

017

| F.A.P.                | SECTION                    | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|-----------------------|----------------------------|----------|--------------------|-----------|
| VAR                   | (137, 129)RS-4, (137X)RS-4 | FULTON   | 73                 | 1         |
| RTE. US 24 & IL 78/97 |                            | ILLINOIS | CONTRACT NO. 68A91 |           |

01-18-2019 LETTING ITEM 017

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

D-94-037-12



**INDEX OF SHEETS**

- 1. COVER SHEET
- 2.-3. GENERAL NOTES
- 4.-8. SUMMARY OF QUANTITIES
- 9.-20. TYPICAL SECTIONS
- 21.-29. SCHEDULE OF QUANTITIES
- 30. LINE DIAGRAM
- 31.-38. ADA IMPROVEMENTS
- 39.-55. PLAN SHEETS
- 56.-57. PAVEMENT MARKING DETAIL
- 58. HMA TRANSITION
- 59. ADA APPROACH NOSE DETAIL
- 60.-73. CADD STANDARDS

**HIGHWAY STANDARD**

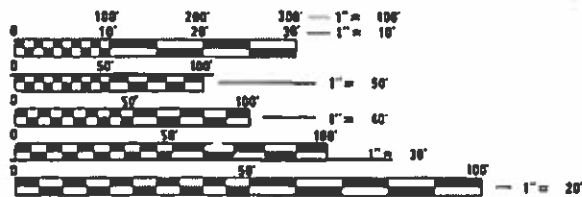
|           |           |
|-----------|-----------|
| 406201-01 | 701011-04 |
| 424001-11 | 701201-05 |
| 424006-04 | 701301-04 |
| 424011-04 | 701306-04 |
| 424016-05 | 701311-03 |
| 424021-05 | 701801-06 |
| 482011-03 | 701901-08 |
| 606001-07 | 725001-01 |
| 630001-12 | 780001-05 |
| 630301-09 | 781001-04 |
| 701006-05 | 782006    |
| 701001-02 | 701326-04 |

**DISTRICT STANDARDS**

|           |           |
|-----------|-----------|
| 406101-D4 | 606002-D4 |
| 440001-D4 | 630101-D4 |
| 606001-D4 | 780001-D4 |

**DESIGN DESIGNATION**

|                |           |
|----------------|-----------|
| US 24          | IL 78 /97 |
| ADT 4050/2500  | ADT 3000  |
| SU 3.46%/3.20% | SU 3.00%  |
| MU 4.44%/3.20% | MU 5.33%  |



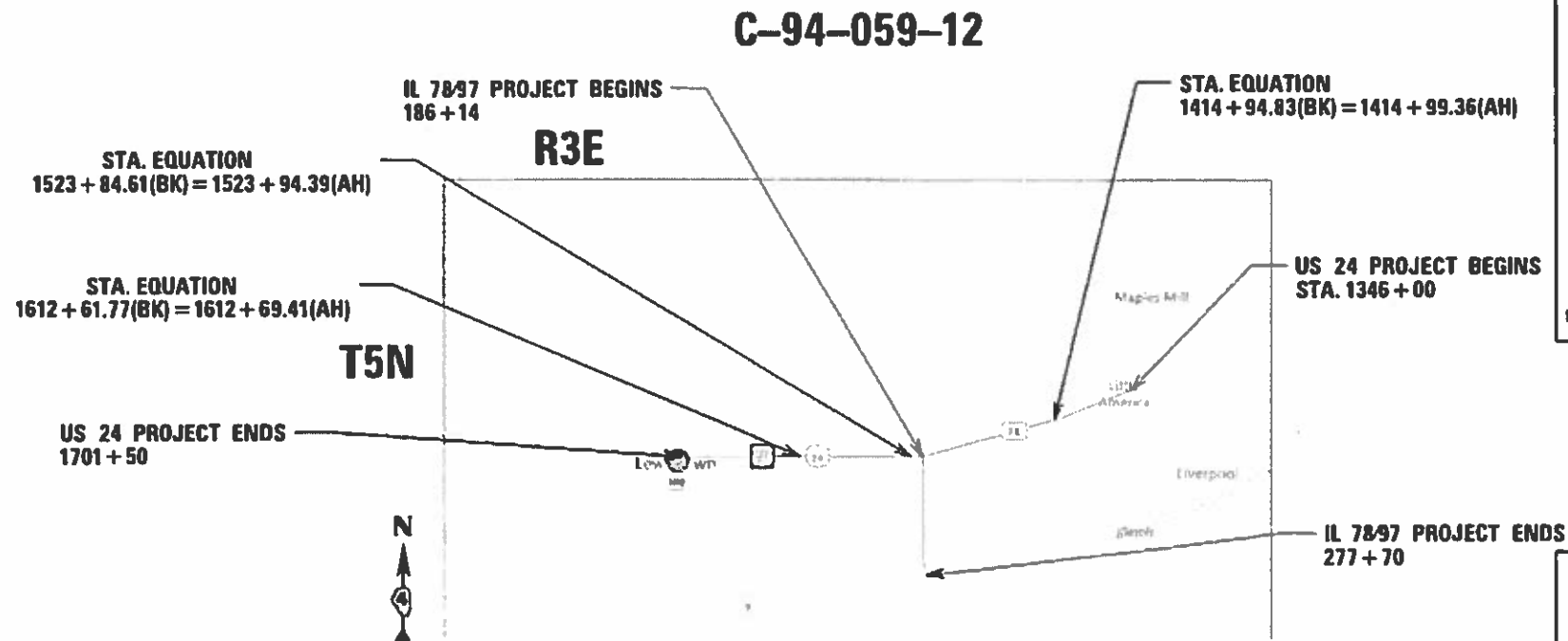
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: Christopher Maushard, (309) 671-3453  
PROJECT MANAGER: Mike Mohamed, (309) 671-3462

CONTRACT NO. 68A91  
CATALOG NO. 034774-00D

FAP ROUTE 317(US 24) & FAP ROUTE 30(IL 97, & IL 78)  
SECTION (137, 129)RS-4; (137X)RS-4  
PROJECT NHPP-STP-F3C8(070)  
TYPE of IMPROVEMENT: RESURFACING  
FULTON COUNTY



**BRIDGE OMISSIONS:**  
STA 1423+78.38 TO STA 1425+17.62  
STA 1446+15.75 TO STA 1447+69.25  
**RAILROAD OMISSION:**  
STA 1696+18 TO STA 1696+31

US 24 GROSS LENGTH = 35,550 FT. = 6.73 MILE  
US 24 NET LENGTH = 35,222 FT. = 6.67 MILE  
IL 78/97 GROSS LENGTH = 9156 FT. = 1.73 MILE  
IL 78/97 NET LENGTH = 9156 FT. = 1.73 MILE

**Job Description:**  
THIS PROJECT CONSISTS OF COLD-IN-PLACE RECYCLING, COLD MILLING, RESURFACING AND GUARDRAIL REPLACEMENT AND HANDICAP RAMP CONSTRUCTION ON US 24 FROM LITTLE AMERICA TO LEWISTOWN. THIS PROJECT CONSISTS OF COLD MILLING, RESURFACING ON IL 78/97 FROM US 24 TO SOUTH OF N. CLARK ROAD.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED *October 15, 2018*

*Paul G. Thomas*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*Dee J. [Signature]*  
ENGINEER OF DESIGN AND ENVIRONMENT

*Paul P. [Signature]*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

# GENERAL NOTES

## COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made.

## PROPERTY OWNER ACCESS REQUIREMENTS

Access must be maintained to all existing properties during construction per Article 107.09 unless arrangements are made in writing by the Contractor with the property owners with a copy to the Engineer for short-term closures.

## WINTER SHUTDOWN RESTRICTIONS ON COLD MILLED PROJECTS

Prior to winter shutdown the following steps shall be taken:

- \* All cold milled surfaces shall be overlaid. Leveling Binder and Surface lifts shall be completed.
- \* All lanes shall be reopened to traffic.
- \* Manholes, where applicable, shall be adjusted to the elevation of the binder course/leveling binder to ease in plowing snow, and re-adjusted to finished grade in the Spring. The initial manhole adjustment will be paid for at the contract unit price and any re-adjustment, as directed by the Engineer, will be paid for in accordance with Article 109.04.
- \* Temporary or permanent pavement marking shall be placed as applicable.

## ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

The required environmental resource documentation shall include the following:

- \*BDE Form 2289 (Cultural and Natural Resources Review of Borrow Areas)
- \*BDE Form 2290 (Waste/Use Area Review)
- \*A location map showing the size limits and location of the use area
- \*Color photographs depicting the use area
- \*Borrow Area Entry Agreement form-D4 PI0101

Prior to any waste materials being removed from the construction site the required environmental resource surveys shall be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standards Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

Please note that a minimum of four weeks shall be allowed for the District to obtain the required waste site environmental clearances and six weeks for the required borrow site environmental clearances.

## AGGREGATE FOR DRIVEWAY REPLACEMENT

The material used for construction of permanent aggregate driveways shall be gravel or crushed stone, as directed by the Engineer, to replace in kind the existing aggregate driveways.

No additional compensation shall be provided for this requirement but shall be considered as included in the cost of the pay item for the aggregate as specified on the plans.

## PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- \* 2, 3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- \* Multi-Lane Divided Roadways – outside edge of pavement in both directions
- \* Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX + X00)", where X represents the pavement station

This work will not be paid for separately, but will be included in the cost of the associated pavement and/or overlay pay items.

## POLYMERIZED BITUMINOUS MATERIALS (TACK COAT) RATES

| Surface Type             | Residual Rate  |
|--------------------------|----------------|
| Milled (HMA or PCC)      | 0.08 lb /sq ft |
| Existing Pavement        | 0.04 lb /sq ft |
| Fog Coat (between lifts) | 0.04 lb /sq ft |
| Cold-In-Place Recycling  | 0.08 lb /sq ft |

MODEL NUMBER: 11/15/13  
FILE NAME: 11/15/13

|                       |            |           |   |   |  |  |                            |         |        |              |           |
|-----------------------|------------|-----------|---|---|--|--|----------------------------|---------|--------|--------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>GENERAL NOTES AND COMMITMENTS</b>    |  |  | F.A.P. RTE.                | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |   |   |  |  | (137, 129)RS-4, (137X)RS-4 | FULTON  | 73     | 2            |           |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |   | SCALE: SHEET 1 OF 2 SHEETS STA. TO STA. |  |  | CONTRACT NO. 68A91         |         |        |              |           |
|                       | DATE -     | REVISED - |   |   |  |  | ILLINOIS FED. AID PROJECT  |         |        |              |           |

## GENERAL NOTES (CONT.)

### HOT-MIX ASPHALT MIXTURE REQUIREMENTS

| Mixture Use(s):                          | Polymer Surface 1.5" | Polymer Level Binder 1" | Shoulders (Surface Lift) | Incidental Surface |
|--|----------------------|-------------------------|--------------------------|--------------------|
| AC/PG:                                   | SBS or SBR 76-28     | SBS or SBR 76-22        | PG 64-22                 | PG 64-22           |
| Design Air Voids:                        | 4.0% @ N=50          | 4.0% @ N=50             | 4.0% @ N=50              | 4.0% @ N=50        |
| Mixture Composition: (Mixture Gradation) | IL 9.5               | IL 4.75                 | IL 9.5                   | IL 9.5             |
| Friction Aggregate:                      | Mix D                | N.A.                    | Mix D                    | Mix C              |
| Quality Management Program:              | PFP                  | PFP                     | QCQA                     | QCQA               |

#### Notes:

- 1) Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal maximum.
- 2) For design purposes, mixture weight for all mixes is determined to be 112.0 lb/s.y/in., unless otherwise noted.
- 3) Sublot sizes for PFP and QCP mixes will be 1,000 tons, unless otherwise agreed to by the Engineer and the paving Contractor.

### BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

### PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

### STATUS OF UTILITIES

Ameren – Ellie Henshaw (217) 2121-0866 EHenshaw@ameren.com  
 AT&T – Nathan Ciota (309) 686-3333 NC2654@att.com  
 South Fulton Water – Phil Workman (309) 329-2239  
 Spoon River Electric – Mark Balbinot (309) 647-2700 mbalbinot@srecoop.org

R.E. shall call Ameren's Illinois Construction Engineering at 888-659-4540 prior to gas valve adjusting

### NO PASSING ZONE VERIFICATION

The resident shall contact Operations to verify the location of no passing zones prior to placement of centerline striping.

## JOB SPECIFIC NOTES

### CROSSING EXISTING STRUCTURES WITH EQUIPMENT

The following structures may be crossed with unloaded MTD.

S.N.# 029-0073  
 S.N.# 029-0074

Any structures not listed above shall be verified by the resident prior to beginning work.

### DROP-OFF BETWEEN TRAVELED LANE DURING MILLING OPERATIONS

At locations where 2 1/4" OR 2 1/2" milling is proposed (Sta 1675+26 to Sta 1696+18, Sta 212+70 to Sta 217+70, Sta 246+00 to Sta 248+00, & Sta 252+00 to Sta 254+00); DUE TO SAFETY; The contractor shall have one of the following three options:

- a) Mill both Eastbound and Westbound lanes the same day prior to opening both lanes to traffic.
- b) Mill the exposed edge of the center line at a 1:3 slope.
- c) Place a temporary wedge at the exposed edge at a minimum slope of 1:3.

### COLD-IN-PLACE RECYCLING

The HMA Binder Course, IL-12.5, N50 shall be placed on top of the Cold in-Place Recycled Bituminous Materials within the time frame described in the Special Provision Entitled "Cold IP (In Place) Recycle of Bituminous Materials."

The 1.5" milling shall be done before the 3" CIP Recycling.

### CURB AND GUTTER

Property owners shall be notified before the construction of the curb and gutter. Access shall be provided at all times. Temporary ramps may be needed after the construction of the curb. All constructed curb shall be depressed at existing entrances and sidewalk ramps. Sawcuts may be required to remove sidewalk or pavement for formwork.

This work will not be paid for separately, but will be included in the cost of the associated milling pay items.

### SIGNS

The Operations Engineers shall be contacted before the relocation of any signs.

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

| F.A.P. RTE.                    | SECTION | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|--------------------------------|---------|----------|--------------------|-----------|
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON   | 73                 | 3         |
|                                |         |          | CONTRACT NO. 68A91 |           |
|                                |         | ILLINOIS | FED. AID PROJECT   |           |

| CODE NO.   | ITEM   | UNIT  | TOTAL QUANTITY | NHPP                  |                       | STP               |                      |
|------------|--|-------|----------------|-----------------------|-----------------------|-------------------|----------------------|
|            |  |       |                | CONSTRUCTION CODE     |                       |                   |                      |
|            |  |       |                | US 24 FULTON 0005     | IL 78/97 FULTON 0005  | US 24 FULTON 0005 | IL 78/97 FULTON 0005 |
|            |  |       |                | 80% FEDERAL 20% STATE | 80% FEDERAL 20% STATE | 100% STATE        | 100% STATE           |
| 25000750   | MOWING   | ACRE  | 29             |                       |                       | 23                | 6                    |
| 28100207   | STONE RIPRAP, CLASS A4                                     | TON   | 40             | 40                    |                       |                   |                      |
| 40200800   | AGGREGATE SURFACE COURSE, TYPE B                           | TON   | 56             | 49                    | 7                     |                   |                      |
| 40600295   | POLYMERIZED BITUMINOUS MATERIALS (TACK COAT)               | POUND | 171492         | 137214                | 34278                 |                   |                      |
| 40600827   | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON   | 7759           | 6052                  | 1707                  |                   |                      |
| 40600982   | HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT             | SQ YD | 2552           | 2335                  | 217                   |                   |                      |
| 40600990   | TEMPORARY RAMP   | SQ YD | 181            | 142                   | 39                    |                   |                      |
| 40603335   | HOT - MIX ASPHALT SURFACE COURSE, MIX "D", N50             | TON   | 11549          | 9078                  | 2471                  |                   |                      |
| 40800050   | INCIDENTAL HOT - MIX ASPHALT SURFACING                     | TON   | 165            | 119                   | 46                    |                   |                      |
| 42300400   | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH         | SQ YD | 885            | 885                   |                       |                   |                      |
| 42400100   | PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH                   | SQ FT | 726            | 726                   |                       |                   |                      |
| * 42400800 | DETECTABLE WARNINGS  | SQ FT | 60             | 60                    |                       |                   |                      |
| 44000151   | HOT - MIX ASPHALT SURFACE REMOVAL, 1/2"                    | SQ YD | 67173          | 67173                 |                       |                   |                      |
| 44000155   | HOT - MIX ASPHALT SURFACE REMOVAL, 1 1/2"                  | SQ YD | 48724          | 48724                 |                       |                   |                      |

\*= SPECIALTY ITEM

MODEL NUMBER  
FILE NAME: STLS

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: SHEET 1 OF 5 SHEETS STA. TO STA.

|             |                            |        |                           |           |
|-------------|----------------------------|--------|---------------------------|-----------|
| F.A.P. RTE. | SECTION                    | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR         | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                        | 4         |
|             |                            |        | CONTRACT NO. 68A91        |           |
|             |                            |        | ILLINOIS FED. AID PROJECT |           |

| CODE NO.    | ITEM  | UNIT       | TOTAL QUANTITY | NHPP              |          | STP    |          |
|-------------|---|------------|----------------|-------------------|----------|--------|----------|
|             |   |            |                | CONSTRUCTION CODE |          |        |          |
|             |   |            |                | US 24             | IL 78/97 | US 24  | IL 78/97 |
|             |   |            |                | FULTON            | FULTON   | FULTON | FULTON   |
| 0005        | 0005  | 0005       | 0005           |                   |          |        |          |
| 80% FEDERAL | 80% FEDERAL                                       | 100% STATE | 100% STATE     |                   |          |        |          |
| 20% STATE   | 20% STATE   |            |                |                   |          |        |          |
| 44000156    | HOT - MIX ASPHALT SURFACE REMOVAL, 1 3/4"         | SQ YD      | 30549          |                   | 30549    |        |          |
| 44000158    | HOT - MIX ASPHALT SURFACE REMOVAL, 2 1/4"         | SQ YD      | 5957           | 5957              |          |        |          |
| 44000159    | HOT - MIX ASPHALT SURFACE REMOVAL, 2 1/2"         | SQ YD      | 1300           |                   | 1300     |        |          |
| 44000500    | COMBINATION CURB AND GUTTER REMOVAL               | FOOT       | 947            | 947               |          |        |          |
| 44000600    | SIDEWALK REMOVAL                                  | SQ FT      | 407            | 407               |          |        |          |
| 44300200    | STRIP REFLECTIVE CRACK CONTROL TREATMENT          | FOOT       | 88757          | 70445             | 18312    |        |          |
| 48101200    | AGGREGATE SHOULDERS, TYPE B                       | TON        | 1970           | 1554              | 416      |        |          |
| 48203100    | HOT - MIX ASPHALT SHOULDERS                       | TON        | 3447           | 3093              | 354      |        |          |
| 56109210    | WATER VALVES TO BE ADJUSTED                       | EACH       | 3              | 3                 |          |        |          |
| 60255500    | MANHOLES TO BE ADJUSTED                           | EACH       | 2              | 2                 |          |        |          |
| 60260100    | INLETS TO BE ADJUSTED                             | EACH       | 4              | 4                 |          |        |          |
| 60605000    | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 | FOOT       | 947            | 947               |          |        |          |
| 60618740    | CONCRETE MEDIAN, TYPE M-2.12                      | SQ FT      | 51             | 51                |          |        |          |
| * 63000001  | STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS | FOOT       | 1675           | 1675              |          |        |          |

\*= SPECIALTY ITEM

MODEL NUMBER  
FILE NAME: STLS

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: SHEET 2 OF 5 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 5         |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |

| CODE NO.   | ITEM   | UNIT    | TOTAL QUANTITY | NHPP                  |                       | STP               |                      |
|------------|--|---------|----------------|-----------------------|-----------------------|-------------------|----------------------|
|            |  |         |                | CONSTRUCTION CODE     |                       |                   |                      |
|            |  |         |                | US 24 FULTON 0005     | IL 78/97 FULTON 0005  | US 24 FULTON 0005 | IL 78/97 FULTON 0005 |
|            |  |         |                | 80% FEDERAL 20% STATE | 80% FEDERAL 20% STATE | 100% STATE        | 100% STATE           |
| * 63000003 | STEEL PLATE BEAM GUARD RAIL, TYPE A, 9 FOOT POSTS        | FOOT    | 75             | 75                    |                       |                   |                      |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT       | EACH    | 12             | 12                    |                       |                   |                      |
| 63200310   | GUARDRAIL REMOVAL  | FOOT    | 2350           | 2350                  |                       |                   |                      |
| 67000400   | ENGINEER'S FIELD OFFICE, TYPE A                          | CAL MO  | 7              | 7                     |                       |                   |                      |
| 67100100   | MOBILIZATION   | L SUM   | 1              | 0.75                  | 0.25                  |                   |                      |
| 70100450   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201          | L SUM   | 1              | 0.75                  | 0.25                  |                   |                      |
| 70100460   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306          | L SUM   | 1              | 0.75                  | 0.25                  |                   |                      |
| 70102640   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801          | L SUM   | 1              | 0.75                  | 0.25                  |                   |                      |
| 70103815   | TRAFFIC CONTROL SURVEILLANCE                             | CAL DA  | 10             | 10                    |                       |                   |                      |
| 70107025   | CHANGEABLE MESSAGE SIGN                                  | CAL DAY | 100            | 100                   |                       |                   |                      |
| 70300100   | SHORT-TERM PAVEMENT MARKING                              | FOOT    | 19841          | 16512                 | 3329                  |                   |                      |
| 70300150   | SHORT TERM PAVEMENT MARKING REMOVAL                      | SQ FT   | 1653           | 1376                  | 277                   |                   |                      |
| * 72501000 | TERMINAL MARKER - DIRECT APPLIED                         | EACH    | 12             | 12                    |                       |                   |                      |
| * 78009000 | MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT   | 310            | 310                   |                       |                   |                      |

\*= SPECIALTY ITEM

MODEL NUMBER FILE NAME FILES

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: SHEET 3 OF 5 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 6         |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |

| CODE NO.   | ITEM  | UNIT   | TOTAL QUANTITY | NHPP              |             | STP        |            |
|------------|---|--------|----------------|-------------------|-------------|------------|------------|
|            |   |        |                | CONSTRUCTION CODE |             |            |            |
|            |   |        |                | US 24             | IL 78/97    | US 24      | IL 78/97   |
|            |   |        |                | FULTON            | FULTON      | FULTON     | FULTON     |
|            |   |        |                | 0005              | 0005        | 0005       | 0005       |
|            |   |        |                | 80% FEDERAL       | 80% FEDERAL | 100% STATE | 100% STATE |
|            |   |        |                | 20% STATE         | 20% STATE   |            |            |
| * 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4"    | FOOT   | 137309         | 109450            | 27859       |            |            |
| * 78009006 | MODIFIED URETHANE PAVEMENT MARKING - LINE 6"    | FOOT   | 128            | 128               |             |            |            |
| * 78009008 | MODIFIED URETHANE PAVEMENT MARKING - LINE 8"    | FOOT   | 1150           | 1150              |             |            |            |
| * 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12"   | FOOT   | 544            | 544               |             |            |            |
| * 78009024 | MODIFIED URETHANE PAVEMENT MARKING - LINE 24"   | FOOT   | 220            | 189               | 31          |            |            |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER               | EACH   | 676            | 557               | 119         |            |            |
| * 78200005 | GUARDRAIL REFLECTORS, TYPE A                    | EACH   | 32             | 32                |             |            |            |
| 78300200   | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL       | EACH   | 651            | 543               | 108         |            |            |
| X4200001   | CIR-FDR EMULSIFIED APHALT                       | GALLON | 54702          | 54702             |             |            |            |
| X4400196   | HOT - MIX ASPHALT SURFACE REMOVAL, SPECIAL      | SQ YD  | 2466           | 1957              | 509         |            |            |
| X4401198   | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SQ YD  | 10537          | 10537             |             |            |            |
| X4402805   | ISLAND REMOVAL                                  | SQ FT  | 51             | 51                |             |            |            |
| XZ013798   | CONSTRUCTION STATION LAYOUT                     | L SUM  | 1              | 0.75              | 0.25        |            |            |
| Z0001002   | GUARDRAIL AGGREGATE EROSION CONTROL             | TON    | 478            | 478               |             |            |            |

\*= SPECIALTY ITEM

MODEL NUMBER  
FILE NAME: STLS

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

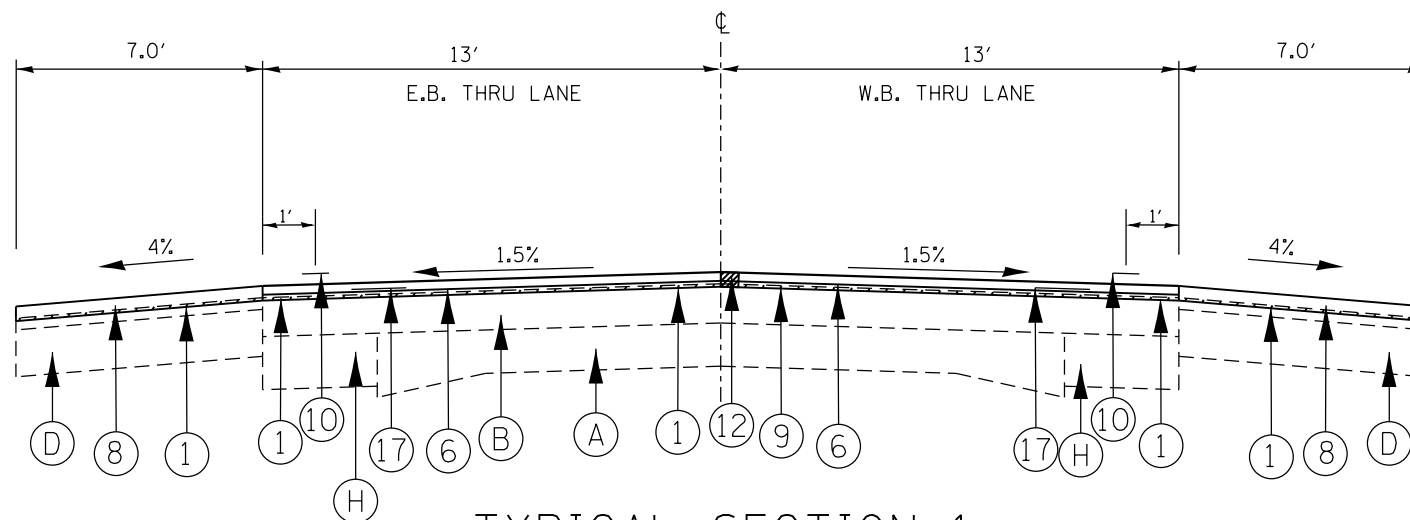
**SUMMARY OF QUANTITIES**

SCALE: SHEET 4 OF 5 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 7         |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |







TYPICAL SECTION 1

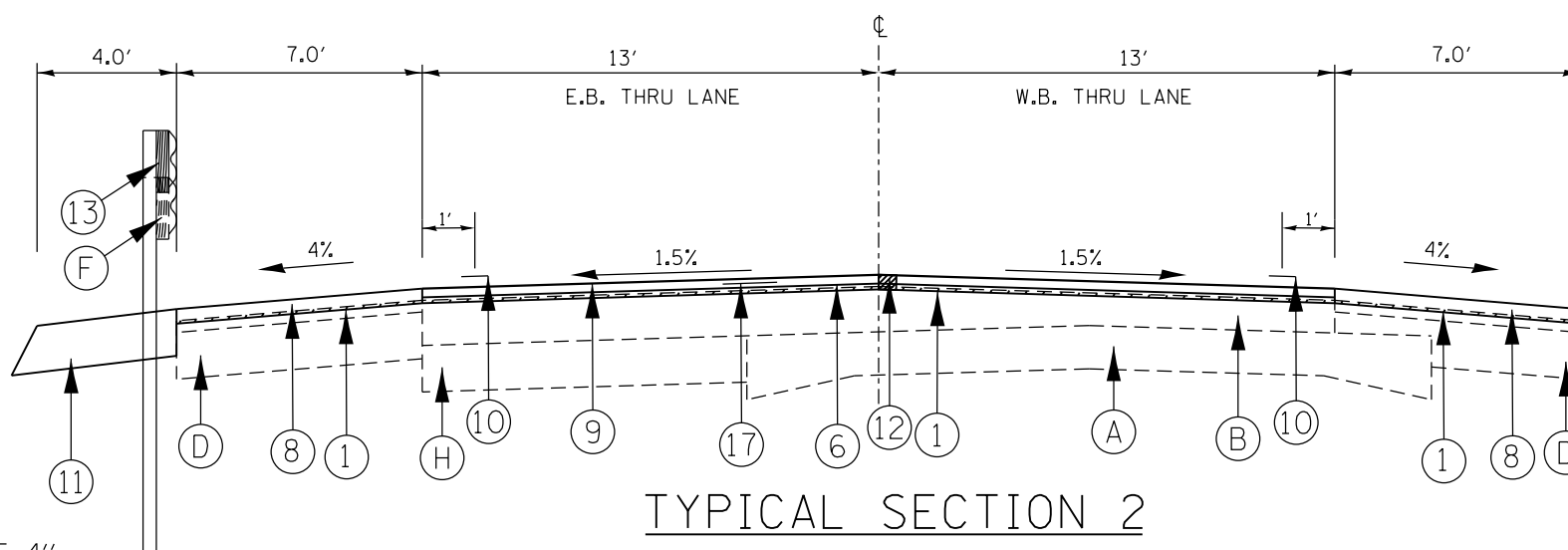
LT. STA. 1346+00 TO STA. 1353+72  
 LT. & RT. STA. 1353+72 TO STA. 1384+92.50  
 LT. & RT. STA. 1450+08 TO STA. 1451+25  
 RT. STA. 1451+25 TO RT. STA. 1451+60  
 LT. STA. 1454+00 TO LT. STA. 1454+35  
 LT. & RT. STA. 1454+35 TO STA. 1454+85  
 LT. & RT. STA. 1473+60 TO STA. 1475+50  
 RT. STA. 1488+55 TO RT. STA. 1488+80  
 LT. & RT. STA. 1488+80 TO 1490+85  
 LT. STA. 1490+85 TO LT. STA. 1490+93

LEGEND - EXISTING

- (A) EXIT. PCC PAVEMENT
- (B) EXIT. HMA OVERLAY
- (C) EXIT. AGGREGATE SHOULDERS
- (D) EXIT. HMA SHOULDER
- (E) EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- (F) EXIT. STEEL PLATE BEAM GUARD RAIL
- (G) EXIT. SIDE WALK
- (H) EXIT. BITUMINOUS WIDENING
- (I) EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- (J) EXISTING PAVEMENT FABRIC

LEGEND - PROPOSED

- (1) PROP. HMA SURFACE REMOVAL - 1/2"
- (2) PROP. HMA SURFACE REMOVAL - 2 1/4"
- (3) PROP. HMA SURFACE REMOVAL - 1 3/4"
- (4) PROP. HMA SURFACE REMOVAL - 1 1/2"
- (5) PROP. HMA SURFACE REMOVAL - 2 1/2"
- (6) PROP. LEVELING BINDER - 1"
- (7) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) PROP. HMA SHOULDERS
- (9) PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (10) PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- (11) PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- (12) PROP. HMA REMOVAL SPECIAL
- (13) PROP. GUARDRAIL REPLACEMENT
- (14) PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- (15) PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- (16) PROP. 3" COLD-IN-PLACE RECYCLING
- (17) PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



TYPICAL SECTION 2

LT. & RT. STA. 1384+92.50 TO STA. 1388+70

OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: S:\MODEL\MAMES  
FILE NAME: ST125

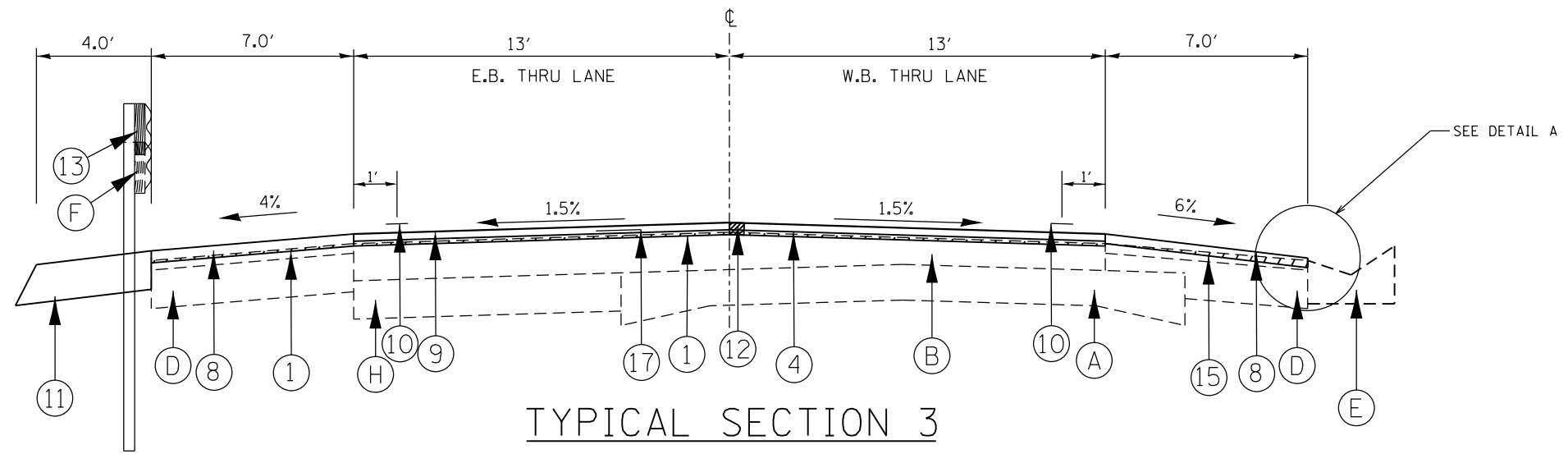
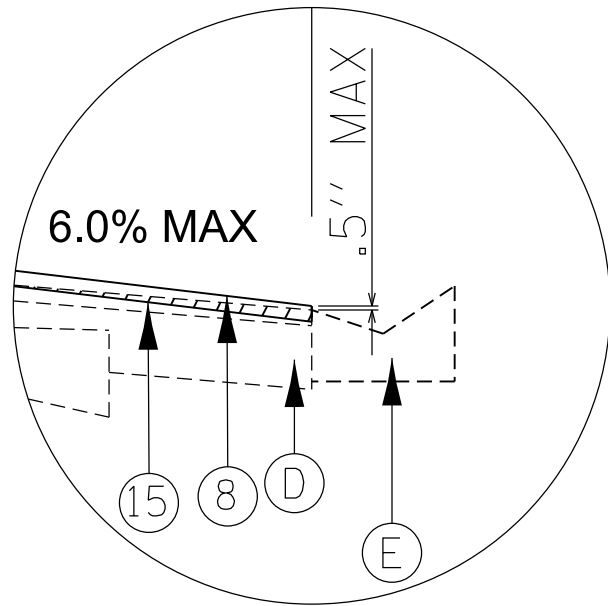
|                      |            |           |
|----------------------|------------|-----------|
| USER NAME = SUSERS   | DESIGNED - | REVISED - |
| PLOT SCALE = SSCALE5 | DRAWN -    | REVISED - |
| PLOT DATE = SDATES   | CHECKED -  | REVISED - |
|                      | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                        |         |              |              |
|------------------------|---------|--------------|--------------|
| TYPICAL SECTIONS US 24 |         |              |              |
| SCALE: NO SCALE        | SHEET 1 | OF 12 SHEETS | STA. TO STA. |

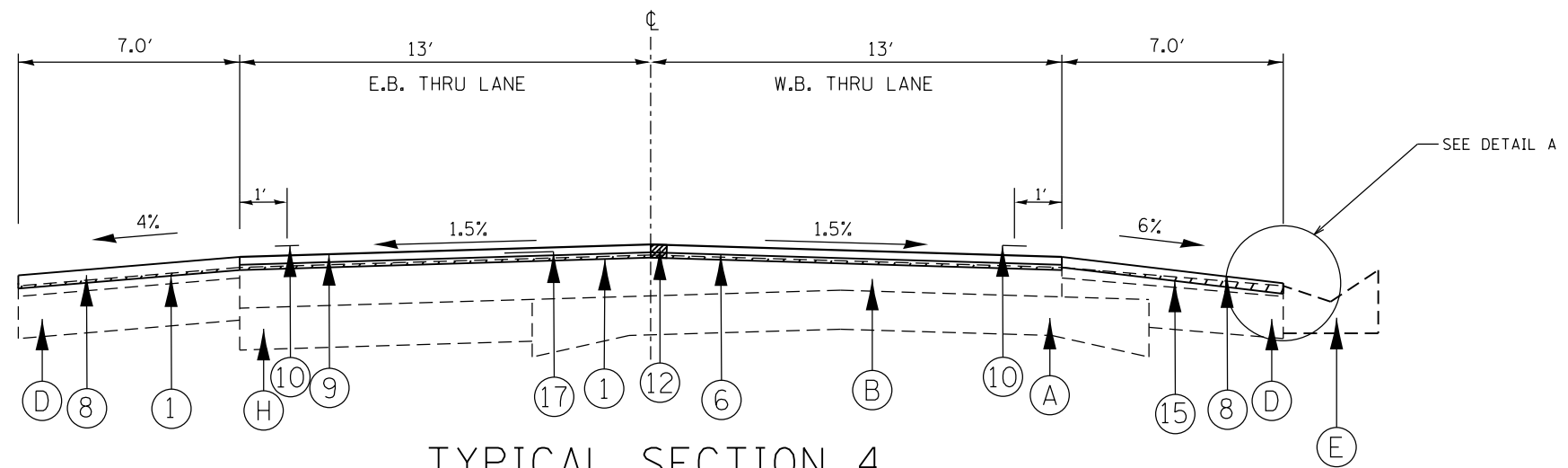
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|--------------------------------|---------|--------|---------------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                        | 9         |
| CONTRACT NO. 68A91             |         |        | ILLINOIS FED. AID PROJECT |           |

# Detail A



TYPICAL SECTION 3

LT. & RT. STA. 1388+70 TO STA. 1390+80



TYPICAL SECTION 4

LT. & RT. STA. 1390+80 TO STA. 1392+90

### LEGEND - EXISTING

- (A) EXIT. PCC PAVEMENT
- (B) EXIT. HMA OVERLAY
- (C) EXIT. AGGREGATE SHOULDERS
- (D) EXIT. HMA SHOULDER
- (E) EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- (F) EXIT. STEEL PLATE BEAM GUARD RAIL
- (G) EXIT. SIDE WALK
- (H) EXIT. BITUMINOUS WIDENING
- (1) EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- (J) EXISTING PAVEMENT FABRIC

### LEGEND - PROPOSED

- (1) PROP. HMA SURFACE REMOVAL - 1/2"
- (2) PROP. HMA SURFACE REMOVAL - 2 1/4"
- (3) PROP. HMA SURFACE REMOVAL - 1 3/4"
- (4) PROP. HMA SURFACE REMOVAL - 1 1/2"
- (5) PROP. HMA SURFACE REMOVAL - 2 1/2"
- (6) PROP. LEVELING BINDER - 1"
- (7) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) PROP. HMA SHOULDERS
- (9) PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (10) PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- (11) PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- (12) PROP. HMA REMOVAL SPECIAL
- (13) PROP. GUARDRAIL REPLACEMENT
- (14) PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- (15) PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- (16) PROP. 3" COLD-IN-PLACE RECYCLING
- (17) PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)

### OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: 140621MAMES  
FILE: 140621MAMES.DWG

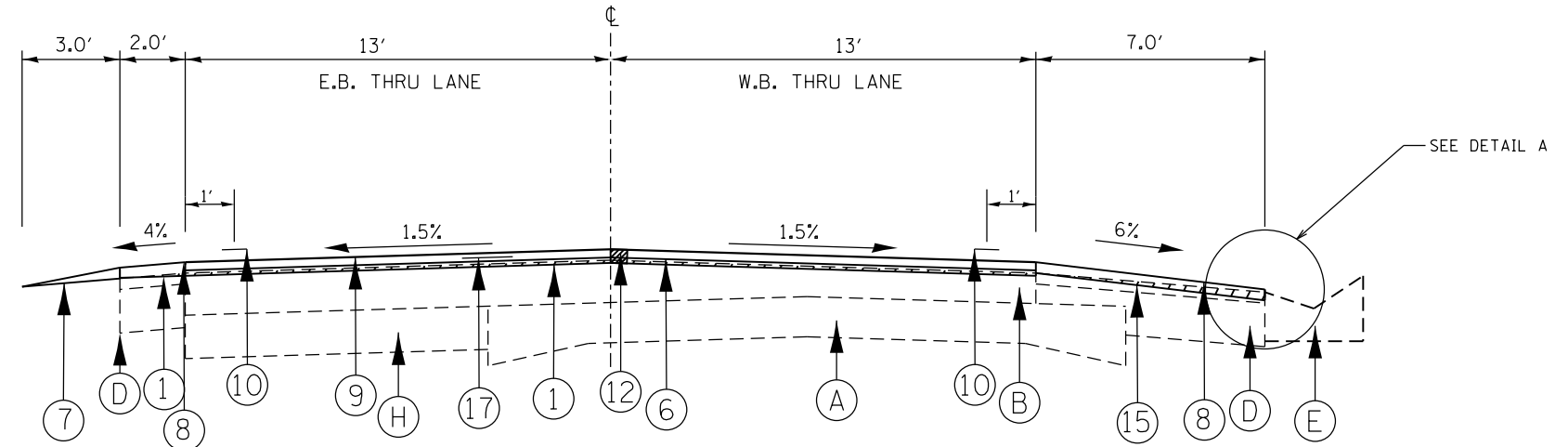
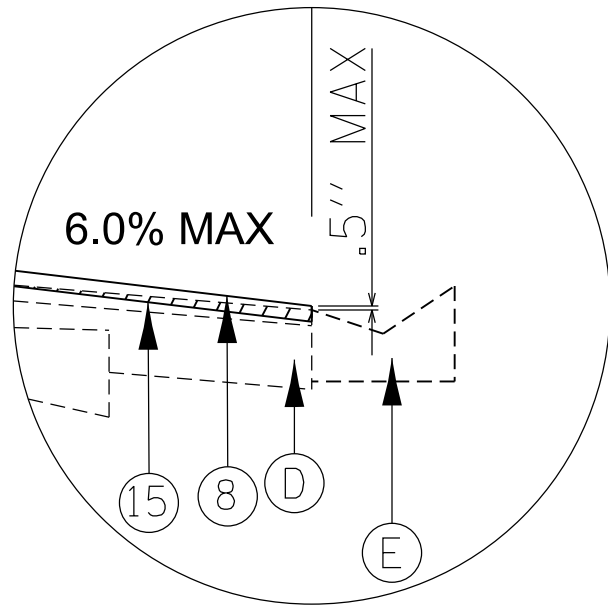
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|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                        |         |              |              |
|------------------------|---------|--------------|--------------|
| TYPICAL SECTIONS US 24 |         |              |              |
| SCALE: NO SCALE        | SHEET 2 | OF 12 SHEETS | STA. TO STA. |

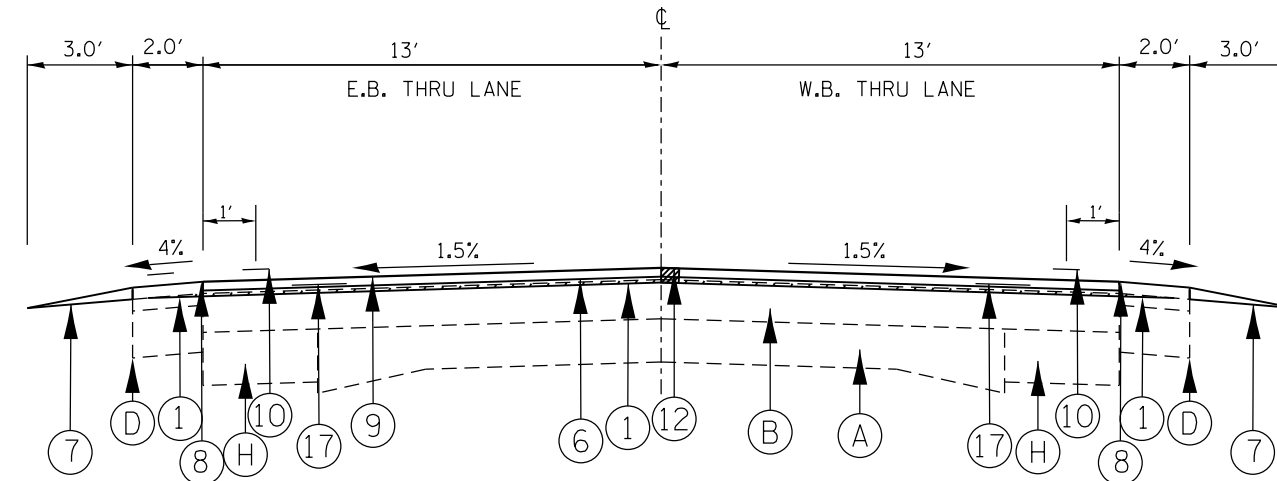
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|--------------------------------|---------|---------------------------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY                    | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON                    | 73                 | 10        |
|                                |         |                           | CONTRACT NO. 68A91 |           |
|                                |         | ILLINOIS FED. AID PROJECT |                    |           |

# Detail A



## TYPICAL SECTION 5

LT. & RT. STA. 1392+90 TO STA. 1400+46  
 LT. & RT. STA. 1461+65 TO STA. 1464+80



## TYPICAL SECTION 6

RT. STA. 1346+00 TO STA. 1353+72  
 LT. & RT. STA. 1400+46 TO STA. 1414+94.83 (BK)  
 LT. & RT. STA. 1414+99.36 (AH) TO STA. 1421+00  
 LT. & RT. STA. 1428+31 TO STA. 1440+60  
 LT. & RT. STA. 1454+85 TO STA. 1457+50  
 LT. & RT. STA. 1475+50 TO STA. 1483+65  
 RT. STA. 1483+65 TO RT. STA. 1483+85

### OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

### LEGEND - EXISTING

- Ⓐ EXIT. PCC PAVEMENT
- Ⓑ EXIT. HMA OVERLAY
- Ⓒ EXIT. AGGREGATE SHOULDERS
- Ⓓ EXIT. HMA SHOULDER
- Ⓔ EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- Ⓕ EXIT. STEEL PLATE BEAM GUARD RAIL
- Ⓖ EXIT. SIDE WALK
- Ⓗ EXIT. BITUMINOUS WIDENING
- ① EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- ⓐ EXISTING PAVEMENT FABRIC

### LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL - 1/2"
- ② PROP. HMA SURFACE REMOVAL - 2 1/4"
- ③ PROP. HMA SURFACE REMOVAL - 1 3/4"
- ④ PROP. HMA SURFACE REMOVAL - 1 1/2"
- ⑤ PROP. HMA SURFACE REMOVAL - 2 1/2"
- ⑥ PROP. LEVELING BINDER - 1"
- ⑦ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROP. HMA SHOULDERS
- ⑨ PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- ⑪ PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- ⑫ PROP. HMA REMOVAL SPECIAL
- ⑬ PROP. GUARDRAIL REPLACEMENT
- ⑭ PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- ⑮ PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑯ PROP. 3" COLD-IN-PLACE RECYCLING
- ⑰ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)

MODEL: SMOBELMAMES  
FILE NAME: ST115

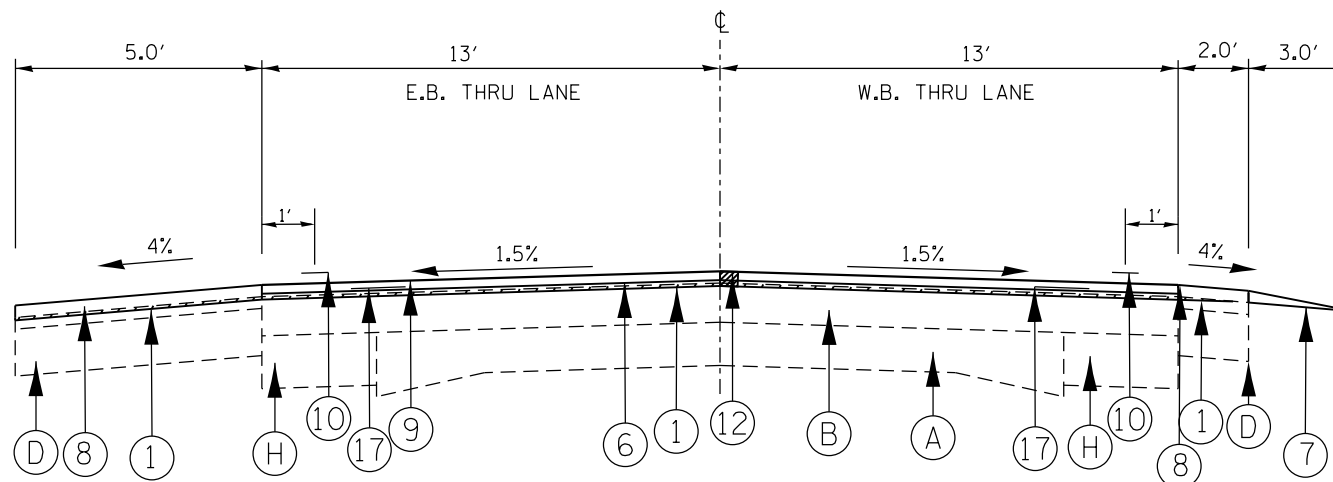
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| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS US 24**

SCALE: NO SCALE SHEET 3 OF 12 SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------|--------|--------------|-----------|
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 11        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



**TYPICAL SECTION 7**

LT. & RT. STA. 1421+00 TO STA. 1422+01

# EXISTING GUARDRAIL TO REMAIN LOCATED FROM  
 LT. STA. 1422+28.38 TO STA. 1423+78.38  
 LT. STA. 1425+17.62 TO STA. 1426+92.62  
 LT. STA. 1444+78.25 TO STA. 1446+15.78  
 LT. STA. 1447+69.25 TO STA. 1448+56.75

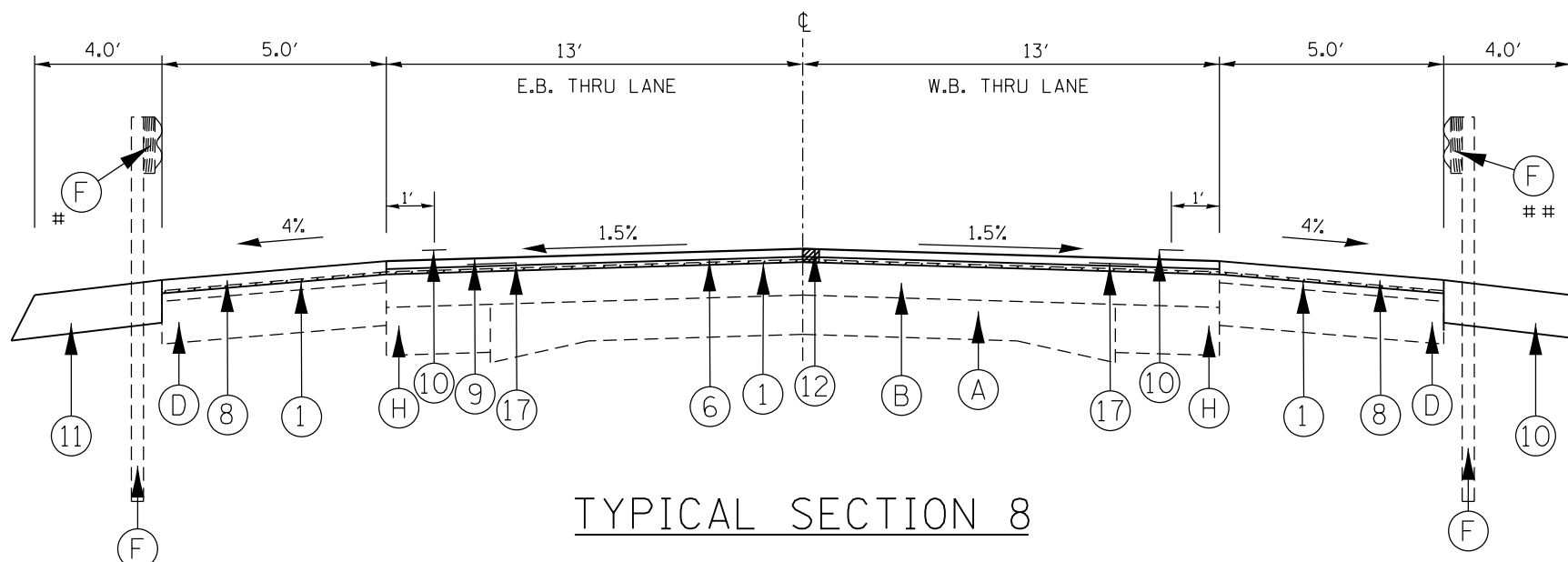
## EXISTING GUARDRAIL TO REMAIN LOCATED FROM  
 RT. STA. 1421+90.88 TO STA. 1423+78.38  
 RT. STA. 1425+17.62 TO STA. 1426+55.12  
 RT. STA. 1445+03.25 TO STA. 1446+15.75  
 RT. STA. 1447+69.25 TO STA. 1448+31.75

**LEGEND - EXISTING**

- (A) EXIT. PCC PAVEMENT
- (B) EXIT. HMA OVERLAY
- (C) EXIT. AGGREGATE SHOULDERS
- (D) EXIT. HMA SHOULDER
- (E) EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- (F) EXIT. STEEL PLATE BEAM GUARD RAIL
- (G) EXIT. SIDE WALK
- (H) EXIT. BITUMINOUS WIDENING
- (1) EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- (J) EXISTING PAVEMENT FABRIC

**LEGEND - PROPOSED**

- (1) PROP. HMA SURFACE REMOVAL - 1/2"
- (2) PROP. HMA SURFACE REMOVAL - 2 1/4"
- (3) PROP. HMA SURFACE REMOVAL - 1 3/4"
- (4) PROP. HMA SURFACE REMOVAL - 1 1/2"
- (5) PROP. HMA SURFACE REMOVAL - 2 1/2"
- (6) PROP. LEVELING BINDER - 1"
- (7) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) PROP. HMA SHOULDERS
- (9) PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (10) PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- (11) PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- (12) PROP. HMA REMOVAL SPECIAL
- (13) PROP. GUARDRAIL REPLACEMENT
- (14) PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- (15) PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- (16) PROP. 3" COLD-IN-PLACE RECYCLING
- (17) PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



**TYPICAL SECTION 8**

LT. & RT. STA. 1422+01 TO STA. 1423+78.38  
 LT. & RT. STA. 1425+17.62 TO STA. 1428+31  
 LT. & RT. STA. 1440+60 TO STA. 1446+15.75  
 LT. & RT. STA. 1447+69.24 TO STA. 1450+08

**OMISSIONS**

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: SHOBELNAMES  
FILE NAME: STILES

|                     |            |           |
|---------------------|------------|-----------|
| USER NAME = SUSERS  | DESIGNED - | REVISED - |
| PLOT SCALE = SCALES | DRAWN -    | REVISED - |
| PLOT DATE = SDATES  | CHECKED -  | REVISED - |
|                     | DATE -     | REVISED - |

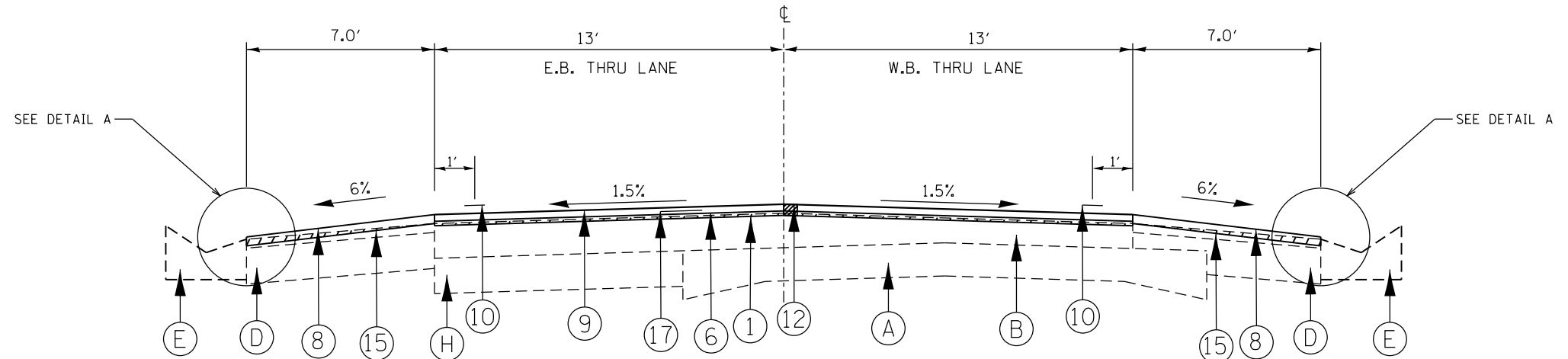
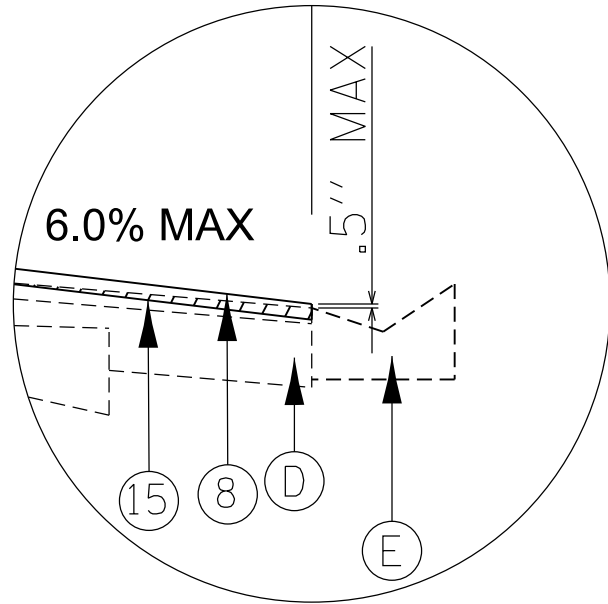
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |                      |
|-------------------------------|----------------------|
| <b>TYPICAL SECTIONS US 24</b> |                      |
| SCALE: NO SCALE               | SHEET 4 OF 12 SHEETS |
| STA.                          | TO STA.              |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 12        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |

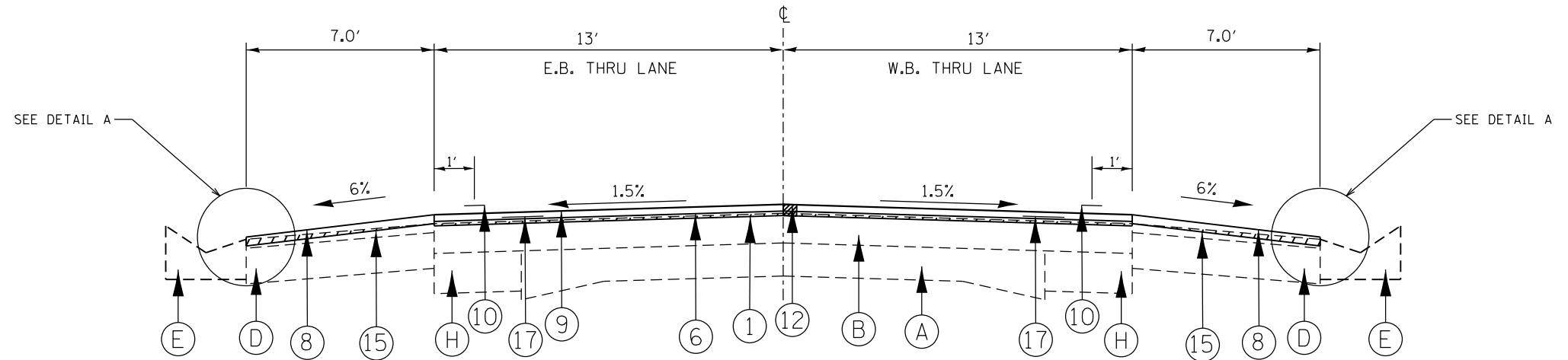


# Detail A



## TYPICAL SECTION 11

LT. & RT. STA. 1464+80 TO STA. 1473+60



## TYPICAL SECTION 12

LT. STA. 1483+65 TO LT. STA. 1483+85  
 LT. & RT. STA. 1483+85 TO STA. 1488+55  
 LT. STA. 1488+55 TO LT. STA. 1488+80  
 RT. STA. 1490+85 TO RT. STA. 1490+93  
 LT. & RT. STA. 1490+93 TO STA. 1494+77  
 RT. STA. 1494+77 TO RT. STA. 1495+98  
 RT. STA. 1498+98 TO RT. STA. 1499+39.50  
 LT. & RT. STA. 1499+39.50 TO STA. 1503+80  
 RT. STA. 1503+80 TO RT. STA. 1503+95

### OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

### LEGEND - EXISTING

- (A) EXIT. PCC PAVEMENT
- (B) EXIT. HMA OVERLAY
- (C) EXIT. AGGREGATE SHOULDERS
- (D) EXIT. HMA SHOULDER
- (E) EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- (F) EXIT. STEEL PLATE BEAM GUARD RAIL
- (G) EXIT. SIDE WALK
- (H) EXIT. BITUMINOUS WIDENING
- (1) EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- (J) EXISTING PAVEMENT FABRIC

### LEGEND - PROPOSED

- (1) PROP. HMA SURFACE REMOVAL - 1/2"
- (2) PROP. HMA SURFACE REMOVAL - 2 1/4"
- (3) PROP. HMA SURFACE REMOVAL - 1 3/4"
- (4) PROP. HMA SURFACE REMOVAL - 1 1/2"
- (5) PROP. HMA SURFACE REMOVAL - 2 1/2"
- (6) PROP. LEVELING BINDER - 1"
- (7) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) PROP. HMA SHOULDERS
- (9) PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (10) PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- (11) PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- (12) PROP. HMA REMOVAL SPECIAL
- (13) PROP. GUARDRAIL REPLACEMENT
- (14) PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- (15) PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- (16) PROP. 3" COLD-IN-PLACE RECYCLING
- (17) PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)

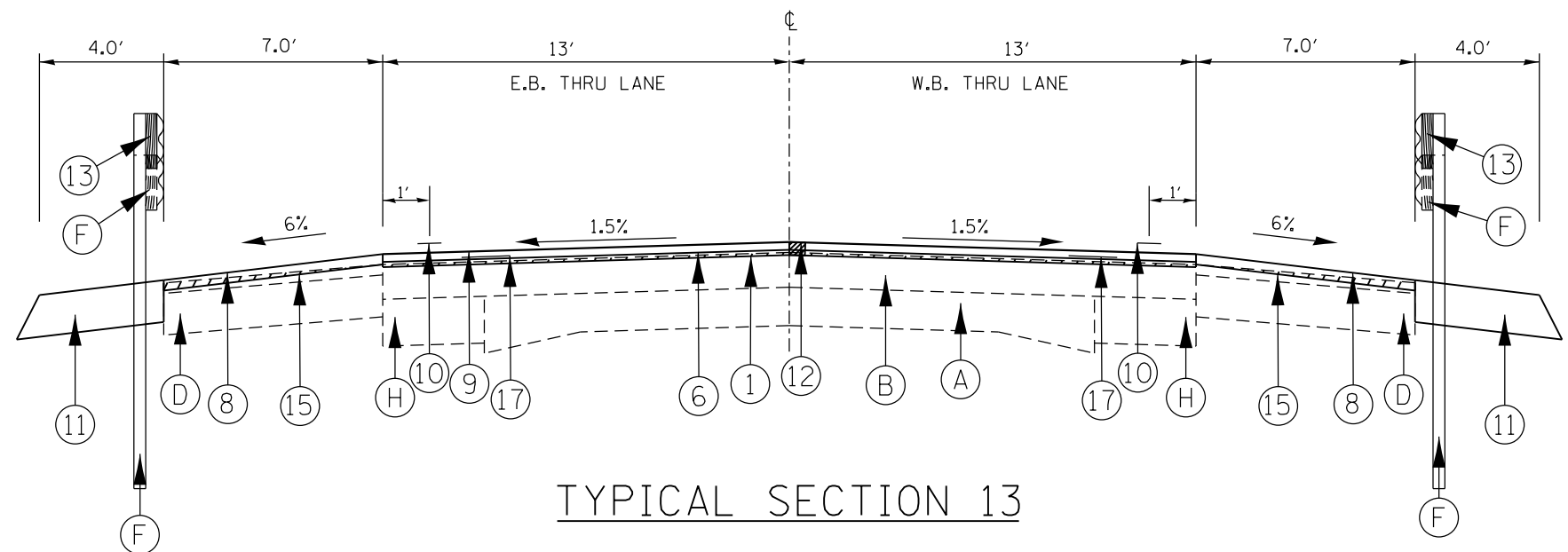
MODEL: 140826MMMS  
FILE: 140826\_MMMS\_STEELS

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |                      |              |
|-------------------------------|----------------------|--------------|
| <b>TYPICAL SECTIONS US 24</b> |                      |              |
| SCALE: NO SCALE               | SHEET 6 OF 12 SHEETS | STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 14        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



**TYPICAL SECTION 13**

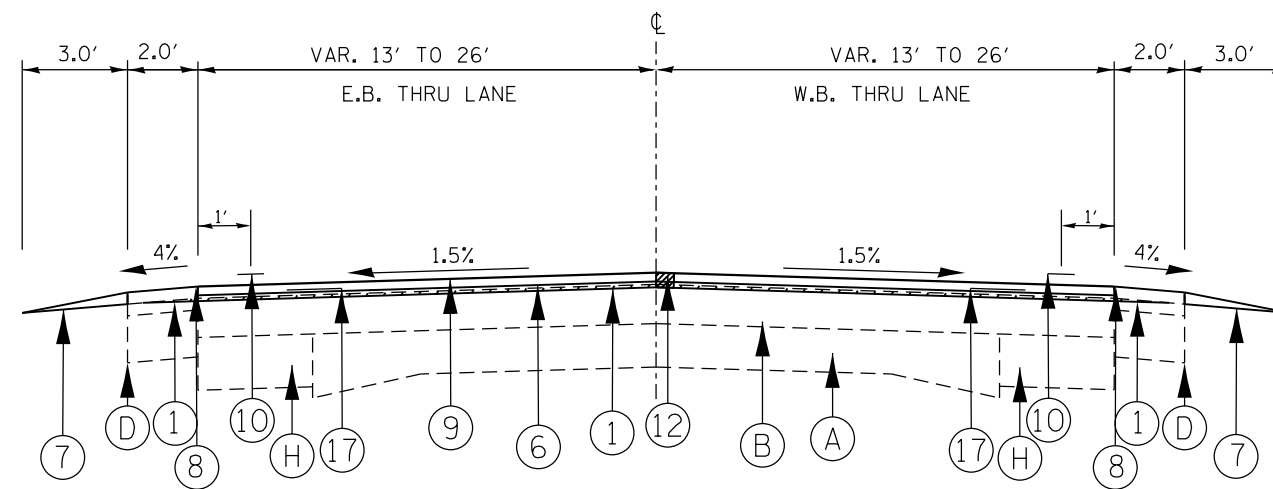
LT. STA. 1494+77 TO LT. STA. 1495+98  
 LT. & RT. STA. 1495+98 TO STA. 1498+98  
 LT. STA. 1498+98 TO LT. STA. 1499+39.50

**LEGEND - EXISTING**

- (A) EXIT. PCC PAVEMENT
- (B) EXIT. HMA OVERLAY
- (C) EXIT. AGGREGATE SHOULDERS
- (D) EXIT. HMA SHOULDER
- (E) EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- (F) EXIT. STEEL PLATE BEAM GUARD RAIL
- (G) EXIT. SIDE WALK
- (H) EXIT. BITUMINOUS WIDENING
- (1) EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- (J) EXISTING PAVEMENT FABRIC

**LEGEND - PROPOSED**

- (1) PROP. HMA SURFACE REMOVAL - 1/2"
- (2) PROP. HMA SURFACE REMOVAL - 2 1/4"
- (3) PROP. HMA SURFACE REMOVAL - 1 3/4"
- (4) PROP. HMA SURFACE REMOVAL - 1 1/2"
- (5) PROP. HMA SURFACE REMOVAL - 2 1/2"
- (6) PROP. LEVELING BINDER - 1"
- (7) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) PROP. HMA SHOULDERS
- (9) PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (10) PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- (11) PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- (12) PROP. HMA REMOVAL SPECIAL
- (13) PROP. GUARDRAIL REPLACEMENT
- (14) PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- (15) PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- (16) PROP. 3" COLD-IN-PLACE RECYCLING
- (17) PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



**TYPICAL SECTION 14**

LT. STA. 1503+80 TO 1503+95  
 LT. & RT. STA. 1503+95 TO STA. 1523+55

**OMISSIONS**

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: S:\MODEL\NAMES  
FILE NAME: STILES

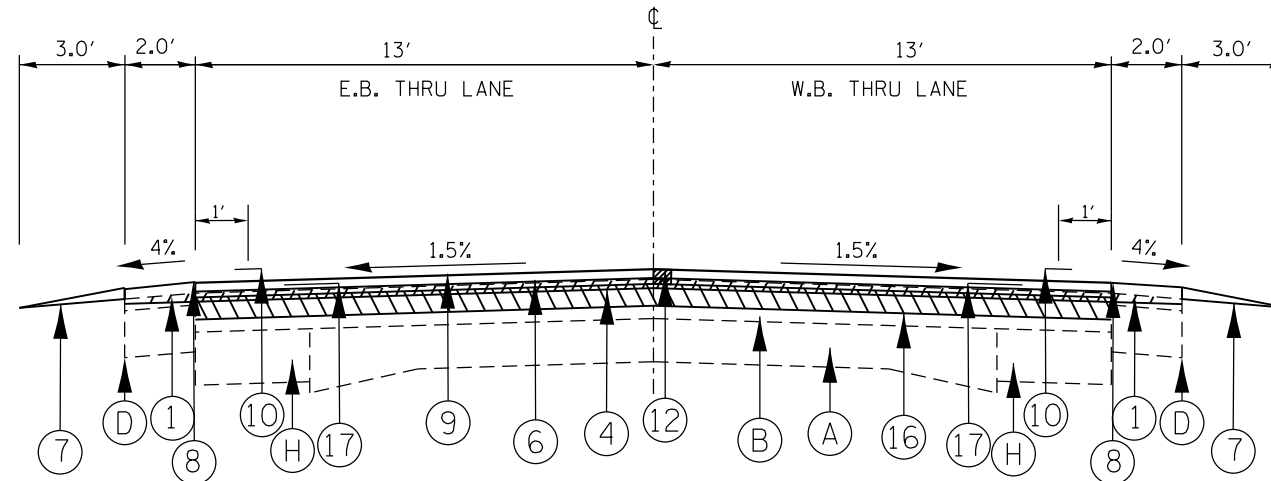
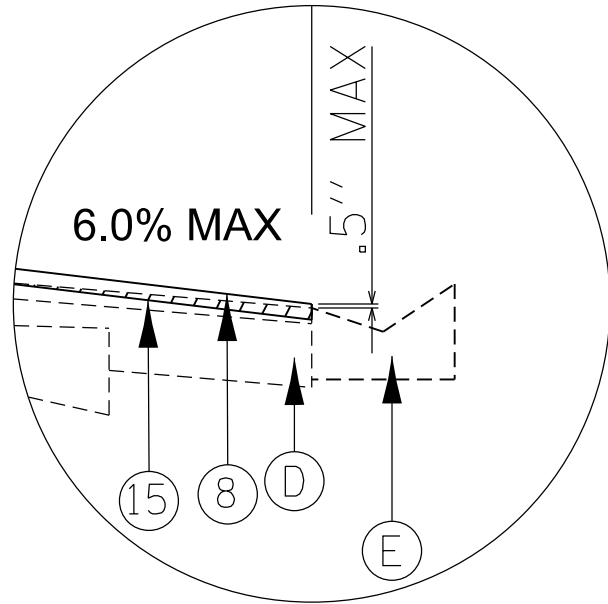
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| USER NAME = SUSERS  | DESIGNED - | REVISED - |
| PLOT SCALE = SCALES | DRAWN -    | REVISED - |
| PLOT DATE = SDATES  | CHECKED -  | REVISED - |
|                     | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS US 24**  
 SCALE: NO SCALE SHEET 7 OF 12 SHEETS STA. TO STA.

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 15        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |

# Detail A



TYPICAL SECTION 15

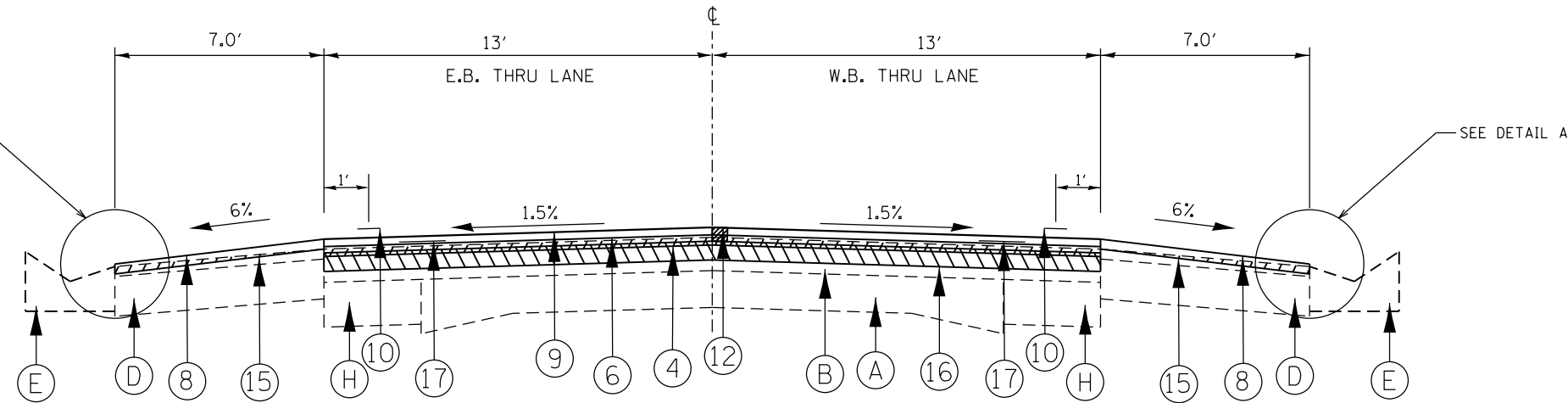
LT. & RT. STA. 1523+55 TO STA. 1523+84.61 (BK)  
 LT. & RT. STA. 1523+94.39 (AH) TO STA. 1555+00  
 LT. STA. 1555+00 TO LT. STA. 1555+55  
 LT. STA. 1560+75 TO LT. STA. 1561+00  
 LT. & RT. STA. 1561+00 TO STA. 1563+25  
 LT. STA. 1565+80 TO LT. STA. 1566+30  
 LT. & RT. STA. 1566+30 TO STA. 1574+25  
 LT. STA. 1574+25 TO LT. STA. 1575+00  
 LT. STA. 1578+35 TO LT. STA. 1578+90  
 LT. & RT. STA. 1578+90 TO STA. 1612+61.77 (BK)  
 LT. & RT. STA. 1612+69.41 (AH) TO STA. 1659+50

**LEGEND - EXISTING**

- (A) EXIT. PCC PAVEMENT
- (B) EXIT. HMA OVERLAY
- (C) EXIT. AGGREGATE SHOULDERS
- (D) EXIT. HMA SHOULDER
- (E) EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- (F) EXIT. STEEL PLATE BEAM GUARD RAIL
- (G) EXIT. SIDE WALK
- (H) EXIT. BITUMINOUS WIDENING
- (1) EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- (J) EXISTING PAVEMENT FABRIC

**LEGEND - PROPOSED**

- (1) PROP. HMA SURFACE REMOVAL - 1/2"
- (2) PROP. HMA SURFACE REMOVAL - 2 1/4"
- (3) PROP. HMA SURFACE REMOVAL - 1 3/4"
- (4) PROP. HMA SURFACE REMOVAL - 1 1/2"
- (5) PROP. HMA SURFACE REMOVAL - 2 1/2"
- (6) PROP. LEVELING BINDER - 1"
- (7) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- (8) PROP. HMA SHOULDERS
- (9) PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- (10) PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- (11) PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- (12) PROP. HMA REMOVAL SPECIAL
- (13) PROP. GUARDRAIL REPLACEMENT
- (14) PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- (15) PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- (16) PROP. 3" COLD-IN-PLACE RECYCLING
- (17) PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



TYPICAL SECTION 16

RT. STA. 1555+00 TO RT. STA. 1555+55  
 LT. & RT. STA. 1555+55 TO STA. 1560+75  
 RT. STA. 1560+75 TO RT. STA. 1561+00  
 LT. & RT. STA. 1563+25 TO STA. 1565+80  
 RT. STA. 1565+80 TO RT. STA. 1566+30  
 RT. STA. 1574+25 TO RT. STA. 1575+00  
 LT. & RT. STA. 1575+00 TO STA. 1578+35  
 RT. STA. 1578+35 TO RT. STA. 1578+90

**OMISSIONS**

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: MODELNAMES  
FILE: MODEL FILES

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

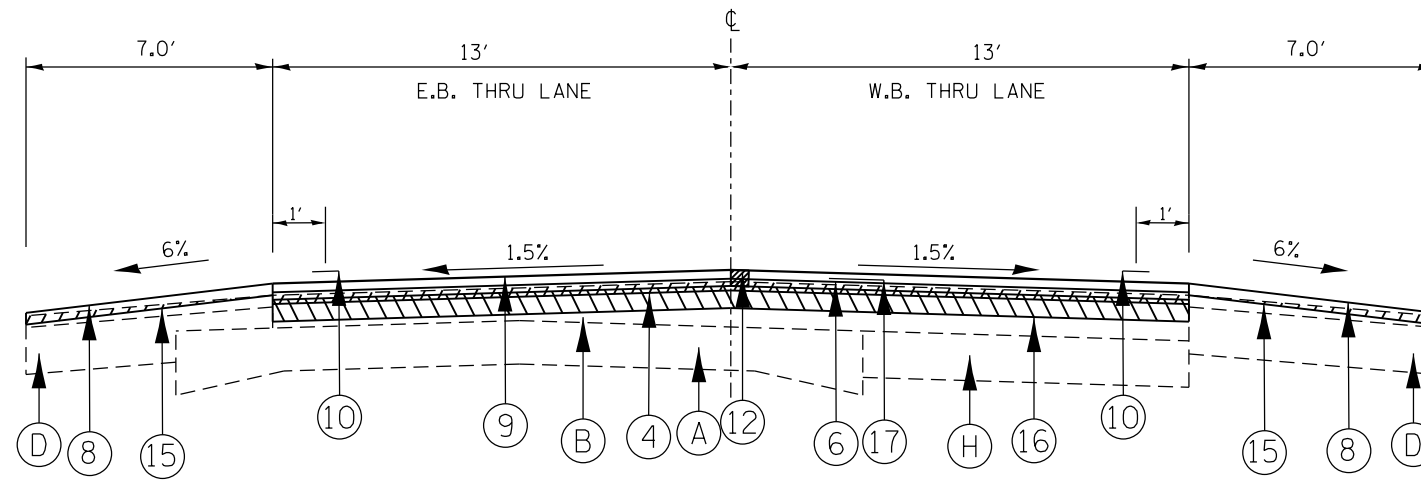
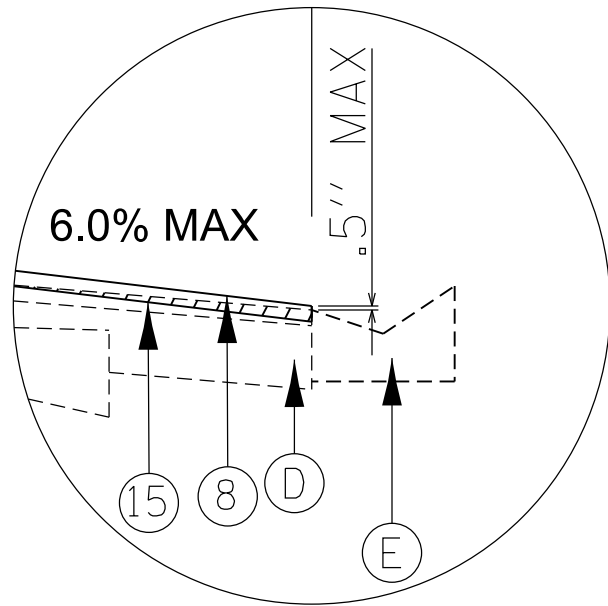
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |                      |              |
|-------------------------------|----------------------|--------------|
| <b>TYPICAL SECTIONS US 24</b> |                      |              |
| SCALE: NO SCALE               | SHEET 8 OF 12 SHEETS | STA. TO STA. |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 16        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |



# Detail A



## TYPICAL SECTION 17

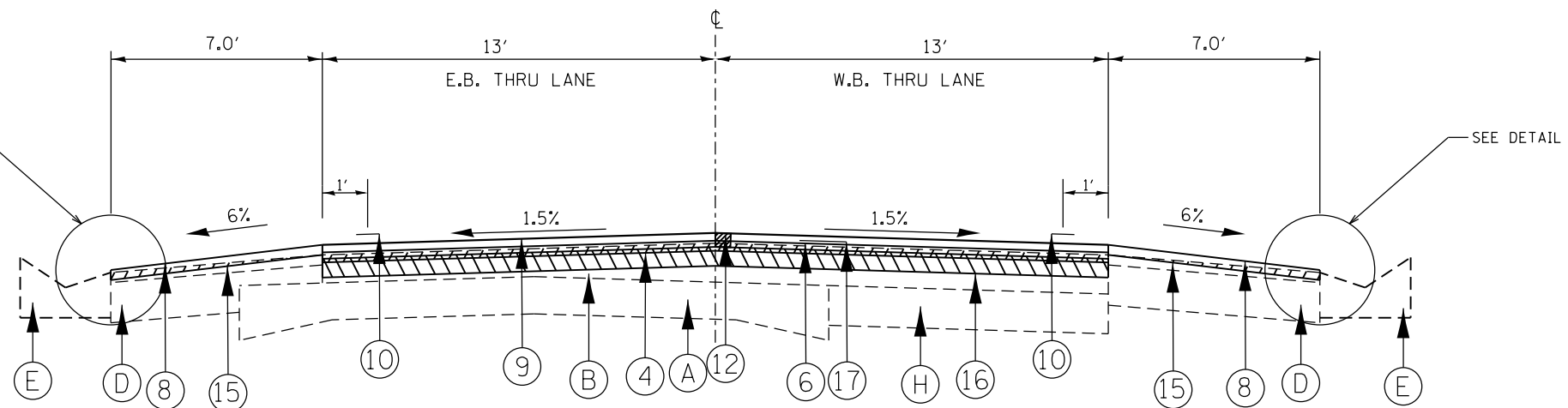
LT. & RT. STA. 1659+50 TO STA. 1664+70  
 RT. STA. 1664+70 TO LT. STA. 1665+63  
 RT. STA. 1670+25 TO RT. STA. 1671+00  
 LT. & RT. STA. 1671+00 TO STA. 1675+26

### LEGEND - EXISTING

- Ⓐ EXIT. PCC PAVEMENT
- Ⓑ EXIT. HMA OVERLAY
- Ⓒ EXIT. AGGREGATE SHOULDERS
- Ⓓ EXIT. HMA SHOULDER
- Ⓔ EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- Ⓕ EXIT. STEEL PLATE BEAM GUARD RAIL
- Ⓖ EXIT. SIDE WALK
- Ⓗ EXIT. BITUMINOUS WIDENING
- ① EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- Ⓝ EXISTING PAVEMENT FABRIC

### LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL - 1/2"
- ② PROP. HMA SURFACE REMOVAL - 2 1/4"
- ③ PROP. HMA SURFACE REMOVAL - 1 3/4"
- ④ PROP. HMA SURFACE REMOVAL - 1 1/2"
- ⑤ PROP. HMA SURFACE REMOVAL - 2 1/2"
- ⑥ PROP. LEVELING BINDER - 1"
- ⑦ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROP. HMA SHOULDERS
- ⑨ PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- ⑪ PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- ⑫ PROP. HMA REMOVAL SPECIAL
- ⑬ PROP. GUARDRAIL REPLACEMENT
- ⑭ PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- ⑮ PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑯ PROP. 3" COLD-IN-PLACE RECYCLING
- ⑰ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



## TYPICAL SECTION 18

LT. STA. 1664+70 TO LT. STA. 1665+63  
 LT. & RT. STA. 1665+63 TO STA. 1670+25  
 LT. STA. 1670+25 TO LT. STA. 1671+00

### OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

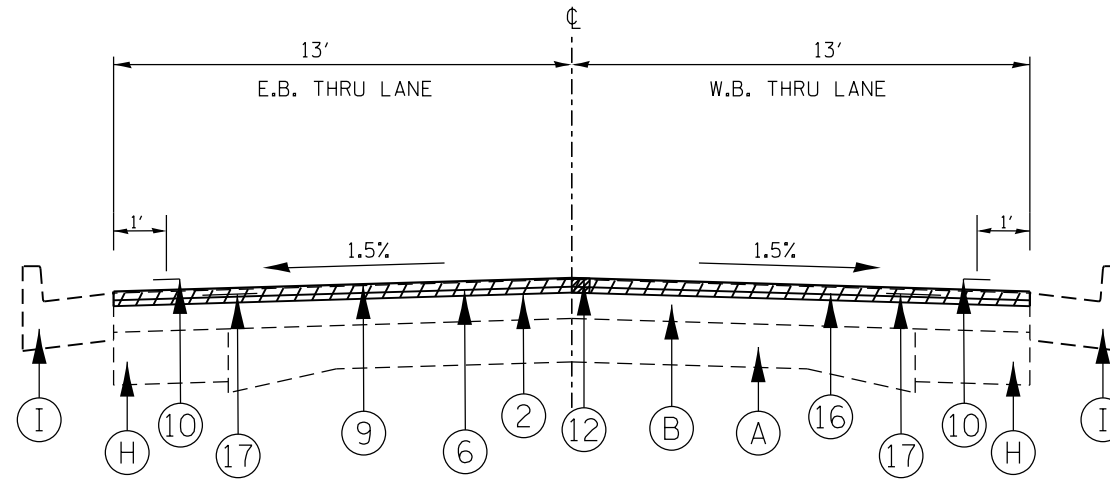
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FILE NAME: STILES

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| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |         |              |              |
|-------------------------------|---------|--------------|--------------|
| <b>TYPICAL SECTIONS US 24</b> |         |              |              |
| SCALE: NO SCALE               | SHEET 9 | OF 12 SHEETS | STA. TO STA. |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 17        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |



**TYPICAL SECTION 19**

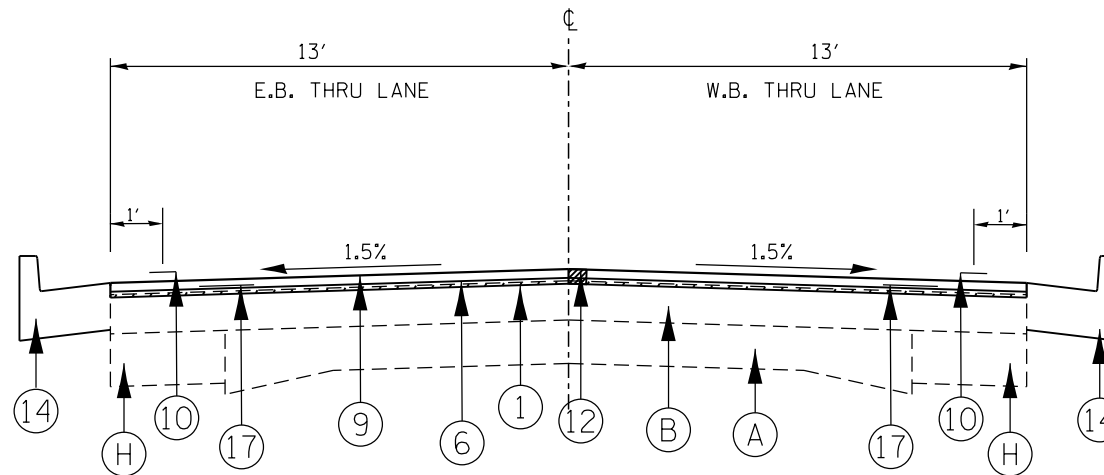
LT. & RT. STA. 1675+26 TO STA. 1696+18

**LEGEND - EXISTING**

- Ⓐ EXIT. PCC PAVEMENT
- Ⓑ EXIT. HMA OVERLAY
- Ⓒ EXIT. AGGREGATE SHOULDERS
- Ⓓ EXIT. HMA SHOULDER
- Ⓔ EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- Ⓕ EXIT. STEEL PLATE BEAM GUARD RAIL
- Ⓖ EXIT. SIDE WALK
- Ⓗ EXIT. BITUMINOUS WIDENING
- Ⓛ EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- Ⓜ EXISTING PAVEMENT FABRIC

**LEGEND - PROPOSED**

- ① PROP. HMA SURFACE REMOVAL - 1/2"
- ② PROP. HMA SURFACE REMOVAL - 2 1/4"
- ③ PROP. HMA SURFACE REMOVAL - 1 3/4"
- ④ PROP. HMA SURFACE REMOVAL - 1 1/2"
- ⑤ PROP. HMA SURFACE REMOVAL - 2 1/2"
- ⑥ PROP. LEVELING BINDER - 1"
- ⑦ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROP. HMA SHOULDERS
- ⑨ PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- ⑪ PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- ⑫ PROP. HMA REMOVAL SPECIAL
- ⑬ PROP. GUARDRAIL REPLACEMENT
- ⑭ PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- ⑮ PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑯ PROP. 3" COLD-IN-PLACE RECYCLING
- ⑰ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



**TYPICAL SECTION 20**

LT. & RT. STA. 1696+31 TO STA. 1701+50

**OMISSIONS**

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: 140DELMAMES  
FILE NAME: 11E15

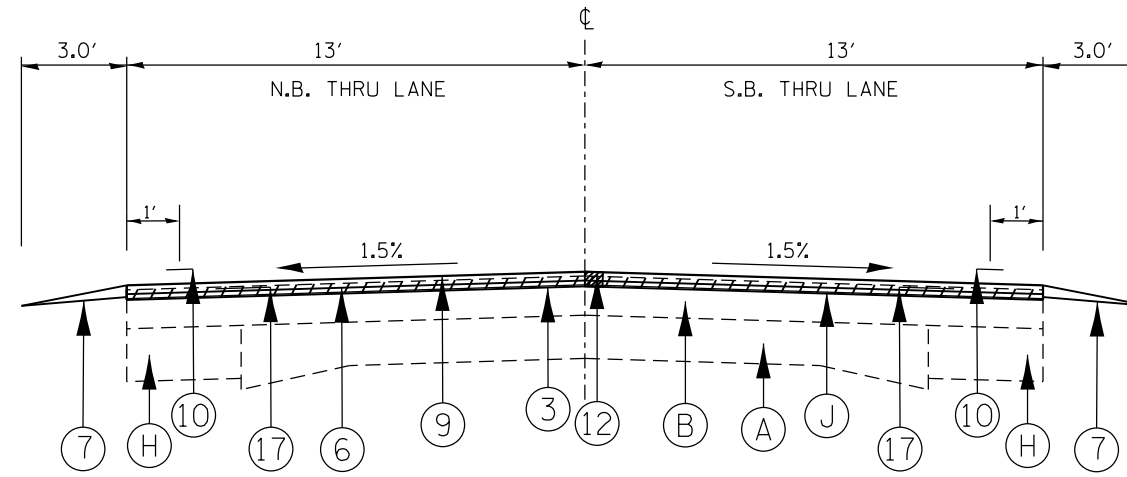
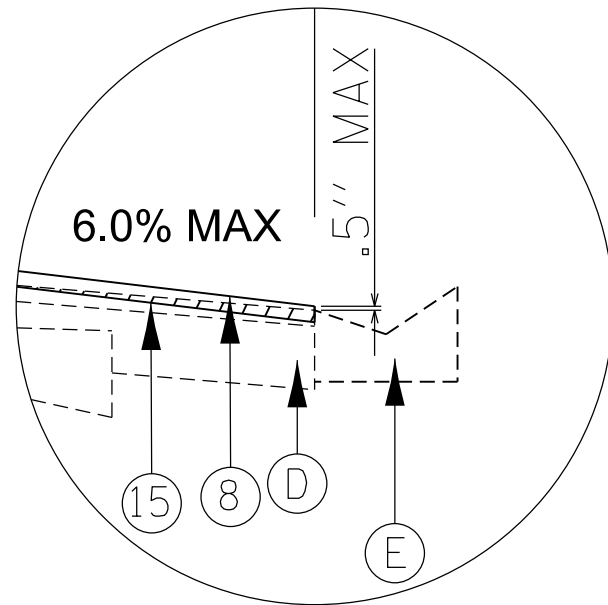
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|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |          |              |              |
|-------------------------------|----------|--------------|--------------|
| <b>TYPICAL SECTIONS US 24</b> |          |              |              |
| SCALE: NO SCALE               | SHEET 10 | OF 12 SHEETS | STA. TO STA. |

| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------|--------|--------------|-----------|
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 18        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |

# Detail A



## TYPICAL SECTION 21

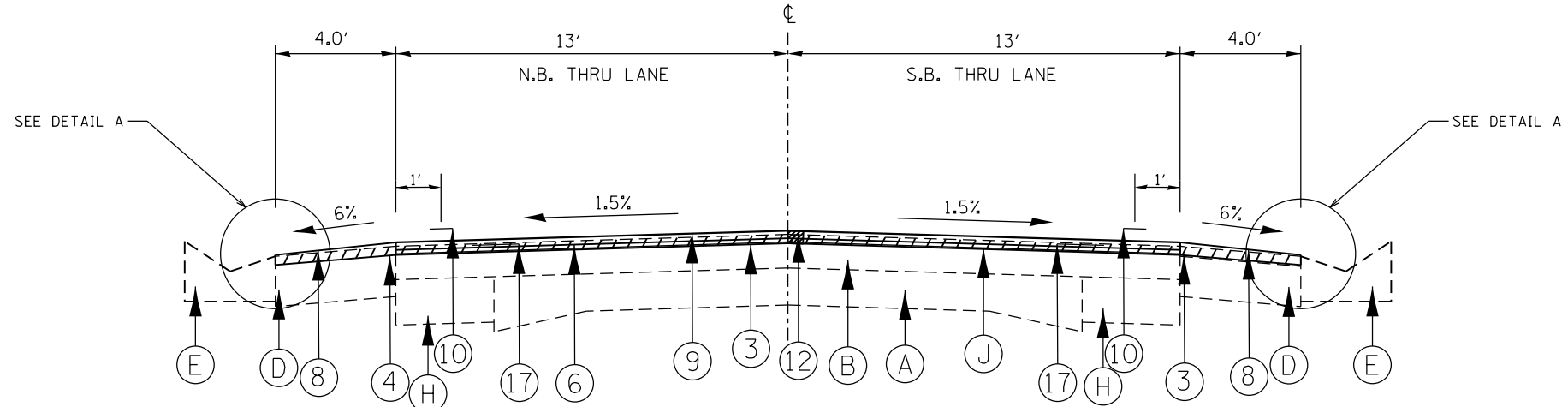
LT. & RT. STA. 187+39 TO STA. 198+15  
 LT. & RT. STA. 201+85 TO STA. 203+55  
 LT. & RT. STA. 210+20 TO STA. 212+55  
 LT. STA. 212+55 TO STA. 217+85  
 LT. & RT. STA. 217+85 TO STA. 227+80  
 LT. & RT. STA. 232+06 TO STA. 245+85  
 RT. STA. 245+85 TO STA. 248+15  
 LT. & RT. STA. 248+15 TO STA. 251+85  
 RT. STA. 251+85 TO STA. 254+15  
 LT. & RT. STA. 254+15 TO STA. 256+80

### LEGEND - EXISTING

- Ⓐ EXIT. PCC PAVEMENT
- Ⓑ EXIT. HMA OVERLAY
- Ⓒ EXIT. AGGREGATE SHOULDERS
- Ⓓ EXIT. HMA SHOULDER
- Ⓔ EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- Ⓕ EXIT. STEEL PLATE BEAM GUARD RAIL
- Ⓖ EXIT. SIDE WALK
- Ⓗ EXIT. BITUMINOUS WIDENING
- Ⓛ EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- Ⓜ EXISTING PAVEMENT FABRIC

### LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL - 1/2"
- ② PROP. HMA SURFACE REMOVAL - 2 1/4"
- ③ PROP. HMA SURFACE REMOVAL - 1 3/4"
- ④ PROP. HMA SURFACE REMOVAL - 1 1/2"
- ⑤ PROP. HMA SURFACE REMOVAL - 2 1/2"
- ⑥ PROP. LEVELING BINDER - 1"
- ⑦ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROP. HMA SHOULDERS
- ⑨ PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- ⑪ PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- ⑫ PROP. HMA REMOVAL SPECIAL
- ⑬ PROP. GUARDRAIL REPLACEMENT
- ⑭ PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- ⑮ PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑯ PROP. 3" COLD-IN-PLACE RECYCLING
- ⑰ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)



## TYPICAL SECTION 22

LT. & RT. STA. 198+15 TO STA. 201+85  
 LT. & RT. STA. 203+55 TO STA. 210+20  
 LT. & RT. STA. 227+80 TO STA. 232+06  
 LT. & RT. STA. 256+80 TO STA. 277+70

### OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: MODELNAME  
FILE: NAME.PLT

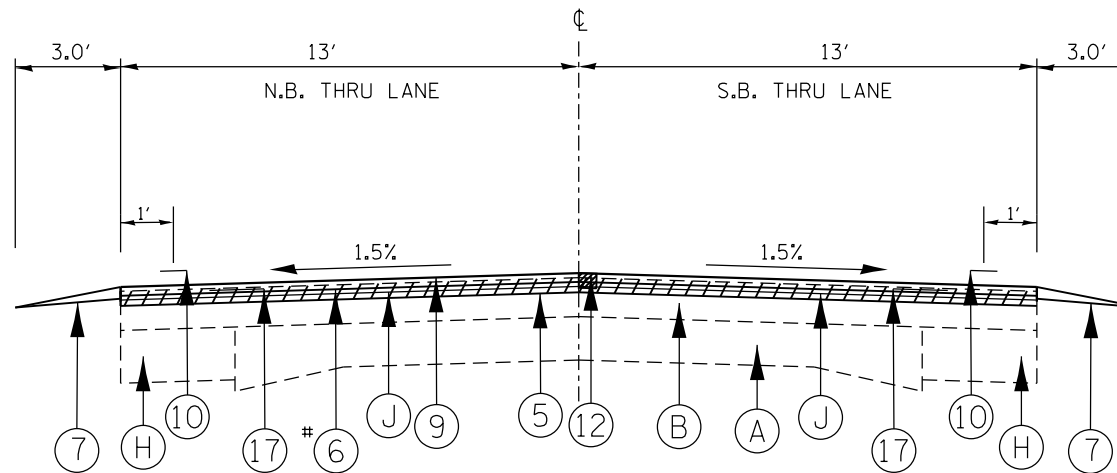
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|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS IL 78 / 97

SCALE: NO SCALE SHEET 11 OF 12 SHEETS STA. TO STA.

|                                |         |        |                           |           |
|--------------------------------|---------|--------|---------------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                        | 19        |
| CONTRACT NO. 68A91             |         |        | ILLINOIS FED. AID PROJECT |           |



TYPICAL SECTION 23

LEGEND - EXISTING

- Ⓐ EXIT. PCC PAVEMENT
- Ⓑ EXIT. HMA OVERLAY
- Ⓒ EXIT. AGGREGATE SHOULDERS
- Ⓓ EXIT. HMA SHOULDER
- Ⓔ EXIT. CONCRETE GUTTER, TYPE B MODIFIED
- Ⓕ EXIT. STEEL PLATE BEAM GUARD RAIL
- Ⓖ EXIT. SIDE WALK
- Ⓗ EXIT. BITUMINOUS WIDENING
- ① EXIT. COMB. CONC. CURB & GUTTER, TY. B 6.24
- Ⓙ EXISTING PAVEMENT FABRIC

- # RT. STA. 212+55 TO STA. 217+85
- # LT. STA. 245+85 TO STA. 248+15
- # LT. STA. 251+85 TO STA. 254+15

LEGEND - PROPOSED

- ① PROP. HMA SURFACE REMOVAL - 1/2"
- ② PROP. HMA SURFACE REMOVAL - 2 1/4"
- ③ PROP. HMA SURFACE REMOVAL - 1 3/4"
- ④ PROP. HMA SURFACE REMOVAL - 1 1/2"
- ⑤ PROP. HMA SURFACE REMOVAL - 2 1/2"
- ⑥ PROP. LEVELING BINDER - 1"
- ⑦ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROP. HMA SHOULDERS
- ⑨ PROP. HMA SURFACE COURSE, MIX "D", N50 - 1 1/2"
- ⑩ PROP. MODIFIED URETHANE PAVEMENT MARKING - LINE 4"
- ⑪ PROP. GUARDRAIL AGGREGATE EROSION CONTROL
- ⑫ PROP. HMA REMOVAL SPECIAL
- ⑬ PROP. GUARDRAIL REPLACEMENT
- ⑭ PROP. COMB. CONC. CURB & GUTTER, TY. B 6.24 REPLACEMENT
- ⑮ PROP. HMA SURFACE REMOVAL, VARIABLE DEPTH
- ⑯ PROP. 3" COLD-IN-PLACE RECYCLING
- ⑰ PROP. STRIP REFLECTIVE CRACK CONTROL TREATMENT (PLACED ON LEVELING BINDER)

#Leveling binder at this location is 1 3/4"

OMISSIONS

STA. 1423+78.38 TO STA. 1425+17.62  
 STA. 1446+15.75 TO STA. 1447+69.25  
 STA. 1696+18 TO STA. 1696+31

MODEL: 140DELEMAMES  
FILE NAME: 311E15

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS IL 78 /97

SCALE: NO SCALE SHEET 12 OF 12 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION                    | COUNTY | TOTAL SHEETS              | SHEET NO. |
|-------------|----------------------------|--------|---------------------------|-----------|
| VAR         | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                        | 20        |
|             |                            |        | CONTRACT NO. 68A91        |           |
|             |                            |        | ILLINOIS FED. AID PROJECT |           |



| SHOULDERS        |                 |         |                    |       |       |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        |                    |       |
|------------------|-----------------|---------|--------------------|-------|-------|---------------------------|-------|----------------------------|-------|-----------------------------|-------|-------------------------------------|-------|--|-------|---------------------------|--------|--------------------|-------|
| Location         |                 | LENGTH  | HMA SHOULDER WIDTH |       | AREA  | HMA SURFACE REMOVAL .5 IN |       | HMA SURFACE REMOVAL 1.5 IN |       | HMA SURFACE REMOVAL 1.75 IN |       | HMA SURFACE REMOVAL, VARIABLE DEPTH |       | POLYMERIZED BITUMINOUS MATERIAL (TACK) |       | HOT MIX ASPHALT SHOULDERS |        | AGG SHOULDER WIDTH |       |
|                  |                 |         | LEFT               | RIGHT |       | LEFT                      | RIGHT | LEFT                       | RIGHT | LEFT                        | RIGHT | LEFT                                | RIGHT | LEFT                                   | RIGHT | LEFT                      | RIGHT  | LEFT               | RIGHT |
| US 24            |                 | FT      | FT                 | FT    | SQ YD | SQ YD                     | SQ YD | SQ YD                      | SQ YD | SQ YD                       | SQ YD | SQ YD                               | SQ YD | POUND                                  | POUND | TON                       | TON    | FT                 | FT    |
| FROM STA         | TO STA          |         |                    |       |       |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        |                    |       |
| 1346+00.00       | 1353+72.00      | 772.00  | 7                  | 2     | 772   | 600                       | 172   |                            |       |                             |       |                                     |       | 432                                    | 124   | 67                        | 19     | 0                  | 3     |
| 1353+72.00       | 1354+77.00      | 105.00  | 7                  | VAR   | 193   | 82                        | 111   |                            |       |                             |       |                                     |       | 59                                     | 80    | 9                         | 12     | 0                  | 0     |
| 1354+77.00       | 1388+70.00      | 3393.00 | 7                  | 7     | 5278  | 2639                      | 2639  |                            |       |                             |       |                                     |       | 1900                                   | 1900  | 296                       | 296    | 0                  | 0     |
| 1388+70.00       | 1392+90.00      | 420.00  | 7                  | 7     | 653   | 327                       |       |                            |       |                             |       |                                     | 327   | 235                                    | 235   | 37                        | 37     | 0                  | 0     |
| 1392+90.00       | 1400+46.00      | 756.00  | 2                  | 7     | 756   | 168                       |       |                            |       |                             |       |                                     | 588   | 121                                    | 423   | 19                        | 66     | 3                  | 0     |
| 1400+46.00       | 1414+94.83 (BK) | 1448.83 | 2                  | 2     | 644   | 322                       | 322   |                            |       |                             |       |                                     |       | 232                                    | 232   | 36                        | 36     | 3                  | 3     |
| 1414+99.36 (AH)  | 1421+00.00      | 603.64  | 2                  | 2     | 268   | 134                       | 134   |                            |       |                             |       |                                     |       | 97                                     | 97    | 15                        | 15     | 3                  | 3     |
| 1421+00.00       | 1422+01.00      | 101.00  | 5                  | 2     | 79    | 56                        | 22    |                            |       |                             |       |                                     |       | 40                                     | 16    | 6                         | 3      | 0                  | 3     |
| 1422+01.00       | 1423+78.38      | 177.38  | 5                  | 5     | 197   | 99                        | 99    |                            |       |                             |       |                                     |       | 71                                     | 71    | 11                        | 11     | 0                  | 0     |
| BRIDGE OMISSION  |                 |         |                    |       |       |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        |                    |       |
| 1425+17.65       | 1428+31.00      | 313.35  | 5                  | 5     | 348   | 174                       | 174   |                            |       |                             |       |                                     |       | 125                                    | 125   | 19                        | 19     | 0                  | 0     |
| 1428+31.00       | 1444+50.00      | 1619.00 | 2                  | 2     | 720   | 360                       | 360   |                            |       |                             |       |                                     |       | 259                                    | 259   | 40                        | 40     | 3                  | 3     |
| 1444+50.00       | 1446+15.75      | 165.75  | 5                  | 5     | 184   | 92                        | 92    |                            |       |                             |       |                                     |       | 66                                     | 66    | 10                        | 10     | 0                  | 0     |
| BRIDGE OMISSION  |                 |         |                    |       |       |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        |                    |       |
| 1447+69.24       | 1450+08.00      | 238.76  | 5                  | 5     | 265   | 133                       | 133   |                            |       |                             |       |                                     |       | 96                                     | 96    | 15                        | 15     | 0                  | 0     |
| 1450+08.00       | 1454+85.00      | 477.00  | 7                  | 7     | 742   | 371                       | 371   |                            |       |                             |       |                                     |       | 267                                    | 267   | 42                        | 42     | 0                  | 0     |
| 1454+85.00       | 1461+65.00      | 680.00  | 2                  | 2     | 302   | 151                       | 151   |                            |       |                             |       |                                     |       | 109                                    | 109   | 17                        | 17     | 3                  | 3     |
| 1461+65.00       | 1464+80.00      | 315.00  | 2                  | 7     | 315   | 70                        |       |                            |       |                             |       |                                     | 245   | 50                                     | 176   | 8                         | 27     | 3                  | 0     |
| 1464+80.00       | 1475+50.00      | 1070.00 | 7                  | 7     | 1664  |                           |       |                            |       |                             |       | 832                                 | 832   | 599                                    | 599   | 93                        | 93     | 0                  | 0     |
| 1475+50.00       | 1483+65.00      | 815.00  | 2                  | 2     | 362   | 181                       | 181   |                            |       |                             |       |                                     |       | 130                                    | 130   | 20                        | 20     | 3                  | 3     |
| 1483+65.00       | 1483+85.00      | 20.00   | 7                  | 2     | 20    |                           | 4     |                            |       |                             |       |                                     | 16    | 11                                     | 3     | 2                         | 0      | 0                  | 3     |
| 1483+85.00       | 1503+80.00      | 1995.00 | 7                  | 7     | 3103  |                           |       |                            |       |                             |       | 1552                                | 1552  | 1117                                   | 1117  | 174                       | 174    | 0                  | 0     |
| 1503+80.00       | 1503+95.00      | 15.00   | 2                  | 7     | 15    | 3                         |       |                            |       |                             |       |                                     | 12    | 2                                      | 8     | 0                         | 1      | 3                  | 0     |
| 1503+95.00       | 1523+55.00      | 1960.00 | 2                  | 2     | 871   | 436                       | 436   |                            |       |                             |       |                                     |       | 314                                    | 314   | 49                        | 49     | 3                  | 3     |
| 1523+55.00       | 1523+84.61 (BK) | 29.61   | 2                  | 2     | 13    |                           |       | 7                          | 7     |                             |       |                                     |       | 5                                      | 5     | 1                         | 1      | 3                  | 3     |
| 1523+94.39(AH)   | 1555+00.00      | 3105.61 | 2                  | 2     | 1380  |                           |       | 690                        | 690   |                             |       |                                     |       | 497                                    | 497   | 77                        | 77     | 3                  | 3     |
| 1555+00.00       | 1555+55.00      | 55.00   | 2                  | 7     | 55    |                           |       | 12                         |       |                             |       |                                     | 43    | 9                                      | 31    | 1                         | 5      | 3                  | 0     |
| 1555+55.00       | 1560+75.00      | 520.00  | 7                  | 7     | 809   |                           |       |                            |       |                             |       | 404                                 | 404   | 291                                    | 291   | 45                        | 45     | 0                  | 0     |
| 1560+75.00       | 1561+00.00      | 25.00   | 2                  | 7     | 25    |                           |       | 6                          |       |                             |       |                                     | 19    | 4                                      | 14    | 1                         | 2      | 3                  | 0     |
| 1561+00.00       | 1563+25.00      | 225.00  | 2                  | 2     | 100   |                           |       | 50                         | 50    |                             |       |                                     |       | 36                                     | 36    | 6                         | 6      | 3                  | 3     |
| 1563+25.00       | 1565+80.00      | 255.00  | 7                  | 7     | 397   |                           |       |                            |       |                             |       | 198                                 | 198   | 143                                    | 143   | 22                        | 22     | 0                  | 0     |
| 1565+80.00       | 1566+30.00      | 50.00   | 2                  | 7     | 50    |                           |       | 11                         |       |                             |       |                                     | 39    | 8                                      | 28    | 1                         | 4      | 3                  | 0     |
| 1566+30.00       | 1574+25.00      | 795.00  | 2                  | 2     | 353   |                           |       | 177                        | 177   |                             |       |                                     |       | 127                                    | 127   | 20                        | 20     | 3                  | 3     |
| 1574+25.00       | 1575+00.00      | 75.00   | 2                  | 7     | 75    |                           |       | 17                         |       |                             |       |                                     | 58    | 12                                     | 42    | 2                         | 7      | 3                  | 0     |
| 1575+00.00       | 1578+35.00      | 335.00  | 7                  | 7     | 521   |                           |       |                            |       |                             |       | 261                                 | 261   | 188                                    | 188   | 29                        | 29     | 0                  | 0     |
| 1578+35.00       | 1578+90.00      | 55.00   | 2                  | 7     | 55    |                           |       | 12                         |       |                             |       |                                     | 43    | 9                                      | 31    | 1                         | 5      | 3                  | 0     |
| 1578+90.00       | 1612+61.77 (BK) | 3371.77 | 2                  | 2     | 1499  |                           |       | 749                        | 749   |                             |       |                                     |       | 539                                    | 539   | 84                        | 84     | 3                  | 3     |
| 1612+69.41 (AH)  | 1659+50.00      | 4680.59 | 2                  | 2     | 2080  |                           |       | 1040                       | 1040  |                             |       |                                     |       | 749                                    | 749   | 116                       | 116    | 3                  | 3     |
| 1659+50.00       | 1675+26.00      | 1576.00 | 7                  | 7     | 2452  |                           |       |                            |       |                             |       | 1226                                | 1226  | 883                                    | 883   | 137                       | 137    | 0                  | 0     |
| IL 78/IL 97      |                 |         |                    |       |       |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        |                    |       |
| 187+39.00        | 198+15.00       | 1076.00 | 0                  | 0     | 0     |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        | 3                  | 3     |
| 198+15.00        | 201+85.00       | 370.00  | 4                  | 4     | 329   |                           |       |                            |       | 164                         | 164   |                                     |       | 118                                    | 118   | 18                        | 18     | 0                  | 0     |
| 201+85.00        | 203+55.00       | 170.00  | 0                  | 0     | 0     |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        | 3                  | 3     |
| 203+55.00        | 210+20.00       | 665.00  | 4                  | 4     | 591   |                           |       |                            |       | 296                         | 296   |                                     |       | 213                                    | 213   | 33                        | 33     | 0                  | 0     |
| 210+20.00        | 227+80.00       | 1760.00 | 0                  | 0     | 0     |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        | 3                  | 3     |
| 227+80.00        | 232+06.00       | 426.00  | 4                  | 4     | 379   |                           |       |                            |       | 189                         | 189   |                                     |       | 136                                    | 136   | 21                        | 21     | 0                  | 0     |
| 232+06.00        | 256+80.00       | 2474.00 | 0                  | 0     | 0     |                           |       |                            |       |                             |       |                                     |       |  |       |                           |        | 3                  | 3     |
| 256+80.00        | 277+70.00       | 2090.00 | 4                  | 4     | 1858  |                           |       |                            |       | 929                         | 929   |                                     |       | 669                                    | 669   | 104                       | 104    | 0                  | 0     |
| SUBTOTALS:       |                 |         |                    |       |       | 6397                      | 5401  | 2771                       | 2713  | 1578                        | 1578  | 4489                                | 5847  | 10969                                  | 11187 | 1706.3                    | 1740.3 |                    |       |
| US 24 SUBTOTALS: |                 |         |                    |       |       | 11798                     |       | 5483                       |       | 0                           |       | 10335                               |       | 19884                                  |       | 3093                      |        |                    |       |
| IL 78 SUBTOTALS: |                 |         |                    |       |       | 0                         |       | 0                          |       | 3156                        |       | 0                                   |       | 2273                                   |       | 354                       |        |                    |       |
| TOTALS:          |                 |         |                    |       |       | 11798                     |       | 5483                       |       | 3156                        |       | 10335                               |       | 22156                                  |       | 3447                      |        |                    |       |

MODEL: MODELNAME  
FILE: MODEL.DWG

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|        |  |         |      |        |      |         |
|--------|--|---------|------|--------|------|---------|
| SCALE: |  | SHEET 2 | OF 9 | SHEETS | STA. | TO STA. |
|--------|--|---------|------|--------|------|---------|

**SCHEDULE OF  
QUANTITIES**

|                           |                            |         |              |           |
|---------------------------|----------------------------|---------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY  | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | Fullton | 73           | 22        |
| CONTRACT NO. 68A91        |                            |         |              |           |
| ILLINOIS FED. AID PROJECT |                            |         |              |           |

| ENTRANCES       |            |      |          |        |       |       |   |                                 |                            |
|-----------------|------------|------|----------|--------|-------|-------|---|---------------------------------|----------------------------|
| Location        |            | TYPE | MATERIAL | LENGTH | WIDTH | AREA  | POLYMERIZED BITUMINOUS MATERIAL (TACK COAT) | INCIDENTAL HMA SURFACING 2 1/2" | AGG. SURFACE COURSE TYPE B |
| US 24           |            |      |          | FT     | FT    | SQ YD | POUND                                       | TONS                            | TON                        |
| LT              | 1348+68.00 | P.E. | AGG      | 45     | 4     | 20.00 |   |                                 | 2.28                       |
| LT              | 1349+74.00 | P.E. | AGG      | 18     | 4     | 8.00  |   |                                 | 0.91                       |
| LT              | 1350+70.00 | P.E. | HMA      | 22     | 4     | 9.78  | 7.04  | 1.37                            |                            |
| LT              | 1351+74.00 | P.E. | AGG      | 11     | 4     | 4.89  |   |                                 | 0.56                       |
| LT              | 1352+80.00 | P.E. | HMA      | 11     | 4     | 4.89  | 3.52  | 0.68                            |                            |
| LT              | 1354+77.00 | P.E. | AGG      | 10     | 4     | 4.44  |   |                                 | 0.51                       |
| RT              | 1355+55.00 | P.E. | AGG      | 28     | 4     | 12.44 |   |                                 | 1.42                       |
| LT              | 1356+04.00 | P.E. | AGG      | 27     | 4     | 12.00 |   |                                 | 1.37                       |
| RT              | 1356+27.00 | P.E. | AGG      | 23     | 4     | 10.22 |   |                                 | 1.16                       |
| LT              | 1357+30.00 | C.E. | AGG      | 44     | 4     | 19.56 |   |                                 | 2.23                       |
| RT              | 1357+35.00 | P.E. | AGG      | 18     | 4     | 8.00  |   |                                 | 0.91                       |
| RT              | 1357+70.00 | P.E. | AGG      | 18     | 4     | 8.00  |   |                                 | 0.91                       |
| LT              | 1359+19.00 | P.E. | HMA      | 18     | 4     | 8.00  | 5.76  | 1.12                            |                            |
| RT              | 1359+65.00 | P.E. | HMA      | 24     | 4     | 10.67 | 7.68  | 1.49                            |                            |
| LT              | 1359+67.00 | P.E. | HMA      | 24     | 4     | 10.67 | 7.68  | 1.49                            |                            |
| LT              | 1361+39.00 | P.E. | HMA      | 22     | 4     | 9.78  | 7.04  | 1.37                            |                            |
| RT              | 1361+90.00 | P.E. | HMA      | 13     | 4     | 5.78  | 4.16  | 0.81                            |                            |
| LT              | 1362+69.00 | P.E. | AGG      | 12     | 4     | 5.33  |   |                                 | 0.61                       |
| RT              | 1363+32.00 | P.E. | AGG      | 12     | 4     | 5.33  |   |                                 | 0.61                       |
| LT              | 1365+47.00 | P.E. | AGG      | 31     | 4     | 13.78 |   |                                 | 1.57                       |
| RT              | 1365+80.00 | C.E. | AGG      | 24     | 4     | 10.67 |   |                                 | 1.21                       |
| LT              | 1366+78.00 | P.E. | HMA      | 23     | 4     | 10.22 | 7.36  | 1.43                            |                            |
| RT              | 1368+87.00 | P.E. | HMA      | 9      | 4     | 4.00  | 2.88  | 0.56                            |                            |
| RT              | 1371+60.00 | P.E. | HMA      | 14     | 4     | 6.22  | 4.48  | 0.87                            |                            |
| LT              | 1374+51.00 | P.E. | AGG      | 12     | 4     | 5.33  |   |                                 | 0.61                       |
| RT              | 1374+61.00 | P.E. | HMA      | 11     | 4     | 4.89  | 3.52  | 0.68                            |                            |
| RT              | 1374+89.00 | P.E. | AGG      | 10     | 4     | 4.44  |   |                                 | 0.51                       |
| RT              | 1376+50.00 | P.E. | AGG      | 12     | 4     | 5.33  |   |                                 | 0.61                       |
| RT              | 1377+17.00 | P.E. | AGG      | 13     | 4     | 5.78  |   |                                 | 0.66                       |
| LT              | 1377+30.00 | P.E. | AGG      | 12     | 4     | 5.33  |   |                                 | 0.61                       |
| LT              | 1379+47.00 | P.E. | HMA      | 12     | 4     | 5.33  | 3.84  | 0.75                            |                            |
| RT              | 1385+32.00 | P.E. | AGG      | 24     | 4     | 10.67 |   |                                 | 1.21                       |
| LT              | 1394+58.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44                                       | 5.72                            |                            |
| RT              | 1394+58.00 | P.E. | HMA      | 24     | 4     | 10.67 | 7.68  | 1.49                            |                            |
| RT              | 1407+29.00 | C.E. | AGG      | 35     | 4     | 15.56 |   |                                 | 1.77                       |
| RT              | 1410+62.00 | C.E. | AGG      | 35     | 4     | 15.56 |   |                                 | 1.77                       |
| LT              | 1415+18.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44                                       | 5.72                            |                            |
| RT              | 1415+18.00 | P.E. | HMA      | 24     | 4     | 10.67 | 7.68  | 1.49                            |                            |
| LT              | 1421+25.00 | P.E. | AGG      | 24     | 4     | 10.67 |   |                                 | 1.21                       |
| RT              | 1430+51.00 | C.E. | HMA      | 35     | 4     | 15.56 | 11.2  | 2.18                            |                            |
| LT              | 1432+04.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44                                       | 5.72                            |                            |
| RT              | 1432+04.00 | C.E. | HMA      | 35     | 4     | 15.56 | 11.2  | 2.18                            |                            |
| RT              | 1441+39.00 | F.E. | HMA      | 72     | 4     | 32.00 | 23.04                                       | 4.48                            |                            |
| US 24 SUBTOTAL: |            |      |          |        |       |       | 214.1                                       | 41.6                            | 25.2                       |

MODEL: \\MODELS\MAMES  
FILE NAME: 31123

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF  
QUANTITIES**

SCALE: SHEET 3 OF 9 SHEETS STA. TO STA.

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | Fulton | 73           | 23        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |

| ENTRANCES CONT. |            |      |          |        |       |       |  |                             |                            |
|-----------------|------------|------|----------|--------|-------|-------|--|-----------------------------|----------------------------|
| Location        |            | TYPE | MATERIAL | LENGTH | WIDTH | AREA  | POLYMERIZED BITUMINOUS MATERIAL (TACK COAT)        | INCIDENTAL HMA SURFACING 2" | AGG. SURFACE COURSE TYPE B |
| US 24           |            |      |          | FT     | FT    | SQ YD | POUND  | TONS                        | TON                        |
| LT              | 1441+67.00 | F.E. | AGG      | 58     | 4     | 25.78 |  |                             | 2.94                       |
| RT              | 1444+86.00 | P.E. | HMA      | 24     | 4     | 10.67 | 7.68   | 1.49                        |                            |
| RT              | 1477+05.00 | P.E. | AGG      | 20     | 4     | 8.89  |  |                             | 1.01                       |
| LT              | 1477+05.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| LT              | 1480+73.00 | C.E. | HMA      | 35     | 4     | 15.56 | 11.2   | 2.18                        |                            |
| LT              | 1481+20.00 | C.E. | HMA      | 35     | 4     | 15.56 | 11.2   | 2.18                        |                            |
| LT              | 1505+52.00 | C.E. | AGG      | 35     | 4     | 15.56 |  |                             | 1.77                       |
| LT              | 1506+78.00 | C.E. | AGG      | 35     | 4     | 15.56 |  |                             | 1.77                       |
| LT              | 1507+99.00 | C.E. | AGG      | 35     | 4     | 15.56 |  |                             | 1.77                       |
| LT              | 1554+95.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| LT              | 1555+25.00 | P.E. | AGG      | 12     | 4     | 5.33  | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| LT              | 1567+31.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| LT              | 1567+34.00 | P.E. | AGG      | 12     | 4     | 5.33  | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| LT              | 1569+75.00 | P.E. | AGG      | 12     | 4     | 5.33  | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| LT              | 1569+80.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| LT              | 1571+83.00 | C.E. | HMA      | 35     | 4     | 15.56 | 11.2   | 2.18                        |                            |
| RT              | 1572+05.00 | F.E. | AGG      | 35     | 4     | 15.56 |  |                             | 1.77                       |
| LT              | 1582+86.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT              | 1582+86.00 | P.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| RT              | 1584+94.00 | F.E. | AGG      | 20     | 4     | 8.89  |  |                             | 1.01                       |
| LT              | 1599+85.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT              | 1599+85.00 | P.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| LT              | 1607+22.00 | P.E. | AGG      | 20     | 4     | 8.89  | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| LT              | 1607+25.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT              | 1634+41.00 | P.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| RT              | 1637+19.00 | C.E. | HMA      | 34     | 4     | 15.11 | 10.88  | 2.12                        |                            |
| LT              | 1637+76.00 | M.B. | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT              | 1637+76.00 | P.E. | HMA      | 13     | 4     | 5.78  | 4.16   | 0.81                        |                            |
| RT              | 1640+13.00 | C.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| RT              | 1644+23.00 | F.E. | AGG      | 15     | 4     | 6.67  |  |                             | 0.76                       |
| RT              | 1654+47.00 | P.E. | AGG      | 15     | 4     | 6.67  |  |                             | 0.76                       |
| RT              | 1662+80.00 | P.E. | HMA      | 12     | 4     | 5.33  | 3.84   | 0.75                        |                            |
| RT              | 1663+97.00 | P.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| RT              | 1665+37.00 | C.E. | AGG      | 35     | 4     | 15.56 |  |                             | 1.77                       |
| LT              | 1672+30.00 | P.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| RT              | 1673+50.00 | P.E. | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| US 24 SUBTOTAL: |            |      |          |        |       |       | 295.7  | 57.5                        | 19.6                       |

MODEL: \\MODELS\FILES  
FILE NAME: 68A91

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF  
QUANTITIES**

SCALE: SHEET 4 OF 9 SHEETS STA. TO STA.

|             |                            |          |                    |           |
|-------------|----------------------------|----------|--------------------|-----------|
| F.A.P. RTE. | SECTION                    | COUNTY   | TOTAL SHEETS       | SHEET NO. |
| VAR         | (137, 129)RS-4, (137X)RS-4 | Fulton   | 73                 | 24        |
|             |                            |          | CONTRACT NO. 68A91 |           |
|             |                            | ILLINOIS | FED. AID PROJECT   |           |



| ENTRANCES CONT.  |            |                      |          |        |       |       |  |                             |                            |
|------------------|------------|----------------------|----------|--------|-------|-------|--|-----------------------------|----------------------------|
| Location         |            | TYPE                 | MATERIAL | LENGTH | WIDTH | AREA  | POLYMERIZED BITUMINOUS MATERIAL (TACK COAT)        | INCIDENTAL HMA SURFACING 2" | AGG. SURFACE COURSE TYPE B |
| IL 78/IL 96      |            |                      |          | FT     | FT    | SQ YD | POUND  | TONS                        | TON                        |
| LT               | 190+40.00  | P.E.                 | AGG      | 28     | 4     | 12.44 |  |                             | 1.42                       |
| LT               | 191+74.00  | P.E.                 | AGG      | 36     | 4     | 16.00 |  |                             | 1.82                       |
| LT               | 195+88.00  | M.B.                 | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT               | 195+88.00  | P.E.                 | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| LT               | 197+08.00  | F.E.                 | HMA      | 18     | 4     | 8.00  | 5.76   | 1.12                        |                            |
| LT               | 198+65.00  | M.B.                 | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| LT               | 198+68.00  | P.E.                 | AGG      | 24     | 4     | 10.67 | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| RT               | 227+28.00  | F.E.                 | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| LT               | 235+50.00  | F.E.                 | AGG      | 22     | 4     | 9.78  |  |                             | 1.11                       |
| RT               | 240+10.00  | P.E.                 | HMA      | 31     | 4     | 13.78 | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| RT               | 240+24.00  | M.B.                 | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT               | 241+60.00  | M.B.                 | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT               | 242+53.00  | P.E.                 | HMA      | 23     | 4     | 10.22 | 7.36   | 1.43                        |                            |
| RT               | 245+82.00  | P.E.                 | HMA      | 29     | 4     | 12.89 | 9.28   | 1.80                        |                            |
| RT               | 246+55.00  | M.B.                 | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| LT               | 246+71.00  | P.E.                 | AGG      | 14     | 4     | 6.22  |  |                             | 0.71                       |
| LT               | 252+37.00  | F.E.                 | AGG      | 20     | 4     | 8.89  |  |                             | 1.01                       |
| RT               | 252+56.00  | M.B.                 | HMA      | 72     | 8     | 40.89 | 29.44  | 5.72                        |                            |
| RT               | 252+66.00  | P.E.                 | HMA      | 20     | 4     | 8.89  | ENTRANCE QUANTITY INCLUDED IN MAILBOX TURNOUT CALC |                             |                            |
| LT               | 252+82.00  | P.E.                 | AGG      | 12     | 4     | 5.33  |  |                             | 0.61                       |
| RT               | 253+35.00  | P.E.                 | HMA      | 20     | 4     | 8.89  | 6.4  | 1.24                        |                            |
| SIDE ROADS       |            |                      |          |        |       |       |  |                             |                            |
| US 24            |            |                      |          |        |       |       |  |                             |                            |
| RT               | 1378+75.00 | N. Prichard Rd       | HMA      | 31     | 4     | 13.78 | 9.92   | 1.93                        |                            |
| LT               | 1407+37.00 | E. Little America Ln | AGG      | 30     | 4     | 13.33 |  |                             | 1.52                       |
| LT               | 1489+00.00 | N. Clark Rd          | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| RT               | 1489+00.00 | N. Clark Rd          | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| RT               | 1516+02.00 | N. Bethel Rd         | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| LT               | 1547+09.00 | N. Dixie Rd          | AGG      | 32     | 4     | 14.22 |  |                             | 1.62                       |
| LT               | 1591+43.00 | N. Berry Rd          | AGG      | 24     | 4     | 10.67 |  |                             | 1.21                       |
| LT               | 1617+62.00 | N. Dickson Mounds Rd | HMA      | 35     | 4     | 15.56 | 11.2   | 2.18                        |                            |
| RT               | 1617+62.00 | N. Dickson Mounds Rd | HMA      | 35     | 4     | 15.56 | 11.2   | 2.18                        |                            |
| LT               | 1658+58.00 | Red Maple            | HMA      | 35     | 4     | 15.56 | 11.2   | 2.18                        |                            |
| RT               | 1676+77.00 | E Euclid Ave         | HMA      | 28     | 4     | 12.44 | 8.96   | 1.74                        |                            |
| LT               | 1686+86.00 | Chestnut St          | HMA      | 28     | 4     | 12.44 | 8.96   | 1.74                        |                            |
| RT               | 1698+97.00 | S. Illinois St       | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| IL 78/IL 97      |            |                      |          |        |       |       |  |                             |                            |
| RT               | 212+98.00  | E. Dixie Rd.         | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| LT               | 212+98.00  | E. Dixie Rd.         | HMA      | 30     | 4     | 13.33 | 9.6  | 1.87                        |                            |
| US 24 SUBTOTALS: |            |                      |          |        |       |       | 610  | 119                         | 49                         |
| IL 78 SUBTOTALS: |            |                      |          |        |       |       | 234  | 46                          | 7                          |
| TOTALS:          |            |                      |          |        |       |       | 844  | 164                         | 56                         |

MODEL: 140DELMNAMES  
FILE NAME: 311215

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF  
QUANTITIES**

SCALE: SHEET 5 OF 9 SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------|--------|--------------|-----------|
| VAR                       | (137, 129)RS-4, (137X)RS-4 | Fulton | 73           | 25        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |

| PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS |                 |         |                          |        |                |         |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         |               |                 |               |     |  |    |  |     |     |
|---|-----------------|---------|--------------------------|--------|----------------|---------|--------------------|--------|--------------------------|-------|--------------------------|-------|---------------------------|-----|---------------------------|--------------------------------------|------------------------------------|---------|---------------|-----------------|---------------|-----|--|----|--|-----|-----|
| LOCATION  |                 |         | PAVEMENT MARKING LINE 4" |        |                |         |                    |        | PAVEMENT MARKING LINE 6" |       | PAVEMENT MARKING LINE 8" |       | PAVEMENT MARKING LINE 12" |     | PAVEMENT MARKING LINE 24" | PAVEMENT MARKING LETTERS AND SYMBOLS | RAISED REFLECTIVE PAVEMENT MARKERS |         |               |                 |               |     |  |    |  |     |     |
|   |                 |         | SOLID - WHITE            |        | SOLID - YELLOW |         | SKIP DASH - YELLOW |        | SOLID WHITE              |       | SOLID WHITE              |       | SOLID - WHITE             |     | DIAGONAL YELLOW           | WHITE                                | WHITE                              | REMOVAL | ONE-WAY AMBER | ONE-WAY CRYSTAL | TWO-WAY AMBER |     |  |    |  |     |     |
|   |                 |         | EB                       | WB     | EB             | WB      | EB                 | WB     | EB                       | WB    | EB                       | WB    | EB                        | WB  | FT                        | FT                                   | SOFT                               | EACH    | EACH          | EACH            | EACH          |     |  |    |  |     |     |
|   |                 |         | FT                       |        | FT             |         | FT                 |        | FT                       |       | FT                       |       | FT                        |     | FT                        | FT                                   | FT                                 | FT      | FT            | FT              | FT            | FT  |  |    |  |     |     |
| US 24   |                 |         |                          |        |                |         |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         |               |                 |               |     |  |    |  |     |     |
| STA.  | 1346+00.00      | TO STA. | 1362+67.00               | 1,667  | 1,667          | 1667    |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 20            |                 |               | 21  |  |    |  |     |     |
| STA.  | 1362+67.00      | TO STA. | 1373+77.00               | 1,110  | 1,110          |         |                    |        |                          | 278   |                          |       |                           |     |                           |                                      |                                    |         | 14            |                 |               | 14  |  |    |  |     |     |
| STA.  | 1373+77.00      | TO STA. | 1378+38.00               | 461    | 461            |         | 461                | 115    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 6             |                 |               | 6   |  |    |  |     |     |
| STA.  | 1378+38.00      | TO STA. | 1386+19.00               | 781    | 781            |         | 1,562              |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 10            |                 |               | 10  |  |    |  |     |     |
| STA.  | 1386+19.00      | TO STA. | 1397+07.00               | 1,088  | 1,088          | 1088    |                    |        |                          | 272   |                          |       |                           |     |                           |                                      |                                    |         | 13            |                 |               | 14  |  |    |  |     |     |
| STA.  | 1397+07.00      | TO STA. | 1414+94.83 (BK)          | 1,788  | 1,788          |         |                    |        |                          | 447   |                          |       |                           |     |                           |                                      |                                    |         | 21            |                 |               | 23  |  |    |  |     |     |
| STA.  | 1414+99.36 (AH) | TO STA. | 1423+78.38               | 879    | 879            |         |                    |        |                          | 220   |                          |       |                           |     |                           |                                      |                                    |         | 10            |                 |               | 11  |  |    |  |     |     |
| BRIDGE OMISSION   |                 |         |                          |        |                |         |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         |               |                 |               |     |  |    |  |     |     |
| STA.  | 1425+17.62      | TO STA. | 1446+15.75               | 2,098  | 2,098          |         |                    |        |                          | 525   |                          |       |                           |     |                           |                                      |                                    |         | 26            |                 |               | 27  |  |    |  |     |     |
| BRIDGE OMISSION   |                 |         |                          |        |                |         |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         |               |                 |               |     |  |    |  |     |     |
| STA.  | 1447+69.25      | TO STA. | 1458+90.00               | 1,121  | 1,121          |         | 172                |        |                          | 280   |                          |       |                           |     | 44                        |                                      |                                    |         | 14            |                 |               | 15  |  |    |  |     |     |
| STA.  | 1458+90.00      | TO STA. | 1471+30.00               | 1,240  | 1,240          |         | 1,240              | 310    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 16            |                 |               | 16  |  |    |  |     |     |
| STA.  | 1471+30.00      | TO STA. | 1490+90.00               | 1,960  | 1,960          |         | 3,920              |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 24            |                 |               | 25  |  |    |  |     |     |
| STA.  | 1490+90.00      | TO STA. | 1505+00.00               | 1,410  | 1,410          |         | 1,410              | 353    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 16            |                 |               | 18  |  |    |  |     |     |
| STA.  | 1505+00.00      | TO STA. | 1523+00.00               | 2127   | 1744           |         | 5,948              |        |                          |       | 753                      | 357   | 158                       |     | 308                       |                                      | 124.8                              |         | 114           | 47              | 27            | 40  |  |    |  |     |     |
| STA.  | 1523+00.00      | TO STA. | 1523+84.61 (BK)          | 84.61  | 84.61          |         | 169                |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 3             |                 |               | 3   |  |    |  |     |     |
| STA.  | 1523+94.39 (AH) | TO STA. | 1524+60.00               | 66     | 66             |         | 131                |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 2             |                 |               | 2   |  |    |  |     |     |
| STA.  | 1524+60.00      | TO STA. | 1529+78.00               | 518    | 518            | 518     |                    |        |                          | 130   |                          |       |                           |     |                           |                                      |                                    |         | 5             |                 |               | 7   |  |    |  |     |     |
| STA.  | 1529+78.00      | TO STA. | 1545+41.00               | 1,563  | 1,563          |         |                    |        |                          | 391   |                          |       |                           |     |                           |                                      |                                    |         | 20            |                 |               | 20  |  |    |  |     |     |
| STA.  | 1545+41.00      | TO STA. | 1554+56.00               | 915    | 915            |         | 915                | 229    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 12            |                 |               | 12  |  |    |  |     |     |
| STA.  | 1554+56.00      | TO STA. | 1555+86.00               | 130    | 130            |         |                    |        |                          | 33    |                          |       |                           |     |                           |                                      |                                    |         | 1             |                 |               | 2   |  |    |  |     |     |
| STA.  | 1555+86.00      | TO STA. | 1563+76.00               | 790    | 790            |         | 790                | 198    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 10            |                 |               | 10  |  |    |  |     |     |
| STA.  | 1563+76.00      | TO STA. | 1612+61.77 (BK)          | 4,886  | 4,886          |         |                    |        |                          | 2,443 |                          |       |                           |     |                           |                                      |                                    |         | 62            |                 |               | 62  |  |    |  |     |     |
| STA.  | 1612+69.41 (AH) | TO STA. | 1646+76.00               | 3,407  | 3,407          |         |                    |        |                          | 1,703 |                          |       |                           |     |                           |                                      |                                    |         | 42            |                 |               | 43  |  |    |  |     |     |
| STA.  | 1646+76.00      | TO STA. | 1650+80.00               | 404    | 404            | 404     |                    |        |                          | 101   |                          |       |                           |     |                           |                                      |                                    |         | 5             |                 |               | 6   |  |    |  |     |     |
| STA.  | 1650+80.00      | TO STA. | 1696+18.00               | 4,538  | 4,538          |         | 9,076              |        |                          |       |                          |       |                           |     | 36                        |                                      | 61.2                               |         | 58            |                 |               | 57  |  |    |  |     |     |
| RAIL ROAD OMISSION                                      |                 |         |                          |        |                |         |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         |               |                 |               |     |  |    |  |     |     |
| STA.  | 1699+31.00      | TO STA. | 1701+50.00               | 361    | 426            |         | 1,072              |        |                          | 128   |                          | 40    |                           |     | 78                        |                                      | 109                                |         | 19            |                 |               | 19  |  |    |  |     |     |
| IL 78/IL 97   |                 |         |                          |        |                |         |                    |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         |               |                 |               |     |  |    |  |     |     |
| STA.  | 187+39.00       | TO STA. | 189+39.00                | 200    | 200            |         | 400                |        |                          |       |                          |       |                           |     | 31                        |                                      |                                    |         | 3             |                 |               | 3   |  |    |  |     |     |
| STA.  | 189+39.00       | TO STA. | 197+49.00                | 810    | 810            |         |                    |        |                          | 203   |                          |       |                           |     |                           |                                      |                                    |         | 10            |                 |               | 11  |  |    |  |     |     |
| STA.  | 197+49.00       | TO STA. | 208+45.00                | 1,096  | 1,096          |         | 1,096              | 274    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 13            |                 |               | 14  |  |    |  |     |     |
| STA.  | 208+45.00       | TO STA. | 216+84.00                | 839    | 839            |         | 1678               |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 10            |                 |               | 11  |  |    |  |     |     |
| STA.  | 216+84.00       | TO STA. | 228+39.00                | 1,155  | 1,155          |         | 1,155              | 289    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 14            |                 |               | 15  |  |    |  |     |     |
| STA.  | 228+39.00       | TO STA. | 229+29.00                | 90     | 90             |         |                    |        |                          | 23    |                          |       |                           |     |                           |                                      |                                    |         | 1             |                 |               | 2   |  |    |  |     |     |
| STA.  | 229+29.00       | TO STA. | 238+63.00                | 934    | 934            | 934     |                    |        |                          | 234   |                          |       |                           |     |                           |                                      |                                    |         | 12            |                 |               | 12  |  |    |  |     |     |
| STA.  | 238+63.00       | TO STA. | 243+88.00                | 525    | 525            |         |                    |        |                          | 131   |                          |       |                           |     |                           |                                      |                                    |         | 6             |                 |               | 7   |  |    |  |     |     |
| STA.  | 243+88.00       | TO STA. | 254+88.00                | 1100   | 1100           |         |                    |        |                          | 275   |                          |       |                           |     |                           |                                      |                                    |         | 11            |                 |               | 14  |  |    |  |     |     |
| STA.  | 254+88.00       | TO STA. | 259+85.00                | 497    | 497            |         | 497                | 124    |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 5             |                 |               | 7   |  |    |  |     |     |
| STA.  | 259+85.00       | TO STA. | 271+50.00                | 1165   | 1165           |         | 2,330              |        |                          |       |                          |       |                           |     |                           |                                      |                                    |         | 14            |                 |               | 15  |  |    |  |     |     |
| STA.  | 271+50.00       | TO STA. | 277+70.00                | 620    | 620            |         |                    |        |                          | 155   |                          |       |                           |     |                           |                                      |                                    |         | 9             |                 |               | 8   |  |    |  |     |     |
| SUB-TOTAL:  |                 |         |                          | 88,527 |                | 38,633  |                    | 10,148 |                          | 128   |                          | 1,150 |                           | 158 |                           | 386                                  |                                    | 220     |               | 310             |               | 651 |  | 47 |  | 27  | 602 |
| US 24 SUB-TOTAL:  |                 |         |                          | 70,465 |                | 30,543  |                    | 8,442  |                          | 128   |                          | 1,150 |                           | 158 |                           | 386                                  |                                    | 189     |               | 543             |               | 47  |  | 27 |  | 483 |     |
| IL 78 SUB-TOTAL:  |                 |         |                          | 18,062 |                | 8,090   |                    | 1,707  |                          | 0     |                          | 0     |                           | 0   |                           | 0                                    |                                    | 31      |               | 108             |               | 0   |  | 0  |  | 119 |     |
| TOTAL:  |                 |         |                          |        |                | 137,308 |                    |        |                          | 128   |                          | 1,150 |                           | 544 |                           | 220                                  |                                    | 310     |               | 651             |               |     |  |    |  | 676 |     |

MODEL: MODELNAMES  
FILE: MODELNAMES.PLT

|                       |             |           |
|-----------------------|-------------|-----------|
| USER NAME = \$USERS   | DESIGNED -  | REVISED - |
| DRAWN -               | REVISIONS - |           |
| PLOT SCALE = \$SCALES | CHECKED -   | REVISED - |
| PLOT DATE = \$DATES   | DATE -      | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |         |      |        |
|-------------------------------|---------|------|--------|
| <b>SCHEDULE OF QUANTITIES</b> |         |      |        |
| SCALE:                        | SHEET 6 | OF 9 | SHEETS |
| STA.                          | TO STA. |      |        |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | Fulton | 73           | 26        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |

| SHORT TERM PAVEMENT MARKING |                 |         |             |                 |            |                |                |         |        |             |                 |            |                |
|-----------------------------|-----------------|---------|-------------|-----------------|------------|----------------|----------------|---------|--------|-------------|-----------------|------------|----------------|
| CENTER LINE                 |                 |         |             |                 |            |                | Edge Line      |         |        |             |                 |            |                |
| FROM STA                    | TO STA          | LENGTH  | COLD MILLED | LEVELING BINDER | PRIME COAT | SURFACE COURSE | FROM STA       | TO STA  | LENGTH | COLD MILLED | LEVELING BINDER | PRIME COAT | SURFACE COURSE |
| US 24                       |                 |         |             |                 |            |                | US 24          |         |        |             |                 |            |                |
| 1346+00.00                  | 1386+19.00      | 4019    | 365.4       | 365.4           | 365.4      | 365.4          | 1346+00        | 1392+90 | 4690   | 375.2       | 375.2           | 375.2      | 375.2          |
| 1386+19.00                  | 1414+94.83 (BK) | 2875.83 | 261.4       | 261.4           | 261.4      | 261.4          | 1392+90        | 1400+46 | 756    | 30.2        | 30.2            | 30.2       | 30.2           |
| 1414+99.36 (AH)             | 1423+78.23      | 878.87  | 79.9        | 79.9            | 79.9       | 79.9           | 1450+08        | 1454+85 | 477    | 38.2        | 38.2            | 38.2       | 38.2           |
| BRIDGE OMISSION             |                 |         |             |                 |            |                | 1461+65        | 1464+80 | 315    | 12.6        | 12.6            | 12.6       | 12.6           |
| 1425+17.62                  | 1446+15.75      | 2098.13 | 190.7       | 190.7           | 190.7      | 190.7          | 1464+80        | 1475+50 | 1070   | 85.6        | 85.6            | 85.6       | 85.6           |
| BRIDGE OMISSION             |                 |         |             |                 |            |                | 1483+65        | 1483+85 | 20     | 0.8         | 0.8             | 0.8        | 0.8            |
| 1447+69.25                  | 1503+00.00      | 5530.75 | 502.8       | 502.8           | 502.8      | 502.8          | 1483+85        | 1503+80 | 1995   | 159.6       | 159.6           | 159.6      | 159.6          |
| 1503+00.00                  | 1523+04.00      | 2004.00 | 182.2       | 182.2           | 182.2      | 182.2          | 1503+80        | 1503+95 | 15     | 0.6         | 0.6             | 0.6        | 0.6            |
| 1523+04.00                  | 1523+84.61 (BK) | 80.61   | 7.3         | 7.3             | 7.3        | 7.3            | 1555+00        | 1561+00 | 600    | 48.0        | 48.0            | 48.0       | 48.0           |
| 1523+94.39 (AH)             | 1612+61.77 (BK) | 8867.38 | 806.1       | 806.1           | 806.1      | 806.1          | 1563+65        | 1565+80 | 215    | 17.2        | 17.2            | 17.2       | 17.2           |
| 1612+69.41 (AH)             | 1696+18.00      | 8348.59 | 759.0       | 759.0           | 759.0      | 759.0          | 1565+80        | 1566+30 | 50     | 2.0         | 2.0             | 2.0        | 2.0            |
| RAILROAD OMISSION           |                 |         |             |                 |            |                | 1574+25        | 1575+00 | 75     | 3.0         | 3.0             | 3.0        | 3.0            |
| 1696+31.00                  | 1701+50.00      | 519.00  | 47.2        | 47.2            | 47.2       | 47.2           | 1575+00        | 1578+35 | 335    | 26.8        | 26.8            | 26.8       | 26.8           |
| IL 78/IL 97                 |                 |         |             |                 |            |                | IL 78/IL 97    |         |        |             |                 |            |                |
| 186+14.00                   | 277+70.00       | 9156    | 832.4       | 832.4           | 832.4      | 832.4          | 1578+35        | 1578+90 | 55     | 2.2         | 2.2             | 2.2        | 2.2            |
| SUBTOTAL                    |                 |         | 16137.5     |                 |            |                | SUBTOTAL       |         |        | 3704.0      |                 |            |                |
| US 24 SUBTOTAL              |                 |         | 12808.1     |                 |            |                | US 24 SUBTOTAL |         |        | 3704.0      |                 |            |                |
| IL 78 SUBTOTAL              |                 |         | 3329.5      |                 |            |                | IL 78 SUBTOTAL |         |        | 0.0         |                 |            |                |
| TOTAL                       |                 |         | 19842       |                 |            |                | TOTAL          |         |        | 19842       |                 |            |                |

| SHORT TERM PAVEMENT MARKING REMOVAL |            |         |            |                |              |
|-------------------------------------|------------|---------|------------|----------------|--------------|
| LOCATION                            |            |         |            | LENGTH APPLIED | AREA REMOVED |
|                                     |            |         |            | FT             | SQ FT        |
| US 24                               |            |         |            |                |              |
| STA.                                | 1346+00.00 | TO STA. | 1701+50.00 | 4128           | 1,376        |
| IL 78/IL 97                         |            |         |            |                |              |
| STA.                                | 187+39.00  | TO STA. | 270+70.00  | 832            | 277          |
| TOTAL                               |            |         |            |                | 1,653        |

MODEL NUMBER: 11/15/01  
FILE NAME: 11/15/01

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF  
QUANTITIES**

SCALE: SHEET 7 OF 9 SHEETS STA. TO STA.

|                                |         |          |                    |           |
|--------------------------------|---------|----------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY   | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | Fulton   | 73                 | 27        |
|                                |         |          | CONTRACT NO. 68A91 |           |
|                                |         | ILLINOIS | FED. AID PROJECT   |           |

| ADA IMPROVEMENT |                     |  |                        |
|-----------------|---------------------|--|------------------------|
| LOCATION        | SIDEWALK<br>REMOVAL | PORTLAND<br>CEMENT<br>SIDEWALK<br>4 INCH | DETECTABLE<br>WARNINGS |
|                 | AREA                | AREA                                     | AREA                   |
|                 | SQ FT               | SQ FT                                    | SQ FT                  |
| 1               | 108.4               | 111.2                                    | 10                     |
| 2               | 75.0                | 125.0                                    | 10                     |
| 3               | 60.0                | 159.0                                    | 10                     |
| 4               | 75.0                | 125.0                                    | 10                     |
| 5               | 36.4                | 135.0                                    | 10                     |
| 6               | 52.5                | 71.3                                     | 10                     |
| TOTAL           | 407                 | 726                                      | 60                     |

| ISLAND REMOVAL    |          |       |
|-------------------|----------|-------|
| LOCATION          | QUANTITY | UNIT  |
| US 24 STA 1515+80 | 51       | SQ FT |
| TOTAL             | 51       | SQ FT |

| CONCRETE MEDIAN, TYPE M-2.12 |          |       |
|------------------------------|----------|-------|
| LOCATION                     | QUANTITY | UNIT  |
| US 24 STA 1515+80            | 51       | SQ FT |
| TOTAL                        | 51       | SQ FT |

| COMBINATION CURB AND GUTTER REMOVAL |            |            |     |
|-------------------------------------|------------|------------|-----|
| RT                                  | 1696+31.00 | 1698+82.00 | 275 |
| LT                                  | 1697+25.00 | 1701+30.00 | 430 |
| RT                                  | 1699+12.00 | 1701+30.00 | 242 |
| US 24 SUBTOTAL                      |            |            | 947 |
| IL 78 SUBTOTAL                      |            |            | 0   |
| TOTAL                               |            |            | 947 |

| COMBINATION CURB AND GUTTER TYPE B-6.24 |            |            |     |
|---|------------|------------|-----|
| RT                                      | 1696+31.00 | 1698+82.00 | 275 |
| LT                                      | 1697+25.00 | 1701+30.00 | 430 |
| RT                                      | 1699+12.00 | 1701+30.00 | 242 |
| US 24 SUBTOTAL                          |            |            | 947 |
| IL 78 SUBTOTAL                          |            |            | 0   |
| TOTAL                                   |            |            | 947 |

MODEL: \\MODELNAME\$  
FILE NAME: \$FILE\$

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF  
QUANTITIES**

SCALE: SHEET 8 OF 9 SHEETS STA. TO STA.

|                |                            |        |                           |              |
|----------------|----------------------------|--------|---------------------------|--------------|
| F.A.P.<br>RTE. | SECTION                    | COUNTY | TOTAL<br>SHEETS           | SHEET<br>NO. |
| VAR            | (137, 129)RS-4, (137X)RS-4 | Fulton | 73                        | 28           |
|                |                            |        | CONTRACT NO. 68A91        |              |
|                |                            |        | ILLINOIS FED. AID PROJECT |              |

| SCHEDULE OF GUARDRAIL QUANTITIES |            |            |  |   |   |                                   |                                   |                               |                                 |
|----------------------------------|------------|------------|--|---|---|-----------------------------------|-----------------------------------|-------------------------------|---------------------------------|
| SIDE                             | FROM STA   | TO STA     | TRAFFIC BARRIER<br>TERMINAL, TYPE 1<br>(SPECIAL) TANGENT | STEEL PLATE<br>BEAM GUARDRAIL,<br>TYPE A, 6 FOOT<br>POSTS | STEEL PLATE<br>BEAM GUARDRAIL,<br>TYPE A, 9 FOOT<br>POSTS | GUARDRAIL<br>REFLECTORS<br>TYPE A | GUARDRAIL<br>AGGREGATE<br>CONTROL | TERMINAL<br>DIRECT<br>APPLIED | STONE<br>RIPRAP,<br>CLASS<br>4A |
|                                  | FROM STA   | TO STA     | EACH   | FOOT  | FOOT  | EACH                              | TON                               | EACH                          | TON                             |
| LT                               | 1344+00.00 | 1348+50.00 | 2  | 350   |   | 6                                 | 91.6                              | 2                             |                                 |
| LT                               | 1384+92.50 | 1390+80.00 | 2  | 487.5   |   | 8                                 | 119.5                             | 2                             |                                 |
| LT                               | 1451+25.00 | 1454+00.00 | 2  | 175   |   | 4                                 | 56.0                              | 2                             |                                 |
| RT                               | 1451+60.00 | 1454+35.00 | 2  | 175   |   | 4                                 | 56.0                              | 2                             |                                 |
| LT                               | 1494+77.00 | 1499+39.50 | 2  | 362.5   |   | 6                                 | 94.1                              | 2                             |                                 |
| RT                               | 1495+98.00 | 1498+98.00 | 2  | 125.0   | 75.0  | 4                                 | 61.0                              | 2                             | 40                              |
| US 24 SUBTOTAL:                  |            |            | 12   | 1675  | 75  | 32                                | 478.2                             | 12                            | 40                              |
| IL 78 SUBTOTAL:                  |            |            | 0  | 0   | 0   | 0                                 | 0                                 | 0                             | 0                               |
| TOTAL:                           |            |            | 12   | 1675  | 75  | 32                                | 478.2                             | 12                            | 40                              |

| GUARDRAIL REMOVAL |            |            |             |
|-------------------|------------|------------|-------------|
| SIDE              | FROM STA   | TO STA     | LENGTH (FT) |
| LT                | 1344+00.00 | 1348+50.00 | 450.0       |
| LT                | 1384+92.50 | 1390+80.00 | 587.5       |
| LT                | 1451+25.00 | 1454+00.00 | 275.0       |
| RT                | 1451+60.00 | 1454+35.00 | 275.0       |
| LT                | 1494+77.00 | 1499+39.50 | 462.5       |
| RT                | 1495+98.00 | 1498+98.00 | 300.0       |
| US 24 SUBTOTAL:   |            |            | 2350        |
| IL 78 SUBTOTAL:   |            |            | 0           |
| TOTAL:            |            |            | 2350        |

| JOBSITE QUANTITIES                        |        |     |
|---|--------|-----|
| MOBILIZATION                              | L SUM  | 1   |
| CONSTRUCTION STATION LAYOUT               | L SUM  | 1   |
| ENGINEERS FIELD OFFICE, TYPE A            | CAL MO | 7   |
| MOWING                                    | ACRE   | 29  |
| TRAFFIC CONTROL AND PROTECTION STD 701201 | L SUM  | 1   |
| TRAFFIC CONTROL AND PROTECTION STD 701306 | L SUM  | 1   |
| TRAFFIC CONTROL AND PROTECTION STD 701801 | L SUM  | 1   |
| WATER VALVES TO BE ADJUSTED               | EACH   | 3   |
| MANHOLES TO BE ADJUSTED                   | EACH   | 2   |
| INLET TO BE ADJUSTED                      | EACH   | 4   |
| TRAFFIC CONTROL SURVEILLANCE              | CAL DA | 10  |
| CHANGEABLE MESSAGE SIGN                   | CAL DA | 100 |
| RAILROAD PROTECTIVE LIABILITY INSURANCE   | L SUM  | 1   |

| CONCRETE ENTRANCES |            |      |                   |       |                                      |
|--------------------|------------|------|-------------------|-------|--------------------------------------|
| Location           |            | TYPE | AVERAGE<br>LENGTH | WIDTH | PORTLAND CEMENT<br>CONCRETE DRIVEWAY |
| US 24              |            |      | FT                | FT    | SQ YD                                |
| RT                 | 1697+22.00 | C.E. | 30                | 13.2  | 396                                  |
| RT                 | 1697+79.00 | C.E. | 27                | 10.7  | 288.9                                |
| LT                 | 1698+45.00 | C.E. | 25                | 8     | 200                                  |
| TOTAL:             |            |      |                   |       | 884.9                                |

MODEL: \\MODELS\FILES  
FILE NAME: 311215

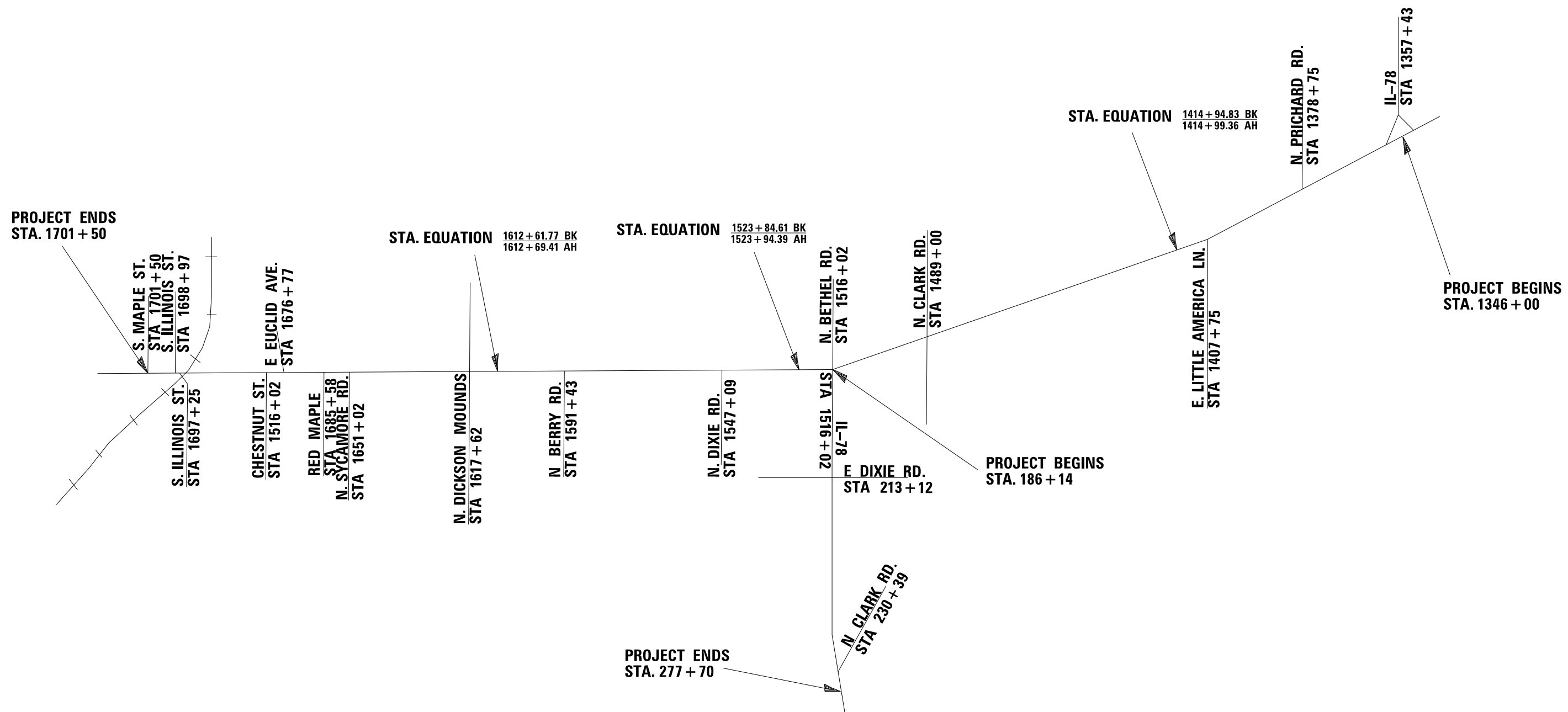
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|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF  
QUANTITIES**

SCALE: SHEET 9 OF 9 SHEETS STA. TO STA.

|                           |                            |        |                 |              |
|---------------------------|----------------------------|--------|-----------------|--------------|
| F.A.P.<br>RTE.            | SECTION                    | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | Fulton | 73              | 29           |
| CONTRACT NO. 68A91        |                            |        |                 |              |
| ILLINOIS FED. AID PROJECT |                            |        |                 |              |



MODEL: \\MODELS\NAME\$  
FILE: \\NAME\$.PTB

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**LINE DIAGRAM**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

|                           |                            |        |                    |           |
|---------------------------|----------------------------|--------|--------------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 30        |
| ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |

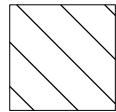
**GENERAL NOTES**

1. WHERE PAVEMENT REMOVAL IS REQUIRED FOR INSTALLATION OF THE SIDEWALK, THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE SIDEWALK TO ALLOW FOR PLACEMENT OF FORMS. SAWCUTTING OF THE PAVEMENT SHALL BE INCLUDED IN THE COST OF PCC SIDEWALK, 4" SIDEWALK REMOVAL.
2. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
3. CURB AND GUTTER REMOVAL LOCATIONS CAN BE FOUND IN THE SCHEDULE OF QUANTITIES.

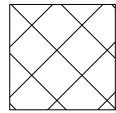
**LOCATIONS #1 - #2 - REMOVAL DETAILS**

**US 24 & MAPLE STREET**

**LEGEND**



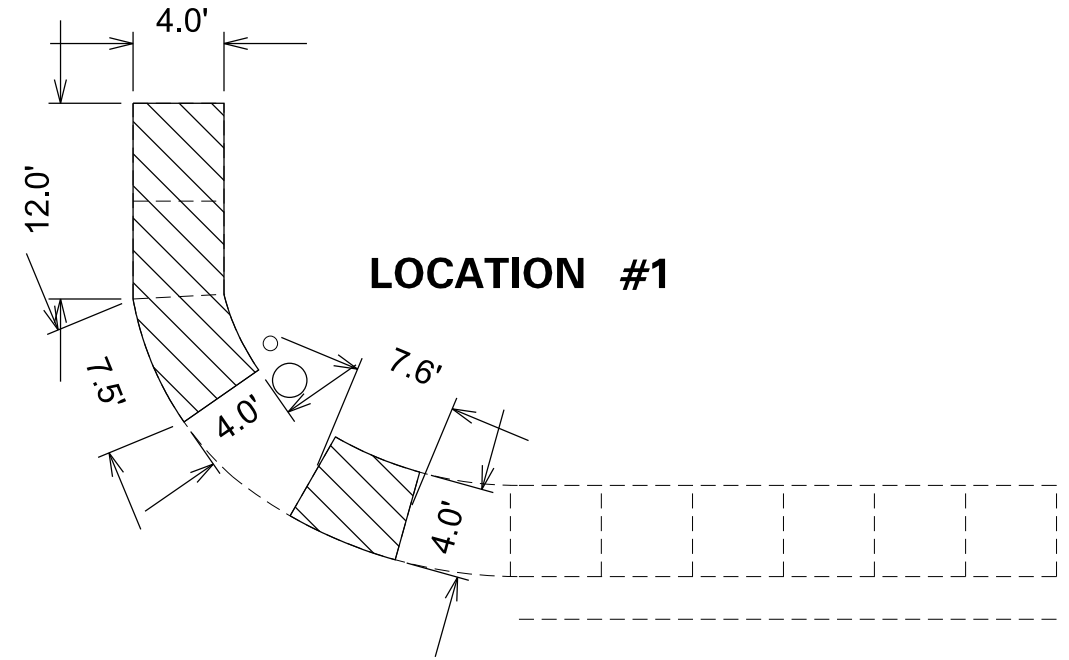
SIDEWALK / SIDE CURB REMOVAL



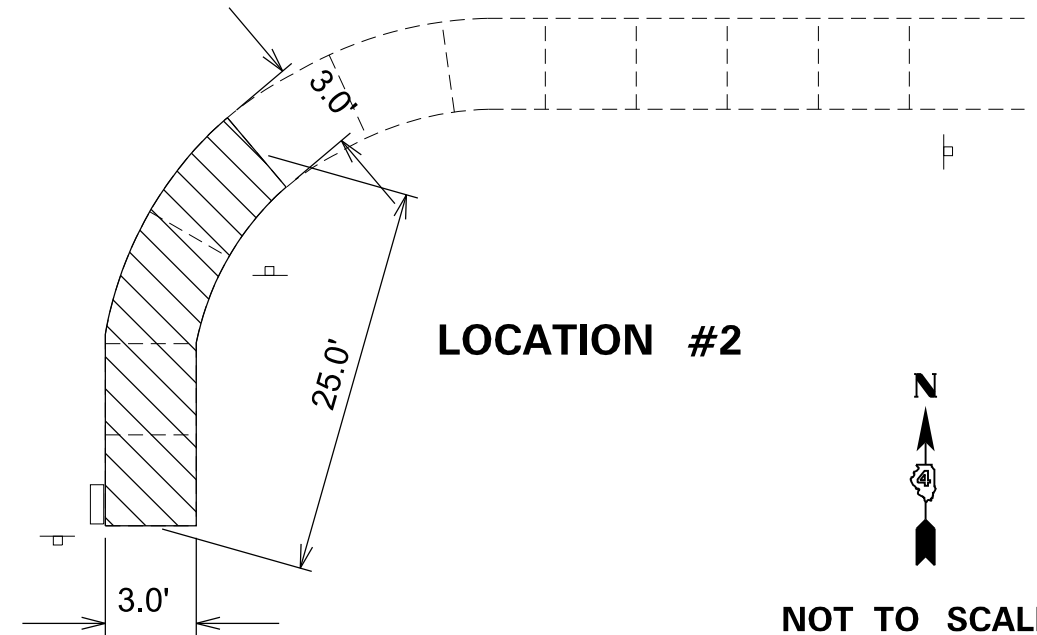
CURB REMOVAL

**US 24**

**MAPLE STREET**



**LOCATION #1**



**LOCATION #2**



**NOT TO SCALE**

MODEL: \\MODELS\BAMES  
FILE NAME: 31115

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

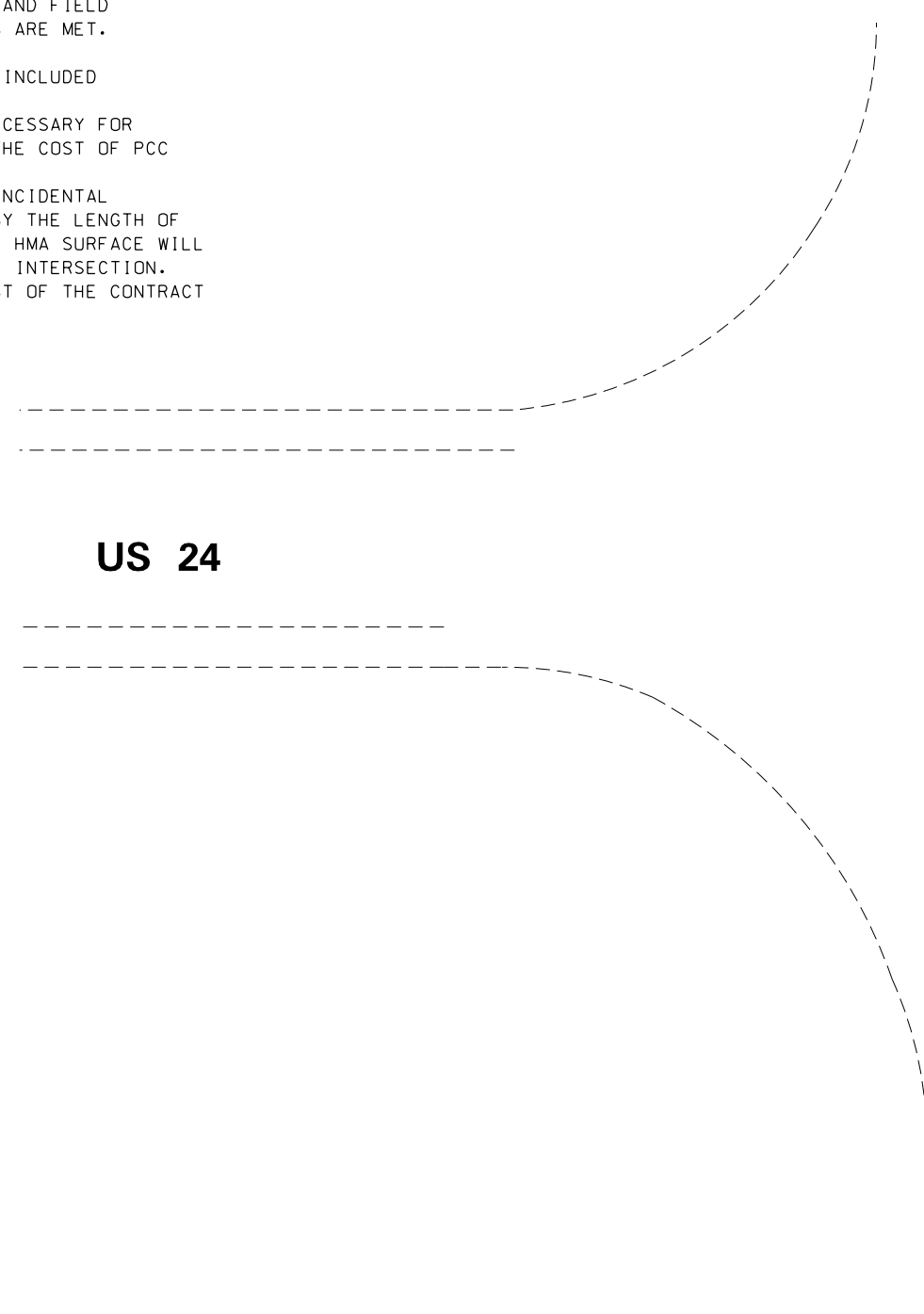
SCALE: SHEET 1 OF 8 SHEETS STA. TO STA.

|                                |         |        |                           |           |
|--------------------------------|---------|--------|---------------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                        | 31        |
| CONTRACT NO. 68A91             |         |        | ILLINOIS FED. AID PROJECT |           |

**GENERAL NOTES**

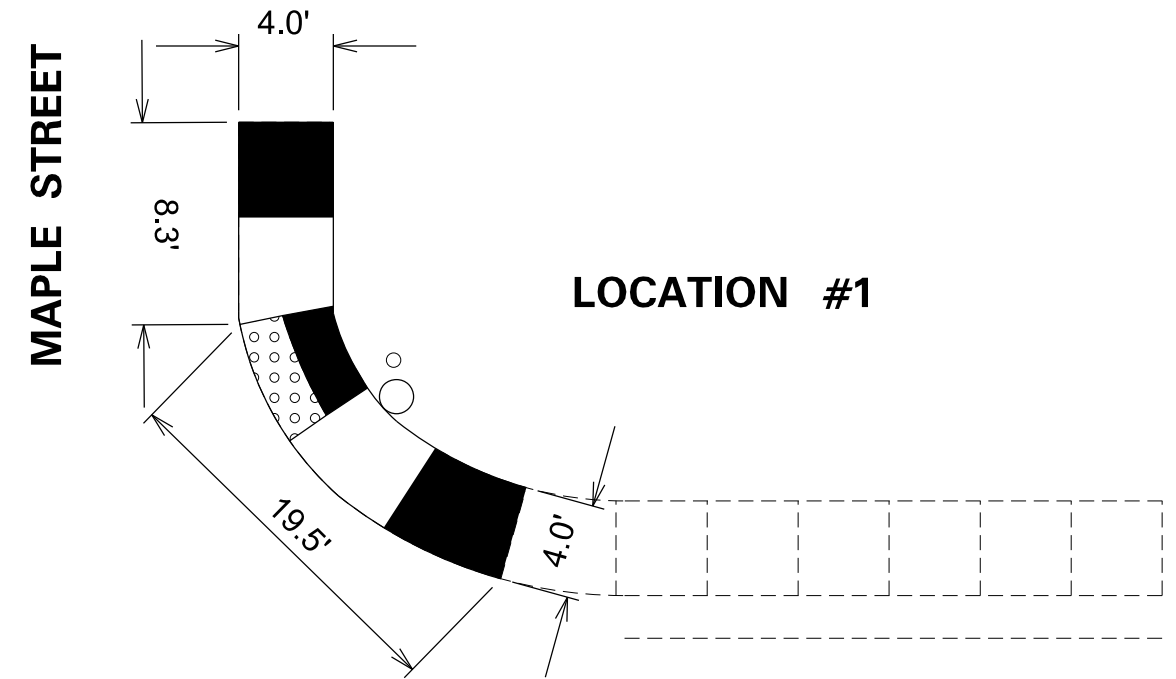
1. ALL SIDEWALK SHOULD FOLLOW STANDARDS 424001-11, 424021-05, 424011-04, AND 424016-05
2. SIDEWALK CROSS SLOPE = 1.5% (2.0% MAX.)
3. UPPER LANDINGS AND LOWER LANDINGS SHALL BE 1.5% MAXIMUM IN ANY DIRECTION; INCLUDING DIAGONALLY. LANDINGS AND TURNING SPACES SHOULD HAVE A MINIMUM DIMENSION OF 4' BY 4', BUT 5' BY 5' IS PREFERRED.
4. MAXIMUM RAMP LONGITUDINAL SLOPE = 8.3% (1:12)
5. MAXIMUM SLOPE AROUND RADIUS = 2%
6. FLARES ARE 1.5' UNLESS OTHERWISE NOTED.
7. CONTRACTOR TO UTILIZE RADIAL DETECTABLE WARNING PANELS AROUND RADIUS. CONTRACTOR IS RESPONSIBLE FOR MEASURING THE RADIUS PRIOR TO ORDERING PANELS.
8. CONTRACTOR SHALL VERIFY REMOVAL LIMITS AND FIELD ELEVATIONS TO ENSURE THAT ADA STANDARDS ARE MET.
9. STREET WIDTHS ARE NOT SHOWN TO SCALE.
10. SAWCUTTING THE SIDEWALK IS CONSIDERED INCLUDED IN THE COST OF SIDEWALK REMOVAL.
11. ANY EXCAVATION OR AGGREGATE SUBBASE NECESSARY FOR THE PROPOSED SIDEWALK IS INCLUDED IN THE COST OF PCC SIDEWALK, 4".
12. UNLESS OTHERWISE NOTED IN THE PLANS, INCIDENTAL HMA SURFACE HAS THE DIMENSIONS OF 1' BY THE LENGTH OF SIDEWALK WITH A DEPTH OF 4". INCIDENTAL HMA SURFACE WILL BE PLACED WHERE THE SIDEWALK MEETS THE INTERSECTION.
13. SIGN RELOCATION IS INCLUDED IN THE COST OF THE CONTRACT PAY ITEMS.

**LOCATION #1**

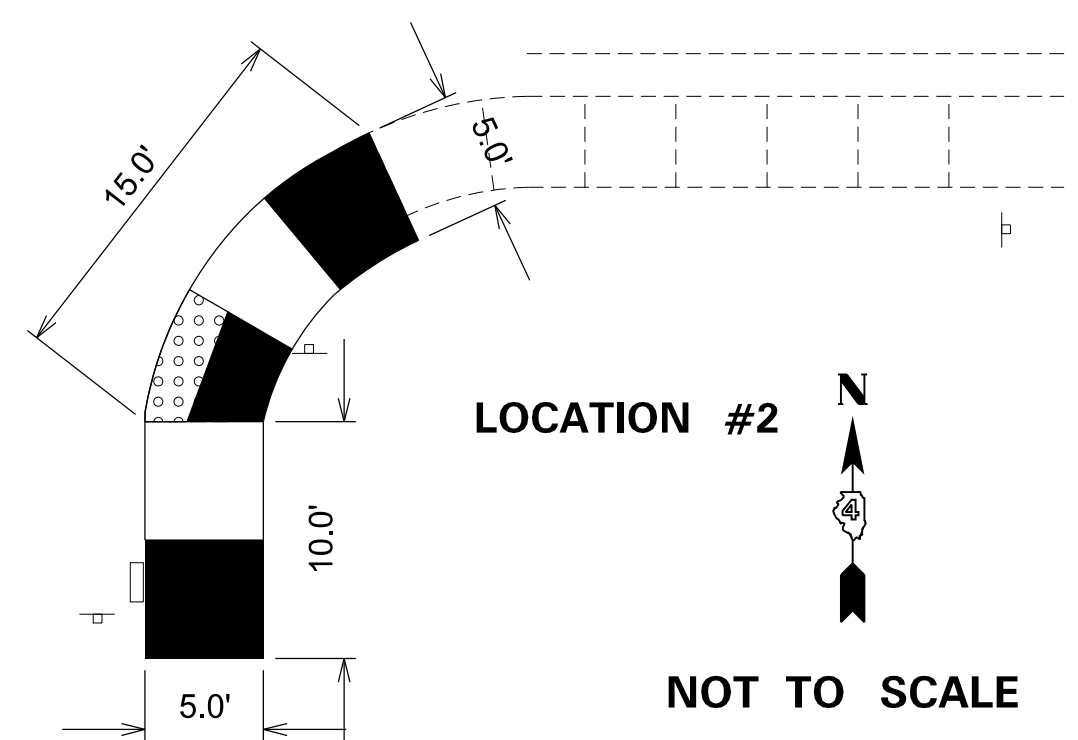


**LOCATIONS #1 - #2 - PROPOSED DETAILS**

**US 24 & MAPLE STREET**



**LOCATION #1**



**LOCATION #2**

**LEGEND**

- EXIST. SIDEWALK / CURB
- PROPOSED ELEMENT
- DETECTABLE WARNINGS
- LANDING AREA / TURNING SPACES



**NOT TO SCALE**

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

SCALE: SHEET 2 OF 8 SHEETS STA. TO STA.

|                    |                            |        |                  |           |
|--------------------|----------------------------|--------|------------------|-----------|
| F.A.P. RTE.        | SECTION                    | COUNTY | TOTAL SHEETS     | SHEET NO. |
| VAR                | (137, 129)RS-4, (137X)RS-4 | FULTON | 73               | 32        |
| ILLINOIS           |                            |        | FED. AID PROJECT |           |
| CONTRACT NO. 68A91 |                            |        |                  |           |

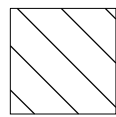


**GENERAL NOTES**

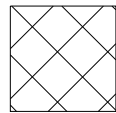
1. WHERE PAVEMENT REMOVAL IS REQUIRED FOR INSTALLATION OF THE SIDEWALK, THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE SIDEWALK TO ALLOW FOR PLACEMENT OF FORMS. SAWCUTTING OF THE PAVEMENT SHALL BE INCLUDED IN THE COST OF PCC SIDEWALK, 4" SIDEWALK REMOVAL.
2. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
3. CURB AND GUTTER REMOVAL LOCATIONS CAN BE FOUND IN THE SCHEDULE OF QUANTITIES.

**LOCATIONS #3 - #4 - REMOVAL DETAILS**  
**US 24 MID-BLOCK CROSSING 1698+00**

**LEGEND**

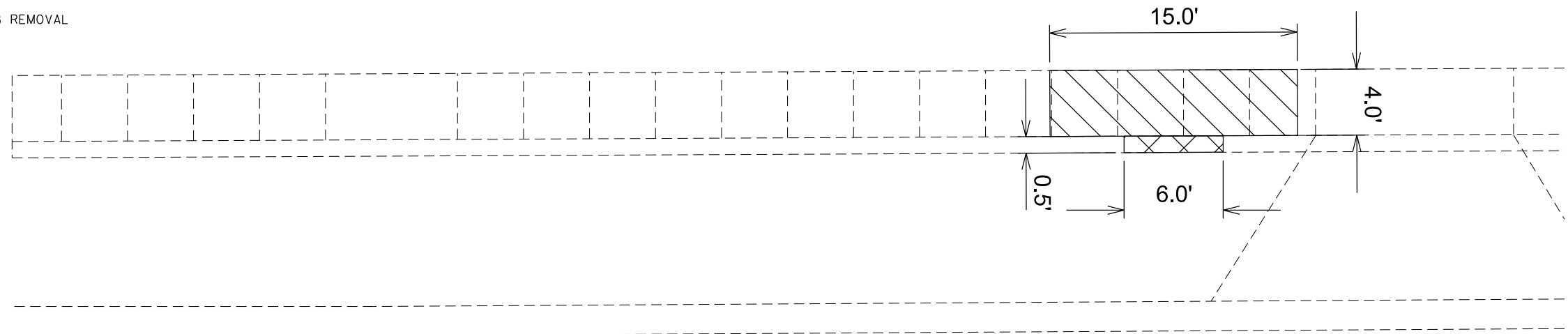


SIDEWALK / SIDE CURB REMOVAL

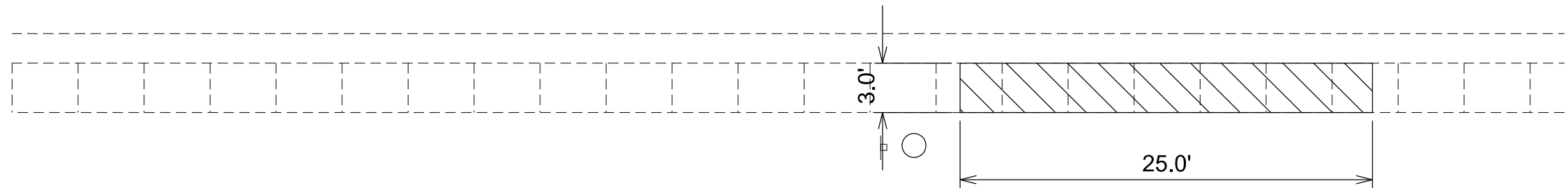


CURB REMOVAL

**LOCATION #3**



**LOCATION #4**



**NOT TO SCALE**

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 FILE NAME: \$FILE\$

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

SCALE: SHEET 3 OF 8 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 33        |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |

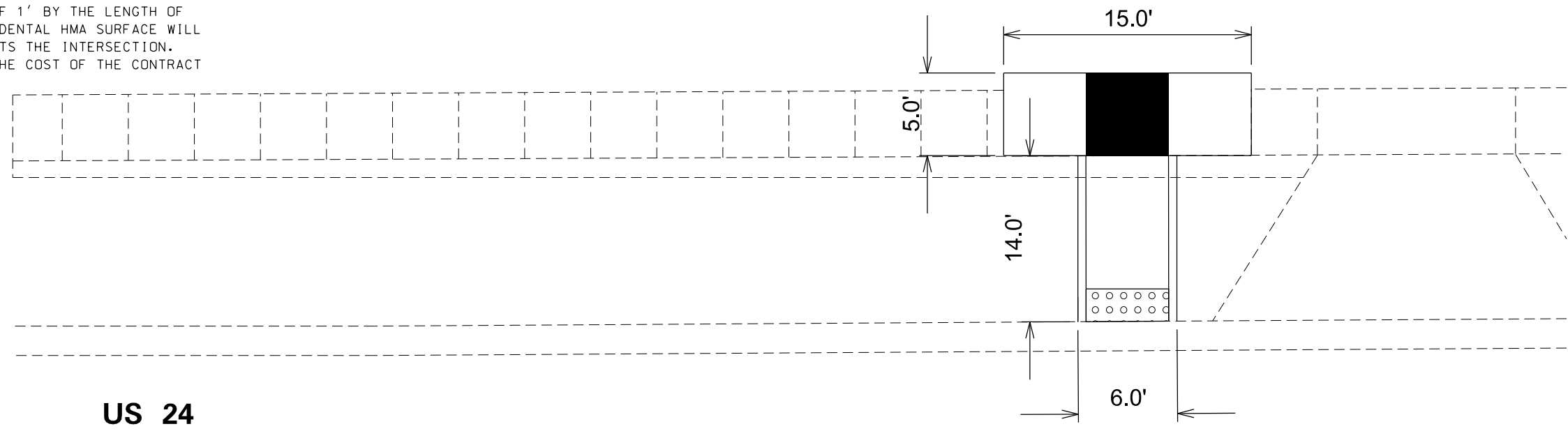
**GENERAL NOTES**

1. ALL SIDEWALK SHOULD FOLLOW STANDARDS 424001-11, 424021-05, 424011-04, AND 424016-05
2. SIDEWALK CROSS SLOPE = 1.5% (2.0% MAX.)
3. UPPER LANDINGS AND LOWER LANDINGS SHALL BE 1.5% MAXIMUM IN ANY DIRECTION; INCLUDING DIAGONALLY. LANDINGS AND TURNING SPACES SHOULD HAVE A MINIMUM DIMENSION OF 4' BY 4', BUT 5' BY 5' IS PREFERRED.
4. MAXIMUM RAMP LONGITUDINAL SLOPE = 8.3% (1:12)
5. MAXIMUM SLOPE AROUND RADIUS = 2%
6. FLARES ARE 1.5' UNLESS OTHERWISE NOTED.
7. CONTRACTOR TO UTILIZE RADIAL DETECTABLE WARNING PANELS AROUND RADIUS. CONTRACTOR IS RESPONSIBLE FOR MEASURING THE RADIUS PRIOR TO ORDERING PANELS.
8. CONTRACTOR SHALL VERIFY REMOVAL LIMITS AND FIELD ELEVATIONS TO ENSURE THAT ADA STANDARDS ARE MET.
9. STREET WIDTHS ARE NOT SHOWN TO SCALE.
10. SAWCUTTING THE SIDEWALK IS CONSIDERED INCLUDED IN THE COST OF SIDEWALK REMOVAL.
11. ANY EXCAVATION OR AGGREGATE SUBBASE NECESSARY FOR THE PROPOSED SIDEWALK IS INCLUDED IN THE COST OF PCC SIDEWALK, 4".
12. UNLESS OTHERWISE NOTED IN THE PLANS, INCIDENTAL HMA SURFACE HAS THE DIMENSIONS OF 1' BY THE LENGTH OF SIDEWALK WITH A DEPTH OF 4". INCIDENTAL HMA SURFACE WILL BE PLACED WHERE THE SIDEWALK MEETS THE INTERSECTION.
13. SIGN RELOCATION IS INCLUDED IN THE COST OF THE CONTRACT PAY ITEMS.

**LOCATIONS #3 - #4 - PROPOSED DETAILS**  
**US 24 MID-BLOCK CROSSING 1698+00**

\* APPROACH NOSE DETAIL MAY BE USED AT LOACTION 3

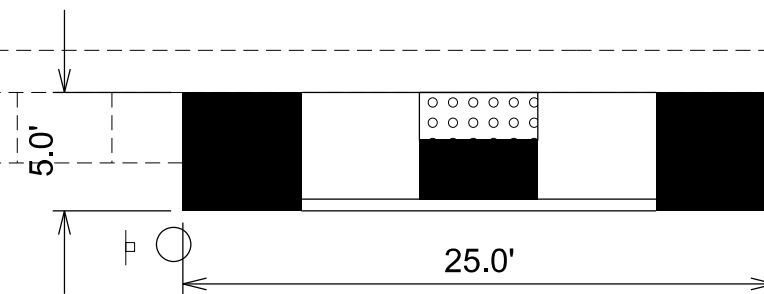
**LOCATION #3**



**US 24**

**LEGEND**

- EXIST. SIDEWALK / CURB
- PROPOSED ELEMENT
- DETECTABLE WARNINGS
- LANDING AREA / TURNING SPACES



**LOCATION #4**



**NOT TO SCALE**

MODEL: \\MODELS\BAMES  
FILE NAME: 311215

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALE\$ | DRAWN -    | REVISED - |
| PLOT DATE = \$DATE\$   | CHECKED -  | REVISED - |
|                        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

SCALE: SHEET 4 OF 8 SHEETS STA. TO STA.

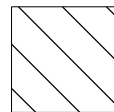
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|----------------------------|--------|--------------------|-----------|
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 34        |
| ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |

**GENERAL NOTES**

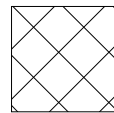
1. WHERE PAVEMENT REMOVAL IS REQUIRED FOR INSTALLATION OF THE SIDEWALK, THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE SIDEWALK TO ALLOW FOR PLACEMENT OF FORMS. SAWCUTTING OF THE PAVEMENT SHALL BE INCLUDED IN THE COST OF PCC SIDEWALK, 4"
2. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
3. CURB AND GUTTER REMOVAL LOCATIONS CAN BE FOUND IN THE SCHEDULE OF QUANTITIES.

**LOCATIONS #5 - REMOVAL DETAILS**  
**US 24 MID-BLOCK CROSSING 1696+55**

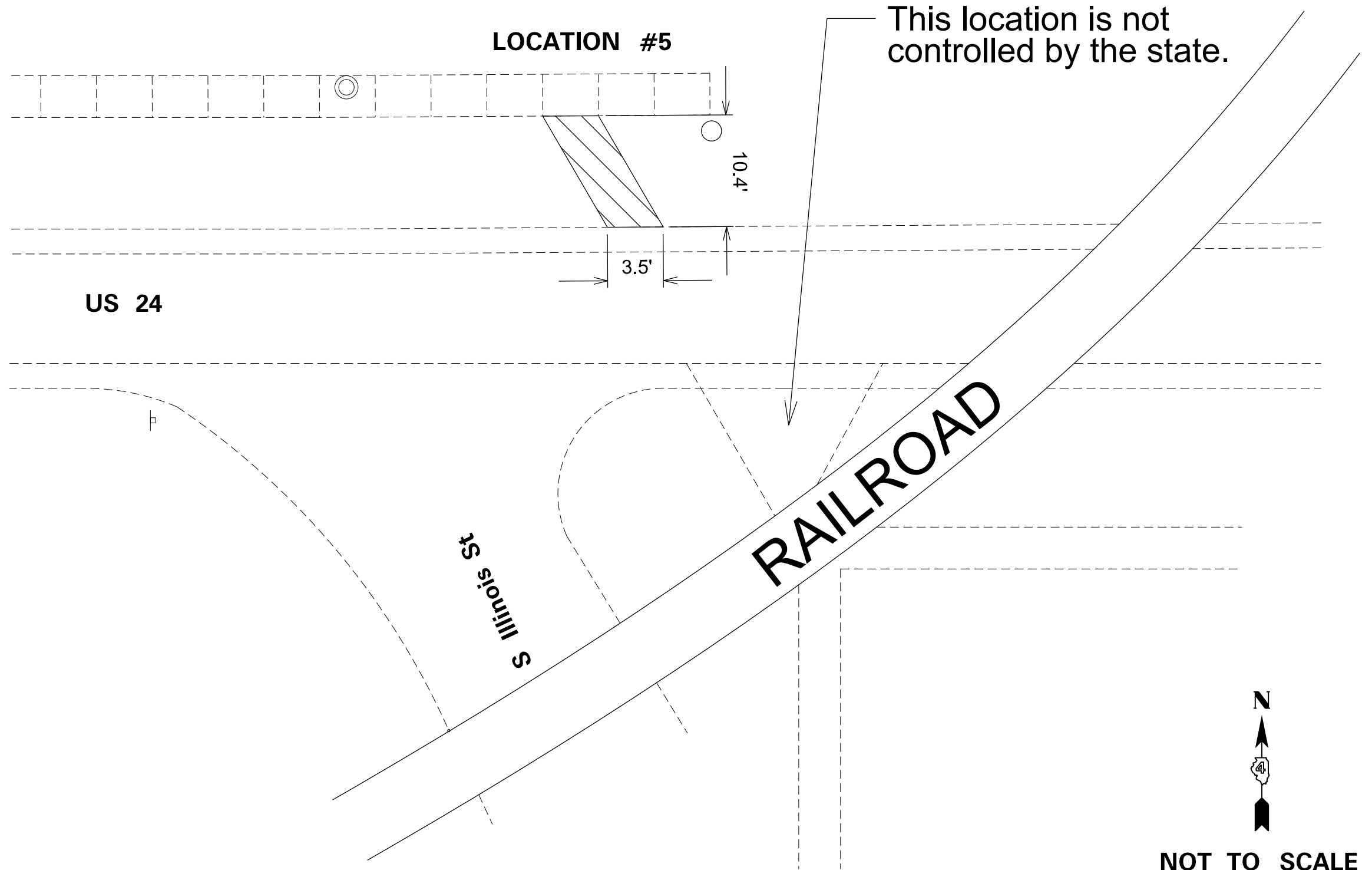
**LEGEND**



SIDEWALK / SIDE CURB REMOVAL



CURB REMOVAL



**NOT TO SCALE**

MODEL: \\MODELS\NAME\$  
FILE NAME: ST153

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

SCALE: SHEET 5 OF 8 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 35        |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |

**GENERAL NOTES**

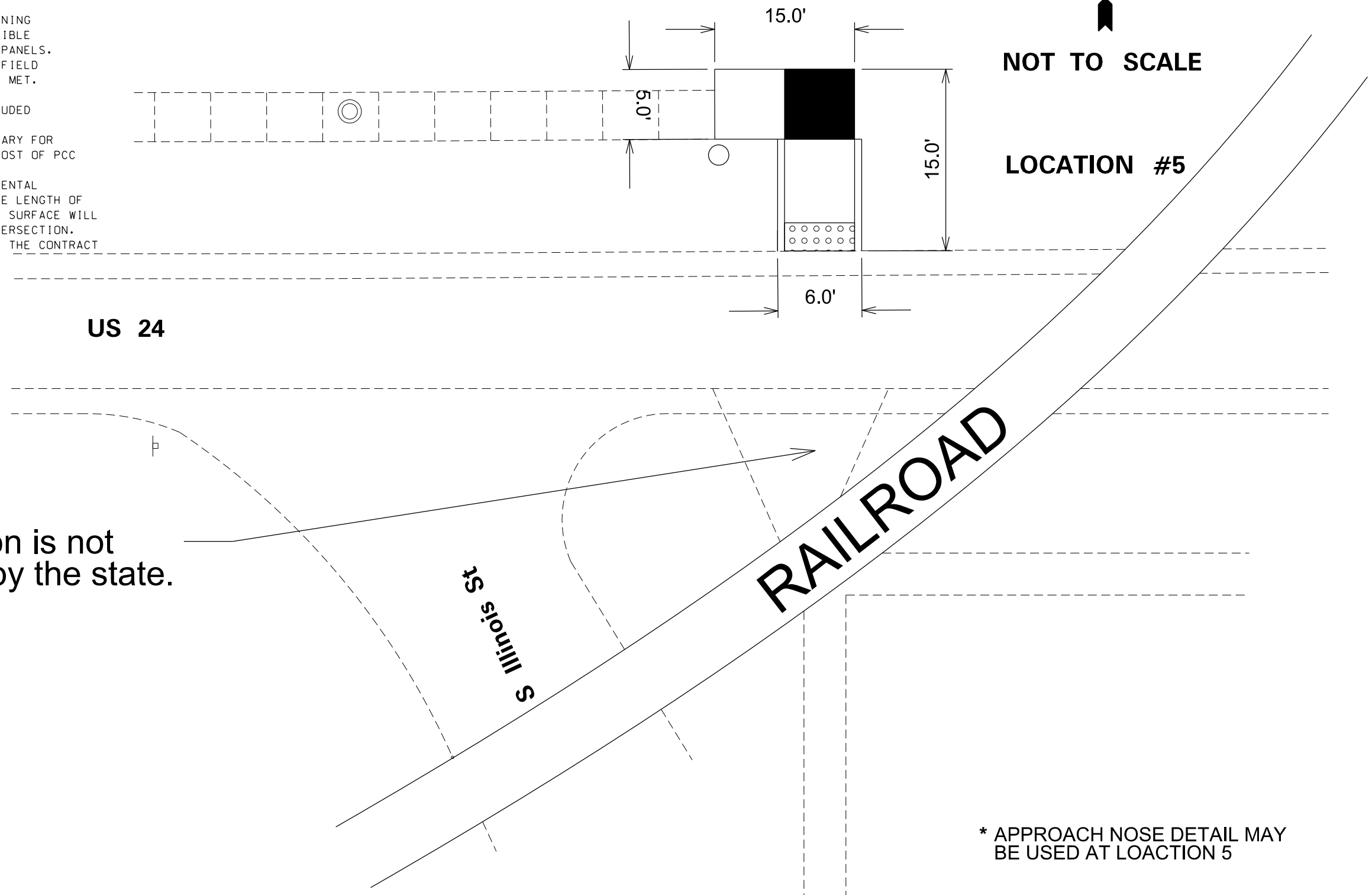
1. ALL SIDEWALK SHOULD FOLLOW STANDARDS 424001-11, 424021-05, 424011-04, AND 424016-05
2. SIDEWALK CROSS SLOPE = 1.5% (2.0% MAX.)
3. UPPER LANDINGS AND LOWER LANDINGS SHALL BE 1.5% MAXIMUM IN ANY DIRECTION; INCLUDING DIAGONALLY. LANDINGS AND TURNING SPACES SHOULD HAVE A MINIMUM DIMENSION OF 4' BY 4', BUT 5' BY 5' IS PREFERRED.
4. MAXIMUM RAMP LONGITUDINAL SLOPE = 8.3% (1:12)
5. MAXIMUM SLOPE AROUND RADIUS = 2%
6. FLARES ARE 1.5' UNLESS OTHERWISE NOTED.
7. CONTRACTOR TO UTILIZE RADIAL DETECTABLE WARNING PANELS AROUND RADIUS. CONTRACTOR IS RESPONSIBLE FOR MEASURING THE RADIUS PRIOR TO ORDERING PANELS.
8. CONTRACTOR SHALL VERIFY REMOVAL LIMITS AND FIELD ELEVATIONS TO ENSURE THAT ADA STANDARDS ARE MET.
9. STREET WIDTHS ARE NOT SHOWN TO SCALE.
10. SAWCUTTING THE SIDEWALK IS CONSIDERED INCLUDED IN THE COST OF SIDEWALK REMOVAL.
11. ANY EXCAVATION OR AGGREGATE SUBBASE NECESSARY FOR THE PROPOSED SIDEWALK IS INCLUDED IN THE COST OF PCC SIDEWALK, 4".
12. UNLESS OTHERWISE NOTED IN THE PLANS, INCIDENTAL HMA SURFACE HAS THE DIMENSIONS OF 1' BY THE LENGTH OF SIDEWALK WITH A DEPTH OF 4". INCIDENTAL HMA SURFACE WILL BE PLACED WHERE THE SIDEWALK MEETS THE INTERSECTION.
13. SIGN RELOCATION IS INCLUDED IN THE COST OF THE CONTRACT PAY ITEMS.

**LOCATIONS #5 - PROPOSED DETAILS**  
**US 24 MID-BLOCK CROSSING 1696 + 55**



**NOT TO SCALE**

**LOCATION #5**



This location is not controlled by the state.

**LEGEND**

- EXIST. SIDEWALK / CURB
- PROPOSED ELEMENT
- DETECTABLE WARNINGS
- LANDING AREA / TURNING SPACES

\* APPROACH NOSE DETAIL MAY BE USED AT LOACTION 5

MODEL: \\MODELNAME\$  
FILE: \\NAME\$.STB

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

SCALE: SHEET 6 OF 8 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 36        |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |

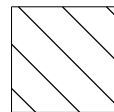
**GENERAL NOTES**

1. WHERE PAVEMENT REMOVAL IS REQUIRED FOR INSTALLATION OF THE SIDEWALK, THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE SIDEWALK TO ALLOW FOR PLACEMENT OF FORMS. SAWCUTTING OF THE PAVEMENT SHALL BE INCLUDED IN THE COST OF PCC SIDEWALK, 4" SIDEWALK REMOVAL.
2. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
3. CURB AND GUTTER REMOVAL LOCATIONS CAN BE FOUND IN THE SCHEDULE OF QUANTITIES.

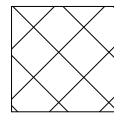
**LOCATIONS #6 – REMOVAL DETAILS  
US 24 & S. CHESTNUT STREET**

This location is outside of the jurisdiction of this project

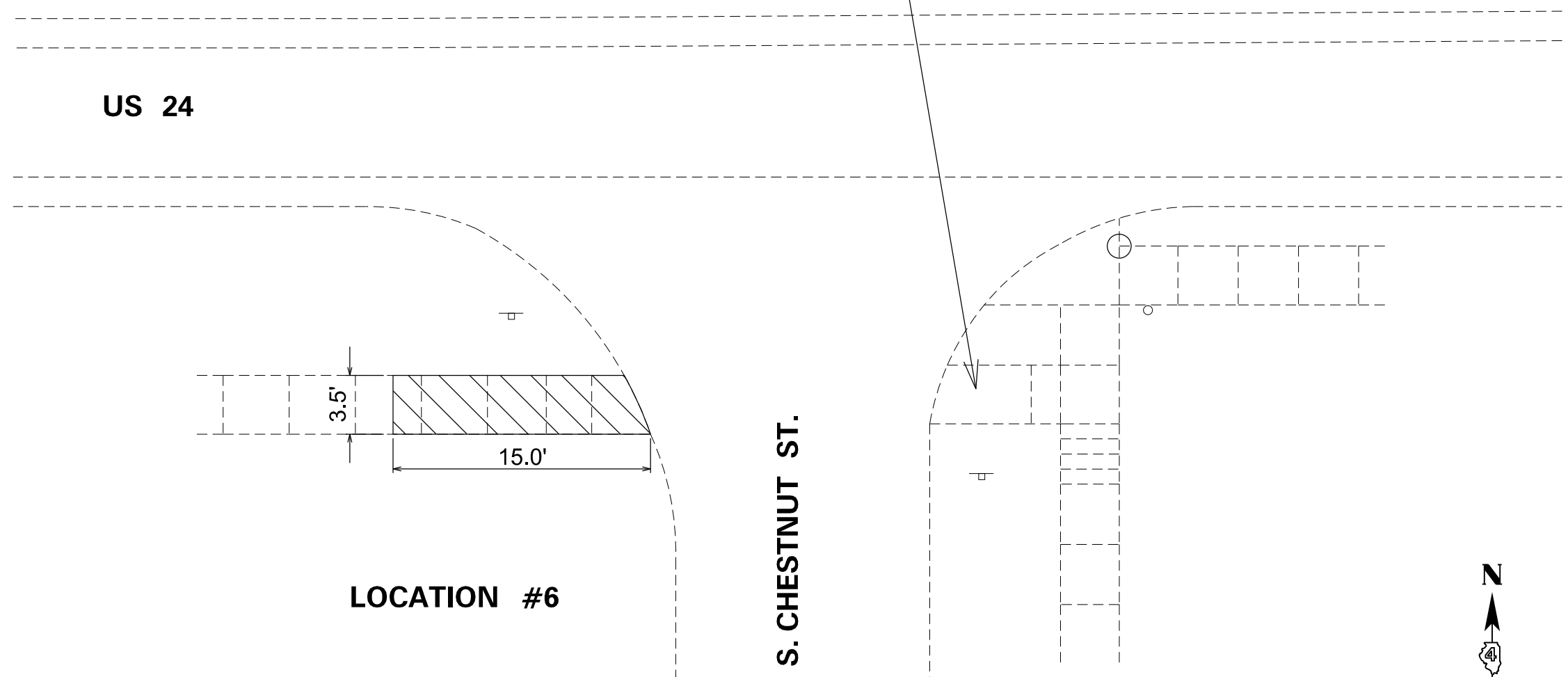
**LEGEND**



SIDEWALK / SIDE CURB REMOVAL

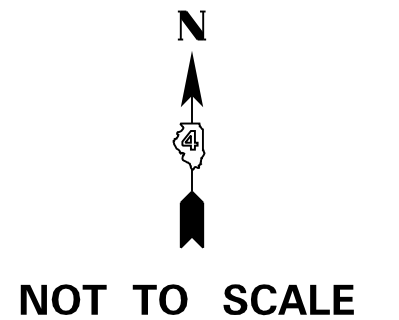


CURB REMOVAL



**LOCATION #6**

**S. CHESTNUT ST.**



MODEL: \\MODELS\NAME\$  
FILE NAME: 31125

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

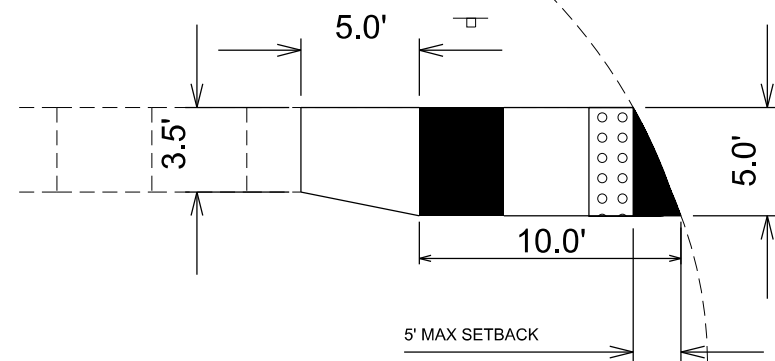
SCALE: SHEET 7 OF 8 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION                    | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|-------------|----------------------------|----------|--------------------|-----------|
| VAR         | (137, 129)RS-4, (137X)RS-4 | FULTON   | 73                 | 37        |
|             |                            |          | CONTRACT NO. 68A91 |           |
|             |                            | ILLINOIS | FED. AID PROJECT   |           |

## LOCATIONS #6 - PROPOSED DETAILS US 24 & S. CHESTNUT STREET

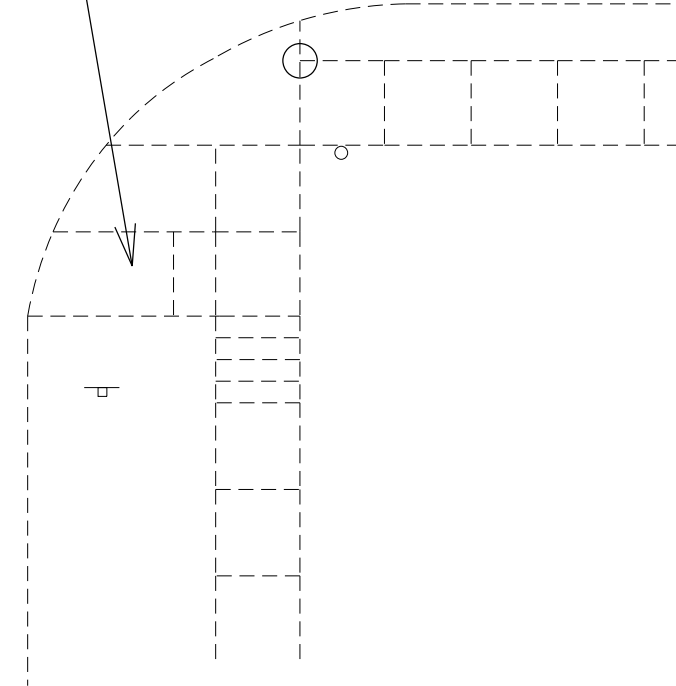
This location is outside of  
the jurisdiction of this project

US 24



**LOCATION #6**

S. CHESTNUT ST.



**NOT TO SCALE**

**LEGEND**

- EXIST. SIDEWALK / CURB
- PROPOSED ELEMENT
- DETECTABLE WARNINGS
- LANDING AREA / TURNING SPACES

MODEL: \$MODELNAME\$  
FILE NAME: \$FILE\$

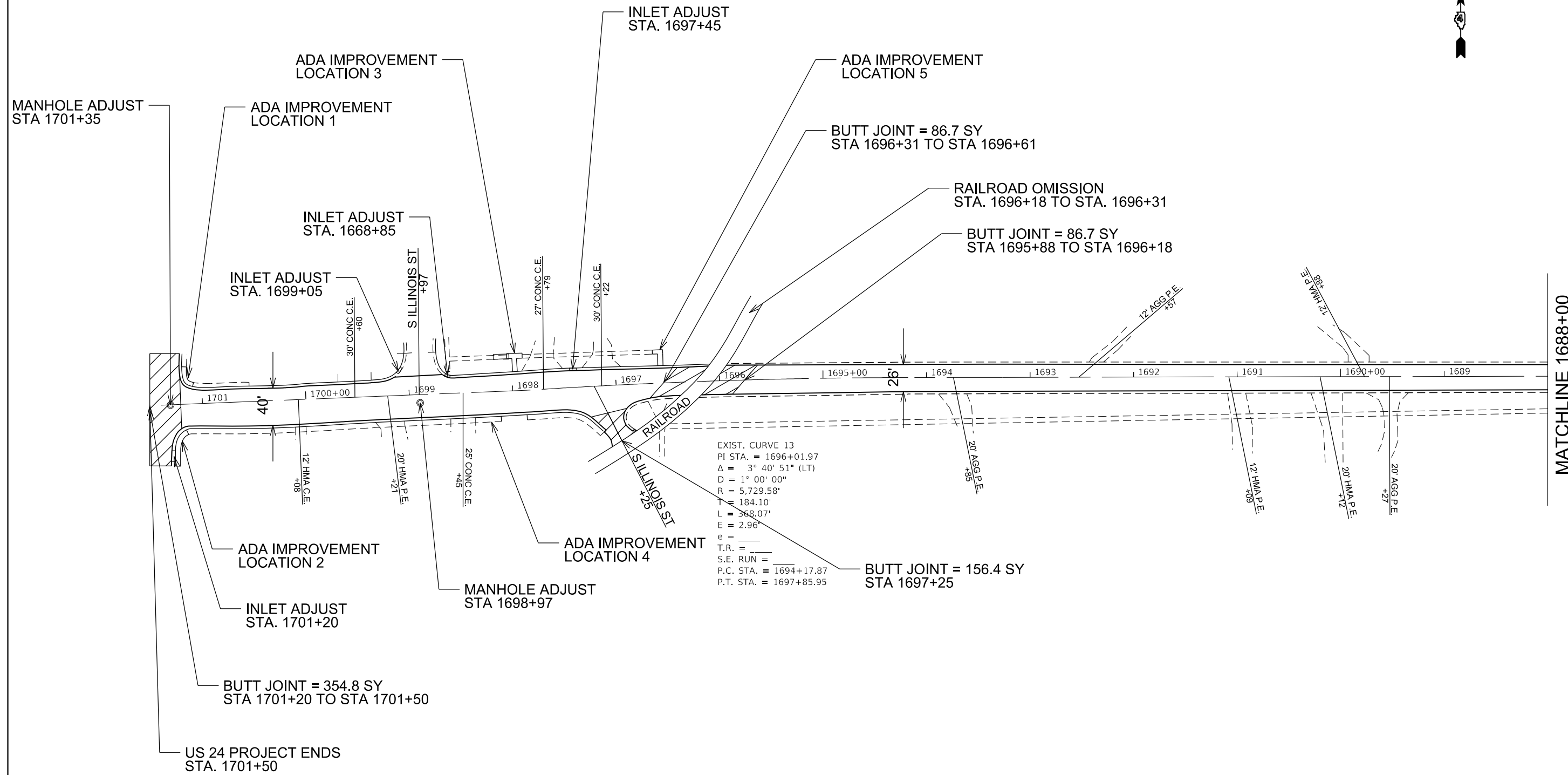
|                        |            |           |  |
|------------------------|------------|-----------|--|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |  |
|                        | DRAWN -    | REVISED - |  |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |  |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENTS**

SCALE: SHEET 8 OF 8 SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|----------------------------|--------|--------------|-----------|
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 38        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



MODEL: 140DELMAMES  
FILE NAME: 31113

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                              |         |              |              |
|------------------------------|---------|--------------|--------------|
| <b>PLAN SHEETS<br/>US 24</b> |         |              |              |
| SCALE:                       | SHEET 1 | OF 17 SHEETS | STA. TO STA. |

|             |                            |          |                    |           |
|-------------|----------------------------|----------|--------------------|-----------|
| F.A.P. RTE. | SECTION                    | COUNTY   | TOTAL SHEETS       | SHEET NO. |
| VAR         | (137, 129)RS-4, (137X)RS-4 | FULTON   | 73                 | 39        |
|             |                            |          | CONTRACT NO. 68A91 |           |
|             |                            | ILLINOIS | FED. AID PROJECT   |           |

MATCHLINE 1688+00

1688 1687 1686 1685+00 1684 1683 1682 1681 1680+00 1679 1678 1677 1676 1675+00

ADA IMPROVEMENT LOCATION 6

CHESTNUT ST +86

EXIST. CURVE N  
PI STA. = 1683+01.64  
Δ = 0° 45' 50" (RT)  
D = 0° 18' 20"  
R = 18,751.00'  
T = 125.00'  
L = 250.00'  
E = 0.42'  
e =  
T.R. =  
S.E. RUN =  
P.C. STA. = 1681+76.63  
P.T. STA. = 1684+26.64

EXIST. CURVE M  
PI STA. = 1680+01.64  
Δ = 0° 45' 50" (RT)  
D = 0° 18' 20"  
R = 18,751.00'  
T = 125.00'  
L = 250.00'  
E = 0.42'  
e =  
T.R. =  
S.E. RUN =  
P.C. STA. = 1678+76.63  
P.T. STA. = 1681+26.63

HMA VARIABLE DEPTH ENDS STA 1675+26

CURB AND GUTTER BEGINS STA 1676+26 LT.

7' HMA SHOULDER ENDS STA 1675+26 LT.

HMA VARIABLE DEPTH BEGINS STA 1674+56 (SEE DETAIL)

7' HMA SHOULDER ENDS STA 1675+26 RT.  
CURB AND GUTTER BEGINS STA 1676+26 RT.



MATCHLINE 1674+00

MATCHLINE 1674+00

1674 1673 1672 1671 1670+00 1669 1668 1667 1666 1665+00 1664 1663 1662 1661 1660+00

EXISTING GUTTER BEGINS STA 1665+63 RT.

EXISTING GUTTER ENDS STA 1670+25 RT.

7' HMA SHOULDER BEGINS STA 1659+50 RT.

12' AGG.P.E. +50

28' AGG.C.E. +28

35' AGG.C.E. +37

12' AGG.P.E. +97

12' HMA.P.E. +80

12' AGG.P.E. +30

EXISTING GUTTER ENDS STA 1671+00 LT.

EXISTING GUTTER BEGINS STA 1664+70 LT.

7' HMA SHOULDER BEGINS STA 1659+50 LT.

EXIST. CURVE J  
PI STA. = 1665+01.67  
Δ = 0° 51' 34" (RT)  
D = 0° 17' 11"  
R = 20,010.00'  
T = 150.08'  
L = 300.15'  
E = 0.56'  
e =  
T.R. =  
S.E. RUN =  
P.C. STA. = 1663+51.59  
P.T. STA. = 1666+51.74

EXIST. CURVE I  
PI STA. = 1661+01.63  
Δ = 0° 51' 34" (RT)  
D = 0° 17' 11"  
R = 20,010.00'  
T = 150.08'  
L = 300.15'  
E = 0.56'  
e =  
T.R. =  
S.E. RUN =  
P.C. STA. = 1659+51.55  
P.T. STA. = 1662+51.70



MATCHLINE 1659+00

MODEL: \\MODELS\NAMES  
FILE: \\MODELS\STYLES

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

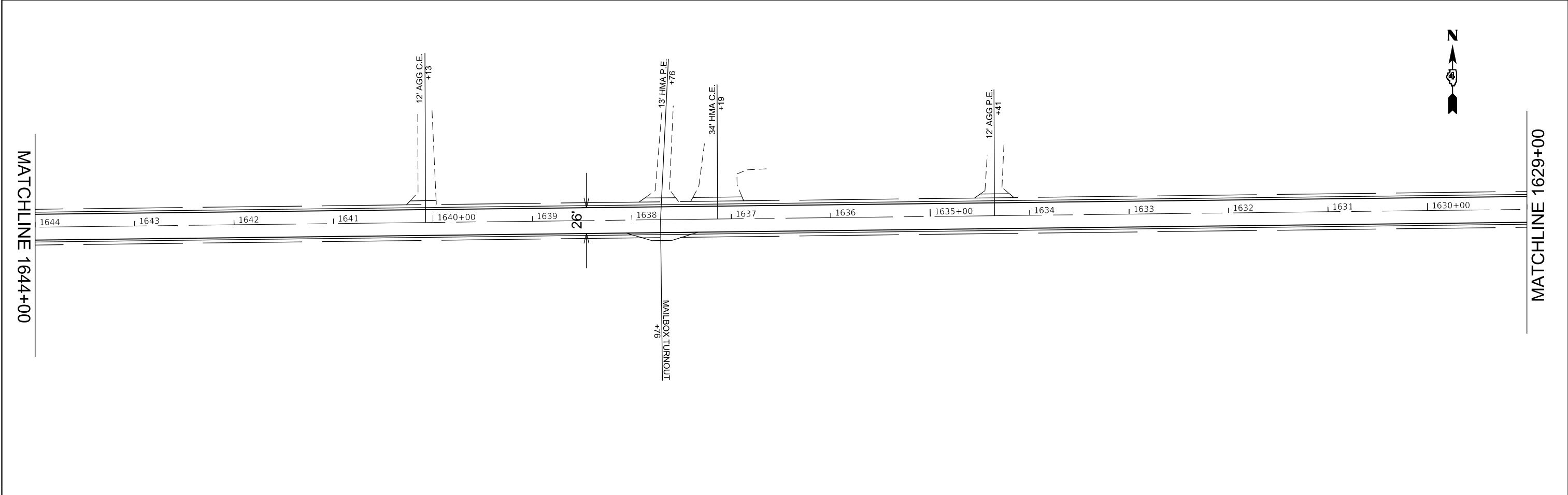
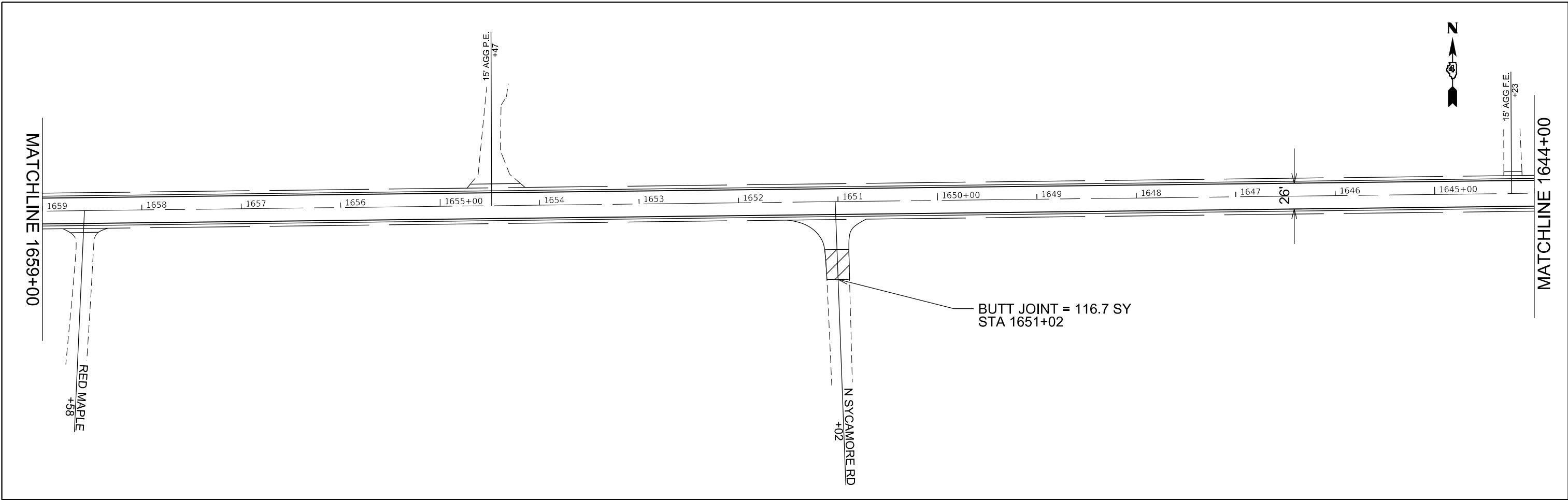
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS  
US 24

SCALE: SHEET 2 OF 17 SHEETS STA. TO STA.

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 40        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |





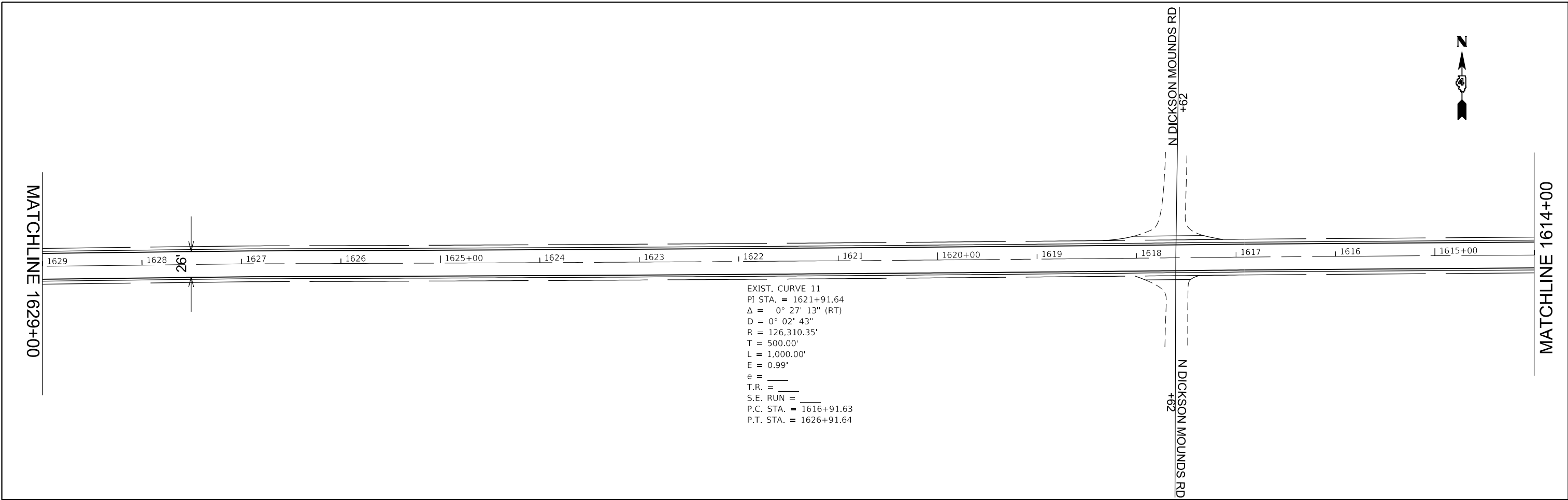
MODEL: \\MODELS\MM\FILES

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

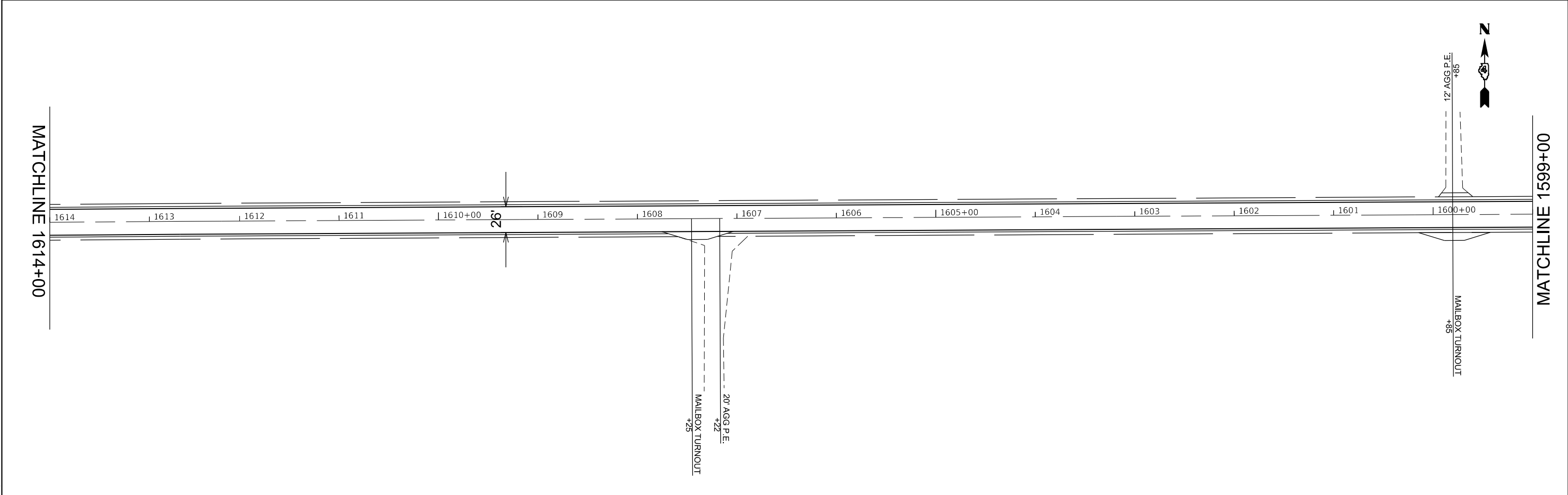
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                    |                                   |
|--------------------|-----------------------------------|
| <b>PLAN SHEETS</b> |                                   |
| <b>US 24</b>       |                                   |
| SCALE:             | SHEET 3 OF 17 SHEETS STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 41        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



EXIST. CURVE 11  
 PI STA. = 1621+91.64  
 $\Delta$  = 0° 27' 13" (RT)  
 D = 0° 02' 43"  
 R = 126,310.35'  
 T = 500.00'  
 L = 1,000.00'  
 E = 0.99'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 1616+91.63  
 P.T. STA. = 1626+91.64



MAILBOX TURNOUT  
 +25  
 20' AGG P.E.  
 +22

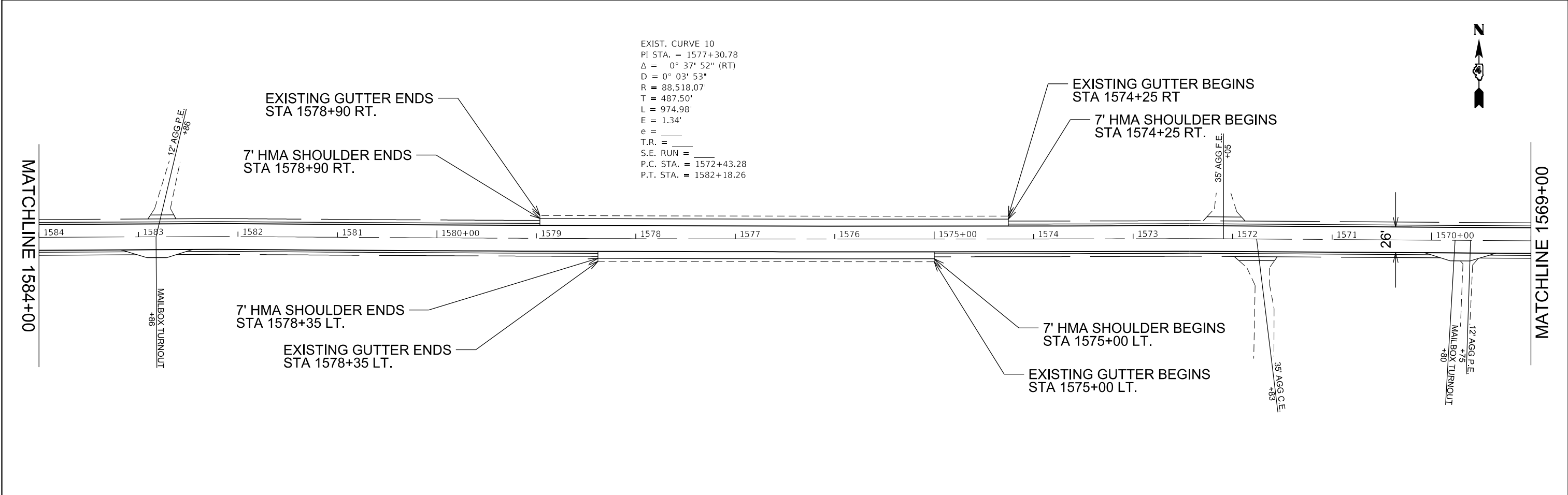
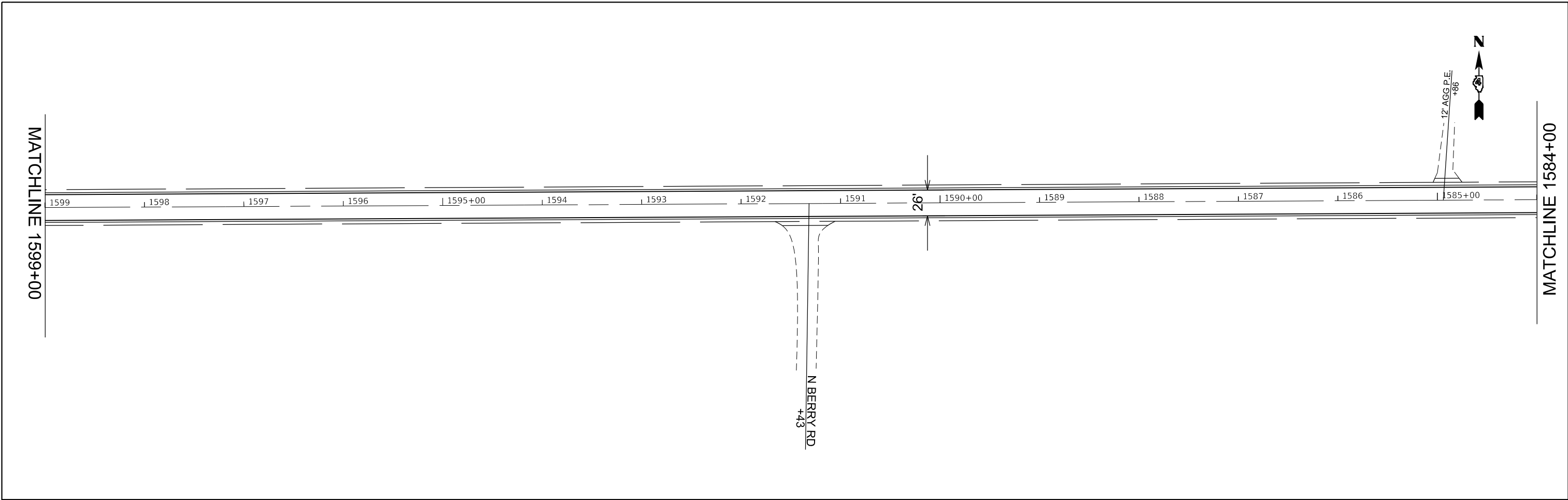
MODEL: \\MODELS\NAME\$  
 FILE NAME: \$FILE\$

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|                              |         |              |              |
|------------------------------|---------|--------------|--------------|
| <b>PLAN SHEETS<br/>US 24</b> |         |              |              |
| SCALE:                       | SHEET 4 | OF 17 SHEETS | STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 42        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



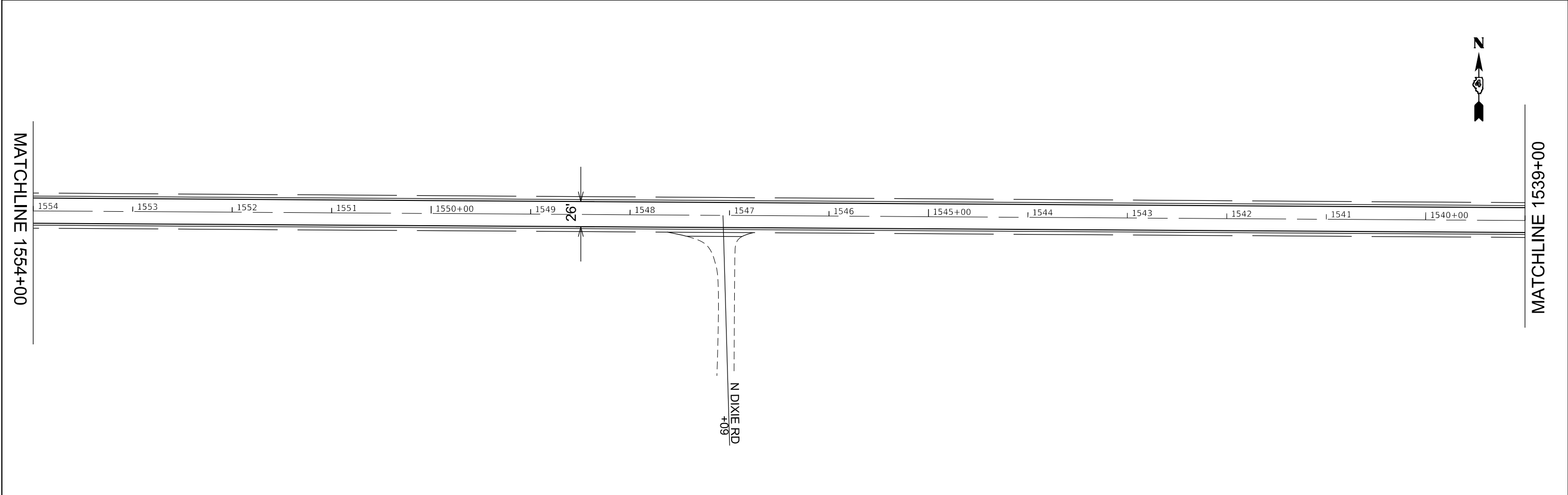
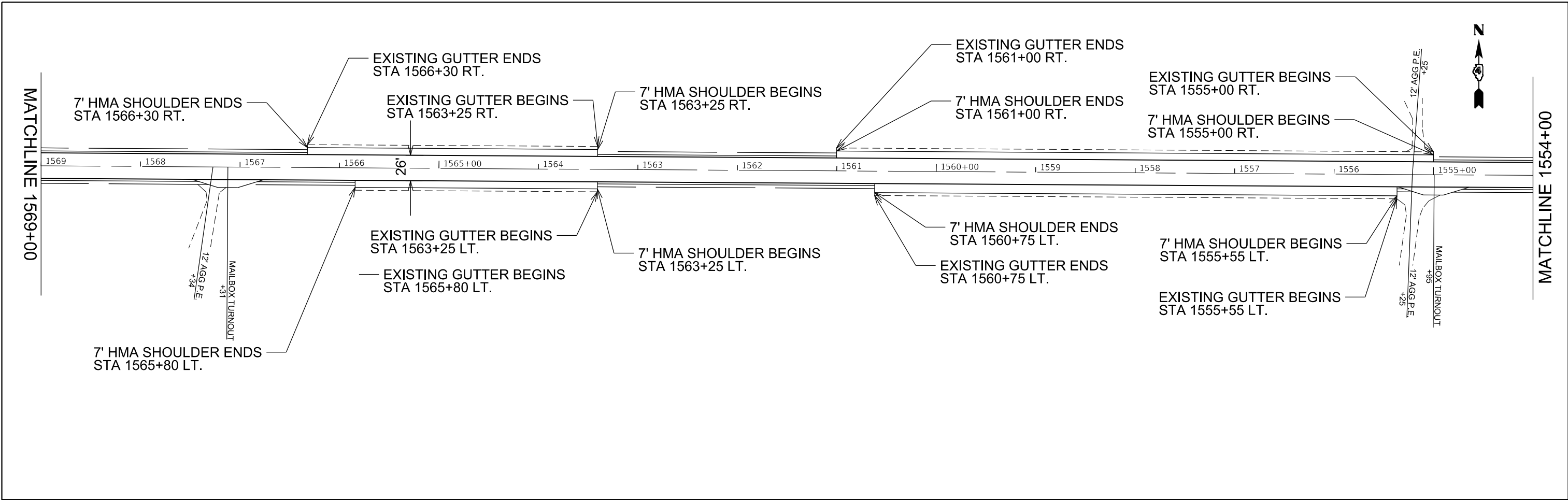
MODEL NUMBER: 1584+00  
 FILE NAME: 1584+00

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|                              |                                   |
|------------------------------|-----------------------------------|
| <b>PLAN SHEETS<br/>US 24</b> |                                   |
| SCALE:                       | SHEET 5 OF 17 SHEETS STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 43        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



MODEL: \\MODELS\FILES  
FILE NAME: STILES

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                    |                                   |
|--------------------|-----------------------------------|
| <b>PLAN SHEETS</b> |                                   |
| <b>US 24</b>       |                                   |
| SCALE:             | SHEET 6 OF 17 SHEETS STA. TO STA. |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 44        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |



MATCHLINE 1539+00

MATCHLINE 1524+00

1539 1538 1537 1536 1535+00 1534 1533 1532 1531 1530+00 1529 1528 1527 1526 1525+00



MATCHLINE 1524+00

MATCHLINE 1509+00

EXIST. CURVE 9  
 PI STA. = 1516+18.99  
 $\Delta = 15^\circ 34' 43''$  (RT)  
 $D = 1^\circ 00' 45''$   
 $R = 5,658.64'$   
 $T = 774.07'$   
 $L = 1,538.58'$   
 $E = 52.70'$   
 $e =$   
 $T.R. =$   
 $S.E. RUN =$   
 $P.C. STA. = 1508+44.92$   
 $P.T. STA. = 1523+83.50$

US 24

N BETHEL RD  
 +02'

IL 78/97

ISLAND REMOVAL  
 AND PLACEMENT  
 STA 1516+75

STA. 1515+88.74  $\dot{C}$  US 24 =  
 STA. 185+7.98  $\dot{C}$  IL 78 / 97

MATCHLINE 188+00

MODEL: \\MODELS\MAMES  
 FILE NAME: 37115

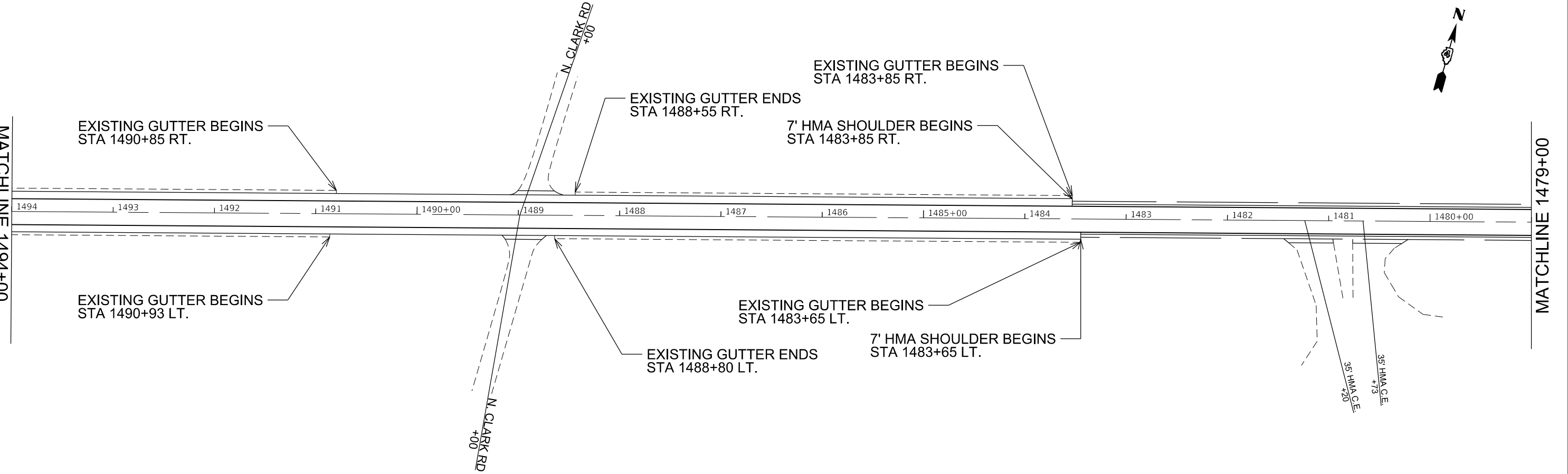
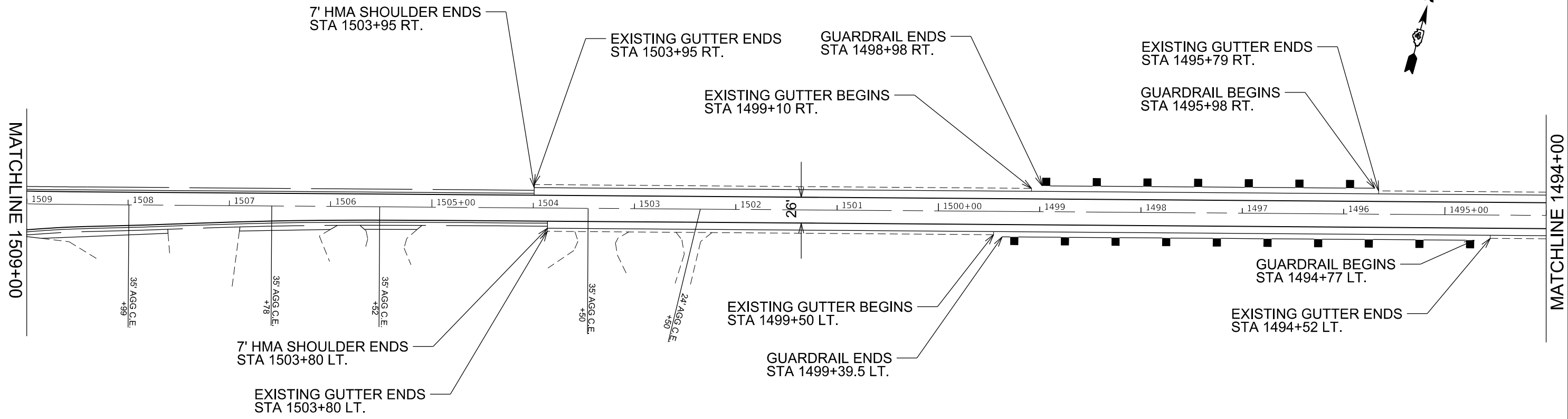
|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PLAN SHEETS  
 US 24

SCALE: SHEET 7 OF 17 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 45        |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |



MODEL: \\MODELS\NAMES  
FILE: \\MODELS\FILES

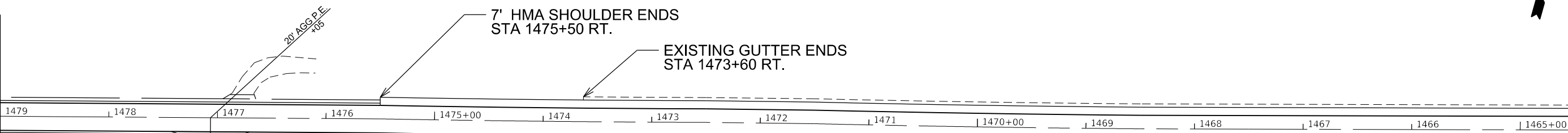
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|-----------------------|------------|-----------|
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|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                              |         |              |              |
|------------------------------|---------|--------------|--------------|
| <b>PLAN SHEETS<br/>US 24</b> |         |              |              |
| SCALE:                       | SHEET 8 | OF 17 SHEETS | STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 46        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |

MATCHLINE 1479+00



7' HMA SHOULDER ENDS  
STA 1475+50 LT.

EXIST. CURVE H  
PI STA. = 1474+00.08  
 $\Delta = 0^\circ 51' 34''$  (RT)  
R = 20,010.00'  
T = 150.08'  
L = 300.15'  
E = 0.56'  
e = —  
T.R. = —  
S.E. RUN = —  
P.C. STA. = 1472+50.00  
P.T. STA. = 1475+50.15

EXISTING GUTTER ENDS  
STA 1473+60 LT.

EXIST. CURVE G  
PI STA. = 1470+00.03  
 $\Delta = 0^\circ 51' 34''$  (RT)  
R = 20,010.00'  
T = 150.08'  
L = 300.15'  
E = 0.56'  
e = —  
T.R. = —  
S.E. RUN = —  
P.C. STA. = 1468+49.96  
P.T. STA. = 1471+50.11

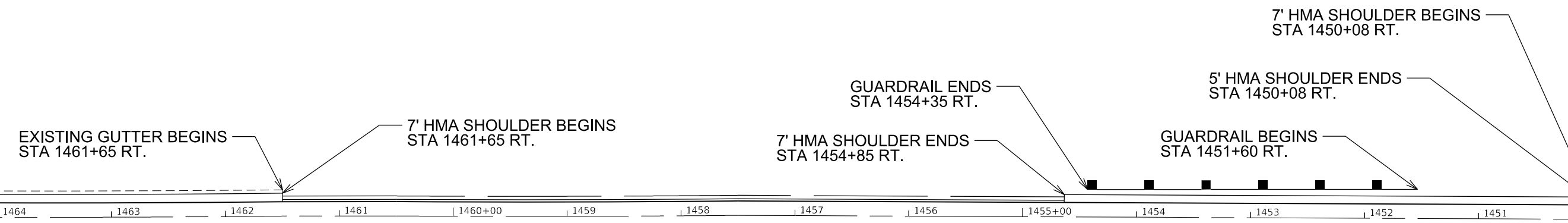
EXISTING GUTTER BEGINS  
STA 1464+80 RT.

7' HMA SHOULDER BEGINS  
STA 1464+80 LT.

MATCHLINE 1464+00



MATCHLINE 1464+00



EXISTING GUTTER BEGINS  
STA 1461+65 RT.

7' HMA SHOULDER BEGINS  
STA 1461+65 RT.

EXIST. CURVE F  
PI STA. = 1463+00.04  
 $\Delta = 0^\circ 51' 34''$  (RT)  
R = 20,010.00'  
T = 150.08'  
L = 300.15'  
E = 0.56'  
e = —  
T.R. = —  
S.E. RUN = —  
P.C. STA. = 1461+49.96  
P.T. STA. = 1464+50.12

EXIST. CURVE E  
PI STA. = 1459+00.00  
 $\Delta = 0^\circ 51' 34''$  (RT)  
R = 20,010.00'  
T = 150.08'  
L = 300.15'  
E = 0.56'  
e = —  
T.R. = —  
S.E. RUN = —  
P.C. STA. = 1457+49.92  
P.T. STA. = 1460+50.07

7' HMA SHOULDER ENDS  
STA 1454+85 RT.

GUARDRAIL ENDS  
STA 1454+35 RT.

7' HMA SHOULDER ENDS  
STA 1454+85 LT.

GUARDRAIL ENDS  
STA 1454+00 LT.

GUARDRAIL BEGINS  
STA 1451+60 RT.

5' HMA SHOULDER ENDS  
STA 1450+08 RT.

GUARDRAIL BEGINS  
STA 1451+25 LT.

7' HMA SHOULDER BEGINS  
STA 1450+08 RT.

MATCHLINE 1450+00



MODEL: \\MODELS\NAMES  
FILE: \\MODELS\FILES

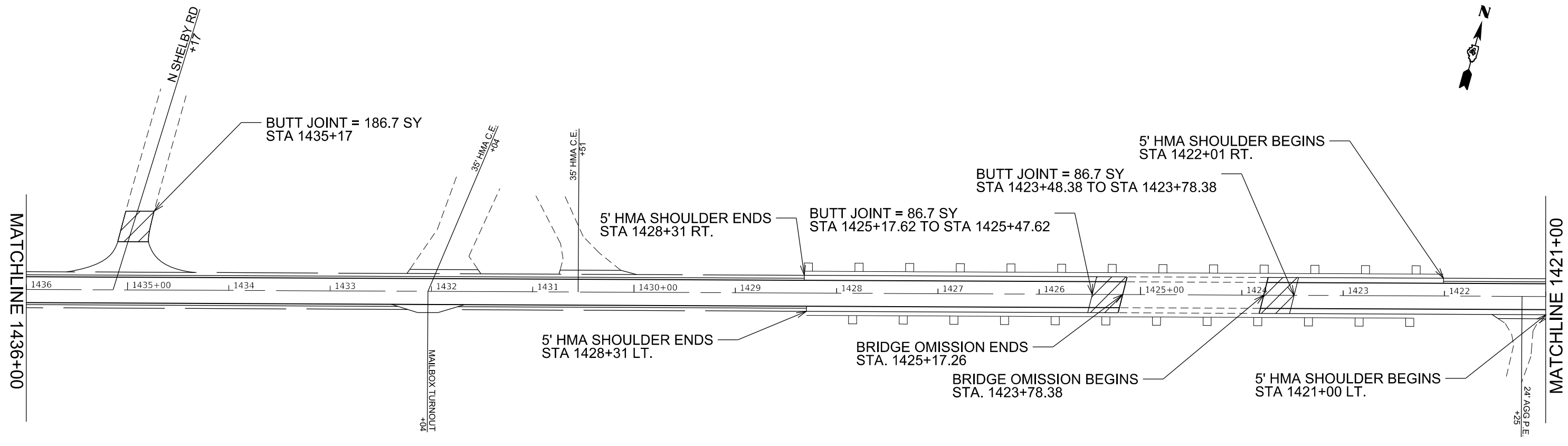
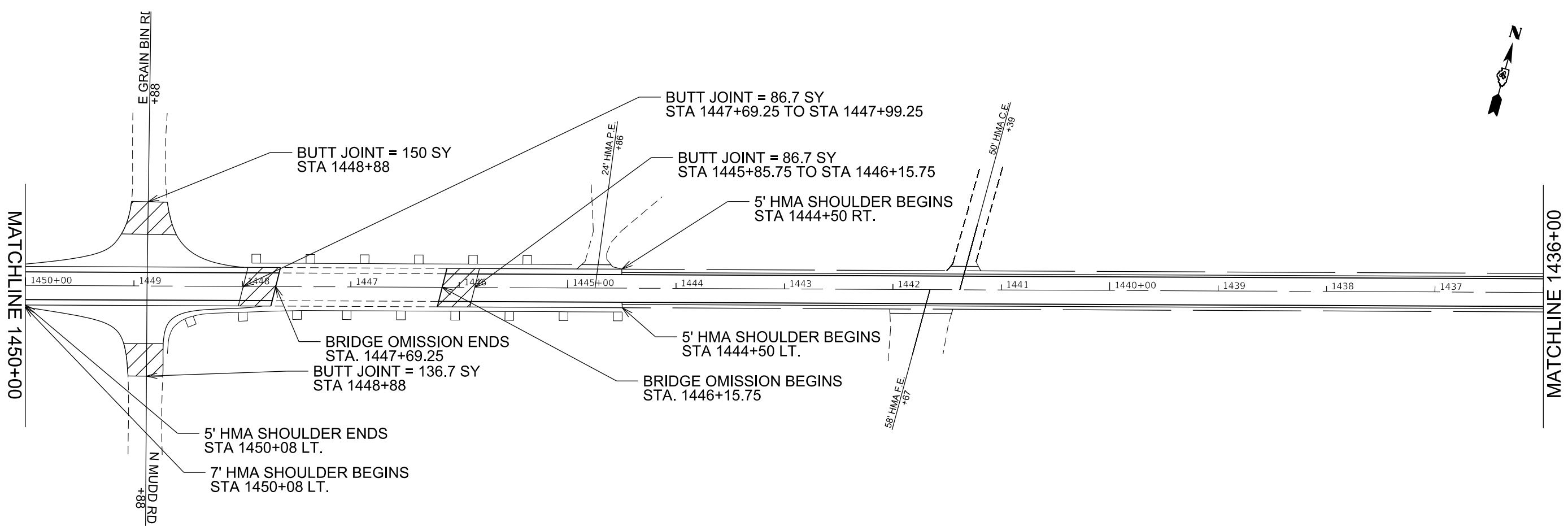
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| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS  
US 24

SCALE: SHEET 9 OF 17 SHEETS STA. TO STA.

|                                |         |        |                           |           |
|--------------------------------|---------|--------|---------------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                        | 47        |
| CONTRACT NO. 68A91             |         |        | ILLINOIS FED. AID PROJECT |           |



MODEL: \\MODELS\NAMES  
FILE: \\MODELS\FILES

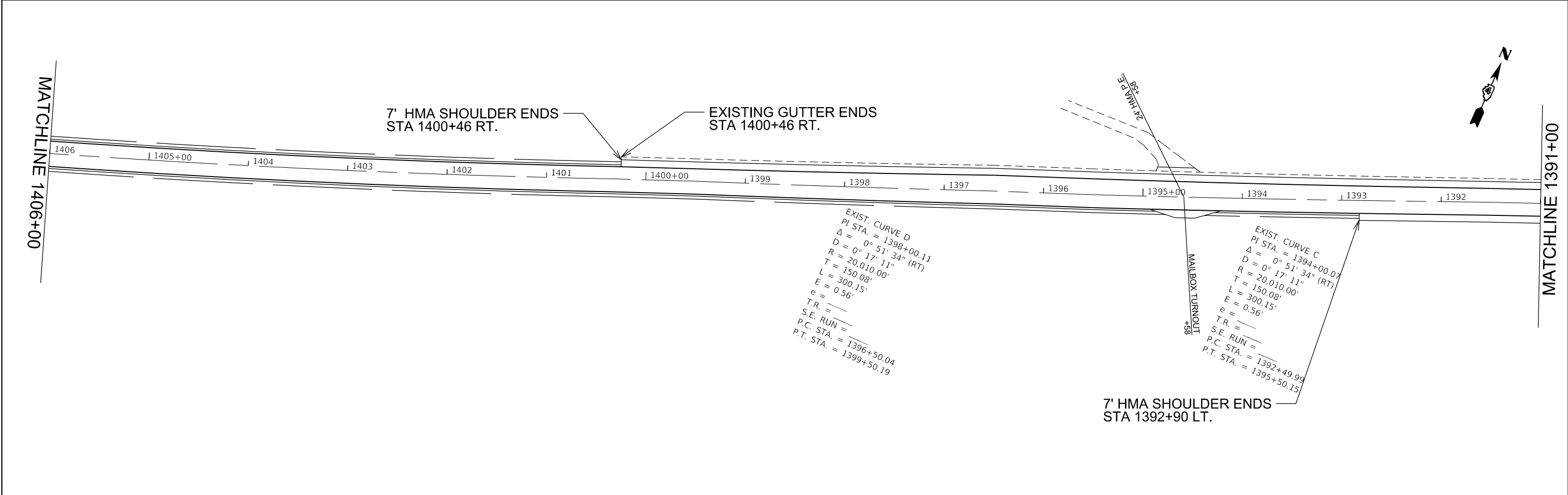
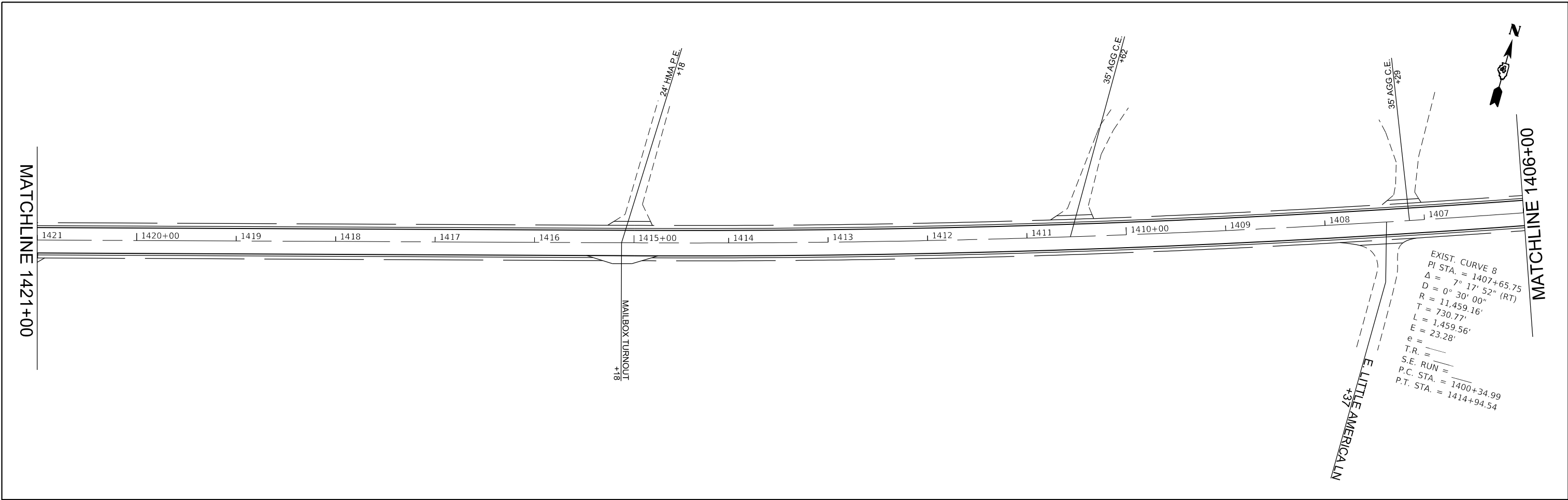
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|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                              |                       |      |         |
|------------------------------|-----------------------|------|---------|
| <b>PLAN SHEETS<br/>US 24</b> |                       |      |         |
| SCALE:                       | SHEET 10 OF 17 SHEETS | STA. | TO STA. |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 48        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |





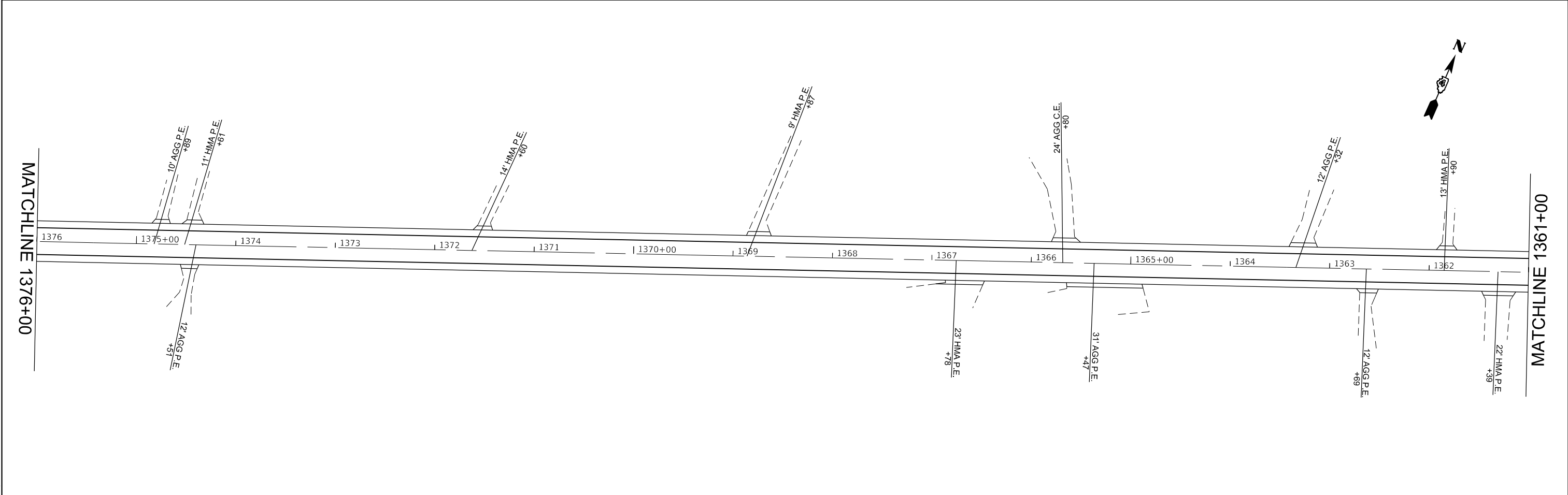
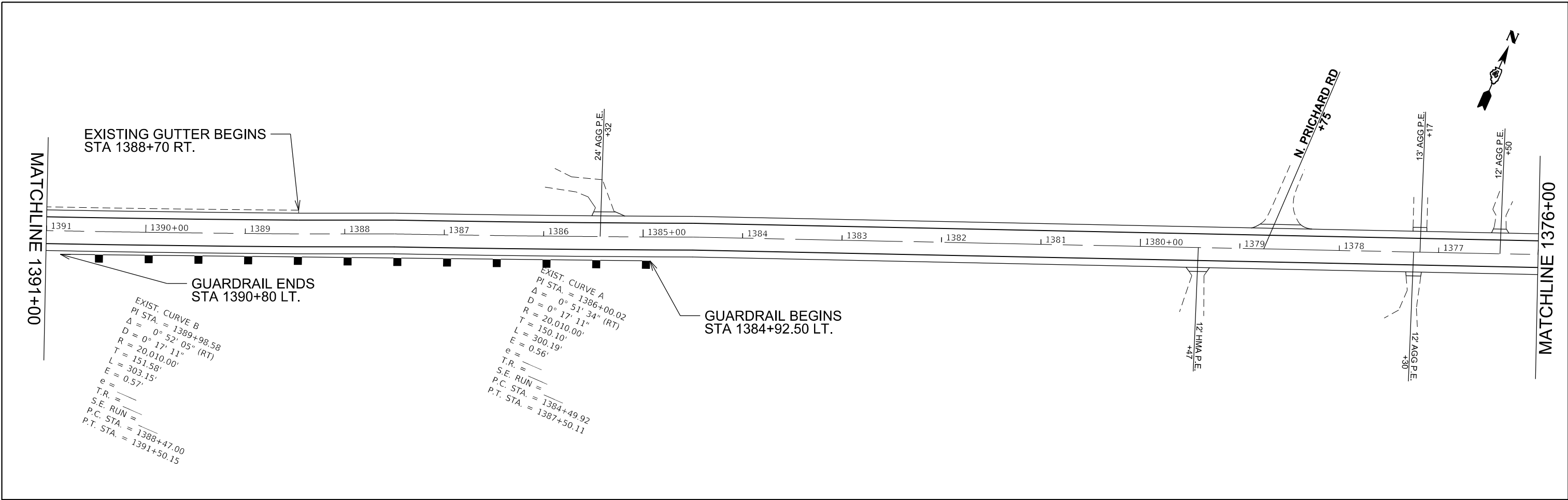
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|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALE\$ | DRAWN -    | REVISED - |
| PLOT DATE = \$DATE\$   | CHECKED -  | REVISED - |
|                        | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|             |                                    |
|-------------|------------------------------------|
| PLAN SHEETS |                                    |
| US 24       |                                    |
| SCALE:      | SHEET 11 OF 17 SHEETS STA. TO STA. |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73           | 49        |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |



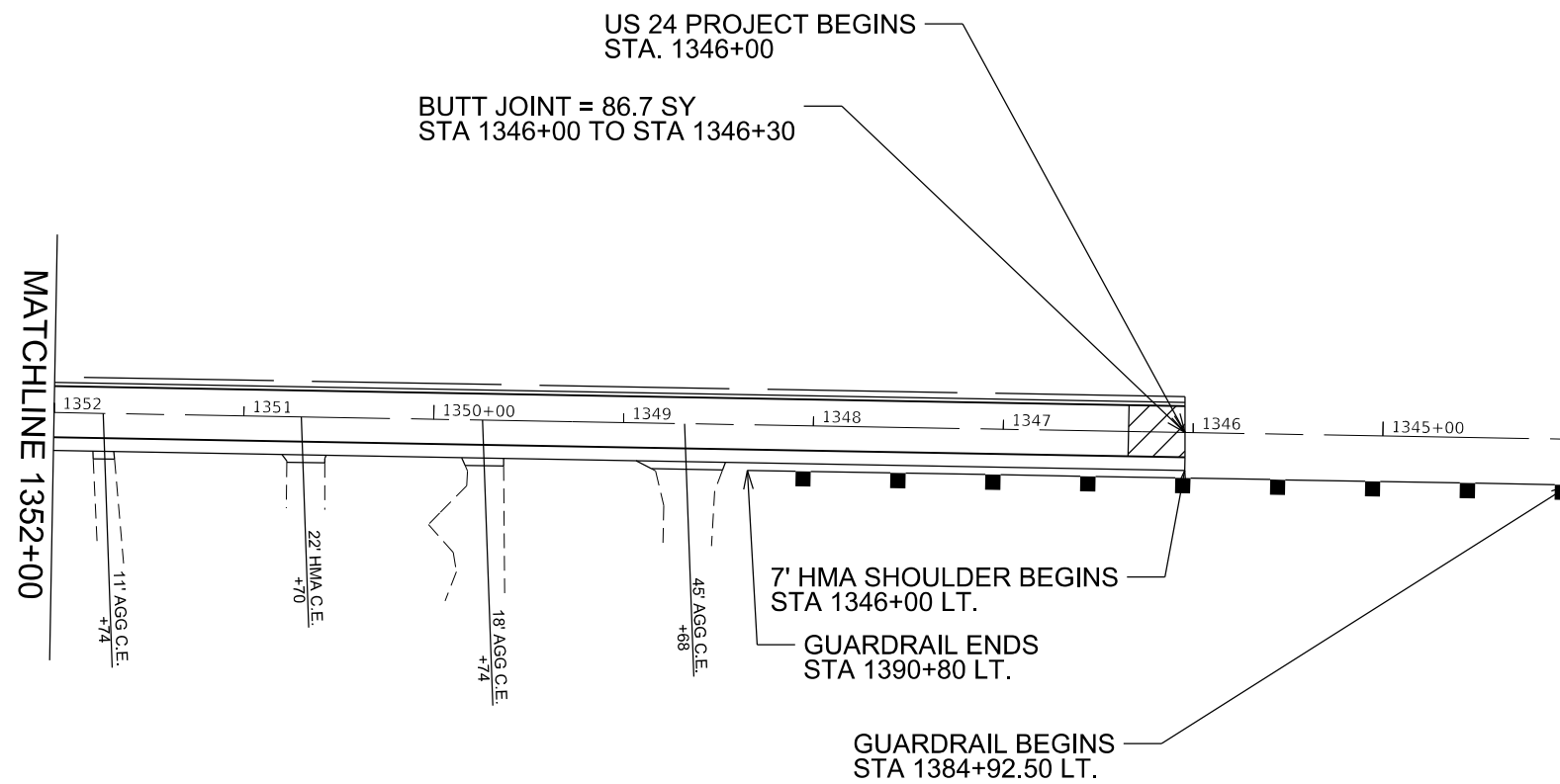
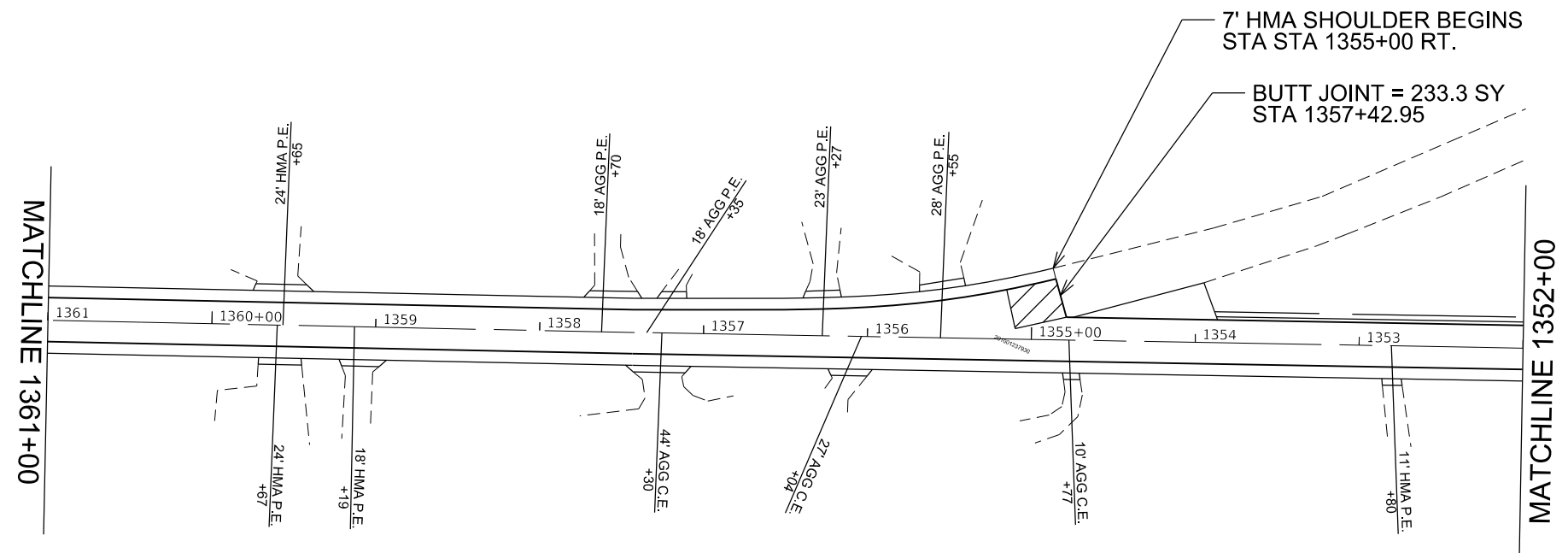
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|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALE\$ | DRAWN -    | REVISED - |
| PLOT DATE = \$DATE\$   | CHECKED -  | REVISED - |
|                        | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|             |          |       |         |
|-------------|----------|-------|---------|
| PLAN SHEETS |          |       |         |
| US 24       |          |       |         |
| SCALE:      | SHEET 12 | OF 17 | SHEETS  |
|             | STA.     |       | TO STA. |

|                           |            |        |              |           |
|---------------------------|------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4,       | (137X)RS-4 | FULTON | 73           | 50        |
| CONTRACT NO. 68A91        |            |        |              |           |
| ILLINOIS FED. AID PROJECT |            |        |              |           |



MODEL NUMBER  
FILE NUMBER

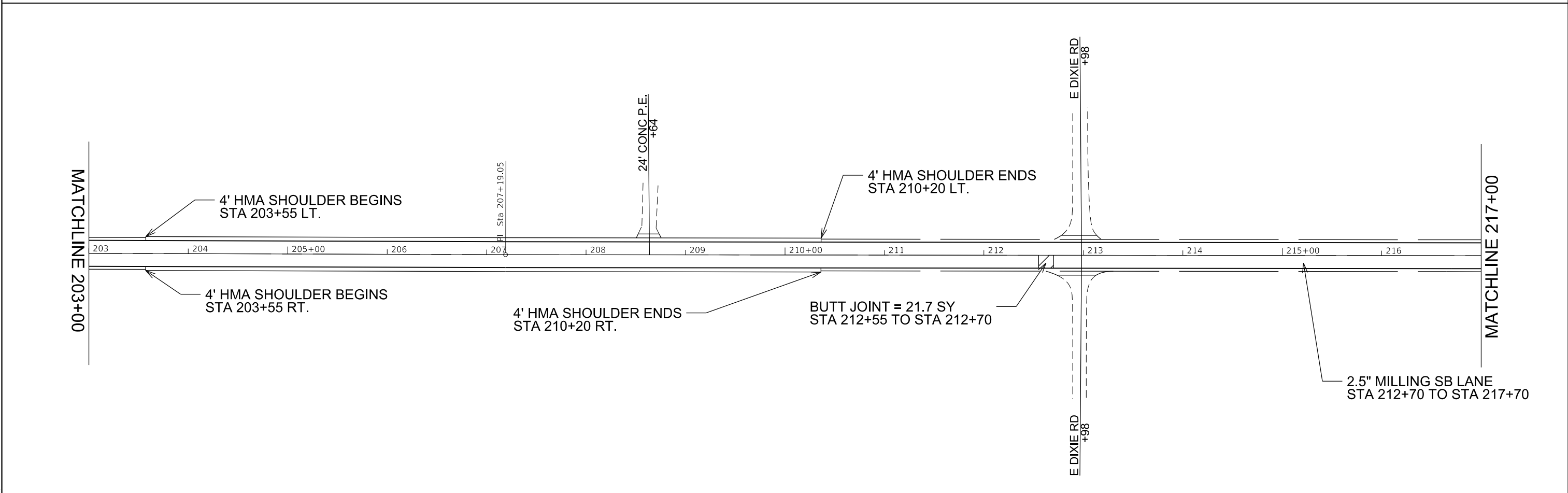
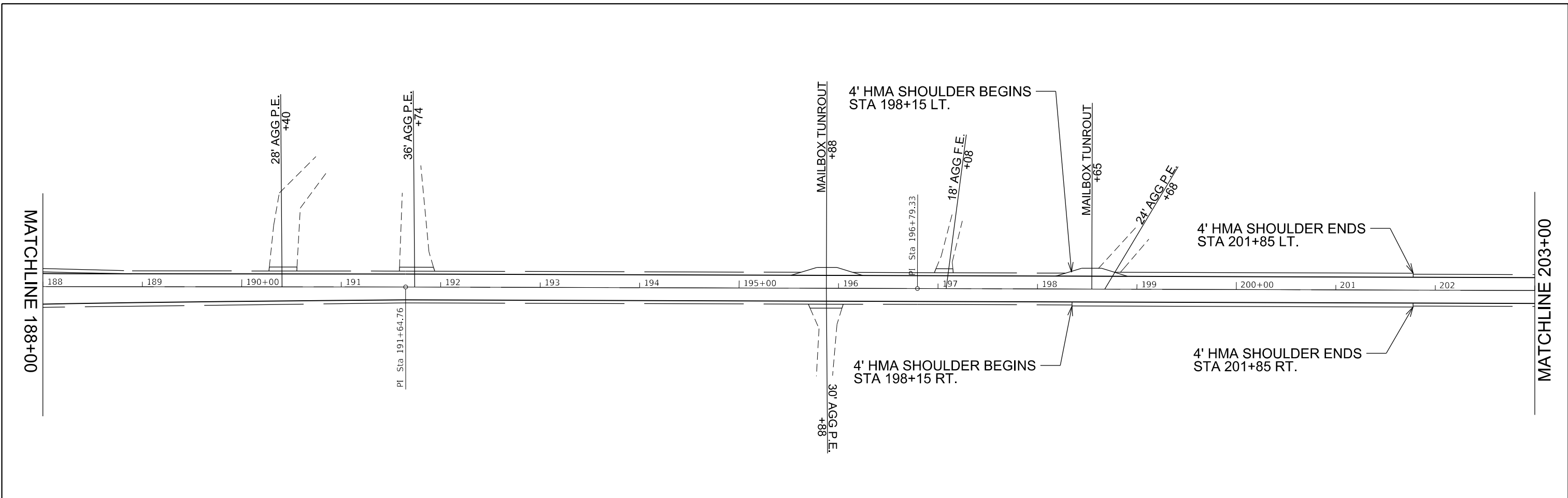
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| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PLAN SHEETS  
US 24**

SCALE: SHEET 13 OF 17 SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|----------------------------|--------|--------------------|-----------|
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 51        |
| ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |



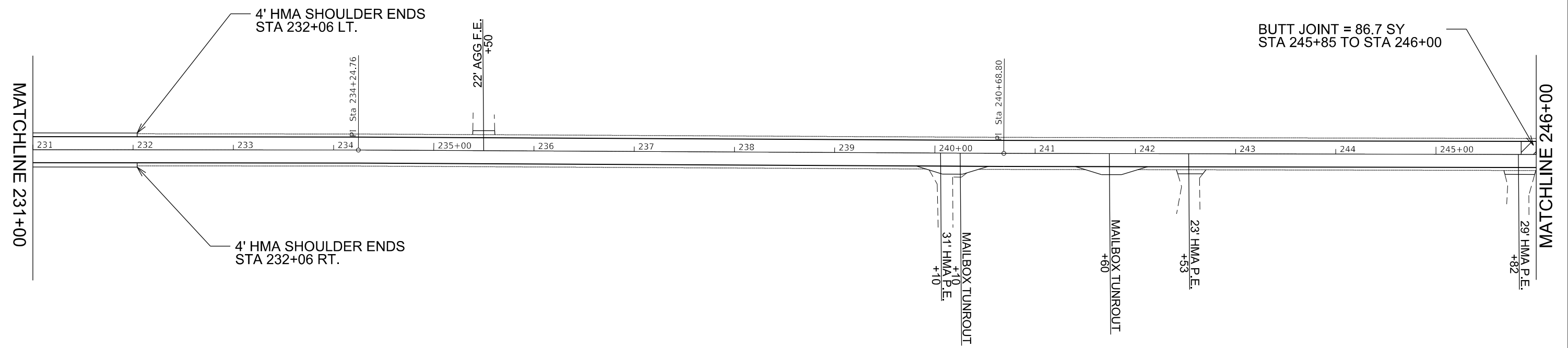
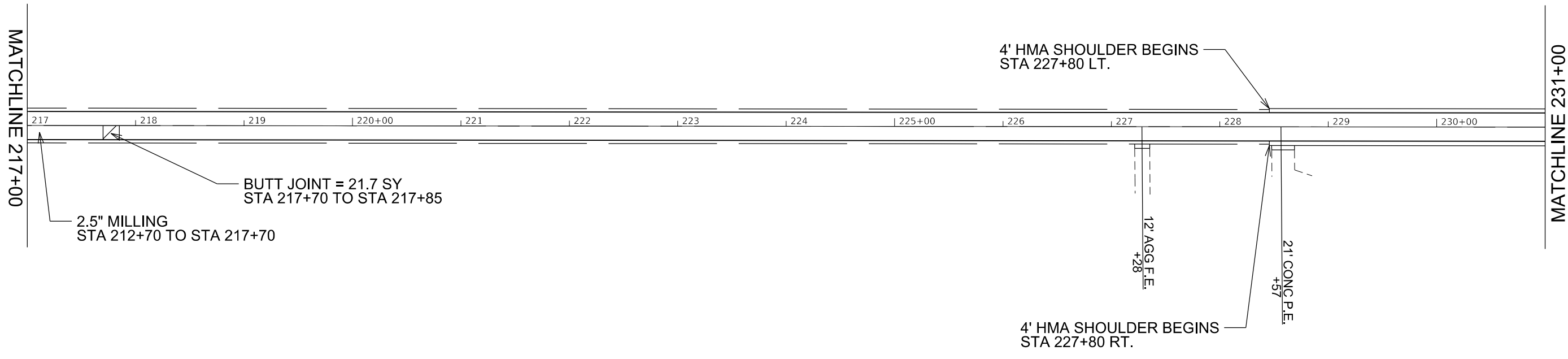
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FILE NAME: ST115

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|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|             |          |              |              |
|-------------|----------|--------------|--------------|
| PLAN SHEETS |          |              |              |
| IL 78 / 97  |          |              |              |
| SCALE:      | SHEET 14 | OF 17 SHEETS | STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 52        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



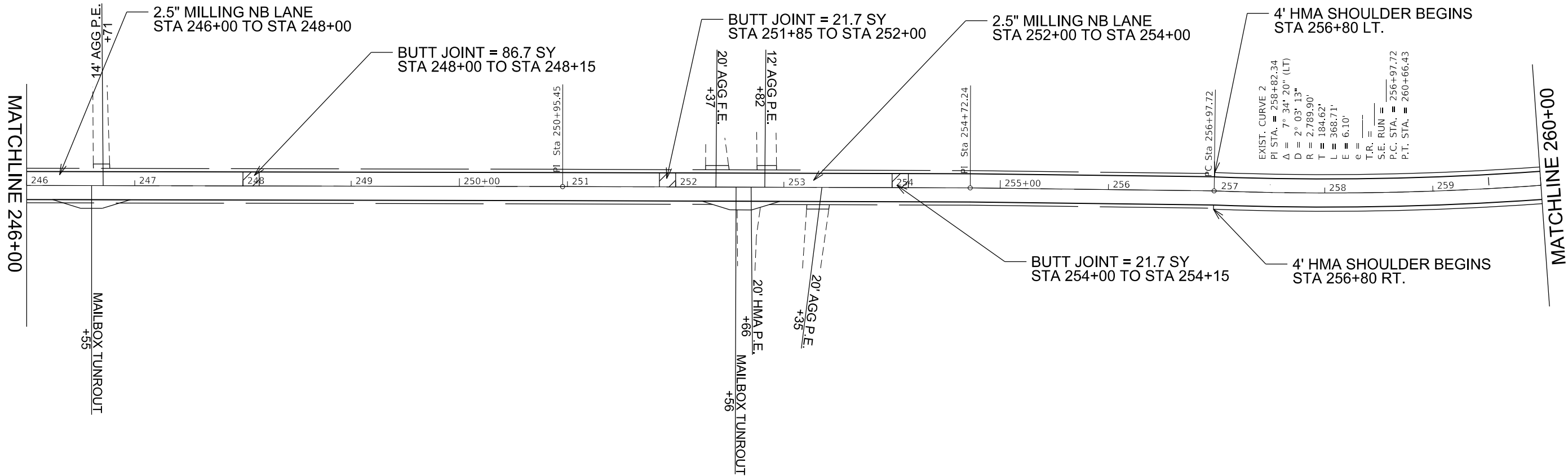
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|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

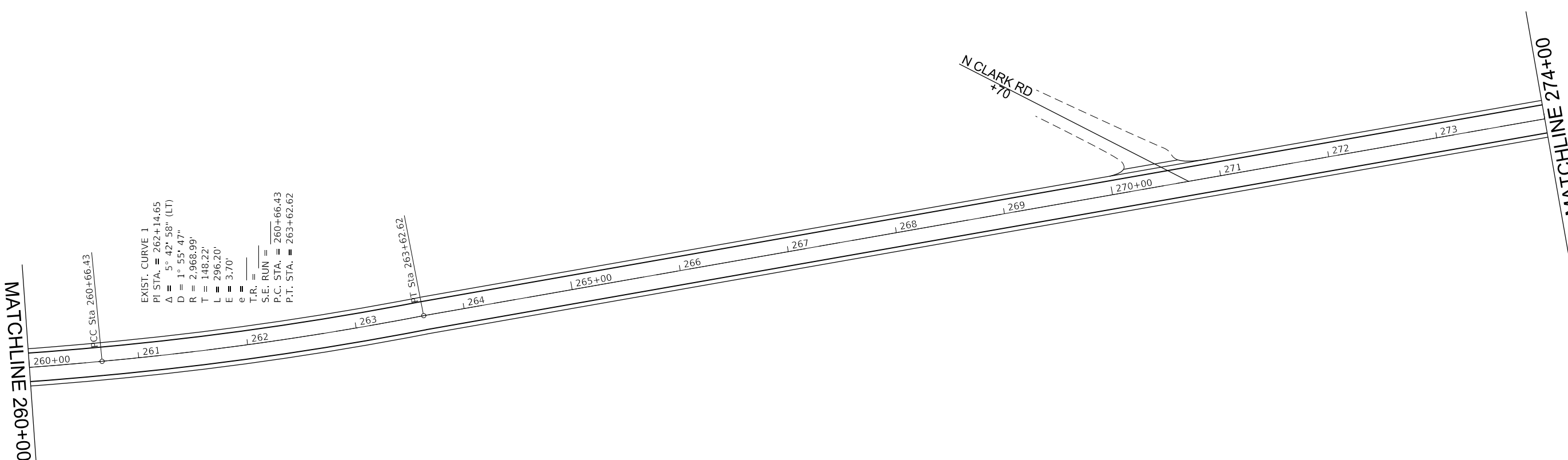
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|--------------------|------------------------------------|
| <b>PLAN SHEETS</b> |                                    |
| <b>IL 78 / 97</b>  |                                    |
| SCALE:             | SHEET 15 OF 17 SHEETS STA. TO STA. |

|                                |         |        |              |           |
|--------------------------------|---------|--------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 | FULTON  | 73     | 53           |           |
| CONTRACT NO. 68A91             |         |        |              |           |
| ILLINOIS FED. AID PROJECT      |         |        |              |           |



EXIST. CURVE 2  
 P.I. STA. = 258+82.34  
 $\Delta$  = 7° 34' 20" (LT)  
 $D$  = 2° 03' 13"  
 $R$  = 2,789.90'  
 $T$  = 184.62'  
 $L$  = 368.71'  
 $E$  = 6.10'  
 $e$  =  
 $T.R.$  =  
 $S.E. RUN$  = 256+97.72  
 $P.C. STA.$  = 256+66.43  
 $P.T. STA.$  = 260+66.43

EXIST. CURVE 1  
 P.I. STA. = 262+14.65  
 $\Delta$  = 5° 42' 58" (LT)  
 $D$  = 1° 55' 47"  
 $R$  = 2,968.99'  
 $T$  = 148.22'  
 $L$  = 296.20'  
 $E$  = 3.70'  
 $e$  =  
 $T.R.$  =  
 $S.E. RUN$  = 260+66.43  
 $P.C. STA.$  = 260+66.43  
 $P.T. STA.$  = 263+62.62



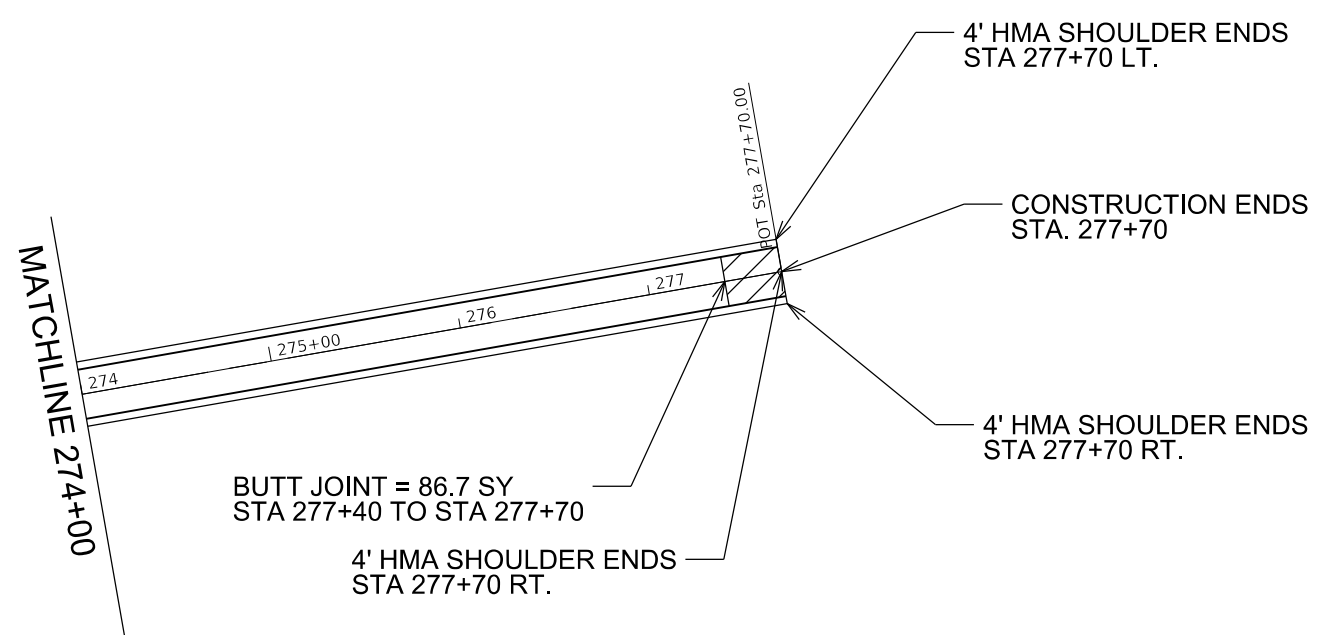
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|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALE\$ | DRAWN -    | REVISED - |
| PLOT DATE = \$DATE\$   | CHECKED -  | REVISED - |
|                        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|                    |                                    |
|--------------------|------------------------------------|
| <b>PLAN SHEETS</b> |                                    |
| <b>IL 78 / 97</b>  |                                    |
| SCALE:             | SHEET 16 OF 17 SHEETS STA. TO STA. |

|                           |                            |        |              |           |
|---------------------------|----------------------------|--------|--------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73           | 54        |
| CONTRACT NO. 68A91        |                            |        |              |           |
| ILLINOIS FED. AID PROJECT |                            |        |              |           |



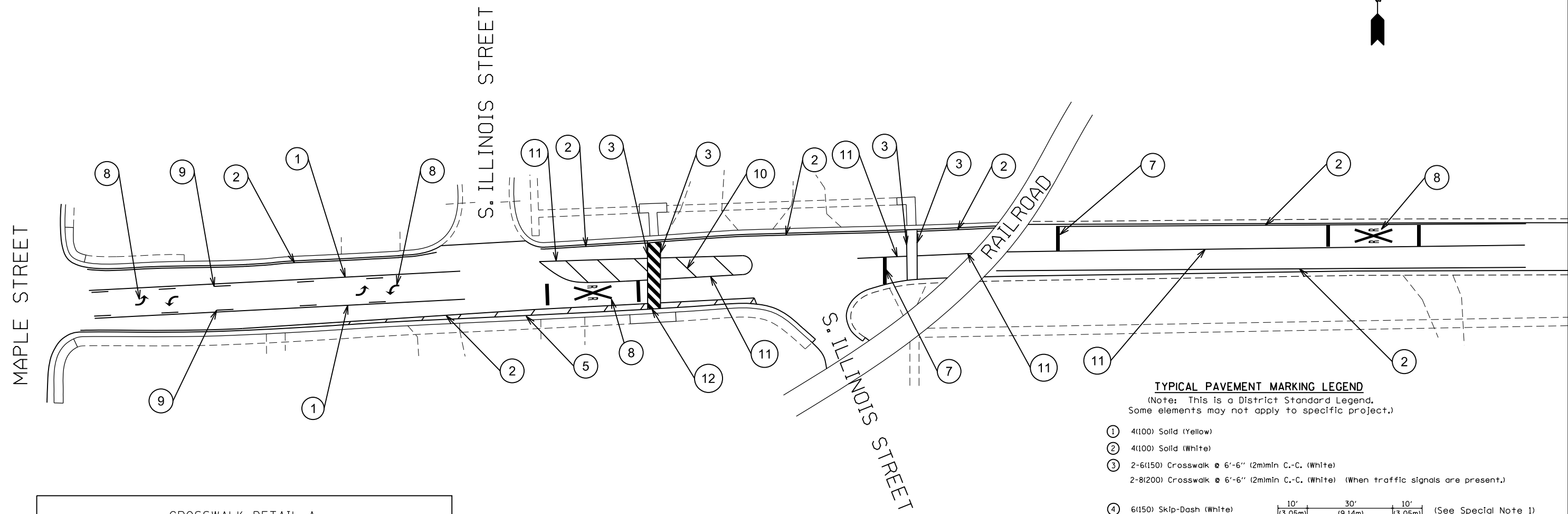
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|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                    |                                    |
|--------------------|------------------------------------|
| <b>PLAN SHEETS</b> |                                    |
| <b>IL 78 /97</b>   |                                    |
| SCALE:             | SHEET 17 OF 17 SHEETS STA. TO STA. |

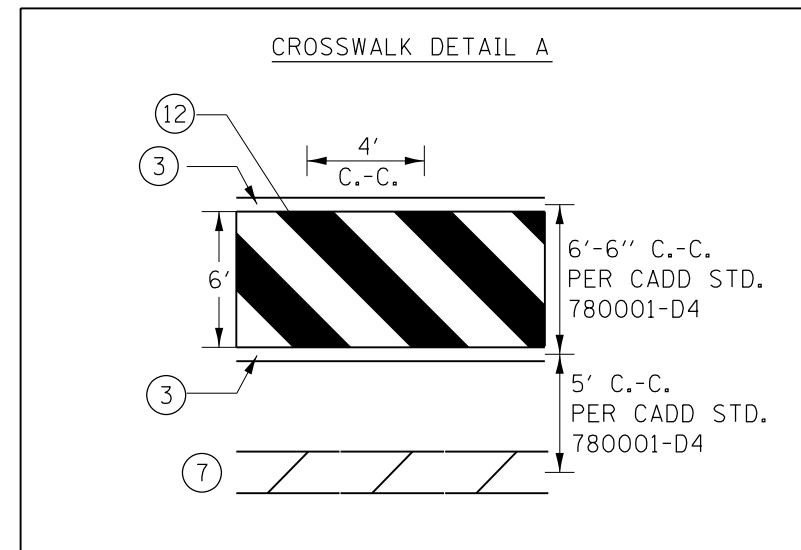
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|--------------------|----------------------------|--------|---------------------------|-----------|
| F.A.P. RTE.        | SECTION                    | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                        | 55        |
| CONTRACT NO. 68A91 |                            |        | ILLINOIS FED. AID PROJECT |           |



**TYPICAL PAVEMENT MARKING LEGEND**

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)  
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)
- ⑫ 24(600) Crosswalk (White) (see Crosswalk Detail A)



MODEL: 140DELENAME  
FILE NAME: 37123

|                        |             |           |
|------------------------|-------------|-----------|
| USER NAME = \$USERS    | DESIGNED -  | REVISED - |
| DRAWN -                | REVISIONS - | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -   | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -      | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING DETAIL  
US 24 IN LEWISTOWN**

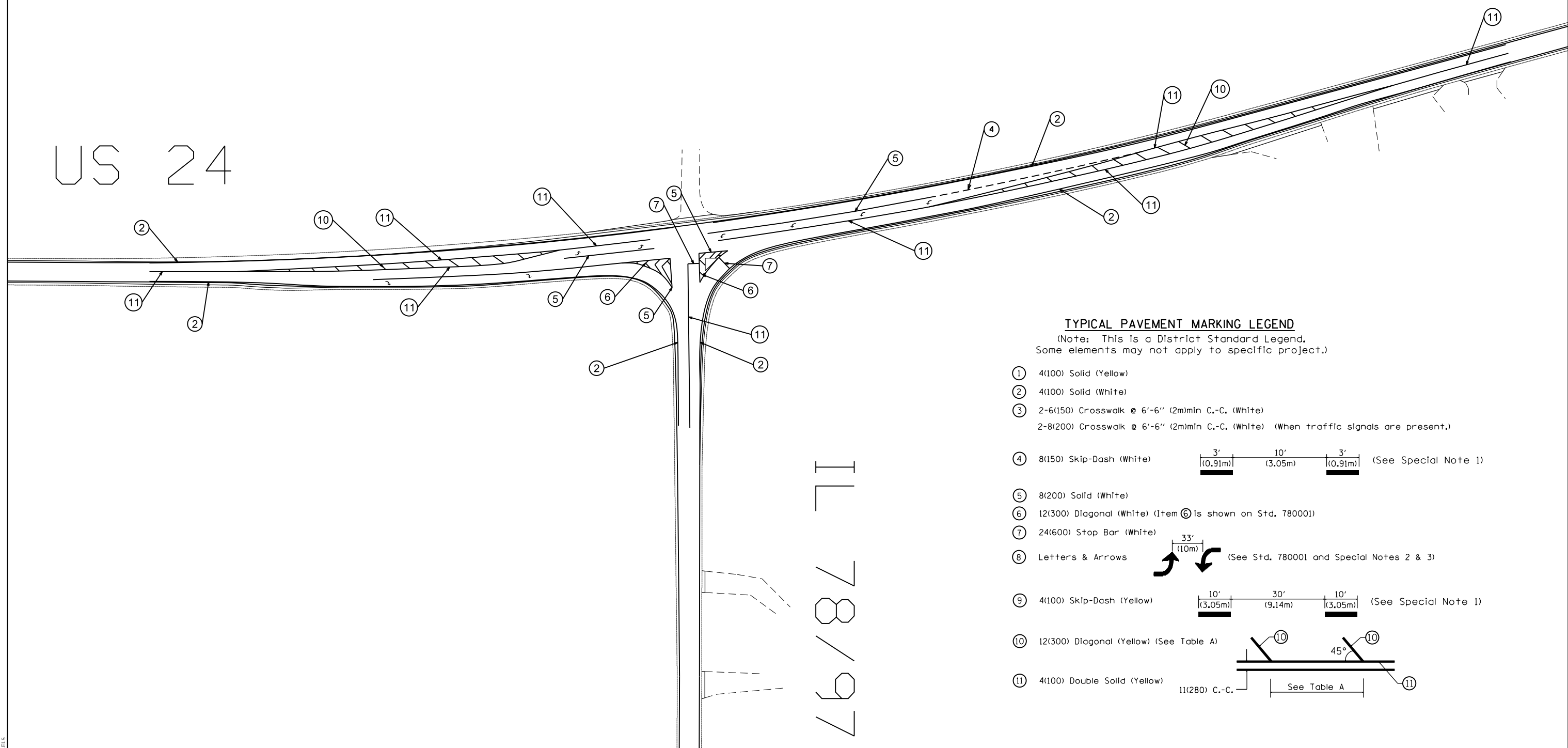
SCALE: SHEET 1 OF 2 SHEETS STA. TO STA.

|                                |         |        |                    |           |
|--------------------------------|---------|--------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                 | 56        |
| ILLINOIS FED. AID PROJECT      |         |        | CONTRACT NO. 68A91 |           |





US 24



**TYPICAL PAVEMENT MARKING LEGEND**

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)  
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 8(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) 45°
- ⑪ 4(100) Double Solid (Yellow) 11(280) C.-C. See Table A

MODEL: \$MODELNAME\$  
FILE: \$NAME\$. \$FILE\$

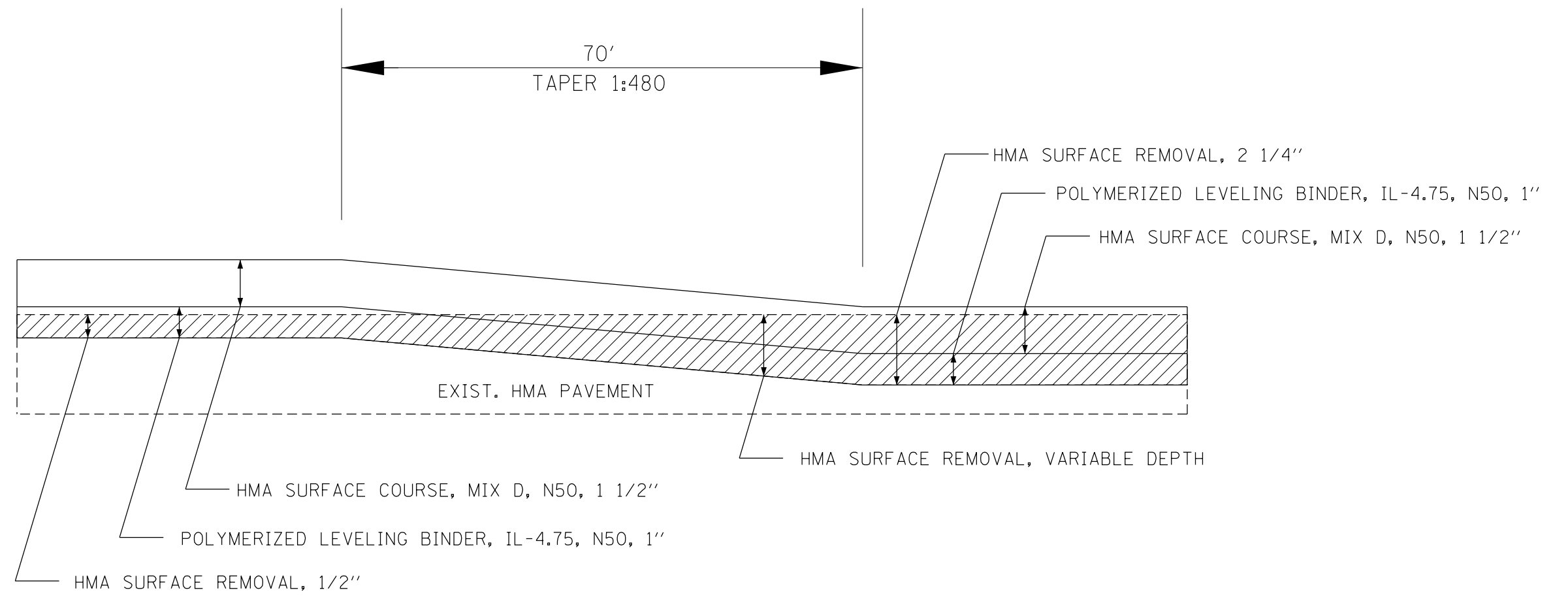
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| PLOT SCALE = \$SCALE\$ | DRAWN -    | REVISED - |
| PLOT DATE = \$DATE\$   | CHECKED -  | REVISED - |
|                        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING DETAIL  
US 24 & IL78 /97 INTERSECTION**

SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.

|                                |         |                  |              |           |
|--------------------------------|---------|------------------|--------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY           | TOTAL SHEETS | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON           | 73           | 57        |
| CONTRACT NO. 68A91             |         |                  |              |           |
| ILLINOIS                       |         | FED. AID PROJECT |              |           |



THIS DETAIL IS TO BE APPLIED AT THE FOLLOWING LOCATIONS:

US 24  
1674+56 TO 1675+26

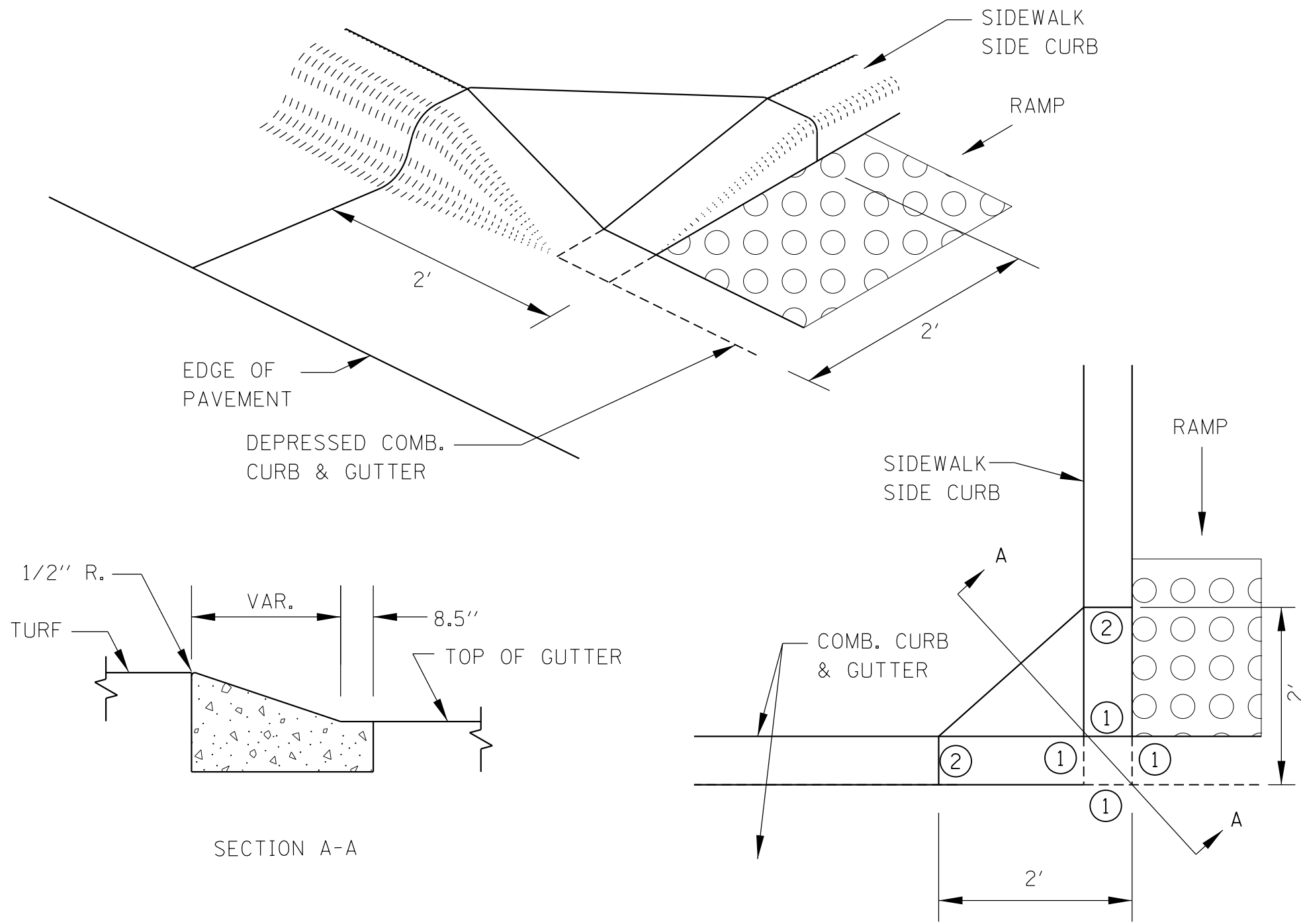
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|                        |            |           |
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| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                                       |         |      |         |
|---------------------------------------|---------|------|---------|
| <b>HMA PAVEMENT TRANSITION DETAIL</b> |         |      |         |
| SCALE:                                | SHEET 1 | OF 1 | SHEETS  |
|                                       | STA.    |      | TO STA. |

|                                |         |          |                    |           |
|--------------------------------|---------|----------|--------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY   | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON   | 73                 | 58        |
|                                |         |          | CONTRACT NO. 68A91 |           |
|                                |         | ILLINOIS | FED. AID PROJECT   |           |



NOTE: APPROACH NOSE TO BE PAID FOR AS P.C.C. SIDEWALK, 4"

### APPROACH NOSE DETAIL

MODEL: 140DELMAMES  
FILE NAME: 31123

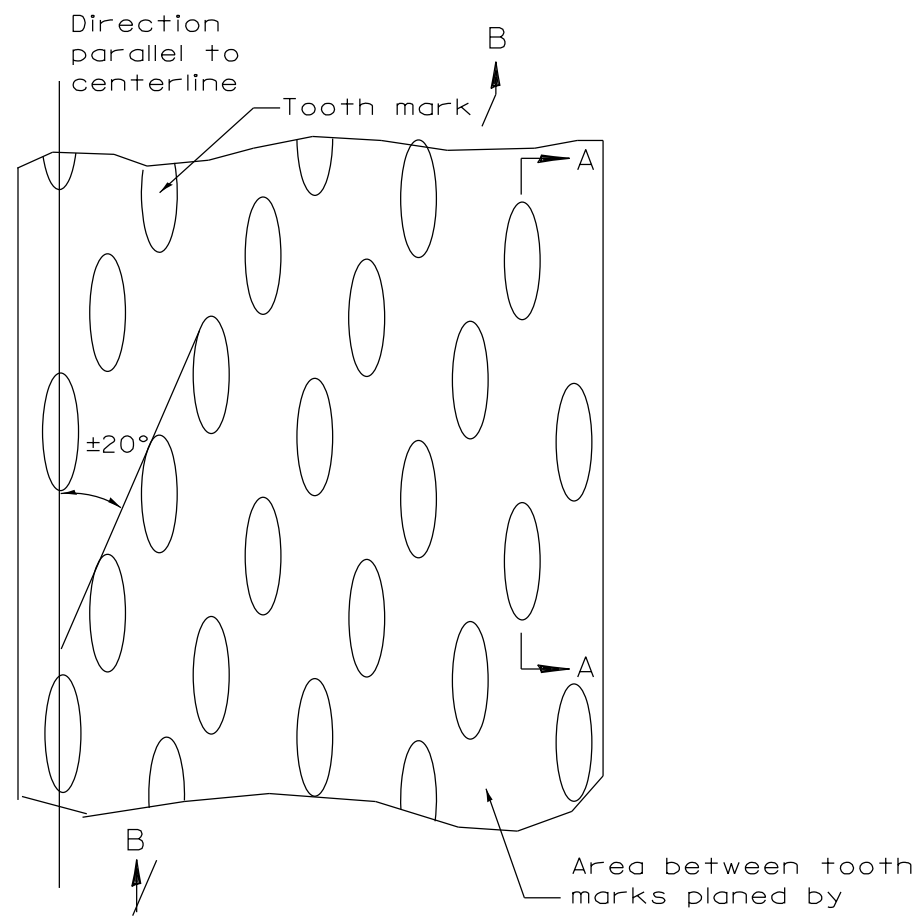
|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ADA APPROACH NOSE DETAIL

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

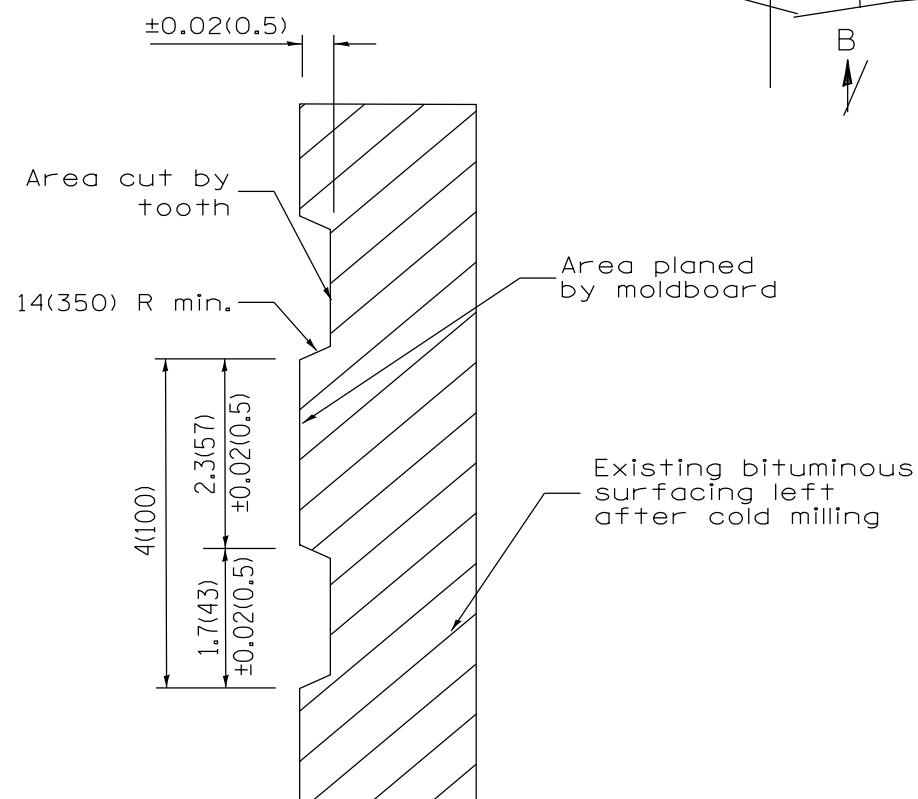
|                    |                            |        |                           |           |
|--------------------|----------------------------|--------|---------------------------|-----------|
| F.A.P. RTE.        | SECTION                    | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                        | 59        |
| CONTRACT NO. 68A91 |                            |        | ILLINOIS FED. AID PROJECT |           |



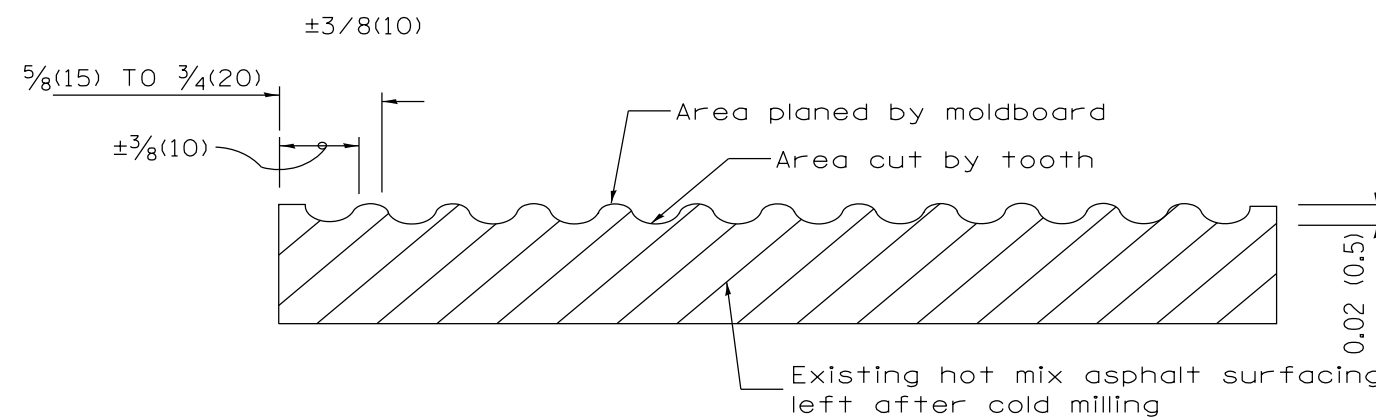
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED  
PERPENDICULAR TO CENTERLINE

All dimensions are in inches (millimeters) unless otherwise noted.

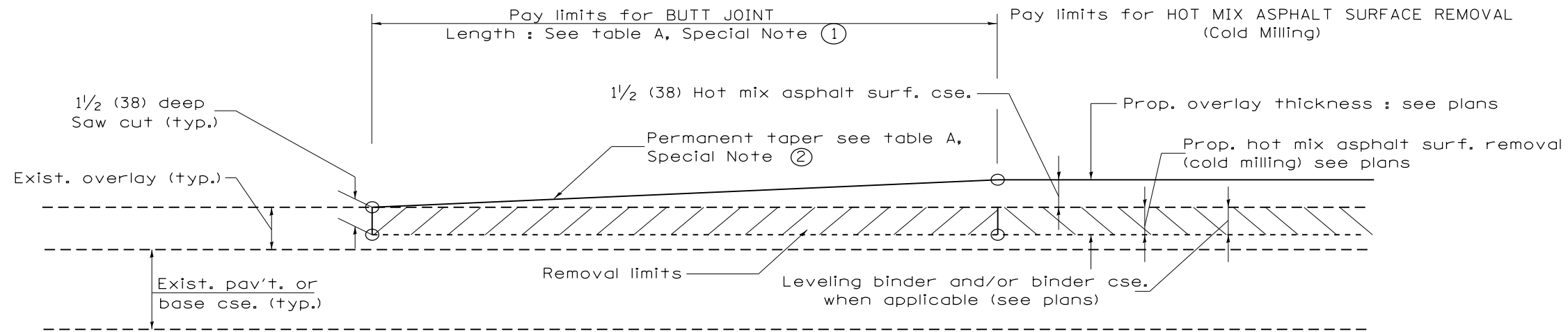
MODEL: 440001-D4  
FILE NAME: 44001.D4

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|  |      |                     |              |
|--|------|---------------------|--------------|
| HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING) |      |                     |              |
| NOT TO SCALE                                   |      |                     |              |
| SCALE:   |      | CADD STD. 440001-D4 |              |
| SHEET 1  | OF 8 | SHEETS              | STA. TO STA. |

|                                |         |        |                           |           |
|--------------------------------|---------|--------|---------------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                        | 60        |
| CONTRACT NO. 68A91             |         |        | ILLINOIS FED. AID PROJECT |           |



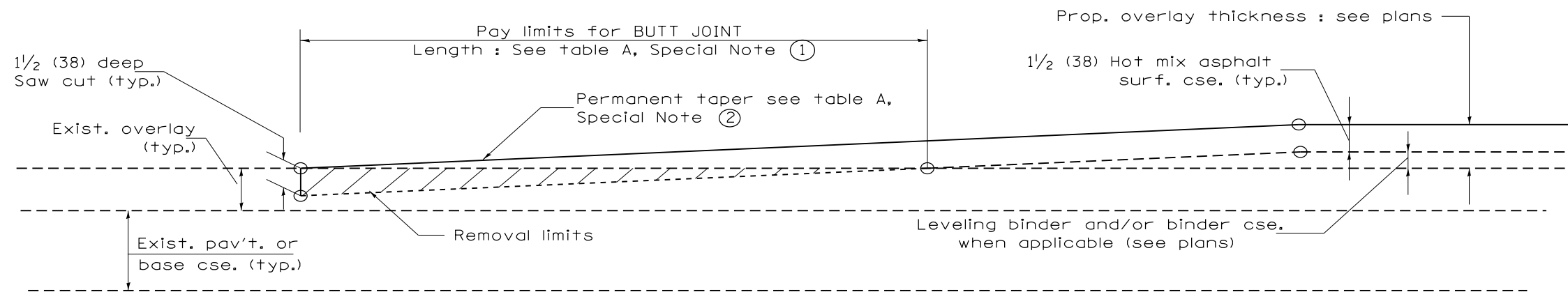
**CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)**

**TABLE A  
(LENGTHS AND TAPER RATES)**

| SPECIAL NOTE NUMBER | ELEMENT                   | MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS | ALL OTHERS |
|---------------------|---------------------------|---|------------|
| ①                   | LENGTH OF BUTT JOINT      | 60'(18.0 m)                               | 30'(9.0 m) |
| ②                   | PERMANENT TAPER RATE      | 1:480                                     | 1:240      |
| ③                   | TEMPORARY RAMP TAPER RATE | 1:80                                      | 1:40       |
| ④                   | TEMPORARY RAMP LENGTH     | 10'(3.0 m)                                | 5'(1.5 m)  |
| ⑤                   | LENGTH OF BUTT JOINT      | 10'(3.0 m)                                | 10'(3.0 m) |

**GENERAL NOTES**

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.



**CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)**

All dimensions are in inches (millimeters) unless otherwise noted.

MODEL: 440DELMAMES  
FILE NAME: 440E15

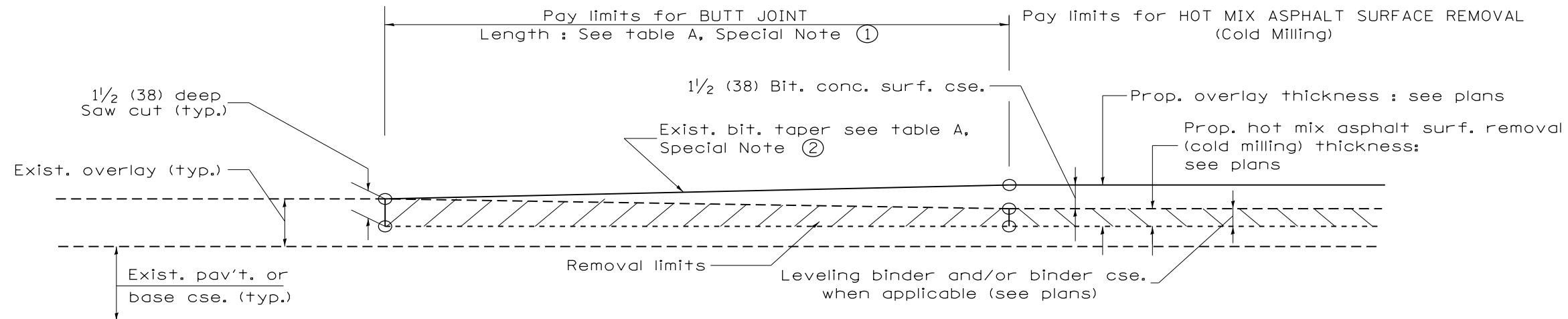
|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN -    | REVISED - |
| PLOT DATE = \$DATES   | CHECKED -  | REVISED - |
|                       | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

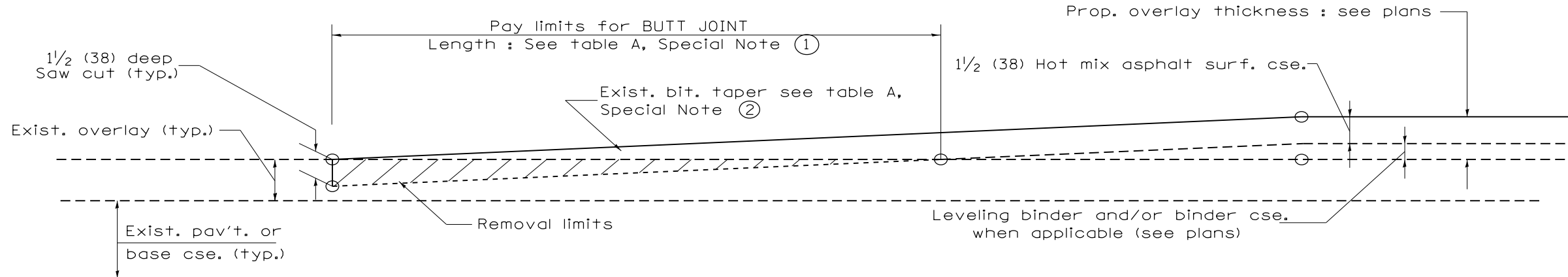
**BUTT JOINTS**  
NOT TO SCALE  
SCALE: SHEET 2 OF 8 SHEETS STA. TO STA.

SHT. 1 OF 3  
CADD STD. 406101-D4

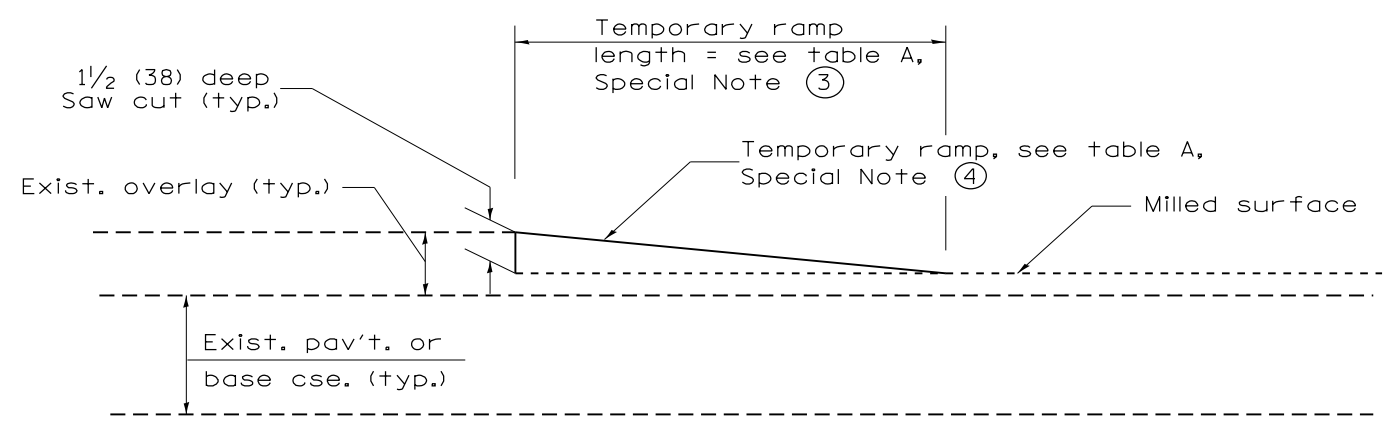
|                           |                            |        |                    |           |
|---------------------------|----------------------------|--------|--------------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 61        |
| ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |



**CASE 3 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)  
TIE-IN TO EXISTING BITUMINOUS TAPER**



**CASE 4 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)  
TIE-IN TO EXISTING BITUMINOUS TAPER**



**DETAIL TEMPORARY RAMP**

All dimensions are in inches (millimeters) unless otherwise noted.

MODEL: 440DELMAMES  
FILE NAME: 37113

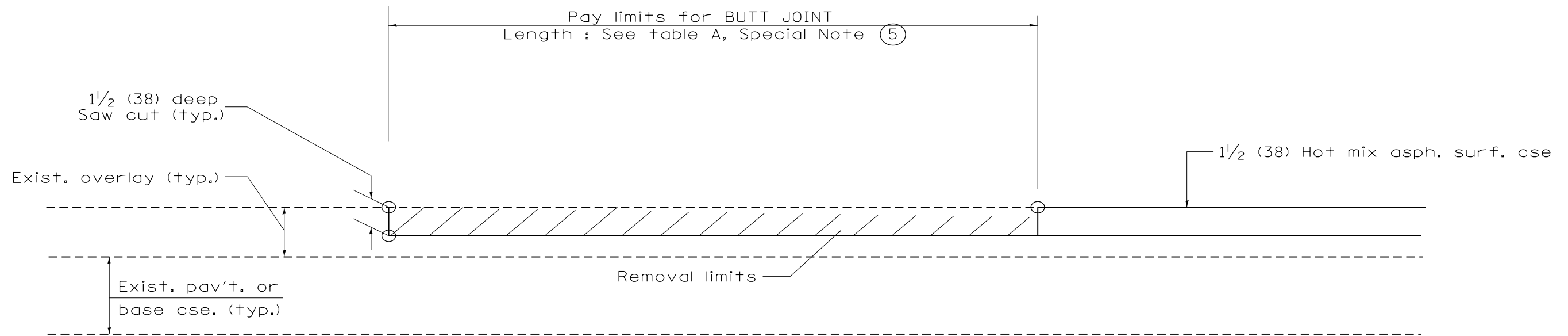
|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINTS**  
NOT TO SCALE  
SCALE: SHEET 3 OF 8 SHEETS STA. TO STA.

SHT. 2 OF 3  
CADD STD. 406101-D4

|                                |         |        |                           |           |
|--------------------------------|---------|--------|---------------------------|-----------|
| F.A.P. RTE.                    | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4 |         | FULTON | 73                        | 62        |
| CONTRACT NO. 68A91             |         |        | ILLINOIS FED. AID PROJECT |           |



CASE 5 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)  
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters)  
 unless otherwise noted.

MODEL: 440DELMNAMES  
 FILE NAME: 441E13

|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

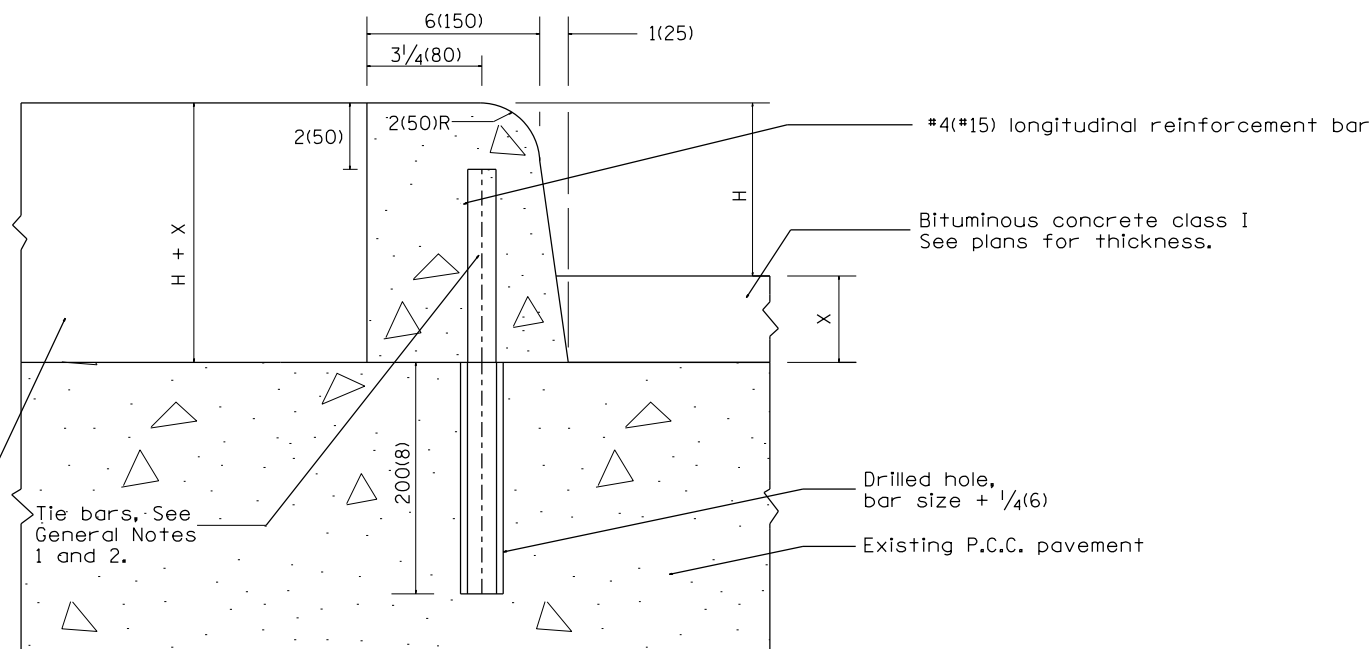
**BUTT JOINTS**  
 NOT TO SCALE

SHT. 3 OF 3  
 CADD STD. 406101-D4

SCALE: SHEET 4 OF 8 SHEETS STA. TO STA.

|                           |                            |        |                    |           |
|---------------------------|----------------------------|--------|--------------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 63        |
| ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |

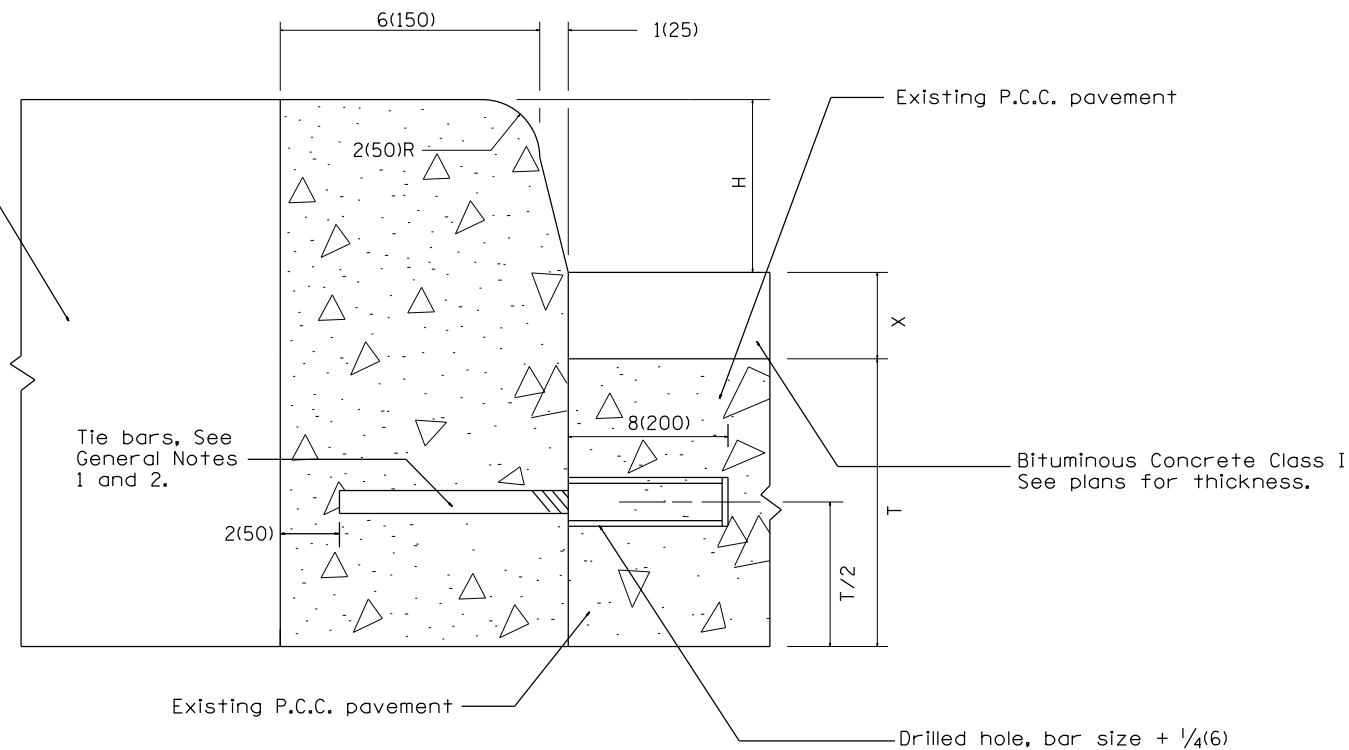
TYPICAL CURB INSTALLATION  
ON TOP OF EXISTING PAVEMENT



LEGEND

- H = curb height: 6(150) or 9(225).  
(see plans for curb height)
- X = thickness of bituminous resurfacing.  
(see plans for thickness)
- T = thickness of existing P.C.C. pavement.  
(see plans for thickness)

Backfill as shown  
elsewhere on plans



TYPICAL CURB INSTALLATION  
AT EDGE OF EXISTING PAVEMENT

GENERAL NOTES:

1. Tie bars shall be #6(#20) at 24(600mm) center in accordance with details for longitudinal construction joint shown in Standard 420001.
2. A minimum clearance of 2(50mm) between the end of the tie bar and the back or top of the curb shall be maintained.
3. Drainage openings 1 sq. ft.(0.1m<sup>2</sup>) in area shall be cut through existing concrete within median along curb at 20'(6.1m) centers and at low points.
4. CONCRETE CURB, TYPE B (MODIFIED) shall be constructed in accordance with the applicable portions of Standards 420001 and 606001, and in accordance with the applicable portions of Article 420.10 and Section 606 of the Standard Specifications except as noted herein.

All dimensions are in inches (millimeters)  
unless otherwise noted.

|          |   |      |
|----------|---|------|
| 01-01-97 | RENUM. A-7.10, NEW REVISION BOX, NEW FORMAT,    | T.P. |
|          | REVISED NOTES, ELIMINATED EXPANSION TIE ANCHORS |      |
| 03-31-99 | REMOVED EPOXY COAT REQUIREMENT                  | J.A. |
| 10-16-06 | REVISED TO 2007 SPEC.                           | M.A. |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

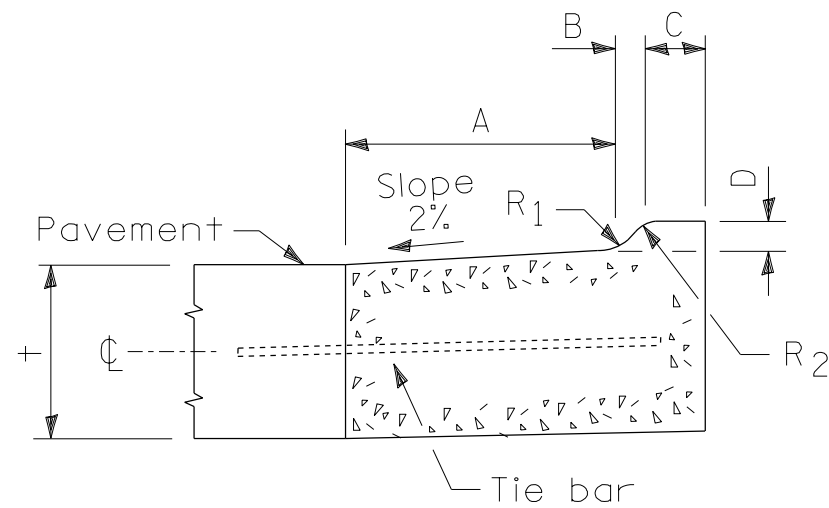
CONCRETE CURB, TYPE B (MODIFIED)

NOT TO SCALE

CADD STD. 606001-D4

| F.A.P. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|---------|--------|--------------------|-----------|
| VAR (137, 129)RS-4, (137X)RS-4                |         | FULTON | 73                 | 64        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 68A91 |           |





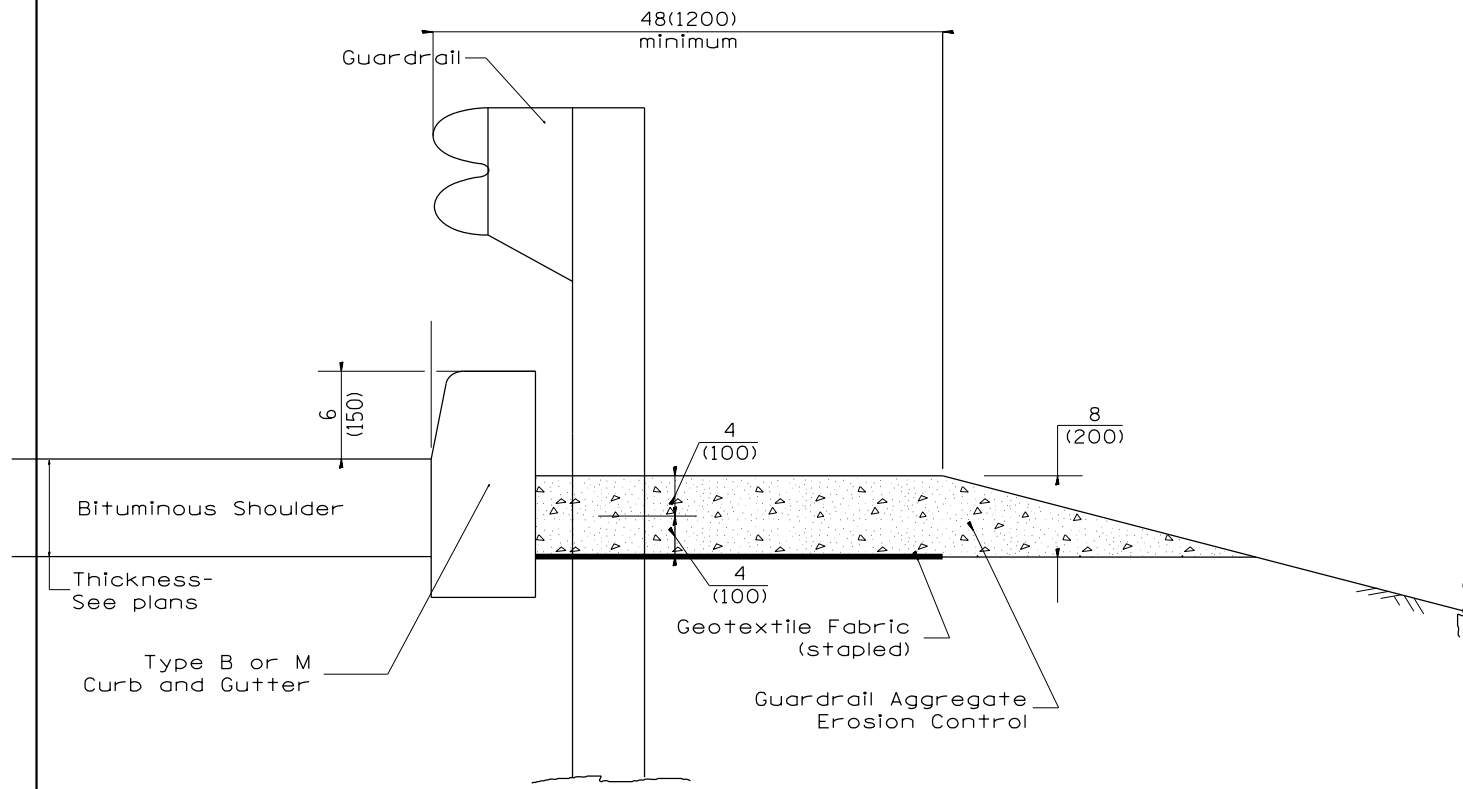
M-2.06(M-5.15) and M-2.12 (M-5.30)

| TABLE OF DIMENSIONS<br>MOUNTABLE CURB |             |           |            |           |                |                |
|---------------------------------------|-------------|-----------|------------|-----------|----------------|----------------|
| TYPE                                  | A           | B         | C          | D         | R <sub>1</sub> | R <sub>2</sub> |
| M-2.06<br>(M-5.15)                    | 6<br>(150)  | 2<br>(50) | 4<br>(100) | 2<br>(50) | 3<br>(75)      | 2<br>(50)      |
| M-2.12<br>(M-5.30)                    | 12<br>(300) | 2<br>(50) | 4<br>(100) | 2<br>(50) | 3<br>(75)      | 2<br>(50)      |

t = Thickness of pavement.

Note: This special detail of M-2.06 (M-5.15) and M-2.12 (M-5.30) shall replace the detail shown on State Standard 606001.

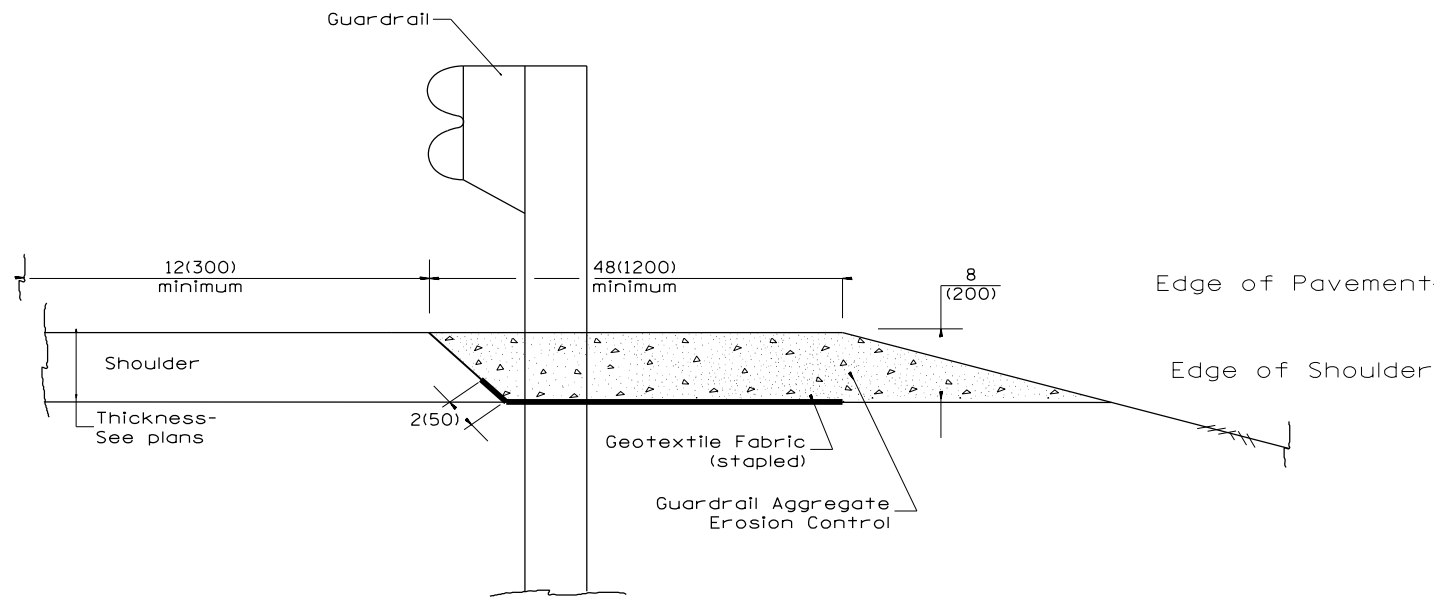
All dimensions are in inches (millimeters) unless otherwise noted.



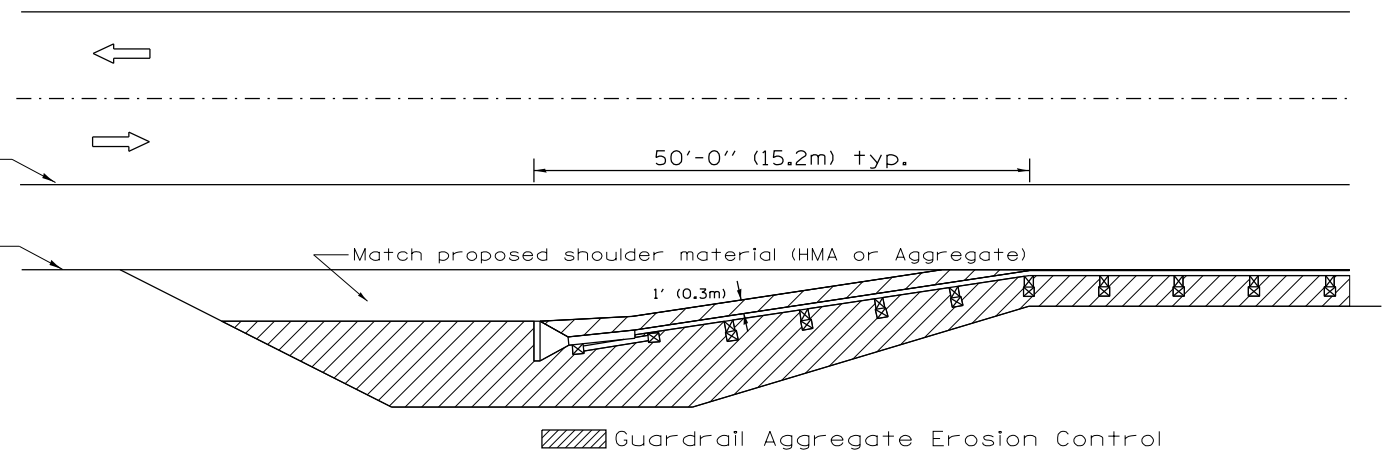
**TYPICAL SECTION WITH EROSION CONTROL CURB**

**GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL**

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
  - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
  - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



**TYPICAL SECTION WITHOUT EROSION CONTROL CURB**



All dimensions are in inches (millimeters) unless otherwise noted.

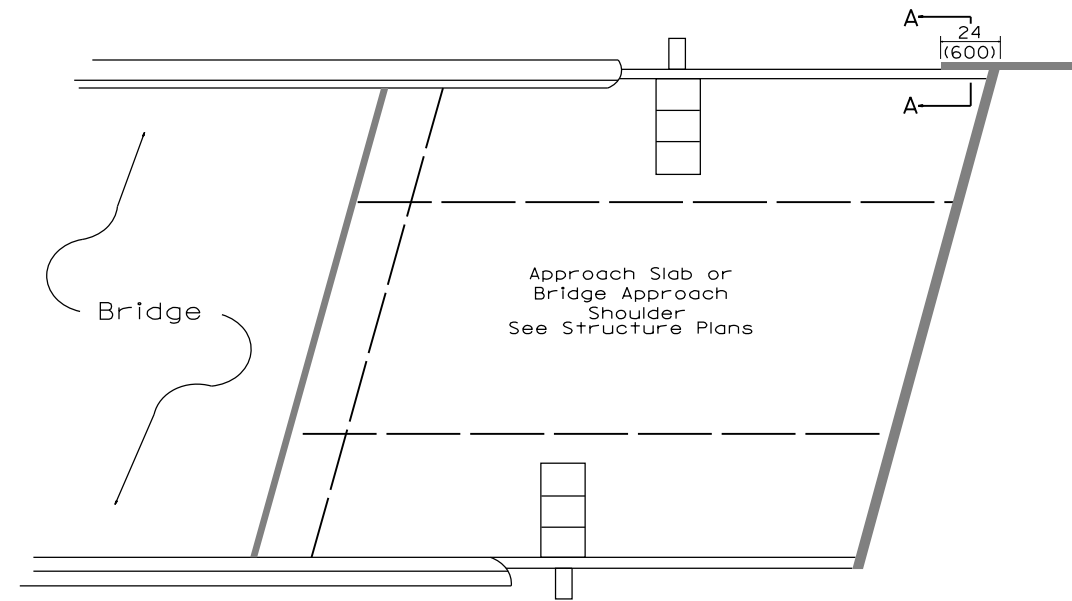
MODEL: SH02ELNAMES  
FILE NAME: ST115

|                       |            |           |
|-----------------------|------------|-----------|
| USER NAME = \$USERS   | DESIGNED - | REVISED - |
|                       | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALES | CHECKED -  | REVISED - |
| PLOT DATE = \$DATES   | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

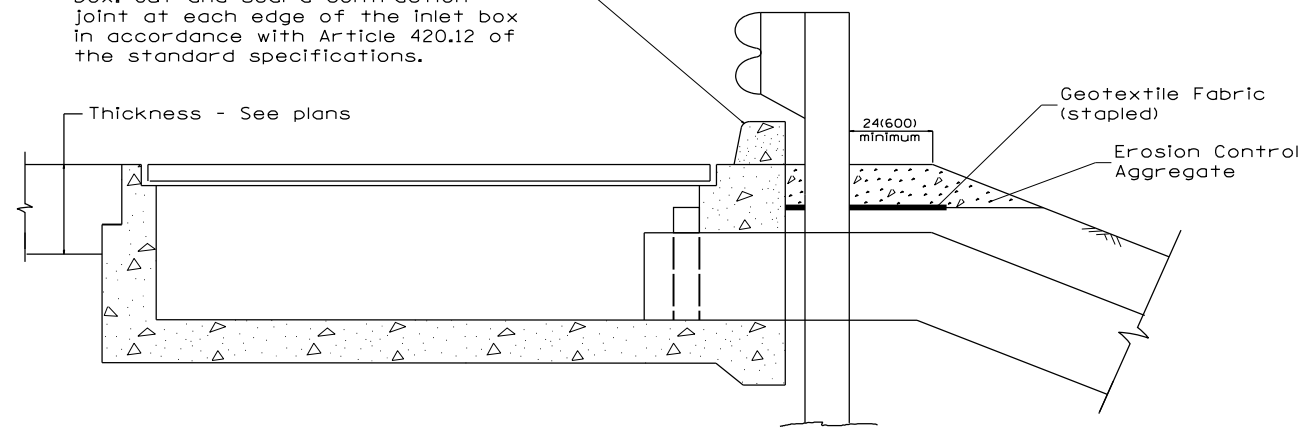
|   |                     |                                    |         |
|---|---------------------|------------------------------------|---------|
| <b>GUARDRAIL EROSION CONTROL TREATMENTS</b> |                     |                                    |         |
| NOT TO SCALE                                |                     | SHT. 1 OF 2<br>CADD STD. 630101-D4 |         |
| SCALE:                                      | SHEET 5 OF 8 SHEETS | STA.                               | TO STA. |

|                           |                            |        |                    |           |
|---------------------------|----------------------------|--------|--------------------|-----------|
| F.A.P. RTE.               | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR                       | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 66        |
| ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |

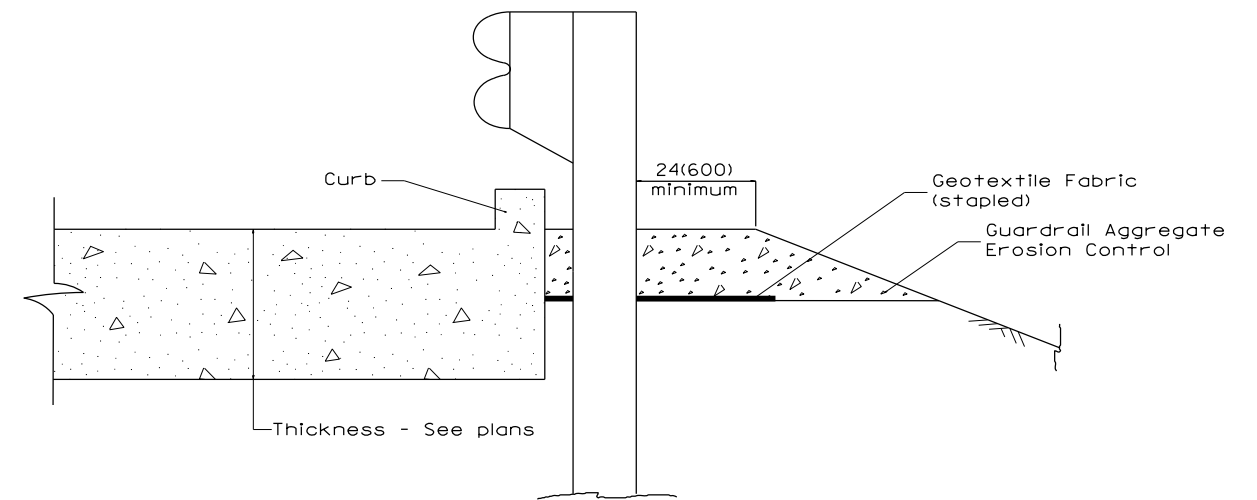


PLAN VIEW  
APPROACH SLAB OR BRIDGE APPROACH SHOULDER  
 (STANDARD 609001 or 609006)

Type B or M curb.  
 Install an 8' long #4 epoxy coated reinforcement bar across the inlet box. Cut and seal a contraction joint at each edge of the inlet box in accordance with Article 420.12 of the standard specifications.



TYPICAL SECTION  
AT INLETS TYPE E & F (STANDARD 610001)



SECTION A-A  
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

MODEL: 630101.DWG  
 FILE NAME: 630101.DWG

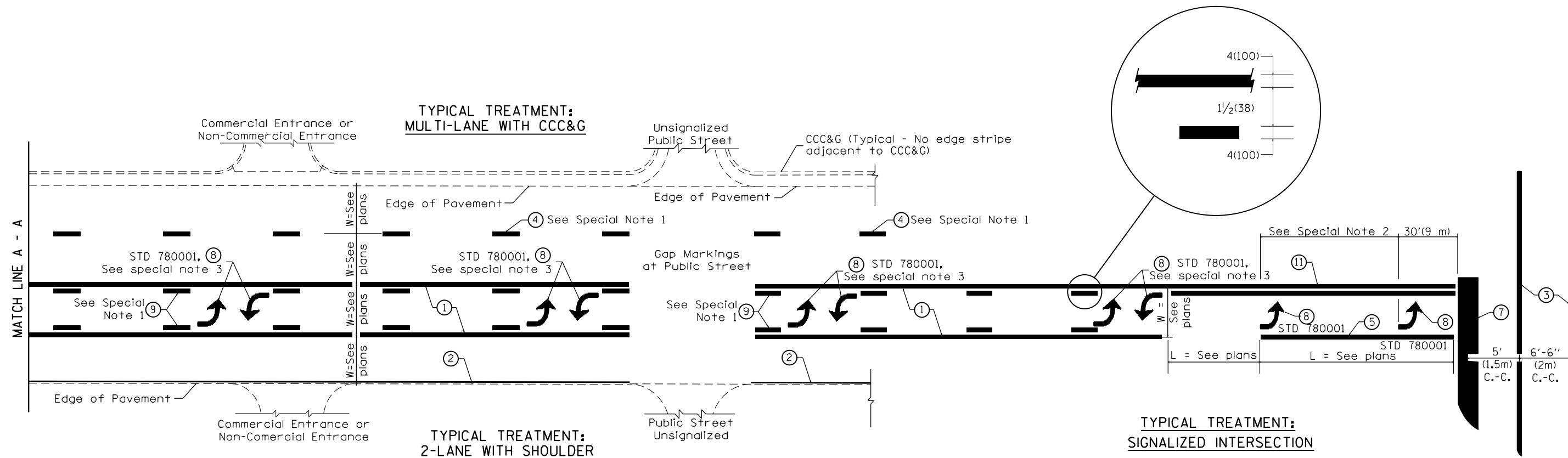
|                        |            |           |
|------------------------|------------|-----------|
| USER NAME = \$USERS    | DESIGNED - | REVISED - |
|                        | DRAWN -    | REVISED - |
| PLOT SCALE = \$SCALE\$ | CHECKED -  | REVISED - |
| PLOT DATE = \$DATE\$   | DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

|                                      |  |                     |              |
|--------------------------------------|--|---------------------|--------------|
| GUARDRAIL EROSION CONTROL TREATMENTS |  |                     |              |
| NOT TO SCALE                         |  |                     |              |
| SCALE:                               |  | SHEET 6 OF 8 SHEETS | STA. TO STA. |

|                    |                            |        |                           |           |
|--------------------|----------------------------|--------|---------------------------|-----------|
| F.A.P. RTE.        | SECTION                    | COUNTY | TOTAL SHEETS              | SHEET NO. |
| VAR                | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                        | 67        |
| CONTRACT NO. 68A91 |                            |        | ILLINOIS FED. AID PROJECT |           |

SHT. 2 OF 2  
 CADD STD. 630101-D4



**FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION**

**TYPICAL PAVEMENT MARKING LEGEND**

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)  
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

**SPECIAL NOTES**

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
  - A. A minimum of two (2) arrows is required.
  - B. The maximum spacing between arrows is 80' (24 m).
  - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
  - A. A minimum of two (2) arrow pairs is required.
  - B. The maximum spacing between arrow pairs is 200' (61 m).
  - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
  - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

**GENERAL NOTES**

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
  - Through Arrow= 14.8 sq. ft.
  - Large Left or Right Arrow= 21.9 sq. ft.
  - 2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
  - Wrong Way Arrow= 29.5 sq. ft.
  - Railroad Crossing Symbol= 69.8 sq. ft.
 (For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

|          |                                  |      |          |                       |      |
|----------|----------------------------------|------|----------|-----------------------|------|
| 01-01-97 | RENUM. F-8.03, NEW REVISION BOX  | T.P. | 10-16-06 | REVISED TO 2007 SPEC. |      |
| 02-07-97 | ADD BI DIRECTIONAL DIMENSION     | J.A. | 2/29/16  | ADDED GROOVING AREAS  | R.D. |
| 10-97    | CORRECT BI DIRECTIONAL DIMENSION | J.A. |          |                       |      |
| 08-02    | ADD CROSSWALK DMNS. WITH T.S.    | M.A. |          |                       |      |

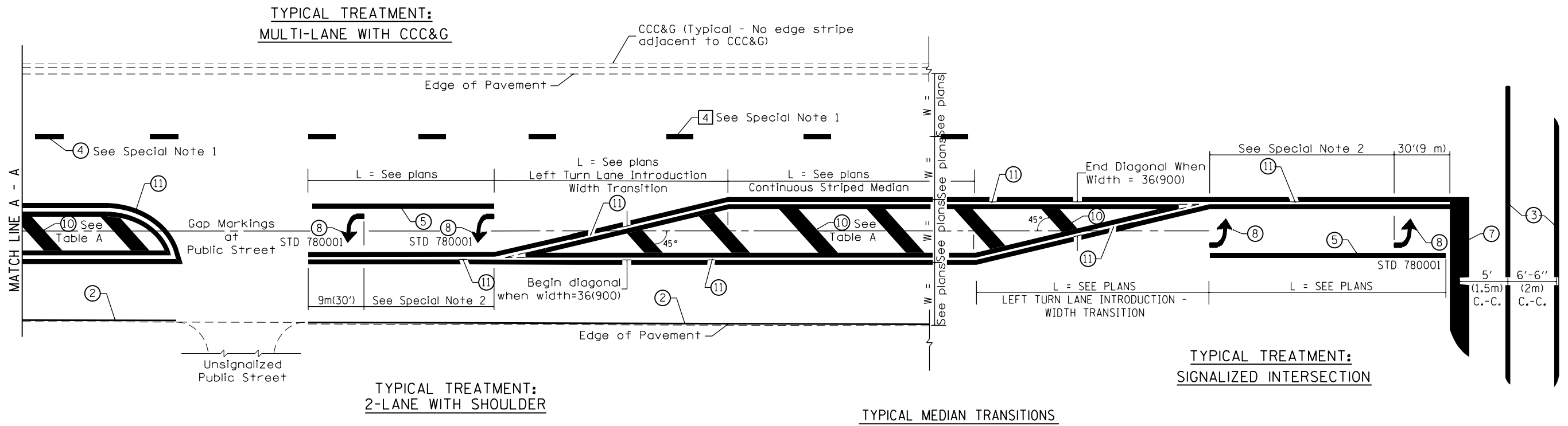
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

**TYPICAL PAVEMENT MARKINGS**

SHT. 1 OF 2  
CADD STD. 780001-D4

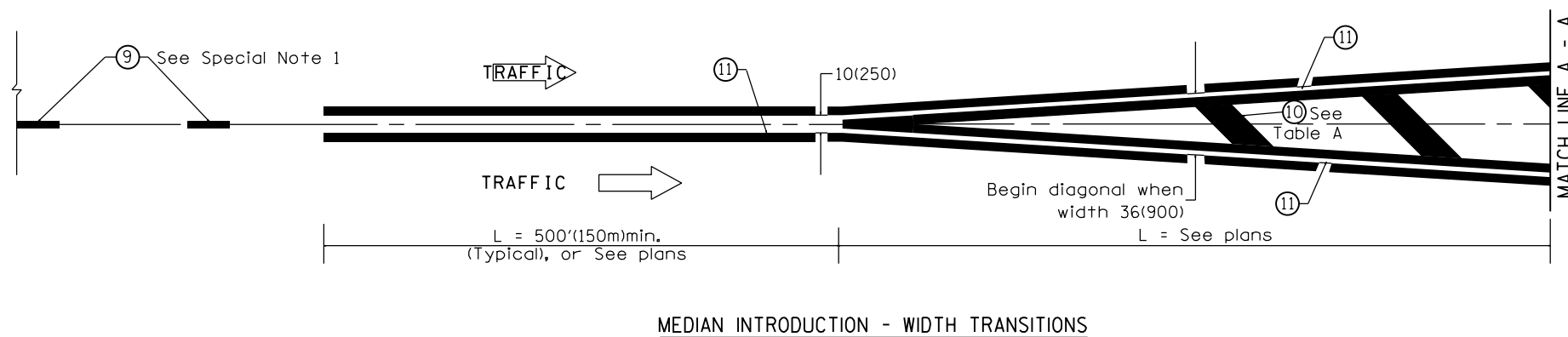
|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.P. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4                |         | FULTON | 73                 | 68        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 68A91 |           |



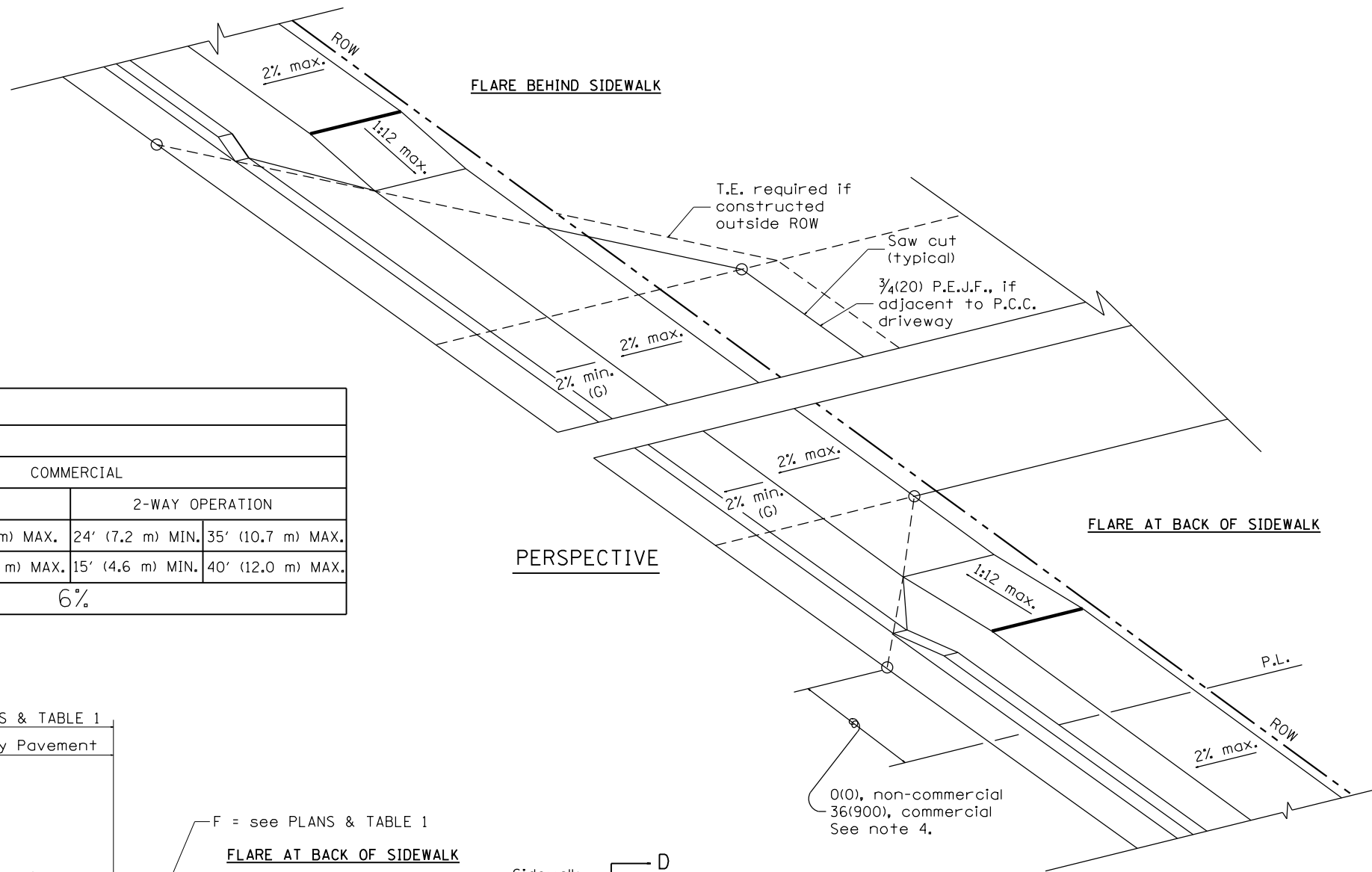
**FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE**

**TABLE A**  
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

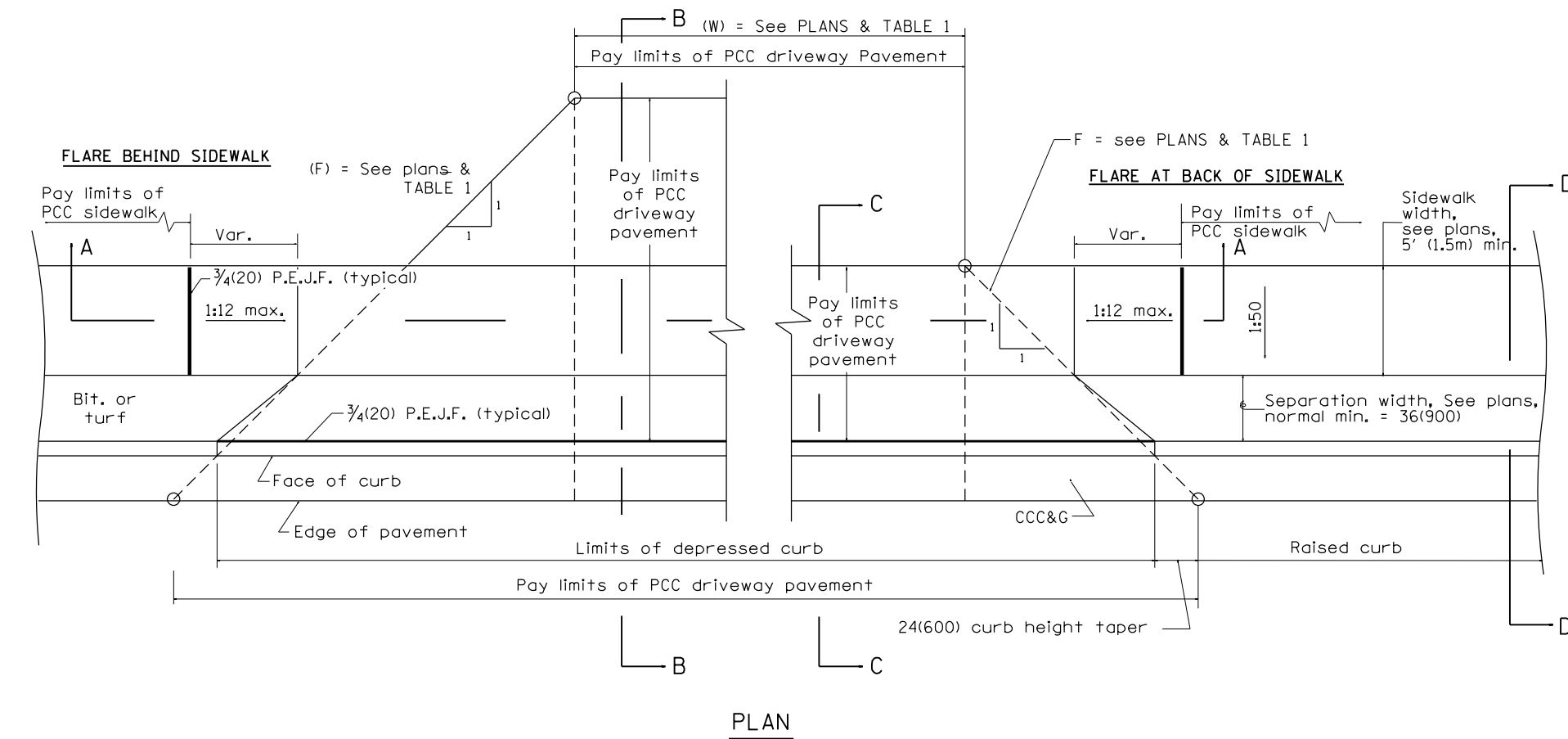
| SPEED LIMIT RANGE          | INTERSECTION CHANNELIZATION<br>(Includes Width Transitions for Median and Left Turn Lane Introductions) |          |
|----------------------------|---|----------|
|                            | CONTINUOUS  |          |
| Less Than 30 mph (50 km/h) | 50' (15m)   | 15' (5m) |
| 30 - 45 mph (50 - 70 km/h) | 75' (23m)   | 20' (6m) |
| Over 45 mph (70 km/h)      | 150' (46m)  | 30' (9m) |



All dimensions are in inches (millimeters) unless otherwise noted.



| TABLE 1<br>URBAN ENTRANCE DESIGN CONTROLS |                  |                  |                  |                   |                  |                   |
|---|------------------|------------------|------------------|-------------------|------------------|-------------------|
| ELEMENT                                   | NON-COMMERCIAL   |                  | COMMERCIAL       |                   |                  |                   |
|   |                  |                  | 1-WAY OPERATION  |                   | 2-WAY OPERATION  |                   |
| WIDTH (W)                                 | 12' (3.6 m) MIN. | 24' (7.2 m) MAX. | 14' (4.3 m) MIN. | 24' (7.2 m) MAX.  | 24' (7.2 m) MIN. | 35' (10.7 m) MAX. |
| RADIUS EQUIVALENT 1:1 FLARE (F)           | 5' (1.5 m) MIN.  | 25' (7.6 m) MAX. | 15' (4.6 m) MIN. | 40' (12.0 m) MAX. | 15' (4.6 m) MIN. | 40' (12.0 m) MAX. |
| MAX. GRADE (G)                            | 8%               |                  | 6%               |                   |                  |                   |



**GENERAL NOTES**

1. The sidewalk area located within the pay limits of P.C.C. Driveway Pavement will be of the same thickness and construction as the P.C.C. Driveway Pavement.
2. Combination Concrete Curb & Gutter shall be depressed in accordance with Standard 606001.
3. Exceptions to the radius flare/property line relationship are as shown in the plans for common entrances, with jointly executed access permits.
4. Refer to State Standard 424026 for detectable warnings at busy commercial entrances.

All dimensions are in inches (millimeters) unless otherwise noted.

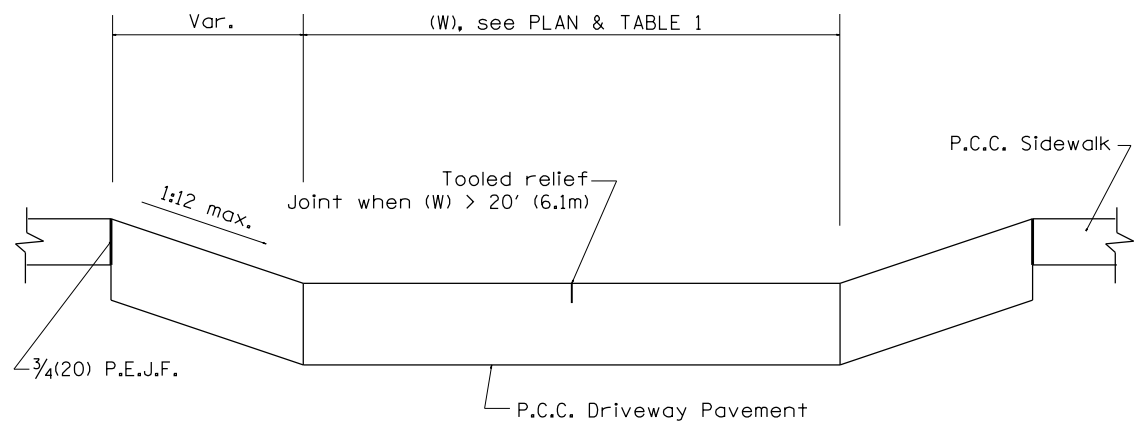
|          |                                   |        |          |                           |      |
|----------|-----------------------------------|--------|----------|---------------------------|------|
| 01-01-97 | RENUM. C-103.01, NEW REVISION BOX | T.P.   | 03-15-12 | REMOVED HATCHING ON RAMPS | R.D. |
| 01-17-03 | REMOVE CURB CONTRACTION           | JATR   |          |                           |      |
| 09-15-05 | JOINT                             | M.M.A. |          |                           |      |
| 10-16-06 | REVISED TO 2007 SPEC.             | M.A.   |          |                           |      |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

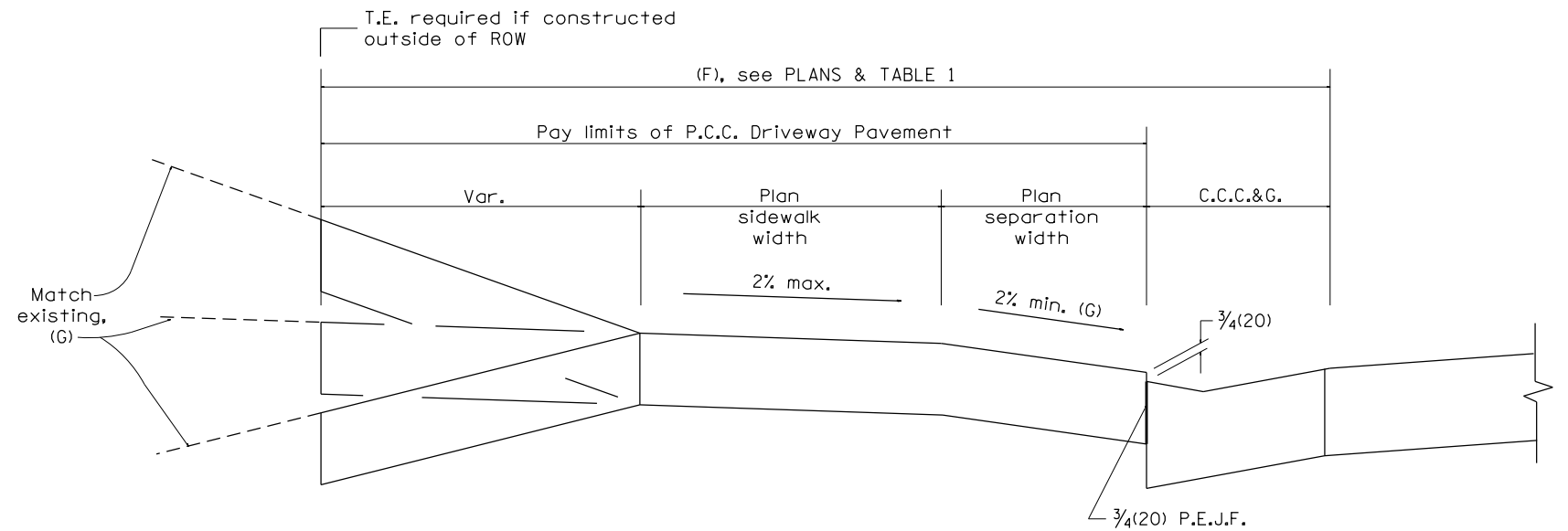
**URBAN ENTRANCES ACCESSIBLE TO THE DISABLED,  
FOR SEPARATED SIDEWALKS**  
NOT TO SCALE

|   |                            |        |                    |           |
|---|----------------------------|--------|--------------------|-----------|
| F.A.P. RTE.                                   | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR   | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 70        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |

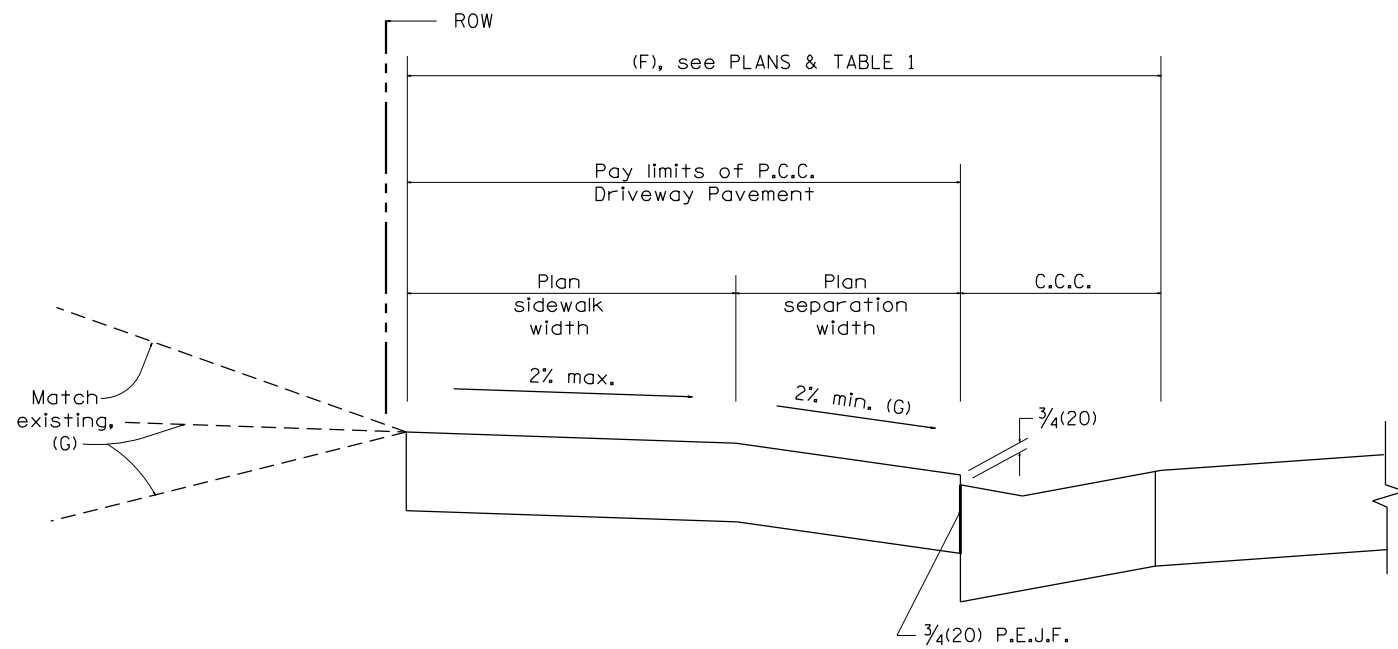
SHT. 1 OF 2  
CADD STD. 423106-D4



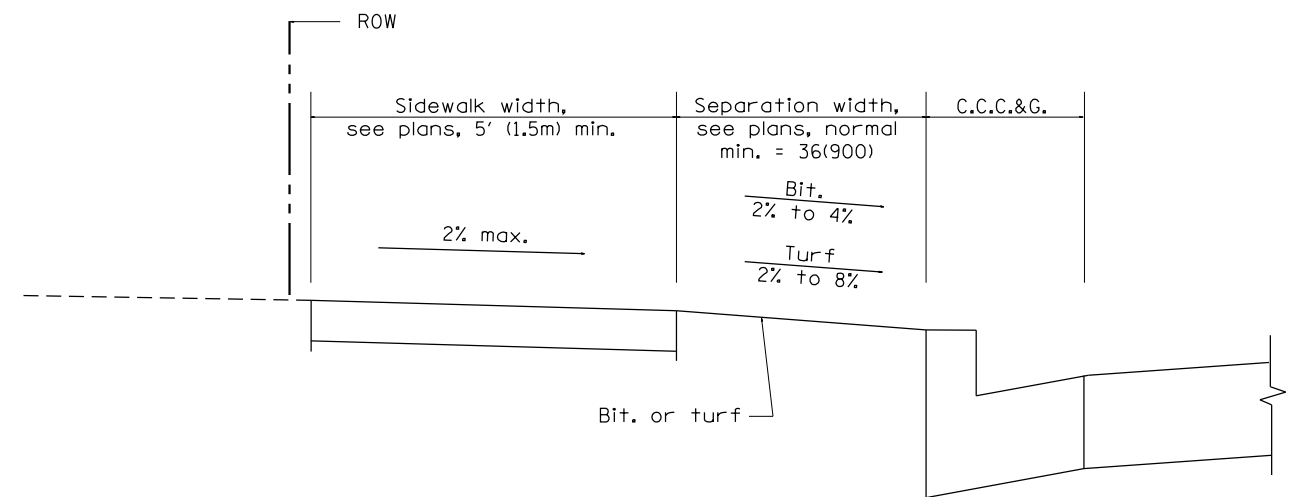
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

All dimensions are in inches (millimeters) unless otherwise noted.

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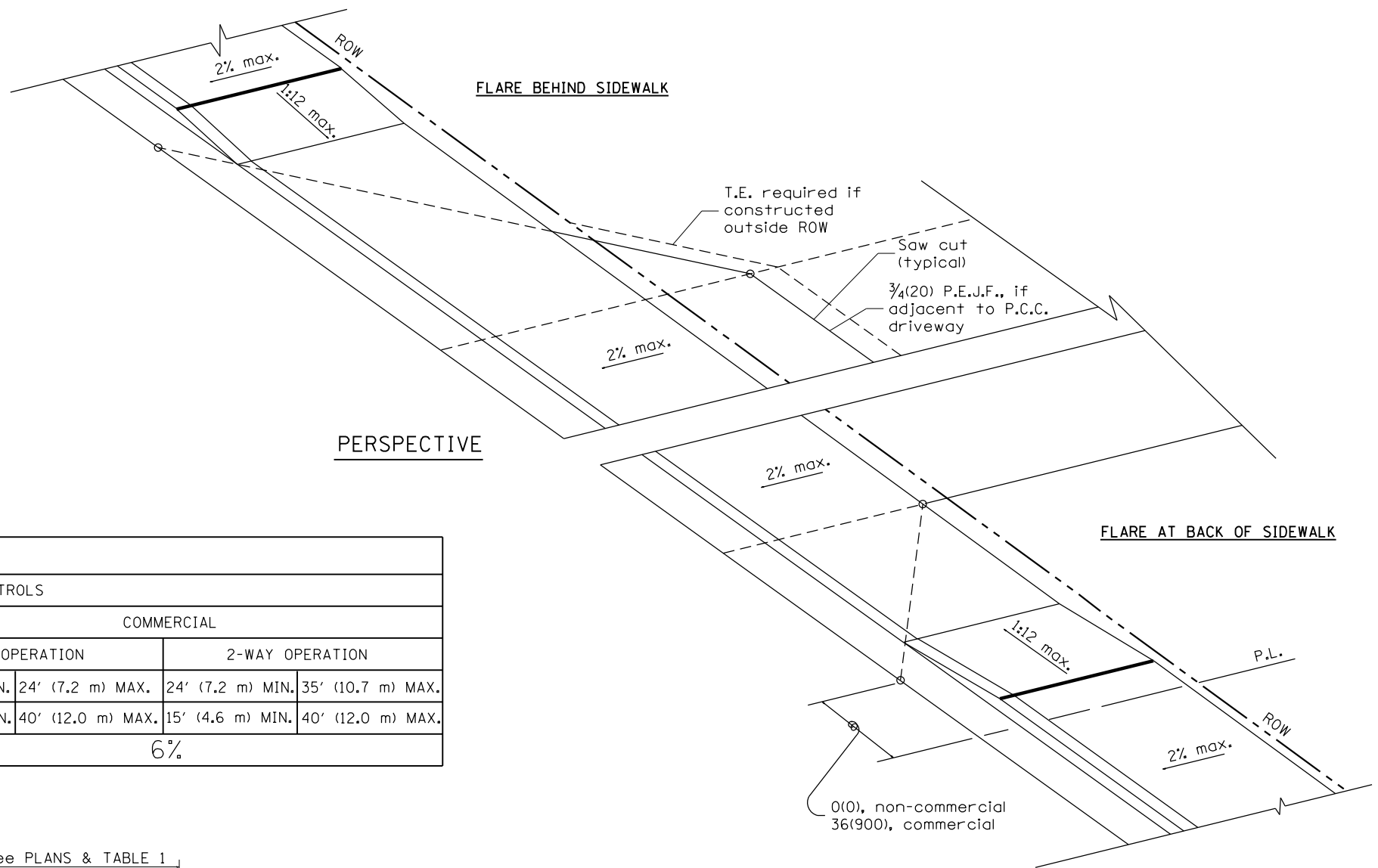
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

URBAN ENTRANCES ACCESSIBLE TO THE DISABLED,  
FOR SEPARATED SIDEWALKS

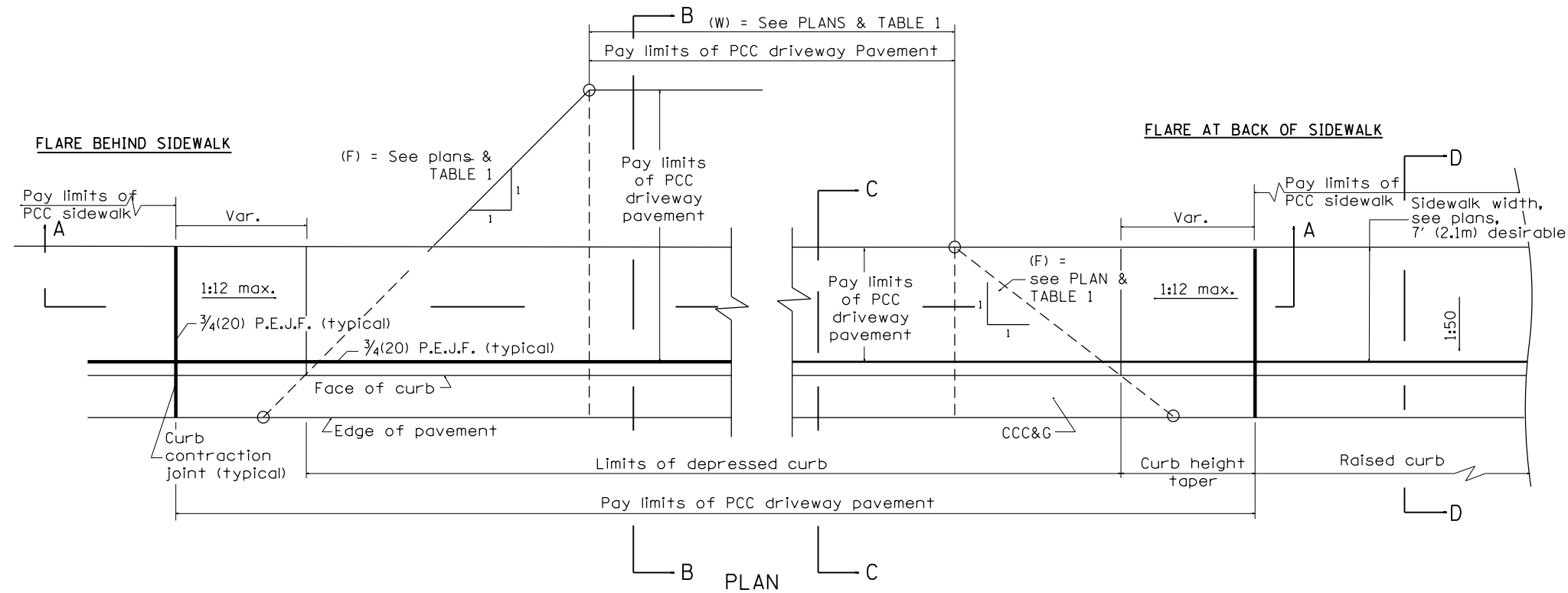
SHT. 2 OF 2  
CADD STD. 423106-D4

| F.A.P. RTE.                                   | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|----------------------------|--------|--------------------|-----------|
| VAR   | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 71        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |



PERSPECTIVE

| TABLE 1                         |                  |                  |                  |                   |                  |                   |
|---------------------------------|------------------|------------------|------------------|-------------------|------------------|-------------------|
| URBAN ENTRANCE DESIGN CONTROLS  |                  |                  |                  |                   |                  |                   |
| ELEMENT                         | NON-COMMERCIAL   |                  | COMMERCIAL       |                   |                  |                   |
|                                 |                  |                  | 1-WAY OPERATION  |                   | 2-WAY OPERATION  |                   |
| WIDTH (W)                       | 12' (3.6 m) MIN. | 24' (7.2 m) MAX. | 14' (4.3 m) MIN. | 24' (7.2 m) MAX.  | 24' (7.2 m) MIN. | 35' (10.7 m) MAX. |
| RADIUS EQUIVALENT 1:1 FLARE (F) | 5' (1.5 m) MIN.  | 25' (7.6 m) MAX. | 15' (4.6 m) MIN. | 40' (12.0 m) MAX. | 15' (4.6 m) MIN. | 40' (12.0 m) MAX. |
| MAX. GRADE (G)                  | 8%               |                  | 6%               |                   |                  |                   |



PLAN

GENERAL NOTES

1. The sidewalk area located within the pay limits of P.C.C. Driveway Pavement will be of the same thickness and construction as the P.C.C. Driveway Pavement.
2. Combination Concrete Curb & Gutter shall be depressed in accordance with Standard 606001.
3. Exceptions to the radius flare/property line relationship are as shown in the plans for common entrances, with jointly executed access permits.
4. Refer to State Standard 424026 for detectable warnings at busy commercial entrances.

All dimensions are in inches (millimeters) unless otherwise noted.

|          |                                    |        |          |                           |      |
|----------|------------------------------------|--------|----------|---------------------------|------|
| 01-01-97 | RENUM. C-103.02, NEW REVISION BOX, | T.P.   | 03-15-12 | REMOVED HATCHING ON RAMPS | R.D. |
|          | REVISED GENERAL NOTES              |        |          |                           |      |
| 09-15-05 | REVISED DESIGNER NOTE              | M.M.A. |          |                           |      |
| 10-16-06 | REVISED TO 2007 SPEC.              | M.A.   |          |                           |      |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

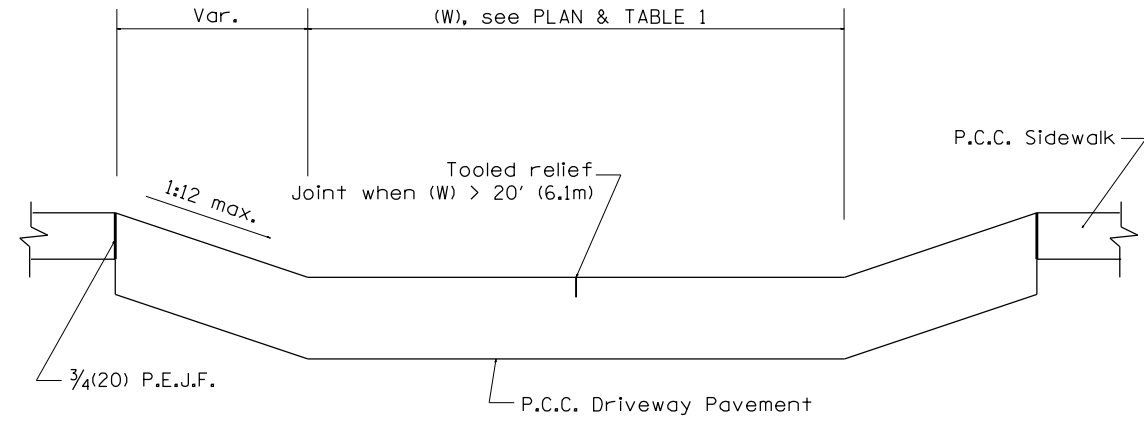
URBAN ENTRANCES ACCESSIBLE TO THE DISABLED,  
FOR CURBLINE SIDEWALKS

NOT TO SCALE

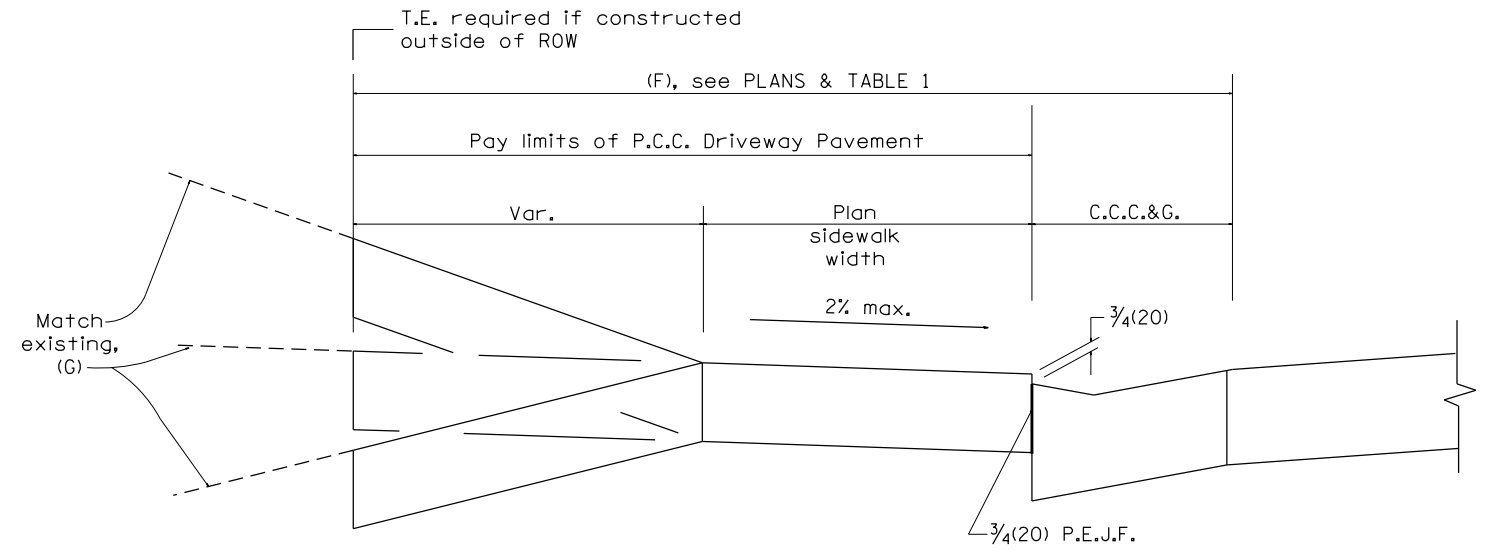
SHT. 1 OF 2  
CADD STD. 423101-D4

|   |         |        |                    |           |
|---|---------|--------|--------------------|-----------|
| F.A.P. RTE.                                   | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| VAR (137, 129)RS-4, (137X)RS-4                |         | FULTON | 73                 | 72        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 68A91 |           |

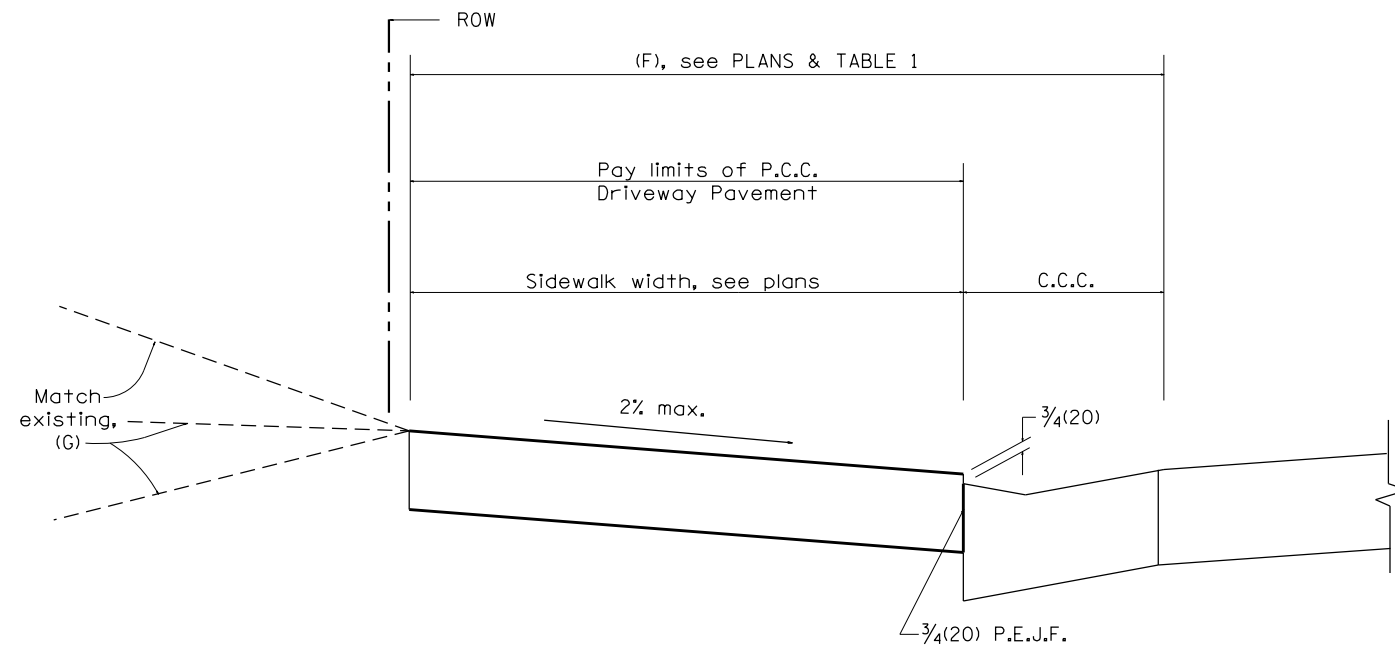




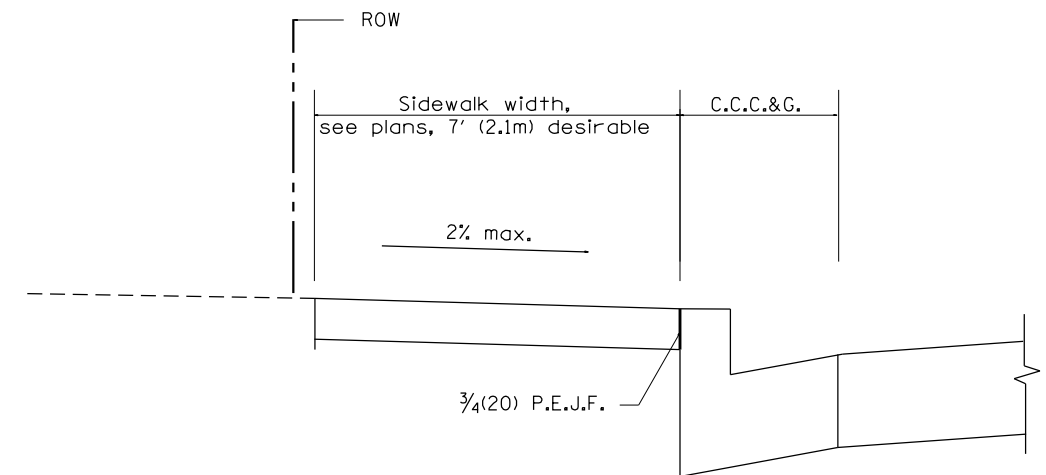
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

All dimensions are in inches (millimeters) unless otherwise noted.

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

URBAN ENTRANCES ACCESSIBLE TO THE DISABLED,  
FOR CURBLINE SIDEWALKS

SHT. 2 OF 2  
CADD STD. 423101-D4

| F.A.P. RTE.                                   | SECTION                    | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---|----------------------------|--------|--------------------|-----------|
| VAR   | (137, 129)RS-4, (137X)RS-4 | FULTON | 73                 | 73        |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT |                            |        | CONTRACT NO. 68A91 |           |