

KANKAKEE VALLEY AIRPORT AUTHORITY

KANKAKEE, ILLINOIS

FINAL

CONSTRUCTION PLANS

FOR

GREATER KANKAKEE AIRPORT

INDEX TO SHEETS

1. COVER SHEET
2. SUMMARY OF QUANTITIES AND MISCELLANEOUS NOTES/DETAILS
3. SITE PLAN AND PROJECT CONTROL PLAN
4. SEQUENCE OF CONSTRUCTION PER AC 150/5370-2G (LATEST EDITION)
5. SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
6. TYPICAL SECTIONS AND PAVEMENT REPAIR DETAILS
7. EXISTING CONDITIONS/PROPOSED REMOVALS
8. STAKING AND LANDSCAPING PLAN
9. DRAINAGE IMPROVEMENTS AND PAVEMENT MARKING PLAN
10. DRAINAGE DETAILS

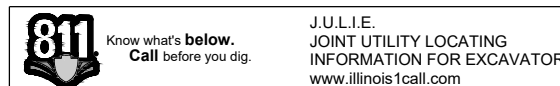
REHABILITATE SW QUADRANT TAXIWAY AND AIRFIELD PAVEMENT REPAIRS

ILLINOIS PROJECT: IKK-4672
 S.B.G. PROJECT: 3-17-SBGP-139/144



D. Kyle Peabody
 11-19-18
 EXPIRES 11-30-19

NOVEMBER 16, 2018



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

TAXIWAYS A, AA, B, C, D AND E

DESIGN AIRCRAFT APPROACH CATEGORY D
 TAXIWAY DESIGN GROUP II

RUNWAY 16/34 AND
 SOUTHWEST QUADRANT DESIGN INFORMATION

DESIGN AIRCRAFT APPROACH CATEGORY B
 TAXIWAY DESIGN GROUP II

KANKAKEE VALLEY AIRPORT AUTHORITY
 GREATER KANKAKEE AIRPORT

SECTION: 21 COUNTY: KANKAKEE
 RANGE: R 12 E TOWNSHIP: T 30 N

UNICOM RADIO FREQUENCY - 123.0

© Copyright CMT, Inc. 18075-02-00

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

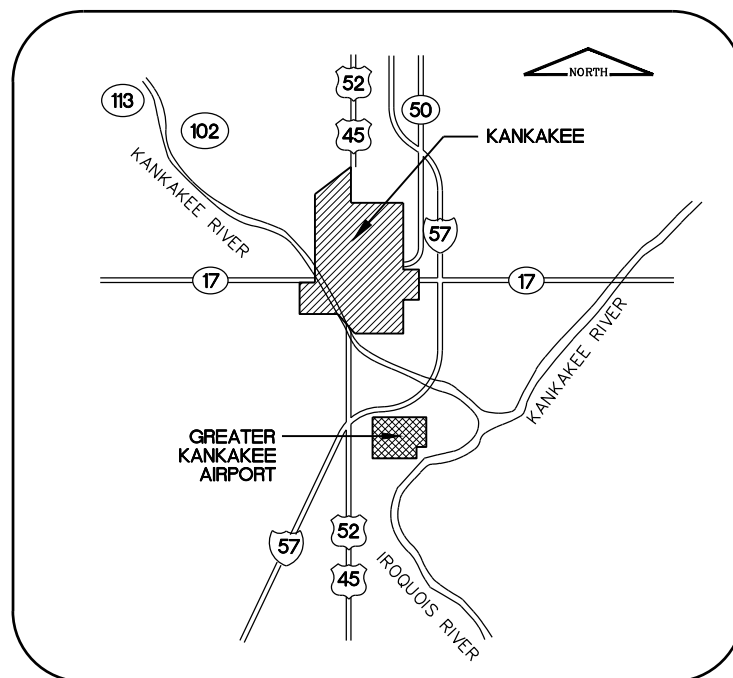
SUBMITTED BY *D. Kyle Peabody*
 D. KYLE PEABODY, P.E.

DATE *November 19,* 20*18*

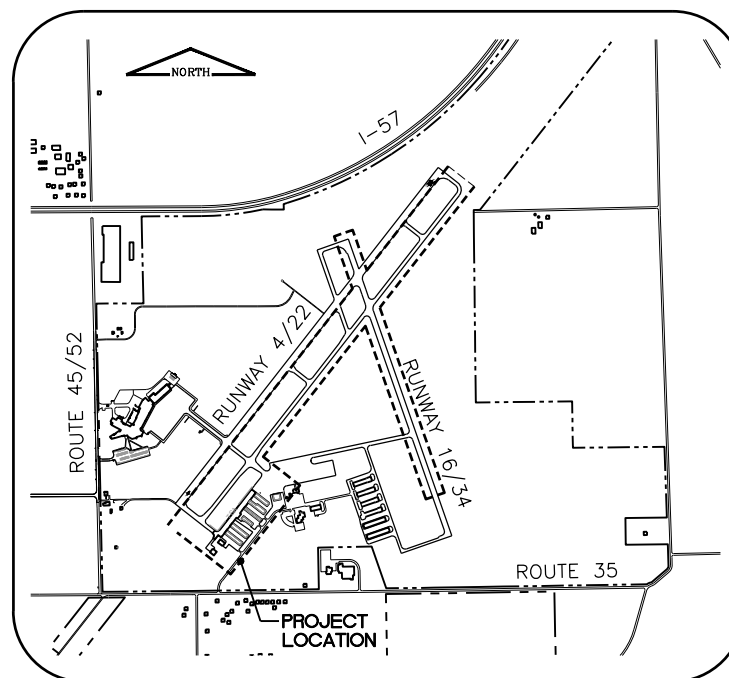
KANKAKEE VALLEY AIRPORT AUTHORITY

APPROVED *[Signature]*
 JEFF BENOIT, AIRPORT MANAGER

DATE *11-15-18* 20*18*



LOCATION MAP



SITE PLAN

6/17/2018

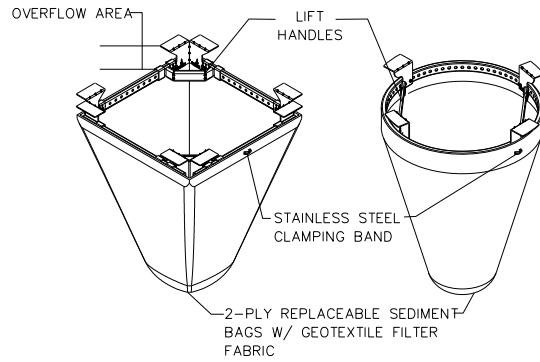
6/17/2018

UPDATE BY: Adam Moulton
LAYOUT: Layout1

DATE: Friday, December 14, 2018 12:49:34 PM
FILE: K:\kankakee\16175-02_Summary of Quantities\DrawSheets\16175-02 - Index And Quantities.dwg

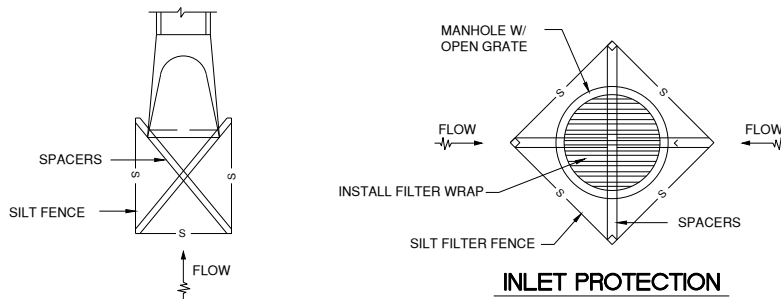
SUMMARY OF QUANTITIES

ITEM	DESCRIPTION	UNIT	FED./STATE/ LOCAL ESTIMATED QUANTITY	FED./LOCAL ESTIMATED QUANTITY	TOTAL ESTIMATED QUANTITY	RECORD QUANTITY
BASE BID						
AR150510	ENGINEER'S FIELD OFFICE	LS	1	0	1	
AR150520	MOBILIZATION	LS	1	0	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	600	150	750	
AR152540	SOIL STABILIZATION FABRIC	SY	1,330	300	1,630	
AR156514	DITCH CHECK	LF	160	0	160	
AR156520	INLET PROTECTION	EACH	7	0	7	
AR156532	EXCELSIOR BLANKET	SY	175	0	175	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	250	6,500	6,750	
AR201663	SAND MIX CRACK REPAIR	LF	240	1,000	1,240	
AR208515	POROUS GRANULAR EMBANKMENT	CY	280	100	380	
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	1,330	300	1,630	
AR401610	BITUMINOUS SURFACE COURSE	TON	1,325	0	1,325	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	6,765	0	6,765	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	1,305	0	1,305	
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	0	610	610	
AR401911	REMOVE & REPLACE BIT. SURFACE	SY	0	395	395	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	300	0	300	
AR602510	BITUMINOUS PRIME COAT	GAL	635	0	635	
AR603510	BITUMINOUS TACK COAT	GAL	1,550	0	1,550	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	3,530	200	3,880	
AR620525	PAVEMENT MARKING - BLACK BORDER	SF	3,530	200	3,880	
AR701515	15" RCP, CLASS IV	LF	360	0	360	
AR701518	18" RCP, CLASS IV	LF	101	0	101	
AR701900	REMOVE PIPE	LF	450	0	450	
AR751920	REPLACE INLET	EACH	1	0	1	
AR751980	RECONSTRUCT INLET	EACH	2	0	2	
AR752415	PRECAST REINFORCED CONC. FES 15"	EACH	4	0	4	
AR752418	PRECAST REINFORCED CONC. FES 18"	EACH	1	0	1	
AR752900	REMOVE END SECTION	EACH	5	0	5	
AR901510	SEEDING	ACRE	0.21	0	0.21	
AR905530	TOPSOILING	SY	1,000	0	1,000	
AR908515	HEAVY-DUTY HYDRAULIC MULCH	ACRE	0.18	0	0.18	
ADDITIVE ALTERNATE NO.1						
AS401910	REMOVE & REPLACE BIT. PAVEMENT	SY	0	250	250	
AS401911	REMOVE & REPLACE BIT. SURFACE	SY	0	250	250	
AS620520	PAVEMENT MARKING - WATERBORNE	SF	0	150	150	
AS620525	PAVEMENT MARKING - BLACK BORDER	SF	0	150	150	



INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

NOT TO SCALE
FOR ALL RECTANGULAR AND CIRCULAR INLETS



INLET PROTECTION (END SECTION)

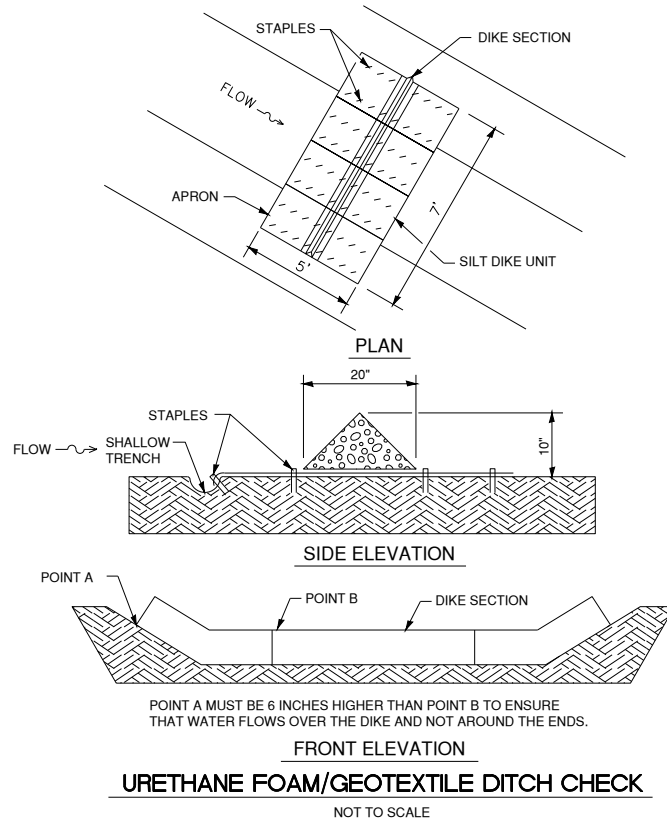
NOT TO SCALE
IDOT STANDARD 280001-07

INLET PROTECTION (INLET/MANHOLES - IN TURF)

NOT TO SCALE
IDOT STANDARD 280001-07

NOTES FOR INLET PROTECTION DETAILS

- FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2016.
- FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
- COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



URETHANE FOAM/GEOTEXTILE DITCH CHECK

NOT TO SCALE

STORM WATER POLLUTION PREVENTION NOTES

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING AN OVERLAY, PAVEMENT PATCHING, PAVEMENT MILLING, STORM SEWER REPLACEMENT AND CRACK REPAIR AT GREATER KANKAKEE AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

- UNCLASSIFIED EXCAVATION

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWERS THAT OUTLET INTO THE KANKAKEE RIVER.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
- EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
- AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
- THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

IL CONTRACT: **KA048**
IL LETTING ITEM: **23A**
IL PROJECT: **IKK-4672**
S.B.G. PROJECT: **3-17-SBGP-139/144**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2"
AT FULL SCALE (34X22).

**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE SW QUADRANT TAXIWAY AND
AIRFIELD PAVEMENT REPAIRS**

**SUMMARY OF QUANTITIES AND
MISCELLANEOUS NOTES/DETAILS**

© Copyright CMT, Inc.

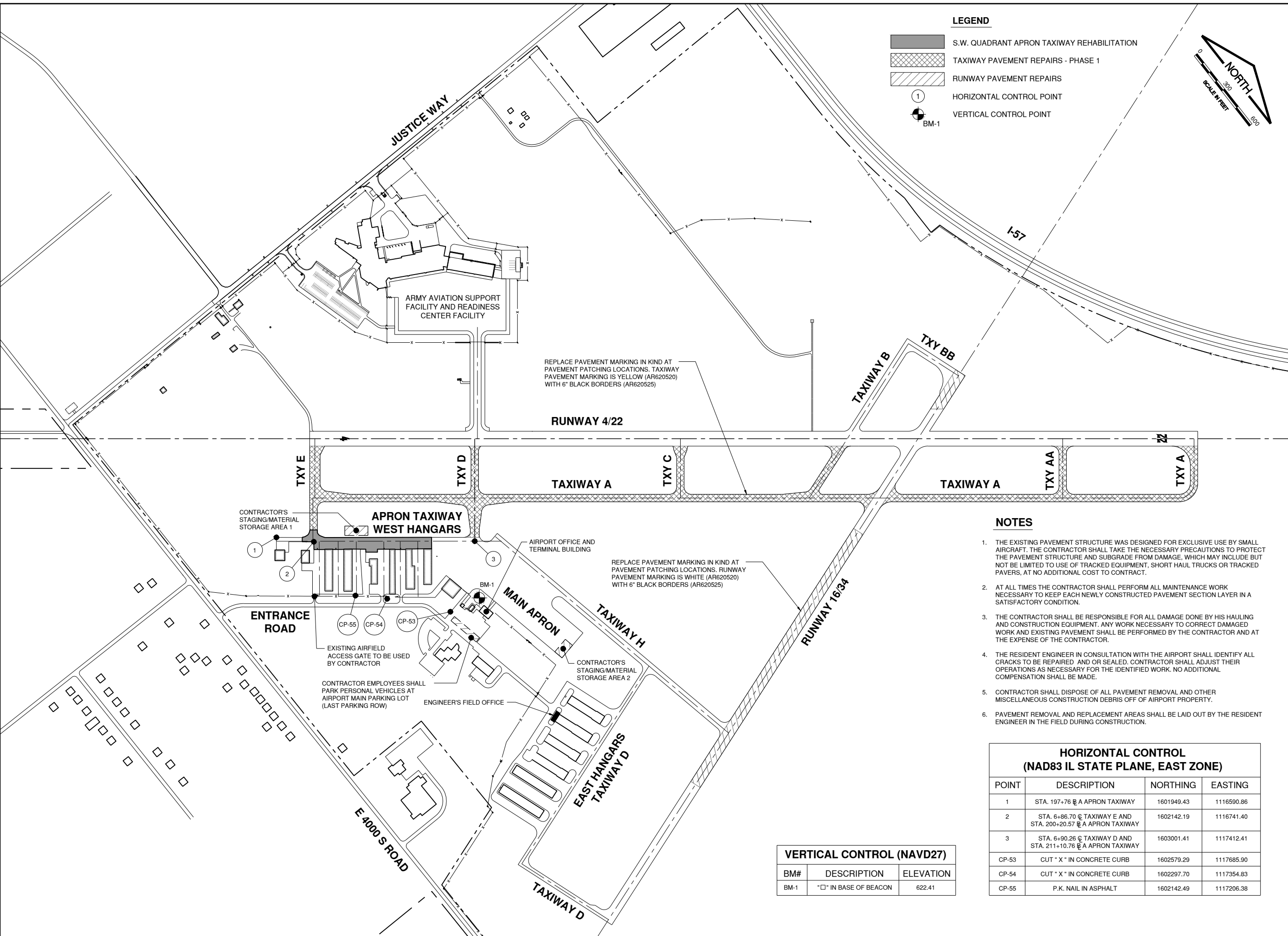
CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

KANKAKEE VALLEY
KVA
AIRPORT AUTHORITY

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ABM
DATE:	11/16/2018
JOB No:	18007502.00

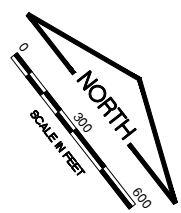
FINAL

DATE: Friday, December 14, 2018 12:49:46 PM
 FILE: K:\kankakee\3175-02_SWO\Draw\DrawSheets\317502_Site_Control_Plan.dwg
 UPDATE BY: Adam Moulton
 LAYOUT: Layout1
 6/16/2017
 6/16/2017



LEGEND

- S.W. QUADRANT APRON TAXIWAY REHABILITATION
- TAXIWAY PAVEMENT REPAIRS - PHASE 1
- RUNWAY PAVEMENT REPAIRS
- HORIZONTAL CONTROL POINT
- VERTICAL CONTROL POINT



REPLACE PAVEMENT MARKING IN KIND AT PAVEMENT PATCHING LOCATIONS. TAXIWAY PAVEMENT MARKING IS YELLOW (AR620520) WITH 6" BLACK BORDERS (AR620525)

REPLACE PAVEMENT MARKING IN KIND AT PAVEMENT PATCHING LOCATIONS. RUNWAY PAVEMENT MARKING IS WHITE (AR620520) WITH 6" BLACK BORDERS (AR620525)

NOTES

1. THE EXISTING PAVEMENT STRUCTURE WAS DESIGNED FOR EXCLUSIVE USE BY SMALL AIRCRAFT. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO CONTRACTOR.
2. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
4. THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT SHALL IDENTIFY ALL CRACKS TO BE REPAIRED AND OR SEALED. CONTRACTOR SHALL ADJUST THEIR OPERATIONS AS NECESSARY FOR THE IDENTIFIED WORK. NO ADDITIONAL COMPENSATION SHALL BE MADE.
5. CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.
6. PAVEMENT REMOVAL AND REPLACEMENT AREAS SHALL BE LAID OUT BY THE RESIDENT ENGINEER IN THE FIELD DURING CONSTRUCTION.

HORIZONTAL CONTROL (NAD83 IL STATE PLANE, EAST ZONE)

POINT	DESCRIPTION	NORTHING	EASTING
1	STA. 197+76 @ A APRON TAXIWAY	1601949.43	1116590.86
2	STA. 6+86.70 @ TAXIWAY E AND STA. 200+20.57 @ A APRON TAXIWAY	1602142.19	1116741.40
3	STA. 6+90.26 @ TAXIWAY D AND STA. 211+10.76 @ A APRON TAXIWAY	1603001.41	1117412.41
CP-53	CUT " X " IN CONCRETE CURB	1602579.29	1117685.90
CP-54	CUT " X " IN CONCRETE CURB	1602297.70	1117354.83
CP-55	P.K. NAIL IN ASPHALT	1602142.49	1117206.38

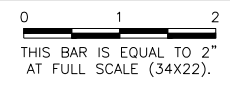
VERTICAL CONTROL (NAVD27)

BM#	DESCRIPTION	ELEVATION
BM-1	"□" IN BASE OF BEACON	622.41

IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE SW QUADRANT TAXIWAY AND
 AIRFIELD PAVEMENT REPAIRS**

SITE PLAN AND PROJECT CONTROL PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY





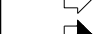



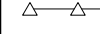







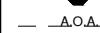
 AIRPORT AUTHORITY

DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: ARM
 APPROVED BY: DKP
 DATE: 11/16/2018
 JOB No: 18007502.00

FINAL

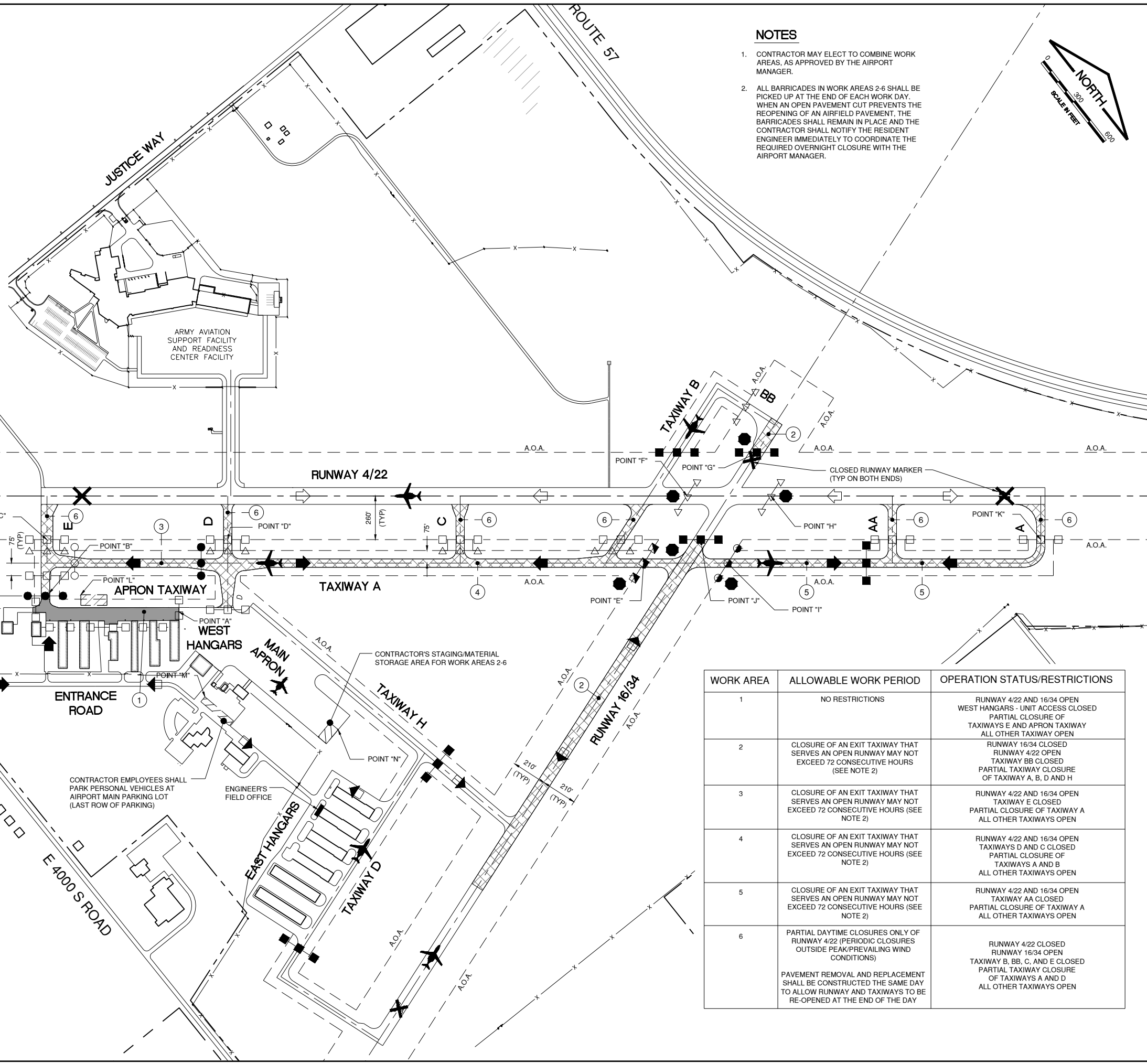
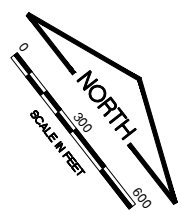
DATE: Friday, December 14, 2018 12:49:56 PM
 FILE: K:\kankakee\1501570-2G_SWO\Drawings\DrawSheets\1501570-2G_Sequence.dwg
 UPDATE BY: Adam Moulton
 LAYOUT: Layout1
 8/11/2018
 8/11/2018

LEGEND

-  EXISTING PAVEMENT MILL AND OVERLAY
-  NEW AIRFIELD PAVEMENT CRACK REPAIRS
-  WORK AREA NUMBER
-  CONTRACTOR'S HAUL ROUTE
-  CONTRACTOR'S HAUL ROUTE - PHASE 6 ONLY
-  CONTRACTOR'S HAUL ROUTE - PHASE 2 ONLY
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - AREAS 1, 3, 4 AND 5
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - PHASE 4 ONLY
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - AREA 6 ONLY
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - AREA 3
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - AREAS 5 AND 6
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - AREA 2 ONLY
-  TEMPORARY IDOT TYPE 2 BARRICADE OR DRUM WITH RED FLASHING LIGHT - AREAS 2 AND 4
-  AIRCRAFT MOVEMENT AREA
-  STOP SIGN
-  AIRCRAFT OPERATIONS AREA (A.O.A.)
-  CLOSED RUNWAY MARKER

NOTES

1. CONTRACTOR MAY ELECT TO COMBINE WORK AREAS, AS APPROVED BY THE AIRPORT MANAGER.
2. ALL BARRICADES IN WORK AREAS 2-6 SHALL BE PICKED UP AT THE END OF EACH WORK DAY. WHEN AN OPEN PAVEMENT CUT PREVENTS THE REOPENING OF AN AIRFIELD PAVEMENT, THE BARRICADES SHALL REMAIN IN PLACE AND THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IMMEDIATELY TO COORDINATE THE REQUIRED OVERNIGHT CLOSURE WITH THE AIRPORT MANAGER.



SUGGESTED SEQUENCE OF CONSTRUCTION

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- WORK AREA 1
 - PLACE BARRICADES
 - REMOVE HMA PAVEMENT
 - REPLACE STORM SEWER
 - REPAIR CRACKS, PATCH PAVEMENT AND PLACE NEW HMA PAVEMENT
 - INSTALL PAVEMENT MARKINGS
 - RESTORE STAGING AREA 1
 - CLEAN PAVEMENTS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
 - REMOVE BARRICADES
- WORK AREAS 2 THRU 6
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT REQUIRED PAVEMENT CLOSURES FOR WORK AREAS
 - PLACE BARRICADES
 - REVIEW CRACKS IDENTIFIED FOR REPAIR BY RESIDENT ENGINEER.
 - REPLACE PAVEMENT AND REPAIR CRACKS WITHIN WORK AREA
 - COORDINATE PAVEMENT CLOSURES WITH RESIDENT ENGINEER AND AIRPORT FOR NEXT WORK AREA.
 - CLEAN PAVEMENTS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
 - RELOCATE BARRICADES FOR NEXT WORK AREA.
- INSTALL PAVEMENT MARKINGS.
- RESTORE STAGING AREA 2-6

CONTRACTOR EMPLOYEES SHALL PARK PERSONAL VEHICLES AT AIRPORT MAIN PARKING LOT (LAST ROW OF PARKING)

ENGINEER'S FIELD OFFICE

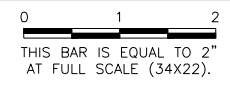
CONTRACTOR'S STAGING/MATERIAL STORAGE AREA FOR WORK AREAS 2-6

WORK AREA	ALLOWABLE WORK PERIOD	OPERATION STATUS/RESTRICTIONS
1	NO RESTRICTIONS	RUNWAY 4/22 AND 16/34 OPEN WEST HANGARS - UNIT ACCESS CLOSED PARTIAL CLOSURE OF TAXIWAYS E AND APRON TAXIWAY ALL OTHER TAXIWAY OPEN
2	CLOSURE OF AN EXIT TAXIWAY THAT SERVES AN OPEN RUNWAY MAY NOT EXCEED 72 CONSECUTIVE HOURS (SEE NOTE 2)	RUNWAY 16/34 CLOSED RUNWAY 4/22 OPEN TAXIWAY BB CLOSED PARTIAL TAXIWAY CLOSURE OF TAXIWAY A, B, D AND H
3	CLOSURE OF AN EXIT TAXIWAY THAT SERVES AN OPEN RUNWAY MAY NOT EXCEED 72 CONSECUTIVE HOURS (SEE NOTE 2)	RUNWAY 4/22 AND 16/34 OPEN TAXIWAY E CLOSED PARTIAL CLOSURE OF TAXIWAY A ALL OTHER TAXIWAYS OPEN
4	CLOSURE OF AN EXIT TAXIWAY THAT SERVES AN OPEN RUNWAY MAY NOT EXCEED 72 CONSECUTIVE HOURS (SEE NOTE 2)	RUNWAY 4/22 AND 16/34 OPEN TAXIWAYS D AND C CLOSED PARTIAL CLOSURE OF TAXIWAYS A AND B ALL OTHER TAXIWAYS OPEN
5	CLOSURE OF AN EXIT TAXIWAY THAT SERVES AN OPEN RUNWAY MAY NOT EXCEED 72 CONSECUTIVE HOURS (SEE NOTE 2)	RUNWAY 4/22 AND 16/34 OPEN TAXIWAY AA CLOSED PARTIAL CLOSURE OF TAXIWAY A ALL OTHER TAXIWAYS OPEN
6	PARTIAL DAYTIME CLOSURES ONLY OF RUNWAY 4/22 (PERIODIC CLOSURES OUTSIDE PEAK/PREVAILING WIND CONDITIONS) PAVEMENT REMOVAL AND REPLACEMENT SHALL BE CONSTRUCTED THE SAME DAY TO ALLOW RUNWAY AND TAXIWAYS TO BE RE-OPENED AT THE END OF THE DAY	RUNWAY 4/22 CLOSED RUNWAY 16/34 OPEN TAXIWAY B, BB, C, AND E CLOSED PARTIAL TAXIWAY CLOSURE OF TAXIWAYS A AND D ALL OTHER TAXIWAYS OPEN


IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**


SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE SW QUADRANT TAXIWAY AND AIRFIELD PAVEMENT REPAIRS
SEQUENCE OF CONSTRUCTION PER AC 150/5370-2G (LATEST EDITION)

 **CMT**
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: ARM
 APPROVED BY: DKP
 DATE: 11/16/2018
 JOB No: 18007502.00

GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES WITH TWO ORANGE FLAGS (20" x 20") ON EACH BARRICADE SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL SUPPLY AND USE AS DIRECTED BY THE AIRPORT, REFLECTIVE LOW PROFILE TYPE BARRICADES. ALL BARRICADES SHALL BE PLACED OUTSIDE OF ACTIVE SAFETY AREAS.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER FOURTEEN (14) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.

- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS / TAXILANES AND THE RUNWAYS AIRCRAFT OPERATIONS AREA.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTORS WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFECIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.

BARRICADE NOTES

- FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OR SOLAR POWER OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 10' SPACING CENTER TO CENTER BETWEEN RED LIGHTS ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.

DESIGN AIRCRAFT APPROACH CATEGORY: B AND D

DESIGN AIRPORT GROUP: II
 RUNWAY 4/22 SAFETY AREA WIDTH: 500'
 RUNWAY 16/34 SAFETY AREA WIDTH: 150'
 TAXIWAY CENTERLINE TO OBJECT SEPARATION: 65.5'
 TAXILANE CENTERLINE TO OBJECT SEPARATION: 57.5'

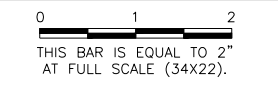
MAXIMUM ANTICIPATED HEIGHT OF EQUIPMENT - 25'

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	ELEVATION
A	RUNWAY 4/22	41°03'59.45"	87°51'04.63"	619
B	RUNWAY 4/22	41°03'53.39"	87°51'11.28"	621
C	RUNWAY 4/22	41°03'55.52"	87°51'14.62"	623
D	RUNWAY 4/22	41°04'03.86"	87°51'05.90"	622
E	RUNWAY 16/34	41°04'22.22"	87°50'44.40"	621
F	RUNWAY 16/34	41°04'26.66"	87°50'46.41"	621
G	RUNWAY 4/22	41°04'31.44"	87°50'45.60"	621
H	RUNWAY 16/34	41°04'30.54"	87°50'42.37"	621
I	RUNWAY 16/34	41°04'26.09"	87°50'40.36"	620
J	RUNWAY 4/22	41°04'25.71"	87°50'43.08"	621
K	RUNWAY 4/22	41°04'41.41"	87°50'26.68"	629
L	RUNWAY 4/22	41°03'55.08"	87°51'09.64"	620
M	RUNWAY 4/22	41°03'57.08"	87°50'57.65"	621
N	RUNWAY 16/34	41°04'02.04"	87°50'48.99"	622

IL. CONTRACT: **KA048**
 IL. LETTING ITEM: **23A**
 IL. PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE SW QUADRANT TAXIWAY AND
 AIRFIELD PAVEMENT REPAIRS**

**SEQUENCE OF CONSTRUCTION
 GENERAL NOTES AND DETAILS**

CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A FULL TIME CROSSING GUARD IN RADIO CONTACT WITH AIR TRAFFIC SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (**\$500 PER OCCURRENCE**) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

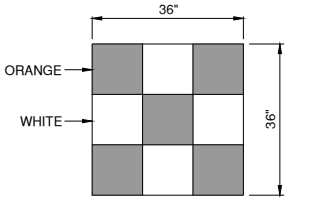
LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY'S AIRCRAFT OPERATIONS AREA (AOA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED. THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

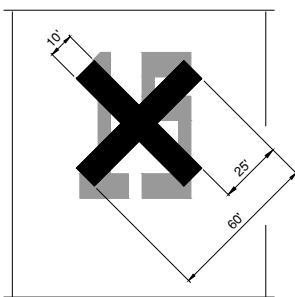
TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

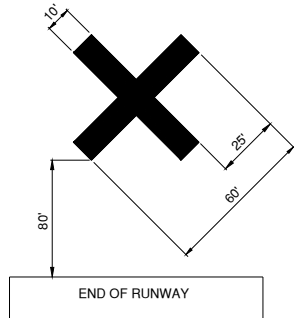


CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.



ON PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE



OFF PAVEMENT TEMPORARY CLOSED RUNWAY MARKER DETAIL
NOT TO SCALE

CLOSED RUNWAY MARKER DETAIL NOTES

- CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
- MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
- COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.

© Copyright CMT, Inc.

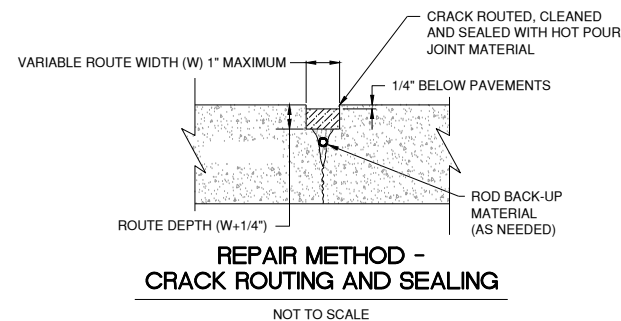
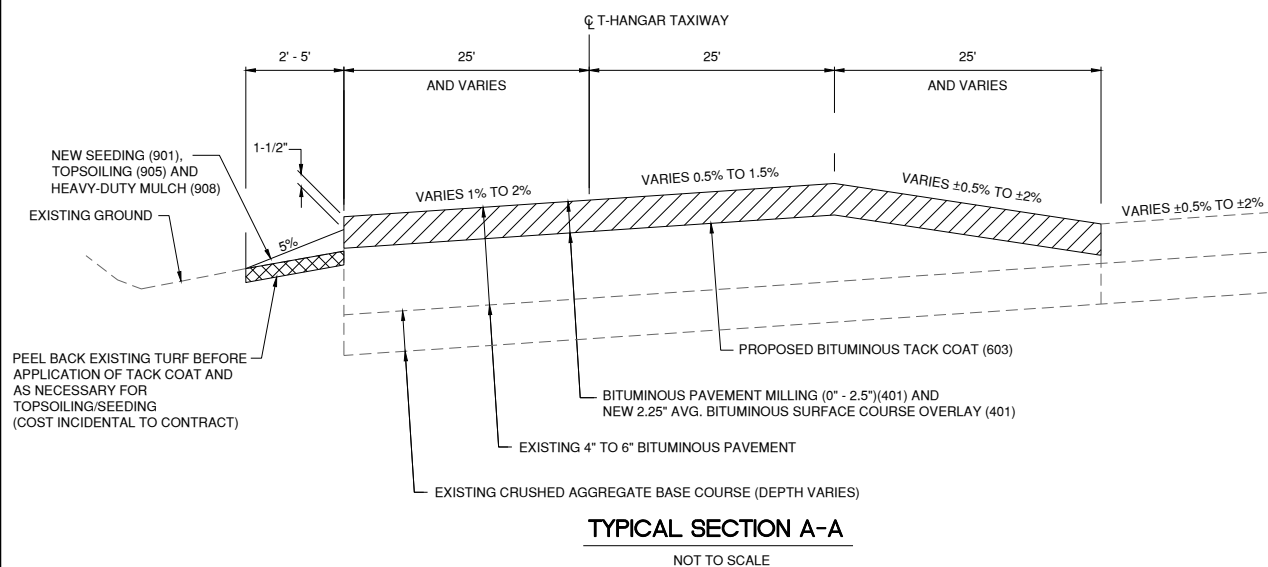
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY
KVA
 AIRPORT AUTHORITY

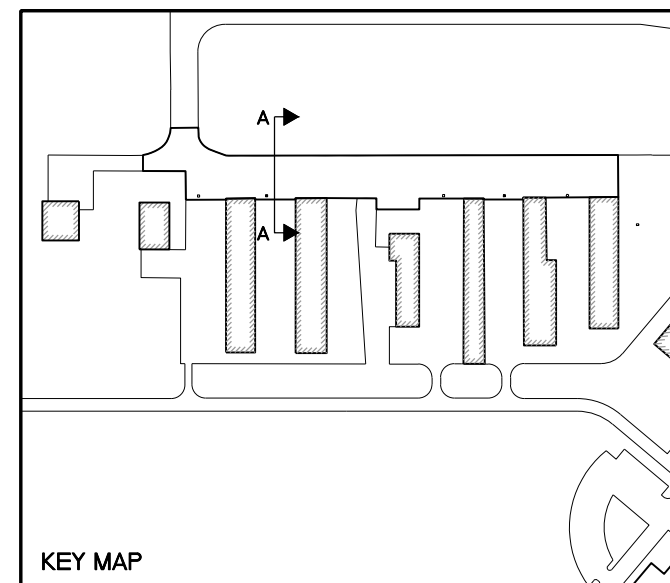
DESIGN BY: ARM
 DRAWN BY: JRO
 CHECKED BY: ARM
 APPROVED BY: DKP
 DATE: 11/16/2018
 JOB No: 18007502.00

DATE: Friday, December 14, 2018 12:50:04 PM
 FILE: K:\cmt\design\181215-02_SWO\Drawings\Plan\Drawn Sheets\021502 - Sequence Notes and Details.dwg
 UPDATE BY: Adam Meulden
 LAYOUT: Layout1

DATE: Friday, December 14, 2018 12:50:11 PM
 FILE: K:\kankakee\1507502_SWQadant\1507502_Typical Sections.dwg
 UPDATE BY: Adam Moulton
 LAYOUT: Layout1
 8d1c0601
 8d1c0601
 8d1c0601

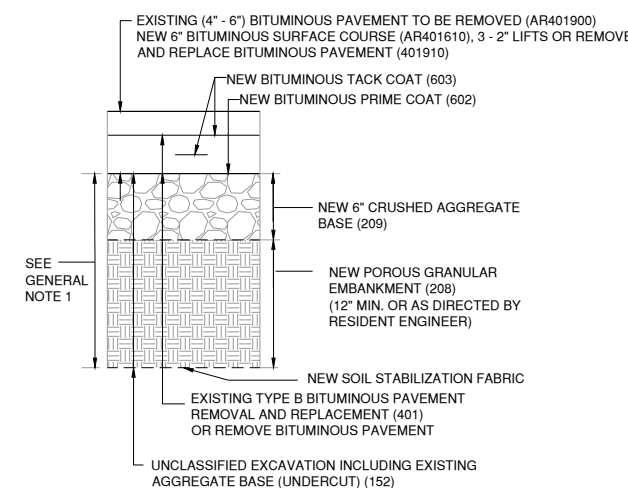
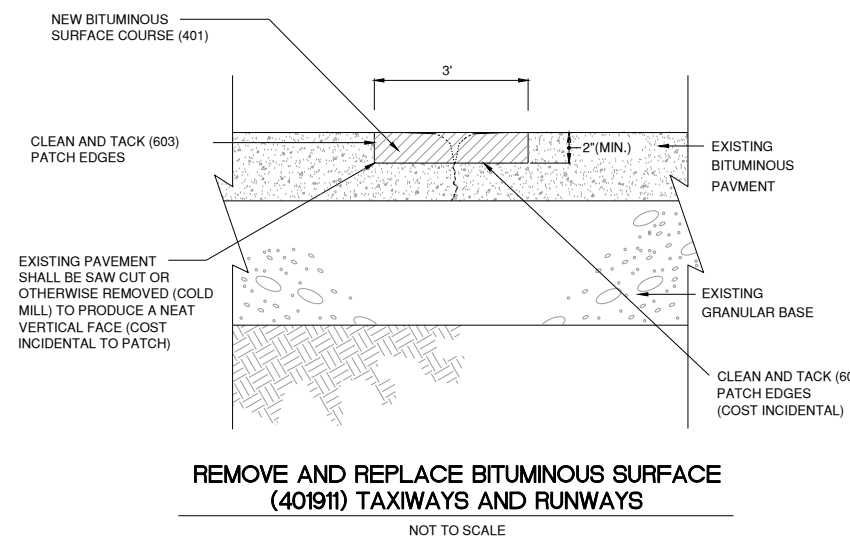
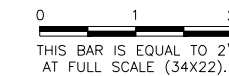


- NOTES**
1. THE CRACK ROUTING DIMENSIONS SHALL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT MATERIAL FOR SEALANT MATERIAL.
 2. CRACKS TO BE ROUTED, CLEANED AND SEALED AT THE LOCATIONS DETERMINED BY THE ENGINEER.

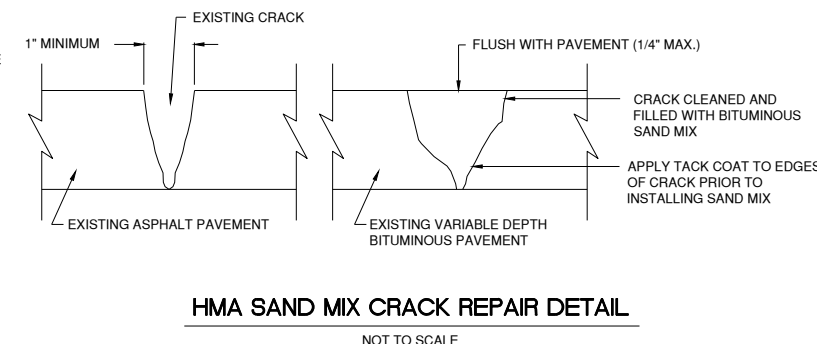
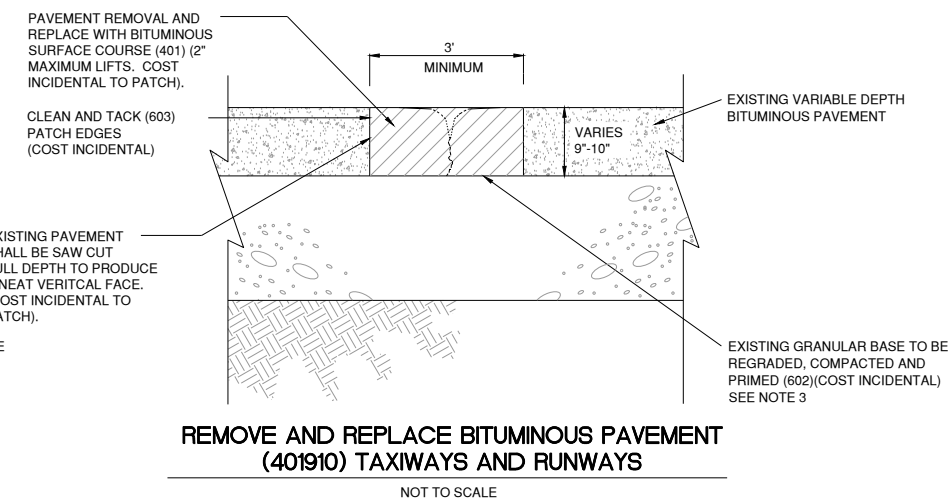
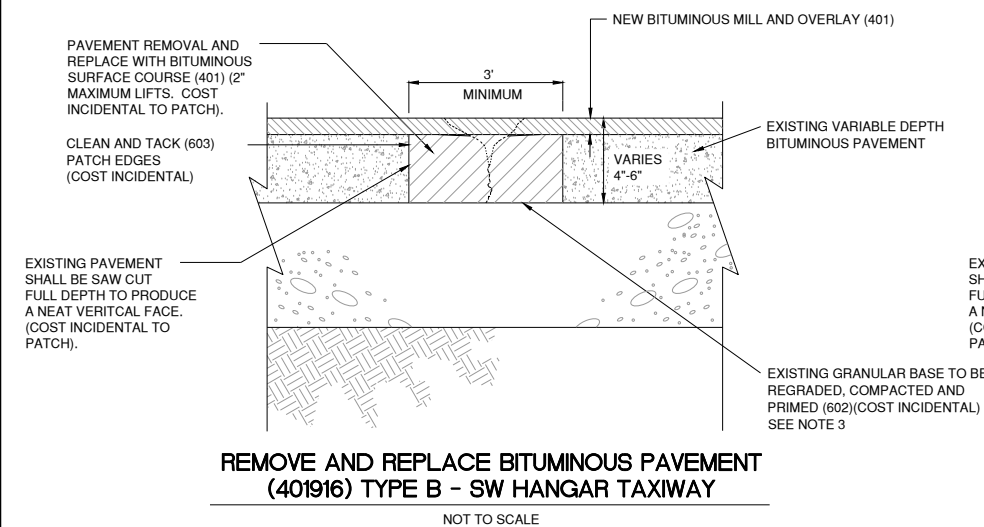


IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

REVISIONS		
NUMBER	BY	DATE



- GENERAL NOTES:**
1. LOCATIONS FOR FURNISHING AND PLACING POROUS GRANULAR EMBANKMENT SHALL BE AS THE FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION. THIS MATERIAL IS INTENDED TO REPAIR SOFT SUBGRADE AS DIRECTED BY THE RESIDENT ENGINEER. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. EXCAVATION OF THE SOFT SUBGRADE SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. (REFER TO STABILIZATION TYPICAL SECTION)



- NOTES**
1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
 2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1\"/>

- PAVEMENT REMOVAL AND REPLACEMENT NOTES**
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS, AIRPORT PERSONNEL, AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
 2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRANSVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
 3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE COURSE SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES. SEE STABILIZATION TYPICAL SECTION FOR DETAIL.

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE SW QUADRANT TAXIWAY AND
 AIRFIELD PAVEMENT REPAIRS**

**TYPICAL SECTIONS AND
 PAVEMENT REPAIR DETAILS**

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613
KANKAKEE VALLEY
AVAA
 AIRPORT AUTHORITY

DESIGN BY:	ABM/ARM
DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	11/16/2018
JOB No:	18007502.00

FINAL

DATE: Friday, December 14, 2018 12:50:23 PM
 FILE: K:\kankakee\1807502_SWO\Drawings\1807502_Existing Conditions.dwg
 UPDATE BY: Adam Moulton
 LAYOUT: Layout1
 Search: Boring.dwg
 Legend: Core Summary.dwg
 Express: Base.dwg

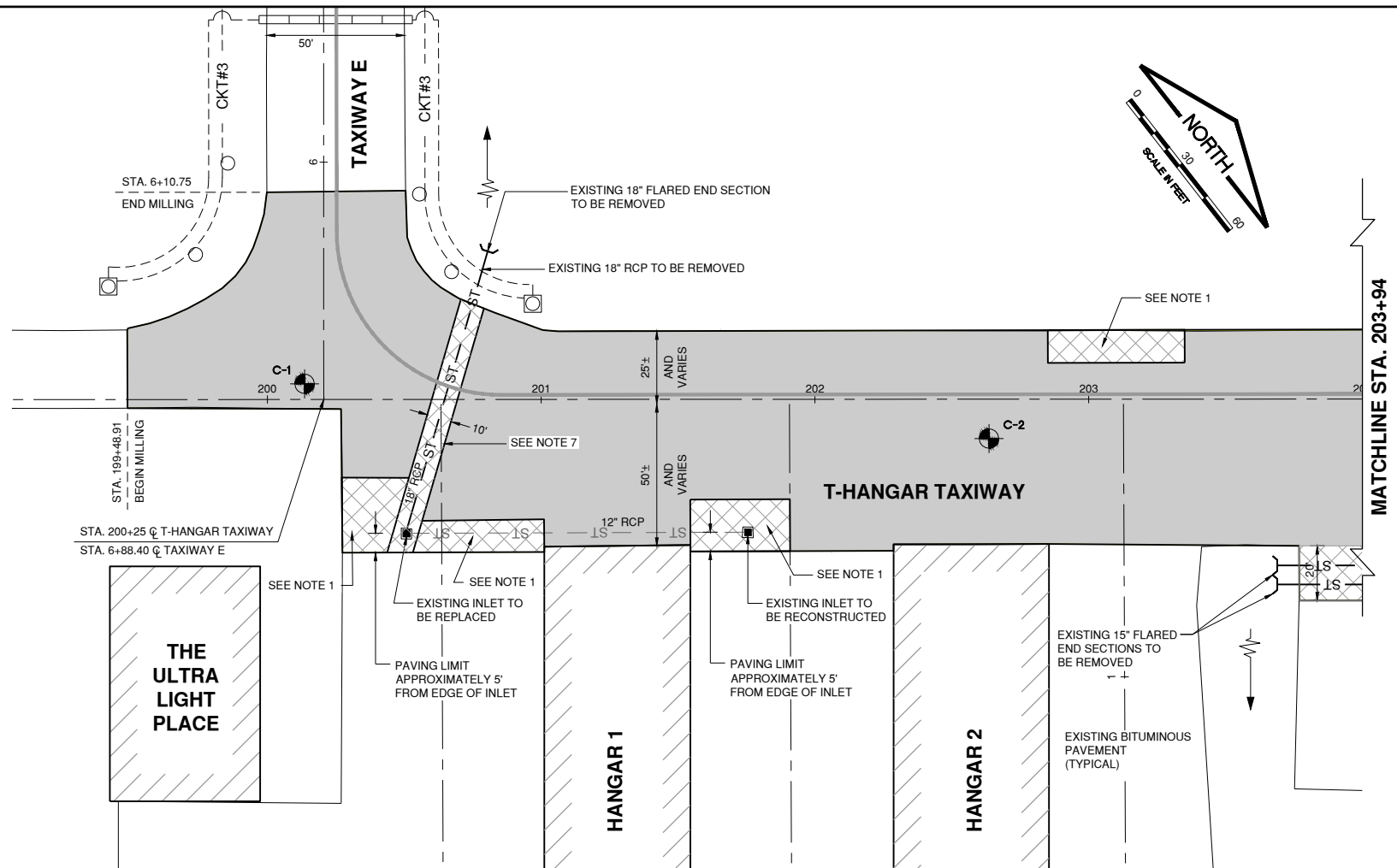
NOTES

- FULL DEPTH OF EXISTING PAVEMENT TO BE REMOVED. EXISTING AGGREGATE BASE AND SUBGRADE SHALL BE REMOVED TO A DEPTH OF APPROXIMATELY 18" BELOW THE EXISTING PAVEMENT AND BE REPLACED WITH 6" AGGREGATE BASE COURSE (209) AND MINIMUM 12" POROUS GRANULAR EMBANKMENT (208). PLACE NEW 6" BITUMINOUS SURFACE COURSE (AR401610) IN 3 - 2" LIFTS.
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWS CUT FULL DEPTH AROUND PERIMETER OF REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
- CONTRACTOR SHALL TAKE MEASURES TO PROTECT EXISTING PAVEMENTS. ANY PAVEMENT DAMAGED BY CONTRACTORS EQUIPMENT SHALL BE SAWCUT PER RESIDENT ENGINEER LAYOUT AND REPLACED IN KIND AT NO ADDITIONAL COST TO CONTRACT.
- CONTRACTOR TO TAKE MEASURES TO PROTECT ALL UNDERGROUND UTILITIES INCLUDING, BUT NOT LIMITED TO, NEW AND EXISTING SHALLOW STORM SEWER PIPE FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.
- EXISTING CONCRETE FOUNDATIONS AND WALLS OUTSIDE EXISTING HANGAR FACE SHALL BE PROTECTED BY CONTRACTOR. CONTRACTOR SHALL COMPLETE PROPOSED PAVEMENT REHABILITATION UP TO THESE CONCRETE FOUNDATIONS AND WALLS. SHOULD THE EXISTING FOUNDATIONS AND WALLS BE DAMAGED, THE CONTRACTOR SHALL REPLACE IN-KIND AT HIS OWN EXPENSE.
- ANY ELECTRICAL DUCT MARKERS REMOVED WITH THE EXISTING PAVEMENT SHALL BE REPLACED IN KIND. ALL COSTS ASSOCIATED WITH ELECTRICAL DUCT MARKER REPLACEMENT SHALL BE INCIDENTAL TO THE CONTRACT.
- FULL DEPTH OF EXISTING PAVEMENT TO BE REMOVED. EXISTING AGGREGATE BASE AND SUBGRADE AT EXISTING STORM LINES TO BE REPLACED SHALL BE REMOVED TO A MINIMUM DEPTH OF 3' BELOW THE TOP OF EXISTING PAVEMENT AND REPLACED WITH 6" CRUSHED AGGREGATE BASE COURSE (209) AND CA-7 TRENCH BACKFILL (701). PLACE NEW 6" BITUMINOUS SURFACE COURSE (AR401610) IN 3 - 2" LIFTS.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

LEGEND

- EXISTING BUILDING
- PAVEMENT BORING LOCATION
- PAVEMENT CORE LOCATION
- EXISTING FLOW DIRECTION
- NEW BITUMINOUS PAVEMENT MILLING (0" - 2.5")(401)
- NEW CLEAN AND SEAL BITUMINOUS CRACKS (201)
- NEW SAND MIX CRACK REPAIR (201)
- NEW FULL DEPTH BITUMINOUS PAVEMENT REMOVAL (4"-6" DEPTH) (401)
- EXISTING DRAINAGE INLET
- EXISTING STORM SEWER
- EXISTING STAKE MOUNTED TAXIWAY EDGE LIGHT
- EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT
- EXISTING TAXIWAY A LIGHTING CIRCUIT
- EXISTING ELECTRICAL DUCT BANK



IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

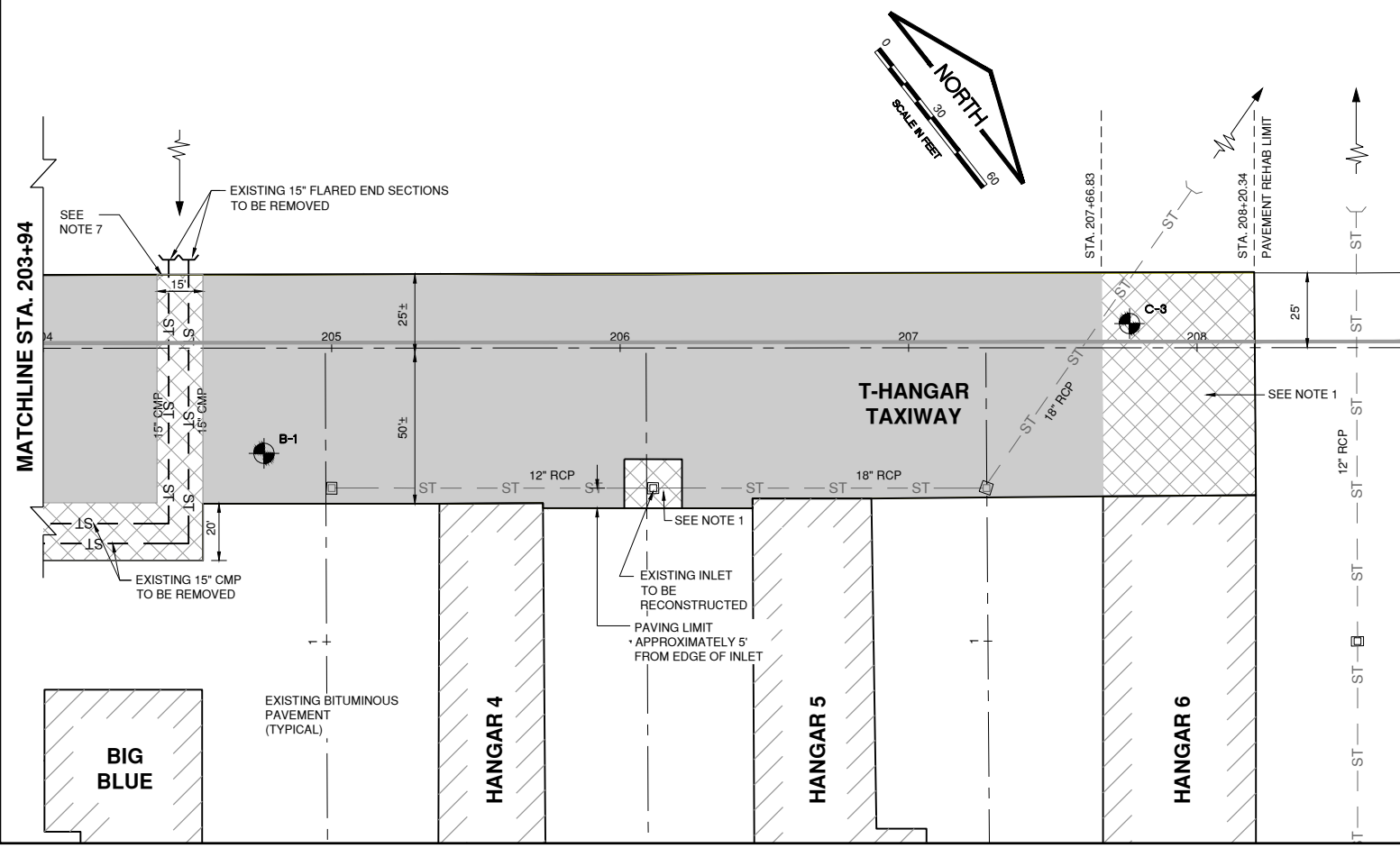
SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE SW QUADRANT TAXIWAY AND
 AIRFIELD PAVEMENT REPAIRS**

EXISTING CONDITIONS/PROPOSED REMOVALS



LOG OF BORING NO. B-01

DEPTH (FT.) BELOW GROUND SURFACE	SAMPLE NUMBER	SAMPLE TYPE	SAMPLE DISTANCE	SAMPLE RECOVERY	DESCRIPTION OF MATERIAL	UNIT DRILL WT. (LBS./FT.)	UNCONFINED COMPRESSIVE STRENGTH (TONS/FT ²)	WATER CONTENT %	STANDARD "N" PENETRATION (BLOWS/FT.)
0.0	1	AS	0.0	100%	GROUND SURFACE ELEVATION 620.85	4.0"	10	20	30
1.0	2	SS	1.0	100%	4.0" ASPHALT	118	10	20	30
2.0	3	SS	2.0	100%	SANDY LOAM (Topsoil)-black-loose (A-2-4)	119	10	20	30
3.0	4	SS	3.0	100%	POORLY GRADED SAND-brown-loose (SP)	119	10	20	30
4.0	5	SS	4.0	100%	Petroleum Odor Noted	119	10	20	30
5.0	6	SS	5.0	100%	LEAN CLAY with Sand-gray-very stiff (CL)	114	10	20	30

WATER LEVEL OBSERVATIONS:
 Water Level While Drilling: 3.5'
 Water Level After Boring: -

BORING STARTED: 06/29/2018
 BORING COMPLETED: 06/29/2018
 RIG: CME-55 FOREMAN: R.J.
 DRAWN: RWC APPROVED: A.P.
 CS: JOB No. 18062 SHEET 1 OF 1

PAVEMENT CORE SUMMARY

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C-01	2.0 2.25 2.5 2.75 3.0	Station: 204+81 Offset: 37.0' Right ASPHALT-well consolidated, fine to medium coarse aggregate. ASPHALT-well consolidated, fine to medium coarse aggregate. CRUSHED STONE LEAN CLAY with Sand-brown (CL)
C-02	1.5 2.25 2.5 2.75 3.0	Station: 202+68 Offset: 15.0' Right ASPHALT-well consolidated, fine to medium coarse aggregate. ASPHALT-well consolidated, fine to medium coarse aggregate. ASPHALT-well consolidated, fine to medium coarse aggregate. CRUSHED STONE CLAYEY SAND & STONE-dark brown & black (FI)
C-03	2.0 1.75 4.25	Station: 207+81 Offset: 8.0' Left ASPHALT-well consolidated, fine to medium coarse aggregate. ASPHALT-slightly porous & fractured, fine to medium coarse aggregate. CRUSHED STONE CLAYEY SAND & STONE-dark brown & black (FI)

THE GEOTECHNICAL REPORT (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY
AVAA
 AIRPORT AUTHORITY

DESIGN BY: ABM
 DRAWN BY: JRO
 CHECKED BY: ABM
 APPROVED BY: DKP
 DATE: 11/16/2018
 JOB No: 18007502.00

FINAL









SHEET 7 OF 10 SHEETS

D:\p\18125035\18125035.dwg
 12/15/2018 12:50:35 PM
 K:\k\18125035\18125035.dwg

UPDATE BY: Adam Moulton
 LAYOUT: Layout1

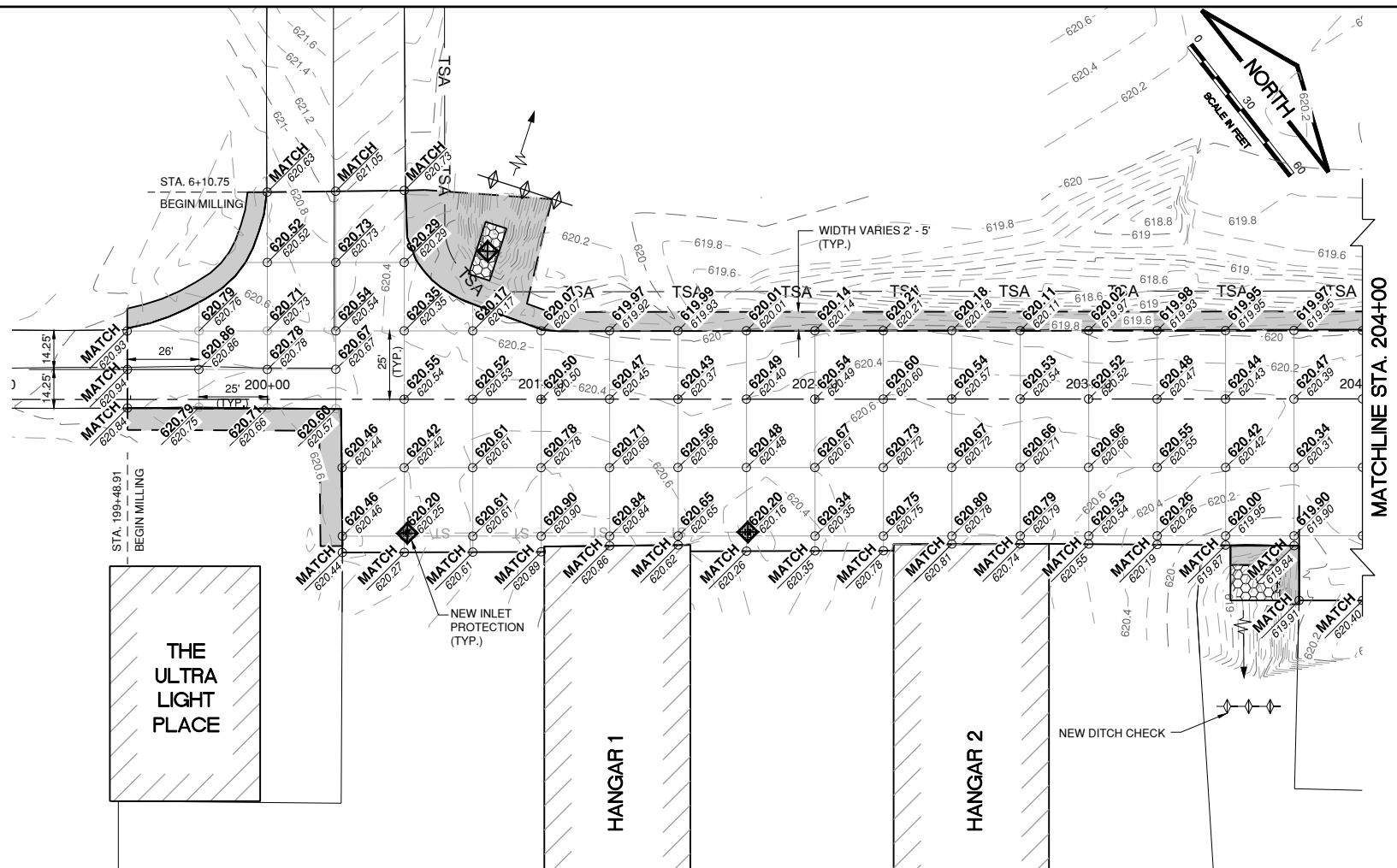
DATE: Friday, December 14, 2018 12:50:35 PM
 FILE: K:\k\18125035\18125035.dwg

LEGEND

-  620.50 NEW GRADE
620.49 EXISTING GRADE
-  NEW INLET PROTECTION (156)
-  NEW TOPSOIL (905), SEEDING (901), AND HEAVY DUTY MULCH (908)
-  NEW TOPSOIL (905), SEEDING (901), AND EXCELSIOR BLANKET (156)
-  EXISTING FLOW DIRECTION
-  NEW DITCH CHECK (156)
-  -619.6- EXISTING CONTOURS
-  TSA TAXIWAY SAFETY AREA

NOTES

1. THE TAXIWAY SAFETY AREA (TSA) IS LOCATED 39.5' FROM THE APRON TAXIWAY CENTERLINE. THE TURF SLOPE WITHIN THE TSA SHALL BE NO LESS THAN 1.5%, EXCEPT DIRECTLY OVER THE NEW STORM SEWER PIPE, AND NO GREATER THAN 5%.
2. RE-GRADE ADJACENT PORTIONS OF THE EXISTING SWALE(S) TO DRAIN TO THE NEW FLARED END SECTIONS. MINIMUM SLOPE OF THE SWALE BOTTOM SHALL BE 1.0% WITH SIDE SLOPES OF 5:1.
3. CUT SHALL BE PAID FOR AS UNCLASSIFIED EXCAVATION (AR152410). UNDERCUT MATERIAL SHALL BE USED FOR TURF FILL, AND IS INCIDENTAL TO UNCLASSIFIED EXCAVATION.

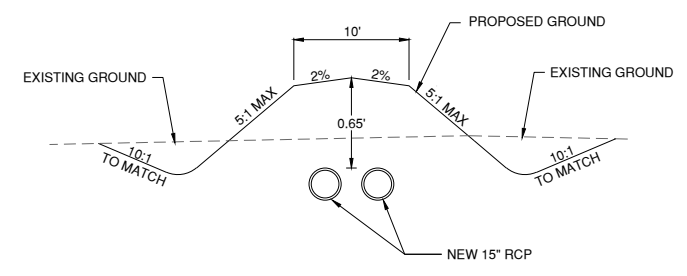
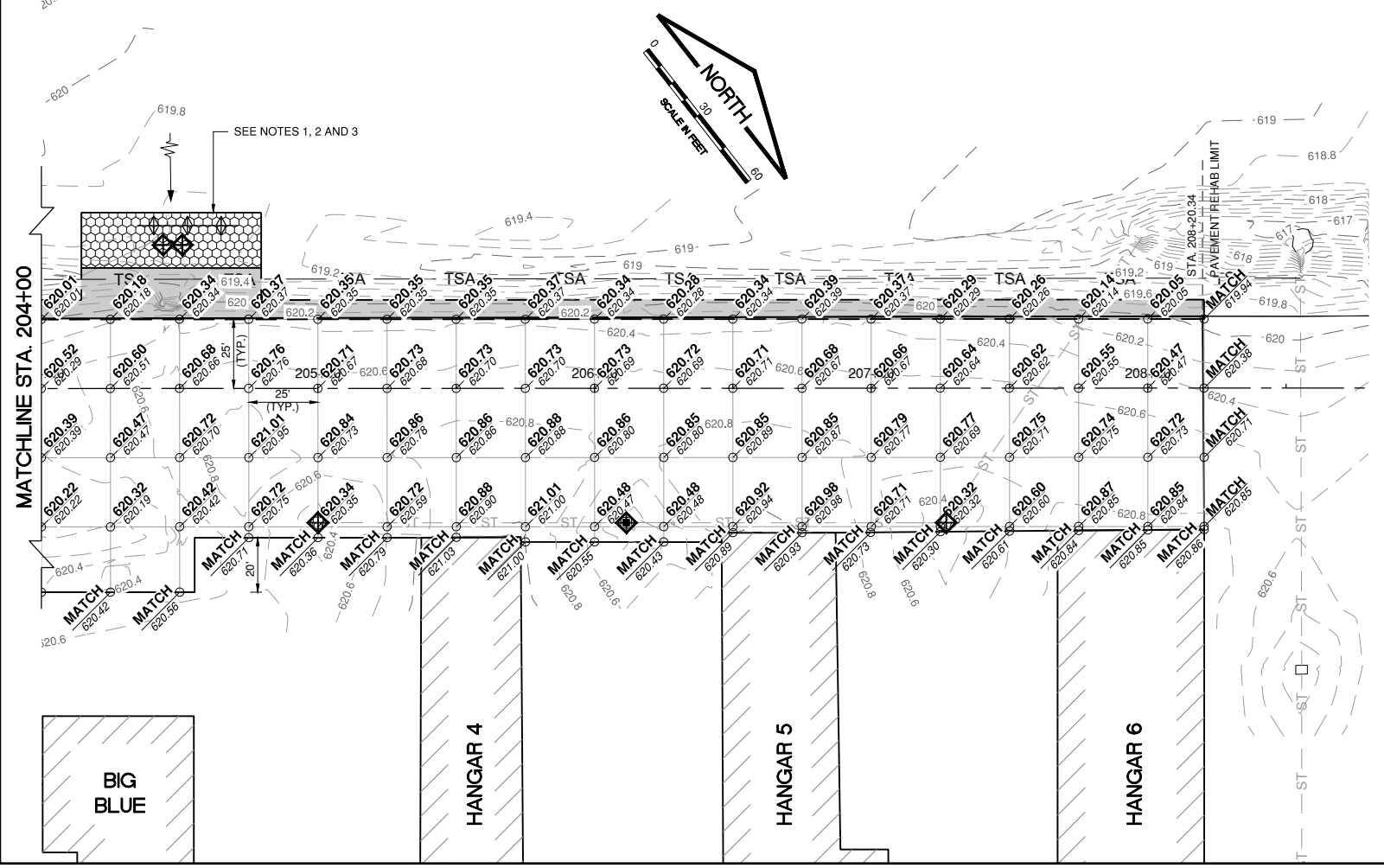


IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



TURF COVER OVER NEW STORM PIPE BEYOND TAXIWAY SAFETY AREA

NOT TO SCALE

NOTE: BETWEEN THE EDGE OF PAVEMENT AND THE TSA, THE SLOPE OF THE TURF DIRECTLY OVER THE NEW RCP PIPE SHALL BE 0%. BEYOND THE TSA, COVERING TURF SHALL BE SLOPED TO THE TOP OF THE NEW FLARED END SECTIONS.

GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS
REHABILITATE SW QUADRANT TAXIWAY AND
AIRFIELD PAVEMENT REPAIRS

STAKING AND LANDSCAPING PLAN

© Copyright CMT, Inc.

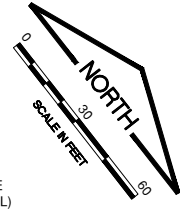
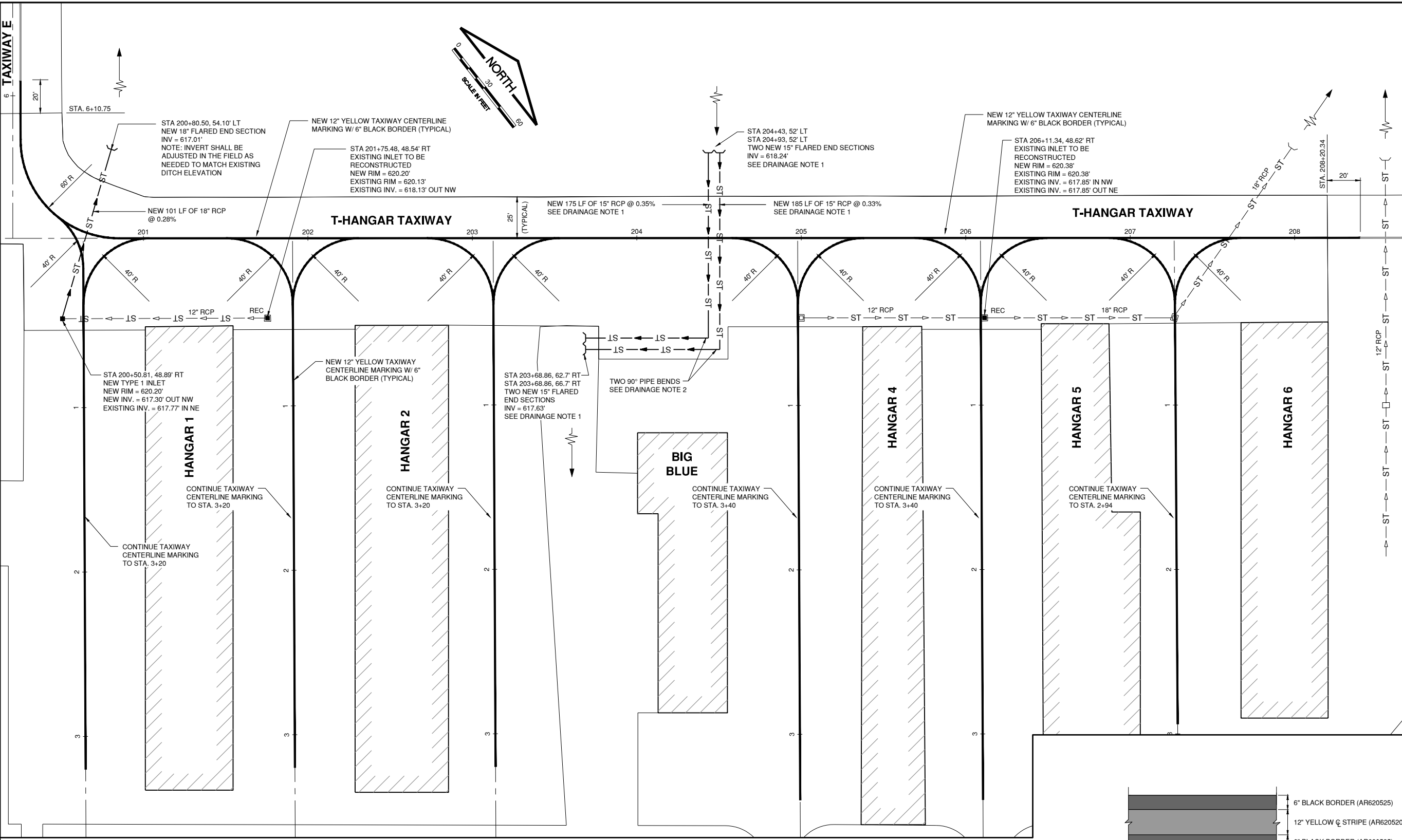
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

KANKAKEE VALLEY

 AIRPORT AUTHORITY

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	11/16/2018
JOB No:	18007502.00
FINAL	
SHEET 8 OF 10 SHEETS	

DATE: Friday, December 14, 2018 12:50:46 PM
 FILE: K:\kankakee\18175-02_SWO\Drawings\18175-02_Drainage And Marking Plans.dwg
 UPDATE BY: Adam Moulton
 LAYOUT: Layout1
 6 IMPRESA QPUSH PRINT AS SWP:01
 6 TAXIWAY E



LEGEND

- NEW END SECTION
- EXISTING END SECTION
- NEW INLET
- EXISTING INLET
- EXISTING INLET TO BE RECONSTRUCTED
- NEW STORM SEWER
- EXISTING STORM SEWER
- NEW PAVEMENT MARKING
- EXISTING PAVEMENT MARKING
- FLOW DIRECTION

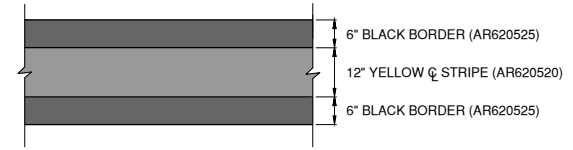
DRAINAGE NOTES

- THE INDICATED NEW FLARED END SECTIONS AND NEW RCP SHALL BE INSTALLED ADJACENT TO EACH OTHER. SEPARATION SHALL BE 4' FROM CENTER OF PIPE TO CENTER OF PIPE, OR AS DIRECTED BY THE RESIDENT ENGINEER.
- THE 90° BENDS IN THE STORM PIPE SHALL NOT BE PAID FOR SEPARATELY, AND SHALL BE INCLUDED IN THE QUANTITY FOR 15" RCP, CLASS IV (AR701515).

PAVEMENT MARKING NOTES

- ALL NEW AND EXISTING PAVEMENTS TO BE MARKED OR RE-MARKED SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- RESIDENT ENGINEER SHALL PROVIDE LAYOUT DIMENSIONS FOR TAXILANE CENTERLINES BETWEEN T-HANGARS IN COORDINATION WITH AIRPORT.

TAXIWAY CENTERLINE DETAIL



NOT TO SCALE

NOTES

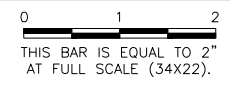
- CONTRACTOR SHALL REPLACE TAXIWAY AND RUNWAY CENTERLINE MARKINGS AT PAVEMENT REMOVAL AND REPLACEMENT LOCATIONS OR AS DIRECTED BY THE ENGINEER.

IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

SURVEY BOOK # BOOK #

REVISIONS

NUMBER	BY	DATE



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE SW QUADRANT TAXIWAY AND
 AIRFIELD PAVEMENT REPAIRS**

**DRAINAGE IMPROVEMENTS/
 PAVEMENT MARKING PLAN**

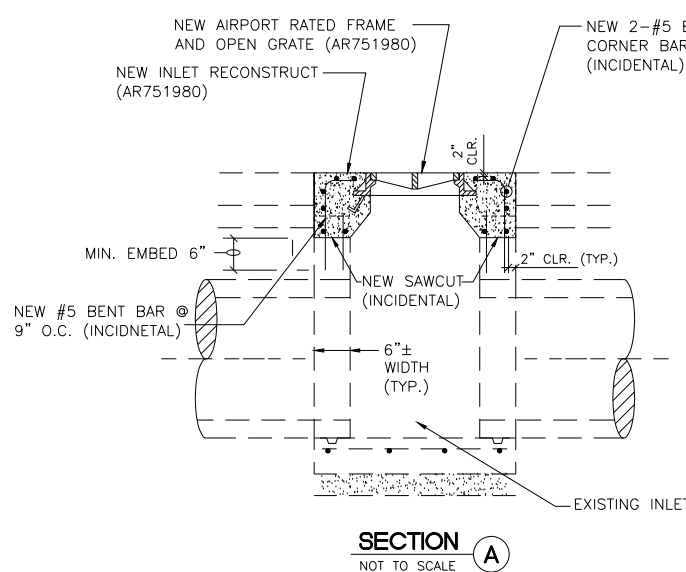
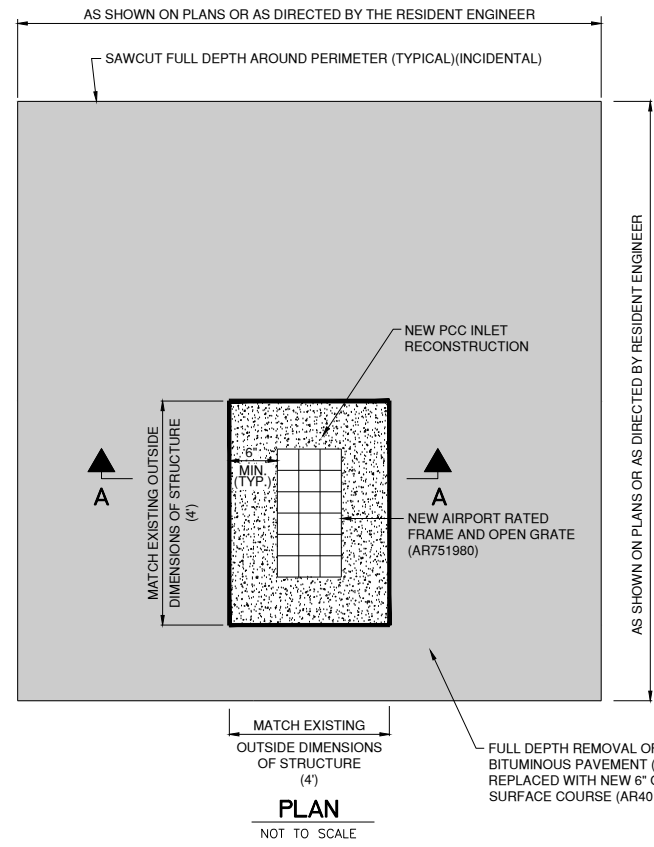
© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613
 KANKAKEE VALLEY

 AIRPORT AUTHORITY

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	ABM
APPROVED BY:	DKP
DATE:	11/16/2018
JOB No:	18007502.00

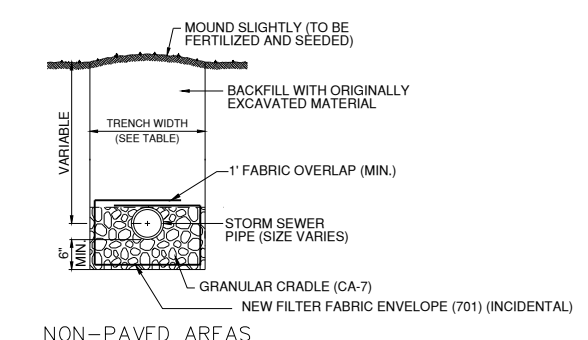
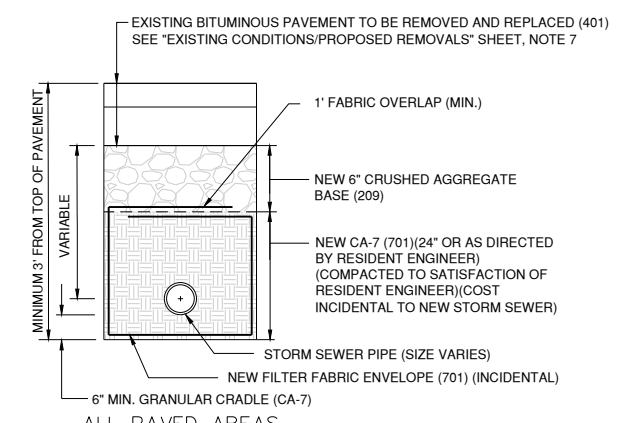
FINAL

DATE: Friday, December 14, 2018 12:50:54 PM
 FILE: K:\kankakee\1675-02_SWO\Drawings\DrawSheets\167502 - Drainage And Misc Details.dwg
 UPDATE BY: Adam Moulton
 LAYOUT: Layout1



RECONSTRUCT INLET - TYPICAL (AR751980)
NOT TO SCALE
SEE EXISTING CONDITIONS/PROPOSED REMOVALS FOR LOCATIONS

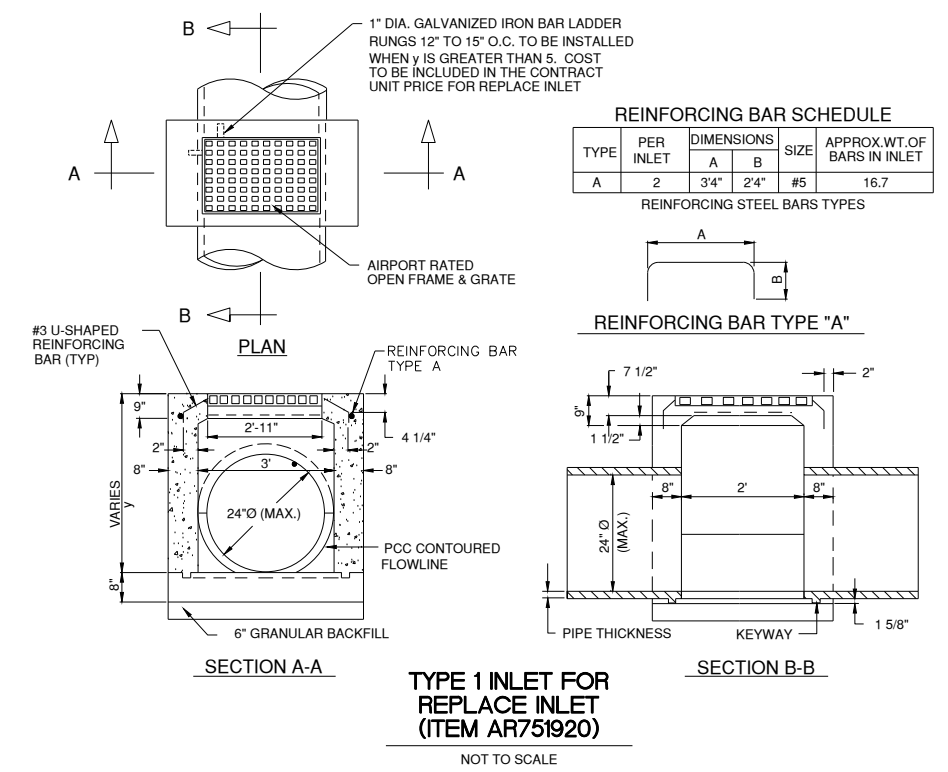
INSIDE DIAMETER OF STORM SEWER (INCHES)	MAXIMUM TRENCH WIDTH
6	3'-7"
8	3'-9"
12	4'-2"
15	4'-6"
18	4'-9"
21	5'-0"
24	5'-4"
27	5'-7"
30	5'-11"
36	6'-6"
42	7'-1"
48	7'-8"
54	8'-3"
60	8'-10"
66	9'-5"
72	10'-0"
78	10'-7"
84	11'-2"
90	11'-9"
96	12'-4"
102	12'-11"
108	13'-6"



TRENCH DETAILS - STORM SEWER
NOT TO SCALE

REPLACE INLET NOTES

- 1/2" CHAMFER TO BE USED ON ALL EXPOSED CORNERS ON INLETS. BARS TO BE INSTALLED 2" FROM FACE OF WALL.
- INLET TO BE CONSTRUCTED OF STRUCTURAL P.C. CONCRETE. THE CONTRACT UNIT PRICE FOR REPLACE INLET SHALL INCLUDE THE AIRPORT RATED GRATE AND FRAME.
- REPLACE INLET SHALL INCLUDE THE COMPLETE REMOVAL AND DISPOSAL OF EXISTING STRUCTURE. PIPE DAMAGED DURING STRUCTURE REMOVAL SHALL BE REPAIRED OR REPLACED AT CONTRACTOR'S EXPENSE.
- THE COST TO CONNECT OR REMOVE/REPAIR EXISTING PIPE SHALL BE INCIDENTAL TO THE REPLACE INLET ITEM.
- SEE "EXISTING CONDITIONS/PROPOSED REMOVALS" AND "DRAINAGE IMPROVEMENTS AND PAVEMENT MARKING PLAN" SHEETS FOR LOCATIONS OF PIPE INLET/OUTLET LOCATIONS.



IL CONTRACT: **KA048**
 IL LETTING ITEM: **23A**
 IL PROJECT: **IKK-4672**
 S.B.G. PROJECT: **3-17-SBGP-139/144**

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34x22).

**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS
 REHABILITATE SW QUADRANT TAXIWAY AND
 AIRFIELD PAVEMENT REPAIRS**
DRAINAGE DETAILS

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613
 KANKAKEE VALLEY
AVIA
 AIRPORT AUTHORITY

DESIGN BY: ABM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: XXX
 DATE: 11/16/2018
 JOB No: 18007502.00

FINAL