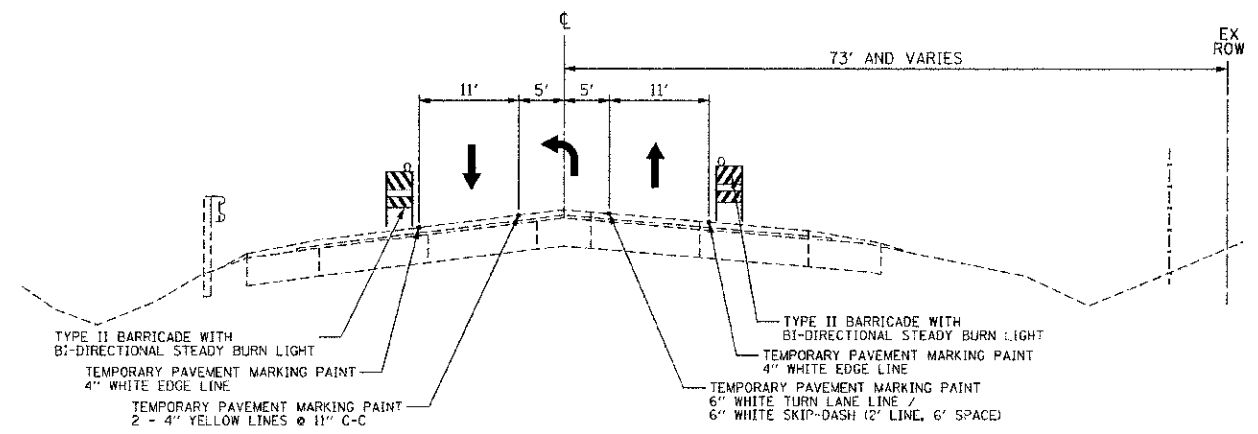
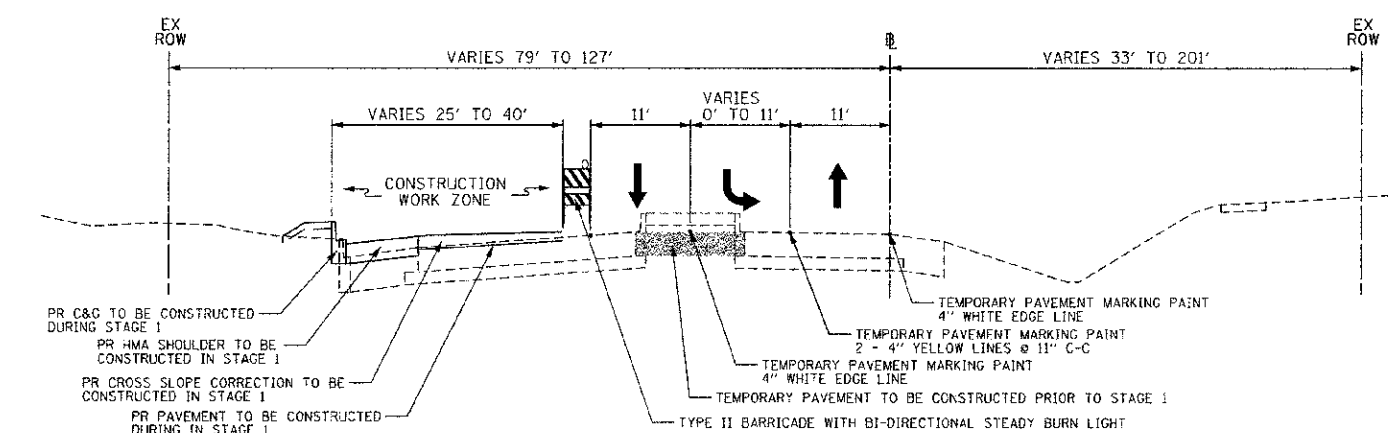


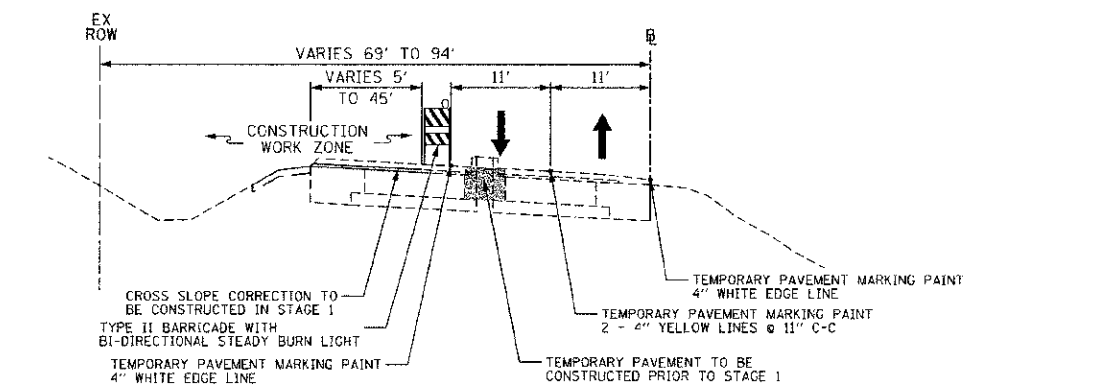
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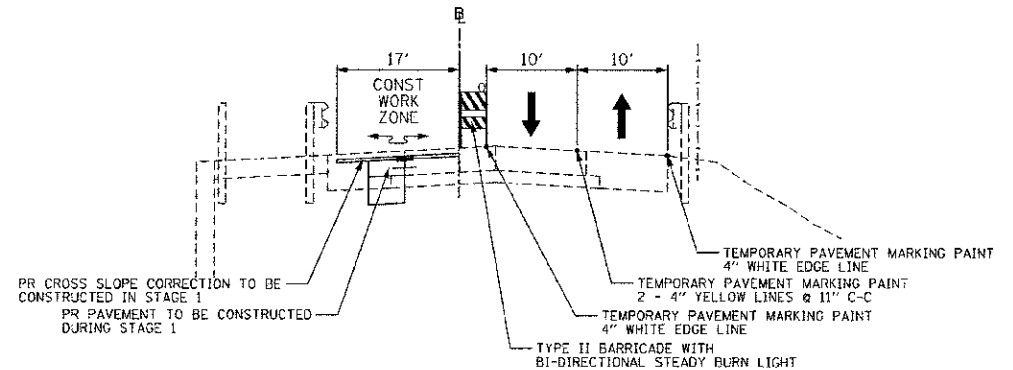
TRAFFIC CONTROL TYPICAL SECTION - STAGE 1
BUTTERFIELD ROAD
STATION 100+20 TO STATION 107+63



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1
COMMONWEALTH LANE
STATION 300+32 TO 304+65



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1
COMMONWEALTH LANE
STATION 304+65 TO 309+68



TRAFFIC CONTROL TYPICAL SECTION - STAGE 1
COMMONWEALTH LANE
STATION 200+61 TO 201+90

PRIOR TO STAGE 1

1. MAINTAIN TWO-WAY TRAFFIC FLOW ON BUTTERFIELD ROAD, COMMONWEALTH LANE AND ONE-WAY TRAFFIC ON THE ILLINOIS ROUTE 38 EXIT RAMP.
2. THE FOLLOWING SHALL BE COMPLETED USING STATE STANDARD 701502-05 FOR URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE. A MINIMUM OF ONE LANE SHALL BE MAINTAINED IN EACH DIRECTION.
 - A. REMOVE THE MEDIAN ISLANDS ALONG COMMONWEALTH LANE. REPLACE WITH TEMPORARY PAVEMENT.
 - B. UTILITIES SHALL BE RELOCATED AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEETS NO. 28 AND 29)
3. INSTALL TEMPORARY TRAFFIC SIGNALS (SEE SHEETS NO. 43 TO 47).

STAGE 1

1. USE TRAFFIC CONTROL STAGE 1 PLANS (SEE SHEET NO. 22 TO 24).
2. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE CENTERLINE OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH.
3. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE SOUTH EDGE OF PAVEMENT OF COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 10' THROUGH LANE IN EACH DIRECTION THROUGHOUT THE ENTIRE LENGTH.
4. ESTABLISH TWO-WAY TRAFFIC FLOW ALONG THE SOUTH EDGE OF PAVEMENT OF COMMONWEALTH LANE SOUTH OF BUTTERFIELD ROAD. MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE THROUGHOUT THE ENTIRE LENGTH.
5. MAINTAIN EXISTING TRAFFIC FLOW ALONG THE ILLINOIS ROUTE 38 EXIT RAMP.
6. ESTABLISH EROSION CONTROL METHODS WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE AS SHOWN ON THE EROSION CONTROL PLAN (SEE SHEET NO. 27).
7. CONSTRUCT PROPOSED STORM SEWER WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE AS SHOWN ON THE DRAINAGE AND UTILITY PLANS (SEE SHEETS NO. 28 AND 29).
8. REMOVE EXISTING CURB & GUTTER AND SHOULDERS WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE. MILL 1" OF THE CONCRETE SURFACE AND MILL THE HMA SURFACE AND BINDER COURSE WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE.
9. CONSTRUCT PAVEMENT PATCHES AS DIRECTED BY THE ENGINEER WITHIN THE STAGE 1 WORK ZONE.
10. CONSTRUCT PROPOSED SHOULDER, CURB & GUTTER AND PCC BASE COURSE ALONG COMMONWEALTH LANE WITHIN THE STAGE 1 CONSTRUCTION WORK ZONE.
11. ALONG COMMONWEALTH LANE NORTH OF BUTTERFIELD ROAD, REMOVE ALL DRUMS AND TYPE II BARRICADES ONCE STAGE 1 CONSTRUCTION IS COMPLETE.
12. BEGIN CROSS SLOPE CORRECTION WITH THE EXCEPTION OF THE HMA SURFACE COURSE ALONG THE WESTBOUND LANES OF COMMONWEALTH LANE. FOR PAVING DETAILS, SEE SHEETS NO. 35 TO 38.
13. CONSTRUCT THE PROPOSED DRIVEWAY AT STATION 302+70. THE DRIVEWAY MUST REMAIN OPEN TO TRAFFIC AT ALL TIMES.
14. CONSTRUCT THE PROPOSED PAVEMENT AND CURB & GUTTER ALONG THE SIDESTREET AT STATION 306+90 BY ALIGNING TRAFFIC ALONG THE WEST EDGE OF PAVEMENT. WHEN THE PAVEMENT AND CURB & GUTTER IS CONSTRUCTED, SPLIT TRAFFIC ALONG THE OUTSIDE EDGES OF PAVEMENT OF THE SIDESTREET TO CONSTRUCT THE CORRUGATED MEDIAN. THE ROAD SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES.
15. REMOVE EXISTING AND INSTALL PROPOSED LIGHT POLES ALONG THE NORTH SIDE OF COMMONWEALTH LANE.
16. COMPLETE LANDSCAPING ALONG THE NORTH SIDE OF COMMONWEALTH LANE.

GENERAL NOTES

1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF ONE 11' THROUGH LANE IN EACH DIRECTION FOR TWO-WAY TRAFFIC FLOW UNLESS OTHERWISE SPECIFIED. A MINIMUM OF 3' SHALL BE MAINTAINED BETWEEN TRAFFIC AND CONSTRUCTION AT ALL TIMES UNLESS OTHERWISE NOTED IN THE PLANS.
2. THE CONTRACTOR SHALL MAINTAIN TRAFFIC IN ACCORDANCE WITH THE SPECIAL PROVISIONS, STATE STANDARDS, STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
3. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING.
4. THE ENGINEER SHALL CONTACT THE ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 IN ADVANCE OF BEGINNING WORK.
5. TEMPORARY LANE CLOSURES FOR ANY REASON SHALL BE RESTRICTED TO THE WEEKDAY HOURS OF 9:00 AM TO 3:30 PM, AS APPROVED IN ADVANCE BY THE ENGINEER.
6. DRUMS OR TYPE II BARRICADES SHALL BE EQUIPPED WITH BI-DIRECTIONAL STEADY BURN LIGHTS AND SHALL BE PLACED AT 25' INTERVALS ALONG THE PROPOSED CONSTRUCTION WORK ZONE AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. TYPE II BARRICADES SHALL HAVE A MINIMUM REFLECTORIZED AREA OF 288 SQUARE INCHES. TYPE II BARRICADES, 2' IN WIDTH, SHALL BE USED IN ALL STAGES OF CONSTRUCTION.
7. ALL DRIVEWAYS SHALL BE OPEN TO TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED.
8. WET REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE, TYPE III, SHALL BE USED ON ALL FINAL PAVEMENT WEARING SURFACES. TEMPORARY PAVEMENT MARKING PAINT SHALL BE USED ELSEWHERE.
9. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN. THIS WORK SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL.
10. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETEIORATION AFTER 7 DAYS SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR 1 PLACEMENT AND 2 REPLACEMENTS HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO 7 DAYS OF SERVICE OR REPLACEMENT AFTER THE THIRD REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.
11. TEMPORARY PAVEMENT SHALL ADHERE TO THE TEMPORARY PAVEMENT SPECIAL PROVISION. TEMPORARY PAVEMENT SHALL CONSIST OF 2 INCHES OF HMA SURFACE COURSE AND 8 INCHES OF HMA BINDER COURSE.
12. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS. THE WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION. (SPECIAL) ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION. (SPECIAL).
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING DRAINAGE OF THE ROADWAY DURING ALL STAGES OF CONSTRUCTION. A QUANTITY OF 3 INLETS, TYPE A, TYPE B GRATE AND 200' OF STORM SEWERS, CLASS A, TYPE 1 12" HAS BEEN PROVIDED FOR TEMPORARY USE. REMOVAL OF THESE ITEMS SHALL BE INCLUDED IN THEIR COST. ALL EXISTING STORM SEWER SHALL REMAIN FUNCTIONAL AND INTACT UNTIL NEW STORM SEWER IS FUNCTIONAL. AT THE END OF EACH DAYS WORK, THE STORM SEWER SHALL BE CONNECTED AND FUNCTIONAL.
14. ALL TRAFFIC CONTROL WARNING SIGNS AND ASSOCIATED SIGNING MOUNTED WITH THE WARNING SIGNS SHALL HAVE BLACK LEGENDS AND BORDERS ON FLUORESCENT ORANGE REFLECTIVE SHEETING.
15. ALL CONSTRUCTION SIGNS, BARRICADES, AND OTHER DEVICES REQUIRED TO CONTROL TRAFFIC SHALL BE FURNISHED, INSTALLED, AND MAINTAINED BY THE CONTRACTOR.
16. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3 INCH X 6 INCH DELINEATOR INSTALLED.
17. THE TRAFFIC CONTROL DEPICTED HEREIN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED BY THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF THE LUMP SUM PAY ITEM "TRAFFIC CONTROL AND PROTECTION. (SPECIAL)" UNLESS OTHERWISE INDICATED IN THE PLANS OR SPECIAL PROVISIONS.
18. A QUANTITY OF HOT-MIX ASPHALT SURFACE COURSE MIX "D", NS0 HAS BEEN INCLUDED FOR TEMPORARY INTERSECTION GRADING FOR ELEVATION DIFFERENCES BETWEEN EXISTING, TEMPORARY AND PROPOSED PAVEMENT.
19. A BIKE PATH CONNECTION SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION FROM THE INTERSECTION OF BUTTERFIELD ROAD AND COMMONWEALTH LANE TO THE EXISTING SALT CREEK TRAIL WITHIN THE PARKING LOT AT STATION 305+20 LT. A QUANTITY OF AGGREGATE FOR TEMPORARY ACCESS HAS BEEN INCLUDED TO MAINTAIN THE BIKE PATH CONNECTION.

NOTE:

BOXED ITEMS ARE INCLUDED IN THE COST AS DESIGNATED.



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PL01 DATE = 12/4/2012	DATE - 10/22/2012	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED CONSTRUCTION PROCEDURES AND TRAFFIC CONTROL
TYPICAL SECTIONS AND NOTES

SCALE: NOT TO SCALE SHEET NO. 19 OF 108 SHEETS

F.A.U.:	SECTION	COUNTY	TOTAL SHEET
RTE.			SHEETS: NO.
3545	09-00170-00-CH	DUPAGE	108 19
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63742