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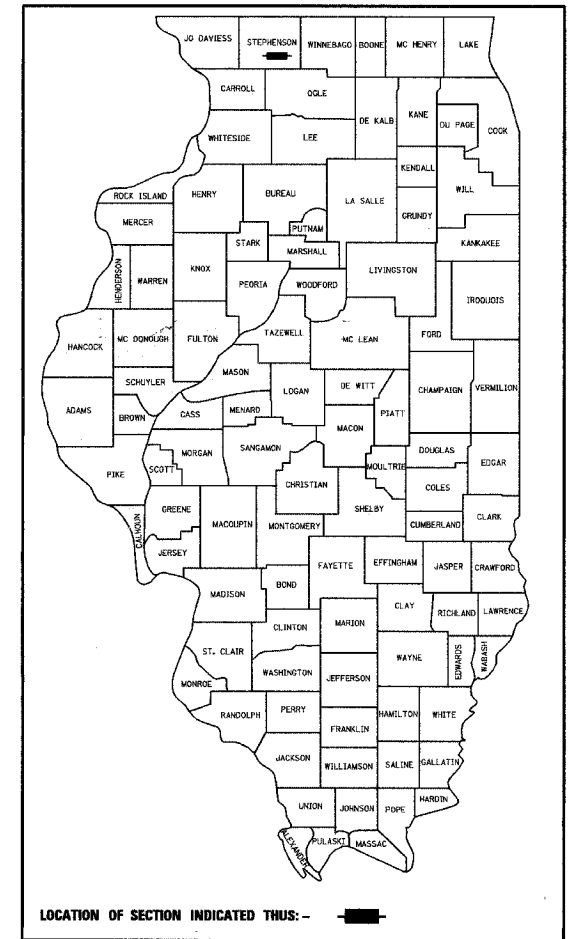
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	292	1
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
• 177-13, 4, 4-1-1				

IDOT LIASON ENGINEER: SAM ABDULLAH (815) 284-5935
 IDOT PROJECT ENGINEER: ROBERT WAGNER (815) 284-5358
 CONSULTANT: HDR, INC. (773) 380-7900
 CONSULTANT: V3 COMPANIES OF ILLINOIS, LTD. (630) 724-9200

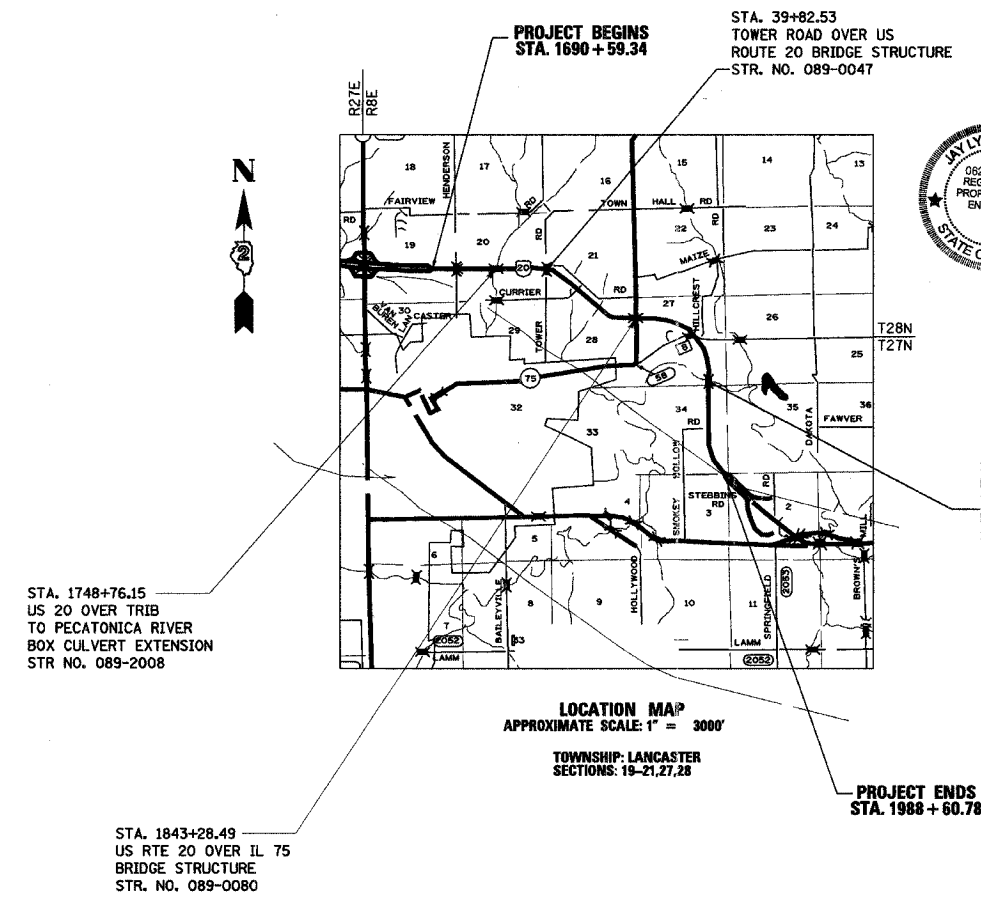
FOR INDEX OF SHEETS SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
F.A.P. ROUTE 301 (US RTE. 20)
SECTION 177 - (3,4,4-1)-1
PROJECT NO. NHF - 301 (034)
STEPHENSON COUNTY
C-92-012-03

D-92-102-00



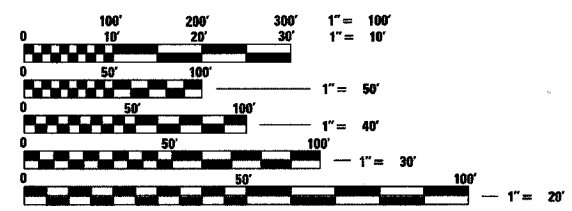
DESIGN DESIGNATION
 US ROUTE 20: 1070(24) OPA
 ILLINOIS RTE 75: 440 (24) MIN. ARTERIAL
 SPRINGFIELD RD: LOCAL ROAD



Jeffrey A. Meindl
 10/27/05 DATE
 IL REGISTRATION 062-054021 Expires 11/30/05
 V3 COMPANIES OF ILLINOIS, LTD.
 TEL 630-724-9270
 PLAN SHEETS: 1-5, 10-12, 15-17, 24-41, 43, 68-73, 86, 98-99, 108, 128-142, 225-247



Jay Chiglo
 10/19/2005 DATE
 IL REGISTRATION 062-054360 Expires 11/30/2005
 HDR ENGINEERING, INC.
 TEL 773-380-7900
 PLAN SHEETS: 6-9, 13-14, 16-23, 44-47, 74-85, 89-97, 100-107, 109-127, 143-225, 249-299



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123

GROSS AND NET LENGTH OF PROJECT = 21441 FT = 4.06 MI (US ROUTE 20) HDR SECTION
 GROSS AND NET LENGTH OF PROJECT = 3288 FT = 0.62 MI (IL ROUTE 75) HDR SECTION
 GROSS AND NET LENGTH OF PROJECT = 8361 FT = 1.58 MI (US ROUTE 20) V3 SECTION

TOTAL GROSS AND NET LENGTH OF PROJECT = 33,090 FT = 6.27 MI



V3 Companies
 7325 Jones Avenue
 Woodridge, IL 60517
 630.724.9200 phone
 630.724.9202 fax
 www.v3coo.com

Visio, Vertere, Virtute... "The Vision" To Transform with Excellence



HDR Engineering, Inc.

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DISTRICT 2 DIXON, IL

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
 SUBMITTED October 27 2005
Gregory L. Monte
 DEPUTY DIRECTOR OF HIGHWAYS, REGION TWO ENGINEER
 2005
 ENGINEER OF PROJECT DEVELOPMENT AND IMPLEMENTATION
December 9, 2005
Mike Stone
 ENGINEER OF DESIGN AND ENVIRONMENT
December 9, 2005
Eric Horn
 DEPUTY DIRECTOR, DIVISION OF HIGHWAYS, CHIEF ENGINEER

CONTRACT NO. 64577

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000001-04	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
202001	EARTH MEDIAN DITCH CHECK
280001-02	TEMPORARY EROSION CONTROL SYSTEMS
406001-02	ENTRANCE RAMP TERMINAL (FLEXIBLE RAMP PAVEMENT ADJACENT TO FLEXIBLE MAINLINE PAVEMENT)
406101-02	EXIT RAMP TERMINAL (FLEXIBLE RAMP PAVEMENT ADJACENT TO FLEXIBLE MAINLINE PAVEMENT)
420001-06	PAVEMENT JOINTS
420101-03	7.2m (24') JOINTED PCC PAVEMENT
420401-05	BRIDGE APPROACH PAVEMENT
420701-01	PAVEMENT FABRIC
442101-05	CLASS B PATCHES
442201-01	CLASS C AND D PATCHES
482001	BITUMINOUS SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
483001-02	PCC SHOULDER
542301	PRECAST REINFORCED CONCRETE FLARED END SECTIONS
542311	GRATING FOR CONCRETE FLARED END SECTION (FOR 600mm (24") THRU 1300mm (54") PIPE)
542401	METAL END SECTION FOR PIPE CULVERT
542531-01	INLET BOX TYPE G, 600mm (24")
542546	FLUSH INLET BOX FOR MEDIAN
601001	SUB-SURFACE DRAINS
601101	CONCRETE HEADWALL FOR PIPE DRAIN
602301	INLET, TYPE A
602401	MANHOLE, TYPE A
602601	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
604001-02	FRAME AND LIDS, TYPE 1
604036-01	GRATE, TYPE 8
606001-02	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-02	PC CONCRETE ISLANDS AND MEDIANS
609006-02	BRIDGE APPROACH PAVEMENT (DRAIN DETAIL)
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630201-03	PCC/BITUMINOUS STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-03	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-02	TRAFFIC BARRIER TERMINAL, TYPE 2
631026-02	TRAFFIC BARRIER TERMINAL TYPE 5 & 5A
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635001	DELINEATORS
635006-02	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-01	REFLECTOR MARKER AND MOUNTING DETAILS
642001	SHOULDER RUMBLE STRIPS
665001-01	WOVEN WIRE FENCE
701001-01	OFF ROAD OPERATIONS, 2L, 2W, MORE THAN 4.5m (15') AWAY
701006-02	OFF-ROAD OPERATIONS 2L, 2W, 4.5m (15') TO 600mm (24") FROM PAVEMENT EDGE
701011-01	OFF-ROAD MOVING OPERATIONS 2L, 2W, DAY ONLY
701101-01	OFF-ROAD OPERATIONS, MULTILANE, 4.5m (15') TO 600mm (24") FROM PAVEMENT EDGE
701106-01	OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 4.5m (15') AWAY
701201-02	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS >= 45 MPH
701301-02	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-01	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311-02	LANE CLOSURE 2L, 2W, MOVING OPERATIONS - DAY ONLY
701400-02	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-03	LANE CLOSURE, FREEWAY/EXPRESSWAY
701402-05	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER
701406-04	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701411-03	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS >= 45MPH
701416-05	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH CROSSOVER AND BARRIER
701422-01	LANE CLOSURE, MULTILANE, FOR SPEEDS >= 45 MPH TO 55 MPH
701426-02	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS >= 45MPH TO 55 MPH
702001-05	TRAFFIC CONTROL DEVICES
704001-02	TEMPORARY CONCRETE BARRIER
720001	SIGN PANEL MOUNTING DETAILS
720006	SIGN PANEL ERECTION DETAILS
720011	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
720021-01	SIGN PANELS, EXTRUDED ALUMINUM TYPE
729001	APPLICATION OF TYPE A AND B METAL POSTS (FOR SIGNS AND MARKERS)
780001-01	TYPICAL PAVEMENT MARKINGS
781001-02	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

CONTRACT NO. 64577				
F.A.P. 301	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		STEPHENSON	290	02
STA.	TO STA.			
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	
• 177-13, 4, 4-10-1				

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS STATE STANDARDS US ROUTE 20

SCALE: VERT.
HORIZ.
DATE 10/28/05

DRAWN BY TVN
CHECKED BY JAM

SUMMARY OF QUANTITIES

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	3
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

← 80% FED 20% STATE →

CODE NO.	Item	Unit	I000-2A		I000-2A		CODE NO.	Item	Unit	I000-2A		I000-2A	
			TOTAL QUANTITY	ROADWAY						TOTAL QUANTITY	ROADWAY		
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNITS	150	150			44004250	PAVED SHOULDER REMOVAL	SQ YD	20103	20103		
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNITS	240	240			44200120	PAVEMENT PATCHING, TYPE II, 10 INCH	SQ YD	216	216		
20200100	EARTH EXCAVATION	CU YD	57535	57535			44200124	PAVEMENT PATCHING, TYPE III, 10 INCH	SQ YD	20	20		
20800150	TRENCH BACKFILL	CU YD	13	13			44200126	PAVEMENT PATCHING, TYPE IV, 10 INCH	SQ YD	47	47		
* 21101825	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	95050	95050			44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	294	294		
* 25000310	SEEDING, CLASS 4	ACRE	0.5	0.5			44200974	CLASS B PATCHES, TYPE III, 10 INCH	SQ YD	16	16		
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1328	1328			44200976	CLASS B PATCHES, TYPE IV, 10 INCH	SQ YD	128	128		
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1328	1328			44213100	PAVEMENT FABRIC	SQ YD	144	144		
* 25000800	POTASSIUM FERTILIZER NUTRIENT	POUND	1328	1328			44213200	SAW CUTS	FOOT	1930	1930		
* 25001830	SEEDING, CLASS 6 (MODIFIED)	ACRE	14.25	14.25			48100100	AGGREGATE SHOULDERS, TYPE A	TON	8,629	8,629		
* 25001845	SEEDING CLASS 7 (SPECIAL)	ACRE	16.25	16.25			48101200	AGGREGATE SHOULDERS, TYPE B	TON	506	506		
* 25100115	MULCH, METHOD 2	ACRE	14.75	14.75			48202400	BITUMINOUS SHOULDERS SUPERPAVE 6"	SQ YD	64381	64381		
25100630	EROSION CONTROL BLANKET	SQ YD	24632	24632			48300300	PORTLAND CEMENT CONCRETE SHOULDERS, 8"	SQ YD	1083	1083		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	7375	7375			48300410	PORTLAND CEMENT CONCRETE SHOULDERS 9 1/2"	SQ YD	219	219		
28000300	TEMPORARY DITCH CHECKS	EACH	139	139			48300500	PORTLAND CEMENT CONCRETE SHOULDERS 10"	SQ YD	2726	2726		
28000400	PERIMETER EROSION BARRIER	FOOT	1092	1092			50105220	PIPE CULVERT REMOVAL	FOOT	401	401		
28000500	INLET AND PIPE PROTECTION	EACH	3	3			54213447	END SECTIONS 12"	EACH	6	6		
28100105	STONE RIPRAP, CLASS A3	SQ YD	8	8			54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	4	4		
28200200	FILTER FABRIC	SQ YD	8	8			54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	1	1		
31100100	SUB-BASE GRANULAR MATERIAL, TYPE A	TON	102866	102866			54213693	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 48"	EACH	1	1		
31200100	STABILIZED SUBBASE, 4"	SQ YD	12960	12960			54244405	FLUSH INLET BOX FOR MEDIAN, STANDARD 542546	EACH	1	1		
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	236	236			54246405	INLET BOX, STANDARD 542531	EACH	2	2		
40600895	CONSTRUCTING TEST STRIP	EACH	2	2			54247190	GRATING FOR CONCRETE FLARED END SECTION 48"	EACH	1	1		
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SQ YD	343	343			60100060	CONCRETE HEADWALL FOR PIPE DRAINS	EACH	143	143		
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	141	141			60100945	PIPE DRAINS 12"	FOOT	238	238		
40702700	FURNISH PROFILOGRAPH	L.SUM	1	1			60107600	PIPE UNDERDRAINS 4"	FOOT	57121	57121		
40800020	BITUMINOUS MATERIALS (PRIME COAT)	TON	24	24			60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	4243	4243		
40800030	AGGREGATE (PRIME COAT)	TON	225	225			60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2		
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	10640	10640			60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	8	8		
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	564		564		60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	1	1		
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	1684		1684		60618300	CONCRETE MEDIAN SURFACE, 4"	SQ FT	3122	3122		
44000100	PAVEMENT REMOVAL	SQ YD	24352	24352			60900315	TYPE D INLET BOX, STANDARD 609006	EACH	6	6		
44000116	BITUMINOUS REMOVAL OVER PATCHES 4"	SQ YD	630	630			60900515	CONCRETE THRUST BLOCKS	EACH	6	6		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	39	39			61100500	EXPLORATION TRENCH 52" DEPTH	FOOT	200	200		
44003100	MEDIAN REMOVAL	SQ FT	80	80			61133200	FIELD TILE JUNCTION VAULTS, 3' DIA	EACH	5	5		

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
US ROUTE 20

SCALE: VERT. N/A
 HORIZ. N/A
 DATE 10/28/05

DRAWN BY TVN
 CHECKED BY JAM

* SPECIALTY ITEM

SUMMARY OF QUANTITIES

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	4
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

← 80% FED 20% ST →

CODE NO.	Item	Unit	1000-2A		CODE NO.	Item	Unit	1000-2A		
			TOTAL QUANTITY	ROADWAY				TOTAL QUANTITY	ROADWAY	
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	2762.5	0	2762.5	* 78003130	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"	FOOT	7435	7435
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	3	0	3	* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	146	146
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	2	0	2	* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	89127	89127
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	8	0	8	* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	1576	1576
* 63100169	TRAFFIC BARRIER TERMINAL TYPE 1, SPECIAL (FLARED)	EACH	9	0	9	* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	678	678
63200310	GUARDRAIL REMOVAL	FOOT	3025	3025		* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	851	851
63500105	DELINEATORS	EACH	350	350		* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	94	94
66500105	WOVEN WIRE FENCE, 4 FT	FOOT	317	317		* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1430	1430
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	16	16		* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	25	25
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	16	16		78100300	REPLACEMENT REFLECTOR	EACH	84	84
67100100	MOBILIZATION	L SUM	1	1		78200100	MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	144	144
70100315	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	EACH	1	1		78200410	GUARDRAIL MARKERS, TYPE A	EACH	53	53
70100410	TRAFFIC CONTROL AND PROTECTION, STANDARD 701416	EACH	2	2		78200510	BARRIER WALL MARKERS, TYPE A	EACH	9	9
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	7	7		78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	14	14
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1		78300100	PAVEMENT MARKING REMOVAL	SQ FT	20213	20213
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1		78300200	REMOVE RAISED REFLECTIVE PAVEMENT MARKERS	EACH	380	380
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1		X4074170	POLYMERIZED BITUMINOUS CONCRETE PAVEMENT, (FULL DEPTH), SUPERPAVE, 14"	SQ YD	90165	90165
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1		A2C10863	TREE, JUGLANS CINEREA (BUTTERNUT HICKORY), CONTAINER GROWN	EACH	100	100
70100805	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	L SUM	1	1		542A0217	PIPE CULVERTS, CLASS A, TYPE 1 12"	FOOT	74	74
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	100	100		542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	281	281
70300100	SHORT TERM PAVEMENT MARKING	FOOT	59957	59957		542A1057	PIPE CULVERTS, CLASS A, TYPE 2 12"	FOOT	40	40
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	1100	1100		542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	500	500
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	227	227		A2C022G3	TREE, CARYA ILINOENSIS (PECAN), CONTAINER GROWN	EACH	300	300
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	28	28		A2C030G3	TREE, FAXINUS PENNSYLVANICA (GREEN ASH), CONTAINER GROWN, 3-GALLON	EACH	300	300
70400100	TEMPORARY CONCRETE BARRIER	FOOT	3852.5	3852.5		A2C040G3	TREE, PLATANUS OCCIDENTALIS (SYCAMORE), CONTAINER GROWN	EACH	300	300
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	3075	3075		A2C050G3	TREE, QUERCUS BICOLOR (SWAMP WHITE OAK), CONTAINER GROWN, 3-GALLON	EACH	300	300
* 72000200	SIGN PANEL - TYPE 2	SQ FT	91	91		A2C060G3	TREE, QUERCUS PALUSTRIS (PIN OAK), CONTAINER GROWN, 3-GALLON	EACH	300	300
* 72000300	SIGN PANEL - TYPE 3	SQ FT	1909	1909		X0322054	REMOVAL OF PRECAST FLARED END SECTION	EACH	2	2
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	7	7		X0322261	REMOVE AND RELAY PRECAST FLARED END SECTION	EACH	1	1
72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	4	4		X0322392	BEVELED PIPE AND GUARD	EACH	4	4
72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	17810	17810		X0323523	REMOVE TEMPORARY LIGHTING	L. SUM	1	1
* 73000100	WOOD SIGN SUPPORT	FOOT	70	70		X0712400	TEMPORARY PAVEMENT	SQ YD	1911	1911
73200100	CONCRETE FOUNDATIONS FOR WOOD SIGN SUPPORT	EACH	4	4		X4068414	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C", N50	TON	10740	10740
* 73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	29.8	29.8		X4068528	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90	TON	3264	3264
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1786	1786		X4068915	POLYMERIZED LEVEL BINDER (MACHINE METHOD), SUPERPAVE, N90	TON	2176	2176

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
US ROUTE 20

SCALE: VERT. N/A
 HORIZ. N/A
 DATE 10/28/05

DRAWN BY TVN
 CHECKED BY JAM

* SPECIALTY ITEM

SUMMARY OF QUANTITIES

CODE NO.	Item	Unit	I000-2A 80% FEB 2007 ST.	
			TOTAL QUANTITY	ROADWAY
X4409410	BITUMINOUS SURFACE REMOVAL, 2 1/4"	SQ YD	2310	2310
X8084201	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06	FOOT	400	400
X8085701	CONCRETE MEDIAN, TYPE SM-4.06	SQ FT	478	478
X7015000	CHANGEABLE MESSAGE SIGNS	CAL MO	16	16
XX004656	EMBANKMENT	CU YD	292	292
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0017100	DOWEL BARS	EACH	940	940
Z0028415	GEOTECHNICAL REINFORCEMENT	SQ YD	283	283
Z0028700	GRANULAR SUB-GRADE REPLACEMENT	CU YD	48	48
⊙ Z0030150	IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3	EACH	4	4
⊙ Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2
⊙ Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	5	5
64Z00105	SHOULDER RUMBLE STRIP	FOOT	63246	63246
Z0065745	SLOTTED DRAIN 12" WITH 2 1/2" SLOT	FOOT	1132	1132
Z0065752	SLOTTED DRAIN 12" WITH 6" SLOT	FOOT	252	252
Z0074100	TERMINATION OF DEAD END ROAD	EACH	1	1
X0322288	MEDIAN CLOSURE	EACH	2	2
† Z0076600	TRAINEES	HOUR	1,000	1,000

⊙ SFTY - 3N
 † Y080

* SPECIALTY ITEM

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

US ROUTE 20

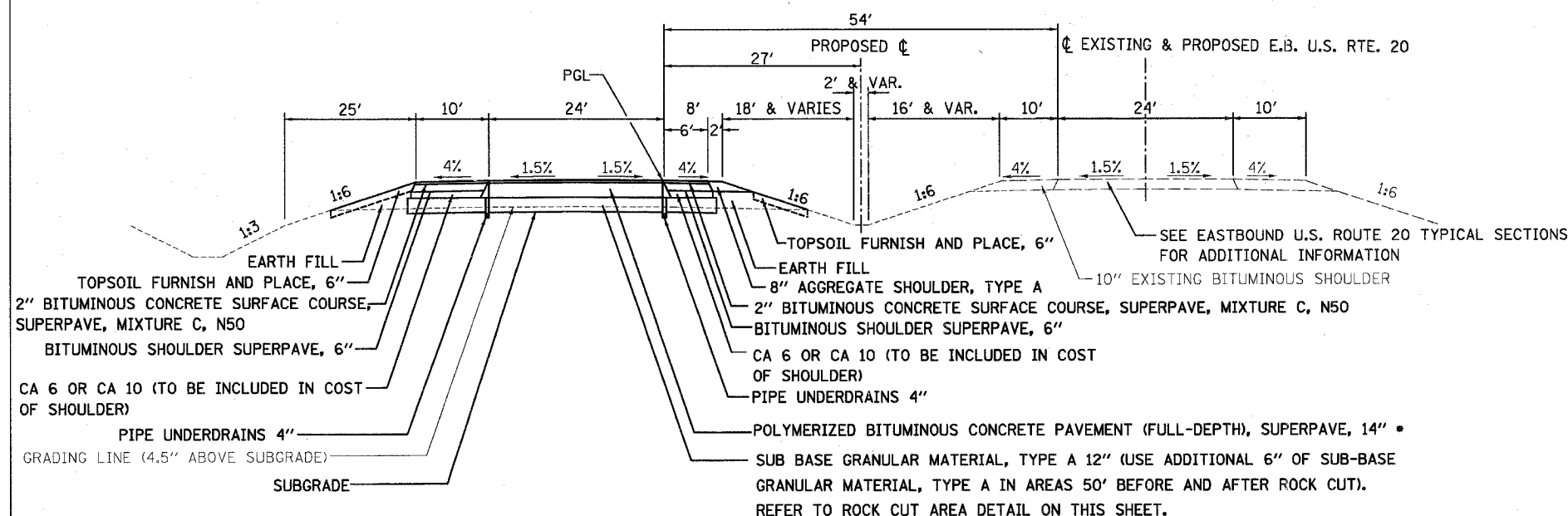
SCALE: VERT. N/A
 HORIZ. N/A

DATE: 10/28/05

DRAWN BY: TVN
 CHECKED BY: JAM

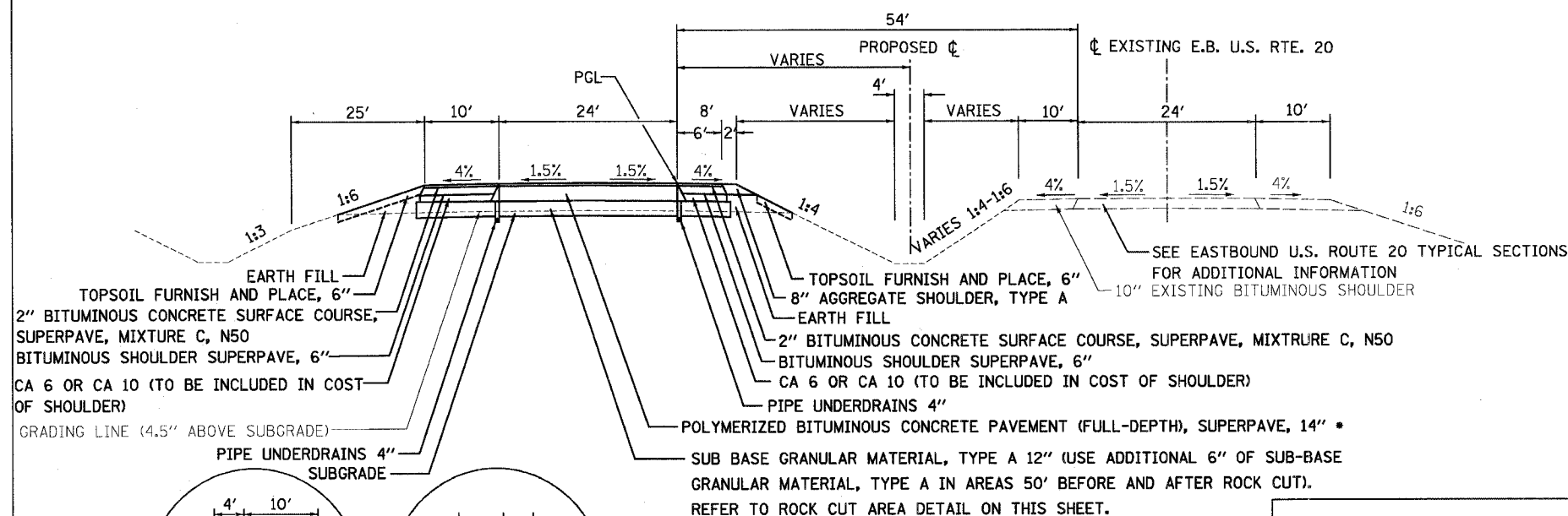
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	6
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		

MAINLINE NORMAL CROWN SECTION
(4 LANE)

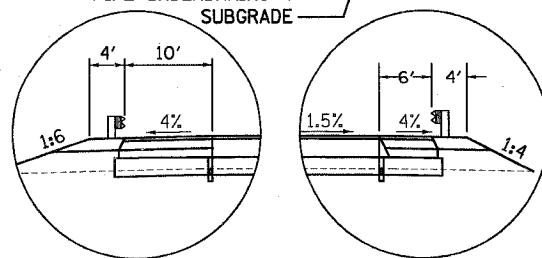


WESTBOUND U.S. ROUTE 20
STATION 1697+00.00 TO STATION 1713+56.00
STATION 1724+42.00 TO STATION 1771+68.00
STATION 1798+77.00 TO STATION 1812+37.00

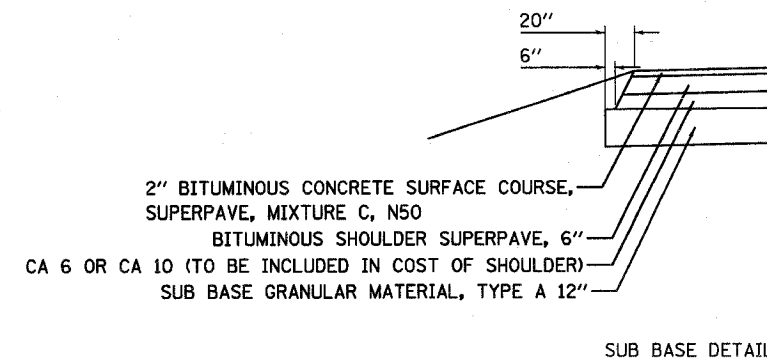
MAINLINE NORMAL CROWN SECTION
(4 LANE)



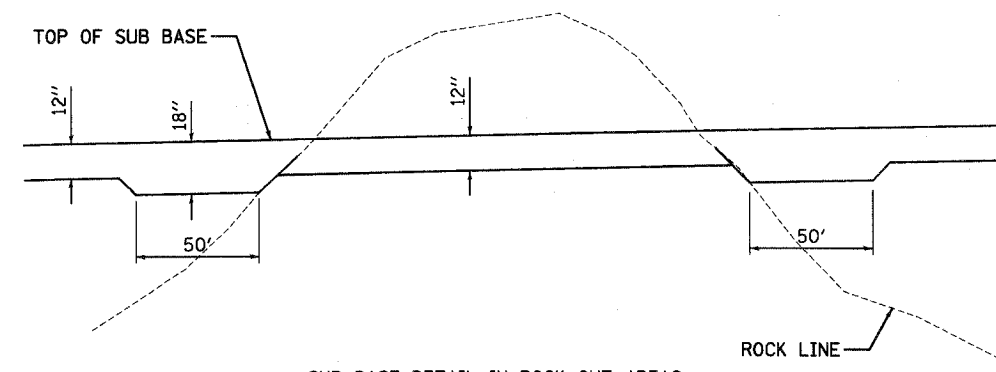
WESTBOUND U.S. ROUTE 20
STATION 1840+66.00 TO STATION 1842+03.95
STATION 1844+55.45 TO STATION 1854+51.00
STATION 1903+13.00 TO STATION 1904+34.31



GUARDRAIL TYPICAL SECTION
SEE SCHEDULE FOR GUARDRAIL LOCATION



SUB BASE DETAIL



SUB BASE DETAIL IN ROCK CUT AREAS

• POLYMERIZED BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:

- 12" POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N90
- 2" POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90

NOTES :
STANDARD DITCH IS 3' DEEP.
FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
SEE CROSS SECTIONS FOR AREAS TO FURNISH AND PLACE TOPSOIL, 6"
BITUMINOUS CONCRETE SURFACE APPLICATION RATE = 112 LB/SY/IN. FOR TYPE C MIXES
112 LB/SY/IN FOR TYPE D MIXES

SHOULDER NOTE :
RUMBLE STRIPS SHALL BE PLACED ON ALL BITUMINOUS SHOULDERS 6' WIDE AND GREATER.

APPROACH PAVEMENT
STA. 1842+03.95 - STA. 1842+39.95
BRIDGE OMISSION
STA. 1842+39.95 - STA. 1844+19.45
APPROACH PAVEMENT
STA. 1844+19.45 - STA. 1844+55.45
APPROACH PAVEMENT
STA. 1904+34.31 - STA. 1904+70.31

REVISIONS	
NAME	DATE

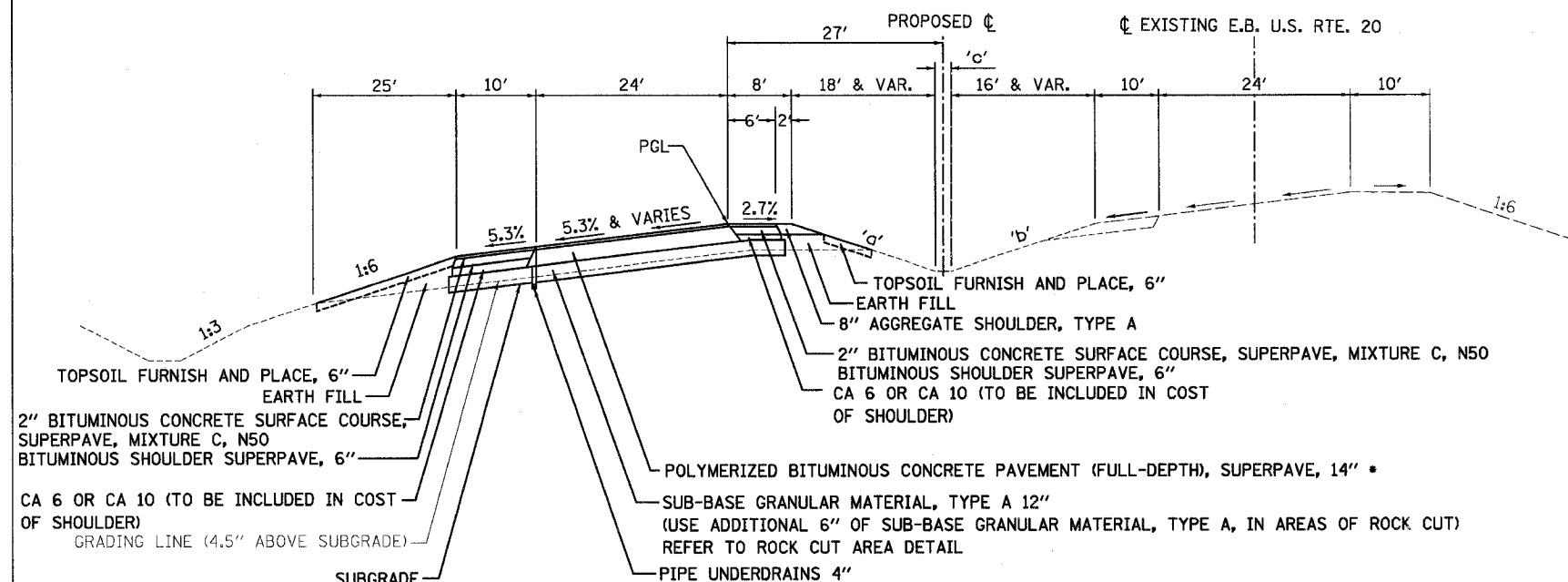
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO.2 DIXON

U.S. ROUTE 20
PAVING TYPICAL SECTIONS

SCALE: VERT. N.T.S.
HORIZ. DATE 7/15/05
DRAWN BY DS
CHECKED BY JC

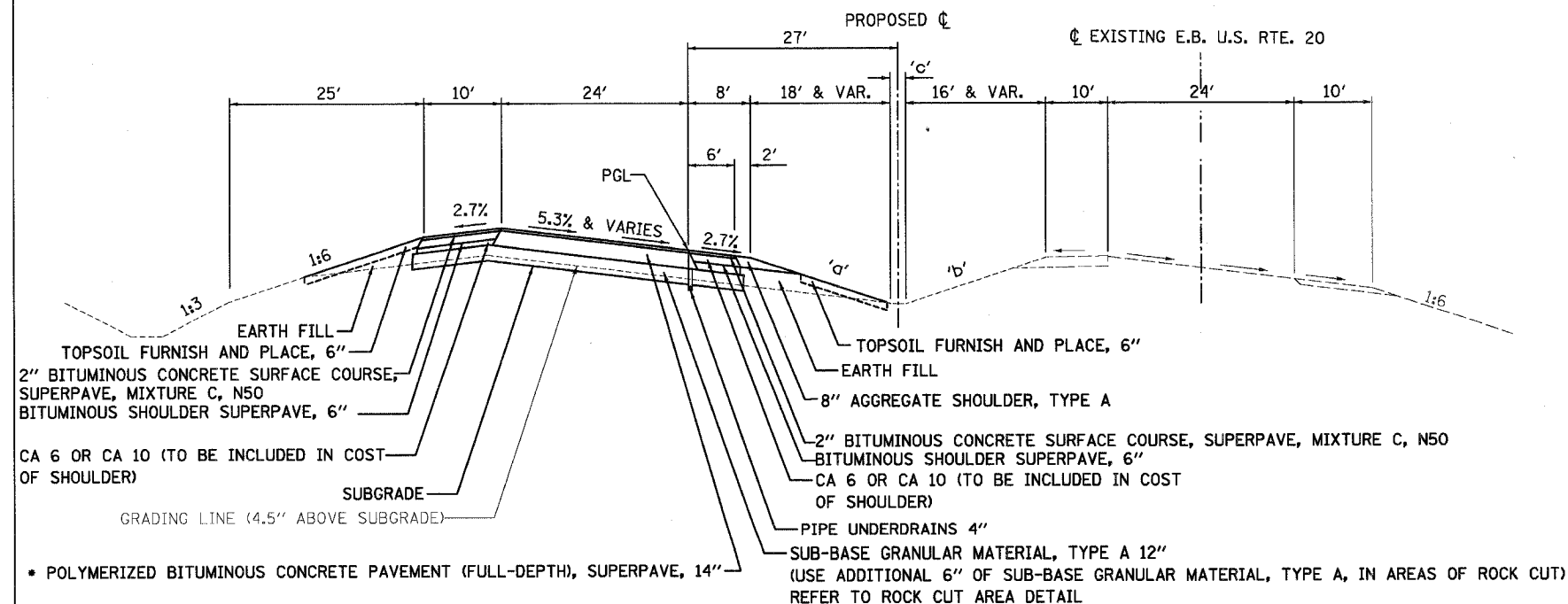
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	7
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		

MAINLINE SUPERELEVATED SECTION
(4 LANE)



WESTBOUND U.S. ROUTE 20 LOCATION	SUPERELEVATION	'a'	'b'	'c'
STATION 1713+56.00 TO STATION 1724+42.00	R.C. 1.5%	1:6	1:6	2' & VAR.
STATION 1812+37.00 TO STATION 1816+00.00	5.3% S.E.	1:6	1:6	2' & VAR.
STATION 1816+00.00 TO STATION 1840+66.00	5.3% S.E.	1:4	1:4 & FLATTER	4'

MAINLINE SUPERELEVATED SECTION
(4 LANE)



WESTBOUND U.S. ROUTE 20 LOCATION	SUPERELEVATION	'a'	'b'	'c'
STATION 1771+68.00 TO STATION 1798+77.00	5.3% S.E.	1:6	1:6	2' & VAR.
STATION 1854+51.00 TO STATION 1857+00.00	5.3% S.E.	1:4	1:4 & FLATTER	4'
STATION 1857+00.00 TO STATION 1886+00.00	5.3% S.E.	1:6	1:6	2' & VAR.
STATION 1886+00.00 TO STATION 1903+13.00	5.3% S.E.	1:4	1:4 & FLATTER	4'

- POLYMERIZED BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:
 - 12" POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N90
 - 2" POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90

NOTES :
 STANDARD DITCH IS 3' DEEP.
 FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
 SEE CROSS SECTIONS FOR AREAS TO FURNISH AND PLACE TOPSOIL, 6"
 BITUMINOUS CONCRETE SURFACE APPLICATION RATE = 112 LB/SY/IN. FOR TYPE C MIXES
 112 LB/SY/IN FOR TYPE D MIXES

SHOULDER NOTE :
 RUMBLE STRIPS SHALL BE PLACED ON ALL BITUMINOUS SHOULDERS 6' WIDE AND GREATER.

REVISIONS	
NAME	DATE

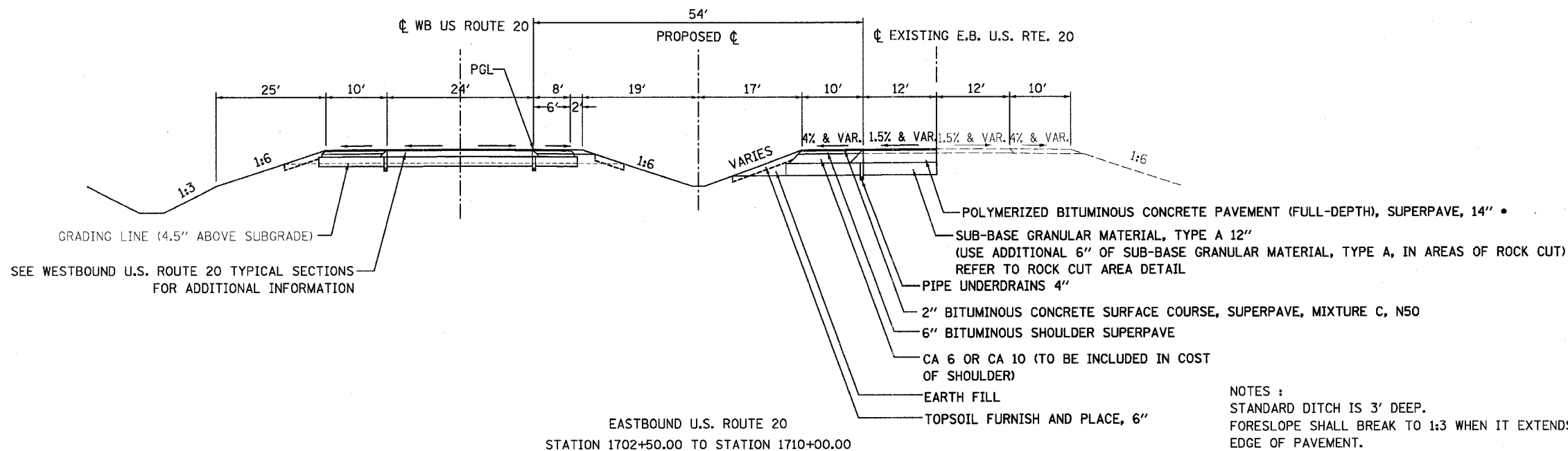
ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO.2 DIXON

U.S. ROUTE 20
 PAVING TYPICAL SECTIONS

SCALE: VERT. N.T.S.
 HORIZ. DATE 7/15/05
 DRAWN BY DS
 CHECKED BY JC

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	8
STA.	TO STA.			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

MAINLINE TANGENT SECTION
(4 LANE)



NOTES :

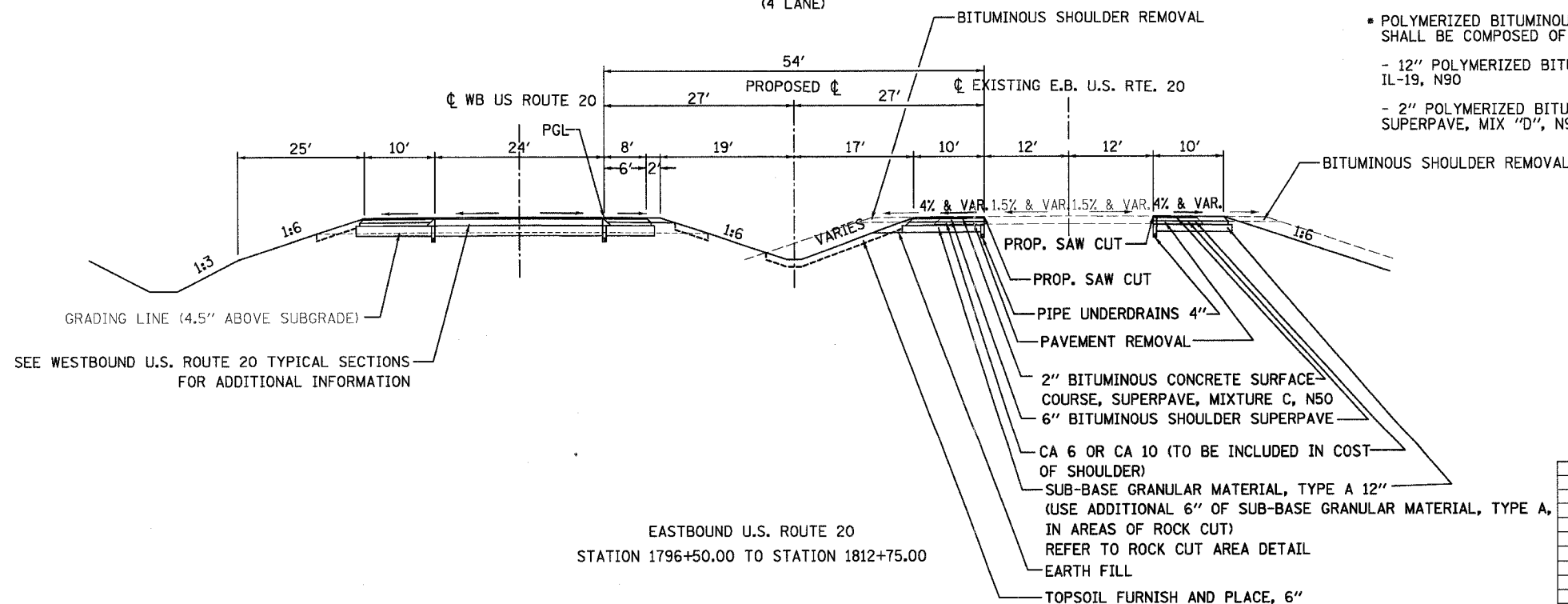
- STANDARD DITCH IS 3' DEEP.
- FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
- SEE CROSS SECTIONS FOR AREAS TO FURNISH AND PLACE TOPSOIL, 6"
- BITUMINOUS CONCRETE SURFACE APPLICATION RATE = 112 LB/SY/IN. FOR TYPE C MIXES
112 LB/SY/IN FOR TYPE D MIXES

SHOULDER NOTE :

RUMBLE STRIPS SHALL BE PLACED ON ALL BITUMINOUS SHOULDERS 6' WIDE AND GREATER.

- POLYMERIZED BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:
 - 12" POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N90
 - 2" POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90

MAINLINE TANGENT SECTION
(4 LANE)



REVISIONS	
NAME	DATE

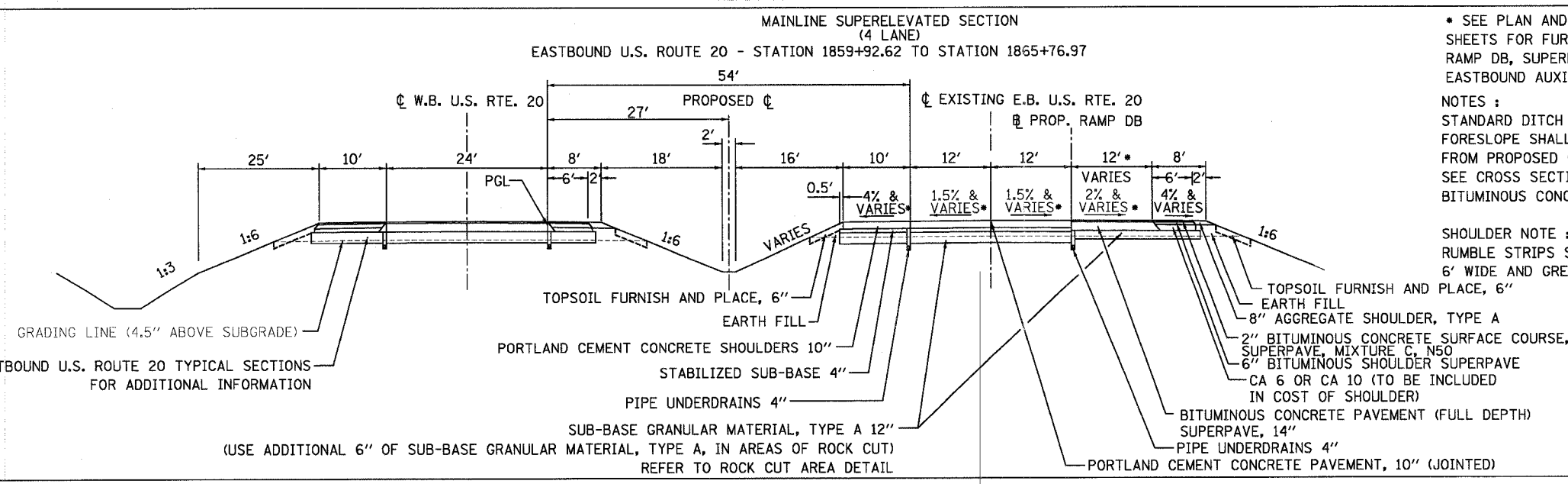
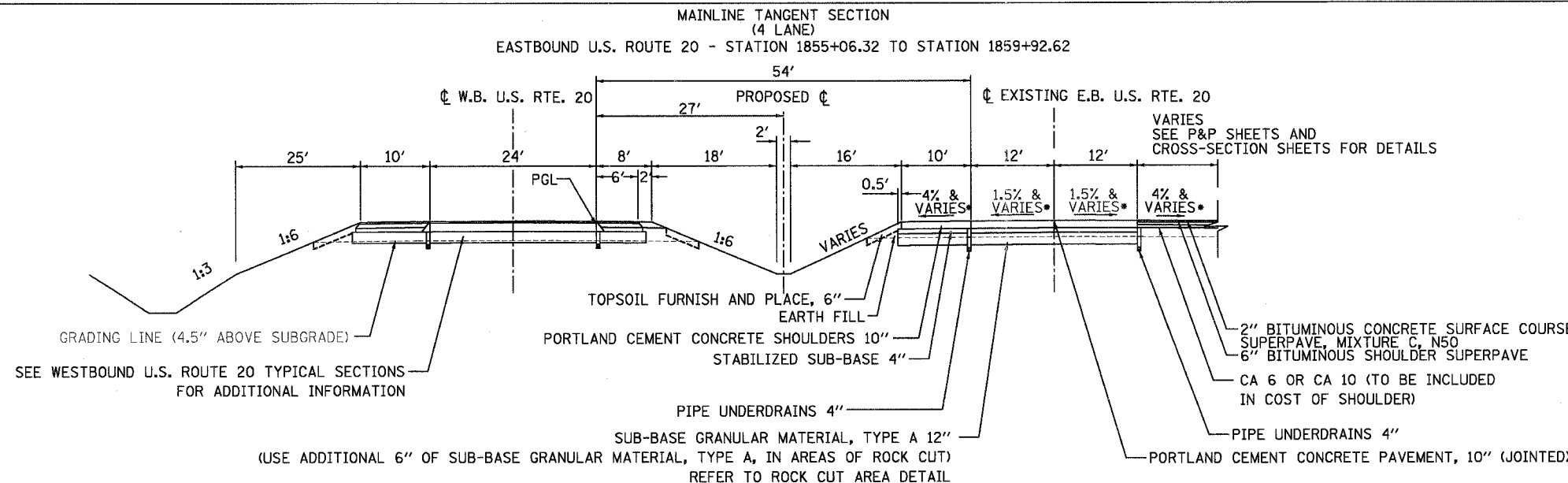
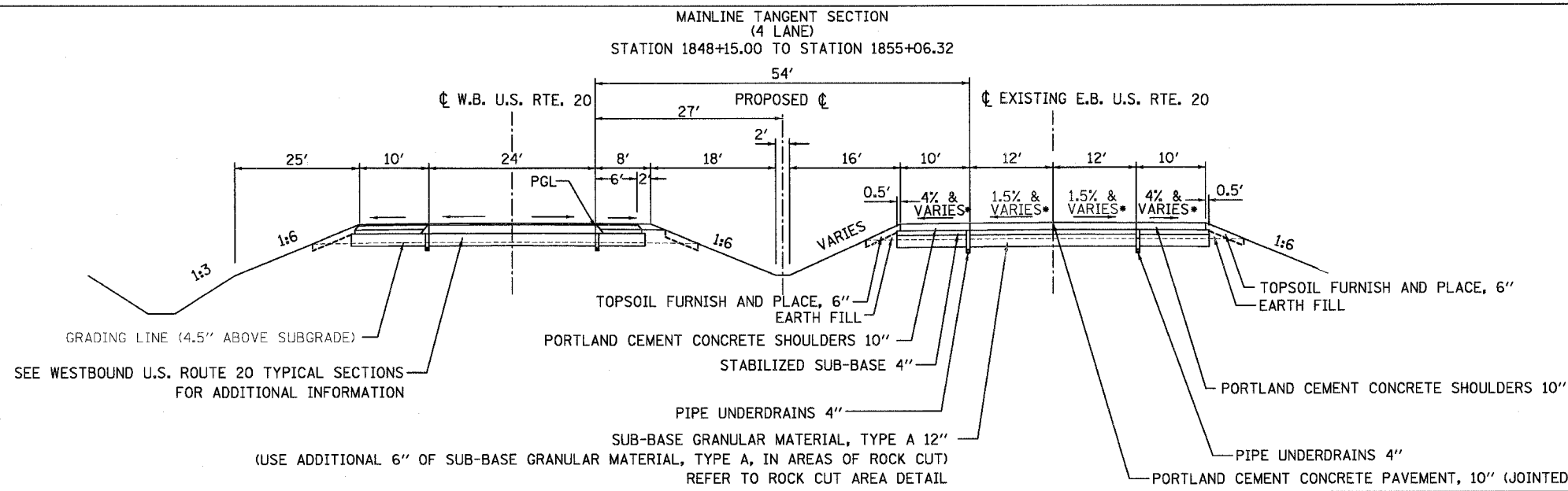
ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO.2 DIXON

U.S. ROUTE 20
PAVING TYPICAL SECTIONS

SCALE: VERT. N.T.S.
HORIZ. N.T.S.
DATE 7/15/05

DRAWN BY JUS
CHECKED BY JLC

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	9
STA.	TO STA.			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



• SEE PLAN AND PROFILE SHEETS AND CROSS SECTION SHEETS FOR FURTHER DETAILS OF EASTBOUND U.S. 20 RAMP DB, SUPERELEVATION DEVELOPMENT, AND PROPOSED EASTBOUND AUXILIARY LANE.

NOTES :

- STANDARD DITCH IS 3' DEEP.
- FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
- SEE CROSS SECTIONS FOR AREAS TO FURNISH AND PLACE TOPSOIL, 6"
- BITUMINOUS CONCRETE SURFACE APPLICATION RATE = 112 LB/SY/IN FOR TYPE C MIXES
112 LB/SY/IN FOR TYPE D MIXES

SHOULDER NOTE :

- RUMBLE STRIPS SHALL BE PLACED ON ALL BITUMINOUS SHOULDERS 6' WIDE AND GREATER.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DISTRICT NO.2 DIXON

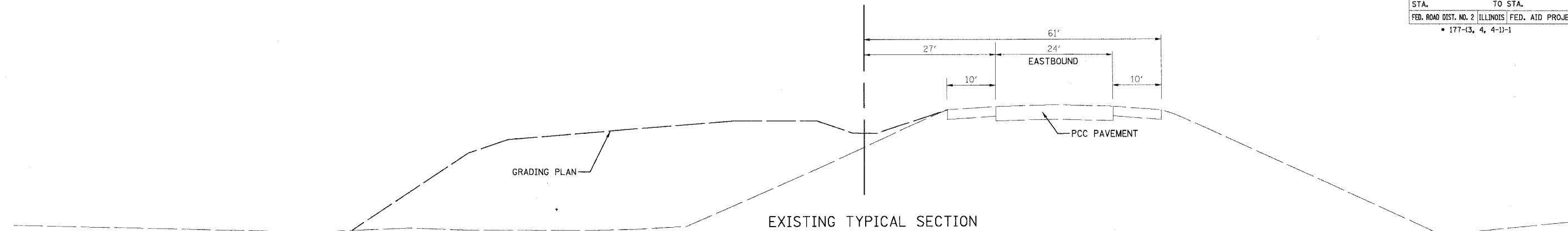
U.S. ROUTE 20
PAVING TYPICAL SECTIONS

SCALE: VERT. N.T.S.
HORIZ. N.T.S.
DATE 7/15/05

DRAWN BY JJS
CHECKED BY JLC

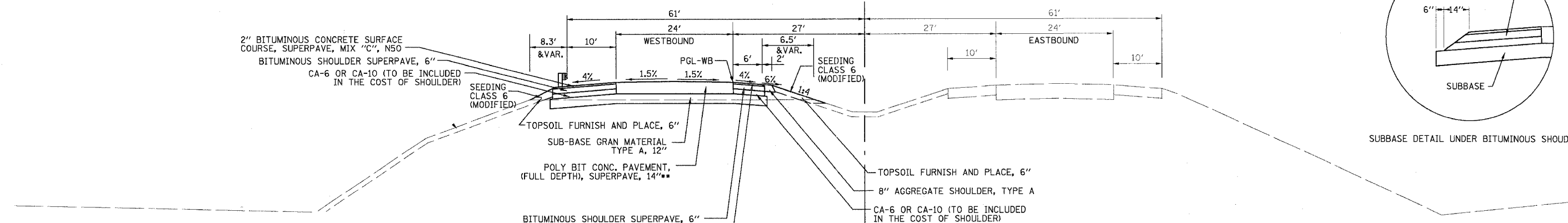
CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	10
STA. TO STA.		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT		
• 177-3, 4, 4-1-1				

PROP \uparrow US ROUTE 20

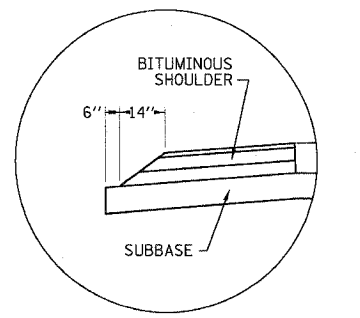


EXISTING TYPICAL SECTION
TANGENT SECTION

PROP \uparrow US ROUTE 20



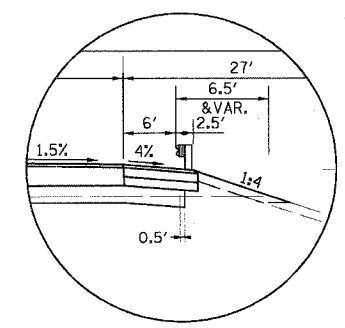
PROPOSED TYPICAL SECTION
STATION 1914+50.00 TO STATION 1927+00.00
GUARDRAIL STATION 1914+41.97 LT TO STATION 1917+04.47 LT



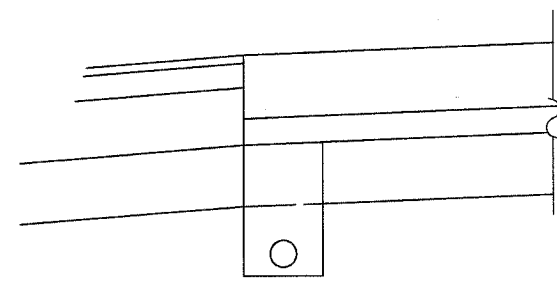
SUBBASE DETAIL UNDER BITUMINOUS SHOULDER

- NOTES:
1. THE CA-16 BACKFILL MATERIAL IN THE PIPE UNDERDRAIN TRENCH SHALL BE INCLUDED IN THE COST OF PIPE UNDERDRAIN AND PIPE UNDERDRAIN (SPECIAL)
 2. STANDARD DITCH IS 3' DEEP.
 3. FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
 4. SEE CROSS SECTIONS FOR AREAS OF TOPSOIL EXCAVATION AND PLACEMENT.
 5. IN AREAS OF ROCK CUT PLACE 12" OF TOPSOIL.
 6. THE FOLLOWING UNIT WEIGHTS WERE USED IN CALCULATING THE BITUMINOUS QUANTITIES:
112/LBS/SY/IN - SURFACE COURSE
112/LBS/SY/IN - BINDER COURSE
115/LBS/SY/IN - SUB-BASE GRANULAR MATERIAL

** POLY BIT CONC. PAVEMENT, (FULL DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:
— 12" POLYMERIZED BITUMINOUS CONCRETE BINDER CSE, SUPERPAVE, IL-19, N90
— 2" POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90



PROPOSED TYPICAL SECTION
STATION 1914+50.00 TO STATION 1927+00.00
GUARDRAIL STATION 1914+41.97 RT TO STATION 1917+66.97 RT



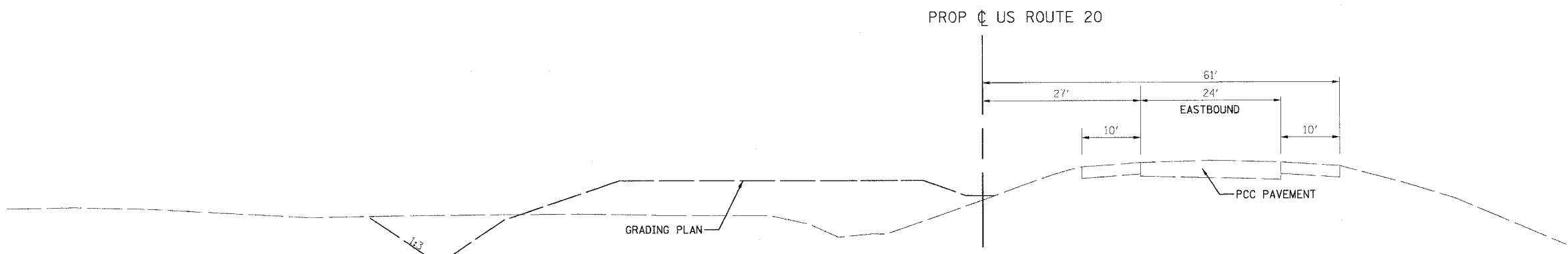
PIPE UNDERDRAINS DETAIL
SEE STANDARD 601001, SUB-SURFACE DRAINS FOR DIMENSIONS
STATION 1914+50 TO STATION 1942+50 BOTH SIDES OF WB PAVEMENT
STATION 1942+50 TO STATION 1969+41 LEFT SIDE ONLY OF WB PAVEMENT
STATION 1969+41 TO STATION 1982+00 BOTH SIDES OF WB PAVEMENT
STATION 1982+00 TO STATION 1969+50 LEFT SIDE ONLY OF EB PAVEMENT

STRUCTURAL DESIGN INFORMATION (FLEXIBLE PAVEMENT)	
DESIGN PERIOD:	20 YEARS
STRUCTURAL DESIGN TRAFFIC: YEAR	2024
PV =	1.795
SU =	325
MU =	1205
ROAD/STREET CLASSIFICATION:	CLASS I
PERCENT OF STRUCTURAL TRAFFIC IN DESIGN LANE:	
P =	32%
S =	45%
M =	45%
TRAFFIC FACTOR: TF =	1.11
MINIMUM SOIL SUPPORT:	
IBR =	3.1 (STA. 1711+00 TO 1904+91.14)

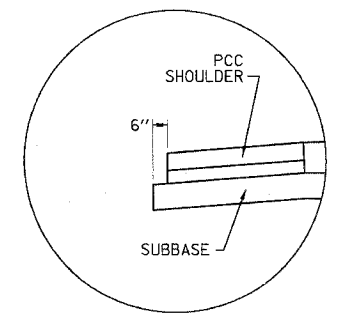
REVISIONS	
NAME	DATE
PRE-FINAL	5/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
US ROUTE 20
SCALE: VERT. N/A
HORIZ. N/A
DATE 10/28/05
DRAWN BY DTF
CHECKED BY TVN

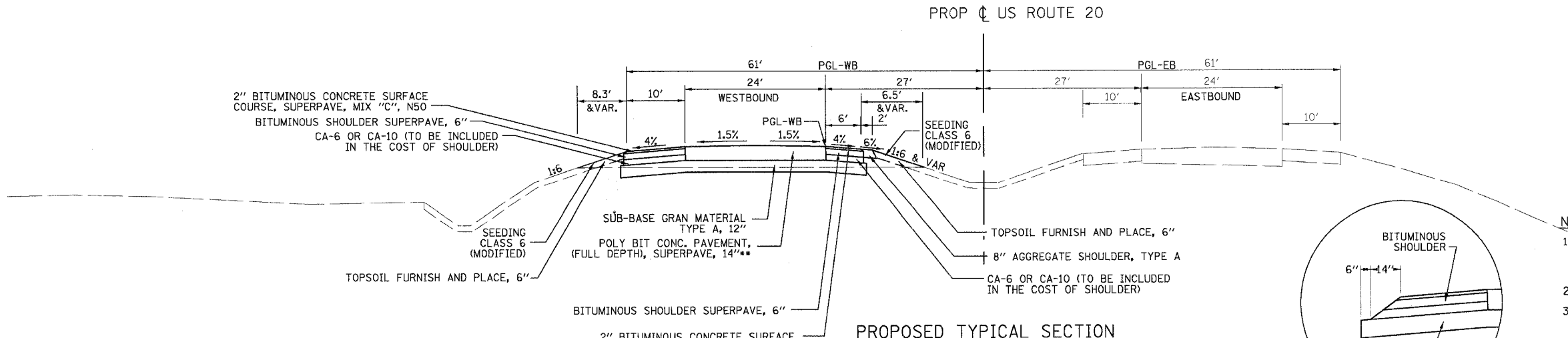
CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	11
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		
• 177-13, 4, 4-1-1				



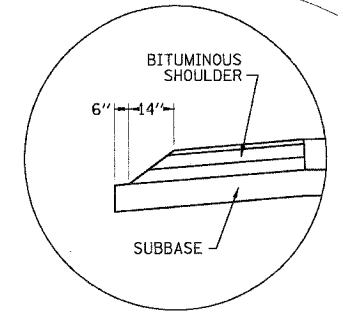
EXISTING TYPICAL SECTION
STATION 1927+00.00 TO STATION 1942+50.00
STATION 1969+41.00 TO STATION 1983+00.00



SUBBASE DETAIL UNDER PCC SHOULDERS

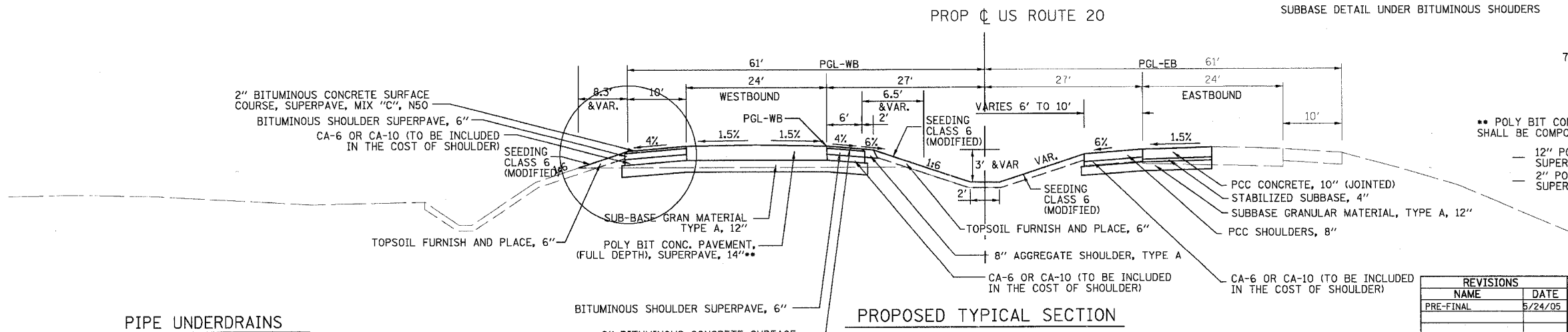


PROPOSED TYPICAL SECTION
STATION 1927+00.00 TO STATION 1942+50.00



SUBBASE DETAIL UNDER BITUMINOUS SHOULDERS

- NOTES:**
1. THE CA-16 BACKFILL MATERIAL IN THE PIPE UNDERDRAIN TRENCH SHALL BE INCLUDED IN THE COST OF PIPE UNDERDRAIN AND PIPE UNDERDRAIN (SPECIAL)
 2. STANDARD DITCH IS 3' DEEP.
 3. FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
 4. SEE CROSS SECTIONS FOR AREAS OF TOPSOIL EXCAVATION AND PLACEMENT.
 5. IN AREAS OF ROCK CUT PLACE 12" OF TOPSOIL.
 6. THE FOLLOWING UNIT WEIGHTS WERE USED IN CALCULATING THE BITUMINOUS QUANTITIES:
112/LBS/SY/IN - SURFACE COURSE
112/LBS/SY/IN - BINDER COURSE
115/LBS/SY/IN - SUB-BASE GRANULAR MATERIAL
 7. SEE MEDIAN CROSS OVER DETAIL SHEET 128 FOR SHOULDERS STA 1977+05 TO STA 1988+60.78



PROPOSED TYPICAL SECTION
STATION 1969+41.00 TO STATION 1982+00.00
EB INSIDE LANE STATION 1969+41.00 TO STATION 1969+50.00
EB INSIDE SHOULDER STATION 1969+41.00 TO STATION 1972+00.00

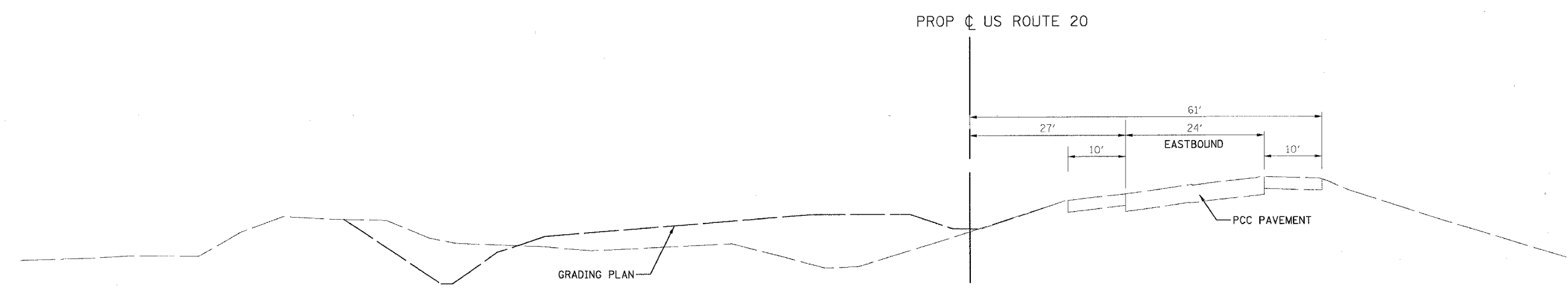
- POLY BIT CONC. PAVEMENT, (FULL DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:
- 12" POLYMERIZED BITUMINOUS CONCRETE BINDER CSE, SUPERPAVE, IL-19, N90
 - 2" POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90

PIPE UNDERDRAINS
STATION 1914+50 TO STATION 1942+50 BOTH SIDES OF WB PAVEMENT
STATION 1942+50 TO STATION 1969+41 LEFT SIDE ONLY OF WB PAVEMENT
STATION 1969+41 TO STATION 1982+00 BOTH SIDES OF WB PAVEMENT
STATION 1964+60 TO STATION 1969+50 LEFT SIDE ONLY OF EB PAVEMENT

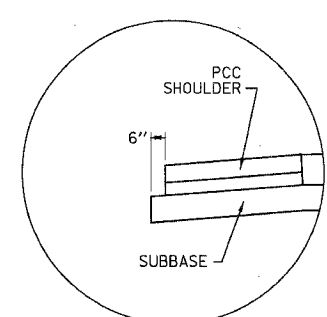
REVISIONS	
NAME	DATE
PRE-FINAL	5/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
US ROUTE 20
SCALE: VERT. 1"=4'
HORIZ. 1"=40'
DATE 10/28/05
DRAWN BY TVN
CHECKED BY JAM

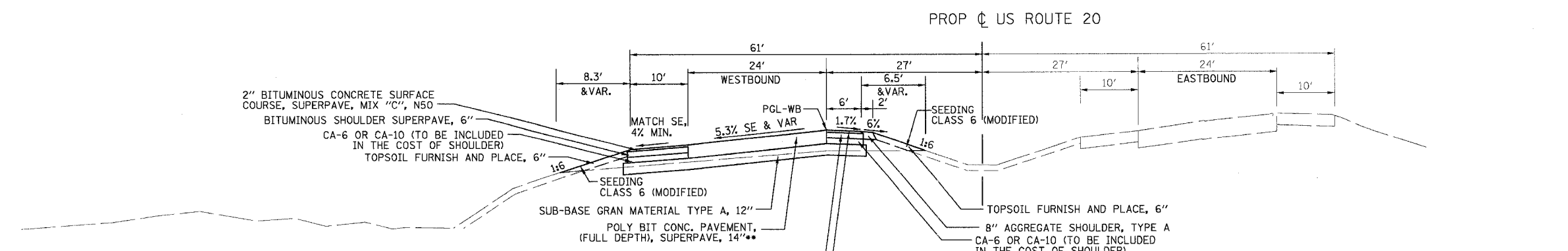
CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	12
STA. TO STA.				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
• 177-13, 4, 4-1-1				



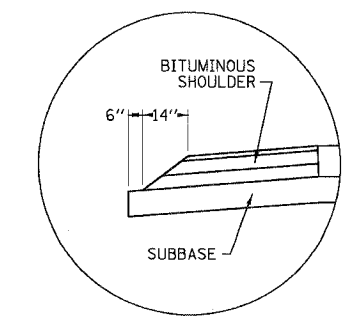
EXISTING TYPICAL SECTION
STATION 1942+50.00 TO STATION 1969+41.00



SUBBASE DETAIL UNDER PCC SHOULDERS

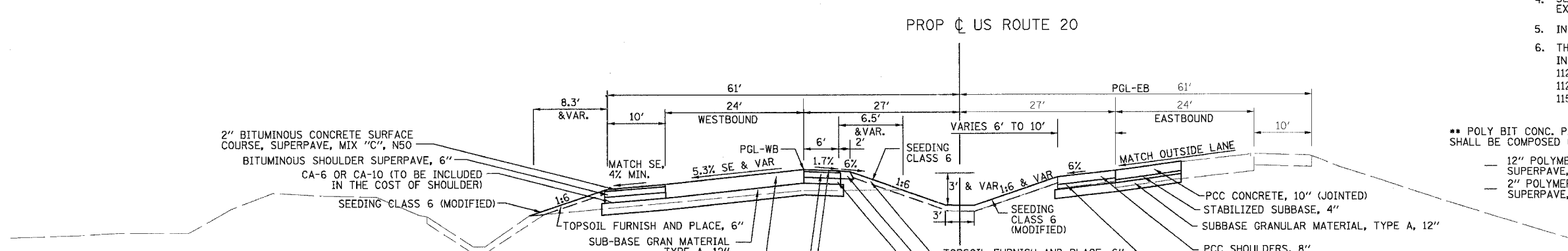


PROPOSED TYPICAL SECTION
STATION 1942+50.00 TO STATION 1957+00.00



SUBBASE DETAIL UNDER BITUMINOUS SHOULERS

- NOTES:**
1. THE CA-16 BACKFILL MATERIAL IN THE PIPE UNDERDRAIN TRENCH SHALL BE INCLUDED IN THE COST OF PIPE UNDERDRAIN AND PIPE UNDERDRAIN (SPECIAL)
 2. STANDARD DITCH IS 3' DEEP.
 3. FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
 4. SEE CROSS SECTIONS FOR AREAS OF TOPSOIL EXCAVATION AND PLACEMENT.
 5. IN AREAS OF ROCK CUT PLACE 12" OF TOPSOIL.
 6. THE FOLLOWING UNIT WEIGHTS WERE USED IN CALCULATING THE BITUMINOUS QUANTITIES:
112/LBS/SY/IN - SURFACE COURSE
112/LBS/SY/IN - BINDER COURSE
115/LBS/SY/IN - SUB-BASE GRANULAR MATERIAL



PROPOSED TYPICAL SECTION
STATION 1957+00.00 TO STATION 1969+41.00

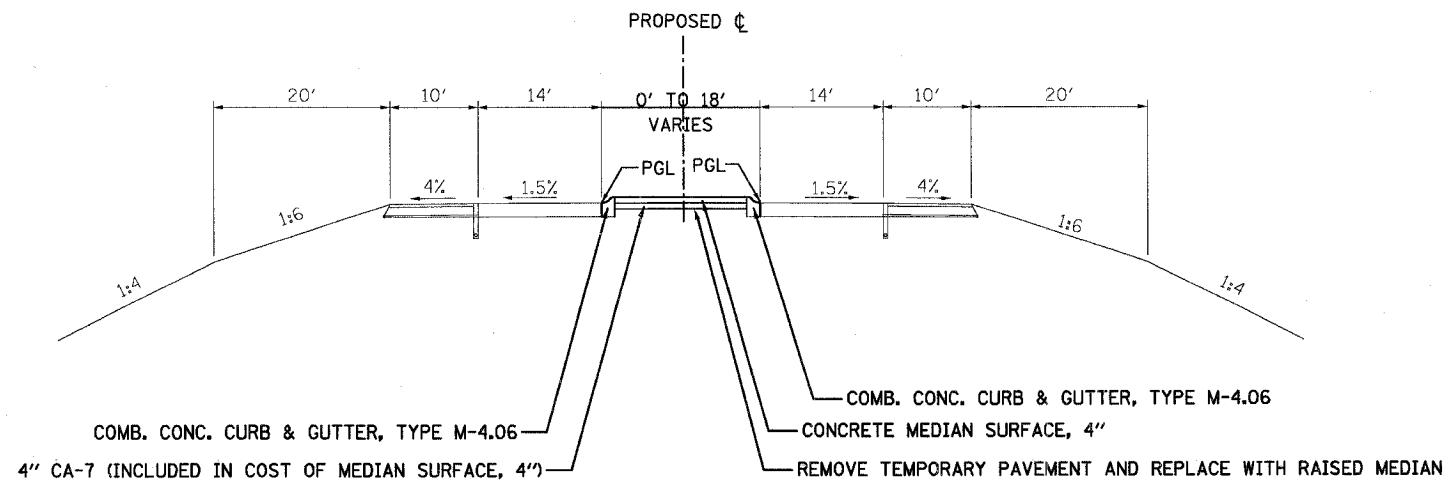
REVISIONS	
NAME	DATE
PRE-FINAL	5/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS
US ROUTE 20
SCALE: VERT. HORIZ.
DATE: 10/28/05
DRAWN BY: TVN
CHECKED BY: JAM

PIPE UNDERDRAINS
STATION 1914+50 TO STATION 1942+50 BOTH SIDES OF WB PAVEMENT
STATION 1942+50 TO STATION 1969+41 LEFT SIDE ONLY OF WB PAVEMENT
STATION 1969+41 TO STATION 1982+00 BOTH SIDES OF WB PAVEMENT
STATION 1964+60 TO STATION 1969+50 LEFT SIDE ONLY OF EB PAVEMENT

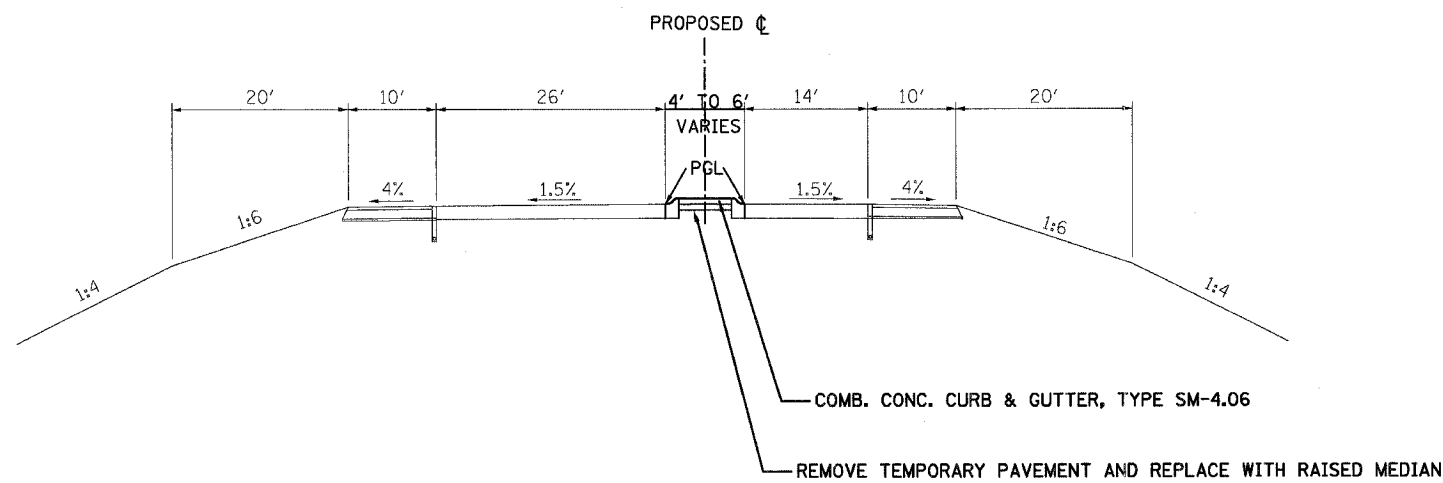
EB INSIDE LANE STATION 1964+60.00 TO STATION 1969+41.00
EB INSIDE SHOULDER STATION 1964+60.00 TO STATION 1969+41.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	13
STA.	TO STA.			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



IL ROUTE 75 TYPICAL CROSS SECTION (RAISED MEDIAN)

STATION 32+00 TO STATION 34+00



IL ROUTE 75 TYPICAL CROSS SECTION (RAISED MEDIAN)

STATION 35+56 TO STATION 36+45

STRUCTURAL DESIGN INFORMATION (RIGID PAVEMENT)

DESIGN PERIOD: 20 YEARS

STRUCTURAL DESIGN TRAFFIC: YEAR 1997
 PV = 3,306 SU = 114 MU = 380

ROAD/STREET CLASSIFICATION: CLASS II

PERCENT OF STRUCTURAL TRAFFIC IN DESIGN LANE:
 P = 50% S = 50% M = 50%

TRAFFIC FACTOR: TF = 5.51

MINIMUM SOIL SUPPORT:
 IBR = 3.1 (STA. 23+25 TO 58+00)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO.2 DIXON

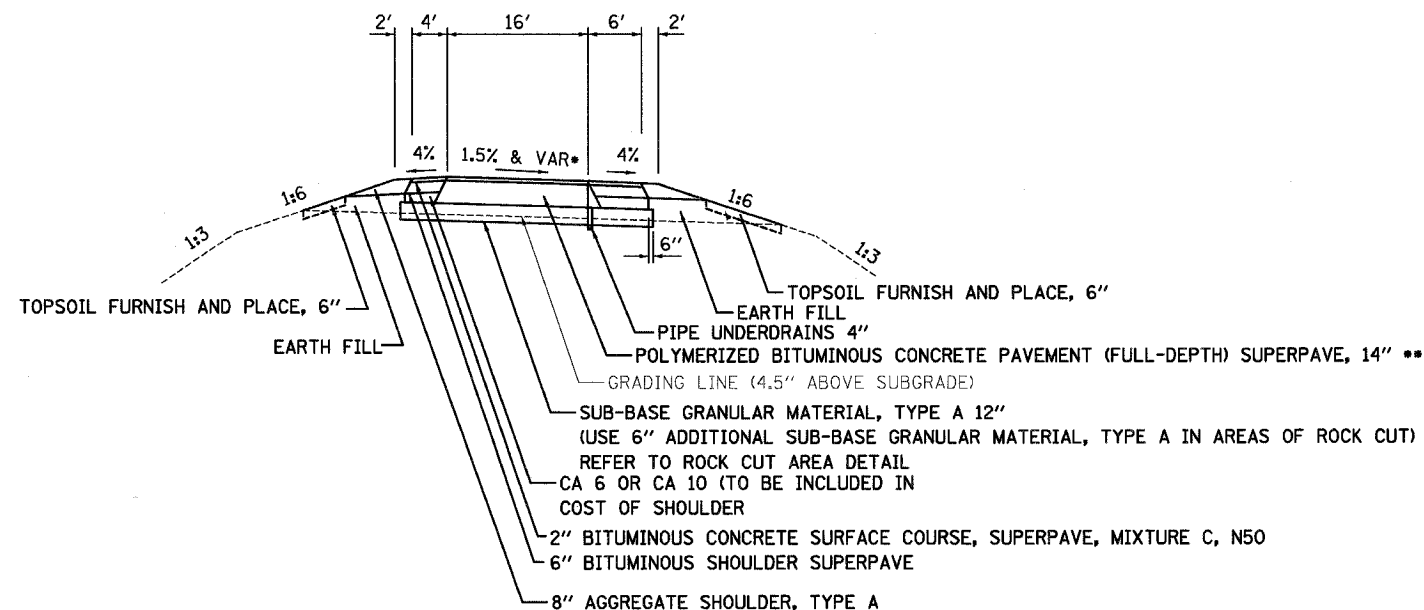
IL ROUTE 75
 TYPICAL SECTION

SCALE: VERT. N.T.S.
 HORIZ. N.T.S.
 DATE 7/15/05

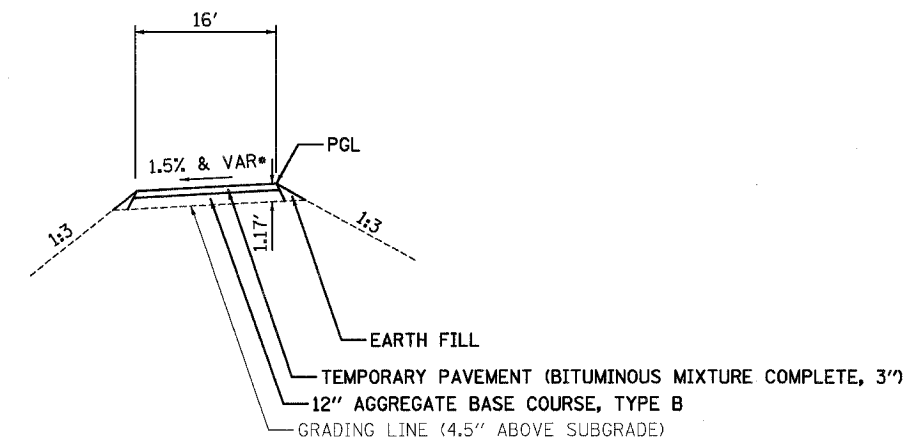
DRAWN BY JJS
 CHECKED BY JLC

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	14
STA.		TO STA.		
FED. ROAD DIST. NO. 2 ILLINOIS		FED. AID PROJECT		

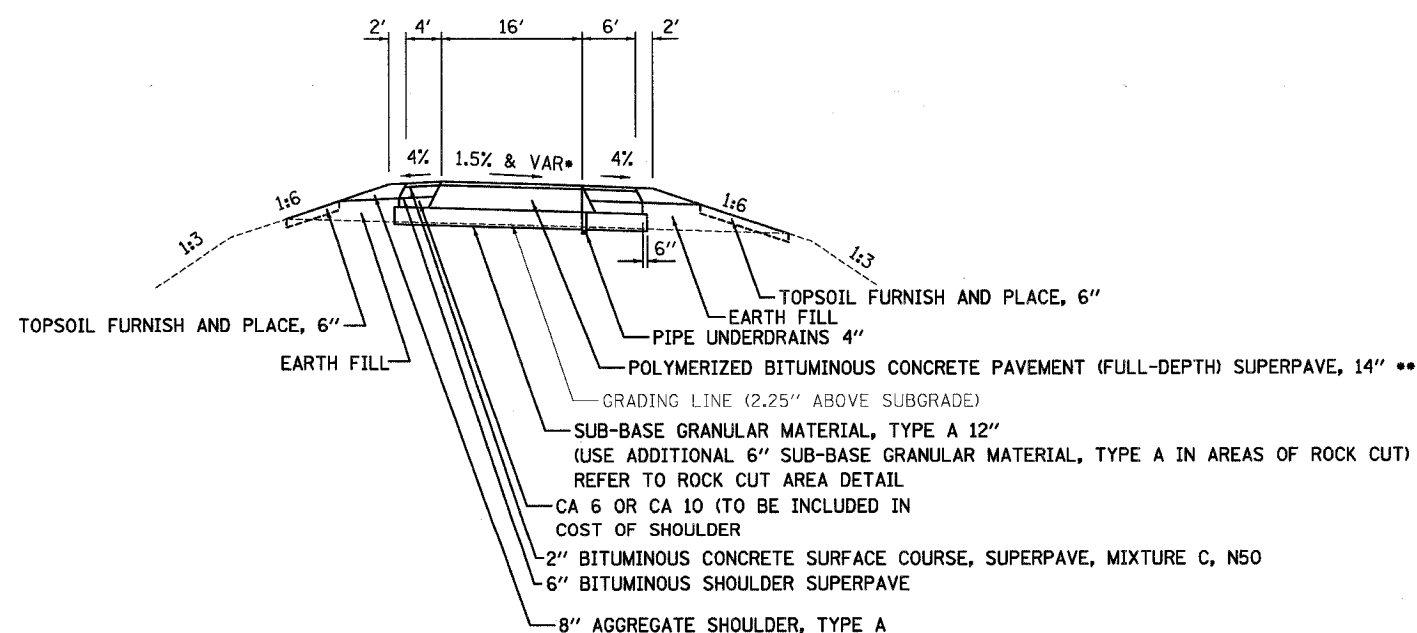
TYPICAL RAMP CROSS SECTION
 RAMP AD (STA. 312+76.26 TO STA. 318+29.95)
 RAMP BC (STA. 100+00.00 TO STA. 118+38.15)
 RAMP CA (STA. 401+71.47 TO STA. 420+00.00)
 RAMP DB (STA. 201+71.47 TO STA. 241+03.12)



TYPICAL CROSS SECTION
 TEMPORARY RAMP
 STATION 500+22 TO STATION 507+58



TYPICAL RAMP CROSS SECTION
 RAMP AD (STA. 300+00.00 TO STA. 312+76.26)



** POLYMERIZED BITUMINOUS CONCRETE PAVEMENT (FULL-DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:

- 12" POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N90
- 2" POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N90

• FOR ADDITIONAL SUPERELEVATION INFORMATION AND STATIONING SEE PLAN & PROFILE AND CROSS SECTION SHEETS

NOTES:

1. SEE CROSS-SECTIONS FOR AREAS TO FURNISH AND PLACE TOPSOIL, 6"
2. BITUMINOUS CONCRETE SURFACE COURSE APPLICATION RATE = 112 LB/SY/IN FOR TYPE C MIXES
112 LB/SY/IN FOR TYPE D MIXES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 DISTRICT NO.2 DIXON

RAMP AND TEMP. RAMP
 TYPICAL SECTION

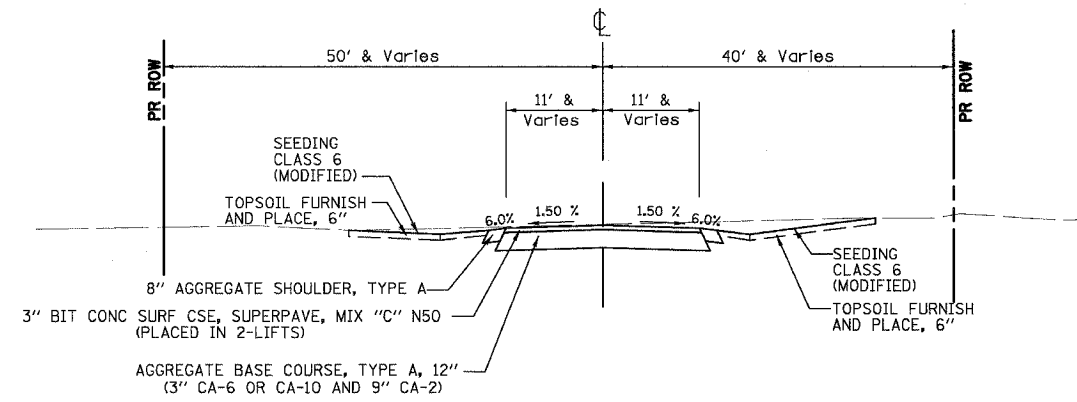
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 HORIZ.
 DATE 7/15/05

DRAWN BY AMV
 CHECKED BY JLC

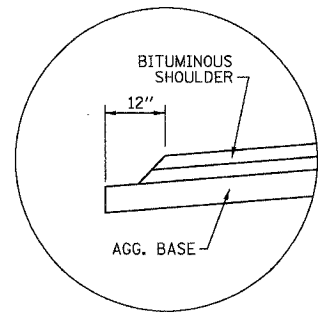
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	15
STA. TO STA.				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

* 177-13, 4, 4-1-1

SMOKEY HOLLOW ROAD



PROPOSED TYPICAL SECTION
STATION 890+90.01 TO STATION 902+76.49



BASE COURSE DETAIL ON SMOKEY HOLLOW ROAD

NOTES:

1. THIS CONTRACT DOES INCLUDE PAVING WORK ON US ROUTE 20 MAINLINE.
2. STANDARD DITCH IS 3' DEEP.
3. FORESLOPE SHALL BREAK TO 1:3 WHEN IT EXTENDS MORE THAN 35' FROM PROPOSED EDGE OF PAVEMENT.
4. SEE CROSS SECTIONS FOR AREAS OF TOPSOIL EXCAVATION AND PLACEMENT.
5. IN AREAS OF ROCK CUT PLACE 12" OF TOPSOIL.
6. THE FOLLOWING UNIT WEIGHTS WERE USED IN CALCULATING THE BITUMINOUS QUANTITIES:
112/LBS/SY/IN - SURFACE COURSE
112/LBS/SY/IN - BINDER COURSE
115/LBS/SY/IN - SUB-BASE GRANULAR MATERIAL

REVISIONS	
NAME	DATE
PRE-FINAL	5/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
SMOKEY HOLLOW ROAD

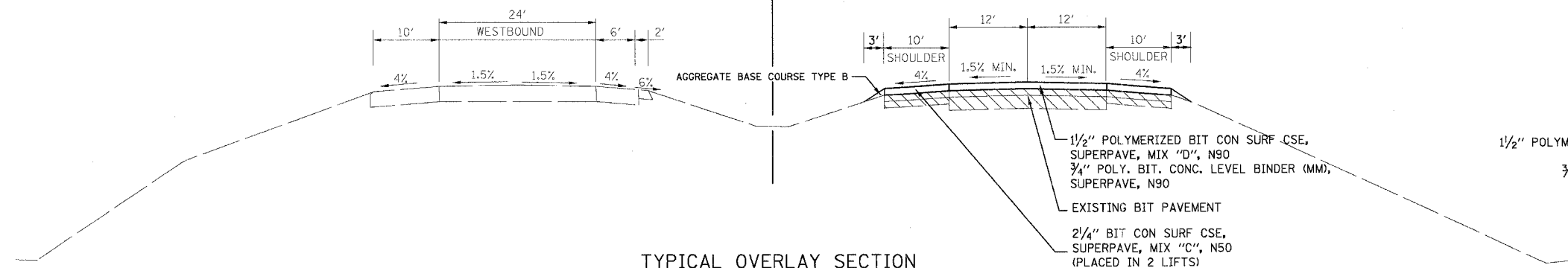
SCALE: VERT. N/A
HORIZ. N/A
DATE 10/28/05

DRAWN BY TVN
CHECKED BY JAM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	16
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		

• 177-(3, 4, 4-D)-1

PROP ϕ US ROUTE 20



TYPICAL OVERLAY SECTION
EXISTING BITUMINOUS PAVEMENT

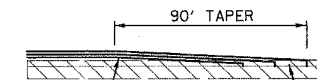
**STATION 1690+59.34 TO STATION 1842+29.38

**PROP RECONSTRUCTION EB
INSIDE SHOULDER
STA 1690+59.34 TO STA 1702+00

PROP RECONSTRUCTION EB
INSIDE LANE & SHOULDER
STA 1702+00 TO STA 1714+00

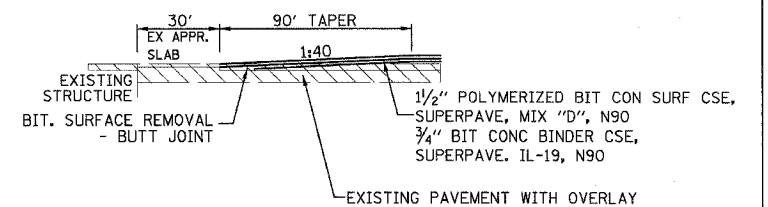
PROP RECONSTRUCTION EB
INSIDE AND OUTSIDE SHOULDERS
STA 1796+50 TO STA 1812+75

PROP RECONSTRUCTION EB
OUTSIDE SHOULDER
STA 1825+22.49 TO STA 1831+72.26

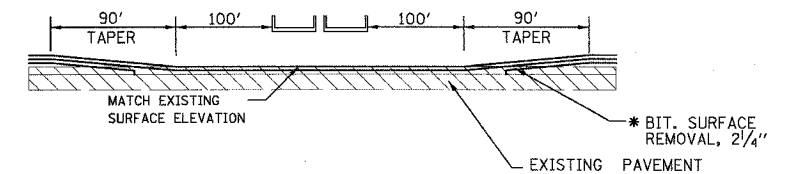


1 1/2" POLYMERIZED BIT CON SURF CSE,
SUPERPAVE, MIX "D", N90
3/4" BIT CONC BINDER CSE,
SUPERPAVE, IL-19, N90

TYPICAL BUTT-JOINT
FOR EXISTING BIT PAVEMENT



TYPICAL BRIDGE TAPER
FOR EXISTING BIT PAVEMENT



TYPICAL TAPER UNDER STRUCTURES

* THE VARIABLE DEPTH MILLING REQUIRED IN THE 130' TAPER SECTIONS TO MAINTAIN CLEARANCE UNDER THE STRUCTURE SHALL BE INCLUDED IN THE COST OF BITUMINOUS SURFACE REMOVAL, 2/4"

NOTES:

1. AGGEGATE PRIME COAT AND BITUMINOUS PRIME COAT SHALL BE PLACE PRIOR TO EACH LAYER OF ASPHALT.
2. THE FOLLOWING UNIT WEIGHTS WERE USED IN CALCULATING THE BITUMINOUS QUANTITIES:
112/LBS/SY/IN - SURFACE COURSE
112/LBS/SY/IN - BINDER COURSE
115/LBS/SY/IN - SUB-BASE GRANULAR MATERIAL

REVISIONS	
NAME	DATE
PRE-FINAL	5/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
US ROUTE 20

SCALE: VERT. N/A
HORIZ. N/A
DATE 10/28/05

DRAWN BY TVN
CHECKED BY TVN

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	17
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		
• 177-C3, 4, 4-D-1				

CURVE DATA AND COORDINATES

LINE	CURVE NO.	CONTROL POINT	STATION	COORDINATES		Δ	T	L	R	e FT/FT	DESIGN SPEED M.P.H.	TIE & REMARKS	
				NORTH	EAST								
US ROUTE 20	10233	POT	1666+43.58	2,060,337.1812	2,440,687.3049								
	240A	CC			2,071,603.2134	2,446,054.5536							
		PC	1715+44.73		2,060,136.3695	2,445,584.3348							
		PI	1718+99.54		2,445,938.8507	2,445,938.8507	3° 32' 30" LT	354.814'	709.402'	11,476.481'			
		PT	1722+54.13		2,060,129.2221	2,446,293.5876							
	250A	CC			2,057,389.2281	2,451,454.4325							
		PC	1773+56.79		2,060,235.5010	2,451,395.1370							
		PI	1785+92.48		2,452,630.5664	2,452,630.5664	46° 55' 36" RT	1,235.697'	2,331.674'	2,846.891'	5.30%		
		PT	1796+88.46		2,059,376.3585	2,453,493.0836							
	260A	CC			2,060,084.1183	2,456,829.8828							
		PC	1815+10.72		2,058,071.4414	2,454,765.0230							
		PI	1827+65.59		2,455,640.9206	2,455,640.9206	47° 02' 11" LT	1,254.867'	2,367.174'	2,883.490'	5.30%		
		PT	1838+77.89		2,057,201.3742	2,456,895.4631							
	270A	CC			2,054,394.9277	2,458,721.9819							
		PC	1856+40.11		2,057,241.4530	2,458,657.2256							
		PI	1885+00.10		2,461,516.4707	2,461,516.4707	90° 15' 20" RT	2,859.985'	4,485.163'	2,847.262'	5.30%		
		PT	1901+25.28		2,054,446.9920	2,461,568.7676							
	280A	CC			2,050,166.8128	2,464,529.2847							
		PC	1944+58.87		2,050,114.1178	2,461,648.0107							
		PI	1956+60.09		2,461,669.9757	2,461,669.9757	45° 15' 22" LT	1,201.212'	2,276.201'	2,881.756'	5.30%		
		PT	1967+35.08		2,048,083.2679	2,462,538.4665							

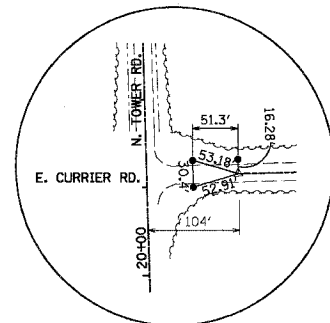
REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
CURVE DATE & COORDINATES
 US ROUTE 20

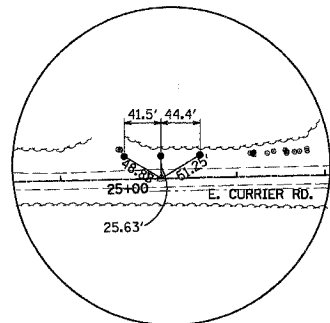
SCALE: VERT. N/A
 HORIZ. N/A
 DATE 10/28/05

DRAWN BY TVN
 CHECKED BY

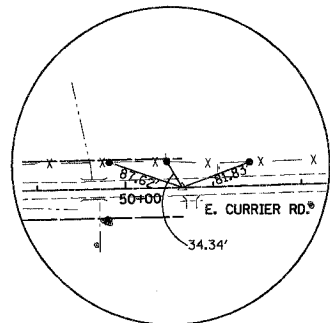
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	18
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		



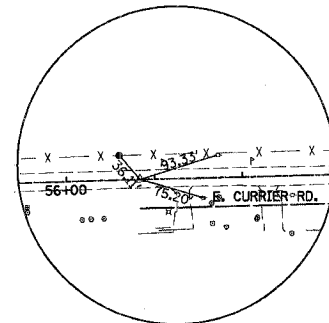
PK NAIL
 POT 16+14.31
 N=2058324.831
 E=2452105.149
 ELEV.=817.052
 ALL TIES SET 5/8" REBAR



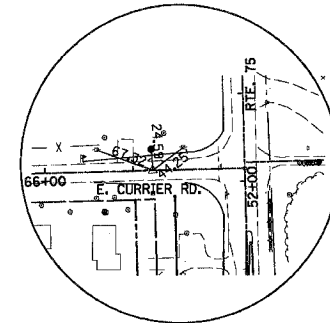
PK NAIL
 POT 25+14.48
 N=2058346.023
 E=2453005.066
 ELEV.=821.106
 ALL TIES SET 5/8" REBAR



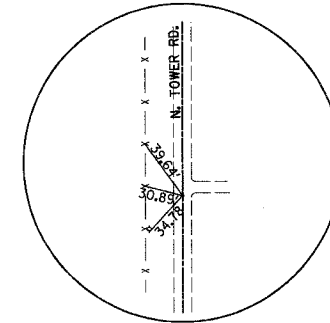
PK NAIL
 POT 50+64.48
 N=2058400.663
 E=2455554.480
 ELEV.=804.278
 ALL TIES ARE NAIL AND WASHER



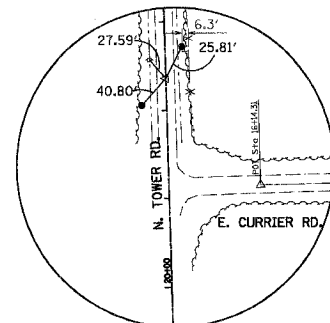
PK NAIL
 POT 56+83.81
 N=2058417.836
 E=2456173.570
 ELEV.=843.111
 ALL TIES ARE NAIL AND WASHER



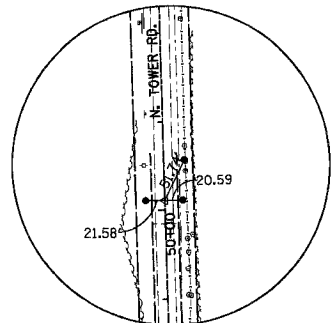
PK NAIL
 POT 67+22.23
 N=2058439.833
 E=2457210.507
 ALL TIES SET 5/8" REBAR
 AND NAIL AND WASHER



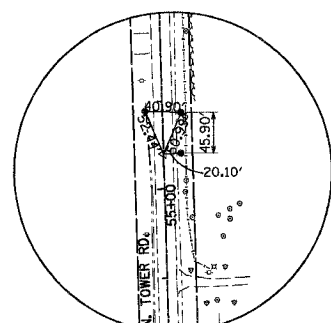
PK NAIL
 13+62.55
 ALL TIES ARE NAIL AND WASHER



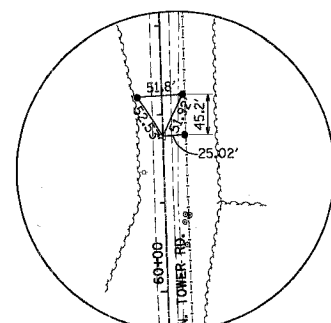
PK NAIL
 22+36.10
 N=2058445.238
 E=2451997.612
 ELEV.=824.792
 TIES ARE SET 5/8" REBAR
 AND NAIL AND WASHER



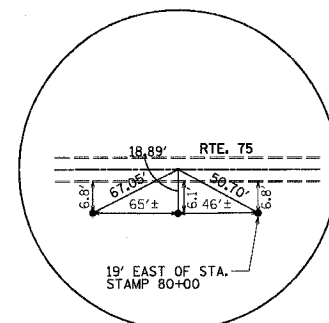
PK NAIL
 50+10.94
 N=2061219.4136
 E=2451934.0873
 ELEV.=825.032
 ALL TIES ARE SET 5/8" REBAR



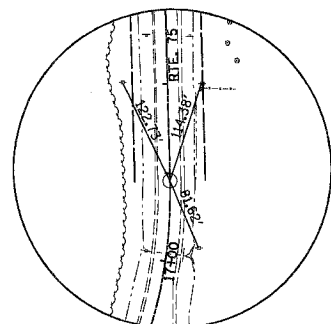
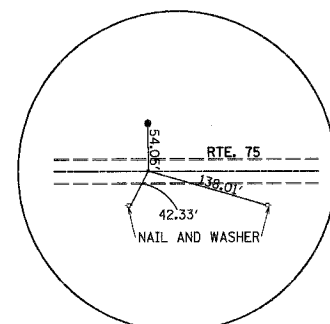
PK NAIL
 55+40.20
 N=2061748.1552
 E=2451919.2744
 ELEV.=847.192
 ALL TIES ARE SET 5/8" REBAR



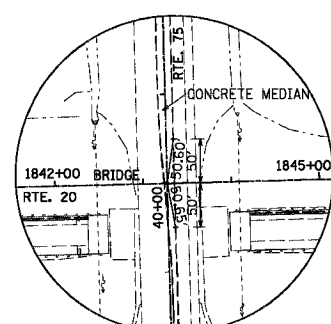
PK NAIL
 61+76.49
 N=2062384.4603
 E=2451899.8872
 ELEV.=839.274
 ALL TIES ARE SET 5/8" REBAR



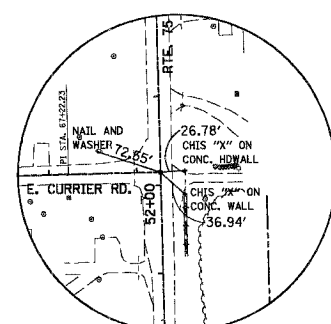
PK NAIL
 POT 0+00.00
 N=2054452.3569
 E=2455923.2301
 ALL TIES SET 5/8" REBAR



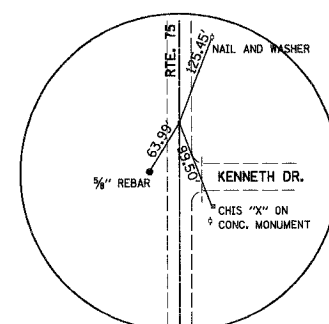
PK NAIL
 PT 17+91.09
 N=2055020.4632
 E=2457398.8316
 ELEV.=761.636
 ALL TIES ARE NAIL AND WASHER



PK NAIL
 40+00.00
 N=2057228.7256
 E=2457345.5285
 ELEV.=810.321
 ALL TIES ARE CHIS. "X"
 OM TOP OF CURB



PK NAIL
 52+14.50
 N=2058442.8710
 E=2457316.2210
 ELEV.=844.486
 TIES ARE CHIS. "X" ON
 TOP OF CURB AND NAIL AND WASHER



PK NAIL
 POT 77+21.97
 N=2060949.614
 E=2457255.7130
 TIES ARE CHIS. "X" ON
 TOP OF CONC. MON.
 NAIL AND WASHER
 AND 5/8" REBAR

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND
 BENCHMARKS

SCALE: VERT.
 HORIZ. NTS
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY JLC

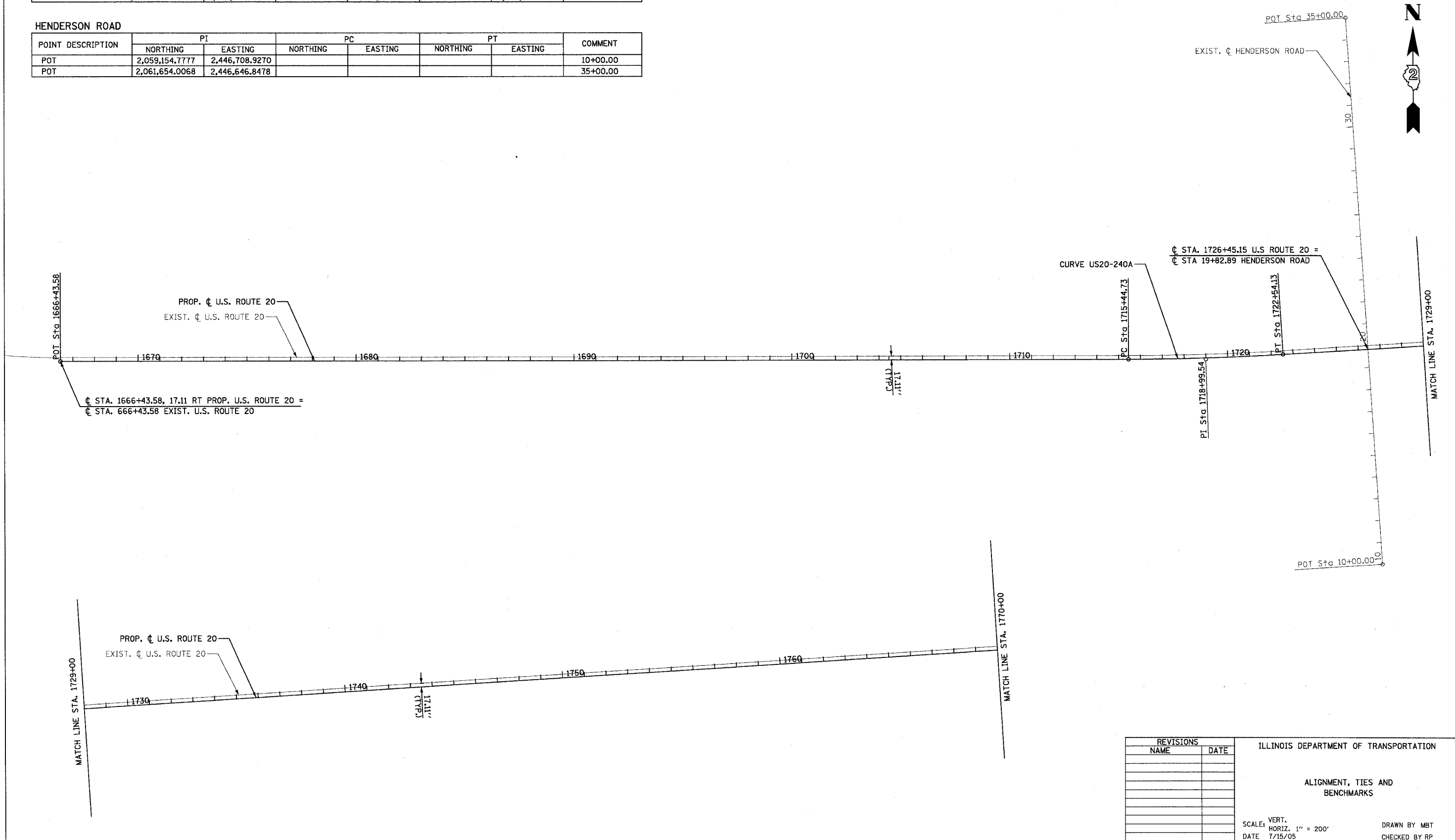
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	19
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		

U.S. ROUTE 20

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2,060,337.1812	2,440,687.3049					1666+43.58
US20-240A	2,060,121.8320	2,445,938.8507	2,060,136.3695	2,445,584.3348	2,060,129.2221	2,446,293.5876	

HENDERSON ROAD

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2,059,154.7777	2,446,708.9270					10+00.00
POT	2,061,654.0068	2,446,646.8478					35+00.00



REVISIONS	
NAME	DATE

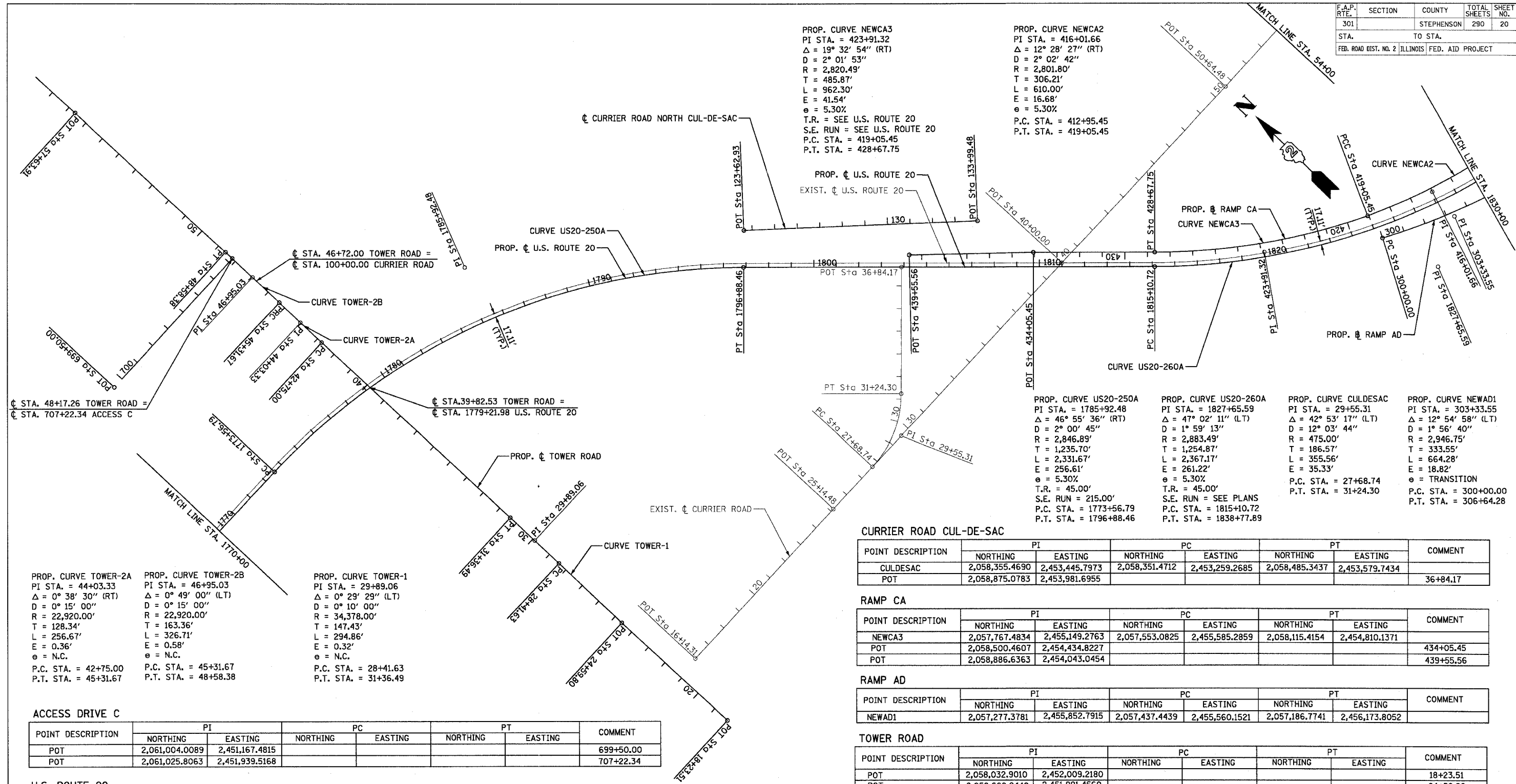
ILLINOIS DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: VERT. 1" = 200'
 HORIZ. 1" = 200'
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY RP

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	20
STA.		TO STA.		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



PROP. CURVE NEWCA3
 PI STA. = 423+91.32
 $\Delta = 19^\circ 32' 54''$ (RT)
 $D = 2^\circ 01' 53''$
 $R = 2,820.49'$
 $T = 485.87'$
 $L = 962.30'$
 $E = 41.54'$
 $e = 5.30\%$
 T.R. = SEE U.S. ROUTE 20
 S.E. RUN = SEE U.S. ROUTE 20
 P.C. STA. = 419+05.45
 P.T. STA. = 428+67.75

PROP. CURVE NEWCA2
 PI STA. = 416+01.66
 $\Delta = 12^\circ 28' 27''$ (RT)
 $D = 2^\circ 02' 42''$
 $R = 2,801.80'$
 $T = 306.21'$
 $L = 610.00'$
 $E = 16.68'$
 $e = 5.30\%$
 P.C. STA. = 412+95.45
 P.T. STA. = 419+05.45

PROP. CURVE TOWER-2A
 PI STA. = 44+03.33
 $\Delta = 0^\circ 38' 30''$ (RT)
 $D = 0^\circ 15' 00''$
 $R = 22,920.00'$
 $T = 128.34'$
 $L = 256.67'$
 $E = 0.36'$
 $e =$ N.C.
 P.C. STA. = 42+75.00
 P.T. STA. = 45+31.67

PROP. CURVE TOWER-2B
 PI STA. = 46+95.03
 $\Delta = 0^\circ 49' 00''$ (LT)
 $D = 0^\circ 15' 00''$
 $R = 22,920.00'$
 $T = 163.36'$
 $L = 326.71'$
 $E = 0.58'$
 $e =$ N.C.
 P.C. STA. = 45+31.67
 P.T. STA. = 48+58.38

PROP. CURVE TOWER-1
 PI STA. = 29+89.06
 $\Delta = 0^\circ 29' 29''$ (LT)
 $D = 0^\circ 10' 00''$
 $R = 34,378.00'$
 $T = 147.43'$
 $L = 294.86'$
 $E = 0.32'$
 $e =$ N.C.
 P.C. STA. = 28+41.63
 P.T. STA. = 31+36.49

PROP. CURVE US20-250A
 PI STA. = 1785+92.48
 $\Delta = 46^\circ 55' 36''$ (RT)
 $D = 2^\circ 00' 45''$
 $R = 2,846.89'$
 $T = 1,235.70'$
 $L = 2,331.67'$
 $E = 256.61'$
 $e = 5.30\%$
 T.R. = 45.00'
 S.E. RUN = 215.00'
 P.C. STA. = 1773+56.79
 P.T. STA. = 1796+88.46

PROP. CURVE US20-260A
 PI STA. = 1827+65.59
 $\Delta = 47^\circ 02' 11''$ (LT)
 $D = 1^\circ 59' 13''$
 $R = 2,883.49'$
 $T = 1,254.87'$
 $L = 2,367.17'$
 $E = 261.22'$
 $e = 5.30\%$
 T.R. = 45.00'
 S.E. RUN = SEE PLANS
 P.C. STA. = 1815+10.72
 P.T. STA. = 1838+77.89

PROP. CURVE CULDESAC
 PI STA. = 29+55.31
 $\Delta = 42^\circ 53' 17''$ (LT)
 $D = 12^\circ 03' 44''$
 $R = 475.00'$
 $T = 186.57'$
 $L = 355.56'$
 $E = 35.33'$
 P.C. STA. = 27+68.74
 P.T. STA. = 31+24.30

PROP. CURVE NEWAD1
 PI STA. = 303+33.55
 $\Delta = 12^\circ 54' 58''$ (LT)
 $D = 1^\circ 56' 40''$
 $R = 2,946.75'$
 $T = 333.55'$
 $L = 664.28'$
 $E = 18.82'$
 $e =$ TRANSITION
 P.C. STA. = 300+00.00
 P.T. STA. = 306+64.28

CURRIER ROAD CUL-DE-SAC

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
CULDESAC	2,058,355.4690	2,453,445.7973	2,058,351.4712	2,453,259.2685	2,058,485.3437	2,453,579.7434	
POT	2,058,875.0783	2,453,981.6955					36+84.17

RAMP CA

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
NEWCA3	2,057,767.4834	2,455,149.2763	2,057,553.0825	2,455,585.2859	2,058,115.4154	2,454,810.1371	
POT	2,058,500.4607	2,454,434.8227					434+05.45
POT	2,058,886.6363	2,454,043.0454					439+55.56

RAMP AD

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
NEWAD1	2,057,277.3781	2,455,852.7915	2,057,437.4439	2,455,560.1521	2,057,186.7741	2,456,173.8052	

TOWER ROAD

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2,058,032.9010	2,452,009.2180					18+23.51
POT	2,058,668.9440	2,451,991.4560					24+59.80
TOWER-1	2,059,198.1248	2,451,982.6740	2,059,050.7141	2,451,985.1204	2,059,345.5090	2,451,978.9634	
TOWER-2A	2,060,611.9542	2,451,947.0791	2,060,483.6587	2,451,950.3091	2,060,740.2779	2,451,945.2860	
TOWER-2B	2,060,903.6211	2,451,943.0035	2,060,740.2783	2,451,945.2860	2,061,066.9148	2,451,938.3930	
POT	2,061,972.0880	2,451,912.8360					57+63.91

ACCESS DRIVE C

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2,061,004.0089	2,451,167.4815					699+50.00
POT	2,061,025.8063	2,451,939.5168					707+22.34

U.S. ROUTE 20

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
US20-250A	2,060,261.2383	2,452,630.5664	2,060,235.5010	2,451,395.1370	2,059,376.3585	2,453,493.0836	
US20-260A	2,057,172.8343	2,455,640.9206	2,058,071.4414	2,454,765.0230	2,057,201.3742	2,456,895.4631	

CURRIER ROAD NORTH CUL-DE-SAC

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2,059,490.0543	2,453,612.3531					123+62.93
POT	2,058,776.6715	2,454,364.3592					133+99.48

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: VERT. HORIZ. 1" = 200'
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY RP

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	21
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	

LANCASTER HEIGHTS ROAD

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2058738.5233	2457309.0845					600+00.00
POT	2058743.5814	2457609.0418					603+00.00

U.S. ROUTE 20

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
US20-270A	2,057,306.4987	2,461,516.4707	2,057,241.4530	2,458,657.2256	2,054,446.9920	2,461,568.7676	

IL ROUTE 75

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
IL75-1	2,054,492.7539	2,457,411.5689	2,054,478.4314	2,456,883.9003	2,055,020.4632	2,457,398.8316	
POT	2,060,949.6139	2,457,255.7128					77+21.97

RAMP BC

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
NEWBC1	2,057,325.5824	2,458,882.4654	2,057,253.2268	2,459,207.6207	2,057,326.2459	2,458,549.3575	
NEWBC2	2,057,326.9118	2,458,215.0507	2,057,326.5247	2,458,409.3578	2,057,419.9766	2,458,044.4800	
POT	2,057,808.9724	2,457,331.5221					119+96.34

RAMP DB

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2,056,604.8043	2,457,360.5885					200+00.00
NEWDB1	2,057,152.9316	2,458,259.1735	2,056,940.1341	2,457,910.3190	2,057,166.6582	2,458,667.5776	
NEWDB2	2,057,171.7844	2,458,820.0980	2,057,166.6582	2,458,667.5776	2,057,162.9496	2,458,972.4486	
NEWDB3	2,057,154.1062	2,459,124.9488	2,057,162.9496	2,458,972.4486	2,057,123.7836	2,459,274.6654	
NEWDB4	2,056,971.1980	2,460,028.0516	2,057,123.7836	2,459,274.6654	2,056,453.6954	2,460,596.4389	
NEWDB5	2,056,268.1733	2,460,800.2030	2,056,453.6954	2,460,596.4389	2,056,053.0083	2,460,972.3723	

RAMP CA

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
NEWCA1	2,057,288.8267	2,456,463.0317	2,057,449.7809	2,456,726.8958	2,057,356.3413	2,456,161.4156	
NEWCA2	2,057,423.2291	2,455,862.5997	2,057,356.3413	2,456,161.4156	2,057,553.0825	2,455,585.2859	
POT	2,057,818.4569	2,457,331.2932					400+00.00

PROP. CURVE 75-DRIV-1
 PI STA. = 804+06.64
 $\Delta = 27^\circ 07' 35''$ (LT)
 $D = 57' 17' 45''$
 $R = 100.00'$
 $T = 24.12'$
 $L = 47.34'$
 $E = 2.87'$
 $\theta = \text{N.C.}$
 P.C. STA. = 803+82.52
 P.T. STA. = 804+29.86

PROP. CURVE 75-DRIV-2
 PI STA. = 804+53.99
 $\Delta = 27^\circ 07' 35''$ (RT)
 $D = 57' 17' 45''$
 $R = 100.00'$
 $T = 24.12'$
 $L = 47.34'$
 $E = 2.87'$
 $\theta = \text{N.C.}$
 P.C. STA. = 804+29.86
 P.T. STA. = 804+77.21

PROP. CURVE US20-270A
 PI STA. = 1885+00.10
 $\Delta = 90^\circ 15' 20''$ (RT)
 $D = 2^\circ 00' 44''$
 $R = 2,847.26'$
 $T = 2,859.98'$
 $L = 4,485.16'$
 $E = 1,188.38'$
 $\theta = 5.30\%$
 $T.R. = 45.00'$
 S.E. RUN = 215.00'
 P.C. STA. = 1856+40.11
 P.T. STA. = 1901+25.28

PROP. CURVE NEWAD2
 PI STA. = 309+73.28
 $\Delta = 12^\circ 51' 21''$ (RT)
 $D = 3^\circ 49' 11''$
 $R = 1,508.00'$
 $T = 168.99'$
 $L = 336.57'$
 $E = 9.49'$
 $\theta = 6.20\%$
 S.E. RUN = 200' @ PT
 P.C. STA. = 308+04.28
 P.T. STA. = 311+40.85

PROP. CURVE NEWBC1
 PI STA. = 103+33.11
 $\Delta = 12^\circ 25' 53''$ (LT)
 $D = 1^\circ 52' 24''$
 $R = 765.00'$
 $T = 333.11'$
 $L = 663.60'$
 $E = 18.09'$
 $\theta = \text{VARIES}$
 P.C. STA. = 100+00.00
 P.T. STA. = 106+63.60

PROP. CURVE NEWBC2
 PI STA. = 109+97.91
 $\Delta = 28^\circ 30' 11''$ (RT)
 $D = 7^\circ 29' 23''$
 $R = 765.00'$
 $T = 194.31'$
 $L = 380.57'$
 $E = 24.29'$
 $\theta = 8.00\%$
 S.E. RUN = 265' @ PC
 P.C. STA. = 108+03.60
 P.T. STA. = 111+84.17

PROP. CURVE NEWCA1
 PI STA. = 410+17.05
 $\Delta = 44^\circ 00' 00''$ (RT)
 $D = 7^\circ 29' 23''$
 $R = 765.00'$
 $T = 309.08'$
 $L = 587.48'$
 $E = 60.08'$
 $\theta = 8.00\%$
 S.E. RUN = 265.00' @ PC
 (ATTAIN SUPER STA. 405+31.00
 TO STA. 407+96.00)
 P.C. STA. = 407+07.97
 P.T. STA. = 412+95.45

PROP. CURVE NEWDB1
 PI STA. = 210+52.57
 $\Delta = 29^\circ 27' 28''$ (RT)
 $D = 3^\circ 41' 09''$
 $R = 1,554.43'$
 $T = 408.63'$
 $L = 799.19'$
 $E = 52.81'$
 $\theta = 6.10\%$
 S.E. RUN = 200' @ PC
 P.C. STA. = 206+43.93
 P.T. STA. = 214+43.12

PROP. CURVE NEWDB2
 PI STA. = 215+95.73
 $\Delta = 5^\circ 14' 38''$ (RT)
 $D = 1^\circ 43' 09''$
 $R = 3,332.51'$
 $T = 152.61'$
 $L = 305.00'$
 $E = 5.42'$
 $\theta = \text{VARIES}$
 P.C. STA. = 214+43.12
 P.T. STA. = 217+48.12

PROP. CURVE NEWDB3
 PI STA. = 219+00.88
 $\Delta = 8^\circ 07' 50''$ (RT)
 $D = 2^\circ 39' 57''$
 $R = 2,149.33'$
 $T = 152.76'$
 $L = 305.00'$
 $E = 5.42'$
 $\theta = 5.30\%$
 P.C. STA. = 217+48.12
 P.T. STA. = 220+53.12

PROP. CURVE NEWDB4
 PI STA. = 228+21.80
 $\Delta = 30^\circ 52' 04''$ (RT)
 $D = 2^\circ 03' 28''$
 $R = 2,784.26'$
 $T = 768.58'$
 $L = 1,500.00'$
 $E = 104.16'$
 $\theta = 5.30\%$
 P.C. STA. = 220+53.12
 P.T. STA. = 235+53.12

PROP. CURVE NEWDB5
 PI STA. = 238+28.69
 $\Delta = 9^\circ 01' 01''$ (RT)
 $D = 1^\circ 38' 22''$
 $R = 3,494.79'$
 $T = 275.57'$
 $L = 550.00'$
 $E = 10.85'$
 $\theta = 5.30\%$
 P.C. STA. = 235+53.12
 P.T. STA. = 241+03.12

RAMP AD

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
NEWAD2	2,057,102.8420	2,456,471.1805	2,057,148.7458	2,456,308.5414	2,057,021.9018	2,456,619.5290	
POT	2,056,617.7439	2,457,360.2762					319+84.68

CURRIER ROAD

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
POT	2058417.8360	2456173.5700					56+83.81
POT	2058440.5820	2457211.7410					67+22.23
POT	2058442.8711	2457316.2209					68+26.73

REVISIONS	
NAME	DATE

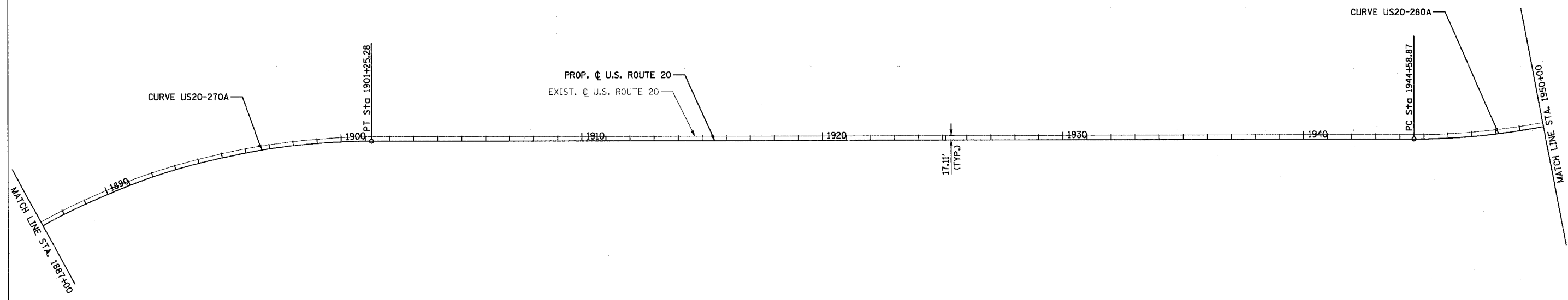
ILLINOIS DEPARTMENT OF TRANSPORTATION
 ALIGNMENT, TIES AND BENCHMARKS

SCALE: VERT. 1" = 200'
 HORIZ. 1" = 200'
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY RP

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	22
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		



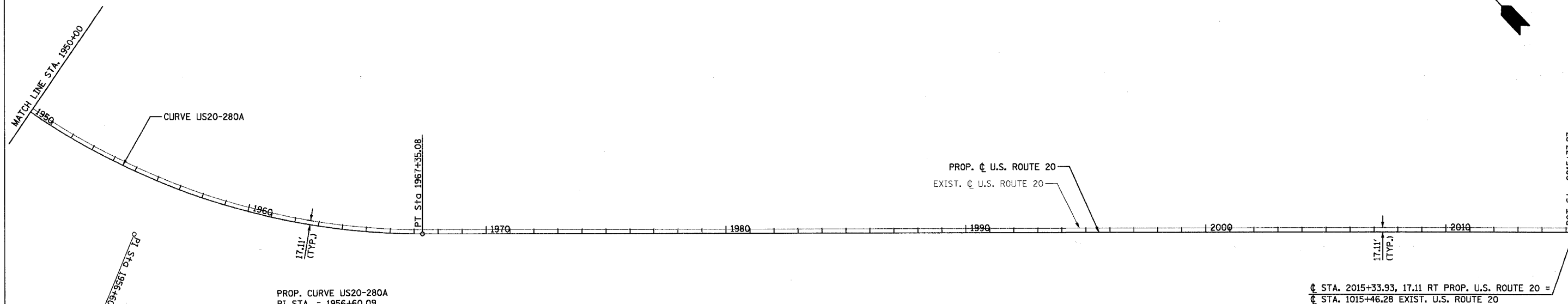
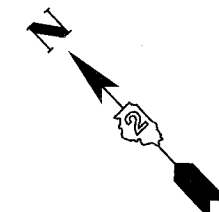
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		ALIGNMENT, TIES AND BENCHMARKS

SCALE: VERT. 1" = 200'
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY RP

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	23
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		



PROP. CURVE US20-280A
 PI STA. = 1956+60.09
 $\Delta = 45^\circ 15' 22''$ (LT)
 $D = 1^\circ 59' 18''$
 $R = 2,881.76'$
 $T = 1,201.21'$
 $L = 2,276.20'$
 $E = 240.33'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 P.C. STA. = 1944+58.87
 P.T. STA. = 1967+35.08

$\text{C STA. } 2015+33.93, 17.11 \text{ RT PROP. U.S. ROUTE } 20 =$
 $\text{C STA. } 1015+46.28 \text{ EXIST. U.S. ROUTE } 20$

U.S. ROUTE 20

POINT DESCRIPTION	PI		PC		PT		COMMENT
	NORTHING	EASTING	NORTHING	EASTING	NORTHING	EASTING	
US20-280A	2,048,913.1071	2,461,669.9757	2,050,114.1178	2,461,648.0107	2,048,083.2679	2,462,538.4665	
POT	2,044,768.0492	2,466,008.0987					2015+33.93

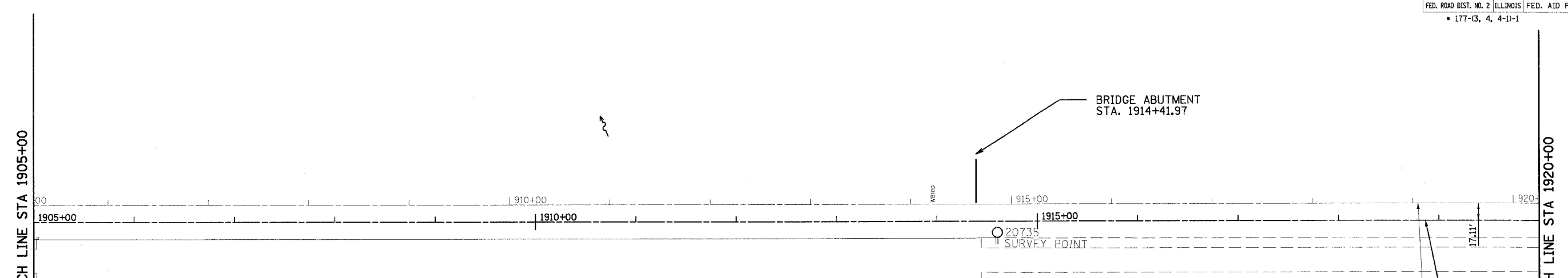
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		ALIGNMENT, TIES AND BENCHMARKS

SCALE: VERT. 1" = 200'
 HORIZ. 1" = 200'
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY RP

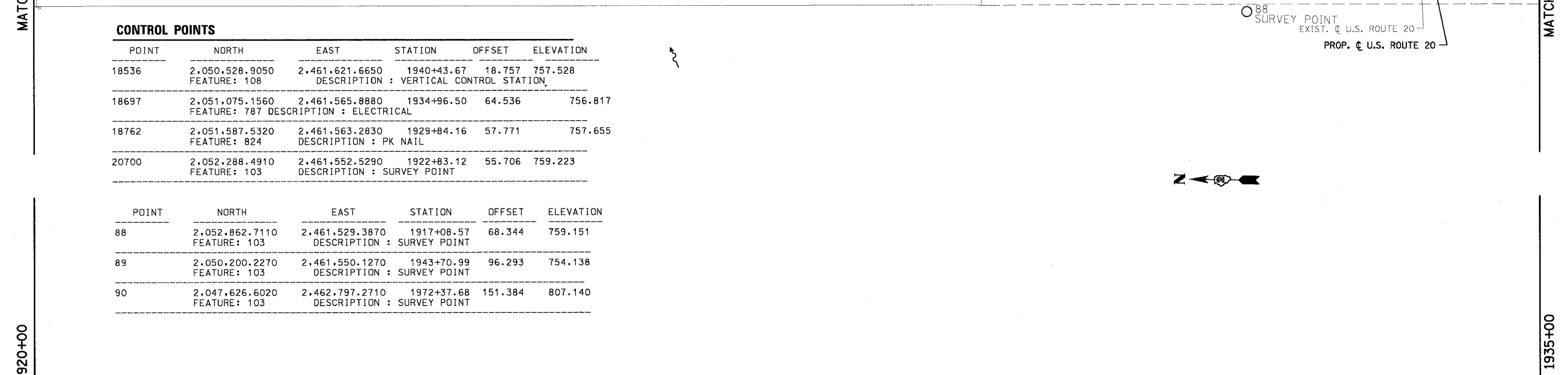
CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	24
STA. 1915+00		TO STA. 1935+00		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		
* 177-(3, 4, 4-1)-1				



CONTROL POINTS

POINT	NORTH	EAST	STATION	OFFSET	ELEVATION
18536	2,050,528.9050 FEATURE: 108	2,461,621.6650 DESCRIPTION: VERTICAL CONTROL STATION	1940+43.67	18.757	757.528
18697	2,051,075.1560 FEATURE: 787	2,461,565.8880 DESCRIPTION: ELECTRICAL	1934+96.50	64.536	756.817
18762	2,051,587.5320 FEATURE: 824	2,461,563.2830 DESCRIPTION: PK NAIL	1929+84.16	57.771	757.655
20700	2,052,288.4910 FEATURE: 103	2,461,552.5290 DESCRIPTION: SURVEY POINT	1922+83.12	55.706	759.223



POINT	NORTH	EAST	STATION	OFFSET	ELEVATION
88	2,052,862.7110 FEATURE: 103	2,461,529.3870 DESCRIPTION: SURVEY POINT	1917+08.57	68.344	759.151
89	2,050,200.2270 FEATURE: 103	2,461,550.1270 DESCRIPTION: SURVEY POINT	1943+70.99	96.293	754.138
90	2,047,626.6020 FEATURE: 103	2,462,797.2710 DESCRIPTION: SURVEY POINT	1972+37.68	151.384	807.140

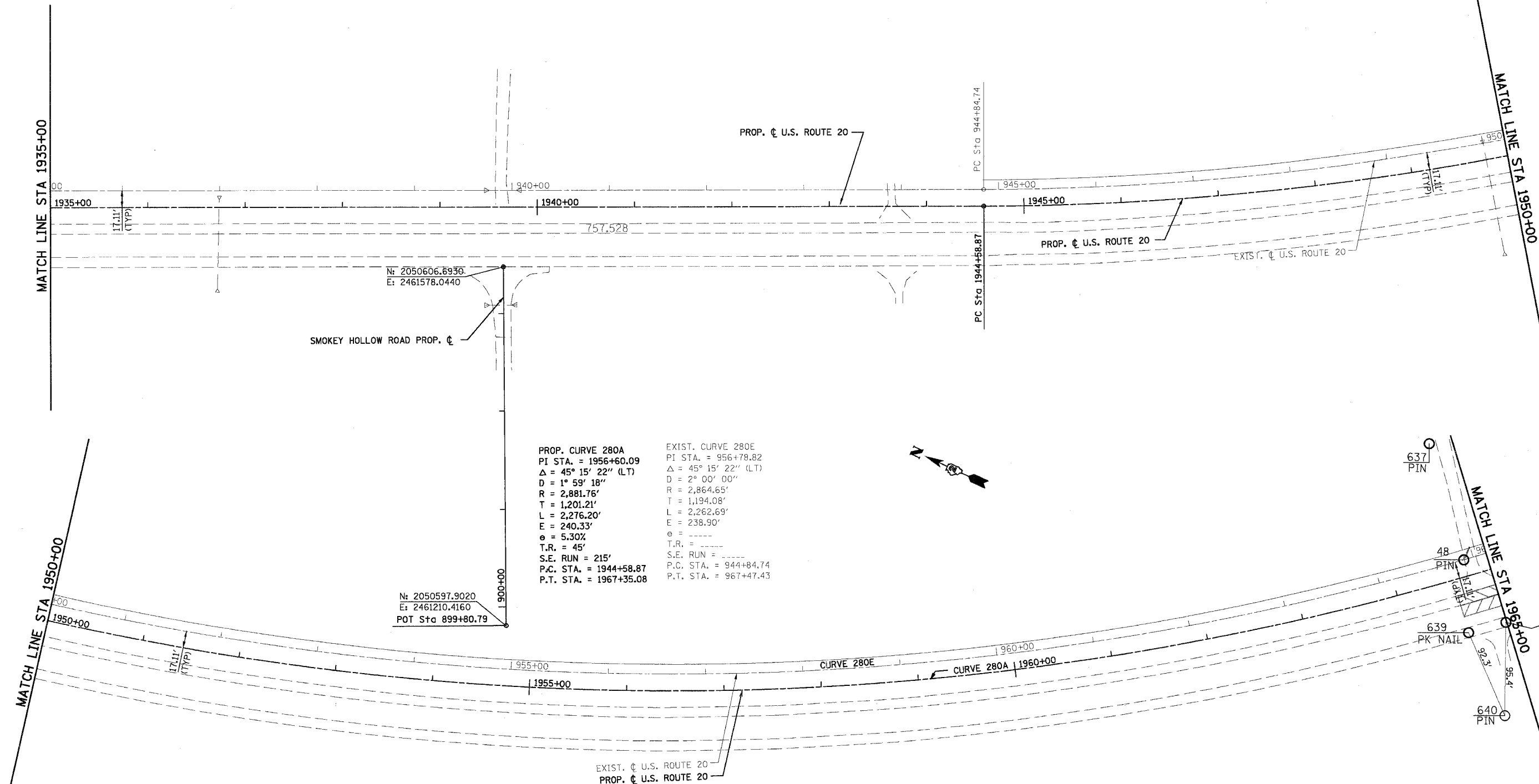
POINT	NORTH	EAST	STATION	OFFSET	ELEVATION
637	2,048,363.1640 FEATURE: 821	2,462,465.0310 DESCRIPTION: PIN	1964+75.63	-140.535	777.099
638	2,048,215.5950 FEATURE: 824	2,462,332.5460 DESCRIPTION: PK NAIL	1964+99.13	56.4259	777.821
639	2,048,245.8950 FEATURE: 824	2,462,306.5950 DESCRIPTION: PK NAIL	1964+60.00	55.9771	777.223
640	2,048,176.8450 FEATURE: 821	2,462,245.3370 DESCRIPTION: PIN	1964+71.59	147.4997	776.423

POINT	NORTH	EAST	STATION	OFFSET	ELEVATION
646	2,047,422.3690 FEATURE: 824	2,463,261.3850 DESCRIPTION: PK NAIL	1977+14.33	-21.5795	792.763
647	2,047,391.9930 FEATURE: 824	2,463,227.2430 DESCRIPTION: PK NAIL	1977+10.63	23.9692	792.641
648	2,047,350.2820 FEATURE: 824	2,463,270.4660 DESCRIPTION: PK NAIL	1977+70.69	24.2668	792.719
649	2,045,661.4870 FEATURE: 824	2,465,037.9550 DESCRIPTION: PK NAIL	2002+15.29	24.2428	789.952
650	2,045,619.6590 FEATURE: 824	2,465,082.1760 DESCRIPTION: PK NAIL	2002+76.16	23.9356	789.533
651	2,045,652.9250 FEATURE: 824	2,465,113.3510 DESCRIPTION: PK NAIL	2002+75.72	-21.6529	789.638
652	2,045,695.9690 FEATURE: 824	2,465,068.2580 DESCRIPTION: PK NAIL	2002+13.38	-21.6224	789.912

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT AND TIES
US ROUTE 20
 SCALE: VERT. _____
 HORIZ. _____
 DATE 10/28/05
 DRAWN BY MKP
 CHECKED BY TVN

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	25
STA. 1935+00		TO STA. 1965+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
* 177-3, 4, 4-1-1				



PROP. CURVE 280A
 PI STA. = 1956+60.09
 $\Delta = 45^\circ 15' 22''$ (LT)
 $D = 1^\circ 59' 18''$
 $R = 2,881.76'$
 $T = 1,201.21'$
 $L = 2,276.20'$
 $E = 240.33'$
 $e = 5.30\%$
 $T.R. = 45'$
 $S.E. RUN = 215'$
 $P.C. STA. = 1944+58.87$
 $P.T. STA. = 1967+35.08$

EXIST. CURVE 280E
 PI STA. = 956+78.82
 $\Delta = 45^\circ 15' 22''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.65'$
 $T = 1,194.08'$
 $L = 2,262.69'$
 $E = 238.90'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 944+84.74$
 $P.T. STA. = 967+47.43$

N: 2050606.6936
 E: 2461578.0440

N: 2050597.9020
 E: 2461210.4160
 POT Sta 899+80.79

CURVE NO.	CONTROL POINT	STATION	COORDINATES		Δ	T	L	R
			NORTH	EAST				
280A	CC		2,050,166.8128	2,464,529.2847				
	PC	1944+58.87	2,050,114.1178	2,461,648.0107				
	PI	1956+60.09	2,461,669.9757	2,461,669.9757	$45^\circ 15' 22''$ LT	1,201.212'	2,276.201'	2,881.756'
	PT	1967+35.08	2,048,083.2679	2,462,538.4665				

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

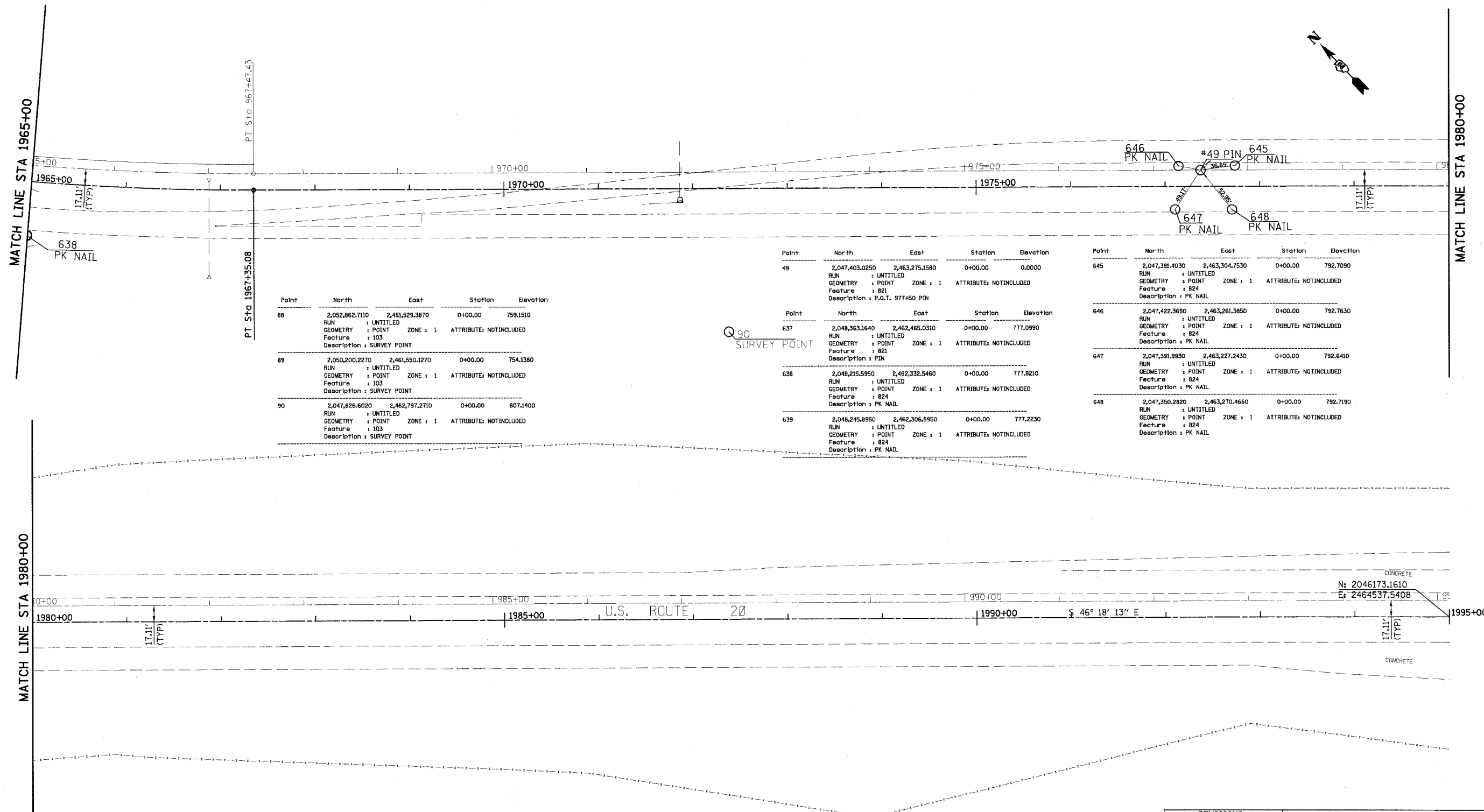
ALIGNMENT AND TIES
US ROUTE 20

SCALE: VERT.
HORIZ.
DATE 10/28/05

DRAWN BY MKP
CHECKED BY TVN

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	26
STA. 1965+00		TO STA. 1995+00		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		
• 177-(3, 4, 4-1)-1				



Point	North	East	Station	Elevation
88	2,052,862.7110	2,461,629.3870	0+00.00	759.1510
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 103				
Description : SURVEY POINT				
89	2,050,200.2270	2,461,550.1270	0+00.00	754.1380
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 103				
Description : SURVEY POINT				
90	2,047,626.6020	2,462,797.2710	0+00.00	807.1400
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 103				
Description : SURVEY POINT				

Point	North	East	Station	Elevation
49	2,047,403.0250	2,463,275.1580	0+00.00	0.0000
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 821				
Description : P.O.T. 977+50 PIN				
637	2,048,363.1640	2,462,465.0310	0+00.00	777.0990
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 821				
Description : PIN				
638	2,048,215.5950	2,462,332.5460	0+00.00	777.8210
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 824				
Description : PK NAIL				
639	2,048,245.8950	2,462,306.5950	0+00.00	777.2230
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 824				
Description : PK NAIL				

Point	North	East	Station	Elevation
645	2,047,381.4030	2,463,304.7530	0+00.00	792.7090
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 824				
Description : PK NAIL				
646	2,047,422.3690	2,463,261.3850	0+00.00	792.7630
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 824				
Description : PK NAIL				
647	2,047,391.9930	2,463,227.2430	0+00.00	792.6410
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 824				
Description : PK NAIL				
648	2,047,350.2820	2,463,270.4660	0+00.00	792.7190
RUN : UNTITLED				
GEOMETRY : POINT ZONE : 1 ATTRIBUTE: NOTINCLUDED				
Feature : 824				
Description : PK NAIL				

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
ALIGNMENT AND TIES
US ROUTE 20
 SCALE: VERT. _____
 HORIZ. _____
 DATE 10/28/05
 DRAWN BY MKP
 CHECKED BY TVN

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	27
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	
• 177-(3, 4, 4-1)-1				

GENERAL NOTES

1. SEE CROSS SECTIONS FOR SPECIAL DITCHES AND BACKSLOPES.
2. THE REMOVAL OF BITUMINOUS SURFACING NOT ON A RIGID TYPE BASE REMOVED IN CONJUNCTION WITH THE BASE SHALL BE REMOVED AS EARTH EXCAVATION. THE REMOVAL OF BITUMINOUS SURFACING ON A RIGID TYPE BASE REMOVED IN CONJUNCTION WITH THE BASE SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT REMOVAL OF THE TYPE SPECIFIED.
3. THE FINAL TOP (6 INCHES) OF SOIL IN ANY RIGHT-OF-WAY AREA DISTURBED BY THE CONTRACTOR MUST BE CAPABLE OF SUPPORTING VEGETATION. THE SOIL MUST BE FROM THE A HORIZON (ZERO TO 2' DEEP) OF SOIL PROFILES OF LOCAL SOILS.
4. THE NUMBER OF CHANGEABLE MESSAGE SIGNS REQUIRED IS FOUR (4).
5. NOT USED
6. THE CONTRACTOR SHALL SEED ALL DISTURBED AREAS WITHIN THE PROJECT LIMITS. SEEDING CLASS 4 OR CLASS 6 (MODIFIED) SHALL BE USED. CLASS 6 (MODIFIED) SHALL BE USED ON FRONT SLOPES 1:6 AND ALL THE MEDIAN. CLASS 4 SHALL BE USED BEHIND TYPE A GUTTER, ON ALL BACKSLOPES AND FORESLOPE GREATER OR EQUAL TO 1:3 AND AREAS BEHIND THE BACKSLOPE, AND BEYOND THE TOE OF FRONT SLOPE ON FILL SECTIONS WITHOUT DITCHES. SEEDING CLASS 7 (SPECIAL) SHALL BE USED AT THE WETLAND MITIGATION SITE.
7. WHEN MULCH WITH EMULSIFIED ASPHALT IS APPLIED, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO COVER OR PROTECT ALL TRAFFIC SIGNS, GUARDRAIL AND CURBS. ANY SIGNS, GUARDRAIL OR CURBS WHICH BECOME COVERED WITH ASPHALTIC MATERIAL SHALL BE CLEANED BY THE CONTRACTOR AT HIS OWN EXPENSE.
8. NOSES OF CURBED CORNER ISLANDS NOTED AS 1 & 2 ON HIGHWAY STANDARD 606301 SHALL BE RAMPED UNLESS THE CURB FUNCTION IS FOR THE PROTECTION OF PEDESTRIANS, SIGNALS, LIGHT STANDARDS OR SIGN TRUSS SUPPORTS. (DIST GN#57)
9. THE SUBGRADE ON THIS PROJECT, EXCLUSIVE OF ROCK CUT AREAS IS SCHEDULED TO BE IMPROVED TO A 300 MM (12") DEPTH ACCORDING TO MECHANISTIC PAVEMENT DESIGN. THE AREAS SCHEDULED TO BE IMPROVED TO A DEPTH GREATER THAN 300 MM (12") ARE ESTIMATED BASED ON THE ORIGINAL GEOTECHNICAL INVESTIGATION. THE SUBGRADE SHALL BE PROCESSED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS BEFORE THE ENGINEER SHALL DETERMINE THE LIMITS AND THE ADDITIONAL THICKNESS OF IMPROVEMENT REQUIRED, IF ANY.
10. THE ADDITIONAL EMBANKMENT REQUIRED TO BUILD UP THE SHOULDER FOR THE TRAFFIC BARRIER TERMINAL, TYPE 1, AS SHOWN ON THE PLANS, SHALL BE HAULED FROM EXCESS EARTH EXCAVATION FROM WITHIN THE PROJECT AND SHALL BE PLACED PRIOR TO THE INSTALLATION OF THE TERMINAL. THE COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD OF EARTH EXCAVATION. AN ESTIMATED 50 CUBIC YARDS OF EMBANKMENT IS REQUIRED AT EACH TERMINAL LOCATION.
11. EXCEPT FOR THE TOP 75 MM (3"), ALL AGGREGATE BASES AND SUBBASES 300 MM (12") IN THICKNESS SHALL BE CONSTRUCTED OF AGGREGATE GRADATION CA-2. IF THE SPECIFIED THICKNESS EXCEEDS 300 MM (12"), THE BASES OR SUBBASES SHALL BE CONSTRUCTED OF TOPSIZE 150 MM (6") BREAKER-RUN CRUSHED STONE WITH 15% TO 40% BY WEIGHT PASSING THE 50 MM (2") SIZE SIEVE, EXCEPT FOR THE TOP 75 MM (3"). THE BREAKER-RUN CRUSHED STONE SHALL BE REASONABLY UNIFORMLY GRADED FROM COARSE TO FINE AND BE TAKEN FROM A QUARRY LEDGE CAPABLE OF PRODUCING CLASS "D" QUALITY AGGREGATE. THE TOP 75 MM (3") SHALL BE GRADATION CA-6 OR CA-10 REGARDLESS OF THICKNESS. THE WATER NECESSARY TO ACHIEVE COMPACTION IN ALL BUT THE TOP 75 MM (3") LAYER MAY BE ADDED AFTER THE SUBBASE OR BASE COURSE IS PLACED ON THE GRADE.
12. ALL EMBANKMENT CONSTRUCTED OF COHESIVE SOIL SHALL BE CONSTRUCTED WITH NOT MORE THAN 110% OF OPTIMUM MOISTURE CONTENT, DETERMINED BY THE STANDARD PROCTOR TEST. COHESIVE SOIL SHALL BE DEFINED AS ANY SOIL WHICH CONTAINS GREATER THAN 10% PARTICLES BY WEIGHT PASSING THE 75 mm (#200 SIEVE). THE 110% OF OPTIMUM MOISTURE LIMIT MAY BE WAIVED IN FREE-DRAINING GRANULAR MATERIAL WHEN APPROVED BY THE ENGINEER.
13. THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

Mixture Use(s):	EB Overlay		WB Full Depth *		Dit Repl Patches & Flex Con	Side Road/ Top Shoulder	Bottom Shoulder
	Surface	Level Binder	Surface	Binder			
ACPG:	SBS PG 70-22	SBS PG 70-22	SBS PG 70-28	SBS PG 70-28	SBS PG 70-22	PG 58-22	PG 58-22
RAP%: (Max)*	0	0	0	0	0	30	30
Design Air Voids:	4.2@N90	4.2@N90	4.2@N90	4.2@N90	4.2@N90	3@N50	2@N50
Mixture Composition (Gradation Mixture)	IL 9.5 OR 12.5	IL 9.5	IL 9.5 OR 12.5	IL 19.0	IL 19.0	IL 12.5 or 9.5	BAM
Friction Aggregate	Mix D	N/A	Mix D	N/A	N/A	Mix C	N/A
20 Year ESAL	11.9	N/A	11.9	11.9	11.9	N/A	N/A

* POLYMERIZED BIT. CONC. PAVEMENT, (FULL DEPTH), SUPERPAVE, 14" SHALL BE COMPOSED OF THE FOLLOWING TWO MIXES:
 - 12" BIT. CONC. BINDER CSE, SUPERPAVE, IL-19, N90
 - 2" POLY. BIT. CONC. SURF. CSE, SUPERPAVE, MIX "D", N90

14. THE CONTRACTOR WILL BE REQUIRED TO FURNISH 140 MM (5 1/2") HIGH BRASS STENCILS AS APPROVED BY THE ENGINEER AND INSTALL STATIONING AT 250' INTERVALS. STATIONING SHALL BE PLACED ON BOTH LANES OF 2-LANE HIGHWAYS AND ON THE OUTSIDE LANES IN BOTH DIRECTIONS ON 4-LANE HIGHWAYS. THE STATIONS SHALL BE PLACED 150 MM (6") INSIDE THE PAVEMENT MARKING EDGE SO THEY CAN BE READ FROM THE SHOULDER. THIS WORK WILL BE INCLUDED IN THE COST OF THE FINAL PAVEMENT SURFACE.
15. ROCK CUT AREAS SHALL HAVE 6 INCHES OF AGGREGATE SUBBASE CONSTRUCTED FROM GRADATION CA-6 OR CA-10. THIS WORK SHALL BE PAID FOR PER TON SUBBASE GRANULAR MATERIAL, TYPE A.

16. GUARDRAIL POSTS MAY BE DRIVEN THROUGH BITUMINOUS SHOULDERS IF THE SHOULDER IS NOT DAMAGED AS DETERMINED BY THE ENGINEER. IF THE SHOULDER IS BEING DAMAGED, THE CONTRACTOR SHALL CORE HOLES IN THE SHOULDER ACCORDING TO ARTICLE 630.06 OF THE STANDARD SPECIFICATIONS.
17. DELINEATORS SHALL BE INSTALLED AS SHOWN IN STANDARD 635001, EXCEPT THAT THE POST SHALL BE ROTATED 180 AND ONLY METAL-BACKED DELINEATORS SHALL BE PERMITTED. (DIST GN#83)
18. USE B-15.15 (B-6.06) OR B-10.15 (B-4.06) CURB AND GUTTER ON ALL SIDES OF ISLANDS WHEN ISLAND IS OFFSET SHOULDER WIDTH, BUT OFFSET SHOULD NOT BE GREATER THAN 2.4 M (8 FEET) EDGE TO FACE. (DIST GN#58)
19. THE CONTRACTOR SHALL REMOVE ALL ENTRANCE CULVERTS IN CONDITION FOR REUSE WHICH ARE NOT TO BE LEFT IN PLACE. THEY SHALL BE CLEANED AND STORED ALONG THE RIGHT OF WAY AS DIRECTED. IN NO CASE SHALL THEY BE ROUGHLY HANDLED OR SHOVED BY HEAVY MACHINERY. UNUSABLE MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS EXPENSE. COST OF THE WORK TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
20. THE PROPOSED PIPES FOR ENTRANCES AND SIDE ROADS SHALL BE PLACED IN LINE WITH THE EXISTING OR PROPOSED DITCH LINE, UNLESS OTHERWISE NOTED.
21. GRADING QUANTITIES FOR THE CONSTRUCTION OF THE TRAFFIC BARRIER TERMINALS, AS SHOWN IN THE PLANS, ARE INCLUDED IN THE QUANTITY FOR EARTH EXCAVATION.
22. SALVAGE EXISTING DELINEATORS WITHIN THE PROJECT LIMITS AND PLACE ONE AT EACH END OF APPROACH GUARDRAIL TERMINAL SECTION. THE WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
23. THE CONTRACTOR SHALL INSTALL 450MM (18") DIAMETER FORMED OPENINGS IN THE CONCRETE MEDIAN SPACED AT INTERVALS NO GREATER THAN 75 M (250 FEET), AND/OR AS DIRECTED BY THE ENGINEER. SURFACE, PAVEMENT SURFACES OR OTHER EXISTING OBSTRUCTIONS BENEATH THESE OPENINGS SHALL BE ALL EXISTING REMOVED BY THE CONTRACTOR. ALL COSTS INCURRED SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE METER (SQUARE FOOT) FOR P.C. CONCRETE MEDIAN SURFACE, 100 MM (4 INCH). (DIST GN#65)
24. NOT USED
25. MULCH SHALL BE APPLIED AT A RATE OF 2 TONS/ ACRE OF SEEDING UNLESS OTHERWISE SPECIFIED BY THE ENGINEER. EMULSIFIED ASPHALT SHALL BE APPLIED AT A RATE OF 75 GALLONS/ TON (764 POUNDS/ TON) OF MULCH.
26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK. THE JULIE NUMBER IS 800-892-0123. THE FOLLOWING LISTED UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS ARE MEMBERS OF JULIE:
27. MR. LARRY MEYERS
COMMONWEALTH EDISON, INC
123 ENERGY AVENUE
ROCKFORD, IL 61109
(815) 490-2869

MR. ROBERT TRUEBLOOD
CITY OF FREEPORT SEWER & WATER
230 WEST STEPHENSON
FREEPORT, IL 61021
(815) 235-7183

FOLLOWING ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS WHICH ARE NOT MEMBERS OF JULIE AND SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR:

MR. DENNIS SCHULTZ
IDOT - DIXON
819 DEPOT AVENUE
DIXON, IL 61021
(815) 284-5469

THE LOCATIONS OF THE UTILITIES ON THESE PLANS ARE APPROXIMATE AND WILL NEED TO BE VERIFIED IN THE FIELD.

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION

GENERAL NOTES US ROUTE 20

SCALE: VERT.
HORIZ.
DATE 10/28/05

DRAWN BY
CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	•	STEPHENSON	290	28
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	

• 177-13, 4, 4-1-1

GENERAL NOTES (CONT.)

28. CADD DATA WILL BE AVAILABLE TO CONTRACTORS AND CONSULTANTS WORKING ON THIS PROJECT. THIS INFORMATION WILL BE PROVIDED UPON REQUEST AS MICROSTATION CADD FILES AND GEOPAK COORDINATE GEOMETRY FILES ONLY. IF DATA IS REQUIRED IN OTHER FORMATS IT WILL BE YOUR RESPONSIBILITY TO MAKE THESE CONVERSIONS. IF ANY DISCREPANCY OR INCONSISTENCY ARISES BETWEEN THE ELECTRONIC DATA AND THE INFORMATION ON THE HARD COPY, THE INFORMATION ON THE HARD COPY SHOULD BE USED. CONTACT THE DISTRICT'S PROJECT ENGINEER TO REQUEST THESE FILES.
29. SUBBASE DRAINS AND UNDERDRAIN SPECIALS SHALL BE FULLY INSTALLED, AND OPERATIONAL, AND OUTLETED PRIOR TO THE PLACEMENT OF ANY RELATED PAVEMENT STRUCTURE.
30. SEPTIC TANKS WITHIN THE RIGHT-OF-WAY, WHICH HAVE NOT BEEN REMOVED AND WILL NOT INTERFERE WITH CONSTRUCTION SHALL BE FILLED WITH FREE-FLOWING SAND AT THE DIRECTION OF THE ENGINEER. COST OF THIS WORK SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD OF EARTH EXCAVATION.
31. IT IS ANTICIPATED THAT ALL MAILBOXES WILL REQUIRE RELOCATION. THE FINAL LOCATION WILL BE TO THE APPROACH SIDE OF THE ENTRANCES. THE CONTRACTOR SHALL BE REQUIRED TO MOUNT THE MAILBOX ON A 4 X 4 WOOD POST 40 INCHES ABOVE THE SHOULDER SURFACE AND EXTENDING TO A MINIMUM OF 42 INCHES INTO THE EMBANKMENT. THE COST OF THIS WORK WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
32. THE CONTRACTOR SHALL SUPPLY THE RESIDENT ENGINEER WITH THE MANUFACTURER'S INSTALLATION REQUIREMENTS FOR THE TYPE OF STEEL PLATE BEAM GUARDRAIL TERMINAL, TYPE 1, SPECIAL (TANGENT) OR STEEL PLATE BEAM GUARDRAIL TERMINAL, TYPE 1, SPECIAL (FLARED).
33. ONE 8d GALVANIZED NAIL SHALL BE USED TO TOE NAIL THE WOOD BLOCK OUT TO THE WOOD POST ON ALL TRAFFIC BARRIER TERMINAL TYPE 1 SPECIALS.
34. THE UNDERDRAIN SYSTEM SCHEDULED ON THIS PROJECT IS TO BE CONSTRUCTED IN ACCORDANCE WITH SECTION 601 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, EXCEPT WHEN THE RECURRING SPECIAL PROVISION PIPE UNDERDRAINS IS INCLUDED, THE FABRIC ENVELOPE ENCASUREMENT OF THE PIPE SHALL BE OMITTED. (DIST GN#75)
35. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED THROUGH THE LIFE OF THE PROJECT, INCLUDING THOSE REQUIRING ACCESS MODIFICATION ALONG U.S. ROUTE 20.
36. THE REMOVAL OF SILT TO MAINTAIN SILT FILTER FENCE WILL BE PAID FOR ACCORDING TO THE RECURRING SPECIAL PROVISION OF EROSION CONTROL.
37. A CONTINGENCY QUANTITY OF 500.0 LINEAL FEET OF TEMPORARY FENCE HAS BEEN INCLUDED FOR THOSE AREAS WHICH MIGHT CONTAIN LIVESTOCK, TO BE USED AS DIRECTED BY THE ENGINEER.
38. MULCH ON TEMPORARY SEEDING SHALL BE MULCH METHOD 2. (DIST GN#12)
39. A NATIONWIDE 404 PERMIT HAS BEEN ISSUED FOR THIS PROJECT AND THE CONDITIONS OF THAT PERMIT MUST BE ADHERED TO.
40. THE THICKNESS FOR THE BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) ADJACENT TO EXISTING PAVEMENT SHALL BE A MINIMUM OF 12". THE MATERIAL SHALL BE 2" BITUMINOUS CONCRETE SURFACE COURSE MIXTURE D, CLASS 1, AND THE REMAINING THICKNESS SHALL BE BITUMINOUS BINDER COURSE CLASS 1 OF THE TYPE SPECIFIED.
41. WHERE FIELD TILE IS ENCOUNTERED, STORM SEWER OR PIPE DRAIN WILL BE USED IN ACCORDANCE WITH SECTION 611. THE MINIMUM SIZE FOR REPLACEMENT WILL BE 6" FOR PIPE DRAINS AND 8" FOR STORM SEWER, BUT THE SIZE MUST BE AT LEAST 2" LARGER THAN THE ADJOINING TILE. A FIELD TILE JUNCTION VAULT WILL BE CONSTRUCTED AT THE RIGHT-OF-WAY TO CONNECT THE TILE AND STORM SEWER.
42. THESE ITEMS HAVE BEEN INCLUDED IN THIS CONTRACT AS CONTINGENCY ITEMS FOR LOCATING AND REPLACING EXISTING FARM TILE SYSTEMS CROSSING WITHIN THE PROPOSED RIGHT-OF-WAY:
- EXPLORATION TRENCH (52" DEPTH) 200 LIN. FT.
FIELD TILE JUNCTION VAULTS, 3' DIA. 5 EACH
43. PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:
- ALL WORDS, SUCH AS ONLY, SHALL BE 8 FEET HIGH.
 - ALL NON-FREEWAY ARROWS SHALL BE THE LARGE SIZE.
 - THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 8", NOT 7" AS SHOWN IN THE DETAIL OF TYPICAL LANE AND EDGE LINES.
44. WETLAND MITIGATION WILL BE CONDUCTED WITH THE U.S. ROUTE 20 PAVING PLAN, JOB NO. C-92-012-03.
45. LAYOUT OF LOCATIONS FOR REPLACEMENT OF TREES WILL BE PROVIDED BY THE DISTRICT LANDSCAPE ARCHITECT ACCORDING TO THE SCHEDULE SHOWN IN DISTRICT STANDARD 90.4. MULCH SHALL BE PROVIDED IN A 1.5 M (5') DIAMETER CIRCLE CENTERED ON EACH TREE IN THE FORM OF 100 MM (4") THICK HARDWOOD WOODCHIPS PLACED ON WEED BARRIER FABRIC. THIS WORK WILL BE INCLUDED IN THE COST OF EACH TREE.
46. FOR THOSE TREES IDENTIFIED ON THE PLANS AS "TO BE SAVED" CARE SHALL BE USED TO AVOID IMPACTS AND TO MINIMIZE ROOT SYSTEM DAMAGE DURING ADJACENT EXCAVATION.
47. NOT USED
48. ALL FIELD ENTRANCES ON US ROUTE 20 SHALL REMAIN OPEN UNTIL REVISED ACCESS IS ESTABLISHED. THE CURRIER ROAD CONNECTION TO US ROUTE 20 SHALL BE REMOVED AFTER THE TOWER ROAD MODIFICATIONS ARE COMPLETE.
49. A MINIMUM OF 48 HOURS IN ADVANCE IS REQUIRED FOR NON EMERGENCY WORK.
50. INSTALL RUMBLE STRIPS IN ALL SHOULDERS IN ACCORDANCE WITH STATE STANDARD 642001. RUMBLE STRIPS SHALL BE PLACED ON SHOULDERS ON BOTH SIDES OF THE PAVEMENT.
51. THE COST OF MAKING SEWER CONNECTIONS TO EXISTING DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE VARIOUS CONTRACT UNIT PRICES FOR SLOTTED DRAIN 12".
52. THE MATERIAL FOR BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE) SHALL BE THE SAME AS THE ADJACENT FLEXIBLE PAVEMENT.
53. IN AREAS WHERE THE PROFILE GRADE IS LESS THAN 0.4%, THE PIPE UNDERDRAINS, 4", WILL BE CONSTRUCTED WITH A MINIMUM SLOPE OF 0.4%.
54. PREVIOUSLY PUGMILLED STOCKPILES OF "TYPE A" OLDER THAN 1 MONTH WILL NOT BE APPROVED FOR USE UNTIL A MOISTURE CHECK IS RUN TO VERIFY MOISTURE CONTENT. MATERIAL SHIPPED TO PROJECTS WITHOUT BEING TESTED WILL NOT BE ACCEPTED.
55. WHEN LAYING OUT FOR PATCHING, THE MINIMUM DISTANCE BETWEEN NEW PATCHES (SAW CUT TO SAW CUT) SHALL BE 15 FEET. WHEN PATCH SPACING IS LESS THAN 15 FEET, THE PAVEMENT BETWEEN PATCHES SHALL ALSO BE REMOVED AND REPLACED.
56. REFLECTIVE CRACK CONTROL SHALL BE PLACED ON THE EXISTING SURFACE PRIOR TO ANY RESURFACING, UNLESS PAVEMENT IS MILLED THEN IT WILL BE PLACED ON THE BINDER COURSE.
57. ON FULL DEPTH PAVEMENT, SHOULDER WIDTHS OF 1.8 M (6 FT) OR LESS MAYBE PLACED, AT THE CONTRACTOR'S OPTION, SIMULTANEOUSLY WITH THE ADJACENT TRAFFIC LANE FOR BOTH THE BINDER AND SURFACE COURSES, PROVIDED THE CROSS SLOPE OF BOTH THE PAVEMENT AND SHOULDER CAN BE SATISFACTORILY OBTAINED. THE SHOULDER WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD FOR BITUMINOUS SHOULDERS OF THE THICKNESS SPECIFIED ON THE PLANS.
58. NOT USED
59. THE ADDITIONAL THICKNESS OF PROPOSED PAVEMENT REQUIRED TO MATCH THE BRIDGE APPROACH PAVEMENT, SHOWN IN STANDARD 420401, SHALL BE INCLUDED IN THE COST OF THE PROPOSED PAVEMENT AND NOT PAID FOR SEPARATELY.
60. USE M-6 CURB ON ISLANDS WHEN LOCATED ADJACENT TO A HIGHWAY WITH SPEEDS OF 45 MPH OR LESS.
61. ON LARGE AND INTERMEDIATE ISLANDS, THE VARIABLE CURB AND GUTTER FLAG WILL BE PAID FOR AS COMBINATION CONCRETE CURB AND GUTTER TYPE M6.24.
62. RURAL MINIMUM ISLAND AREA = 100 SQ FT.
URBAN ISLAND AREA = USUALLY 75 SQ FT BUT NOT LESS THAN 50 SQ FT.
(ISLAND AREA INCLUDES THE CONCRETE MEDIAN SURFACE AND THE CURB.)
63. THE ISLANDS ON THIS PROJECT ARE INTERMEDIATE ISLANDS AS SHOWN ON THE DETAIL OF ISLAND SHEET IN THE PLANS.
64. DELINEATORS SHALL BE PLACED AT THE ENDS OF APPROACH GUARDRAIL TERMINAL SECTIONS, AND AT EACH HEADWALL OR END SECTION OF AR CULVERTS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR DELINEATORS.
65. THE QUANTITY FOR THE TREE REMOVAL INCLUDED IN THIS CONTRACT IN THE AREA OF THE WETLAND SITE IS ESTIMATED. THE RESIDENT ENGINEER AND THE CONTRACTOR SHALL COORDINATE THE TREE REMOVAL AND OTHER WORK ON THE WETLAND SITE WITH THE DISTRICT ENVIRONMENTAL UNIT.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<p>GENERAL NOTES US ROUTE 20</p> <p>SCALE: VERT. HORIZ. DATE 10/28/05</p> <p style="text-align: right;">DRAWN BY CHECKED BY</p>

SCHEDULE OF QUANTITIES

20100110 TREE REMOVAL (6 TO 15 UNITS DIAMETER)			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) UNITS

US ROUTE 20 (V3 SECTION)			
LOCATION 1 (WETLAND)			75.0
LOCATION 2 (WETLAND)			75.0
TOTAL =			150.0
20100210 TREE REMOVAL (OVER 15 UNITS DIAMETER)			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) UNITS

US ROUTE 20 (V3 SECTION)			
LOCATION 1 (WETLAND)			120.0
LOCATION 2 (WETLAND)			120.0
TOTAL =			240.0
20800150 TRENCH BACKFILL			

STATION	AREA (SF)	- STATION	AREA (SF) CY YD

US ROUTE 20 (HDR SECTION)			
RAMP AD			
304+00.0			9.0
RAMP DB			
216+40.0			4.0
TOTAL =			13.0
25000310 SEEDING, CLASS 4			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) ACRE

HDR SECTION			
US ROUTE 20- WB LT			0.50
TOTAL =			0.50
25000400 NITROGEN FERTILIZER NUTRIENT			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) POUNDS

90.0lbs/Acre			
(V3 SECTION)			
SEEDING, CLASS 6 (MODIFIED)	4.5ACRES		405
SUB-TOTAL =			405
HDR SECTION			
US ROUTE 20- WB OUTSIDE			158
US ROUTE 20- MEDIAN			158
RAMP DB			608
SUB-TOTAL =			923
TOTAL =			1,328

25000500 PHOSPHOPUS FERTILIZER NUTRIENT			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) POUNDS

90.0lbs/Acre			
(V3 SECTION)			
SEEDING, CLASS 6 (MODIFIED)	4.5ACRES		405
SUB-TOTAL =			405
HDR SECTION			
US ROUTE 20- WB OUTSIDE			158
US ROUTE 20- MEDIAN			158
RAMP DB			608
SUB-TOTAL =			923
TOTAL =			1,328
25000600 POTASSIUM FERTILIZER NUTRIENT			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) POUNDS

90.0lbs/Acre			
(V3 SECTION)			
SEEDING, CLASS 6 (MODIFIED)	4.5ACRES		405
SUB-TOTAL =			405
HDR SECTION			
US ROUTE 20- WB OUTSIDE			158
US ROUTE 20- MEDIAN			158
RAMP DB			608
SUB-TOTAL =			923
TOTAL =			1,328
25001830 SEEDING, CLASS 6 (MODIFIED)			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) ACRE

HDR SECTION			
US ROUTE 20- WB LT			1.25
US ROUTE 20- MEDIAN			1.75
RAMP DB			6.75
SUB-TOTAL =			9.75
US ROUTE 20 (V3 SECTION)			
1914+42.0	8	- 1982+00.0	1.25
1914+42.0	7.0	- 1982+00.0	1.09
1971+00.0	0-4	- 1977+05.0	0.03
1967+00.0	40	- 1988+60.0	2.00
SUB-TOTAL =			4.50
TOTAL =			14.25
25001845 SEEDING CLASS 7 (SPECIAL)			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) ACRE

US ROUTE 20 (V3 SECTION)			
WETLAND SITE			16.25
TOTAL =			16.25

25100115 MULCH, METHOD 2			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) ACRE

US ROUTE 30 (V3 SECTION)			
FROM SEEING CLASS 6 (MODIFIED)			4.50
SUB-TOTAL =			4.50
HDR SECTION			
US ROUTE 20- WB LT			1.75
US ROUTE 20- MEDIAN			1.75
RAMP DB			6.75
SUB-TOTAL =			10.25
TOTAL =			14.75
25100630 EROSION CONTROL BLANKET			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) SQ. YD.

HDR SECTION			
US ROUTE 20- WB LT			
LT 1697+00 TO 1713+00			3.911
LT 1802+00 TO 1809+00			1.711
US ROUTE 20- MEDIAN			
1690+50 TO 1695+20			627
1697+00 TO 1714+00			2,267
1797+00 TO 1813+00			2,133
1847+00 TO 1866+00			2,533
RAMP DB			1,636
US ROUTE 20- CURRIER ROAD			214
SUB-TOTAL =			15,032
V3 SECTION			
US ROUTE 20- MEDIAN (FULL WIDTH)			
1967+00 TO 1988+60			9,600
SUB-TOTAL =			9,600
TOTAL =			24,632
28000250 TEMPORARY EROSION CONTROL SEEDING			

STATION	WIDTH(FT)	- STATION	WIDTH(FT) POUNDS

100 LBS/ACRE x NO. OF APPLICATIONS			
HDR SECTION			
US ROUTE 20- WB OUTSIDE			875
US ROUTE 20- MEDIAN			875
RAMP DB			3375
SUB-TOTAL =			5125
US ROUTE 20 (V3 SECTION)			
4.5 ACRES X 100 LBS X 5 APPLICATIONS			2250
SUB-TOTAL =			2250
TOTAL =			7375

28000300 TEMPORARY DITCH CHECKS				

STATION	OFFSET	- STATION	OFFSET	EACH

US ROUTE 20 (HDR SECTION)				
1690+75.0	MEDIAN			1
1691+50.0	MEDIAN			1
1692+25.0	MEDIAN			1
1693+00.0	MEDIAN			1
1693+75.0	MEDIAN			1
1694+50.0	MEDIAN			1
1697+40.0	LT & MEDIAN			2
1698+15.0	LT & MEDIAN			2
1698+90.0	LT & MEDIAN			2
1699+65.0	LT & MEDIAN			2
1700+40.0	LT & MEDIAN			2
1701+15.0	LT & MEDIAN			2
1701+90.0	LT & MEDIAN			2
1702+65.0	LT & MEDIAN			2
1703+40.0	LT & MEDIAN			2
1704+15.0	LT & MEDIAN			2
1704+90.0	LT & MEDIAN			2
1705+65.0	LT & MEDIAN			2
1706+40.0	LT & MEDIAN			2
1707+15.0	LT & MEDIAN			2
1707+90.0	LT & MEDIAN			2
1708+65.0	LT & MEDIAN			2
1709+40.0	LT & MEDIAN			2
1710+15.0	LT & MEDIAN			2
1710+90.0	LT & MEDIAN			2
1711+65	LT & MEDIAN			2
1712+40	MEDIAN			1
1713+15	MEDIAN			1
1713+90	MEDIAN			1
1798+00	MEDIAN			1
1799+50	MEDIAN			1
1801+00	MEDIAN			1
1802+50	MEDIAN			1
1803+30	LT			1
1805+40	LT & MEDIAN			2
1806+90	MEDIAN			1
1808+40	MEDIAN			1
1809+90	MEDIAN			1
1811+40	MEDIAN			1
1812+90	MEDIAN			1
1847+00	MEDIAN			1
1848+50	MEDIAN			1
1850+00	MEDIAN			1
1851+50	MEDIAN			1
1853+00	MEDIAN			1
1854+00	MEDIAN			1
1854+50	MEDIAN & RT			2
1855+00	MEDIAN & RT			2
1855+50	MEDIAN & RT			2
1856+00	MEDIAN & RT			2
1856+50	MEDIAN & RT			2
1857+00	MEDIAN & RT			2
1857+50	MEDIAN & RT			2
1858+00	MEDIAN & RT			2
1858+50	MEDIAN & RT			2
1859+00	MEDIAN & RT			2
1859+50	MEDIAN & RT			2
1860+00	MEDIAN & RT			2
1860+50	MEDIAN & RT			2
1861+00	MEDIAN & RT			2
1861+50	MEDIAN & RT			2
1862+00	MEDIAN & RT			2
1862+50	MEDIAN & RT			2

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

US ROUTE 20

SCALE: VERT. DATE 10/28/05

HORIZ. DATE 10/28/05

DRAWN BY DTE

CHECKED BY JAM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
• 177-(3,4,4-1)-1				

SCHEDULE OF QUANTITIES

28000300 TEMPORARY DITCH CHECKS (CONTINUED)				
STATION	OFFSET	STATION	OFFSET	EACH
1863+00	MEDIAN & RT			2
1863+50	MEDIAN & RT			2
1864+00	MEDIAN & RT			2
1864+50	MEDIAN & RT			2
1865+00	MEDIAN & RT			2
1865+50	MEDIAN & RT			2
1866+00	MEDIAN & RT			2
SUB-TOTAL =				115
RAMP DB				
203+00.0	RT			1
204+00.0	RT			1
205+00.0	RT			1
206+00.0	RT			1
207+00.0	RT			1
207+50.0	RT			1
208+00.0	RT			1
208+50.0	RT			1
209+00.0	RT			1
209+50.0	RT			1
210+00.0	RT			1
210+50.0	RT			1
211+00.0	RT			1
211+50.0	RT			1
212+00.0	RT			1
SUB-TOTAL =				15
HDR SUB-TOTAL =				130
US ROUTE 20 (V3 SECTION)				
1965+75.0	MEDIAN			1
1968+00.0	MEDIAN			1
1970+00.0	MEDIAN			1
1972+00.0	MEDIAN			1
1974+00.0	MEDIAN			1
1977+00.0	MEDIAN			1
1979+00.0	MEDIAN			1
1986+05.0	MEDIAN			1
1988+50.0	MEDIAN			1
SUB-TOTAL =				9
TOTAL =				139
28000400 PERIMETER EROSION BARRIER				
STATION	OFFSET	STATION	OFFSET	FT
US ROUTE 20 (V3 SECTION)				
1967+00.0	LT			193
1982+00.0	LT			124
SUB-TOTAL =				317
TEMPORARY RAMP DB (HDR SECTION)				
500+55.0		508+20.0	RT	775
SUB-TOTAL =				775
TOTAL =				1,092

28000500 INLET AND PIPE PROTECTION				
STATION	OFFSET	STATION	OFFSET	EACH
US ROUTE 20 (V3 SECTION)				
1981+60.0				1
SUB-TOTAL =				1
MEDIAN CROSSOVER (HDR SECTION)				
1698+50.0				1
MEDIAN PIPE				
1725+50.0				1
SUB-TOTAL =				2
TOTAL =				3
28100105 STONE RIPRAP, CLASS A3				
STATION	OFFSET	STATION	OFFSET	SQ YD
US ROUTE 20 (V3 SECTION)				
STA. 1981+58				8
TOTAL =				8
28200100 FILTER FABRIC FOR USE WITH RIPRAP				
STATION	OFFSET	STATION	OFFSET	SQ YD
US ROUTE 20 (V3 SECTION)				
STA. 1981+58				8
TOTAL =				8

31100100 SUB-BASE GRANULAR MATERIAL, TYPE A				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	TONS
115.0lbs/SY/in 12.0Thickness				
US ROUTE 20 (V3 SECTION)				
1914+71.0		1982+00.00		21,040.0
1964+60.0		1969+50.00		838.7
1969+50.0		1971+00.00		104.8
1971+00.0		1977+05.00		360.1
SMOKEY HOLLOW ROAD				
CROSS OVER, QUANTITY FROM MEDIAN CROSS OVER SHEET				
1977+05.0		1988+60.8		1,900.0
SUB-TOTAL =				24,960.0
U.S. ROUTE 20 (HDR SECTION)				
1690+59.34	1702+00.00	MEDIAN CROSS-OVER		1927.0
1697+00.00	1702+15.51	WB		1310
1702+00.00	1702+15.51	EB		21
1702+15.51	1714+00.00	WB		3622
1702+15.51	1710+00.00	EB		1312
1714+00.00	1781+50.00	WB		20638
1781+00.00	1781+50.00	WB - ROCK CUT		72.3
1792+00.00	1792+50.00	WB - ROCK CUT		72.3
1792+00.00	1796+50.00	EB		1376
1796+50.00	1809+00.00	WB		3437
1796+50.00	1812+75.00	EB		2621
1808+50.00	1809+00.00	WB - ROCK CUT		82.2
1814+50.00	1815+00.00	WB - ROCK CUT		86.7
1814+50.00	1841+09.95	WB		6694
1845+49.45	1848+28.89	EB		855
1848+28.89	1858+00.00	WB		2755
1848+28.89	1856+00.00	EB		2393
1855+50.00	1856+00.00	WB - ROCK CUT		54.2
1857+50.00	1858+00.00	WB - ROCK CUT		54.2
1867+00.00	1867+50.00	WB - ROCK CUT		72.3
1867+00.00	1904+34.31	WB		11130
TOTAL U.S. ROUTE 20				60585
IL ROUTE 75				
34+99.00	36+95.50			142
TOTAL IL ROUTE 75				142
RAMPS				
300+00.00	303+56.76	AD		455
303+56.76	306+64.28	AD		782
306+64.28	318+29.95	AD		2386
401+71.47	402+56.47	CA		190
402+56.47	411+86.34	CA		1903
411+86.34	414+95.45	CA		694
414+95.45	416+98.52	CA		364
416+98.52	419+05.45	CA		317
419+05.45	434+05.45	CA		2077
434+05.45	439+55.56	CA		560
104+00.00	104+50.00	BC - ROCK CUT		54.2
106+00.00	106+63.60	BC		194
106+63.60	118+38.15	BC		2404
201+71.47	202+56.47	DB		190
202+56.47	210+50.00	DB		1624
210+50.00	213+02.43	DB		517
213+02.43	214+00.00	DB		249
213+50.00	214+00.00	DB - ROCK CUT		57.8
224+50.00	225+00.00	DB - ROCK CUT		32.6
224+50.00	235+53.12	DB		1528
235+53.12	241+03.12	DB		602
TOTAL RAMPS				17178
TOTAL				77906
TOTAL =				102,866

31200100 STABILIZED SUBBASE, 4"				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	SQ YD
US ROUTE 20 (V3 SECTION)				
EB LANE (SAME AS PCC, 10")				654.0
CROSSOVER SECTION				1551.3
SUB-TOTAL =				2206.0
IL ROUTE 75 (HDR SECTION)				
34+99.00		36+95.50		229.0
US ROUTE 20				
1848+15.00	EB LT LANE/SHLDR	1865+76.97		4405.0
1848+15.00	EB RT LANE/SHLDR	1855+06.32		1728.0
1855+06.32	EB RT LANE	1865+76.97		1428.0
CROSSOVER SECTION				2964.0
SUB-TOTAL =				10,754.0
TOTAL =				12,960.0
40200800 AGGREGATE SURFACE COURSE, TYPE B				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	TONS
U.S. ROUTE 20 (HDR SECTION)				
1804+00.0				236
TOTAL =				236
40600895 CONSTRUCTING TEST STRIP				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)				
SURFACE COURSE				1
BINDER COURSE				1
TOTAL =				2
40600980 BIT. SURF. REMOVAL - BUTT JOINT				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	SQ YD
US ROUTE 20 (V3 SECTION)				
@ 1690+59.34	44 x 35 -			171.1
@ 1842+29.38	44 x 35 -			171.1
TOTAL =				343

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES
US ROUTE 20

SCALE: VERT. _____
HORIZ. _____
DATE: 10/28/05

DRAWN BY: DTE
CHECKED BY: JAM

CONTRACT NO. 64577				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	31
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
• 177-(3,4,4)-1-1				

SCHEDULE OF QUANTITIES

<p>42000501 PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ. YD.</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 EASTBOUND (HDR SECTION)</td> </tr> <tr> <td>1848+15.00</td> <td></td> <td>-</td> <td>1865+76.97</td> <td></td> <td>4,700.0</td> </tr> <tr> <td colspan="6">MEDIAN CROSS-OVER</td> </tr> <tr> <td>1690+59.34</td> <td></td> <td>-</td> <td>1702+00.00</td> <td></td> <td>2,735</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 7,435.0</td> </tr> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td>1964+60.0</td> <td>12.0</td> <td>-</td> <td>1969+50.0</td> <td>12.0</td> <td>654</td> </tr> <tr> <td colspan="6">CROSS OVER, QUANTITY FROM MEDIAN CROSS OVER SHEET</td> </tr> <tr> <td>1977+05.0</td> <td></td> <td>-</td> <td>1988+60.8</td> <td></td> <td>2,551</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 3,205</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 10,640</td> </tr> </tbody> </table> <p>42001165 BRIDGE APPROACH PAVEMENT</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ. YD.</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 (HDR SECTION)</td> </tr> <tr> <td>1842+09.95</td> <td></td> <td>-</td> <td>1842+39.95</td> <td></td> <td>139.0</td> </tr> <tr> <td>1844+19.45</td> <td></td> <td>-</td> <td>1844+49.45</td> <td></td> <td>142.0</td> </tr> <tr> <td>1904+40.31</td> <td></td> <td>-</td> <td>1904+70.31</td> <td></td> <td>139.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 420.0</td> </tr> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td>1914+41.97</td> <td></td> <td>-</td> <td>1914+71.97</td> <td></td> <td>144.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 144.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 564.0</td> </tr> </tbody> </table> <p>42001430 BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ. YD.</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 (HDR SECTION)</td> </tr> <tr> <td>1841+09.95</td> <td></td> <td>-</td> <td>1842+09.95</td> <td></td> <td>472.0</td> </tr> <tr> <td>1844+49.45</td> <td></td> <td>-</td> <td>1845+49.45</td> <td></td> <td>472.0</td> </tr> <tr> <td>1903+40.31</td> <td></td> <td>-</td> <td>1904+40.31</td> <td></td> <td>472.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 1417.0</td> </tr> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td>1941+71 WB</td> <td></td> <td>-</td> <td>1915+71</td> <td></td> <td>267.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 267.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 1684.0</td> </tr> </tbody> </table>	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ. YD.	US ROUTE 20 EASTBOUND (HDR SECTION)						1848+15.00		-	1865+76.97		4,700.0	MEDIAN CROSS-OVER						1690+59.34		-	1702+00.00		2,735						SUB-TOTAL = 7,435.0	US ROUTE 20 (V3 SECTION)						1964+60.0	12.0	-	1969+50.0	12.0	654	CROSS OVER, QUANTITY FROM MEDIAN CROSS OVER SHEET						1977+05.0		-	1988+60.8		2,551						SUB-TOTAL = 3,205						TOTAL = 10,640	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ. YD.	US ROUTE 20 (HDR SECTION)						1842+09.95		-	1842+39.95		139.0	1844+19.45		-	1844+49.45		142.0	1904+40.31		-	1904+70.31		139.0						SUB-TOTAL = 420.0	US ROUTE 20 (V3 SECTION)						1914+41.97		-	1914+71.97		144.0						SUB-TOTAL = 144.0						TOTAL = 564.0	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ. YD.	US ROUTE 20 (HDR SECTION)						1841+09.95		-	1842+09.95		472.0	1844+49.45		-	1845+49.45		472.0	1903+40.31		-	1904+40.31		472.0						SUB-TOTAL = 1417.0	US ROUTE 20 (V3 SECTION)						1941+71 WB		-	1915+71		267.0						SUB-TOTAL = 267.0						TOTAL = 1684.0	<p>44000100 PAVEMENT REMOVAL</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ. YD.</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td colspan="6">EASTBOUND LANE</td> </tr> <tr> <td colspan="6">SEE PCC CONCRETE 10"</td> </tr> <tr> <td colspan="6">CROSSOVER</td> </tr> <tr> <td colspan="6">SEE CROSSOVER SHEET</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">1,222</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 5,174</td> </tr> <tr> <td colspan="6">U.S. ROUTE 20 (HDR SECTION)</td> </tr> <tr> <td>STA. 1697+00 TO STA. 1702+00</td> <td></td> <td></td> <td>LT</td> <td></td> <td>1334</td> </tr> <tr> <td>STA. 1702+00 TO STA. 1706+00</td> <td></td> <td></td> <td>LT</td> <td></td> <td>1075</td> </tr> <tr> <td>STA. 1702+00 TO STA. 1706+00</td> <td></td> <td></td> <td>RT</td> <td></td> <td>367</td> </tr> <tr> <td>STA. 1706+00 TO STA. 1710+00</td> <td></td> <td></td> <td>LT/RT</td> <td></td> <td>1622</td> </tr> <tr> <td>STA. 1796+50 TO STA. 1812+75</td> <td></td> <td></td> <td>RT</td> <td></td> <td>1710</td> </tr> <tr> <td>STA. 1848+15 TO STA. 1865+76.97</td> <td></td> <td></td> <td>RT</td> <td></td> <td>6322</td> </tr> <tr> <td>STA. 1803+30 TO STA. 1804+46</td> <td></td> <td></td> <td>41' LT TO 184' RT</td> <td></td> <td>445</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL U.S. ROUTE 20 12875</td> </tr> <tr> <td colspan="6">EXIST. IL. ROUTE 75 RAMP</td> </tr> <tr> <td colspan="6">RADIUS RETURNS AT IL. ROUTE 75</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">770</td> </tr> <tr> <td colspan="6">RAMP</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">3732</td> </tr> <tr> <td colspan="6">RADIUS RETURNS AT U.S. ROUTE 20</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">845</td> </tr> <tr> <td colspan="6">TOTAL EXIST. IL. ROUTE 75 RAMP</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">5347</td> </tr> <tr> <td colspan="6">TEMPORARY PAVEMENT REMOVAL</td> </tr> <tr> <td colspan="6">LOCATION</td> </tr> <tr> <td colspan="6">U.S. ROUTE 20</td> </tr> <tr> <td>STA. 1703+00 TO STA. 1706+00</td> <td></td> <td></td> <td>20' LT TO 41' LT</td> <td></td> <td>200</td> </tr> <tr> <td>STA. 1803+28 TO STA. 1804+49</td> <td></td> <td></td> <td>20' LT TO 41' LT</td> <td></td> <td>324</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL U.S. ROUTE 20 524</td> </tr> <tr> <td colspan="6">IL. ROUTE 75</td> </tr> <tr> <td>STA. 32+00 TO STA. 34+00</td> <td></td> <td></td> <td>9' LT. TO 9' RT.</td> <td></td> <td>400</td> </tr> <tr> <td>STA. 35+58 TO STA. 36+45</td> <td></td> <td></td> <td></td> <td></td> <td>32</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL IL. ROUTE 75 432</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 19178</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 24,352</td> </tr> </tbody> </table> <p>44000500 COMBINATION CURB AND GUTTER REMOVAL</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>FOOT</th> </tr> </thead> <tbody> <tr> <td colspan="6">ISLAND AT US ROUTE 20 INTERSECTION (HDR SECTION)</td> </tr> <tr> <td colspan="6">WITH EXIST. CONNECTOR (STA. 1853+40) RT</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">39.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 39.0</td> </tr> </tbody> </table> <p>44003100 MEDIAN REMOVAL</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ. 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ROUTE 75											770	RAMP											3732	RADIUS RETURNS AT U.S. ROUTE 20											845	TOTAL EXIST. IL. ROUTE 75 RAMP											5347	TEMPORARY PAVEMENT REMOVAL						LOCATION						U.S. ROUTE 20						STA. 1703+00 TO STA. 1706+00			20' LT TO 41' LT		200	STA. 1803+28 TO STA. 1804+49			20' LT TO 41' LT		324						TOTAL U.S. ROUTE 20 524	IL. ROUTE 75						STA. 32+00 TO STA. 34+00			9' LT. TO 9' RT.		400	STA. 35+58 TO STA. 36+45					32						TOTAL IL. ROUTE 75 432						SUB-TOTAL = 19178						TOTAL = 24,352	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT	ISLAND AT US ROUTE 20 INTERSECTION (HDR SECTION)						WITH EXIST. CONNECTOR (STA. 1853+40) RT											39.0						TOTAL = 39.0	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ. FT.	ISLAND AT US ROUTE 20 INTERSECTION (HDR SECTION)						WITH EXIST. CONNECTOR (STA. 1853+40) RT											80						TOTAL = 80	<p>44004250 PAVED SHOULDER REMOVAL</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ. YD.</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td>1964+60 TO 1982+00</td> <td></td> <td></td> <td>LT</td> <td></td> <td>1,955.0</td> </tr> <tr> <td>1966+95 TO 1988+61</td> <td></td> <td></td> <td>RT</td> <td></td> <td>2,498.0</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 4,453.0</td> </tr> <tr> <td colspan="6">U.S. ROUTE 20 (HDR SECTION)</td> </tr> <tr> <td>STA. 1697+00 TO STA. 1702+00</td> <td></td> <td></td> <td>LT</td> <td></td> <td>1316</td> </tr> <tr> <td>STA. 1690+59.34 TO STA. 1702+00</td> <td></td> <td></td> <td>RT</td> <td></td> <td>761</td> </tr> <tr> <td>STA. 1702+00 TO STA. 1714+00</td> <td></td> <td></td> <td>LT/RT</td> <td></td> <td>1274</td> </tr> <tr> <td>STA. 1702+00 TO STA. 1706+00</td> <td></td> <td></td> <td>RT</td> <td></td> <td>267</td> </tr> <tr> <td>STA. 1796+50 TO STA. 1812+75</td> <td></td> <td></td> <td>RT</td> <td></td> <td>3423</td> </tr> <tr> <td>STA. 1825+22.49 TO STA. 1831+70.70</td> <td></td> <td></td> <td>RT</td> <td></td> <td>721</td> </tr> <tr> <td>STA. 1848+15 TO STA. 1865+76.97</td> <td></td> <td></td> <td>RT</td> <td></td> <td>3638</td> </tr> <tr> <td>STA. 1865+76.97 TO STA. 1883+69.80</td> <td></td> <td></td> <td>RT</td> <td></td> <td>1992</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL U.S. ROUTE 20 13392</td> </tr> <tr> <td colspan="6">EXIST. IL. ROUTE 75 RAMP</td> </tr> <tr> <td colspan="6">RADIUS RETURNS AT IL. ROUTE 75</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">271</td> </tr> <tr> <td colspan="6">RAMP</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">1659</td> </tr> <tr> <td colspan="6">RADIUS RETURNS AT U.S. ROUTE 20</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">328</td> </tr> <tr> <td colspan="6">TOTAL EXIST. IL. ROUTE 75 RAMP</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">2258</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 15,650</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 20,103</td> </tr> </tbody> </table>	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ. YD.	US ROUTE 20 (V3 SECTION)						1964+60 TO 1982+00			LT		1,955.0	1966+95 TO 1988+61			RT		2,498.0						SUB-TOTAL = 4,453.0	U.S. ROUTE 20 (HDR SECTION)						STA. 1697+00 TO STA. 1702+00			LT		1316	STA. 1690+59.34 TO STA. 1702+00			RT		761	STA. 1702+00 TO STA. 1714+00			LT/RT		1274	STA. 1702+00 TO STA. 1706+00			RT		267	STA. 1796+50 TO STA. 1812+75			RT		3423	STA. 1825+22.49 TO STA. 1831+70.70			RT		721	STA. 1848+15 TO STA. 1865+76.97			RT		3638	STA. 1865+76.97 TO STA. 1883+69.80			RT		1992						TOTAL U.S. ROUTE 20 13392	EXIST. IL. ROUTE 75 RAMP						RADIUS RETURNS AT IL. ROUTE 75											271	RAMP											1659	RADIUS RETURNS AT U.S. ROUTE 20											328	TOTAL EXIST. IL. ROUTE 75 RAMP											2258						SUB-TOTAL = 15,650						TOTAL = 20,103	<p>48100100 AGGREGATE SHOULDER TYPE A</p> <p>*****</p> <table border="1"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>TONS</th> </tr> </thead> <tbody> <tr> <td colspan="6">115.0lbs/SY/in</td> </tr> <tr> <td colspan="6">8.0Thickness</td> </tr> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td colspan="6">1.95 TONS/CU YD</td> </tr> <tr> <td>1914+71.0</td> <td>2.0</td> <td>-</td> <td>1982+00.0</td> <td>2.0</td> <td>648</td> </tr> <tr> <td>1965+50.0</td> <td>2.0</td> <td>-</td> <td>1977+05.0</td> <td>2.0</td> <td>111</td> </tr> <tr> <td>1980+05.0 x 2 SIDES</td> <td></td> <td>-</td> <td>1985+60.0</td> <td>6.0</td> <td>321</td> </tr> <tr> <td colspan="6">SMOKEY HOLLOW ROAD 2' x 544.2'</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">52</td> </tr> <tr> <td colspan="6">CROSS OVER, SEE MEDIAN CROSS OVER SHEET FOR DETAILS</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">423</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 1,555</td> </tr> <tr> <td colspan="6">MEDIAN CROSS-OVER (HDR SECTION)</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">410.0</td> </tr> <tr> <td colspan="6">WEST BOUND</td> </tr> <tr> <td>1697+00.00</td> <td>24</td> <td>-</td> <td>1702+15.51</td> <td></td> <td></td> </tr> <tr> <td>1702+15.51</td> <td>24</td> <td>-</td> <td>1714+00.00</td> <td></td> <td>222</td> </tr> <tr> <td>1714+00.00</td> <td>24</td> <td>-</td> <td>1796+50.00</td> <td></td> <td>1546</td> </tr> <tr> <td>1796+50.00</td> <td>24</td> <td>-</td> <td>1812+75.00</td> <td></td> <td>305</td> </tr> <tr> <td>1812+75.00</td> <td>24</td> <td>-</td> <td>1841+09.95</td> <td></td> <td>531</td> </tr> <tr> <td>1845+49.35</td> <td>24</td> <td>-</td> <td>1848+15.00</td> <td></td> <td>50</td> </tr> <tr> <td>1848+15.00</td> <td>24</td> <td>-</td> <td>1865+76.97</td> <td></td> <td>330</td> </tr> <tr> <td>1865+76.97</td> <td>24</td> <td>-</td> <td>1903+40.31</td> <td></td> <td>705</td> </tr> <tr> <td colspan="6">RAMP AD</td> </tr> <tr> <td>300+00.00</td> <td></td> <td>-</td> <td>303+56.76</td> <td></td> <td>67</td> </tr> <tr> <td>303+56.76</td> <td>16</td> <td>-</td> <td>306+64.28</td> <td></td> <td>58</td> </tr> <tr> <td>306+64.28</td> <td>16</td> <td>-</td> <td>318+29.95</td> <td></td> <td>437</td> </tr> <tr> <td colspan="6">RAMP BC</td> </tr> <tr> <td>100+00.00</td> <td></td> <td>-</td> <td>103+00.45</td> <td></td> <td>56</td> </tr> <tr> <td>103+00.45</td> <td>16</td> <td>-</td> <td>104+00.88</td> <td></td> <td>19</td> </tr> <tr> <td>104+00.88</td> <td>16</td> <td>-</td> <td>106+63.60</td> <td></td> <td>49</td> </tr> <tr> <td>106+63.60</td> <td>16</td> <td>-</td> <td>118+38.15</td> <td></td> <td>440</td> </tr> <tr> <td colspan="6">RAMP CA</td> </tr> <tr> <td>401+71.47</td> <td></td> <td>-</td> <td>402+56.47</td> <td></td> <td>32</td> </tr> <tr> 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<td>16</td> <td>-</td> <td>217+78.67</td> <td></td> <td>25</td> </tr> <tr> <td>217+78.67</td> <td></td> <td>-</td> <td>220+53.12</td> <td></td> <td>51</td> </tr> <tr> <td>220+53.12</td> <td>12</td> <td>-</td> <td>235+53.12</td> <td></td> <td>281</td> </tr> <tr> <td>235+53.12</td> <td></td> <td>-</td> <td>241+03.12</td> <td></td> <td>103</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTALS 6664</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">SUB-TOTAL = 7074</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">TOTAL = 8629</td> </tr> </tbody> </table>	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	TONS	115.0lbs/SY/in						8.0Thickness						US ROUTE 20 (V3 SECTION)						1.95 TONS/CU YD						1914+71.0	2.0	-	1982+00.0	2.0	648	1965+50.0	2.0	-	1977+05.0	2.0	111	1980+05.0 x 2 SIDES		-	1985+60.0	6.0	321	SMOKEY HOLLOW ROAD 2' x 544.2'											52	CROSS OVER, SEE MEDIAN CROSS OVER SHEET FOR DETAILS											423						SUB-TOTAL = 1,555	MEDIAN CROSS-OVER (HDR SECTION)											410.0	WEST BOUND						1697+00.00	24	-	1702+15.51			1702+15.51	24	-	1714+00.00		222	1714+00.00	24	-	1796+50.00		1546	1796+50.00	24	-	1812+75.00		305	1812+75.00	24	-	1841+09.95		531	1845+49.35	24	-	1848+15.00		50	1848+15.00	24	-	1865+76.97		330	1865+76.97	24	-	1903+40.31		705	RAMP AD						300+00.00		-	303+56.76		67	303+56.76	16	-	306+64.28		58	306+64.28	16	-	318+29.95		437	RAMP BC						100+00.00		-	103+00.45		56	103+00.45	16	-	104+00.88		19	104+00.88	16	-	106+63.60		49	106+63.60	16	-	118+38.15		440	RAMP CA						401+71.47		-	402+56.47		32	402+56.47	16	-	411+86.34		349	411+86.34	16	-	414+95.45		58	414+95.45	16	-	416+98.52		38	416+98.52		-	419+05.45		39	419+05.45	12	-	434+05.45		281	434+05.45		-	439+55.56		103	RAMP DB						201+71.47		-	202+56.47		32	202+56.47	16	-	210+50.00		297	210+50.00	16	-	213+02.43		95	213+02.43	16	-	216+43.12		64	216+43.12	16	-	217+78.67		25	217+78.67		-	220+53.12		51	220+53.12	12	-	235+53.12		281	235+53.12		-	241+03.12		103						TOTALS 6664						SUB-TOTAL = 7074						TOTAL = 8629
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1848+15.00	24	-	1865+76.97		330																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
1865+76.97	24	-	1903+40.31		705																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
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300+00.00		-	303+56.76		67																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
303+56.76	16	-	306+64.28		58																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
306+64.28	16	-	318+29.95		437																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
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100+00.00		-	103+00.45		56																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
103+00.45	16	-	104+00.88		19																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
104+00.88	16	-	106+63.60		49																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
106+63.60	16	-	118+38.15		440																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
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401+71.47		-	402+56.47		32																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
402+56.47	16	-	411+86.34		349																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
411+86.34	16	-	414+95.45		58																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
414+95.45	16	-	416+98.52		38																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
416+98.52		-	419+05.45		39																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
419+05.45	12	-	434+05.45		281																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
434+05.45		-	439+55.56		103																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
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201+71.47		-	202+56.47		32																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
202+56.47	16	-	210+50.00		297																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
210+50.00	16	-	213+02.43		95																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
213+02.43	16	-	216+43.12		64																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
216+43.12	16	-	217+78.67		25																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
217+78.67		-	220+53.12		51																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
220+53.12	12	-	235+53.12		281																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
235+53.12		-	241+03.12		103																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
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REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
SCALE: VERT. DRAWN BY DTE		SCHEDULE OF QUANTITIES
DATE 10/28/05 CHECKED BY JAM		
		US ROUTE 20

SCHEDULE OF QUANTITIES

48101200 AGGREGATE SHOULDERS, TYPE B			
STATION	WIDTH(FT)	- STATION	WIDTH(FT) TONS
U.S. ROUTE 20 (V3 SECTION)			
115 LB/SY/IN = 2.016 TON/CY			
CROSS SECTION AREA = 0.281 SF			
1690+59.34 TO 1714+00.00		OUTSIDE	49.1
1714+00.00 TO 1796+50.00		BOTH	346.2
1812+75.00 TO 1825+22.49		BOTH	52.3
1825+22.49 TO 1831+72.26		INSIDE	13.6
1831+72.26 TO 1842+29.38		BOTH	44.4
TOTAL =			506
48300300 PORTLAND CEMENT CONCRETE SHOULDERS, 8"			
STATION	WIDTH(FT)	- STATION	WIDTH(FT) SQ YD
US ROUTE 20 (V3 SECTION)			
STA 1964+60 TO 1977+05			
TOTAL =			1083
48300410 PORTLAND CEMENT CONCRETE SHOULDERS, 9.5"			
STATION	WIDTH(FT)	- STATION	WIDTH(FT) SQ YD
IL ROUTE 75 (HDR SECTION)			
STA 34+99.0 TO 36+95.5			
TOTAL =			219
48300500 PORTLAND CEMENT CONCRETE SHOULDERS, 10"			
STATION	WIDTH(FT)	- STATION	WIDTH(FT) SQ YD
US ROUTE 20 (HDR SECTION)			
STA 1848+15.00	EB LT	1865+76.97	1957.7
STA 1848+15.00	EB RT	1855+06.32	768.1
TOTAL =			2726
50105220 PIPE CULVERT REMOVAL			
STATION	WIDTH(FT)	- STATION	WIDTH(FT) FOOT
IL ROUTE 20 (HDR SECTION)			
STA 1803+50 TO 1804+35	2' LT		85
STA 1803+50 TO 1804+35	2' RT		85
IL ROUTE 75			
STA 32+32 TO 33+48	95' RT TO 90' RT		118
STA 33+48	90' RT TO 105' RT		15
SUB-TOTAL =			303
IL ROUTE 20 (V3 SECTION)			
STA 1971+00	RT		65
SMOKEY HOLLOW ROAD			
			33
SUB-TOTAL =			98
TOTAL =			401

54213447 END SECTIONS 12"				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
1842+29.37	LT			1
1842+29.37	RT			1
1904+55.71	LT			1
1904+55.71	RT			1
SUB-TOTAL =				4
US ROUTE 20 (V3 SECTION)				
1915+00.00	RT			1
1915+00.00	LT			1
TOTAL =				6
54213657 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"				
STATION	WIDTH(FT)	- STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)				
RAMP AD STA. 304+00.0				
				1
RAMP DB STA. 216+40.0				
				1
TEMP RAMP DB 211+00.0				
				2
TOTAL =				4
54213669 PRECAST REINFORCED CONCRETE FES 24"				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (V3 SECTION)				
1981+50.0				1
TOTAL =				1
54213693 PRECAST REINFORCED CONCRETE FLARED END SECTIONS 48"				
STATION	WIDTH(FT)	- STATION	WIDTH(FT)	EACH
IL ROUTE 75 (HDR SECTION)				
STA. 35+07.50 RT				
				1
TOTAL =				1
54244405 FLUSH INLET BOX FOR MEDIAN, STD 542546				
STATION	WIDTH(FT)	- STATION	WIDTH(FT)	EACH
US ROUTE 20 (V3 SECTION)				
STA. 1981+50.00				
				1
TOTAL =				1
54246405 INLET BOX, STANDARD 542531				
STATION	WIDTH(FT)	- STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)				
1725+50	0.00' LT			1
1727+50	0.00' LT			1
TOTAL =				2

54247190 GRATING FOR CONCRETE FLARED END SECTION, 48"				
STATION	WIDTH(FT)	- STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)				
STA. 35+07.50 RT				
				1
TOTAL =				1
60100945 PIPE DRAINS 12"				
STATION	WIDTH(FT)	- STATION	WIDTH(FT)	FOOT
US ROUTE 20 (HDR SECTION)				
STA. 1842+29.37	LT			110
STA. 1842+29.37	RT			20
STA. 1904+55.71	LT			40
STA. 1904+55.71	RT			20
SUB-TOTAL =				190
US ROUTE 20 (V3 SECTION)				
STA. 1915+00.00	RT			24
STA. 1915+00.00	LT			24
SUB-TOTAL =				48
TOTAL =				238
60221100 MANHOLES, TYPE A, 5' DIAMETER, TYPE 1 FRAME, CLOSED LID				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
1725+68.5	14.00' LT			1
1727+31.5	14.00' LT			1
TOTAL =				2
60234200 INLETS, TYPE A, TYPE 1 FRAME, OPEN LID				
STATION	OFFSET	RIM	INVERT	EACH
US ROUTE 20 (HDR SECTION)				
RAMP DB				
213+05.00	20.00' LT	827.61	826.03 (E)	1
214+43.12	21.00' LT	825.01	823.43 (W) 823.33 (E)	1
216+40.00	18.00' LT	820.83	816.73 (S) 819.25 (W)	1
RAMP BC				
106+47.00	28.00' LT	828.92	826.82 (E) 826.92 (W)	1
RAMP AD				
304+00.00	19.30' LT	821.94	817.54 (S) 820.17 (E)	1
306+58.50	27.00' LT	823.81	822.04 (W)	1
RAMP CA				
411+90.00	21.00' LT	822.91	821.33 (W)	1
412+95.45	20.00' LT	822.06	820.38 (W) 820.48 (E)	1
TOTAL =				8
60236200 INLETS, TYPE A, TYPE B GRATE				
STATION	OFFSET	RIM	INVERT	EACH
US ROUTE 20 (HDR SECTION)				
RAMP BC				
106+67.40	24.16' LT	828.50	827.08	1
TOTAL =				1

60618300 CONCRETE MEDIAN SURFACE, 4"				
STATION	OFFSET	- STATION	OFFSET	SQ FT
IL ROUTE 75 (HDR SECTION)				
32+00.00 TO 34+00.00				
TOTAL =				3,122
60900315 TYPE D INLET BOX, STANDARD 609006				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
STA. 1842+29.37	LT			1
STA. 1842+29.37	RT			1
STA. 1904+55.71	LT			1
STA. 1904+55.71	RT			1
SUB-TOTAL =				4
US ROUTE 20 (V3 SECTION)				
STA. 1915+00.00	RT			1
STA. 1915+00.00	LT			1
SUB-TOTAL =				2
TOTAL =				6
60900515 CONCRETE THRUST BLOCKS				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
STA. 1842+29.37	LT			1
STA. 1842+29.37	RT			1
STA. 1904+55.71	LT			1
STA. 1904+55.71	RT			1
SUB-TOTAL =				4
US ROUTE 20 (V3 SECTION)				
STA. 1915+00.00	RT			1
STA. 1915+00.00	LT			1
SUB-TOTAL =				2
TOTAL =				6
61100500 EXPLORATION TRENCH 52" DEPTH				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
CONTINGENCY				
TOTAL =				200
TOTAL =				200
61133200 FIELD TILE JUNCTION VAULTS, 3' DIA.				
STATION	OFFSET	- STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
CONTINGENCY				
TOTAL =				5
TOTAL =				5

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCHEDULE OF QUANTITIES US ROUTE 20

SCALE: VERT. DATE 10/28/05
DRAWN BY DTE
CHECKED BY JAM

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	33
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
* 177-(3,4,4)-1				

SCHEDULE OF QUANTITIES

63000000 STEEL PLATE BEAM GUARD RAIL, TYPE A				
STATION	OFFSET	STATION	OFFSET	FOOT
US ROUTE 20 (V3 SECTION)				
1914+79.5	61.0 RT	1917+42.0	61.0 RT	262.50
1914+79.5	19.0 RT	1918+04.5	19.0 RT	325.00
SUB-TOTAL =				587.5
US ROUTE 20 (HDR SECTION)				
1745+74.06	-	1750+36.5	RT.	462.5
1839+53.49	-	1842+21.54	RT.	275.0
1839+52.45	-	1842+21.2	RT.	275.0
1844+49.45	-	1847+24.45	LT.	275.0
1844+49.95	-	1847+86.95	LT.	337.5
1902+06.52	-	1904+75.27	RT.	275.0
1902+06.66	-	1904+75.41	RT.	275.0
SUB-TOTAL =				2175.0
TOTAL =				2763
63100045 TRAFFIC BARRIER TERMINAL, TYPE 2				
STATION	OFFSET	STATION	OFFSET	EACH
US ROUTE 20 (HDR SECTION)				
STA. 1750+49				1
SUB-TOTAL =				1
US ROUTE 20 (V3 SECTION)				
STA. 314+41 EB				2
TOTAL =				3
63100070 TRAFFIC BARRIER TERMINAL, TYPE 5				
STATION	OFFSET	STATION	OFFSET	EACH
US ROUTE 20 (V3 SECTION)				
1914+71.97 EB				2
TOTAL =				2
63100085 TRAFFIC BARRIER TERMINAL, TYPE 6				
STATION	OFFSET	STATION	OFFSET	EACH
US ROUTE 20 (V3 SECTION)				
1914+41.9	61.0 RT	1914+79.0		1
1914+41.9	19.0 RT	1914+79.0		1
SUB-TOTAL =				2
US ROUTE 20 (HDR SECTION)				
WB SOUTH SIDE @ IL 75				1
WB NORTH SIDE @ IL 75				1
EB SOUTH SIDE @ IL 75				1
EB NORTH SIDE @ IL 75				1
STA. 1905+00				1
STA. 1905+00				1
SUB-TOTAL =				6
TOTAL =				8

63100169 TRAFFIC BARRIER TERMINAL TYPE 1 SPECIAL (FLARED)				
STATION	OFFSET	STATION	OFFSET	EACH
US ROUTE 20 (V3 SECTION)				
1917+42.0	61.0 RT	1917+92.0		1
1918+04.5	19.0 RT	1918+54.5		1
SUB-TOTAL =				2
US ROUTE 20 (HDR SECTION)				
@ IL ROUTE 75 EB				2
@ IL ROUTE 75 WB				2
@ PECATONICA EB				1
@ PECATONICA EB				1
@ BOX CULVERT EXTENSION EB				1
SUB-TOTAL =				7
TOTAL =				9
63200310 GUARDRAIL REMOVAL				
STATION	OFFSET	STATION	OFFSET	FOOT
US ROUTE 20 (HDR SECTION)				
1725+82.05	6' RT	1727+07.05		125.00
1745+49.06	63.5' RT	1750+49.06		500.00
1839+88.39	16.9' RT	1842+50.89		262.50
1839+37.81	60.8' RT	1842+50.31		312.50
1844+05.97	13.2' RT	1847+18.47		312.50
1844+05.96	64.9' RT	1846+68.46		262.50
1902+42.49	17.2' RT	1905+04.99		262.50
1901+91.98	62.1' RT	1905+04.48		312.50
1885+67.60	4.7' RT	1886+92.60		125.00
SUB-TOTAL =				2475
US ROUTE 20 (V3 SECTION)				
1914+41.00	16.3' EB	1917+16.00		275.00
1914+41.00	61.5' EB	1917+16.00		275.00
SUB-TOTAL =				550.00
TOTAL =				3025

63500105 DELINEATORS					
STATION	OFFSET	STATION	OFFSET	COMMENTS	EACH
US RTE 20 (V3 SECTION)					
STA 1914+41 TO STA 1982+00	LT			SINGLE WHITE	17
V3 SUB-TOTAL =					17
U.S. ROUTE 20 (HDR SECTION)					
STA. 1690+60 TO STA. 1773+57	LT			SINGLE WHITE	20
STA. 1690+60 TO STA. 1745+24	RT			SINGLE WHITE	15
STA. 1750+47 TO STA. 1773+57	RT			SINGLE WHITE	5
STA. 1773+57 TO STA. 1796+89	LT/RT			SINGLE WHITE	40
STA. 1796+89 TO STA. 1815+11	RT			SINGLE WHITE	3
STA. 1796+89 TO STA. 1804+23	LT			SINGLE WHITE	2
STA. 1815+11 TO STA. 1825+23	RT			SINGLE WHITE	9
STA. 1831+71 TO STA. 1839+03	RT			SINGLE WHITE	6
STA. 1832+33 TO STA. 1842+10	LT			SINGLE WHITE	8
STA. 1846+71 TO STA. 1855+07	RT			SINGLE WHITE	4
STA. 1847+74 TO STA. 1855+34	LT			SINGLE WHITE	4
STA. 1861+84 TO STA. 1901+26	LT			SINGLE WHITE	29
STA. 1883+69 TO STA. 1901+26	RT			SINGLE WHITE	14
STA. 1901+26 TO STA. 1904+41	LT			SINGLE WHITE	2
TOTAL U.S. ROUTE 20					161
RAMP AD					
STA. 300+00 TO STA. 306+64	RT			DOUBLE WHITE	7
STA. 306+64 TO STA. 318+30	RT			SINGLE WHITE	13
STA. 306+64 TO STA. 318+30	LT			SINGLE YELLOW	13
TOTAL RAMP AD					33
RAMP CA					
STA. 401+07 TO STA. 411+87	RT			SINGLE WHITE	12
STA. 411+87 TO STA. 439+56	RT			DOUBLE WHITE	28
STA. 401+71 TO STA. 411+87	LT			SINGLE YELLOW	12
TOTAL RAMP CA					52
RAMP BC					
STA. 100+00 TO STA. 106+63	RT			DOUBLE WHITE	7
STA. 106+63 TO STA. 118+96	RT			SINGLE WHITE	13
STA. 106+63 TO STA. 118+38	LT			SINGLE YELLOW	13
TOTAL RAMP BC					33
RAMP DB					
STA. 201+07 TO STA. 213+02	RT			SINGLE WHITE	13
STA. 213+02 TO STA. 241+03	RT			DOUBLE WHITE	28
STA. 201+71 TO STA. 213+02	LT			SINGLE YELLOW	13
TOTAL RAMP DB					54
HDR SUBTOTAL					333
TOTAL =					350
66500105 WOVEN WIRE FENCE, 4 FT					
STATION	OFFSET	STATION	OFFSET	FT	
US RTE 20 (V3 SECTION)					
1939+49.0	131' RT	1939+83.0		34.0	
SUB-TOTAL =					34.0
US RTE 20 (HDR SECTION)					
1802+87.0	130' LT	1804+87.0	135' LT	200.0	
RAMP DB					
209+90	103' RT	202+73	83' RT	83.0	
SUB-TOTAL =					283.0
TOTAL =					317.0

70300100 SHORT-TERM PAVEMENT MARKING				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	FOOT
US ROUTE 20 (HDR SECTION)				
1690+00.00	1842+29.48	EB US20 LT		15229
1690+00.00	1842+29.48	EB US 20 CL		3807
306+64.28	318+29.95	RAMP AD LT		1166
300+00.00	318+29.95	RAMP AD RT		1830
SUB-TOTAL =				22032
US ROUTE 20 (V3 SECTION)				
1690+59.34	TO 1842+29.38	EDGE LINE X2		30340
1690+59.34	TO 1842+29.38	SKIP-DASH X2		7585
SUB-TOTAL =				37925
TOTAL =				59957
70300220 TEMPORARY PAVEMENT MARKING, LINE 4"				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	FOOT
US ROUTE 20 (HDR SECTION)				
STAGE II				
Temporary Ramp				
RT		SOLID WHITE		550
LT		SOLID YELLOW		550
TOTAL				1100
70300260 TEMPORARY PAVEMENT MARKING - LINE 12"				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	FOOT
IL ROUTE 75 (HDR SECTION)				
31+00	0.0'	34+70	0.0'	YELLOW DIAGONALS FOR MEDIAN 227
TOTAL =				227
70300280 TEMPORARY PAVEMENT MARKING - LINE 24"				
STATION	WIDTH(FT)	STATION	WIDTH(FT)	FOOT
IL ROUTE 75 (HDR SECTION)				
IL ROUTE 75				12
STAGE II - TEMP. RAMP TERMINAL AT US ROUTE 20				16
TOTAL				28

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

US ROUTE 20

SCALE: VERT. DATE 10/28/05
 HORIZ. DATE 10/28/05

DRAWN BY DTE
 CHECKED BY JAM

CONTRACT NO. 64577				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	34
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
* 177-13.4.4-11-1				

SCHEDULE OF QUANTITIES

<p>70400100 TEMPORARY CONCRETE BARRIER *****</p> <table border="1" style="width: 100%; border-collapse: collapse; font-size: small;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>FOOT</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 (HDR SECTION)</td> </tr> <tr> <td colspan="6">EASTBOUND US ROUTE 20 - STAGE II</td> </tr> <tr> <td>1704+50.0</td> <td></td> <td></td> <td>1710+00.0</td> <td></td> <td>550.0</td> </tr> <tr> <td>1853+58.0</td> <td></td> <td></td> <td>1866+70.5</td> <td></td> <td>1312.5</td> </tr> <tr> <td colspan="5" style="text-align: right;">SUB-TOTAL =</td> <td style="text-align: right;">1862.5</td> </tr> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td colspan="6">STD. 701402</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">1500</td> </tr> <tr> <td colspan="6">WORKZONE</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">490</td> </tr> <tr> <td colspan="5" style="text-align: right;">SUB-TOTAL =</td> <td style="text-align: right;">1990.0</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL =</td> <td style="text-align: right;">3852.5</td> </tr> </tbody> </table> <p>70400200 RELOCATE TEMPORARY CONCRETE BARRIER *****</p> <table border="1" style="width: 100%; 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border-collapse: collapse; font-size: small;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ FT</th> </tr> </thead> <tbody> <tr> <td colspan="6">US ROUTE 20 (HDR SECTION)</td> </tr> <tr> <td colspan="6">EASTBOUND</td> </tr> <tr> <td>EB1</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">145.7</td> </tr> <tr> <td>EB2</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">143.8</td> </tr> <tr> <td>EB3</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">156.9</td> </tr> <tr> <td>EB4</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">26.2</td> </tr> <tr> <td>EB5</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">62.3</td> </tr> <tr> <td colspan="6">WESTBOUND</td> </tr> <tr> <td>WB1</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">146.7</td> </tr> <tr> <td>WB2</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">143.8</td> </tr> <tr> <td>WB3</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">155.9</td> </tr> <tr> <td>WB4</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">26.2</td> </tr> <tr> <td>WB5</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">62.3</td> </tr> <tr> <td colspan="6">IL ROUTE 75</td> </tr> <tr> <td>75N1</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">274.9</td> </tr> <tr> <td>75N2</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">138.2</td> </tr> <tr> <td>75S1</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">265.2</td> </tr> <tr> <td>75S2</td> <td></td> <td></td> <td></td> <td></td> <td style="text-align: right;">160.5</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL =</td> <td style="text-align: right;">1909.0</td> </tr> </tbody> </table>	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT	US ROUTE 20 (HDR SECTION)						EASTBOUND US ROUTE 20 - STAGE II						1704+50.0			1710+00.0		550.0	1853+58.0			1866+70.5		1312.5	SUB-TOTAL =					1862.5	US ROUTE 20 (V3 SECTION)						STD. 701402											1500	WORKZONE											490	SUB-TOTAL =					1990.0	TOTAL =					3852.5	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT	US ROUTE 20 (HDR SECTION)						EASTBOUND US ROUTE 20 - STAGE III						1846+23.0			1856+48.0		1025.0	EASTBOUND US ROUTE 20 - STAGE IV						1846+20.0			1866+70.0		2050.0	TOTAL =					3075.0	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ FT	US ROUTE 20 (HDR SECTION)						EASTBOUND						EB1					15.1	EB2					15.1	EB3					15.1	WESTBOUND						WB1					15.1	WB2					15.1	WB3					15.1	TOTAL =					91.0	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	SQ FT	US ROUTE 20 (HDR SECTION)						EASTBOUND						EB1					145.7	EB2					143.8	EB3					156.9	EB4					26.2	EB5					62.3	WESTBOUND						WB1					146.7	WB2					143.8	WB3					155.9	WB4					26.2	WB5					62.3	IL ROUTE 75						75N1					274.9	75N2					138.2	75S1					265.2	75S2					160.5	TOTAL =					1909.0	<p>72400100 REMOVE SIGN PANEL ASSEMBLY - TYPE A *****</p> <table border="1" style="width: 100%; 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(HDR SECTION)</td> </tr> <tr> <td colspan="6">STA. 1697+00 TO STA. 1842+10</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">3627</td> </tr> <tr> <td colspan="6">STA. 1844+49 TO STA. 1904+40</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">1498</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL U.S. 20 - W.B.</td> <td style="text-align: right;">5125</td> </tr> <tr> <td colspan="6">U.S. 20 - E.B.</td> </tr> <tr> <td colspan="6">STA. 1702+00 TO STA. 1710+00</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">200</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL U.S. 20 - E.B.</td> <td style="text-align: right;">200</td> </tr> <tr> <td colspan="5" style="text-align: right;">SUB-TOTAL</td> <td style="text-align: right;">5325</td> </tr> <tr> <td colspan="6">US ROUTE 20 (V3 SECTION)</td> </tr> <tr> <td colspan="6">SKIP-DASH, 2' PAINT = 6' SPACE</td> </tr> <tr> <td colspan="6">STA. 1914+41 TO 1998+81 WB LT</td> </tr> <tr> <td colspan="5"></td> <td style="text-align: right;">2,110</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL =</td> <td style="text-align: right;">7435</td> </tr> </tbody> </table> <p>78008200 POLYUREA PAVEMENT MARKING, TYPE I - LETTERS AND SYMBOLS *****</p> <table border="1" style="width: 100%; border-collapse: collapse; font-size: small;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>-</th> <th>STATION</th> <th>OFFSET</th> <th>SQ FT</th> </tr> </thead> <tbody> <tr> <td colspan="6">IL ROUTE 75 (HDR SECTION)</td> </tr> <tr> <td>STA. 35+75</td> <td></td> <td></td> <td></td> <td>LT</td> <td style="text-align: right;">36.4</td> </tr> <tr> <td>STA. 38+25</td> <td></td> <td></td> <td></td> <td>LT</td> <td style="text-align: right;">36.4</td> </tr> <tr> <td>STA. 41+40</td> <td></td> <td></td> <td></td> <td>RT</td> <td style="text-align: right;">36.4</td> </tr> <tr> <td>STA. 43+90</td> <td></td> <td></td> <td></td> <td>RT</td> <td style="text-align: right;">36.4</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL RAMP IL ROUTE 75</td> <td style="text-align: right;">146</td> </tr> <tr> <td colspan="5" style="text-align: right;">TOTAL =</td> <td style="text-align: right;">146</td> </tr> </tbody> </table>	STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT	US ROUTE 20 (HDR SECTION)						STA. 1827+06.49 TO STA. 1829+14.51											53	STA. 1855+33.81 TO STA. 1858+89.34											356	TOTAL US ROUTE 20 - W.B.					409	US ROUTE 20 - E.B.						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REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULE OF QUANTITIES US ROUTE 20
NAME	DATE	
SCALE: VERT. DRAWN BY DTE HORIZ. CHECKED BY JAM DATE 10/28/05		

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	35
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
* 177-(3,4,4-1)-1				

SCHEDULE OF QUANTITIES

78008210 POLYUREA PAVEMENT MARKING TYPE I - LINE 4"				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) FOOT
US ROUTE 20 (HDR SECTION)				
STA. 1697+00 TO STA. 1804+23	51'		LT	10723
STA. 1697+00 TO STA. 1842+10	27'		LT	14510
STA. 1804+23 TO STA. 1827+07	51'		LT	572
STA. 1804+23 TO STA. 1809+73	51'		LT TO 63' LT	550
STA. 1809+73 TO STA. 1824+95	63'		LT	1522
STA. 1829+14 TO STA. 1842+10	51'		LT	1296
STA. 1844+49 TO STA. 1855+34	51'		LT	1085
STA. 1844+49 TO STA. 1904+43	27'		LT	5994
STA. 1861+84 TO STA. 1904+43	51'		LT	4260
TOTAL U.S. 20 - W.B.				40512
U.S. 20 - E.B.				
STA. 1702+00 TO STA. 1714+00	27'		RT	1200
STA. 1796+50 TO STA. 1812+75	51'		RT	1625
STA. 1796+50 TO STA. 1812+75	27'		RT	1625
STA. 1848+15 TO STA. 1865+77	27'		RT	1762
STA. 1862+74 TO STA. 1878+08	63'		RT	1534
STA. 1865+77 TO STA. 1883+70	51'		RT	450
STA. 1878+08 TO STA. 1904+43	63'		RT TO 51' LT	562
TOTAL U.S. 20 - E.B.				8758
U.S. 20				
STA. 1842+09.95 TO STA. 1844+49.45	51'		LT	240
STA. 1904+43.14 TO STA. 1904+73.14	51'		LT	30
STA. 1842+09.95 TO STA. 1844+49.45	27'		LT	240
STA. 1848+15.00 TO STA. 1855+06.72	51'		RT	692
STA. 1859+92.62 TO STA. 1865+76.97	51'		RT	146
STA. 1904+43.14 TO STA. 1904+73.14	27'		LT	30
TOTAL U.S. 20				1378
RAMP BC				
STA. 100+00 TO STA. 118+39	16'		RT	1839
STA. 106+63 TO STA. 118+38	16'		LT	1175
STA. 118+38.15 TO STA. 119+69.78	LT			114
STA. 118+38.15 TO STA. 119+69.78	RT			223
TOTAL RAMP BC				3351
RAMP DB				
STA. 201+71 TO STA. 220+53	16'		RT	1882
STA. 201+71 TO STA. 213+02	16'		LT	1131
STA. 200+26.56 TO STA. 201+71.47	LT			128
STA. 200+26.56 TO STA. 201+71.47	RT			181
TOTAL RAMP DB				3322
RAMP AD				
STA. 300+00 TO STA. 318+30	16'		RT	1830
STA. 306+64 TO STA. 318+30	16'		LT	1166
STA. 318+29.95 TO STA. 319+58.12	LT			111
STA. 318+29.95 TO STA. 319+58.12	RT			219
TOTAL RAMP AD				3326
RAMP CA				
STA. 401+71 TO STA. 419+05	16'		RT	1734
STA. 401+71 TO STA. 411+86	16'		LT	1015
STA. 400+26.56 TO STA. 401+71.47	LT			128
STA. 400+26.56 TO STA. 401+71.47	RT			181
TOTAL RAMP CA				3058
SUB-TOTAL = 63705				
US ROUTE 20 (V3 SECTION)				
STA. 1914+41 TO 1998+81	WB		LT/RT	16,880
STA. 1960+00 TO 1993+00	EB		LT/RT	6,600
STA. 1904+70 TO 1914+41	WB		LT/RT	1,942
SUB-TOTAL = 25,422				
TOTAL = 89,127				

78008230 POLYUREA PAVEMENT MARKING TYPE I - LINE 6"				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) FOOT
US ROUTE 20 (V3 SECTION)				
SKIP-DASH, 2' PAINT = 6' SPACE				
STA. 1960+00 TO 1993+00	EB		LT	825
STA. 1904+70 TO 1914+41	WB		LT	243
SUB-TOTAL = 1,068				
U.S. 20 - W.B. (HDR SECTION)				
STA. 1842+10 TO STA. 1844+49	39'		LT	60
STA. 1904+40 TO STA. 1904+73	39'		LT	8
TOTAL U.S. 20 - W.B.				68
U.S. 20 - E.B.				
STA. 1848+15 TO STA. 1865+77	39'		RT	440
TOTAL U.S. 20 - E.B. 440				
SUB-TOTAL = 508				
TOTAL = 1576				
78008240 POLYUREA PAVEMENT MARKING, TYPE I - LINE 8"				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) FOOT
US ROUTE 20 (HDR SECTION)				
RAMP BC				
PAINTED ISLAND WHITE EDGE LINE 172				
TOTAL RAMP BC 172				
RAMP DB				
PAINTED ISLAND WHITE EDGE LINE 169				
TOTAL RAMP DB 169				
RAMP AD				
PAINTED ISLAND WHITE EDGE LINE 166				
TOTAL RAMP AD 166				
RAMP CA				
PAINTED ISLAND WHITE EDGE LINE 171				
TOTAL RAMP CA 171				
TOTAL = 678				
78008250 POLYUREA PAVEMENT MARKING, TYPE I - LINE 12"				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) FOOT
US ROUTE 20 (HDR SECTION)				
RAMP BC				
PAINTED ISLAND WHITE DIAGONALS 154				
TOTAL RAMP BC 154				
RAMP DB				
PAINTED ISLAND WHITE DIAGONALS 283				
TOTAL RAMP DB 283				
RAMP AD				
PAINTED ISLAND WHITE DIAGONALS 123				
TOTAL RAMP AD 123				
RAMP CA				
PAINTED ISLAND WHITE DIAGONALS 291				
TOTAL RAMP CA 291				
TOTAL = 851				

78008270 POLYUREA PAVEMENT MARKING, TYPE I - LINE 24"				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) FOOT
IL ROUTE 75 (HDR SECTION)				
STA. 34+00			WHITE STOP BAR LT	20
STA. 34+67			WHITE STOP BAR LT	27
STA. 45+00			WHITE STOP BAR RT	27
STA. 45+73			WHITE STOP BAR RT	20
TOTAL RAMP IL ROUTE 75				94
TOTAL = 94				
78100100 RAISED REFLECTIVE PAVEMENT MARKER				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) EACH
US ROUTE 20 (V3 SECTION)				
1690+59.34	2 x 80'	-	1842+29.38	380
1914+41.0	WB 2 x 80'	-	1998+81.0	211
1960+00.0	EB 2 x 80'	-	1993+00.0	83
SUB-TOTAL = 674				
US ROUTE 20 (HDR SECTION)				
STA. 1697+00 TO STA. 1904+73.14			LT	520
STA. 1702+00 TO STA. 1710+00			RT	18
STA. 1796+50 TO STA. 1812+75			RT	40
STA. 1828+17 TO STA. 1831+71			RT	18
STA. 1848+15 TO STA. 1865+76.97			RT	44
STA. 1855+34 TO STA. 1858+90			LT	18
TOTAL U.S. ROUTE 20				658
RAMP AD				
STA. 300+00 TO STA. 306+64			RT	35
STA. 306+64 TO STA. 311+41			LT	13
TOTAL RAMP AD				48
RAMP BC				
STA. 100+00 TO STA. 106+64			RT	35
STA. 106+63 TO STA. 111+85			LT	15
TOTAL RAMP BC				50
SUB-TOTAL = 756				
TOTAL = 1430				
78100105 RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) EACH
US ROUTE 20 (V3 SECTION)				
1904+70.0	WB 2 x 80'	-	1914+41.0	25
TOTAL = 25				
78100300 REPLACEMENT REFLECTOR				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) EACH
US ROUTE 20 (V3 SECTION)				
1960+00.0	2 x 80'	-	1993+00.0	84
SUB-TOTAL = 84				
TOTAL = 84				

78200100 MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) EACH
US ROUTE 20 (HDR SECTION)				
1704+50.0	1710+00.0		EASTBOUND US ROUTE 20 - STAGE II	12
1853+58.0	1866+70.5		EASTBOUND US ROUTE 20 - STAGE II	27
1846+23.0	1856+48.0		EASTBOUND US ROUTE 20 - STAGE III	22
1846+20.0	1866+70.0		EASTBOUND US ROUTE 20 - STAGE IV	42
SUB-TOTAL = 103				
US ROUTE 20 (V3 SECTION)				
1 EVERY 50 FT + 1				
STD. 701402 AND WORKZONE LENGTH, 1900 FT				
TOTAL = 41				
TOTAL = 144				
78200410 GUARDRAIL MARKERS, TYPE A				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) EACH
US ROUTE 20 (V3 SECTION)				
1914+79.5	LT 1 x 80'	-	1917+42.0	4.0
1914+79.5	RT 1 x 80'	-	1918+04.5	5.0
SUB-TOTAL = 9.0				
US ROUTE 20 (HDR SECTION)				
STA. 1722+26 TO STA. 1726+26			RT	5
STA. 1726+68 TO STA. 1730+68			LT	5
STA. 1779+32.12 TO STA. 1783+19.62			LT	5
STA. 1839+02.79 TO STA. 1840+40.29			RT	2
STA. 1839+06 TO STA. 1839+90			RT	1
STA. 1844+19 TO STA. 1847+74			LT	5
STA. 1844+19.50 TO STA. 1848+37			LT	6
STA. 1881+80 TO STA. 1886+18			RT	6
STA. 1886+40 TO STA. 1890+78			LT	6
STA. 1901+57 TO STA. 1902+94			RT	2
STA. 1901+56 TO STA. 1902+44			RT	1
TOTAL U.S. ROUTE 20				44
SUB-TOTAL = 44				
TOTAL = 53				
78200510 BARRIER WALL MARKERS, TYPE A				
STATION	WIDTH(FT)	-	STATION	WIDTH(FT) EACH
US ROUTE 20 (HDR SECTION)				
STA. 1842+40 TO STA. 1844+20			LT	2
STA. 1842+40 TO STA. 1844+20			RT	1
STA. 1904+70 TO STA. 1914+42			LT	6
TOTAL = 9				

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

US ROUTE 20

SCALE: VERT. DATE 10/28/05

DRAWN BY DTE
CHECKED BY JAM

SCHEDULE OF QUANTITIES

CONTRACT NO. 64577

P.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	280	36
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
• 177-(3,4,4-1)-1				

<p>78201000 TERMINAL MARKER - DIRECT APPLIED</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>OFFSET</th> <th>EACH</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 (HDR SECTION)</td></tr> <tr><td>EB MAINLINE @ IL ROUTE 75</td><td></td><td></td><td></td><td>2</td></tr> <tr><td>WB MAINLINE @ IL ROUTE 75</td><td></td><td></td><td></td><td>2</td></tr> <tr><td>EB MAINLINE @ PECATONICA</td><td></td><td></td><td></td><td>2</td></tr> <tr><td>EB MAINLINE @ HENDERSON ROAD</td><td></td><td></td><td></td><td>1</td></tr> <tr><td>WB MAINLINE @ HENDERSON ROAD</td><td></td><td></td><td></td><td>1</td></tr> <tr><td>EB MAINLINE @ BOX CULVERT EXTENSION</td><td></td><td></td><td></td><td>1</td></tr> <tr><td>WB MAINLINE @ TOWER ROAD</td><td></td><td></td><td></td><td>1</td></tr> <tr><td>EB MAINLINE @ RIVER ROAD</td><td></td><td></td><td></td><td>1</td></tr> <tr><td>WB MAINLINE @ RIVER ROAD</td><td></td><td></td><td></td><td>1</td></tr> <tr><td colspan="4" style="text-align: right;">SUB-TOTAL =</td><td>12</td></tr> <tr><td colspan="5"> </td></tr> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>WB MAINLINE @ PECATONICA</td><td></td><td></td><td></td><td>2</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>14</td></tr> <tr><td colspan="5"> </td></tr> <p>78300100 PAVEMENT MARKING REMOVAL</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>OFFSET</th> <th>SQ FT</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 EB (HDR SECTION)</td></tr> <tr><td>STA. 1690+00 TO STA. 1702+00</td><td></td><td></td><td></td><td>400</td></tr> <tr><td>STA. 1690+00 TO STA. 1701+72</td><td></td><td></td><td></td><td>391</td></tr> <tr><td>STA. 1701+72 TO STA. 1702+00</td><td></td><td></td><td></td><td>2</td></tr> <tr><td>STA. 1710+00 TO STA. 1796+50</td><td></td><td></td><td></td><td>2883</td></tr> <tr><td>STA. 1710+00 TO STA. 1720+43</td><td></td><td></td><td></td><td>348</td></tr> <tr><td>STA. 1710+00 TO STA. 1720+43</td><td></td><td></td><td></td><td>348</td></tr> <tr><td>STA. 1720+43 TO STA. 1789+09</td><td></td><td></td><td></td><td>572</td></tr> <tr><td>STA. 1789+09 TO STA. 1796+59</td><td></td><td></td><td></td><td>250</td></tr> <tr><td>STA. 1789+09 TO STA. 1796+59</td><td></td><td></td><td></td><td>63</td></tr> <tr><td>STA. 1796+59 TO STA. 1804+00</td><td></td><td></td><td></td><td>840</td></tr> <tr><td>STA. 1797+00 TO STA. 1802+39</td><td></td><td></td><td></td><td>355</td></tr> <tr><td>STA. 1802+39 TO STA. 1803+39</td><td></td><td></td><td></td><td>67</td></tr> <tr><td>STA. 1804+00 TO STA. 1811+41</td><td></td><td></td><td></td><td>840</td></tr> <tr><td>STA. 1804+61 TO STA. 1805+61</td><td></td><td></td><td></td><td>67</td></tr> <tr><td>STA. 1805+61 TO STA. 1810+50</td><td></td><td></td><td></td><td>355</td></tr> <tr><td>STA. 1811+41 TO STA. 1818+91</td><td></td><td></td><td></td><td>250</td></tr> <tr><td>STA. 1811+41 TO STA. 1818+91</td><td></td><td></td><td></td><td>63</td></tr> <tr><td>STA. 1812+75 TO STA. 1842+35</td><td></td><td></td><td></td><td>987</td></tr> <tr><td>STA. 1818+91 TO STA. 1841+27</td><td></td><td></td><td></td><td>186</td></tr> <tr><td>STA. 1841+27 TO STA. 1842+35</td><td></td><td></td><td></td><td>36</td></tr> <tr><td>STA. 1841+27 TO STA. 1842+35</td><td></td><td></td><td></td><td>9</td></tr> <tr><td>STA. 1842+35 TO STA. 1844+22</td><td></td><td></td><td></td><td>62</td></tr> <tr><td>STA. 1842+35 TO STA. 1844+22</td><td></td><td></td><td></td><td>125</td></tr> <tr><td>STA. 1844+22 TO STA. 1848+15</td><td></td><td></td><td></td><td>131</td></tr> <tr><td>STA. 1844+22 TO STA. 1848+15</td><td></td><td></td><td></td><td>131</td></tr> <tr><td>STA. 1844+22 TO STA. 1848+15</td><td></td><td></td><td></td><td>33</td></tr> <tr><td>STA. 1845+76.97 TO STA. 1904+43.14</td><td></td><td></td><td></td><td>1289</td></tr> <tr><td>STA. 1865+76.97 TO STA. 1904+43.14</td><td></td><td></td><td></td><td>2578</td></tr> <tr><td>STA. 1904+43.14 TO STA. 1904+73.14</td><td></td><td></td><td></td><td>10</td></tr> <tr><td>STA. 1904+43.14 TO STA. 1904+73.14</td><td></td><td></td><td></td><td>20</td></tr> <tr><td colspan="4" style="text-align: right;">SUB-TOTAL =</td><td>13691</td></tr> <tr><td colspan="5"> </td></tr> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>1914+41.97</td><td>1993+00</td><td></td><td>6522</td><td></td></tr> <tr><td colspan="4" style="text-align: right;">SUB-TOTAL =</td><td>6522</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>20213</td></tr> <tr><td colspan="5"> </td></tr> <p>78300200 REMOVE RAISED REFLECTIVE PAVEMENT MARKERS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>OFFSET</th> <th>EACH</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>1690+59.34</td><td>2 x 80'</td><td>1842+29.38</td><td>380</td><td></td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>380</td></tr> </tbody> </table> </tbody></table></tbody></table>	STATION	WIDTH(FT)	STATION	OFFSET	EACH	US ROUTE 20 (HDR SECTION)					EB MAINLINE @ IL ROUTE 75				2	WB MAINLINE @ IL ROUTE 75				2	EB MAINLINE @ PECATONICA				2	EB MAINLINE @ HENDERSON ROAD				1	WB MAINLINE @ HENDERSON ROAD				1	EB MAINLINE @ BOX CULVERT EXTENSION				1	WB MAINLINE @ TOWER ROAD				1	EB MAINLINE @ RIVER ROAD				1	WB MAINLINE @ RIVER ROAD				1	SUB-TOTAL =				12						US ROUTE 20 (V3 SECTION)					WB MAINLINE @ PECATONICA				2	TOTAL =				14						STATION	WIDTH(FT)	STATION	OFFSET	SQ FT	US ROUTE 20 EB (HDR SECTION)					STA. 1690+00 TO STA. 1702+00				400	STA. 1690+00 TO STA. 1701+72				391	STA. 1701+72 TO STA. 1702+00				2	STA. 1710+00 TO STA. 1796+50				2883	STA. 1710+00 TO STA. 1720+43				348	STA. 1710+00 TO STA. 1720+43				348	STA. 1720+43 TO STA. 1789+09				572	STA. 1789+09 TO STA. 1796+59				250	STA. 1789+09 TO STA. 1796+59				63	STA. 1796+59 TO STA. 1804+00				840	STA. 1797+00 TO STA. 1802+39				355	STA. 1802+39 TO STA. 1803+39				67	STA. 1804+00 TO STA. 1811+41				840	STA. 1804+61 TO STA. 1805+61				67	STA. 1805+61 TO STA. 1810+50				355	STA. 1811+41 TO STA. 1818+91				250	STA. 1811+41 TO STA. 1818+91				63	STA. 1812+75 TO STA. 1842+35				987	STA. 1818+91 TO STA. 1841+27				186	STA. 1841+27 TO STA. 1842+35				36	STA. 1841+27 TO STA. 1842+35				9	STA. 1842+35 TO STA. 1844+22				62	STA. 1842+35 TO STA. 1844+22				125	STA. 1844+22 TO STA. 1848+15				131	STA. 1844+22 TO STA. 1848+15				131	STA. 1844+22 TO STA. 1848+15				33	STA. 1845+76.97 TO STA. 1904+43.14				1289	STA. 1865+76.97 TO STA. 1904+43.14				2578	STA. 1904+43.14 TO STA. 1904+73.14				10	STA. 1904+43.14 TO STA. 1904+73.14				20	SUB-TOTAL =				13691						US ROUTE 20 (V3 SECTION)					1914+41.97	1993+00		6522		SUB-TOTAL =				6522	TOTAL =				20213						STATION	WIDTH(FT)	STATION	OFFSET	EACH	US ROUTE 20 (V3 SECTION)					1690+59.34	2 x 80'	1842+29.38	380		TOTAL =				380	<p>542A0217 PIPE CULVERTS, CLASS A, TYPE 1 12"</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>FOOT</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 (HDR SECTION)</td></tr> <tr><td>RAMP BC STA. 106+56</td><td></td><td></td><td></td><td>16</td></tr> <tr><td>RAMP DB STA. 216+40</td><td></td><td></td><td></td><td>32</td></tr> <tr><td>RAMP DB STA. 211+00</td><td></td><td></td><td></td><td>26</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>74</td></tr> <tr><td colspan="5"> </td></tr> <p>542A0229 PIPE CULVERTS, CLASS A, TYPE 1 24"</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>FOOT</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 (HDR SECTION)</td></tr> <tr><td>STA. 1725+68.50</td><td></td><td></td><td></td><td>10</td></tr> <tr><td>STA. 1726+50.00</td><td></td><td></td><td></td><td>158</td></tr> <tr><td>STA. 1727+31.50</td><td></td><td></td><td></td><td>10</td></tr> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>STA. 1981+50.0</td><td></td><td></td><td></td><td>103</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>281</td></tr> <tr><td colspan="5"> </td></tr> <p>542A1057 PIPE CULVERTS, CLASS A, TYPE 2 12"</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>FOOT</th> </tr> </thead> <tbody> <tr><td colspan="5">RAMP AD (HDR SECTION)</td></tr> <tr><td>STA. 304+00.0</td><td></td><td></td><td></td><td>40</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>40</td></tr> <tr><td colspan="5"> </td></tr> <p>542D0220 PIPE CULVERTS, CLASS D, TYPE 1 15"</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>FOOT</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td colspan="5">CROSS OVER, SEE MEDIAN CROSS OVER SHEET FOR DETAILS</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>250</td></tr> <tr><td colspan="5"> </td></tr> <tr><td colspan="5">MEDIAN CROSS-OVER (HDR SECTION)</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>500</td></tr> <tr><td colspan="5"> </td></tr> <p>X0322054 REMOVAL OF PRECAST FLARED END SECTION</p> <table border="1" style="width: 100%; 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BIT. CONC. SURF. CSE., SUPERPAVE, MIX "D", N90</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>TONS</th> </tr> </thead> <tbody> <tr><td colspan="5">112 LBS/YIN</td></tr> <tr><td colspan="5">1.5" THICKNESS</td></tr> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>1690+59.34</td><td>24.0</td><td>1702+00.0</td><td>24.0</td><td>255.5</td></tr> <tr><td>1702+00.0</td><td>12.0</td><td>1714+00.0</td><td>12.0</td><td>134.4</td></tr> <tr><td>1714+00.0</td><td>24.0</td><td>1842+24.38</td><td>24.0</td><td>2,873.8</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>3,264</td></tr> <tr><td colspan="5"> </td></tr> <p>X4066915 POLY. BIT. CONC. LEVEL BIND. (MACHINE METHOD), SUPERPAVE, N90</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>TONS</th> </tr> </thead> <tbody> <tr><td colspan="5">112 LBS/YIN</td></tr> <tr><td colspan="5">.75" THICKNESS</td></tr> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>1690+59.3</td><td>24.0</td><td>1702+00.0</td><td>24.0</td><td>170.4</td></tr> <tr><td>1702+00.0</td><td>12.0</td><td>1714+00.0</td><td>12.0</td><td>89.6</td></tr> <tr><td>1714+00.0</td><td>24.0</td><td>1796+50.0</td><td>24.0</td><td>1232</td></tr> <tr><td>1796+50.0</td><td>24.0</td><td>1812+75.0</td><td>24.0</td><td>242.7</td></tr> <tr><td>1812+75.0</td><td>24.0</td><td>1825+22.5</td><td>24.0</td><td>186.3</td></tr> <tr><td>1825+22.5</td><td>24.0</td><td>1831+72.3</td><td>24.0</td><td>97.1</td></tr> <tr><td>1831+72.3</td><td>24.0</td><td>1842+29.4</td><td>24.0</td><td>157.9</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>2,176</td></tr> </tbody> </table> </tbody></table></tbody></table></tbody></table></tbody></table>	STATION	WIDTH(FT)	STATION	WIDTH(FT)	EACH	US ROUTE 20 (V3 SECTION)					CROSS OVER, SEE MEDIAN CROSS OVER SHEET FOR DETAILS					SUB-TOTAL =				2						US ROUTE 20 (HDR SECTION)					MEDIAN CROSS-OVER					SUB-TOTAL =				2	TOTAL =				4						STATION	WIDTH(FT)	STATION	WIDTH(FT)	L SUM	US ROUTE 20 (HDR SECTION)					STA. 1853+58				1	TOTAL =				1						STATION	WIDTH(FT)	STATION	OFFSET	SQ YD	US ROUTE 20 - STAGE I, PHASE 2 (HDR SECTION)					STA. 1703+00 TO STA. 1706+00	RT			200	TOTAL US ROUTE 20				200						TEMPORARY RAMP					STA. 500+33 TO STA. 508+62.85				1431	CONNECTOR AT RAMP DB STA. 211				280	TOTAL TEMPORARY RAMP				1711	TOTAL				1911						STATION	WIDTH(FT)	STATION	WIDTH(FT)	TONS	112 LBS/YIN					1.5" THICKNESS					US ROUTE 20 (V3 SECTION)					1690+59.34	24.0	1702+00.0	24.0	255.5	1702+00.0	12.0	1714+00.0	12.0	134.4	1714+00.0	24.0	1842+24.38	24.0	2,873.8	TOTAL =				3,264						STATION	WIDTH(FT)	STATION	WIDTH(FT)	TONS	112 LBS/YIN					.75" THICKNESS					US ROUTE 20 (V3 SECTION)					1690+59.3	24.0	1702+00.0	24.0	170.4	1702+00.0	12.0	1714+00.0	12.0	89.6	1714+00.0	24.0	1796+50.0	24.0	1232	1796+50.0	24.0	1812+75.0	24.0	242.7	1812+75.0	24.0	1825+22.5	24.0	186.3	1825+22.5	24.0	1831+72.3	24.0	97.1	1831+72.3	24.0	1842+29.4	24.0	157.9	TOTAL =				2,176	<p>X4409410 BITUMINOUS SURFACE REMOVAL, 2"</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>SQ YD</th> </tr> </thead> <tbody> <tr><td colspan="5">US ROUTE 20 (V3 SECTION)</td></tr> <tr><td>TOWER RD @ 1779+21.98</td><td>44.0</td><td></td><td></td><td>1,149.7</td></tr> <tr><td>HENDERSON RD @ 1726+45.15</td><td>44.0</td><td></td><td></td><td>1,159.5</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>2,310.0</td></tr> <tr><td colspan="5"> </td></tr> <p>X6064201 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.06</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>STATION</th> <th>WIDTH(FT)</th> <th>STATION</th> <th>WIDTH(FT)</th> <th>LF</th> </tr> </thead> <tbody> <tr><td colspan="5">IL ROUTE 75 (HDR SECTION)</td></tr> <tr><td>STA. 32+00.00 TO STA. 34+00.00</td><td></td><td></td><td></td><td>400</td></tr> <tr><td colspan="4" style="text-align: right;">TOTAL =</td><td>400</td></tr> <tr><td colspan="5"> </td></tr> <p>X6065701 CONCRETE MEDIAN, TYPE SM-4.06</p> <table border="1" style="width: 100%; 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1702+00.0	12.0	1714+00.0	12.0	134.4																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
1714+00.0	24.0	1842+24.38	24.0	2,873.8																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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1714+00.0	24.0	1796+50.0	24.0	1232																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
1796+50.0	24.0	1812+75.0	24.0	242.7																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
1812+75.0	24.0	1825+22.5	24.0	186.3																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
1825+22.5	24.0	1831+72.3	24.0	97.1																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
1831+72.3	24.0	1842+29.4	24.0	157.9																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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HENDERSON RD @ 1726+45.15	44.0			1,159.5																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

US ROUTE 20

SCALE: VERT. _____
 HORIZ. _____
 DATE 10/28/05

DRAWN BY DTE
 CHECKED BY JAM

SCHEDULE OF QUANTITIES

Z0030150 IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)					
1726+15					1
1726+75					1
1886+00					1
1886+60					1
=====					TOTAL = 4

Z0030250 IMPACT ATTENUATORS (TEMPORARY) NON REDIRECTIVE, TEST LEVEL 3

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)					
EB	STAGE I CROSSOVER - STANDARD		701402		1
WB	STAGE I CROSSOVER - STANDARD		701403		1
=====					TOTAL = 2

Z0030350 IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	EACH
US ROUTE 20 (HDR SECTION)					
1704+50	EB		STAGE II		1
1779+50	MEDIAN		PERMANENT ATTENUATOR		1
1853+58	EB		STAGE II		1
1846+23	EB	RT	STAGE III		1
1846+23	EB	LT	STAGE IV		1
=====					TOTAL = 5

64200105 SHOULDER RUMBLE STRIP

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT
US ROUTE 20 (V3 SECTION)					
1914+41.0	WB x 2	-	1982+00.0		13,518.0
1964+60.0	EB	-	1977+05.0		1,245.0
=====					SUB-TOTAL = 14,763.0

US ROUTE 20 (HDR SECTION)

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT
1697+00		-	1829+15	LT. OUTSIDE	13215
1702+15		-	1842+10	LT. INSIDE	13995
1702+50		-	1710+00	RT. INSIDE	750
1796+50		-	1812+75	RT. INSIDE	1625
1796+50		-	1812+75	RT. OUTSIDE	1625
1832+33		-	1842+10	LT. OUTSIDE	977
1844+49		-	1855+34	LT. OUTSIDE	1085
1844+49		-	1904+41	LT. INSIDE	5992
1848+15		-	1865+77	RT. INSIDE	1762
1848+15		-	1855+07	RT. OUTSIDE	692
1861+84		-	1904+41	LT. OUTSIDE	4257
1858+54		-	1883+62	RT. OUTSIDE	2508
=====					SUB-TOTAL = 48,483.0
=====					TOTAL = 63,246.0

Z0065745 SLOTTED DRAIN 12" WITH 2 1/2" SLOT

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT
RAMP CA (HDR SECTION)					
411+90.00	TO	412+95.45	LT		821.33
412+95.45	TO	414+91.00	LT		820.38
=====					TOTAL RAMP CA = 298

RAMP AD

304+00.00	TO	306+58.50	LT		822.04
=====					TOTAL RAMP AD = 257

RAMP BC

104+00.00	TO	106+47.00	LT		826.82
=====					TOTAL RAMP BC = 245

RAMP DB

213+05.00	TO	214+43.12	LT		826.03
214+43.12	TO	216+40.00	LT		823.33
=====					TOTAL RAMP DB = 332
=====					TOTAL = 1132

Z0065752 SLOTTED DRAIN 12" WITH 6" SLOT

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	FOOT
US ROUTE 20 (V3 SECTION)					
CROSS OVER, SEE MEDIAN CROSS OVER SHEET FOR DETAILS					
=====					126.0
=====					SUB-TOTAL = 126.0

US ROUTE 20 (HDR SECTION)

MEDIAN CROSS-OVER					126
=====					SUB-TOTAL = 126
=====					TOTAL = 252

Z0074100 TERMINATION OF DEADEND ROADS

STATION	WIDTH(FT)	-	STATION	WIDTH(FT)	EACH
SMOKEY HOLLOW CUL-DE-SAC (V3 SECTION)					
=====					1
=====					TOTAL = 1

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		SCHEDULE OF QUANTITIES US ROUTE 20

SCALE: VERT. HORIZ.
DATE: 10/28/05

DRAWN BY: DTE
CHECKED BY: JAM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	37
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
* 177-3,4,4-1-1				

V3 SCHEDULE

Station	Earth Excavation Cu Yd	Earth Exc Adjusted for Shrinkage Cu	Embankment Cu Yd	Earthwork Balance Waste (+) or Shortage (-)
1915+00.00	66	49	18	32
1916+00.00	66	49	18	32
1917+00.00	66	49	18	32
1918+00.00	66	49	18	32
1919+00.00	66	49	18	32
1920+00.00	66	49	18	32
1921+00.00	66	49	18	32
1922+00.00	66	49	18	32
1923+00.00	66	49	18	32
1924+00.00	66	49	18	32
1925+00.00	66	50	18	32
1926+00.00	66	50	18	32
1927+00.00	67	50	18	32
1928+00.00	66	50	18	32
1929+00.00	68	51	23	28
1930+00.00	67	50	18	33
1931+00.00	67	50	18	32
1932+00.00	67	50	19	31
1933+00.00	67	50	19	31
1934+00.00	67	50	19	31
1935+00.00	67	50	18	32
1936+00.00	67	50	19	31
1937+00.00	67	50	19	31
1938+00.00	67	50	18	32
1939+00.00	66	50	18	32
1940+00.00	68	51	24	27
1941+00.00	67	50	18	33
1942+00.00	66	50	18	32
1943+00.00	68	51	22	30
1944+00.00	70	53	23	29
1945+00.00	72	54	24	30
1946+00.00	72	54	24	30
1947+00.00	72	54	23	31
1948+00.00	94	71	21	50
1949+00.00	72	54	23	31
1950+00.00	72	54	24	30
1951+00.00	72	54	25	29
1952+00.00	73	54	27	28
1953+00.00	73	54	27	27
1954+00.00	73	54	26	28
1955+00.00	72	54	26	29
1956+00.00	73	54	27	28
1957+00.00	73	54	24	30
1958+00.00	73	54	27	28
1959+00.00	73	54	25	29
1960+00.00	73	54	25	30
1961+00.00	72	54	25	29
1962+00.00	72	54	26	29
1963+00.00	72	54	24	30
1964+00.00	73	54	25	29
1965+00.00	72	54	109	-55
1966+00.00	158	118	37	82
1967+00.00	124	93	78	15
1968+00.00	229	172	25	146
1969+00.00	239	179	24	156
1970+00.00	210	158	25	133
1971+00.00	219	164	15	149
1972+00.00	275	206	9	197
1973+00.00	476	357	9	348

Station	Earth Excavation Cu Yd	Earth Exc Adjusted for Shrinkage Cu	Embankment Cu Yd	Earthwork Balance Waste (+) or Shortage (-)
1974+00.00	401	301	9	292
1975+00.00	445	334	0	334
1976+00.00	375	282	0	282
1977+00.00	345	259	0	259
1978+00.00	244	183	330	-147
1979+00.00	231	173	0	173
1980+00.00	261	196	3	192
1981+00.00	231	173	66	107
1982+00.00	260	195	79	116
1983+00.00	117	88	46	42
1984+00.00	120	90	108	-18
1985+00.00	85	64	32	32
1986+00.00	76	57	8	49
1987+00.00	94	70	19	51
SMOKEY HOLLOW	510	383		383
Total	9265	6949	2084	4865

Earth Excavation: 9265 CU YD V3 SUB-TOTAL
 *TOPSOIL FURNISH AND PLACE, 6" 21780 SQ YD V3 SUB-TOTAL
 *SEE SEEDING CLASS 6 (MODIFIED) FOR AREA

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		EARTHWORK SCHEDULE US ROUTE 20

SCALE: VERT. N/A
 HORIZ. N/A
 DATE 10/28/05

DRAWN BY DTE
 CHECKED BY JAM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	38
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
177-(3,4,4-1)-1				

LOCATION	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
U. S. ROUTE 20					
1691+00 TO 1695+00	420	315	39	276	344
1695+00 TO 1700+00	1378	1033	141	893	1578
1700+00 TO 1705+00	4802	3601	124	3477	4567
1705+00 TO 1710+00	3269	2451	944	1507	4078
1710+00 TO 1715+00	987	740	207	533	1767
1715+00 TO 1720+00	380	285	102	183	678
1720+00 TO 1725+00	474	356	102	254	878
1725+00 TO 1730+00	402	301	259	42	1322
1730+00 TO 1735+00	333	250	148	102	889
1735+00 TO 1740+00	333	250	148	102	889
1740+00 TO 1745+00	330	247	148	99	844
1745+00 TO 1750+00	333	250	148	102	889
1750+00 TO 1755+00	333	250	148	102	889
1755+00 TO 1760+00	333	250	159	91	978
1760+00 TO 1765+00	333	250	148	102	889
1765+00 TO 1770+00	333	250	148	102	889
1770+00 TO 1775+00	333	250	191	59	1044
1775+00 TO 1780+00	313	235	243	-8	789
1780+00 TO 1785+00	470	353	230	123	1089
1785+00 TO 1790+00	426	319	222	97	1222
1790+00 TO 1795+00	439	329	248	81	1300
1795+00 TO 1800+00	950	713	200	513	1933
1800+00 TO 1805+00	1219	914	663	251	2133
1805+00 TO 1810+00	1376	1032	206	826	1833
1810+00 TO 1815+00	1006	754	170	584	1000
1815+00 TO 1820+00	433	325	276	49	922
1820+00 TO 1825+00	372	279	243	37	822
1825+00 TO 1830+00	233	175	93	82	222
1830+00 TO 1835+00	304	228	128	100	500
1835+00 TO 1840+00	328	246	194	51	1144
1840+00 TO 1842+95.95	356	267	53	214	375
1844+19.45 TO 1845+00	25	19	9	10	152
1845+00 TO 1850+00	944	708	111	597	1022
1850+00 TO 1855+00	1793	1344	111	1233	1400
1855+00 TO 1860+00	1669	1251	172	1079	1333
1860+00 TO 1865+00	1591	1193	206	988	1911
1865+00 TO 1870+00	576	432	243	189	1367
1870+00 TO 1875+00	333	250	278	-28	1333
1875+00 TO 1880+00	333	250	278	-28	1333
1880+00 TO 1885+00	268	201	197	3	967
1885+00 TO 1890+00	350	263	285	-23	1778
1890+00 TO 1895+00	315	236	204	32	1111
1895+00 TO 1900+00	315	236	204	32	1111
1900+00 TO 1904+70.31	541	405	162	244	977
U. S. ROUTE 20 SUBTOTAL					
	32384	24288	8932	15356	54493
SHRINKAGE FACTOR	25%				

LOCATION	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
RAMP BC					
100+00 TO 101+00	4	3	35	-32	156
101+00 TO 103+00	7	6	89	-83	267
103+00 TO 105+00	83	63	74	-12	189
105+00 TO 107+00	93	69	44	25	122
107+00 TO 109+00	89	67	81	-15	256
109+00 TO 111+00	89	67	104	-37	222
111+00 TO 113+00	89	67	91	-24	211
113+00 TO 115+00	89	67	76	-9	178
115+00 TO 117+00	89	67	74	-7	178
117+00 TO 118+38.15	53	40	44	-4	106
RAMP BC SUBTOTAL					
	684	513	713	-199	1884
SHRINKAGE FACTOR	25%				

LOCATION	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
RAMP AD					
300+00 TO 302+00	28	21	116	-95	189
302+00 TO 304+00	45	34	102	-68	178
304+00 TO 306+00	82	61	62	-1	122
306+00 TO 308+00	94	70	90	-20	256
308+00 TO 310+00	72	54	150	-96	256
310+00 TO 312+00	77	58	125	-67	222
312+00 TO 314+00	88	66	79	-13	189
314+00 TO 316+00	89	67	74	-7	178
316+00 TO 318+29.95	96	72	80	-8	191
RAMP AD SUBTOTAL					
	669	502	877	-375	1780
SHRINKAGE FACTOR	25%				

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
EARTHWORK SCHEDULE
US ROUTE 20
 SCALE: VERT. N/A
 HORIZ. N/A
 DATE 10/28/05
 DRAWN BY DTE
 CHECKED BY JAM

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	39
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
* 177-13,4,4-D-1				

LOCATION	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
RAMP CA					
401+71.47 TO 403+00	54	40	42	-2	90
403+00 TO 405+00	87	65	74	-9	167
405+00 TO 407+00	89	67	81	-15	211
407+00 TO 409+00	89	67	100	-33	222
409+00 TO 411+00	87	65	104	-38	211
411+00 TO 413+00	98	74	85	-12	178
413+00 TO 415+00	87	65	69	-3	178
415+00 TO 417+00	74	56	67	-11	178
417+00 TO 419+00	65	49	67	-18	178
419+00 TO 420+00	30	22	33	-11	89
RAMP CA SUBTOTAL	759	569	722	-153	1702
SHRINKAGE FACTOR	25%				

LOCATION	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
TEMPORARY RAMP					
500+32 TO 502+00	2713	2035	930	1105	0
502+00 TO 503+00	69	51	1139	-1088	0
503+00 TO 504+00	559	419	241	179	0
504+00 TO 505+00	1317	988	98	889	0
505+00 TO 506+00	1659	1244	67	1178	0
506+00 TO 507+58	1032	774	23	751	0
TEMPORARY RAMP SUBTOTAL	7349	5512	2497	3014	0
SHRINKAGE FACTOR	25%				

LOCATION	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
RAMP DB					
201+71.47 TO 203+00	507	380	233	147	643
203+00 TO 205+00	1711	1283	1669	-385	2411
205+00 TO 207+00	1324	993	7154	-6161	2967
207+00 TO 209+00	35	26	9237	-9211	2289
209+00 TO 211+00	670	503	2987	-2484	1356
211+00 TO 213+00	1070	803	19	784	778
213+00 TO 215+00	183	138	63	75	211
215+00 TO 217+00	110	83	59	23	167
217+00 TO 219+00	91	68	59	9	222
219+00 TO 221+00	81	61	59	2	222
221+00 TO 223+00	81	61	59	2	222
223+00 TO 225+00	94	71	61	10	222
225+00 TO 227+00	78	58	67	-8	222
227+00 TO 229+00	59	44	67	-22	222
229+00 TO 230+00	30	22	33	-11	111
230+00 TO 232+00	59	44	67	-22	222
232+00 TO 234+00	59	44	67	-22	222
234+00 TO 236+00	59	44	67	-22	222
236+00 TO 238+00	57	43	67	-24	211
238+00 TO 240+00	44	33	67	-33	178
240+00 TO 241+03.12	19	14	34	-20	90
RAMP DB SUBTOTAL	6425	4818	22193	-17374	13411
SHRINKAGE FACTOR	25%				

	20200100 EARTH EXCAVATION (CU YD)	EXCAVATION ADJUSTED FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	21101625 TOPSOIL FURNISH AND PLACE, 6" (SQ YD)
HDR	TEMPORARY RAMP SUBTOTAL*	7349	5512	2497	3014
	U. S. ROUTE 20 SUBTOTAL	32384	24288	8932	15356
	RAMP BC SUBTOTAL	684	513	713	-199
	RAMP AD SUBTOTAL	669	502	877	-375
	RAMP DB SUBTOTAL	6425	4818	22193	-17374
	RAMP CA SUBTOTAL	759	569	722	-153
V3	U. S. ROUTE 20 SUBTOTAL	9265	6949	2084	4865
	Totals	57535	43151	38018	5134

*THE TEMPORARY RAMP SHALL BE REMOVED AS ONE OF THE LAST ITEMS OF CONSTRUCTION.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		EARTHWORK SCHEDULE US ROUTE 20 SCALE: VERT. N/A HORIZ. N/A DATE 10/28/05

DRAWN BY DTE
CHECKED BY JAM

STATION	TO	STATION	LENGTH (FEET)	PROPOSED SURFACE		X4066414	#2001371	48202400	40800020	40800030
				WIDTH (FEET)	AREA (SQ YD)	BIT CONC SURF CSE	POLY BIT CONC PYMT (FULL DEPTH), SUPERPAVE, 14"	BITUMINOUS SHOULDERS, SUPERPAVE 6"	BITUMINOUS PRIME COAT	AGGREGATE PRIME COAT
EAST BOUND										
1702+00.00	-	1702+15.51	15.51	12	20.68		21			
1702+15.51	-	1710+00.00	784.49	12	1045.99	98	1046	872		
1796+50.00	-	1812+75.00	-	-	-	405		3611		
1848+28.89	-	1865+76.97	-	-	-	302		2695		
WEST BOUND										
1697+00.00	-	1702+15.51	515.51	24	1374.69	64	1375	573		
1702+15.51	-	1714+00.00	1184.49	24	3158.64	236	3159	2106		
1714+00.00	-	1796+50.00	8250.00	24	22000.00	1643	22000	14667		
1796+50.00	-	1812+75.00	1625.00	24	4333.33	218	4333	1943		
1812+75.00	-	1841+09.95	2834.95	24	7559.87	321	7560	3031		
1845+49.35	-	1848+15.00	265.65	24	708.40	60	708	1079		
1848+15.00	-	1865+76.97	1761.97	24	4698.59	270	4699	2311		
1865+76.97	-	1903+40.31	3763.34	24	10035.57	750	10036	6858		
RAMP AD										
300+00.00	-	303+56.76	-	-	415.31	27	415	238	0.10	0.98
303+56.76	-	306+64.28	307.52	16	546.70	71	547	616	0.18	1.74
306+64.28	-	318+29.95	1165.67	16	2072.30	145	2072	1296	0.53	5.05
RAMP BC										
100+00.00	-	103+00.45	-	-	295.54	24	296	210		
103+00.45	-	104+00.88	100.43	16	178.54	8	179	67		
104+00.88	-	106+63.60	262.72	16	467.06	70	467	583		
106+63.60	-	118+38.15	1174.55	16	2088.09	146	2088	1306		
RAMP CA										
401+71.47	-	402+56.47	-	-	174.59	11	175	95		
402+56.47	-	411+86.34	929.87	16	1653.10	116	1653	1034		
411+86.34	-	414+95.45	309.11	16	549.53	54	550	478		
414+95.45	-	416+98.52	203.07	16	361.01	15	361	134		
416+98.52	-	419+05.45	-	-	322.33	15	322	138		
419+05.45	-	434+05.45	1500.00	12	2000.00	112	2000	1000		
434+05.45	-	439+55.56	-	-	397.22	263	397	392		
RAMP DB										
201+71.47	-	202+56.47	-	-	174.72	11	175	95		
202+56.47	-	210+50.00	793.53	16	1410.72	99	1411	882		
210+50.00	-	213+02.43	252.43	16	448.76	31	449	281		
213+02.43	-	216+43.12	340.69	16	605.67	60	606	539		
216+43.12	-	217+78.67	135.55	16	240.98	10	241	91		
217+78.67	-	220+53.12	-	-	407.51	21	408	183		
220+53.12	-	235+53.12	1500.00	12	2000.00	112	2000	1000		
235+53.12	-	241+03.12	-	-	475.76	43	476	379		
HDR SUBTOTALS						5831	72223	50783	0.8	8
RECON SUBTOTAL										
1914+71	-	1982+00	6729	10	7512					
1914+71	-	1982+00	6729	6	4486					
1964+60	-	1969+50	490	10	545					
1969+50	-	1971+00	150	8	134					
1971+00	-	1977+05	605	6	404					
SMOKEY HOLLOW ROAD										
1690+59.34	-	1714+00	2340.66	10	2600.8					
1714+00	-	1796+50	8250	20	165000					
1796+50	-	1812+75	1625	0	0					
1812+75	-	1831+72.26	1897.26	10	2108.1					
1831+72.26	-	1842+29.38	1057.12	20	2349.2					

STATION	TO	STATION	LENGTH (FEET)	PROPOSED SURFACE		X4066414	#2001371	48202400	40800020	40800030
				WIDTH (FEET)	AREA (SQ YD)	BIT CONC SURF CSE	POLY BIT CONC PYMT (FULL DEPTH), SUPERPAVE, 14"	BITUMINOUS SHOULDERS, SUPERPAVE 6"	BITUMINOUS PRIME COAT	AGGREGATE PRIME COAT
RESURFACING SUBTOTAL										
						38437	3200			
1914+71.97	-	1982+00	6729	24	17942					
RECON SUBTOTAL										
						17942	17942			
1914+71.97	-	1982+00	6729	2	1496					
1965+60	-	1977+05	1145	2	255					
1980+05	-	1985+59.98	554.98	6	370					
1980+05	-	1985+59.98	554.98	6	370					
SUBTOTAL						2491				
SMOKEY HOLLOW ROAD										
1914+71	-	1982+00	6729	10	7786					
1914+71	-	1982+00	6729	6	4486					
1964+60	-	1969+50	490	10	545					
1969+50	-	1971+00	150	8	134					
1971+00	-	1977+05	605	6	404					
SUBTOTAL						13355		12272		
BITUMINOUS FULL DEPTH PAVT						17942				
WB SHOULDER						12016				
EB SHOULDER						1384				
RESURFACE AREA						65631				
SUBTOTAL						162684		22.9		
BITUMINOUS FULL DEPTH PAVT						17942				
WB SHOULDER						12016				
EB SHOULDER						1384				
RESURFACE AREA						65631				
SUBTOTAL						162684				217
V3 SUBTOTALS						4760	17942	12272	22.9	217
TOTAL						10740	90164.7	64381	23.7	224.8

STATION	TO	STATION	PROPOSED SURFACE AREA (SQ YD)	Surface Area TY1 (SQ YD)	X4066414 BIT CONC SURF CSE MIX C N50 (TON)	48202400 BITUMINOUS SHOULDERS, SUPERPAVE 6" (SQ YD)
EAST BOUND						
1745+74.06	-	1750+49.06	184.72	59.00	27.31	244
1839+53.49	-	1842+52.79	116.39	59.00	19.65	175
1839+52.45	-	1842+52.45	116.67	59.00	19.68	176
1902+06.52	-	1905+06.52	116.67	59.00	19.68	176
1902+06.66	-	1905+06.66	116.67	59.00	19.68	176
WEST BOUND						
1844+19.45	-	1847+24.45	118.61	59.00	19.90	178
1844+19.45	-	1847+86.95	142.92	59.00	22.62	202
TOTALS						
			148.54	1326		

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION PAVING SCHEDULE US ROUTE 20 SCALE: VERT. N/A HORIZ. N/A DATE 10/28/05
NAME	DATE	
DRAWN BY: PSL		CHECKED BY: JAM

CONTRACT 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	41A
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	
* 177-13,4,4-1-1				

LOCATION	ESTBOUND OR EASTBOUND	MEDIAN OR OUTSIDE	60107600 PIPE UNDERDRAINS 4" FOOT	60108100 PFE UNDERDRAINS 4" (SPECIAL) FOOT	60100060 CONC. HDWL. FOR PIPE DRAINS EACH	REMARKS
BEGIN STA.	END STA.					
232+00.00	237+00.00	OUT	500	34	1	
237+00.00	241+05.00	OUT	405	34	1	
TOTAL RAMP DB			3933	340	10	
TOTALS			45822	3648	116	

US ROUTE 20	WESTBOUND OR EASTBOUND	MEDIAN OR OUTSIDE	60107600 PIPE UNDERDRAINS 4" FOOT	60108100 PIPE UNDERDRAINS 4" (SPECIAL) FOOT	60100060 CONC. HDWL. FOR PIPE DRAINS EACH
1914+50.00 - 1942+50.00	WB	OUT	2800	164.5	7
1914+50.00 - 1942+50.00	WB	MEDIAN	2800	136.5	7
1942+50.00 - 1969+41.00	WB	OUT	2691	141	6
1969+41.00 - 1982+00.00	WB	OUT	1259	70.5	3
1969+41.00 - 1982+00.00	WB	MEDIAN	1259	58.5	3
1964+60.00 - 1969+50.00	EB	OUT	490	23.5	1
TOTAL US ROUTE 20			11299	595	27
V3 SUB-TOTALS			11299	595	27
TOTAL			57121	4243	143

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		PIPE UNDERDRAIN SCHEDULE US ROUTE 20

SCALE: VERT. N/A
HORIZ. N/A
DATE 10/28/05

DRAWN BY DTE
CHECKED BY JAM

BITUMINOUS PAVEMENT PATCHING

STATION	LANE WIDTH	LENGTH OF PATCH		44200120		44200124		44200126		44000116		40601000	
		LT LANE (feet)	RT LANE (feet)	TYPE 2		TYPE 3		TYPE 4		Bit Rem over Patches 4"		Bit Repl over Patches	
				(Sq Yd)	(Sq Yd)	(Sq Yd)	(Sq Yd)	(Sq Yd)	(Sq Yd)	(Sq Yd)	(Sq Yd)	TON	TON
1723 + 18	12		10		13.3						14.6		3.3
1724 + 37	12	20						26.7		28.0		6.3	
1727 + 62	12		10		13.3						14.6		3.3
1731 + 15	12		6		8.0						9.3		2.1
1732 + 57	12		6		8.0						9.3		2.1
1734 + 16	12	10	10	13.3	13.3					14.6	14.6	3.3	3.3
1737 + 16	12		6		8.0						9.3		2.1
1737 + 54	12		15				20.0				21.3		4.8
1739 + 2	12		8		10.7						12.0		2.7
1740 + 3	12		6		8.0						9.3		2.1
1743 + 64	12		6		8.0						9.3		2.1
1744 + 76	12		8		10.7						12.0		2.7
1746 + 29	12		8		10.7						12.0		2.7
1746 + 30	12		6		8.0						9.3		2.1
1746 + 85	12		10		13.3						14.6		3.3
1748 + 35	12		6		8.0						9.3		2.1
1750 + 74	12		6		8.0						9.3		2.1
1751 + 70	12	6		8.0						9.3		2.1	
1753 + 5	12		6		8.0					9.3		2.1	
1754 + 81	12		10		13.3						14.6		3.3
1756 + 50	12		20					26.7		28.0		6.3	
1757 + 36	12		6		8.0						9.3		2.1
1760 + 89	12		8		10.7						12.0		2.7
1764 + 4	12		8		10.7						12.0		2.7
1764 + 95	12		8		10.7						12.0		2.7
1767 + 1	12		8		10.7						12.0		2.7
1769 + 17	12	6		8.0						9.3		2.1	
1769 + 18	12		6		8.0						9.3		2.1
1770 + 00	12		6		8.0						9.3		2.1
1770 + 27	12		6		8.0						9.3		2.1
1787 + 96	12		6		8.0						9.3		2.1
1790 + 49	12		6		8.0						9.3		2.1
1809 + 61	12	10		13.3						14.6		3.3	
1811 + 3	12		6		8.0						9.3		2.1
1811 + 73	12		8		10.7						12.0		2.7
1812 + 8	12	30						40.0		41.3		9.3	
1815 + 00	12		6		8.0						9.3		2.1
1817 + 24	12		8		10.7						12.0		2.7
1819 + 12	12		6		8.0						9.3		2.1
1819 + 33	12		6		8.0						9.3		2.1
1820 + 8	12		6		8.0						9.3		2.1
1824 + 16	12		6		8.0						9.3		2.1
1830 + 00	12		8		10.7						12.0		2.7
1835 + 00	12		10		13.3						14.6		3.3
1837 + 95	12		10		13.3						14.6		3.3
1838 + 77	12		10		13.3						14.6		3.3
1839 + 74	12		6		8.0						9.3		2.1
1840 + 19	12	15				20.0				21.3		4.8	
				42.7	389.3	20.0	20.0	66.7	26.7	147.7	481.4	33.1	107.8
FULL DEPTH PATCHES (50%)				216.0		20.0		47.0		629.1		140.9	

CLASS B PAVEMENT PATCHING

CONTRACT NO. 64577
 F.A.P. SECTION COUNTY TOTAL SHEETS SHEET NO.
 301 * STEPHENSON 290 42
 STA. TO STA.
 FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT
 • 177-(3, 4, 4-1)-1

24 FEET LANE WIDTH
12 ANE WIDTH

STATION	LANE WIDTH (feet)	LENGTH OF PATCH		AREA OF PATCHES								SAWING (3W+?L) (FT)	DOWEL BARS (EACH)	TIE BARS (EACH)	PAVEMEN FABRIC (SY)	
		LT LANE (FEET)	RT LANE (FEET)	TYPE 1		TYPE 2		TYPE 3		TYPE 4						
				(SY)	(SY)	(SY)	(SY)	(SY)	(SY)	(SY)	(SY)					
1853 + 60	12		6				8.0					42	20			
1864 + 28	12	6				8.0						42	20			
1866 + 62	12		6			8.0						42	20			
1867 + 11	12		6			8.0						42	20			
1873 + 60	12		6			8.0						42	20			
1874 + 67	12		6			8.0						42	20			
1875 + 43	12	6				8.0						42	20			
1875 + 94	12		6			8.0						42	20			
1880 + 9	12		6			8.0						42	20			
1880 + 24	12	6				8.0						42	20			
1884 + 93	12		6			8.0						42	20			
1885 + 00	12		6			8.0						42	20			
1885 + 31	12		6			8.0						42	20			
1886 + 7	12		6			8.0						42	20			
1886 + 78	12		6			8.0						42	20			
1887 + 74	12		6			8.0						42	20			
1888 + 33	12		6			8.0						42	20			
1889 + 7	12		6			8.0						42	20			
1891 + 48	12		6			8.0						42	20			
1893 + 91	12		6			8.0						42	20			
1894 + 58	12		6			8.0						42	20			
1941 + 20	12		6			8.0						42	20			
1940 + 25	12		6			8.0						42	20			
1940 + 31	12		8			10.7						44	20			
1942 + 64	12		6			8.0						42	20			
1942 + 83	12		6			8.0						42	20			
1951 + 65	12	6				8.0						42	20			
1953 + 79	12		6			8.0						42	20			
1956 + 44	12		6			8.0						42	20			
1957 + 9	12	6				8.0						42	20			
1957 + 50	12		6			8.0						42	20			
1958 + 14	12	8				10.7						44	20			
1960 + 26	12		6			8.0						42	20			
1960 + 28	12	6				8.0						42	20			
1960 + 75	12	6				8.0						42	20			
1963 + 12	12		6			8.0						42	20			
1970 + 45	24	12										84	44	32.0		
1970 + 83	24		12									84	44	32.0		
1974 + 28	24	12										84	44	32.0		
1975 + 00	24		6									78	44	16.0		
1978 + 73	24		12									84	44	32.0		
TOTALS						66.7	226.7			16.0	64.0	64.0	1930	940		144.0

REVISIONS
 NAME DATE
 PRE-FINAL 05/24/05

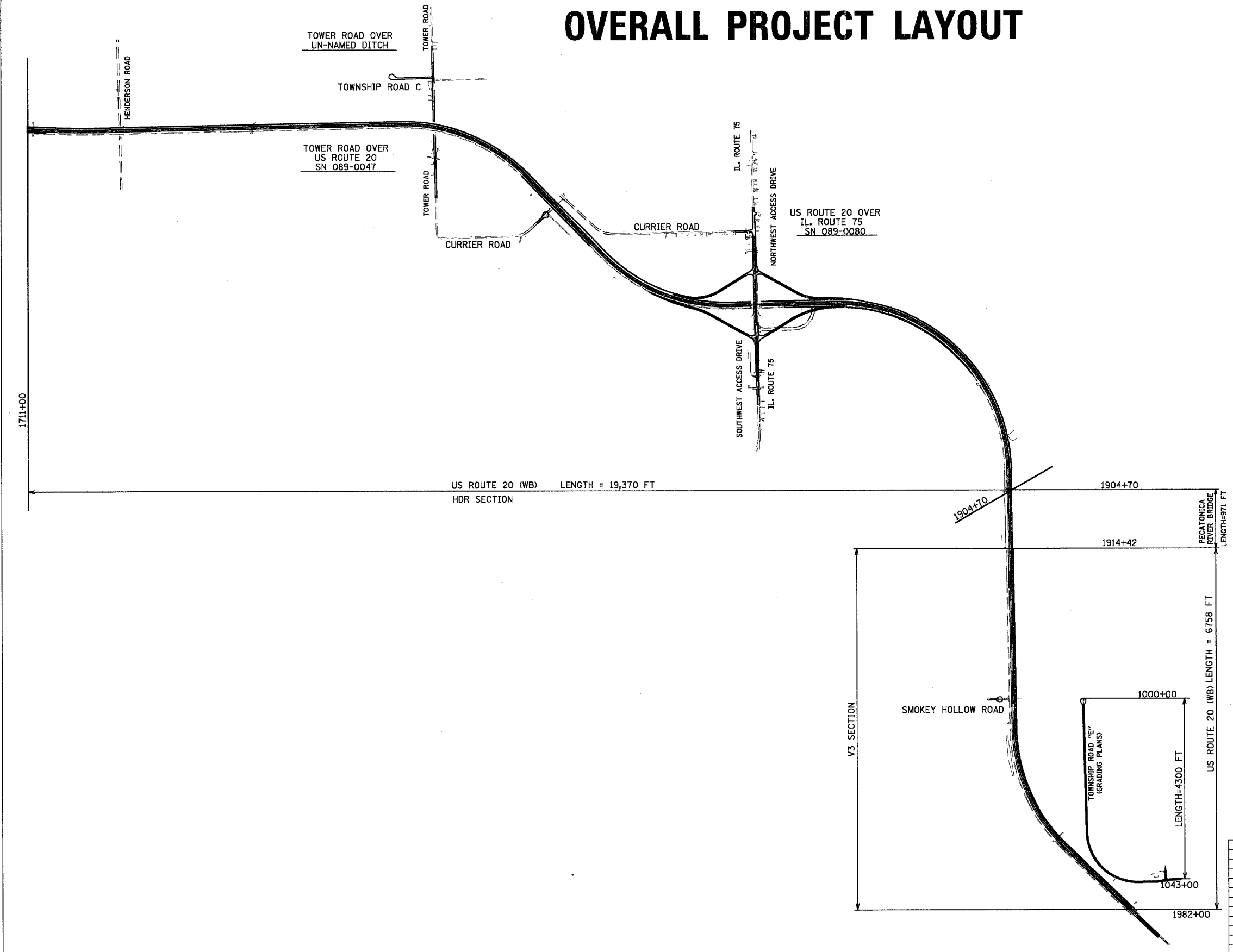
ILLINOIS DEPARTMENT OF TRANSPORTATION
PATCHING SCHEDULE
US ROUTE 20

SCALE: VERT. N/A
 HORIZ. N/A
 DATE 10/28/05

DRAWN BY TVN
 CHECKED BY JAM

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	43
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	
• 177-3, 4, 4-D-1				

OVERALL PROJECT LAYOUT



REVISIONS	
NAME	DATE

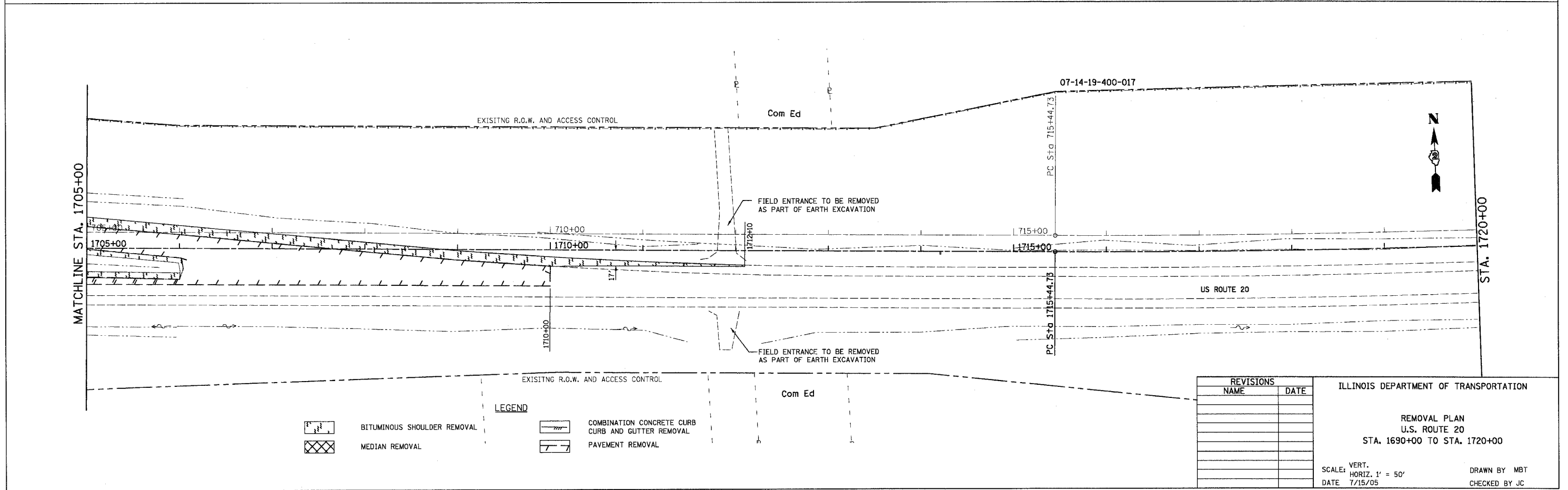
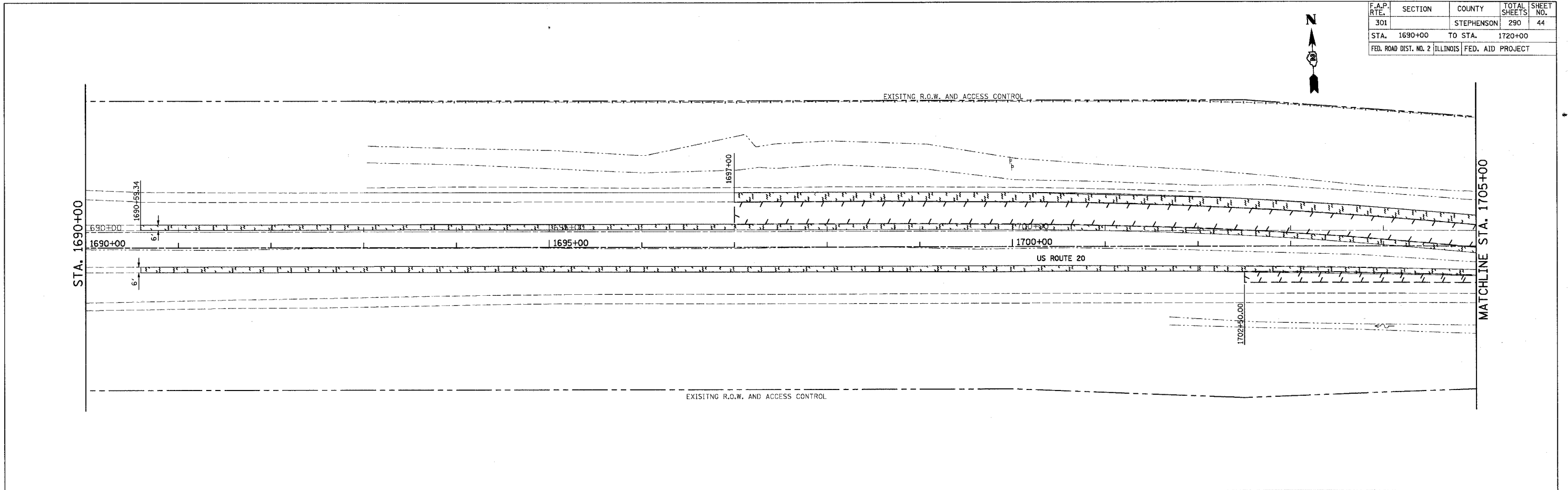
ILLINOIS DEPARTMENT OF TRANSPORTATION
OVERALL PROJECT LAYOUT
US ROUTE 20

SCALE: VERT. N/A
 HORIZ. 1"=800"
 DATE 10/28/05

DRAWN BY TVN
 CHECKED BY

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	44
STA. 1690+00		TO STA. 1720+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



LEGEND

	BITUMINOUS SHOULDER REMOVAL		COMBINATION CONCRETE CURB CURB AND GUTTER REMOVAL
	MEDIAN REMOVAL		PAVEMENT REMOVAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

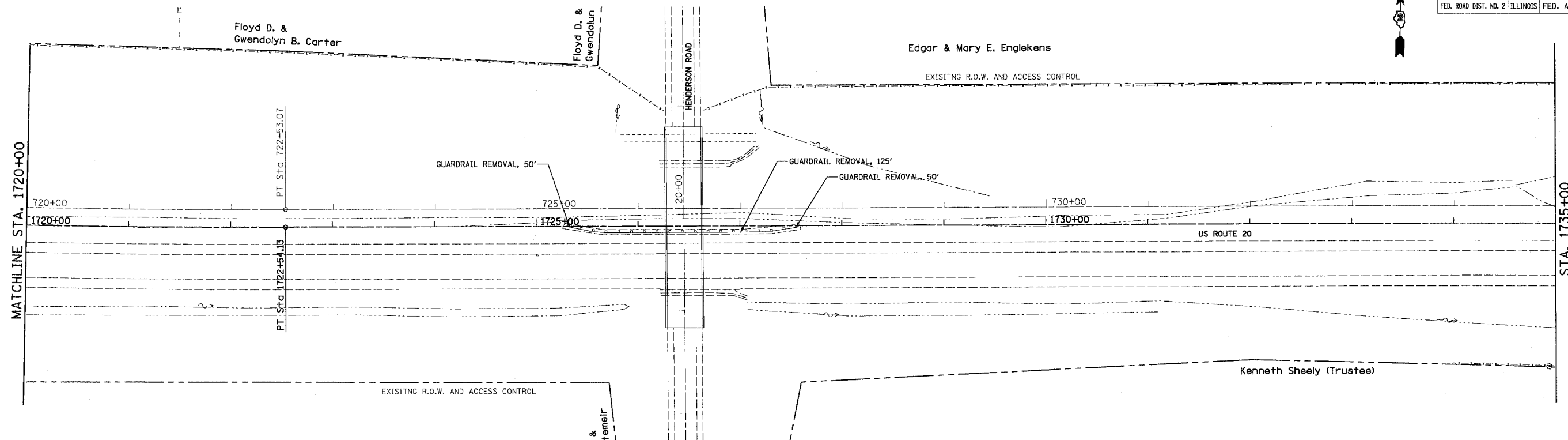
REMOVAL PLAN
U.S. ROUTE 20
STA. 1690+00 TO STA. 1720+00

SCALE: VERT. 1" = 50'
DATE 7/15/05

DRAWN BY MBT
CHECKED BY JC

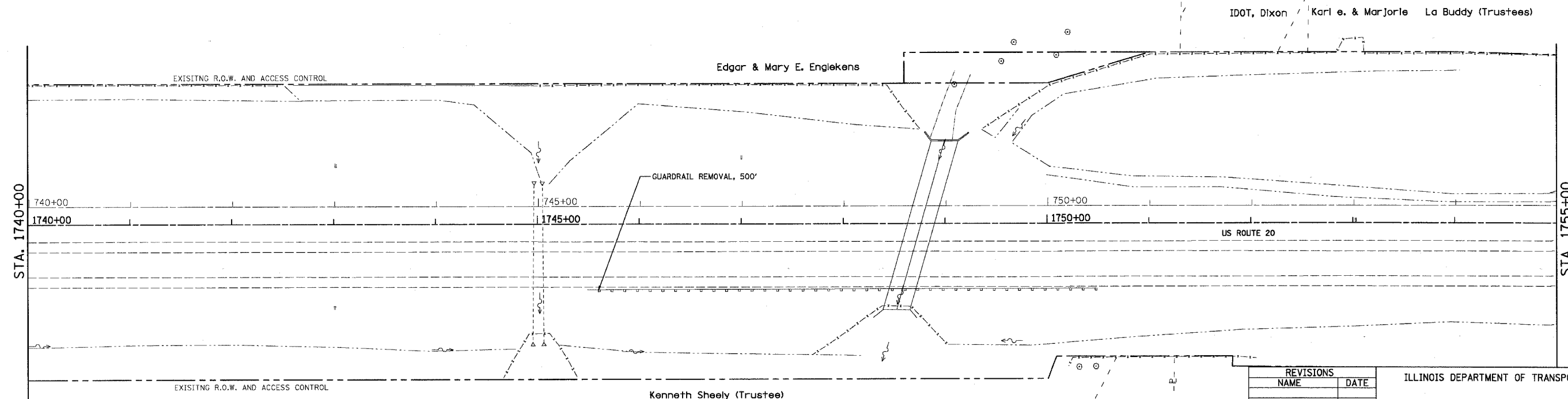
CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	45
STA. 1720+00		TO STA. 1755+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



LEGEND

- BITUMINOUS SHOULDER REMOVAL
- MEDIAN REMOVAL
- COMBINATION CONCRETE CURB
CURB AND GUTTER REMOVAL
PAVEMENT REMOVAL



REVISIONS	
NAME	DATE

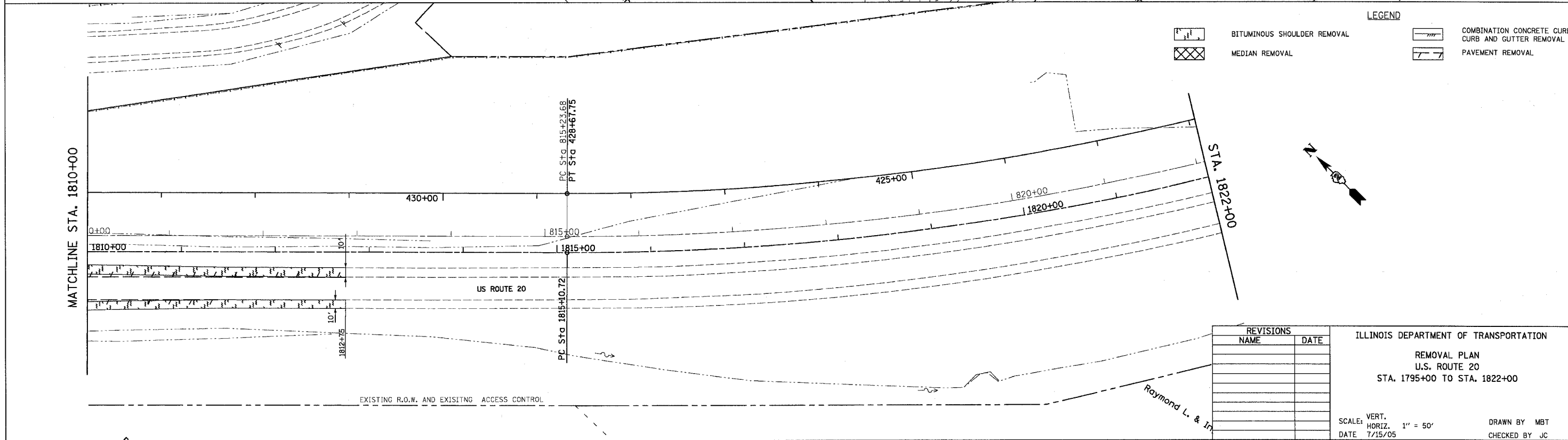
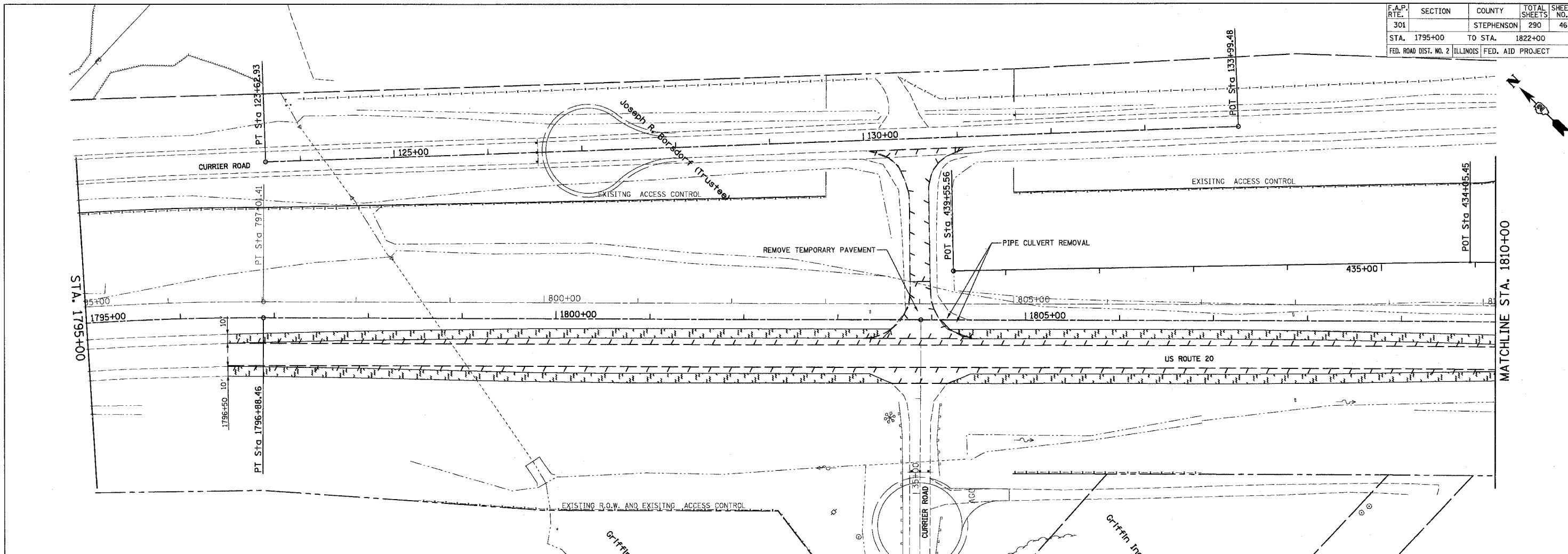
ILLINOIS DEPARTMENT OF TRANSPORTATION
REMOVAL PLAN
U.S. ROUTE 20
STA. 1720+00 TO STA. 1755+00

SCALE: VERT. 1"= 50'
DATE 7/15/05
DRAWN BY MBT
CHECKED BY JC

07-14-20-326-003

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	46
STA. 1795+00		TO STA. 1822+00		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		



LEGEND

- BITUMINOUS SHOULDER REMOVAL
- MEDIAN REMOVAL
- COMBINATION CONCRETE CURB CURB AND GUTTER REMOVAL
- PAVEMENT REMOVAL

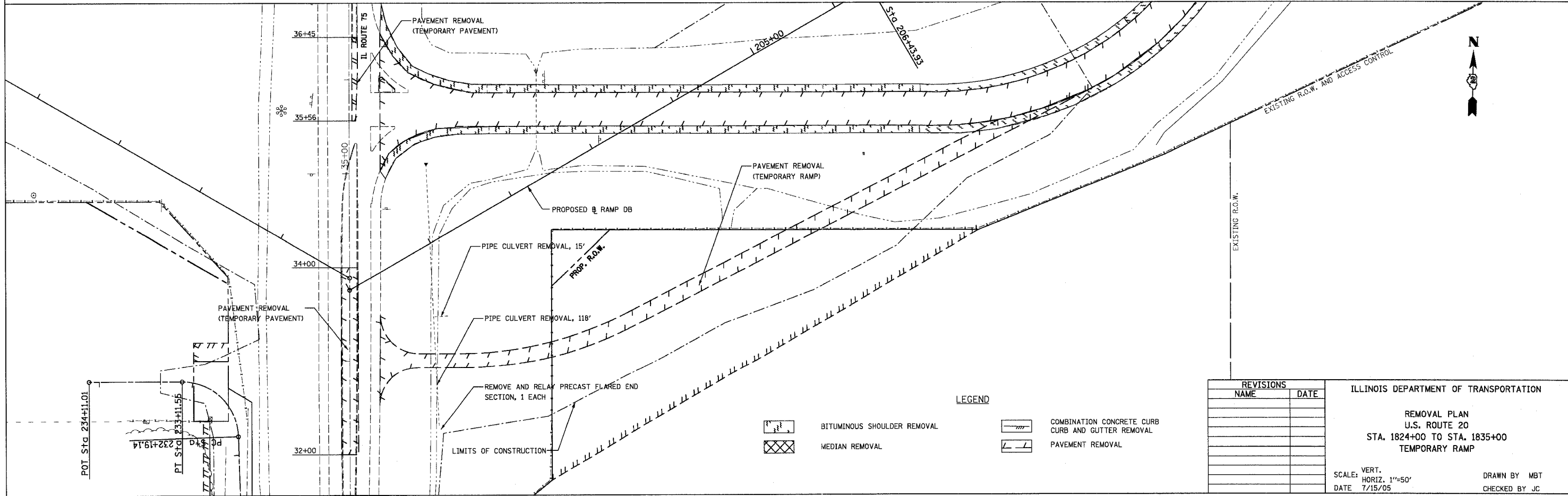
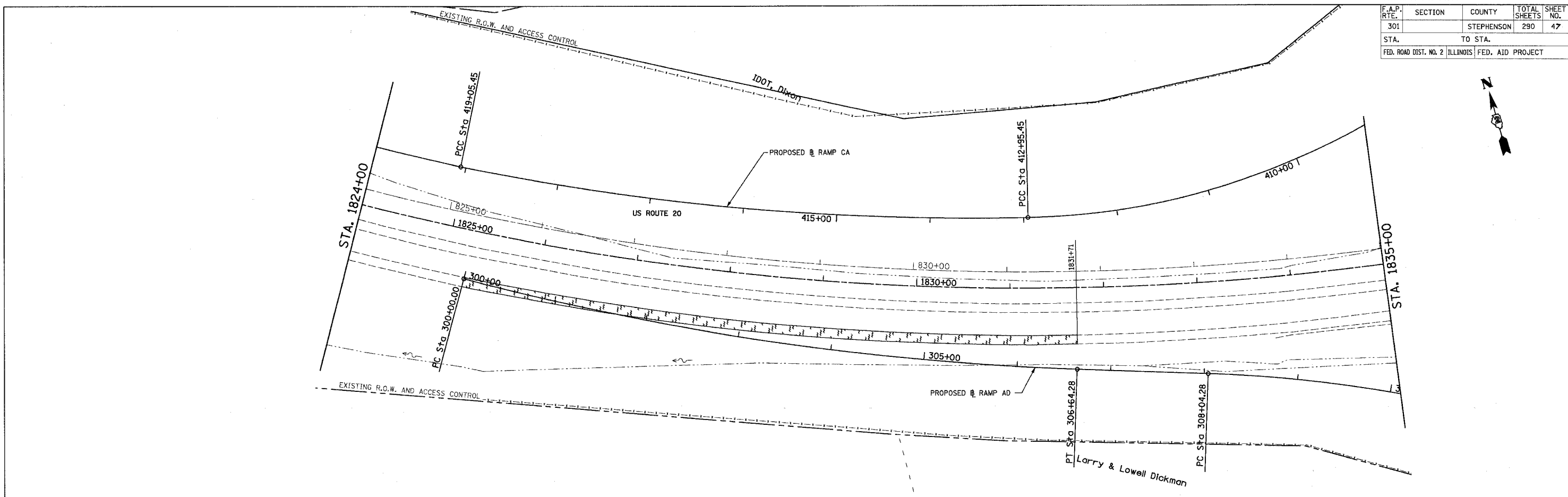
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 REMOVAL PLAN
 U.S. ROUTE 20
 STA. 1795+00 TO STA. 1822+00

SCALE: VERT. 1" = 50'
 HORIZ. 1" = 50'
 DATE 7/15/05
 DRAWN BY MBT
 CHECKED BY JC

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	47
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		



LEGEND

-  BITUMINOUS SHOULDER REMOVAL
-  MEDIAN REMOVAL
-  COMBINATION CONCRETE CURB AND GUTTER REMOVAL
-  PAVEMENT REMOVAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

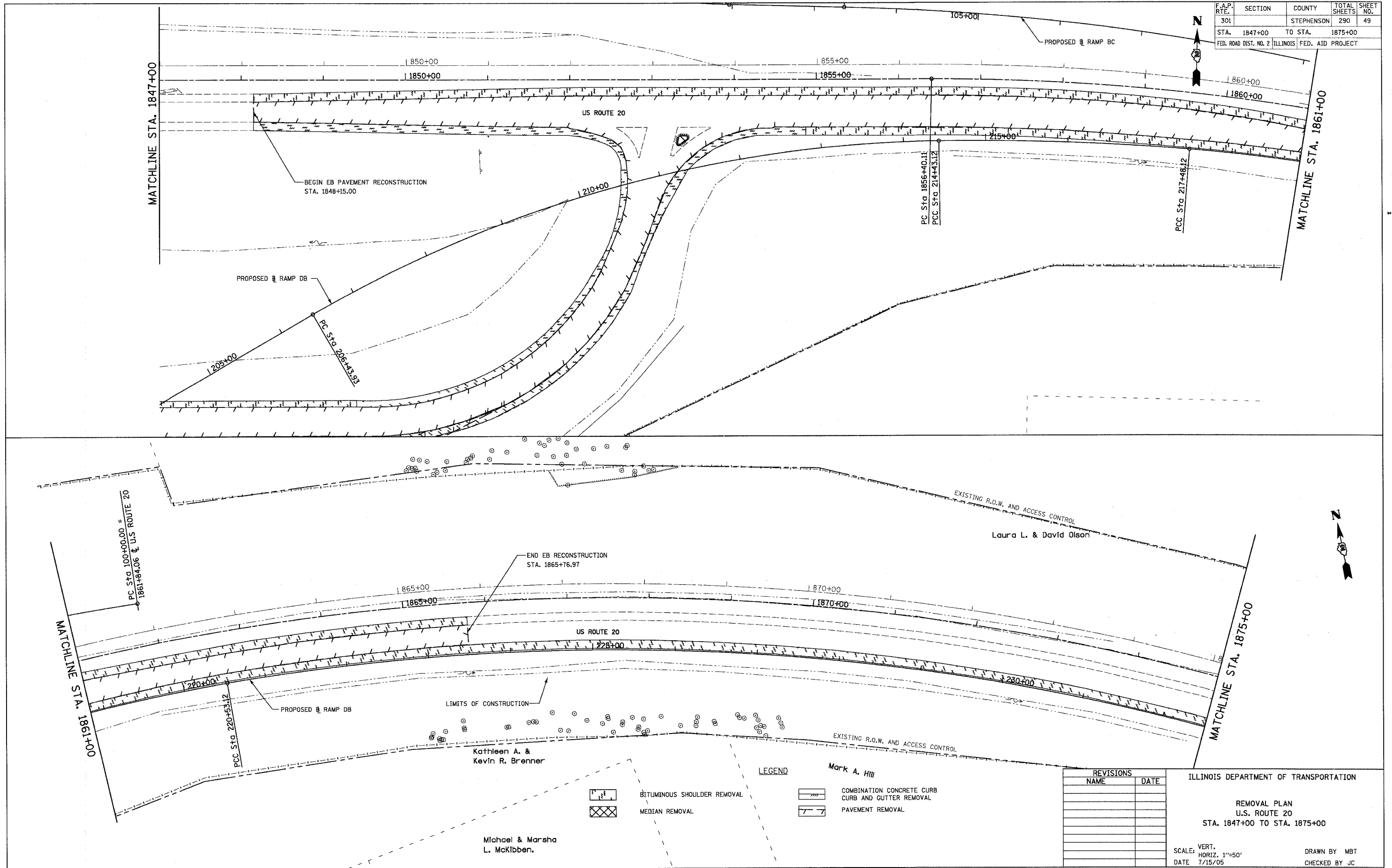
REMOVAL PLAN
U.S. ROUTE 20
STA. 1824+00 TO STA. 1835+00
TEMPORARY RAMP

SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 7/15/05

DRAWN BY MBT
CHECKED BY JC

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	49
STA. 1847+00		TO STA. 1875+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

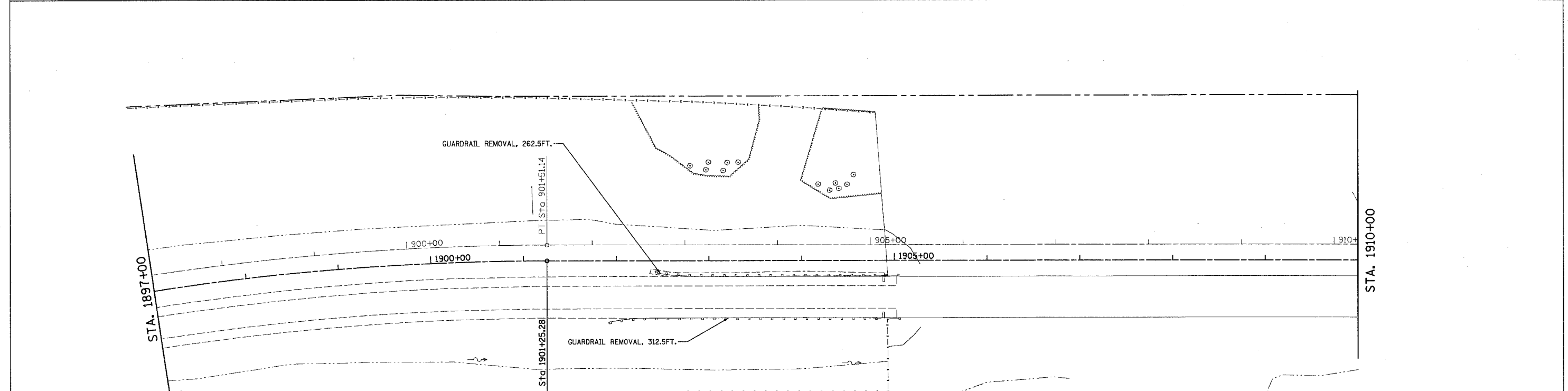
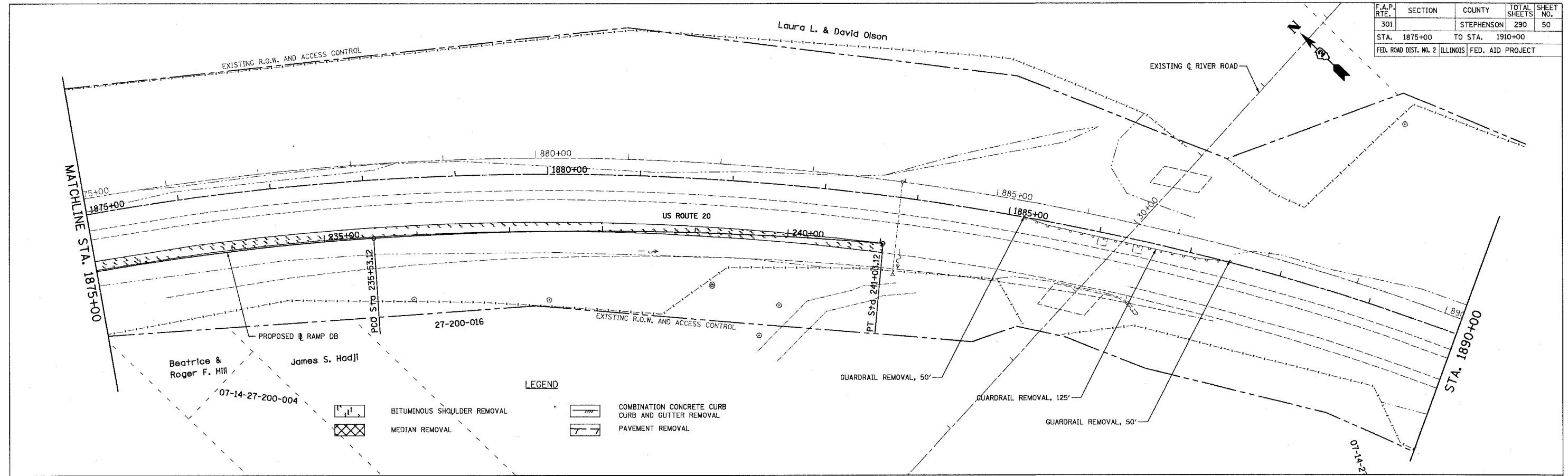
REMOVAL PLAN
U.S. ROUTE 20
STA. 1847+00 TO STA. 1875+00

SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 7/15/05

DRAWN BY MBT
CHECKED BY JC

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	50
STA. 1875+00		TO STA. 1910+00		
FED. ROAD DIST. NO. 2		ILLINOIS		FED. AID PROJECT



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

REMOVAL PLAN
U.S. ROUTE 20
STA. 1875+00 TO STA. 1910+00

SCALE: VERT. 1" = 50'
DATE 7/15/05

DRAWN BY MBT
CHECKED BY JC

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	51
STA. 1680+00 TO STA. 1695+00		ILLINOIS FED. AID PROJECT		

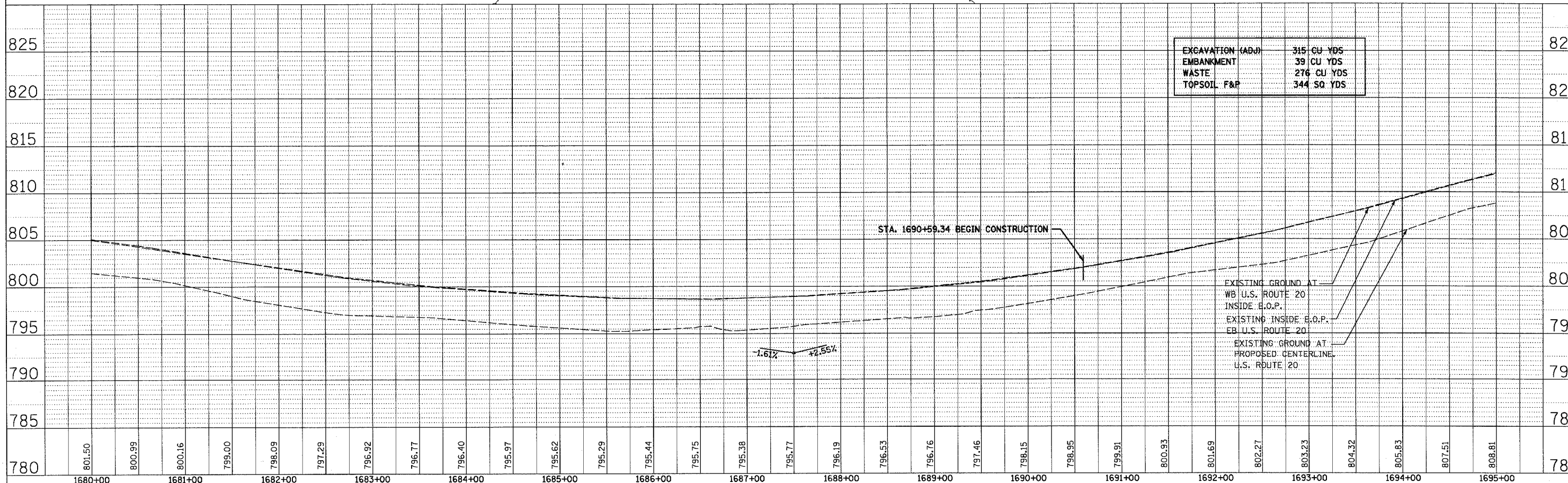
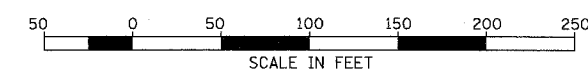
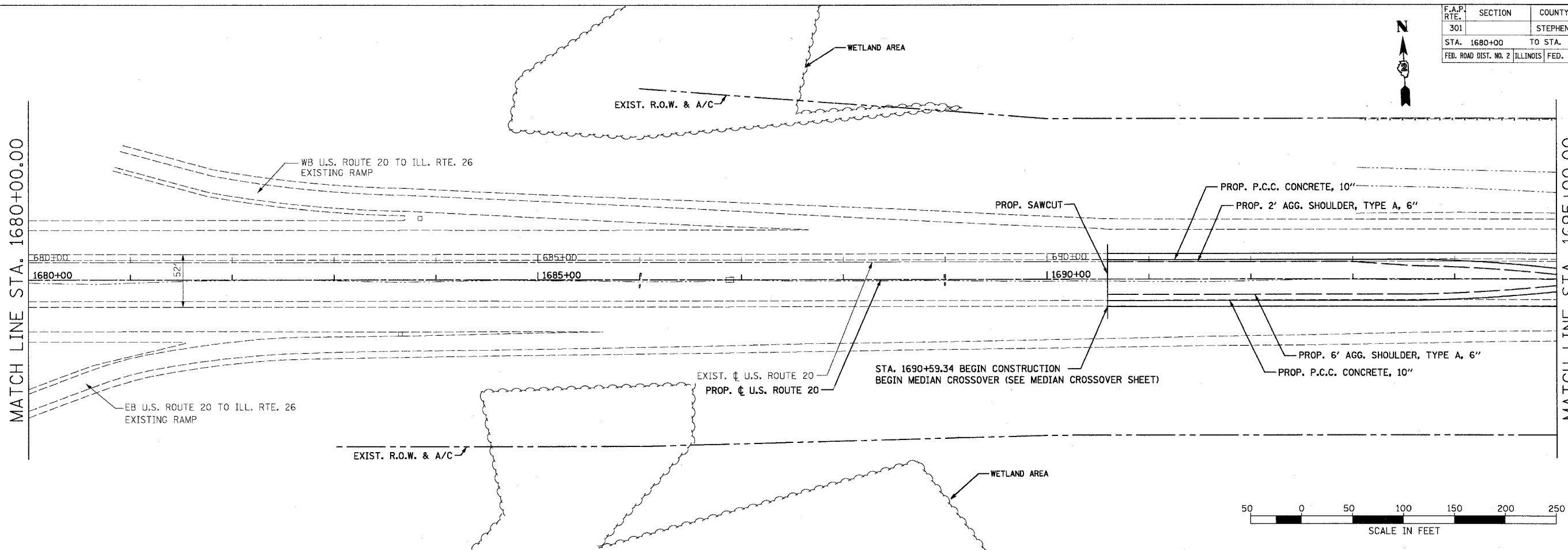


PLAN	DATE	BY
REVISIONS		
NO.	DESCRIPTION	DATE
1	ALIGNED	
2	CHECKED	
3	PLOTTED	
4	FILED	
5	NO. 1	

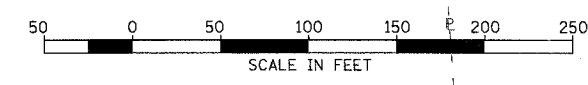
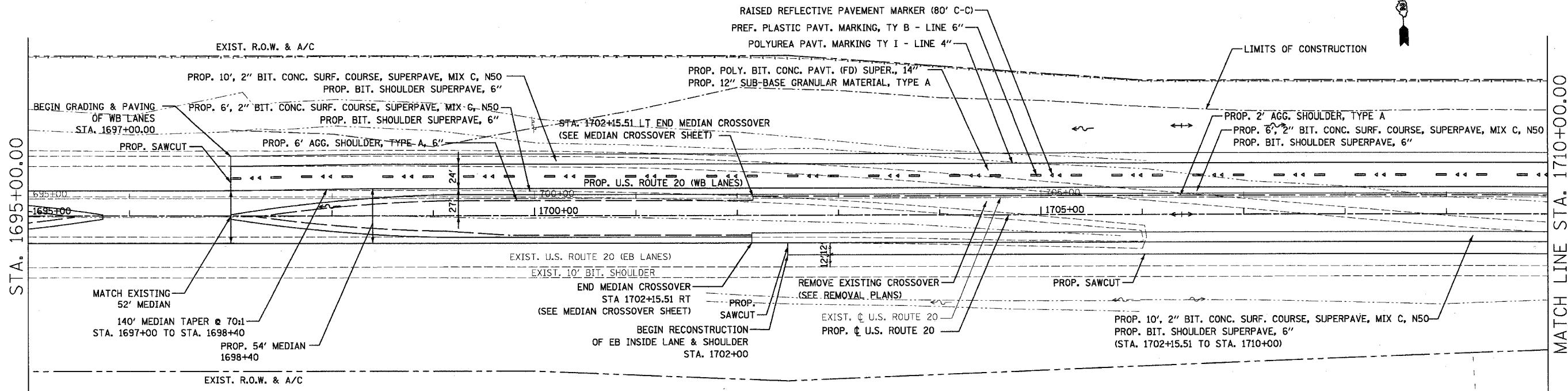
PROFILE	DATE	BY
REVISIONS		
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1	GRADES CHECKED	
2	STRUCTURE NOTATION SHOWN	
3	NO. 1	

MATCH LINE STA. 1680+00.00

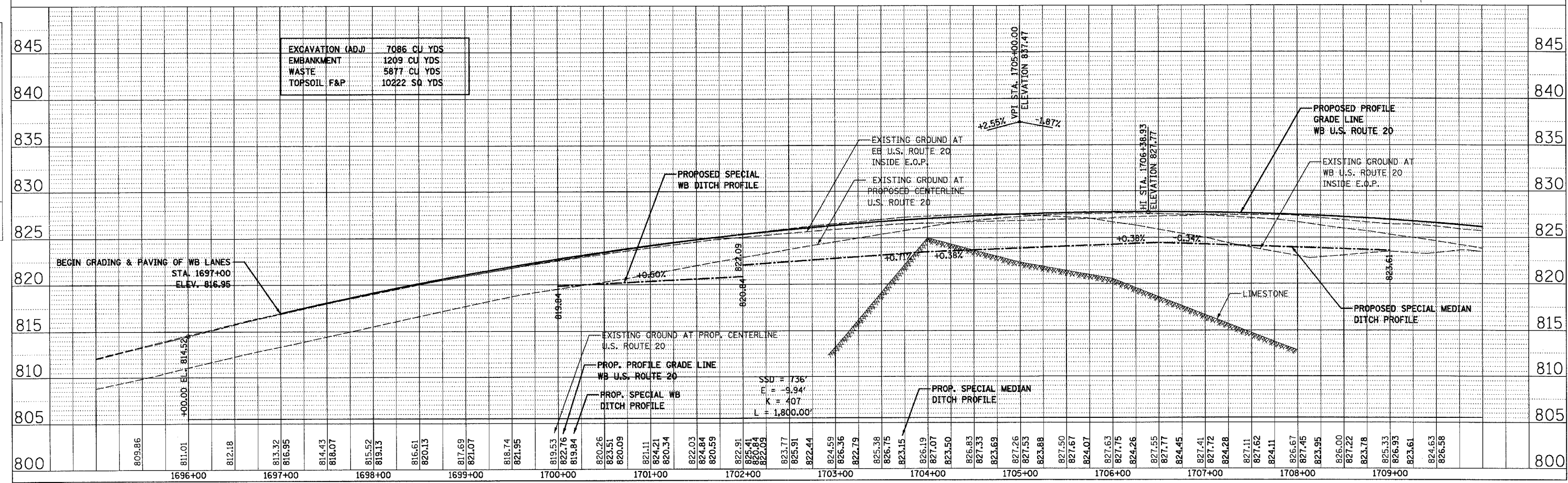
MATCH LINE STA. 1695+00.00



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	52
STA. 1695+00		TO STA. 1710+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



EXCAVATION (ADJ.)	7086 CU YDS
EMBANKMENT	1209 CU YDS
WASTE	5877 CU YDS
TOPSOIL F&P	10222 SQ YDS

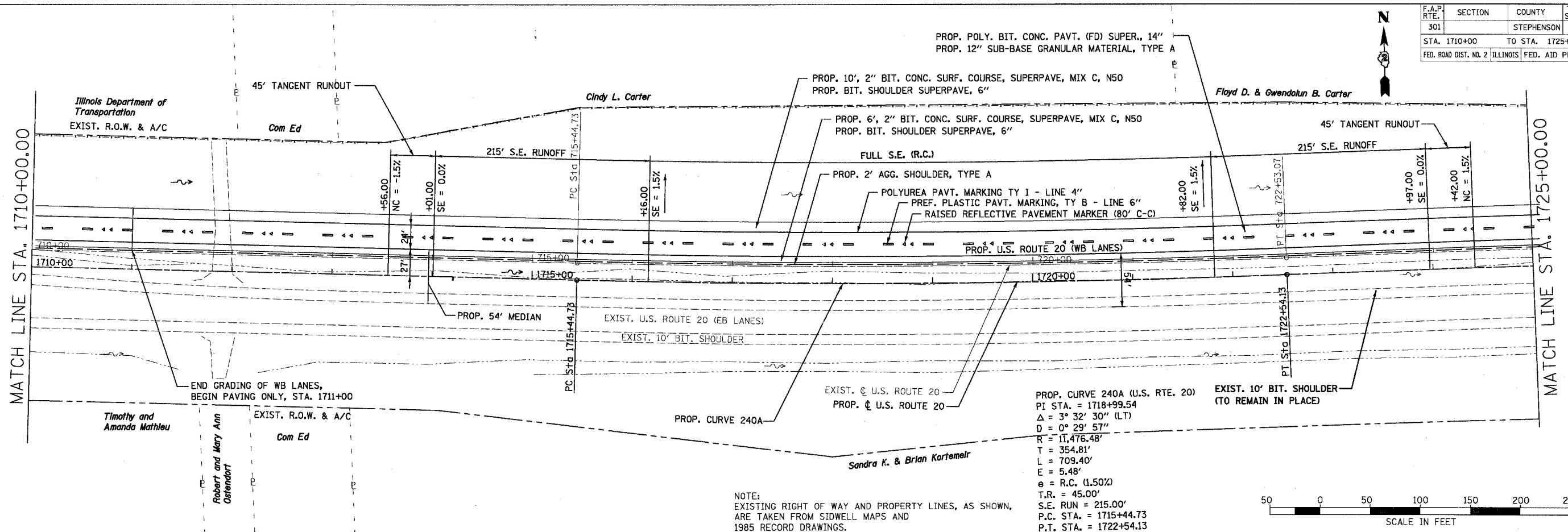


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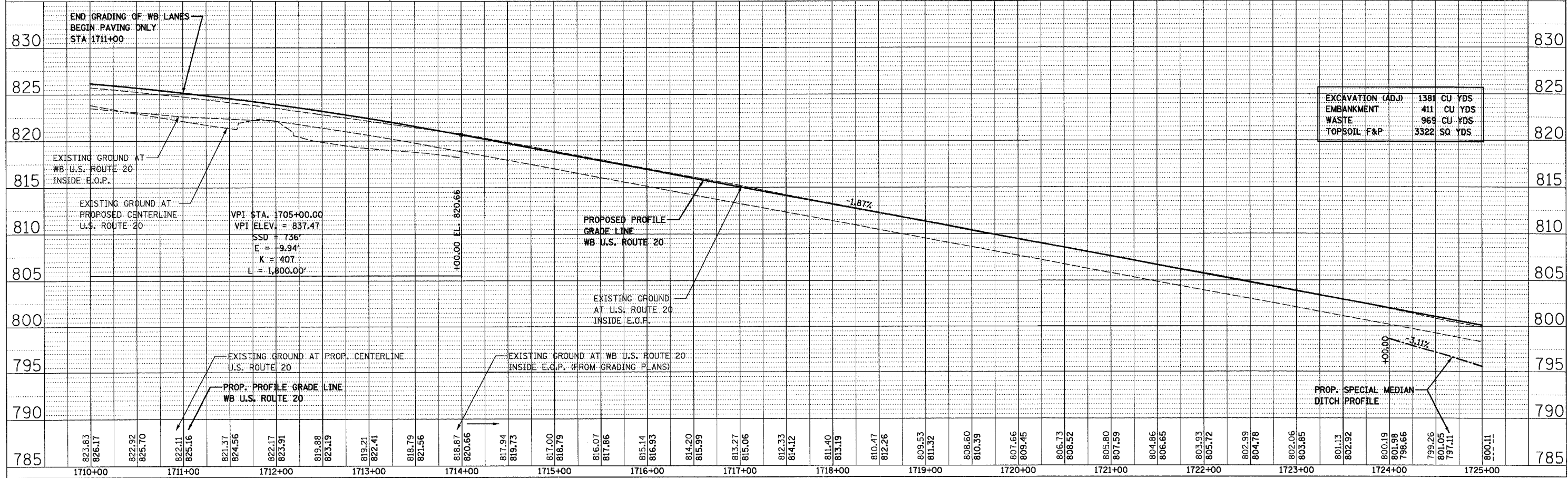
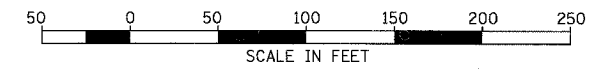
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	53
STA. 1710+00		TO STA. 1725+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE	BY
SURVEYED		
ALIGNED		
CHECKED		
NO. OF WAY CHECKED		
NO. OF FILE NAME		

PROFILE	DATE	BY
SURVEYED		
GRADES CHECKED		
NO. NOTED		
STRUCTURE NOTATIONS CHECKED		

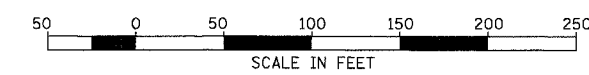
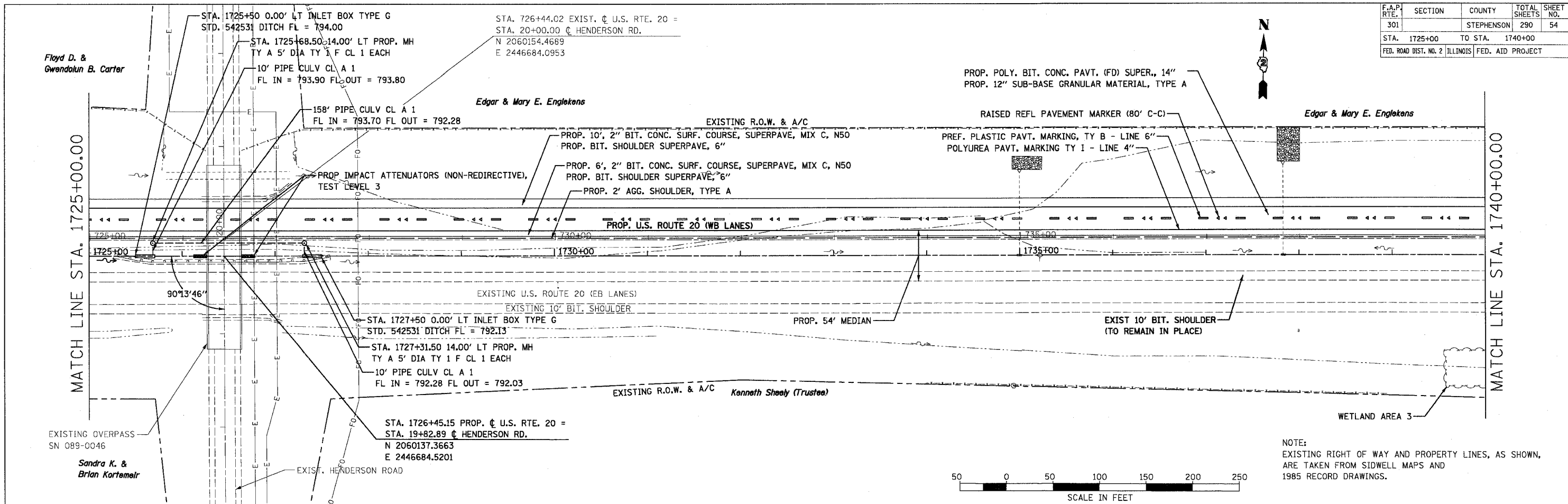


NOTE:
 EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN,
 ARE TAKEN FROM SIDWELL MAPS AND
 1985 RECORD DRAWINGS.

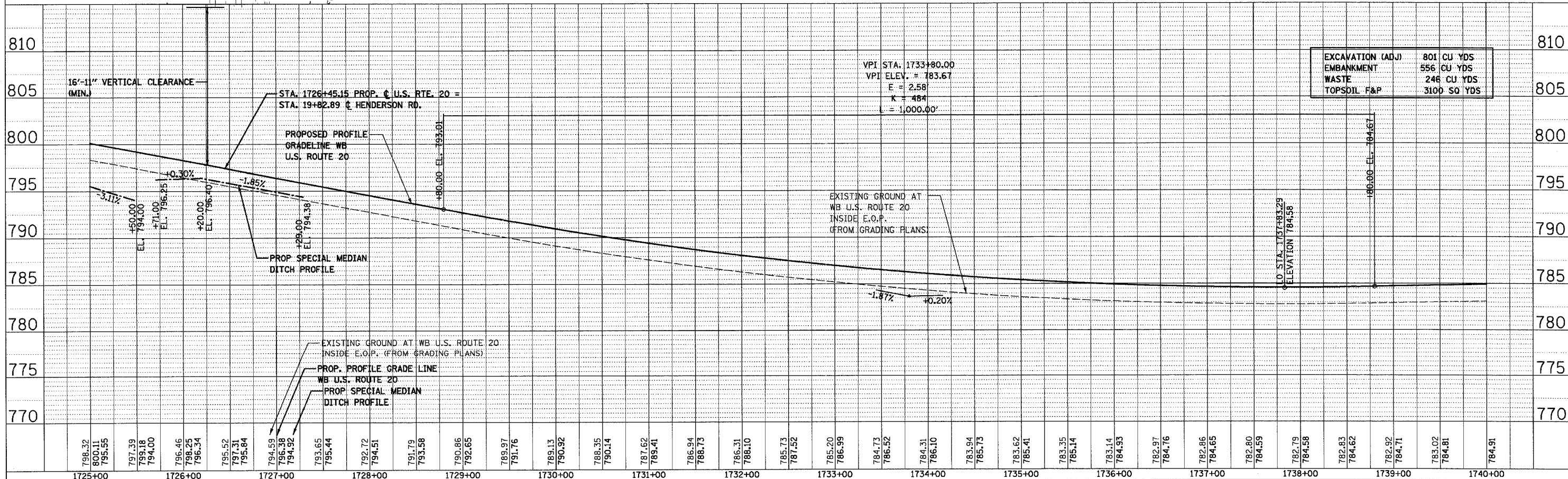


US ROUTE 20 (SHEET 3 OF 17)
 STA. 1710+00 TO STA. 1725+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	54
STA. 1725+00		TO STA. 1740+00		
FED. ROAD DIST. NO. 2		ILLINOIS		FED. AID PROJECT



NOTE:
 EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN, ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.

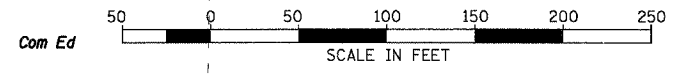
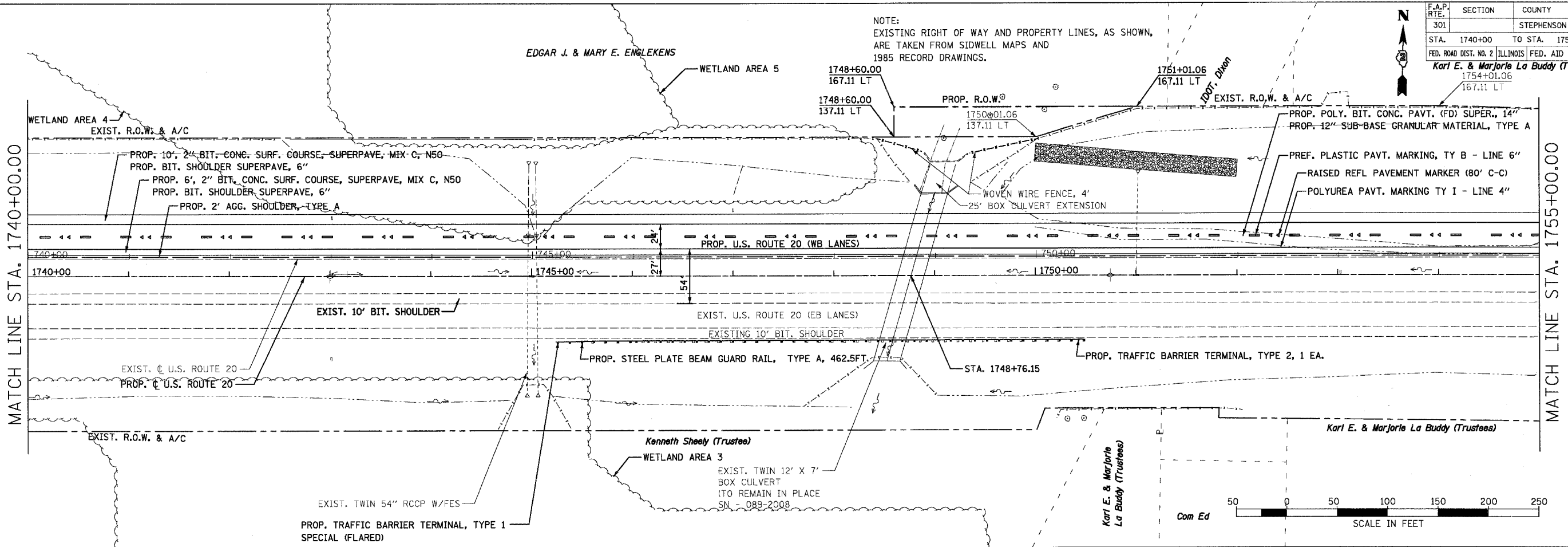


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	55

STA. 1740+00 TO STA. 1755+00
 FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT
 Karl E. & Marjorie La Buddy (Trustees)
 1754+01.06
 167.11 LT



NOTE:
 EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN,
 ARE TAKEN FROM SIDWELL MAPS AND
 1985 RECORD DRAWINGS.

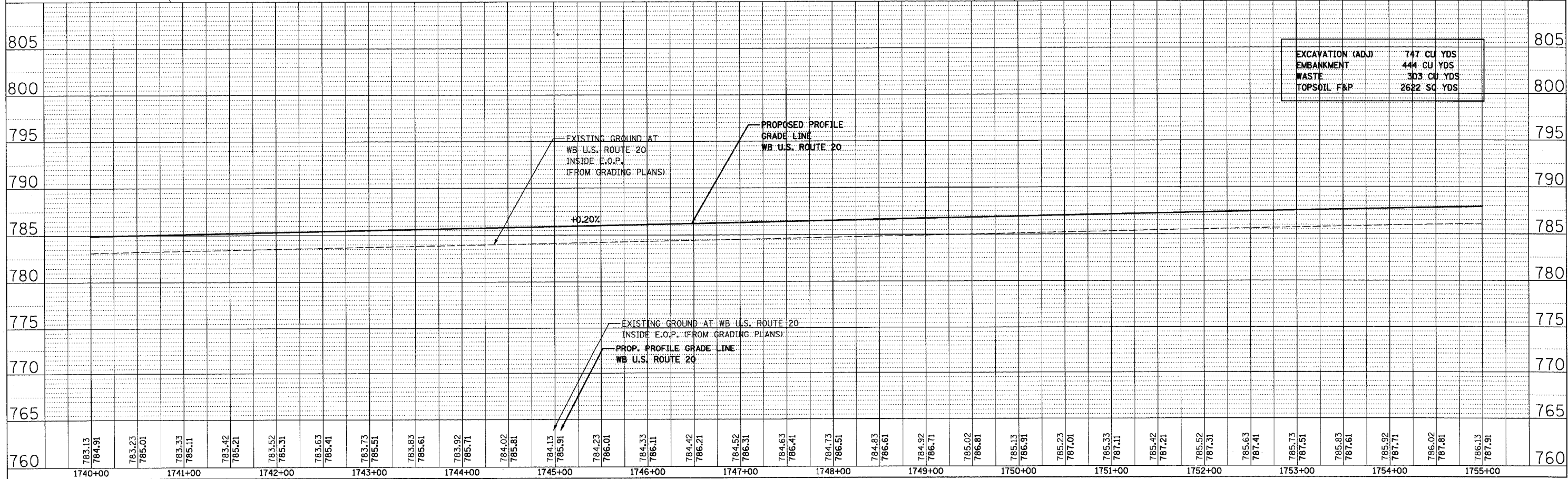


PLAN	SURVEYED	DATE
BY		
DATE		

NOTE: NO. 1
 CHECKED BY: DATE: FILE NAME:

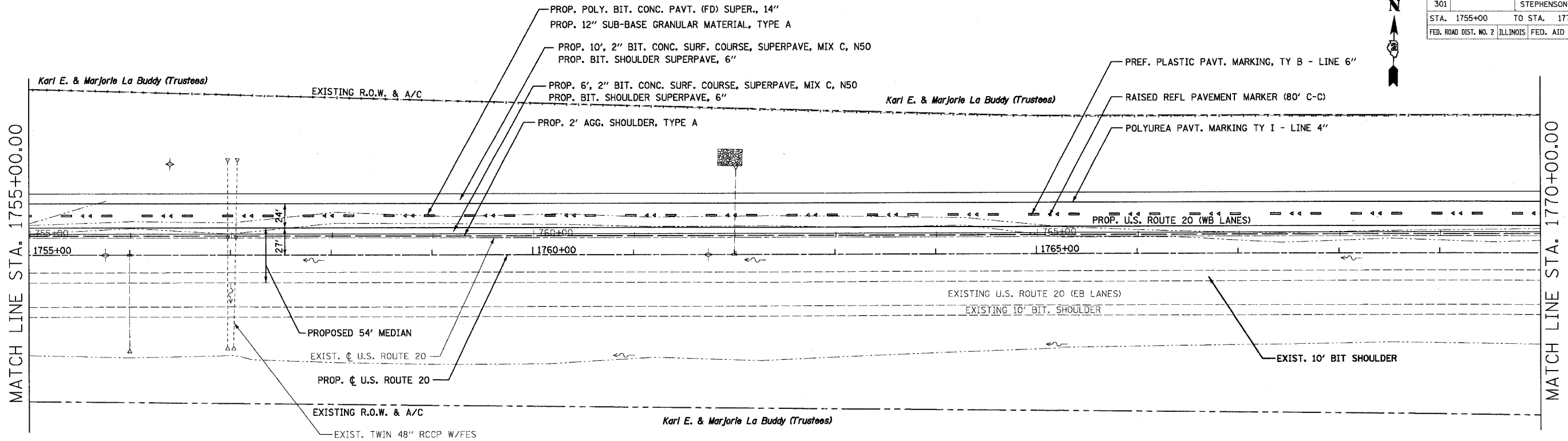
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BY		
DATE		

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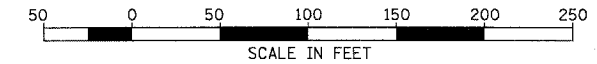


EXCAVATION (ADJ)	747 CU YDS
EMBANKMENT	444 CU YDS
WASTE	303 CU YDS
TOPSOIL F&P	2622 SQ YDS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	56
STA. 1755+00		TO STA. 1770+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

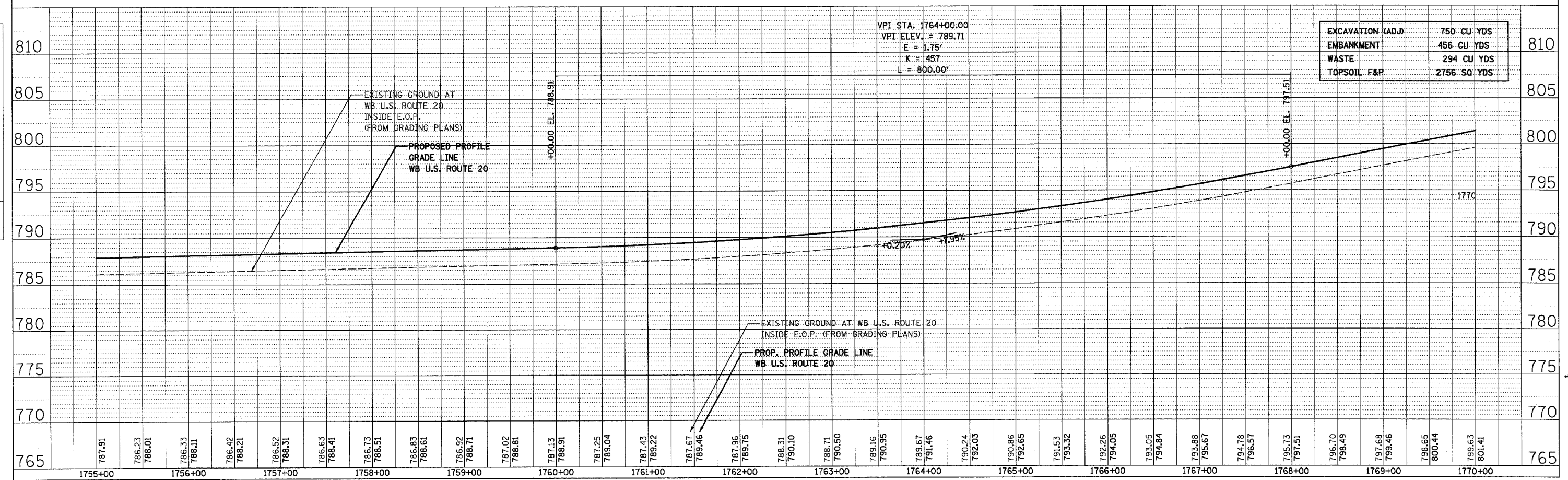


NOTE:
EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN,
ARE TAKEN FROM SIDWELL MAPS AND
1985 RECORD DRAWINGS.



PLAN	SURVEYED	DATE
NOTE BOOK	ALIGNED	
NO.	RT. OF WAY CHECKED	
	PLD FILE NAME	

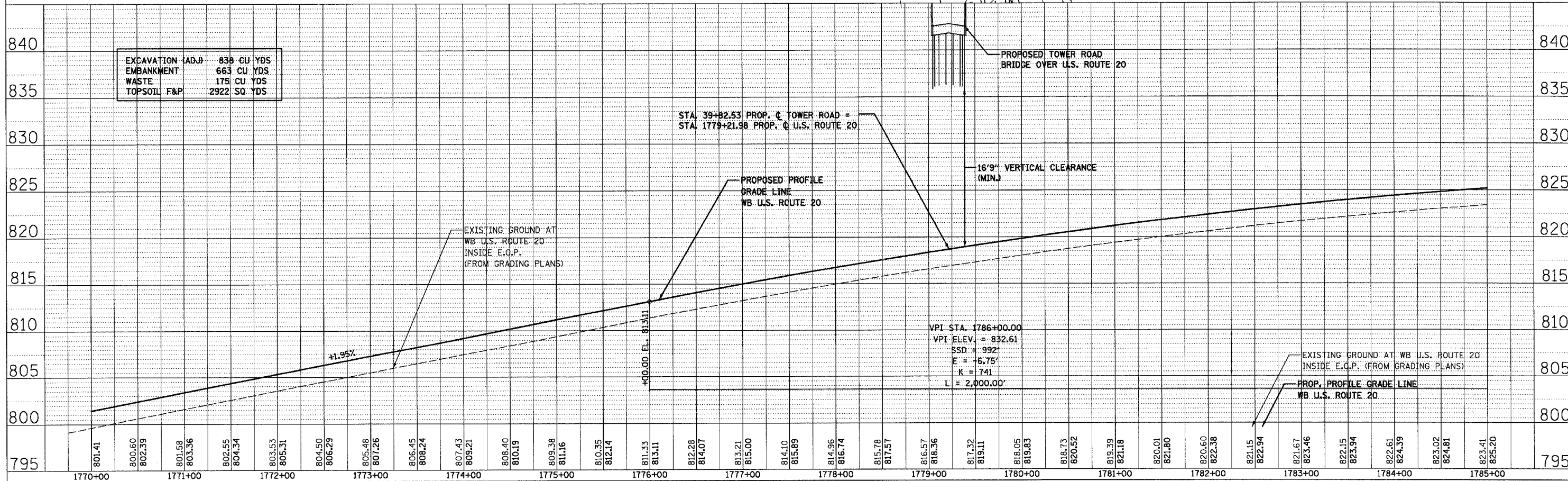
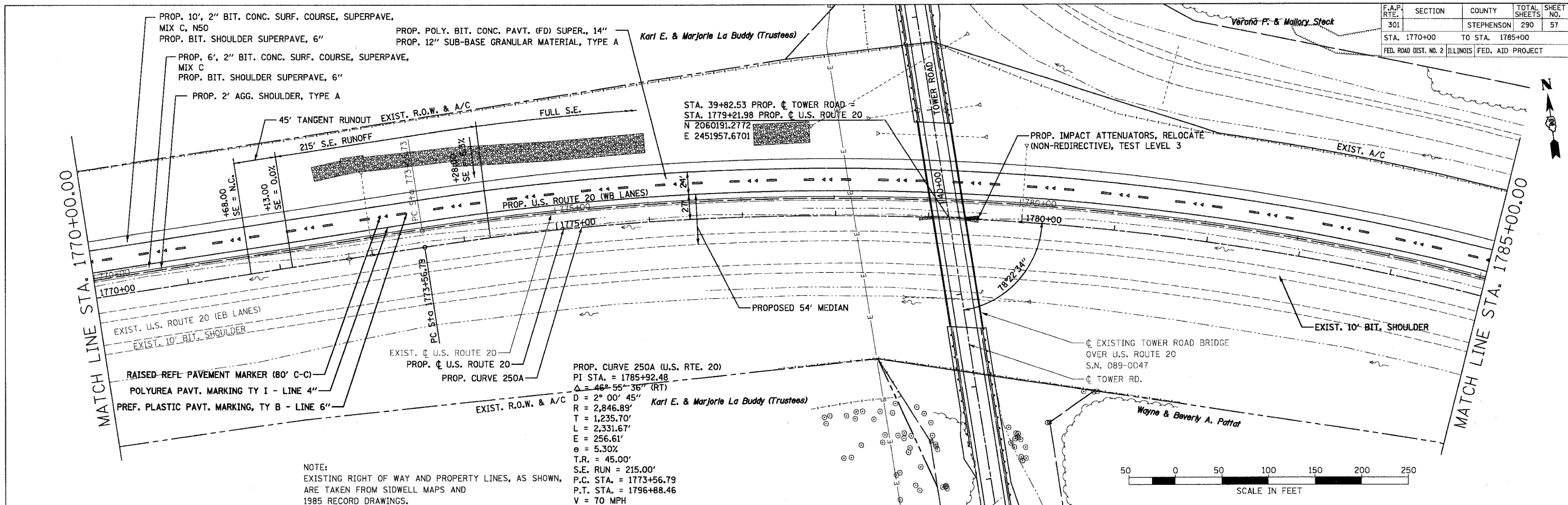
PROFILE	SURVEYED	DATE
NOTE BOOK	NOTED	
NO.	STRUCTURE NOTATIONS CHKD	



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	57
STA. 1770+00		TO STA. 1785+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE	BY
SURVEYED		
ALIGNED		
CHECKED		
NOTE BOOK		
NO.		

PROFILE	DATE	BY
SURVEYED		
GRADES		
CHECKED		
NOTE BOOK		
NO.		

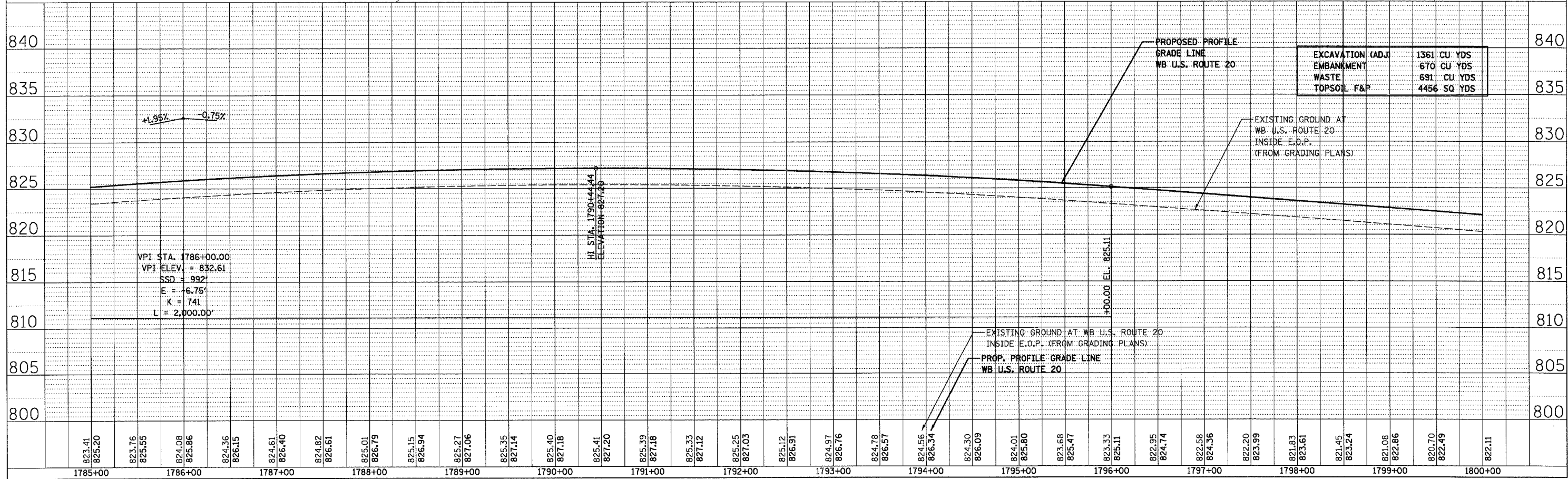
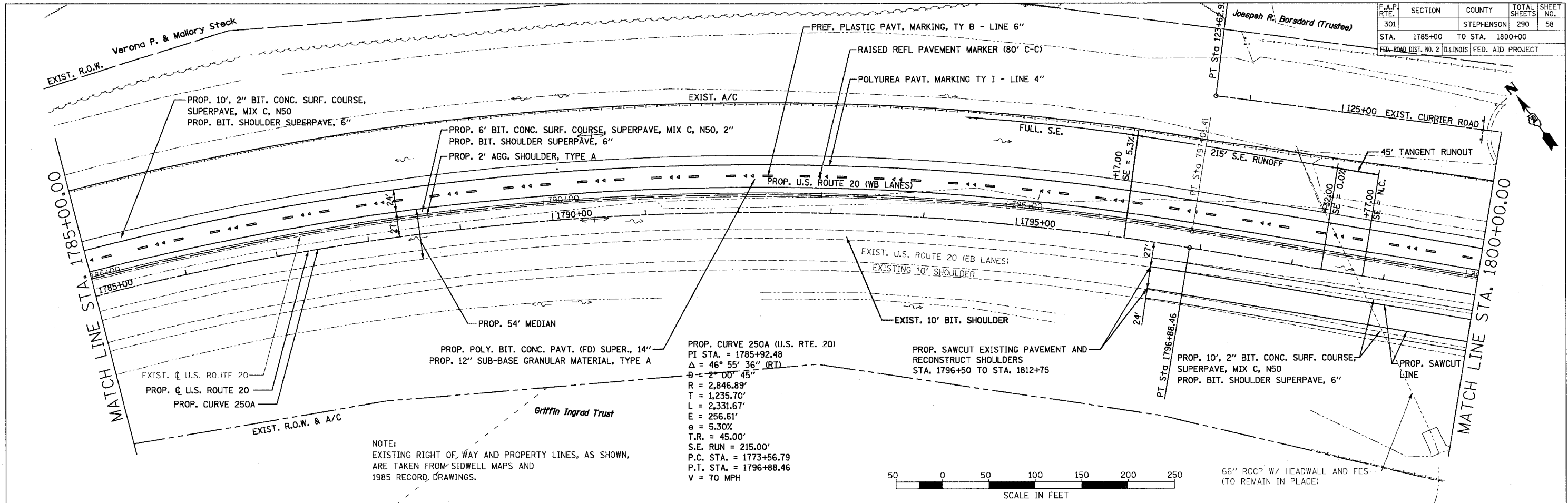


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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	58
STA. 1785+00		TO STA. 1800+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE	BY
REVIEWED		
DESIGNED		
CHECKED		
DATE		

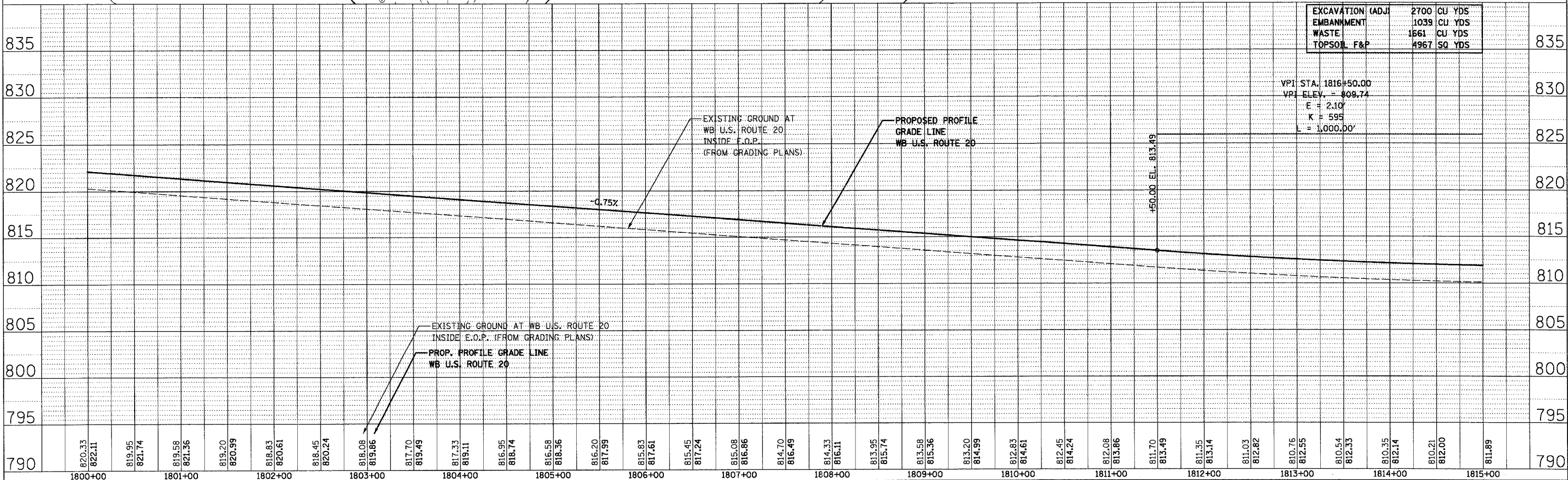
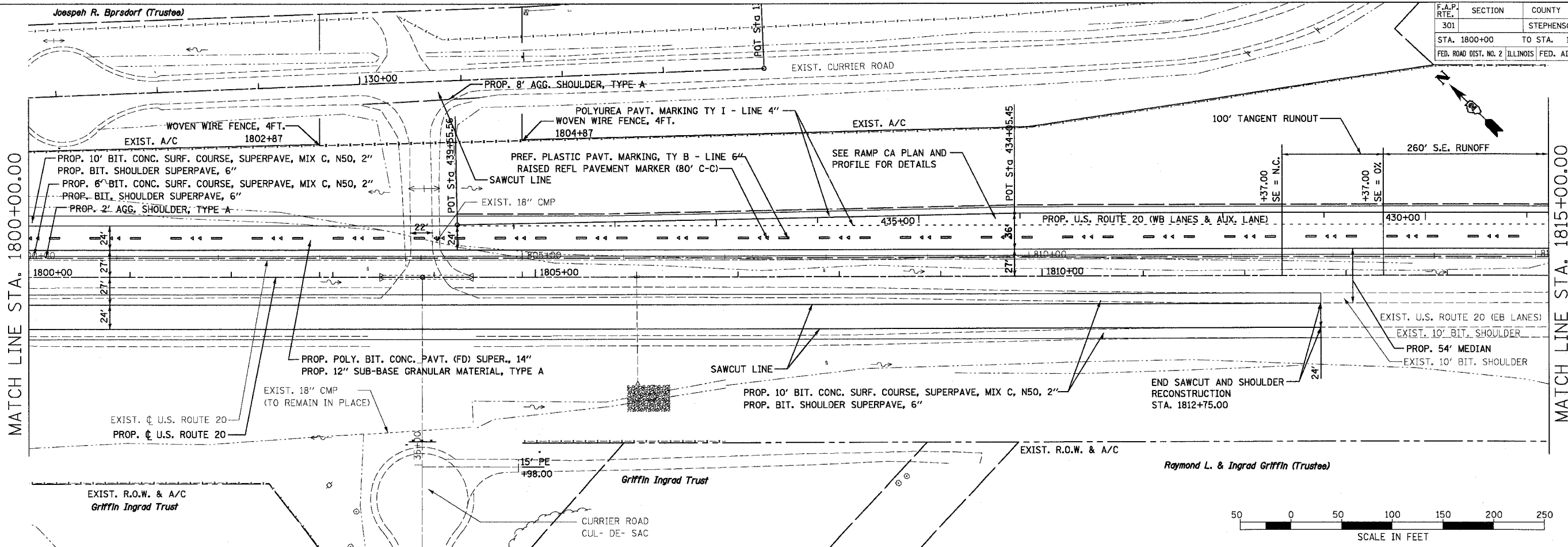
PROFILE	DATE	BY
REVIEWED		
DESIGNED		
CHECKED		
DATE		



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	59
STA. 1800+00		TO STA. 1815+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE
BY	
REVISIONS	
NO.	DESCRIPTION

PROFILE	DATE
BY	
REVISIONS	
NO.	DESCRIPTION

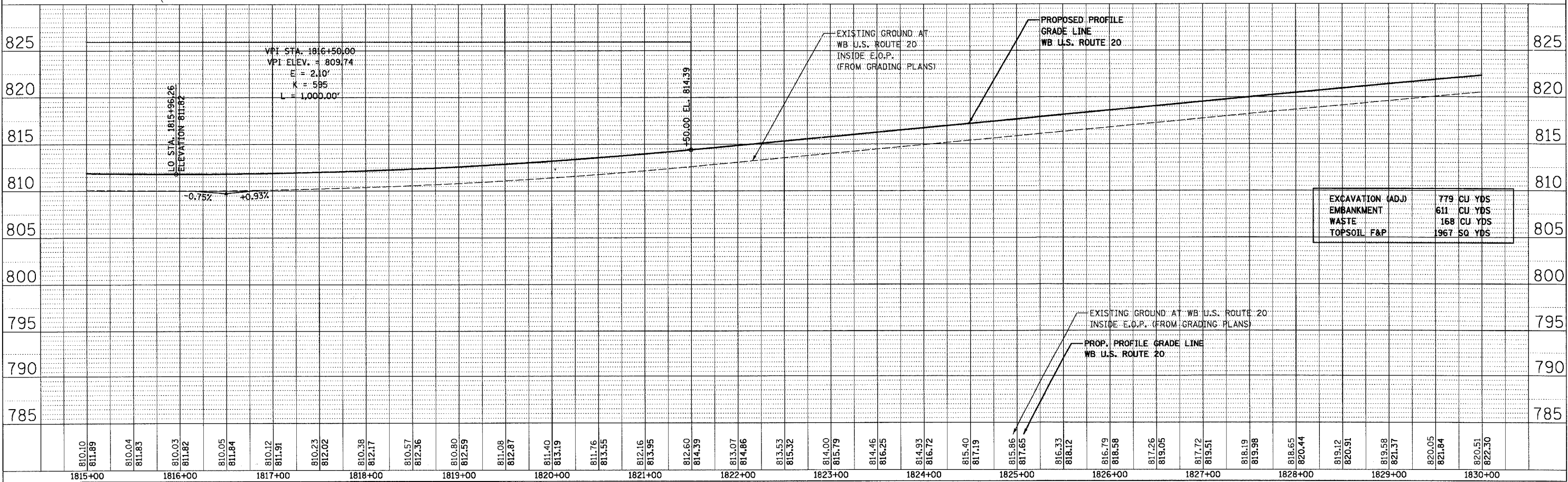
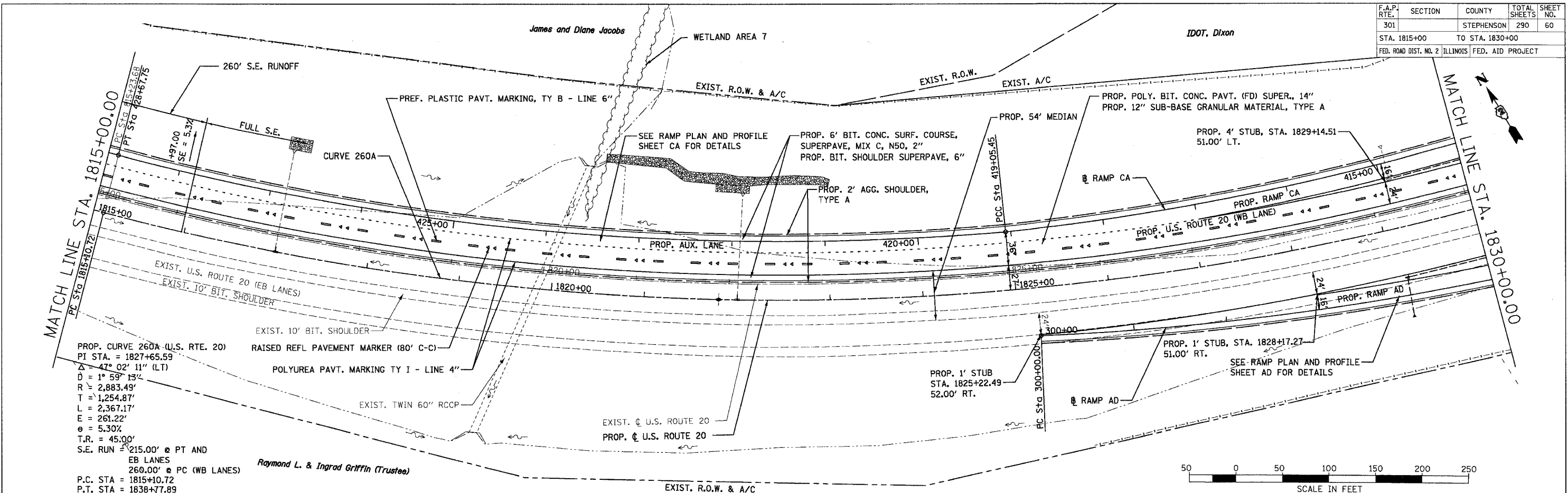


US ROUTE 20 (SHEET 9 OF 17)
 STA. 1800+00 TO STA. 1815+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	60
STA. 1815+00		TO STA. 1830+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

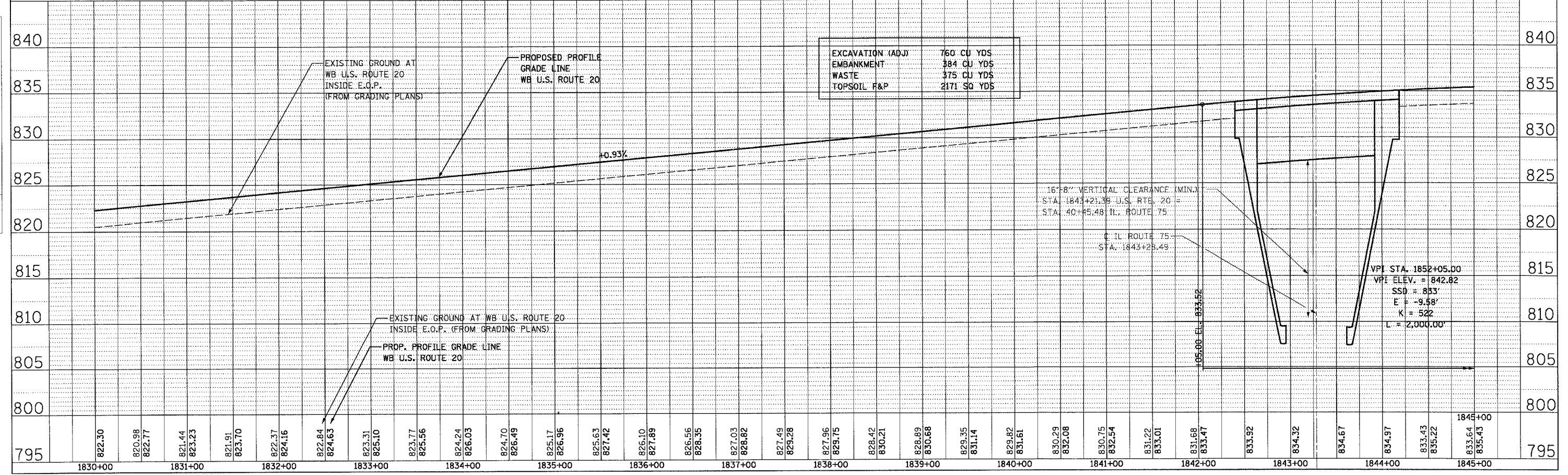
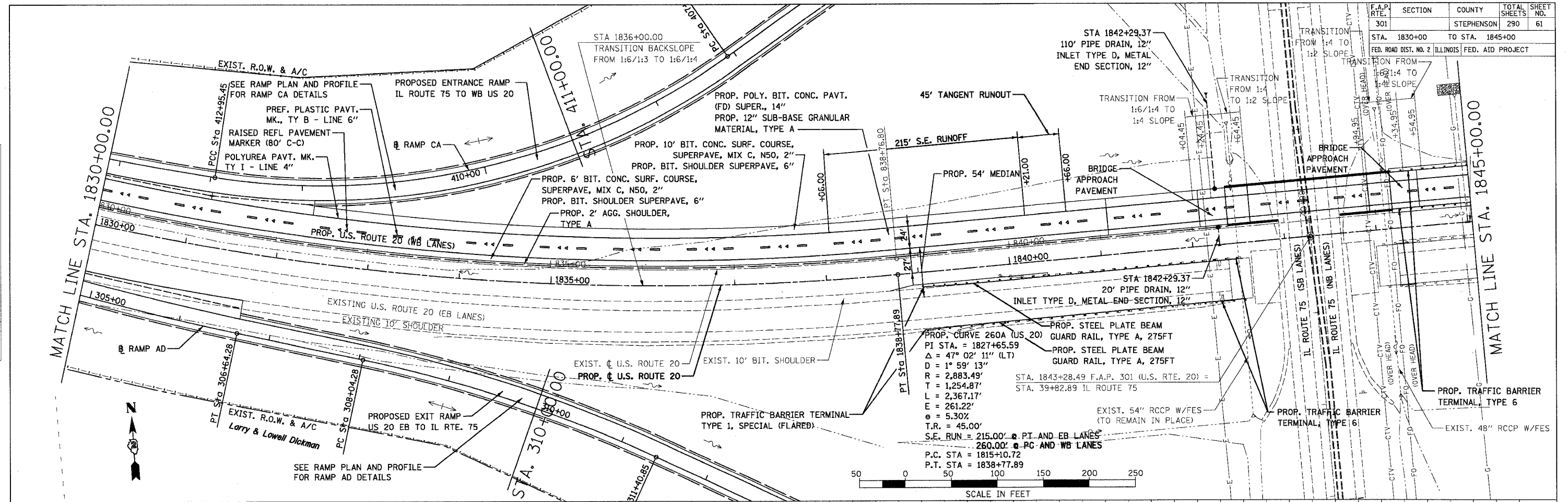
PLAN	SURVEYED	DATE
	ALIGNED	BY
	CHECKED	
	REVISION	
	NO. OF SHEETS	
	DATE	

PROFILE	SURVEYED	DATE
	ALIGNED	BY
	CHECKED	
	REVISION	
	NO. OF SHEETS	
	DATE	



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CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	61
STA. 1830+00		TO STA. 1845+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



US ROUTE 20 (SHEET 11 OF 17)
STA. 1830+00 TO STA. 1845+00

DATE	BY	DATE
PLAN	SURVEYED	CHECKED
	NOTE BOOK	ALTOUGHMENT
		FILED
		NO. OF DAYS CHECKED
		NO. OF DAYS FILED
		NO. OF DAYS

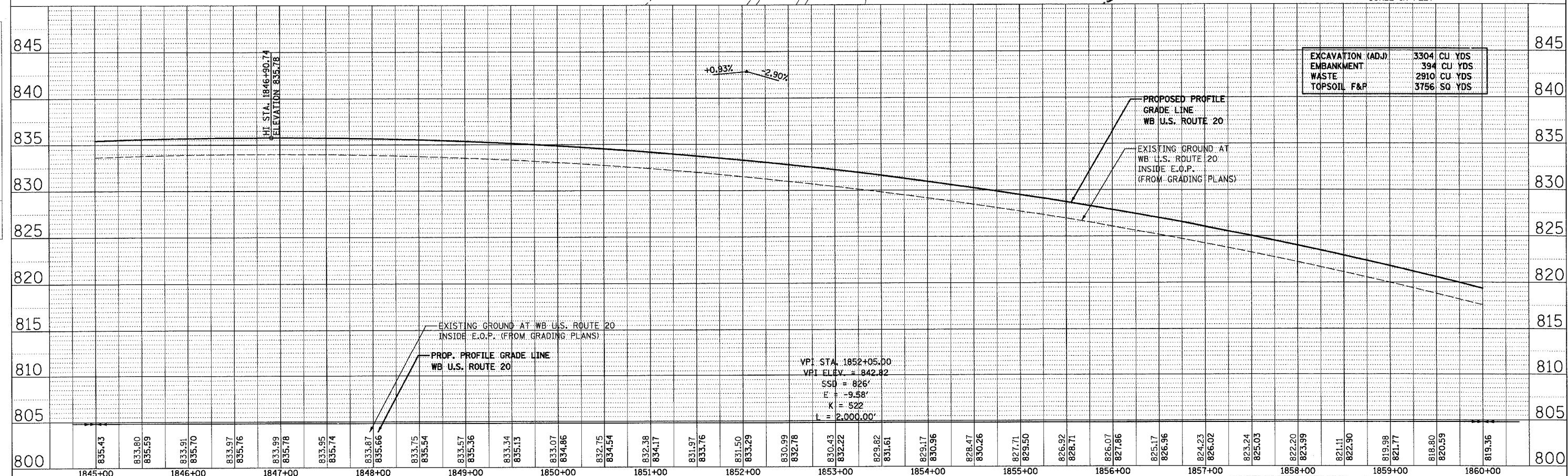
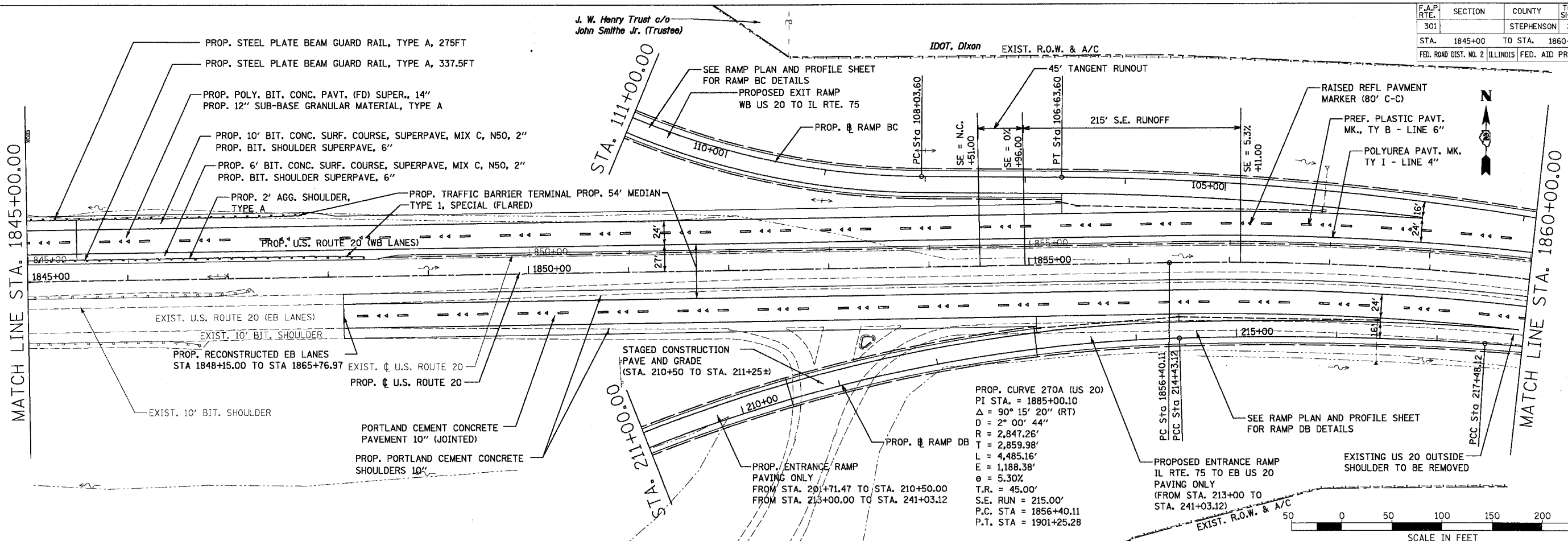
DATE	BY	DATE
PROFILE	SURVEYED	CHECKED
	NOTE BOOK	ALTOUGHMENT
		FILED
		NO. OF DAYS CHECKED
		NO. OF DAYS FILED
		NO. OF DAYS

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	62
STA. 1845+00 TO STA. 1860+00		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT		

PLAN	DATE
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	

PROFILE	DATE
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	

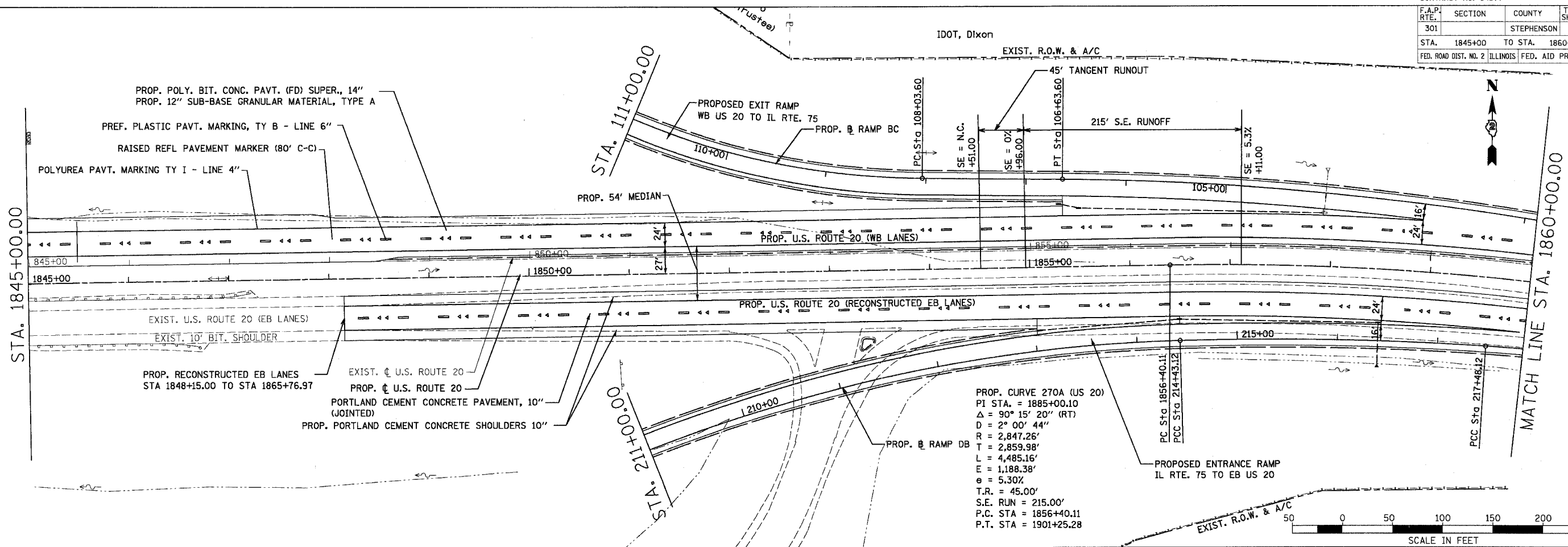


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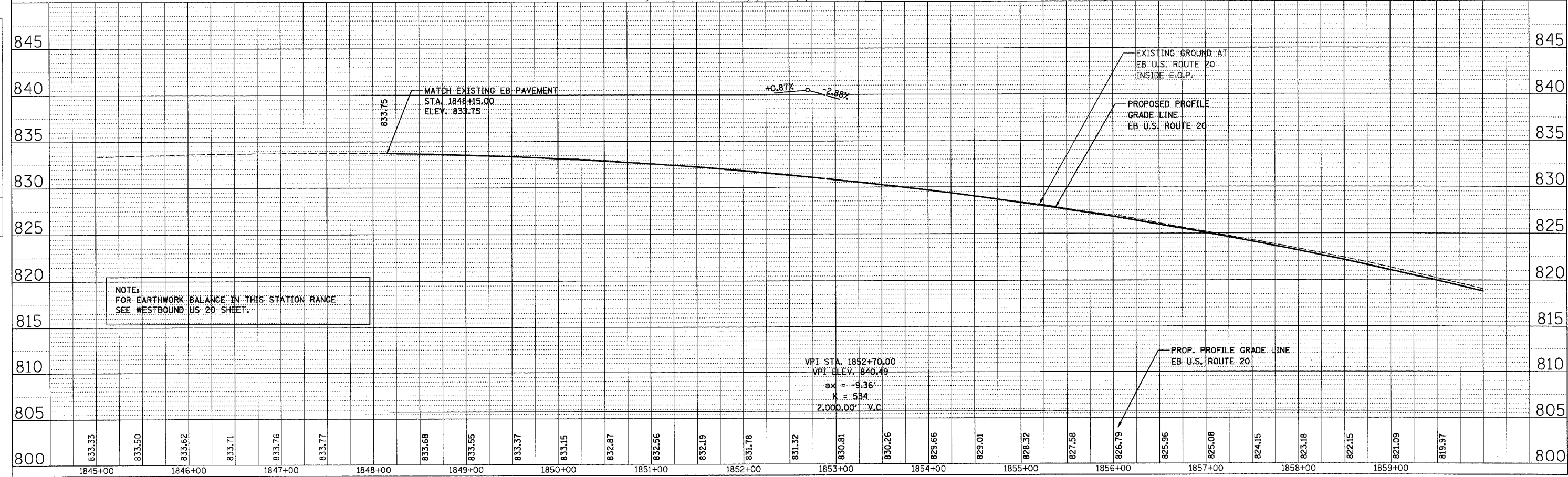
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	63
STA. 1845+00 TO STA. 1860+00				
FED. ROAD DIST. NO. 2 ILLINOIS		FED. AID PROJECT		

PLAN	SURVEYED	DATE
NOTE BOOK NO.	ALIGNED	
	CHECKED	
	BY	
	DATE	

PROFILE	SURVEYED	DATE
NOTE BOOK NO.	ALIGNED	
	CHECKED	
	BY	
	DATE	



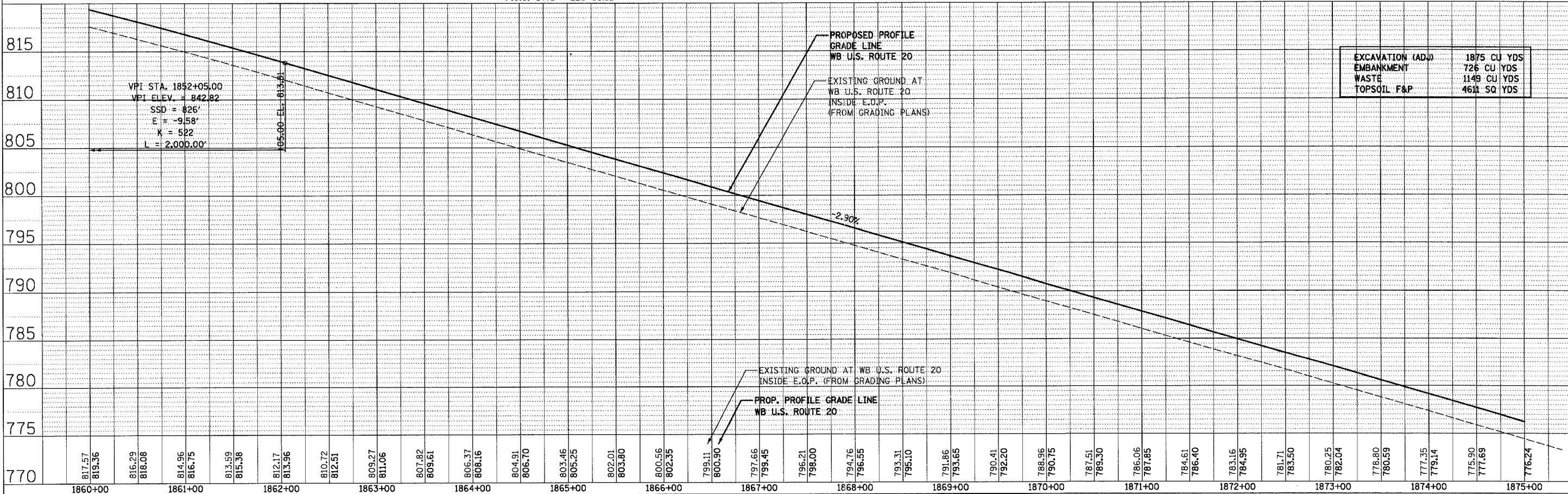
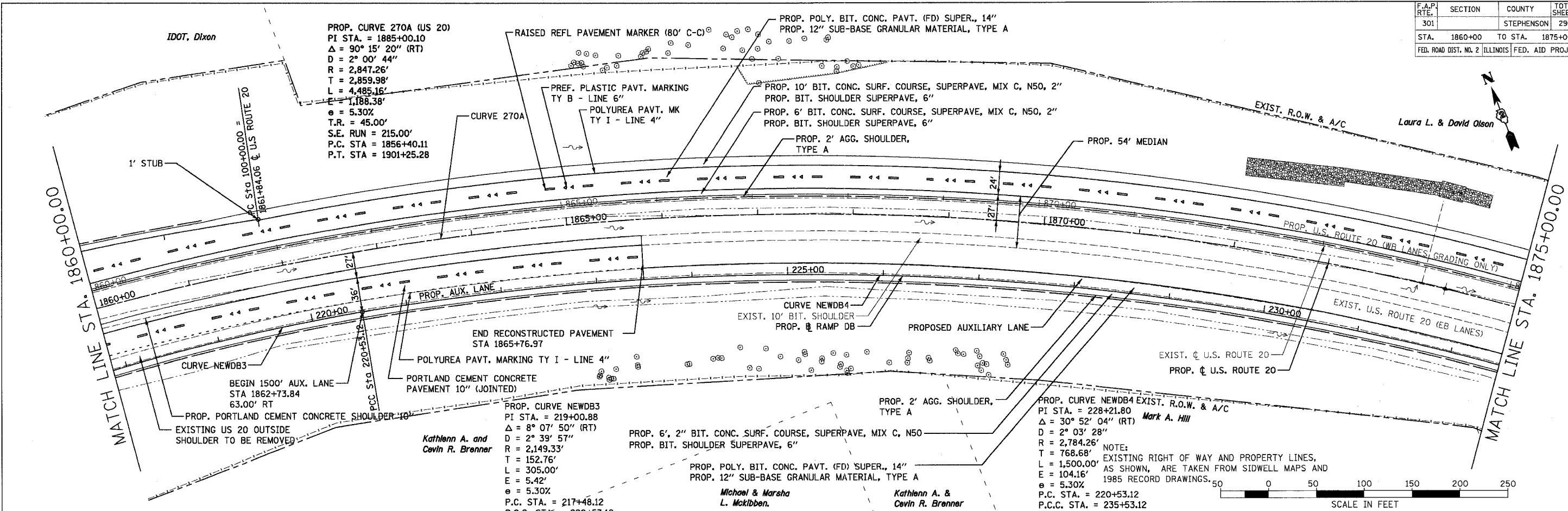
PROP. CURVE 270A (US 20)
 PI STA. = 1885+00.10
 $\Delta = 90^\circ 15' 20''$ (RT)
 $D = 2^\circ 00' 44''$
 $R = 2,847.26'$
 $T = 2,859.98'$
 $L = 4,485.16'$
 $E = 1,188.38'$
 $e = 5.30\%$
 $T.R. = 45.00'$
 $S.E. RUN = 215.00'$
 $P.C. STA = 1856+40.11$
 $P.T. STA = 1901+25.28$



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	64
STA. 1860+00	TO STA. 1875+00			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE	BY
DESIGNED		
CHECKED		
APPROVED		

PROFILE	DATE	BY
DESIGNED		
CHECKED		
APPROVED		

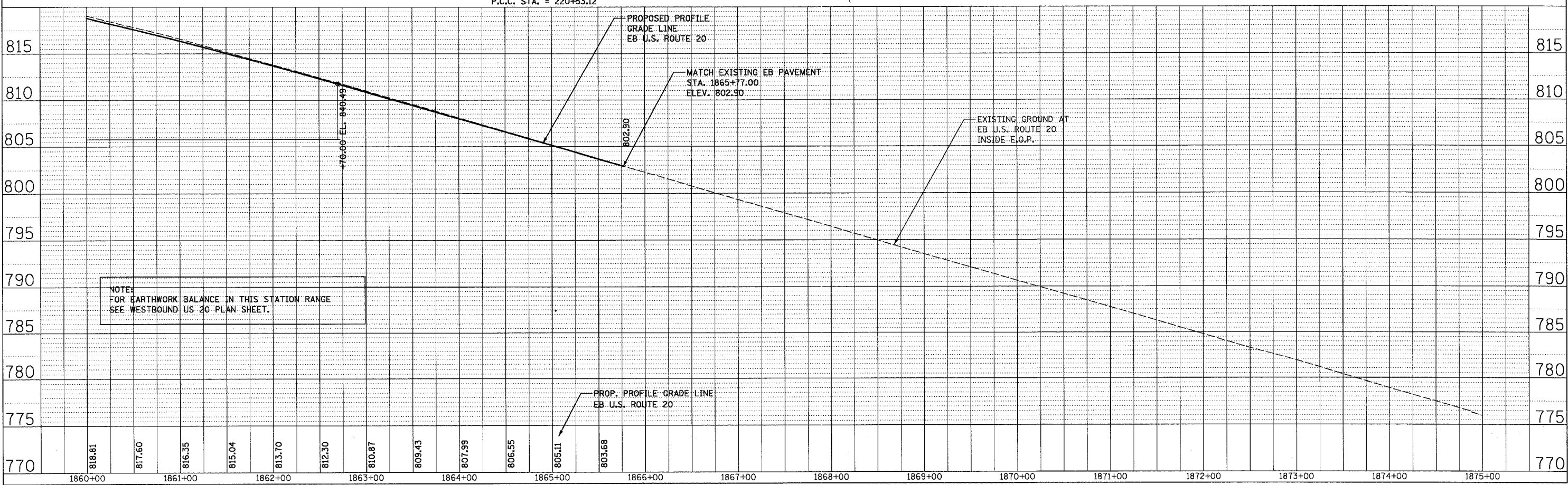
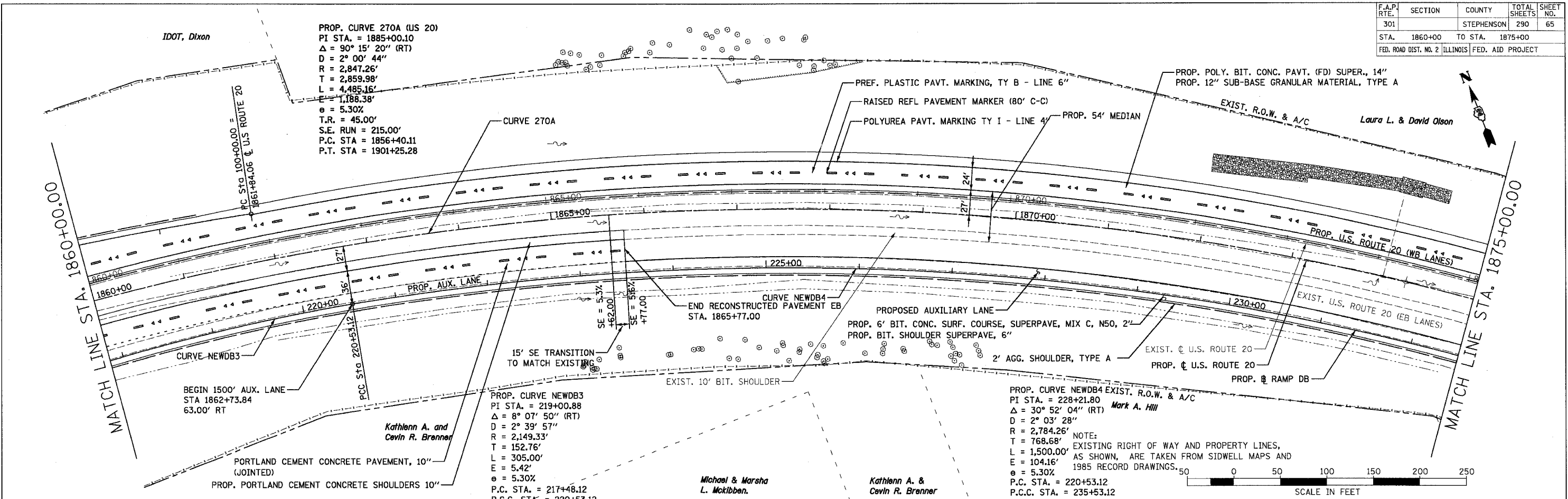


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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	65
STA. 1860+00 TO STA. 1875+00		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT		

PLAN	DATE	BY
SURVEYED		
DESIGNED		
CHECKED		
IN CHARGE		
NOTE BOOK NO.		
FIELD FILE NAME		

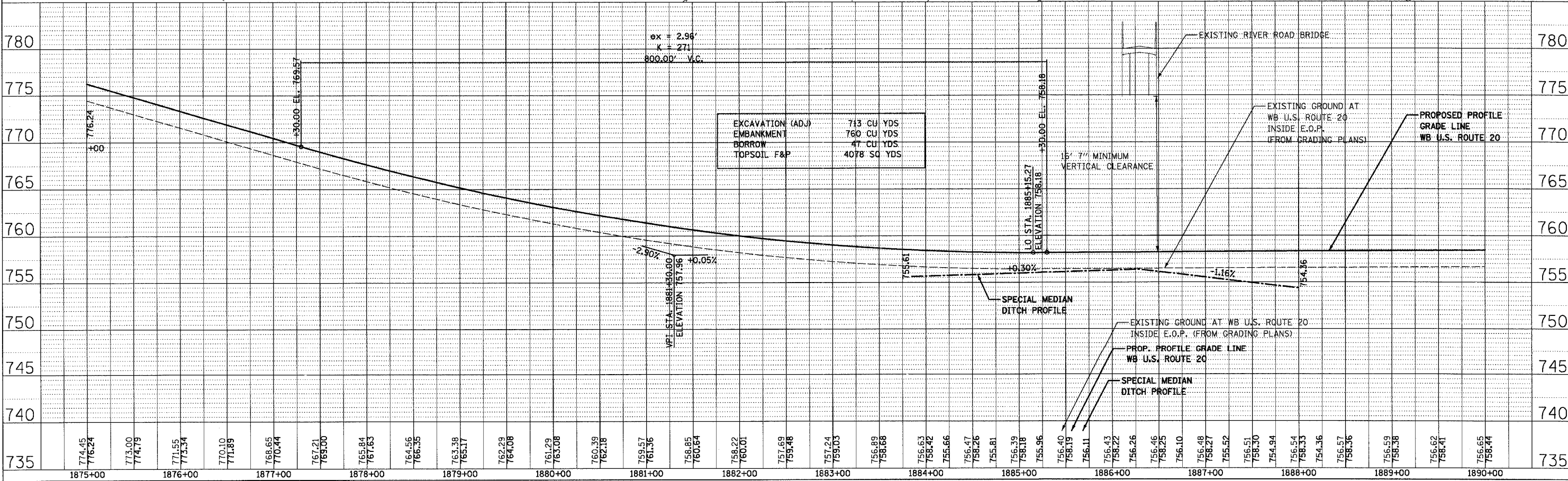
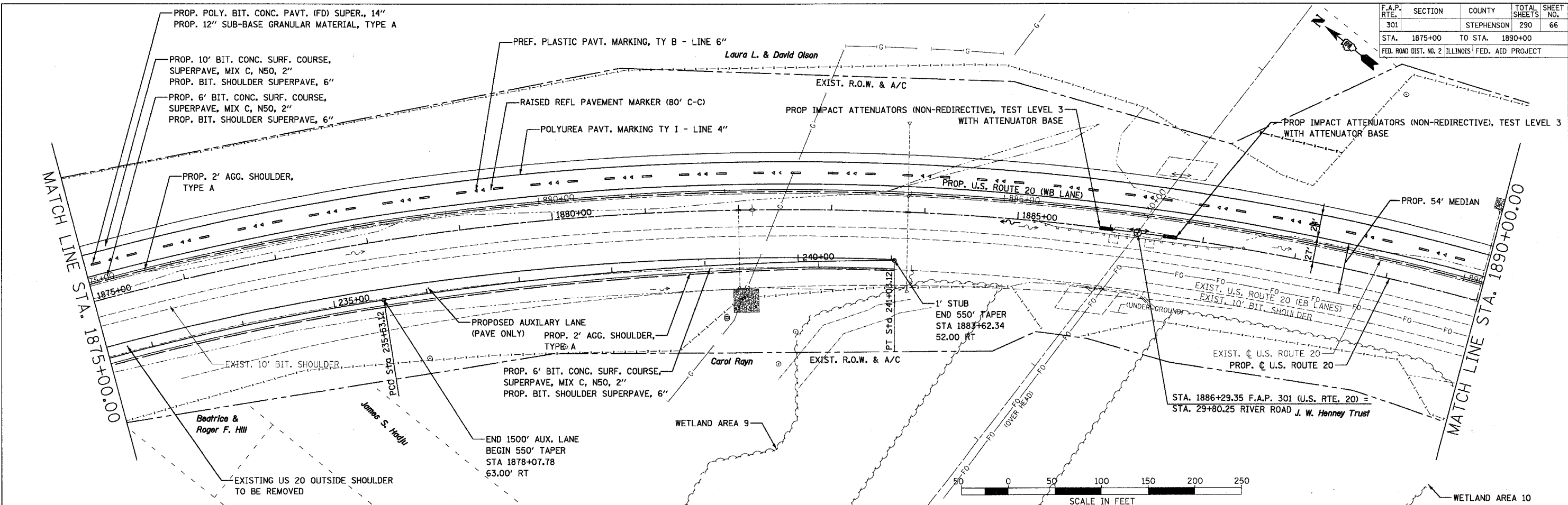
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SURVEYED		
DESIGNED		
CHECKED		
IN CHARGE		
NOTE BOOK NO.		
FIELD FILE NAME		



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	66
STA. 1875+00 TO STA. 1890+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

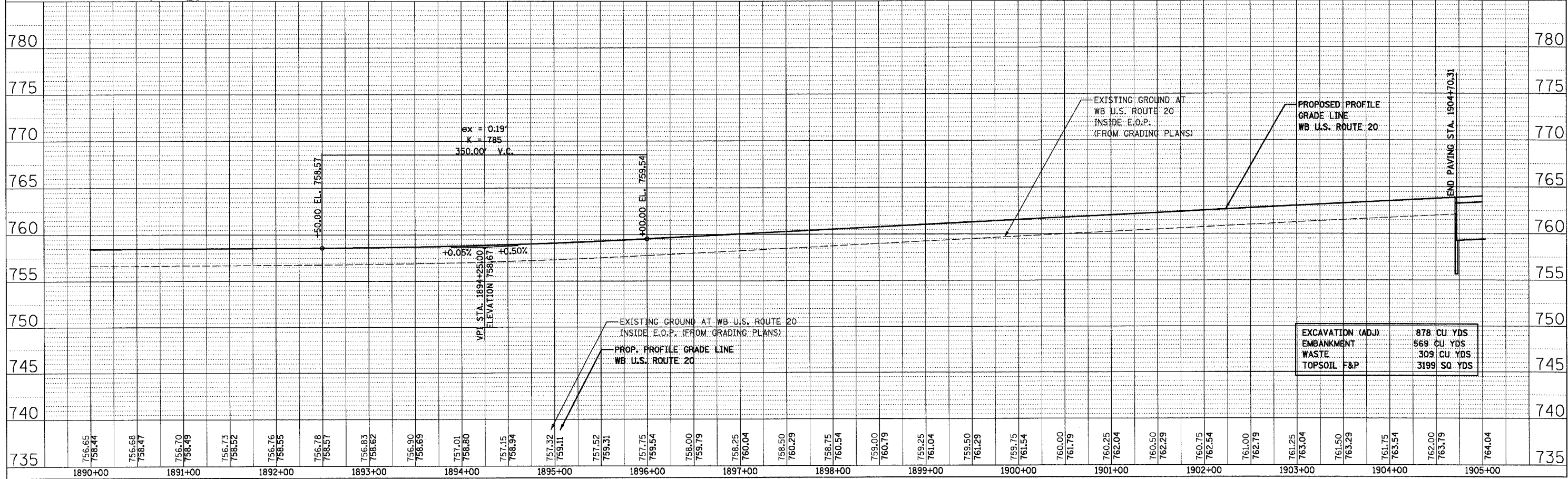
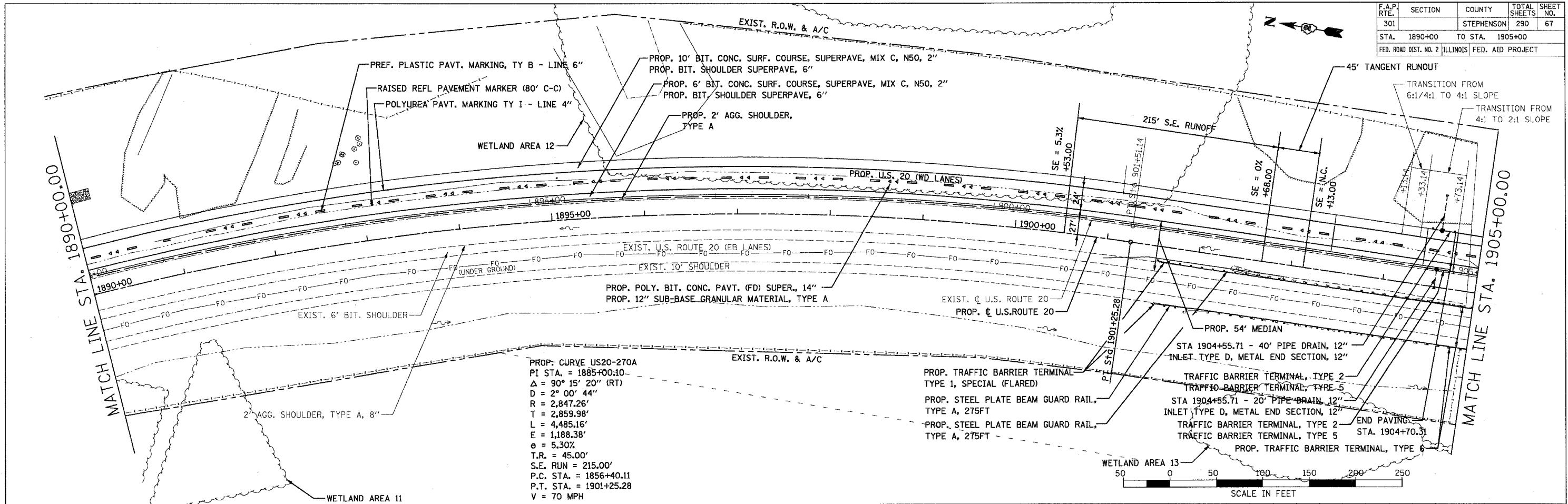
PLAN	SURVEYED	BY	DATE

PROFILE	SURVEYED	BY	DATE



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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	67
STA. 1890+00		TO STA. 1905+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

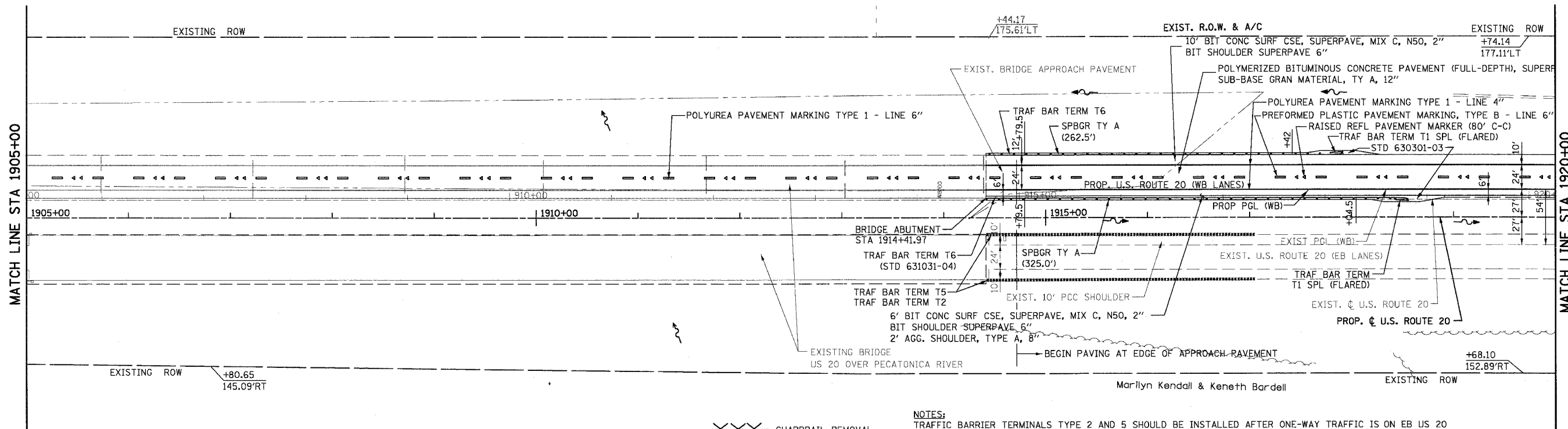


EXCAVATION (ADJ.)	878 CU YDS
EMBANKMENT	569 CU YDS
WASTE	309 CU YDS
TOPSOIL F&P	3199 SQ YDS

PLAN	DATE	BY
SURVEYED		
DESIGNED		
CHECKED		
IN CHARGE		
NOTE BOOK NO.		
PROJECT FILE NAME		

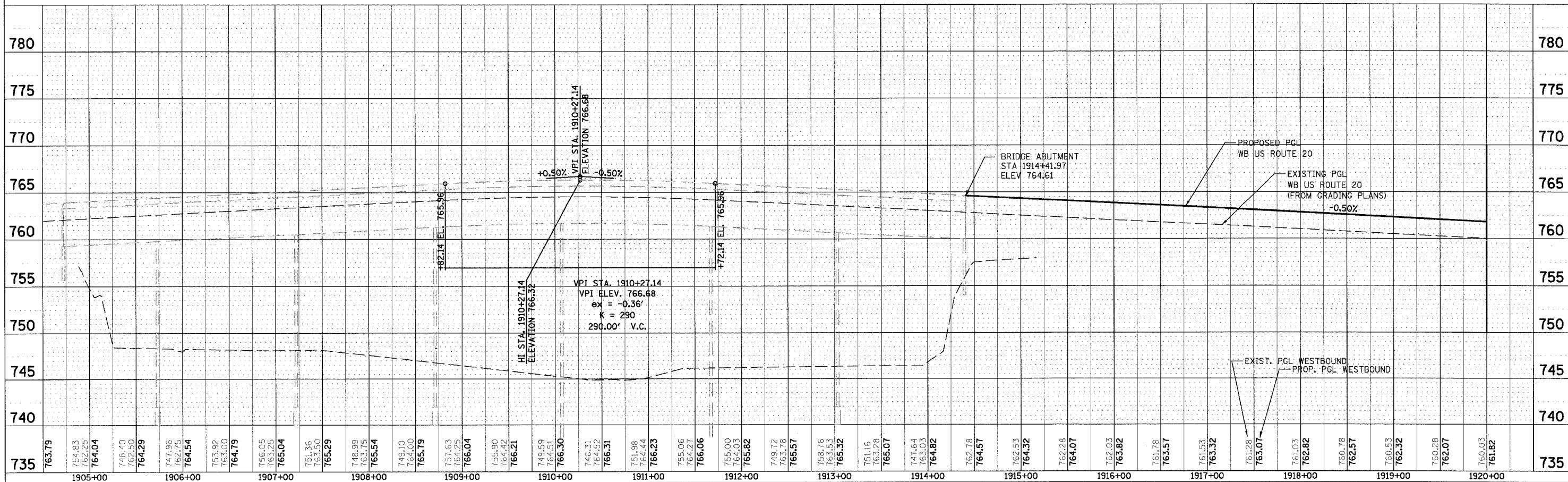
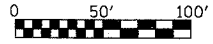
PROFILE	DATE	BY
SURVEYED		
DESIGNED		
CHECKED		
IN CHARGE		
NOTE BOOK NO.		
PROJECT FILE NAME		

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	68
STA. 1915+00		TO STA. 1920+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
• 177-13, 4, 4-1-1				



XXX GUARDRAIL REMOVAL

NOTES:
 TRAFFIC BARRIER TERMINALS TYPE 2 AND 5 SHOULD BE INSTALLED AFTER ONE-WAY TRAFFIC IS ON EB US 20
 THE CONTRACTOR SHALL NOT REMOVE THE EXISTING GUARDRAIL UNTIL THE WB LANES ARE OPEN TO TRAFFIC

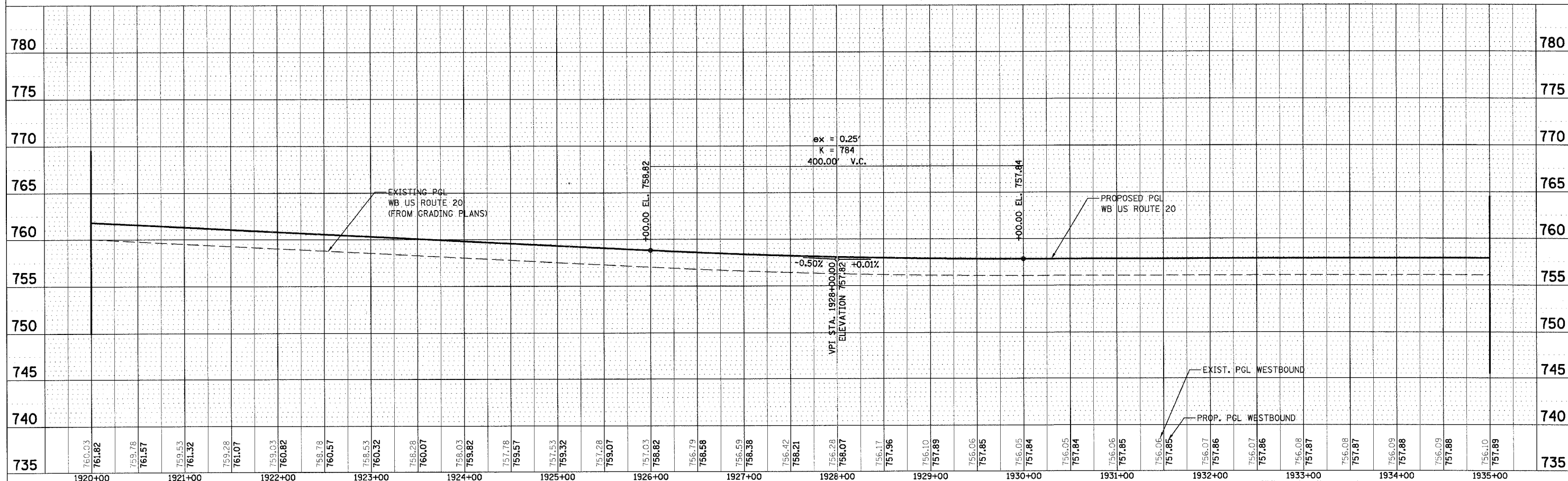
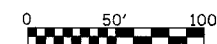
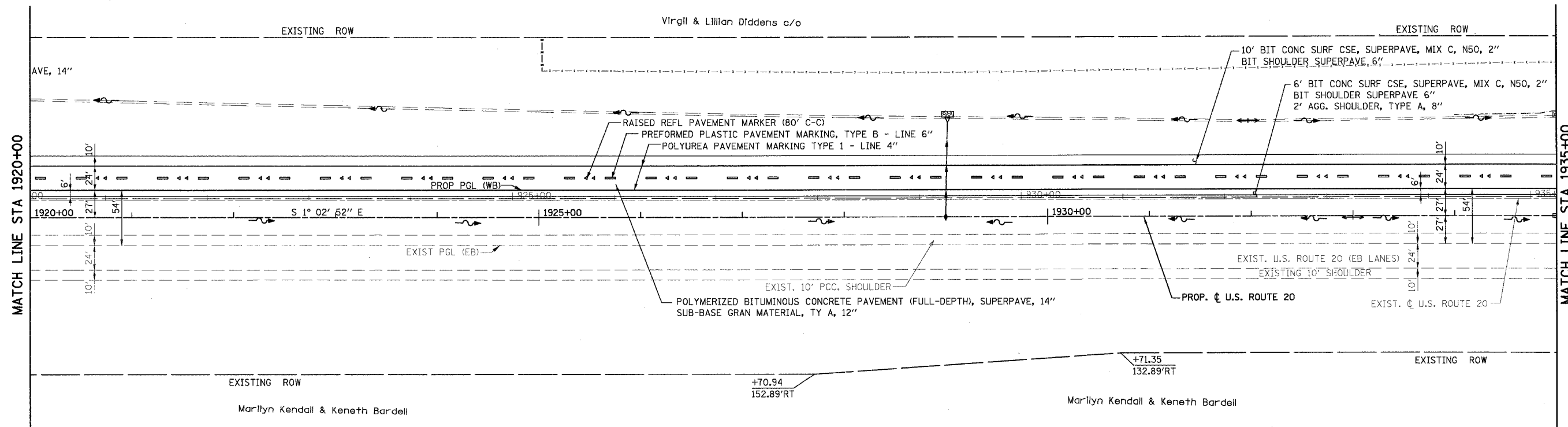
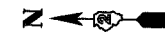


US ROUTE 20 PLAN & PROFILE
 STA 1914+41 TO STA 1920+00

PLAN	DATE
BY	
REVIEWED	
DATE	
NO.	
NAME	

PROFILE	DATE
BY	
REVIEWED	
DATE	
NO.	
NAME	

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	69
STA. 1920+00 TO STA. 1935+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
• 177-(3, 4, 4-1)-1				



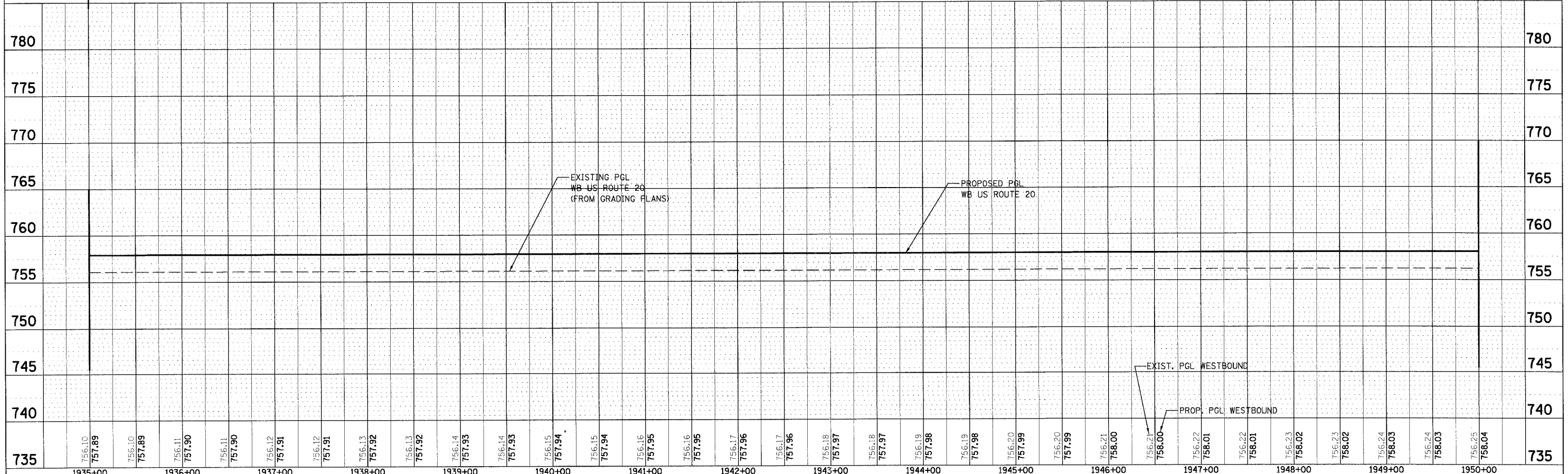
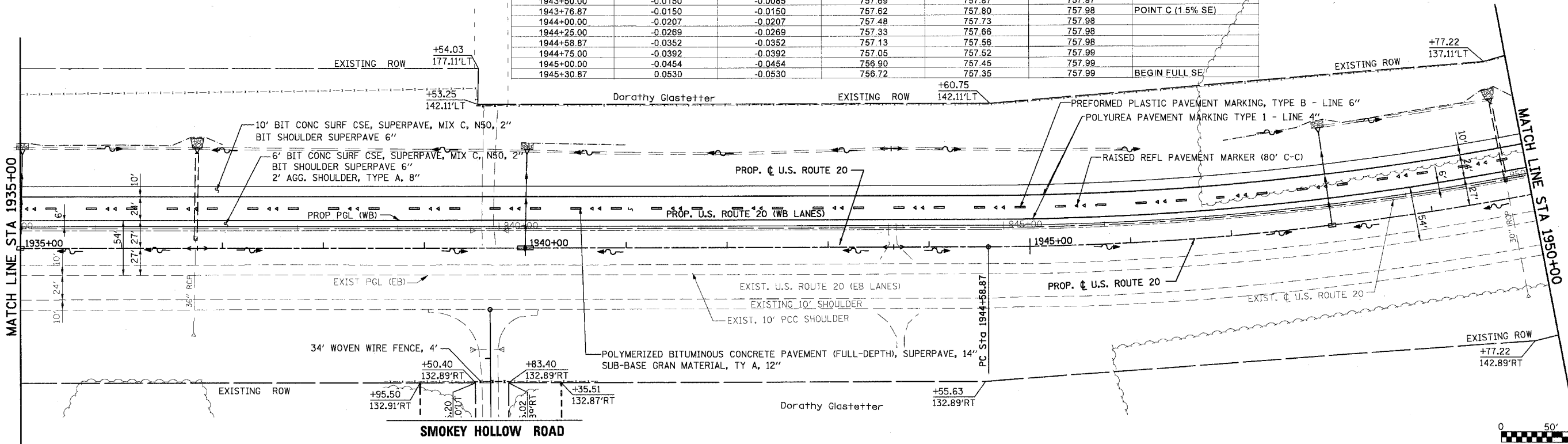
US ROUTE 20 PLAN & PROFILE
STA 1920+00 TO STA 1935+00

PLAN	STARTED	BY	DATE
NO. _____	NOTE BOOK _____	ALIGNED	CHECKED _____
		PAID FILE NAME	

PROFILE	STARTED	BY	DATE
NO. _____	NOTE BOOK _____	ALIGNED	CHECKED _____
		PAID FILE NAME	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	70
STA. 1935+00		TO STA. 1950+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
* 177-(3, 4, 4-1)-1				

STATION	OUTSIDE LANE CROSS SLOPE	INSIDE LANE CROSS SLOPE	OUTSIDE E.O.P. PAVT. ELEV. 51.00' LT	WB CENTERLINE PAVT. ELEV. 39.00' LT	INSIDE E.O.P. (PGL) PAVT. ELEV. 27.00' LT	REMARKS
1942+52.87	-0.0150	0.0150	757.96	758.14	757.96	END (NC)
1942+75.00	-0.0150	0.0096	757.91	758.09	757.97	
1943+00.00	-0.0150	0.0036	757.83	758.01	757.97	
1943+14.87	-0.0150	0.0000	757.79	757.97	757.97	0% SE
1943+25.00	-0.0150	-0.0025	757.76	757.94	757.97	
1943+50.00	-0.0150	-0.0085	757.69	757.87	757.97	
1943+76.87	-0.0150	-0.0150	757.62	757.80	757.98	POINT C (1.5% SE)
1944+00.00	-0.0207	-0.0207	757.48	757.73	757.98	
1944+25.00	-0.0269	-0.0269	757.33	757.66	757.98	
1944+58.87	-0.0352	-0.0352	757.13	757.56	757.98	
1944+75.00	-0.0392	-0.0392	757.05	757.52	757.99	
1945+00.00	-0.0454	-0.0454	756.90	757.45	757.99	
1945+30.87	0.0530	-0.0530	756.72	757.35	757.99	BEGIN FULL SE



US ROUTE 20 PLAN & PROFILE
STA 1935+00 TO STA 1950+00

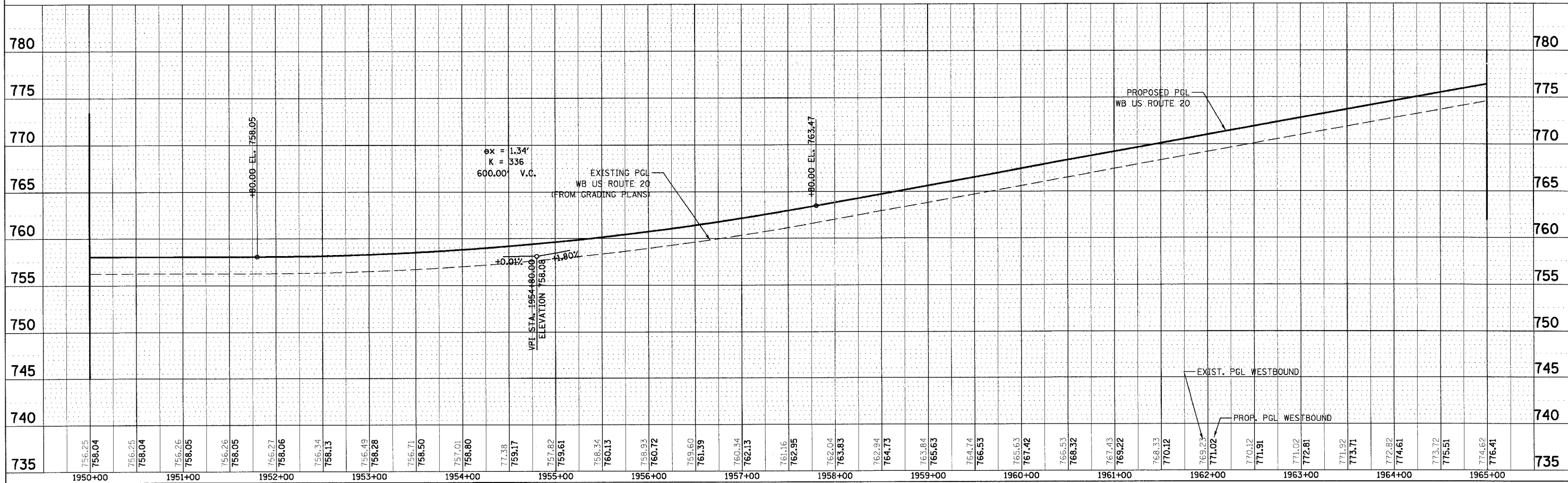
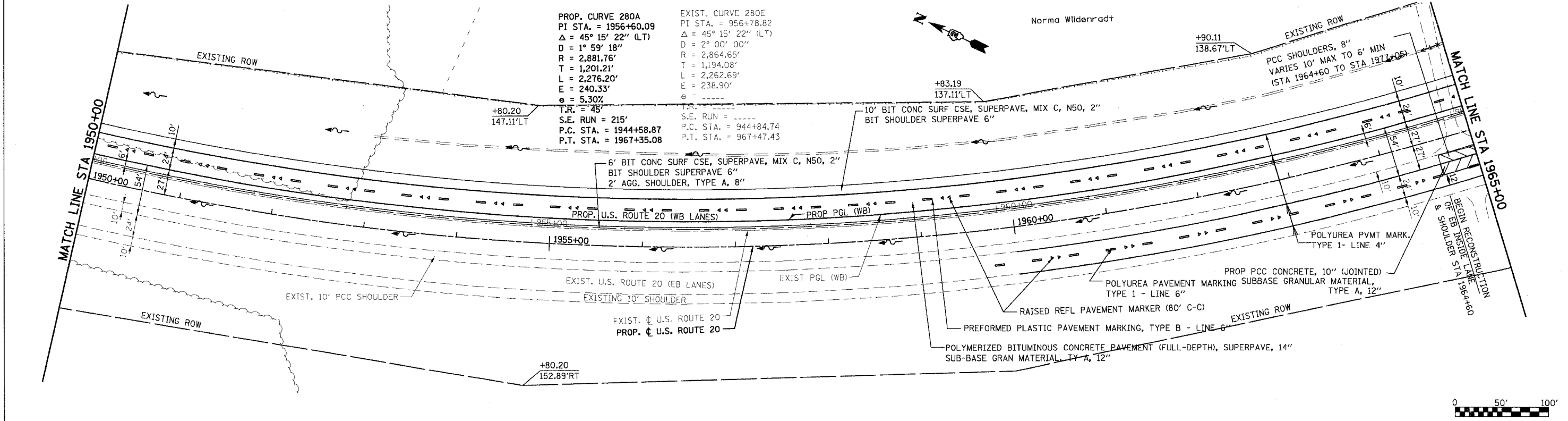
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 SURVEYED: _____
 PLOTTED: _____
 CHECKED: _____
 NOTE BOOK NO.: _____
 PLAN FILE NAME: _____

DATE: _____ BY: _____
 SURVEYED: _____
 PLOTTED: _____
 CHECKED: _____
 NOTE BOOK NO.: _____
 PROFILE FILE NAME: _____

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	71
STA. 1950+00		TO STA. 1965+00		
FED. ROAD DIST. NO. 2		ILLINOIS FED. AID PROJECT		
* 177-13, 4, 4-11-1				

PLAN	SURVEYED	DATE
	BY	
	NOTED	
	BY	
	DATE	
	BY	
	DATE	

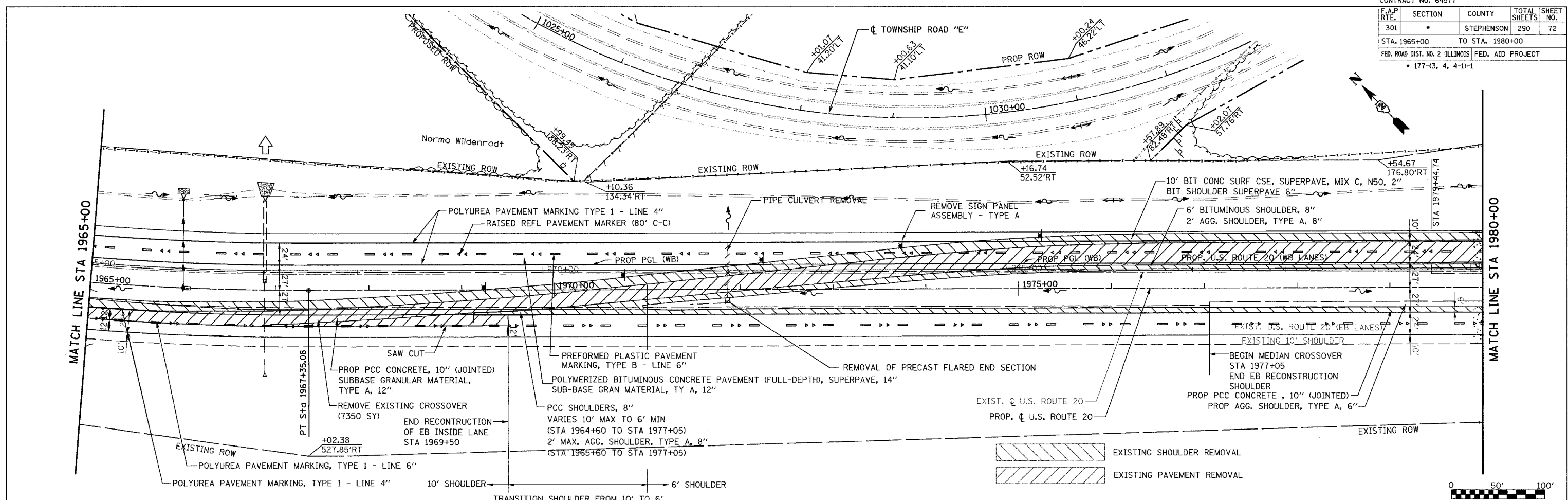
PROFILE	SURVEYED	DATE
	BY	
	NOTED	
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	DATE	
	BY	
	DATE	



US ROUTE 20 PLAN & PROFILE
 STA 1950+00 TO STA 1965+00

DATE
 BY
 SUBMITTED
 PLOTTED
 ALIGNED
 CHECKED
 FILED
 NO.

DATE
 BY
 PROFILE
 GRADES
 CHECKED
 STRUCTURE
 NOTATIONS
 CHKD
 NO.



STATION	OUTSIDE LANE CROSS SLOPE	INSIDE LANE CROSS SLOPE	PAV. ELEV.		INSIDE E.O.P. (PGL) PAVT. ELEV.	REMARKS
			OUTSIDE E.O.P. PAVT. ELEV.	WB CENTERLINE PAVT. ELEV.		
805	1966+63.08	-0.0530	678.07	678.70	679.34	END FULL SE
800	1966+75.00	-0.0501	678.35	678.95	679.55	
	1967+00.00	-0.0439	778.95	779.47	780.00	
795	1967+35.08	-0.0352	779.78	780.21	780.63	
	1967+50.00	-0.0316	780.14	780.52	780.90	
790	1967+75.00	-0.0254	780.74	781.05	781.35	
	1968+00.00	-0.0192	781.33	781.56	781.79	
785	1968+17.08	-0.0150	781.74	781.92	782.10	POINT C (1.5% SE)
	1968+50.00	-0.0150	782.43	782.61	782.69	
780	1968+79.00	-0.0150	783.03	783.21	783.21	0% SE
	1969+00.00	-0.0150	783.47	783.65	783.59	
775	1969+25.00	-0.0150	783.99	784.17	784.04	
	1969+41.08	-0.0150	784.33	784.51	784.33	BEGIN (NC)

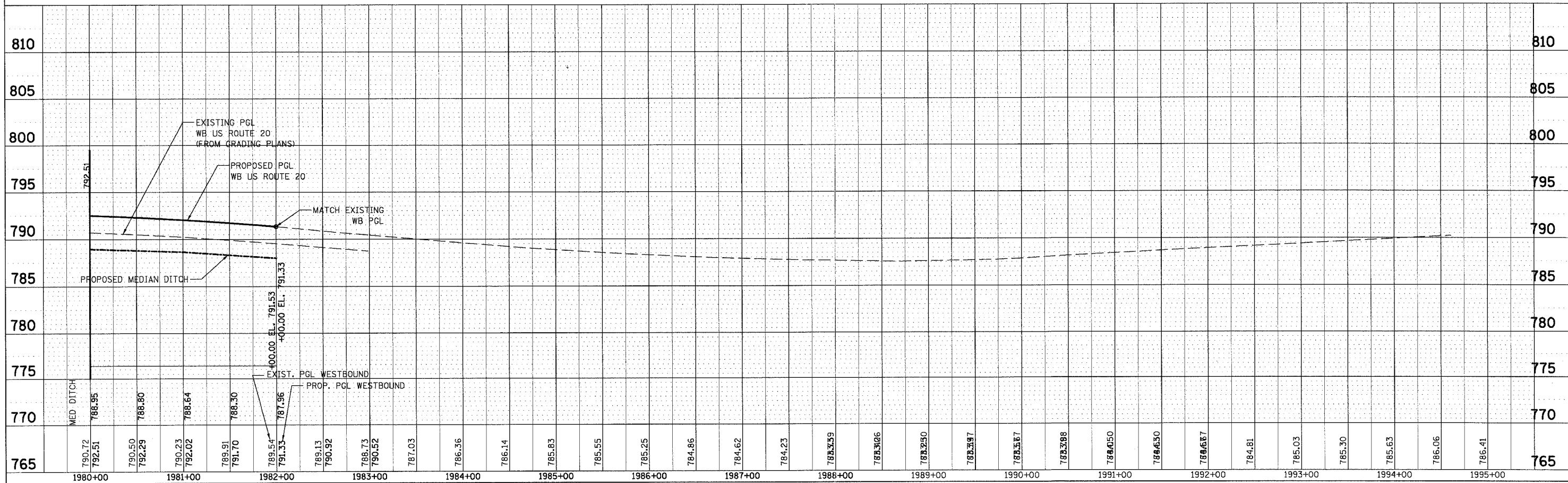
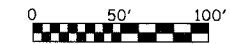
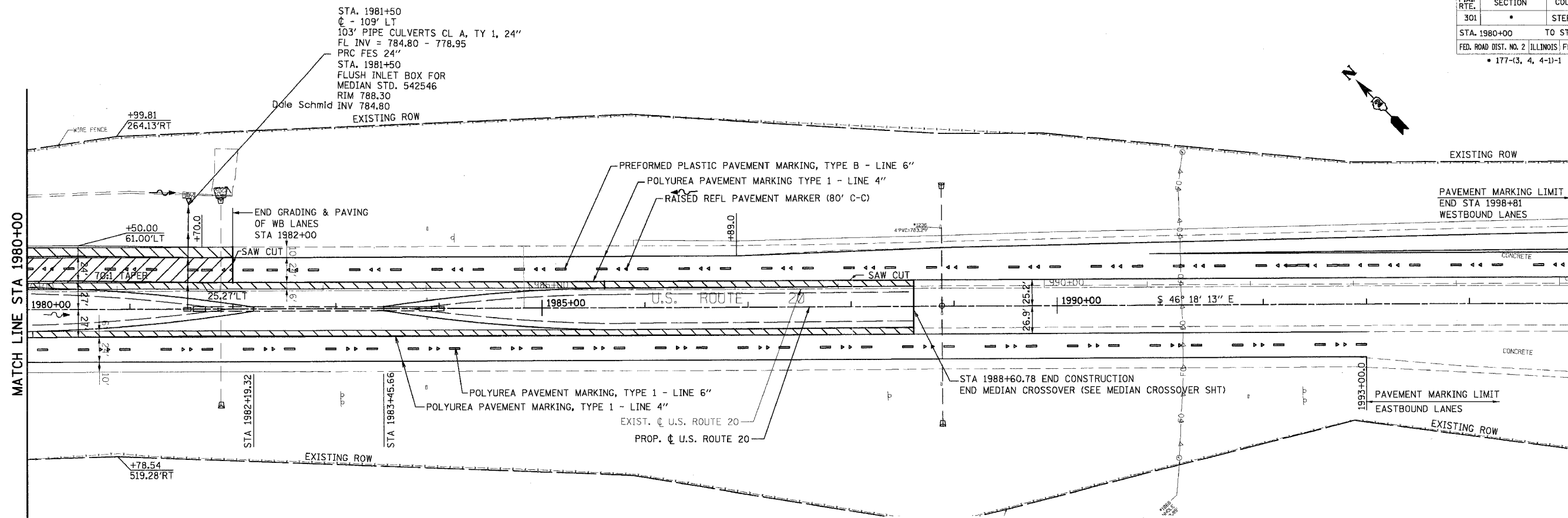
STATION	PROPOSED PGL WB US ROUTE 20	EXISTING PGL WB US ROUTE 20 (FROM GRADING PLANS)
1965+00	774.62	774.62
1966+00	776.41	776.41
1967+00	777.30	777.30
1968+00	778.20	778.20
1969+00	779.10	779.10
1970+00	780.00	780.00
1971+00	780.90	780.90
1972+00	781.80	781.80
1973+00	782.70	782.70
1974+00	783.60	783.60
1975+00	784.50	784.50
1976+00	785.40	785.40
1977+00	786.30	786.30
1978+00	787.20	787.20
1979+00	788.10	788.10
1980+00	789.00	789.00

US ROUTE 20 PLAN & PROFILE
 STA 1965+00 TO STA 1980+00

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	73
STA. 1980+00		TO STA. 1995+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
* 177-3, 4, 4-1-1				

PLAN	DATE
BY	
REVISIONS	
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	

PROFILE	DATE
BY	
REVISIONS	
NO. 1	
NO. 2	
NO. 3	
NO. 4	
NO. 5	

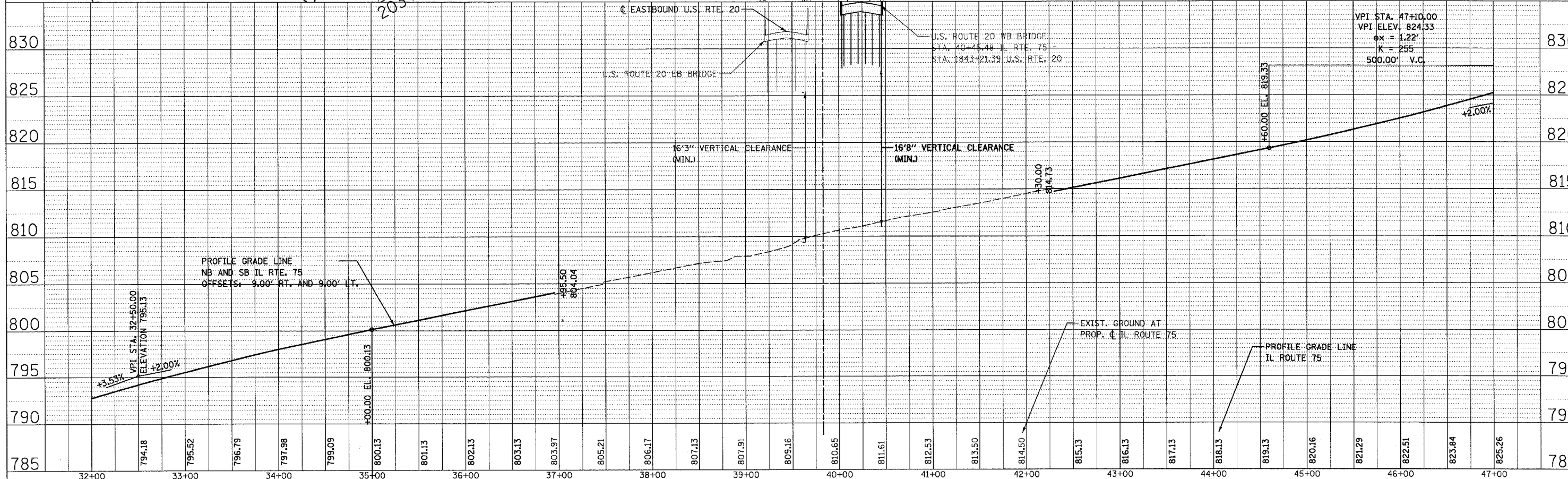
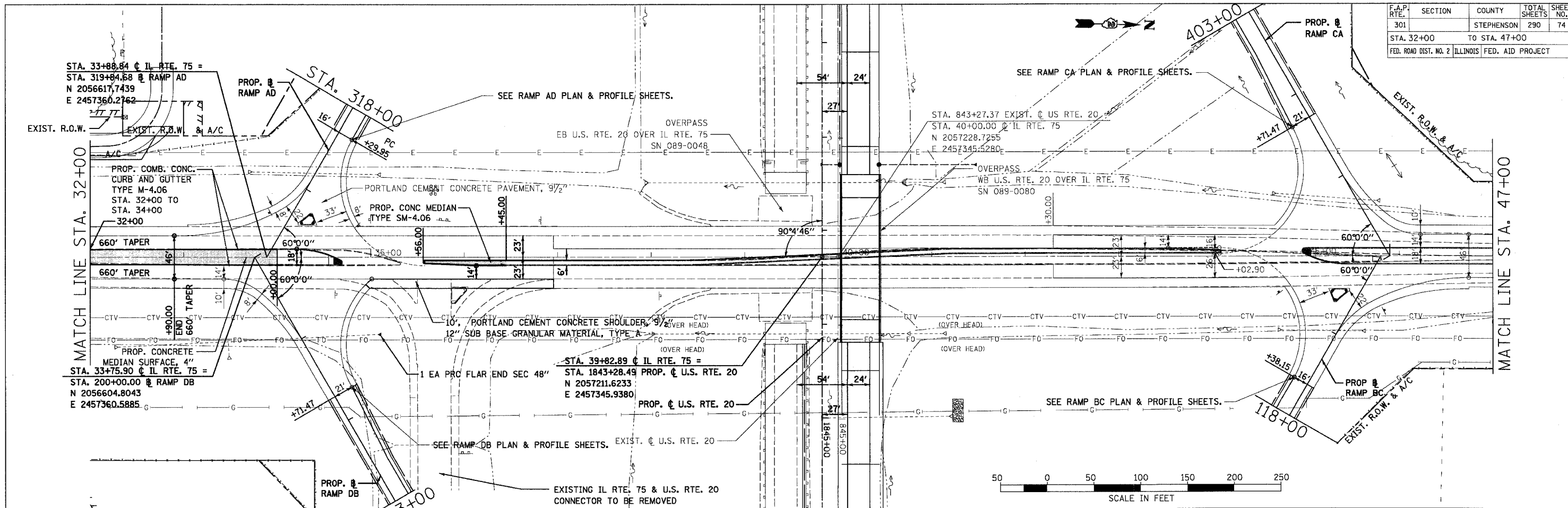


US ROUTE 20 PLAN & PROFILE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	74
STA. 32+00 TO STA. 47+00		ILLINOIS FED. AID PROJECT		

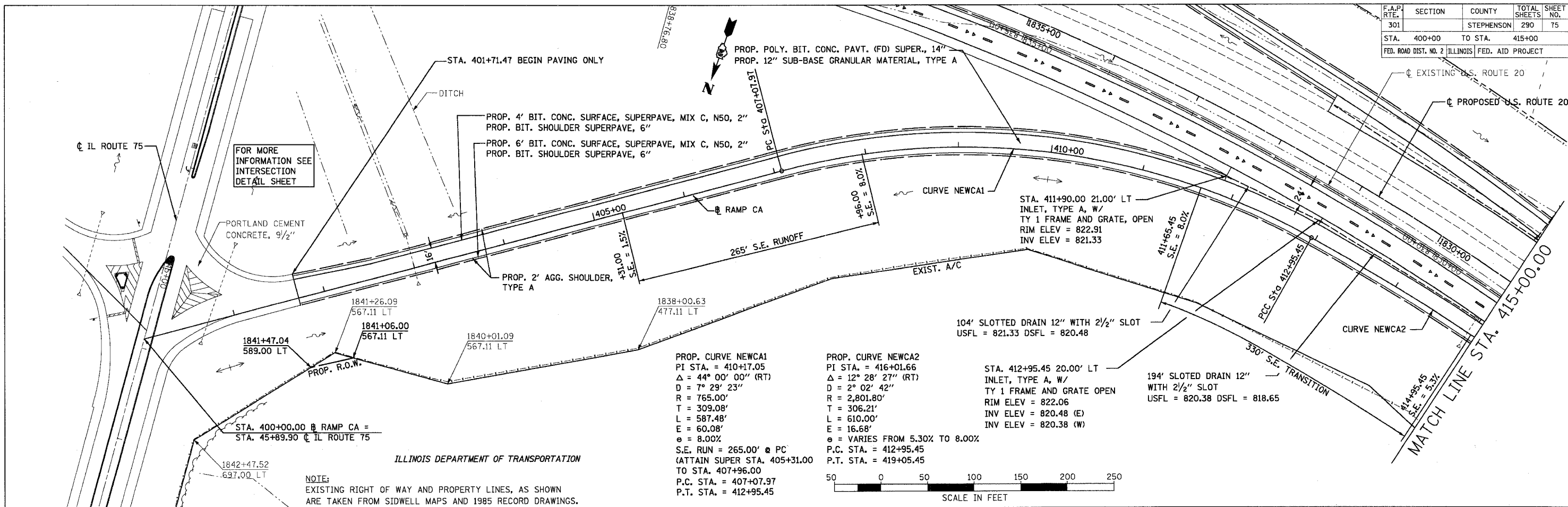
PLAN	DATE	BY
SURVEYED		
DESIGNED		
CHECKED		
IN CHARGE		
NOTE BOOK NO.		
FILE NAME		

PROF. ILE	DATE	BY
SURVEYED		
DESIGNED		
CHECKED		
IN CHARGE		
NOTE BOOK NO.		
STRUCTURE NOTATIONS		

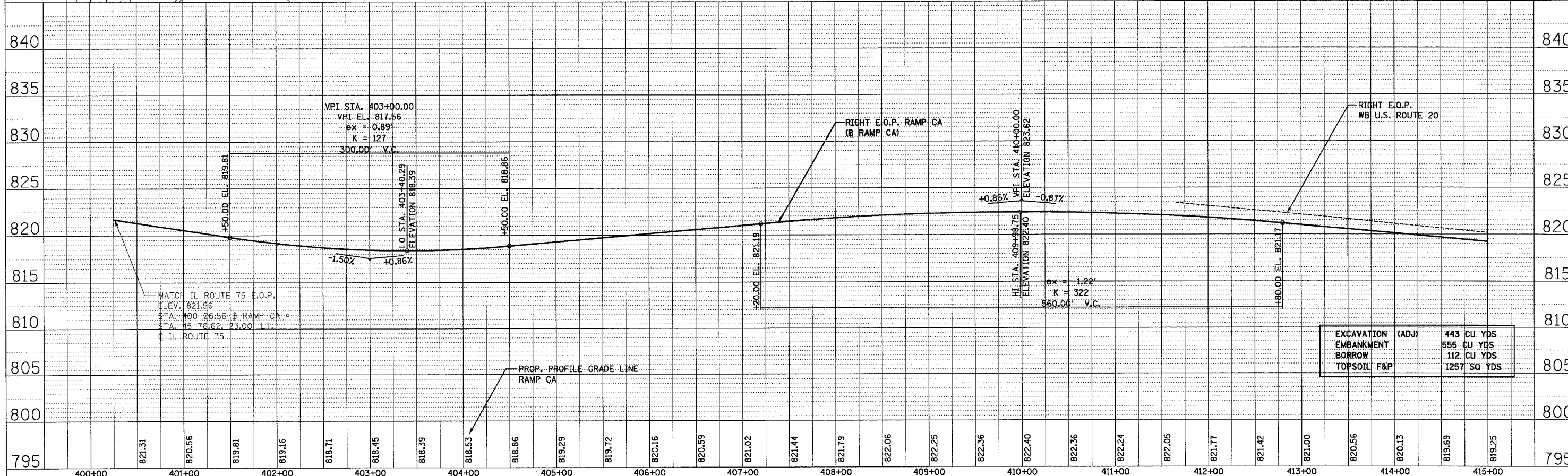


CONTRACT NO. 64577				
F.A.P. RITE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	75
STA. 400+00		TO STA. 415+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	SUBMITTED	DATE
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	



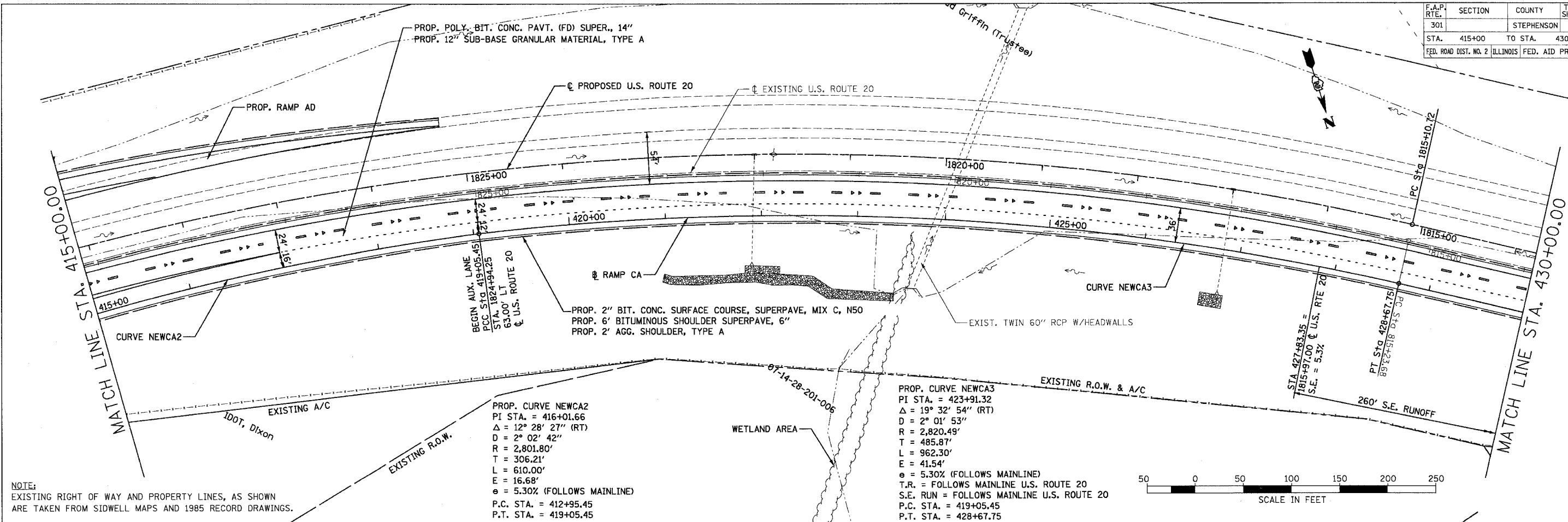
PROFILE	SUBMITTED	DATE
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	



RAMP CA (SHEET 1 OF 3)
STA. 400+00 TO STA. 415+00

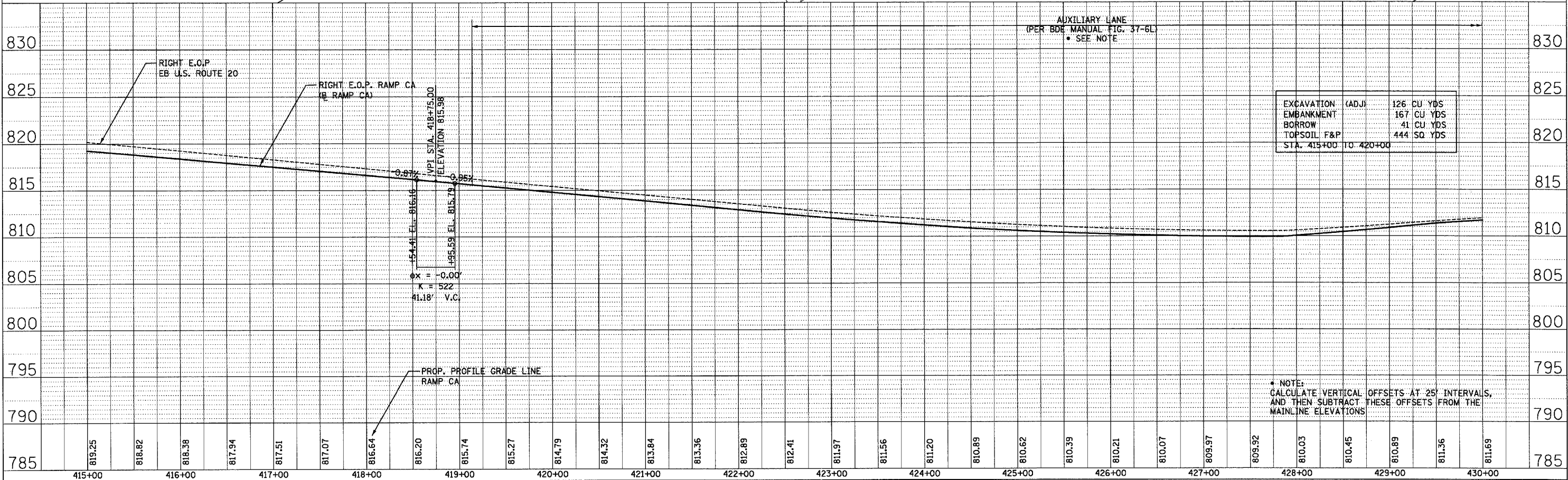
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	76
STA. 415+00	TO STA. 430+00			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE	BY
SURVEYED		
ALIGNED		
CHECKED		
NO. OF WAY CHECKED		
NO. OF FILE NAME		



NOTE:
EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN
ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.

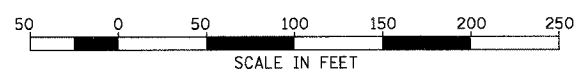
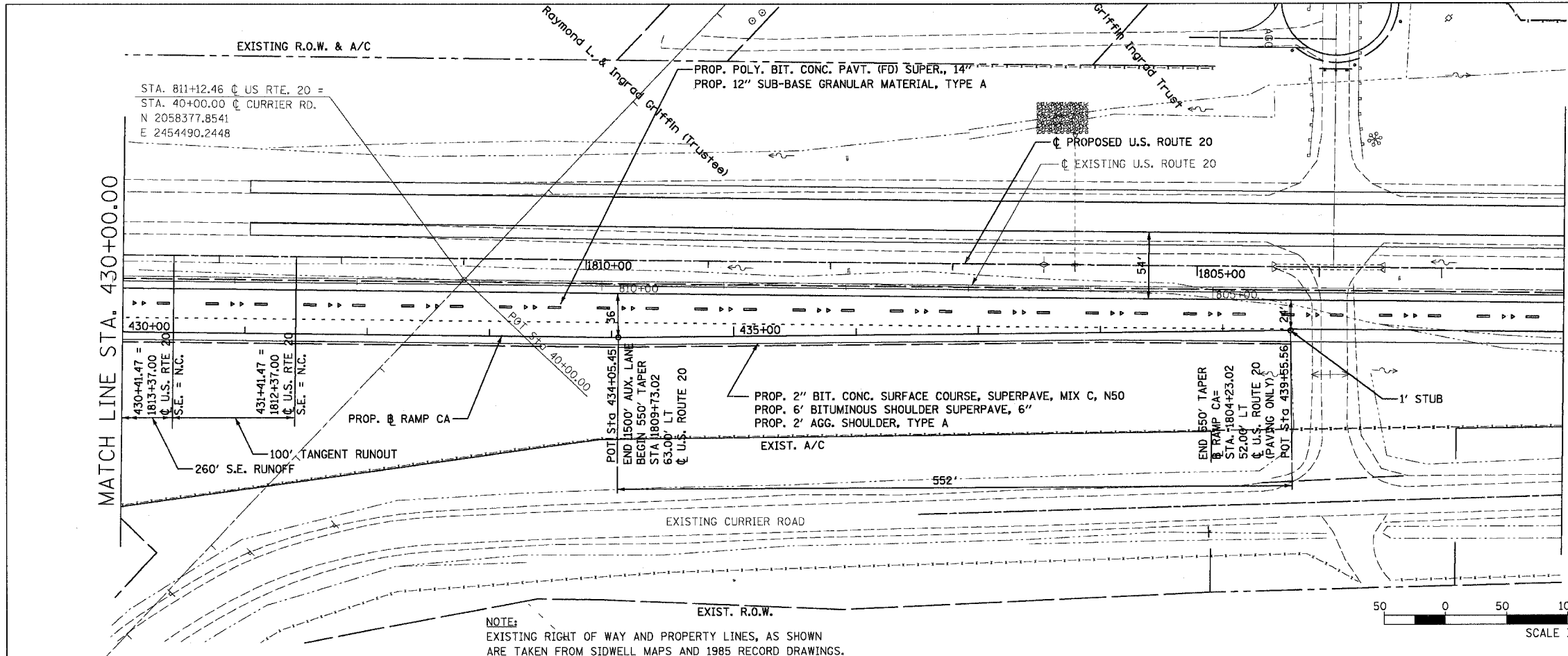
PROFILE	DATE	BY
SURVEYED		
GRADES CHECKED		
NO. OF NOTED		
STRUCTURE NOTATIONS CHECKED		



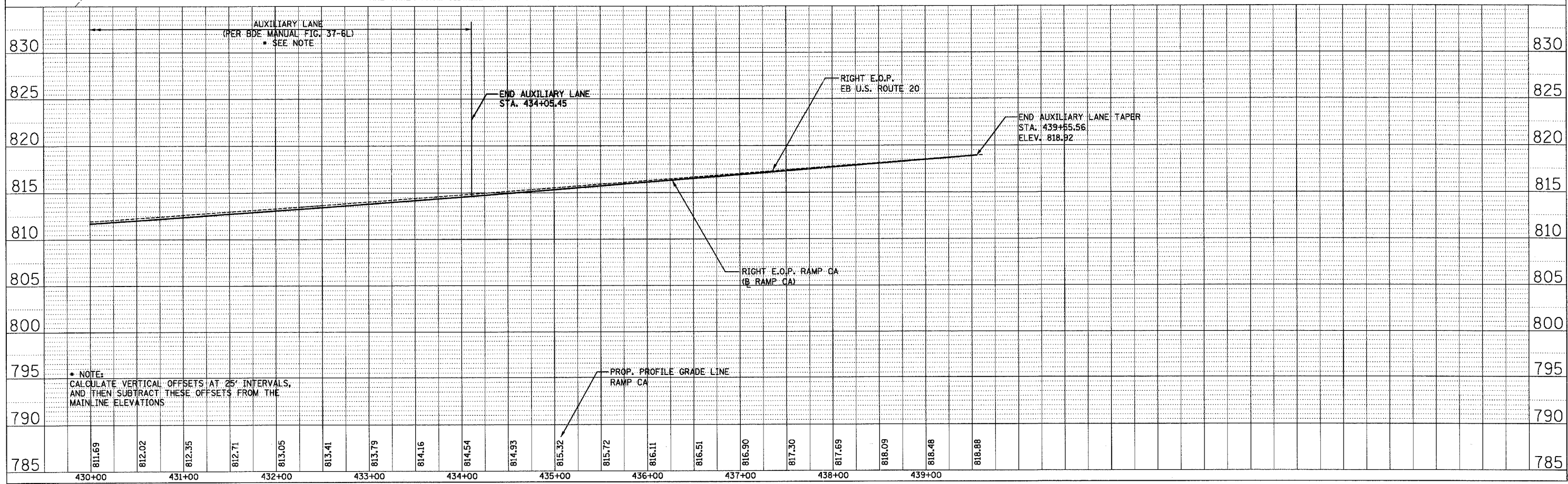
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	77
STA. 430+00 TO STA. 439+55.56		FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
	ALIGNED		
	CHECKED		
	NO.		

PROF. ILE	SURVEYED	BY	DATE
	GRADES		
	CHECKED		
	NO.		

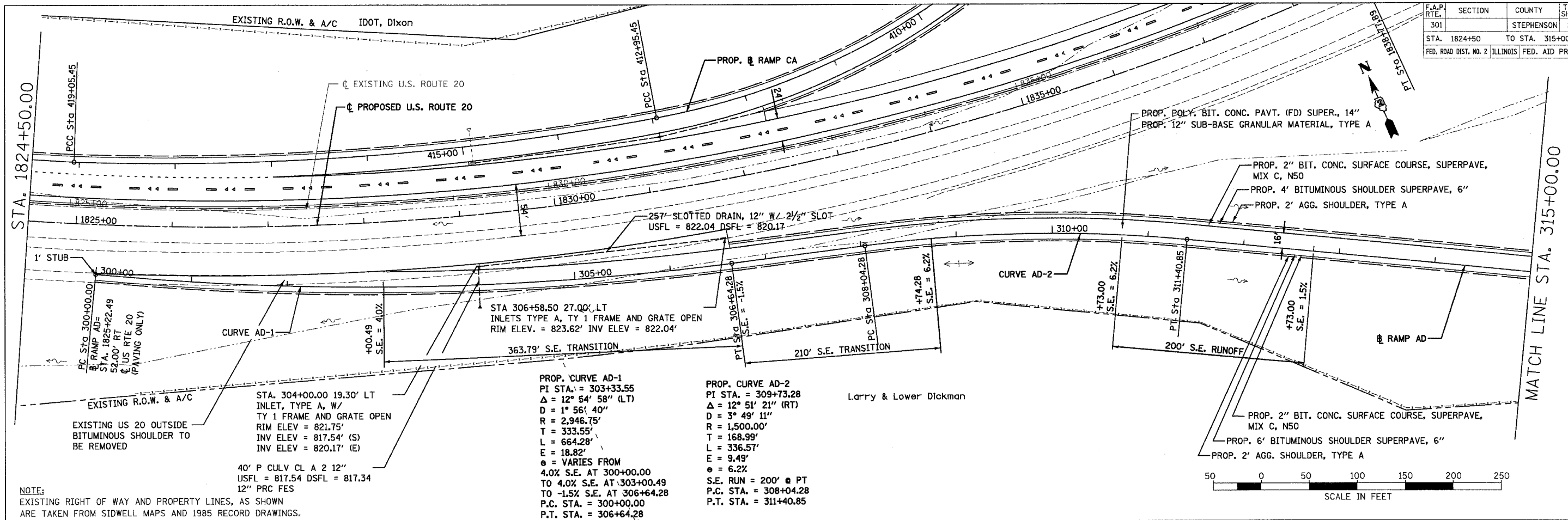


NOTE:
EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN
ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.



* NOTE:
CALCULATE VERTICAL OFFSETS AT 25' INTERVALS,
AND THEN SUBTRACT THESE OFFSETS FROM THE
MAIN LINE ELEVATIONS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	78
STA. 1824+50 TO STA. 315+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

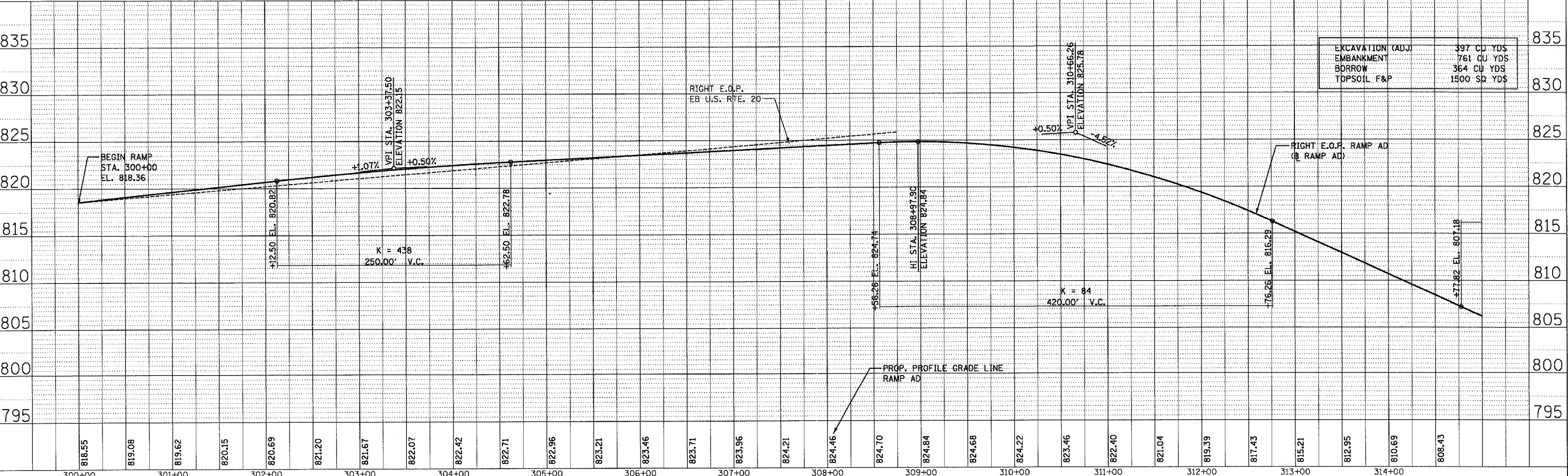


DATE	
BY	
REVISION	
NO.	
DESCRIPTION	

DATE	
BY	
REVISION	
NO.	
DESCRIPTION	

PROP. CURVE AD-1
 PI STA. = 303+33.55
 $\Delta = 12^\circ 54' 58''$ (LT)
 $D = 1^\circ 56' 40''$
 $R = 2,946.75'$
 $T = 333.55'$
 $L = 664.28'$
 $E = 18.82'$
 $\theta = \text{VARIES FROM } 4.0\% \text{ S.E. AT } 300+00.00 \text{ TO } 4.0\% \text{ S.E. AT } 303+00.49 \text{ TO } -1.5\% \text{ S.E. AT } 306+64.28$
 P.C. STA. = 300+00.00
 P.T. STA. = 306+64.28

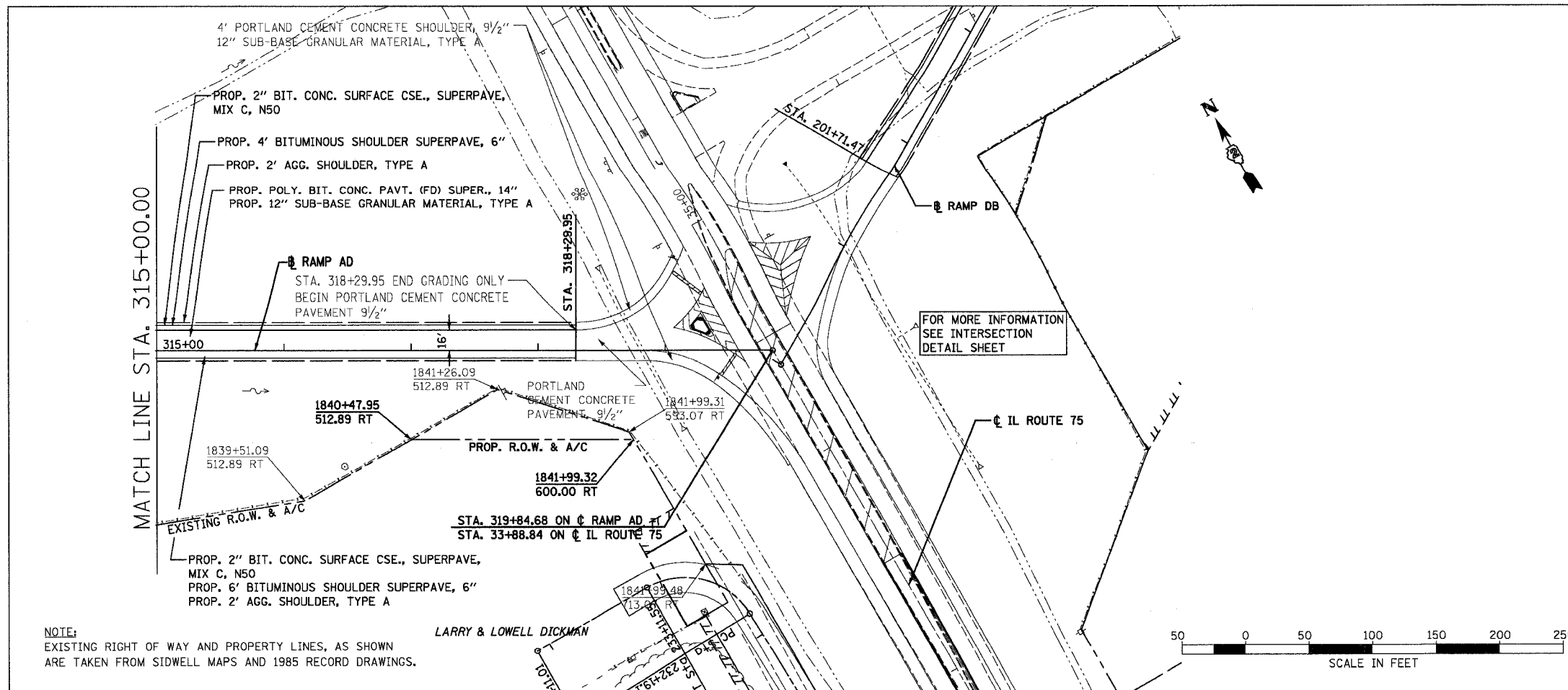
PROP. CURVE AD-2
 PI STA. = 309+73.28
 $\Delta = 12^\circ 51' 21''$ (RT)
 $D = 3^\circ 49' 11''$
 $R = 1,500.00'$
 $T = 168.99'$
 $L = 336.57'$
 $E = 9.49'$
 $\theta = 6.2\%$
 $\text{S.E. RUN} = 200' @ \text{ PT}$
 P.C. STA. = 308+04.28
 P.T. STA. = 311+40.85



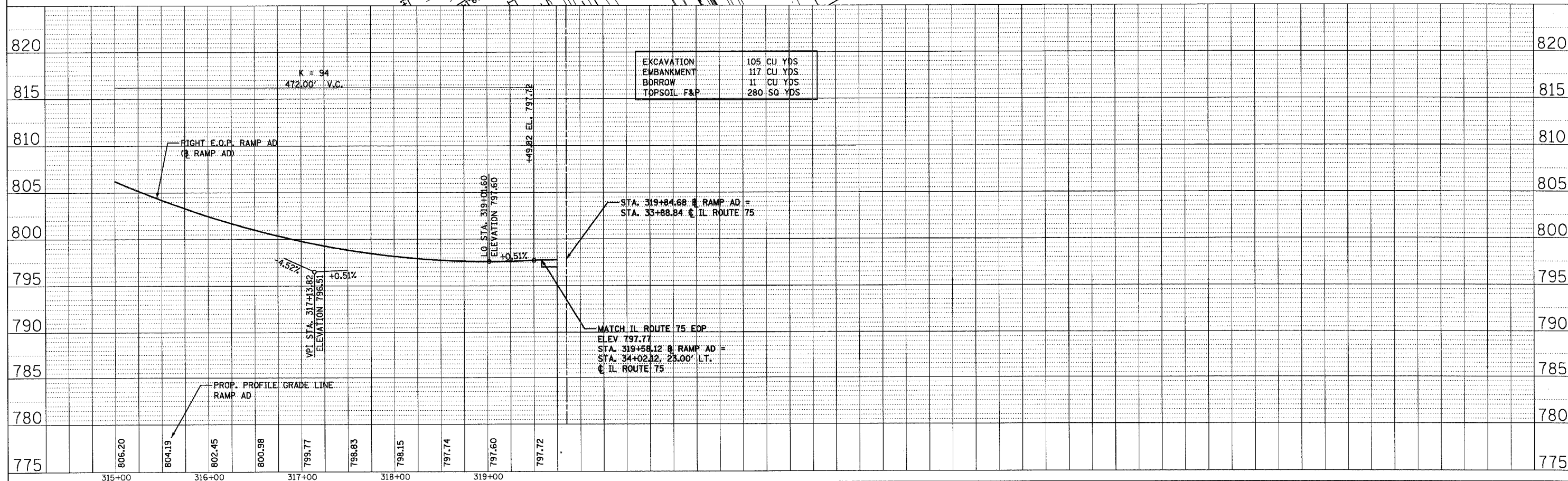
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	79
STA. 315+00 TO STA. 319+84.68				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE	BY
DESIGNED		
CHECKED		
APPROVED		

PROFILE	DATE	BY
DESIGNED		
CHECKED		
APPROVED		

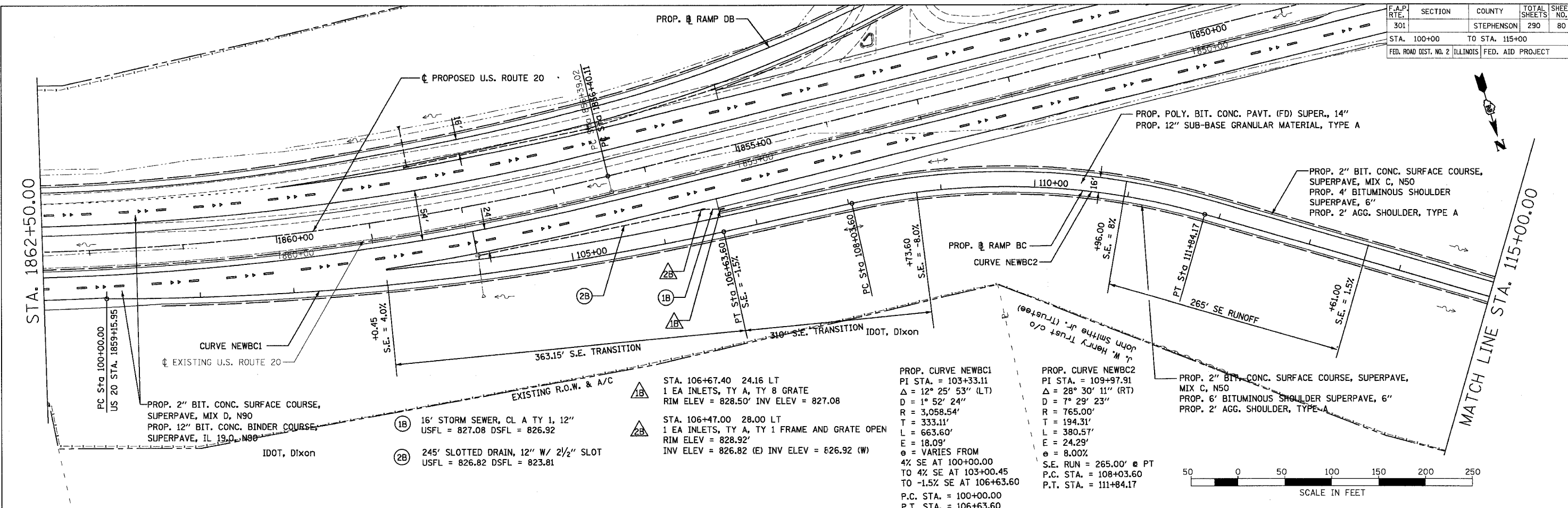


NOTE:
EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN
ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.

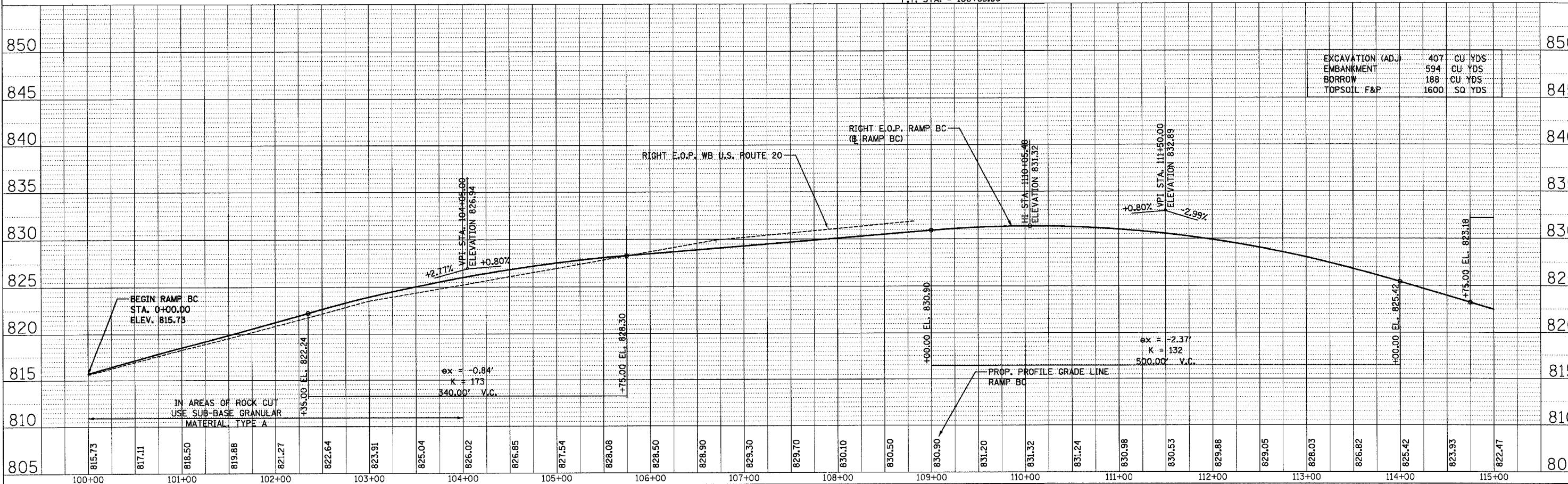


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	80
STA. 100+00		TO STA. 115+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DATE
BY	
CHECKED	
DATE	
NO. OF REV. CHECKED	
NO. OF REV. DATE	



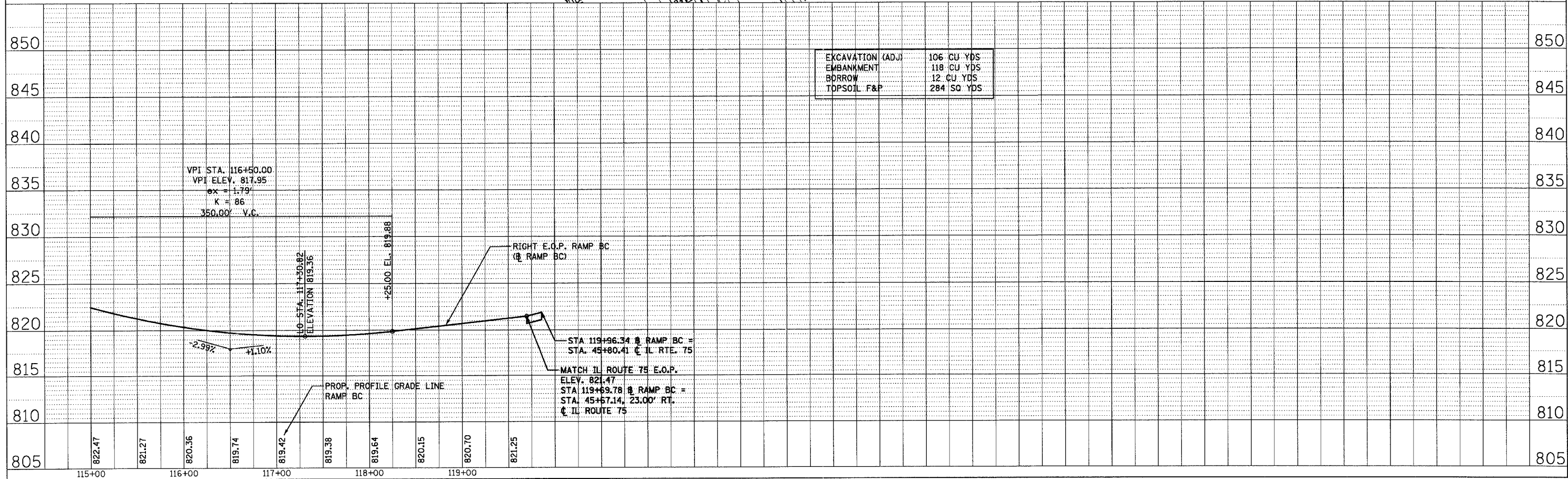
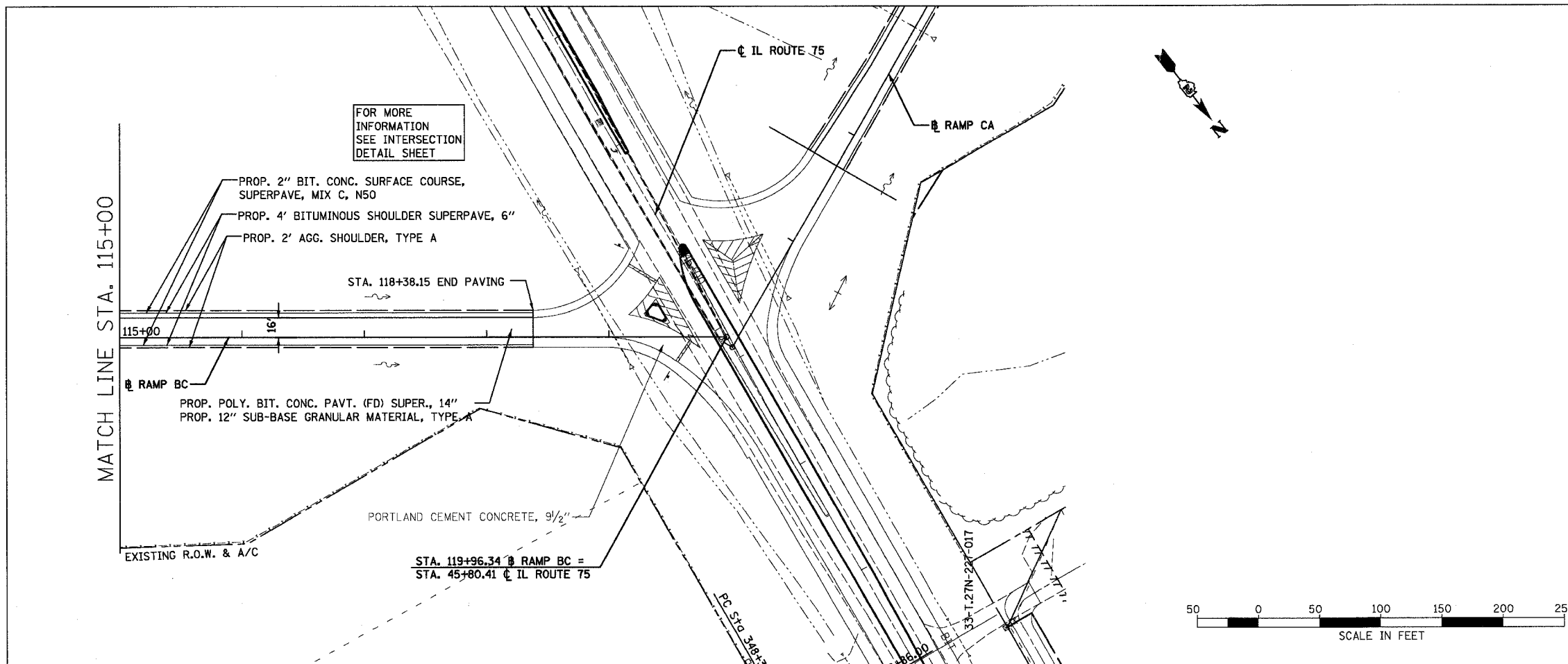
PROFILE	DATE
BY	
CHECKED	
DATE	
NO. OF REV. CHECKED	
NO. OF REV. DATE	



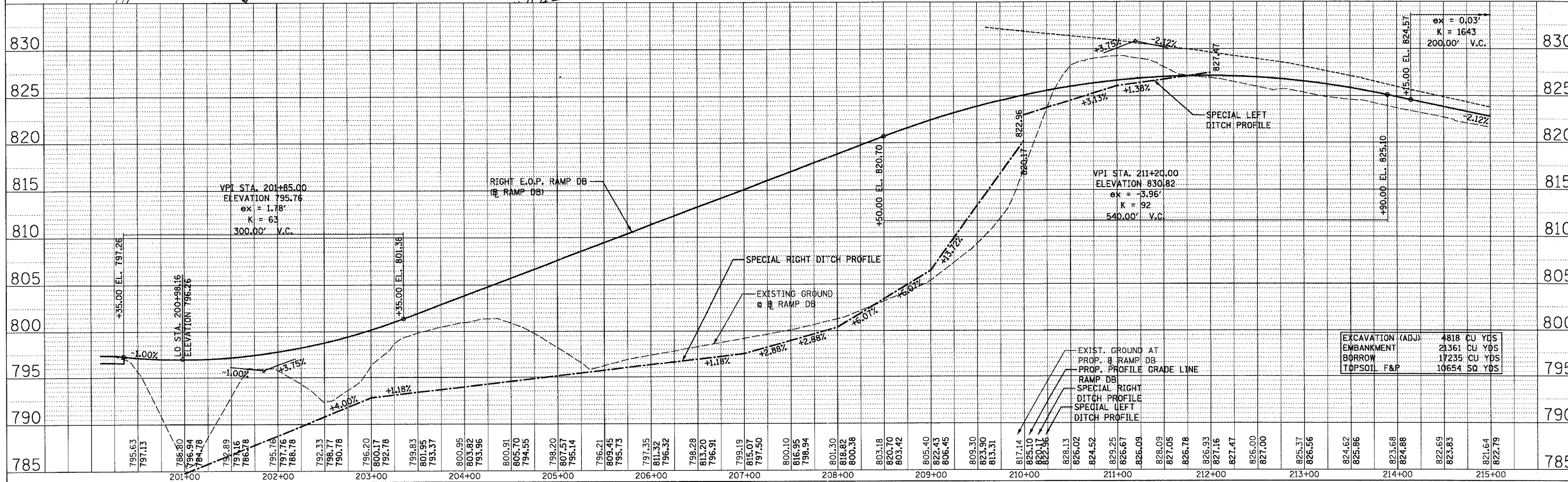
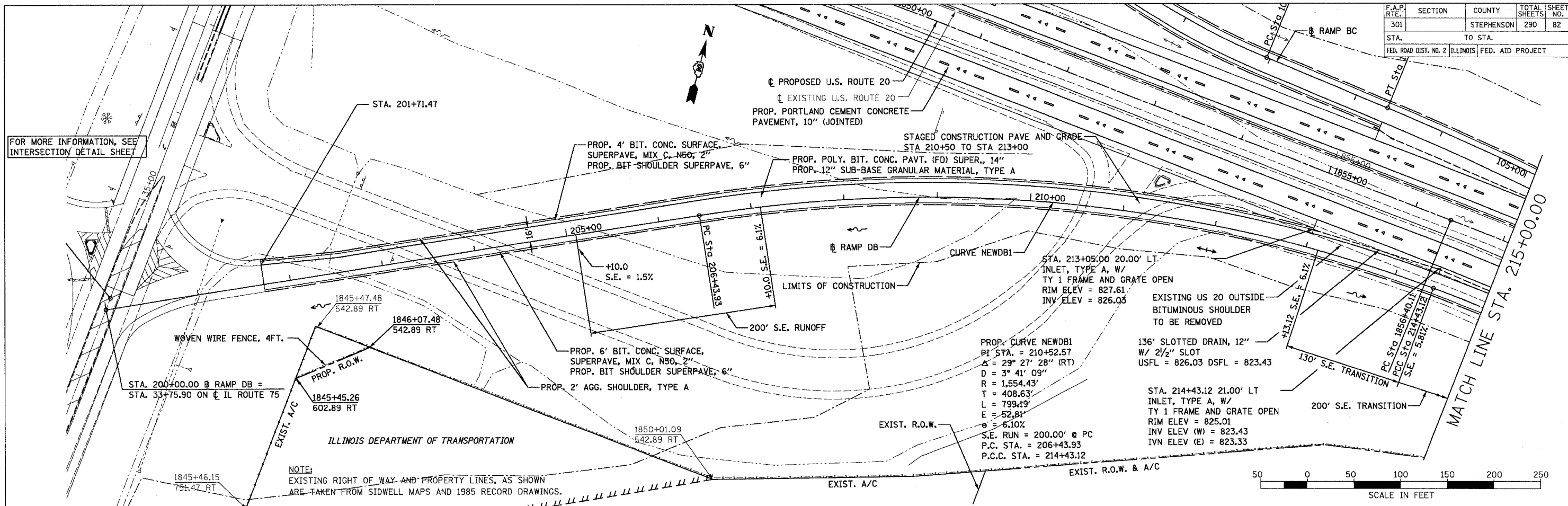
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	81
STA. 115+00 TO STA. 119+96.34				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

PLAN	DESIGNED	DATE
NOTE BOOK	BY	
	CHECKED	
	DATE	

PROFILE	DESIGNED	DATE
NOTE BOOK	BY	
	CHECKED	
	DATE	



CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	82
STA.	TO STA.			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

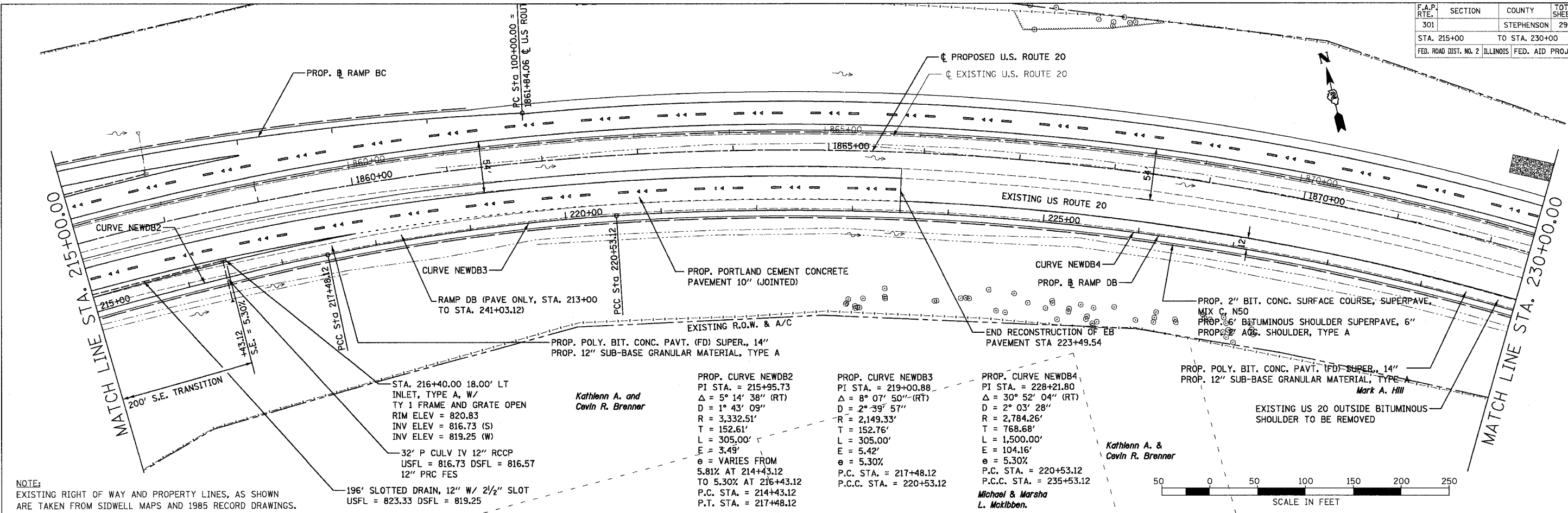


PLAN	DATE	BY
REVISION		
NO.		
DATE		
BY		
NO.		
DATE		
BY		
NO.		

PROFILE	DATE	BY
REVISION		
NO.		
DATE		
BY		
NO.		

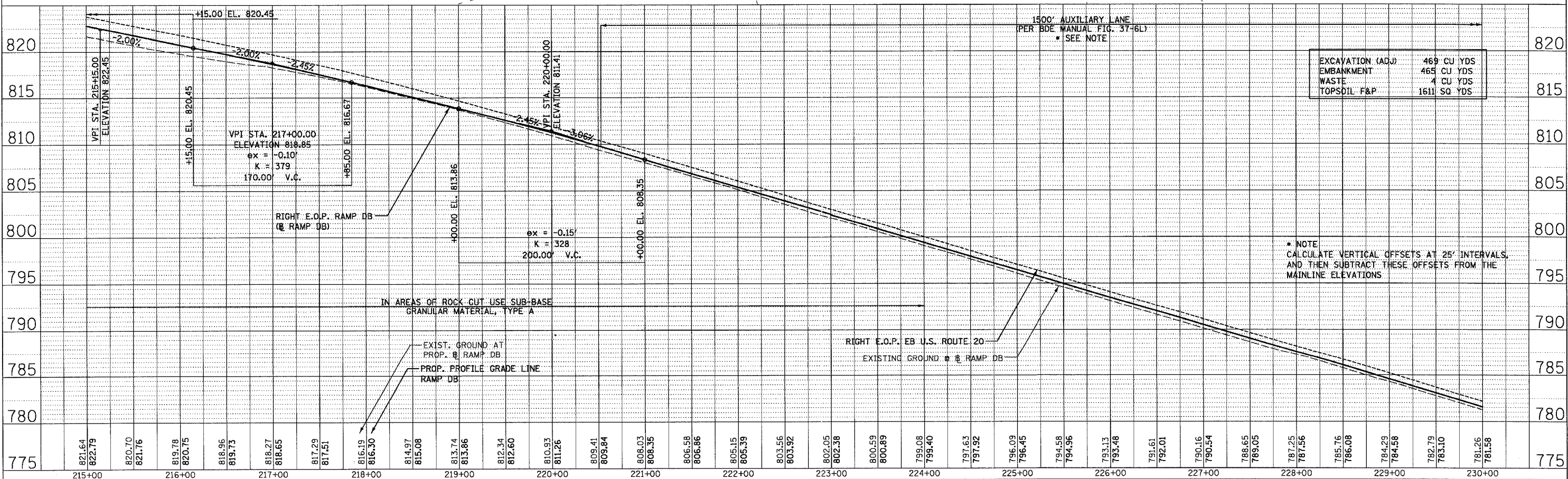
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	83
STA. 215+00		TO STA. 230+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

DATE	BY	REVIEWED	DATE
PLAN	NO. 1	NO. 2	NO. 3
NOTE BOOK	NO. 1	NO. 2	NO. 3
NO. 1	NO. 2	NO. 3	NO. 4



NOTE:
 EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.

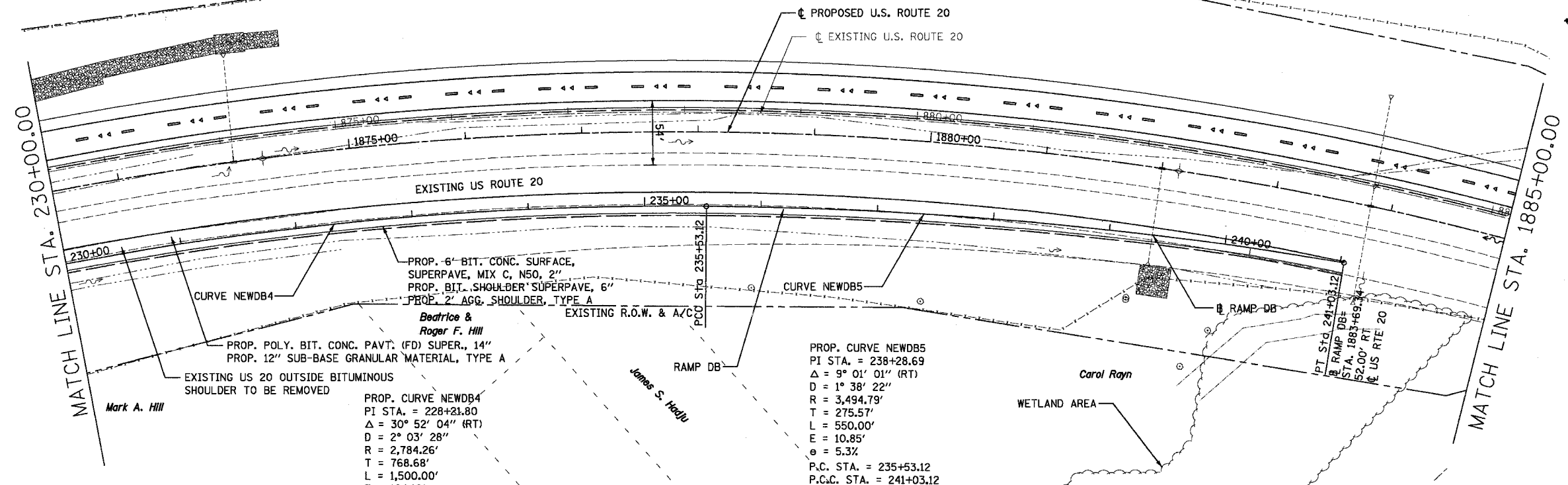
DATE	BY	REVIEWED	DATE
PROFILE	NO. 1	NO. 2	NO. 3
NOTE BOOK	NO. 1	NO. 2	NO. 3
NO. 1	NO. 2	NO. 3	NO. 4



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	84
STA. 230+00		TO STA. 241+03.21		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

Laura L. & David Olson

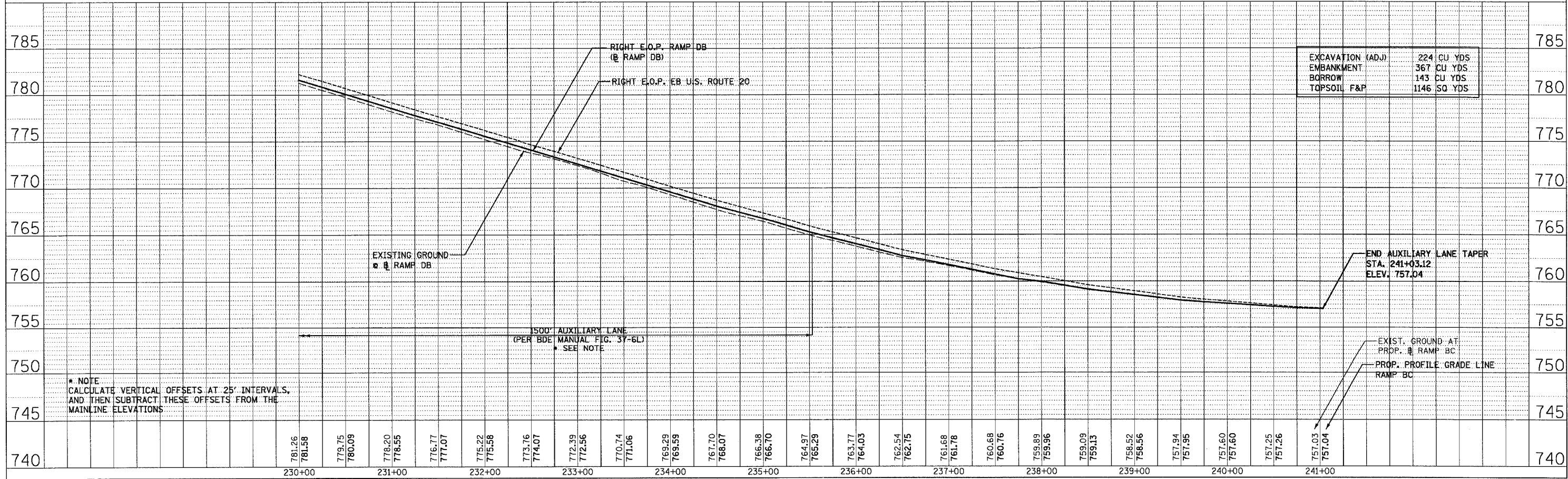
Laura L. & David Olson



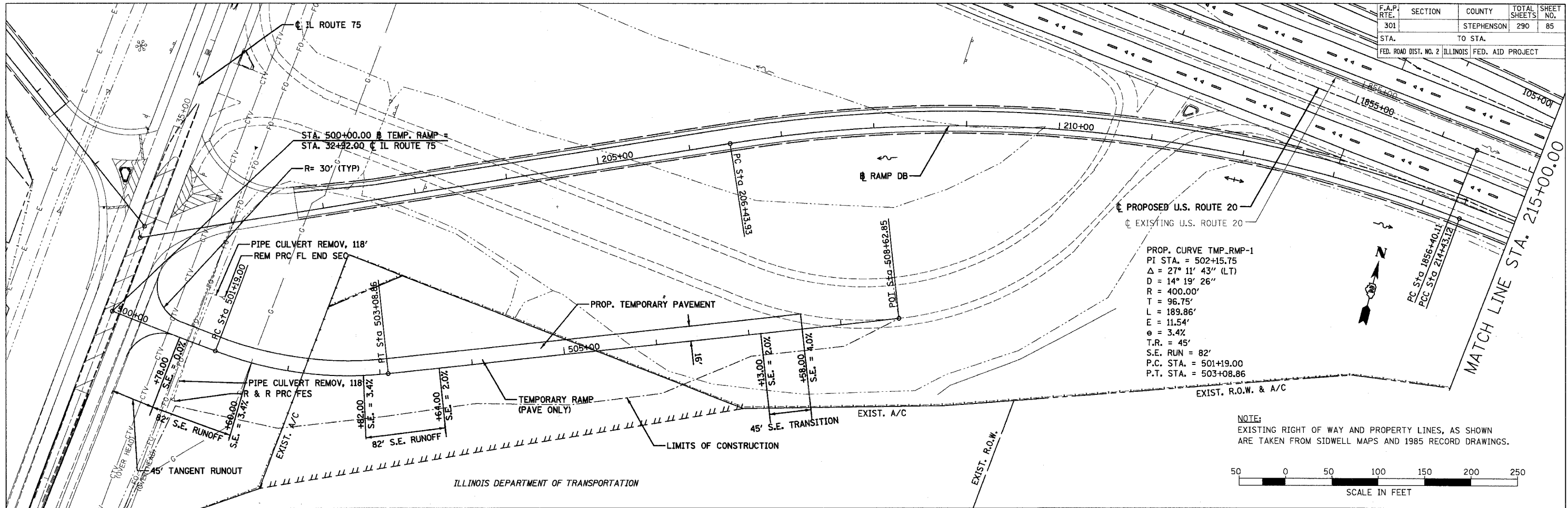
NOTE:
EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN
ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.

PLAN	SURVEYED	BY	DATE
	ALIGNED		
	CHECKED		
	NO. OF WAY CHECKED		
	NO.		

PROFILE	SURVEYED	BY	DATE
	GRADES		
	CHECKED		
	NO. NOTED		
	STRUCTURE		
	NOTATION		
	CHFD		



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	85
STA.	TO STA.			
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

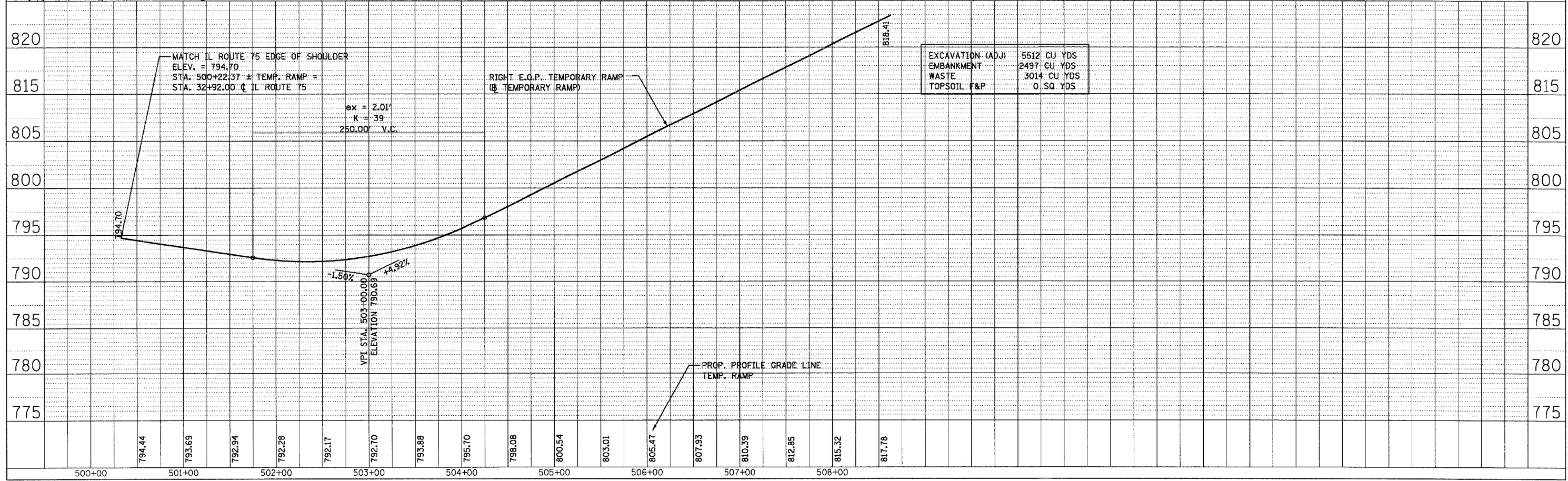


PROP. CURVE TMP_RMP-1
 PI STA. = 502+15.75
 $\Delta = 27^\circ 11' 43''$ (LT)
 $D = 14^\circ 19' 26''$
 $R = 400.00'$
 $T = 96.75'$
 $L = 189.86'$
 $E = 11.54'$
 $e = 3.4\%$
 $T.R. = 45'$
 $S.E. RUN = 82'$
 $P.C. STA. = 501+19.00$
 $P.T. STA. = 503+08.86$

NOTE:
 EXISTING RIGHT OF WAY AND PROPERTY LINES, AS SHOWN
 ARE TAKEN FROM SIDWELL MAPS AND 1985 RECORD DRAWINGS.

DATE	BY

DATE	BY

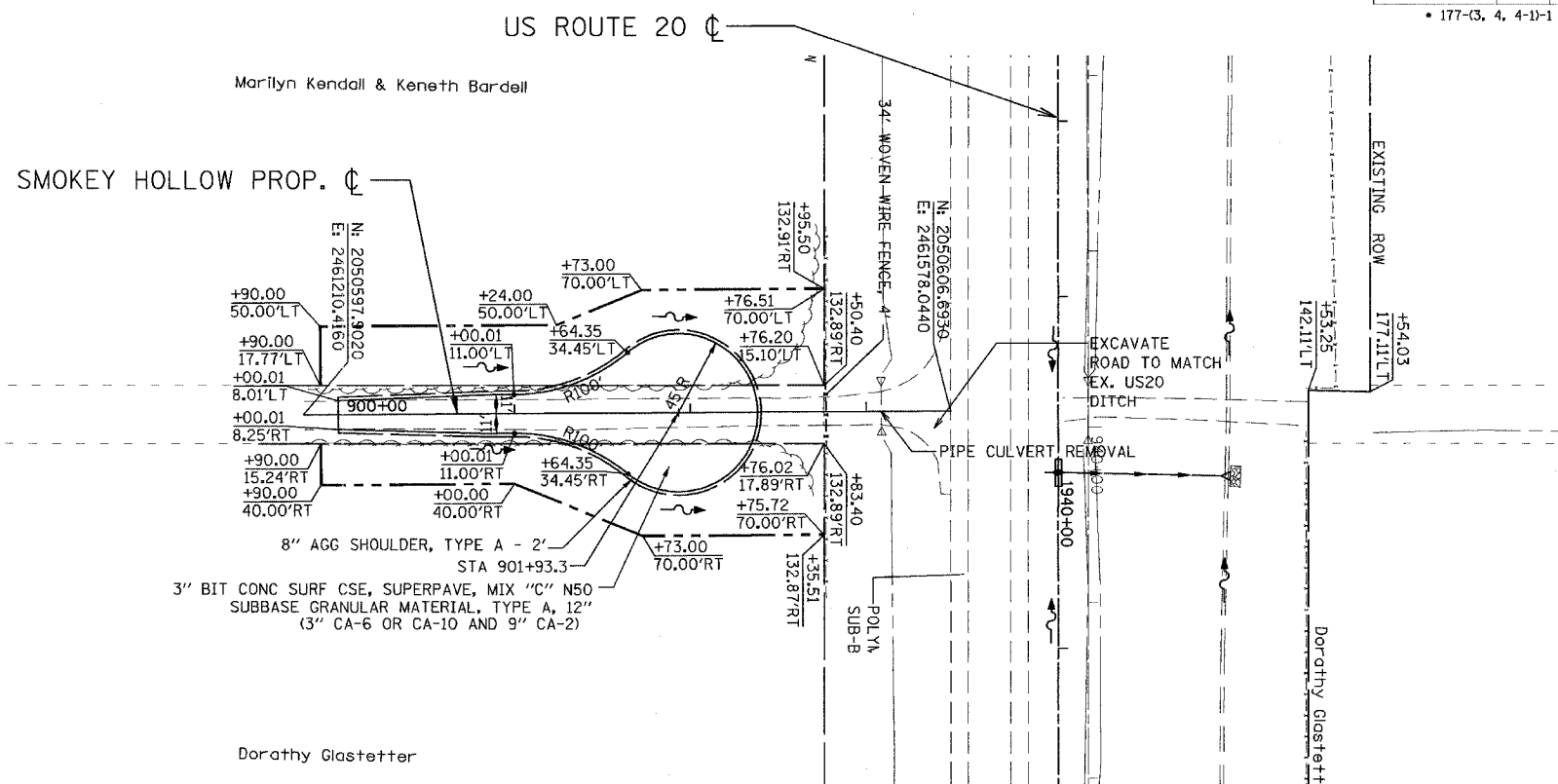


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CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	86
STA. 899+80.00		TO STA. 903+00.00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				
* 177-13, 4, 4-1-1				

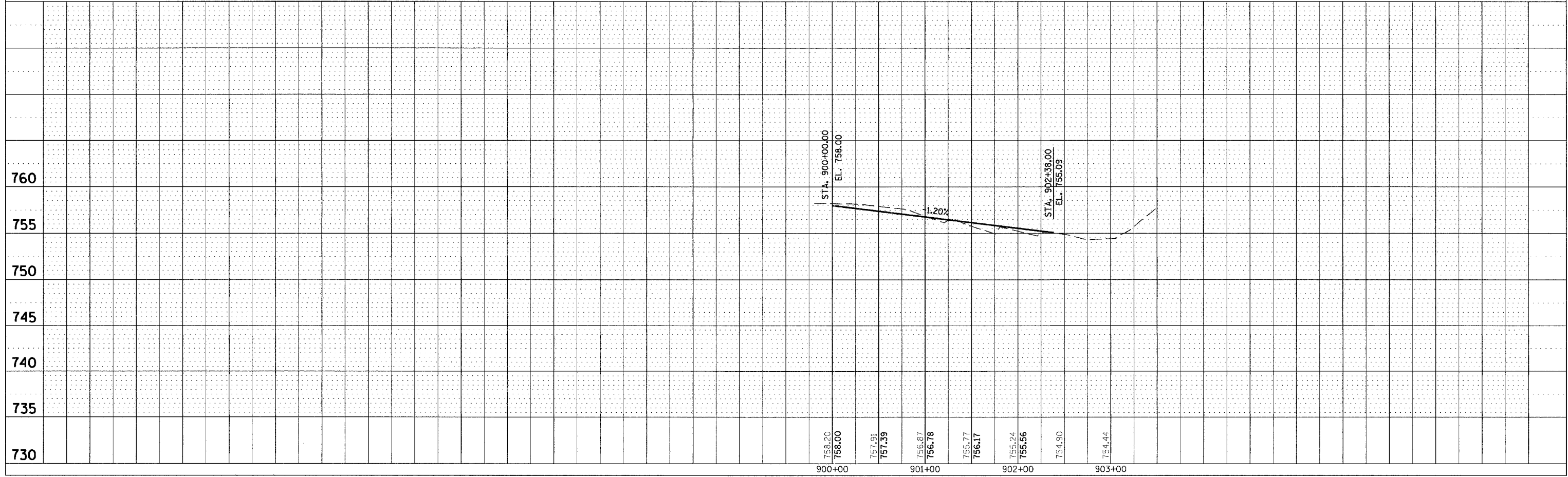
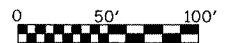
PLAN	REVISIONS	DATE
NO.	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	

PROFILE	REVISIONS	DATE
NO.	BY	
	DATE	
	BY	
	DATE	
	BY	
	DATE	



NOTES:
SIGNING AND DELINEATORS PER DISTRICT STANDARD 89.2

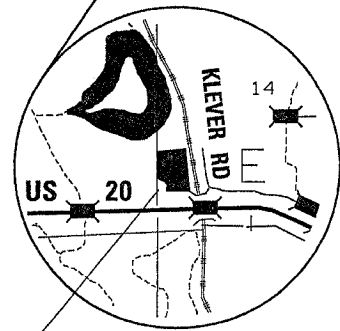
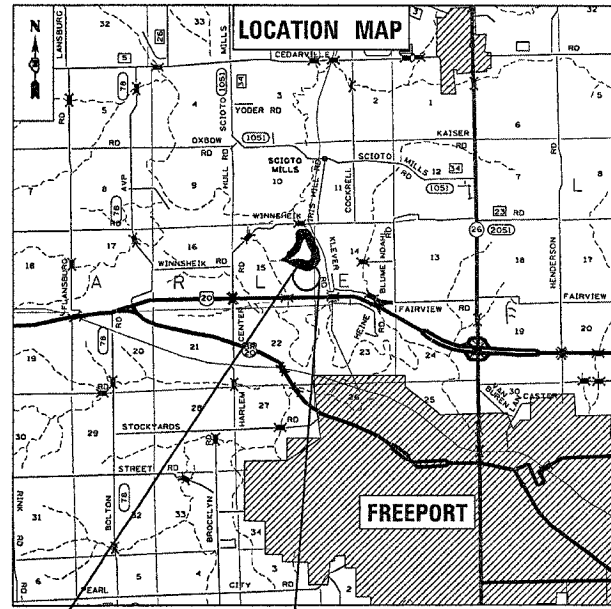
8" AGG SHOULDER, TYPE A - 2'
STA 901+93.3
3" BIT CONC SURF CSE, SUPERPAVE, MIX "C" N50
SUBBASE GRANULAR MATERIAL, TYPE A, 12"
(3" CA-6 OR CA-10 AND 9" CA-2)



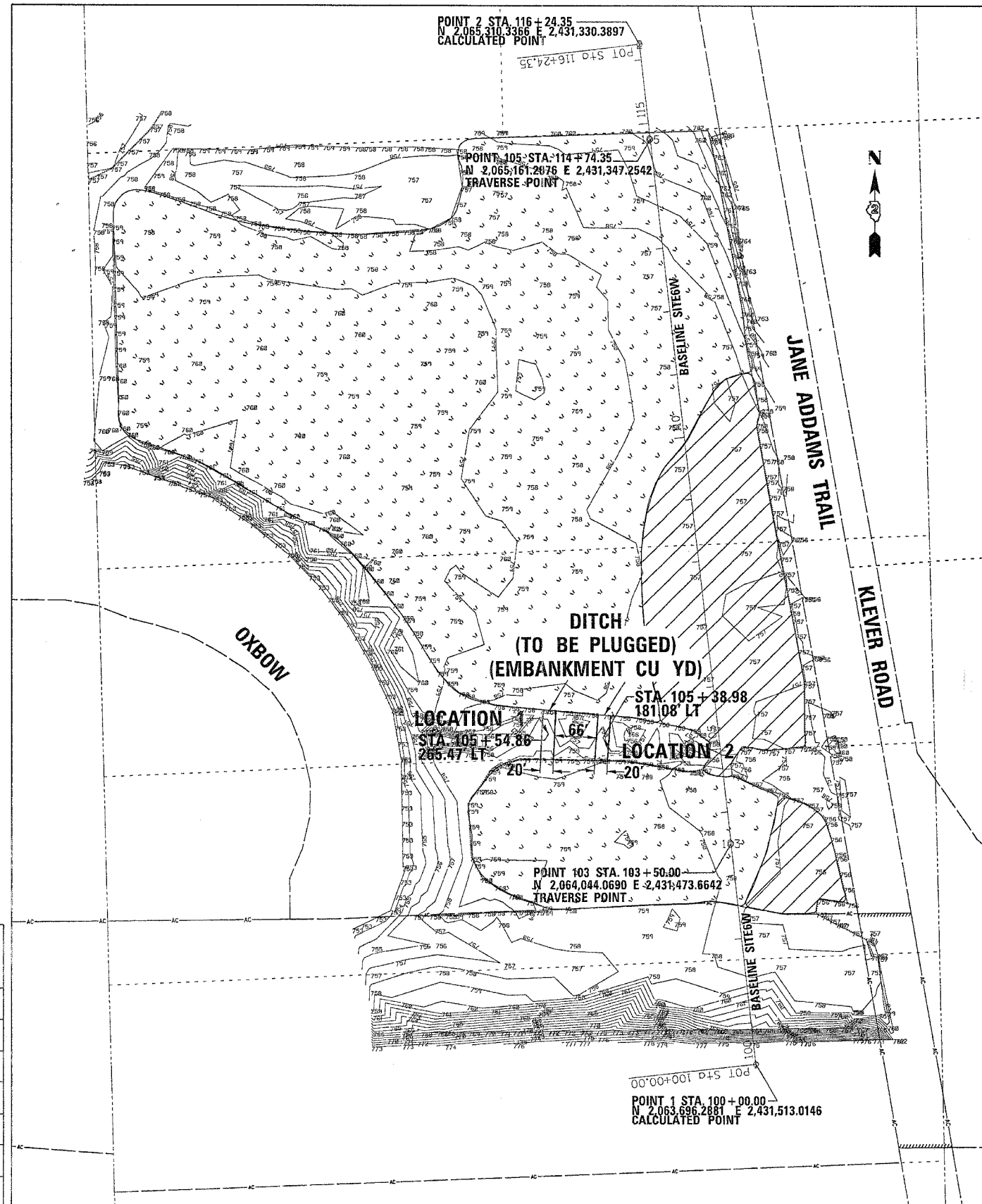
SMOKEY HOLLOW ROAD PLAN & PROFILE

JANE ADDAMS TRAIL WETLAND MITIGATION SITE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	177-(3,4,4-1)-1	STEPHENSON	290	87
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



MITIGATION SITE



NOTES:

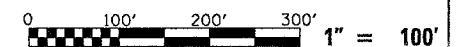
1. THE CONTRACTOR WILL NOT PERFORM ANY WORK OUTSIDE THE CONSTRUCTION LIMITS UNLESS DIRECTED BY THE ENGINEER. OBTAINING PROPER EQUIPMENT AS NEEDED FOR CONDITIONS AT THE PROJECT SITE IS THE RESPONSIBILITY OF THE CONTRACTOR.
2. LAYOUT SHALL BE PERFORMED BY THE DISTRICT LANDSCAPE ENGINEER.
3. MULCH SHALL BE HARDWOOD WOOD CHIPS, 5 FOOT WIDTH, 4 INCHES THICK WITH WEED BARRIER FABRIC.
4. TREES WILL BE PLANTED ON 20 FOOT CENTERS IN THE PROPOSED FORESTED AREAS.
5. THIS WETLAND SITE IS COVERED BY AN NPDES GENERAL PERMIT FROM THE IEPA. CONTRACTOR IS REQUIRED TO COMPLY WITH ALL THE REQUIREMENTS OF THE GENERAL PERMIT AND THE EROSION CONTROL PLAN.
6. THE DITCH LOCATED IN THE SOUTHERN ONE-THIRD OF THE SITE WILL BE PLUGGED IN TWO PLACES AS DESCRIBED IN THE SPECIAL PROVISIONS.

RPM TREE REPLACEMENT SCHEDULE

CODE NO.	SCIENTIFIC NAME	COMMON NAME	SIZE	UNIT	QUANTITY
A2C050G3	QUERCUS BICOLOR	SWAMP WHITEOAK	3 gal.RPM	each	300
A2C060G3	QUERCUS PALUSTRIS	PIN OAK	3 gal.RPM	each	300
A2C030G3	FRAXINUS PENN LANCEOLATA	GREEN ASH	3 gal.RPM	each	300
A2C040G3	PLATANUS OCCIDENTALIS	SYCAMORE	3 gal.RPM	each	300
A2C022G3	CARYA ILLINOENSIS	PECAN	3 gal.RPM	each	300
#2001549	JUGLANS CINEREA	BUTTERNUT HICKORY	3 gal.RPM	each	100

LEGEND

- FORESTED WETLAND - 16.0 ACRES
- NATURAL VEGETATION WETLAND - 3.1 ACRES
- EXISTING TREE LINE
- PROPERTY LINE
- PERMANENT EASEMENT
- EXISTING R.O.W.
- EXISTING ACCESS CONTROL



U.S. 20 FREEPORT BYPASS (FAP ROUTE 301)
WETLAND COMPENSATION PLAN
WETLAND MITIGATION PLAN

•DGN-SPEC*
•DATE-TIME*
•REV*
•REF*
•REV*
•REF*

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	177-(3,4,4-1)-1	STEPHENSON	290	88
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

EAST BANK
LOCATION 2

CENTER
LOCATION 2

WEST BANK
LOCATION 2

EAST BANK
LOCATION 1

CENTER
LOCATION 1

WEST BANK
LOCATION 1

160 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160

FINAL SURVEY	DATE
SWAYED	BY
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	

ORIGINAL SURVEY	DATE
SWAYED	BY
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	

PLOT DATE : Thu Oct 27 14:16:46 2005
 FILE NAME : c:\pcc\pcc\177-3,4,4-1-1.dwg
 USER NAME : mgj11

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	89
STA.		TO STA.		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	

**SUGGESTED SEQUENCE OF WORK
US ROUTE 20, IL ROUTE 75, AND RAMP DB**

**MAINTENANCE OF TRAFFIC
GENERAL NOTES**

STAGE I - USE TRAFFIC CONTROL STANDARD 701401 ON THE EB AND WB LANES TO CONSTRUCT THE CROSSOVER AND THE PAVEMENT WIDENING FROM STA 1703+00 TO 1706+00. IN ADDITION A W1-4R(O)-48 SIGN SHOULD REPLACE THE W4-2R(O)-48 SIGN IN THE WB DIRECTION ONLY DURING THE CONSTRUCTION OF THE CROSSOVER. THE COST OF THESE SUBSTITUTIONS IS INCLUDED IN BID ITEM 70100410. USE TRAFFIC CONTROL STANDARD 701411 FOR THE EB ENTRANCE RAMP.

STAGE II - USE TRAFFIC CONTROL STANDARD 701201 FOR WORK ON IL ROUTE 75.

STAGE II - RAMP DB @ US 20 USE TRAFFIC CONTROL STANDARD 701411.

STAGE II - USE TRAFFIC CONTROL STANDARD 701402 TO COMPLETE CONSTRUCTION FROM STA 1706+00 TO STA 1710+00.

STAGE II - USE TRAFFIC CONTROL STANDARD 701401 TO RECONSTRUCT SHOULDERS STA 1796+50 TO STA 1812+75.

CONCRETE BARRIER WILL NOT BE USED WITH TRAFFIC CONTROL STANDARD 701416.

THE FINAL SURFACE FOR RAMP AD SHALL BE PLACED AT THE SAME TIME AS MAINLINE US20 EASTBOUND RESURFACING.

CONSTRUCTION

STAGE I

1. PROVIDE TEMPORARY PAVEMENT ALONG EB US ROUTE 20 JUST TO THE WEST OF THE EXISTING CROSSOVER, FROM STATION 1703+00 TO STATION 1706+00.

2. BUILD PROPOSED CROSSOVER WEST OF PROJECT LIMITS AND MOVE TRAFFIC ONTO IT.

STAGE II

1. GRADE AND PAVE WESTBOUND US ROUTE 20, RAMPS AD, BC, CA, AND THE TEMPORARY RAMP.

2. PLACE CURB AND GUTTER, MEDIAN CONCRETE PAVEMENT, AND PAVEMENT MARKINGS ON IL ROUTE 75 WHERE APPROPRIATE BEFORE OPENING RAMPS AD, BC, AND CA TO TRAFFIC. REMOVE TRAFFIC FROM THE EXISTING "JUG-HANDLE" RAMP AT US ROUTE 20/IL ROUTE 75 ONTO THE TEMPORARY RAMP IN THE SOUTHEAST QUADRANT OF THE US ROUTE 20/IL ROUTE 75 INTERCHANGE.

3. MOVE WESTBOUND US ROUTE 20 TRAFFIC ONTO THE PROPOSED WESTBOUND LANES.

4. MOVE TRAFFIC ONTO THE TEMPORARY RAMP (DB). CONSTRUCT RAMP DB FROM STA. 201+80 TO 210+50 & 211+25 TO 241+03 ALONG WITH THE SOUTH LANE OF US 20 FROM STA. 1855+50 TO 1865+76. MAINTAIN EB TRAFFIC ON THE EXISTING NORTH LANE.

5. CONSTRUCT A TEMPORARY CONNECTION FROM EXISTING RAMP DB TO PROPOSED RAMP DB AT STA. 212±. MAINTAIN RAMP TRAFFIC ON THE TEMPORARY RAMP PAVEMENT.

6. RECONSTRUCT PAVEMENT SECTION FROM STA 1706+00 TO STA 1710+00.

7. RECONSTRUCT SHOULDERS FROM STATION 1796+50 TO STA 1812+75.

STAGE III

1. PLACE TRAFFIC ON TEMPORARY RAMP AND RAMP DB FROM STA. 212+50 TO 241+03.

2. CONSTRUCT THE GAP IN RAMP RAMP DB FROM STA. 210+50 TO 211+25.

3. MOVE RAMP TRAFFIC ONTO THE COMPLETED RAMP DB AND REMOVE THE TEMPORARY AND OLD EXISTING RAMP PAVEMENT.

4. CONSTRUCT THE SOUTH LANE OF EB US 20 FROM STA. 1848+15 TO 1855+50.

STAGE IV

1. MOVE US 20 EB TRAFFIC ONTO THE NEW SOUTH LANE. CONSTRUCT THE NORTH LANE OF EB US 20 FROM STA 1848+15 TO 1865+76.

2. THE CURRIER ROAD/US ROUTE 20 INTERSECTION WILL REMAIN OPEN UNTIL THE PERMANENT RAMPS AT THE US ROUTE 20/IL ROUTE 75 INTERCHANGE ARE ALL OPEN TO TRAFFIC. AT THAT TIME, THE INTERSECTION WILL BE REMOVED.

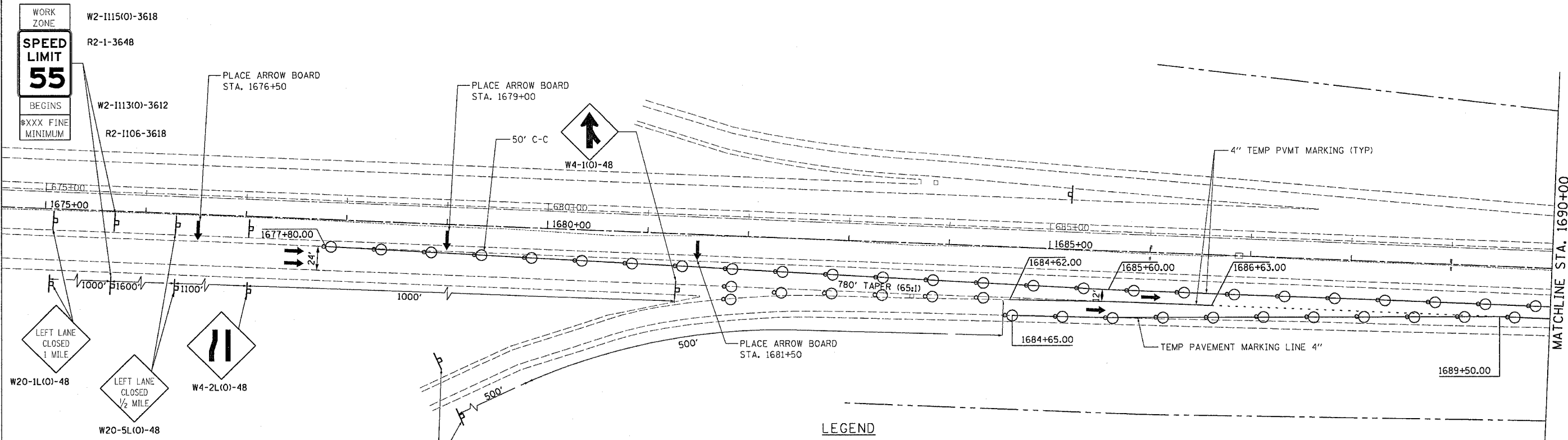
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**CONSTRUCTION STAGING
US ROUTE 20, IL ROUTE 75, &
RAMP DB
SEQUENCE OF CONSTRUCTION**

SCALE: VERT. DRAWN BY
 HORIZ. CHECKED BY
DATE 7/15/05

CONTRACT NO. 64577			
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
301		STEPHENSON	290 90
STA.	1675+00	TO STA.	1690+00
FED. ROAD DIST. NO. 2	ILLINOIS	FED. AID PROJECT	

TRAFFIC CONTROL AS PER
IDOT STANDARDS 701400 (WB),
701411, 701416, 701422



LEGEND

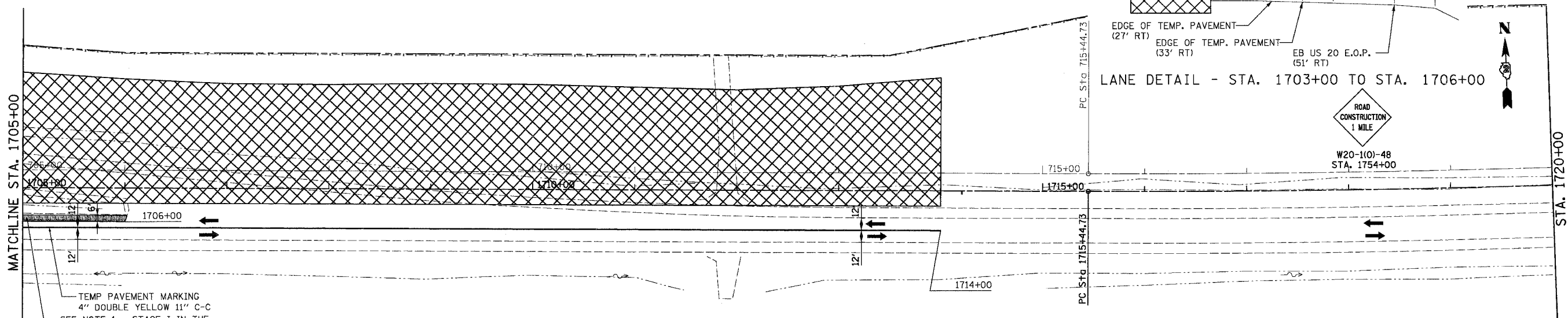
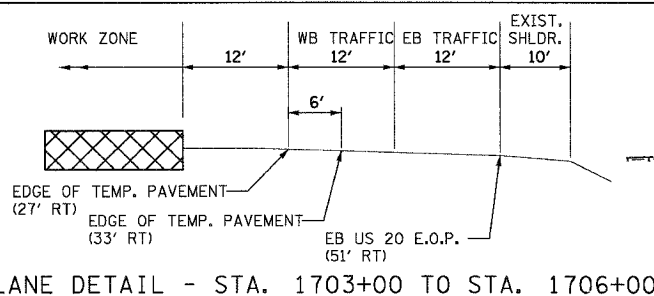
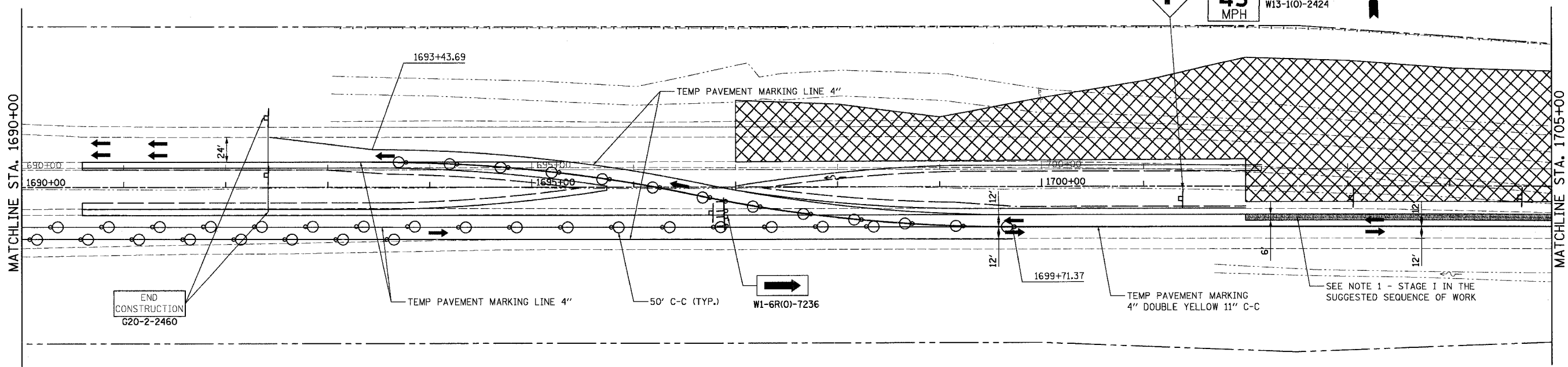
- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
CONSTRUCTION STAGING
STAGE I
U.S. ROUTE 20
STA. 1675+00 TO STA. 1690+00
SCALE: VERT. 1" = 50'
DATE 7/15/05
DRAWN BY MVT
CHECKED BY JC

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	91
STA. 1690+00 TO STA. 1720+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

TRAFFIC CONTROL AS PER IDOT STANDARD 701416



- LEGEND**
- WORK AREA
 - TEMP. PAVEMENT
 - BARRICADE TY III WITH FLASHING LIGHTS
 - BARRICADE TY II
 - SIGN
 - VERTICAL PANEL
 - BARRICADE OR DRUM WITH STEADY BURN LIGHTS
 - TRAFFIC FLOW
 - FLASHING LIGHTS
 - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
 - IMPACT ATTENUATOR
 - TEMPORARY CONCRETE BARRIER

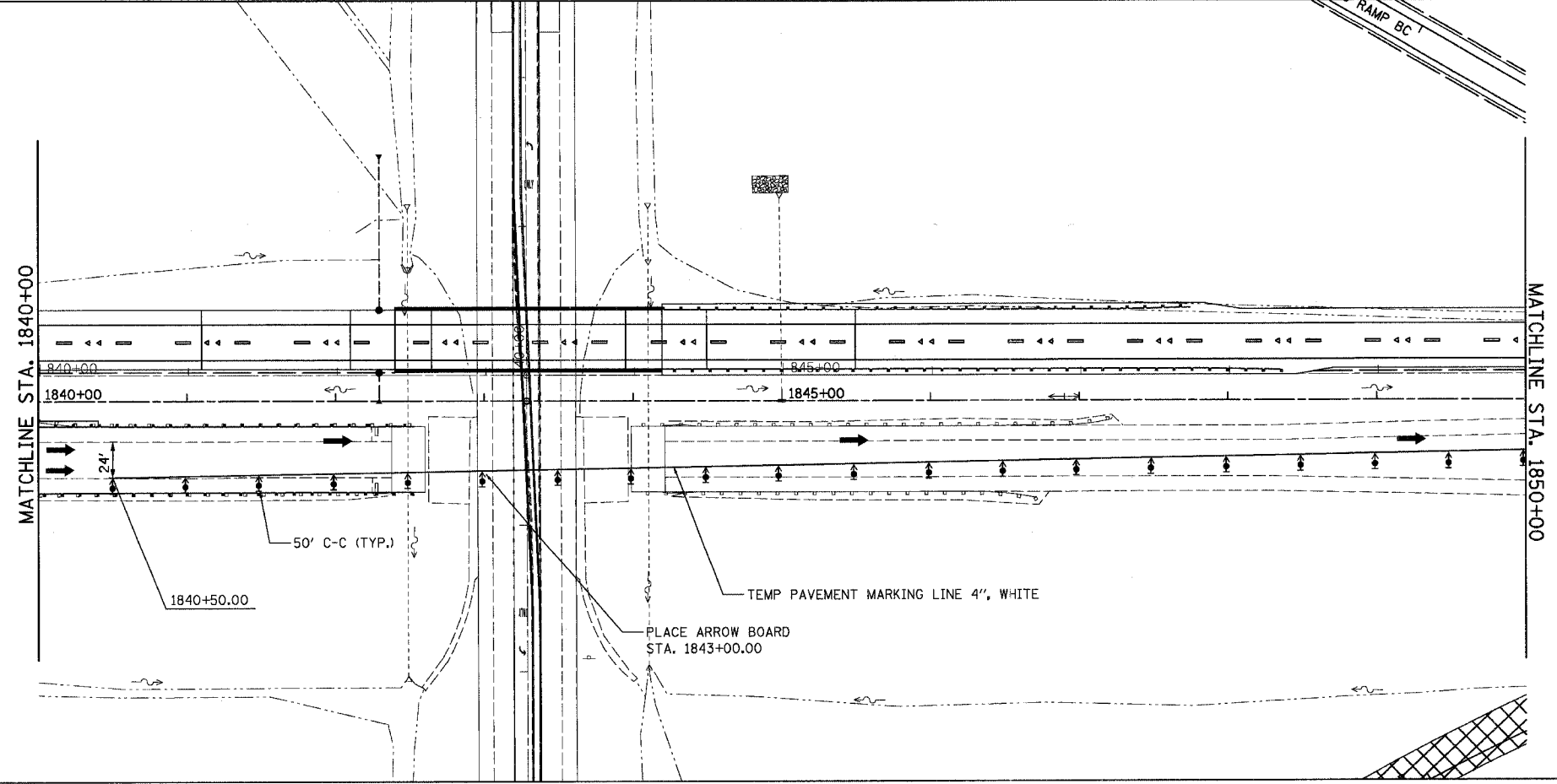
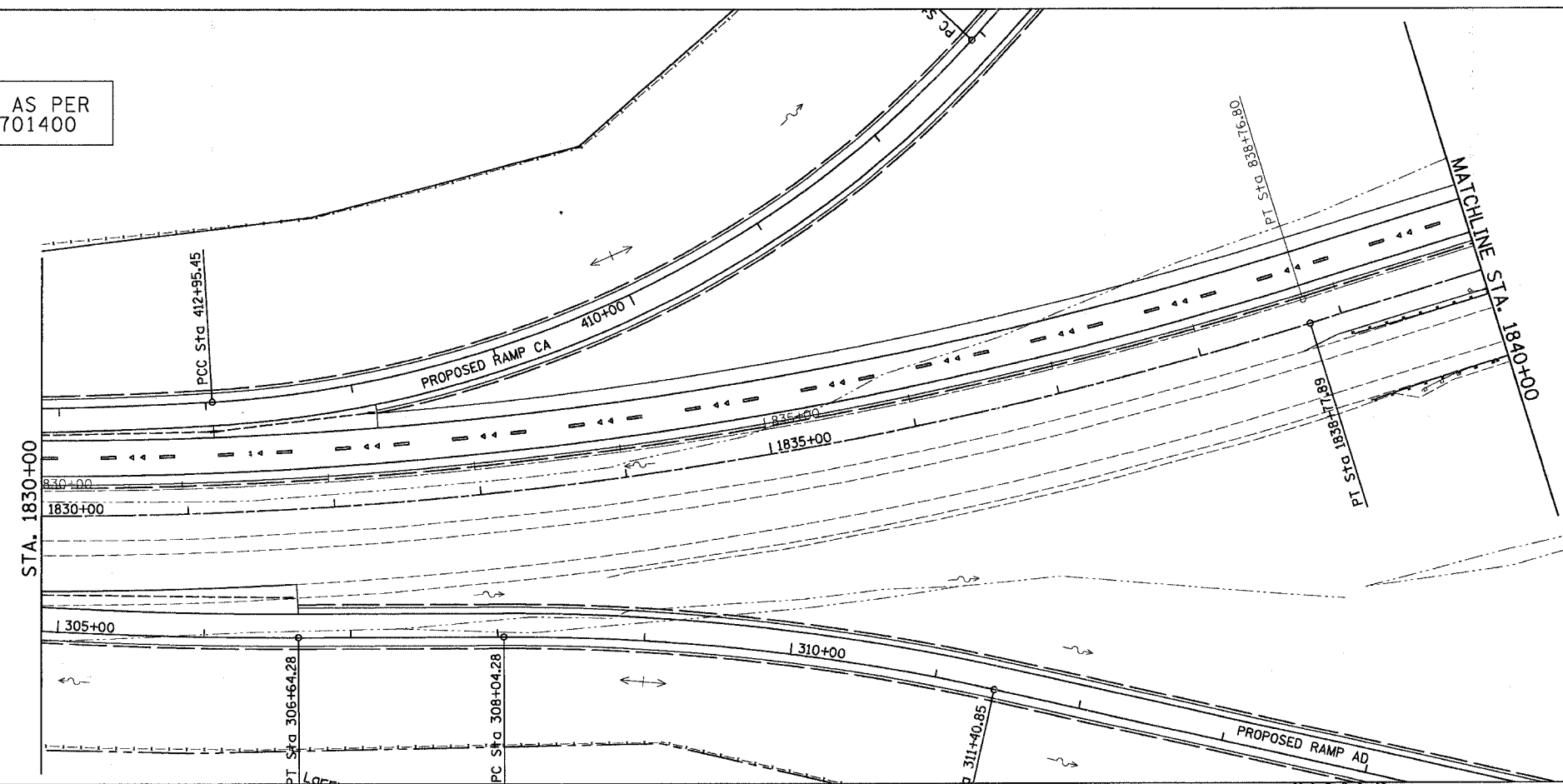
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION STAGING
 STAGE I
 U.S. ROUTE 20
 STA. 1690+00 TO STA. 1720+00
 SCALE: VERT. 1" = 50'
 HORIZ. 1" = 50'
 DATE 7/15/05
 DRAWN BY MBT
 CHECKED BY JC

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	92
STA. 1830+00		TO STA. 1850+00		
FED. ROAD DIST. NO. 2		ILLINOIS		FED. AID PROJECT

TRAFFIC CONTROL AS PER IDOT STANDARD 701400



LEGEND

- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER

REVISIONS

NAME	DATE
WILL REEDY	10/5/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION STAGING
 STAGE II
 U.S. ROUTE 20
 STA. 1830+00 TO STA. 1850+00

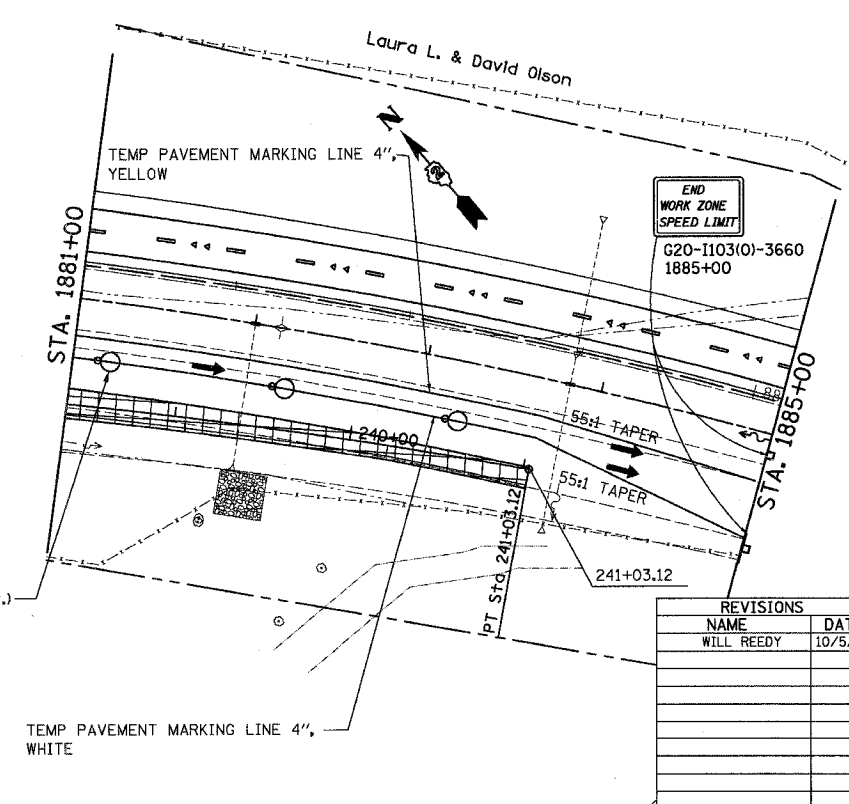
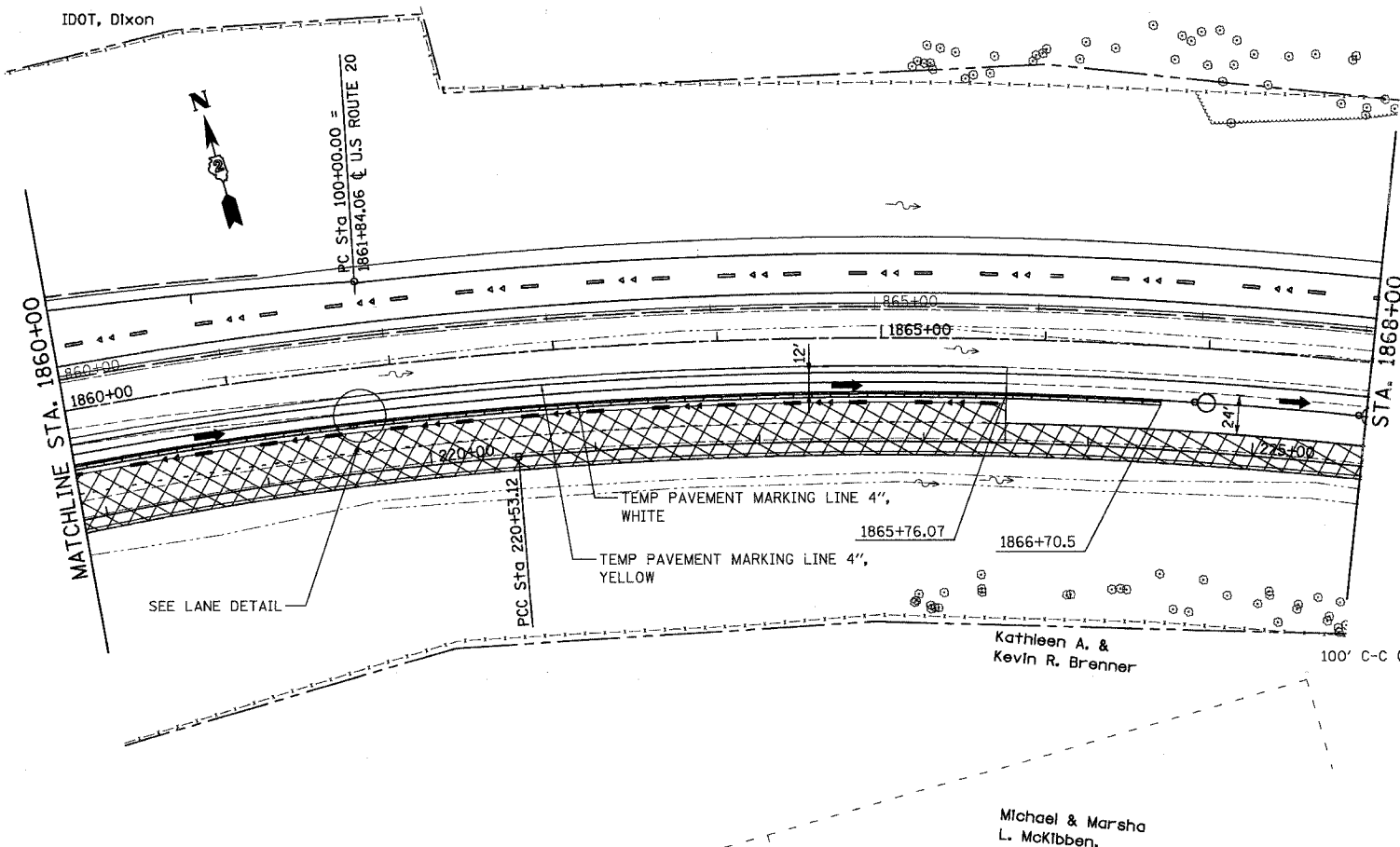
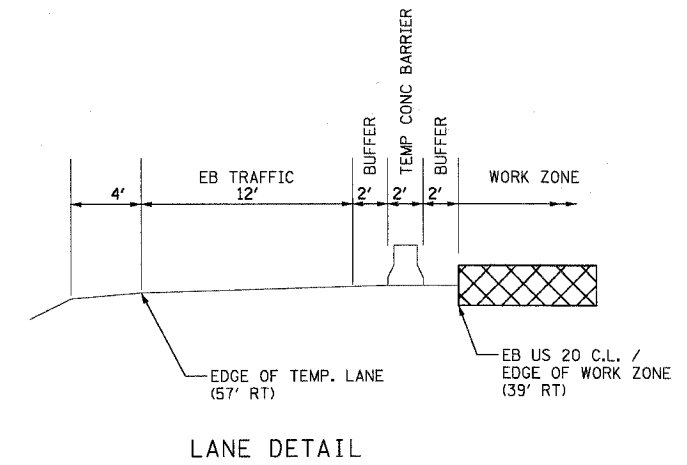
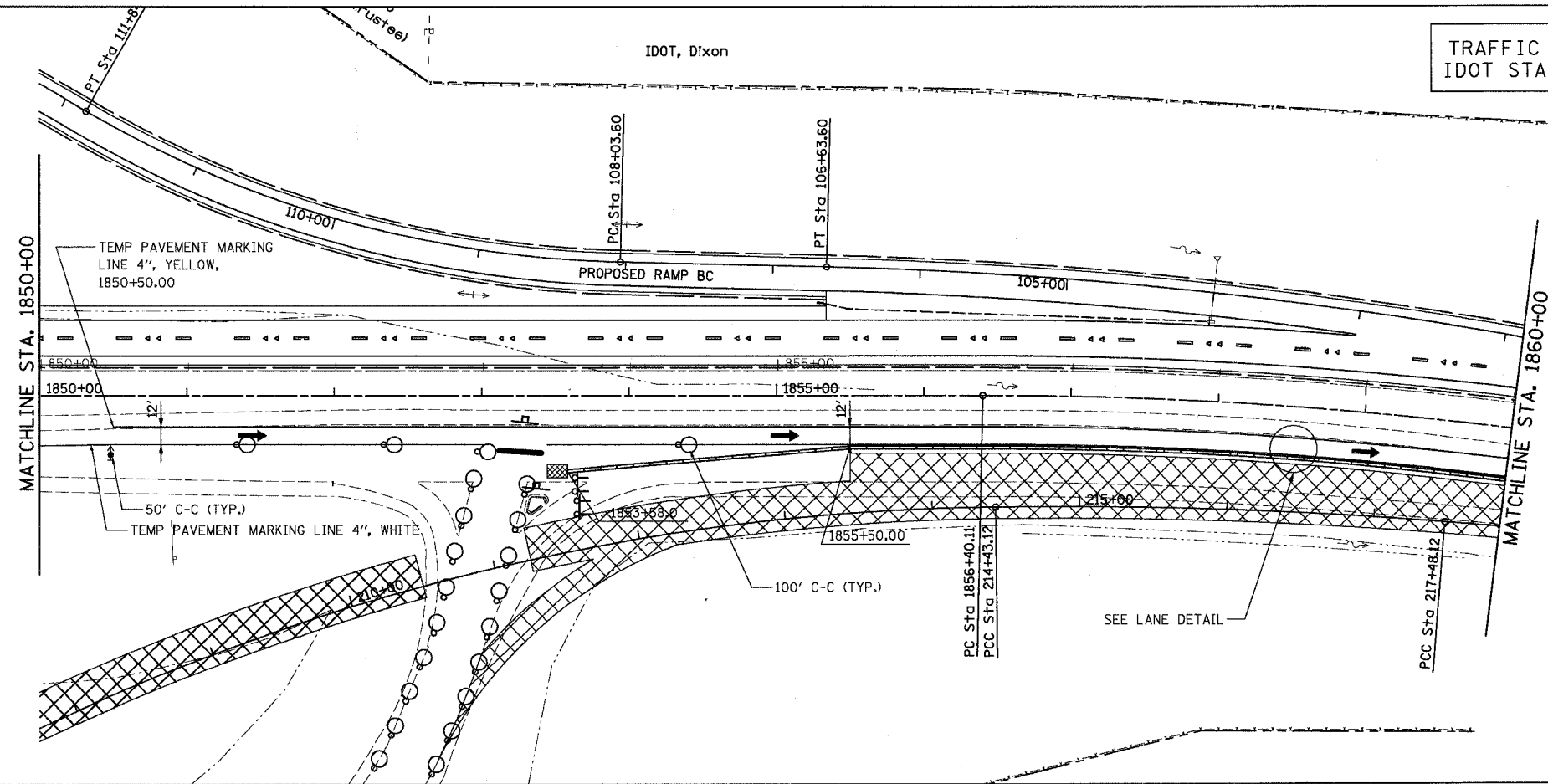
SCALE: VERT. 1" = 50'
 HORIZ. 1" = 50'
 DATE: 7/15/05

DRAWN BY MBT
 CHECKED BY JC

CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	93
STA. 1850+00		TO STA. 1870+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

TRAFFIC CONTROL AS PER IDOT STANDARD 701402-05



LEGEND

- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER

REVISIONS	
NAME	DATE
WILL REEDY	10/5/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION STAGING
 STAGE II
 U.S. ROUTE 20
 STA. 1850+00 TO STA. 1870+00
 SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 7/15/05
 DRAWN BY MBT
 CHECKED BY JC

Michael & Marsha L. McKibben.

Kathleen A. & Kevin R. Brenner

SEE LANE DETAIL

100' C-C (TYP.)

TEMP PAVEMENT MARKING LINE 4", WHITE
 TEMP PAVEMENT MARKING LINE 4", YELLOW

TEMP PAVEMENT MARKING LINE 4", WHITE

END WORK ZONE SPEED LIMIT
 G20-1103(O)-3660
 1885+00

Laura L. & David Olson

IDOT, Dixon

IDOT, Dixon

TEMP PAVEMENT MARKING LINE 4", YELLOW, 1850+50.00

50' C-C (TYP.)
 TEMP PAVEMENT MARKING LINE 4", WHITE

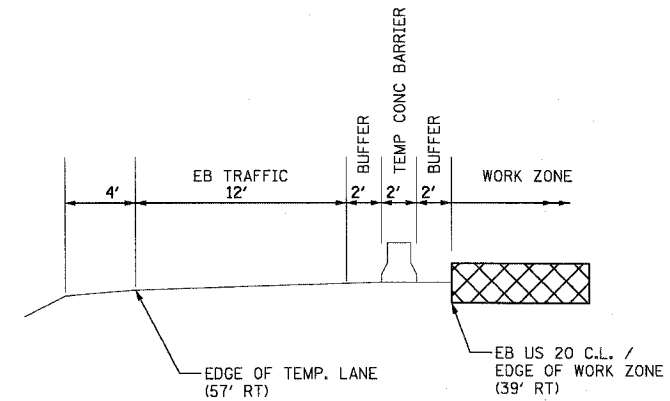
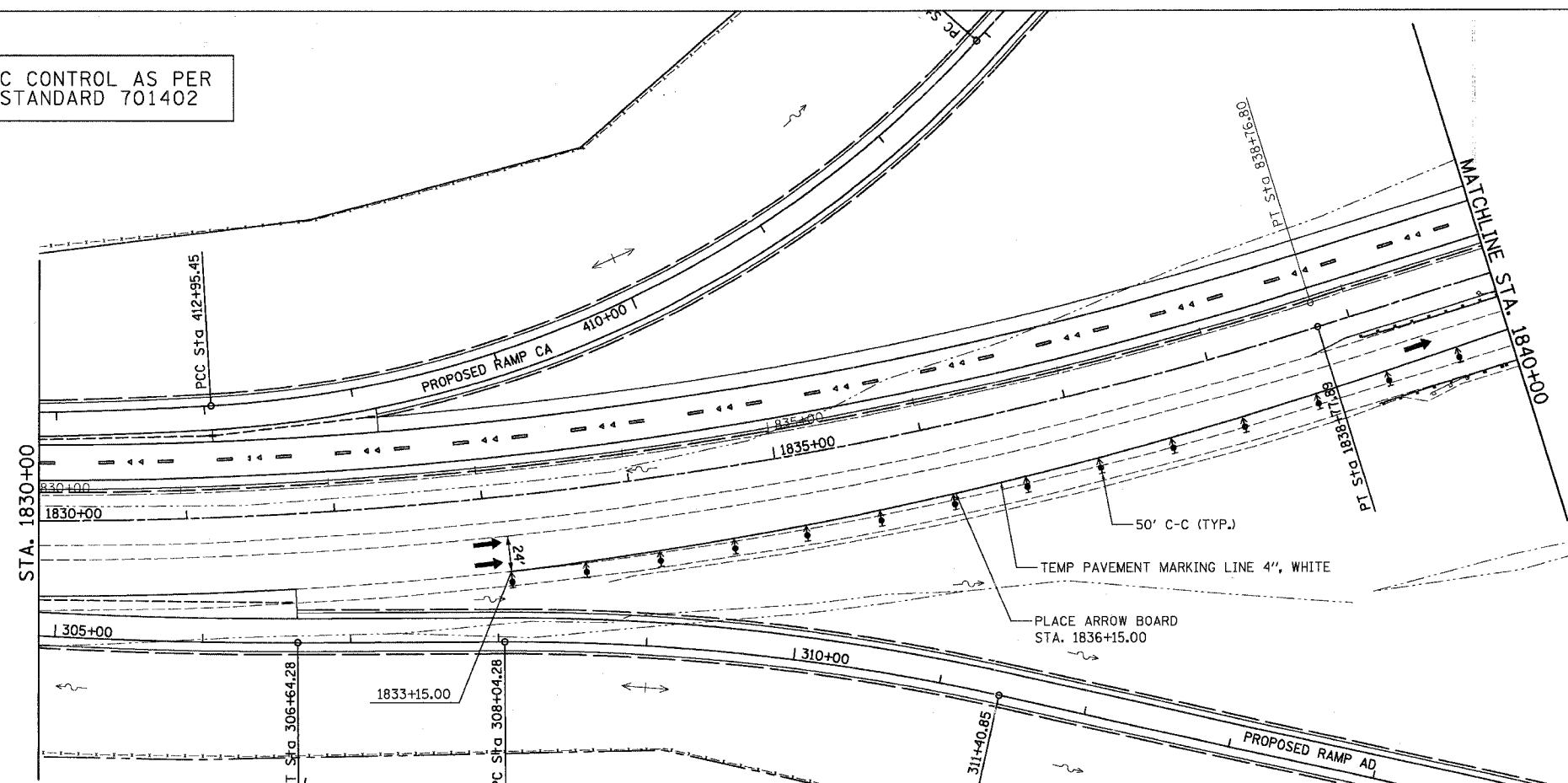
PROPOSED RAMP BC

SEE LANE DETAIL

LANE DETAIL

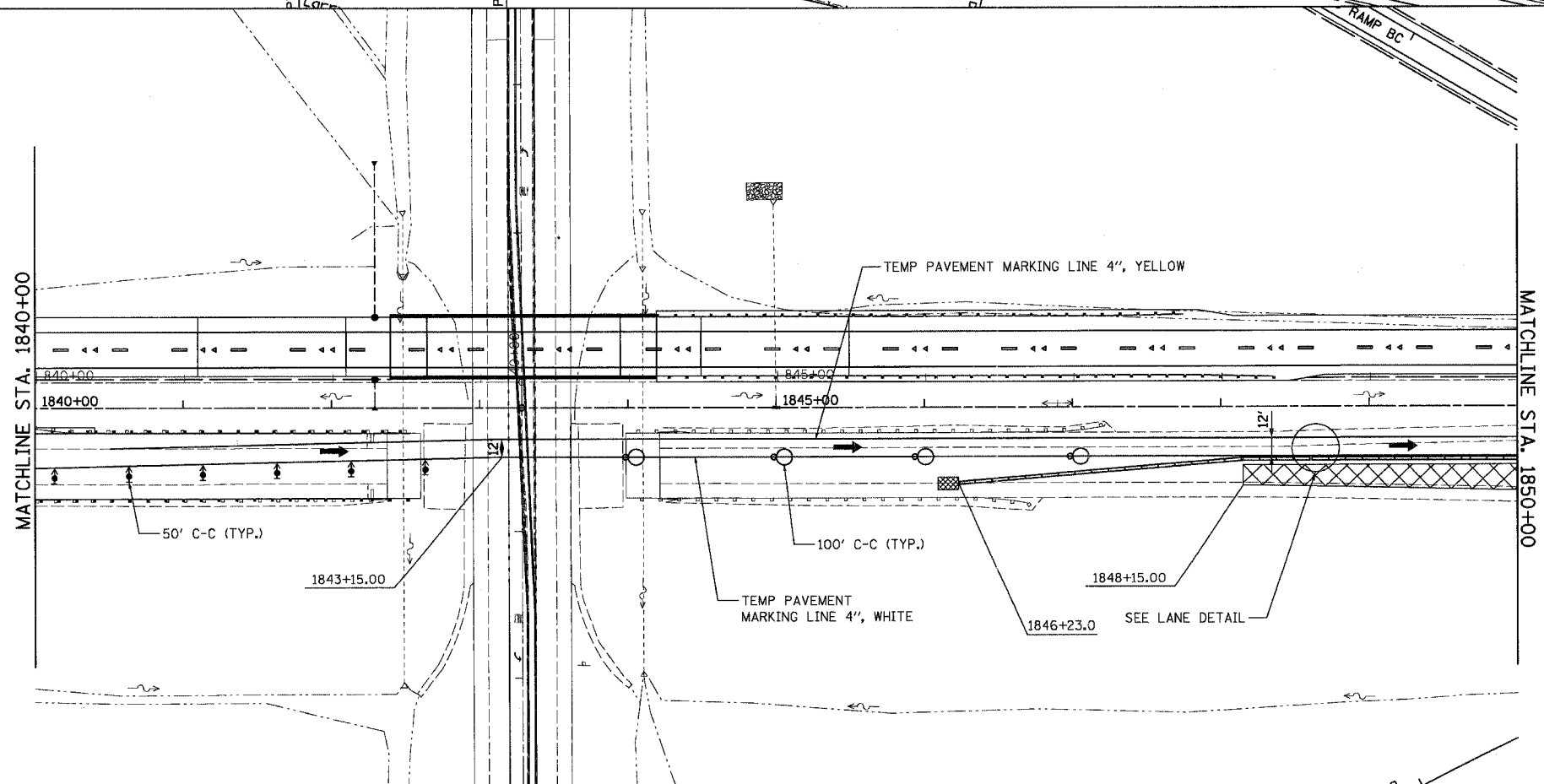
CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	94
STA. 1830+00		TO STA. 1850+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

TRAFFIC CONTROL AS PER IDOT STANDARD 701402



LANE DETAIL

LANE DETAIL



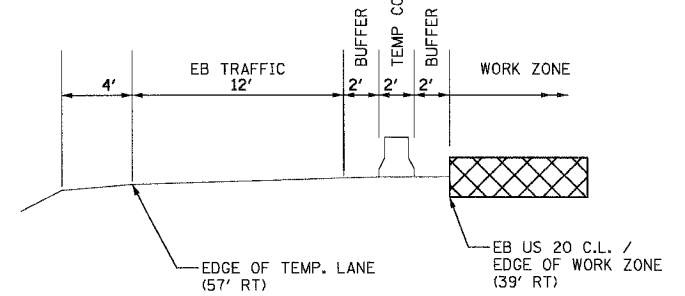
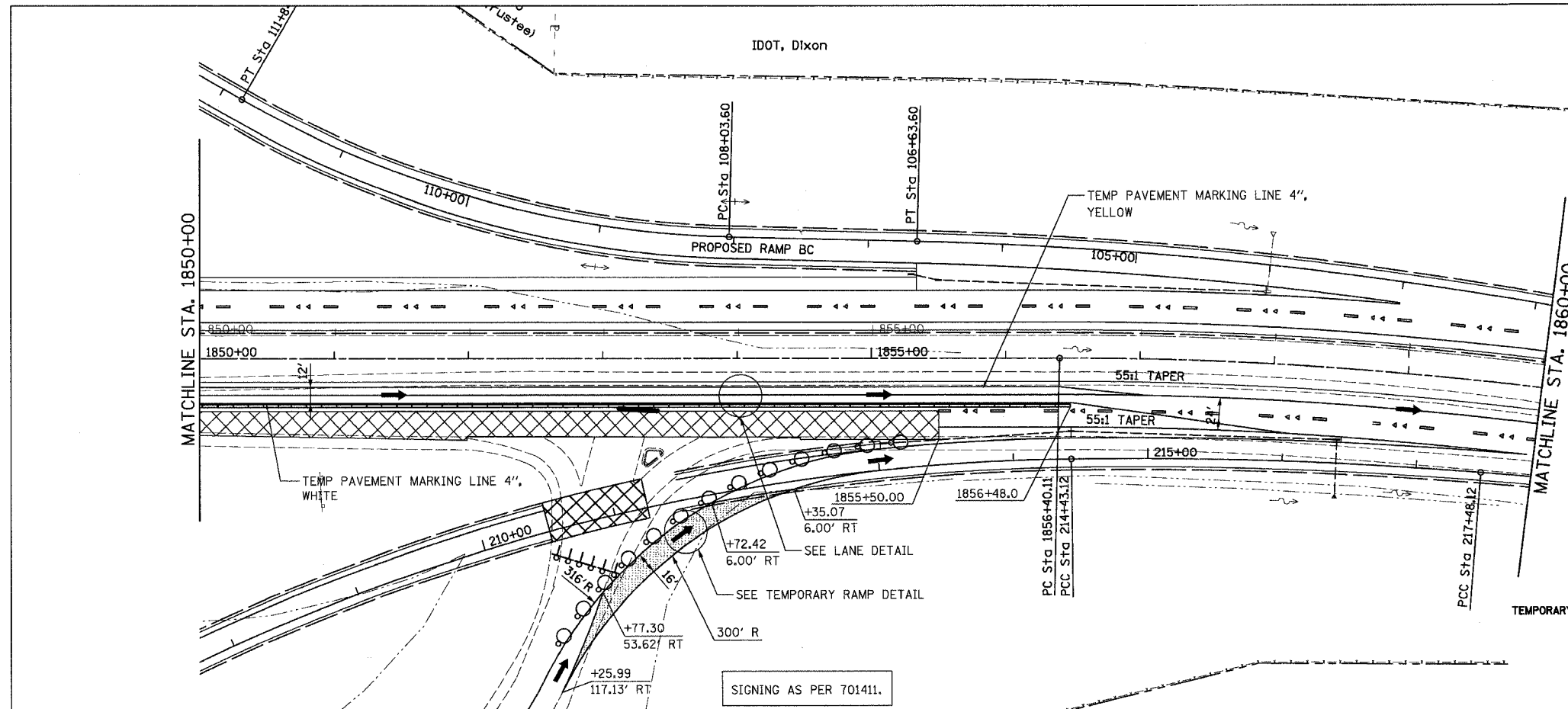
LEGEND

- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER

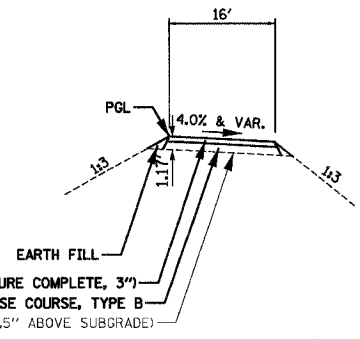
REVISIONS	
NAME	DATE
WILL REEDY	10/5/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION STAGING
 STAGE III
 U.S. ROUTE 20
 STA. 1830+00 TO STA. 1850+00
 SCALE: VERT. 1" = 50'
 HORIZ. 1" = 50'
 DATE 7/15/05
 DRAWN BY MBT
 CHECKED BY JC

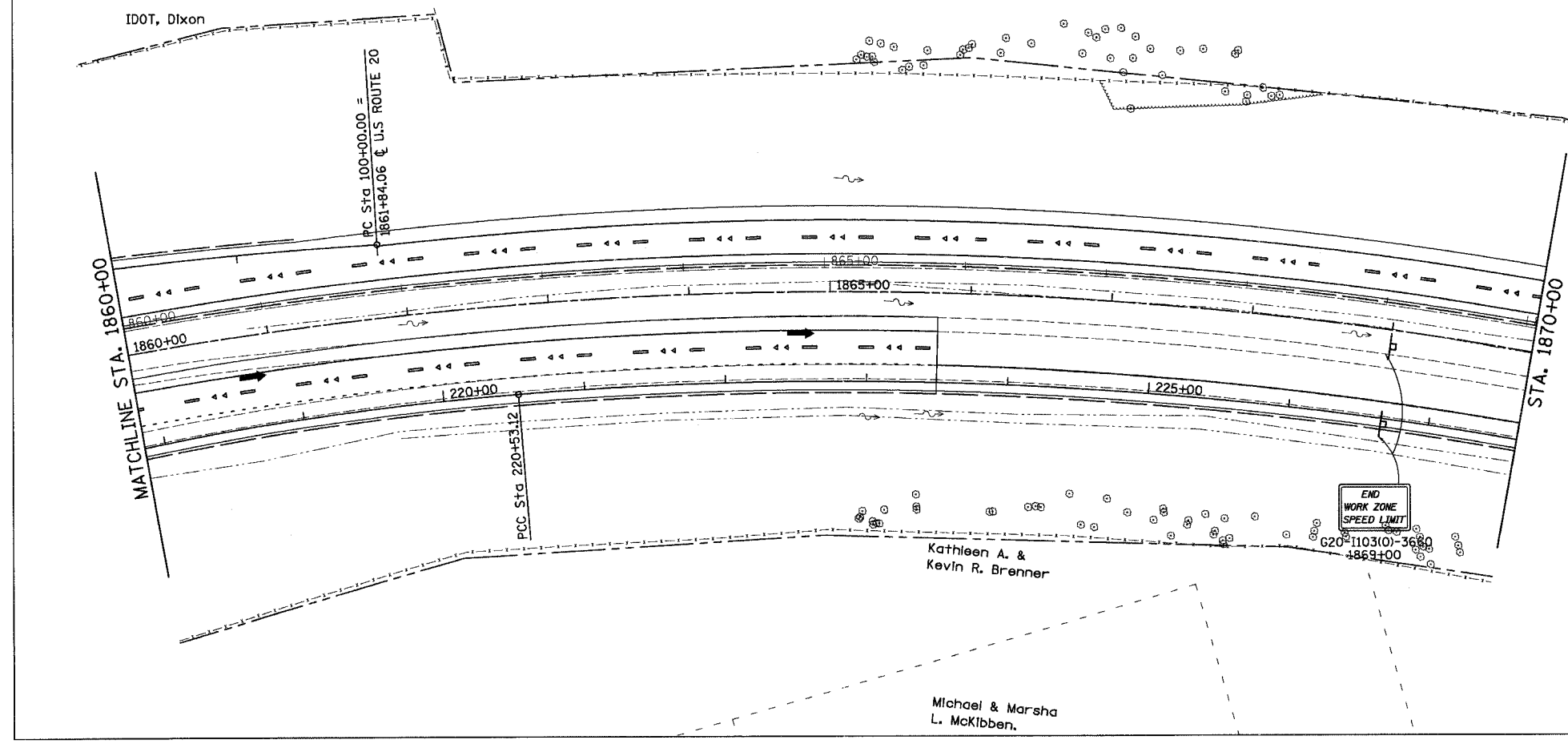
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	95
STA. 1850+00 TO STA. 1870+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



LANE DETAIL



TEMPORARY RAMP DETAIL



LEGEND

- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER

REVISIONS	
NAME	DATE
WILL REEDY	10/5/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION STAGING
 U.S. ROUTE 20
 STAGE III
 STA. 1850+00 TO STA. 1870+00

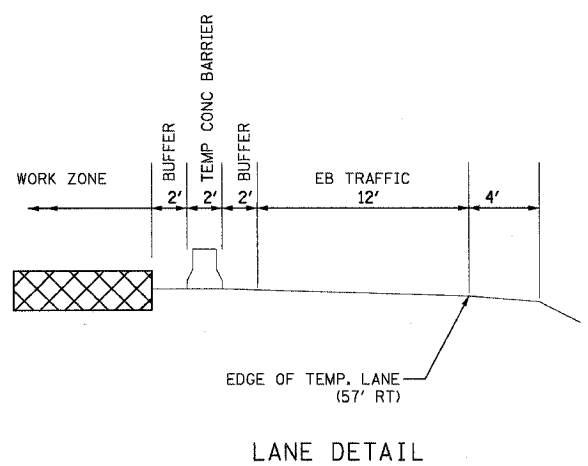
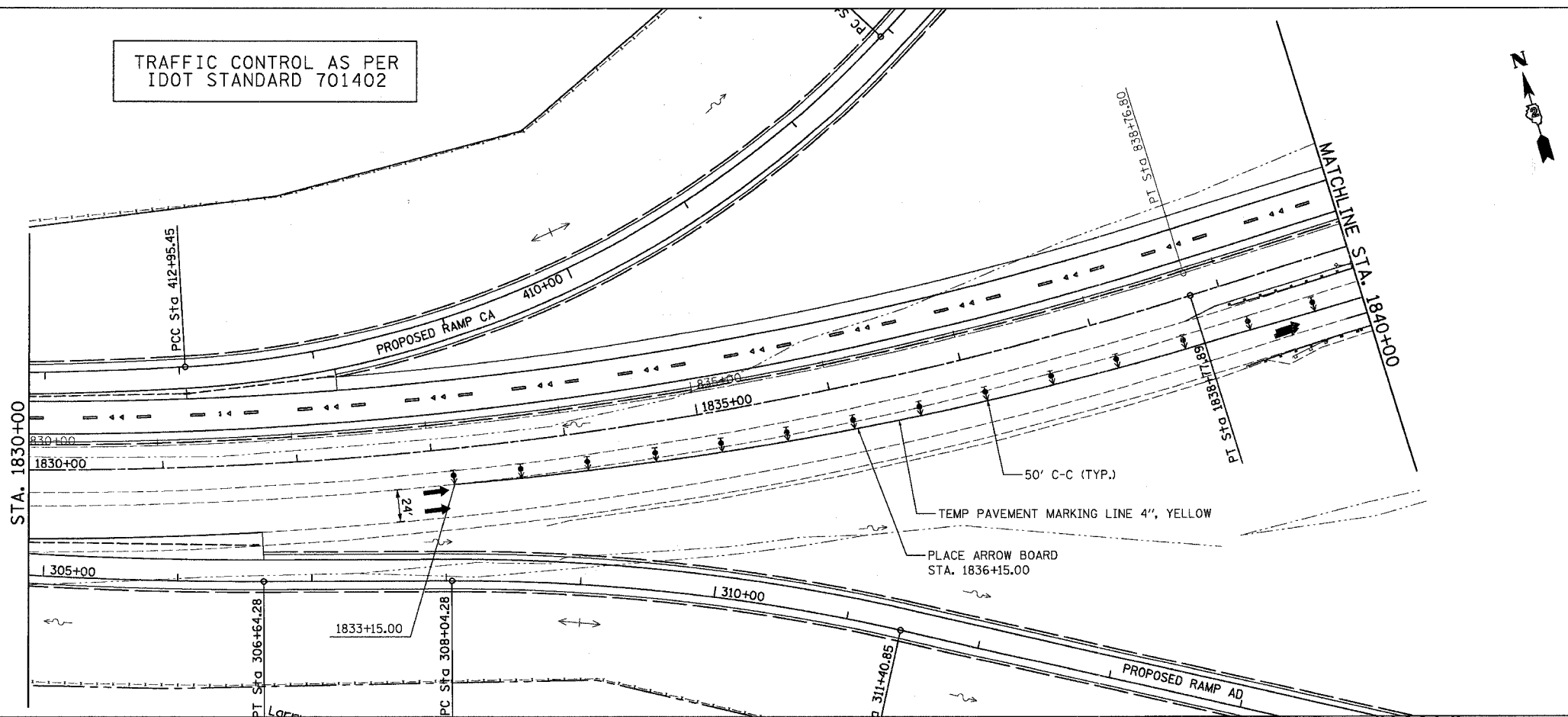
SCALE: VERT. 1"=50'
 HORIZ. 1"=50'
 DATE 7/15/05

DRAWN BY MBT
 CHECKED BY JC

CONTRACT NO. 64577

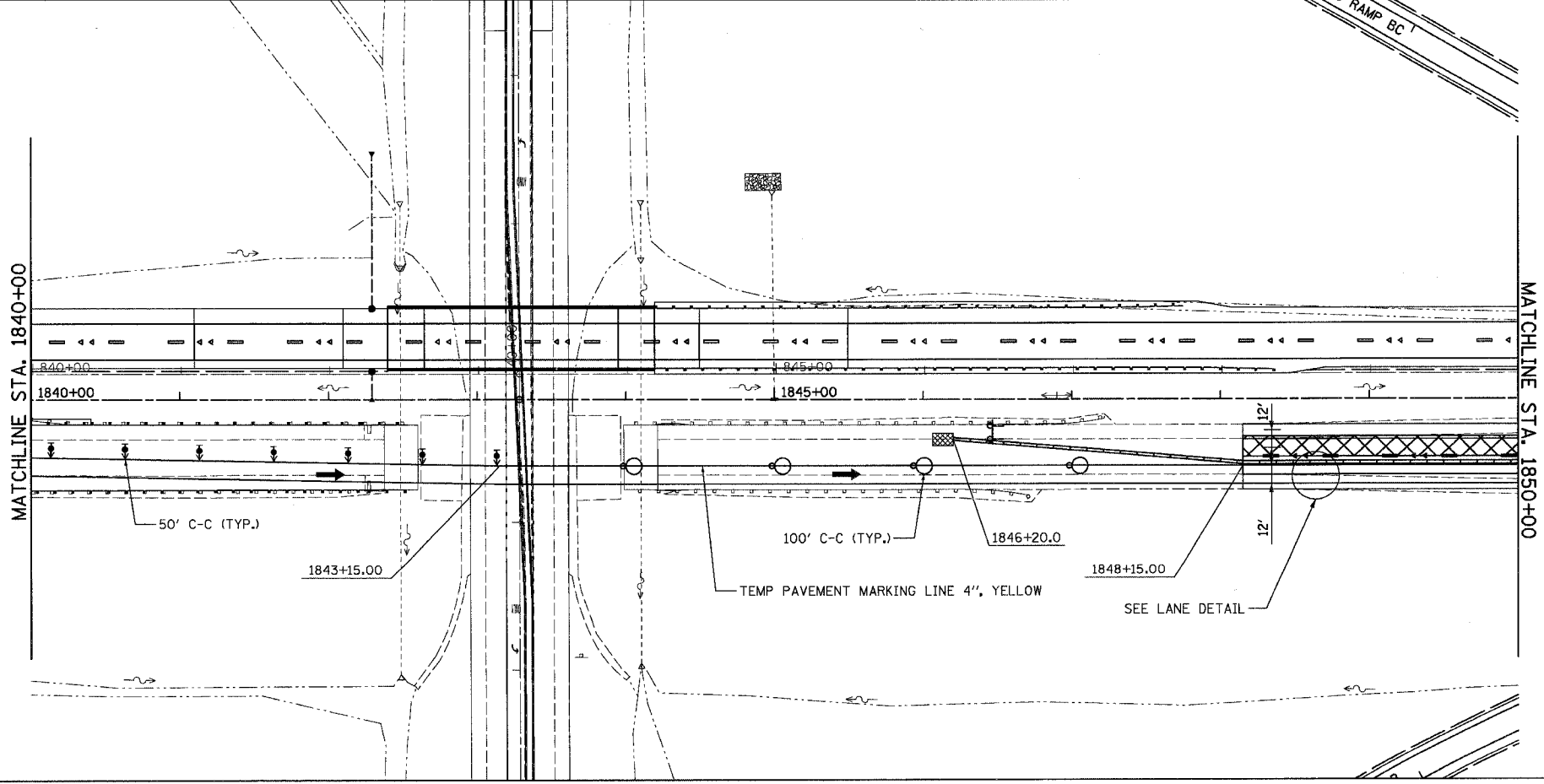
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	96
STA. 1830+00 TO STA. 1850+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

TRAFFIC CONTROL AS PER IDOT STANDARD 701402



LEGEND

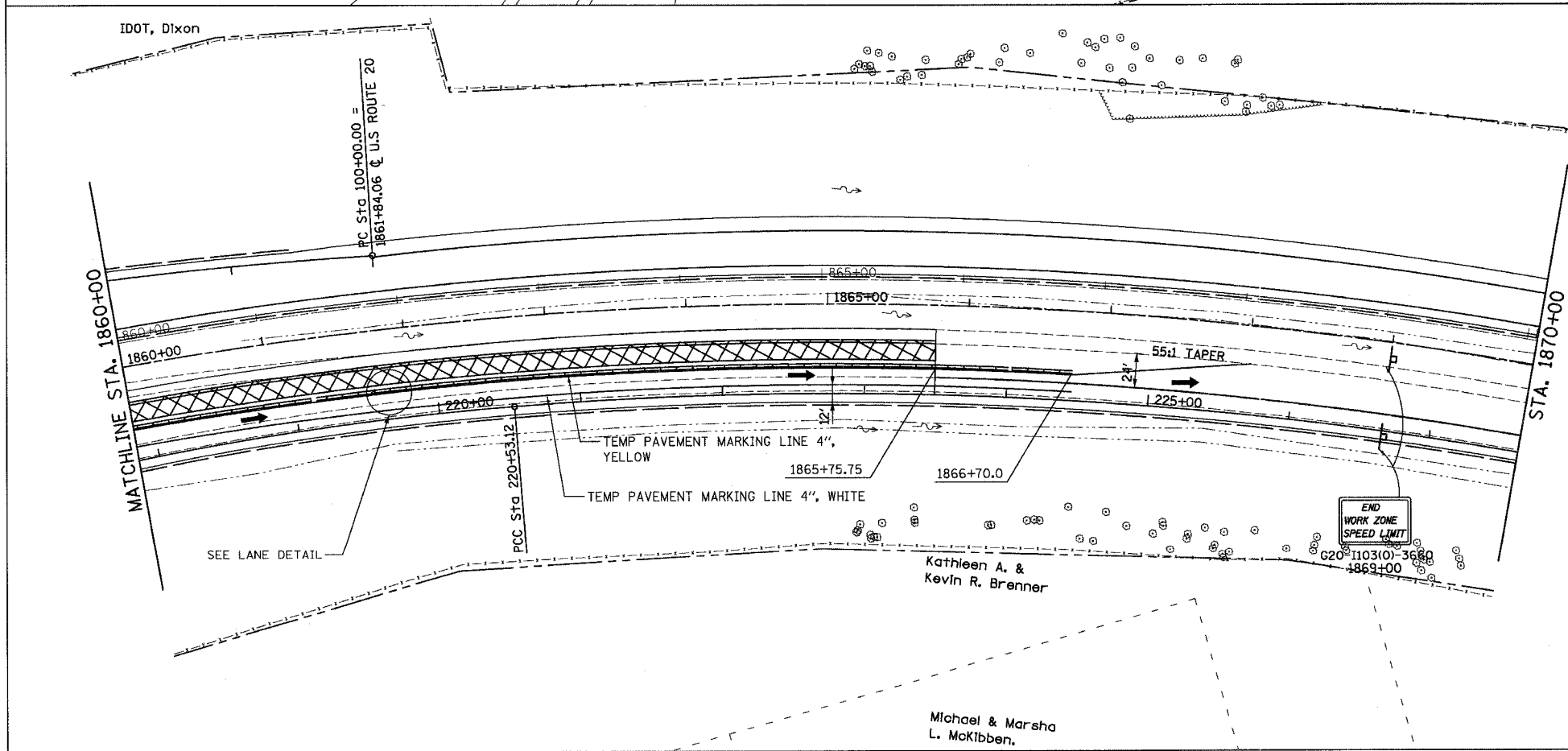
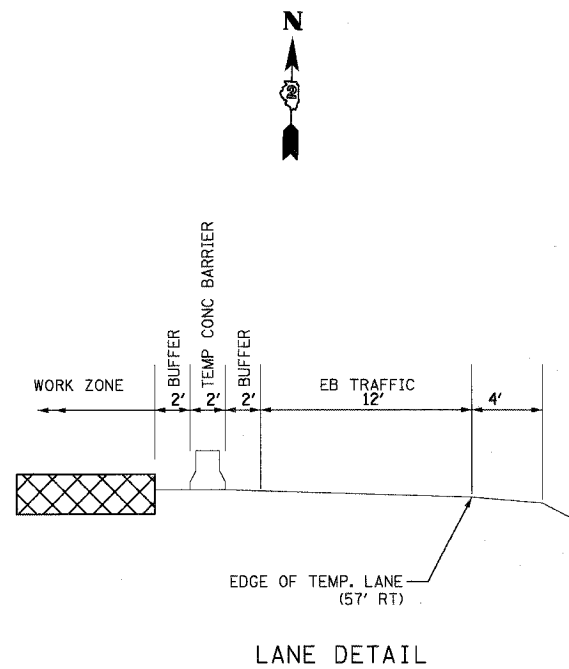
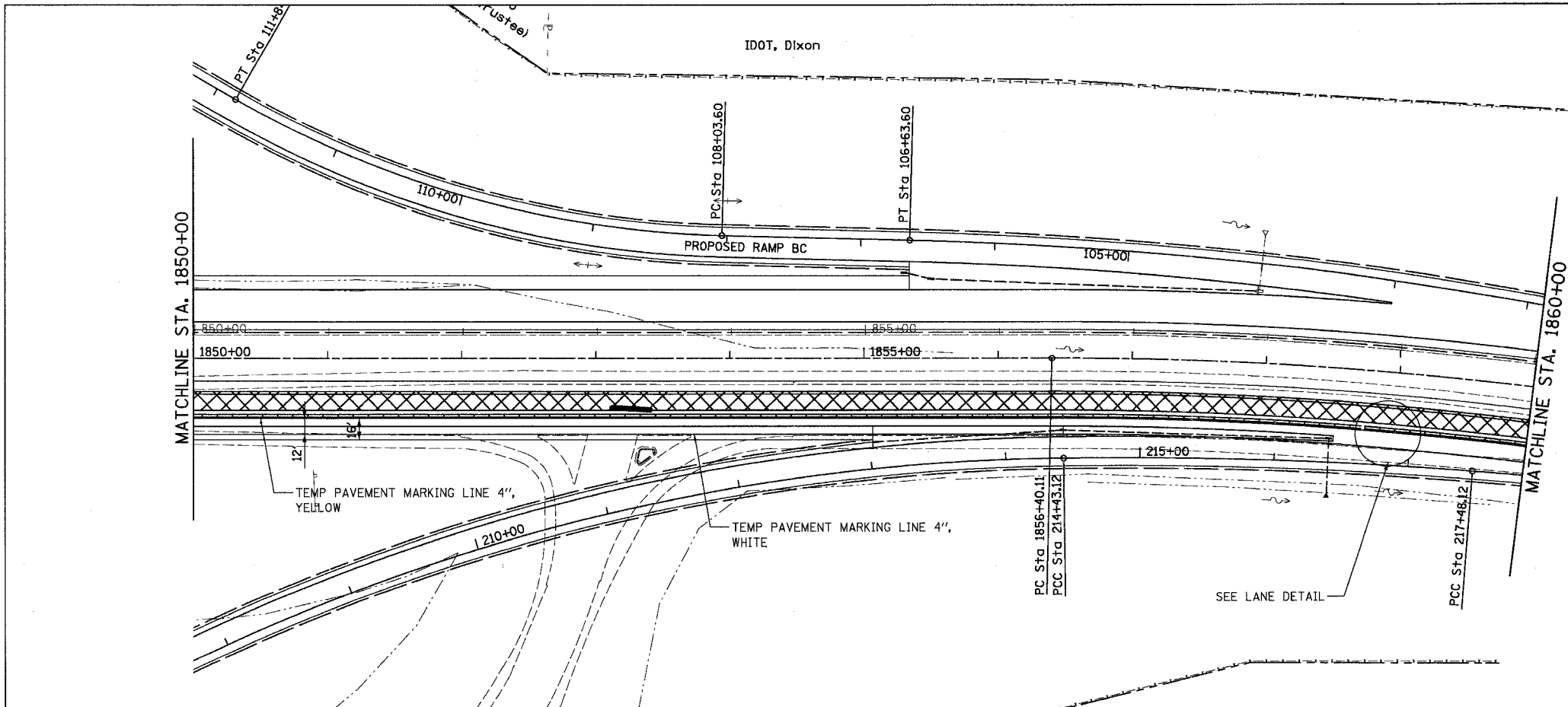
- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER



REVISIONS	
NAME	DATE
WILL REEDY	10/5/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
 CONSTRUCTION STAGING
 STAGE IV
 U.S. ROUTE 20
 STA. 1830+00 TO STA. 1850+00
 SCALE: VERT. 1" = 50'
 HORIZ. 1" = 50'
 DATE 7/15/05
 DRAWN BY MBT
 CHECKED BY JC

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	97
STA. 1850+00 TO STA. 1870+00				
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



LEGEND

- WORK AREA
- TEMP. PAVEMENT
- BARRICADE TY III WITH FLASHING LIGHTS
- BARRICADE TY II
- SIGN
- VERTICAL PANEL
- BARRICADE OR DRUM WITH STEADY BURN LIGHTS
- TRAFFIC FLOW
- FLASHING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER

REVISIONS	
NAME	DATE
WILL REEDY	10/5/05

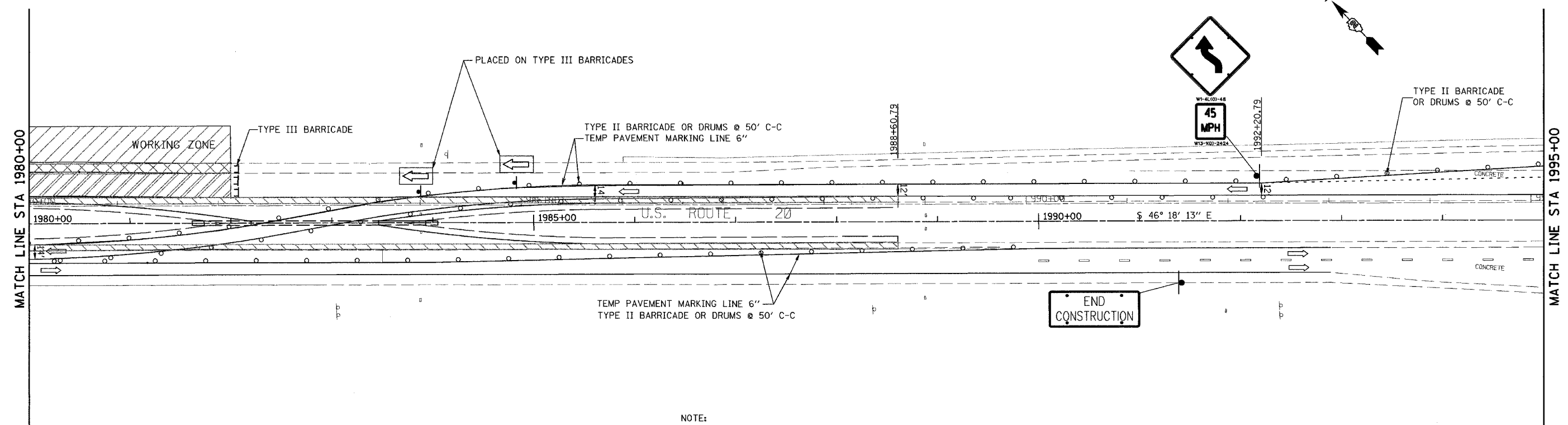
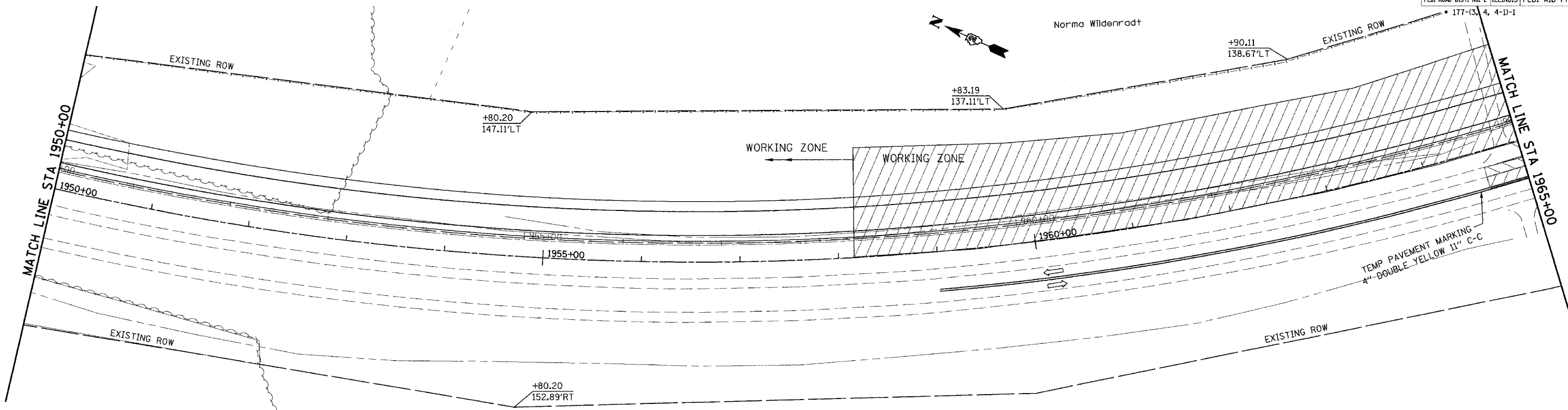
ILLINOIS DEPARTMENT OF TRANSPORTATION

CONSTRUCTION STAGING
STAGE IV
U.S. ROUTE 20
STA. 1850+00 TO STA. 1870+00

SCALE: VERT. 1"=50'
HORIZ. 1"=50'
DATE 7/15/05

DRAWN BY MBT
CHECKED BY JC

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	*	STEPHENSON	290	98
STA. 1965+00		TO STA. 1995+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				

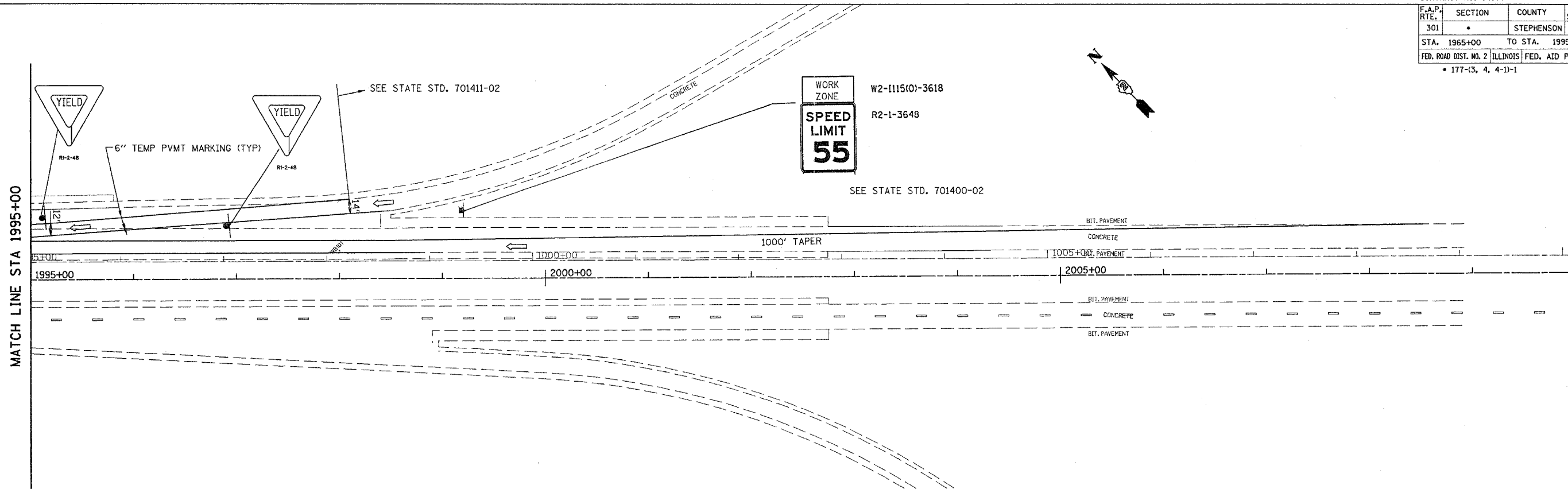


NOTE:
SEE STATE STD. 701416-05 FOR THE MOST CURRENT SIGNING REQUIREMENTS.

REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
US ROUTE 20
 SCALE: VERT. DRAWN BY TVN
 HORIZ. CHECKED BY JAM
 DATE 10/28/05

CONTRACT NO. 64577				
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	.	STEPHENSON	290	99
STA. 1965+00		TO STA. 1995+00		
FED. ROAD DIST. NO. 2		ILLINOIS	FED. AID PROJECT	
• 177-13, 4, 4-1-1				



REVISIONS	
NAME	DATE
PRE-FINAL	05/24/05

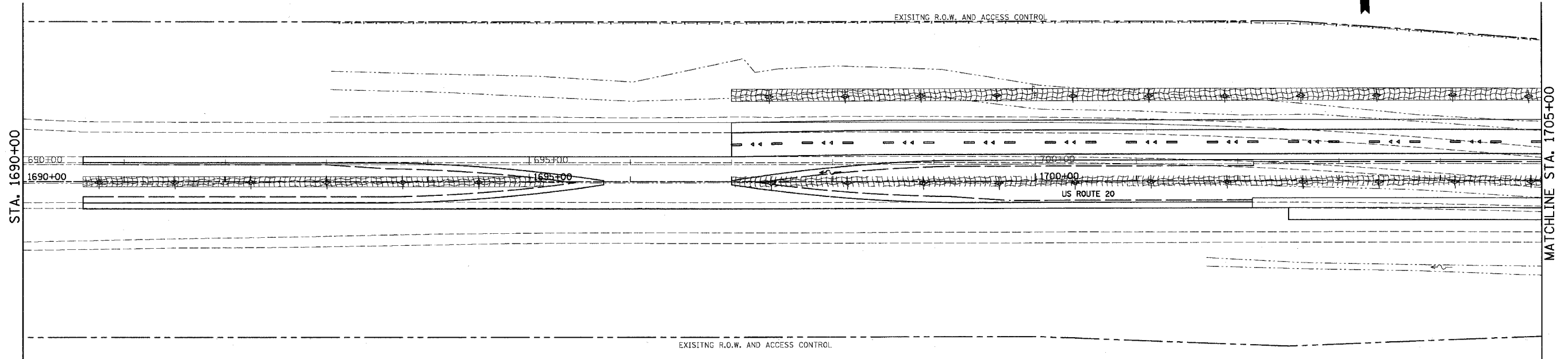
ILLINOIS DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
US ROUTE 20

SCALE: VERT. _____
 HORIZ. _____
 DATE 10/28/05

DRAWN BY TVN
 CHECKED BY JAM

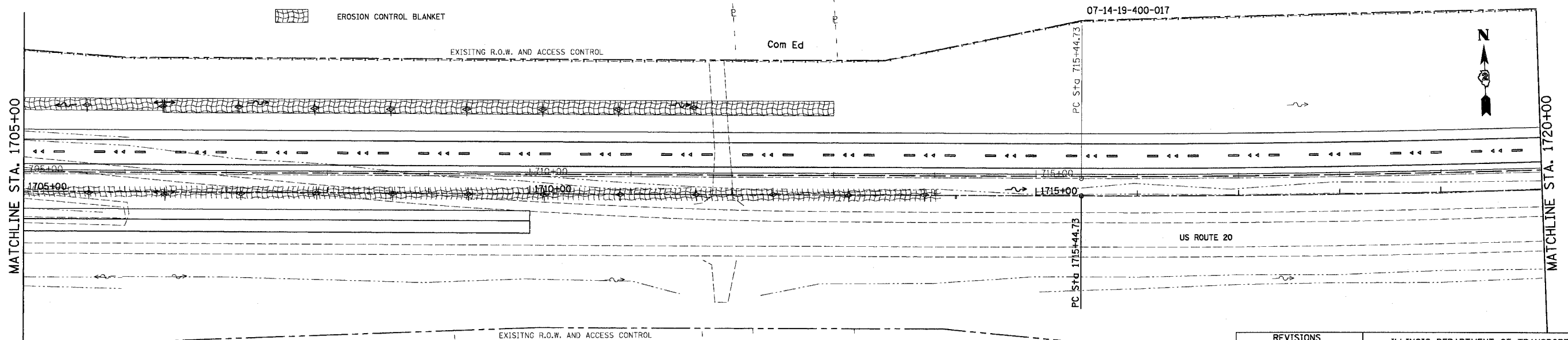
CONTRACT NO. 64577

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301		STEPHENSON	290	100
STA. 1690+00		TO STA. 1720+00		
FED. ROAD DIST. NO. 2 ILLINOIS FED. AID PROJECT				



LEGEND

- ◆ TEMPORARY DITCH CHECK
- ◆ PERMANENT DITCH CHECK
- ▨ EROSION CONTROL BLANKET
- ◆ INLET & PIPE PROTECTION
- PERIMETER EROSION BARRIER



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

EROSION CONTROL
U.S. ROUTE 20
STA. 1690+00 TO STA. 1720+00

SCALE: VERT. 1" = 20'
HORIZ. 1" = 50'

DATE 7/15/05

DRAWN BY MBT
CHECKED BY JC