

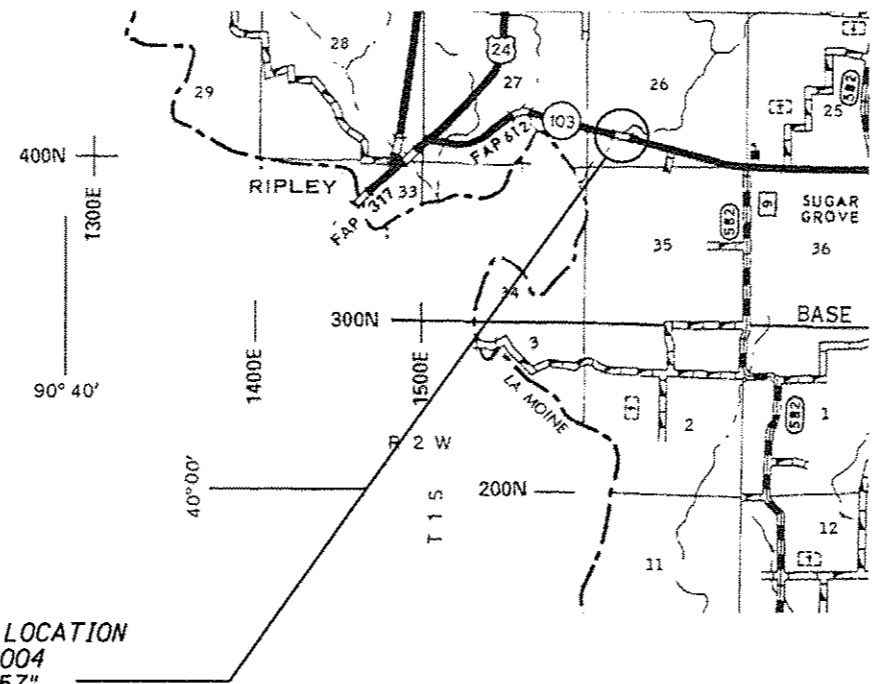
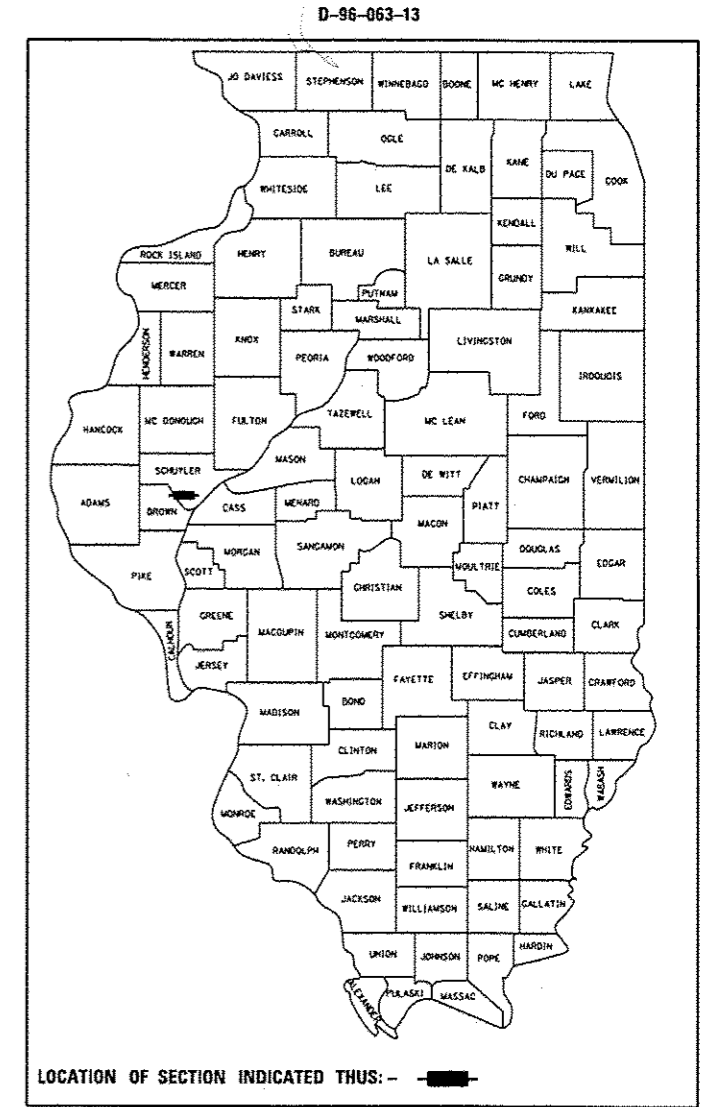
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|-----------|----------|--------------------|-----------|
| 612 | (101) 1-2 | SCHUYLER | 16 | 1 |
| FED. ROAD DIST. NO. 6 | | ILLINOIS | CONTRACT NO. 72G27 | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
CONTRACT MAINTENANCE PROJECT

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FAP ROUTE 612 (IL 103)
SECTION (101) 1-2
C-96-063-13
BRIDGE RESURFACING AND JOINT REPLACEMENT
SCHUYLER COUNTY



PROJECT LOCATION
 SN 085-0004
 N40° 01' 57"
 W90° 36' 05"

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

BRIDGE MAINTENANCE ENGINEER: BRANDON DUDLEY (ACTING) 217-785-9290
BRIDGE INSPECTION ENGINEER: DAVE COPENBARGER 217-785-5306

GROSS LENGTH = 180 FT. = 0.03 MILE
 NET LENGTH = 180 FT. = 0.03 MILE

CONTRACT NO. 72G27

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED October 31, 2013

Roger L. Amickell
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Dec 6, 2013
John D. Baranzelli, P.E.
 SENIOR ENGINEER OF DESIGN AND ENVIRONMENT

Dec 6, 2013
Omer Osman, P.E.
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

| | |
|------|---|
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| 2 | INDEX, STANDARDS, SIGNATURES, AND GENERAL NOTES |
| 3-4 | QUANTITIES |
| 5 | TYPICAL SECTIONS |
| 6 | ROADWAY PLAN |
| 7 | TRAFFIC STAGING DETAIL |
| 8 | WIDTH RESTRICTION SIGNING DETAIL |
| 9-15 | BRIDGE DETAILS |
| 16 | INLET DETAILS |

STANDARDS

| |
|-----------|
| 606001-05 |
| 701001-02 |
| 701006-05 |
| 701201-04 |
| 701316-08 |
| 701326-04 |
| 701901-03 |
| 701301 |

GENERAL NOTES:

ALL STRUCTURAL STEEL SHALL BE AASHTO M-270 GRADE 36

REINFORCEMENT BARS SHALL BE A706 GRADE 60. REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE MILL SCALE AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN CONCRETE REMOVAL.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50°F.

THE EXISTING STRUCTURAL STEEL COATING CONTAINS LEAD. THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.

| | |
|---|---|
| STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS DISTRICT 6 | |
| EXAMINED | Oct 7 20 13 <i>Bill Boyer</i> ENGINEER OF OPERATIONS |
| EXAMINED | Oct 9 20 12 <i>Jimmy [Signature]</i> ENGINEER OF PROJECT IMPLEMENTATION |
| EXAMINED | Oct. 15 20 15 <i>SRMLU</i> ENGINEER OF PROGRAM DEVELOPMENT |

| MIXTURE USE(S) | HMA SURFACE COURSE |
|--|--------------------|
| AC/PG | PG 64-22 |
| DESIGN AIR VOIDS | 4.0% @ N50 |
| MIX COMPOSITION (GRADATION MIXTURE) | IL 9.5 |
| FRICITION AGGREGATE | MIX C |

INDEX, STANDARDS, & NOTES
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY

S U M M A R Y O F Q U A N T I T I E S

| CODE NO. | ITEM | UNIT | 100% State |
|----------|---|-------|------------|
| | | | 0014 |
| | | | TOTAL QTY |
| 20200500 | EARTH EXCAVATION (WIDENING) | CU YD | 37 |
| 35400500 | PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10" | SQ YD | 59 |
| 40600200 | BITUMINOUS MATERIALS (PRIME COAT) | TON | 0.4 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 2 |
| 40600990 | TEMPORARY RAMP | SQ YD | 72 |
| 40603310 | HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 | TON | 72 |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1-1/2" | SQ YD | 539 |
| 50102400 | CONCRETE REMOVAL | CU YD | 6 |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 6 |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 880 |
| 50800515 | BAR SPLICERS | EACH | 12 |
| 52000110 | PREFORMED JOINT STRIP SEAL | FOOT | 45 |
| 60605000 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT | 262 |
| 67100100 | MOBILIZATION | L SUM | 1 |
| 70100100 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701316 | EACH | 1 |

15

QUANTITIES
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY

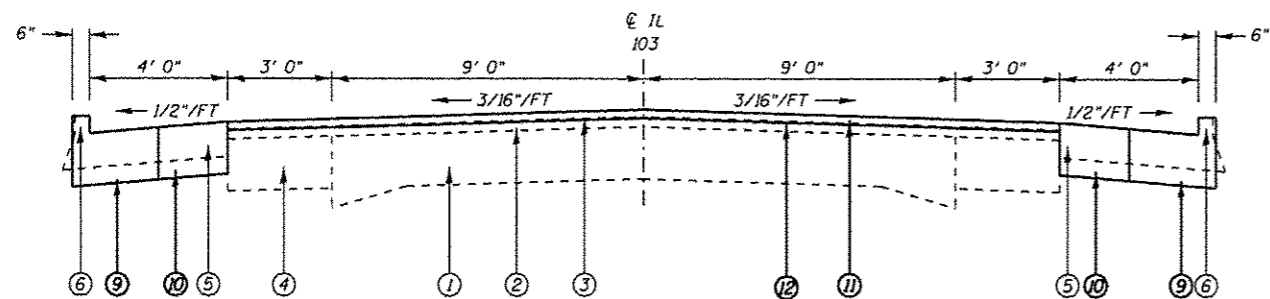
S U M M A R Y O F Q U A N T I T I E S

| CODE NO. | ITEM | UNIT | 100% State |
|------------|---|--------|------------|
| | | | 0014 |
| | | | TOTAL QTY |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 |
| 70100500 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701326 | L SUM | 1 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 5 |
| 70106500 | TEMPORARY BRIDGE TRAFFIC SIGNAL | EACH | 1 |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 8 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 50 |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 17 |
| * 78001120 | PAINT PAVEMENT MARKING - LINE 5" | FOOT | 700 |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 2 |
| X5810100 | WATERPROOFING MEMBRANE SYSTEM (SPECIAL) | SQ YD | 308 |
| → X6026800 | INLETS TO BE RECONSTRUCTED (SPECIAL) | EACH | 8 |
| X7200201 | WIDTH RESTRICTION SIGNING | L SUM | 1 |
| Z0004556 | HOT-MIX ASPHALT SURFACE REMOVAL (DECK) | SQ YD | 311 |
| | | | |
| | | | |
| | | | |

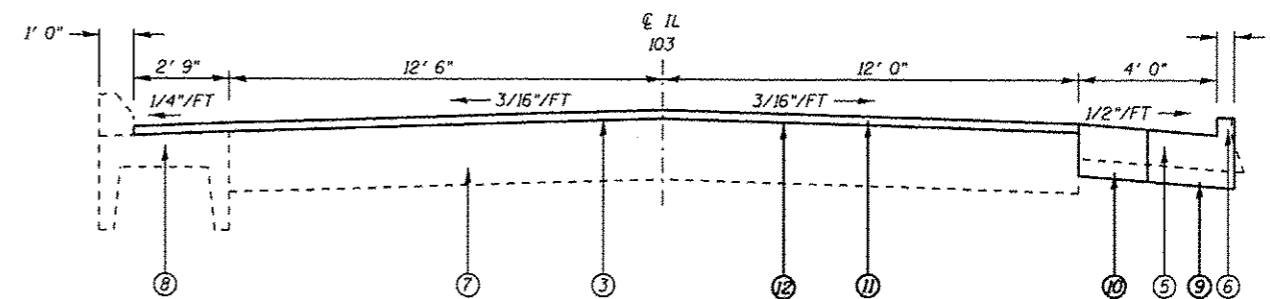
13

* SPECIALTY ITEM

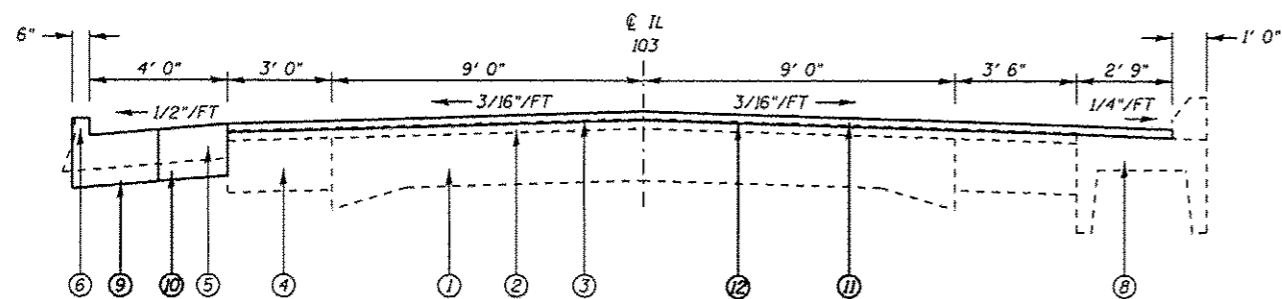
QUANTITIES
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY



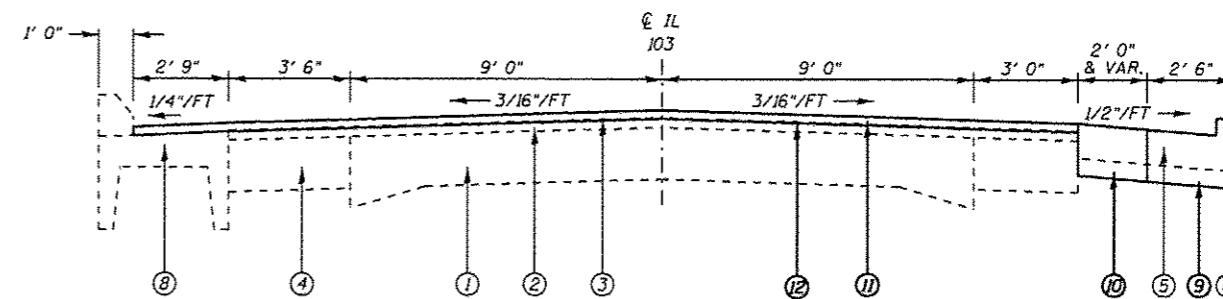
STA 76+65 TO STA 77+17
STA 78+93 TO STA 79+33



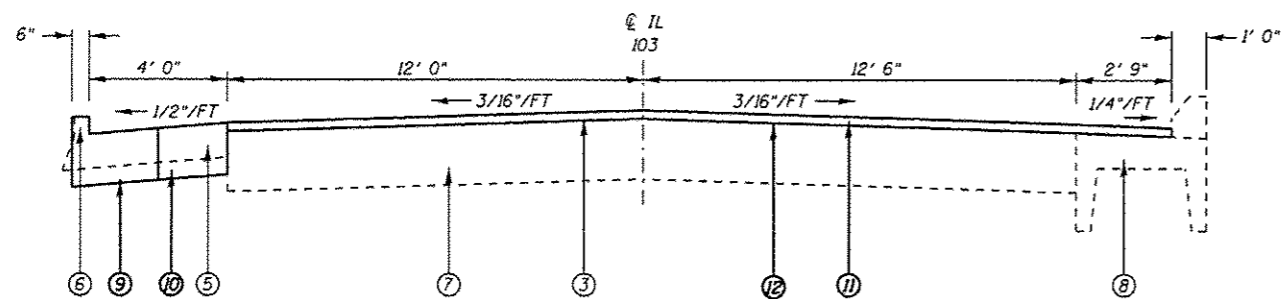
STA 78+60 TO STA 78+81



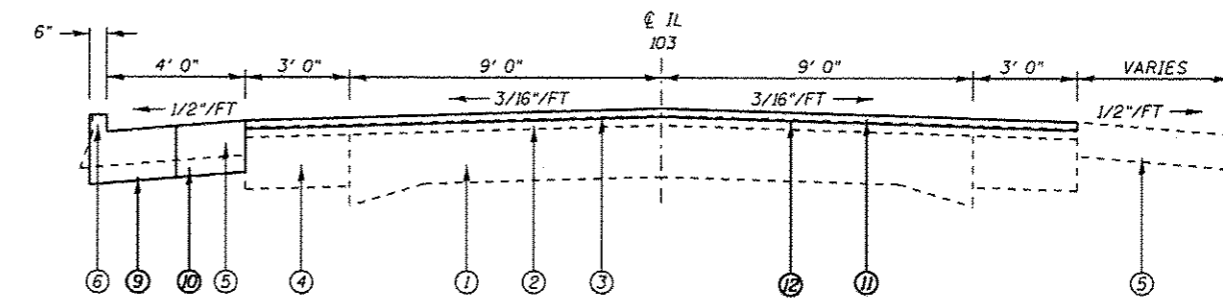
STA 77+17 TO STA 77+29



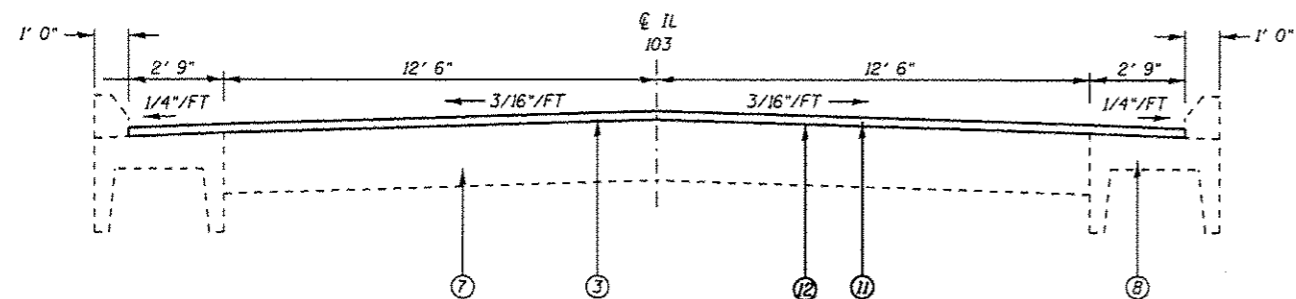
STA 78+81 TO STA 78+93



STA 77+29 TO STA 77+50



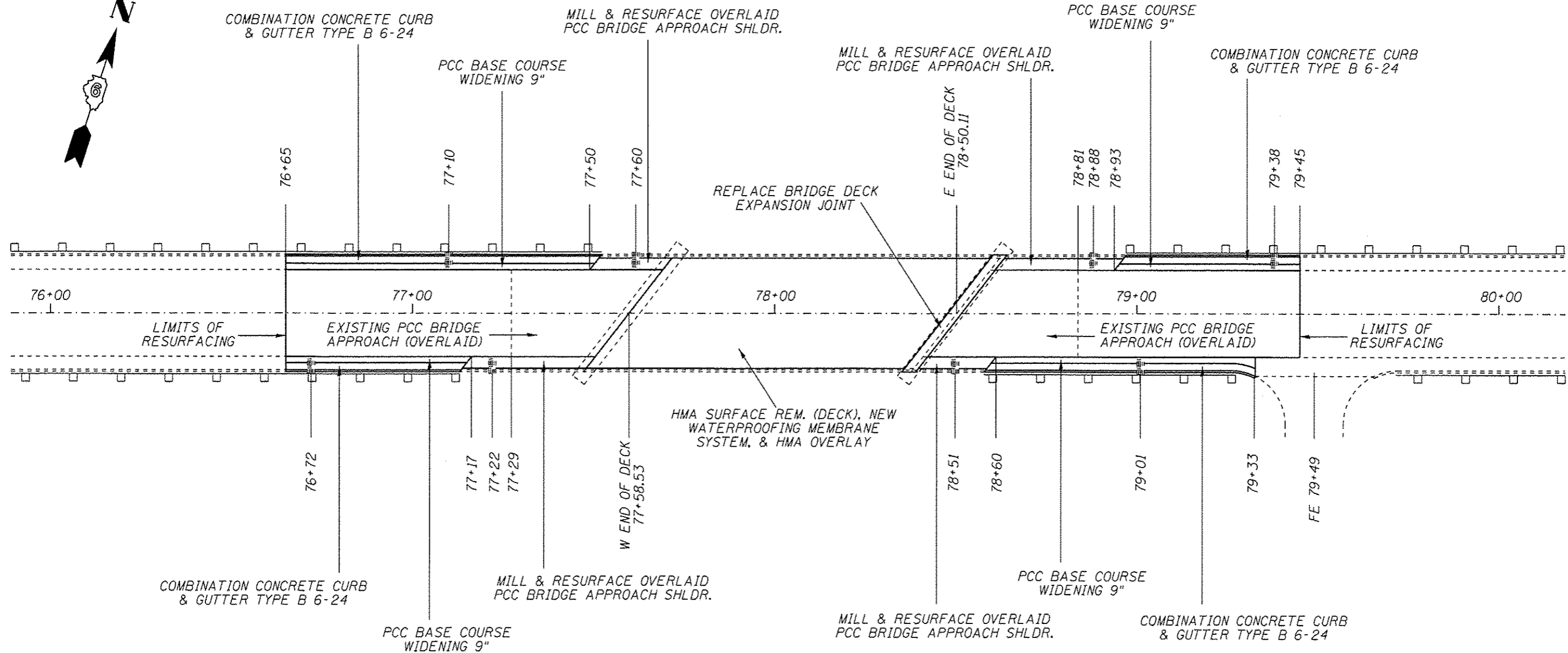
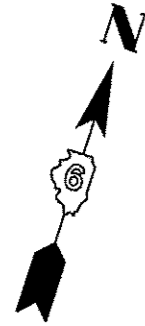
STA 79+33 TO STA 79+45



STA 77+50 TO STA 77+58.53
STA 77+58.53 TO STA 78+50.11 - BRIDGE DECK
STA 78+50.11 TO STA 78+60

- ① EXISTING PCC & HMA PAVEMENT (VARIABLE DEPTH)
- ② EXISTING HMA BINDER COURSE (1-3/4")
- ③ EXISTING HMA SURFACE COURSE (1-1/4")
- ④ EXISTING HMA BASE COURSE (9")
- ⑤ EXISTING HMA SHOULDER (6")
- ⑥ EXISTING HMA CURB (5")
- ⑦ EXISTING PCC APPROACH PAVEMENT
- ⑧ EXISTING PRECAST APPROACH SHOULDER UNIT
- ⑨ PROPOSED COMBINATION CONCRETE CURBE AND GUTTER TYPE B-6.24
- ⑩ PROPOSED PCC BASE COURSE WIDENING (10")
- ⑪ PROPOSED HMA SURFACE COURSE (1-1/2")
- ⑫ PROPOSED ASPHALT SURFACE REMOVAL 1 1/2"

TYPICAL SECTIONS
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY



ROADWAY PLAN VIEW

INLETS TO BE
ADJUSTED (SPC.)

- 76+72 RT
- 77+10 LT
- 77+22 RT
- 77+60 LT
- 78+51 RT
- 78+88 LT
- 79+01 RT
- 79+38 LT

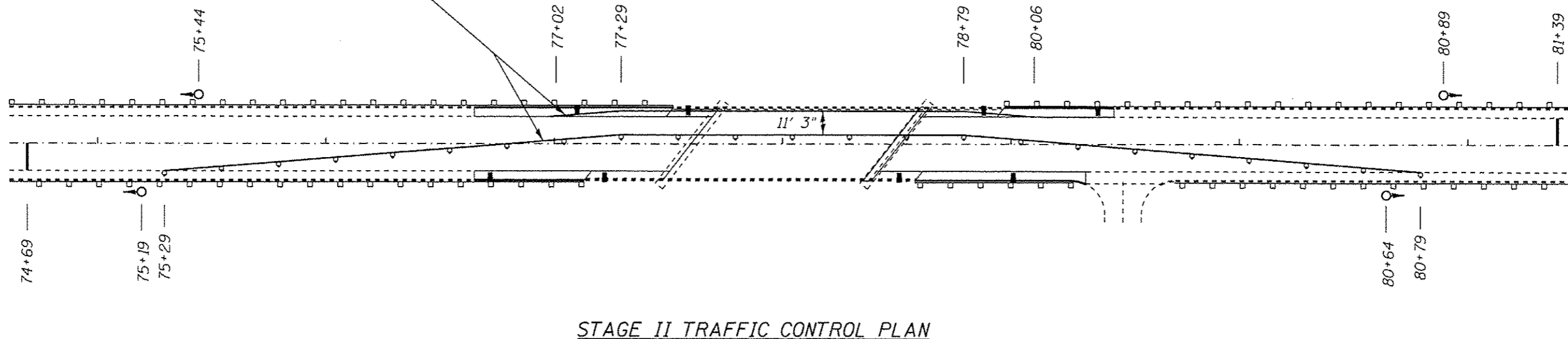
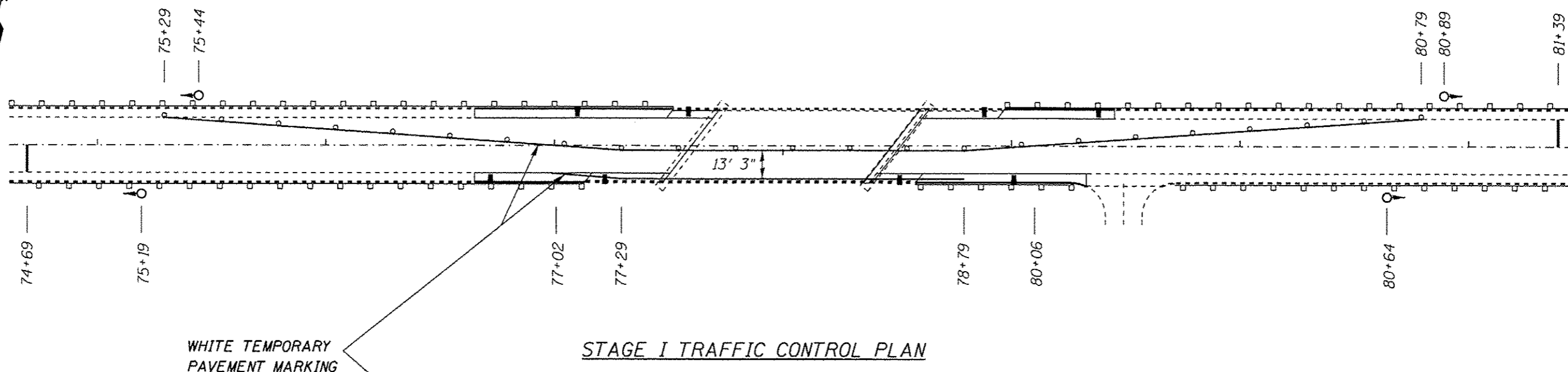
ROADWAY PLAN
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY

STAGE CONSTRUCTION PLAN:

PRE STAGE I - REMOVE EXISTING HMA SHOULDERS AND PLACE COMB. CONC. CURB AND GUTTER, AND PCC BASE COURSE WIDENING

STAGE I - REPLACE NORTH HALF OF EXPANSION JOINT, MILL WB LANE, PLACE WATERPROOFING MEMBRANE ON DECK, AND RESURFACE WB LANE

STAGE II - REPLACE SOUTH HALF OF EXPANSION JOINT, MILL EB LANE, PLACE WATERPROOFING MEMBRANE ON DECK, AND RESURFACE EB LANE



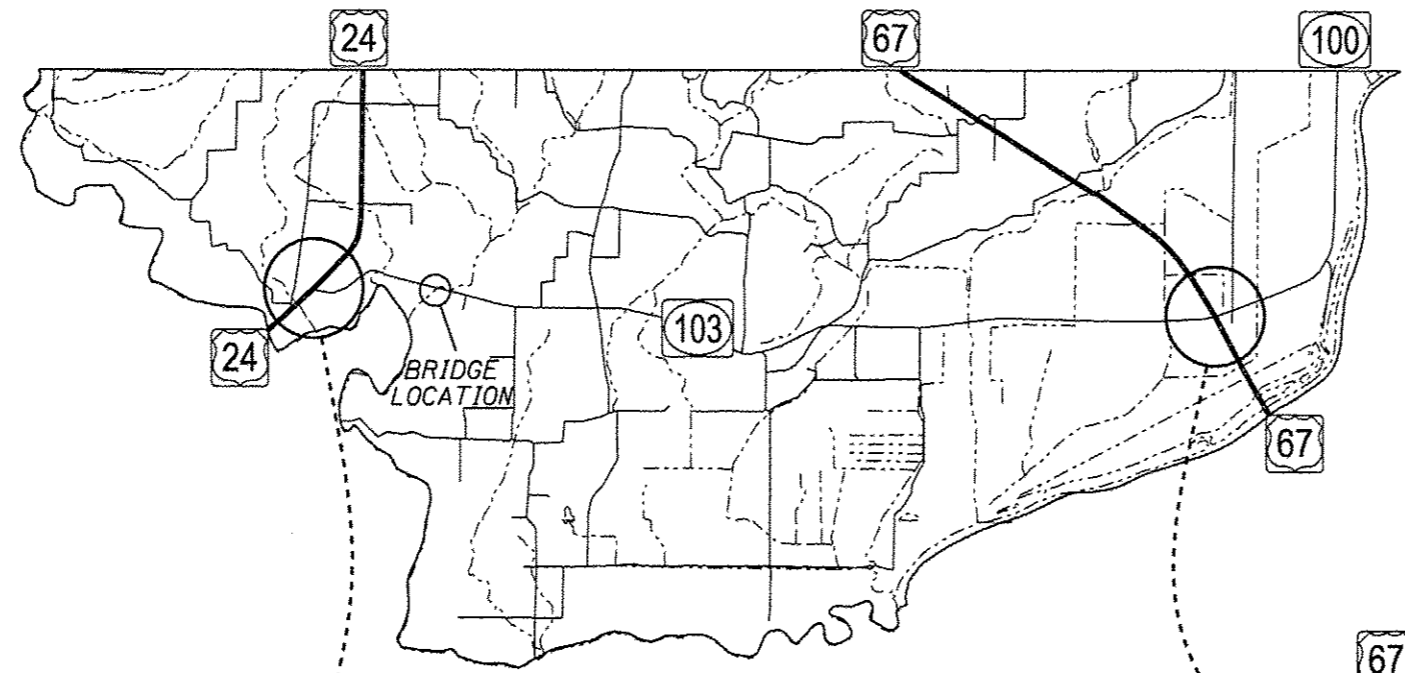
NOTES:

TRAFFIC CONTROL SHALL FOLLOW STANDARD 701316 EXCEPT AS OUTLINED ON THIS SHEET. ALL MATERIAL AND WORK DETAILED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE STANDARD AND SHALL NOT BE MEASURED FOR PAYMENT SEPARATELY.

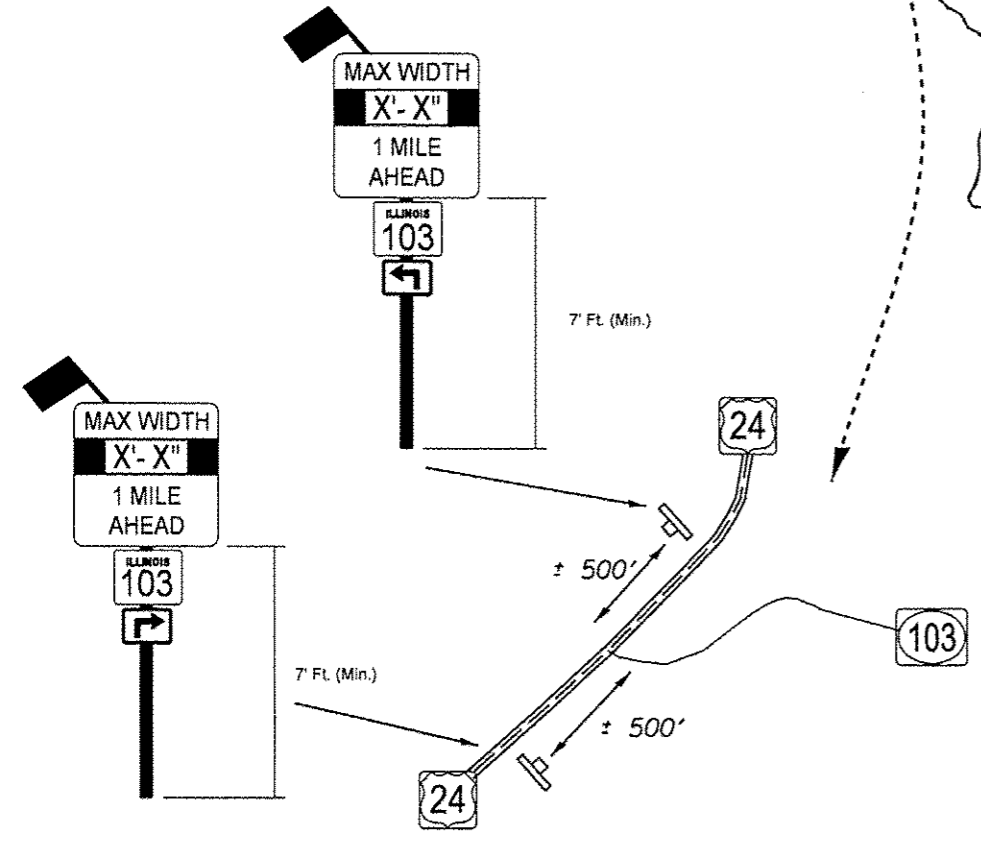
THE CONTRACTOR SHALL USE STEEL PLATES TO COVER ALL OPEN AREAS OF JOINT REMOVAL OVERNIGHT. THE COST OF FURNISHING PLATES AND ALL WORK INVOLVED WITH MOVING, SETTING, AND RESETTING THEM SHALL BE INCLUDED IN THE BID PRICE FOR TRAFFIC CONTROL AND PROTECTION STANDARD 701316.

SYMBOLS SHOWN ON THIS SHEET CORRESPOND WITH THE LEGEND SHOWN IN STANDARD 701316.

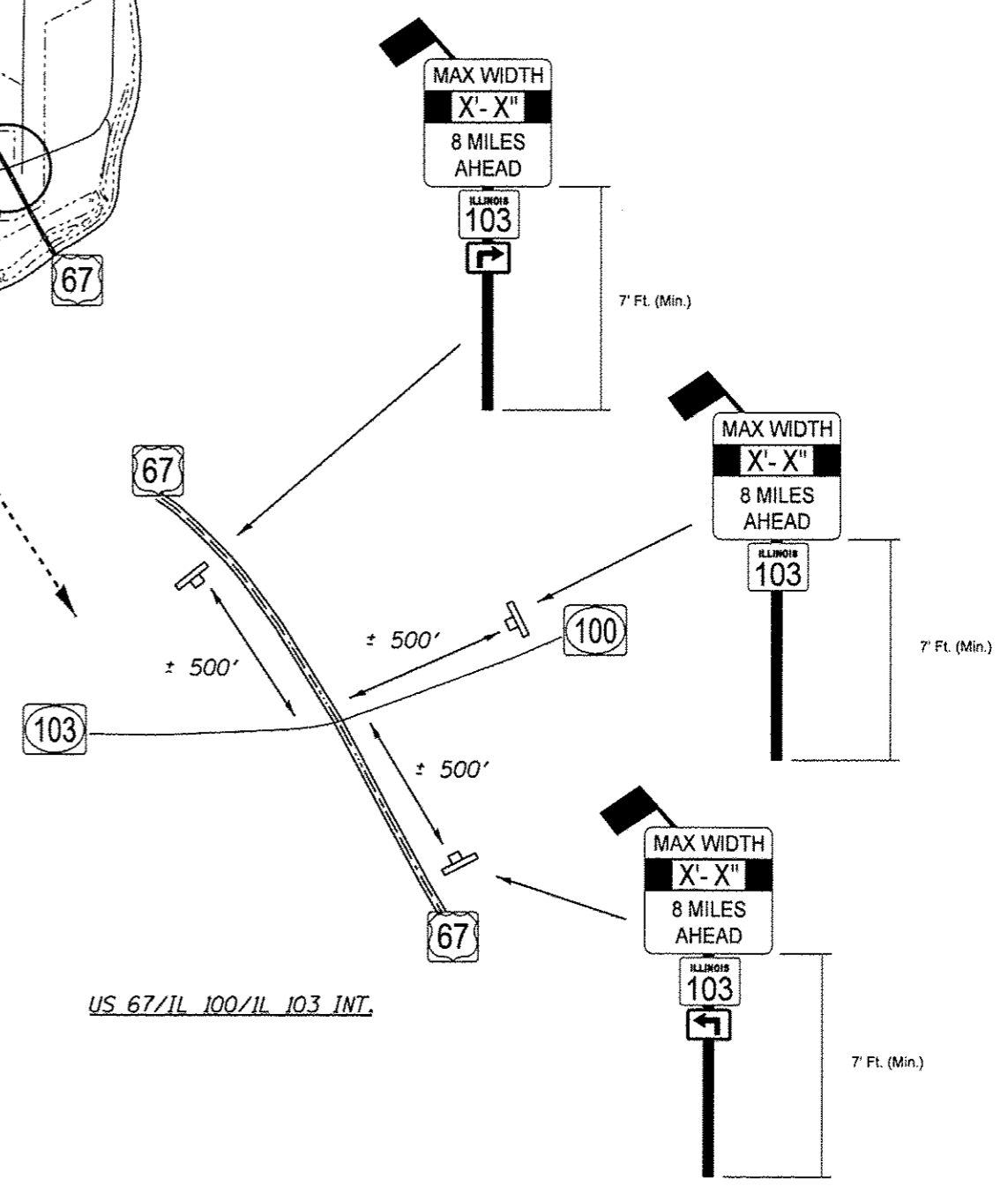
TRAFFIC STAGING DETAIL
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY



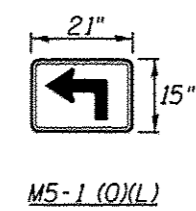
LOCATION MAP



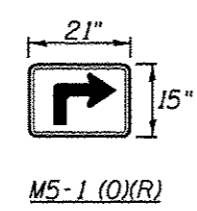
US 24/IL 103 INT.



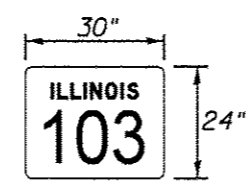
US 67/IL 100/IL 103 INT.



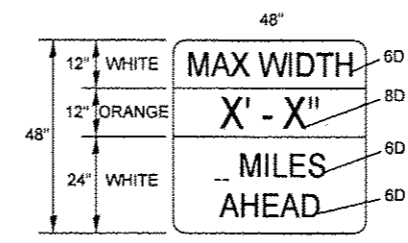
M5-1 (O)(L)



M5-1 (O)(R)

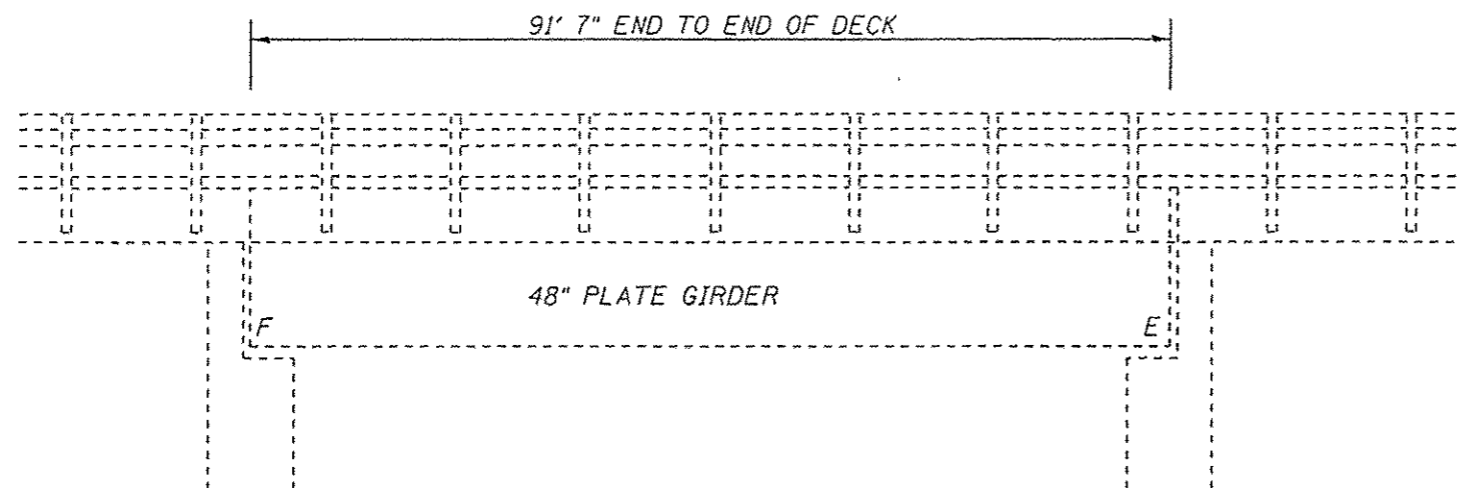


MI-1 100



W12-1 103 (48)

WIDTH RESTRICTION SIGNING
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY



BRIDGE ELEVATION

GENERAL NOTES:

ALL STRUCTURAL STEEL SHALL BE AASHTO M-270 GRADE 36, UNLESS OTHERWISE NOTED.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE RUST, MILL SCALE, AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN CONCRETE REMOVAL.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED ON THE UNIT PRICE BID FOR THE WORK.

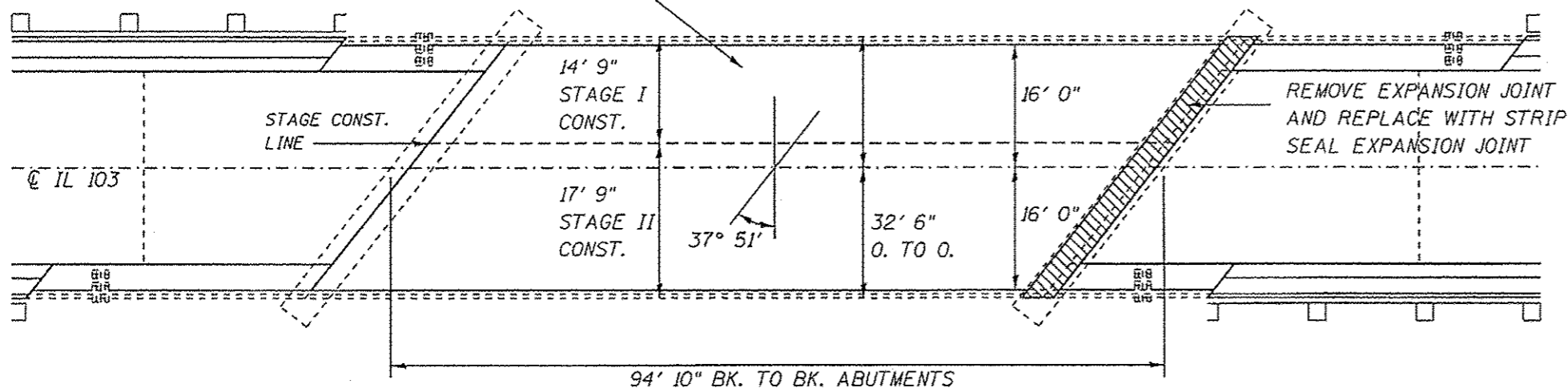
EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50°F.

THE EXISTING STRUCTURAL STEEL COATING CONTAINS LEAD. THE CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO DEAL WITH THE PRESENCE OF LEAD ON THIS PROJECT.

THE DECK SURFACE SHALL HAVE ITS FINAL FINISH TINED ACCORDING TO ARTICLE 420.09(e)(11) OF THE STANDARD SPECIFICATIONS. COST INCLUDED WITH CONCRETE SUPERSTRUCTURES.

REMOVE EXISTING HMA WEARING SURFACE
INSTALL NEW WATERPROOF MEMBRANE SYSTEM
AND NEW HMA WEARING SURFACE



BRIDGE PLAN

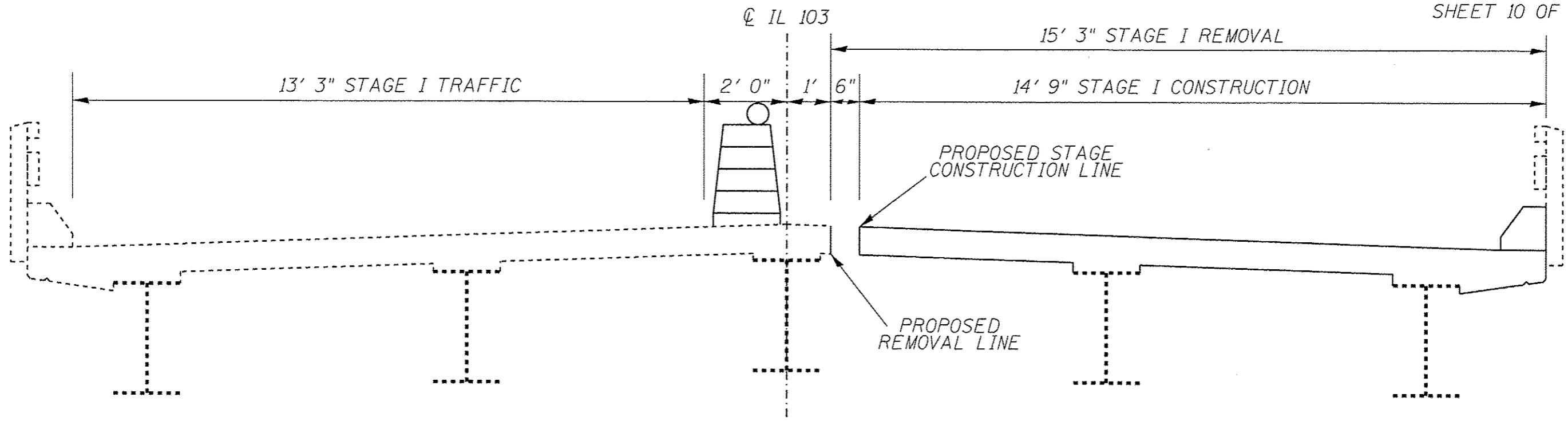


David Carl Puze 11/18/13
Expires 11/30/14

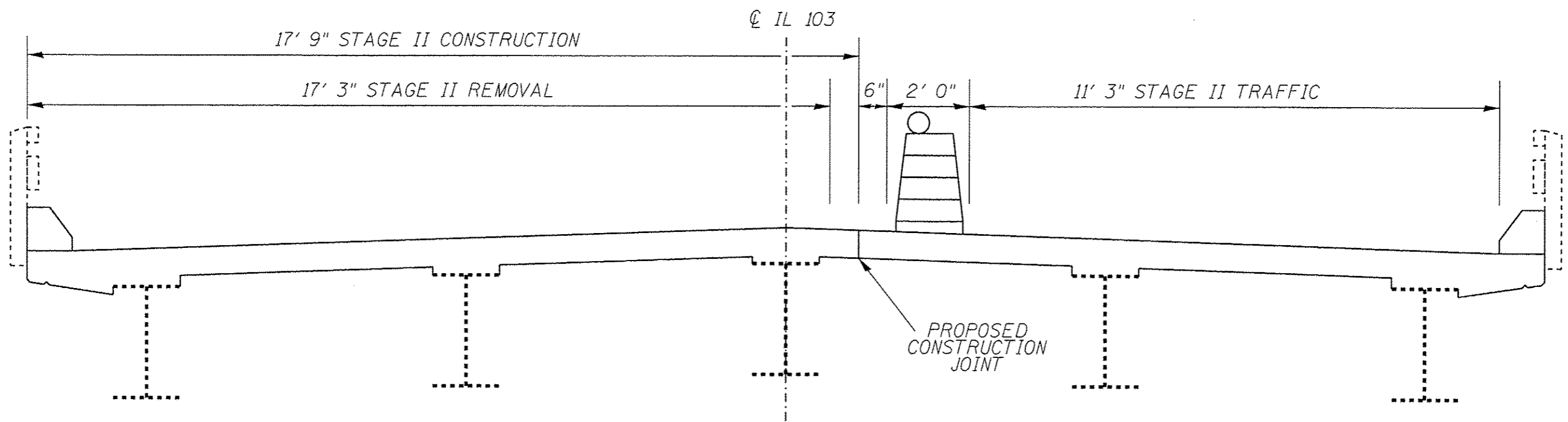
DESIGNER: DAC
CHECKER: TLC

| BRIDGE WORK - BILL OF MATERIAL | |
|--------------------------------------|-----------|
| CONCRETE REMOVAL | 6 CU YD |
| CONCRETE SUPERSTRUCTURE | 6 CU YD |
| REINFORCEMENT BARS, EPOXY COATED | 880 POUND |
| BAR SPLICERS | 12 EACH |
| PREFORMED JOINT STRIP SEAL | 45 FOOT |
| HMA SURFACE REMOVAL (DECK) | 311 SQ YD |
| WATERPROOF MEMBRANE SYSTEM (SPECIAL) | 308 SQ YD |
| HMA SURFACE COURSE, MIX "C", N.50 | 26 TONS |

BRIDGE PLAN (085-0004)
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY

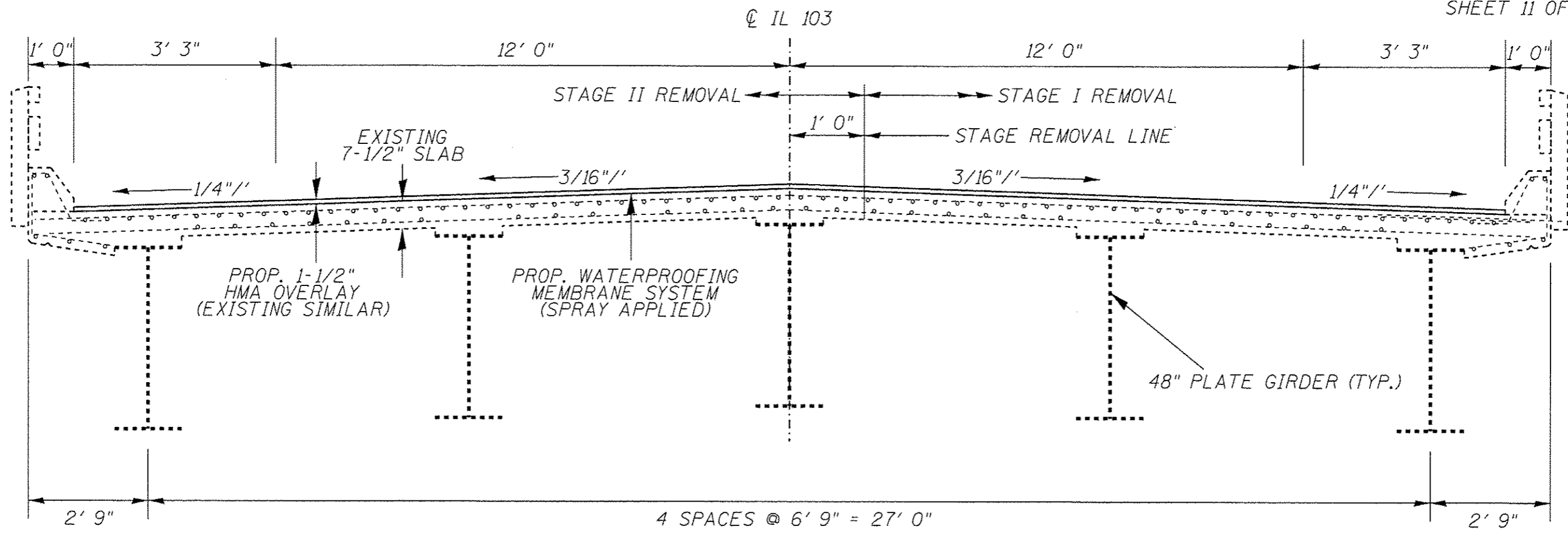


STAGE I CROSS SECTION (LOOKING WEST)



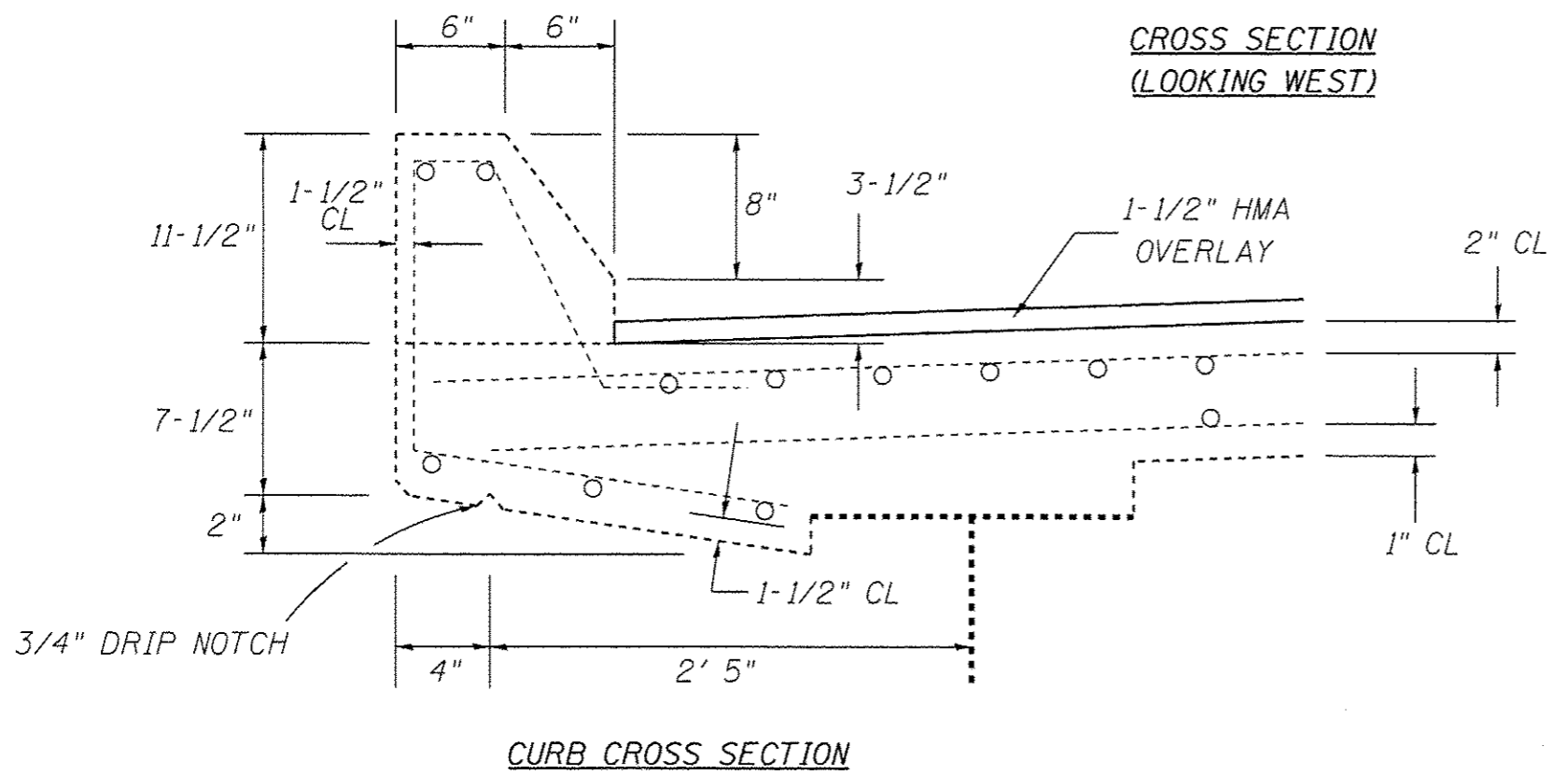
STAGE II CROSS SECTION (LOOKING WEST)

STAGING DETAIL
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY



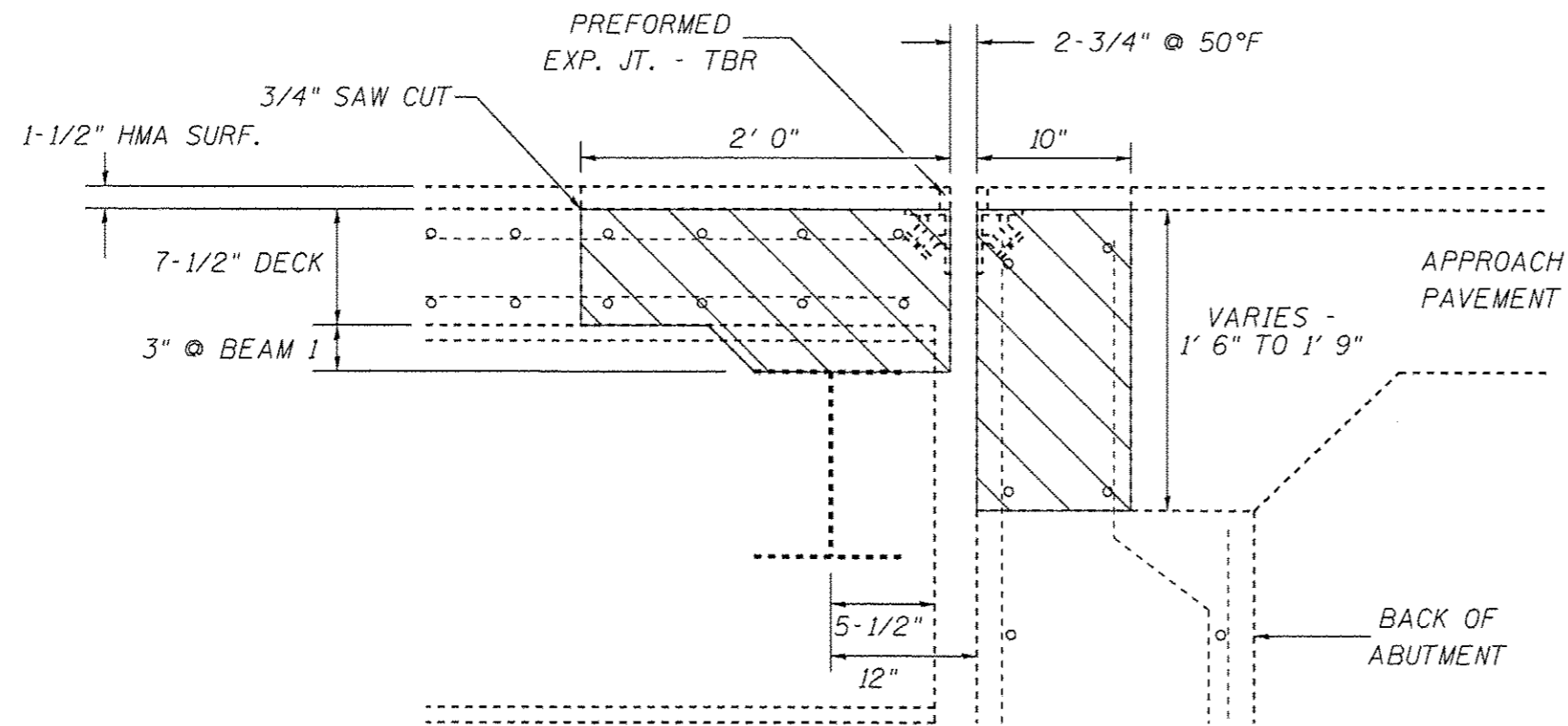
CL IL 103

CROSS SECTION
(LOOKING WEST)



CURB CROSS SECTION

EXISTING BRIDGE X-SECTIONS
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY

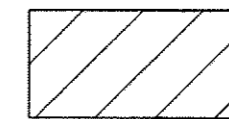


EAST ABUTMENT EXPANSION JOINT CROSS SECTION
(HORIZ. DIM. @ RIGHT ANGLES TO JOINT)

NOTES:

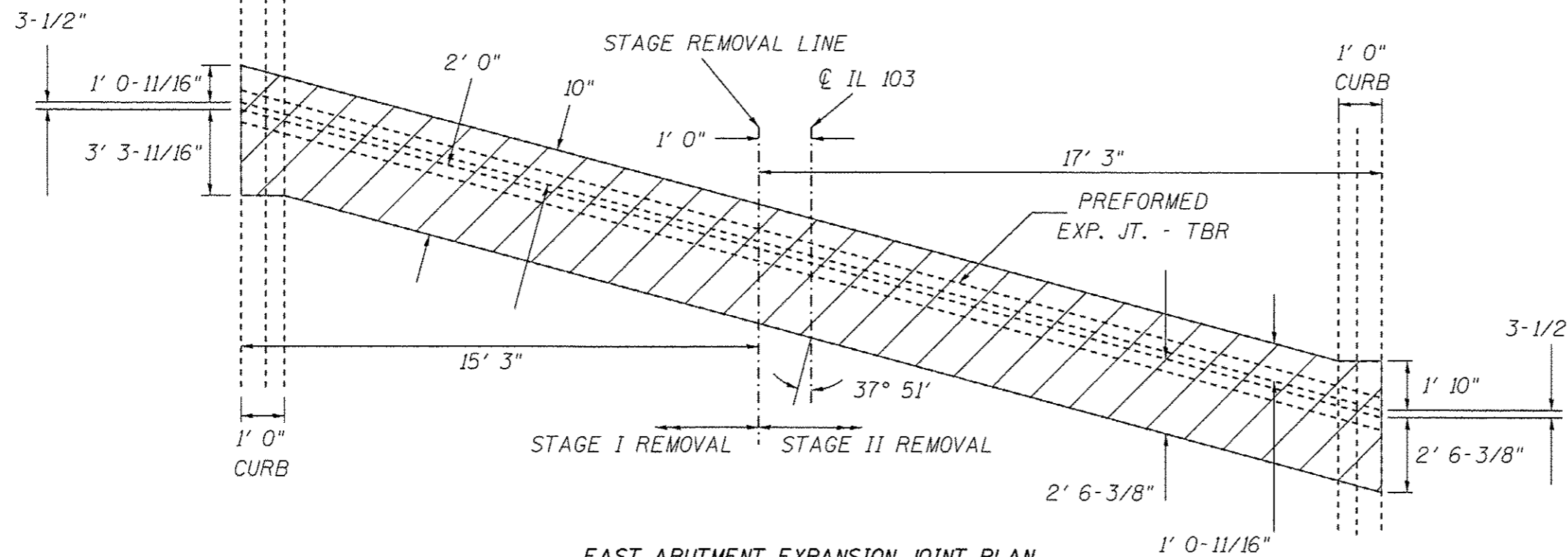
THE EXISTING EXPANSION JOINT SYSTEMS SHALL BE REMOVED COMPLETELY, AS WELL AS ANY FOREIGN MATERIAL THAT HAS ACCUMULATED OR BEEN PLACED IN THE JOINT OPENINGS. THE COST FOR THIS WORK IS INCLUDED IN CONCRETE REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE COST OF CUTTING THE EXISTING STEEL 3/4" X 8" STUDS AT THE CONCRETE REMOVAL LINE SHALL BE INCLUDED IN THE COST OF CONCRETE REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



CONCRETE
REMOVAL AREAS

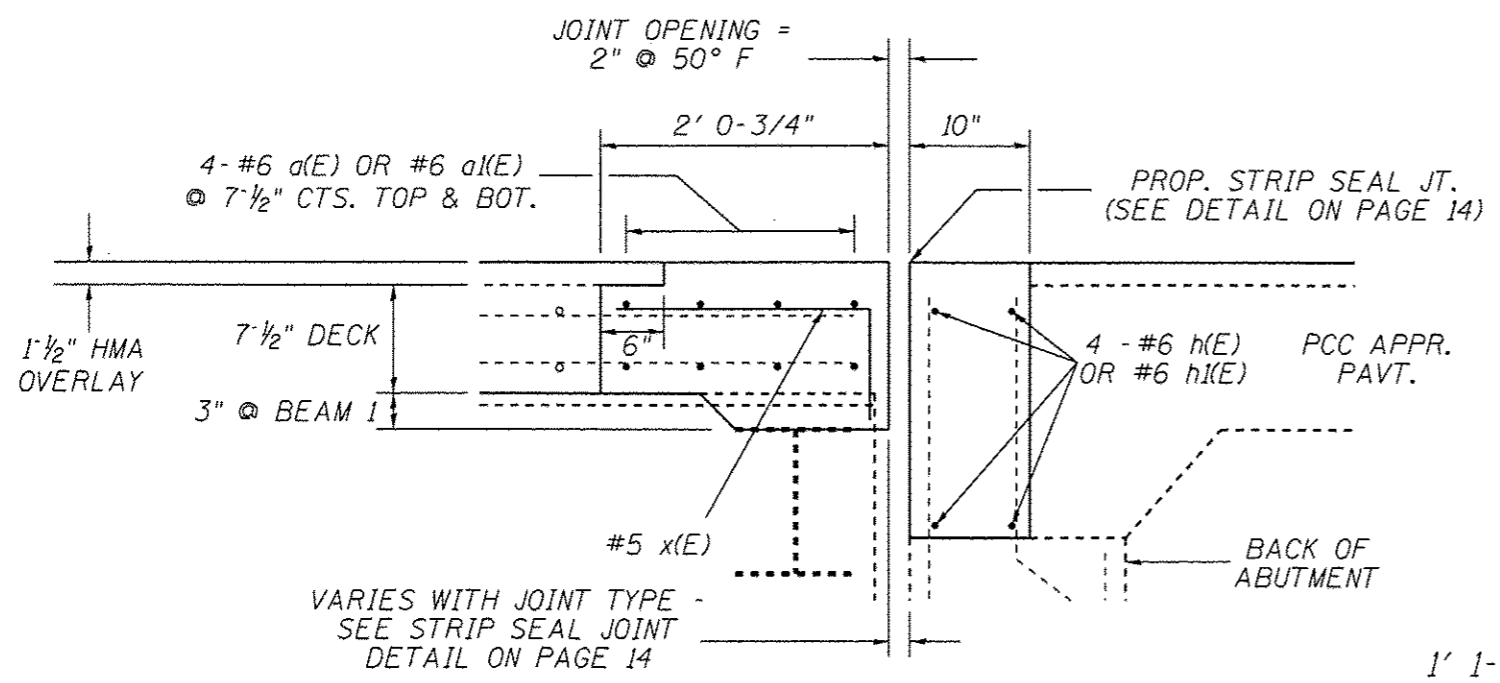
SEE SHEET 13 FOR BILL OF MATERIALS



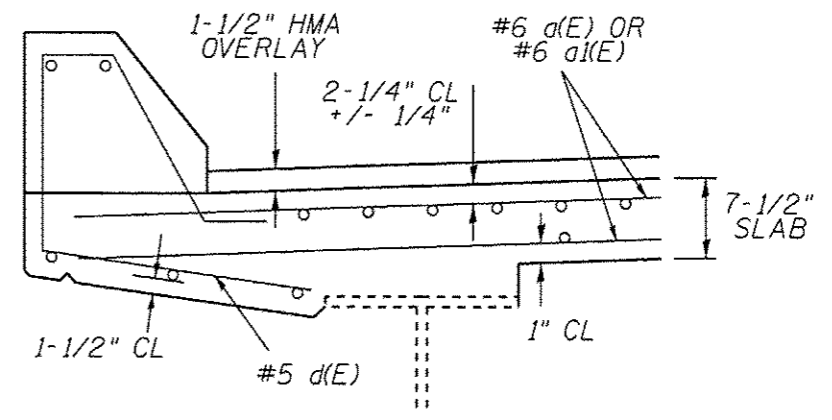
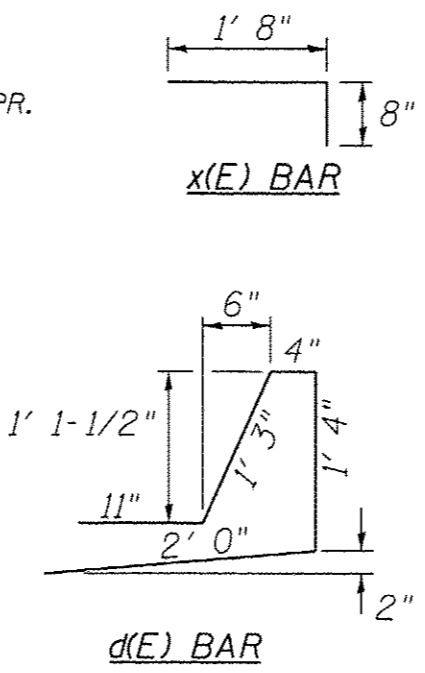
EAST ABUTMENT EXPANSION JOINT PLAN



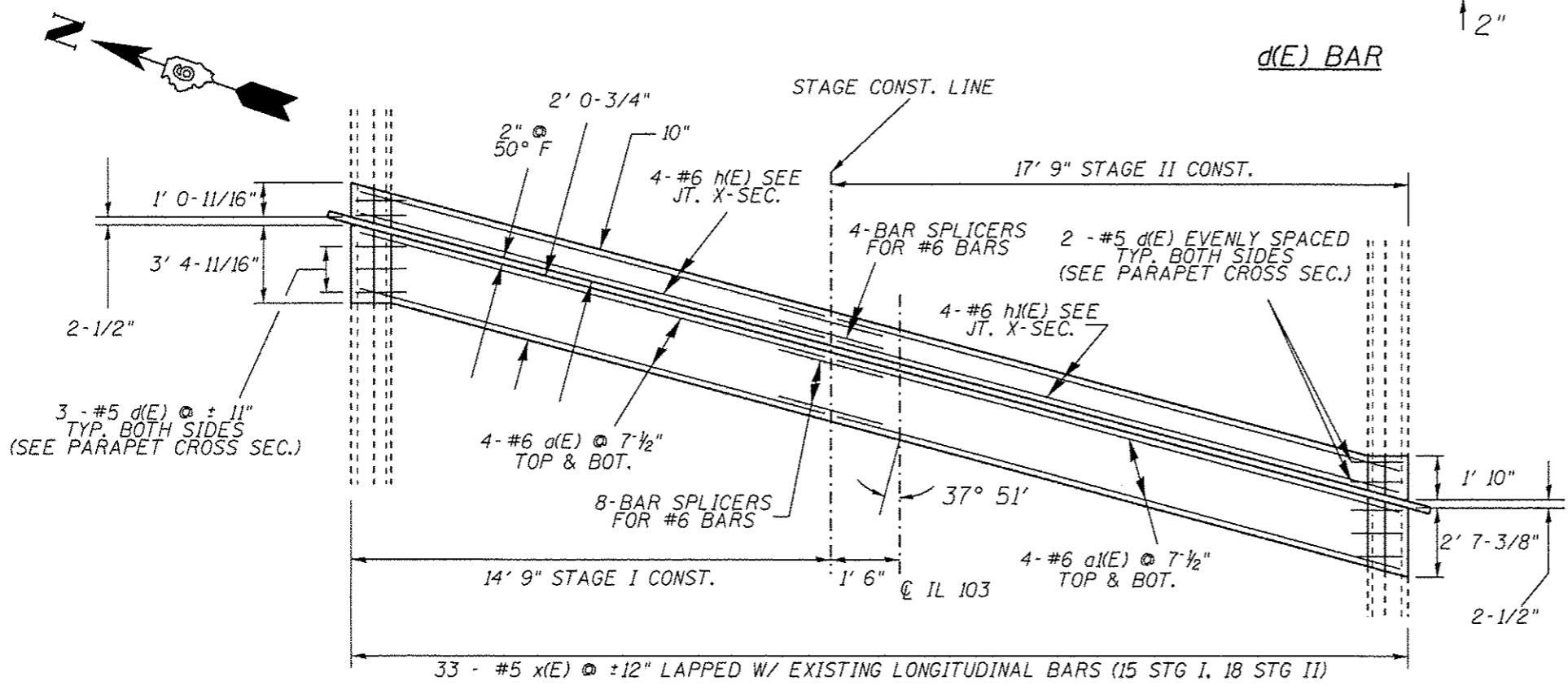
REMOVAL DETAIL
FAP 612 (IL 103)
SECTION (101) I-2
SCHUYLER COUNTY



EAST ABUTMENT JOINT CROSS SECTION
(HORIZ. DIM. @ RIGHT ANGLES TO JOINT)



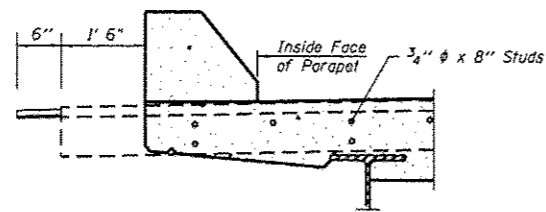
TYPICAL PARAPET CROSS SECTION



EAST ABUTMENT JOINT PLAN

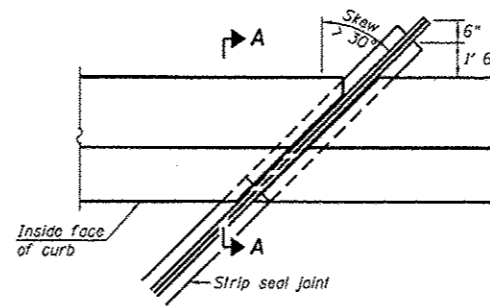
| BILL OF MATERIAL | | | | |
|-----------------------------------|----|-------|--------|-------|
| BAR | # | SIZE | LENGTH | SHAPE |
| d(E) | 8 | #6 | 18' 4" | — |
| a(E) | 8 | #6 | 22' 1" | — |
| d(E) | 10 | #5 | 5' 10" | ⌋ |
| h(E) | 4 | #6 | 18' 4" | — |
| h(E) | 4 | #6 | 22' 1" | — |
| x(E) | 33 | #5 | 2' 4" | ⌋ |
| REINFORCEMENT BARS (EPOXY COATED) | | POUND | 880 | |
| CONC. SUPERSTRUCTURE | | CU YD | 6 | |
| CONC. REMOVAL | | CU YD | 6 | |
| BAR SPLICERS | | EACH | 12 | |

EXPANSION JOINT DETAILS
FAP 612 (IL 103)
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SCHUYLER COUNTY

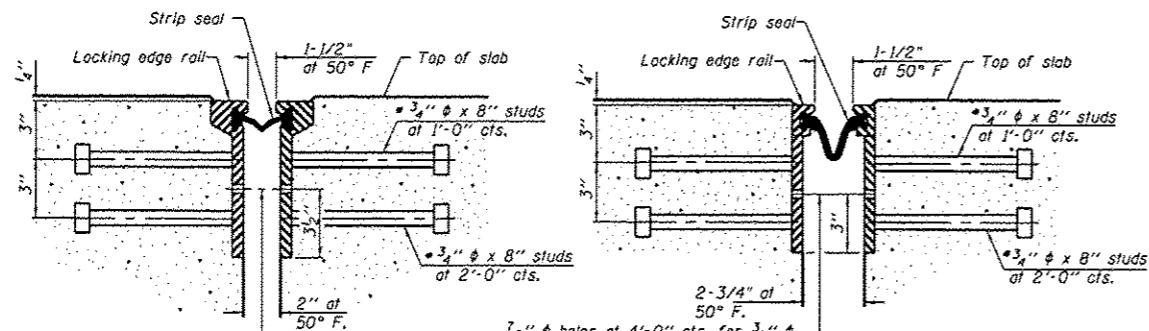


SECTION A-A

POINT BLOCK DETAILS
(for skews > 30°)



PLAN
(For skews > 30°)
Showing point block



1/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU
ROLLED RAIL JOINT

• Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

SECTION THRU
WELDED RAIL JOINT

ROLLED
EXTRUDED RAIL

WELDED RAIL

LOCKING EDGE
RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

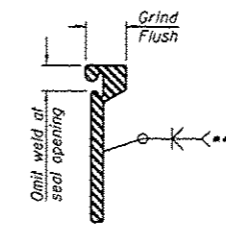
The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications. Maximum space between rail segments shall be 3/16", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

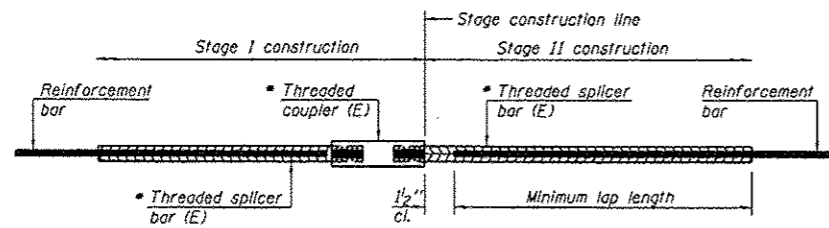


*** Back gouge not required if complete joint penetration is verified by mock-up.

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 45 |

STRIP SEAL DETAILS
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STANDARD BAR SPLICER ASSEMBLY

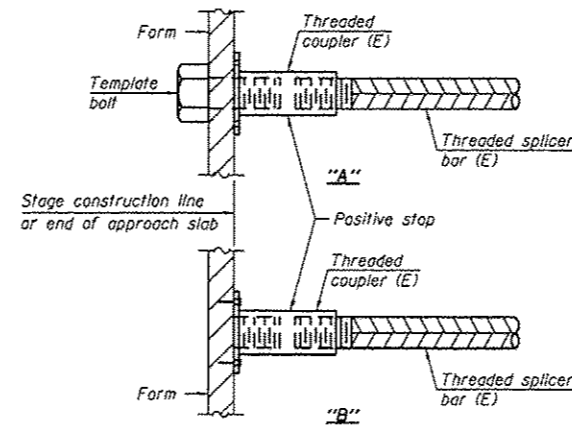
| Bar size to be spliced | Minimum Lap Lengths | | | | | |
|------------------------|---------------------|---------|---------|---------|---------|---------|
| | Table 1 | Table 2 | Table 3 | Table 4 | Table 5 | Table 6 |
| 3, 4 | 1'-5" | 1'-11" | 2'-1" | 2'-4" | 2'-7" | 2'-11" |
| 5 | 1'-9" | 2'-5" | 2'-7" | 2'-11" | 3'-3" | 3'-8" |
| 6 | 2'-1" | 2'-11" | 3'-1" | 3'-6" | 3'-10" | 4'-5" |
| 7 | 2'-9" | 3'-10" | 4'-2" | 4'-8" | 5'-2" | 5'-10" |
| 8 | 3'-8" | 5'-1" | 5'-5" | 6'-2" | 6'-9" | 7'-8" |
| 9 | 4'-7" | 6'-5" | 6'-10" | 7'-9" | 8'-7" | 9'-8" |

- Table 1: Black bar, O.B Class C
- Table 2: Black bar, Top bar lap, O.B Class C
- Table 3: Epoxy bar, O.B Class C
- Table 4: Epoxy bar, Top bar lap, O.B Class C
- Table 5: Epoxy bar, Class C
- Table 6: Epoxy bar, Top bar lap, Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

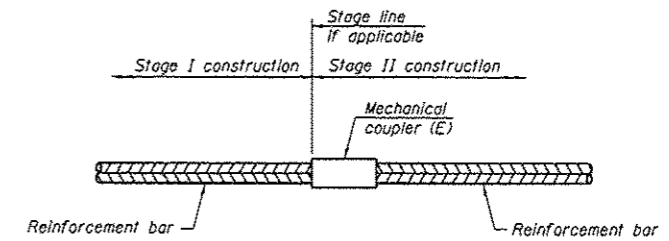
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Table for minimum lap length |
|-------------------|----------|-------------------------|------------------------------|
| Approaches | #6 | 4 | 3 |
| Deck (Top Mat) | #6 | 4 | 3 |
| Deck (Bottom Mat) | #6 | 4 | 3 |



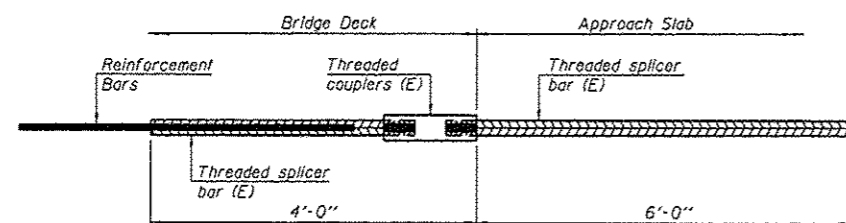
INSTALLATION AND SETTING METHODS

- "A": Set bar splicer assembly by means of a template bolt.
- "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
- (E): Indicates epoxy coating.



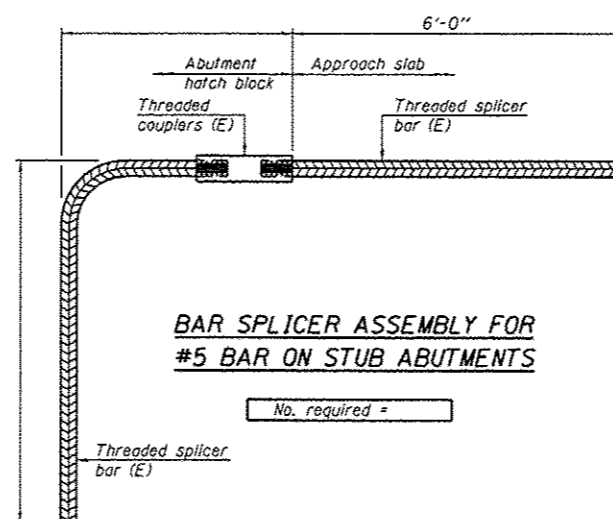
STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =

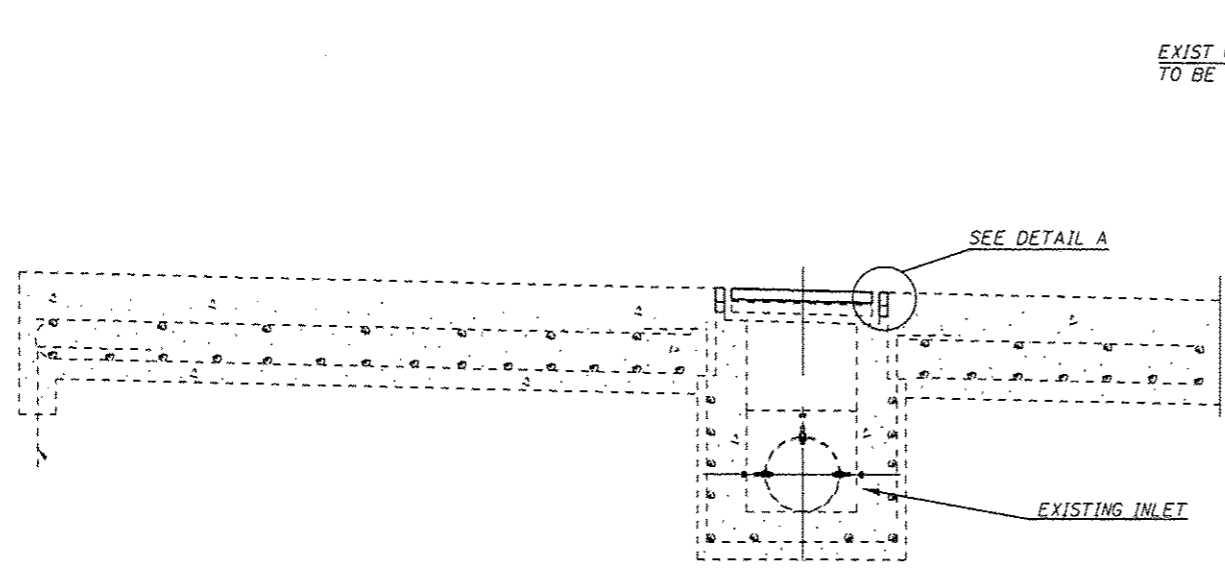


BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

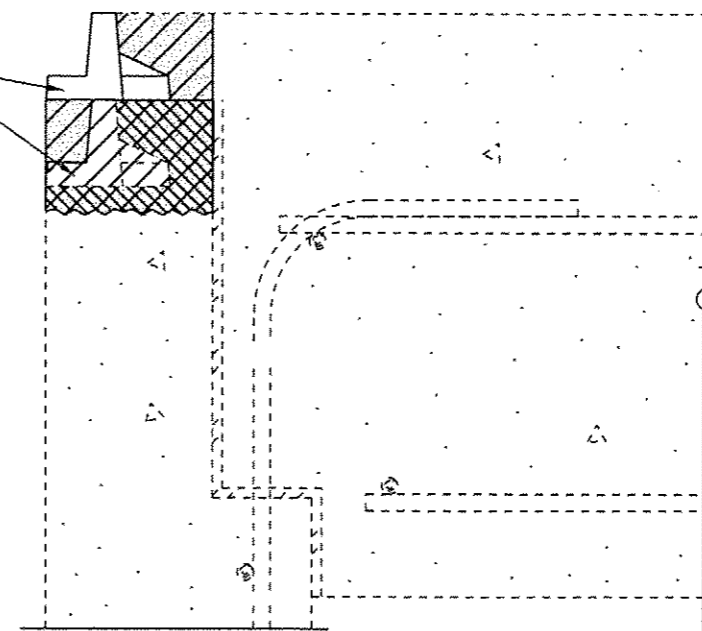
NOTES

- Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
- All reinforcement shall be lapped and tied to the splicer bars.
- Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
- See approved list of bar splicer assemblies and mechanical splicers for alternatives.



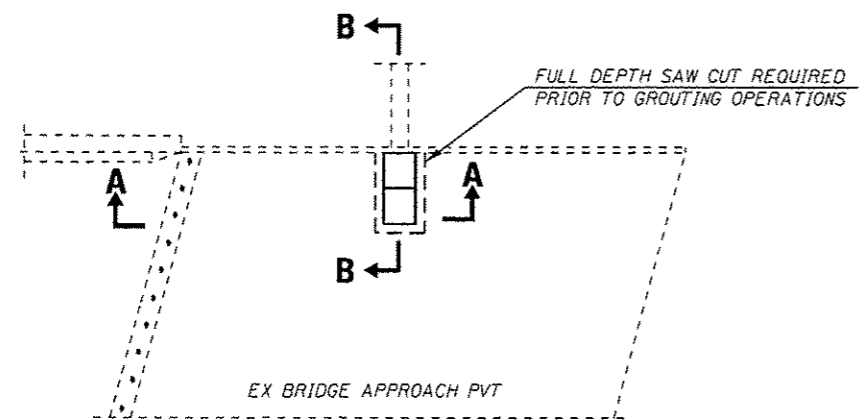
SECTION A-A

EXIST CASTING AND GRATE
TO BE REMOVED AND REINSTALLED

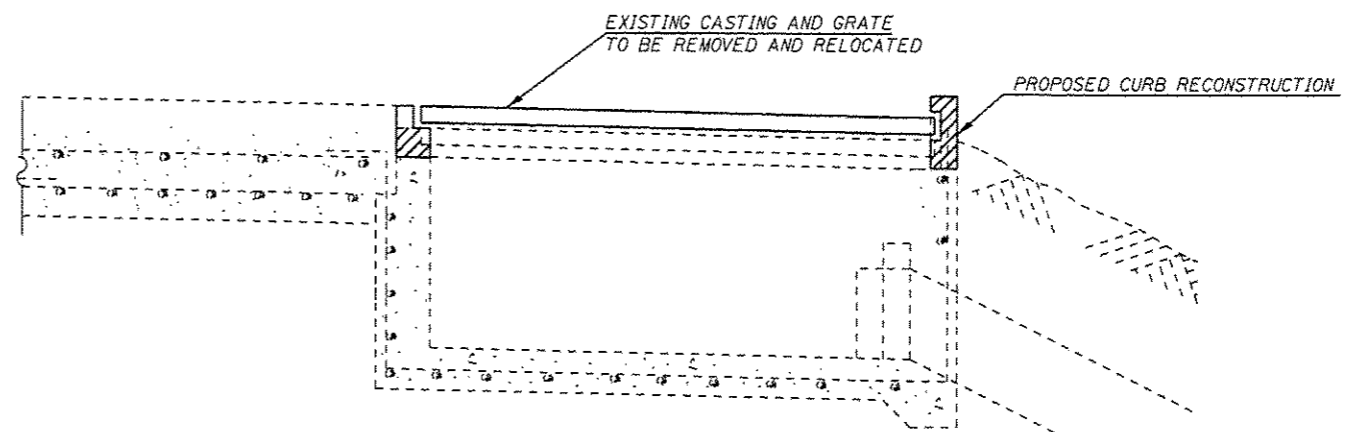


DETAIL A

- CONCRETE TO BE REMOVED AND REPLACED WITH POLYMER CONCRETE
- PROPOSED POLYMER CONCRETE POUR



TYPICAL DETAIL PLAN



SECTION B-B

INLET ADJUSTMENT DETAILS
FAP 612 (IL 103)
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SCHUYLER COUNTY