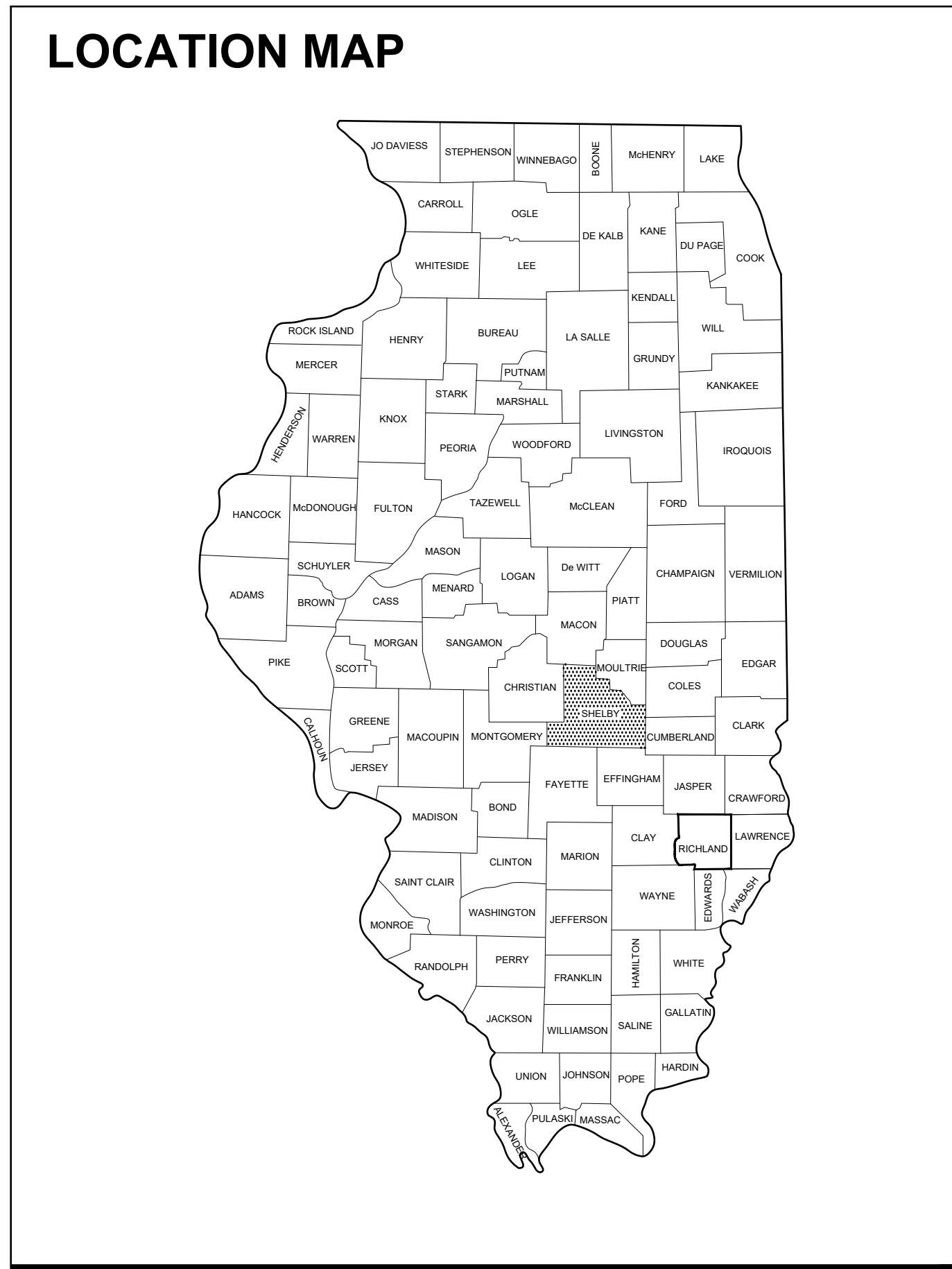
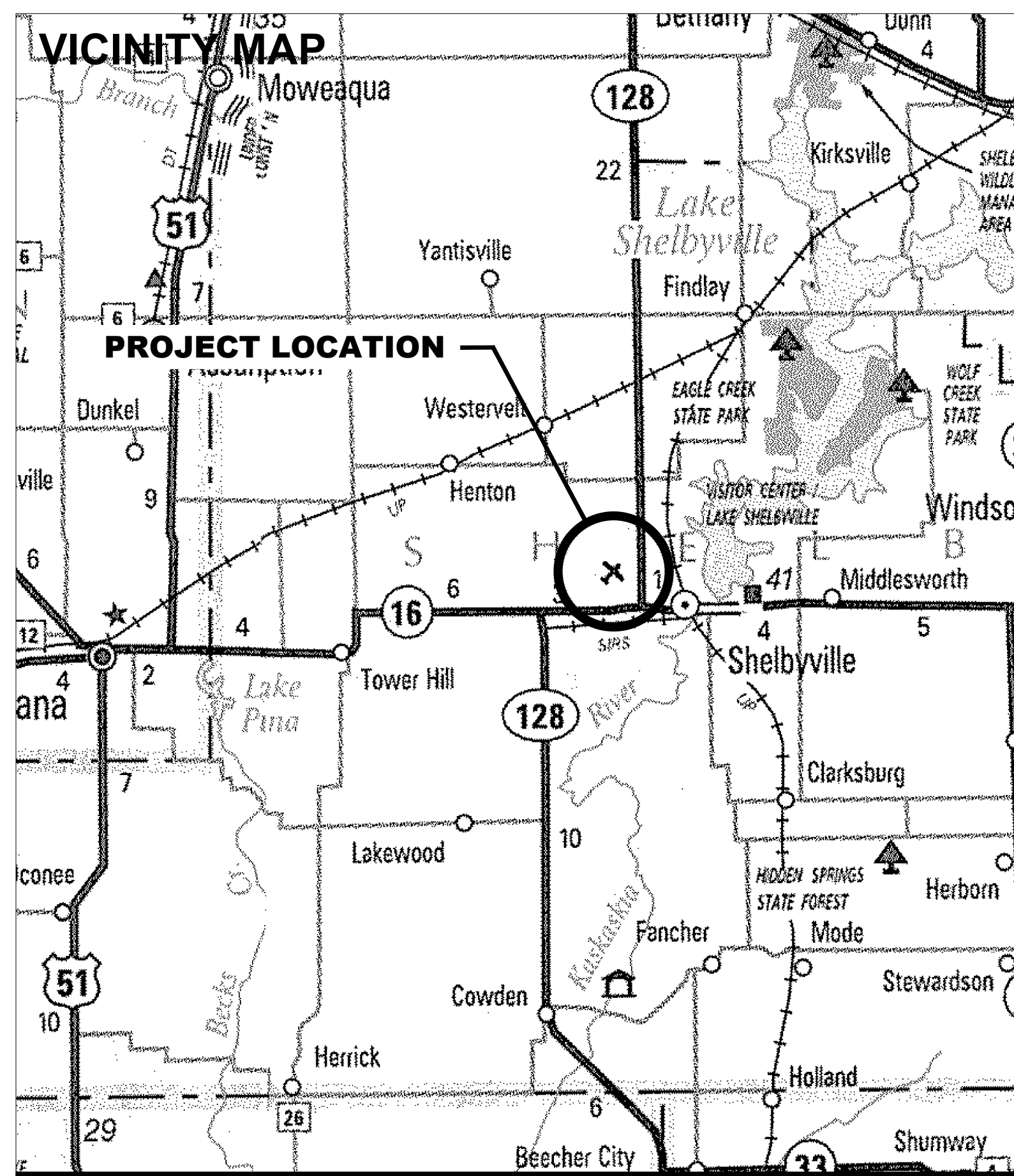


CONSTRUCTION PLANS

REHABILITATE MAIN APRON

**SHELBY COUNTY AIRPORT
SHELBY COUNTY AIRPORT AND LANDING
FIELD COMMISSION
SHELBYVILLE, SHELBY COUNTY, ILLINOIS**

**IDA PROJECT NO.: 2H0-5148
FAA AIP PROJECT : 3-17-SBGP-171/184/197/TBD
NOVEMBER 22, 2024
(100% PLANS)**



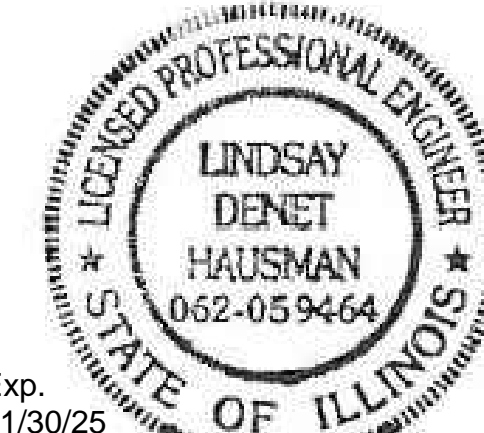
NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

HANSON
HANSON PROFESSIONAL SERVICES INC.
1525 South Sixth Street
Springfield, Illinois 62703-2886
Telephone: 217.788.2450
Fax: 217.788.2503

Lindsay Hausman
Lindsay Hausman, P.E.
Project Manager



Exp. 11/30/25
NOVEMBER 22, 2024
Date

SHELBY COUNTY AIRPORT AND LANDING FIELD COMMISSION
P. O. Box 295
Assumption, Illinois 62510

Steve R. Wempen
Steve R. Wempen
Secretary
NOVEMBER 22, 2024
Date



INDEX OF SHEETS	
SHEET #	SHEET TITLE
1	COVER SHEET
2	SHEET INDEX AND SUMMARY OF QUANTITIES
3	CONSTRUCTION SAFETY PLAN
4	CONSTRUCTION SAFETY NOTES AND DETAILS
5	TYPICAL PAVEMENT SECTIONS AND DUCT MARKER DETAILS
6	PAVEMENT PATCHING DETAILS
7	TIE DOWN SCHEDULE
8	TIE DOWN DETAILS
9	PROPOSED PAVEMENT PREPARATION PLAN
10	PROPOSED CONSTRUCTION PLAN
11	SHOULDER ADJUSTMENT AND EROSION CONTROL PLAN
12	EROSION CONTROL DETAILS
13	PROPOSED MARKING PLAN

SUMMARY OF QUANTITIES - BASE BID				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150520	MOBILIZATION	L SUM	1	
AR150530	TRAFFIC MAINTENANCE	L SUM	1	
AR152480	SHOULDER ADJUSTMENT	SQ YD	1,454	
AR156510	SILT FENCE	FOOT	490	
AR156520	INLET PROTECTION	EACH	1	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	FOOT	3,700	
AR201670	CRACK CONTROL FABRIC	SQ YD	300	
AR401613	BIT. SURF. CSE.-METHOD I, SUPERPAVE	TON	1,075	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	9,347	
AR401660	SAW & SEAL BIT. JOINTS	FOOT	292	
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	734	
AR401915	REM & REP BIT PAVEMENT-TYPE A	SQ YD	120	
AR401916	REM & REP BIT PAVEMENT-TYPE B	SQ YD	120	
AR510510	TIE DOWN	EACH	33	
AR510900	REMOVE TIE DOWN	EACH	36	
AR603510	BITUMINOUS TACK COAT	GALLON	748	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	770	
AR620525	PAVEMENT MARKING-BLACK BORDER	SQ FT	792	
AR800932	REMOVE TIE DOWN AND BACKFILL	EACH	12	
AR800999	PAVEMENT RESTORATION	SQ YD	65	
AR901510	SEEDING	ACRE	0.3	
AR908510	MULCHING	ACRE	0.3	

SUMMARY OF QUANTITIES - ADDITIVE ALTERNATE NO. 1				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AS401915	REM & REP BIT PAVEMENT-TYPE A	SQ YD	120	
AS401916	REM & REP BIT PAVEMENT-TYPE B	SQ YD	120	
AS201670	CRACK CONTROL FABRIC	SQ YD	300	

GENERAL NOTES

- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- NPDES PERMIT - THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS **NOT** REQUIRED.
- QUANTITIES - PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.
- MATERIAL CERTIFICATIONS - MATERIALS **CANNOT** BE INSTALLED UNTIL **ALL** THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- CERTIFIED PAYROLLS - THE RESIDENT ENGINEER **CANNOT** FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL **ALL** CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY _____ SHELBY
CITY _____ SHELBYVILLE
TOWNSHIP _____ ROSE TOWNSHIP
TOWNSHIP _____ 11N
RANGE _____ 3E
SECTION _____ 10,11
ADDRESS _____ SHELBY COUNTY AIRPORT
RR #2
SHELBYVILLE, IL 62565

DATE SIGNED:	LICENSE EXPIRES:
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REHABILITATE MAIN APRON

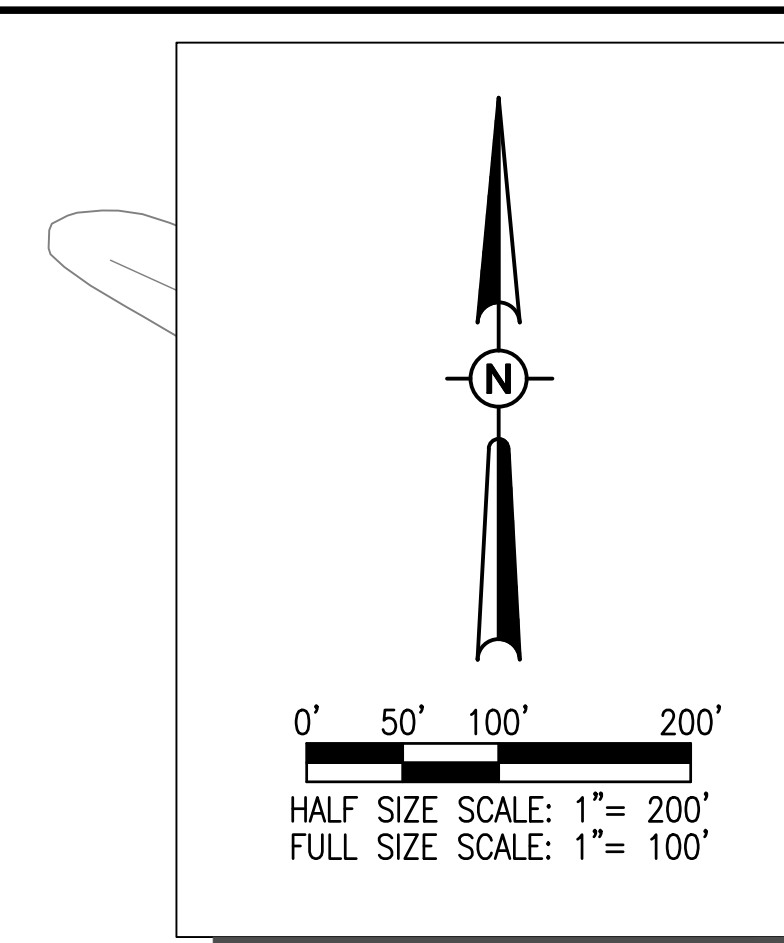
IDA No: 2H0-5148
SBG No.:
3-17-SBGP-171/184/197/TBD

Contract No. SE007

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 22, 2024
PROJECT NO: 24A0057
CAD FILE: G-002-FLP.DWG
DESIGN BY: HLE 05/03/2024
DRAWN BY: HLE 05/03/2024
REVIEWED BY: LDH 10/13/2024

SHEET INDEX AND SUMMARY OF QUANTITIES



SCOPE

MILL AND OVERLAY 2-INCHES ON APRON PAVEMENT, PERFORM CRACK SEALING, REPAIR WIDE TRANSVERSE CRACKS WITH FULL AND PARTIAL DEPTH PATCHING, TIE DOWN REMOVAL AND REPLACEMENT, RE-MARK, SEED, AND MULCH.

NOTES

1. ALL CONTRACTOR ACTIVITIES SHALL TAKE PLACE WITHIN THE PROPOSED WORK AREA AS SHOWN.
2. ALL CONSTRUCTION EQUIPMENT WILL BE LIMITED TO A HEIGHT OF 25 FEET UNLESS PRIOR APPROVAL IS GIVEN BY THE ENGINEER (SEE SPECIAL PROVISIONS).
3. CONTRACTOR'S EQUIPMENT MAY NOT DISRUPT FLIGHT OPERATIONS ON RUNWAY 18-36, RUNWAY 4-22 AND RUNWAY 14-32 AT ANY TIME.
4. TRAFFIC TO BE MAINTAINED ON AIRPORT ROADWAYS AT ALL TIMES.
5. CONTRACTOR VEHICLES SHALL OPERATE IN ACCORDANCE WITH SPECIAL PROVISIONS SECTION 40 WHEN WITHIN THE AIRPORT OPERATIONS AREA.

THIS PROJECT IS TO BE COMPLETED IN ONE PHASE. WORK ITEMS INCLUDE BUT ARE NOT LIMITED TO:

1. INSTALLATION OF TRAFFIC MAINTENANCE AND EROSION CONTROL
2. TIE DOWN REMOVAL AND REPLACEMENT
3. MILLING 2" OF EXISTING PAVEMENTS
4. CRACK REPAIR
5. INSTALLATION OF CRACK CONTROL FABRIC
6. PLACEMENT OF 2" BITUMINOUS OVERLAY
7. PLACEMENT OF PAVEMENT MARKINGS
8. SHOULDER ADJUSTMENT
9. SEEDING AND MULCHING IN ALL DISTURBED AREA, INCLUDING ALONG NEW PAVEMENT EDGES

CONTROL POINTS

POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
KB1565	1,000,921.31	839,095.45	615.27	SHELPORT
KB1566	998,640.33	839,095.93	615.69	SHELPORT AZ MK

SAFETY CRITICAL POINTS

POINT NO.	DESCRIPTION	LAT.	LONG.	GND ELEV. (MSL)	MAX EQUIP. HEIGHT (FT)	TOP ELEV. GND + EQUIP HEIGHT (MSL)
1	CONTRACTOR STAGING AREA	N039° 28' 52.436"	W088° 16' 39.625"	614	25	639
2	CONSTRUCTION ACCESS & HAUL ROUTE	N039° 28' 59.433"	W088° 16' 02.377"	613	25	638
3	EQUIPMENT/LOW PROFILE BARRICADE	N039° 29' 00.175"	W088° 16' 06.231"	612	25	637
4	EQUIPMENT/LOW PROFILE BARRICADE	N039° 29' 00.917"	W088° 16' 10.085"	612	25	637
5	EQUIPMENT/LOW PROFILE BARRICADE	N039° 28' 59.211"	W088° 16' 14.551"	614	25	639
6	EQUIPMENT/LOW PROFILE BARRICADE	N039° 28' 46.497"	W088° 16' 44.126"	614	25	639
7	EQUIPMENT/LOW PROFILE BARRICADE	N039° 28' 46.497"	W088° 16' 44.126"	612	25	637

PROPOSED	LEGEND
	PROPOSED WORK AREA
	STAGING AREA (AR150520)
	LOW PROFILE BARRICADE (AR150530)
	TEMPORARY HAUL ROUTE / ACCESS ROUTE
	SAFETY CRITICAL POINT

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DATE SIGNED: LICENSE EXPIRES:

REHABILITATE MAIN APRON

IDA No: 2H0-5148
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Contract No. SE007

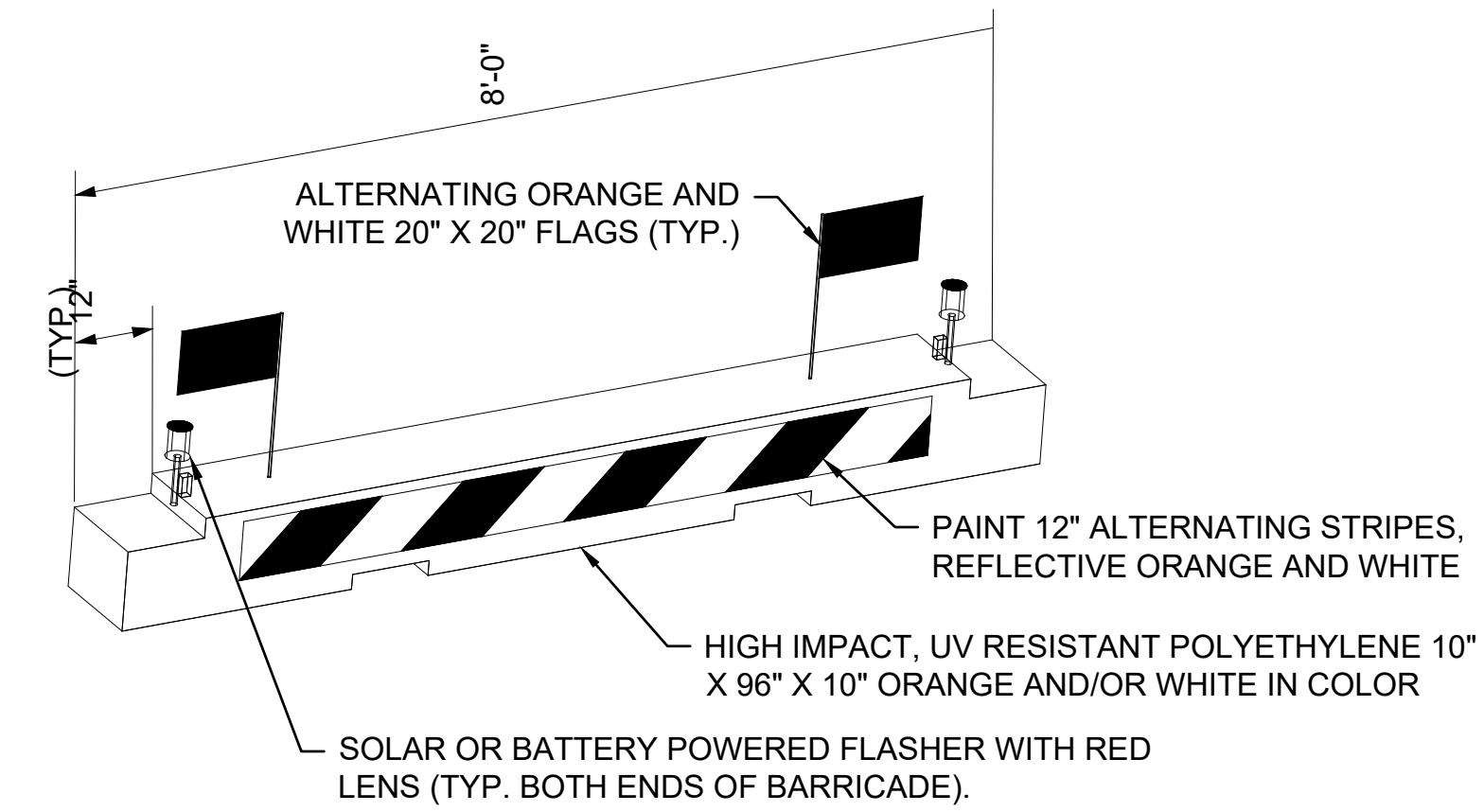
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 DRAWN BY: HLE 05/03/2024
 REVIEWED BY: LDH 10/13/2024

CONSTRUCTION SAFETY PLAN

CONSTRUCTION SAFETY NOTES

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN.
- ALL COSTS ASSOCIATED WITH THE WORK AND MATERIALS INVOLVING CONSTRUCTION SAFETY & PHASING AS SHOWN OR DESCRIBED IN THE CONTRACT DOCUMENTS, OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN OR AIRPORT MANAGER THROUGHOUT THE PROJECT, SHALL BE PAID UNDER ITEM AR150530 TRAFFIC MAINTENANCE.
- AIRFIELD SAFETY ASSURANCE - AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FORTH IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT EDITION) WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION OF ACCESS TO THE AIRFIELD ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- THE CONTRACTOR SHALL PROVIDE THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN WITH CONTACT INFORMATION FOR A PERSON ON CALL 24 HOURS A DAY FOR EMERGENCY MAINTENANCE OF BARRICADES AND HAZARD LIGHTING.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" (CURRENT EDITION).
- CLOSED AIRFIELD WORK AREAS, OPEN TRENCHES, AND OTHER HAZARDOUS AREAS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO PAVEMENT EDGE DROP-OFFS GREATER THAN 3" ARE ALLOWED TO REMAIN ON ANY ACTIVE AIRFIELD PAVEMENT AREA. TEMPORARY EARTH SHOULDERS SHALL BE PLACED IF AN INCOMPLETE AREA IS TO BE REOPENED TO AIRCRAFT TRAFFIC.
- AIRPORT SECURITY - AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. NO CONTRACTOR PERSONNEL OR EQUIPMENT WILL BE PERMITTED OUTSIDE OF THE CONSTRUCTION LIMITS DEPICTED ON THESE PLANS. PERSONAL VEHICLES OF CONTRACTOR EMPLOYEES SHALL BE PARKED IN THE DESIGNATED STAGING AREA AND ARE NOT ALLOWED ACCESS ON THE SITE. THE CONTRACTOR WILL CLOSE AND LOCK THE GATE AT THE DESIGNATED CONSTRUCTION ENTRANCE AT THE END OF EACH DAY. ALL CONTRACTOR EMPLOYEES SHALL WEAR A GARMENT IDENTIFYING THEM AS CONSTRUCTION PERSONNEL.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO REMAIN IN CONSTANT TWO-WAY RADIO CONTACT WITH THE SHELBY COUNTY AIRPORT UNICOM (122.8 MHz) AT ALL TIMES WHILE HE HAS PERSONNEL OR EQUIPMENT ON THE AIRFIELD.



LOW PROFILE BARRICADE EXAMPLE
NOT TO SCALE

BARRICADE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT WITH A MAXIMUM SPACING OF 4' BETWEEN ENDS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE PAID UNDER ITEM AR150530 TRAFFIC MAINTENANCE.
- BARRICADES MUST BE IN ACCEPTABLE CONDITION.

DATE SIGNED:	LICENSE EXPIRES:
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REHABILITATE MAIN APRON

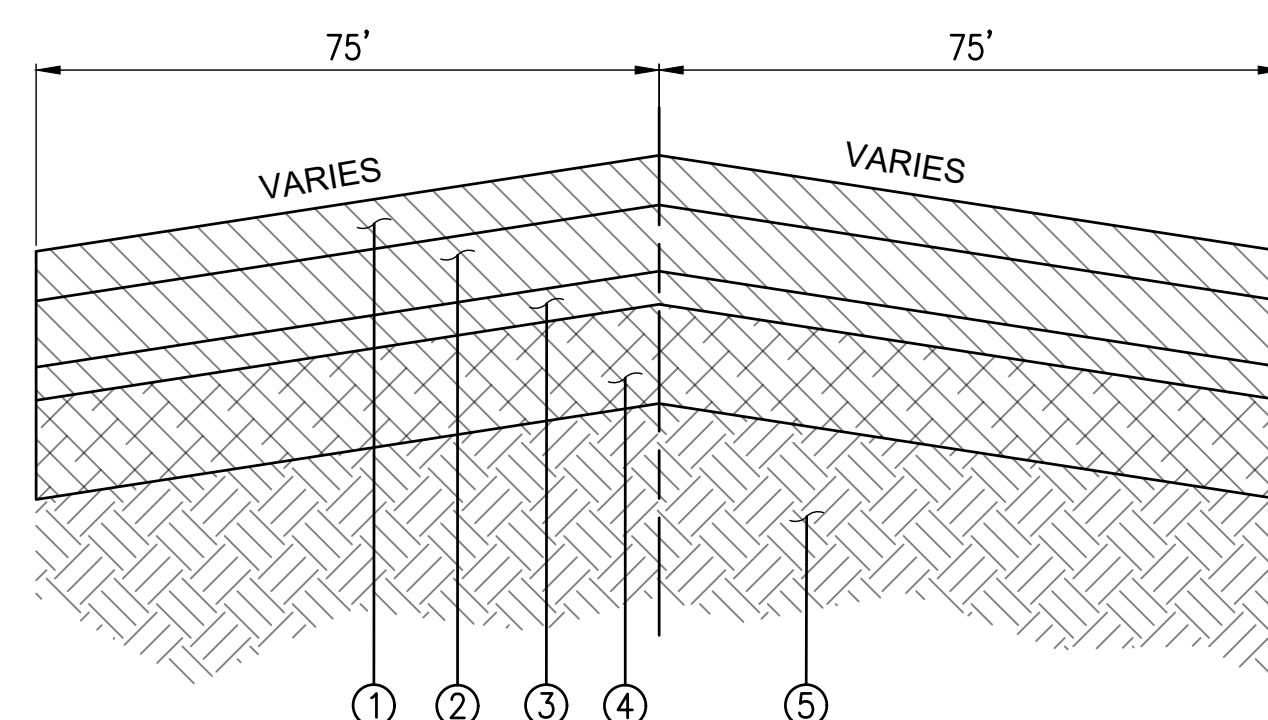
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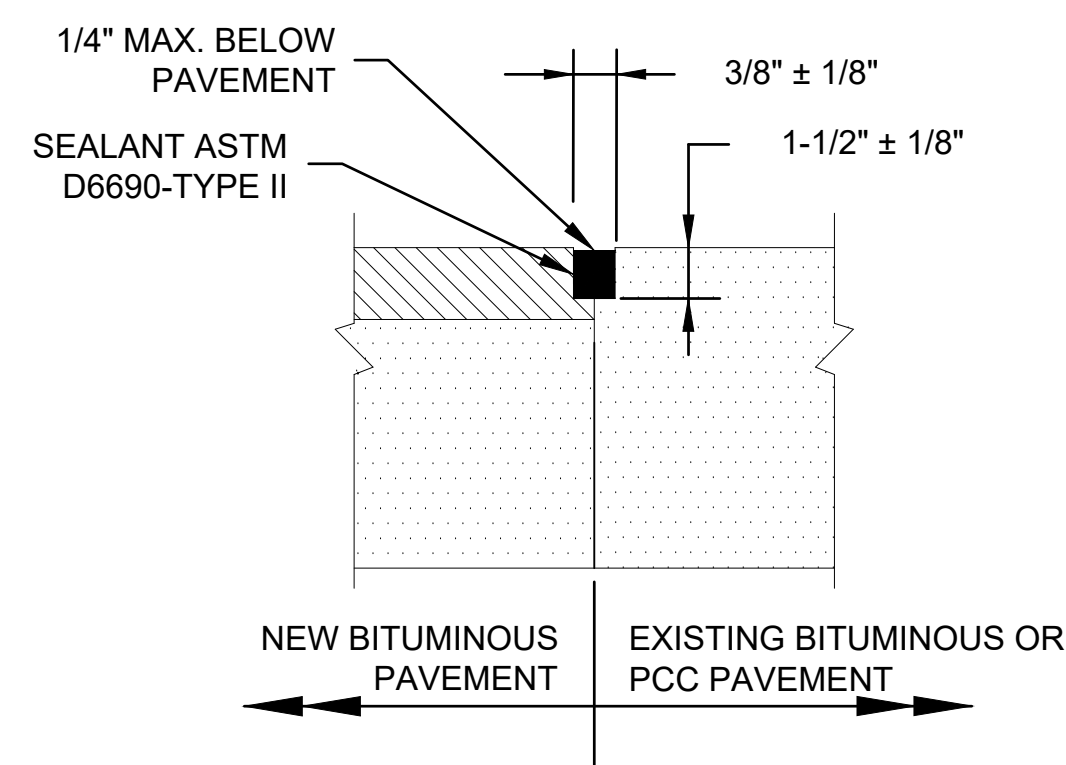
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CONSTRUCTION SAFETY NOTES AND DETAILS



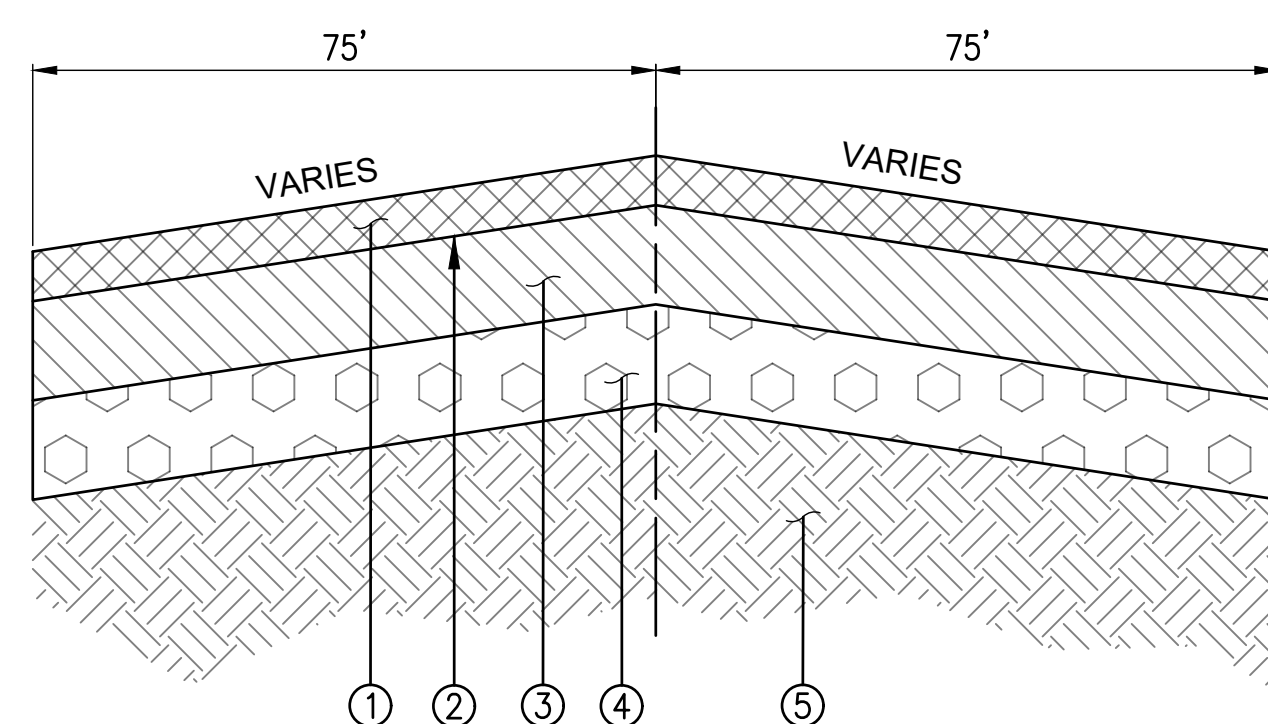
EXISTING APRON TYPICAL SECTION
NOT TO SCALE

- ① EXISTING BIT. SURFACE COURSE (3" AVG.)
- ② EXISTING BIT. SURFACE COURSE (4")
- ③ EXISTING BIT. SURFACE COURSE (2")
- ④ SOUTHWEST HALF - EXISTING BIT. BASE COURSE (6")
NORTHEAST HALF - EXISTING AGGREGATE BASE COURSE (6")
- ⑤ EXISTING SUBGRADE



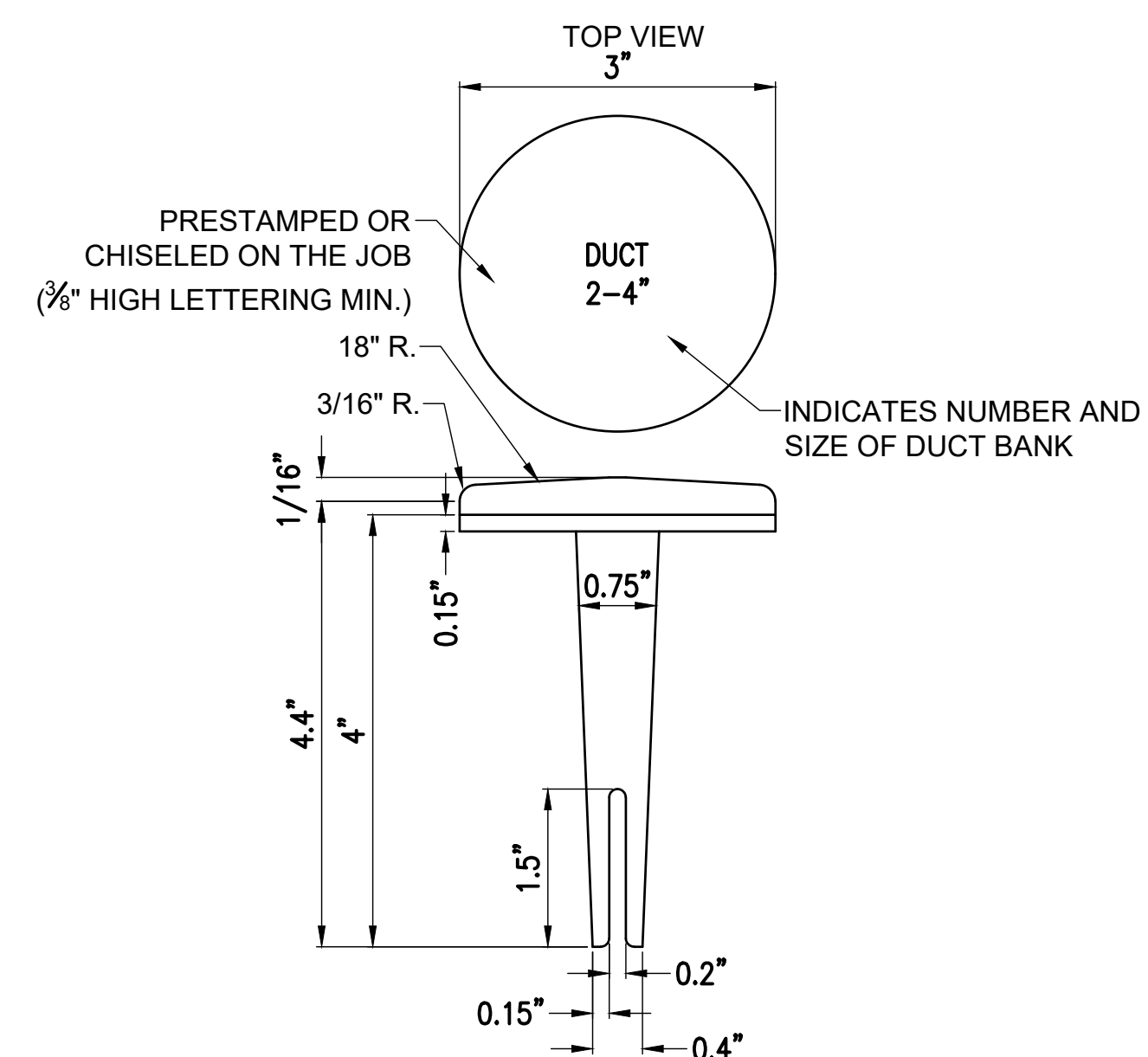
NOTE:
ALL BITUMINOUS/BITUMINOUS JOINT SEALING TO BE PAID UNDER SAW AND SEAL BITUMINOUS JOINTS, ITEM AR401660.

BITUMINOUS/BITUMINOUS SEAL



PROPOSED APRON TYPICAL SECTION
NOT TO SCALE

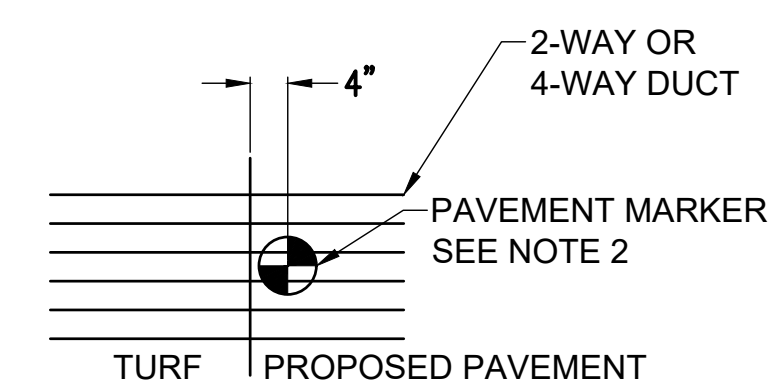
- ① PROPOSED 2-IN MILL AND HMA OVERLAY
- ② PROPOSED BITUMINOUS TACK COAT
- ③ EXISTING BITUMINOUS SURFACE COURSE TO REMAIN (7")
- ④ SOUTHWEST HALF - EXISTING BIT. BASE COURSE (6")
NORTHEAST HALF - EXISTING AGGREGATE BASE COURSE (6")
- ⑤ EXISTING SUBGRADE



BITUMINOUS PAVEMENT DUCT MARKERS
"NOT TO SCALE"

NOTE:

- TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE
- BRASS DUCT MARKERS ARE AVAILABLE FROM BERNTSEN INTERNATIONAL INC., P.O. BOX 8670, MADISON, WI. 53708-8670, PHONE: 1-877-959-8556, SURV-KAP, 3225 E. 47TH ST., TUCSON, AZ 85713, PHONE: (502)-622-6011, OR OTHER EQUIVALENT MANUFACTURERS.



DUCT MARKER DETAIL-PLAN
"NOT TO SCALE"

DUCT MARKER NOTES:

- BITUMINOUS PAVEMENT DUCT MARKERS TO BE PROVIDED AS SHOWN ON THE PLAN.
- THE COST OF REMOVAL AND REPLACEMENT OF DUCT MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.

DATE SIGNED:	LICENSE EXPIRES:
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REHABILITATE MAIN APRON

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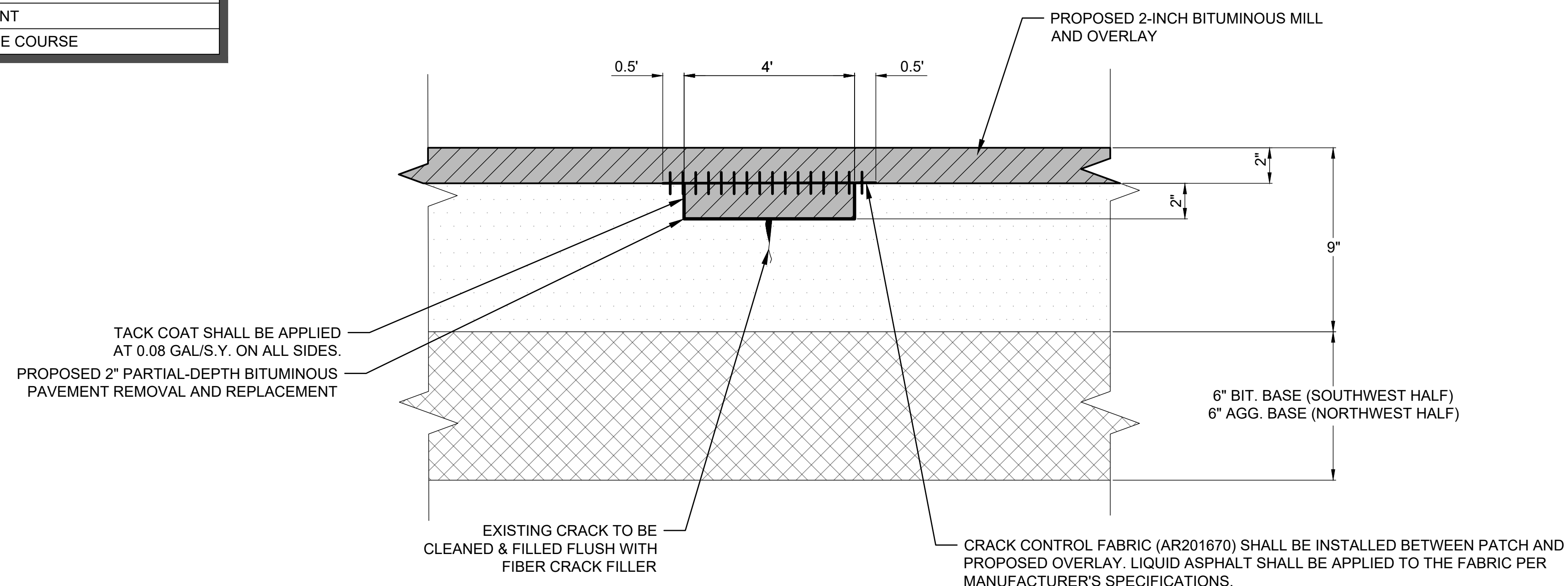
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PROJECT NO: 24A0057
CAD FILE: C-301-TYP.DWG
DESIGN BY: HLE 05/03/2024
DRAWN BY: HLE 05/03/2024
REVIEWED BY: LDH 10/13/2024

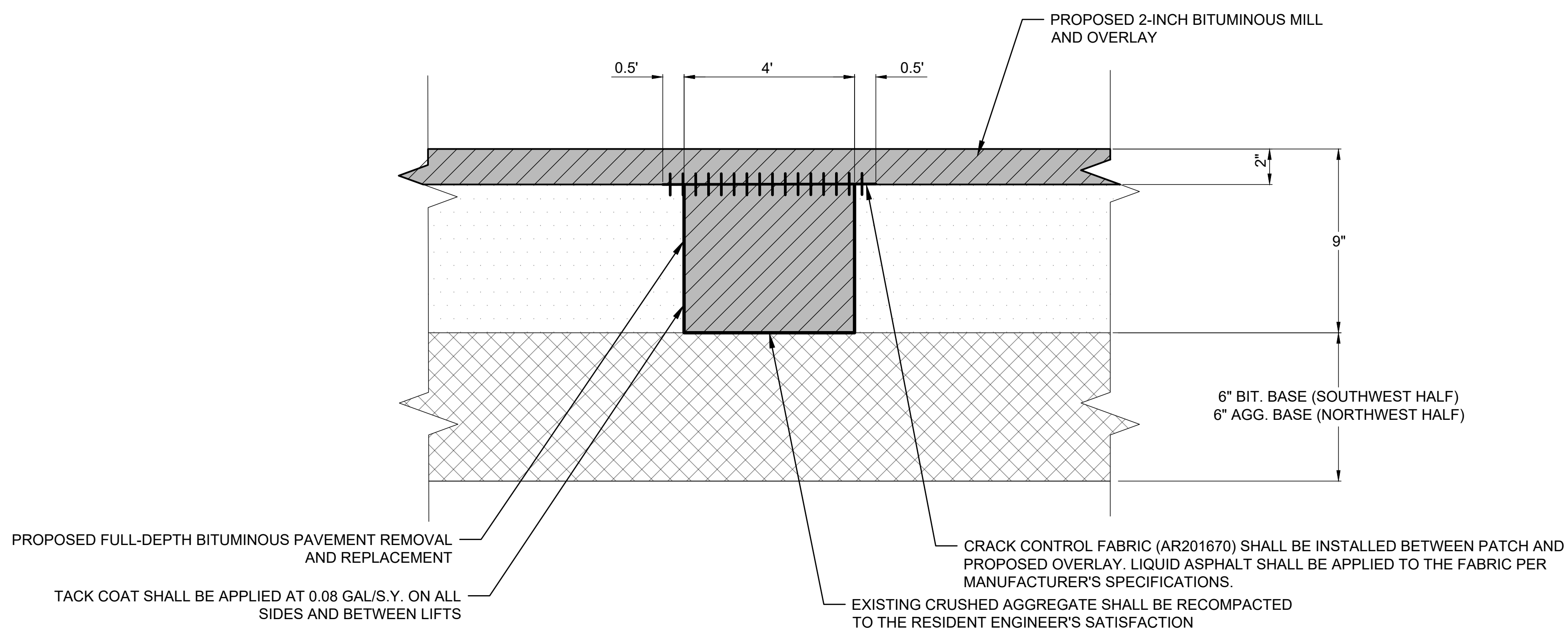
TYPICAL PAVEMENT SECTIONS AND DUCT MARKER DETAILS

EXISTING	PROPOSED	LEGEND
		BIT. PAVEMENT
		BIT. PAVEMENT REMOVAL
		BITUMINOUS PAVEMENT
		AGGREGATE/BIT. BASE COURSE



AR401915 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE A (PARTIAL DEPTH)

NOT TO SCALE



AR401916 REMOVE & REPLACE BITUMINOUS PAVEMENT - TYPE B (FULL DEPTH)

NOT TO SCALE

DATE SIGNED: LICENSE EXPIRES:

REHABILITATE MAIN APRON

IDA No: 2H0-5148
SBG No.:
3-17-SBGP-171/184/197/TBD

Contract No. SE007

NO.	DATE	DESCRIPTION		
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PAVEMENT PATCHING DETAILS



Engineering | Planning | Allied Services

Hanson Professional Services Inc.
1525 S. Sixth St.
Springfield, IL 62703
Phone: (217) 788-2450

Offices Nationwide
www.hanson-inc.com

SHELBY COUNTY AIRPORT

P.O. BOX 295
Assumption, IL 62510

DATE LICENSE
SIGNED: EXPIRES:

**REHABILITATE MAIN
APRON**

IDA No: 2H0-5148
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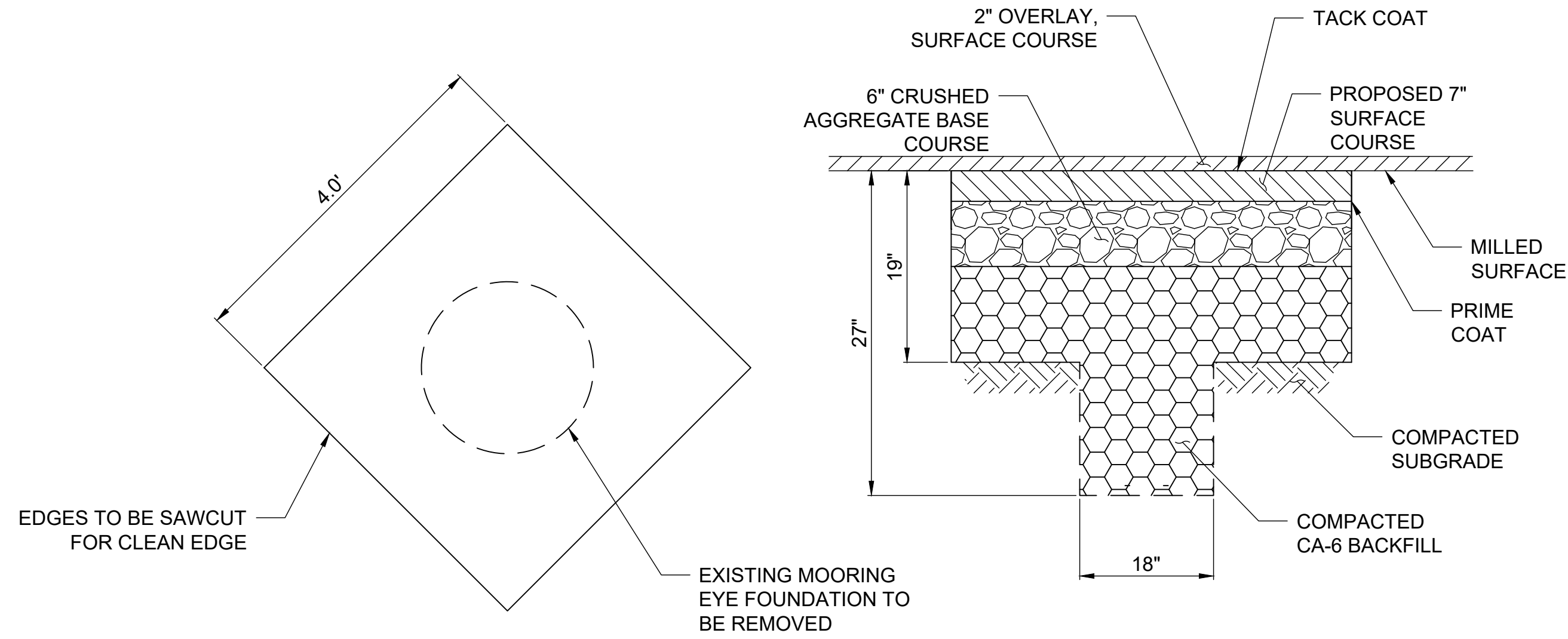
DRAWN BY: HLE 05/03/2024

REVIEWED BY: LDH 10/13/2024

TIE DOWN SCHEDULE

NO.	DESCRIPTION	STATION	OFFSET		TIE DOWN (AR510510) EACH	REMOVE TIE DOWN (AR510900) EACH	TIE DOWN REMOVE AND BACKFILL (AR510900) EACH	PAVEMENT RESTORATION (AR800999) SQ YD
TD01	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	3+15.12	49.93'	LT	1	1		1.8
TD02	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	3+27.11	66.74'	LT	1	1		1.8
TD03	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	3+39.07	49.92'	LT	1	1		1.8
TD04	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	3+60.07	49.91'	LT	1	1		1.8
TD05	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	3+72.07	66.72'	LT	1	1		1.8
TD06	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	3+84.03	49.90'	LT	1	1		1.8
TD07	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+05.03	49.88'	LT	1	1		1.8
TD08	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+17.03	66.70'	LT	1	1		1.8
TD09	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+28.99	48.87'	LT	1	1		1.8
TD10	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+49.99	49.86'	LT	1	1		1.8
TD11	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+61.98	66.67'	LT	1	1		1.8
TD12	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+73.95	49.85'	LT	1	1		1.8
TD13	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	4+94.95	49.85'	LT	1	1		1.8
TD14	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+06.94	66.65'	LT	1	1		1.8
TD15	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+18.90	49.83'	LT	1	1		1.8
TD16	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+39.90	49.81'	LT	1	1		1.8
TD17	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+51.90	66.62'	LT	1	1		1.8
TD18	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+63.86	49.80'	LT	1	1		1.8
TD19	TIE DOWN REMOVAL AND PAVEMENT RESTORATION	5+84.86	49.79'	LT		1		1.8
TD20	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	5+96.86	66.60'	LT			1	
TD21	TIE DOWN REMOVAL AND PAVEMENT RESTORATION	6+08.82	49.78'	LT		1		1.8
TD22	TIE DOWN REMOVAL AND PAVEMENT RESTORATION	6+29.82	49.76'	LT		1		1.8
TD23	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	6+41.81	66.58'	LT			1	
TD24	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	6+53.78	49.75'	LT			1	
TD25	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	6+74.78	49.74'	LT			1	
TD26	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	6+86.77	66.55'	LT			1	
TD27	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	6+98.73	49.73'	LT			1	
TD28	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	7+19.73	49.72'	LT			1	
TD29	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	7+31.73	66.53'	LT			1	
TD30	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	7+43.69	49.71'	LT			1	
TD31	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	7+64.69	49.69'	LT			1	
TD32	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	7+76.69	66.50'	LT			1	
TD33	TIE DOWN REMOVAL AND TOPSOIL BACKFILL	7+88.65	49.68'	LT			1	
TD34	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+24.04	58.52'	RT	1	1		1.8
TD35	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+36.00	75.34'	RT	1	1		1.8
TD36	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+48.00	58.53'	RT	1	1		1.8
TD37	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+69.00	58.54'	RT	1	1		1.8
TD38	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+80.96	75.36'	RT	1	1		1.8
TD39	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	5+92.96	58.55'	RT	1	1		1.8
TD40	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	6+13.96	58.56'	RT	1	1		1.8
TD41	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	6+25.92	75.38'	RT	1	1		1.8
TD42	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	6+37.91	58.57'	RT	1	1		1.8
TD43	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	6+58.91	58.58'	RT	1	1		1.8
TD44	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	6+70.88	75.40'	RT	1	1		1.8
TD45	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	6+82.87	58.59'	RT	1	1		1.8
TD46	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	7+03.87	58.60'	RT	1	1		1.8
TD47	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	7+15.83	75.42'	RT	1	1		1.8
TD48	TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN	7+27.83	58.61'	RT	1	1		1.8
TOTAL					33	36	12	64.8

TIE DOWN SCHEDULE

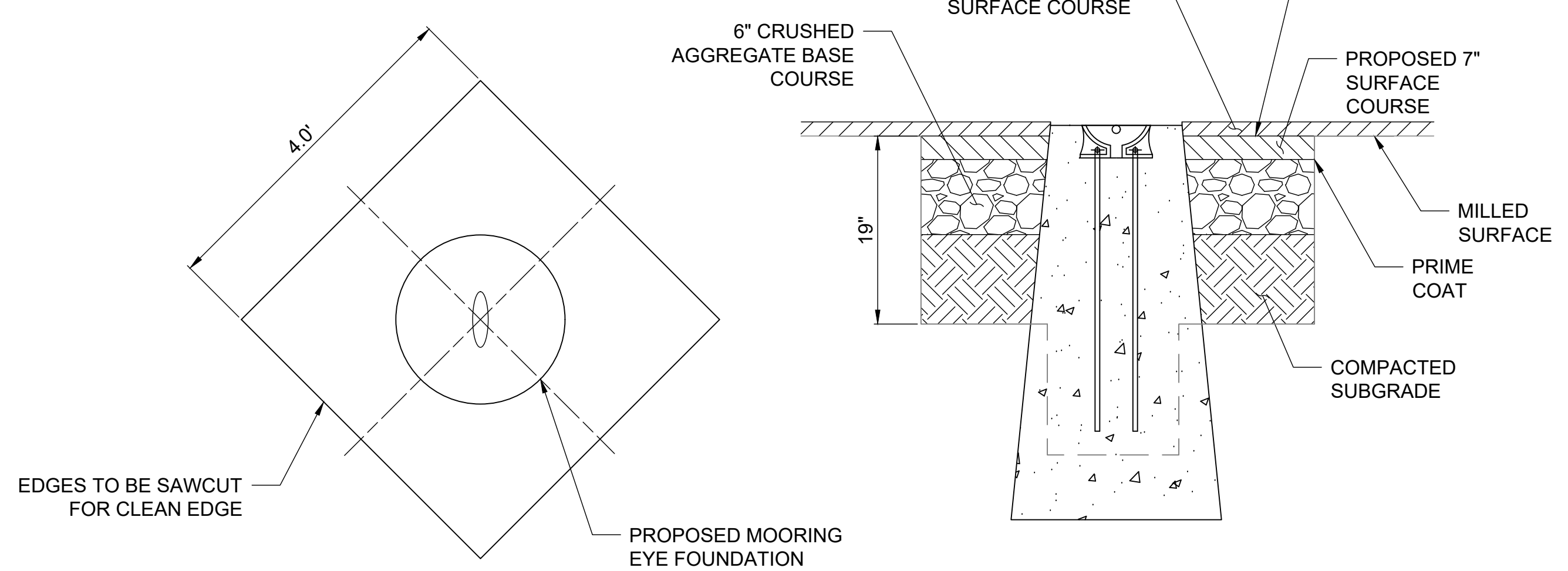


NOTES:

1. SAWCUT, PAVEMENT REMOVAL, EXCAVATION AND DISPOSAL, AND BACKFILL SHALL BE INCIDENTAL TO REMOVE TIE DOWN.
2. 6" CRUSHED AGGREGATE BASE COURSE, PRIME COAT, AND BITUMINOUS SURFACE COURSE SHALL BE PAID FOR UNDER ITEM AR800999 PAVEMENT RESTORATION.
3. TACK COAT AND BITUMINOUS SURFACE COURSE TO BE PAID FOR UNDER AR603510 AND AR401613 RESPECTIVELY.

TIE DOWN REMOVAL AND PAVEMENT RESTORATION

NTS
 SEE SCHEDULE FOR LOCATIONS

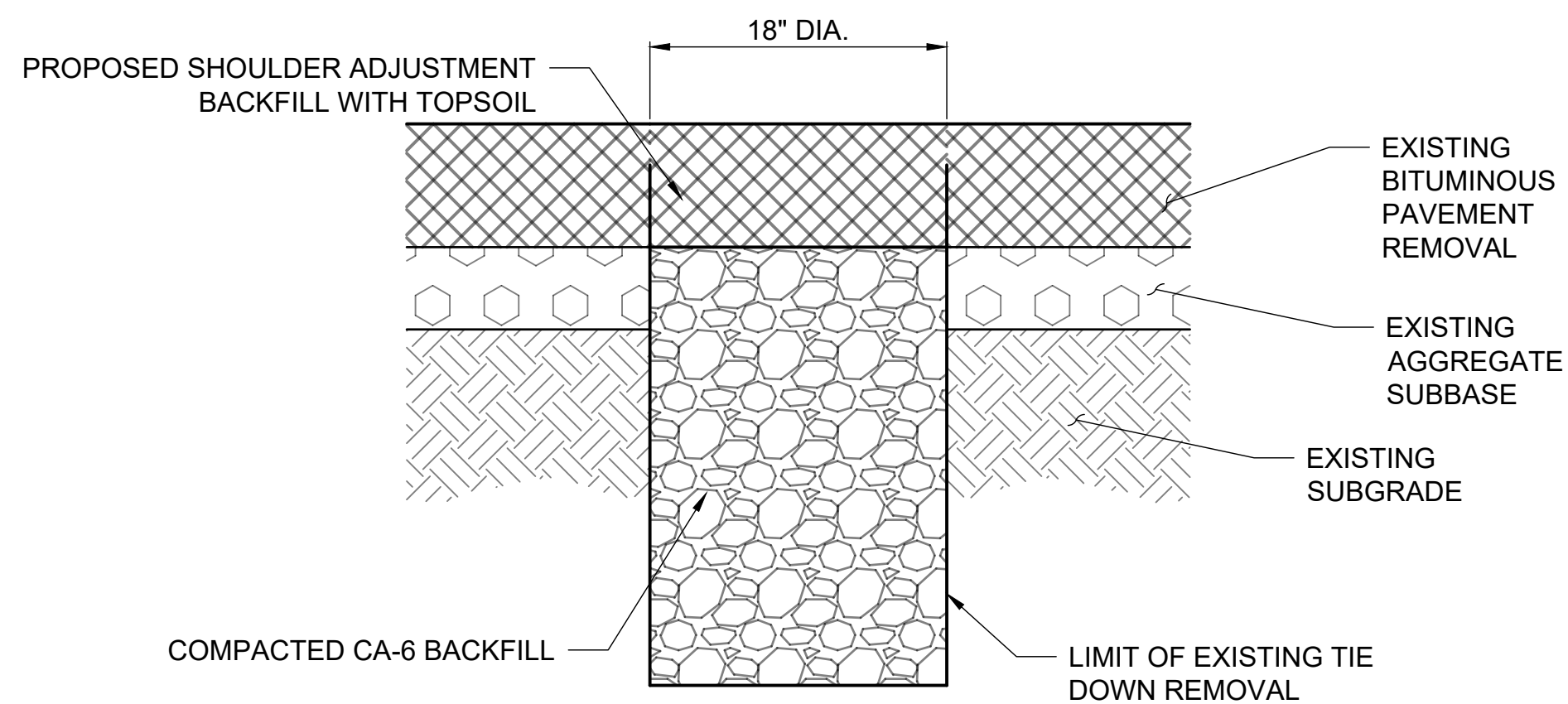


NOTES:

1. SAWCUT, PAVEMENT REMOVAL, EXCAVATION AND DISPOSAL SHALL BE INCIDENTAL TO REMOVE TIE DOWN.
2. 6" CRUSHED AGGREGATE BASE COURSE, PRIME COAT, AND BITUMINOUS SURFACE COURSE SHALL BE PAID FOR UNDER ITEM AR800999 PAVEMENT RESTORATION.
3. EXCAVATION, BACKFILL, CONCRETE FOUNDATION, MOORING EYE SHALL BE INCIDENTAL TO TIE DOWN.
4. TACK COAT AND BITUMINOUS SURFACE COURSE TO BE PAID FOR UNDER AR603510 AND AR401613 RESPECTIVELY.

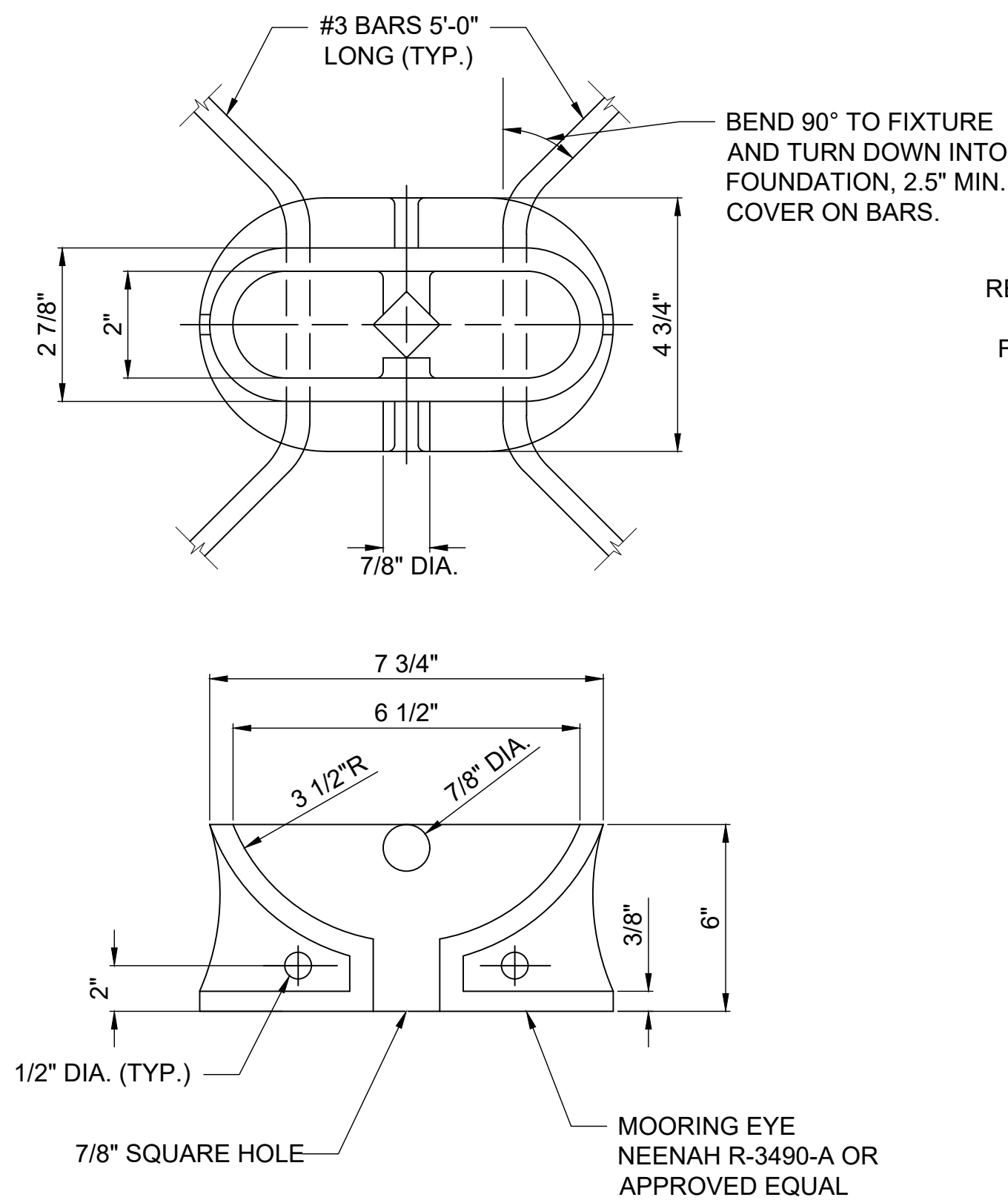
TIE DOWN REMOVAL, PAVEMENT RESTORATION AND PROPOSED TIE DOWN

NTS
 SEE SCHEDULE FOR LOCATIONS



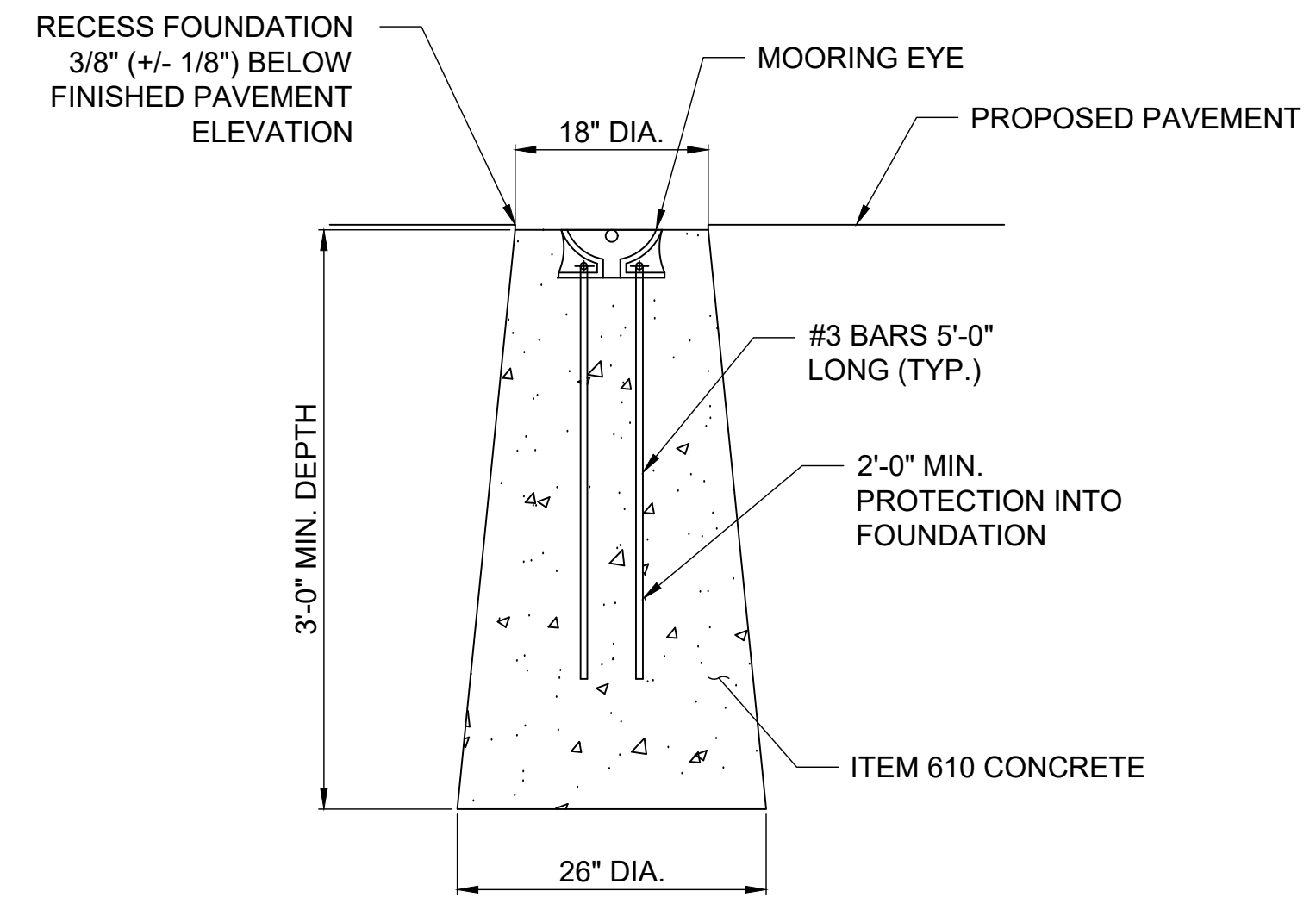
TIE DOWN REMOVAL AND BACKFILL

NTS
 SEE SCHEDULE FOR LOCATIONS



PROPOSED TIE DOWN DETAILS

NTS



DATE SIGNED:	LICENSE EXPIRES:
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REHABILITATE MAIN APRON

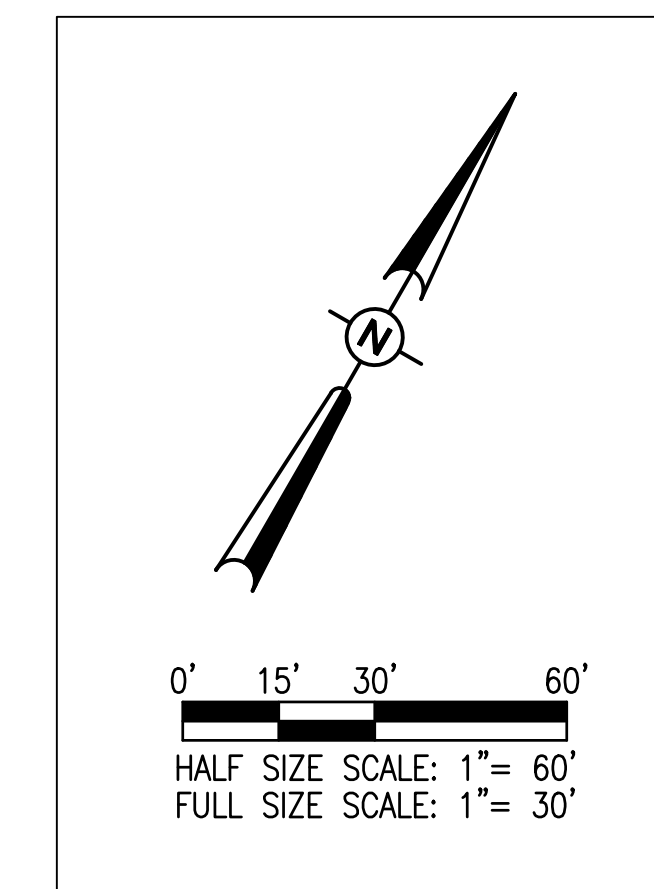
IDA No: 2H0-5148
 SBG No.:
 3-17-SBGP-171/184/197/TBD

Contract No. SE007

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 DESIGN BY: HLE 05/03/2024
 DRAWN BY: HLE 05/03/2024
 REVIEWED BY: LDH 10/13/2024

TIE DOWN DETAILS



UTILITY NOTES

1. THE CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.
2. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN.

CONSTRUCTION NOTES

CLEAN & SEAL BITUMINOUS CRACKS

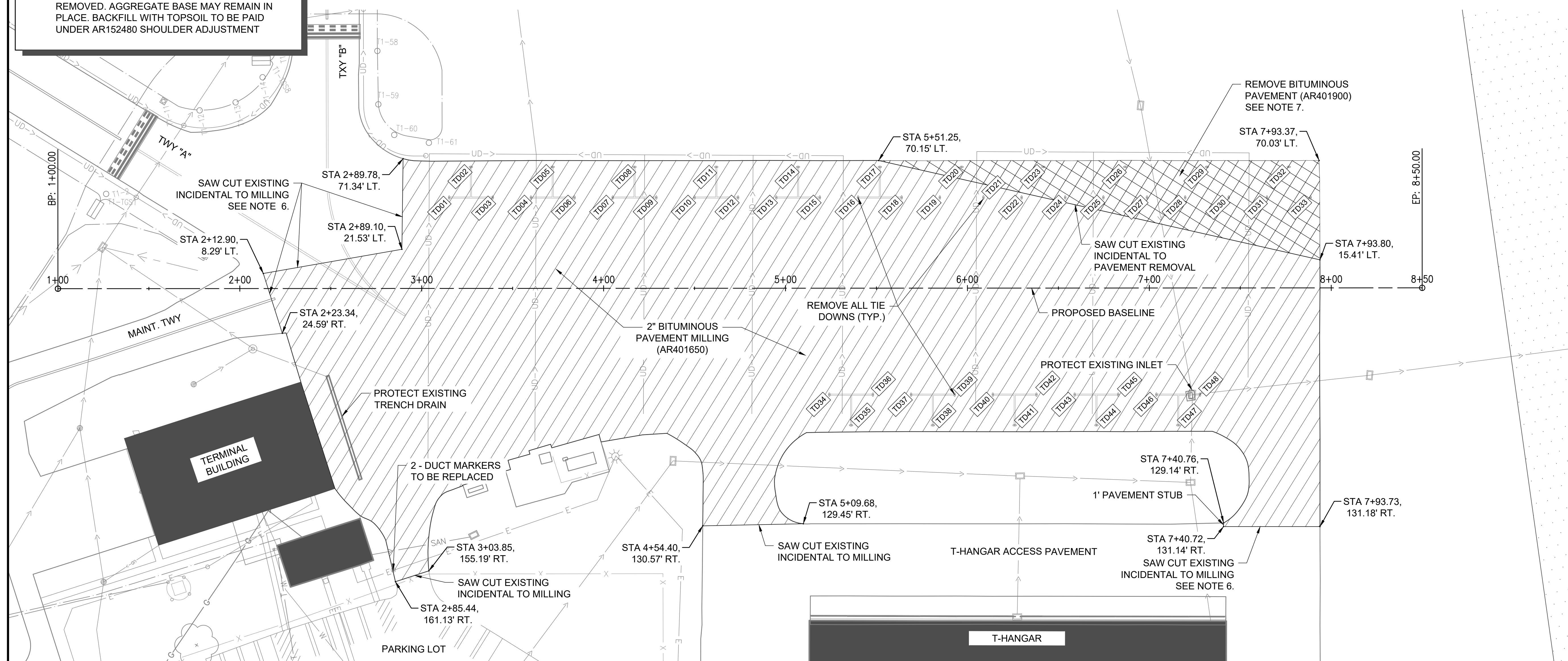
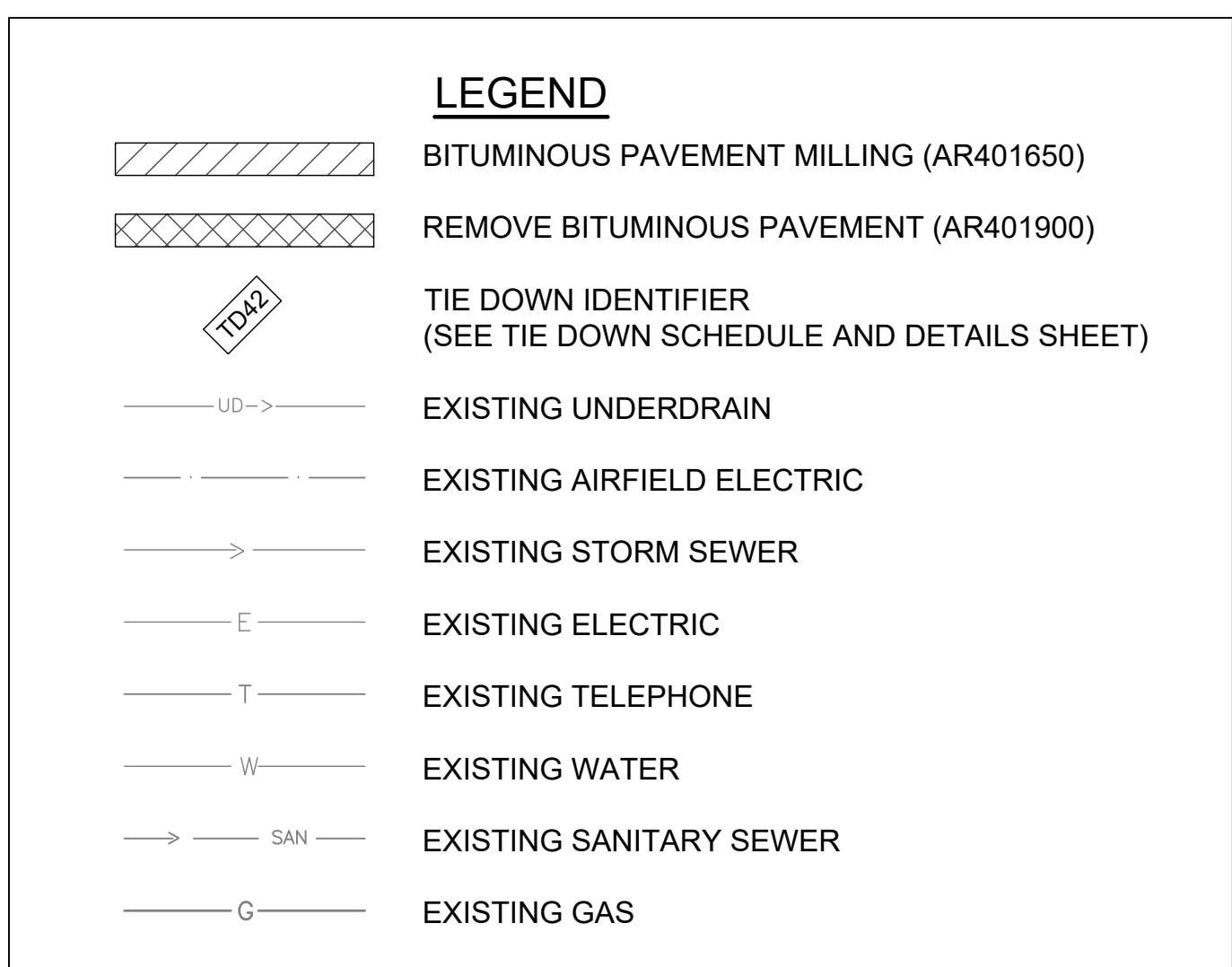
3. A NOMINAL QUANTITY OF AR201661 "CLEAN AND SEAL BITUMINOUS CRACKS" HAS BEEN INCLUDED AND WILL BE LAID OUT BY THE RESIDENT ENGINEER.

REMOVE & REPLACE BITUMINOUS PAVEMENT

4. SEE TYPICAL SECTIONS FOR PATCHING. PATCHING LOCATIONS ARE APPROXIMATE AND WILL BE CONFIRMED AND LAID OUT BY THE RESIDENT ENGINEER/TECHNICIAN.
5. ALL PATCHES WERE DETERMINED DURING A FIELD SURVEY. UPON INSPECTION BY RESIDENT ENGINEER/TECHNICIAN, PATCHES WILL BE LAID OUT AND LOCATED ON THE APRON. ALL PATCHES WILL BE MEASURED AND PAID FOR AS AR401915 AND AR401916.
6. AT NOTED LOCATIONS, LIMITS OF SAWCUT SHALL BE ONE FOOT BEYOND EXISTING JOINT.
7. IN PAVEMENT REMOVAL AREA ALL HMA SHALL BE REMOVED. AGGREGATE BASE MAY REMAIN IN PLACE. BACKFILL WITH TOPSOIL TO BE PAID UNDER AR152480 SHOULDER ADJUSTMENT

ALIGNMENT DATA				
ALIGNMENT	LOCATION	STATION	EASTING	NORTHING
BASELINE	BEGIN	1+00	839,579.98	998,456.61
	END	8+50	840,216.39	998,853.44

PATCHING SCHEDULE							
CRACK NO.	LOCATION	PATCH TYPE	PATCHING LENGTH (FT)	PATCHING WIDTH (FT)	TYPE A TOTAL (AR401915) (SY)	TYPE B TOTAL (AR401916) (SY)	CRACK CONTROL FABRIC TOTAL (AR201670) (SY)
NOMINAL A	-	A	270	4	120.0		150.0
NOMINAL B	-	B	270	4		120.0	150.0



DATE SIGNED:	LICENSE EXPIRES:
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REHABILITATE MAIN APRON

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Contract No. SE007

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CAD FILE: C-101-PRP.DWG

DESIGN BY: HLE 05/03/2024

DRAWN BY: HLE 05/03/2024

REVIEWED BY: LDH 10/13/2024

PROPOSED PAVEMENT PREPARATION PLAN

HOT MIX ASPHALT MIXTURE REQUIREMENTS						
ITEM	Ndes	TARGET AIR VOIDS	PG BINDER	AGGREGAT E QUALITY	Max RAP	DENISTY ACCEPTANCE
BITUMINOUS SURFACE COURSE - METHOD I, SUPERPAVE	30	3.00%	76-22	A	0.00%	MAINLINE - NUCLEAR GAUGE DENSITY, 92.8% LONG. JOINT - CORES, 90.5%
REMOVE & REPLACE - TYPE A & B	30	3.00%	76-22 OR 64-22	A	0.00%	NUCLEAR GAUGE DENSITY, 92.8%

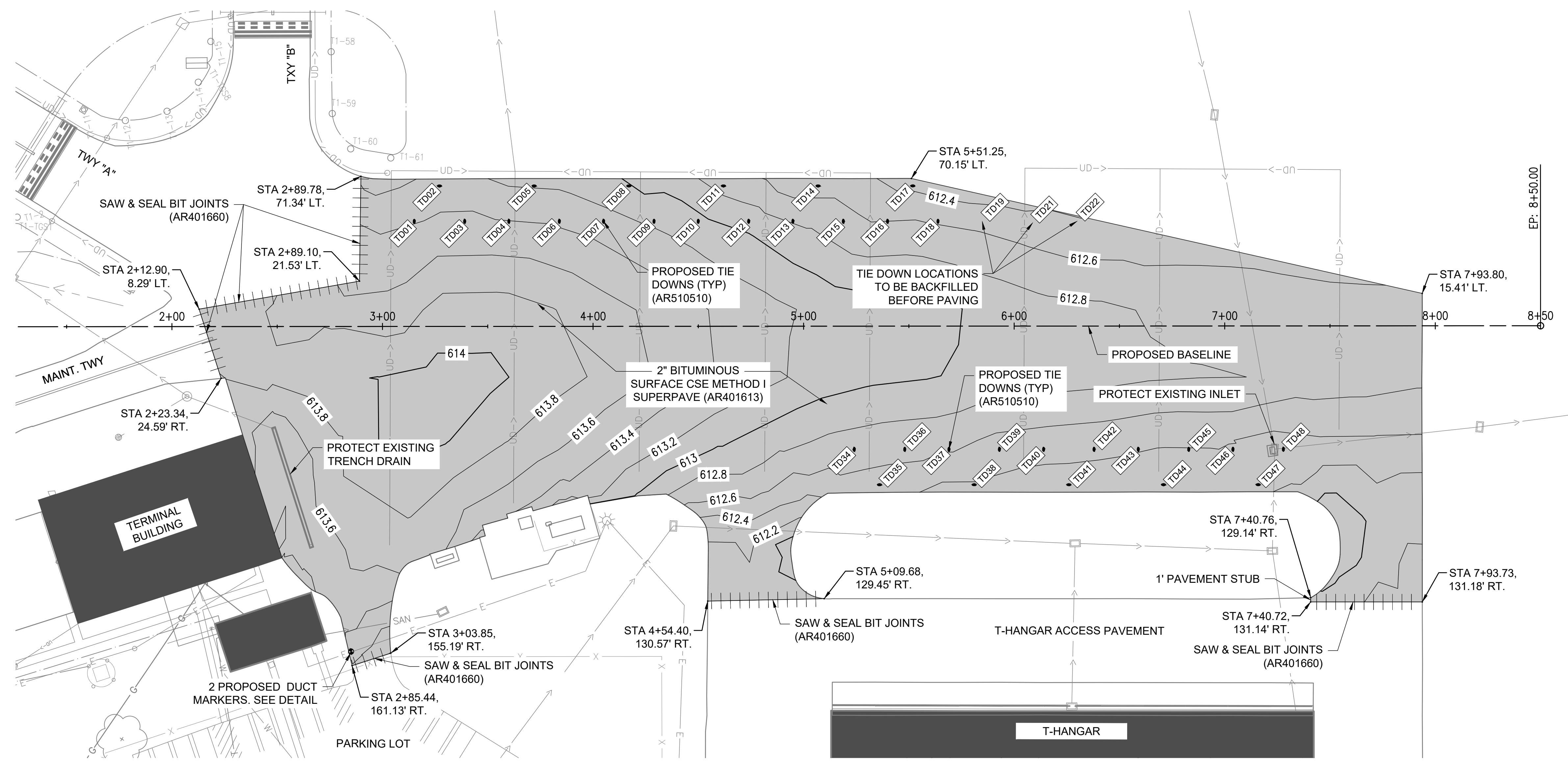
LEGEND

- BITUMINOUS SURFACE COURSE METHOD 1 SUPERPAVE (AR401613)
- TIE DOWN IDENTIFIER (SEE TIE DOWN SCHEDULE AND DETAILS SHEET)
- EXISTING UNDERDRAIN
- EXISTING AIRFIELD ELECTRIC
- EXISTING STORM SEWER
- EXISTING ELECTRIC
- EXISTING TELEPHONE
- EXISTING WATER
- EXISTING SANITARY SEWER
- EXISTING GAS

0' 15' 30' 60'

HALF SIZE SCALE: 1" = 60'
 FULL SIZE SCALE: 1" = 30'

NOTE: PROPOSED PAVEMENT ELEVATIONS SHALL MATCH EXISTING. EDGES OF PROPOSED OVERLAID PAVEMENT SHALL MATCH EXISTING GRADES AT THE TIE IN LOCATIONS.



DATE SIGNED:	LICENSE EXPIRES:

REHABILITATE MAIN APRON

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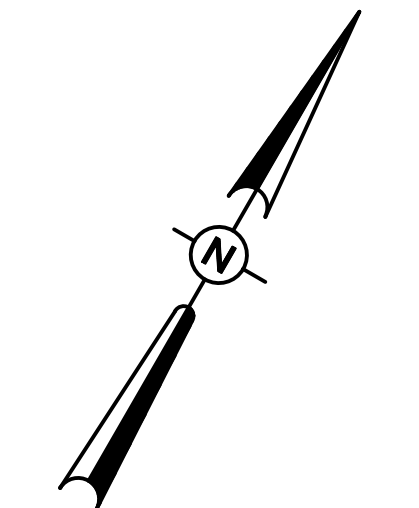
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 CAD FILE: C-103-CON.DWG
 DESIGN BY: HLE 05/03/2024
 DRAWN BY: HLE 05/03/2024
 REVIEWED BY: LDH 10/13/2024

PROPOSED CONSTRUCTION PLAN

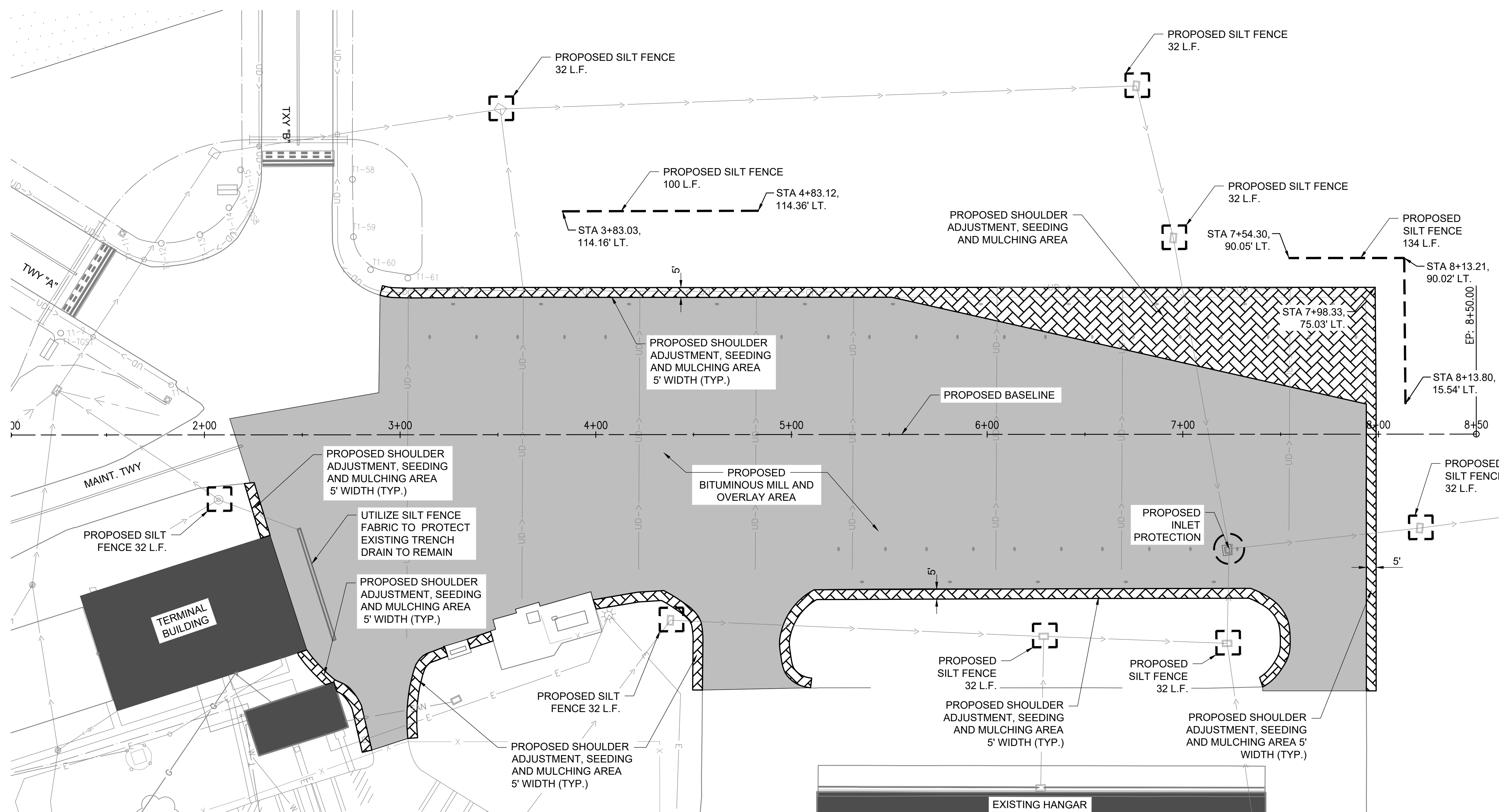
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0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

LEGEND

- SHOULDER ADJUSTMENT, SEEDING, AND MULCHING
- SILT FENCE
- INLET PROTECTION
- EXISTING UNDERDRAIN
- EXISTING AIRFIELD ELECTRIC
- EXISTING STORM SEWER
- EXISTING ELECTRIC
- EXISTING TELEPHONE
- EXISTING WATER
- EXISTING SANITARY SEWER
- EXISTING GAS



DATE SIGNED: _____ LICENSE EXPIRES: _____

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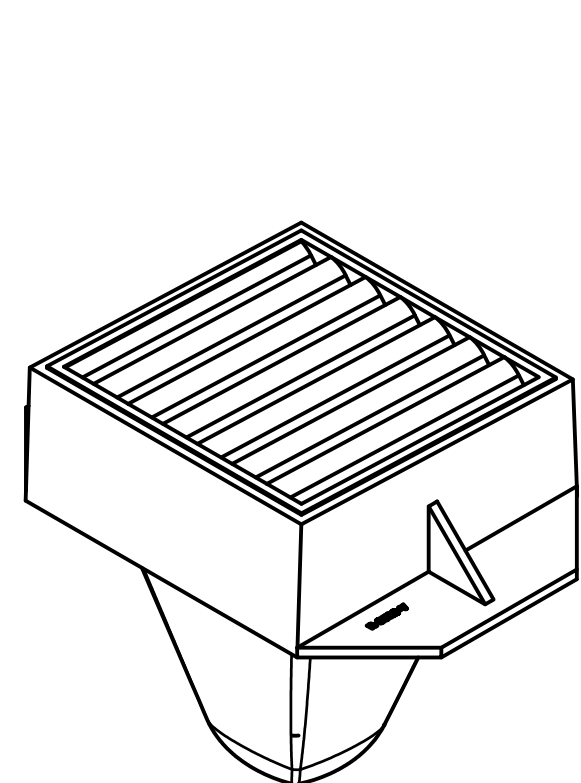
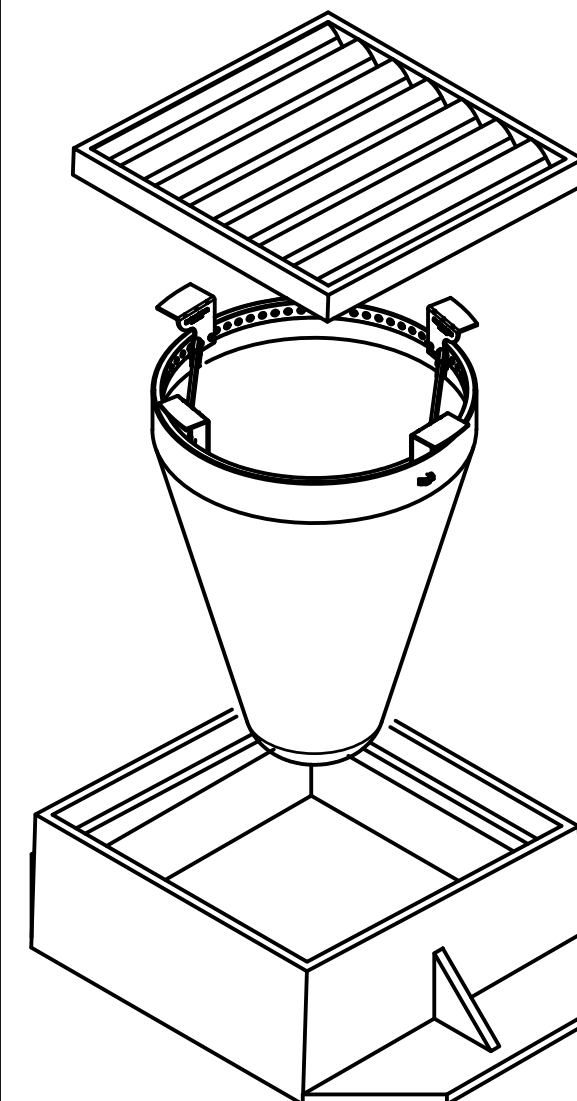
NO.	DATE	DESCRIPTION		
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PROJECT NO: 24A0057
CAD FILE: C-181-SWP.DWG
DESIGN BY: HLE 05/03/2024
DRAWN BY: HLE 05/03/2024
REVIEWED BY: LDH 10/13/2024

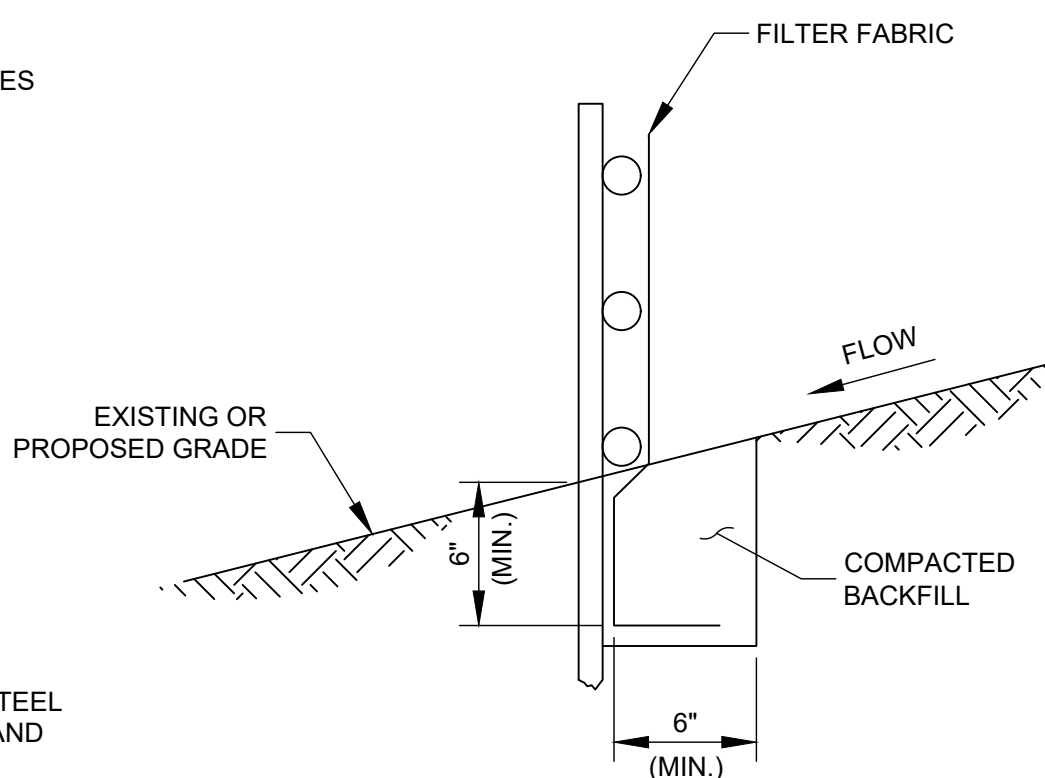
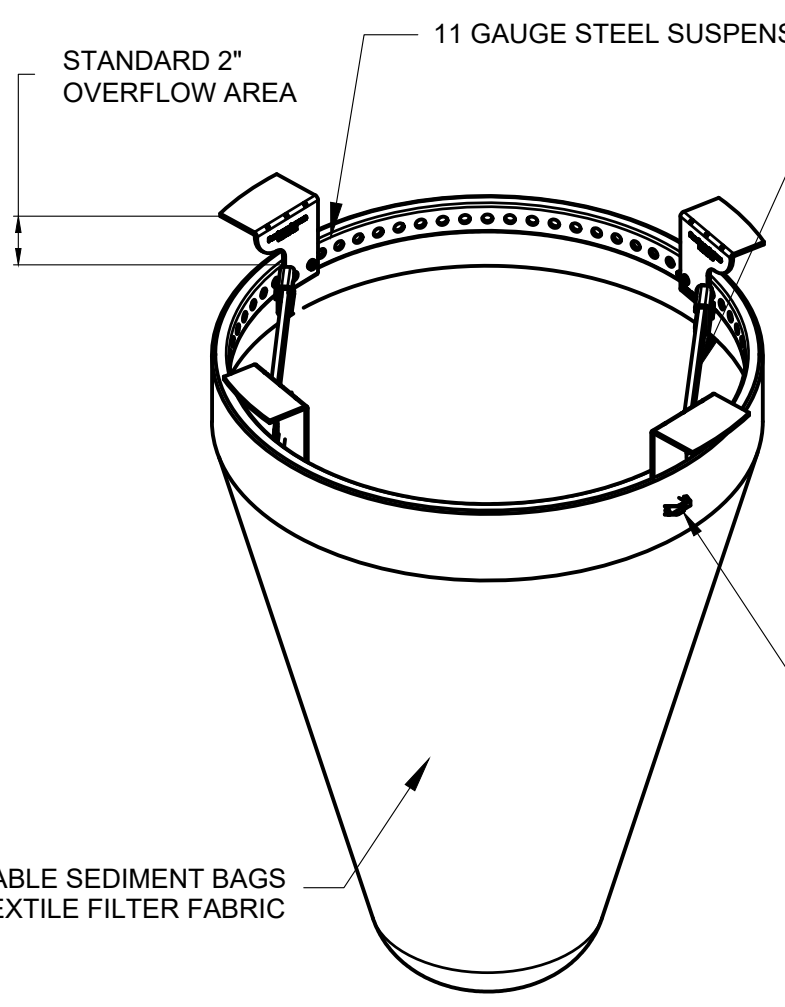
SHOULDER ADJUSTMENT AND EROSION CONTROL PLAN

SEDIMENTATION AND EROSION CONTROL NOTES:

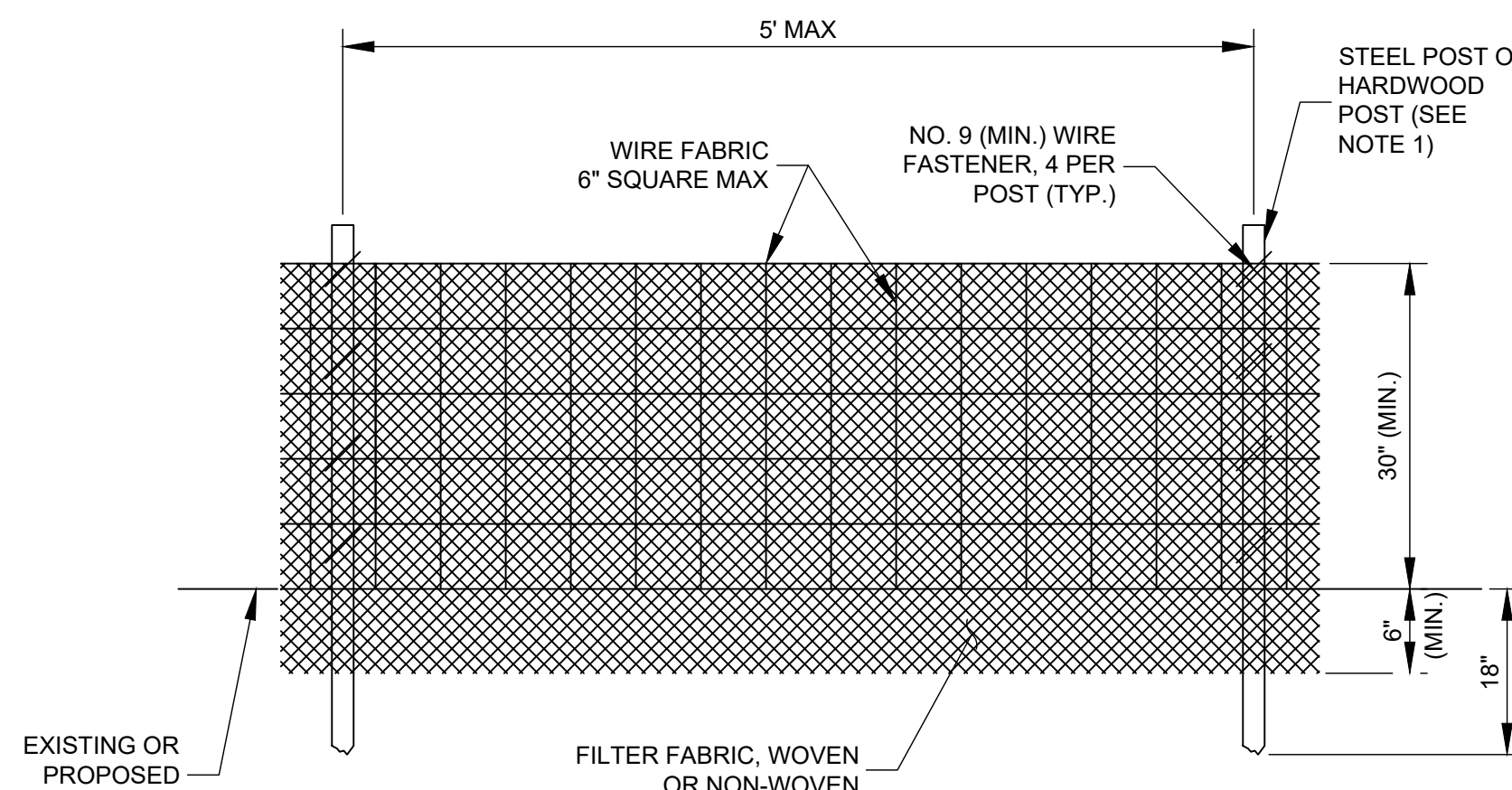
- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
 - UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
 - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- I. APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.



- INSTALLATION:**
1. REMOVE GRATE
 2. DROP FLEXSTORM INLET FILTER ONTO LOAD BEARING LIP OF CASTING OR CONCRETE STRUCTURE
 3. REPLACE GRATE



FABRIC ANCHOR DETAIL

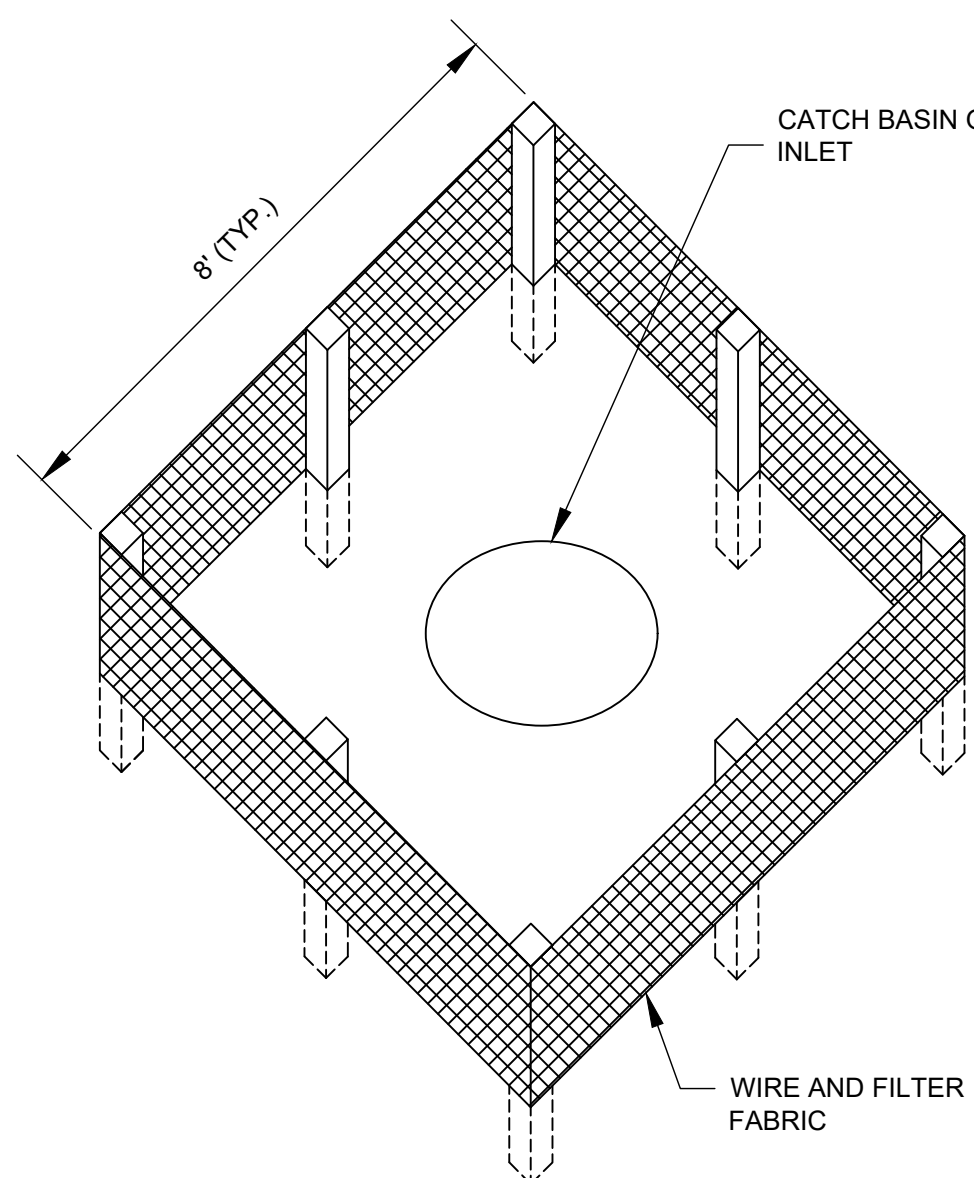


ELEVATION

NOTES:

1. FILTER FABRIC INLET PROTECTION SHALL CONSIST OF INLET BASKET AND FABRIC INSERT, FLEXSTORM CATCH-IT BY ADVANCED DRAINAGE SYSTEMS, FLOGARD TEMPORARY INLET FILTER BY OLDCASTLE, OR APPROVED EQUAL.
2. DEVICE SHALL BE EQUIPPED WITH AN OVERFLOW FEATURE SO DRAINAGE TO INLET IS NOT COMPLETELY BLOCKED IF DEVICE IS FULL OF SILT.
3. INLET BASKET IS AVAILABLE TO FIT ROUND, RECTANGULAR, BEEHIVE OR CURB INLET CASTINGS.
4. FILTER FABRIC SHALL HAVE AN APPARENT OPENING SIZE (AOS) OF AT LEAST 70 SIEVE FOR NONWOVEN.
5. FILTER FABRIC SHALL HAVE A GRAB TENSILE STRENGTH OF A LEAST 100 LBS FOR NON WOVEN.
6. POLYESTER OUTER REINFORCEMENT BAG SHALL HAVE FABRIC WITH A WEIGHT OF 4.55 OZ/SQYD +/- 15 PERCENT.
7. FRAME CONSTRUCTION SHALL HAVE A TENSILE STRENGTH OF AT LEAST 58,000 PSI AND A YIELD STRENGTH OF AT LEAST 36,000 PSI.
8. MAINTENANCE SHALL BE PERFORMED AS NEEDED. REMOVE SILT FROM FABRIC INSERT WHEN 50% OF CAPACITY IS REACHED. REMOVE SILT FROM INTERIOR AND EXTERIOR OF INLET DAM WHEN 50% OF DAM HEIGHT IS REACHED.
9. PAYMENT FOR INLET PROTECTION MAINTENANCE SHALL BE INCIDENTAL TO INLET PROTECTION.

INLET PROTECTION AT MANHOLES IN IMPERVIOUS AREAS



SILT FENCE AT MANHOLES IN PERVIOUS AREAS

NOTES:

1. FILTER FABRIC SHALL BE EMBEDDED 8" INTO THE SOIL.
2. INSPECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
3. SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS AT THE DIRECTION OF THE AIRPORT REPRESENTATIVE OR OWNER SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. CONTRACTOR SHALL PLACE SEED AND MULCH AROUND STRUCTURES PER LANDSCAPING PLAN. COST OF REMOVAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SILT FENCE.
4. AREAS DISTURBED OUTSIDE OF CONSTRUCTION LIMITS DURING PLACEMENT OF INLET PROTECTION TO BE RE-GRADED, SEEDED AND MULCHED, COST INCIDENTAL TO SILT FENCE.
5. FENCE AND POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
6. PAID UNDER AR156510 SILT FENCE

NOTES:

1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.j.1.f.i, AS AMENDED), OR EQUIVALENT.
7. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
8. SOIL DISTURBANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
9. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

STORM WATER POLLUTION PREVENTION NOTES

GENERAL
THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

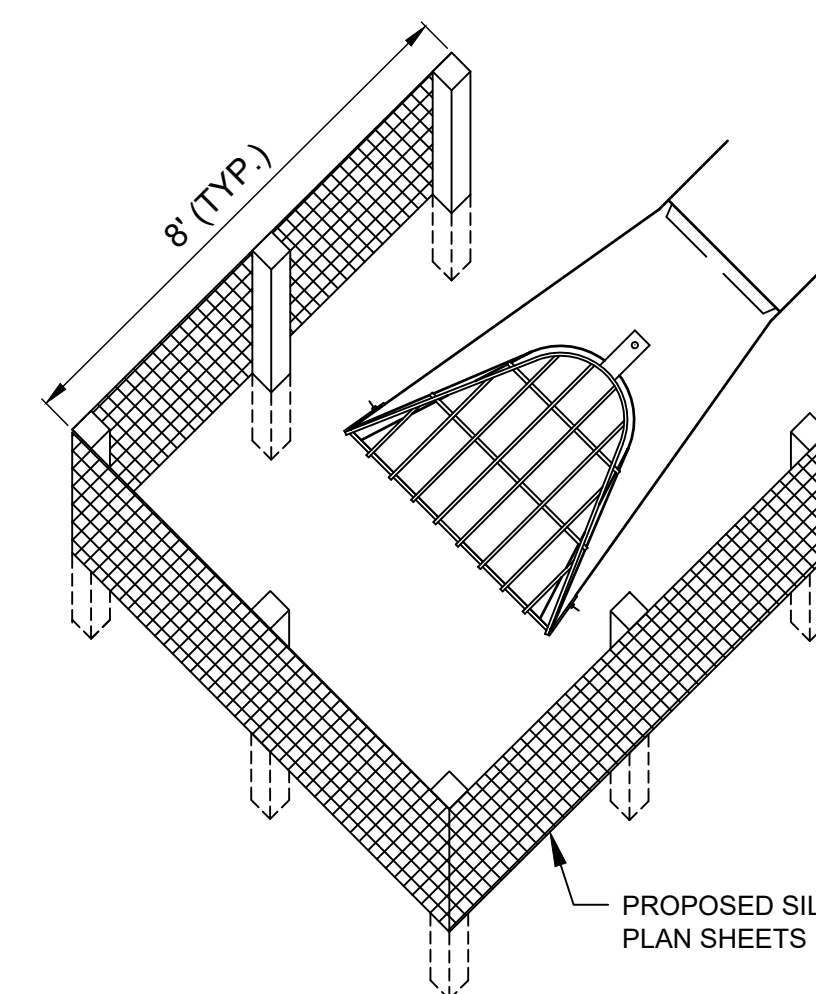
THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

POLLUTION PREVENTION MEASURES
THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.



SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)

DATE SIGNED:	LICENSE EXPIRES:
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REHABILITATE MAIN APRON

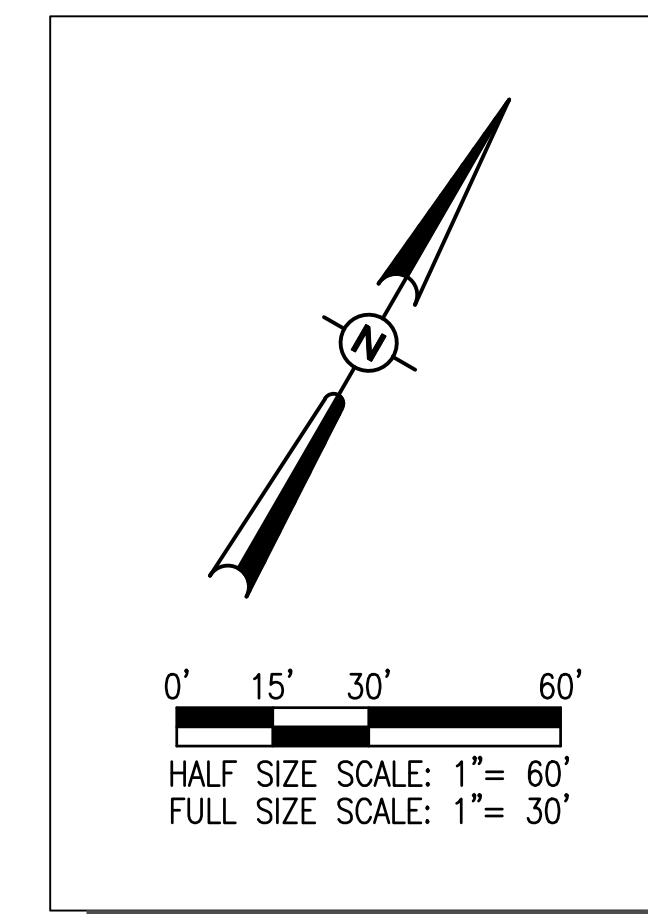
IDA No: 2H0-5148
SBG No.:
3-17-SBGP-171/184/197/TBD

Contract No. SE007

NO.	DATE	DESCRIPTION	DES	DWN	REV

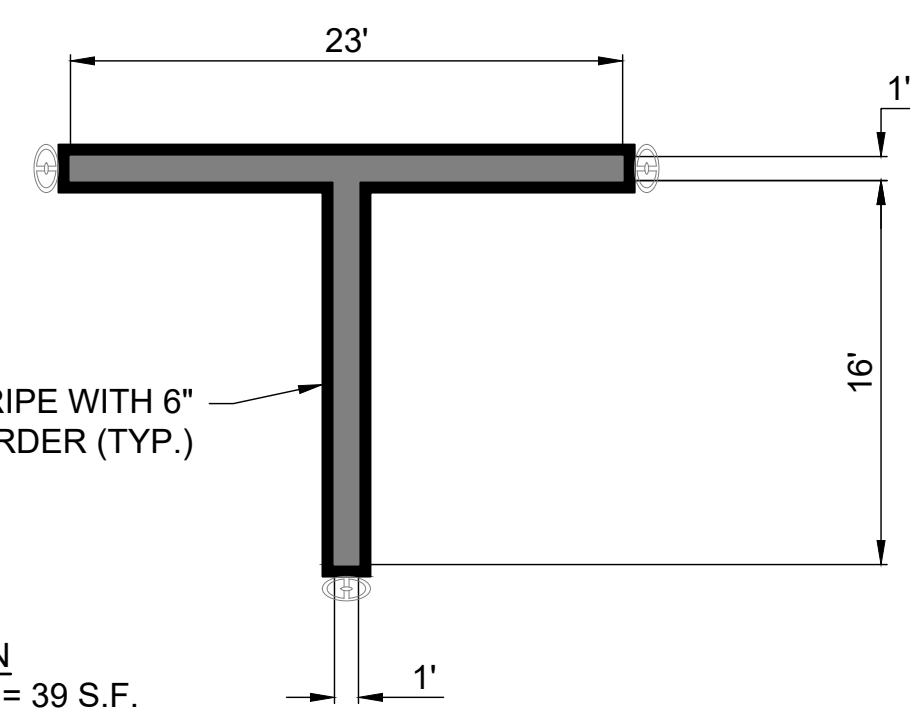
ISSUE: NOVEMBER 22, 2024
PROJECT NO: 24A0057
CAD FILE: C-501-SWP.DWG
DESIGN BY: HLE 05/03/2024
DRAWN BY: HLE 05/03/2024
REVIEWED BY: LDH 10/13/2024

EROSION CONTROL DETAILS

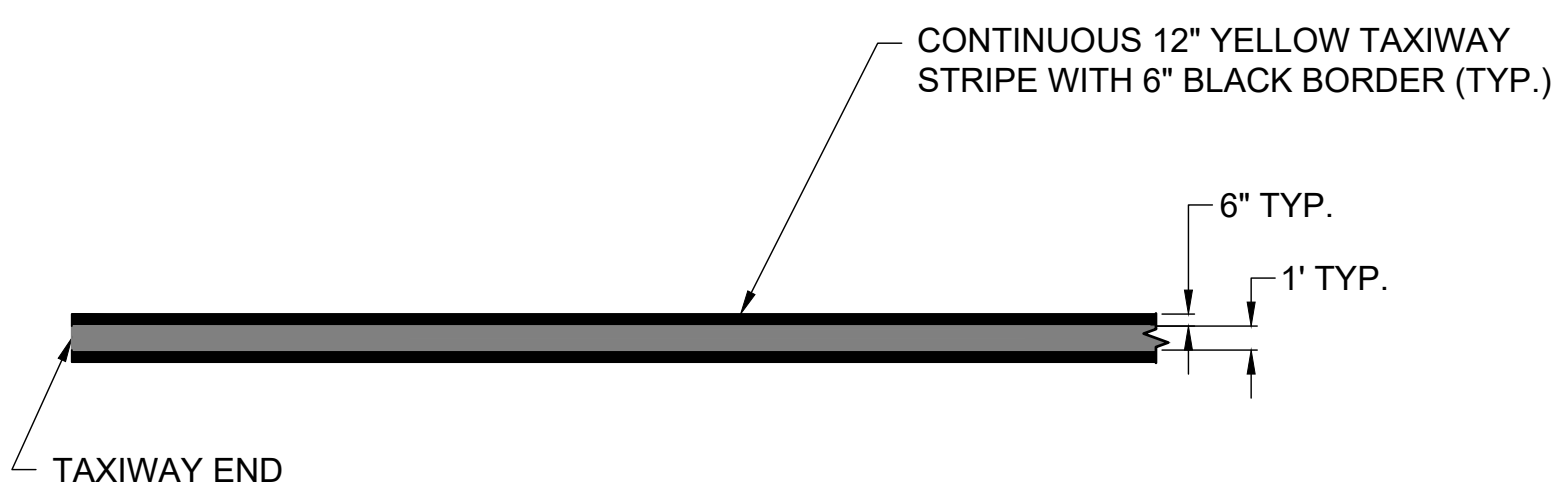


WATERBORNE PAVEMENT MARKING NOTES

1. AN INITIAL COAT OF TEMPORARY MARKINGS SHALL BE PLACED AT 50% OF THE NORMAL APPLICATION RATE. BLACK MARKINGS SHALL NOT BE PLACED AS PART OF THE INITIAL COAT.
2. A 30-DAY WAITING PERIOD WILL BE REQUIRED BEFORE PLACING THE FINAL APPLICATION OF MARKING PAINT.
3. WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS AND WILL BE APPLIED IN ONE APPLICATION.
4. GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
5. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
6. THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES.

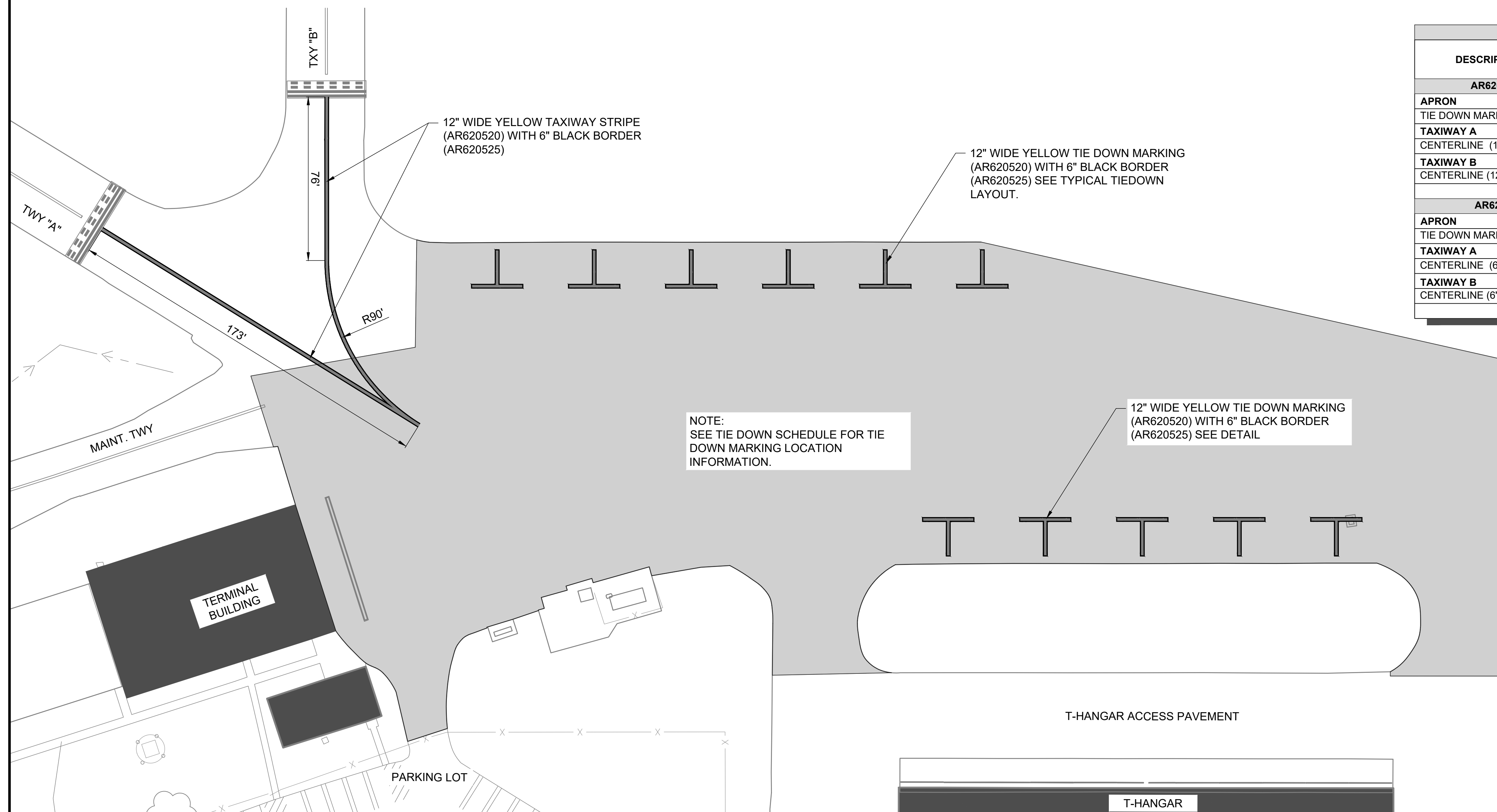


TYPICAL TIEDOWN LAYOUT
NOT TO SCALE



TAXIWAY CENTERLINE DETAIL
NOT TO SCALE

MARKING SCHEDULE			
DESCRIPTION	UNIT AREA (S.F.)	NO. REQUIRED	TOTAL AREA (S.F.)
AR620520 - PAVEMENT MARKING - WATERBORNE (YELLOW)			
APRON			
TIE DOWN MARKING (12")	39	11	429
TAXIWAY A			
CENTERLINE (12")	173	1	173
TAXIWAY B			
CENTERLINE (12")	168	1	168
		TOTAL	770
AR620525 - PAVEMENT MARKING - WATERBORNE (BLACK)			
APRON			
TIE DOWN MARKING (6")	41	11	451
TAXIWAY A			
CENTERLINE (6")	87	2	173
TAXIWAY B			
CENTERLINE (6")	84	2	168
		TOTAL	792



NOTE:
SEE TIE DOWN SCHEDULE FOR TIE
DOWN MARKING LOCATION
INFORMATION.

12" WIDE YELLOW TIE DOWN MARKING
(AR620520) WITH 6" BLACK BORDER
(AR620525) SEE DETAIL

DATE LICENSE
SIGNED: EXPIRES:

**REHABILITATE MAIN
APRON**

IDA No: 2H0-5148
SBG No.:
3-17-SBGP-171/184/197/TBD

Contract No. SE007

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 22, 2024
PROJECT NO: 24A0057
CAD FILE: C-102-MRK.DWG
DESIGN BY: HLE 05/03/2024
DRAWN BY: HLE 05/03/2024
REVIEWED BY: LDH 10/13/2024

**PROPOSED MARKING
PLAN**

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