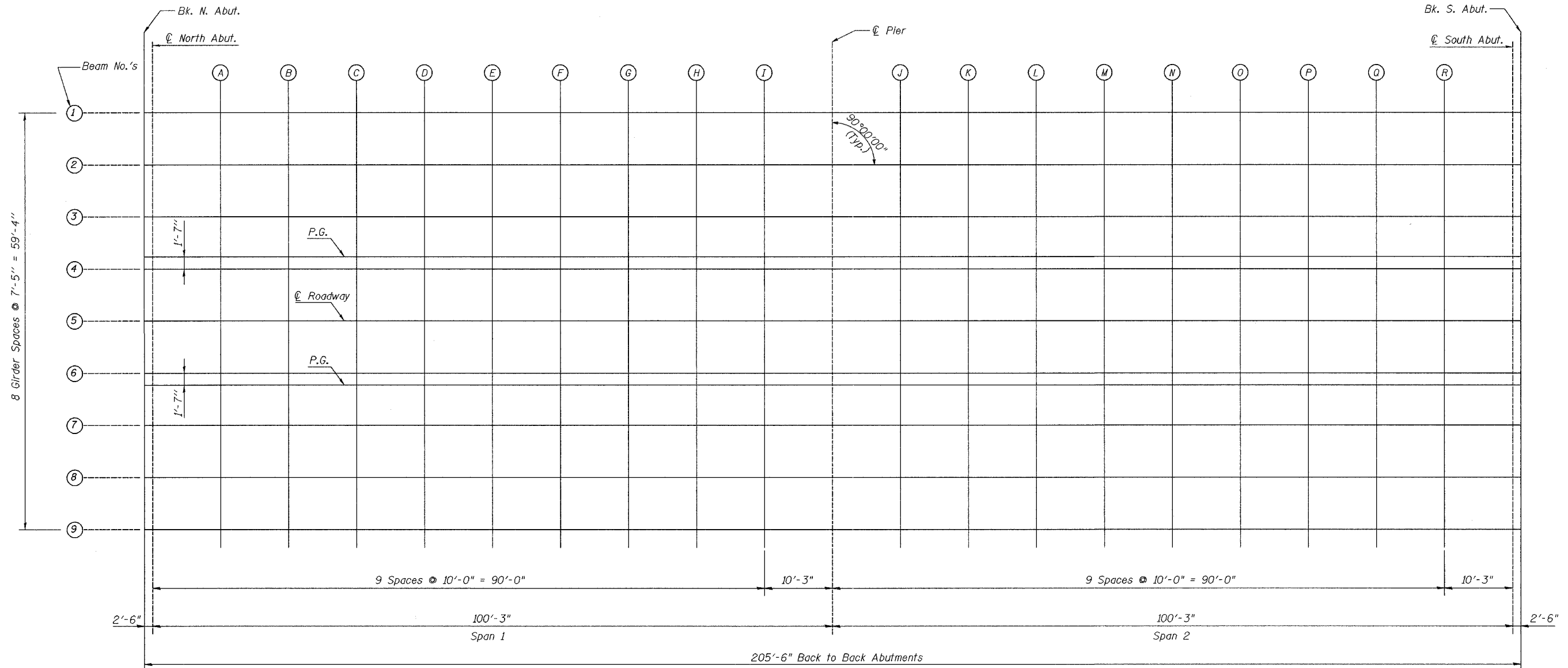
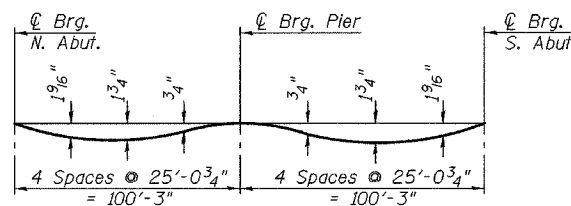


F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
623	32-2 HBR	GRUNDY	171	96
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 66412				
SHEET S3 OF S20				



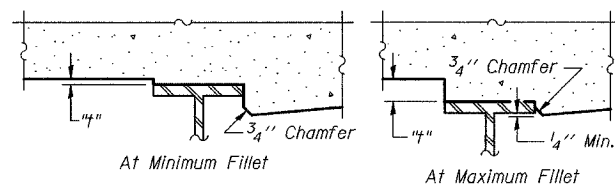
PLAN



DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on Sheets S4 and S5.



To determine "f": After all structural steel has been erected, elevations of the top flanges of the girders shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on Sheets S4 and S5, minus slab thickness, equals the fillet heights "f" above the top flanges of the girders.

FILLET HEIGHTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SENECA ROAD
 OVER F.A.I. 80
 F.A.P. 623 SEC. 32-2 HBR GRUNDY CO.
 STRUCTURE No. 032-0114
 STATION 19+49.99
 TOP OF SLAB ELEVATION LAYOUT

SCALE: NONE
 DATE: OCTOBER, 2005

DRAWN BY: NJH
 CHECKED BY: JLG

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 Manchester, NH 03101