

CITY OF FREEPORT FREEPORT, ILLINOIS

CONSTRUCTION PLANS FOR FREEPORT-ALBERTUS AIRPORT

CONSTRUCT, LIGHT AND MARK SOUTHWEST TERMINAL AREA ENTRANCE ROAD - BAILEYVILLE ROAD CONNECTOR



Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

ILLINOIS PROJECT: FEP- 4626
 NOVEMBER 16, 2018

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PROFESSIONAL ENGINEER
 DOUGLAS J. KLONOWSKI
 062-050464
 STATE OF ILLINOIS
 expires: 11-30-19

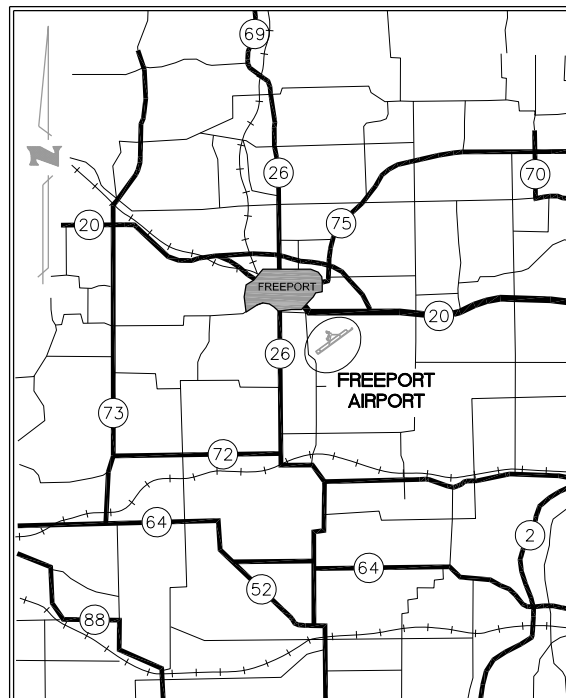
SUBMITTED BY *Douglas J. Klonowski PE*
 DOUGLAS J. KLONOWSKI, P.E.

DATE NOVEMBER 16 2018

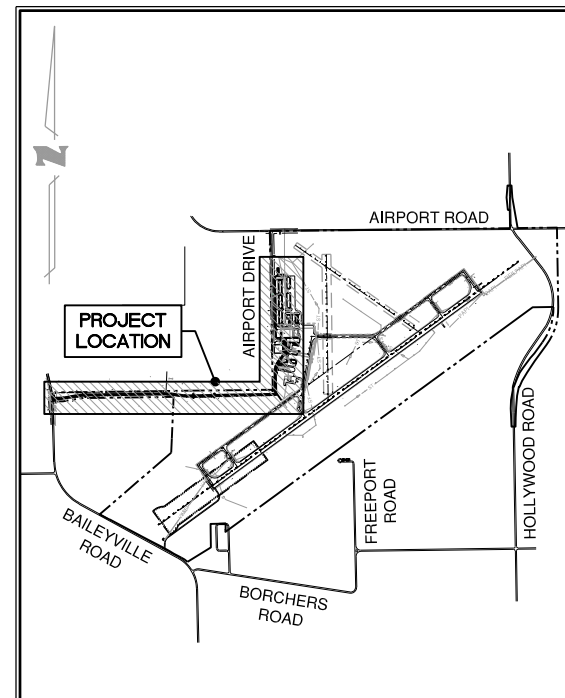
FREEPORT - ALBERTUS AIRPORT
 FREEPORT, ILLINOIS

APPROVED BY *Lowell D. Crow*
 LOWELL D. CROW, CITY MANAGER

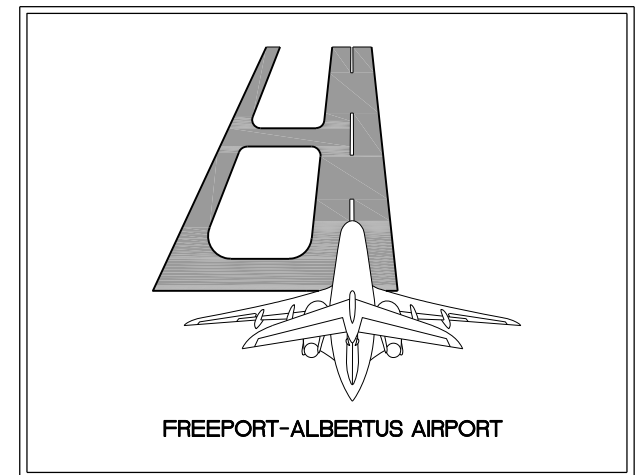
DATE NOVEMBER 16 2018



LOCATION MAP



SITE PLAN



DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY: B
 DESIGN AIRCRAFT GROUP: II

TOWNSHIP: 26 NORTH SILVER CREEK TOWNSHIP
 RANGE: 8 EAST (SECTION: 21)
 STEPHENSON COUNTY

DATE: Friday, November 30, 2018, 1:58:49 PM
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 XREF DWG: CMT NCS Training
 IMAGE FILES:
 UPDATE BY: Jim Chase
 LAYOUT: Layout1

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5	SEQUENCE OF CONSTRUCTION - SHEET 2
6	SEQUENCE OF CONSTRUCTION - SHEET 3
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SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
BASE BID				
AR108088	1/C #8 XLP-USE	LF	1030	
AR108758	1/C #8 GROUND	LF	515	
AR110217	1 1/2" STEEL DUCT, DIRECT BURY	LF	465	
AR110610	ELECTRICAL HANDHOLE	EACH	3	
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR150520	MOBILIZATION	LS	1	
AR151465	CLEARING AND GRUBBING	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	6544	
AR152442	OFFSITE BORROW EXCAVATION	CY	2377	
AR152540	SOIL STABILIZATION FABRIC	SY	11850	
AR156510	SILT FENCE	LF	9800	
AR156511	DITCH CHECK	EACH	10	
AR156520	INLET PROTECTION	EACH	8	
AR156531	EROSION CONTROL BLANKET	SY	13500	
AR156540	RIPRAP	SY	100	
AR162216	CLASS E MANUAL SLIDE GATE - 16'	EACH	1	
AR162504	CLASS E FENCE 4'	LF	100	
AR162506	CLASS E FENCE 6'	LF	552	
AR162716	ELECTRIC GATE - 16'	EACH	1	
AR162900	REMOVE CLASS E FENCE	LF	210	
AR162908	REMOVE ELECTRIC GATE	EACH	1	
AR208515	POROUS GRANULAR EMBANKMENT	CY	3707	
AR209608	CRUSHED AGG. BASE COURSE - 8"	SY	12327	
AR401610	BITUMINOUS SURFACE COURSE	TON	50	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	672	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	125	
AR403610	BITUMINOUS BASE COURSE	TON	1390	
AR602510	BITUMINOUS PRIME COAT	GAL	3592	
AR603510	BITUMINOUS TACK COAT	GAL	80	
AR620520	PAVEMENT MARKING-WATERBORNE	SF	443	
AR701212	12" CMP	LF	20	
AR701512	12" RCP, CLASS IV	LF	291	
AR701524	24" RCP, CLASS IV	LF	50	
AR701722	RCEP SPAN 45 RISE 29	LF	96	
AR701900	REMOVE PIPE	LF	210	
AR751411	INLET - TYPE A	EACH	1	
AR751540	MANHOLE 4'	EACH	1	
AR751903	REMOVE MANHOLE	EACH	1	
AR752412	PRECAST REINFORCED CONC. FES 12"	EACH	1	
AR752424	PRECAST REINFORCED CONC. FES 24"	EACH	2	
AR752736	P R CONC. FES EQ. ROUND SIZE 36"	EACH	6	
AR752900	REMOVE END SECTION	EACH	1	
AR770506	6" SANITARY SEWER	LF	400	
AR770508	8" SANITARY SEWER	LF	1291	
AR770704	SANITARY MANHOLE 4'	EACH	8	
AR770720	SEPTIC SYSTEM	LS	1	
AR770908	REMOVE SEPTIC SYSTEM	LS	1	
AR800234	DEMOLISH STRUCTURE	LS	1	
AR901510	SEEDING	ACRE	5.0	
AR908510	MULCHING	ACRE	2.0	
AR910200	ROADWAY SIGN	EACH	7	
AR910965	RELOCATE RDWY LGT POLE W/ FXTR	EACH	2	

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	ESTIMATED TOTAL COST
ADDITIVE ALTERNATE NO. 1				
AS401610	BITUMINOUS SURFACE COURSE	TON	140	
AS603510	BITUMINOUS TACK COAT	GAL	264	

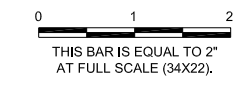
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	ESTIMATED TOTAL COST
ADDITIVE ALTERNATE NO. 2				
AT108084	1/C #4 XLP-USE	LF	195	
AT108088	1/C #8 XLP-USE	LF	60	
AT108758	1/C #8 GROUND	LF	95	
AT110217	1 1/2" STEEL DUCT, DIRECT BURY	LF	45	
AT910100	ROADWAY LIGHT POLE	EACH	1	
AT910160	ROADWAY ELECTRIC SERVICE	LS	1	
AT910162	LIGHTING CONTROLLER	LS	1	

ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	ESTIMATED TOTAL COST
ADDITIVE ALTERNATE NO. 3				
AU401610	BITUMINOUS SURFACE COURSE	TON	760	
AU603510	BITUMINOUS TACK COAT	GAL	1,291	
AU770506	6" SANITARY SEWER	LF	180	
AU770704	SANITARY MANHOLE 4'	EACH	1	

IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK # ###

REVISIONS		
NUMBER	BY	DATE



FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
INDEX TO SHEETS/SUMMARY OF QUANTITIES

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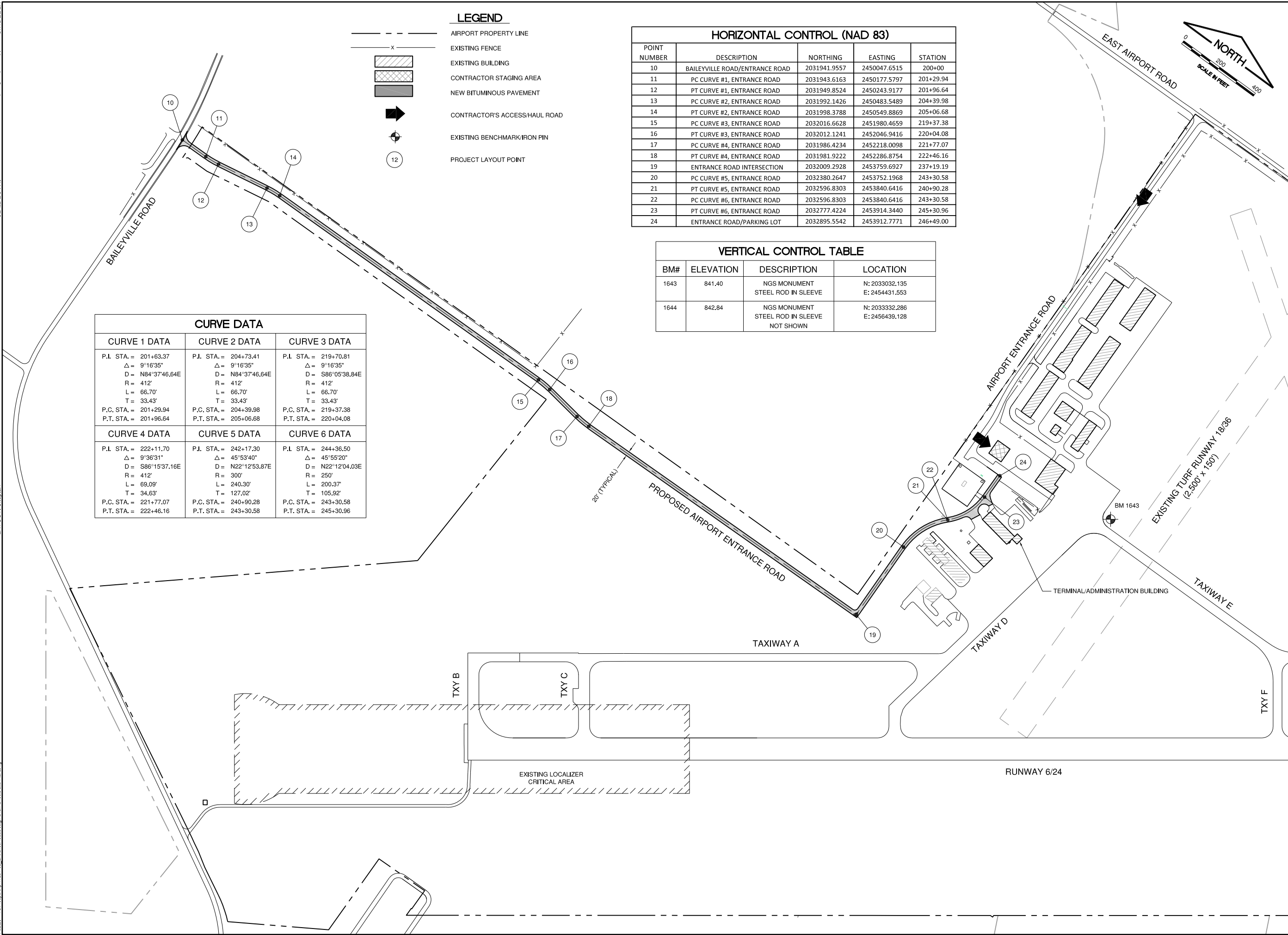
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 DRAWN BY: JRO
 CHECKED BY: DJK
 APPROVED BY: DJK
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 JOB No: 17294-02

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UPDATE BY: Jim Ohse
LAYOUT: Stigeban

REF: DWG: Road Assets

IMAGE FILES:



CURVE DATA		
CURVE 1 DATA	CURVE 2 DATA	CURVE 3 DATA
P.I. STA. = 201+63.37 $\Delta = 9^{\circ}16'35''$ D = N84°37'46.64E R = 412' L = 66.70' T = 33.43' P.C. STA. = 201+29.94 P.T. STA. = 201+96.64	P.I. STA. = 204+73.41 $\Delta = 9^{\circ}16'35''$ D = N84°37'46.64E R = 412' L = 66.70' T = 33.43' P.C. STA. = 204+39.98 P.T. STA. = 205+06.68	P.I. STA. = 219+70.81 $\Delta = 9^{\circ}16'35''$ D = S86°05'38.84E R = 412' L = 66.70' T = 33.43' P.C. STA. = 219+37.38 P.T. STA. = 220+04.08
CURVE 4 DATA	CURVE 5 DATA	CURVE 6 DATA
P.I. STA. = 222+11.70 $\Delta = 9^{\circ}36'31''$ D = S86°15'37.16E R = 412' L = 69.09' T = 34.63' P.C. STA. = 221+77.07 P.T. STA. = 222+46.16	P.I. STA. = 242+17.30 $\Delta = 45^{\circ}53'40''$ D = N22°12'53.87E R = 300' L = 240.30' T = 127.02' P.C. STA. = 240+90.28 P.T. STA. = 243+30.58	P.I. STA. = 244+36.50 $\Delta = 45^{\circ}55'20''$ D = N22°12'04.03E R = 250' L = 200.37' T = 105.92' P.C. STA. = 243+30.58 P.T. STA. = 245+30.96

HORIZONTAL CONTROL (NAD 83)				
POINT NUMBER	DESCRIPTION	NORTHING	EASTING	STATION
10	BAILEYVILLE ROAD/ENTRANCE ROAD	2031941.9557	2450047.6515	200+00
11	PC CURVE #1, ENTRANCE ROAD	2031943.6163	2450177.5797	201+29.94
12	PT CURVE #1, ENTRANCE ROAD	2031949.8524	2450243.9177	201+96.64
13	PC CURVE #2, ENTRANCE ROAD	2031992.1426	2450483.5489	204+39.98
14	PT CURVE #2, ENTRANCE ROAD	2031998.3788	2450549.8869	205+06.68
15	PC CURVE #3, ENTRANCE ROAD	2032016.6628	2451980.4659	219+37.38
16	PT CURVE #3, ENTRANCE ROAD	2032012.1241	2452046.9416	220+04.08
17	PC CURVE #4, ENTRANCE ROAD	2031986.4234	2452218.0098	221+77.07
18	PT CURVE #4, ENTRANCE ROAD	2031981.9222	2452286.8754	222+46.16
19	ENTRANCE ROAD INTERSECTION	2032009.2928	2453759.6927	237+19.19
20	PC CURVE #5, ENTRANCE ROAD	2032380.2647	2453752.1968	243+30.58
21	PT CURVE #5, ENTRANCE ROAD	2032596.8303	2453840.6416	240+90.28
22	PC CURVE #6, ENTRANCE ROAD	2032596.8303	2453840.6416	243+30.58
23	PT CURVE #6, ENTRANCE ROAD	2032777.4224	2453914.3440	245+30.96
24	ENTRANCE ROAD/PARKING LOT	2032895.5542	2453912.7771	246+49.00

VERTICAL CONTROL TABLE			
BM#	ELEVATION	DESCRIPTION	LOCATION
1643	841.40	NGS MONUMENT STEEL ROD IN SLEEVE	N: 2033032.135 E: 2454431.553
1644	842.84	NGS MONUMENT STEEL ROD IN SLEEVE NOT SHOWN	N: 2033332.286 E: 2456439.128

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
SITE PLAN/PROJECT CONTROL PLAN

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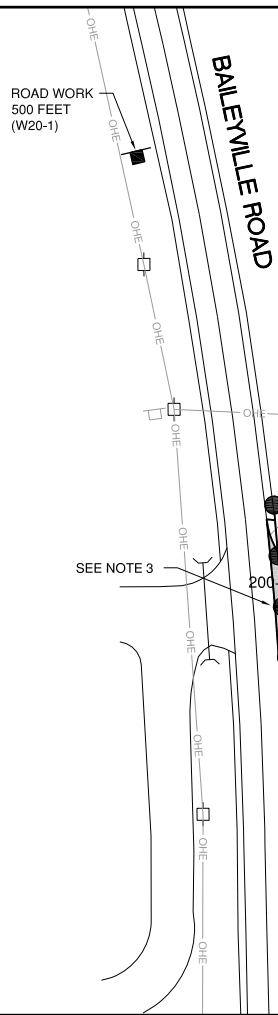
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DRAWN BY: JRO
CHECKED BY: DJK
APPROVED BY: DJK
DATE: 11-16-2018
JOB No: 17294-02

GENERAL NOTES: Road Work 500 Feet (W20-1)

IMAGE FILES:

UPDATE BY: Jim Ohse
LAYOUT: Layout

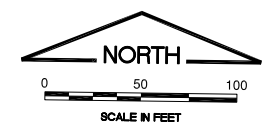
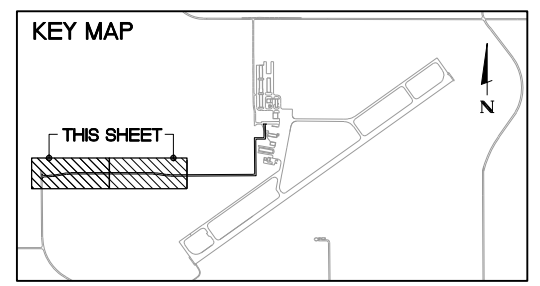
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SEE SHEET 6 FOR
FAA CRITICAL POINT TABLE

NOTES

1. PHASE 1A, 1B AND 2 MAY WORK CONCURRENTLY.
2. PHASE 1B SHALL BE CONSTRUCTED TO MINIMIZE GATE AND AIRSIDE ACCESS DISRUPTION TO TENANTS. WHEN A GATE AND ROAD CLOSURE IS REQUIRED FOR PHASE 1B CONSTRUCTION, THE AIRPORT SHALL BE NOTIFIED 2 WEEKS IN ADVANCE TO ALLOW NOTIFICATION TO THE TENANTS.
3. IF A TEMPORARY LANE CLOSURE IS REQUIRED ON BAILEYVILLE ROAD, THE CONTRACTOR SHALL NOTIFY THE AIRPORT, THE CITY OF FREEPORT AND STEPHENSON COUNTY 2 WEEKS IN ADVANCE. THE CONTRACTOR SHALL INSTALL THE NECESSARY SIGNS AND BARRICADES PER IDOT AND MUTCD STANDARDS AND A FLAGGER SHALL DIRECT TRAFFIC DURING THE LANE CLOSURE DURATION.
4. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF A TAXILANE CLOSURE IS NEEDED FOR PHASE 2 CONSTRUCTION. AT THE END OF THE CONSTRUCTION DAY, AIRFIELD PAVEMENT SHALL BE SWEEPED, CLEANED AND OPEN TO TRAFFIC.

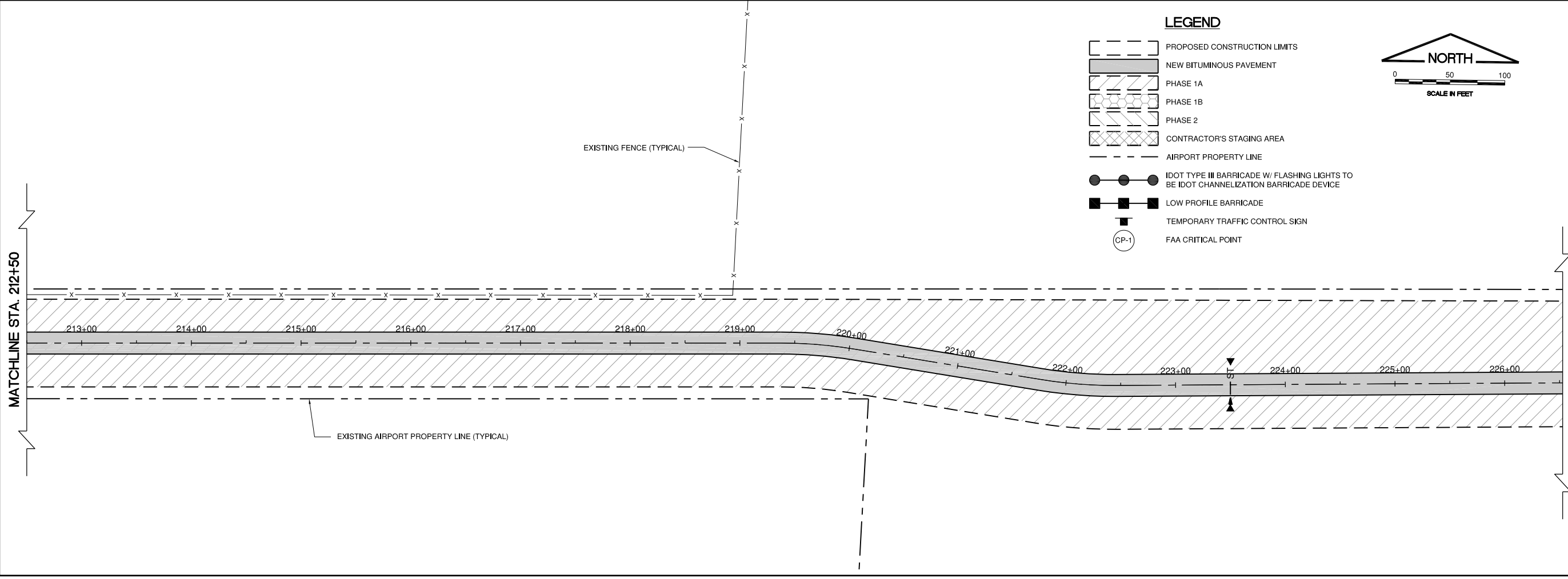
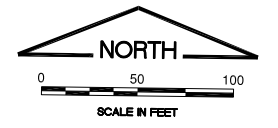


PHASE 1A - SUGGESTED SEQUENCE OF CONSTRUCTION

- PERFORM CLEARING AND GRUBBING, AND UNCLASSIFIED EXCAVATION FOR PROPOSED ROADWAY GEOMETRY CONSTRUCTION.
- CONSTRUCT PROPOSED STORM SEWERS AND DRAINAGE STRUCTURES TO GRADE.
- CONSTRUCT POROUS GRANULAR EMBANKMENT FOR PROPOSED ROADWAY
- CONSTRUCT ROADWAY INCLUDING: 8" CRUSHED AGGREGATE BASE COURSE, BITUMINOUS BASE COURSE, AND BITUMINOUS SURFACE COURSE.
- CONSTRUCT PROPOSED LIGHTING AT BAILEYVILLE ROAD
- DURING PHASE 1A, PHASE 1B SHALL REMAIN OPERATIONAL TO AIRPORT OPERATIONS. PHASE 1B WORK MAY WORK CONCURRENTLY WITH PHASE 1A AS APPROVED BY THE ENGINEER.

LEGEND

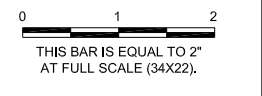
- PROPOSED CONSTRUCTION LIMITS
- NEW BITUMINOUS PAVEMENT
- PHASE 1A
- PHASE 1B
- PHASE 2
- CONTRACTOR'S STAGING AREA
- AIRPORT PROPERTY LINE
- IDOT TYPE III BARRICADE W/ FLASHING LIGHTS TO BE IDOT CHANNELIZATION BARRICADE DEVICE
- LOW PROFILE BARRICADE
- TEMPORARY TRAFFIC CONTROL SIGN
- FAA CRITICAL POINT



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK #

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NUMBER	BY	DATE



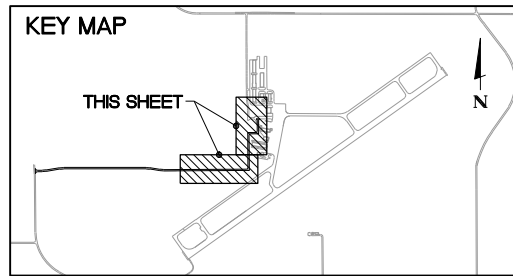
FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD
 SEQUENCE OF CONSTRUCTION
 SHEET 1

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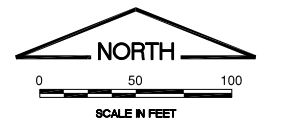
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DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
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JOB No:	17294-02

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 SHEET: 5 OF 52 SHEETS



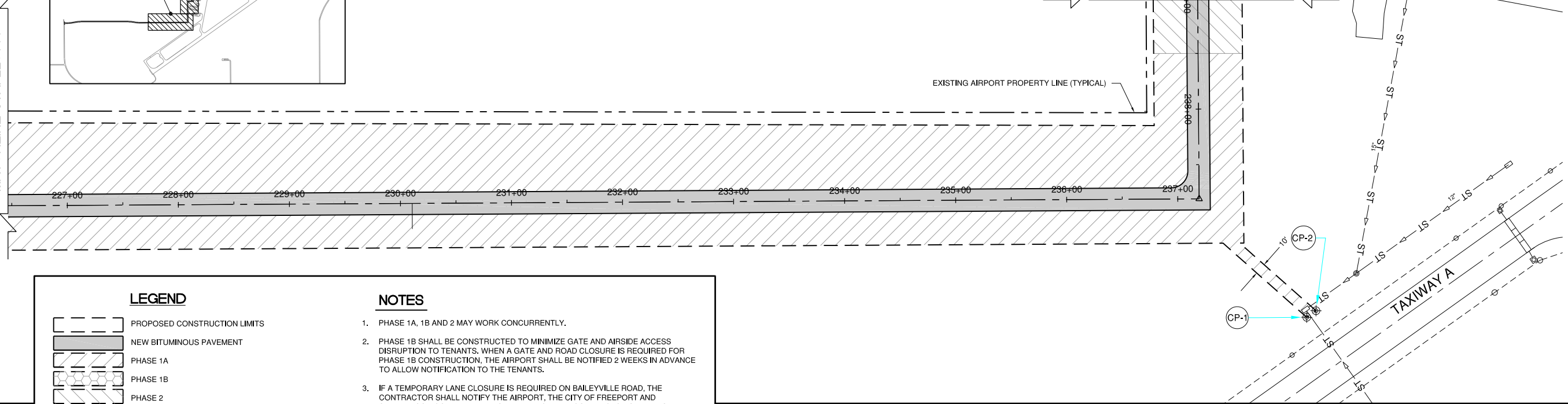
SEE SHEET 6 FOR
FAA CRITICAL POINT TABLE



MATCHLINE STA. 226+50

MATCHLINE STA. 238+50

MATCHLINE STA. 238+50



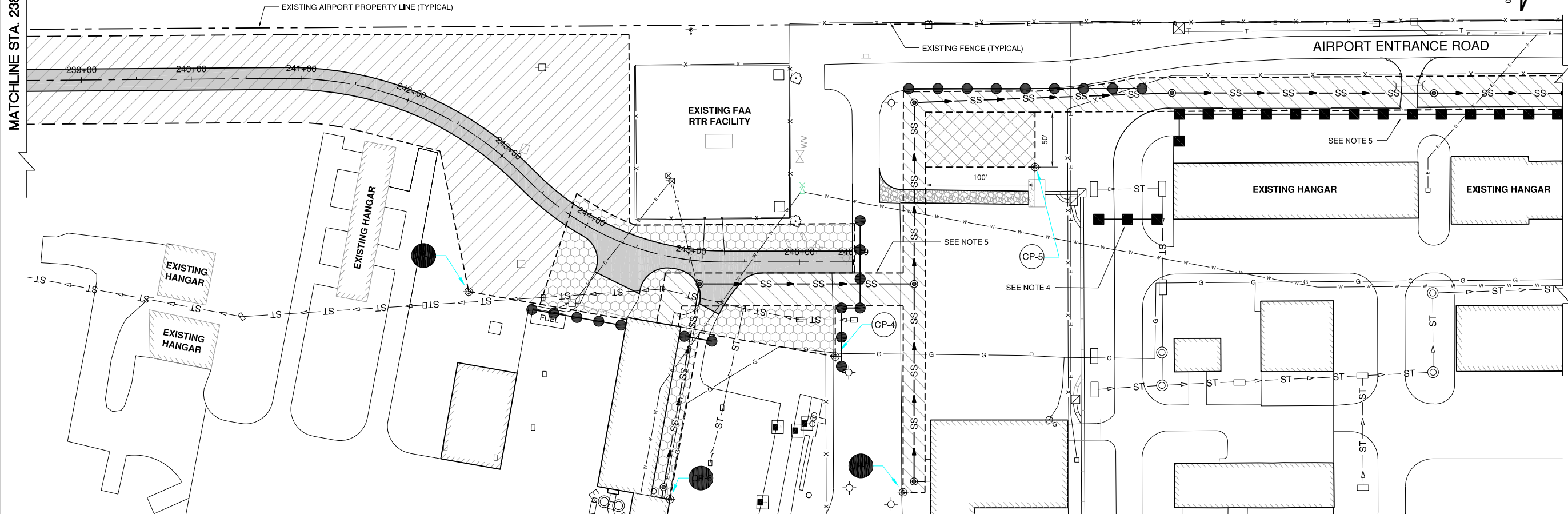
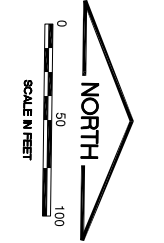
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- PROPOSED CONSTRUCTION LIMITS
- NEW BITUMINOUS PAVEMENT
- PHASE 1A
- PHASE 1B
- PHASE 2
- CONTRACTOR'S STAGING AREA
- AIRPORT PROPERTY LINE
- IDOT TYPE III BARRICADE W/ FLASHING LIGHTS TO BE IDOT CHANNELIZATION BARRICADE DEVICE
- LOW PROFILE BARRICADE
- TEMPORARY TRAFFIC CONTROL SIGN
- FAA CRITICAL POINT

- NOTES**
1. PHASE 1A, 1B AND 2 MAY WORK CONCURRENTLY.
 2. PHASE 1B SHALL BE CONSTRUCTED TO MINIMIZE GATE AND AIRSIDE ACCESS DISRUPTION TO TENANTS. WHEN A GATE AND ROAD CLOSURE IS REQUIRED FOR PHASE 1B CONSTRUCTION, THE AIRPORT SHALL BE NOTIFIED 2 WEEKS IN ADVANCE TO ALLOW NOTIFICATION TO THE TENANTS.
 3. IF A TEMPORARY LANE CLOSURE IS REQUIRED ON BAILEYVILLE ROAD, THE CONTRACTOR SHALL NOTIFY THE AIRPORT, THE CITY OF FREEPORT AND STEPHENSON COUNTY 2 WEEKS IN ADVANCE. THE CONTRACTOR SHALL INSTALL THE NECESSARY SIGNS AND BARRICADES PER IDOT AND MUTCD STANDARDS AND A FLAGGER SHALL DIRECT TRAFFIC DURING THE LANE CLOSURE DURATION.
 4. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF A TAXILANE CLOSURE IS NEEDED FOR PHASE 2 CONSTRUCTION. AT THE END OF THE CONSTRUCTION DAY, AIRFIELD PAVEMENT SHALL BE SWEEPED, CLEANED AND OPEN TO TRAFFIC.
 5. PHASE 2 CONSTRUCTION SHALL BE PERFORMED TO ALLOW VEHICLE ACCESS TO THE AIRFIELD AT ALL TIMES.

PHASE 1B - SUGGESTED SEQUENCE OF CONSTRUCTION

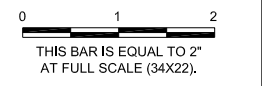
- COORDINATE CLOSURE AND NEW AIRFIELD ACCESS WITH AIRPORT AND INSTALL TRAFFIC CONTROL BARRICADES.
- DISCONNECT EXISTING GATE AND REMOVE EXISTING BITUMINOUS PAVEMENT.
- CONSTRUCT UNCLASSIFIED EXCAVATION FOR PROPOSED ROADWAY GEOMETRY.
- CONSTRUCT PROPOSED STORM SEWER AND DRAINAGE STRUCTURES TO GRADE.
- CONSTRUCT ROADWAY INCLUDING: 8" CRUSHED AGGREGATE BASE COURSE, BITUMINOUS BASE COURSE, AND BITUMINOUS SURFACE COURSE.
- DURING PHASE 1B, PHASE 2 WORK MAY WORK CONCURRENTLY FOR THE INSTALLATION OF PROPOSED SANITARY SEWER PIPE AND STRUCTURES.
- INSTALL NEW GATES.
- CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ROADWAY TO TRAFFIC AFTER COMPLETION OF ALL PHASE 1 CONSTRUCTION.



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
SEQUENCE OF CONSTRUCTION
SHEET 2

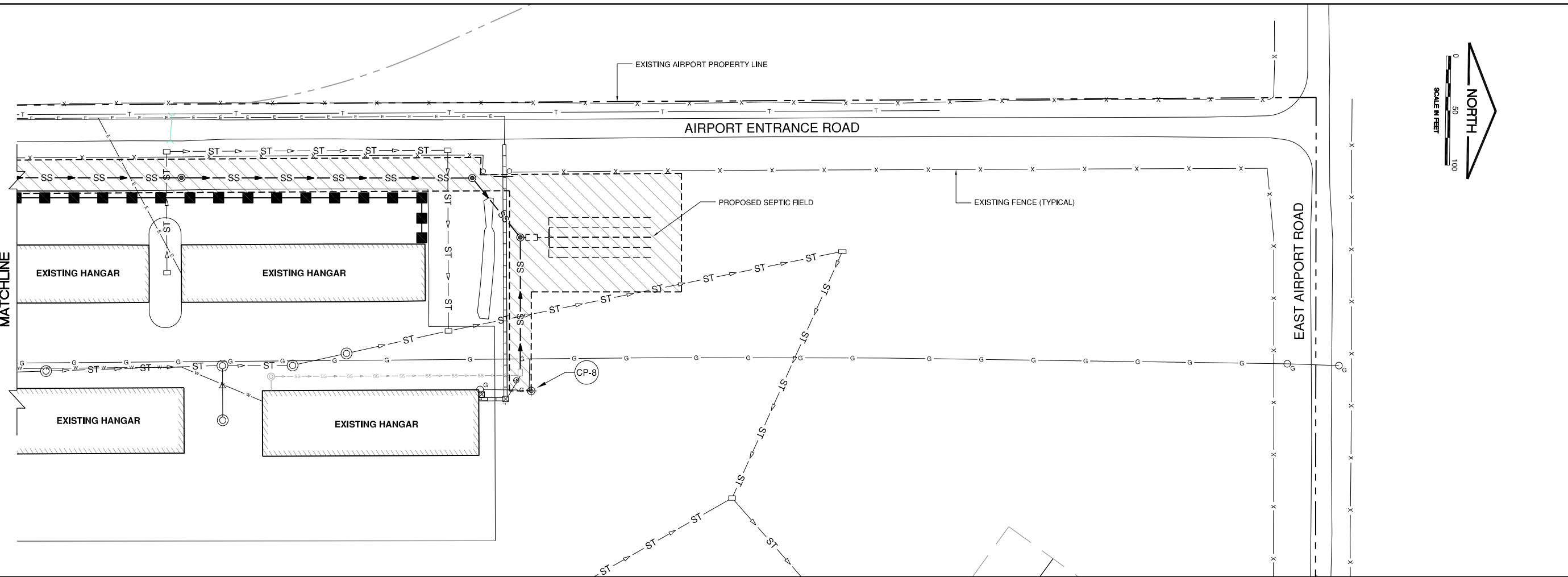
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APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

SHEET 5 OF 52 SHEETS

DATE: Friday, November 30, 2018, 1:59:30 PM
 FILE: K:\Freeport\17294-02 - Entrance Road\Design\Draw Sheets\Sequence of Construction - 3.dwg
 UPDATE BY: Jim Ohee
 LAYOUT: Layout
 REF: DWG: 17294-02 - Entrance Road
 GENERAL SCALE: 1/4" = 1'-0"
 MATCHLINE:



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

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FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
SEQUENCE OF CONSTRUCTION
SHEET 3

PHASE 2 - SUGGESTED SEQUENCE OF CONSTRUCTION

- INSTALL TRAFFIC CONTROL BARRICADES.
- CONSTRUCT UNCLASSIFIED EXCAVATION AND REMOVE EXISTING PAVEMENT FOR SANITARY SEWER INSTALLATION.
- CONSTRUCT SANITARY SEWER.
- CONSTRUCT PROPOSED SEPTIC SEWER.
- REPAIR EXISTING PAVEMENT AND LANDSCAPING.

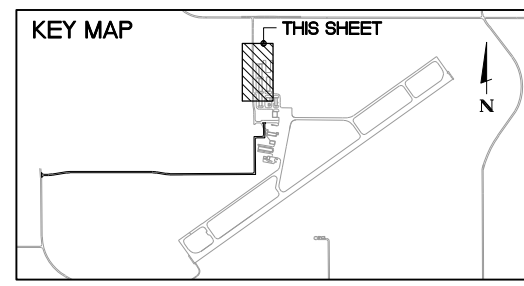
FAA AIRSPACE CRITICAL POINT TABLE					
POINT	LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT	ABOVE GROUND ELEVATION
CP-1	42° 14' 37.71"	89° 35' 08.92"	841.0	25'	866.0
CP-2	42° 14' 37.77"	89° 35' 08.81"	841.0	25'	866.0
CP-3	42° 14' 44.04"	89° 35' 07.72"	846.0	25'	871.0
CP-4	42° 14' 47.35"	89° 35' 06.96"	844.3	25'	869.3
CP-5	42° 14' 49.13"	89° 35' 09.27"	845.5	25'	870.5
CP-6	42° 14' 45.87"	89° 35' 05.22"	846.3	25'	871.3
CP-7	42° 14' 47.96"	89° 35' 05.32"	845.0	25'	870.0
CP-8	42° 14' 58.55"	89° 35' 07.64"	839.4	25'	864.4

LEGEND

- PROPOSED CONSTRUCTION LIMITS
- NEW BITUMINOUS PAVEMENT
- PHASE 1A
- PHASE 1B
- PHASE 2
- CONTRACTOR'S STAGING AREA
- AIRPORT PROPERTY LINE
- IDOT TYPE III BARRICADE W/ FLASHING LIGHTS TO BE IDOT CHANNELIZATION BARRICADE DEVICE
- LOW PROFILE BARRICADE
- TEMPORARY TRAFFIC CONTROL SIGN
- FAA CRITICAL POINT

NOTES

1. PHASE 1A, 1B AND 2 MAY WORK CONCURRENTLY.
2. PHASE 1B SHALL BE CONSTRUCTED TO MINIMIZE GATE AND AIRSIDE ACCESS DISRUPTION TO TENANTS. WHEN A GATE AND ROAD CLOSURE IS REQUIRED FOR PHASE 1B CONSTRUCTION, THE AIRPORT SHALL BE NOTIFIED 2 WEEKS IN ADVANCE TO ALLOW NOTIFICATION TO THE TENANTS.
3. IF A TEMPORARY LANE CLOSURE IS REQUIRED ON BAILEYVILLE ROAD, THE CONTRACTOR SHALL NOTIFY THE AIRPORT, THE CITY OF FREEPORT AND STEPHENSON COUNTY 2 WEEKS IN ADVANCE. THE CONTRACTOR SHALL INSTALL THE NECESSARY SIGNS AND BARRICADES PER IDOT AND MUTCD STANDARDS AND A FLAGGER SHALL DIRECT TRAFFIC DURING THE LANE CLOSURE DURATION.
4. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF A TAXILANE CLOSURE IS NEEDED FOR PHASE 2 CONSTRUCTION. AT THE END OF THE CONSTRUCTION DAY, AIRFIELD PAVEMENT SHALL BE SWEEPED, CLEANED AND OPEN TO TRAFFIC.



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 DATE: 11-16-2018
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SHEET 6 OF 52 SHEETS

DATE: Friday, November 30, 2018, 1:50:39 PM
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 LAYOUT: Layout1
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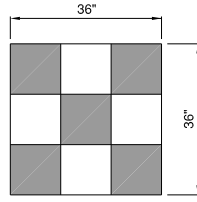
GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL BE PAID FOR UNDER MOBILIZATION. BARRICADES WITH ONE ORANGE FLAG (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHTED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

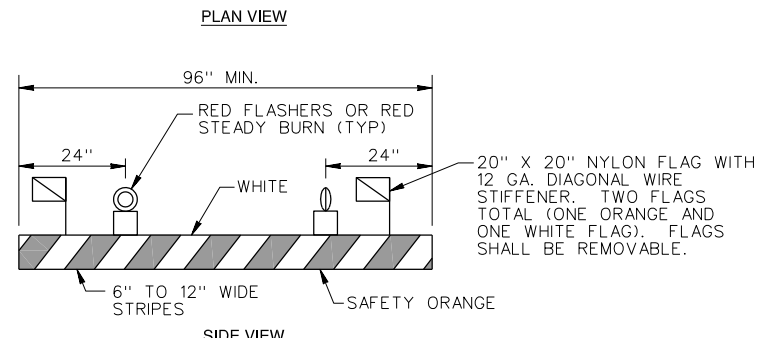
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS. ANY NECESSARY REPAIRS TO FAA CABLES SHALL BE IN THEIR ENTIRETY, NO SPLICES SHALL BE PERMITTED IN FAA CABLES. ANY REPAIRS SHALL BE INCIDENTAL TO THE CONTRACT.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDING, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES AND THE RUNWAYS AIRCRAFT OPERATIONS AREA.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL. UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER, ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- THE CONTRACTOR SHALL PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD). THE SPCD SHALL DETAIL HOW THE CONTRACTOR WILL COMPLY WITH THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). DURING THE DEVELOPMENT OF THE CSPP, IT IS NOT POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS, SUCH AS SPECIFIC EQUIPMENT HAZARDS AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHT, ETC. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AN SPCD THAT IS REVIEWED BY THE AIRPORT OPERATOR FOR APPROVAL PRIOR TO ISSUANCE OF THE NOTICE TO PROCEED. THE SPCD IS A SUBSET OF THE CSPP AND WILL BE ADDRESSED IN THE SAME FASHION AS A SHOP DRAWING FOR TECHNICAL COMPLIANCE WITH THE CSPP.
- TEMPORARY TRAFFIC CONTROL SIGNS, BARRICADES, MARKERS, ETC. SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE



LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

BARRICADE NOTES

- FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
- PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**SEQUENCE OF CONSTRUCTION
 GENERAL NOTES AND DETAILS**

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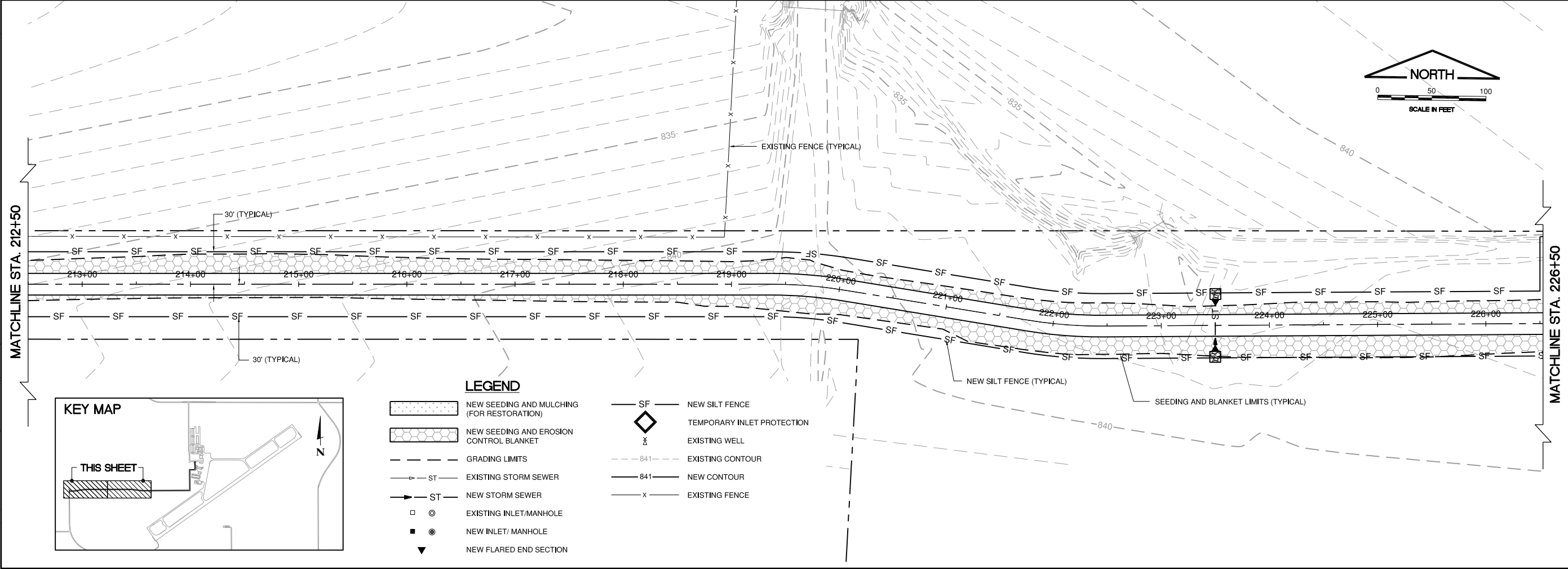
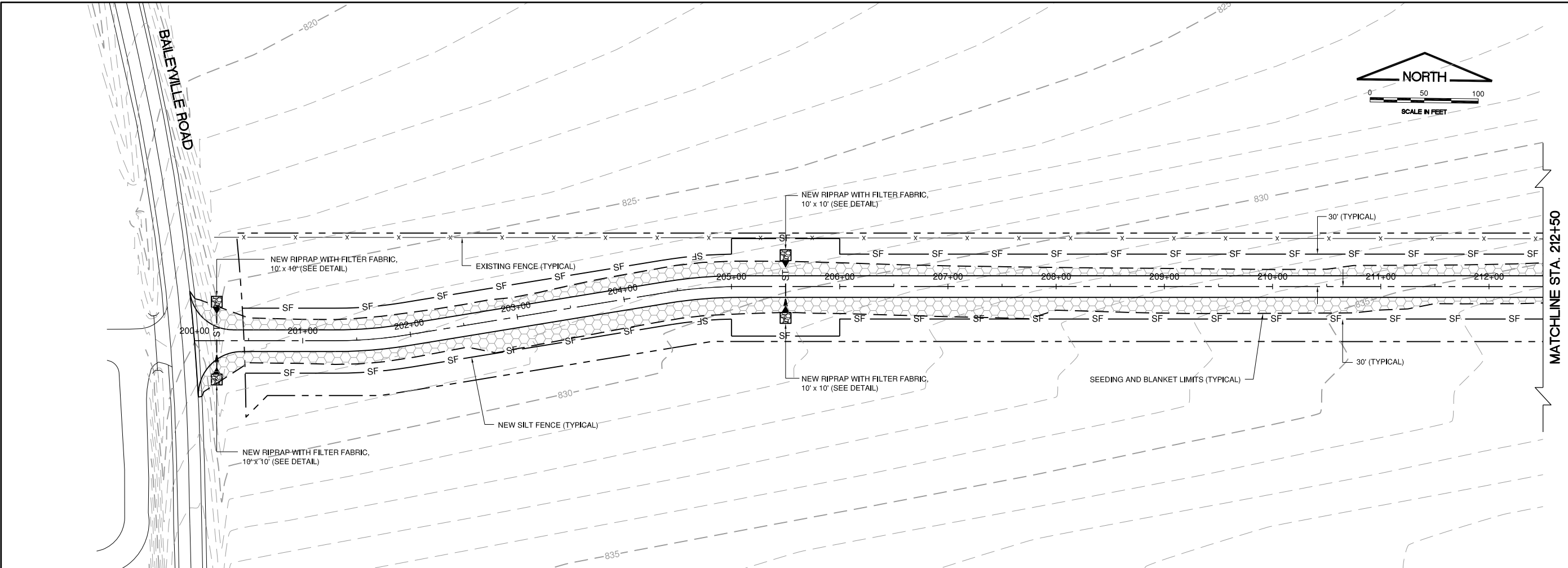
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JOB No:	17294-02

XREF DWG: Road Base, Stormwater

IMAGE FILES:

UPDATE BY: Jim Chase
LAYOUT: Stiglan

DATE: Friday, November 30, 2018, 1:59:52 PM
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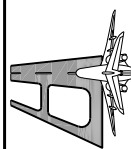
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**STORM WATER POLLUTION PREVENTION PLAN
 SHEET 1**

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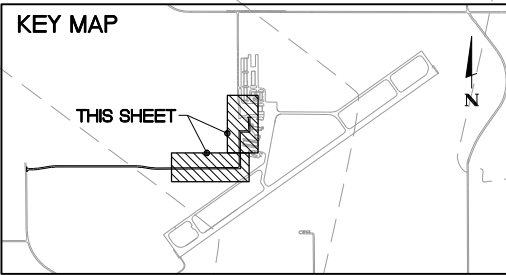
SHEET 8 OF 52 SHEETS

GENERAL NOTES:
1. SEE SHEET 8 FOR LEGEND AND NOTES

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UPDATE BY: Jim Chase
LAYOUT: Stiglan

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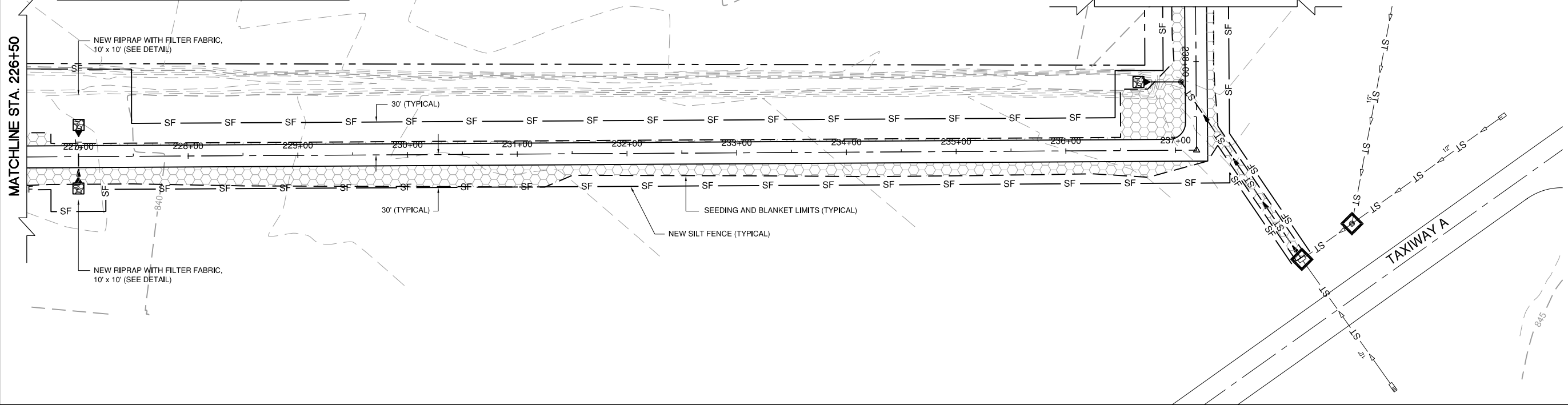


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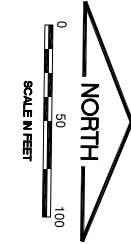
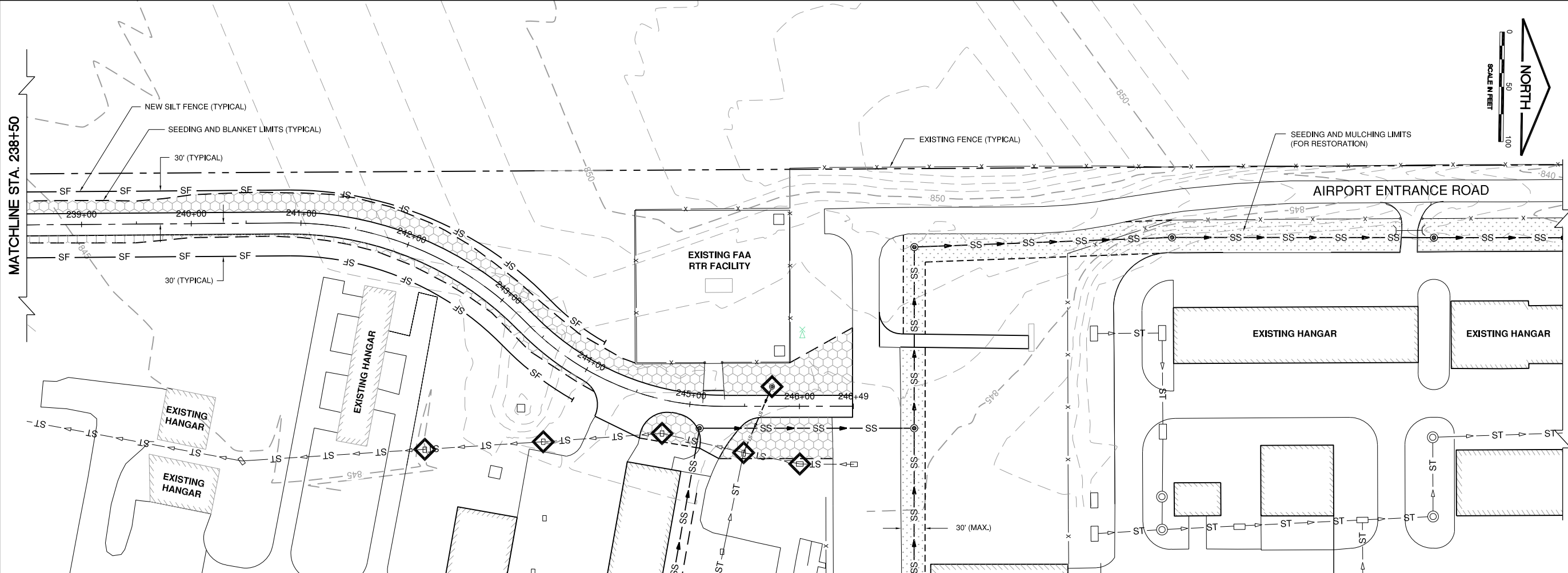
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MATCHLINE STA. 238+50

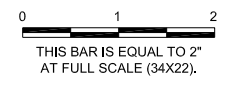
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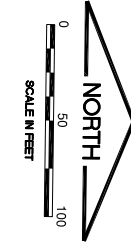
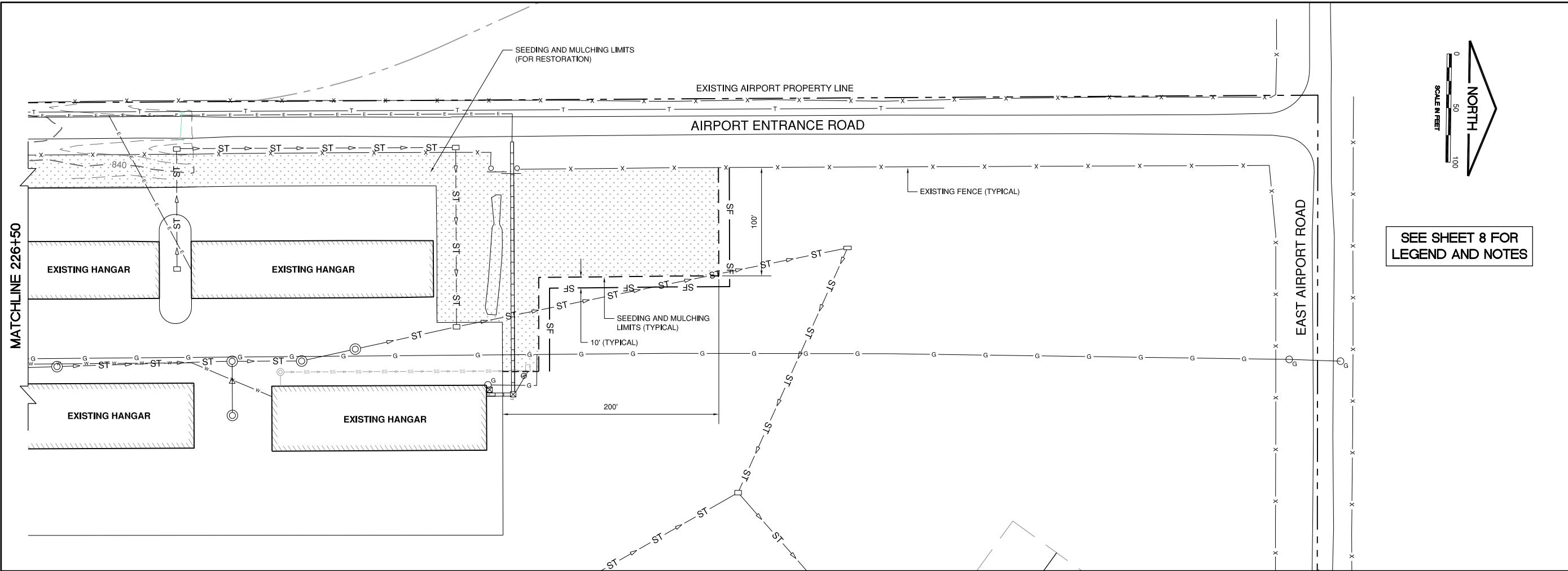


FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
STORM WATER POLLUTION PREVENTION PLAN
SHEET 2

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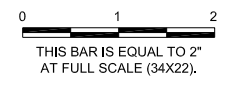


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 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

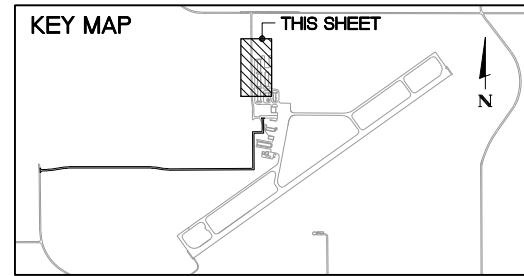
SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD

STORMWATER POLLUTION PREVENTION PLAN
 SHEET 3



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DESIGN BY:	MND
DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

DATE: Friday, November 30, 2018, 2:00:19 PM
FILE: K:\Freeport\17294-02 - Entrance Road\Design\Draw\Sheets\supp-r-notes And Details - 1.dwg

UPDATE BY: Jim Chise
LAYOUT: Layout1

REFERENCE: CMT NCS Form

IMAGE FILES:

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF THE CONSTRUCTION OF A BITUMINOUS ENTRANCE ROAD AT THE FREEPORT-ALBERTUS AIRPORT. THE PROJECT INCLUDES GRADING, DRAINAGE, PAVEMENT CONSTRUCTION, ELECTRICAL WORK, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

- PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS INLET PROTECTION.
- REMOVAL, ADJUSTMENTS AND INSTALLATION OF SANITARY SEWER ITEMS.
- PAVEMENT CONSTRUCTION INCLUDING GRADING AND DRAINAGE.
- SEEDING AND MULCHING.
- INSTALLATION OF NEW PAVEMENT MARKING AND SIGNAGE.
- REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 11 ACRES OF WHICH 10 ACRES WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

- INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
- PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE PECATONICA RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

- A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
- THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS' REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI, G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS"

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT #	_____
DATE ISSUED	_____
DATE EXPIRED	_____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

- ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
- NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
- CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
- TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
- THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
- SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, STEPHENSON COUNTY, FREEPORT-ALBERTUS AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
- THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
- WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
- SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
- ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
- THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
- PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:

ROUTE: FREEPORT-ALBERTUS AIRPORT MARKED: SOUTHWEST TERMINAL AREA ENTRANCE ROAD

SECTION: 21 PROJECT NUMBER: FEP-4626

COUNTY: STEPHENSON CONTRACT NUMBER: FR042

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____

PRINTED NAME: _____ TITLE: _____

NAME OF FIRM: _____

STREET ADDRESS: _____

CITY, STATE, ZIP: _____

PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

RECORD OF SITE DISTURBANCE AND STABILIZATION

MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
CONSTRUCTION CEASED: EXPLANATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.

IL CONTRACT: FR042

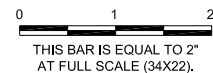
IL LETTING ITEM: 14A

IL PROJECT: FEP-4626

S.B.G. PROJECT: N/A

SURVEY BOOK # ###

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NUMBER	BY	DATE

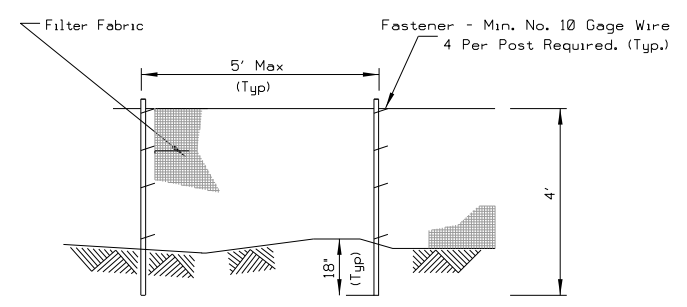


FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD
 STORMWATER POLLUTION PREVENTION PLAN
 NOTES AND DETAILS - SHEET 1

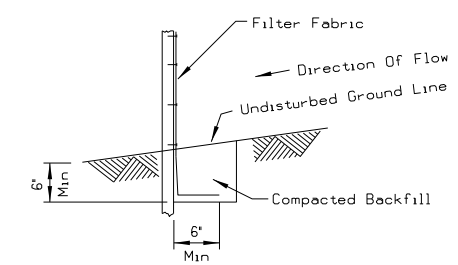
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DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



ELEVATION

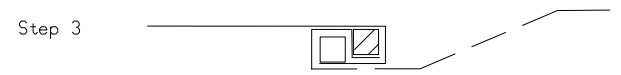
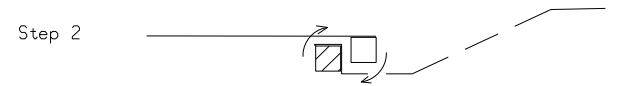
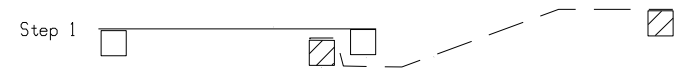


FABRIC ANCHOR DETAIL

NOTES:

1. TEMPORARY SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
2. FILTER FABRIC SHALL BE A WOVEN FABRIC MEETING THE REQUIREMENTS OF AASHTO M288 FOR UNSUPPORTED SILT FENCE WITH LESS THAN 50 PERCENT GEOTEXTILE ELONGATION.
3. FENCE POSTS SHALL BE WOOD POST WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN.

SILT FENCE DETAIL

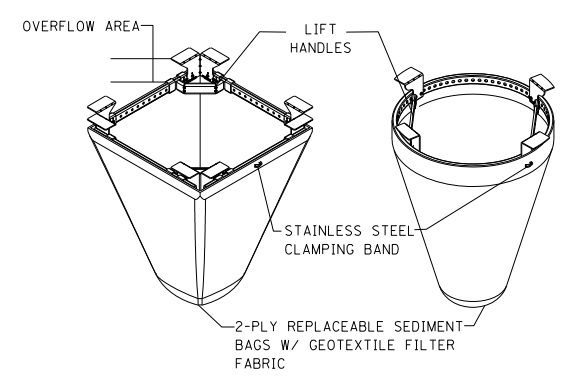


ATTACHING TWO SILT FENCES

NOTES:

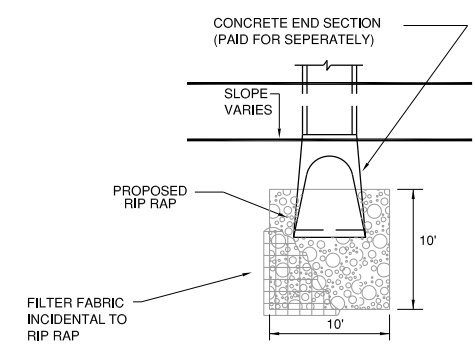
1. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
2. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
3. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.
4. SILT FILTER J-HOOK PLACEMENT SHALL BE IN ACCORDANCE WITH IDOT STD. 280001-07.

ATTACHING TWO SILT FENCES DETAIL



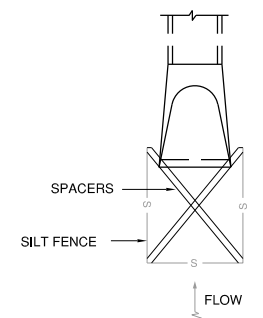
INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

FOR ALL RECTANGULAR AND CIRCULAR INLETS



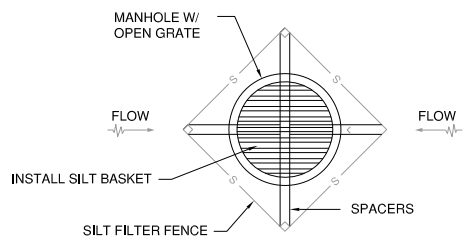
INLET PROTECTION (END SECTION) WITH RIP RAP

NOT TO SCALE



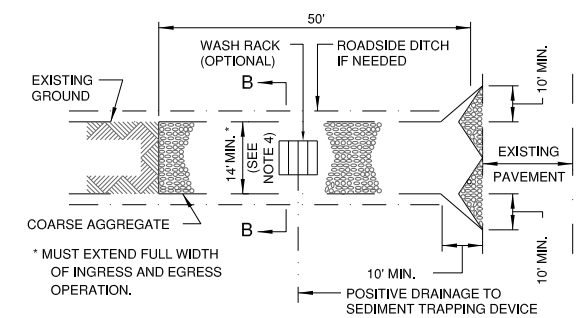
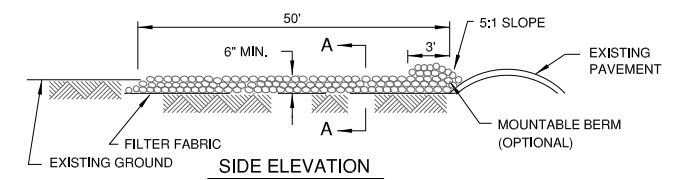
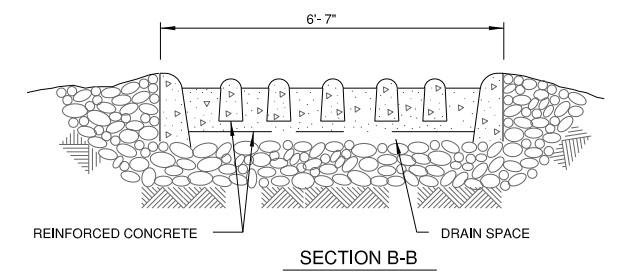
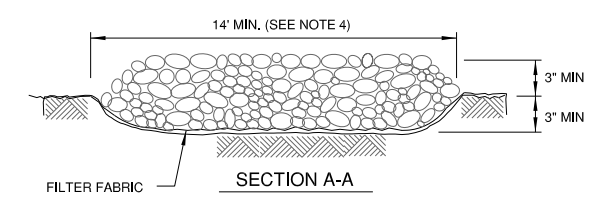
INLET PROTECTION (END SECTION)

NOT TO SCALE
IDOT STANDARD 280001-07



INLET PROTECTION (INLET/MANHOLES - IN TURF)

NOT TO SCALE
IDOT STANDARD 280001-07



STABILIZED CONSTRUCTION ENTRANCE

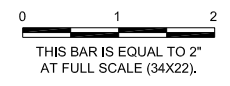
FROM NRCS STANDARD DRAWING NO. IL-630

1. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED UNDER SECTION 1080.03, OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2012.
2. ROCK OR RECLAIMED CONCRETE SHALL MEET ONE OF THE FOLLOWING IDOT COARSE AGGREGATE GRADATION, CA-1, CA-2, CA-3 OR CA-4. COMPACTION SHALL BE TO THE SATISFACTION OF THE ENGINEER.
3. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHALL BE CONSTRUCTED ACCORDING TO MANUFACTURERS SPECIFICATIONS AND SHALL BE INCIDENTAL TO THE CONTRACT.
4. MINIMUM WIDTH IS 14' FOR ONE-WAY TRAFFIC AND 20' FOR TWO WAY TRAFFIC. TWO-WAY TRAFFIC WIDTHS SHALL BE INCREASED A MINIMUM OF 4' FOR TRAILER TRAFFIC. DEPENDING ON THE TYPE OF VEHICLE OR EQUIPMENT, SPEED, LOADS, CLIMATIC AND OTHER CONDITIONS UNDER WHICH VEHICLES AND EQUIPMENT OPERATE AN INCREASE IN THE MINIMUM WIDTHS MAY BE REQUIRED.
5. ROADWAY SHALL FOLLOW THE CONTOUR OF THE NATURAL TERRAIN TO THE EXTENT POSSIBLE.
6. IF WASH RACK ARE USED THEY SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.
7. THE STABILIZED CONSTRUCTION ENTRANCE SHALL NOT BE PAID FOR BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK # ###

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NUMBER	BY	DATE



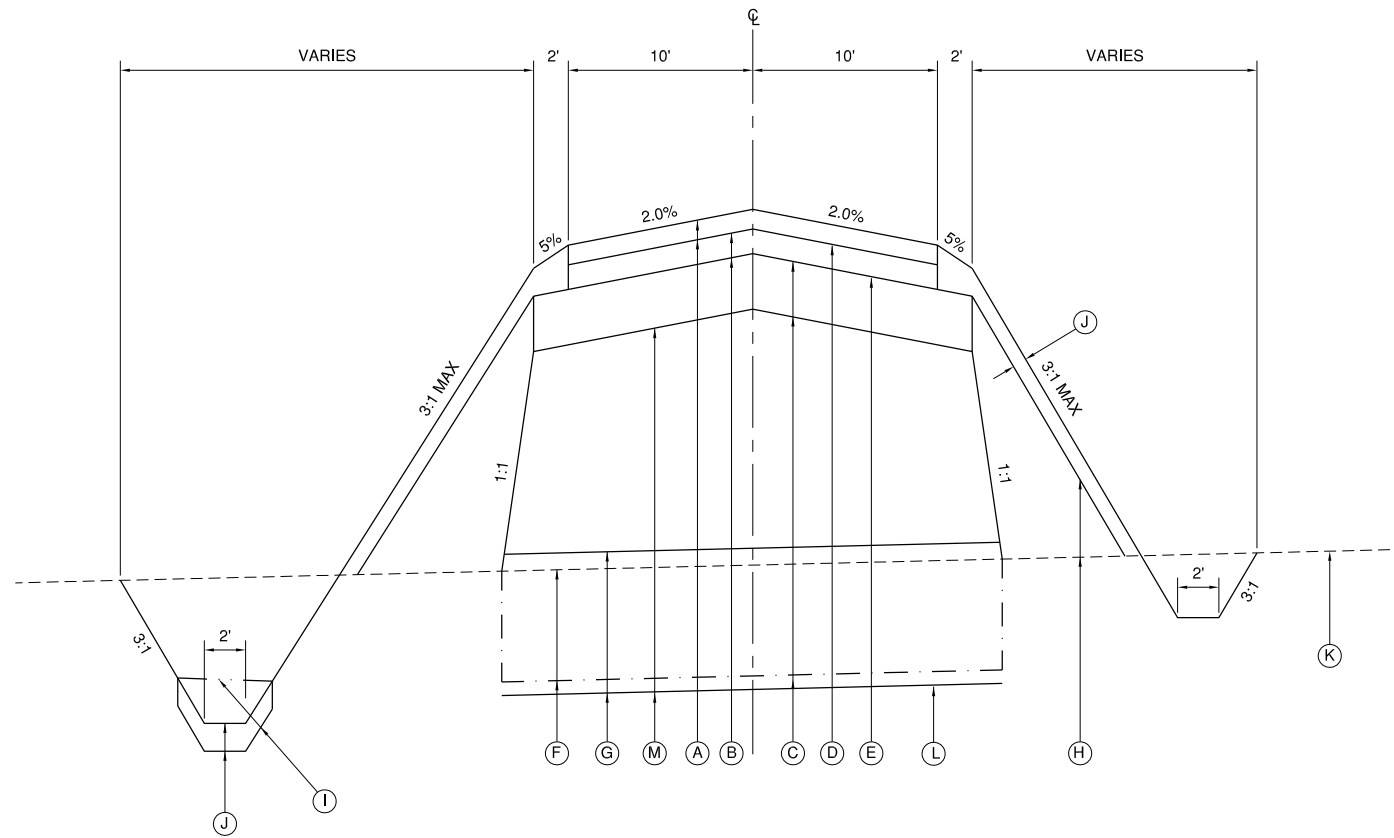
FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD

STORMWATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS - SHEET 2

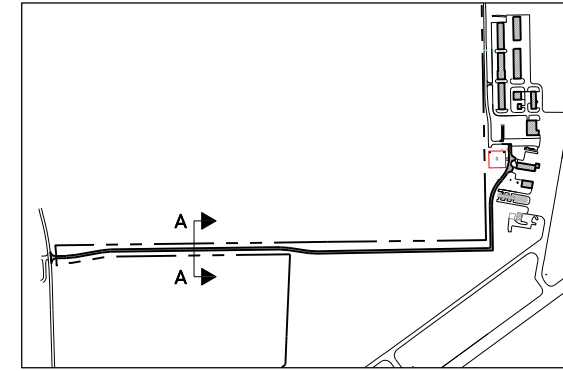
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DATE:	11-16-2018
JOB No:	17294-02



SOUTHWEST TERMINAL ENTRANCE ROAD TYPICAL SECTION
 NOT TO SCALE



KEY MAP

LEGEND

- (A) BITUMINOUS SURFACE COURSE, 1-1/2" (401)(SEE NOTE 1)))))
- (B) BITUMINOUS BASE COURSE, 2" (201)
- (C) CRUSHED AGGREGATE BASE COURSE, 8" (209)
- (D) BITUMINOUS TACK COAT (603)
- (E) BITUMINOUS PRIME COAT (602)
- (F) TOPSOIL STRIPPING (12" MAX.)(152)
- (G) POROUS GRANULAR EMBANKMENT (12")(208)(SEE NOTE 3)
- (H) SHOULDER FILL (152)
- (I) UNCLASSIFIED EXCAVATION (152)(SEE NOTE 2)
- (J) TOPSOIL PLACEMENT, 4" (MIN.)(905)
- (K) EXISTING GROUNDLINE
- (L) SOIL STABILIZATION FABRIC (152540)
- (M) EMBANKMENT FILL (152)

NOTES

1. THE BASE BID LIMITS OF BITUMINOUS SURFACE COURSE SHALL EXTEND FROM STA. 200+00 TO STA. 202+00. THE ADDITIVE ALTERNATE NO. 1 LIMITS SHALL EXTEND FROM STA. 240+75 TO STA. 246+49. THE ADDITIVE ALTERNATE NO. 3 LIMITS SHALL EXTEND FROM STA. 202+00 TO STA. 240+75.
2. THE UNCLASSIFIED EXCAVATION TO BE USED AS EMBANKMENT FILL SHALL NOT BE PLACED IN THE TOP 12" BELOW THE PROPOSED 8" CRUSHED AGGREGATE BASE COURSE.
3. THE 12" POROUS GRANULAR EMBANKMENT IS TO BE PLACED AT THE BOTTOM LAYER OF THE PROPOSED EMBANKMENT AFTER TOPSOIL STRIPPING.

IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

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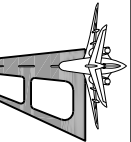
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TYPICAL SECTIONS

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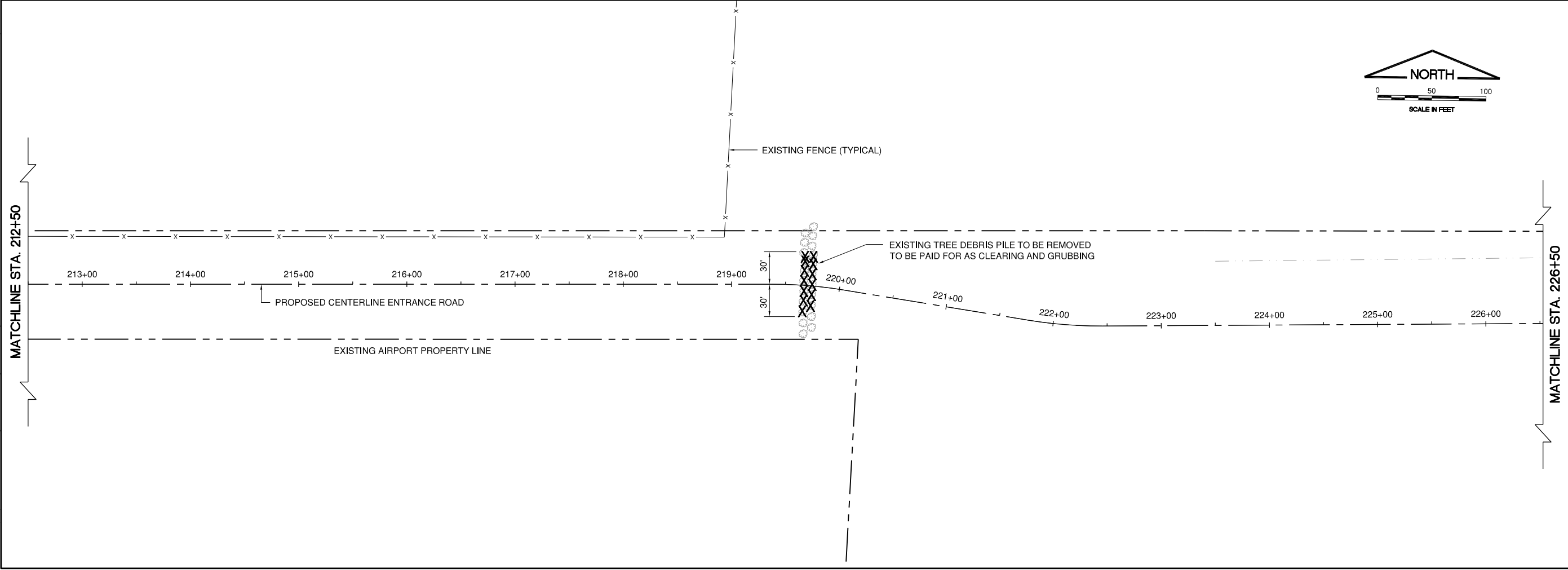
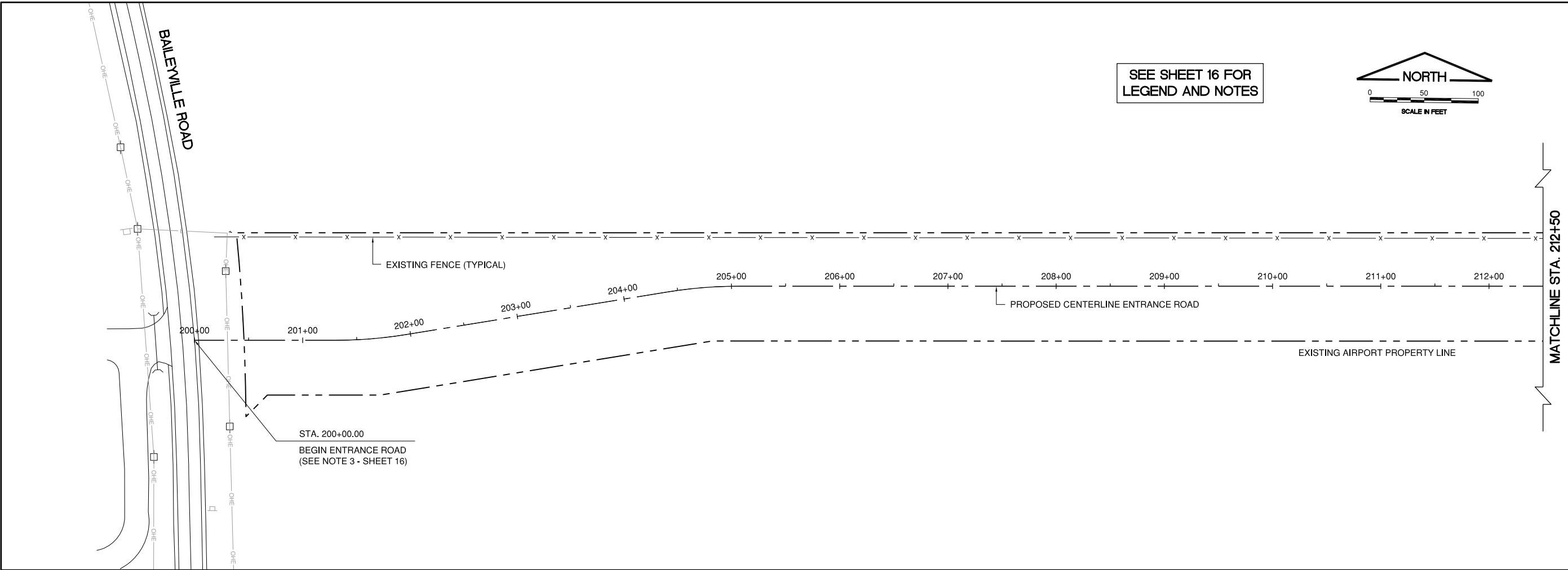
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IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

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 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

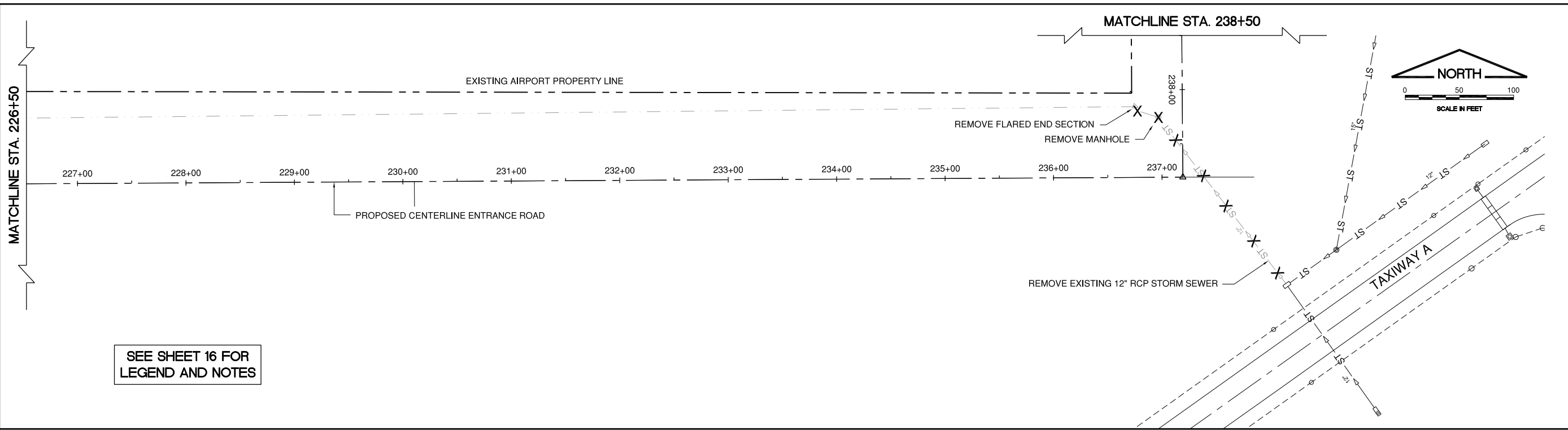
**EXISTING CONDITIONS AND PROPOSED REMOVALS
 SHEET 1**

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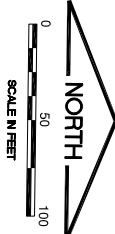
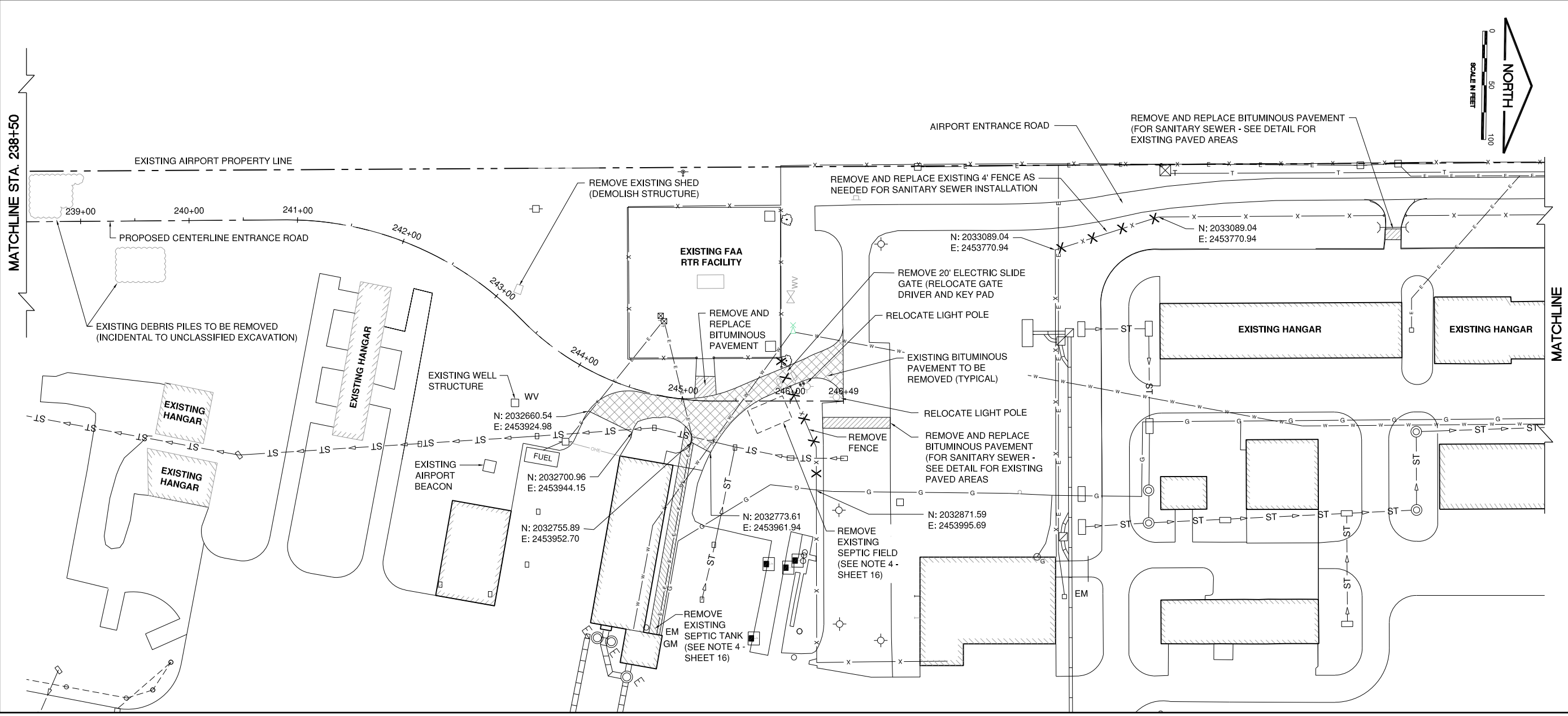
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SHEET 14 OF 52 SHEETS	

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 UPDATE BY: Jim Ohse
 LAYOUT: Sligden
 REF: DWG: Road Design
 IMAGE FILES:



SEE SHEET 16 FOR
LEGEND AND NOTES



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

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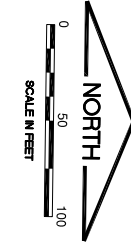
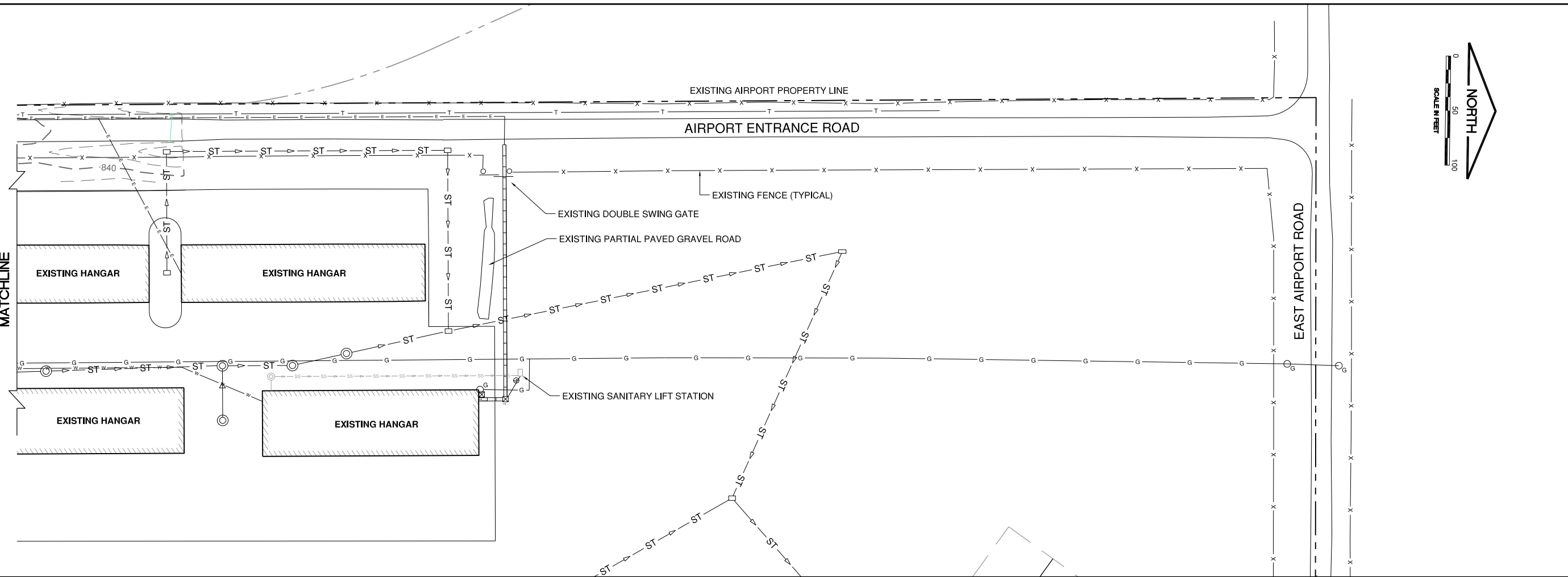
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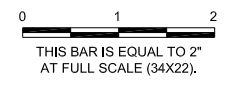
SHEET 15 OF 52 SHEETS



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

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**FREEPORT-ALBERTUS AIRPORT
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 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**EXISTING CONDITIONS AND PROPOSED REMOVALS
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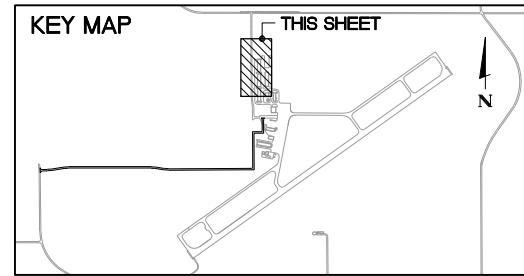
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JOB No:	17294-02

LEGEND

- EXISTING INLET/MANHOLE
- EXISTING LIGHT POLE
- EXISTING WATER VALVE
- EXISTING WELL
- EXISTING ROADWAY SIGN
- EXISTING UTILITY POLE
- EXISTING UTILITY STRUCTURE
- EXISTING GAS MAIN
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING CONDUIT/DUCT BANK
- EXISTING SANITARY SEWER
- EXISTING WATERLINE
- EXISTING FENCE
- AIRPORT PROPERTY LINE
- EXISTING CONTOUR
- EXISTING BUILDING
- REMOVAL ITEM
- REMOVE BITUMINOUS PAVEMENT
- REMOVAL AND REPLACEMENT BITUMINOUS PAVEMENT - TYPE B

NOTES

1. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
2. THE EXISTING BITUMINOUS PAVEMENT TO BE REMOVED SHALL BE SAWCUT AT THE LIMITS TO PROVIDE A CLEAN VERTICAL FACE. COST FOR SAWCUT SHALL BE INCLUDED IN THE CONTRACT.
3. THE CONTRACTOR SHALL SAWCUT 12" BACK (FULL DEPTH) FROM PROPOSED TIE-IN OF NEW ENTRANCE ROAD TO ESTABLISH A CLEAN JOINT. FULL DEPTH BITUMINOUS REMOVAL AND REPLACEMENT SHALL BE PAID FOR UNDER AR401916.
4. LIMITS OF EXISTING SEPTIC TANK SHOWN ARE APPROXIMATE. SEE SANITARY DETAILS FOR TYPICAL REMOVAL DEPTHS, BACKFILL REQUIREMENTS, AND OTHER RELATED NOTES.

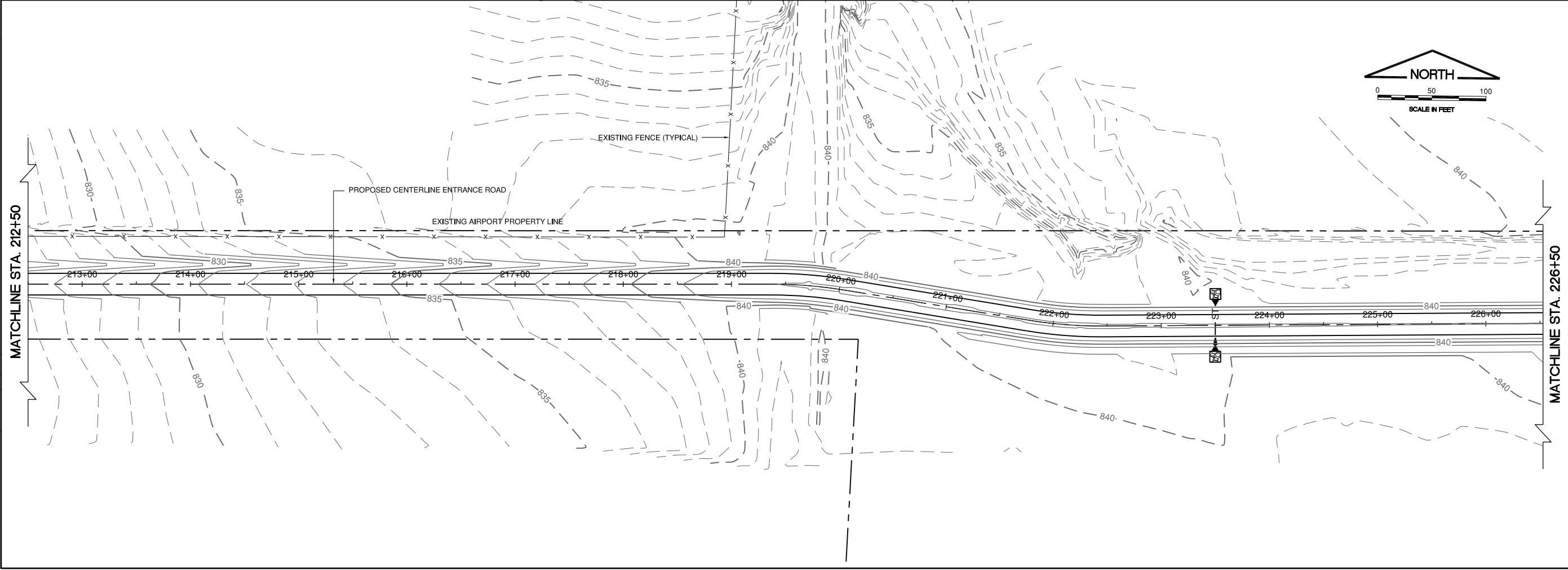
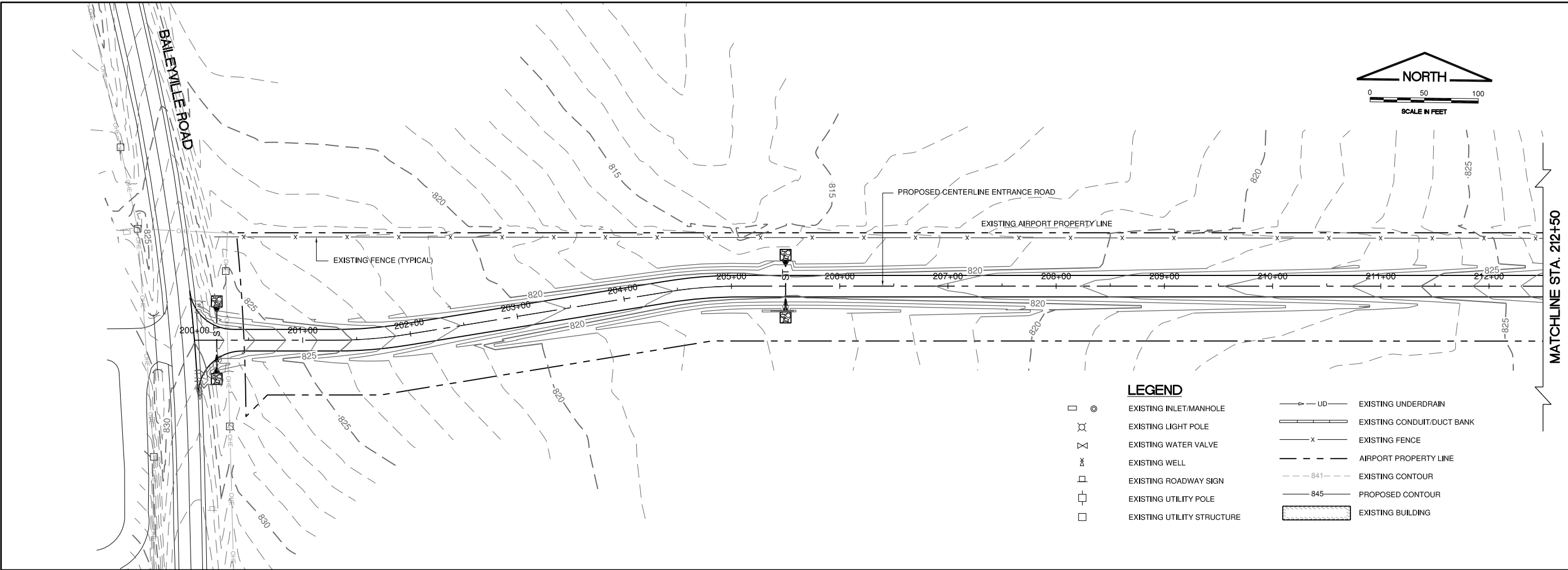


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LAYOUT: Layout

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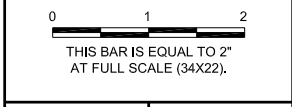
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IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

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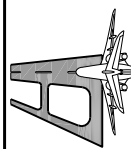
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FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**GRADING PLAN
SHEET 1**

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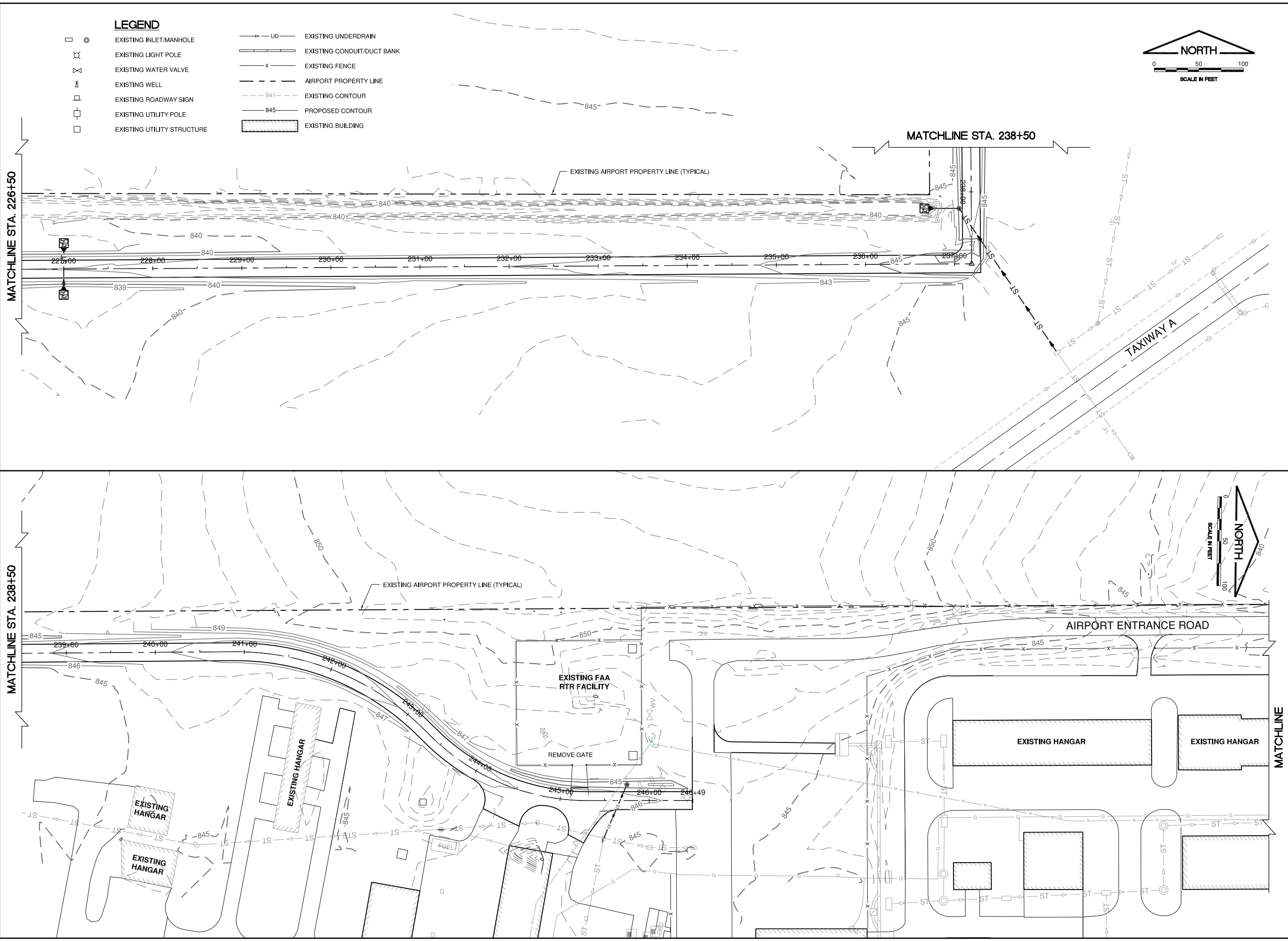


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APPROVED BY:	DJK
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JOB No:	17294-02
SHEET 17 OF 52 SHEETS	

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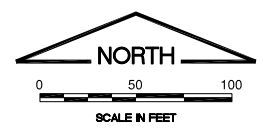
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LEGEND

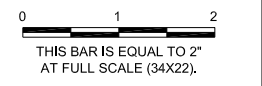
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- ⊗ EXISTING LIGHT POLE
- ⊗ EXISTING WATER VALVE
- ⊗ EXISTING WELL
- EXISTING ROADWAY SIGN
- EXISTING UTILITY POLE
- EXISTING UTILITY STRUCTURE
- UD — EXISTING UNDERDRAIN
- EXISTING CONDUIT/DUCT BANK
- x - EXISTING FENCE
- - - AIRPORT PROPERTY LINE
- - - 841 - - - EXISTING CONTOUR
- - - 845 - - - PROPOSED CONTOUR
- ▭ EXISTING BUILDING



IL. CONTRACT: FR042
 IL. LETTING ITEM: 14A
 IL. PROJECT: FEP-4626
 S.B.G. PROJECT: N/A


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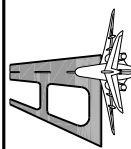


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD
 GRADING PLAN
 SHEET 2**

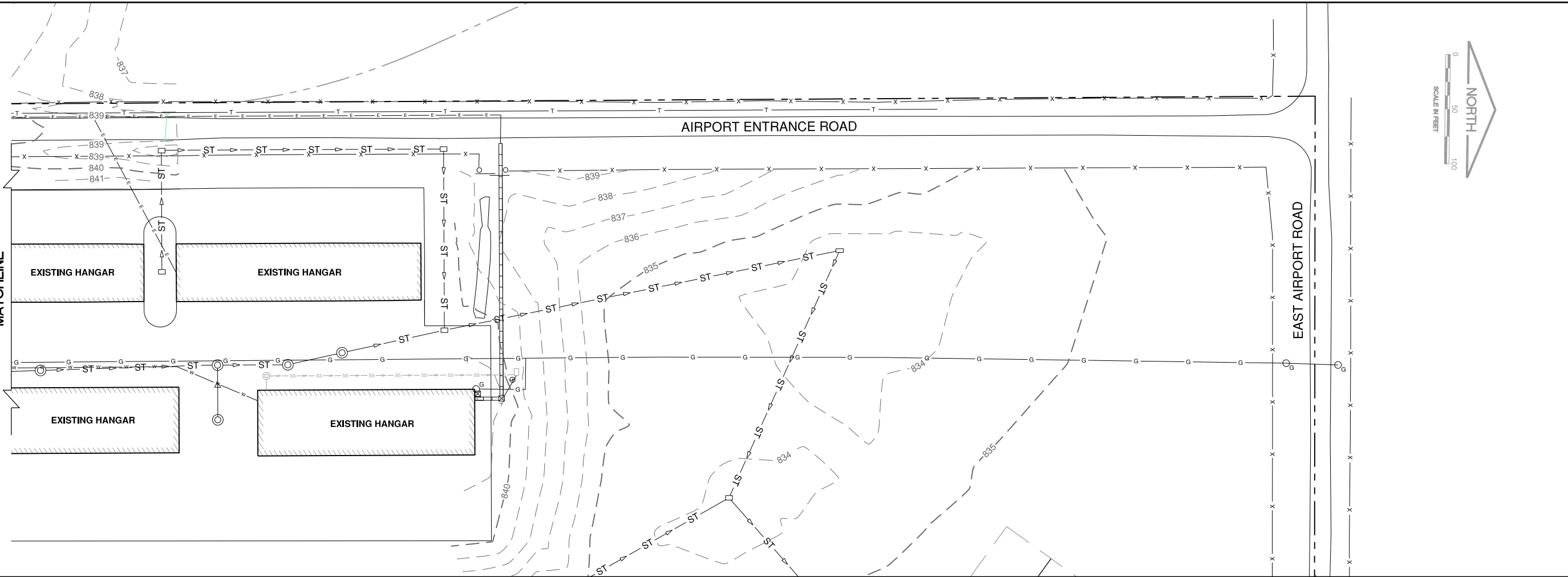
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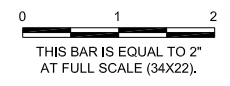
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APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK #

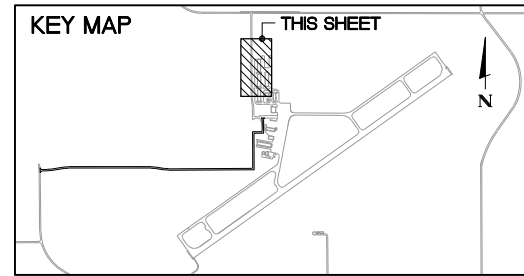
REVISIONS		
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**GRADING PLAN
SHEET 3**

- LEGEND**
- ● EXISTING INLET/MANHOLE
 - ⊗ EXISTING LIGHT POLE
 - ⊗ EXISTING WATER VALVE
 - ⊗ EXISTING WELL
 - EXISTING ROADWAY SIGN
 - EXISTING UTILITY POLE
 - EXISTING UTILITY STRUCTURE
 - EXISTING GAS MAIN
 - >ST— EXISTING STORM SEWER
 - X— EXISTING FENCE
 - AIRPORT PROPERTY LINE
 - - - 841 - - - EXISTING CONTOUR
 - ▨ EXISTING BUILDING



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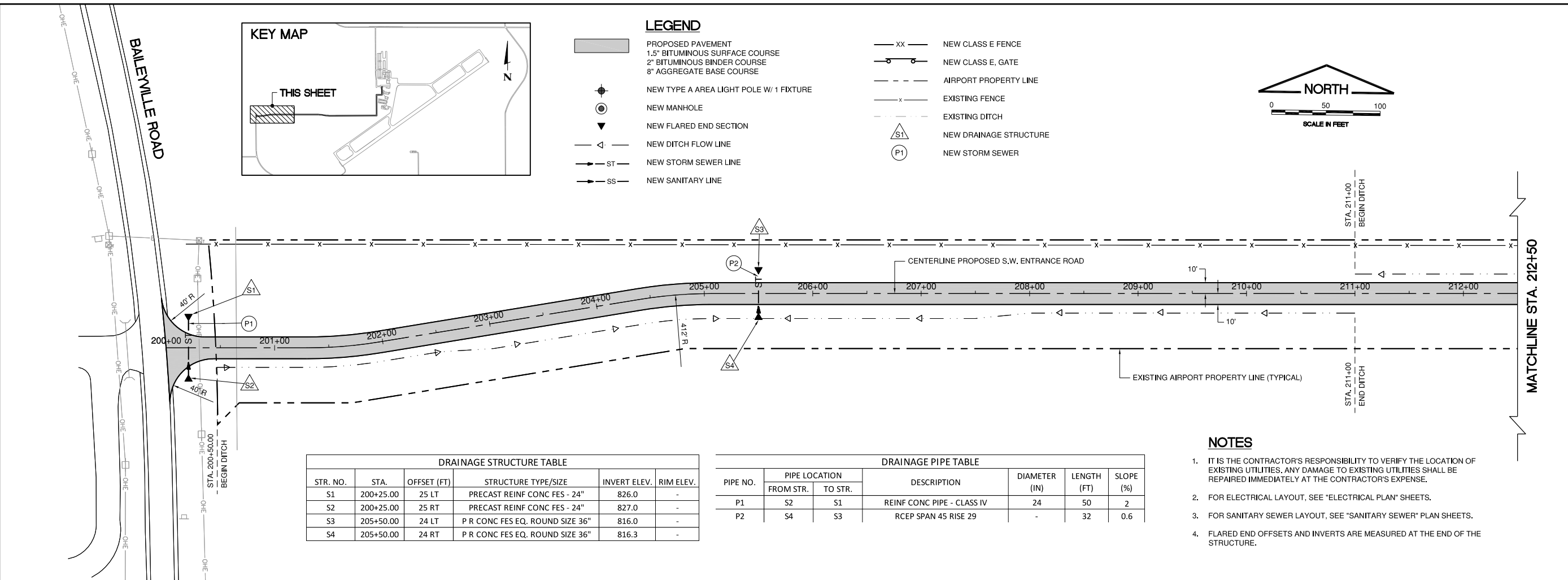
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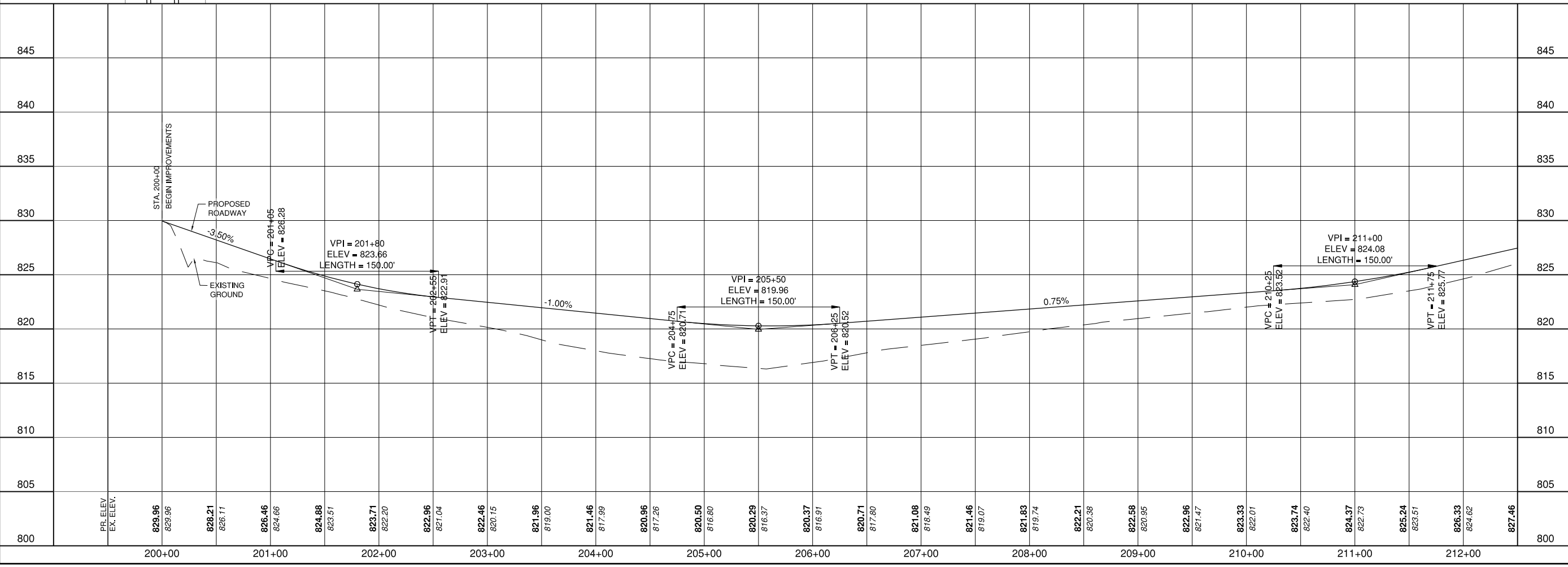
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STR. NO.	STA.	OFFSET (FT)	STRUCTURE TYPE/SIZE	INVERT ELEV.	RIM ELEV.
S1	200+25.00	25 LT	PRECAST REINF CONC FES - 24"	826.0	-
S2	200+25.00	25 RT	PRECAST REINF CONC FES - 24"	827.0	-
S3	205+50.00	24 LT	P R CONC FES EQ. ROUND SIZE 36"	816.0	-
S4	205+50.00	24 RT	P R CONC FES EQ. ROUND SIZE 36"	816.3	-

PIPE NO.	PIPE LOCATION		DESCRIPTION	DIAMETER (IN)	LENGTH (FT)	SLOPE (%)
	FROM STR.	TO STR.				
P1	S2	S1	REINF CONC PIPE - CLASS IV	24	50	2
P2	S4	S3	RCEP SPAN 45 RISE 29	-	32	0.6



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK #

NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**PLAN AND PROFILE
 SHEET 1**

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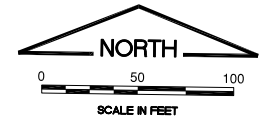
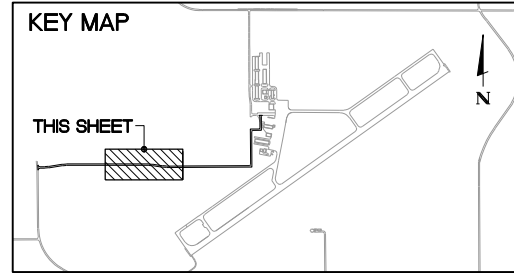
SHEET 20 OF 52 SHEETS

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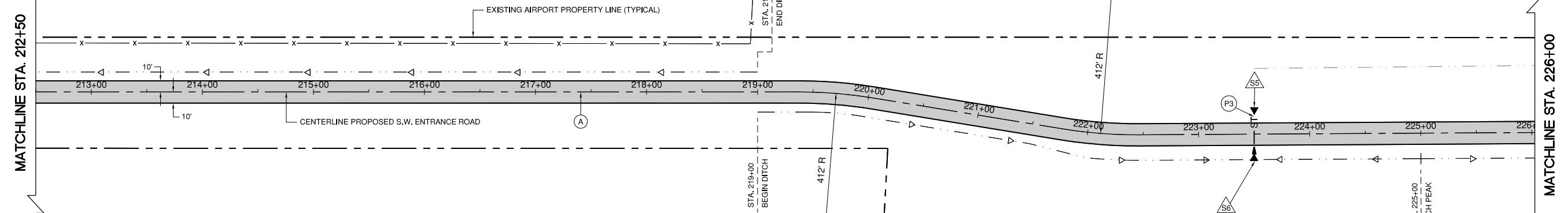
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LEGEND

	PROPOSED PAVEMENT 1.5" BITUMINOUS SURFACE COURSE 2" BITUMINOUS BINDER COURSE 8" AGGREGATE BASE COURSE		NEW CLASS E FENCE
	NEW TYPE A AREA LIGHT POLE W/ 1 FIXTURE		NEW CLASS E GATE
	NEW FLARED END SECTION		AIRPORT PROPERTY LINE
	NEW DITCH FLOW LINE		EXISTING FENCE
	NEW STORM SEWER LINE		EXISTING DITCH
	NEW SANITARY LINE		NEW DRAINAGE STRUCTURE
			NEW STORM SEWER



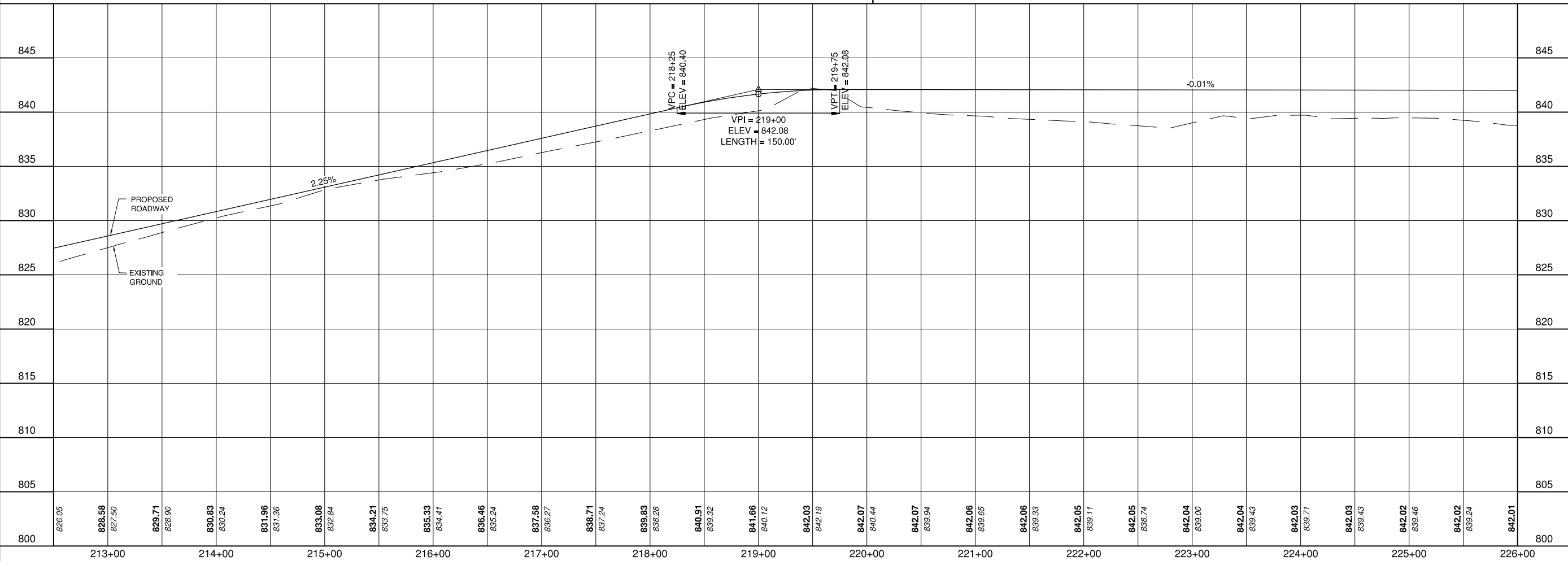
DRAINAGE STRUCTURE TABLE

STR. NO.	STA.	OFFSET (FT)	STRUCTURE TYPE/SIZE	INVERT ELEV.	RIM ELEV.
S5	223+50	24 LT	P R CONC FES EQ. ROUND SIZE 36"	837.9	-
S6	223+50	24 RT	P R CONC FES EQ. ROUND SIZE 36"	838.2	-

DRAINAGE STRUCTURE TABLE

PIPE NO.	PIPE LOCATION		DESCRIPTION	DIAMETER (IN)	LENGTH (FT)	SLOPE (%)
	FROM STR.	TO STR.				
P3	S6	S5	RCEP SPAN 45 RISE 29	-	32	0.6

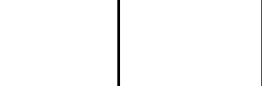
- NOTES**
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - FOR ELECTRICAL LAYOUT, SEE "ELECTRICAL PLAN" SHEETS.
 - FOR SANITARY SEWER LAYOUT, SEE "SANITARY SEWER" PLAN SHEETS.
 - FLARED END OFFSETS AND INVERTS ARE MEASURED AT THE END OF THE STRUCTURE.



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK #

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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**PLAN AND PROFILE
 SHEET 2**

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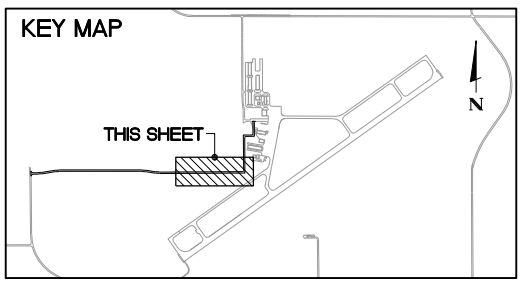
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UPDATE BY: Jim Ohse
 LAYOUT: Layout

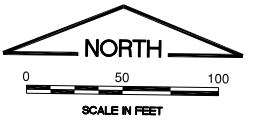
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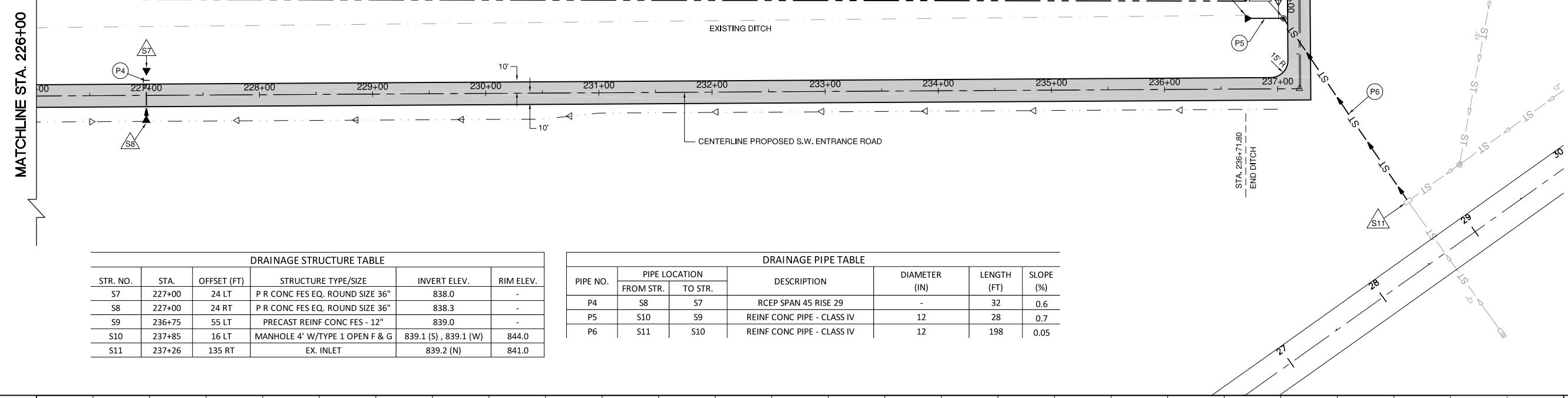


LEGEND

	PROPOSED PAVEMENT 1.5" BITUMINOUS SURFACE COURSE 2" BITUMINOUS BINDER COURSE 8" AGGREGATE BASE COURSE		NEW CLASS E FENCE
	NEW TYPE A AREA LIGHT POLE W/ 1 FIXTURE		NEW CLASS E GATE
	NEW MANHOLE		AIRPORT PROPERTY LINE
	NEW FLARED END SECTION		EXISTING FENCE
	NEW DITCH FLOW LINE		EXISTING DITCH
	NEW STORM SEWER LINE		NEW DRAINAGE STRUCTURE
	NEW SANITARY LINE		NEW STORM SEWER



- NOTES**
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - FOR ELECTRICAL LAYOUT, SEE "ELECTRICAL PLAN" SHEETS.
 - FOR SANITARY SEWER LAYOUT, SEE "SANITARY SEWER" PLAN SHEETS.
 - FLARED END OFFSETS AND INVERTS ARE MEASURED AT THE END OF THE STRUCTURE.



DRAINAGE STRUCTURE TABLE

STR. NO.	STA.	OFFSET (FT)	STRUCTURE TYPE/SIZE	INVERT ELEV.	RIM ELEV.
S7	227+00	24 LT	P R CONC FES EQ. ROUND SIZE 36"	838.0	-
S8	227+00	24 RT	P R CONC FES EQ. ROUND SIZE 36"	838.3	-
S9	236+75	55 LT	PRECAST REINF CONC FES - 12"	839.0	-
S10	237+85	16 LT	MANHOLE 4' W/TYPE 1 OPEN F & G	839.1 (S), 839.1 (W)	844.0
S11	237+26	135 RT	EX. INLET	839.2 (N)	841.0

DRAINAGE PIPE TABLE

PIPE NO.	PIPE LOCATION		DESCRIPTION	DIAMETER (IN)	LENGTH (FT)	SLOPE (%)
	FROM STR.	TO STR.				
P4	S8	S7	RCEP SPAN 45 RISE 29	-	32	0.6
P5	S10	S9	REINF CONC PIPE - CLASS IV	12	28	0.7
P6	S11	S10	REINF CONC PIPE - CLASS IV	12	198	0.05

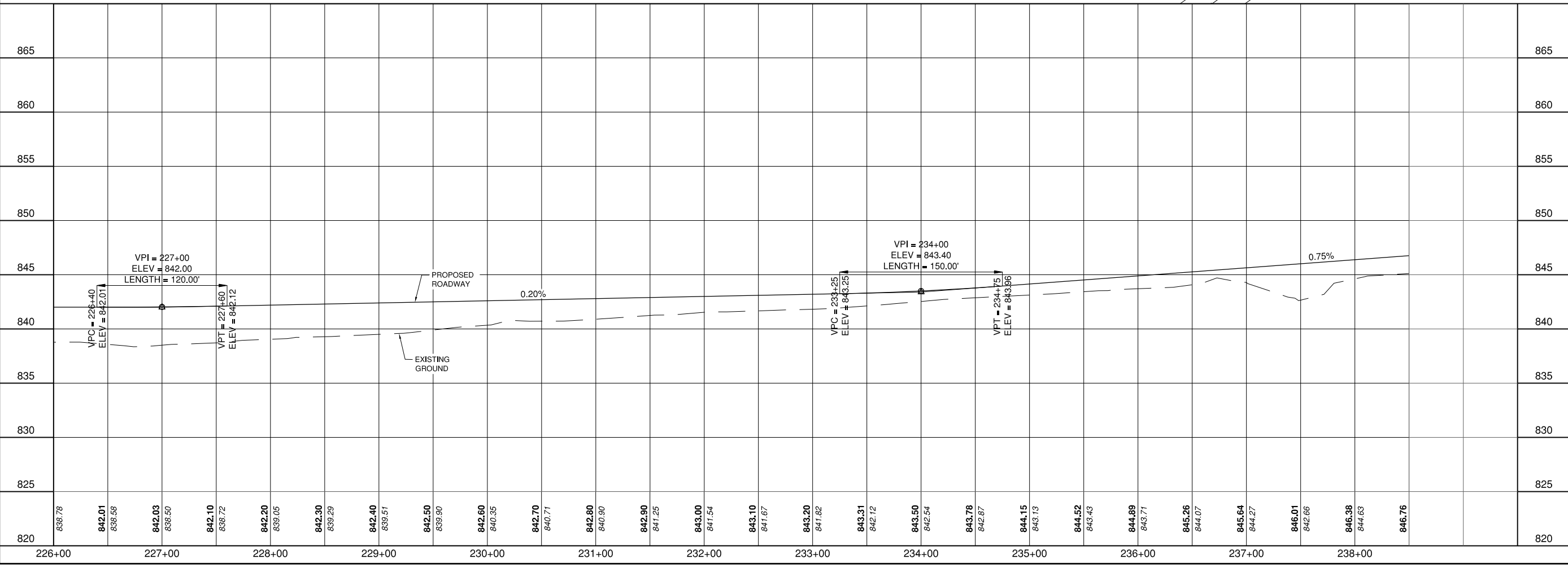
IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK # ###

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
PLAN AND PROFILE
SHEET 3



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DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

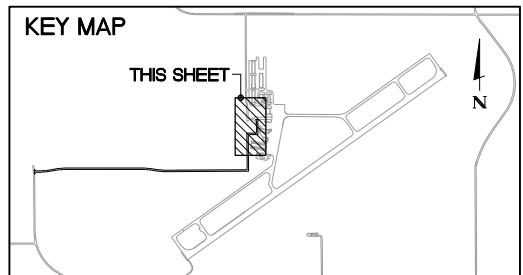
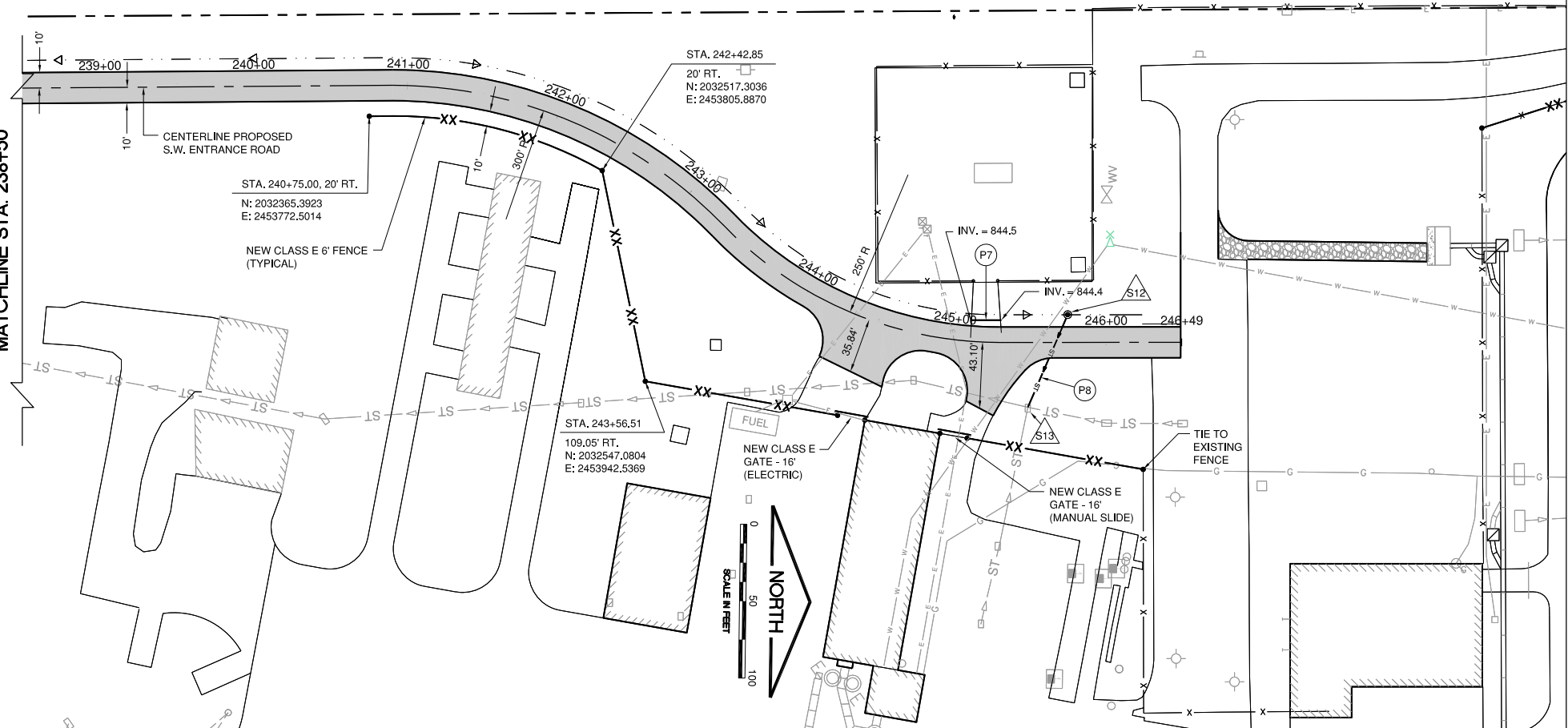
SHEET 22 OF 52 SHEETS

DATE: Friday, November 30, 2018, 2:03:25 PM
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UPDATE BY: Jim Ohse
 LAYOUT: Layout1

DATE: Friday, November 30, 2018, 2:03:25 PM
 FILE: K:\Freight\17294-02_EntranceRoad\Design\Drawn Sheets\17294-02.dwg

MATCHLINE STA. 238+50



NOTES

- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- FOR ELECTRICAL LAYOUT, SEE "ELECTRICAL PLAN" SHEETS.
- FOR SANITARY SEWER LAYOUT, SEE "SANITARY SEWER" PLAN SHEETS.
- FLARED END OFFSETS AND INVERTS ARE MEASURED AT THE END OF THE STRUCTURE.

LEGEND

- PROPOSED PAVEMENT
 1.5" BITUMINOUS SURFACE COURSE
 2" BITUMINOUS BINDER COURSE
 8" AGGREGATE BASE COURSE
- NEW TYPE A AREA LIGHT POLE W/ 1 FIXTURE
- NEW MANHOLE
- NEW FLARED END SECTION
- NEW DITCH FLOW LINE
- NEW STORM SEWER LINE
- NEW SANITARY LINE
- XX NEW CLASS E FENCE
- NEW CLASS E GATE
- AIRPORT PROPERTY LINE
- EXISTING FENCE
- EXISTING DITCH
- NEW DRAINAGE STRUCTURE
- NEW STORM SEWER

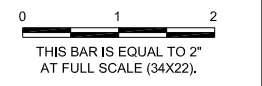
STR. NO.	STA.	OFFSET (FT)	STRUCTURE TYPE/SIZE	INVERT ELEV.	RIM ELEV.
S12	245+75	18 LT	TYPE A INLET (W/ TYPE 8 F&G)	842.4	844.0
S13	245+49	42.2 RT	EXISTING INLET	842.1 (W)	845.0

PIPE NO.	PIPE LOCATION		DESCRIPTION	DIAMETER (IN)	LENGTH (FT)	SLOPE (%)
	FROM STR.	TO STR.				
P7	INV = 844.5	INV = 844.4	CORR METAL PIPE	12"	20	0.5
P8	S12	S13	REINF CONC PIPE - CLASS IV	12"	65	0.5

IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

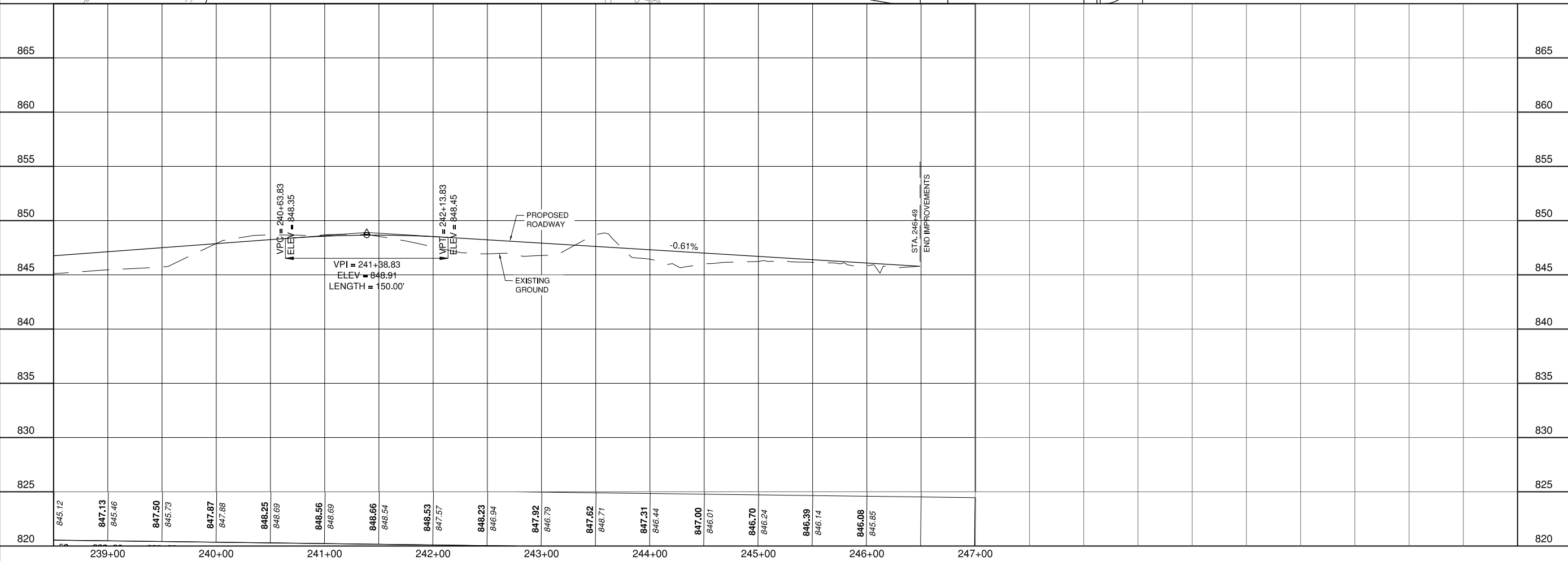
SURVEY BOOK # ###

REVISIONS		
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**PLAN AND PROFILE
 SHEET 4**



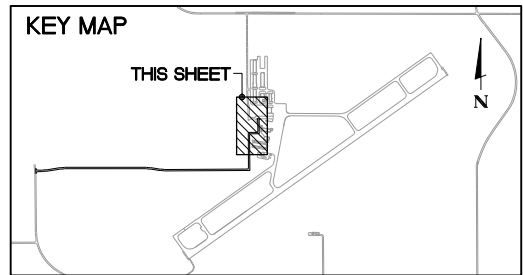
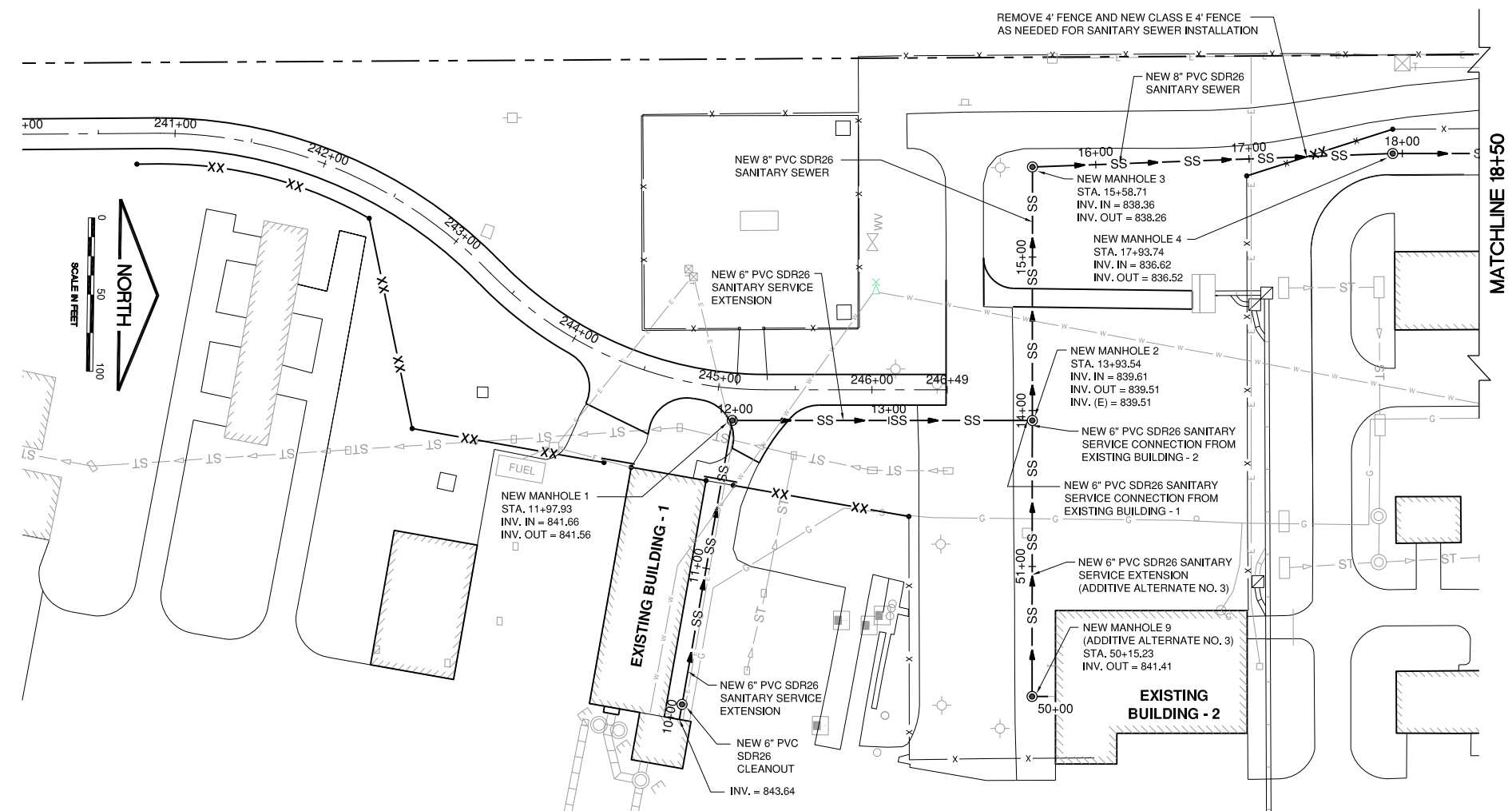
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CHECKED BY:	DJK
APPROVED BY:	DJK
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JOB No:	17294-02

SHEET 23 OF 52 SHEETS

DATE: Friday, November 30, 2018, 2:03:33 PM
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 UPDATE BY: Jim Ohse
 LAYOUT: Layout1
 XREF: DWG: CMT MCS Fielding
 IMAGE FILES:



SEPTIC SYSTEM NOTES

- CONSTRUCTION OF SEPTIC SYSTEM SHALL BE IN ACCORDANCE WITH REQUIREMENTS OF JOINT COMMITTEE ON ADMINISTRATIVE RULES ADMINISTRATION CODE, TITLE 77: PUBLIC HEALTH, CHAPTER 1: DEPARTMENT OF PUBLIC HEALTH, SUBCHAPTER 1: WATER AND SEWAGE, PART 905: PRIVATE SEWAGE DISPOSAL CODE.
- CONTRACTOR SHALL ADJUST GRADE TO PROVIDE SUFFICIENT COVER PER MANUFACTURER REQUIREMENTS AND TITLE 77: ILLINOIS ADMINISTRATIVE CODE 905.
- CONTRACTOR SHALL ADJUST GRADE TO LEVEL PRIOR TO INSTALLATION OF DISTRIBUTION BOX AND PIPES, AND INFILTRATION CHAMBERS PER MANUFACTURER RECOMMENDATIONS, AND TITLE 77: ILLINOIS ADMINISTRATIVE CODE 905.

LEGEND

- (A) PROPOSED PAVEMENT
1.5" BITUMINOUS SURFACE COURSE
2" BITUMINOUS BINDER COURSE
8" AGGREGATE BASE COURSE
- NEW ROADWAY SIGN
- NEW TYPE A AREA LIGHT POLE W/ 1 FIXTURE
- NEW FLARED END SECTION
- NEW DITCH FLOW LINE
- ST NEW STORM SEWER LINE
- SS NEW SANITARY LINE
- E NEW ELECTRIC LINE
- XX NEW CLASS E 6' FENCE

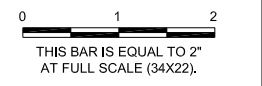
NOTE:

- CONTRACTOR SHALL FIELD VERIFY THE EXISTING GROUND ELEVATION PRIOR TO CONSTRUCTION.

SEE SHEET 26 FOR SANITARY NOTES

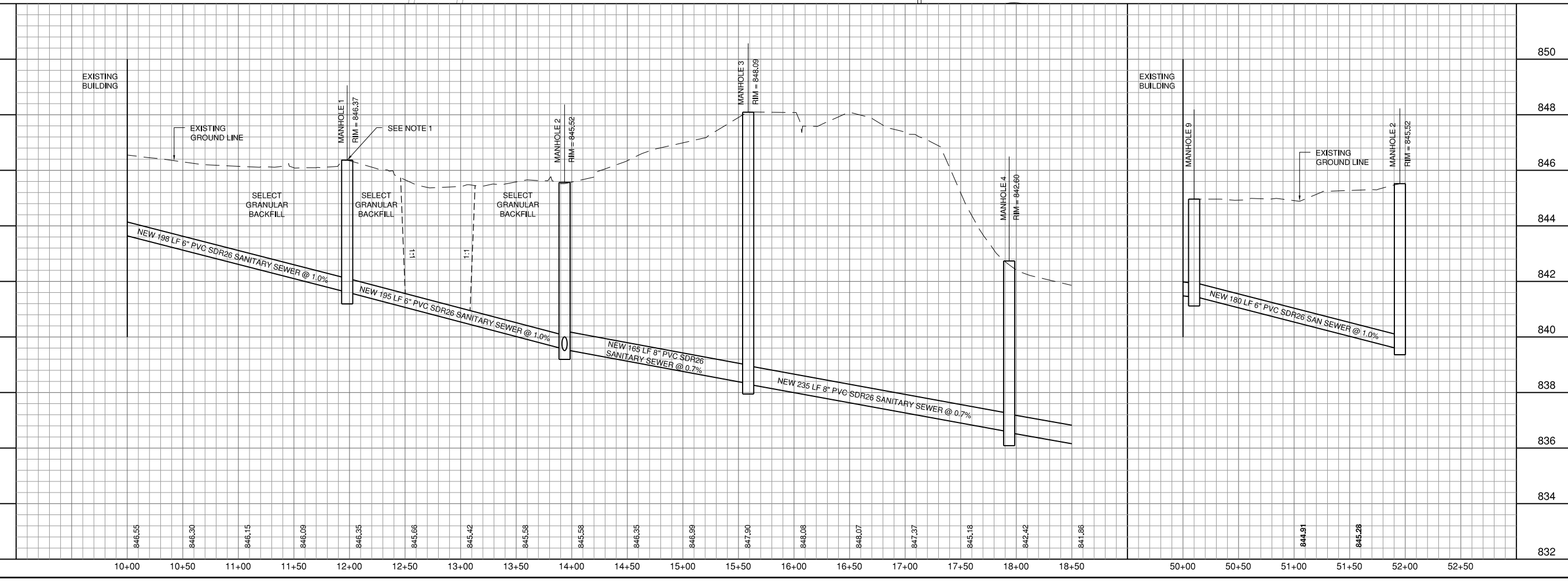
IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

REVISIONS		
NUMBER	BY	DATE



**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**SANITARY SEWER PLAN AND PROFILE
 SHEET 1**

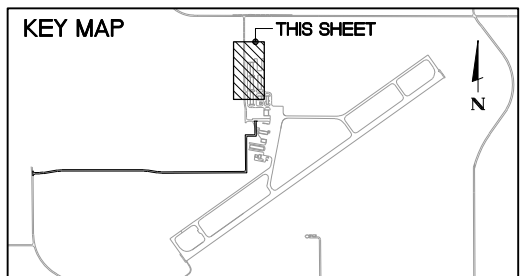
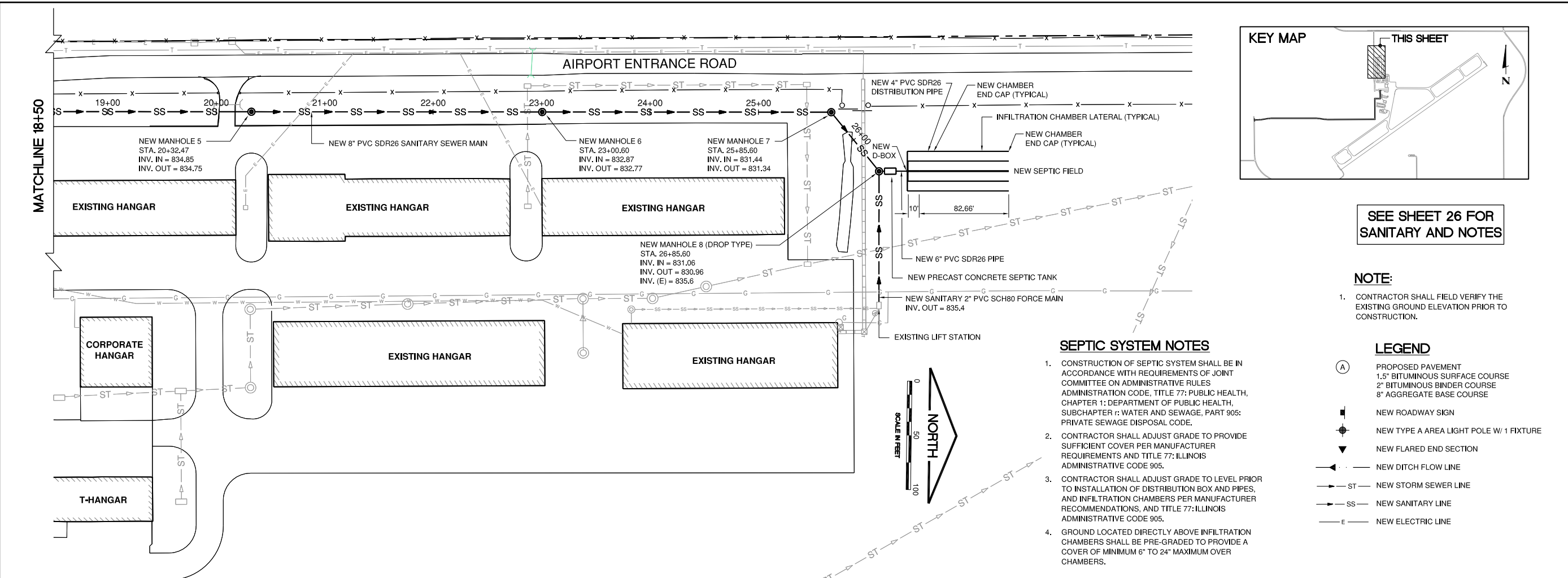


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DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

DATE: Friday, November 30, 2018, 2:12:05 PM
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 UPDATE BY: Jim Ohse
 LAYOUT: Layout1
 REF: DING: CMT MCS TRADING
 ENGINEERING CONSULTANTS
 1800 W. 15TH AVENUE
 SUITE 100
 DENVER, CO 80202
 PHONE: 303.733.8800
 FAX: 303.733.8801
 WWW: WWW.CMT-ENGINEERS.COM
 IMAGE FILES:



**SEE SHEET 26 FOR
SANITARY AND NOTES**

NOTE:

- CONTRACTOR SHALL FIELD VERIFY THE EXISTING GROUND ELEVATION PRIOR TO CONSTRUCTION.

LEGEND

- (A) PROPOSED PAVEMENT
1.5" BITUMINOUS SURFACE COURSE
2" BITUMINOUS BINDER COURSE
8" AGGREGATE BASE COURSE
- NEW ROADWAY SIGN
- NEW TYPE A AREA LIGHT POLE W/ 1 FIXTURE
- NEW FLARED END SECTION
- NEW DITCH FLOW LINE
- ST NEW STORM SEWER LINE
- SS NEW SANITARY LINE
- E NEW ELECTRIC LINE

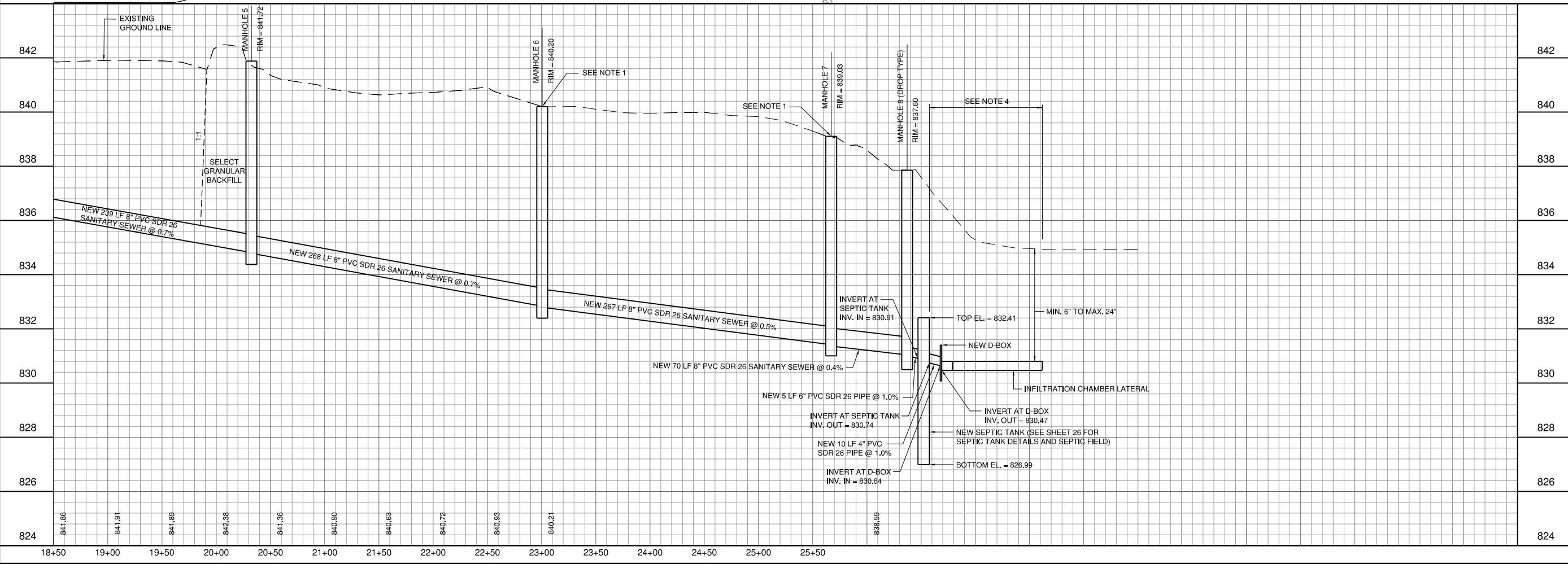
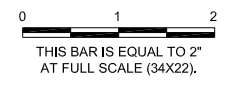
SEPTIC SYSTEM NOTES

- CONSTRUCTION OF SEPTIC SYSTEM SHALL BE IN ACCORDANCE WITH REQUIREMENTS OF JOINT COMMITTEE ON ADMINISTRATIVE RULES ADMINISTRATION CODE, TITLE 77: PUBLIC HEALTH, CHAPTER 1: DEPARTMENT OF PUBLIC HEALTH, SUBCHAPTER r: WATER AND SEWAGE, PART 905: PRIVATE SEWAGE DISPOSAL CODE.
- CONTRACTOR SHALL ADJUST GRADE TO PROVIDE SUFFICIENT COVER PER MANUFACTURER REQUIREMENTS AND TITLE 77: ILLINOIS ADMINISTRATIVE CODE 905.
- CONTRACTOR SHALL ADJUST GRADE TO LEVEL PRIOR TO INSTALLATION OF DISTRIBUTION BOX AND PIPES, AND INFILTRATION CHAMBERS PER MANUFACTURER RECOMMENDATIONS, AND TITLE 77: ILLINOIS ADMINISTRATIVE CODE 905.
- GROUND LOCATED DIRECTLY ABOVE INFILTRATION CHAMBERS SHALL BE PRE-GRADED TO PROVIDE A COVER OF MINIMUM 6" TO 24" MAXIMUM OVER CHAMBERS.

IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK # ###

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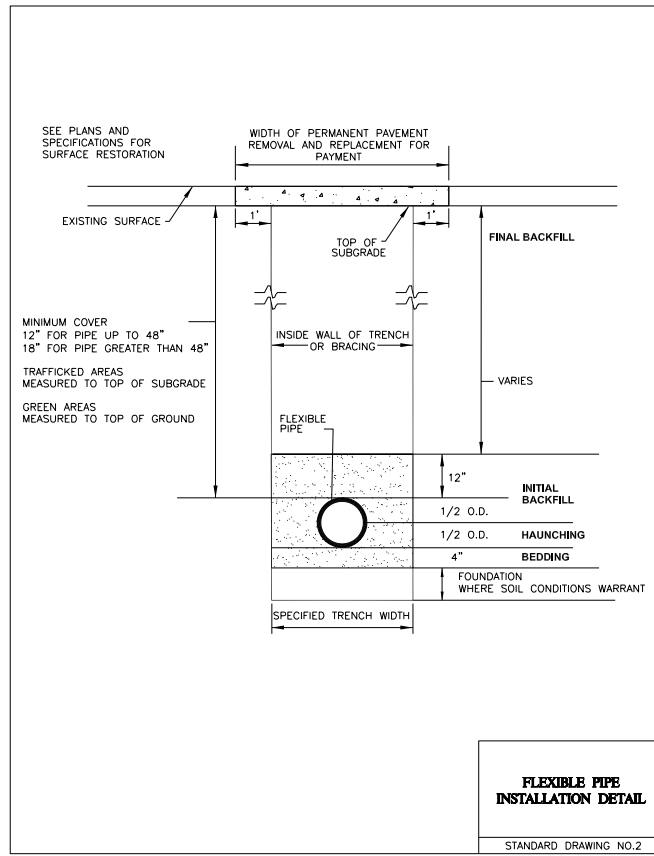
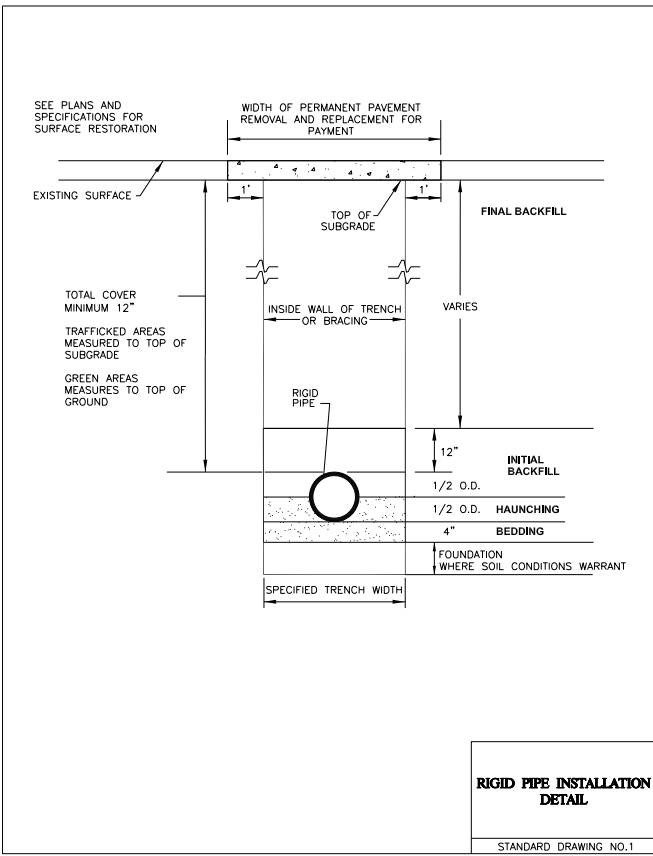
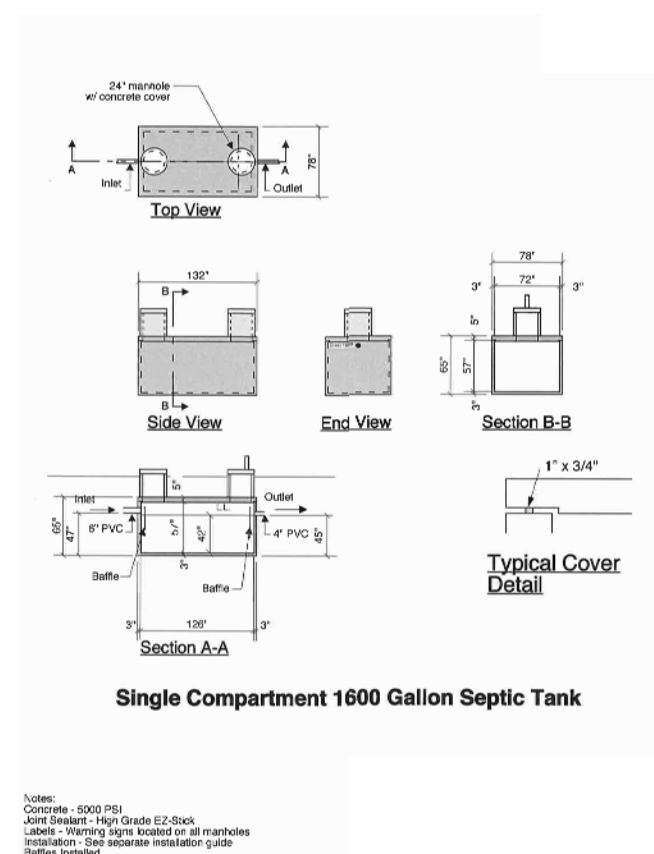
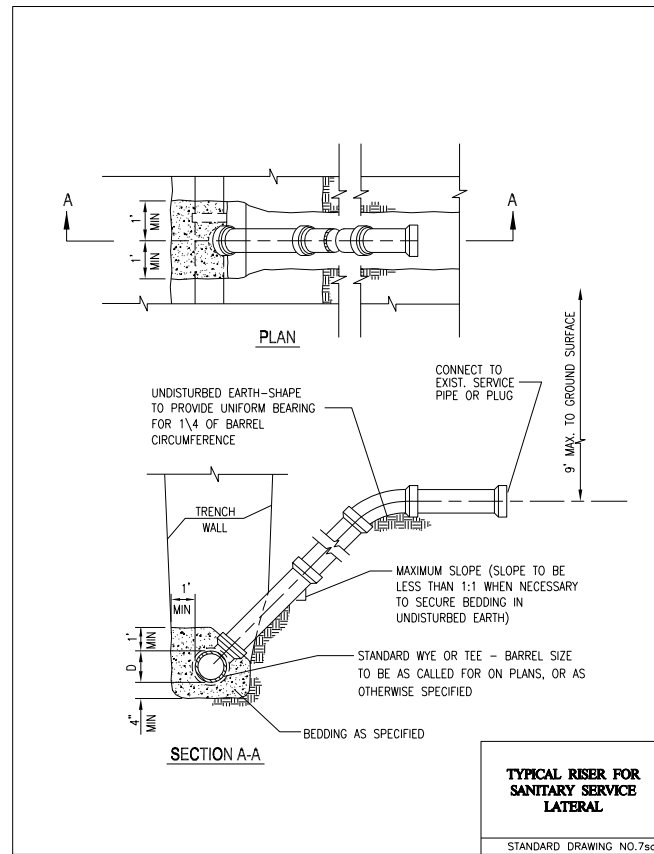
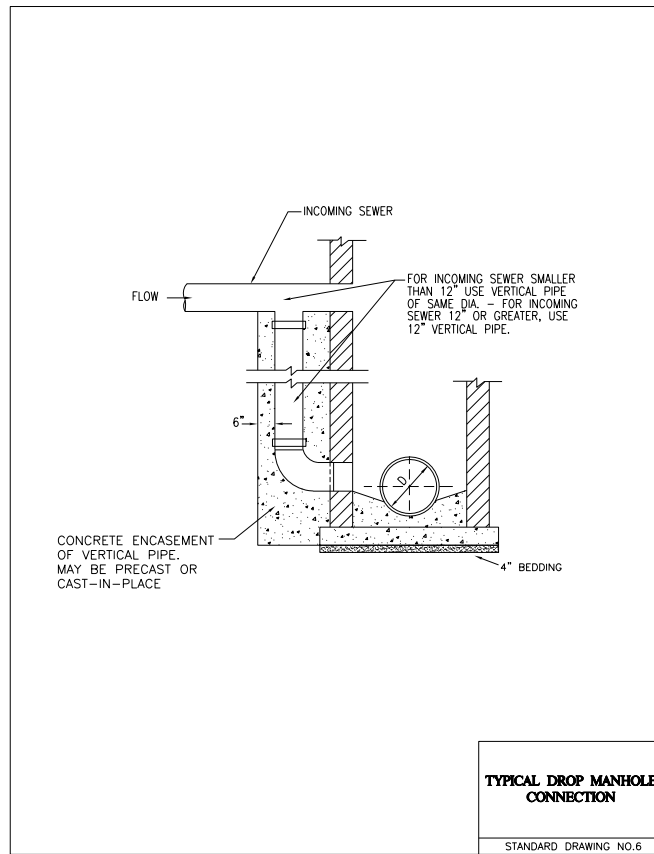
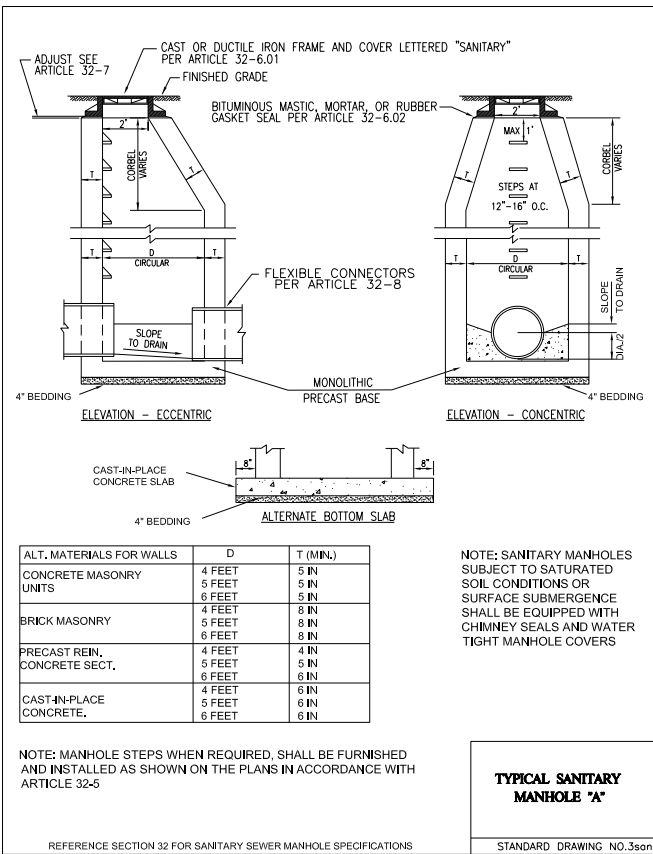
**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**SANITARY SEWER PLAN AND PROFILE
 SHEET 2**

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 CHECKED BY: DJK
 APPROVED BY: DJK
 DATE: 11-16-2018
 JOB No: 17294-02

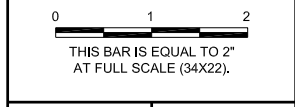


- SANITARY NOTES**
1. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF WORK TO THE LINES AND GRADES SHOWN ON THESE PLANS. CONSTRUCTION SHALL NOT VARY FROM PLANS WITHOUT CITY'S APPROVAL.
 2. THE CITY OF FREEPORT PUBLIC WORKS DEPARTMENT, AND THE AIRPORT SHALL BE NOTIFIED AT LEAST 48 HOURS BEFORE CONSTRUCTION CAN COMMENCE.
 3. ALL SERVICE LOCATIONS ARE APPROXIMATE AND MAY BE CHANGED IN THE FIELD WITH THE APPROVAL OF THE ENGINEER. NO ADDITIONAL COMPENSATION WILL BE AWARDED DUE TO REVISED LOCATION.
 4. ALL CASTINGS SHALL BE SET TO FINAL GRADE PER CITY OF FREEPORT.
 5. TRENCHES MUST BE CONSTRUCTED TO GRADE OR SUBGRADE, AS APPLICABLE, BEFORE AIR AND DEFLECTION TESTS ARE PERFORMED.
 6. SANITARY SEWER SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION IN THE STATE OF ILLINOIS, LATEST EDITION, AND THE REQUIREMENTS OF THE CITY OF FREEPORT, ILLINOIS.
 7. PROTECTION OF WATER SUPPLIES SHALL BE PER THE REQUIREMENTS OF TITLE 35, C, SECTION 370.350 OF THE ILLINOIS RECOMMENDED STANDARDS FOR SEWAGE WORKS.
 8. ALL SEWER MAINS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD OF SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS (CURRENT EDITION) AND THE REQUIREMENTS OF THE CITY OF FREEPORT.
 9. UPON COMPLETION OF CONSTRUCTION, ALL SANITARY MANHOLE CASTINGS SHALL BE EXPOSED AND SET AT FINAL GRADE.

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK # ####

REVISIONS		
NUMBER	BY	DATE



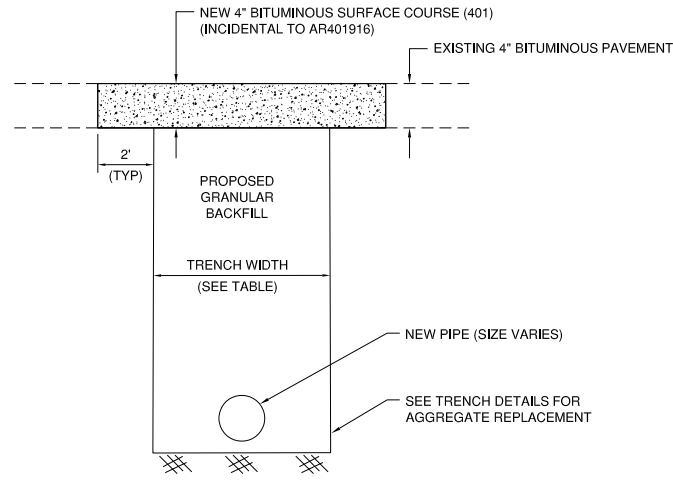
**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

SANITARY AND SEPTIC DETAILS

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CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

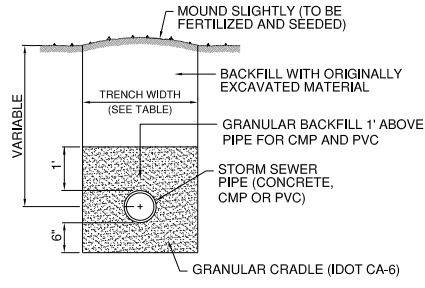
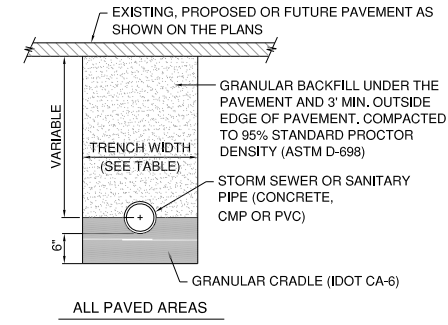


REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B DETAIL

NOT TO SCALE

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. SELECT GRANULAR BACKFILL SHALL BE PLACED 3 FEET (MIN.) BEYOND EDGE OF PAVEMENT.

INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH	MAXIMUM PAVEMENT REMOVAL WIDTH
6	3'-7"	5'-7"
8	3'-9"	5'-9"
12	4'-2"	6'-2"
15	4'-6"	6'-6"
18	4'-9"	6'-9"
21	5'-0"	7'-0"
24	5'-4"	7'-4"
27	5'-7"	7'-7"
30	5'-11"	7'-11"
36	6'-6"	8'-6"
42	7'-1"	9'-1"
48	7'-8"	9'-8"
54	8'-3"	10'-3"
60	8'-10"	10'-10"
66	9'-5"	11'-5"
72	10'-0"	12'-0"
78	10'-7"	12'-7"
84	11'-2"	13'-2"
90	11'-9"	13'-9"
96	12'-4"	14'-4"
102	12'-11"	14'-11"
108	13'-6"	15'-6"

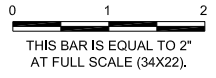


TRENCH DETAILS
NOT TO SCALE

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK # ###

REVISIONS		
NUMBER	BY	DATE

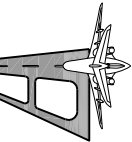


**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

MISCELLANEOUS DETAILS

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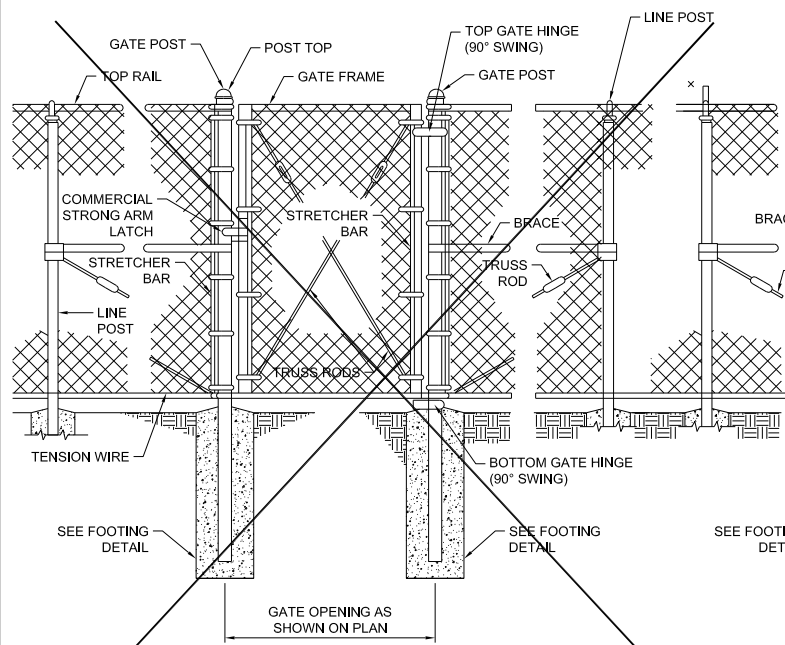
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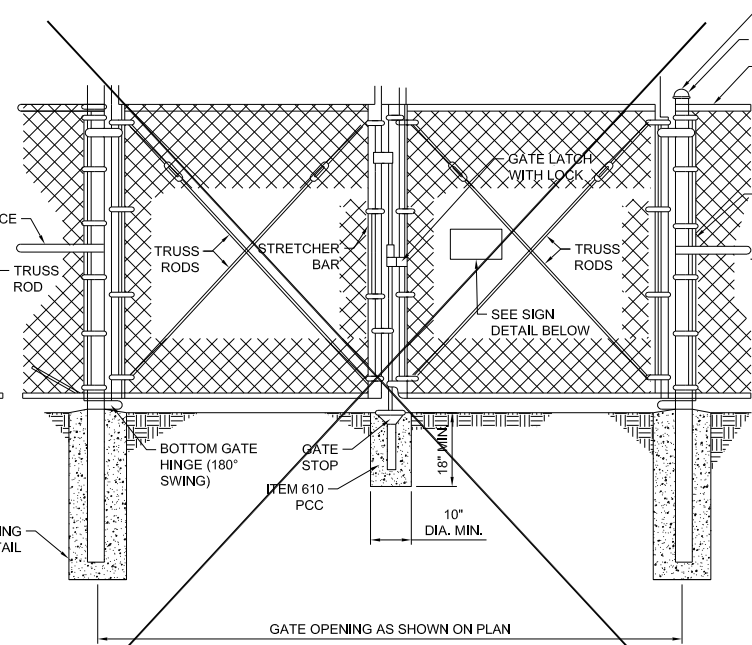
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LAYOUT: Layout

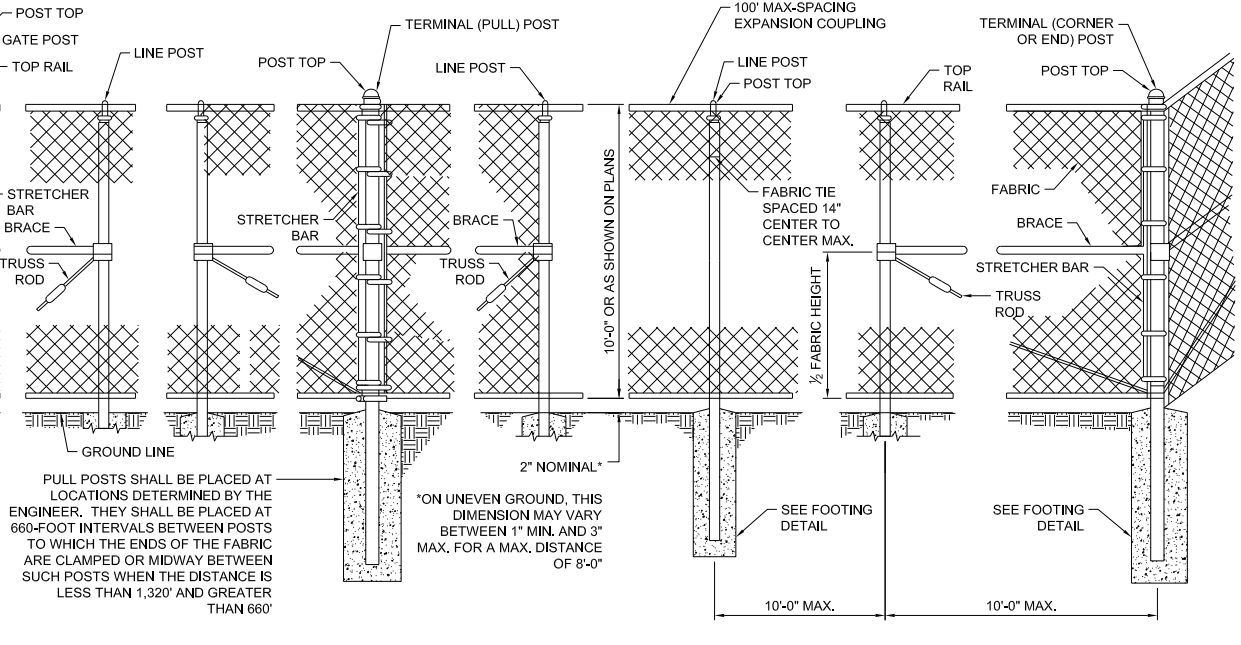
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PEDESTRIAN GATE ARRANGEMENT



VEHICLE GATE ARRANGEMENT



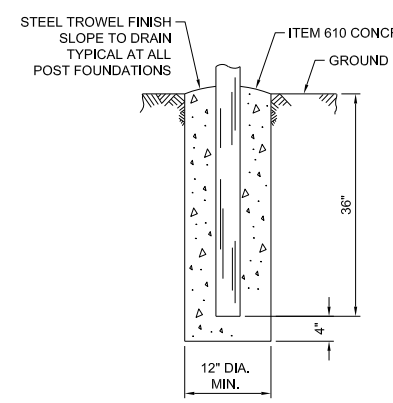
PULL POST ARRANGEMENT

LINE POST ARRANGEMENT

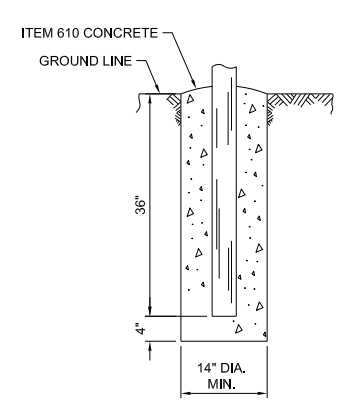
CORNER OR END POST ARRANGEMENT

PULL POSTS SHALL BE PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THEY SHALL BE PLACED AT 660-FOOT INTERVALS BETWEEN POSTS TO WHICH THE ENDS OF THE FABRIC ARE CLAMPED OR MIDWAY BETWEEN SUCH POSTS WHEN THE DISTANCE IS LESS THAN 1,320' AND GREATER THAN 660'

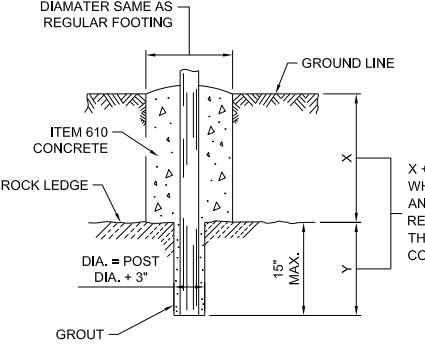
*ON UNEVEN GROUND, THIS DIMENSION MAY VARY BETWEEN 1' MIN. AND 3' MAX. FOR A MAX. DISTANCE OF 8'-0"



FOOTING FOR LINE POST

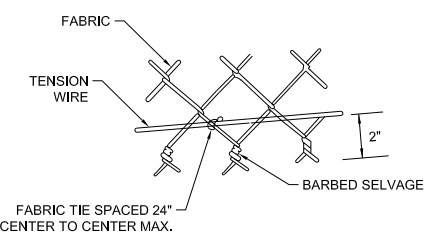


FOOTING FOR GATE & TERMINAL POST

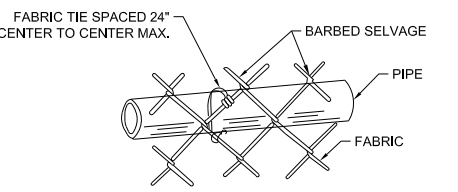


FOOTING FOR POST IN ROCK LEDGE

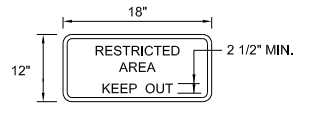
X + Y SHALL NOT EXCEED 36 INCHES. WHEN X IS 21 INCHES, THEN Y = 15 INCHES AND THE POST SHALL BE SHORTENED AS REQUIRED. WHEN X EXCEEDS 21 INCHES, THEN Y SHALL BE DECREASED CORRESPONDINGLY.



METHOD OF TYING FABRIC TO TENSION WIRE

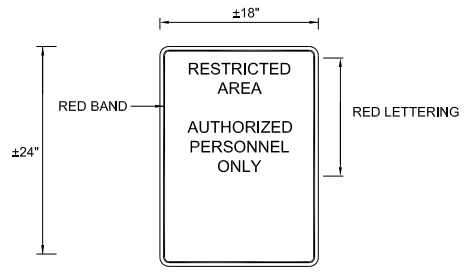


METHOD OF TYING FABRIC TO PIPE



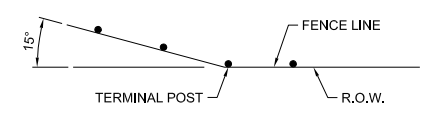
SIGN DETAILS

- NOTES:
- EVERY 100' OF FENCE SHALL REQUIRE ONE "RESTRICTED" SIGN. COST INCIDENTAL TO FENCE.
 - 0.08 GA ALUMINUM ALLOY SHEET. LETTERING SHALL BE RED ON A WHITE BACKGROUND.



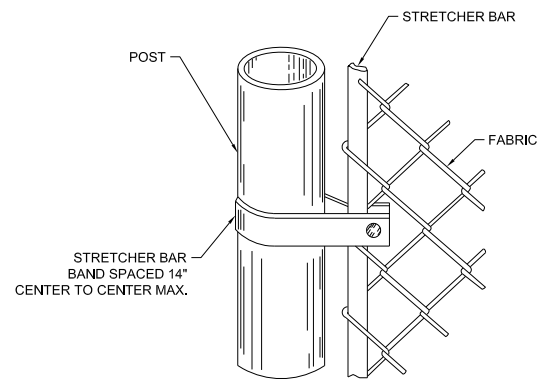
SIGN DETAILS

- NOTES:
- EACH PEDESTRIAN, SWING OR SLIDE GATE SHALL REQUIRE ONE "RESTRICTED" ON OR ADJACENT TO THE GATE.
 - 0.08 GA ALUMINUM ALLOY WITH A WHITE BACKGROUND.
 - CONTRACTOR SHALL VERIFY PANEL AND FONT SIZE WITH EXISTING AIRPORT SIGNS.
 - CANTILEVERED SLIDE GATES SHALL INCLUDE ONE ADDITIONAL SIGN AS DETAILED ON THE CANTILEVER GATE DETAIL SHEETS.



INSTALLATION AT CORNERS

- NOTES:
- WHEN THE FENCE LINE HAS A CHANGE IN DIRECTION OF 15° OR MORE, A TERMINAL POST SHALL BE PLACED AS SHOWN ABOVE.
- WHERE THE ANGLE IS LESS THAN 15° AND EXISTING CONDITIONS REQUIRE A TERMINAL POST, THEY SHALL BE PLACED AS DIRECTED BY THE ENGINEER.

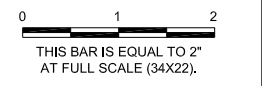


METHOD OF FASTENING STRETCHER BAR TO POST

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK # ###

REVISIONS		
NUMBER	BY	DATE



FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
FENCING DETAILS
SHEET 1

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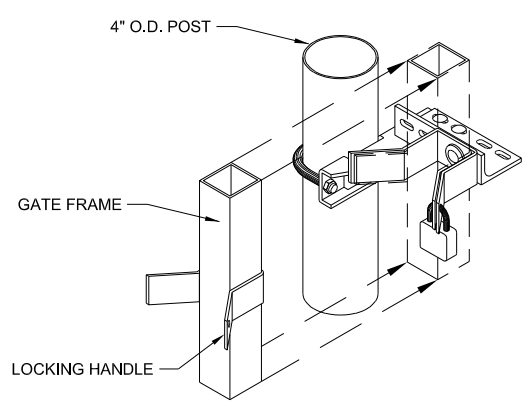
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DRAWN BY:	JRO
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

XREF DWG: CMT NCS TRAILING

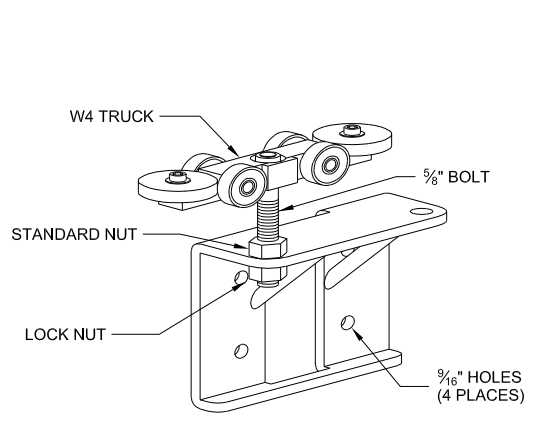
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UPDATE BY: Jim Chise
LAYOUT: Layout

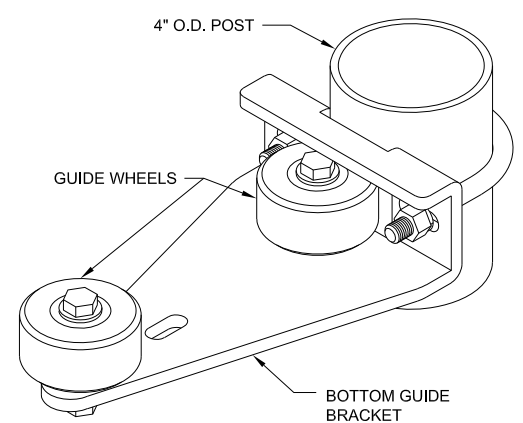
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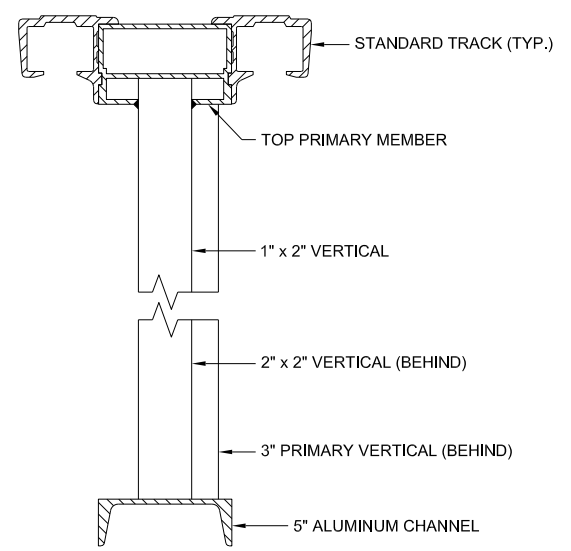
CATCH ASSEMBLY DETAIL
N.T.S.



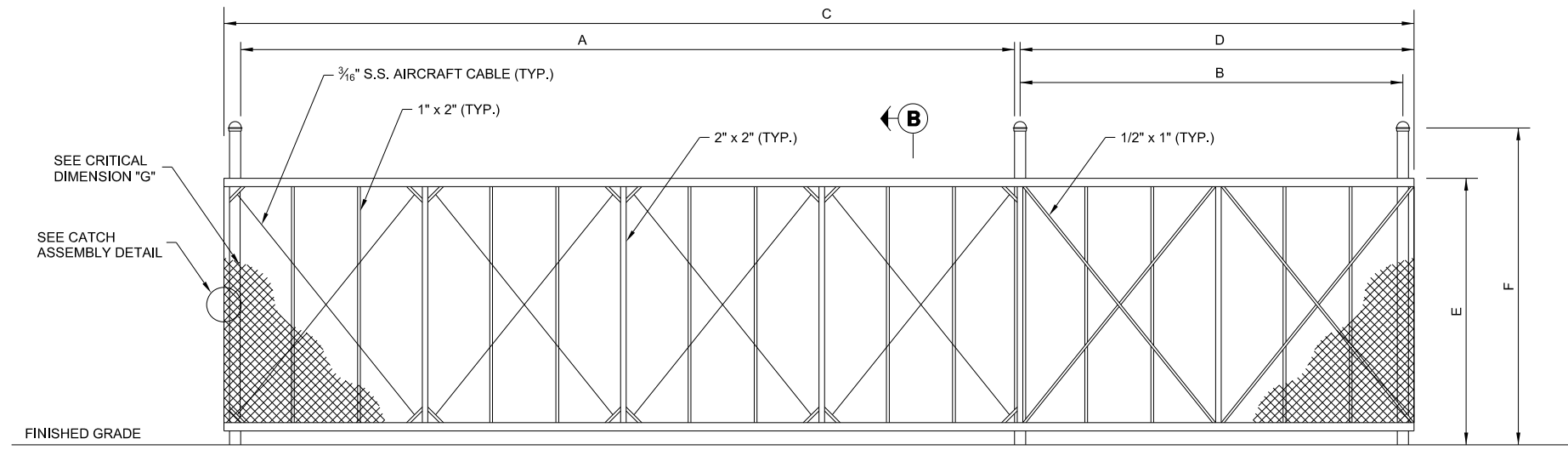
GATE HANGER ASSEMBLY
N.T.S.



STANDARD BOTTOM GUIDE ASSEMBLY
N.T.S.



GATE FRAME SECTION
N.T.S.



ELEVATION
CANTILEVER SLIDE GATE
N.T.S.

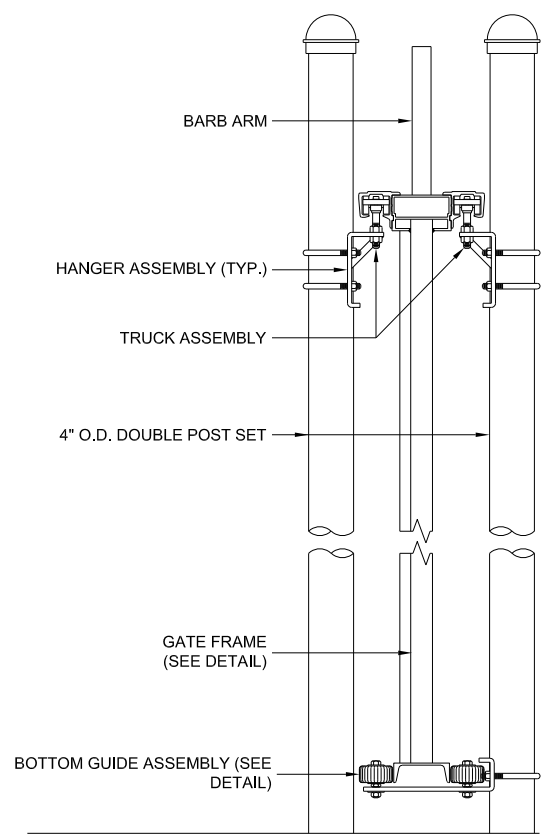
CRITICAL DIMENSIONS		
A	CLEAR OPENING	* 16'-0"
B	COUNTER BALANCE POST SPACING, CENTER TO CENTER	7'-1"
C	OVERALL GATE LENGTH	24'-0"
D	COUNTERBALANCE LENGTH	8'-0"
E	NOMINAL GATE HEIGHT	5'-0"
F	POST HEIGHT	5'-3"
G	FABRIC HEIGHT	4'-0"

* GATE FOR 6' CLASS E FENCE

UL 235 COMPLIANCE NOTES

GATE INSTALLATION SHALL COMPLY WITH ALL REQUIREMENTS OF UL 235, INCLUDING, BUT NOT LIMITED TO:

- ALL OPENINGS OF THE SLIDE GATE ARE GUARDED OR SCREENED FROM THE BOTTOM OF THE GATE TO A MINIMUM OF 4 FEET ABOVE GROUND TO PREVENT A 2-1/4" DIAMETER SPHERE FROM PASSING THROUGH THE OPENINGS ANYWHERE IN THE GATE, AND IN THAT PORTION OF THE ADJACENT FENCE THAT THE GATE COVERS IN THE OPEN POSITION.
- ALL EXPOSED PINCH POINTS ARE ELIMINATED OR GUARDED AND GUARDING IS SUPPLIED FOR ALL EXPOSED ROLLERS.
- A WARNING SIGN (SEE DETAIL, THIS SHEET) MUST BE AFFIXED TO BOTH SIDES OF THE SLIDE GATE, BOTH MANUAL AND ELECTRIC.



NOTE:
EXTRUSIONS SHALL BE OVERSIZED FOR EXTRA RIGIDITY OVER "STANDARD" GATE DESIGN.

SECTION B-B
(TYPICAL ALL GATES)

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.

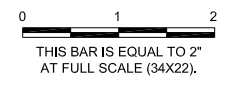
MANUAL SLIDE GATE NOTES:

- CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
- CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO: GATE, POSTS, AND ALL CONNECTIONS, SIGNS, LABOR AND MATERIALS NECESSARY FOR COMPLETE OPERATION.
- THE FABRIC TYPE & FINISH OF THE GATE SHALL MATCH THE PROPOSED FENCE OR BE AS DIRECTED BY THE ENGINEER.
- ALL SLIDING GATES SHALL HAVE ALL ROLLERS ENCLOSED IN STEEL OR PLASTIC SHROUDS TO PREVENT ACCIDENTAL INJURY.
- SEE FENCING DETAILS SHEET 1 FOR ADDITIONAL SIGN DETAILS.

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK # ###

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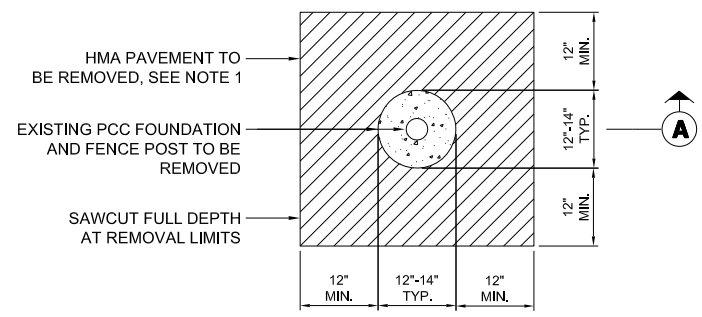
FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD
FENCING DETAILS
SHEET 2

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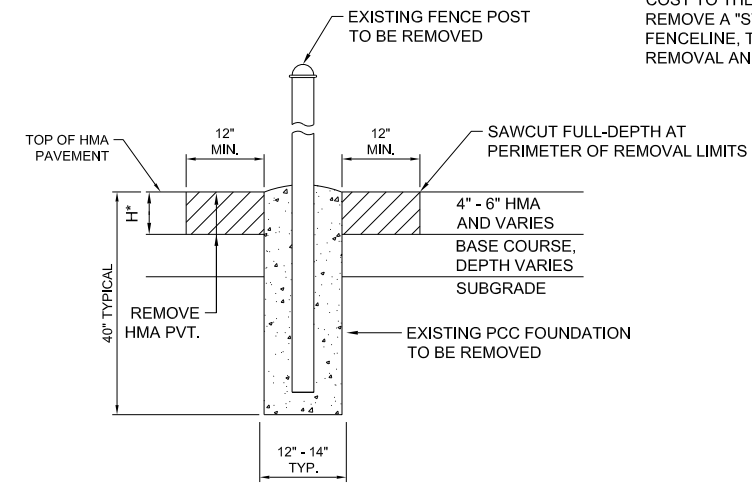
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DRAWN BY:	JRO
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

DATE: Friday, November 30, 2018, 2:04:14 PM
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 UPDATE BY: Jim Chise
 LAYOUT: Layout
 XREF DWG: CMT NCS Forming
 IMAGE FILES:



PLAN



SECTION A-A

FENCE POST REMOVAL IN HMA PAVEMENT

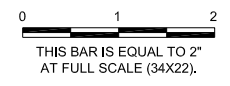
NOTES:

- HMA PAVEMENT TO BE REMOVED FULL DEPTH
- BASE COURSE AND/OR SUBGRADE TO BE REMOVED SUCH THAT THE DEPTH "H" IS A MINIMUM OF 6 INCHES.
- WHEN THE NEW FENCE ROUTE IS THE SAME AS THE OLD FENCE ROUTE, NEW FENCE SHALL REUSE EXISTING POST HOLES. SEE DETAIL FOR NEW FENCE POST FOUNDATION IN EXISTING HMA PAVEMENT.
- COSTS OF PAVEMENT AND FOUNDATION REMOVAL, SAW CUTTING, CLEANUP AND DISPOSAL, SHALL BE CONSIDERED INCIDENTAL TO AR162900, REMOVE CLASS E FENCE.
- AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, THE CONTRACTOR MAY REMOVE A "STRIP" OF ASPHALT PAVEMENT ALONG THE FENCELINE, TO BE REPLACED IN KIND AFTER FENCELINE REMOVAL AND REPLACEMENT.

IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

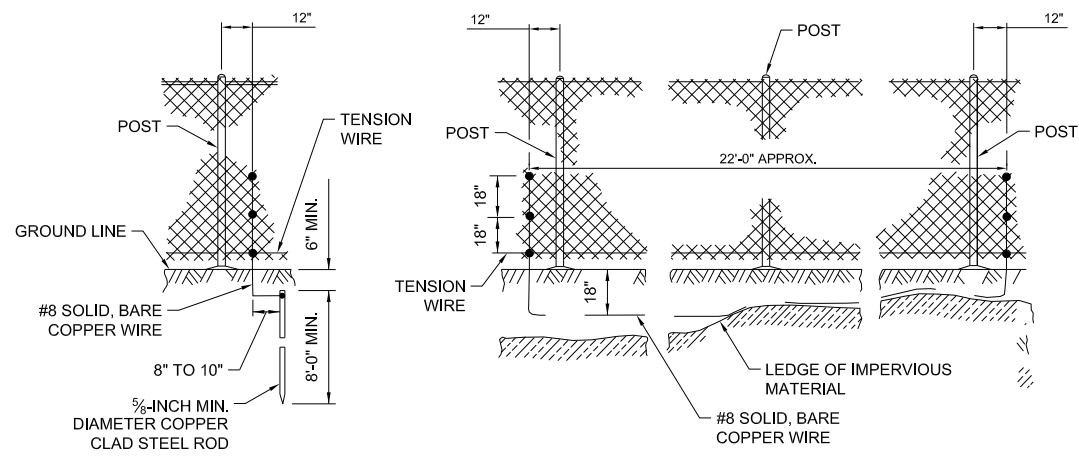
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**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**FENCING DETAILS
 SHEET 3**



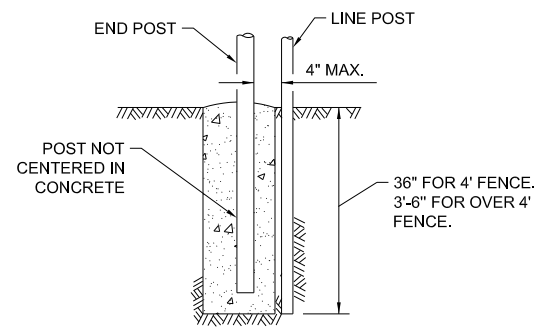
STANDARD GROUND

**COUNTERPOISE GROUND
(ALTERNATE)**

PROTECTIVE ELECTRICAL GROUNDS

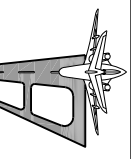
ELECTRICAL GROUNDING NOTES:

- CONTINUOUS FENCE SHALL BE GROUNDED AT INTERVALS NOT EXCEEDING 1000' EXCEPT THERE SHALL BE A GROUND NOT EXCEEDING 100 FT. FROM A GATE IN EACH SECTION OF THE FENCE ADJACENT TO THE GATE.
- FENCE UNDER POWER LINE SHALL BE GROUNDED BY THREE GROUNDS, ONE DIRECTLY UNDER THE CROSSING AND ONE ON EACH SIDE 25 TO 50 FT. AWAY. A SINGLE GROUND SHALL BE LOCATED DIRECTLY UNDER EACH TELEPHONE WIRE OR CABLE CROSSING.
- THE COUNTERPOISE SHALL BE USED ONLY WHERE IT IS IMPOSSIBLE TO DRIVE A GROUND ROD BECAUSE OF AN IMPERVIOUS EARTH STRUCTURES.
- THE GROUND WIRE SHALL BE CONNECTED TO THE FABRIC AND THE GROUND ROD BY A MECHANICAL CLAMP OF CAST BRONZE BODY AND BRONZE OR STAINLESS STEEL BOLTS AND WASHERS. WHEN A TENSION WIRE IS REQUIRED, THE BOTTOM CONNECTION OF THE GROUND WIRE SHALL BE MADE TO THE TENSION WIRE.



DETAIL A

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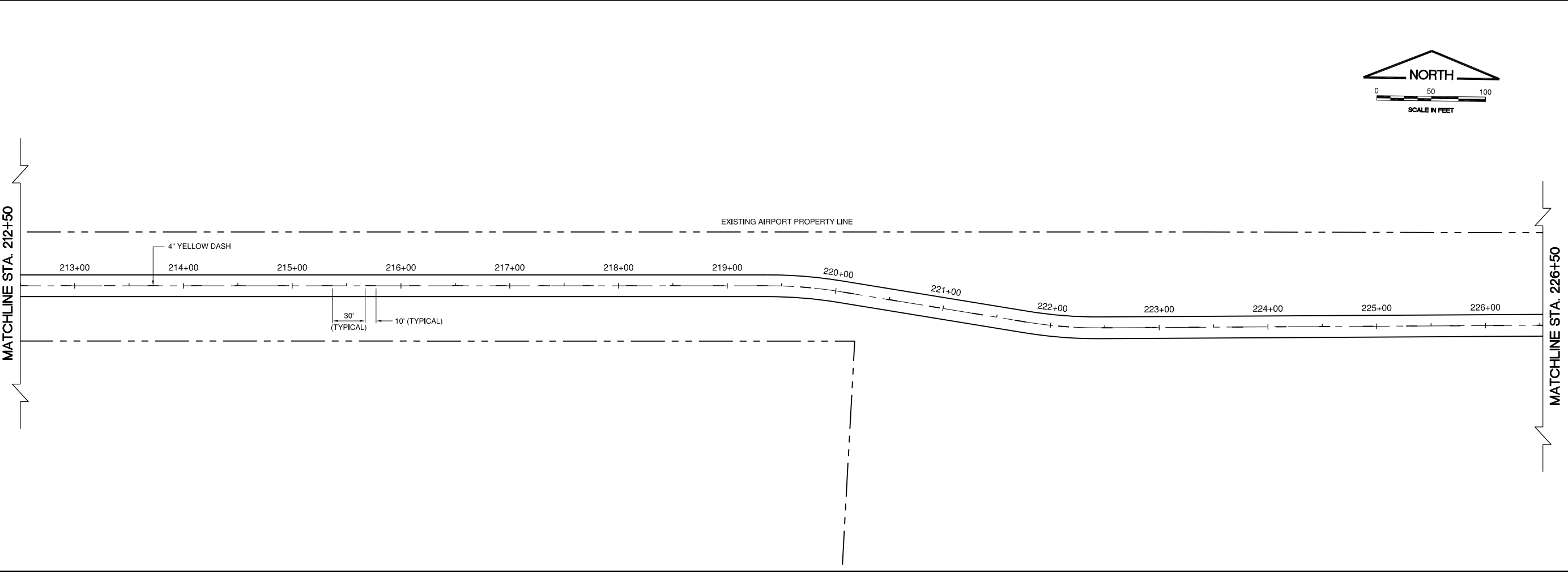
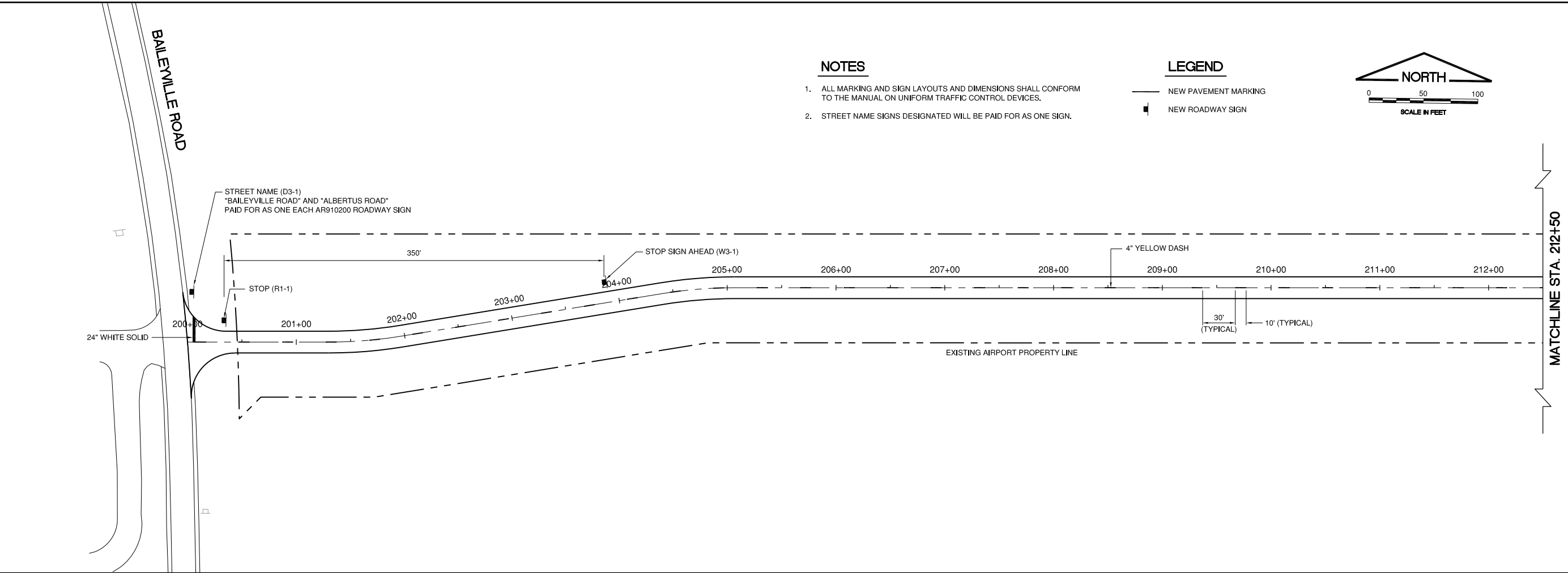
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DRAWN BY:	JRO
CHECKED BY:	DJK
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

REF: DWG: Road Signs and Marking

IMAGE FILES:

UPDATE BY: Jim Chae
LAYOUT: Stieglan

DATE: Friday, November 30, 2018, 2:04:21 PM
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IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK #

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NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

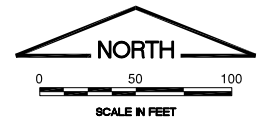
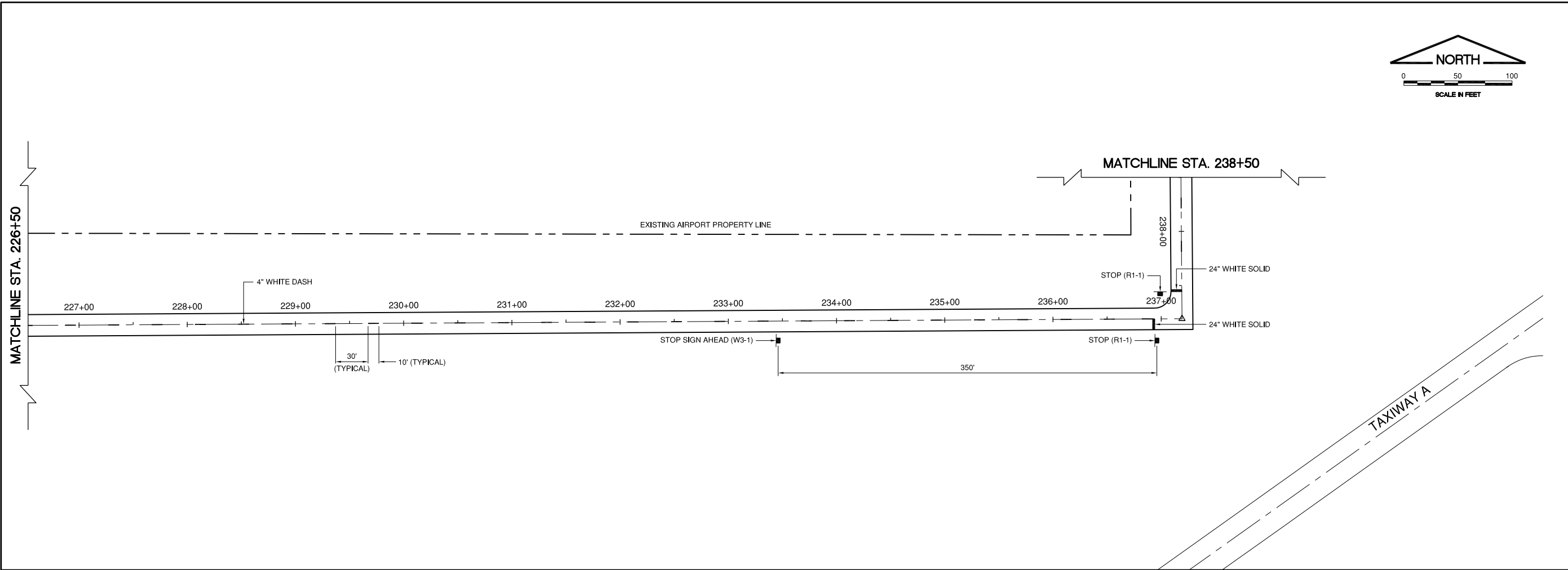
**PAVEMENT MARKING AND SIGNAGE PLAN
SHEET 1**

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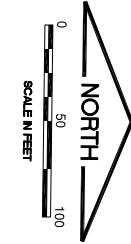
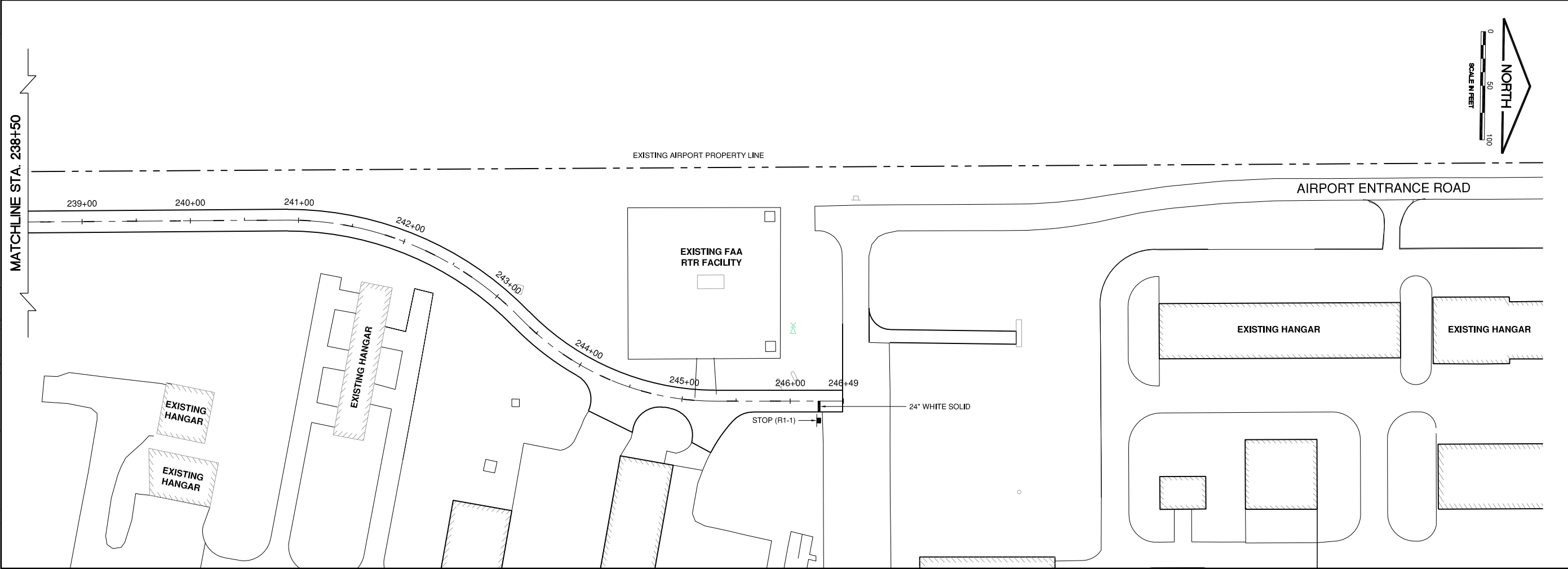
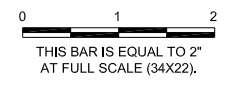
SHEET 31 OF 52 SHEETS



IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

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NUMBER	BY	DATE



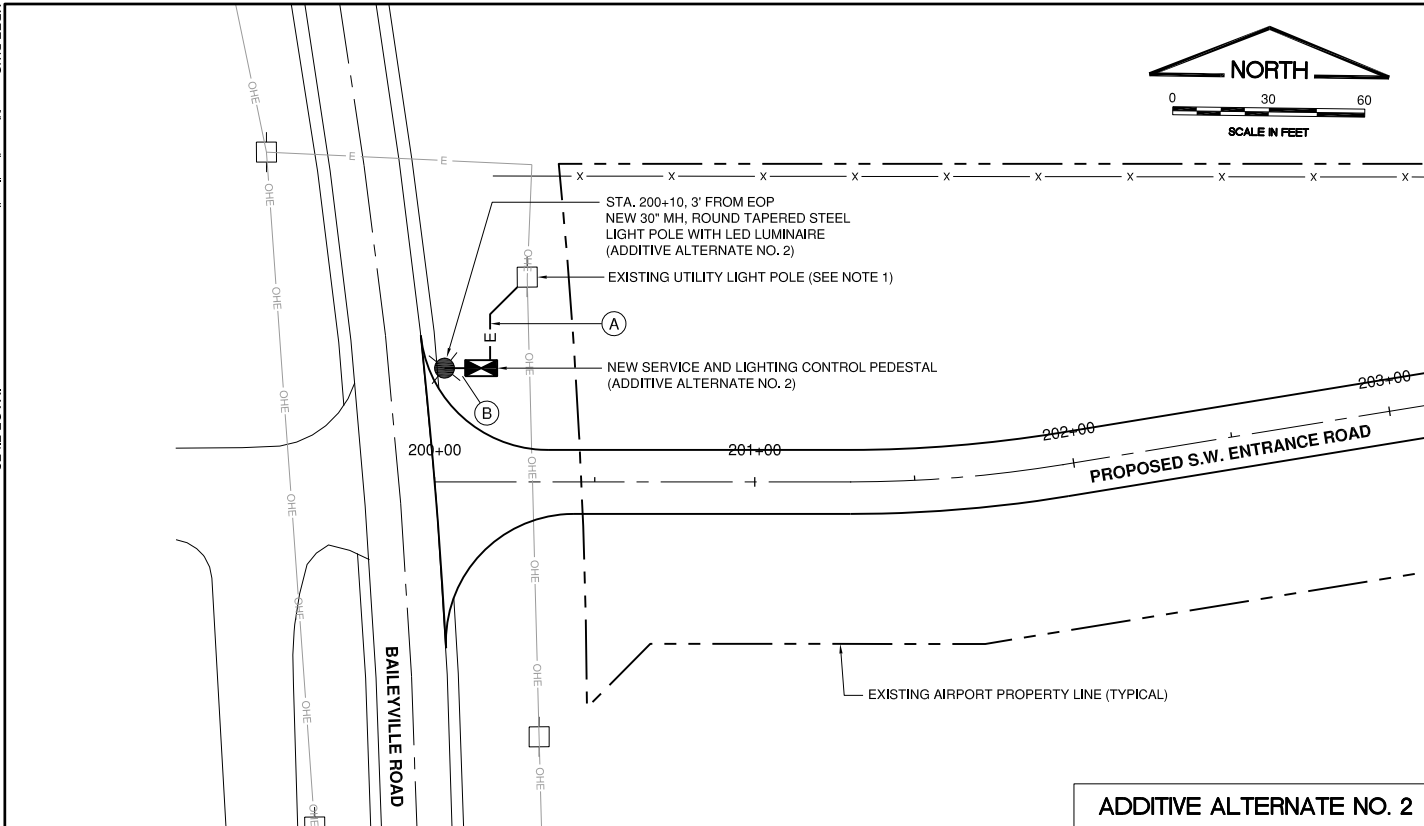
**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

**PAVEMENT MARKING AND SIGNAGE PLAN
SHEET 2**

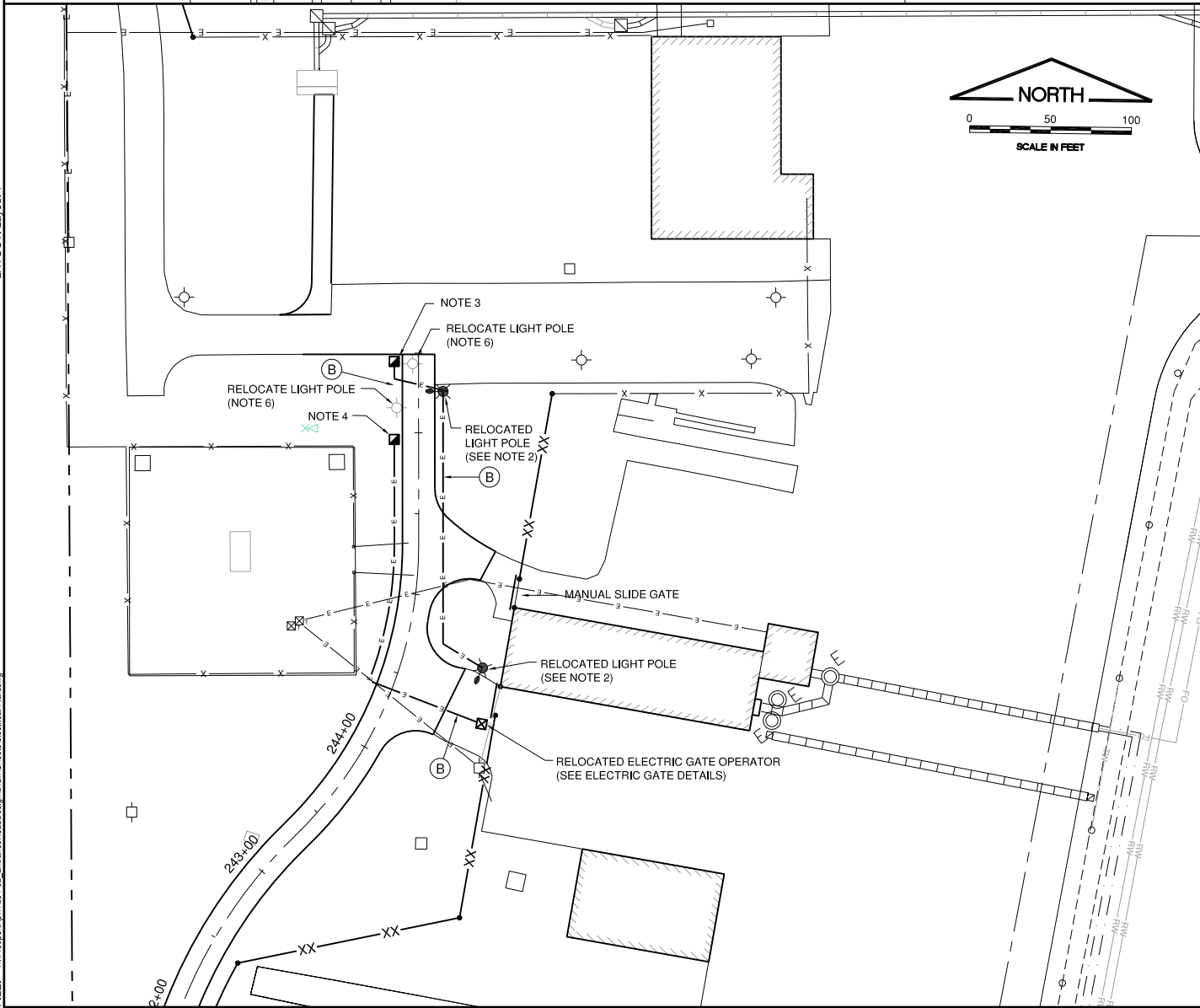
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DATE:	11-16-2018
JOB No:	17294-02
SHEET 32 OF 52 SHEETS	



ADDITIVE ALTERNATE NO. 2



LEGEND

- EXISTING ELECTRIC POLE
- EXISTING LIGHT POLE
- NEW LIGHT POLE
- NEW GATE
- NEW FENCE
- NEW SERVICE AND LIGHTING CONTROL PEDESTAL
- NEW HANDHOLE
- EXISTING ELECTRIC
- EXISTING OVERHEAD ELECTRIC

CABLE/CONDUIT NOMENCLATURE

- (A) NEW 3 #4 XLP-USE, 1 #8 GROUND IN 1-1/2" GRS CONDUIT.
- (B) NEW 2 #8 XLP-USE, 1 #8 GROUND IN 1-1/2" GRS CONDUIT.

NOTES

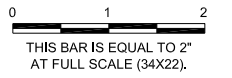
1. CONTRACTOR SHALL INSTALL CONDUIT RISER ON EXISTING UTILITY POLE WITH WEATHERHEAD FOR NEW ELECTRIC SERVICE. NEW ELECTRIC SERVICE SHALL BE 60A, 120/240V, 1-PHASE, 3-WIRE. COORDINATE NEW SERVICE WITH UTILITY COMPANY.
2. REMOVE AND RELOCATE EXISTING LIGHTPOLE ON NEW CONCRETE FOUNDATION. LOCATE AND EXTEND LIGHTING CIRCUIT. NEW LIGHT POLE SHALL BE MINIMUM 3' FROM EDGE OF PAVEMENT.
3. LOCATE AND INTERCEPT EXISTING PARKING LOT LIGHTING CIRCUIT. INSTALL NEW ELECTRIC HANDHOLE, SPLICE NEW CIRCUIT TO EXISTING CIRCUIT. COST OF SPLICING AND CONNECTIONS SHALL BE INCLUDED IN PAY ITEM FOR NEW HANDHOLE.
4. LOCATE AND INTERCEPT EXISTING POWER FEED TO EXISTING GATE OPERATOR. INSTALL NEW HANDHOLE, SPLICE NEW POWER FEED TO RELOCATED GATE OPERATOR. COST OF SPLICING AND CONNECTIONS SHALL BE INCLUDED IN PAY ITEM FOR NEW HANDHOLE.
5. LOCATIONS OF RELOCATED LIGHT POLES, HANDHOLES, PEDESTAL, GATE OPERATOR AND ROUTING OF CONDUITS ARE SHOWN FOR INFORMATION ONLY. EXACT LOCATIONS AND ROUTING SHALL BE FIELD DETERMINED.
6. CUT AND REMOVE EXISTING LIGHTING POLE FOUNDATION, MINIMUM 1'-0" BELOW GRADE.

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

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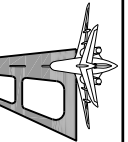
**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

ELECTRICAL PLAN

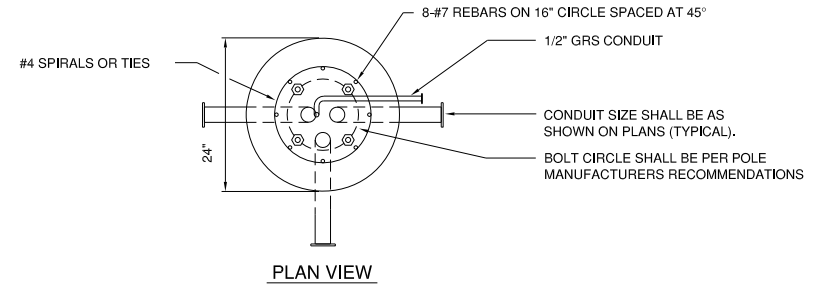
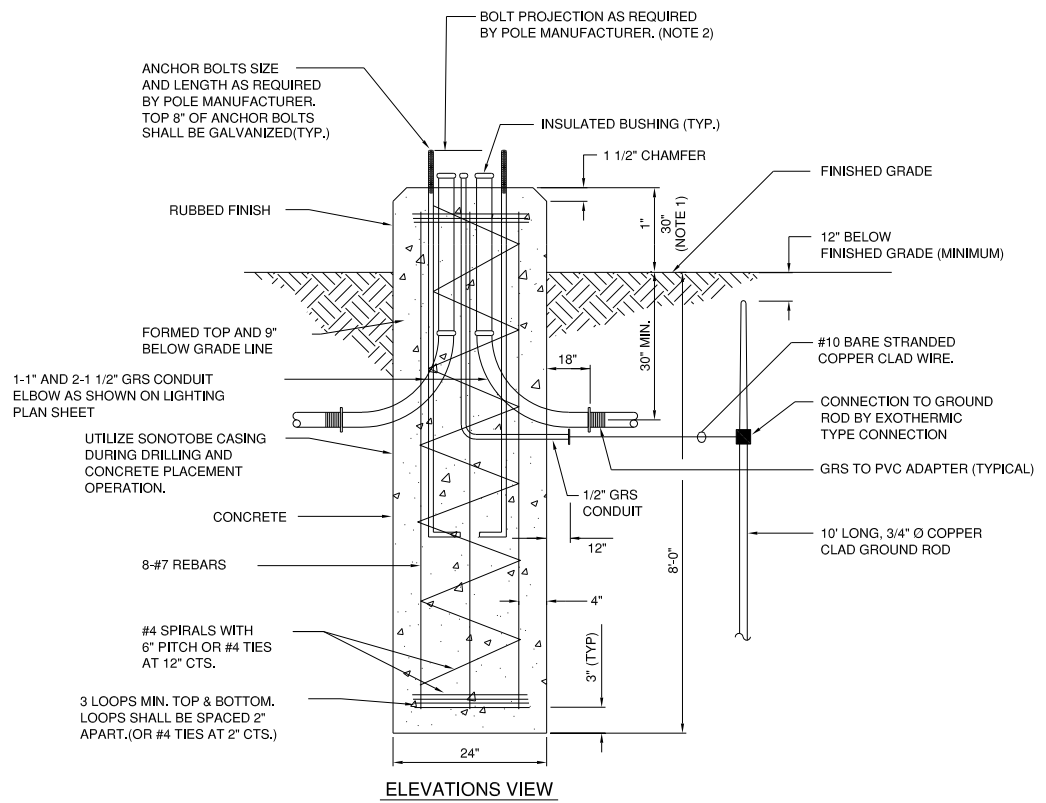
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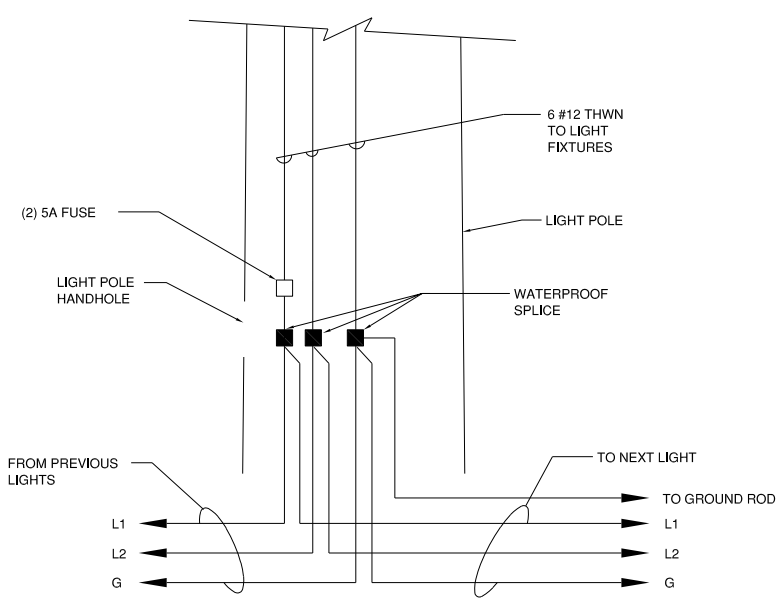


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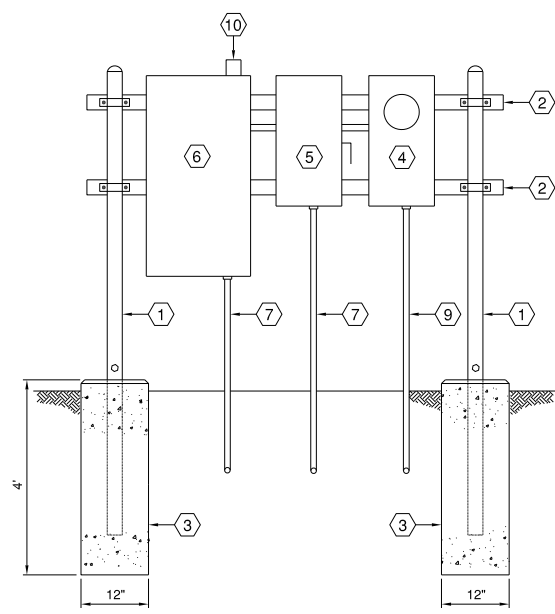


LIGHT POLE FOUNDATION DETAIL (RELOCATED AND NEW LIGHT POLE)
NOT TO SCALE

- NOTES**
- 30" FOR RELOCATED LIGHT POLES AND 1" FOR NEW LIGHT POLE.
 - NEW LIGHT POLE SHALL BE INSTALLED WITH COUPLINGS.

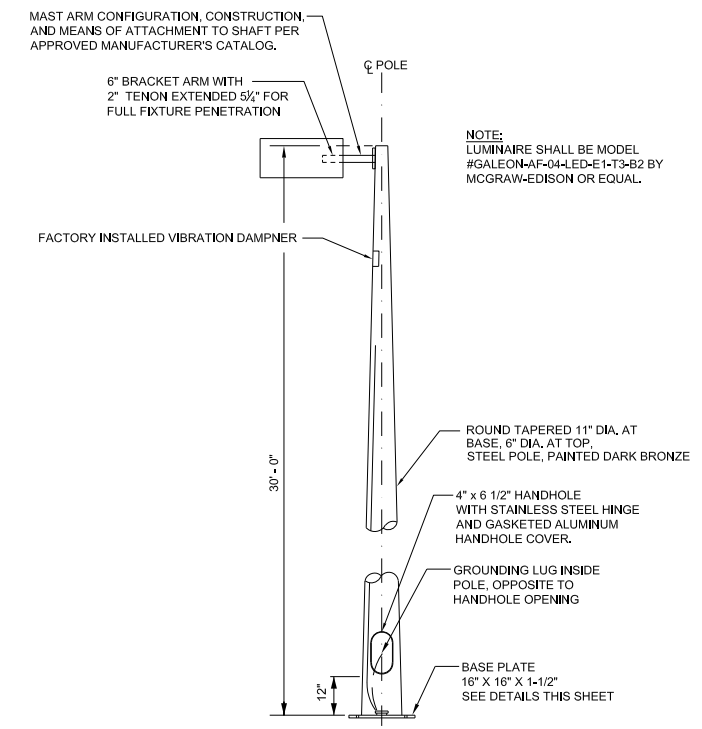


- NOTES:**
- ALL CONNECTIONS TO GROUND RODS SHALL BE MADE WITH ONE-SHOT EXOTHERMIC TYPE CONNECTORS.

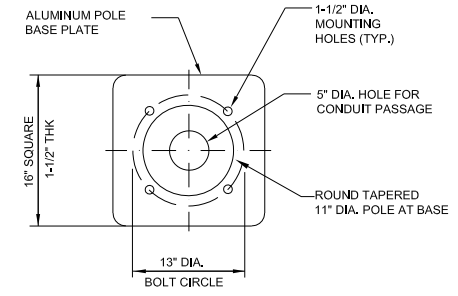


SERVICE AND LIGHTING CONTROL PEDESTAL
NOT TO SCALE

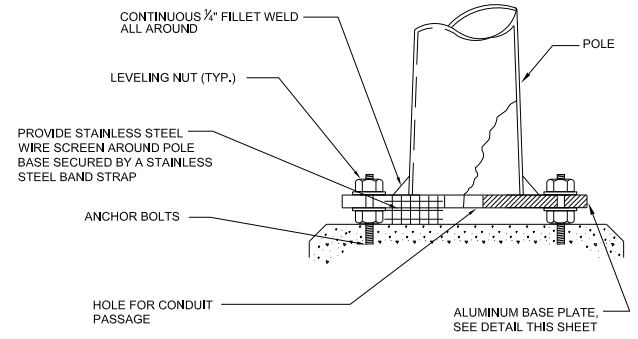
- NOMENCLATURE**
- 3" GRS CONDUIT PEDESTAL SUPPORT WITH END CAPS
 - GALVANIZED UNITSTRUT ATTACHED TO CONDUITS
 - 12" DIA. x 4'-0" DEEP CONCRETE FOUNDATION
 - UTILITY METER BASE. UTILITY TO INSTALL METER
 - 60A, 2-POLE, NEMA 3R, SERVICE ENTRANCE DISCONNECT
 - 60A LIGHTING CONTACTOR WITH HAND-OFF-AUTO SELECTOR SWITCH IN NEMA 3R ENCLOSURE, LOCKABLE
 - 1/C #8 GROUND WIRE IN 1" PVC SCH.40 CONDUIT, CADWELDED TO 3/4" DIA. x 10'-0" LONG GROUND ROD
 - 2 #10 XLP-USE, 1 #10 GROUND IN 1" GRS CONDUIT TO LIGHT POLE
 - 3 #6 XLP-USE, 1 #8 GROUND IN 1-1/2" GRS CONDUIT TO UTILITY POLE
 - PHOTOCELL MOUNTED ON ENCLOSURE



LIGHT POLE DETAIL
NOT TO SCALE



LIGHT POLE BASE PLAN
NOT TO SCALE

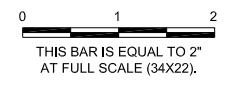


LIGHT POLE BASE DETAIL
NOT TO SCALE

IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

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**FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

ELECTRICAL DETAILS

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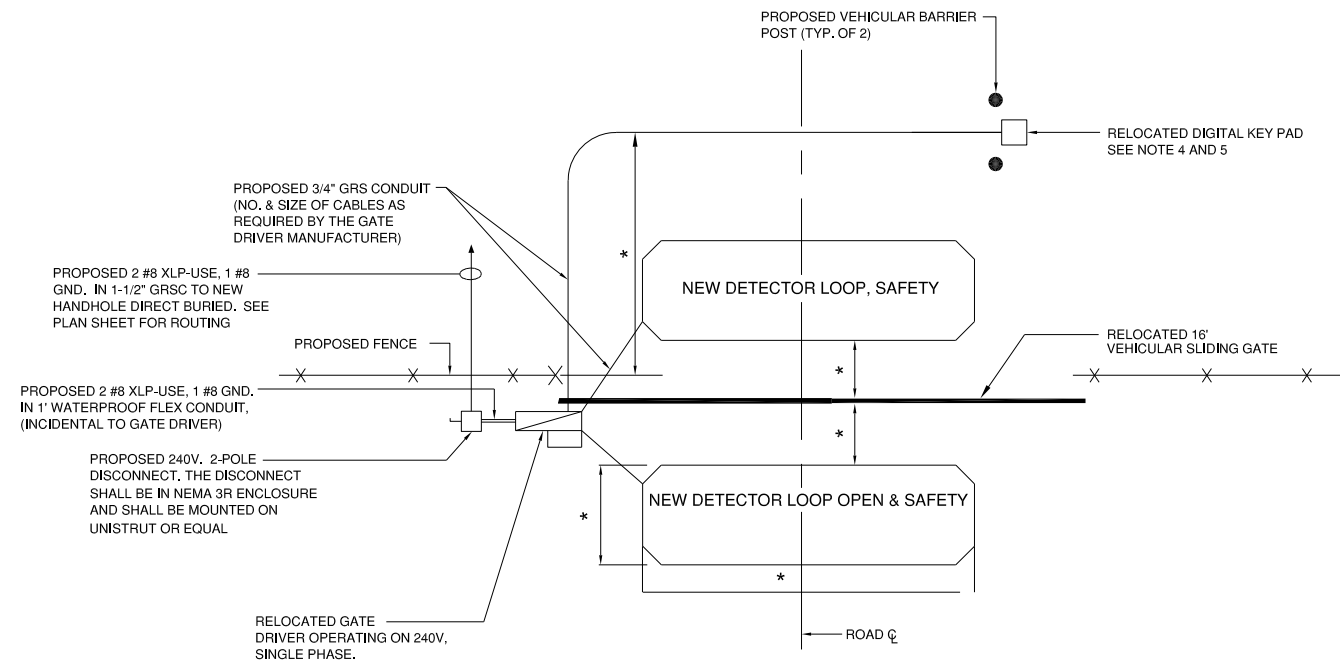
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APPROVED BY:	DJK
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JOB No:	17294-02

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UPDATE BY: Jim Ohse
LAYOUT: Layout

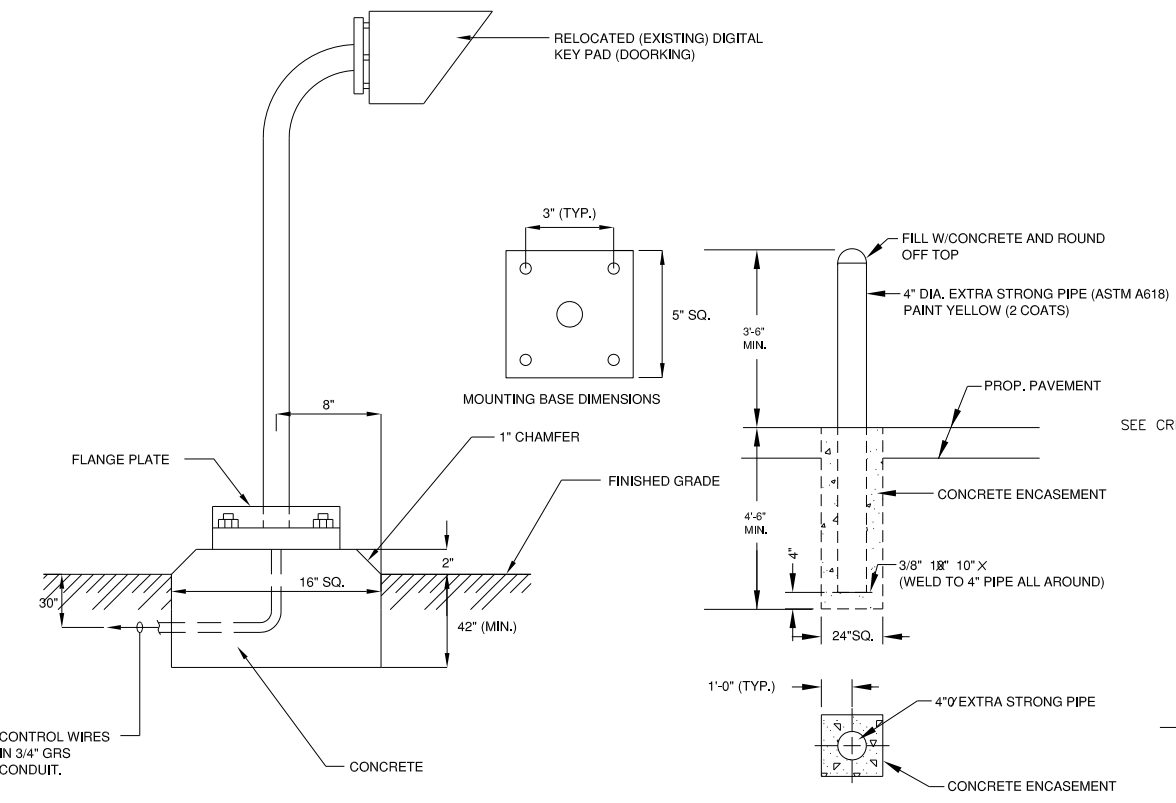
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PROPOSED KEY PAD OPERATED GATE AND DETECTOR LOOP LAYOUT

NOT TO SCALE
* PER MANUFACTURERS RECOMENDATION CONTRACTOR SHALL COORDINATE THIS WORK WITH ENGINEER.



DIGITAL KEY PAD MOUNTING DETAIL

NOT TO SCALE

VEHICULAR BARRIER DETAIL

NOT TO SCALE

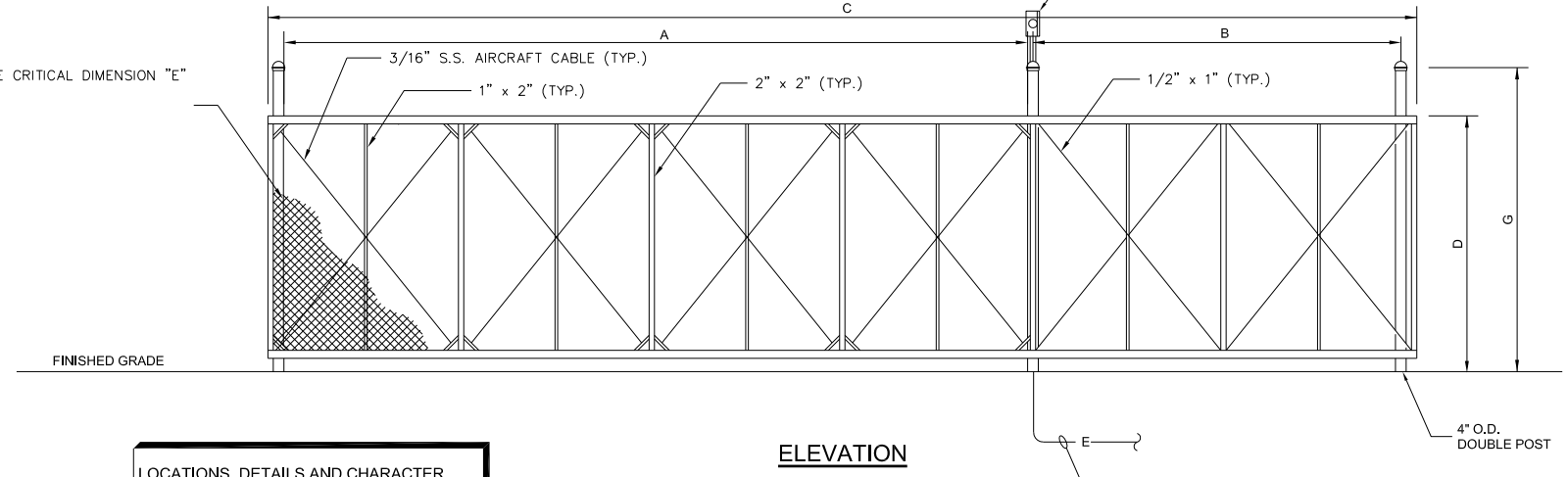
- NOTES:**
- THE VEHICULAR BARRIERS WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO ELECTRIC GATE.
 - LOCATION OF UNDERGROUND ELECTRICAL ITEMS SHALL BE COORDINATED WITH VEHICULAR BARRIERS TO AVOID ANY CONFLICTS.

- NOTES:**
- THE LOCATION OF THE PROPOSED KEY PAD OPERATED GATE DRIVER, DISCONNECT, KEY PAD, AND DETECTOR LOOPS ARE FOR INFORMATION ONLY AND SHALL BE FIELD ADJUSTED PER THE MANUFACTURER RECOMMENDATION.
 - THE MINIMUM BURIAL DEPTH FOR GRS CONDUIT IS 30" BELOW FINISHED GRADE.
 - NO DIRECT BURIED CABLE WILL BE ALLOWED IN THE INSTALLATION OF THE PROPOSED KEY PAD OPERATED GATE DRIVER.
 - EXACT LOCATION OF KEYPAD TO BE AS DIRECTED BY THE AIRPORT.
 - EXISTING KEYPAD AND STAND TO BE RELOCATED, MANUFACTURED BY DOORKING.

GATE TYPE	GATE
A	DISTANCE BETWEEN GATE POSTS (INSIDE FACE TO INSIDE FACE) 16'-0"
B	DISTANCE BETWEEN HANGER POSTS (CENTER TO CENTER) 8'-0" *
C	OVERALL GATE LENGTH 24'-0" *
D	OVERALL GATE HEIGHT 6'-0"
E	HEIGHT OF FABRIC IN GATE 5'-0"
F	DOUBLE POST SET WIDTH (INSIDE FACE TO INSIDE FACE) 13'-1/2"
G	HEIGHT OF 4" POSTS ABOVE GRADE 6'-6"

* OR AS RECOMMENDED BY MFG.

- NOTES:**
- CANTILEVERED GATE SHALL BE SUFFICIENTLY RIGID TO WITHSTAND FLEXING OR BENDING DURING WINDY CONDITIONS. CONTRACTOR SHALL PROVIDE STIFFENERS, STRUCTURAL SHAPES IN EXCESS OF THE MINIMUM SPECIFIED DIMENSIONS OR ADDITIONAL ROLLERS AND POSTS SUFFICIENT TO PREVENT DISPLACEMENT OF THE GATE BY WIND OR BY UNAUTHORIZED PERSONNEL.
 - CONTRACTOR SHALL PROVIDE AND INSTALL GATE AS A COMPLETE WORKING UNIT. THE GATE WORK SHALL INCLUDE, BUT NOT BE LIMITED TO THE GATE, RELOCATED (EXISTING) GATE OPERATOR, FOUNDATION, AND POWER CABLES CONDUIT, RELOCATED (EXISTING) CARD READER, TRENCHING, CIRCUIT BREAKERS, VEHICULAR BARRIERS AND ALL CONNECTIONS, LABOR AND MATERIALS NECESSARY TO COMPLETE OPERATION.
 - LOCATION OF THE GATE OPERATOR SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
 - THE FABRIC TYPE AND FINISH OF THE GATE SHALL MATCH WITH THE PROPOSED FENCE OR AS DIRECTED BY THE ENGINEER.
 - ALL PROPOSED SLIDING VEHICULAR GATES SHALL HAVE ENCLOSED TRUCK ASSEMBLIES, PAGE FORTRESS OR EQUAL.

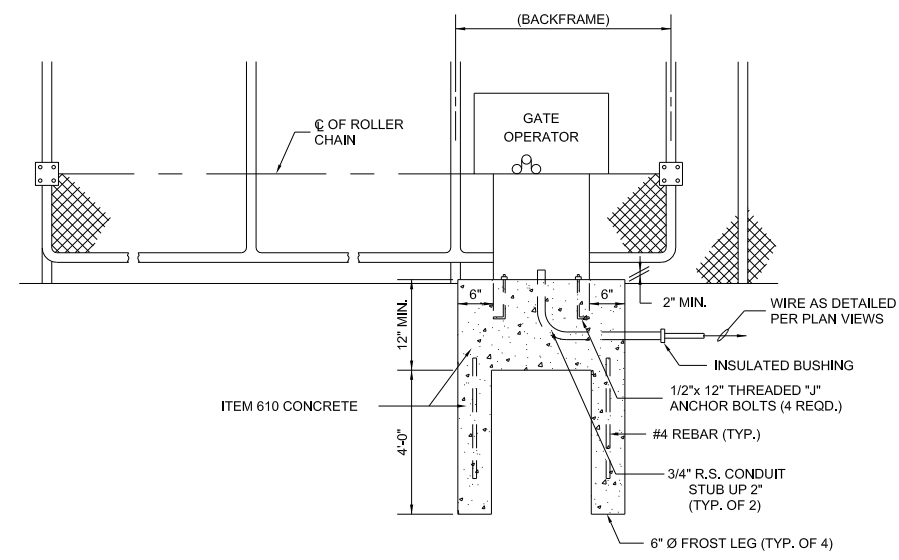


ELEVATION

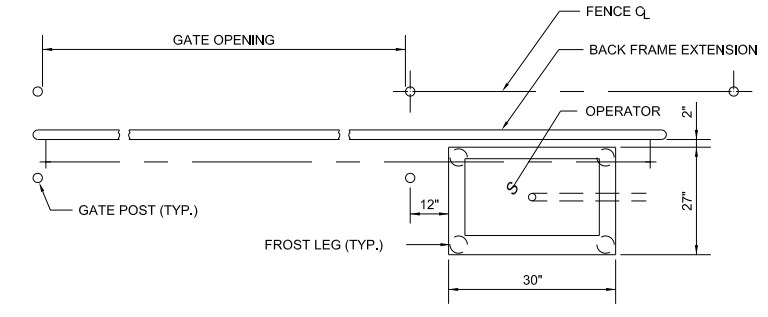
CANTILEVER SLIDE GATE

NOT TO SCALE

LOCATIONS, DETAILS AND CHARACTER OF EQUIPMENT SHOWN ON THIS SHEET ARE GENERIC. EQUIPMENT LOCATIONS SHALL BE AS RECOMMENDED BY THE EQUIPMENT MANUFACTURER.



SECTION



PLAN

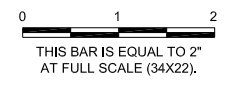
GATE OPERATOR DETAIL

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IL CONTRACT: FR042
IL LETTING ITEM: 14A
IL PROJECT: FEP-4626
S.B.G. PROJECT: N/A

SURVEY BOOK # ###

REVISIONS		
NUMBER	BY	DATE



FREEPORT-ALBERTUS AIRPORT
FREEPORT, ILLINOIS
SOUTHWEST TERMINAL AREA ENTRANCE ROAD

ELECTRIC GATE DETAILS

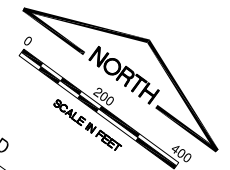
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APPROVED BY:	DJK
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JOB No:	17294-02

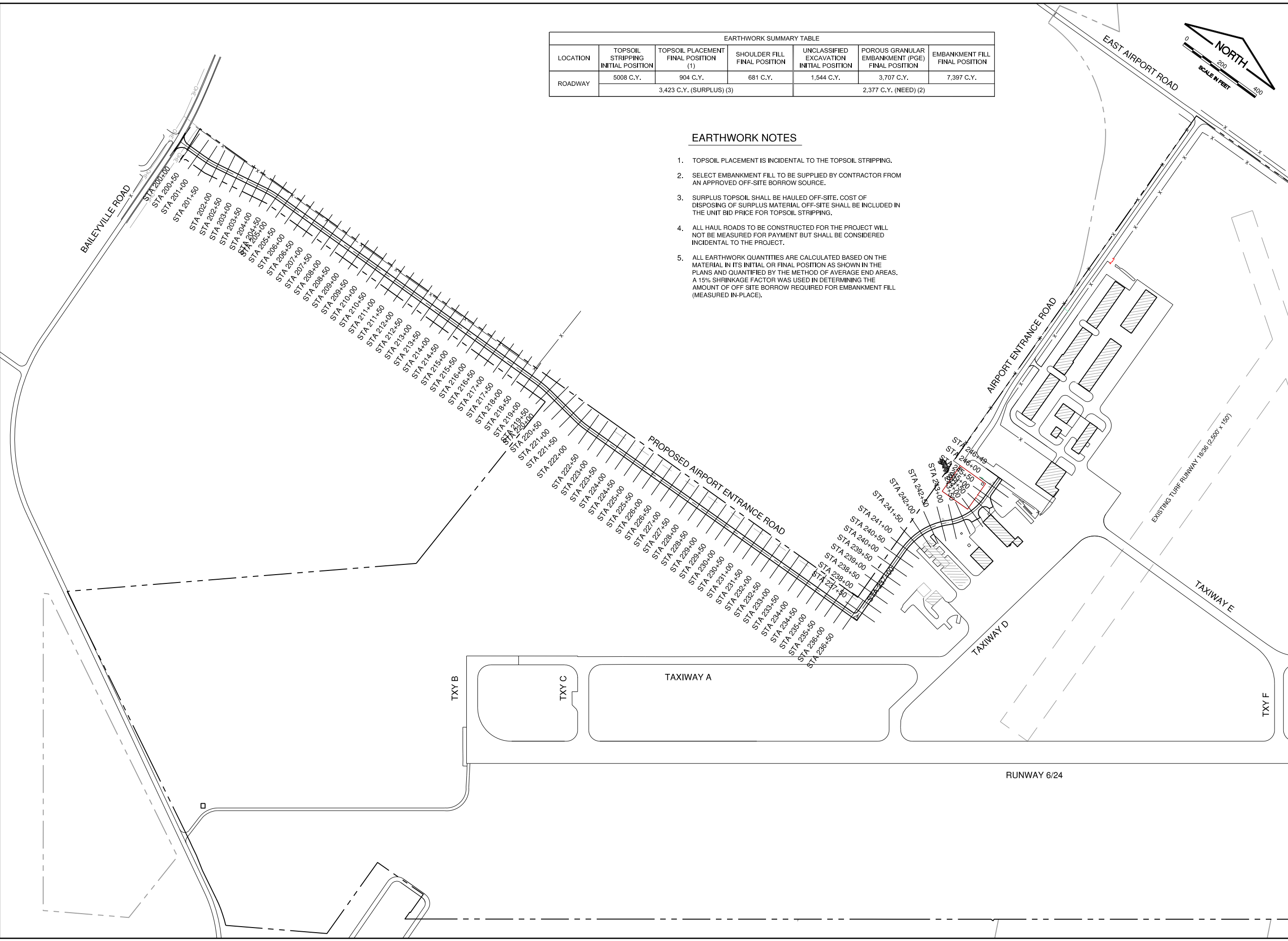
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 LAYOUT: Layout
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EARTHWORK SUMMARY TABLE						
LOCATION	TOPSOIL STRIPPING INITIAL POSITION	TOPSOIL PLACEMENT FINAL POSITION (1)	SHOULDER FILL FINAL POSITION	UNCLASSIFIED EXCAVATION INITIAL POSITION	POROUS GRANULAR EMBANKMENT (PGE) FINAL POSITION	EMBANKMENT FILL FINAL POSITION
ROADWAY	5008 C.Y.	904 C.Y.	681 C.Y.	1,544 C.Y.	3,707 C.Y.	7,397 C.Y.
	3,423 C.Y. (SURPLUS) (3)			2,377 C.Y. (NEED) (2)		



EARTHWORK NOTES

- TOPSOIL PLACEMENT IS INCIDENTAL TO THE TOPSOIL STRIPPING.
- SELECT EMBANKMENT FILL TO BE SUPPLIED BY CONTRACTOR FROM AN APPROVED OFF-SITE BORROW SOURCE.
- SURPLUS TOPSOIL SHALL BE HAULED OFF-SITE. COST OF DISPOSING OF SURPLUS MATERIAL OFF-SITE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR TOPSOIL STRIPPING.
- ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS. A 15% SHRINKAGE FACTOR WAS USED IN DETERMINING THE AMOUNT OF OFF SITE BORROW REQUIRED FOR EMBANKMENT FILL (MEASURED IN-PLACE).



IL CONTRACT: FR042
 IL LETTING ITEM: 14A
 IL PROJECT: FEP-4626
 S.B.G. PROJECT: N/A

SURVEY BOOK #

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

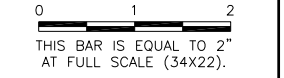
INDEX TO CROSS SECTIONS

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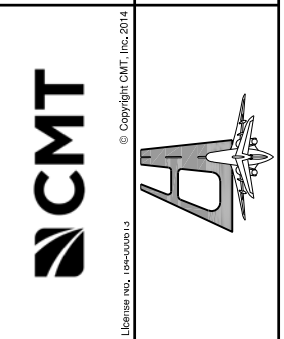
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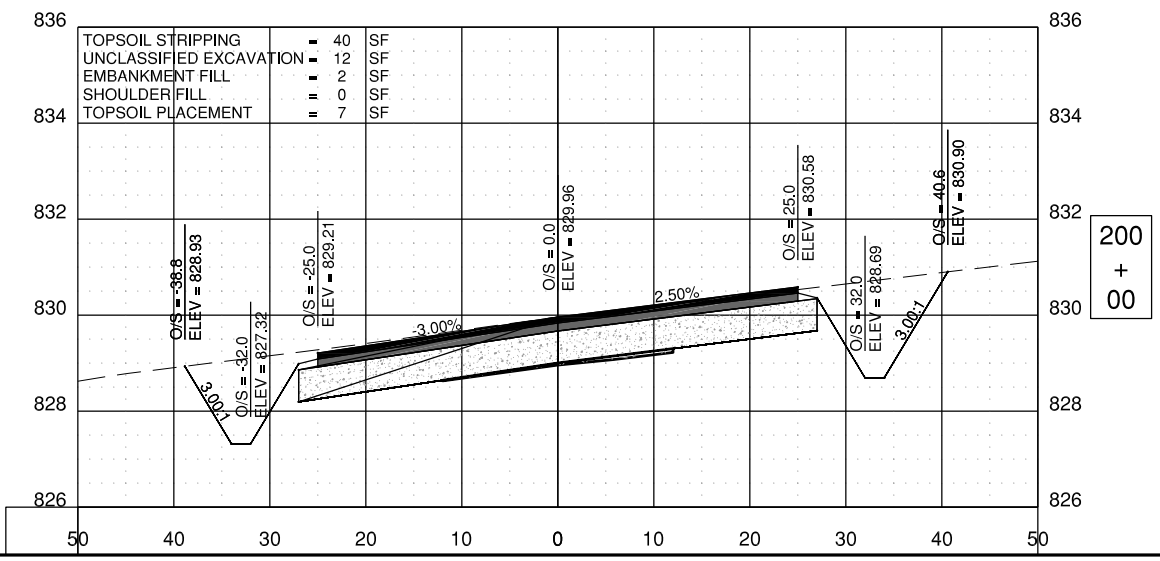
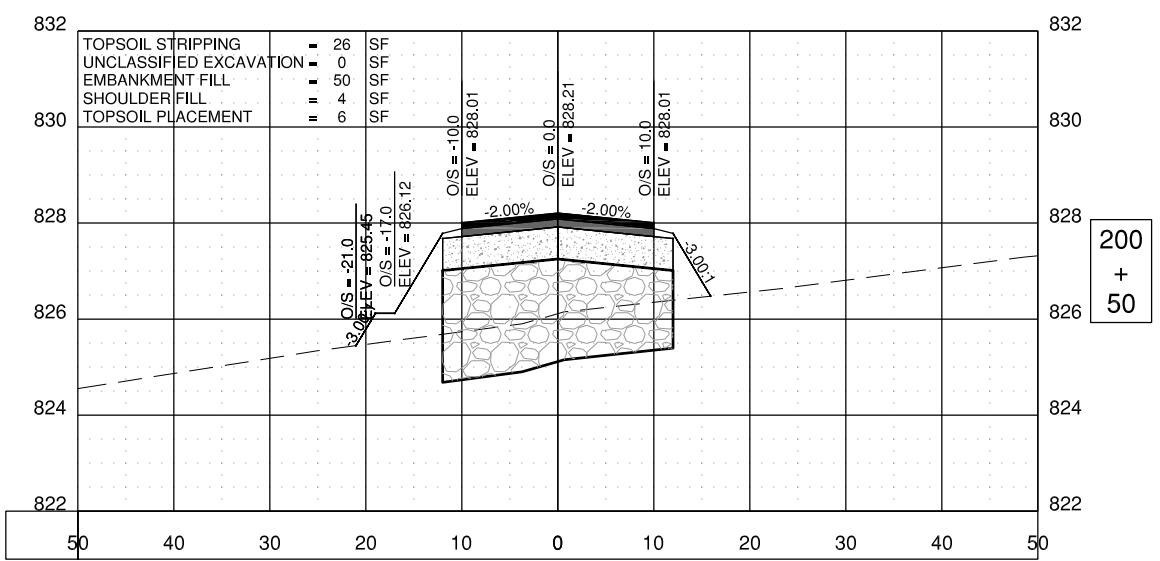
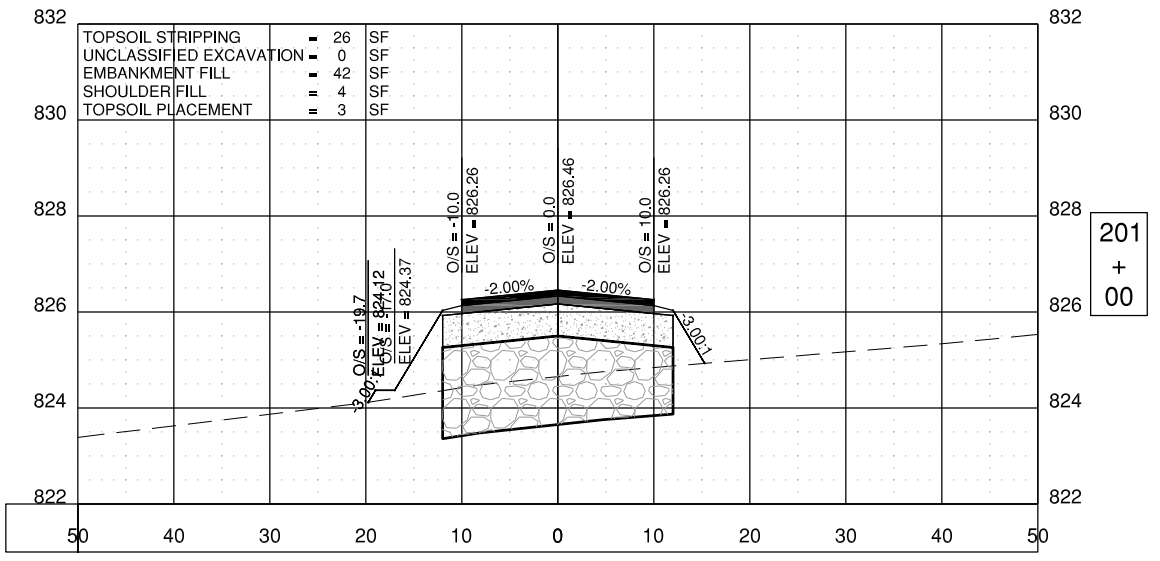
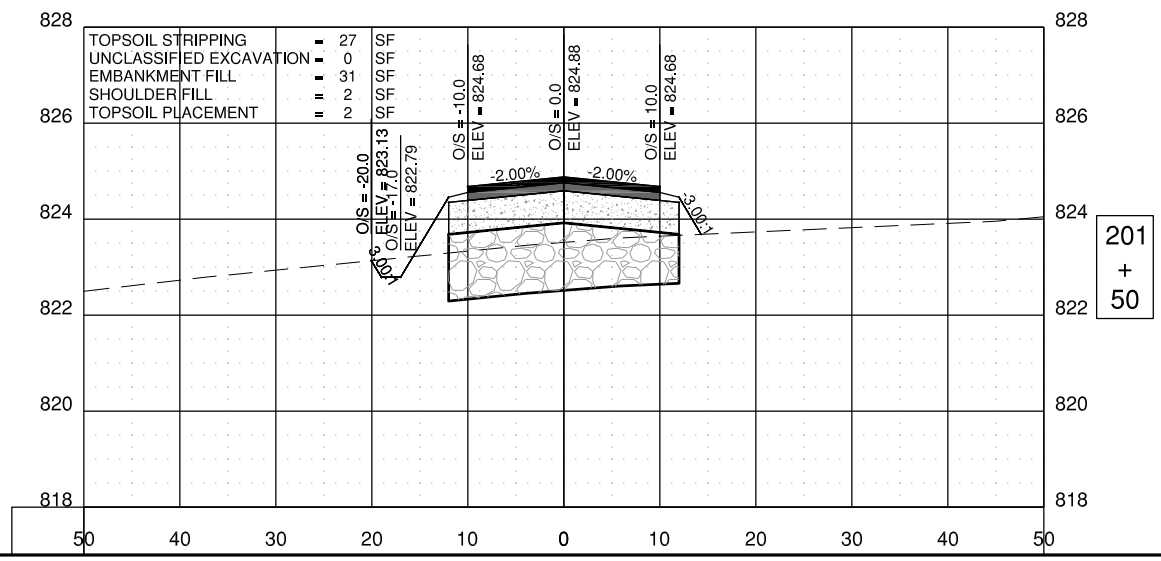
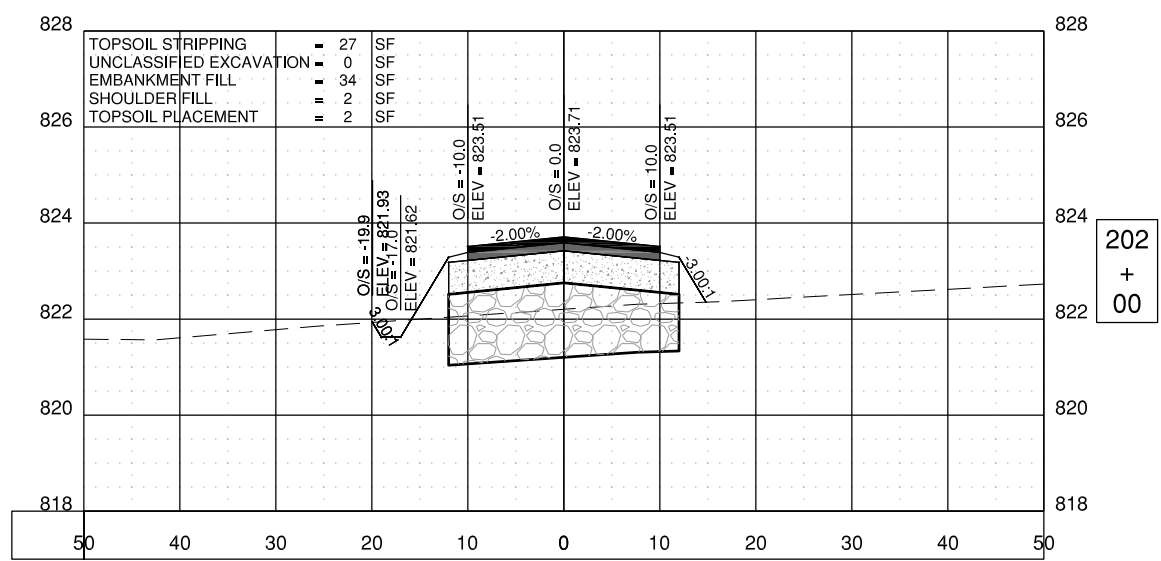
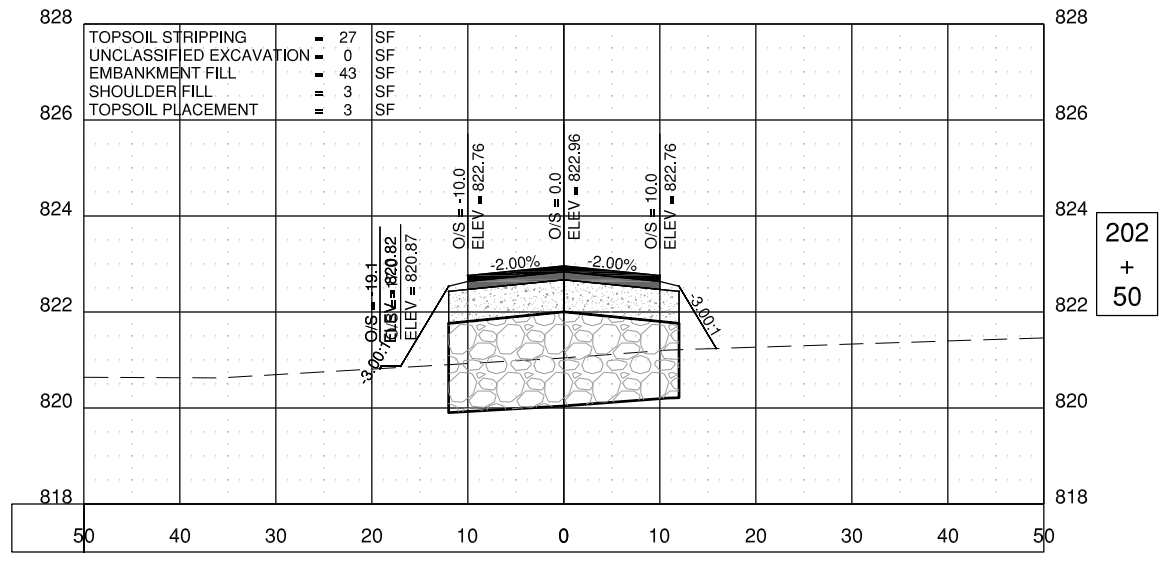


FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD

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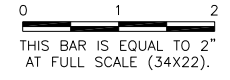
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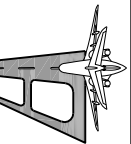


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

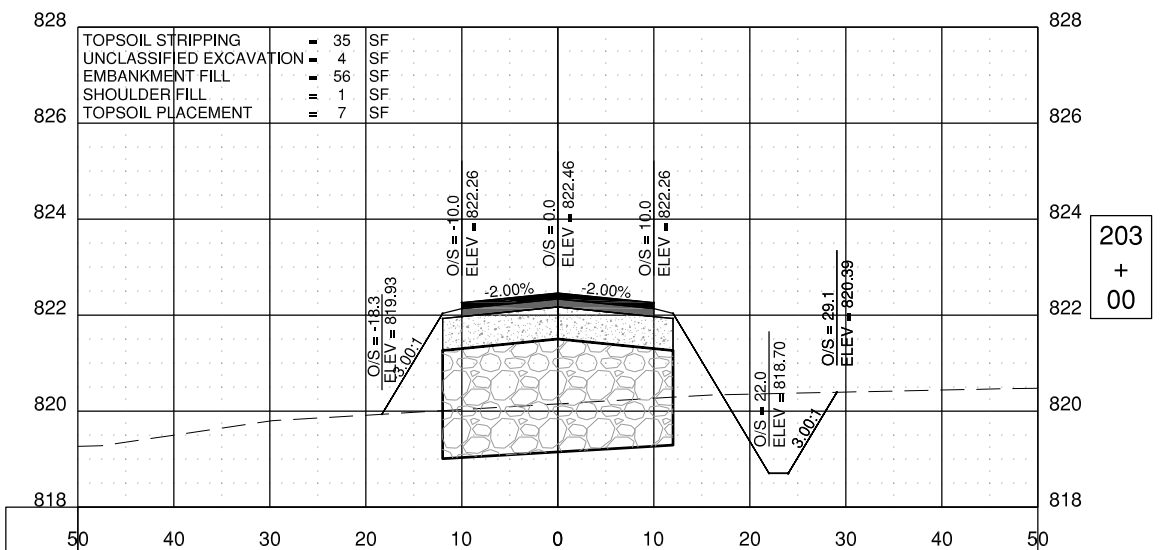
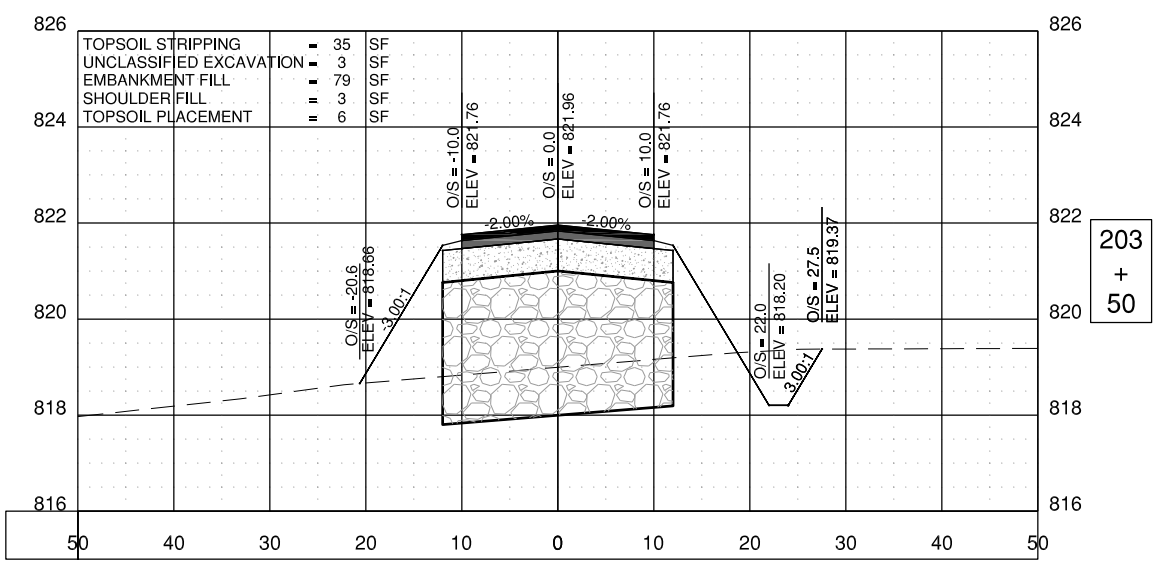
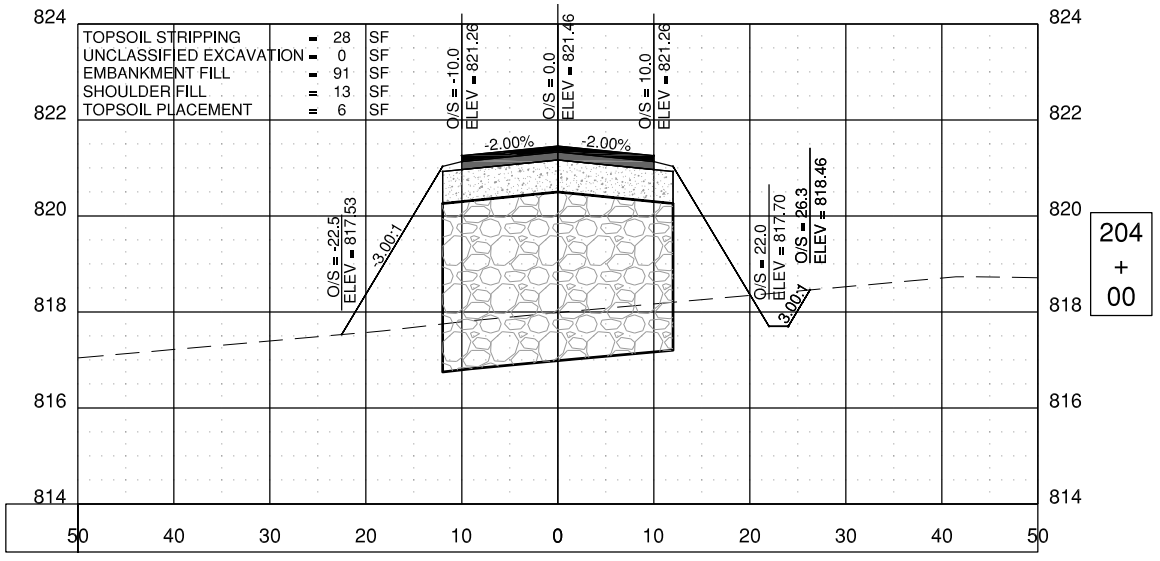
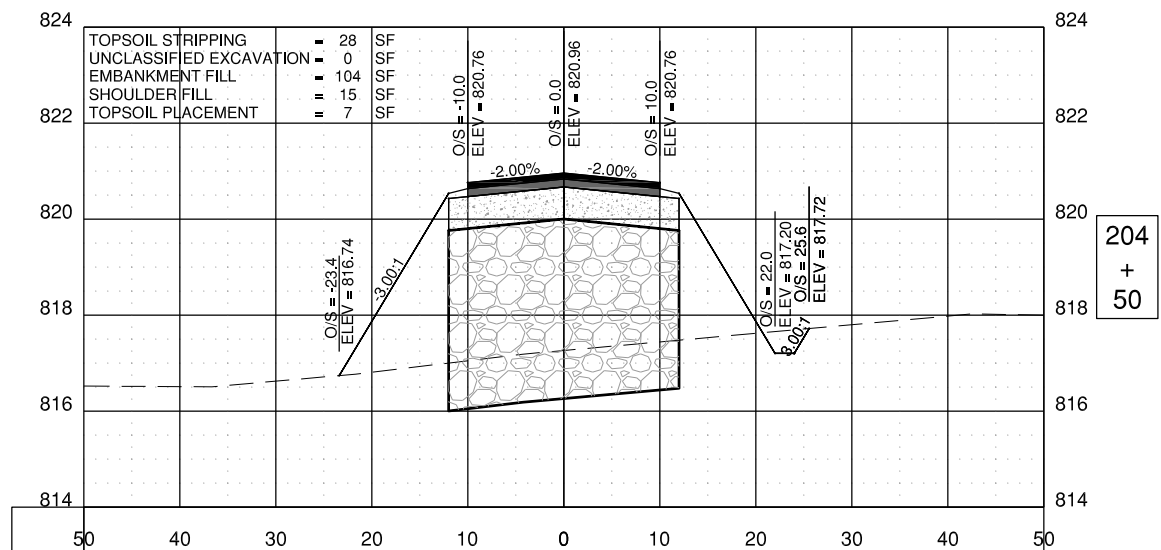
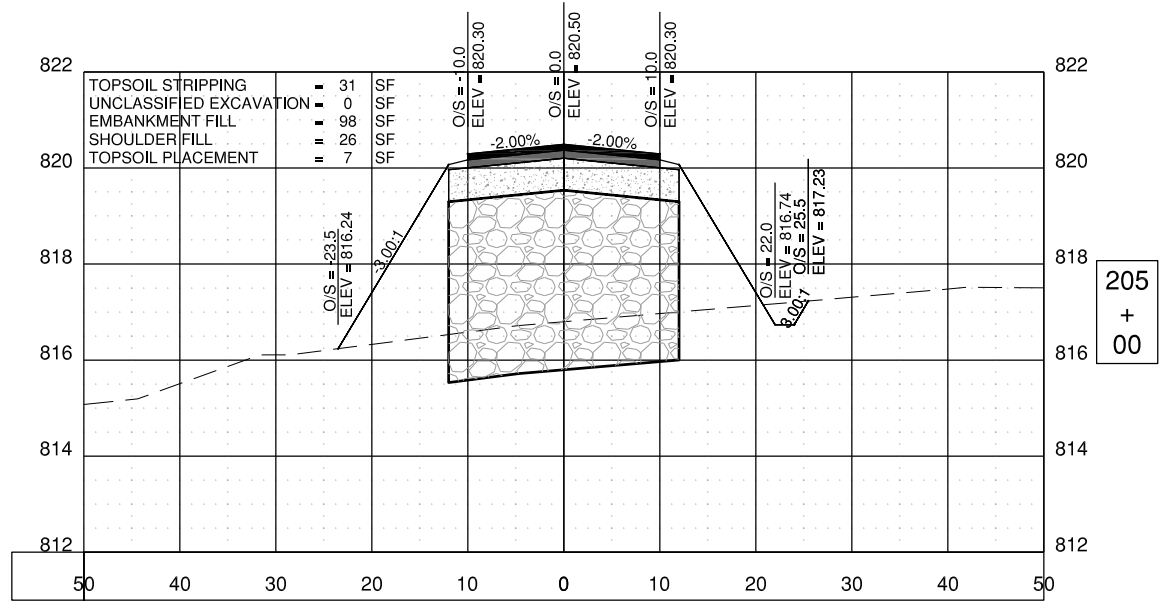
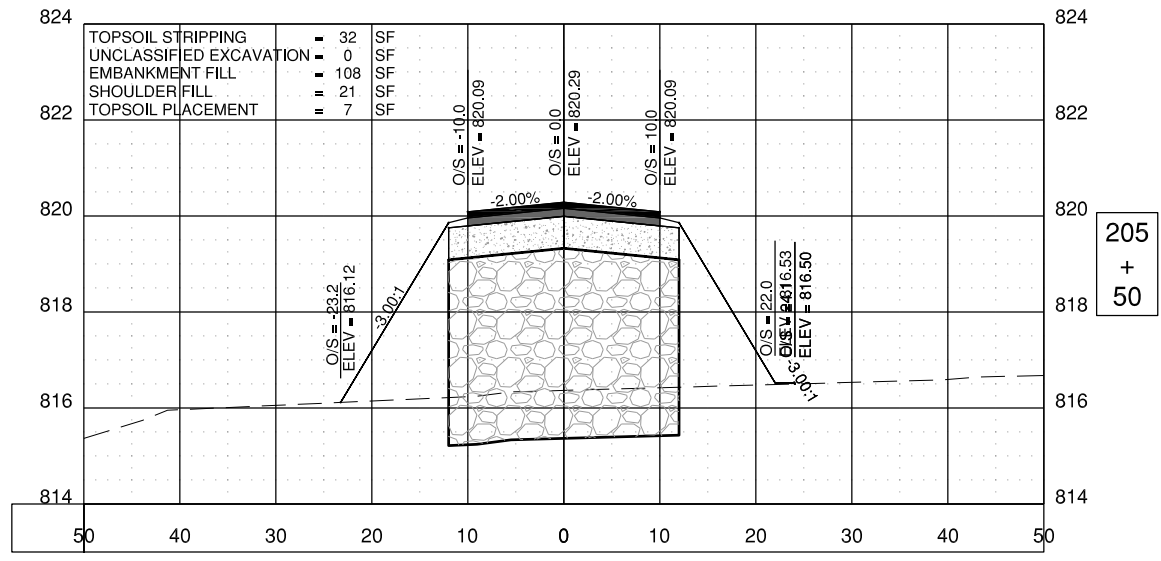
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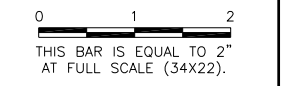


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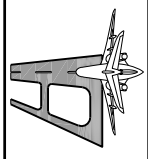


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 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

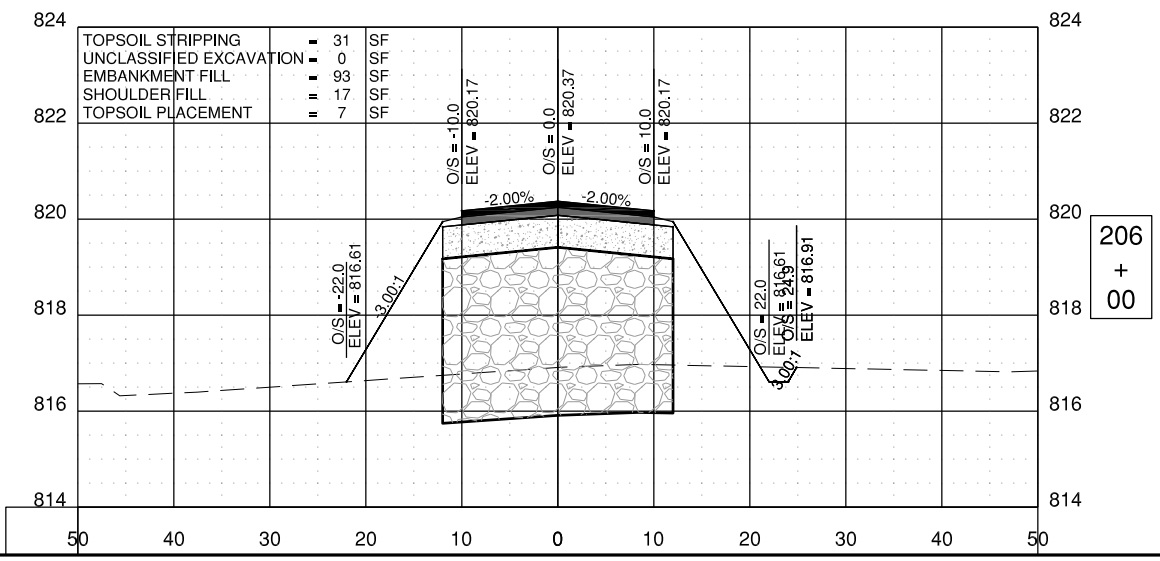
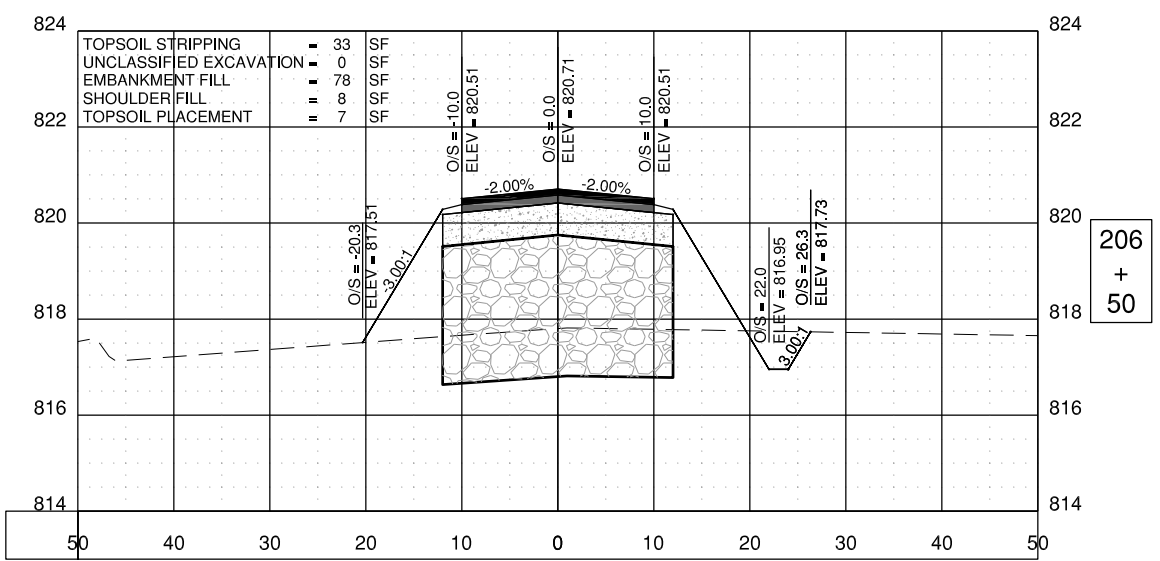
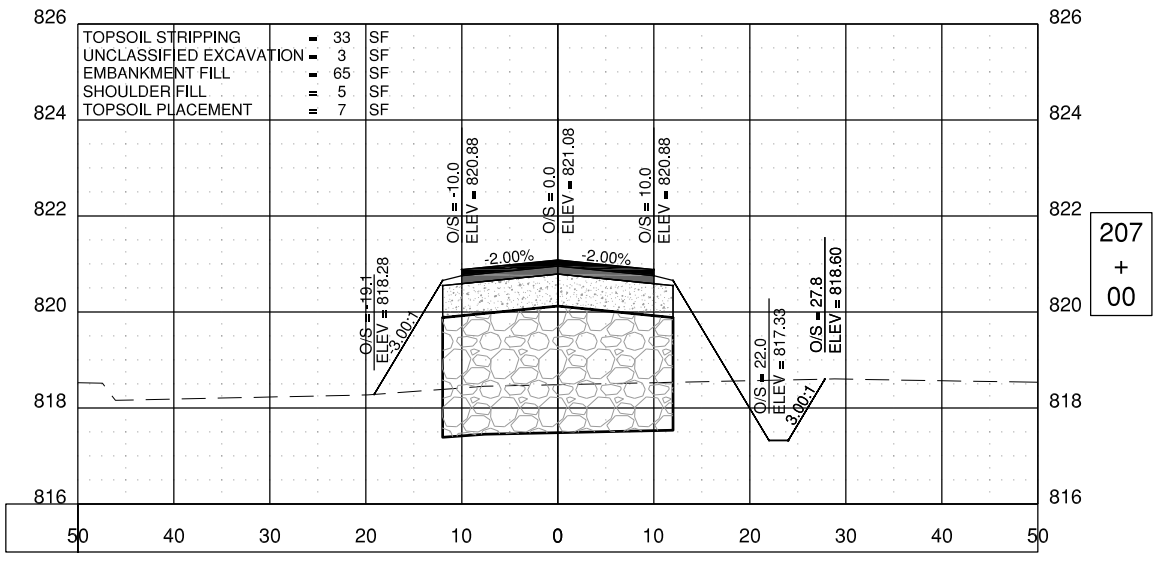
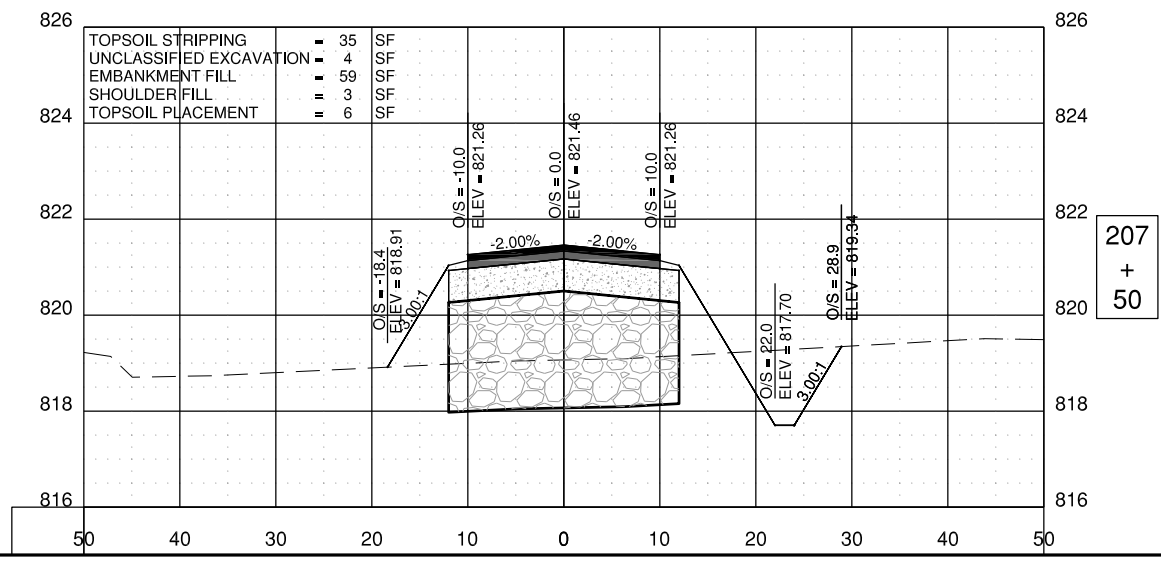
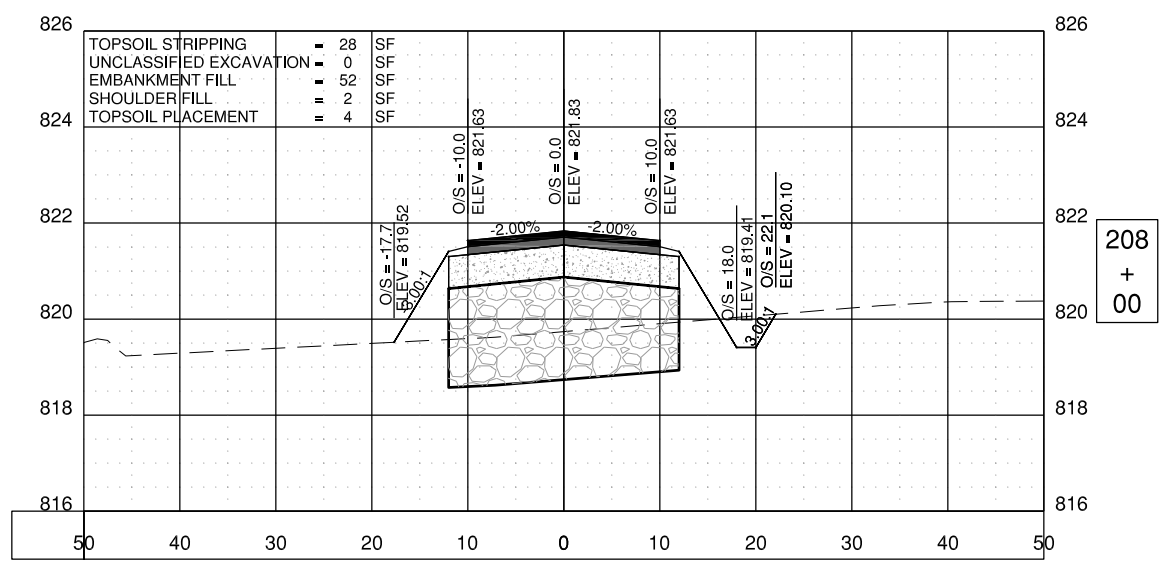
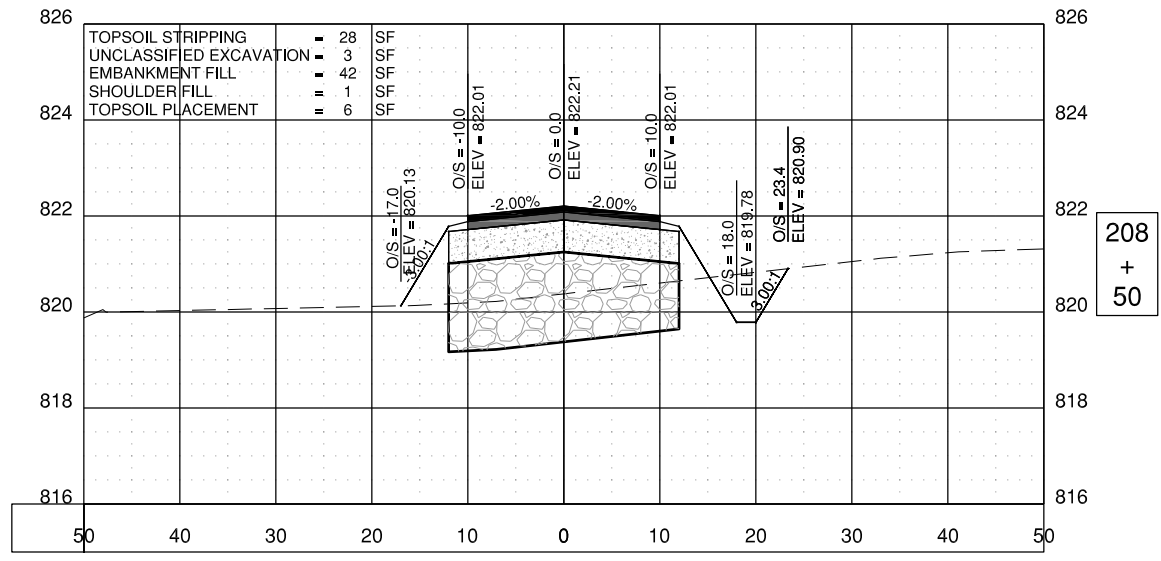
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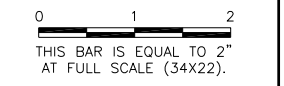


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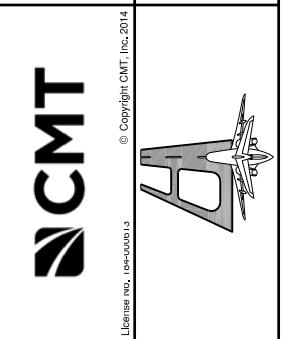
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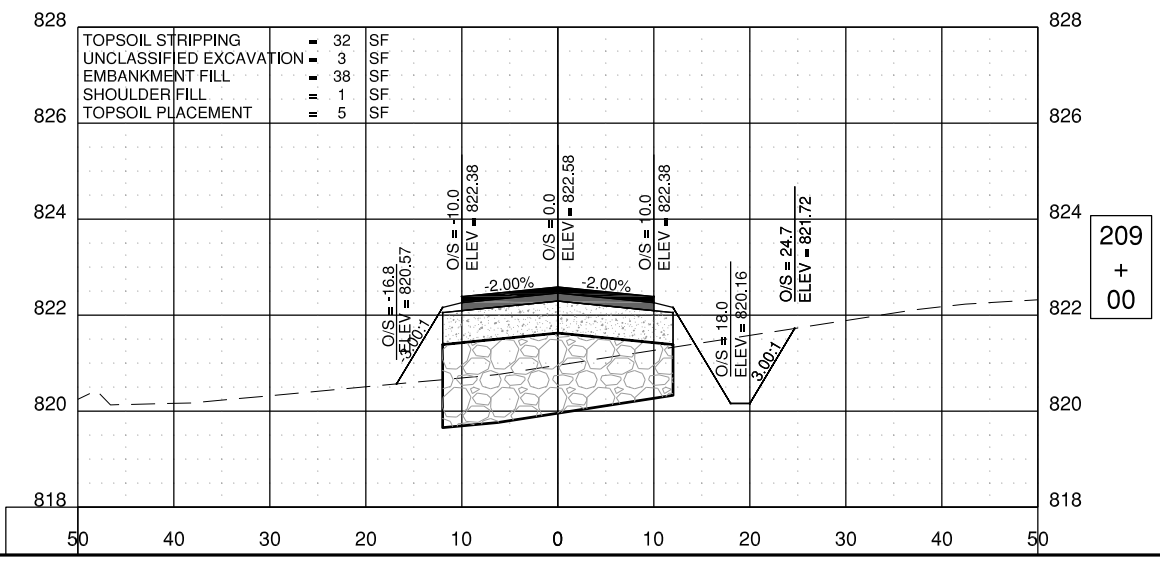
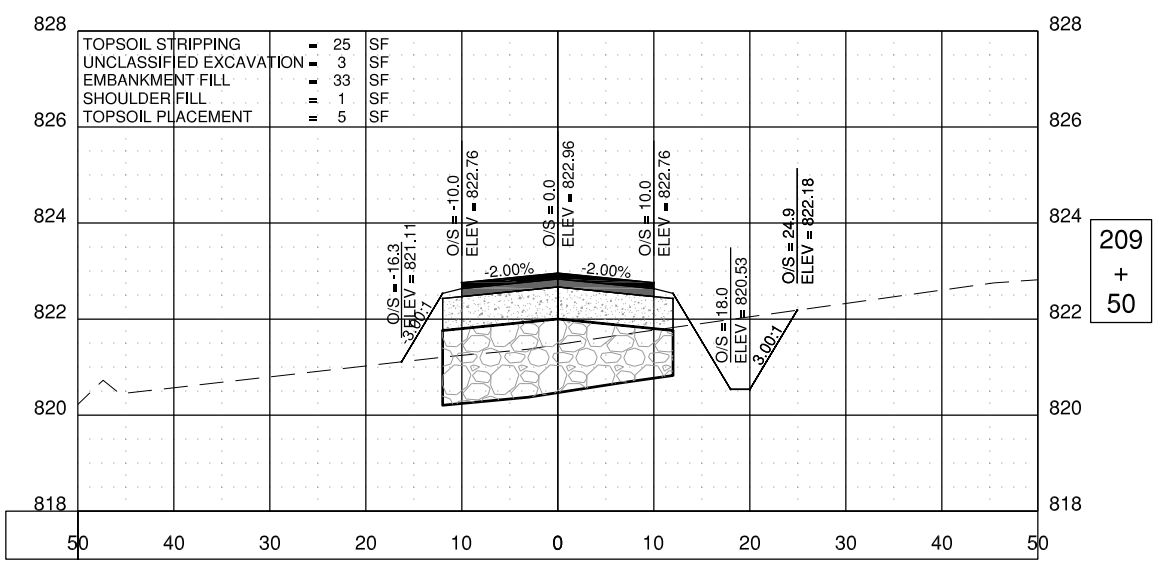
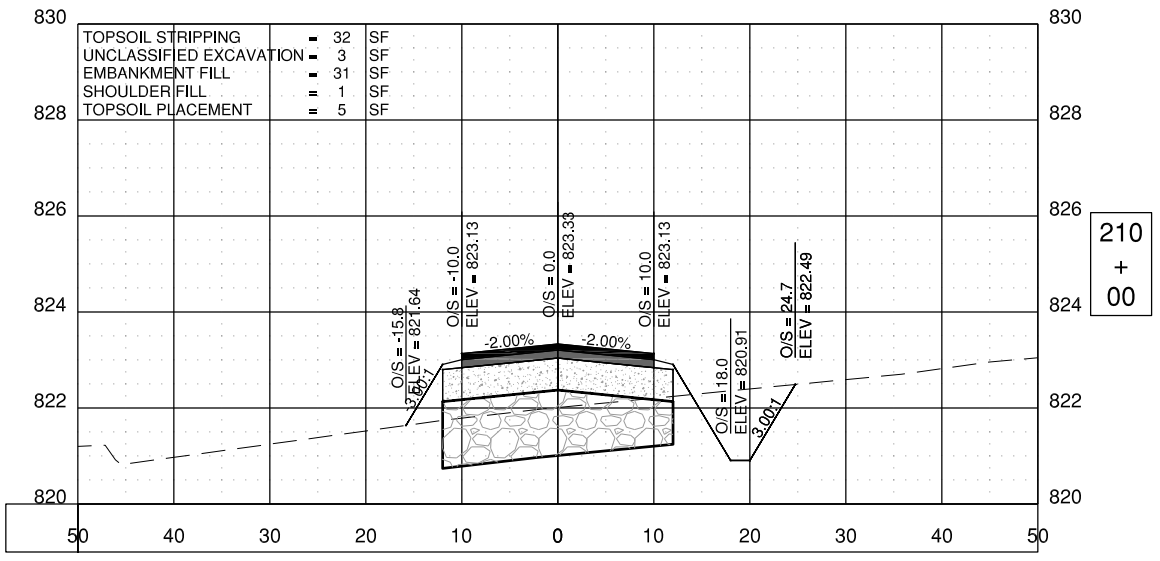
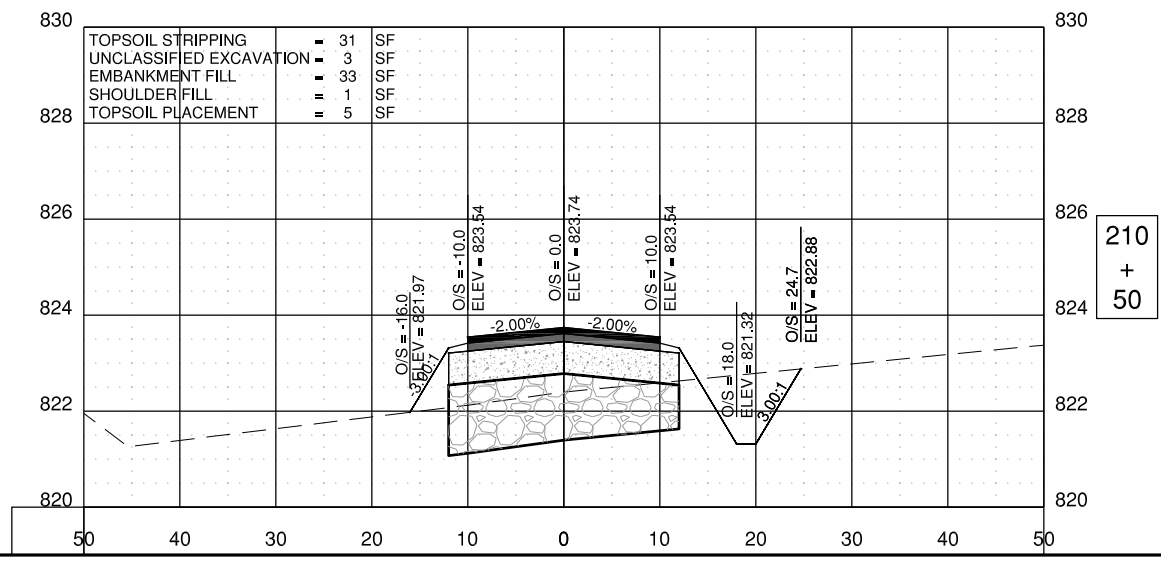
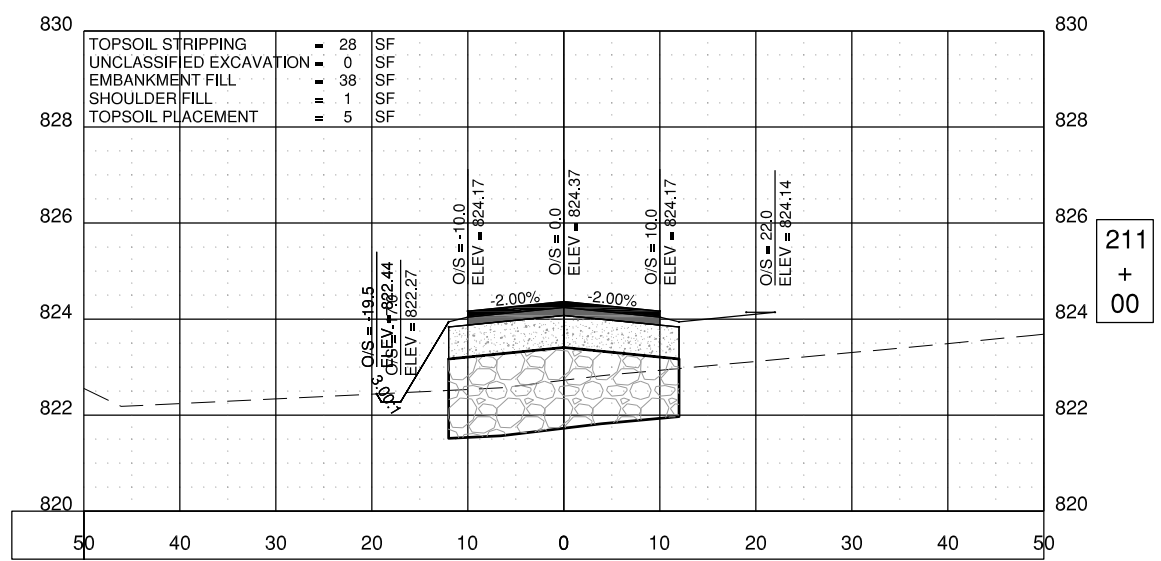
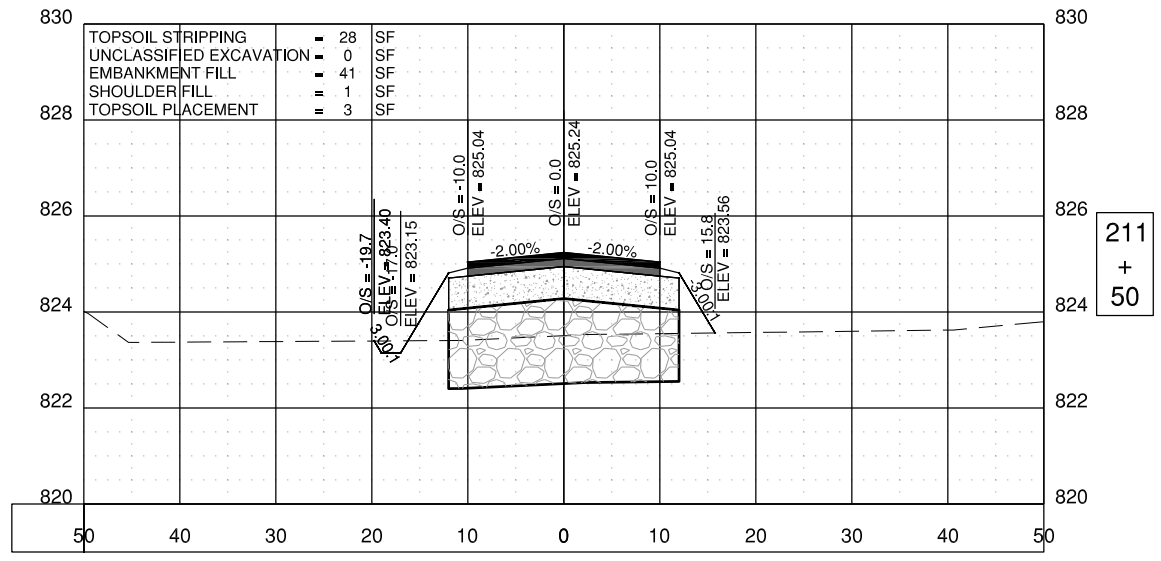


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

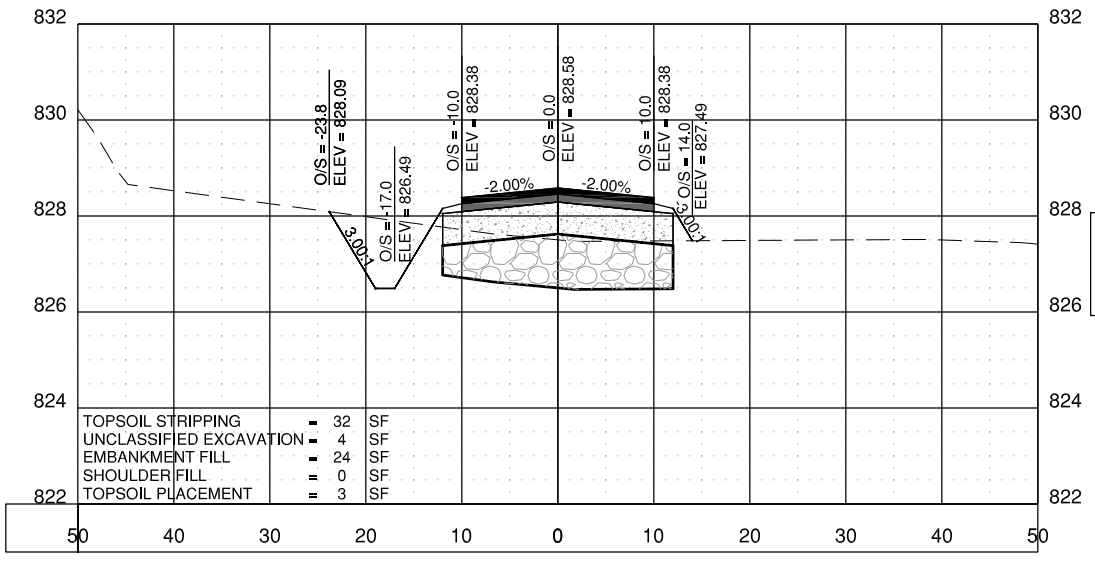
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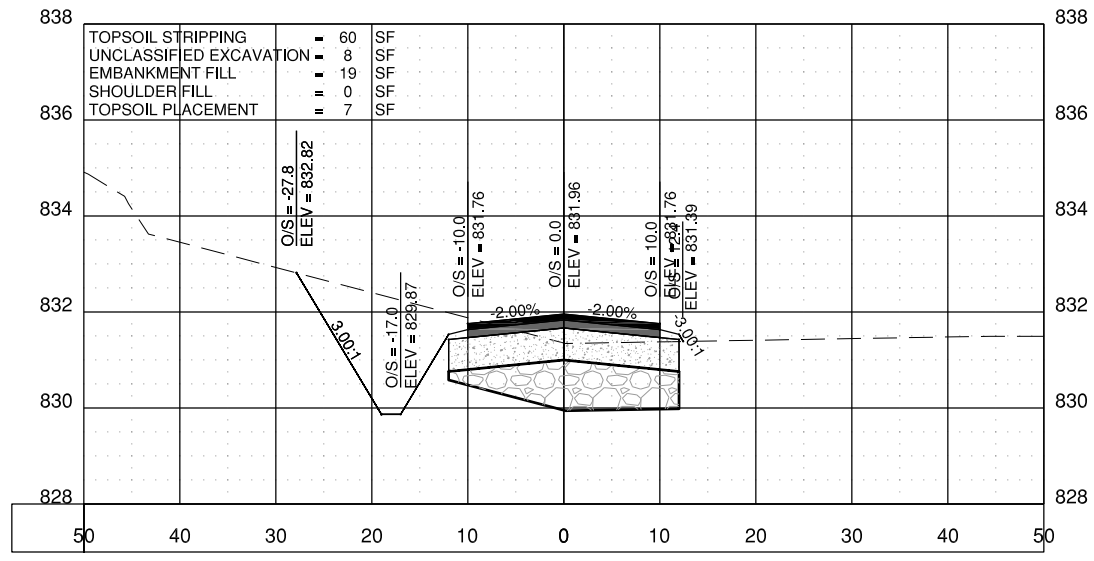
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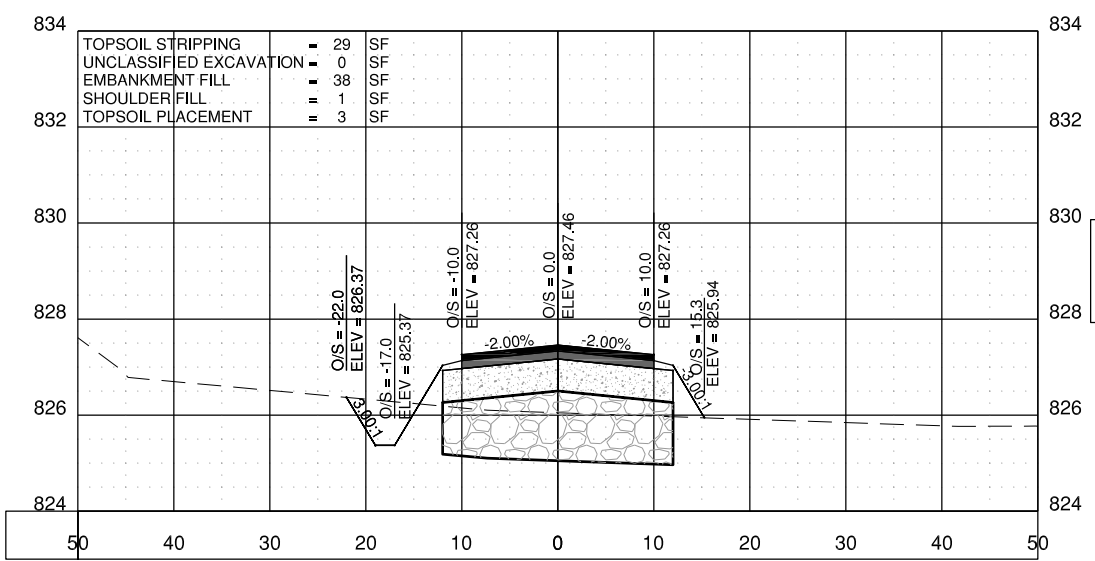
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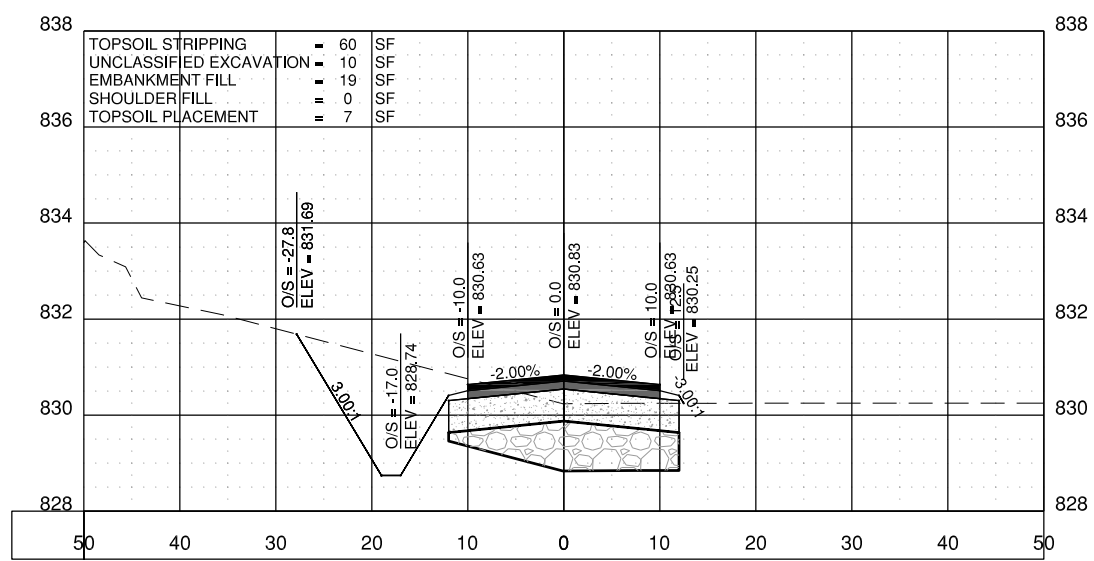
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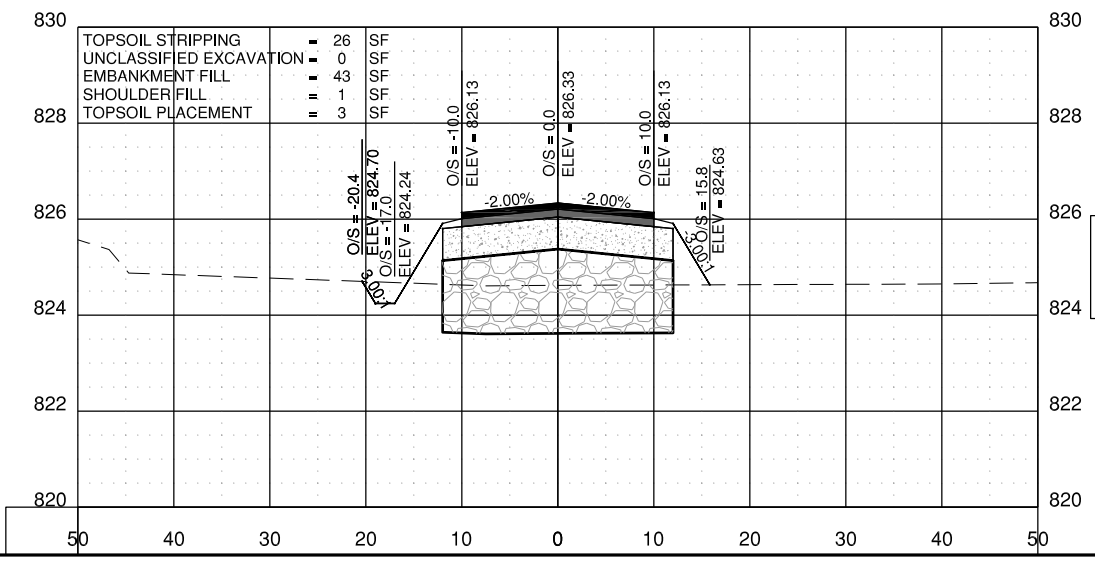
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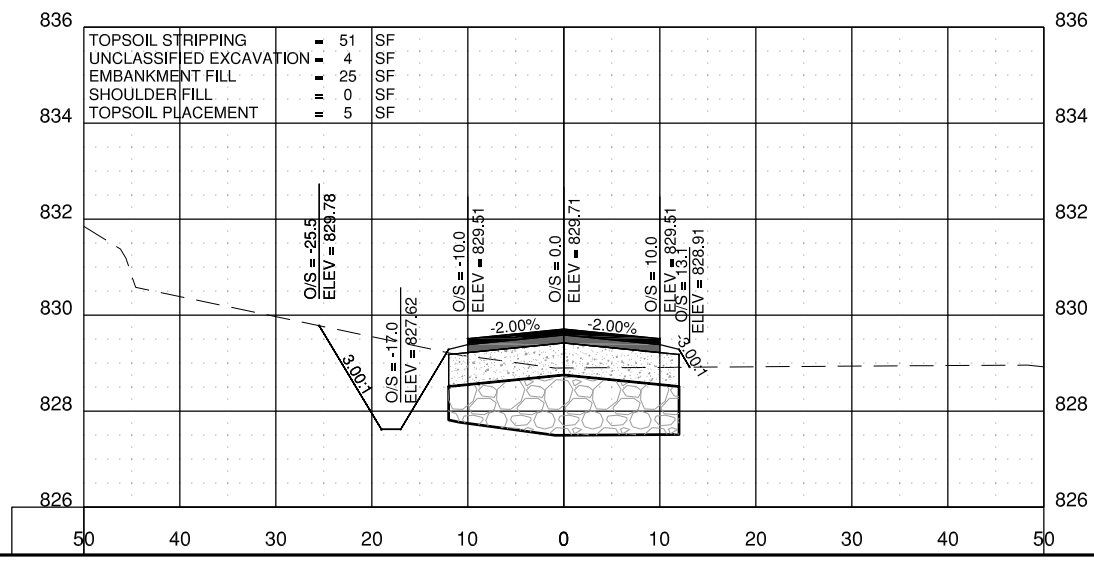
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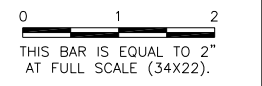


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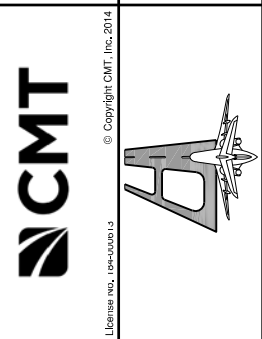


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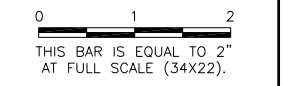
**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD
 CROSS SECTIONS - SHEET 5**



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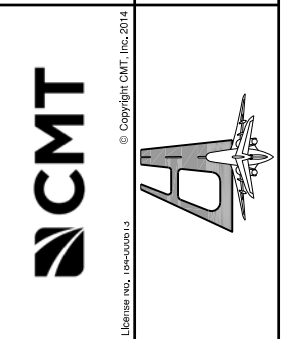
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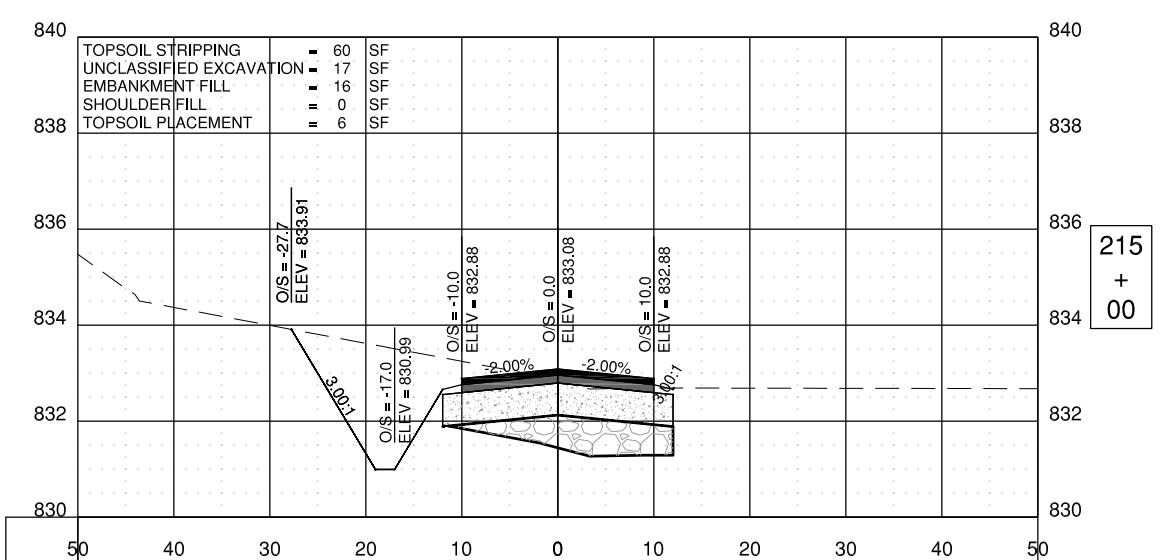
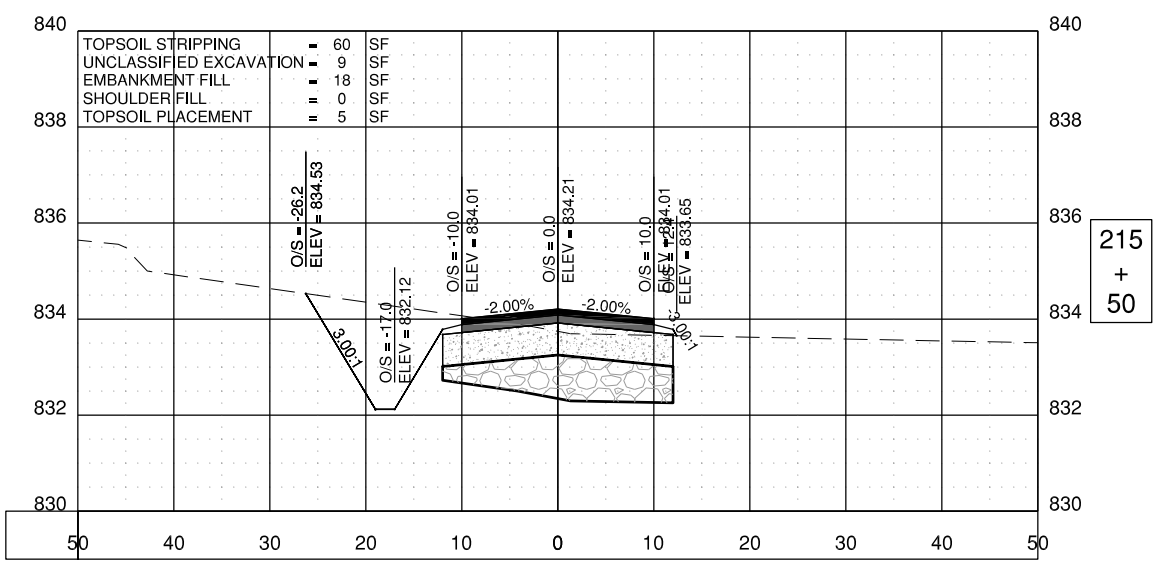
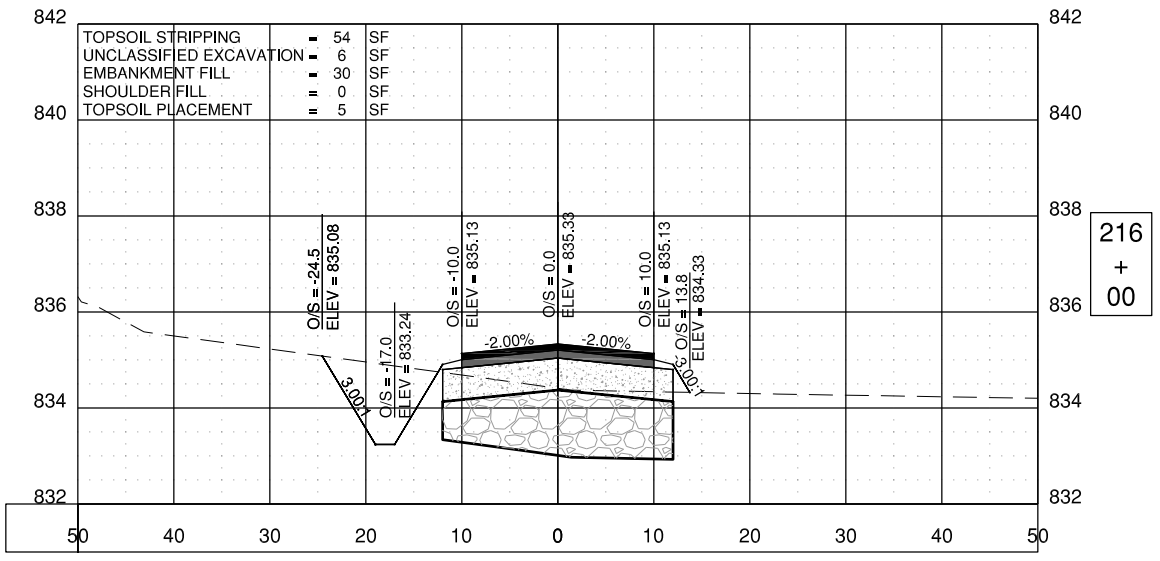
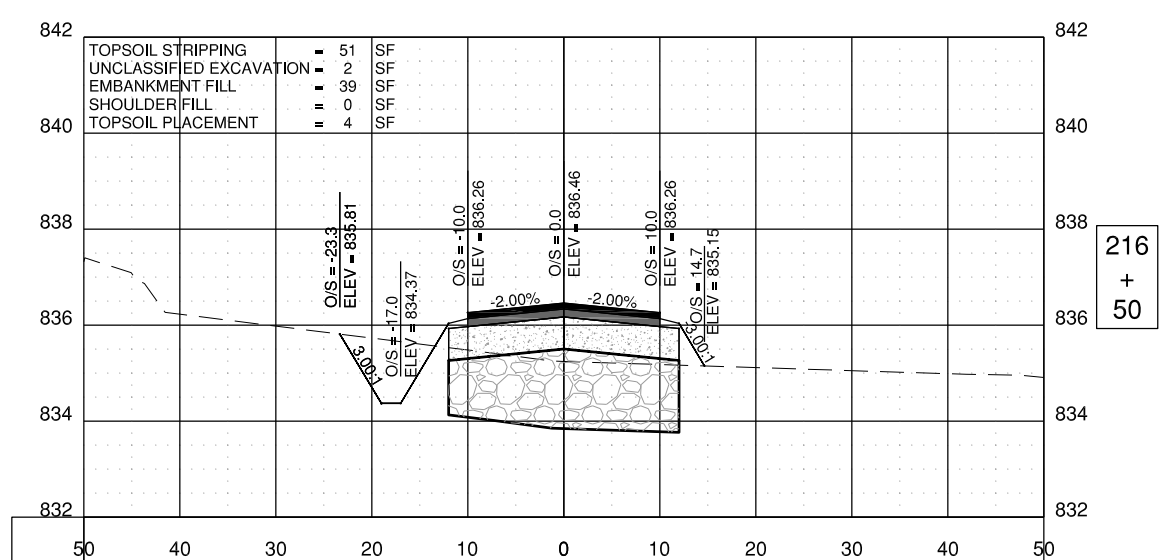
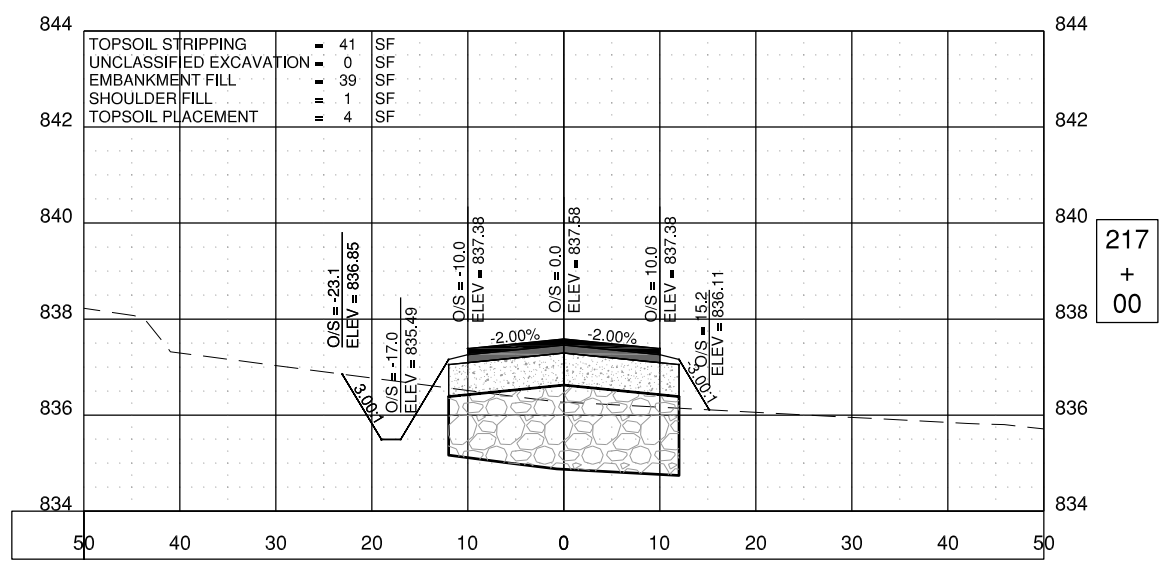
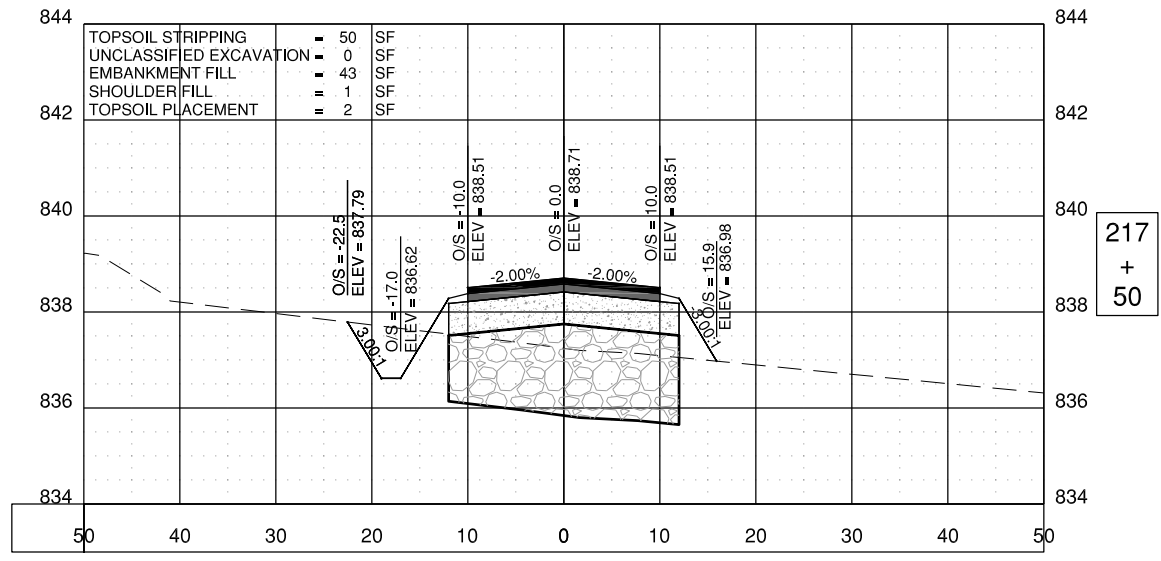


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

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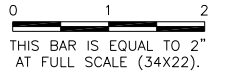
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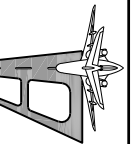


FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD

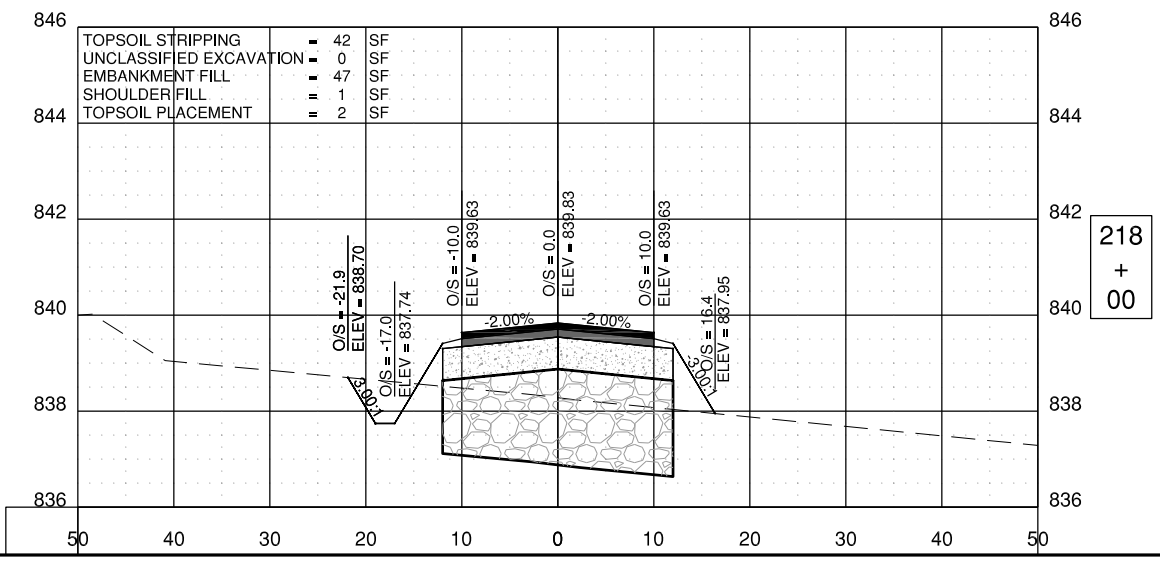
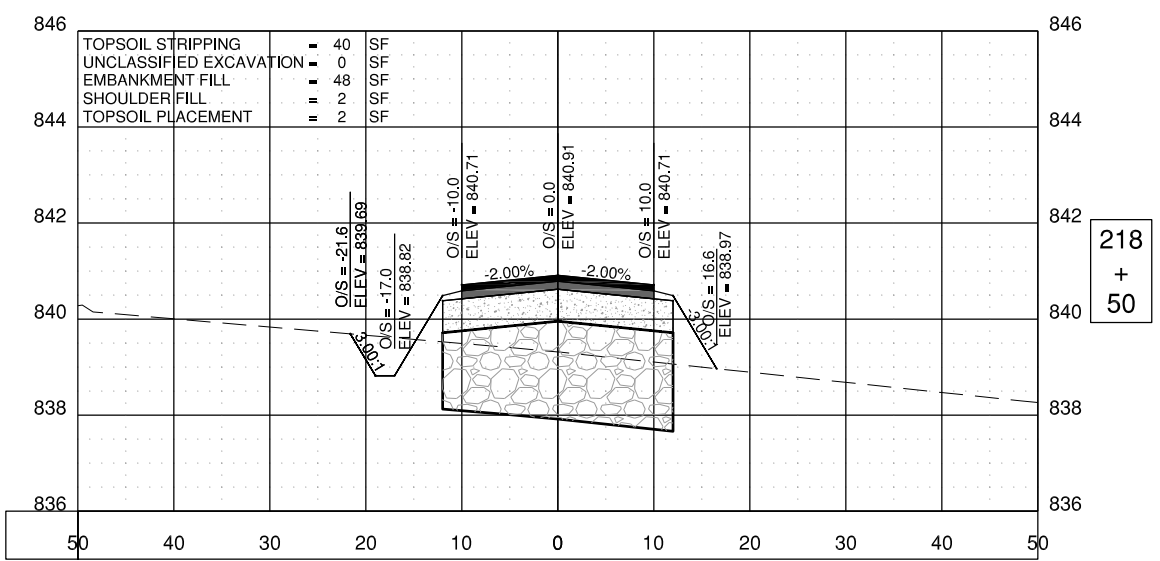
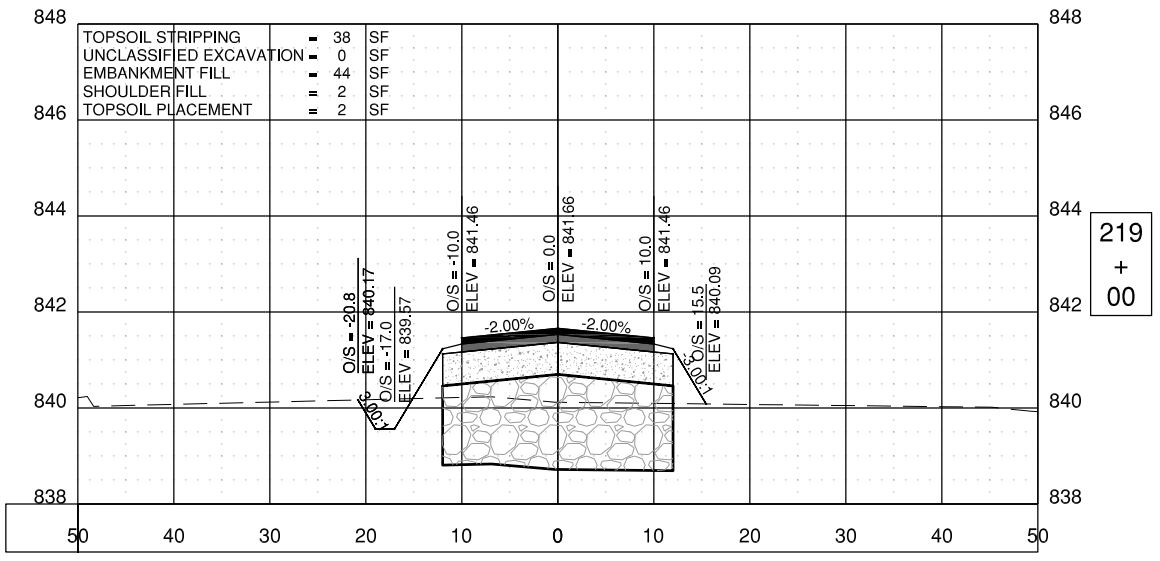
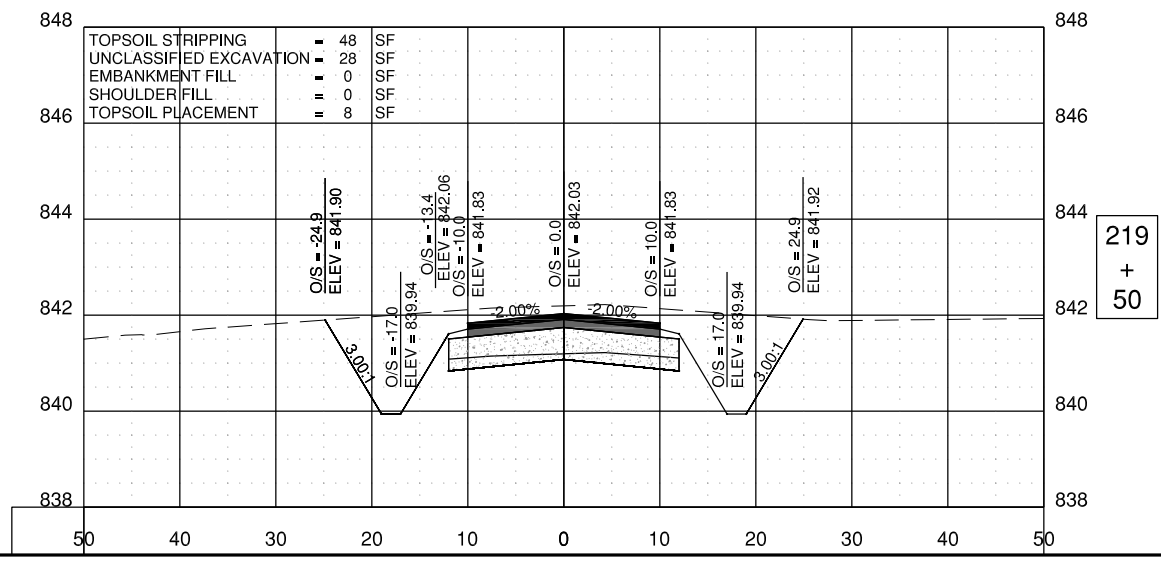
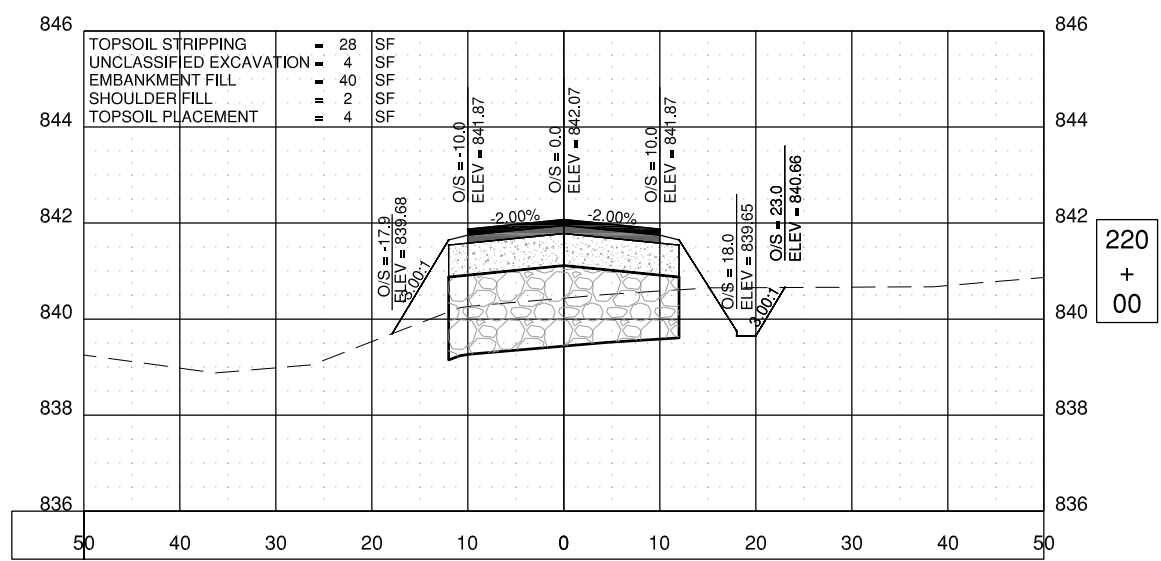
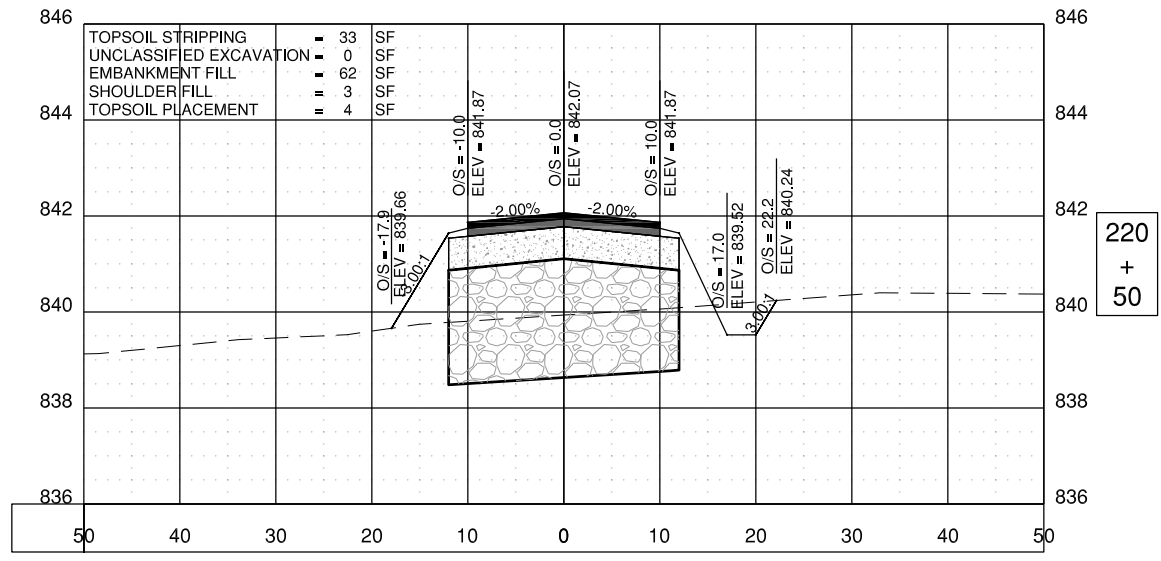
CROSS SECTIONS - SHEET 7



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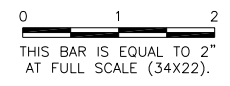


DESIGN BY:	KWS
DRAWN BY:	KWS
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



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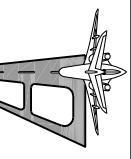


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

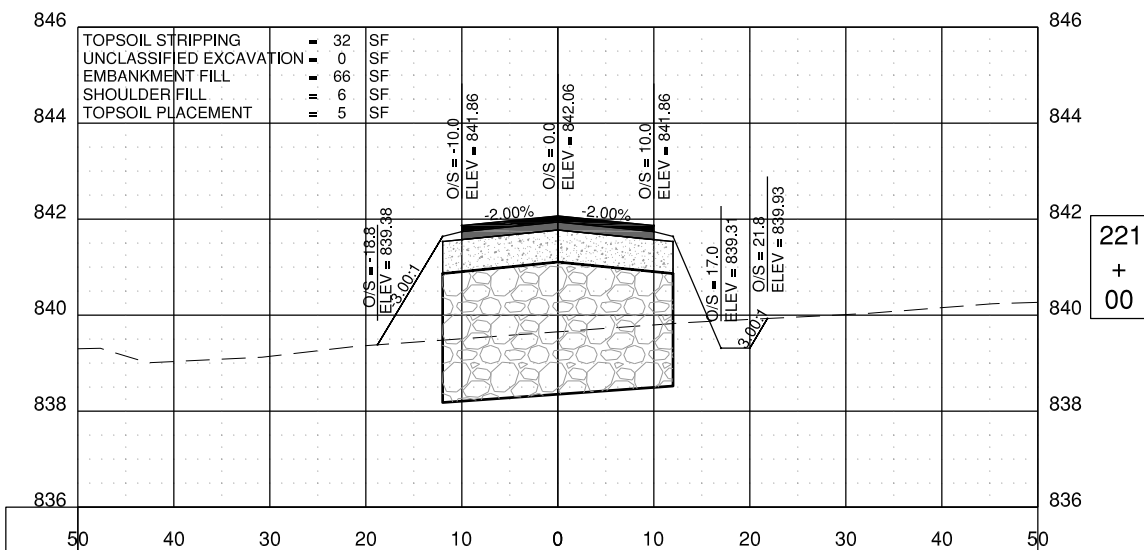
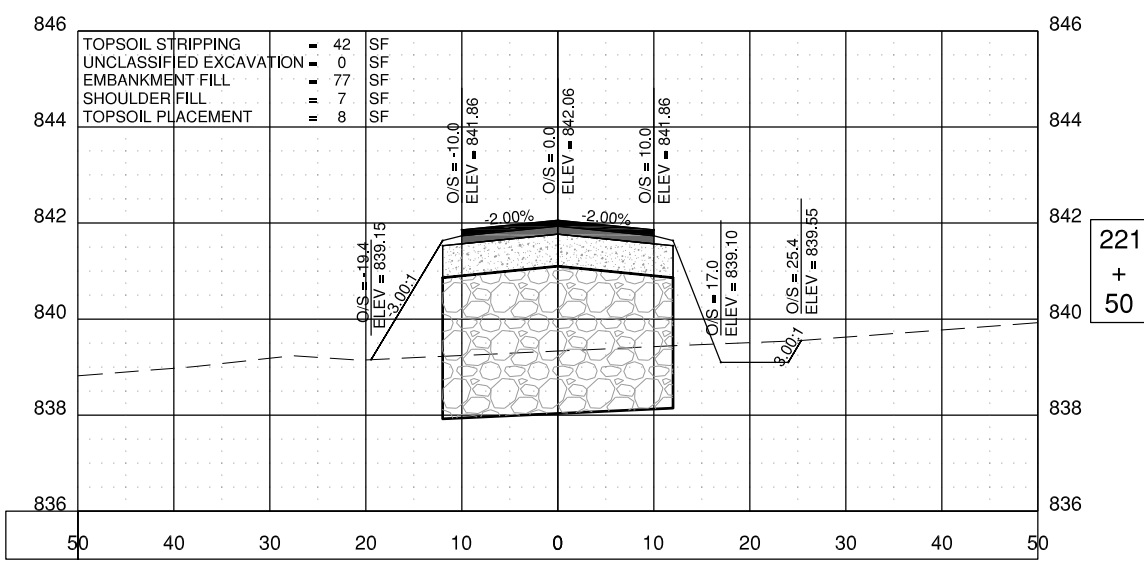
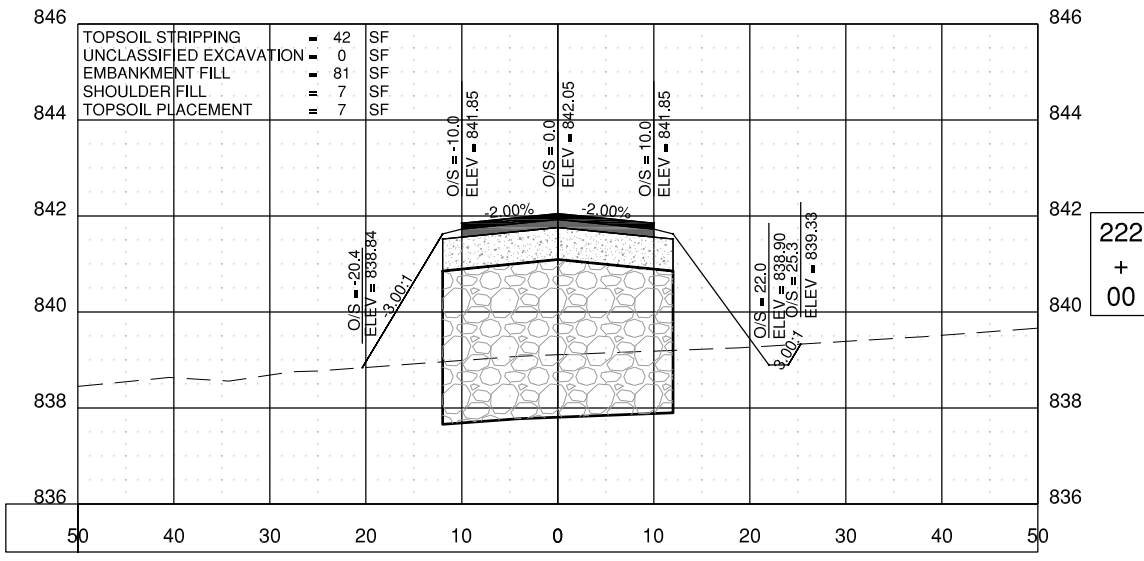
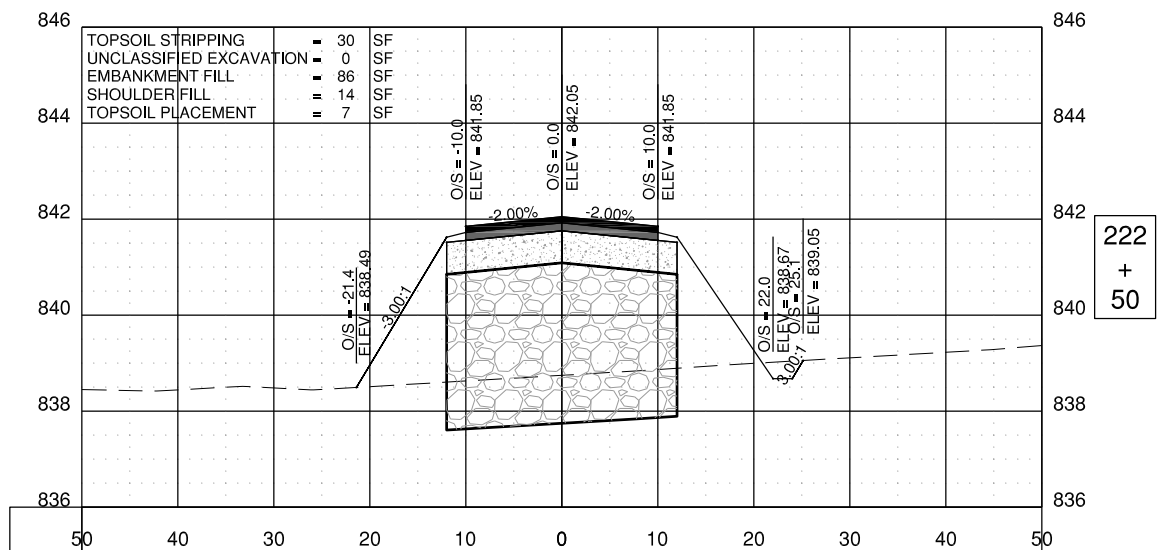
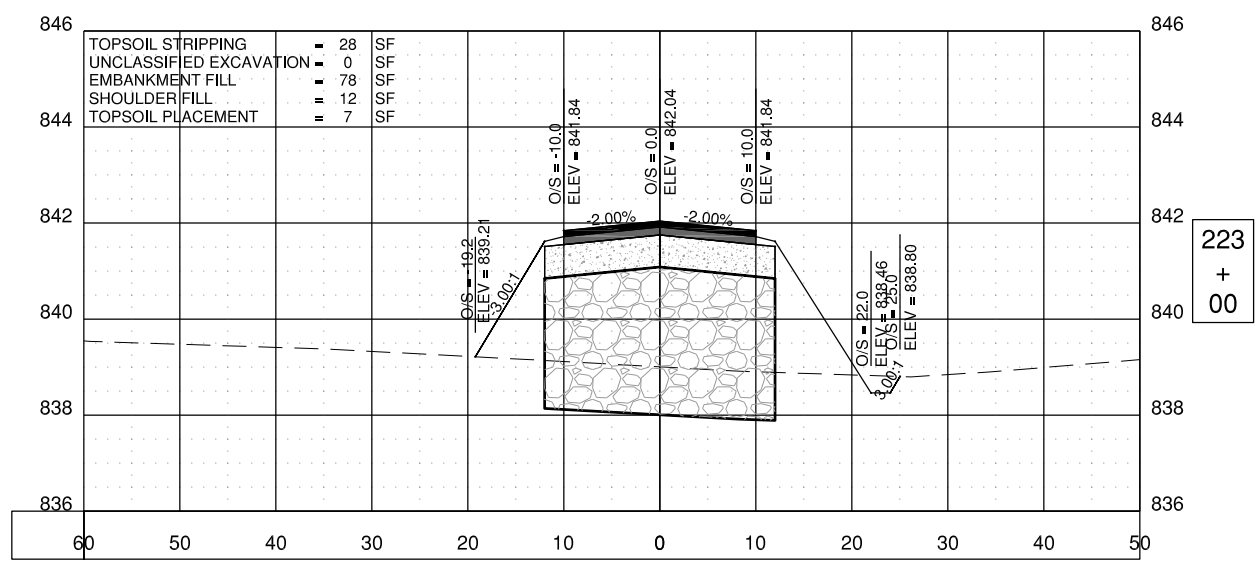
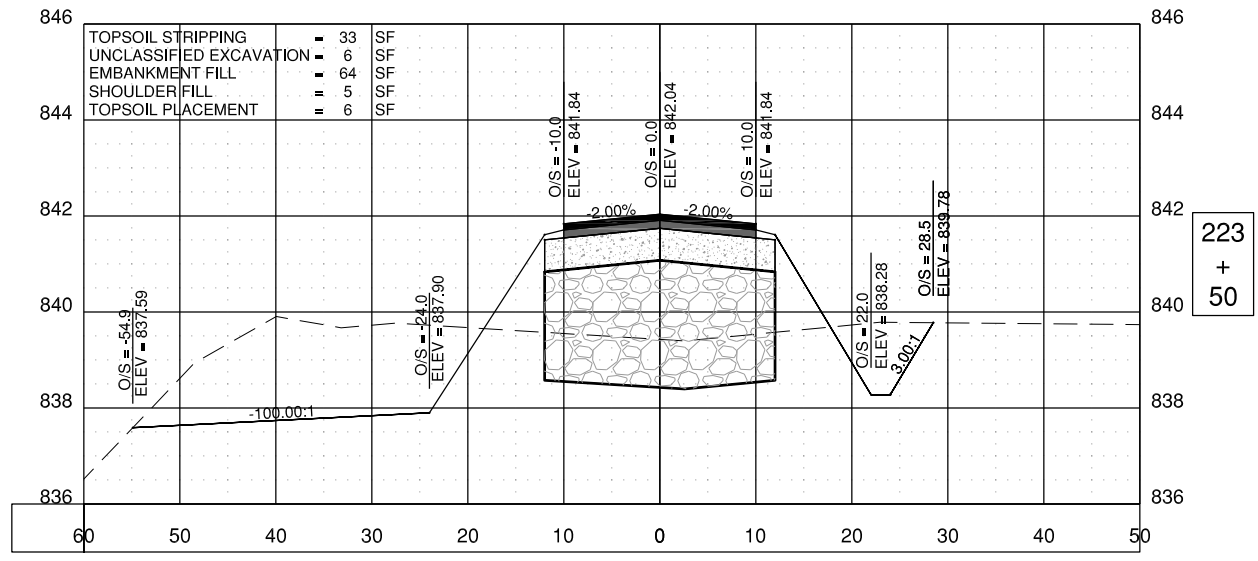
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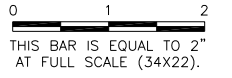


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APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



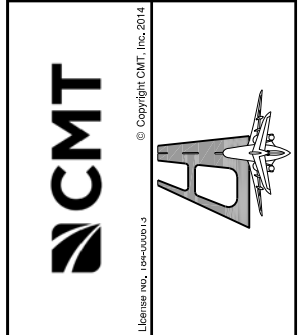
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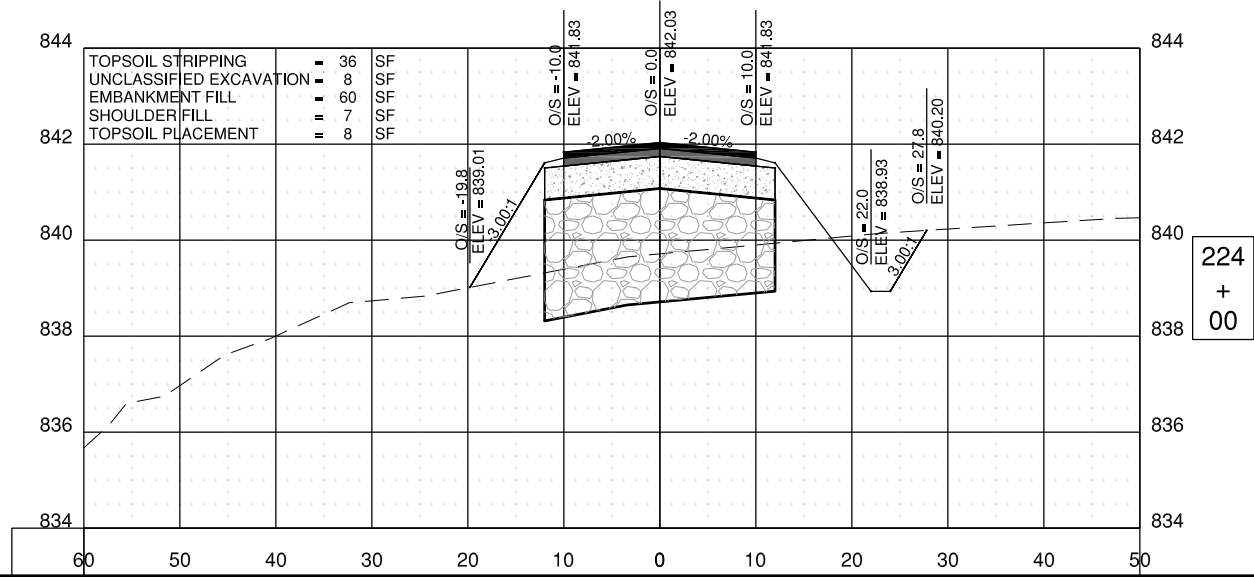
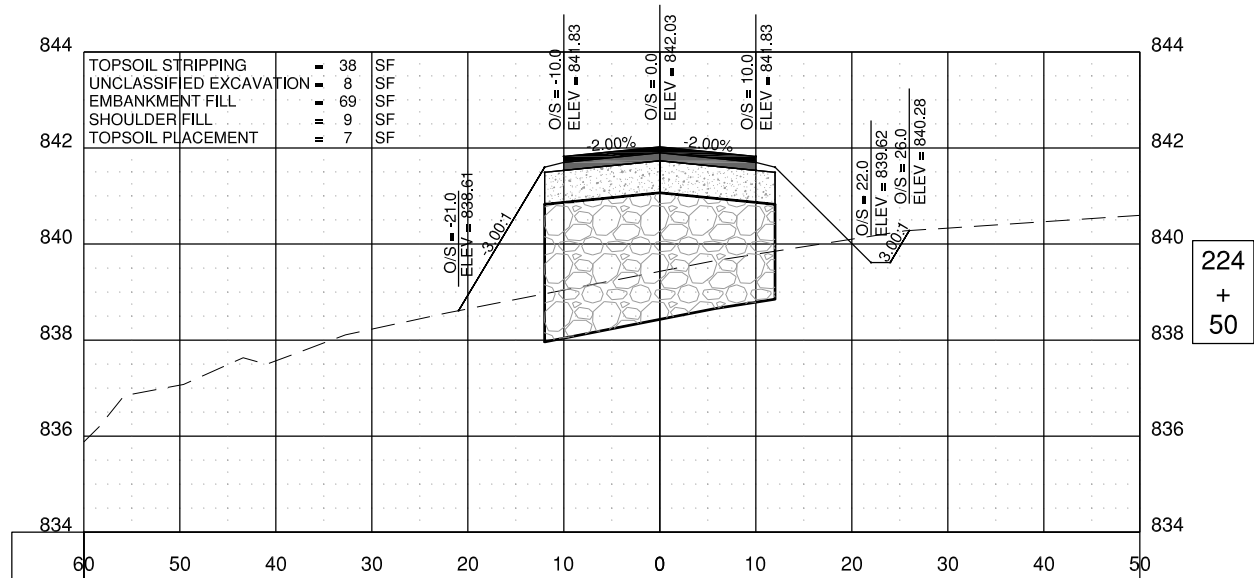
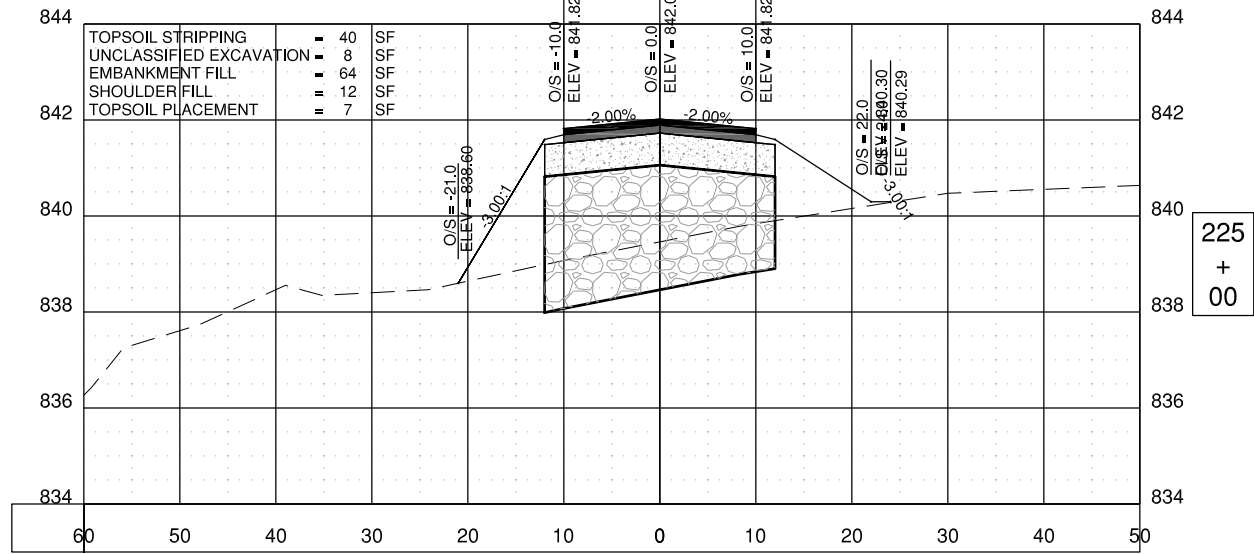
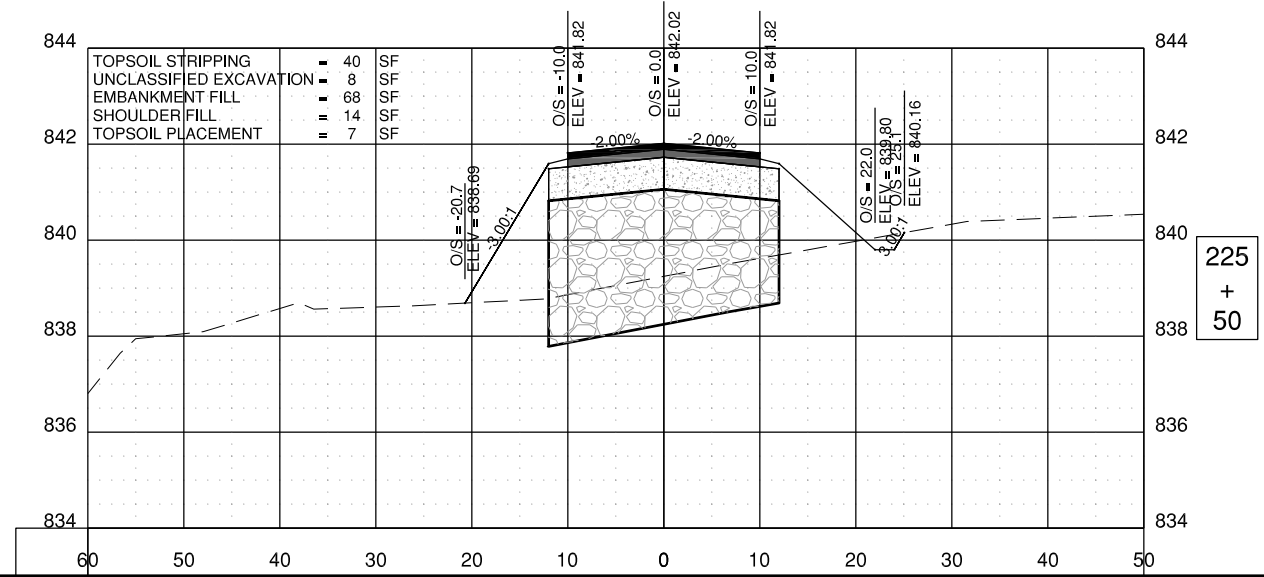
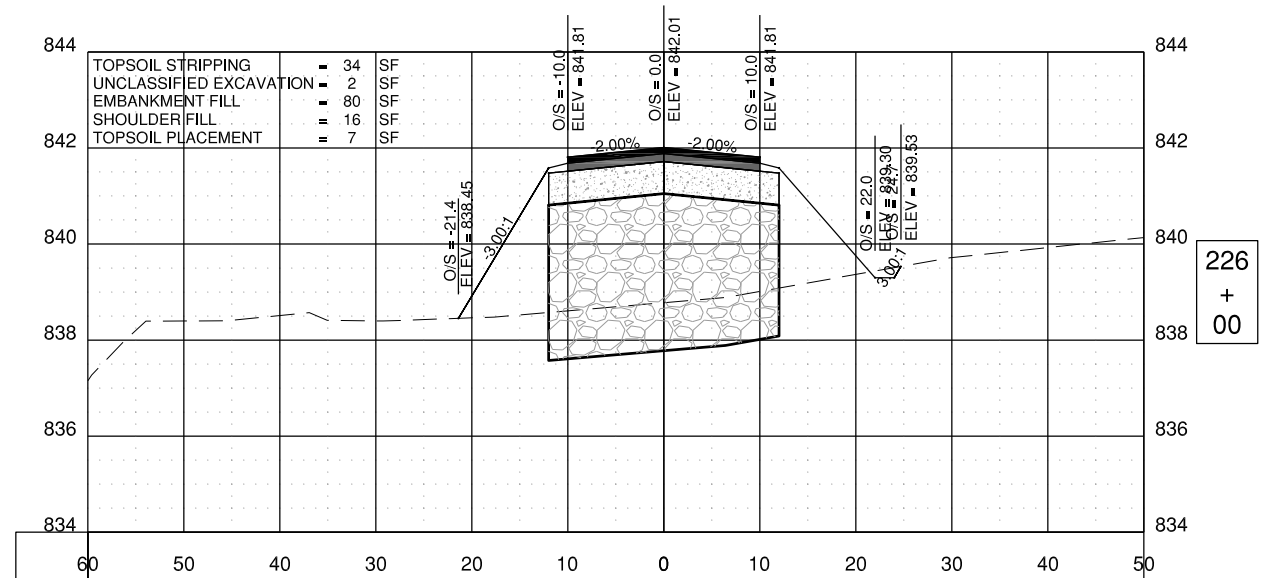
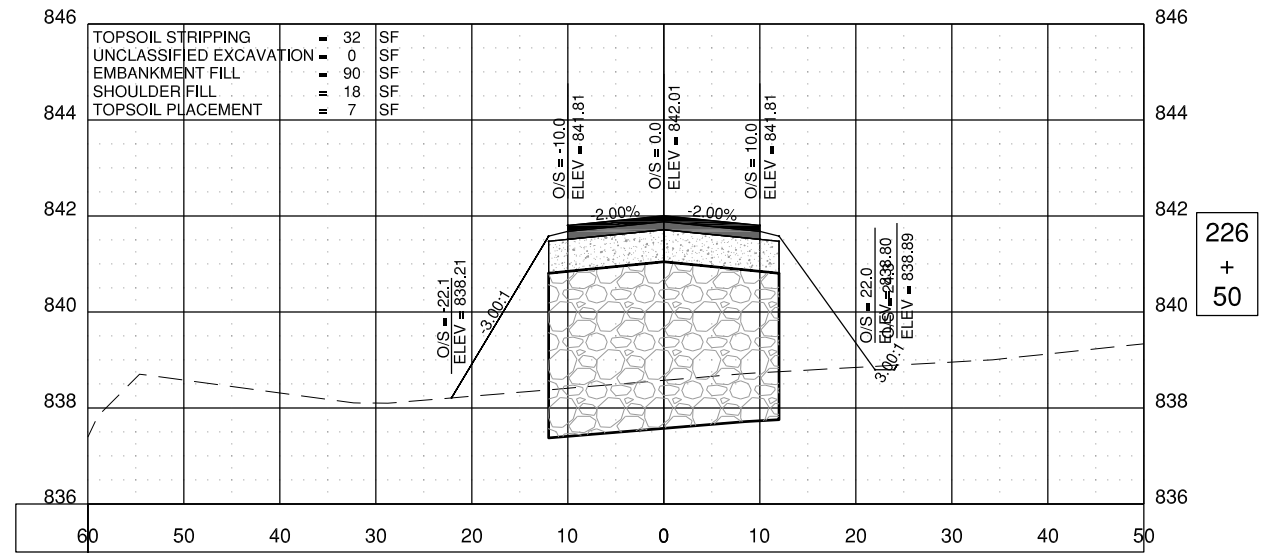


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

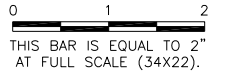
CROSS SECTIONS - SHEET 9



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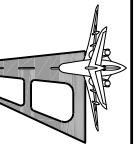


**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

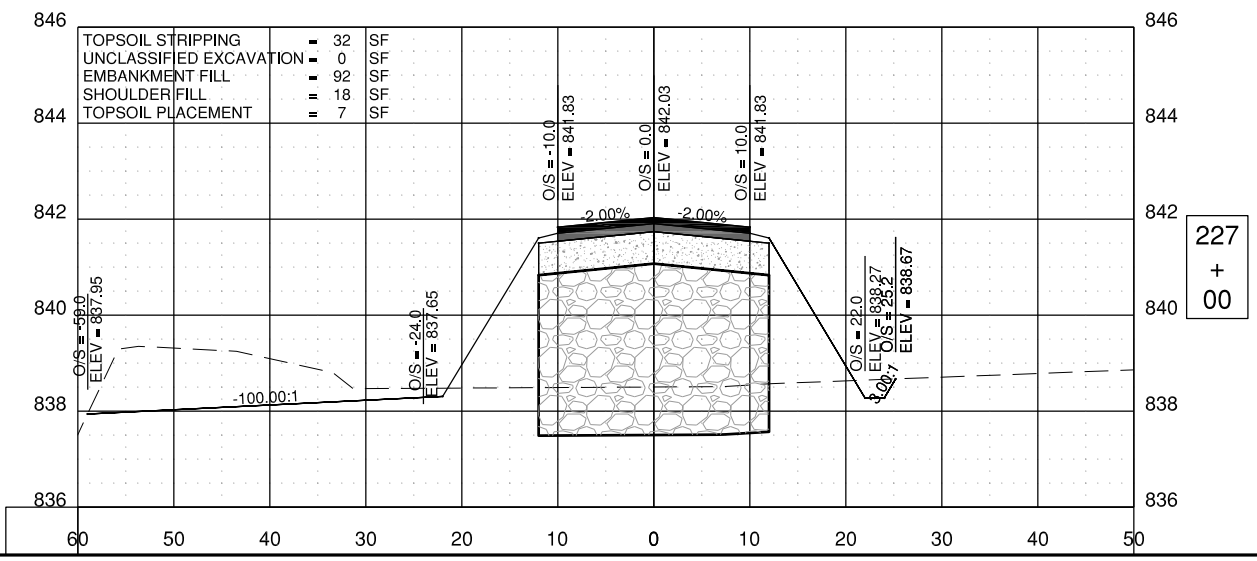
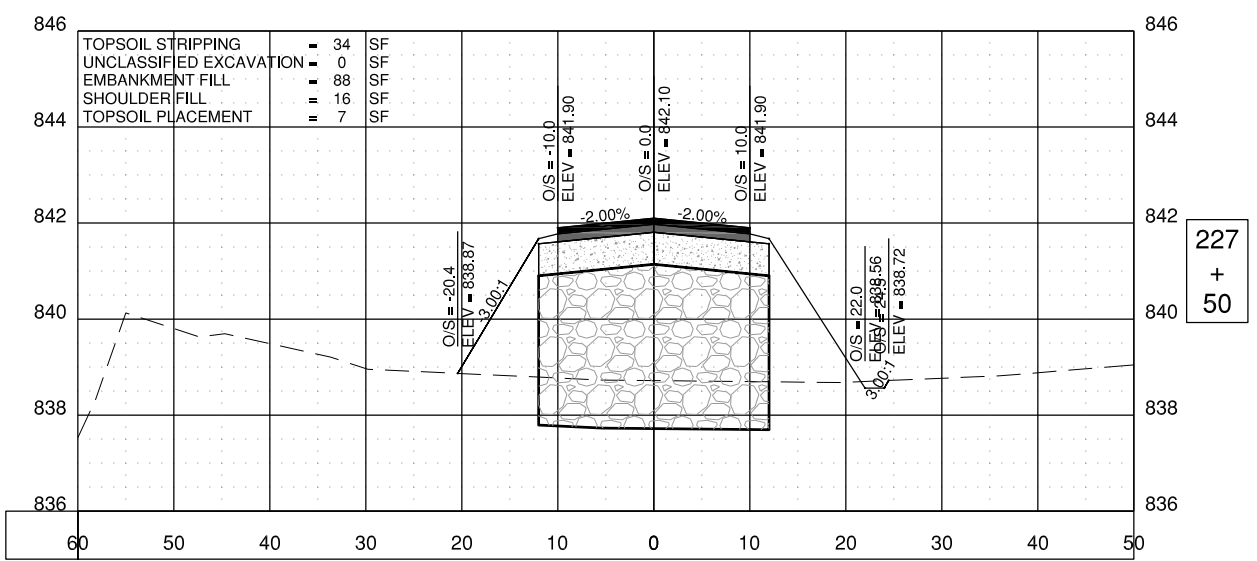
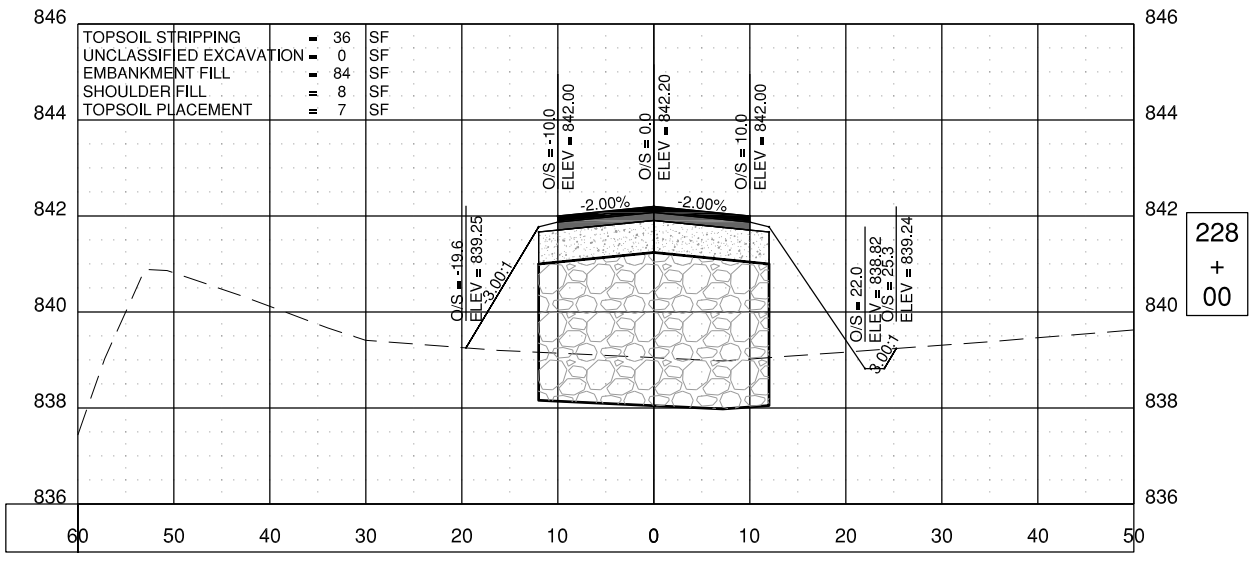
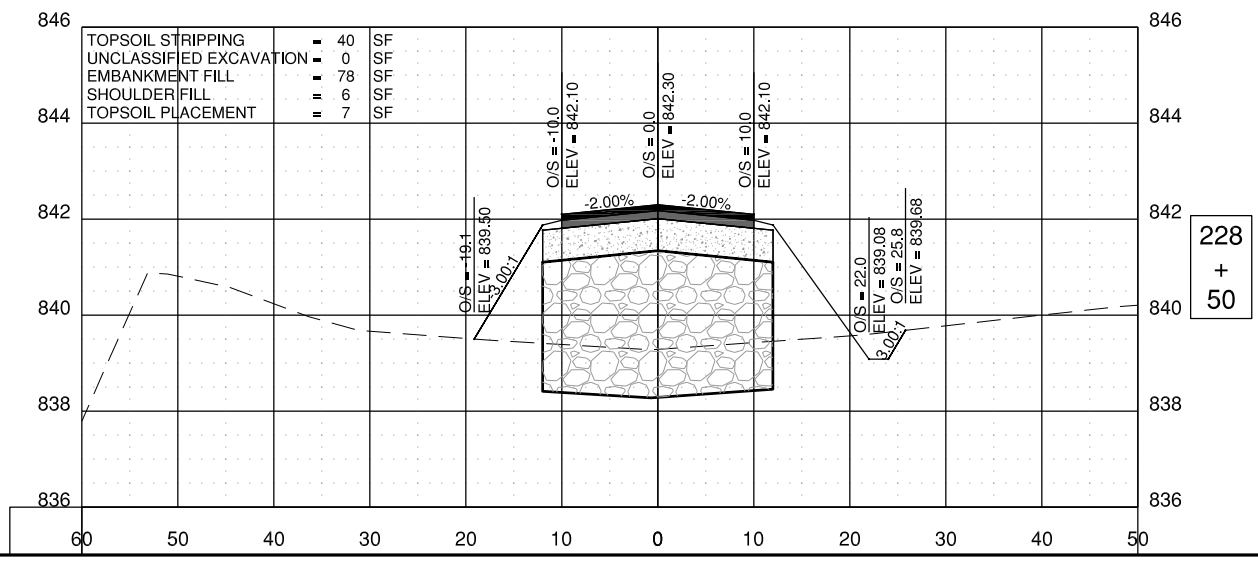
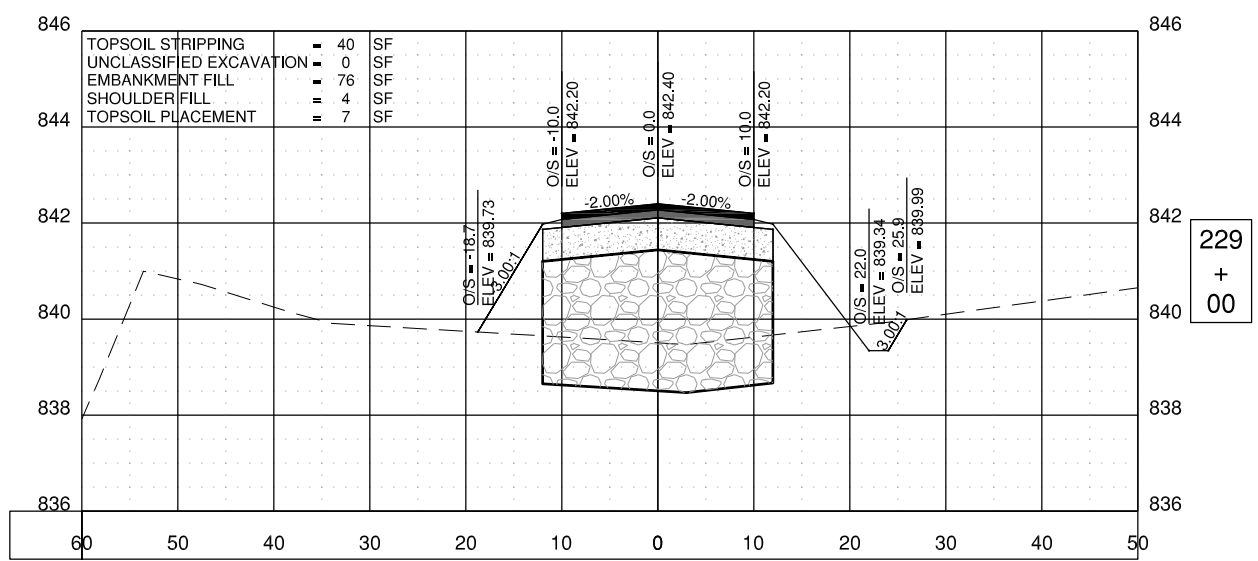
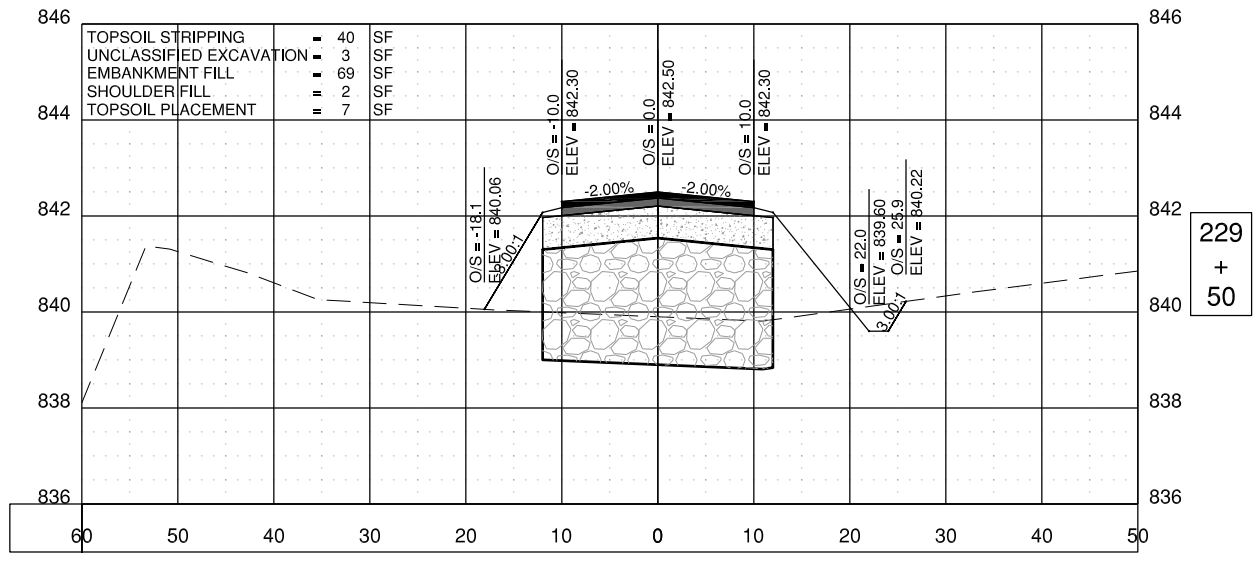
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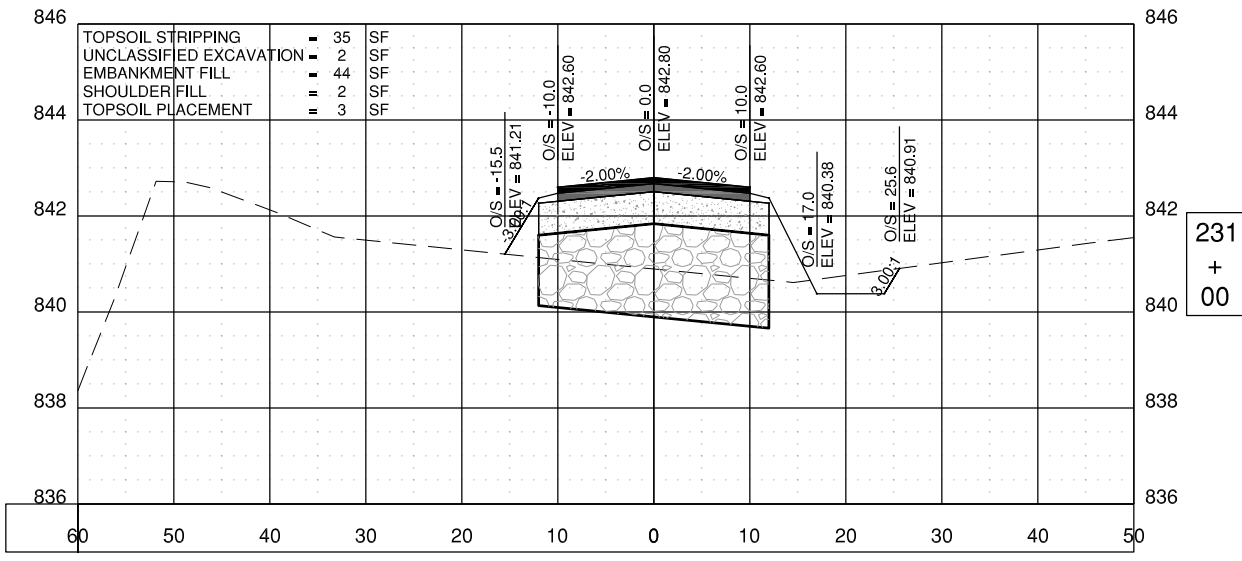
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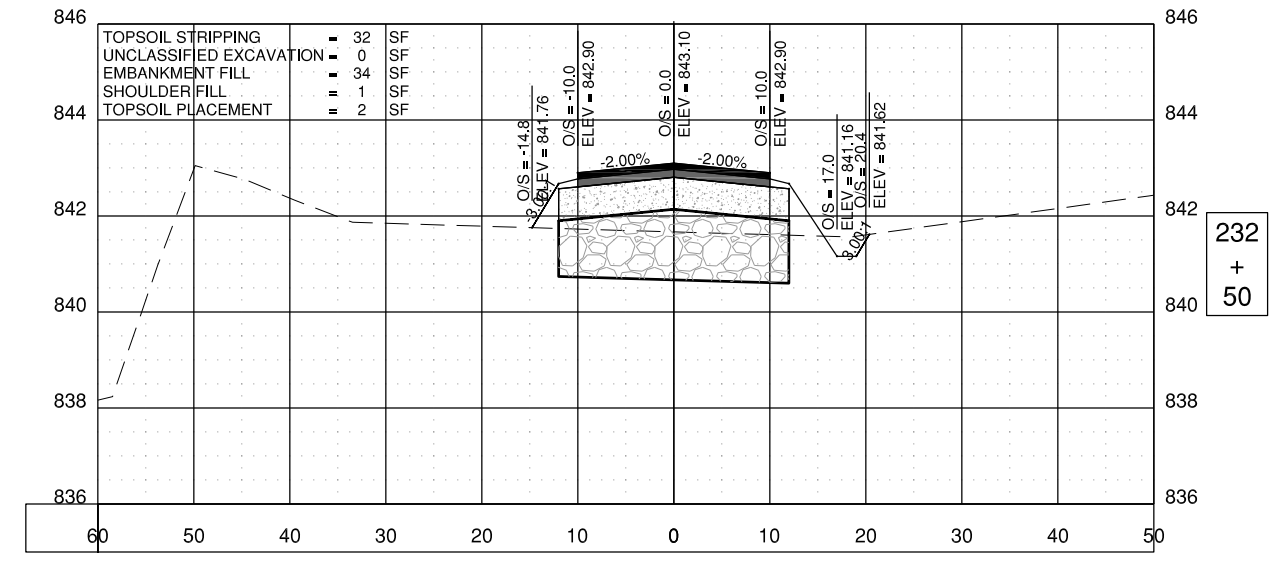
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DRAWN BY:	KWS
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



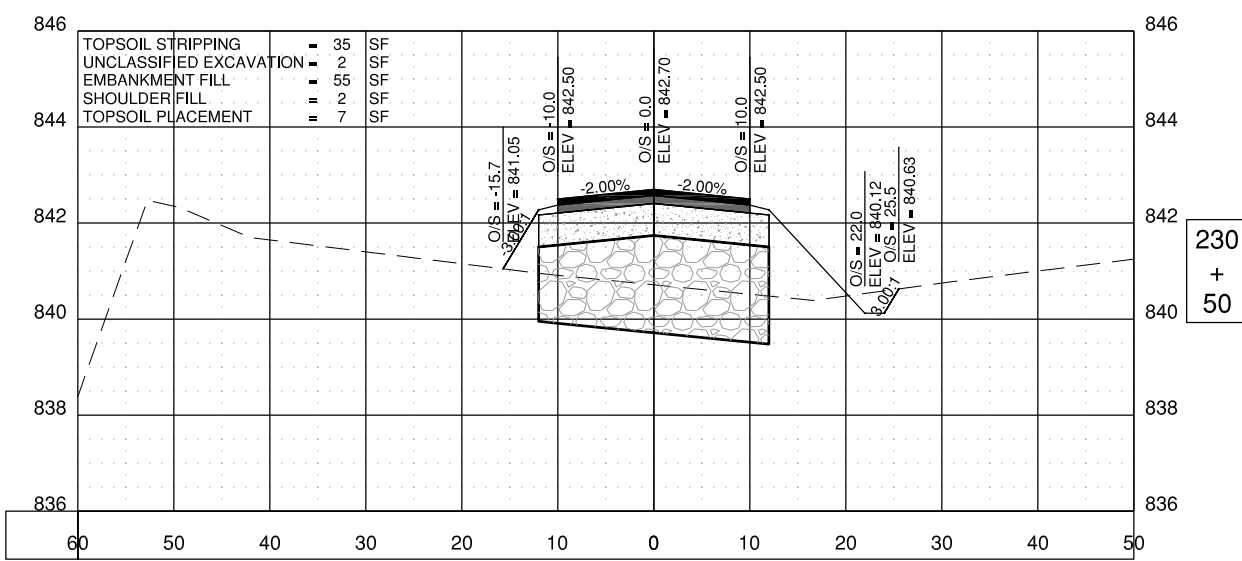
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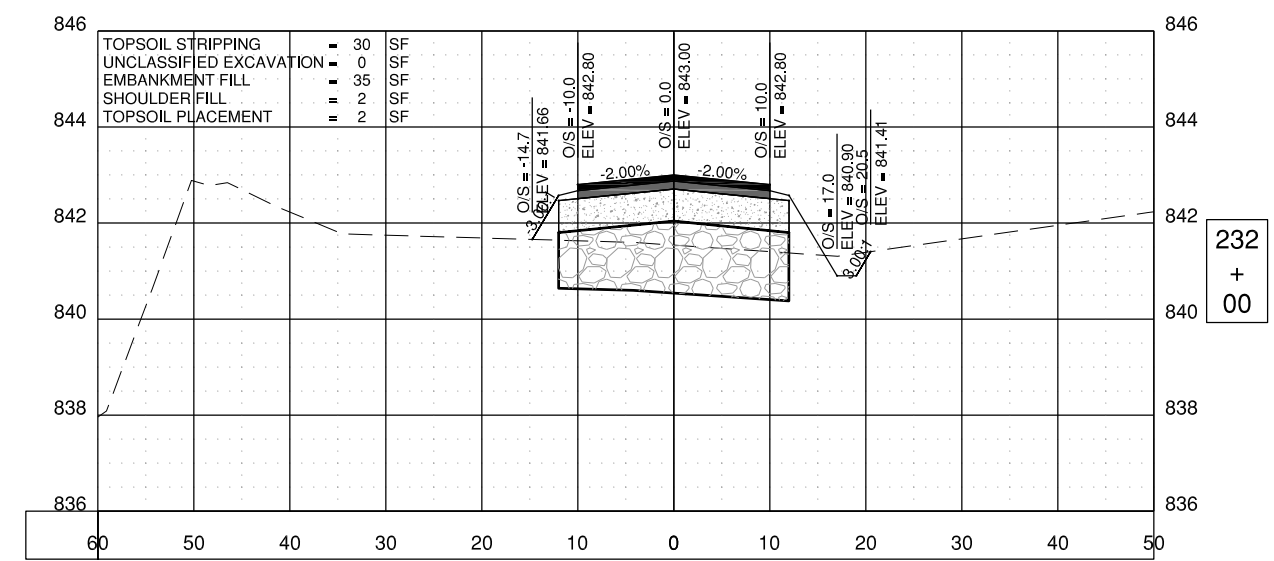
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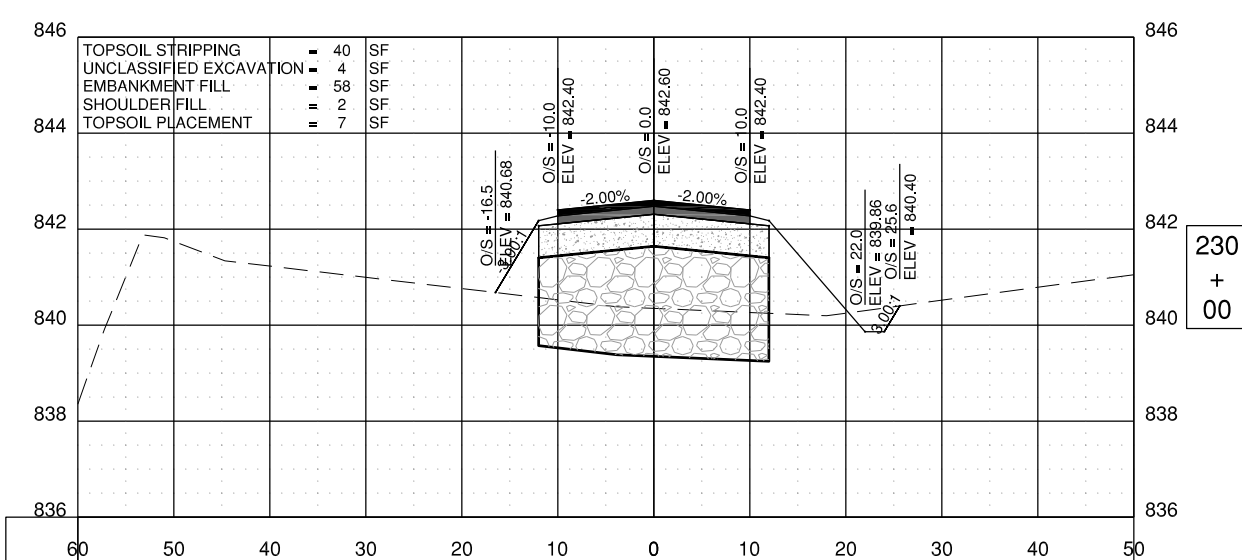
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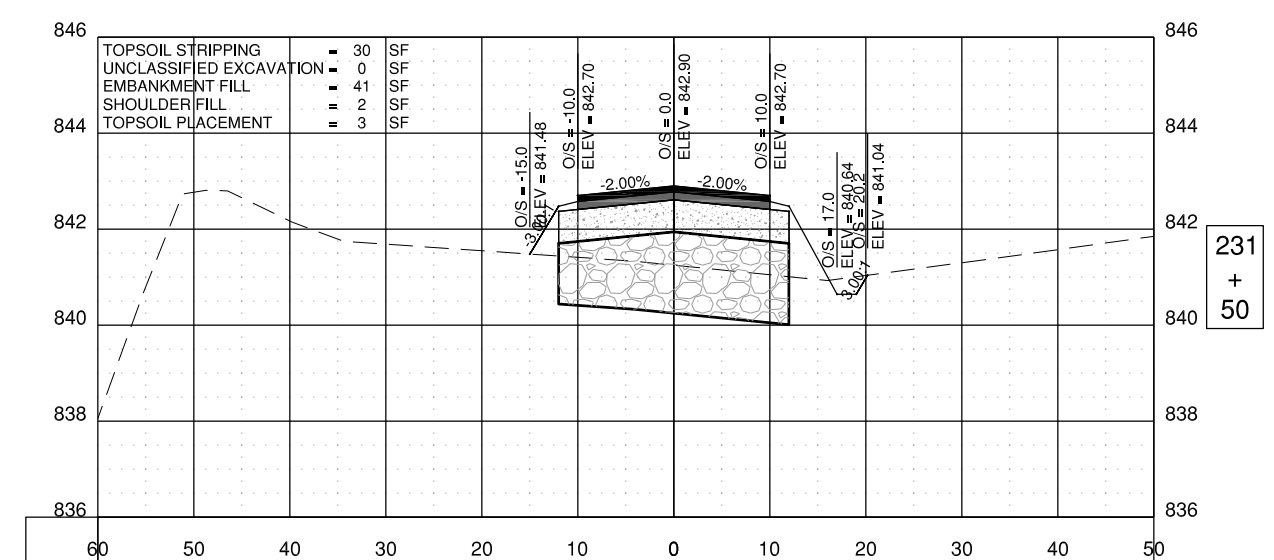
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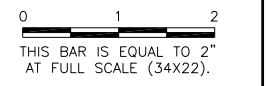


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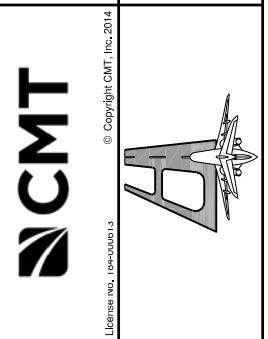
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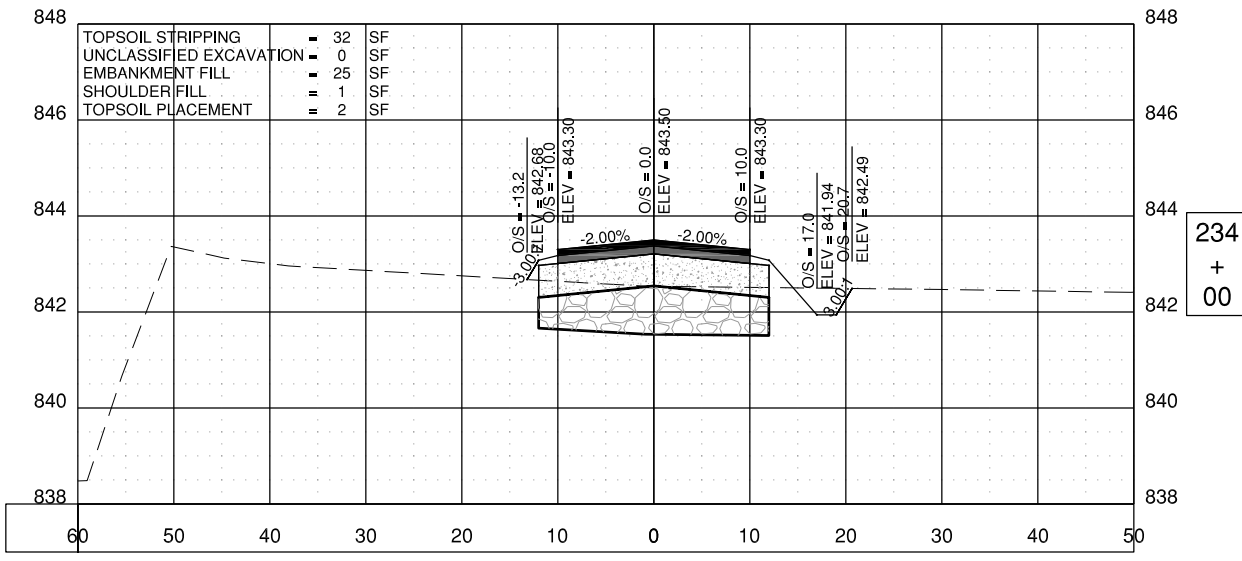
**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

CROSS SECTIONS - SHEET 11

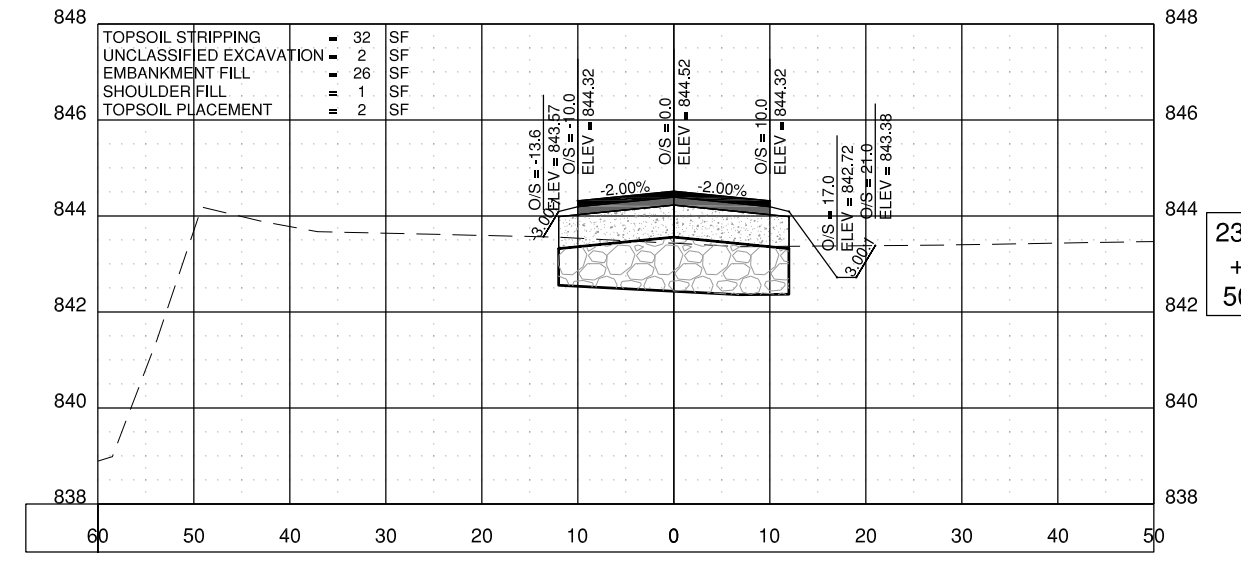


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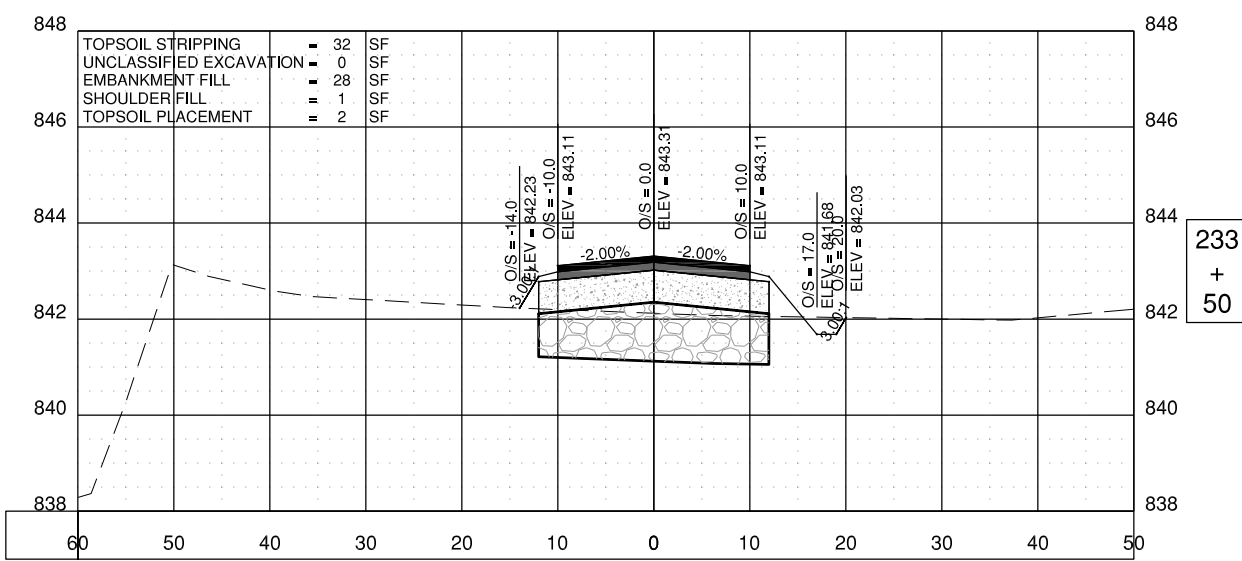
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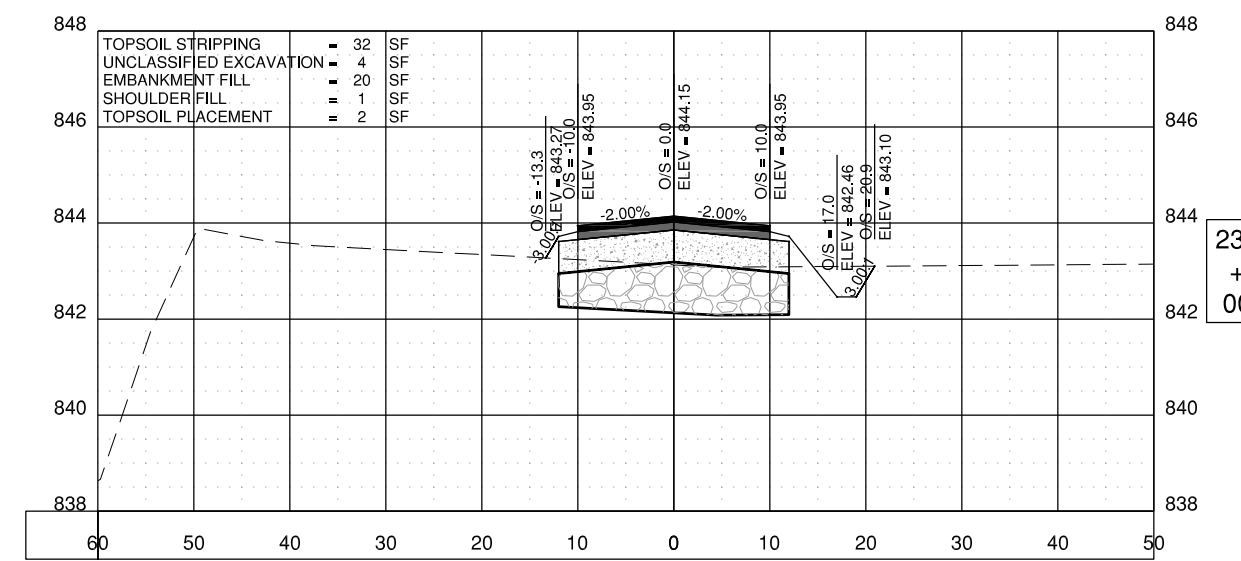
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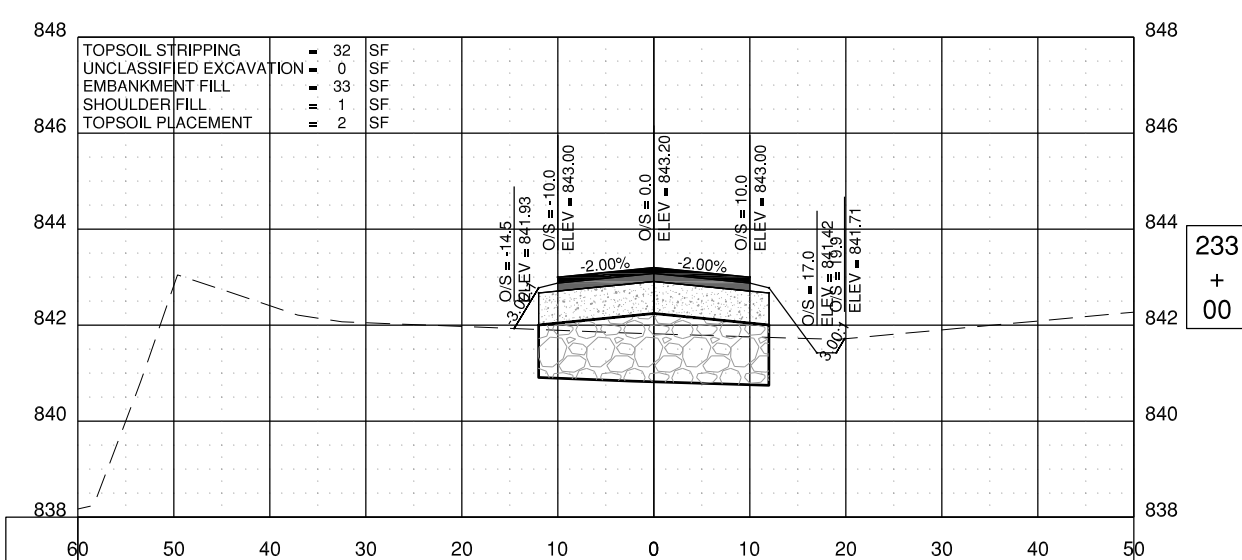
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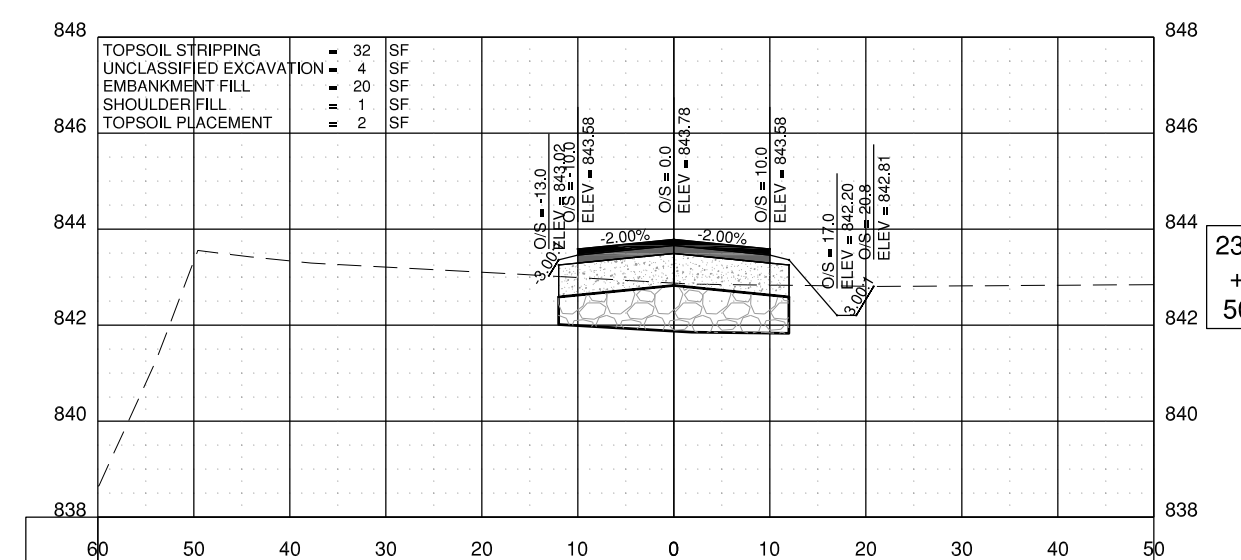
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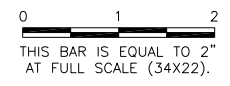


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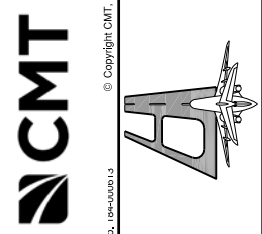
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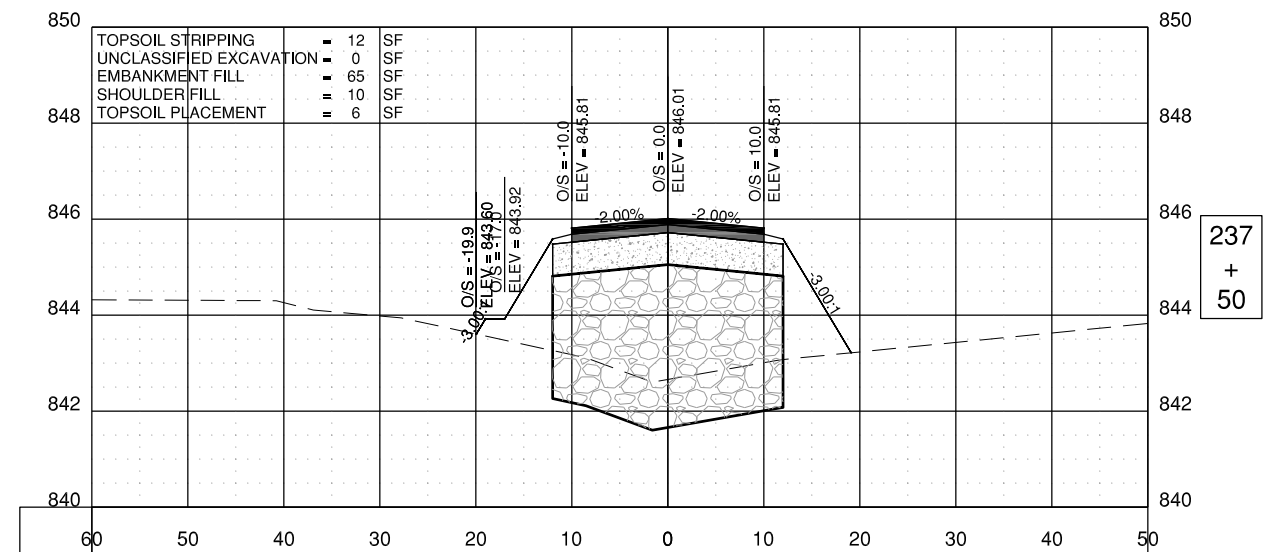
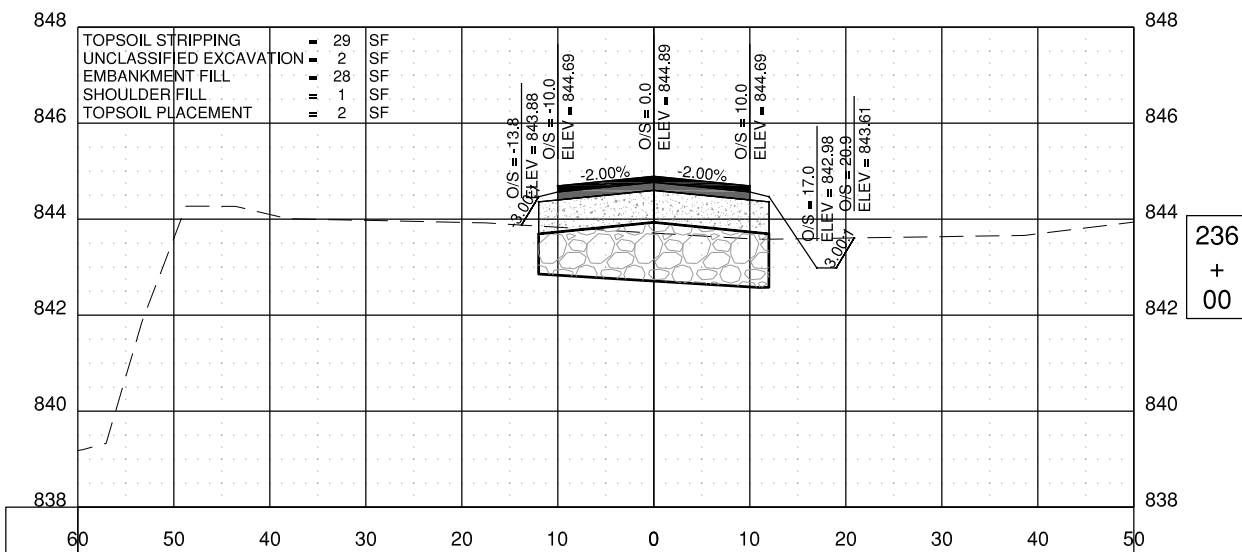
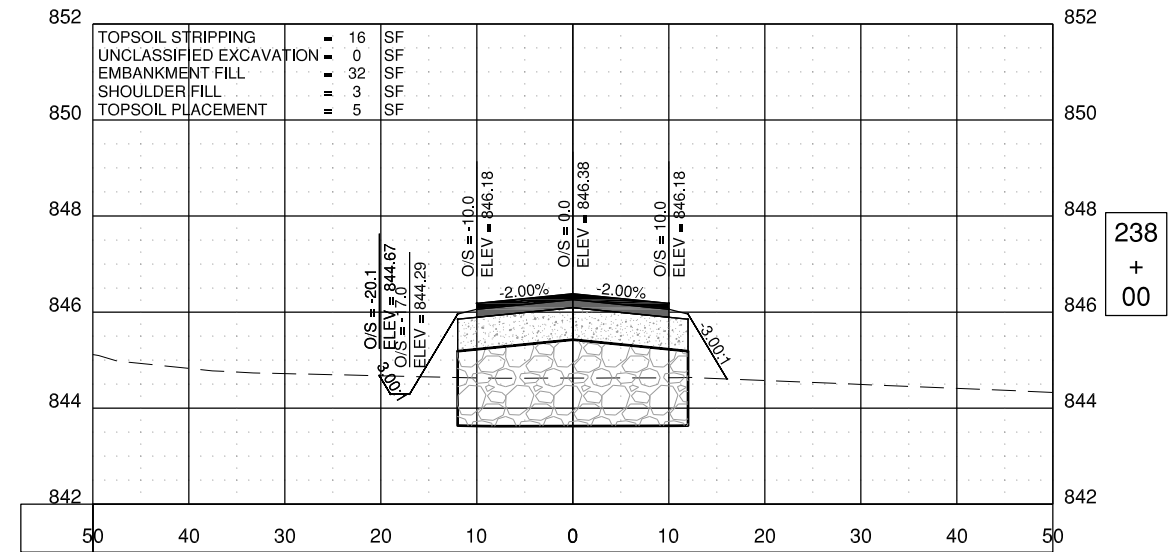
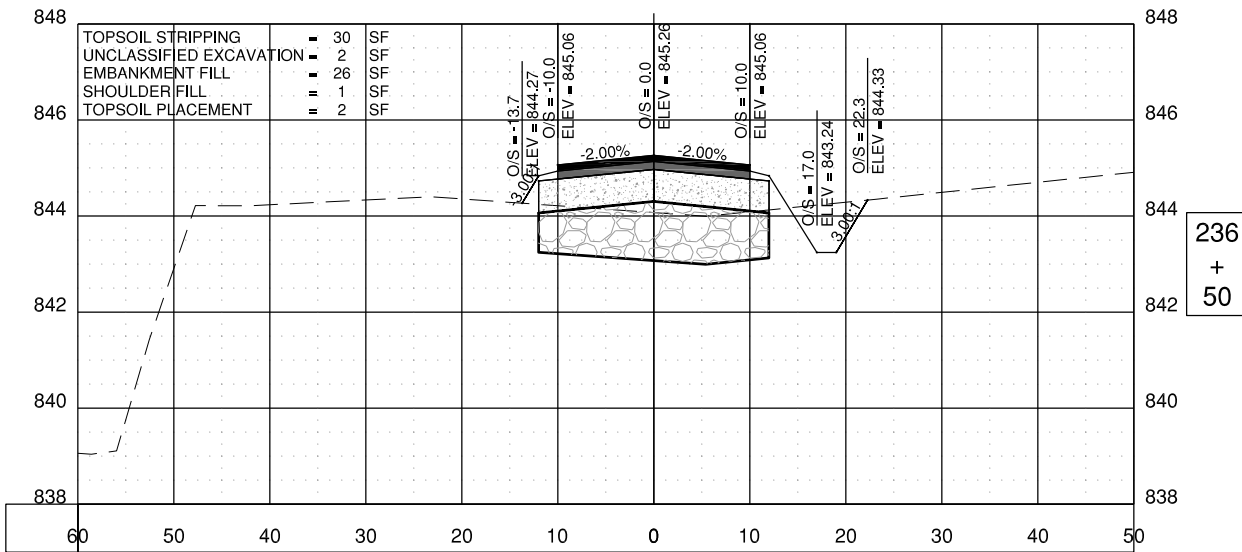
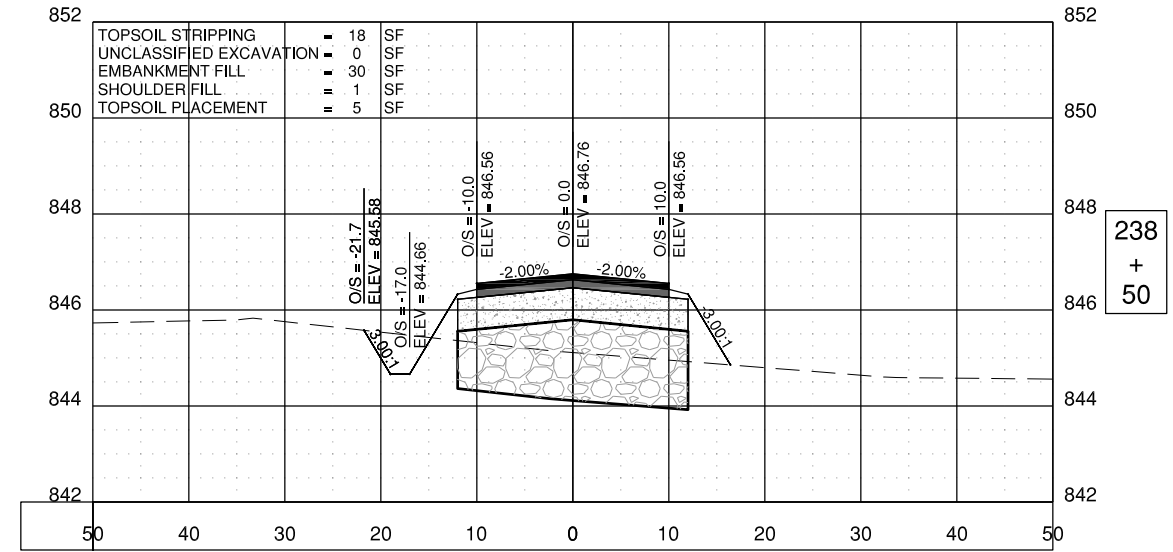
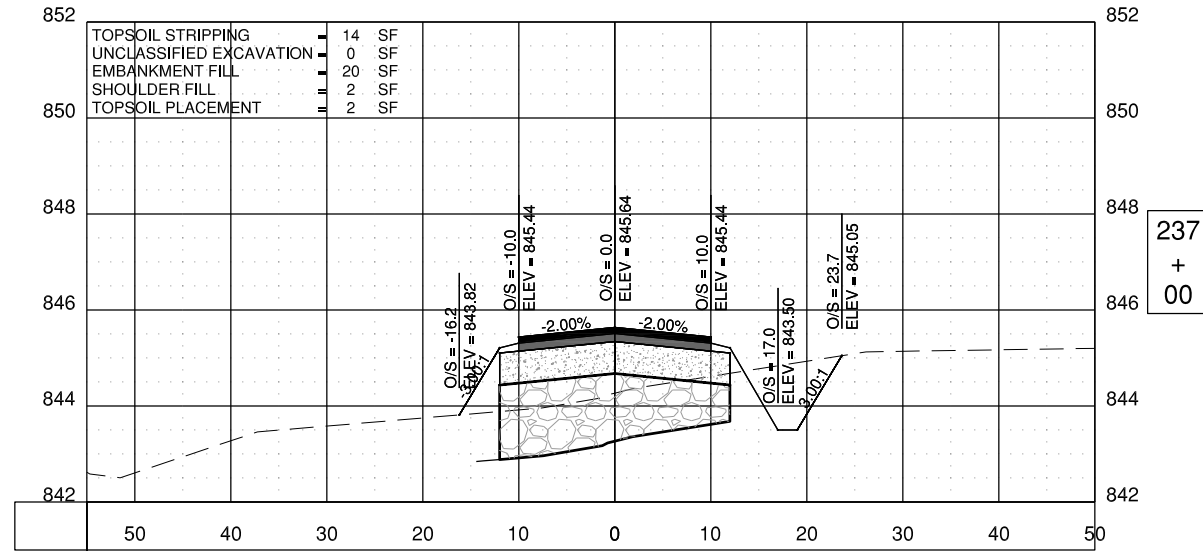
**FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

CROSS SECTIONS - SHEET 12

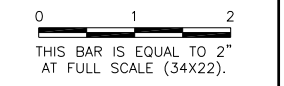


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FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD

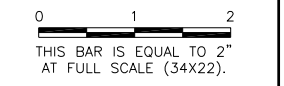
CROSS SECTIONS - SHEET 13

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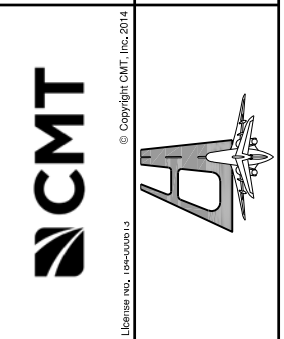
DESIGN BY:	KWS
DRAWN BY:	KWS
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02

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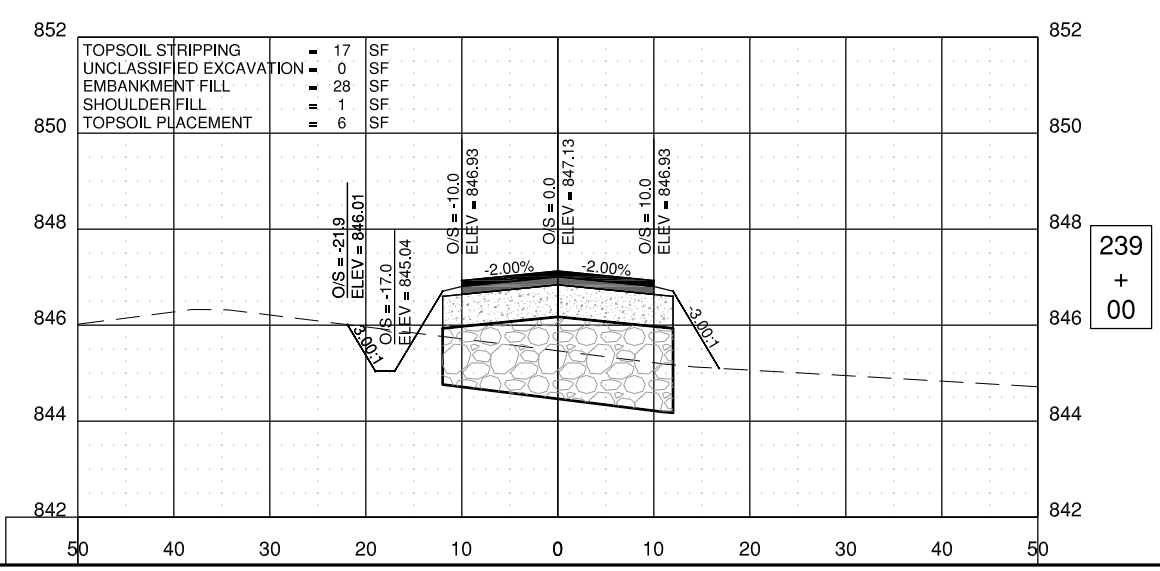
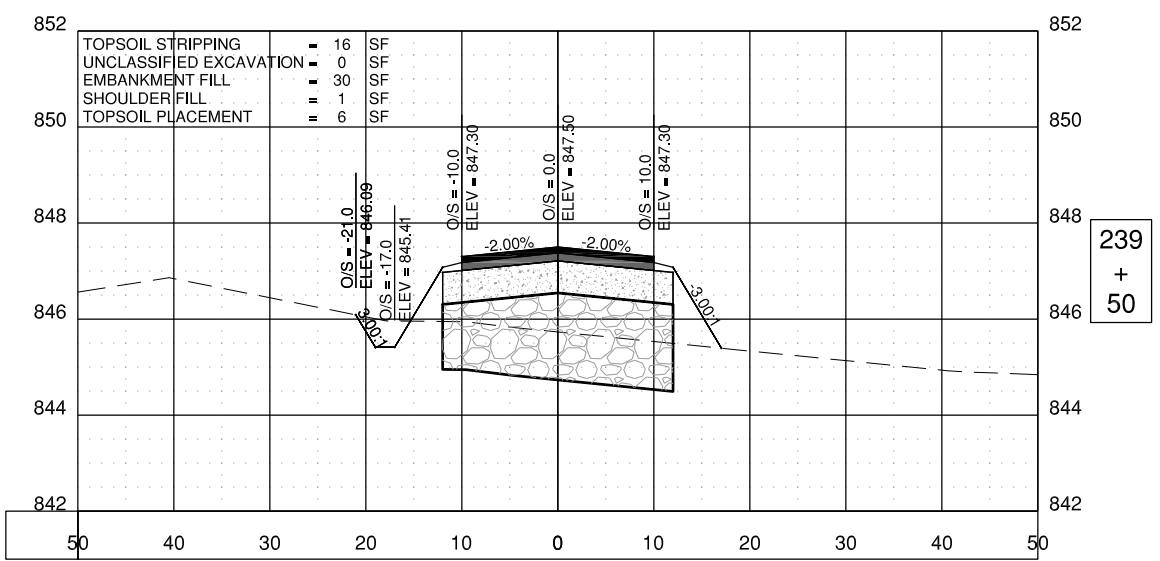
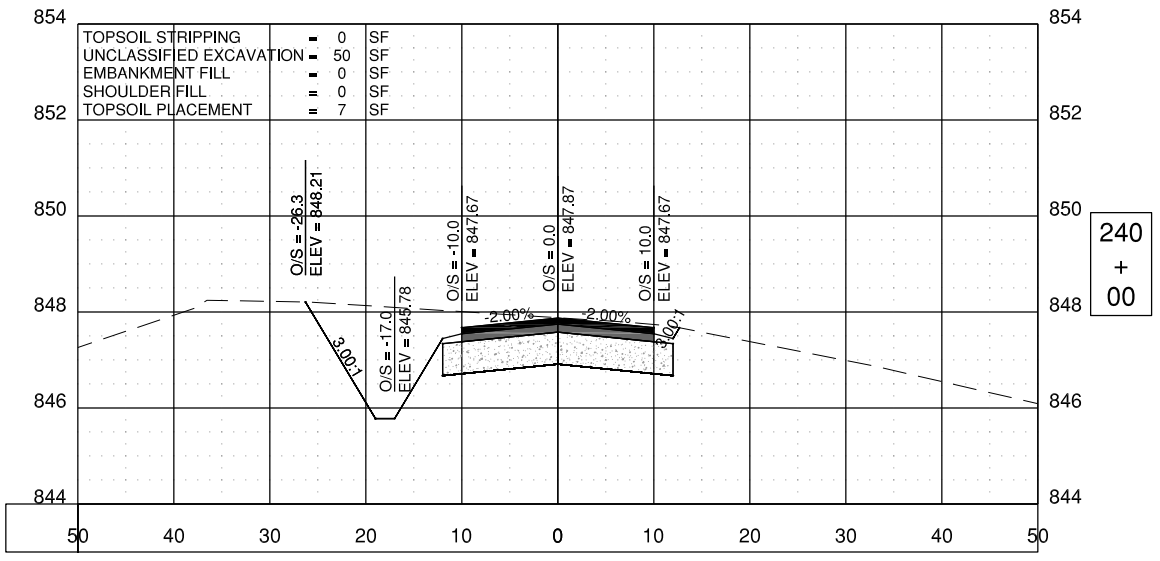
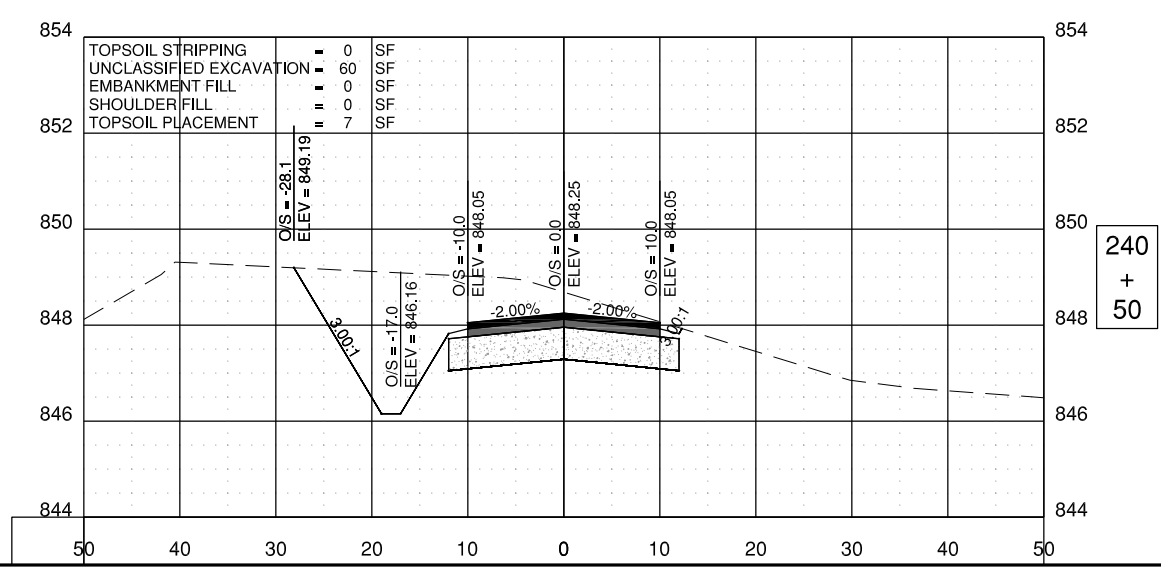
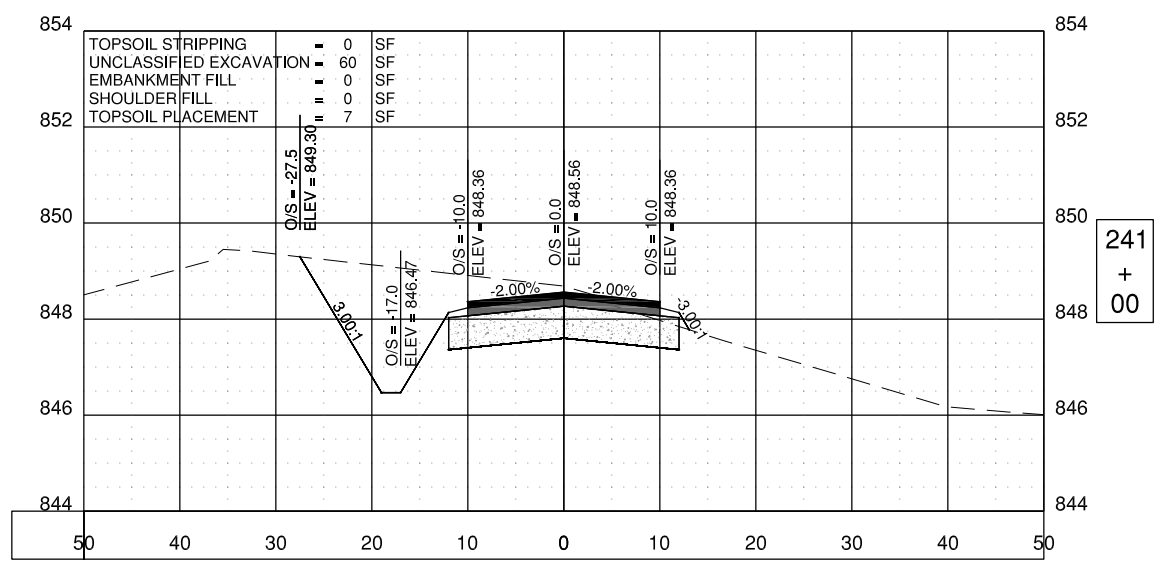
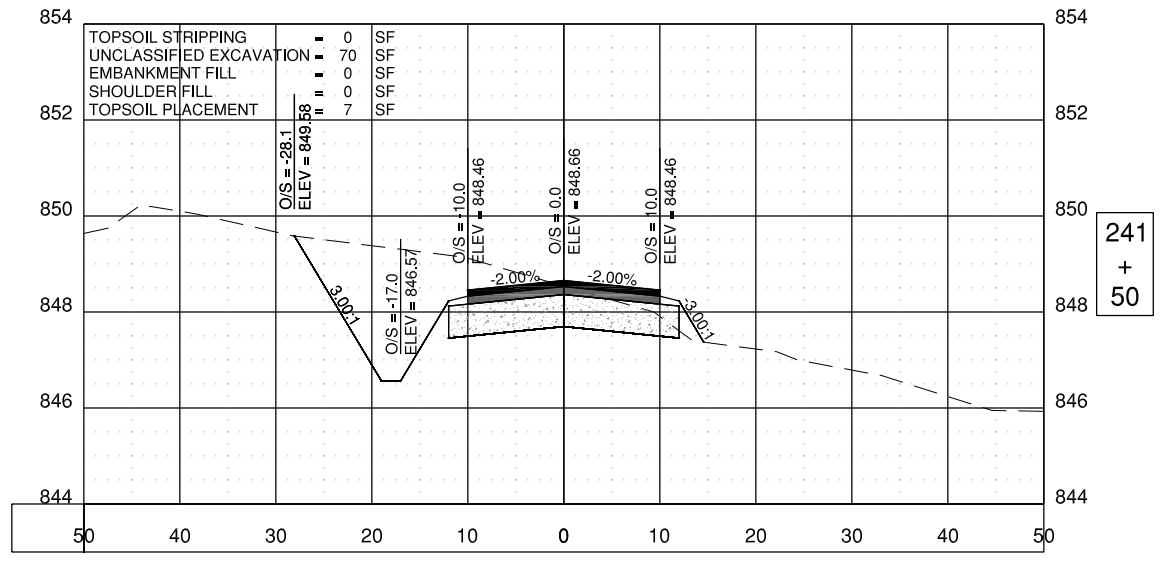
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 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD
 CROSS SECTIONS - SHEET 14**

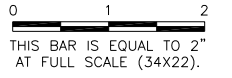


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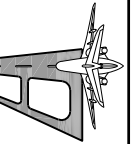


**FREEPORT-ALBERTUS AIRPORT
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 SOUTHWEST TERMINAL AREA ENTRANCE ROAD**

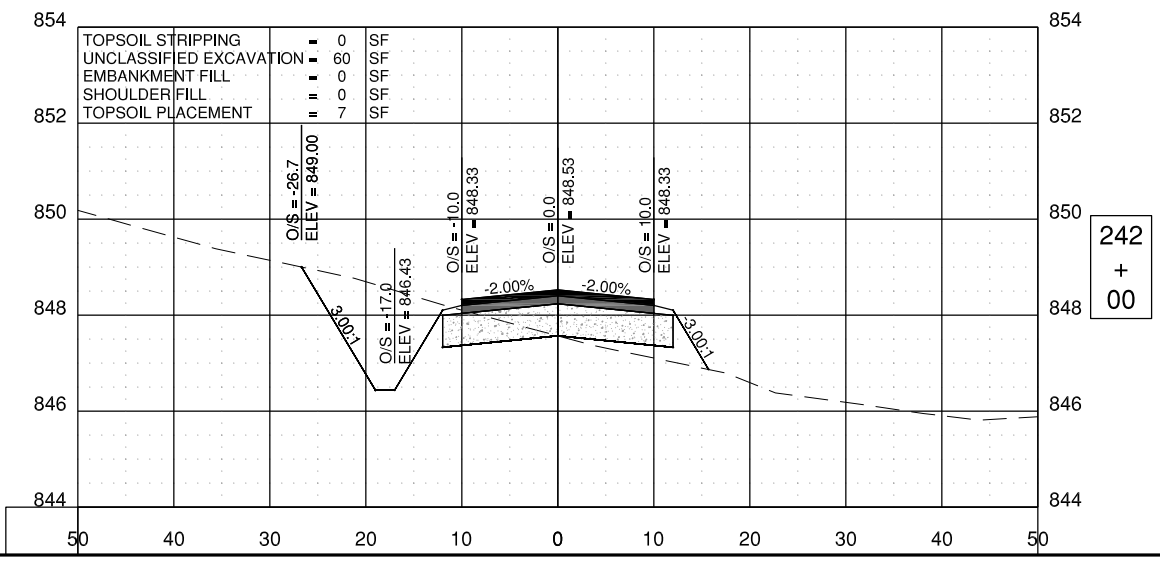
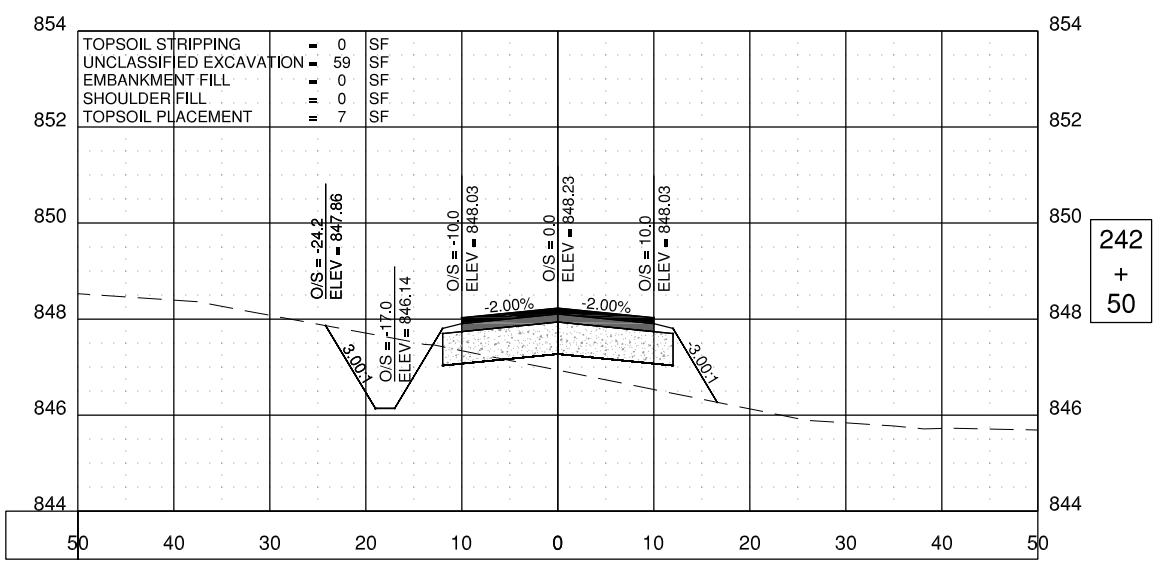
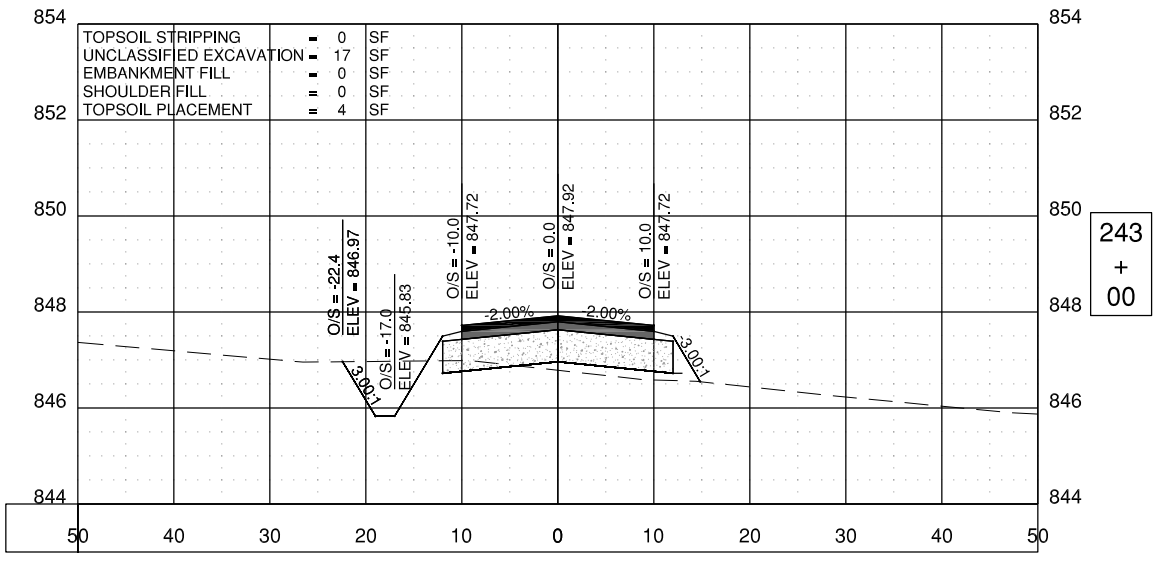
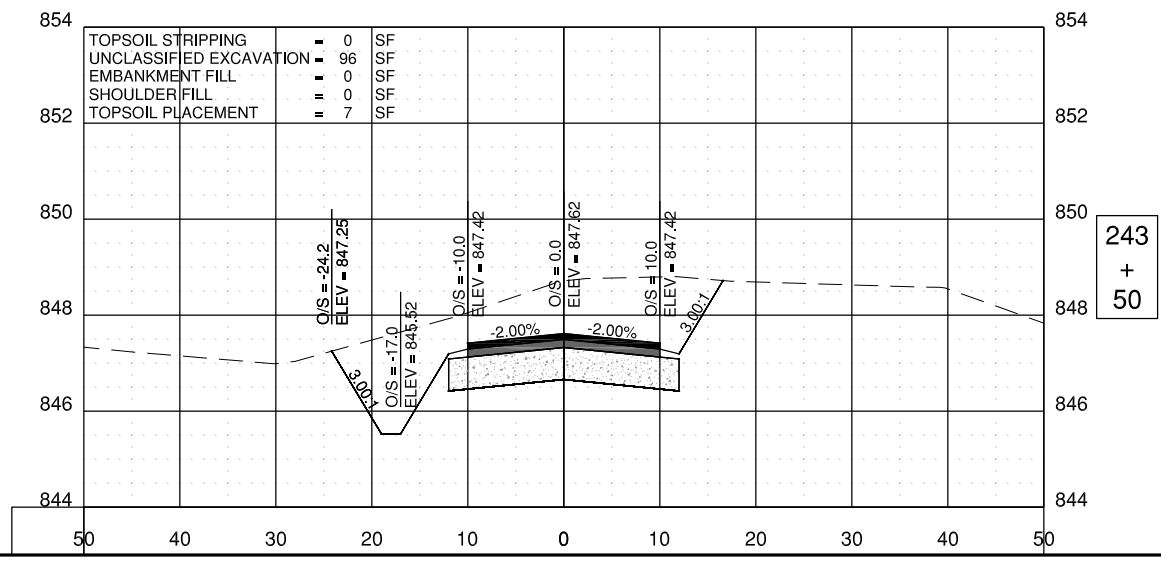
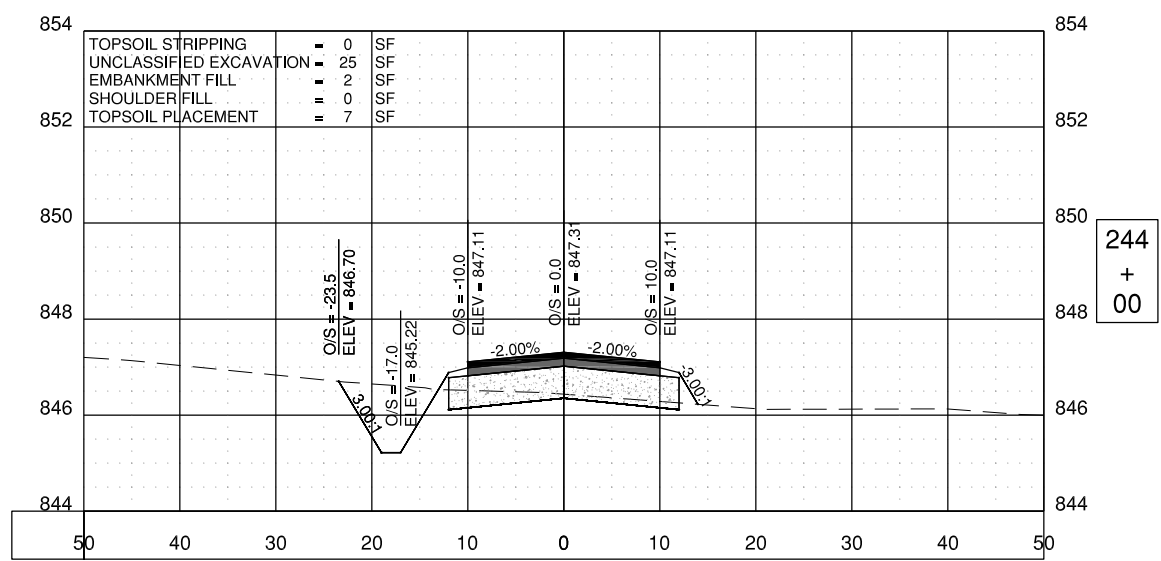
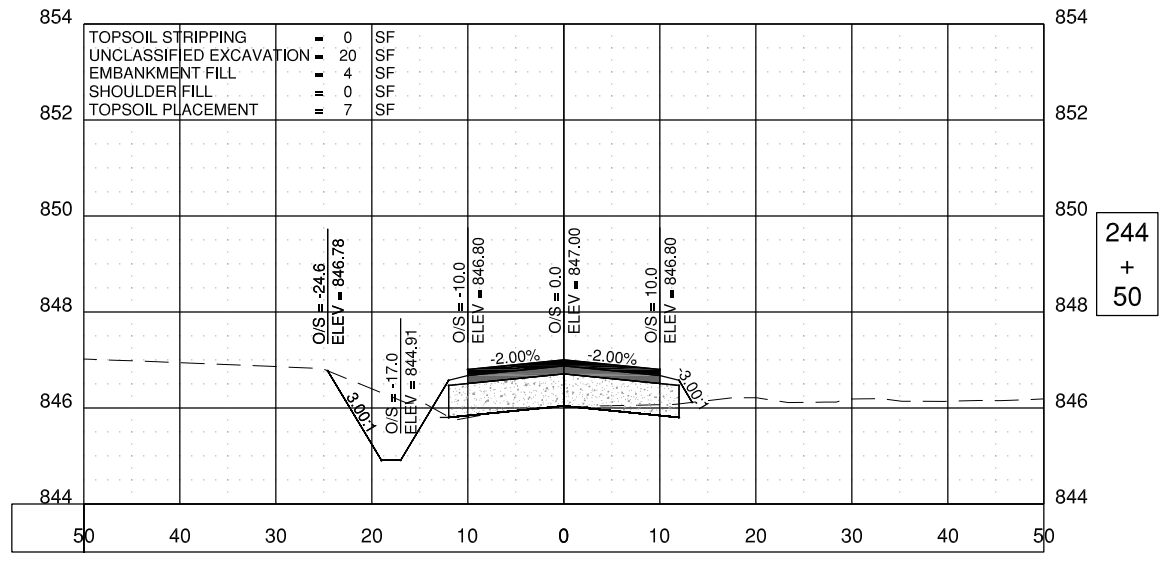
CROSS SECTIONS - SHEET 15



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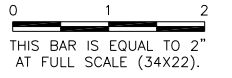
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DRAWN BY:	KWS
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



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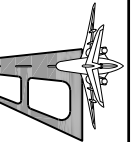


FREEPORT-ALBERTUS AIRPORT
 FREEPORT, ILLINOIS
 SOUTHWEST TERMINAL AREA ENTRANCE ROAD

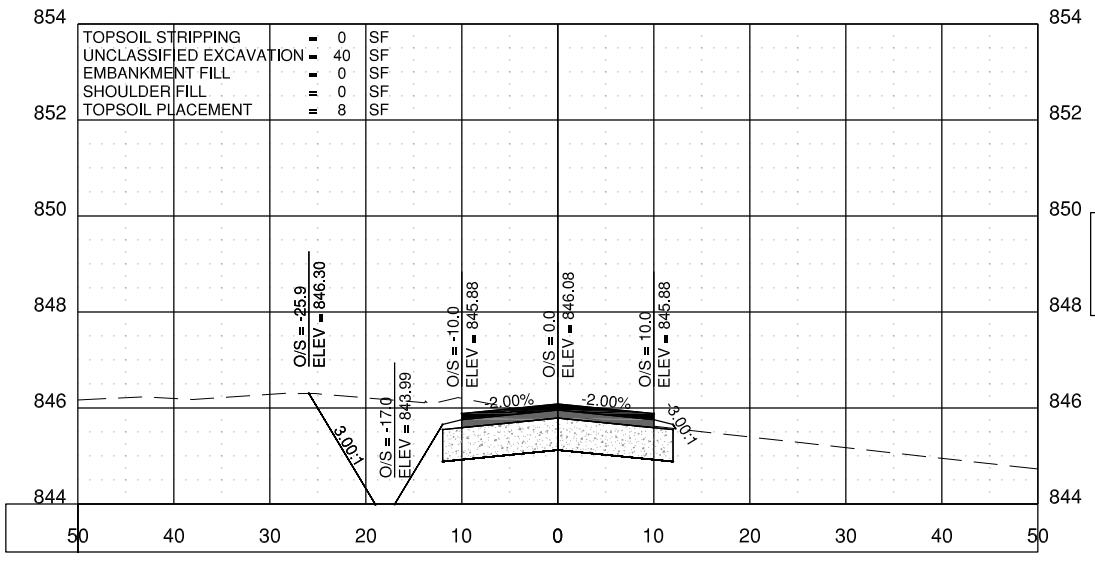
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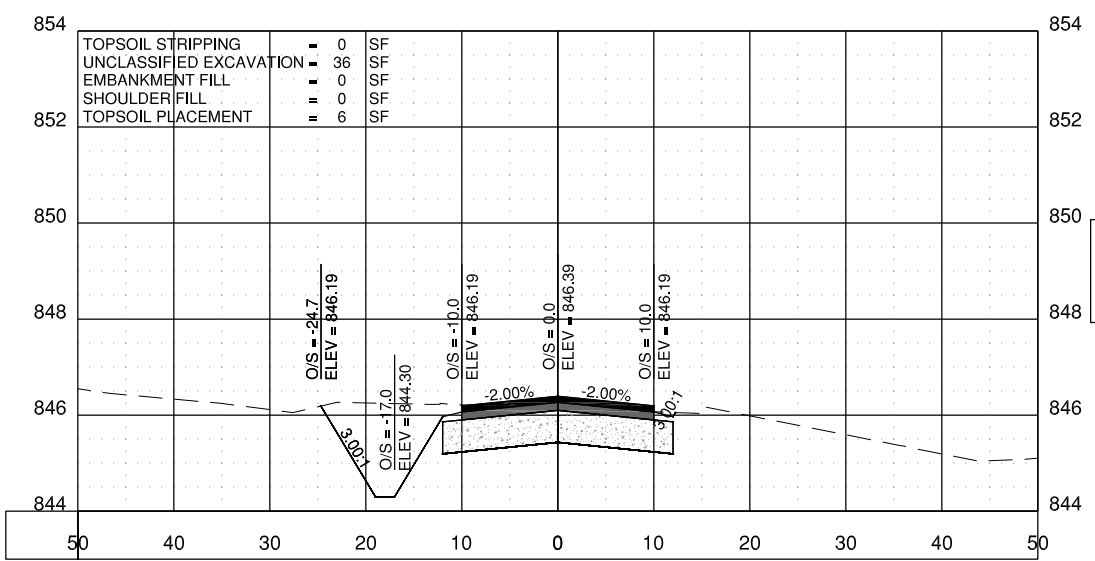
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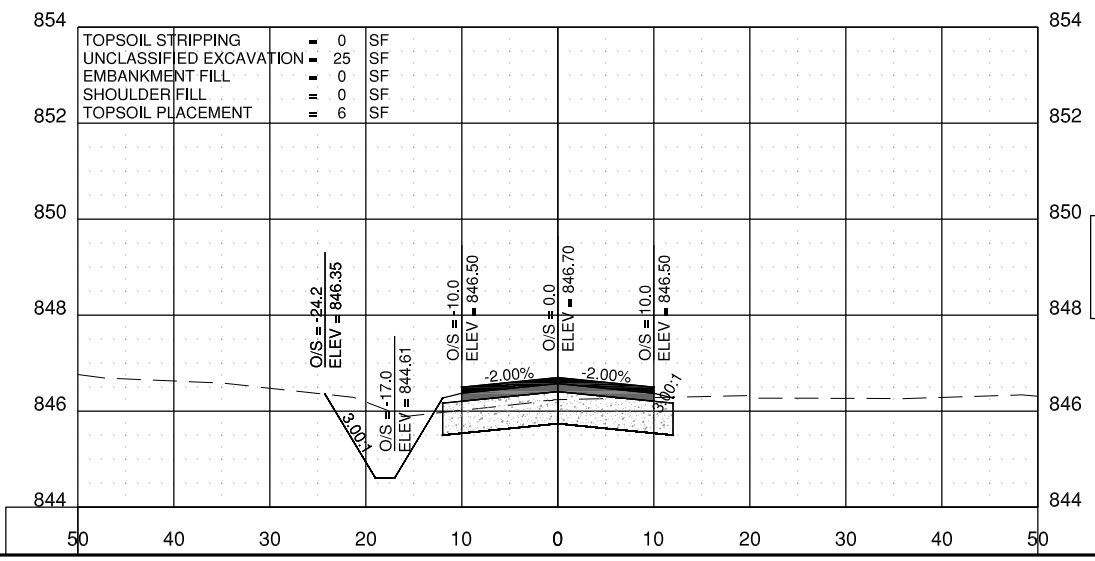
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DRAWN BY:	KWS
CHECKED BY:	MND
APPROVED BY:	DJK
DATE:	11-16-2018
JOB No:	17294-02



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