1-18-13 LETTING ITEM 117

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 1 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT -- STP | | |

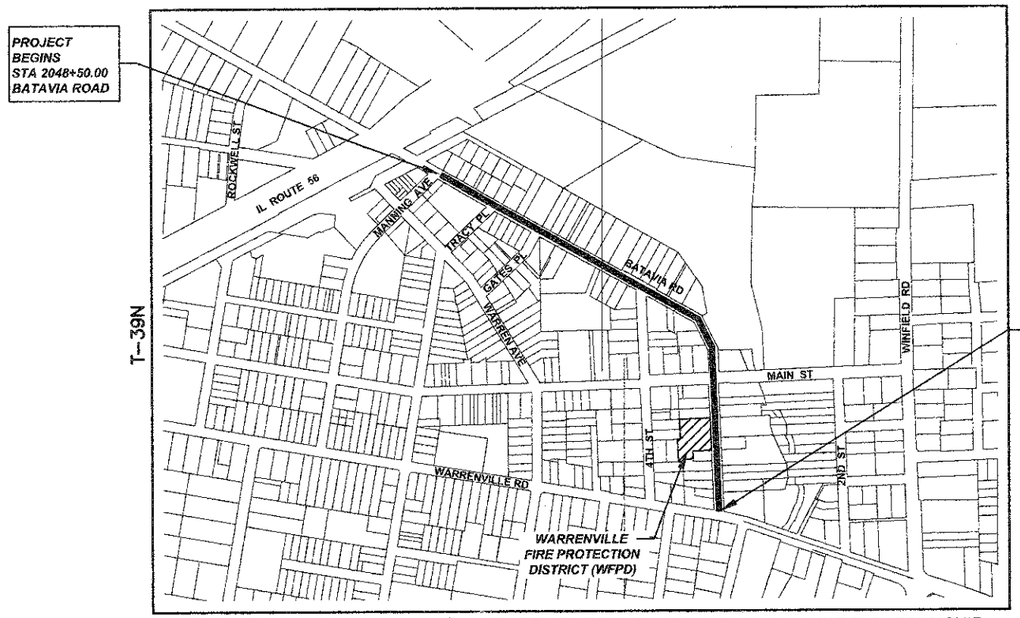
# STATE OF ILLINOIS
# DEPARTMENT OF TRANSPORTATION
# DIVISION OF HIGHWAYS
# PLANS FOR PROPOSED
# FEDERAL-AID HIGHWAY

## FAU 3553 (BATAVIA ROAD)
## FROM EAST OF FAP 365 (IL ROUTE 56)
## TO FAU 1479 (WARRENVILLE ROAD)
## WIDENING AND RESURFACING
## SECTION NO. 10-00031-00-RS
## PROJECT NO. M-9003(866)
## CITY OF WARRENVILLE
## DUPAGE COUNTY
## JOB NO. C-91-086-12

PROJECT LOCATED IN THE CITY OF WARRENVILLE


LOCATION OF SECTION INDICATED THUS: --

R-9E




N

SE 1/4, NW 1/4 AND NE 1/4 SEC 35, T-39N, R-9E, 3RD P.M., WINFIELD TOWNSHIP

**LOCATION MAP**
1" = 500'

TOTAL GROSS & NET LENGTH OF PROJECT = 3,255 FEET (0.616 MILES)

**INDEX OF SHEETS**

| SHEET NO. | SHEET DESCRIPTION |
|---|---|
| 1. | COVER SHEET |
| 2. | GENERAL NOTES AND STATE STANDARDS |
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| 4.-6. | EXISTING AND PROPOSED TYPICAL SECTIONS |
| 7. | SCHEDULE OF QUANTITIES |
| 8. | ALIGNMENT, TIES AND BENCHMARKS |
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| 10. | BATAVIA ROAD DETOUR PLAN |
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| 17.-19. | PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN |
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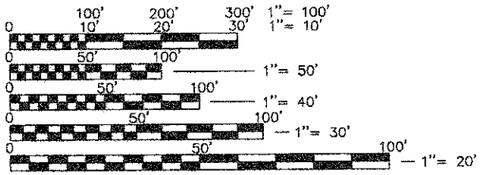
| 23. | (TC-10) TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS |
| 24. | (TC-11) TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) |
| 25. | (TC-13) DISTRICT ONE - TYPICAL PAVEMENT MARKINGS |
| 26. | (TC-22) ARTERIAL ROAD INFORMATION SIGN |
| 27. | (BD-22) PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT |
| 28. | (BD-32) BUTT JOINT AND HMA TAPER DETAILS |
| 29. | (BD-36) FIRE HYDRANT TO BE MOVED |

DESIGN DESIGNATION

BATAVIA ROAD: URBAN MINOR ARTERIAL
DESIGN SPEED = 35 M.P.H.
POSTED SPEED = 35 M.P.H.
ADT(2012) = 11,000 VPD




FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 10/11/2012
Richard Smith
CITY OF WARRENVILLE, SUPERINTENDENT OF PUBLIC WORKS

PASSED November 8, 2012
C. Holt
C. HOLT
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW November 8 2012
John Fortmann
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

DATE: 10/11/12

BY: James R. Lenzini
JAMES R. LENZINI

LICENSE EXPIRES: NOVEMBER 30, 2013

JAMES R. LENZINI 062-062124 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

SEAL

JULIE JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811

811 Know what's below. Call before you dig.

CONTRACT NO. 63758


Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
P: 630.466.6700 -- W: www.eeiweb.com


PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS




PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL
CONSULTING ENGINEER: ENGINEERING ENTERPRISES, INC. CONTACT: JAMES R. LENZINI P.E. 630-466-6700



Plotted: October 10, 2012 @ 10:41 AM By: Jim Schmidt -- Tab: 01 Cover -- 22x34
Path: H:\SDSIPROJ\W70003\DWG\DWG FINAL ENG\W70003-CVR

## GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE 'STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION,' ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS), THE 'SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS,' ADOPTED JANUARY 1, 2013, THE LATEST EDITION OF THE 'ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS', THE 'STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS', LATEST EDITION AND REVISIONS THERETO, THE CODES AND ORDINANCES OF THE CITY OF WARRENVILLE, ILLINOIS, THE DETAILS IN THE PLANS AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

NO SUBSTITUTIONS OR VARIANCES WILL BE PERMITTED TO ANY STANDARD NOTES OR ORDINANCES UNLESS APPROVED OTHERWISE IN WRITING PRIOR TO COMMENCING CONSTRUCTION ACTIVITY.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL AT ALL TIMES PROVIDE PROTECTION FOR TRAFFIC AS CALLED FOR IN THE APPLICATION OF TRAFFIC CONTROL DEVICES, THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE PLANS.

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE OWNER IF ANY UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, FIELD TILES AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND NOT NECESSARILY COMPLETE; THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED. THIS WORK SHALL BE ARRANGED BY THE UTILITY COMPANY AND SHALL BE AT THE CONTRACTOR'S EXPENSE.

IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR OPERATE ANY VALVES OR HYDRANTS.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THE OWNER'S AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE BACK OF CURB, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN AT POINTS OF CURVE, ETC., ARE TOP OF CURB UNLESS OTHERWISE NOTED.

STRUCTURE OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS ARE TO THE FOLLOWING POINTS:

A) STRUCTURES FALLING IN THE CURB LINE ARE MEASURED TO THE BACK OF CURB
B) ALL OTHER STRUCTURES ARE MEASURED TO THE CENTER OF THE STRUCTURE

ELEVATIONS ARE NGVD 29 DATUM, ESTABLISHED FROM THE DUPAGE COUNTY GEODETIC SURVEY BENCHMARK NETWORK.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, BACK OF CURB, ETC., ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.

SEWERS AND WATER MAINS

ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, IT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN IN AN OPERATING CONDITION TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE COST OF INTERCONNECTIONS BETWEEN THE PROPOSED AND EXISTING SEWER SYSTEMS AND PROPOSED AND EXISTING WATER MAIN SYSTEMS SHALL BE INCLUDED IN THE VARIOUS UNIT PRICES ON THE ITEMS BEING CONNECTED UNLESS NOTED OTHERWISE. THE COSTS OF PLUGGING ANY EXISTING STORM SEWER CONNECTIONS AS INDICATED ON THE PLANS SHALL BE INCLUDED IN THE COST OF STORM SEWER REMOVAL.

ALL FRAMES, GRATES, OR LIDS SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY OF THE CITY. ANY ITEMS DAMAGED DURING REMOVAL SHALL BE REPLACED BY THE CONTRACTOR AT THEIR OWN EXPENSE. THE COST OF SALVAGING EXISTING FRAMES, GRATES, OR LIDS AND/OR STOCKPILING THEM ON THE JOB SITE FOR PICKUP BY THE CITY OR DELIVERY TO THE CITY MAINTENANCE YARD SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THIS CONTRACT FOR ANY MANHOLE, CATCH BASIN, INLET, OR VALVE VAULT SHALL HAVE ONE OF THE FOLLOWING WORDS CAST INTO THE LID: "STORM", "SANITARY", OR "WATER" AS APPLICABLE. ANY ADDITIONAL COST FOR THIS REQUIREMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE FRAME AND CLOSED LID PROVIDED.

FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION AND CROSS SLOPE OF THE AREA IN WHICH THEY ARE LOCATED. ALL FINAL ADJUSTMENTS OF FRAMES WILL BE ACCOMPLISHED BY THE USE OF CONCRETE ADJUSTING RINGS SET IN BUTYL ROPE JOINT SEALANT; MORTAR JOINTS WILL NOT BE ALLOWED. HEIGHT OF ADJUSTING RINGS SHALL NOT EXCEED EIGHT INCHES (8"). THE COST OF THE ADJUSTMENT TO FINAL ELEVATION IS INCLUDED IN THE COST OF THE ITEM CONSTRUCTED.

ALL STORM SEWERS SHALL BE PVC SDR 26, MEETING ASTM D-3034, UNLESS NOTED OTHERWISE ON THE PLAN. JOINTS SHALL BE GASKET TYPE MEETING ASTM D-3212.

STORM SEWERS, WATER MAIN QUALITY PIPE SHALL BE PVC DR25 MEETING AWWA C900. JOINTS SHALL BE GASKET TYPE MEETING ASTM D-3139.

BACKFILL

THE COST OF TRENCH BACKFILL SHALL BE CONSIDERED INCLUDED IN THE COST OF STORM SEWER, SANITARY SEWER OR WATER MAIN SPECIFIED

STORM SEWER, SANITARY SEWER, AND WATER MAIN SHALL BE BACKFILLED IN ACCORDANCE WITH ARTICLE 550.07, METHOD 1 ONLY, OR AS DIRECTED BY THE ENGINEER, WITH THE FOLLOWING MODIFICATIONS.

INITIAL TRENCH BACKFILL SHALL BE CA-7. FINAL TRENCH BACKFILL SHALL BE GRADATION CA-6. THE FINAL TRENCH BACKFILL SHALL BE PLACED IN 6" LIFTS AND SHALL BE COMPACTED IN PLACE TO NINETY FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE AS DETERMINED BY THE MODIFIED PROCTOR TEST.

SIGNS

PRIOR TO THE START OF CONSTRUCTION, THE CITY, CONTRACTOR AND ENGINEER SHALL INVENTORY THE LOCATION, SIZE, TYPE, AND CONDITION OF ALL EXISTING SIGNS. ANY SIGN DAMAGED DURING CONSTRUCTION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

PRIOR TO THE START OF CONSTRUCTION THE CITY WILL REMOVE ALL SIGNS THAT MAY INTERFERE WITH CONSTRUCTION, THE CITY SHALL ALSO REPLACE SIGNS AND FURNISH AND INSTALL PERMANENT SIGNS AS INDICATED ON THE PLANS.

MISCELLANEOUS

THE CONTRACTOR SHALL MAINTAIN EXISTING SIDE STREET, DRIVEWAY AND PEDESTRIAN ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION OF THE PROJECT, UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE ITEM 'AGGREGATE FOR TEMPORARY ACCESS'.

SAWING OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR AS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

AT ALL BUTT JOINT LOCATIONS, THE EXISTING SURFACE SHALL BE CUT TO A MINIMUM THICKNESS OF TWO (2) INCHES AS INDICATED ON THE PLANS.

THE THICKNESS OF ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE ASPHALT MIXTURES ARE TO BE PLACED.

PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACE AND TOP OF CURB, PCC SIDEWALK, PCC DRIVEWAY PAVEMENT, AND AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FRESH CONCRETE FROM DAMAGE AND VANDALISM. ANY DAMAGED OR VANDALIZED CONCRETE SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AND CITY AT THEIR OWN EXPENSE. ANY SHEETING AND/OR SHORING USED FOR THIS IMPROVEMENT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

THE CONTRACTOR SHALL PROTECT ALL EXISTING FACILITIES (E.G. CURB, DRIVEWAYS, PAVEMENT) THAT ARE NOT INDICATED TO BE REMOVED ON THE PLANS. ANY FACILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE CONTRACTOR'S EXPENSE.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS CONTRACT.

EXISTING PAVEMENT THICKNESSES SHOWN ON THE PLANS ARE APPROXIMATE, BASED ON AVAILABLE INFORMATION AT THE TIME OF DESIGN. ANY ADDITIONAL COSTS REQUIRED BY THE CONTRACTOR DUE TO THICKNESSES OTHER THAN THOSE SHOWN ON THE PLANS WILL BE INCLUDED IN THE COST OF THE CONTRACT.

WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, THE CONTRACTOR SHALL FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION. IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES.

THE CONTRACTOR WILL BE REQUIRED TO COMPLY WITH ALL STATE REGULATIONS REGARDING AIR, WATER AND NOISE POLLUTION. THE CONTRACTOR IS PROHIBITED FROM BURNING ANY MATERIAL WITHIN OR ADJACENT TO THE IMPROVEMENT.

THE CONTRACTOR SHALL PREPARE THE SUBGRADE IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS PRIOR TO THE REMOVAL OF ANY UNSTABLE MATERIALS.

ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SEEDED AS SHOWN IN THE PLANS. LIMITS SHOWN ON THE PLANS ARE THE MAXIMUM PAY WIDTHS FOR PAYMENT PURPOSES. ADDITIONAL AREAS DAMAGED BY MACHINERY, CONSTRUCTION EQUIPMENT, CONTRACTOR NEGLIGENCE OR OVER-EXCAVATION SHALL BE RESTORED TO A CONDITION EQUAL TO THAT EXISTING BEFORE THE DAMAGE OCCURRED AT THE COST OF THE CONTRACTOR.

THE CONTRACTOR SHALL DISPOSE OF AND REMOVE FROM THE SITE EACH DAY ALL CURB AND GUTTER, PAVEMENT AND ALL OTHER EXCAVATED MATERIAL NOT FOR SALVAGE. THE COST FOR HAULING AND TRUCKING TO DISPOSAL LOCATIONS WILL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

ALL EARTH EXCAVATION OR EMBANKMENT REQUIRED TO CONSTRUCT PROPOSED CONTRACT ITEMS SHALL BE INCLUDED IN THE COST OF THE ITEM BEING INSTALLED.

THE ENGINEER AND CITY ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF THEIR WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

BITUMINOUS MATERIALS (PRIME COAT) SHALL BE APPLIED AT A RATE OF 0.1 GALLONS PER SQUARE YARD ON CONCRETE/ASPHALT AND 0.5 GALLONS PER SQUARE YARD ON AGGREGATE. BITUMINOUS MATERIALS SHALL BE SS-1 ON CONCRETE/ASPHALT AND MC-30 ON AGGREGATE.

AGGREGATE (PRIME COAT) SHALL BE MECHANICALLY SPREAD AT A UNIFORM RATE OF 4 POUNDS PER SQUARE YARD.

DRIVEWAY PAVEMENT REMOVAL PAY ITEMS SHALL INCLUDE REMOVAL OF ALL EXISTING MATERIAL (WHETHER ASPHALT, CONCRETE, STONE, OR EARTH) TO A DEPTH OF 9 INCHES FROM PROPOSED DRIVEWAY GRADE FOR PCC DRIVEWAY PAVEMENT AND 10 INCHES FOR STABILIZED DRIVEWAYS.

PAVEMENT WIDENING AREAS FOUR FOOT AND LESS IN WIDTH, ADJACENT TO PROPOSED CURB AND GUTTER, SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND HAVE A HMA SURFACE COURSE AS SHOWN IN THE SPECIAL DETAIL. THE CLASS SI CONCRETE WILL BE CONSIDERED INCLUDED IN THE COST OF THE CURB AND GUTTER.

ANY STRUCTURE TO BE ADJUSTED SHALL HAVE ALL RINGS REMOVED. DETERIORATED RINGS SHALL BE REPLACED. BUTYL ROPE SHALL BE USED WHEN RESETTING THE RINGS AND PRIOR TO ANY MORTAR REPAIR. ALL ADJUSTING RINGS, STRUCTURES AND PIPE ENTRANCES SHALL BE MORTARED (FROM BOTH THE INSIDE AND THE OUTSIDE AS NECESSARY) TO CORRECT ANY EXISTING INFILTRATION. THE GRATE SHALL BE ADJUSTED TO GRADE AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE FOR THE ITEMS TO BE ADJUSTED.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT ANY RESIDENT OR BUSINESS OF ANY REMOVAL AND REPLACEMENT ACTIVITIES THAT WILL INHIBIT OR PROHIBIT ACCESS TO THEIR DRIVEWAY, IN WRITING, A MINIMUM OF 48 HOURS BUT NOT MORE THAN 72 HOURS, PRIOR TO THE COMMENCEMENT OF THESE ACTIVITIES. THE MORNING OF THE WORK, THE CONTRACTOR SHALL AGAIN NOTIFY THE OWNER VERBALLY, TO ALLOW THE OWNER TIME TO MOVE THEIR VEHICLE SO AS NOT TO PROHIBIT THE VEHICLE FROM LEAVING THE DRIVEWAY UPON REMOVAL OF ANY MATERIAL. THE NOTICE GIVEN OUT BY THE CONTRACTOR SHALL PROVIDE INFORMATION REGARDING THE ANTICIPATED DATE THAT FULL ACCESS WILL BE RESTORED. COORDINATION BETWEEN ACTIVITIES SHOULD ALLOW ALL WORK TO BE DONE IN A TIMELY MATTER SO AS TO PERMIT ACCESS TO THE ROADWAY. ANY ADDITIONAL COST OF STAGING REQUIRED TO MAINTAIN ACCESS IS CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

NO DRIVEWAY SHALL BE REMOVED UNLESS THE CONTRACTOR HAS SCHEDULED, WITHIN 24 HOURS, THE APPROPRIATE MATERIAL TO REPLACE THE DRIVEWAY.

THE WARRENVILLE FIRE PROTECTION DISTRICT (WFPD) IS LOCATED AT 3S472 BATAVIA ROAD, WARRENVILLE, IL. REGULAR BUSINESS HOURS ARE FROM 8:00AM TO 4:30PM, MONDAY THROUGH FRIDAY. THE CONTRACTOR SHALL COORDINATE WITH THE WFPD (630-393-1381) AT LEAST 24 HOURS AND IMMEDIATELY BEFORE ANY WORK OCCURS ON THE FIRE STATION ENTRANCES UNLESS OTHERWISE APPROVED OF BY THE WFPD, AT LEAST HALF OF THE DRIVEWAY IN FRONT OF THE VEHICLE BAYS SHALL REMAIN OPEN AT ALL TIMES.

TRAFFIC CONTROL FOR PATCHES OF THE TYPE AND DEPTH SPECIFIED SHALL BE IN ACCORDANCE WITH ARTICLE 701.17 (E) OF THE STANDARD SPECIFICATIONS, OTHER TRAFFIC CONTROL STANDARDS, DETAILS NOTED HEREIN AND AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL COORDINATE WITH THE WARRENVILLE FIRE PROTECTION DISTRICT AS NOTED HEREIN. ANY PATCHES NEAR THE FIRE STATION MUST BE STAGED SUCH THAT ACCESS IS MAINTAINED AT ALL TIMES.

THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL EXISTING MAILBOXES WHICH INTERFERE WITH CONSTRUCTION OPERATIONS. AFTER COMPLETION OF ROADWAY CONSTRUCTION, THE CONTRACTOR SHALL SET THE MAILBOXES IN THEIR PERMANENT LOCATIONS AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE IN CONFORMANCE WITH ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS AND THE COST WILL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

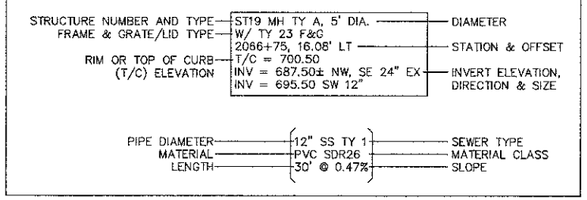
THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR, AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

ALL ROADWAY PATCHING NOT RELATED TO STORM SEWER INSTALLATION SHALL BE COMPLETED AFTER MILLING OPERATIONS.

PCC SIDEWALK AND PCC DRIVEWAY PAVEMENT SHALL HAVE FIBER MESH INSTALLED PER CITY REQUIREMENTS.

DETECTABLE WARNINGS SHALL BE BRICK RED IN COLOR.

### STORM SEWER STRUCTURE AND PIPE NOTATION




### IDOT HIGHWAY STANDARDS

| STD. NO. | DESCRIPTION |
|---|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 424001-07 | PERPENDICULAR CURB RAMPS |
| 424016-01 | MID-BLOCK CURB RAMPS |
| 424031-01 | MEDIAN PEDESTRIAN CROSSINGS |
| 442101-07 | CLASS B PATCHES |
| 442201-03 | CLASS C AND D PATCHES |
| 602001-02 | CATCH BASIN TYPE A |
| 602011-02 | CATCH BASIN TYPE C |
| 602301-03 | INLET TYPE A |
| 602306-03 | INLET TYPE B |
| 602401-03 | MANHOLE TYPE A |
| 602601-02 | PRECAST REINFORCED CONCRETE FLAT SLAB TOP |
| 604001-03 | FRAME AND LID TYPE 1 |
| 604006-04 | FRAME AND GRATE TYPE 3 |
| 604011-04 | FRAME AND GRATE TYPE 3V |
| 604086-02 | FRAME AND GRATE TYPE 23 |
| 606001-05 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 606301-04 | PC CONCRETE ISLANDS AND MEDIANS |
| 701311-03 | LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701606-08 | URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 701701-08 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-05 | LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE |
| 701901-02 | TRAFFIC CONTROL DEVICES |
| 720001-01 | SIGN PANEL MOUNTING DETAILS |
| 720006-03 | SIGN PANEL ERECTION DETAILS |
| 728001-01 | TELESCOPING STEEL SIGN SUPPORT |
| 780001-03 | TYPICAL PAVEMENT MARKINGS |



Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES AND STATE STANDARDS**

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 2 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: November 19, 2012 @ 10:26 AM By: Jim Schmidt - Tab: 02 Notes - 22x34

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## SUMMARY OF QUANTITIES

| SPECIAL PROVISION | SPECIALTY ITEM | CODE NO. | ITEM DESCRIPTION | UNIT | URBAN TOTAL QUANTITY | FEDERAL = 70% / CITY = 30% ROADWAY 0005 | FEDERAL = 70% / CITY = 30% TRAINEES 0042 |
|---|---|---|---|---|---|---|---|
| | | 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 2,286 | 2,286 | |
| | | 21301072 | EXPLORATION TRENCH 72" DEPTH | FOOT | 250 | 250 | |
| | | 25000110 | SEEDING, CLASS 1A | ACRE | 0.5 | 0.5 | |
| | | 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 43 | 43 | |
| | | 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 43 | 43 | |
| | | 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 43 | 43 | |
| Δ | | 25100630 | EROSION CONTROL BLANKET | SQ YD | 2,286 | 2,286 | |
| | | 31101200 | SUBBASE GRANULAR MATERIAL, TYPE B 4" | SQ YD | 2,138 | 2,138 | |
| Δ | | 40201000 | AGGREGATE FOR TEMPORARY ACCESS | TON | 250 | 250 | |
| | | 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 1,063 | 1,063 | |
| | | 40600300 | AGGREGATE (PRIME COAT) | TON | 21 | 21 | |
| | | 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 121 | 121 | |
| | | 40603335 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 | TON | 82 | 82 | |
| | | 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 1,183 | 1,183 | |
| | | 42001300 | PROTECTIVE COAT | SQ YD | 2,425 | 2,425 | |
| | | 42300300 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH | SQ YD | 257 | 257 | |
| | | 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SQ FT | 5,104 | 5,104 | |
| | | 42400400 | PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH | SQ FT | 1,131 | 1,131 | |
| | | 42400800 | DETECTABLE WARNINGS | SQ FT | 200 | 200 | |
| | | 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 268 | 268 | |
| | | 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 6,504 | 6,504 | |
| | | 44000600 | SIDEWALK REMOVAL | SQ FT | 2,330 | 2,330 | |
| | | 44003100 | MEDIAN REMOVAL | SQ FT | 503 | 503 | |
| Δ | | 44200966 | CLASS B PATCHES, TYPE I, 10 INCH | SQ YD | 53 | 53 | |
| Δ | | 44200970 | CLASS B PATCHES, TYPE II, 10 INCH | SQ YD | 149 | 149 | |
| Δ | | 44200974 | CLASS B PATCHES, TYPE III, 10 INCH | SQ YD | 54 | 54 | |
| Δ | | 44200976 | CLASS B PATCHES, TYPE IV, 10 INCH | SQ YD | 191 | 191 | |
| | | 44201690 | CLASS D PATCHES, TYPE I, 4 INCH | SQ YD | 2 | 2 | |
| | | 44201696 | CLASS D PATCHES, TYPE IV, 4 INCH | SQ YD | 382 | 382 | |
| | | 44201769 | CLASS D PATCHES, TYPE III, 10 INCH | SQ YD | 68 | 68 | |
| | | 44300200 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | FOOT | 6,246 | 6,246 | |
| | | 550B0050 | STORM SEWERS, CLASS B, TYPE 1 12" | FOOT | 316 | 316 | |
| | | 55100300 | STORM SEWER REMOVAL 8" | FOOT | 105 | 105 | |
| | | 55100500 | STORM SEWER REMOVAL 12" | FOOT | 27 | 27 | |
| Δ | * | 56106300 | ADJUSTING WATER MAIN 6" | FOOT | 150 | 150 | |
| | * | 56400100 | FIRE HYDRANTS TO BE MOVED | EACH | 1 | 1 | |
| | | 60200305 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 3 FRAME AND GRATE | EACH | 3 | 3 | |
| | | 60200310 | CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 3V FRAME AND GRATE | EACH | 5 | 5 | |
| | | 60207105 | CATCH BASINS, TYPE C, TYPE 3 FRAME AND GRATE | EACH | 2 | 2 | |
| | | 60207115 | CATCH BASINS, TYPE C, TYPE 3V FRAME AND GRATE | EACH | 11 | 11 | |
| | | 60208230 | CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE | EACH | 2 | 2 | |

| SPECIAL PROVISION | SPECIALTY ITEM | CODE NO. | ITEM DESCRIPTION | UNIT | URBAN TOTAL QUANTITY | FEDERAL = 70% / CITY = 30% ROADWAY 0005 | FEDERAL = 70% / CITY = 30% TRAINEES 0042 |
|---|---|---|---|---|---|---|---|
| | | 60218400 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH | 2 | 2 | |
| | | 60222230 | MANHOLES, TYPE A, 5'-DIAMETER, TYPE 23 FRAME AND GRATE | EACH | 2 | 2 | |
| | | 60255500 | MANHOLES TO BE ADJUSTED | EACH | 1 | 1 | |
| | | 60260500 | INLETS TO BE ADJUSTED WITH NEW TYPE 3 FRAME AND GRATE | EACH | 2 | 2 | |
| | | 60260505 | INLETS TO BE ADJUSTED WITH NEW TYPE 3V FRAME AND GRATE | EACH | 5 | 5 | |
| | | 60266600 | VALVE BOXES TO BE ADJUSTED | EACH | 1 | 1 | |
| | | 60500040 | REMOVING MANHOLES | EACH | 1 | 1 | |
| | | 60500060 | REMOVING INLETS | EACH | 12 | 12 | |
| | | 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 288 | 288 | |
| Δ | * | 66900200 | NON-SPECIAL WASTE DISPOSAL | CU YD | 6 | 6 | |
| Δ | * | 66900450 | SPECIAL WASTE PLANS AND REPORTS | L SUM | 1 | 1 | |
| | | 67100100 | MOBILIZATION | L SUM | 1 | 1 | |
| | | 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 3 | 3 | |
| | | 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 1,305 | 1,305 | |
| | | 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 5 | 5 | |
| | | 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 1,105 | 1,105 | |
| | | 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 435 | 435 | |
| | * | 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 4.6 | 4.6 | |
| | * | 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 6,352 | 6,352 | |
| | * | 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 535 | 535 | |
| | * | 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 50 | 50 | |
| | * | 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 100 | 100 | |
| | | 78300100 | PAVEMENT MARKING REMOVAL | SQ FT | 145 | 145 | |
| | | 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 140 | 140 | |
| Δ | | X4400157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL | SQ YD | 10,738 | 10,738 | |
| Δ | | X4401198 | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SQ YD | 96 | 96 | |
| Δ | | X6026050 | SANITARY MANHOLES TO BE ADJUSTED | EACH | 5 | 5 | |
| Δ | | X6060048 | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL) | FOOT | 6,246 | 6,246 | |
| Δ | | X7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 | 1 | |
| Δ | * | X7810300 | RECESSED REFLECTIVE PAVMENT MARKER | EACH | 146 | 146 | |
| Δ | | XX006215 | BRICK PAVER REMOVAL AND REPLACEMENT | SQ YD | 26 | 26 | |
| Δ | | XX006457 | WHEEL STOP REMOVAL AND REPLACEMENT | EACH | 5 | 5 | |
| Δ | | XX008200 | STABILIZED DRIVEWAY PAVEMENT | SQ YD | 206 | 206 | |
| Δ | | Z0004544 | HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL | SQ YD | 764 | 764 | |
| Δ | | Z0030850 | TEMPORARY INFORMATION SIGNING | SQ FT | 52 | 52 | |
| Δ | | Z0056848 | STORM SEWERS, TYPE 1, WATER MAIN QUALITY PIPE, 12" | FOOT | 113 | 113 | |
| Δ | | Z0066700 | STABILIZED DRIVEWAYS 10" | SQ YD | 517 | 517 | |
| Δ | | Z0076600 | TRAINEES | HOUR | 500 | | 500 |
| Δ | | Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE | HOUR | 500 | | 500 |
| Δ | | XX008743 | STORM SEWERS, CLASS B, TYPE 1 8" | FOOT | 20 | 20 | |

Δ SEE SPECIAL PROVISIONS
* SPECIALTY ITEMS



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| | | |
|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - |
| | DRAWN - KKP/CLN/JPS | REVISED - |
| PLOT SCALE = | CHECKED - JRL | REVISED - |
| PLOT DATE = | DATE - 8/24/12 | REVISED - |

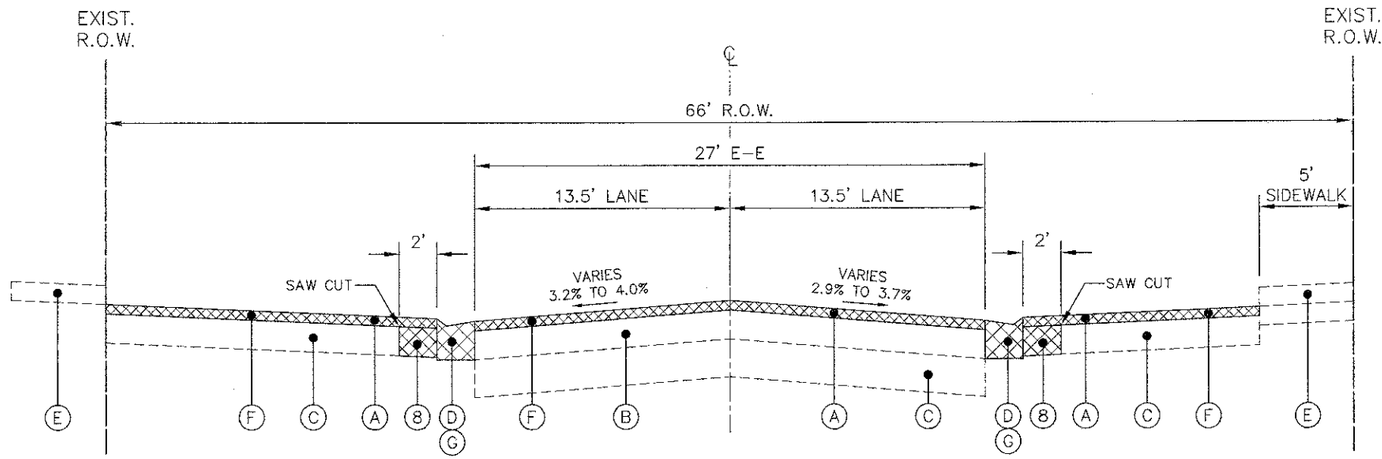
**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

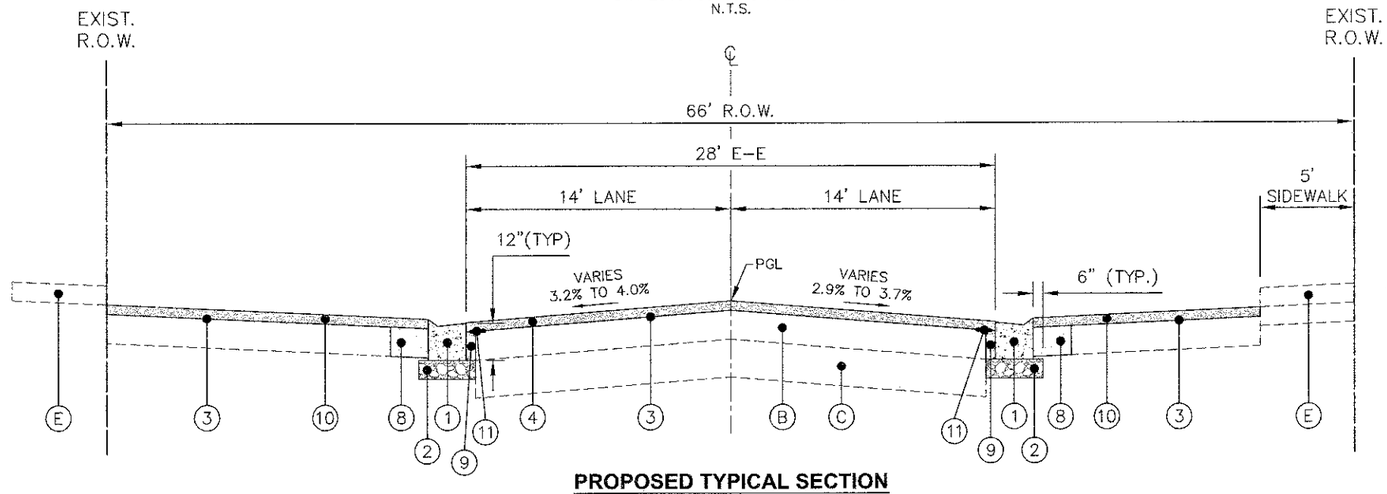
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 3 |
| | | CONTRACT NO. 63758 | | |

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866)




## EXISTING TYPICAL SECTION

BATAVIA ROAD
STA 2048+50 TO STA 2051+42
N.T.S.

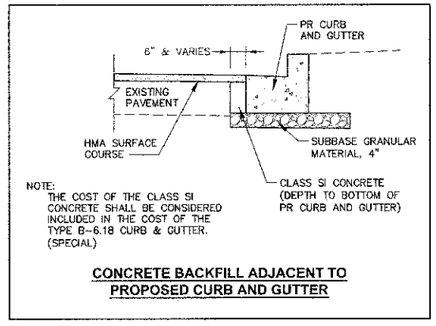



## PROPOSED TYPICAL SECTION

BATAVIA ROAD
STA 2048+50 TO STA 2051+42
N.T.S.

### EXISTING PAVEMENT CORE DATA

| CORE NUMBER | 1 | 2 | 3 | 4 |
|---|---|---|---|---|
| APPROXIMATE STATION | 2050+20 6' RT | 2064+50 6' RT | 2074+30 4' LT | 2057+30 4' LT |
| ASPHALT OVERLAY | 1.5" | 1.75" | 1.25" | 2.25" |
| REINFORCED CONCRETE | 10.5" | 11" | 10" | 10.75" |
| STONE SUBBASE | 18" | 17.25" | 18.75" | 17" |




NOTE:
THE COST OF THE CLASS SI CONCRETE SHALL BE CONSIDERED INCLUDED IN THE COST OF THE TYPE B-6.18 CURB & GUTTER. (SPECIAL)

### CONCRETE BACKFILL ADJACENT TO PROPOSED CURB AND GUTTER

### LEGEND

- (A) EXISTING ASPHALT SURFACE
- (B) EXISTING REINFORCED CONCRETE BASE
- (C) EXISTING AGGREGATE SUBBASE
- (D) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (E) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (F) HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL
- (G) COMBINATION CURB AND GUTTER REMOVAL
- (H) SIDEWALK REMOVAL - SPOT REPAIR / LOCATIONS VARY
- (1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL)
- (2) SUBBASE GRANULAR MATERIAL, TYPE B, 4"
- (3) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (4) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"
- (5) TOPSOIL, 4", SEEDING, FERTILIZER, EROSION CONTROL BLANKET
- (6) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
- (7) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF SIDEWALK)
- (8) CLASS D PATCH, 4"
- (9) CONCRETE BACKFILL - SEE DETAIL
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT

### HOT-MIX ASPHALT MIXTURE REQUIREMENTS

ALL ROADWAY PATCHING NOT RELATED TO STORM SEWER INSTALLATION SHALL BE COMPLETED AFTER MILLING OPERATIONS.

| OPERATION | MIXTURE TYPE | AIR VOIDS @ Ndes |
|---|---|---|
| ROADWAY RESURFACING | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm), 2.0" | 4% @ 70 Gyr. |
| PARKING RESURFACING | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 mm), 2.0" | 4% @ 50 Gyr. |
| DRIVEWAY RECONSTRUCTION (PE) | STABILIZED DRIVEWAYS, 10"<br>HMA SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2.0" | 4% @ 50 Gyr. |
| DRIVEWAY RECONSTRUCTION (CE) | STABILIZED DRIVEWAY PAVEMENT<br>HMA BINDER COURSE, IL-19.0, N50, 2.25"<br>HMA SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 2.0" | <br>4% @ 50 Gyr.<br>4% @ 50 Gyr. |
| PATCHING | CLASS D PATCHES, 4"<br>HMA BINDER COURSE, IL-19.0, N70, 4.0" | <br>4% @ 70 Gyr. |
| | CLASS D PATCHES, 10"<br>HMA BINDER COURSE, IL-19.0, N70, 10.0" (3 LIFTS) | <br>4% @ 70 Gyr. |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/INCH.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.



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| | | | |
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| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS**

SCALE: N/A | SHEET NO. 1 OF 3 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 4 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 19, 2012 @ 7:13 AM By: Jim Schmidt - Tab: 04 Typ Section - 22x34

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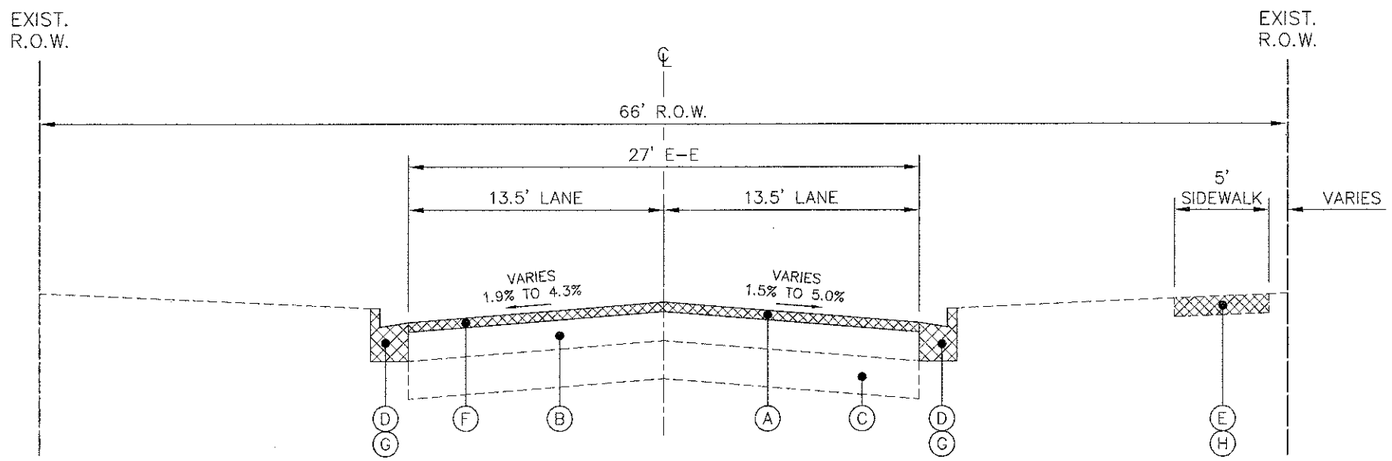
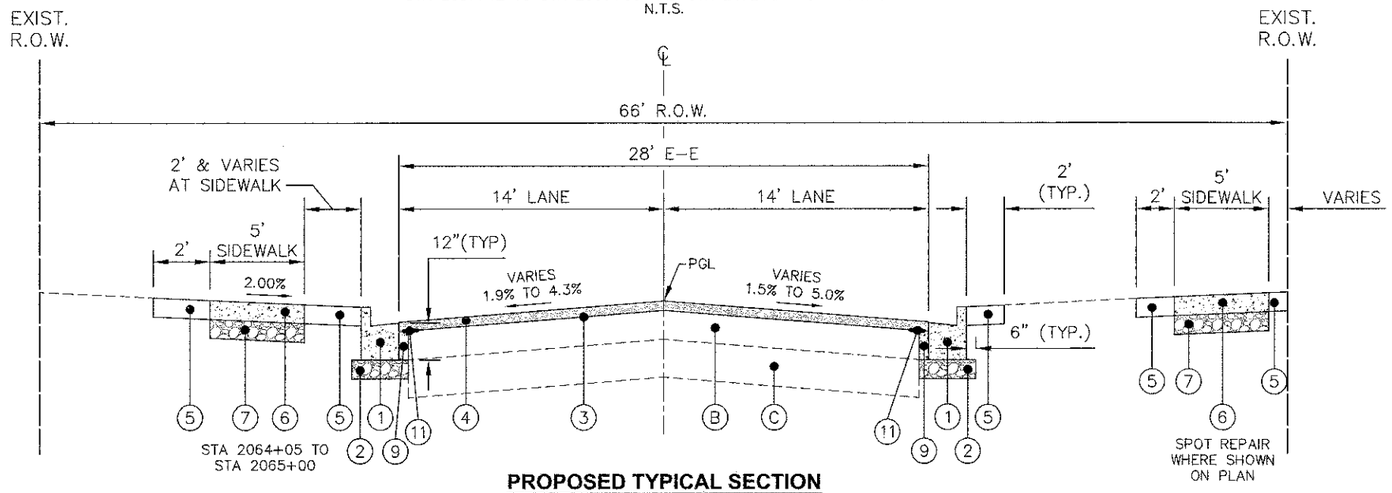



**EXISTING TYPICAL SECTION**

BATAVIA ROAD
STA 2051+42 TO STA 2065+00 AND STA 2072+50 TO STA 2081+05
N.T.S.




**PROPOSED TYPICAL SECTION**

BATAVIA ROAD
STA 2051+42 TO STA 2065+00 AND STA 2072+50 TO STA 2081+05
N.T.S.

**LEGEND**

- (A) EXISTING ASPHALT SURFACE
- (B) EXISTING REINFORCED CONCRETE BASE
- (C) EXISTING AGGREGATE SUBBASE
- (D) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (E) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (F) HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL
- (G) COMBINATION CURB AND GUTTER REMOVAL
- (H) SIDEWALK REMOVAL - SPOT REPAIR / LOCATIONS VARY
- (1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL)
- (2) SUBBASE GRANULAR MATERIAL, TYPE B, 4"
- (3) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (4) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"
- (5) TOPSOIL, 4", SEEDING, FERTILIZER, EROSION CONTROL BLANKET
- (6) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
- (7) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF SIDEWALK)
- (8) CLASS D PATCH, 4"
- (9) CONCRETE BACKFILL - SEE DETAIL
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT



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| | | | |
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| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS**

SCALE: N/A | SHEET NO. 2 OF 3 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3353 | 10-00031-00-RS | DUPAGE | 29 | 5 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:08 AM By: Jim Schmidt - Tab: 05 Typ Section - 22x34

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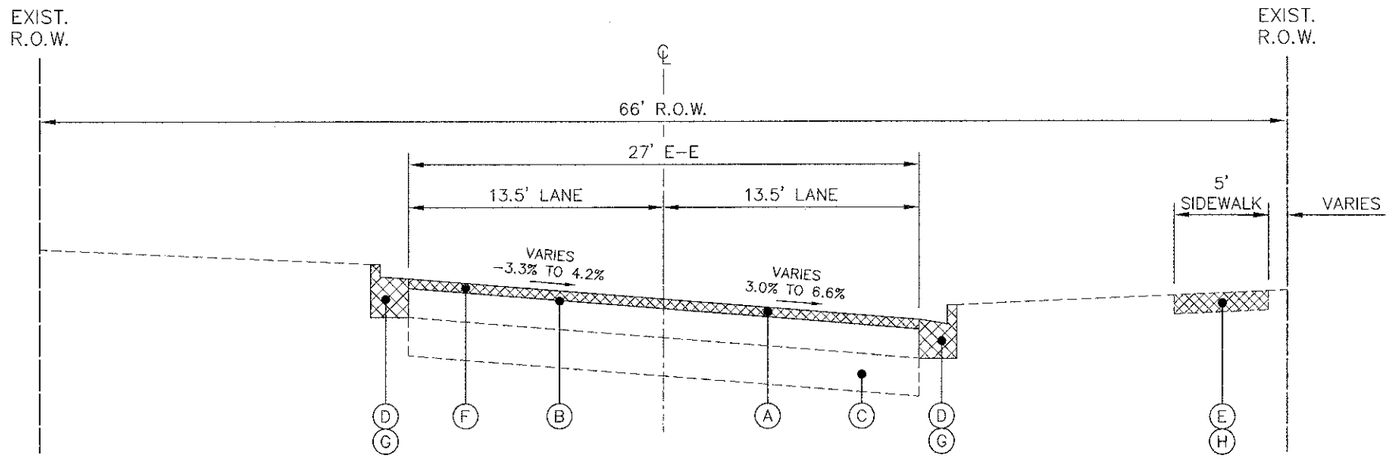
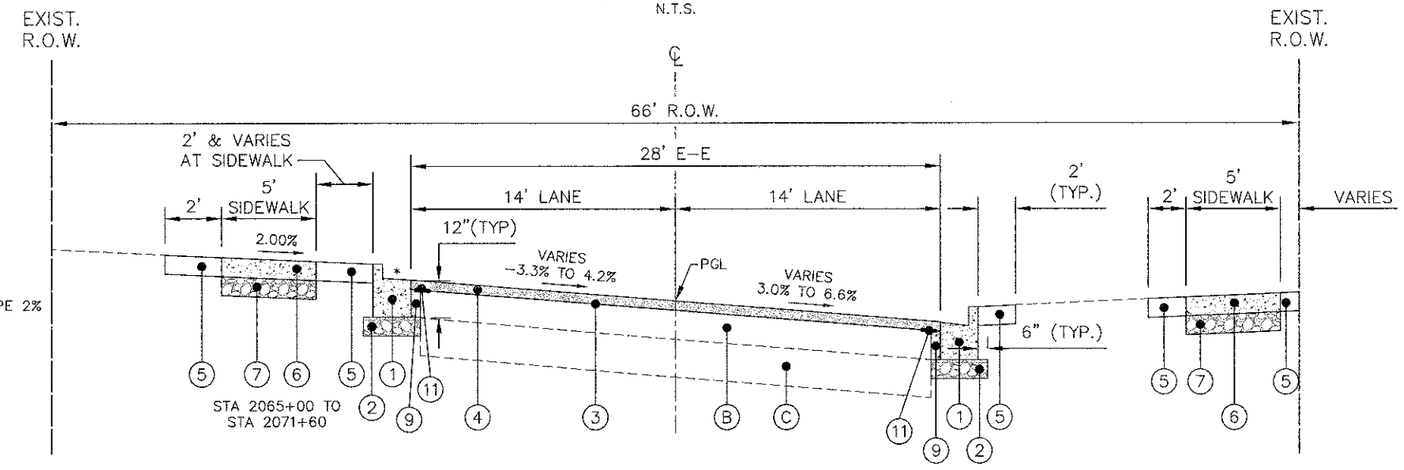



**EXISTING TYPICAL SECTION**

BATAVIA ROAD
STA 2065+00 TO STA 2072+50
N.T.S.




**PROPOSED TYPICAL SECTION**

BATAVIA ROAD
STA 2065+00 TO STA 2072+50
N.T.S.

**LEGEND**

- (A) EXISTING ASPHALT SURFACE
- (B) EXISTING REINFORCED CONCRETE BASE
- (C) EXISTING AGGREGATE SUBBASE
- (D) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (E) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (F) HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL
- (G) COMBINATION CURB AND GUTTER REMOVAL
- (H) SIDEWALK REMOVAL - SPOT REPAIR / LOCATIONS VARY
- (1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL)
- (2) SUBBASE GRANULAR MATERIAL, TYPE B, 4"
- (3) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (4) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"
- (5) TOPSOIL, 4", SEEDING, FERTILIZER, EROSION CONTROL BLANKET
- (6) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
- (7) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF SIDEWALK)
- (8) CLASS D PATCH, 4"
- (9) CONCRETE BACKFILL - SEE DETAIL
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT



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| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS**

SCALE: N/A | SHEET NO. 3 OF 3 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 6 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:09 AM By: Jim Schmidt - Tab: 06 Typ Section - 22x34

Path: H:\SDSKPROJ\WW1603\DWG\DWG FINAL ENG\WW1003-CVR

## ROADWAY QUANTITIES

| | | HMA SURF REM 2 SPL | HMA SURF REM BUTT JT | HMA SURF REM VAR DEPTH | COMB CURB GUTTER REM | COMB CC&G TB6.18 SPL | SUB GRAN MTL B 4 | HMA SC "D" N50 (2") | HMA SC "D" N70 (2") | STRIP REF CR CON TR | BIT MATLS PR CT | AGG PR CT |
|---|---|---|---|---|---|---|---|---|---|---|---|---|
| STATION | STATION | SQ YD | SQ YD | SQ YD | FOOT | FOOT | SQ YD | TON | TON | FOOT | GAL | TON |
| 2048+50 | 2050+50 | 1,182 | 14 | | 400 | 400 | 137 | 65 | 67 | 400 | 59 | 1 |
| 2050+50 | 2058+50 | 2,400 | 32 | 60 | 1,610 | 1,488 | 509 | 17 | 289 | 1,488 | 265 | 5 |
| 2058+50 | 2066+50 | 2,400 | 19 | | 1,600 | 1,524 | 522 | | 283 | 1,524 | 253 | 5 |
| 2066+50 | 2074+50 | 2,400 | 18 | 36 | 1,624 | 1,564 | 535 | | 280 | 1,564 | 250 | 5 |
| 2074+50 | 2081+05 | 2,356 | 38 | | 1,270 | 1,270 | 435 | | 264 | 1,270 | 236 | 5 |
| TOTAL | | 10,738 | 121 | 96 | 6,504 | 6,246 | 2,138 | 82 | 1,183 | 6,246 | 1,063 | 21 |

## PATCHING

| | | CL B PATCH T1 10 | CL B PATCH T2 10 | CL B PATCH T3 10 | CL B PATCH T4 10 | CL D PATCH T3 10 | CL D PATCH T1 4 | CL D PATCH T4 4 |
|---|---|---|---|---|---|---|---|---|
| STATION | STATION | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD |
| 2048+50 | 2050+50 | 4 | | | | | 2 | 112 |
| 2050+50 | 2058+50 | 26 | 33 | 24 | | 33 | | 113 |
| 2058+50 | 2066+50 | 11 | 36 | 30 | 31 | 20 | | 112 |
| 2066+50 | 2074+50 | 6 | 68 | | 25 | 15 | | 45 |
| 2074+50 | 2081+05 | 6 | 12 | | 135 | | | |
| TOTAL | | 53 | 149 | 54 | 191 | 68 | 2 | 382 |

## LANDSCAPING

| | | NITROGEN FERT NUTR | PHOSPHOROUS FERT NUTR | POTASSIUM FERT NUTR | EROSION CONTROL BLANKET | SEEDING CLASS 1A | TOPSOIL F&P, 4" |
|---|---|---|---|---|---|---|---|
| STATION | STATION | POUND | POUND | POUND | SQ YD | ACRE | SQ YD |
| 2048+50 | 2050+50 | 2 | 2 | 2 | 93 | 0.02 | 93 |
| 2050+50 | 2058+50 | 9 | 9 | 9 | 477 | 0.10 | 477 |
| 2058+50 | 2066+50 | 11 | 11 | 11 | 572 | 0.12 | 572 |
| 2066+50 | 2074+50 | 14 | 14 | 14 | 753 | 0.16 | 753 |
| 2074+50 | 2081+05 | 7 | 7 | 7 | 391 | 0.08 | 391 |
| TOTAL | | 43 | 43 | 43 | 2,286 | 0.5 | 2,286 |

## DRIVEWAY PAVEMENT, MEDIAN, AND SIDEWALK

| | | DRIVEWAY PAVEMENT REM | HMA DRIVEWAY PVT REM | BRICK PAVER R&R | SIDEWALK REM | MEDIAN REM | CONC MEDIAN SURF 4 | PC CONC SIDEWALK 5 | PC CONC SIDEWALK 7 | DETECTABLE WARNINGS | PCC DRIVEWAY PAVT 7 | STAB DRIVE PAVEMENT | STAB DRIVEWAYS 10 |
|---|---|---|---|---|---|---|---|---|---|---|---|---|---|
| STATION | STATION | SQ YD | SQ YD | SQ YD | SQ FT | SQ FT | SQ FT | SQ FT | SQ FT | SQ FT | SQ YD | SQ YD | SQ YD |
| 2048+50 | 2050+50 | | | | 125 | | | 125 | | | | | |
| 2050+50 | 2058+50 | 49 | 178 | | 1,107 | | | 857 | 250 | 60 | 49 | | 178 |
| 2058+50 | 2066+50 | 73 | 124 | 26 | 320 | | | 1,178 | 397 | 40 | 62 | | 112 |
| 2066+50 | 2075+00 | 66 | 227 | | 400 | | | 2,731 | 319 | 60 | 66 | 47 | 151 |
| 2075+00 | 2081+05 | 80 | 235 | | 378 | 288 | 520 | 213 | 165 | 40 | 80 | 159 | 76 |
| TOTAL | | 268 | 764 | 26 | 2,330 | 288 | 520 | 5,104 | 1,131 | 200 | 257 | 206 | 517 |

## PAVEMENT MARKING

THERMOPLASTIC PAVEMENT MARKINGS

| | | 4" | | 6" | 12" | 24" | LETTER& SYMBOLS | REC REF PVT MARKER |
|---|---|---|---|---|---|---|---|---|
| | | YELLOW | WHITE | WHITE | YELLOW | WHITE | | |
| STATION | STATION | FOOT | FOOT | FOOT | FOOT | FOOT | SQ FT | EACH |
| 2048+50 | 2050+50 | 380 | 668 | | | | 4.6 | 10 |
| 2050+50 | 2058+50 | 1,316 | | 190 | | 35 | | 34 |
| 2058+50 | 2066+50 | 1,406 | | 170 | | 22 | | 36 |
| 2066+50 | 2075+00 | 1,454 | | 140 | | 15 | | 38 |
| 2075+00 | 2081+05 | 1,050 | 48 | 35 | 50 | 28 | | 28 |
| TOTAL | | 6,352 | | 535 | 50 | 100 | 4.6 | 146 |

## STORM SEWER AND STORM SEWER STRUCTURES

| STRUCTURE NUMBER | STATION | OFFSET | LT./RT. | CB TC T3V F&G | CB TC T3 F&G | CB TC T23 F&G | CB TA 4 DIA T3 F&G | CB TA 4 DIA T3V F&G | MH TA 4 DIA T1 CL | MH TA 5 DIA TY 23 F&G | PIPE DIA. | LENGTH | SLOPE | SS TY 1, WM QUAL PIPE, 12 | STORM SEW CL B 1 8 (PVC SDR 26) | STORM SEW CL B 1 12 (PVC SDR 26) |
|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|
| | | | | EACH | EACH | EACH | EACH | EACH | EACH | EACH | INCH | FOOT | % | FOOT | FOOT | FOOT |
| ST01 | 2049+92 | 16.08 | RT | 1 | | | | | | | | | | | | |
| ST02 | 2049+94 | 16.08 | LT | | | | | 1 | | | | | | | | |
| ST07 | 2051+90 | 16.08 | RT | 1 | | | | | | | 12 | 27 | 0.44 | 27 | | |
| ST06 | 2052+12.34 | 28.34 | RT | 1 | | | | | | | 12 | 41 | 0.44 | 41 | | |
| ST05 | 2052+55.76 | 28.68 | RT | 1 | | | | | | | 12 | 45 | 0.67 | 45 | | |
| ST04 | 2052+98 | 16.08 | RT | | | | | 1 | | | 12 | 30 | 0.8 | | | 30 |
| | | | | | | | | | | | 8 | 20 | 0.5 | | 20 | |
| ST03 | 2052+98 | 16.08 | LT | 1 | | | | | | | | | | | | |
| ST08 | 2054+96 | 16.08 | RT | | | 1 | | | | | | | | | | |
| ST13 | 2055+60.67 | 29.33 | RT | 1 | | | | | | | 12 | 35 | 0.46 | | | 35 |
| ST12 | 2055+95.64 | 18.65 | RT | | | | | | 1 | | 12 | 18 | 0.67 | | | 18 |
| ST11 | 2056+14.1 | 16.71 | RT | | | 1 | | | | | 12 | 66 | 0.8 | | | 66 |
| ST10 | 2056+80 | 16.08 | RT | 1 | | | | | | | | | | | | |
| ST09 | 2056+81 | 16.08 | LT | 1 | | | | | | | | | | | | |
| ST14 | 2059+83 | 16.08 | RT | 1 | | | | | | | | | | | | |
| ST17 | 2063+48.16 | 27.28 | RT | | 1 | | | | | | 12 | 52 | 0.44 | | | 52 |
| ST16 | 2064+02.13 | 25.83 | RT | | | | | 1 | | | 12 | 45 | 0.44 | | | 45 |
| ST15 | 2063+97 | 20.28 | LT | | | | | | 1 | | | | | | | |
| ST18 | 2065+44 | 16.08 | LT | | 1 | | | | | | | | | | | |
| ST20 | 2066+75 | 16.08 | RT | | | | | 1 | | | 12 | 30 | 0.47 | | | 30 |
| ST19 | 2065+75 | 16.08 | LT | | | | | | | 1 | | | | | | |
| ST22 | 2069+25 | 16.08 | RT | | | | | 1 | | | 12 | 30 | 0.47 | | | 30 |
| ST21 | 2069+25 | 16.08 | LT | | | | | | | 1 | | | | | | |
| ST23 | 2070+47 | 16.08 | RT | | | | 1 | | | | | | | | | |
| ST24 | 2070+57 | 16.08 | RT | | | | 1 | | | | 12 | 10 | 0.5 | | | 10 |
| ST25 | 2072+12.45 | 21.69 | RT | | | | 1 | | | | | | | | | |
| ST26 | 2072+58 | 16.08 | LT | 1 | | | | | | | | | | | | |
| ST27 | 2077+09 | 16.08 | LT | 1 | | | | | | | | | | | | |
| TOTAL | | | | 11 | 2 | 2 | 3 | 6 | 2 | 2 | | | | 113 | 20 | 316 |

FOR STORM SEWERS: STATION SHOWN IS FOR THE UPSTREAM END OF THE PIPE

## STRUCTURE ADJUSTMENT SCHEDULE

| | | | INLETS ADJ NEW T3 F&G | INLETS ADJ NEW T3V F&G | MAN ADJUST | SANITARY MANHOLE ADJ | FIRE HYDNTS TO BE MVD | VALVE BOX ADJ |
|---|---|---|---|---|---|---|---|---|
| STATION | OFFSET | LT/RT | EACH | EACH | EACH | EACH | EACH | EACH |
| 2052+56 | 17.48 | RT | | | | 1 | | |
| 2052+56 | 22.28 | RT | | | | | | 1 |
| 2054+98 | 16.08 | LT | 1 | | | | | |
| 2055+55 | 19.05 | RT | | | | 1 | | |
| 2056+80 | 18.82 | RT | | | 1 | | | |
| 2059+82 | 16.08 | RT | | 1 | | | | |
| 2061+99 | 18.80 | RT | | | | 1 | | |
| 2062+50 | 17.47 | RT | | | | 1 | | |
| 2065+43 | 16.08 | RT | | 1 | | | | |
| 2067+49 | 23.09 | LT | | | | | 1 | |
| 2067+99 | 16.08 | RT | | 1 | | | | |
| 2072+57 | 16.08 | RT | | 1 | | | | |
| 2077+10 | 16.08 | RT | | 1 | | | | |
| 2077+70 | 22.78 | LT | | | | 1 | | |
| 2080+43 | 16.57 | RT | 1 | | | | | |
| TOTAL | | | 2 | 5 | 1 | 5 | 1 | 1 |

## STRUCTURE REMOVAL SCHEDULE

| | | | REMOV INLETS | REMOV MANHOLES |
|---|---|---|---|---|
| STATION | OFFSET | LT/RT | EACH | EACH |
| 2049+92 | 16.55 | RT | 1 | |
| 2049+94 | 16.77 | LT | 1 | |
| 2052+98 | 15.58 | RT | 1 | |
| 2052+98 | 15.50 | LT | 1 | |
| 2054+98 | 15.50 | RT | 1 | |
| 2055+95 | 18.65 | RT | | 1 |
| 2056+80 | 15.50 | RT | 1 | |
| 2056+81 | 15.50 | LT | 1 | |
| 2059+83 | 15.50 | LT | 1 | |
| 2065+44 | 15.50 | LT | 1 | |
| 2070+47 | 15.50 | RT | 1 | |
| 2072+58 | 15.50 | LT | 1 | |
| 2077+09 | 15.50 | LT | 1 | |
| TOTAL | | | 12 | 1 |



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| | | | | |
|---|---|---|---|---|
| USER NAME = | | DESIGNED - SWM/TVW | REVISED - | |
| | | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | | CHECKED - JRL | REVISED - | |
| PLOT DATE = | | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**

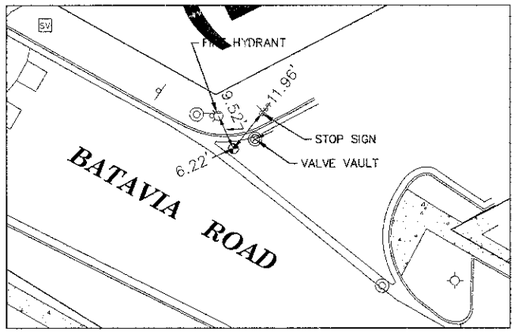
SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 7 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

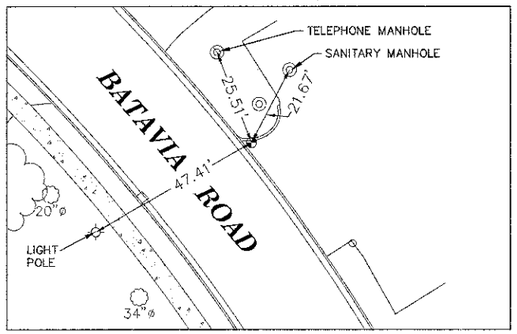
Plotted: October 19, 2012 @ 9:37 AM By: Jim Schmidt - Tab: 07 Schedule - 22x34

Path: H:\SDSKPROJ\WY1003\DWG\DWG FINAL ENG\WY1003-CVR

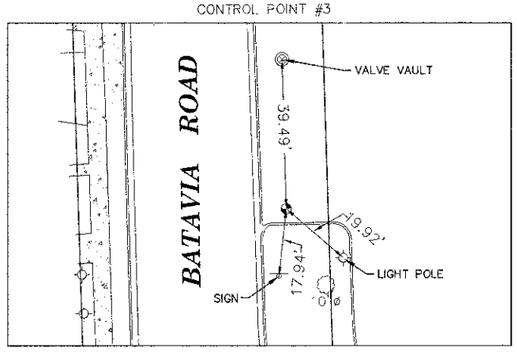
CONTROL POINT #1

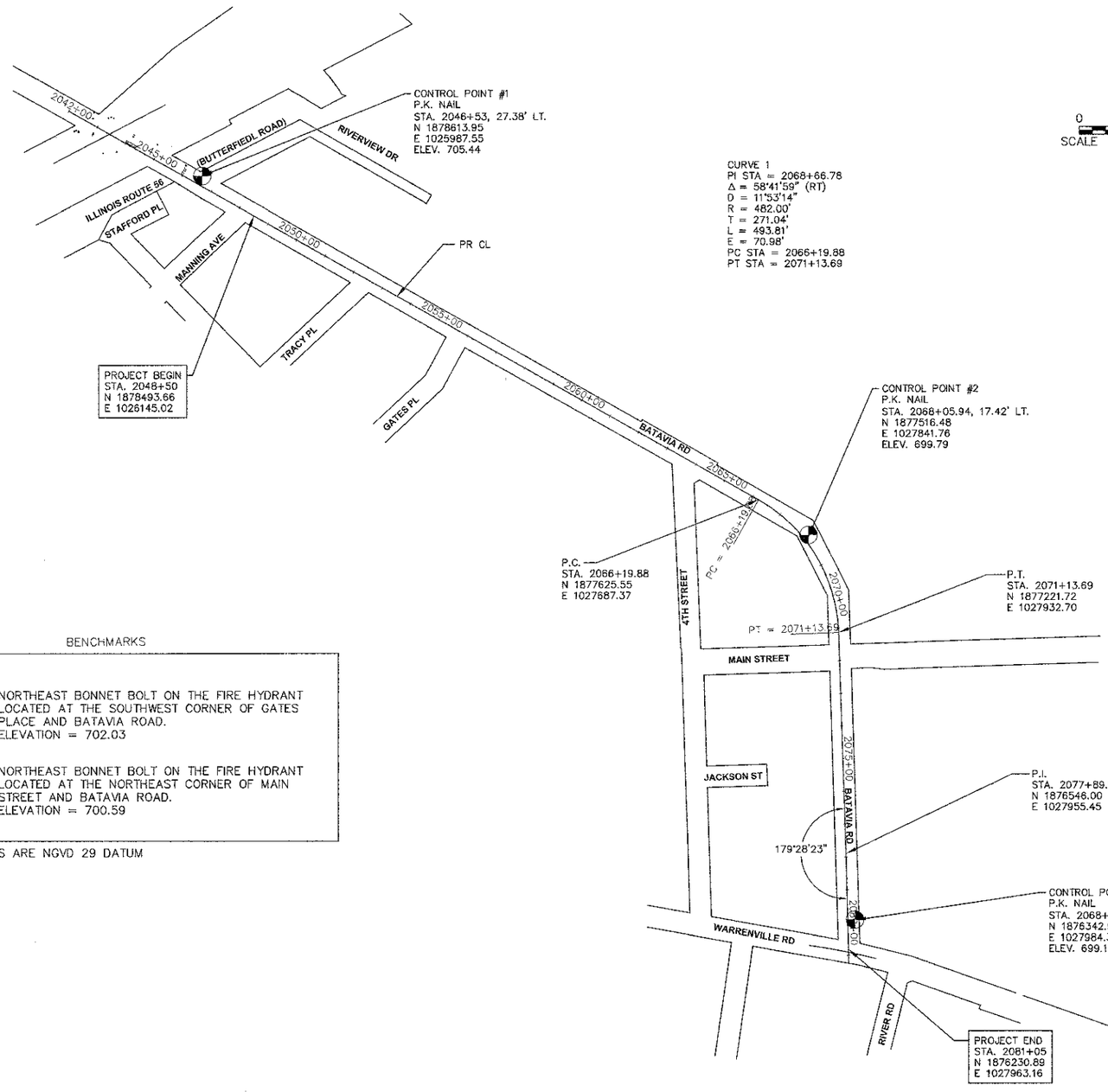



CONTROL POINT #2




CONTROL POINT #3




CURVE 1
PI STA = 2068+66.78
Δ = 58°41'59" (RT)
D = 11°53'14"
R = 482.00'
T = 271.04'
L = 493.81'
E = 70.98'
PC STA = 2066+19.88
PT STA = 2071+13.69

N

0 200' 400'
SCALE FEET

BENCHMARKS

| | |
|---|---|
| BM#1 | NORTHEAST BONNET BOLT ON THE FIRE HYDRANT LOCATED AT THE SOUTHWEST CORNER OF GATES PLACE AND BATAVIA ROAD. ELEVATION = 702.03 |
| BM#2 | NORTHEAST BONNET BOLT ON THE FIRE HYDRANT LOCATED AT THE NORTHEAST CORNER OF MAIN STREET AND BATAVIA ROAD. ELEVATION = 700.59 |

ELEVATIONS ARE NGVD 29 DATUM



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| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES, AND BENCHMARKS**

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 8 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:38 AM By: Jim Schmidt — Tab: 08 Alignment — 22x34

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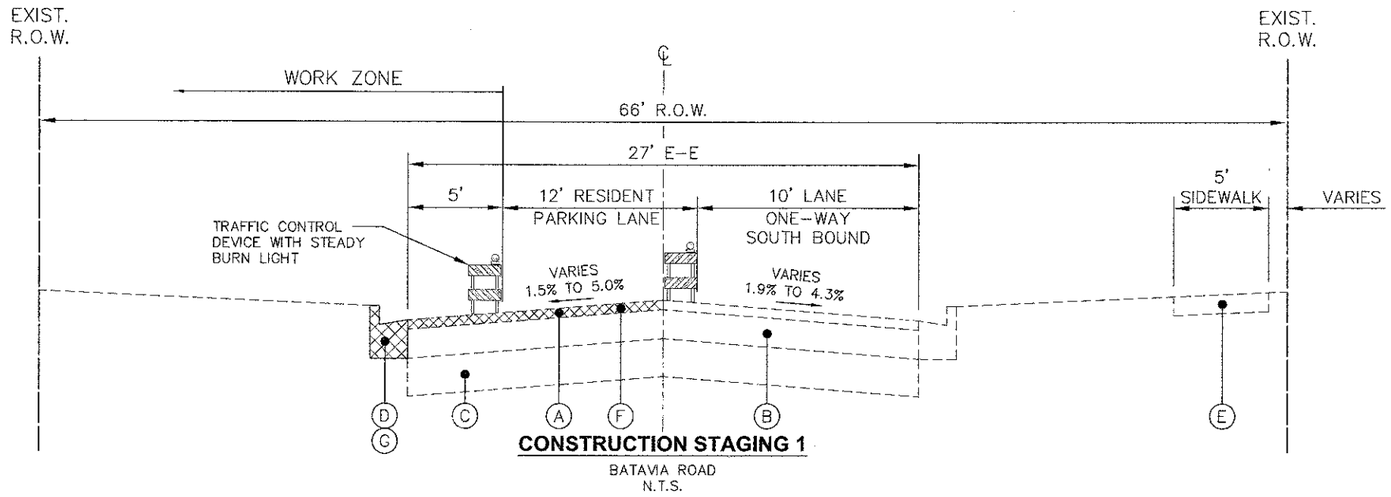
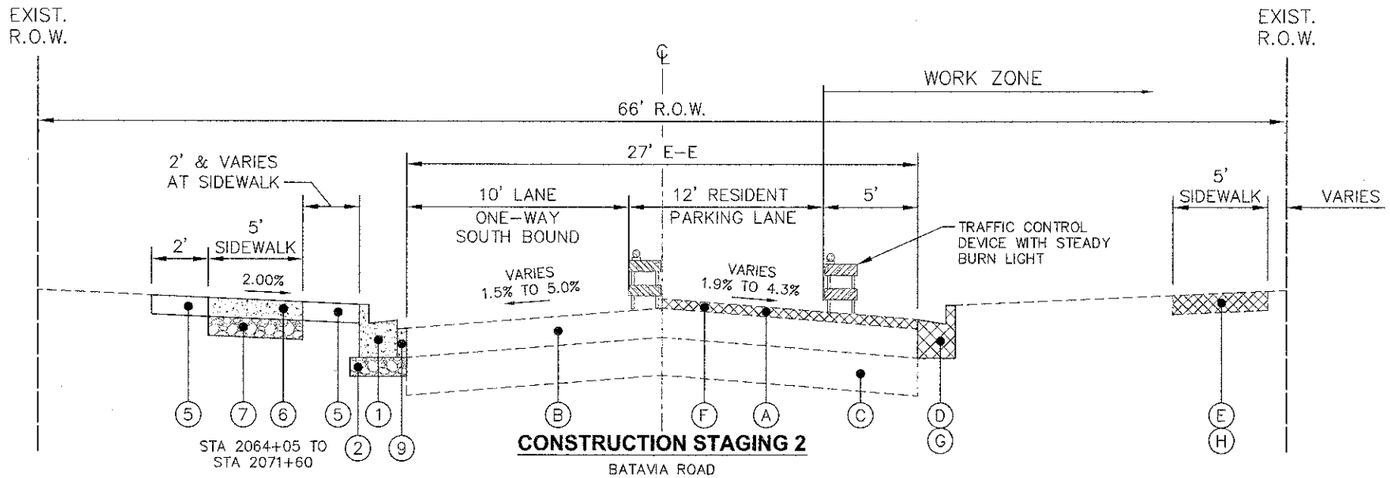



**CONSTRUCTION STAGING 1**

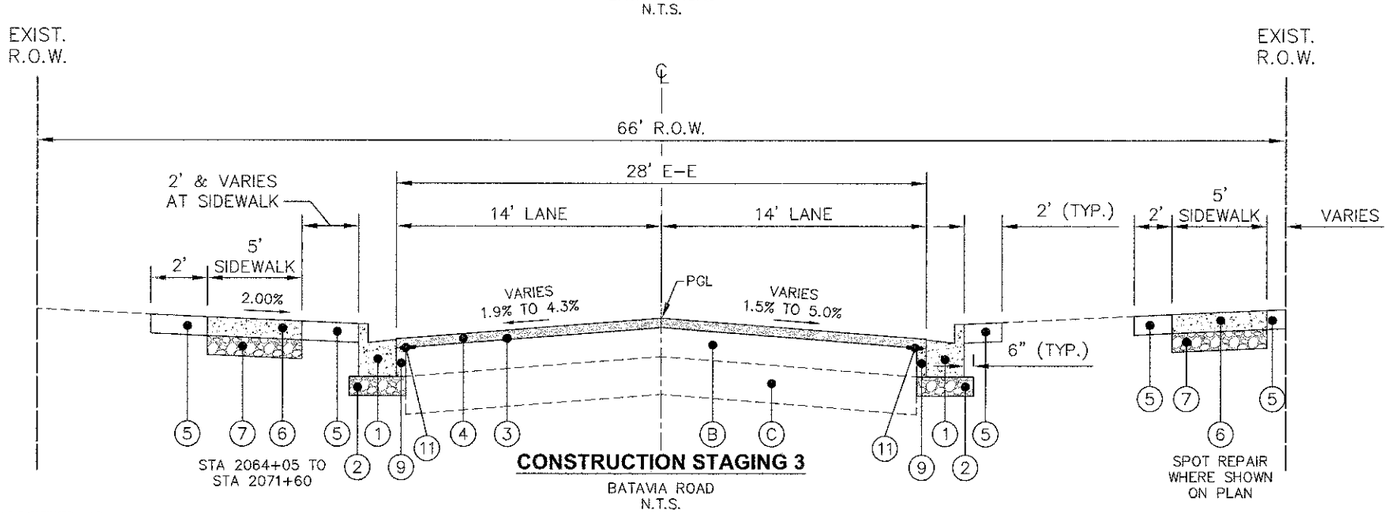
BATAVIA ROAD
N.T.S.




STA 2064+05 TO STA 2071+60

**CONSTRUCTION STAGING 2**

BATAVIA ROAD
N.T.S.




STA 2064+05 TO STA 2071+60

**CONSTRUCTION STAGING 3**

BATAVIA ROAD
N.T.S.

## TRAFFIC CONTROL AND PROTECTION GENERAL NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, CONTRACT SPECIAL PROVISIONS, CONSTRUCTION STAGING PLAN, DETOUR PLAN, THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS," AND AS DIRECTED BY THE ENGINEER. TRAFFIC CONTROL SHOWN IN THE CONSTRUCTION STAGING PLAN AND DETOUR PLAN REPRESENT A GUIDE FOR THE SAFE MANAGEMENT OF TRAFFIC DURING THE EXECUTION OF THE WORK. MODIFICATIONS MAY BE NECESSARY DUE TO LOCAL CONDITIONS AT THE TIME OF CONSTRUCTION. ANY PROPOSED CHANGES BY THE CONTRACTOR TO THESE TRAFFIC CONTROL PLANS SHALL BE APPROVED BY THE ENGINEER PRIOR TO BEING IMPLEMENTED. ANY MODIFICATIONS OR ADDITIONS REQUIRED BY THE ENGINEER WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL) UNLESS A SEPARATE PAY ITEM HAS BEEN ESTABLISHED FOR THE WORK.
2. ANY EXISTING OR TEMPORARY MARKINGS WHICH CONFLICT WITH MARKINGS REQUIRED FOR CONSTRUCTION STAGING SHALL BE REMOVED ACCORDING TO SECTION 783 OF THE STANDARD SPECIFICATIONS. WATER BLASTING SHALL BE USED TO REMOVE EXISTING PAVEMENT MARKINGS ON FINAL PAVEMENT SURFACES.
3. ACCESS TO ADJACENT PROPERTIES AND SIDE STREETS SHALL BE MAINTAINED AT ALL TIMES, EXCEPT AS NOTED HEREIN OR AS APPROVED BY THE ENGINEER.
4. TEMPORARY PAVEMENT MARKING APPLIED TO FINAL PAVEMENT SURFACES AND EXISTING PAVEMENT SURFACES TO REMAIN SHALL BE PAVEMENT MARKING TAPE, TYPE III.
5. PLATING AND/OR TEMPORARY STRUCTURE ADJUSTMENTS MAY BE REQUIRED DUE TO THE STAGING OF CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
6. DURING THE CONCRETE CURING PROCESS RESIDENTS WILL BE ALLOWED TO PARK ADJACENT TO THEIR DRIVEWAYS ALONG BATAVIA ROAD IN THE RESIDENT PARKING LANE AREAS. RESIDENTS SHALL BE NOTIFIED IN WRITING A MINIMUM OF 48 HOURS IN ADVANCE OF LOSING ACCESS. THE CONTRACTOR SHALL TEMPORARILY SIGN AND BARRICADE THE DESIGNATED PARKING AREAS IN ORDER TO AVOID CONFLICTS DURING CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
7. DURING CONSTRUCTION BATAVIA ROAD WILL BE A ONE-WAY SOUTH BOUND ROAD FOR LOCAL TRAFFIC. THE DETOUR ROUTE SHOWN ON THE PLANS SHALL BE IMPLEMENTED IMMEDIATELY PRIOR TO ONE-WAY OPERATION. IF NOT IMMEDIATELY PRIOR, DETOUR SIGNS SHALL BE BAGGED UNTIL IN USE.
8. PRIOR TO CONSTRUCTION STAGE 1, THE COMMERCIAL PARKING SPACES BETWEEN MANNING AVENUE AND TRACY PLACE SHALL BE RESTRIPED. SEE THE PARKING STALL PAVEMENT MARKING DETAIL SHEET FOR DETAILS.
9. STORM SEWER CROSSINGS AND FULL ROADWAY PATCHING SHALL BE CONSTRUCTED ONE HALF AT A TIME.
10. PRIOR TO INSTALLATION OF THE DETOUR SIGNING, THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470

## LEGEND

- (A) EXISTING ASPHALT SURFACE
- (B) EXISTING REINFORCED CONCRETE BASE
- (C) EXISTING AGGREGATE SUBBASE
- (D) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18
- (E) EXISTING PORTLAND CEMENT CONCRETE SIDEWALK
- (F) HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL
- (G) COMBINATION CURB AND GUTTER REMOVAL
- (H) SIDEWALK REMOVAL - SPOT REPAIR / LOCATIONS VARY
- (1) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL)
- (2) SUBBASE GRANULAR MATERIAL, TYPE B, 4"
- (3) BITUMINOUS MATERIALS (PRIME COAT) AND AGGREGATE (PRIME COAT)
- (4) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2.0"
- (5) TOPSOIL, 4", SEEDING, FERTILIZER, EROSION CONTROL BLANKET
- (6) PORTLAND CEMENT CONCRETE SIDEWALK, 5" (7" AT DRIVEWAYS)
- (7) AGGREGATE BASE COURSE, TYPE B, 2" (INCLUDED IN COST OF SIDEWALK)
- (8) CLASS D PATCH, 4"
- (9) CONCRETE BACKFILL - SEE DETAIL ON TYPICAL SECTION SHEET
- (10) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2.0"
- (11) STRIP REFLECTIVE CRACK CONTROL TREATMENT



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| | | |
|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - |
| | DRAWN - KKP/CLN/JPS | REVISED - |
| PLOT SCALE = | CHECKED - JRL | REVISED - |
| PLOT DATE = | DATE - 8/24/12 | REVISED - |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

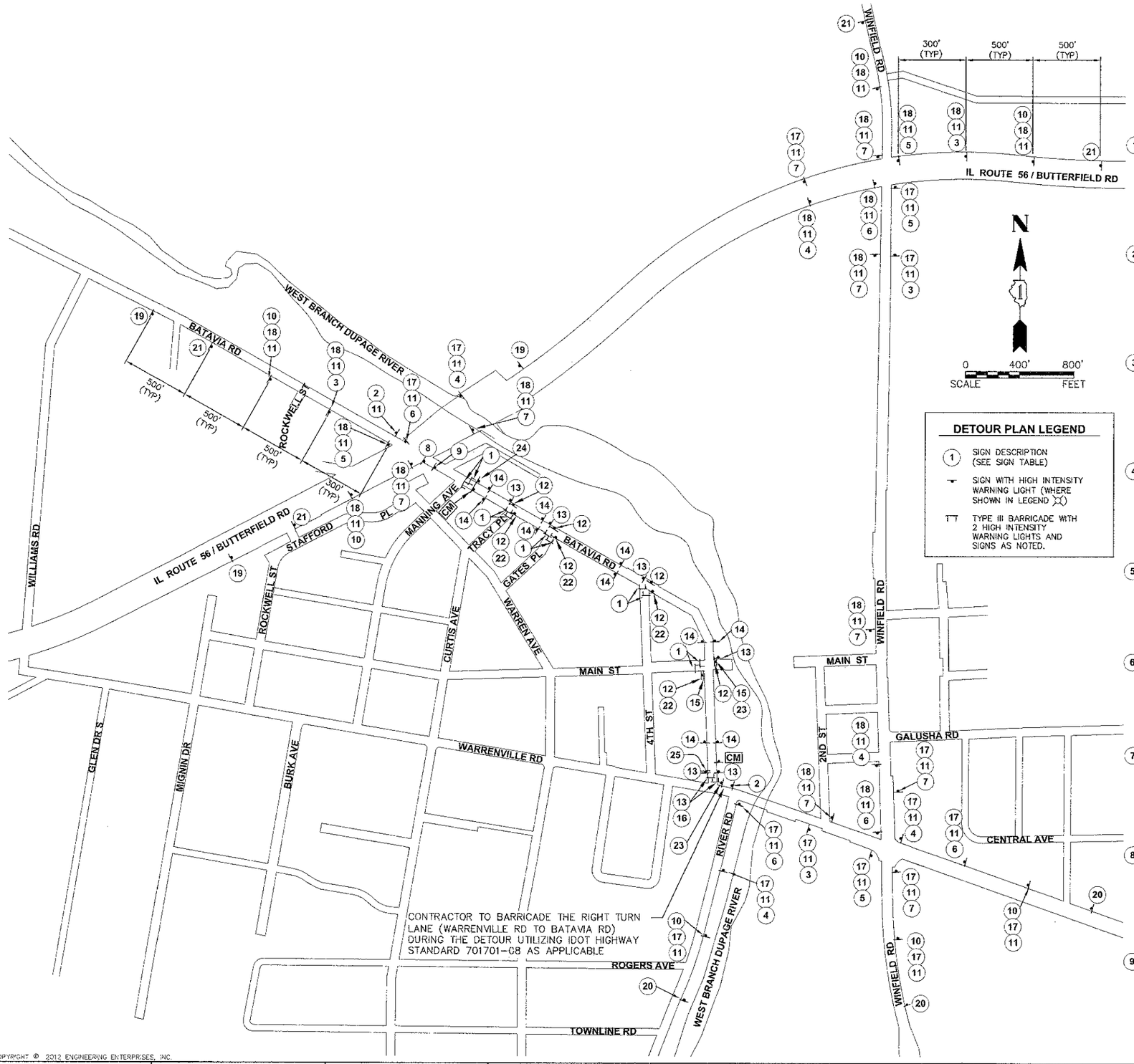
**SUGGESTED CONSTRUCTION STAGING PLAN**

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 9 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: November 18, 2012 @ 11:48 AM By: Jim Schmidt - Tab: 09 Staging - 22x34

Path: N:\ESP\PROJ\WW1003\DWG\DWG FINAL DWG\WW1003-E36




**DETOUR PLAN LEGEND**

- (1) SIGN DESCRIPTION (SEE SIGN TABLE)
- SIGN WITH HIGH INTENSITY WARNING LIGHT (WHERE SHOWN IN LEGEND)
- TYPE III BARRICADE WITH 2 HIGH INTENSITY WARNING LIGHTS AND SIGNS AS NOTED.

NOTES:

1. LOCATIONS OF ALL SIGNS ARE APPROXIMATE. FINAL LOCATION SHALL BE BASED ON FIELD CONDITIONS AND AS APPROVED BY THE ENGINEER.
2. SIGN SPACING = 500 FEET UNLESS NOTED OTHERWISE OR AS DIRECTED BY THE ENGINEER.
3. STREET NAME SIGN (11) TO BE PLACED ABOVE DETOUR SIGNS AND BELOW WARNING SIGNS.
4. CHANGEABLE MESSAGE SIGNS [CM] TO BE INSTALLED A MINIMUM OF TWO WEEKS IN ADVANCE OF ANY CONSTRUCTION WORK NOTIFYING PUBLIC OF PENDING ROAD WORK AND CLOSURE. THE CHANGEABLE MESSAGE SIGNS SHALL BE REMOVED ONCE CONSTRUCTION BEGINS AND THE DETOUR IS IN PLACE.

(1) ROAD CLOSED TO THRU TRAFFIC — R11-4, 60X30
ON TYPE III BARRICADE W/ FLASHING LIGHT (2 STAGGERED ACROSS ROAD)

(2) END DETOUR — M4-8A, 24X18

(3) DETOUR — M4-9L, 30X30

(4) DETOUR — M4-9R, 30X30

(5) DETOUR — M4-9L, 30X24

(6) DETOUR — M4-9R, 30X24

(7) DETOUR — M4-9, 30X24

(8) ROAD CONSTRUCTION AHEAD — W20-1, 36X36

(9) ROAD CLOSED AHEAD — W20-3, 36X36

(10) DETOUR AHEAD — W20-2, 48X48

(11) BATAVIA RD — 30X12, BLACK ON ORANGE

(12) ONE WAY — R6-1R, 36X12

(13) DO NOT ENTER — R5-1, 30X30

(14) WRONG WAY — R5-1a, 36X24

(15) ONE WAY — R6-1L, 36X12

(16) ROAD CLOSED — R11-2, 48X30
ON TYPE III BARRICADE W/ FLASHING LIGHT (2 STAGGERED ACROSS ROAD)

(17) NORTH — M3-1, 24X12

(18) SOUTH — M3-3, 24X12

(19) BATAVIA RD CLOSED SOUTH OF MANNING AVE LOCAL TRAFFIC ONLY — BLACK ON ORANGE

(20) BATAVIA ROAD CLOSED USE DETOUR — BLACK ON ORANGE

(21) BATAVIA RD CLOSED SOUTH OF MANNING AVE USE DETOUR — BLACK ON ORANGE

(22) R3-2, 24X24

(23) R3-1, 24X24

(24) BEGIN ONE WAY — R6-6, 18X24

(25) END ONE WAY — R6-7, 18X24

[CM] CHANGEABLE MESSAGE SIGN



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| | | |
|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - |
| | DRAWN - KKP/CLN/JPS | REVISED - |
| PLOT SCALE = | CHECKED - JRL | REVISED - |
| PLOT DATE = | DATE - 8/24/12 | REVISED - |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**BATAVIA ROAD DETOUR PLAN**

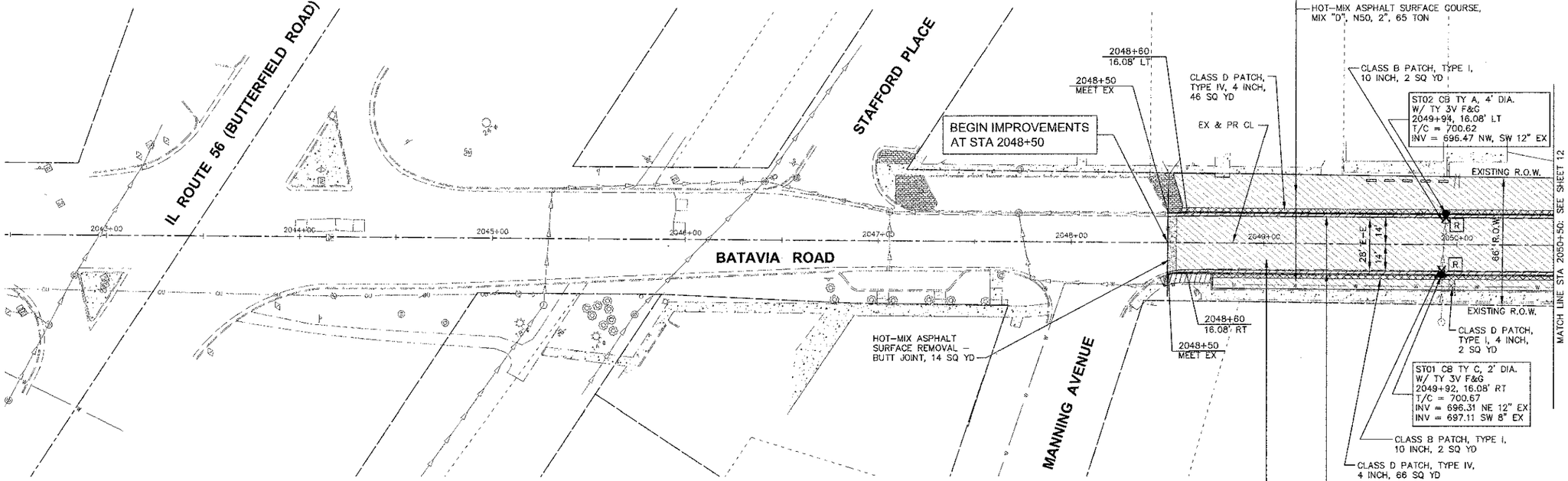
SCALE: N.T.S. | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

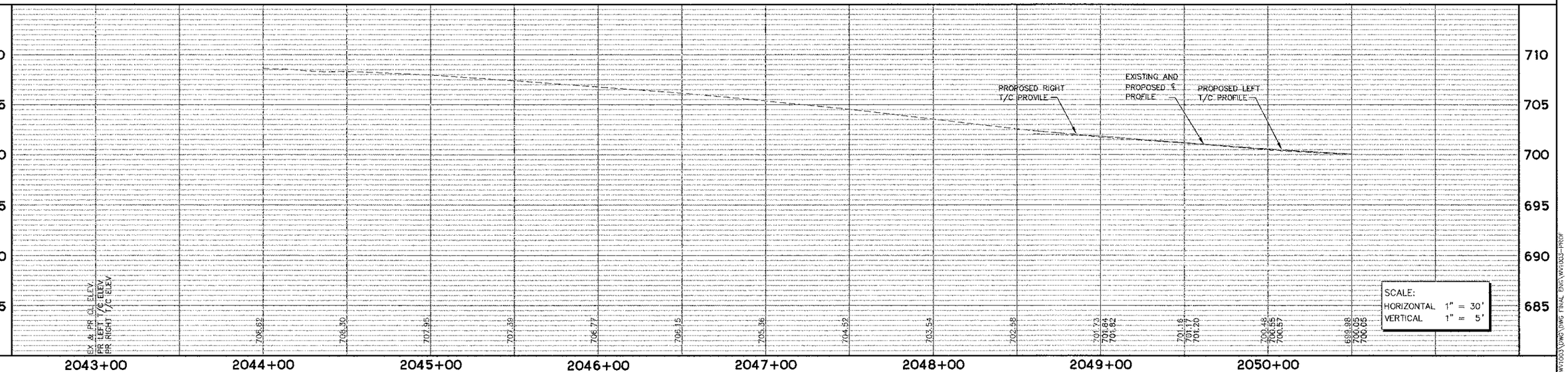
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 10 |
| | | CONTRACT NO. 63758 | | |

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866)

Plotted: November 19, 2012 @ 1:26 PM By: Jim Schmidt - Tab: 10 Detour Plan 22x34

Path: N:\SHSPROJ\WV1003\DWG\DWG FINAL ENG\WV1003-DETOUR 40ISC



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| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

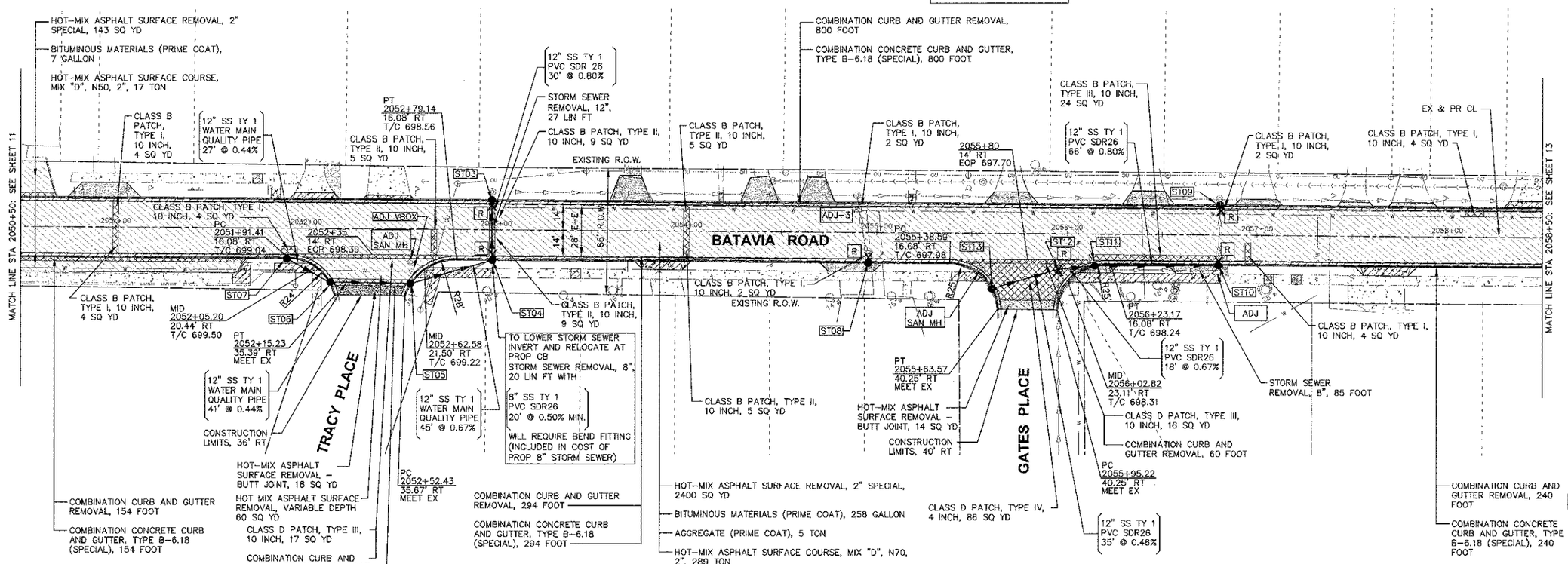
**BATAVIA ROAD PLAN AND PROFILE**

SCALE: 1"=30' | SHEET NO. 1 OF 5 SHEETS | STA. 2042+50 TO STA. 2050+50

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 11 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 at 4:07 PM by Jim Schmidt - Top: I:\P&P - 22x34
Path: H:\SDSKPROJ\W91003\DWG\DWG FINAL ENG\W91003-PROF

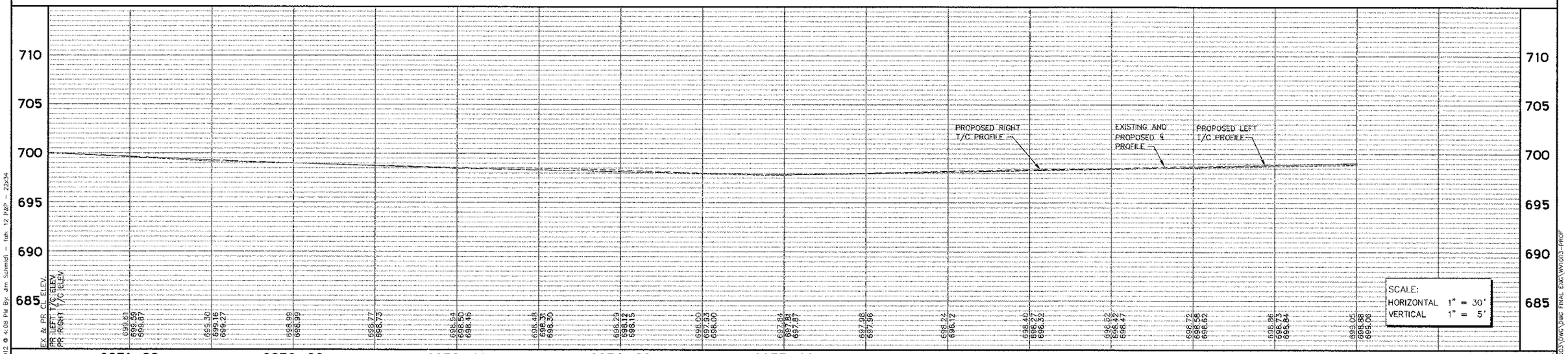
| ST03 CB TY C, 2' DIA. | ST04 CB TY A, 4' DIA. | ST05 CB TY C, 2' DIA. | ST06 CB TY C, 2' DIA. | ST07 CB TY C, 2' DIA. | ST08 CB TY C, 2' DIA. | ST09 CB TY C, 2' DIA. | ST10 CB TY C, 2' DIA. | ST11 CB TY C, 2' DIA. | ST12 MH TY A, 4' DIA. | ST13 CB TY C, 2' DIA. |
|---|---|---|---|---|---|---|---|---|---|---|
| W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 3V F&G | W/ TY 23 F&G | W/ TY 1 CL | W/ TY 3V F&G |
| 2052+98, 16.08' LT | 2052+98, 16.08' RT | 2052+55.76, 28.66' RT | 2052+12.34, 28.34' RT | 2051+90.16, 16.08' RT | 2054+96, 16.08' RT | 2056+81, 16.08' LT | 2056+80, 16.08' RT | 2056+14.1, 16.71' RT | 2055+95.54, 18.65' RT | 2055+60.57, 29.33' RT |
| T/C = 698.51 | T/C = 698.46 | T/C = 699.62 | T/C = 699.90 | T/C = 699.05 | T/C = 697.86 | T/C = 698.36 | T/C = 698.41 | T/C = 698.21 | RIM = 697.80 | T/C = 698.07 |
| INV = 694.84 NE 12" EX | INV = 695.80 NW 8" | INV = 695.44 NW, SE 12" | INV = 695.62 N, SE 12" | INV = 695.74 S 12" | INV = 693.51 NE 12" EX | INV = 694.09 NE, SW 12" EX | INV = 694.30 NE 12" EX | INV = 694.83 NW, SE 12" | INV = 694.95 NW, SE 12" | INV = 695.11 SE, 12" |
| INV = 694.90 SW 12" | INV = 695.14 NE&NW 12" | | | | | | INV = 695.70 SW 8" EX | | INV = 695.08 SW 8" EX | |
| | | | | | | | INV = 694.30 NW | | | |




## LEGEND

- CLASS B OR D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL – BUTT JOINT
- HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL OR VARIABLE DEPTH
- EXISTING CONCRETE SIDEWALK OR DRIVEWAY TO REMAIN IN PLACE
- SIDEWALK OR PCC DRIVEWAY REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT
- ADJ-3V — STRUCTURE TO BE ADJUSTED; NEW F&G/LID TYPE (WHERE SHOWN)
- R / X — STRUCTURE TO BE REMOVED

NOTES:
1. SEE "PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN" FOR SIDEWALK AND DRIVEWAY INFORMATION.



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|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

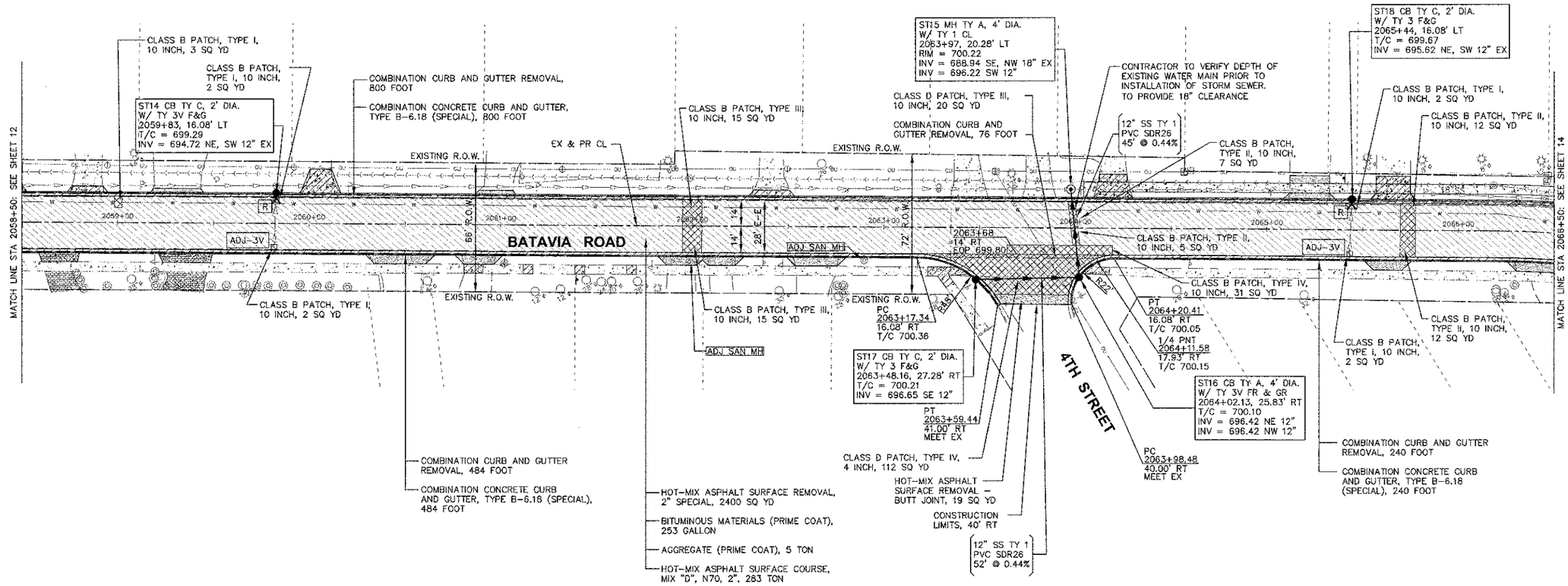
**BATAVIA ROAD PLAN AND PROFILE**

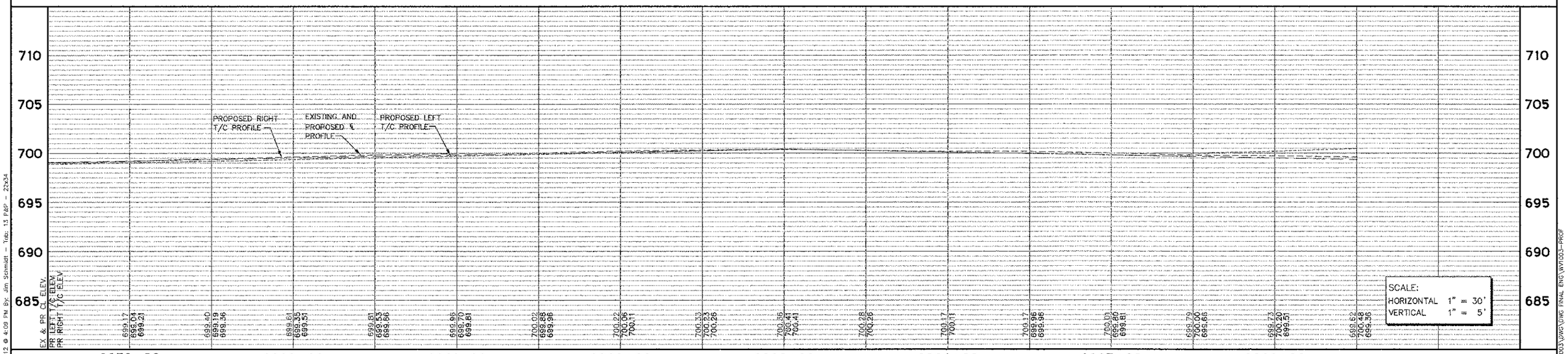
SCALE: 1"=30' | SHEET NO. 2 OF 5 SHEETS | STA. 2050+50 TO STA. 2058+50

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 12 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 16, 2012 @ 4:08 PM By: Jim Schmidt – Path: 12_P&P – 22x34

Path: H:\SDSKPROJ\WW003\DWG\DWG\FINAL ENG\WW003-PROF



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|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**BATAVIA ROAD PLAN AND PROFILE**

SCALE: 1"=30' | SHEET NO. 3 OF 5 SHEETS | STA. 2058+50 TO STA. 2066+50

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 13 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:09 PM By: Jim Sinmait - Tab: 13 P&P - 22x34

Path: H:\SDSKPROJ\WW1603\DWG\DWG FINAL ENG\WW1603-PROF

## WARRENVILLE GROVE FOREST PRESERVE PARKING LOT

CURVE 1
PI STA = 2068+66.78
Δ = 58°41'59" (RT)
D = 11°53'14"
R = 482.00'
T = 271.04'
L = 493.81'
E = 70.98'
PC STA = 2066+19.88
PT STA = 2071+13.69

ST21 MH TY A, 5' DIA.
W/ TY 23 F&G
2069+25, 16.08' LT
T/C = 699.91
INV = 686.90± NW, SE 24" EX
INV = 694.75 W 12"

CLASS B PATCH, TYPE II, 10 INCH, 9 SQ YD

CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN PRIOR TO INSTALLATION OF STORM SEWER. TO PROVIDE 18" CLEARANCE

12" SS TY 1 PVC SDR26 30' @ 0.47%

FIRE HYDRANT TO BE MOVED

CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN PRIOR TO INSTALLATION OF STORM SEWER. TO PROVIDE 18" CLEARANCE

12" SS TY 1 PVC SDR26 30' @ 0.47%

ST19 MH TY A, 5' DIA.
W/ TY 23 F&G
2066+75, 16.08' LT
T/C = 700.50
INV = 687.50± NW, SE 24" EX
INV = 695.50 SW 12"

CLASS B PATCH, TYPE II, 10 INCH, 9 SQ YD

MATCH LINE STA 2066+50 SEE SHEET 13

CLASS B PATCH, TYPE II, 10 INCH, 9 SQ YD

ST20 CB TY A, 4' DIA.
W/ TY 3V F&G
2066+75, 16.08' RT
T/C = 699.24
INV = 695.64 NE 12"

CURVE 1

ADJ-3V

CLASS B PATCH, TYPE II, 5 SQ YD

CLASS B PATCH, TYPE I, 10 INCH, 2 SQ YD

CLASS B PATCH, TYPE I, 10 INCH, 2 SQ YD

R.O.W. IS OCCUPIED

CLASS B PATCH, TYPE IV, 10 INCH, 25 SQ YD

CLASS B PATCH, TYPE II, 10 INCH, 9 SQ YD

ST22 CB TY A, 4' DIA.
W/ TY 3V F&G
2069+25, 16.08' RT
T/C = 698.20
INV = 694.89 E 12"

R.O.W. IS OCCUPIED

CLASS B PATCH, TYPE II, 10 INCH, 5 SQ YD

EX & PR CL

PC 2071+42.65 16.08' RT T/C 698.14

CLASS B PATCH, TYPE II, 10 INCH, 4 SQ YD

2071+87 14' RT EOP 697.90

12" SS TY 1 PVC SDR26 10' @ 0.50%

ST23 CB TY A, 4' DIA.
W/ TY 3 F&G
2070+47, 16.08' RT
T/C = 697.79
INV = 692.62 NE 12" EX
INV=692.72 S 12"

ST24 CB TY A, 4' DIA.
W/ TY 3 F&G
2070+57, 16.08' RT
T/C = 697.83
INV = 692.77 N 12"

MID-PT 2071+59.46 21.69' RT T/C 698.60

PT 2071+69.54 36.27' RT MEET EX

HOT-MIX ASPHALT SURFACE REMOVAL VARIABLE DEPTH, 36 SQ YD

CONSTRUCTION LIMITS, 36' RT

HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL, 2400 SQ YD

BITUMINOUS MATERIALS (PRIME COAT), 250 GALLON

AGGREGATE (PRIME COAT), 5 TON

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2", 280 TON

COMBINATION CURB AND GUTTER REMOVAL, 490 FOOT

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL), 490 FOOT

CLASS D PATCH, TYPE III, 10 INCH, 15 SQ YD

COMBINATION CURB AND GUTTER REMOVAL, 60 FOOT

ST26 CB TY C, 2' DIA.
W/ TY 3V F&G
2072+58, 16.08' LT
T/C = 698.46
INV = 695.02 W 12" EX

CLASS B PATCH, TYPE I, 10 INCH, 2 SQ YD

EXISTING R.O.W.

PT 2072+29.71 16.08' RT T/C 698.46

ADJ-3V

MAIN STREET

ST25 CB TY A, 4' DIA.
W/ TY 3 F&G
2072+12.85, 21.73' RT
T/C = 698.35
INV = 689.80± N, S 18" EX

CLASS D PATCH, TYPE IV, 4 INCH, 45 SQ YD

PC 2072+02.78 36.39' RT MEET EX

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT, 18 SQ YD

EXISTING R.O.W.

CLASS B PATCH, TYPE I, 10 INCH, 2 SQ YD

COMBINATION CURB AND GUTTER REMOVAL, 820 FOOT

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL), 820 FOOT

CLASS B PATCH, TYPE II, 10 INCH, 9 SQ YD

66' R.O.W.

BATAVIA ROAD

14' 26' 14'

CLASS B PATCH, TYPE II, 10 INCH, 9 SQ YD

COMBINATION CURB AND GUTTER REMOVAL, 254 FOOT

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.18 (SPECIAL), 254 FOOT

FIRE DEPARTMENT

MATCH LINE STA 2074+50 SEE SHEET 15

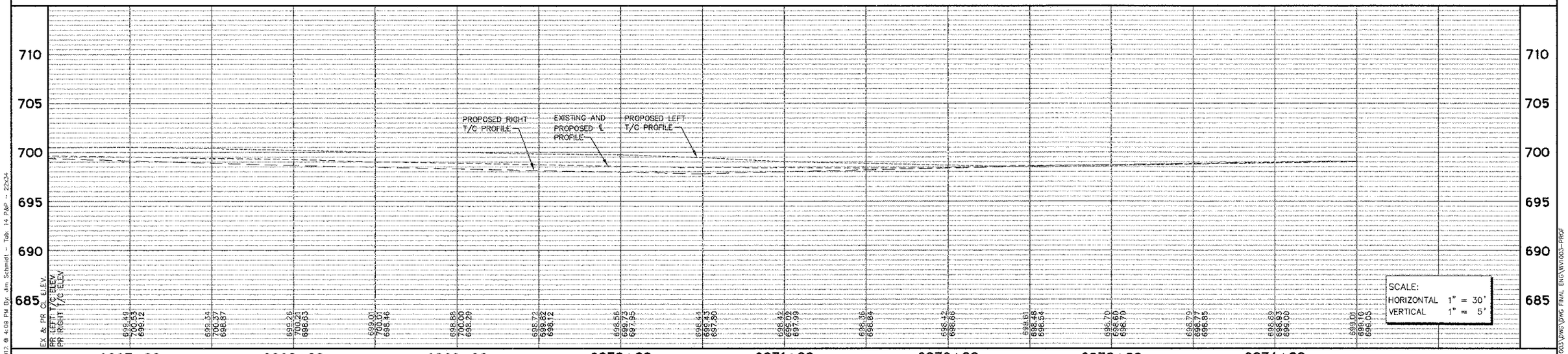
N

0 30' 60'
SCALE FEET

### LEGEND

- CLASS B OR D PATCHES
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
- HOT-MIX ASPHALT SURFACE REMOVAL, 2" SPECIAL OR VARIABLE DEPTH
- EXISTING CONCRETE SIDEWALK OR DRIVEWAY TO REMAIN IN PLACE
- SIDEWALK OR PCC DRIVEWAY REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT
- ADJ-3V — STRUCTURE TO BE ADJUSTED; NEW F&G/LID TYPE (WHERE SHOWN)
- R — STRUCTURE TO BE REMOVED

NOTES:
1. SEE "PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN" FOR SIDEWALK AND DRIVEWAY INFORMATION.



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|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

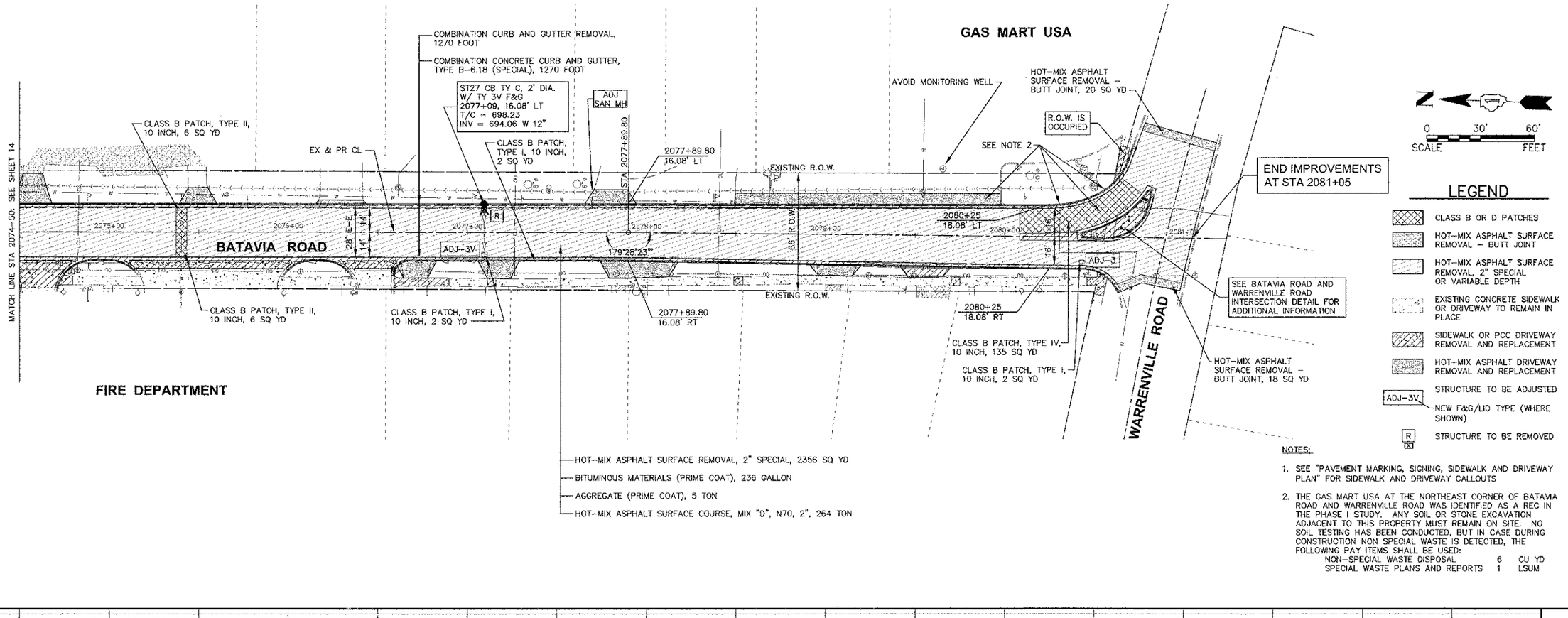
**BATAVIA ROAD PLAN AND PROFILE**

SCALE: 1"=30' | SHEET NO. 4 OF 5 SHEETS | STA. 2066+50 TO STA. 2074+50

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 14 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:08 PM By: Jim Schmidt - Tab: 14 P&P - 22x34

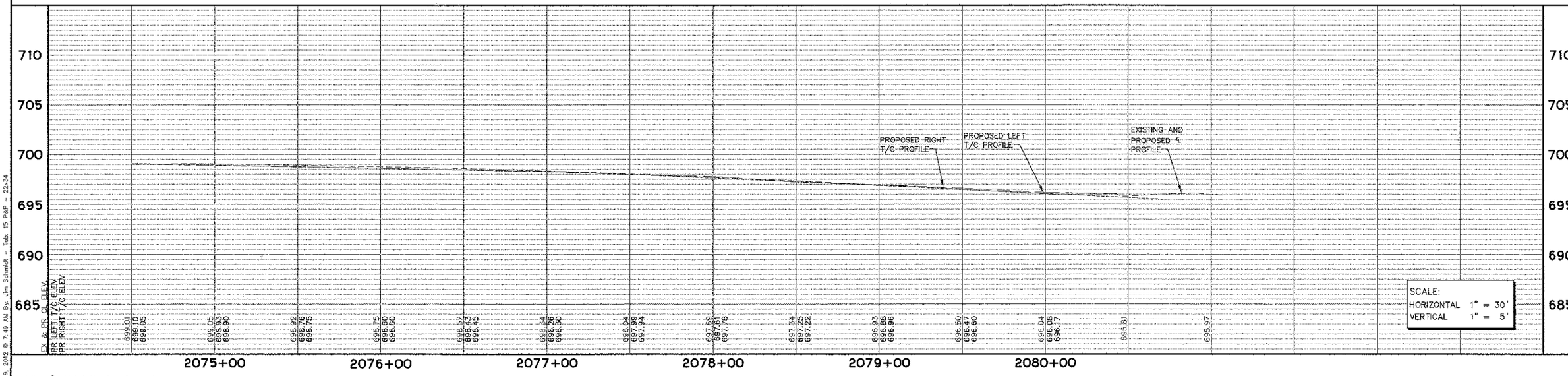
Path: I:\SKKPROJ\WM603\DWG\DWG FINAL ENG\WM603-PROF




NOTES:

1. SEE "PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN" FOR SIDEWALK AND DRIVEWAY CALLOUTS
2. THE GAS MART USA AT THE NORTHEAST CORNER OF BATAVIA ROAD AND WARRENVILLE ROAD WAS IDENTIFIED AS A REC IN THE PHASE I STUDY. ANY SOIL OR STONE EXCAVATION ADJACENT TO THIS PROPERTY MUST REMAIN ON SITE. NO SOIL TESTING HAS BEEN CONDUCTED, BUT IN CASE DURING CONSTRUCTION NON SPECIAL WASTE IS DETECTED, THE FOLLOWING PAY ITEMS SHALL BE USED:

| | | |
|---|---|---|
| NON-SPECIAL WASTE DISPOSAL | 6 | CU YD |
| SPECIAL WASTE PLANS AND REPORTS | 1 | LSUM |



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| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

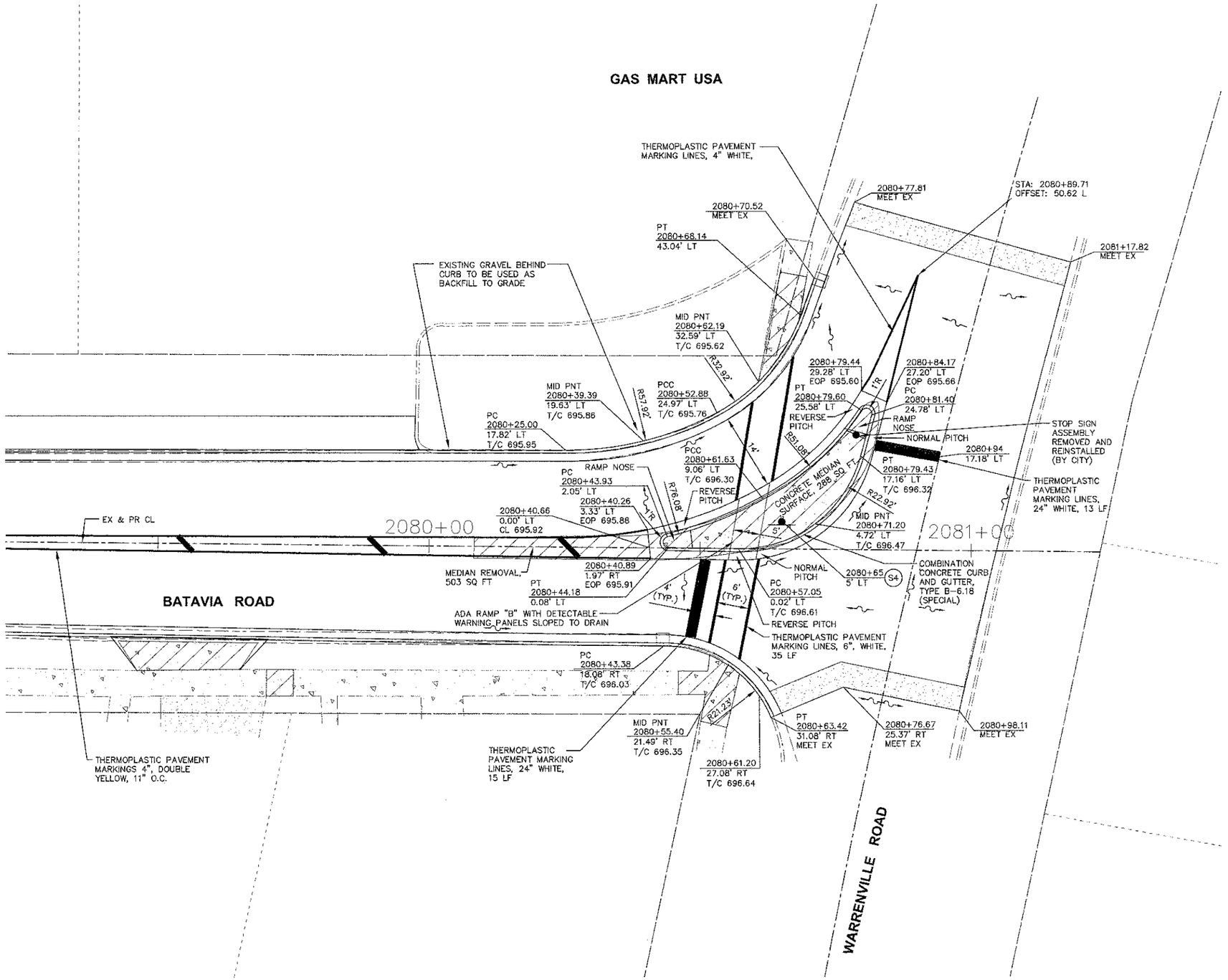
**BATAVIA ROAD PLAN AND PROFILE**

SCALE: 1"=30' | SHEET NO. 5 OF 5 SHEETS | STA. 2074+50 TO STA. 2080+80

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 15 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 19, 2012 @ 7:49 AM By: Jim Schmidt - Tab: 15 P&P - 22x34

Path: H:\SDSKPROJ\WW1003\WWC\WWC FINAL ENG\WW1003-PROF



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| | | |
|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - |
| | DRAWN - KKP/CLN/JPS | REVISED - |
| PLOT SCALE = | CHECKED - JRL | REVISED - |
| PLOT DATE = | DATE - 8/24/12 | REVISED - |

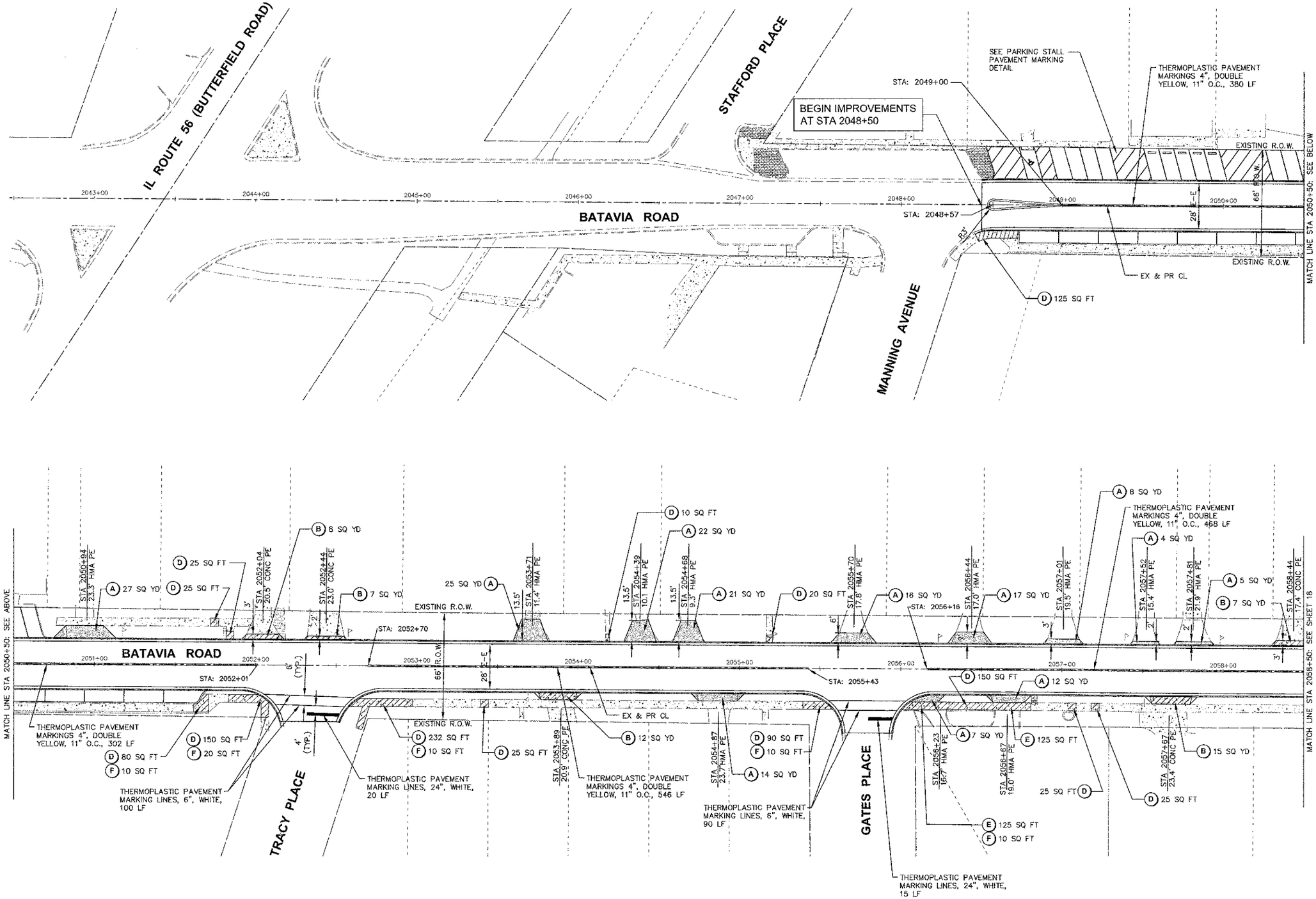
**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**BATAVIA ROAD AND WARRENVILLE ROAD INTERSECTION DETAIL**

SCALE: N/A | SHEET NO. 1 OF 1 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 16 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:12 PM By: Jim Schmidt - Tab: 16 Intersection - 22x34
Path: H:\SDSKPROJ\WV1003\DWG\DWG FINAL ENG\WV1003-PLAN



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| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

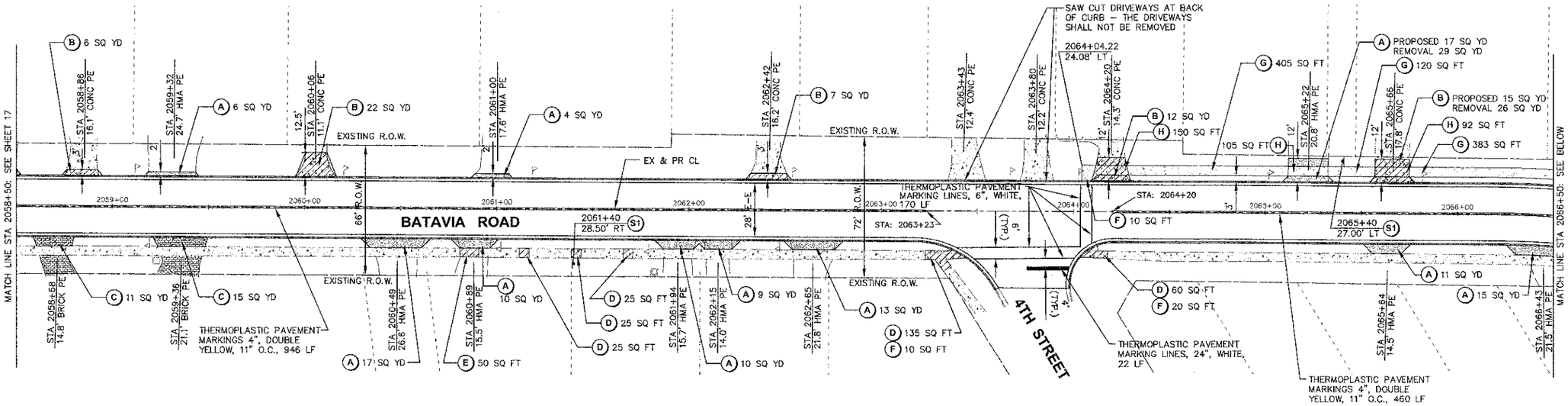
**PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN**

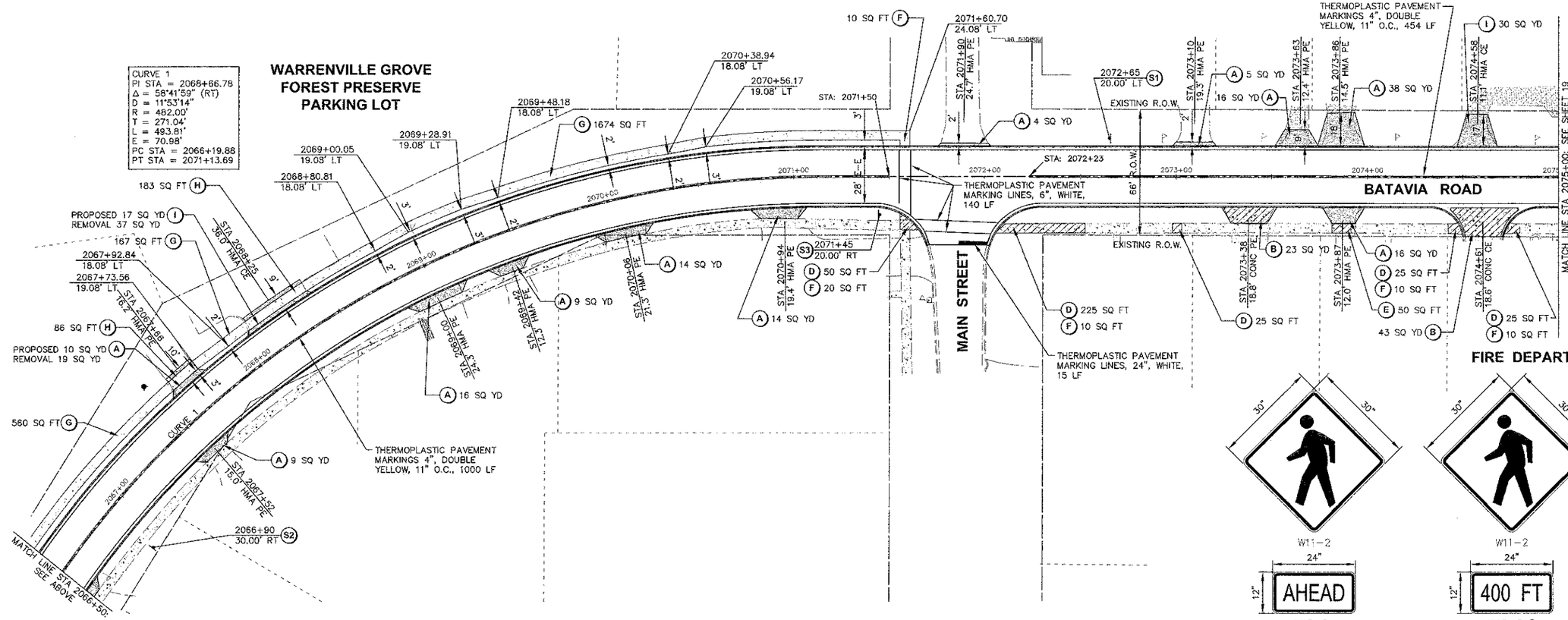
SCALE: N/A | SHEET NO. 1 OF 3 SHEETS | STA. 2043+00 TO STA. 2058+50

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 17 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:19 PM By: Jim Schmidt - Tab: 17 Drive - 22x34

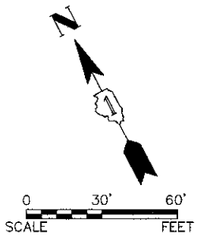
Path: H:\SDSRPROJ\WW1003\DWG\DWG FINAL ENG\WW1003-DRIVE










## LEGEND

- EXISTING CONCRETE SIDEWALK OR DRIVEWAY TO REMAIN IN PLACE
- SIDEWALK OR PCC DRIVEWAY REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT
- BRICK
- (A) HOT-MIX ASPHALT DRIVEWAY AND PAVEMENT REMOVAL STABILIZED DRIVEWAYS, 10"
- (B) DRIVEWAY PAVEMENT REMOVAL AND PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7"
- (C) BRICK PAVER REMOVAL AND REPLACEMENT
- (D) SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (E) SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK, 7"
- (F) DETECTABLE WARNINGS
- (G) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (H) PORTLAND CEMENT CONCRETE SIDEWALK, 7"
- (I) HOT-MIX ASPHALT DRIVEWAY AND PAVEMENT REMOVAL STABILIZED DRIVEWAY PAVEMENT

NOTES:
1. THE CITY OF WARRENVILLE SHALL REMOVE, REPLACE, RELOCATE AND FURNISH ALL SIGNS REQUIRED.
2. RECESSED REFLECTIVE PAVEMENT MARKERS PLACEMENT AND COLOR SHALL CONFORM TO IDOT DISTRICT 1 HIGHWAY STANDARD TC-11






Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

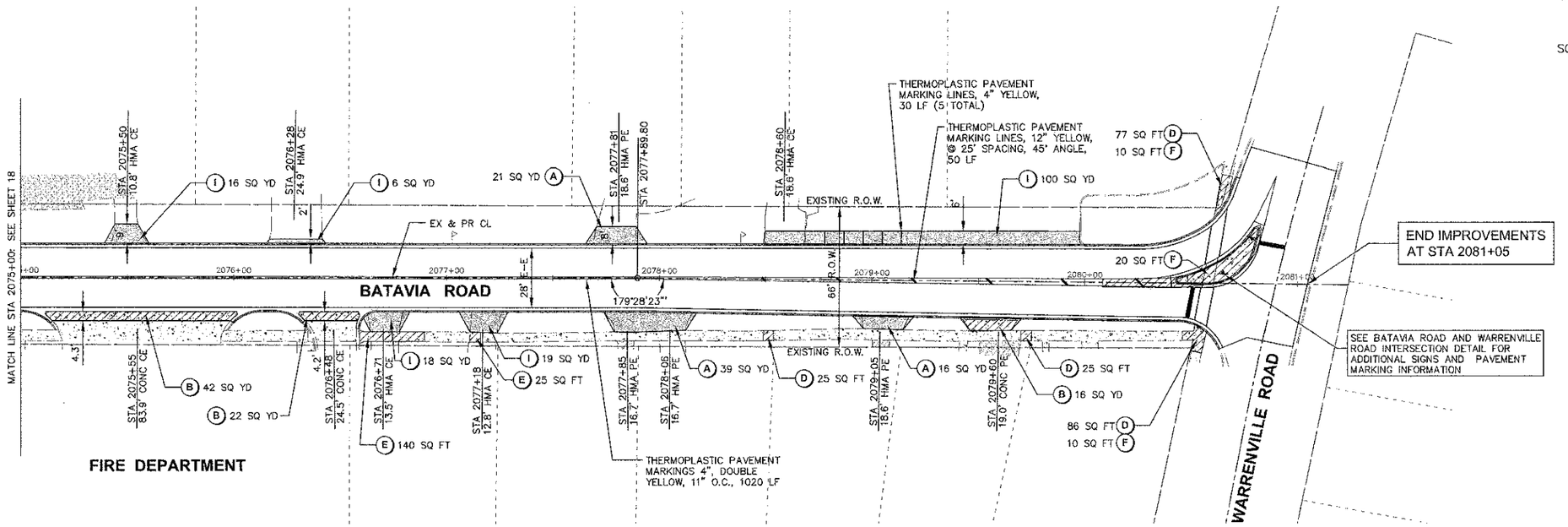
**PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN**

SCALE: N/A | SHEET NO. 2 OF 3 SHEETS | STA. 2058+50 TO STA. 2075+00

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 18 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:23 PM By: Jim Schmidt - Tab: 18 Drive - 17x34

Path: K:\SDSPROJ\WV1603\DWG\DWG FINAL ENG\WV1603-DRIVE




## LEGEND

- EXISTING CONCRETE SIDEWALK OR DRIVEWAY TO REMAIN IN PLACE
- SIDEWALK OR PCC DRIVEWAY REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT
- BRICK
- (A) HOT-MIX ASPHALT DRIVEWAY AND PAVEMENT REMOVAL STABILIZED DRIVEWAYS, 10"
- (B) DRIVEWAY PAVEMENT REMOVAL AND PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7"
- (C) BRICK PAVER REMOVAL AND REPLACEMENT
- (D) SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (E) SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK, 7"
- (F) DETECTABLE WARNINGS
- (G) PORTLAND CEMENT CONCRETE SIDEWALK, 5"
- (H) PORTLAND CEMENT CONCRETE SIDEWALK, 7"
- (I) HOT-MIX ASPHALT DRIVEWAY AND PAVEMENT REMOVAL STABILIZED DRIVEWAY PAVEMENT

NOTES:
1. THE CITY OF WARRENVILLE SHALL REMOVE, REPLACE, RELOCATE AND FURNISH ALL SIGNS REQUIRED.
2. RECESSED REFLECTIVE PAVEMENT MARKERS PLACEMENT AND COLOR SHALL CONFORM TO IDOT DISTRICT 1 HIGHWAY STANDARD TC-11



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| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

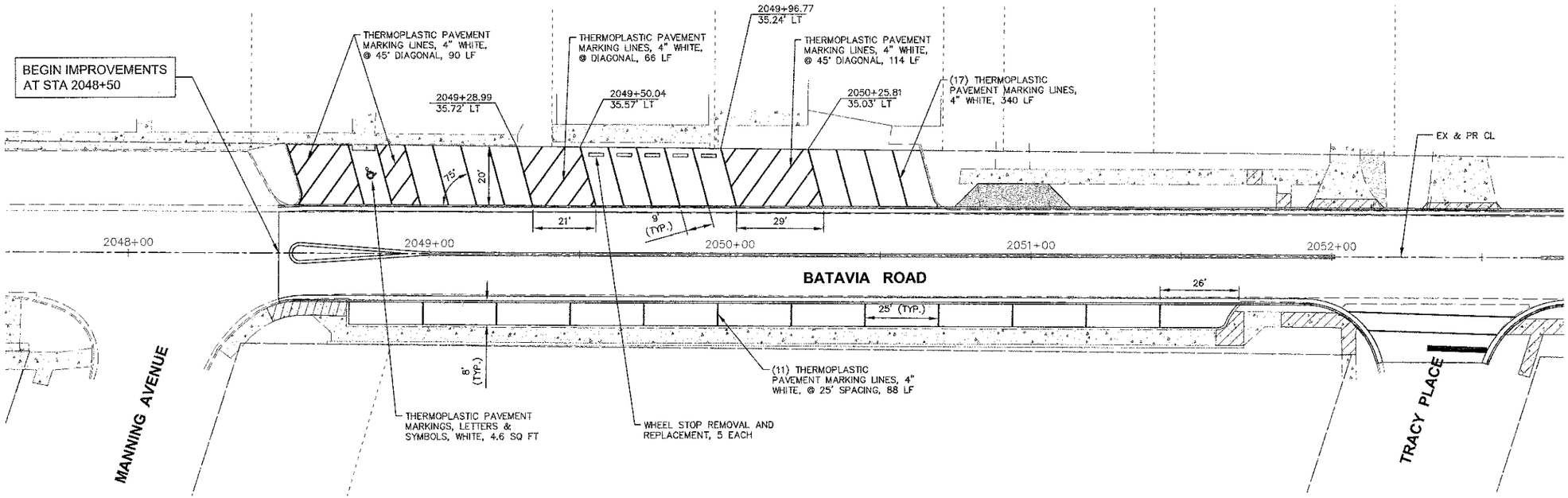
**PAVEMENT MARKING, SIGNING, SIDEWALK AND DRIVEWAY PLAN**

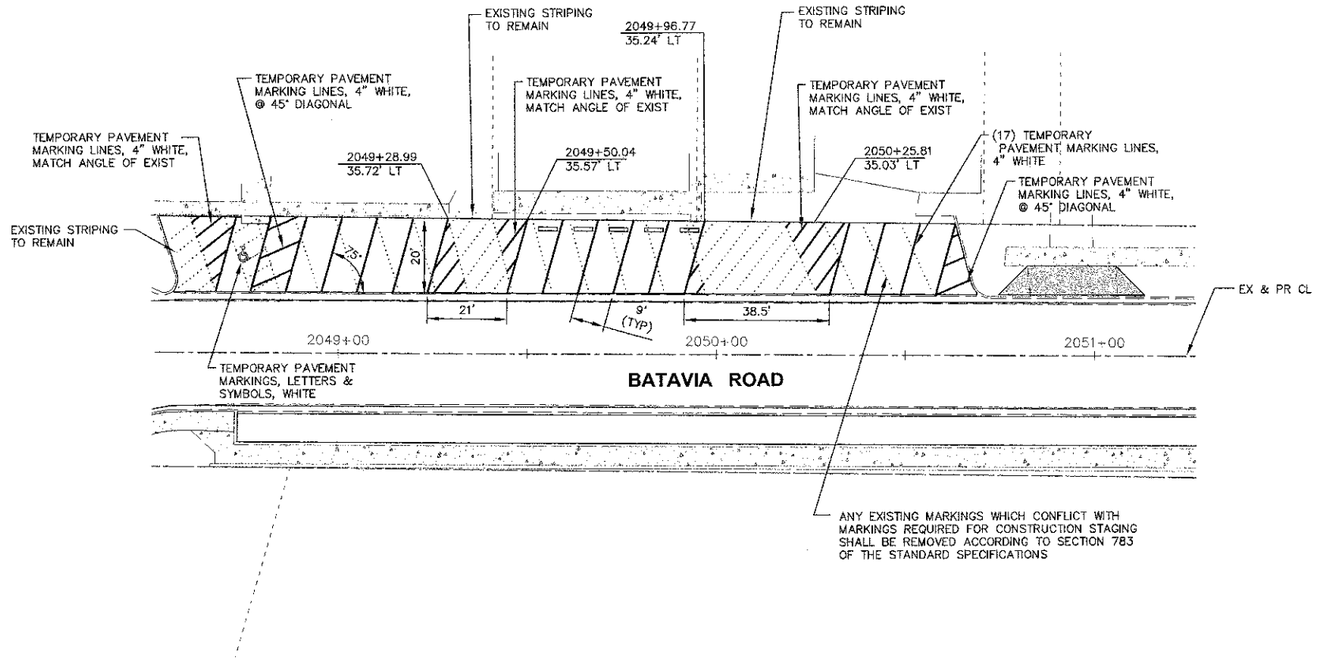
SCALE: N/A | SHEET NO. 3 OF 3 SHEETS | STA. 2075+00 TO STA. 2081+00

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 19 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 16, 2012 @ 3:24 PM By: Jim Schmidt - Tab: 19 Drive - 22x34

Path: N:\SSSPROJ\WW1003\DWG\DWG FINAL ENG\WW1003-DRIVE




**TEMPORARY PARKING STALL PAVEMENT MARKING**



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| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**PARKING STALL PAVEMENT MARKING DETAIL**

SCALE: 1"=20' | SHEET NO. 1 OF 1 SHEETS | STA. 2048+00 TO STA. 2052+50

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 20 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:58 PM By: Jim Schmidt - Tab: 20 Parking - 22x34

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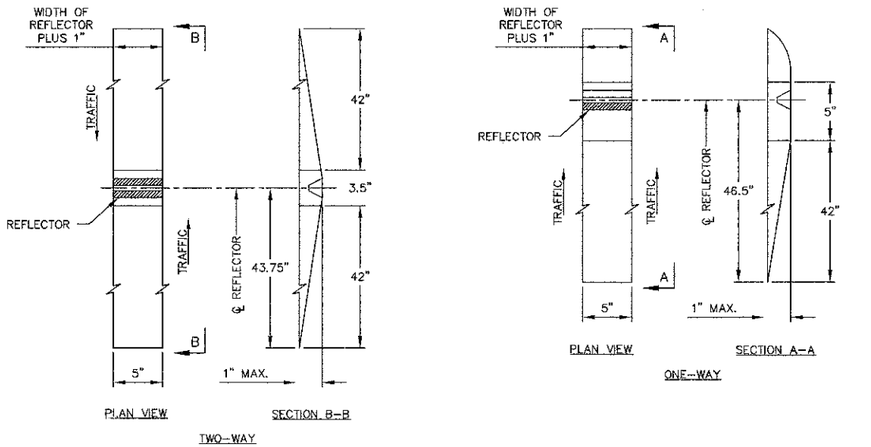



## RECESSED REFLECTIVE PAVEMENT MARKERS

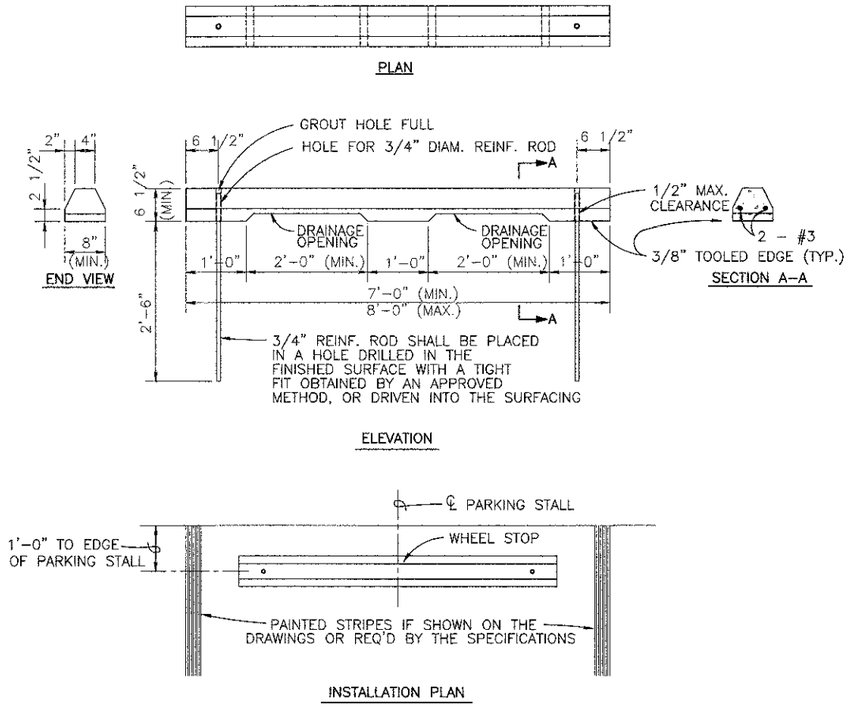
N.T.S.

INSTALLATION NOTES:

1. SAW CUT TO DIMENSIONS SHOWN.
2. SAW CUT AREAS TO BE DRY AND FREE OF MATERIAL THAT ADVERSELY AFFECTS THE ADHESIVE BOND.
3. INSTALL THE REFLECTOR WITH AN APPROVED TWO–COMPONENT EPOXY ADHESIVE. EPOXY SHALL NOT OBSCURE OR BLOCK THE LENS.
4. INSTALL TOP OF REFLECTOR 1/2 TO 1/4 INCH BELOW THE PAVEMENT SURFACE.
5. REFLECTOR SHALL BE 3M SERIES 180 OR APPROVED EQUAL.

GENERAL NOTE:

1. MARKER PLACEMENT AND COLOR SHALL CONFORM TO IDOT DISTRICT 1 HIGHWAY STANDARD TC–11.




## PRECAST CONCRETE WHEEL STOP DETAILS

N.T.S.

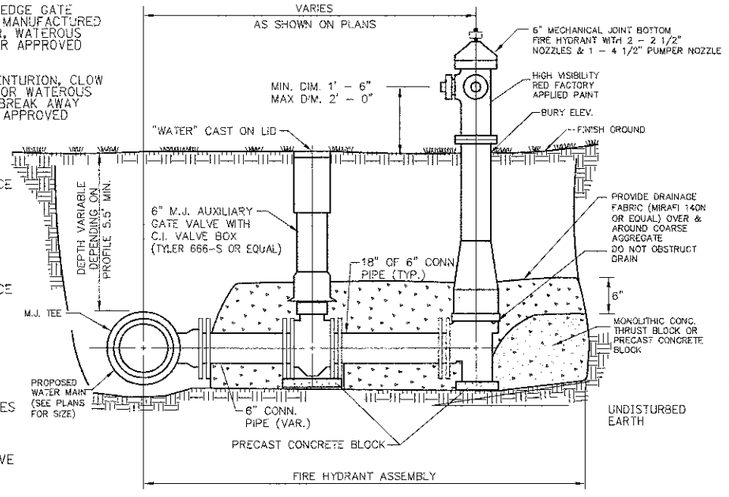
CITY OF WARRENVILLE

GATE VALVE - RESILENT WEDGE GATE VALVES AS MANUFACTURED BY MUELLER, WATEROUS OR CLOW (OR APPROVED EQUAL)

FIRE HYDRANT - MUELLER CENTURION, CLOW MEDALLION OR WATEROUS WB67-250 (BREAK AWAY STYLE) (OR APPROVED EQUAL)

NOTES:

1. THE CONTRACT UNIT PRICE OF EACH FOR FIRE HYDRANT COMPLETE SHALL INCLUDE THE TEE AT THE MAIN, THE 6" HYDRANT LEAD, CONNECTION PIPE, AUXILIARY VALVE, AND FIRE HYDRANT
2. THE CONTRACT UNIT PRICE OF EACH FOR FIRE HYDRANTS SHALL INCLUDE THE FIRE HYDRANT ONLY. AN AUXILIARY VALVE AND 6" LEAD ARE NOT REQUIRED. AN 8"X6" REDUCE WILL BE REQUIRED AS SHOWN ON THE PLANS.
3. ALL BOLTS, NUTS, FLANGES AND SPRINGS SHALL BE STAINLESS STEEL.
4. ALL HYDRANTS SHALL HAVE A BRONZE CROSS ARM / TOP PLATE.
5. VALVES AND HYDRANTS SHALL BE THE SAME MANUFACTURER, AND ONLY ONE MANUFACTURER SHALL BE USED THROUGHOUT THE PROJECT LIMITS.




## FIRE HYDRANT ASSEMBLY INSTALLATION



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| | | | |
|---|---|---|---|
| USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | DRAWN - KKP/CLN/JPS | REVISED - | |
| PLOT SCALE = | CHECKED - JRL | REVISED - | |
| PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**SPECIAL DETAILS**

SCALE: N/A | SHEET NO. 1 OF 2 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 21 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:13 AM By: Jim Schmidt - Tab: 21 Details 22x34

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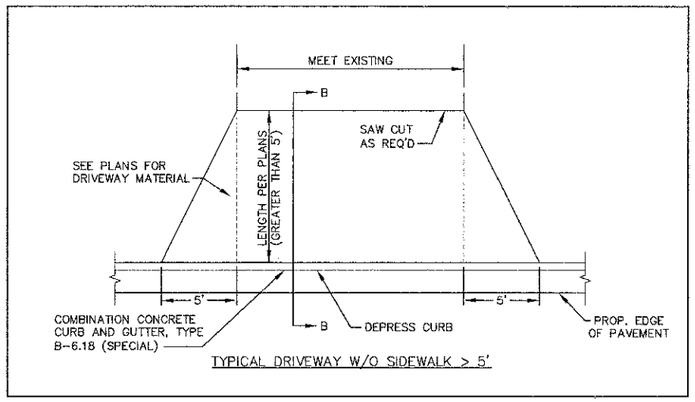
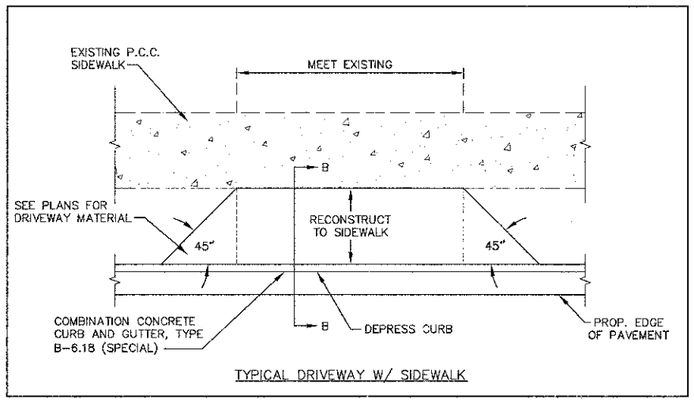
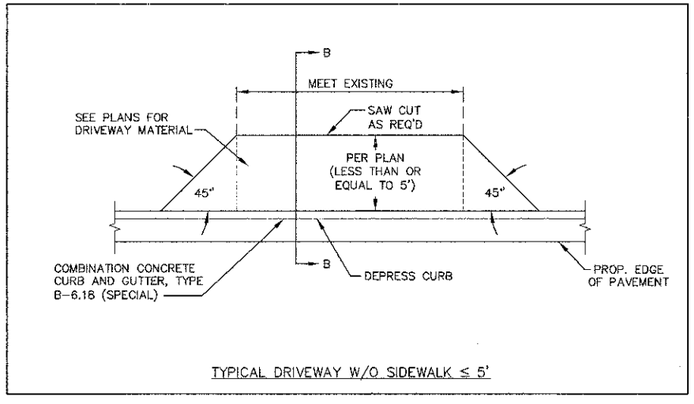


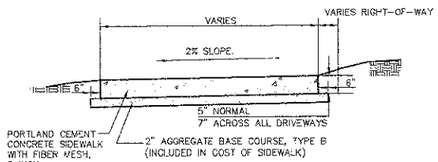

TYPICAL DRIVEWAY W/O SIDEWALK > 5'




TYPICAL DRIVEWAY W/ SIDEWALK

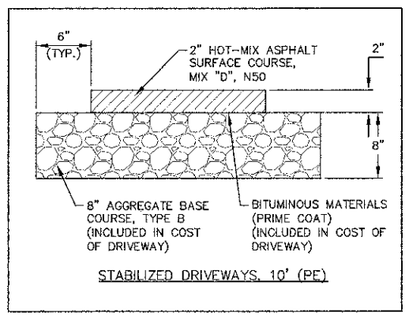



TYPICAL DRIVEWAY W/O SIDEWALK ≤ 5'

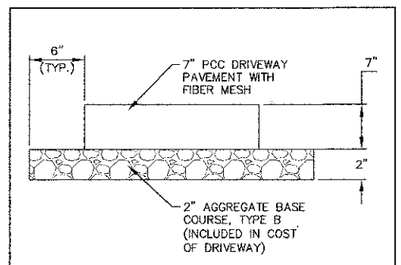



| TYPICAL EXPANSION JOINT SPACING CURB AND SIDEWALK | |
|---|---|
| POUR TEMP. (DEG. F°) | EXPANSION JOINT (SPACING FT.) |
| 40 MIN. | 70' |
| 45 | 75' |
| 50 | 80' |
| 55 | 90' |
| 60 | 95' |
| 65 | 105' |
| 70 | 115' |
| 75 | 125' |
| 80 | 145' |
| 85 | 160' |
| 90 MAX. | 180' |

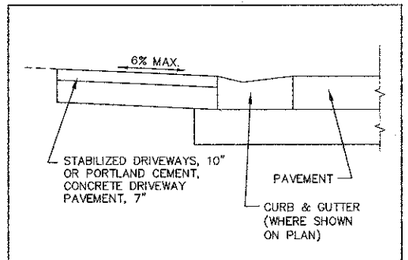
| TYPICAL SIDEWALK SECTION | | | | |
|---|---|---|---|---|
| Engineering Enterprises, Inc. 52 Wheeler Road Sugar Grove, Illinois 60554 Phone: (630) 466-9350 | SCALE: N.T.S. | FOLDER: EE-PV | DRAWING NUMBER: 02# | DRAWN BY: CLN |
| | DATE: 5/18/07 | | | REVISION: |



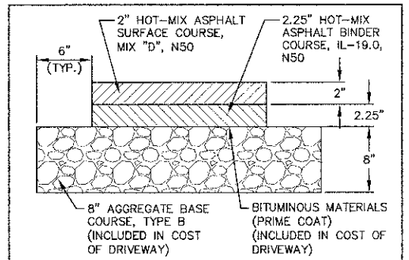

STABILIZED DRIVEWAYS, 10' (PE)



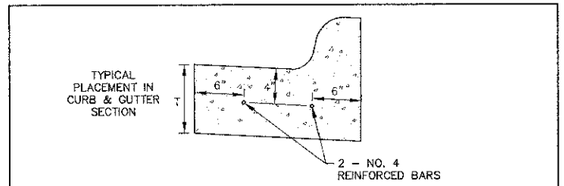

PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7"

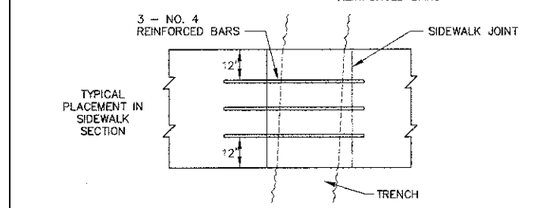


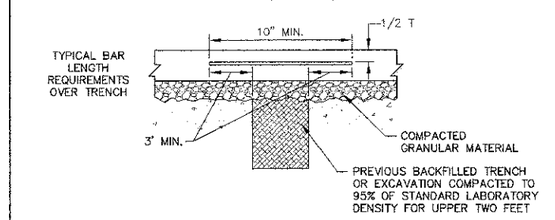

SECTION B – B



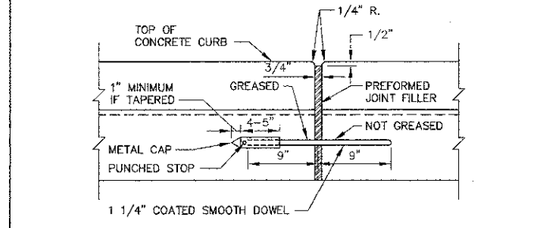

STABILIZED DRIVEWAY PAVEMENT (CE)



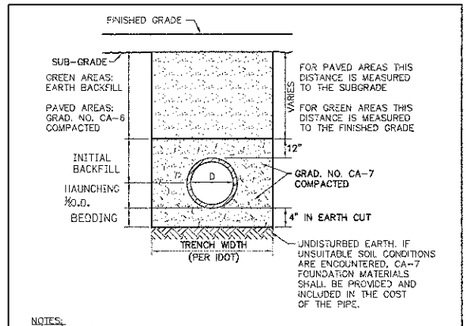




**REINFORCING CURB & GUTTER OR SIDEWALK INSTALLED OVER TRENCH**




**CURB EXPANSION JOINT DETAIL**




NOTES:
1. ANY FLARE OR EXCAVATION BEYOND THE LIMITS SPECIFIED SHALL BE BACKFILLED ACCORDINGLY AT THE CONTRACTOR'S EXPENSE.
2. PAVEMENT LIMITS SHALL BE DEFINED AS 2' BEYOND CURB AND GUTTERS, PAVING, BIKE PATHS, SIDEWALKS, AGGREGATE SHOULDERS AND ANY OTHER PAVED AREAS.
3. ALL TRENCH BACKFILL, INCLUDING FINAL BACKFILL, SHALL BE INCLUDED IN THE COST OF THE PIPE.
4. PIPE SHALL BE BACKFILLED IN ACCORDANCE WITH ARTICLE 550.07, METHOD 1 ONLY, OR AS DIRECTED BY THE ENGINEER, EXCEPT AS NOTED IN THIS DETAIL.
5. TRENCH BACKFILL SHALL BE PLACED IN 6" LIFTS AND SHALL BE COMPACTED IN PLACE TO 95% OF MAXIMUM DENSITY AT OPTIMUM MOISTURE AS DETERMINED BY THE MODIFIED PROCTOR TEST.



| STORM SEWER (PVC) TRENCH SECTION | | | | |
|---|---|---|---|---|
| Engineering Enterprises, Inc. 52 Wheeler Road Sugar Grove, Illinois 60554 Phone: (630) 466-9350 | SCALE: N.T.S. | FOLDER: EE-SA | DRAWING NUMBER: TR-08 | DRAWN BY: JPS |
| | DATE: 8/23/12 | | | REVISION: 8/21/12 |



| | | | | |
|---|---|---|---|---|
| Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.enweb.com | USER NAME = | DESIGNED - SWM/TVW | REVISED - | |
| | | DRAWN - KKP/CLN/JPS | REVISED - | |
| | PLOT SCALE = | CHECKED - JRL | REVISED - | |
| | PLOT DATE = | DATE - 8/24/12 | REVISED - | |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

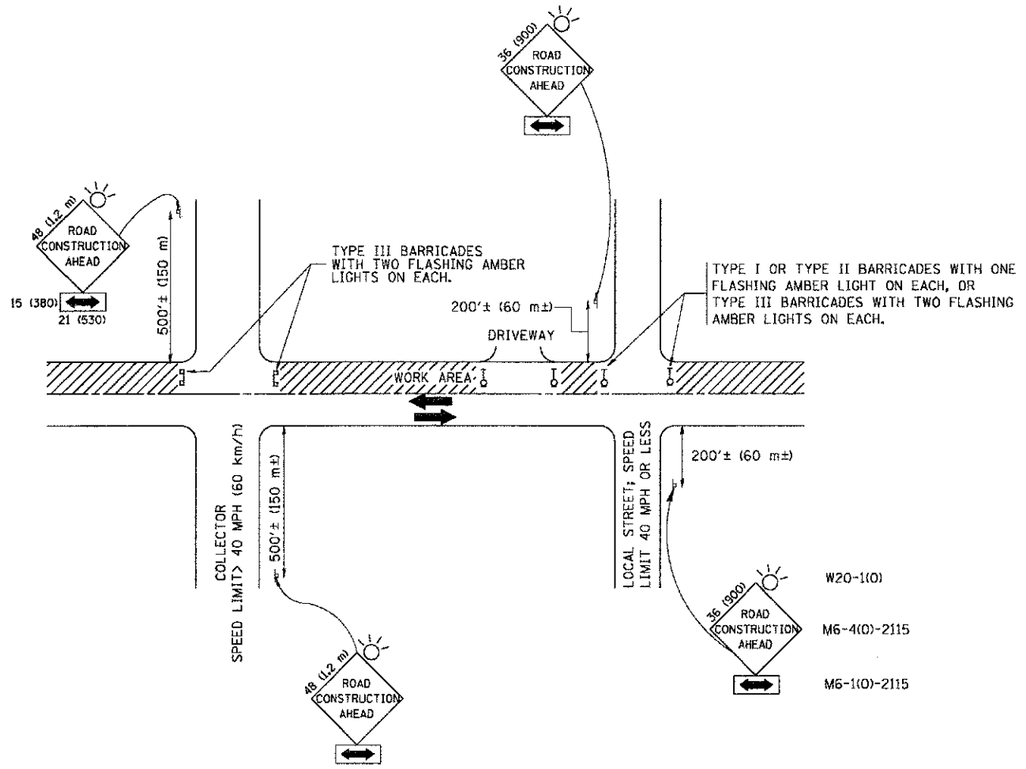
**SPECIAL DETAILS**

SCALE: N/A | SHEET NO. 2 OF 2 SHEETS | STA. N/A | TO STA. N/A

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 22 |
| | | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 18, 2012 @ 4:31 PM By: Jim Schmidt - fax: 22 Details 22x34

Path: H:\SDSKPROJ\WY003\DWG\WY003.DWG FINAL ENG\WY003-CVR




# TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

## NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.



| | | | |
|---|---|---|---|
| FILE NAME = W:\diststd\22x34\tc10.dgn | USER NAME = gaglionobt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 50.000 ' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 1/4/2008 | DATE - 06-89 | REVISED -T. RAMMACHER 01-06-00 |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

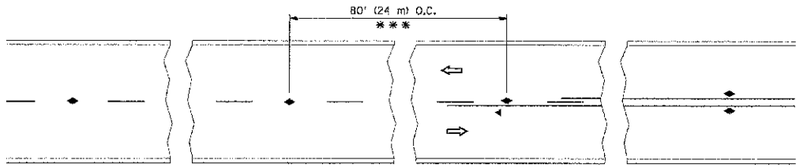
**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3353 | 10-00031-00-RS | DUPAGE | 29 | 23 |
| | **TC-10** | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

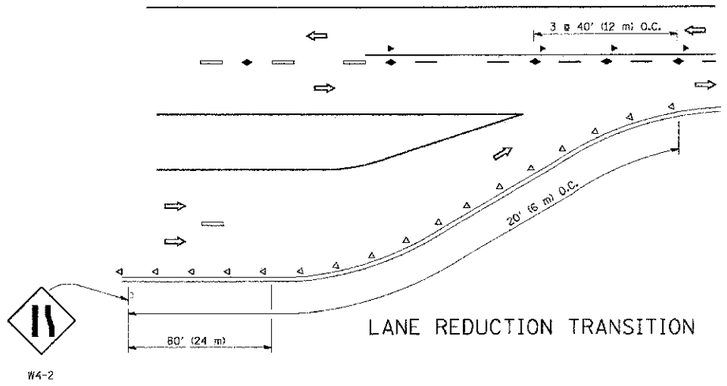
Plotted: October 11, 2012 @ 11:18 AM By: Jim Schmidt - Tab: 23 TC-10 22x34

Path: H:\SIGSHOP\WW1003\DWG\DWG FINAL ENG\WW1003-CVR

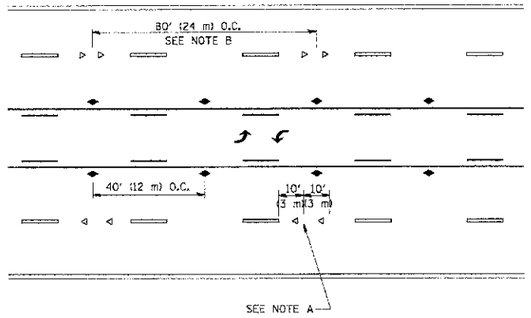



*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

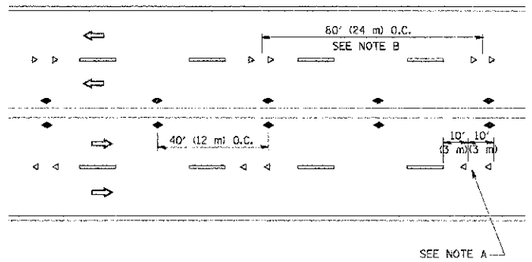
TWO-LANE/TWO-WAY



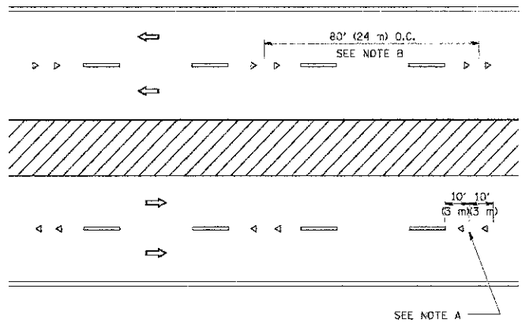

LANE REDUCTION TRANSITION




TWO-WAY LEFT TURN




MULTI-LANE/UNDIVIDED




MULTI-LANE/DIVIDED

## GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

## SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

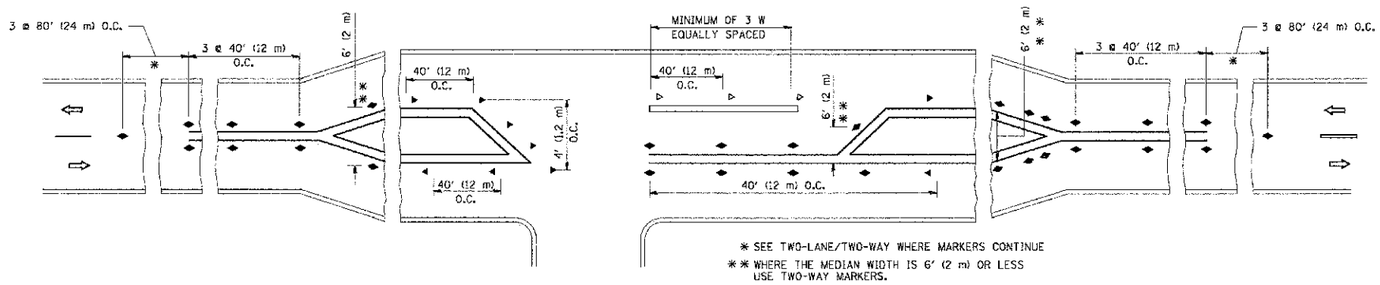
## LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

## DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.




* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.



| | | | |
|---|---|---|---|
| FILE NAME = c:\pw_work\pwidot\drivakosgn\d0128315\tc11.dgn | USER NAME = drivakosgn | DESIGNED - | REVISED - T. RAMMACHER 09-19-94 |
| | | DRAWN - | REVISED - T. RAMMACHER 03-12-99 |
| | PLOT SCALE = 50.000 ' / IN. | CHECKED - | REVISED - T. RAMMACHER 01-06-00 |
| | PLOT DATE = 9/9/2009 | DATE - | REVISED - C. JUCIUS 09-09-09 |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

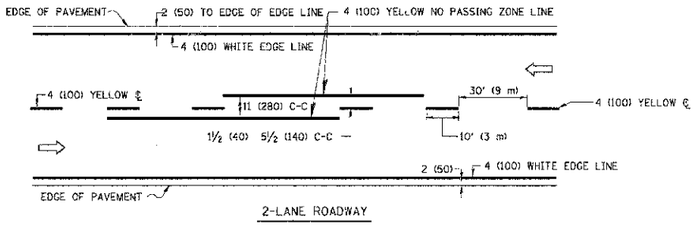
**TYPICAL APPLICATIONS**
**RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

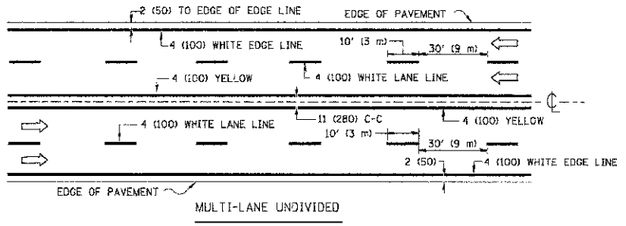
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 24 |
| | TC-11 | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:20 AM By: Jim Schmidt - Tool 24 10-11 22x34

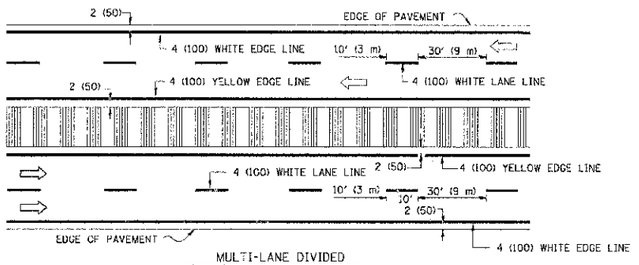
Path: H:\BUSPROJ\WV1002\DWG\DWG FINAL ENG\WV1003-CVR




2-LANE ROADWAY



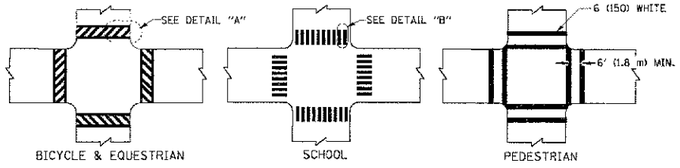

MULTI-LANE UNDIVIDED



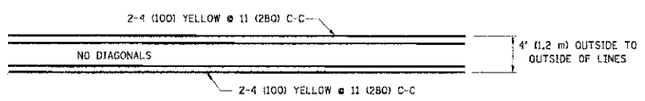

MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

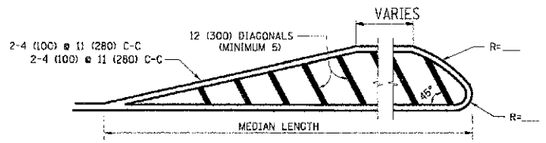
## TYPICAL LANE AND EDGE LINE MARKING




## TYPICAL CROSSWALK MARKING



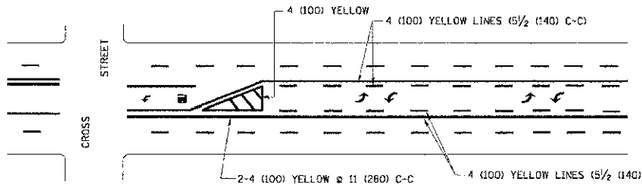

4' (1.2 m) WIDE MEDIANS ONLY



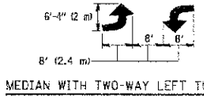

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

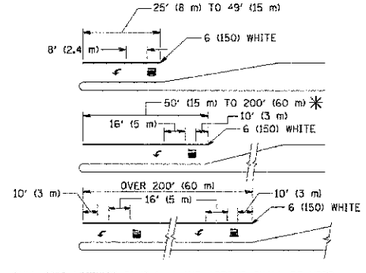



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.




MEDIAN WITH TWO-WAY LEFT TURN LANE

## TYPICAL PAINTED MEDIAN MARKING

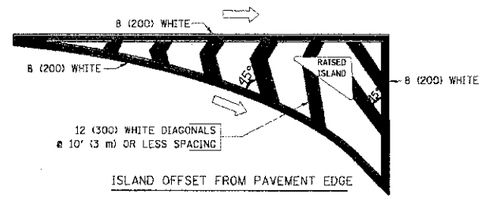



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

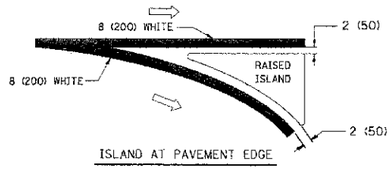
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

## TYPICAL TURN LANE MARKING




ISLAND OFFSET FROM PAVEMENT EDGE




ISLAND AT PAVEMENT EDGE

## TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|---|---|---|---|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES:<br>FOR ONE DIRECTION<br>FOR BOTH DIRECTIONS | 4 (100)<br>2 @ 4 (100) | SOLID<br>SOLID | YELLOW<br>YELLOW | 5½ (140) C-C FROM SKIP-DASH CENTERLINE<br>11 (280) C-C<br>OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100)<br>5 (125) ON FREEWAYS | SKIP-DASH<br>SKIP-DASH | WHITE<br>WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT<br>WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION<br><br>8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID<br><br>IN PAIRS | YELLOW<br><br>WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE<br><br>SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN)<br>A. DIAGONALS (BIKE & EQUESTRIAN)<br>B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150)<br>12 (300) @ 45°<br>12 (300) @ 90° | SOLID<br>SOLID<br>SOLID | WHITE<br>WHITE<br>WHITE | NOT LESS THAN 6' (1.8 m) APART<br>2' (600) APART<br>2' (600) APART<br>SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°<br><br>NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW:<br>TWO WAY TRAFFIC<br><br>WHITE:<br>ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE<br><br>SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS:<br>15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h))<br>20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))<br>30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001<br>AREA OF:<br>"R"=3.6 SQ. FT. (0.33 m²) EACH<br>"X"=54.0 SQ. FT. (5.0 m²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT<br><br>YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))<br>75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h))<br>150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



| | | | |
|---|---|---|---|
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| | PLOT SCALE = 50.002 ' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 9/9/2009 | DATE - 03-19-90 | REVISED - |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE**
**TYPICAL PAVEMENT MARKINGS**

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 25 |
| | TC-13 | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:21 AM By: Jim Schmidt - Tab: 25 TC-13 22x34

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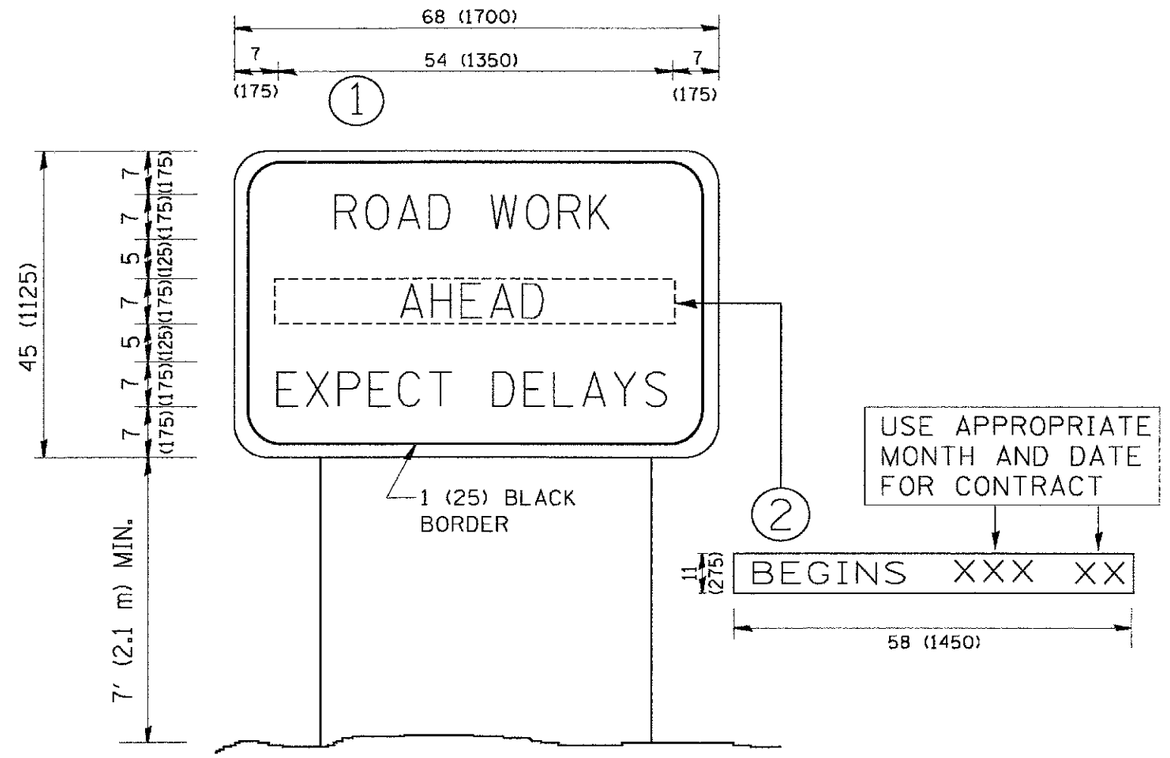


## NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



Plotted: October 11, 2012 @ 11:23 AM By: Jim Schmidt - Tab: 26 TC-22 22x34

Path: H:\SDSKPROJ\WV1003\DWG\DWG FINAL ENG\WV1003-CVR

| | | | |
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| | | DRAWN - | REVISED - R. MIRS 12-11-97 |
| | PLOT SCALE = 50.000 ' / IN. | CHECKED - | REVISED - T. RAMMACHER 02-02-99 |
| | PLOT DATE = 1/4/2008 | DATE - | REVISED - C. JUCIUS 01-31-07 |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD INFORMATION SIGN**

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 26 |
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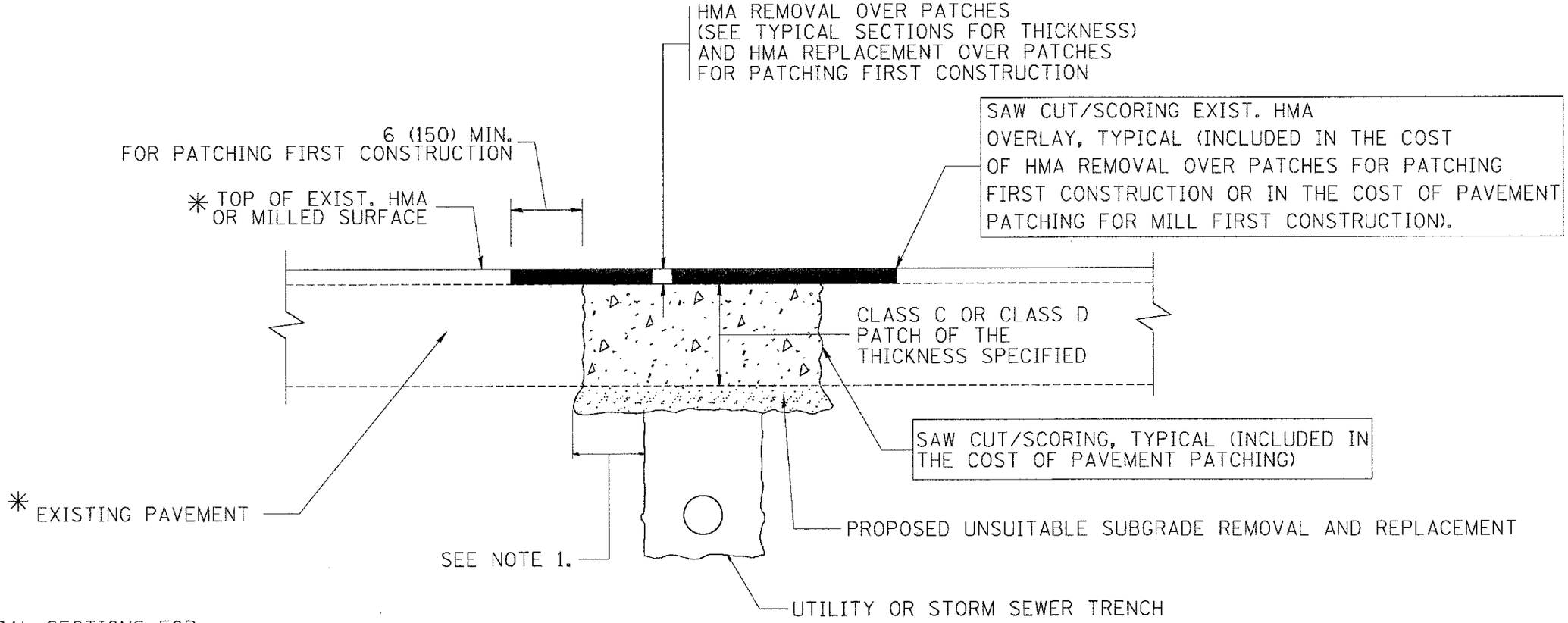
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |




* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



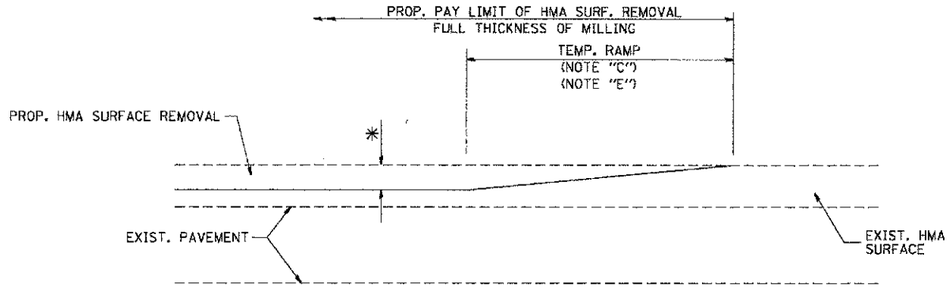
| | | | |
|---|---|---|---|
| FILE NAME = c:\projects\diststd22x34\bd22.dgn | USER NAME = bauerdl | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 |
| | | DRAWN - | REVISED - R. BORO 01-01-07 |
| | PLOT SCALE = 50.000 ' / IN. | CHECKED - | REVISED - R. BORO 09-04-07 |
| | PLOT DATE = 10/27/2008 | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

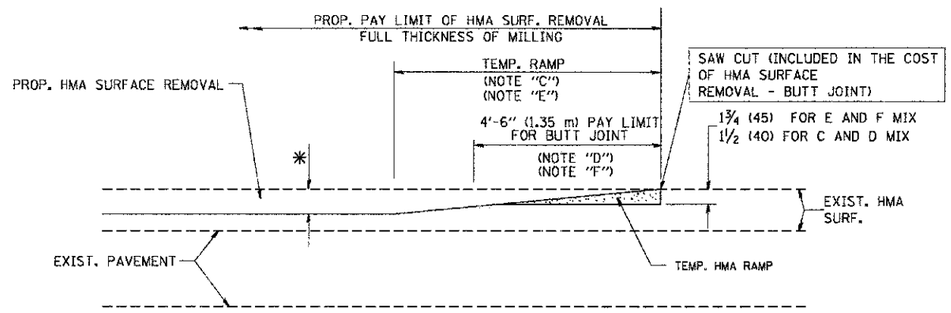
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 27 |
| | BD400-04 (BD-22) | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |




MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

## OPTION 1

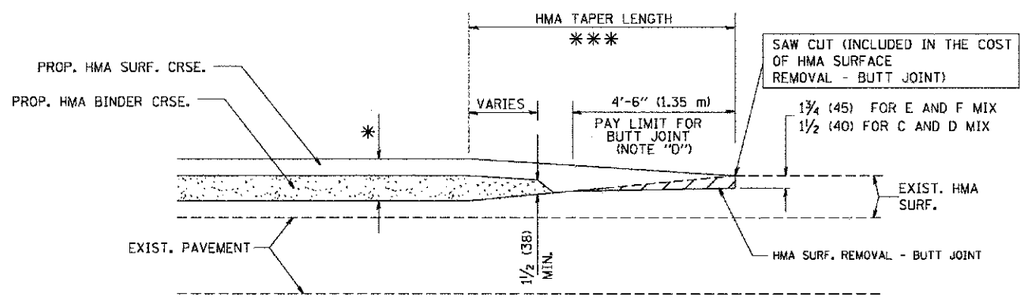



HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

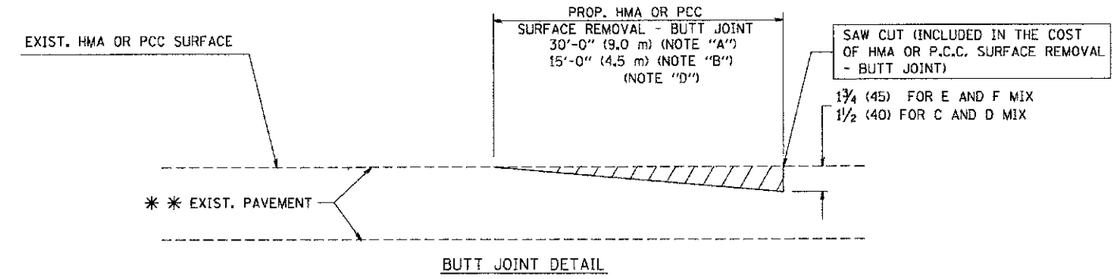
## OPTION 2

## TYPICAL TEMPORARY RAMP

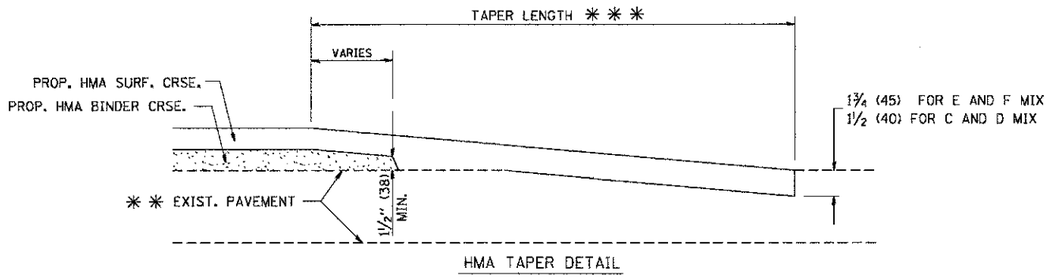



BUTT JOINT AND HMA TAPER

## TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING




BUTT JOINT DETAIL




HMA TAPER DETAIL

## TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



| | | | |
|---|---|---|---|
| FILE NAME = W:\dbstasd\22x34\bd32.dgn | USER NAME = gaglioncbt | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | PLOT SCALE = 50.2000 ' / IN. | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | PLOT DATE = 1/4/2008 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

**STATE OF ILLINOIS**
**DEPARTMENT OF TRANSPORTATION**

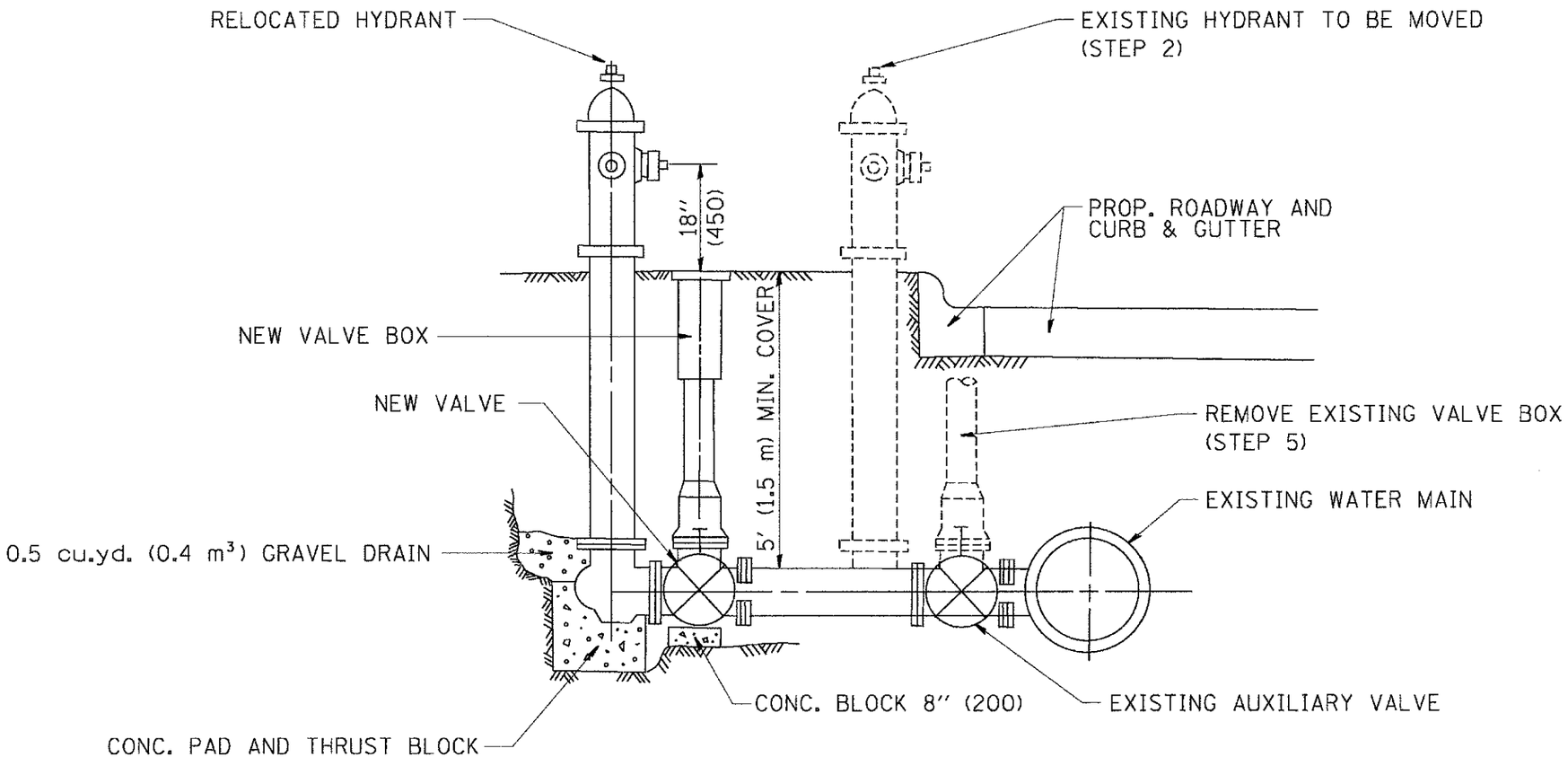
**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|---|---|---|---|
| 3553 | 10-00031-00-RS | DUPAGE | 29 | 28 |
| | BD400-05 BD32 | CONTRACT NO. 63758 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:25 AM By: Jim Schmidt - Tab: 28 BD-32 22X34

Path: H:\BSK\PROJ\WW1003\DWG\DWG FINAL ENG\WW1003-C4R




SEQUENCE OF CONSTRUCTION:

1. CLOSE EXISTING VALVE.
2. REMOVE EXISTING HYDRANT.
3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
4. RELOCATE EXISTING HYDRANT.
5. OPEN EXISTING VALVE, REMOVE BOX.
6. BACKFILL.
7. FLUSH AND TEST FOR CHLORIDE RESIDUAL AND PROVIDE TEST.

ALL WORK TO BE DONE IN ACCORDANCE WITH ARTICLE 564 OF THE STANDARD SPECIFICATIONS. NEW VALVE AND BOX SHALL BE SAME MAKE AND MODEL AS EXISTING.

# FIRE HYDRANT TO BE MOVED



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | |
|---|---|---|---|---|---|---|---|---|---|
| FILE NAME = W:\diststd\22x34\bd36.dgn | USER NAME = gaglenobt | DESIGNED - | REVISED - R. SHAH 09-09-94 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | FIRE HYDRANT TO BE MOVED | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - R. SHAH 10-25-94 | | | | | 3553 | 10-00031-00-RS | DUPAGE | 29 | 29 |
| | PLOT SCALE = 50.0000 ' / IN. | CHECKED - | REVISED - | | | | | | BD-36 | | CONTRACT NO. 63758 | |
| | PLOT DATE = 1/4/2008 | DATE - | REVISED - | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT - M-9003(866) | | |

Plotted: October 11, 2012 @ 11:27 AM By: Jim Schmidt - Tab: 29 BD-36 22X34

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