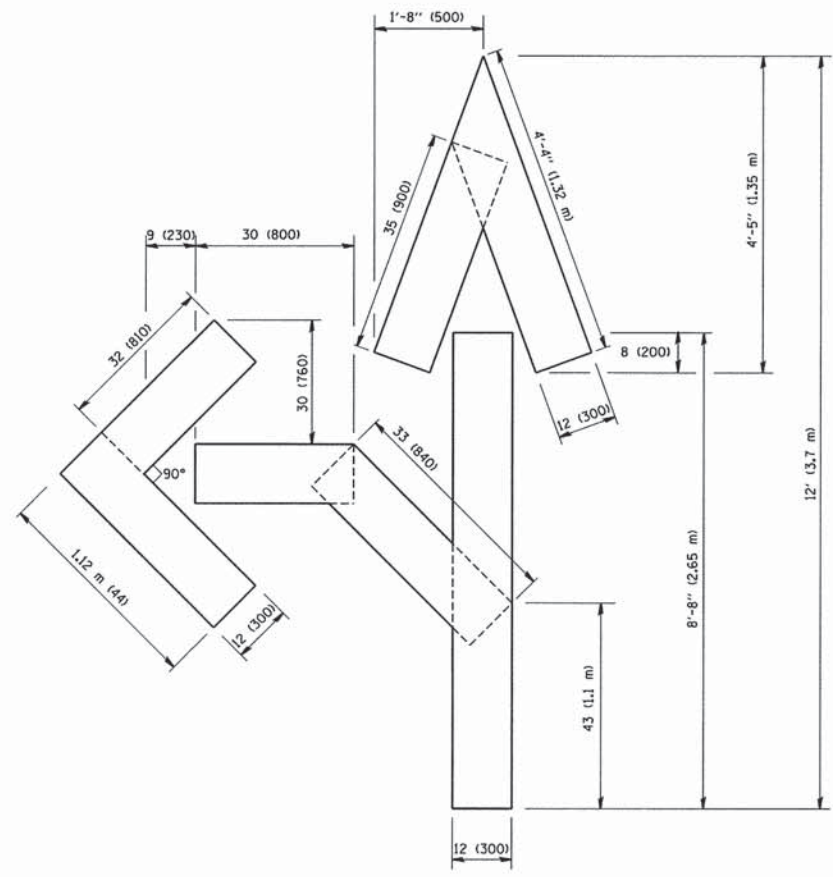




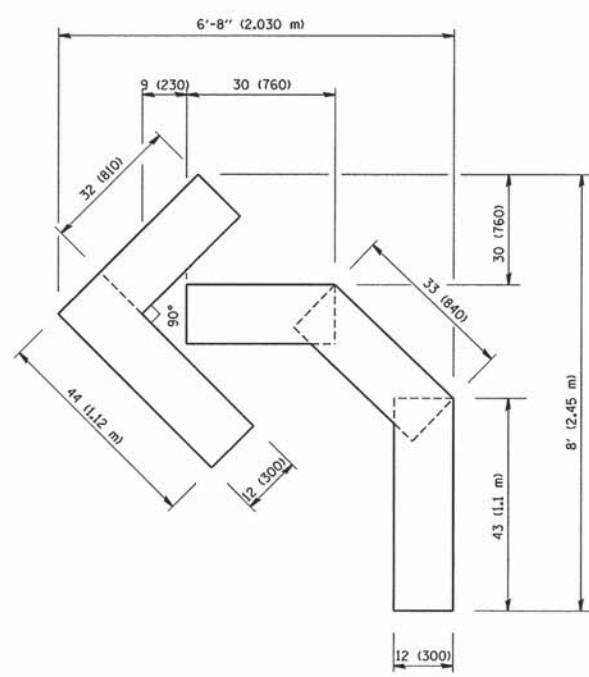




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =  
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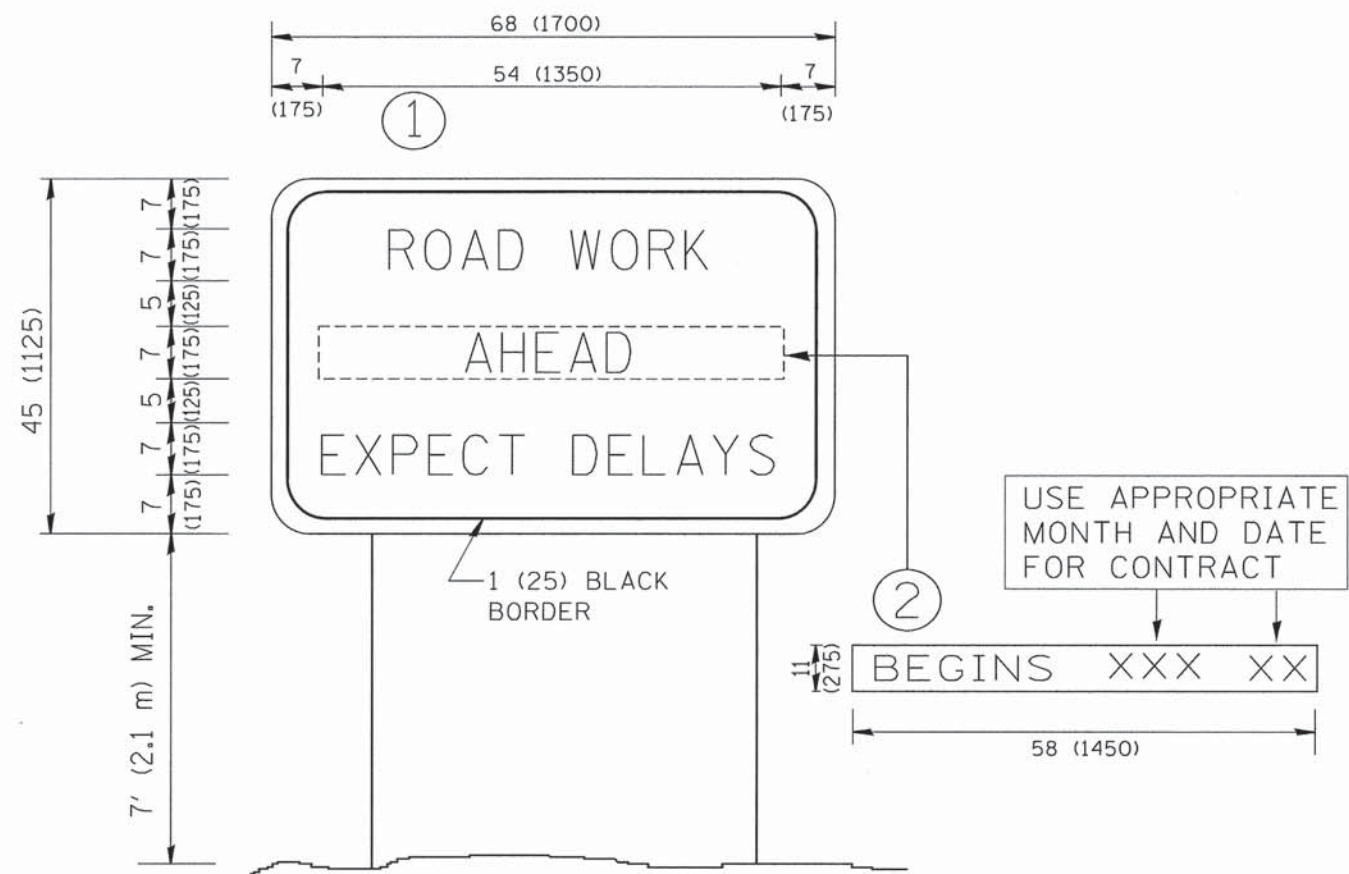
USER NAME = geglianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
PLOT SCALE = 50.0000 "/ IN.	DRAWN -	REVISED -T. RAMMACHER 11-04-97
PLOT DATE = 1/4/2008	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1039	13-00159-00-RS	COOK	19	18
TC-16			CONTRACT NO. 63883	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\dstatd\22+34\to22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.V. RTE. 1039	SECTION 13-00159-00-RS	COUNTY COOK	TOTAL SHEETS 19	SHEET NO. 19
TC-22		CONTRACT NO. 63883		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				