

CONSTRUCTION PLANS

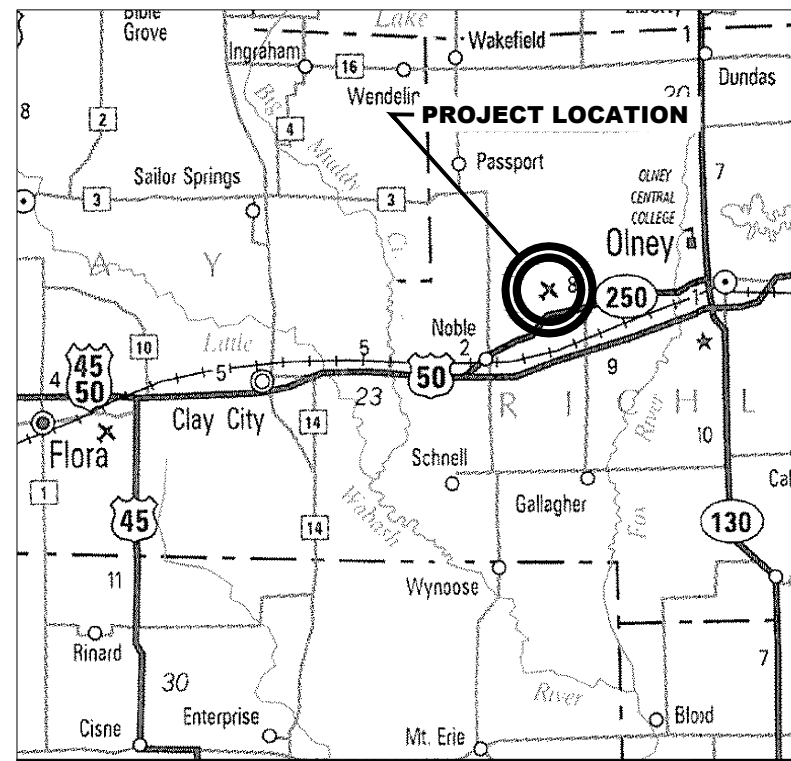
REHABILITATE AIRFIELD PAVEMENT MARKINGS

**OLNEY-NOBLE AIRPORT AUTHORITY
OLNEY-NOBLE AIRPORT (OLY)
NOBLE, RICHLAND COUNTY, ILLINOIS**

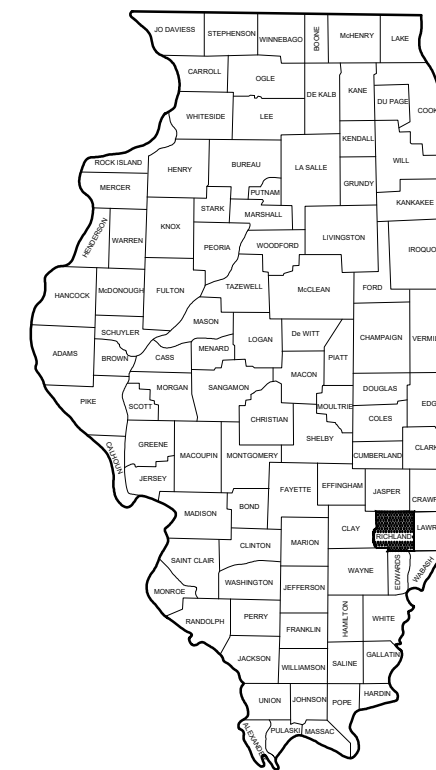
**IDA PROJECT NO.: OLY-4577
FAA AIP PROJECT : 3-17-SBGP-133/139**

NOVEMBER 16, 2018

VICINITY MAP



LOCATION MAP



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

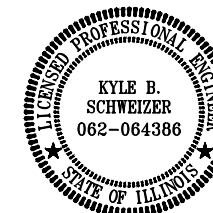
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No.	Issue/Description	Sheets Changed	Date	By



HANSON PROFESSIONAL SERVICES INC.
1525 South Sixth Street
Springfield, Illinois 62703-2886
Telephone: 217.788.2450
Fax: 217.788.2503

Kyle B. Schweizer
Kyle B. Schweizer, P.E.
Project Engineer



November 16, 2018
Date

Lic. Exp. 11/30/2019

**OLNEY-NOBLE AIRPORT AUTHORITY
OLNEY-NOBLE AIRPORT**
4395 North Airport Road
Noble, Illinois 62868
Telephone: 618-393-2967

Thomas D. Baker
Thomas D. Baker
Airport Manager

November 16, 2018
Date

INDEX TO SHEETS

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SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150520	MOBILIZATION	L.S.	1	
AR401661	BITUMINOUS CRACK SEALING	L.F.	1,982	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	41,188	
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	14,796	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	8,598	

GENERAL NOTES

- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPERLY MONITORING ANY CONFINED SPACES AND PROVIDING ANY TRAINING TO HIS EMPLOYEES AS NECESSARY TO COMPLETE THE PROJECT.
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
- THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- CONTRACTOR SHALL NOTE THAT SOME AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING ANY WORK. ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITY WHICH HAVE BEEN PREVIOUSLY FARMED OR ARE DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION SHALL BE CHISEL PLOWED OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER) AT THE CONTRACTOR'S EXPENSE.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- NPDES PERMIT - THIS PROJECT WILL NOT DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS NOT REQUIRED.
- MATERIAL CERTIFICATIONS - MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
- CERTIFIED PAYROLLS - THE RESIDENT ENGINEER CANNOT FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY _____ RICHLAND
 CITY _____ OLNEY
 TOWNSHIP _____ NOBLE
 SECTION NO. _____ 2
 ADDRESS _____ OLNEY-NOBLE AIRPORT
 4395 N. AIRPORT RD.
 NOBLE, ILLINOIS 62868

MARKING SCHEDULE

WHITE			
DESCRIPTION	UNIT AREA	NO. REQUIRED	TOTAL AREA
11/29 RUNWAY CENTERLINES	360	19	6,840
4/22 RUNWAY CENTERLINES	360 (VAR.)	17	5,652
RUNWAY DESIGNATOR NUMERAL 1	318	2	636
RUNWAY DESIGNATOR NUMERAL 2	652	3	1,956
RUNWAY DESIGNATOR NUMERAL 4	602	1	602
RUNWAY DESIGNATOR NUMERAL 9	712	1	712
THRESHOLD STRIPE	863	10	8,630
AIMING POINT	2,250	4	9,000
TOTAL WHITE			34,028
YELLOW			
TAXIWAY CENTERLINES	SEE PLANS	SEE PLANS	6,250
HOLDING POSITIONS	VARIES	5	598
TIEDOWNS	39	8	312
TOTAL YELLOW			7,160
BLACK			
11/29 RUNWAY CENTERLINES	124	19	2,356
4/22 RUNWAY CENTERLINES	124 (VARIES)	17	1,952
RUNWAY DESIGNATOR NUMERAL 1	68	2	136
RUNWAY DESIGNATOR NUMERAL 2	107	3	321
RUNWAY DESIGNATOR NUMERAL 4	107	1	107
RUNWAY DESIGNATOR NUMERAL 9	114	1	114
THRESHOLD STRIPE	157	10	1,570
AIMING POINT	166	4	664
TAXIWAY CENTERLINES	SEE PLANS	SEE PLANS	6,250
HOLDING POSITIONS	VARIES	5	998
TIEDOWNS	41	8	328
TOTAL BLACK			14,796
REMOVAL			
RUNWAY DESIGNATOR 21	2,520	1	2,520
RUNWAY DESIGNATOR 3	1,200	1	1,200
11/29 RUNWAY CENTERLINES	SEE PLANS	SEE PLANS	2,116
TAXIWAY CENTERLINE B	SEE PLANS	SEE PLANS	1,982
SUPP. (+10%)			780
TOTAL REMOVAL			8,598

REHABILITATE AIRFIELD PAVEMENT MARKINGS

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

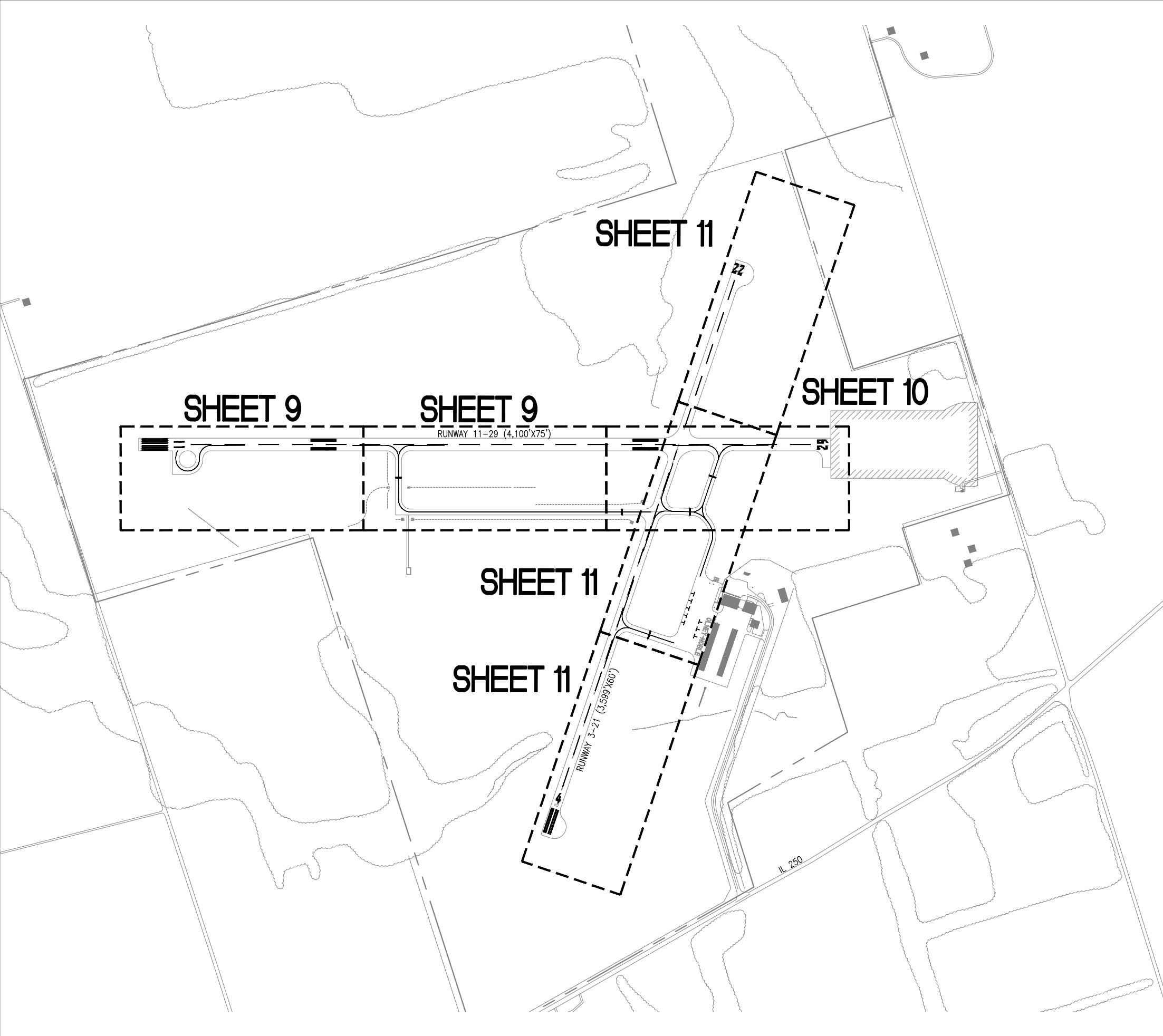
ISSUE: NOVEMBER 16, 2018
 PROJECT NO: 17A0006
 CAD FILE: G-002-SQ.DWG
 DESIGN BY: MJD 10/09/2018
 DRAWN BY: MJD 10/09/2018
 REVIEWED BY: KBS 11/14/2018

SHEET TITLE

SUMMARY OF QUANTITIES AND INDEX TO SHEETS

PROJECT SCOPE

THE PROJECT WILL CONSIST OF MARKING THE ENTIRE AIRFIELD INCLUDING ALL RUNWAYS, TAXIWAYS, AND THE APRON. RUNWAY 3/21 WILL BE ALSO BE MARKED AS 4/22 AS PART OF THE PROJECT.



**REHABILITATE AIRFIELD
PAVEMENT MARKINGS**

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2018
PROJECT NO: 17A0006
CAD FILE: G-004-KEY.DWG
DESIGN BY: MJD 10/09/2018
DRAWN BY: MJD 10/09/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

PROJECT OVERVIEW

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CONSTRUCTION SAFETY PLAN PHASE 1A NOTES

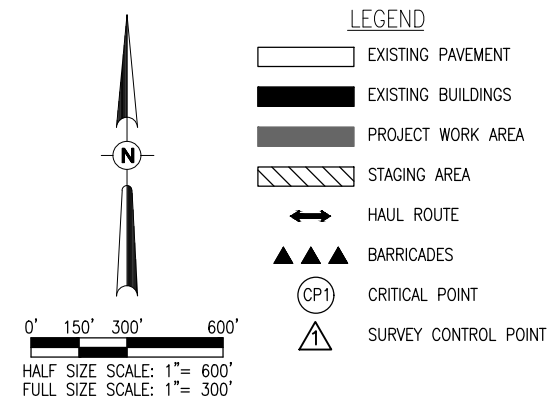
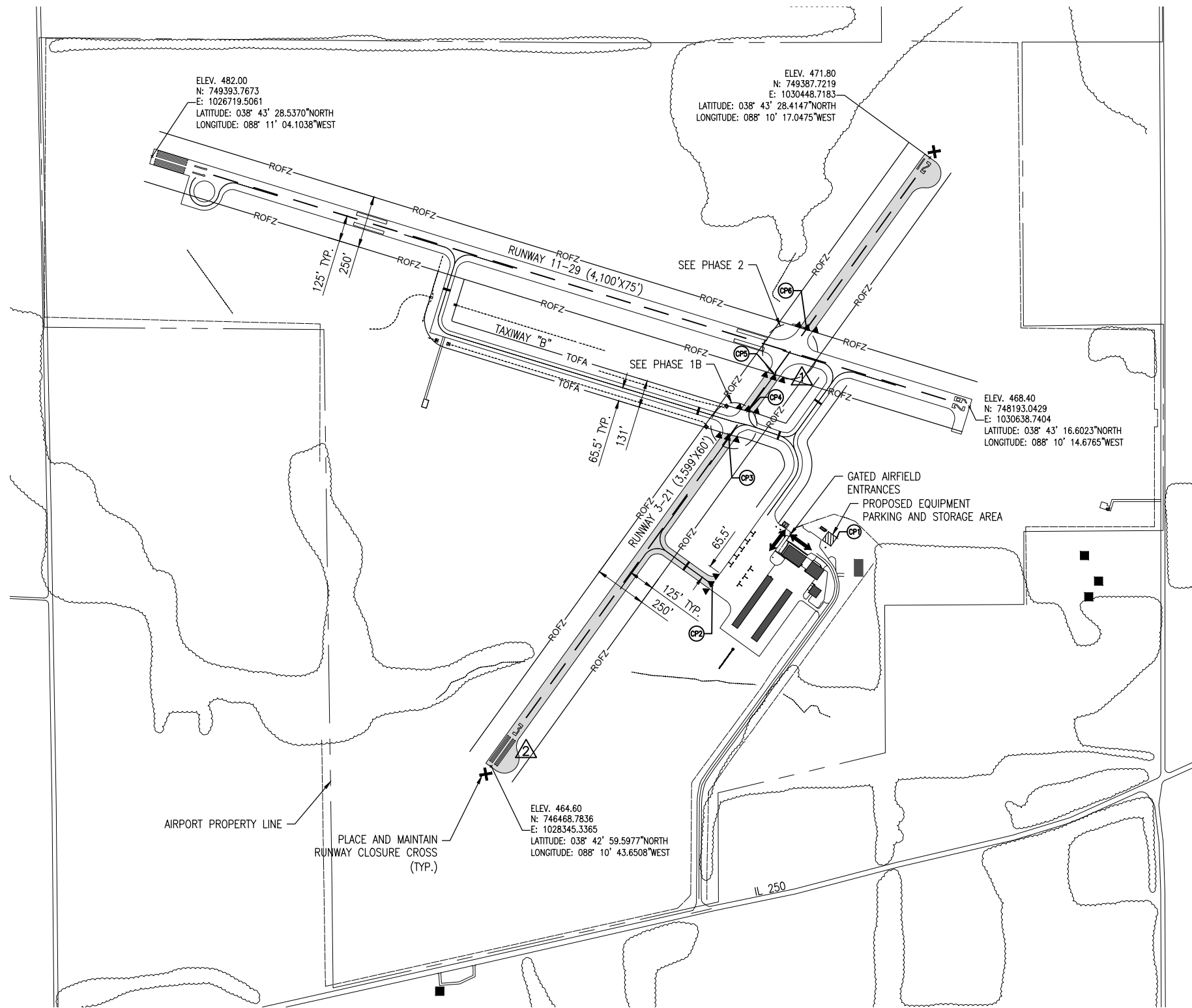
- SCOPE** - PHASE 1A WILL INCLUDE REMOVAL AND REPLACEMENT OF EXISTING RUNWAY 3/21 (PROPOSED 4/22) MARKINGS AS DETAILED HEREIN.
- COORDINATION** - RUNWAY/TAXIWAY CLOSURES FOR EACH PHASE SHALL BE COORDINATED WITH THE AIRPORT MANAGER A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME. THE AIRPORT MANAGER HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- AIR OPERATIONS AREA (AOA)** - AT NO TIME MAY THE CONTRACTOR HAVE PERSONNEL, MATERIAL, OR EQUIPMENT WITHIN 125' OF RUNWAY CENTERLINES WHILE THE RUNWAY IS OPEN FOR AIRCRAFT OPERATIONS.
- CLOSURES** - THE CONTRACTOR SHALL CLOSE EXISTING RUNWAYS AND TAXIWAYS ON A DAILY BASIS WHEN NO PERSONNEL OR EQUIPMENT ARE WITHIN THE AOA. NO OVERNIGHT CLOSURES WILL BE PERMITTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THAT HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BEING ISSUED THE NOTICE TO PROCEED. ANY CHANGES WILL REQUIRE APPROVAL BY THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- REFER TO THE CONSTRUCTION SAFETY NOTES & DETAILS SHEET FOR ADDITIONAL SAFETY INFORMATION.

RUNWAY/TAXIWAY CLOSURE PROCEDURES

- CONTACT THE AIRPORT MANAGER OR ASSIGNED REPRESENTATIVE A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME.
- ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
- PLACEMENT OF CROSSES AND BARRICADES.
- ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

RUNWAY/TAXIWAY - OPENING PROCEDURES

- ENSURE ALL PERSONNEL, EQUIPMENT, AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES
- REMOVE BARRICADES AND CROSSES.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.



NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	"OLNEPORT" NGS MONUMENT	748,331.50	1,029,840.81	469.12
2	"OLNEPORT AZ MK" NGS MONUMENT	746,543.74	1,028,518.32	460.68

- SURVEY NOTES**
- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND (MSL)	HEIGHT (AGL)	EQUIP. ELEV.
1	CONSTRUCTION EQUIPMENT	038° 43' 10.41"	-088° 10' 22.72"	464	15'	479
2	BARRICADE	038° 43' 08.20"	-088° 10' 30.26"	460	2'	462
3	BARRICADE	038° 43' 15.31"	-088° 10' 29.25"	465	2'	467
4	BARRICADE	038° 43' 16.42"	-088° 10' 28.10"	466	2'	468
5	BARRICADE	038° 43' 18.16"	-088° 10' 26.46"	467	2'	469
6	BARRICADE	038° 43' 20.28"	-088° 10' 24.53"	467	2'	469
7	BARRICADE	038° 43' 15.49"	-088° 10' 27.03"	468	2'	470

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND (MSL)	HEIGHT (AGL)	EQUIP. ELEV.
8	BARRICADE	038° 43' 22.88"	-088° 10' 46.03"	473	2'	475
9	BARRICADE	038° 43' 11.47"	-088° 10' 26.92"	463	2'	465
10	BARRICADE	038° 43' 08.36"	-088° 10' 29.24"	461	2'	463
11	BARRICADE	038° 43' 16.23"	-088° 10' 30.24"	466	2'	468
12	BARRICADE	038° 43' 18.85"	-088° 10' 23.93"	468	2'	470
13	BARRICADE	038° 43' 19.62"	-088° 10' 27.11"	470	2'	472

REHABILITATE AIRFIELD
PAVEMENT MARKINGS

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2018
PROJECT NO: 17A0006
CAD FILE: G-003-SFY.DWG
DESIGN BY: MJD 10/03/2018
DRAWN BY: MJD 10/03/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

CONSTRUCTION
SAFETY PHASING
PLAN - PHASE 1A

CONSTRUCTION SAFETY PLAN PHASE 1B NOTES

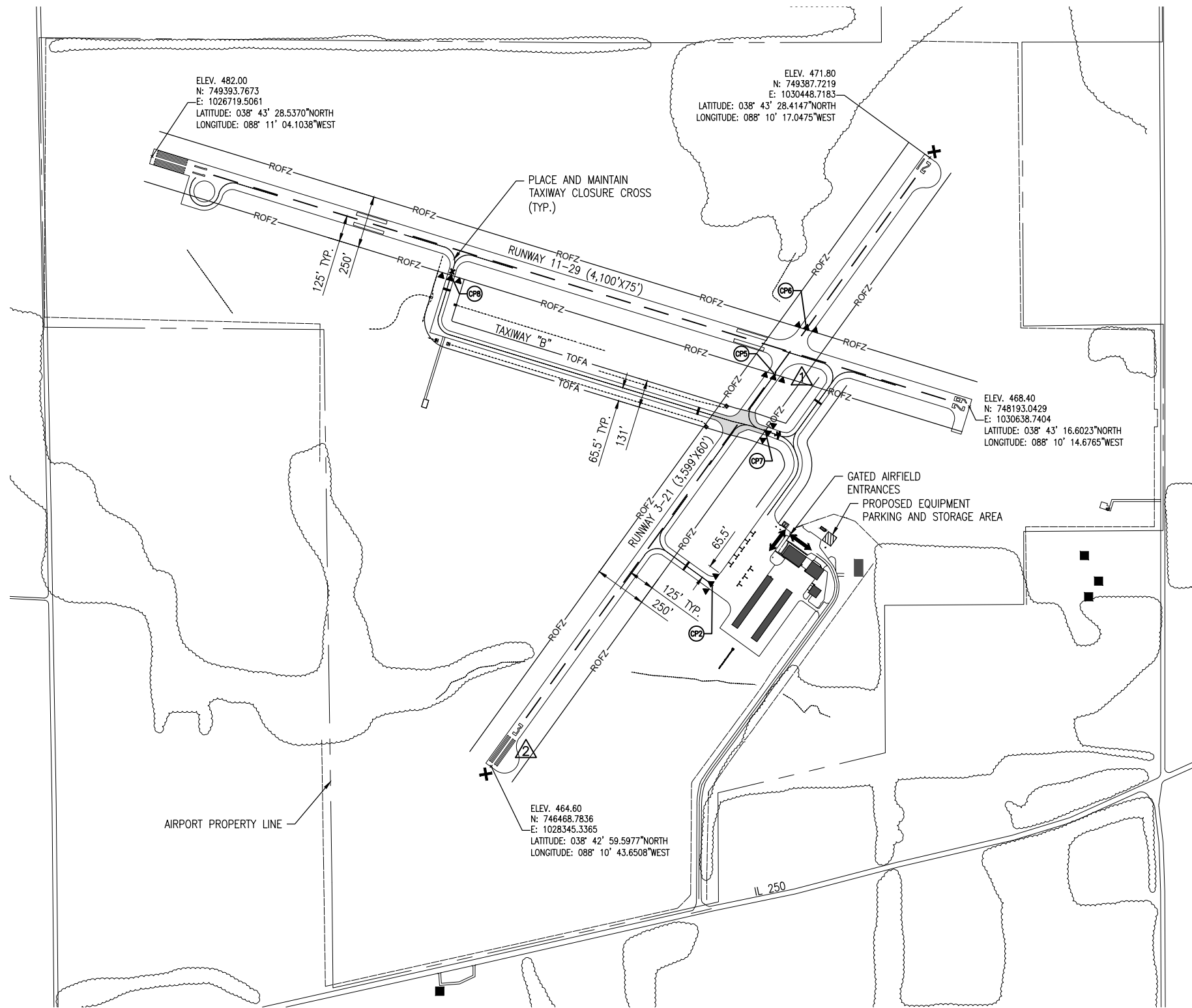
- SCOPE** - PHASE 1B WILL INCLUDE REMOVAL AND REPLACEMENT OF EXISTING RUNWAY 3/21 (PROPOSED 4/22) AND TAXIWAY B MARKINGS AS DETAILED HEREIN.
- COORDINATION** - RUNWAY/TAXIWAY CLOSURES FOR EACH PHASE SHALL BE COORDINATED WITH THE AIRPORT MANAGER A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME. THE AIRPORT MANAGER HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- AIR OPERATIONS AREA (AOA)** - AT NO TIME MAY THE CONTRACTOR HAVE PERSONNEL, MATERIAL, OR EQUIPMENT WITHIN 125' OF RUNWAY CENTERLINES WHILE THE RUNWAY IS OPEN FOR AIRCRAFT OPERATIONS.
- CLOSURES** - THE CONTRACTOR SHALL CLOSE EXISTING RUNWAYS AND TAXIWAYS ON A DAILY BASIS WHEN NO PERSONNEL OR EQUIPMENT ARE WITHIN THE AOA. NO OVERNIGHT CLOSURES WILL BE PERMITTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THAT HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BEING ISSUED THE NOTICE TO PROCEED. ANY CHANGES WILL REQUIRE APPROVAL BY THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- REFER TO THE CONSTRUCTION SAFETY NOTES & DETAILS SHEET FOR ADDITIONAL SAFETY INFORMATION.

RUNWAY/TAXIWAY CLOSURE PROCEDURES

- CONTACT THE AIRPORT MANAGER OR ASSIGNED REPRESENTATIVE A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME.
- ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
- PLACEMENT OF CROSSES AND BARRICADES.
- ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

RUNWAY/TAXIWAY RW-OPENING PROCEDURES

- ENSURE ALL PERSONNEL, EQUIPMENT, AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES.
- REMOVE BARRICADES AND CROSSES.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.



LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDINGS
- PROJECT WORK AREA
- STAGING AREA
- HAUL ROUTE/SECONDARY HAUL ROUTE
- BARRICADES
- CRITICAL POINT
- SURVEY CONTROL POINT

Scale:
0' 150' 300' 600'
HALF SIZE SCALE: 1" = 600'
FULL SIZE SCALE: 1" = 300'

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SURVEY NOTES

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3	BARRICADE	038° 43' 15.31"	-088° 10' 29.25"	465	2'	467
4	BARRICADE	038° 43' 16.42"	-088° 10' 28.10"	466	2'	468
5	BARRICADE	038° 43' 18.16"	-088° 10' 26.46"	467	2'	469
6	BARRICADE	038° 43' 20.28"	-088° 10' 24.53"	467	2'	469
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AIP No:
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ISSUE: NOVEMBER 16, 2018
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DRAWN BY: MJD 10/03/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN - PHASE 1B

CONSTRUCTION SAFETY PLAN PHASE 2 NOTES

- SCOPE** - PHASE 2 WILL INCLUDE REMOVAL AND REPLACEMENT OF EXISTING RUNWAY 3/21 (PROPOSED 4/22) AND RUNWAY 11/29 MARKINGS AS DETAILED HEREIN.
- COORDINATION** - RUNWAY/TAXIWAY CLOSURES FOR EACH PHASE SHALL BE COORDINATED WITH THE AIRPORT MANAGER A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME. THE AIRPORT MANAGER HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- AIR OPERATIONS AREA (AOA)** - AT NO TIME MAY THE CONTRACTOR HAVE PERSONNEL, MATERIAL, OR EQUIPMENT WITHIN 125' OF RUNWAY CENTERLINES WHILE THE RUNWAY IS OPEN FOR AIRCRAFT OPERATIONS.
- CLOSURES** - THE CONTRACTOR SHALL CLOSE EXISTING RUNWAYS AND TAXIWAYS ON A DAILY BASIS WHEN NO PERSONNEL OR EQUIPMENT ARE WITHIN THE AOA. NO OVERNIGHT CLOSURES WILL BE PERMITTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THAT HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BEING ISSUED THE NOTICE TO PROCEED. ANY CHANGES WILL REQUIRE APPROVAL BY THE RESIDENT ENGINEER AND AIRPORT MANAGER.
- REFER TO THE CONSTRUCTION SAFETY NOTES & DETAILS SHEET FOR ADDITIONAL SAFETY INFORMATION.

RUNWAY/TAXIWAY CLOSURE PROCEDURES

- CONTACT THE AIRPORT MANAGER OR ASSIGNED REPRESENTATIVE A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME.
- ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
- PLACEMENT OF CROSSES AND BARRICADES.
- ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.

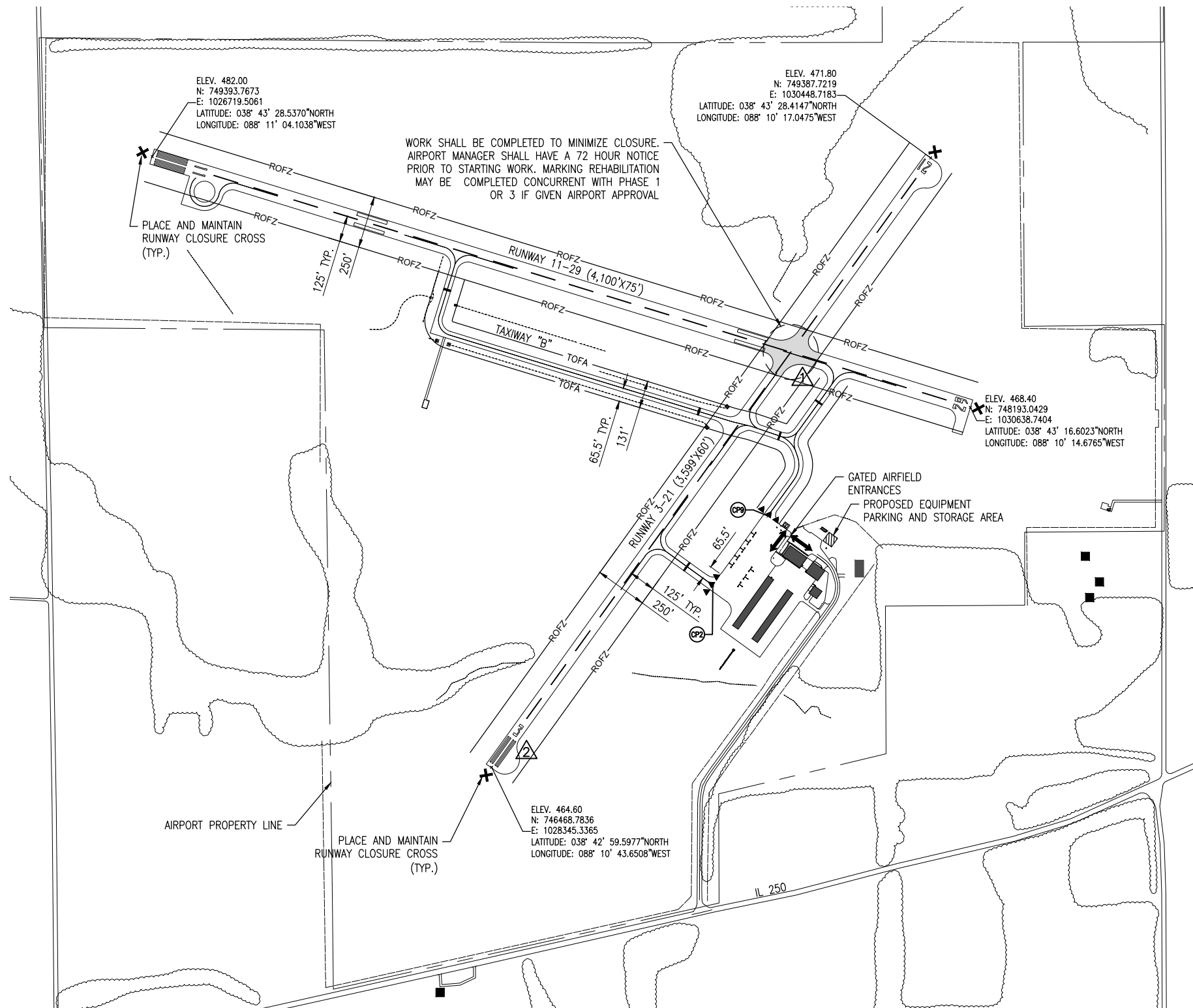
RUNWAY/TAXIWAY RW-OPENING PROCEDURES

- ENSURE ALL PERSONNEL, EQUIPMENT, AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES.
- REMOVE BARRICADES AND CROSSES.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.

LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDINGS
- PROJECT WORK AREA
- STAGING AREA
- HAUL ROUTE/SECONDARY HAUL ROUTE
- BARRICADES
- CRITICAL POINT
- SURVEY CONTROL POINT

0' 150' 300' 600'
HALF SIZE SCALE: 1" = 600'
FULL SIZE SCALE: 1" = 300'



HORIZONTAL AND VERTICAL CONTROL DATA

NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
△	"OLNEPORT" NGS MONUMENT	748,331.50	1,029,840.81	469.12
△	"OLNEPORT AZ MK" NGS MONUMENT	746,543.74	1,028,518.32	460.68

SURVEY NOTES

- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

CRITICAL POINTS

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND (MSL)	HEIGHT (AGL)	EQUIP. ELEV.
1	CONSTRUCTION EQUIPMENT	038° 43' 10.41"	-088° 10' 22.72"	464	15'	479
2	BARRICADE	038° 43' 08.20"	-088° 10' 30.26"	460	2'	462
3	BARRICADE	038° 43' 15.31"	-088° 10' 29.25"	465	2'	467
4	BARRICADE	038° 43' 16.42"	-088° 10' 28.10"	466	2'	468
5	BARRICADE	038° 43' 18.16"	-088° 10' 26.46"	467	2'	469
6	BARRICADE	038° 43' 20.28"	-088° 10' 24.53"	467	2'	469
7	BARRICADE	038° 43' 15.49"	-088° 10' 27.03"	468	2'	470

CRITICAL POINTS

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GROUND (MSL)	HEIGHT (AGL)	EQUIP. ELEV.
8	BARRICADE	038° 43' 22.88"	-088° 10' 46.03"	473	2'	475
9	BARRICADE	038° 43' 11.47"	-088° 10' 26.92"	463	2'	465
10	BARRICADE	038° 43' 08.36"	-088° 10' 29.24"	461	2'	463
11	BARRICADE	038° 43' 16.23"	-088° 10' 30.24"	466	2'	468
12	BARRICADE	038° 43' 18.85"	-088° 10' 23.93"	468	2'	470
13	BARRICADE	038° 43' 19.62"	-088° 10' 27.11"	470	2'	472

REHABILITATE AIRFIELD PAVEMENT MARKINGS

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

NO.	DATE	DESCRIPTION
DES	DWN	REV

ISSUE: NOVEMBER 16, 2018
PROJECT NO: 17A0006
CAD FILE: G-003-SFY.DWG
DESIGN BY: MJD 10/03/2018
DRAWN BY: MJD 10/03/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

CONSTRUCTION SAFETY PHASING PLAN - PHASE 2

CONSTRUCTION SAFETY PLAN PHASE 3 NOTES

- SCOPE** - PHASE 3 WILL INCLUDE REMOVAL AND REPLACEMENT OF EXISTING RUNWAY 11/29, TAXIWAY B, AND APRON MARKINGS AS DETAILED HEREIN.
- COORDINATION** - RUNWAY/TAXIWAY CLOSURES FOR EACH PHASE SHALL BE COORDINATED WITH THE AIRPORT MANAGER A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME. THE AIRPORT MANAGER HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.
- AIR OPERATIONS AREA (AOA)** - AT NO TIME MAY THE CONTRACTOR HAVE PERSONNEL, MATERIAL, OR EQUIPMENT WITHIN 125' OF RUNWAY CENTERLINES WHILE THE RUNWAY IS OPEN FOR AIRCRAFT OPERATIONS.
- CLOSURES** - THE CONTRACTOR SHALL CLOSE RUNWAYS TAXIWAYS ON A DAILY BASIS WHEN NO PERSONNEL OR EQUIPMENT ARE WITHIN THE AOA. NO OVERNIGHT CLOSURES WILL BE PERMITTED UNLESS AUTHORIZED BY THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL PROVIDE A WRITTEN STATEMENT OF HIS ACCEPTANCE AND ASSURANCE THAT HE WILL COMPLY WITH THE PROPOSED CONSTRUCTION SAFETY AND PHASING PLAN PRIOR TO BEING ISSUED THE NOTICE TO PROCEED. ANY CHANGES WILL REQUIRE APPROVAL BY THE RESIDENT ENGINEER AND AIRPORT MANAGER.
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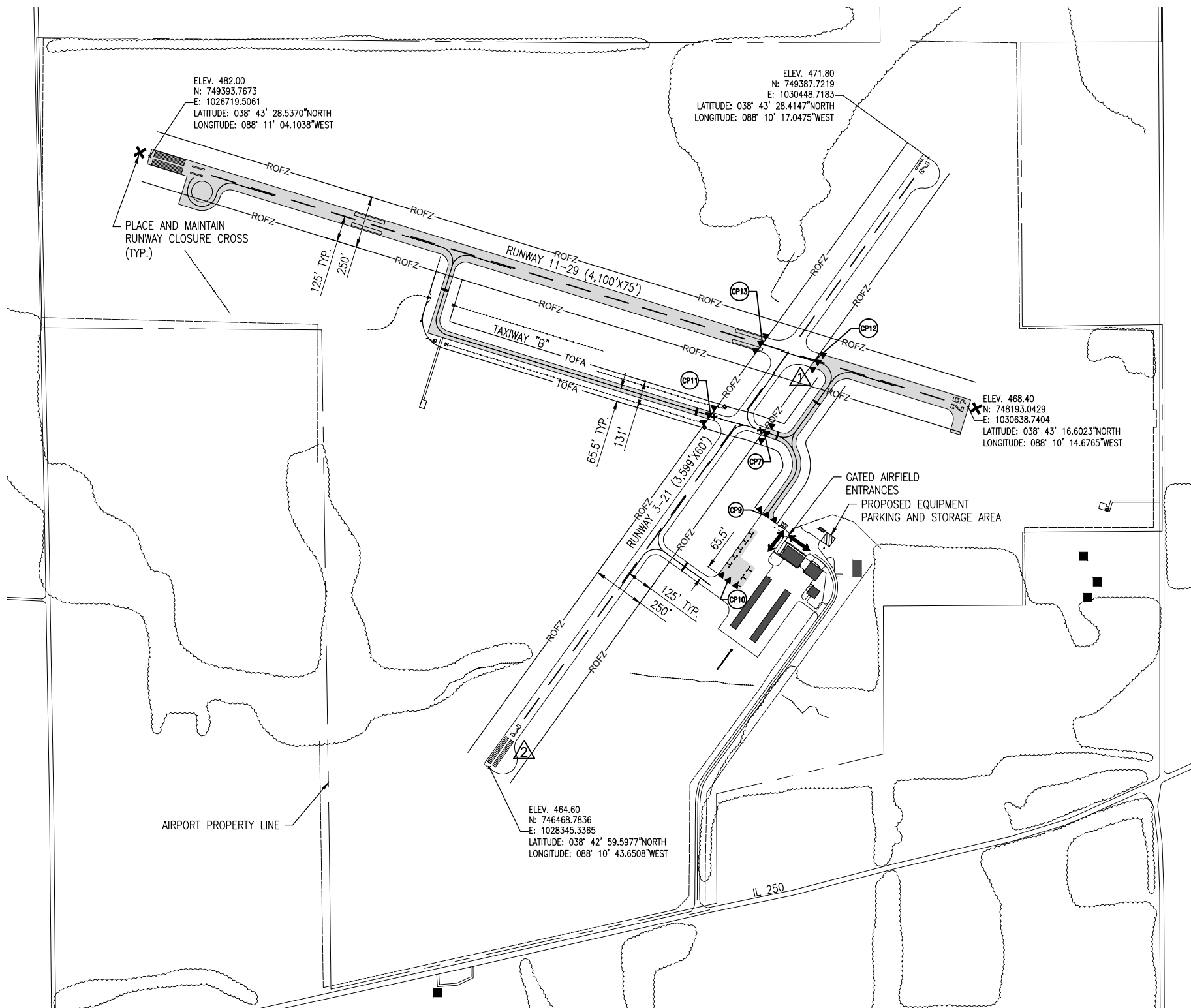
RUNWAY/TAXIWAY RW-OPENING PROCEDURES

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LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDINGS
- PROJECT WORK AREA
- STAGING AREA
- HAUL ROUTE/SECONDARY HAUL ROUTE
- BARRICADES
- CRITICAL POINT
- SURVEY CONTROL POINT

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HORIZONTAL AND VERTICAL CONTROL DATA				
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SURVEY NOTES

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**REHABILITATE AIRFIELD
PAVEMENT MARKINGS**

AIP No:
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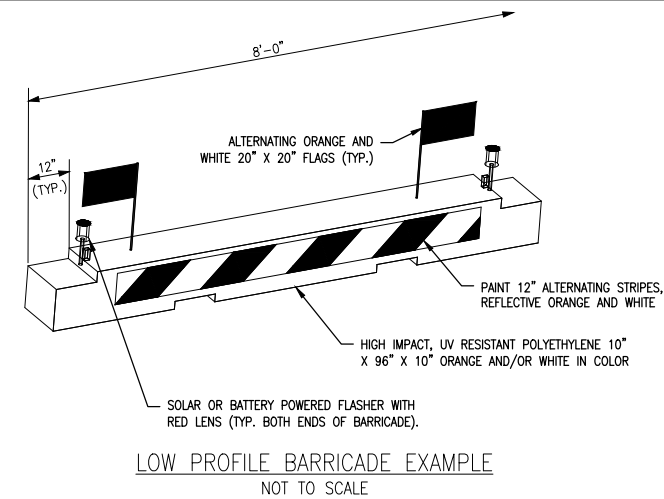
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SHEET TITLE

**CONSTRUCTION
SAFETY PHASING
PLAN - PHASE 3**

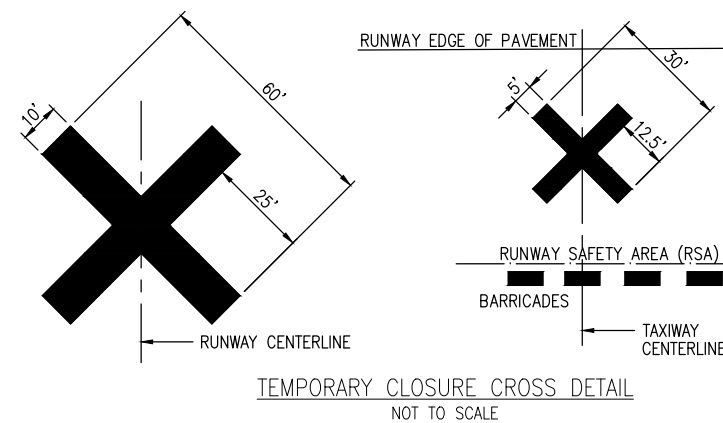
CONSTRUCTION SAFETY NOTES

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN.
- ALL COSTS ASSOCIATED WITH THE WORK AND MATERIALS INVOLVING CONSTRUCTION SAFETY & PHASING AS SHOWN OR DESCRIBED IN THE CONTRACT DOCUMENTS, OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN OR AIRPORT MANAGER THROUGHOUT THE PROJECT, SHALL BE INCIDENTAL TO THE CONTRACT.
- AIRFIELD SAFETY ASSURANCE** - AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FOR IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2F WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION OF ACCESS TO THE AIRFIELD ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- THE CONTRACTOR SHALL PROVIDE THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN WITH CONTACT INFORMATION FOR A PERSON ON CALL 24 HOURS A DAY FOR EMERGENCY MAINTENANCE OF BARRICADES AND HAZARD LIGHTING.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" (CURRENT EDITION).
- CLOSED AIRFIELD WORK AREAS, OPEN TRENCHES, AND OTHER HAZARDOUS AREAS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO PAVEMENT EDGE DROP-OFFS GREATER THAN 3" ARE ALLOWED TO REMAIN ON ANY ACTIVE AIRFIELD PAVEMENT AREA. TEMPORARY EARTH SHOULDERS SHALL BE PLACED IF AN INCOMPLETE AREA IS TO BE REOPENED TO AIRCRAFT TRAFFIC.
- AIRPORT SECURITY** - AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. NO CONTRACTOR PERSONNEL OR EQUIPMENT WILL BE PERMITTED OUTSIDE OF THE CONSTRUCTION LIMITS DEPICTED ON THESE PLANS. PERSONAL VEHICLES OF CONTRACTOR EMPLOYEES SHALL BE PARKED IN THE DESIGNATED STAGING AREA AND ARE NOT ALLOWED ACCESS ON THE SITE. THE CONTRACTOR WILL CLOSE AND LOCK THE GATE AT THE DESIGNATED CONSTRUCTION ENTRANCE AT THE END OF EACH DAY. ALL CONTRACTOR EMPLOYEES SHALL WEAR A GARMENT IDENTIFYING THEM AS CONSTRUCTION PERSONNEL.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- RADIO CONTROL** - THE CONTRACTOR WILL BE REQUIRED TO REMAIN CONSTANT TWO-WAY RADIO CONTACT WITH THE OLNEY-NOBLE AIRPORT UNICOM (122.8 MHZ) AT ALL TIMES WHILE HE HAS PERSONNEL OR EQUIPMENT ON THE AIRFIELD.



BARRICADE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT WITH A MAXIMUM SPACING OF 4' BETWEEN ENDS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED AS AN INCIDENTAL COST TO THE CONTRACT.
- UST BE IN ACCEPTABLE CONDITION.



TEMPORARY CLOSURE CROSS NOTES

- TEMPORARY CLOSED RUNWAY AND CLOSED TAXIWAY MARKINGS SHALL BE AVIATION YELLOW.
- TEMPORARY CLOSED RUNWAY MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
- TEMPORARY CLOSED RUNWAY MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- TEMPORARY CLOSED TAXIWAY MARKINGS SHALL BE PLACED WHEN AN ADJACENT RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE CLOSED TAXIWAY MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
- THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY OR TAXIWAY IS CLOSED AND REMOVED WHEN THE RUNWAY OR TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES.

REHABILITATE AIRFIELD PAVEMENT MARKINGS

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

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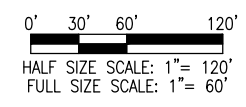
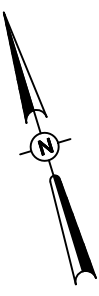
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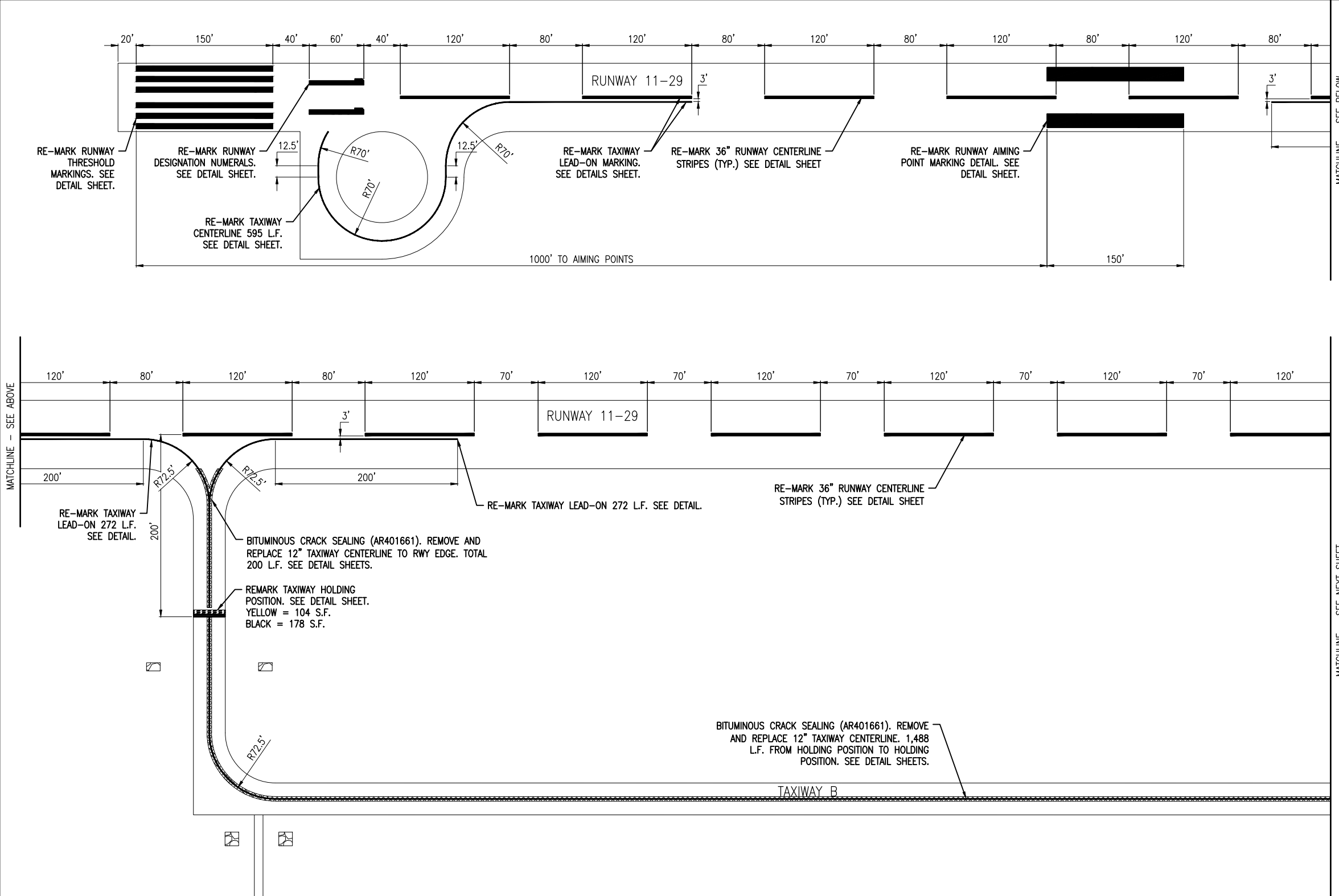
SHEET TITLE

CONSTRUCTION SAFETY PHASING NOTES AND DETAILS



LEGEND

- PROPOSED PAVEMENT MARKING
- AR620900 MARKING REMOVAL, PROPOSED AR401661 BITUMINOUS CRACK SEALING, AND PAVEMENT MARKING
- EXISTING MARKING TO BE PERMANENTLY REMOVED



620 – WATERBORNE PAVEMENT MARKING NOTES

1. PAVEMENT MARKINGS WILL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED IN THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED APRIL 1, 2012.
2. PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.
3. WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS.
4. GLASS BEADS SHALL BE DISTRIBUTED UPON THE MARKED AREAS AT THE LOCATIONS SHOWN ON THE PANS IMMEDIATELY AFTER THE SECOND APPLICATION OF THE PAINT. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
5. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.

PAVEMENT MARKING REMOVAL NOTES

1. THE AREAS OF EXISTING MARKING DESIGNATED FOR REMOVAL MAY ONLY BE REMOVED BY GRINDING, WATER, SHOT, OR SAND BLASTING WILL NOT BE ALLOWED.
2. ALL AREAS TO BE REMOVED ARE CALCULATED AT NOMINAL WIDTHS AS SHOWN. ANY ADDITIONAL AREAS THAT ARE REMOVED WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. AREAS TO BE PERMANENTLY REMOVED SHALL HAVE A REMOVAL AREA THAT ENCOMPASSES THE OLD MARKINGS. THE REMOVAL AREA WILL BE DOUBLE THE SQUARE FOOTAGE OF THE EXISTING MARKING AND FORM A RECTANGULAR PATTERN.

**REHABILITATE AIRFIELD
PAVEMENT MARKINGS**

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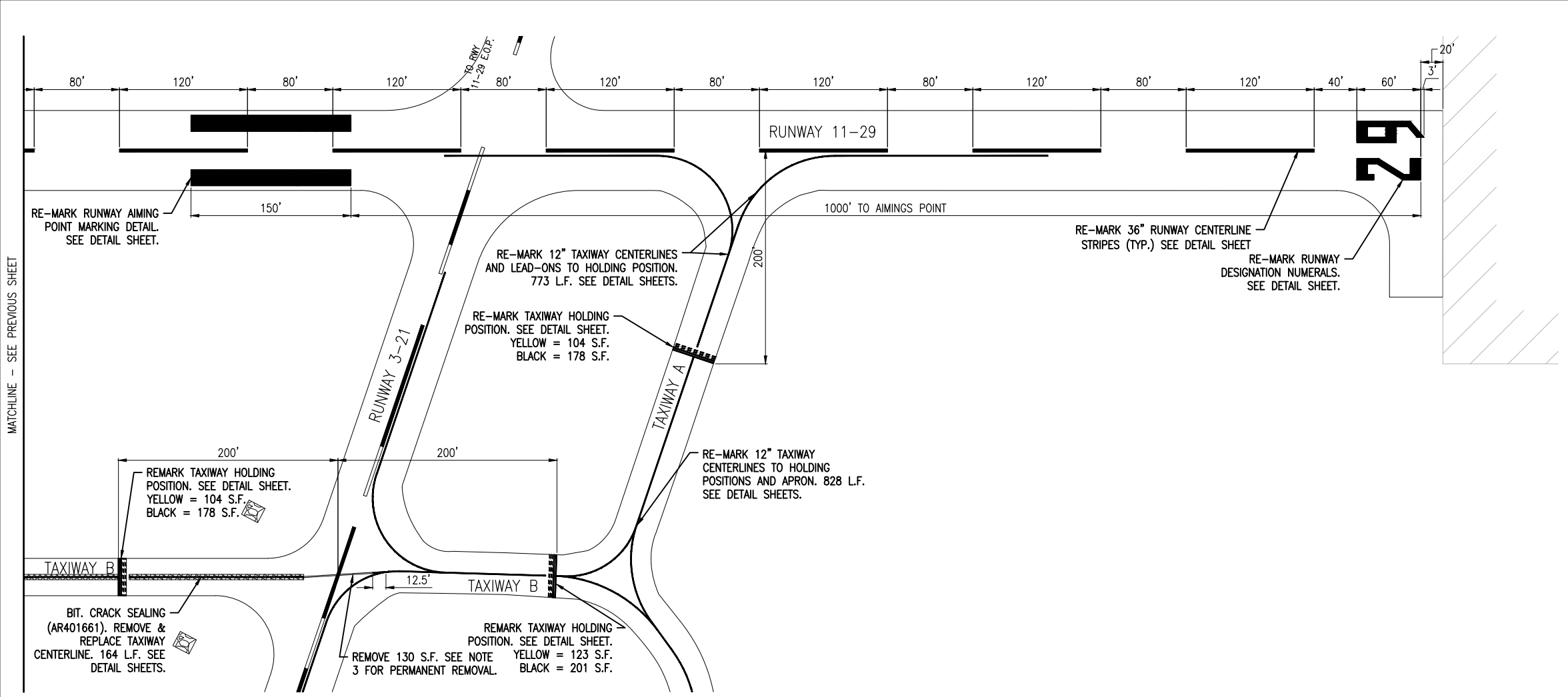
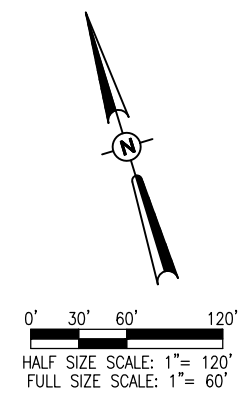
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DESIGN BY: JRH 07/03/2018
DRAWN BY: MJD 10/09/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

**PROPOSED MARKING
PLAN - RUNWAY 11-29
SHEET 1**

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LEGEND

- PROPOSED PAVEMENT MARKING
- AR620900 MARKING REMOVAL, PROPOSED AR401661 BITUMINOUS CRACK SEALING, AND PAVEMENT MARKING
- EXISTING MARKING TO BE PERMANENTLY REMOVED

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5. CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.

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2. ALL AREAS TO BE REMOVED ARE CALCULATED AT NOMINAL WIDTHS AS SHOWN. ANY ADDITIONAL AREAS THAT ARE REMOVED WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
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REHABILITATE AIRFIELD PAVEMENT MARKINGS

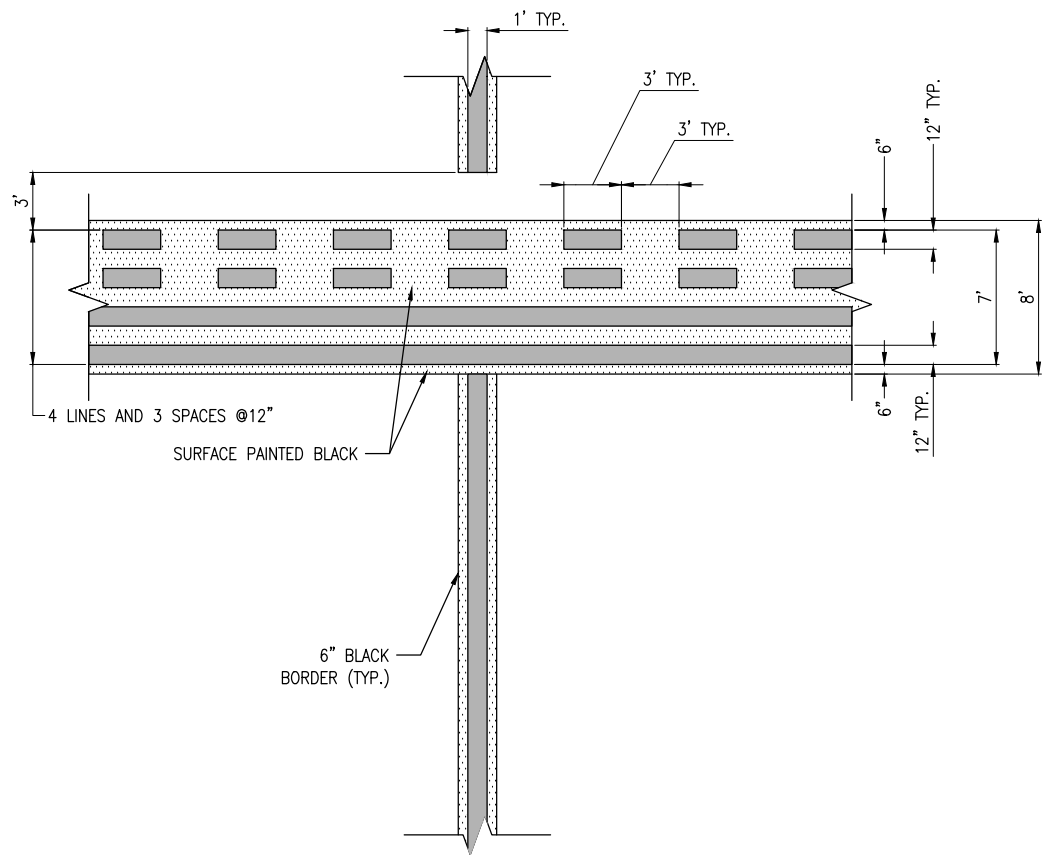
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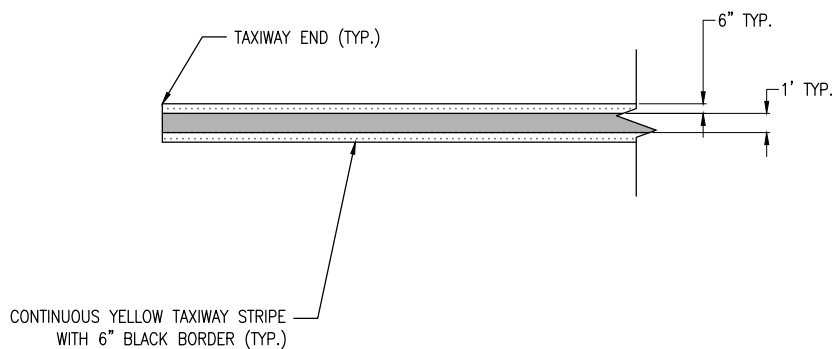
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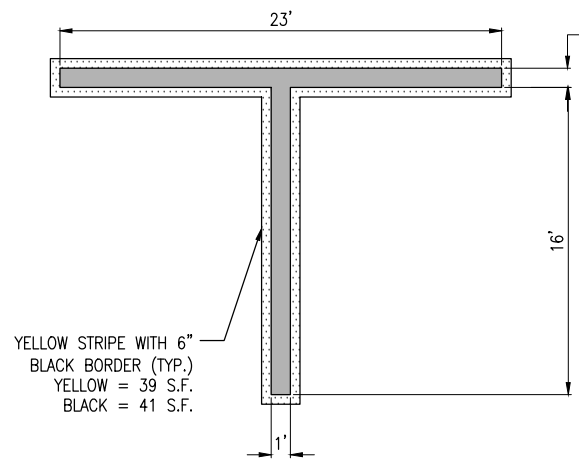
PROPOSED MARKING PLAN - RUNWAY 11-29 SHEET 2



HOLDING POSITION DETAIL
NOT TO SCALE



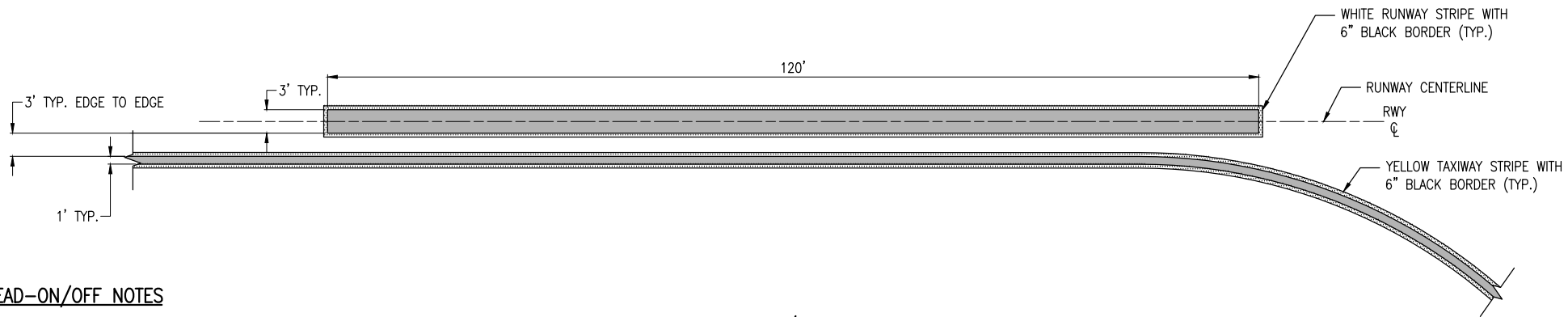
TAXIWAY CENTERLINE DETAIL
NOT TO SCALE



APRON TYPICAL TIEDOWN LAYOUT
NOT TO SCALE

TOTAL (8 TIEDOWNS):
YELLOW = 312 S.F.
BLACK = 328 S.F.

YELLOW STRIPE WITH 6"
BLACK BORDER (TYP.)
YELLOW = 39 S.F.
BLACK = 41 S.F.



TAXIWAY CENTERLINE LEAD-ON/OFF DETAIL
NOT TO SCALE

RUNWAY LEAD-ON/OFF NOTES

1. THE TAXIWAY CENTERLINE CONTINUES ONTO THE RUNWAY AND EXTENDS PARALLEL TO THE RUNWAY CENTERLINE FOR A DISTANCE OF 200 FEET BEYOND THE POINT OF TANGENCY AND IS 3 FEET FROM THE RUNWAY CENTERLINE AND TAXIWAY LINE AS MEASURED EDGE TO EDGE.

**REHABILITATE AIRFIELD
PAVEMENT MARKINGS**

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2018

PROJECT NO: 17A0006

CAD FILE: C-501-DET.DWG

DESIGN BY: MJD 10/03/2018

DRAWN BY: MJD 10/03/2018

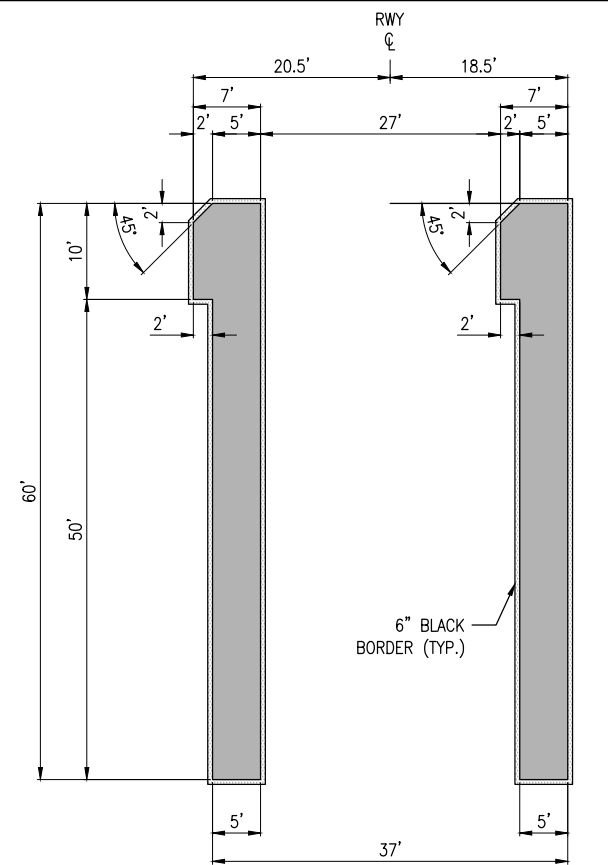
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

TAXIWAY AND
APRON DETAILS

RUNWAY DESIGNATION MARKING NOTES:

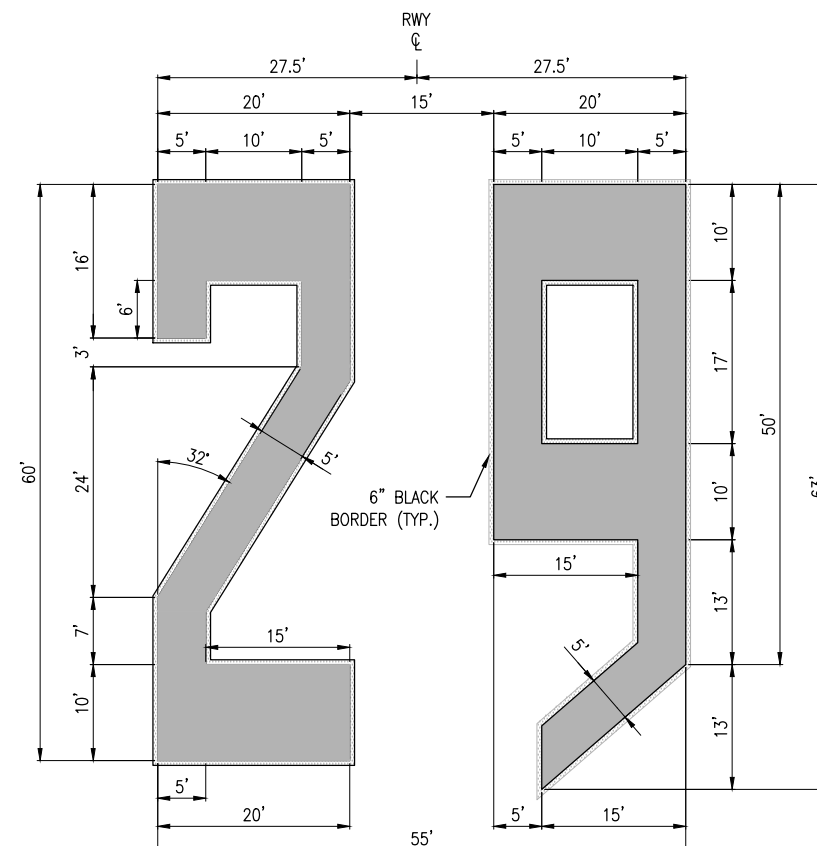
1. ALL NUMERAL MARKING WILL BE WHITE IN COLOR WITH A 6" BLACK OUTLINE.
2. ALL NUMERALS EXCEPT THE NUMBER 11 AS SHOWN ARE TO BE HORIZONTALLY SPACED 15 FEET APART.
3. SINGLE DESIGNATIONS ARE CENTERED ON THE RUNWAY PAVEMENT CENTERLINE. FOR DOUBLE DESIGNATIONS, THE CENTER OF THE OUTER EDGES OF THE TWO NUMERALS IS CENTERED ON THE RUNWAY PAVEMENT CENTERLINE.



WHITE = 318 S.F.
BLACK = 68 S.F.

WHITE = 318 S.F.
BLACK = 68 S.F.

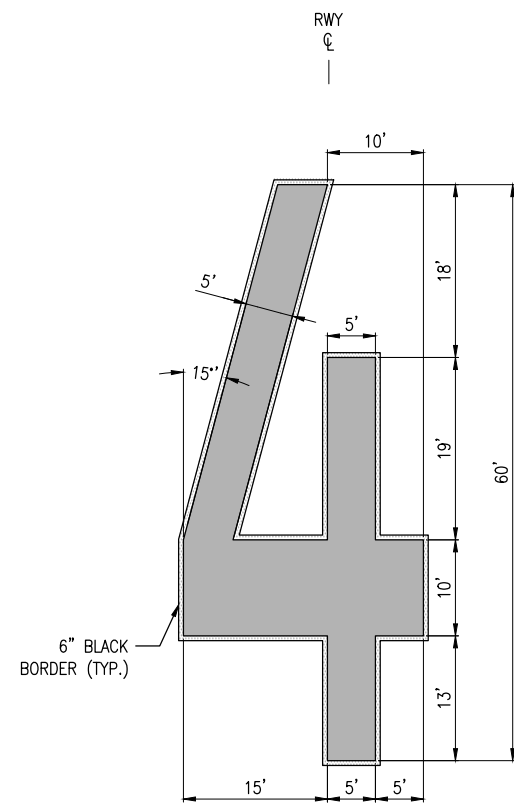
RUNWAY 11 DESIGNATION MARKING
NOT TO SCALE



WHITE = 652 S.F.
BLACK = 107 S.F.

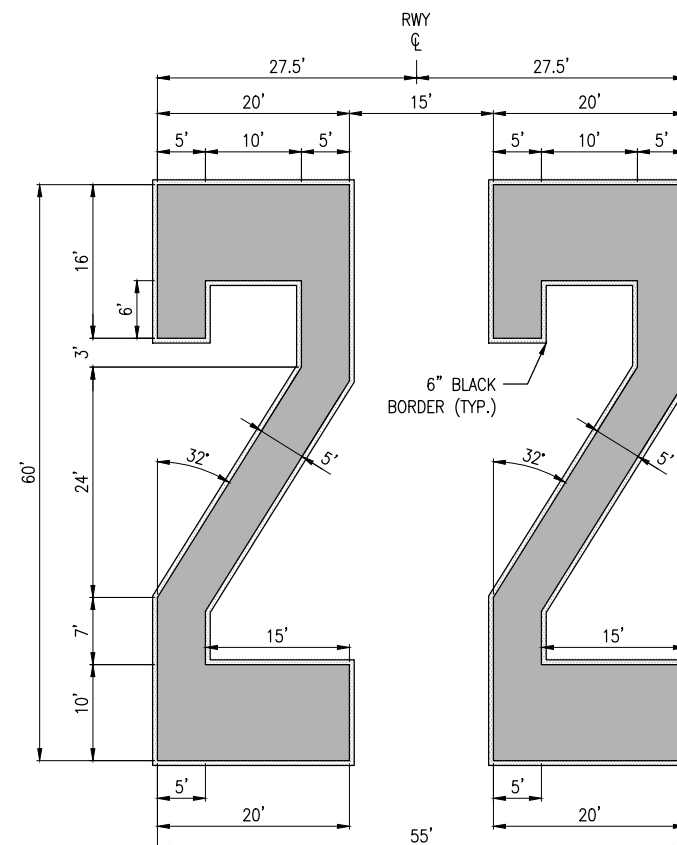
WHITE = 712 S.F.
BLACK = 114 S.F.

RUNWAY 29 DESIGNATION MARKING
NOT TO SCALE



WHITE = 602 S.F.
BLACK = 107 S.F.

RUNWAY 4 DESIGNATION MARKING
NOT TO SCALE



WHITE = 652 S.F.
BLACK = 107 S.F.

WHITE = 652 S.F.
BLACK = 107 S.F.

RUNWAY 22 DESIGNATION MARKING
NOT TO SCALE

**REHABILITATE AIRFIELD
PAVEMENT MARKINGS**

AIP No:
3-17-SBGP-133-139
IDA No: OLY- 4577
Contract No. OL017

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2018

PROJECT NO: 17A0006

CAD FILE: C-501-DET.DWG

DESIGN BY: MJD 10/05/2018

DRAWN BY: MJD 10/05/2018

REVIEWED BY: KBS 11/14/2018

SHEET TITLE

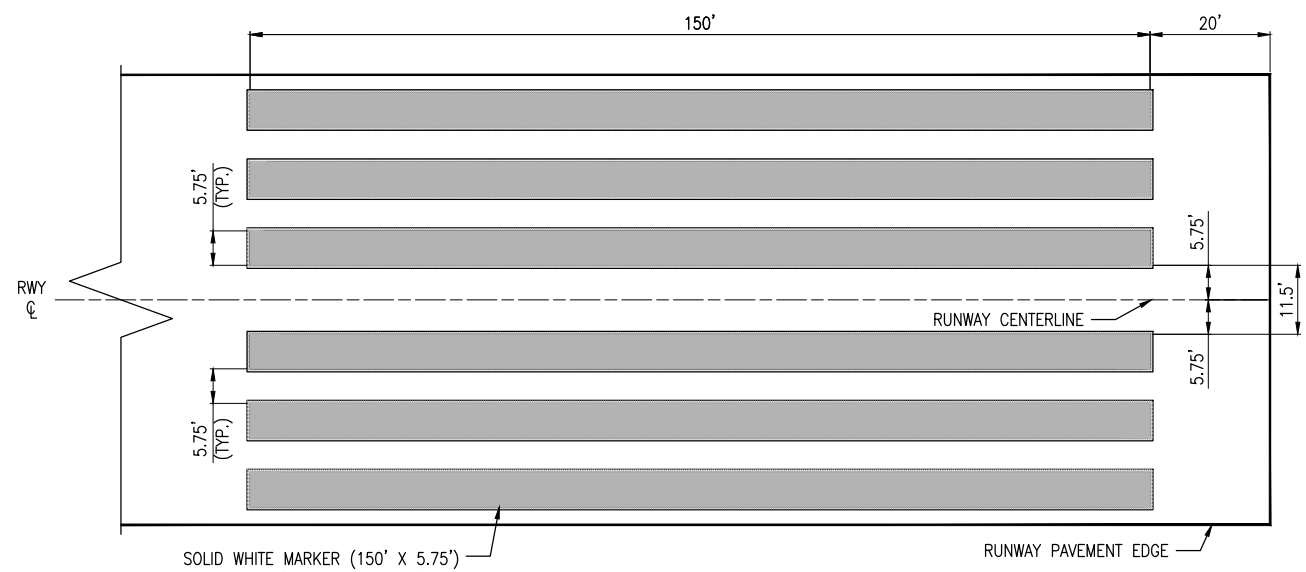
**RUNWAY
DESIGNATION
NUMERAL DETAILS**

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2018
PROJECT NO: 17A0006
CAD FILE: C-501-DET.DWG
DESIGN BY: MJD 10/03/2018
DRAWN BY: MJD 10/03/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

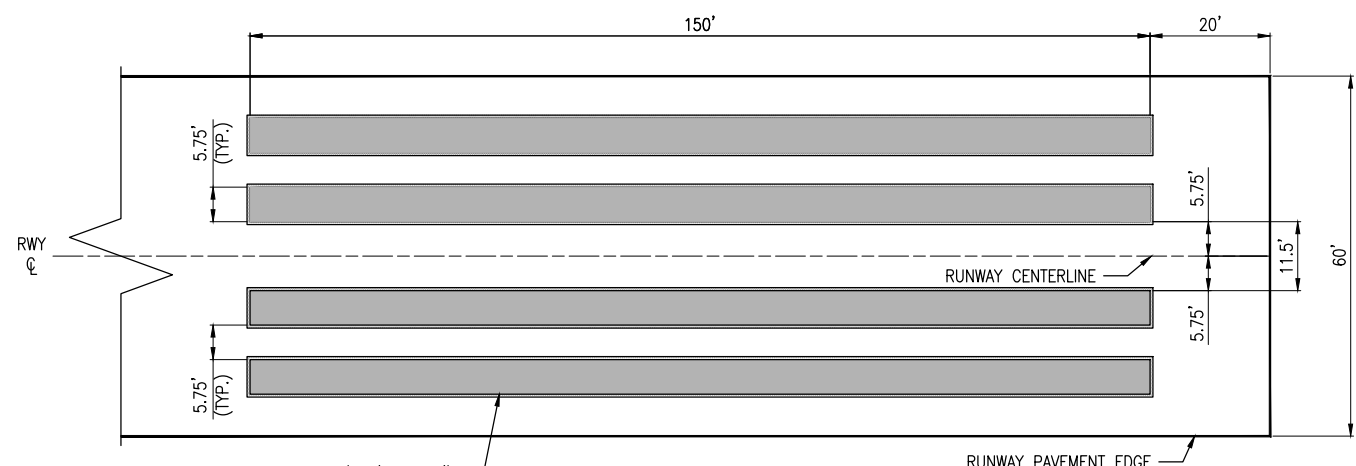
RUNWAY MARKING
DETAILS



TOTAL:
WHITE = 5,178 S.F.
BLACK = 942 S.F.

SOLID WHITE MARKER (150' X 5.75')
WITH 6" BLACK BORDER (TYP.)
WHITE = 863 S.F.
BLACK = 157 S.F.

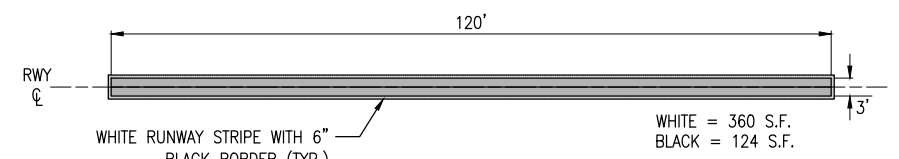
RUNWAY 11 THRESHOLD MARKING
NOT TO SCALE



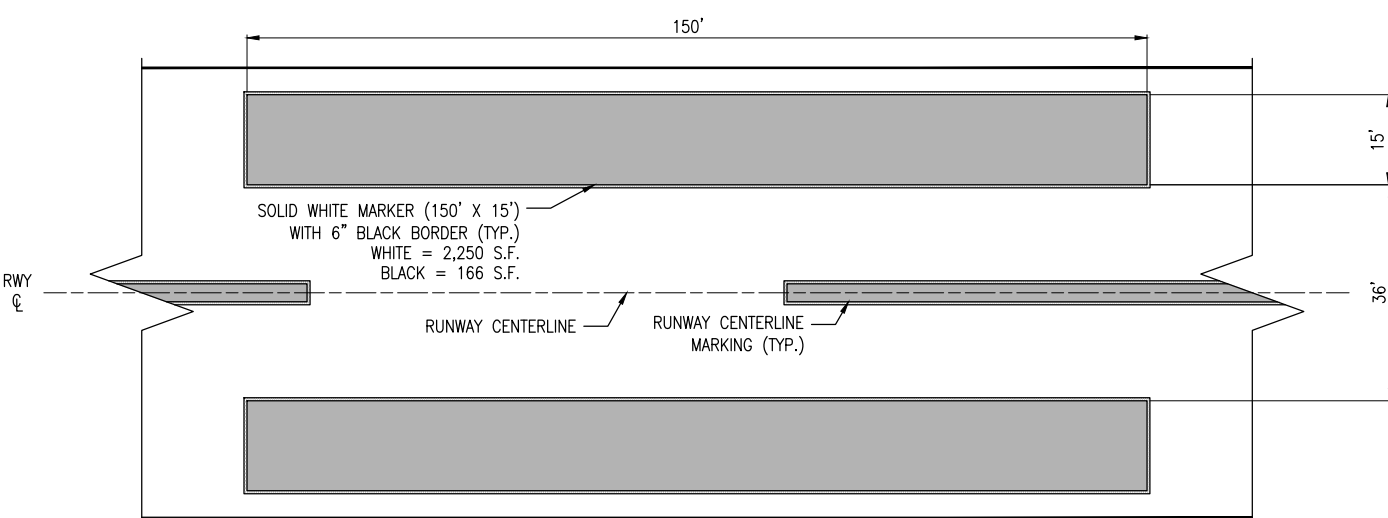
TOTAL:
WHITE = 3,452 S.F.
BLACK = 628 S.F.

SOLID WHITE MARKER (150' X 5.75')
WITH 6" BLACK BORDER (TYP.)
WHITE = 863 S.F.
BLACK = 157 S.F.

RUNWAY 4 THRESHOLD MARKING
NOT TO SCALE



TYPICAL RUNWAY CENTERLINE MARKING
NOT TO SCALE



TOTAL (4 AIMING POINT MARKERS):
WHITE = 9,000 S.F.
BLACK = 664 S.F.

SOLID WHITE MARKER (150' X 15')
WITH 6" BLACK BORDER (TYP.)
WHITE = 2,250 S.F.
BLACK = 166 S.F.

RUNWAY AIMING POINT MARKING (RUNWAY 11/29)
NOT TO SCALE

RUNWAY AIMING POINT MARKING NOTES

1. THE LATERAL SPACING BETWEEN THE INNER SIDES OF THE RECTANGULAR BARS ARE CENTERED ON THE RUNWAY CENTERLINE.

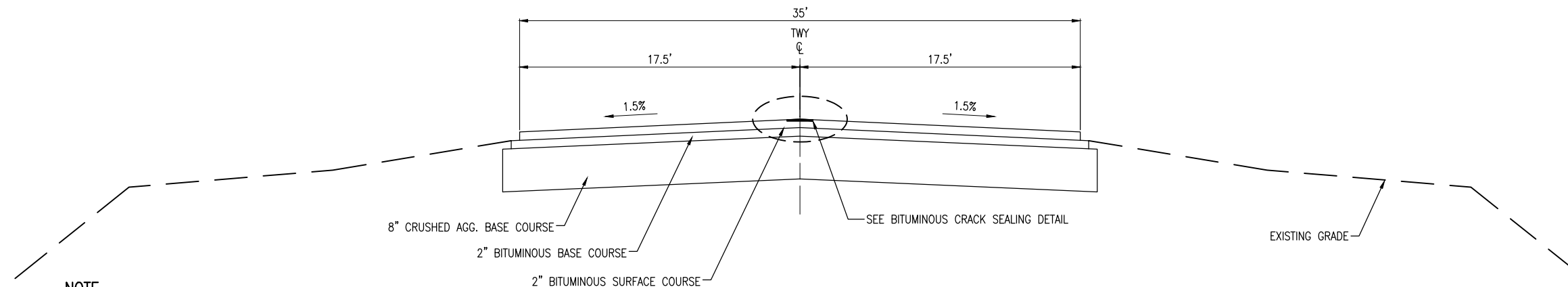
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2018
PROJECT NO: 17A0006
CAD FILE: C-501-DET.DWG
DESIGN BY: KBS 11/13/2018
DRAWN BY: MJD 11/13/2018
REVIEWED BY: KBS 11/14/2018

SHEET TITLE

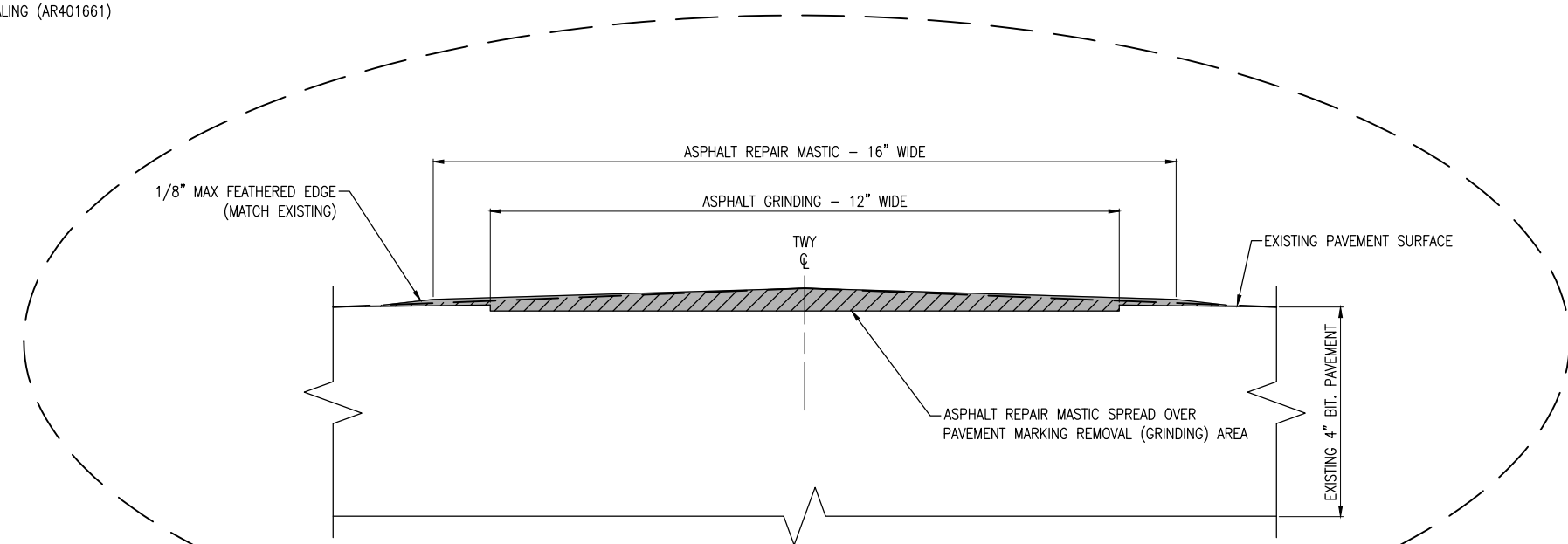
BITUMINOUS CRACK
SEALING DETAIL



TAXIWAY B TYPICAL SECTION
NOT TO SCALE

NOTE

1. PAYMENT FOR THE CRACK SEALING REPAIR AREA AND ASSOCIATED WORK WILL BE MADE UNDER AR401661 BITUMINOUS CRACK SEALING. MEASUREMENTS WILL BE TAKEN PER LINEAL FOOT.
2. PAYMENT FOR GRINDING WILL BE MADE UNDER AR620900 PAVEMENT MARKING REMOVAL AS INDICATED ON THE PLANS.
3. ANY ADDITIONAL GRINDING OR CLEANING WILL BE INCIDENTAL TO THE CRACK SEALING (AR401661)



BITUMINOUS CRACK SEALING DETAIL
NOT TO SCALE

BITUMINOUS CRACK SEALING NOTES

1. THE MATERIAL SHALL ONLY BE APPLIED TO A CLEAN, SOUND, AND DRY SURFACE.
2. THE AREA SHALL BE BLOWN WITH COMPRESSED AIR, SWEEPED WITH A STIFF WIRE BRISTLE BROOM, ROUTED, OR GRINDED IN A MANNER NECESSARY TO EFFECTIVELY PLACE THE MATERIAL AND REPAIR THE EXISTING PAVEMENT.
3. SAND, WATER, OR SHOT BLASTING WILL NOT BE PERMITTED.
4. THE MATERIAL SHALL BE LEVELED SMOOTH TO MATCH EXISTING PAVEMENT.
5. THE MANUFACTURER'S RECOMMENDATION FOR PRIMER AND DETACKING SHALL GOVERN.
6. MARKING SHALL NOT BE PLACED UNTIL THE MATERIAL IS FULLY CURED.