

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2990	06-00078-00-RS	DUPAGE	14	1
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO: 83879

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL-AID URBAN PROJECT FAU RTE 2990 (STRUCKMAN BOULEVARD) FROM ILLINOIS RT 59 TO SOUTH BARTLETT ROAD RESURFACING (LAPP PROJECTS) SECTION NO. 06-00078-00-RS PROJECT NO: M-8003(696) JOB NO. C-91-015-07 VILLAGE OF BARTLETT, DuPAGE COUNTY

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### TRAFFIC DATA

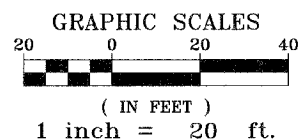
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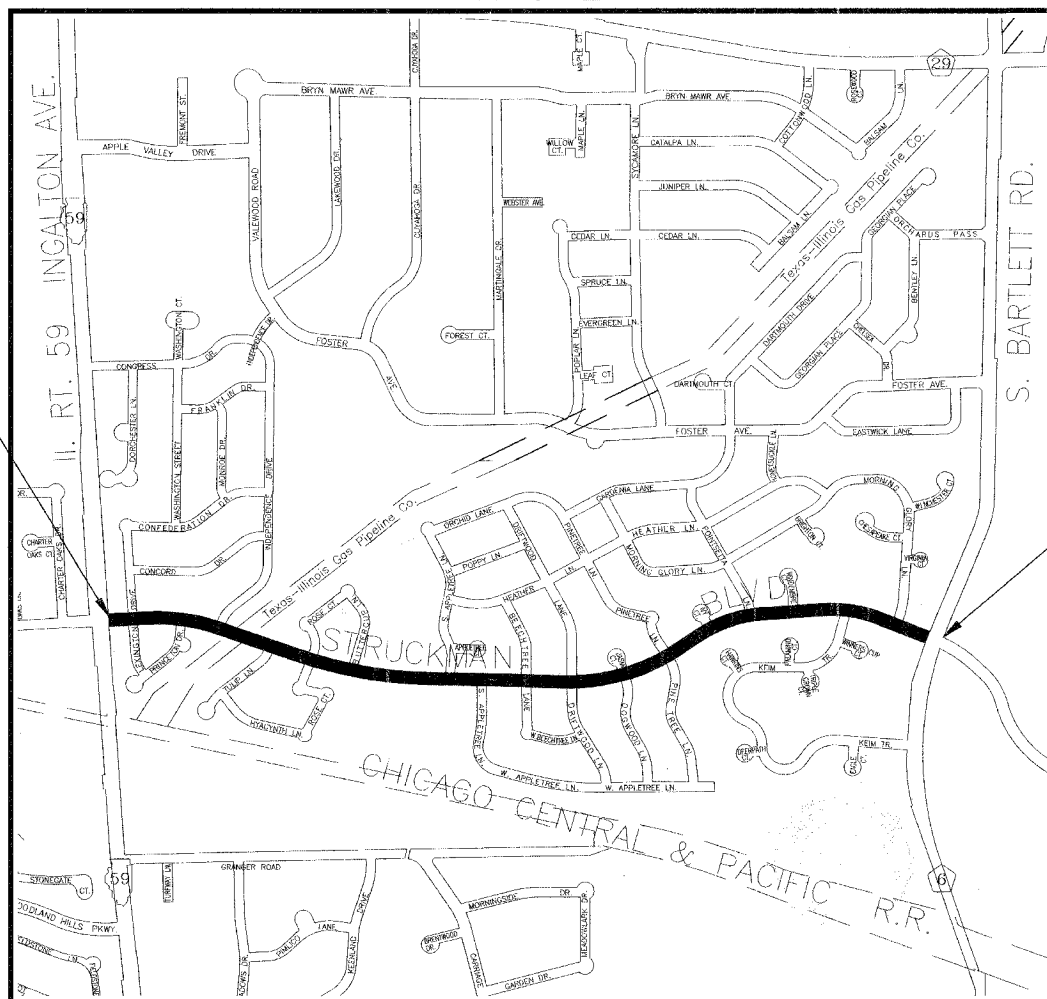
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MINOR ARTERIAL ROAD

### FOR JOINT UTILITY INFORMATION

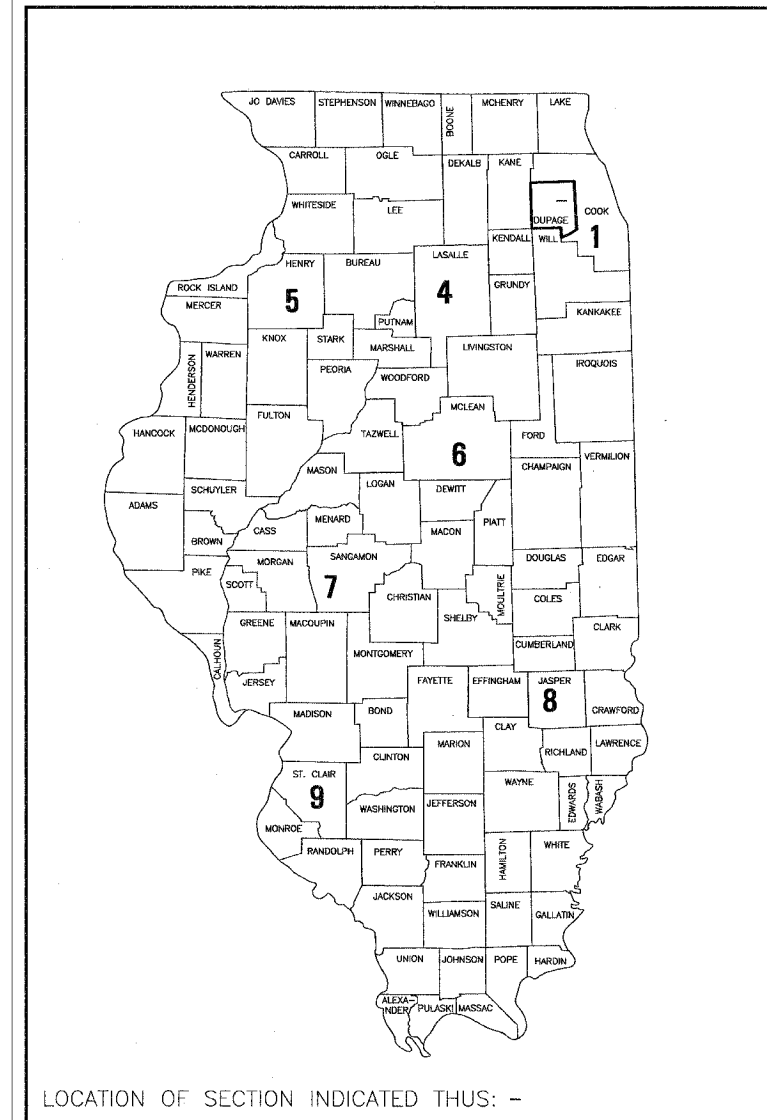


IMPROVEMENT BEGINS  
STA. 10+04  
STRUCKMAN BOULEVARD



SCALE: NONE

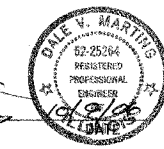
IMPROVEMENT ENDS  
STA. 67+78  
STRUCKMAN BOULEVARD



LOCATION OF SECTION INDICATED THUS: -

LENGTH OF IMPROVEMENT  
GROSS AND NET PROJECT LENGTH (STRUCKMAN BOULEVARD) = 5774 FEET = 1.094 MILES

DALE V. MARTING, P.E.  
No. 062-25264



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	<i>October 9 2006</i> <i>[Signature]</i> DIRECTOR OF PUBLIC WORKS
PASSED	<i>October 31 2006</i> <i>[Signature]</i> DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
Released for Final Review	<i>November 1 2006</i> <i>[Signature]</i> DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

**PAVIA-MARTING & Co.**  
910 WEST LAKE STREET  
ROSELLE, IL 60172-3352  
(630) 329-8000 FAX (630) 894-6910  
Design Firm Professional Registration #184802376

PMC PROJECT NO. 2812  
PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS

IDOT FEDERAL AID DESIGN ENGINEER: ABIGAIL WILGREEN (847) 705-4233

10/9/2006 11:44 AM F:\2812\Code\2812 - COVER SHEET.DWG

CONTRACT NO: 83879

**GENERAL NOTES**

- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2007, THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND "THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS" ADOPTED MARCH 1, 2005.
- THE CONTRACTOR SHALL ARRANGE WITH THE VARIOUS UTILITY COMPANIES FOR THE LOCATION AND ANY NECESSARY ADJUSTING OF THE PRIVATELY OWNED OVERHEAD OR UNDERGROUND UTILITIES WITHIN THE LIMITS OF CONSTRUCTION.
- THE CONTRACTOR SHALL LIMIT CONSTRUCTION ACTIVITIES AND STORAGE OF EQUIPMENT OR MATERIALS TO THE PUBLIC R.O.W. ANY DAMAGE TO PRIVATE PROPERTY OR FENCES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE TO OWNERS SATISFACTION.
- WHERE SECTION, SUB-SECTION MONUMENTS OR BENCH MARKS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BY THE CONTRACTOR BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY IRONS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SAND BAG SHALL BE USED ACROSS EACH BOTTOM RAIL.
- ALL AGGREGATE FOR SUB-BASE AND BASE COURSE SHALL BE CRUSHED.
- ALL ITEMS TO BE REMOVED (DRIVEWAYS, PAVEMENT, CURB & GUTTER, ETC.) SHALL BE SAW CUT FULL DEPTH. THE COST FOR THE SAW CUT SHALL BE INCLUDED IN THE COST OF THE RESPECTIVE PAY ITEM.
- CONTRACTION JOINTS SHALL BE SAWED A MINIMUM OF 3" DEEP IN THE COMBINATION CURB AND GUTTER AT FIFTEEN (15) FOOT INTERVALS. PREFORMED EXPANSION JOINTS (3/4" OR 1") SHALL BE REQUIRED AT EVERY PC AND PT AND AT FORTY-FIVE (45) FOOT INTERVALS.
- AT A POINT FIVE (5) FEET ON BOTH SIDES OF EACH DRAINAGE STRUCTURE CONSTRUCTED IN THE COMBINATION CURB AND GUTTER LINE, THE CONTRACTOR SHALL INSTALL A ONE (1) INCH EXPANSION JOINT CONFORMING TO THE CROSS SECTION OF THE CURB AND GUTTER INVOLVED. THE CURB AND GUTTER SHALL BE WIDENED AND THICKENED AT EACH DRAINAGE STRUCTURE LOCATED IN THE GUTTER LINE IN ACCORDANCE WITH THE DETAILS SHOWN ON THE PLANS.
- NO CONCRETE SHALL BE PLACED UNTIL ALL THE FORMS HAVE BEEN INSPECTED FOR LINE, GRADE AND SUBGRADE CONDITIONS BY THE ENGINEER. INSPECTIONS SHALL BE ARRANGED AT LEAST TWENTY-FOUR (24) HOURS IN ADVANCE OF ANY CONCRETE PLACEMENT.
- THE CONTRACTOR SHALL DEPRESS CURBS AT ALL DRIVEWAYS AND CROSSWALK LOCATIONS IN ACCORDANCE WITH THE DETAILS SHOWN ON THE PLANS.
- THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A APPLICATION RATE OF 0.375 GAL./SQ. YD.
- THE TRANSITION FROM PROPOSED CURB AND GUTTER SECTION TO EXISTING CURB AND GUTTER SECTION WILL BE WITHIN SIX (6) FEET AND WILL BE PAID FOR AS THE PROPOSED CURB AND GUTTER OF THE TYPE SPECIFIED.
- THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS BITUMINOUS LIFTS.
- THE CONTRACTOR SHALL TAKE ALL STEPS NECESSARY TO PROTECT ALL WET CONCRETE. ANY COMBINATION CURB AND GUTTER THAT IS DEFACED SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- DUST SHALL BE CONTROLLED BY THE UNIFORM APPLICATION OF SPRINKLED WATER IN CONFORMANCE WITH THE SPECIAL PROVISIONS, BUT SHALL BE APPLIED ONLY WHEN DIRECTED BY ENGINEER. NO ADDITIONAL COMPENSATION SHALL BE PAID FOR THIS WORK.
- PORTABLE TOILETS SHALL BE PROVIDED ON THE JOB SITE IN CONFORMANCE WITH SECTION 107.08 OF THE STANDARD SPECIFICATION. THEY WILL BE REQUIRED DURING ALL PHASES OF CONSTRUCTION. THE COST FOR THE PORTABLE TOILETS SHALL BE INCIDENTAL TO THE CONTRACT.
- TEMPORARY RAMPS SHALL BE PROVIDED AT EACH DRIVEWAY, SIDEWALK AND BIKEPATH. THE RAMPS SHALL BE CONSTRUCTED IN SUCH A MANNER AS TO PREVENT DAMAGE TO VEHICULAR TRAFFIC AND AS PER DETAIL 1 SHOWN ON THE PLANS. THE COST OF THE TEMPORARY RAMPS SHALL BE INCIDENTAL TO THE CONTRACT.
- ANY REMOVAL AND DISPOSAL OF EARTH OR SUBGRADE REQUIRED TO MEET THE MINIMUM PATCH OR DEPTH, AS DIRECTED BY THE ENGINEER, SHALL BE INCLUDED IN THE COST OF THE PATCH ITEM.
- ANY REMOVAL AND DISPOSAL OF UNSUITABLE OR UNSTABLE SUBGRADE, AS DIRECTED BY THE ENGINEER, SHALL BE INCLUDED IN THE COST OF THE PATCH ITEM.
- THE QUANTITY FOR CLASS D PATCHES, TYPE I, II, III, IV, 6 INCH, PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT, BITUMINOUS DRIVEWAY REMOVAL AND REPLACEMENT, AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL NOT EXCEED THE AMOUNT LISTED IN THE SUMMARY OF QUANTITIES.

**LEGEND**

	<b>EXISTING</b>	<b>PROPOSED</b>
CATCH BASIN	○	
STORM MANHOLE	⊙	
INLET	□	
SANITARY MANHOLE	⊙	
VALVE VAULT	□	
VALVE BOX	●	
FIRE HYDRANT	⊙	
BUFFALO BOX	⊙	
POWER POLE	⊙	
LIGHT POLE	⊙	
LIGHT POLE WITH MAST ARM	⊙	
DECIDUOUS TREE	⊙	
SPLICE BOX	⊙	
TRAFFIC SIGN	+	
STREET SIGN	+	
CONTROL POINT	△	
INLET & PIPE PROTECTION	— —	
SILT FENCE	— —	
GAS MAIN	— —	
WATER MAIN	— —	
ELECTRIC LINE	— —	
O/H ELECTRIC LINE	— —	
CABLE	— —	
O/H CABLE	— —	
TELEPHONE	— —	
O/H TELEPHONE	— —	
PROPERTY LINE	— —	
STORM SEWER	— —	
SANITARY SEWER	— —	
CONTOUR	— —	
COMBINATION CONC. CURB & GUTTER	— —	=====
STRUCTURE TO BE ADJUSTED		[ADJ]

**SUMMARY OF QUANTITIES**

CODE	PAY ITEM	UNIT	QUANTITY	CONSTRUCTION CODE I-000
21101625	TOP SOIL FURNISH AND PLACE 6"	SQ YD	480	
25200650	SODDING, SALT TOLERANT (SPECIAL)	SQ YD	480	
28000510	INLET FILTERS	EACH	29	
40600100	BITUMINOUS MATERIAL (PRIME COAT)	GALLON	10,125	
40600300	AGGREGATE (PRIME COAT)	TON	41	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	710	
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N50	TON	3,998	
42001300	PROTECTIVE COAT	SQ YD	630	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5"	SQ FT	1,024	
42400800	DETECTABLE WARNING	SQ FT	876	
44000162	HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"	SQ YD	26,875	
44001700	COMBINATION CONCRETE CURB AND GUTTER, REMOVAL AND REPLACEMENT	FOOT	1,730	
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	32	
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	161	
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	147	
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	2790	
44300200	STRIP REFLECTIVE CRACK CONTROL	FOOT	5,700	
60251000	CATCH BASIN TO BE ADJUSTED WITH NEW T-6 FRAME AND GRATE	EACH	17	
60262510	INLETS TO BE ADJUSTED WITH NEW FRAME AND GRATE (SPECIAL)	EACH	15	
60300305	FRAME AND LIDS TO BE ADJUSTED	EACH	3	
67100100	MOBILIZATION	L SUM	1	
70102620	TRAFFIC CONTROL AND PROTECTION STANDARD 701501	L SUM	1	
70102635	TRAFFIC CONTROL AND PROTECTION STANDARD 701701	L SUM	1	
70102640	TRAFFIC CONTROL AND PROTECTION STANDARD 701801	L SUM	1	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	1,420	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	245	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	9,140	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,610	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	210	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	392	
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	52	
44000600	SIDEWALK REMOVAL	SQ FT	1,900	
XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	335	
XX004238	BITUMINOUS DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	220	
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,165	

\* SPECIALTY ITEMS

**SUGGESTED CONSTRUCTION SEQUENCE**

- INSTALL INFORMATION SIGNS AND INFORMATIONAL BOARDS.
- PLACE TRAFFIC CONTROL SIGNS.
- GRIND EXISTING PAVEMENT AS PER PLANS.
- INSTALL TEMPORARY RAMPS AND SHORT TERM PAVEMENT MARKINGS.
- PATCH PAVEMENT, CURB AND GUTTER REMOVAL AND REPLACEMENT, AND SIDE WALK REMOVAL AND REPLACEMENT.
- REMOVE WORK ZONE PAVEMENT MARKINGS AND INSTALL SHORT TERM PAVEMENT MARKINGS.
- PLACE LEVEL BINDER AS PER PLANS.
- PLACE BINDER COURSE AS PER PLANS AND SPECIFICATIONS.
- INSTALL TEMPORARY RAMPS AND SHORT TERM PAVEMENT MARKINGS.
- PLACE SURFACE COURSE AS PER PLANS AND SPECIFICATIONS.
- INSTALL SHORT TERM PAVEMENT MARKINGS.
- INSTALL THERMOPLASTIC PAVEMENT MARKINGS.
- REMOVE TRAFFIC CONTROL WARNING SIGNS.

NOTE: THIS IS A SUGGESTED CONSTRUCTION SEQUENCE ONLY. THE CONTRACTOR CAN DEViate AT HIS OR HER OWN EXPENSE INCLUDING MOBILIZATION COSTS.

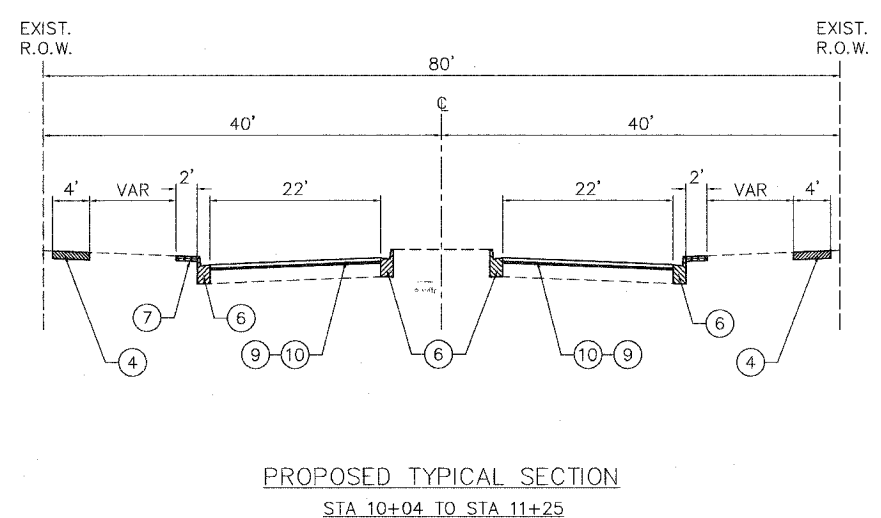
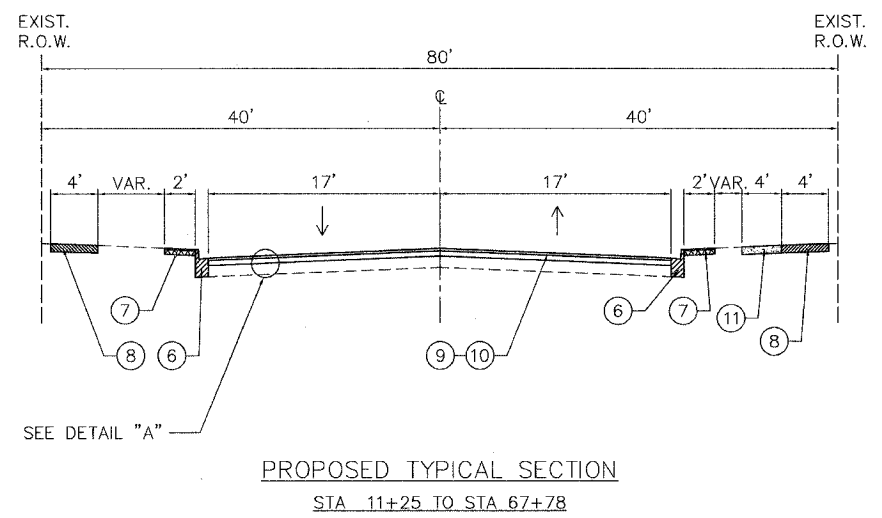
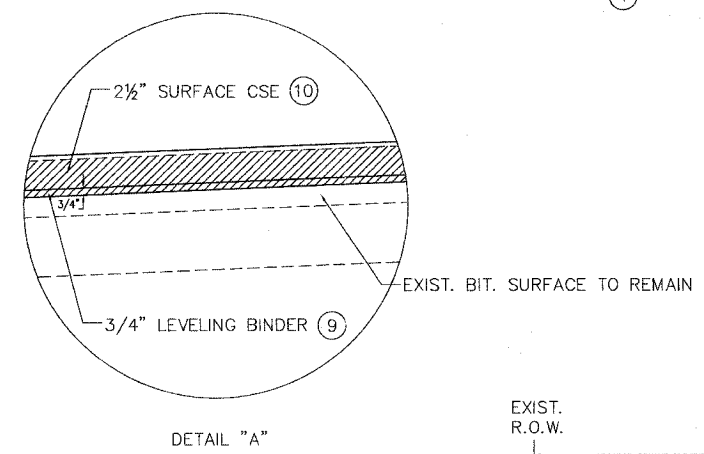
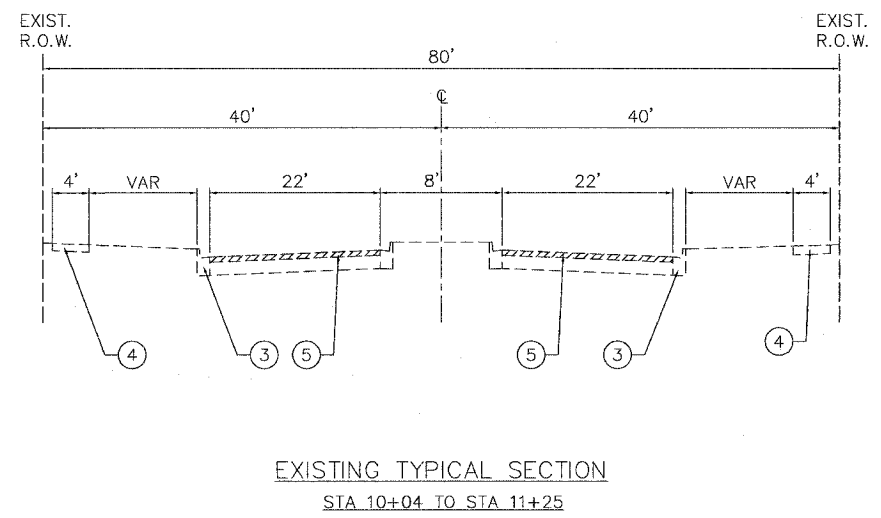
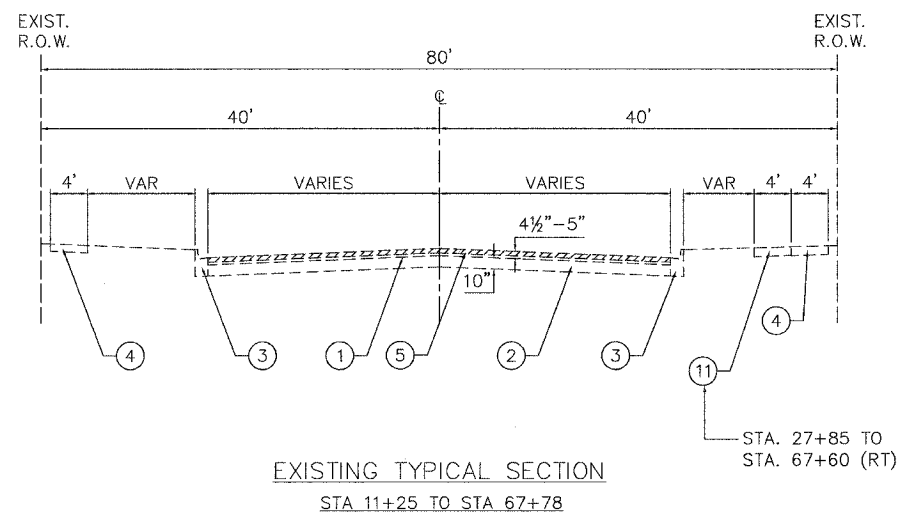
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CHECKED BY:	VP
APPROVED BY:	DVM

**PAVIA-MARTING & Co.**  
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 (630) 529-8000 FAX (630) 894-4910  
 Design Firm Professional Registration #154002376

TITLE: **STRUCKMAN BOULEVARD RESURFACING IMPROVEMENTS**  
 LEGEND, GENERAL NOTES AND SUMMARY OF QUANTITIES  
 SECTION NO: 06-00078-00-RS

SCALE:	NONE
DATE:	7/28/06
JOB NO:	2812
SHEET	2 OF 14



- LEGEND**
- EXISTING BITUMINOUS SURFACE 4 1/2" TO 5"
  - AGGREGATE BASE
  - EXISTING COMBINATION CURB AND GUTTER TYPE B-6.12
  - EXISTING P.C.C. CONCRETE SIDEWALK
  - PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 3 1/4"
  - PROPOSED CURB AND GUTTER REMOVAL & REPLACEMENT OF TYPE B-6.12 AT DESIGNATED LOCATION BY ENGINEER
  - PROPOSED SODDING SALT TOLERANT (SPECIAL) AT LOCATION C&G REPLACEMENT
  - PROPOSED SIDEWALK REMOVAL AND PORTLAND CEMENT CONCRETE SIDEWALK 5" AT LOCATION DESIGNATED BY THE ENGINEER BUT NOT TO EXCEED THE QUANTITY LISTED IN THE SUMMARY OF QUANTITIES.
  - PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N-50 3/4"
  - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
  - EXISTING BITUMINOUS SIDE WALK.

**MIX REQUIREMENT TABLE**

ITEM	AC TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N 50	PG 64-22	4%±50 GYR
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N 50	SBS/SBR PG 76-28/-22	4%±50 GYR
CLASS D PATCHES 6", TYPE I, II, III & IV (BINDER IL 19)	PG 64-22/58-22	4%±70 GYR

\* THE UNIT HEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 Lbs/Sq Yd/in THICKNESS.

\*\* WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER MIX SHALL BE PG-58-22.

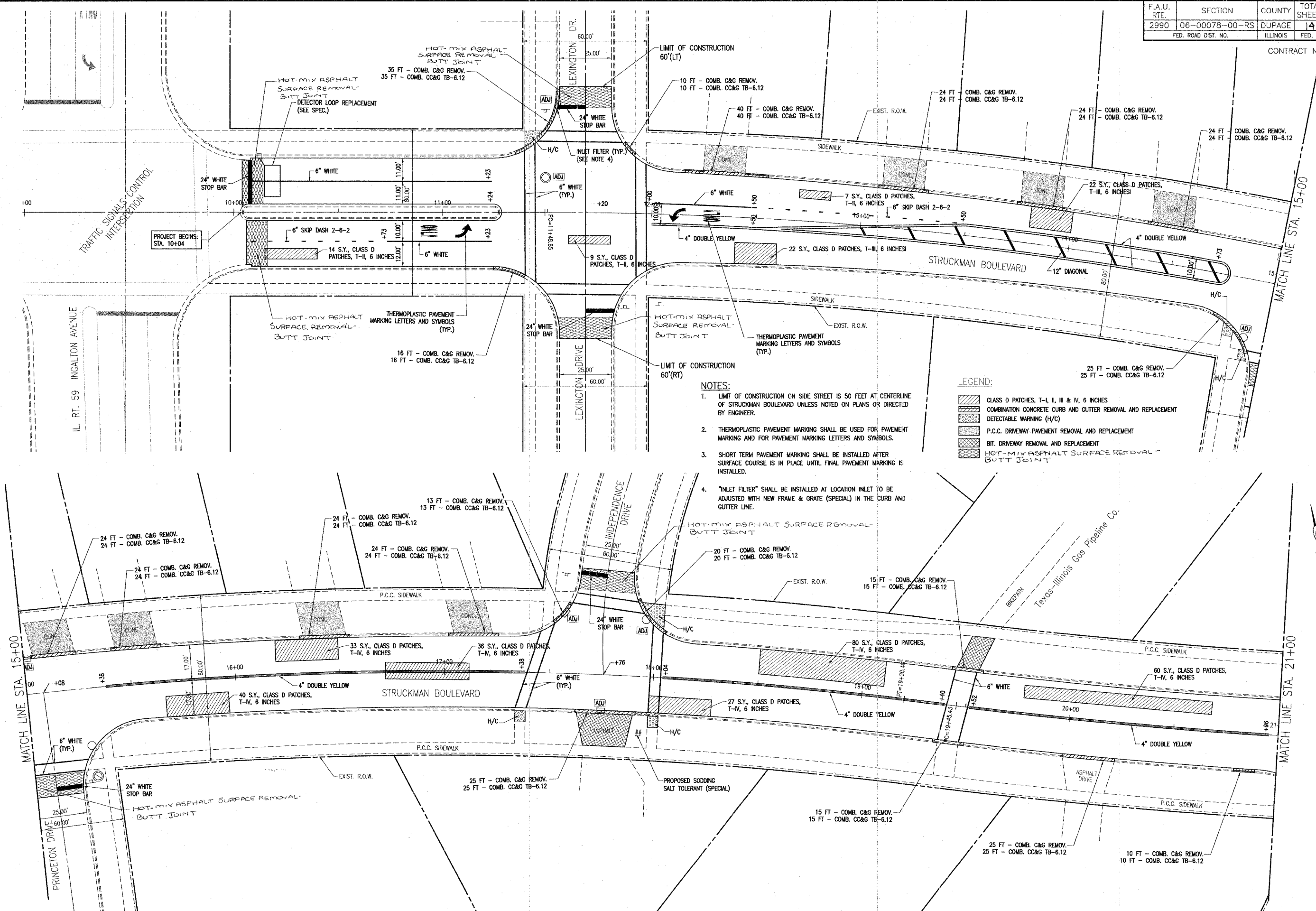
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10-30-2006	SMM			REVISED PER IDOT COMMENTS			

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**TITLE**  
 STRUCKMAN BOULEVARD RESURFACING IMPROVEMENTS  
 TYPICAL SECTIONS  
 SECTION NO: 06-00078-00-RS

SCALE:	NONE
DATE:	7/28/06
JOB NO:	2812
SHEET:	3 OF 3



- NOTES:**
- LIMIT OF CONSTRUCTION ON SIDE STREET IS 50 FEET AT CENTERLINE OF STRUCKMAN BOULEVARD UNLESS NOTED ON PLANS OR DIRECTED BY ENGINEER.
  - THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR PAVEMENT MARKING AND FOR PAVEMENT MARKING LETTERS AND SYMBOLS.
  - SHORT TERM PAVEMENT MARKING SHALL BE INSTALLED AFTER SURFACE COURSE IS IN PLACE UNTIL FINAL PAVEMENT MARKING IS INSTALLED.
  - "INLET FILTER" SHALL BE INSTALLED AT LOCATION INLET TO BE ADJUSTED WITH NEW FRAME & GRATE (SPECIAL) IN THE CURB AND GUTTER LINE.

- LEGEND:**
- CLASS D PATCHES, T-I, II, III & IV, 6 INCHES
  - COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
  - DETECTABLE WARNING (H/C)
  - P.C.C. DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
  - BIT. DRIVEWAY REMOVAL AND REPLACEMENT
  - HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT

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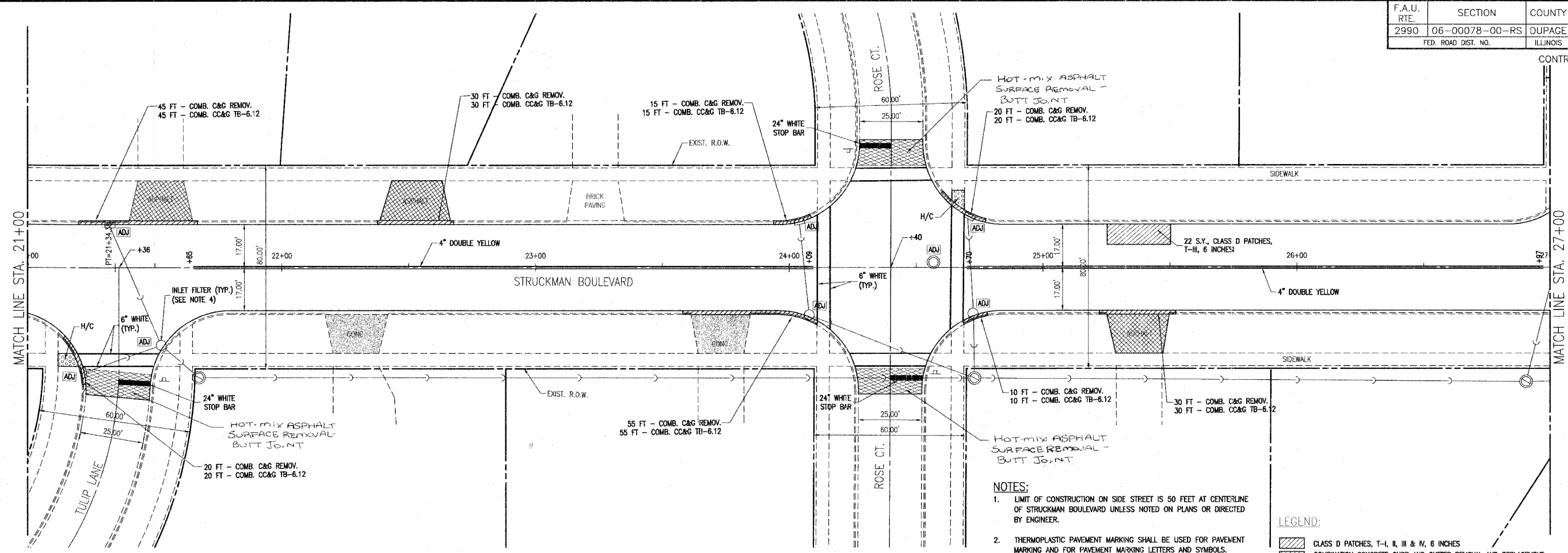
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**STRUCKMAN BLVD. RESURFACING IMPROVEMENTS**  
 PLAN STA. 9+00 TO STA. 21+00  
 SECTION NO: 06-00078-00-RS

SCALE: 1"=20'  
 DATE: 7/28/06  
 JOB NO: 2812  
 SHEET 4 OF 14

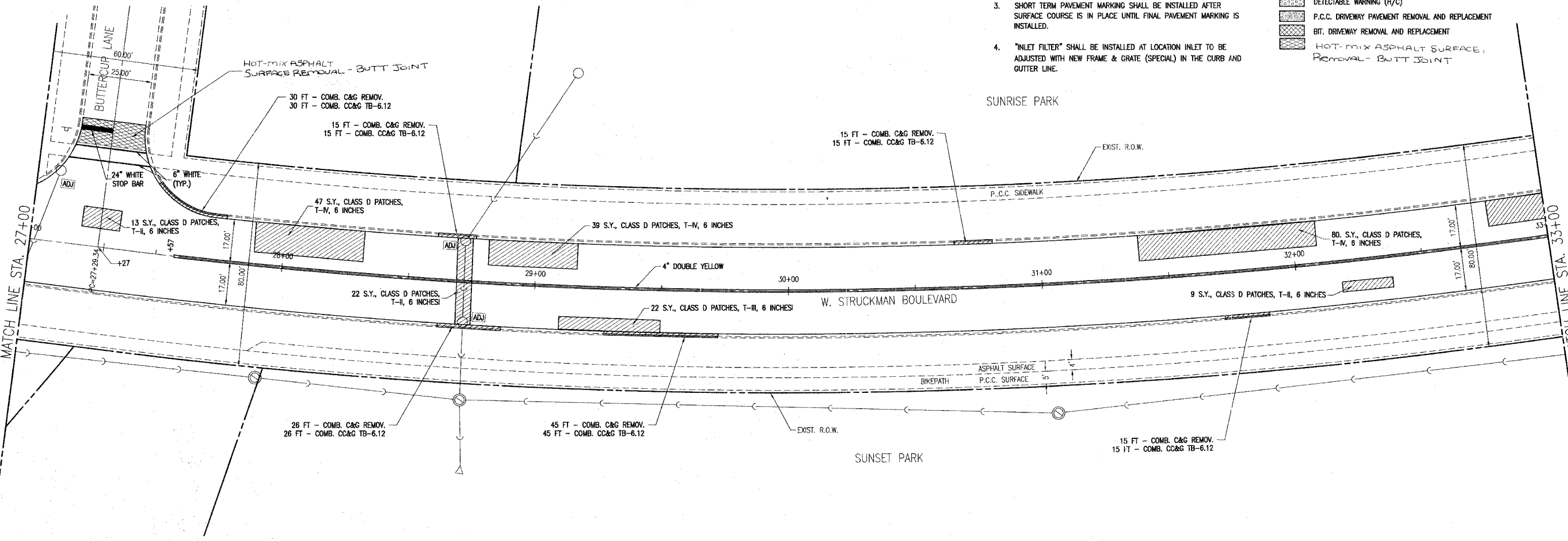
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  - P.C.C. DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
  - BIT. DRIVEWAY REMOVAL AND REPLACEMENT
  - HOT-MIX ASPHALT SURFACE, Removal - BUTT JOINT



REVISIONS:	DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
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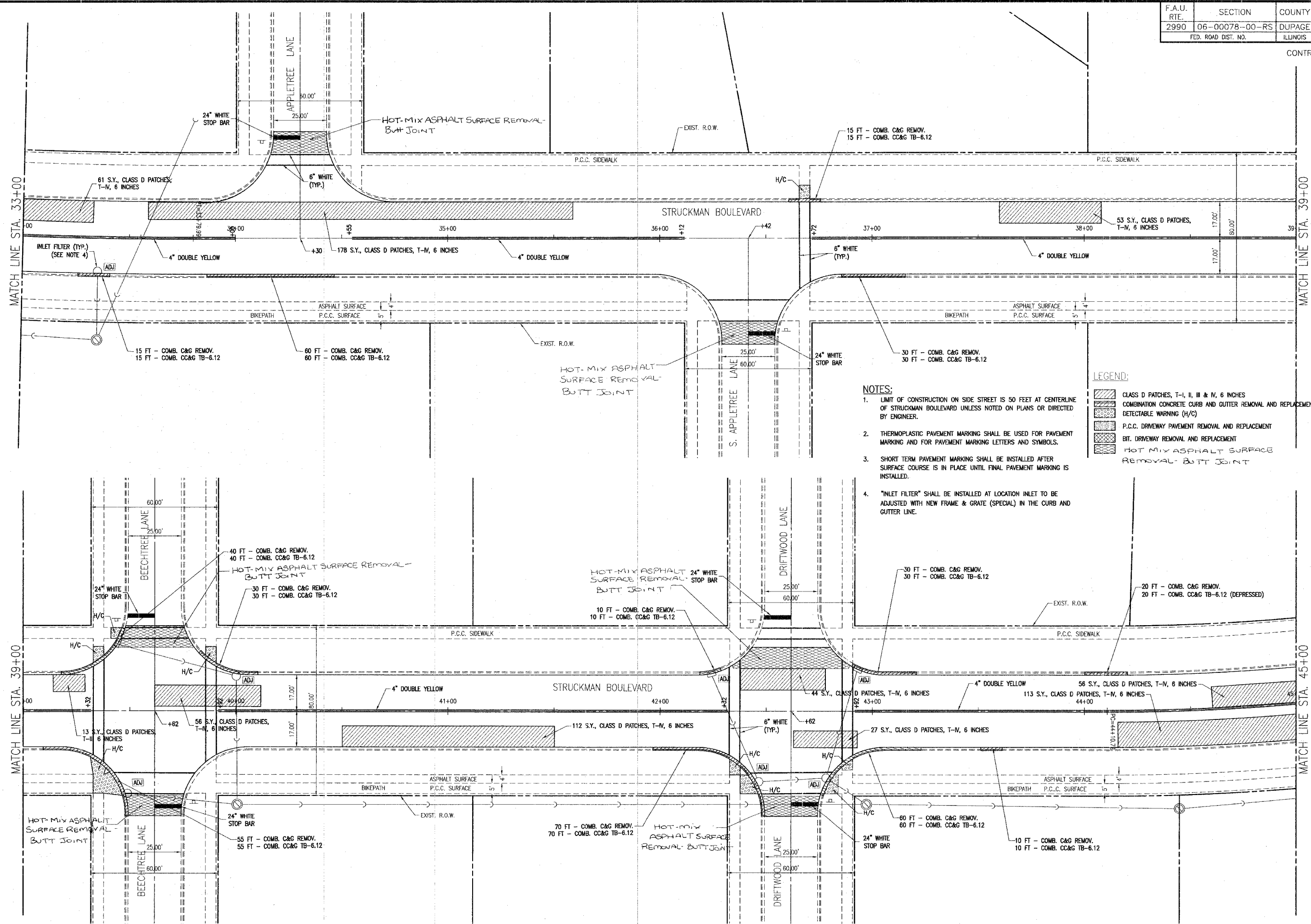
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**TITLE:** STRUCKMAN BLVD. RESURFACING IMPROVEMENTS  
 PLAN STA. 21+00 TO STA. 33+00  
 SECTION NO: 06-00078-00-RS

SCALE: 1"=20'  
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 SHEET 5 OF 5

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**NOTES:**

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**LEGEND:**

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- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- DETECTABLE WARNING (H/C)
- P.C.C. DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- BIT. DRIVEWAY REMOVAL AND REPLACEMENT
- HOT MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

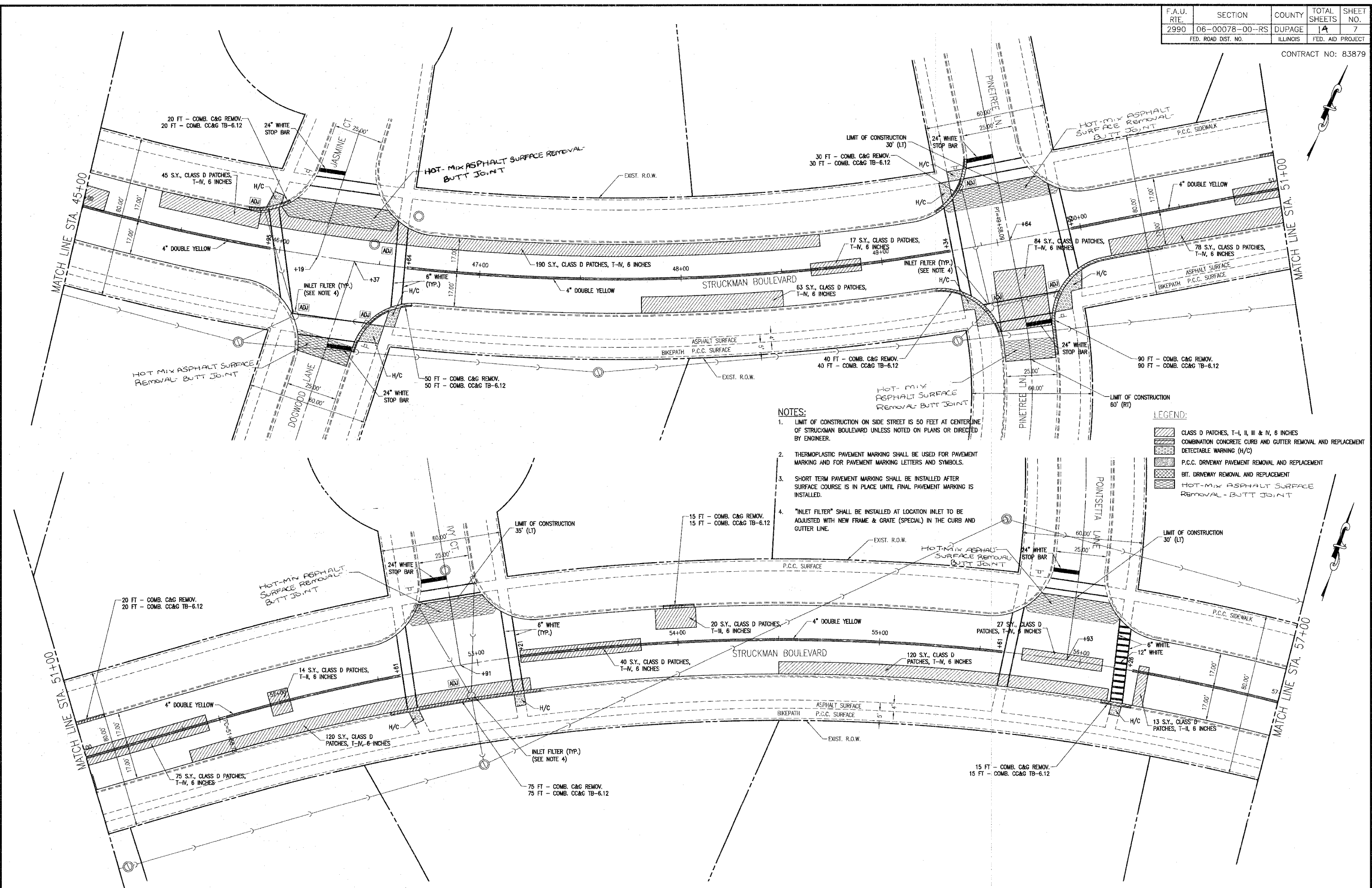
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9-27-2006	SMM			REVISED PER IDOT COMMENTS			
10-06-2006	SMM			REVISED PER IDOT COMMENTS			

PLOTTED BY:	SMM
CHECKED BY:	VP
DRAWN BY:	SMM
CHECKED BY:	VP
APPROVED BY:	DMM

**PAVIA-MARTING & Co.**  
 910 WEST LAKE STREET  
 ROSELLE, IL 60172-3352  
 (630) 529-8000 FAX (630) 894-4910  
 Design Firm: Professional Registration #184992376

**TITLE:** STRUCKMAN BLVD. RESURFACING IMPROVEMENTS  
 PLAN STA. 33+00 TO STA. 45+00  
 SECTION NO: 06-00078-00-RS

SCALE:	1"=20'
DATE:	7/28/06
JOB NO:	2812
SHEET:	6 OF 6



**NOTES:**

1. LIMIT OF CONSTRUCTION ON SIDE STREET IS 50 FEET AT CENTERLINE OF STRUCKMAN BOULEVARD UNLESS NOTED ON PLANS OR DIRECTED BY ENGINEER.
2. THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR PAVEMENT MARKING AND FOR PAVEMENT MARKING LETTERS AND SYMBOLS.
3. SHORT TERM PAVEMENT MARKING SHALL BE INSTALLED AFTER SURFACE COURSE IS IN PLACE UNTIL FINAL PAVEMENT MARKING IS INSTALLED.
4. "INLET FILTER" SHALL BE INSTALLED AT LOCATION INLET TO BE ADJUSTED WITH NEW FRAME & GRATE (SPECIAL) IN THE CURB AND GUTTER LINE.

**LEGEND:**

- CLASS D PATCHES, T-I, II, III & IV, 6 INCHES
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- DETECTABLE WARNING (H/C)
- P.C.C. DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- BIT. DRIVEWAY REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

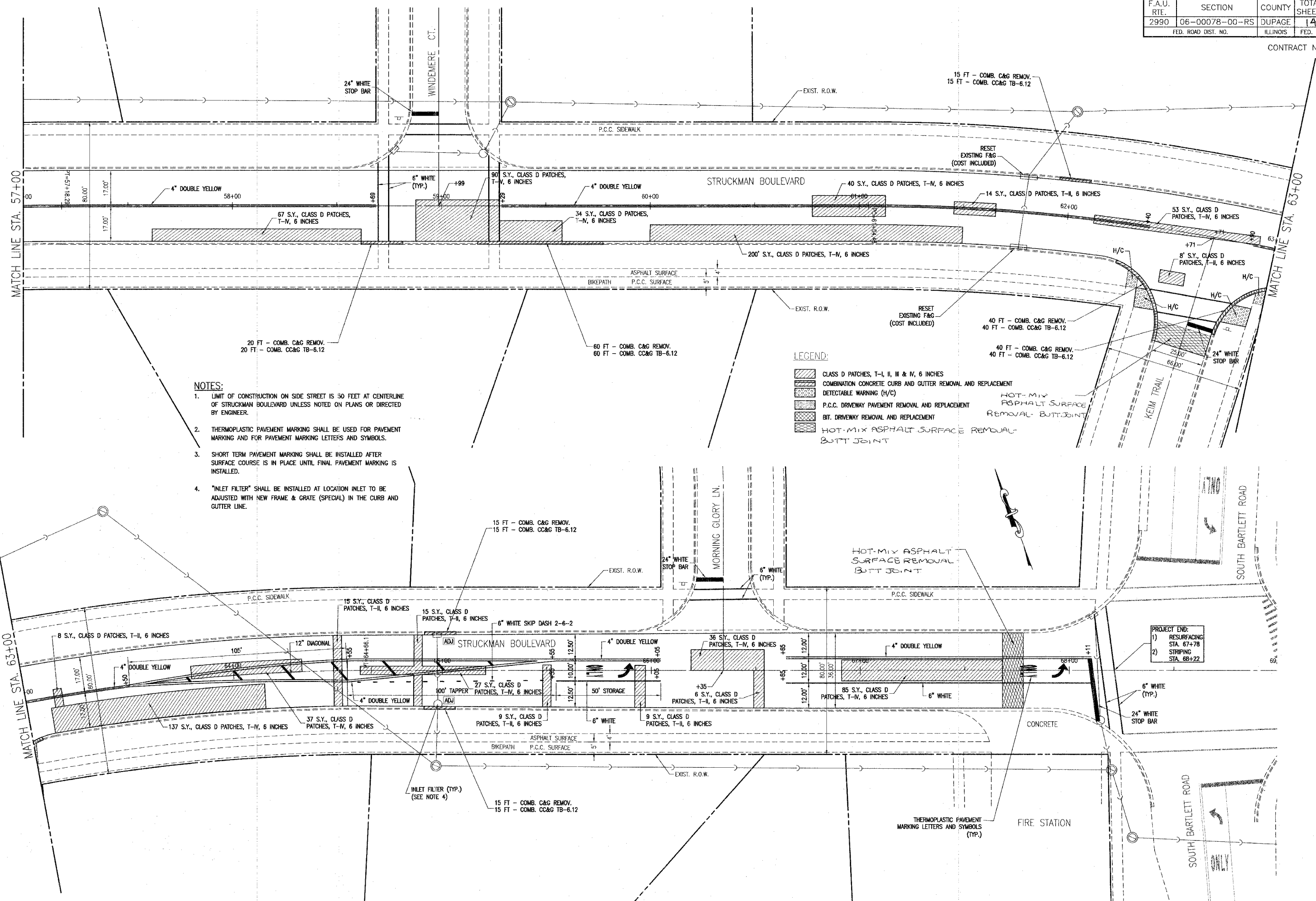
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10-06-2006	SM	SM	REVISED PER IDOT COMMENTS			

PLOTTED BY: SMW  
 CHECKED BY: JP  
 DRAWN BY: SMW  
 CHECKED BY: JP  
 APPROVED BY: DVM

**PAVIA-MARTING & Co.**  
 910 WEST LAKE STREET  
 ROSELLE, IL 60172-3352  
 (630) 529-8000 FAX (630) 894-4910  
 Design Firm Professional Registration #184992376

**STRUCKMAN BLVD. RESURFACING IMPROVEMENTS**  
 PLAN STA. 45+00 TO STA. 57+00  
 SECTION NO: 06-00078-00-RS

SCALE: 1"=20'  
 DATE: 7/28/06  
 JOB NO: 2812  
 SHEET 7 OF 7



**NOTES:**

1. LIMIT OF CONSTRUCTION ON SIDE STREET IS 50 FEET AT CENTERLINE OF STRUCKMAN BOULEVARD UNLESS NOTED ON PLANS OR DIRECTED BY ENGINEER.
2. THERMOPLASTIC PAVEMENT MARKING SHALL BE USED FOR PAVEMENT MARKING AND FOR PAVEMENT MARKING LETTERS AND SYMBOLS.
3. SHORT TERM PAVEMENT MARKING SHALL BE INSTALLED AFTER SURFACE COURSE IS IN PLACE UNTIL FINAL PAVEMENT MARKING IS INSTALLED.
4. "INLET FILTER" SHALL BE INSTALLED AT LOCATION INLET TO BE ADJUSTED WITH NEW FRAME & GRATE (SPECIAL) IN THE CURB AND GUTTER LINE.

**LEGEND:**

- CLASS D PATCHES, T-I, II, III & IV, 6 INCHES
- COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
- DETECTABLE WARNING (H/C)
- P.C.C. DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- BIT. DRIVEWAY REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT

REVISIONS:		DATE		BY		DESCRIPTION	
8-27-2006	SMM	10-06-2006	SMM			REVISED PER IDOT COMMENTS	REVISED PER IDOT COMMENTS

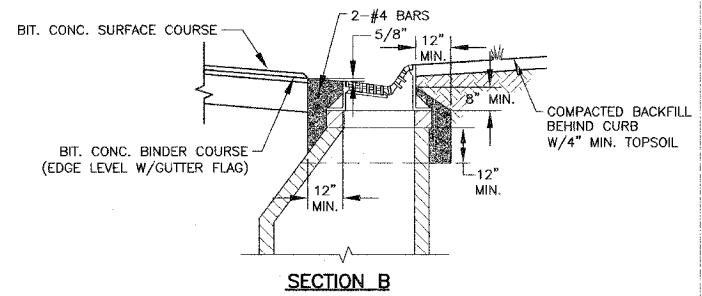
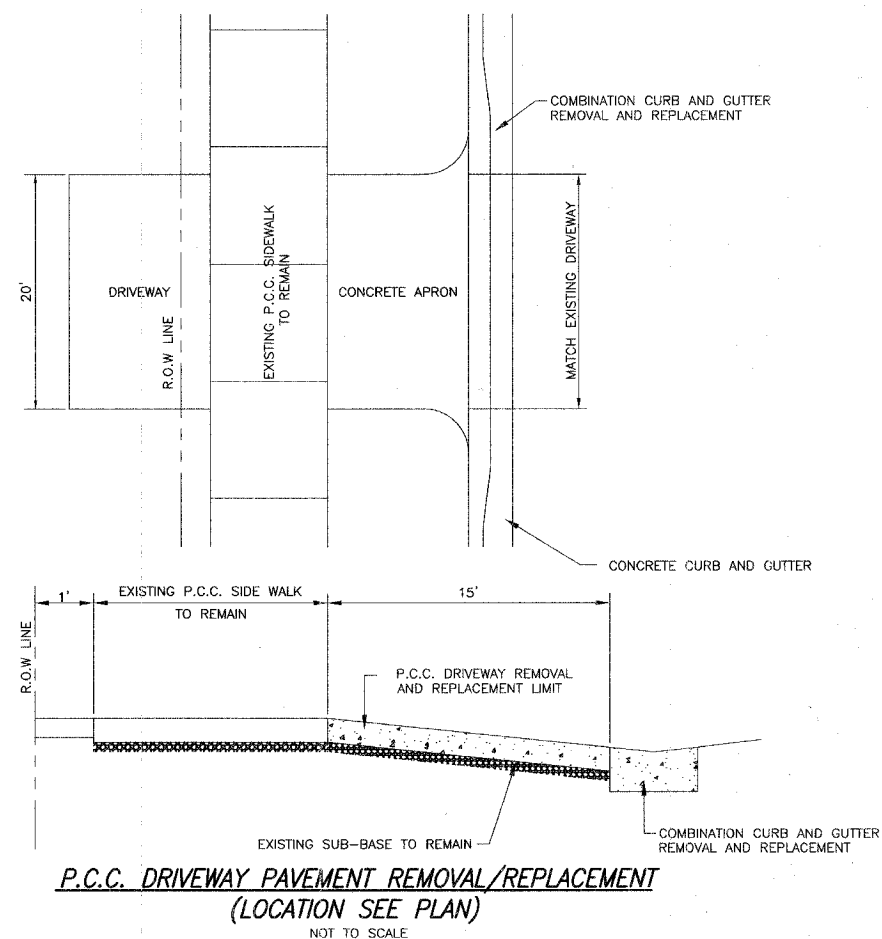
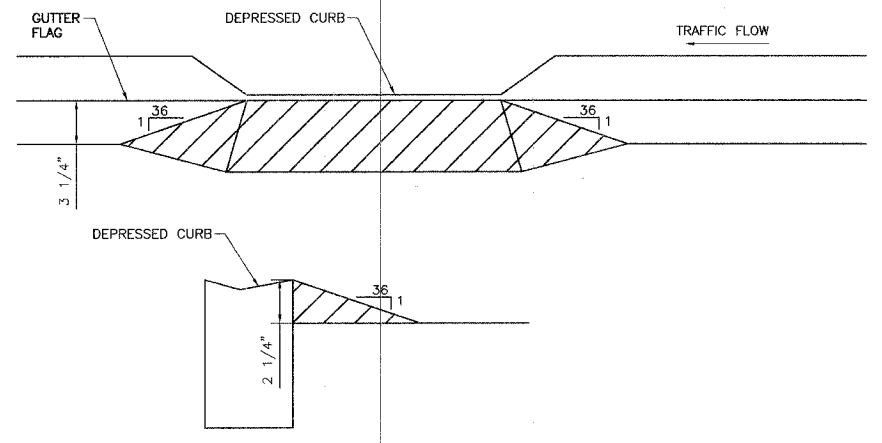
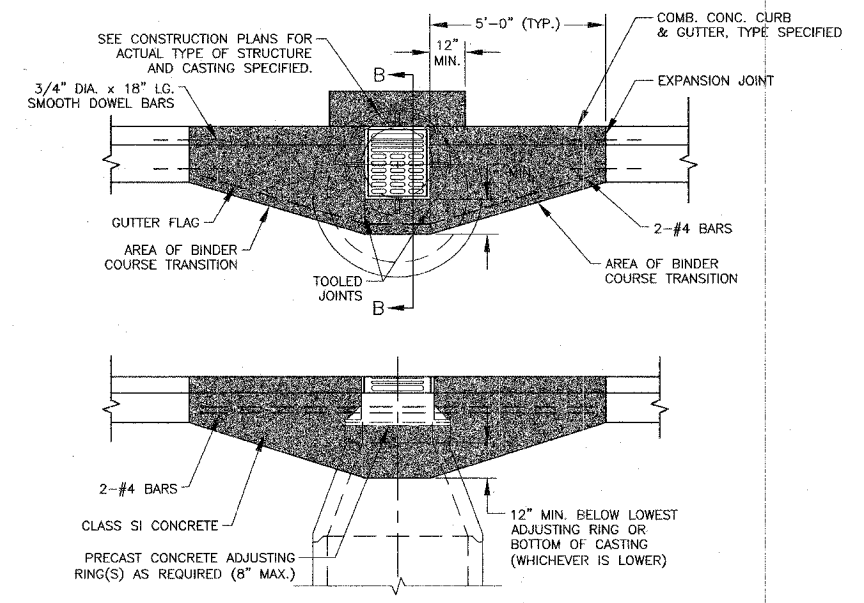
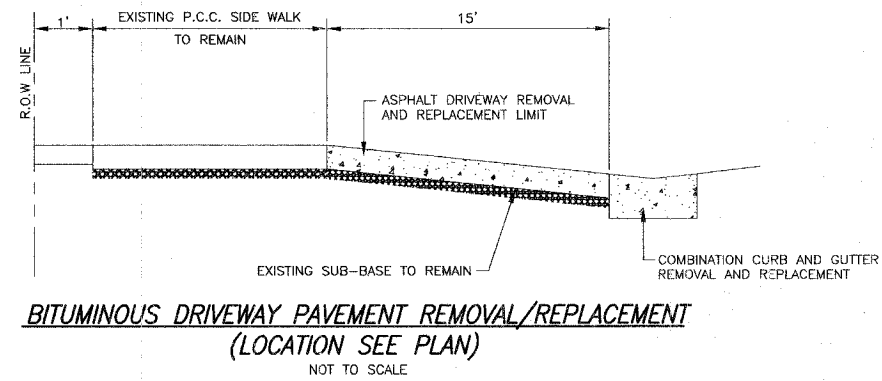
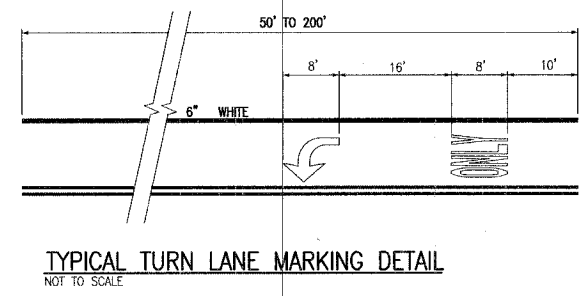
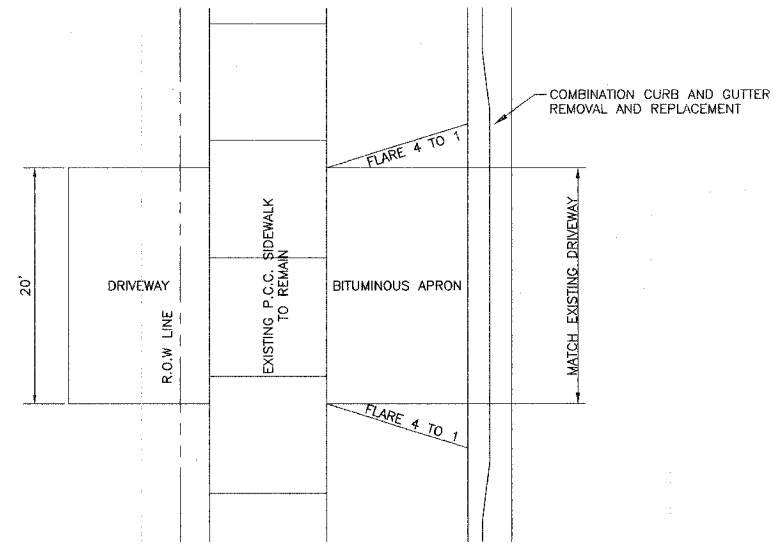
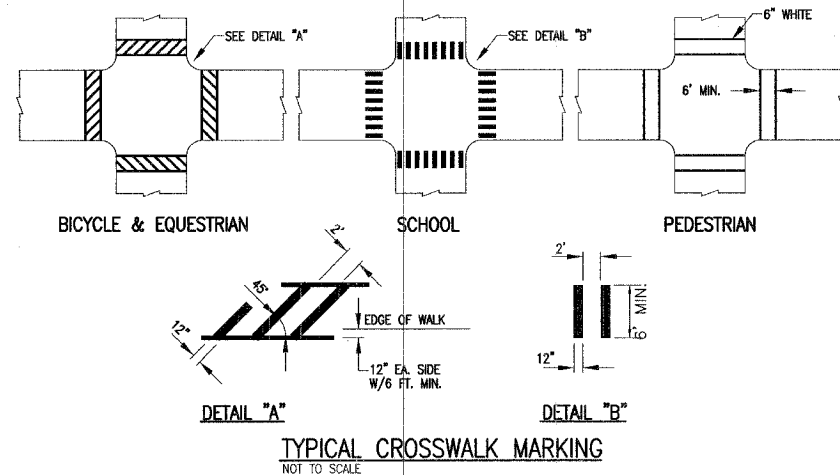
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CHECKED BY:	JP
DRAWN BY:	SMM
CHECKED BY:	JP
APPROVED BY:	DVM

**PAVIA-MARTING & Co.**  
 910 WEST LAKE STREET  
 ROSELLE, IL 60172-3352  
 (630) 529-8800 FAX (630) 894-4910  
Design Firm Professional Registration #164982376

**TITLE: STRUCKMAN BLVD. RESURFACING IMPROVEMENTS**  
 PLAN STA. 57+00 TO STA. 69+00  
 SECTION NO: 06-00078-00-RS

SCALE: 1"=20'  
 DATE: 7/28/06  
 JOB NO: 2812  
 SHEET: 8 OF





- NOTES:
- COST TO CONSTRUCT, MAINTAIN AND REMOVE THE RAMP SHALL BE INCIDENTAL TO THE CONTRACT.
  - "BUMP" SIGN SHOULD BE POSTED IN SUCH A MANNER TO AVOID OBSTRUCTING VISION OF THE DRIVE TO SEE ON COMING TRAFFIC OR FROM BEING SEEN.

DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
10-30-2006	SMM	REVISED PER IDOT COMMENTS						

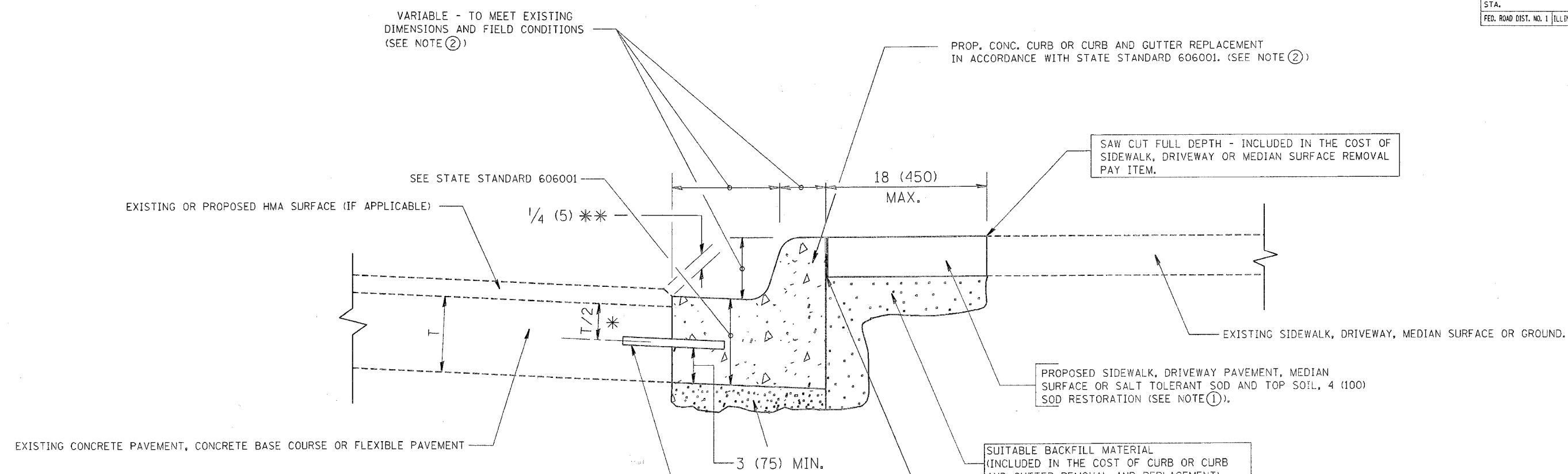
PLOTTED BY:	SMM
CHECKED BY:	VP
DRAWN BY:	SMM
CHECKED BY:	VP
APPROVED BY:	DVM

**PAVIA-MARTING & Co.**  
918 WEST LAKE STREET  
ROSELLE, IL 60172-3352  
(630) 529-8000 FAX (630) 894-4916  
Design Firm Professional Registration #18-002374

TITLE:	STRUCKMAN BLVD. RESURFACING IMPROVEMENTS CONSTRUCTION DETAILS SECTION NO: 06-00078-00-RS
SCALE:	NONE
DATE:	07/28/06
JOB NO:	2812
SHEET	9 OF 14

F:\2812\06d\2812 - CONSTRUCTION DETAILS.DWG 10/31/2006 8:48 AM

F.A.V. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2990	06-0078-00-25	DuPage	14	10
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



- \* 3 (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE:
- ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
  - ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

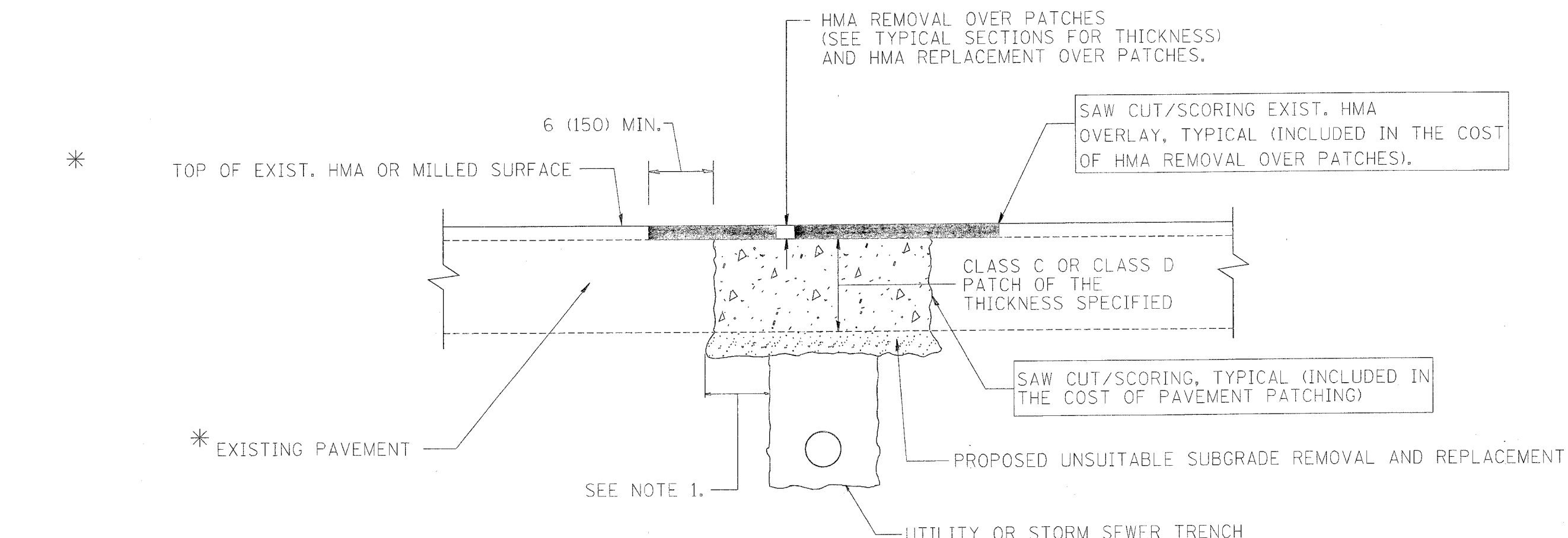
REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CURB OR CURB AND GUTTER  
 REMOVAL AND REPLACEMENT**

SCALE: VERT. NONE  
 HORIZ. 1" = 10'  
 PLOT DATE: 10/31/2006  
 DRAWN BY: [blank]  
 CHECKED BY: [blank]  
 BD600-06 (BD-24)  
 REVISION DATE: 01/01/07

PLOT DATE = 10/31/2006  
 PLOT SCALE = 1/8" = 1'-0"  
 USER NAME = jaysh

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
270-06-0001B-005			14	11
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT**

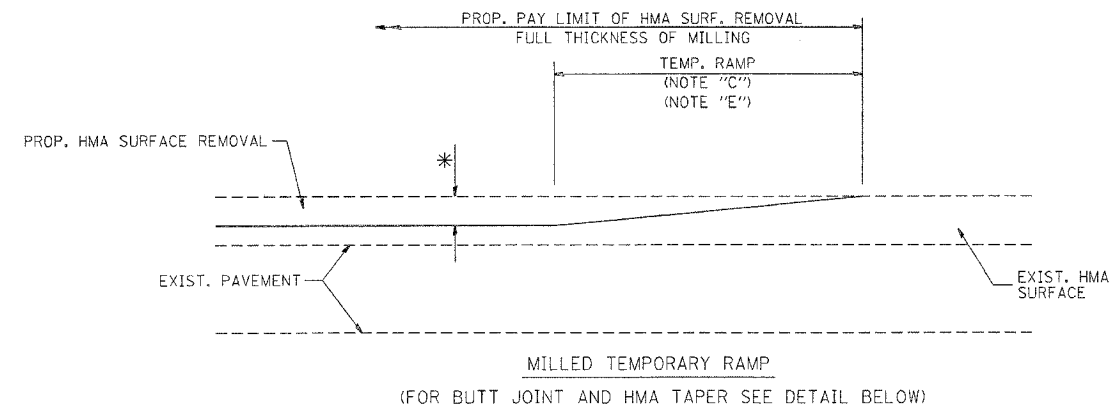
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PLOT DATE: 10/31/2006

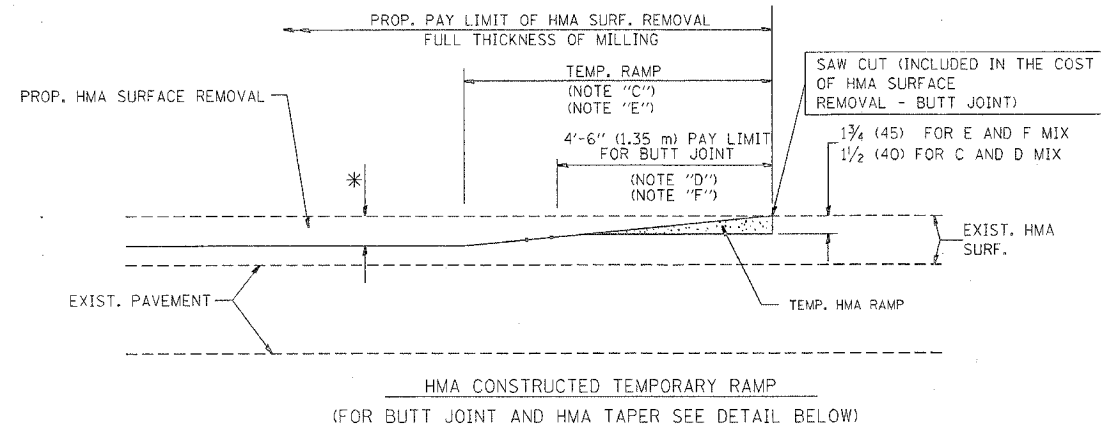
DRAWN BY  
CHECKED BY  
BD400-04 (BD-22)  
REVISION DATE: 01/01/07

PLOT DATE = 10/31/2006  
PLOT SCALE = 1/8" = 1'-0"  
USER NAME = lrygn

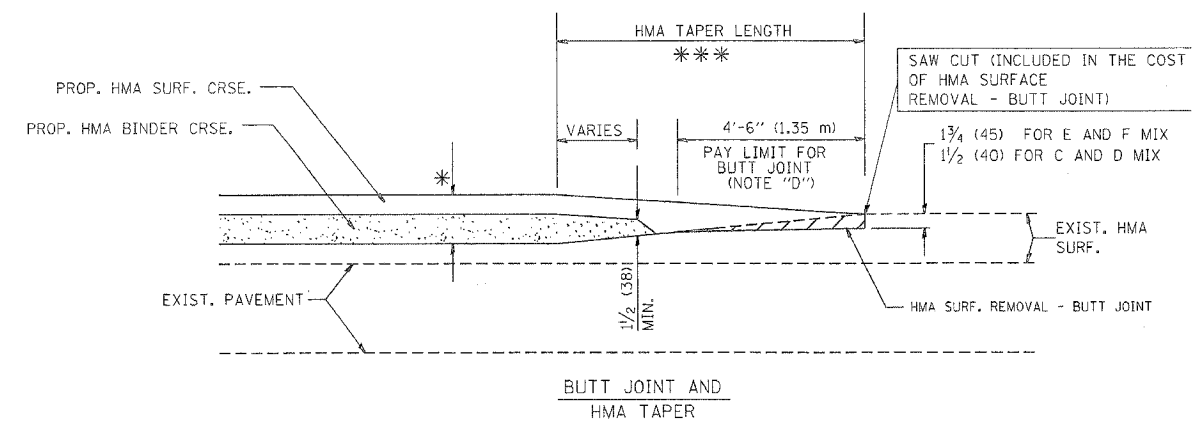
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



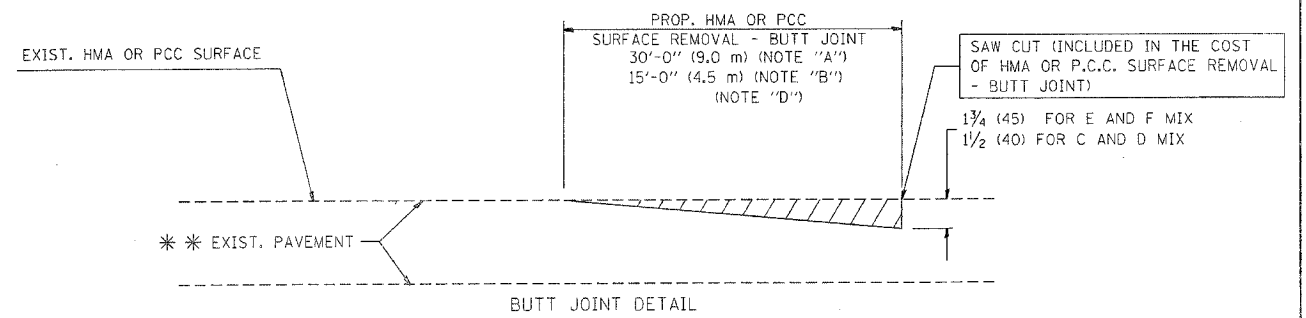
OPTION 1



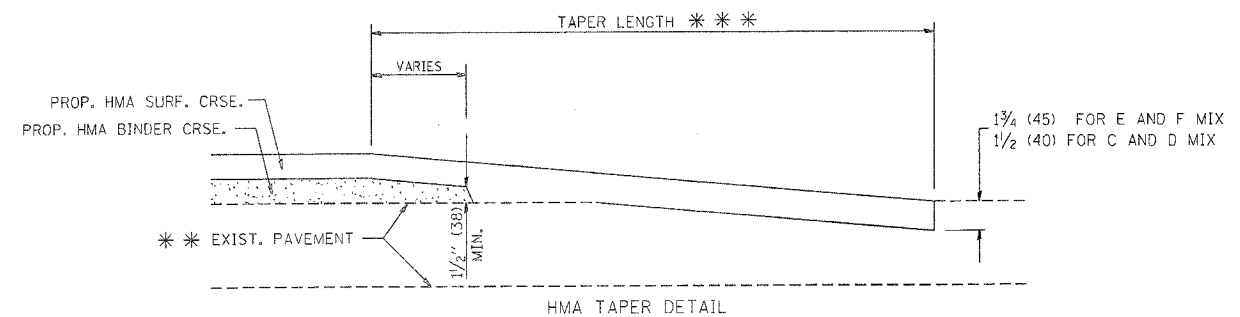
OPTION 2  
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

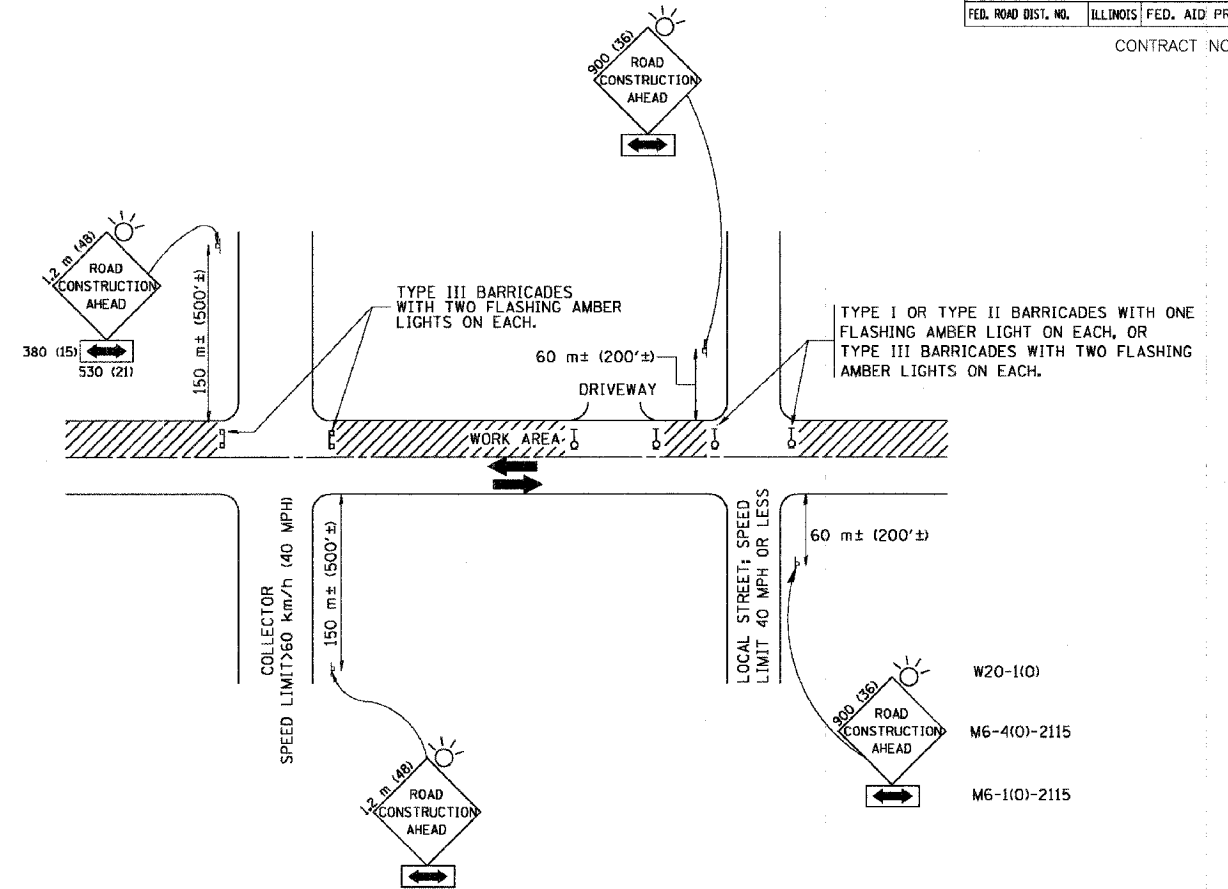
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HORIZ. 1" = 10'

DRAWN BY  
CHECKED BY

BD400-05 (VI-BD32)  
REVISION DATE: 01/01/07

PLOT DATE = 10/31/2006  
PLOT SCALE = 1/4" = 1'-0"  
USER NAME = jaygo

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2990	06-00078-00-RS	DUPAGE	14	13
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

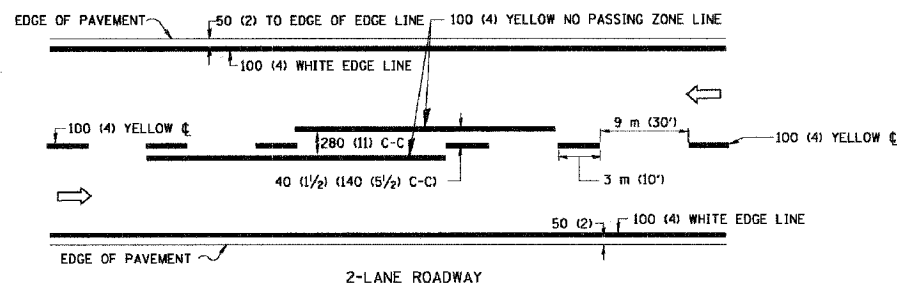
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TRAFFIC CONTROL AND PROTECTION  
 FOR  
 SIDE ROADS, INTERSECTIONS, AND  
 DRIVEWAYS**

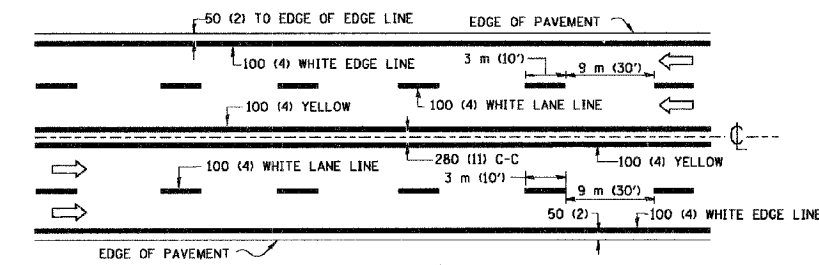
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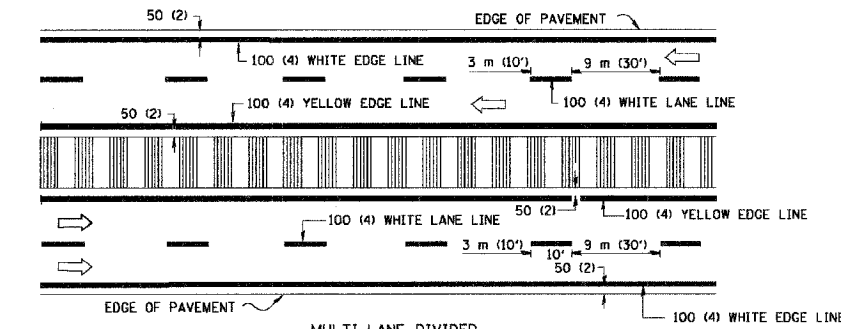
TC-10  
 REVISION DATE: 01/06/00



2-LANE ROADWAY



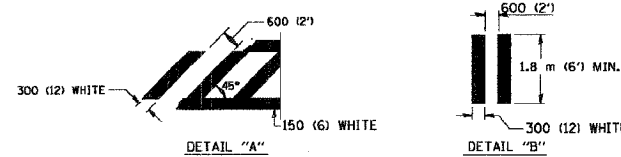
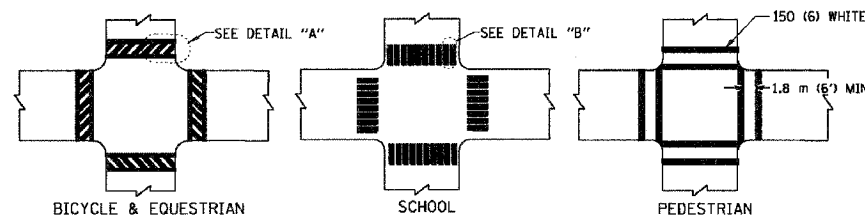
MULTI-LANE UNDIVIDED



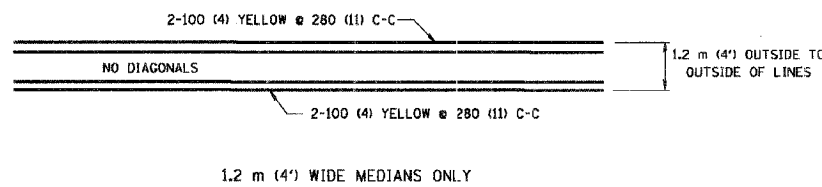
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

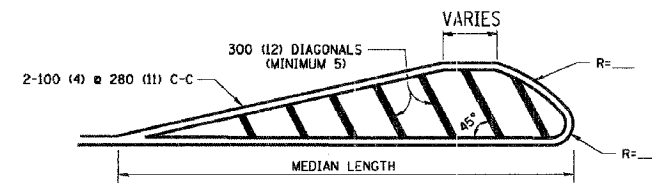
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

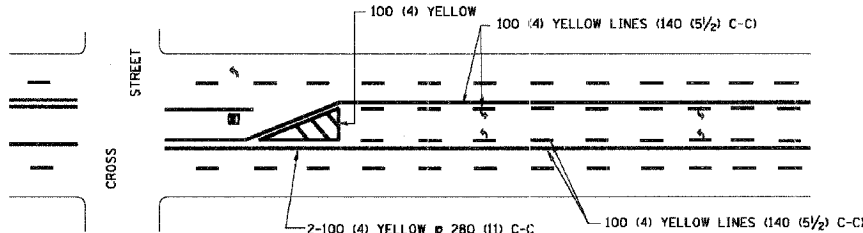


1.2 m (4') WIDE MEDIANS ONLY

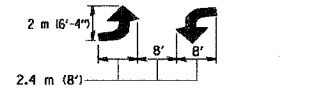


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))  
 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))  
 45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

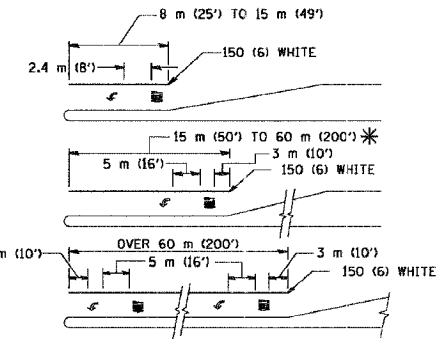


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

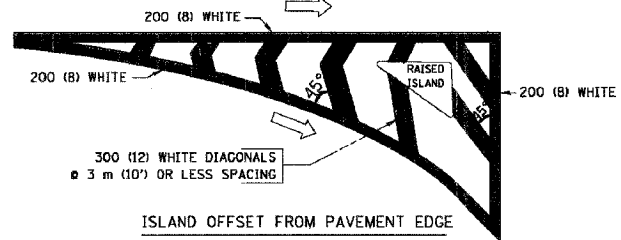
TYPICAL PAINTED MEDIAN MARKING



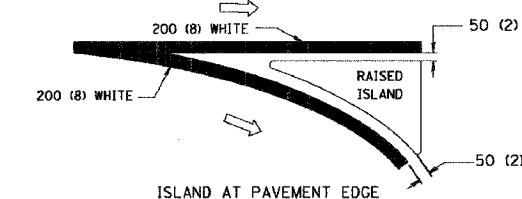
FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.  
 AREA = 1.5 m<sup>2</sup> (15.6 SQ. FT.) AREA = 1.9 m<sup>2</sup> (20.8 SQ. FT.)  
 \* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINES; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m <sup>2</sup> (3.6 SQ. FT.) EACH "X"=5.0 m <sup>2</sup> (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DISTRICT ONE  
 TYPICAL PAVEMENT MARKINGS

SCALE: NONE  
 DATE: 2/15/2006  
 DRAWN BY CADD  
 CHECKED BY