

LETTING ITEM NO.  
JANUARY 17, 2025 LETTING

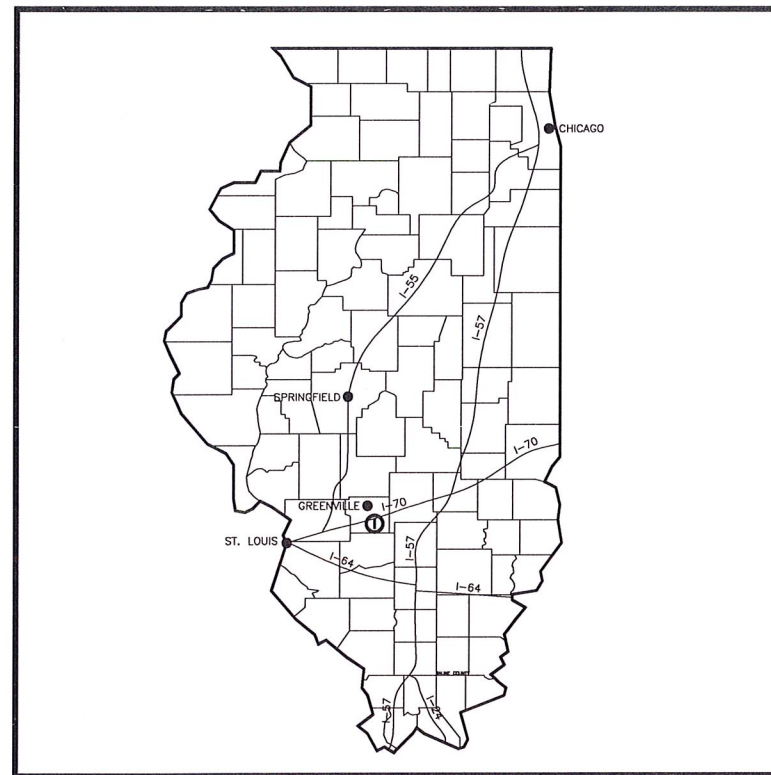
# CONSTRUCTION PLANS FOR GREENVILLE AIRPORT AIRCRAFT T-HANGAR APRON EXPANSION

GR013  
TOTAL SHEETS: 11

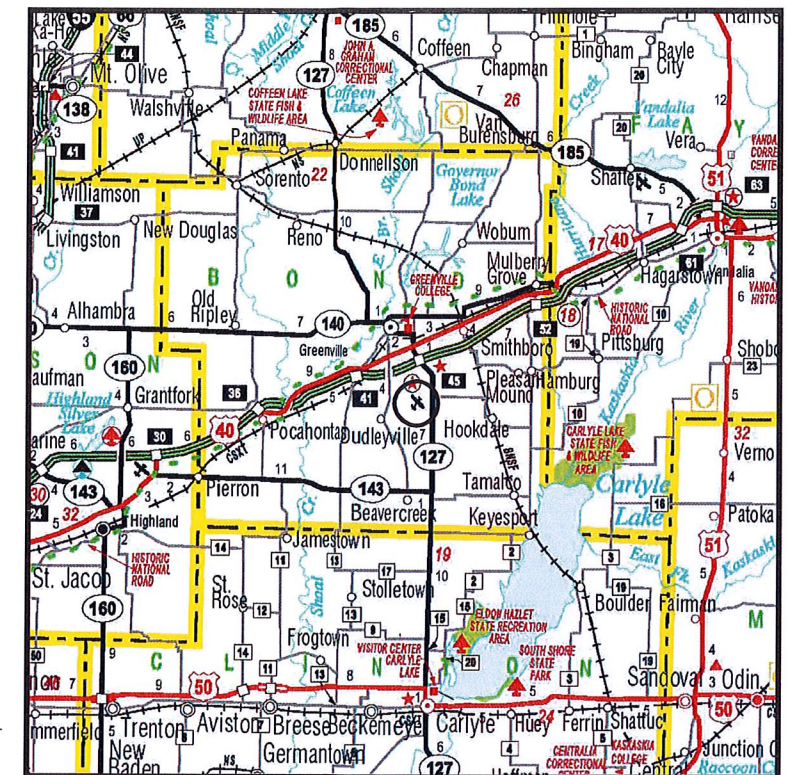
ILLINOIS PROJECT NUMBER: GRE-4992  
REBUILD ILLINOIS AIRPORT CAPITAL  
INVESTMENT PROGRAM

100% SUBMITTAL - NOVEMBER 22, 2024

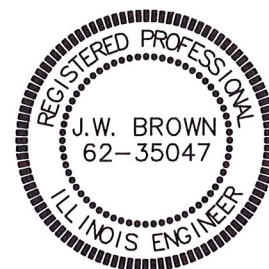
GREENVILLE, ILLINOIS  
BOND COUNTY



LOCATION MAP



VICINITY MAP



**DESIGN INFORMATION**  
- CRITICAL AIRCRAFT = CESSNA CITATION III  
- AIRCRAFT APPROACH CATEGORY (AAC) = B  
- AIRPLANE DESIGN GROUP (ADG) = II  
- TAXIWAY DESIGN GROUP (TDG) = 2  
- DEPARTURE WEIGHT = 22,000 LBS.

BROWN AND ROBERTS, INC.  
CONSULTING ENGINEER  
PRESIDENT  
SUBMITTED BY: *Jim W. Brown*  
JIM W. BROWN  
DATE SUBMITTED: *11/22/24*  
LICENSE NUMBER: 184-002518  
LICENSE EXPIRATION DATE: APRIL 30, 2025

PLANS PREPARED BY:  
  
BROWN AND ROBERTS, INC.  
1 WESTRIDGE ROAD  
HARRISBURG, IL. 62946  
(618) 252-8111

GREENVILLE AIRPORT AUTHORITY  
CHAIRMAN  
APPROVED BY: *Randall Vaal* 11-19-24  
DATE  
SECRETARY  
ATTESTED BY: *Paul A. Jackson* 11/22/24  
DATE

25 Nov 2024 - 7:32am X:\2023\23052\REBID JANUARY 2025 - October Revision\acplans\23052 Cover and Quantity Sheets.dwg: Layout Tab 'Cover'

**GREENVILLE AIRPORT**

IL PROJECT NO. GRE-4992

**REBUILD ILLINOIS****SUMMARY OF QUANTITIES  
& INDEX OF SHEETS**

SHEET 2 OF 11

GR013

**SUMMARY OF QUANTITIES**

ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY
AR150520	MOBILIZATION	L SUM	1
AR152410	UNCLASSIFIED EXCAVATION	CU YD	475
AR152511	SUBGRADE REPAIR	SQ YD	50
AR156510	SILT FENCE	FOOT	260
AR156514	DITCH CHECK	FOOT	14
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	450
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	650
AR501506	6" PCC PAVEMENT	SQ YD	1875
AR501530	PCC TEST BATCH	EACH	1
AR705943	ADJUST UNDERDRAIN INSP. HOLE	EACH	1
AR901525	SEEDING	L SUM	1

**INDEX OF SHEETS**

SHEET NUMBER	DESCRIPTION
1	COVER SHEET
2	SUMMARY OF QUANTITIES & INDEX OF SHEETS
3	CONSTRUCTION SAFETY & PHASING PLAN
4	TYPICAL SECTIONS
5	REMOVAL PLAN
6	APRON PAVING PLAN
7	JOINT PLAN
8	JOINT DETAILS
9	APRON CROSS SECTIONS 1
10	APRON CROSS SECTIONS 2
11	APRON CROSS SECTIONS 3

**SCOPE OF WORK**

THE PROJECT SCOPE CONSISTS OF THE EXPANSION OF THE EXISTING T-HANGAR APRON AREA.

**PROPOSED SAFETY PLAN**

GENERAL- THE GREENVILLE AIRPORT CURRENTLY HAS A TURF RUNWAY 9-27 WHICH IS 2822 FT. x 250 FT. AND A PAVED RUNWAY 18-36 WHICH IS 4001 FT. x 75 FT.

IT IS ANTICIPATED THAT NO CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT WILL IMPACT ANY AIRCRAFT OPERATIONS ON THE AIRPORT RUNWAYS.

**ANY WORK WITHIN 125' OF THE CENTERLINE OF A RUNWAY WILL REQUIRE CLOSURE OF THAT RUNWAY.**

THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) CONFIRMING COMPLIANCE WITH THE CONSTRUCTION SAFETY PHASING PLAN (CSPP) PRIOR TO THE ISSUANCE OF THE NOTICE TO PROCEED AS SPECIFIED IN FAA AC 150/5370-2.

**CONTRACTOR'S RESPONSIBILITIES**

IDENTIFICATION- THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE PROPERLY MARKED WITH 3-FOOT SQUARE INTERNATIONAL ORANGE AND WHITE CHECKERED FLAGS ANYTIME THEY ARE ON AIRPORT PROPERTY.

THE CONTRACTOR AND HIS EMPLOYEES SHALL BE RESTRICTED TO THE WORK AREA.

EQUIPMENT PARKING AND STORAGE- THE CONTRACTOR'S EQUIPMENT PARKING, STORAGE, AND EMPLOYEE PARKING WILL BE AT THE LOCATION SHOWN ON THIS SHEET. ONLY CONTRACTOR VEHICLES AND EQUIPMENT REQUIRED FOR CONSTRUCTION WILL BE ALLOWED OUTSIDE THIS AREA.

BARRICADES AND TRAFFIC CONES- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AND TRAFFIC CONES AS REQUIRED AND AS DIRECTED BY THE RESIDENT ENGINEER. BARRICADES, THEIR MAINTENANCE, PLACEMENT, AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

THE CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING PAVEMENTS CAUSED BY HIS PERSONNEL OR EQUIPMENT.

**ACCESS/HAUL ROUTE AND EQUIPMENT PARKING**

THE CONTRACTOR WILL USE THE DESIGNATED ACCESS/HAUL ROUTE AND EQUIPMENT PARKING AREA SHOWN ON THIS SAFETY PLAN. THE PROPOSED EQUIPMENT PARKING AREA WILL BE APPROXIMATELY 70-FT BY 150-FT. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED ACCESS/HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. AT THE CONCLUSION OF THE PROJECT, ALL AREAS DISTURBED WILL BE RESTORED AS NEEDED TO ITS ORIGINAL STATE. RESTORATION OF THE ACCESS/HAUL ROUTE AND EQUIPMENT PARKING AREA WILL BE CONSIDERED INCLUDED WITH THE AR150540 HAUL ROUTE PAY ITEM AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**UTILITY NOTE**

THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES AND ORGANIZATIONS THAT HAVE LINES OR CONDUITS IN THE PROPOSED WORK AREA. ALL LINES AND CONDUITS SHALL BE LOCATED AND IDENTIFIED FOR DEPTH BEFORE ANY EXCAVATION BEGINS. THE CONTRACTOR SHALL CALL JULIE (1-800-892-0123) TO ACCOMPLISH THESE REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING ALL NON-JULIE UTILITIES LOCATED WITHIN THE PROPOSED CONSTRUCTION LIMITS. THESE UTILITIES ARE TO BE LOCATED PRIOR TO THE START OF CONSTRUCTION.

**J.U.L.I.E. INFORMATION**

COUNTY.....BOND  
 CITY.....GREENVILLE (4 MI SOUTHEAST)  
 TOWNSHIP.....CENTRAL  
 SECTION NO.....36  
 NEAREST MAJOR ROAD INTERSECTION...ILLINOIS RT. 127 & SKY LANE  
 AIRPORT ADDRESS...GREENVILLE AIRPORT  
 1374 SKY LANE  
 GREENVILLE, IL 62246

THE AIRPORT MANAGER WILL BE RESPONSIBLE FOR RELOCATING AIRCRAFT FROM HANGARS WITHIN THE BARRICADED AREA TO THE AIRCRAFT PARKING APRON PRIOR TO PLACEMENT OF THE BARRICADES AND PRIOR TO CONSTRUCTION OPERATIONS.

**HEIGHT OF CONSTRUCTION EQUIPMENT**

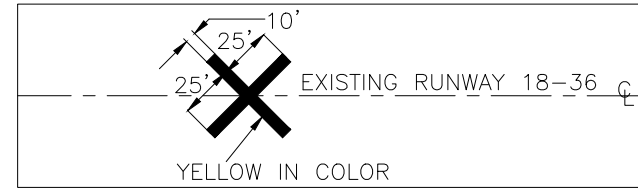
THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 20 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A SEMI TRAILER IN THE UP POSITION.

**AIRPORT SECURITY**

AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE PROPOSED HAUL ROUTE SHOWN ON THIS SAFETY PLAN IS THE ONLY ACCESS CONTRACTOR EQUIPMENT AND PERSONNEL WILL BE ALLOWED TO USE. THE CONTRACTOR SHALL PROVIDE BARRICADES AT THIS ACCESS AND ENSURE THE BARRICADES ARE IN PLACE AT THE END OF EACH WORKING DAY.

**AIRCRAFT OPERATIONAL AREA**

THE CONTRACTOR, HIS EMPLOYEES, OR ANY EQUIPMENT WILL NOT PROCEED WITH ANY WORK WITHIN THE AIRCRAFT OPERATIONAL AREA WITHOUT FIRST CLOSING THE RUNWAY.



**NOTE:**

THE COST OF CONSTRUCTING, PLACING, MAINTAINING, AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE RESIDENT ENGINEER. THE CROSSES WILL BE PLACED AT THE ENDS OF THE RUNWAY AND SECURED IN A MANNER APPROVED BY THE RESIDENT ENGINEER. THE PROPOSED CROSSES WILL BE PLACED WHEN THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES AT NO ADDITIONAL COST TO THE CONTRACT.

**RUNWAY CLOSURE PROCEDURES:**

- \* CONTACT THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- \* ISSUANCE OF NOTAM BY THE AIRPORT MANAGER OR HIS ASSIGNED REPRESENTATIVE.
- \* PLACEMENT OF CROSSES (SEE DETAIL THIS SHEET).
- \* PLACEMENT OF LIGHTED BARRICADES. ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS WITHIN 200-FT OF THE AFFECTED RUNWAY CENTERLINE AND WITHIN 600 FT OF THE RUNWAY END BEGIN.
- \* RUNWAY LIGHTS SHALL BE DISABLED

**RUNWAY RE-OPENING PROCEDURES:**

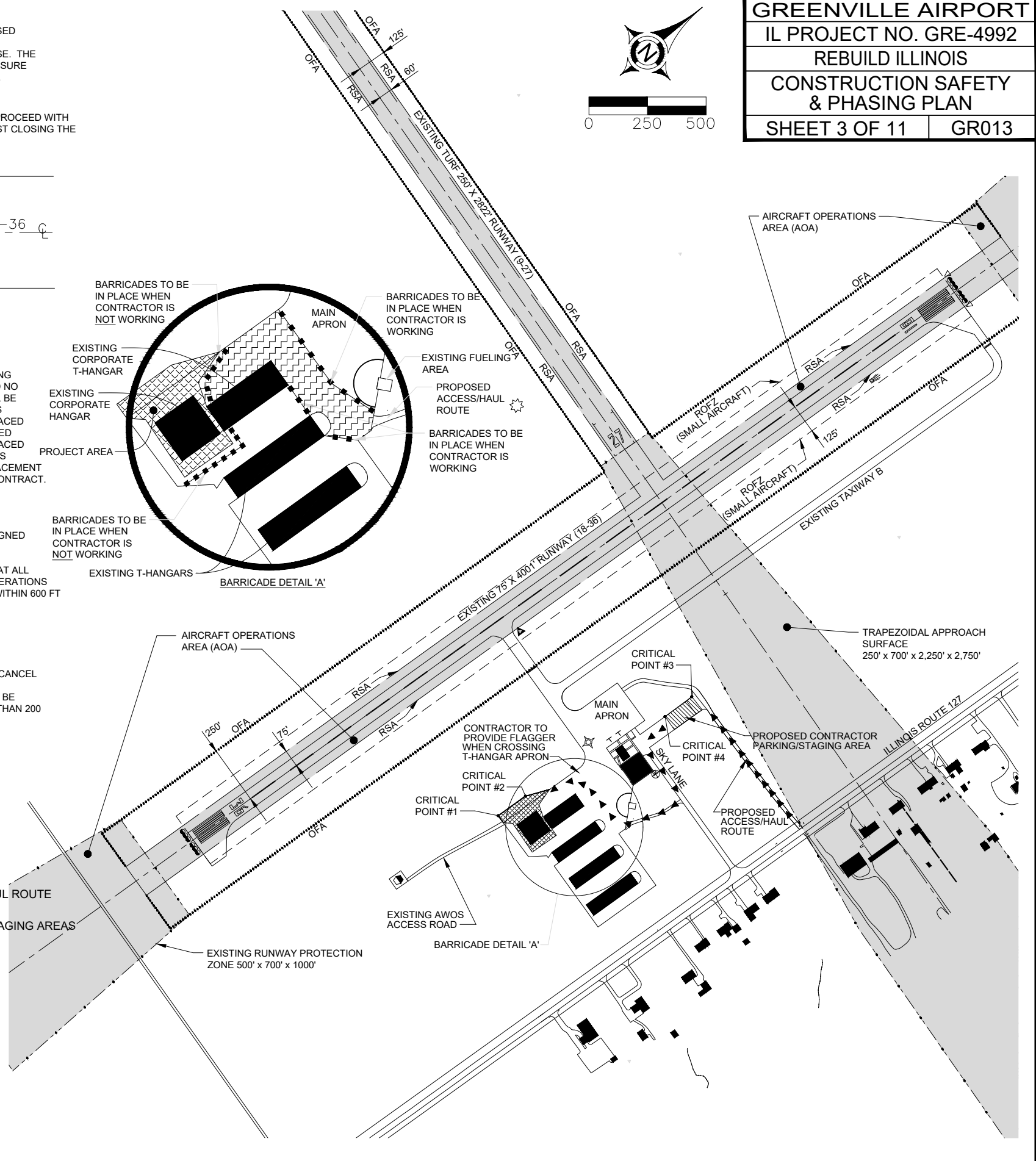
- \* REMOVE CROSSES.
- \* REMOVE LIGHTED BARRICADES.
- \* NOTIFY THE AIRPORT MANAGER OR HIS REPRESENTATIVE TO CANCEL THE NOTAM.
- \* CANCELLATION OF THE NOTAM. A CLOSED RUNWAY WILL NOT BE RE-OPENED UNTIL ALL EQUIPMENT AND WORK ARE FURTHER THAN 200 FEET FROM THE AFFECTED RUNWAY CENTERLINE.
- \* RUNWAY LIGHTS SHALL BE REACTIVATED.

**LEGEND**

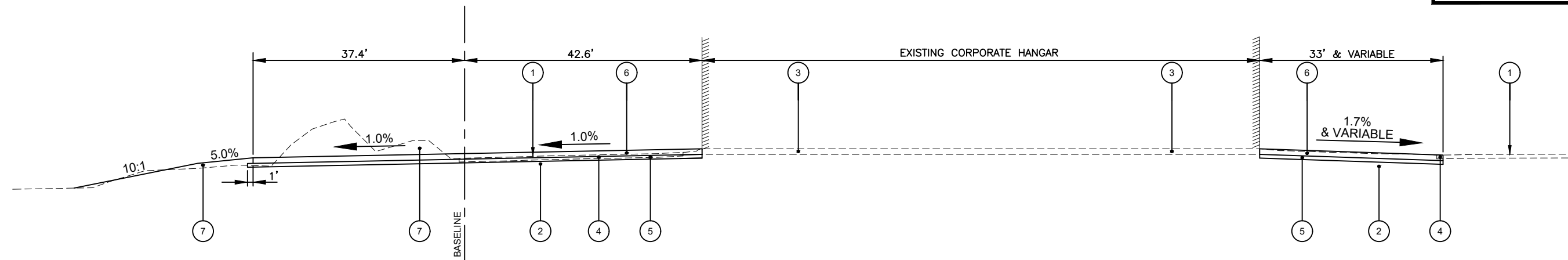
- EXISTING PAVEMENT
- PROPOSED PAVEMENT IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED CONTRACTOR ACCESS AND HAUL ROUTE
- PROPOSED CONTRACTOR PARKING AND STAGING AREAS
- AIRCRAFT OPERATIONS AREA (AOA)
- PROPOSED CONTRACTOR STAGING AREA FOR HAUL TRUCKS

**CRITICAL POINTS**

NO.	LATITUDE	LONGITUDE	GROUND ELEVATION	DESCRIPTION
1	38° 49' 59.76" N	89° 22' 26.55" W	536.0' MSL	PROPOSED SW CORNER APRON
2	38° 50' 01.27" N	89° 22' 26.55" W	536.0' MSL	PROPOSED NW CORNER APRON
3	38° 50' 09.34" N	89° 22' 25.68" W	537.0' MSL	PROPOSED NW CORNER STAGING AREA
4	38° 50' 07.86" N	89° 22' 25.67" W	537.0' MSL	PROPOSED SW CORNER STAGING AREA



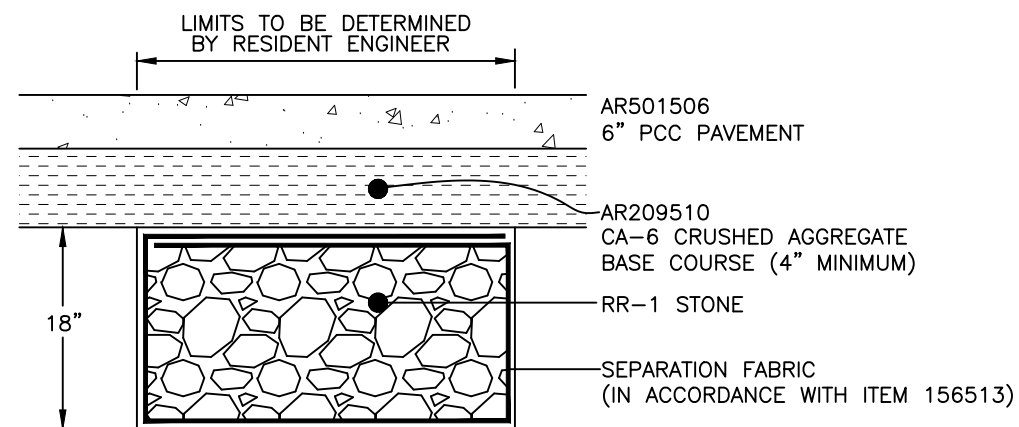
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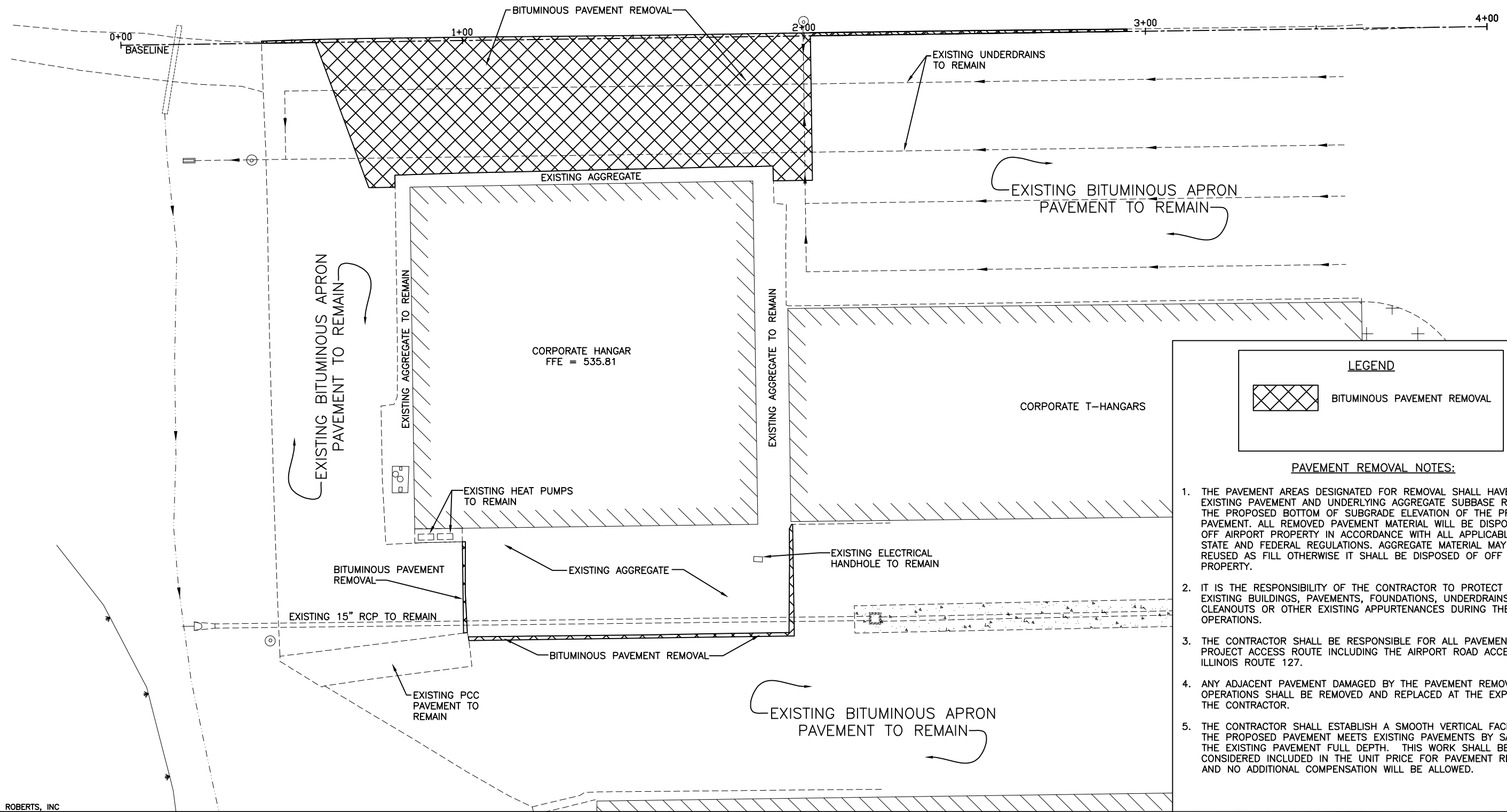
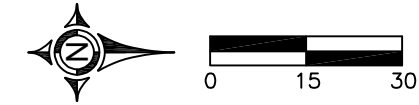
TYPICAL SECTION STATION 1+00 (WEST - EAST)  
NO SCALE

LEGEND


- |   |                                       |   |   |
|---|---------------------------------------|---|---|
| 1 | EXISTING HMA SURFACE                  | 5 | AR209510 - CRUSHED AGGREGATE BASE COURSE (4" DEPTH) |
| 2 | EXISTING AGGREGATE BASE TO REMAIN     | 6 | AR501506 - 6" PCC PAVEMENT                          |
| 3 | EXISTING PCC PAVEMENT TO REMAIN       | 7 | AR152410 - UNCLASSIFIED EXCAVATION                  |
| 4 | AR401900 - REMOVE BITUMINOUS PAVEMENT |   |   |



SUBGRADE REPAIR DETAIL  
NO SCALE

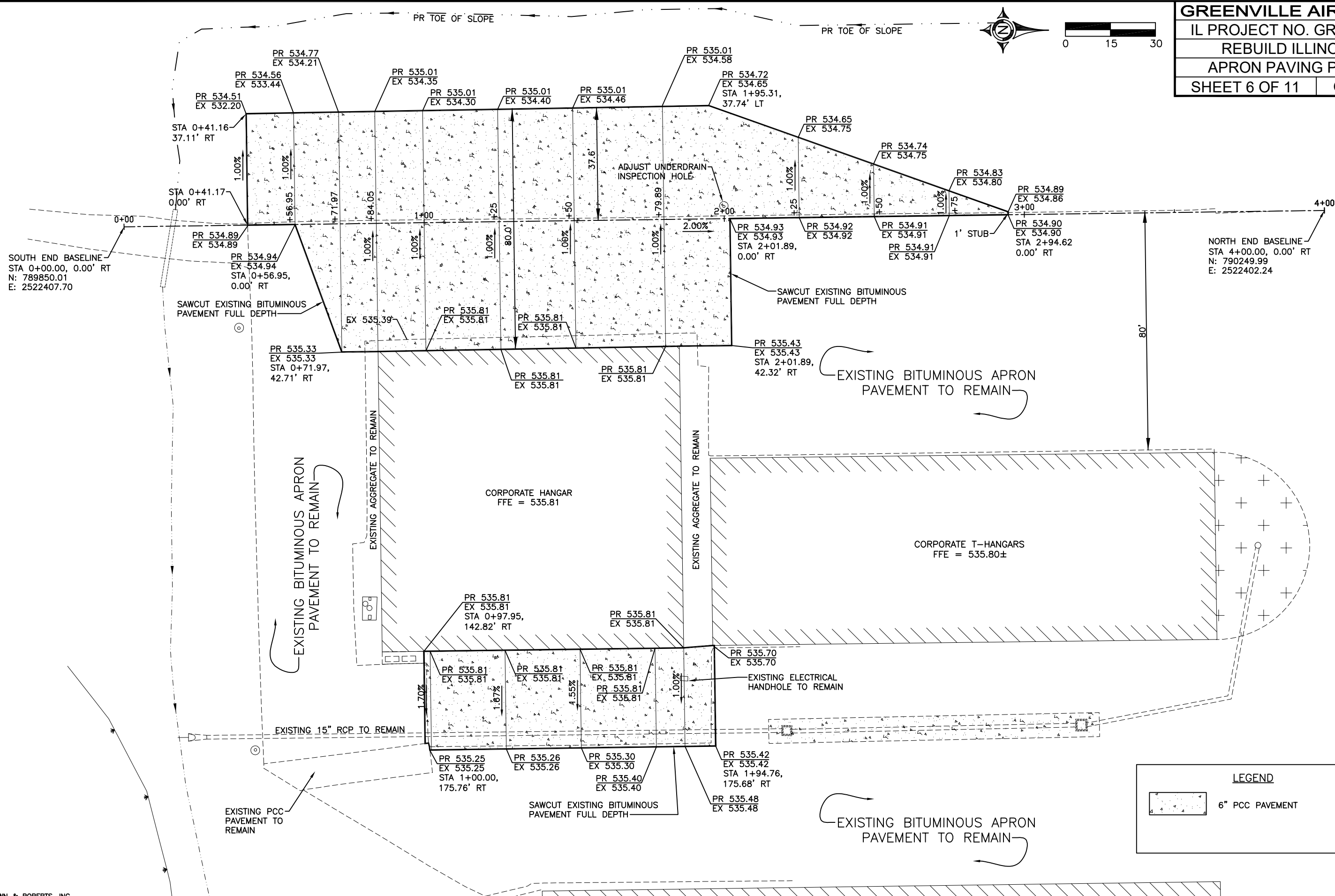
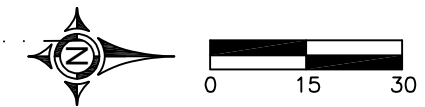


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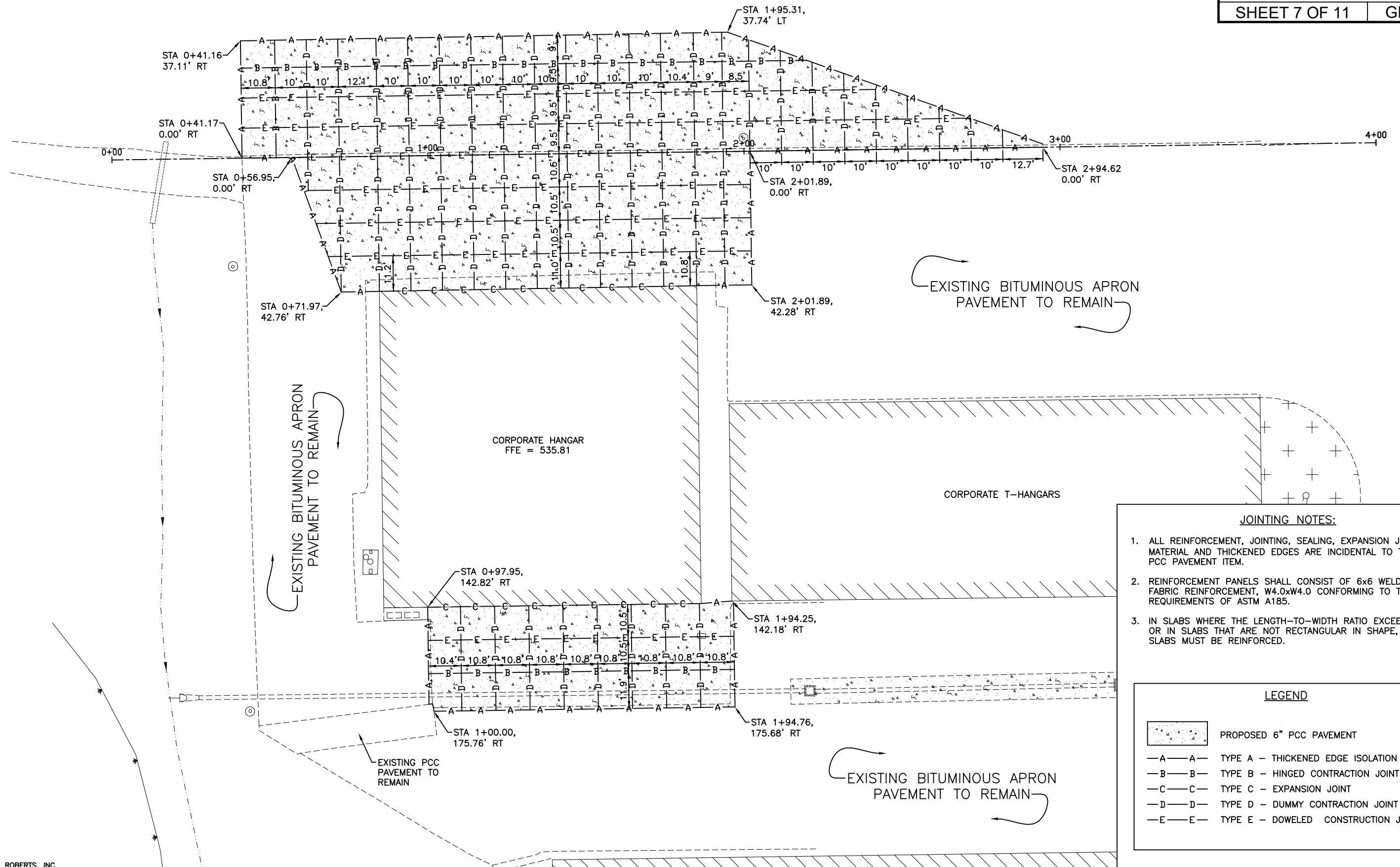
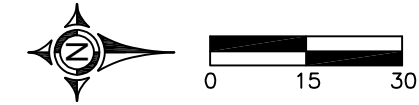
 BITUMINOUS PAVEMENT REMOVAL

- PAVEMENT REMOVAL NOTES:**
1. THE PAVEMENT AREAS DESIGNATED FOR REMOVAL SHALL HAVE THE EXISTING PAVEMENT AND UNDERLYING AGGREGATE SUBBASE REMOVED TO THE PROPOSED BOTTOM OF SUBGRADE ELEVATION OF THE PROPOSED PAVEMENT. ALL REMOVED PAVEMENT MATERIAL WILL BE DISPOSED OF OFF AIRPORT PROPERTY IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE AND FEDERAL REGULATIONS. AGGREGATE MATERIAL MAY BE REUSED AS FILL OTHERWISE IT SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
  2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL EXISTING BUILDINGS, PAVEMENTS, FOUNDATIONS, UNDERDRAINS, CLEANOUTS OR OTHER EXISTING APPURTENANCES DURING THE REMOVAL OPERATIONS.
  3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PAVEMENTS ON THE PROJECT ACCESS ROUTE INCLUDING THE AIRPORT ROAD ACCESS FROM ILLINOIS ROUTE 127.
  4. ANY ADJACENT PAVEMENT DAMAGED BY THE PAVEMENT REMOVAL OPERATIONS SHALL BE REMOVED AND REPLACED AT THE EXPENSE OF THE CONTRACTOR.
  5. THE CONTRACTOR SHALL ESTABLISH A SMOOTH VERTICAL FACE WHERE THE PROPOSED PAVEMENT MEETS EXISTING PAVEMENTS BY SAW CUTTING THE EXISTING PAVEMENT FULL DEPTH. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE FOR PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

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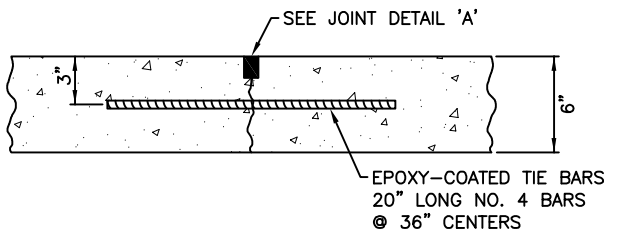
**JOINTING NOTES:**

- ALL REINFORCEMENT, JOINTING, SEALING, EXPANSION JOINT MATERIAL AND THICKENED EDGES ARE INCIDENTAL TO THE 6" PCC PAVEMENT ITEM.
- REINFORCEMENT PANELS SHALL CONSIST OF 6x6 WELDED WIRE FABRIC REINFORCEMENT, W4.0xW4.0 CONFORMING TO THE REQUIREMENTS OF ASTM A185.
- IN SLABS WHERE THE LENGTH-TO-WIDTH RATIO EXCEEDS 1.25, OR IN SLABS THAT ARE NOT RECTANGULAR IN SHAPE, THE SLABS MUST BE REINFORCED.

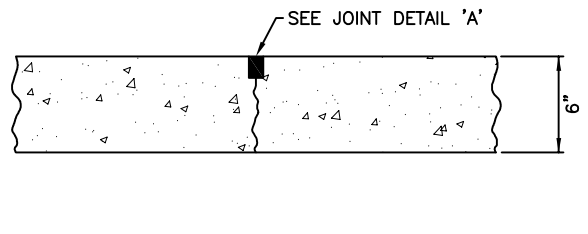
  

**LEGEND**

	PROPOSED 6" PCC PAVEMENT
-A-A-	TYPE A - THICKENED EDGE ISOLATION JOINT
-B-B-	TYPE B - HINGED CONTRACTION JOINT
-C-C-	TYPE C - EXPANSION JOINT
-D-D-	TYPE D - DUMMY CONTRACTION JOINT
-E-E-	TYPE E - DOWELED CONSTRUCTION JOINT

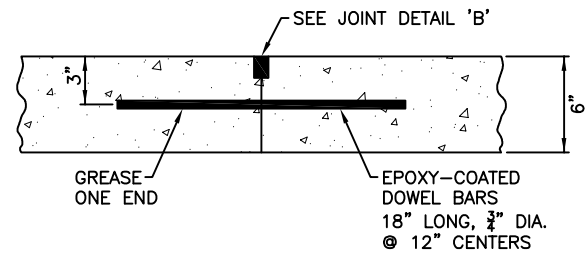


TYPE B HINGED



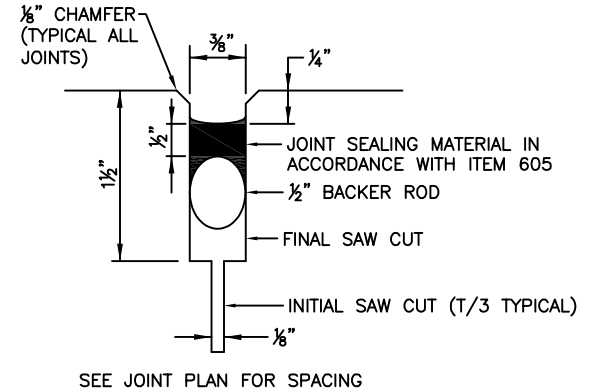
TYPE D DUMMY

CONTRACTION JOINTS

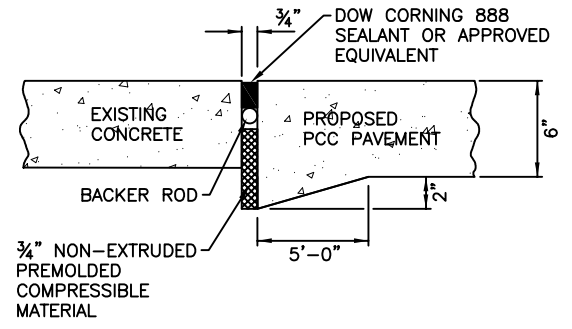


TYPE E DOWELED

CONSTRUCTION JOINTS

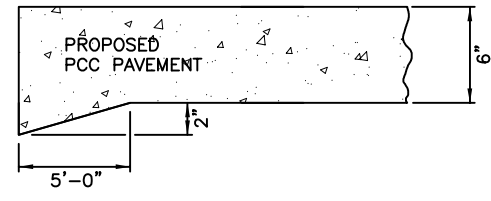


JOINT DETAIL 'A'  
NO SCALE



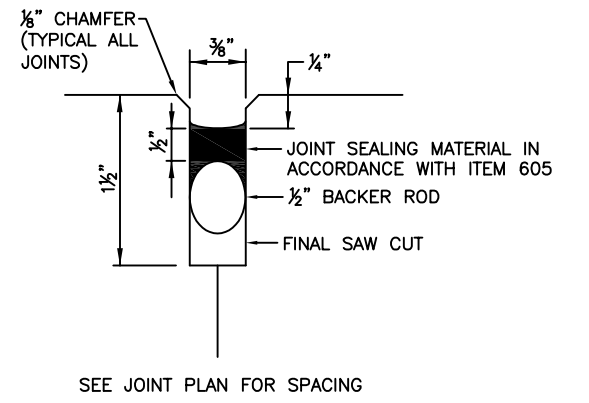
TYPE C EXPANSION

EXPANSION JOINTS



TYPE A THICKENED EDGE

ISOLATION JOINTS



JOINT DETAIL 'B'  
NO SCALE



