

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	2
STA.		TO STA.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* FA155(I-55), FA172(I-72), FAP666(I-55 BUS)				
** D6 INTERSTATE RS,BR,M CAB 2008				

**GENERAL NOTES**

- ALL OF THE DISTURBED AREAS WITHIN THE RIGHT OF WAY NOT COVERED BY SURFACING MATERIAL SHALL BE SEEDED.
- CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION PROTECTION FOR THE DURATION OF THIS PROJECT.
- SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE JULIE NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED. ANY DAMAGE TO THE UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
- THE LOCATIONS OF THOSE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTIONS PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVEGROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- THE THICKNESS OF HOT-MIX ASPHALT SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- BEFORE ORDERING PIPE CULVERTS, PIPE DRAINS, OR STORM SEWER, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.
- FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
- THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05	TONS/CU YD
HOT-MIX ASPALT MATERIAL (PRIME COAT)	0.00038	TON/SQ YD
HOT-MIX ASPHALT SURFACE CSE	112	LBS/SQ YD/IN
HOT-MIX ASPHALT BINDER CSE	112	LBS/SQ YD/IN
AGGREGATE (PRIME COAT)	0.002	TON/SQ YD
RIPRAP	1.5	TON/CU YD
AGRICULTURAL GROUND LIMESTONE	2.00	TON/ACRE

- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- UNLESS NOTED OTHERWISE, STATIONS AND OFFSETS REFER TO CENTERLINE OF PROJECT.
- LAYOUT OF RIPRAP MAY BE VARIED IN THE FIELD TO SUIT GROUND CONDITIONS AS DIRECTED BY THE ENGINEER.
- IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- THE GRADING AND SHAPING OF THE DITCHES SHALL BE DONE IN SUCH A MANNER TO ESTABLISH POSITIVE FLOW BETWEEN DRAINAGE STRUCTURES, OR BETWEEN THE EXISTING ELEVATION AT THE UPSTREAM END OF THE GRADING TO THE DOWNSTREAM DRAINAGE STRUCTURE.
- GUARDRAIL REMOVAL WILL INCLUDE THE REMOVAL OF ANY CONCRETE ENCASUREMENTS OF THE POSTS AND ANY ADDITIONAL RAILING LOCATED AT THE BOTTOM OF THE POSTS.
- ANY REFERENCES OR CALL-OUTS IN THESE PLANS TO "BITUMINOUS CONCRETE" SHALL BE INTERPRETED TO MEAN "HOT-MIX ASPHALT".

**COMMITMENTS**

COMMITMENT TO NOT DISTURB TRACY MOYER'S TREES AND OTHER LANDSCAPING WHICH ARE ON PRIVATE PROPERTY IN THE NE QUADRANT OF SOUTH GRAND INTERCHANGE.

COMMITMENT TO LAKESIDE CHURCH TO REMOVE SMALL PATCH OF TREES ON STATE ROW IN FRONT OF CHURCH ALONG I-55. SEE PLAN SHEETS NEAR RT STA 138+50

IN FENCE REPLACEMENT AREAS ADJACENT TO RESIDENCES, EFFORT WILL BE MADE TO SAVE AS MANY TREES AS POSSIBLE, AS DIRECTED BY THE ENGINEER.

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△ \* 193A, -193D. SPBGR DETAILS

**DISTRICT SIX**

EXAMINED October 17 2007  
*Sam Hoar*  
**OPERATIONS ENGINEER**

EXAMINED Oct 19 2007  
*W.B. Jung*  
**PROGRAM IMPLEMENTATION ENGINEER**

EXAMINED Oct 19 2007  
*W.B. Jung*  
**PROGRAM DEVELOPMENT ENGINEER**

The following mixture requirements are applicable for this project:

Mixture Use(s)	Polymerized Hot-Mix Asphalt Surface Course, Mix "E", N105	Polymerized Hot-Mix Asphalt Binder Course, IL-19.0, N105	Hot-Mix Asphalt Surface Course, Mix "D", N50	Hot-Mix Asphalt Surface Course, Mix "C", N30	Hot-Mix Asphalt Base Course Widening, 12"	Hot-Mix Asphalt Base Course, 8"	Hot-Mix Asphalt Shoulders, 4"
AC/PG:	SBS PG 76-22	SBS PG 70-22	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 58-22
Design Air Voids:	4.0% @ N DESIGN =105	4.0% @ N DESIGN =105	4.0% @ N DESIGN =50	3.0% @ N DESIGN =30	4.0% @ N DESIGN =50	4.0% @ N DESIGN =50	2.0% @ N DESIGN =30
Mixture Composition (Gradation Mixture)	IL 9.5 OR 12.5	IL 19.0	IL 9.5 OR 12.5	IL 9.5L	IL 19.0	IL 19.0	BAM
Friction Aggregate:	MIX "E"	N/A	MIX "D"	MIX "C"	N/A	N/A	N/A

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**GENERAL NOTES**  
 FA155(I-55), FA172(I-72), FAP666(I-55 BUS)  
 SECTION D6 INTERSTATE RS,BR,M CAB 2008  
 SANGAMON COUNTY

SCALE: \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
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△ Rev. 1-B-08

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	6
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
* FA155(I-55), FA172(I-72), FAP666(I-55 BUS)				
** D6 INTERSTATE RS,BR,M CAB 2008				

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNITS	TOTAL QUANTITY	CONSTRUCTION TYPE CODE					
				1000-2A	SFTY-4A	SFTY-2A	SFTY-2A	SFTY-2A	SFTY-2A
55034500	STORM SEWERS, TYPE 1, REINFORCED CONCRETE ELLIPTICAL PIPE, SPAN 38, RISE 24	FOOT	1,192	1,192					
58100200	WATERPROOFING MEMBRANE SYSTEM	SO YD	1,527				1,027	500	
60219000	MANHOLES, TYPE A, 4' -DIAMETER, TYPE 8 GRATE	EACH	2	2					
60225100	RESTRICTED DEPTH MANHOLES, 4' -DIAMETER, TYPE 8 GRATE	EACH	5	5					
60260100	INLETS TO BE ADJUSTED	EACH	1	1					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	2	2					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	4	4					
60500060	REMOVING INLETS	EACH	1	1					
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	5,262.5	5,262.5					
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	9	9					
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	1	1					
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4					
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	29	29					
63200310	GUARDRAIL REMOVAL	FOOT	4,547.5	4,547.5					
63300575	REMOVE AND RE-ERECT RAIL ELEMENT OF EXISTING GUARD RAIL	FOOT	18,650	18,650					
63400105	GUARD POSTS	EACH	8	8					
63500105	DELINEATORS	EACH	774	774					
64200105	SHOULDER RUMBLE STRIP	FOOT	190,468	190,468					
66400105	CHAIN LINK FENCE, 4'	FOOT	60,118	60,118					
66410300	CHAIN LINK FENCE REMOVAL	FOOT	25,758	25,758					
66502300	WOVEN WIRE FENCE REMOVAL	FOOT	34,360	34,360					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	10	10					
67100100	MOBILIZATION	L SUM	1	1					
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	32	32					
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1					
70100815	TRAFFIC CONTROL AND PROTECTION, STANDARD 701446	L SUM	1	1					
70101205	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1					1	
70101605	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402 (SPECIAL)	EACH	2			1	1		
70103710	TRAFFIC CONTROL FOR RAMPS	L SUM	1	1					
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	15	6	5	2	2		
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1					1	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	67	67					

ACIM

ACHSIP

ACIM

HMA ROADWAY  
90% FEDERAL  
10% STATE  
URBAN

CABLE  
MEDIAN  
BARRIER  
90% FEDERAL  
10% STATE  
URBAN

STRUCTURE  
084-0028  
NB BL-55  
90% FEDERAL  
10% STATE  
URBAN

STRUCTURE  
084-0076  
WB I-72  
90% FEDERAL  
10% STATE  
URBAN

STRUCTURE  
084-0077  
EB I-72  
90% FEDERAL  
10% STATE  
URBAN

STRUCTURE  
084-0092  
W LAKE  
SHORE DR  
90% FEDERAL  
10% STATE  
URBAN

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUMMARY OF QUANTITIES**  
 FA155(I-55), FA172(I-72), FAP666(I-55 BUS)  
 SECTION D6 INTERSTATE RS,BR,M CAB 2008  
 SANGAMON COUNTY

SCALE: \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 DATE \_\_\_\_\_ CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	40
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
* FAI55(I-55), FAI72(I-72), FAP666(I-55 BUS)				
** D6 INTERSTATE RS,BR,M CAB 2008				

SHOULDER RUMBLE STRIP

LOCATION	LENGTH (FOOT)
<b>I-72 MAINLINE</b>	
<b>EASTBOUND OUTSIDE SHOULDER</b>	
STA 627+00.00 TO STA 635+79.29 BK	879.29
STA 634+03.49 AH TO STA 652+57.06	1,853.57
STA 659+20.00 TO STA 671+02.00	1,182.00
STA 678+14.40 TO STA 678+44.00	29.60
STA 678+44.00 TO STA 684+54.96	610.96
STA 688+01.96 TO STA 694+78.00	676.04
<b>EASTBOUND INSIDE SHOULDER</b>	
STA 627+00.00 TO STA 635+79.29 BK	879.29
STA 634+03.49 AH TO STA 673+12.11	3,908.62
STA 675+04.78 TO STA 684+54.96	950.18
STA 688+01.96 TO STA 694+78.00	676.04
<b>WESTBOUND OUTSIDE SHOULDER</b>	
STA 627+00.00 TO STA 635+79.29 BK	879.29
STA 634+03.49 AH TO STA 657+60.37	2,356.88
STA 669+30.00 TO STA 673+13.13	383.13
STA 677+20.00 TO STA 684+19.00	699.00
<b>WESTBOUND INSIDE SHOULDER</b>	
STA 627+00.00 TO STA 635+79.29 BK	879.29
STA 634+03.49 AH TO STA 673+13.13	3,909.64
STA 675+05.80 TO STA 691+08.25	1,602.45
<b>I-55 MAINLINE</b>	
<b>NORTHBOUND OUTSIDE SHOULDER</b>	
STA 298+62.00 TO STA 281+62.00	1,700.00 *
STA 274+40.00 TO STA 236+42.00	3,798.00 *
STA 229+50.00 TO STA 209+02.00	2,048.00 *
STA 198+14.00 TO STA 167+19.74 AH	3,094.26 *
STA 0+00.00 BK TO STA 9+97.00	997.00 *
STA 9+97.00 TO STA 128+87.38 BK	11,890.38
STA 128+91.29 AH TO STA 171+00.60	4,209.31
STA 175+82.00 TO STA 182+68.00	686.00
STA 194+76.00 TO STA 233+44.43 BK	3,868.43
STA 233+52.70 AH TO STA 259+78.00	2,625.30
STA 265+30.00 TO STA 271+56.00	626.00
STA 279+84.00 TO STA 286+74.00	690.00
STA 297+35.71 TO STA 315+70.88	1,835.17
STA 322+25.00 TO STA 330+83.00	858.00
STA 337+25.00 TO STA 343+95.00	670.00
STA 354+63.47 TO STA 355+49.48 BK	86.01
STA 695+67.32 AH TO STA 693+00.00	267.32

SHOULDER RUMBLE STRIP

LOCATION	LENGTH (FOOT)
<b>NORTHBOUND INSIDE SHOULDER</b>	
STA 298+62.00 TO STA 281+62.00	1,700.00 *
STA 274+40.00 TO STA 167+19.74 AH	10,720.26 *
STA 0+00.00 BK TO STA 9+97.00	997.00 *
STA 9+97.00 TO STA 27+77.96	1,780.96
STA 27+77.96 TO STA 63+75.00	3,597.04
STA 63+75.00 TO STA 128+87.38 BK	6,512.38
STA 128+91.29 AH TO STA 233+44.43 BK	10,453.14
STA 233+52.70 AH TO STA 250+04.00	1,651.30
STA 286+04.00 TO STA 355+49.48 BK	6,945.48
STA 695+67.32 AH TO STA 693+00.00	267.32
<b>SOUTHBOUND OUTSIDE SHOULDER</b>	
STA 293+45.00 TO STA 281+62.00	1,183.00 *
STA 274+40.00 TO STA 239+45.00	3,495.00 *
STA 228+48.00 TO STA 207+21.00	2,127.00 *
STA 200+07.00 TO STA 167+19.74 AH	3,287.26 *
STA 0+00.00 BK TO STA 13+59.00	1,359.00 *
STA 13+59.00 TO STA 31+22.04	1,763.04
STA 34+95.41 TO STA 62+07.00	2,711.59
STA 64+19.00 TO STA 86+63.86 BK	2,244.86
STA 82+99.33 AH TO STA 128+87.38 BK	4,588.05
STA 128+91.29 AH TO STA 163+18.00	3,426.71
STA 174+02.00 TO STA 181+27.00	725.00
STA 187+95.91 TO STA 233+44.43 BK	4,548.52
STA 233+52.70 AH TO STA 253+81.91	2,029.21
STA 264+83.00 TO STA 271+25.00	642.00
STA 278+87.00 TO STA 287+33.00	846.00
STA 293+54.97 TO STA 312+52.40	1,897.43
STA 323+12.00 TO STA 329+37.00	625.00
STA 336+14.00 TO STA 344+89.00	875.00
STA 351+24.04 TO STA 355+49.48 BK	425.44
STA 695+67.32 AH TO STA 693+00.00	267.32
<b>SOUTHBOUND INSIDE SHOULDER</b>	
STA 293+45.00 TO STA 281+62.00	1,183.00 *
STA 274+40.00 TO STA 167+19.74 AH	10,720.26 *
STA 0+00.00 BK TO STA 13+59.00	1,359.00 *
STA 13+59.00 TO STA 86+63.86 BK	7,304.86
STA 82+99.33 AH TO STA 128+87.38 BK	4,588.05
STA 128+91.29 AH TO STA 233+44.43 BK	10,453.14
STA 233+52.70 AH TO STA 250+04.00	1,651.30
STA 286+04.00 TO STA 355+49.48 BK	6,945.48
STA 695+67.32 AH TO STA 693+00.00	267.32
<b>TOTAL</b>	<b>190,468.17</b>

\* THE EXACT STATIONING OF THESE LOCATIONS MAY VARY IN THE FIELD AS DIRECTED BY THE ENGINEER.

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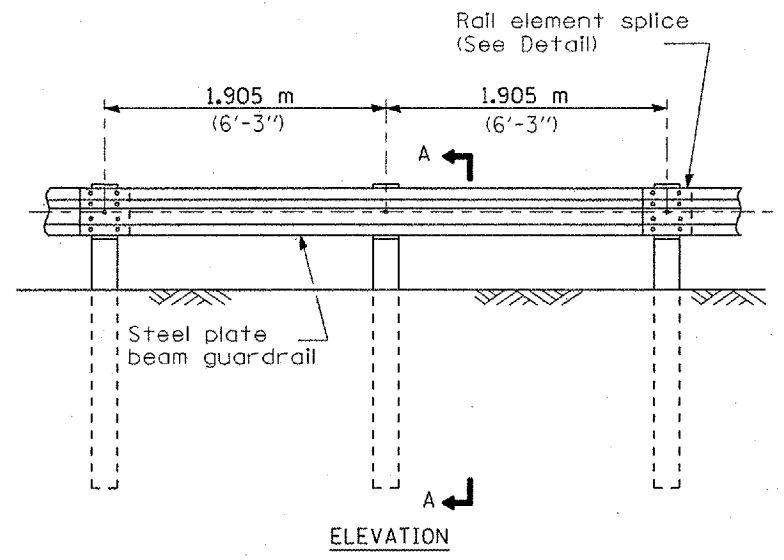
Rev. 1-8-08

REVISIONS		DATE
NAME		

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SCHEDULE OF QUANTITIES**  
 FAI55(I-55), FAI72(I-72), FAP666(I-55 BUS)  
 SECTION D6 INTERSTATE RS,BR,M CAB 2008  
 SANGAMON COUNTY

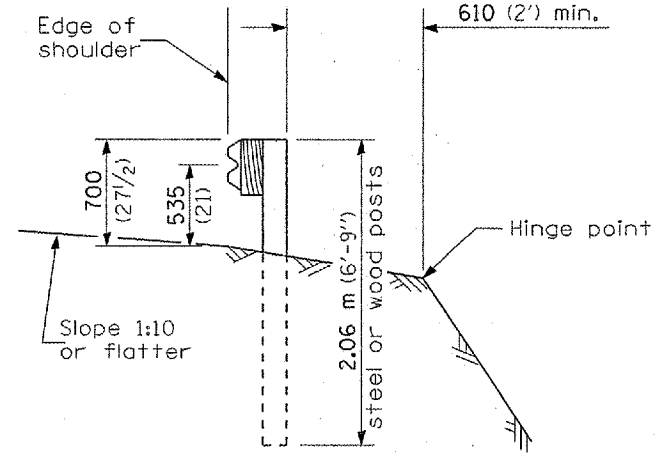
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	193A
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
* FA155(I-55), FA172(I-72), FAP666(I-55 BUS)				
** D6 INTERSTATE RS,BR,M CAB 2008				

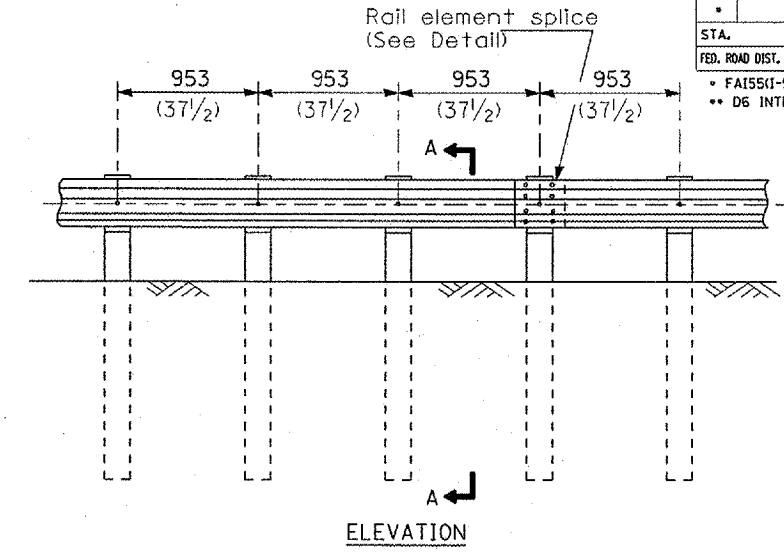


**TYPE A**

1.905 m (6'-3") Typical post spacing

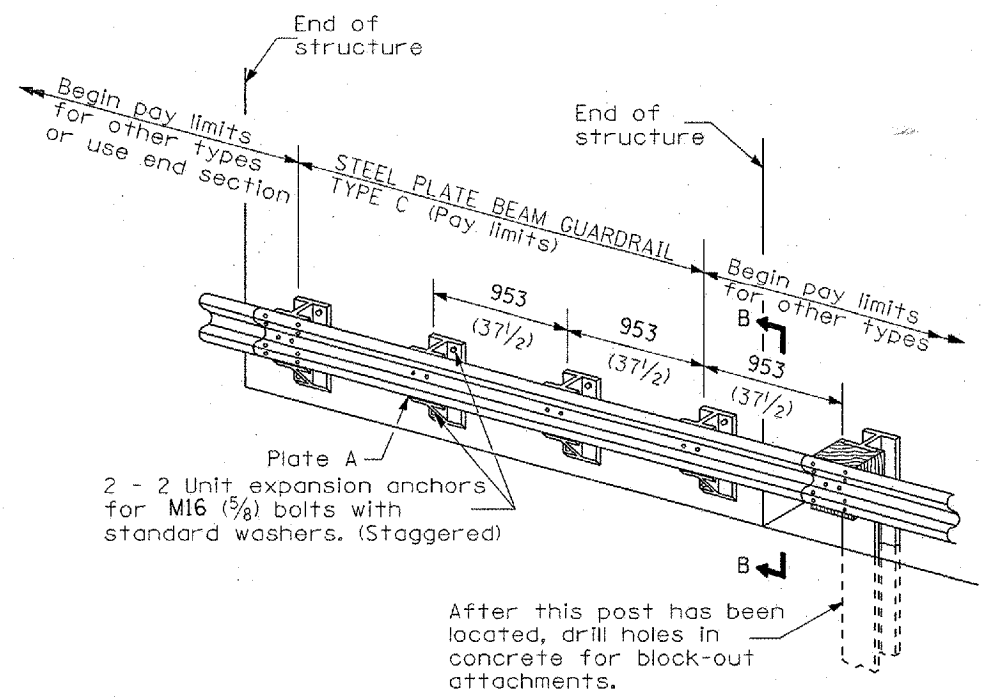


**SECTION A-A**



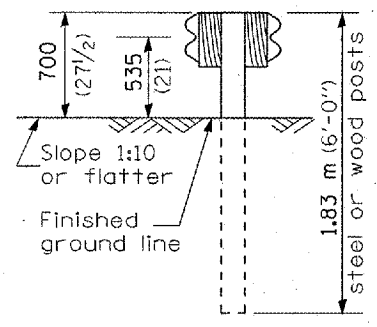
**TYPE B**

953 (37 1/2) Closed post spacing

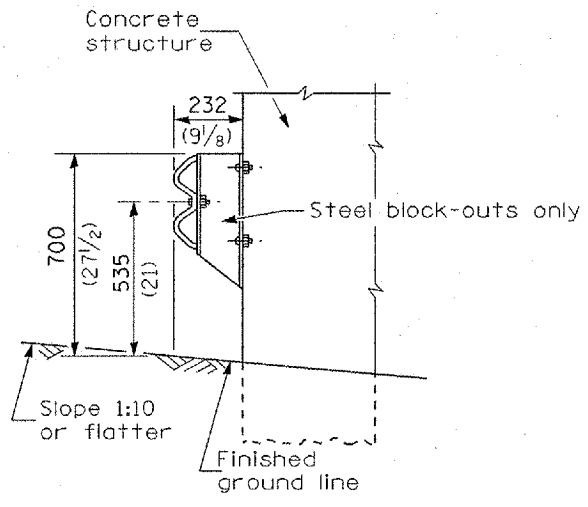


**TYPE C**

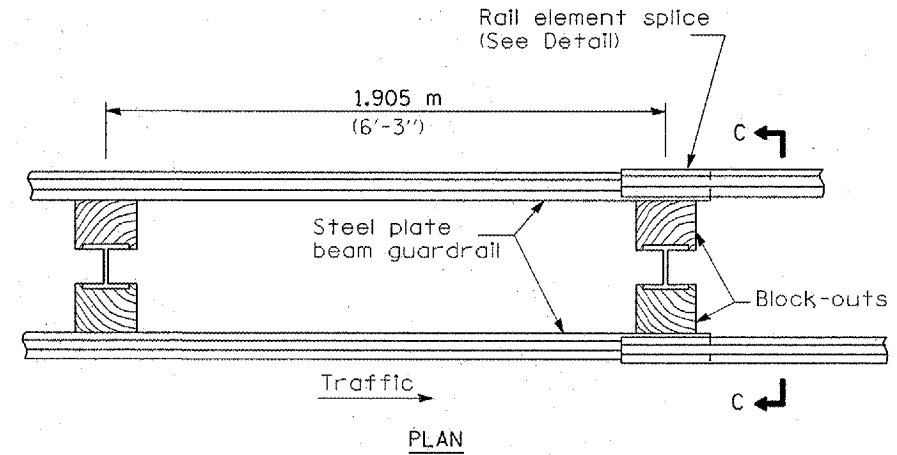
953 (37 1/2) Block-out spacing



**SECTION C-C**



**SECTION B-B**



**TYPE D**

Double steel plate beam guardrail  
1.905 m (6'-3") typical post spacing

**GENERAL NOTES**

**NOTE:**

THIS DETAIL SHALL BE USED FOR GUARDRAIL REPAIRS WHERE NEW STEEL PLATE BEAM GUARDRAIL WILL BE TIED INTO EXISTING STEEL PLATE BEAM GUARDRAIL, AS WELL AS FOR REMOVING AND RE-ERECTING THE RAIL ELEMENT.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

All dimensions are in millimeters (inches) unless otherwise shown.

The existing steel posts may be drilled to match the bolt pattern shown herein for the wood block-out, or a new steel post shall be provided.

This detail is applicable to the guardrail system used prior to January 1, 2007. For details on the Midwest Guardrail System, see Standard 630001.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

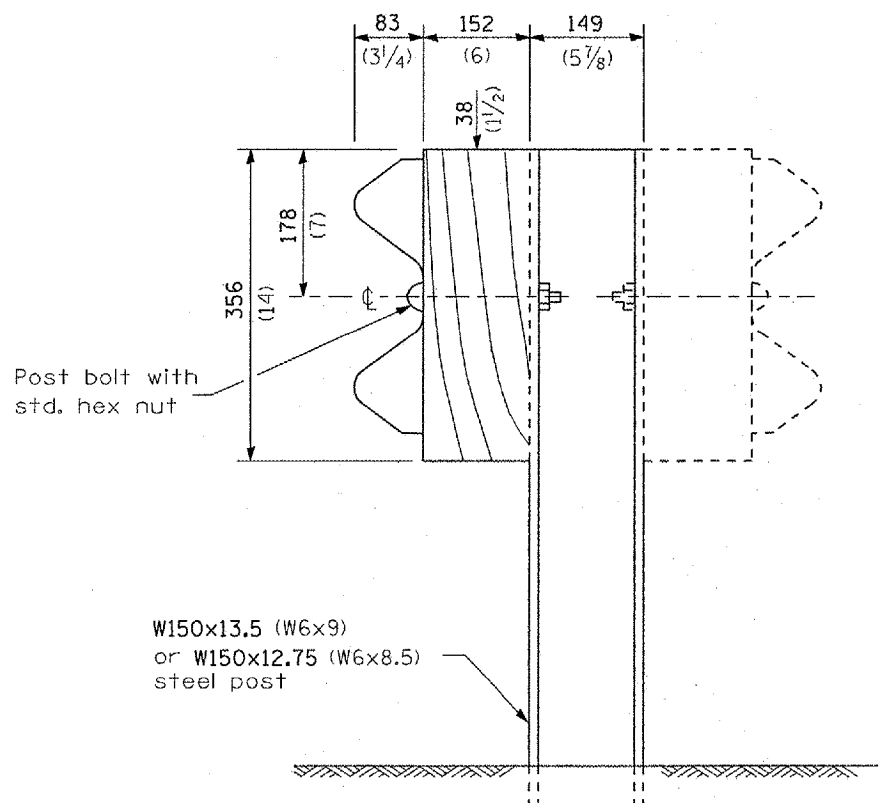
**STEEL PLATE BEAM GUARDRAIL DETAILS**  
(Sheet 1 of 4)

SCALE: VERT. DATE: HORIZ. DRAWN BY: CHECKED BY:

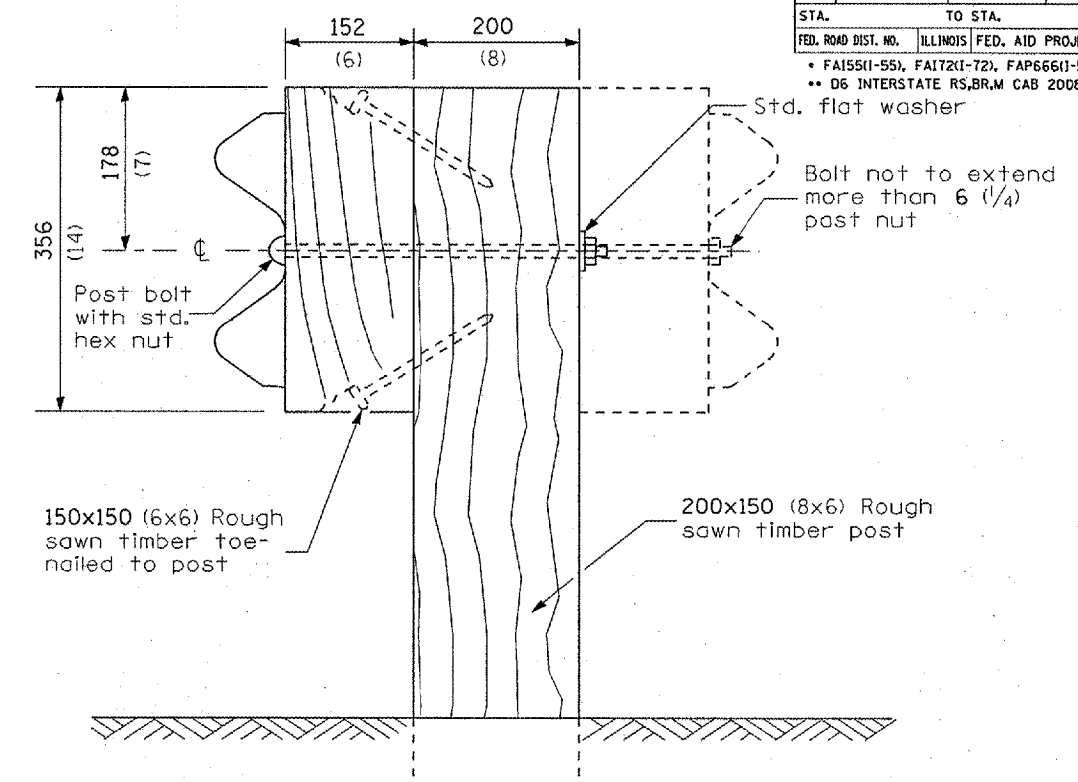
Added Sheet 1-B-08

FILE DATE = 1-10-08-2008 07:06:55AM  
FILE NAME = C:\Users\jcm\Documents\Steel-Beam-Guardrail.dgn  
PLOT SCALE = 0.8534 / IN.  
USER NAME = jcm

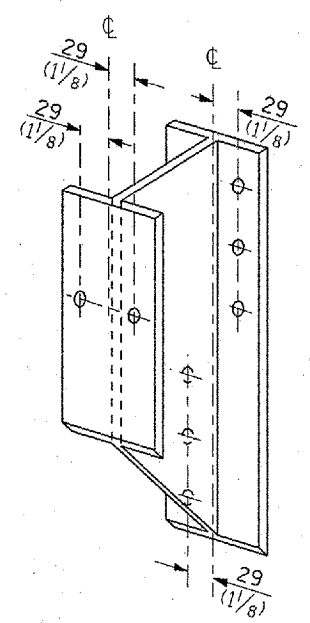
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	193B
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
• FA155(I-55), FA172(I-72), FAP666(I-55 BUS) •• D6 INTERSTATE RS,BR,M CAB 2008				



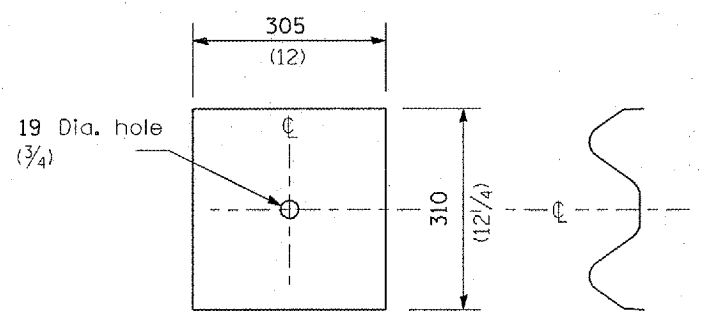
STEEL POST CONSTRUCTION



WOOD POST CONSTRUCTION



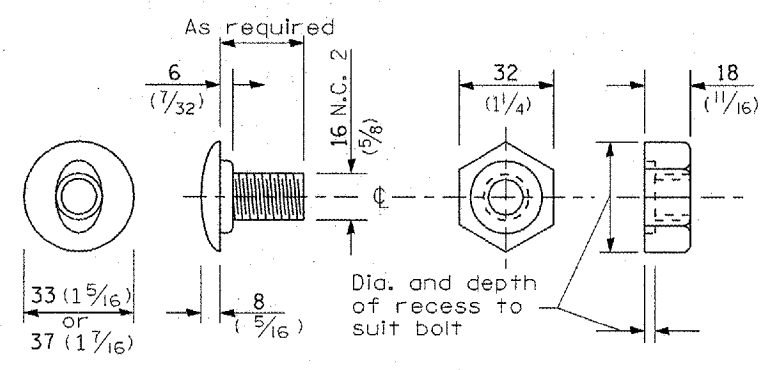
STEEL BLOCK-OUT DETAIL



NOTE

Plate A shall be placed between rail element and block-out at non-splice mounting points only when steel block-outs are used.

PLATE A



POST OR SPLICE BOLT & NUT

REVISIONS	
NAME	DATE

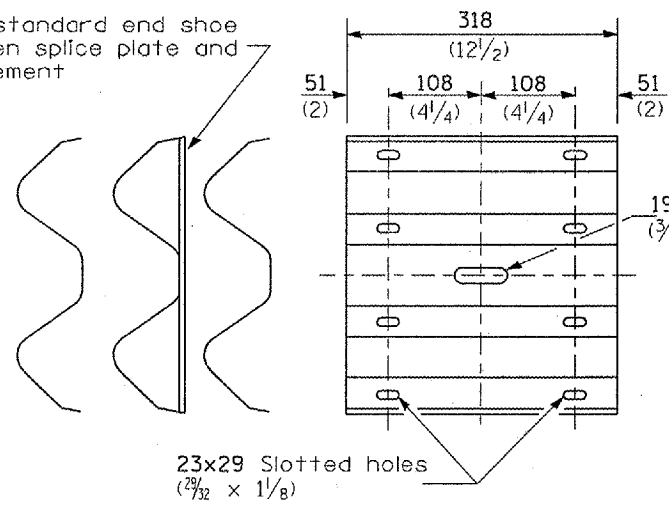
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STEEL PLATE BEAM  
 GUARDRAIL DETAILS**  
 (Sheet 2 of 4)  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

Added Sheet 1-8-08

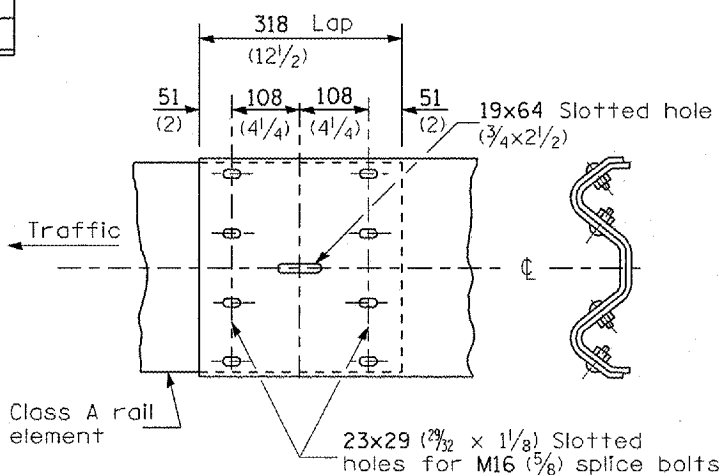
PLOT DATE: Jan-08-2008 07:05:14 AM  
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 PLOT SCALE: 0.8000 / IN.  
 USER NAME: laughton1

F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
**	**	SANGAMON	248	193C
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
• FA155(1-55), FA172(1-72), FAP66(1-55 BUS) • D6 INTERSTATE RS,BR,M CAB 2008				

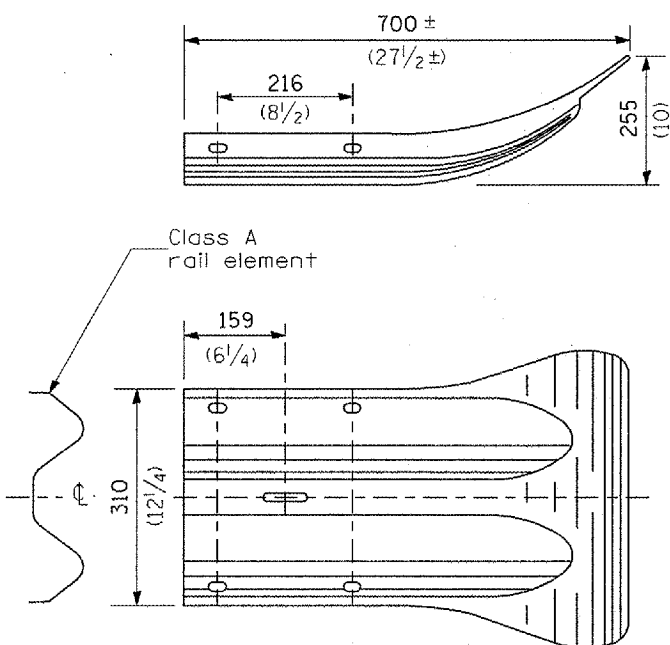
Place standard end shoe between splice plate and rail element



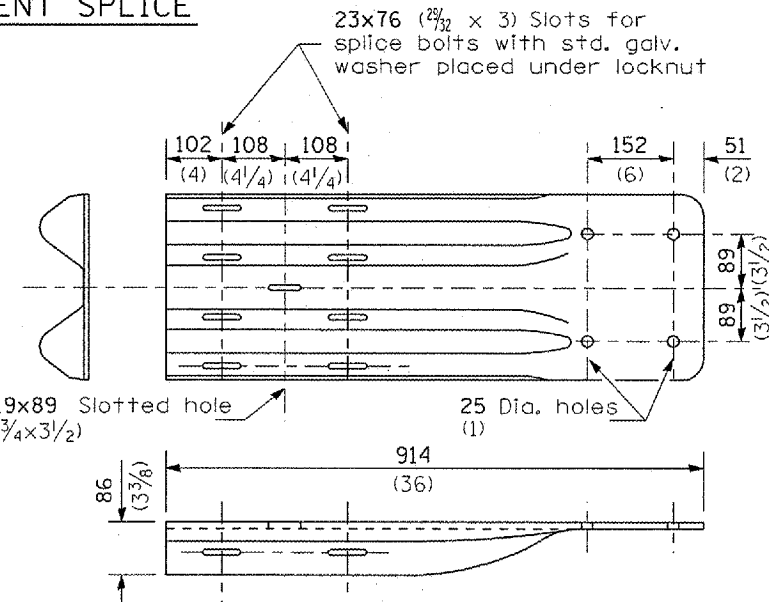
**SPLICE PLATE**



**RAIL ELEMENT SPLICE**



**END SECTION**



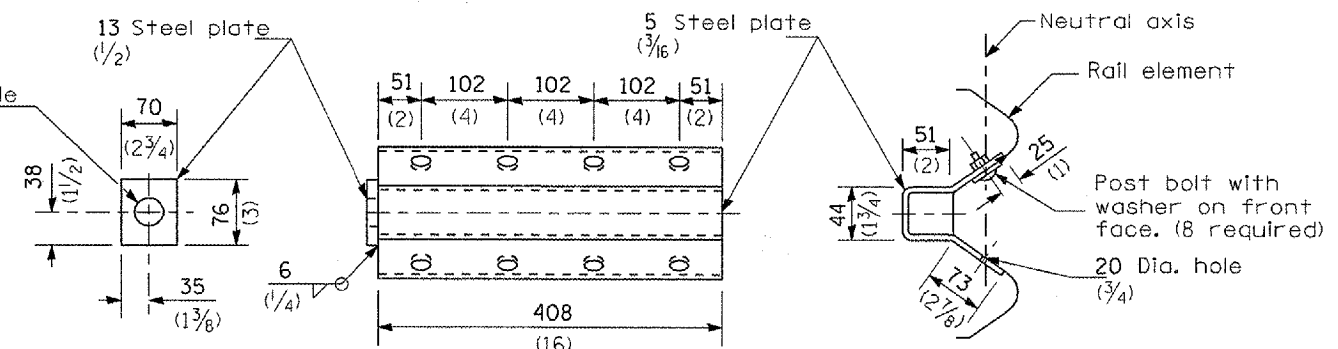
**NOTE**

When end shoe is attached to a bridge parapet which has an expansion joint, the bolts shall be provided with a locknut or double nut and shall be tightened only to a point that will allow guardrail movement.

The standard end shoe shall be attached to the concrete with pre-drilled or self-drilling anchor bolts. The anchor cone shall be set flush with the surface of the concrete.

Externally threaded studs protruding from the surface of the concrete will not be permitted.

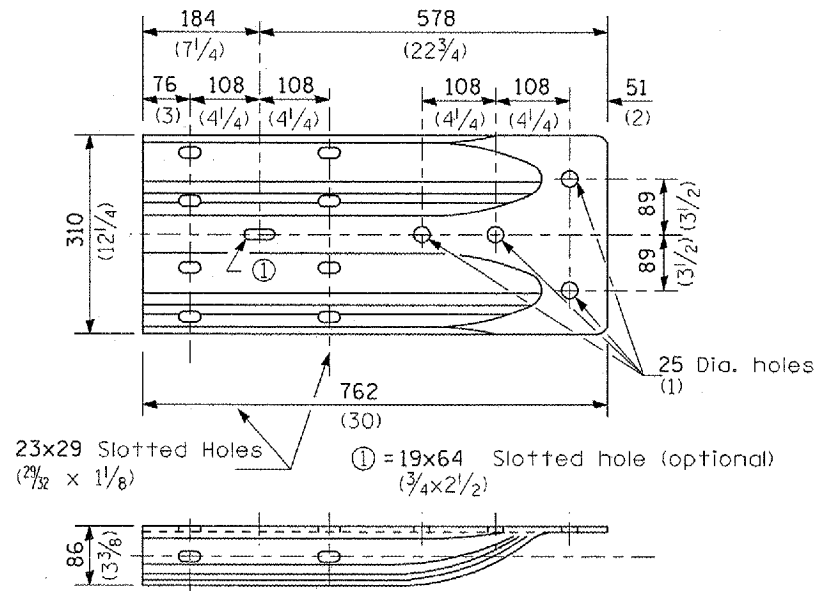
**END SHOE**



**NOTE**

Anchor plate T shall be used to attach cable assembly to guardrail when required on traffic barrier terminals.

**ANCHOR PLATE T DETAILS**



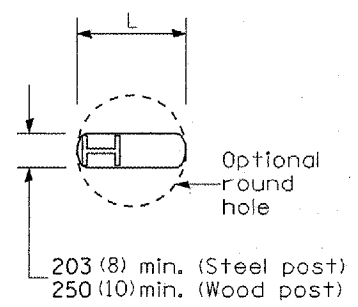
**ALTERNATE END SHOE**

PLOT DATE: Jan-30-2008 07:10:09AM  
 FILE NAME: c:\projects\652282\detail-remove-rearrest-guardrail.dgn  
 PLOT SCALE: B0824 / IN.  
 USER NAME: laughton1

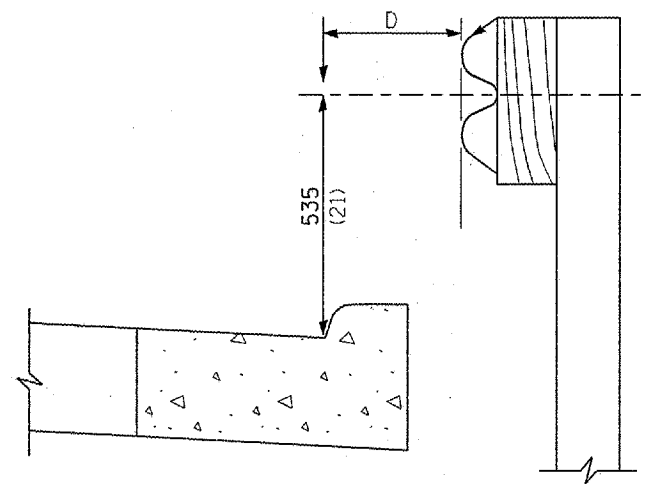
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<b>STEEL PLATE BEAM GUARDRAIL DETAILS</b>  (Sheet 3 of 4)  SCALE: VERT.    DRAWN BY HORIZ.            CHECKED BY DATE

Added Sheet 1-B-08

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	**	SANGAMON	248	193D
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		* FA155(I-55), FA172(I-72), FAP666(I-55 BUS)		
		** D6 INTERSTATE RS,BR,M CAB 2008		



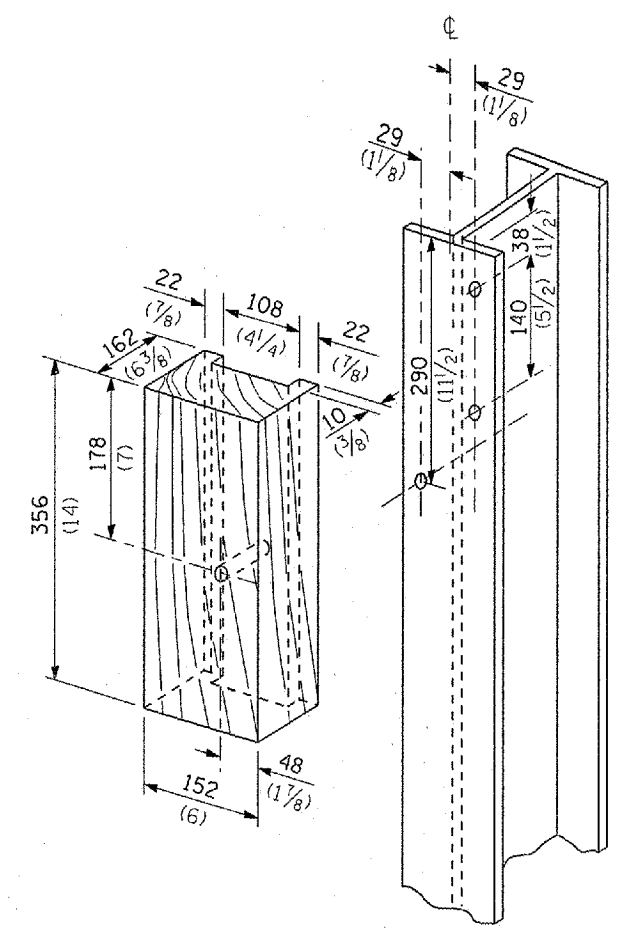
PLAN



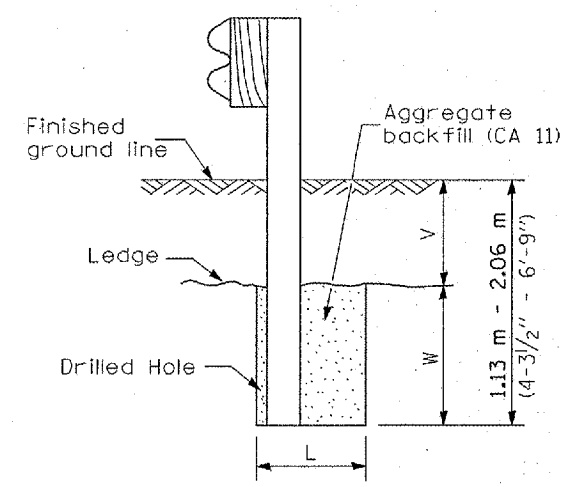
Note:  
If it is necessary for D to be more than 300 (12) and less than 3.0 m (10'-0") type M-5 (M-2) curb and gutter (Std. 606001) shall be used in front of and in advance of the guardrail.

GUARDRAIL PLACED BEHIND CURB

(D = 0 desirable to 300 (12) maximum)



WOOD BLOCK-OUT AND STEEL POST DETAILS

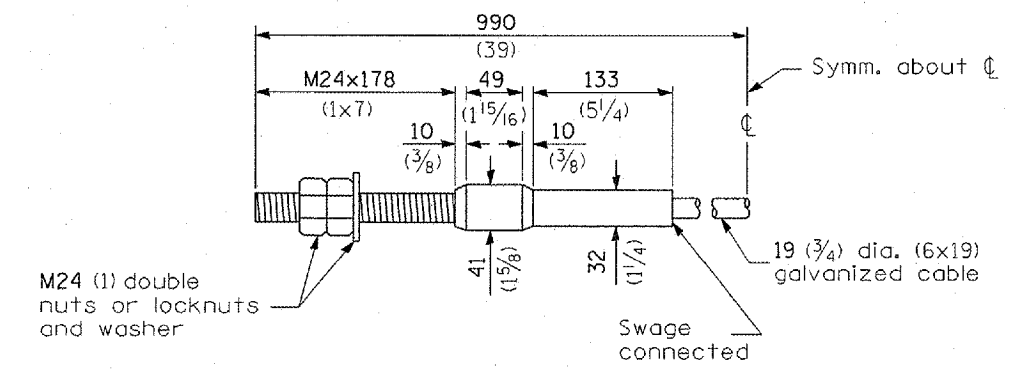


Note:  
Ledge line is top of rock ledge or hard slag fill.

ELEVATION

FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED

V	W	L	
		Steel Post	Wood Post
0 - 460 (0 - 18)	610 (24)	530 (21)	580 (23)
>460 - 825 (>18 - 41.5)	305 (12)	203 (8)	250 (10)
>825 - 1.13 m (>41.5 - 53.5)	305 - 0 (12 - 0)	203 (8)	250 (10)



CABLE ASSEMBLY

(18,100 kg (40,000 lbs.) min. breaking strength)  
Tighten to taut tension.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STEEL PLATE BEAM GUARDRAIL DETAILS**  
(Sheet 4 of 4)  
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

Added Sheet 1-8-08

PLOT DATE : 08-08-08 07:08:24  
 FILE NAME : c:\p\080808\080808.dgn  
 PLOT SCALE : 0.0034 x 1 in.  
 USER NAME : joughnert