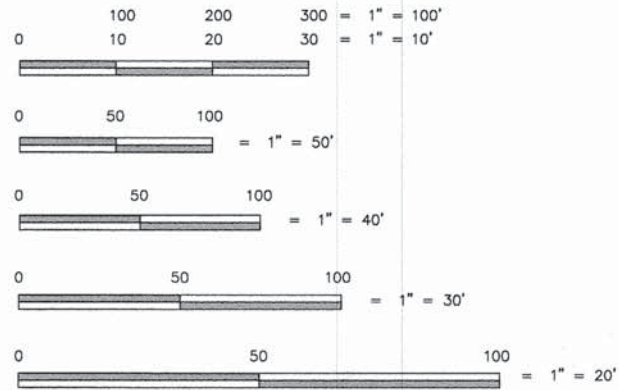


FOR INDEX OF SHEETS
SEE SHEET NO. 2

	STATE STREET
2013 ADT -	3,010
POSTED SPEED LIMIT -	35 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	35 mph
STREET CLASSIFICATION -	MAJOR COLLECTOR



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZE PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED SIZE PLANS, THE ABOVE SCALES MAY BE USED.

Call Before You Dig
JULIE
ILLINOIS ONE-CALL SYSTEM
Call: 811 OR 1-800-892-0123

JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
48 hours before you dig (EXCLUDING SAT., SUN. & HOL.)
811

CONTRACT NO. 63889

ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-002235

TECH 3 Consulting Group, Inc.

ENGINEERS SURVEYORS
737 West Exchange St. Crete, IL 60417
ph 708.672.4994 fax 708.672.3739

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

FAU ROUTE 2916 (STATE STREET)
(BERK LANE) TO FAU 1635 (STEGER ROAD)

ROADWAY RESURFACING
PROJECT NO.: M-4003(206)

L.A. SECTION NO.: 13-00048-00-RS

VILLAGE OF CRETE

WILL COUNTY

JOB NO. C-91-372-13

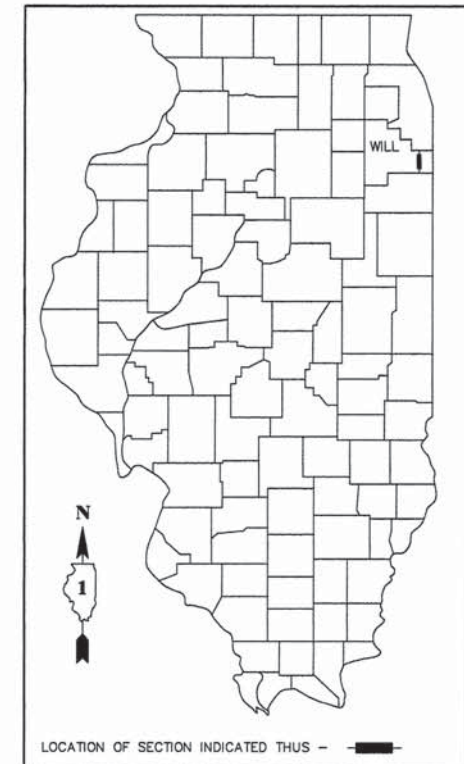
R 14 E 3RD PRINCIPAL MERIDIAN



GROSS AND NET LENGTH OF IMPROVEMENT = 1,170 LINEAL FEET OR 0.22 MILES

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 63889		

CONTRACT # 63889



LOCATION OF SECTION INDICATED THUS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED NOVEMBER 1 20 13

Michael Bond

PRESIDENT, VILLAGE OF CRETE

PASSED NOVEMBER 12 20 13

C. J. Holt

DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW

NOVEMBER 12 20 13

John Fortman

DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

Robert W. Matthey

ROBERT W. MATTHEY, P.E.
LICENSE NO. 062 - 39765
LICENSE EXPIRES: 11/30/15
TECH 3 CONSULTING GROUP, INC.



PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

SHEET NO.

INDEX OF SHEETS

DESCRIPTION

1. COVER SHEET
2. INDEX OF SHEETS, HIGHWAY STANDARDS & GENERAL NOTES
3. SUMMARY OF QUANTITIES
4. TYPICAL SECTIONS
- 5.-6. PAVEMENT PLAN - STA. 1+00 TO 12+70
- 7.-8. PAVEMENT MARKING PLAN - STA. 1+00 TO 12+70
9. BUTT JOINT AND HMA TAPER DETAIL (BD-32)
10. RAISED REFLECTIVE PAVEMENT MARKINGS DETAIL (TC-11)
11. DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
12. TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS DETAIL (TC-10)
13. PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
14. PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)
15. DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS LOOP DETECTORS (TS-05a)
16. DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)

INDEX OF STATE STANDARDS

- | | |
|-----------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 701306-03 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY |
| 701901-03 | TRAFFIC CONTROL DEVICES |
| 701701-09 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS |
| 701606-09 | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED).
2. UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTOR'S USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
4. THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NORMAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
5. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR AT THEIR EXPENSE.
6. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF CRETE.
7. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN PLANS, UNLESS OTHERWISE SPECIFIED.

USER NAME =	DESIGNED - RWM	REVISED - SEPT. 24, 2013
	DRAWN - EGH	REVISED -
PLOT SCALE =	CHECKED - WJS	REVISED -
PLOT DATE =	DATE - AUGUST 19, 2013	REVISED -



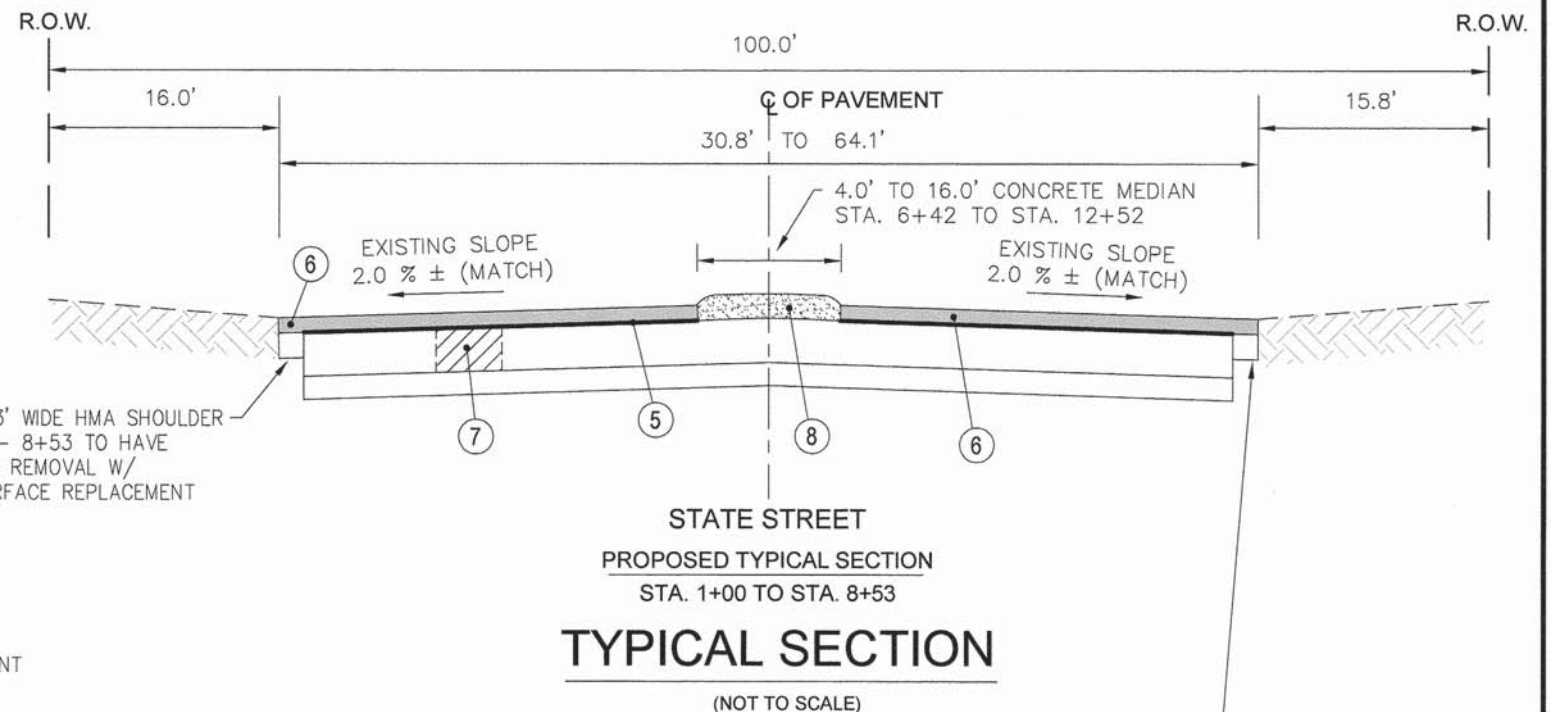
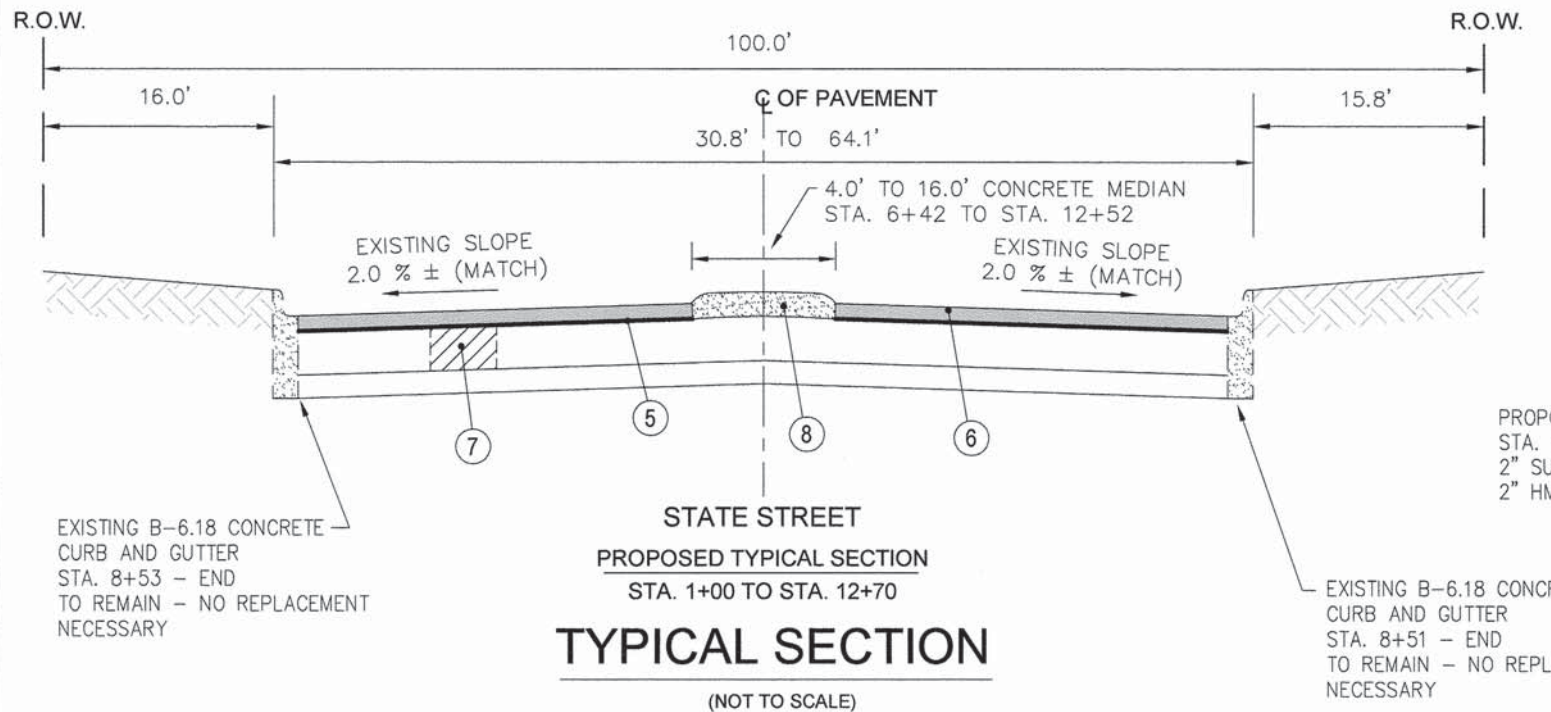
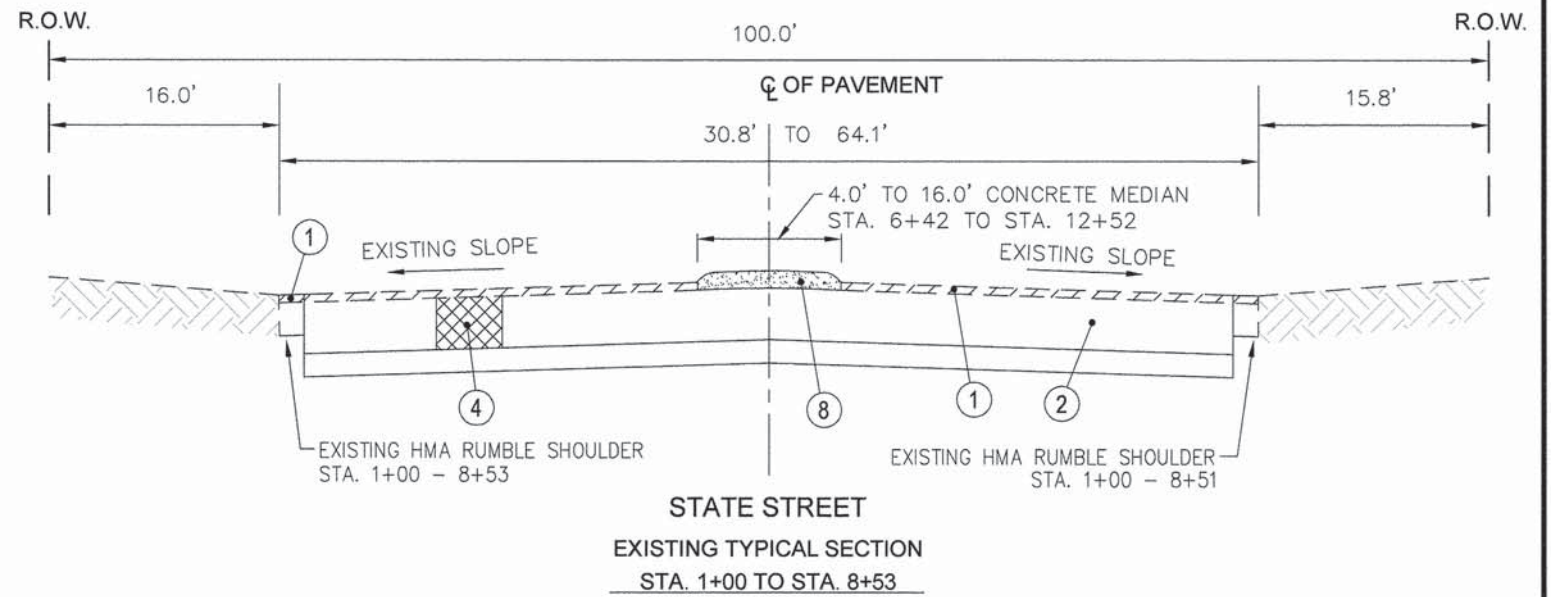
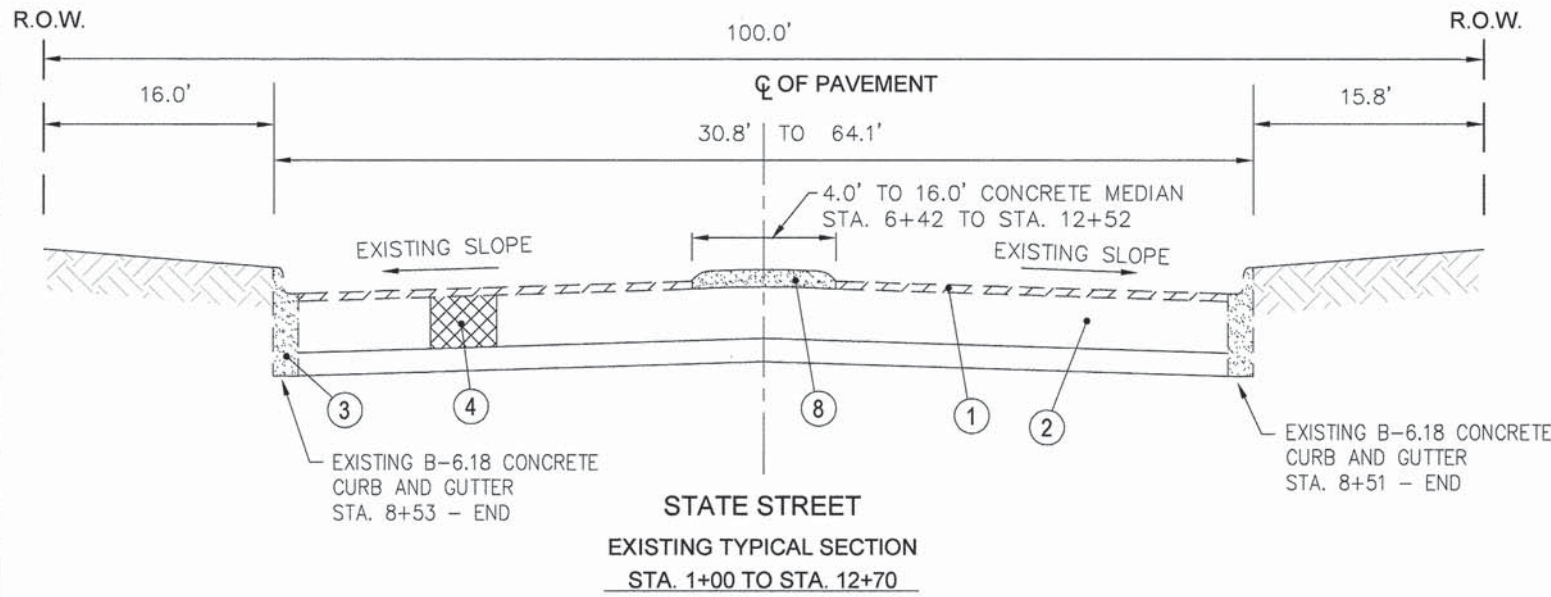
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00RS	WILL	16	2
CONTRACT NO. 63889				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT:	

SUMMARY OF QUANTITIES

S. I.	PAY ITEM CODE #	DESCRIPTION	UNIT	QUANTITY	CONSTRUCTION TYPE CODE 0005
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	545	545
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ. YD.	335	335
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	780	780
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ. YD.	6800	6800
	44201705	CLASS D PATCHES, TYPE II , 5 INCH	SQ. YD.	10	10
	67100100	MOBILIZATION	L SUM	1	1
	70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
*	78001100	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ. FT.	37	37
*	78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	3630	3630
*	78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	170	170
*	78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	40	40
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	66	66
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	64	64
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	185	185
	X6030310	FRAME AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2

* INDICATES SPECIALTY ITEMS (S.I.)





TYPICAL SECTION
(NOT TO SCALE)

TYPICAL SECTION
(NOT TO SCALE)

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS (%)
PAVEMENT RESURFACING:	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm) 2 1/4"	4 % @ 50 GYR.
PATCHING	
CLASS 'D' PATCHES TYPE II, - HOT-MIX ASPHALT BINDER IL-19mm - 5" (2 LIFTS)	4 % @ 70 GYR.

NOTES:
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MISTURE QUANTITIES IS 112 LBS/SQ. YD./IN.. THE "AC TYPE" FOR NON-POLYMERIZED HMA MIXES SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.

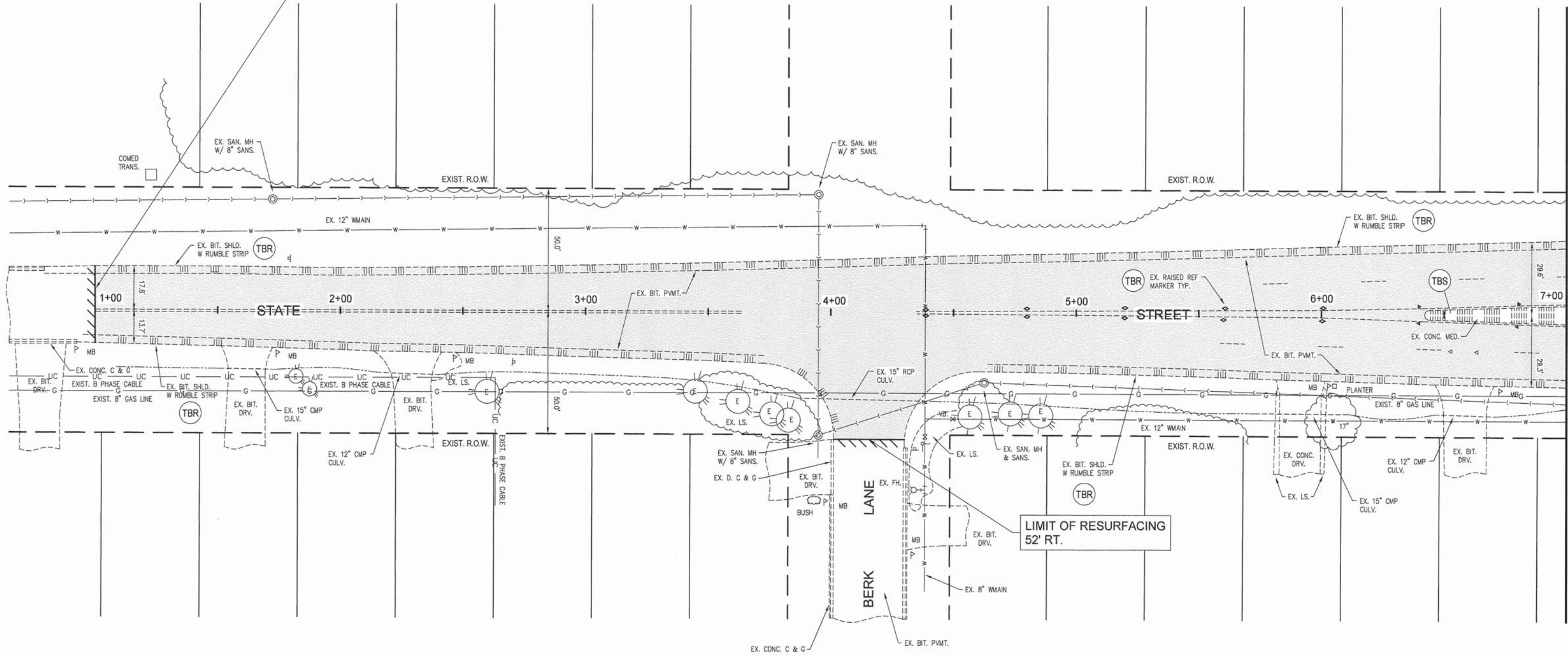
LEGEND	
①	EXISTING HOT-MIX ASPHALT SURFACE REMOVAL, 2"
②	EXISTING PAVEMENT
③	EXISTING CONCRETE CURB & GUTTER TO REMAIN
④	PAVEMENT REMOVAL FOR CLASS 'D' PATCHES IF NEEDED
⑤	BITUMINOUS MATERIALS (PRIME COAT) (0.08 GAL./S.Y.)
⑥	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
⑦	CLASS D PATCHES, 5" (TO BE DELINEATED IN FIELD BY ENGINEER) IF NEEDED
⑧	EXISTING CONCRETE MEDIAN TO REMAIN

PROPOSED 3' WIDE HMA SHOULDER STA. 1+00 - 8+51 TO HAVE 2" SURFACE REMOVAL W/ 2" HMA SURFACE REPLACEMENT

PAVEMENT PLAN LEGEND	
	= APPROXIMATE LOCATIONS OF CLASS "D" PATCHES, TYPE I, II, III, OR IV, 5 INCH
	= HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, (L 9.5mm) 1 1/2"
	= BUTT JOINTS
TBR	= TO BE REMOVED
TBS	= TO BE SAVED



LIMIT OF ROADWAY IMPROVEMENT STA. 1+00



LIMIT OF RESURFACING 52' RT.

MATCH LINE - STA. 7+00 CONTINUED ON SHEET 6

TECH 3 Consulting Group, Inc.
 ENGINEERS SURVEYORS
 737 West Exchange St., Crete, IL 60417
 ph 708.672.4994 fax 708.672.3729
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 194-002235

USER NAME =	DESIGNED - RWM	REVISED - SEPT. 24, 2013
PLOT SCALE =	DRAWN - EGH	REVISED -
PLOT DATE =	CHECKED - WJS	REVISED -
	DATE - AUGUST 19, 2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

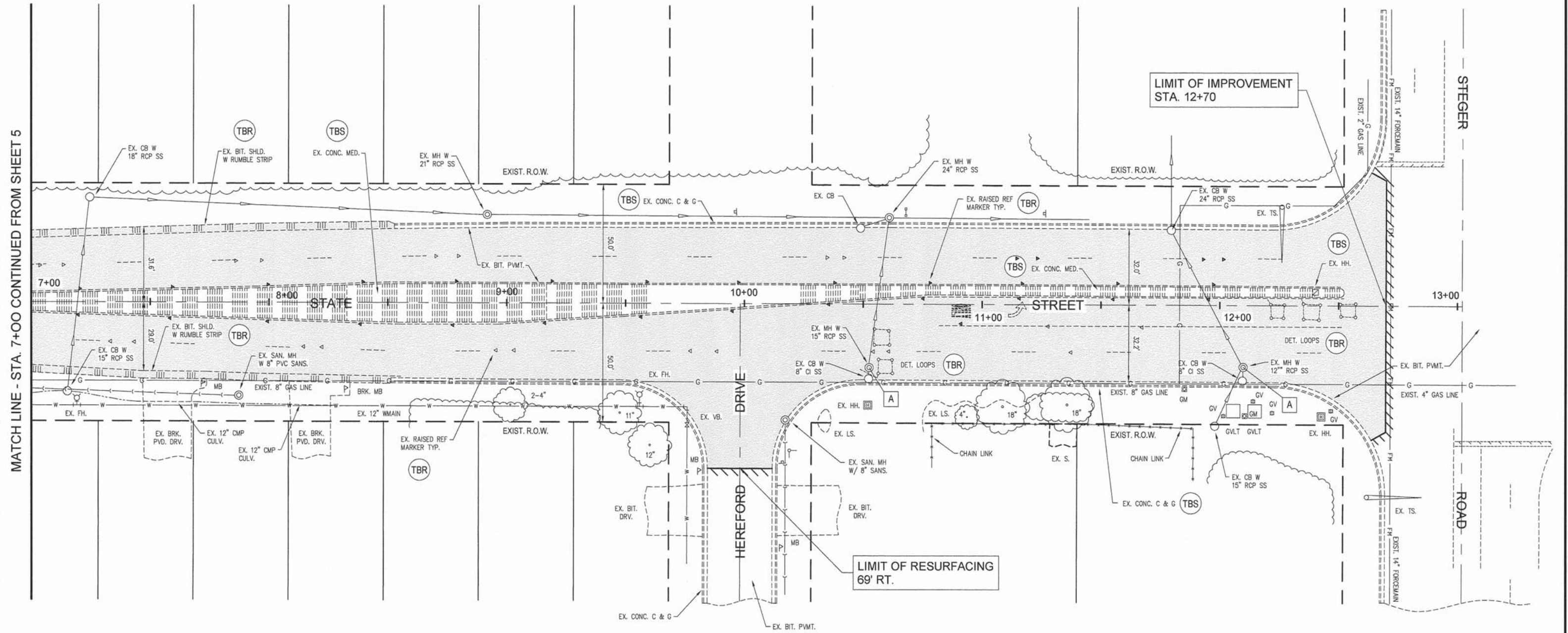
Village of CRETE
 WILL COUNTY, ILLINOIS
 60417
 504 BROADWAY STREET • P.O. BOX 207 • Crete, IL 60417

STATE STREET RESURFACING
FAIRWAY DRIVE TO STEGER ROAD
PAVEMENT PLAN - STA. 1+00 TO 7+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	5
CONTRACT NO. 63889				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT:	

SCALE: 1" = 20' SHEET NO. 5 OF 16 SHEETS STA. - 1+00 TO STA. - 7+00

PAVEMENT PLAN LEGEND	
	APPROXIMATE LOCATIONS OF CLASS "D" PATCHES, TYPE I, II, III, OR IV, 5 INCH
	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, (I.L. 9.5mm) 1 1/2"
	BUTT JOINTS
TBR	TO BE REMOVED
TBS	TO BE SAVED
A	FRAME AND GRATE TO BE ADJUSTED



MATCH LINE - STA. 7+00 CONTINUED FROM SHEET 5

TECH 3 Consulting Group, Inc.
 ENGINEERS SURVEYORS
 737 West Exchange St., Chgo., IL 60611
 ph 708.672.4994 fax 708.672.3739
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-002235

USER NAME =
 PLOT SCALE =
 PLOT DATE =

DESIGNED - RWM
 DRAWN - EGH
 CHECKED - WJS
 DATE - AUGUST 19, 2013

REVISED - SEPT. 24, 2013
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

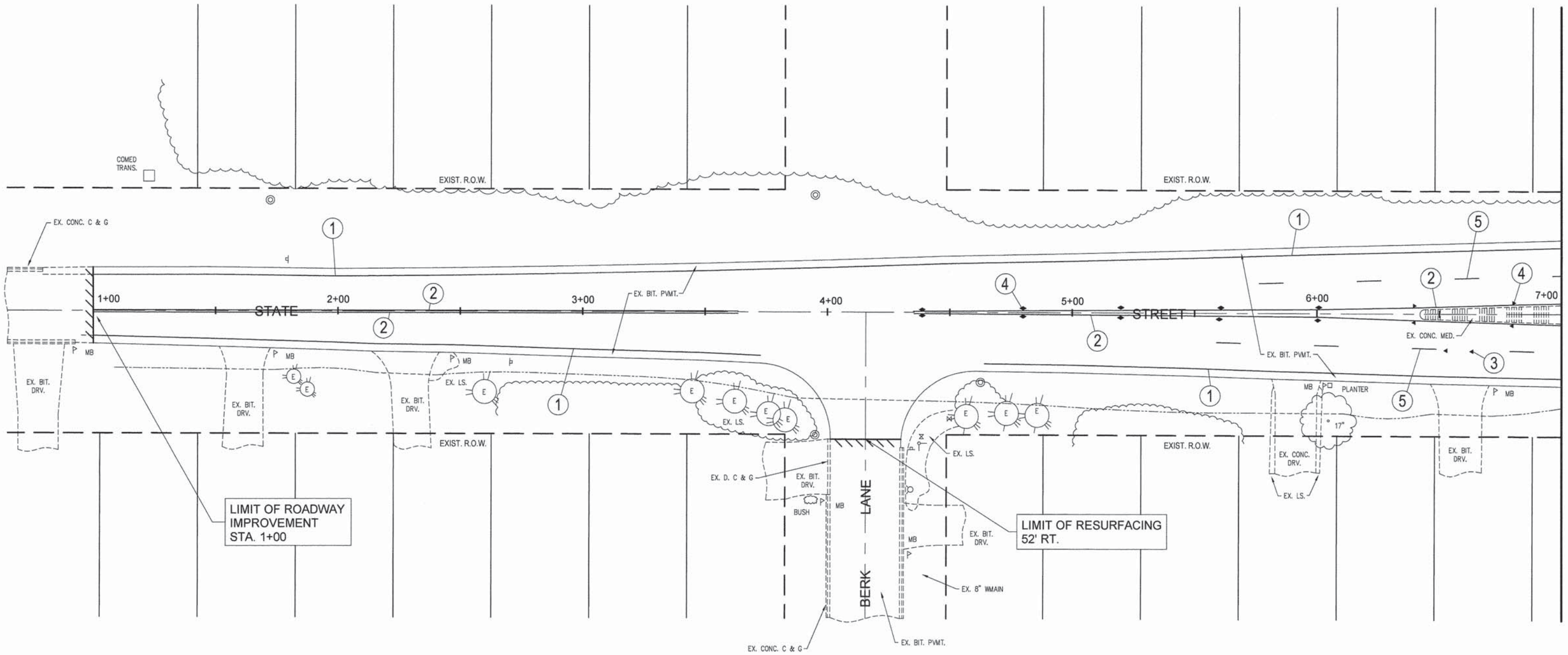
Village of CRETE
 WILL COUNTY, ILLINOIS
54 ROCKLAND STREET • P.O. BOX 57 • P.O. 60419

STATE STREET RESURFACING
FAIRWAY DRIVE TO STEGER ROAD
PAVEMENT PLAN - STA. 7+00 TO 12+70

F.A.U. RTE. 2916	SECTION 13-00048-00-RS	COUNTY WILL	TOTAL SHEETS 16	SHEET NO. 6
SCALE: 1" = 20'		SHEET NO. 6 OF 16 SHEETS		STA. -7+00 TO STA. -12+70
FED. ROAD DIST. NO. 1		ILLINOIS		CONTRACT NO. 63889
FED. AID PROJECT:				

LEGEND FOR PAVEMENT MARKING PLAN

- ① PAINT PAVEMENT MARKING - LINE 4", WHITE
- ② PAINT PAVEMENT MARKING - LINE 4", YELLOW
- ③ RAISED REFLECTIVE PAVEMENT MARKER, CRYSTAL
- ④ RAISED REFLECTIVE PAVEMENT MARKER, AMBER
- ⑤ PAINT PAVEMENT MARKING - LINE 4", SKIP-DASH WHITE



MATCH LINE - STA. 7+00 CONTINUED ON SHEET 8

TECH 3 Consulting Group, Inc.
 ENGINEERS SURVEYORS
 737 West Exchange St., Crete, IL 60417
 ph 708.672.4994 fax 708.672.3739
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-002235

USER NAME =
 PLOT SCALE =
 PLOT DATE =

DESIGNED - RWM
 DRAWN - EGH
 CHECKED - WJS
 DATE - AUGUST 19, 2013

REVISED - SEPT 24, 2013
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Village of CRETE
 WILL COUNTY, ILLINOIS
 60417
501 E. EXCHANGE STREET • P.O. BOX 507 • CRETE, IL 60417

STATE STREET RESURFACING
FAIRWAY DRIVE TO STEGER ROAD
PAVEMENT MARKING PLAN - STA. 1+00 TO 7+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	7
CONTRACT NO. 63889				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT:	

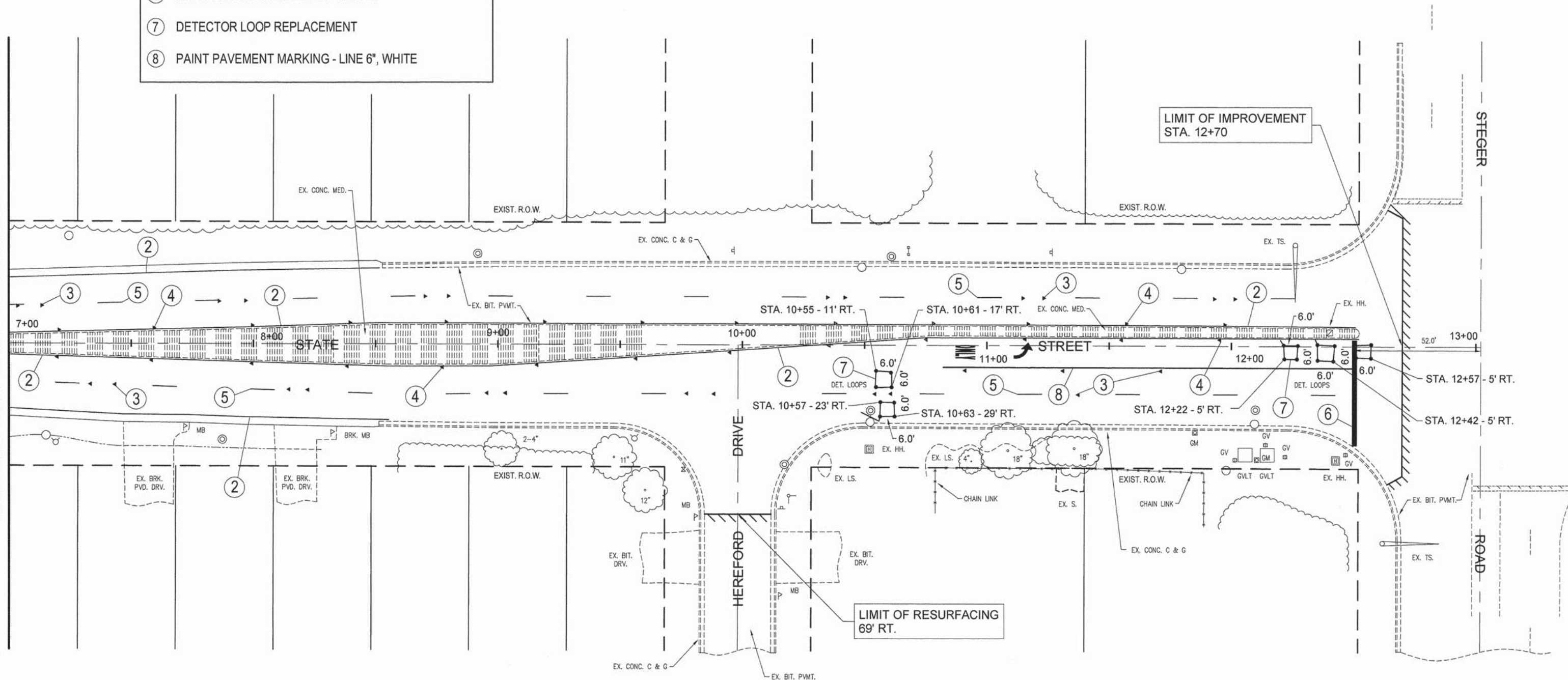
SCALE: 1" = 20' SHEET NO. 7 OF 16 SHEETS STA. - 1+00 TO STA. - 7+00

LEGEND FOR PAVEMENT MARKING PLAN

- ① PAINT PAVEMENT MARKING - LINE 4", WHITE
- ② PAINT PAVEMENT MARKING - LINE 4", YELLOW
- ③ RAISED REFLECTIVE PAVEMENT MARKER, CRYSTAL
- ④ RAISED REFLECTIVE PAVEMENT MARKER, AMBER
- ⑤ PAINT PAVEMENT MARKING - LINE 4", SKIP-DASH WHITE
- ⑥ PAINT PAVEMENT MARKING - LINE 24"
- ⑦ DETECTOR LOOP REPLACEMENT
- ⑧ PAINT PAVEMENT MARKING - LINE 6", WHITE



MATCH LINE - STA. 7+00 CONTINUED FROM SHEET 7



TECH 3 Consulting Group, Inc.
 ENGINEERS SURVEYORS
 737 West Exchange St. Chgo, IL 60617
 PH: 708.672.4994 FAX: 708.672.3739
 ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184-002235

USER NAME =
 PLOT SCALE =
 PLOT DATE =

DESIGNED - RWM
 DRAWN - EGH
 CHECKED - WJS
 DATE - AUGUST 19, 2013

REVISED - SEPT 24, 2013
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



Village of CRETE
 WILL COUNTY, ILLINOIS
 500 BROADWAY STREET • P.O. BOX 107 • CRETE, ILLINOIS 60431

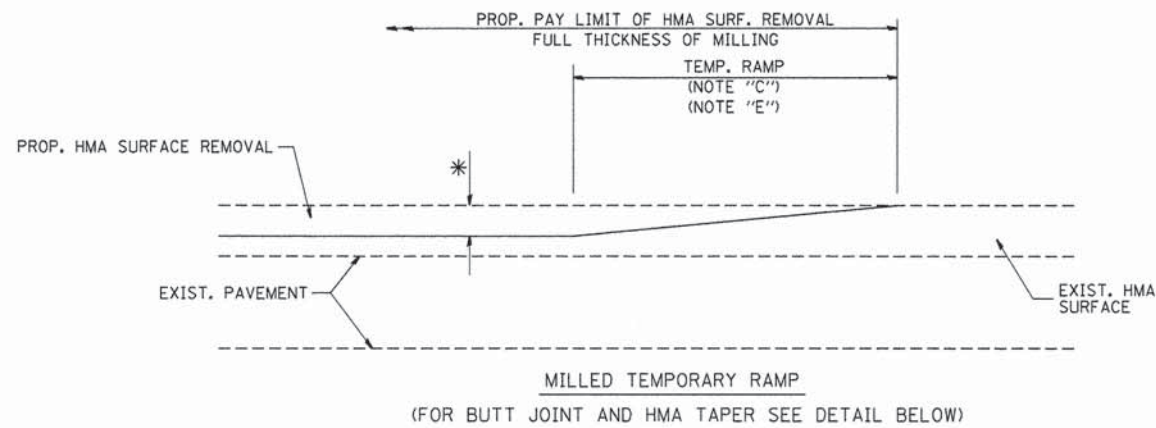
SCALE: 1" = 20'

SHEET NO. 8 OF 16 SHEETS

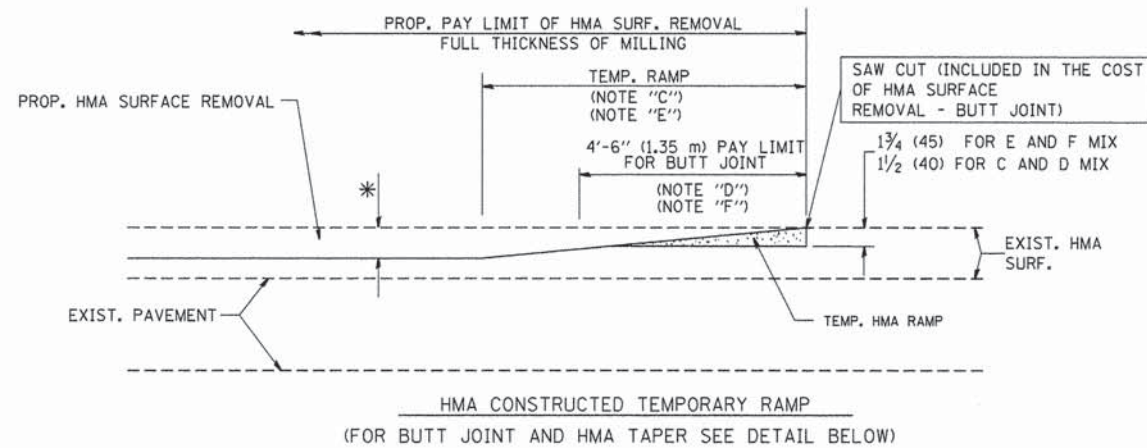
STATE STREET RESURFACING
FAIRWAY DRIVE TO STEGER ROAD
PAVEMENT PLAN - STA. 7+00 TO 12+70

STA. - 7+00 TO STA. - 12+70

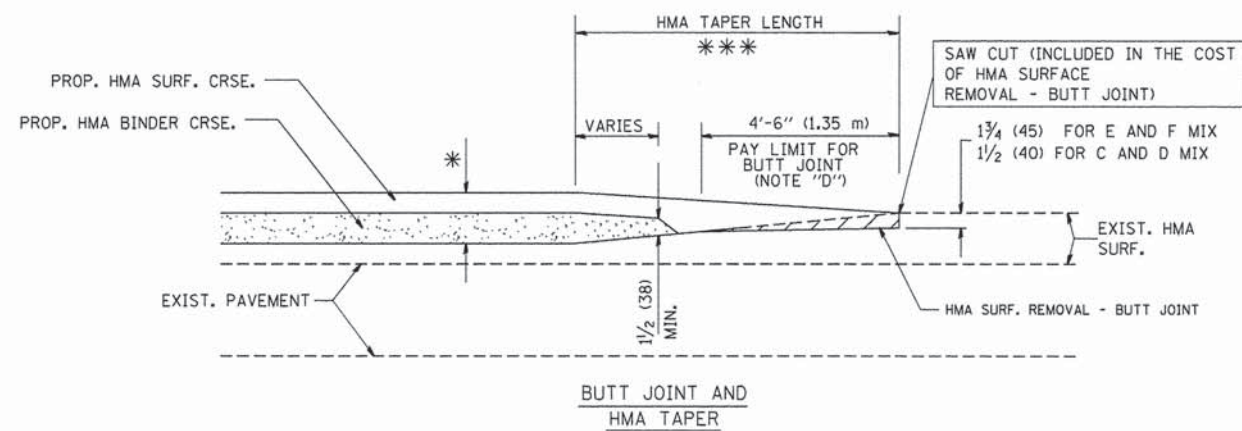
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	8
CONTRACT NO. 63889				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT:		



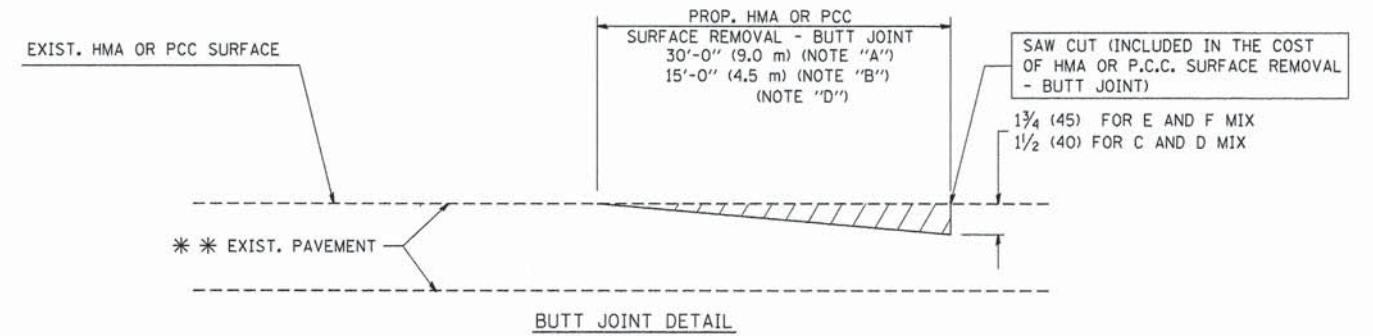
OPTION 1



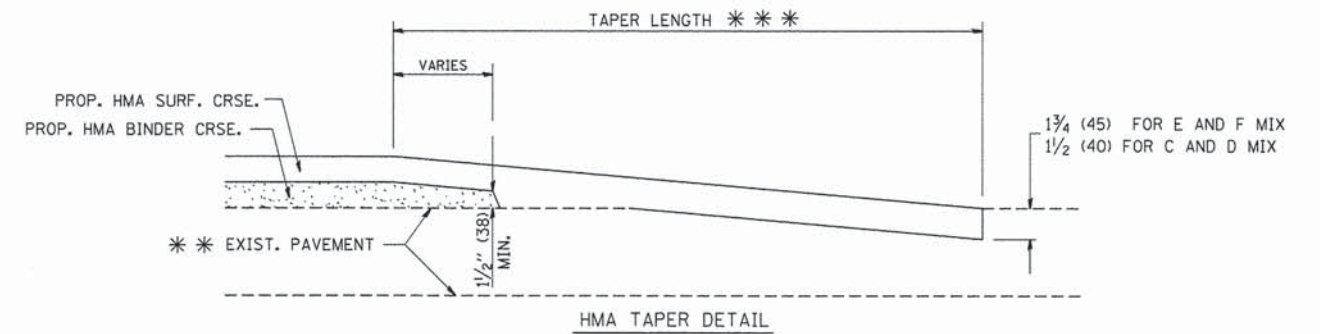
**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

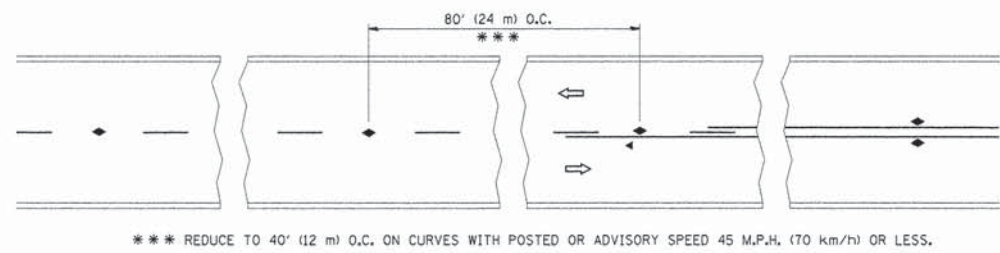
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	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

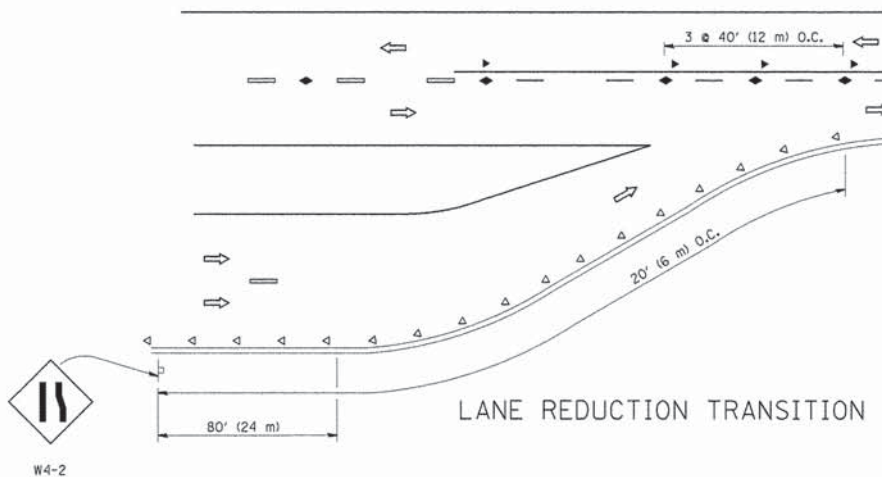
**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 9 OF 16 SHEETS STA. TO STA.

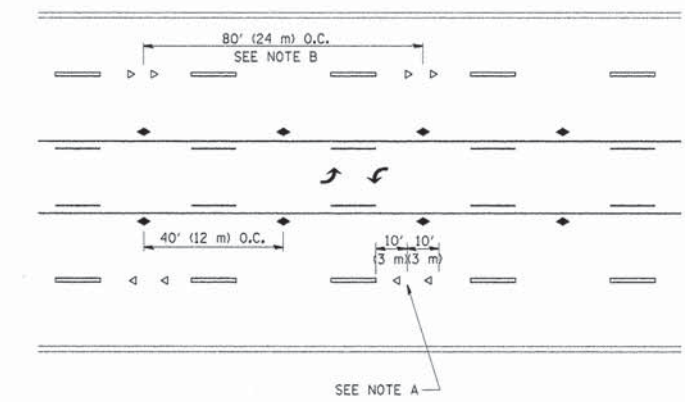
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	9
BD400-05 BD32		CONTRACT NO. 63889		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



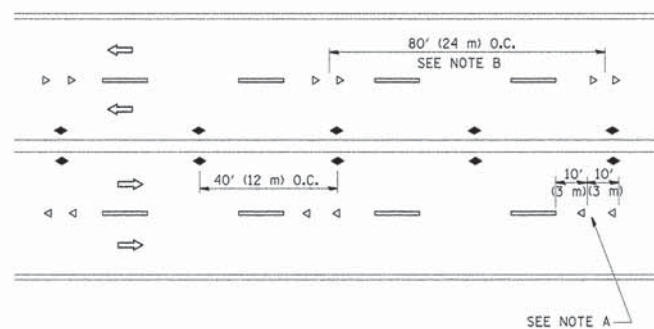
TWO-LANE/TWO-WAY



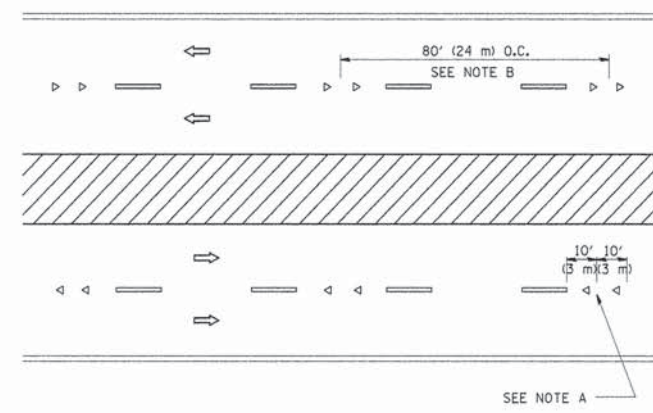
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

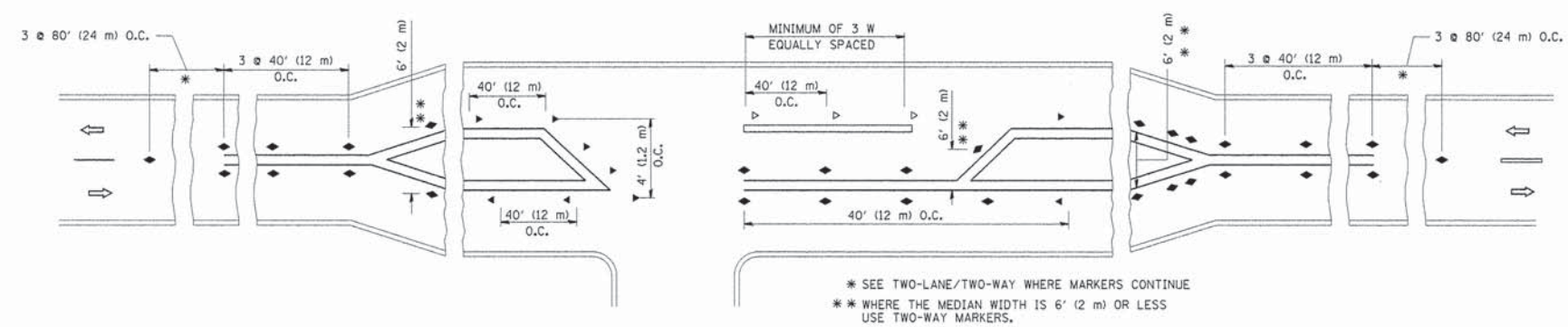
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

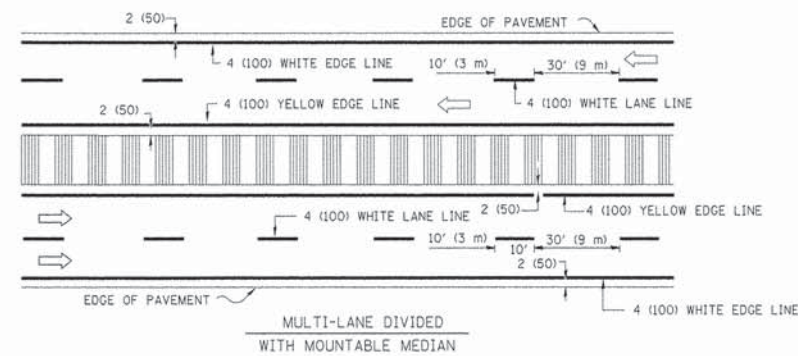
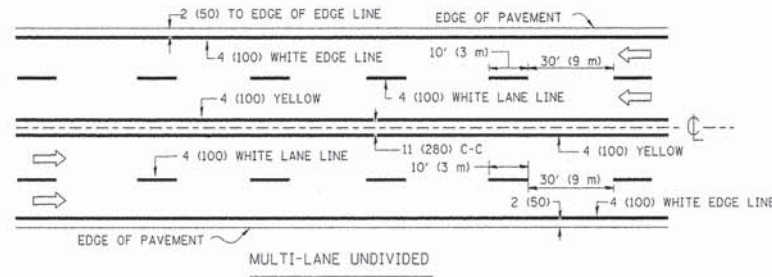
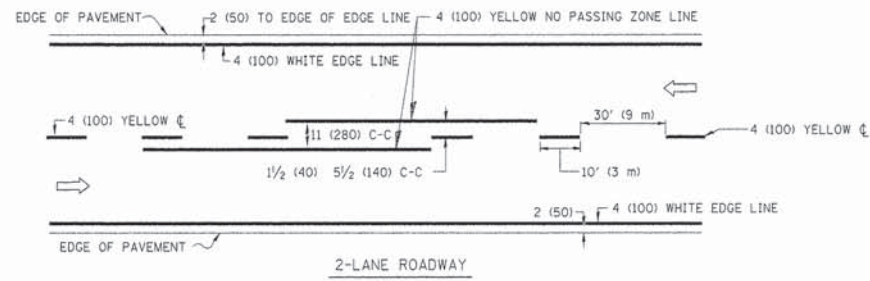
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		CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

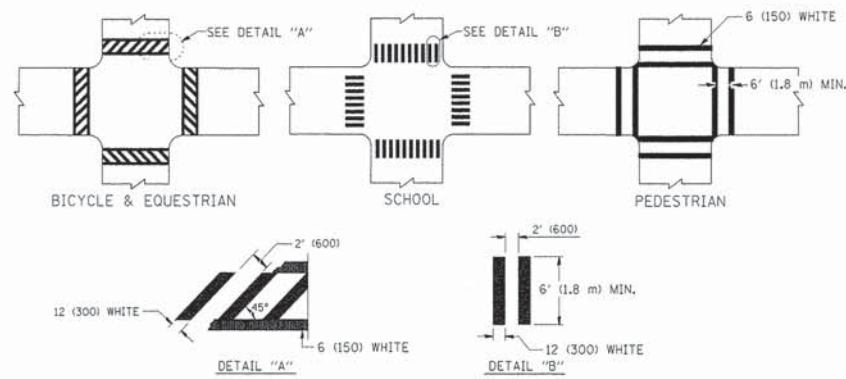
SCALE: NONE SHEET NO. 10 OF 16 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	10
TC-11			CONTRACT NO. 63889	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

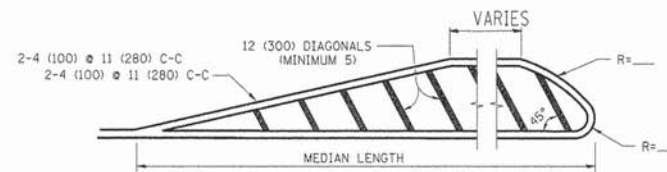
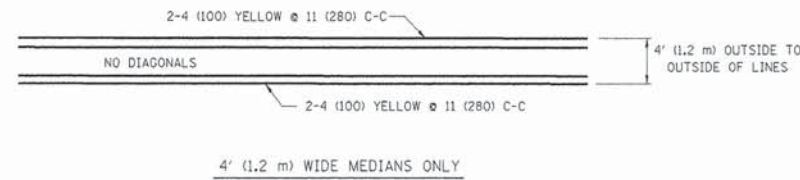


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

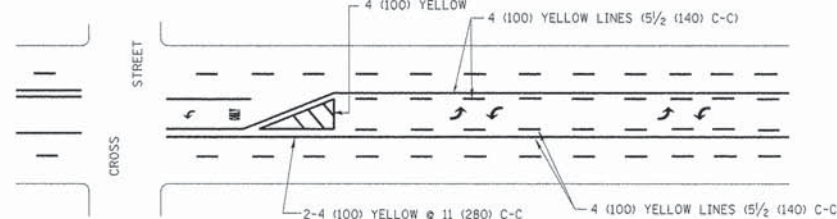


TYPICAL CROSSWALK MARKING

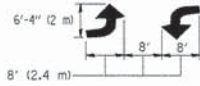


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

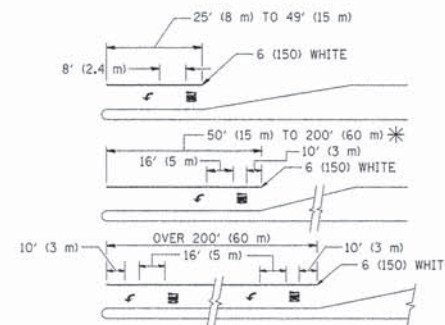


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

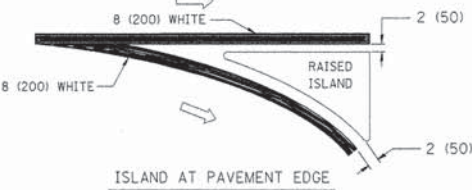
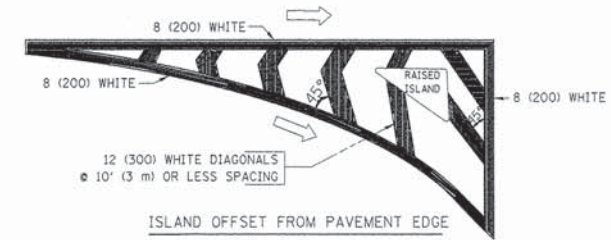


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

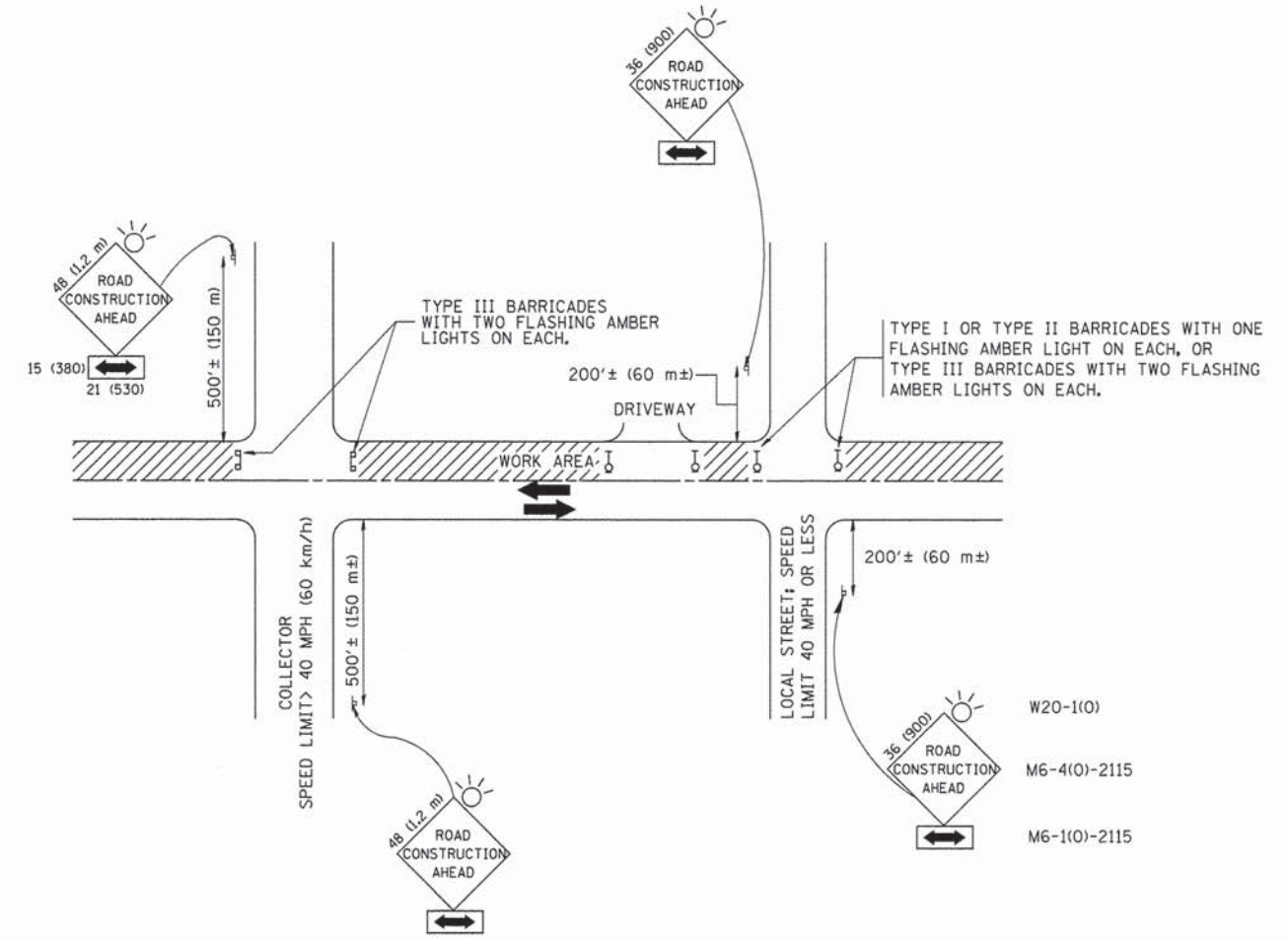
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PLOT DATE = 9/9/2009		DATE = 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 11 OF 16 SHEETS STA. TO STA.

F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	11
TC-13			CONTRACT NO. 63889	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 70150), STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

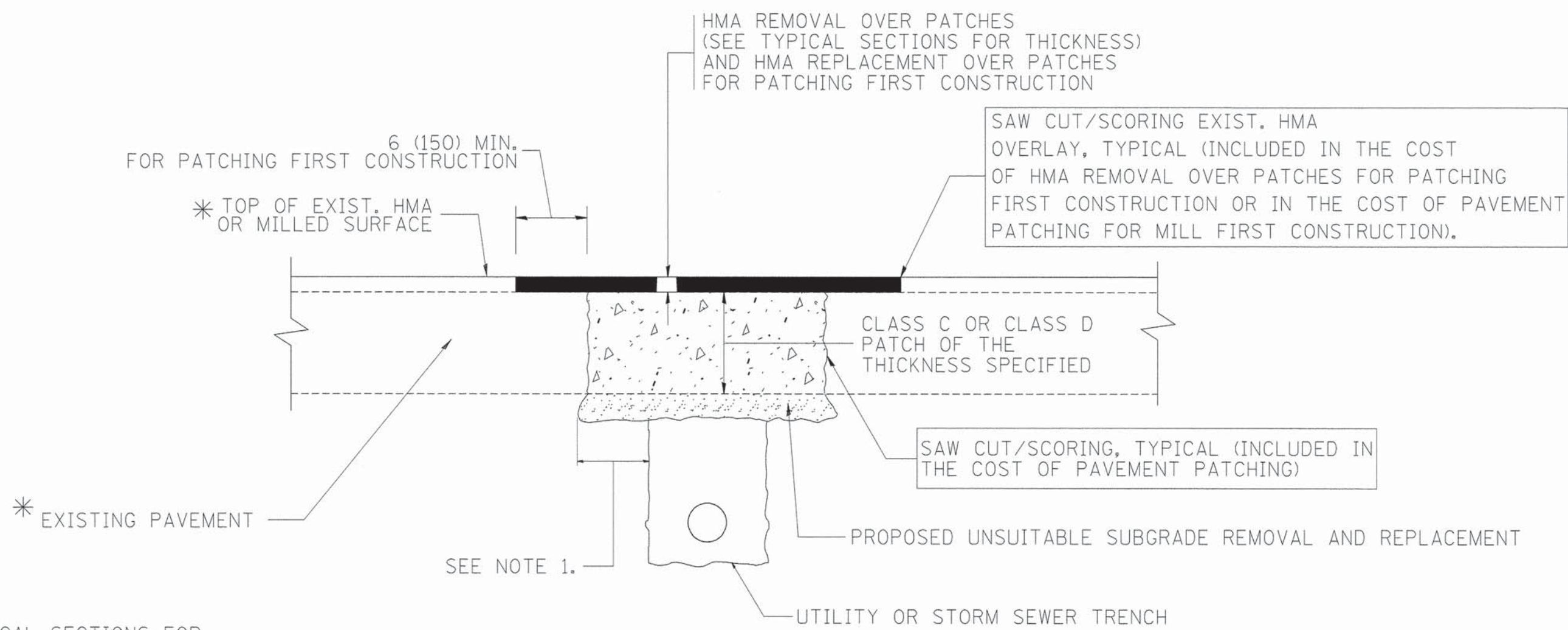
All dimensions are in millimeters (inches) unless otherwise shown.

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		DRAWN -	REVISED - A. HOUSEH 03-06-96
		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 12 OF 16 SHEETS
STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	12
TC-10			CONTRACT NO. 63889	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

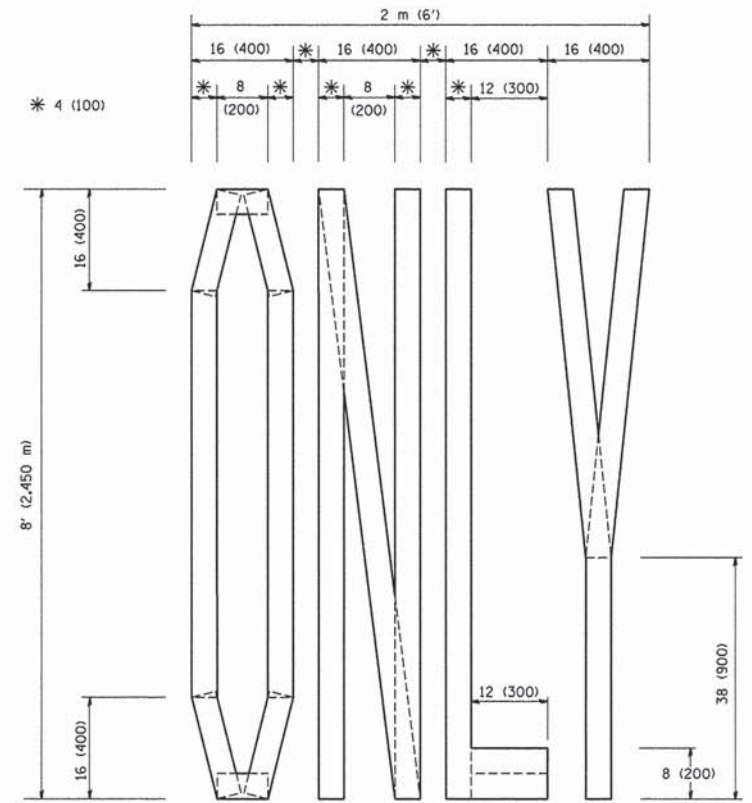
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

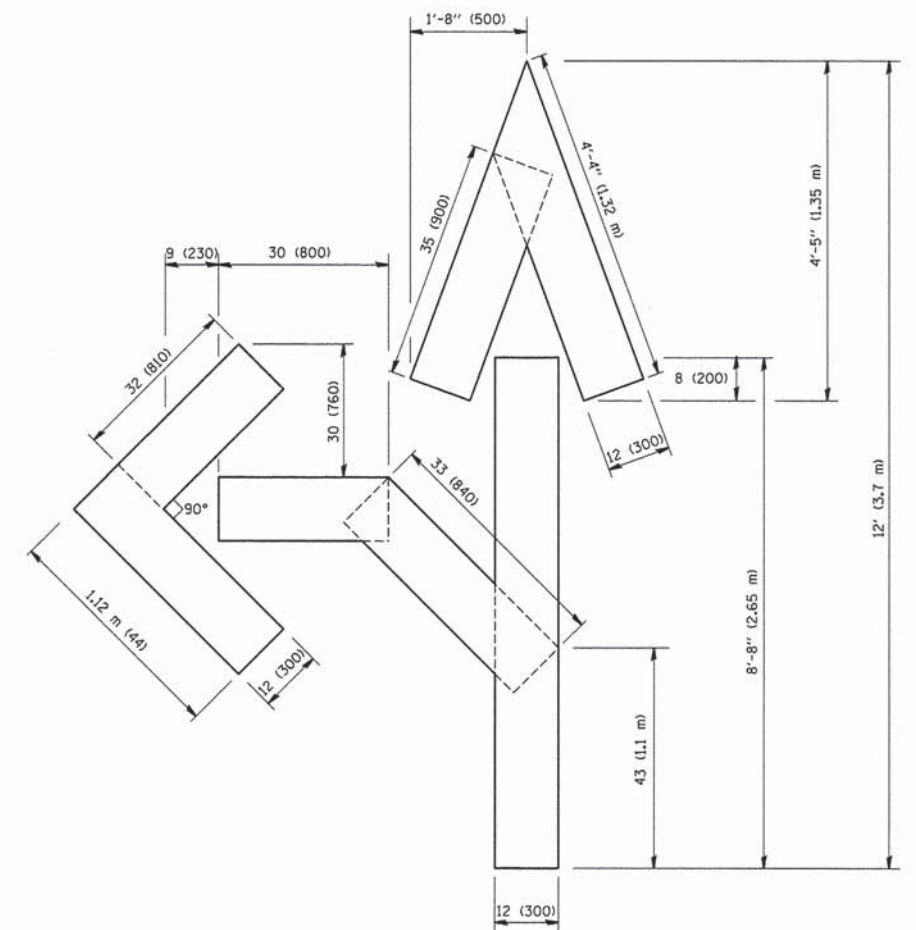
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

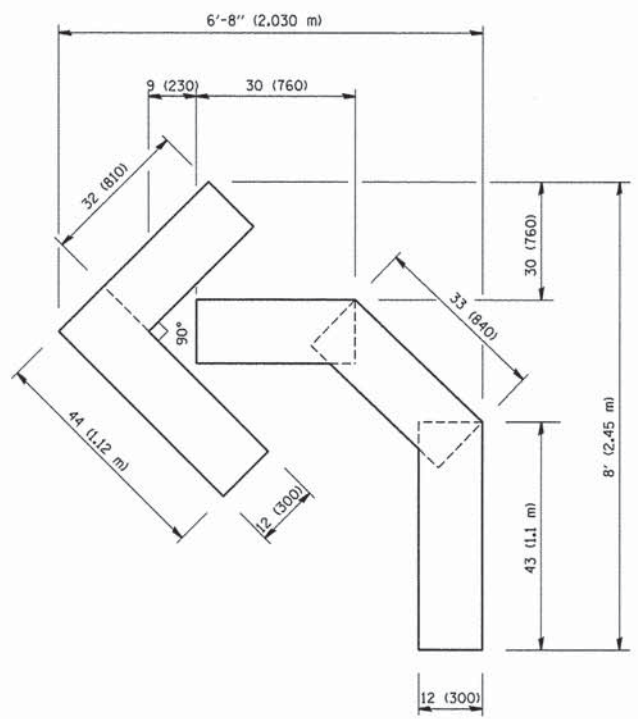
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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 13 OF 16 SHEETS	STA. TO STA.	BD400-04 (BD-22) CONTRACT NO. 63889				
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - R. BORO 09-04-07									
			REVISED - K. ENG 10-27-08									



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

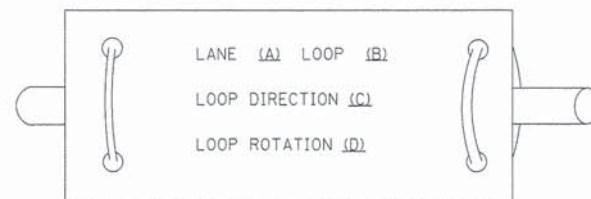
All dimensions are in inches (millimeters) unless otherwise shown.

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		DRAWN -	REVISED -T. RAMMACHER 11-04-97		SCALE: NONE	SHEET NO. 14 OF 16 SHEETS	STA. TO STA.	TC-16				
		PLOT SCALE = 50.0000' / IN.	REVISD -T. RAMMACHER 03-02-98		CONTRACT NO. 63889							
		PLOT DATE = 1/4/2008	REVISD -E. GOMEZ 08-28-00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

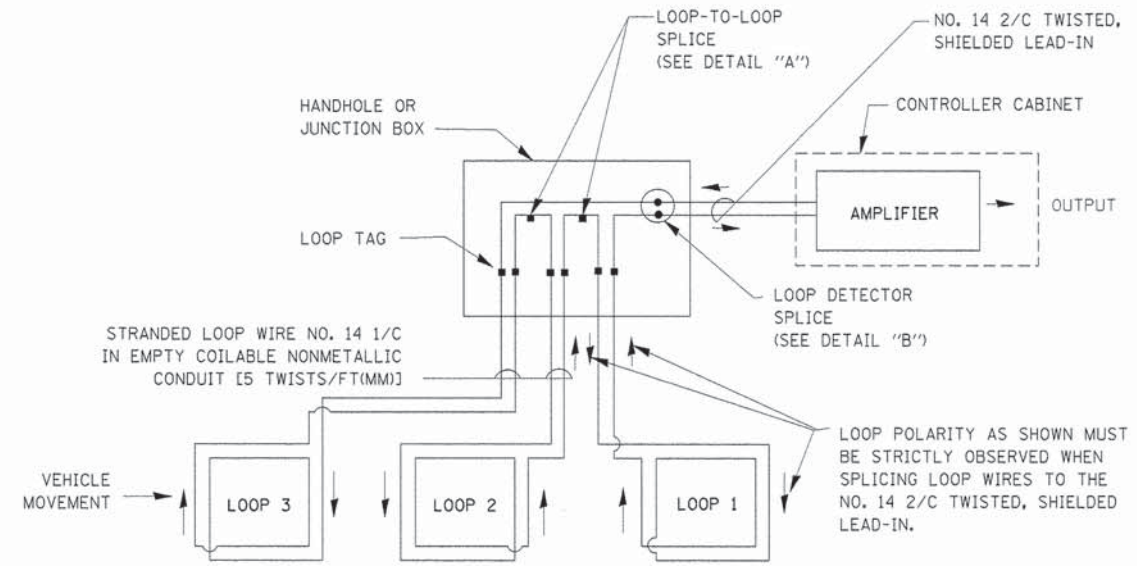
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

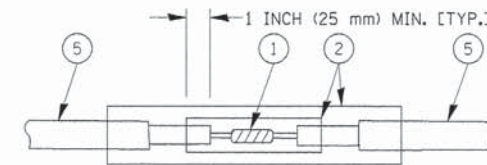


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

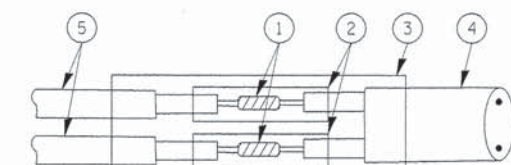


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

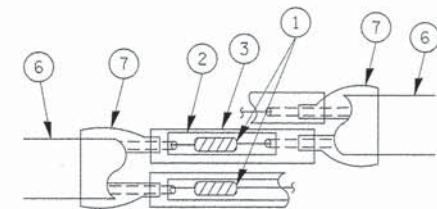


DETAIL "A" LOOP-TO-LOOP SPLICE

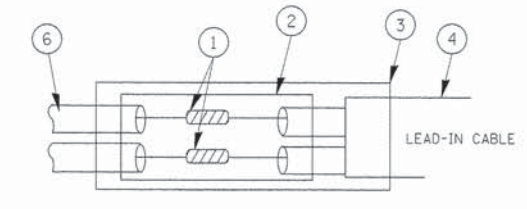


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



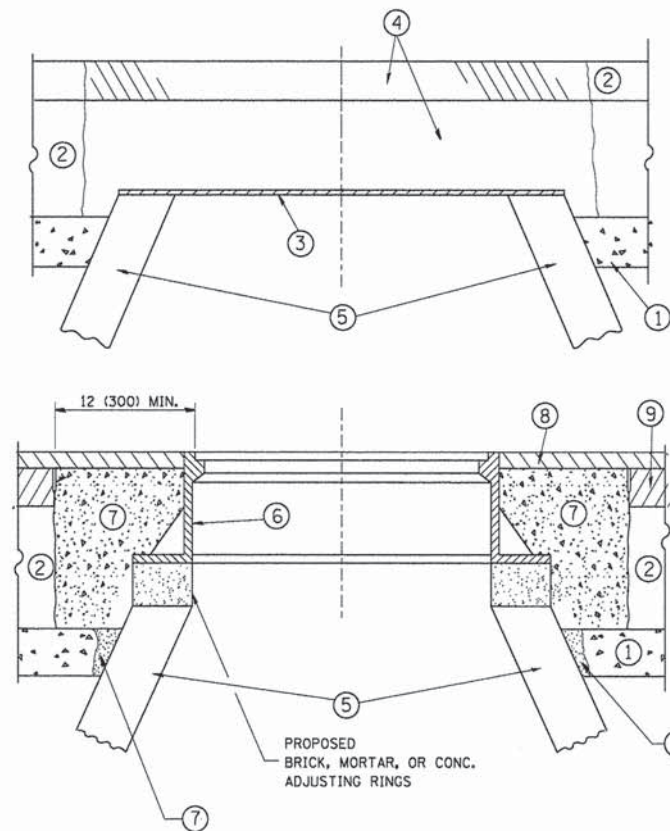
DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME *	USER NAME = bawerd1	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
os\p_w\work\pwsdot\bawerd1\d0106315\ba08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 16 OF 16 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2916	13-00048-00-RS	WILL	16	16
BD600-03 (BD-8)			CONTRACT NO. 63889	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				