

CONSTRUCTION PLANS FOR GALESBURG MUNICIPAL AIRPORT

GALESBURG, KNOX COUNTY, ILLINOIS

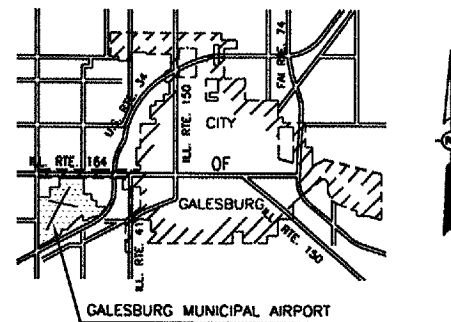
APPLY POROUS FRICTION COURSE AND MARK RUNWAY 10-28

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	AS BID QUANTITY	AS BUILT QUANTITY
AR150530	TRAFFIC MAINTENANCE	L. SUM	1	
AR152480	SHOULDER ADJUSTMENT	SQ. YD.	800	
AR201660	BITUMINOUS CRACK REPAIR	LIN. FT.	23,260	
AR201666	BASE COURSE CRACK REPAIR	SQ. YD.	333	
AR401110	PAVEMENT PREPARATION	L. SUM	1	
AR401655	BUTT JOINT CONSTRUCTION	SQ. YD.	1,689	
AR402622	POROUS FRICTION COURSE, 0.10'	SQ. YD.	39,369	
AR603510	BITUMINOUS TACK COAT	GAL.	11,811	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ. FT.	22,920	

ILLINOIS PROJECT NO. GBG-3624
AIP PROJECT NO. 3-17-0047-B9

DECEMBER 15, 2006

INDEX OF SHEETS	
NO.	DESCRIPTION
1	COVER SHEET
2	IMPROVEMENT AND SAFETY PLAN
3	TYPICAL SECTIONS AND SPECIAL DETAILS
4-5	RUNWAY 10-28 PLAN AND PROFILE
6	RUNWAY MARKING LAYOUT



<p>EXPIRES 11/30/07 ENGINEER'S SEAL</p>	PLANS PREPARED BY : HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	CITY OF GALESBURG, ILLINOIS
	SUBMITTED <i>Doug L. Hutchison</i> DATE <i>November 15, 2006</i>	APPROVED <i>Larry Cox</i> DIRECTOR OF PUBLIC WORKS DATE <i>Nov. 20, 2006</i>

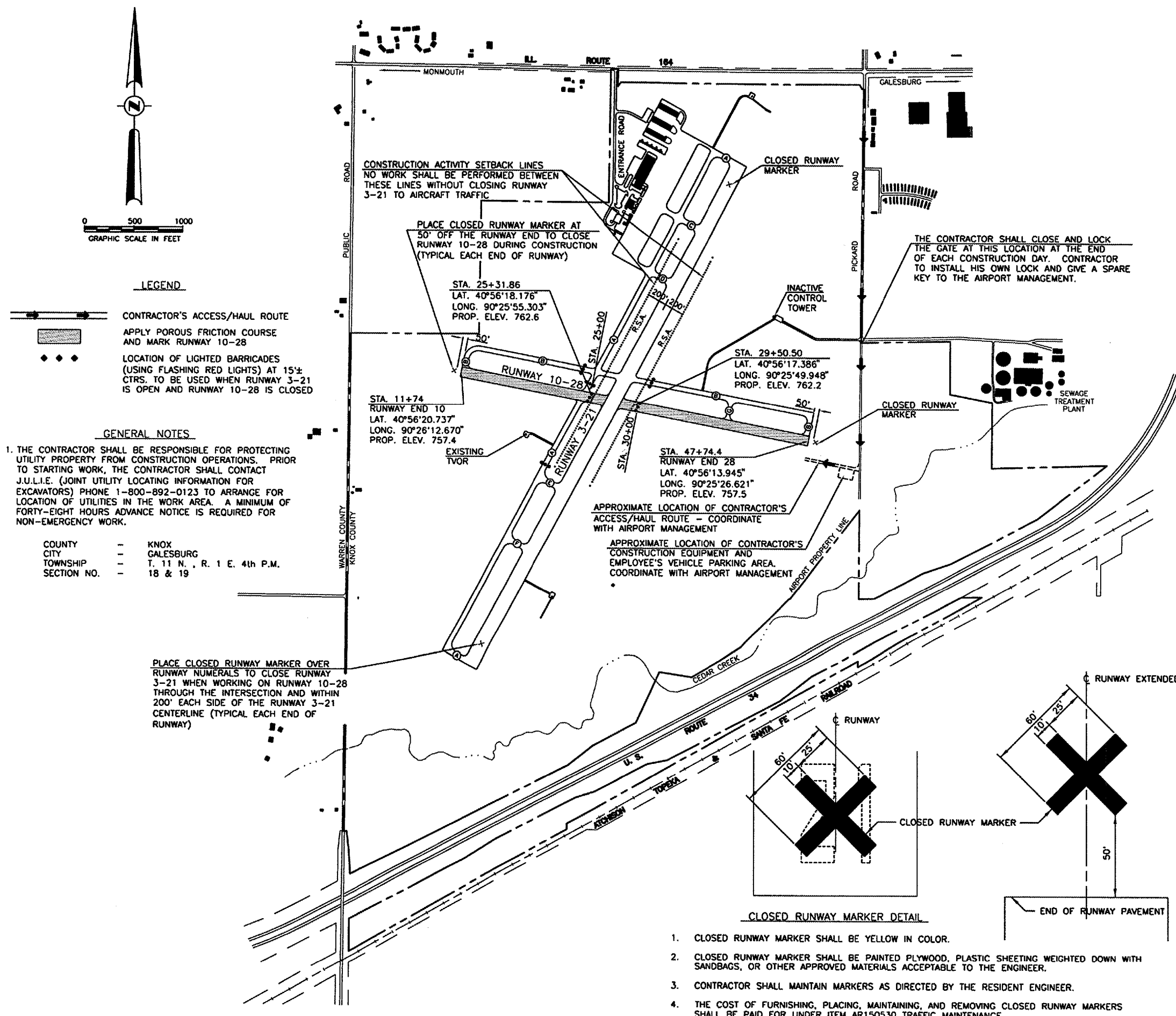
CONSTRUCTION PROCEDURE NOTES

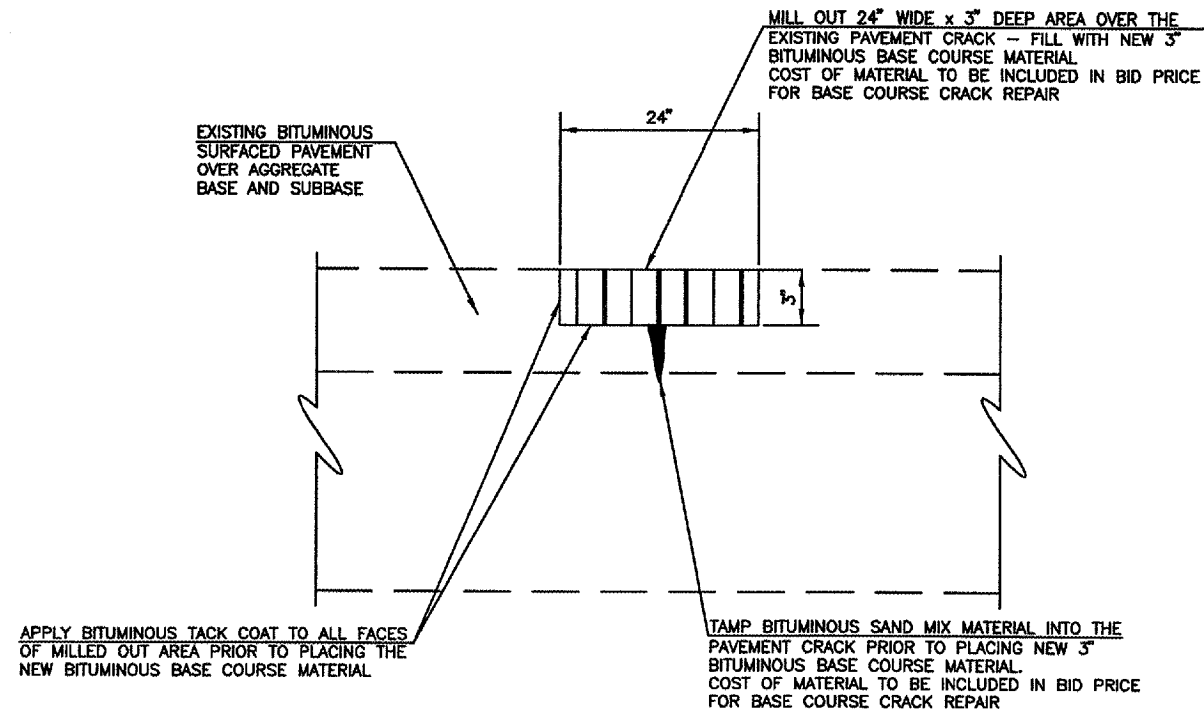
1. ALL CONTRACTOR EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AREA DESIGNATED BY THE AIRPORT MANAGER FOR USE AS A VEHICLE PARKING AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY AUTHORIZED CONTRACTOR VEHICLES WILL BE ALLOWED ON THE AIR OPERATIONS AREA OF THE AIRPORT AND ON THE CONSTRUCTION SITE.
2. THE CONTRACTOR SHALL FURNISH ALL EMPLOYEES WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THEM AS BEING PART OF THE CONSTRUCTION CREW.
3. FLAGS WILL BE REQUIRED ON ALL CONTRACTOR'S VEHICLES AND EQUIPMENT USED ON THE AIRPORT OPERATIONS AREA AND CONSTRUCTION SITE. THE FLAGS SHALL BE THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE) DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
4. THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA FOR THE DURATION OF THE PROJECT. ANY DAMAGE TO EXISTING PAVEMENTS USED TO HAUL MATERIAL TO THE CONSTRUCTION SITE WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
5. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL BARRICADES REQUIRED TO DELINEATE THE WORK AREA AND KEEP AIRCRAFT FROM ENCRANCHING INTO SAID WORK AREA. THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
6. CONSTRUCTION ACTIVITY CONTROL AND OPERATION AREA PROTECTION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR NO. 150/5370-2E.
7. THE MAXIMUM CONSTRUCTION EQUIPMENT HEIGHT UTILIZED ON THE AIRPORT FOR THIS PROJECT SHALL BE 30'.
8. THIS PROJECT CONSISTS OF WORK TO APPLY POROUS FRICTION COURSE AND MARK RUNWAY 10-28.
9. THE CONTRACTOR SHALL COORDINATE A METHOD OF DISABLING THE RUNWAY 10-28 EDGE LIGHTS WITH THE AIRPORT MANAGEMENT SO AS TO KEEP THE LIGHTS INOPERABLE WHILE THE RUNWAY IS CLOSED TO AIR TRAFFIC. THE RUNWAY 3-21 EDGE LIGHTS SHALL ALSO BE DISABLED WHEN THAT RUNWAY IS CLOSED TO AIR TRAFFIC. THE COST OF DISABLING AND RE-ENERGIZING THE EDGE LIGHT CIRCUITS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE UNICOM FREQUENCY AT THE AIRPORT IS 123.0 MHZ. THE CONTRACTOR SHALL FURNISH A FLAGMAN WITH A RADIO IN HIS POSSESSION TO MONITOR AIR TRAFFIC ON THIS FREQUENCY AND DIRECT EQUIPMENT ACROSS RUNWAY 3-21, SHOULD IT BE NECESSARY TO CROSS SAID RUNWAY DURING CONSTRUCTION WHILE THE RUNWAY IS ACTIVE. NO VEHICLE, EQUIPMENT OR PERSON SHALL CROSS AN ACTIVE RUNWAY WITHOUT RECEIVING CLEARANCE TO CROSS FROM THE FLAGMAN. VEHICLE DRIVERS SHALL ALSO CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING WHEN GIVEN CLEARANCE TO CROSS.
11. PRIOR TO THE OPERATION OF ANY CONSTRUCTION VEHICLES WITHIN THE AIR OPERATIONS AREA, IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE TRAINING FROM THE AIRPORT MANAGEMENT REGARDING PROCEDURES FOR VEHICLE OPERATIONS TO ENSURE AIRCRAFT SAFETY DURING CONSTRUCTION. IT WILL BE REQUIRED THAT THE CONTRACTOR RECEIVE PERMISSION FROM THE AIRPORT MANAGEMENT IN ADVANCE OF ANY ACTIVITY SPECIFIC TO THE DAY, TIME AND EXACT PURPOSE OF ANY NEED TO ENTER THE AIRPORT OPERATIONS AREA.
12. THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND.

RECOMMENDED CONSTRUCTION SEQUENCE

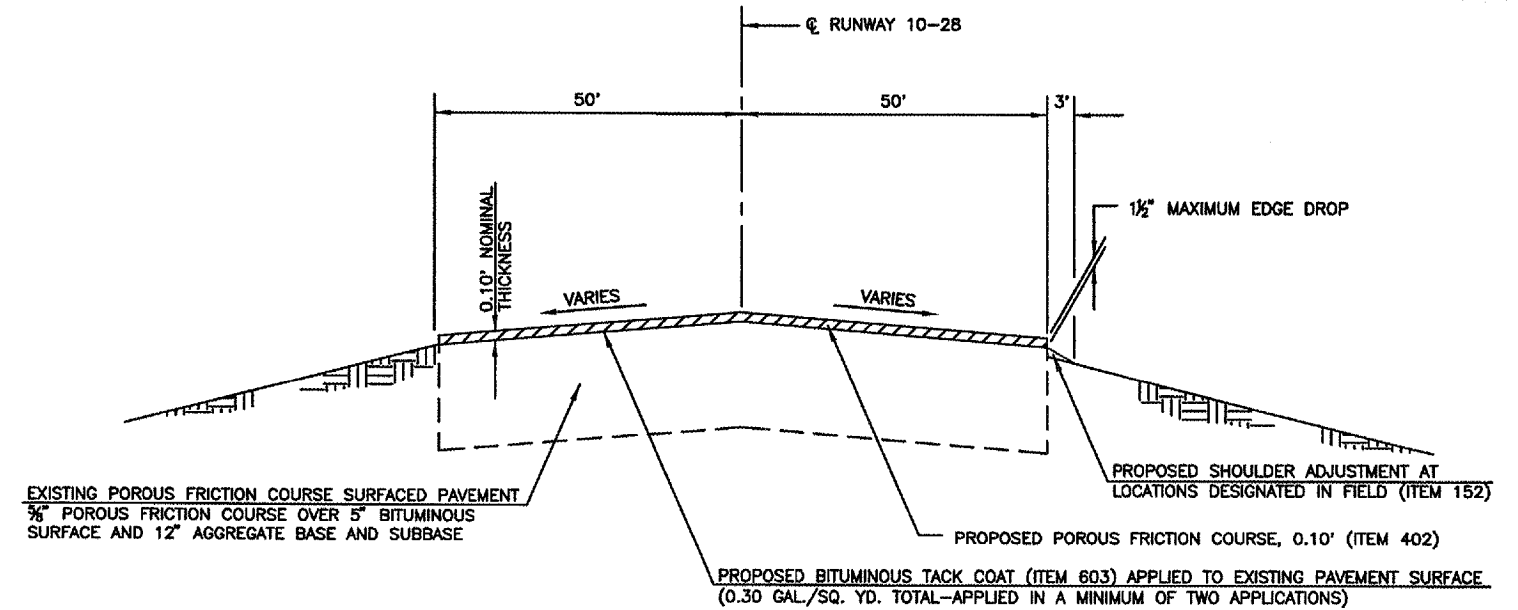
1. INSTALL CLOSED RUNWAY MARKERS AT THE ENDS OF RUNWAY 10-28 TO CLOSE THE RUNWAY DURING THE CONSTRUCTION PERIOD. DISABLE THE RUNWAY 10-28 EDGE LIGHTING CIRCUIT FOR THE DURATION OF THE CONSTRUCTION PERIOD.
2. WORK SHALL BE ACCOMPLISHED ON RUNWAY 10-28 LEAVING THE AREA FROM STA. 25+00 TO STA. 30+00 OPEN FOR AIR TRAFFIC TO UTILIZE RUNWAY 3-21 AND ITS PARALLEL TAXIWAY A.
3. WORK SHALL BE SEQUENCED SO AS NOT TO ALLOW THE CONTRACTOR'S HAULING EQUIPMENT TO DRIVE ON THE FINISHED SURFACE.
4. AT SUCH TIME AS WORK IS ACCOMPLISHED FROM STA. 25+00 TO STA. 30+00, RUNWAY 3-21 SHALL ALSO BE CLOSED TO AIR TRAFFIC BY PLACING CLOSED RUNWAY MARKERS OVER THE RUNWAY 3-21 NUMERALS. ALL PAVEMENT PREPARATION, CRACK REPAIR, MILLING AND PAVING WORK IN THIS AREA SHALL BE DONE AS EXPEDITIOUSLY AS POSSIBLE IN ORDER TO KEEP THE AIRPORT CLOSED FOR AS SHORT A TIME AS IS NECESSARY. ENTIRE AIRPORT CLOSURE SHALL NOT EXCEED TWO CONSECUTIVE DAYS. UPON COMPLETION OF WORK AND PAVEMENT CURING IN THIS AREA, REMOVE THE CLOSED RUNWAY MARKERS FROM THE RUNWAY 3-21 NUMERALS AND RE-OPEN RUNWAY 3-21 TO AIR TRAFFIC.

GALESBURG MUNICIPAL AIRPORT GALESBURG, ILLINOIS	
IMPROVEMENT AND SAFETY PLAN	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: T.J.D. DATE: NOVEMBER, 2006	R.L. PROJ. NO. GBC-3624 AP PROJ. NO. 3-17-0047-89





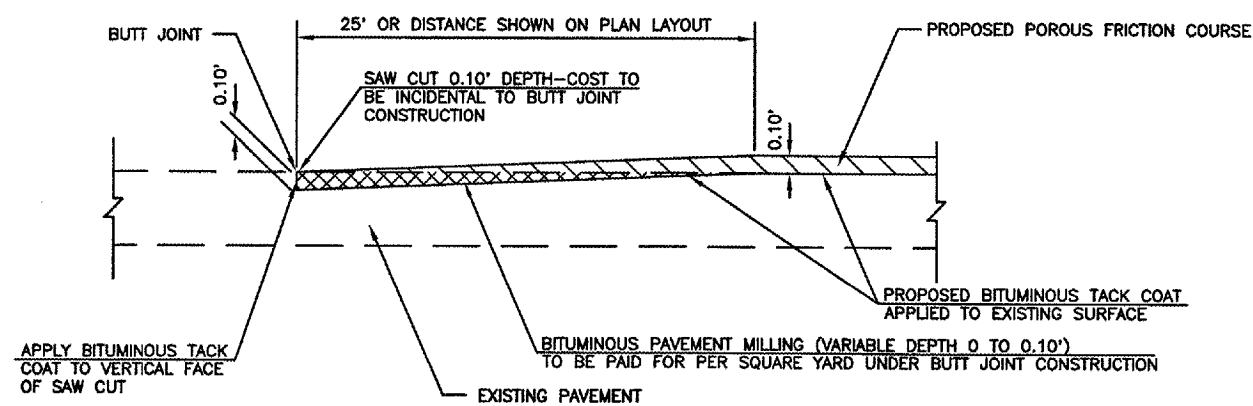
BASE COURSE CRACK REPAIR DETAIL
NOT TO SCALE



RUNWAY 10-28 TYPICAL SECTION
NOT TO SCALE

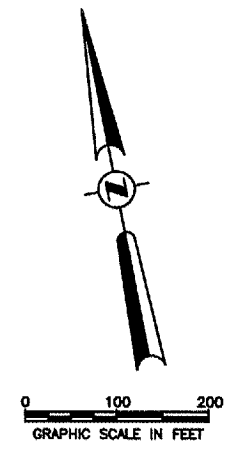
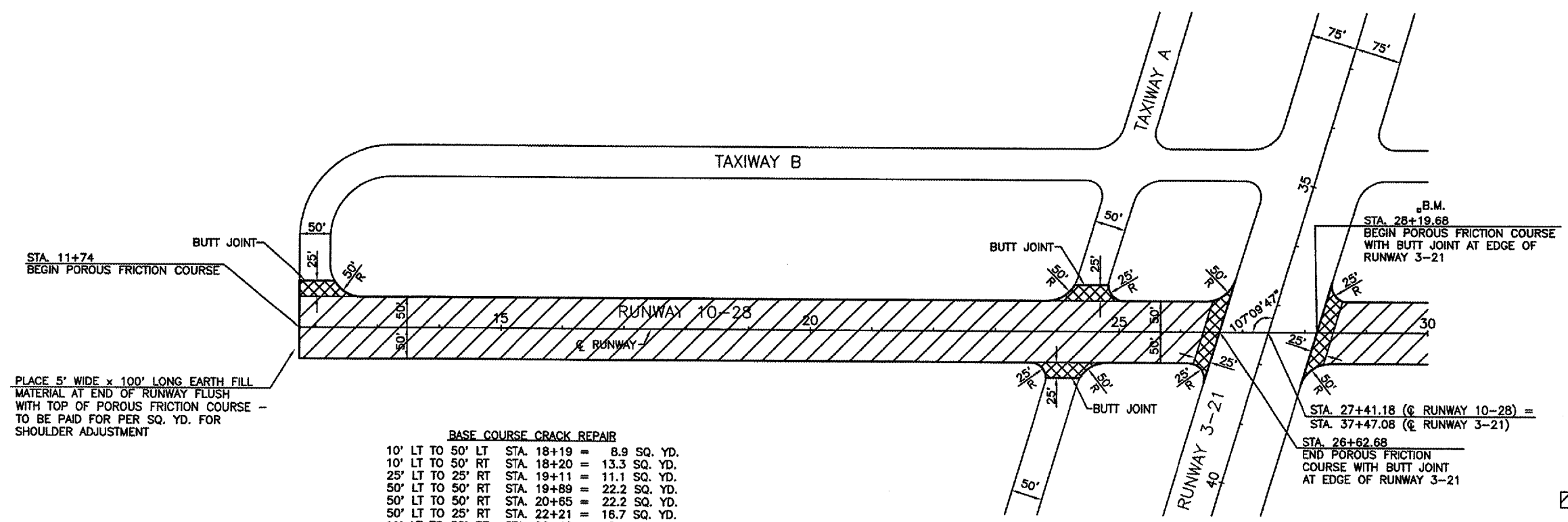
GENERAL NOTES

1. OPEN PAVING JOINTS AND/OR CRACKS IN THE EXISTING SURFACE OF THE PAVEMENT OVER 3/8" IN WIDTH SHALL BE DESIGNATED BY THE PROJECT ENGINEER IN THE FIELD TO BE REPAIRED PRIOR TO PLACING THE POROUS FRICTION COURSE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR BITUMINOUS CRACK REPAIR.
2. THE QUANTITIES FOR CRACK REPAIR HAVE BEEN ESTIMATED BY MEASURING CRACK REPAIR IN SELECTED REPRESENTATIVE LOCATIONS ON THE PAVEMENT. THESE QUANTITIES MAY BE INCREASED OR REDUCED AT THE TIME OF CONSTRUCTION BASED UPON THE CRACK WORK THAT IS MARKED OUT BY THE RESIDENT ENGINEER AFTER FIELD INSPECTION OF THE PAVEMENT CRACKS.
3. ANY SOD WHICH HAS ENCRONCHED UPON THE PAVEMENT EDGE SHALL BE BLADED BACK PRIOR TO THE PROPOSED POROUS FRICTION COURSE APPLICATION. THE ENTIRE PAVEMENT SURFACE SHALL BE SWEEPED TO REMOVE ALL LOOSE MATERIALS BEFORE THE APPLICATION OF BITUMINOUS TACK COAT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR PAVEMENT PREPARATION.
4. ANY DAMAGE TO THE EXISTING SOD BEYOND THE EDGE OF PAVEMENT RESULTING FROM THE BLADING OF THE SOD OFF THE PAVEMENT SURFACE SHALL BE FILLED WITH DIRT, GRADED, FERTILIZED AND SEEDED TO RESTORE THE SOD SHOULDERS TO THEIR ORIGINAL CONDITION. ANY LOCALLY AVAILABLE 12-12-12 FERTILIZER MAY BE USED APPLIED AT A RATE OF 1 POUND OF FERTILIZER PER EACH 160 SQUARE FEET OF DISTURBED OR RESEEDING AREA. SEED USED SHALL BE ALTA FESCUE OR KENTUCKY 31, APPLIED AT THE RATE OF 1 POUND OF PURE LIVE SEED PER EACH 436 SQUARE FEET OF RESEEDING AREA. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCIDENTAL TO PAVEMENT PREPARATION.
5. A BITUMINOUS TACK COAT SHALL BE APPLIED TO THE EXISTING PAVEMENT PRIOR TO PLACING THE POROUS FRICTION COURSE. THE APPLICATION RATE SHOWN ON THE TYPICAL SECTION WAS USED TO CALCULATE THE PLAN QUANTITY. THE EXACT AMOUNT OF BITUMINOUS TACK COAT TO BE USED SHALL BE DETERMINED BY THE PROJECT ENGINEER AT THE TIME OF CONSTRUCTION.
6. THE POROUS FRICTION COURSE SHALL BE ABUTTED TO THE EXISTING PAVEMENT AT RUNWAY 3-21 AND INTERSECTING TAXIWAYS AS SHOWN ON THE BITUMINOUS BUTT JOINT DETAIL. THE EXISTING PAVEMENT CONFINED WITHIN THE BITUMINOUS PAVEMENT MILLING AREA WILL BE REMOVED IN ACCORDANCE WITH THE DETAIL FROM 0 TO 0.10' DEPTH AT THE BUTT JOINT LOCATION. FILLING OF THE MILLED AREA WILL BE MADE USING POROUS FRICTION COURSE MIX AND WILL BE PAID FOR PER SQUARE YARD FOR POROUS FRICTION COURSE.
7. AT LOCATIONS WHERE THE EDGE DROP FROM THE FINISHED SURFACE TO THE EXISTING TURF SHOULDER EXCEEDS 1/2", THE SHOULDER SHALL BE ADJUSTED AS SHOWN IN THE TYPICAL SECTION USING SOIL OBTAINED OFF SITE THAT IS FREE OF HEAVY SOD, DECAYED VEGETATIVE MATTER, RUBBISH, AGGREGATE AND OTHER UNSUITABLE MATERIAL. THE SHOULDER ADJUSTMENT AREAS SHALL BE FERTILIZED AND SEEDED AND THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR SHOULDER ADJUSTMENT.
8. ALL AREAS DISTURBED BY THE CONTRACTOR INCLUDING THE CONTRACTOR'S PARKING AREA, HAUL ROADS, ETC. SHALL BE REGRADED AND RESEEDED AND/OR RESTORED TO THEIR ORIGINAL CONDITION. THIS RESTORATION SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT HIS OWN EXPENSE UPON COMPLETION OF THIS IMPROVEMENT PROJECT.



BITUMINOUS BUTT JOINT CONSTRUCTION DETAIL
NOT TO SCALE

GALESBURG MUNICIPAL AIRPORT GALESBURG, ILLINOIS	
TYPICAL SECTIONS AND SPECIAL DETAILS	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: T.J.D.	ILL. PROJ. NO. 68G-3824
DATE: NOVEMBER, 2006	AP PROJ. NO. 3-17-0047-89



STA. 11+74
BEGIN POROUS FRICTION COURSE

BUTT JOINT

PLACE 5' WIDE x 100' LONG EARTH FILL MATERIAL AT END OF RUNWAY FLUSH WITH TOP OF POROUS FRICTION COURSE - TO BE PAID FOR PER SQ. YD. FOR SHOULDER ADJUSTMENT

BASE COURSE CRACK REPAIR

10' LT TO 50' LT	STA. 18+19	=	8.9 SQ. YD.
10' LT TO 50' RT	STA. 18+20	=	13.3 SQ. YD.
25' LT TO 25' RT	STA. 19+11	=	11.1 SQ. YD.
50' LT TO 50' RT	STA. 19+89	=	22.2 SQ. YD.
50' LT TO 50' RT	STA. 20+65	=	22.2 SQ. YD.
50' LT TO 25' RT	STA. 22+21	=	16.7 SQ. YD.
11' LT TO 50' RT	STA. 22+42	=	13.6 SQ. YD.
50' LT TO 50' RT	STA. 29+50	=	22.2 SQ. YD.
TOTAL THIS SHEET = 130.2 SQ. YD.			

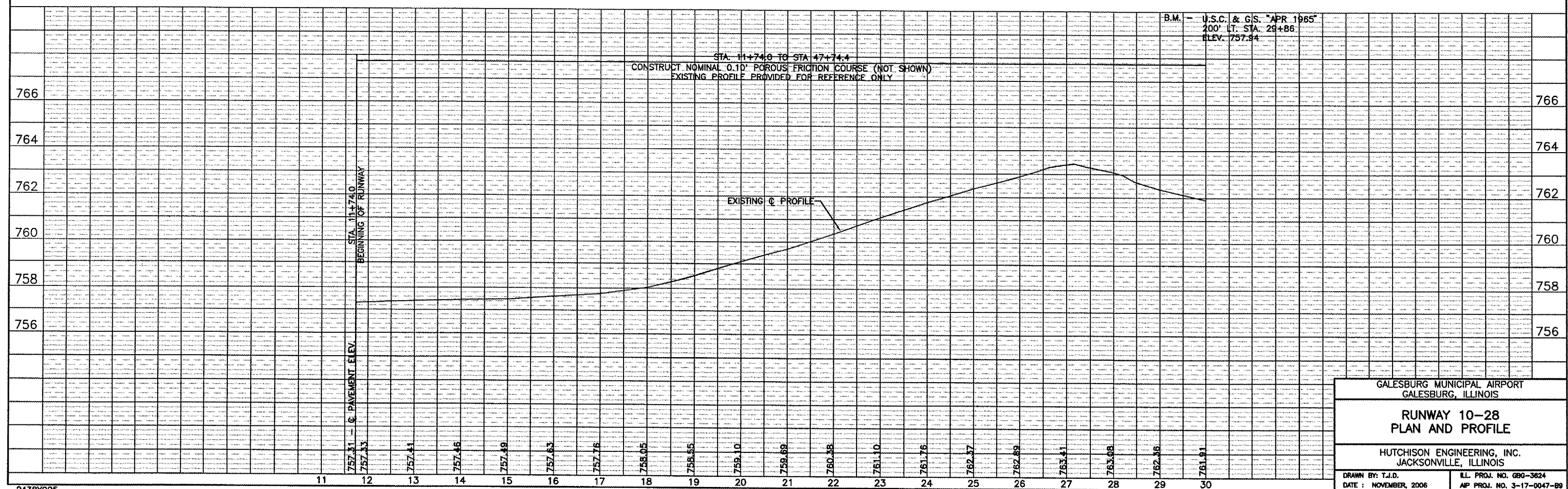
B.M. STA. 28+19.68
BEGIN POROUS FRICTION COURSE WITH BUTT JOINT AT EDGE OF RUNWAY 3-21

STA. 27+41.18 (C RUNWAY 10-28) =
STA. 37+47.08 (C RUNWAY 3-21)

STA. 26+62.68
END POROUS FRICTION COURSE WITH BUTT JOINT AT EDGE OF RUNWAY 3-21

LEGEND

	POROUS FRICTION COURSE, 0.10'
	BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION

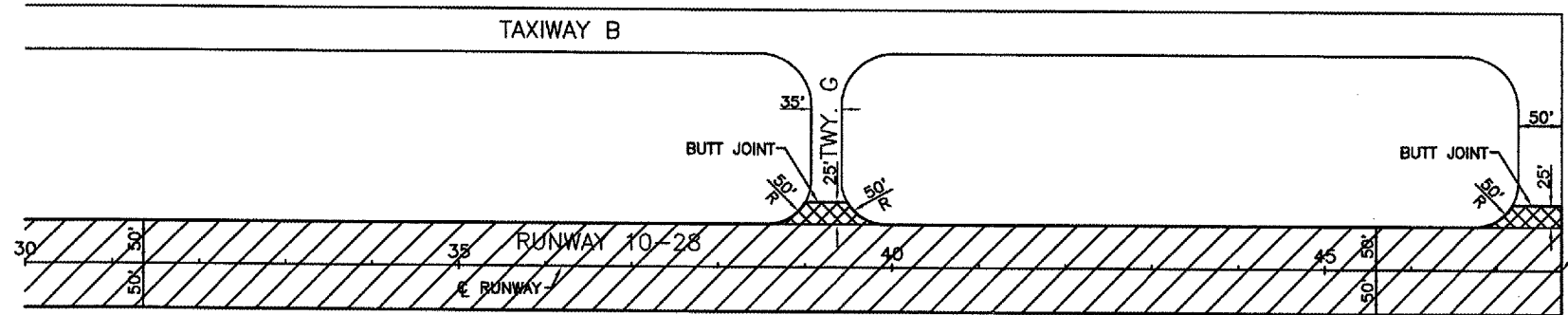
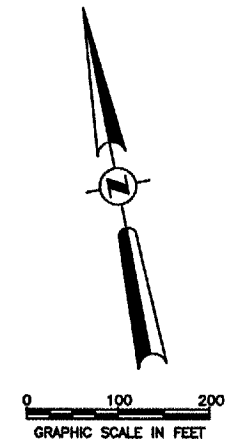


GALESBURG MUNICIPAL AIRPORT
GALESBURG, ILLINOIS

**RUNWAY 10-28
PLAN AND PROFILE**

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

DRAWN BY: T.J.D. ILL. PROJ. NO. GBG-3824
DATE: NOVEMBER, 2006 ASP PROJ. NO. 3-17-0047-B9



STA. 47+74.4
END POROUS FRICTION COURSE

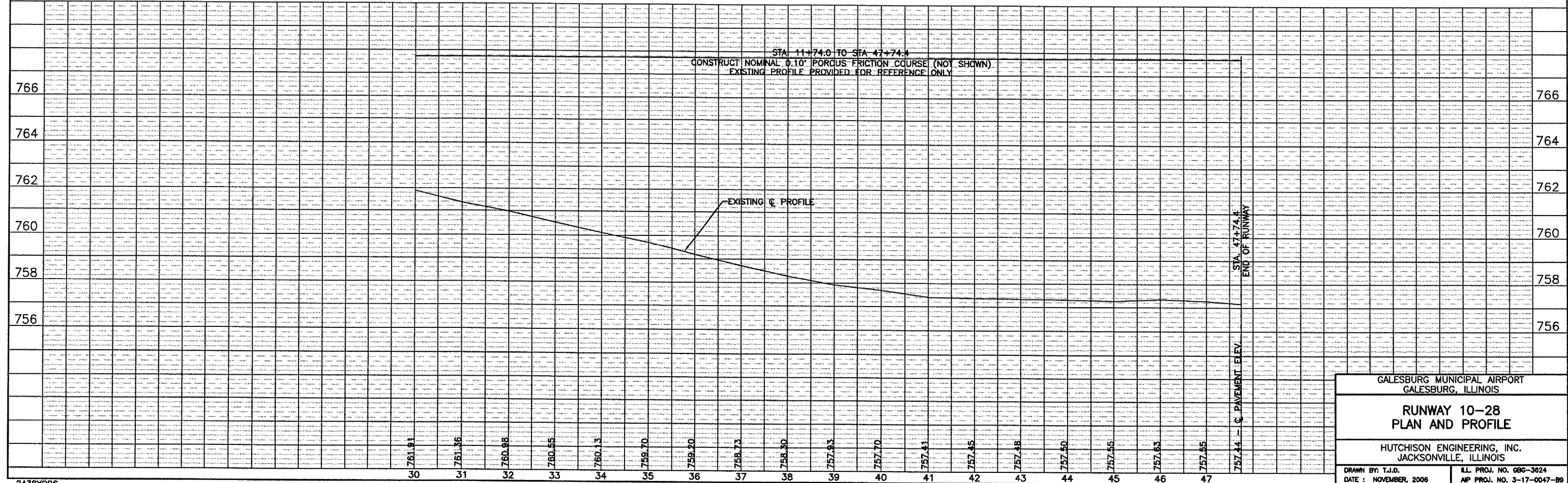
PLACE 5' WIDE x 100' LONG EARTH FILL MATERIAL AT END OF RUNWAY FLUSH WITH TOP OF POROUS FRICTION COURSE - TO BE PAID FOR PER SQ. YD. FOR SHOULDER ADJUSTMENT

BASE COURSE CRACK REPAIR

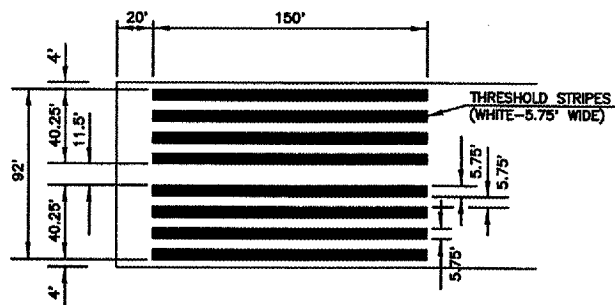
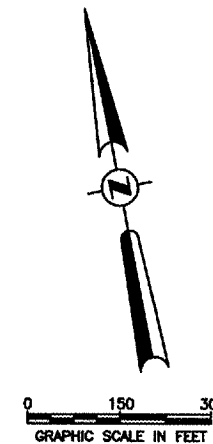
50' LT TO 50' RT	STA. 30+98	=	22.2	SQ. YD.
50' LT TO 50' RT	STA. 32+37	=	22.2	SQ. YD.
50' LT TO 50' RT	STA. 33+89	=	22.2	SQ. YD.
13' LT TO 25' RT	STA. 34+41	=	8.5	SQ. YD.
50' LT TO 50' RT	STA. 35+48	=	22.2	SQ. YD.
50' LT TO 50' RT	STA. 37+28	=	22.2	SQ. YD.
50' LT TO C	STA. 37+93	=	11.1	SQ. YD.
C TO 50' RT	STA. 38+04	=	11.1	SQ. YD.
25' RT TO 50' LT	STA. 38+77	=	16.7	SQ. YD.
50' LT TO 50' RT	STA. 39+57	=	22.2	SQ. YD.
50' LT TO 50' RT	STA. 46+02	=	22.2	SQ. YD.
TOTAL THIS SHEET				= 202.8 SQ. YD.

LEGEND

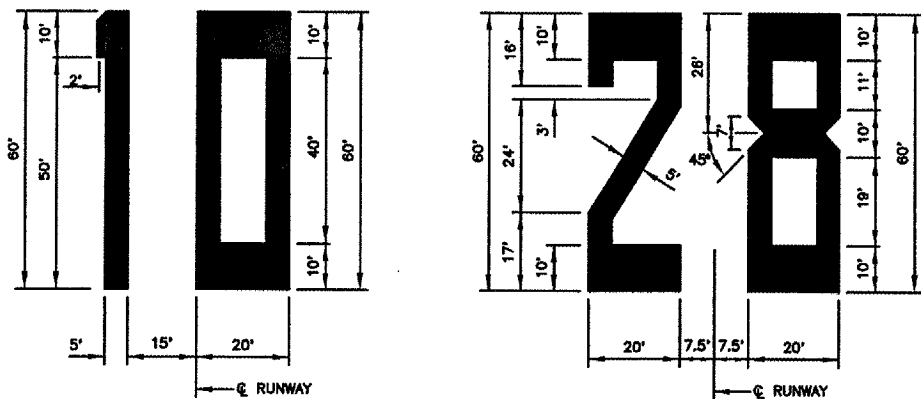
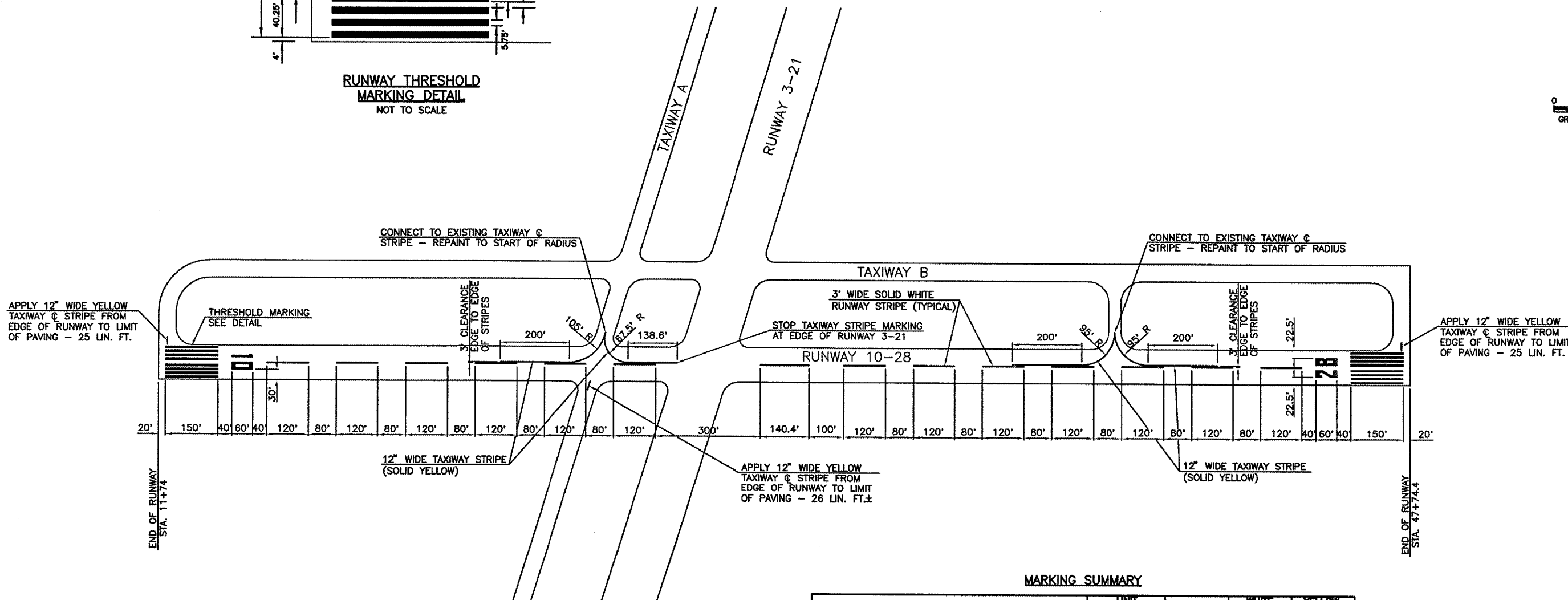
- POROUS FRICTION COURSE, 0.10'
- BITUMINOUS PAVEMENT MILLING FOR BUTT JOINT CONSTRUCTION



GALESBURG MUNICIPAL AIRPORT GALESBURG, ILLINOIS	
RUNWAY 10-28 PLAN AND PROFILE	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: T.J.D. DATE: NOVEMBER, 2006	ILL. PROJ. NO. 686-3024 AP PROJ. NO. 3-17-0047-B9



RUNWAY THRESHOLD MARKING DETAIL
NOT TO SCALE



NUMERAL DETAILS
NOT TO SCALE

MARKING SUMMARY

DESCRIPTION	UNIT AREA	NUMBER REQ'D	WHITE PAINT	YELLOW PAINT
	SQ. FT.		SQ. FT.	SQ. FT.
NUMERAL "0"	800	1	800	
NUMERAL "1"	318	1	318	
NUMERAL "2"	652	1	652	
NUMERAL "8"	876	1	876	
THRESHOLD MARKING	6,900	2	13,800	
RUNWAY CENTERLINE STRIPES (120' LONG)	360	13	4,680	
RUNWAY CENTERLINE STRIPES (140.4' LONG)	421	1	421	
TAXIWAY CENTERLINE STRIPES	1,373			1,373
TOTALS			21,547	1,373

NOTES

1. ALL RUNWAY MARKING SHALL BE SOLID MARKING AND WHITE IN COLOR.
2. ALL TAXIWAY MARKING SHALL BE SOLID MARKING AND YELLOW IN COLOR.
3. ALL PAVEMENT MARKING SHALL CONSIST OF TWO APPLICATIONS OF WATERBORNE PAINT AND ONE APPLICATION OF TYPE B GLASS BEADS (REFLECTIVE MEDIA). PAINT SHALL BE APPLIED AT THE RATE OF 115 SQ. FT./GAL. MAXIMUM PER APPLICATION. GLASS BEADS SHALL BE APPLIED AT THE RATE OF 7 LBS./GAL. MINIMUM.

GALESBURG MUNICIPAL AIRPORT
GALESBURG, ILLINOIS

MARKING LAYOUT

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

DRAWN BY: T.J.D. ILL. PROJ. NO. 090-3624
DATE: NOVEMBER, 2006 AIP PROJ. NO. 3-17-0047-B9