

CITY OF AURORA AURORA, ILLINOIS

CONSTRUCTION PLANS FOR AURORA MUNICIPAL AIRPORT

CLEAN AND SEAL JOINTS ON RUNWAY 15/33

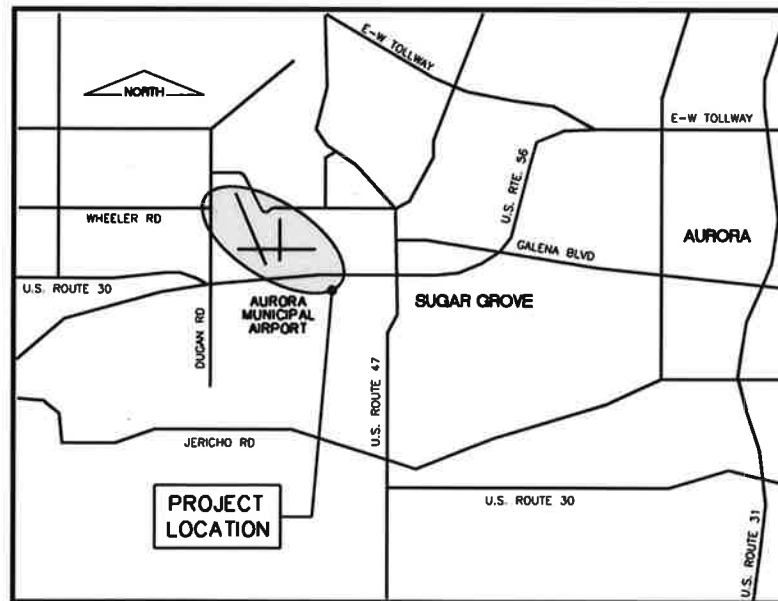
ILLINOIS PROJECT: ARR-4285
S.B.G. PROJECT: 3-17-0003-95/99/XX

NOVEMBER 15, 2013

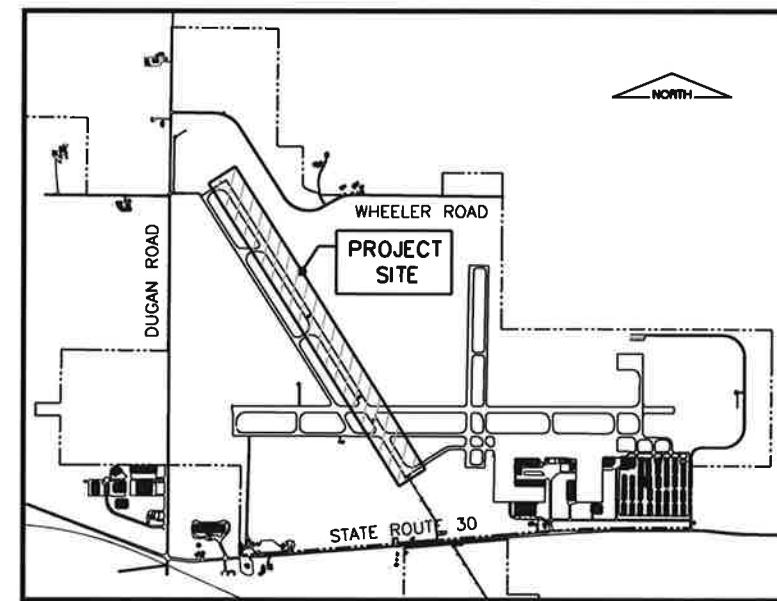


D. Kyle Peabody
11-25-13
EXPIRES 11-30-15

INDEX TO SHEETS	
1	COVER SHEET
2	SITE PLAN / SUMMARY OF QUANTITIES
3	SEQUENCE OF CONSTRUCTION PER AC 150/5370-2F (LATEST EDITION)
4	SEQUENCE OF CONSTRUCTION GENERAL NOTES
5	SEQUENCE OF CONSTRUCTION DETAILS
6	JOINT AND PAVEMENT IMPROVEMENTS - RUNWAY 15/33
7	JOINT AND PAVEMENT IMPROVEMENTS - TAXIWAY ALPHA
8	PAVEMENT JOINTING DETAILS
9	MISCELLANEOUS DETAILS



LOCATION MAP



SITE PLAN

811 Know what's below. Call before you dig. www.illinois1call.com

J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E BEFORE EXCAVATING AT 811

AURORA MUNICIPAL AIRPORT

SECTION: 7 AND 18	COUNTY: KANE
RANGE: 7 EAST	U.S. ROUTE 30
TOWNSHIP: 38 NORTH	SUGAR GROVE TOWNSHIP

13285-02

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS

■ SPRINGFIELD, IL ■ AURORA, IL ■ CHICAGO, IL ■ COLUMBUS, OH ■ EDWARDSVILLE, IL
■ INDIANAPOLIS, IN ■ PEORIA, IL ■ ROCKFORD, IL ■ ST. LOUIS, MO ■ SPRINGFIELD, MO

SUBMITTED BY *D. Kyle Peabody*
D. KYLE PEABODY, P.E.

DATE *November 25* 20*13*

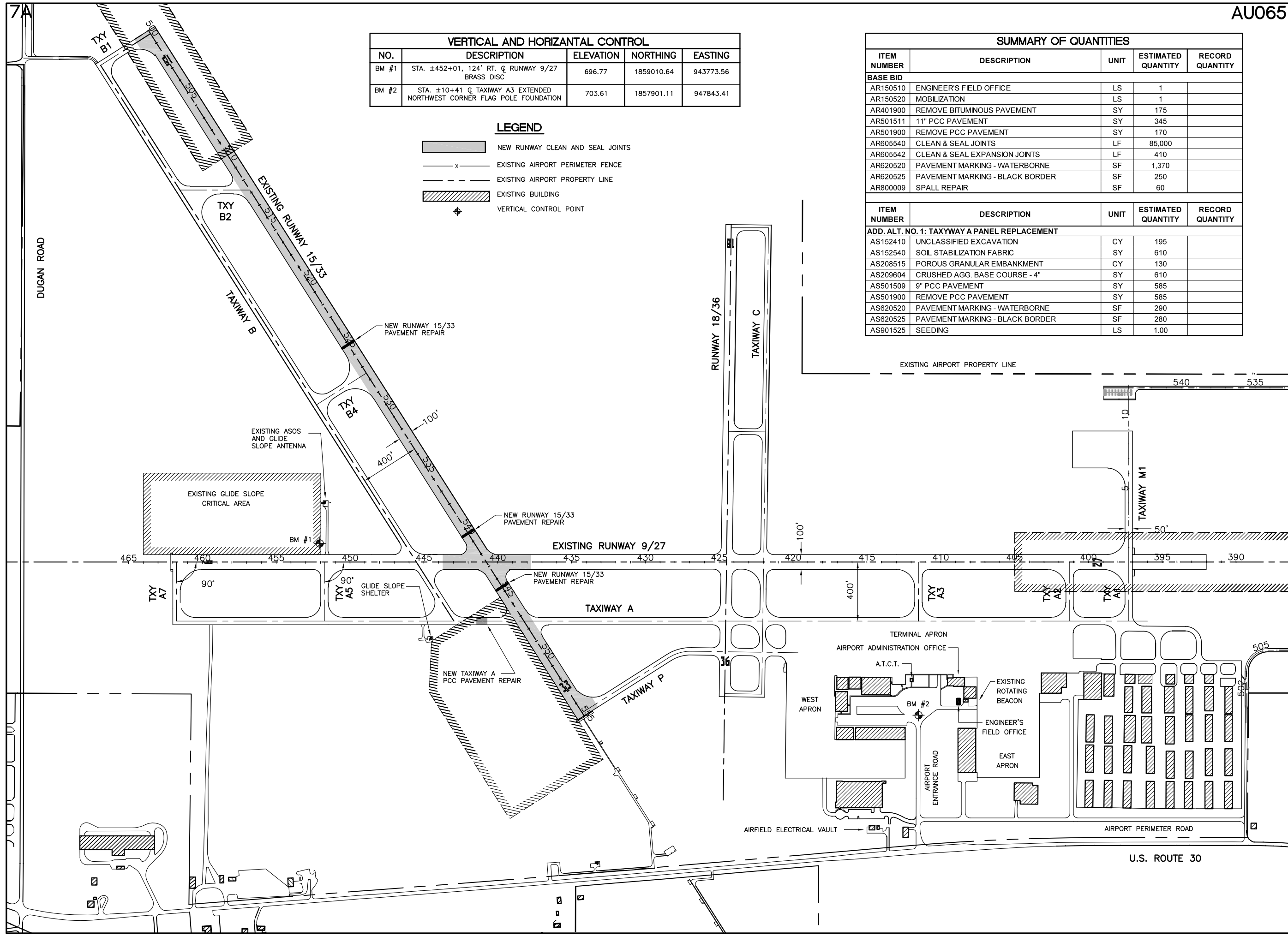
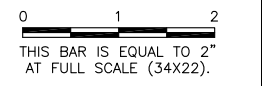
VERTICAL AND HORIZONTAL CONTROL				
NO.	DESCRIPTION	ELEVATION	NORTHING	EASTING
BM #1	STA. ±452+01, 124' RT. C. RUNWAY 9/27 BRASS DISC	696.77	1859010.64	943773.56
BM #2	STA. ±10+41 C TAXIWAY A3 EXTENDED NORTHWEST CORNER FLAG POLE FOUNDATION	703.61	1857901.11	947843.41

- LEGEND**
- NEW RUNWAY CLEAN AND SEAL JOINTS
 - EXISTING AIRPORT PERIMETER FENCE
 - EXISTING AIRPORT PROPERTY LINE
 - EXISTING BUILDING
 - VERTICAL CONTROL POINT

SUMMARY OF QUANTITIES				
ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
BASE BID				
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	175	
AR501511	11" PCC PAVEMENT	SY	345	
AR501900	REMOVE PCC PAVEMENT	SY	170	
AR605540	CLEAN & SEAL JOINTS	LF	85,000	
AR605542	CLEAN & SEAL EXPANSION JOINTS	LF	410	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	1,370	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	250	
AR800009	SPALL REPAIR	SF	60	

ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
ADD. ALT. NO. 1: TAXIWAY A PANEL REPLACEMENT				
AS152410	UNCLASSIFIED EXCAVATION	CY	195	
AS152540	SOIL STABILIZATION FABRIC	SY	610	
AS208515	POROUS GRANULAR EMBANKMENT	CY	130	
AS209604	CRUSHED AGG. BASE COURSE - 4"	SY	610	
AS501509	9" PCC PAVEMENT	SY	585	
AS501900	REMOVE PCC PAVEMENT	SY	585	
AS620520	PAVEMENT MARKING - WATERBORNE	SF	290	
AS620525	PAVEMENT MARKING - BLACK BORDER	SF	280	
AS901525	SEEDING	LS	1.00	

REVISIONS		
NUMBER	BY	DATE



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
CLEAN AND SEAL JOINTS ON RUNWAY 15/33
SITE PLAN/
SUMMARY OF QUANTITIES

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CHICAGO AURORA MUNICIPAL AIRPORT

DESIGN BY:	AAO
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	11/15/13
JOB No:	13285-02
ILLINOIS PROJECT:	ARR-4285
S.B.C. PROJECT:	3-17-0003-95/99/XX
SHEET	2 OF 9 SHEETS



DESIGN AIRCRAFT APPROACH CATEGORY: D
 DESIGN AIRPORT GROUP: III

GROUND CONTROL FREQUENCY: 121.70
 AIR CONTROL FREQUENCY: 120.60
 MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25'
 CLOSEST CONSTRUCTION POINT TO RUNWAY 9/27 (POINT A)
 ELEV. = 694.48
 LATITUDE: 41°46'15.31"
 LONGITUDE: 88°28'41.40"
 TO RUNWAY 9/27 (POINT B)
 ELEV. = 695.35
 LATITUDE: 41°46'12.87"
 LONGITUDE: 88°28'39.26"
 TO RUNWAY 18/36 (POINT C)
 ELEV. = 702.70
 LATITUDE: 41°46'13.09"
 LONGITUDE: 88°28'19.78"

EXISTING CRITICAL AIRCRAFT AND REQUIRED SAFETY AREAS			
RUNWAY	9/27	15/33	18/36
APPROACH CATEGORY	D	D	B
DESIGN GROUP	III	III	II
DESIGN AIRCRAFT	GULFSTREAM 550	GULFSTREAM 550	KING AIR 200
WINGSPAN	94 FEET	94 FEET	55 FEET
TAIL HEIGHT	25.8 FEET	25.8 FEET	15 FEET
AOA ● RUNWAY SAFETY AREA WIDTH (RSA)	500	500	150
RUNWAY OBJECT FREE AREA WIDTH (ROFA)	800	800	500
TAXIWAY SAFETY AREA WIDTH (TSA)	118	118	79
AOA ● TAXIWAY OBJECT FREE AREA WIDTH (TOFA)	152 (SEE NOTE 1)	152 (SEE NOTE 1)	131

AOA = AIRCRAFT OPERATIONS AREA
 NOTE 1: COMPUTED TOFA = (94' x 0.7 + 10') x 2 = 151.6' (USE 152')

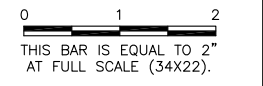
WORK AREA	ALLOWABLE WORK PERIODS	OPERATIONAL STATUS/RESTRICTIONS
PHASE 1A - RUNWAY 15/33 OUTSIDE RUNWAY 9/27 A.O.A.; TAXIWAY A	NO RESTRICTIONS	RUNWAY 9/27 OPEN; RUNWAY 15/33 CLOSED; TAXIWAY A FROM TAXIWAY B TO RUNWAY 18/36 CLOSED; TAXIWAY B CLOSED
PHASE 1B - RUNWAY 15/33 WITHIN RUNWAY 9/27 A.O.A.; TAXIWAY A	24 HOUR PERIOD (START AT 7:00 AM) FOR PCC PAVING AND CLEAN AND SEAL JOINTS ONE WEEKEND (SATURDAY AT 7:00 AM TO SUNDAY AT 7:00 PM) FOR REMAINING CLEAN AND SEAL JOINTS	RUNWAY 9/27 CLOSED; RUNWAY 15/33 CLOSED; ALL TAXIWAYS WEST OF RUNWAY 18/36 CLOSED

LEGEND

- RUNWAY CLEAN AND SEAL JOINTS
- NEW RUNWAY 15/33 PAVEMENT REPAIR
- NEW TAXIWAY A PAVEMENT REPAIR
- EXISTING AIRPORT PROPERTY LINE
- EXISTING 6" CLASS E FENCE
- AIR OPERATIONS AREA (A.O.A.)
- ACTIVE RUNWAYS: USE RUNWAY SAFETY AREA
- ACTIVE TAXIWAYS: USE TAXIWAY OBJECT FREE AREA
- AIRCRAFT MOVEMENT AREAS
- PHASE 1A - LOW PROFILE BARRICADES WITH RED LIGHTS AND SIGNS "DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA"
- PHASE 1B - LOW PROFILE BARRICADES WITH RED LIGHTS AND SIGNS "DO NOT ENTER" AND "AIRCRAFT MOVEMENT AREA"
- CONTRACTOR'S ACCESS/HAUL ROAD
- TEMPORARY CLOSED RUNWAY MARKER
- TEMPORARY CLOSED TAXIWAY MARKER
- CROSSING GUARD AND TEMPORARY STOP-AOA SIGN (SEE NOTE 1)

REVISIONS

NUMBER	BY	DATE



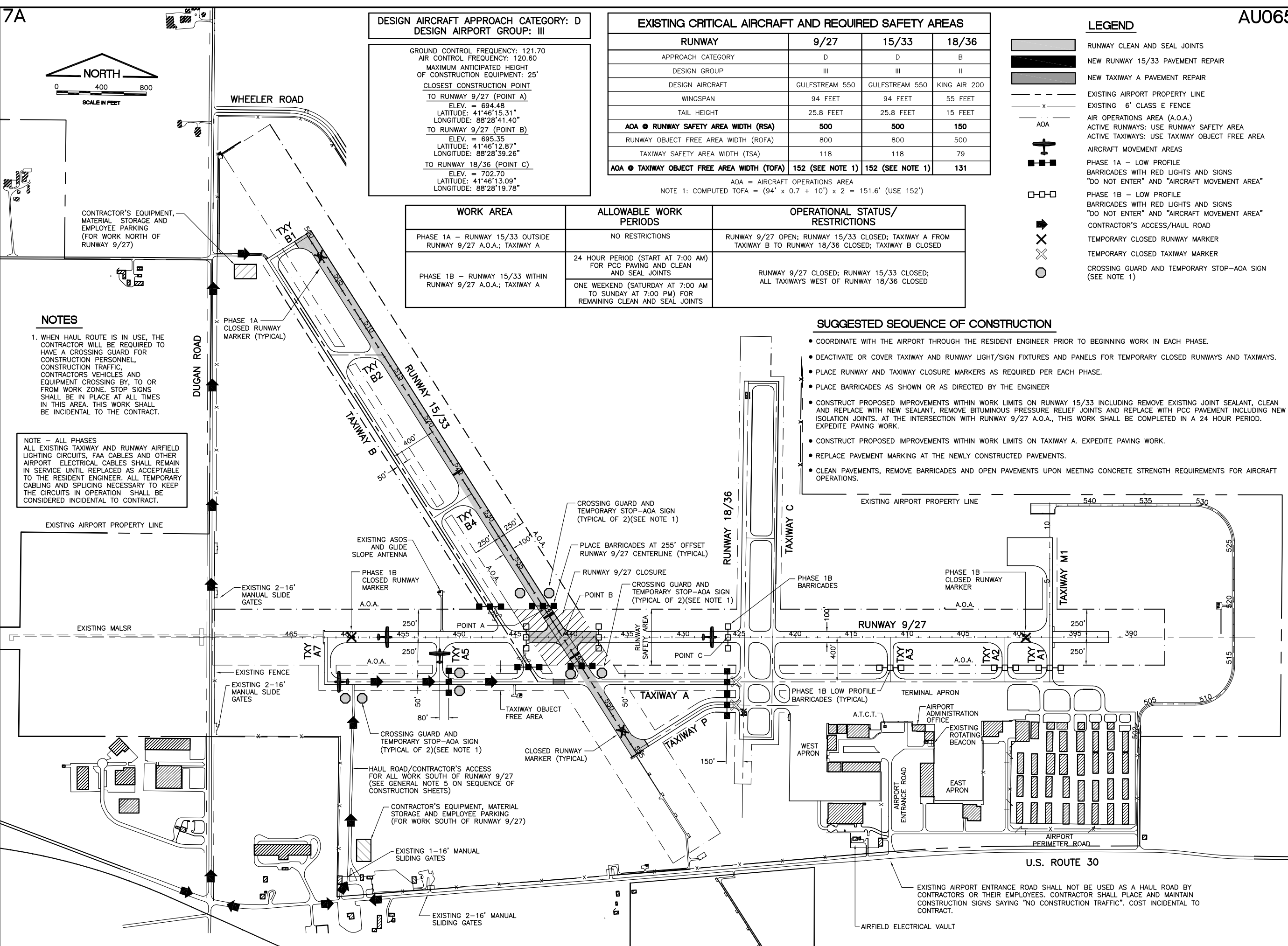
NOTES

1. WHEN HAUL ROUTE IS IN USE, THE CONTRACTOR WILL BE REQUIRED TO HAVE A CROSSING GUARD FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.

NOTE - ALL PHASES ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER PRIOR TO BEGINNING WORK IN EACH PHASE.
- DEACTIVATE OR COVER TAXIWAY AND RUNWAY LIGHT/SIGN FIXTURES AND PANELS FOR TEMPORARY CLOSED RUNWAYS AND TAXIWAYS.
- PLACE RUNWAY AND TAXIWAY CLOSURE MARKERS AS REQUIRED PER EACH PHASE.
- PLACE BARRICADES AS SHOWN OR AS DIRECTED BY THE ENGINEER
- CONSTRUCT PROPOSED IMPROVEMENTS WITHIN WORK LIMITS ON RUNWAY 15/33 INCLUDING REMOVE EXISTING JOINT SEALANT, CLEAN AND REPLACE WITH NEW SEALANT, REMOVE BITUMINOUS PRESSURE RELIEF JOINTS AND REPLACE WITH PCC PAVEMENT INCLUDING NEW ISOLATION JOINTS. AT THE INTERSECTION WITH RUNWAY 9/27 A.O.A., THIS WORK SHALL BE COMPLETED IN A 24 HOUR PERIOD. EXPEDITE PAVING WORK.
- CONSTRUCT PROPOSED IMPROVEMENTS WITHIN WORK LIMITS ON TAXIWAY A. EXPEDITE PAVING WORK.
- REPLACE PAVEMENT MARKING AT THE NEWLY CONSTRUCTED PAVEMENTS.
- CLEAN PAVEMENTS, REMOVE BARRICADES AND OPEN PAVEMENTS UPON MEETING CONCRETE STRENGTH REQUIREMENTS FOR AIRCRAFT OPERATIONS.



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
CLEAN AND SEAL JOINTS ON RUNWAY 15/33
SEQUENCE OF CONSTRUCTION
PER AC 150/5370-2F (LATEST EDITION)

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AURORA
 MUNICIPAL AIRPORT

DESIGN BY:	AAO
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	11/15/13
JOB No:	13285-02
ILLINOIS PROJECT:	ARR-4285
S.B.G. PROJECT:	3-17-0003-95/99/XX
SHEET	3 OF 9 SHEETS

GENERAL NOTES

1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
2. ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2F (LATEST EDITION) **OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION**.
3. CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
4. THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
5. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR.
6. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
7. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
8. THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT LIGHTS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
9. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
11. PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, TAXIWAY AND RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
12. THE CONTRACTOR SHALL CONTACT THE AIRPORT DIRECTOR THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
14. CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
15. IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.
16. DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
17. THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
18. IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
19. AURORA MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
20. APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
21. MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

22. LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
23. COORDINATION MEETINGS – THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
24. THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
25. DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
26. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER.
27. CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
28. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
29. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
30. ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT DIRECTOR. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
31. ORANGE CONES SHALL BE PLACED AT 25' CENTERS ALONG THE PAVEMENT EDGE DURING CONCRETE POURING OPERATIONS OF THE CLOSURE LANES TO PREVENT VEHICLES FROM ENTERING PLASTIC CONCRETE. IN THE EVENT A VEHICLE ENTERS THE CONCRETE BEFORE A MINIMUM COMPRESSIVE STRENGTH OF 3,500 PSI HAS BEEN OBTAINED, SAID PAVEMENT SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
32. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS AND TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP OR I.D.O.T..

CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

1. ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
2. ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
3. WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREA (RSA) AND TAXIWAY OBJECT FREE AREA (TOFA)**RUNWAYS:**

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED UNLESS OTHERWISE SHOWN ON THE PLANS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

TAXIWAYS:

ANY WORK WITHIN TAXIWAY OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY SAFETY AREAS OF OPEN TAXIWAYS.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 30-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE WORK SITE DUE TO ADJACENT CONSTRUCTION

- CONSTRUCT RUNWAY 18 PAPI AND RUNWAY 27 PAPI INCLUDING CABLING.

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

**AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
CLEAN AND SEAL JOINTS ON RUNWAY 15/33**

**SEQUENCE OF CONSTRUCTION
GENERAL NOTES**

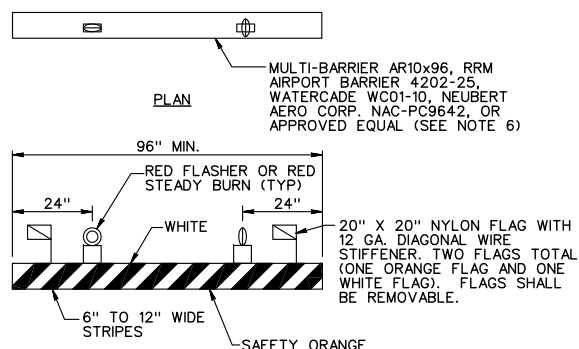
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DATE:	11/15/13
JOB No:	13285-02

ILLINOIS PROJECT: ARR-4285
S.B.C. PROJECT: 3-17-0003-95/99/XX

SHEET 4 OF 9 SHEETS

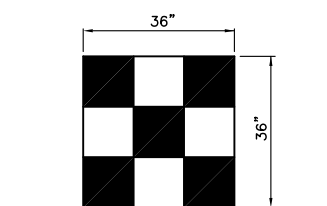


LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

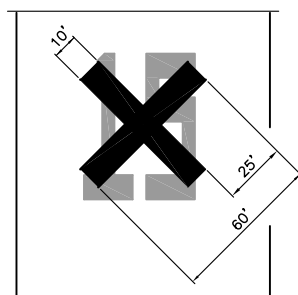
BARRICADE NOTES:

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 10' SPACING CENTER TO CENTER BETWEEN RED LIGHTS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN.



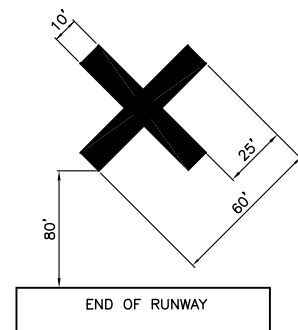
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE



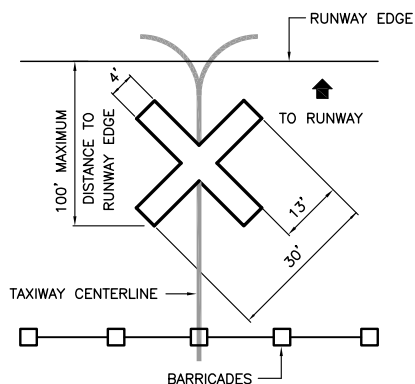
ON PAVEMENT CLOSED RUNWAY MARKER DETAIL

NOT TO SCALE



OFF PAVEMENT CLOSED RUNWAY MARKER DETAIL

NOT TO SCALE

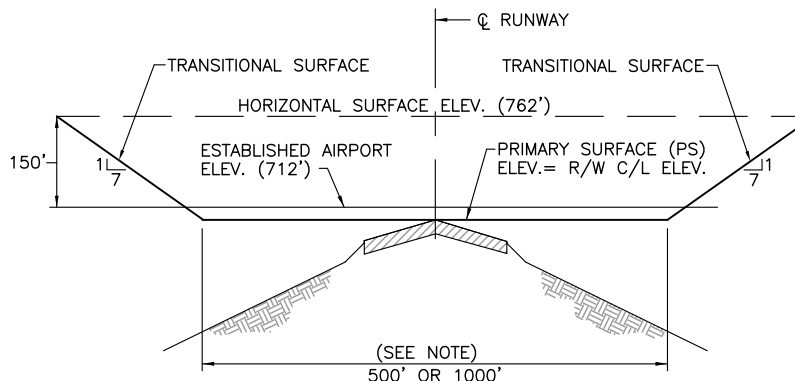


CLOSED TAXIWAY MARKER DETAIL

NOT TO SCALE

CLOSED TAXIWAY MARKER DETAIL NOTES

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION.
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.

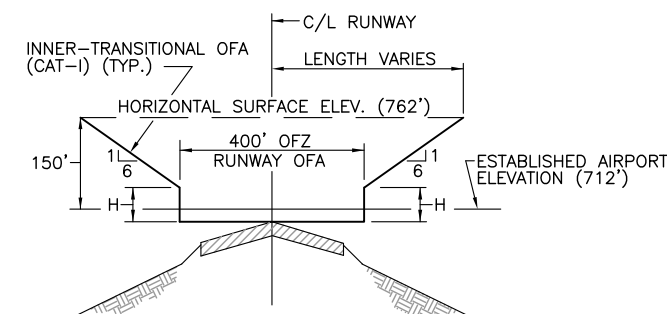


TYPICAL SECTION F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

NOTE:

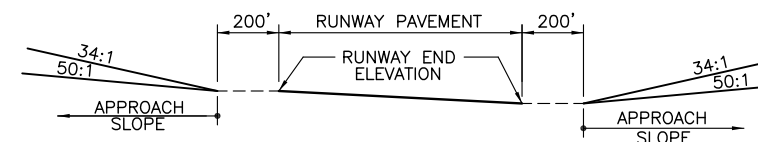
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY
 R/W 18-36 500' PS (250' LT & RT OF C)
 R/W 9-27,15/33 1000' PS (500' LT & RT OF C)



TYPICAL SECTION OBSTACLE FREE ZONE (OFZ)

NO SCALE

RUNWAY	TYPE OF RUNWAY	H (FEET)
9-27	PRECISION (CAT I)	43
15-33	VISUAL	150
18-36		



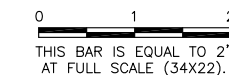
TYPICAL PROFILE F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
9	707	50:1
27	706	34:1
18	700	20:1
36	700	20:1
15	712	20:1
33	699	20:1

REVISIONS

NUMBER	BY	DATE



**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 CLEAN AND SEAL JOINTS ON RUNWAY 15/33**

SEQUENCE OF CONSTRUCTION DETAILS

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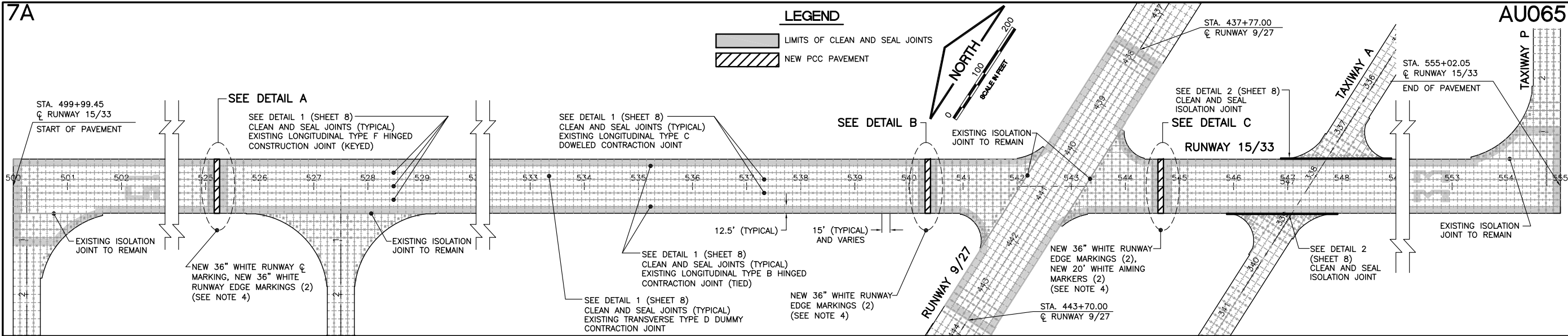
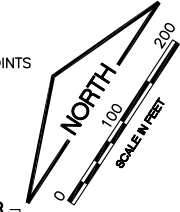
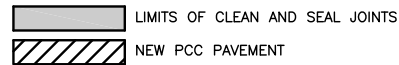
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CHICAGO AURORA MUNICIPAL AIRPORT

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CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	11/15/13
JOB No:	13285-02

ILLINOIS PROJECT: ARR-4285
 S.B.C. PROJECT: 3-17-0003-95/99/XX

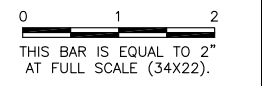
LEGEND



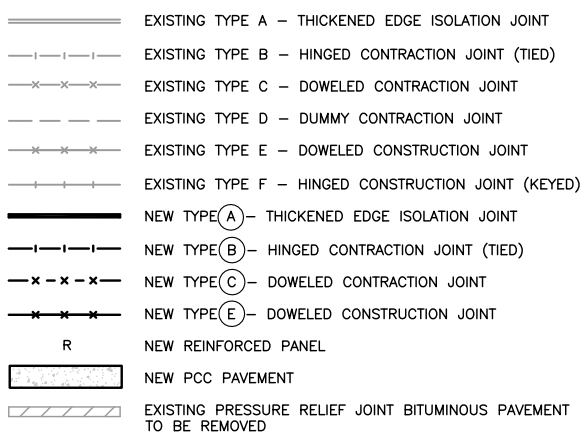
K:\Aurora\p\13285-02_Rwy15-33SealJoints\Draw\Sheets\C
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 LAYOUT: Layout1
 UPDATE BY: Adham Odeh
 SURVEY BOOK #
 DATE: Monday, November 25, 2013 9:46:47 AM
 XREF DWG: AAR-base.dwg
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 V-VF3D01_Rwy1533_Joints.d

REVISIONS

NUMBER	BY	DATE



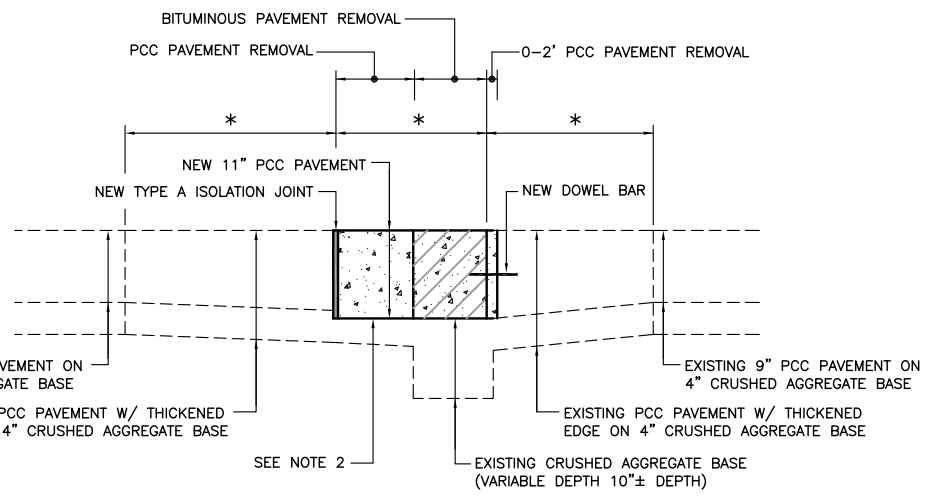
LEGEND



NOTES

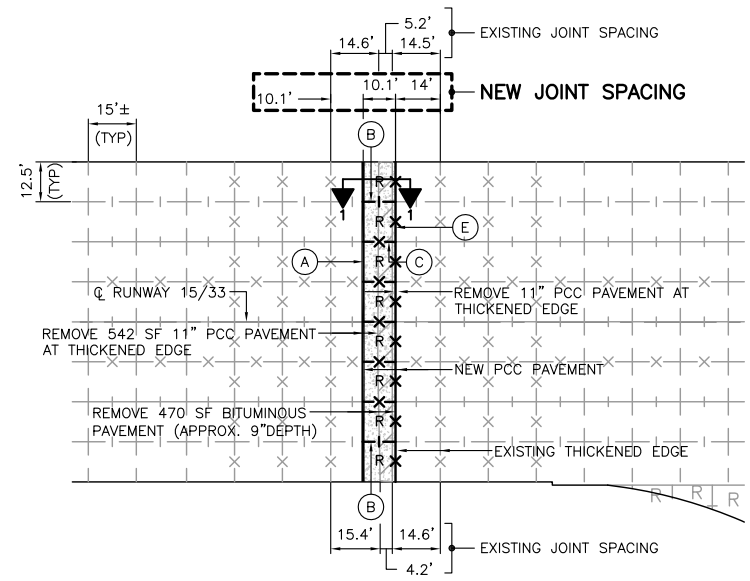
1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. THE CONTRACTOR SHALL REGRADE AND RECOMPACT EXISTING CRUSHED AGGREGATE BASE COURSE TO THE SATISFACTION OF THE RESIDENT ENGINEER. THIS WORK, ALONG WITH ANY CUT OR FILL OF THE AGGREGATE BASE SHALL BE INCIDENTAL TO THE CONTRACT.
3. EXACT LOCATION OF PCC SPALL REPAIR AND CLEAN AND SEAL CRACKS TO BE DETERMINED BY THE RESIDENT ENGINEER.
4. NEW PAVEMENT MARKINGS SHALL EXTEND 5' BEYOND THE NEW PCC PAVEMENT EDGE. RUNWAY PAVEMENT MARKINGS ARE WHITE WITH 6" BLACK BORDER.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

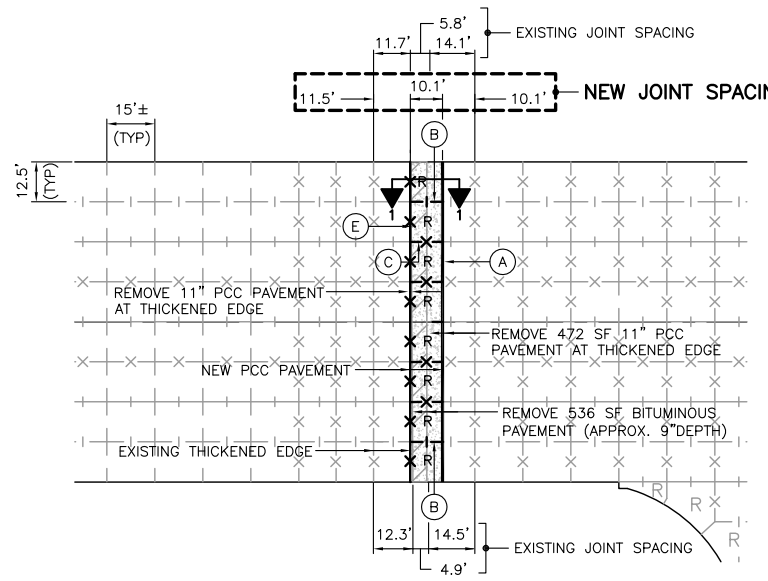


**NEW PCC PAVEMENT
TYPICAL SECTION 1-1**

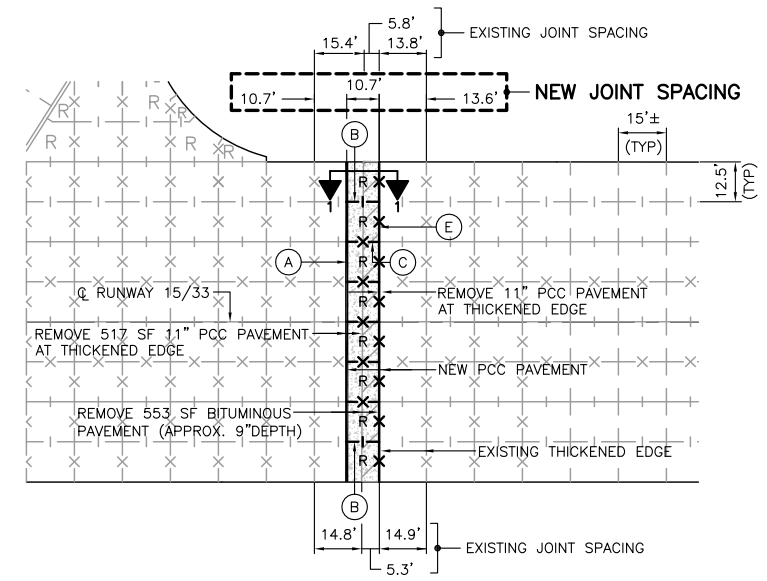
NOT TO SCALE
 * REFER TO DETAILS A, B AND C FOR EXACT NEW JOINT SPACINGS.



DETAIL A
NOT TO SCALE



DETAIL B
NOT TO SCALE



DETAIL C
NOT TO SCALE

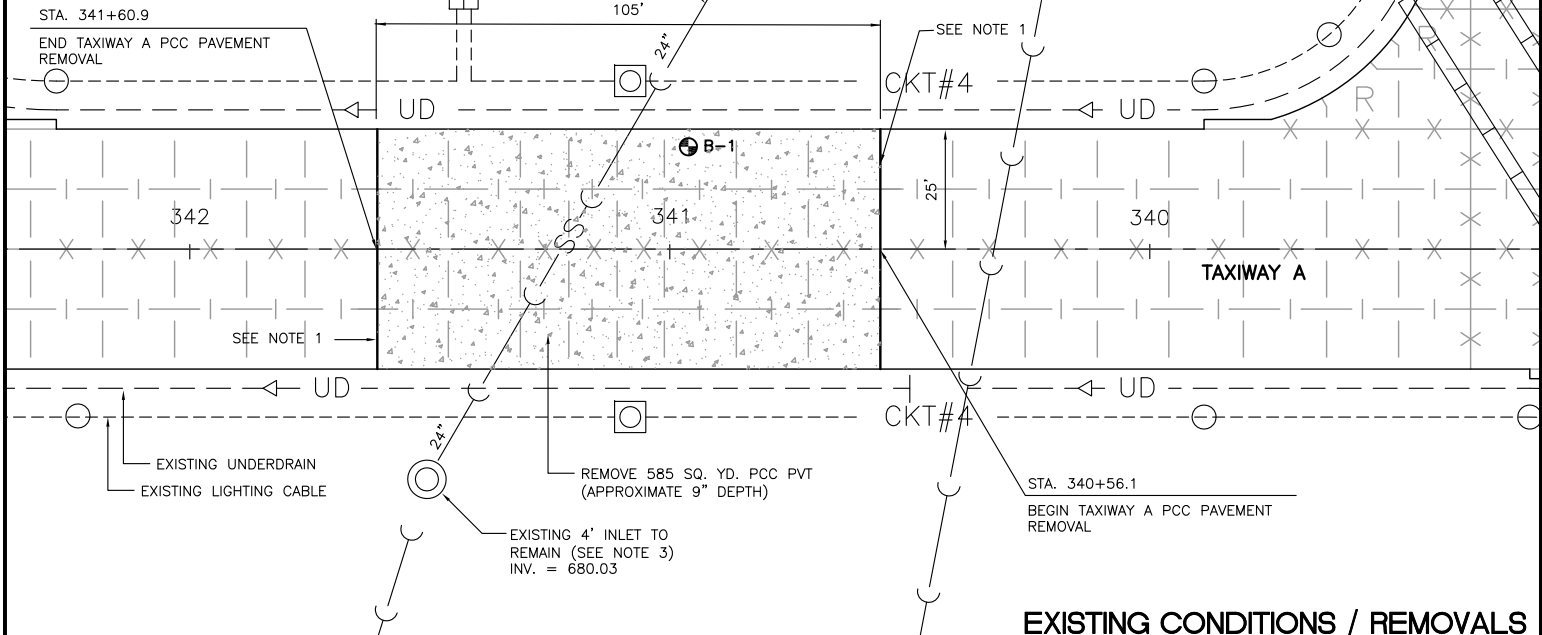
**AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 CLEAN AND SEAL JOINTS ON RUNWAY 15/33**

**JOINT AND PAVEMENT IMPROVEMENTS
 RUNWAY 15/33**

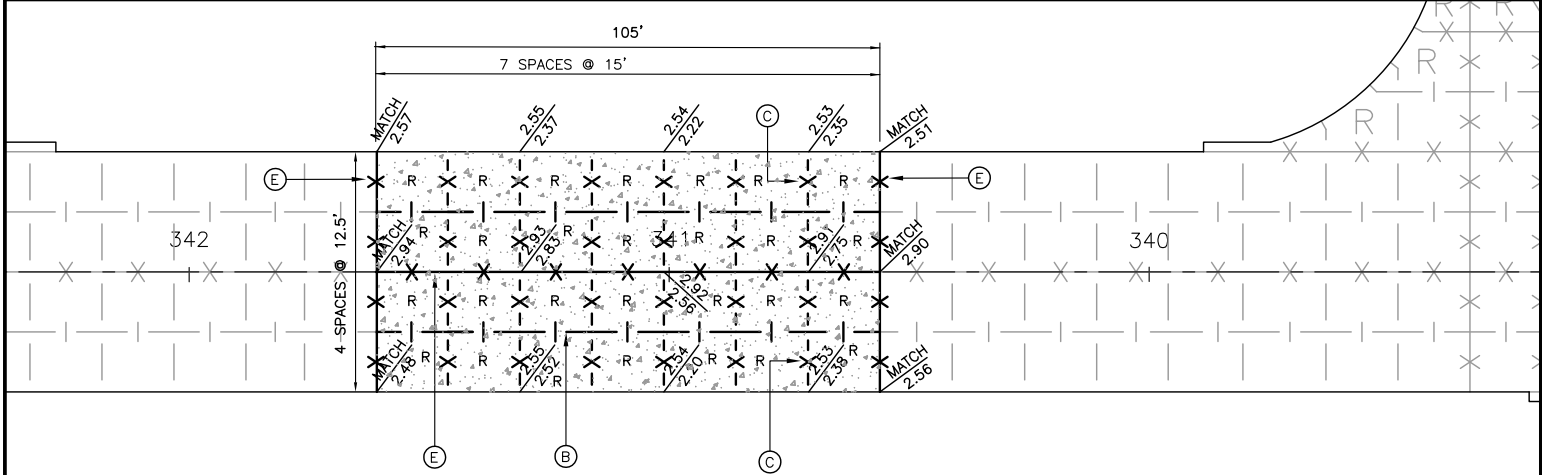
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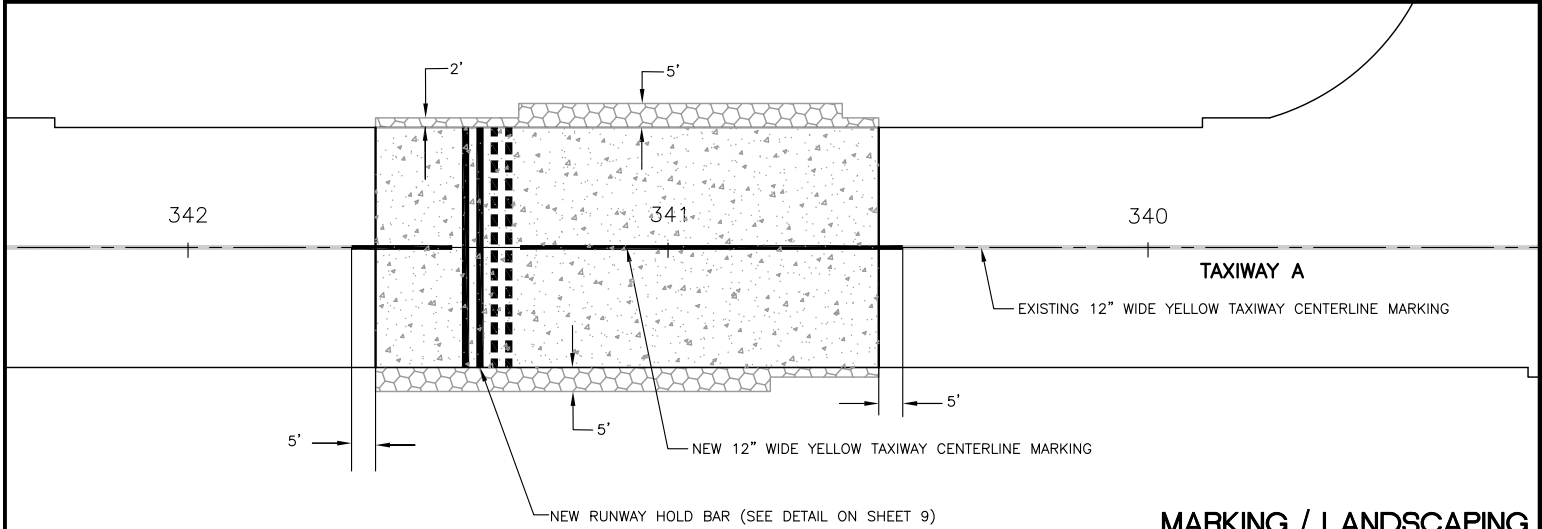
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APPROVED BY:	DKP
DATE:	11/15/13
JOB No:	13285-02
ILLINOIS PROJECT: ARR-4285	
S.B.C. PROJECT: 3-17-0003-95/99/XX	
SHEET	6 OF 9 SHEETS



EXISTING CONDITIONS / REMOVALS



JOINTING / GRADING



MARKING / LANDSCAPING

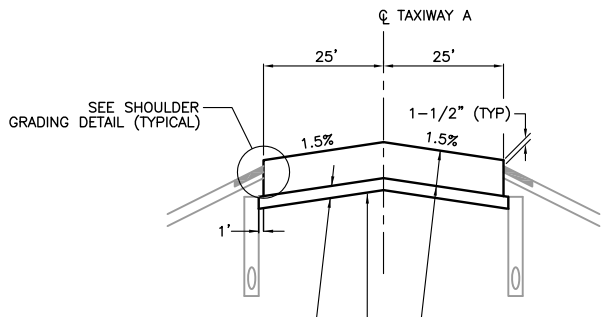
NOTES

1. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
2. SEE SHEET 9 FOR PAVEMENT BORING LOG.
3. CONTRACTOR TO TAKE MEASURES TO PROTECT EXISTING STORM SEWER PIPE, UNDERDRAIN AND DRAINAGE INLETS FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
4. LOCATIONS FOR FURNISHING AND PLACING POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. (REFER TO STABILIZATION TYPICAL SECTION)
5. CONTRACTOR SHALL, IF NEEDED, PLACE EROSION CONTROL MATERIAL TO THE SATISFACTION OF THE ENGINEER. ANY EROSION CONTROL MEASURES TAKEN SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. ALL EXCESS UNCLASSIFIED EXCAVATION MATERIAL SHALL BE STOCKPILED ON AIRPORT PROPERTY.

LEGEND

- EXISTING TYPE A - THICKENED EDGE ISOLATION JOINT
- - - EXISTING TYPE B - HINGED CONTRACTION JOINT (TIED)
- x - x - x EXISTING TYPE C - DOWELED CONTRACTION JOINT
- - - EXISTING TYPE D - DUMMY CONTRACTION JOINT
- x - x - x EXISTING TYPE E - DOWELED CONSTRUCTION JOINT
- - - EXISTING TYPE F - HINGED CONSTRUCTION JOINT (KEYED)
- NEW TYPE (A) - THICKENED EDGE ISOLATION JOINT
- - - NEW TYPE (B) - HINGED CONTRACTION JOINT (TIED)
- x - x - x NEW TYPE (C) - DOWELED CONTRACTION JOINT
- x - x - x NEW TYPE (E) - DOWELED CONSTRUCTION JOINT
- R NEW REINFORCED PANEL
- [Pattern] NEW PCC PAVEMENT
- [Pattern] NEW TOPSOILING, SEEDING AND MULCHING
- 2.63 / 2.63 NEW ELEVATION (2.63)
- 2.63 / 2.63 EXISTING ELEVATION (2.63)
- (ADD 690.00 FOR ELEVATIONS FOR USGS DATUM)
- B-1 PAVEMENT BORING

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

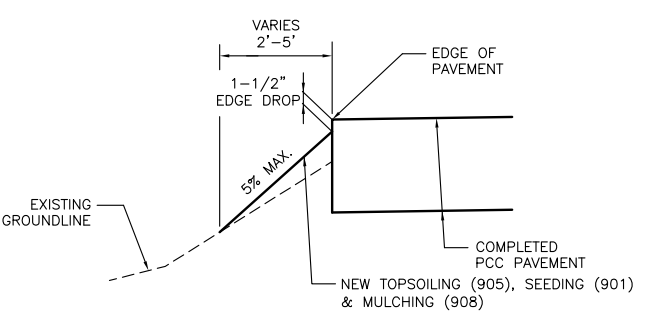


NEW 4" CRUSHED AGGREGATE BASE COURSE (209) NEW 9" PCC CONCRETE PAVEMENT (501)

CONTRACTOR SHALL REGRADE, RECOMPACT AND PROOF ROLL EXISTING SUBGRADE TO THE SATISFACTION OF THE RESIDENT ENGINEER (COST INCIDENTAL)

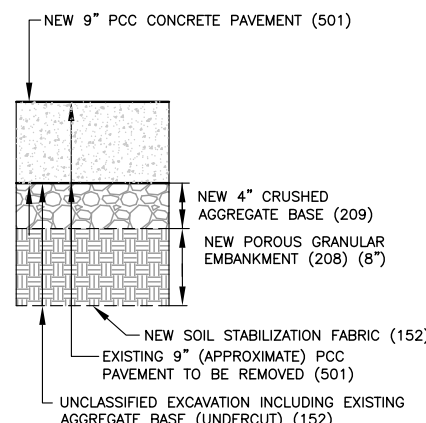
TYPICAL SECTION

NOT TO SCALE



SHOULDER GRADING DETAIL

NOT TO SCALE



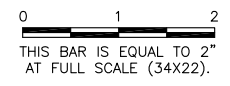
STABILIZATION TYPICAL SECTION

NOT TO SCALE (SEE NOTE 4)

K:\Aurora\p\13285-02_Rwy15-33SealJoints\Draw\Sheets\C FILE: 07_proposed Improvements - Taxi LAYOUT: Layout1 UPDATE BY: Adham Odeh SURVEY BOOK # DATE: Monday, November 25, 2013 9:47:07 AM XREF DWG: AAR-base.dwg tbcint.dwg V-VF3D01_Rwy1533_Joints.d

REVISIONS

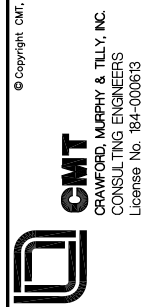
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AURORA MUNICIPAL AIRPORT
 AURORA, ILLINOIS
 CLEAN AND SEAL JOINTS ON RUNWAY 15/33

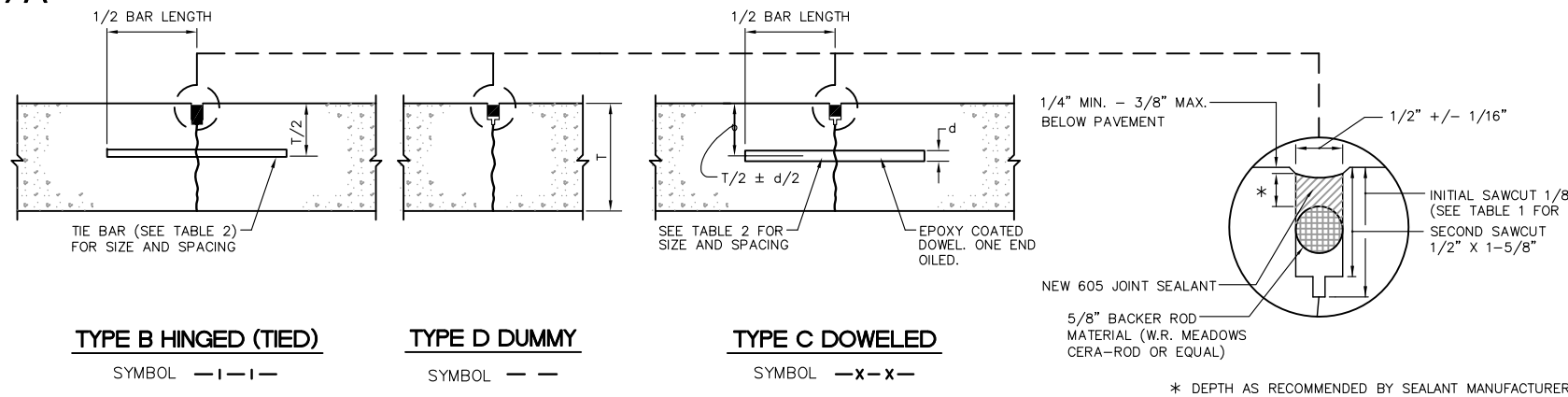
JOINT AND PAVEMENT IMPROVEMENTS
 TAXIWAY ALPHA

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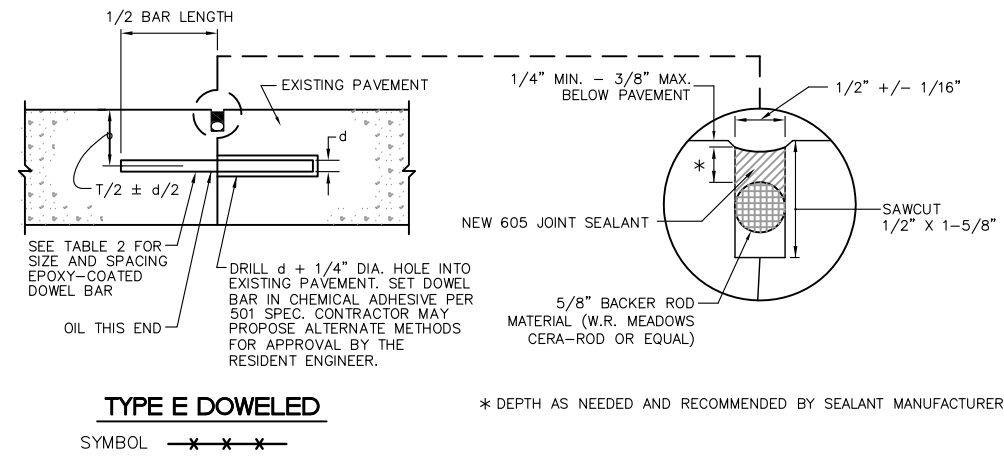


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CHECKED BY:	AAO
APPROVED BY:	DKP
DATE:	11/15/13
JOB No:	13285-02
ILLINOIS PROJECT:	ARR-4285
S.B.C. PROJECT:	3-17-0003-95/99/XX
SHEET	7 OF 9 SHEETS

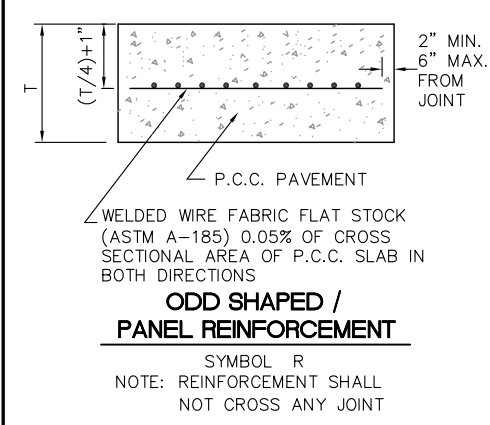
7A



CONTRACTION JOINTS - NEW PAVEMENT



CONSTRUCTION JOINTS - NEW PAVEMENT



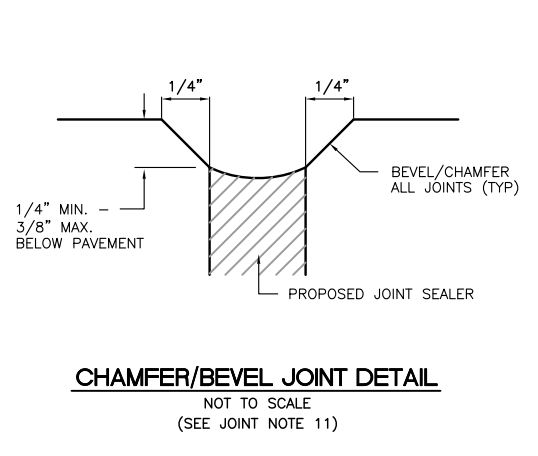
ODD SHAPED / PANEL REINFORCEMENT

- JOINT NOTES** **AU065**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
 - THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
 - ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
 - ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
 - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OT ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
 - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
 - JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
 - COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
 - SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
 - DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
 - CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS (BOTH NEW JOINTS AND EXISTING JOINTS) PER THE DETAIL ON THIS SHEET AT NO ADDITIONAL COST. CHAMFERS ARE NOT NEEDED WHERE CHAMFER IS NOT CONSTRUCTIBLE AT SMALLER SPALLING AREAS.

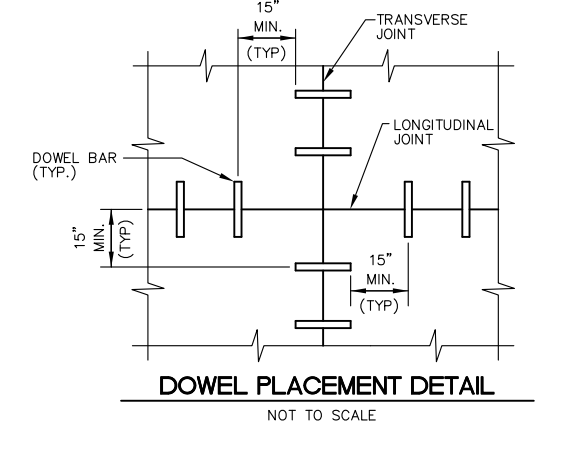
FILE: 08_jointing_Details.dwg
 LAYOUT: 11 PAVEMENT JOINTING DETAILS
 UPDATE BY: Adham Odeh
 SURVEY BOOK #
 DATE: Monday, November 25, 2013 9:47:19 AM
 XREF DWG: tbcint.dwg

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



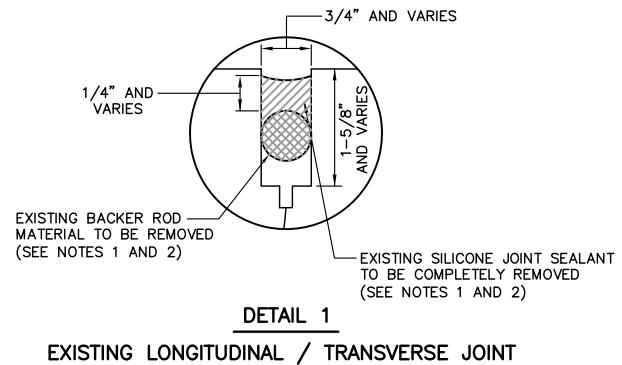
CHAMFER/BEVEL JOINT DETAIL



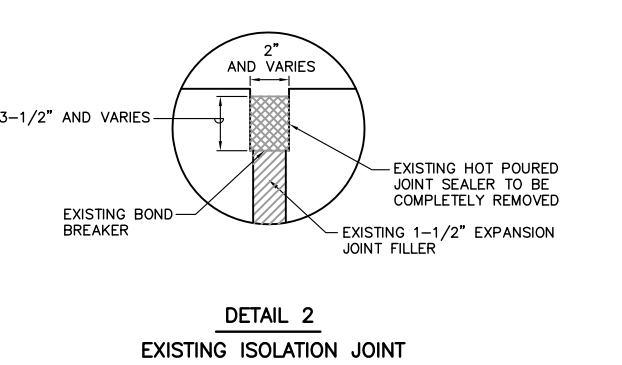
DOWEL PLACEMENT DETAIL

NOTES

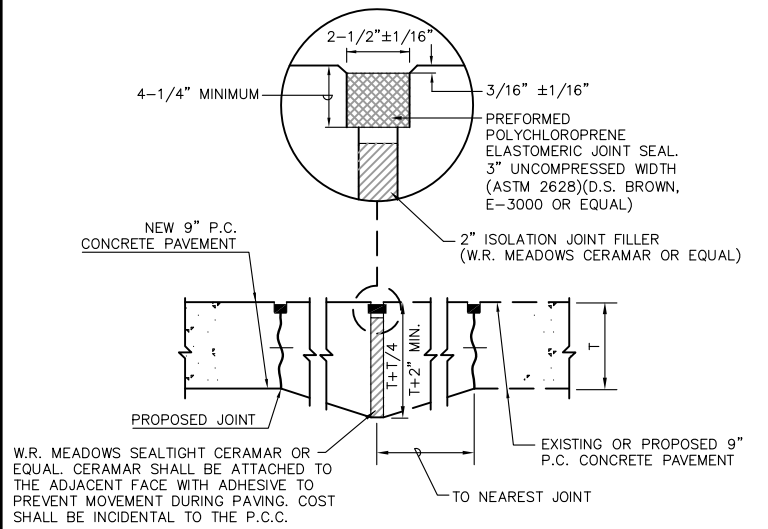
- THE CONTRACTOR SHALL VISIT THE PROJECT SITE AND ACQUAINT HIMSELF WITH THE REQUIRED WORK. CONTRACTOR SHALL BE FAMILIAR TO THE SITE AND REQUIRED WORK PRIOR TO BID. SITE VISITS SHALL BE COORDINATED WITH THE AIRPORT DIRECTOR AT (630) 466-7000.
- PRIOR TO ORDERING NEW MATERIAL, CONTRACTOR SHALL FIELD MEASURE FOR JOINTING MATERIAL SIZES AND AMOUNT. MEASUREMENTS SHALL ALSO DETERMINE BACKER ROD SIZE AND QUANTITIES. ALSO PER NEW JOINTS DETAILS, DEPTH OF NEW JOINT SEALANT SHALL BE AS RECOMMENDED BY MANUFACTURER. NO EXTRA COMPENSATION WILL BE MADE FOR EXCESS MATERIALS NOT USED.
- FOR VARYING JOINT WIDTHS AND DEPTHS, THERE SHALL BE NO ADDITIONAL COST FOR DEEPER SAWCUTS, LARGER BACKER RODS AND SEALANT MATERIAL USED.



DETAIL 1
EXISTING LONGITUDINAL / TRANSVERSE JOINT



DETAIL 2
EXISTING ISOLATION JOINT

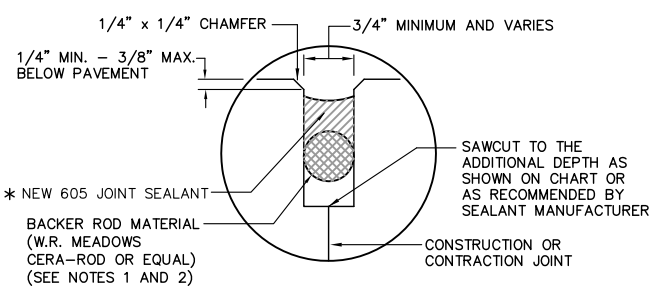


TYPE A ISOLATION
SYMBOL

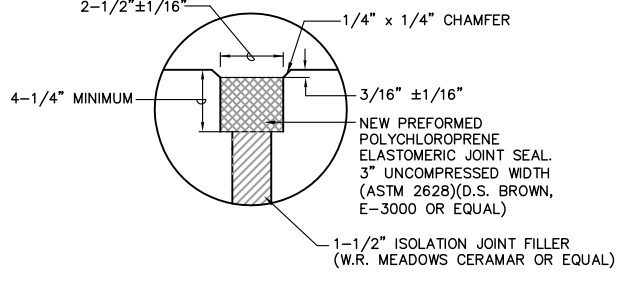
CLEAN AND SEAL JOINT CHART

EXISTING JOINT WIDTH	SUGGESTED BACKER ROD DIAMETER (SEE NOTE A)	SUGGESTED JOINT TOTAL DEPTH
1/2"	5/8"	1"-5/8"
5/8"	3/4"	1"-7/8"
3/4"	7/8"	2"-1/8"
7/8"	1"	2"-1/4"
1"	1"-1/4"	2"-7/8"
1"-1/8"	1"-1/2"	3"-3/8"
1"-1/4"	1"-1/2"	3"-1/2"

NOTE A: IF LARGER SIZE IS USE, CONTRACTOR SHALL INCREASE SAW DEPTH (JOINT TOTAL DEPTH) AS REQUIRED AT NO ADDITIONAL COST TO THE CONTRACT.



CLEAN AND SEAL LONGITUDINAL / TRANSVERSE JOINTS



CLEAN AND SEAL ISOLATION JOINT

ISOLATION JOINTS

TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ±1/4"
9	2.25"
11	2.75"

TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
9	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"

DIMENSION TABLES

CLEAN AND SEAL JOINT DETAILS

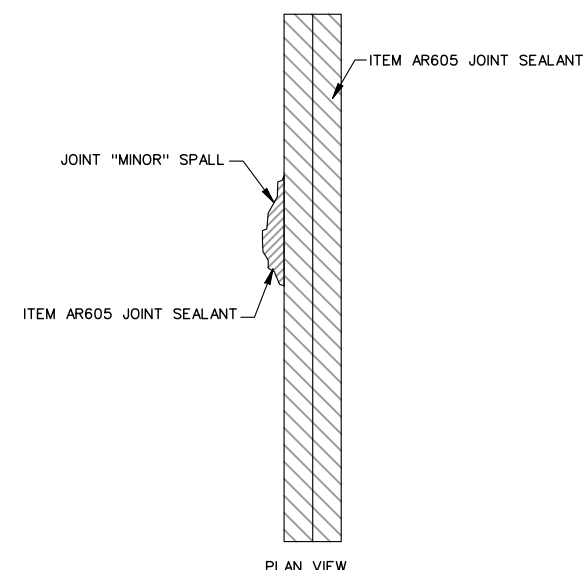
AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
CLEAN AND SEAL JOINTS ON RUNWAY 15/33
PAVEMENT JOINTING DETAILS

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AURORA
 MUNICIPAL AIRPORT

DESIGN BY: DKP
 DRAWN BY: JRO
 CHECKED BY: AAO
 APPROVED BY: DKP
 DATE: 11/15/13
 JOB No: 13285-02
 ILLINOIS PROJECT: ARR-4285
 S.B.C. PROJECT: 3-17-0003-95/99/XX
 SHEET 8 OF 9 SHEETS

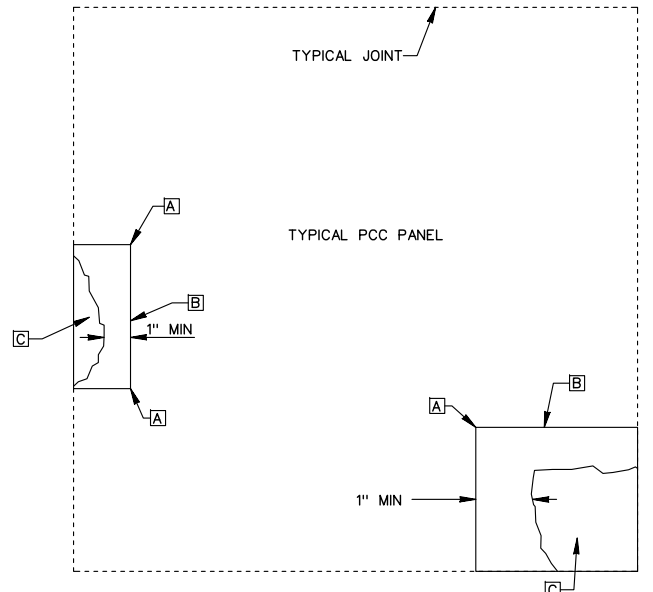
LOG OF BORING NO. B-1			
CLIENT		BORING LOCATION	
Crawford, Murphy, and Tilly, Inc. 550 N. Commons Drive, Suite 116, Aurora, IL 60504		Station: 340+97.1 Offset: 20' Right	
PROJECT LOCATION		PROJECT DESCRIPTION	
Taxiway Alpha- Aurora Municipal Airport Sugar Grove, Illinois		Geotechnical Investigation Settled Slab at Taxiway Alpha	
DEPTH (ft.) BELOW GROUND SURFACE	SAMPLE NUMBER	DESCRIPTION OF MATERIAL	UNIT DRY WT. LBS./FT.
			UNCONFINED COMPRESSIVE STRENGTH TONS/FT. ²
			CALIBRATED PENETROMETER TONS/FT. ²
			WATER CONTENT %
			STANDARD "N" PENETRATION (BLOWS/FT.)
		GROUND SURFACE ELEVATION 692.2	
0	AS	9" CONCRETE with 4" Crushed Stone Base Course	
1	SS	CRUSHED ASPHALT (Fill)	
2	SS	SILT-brown-medium dense (ML) (Fill)	
3	ST	SAND & GRAVEL-brown-very dense (Fill)	
4	SS		
5.0	SS		
6	SS	CRUSHED ASPHALT-black-very dense (Fill)	
7	SS		
10.0			
8	ST	ORGANIC SILT- dark brown to black-soft (OH), Organic Content= 19.3%	87.0%
9	ST	SILTY CLAY- black and gray-stiff (CL)	
15.0			
10	SS	POORLY GRADED SAND & GRAVEL-gray-medium dense (GP)	
11	SS		
12	SS		
20.0			
END OF BORING			
WATER LEVEL OBSERVATIONS		BORING STARTED April 3, 2013	
Water Level While Drilling 11"		BORING COMPLETED April 3, 2013	
Water Level After Boring 11"		RIG CME B57 FOREMAN JK	
		DRAWN RR APPROVED SB	
		GSI JOB No. 13026 SHEET 1 OF 1	

Geo Services, Inc.
 Geotechnical, Environmental & Civil Engineering
 805 Antwerp Court, Suite 204
 Naperville, Illinois 60563
 (630) 353-2838



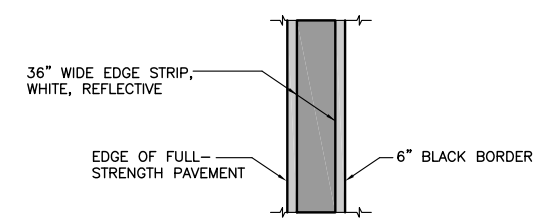
MINOR SPALL REPAIR DETAIL
 NOT TO SCALE

NOTE: FOR MINOR SPALLS, CONTRACTOR SHALL FILL IN WITH JOINT SEALANT AT NO ADDITIONAL COST TO THE CONTRACT.

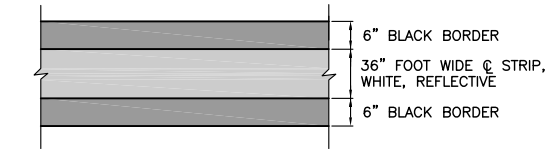


PLAN VIEW SPALL REPAIR DETAIL
 NOT TO SCALE

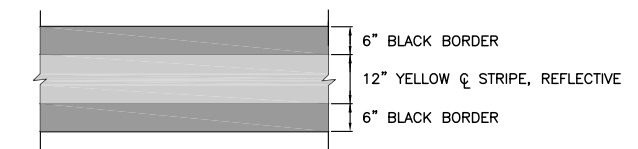
- SPALL REPAIR NOTES:**
- A** CONTRACTOR SHALL NOT OVERSAW.
 - B** CONTRACTOR SHALL SAWCUT 2" DEPTH EXCEPT AS NECESSARY TO NOT OVERSAW.
 - C** CONTRACTOR SHALL REMOVE EXISTING PCC DOWN TO UNDISTURBED MATERIAL. VOID SHALL BE FILLED WITH ELASTOMERIC CONCRETE AS SPECIFIED IN ITEM AR501 FOR SPALL REPAIR.



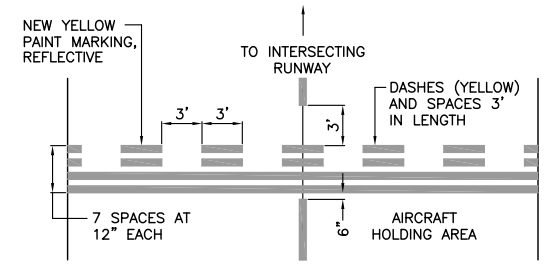
RUNWAY EDGE MARKINGS CONTINUOUS
 NOT TO SCALE



RUNWAY CENTERLINE DETAIL
 NOT TO SCALE



TAXIWAY CENTERLINE ON PCC DETAIL
 NOT TO SCALE



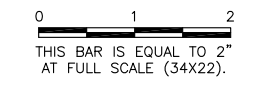
RUNWAY HOLDING POSITION MARKER
 NOT TO SCALE

NOTE: ALL HOLDING POSITION MARKINGS SHALL HAVE 6" WIDE BLACK BORDER.

PAVEMENT MARKING NOTES

1. CURING COMPOUND ON CONCRETE PAVEMENTS SHALL BE REMOVED BY WATER BLASTING OR OTHER METHODS APPROVED BY THE ENGINEER PRIOR TO MARKING.
2. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
3. WHEN YELLOW TAXIWAY STRIPPING CROSSES ANY WHITE RUNWAY STRIPPING, THE TAXIWAY STRIPE SHALL BE GAPPED 6" EACH SIDE OF RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINE.
4. ALL MARKINGS ON PCC PAVEMENTS SHALL HAVE A 6" BLACK BORDER.

REVISIONS		
NUMBER	BY	DATE



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS
CLEAN AND SEAL JOINTS ON RUNWAY 15/33

MISCELLANEOUS DETAILS

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AURORA
 MUNICIPAL AIRPORT

DESIGN BY:	DKP
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	11/15/13
JOB No:	13285-02
ILLINOIS PROJECT: ARR-4285	
S.B.C. PROJECT: 3-17-0003-95/99/XX	
SHEET	9 OF 9 SHEETS