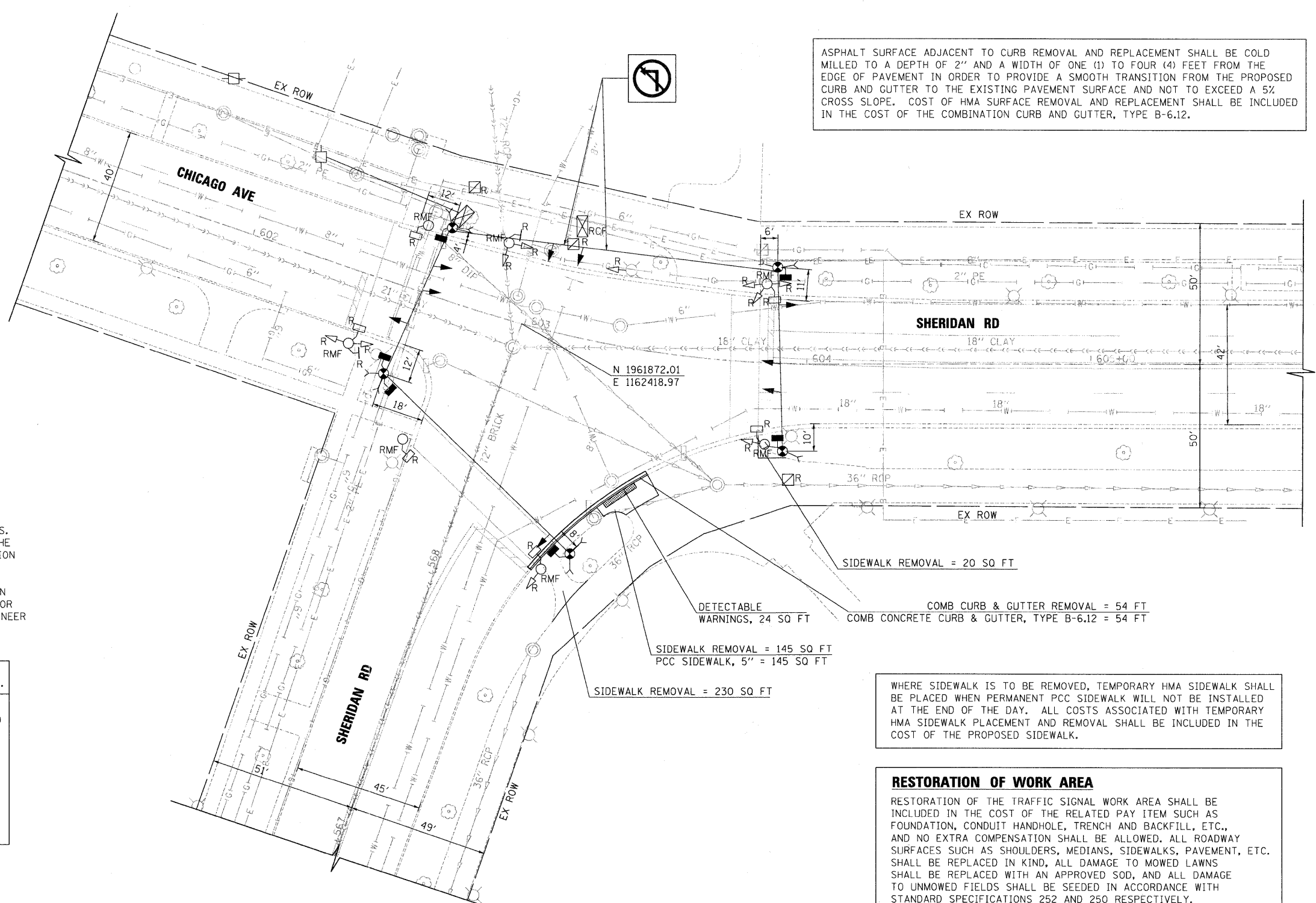


NOTES FOR TEMPORARY TRAFFIC SIGNALS

- ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
- ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
- ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
- ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
- ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
- THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON. IF NO TRAFFIC STAGING IS IN PLACE OR WILL BE STAGED ON THE DAY OF THE TURN ON.
- UNINTERRUPTIBLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE (UPS) IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL. TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
- TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
- DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
- WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

ASPHALT SURFACE ADJACENT TO CURB REMOVAL AND REPLACEMENT SHALL BE COLD MILLED TO A DEPTH OF 2" AND A WIDTH OF ONE (1) TO FOUR (4) FEET FROM THE EDGE OF PAVEMENT IN ORDER TO PROVIDE A SMOOTH TRANSITION FROM THE PROPOSED CURB AND GUTTER TO THE EXISTING PAVEMENT SURFACE AND NOT TO EXCEED A 5% CROSS SLOPE. COST OF HMA SURFACE REMOVAL AND REPLACEMENT SHALL BE INCLUDED IN THE COST OF THE COMBINATION CURB AND GUTTER, TYPE B-6.12.



BEFORE REMOVING ANY EQUIPMENT OR HANDHOLES, THE CONTRACTOR SHALL CONFIRM THAT THE ITEM IS RELATED TO THE EXISTING TRAFFIC SIGNAL SYSTEM.

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY DIRECTION OF THE CITY. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

1	EACH	CONTROLLER AND CABINET (COMPLETE)
3	EACH	SIGNAL HEAD, 1-FACE, 3-SECTION
3	EACH	SIGNAL HEAD, 2-FACE, 3-SECTION
2	EACH	SIGNAL HEAD, 1-FACE, 4-SECTION
7	EACH	TRAFFIC SIGNAL POST
6	EACH	PEDESTRIAN SIGNAL HEAD, 1-FACE
2	EACH	INTERNALLY ILLUMINATED SIGN

SIDEWALK REMOVAL = 20 SQ FT
 COMB CURB & GUTTER REMOVAL = 54 FT
 COMB CONCRETE CURB & GUTTER, TYPE B-6.12 = 54 FT
 DETECTABLE WARNINGS, 24 SQ FT
 SIDEWALK REMOVAL = 145 SQ FT
 PCC SIDEWALK, 5" = 145 SQ FT
 SIDEWALK REMOVAL = 230 SQ FT

WHERE SIDEWALK IS TO BE REMOVED, TEMPORARY HMA SIDEWALK SHALL BE PLACED WHEN PERMANENT PCC SIDEWALK WILL NOT BE INSTALLED AT THE END OF THE DAY. ALL COSTS ASSOCIATED WITH TEMPORARY HMA SIDEWALK PLACEMENT AND REMOVAL SHALL BE INCLUDED IN THE COST OF THE PROPOSED SIDEWALK.

RESTORATION OF WORK AREA
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCLUDED IN THE COST OF THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND, ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



FILE NAME = 04_023301-pln-007.dgn	USER NAME = USER0291	DESIGNED - THG/MPW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN SHERIDAN ROAD AND CHICAGO AVENUE	F.A.U. RTE. = 2853	SECTION = 10-00258-00TL	COUNTY = COOK	TOTAL SHEETS = 38	SHEET NO. = 4	
PLOT SCALE = 1:20	DRAWN - THG/MPW	CHECKED - SRS	REVISED -			SCALE: 1" = 20'	SHEET NO. OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT			
PLOT DATE = 8/29/2011	DATE = 08/16/11	REVISED -	REVISED -			CONTRACT NO. 63630					