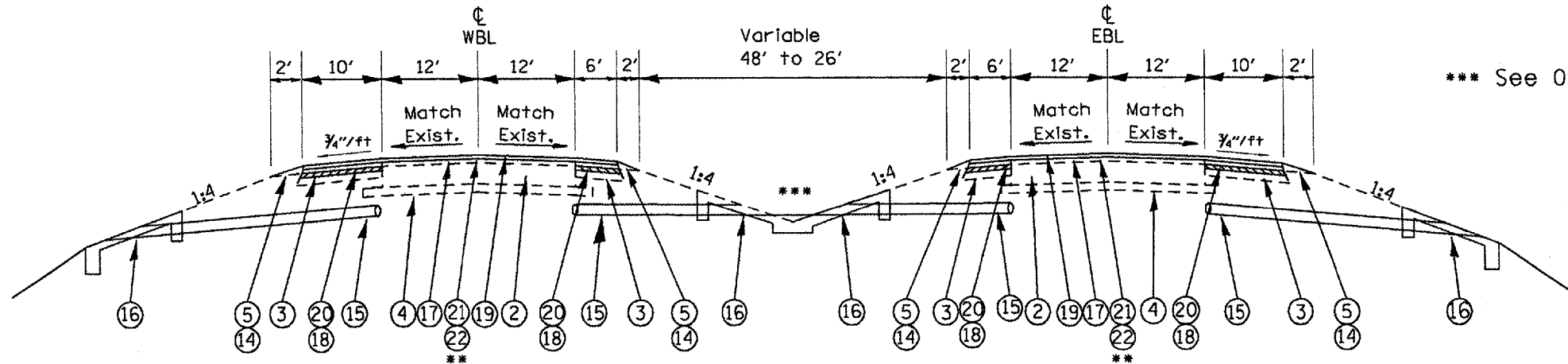
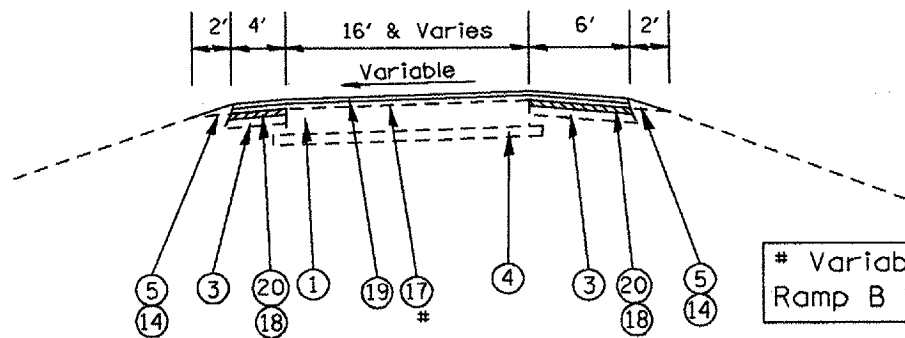


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	18RS,18HDI	KNOX	70	5
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

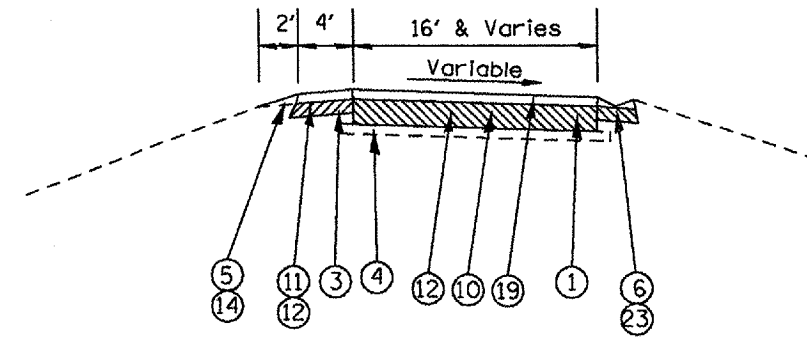


*** See Outlet Protector Detail Page 51-52

US 34
Sta. 363+00 to 477+00



US 34/Main St. Ramp A
Sta. 0+00 to 12+49
US 34/Main St. Ramp B
Sta. 1+30 to 23+32

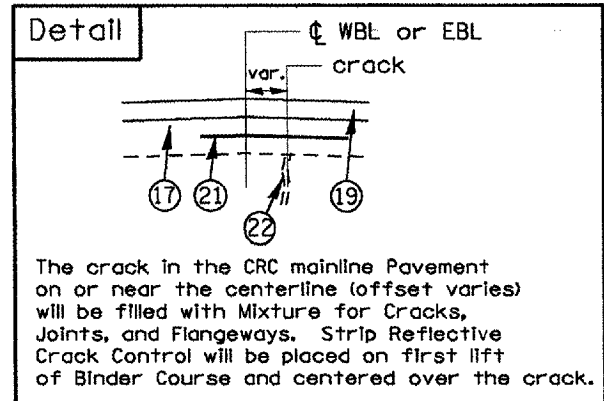


US 34/Main St. Ramp B
Sta. 0+29 to 1+30

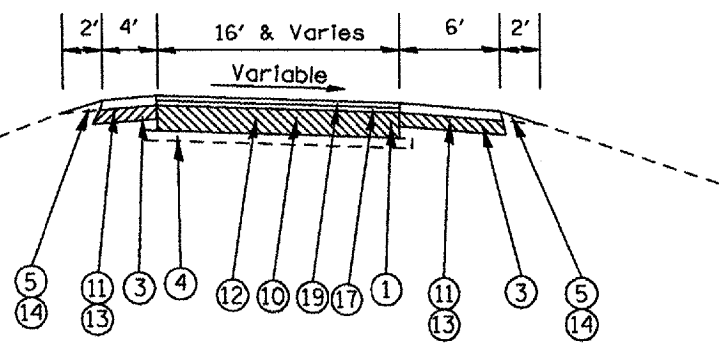
Variable Depth
Ramp B 1+30 to 2+72

- ① Existing PCC Pavement 10"
- ② Existing CRC Pavement 8"
- ③ Existing Bituminous Shoulders 8"
- ④ Existing Subbase Granular Material
- ⑤ Existing Aggregate Shoulder
- ⑥ Existing Concrete Curb and Gutter
- ⑩ Proposed Pavement Removal
- ⑪ Proposed Bituminous Shoulder Removal
- ⑫ Proposed PCC Pavement 10"
- ⑬ Proposed Hot-Mix Asphalt Shoulder 8"
- ⑭ Proposed Aggregate Shoulder, Type B
- ⑮ Proposed Pipe Underdrains & Pipe Underdrains (Special)*
- ⑯ Proposed Outlet Protector
- ⑰ Proposed Hot-Mix Asphalt Binder Course, IL-19.0, N70 2.25"
- ⑱ Proposed Hot-Mix Asphalt Shoulder 5.75"
- ⑲ Proposed Hot-Mix Surface Course, N70 1.5"
- ⑳ Proposed Hot-Mix Asphalt Surface Removal 2"
- ㉑ Proposed Strip Reflective Crack Control**
- ㉒ Proposed Mixture for Cracks, Joints, and Flangeways**
- ㉓ Proposed Type A Gutter

* See Plan Sheets for Diameter
** See Detail



The crack in the CRC mainline Pavement on or near the centerline (offset varies) will be filled with Mixture for Cracks, Joints, and Flangeways. Strip Reflective Crack Control will be placed on first lift of Binder Course and centered over the crack.



US 34/Main St. Ramp A
Sta. 12+49 to 14+89

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

Typical Sections
not to scale

SCALE: VERT.
DATE

DRAWN BY
CHECKED BY