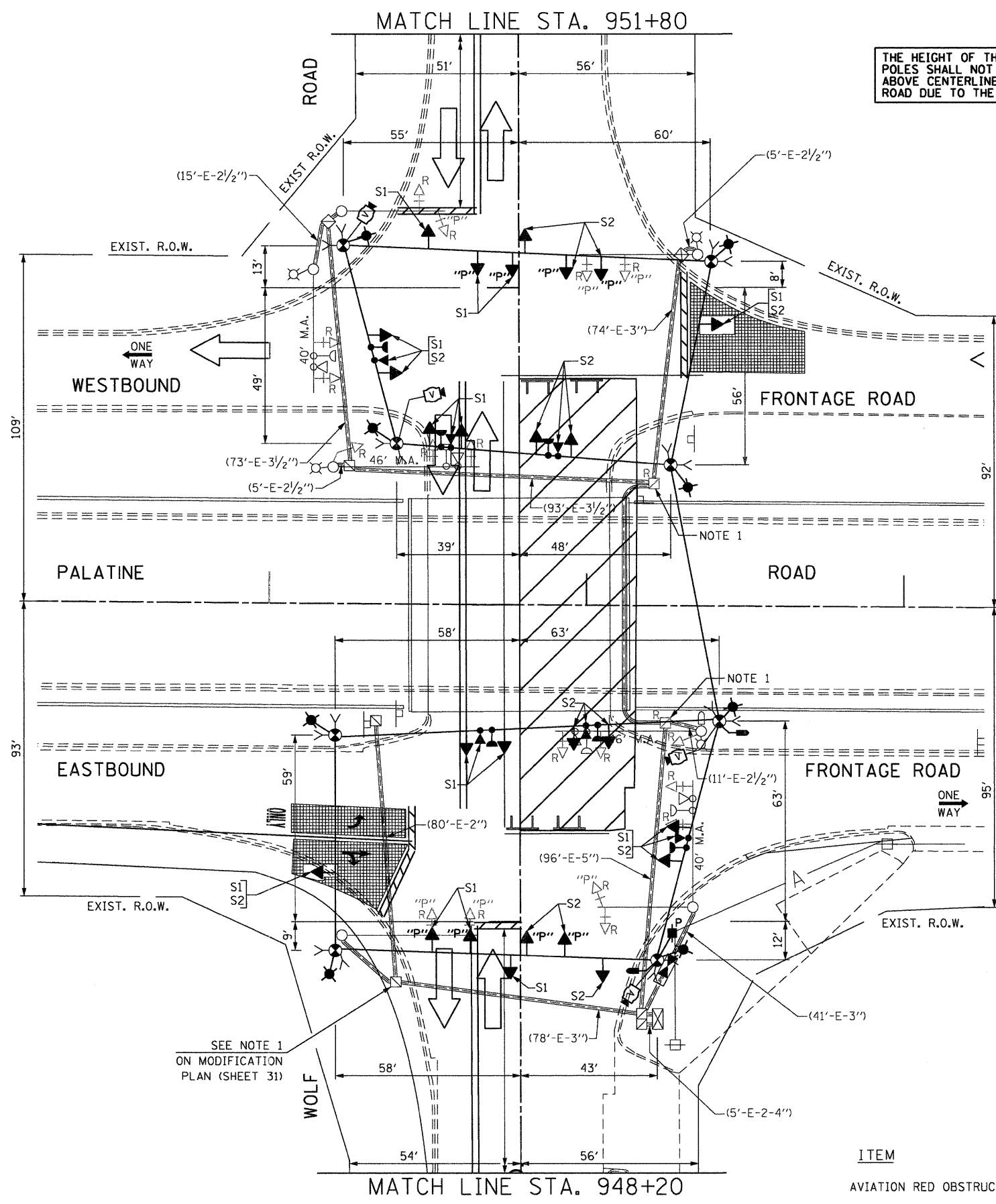


NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

THE HEIGHT OF THE TEMPORARY WOOD POLES SHALL NOT EXCEED 26 FEET ABOVE CENTERLINE ELEVATION OF WOLF ROAD DUE TO THE AIRPORT.



SPECIAL NOTE TO CONTRACTOR:
 THE EXISTING TRAFFIC SIGNAL INSTALLATION SHALL BE DISCONNECTED AND ALL THE EXISTING SIGNAL HEADS SHALL BE REMOVED WHEN THE TEMPORARY TRAFFIC SIGNAL INSTALLATION IS IN PLACE AND IN OPERATION. UPON COMPLETION OF THE CONSTRUCTION ACTIVITIES, AND PRIOR TO REMOVAL OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION, THE EXISTING TRAFFIC SIGNAL INSTALLATION SHALL BE MADE OPERATIONAL AS DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION".
 THE EXISTING SIGNAL HEADS SHALL BE REPLACED WITH LED SIGNAL HEADS AS SHOWN IN THE "EXISTING TRAFFIC SIGNAL PLAN MODIFICATION" AND SHALL BE PAID FOR AS APPLICABLE LED SIGNAL HEAD PAY ITEMS AS SHOWN IN THE SCHEDULE OF QUANTITIES. THE NEW LED SIGNAL HEADS SHALL NOT BE INSTALLED UNTIL THE END OF THE CONTRACT, JUST PRIOR TO MAKING THE EXISTING TRAFFIC SIGNAL OPERATIONAL. SO AS TO AVOID INTERFERENCE WITH SOME OF THE TEMPORARY TRAFFIC SIGNAL HEADS.
 THE EXISTING TRAFFIC SIGNAL CONTROLLER, MAST ARMS, POSTS, HANDHOLES AND APPURTENANCES ARE SHOWN FOR INFORMATION ONLY AND SHALL REMAIN IN PLACE, EXCEPT FOR THE HANDHOLE IN THE NORTHEAST CORNER OF EASTBOUND FRONTAGE ROAD AND A HANDHOLE IN THE SOUTHEAST CORNER OF WESTBOUND FRONTAGE ROAD.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACTOR'S BID PRICE.

- 1 EACH CONTROLLER AND CABINET, COMPLETE
- 2 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, BRACKET MOUNTED
- 6 EACH SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
- 4 EACH SIGNAL HEAD, 1-FACE, 4-SECTION, MAST ARM MOUNTED
- 2 EACH OPTICALLY PROGRAMMED SIGNAL HEAD, 1-FACE, 3-SECTION, MAST ARM MOUNTED
- 4 EACH OPTICALLY PROGRAMMED SIGNAL HEAD, 1-FACE, 4-SECTION, MAST ARM MOUNTED
- 16 EACH TRAFFIC SIGNAL BACKPLATE

NOTE 1: THE EXISTING HANDHOLE SHALL BE REMOVED IN A MANNER THAT WILL NOT DAMAGE THE EXISTING CONDUITS WHICH WILL REMAIN IN PLACE FOR FUTURE USE. A NEW DOUBLE HANDHOLE SHALL BE BUILT IN PLACE OF REMOVED EXISTING HANDHOLE AS SHOWN IN THE "EXISTING TRAFFIC SIGNAL PLAN MODIFICATION".

NOTE 2: THE VIDEO DETECTION ZONES SHOWN ON THE PLANS ARE FOR STAGE 1 AND SHALL BE REDEFINED FOR EACH CONSTRUCTION STAGE AS A PART OF "TEMPORARY TRAFFIC SIGNAL INSTALLATION" WORK.

SEE NOTE 1 ON MODIFICATION PLAN (SHEET 31)

ITEM	EXISTING	PROPOSED
AVIATION RED OBSTRUCTION LIGHT		
AVIATION PHOTOELECTRIC CONTROL		