1-20-2012 LETTING ITEM 154

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353 #354	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 76E06		

#353+1=354

LIST OF STRUCTURAL WORK

NORTH APPROACH SLAB ONLY
RELOCATED IL RTE 3 OVER I–70
STR. NO. 082–0328 PROPOSED RELOCATED IL RTE 3 BRIDGE
BACK OF ABUTMENT STA. 1684+40.93

SOUTH APPROACH SLAB ONLY
RELOCATED IL RTE 3 OVER TRRA AND ST. CLAIR AVE.
STR. NO. 082–0329 PROPOSED RELOCATED IL RTE 3 BRIDGE
BACK OF ABUTMENT STA. 1677+17.25

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

FAP ROUTE 998 (RELOCATED I–70)
SECTION 82–2–1K

ST. CLAIR COUNTY
C–98–093–10
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE AT RELOCATED I–70 AND ITS COMMUNICATION

D–98–059–08

LOCATION OF SECTION INDICATED THUS:–

FOR INDEX OF SHEETS, SEE SHEET NO. 2

INTERSTATE 70
ADT = 59,940 (2030) 6,594 (30) INTERSTATE 48.96 (PCC–20)
DESIGN SPEED = 60 MPH
POSTED SPEED = 55 MPH

RELOCATED IL ROUTE 3
ADT = 34,760 (2030) 3,476 (30) PRINCIPAL ARTERIAL 18.46 (PCC–20)
DESIGN SPEED = 45 MPH
POSTED SPEED = 40 MPH

RAMPS A–D
DESIGN SPEED = 50 MPH AT GORE, 35 MPH AT END
POSTED SPEED = 30 MPH
RAMP A ADT = 12,610 (2030) 1,214(30) INTERSTATE RAMP 14.30 (PCC–20)
RAMP B ADT = 6,010 (2030) 443(30) INTERSTATE RAMP 7.13 (PCC–20)
RAMP C ADT = 16,940 (2030) 1,690 (30) INTERSTATE RAMP 17.20 (PCC–20)
RAMP D ADT = 16,940 (2030) 1,690 (30) INTERSTATE RAMP 17.20 (PCC–20)

CONNECTOR ROAD
ADT = 26,550 (2030) 2,655 (30) COLLECTOR 9.40 (PCC–20)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH

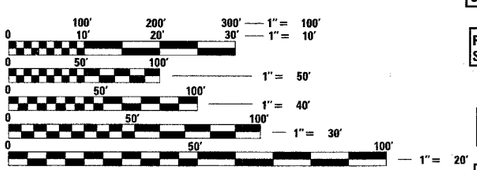
EXCHANGE AVENUE
ADT = 16,940 (2030) 1,694 (30) COLLECTOR 7.26 (PCC–20)
DESIGN SPEED = 30 MPH
POSTED SPEED = 25 MPH

EXISTING IL ROUTE 3 (ST. CLAIR AVE.)
ADT = 9,000 (2008), 26,190 (2030) 2,619 (30) MINOR ARTERIAL 7.60 (FD–20)
DESIGN SPEED = 40 MPH (S. OF TRRA CROSSING)
DESIGN SPEED = 50 MPH (N. OF TRRA CROSSING)
POSTED SPEED = 35 MPH (S. OF TRRA CROSSING)
POSTED SPEED = 45 MPH (N. OF TRRA CROSSING)

1ST STREET
ADT = 1,350 (2008), 1,700 (2030)
DESIGN SPEED = 35 MPH
POSTED SPEED = 30 MPH

PROJECT LOCATED IN: FAIRMONT CITY, ILLINOIS
EAST ST. LOUIS, ILLINOIS

PLANS 1"=50'
PROFILES 1"=5'
X–SECTIONS 1"=10' H
1"=5' V



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

RELOCATED I–70 & SOUTHBOUND I–55
STA. 116+90 (I–70) TO STA. 121+85 (I–55)

RAMP D
STA. 250+00.00 TO STA. 258+00.00

RAMP C
STA. 110+40.00 TO STA. 120+52.95

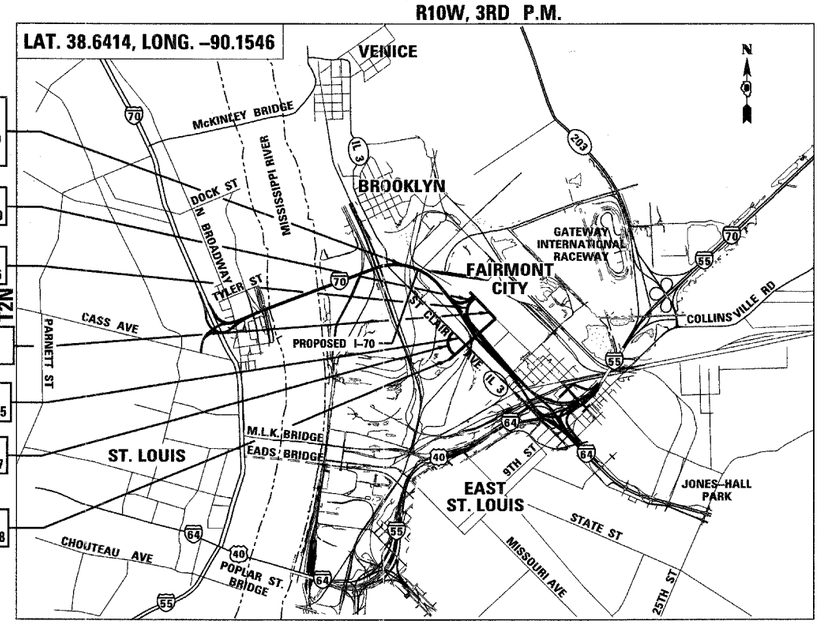
EXCHANGE AVE.
STA. 35+11.34 TO STA. 55+41.84

ST. CLAIR AVE.
STA. 107+15.07 TO STA. 125+36.55

ST. CLAIR AVE. CONNECTOR
STA. 20+00.00 TO STA. 32+54.37

RELOCATED IL ROUTE 3
STA. 1668+15.65 TO STA. 1693+28.38

R10W, 3RD P.M.



0 1 MI 2 MI 3 MI
SCALE
CANTEEN TOWNSHIP

GROSS / NET LENGTH OF PROJECT:	20,658' (3.91 MI)
I–70 / I–55	11,462' (2.17 MI)
EXCHANGE AVE.	2,031' (0.385 MI)
RAMP C	1,013' (0.192 MI)
RAMP D	800' (0.152 MI)
ST. CLAIR AVE.	1,585' (0.300 MI)
ST. CLAIR AVE. CONNECTOR	1,254' (0.238 MI)
RELOCATED IL ROUTE 3	2,513' (0.476 MI)

ROBERT J. STERN
062-040104
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
ROBERT J. STERN
EXPIRES: 11/30/2011
DATE: 10/17/2011
SHEETS: 1-79, 110-137, 175-183, 217, 227-260, 277-353

STATE OF ILLINOIS
BYRON T. DANLEY
081-004737
CHICAGO IL
LICENSED STRUCTURAL ENGINEER
BYRON T. DANLEY
EXPIRES: 11/30/12
DATE: 10/17/11
SHEETS: 218-226, 271-276

FRANCIS S. MACK
062-042527
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
FRANCIS S. MACK
EXPIRES: 11/30/2011
DATE: 10/13/2011
SHEETS: 261-270

FRED M. LIN
062-056704
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
FRED M. LIN
EXPIRES: 11/30/2013
DATE: 10/14/2011
SHEETS: 80-109, 184-216

LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

ABNA engineering

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED Oct 25th 2011
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
December 9 2011
Scott E. Stitt P.E. for
acting ENGINEER OF DESIGN AND ENVIRONMENT
December 9 2011
William R. Frenk for
Interim DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PROJECT ENGINEER : MICHAEL D. PRITCHETT (618) 346–3180
PROJECT MANAGER : JANE CHASTAIN MERCER (618) 346–3206

CONTRACT NO. 76E06

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE: 312/616-0000

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INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM.

GENERAL NOTES

1. ILLINOIS STATE LAW REQUIRES A 48 - HOUR NOTICE BE GIVEN TO ALL UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:
   - AmerenCIPS - ELECRIC
   - AmerenIP - GAS
   - AT&T ILLINOIS - COMMUNICATIONS
   - ILLINOIS AMERICAN WATER COMPANY - WATER
   - QWEST SOLUTIONS CENTER - COMMUNICATIONS
   - SHO-ME TECHNOLOGIES - COMMUNICATIONS

   FAIRMONT CITY - SEWER
   METRO EAST SANITARY DISTRICT (M.E.S.D.) - CAHOKIA CANAL

   MEMBERS OF J.U.L.I.E. (800-892-0123, OR 811) ARE INDICATED BY AN •. NON-J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

2. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO THE SPECIAL PROVISIONS FOR POTENTIAL UTILITY CONFLICTS.

3. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO THE FACT THAT THERE ARE WATER MAINS MAINTAINED BY THE ILLINOIS AMERICAN WATER COMPANY (IAWC) AND/OR PRIVATE OWNERS WITHIN THE R.O.W. THE CONTRACTOR SHALL LOCATE THE WATER MAIN LOCATIONS IN CONJUNCTION WITH IAWC AND/OR PRIVATE OWNERS PRIOR TO COMMENCEMENT OF CONSTRUCTION IN THESE AREAS.

4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

5. BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL OVERHEAD, SURFACE, AND UNDERGROUND UTILITIES WITHIN THE PROJECT LIMITS WHETHER OR NOT THE UTILITIES ARE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED OR RELOCATED BY THE CONTRACTOR AT HIS/ HER OWN EXPENSE.

GENERAL NOTES (CONTINUED)

7. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED/REPLACED BY THE CONTRACTOR AT THE CONTRACTORS' OWN EXPENSE.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE EAST ST. LOUIS AND THE FAIRMONT CITY FIRE AND POLICE DEPARTMENTS, AND MESD 14 WORKING DAYS PRIOR TO START OF CONSTRUCTION IN ORDER TO REVIEW AND COORDINATE THE CONSTRUCTION SCHEDULE.
   EAST ST. LOUIS FIRE DEPT. CHIEF THOMAS GRIMMETT (618) 482-6875.
   EAST ST. LOUIS POLICE DEPT. CHIEF LENZIE STEWART (618) 482-6740 OR 482-6700.
   FAIRMONT CITY: FIRE AND POLICE DEPARTMENT: SCOTT PENNY (618) 274-4504 EXT. 2.
   METRO EAST SANITARY DISTRICT (MESD): BOB SHIPLEY - (618) 452-9400

9. CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE STATE ARCHAELOGICAL COORDINATOR - JOSEPH M. GALLOY, Ph.D., RPA (618) 251-3922 - 14 WORKING DAYS PRIOR TO START OF ANY PAVEMENT OR FOUNDATION REMOVAL.

10. THE EXISTING TOPOGRAPHY SHOWN ON THE PLANS REFLECTS THE FINAL CONDITION AFTER THE COMPLETION OF CONTRACTS 76C39, 76C40, 76C44, 76C68, 76C69, GRADING FROM CONCURRENT CONTRACT 76C41, AND FROM THE CONCURRENT REL IL 3 STRUCTURES CONTRACT 76D05, AND DOES NOT NECESSARILY REFLECT CURRENT FIELD CONDITIONS.

11. ALL MANHOLE LIDS OR INLET GRATES THAT ARE REMOVED DURING THIS CONTRACT SHALL REMAIN PROPERTY OF THE ILLINOIS DEPT. OF TRANSPORTATION AND SHALL BE REMOVED WITHOUT DAMAGE AND TRANSPORTED TO THE FOLLOWING ADDRESS: BOWMAN AVE. PUMP STATION, 728 EXCHANGE AVE., EAST ST. LOUIS, ILLINOIS - IF THERE ARE ANY QUESTIONS PLEASE CONTACT PETE SAWYER AT 618-346-3275.

12. ANY SEWER OR STRUCTURE REMOVAL WHICH REQUIRES ABANDONMENT OF A PORTION OF EXISTING SEWER, OR RESULTS IN A HOLE IN AN EXISTING STRUCTURE, WILL REQUIRE THE REMAINING SEWER OR STRUCTURE TO BE SECURELY SEALED WITH CLASS SI CONCRETE OR BRICK AND MORTAR PER ARTICLE 605.04 OF THE STANDARD SPECIFICATIONS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE SEWER OR STRUCTURE REMOVAL ITEM.

13. RIGHT OF WAY MARKERS SHALL BE INSTALLED SO THE BACK OF THE POST IS TWELVE (12") INCHES INSIDE THE RIGHT OF WAY BOUNDARY. THE RIGHT OF WAY MARKER SHALL BE A WITNESS TO THE RIGHT OF WAY CORNER WHICH IS THE PROPERTY PIN. THE RIGHT OF WAY CORNER OR PROPERTY PIN IS A 5/8" IRON ROD WITH IDOT ALUMINUM CAP THAT SHALL NOT BE REMOVED, DAMAGED OR DISTURBED WHEN SETTING THE RIGHT OF WAY MARKERS AT THE TWELVE INCH (12") OFFSET.

14. ABBREVIATIONS: T.E. = TEMPORARY EASEMENT, P.E. = PERMANENT EASEMENT.

15. ALL DOWEL BARS AND TIE BARS UTILIZED IN PCC PAVEMENT SHALL BE EPOXY COATED.

16. THE CONTRACTOR SHALL USE CAUTION WHEN EXCAVATING NEAR ANY RAILROAD LINE. THE DETAIL SHOWN ON SHEET 229 SHALL BE THE MINIMUM REQUIREMENTS FOR EXCAVATION IN THE VICINITY OF ANY RAILROAD.

17. IF THE CONTRACTOR ENCOUNTERS PREVIOUSLY UNIDENTIFIED UTILITIES DURING EXCAVATION, THE CONTRACTOR SHALL FIRST DETERMINE IF THE UTILITY IS ABANDONED. IF THE UTILITY IS ABANDONED, THE CONTRACTOR SHALL REMOVE THE PORTIONS OF THE UTILITY THAT CONFLICT WITH CONSTRUCTION OPERATIONS AND DISPOSE OF THE REMOVED UTILITY OUTSIDE OF THE STATE RIGHT OF WAY. THIS WORK SHALL BE PAID FOR PER ARTICLE 109.04. IF THE UTILITY IS NOT ABANDONED, THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANY AND REQUEST THAT THE SERVICE BE TERMINATED OR RELOCATED TO THE SATISFACTION OF THE ENGINEER SO THAT THE PORTIONS OF THE UTILITY THAT CONFLICT WITH CONSTRUCTION OPERATIONS CAN BE REMOVED. THE DISPOSAL OF THE REMOVED UTILITY SHALL BE OUTSIDE OF THE STATE RIGHT OF WAY. THIS WORK SHALL BE PAID FOR PER ARTICLE 109.04.

18. THE DEPARTMENT STRONGLY ENCOURAGES THE PRIME CONTRACTOR AND THEIR APPROVED SUB-CONTRACTORS TO HIRE MINORITY, WOMEN AND DISADVANTAGED INDIVIDUALS FROM ITS FEDERALLY FUNDED HIGHWAY CONSTRUCTION CAREERS TRAINING PROGRAM (HCCTP) TO HELP MEET WORKFORCE AND TRAINEE GOALS. THIS PROGRAM IS TRAINING MINORITIES, WOMEN AND DISADVANTAGED INDIVIDUALS IN HIGHWAY CONSTRUCTION-RELATED SKILLS, E.G., MATH FOR THE TRADES, JOB READINESS, TECHNICAL SKILLS COURSEWORK (CARPENTRY, CONCRETER FLATWORK, BLUEPRINT READING, SITE PLANS, SITE WORK, TOOLS USE, ETC.) AND OSHA 10 HOUR CERTIFICATION, TO PREPARE THEM FOR A CAREER IN THE HIGHWAY CONSTRUCTION TRADES. GRADUATES ARE WELL-TRAINED AND READY TO BECOME PRODUCTIVE ENTRY-LEVEL CONSTRUCTION WORKERS. CONTACT THE DISTRICT 8 EEO OFFICE AT 618-346-3360 AND/OR THE HCCTP COORDINATOR AT 618/874-6528 TO LEARN MORE ABOUT THE PROGRAM AND FOR ASSISTANCE IN MEETING WORKFORCE AND TRAINEE GOALS.

HIGHWAY STANDARDS

000001	-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001	-02	AREAS OF REINFORCEMENT BARS
001006		DECIMAL OF AN INCH AND OF A FOOT
280001	-06	TEMPORARY EROSION CONTROL SYSTEMS
420001	-07	PAVEMENT JOINTS
420101	-04	24' JOINTED PCC PAVEMENT
420106	-04	36' JOINTED PCC PAVEMENT
420111	-03	PCC PAVEMENT ROUNDOUTS
420401	-08	BRIDGE APPROACH PAVEMENT CONNECTOR
421001	-02	BAR REINFORCEMENT FOR CRC PAVEMENT
482001	-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
482006	-03	HMA SHOULDER ADJACENT TO RIGID PAVEMENT
483001	-04	PCC SHOULDER
542101	-02	REINFORCED CONCRETE END SECTIONS FOR PIPE CULVERTS 15" - 36" IN DIA AT RT ANGLE WITH ROADWAY
542301	-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
542401	-01	METAL END SECTION FOR PIPE CULVERTS
601001	-04	SUB-SURFACE DRAINS
601101	-01	CONCRETE HEADWALL FOR PIPE DRAIN
602001	-02	CATCH BASIN TYPE A
602306	-03	INLET - TYPE B
602401	-03	MANHOLE TYPE A
602601	-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701	-02	MANHOLE STEPS
604001	-03	FRAME AND LIDS TYPE 1
604036	-02	GRATE TYPE 8
604051	-03	FRAME AND GRATE TYPE 11
604091	-02	FRAME AND GRATE TYPE 24
606001	-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606006	-02	OUTLETS FOR CONCRETE CURB AND GUTTER TYPE B-6.24
606301	-04	PC CONCRETE ISLANDS AND MEDIANS
630001	-10	STEEL PLATE BEAM GUARDRAIL
630201	-06	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301	-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011	-08	TRAFFIC BARRIER TERMINAL, TYPE 2
631026	-05	TRAFFIC BARRIER TERMINAL, TYPE 5
631031	-10	TRAFFIC BARRIER TERMINAL, TYPE 6
635001	-01	DELINEATORS
635006	-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011	-02	REFLECTOR MARKER AND MOUNTING DETAILS
664001	-02	CHAIN LINK FENCE
666001	-01	RIGHT OF WAY MARKERS
701001	-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
701006	-03	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011	-02	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301	-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306	-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45 MPH
701311	-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701326	-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING SPEEDS > 45 MPH
701501	-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901	-02	TRAFFIC CONTROL DEVICES
720001	-01	SIGN PANEL MOUNTING DETAILS
720006	-03	SIGN PANEL ERECTION DETAILS
720011	-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
720021	-02	SIGN PANELS EXTRUDED ALUMINUM TYPE
728001	-01	TELESCOPING STEEL SIGN SUPPORT
729001	-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
731001	-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001	-03	TYPICAL PAVEMENT MARKINGS
781001	-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782001		PRISMATIC CURB REFLECTORS
805001	-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001	-02	HANDHOLES
814006	-02	DOUBLE HANDHOLES
825016	-02	LIGHTING CONTROLLER PEDESTAL MOUNTED, 480V
830001		LIGHT POLE ALUMINUM MAST ARM
830021		LIGHT POLE STEEL TENON TOP
835001		LIGHT TOWER
836001	-01	LIGHT POLE FOUNDATION
837001	-01	LIGHT TOWER FOUNDATIONS
878001	-09	CONCRETE FOUNDATION DETAILS

EROSION AND SEDIMENT CONTROL

1. THE WORK DESCRIBED ON THESE DRAWINGS ARE AN INTEGRAL PART OF THE STORM WATER POLLUTION PREVENTION PLAN USED TO OBTAIN THE NPDES PERMIT FROM IEPA FOR THE CONSTRUCTION OF THIS PROJECT.
2. THE PURPOSE OF THE EROSION AND SEDIMENT CONTROL MEASURES INCLUDED FOR THIS PROJECT IS TO LIMIT THE SEDIMENT POLLUTION IMPACT, OF ANY STORM WATER DISCHARGES THAT ORIGINATE ON THIS SITE OR OFF-SITE FLOWS THAT FLOW OVER THE DISTURBED AREAS, ON DOWNSTREAM AREAS.
3. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SEDIMENT TRANSPORTED OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
4. TO THE MAXIMUM EXTENT POSSIBLE, ALL FLOWS ORIGINATING OFF THE CONSTRUCTION SITE WILL BE DIVERTED AROUND DISTURBED AREAS OR WILL BE CONVEYED THROUGH THE SITE IN A MANNER THAT UNTREATED ON-SITE RUNOFF DOES NOT MIX WITH THE OFF-SITE RUNOFF.
5. ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITY.
6. A MAXIMUM OF 10 ACRES MAY BE IN SOME STAGE OF GRADING/DISTURBANCE AT A SINGLE TIME. ADDITIONAL AREAS (UP TO 10 ACRES) MAY BE CLEARED BUT WILL NOT BE STRIPPED OF VEGETATION UNTIL THE GRADED/DISTURBED AREAS HAVE BEEN PROTECTED FROM EROSION THROUGH INSTALLATION OF EITHER TEMPORARY OR PERMANENT MEASURES (PERMANENT MEASURES BY OTHERS). WHENEVER POSSIBLE, THE GRADING WILL BE COMPLETED TO THE DESIGN GRADE AND THE PERMANENT VEGETATION PLAN (BY OTHERS) IMPLEMENTED PRIOR TO STARTING GRADING ACTIVITIES ON THE NEXT SITE.
7. DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE - PERMANENT MEASURES BY OTHERS). TEMPORARY STABILIZATION THROUGH USE OF GROUND COVER, MULCHING, TEMPORARY SEEDING, OR OTHER APPROVED MEASURES WILL BE INSTALLED WHENEVER SITE DEVELOPMENT WORK, GRADING OR OTHER EARTH DISTURBING ACTIVITIES CEASE TO BE CONTINUOUS FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. THE 7/14 DAY REQUIREMENT IS TAKEN TO MEAN THAT THE STABILIZATION OPERATION IS COMPLETE OR NEARING COMPLETION IN THE DEFINED TIME.
8. THE CONTRACTOR SHALL DESIGNATE ONE OF HIS EMPLOYEES AS RESPONSIBLE FOR IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN ON ALL DISTURBED AREAS. THIS PERSON IS TO BE KNOWLEDGEABLE ABOUT INSTALLATION AND MAINTENANCE OF THE REQUIRED MEASURES. THIS EMPLOYEE IS TO HAVE THE AUTHORITY TO CARRY OUT THE IMPLEMENTATION OF ANY INSTRUCTIONS CONCERNING THE EROSION AND SEDIMENT CONTROL PLAN GIVEN BY THE ENGINEER. ALL MEASURES WILL BE INSPECTED BY THIS INDIVIDUAL AND THE ENGINEER ON A REGULAR BASIS (AT LEAST ONCE EVERY 7 DAYS) AND AFTER RAINFALL EVENTS GREATER THAN 1/2 INCH.
9. INLET FILTERS ARE REQUIRED FOR THE STRUCTURES SHOWN ON THE PLANS. STRUCTURE OPENINGS VARY SUCH THAT FIELD MEASUREMENT AND/OR CONTRACTOR DESIGN WILL BE REQUIRED. COST OF DESIGN, LABOR AND MATERIALS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR "INLET FILTERS".
10. ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDED AT THE CONTRACTOR'S EXPENSE.
11. THE RESIDENT ENGINEER SHALL HAVE FINAL DETERMINATION OF THE PLACEMENT AND LOCATION OF THE PERIMETER EROSION BARRIER.
12. SEE EROSION AND SEDIMENT CONTROL PLANS FOR PLACEMENT OF ALL EROSION AND SEDIMENT CONTROL PAY ITEMS.
13. AT THE DIRECTION OF THE ENGINEER, ANY SUITABLE EXISTING EROSION CONTROL DEVICES (SUCH AS PERIMETER EROSION BARRIER) THAT IS LEFT IN PLACE FROM A PREVIOUS (OR CONCURRENT) CONTRACT MAY BE UTILIZED IN LIEU OF THE EROSION CONTROL DEVICES AS SHOWN IN THE CONTRACT PLANS. IN THIS INSTANCE, THE QUANTITY FOR THE UNUSED EROSION CONTROL DEVICES SHALL BE DELETED FROM THE CONTRACT.

ARCHAEOLOGICAL EXCAVATION PITS

1. THE CONTRACTOR SHALL BE ADVISED THAT ARCHAEOLOGICAL EXPLORATORY EXCAVATION MAY BE ONGOING WITHIN THE I-70 AND RELOCATED IL ROUTE 3 INTERCHANGE RIGHT OF WAY DURING THIS CONTRACT. SEE SHEET 48 FOR ADDITIONAL INFORMATION.
2. SOME VARIATIONS IN REMOVAL QUANTITIES AND FURNISHED EMBANKMENT QUANTITIES DUE TO ONGOING ADJACENT CONTRACT CONSTRUCTION AND/ OR ENVIRONMENTAL/ARCHAEOLOGICAL INVESTIGATION IS TO BE EXPECTED. ADJUSTMENTS FOR SUCH VARIATIONS SHALL BE MADE IN THE FIELD BY THE ENGINEER. ALL REMOVAL ITEMS THAT HAVE BEEN DISPLACED DUE TO ONGOING ADJACENT CONTRACT CONSTRUCTION AND/ OR ENVIRONMENTAL/ ARCHAEOLOGICAL INVESTIGATION, BUT REMAIN ON SITE, MUST BE REMOVED BY THE CONTRACTOR PER THE CONTRACT PLANS. QUANTITIES FOR REMOVAL ITEMS THAT NO LONGER REMAIN ON SITE SHALL BE SUBTRACTED FROM THE CONTRACT IN ACCORDANCE WITH ARTICLE 104.02.
3. ARCHAEOLOGICAL EXCAVATION PITS WHICH HAVE BEEN PREVIOUSLY EXCAVATED AND BACKFILLED BY OTHERS MAY BE PRESENT WITHIN THE CONTRACT LIMITS. ANY SUCH ARCHAEOLOGICAL EXCAVATION PIT AREAS WHICH LIE UNDER FUTURE ROADWAY EMBANKMENT WILL BE IDENTIFIED BY THE ENGINEER FOR RE-EXCAVATION AND RE-COMPACTION PER THE SPECIAL PROVISION FOR "EARTH EXCAVATION (SPECIAL)". UNUSED QUANTITY OF EARTH EXCAVATION (SPECIAL) SHALL BE SUBTRACTED FROM THE CONTRACT IN ACCORDANCE WITH 104.02.

COMMITMENTS

1. ANY WATER WELLS FOUND DURING CONSTRUCTION WILL BE SEALED ACCORDING TO IEPA TO PREVENT GROUNDWATER POLLUTION FROM CONSTRUCTION AND FROM FUTURE ROADWAY MAINTENANCE.
2. THE TREE REMOVALS WILL BE MITIGATED IN A FUTURE CONTRACT AS PART OF THE IDOT TREE REPLACEMENT PROGRAM UNDER DEPARTMENT POLICY D & E-18.
3. IF ASH TREES ARE REMOVED ON THE PROJECT, THE CONTRACTOR SHALL BECOME FAMILIAR WITH AND COMPLY WITH MEASURES SPECIFIED BY THE ILLINOIS DEPARTMENT OF AGRICULTURE (IDOA) TO PREVENT THE SPREAD OF THE EMERALD ASH BORER. THE IDOA INFORMATION FOR ASH TREE REMOVAL CAN BE FOUND ON THE IDOA WEBSITE AT: www.AGR.STATE.IL.US/EAB.
4. ACCESS TO THE MESD AND POLARVILLE FACILITIES SHALL BE MAINTAINED BY THE CONTRACTOR. IN ADDITION, THE CONTRACTOR SHALL GIVE TWO WEEKS NOTICE TO MESD PRIOR TO CONSTRUCTION ALONG LANDSDOWNE ST. AND TWO WEEKS NOTICE TO POLARVILLE PRIOR TO CONSTRUCTION ALONG PACKERS AVE. AND EXCHANGE AVE.

MIX DESIGN CHARTS

FULL DEPTH WIDENING = 2" SURFACE, 13.75" BINDER		
MIXTURE USE	SURFACE	BINDER/WIDENING
AC/PG	PG 64-22	PG 64-22
RAP % (MAX)	SEE SPEC.	SEE SPEC.
DESIGN AIR VOIDS MIX COMPOSITION	4.0% @ NDES=70	4.0% @ NDES=90 POLYMERIZED
(GRADATION MIXTURE)	IL 12.5/9.5	IL 19.0 FG
FRICTION AGG	MIXTURE "D"	
SHOULDERS = HOT-MIX-ASPHALT SHOULDERS, 8"		
MIXTURE USE	SHOULDERS > 2.25"	SHOULDERS < 2.25"
AC/PG	PG 64-22	PG 64-22
RAP % (MAX) DESIGN AIR VOIDS	SEE RAP SPEC.	SEE RAP SPEC.
MIX COMPOSITION	**2.0% @ NDES=30	**2.0% @ NDES=30
(GRADATION MIXTURE)	NMAS 3/4"	NMAS 1/2"
FRICTION AGG		

MIXTURE USE	STABILIZED SUBBASE - HMA 4"	
AC/PG	PG 64-22	
RAP % (MAX)	SEE SPEC.	
DESIGN AIR VOIDS MIX COMPOSITION	2.0% @ NDES=30	
(GRADATION MIXTURE)	BAM	
FRICTION AGG		
MIXTURE USE	HOT-MIX ASPHALT DRIVEWAYS	
	SURFACE COURSE, 2"	BINDER COURSE, 8"
AC/PG	PG 64-22	PG 64-22
RAP % (MAX)	SEE RAP SPEC.	SEE RAP SPEC.
DESIGN AIR VOIDS MIX COMPOSITION	4.0% @ NDES=70	4.0% @ NDES=70
(GRADATION MIXTURE)	IL 12.5/9.5	IL 19.0 FG
FRICTION AGG	MIXTURE "D"	

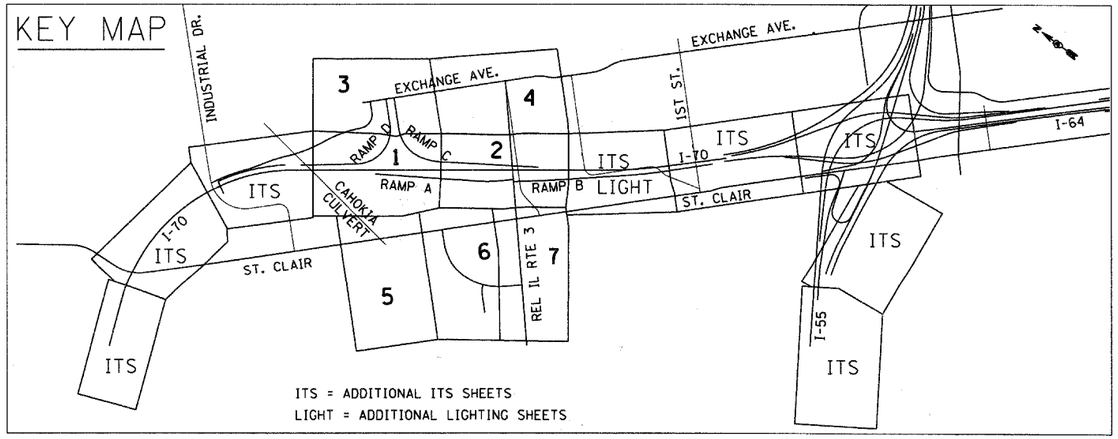
MIXTURE USE	TEMPORARY PAVEMENT	
	SURFACE COURSE, 2"	BINDER COURSE, 6"
AC/PG	PG 64-22	PG 64-22
RAP % (MAX)	SEE RAP SPEC.	SEE RAP SPEC.
DESIGN AIR VOIDS MIX COMPOSITION	4.0% @ NDES=50	4.0% @ NDES=50
(GRADATION MIXTURE)	IL 9.5	IL 19.0
FRICTION AGG	MIXTURE "D"	

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

** TOP LIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOIDS AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%.

KEY MAP



SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
				CONSTRUCTION TYPE CODE								
				0001	0008	0021	0021					
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	158	158								
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	34	34								
20100500	TREE REMOVAL, ACRES	ACRE	7.75	7.75								
20101000	TEMPORARY FENCE	FOOT	157	157								
20200100	EARTH EXCAVATION	CU YD	44,805	44,805								
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	20,449	20,449								
20400800	FURNISHED EXCAVATION	CU YD	71,855	71,855								
20800150	TRENCH BACKFILL	CU YD	852	852								
21301060	EXPLORATION TRENCH 60" DEPTH	FOOT	100	100								
21301084	EXPLORATION TRENCH 84" DEPTH	FOOT	100	100								
25100115	MULCH, METHOD 2	ACRE	17.5	17.5								
25100630	EROSION CONTROL BLANKET	SQ YD	35,680	35,680								
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	9,900	9,900								
28000305	TEMPORARY DITCH CHECKS	FOOT	338	338								
28000400	PERIMETER EROSION BARRIER	FOOT	11,135	11,135								
28000500	INLET AND PIPE PROTECTION	EACH	19	19								
28000510	INLET FILTERS	EACH	27	27								
28100105	STONE RIPRAP, CLASS A3	SQ YD	98	98								
28100109	STONE RIPRAP, CLASS A5	SQ YD	52	52								
28200200	FILTER FABRIC	SQ YD	52	52								
31200500	STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"	SQ YD	30,247	30,247								
35101400	AGGREGATE BASE COURSE, TYPE B	TON	11	11								
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	1,333	1,333								
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	2,174	2,174								
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	245	245								
40600300	AGGREGATE (PRIME COAT)	TON	5	5								
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	456	456								
40701996	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15 3/4"	SQ YD	566	566								
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	18,509	18,509								
42000521	PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)	SQ YD	7,467	7,467								

✱ DENOTES SPECIALTY ITEMS

● 0042

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
				CONSTRUCTION TYPE CODE								
				0001	0008	0021	0021					
42000541	PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)	SQ YD	651	651								
42001300	PROTECTIVE COAT	SQ YD	41,235	41,235								
42001420	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)	SQ YD	662	662								
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	523	523								
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,683	2,683								
44000100	PAVEMENT REMOVAL	SQ YD	8,598	8,598								
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	3,988	3,988								
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	147	147								
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,376	1,376								
44000600	SIDEWALK REMOVAL	SQ FT	2,683	2,683								
44004250	PAVED SHOULDER REMOVAL	SQ YD	674	674								
44201811	CLASS D PATCHES, TYPE I, 14 INCH	SQ YD	10	10								
44213200	SAW CUTS	FOOT	2,449	2,449								
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	3,540	3,540								
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	1,824	1,824								
48300500	PORTLAND CEMENT CONCRETE SHOULDERS 10"	SQ YD	5,572	5,572								
48300600	PORTLAND CEMENT CONCRETE SHOULDERS 11"	SQ YD	4,014	4,014								
48300700	PORTLAND CEMENT CONCRETE SHOULDERS 12"	SQ YD	387	387								
50105220	PIPE CULVERT REMOVAL	FOOT	22	22								
50300225	CONCRETE STRUCTURES	CU YD	39.5		39.5							
50300255	CONCRETE SUPERSTRUCTURE	CU YD	206.6		206.6							
50300260	BRIDGE DECK GROOVING	SQ YD	328		328							
50300300	PROTECTIVE COAT	SQ YD	433		433							
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	49,600		49,600							
54213443	END SECTIONS 8"	EACH	4	4								
54213447	END SECTIONS 12"	EACH	2	2								
54213450	END SECTIONS 15"	EACH	4	4								
54213453	END SECTIONS 18"	EACH	2	2								
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	5	5								
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	3	3								

✱ DENOTES SPECIALTY ITEMS

● 0042

..\DBCONN-11-BORDER.DGN,..\DBCONN-11-SGD.DGN,..\DBCONN-99-MOTIF.DGN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-MOTIF.DGN
10-19-2011,14:53:48 BONDHUJO \\FS-0244\AM\VAULT_D-TRANS.07\2202\208863-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-SUMMARY2.DGN

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
				CONSTRUCTION TYPE CODE								
				0001	0008	0021	0021					
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	6	6								
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 30"	EACH	2	2								
54213681	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36"	EACH	2	2								
5422D018	PIPE CULVERTS, CLASS D, TYPE 2 18" (TEMPORARY)	FOOT	75	75								
54247130	GRATING FOR CONCRETE FLARED END SECTION 24"	EACH	6	6								
54247150	GRATING FOR CONCRETE FLARED END SECTION 30"	EACH	2	2								
54247170	GRATING FOR CONCRETE FLARED END SECTION 36"	EACH	2	2								
542A1060	PIPE CULVERTS, CLASS A, TYPE 2 15"	FOOT	79	79								
542A1063	PIPE CULVERTS, CLASS A, TYPE 2 18"	FOOT	74	74								
542A1069	PIPE CULVERTS, CLASS A, TYPE 2 24"	FOOT	287	287								
542A1921	PIPE CULVERTS, CLASS A, TYPE 3 36"	FOOT	188	188								
542D0213	PIPE CULVERTS, CLASS D, TYPE 1 8"	FOOT	44	44								
542D0217	PIPE CULVERTS, CLASS D, TYPE 1 12"	FOOT	28	28								
542D0220	PIPE CULVERTS, CLASS D, TYPE 1 15"	FOOT	88	88								
542D0223	PIPE CULVERTS, CLASS D, TYPE 1 18"	FOOT	64	64								
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	32	32								
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	1,028	1,028								
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	53	53								
550A0430	STORM SEWERS, CLASS A, TYPE 2 30"	FOOT	85	85								
550A0660	STORM SEWERS, CLASS A, TYPE 3 15"	FOOT	74	74								
55100200	STORM SEWER REMOVAL 6"	FOOT	119	119								
55100300	STORM SEWER REMOVAL 8"	FOOT	85	85								
55100500	STORM SEWER REMOVAL 12"	FOOT	502	502								
55100700	STORM SEWER REMOVAL 15"	FOOT	147	147								
55100900	STORM SEWER REMOVAL 18"	FOOT	383	383								
* 56400500	FIRE HYDRANTS TO BE REMOVED	EACH	1	1								
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	10	10								
60107600	PIPE UNDERDRAINS 4"	FOOT	651	651								
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	113	113								
60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	8	8								

* DENOTES SPECIALTY ITEMS

● 0042

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
				CONSTRUCTION TYPE CODE								
				0001	0008	0021	0021					
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	4	4								
60204805	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	1	1								
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1								
60240310	INLETS, TYPE B, TYPE 11 FRAME AND GRATE	EACH	6	6								
60240328	INLETS, TYPE B, TYPE 24 FRAME AND GRATE	EACH	5	5								
60255500	MANHOLES TO BE ADJUSTED	EACH	12	12								
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1								
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	16	16								
60402210	GRATES, TYPE 8	EACH	4	4								
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	1	1								
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	7.4	7.4								
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	901	901								
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1,214	1,214								
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	34	34								
60610400	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	FOOT	255	255								
60618320	CONCRETE MEDIAN SURFACE, 6 INCH	SQ FT	12,130	12,130								
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SQ FT	5,211	5,211								
60620000	CONCRETE MEDIAN, TYPE SB-6.24	SQ FT	1,912	1,912								
60623200	CONCRETE MEDIAN, TYPE SM-6.24	SQ FT	119	119								
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	700	700								
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2								
* 63100070	TRAFFIC BARRIER TERMINAL, TYPE 5	EACH	2	2								
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	2	2								
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2								
63500105	DELINEATORS	EACH	83	83								
66400305	CHAIN LINK FENCE, 6'	FOOT	5,044	5,044								
66408000	CHAIN LINK GATES, 6' X 20' DOUBLE	EACH	2	2								
66600105	FURNISHING AND ERECTING RIGHT OF WAY MARKERS	EACH	36	36								
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10,000	10,000								
* 66900400	SPECIAL WASTE GROUNDWATER DISPOSAL	GALLON	5,000	5,000								

* DENOTES SPECIALTY ITEMS

● 0042

..\DBCONN-11-BORDERLI.DGN,..\DBCONN-11-SGGI.DGN,..\DBCONN-99-MOTIF1.DGN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-MOTIF1.DGN
10-19-2011,14:54:20 BONDHUJO \\FS-0044\AM\VAULT_D-TRANS_07\2202\208658-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-SUMMARY4.DGN

SUMMARY OF QUANTITIES

	CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
					CONSTRUCTION TYPE CODE								
					0001	0008	0021	0021					
*	66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1								
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	5	5								
	67100100	MOBILIZATION	L SUM	1	1								
	67201000	SEALING ABANDONED WATER WELLS	EACH	2	2								
	70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1								
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1								
	70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	30	30								
	70106800	CHANGEABLE MESSAGE SIGN	CAL MO	30	30								
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	452	452								
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	39,359	39,359								
	70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	460	460								
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1,566	1,566								
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	371	371								
	70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	20,985	20,985								
*	72000100	SIGN PANEL - TYPE 1	SQ FT	852	852								
*	72000200	SIGN PANEL - TYPE 2	SQ FT	22	22								
*	72000300	SIGN PANEL - TYPE 3	SQ FT	901	901								
	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	5	5								
*	72700100	STRUCTURAL STEEL SIGN SUPPORT - BREAKAWAY	POUND	10,500	10,500								
*	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	287	287								
*	73000100	WOOD SIGN SUPPORT	FOOT	958	958								
*	73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	24	24								
*	73301810	OVERHEAD SIGN STRUCTURE WALKWAY, TYPE A	FOOT	6.7				6.7					
*	73400100	CONCRETE FOUNDATIONS	CU YD	16.6	16.6								
*	78003100	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS	SQ FT	421	421								
*	78003110	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"	FOOT	32,043	32,043								
*	78003140	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 8"	FOOT	404	404								
*	78003150	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 12"	FOOT	1,641	1,641								
*	78003180	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 24"	FOOT	305	305								
*	78004200	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LETTERS AND SYMBOLS	SQ FT	31	31								

* DENOTES SPECIALTY ITEMS

● 0042

..\DBCONN-11-BORDER.DGN,..\DBCONN-11-SOO.DGN,..\DBCONN-99-MOTIF.DGN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-MOTIF.DGN
10-19-2011, 14:54:33 BONDHUJO \\FS-0844\AMVAULT\D-TRANS.07\2202\20866B.001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-SUMMARY5.DGN

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

SUMMARY OF QUANTITIES 5

SCALE: N.T.S. | SHEET NO. 5 OF 9 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	8
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

	CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
					CONSTRUCTION TYPE CODE 0001	0008	0021	0021					
*	78004210	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	6,732	6,732								
*	78004250	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 12"	FOOT	242	242								
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	175	175								
*	78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	4	4								
*	78200300	PRISMATIC CURB REFLECTOR	EACH	187	187								
*	78200410	GUARDRAIL MARKERS, TYPE A	EACH	16	16								
*	78200520	BARRIER WALL MARKERS, TYPE B	EACH	10	10								
*	78200530	BARRIER WALL MARKERS, TYPE C	EACH	27	27								
*	78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4								
	78300100	PAVEMENT MARKING REMOVAL	SQ FT	2,288	2,288								
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	31	31								
*	80400100	ELECTRIC SERVICE INSTALLATION	EACH	1			1						
*	80500100	SERVICE INSTALLATION, TYPE A	EACH	4				4					
*	81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	347			347						
*	81028370	UNDERGROUND CONDUIT, PVC, 3" DIA.	FOOT	243				243					
*	81028390	UNDERGROUND CONDUIT, PVC, 4" DIA.	FOOT	5,874				5,874					
*	81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	6				6					
*	81200250	CONDUIT EMBEDDED IN STRUCTURE, 3" DIA., PVC	FOOT	30				30					
*	81200270	CONDUIT EMBEDDED IN STRUCTURE, 4" DIA., PVC	FOOT	15				15					
*	81300835	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18" X 18" X 10"	EACH	4				4					
*	81300915	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 20" X 20" X 8"	EACH	3				3					
	~~81300980~~	~~JUNCTION BOX, STAINLESS STEEL, EMBEDDED IN STRUCTURE, 8" X 8" X 6"~~	~~EACH~~	~~2~~			~~2~~						
*	81400100	HANDHOLE	EACH	10				10					
*	81400300	DOUBLE HANDHOLE	EACH	7				7					
*	~~81702110~~	~~ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10~~	~~FOOT~~	~~2,760~~			~~2,760~~						
*	81702415	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 6	FOOT	1,789			1,789						
*	82102250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	23			23						
*	82500340	LIGHTING CONTROLLER, PEDESTAL MOUNTED, 480VOLT, 60AMP	EACH	1			1						
*	83009600	LIGHT POLE, ALUMINUM, 45 FT. M.H., 15 FT. MAST ARM	EACH	23			23						
*	83060800	LIGHT POLE, GALVANIZED STEEL, 30 FT. M.H., TENON MOUNT	EACH	1				1					

* DENOTES SPECIALTY ITEMS

● 0042

..\DBCONN-11-BORDER.DGN,..\DBCONN-11-SGQ1.DGN,..\DBCONN-99-MOTIF.DGN,..\DBCONN-11-LEGEND1.DGN,..\DBCONN-11-MOTIF.DGN
10-19-2011,1:45:46 BONDHLUD \\FS-0044\GM\VAULTJD-TRANS.07\2202\28868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-SUMMARY6.DGN

SUMMARY OF QUANTITIES

	CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY 0001	BRIDGE 0008	LIGHTING 0021	ITS 0021					
*	83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	5				5					
*	83600300	LIGHT POLE FOUNDATION, 30" DIAMETER	FOOT	124			117	7					
*	83700300	LIGHT TOWER FOUNDATION, 48" DIAMETER	FOOT	33				33					
*	83800205	BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	18			18						
*	X8630103	CONTROLLER CABINET TYPE III, SPECIAL	EACH	3				3					
*	87100110	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, 6F	FOOT	4,396				4,396					
*	87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	13,007				13,007					
*	87800210	CONCRETE FOUNDATION, TYPE D (SPECIAL)	FOOT	11				11					
*	87900200	DRILL EXISTING HANDHOLE	EACH	1				1					
*	X0322227	CLOSED CIRCUIT TELEVISION CAMERA SYSTEM	EACH	3				3					
	X0324045	SEDIMENT CONTROL, STABILIZED CONSTRUCTION ENTRANCE REMOVAL	EACH	5	5								
	X0324775	SEDIMENT CONTROL, STABILIZED CONSTRUCTION ENTRANCE MAINTENANCE	SQ YD	550	550								
	X0324993	SEPARATION JOINT WITH SLEEPER SLAB	FOOT	676	676								
*	X0325076	WIDE AREA NETWORK	L SUM	1				1					
*	X0325077	FIBER OPTIC UTILITY MARKER	EACH	20				20					
	X0325155	REMOVE ABANDONED GAS MAIN	FOOT	300	300								
*	X0325476	RADAR VEHICLE DETECTION SYSTEM	EACH	2				2					
*	X0325482	REMOVE EXISTING ITS EQUIPMENT	EACH	1				1					
*	X0325483	SFP-GE-L SFP MODULE	EACH	5				5					
*	X0325485	TRUSS MOUNTED LED DYNAMIC MESSAGE SIGN	EACH	1				1					
*	X0325487	WIRED COMMUNICATION DATA CONVERTOR	EACH	3				3					
*	X0325593	LIGHT TOWER, 80 FT. WITH CAMERA LOWERING SYSTEM	EACH	2				2					
*	X0326091	LIGHT POLE, STEEL 50 FT. WITH CAMERA LOWERING SYSTEM	EACH	1				1					
*	X0326802	UTILITY STRUCTURE REMOVAL	EACH	31	31								
*	X0326912	3000 LAYER 2 SWITCH	EACH	2				2					
*	X0327096	ETHERNET MODEM	EACH	4				4					
	X2010505	CLEARING, SPECIAL	L SUM	1	1								
	X2020410	EARTH EXCAVATION (SPECIAL)	CU YD	2,135	2,135								
	X4020700	AGGREGATE SURFACE COURSE, TYPE B 8"	SQ YD	160	160								
	X5012502	CONCRETE REMOVAL (SPECIAL)	CU YD	50	50								

* DENOTES SPECIALTY ITEMS

● 0042

SUMMARY OF QUANTITIES

	CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
					CONSTRUCTION TYPE CODE								
					0001	0008	0021	0021					
*	X5610704	WATER MAIN REMOVAL, 4"	FOOT	428	428								
*	X5610706	WATER MAIN REMOVAL, 6"	FOOT	300	300								
*	X5610708	WATER MAIN REMOVAL, 8"	FOOT	432	432								
*	X5610710	WATER MAIN REMOVAL, 10"	FOOT	335	335								
	X6024240	INLETS, SPECIAL	EACH	1	1								
	X6640300	CHAIN LINK FENCE REMOVAL	FOOT	3,123	3,123								
	X6640308	CHAIN LINK GATES (SPECIAL)	EACH	1	1								
	X6640310	CHAIN LINK GATES REMOVAL	EACH	1	1								
	X6650202	WOVEN WIRE FENCE REMOVAL	FOOT	654	654								
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1								
*	X7030030	WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH	FOOT	14,327	14,327								
*	X7240500	RELOCATE EXISTING SIGNS	EACH	2	2								
*	X7330064	SIGN SUPPORT SPECIAL	EACH	9	9								
*	X7830068	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS, NUMBERS AND SYMBOLS	SQ FT	421	421								
*	X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	31,279	31,279								
*	X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	404	404								
*	X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	1,300	1,300								
*	X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	305	305								
*	X8110128	CONDUIT ATTACHED TO STRUCTURE, 4" DIA., PVC TYPE C	FOOT	1,390				1,390					
*	X8710048	OPTICAL ETHERNET TRANSCEIVER	EACH	2				2					
*	X8710075	FIBER OPTIC CABLE IN CONDUIT, 72 COND. S.M. F.O.	FOOT	12,894				12,894					
	Z0001050	AGGREGATE SUBGRADE 12"	SQ YD	41,326	41,326								
	Z0013300	CONCRETE REMOVAL (SPECIAL)	SQ YD	2,984	2,984								
	Z0013796	SEDIMENT CONTROL, STABILIZED CONSTRUCTION ENTRANCE	SQ YD	550	550								
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1								
	Z0015551	DEBRIS REMOVAL (SPECIAL)	CU YD	10,000	10,000								
*	Z0016702	DETOUR SIGNING	L SUM	1	1								
	Z0019500	DRYWELL	EACH	4	4								
	Z0022800	FENCE REMOVAL	FOOT	336	336								
*	Z0036200	PAINT CURB	FOOT	966	966								

* DENOTES SPECIALTY ITEMS

● 0042

..\DBCONN-11-BORDERLDGN,..\DBCONN-11-SDGLDGN,..\DBCONN-99-MOTIF1.DGN,..\DBCONN-11-LEGENDLDGN,..\DBCONN-11-MOTIF1.DGN
10-15-2011,14:55:14 BONDHUJO \\FS-0044\AM\VAULT\D-TRANS.07\2282\208868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-SUMMARY.DGN

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES 8	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	11
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE				CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: N.T.S. / SHEET NO. 8 OF 9 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES

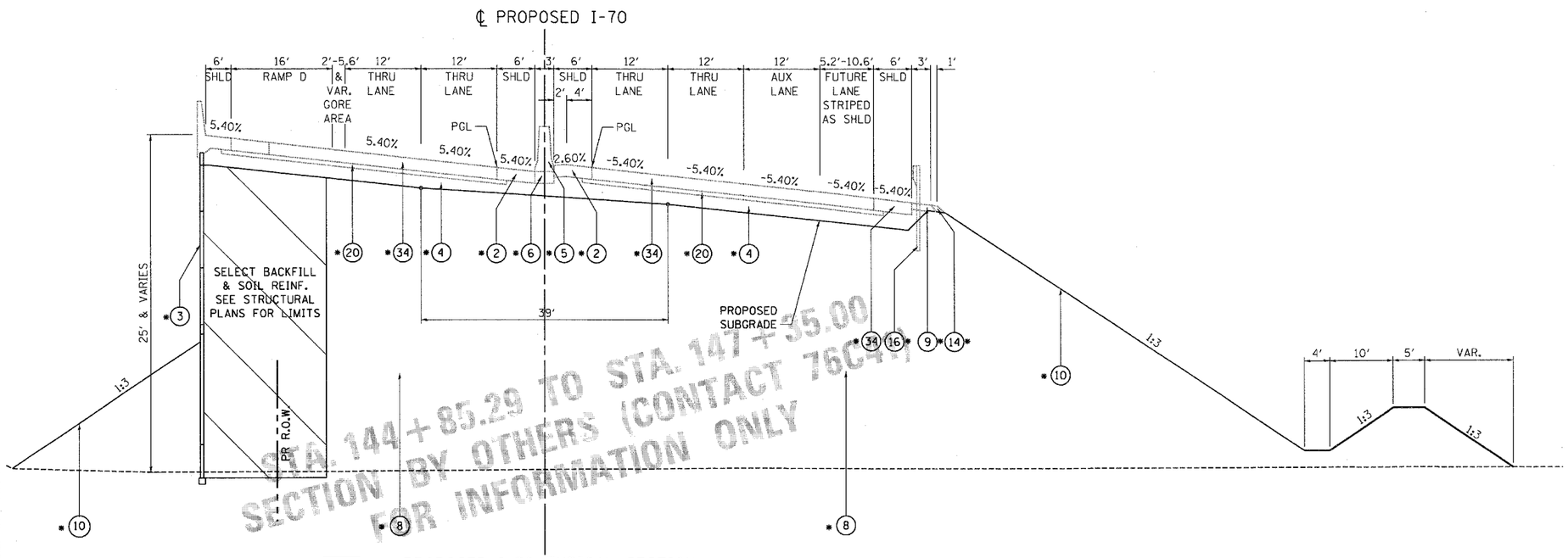
	CODE NO.	ITEM	UNIT	100% STATE URBAN TOTAL QUANTITY	ST. CLAIR COUNTY ROADWAY	BRIDGE	LIGHTING	ITS					
					CONSTRUCTION TYPE CODE 0001	0008	0021	0021					
	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1								
	Z0062456	TEMPORARY PAVEMENT	SQ YD	253	253								
●	~~Z0076600~~	~~TRAINEES~~	~~HOUR~~	~~5,000~~	~~5,000~~								
●	Z0076602	TRAINEES, SPECIAL	HOUR	5,000	5,000								
	Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	4,000	4,000								
	40603087	HOT-MIX ASPHALT BINDER COURSE, IL19 FG, N70	TON	34	34								
*	73301805	OVERHEAD SIGN STRUCTURE BUTTERFLY TYPE III-F-A	FOOT	38.2				38.2					
		~~PULL BOX ATTACHED TO STRUCTURE, AISI TYPE 316 STAINLESS STEEL NEMA 4X, 8" X 18" X 6" (HXWXD)~~	~~EACH~~	~~6~~			~~2~~	~~4~~					
		~~UNDERGROUND ELECTRIC CABLE ASSEMBLY, 600V (XLP-TYPE USE) 3-1/C NO. 6~~	~~FOOT~~	~~7,295~~			~~7,295~~						
*	81300336	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 8"x18"x6"	EACH	6			2	4					
*	X8700505	ELECTRIC CABLE ASSEMBLY IN TRENCH, 600V (XLP TYPE USE) 3-1/C NO.6	FOOT	7,295			7,295						

* DENOTES SPECIALTY ITEMS

● 0042

..\DBCONN-11-BORDER1.DGN ..\DBCONN-11-SQ01.DGN ..\DBCONN-99-MOTIF1.DGN ..\DBCONN-11-LEGEND1.DGN ..\DBCONN-11-MOTIF1.DGN
10-19-2011, 14:55:28 BONDHUJO \\FS-0044\AM\VAULT\D-TRANS.07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-SUMMARY9.DGN

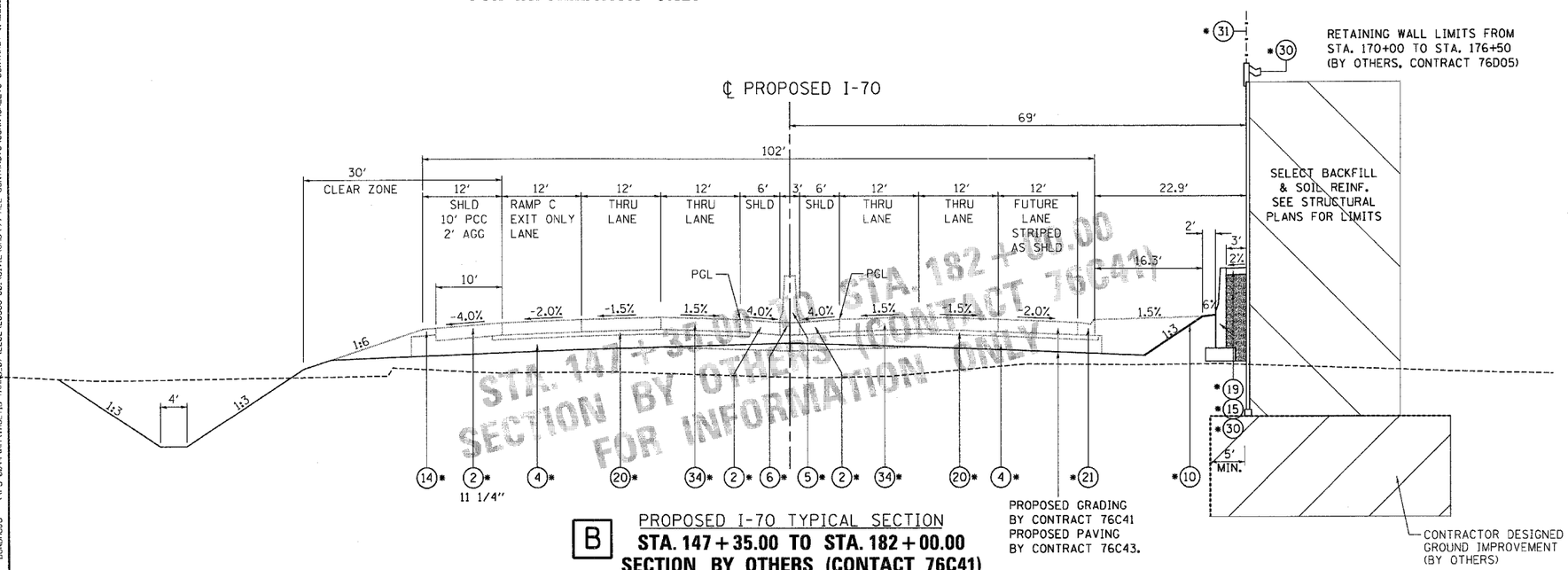
FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES 9	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353 / SHEET NO. 12
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	DRAWN - JB	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE				CONTRACT NO. 76E06	
	PLOT DATE = $DATE$	CHECKED - ACL	REVISED -		SCALE: N.T.S. / SHEET NO. 9 OF 9 SHEETS / STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	
		DATE - 10/21/11	REVISED -						



STA. 144 + 85.29 TO STA. 147 + 35.00
SECTION BY OTHERS (CONTACT 76C41)
FOR INFORMATION ONLY

A

PROPOSED I-70 TYPICAL SECTION
STA. 144 + 85.29 TO STA. 147 + 35.00
SECTION BY OTHERS (CONTACT 76C41)
FOR INFORMATION ONLY



STA. 147 + 35.00 TO STA. 182 + 00.00
SECTION BY OTHERS (CONTACT 76C41)
FOR INFORMATION ONLY

B

PROPOSED I-70 TYPICAL SECTION
STA. 147 + 35.00 TO STA. 182 + 00.00
SECTION BY OTHERS (CONTACT 76C41)
FOR INFORMATION ONLY

EXISTING LEGEND

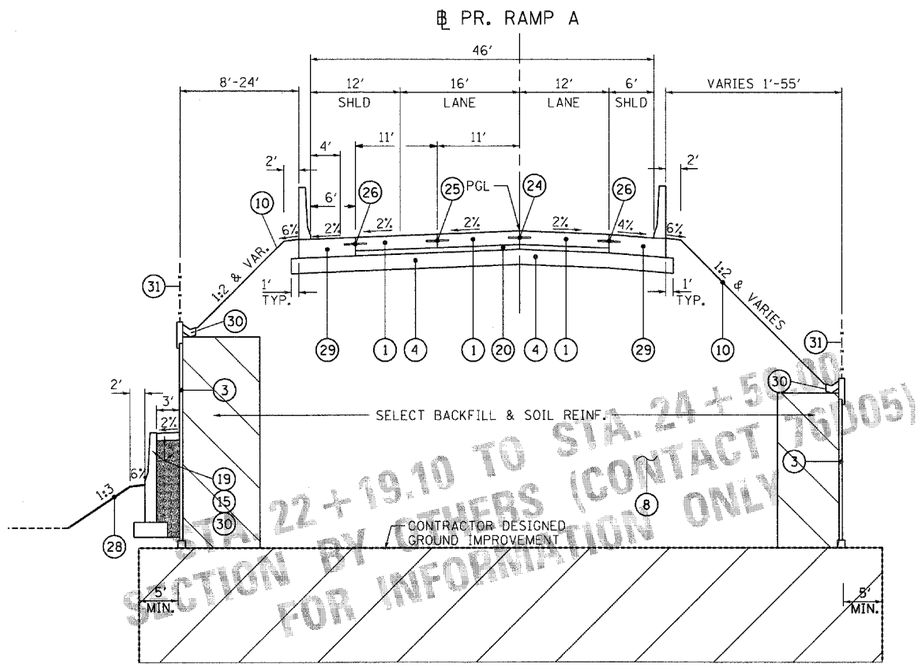
- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (2) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (3) RETAINING WALL (BY OTHERS)
- (4) AGGREGATE SUBGRADE 12"
- (5) CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
- (6) CONCRETE BARRIER BASE
- (7) AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
- (8) EMBANKMENT
- (9) HOT-MIX ASPHALT SHOULDERS, 8"
- (10) SEEDING & LANDSCAPING BY OTHERS
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (14) AGGREGATE SHOULDERS, TYPE B 6"
- (15) 42" CONCRETE BARRIER (BY OTHERS)
- (16) STEEL PLATE BEAM GUARDRAIL, TYPE A
- (17) CHAIN LINK FENCE, 6'
- (18) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (19) CONCRETE MEDIAN SURFACE, 6"
- (20) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (21) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (22) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (23) PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
- (25) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
- (26) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

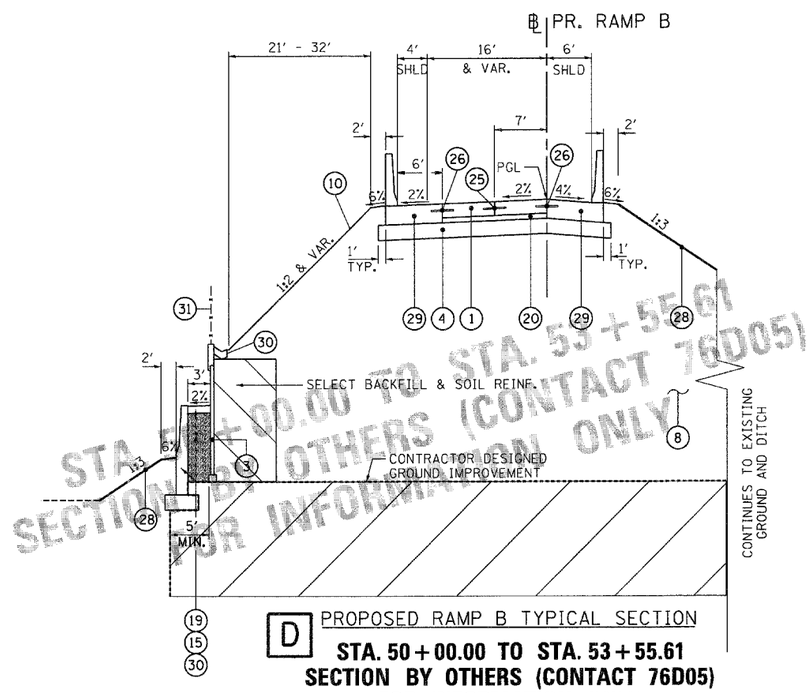
  ALL TIE BARS SHALL BE EPOXY COATED
- (27) AGGREGATE BASE COURSE, TYPE B 6"
- (28) PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
- (29) ANCHORAGE SLAB (BY OTHERS)
- (30) POROUS GRANULAR BACKFILL
- (31) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
- (32) LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
- (33) PORTLAND CEMENT CONCRETE SHOULDERS 12"
- (34) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
- (35) PIPE UNDERDRAINS 4"
- (36) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (37) CONCRETE MEDIAN, TYPE SB-6.12
- (38) CONCRETE MEDIAN, TYPE SB-6.24
- * = BY OTHERS
- [ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".



C PROPOSED RAMP A TYPICAL SECTION

STA. 22+19.10 TO STA. 24+50.00
SECTION BY OTHERS (CONTACT 76D05)
FOR INFORMATION ONLY



D PROPOSED RAMP B TYPICAL SECTION

STA. 50+00.00 TO STA. 53+55.61
SECTION BY OTHERS (CONTACT 76D05)
FOR INFORMATION ONLY

EXISTING LEGEND

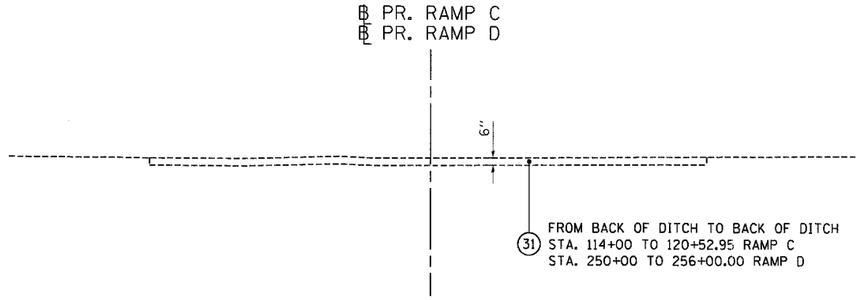
- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (2) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (3) RETAINING WALL (BY OTHERS)
- (4) AGGREGATE SUBGRADE 12"
- (5) CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
- (6) CONCRETE BARRIER BASE
- (7) AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
- (8) EMBANKMENT
- (9) HOT-MIX ASPHALT SHOULDERS, 8"
- (10) SEEDING & LANDSCAPING BY OTHERS
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (14) AGGREGATE SHOULDERS, TYPE B 6"
- (15) 42" CONCRETE BARRIER (BY OTHERS)
- (16) STEEL PLATE BEAM GUARDRAIL, TYPE A
- (17) CHAIN LINK FENCE, 6'
- (18) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (19) CONCRETE MEDIAN SURFACE, 6"
- (20) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (21) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (22) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (23) PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
- (25) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
- (26) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

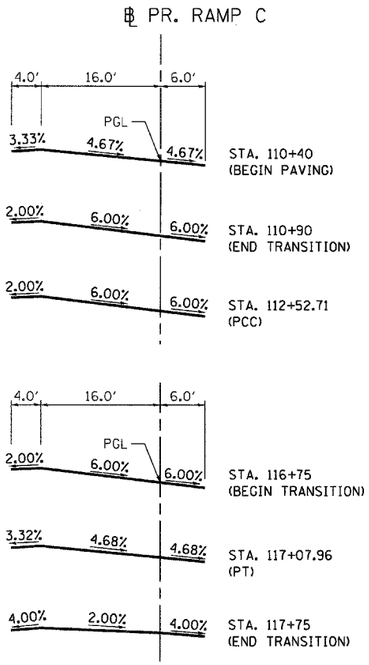
  ALL TIE BARS SHALL BE EPOXY COATED
- (27) AGGREGATE BASE COURSE, TYPE B 6"
- (28) PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
- (29) ANCHORAGE SLAB (BY OTHERS)
- (30) POROUS GRANULAR BACKFILL
- (31) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
- (32) LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
- (33) PORTLAND CEMENT CONCRETE SHOULDERS 12"
- (34) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
- (35) PIPE UNDERDRAINS 4"
- (36) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (37) CONCRETE MEDIAN, TYPE SB-6.12
- (38) CONCRETE MEDIAN, TYPE SB-6.24
- * = BY OTHERS
- [ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

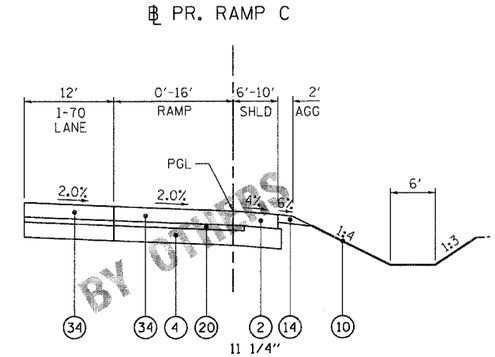
NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".



E EXISTING RAMP C & D TYPICAL SECTION

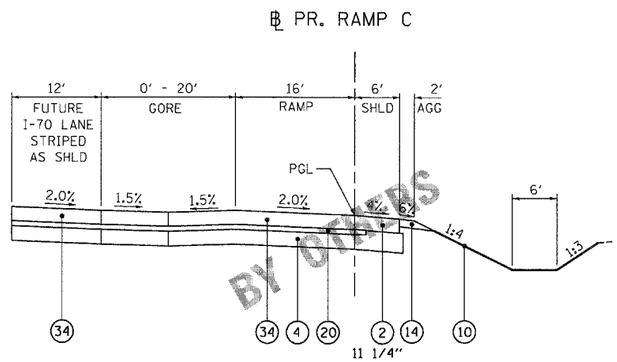
STA. 100+00.00 TO STA. 120+52.95 - RAMP C
STA. 250+00.00 TO STA. 265+35.67 - RAMP D





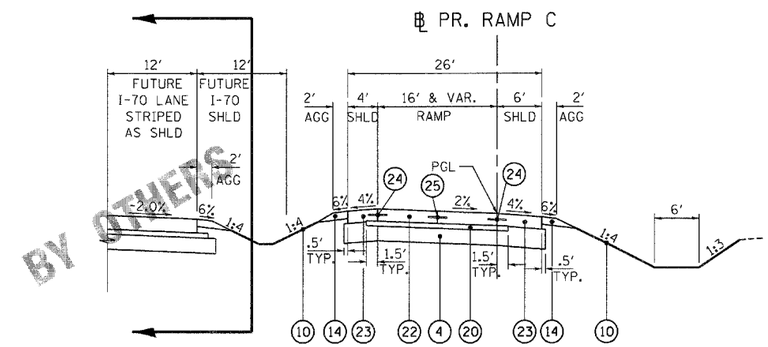
F PROPOSED RAMP C TYPICAL SECTION

STA. 102+38.79 TO STA. 105+32.24



G PROPOSED RAMP C TYPICAL SECTION

STA. 105+32.24 TO STA. 108+85.35



H PROPOSED RAMP C TYPICAL SECTION

STA. 108+85.35 TO STA. 110+90.00
S.E. TRANSITION 109+40 (+2%) TO 110+90 (+6%)
PAVING LIMTS BEGIN AT STA. 110+40.00

RAMP C

STRUCTURAL DESIGN TRAFFIC: YEAR 2030

PV = 11,092 SU = NA MU = 1,232

ROAD/STREET CLASSIFICATION: CLASS 1

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 100% S = NA M = 100%

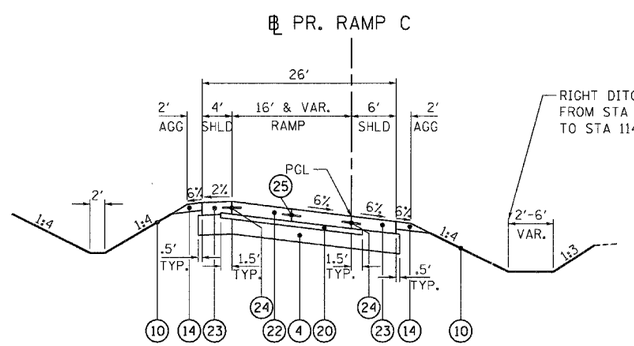
TRAFFIC FACTOR ACTUAL TF = 17.20 AC TYPE = 20

MINIMUM TF = 6.70

PG GRADE: BINDER = NA SURFACE = NA

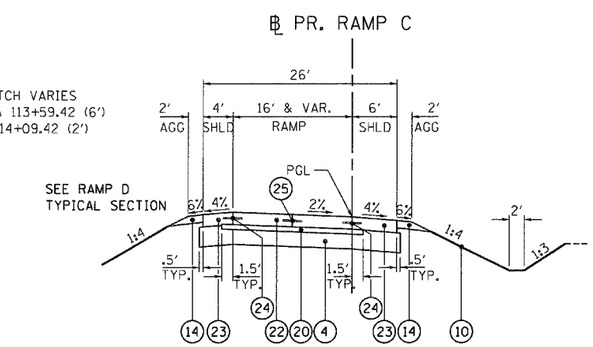
SUBGRADE SUPPORT RATING:

SSR = POOR



I PROPOSED RAMP C TYPICAL SECTION

STA. 110+90.00 TO STA. 117+75.00
S.E. TRANSITION 116+75 (+6%) TO 117+75 (+2%)



J PROPOSED RAMP C TYPICAL SECTION

STA. 117+75.00 TO STA. 120+52.95

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
2. PORTLAND CEMENT CONCRETE SHOULDERS 11"
3. RETAINING WALL (BY OTHERS)
4. AGGREGATE SUBGRADE 12"
5. CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
6. CONCRETE BARRIER BASE
7. AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
8. EMBANKMENT
9. HOT-MIX ASPHALT SHOULDERS, 8"
10. SEEDING & LANDSCAPING BY OTHERS
11. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
12. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
13. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
14. AGGREGATE SHOULDERS, TYPE B 6"
15. 42" CONCRETE BARRIER (BY OTHERS)
16. STEEL PLATE BEAM GUARDRAIL, TYPE A
17. CHAIN LINK FENCE, 6'
18. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
19. CONCRETE MEDIAN SURFACE, 6"
20. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
21. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
22. PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
23. PORTLAND CEMENT CONCRETE SHOULDERS 10"
24. LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
25. LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
26. LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

   ALL TIE BARS SHALL BE EPOXY COATED

27. AGGREGATE BASE COURSE, TYPE B 6"
28. PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
29. ANCHORAGE SLAB (BY OTHERS)
30. POROUS GRANULAR BACKFILL
31. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
32. LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
33. PORTLAND CEMENT CONCRETE SHOULDERS 12"
34. CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
35. PIPE UNDERDRAINS 4"
36. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
37. CONCRETE MEDIAN, TYPE SB-6.12
38. CONCRETE MEDIAN, TYPE SB-6.24

* = BY OTHERS

[ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".

RAMP D

STRUCTURAL DESIGN TRAFFIC: YEAR 2030

PV = 11,092 SU = NA MU = 1,232

ROAD/STREET CLASSIFICATION: CLASS 1

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 100% S = NA M = 100%

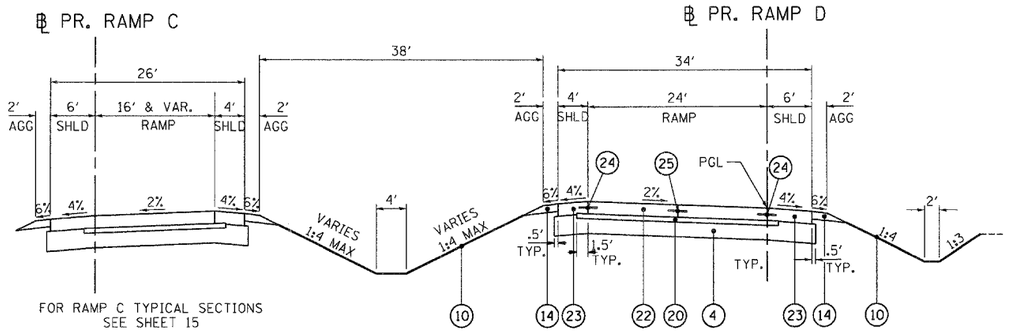
TRAFFIC FACTOR ACTUAL TF = 17.20 AC TYPE = 20

MINIMUM TF = 6.70

PG GRADE: BINDER = NA SURFACE = NA

SUBGRADE SUPPORT RATING:

SSR = POOR

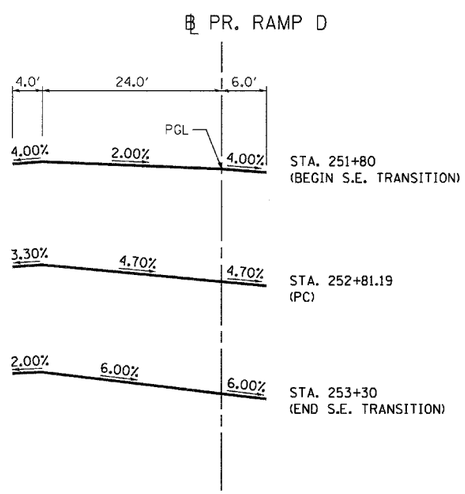


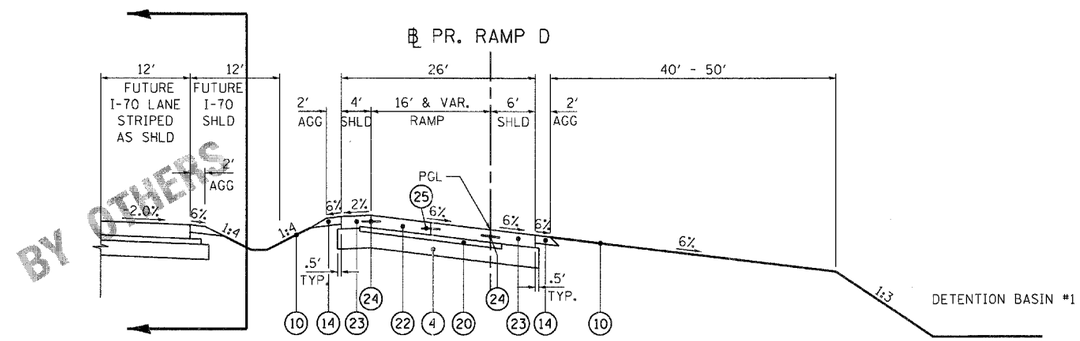
FOR RAMP C TYPICAL SECTIONS SEE SHEET 15

K

PROPOSED RAMP D TYPICAL SECTION

STA. 250+00.00 TO STA. 252+30.00

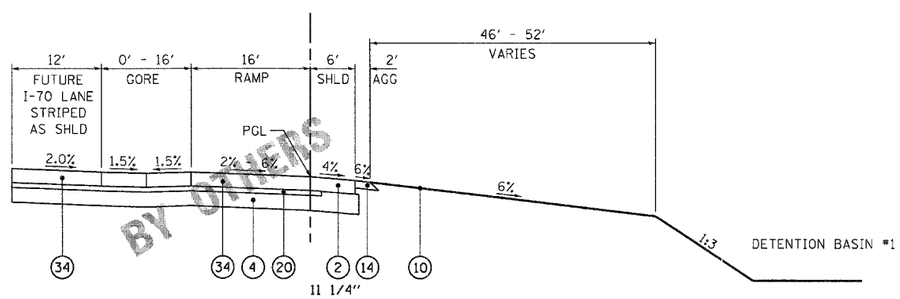




L

PROPOSED RAMP D TYPICAL SECTION

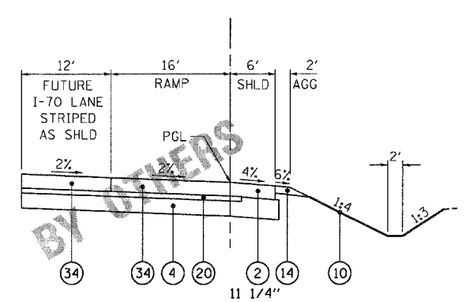
STA. 252+30.00 TO STA. 258+85.00
S.E. TRANSITION 251+80.00 (+2%) TO 253+30.00 (+6%)
PAVING LIMITS END AT STA. 258+00.00



M

PROPOSED RAMP D TYPICAL SECTION

STA. 258+59.78 TO STA. 263+35.79
S.E. TRANSITION 258+85.00 (+6%) TO 260+15.00 (+2%)



N

PROPOSED RAMP D TYPICAL SECTION

STA. 263+35.79 TO STA. 265+35.67

EXISTING LEGEND

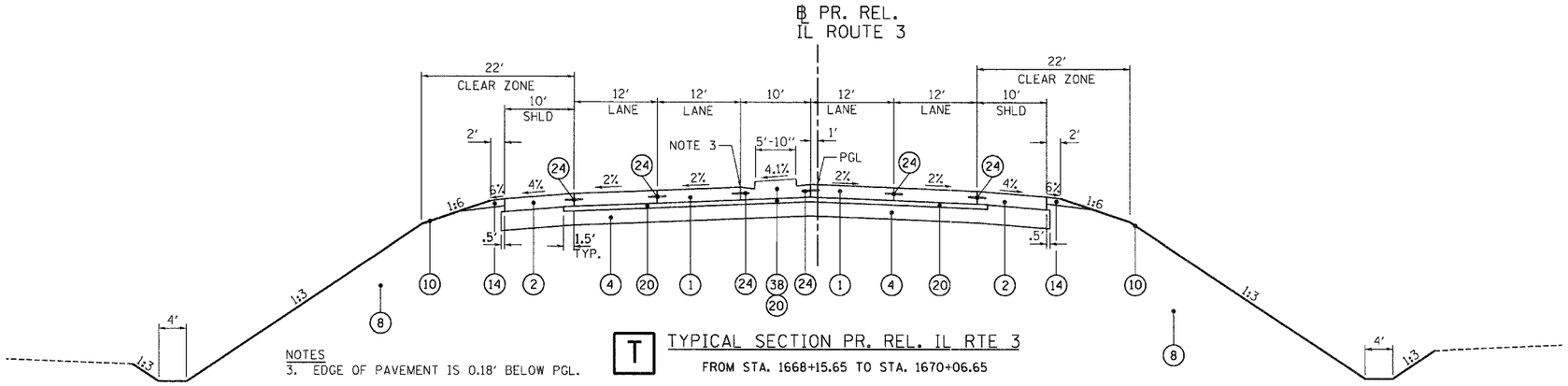
- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (2) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (3) RETAINING WALL (BY OTHERS)
- (4) AGGREGATE SUBGRADE 12"
- (5) CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
- (6) CONCRETE BARRIER BASE
- (7) AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
- (8) EMBANKMENT
- (9) HOT-MIX ASPHALT SHOULDERS, 8"
- (10) SEEDING & LANDSCAPING BY OTHERS
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (14) AGGREGATE SHOULDERS, TYPE B 6"
- (15) 42" CONCRETE BARRIER (BY OTHERS)
- (16) STEEL PLATE BEAM GUARDRAIL, TYPE A
- (17) CHAIN LINK FENCE, 6'
- (18) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (19) CONCRETE MEDIAN SURFACE, 6"
- (20) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (21) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (22) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (23) PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
- (25) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
- (26) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

  ALL TIE BARS SHALL BE EPOXY COATED
- (27) AGGREGATE BASE COURSE, TYPE B 6"
- (28) PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
- (29) ANCHORAGE SLAB (BY OTHERS)
- (30) POROUS GRANULAR BACKFILL
- (31) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
- (32) LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
- (33) PORTLAND CEMENT CONCRETE SHOULDERS 12"
- (34) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
- (35) PIPE UNDERDRAINS 4"
- (36) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (37) CONCRETE MEDIAN, TYPE SB-6.12
- (38) CONCRETE MEDIAN, TYPE SB-6.24
- * = BY OTHERS
- [ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

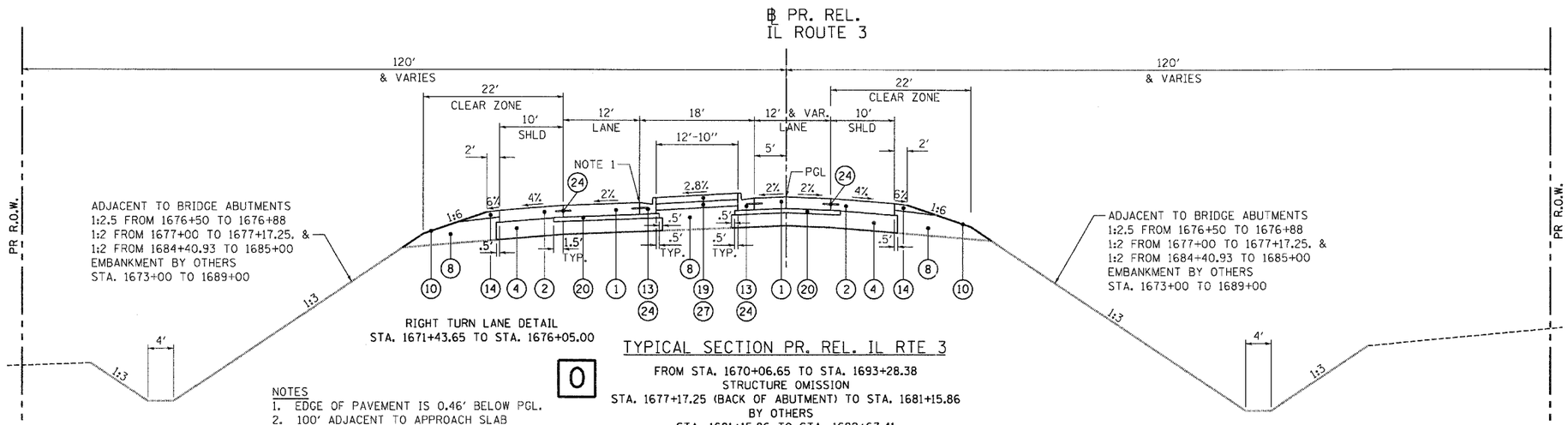
NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".



NOTES
3. EDGE OF PAVEMENT IS 0.18' BELOW PGL.

T TYPICAL SECTION PR. REL. IL RTE 3

FROM STA. 1668+15.65 TO STA. 1670+06.65

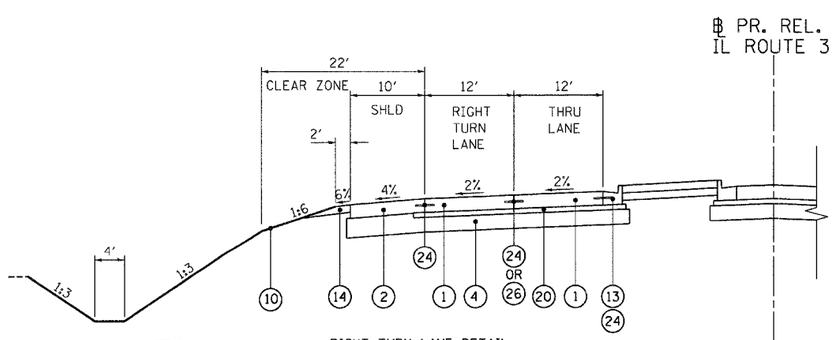


RIGHT TURN LANE DETAIL
STA. 1671+43.65 TO STA. 1676+05.00

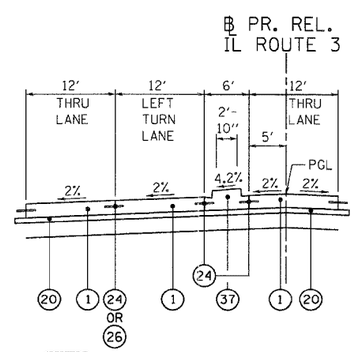
NOTES
1. EDGE OF PAVEMENT IS 0.46' BELOW PGL.
2. 100' ADJACENT TO APPROACH SLAB SHALL BE BRIDGE APPROACH PAVEMENT CONNECTOR (PCC).

O TYPICAL SECTION PR. REL. IL RTE 3

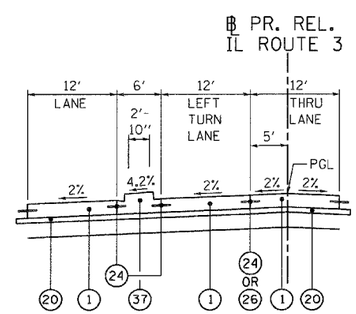
FROM STA. 1670+06.65 TO STA. 1693+28.38
STRUCTURE OMISSION
STA. 1677+17.25 (BACK OF ABUTMENT) TO STA. 1681+15.86
BY OTHERS
STA. 1681+15.86 TO STA. 1682+67.41
STRUCTURE OMISSION
STA. 1682+67.41 TO STA. 1684+40.93 (BACK OF ABUTMENT)



Q RIGHT TURN LANE DETAIL
12' RIGHT TURN LANE STA. 1671+43.65 TO STA. 1674+05.00
TAPER FROM STA. 1674+05.00 TO STA. 1676+05.00



R LEFT TURN LANE DETAIL
12' LEFT TURN LANE STA. 1683+42.51 TO STA. 1685+27.51
PORTIONS OF LEFT TURN LANE ON STR.



S LEFT TURN LANE DETAIL
12' LEFT TURN LANE STA. 1689+77.98 TO STA. 1692+39.98

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
2. PORTLAND CEMENT CONCRETE SHOULDERS 11"
3. RETAINING WALL (BY OTHERS)
4. AGGREGATE SUBGRADE 12"
5. CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
6. CONCRETE BARRIER BASE
7. AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
8. EMBANKMENT
9. HOT-MIX ASPHALT SHOULDERS, 8"
10. SEEDING & LANDSCAPING BY OTHERS
11. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
12. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
13. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
14. AGGREGATE SHOULDERS, TYPE B 6"
15. 42" CONCRETE BARRIER (BY OTHERS)
16. STEEL PLATE BEAM GUARDRAIL, TYPE A
17. CHAIN LINK FENCE, 6'
18. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
19. CONCRETE MEDIAN SURFACE, 6"
20. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
21. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
22. PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
23. PORTLAND CEMENT CONCRETE SHOULDERS 10"
24. LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
25. LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
26. LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

   ALL TIE BARS SHALL BE EPOXY COATED
27. AGGREGATE BASE COURSE, TYPE B 6"
28. PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
29. ANCHORAGE SLAB (BY OTHERS)
30. POROUS GRANULAR BACKFILL
31. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
32. LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
33. PORTLAND CEMENT CONCRETE SHOULDERS 12"
34. CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
35. PIPE UNDERDRAINS 4"
36. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
37. CONCRETE MEDIAN, TYPE SB-6.12
38. CONCRETE MEDIAN, TYPE SB-6.24

* = BY OTHERS

[ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RELOCATED IL ROUTE 3
TYPICAL SECTIONS

SCALE: N.T.S. | SHEET NO. 5 OF 9 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	17
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIFLIGN..\DBCONN-11-BORDERLIGN..\DBCONN-11-TYPSLIGN..\DBCONN-11-LEGENDLIGN..\DBCONN-11-MOTIFLIGN..\DBCONN-05-MOTIFLIGN
10-19-2011, 14:56:20 BONDHUJO \\FS-0044\AMVAULT0-TRANS.07\2282\28868 00B\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-TYPICAL5.DGN

REL. IL ROUTE 3

STRUCTURAL DESIGN TRAFFIC: YEAR 2030

PV = 23,810 SU = NA MU = 2,646

ROAD/STREET CLASSIFICATION: CLASS 1

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 50% S = NA M = 50%

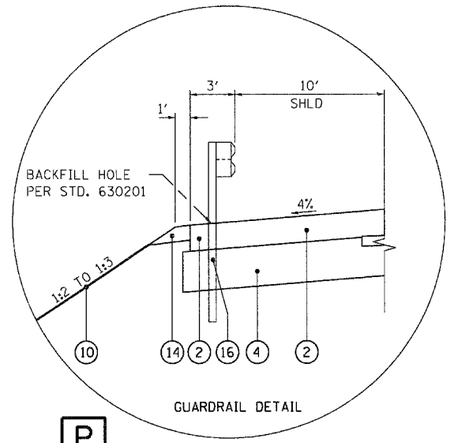
TRAFFIC FACTOR ACTUAL TF = 18.46 AC TYPE = 20

MINIMUM TF = 6.70

PG GRADE: BINDER = NA SURFACE = NA

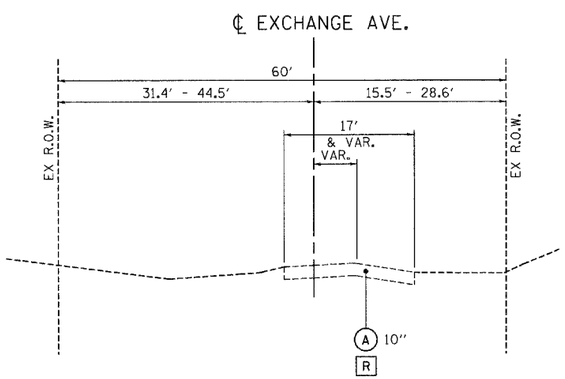
SUBGRADE SUPPORT RATING:

SSR = POOR



GUARDRAIL DETAIL

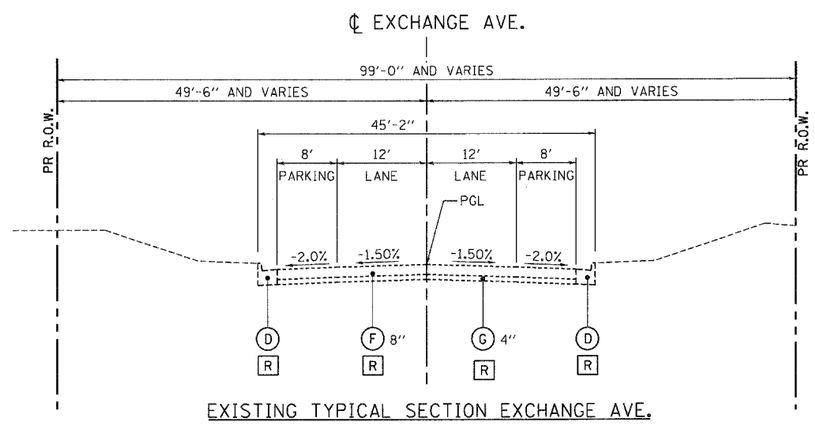
P



EXISTING TYPICAL SECTION EXCHANGE AVE.

FROM STA. 35+11.34 TO STA. 49+30.36

U



EXISTING TYPICAL SECTION EXCHANGE AVE.

FROM STA. 49+30.36 TO STA. 52+52.49
CONSTRUCTED IN CONTRACT 76C40

V

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

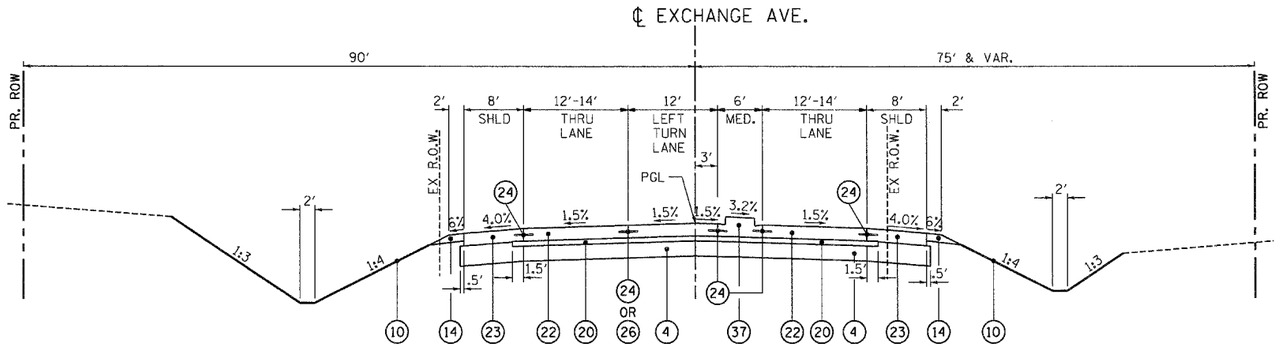
PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (2) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (3) RETAINING WALL (BY OTHERS)
- (4) AGGREGATE SUBGRADE 12"
- (5) CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
- (6) CONCRETE BARRIER BASE
- (7) AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
- (8) EMBANKMENT
- (9) HOT-MIX ASPHALT SHOULDERS, 8"
- (10) SEEDING & LANDSCAPING BY OTHERS
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (14) AGGREGATE SHOULDERS, TYPE B 6"
- (15) 42" CONCRETE BARRIER (BY OTHERS)
- (16) STEEL PLATE BEAM GUARDRAIL, TYPE A
- (17) CHAIN LINK FENCE, 6'
- (18) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (19) CONCRETE MEDIAN SURFACE, 6"
- (20) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (21) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (22) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (23) PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
- (25) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
- (26) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

ALL TIE BARS SHALL BE EPOXY COATED

- (27) AGGREGATE BASE COURSE, TYPE B 6"
- (28) PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
- (29) ANCHORAGE SLAB (BY OTHERS)
- (30) POROUS GRANULAR BACKFILL
- (31) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
- (32) LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
- (33) PORTLAND CEMENT CONCRETE SHOULDERS 12"
- (34) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
- (35) PIPE UNDERDRAINS 4"
- (36) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (37) CONCRETE MEDIAN, TYPE SB-6.12
- (38) CONCRETE MEDIAN, TYPE SB-6.24
- * = BY OTHERS
- [ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".



W

PROPOSED TYPICAL SECTION EXCHANGE AVE.

FROM STA. 35+11.34 TO STA. 44+14.45

EXCHANGE AVE.

STRUCTURAL DESIGN TRAFFIC: YEAR 2030

PV = 11,488 SU = NA MU = 1,276

ROAD/STREET CLASSIFICATION: CLASS 2

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 50% S = NA M = 50%

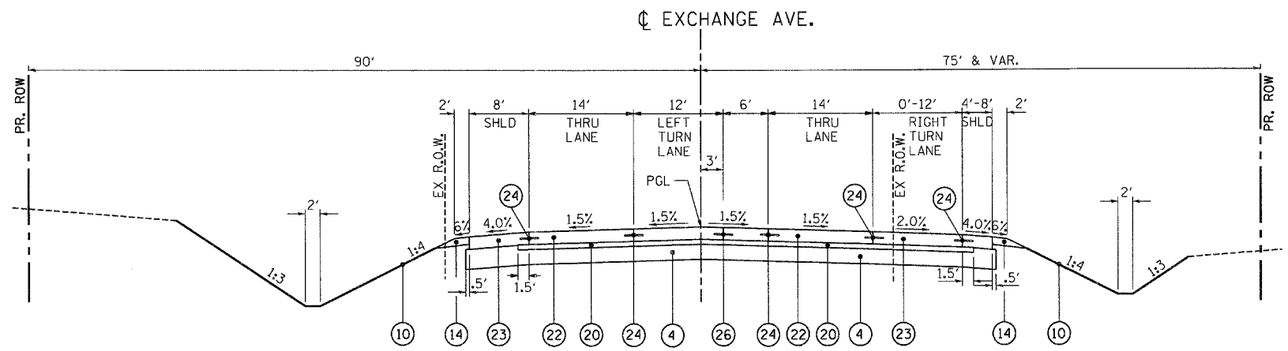
TRAFFIC FACTOR ACTUAL TF = 7.26 AC TYPE = 20

MINIMUM TF = NA

PG GRADE: BINDER = NA SURFACE = NA

SUBGRADE SUPPORT RATING:

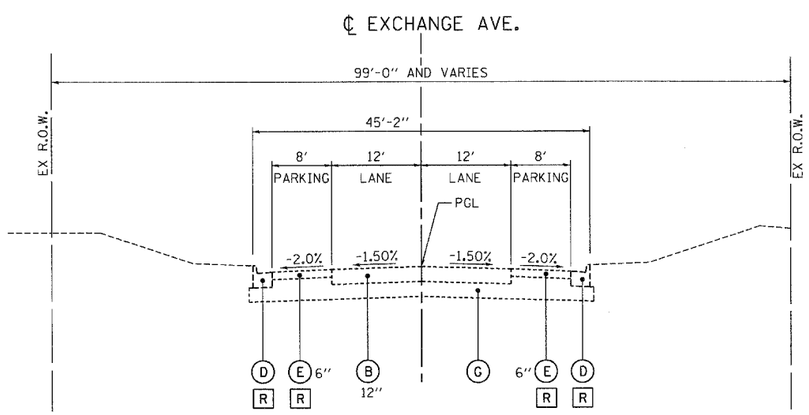
SSR = POOR



X

PROPOSED TYPICAL SECTION EXCHANGE AVE.

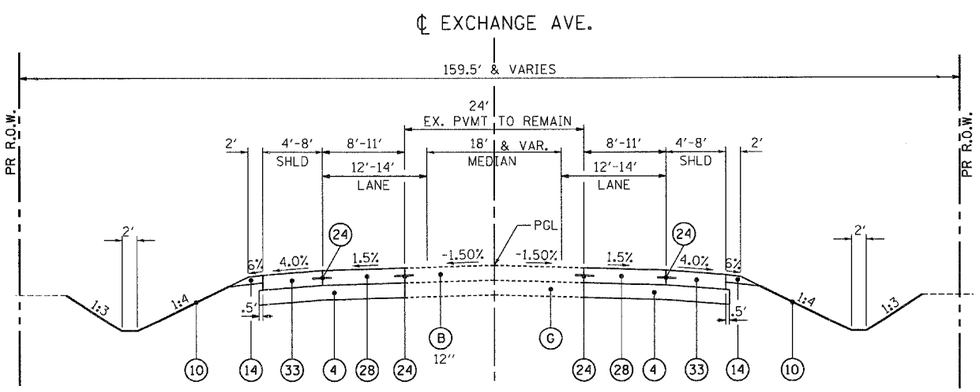
FROM STA. 44+14.45 TO STA. 52+52.49



EXISTING TYPICAL SECTION EXCHANGE AVE.

FROM STA. 52+52.49 TO STA. 55+41.84
CONSTRUCTED IN CONTRACT 76C40

Y



Z

PROPOSED TYPICAL SECTION EXCHANGE AVE.

FROM STA. 52+52.49 TO STA. 55+41.84

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
- (2) PORTLAND CEMENT CONCRETE SHOULDERS 11"
- (3) RETAINING WALL (BY OTHERS)
- (4) AGGREGATE SUBGRADE 12"
- (5) CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
- (6) CONCRETE BARRIER BASE
- (7) AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
- (8) EMBANKMENT
- (9) HOT-MIX ASPHALT SHOULDERS, 8"
- (10) SEEDING & LANDSCAPING BY OTHERS
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (14) AGGREGATE SHOULDERS, TYPE B 6"
- (15) 42" CONCRETE BARRIER (BY OTHERS)
- (16) STEEL PLATE BEAM GUARDRAIL, TYPE A
- (17) CHAIN LINK FENCE, 6'
- (18) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (19) CONCRETE MEDIAN SURFACE, 6"
- (20) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
- (21) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (22) PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
- (23) PORTLAND CEMENT CONCRETE SHOULDERS 10"
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
- (25) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
- (26) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

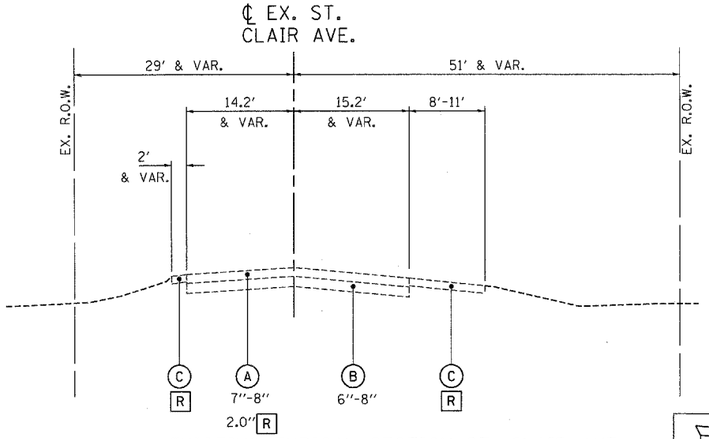
ALL TIE BARS SHALL BE EPOXY COATED

- (27) AGGREGATE BASE COURSE, TYPE B 6"
- (28) PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
- (29) ANCHORAGE SLAB (BY OTHERS)
- (30) POROUS GRANULAR BACKFILL
- (31) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
- (32) LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
- (33) PORTLAND CEMENT CONCRETE SHOULDERS 12"
- (34) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
- (35) PIPE UNDERDRAINS 4"
- (36) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (37) CONCRETE MEDIAN, TYPE SB-6.12
- (38) CONCRETE MEDIAN, TYPE SB-6.24
- * = BY OTHERS
- [ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".

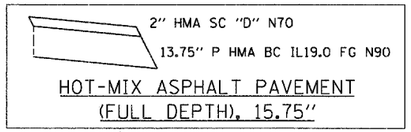
..\DBCONN-09-MOTIFLJDN,..\DBCONN-11-BORDERLJDN,..\DBCONN-11-TYPSLJDN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-MOTIFLJDN,..\DBCONN-05-MOTIFLJDN
10-19-2011, 14:56:57 BONDHUJO \\FS-02\44\JAN\PAUL.JD-TRANS.97\2282\206888-88\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-TYPICAL7.DGN

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	PROPOSED EXCHANGE AVENUE TYPICAL SECTIONS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	19
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: N.T.S. / SHEET NO. 7 OF 9 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

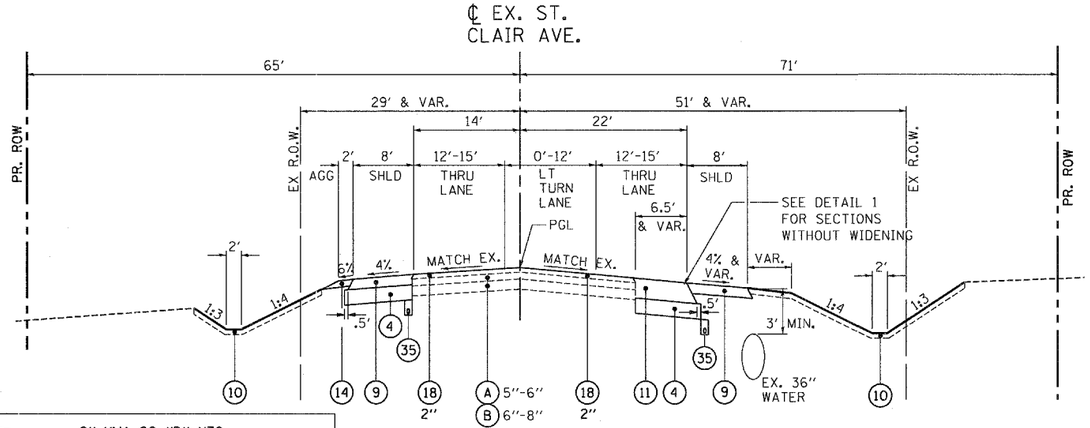


AA EXISTING TYPICAL SECTION ST. CLAIR AVE.

FROM STA. 107+15.09 TO STA. 119+42.50



HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 15.75"



BB PROPOSED TYPICAL SECTION ST. CLAIR AVE.

FROM STA. 107+15.07 TO STA. 119+42.50

LONGITUDINAL UNDERDRAINS ARE REQUIRED ONLY BET. STA. 117+61 to 119+42.50

EX. ST. CLAIR AVE.

STRUCTURAL DESIGN TRAFFIC: YEAR 2030

PV = 17,677 SU = NA MU = 1,964

ROAD/STREET CLASSIFICATION: CLASS 2

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 50% S = NA M = 50%

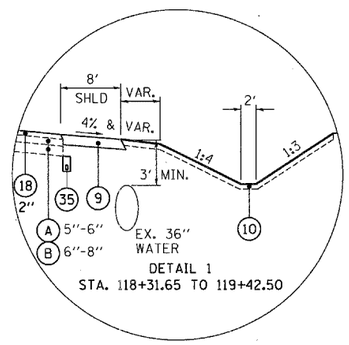
TRAFFIC FACTOR ACTUAL TF = 7.60 AC TYPE = 20

MINIMUM TF = 3.81

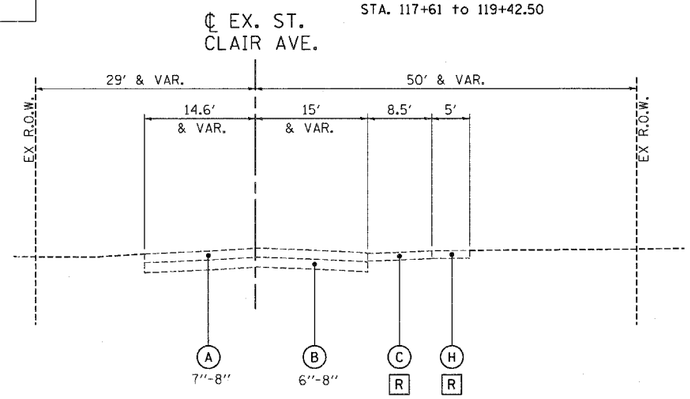
PG GRADE: BINDER = IL-19.0, N70 SURFACE = MIX "D", N70

SUBGRADE SUPPORT RATING:

SSR = POOR

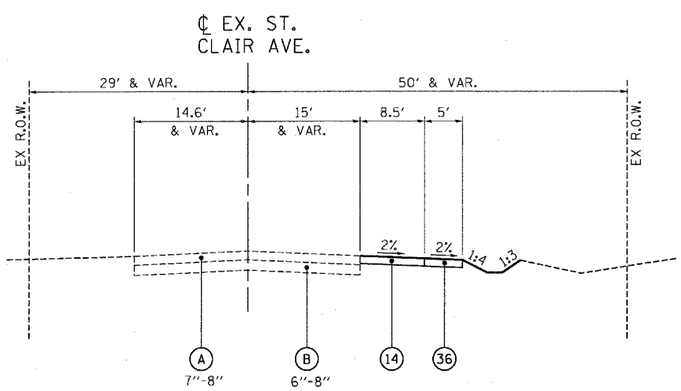


DETAIL 1
STA. 118+31.65 TO 119+42.50



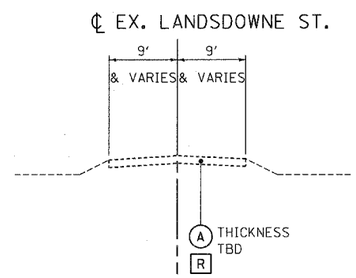
CC EXISTING TYPICAL SECTION ST. CLAIR AVE.

FROM STA. 119+42.50 TO STA. 125+36.55
EX. SIDEWALK BEGINS AT STA. 120+00.04



DD PROPOSED TYPICAL SECTION ST. CLAIR AVE.

FROM STA. 119+42.50 TO STA. 125+36.55
PR. SIDEWALK FROM STA. 120+00.04 TO STA. 125+36.55



EE EXISTING TYPICAL SECTION LANDSDOWNE ST.

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED)
2. PORTLAND CEMENT CONCRETE SHOULDERS 11"
3. RETAINING WALL (BY OTHERS)
4. AGGREGATE SUBGRADE 12"
5. CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
6. CONCRETE BARRIER BASE
7. AGGREGATE SURFACE COURSE, TYPE B, 12" (NOT USED)
8. EMBANKMENT
9. HOT-MIX ASPHALT SHOULDERS, 8"
10. SEEDING & LANDSCAPING BY OTHERS
11. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75"
12. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
13. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
14. AGGREGATE SHOULDERS, TYPE B 6"
15. 42" CONCRETE BARRIER (BY OTHERS)
16. STEEL PLATE BEAM GUARDRAIL, TYPE A
17. CHAIN LINK FENCE, 6'
18. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
19. CONCRETE MEDIAN SURFACE, 6"
20. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
21. COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
22. PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)
23. PORTLAND CEMENT CONCRETE SHOULDERS 10"
24. LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24" CTS.
25. LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18" CTS.
26. LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30" CTS.

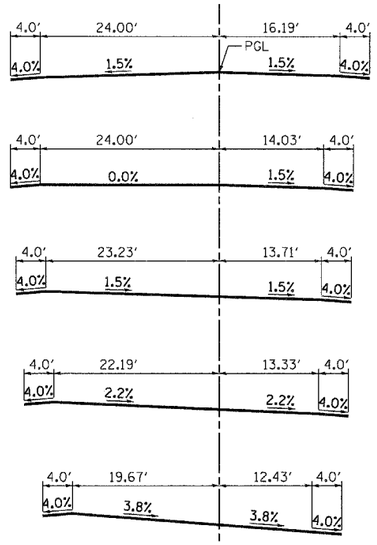
   ALL TIE BARS SHALL BE EPOXY COATED
27. AGGREGATE BASE COURSE, TYPE B 6"
28. PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED)
29. ANCHORAGE SLAB (BY OTHERS)
30. POROUS GRANULAR BACKFILL
31. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
32. LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
33. PORTLAND CEMENT CONCRETE SHOULDERS 12"
34. CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4" (BY OTHERS)
35. PIPE UNDERDRAINS 4"
36. PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
37. CONCRETE MEDIAN, TYPE SB-6.12
38. CONCRETE MEDIAN, TYPE SB-6.24

* = BY OTHERS

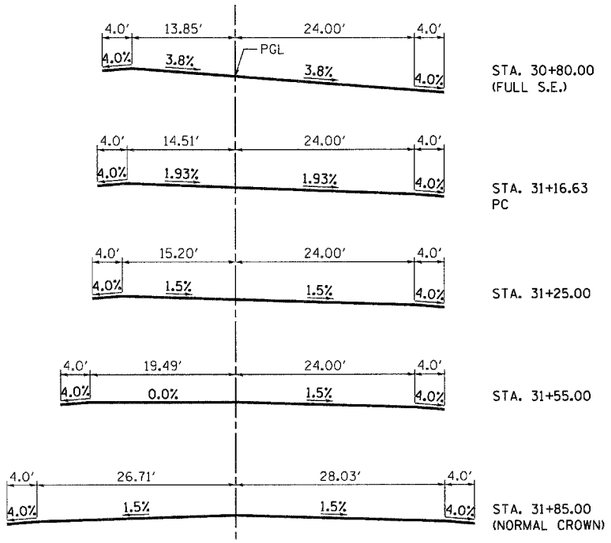
[ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12" REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12".

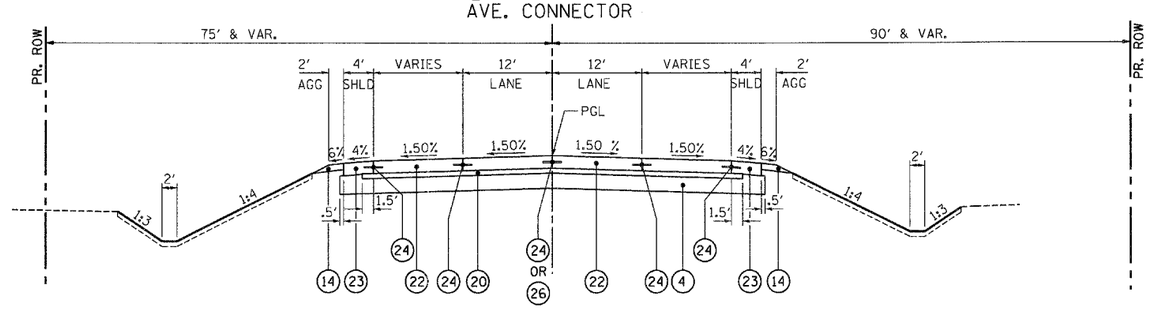
℄ PR. ST. CLAIR AVE. CONNECTOR



℄ PR. ST. CLAIR AVE. CONNECTOR

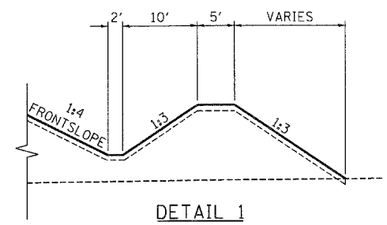


℄ PR. ST. CLAIR AVE. CONNECTOR



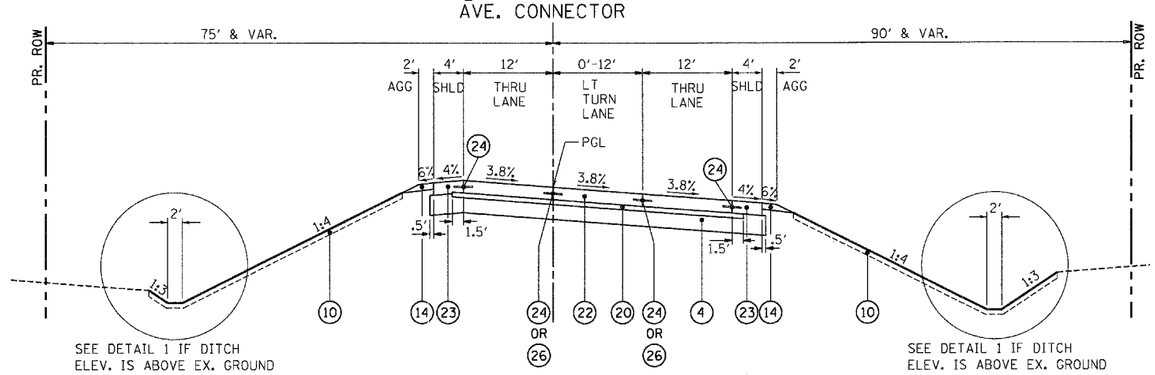
FF PROPOSED TYPICAL SECTION ST. CLAIR AVE. CONNECTOR

FROM STA. 20+00.00 TO STA. 21+40.00
& STA. 31+85.00 TO STA. 32+54.37



DETAIL 1

℄ PR. ST. CLAIR AVE. CONNECTOR



GG PROPOSED TYPICAL SECTION ST. CLAIR AVE. CONNECTOR

FROM STA. 21+40.00 TO STA. 31+85.00
S.E. TRANSITION FROM 21+40 (N.C.) TO 22+45 (3.8%)
S.E. TRANSITION FROM 30+80 (3.8%) TO 31+85 (N.C.)

ST. CLAIR AVE. CONNECTOR

STRUCTURAL DESIGN TRAFFIC: YEAR 2030

PV = 14,877 SU = NA MU = 1,653

ROAD/STREET CLASSIFICATION: CLASS 2

PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:

P = 50% S = NA M = 50%

TRAFFIC FACTOR ACTUAL TF = 9.40 AC TYPE = 20

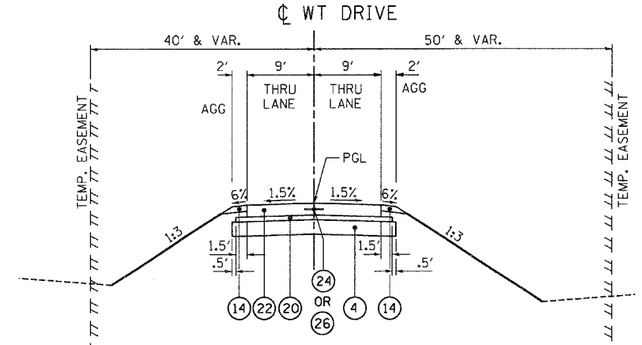
MINIMUM TF = 5.51

PG GRADE: BINDER = NA SURFACE = NA

SUBGRADE SUPPORT RATING:

SSR = POOR

℄ WT DRIVE



HH PROPOSED TYPICAL SECTION WT DRIVE

FROM STA. 900+50.00 TO STA. 903+13.13

EXISTING LEGEND

- (A) EXISTING BITUMINOUS PAVEMENT
- (B) EXISTING PCC PAVEMENT
- (C) AGGREGATE SHOULDER
- (D) CURB AND GUTTER
- (E) BITUMINOUS SHOULDER
- (F) TEMPORARY PAVEMENT
- (G) AGGREGATE SUBGRADE
- (H) EXISTING SIDEWALK
- [R] = REMOVAL

PROPOSED LEGEND

- (1) PORTLAND CEMENT CONCRETE PAVEMENT 11″ (JOINTED)
- (2) PORTLAND CEMENT CONCRETE SHOULDERS 11″
- (3) RETAINING WALL (BY OTHERS)
- (4) AGGREGATE SUBGRADE 12″
- (5) CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
- (6) CONCRETE BARRIER BASE
- (7) AGGREGATE SURFACE COURSE, TYPE B, 12″ (NOT USED)
- (8) EMBANKMENT
- (9) HOT-MIX ASPHALT SHOULDERS, 8″
- (10) SEEDING & LANDSCAPING BY OTHERS
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15.75″
- (12) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (13) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (14) AGGREGATE SHOULDERS, TYPE B 6″
- (15) 42″ CONCRETE BARRIER (BY OTHERS)
- (16) STEEL PLATE BEAM GUARDRAIL, TYPE A
- (17) CHAIN LINK FENCE, 6′
- (18) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70
- (19) CONCRETE MEDIAN SURFACE, 6″
- (20) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4″
- (21) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (22) PORTLAND CEMENT CONCRETE PAVEMENT 10″ (JOINTED)
- (23) PORTLAND CEMENT CONCRETE SHOULDERS 10″
- (24) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 TIE BARS AT 24″ CTS.
- (25) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 18″ CTS.
- (26) LONGITUDINAL SAWED JOINT WITH NO. 6 TIE BARS AT 30″ CTS.

  ALL TIE BARS SHALL BE EPOXY COATED
- (27) AGGREGATE BASE COURSE, TYPE B 6″
- (28) PORTLAND CEMENT CONCRETE PAVEMENT 12″ (JOINTED)
- (29) ANCHORAGE SLAB (BY OTHERS)
- (30) POROUS GRANULAR BACKFILL
- (31) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL OR NON-SPECIAL WASTE DISPOSAL
- (32) LIMITS OF ARCHAEOLOGICAL EXPLORATION EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION
- (33) PORTLAND CEMENT CONCRETE SHOULDERS 12″
- (34) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 11 1/4″ (BY OTHERS)
- (35) PIPE UNDERDRAINS 4″
- (36) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH
- (37) CONCRETE MEDIAN, TYPE SB-6.12
- (38) CONCRETE MEDIAN, TYPE SB-6.24
- * = BY OTHERS
- [ ] = INDICATES PAYMENT INCLUDED IN COST OF ANOTHER ITEM

NOTE: THE COST OF THE ADDITIONAL THICKNESS OF AGGREGATE SUBGRADE 12″ REQUIRED BENEATH THE SHOULDERS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQ YD FOR AGGREGATE SUBGRADE 12″.

CROSS SECTION EARTHWORK LOCATION STA.	TO STA.	EARTH EXCAVATION	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (15% OF EARTH EXC.)	TOTAL SUITABLE EARTH EXCAVATION (85% OF EARTH EXCAVATION)	SUITABLE EARTH EXC. TO BE USED IN EMBANKMENT (25% SHRINKAGE)	EMBANKMENT REQUIRED	EARTHWORK BALANCE SEE NOTE 3	NON-SPECIAL WASTE DISPOSAL	TOPSOIL REMOVAL, PAID AS REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL
		(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
RAMP C									
110+40.00	110+50.00	0.0	0.0	0.0	0.0	32.4	-32.4	0.0	9.7
110+50.00	111+00.00	0.0	0.0	0.0	0.0	174.6	-174.6	0.0	49.4
111+00.00	111+50.00	0.0	0.0	0.0	0.0	196.9	-196.9	0.0	54.0
111+50.00	112+00.00	5.0	0.8	4.3	3.2	221.5	-218.3	0.0	63.8
112+00.00	112+50.00	28.2	4.2	24.0	18.0	247.9	-230.0	0.0	76.7
112+50.00	113+00.00	87.3	13.1	74.2	55.7	276.1	-220.4	0.0	95.0
113+00.00	113+50.00	185.1	27.8	157.3	118.0	286.9	-169.0	0.0	118.6
113+50.00	114+00.00	547.9	82.2	465.7	349.3	232.2	117.1	0.0	146.9
114+00.00	114+50.00	1,200.1	180.0	1,020.1	765.0	210.5	554.5	0.0	182.8
114+50.00	115+00.00	1,570.8	235.6	1,335.2	1,001.4	227.0	774.4	0.0	203.4
115+00.00	115+50.00	1,230.4	184.6	1,045.8	784.4	224.5	559.9	0.0	183.1
115+50.00	116+00.00	604.4	90.7	513.7	385.3	207.0	178.3	0.0	163.5
116+00.00	116+50.00	304.0	45.6	258.4	193.8	168.8	25.0	0.0	141.8
116+50.00	117+00.00	239.2	35.9	203.3	152.5	138.6	13.9	0.0	109.4
117+00.00	117+50.00	197.7	29.7	168.0	126.0	90.0	36.0	0.0	97.6
117+50.00	118+00.00	189.1	28.4	160.7	120.6	40.8	79.8	0.0	93.3
118+00.00	118+50.00	161.6	24.2	137.4	103.0	29.5	73.5	0.0	91.0
118+50.00	119+00.00	99.4	14.9	84.5	63.4	44.6	18.8	0.0	84.9
119+00.00	119+50.00	44.1	6.6	37.4	28.1	175.9	-147.8	0.0	76.5
119+50.00	120+00.00	38.0	5.7	32.3	24.2	207.4	-183.2	0.0	101.2
120+00.00	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	6,732.3	1,009.8	5,722.5	4,291.9	3,433.1	858.7	0.0	2,142.5
RAMP D									
250+50.00	251+00.00	32.8	4.9	27.8	20.9	158.6	-137.8	0.0	114.7
251+00.00	251+50.00	0.3	0.1	0.3	0.2	393.0	-392.7	0.0	105.4
251+50.00	252+00.00	57.9	8.7	49.2	36.9	419.0	-382.1	0.0	90.5
252+00.00	252+50.00	123.8	18.6	105.2	78.9	177.3	-98.4	0.0	89.1
252+50.00	253+00.00	92.1	13.8	78.3	58.7	57.5	1.3	0.0	100.2
253+00.00	253+50.00	47.3	7.1	40.2	30.2	82.6	-52.5	0.0	88.5
253+50.00	254+00.00	58.1	8.7	49.4	37.1	114.9	-77.8	0.0	90.8
254+00.00	254+50.00	103.9	15.6	88.3	66.2	137.2	-70.9	0.0	95.2
254+50.00	255+00.00	281.5	42.2	239.3	179.5	116.8	62.6	0.0	117.5
255+00.00	255+50.00	592.9	88.9	504.0	378.0	198.3	179.7	0.0	159.9
255+50.00	256+00.00	601.1	90.2	510.9	383.2	271.8	111.4	0.0	176.6
256+00.00	256+50.00	253.7	38.0	215.6	161.7	203.0	-41.3	0.0	148.8
256+50.00	257+00.00	54.0	8.1	45.9	34.4	219.5	-185.1	0.0	135.9
257+00.00	257+50.00	24.4	3.7	20.7	15.5	261.3	-245.8	0.0	124.9
257+50.00	258+00.00	1.2	0.2	1.0	0.8	234.4	-233.7	0.0	94.7
258+00.00	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	2,324.9	348.7	1,976.2	1,482.1	3,045.2	-1,563.1	0.0	1,732.6

CROSS SECTION EARTHWORK LOCATION STA.	TO STA.	EARTH EXCAVATION	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (15% OF EARTH EXC.)	TOTAL SUITABLE EARTH EXCAVATION (85% OF EARTH EXCAVATION)	SUITABLE EARTH EXC. TO BE USED IN EMBANKMENT (25% SHRINKAGE)	EMBANKMENT REQUIRED	EARTHWORK BALANCE SEE NOTE 3	NON-SPECIAL WASTE DISPOSAL	TOPSOIL REMOVAL, PAID AS REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL
		(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
PR. REL. IL RTE 3									
1668+15.65	1668+50.00	0.0	0.0	0.0	0.0	5,691.3 *	-5,691.3	0.0	348.7
1668+50.00	1669+00.00	95.1	14.3	80.8	60.6	3,453.5	-3,392.8	0.0	237.7
1669+00.00	1669+50.00	95.1	14.3	80.8	60.6	3,493.0	-3,432.4	0.0	234.8
1669+50.00	1670+00.00	0.0	0.0	0.0	0.0	5,239.5	-5,239.5	0.0	258.9
1670+00.00	1670+50.00	0.0	0.0	0.0	0.0	5,579.4	-5,579.4	0.0	235.2
1670+50.00	1671+00.00	0.0	0.0	0.0	0.0	5,800.3	-5,800.3	0.0	232.3
1671+00.00	1671+50.00	3.6	0.5	3.1	2.3	5,641.8	-5,639.5	0.0	230.9
1671+50.00	1672+00.00	8.6	1.3	7.3	5.5	4,410.0	-4,404.5	0.0	118.6
1672+00.00	1672+50.00	11.3	1.7	9.6	7.2	3,296.6	-3,289.4	0.0	6.3
1672+50.00	1673+00.00	16.8	2.5	14.2	10.7	1,650.6	-1,639.9	0.0	6.9
1673+00.00	1673+50.00	22.4	3.4	19.1	14.3	109.6	-95.3	0.0	7.4
1673+50.00	1674+00.00	25.0	3.8	21.3	16.0	108.5	-92.5	0.0	7.9
1674+00.00	1674+50.00	26.2	3.9	22.3	16.7	108.5	-91.8	0.0	8.4
1674+50.00	1675+00.00	34.1	5.1	29.0	21.8	105.6	-83.9	0.0	10.0
1675+00.00	1675+50.00	23.0	3.4	19.5	14.6	125.3	-110.6	0.0	11.5
1675+50.00	1676+00.00	4.5	0.7	3.9	2.9	138.0	-135.1	0.0	9.9
1676+00.00	1676+50.00	5.2	0.8	4.4	3.3	119.8	-116.5	0.0	6.9
1676+50.00	1676+88.00	4.0	0.6	3.4	2.5	84.0	-81.5	0.0	3.6
1676+88.00	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	374.9	56.2	318.7	239.0	45,155.3	-44,916.3	0.0	1,975.9
PR. REL. IL RTE 3									
1684+69.18	1685+00.00	3.9	0.6	3.3	2.5	39.0	-36.5	0.0	0.0
1685+00.00	1685+50.00	6.3	0.9	5.3	4.0	63.1	-59.1	0.0	0.0
1685+50.00	1686+00.00	5.9	0.9	5.0	3.8	74.7	-70.9	0.0	0.0
1686+00.00	1686+50.00	5.5	0.8	4.7	3.5	86.5	-83.0	0.0	0.0
1686+50.00	1687+00.00	5.2	0.8	4.4	3.3	95.6	-92.3	0.0	0.0
1687+00.00	1687+50.00	5.0	0.7	4.2	3.2	102.7	-99.5	0.0	0.0
1687+50.00	1688+00.00	5.0	0.8	4.3	3.2	102.3	-99.1	0.0	0.0
1688+00.00	1688+50.00	5.1	0.8	4.3	3.2	98.5	-95.2	0.0	0.0
1688+50.00	1689+00.00	5.2	0.8	4.4	3.3	92.9	-89.6	0.0	0.0
1689+00.00	1689+50.00	2.7	0.4	2.3	1.7	564.9	-563.2	0.0	54.6
1689+50.00	1690+00.00	188.1	28.2	159.9	119.9	924.3	-804.4	0.0	137.4
1690+00.00	1690+50.00	426.3	63.9	362.3	271.8	655.0	-383.2	0.0	163.0
1690+50.00	1691+00.00	572.7	85.9	486.8	365.1	347.7	17.4	0.0	159.2
1691+00.00	1691+50.00	567.2	85.1	482.1	361.6	147.1	214.4	0.0	153.3
1691+50.00	1692+00.00	373.8	56.1	317.7	238.3	66.5	171.8	0.0	144.2
1692+00.00	1692+50.00	356.8	53.5	303.3	227.5	16.6	210.9	0.0	151.3
1692+50.00	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	2,534.5	380.2	2,154.3	1,615.7	3,477.5	-1,861.7	0.0	963.0

* INCLUDES EMBANKMENT FROM 1667+56.23 TO 1668+15.65 TO MATCH EXISTING GROUND.

NOTES:
1. EARTH EXCAVATION, STOCKPILE (IF REQUIRED) AND SUBSEQUENT PLACEMENT (WHEN APPLICABLE) SHALL ONLY BE MEASURED AND PAID FOR ONCE, AND NOT FOR EACH TRANSPORT.

2. SUITABLE EARTH EXCAVATION SHALL REMAIN ON SITE WITHIN THE PROJECT LIMITS AND BE PLACED AT THE DIRECTION OF THE ENGINEER.

3. (-) INDICATES SHORTAGE. FURNISHED EXCAVATION OR EARTH EXCAVATION IS REQUIRED.
(+) INDICATES SURPLUS. EARTH EXCAVATION TO BE USED ELSEWHERE WITHIN THE PROJECT LIMITS.

EARTHWORK CONTINUED ON SHEET 23

CROSS SECTION EARTHWORK LOCATION STA.	TO STA.	EARTH EXCAVATION	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (15% OF EARTH EXC.)	TOTAL SUITABLE EARTH EXCAVATION (85% OF EARTH EXCAVATION)	SUITABLE EARTH EXC. TO BE USED IN EMBANKMENT (25% SHRINKAGE)	EMBANKMENT REQUIRED	EARTHWORK BALANCE SEE NOTE 3	NON-SPECIAL WASTE DISPOSAL	TOPSOIL REMOVAL, PAID AS REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL
		(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
ST CLAIR AVE CONNECTOR									
21+15.00	21+50.00	0.0	0.0	0.0	0.0	2,406.7	-2,406.7	0.0	284.8
21+50.00	22+00.00	24.0	3.6	20.4	15.3	2,592.9	-2,577.6	0.0	361.0
22+00.00	22+50.00	25.0	3.8	21.3	15.9	2,208.2	-2,192.2	0.0	321.3
22+50.00	23+00.00	1.1	0.2	0.9	0.7	2,318.3	-2,317.6	0.0	309.1
23+00.00	23+50.00	0.1	0.0	0.0	0.0	2,201.8	-2,201.7	0.0	293.5
23+50.00	24+00.00	0.0	0.0	0.0	0.0	2,069.6	-2,069.6	0.0	252.7
24+00.00	24+50.00	0.0	0.0	0.0	0.0	1,499.9	-1,499.9	0.0	230.4
24+50.00	25+00.00	31.4	4.7	26.7	20.0	1,303.8	-1,283.8	0.0	280.2
25+00.00	25+50.00	31.4	4.7	26.7	20.0	1,583.6	-1,563.6	0.0	299.7
25+50.00	26+00.00	0.0	0.0	0.0	0.0	1,421.3	-1,421.3	0.0	243.5
26+00.00	26+50.00	0.0	0.0	0.0	0.0	1,193.7	-1,193.7	0.0	208.7
26+50.00	27+00.00	0.3	0.0	0.3	0.2	1,086.5	-1,086.3	0.0	208.3
27+00.00	27+50.00	5.9	0.9	5.0	3.8	971.3	-967.6	0.0	207.5
27+50.00	28+00.00	23.8	3.6	20.2	15.2	759.0	-743.8	0.0	204.9
28+00.00	28+50.00	44.2	6.6	37.6	28.2	545.9	-517.8	0.0	196.0
28+50.00	29+00.00	53.2	8.0	45.2	33.9	445.4	-411.5	0.0	184.6
29+00.00	29+50.00	53.3	8.0	45.3	34.0	416.0	-382.0	0.0	182.2
29+50.00	30+00.00	47.1	7.1	40.0	30.0	415.6	-385.6	0.0	186.9
30+00.00	30+50.00	35.9	5.4	30.5	22.9	447.0	-424.1	0.0	191.5
30+50.00	31+00.00	30.4	4.6	25.8	19.4	465.0	-445.7	0.0	195.9
31+00.00	31+50.00	34.9	5.2	29.7	22.3	482.2	-459.9	0.0	204.3
31+50.00	32+00.00	25.0	3.7	21.2	15.9	620.6	-604.7	0.0	244.3
32+00.00	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	466.9	70.0	396.9	297.7	27,454.3	-27,156.6	0.0	5,291.6
EX. ST. CLAIR AVE									
107+16.00	107+50.00	11.9	1.8	10.1	7.6	3.0	4.6	0.0	17.2
107+50.00	108+00.00	35.4	5.3	30.1	22.6	13.2	9.4	0.0	48.7
108+00.00	108+50.00	48.9	7.3	41.5	31.1	37.4	-6.3	0.0	74.8
108+50.00	109+00.00	63.5	9.5	54.0	40.5	52.1	-11.7	0.0	83.3
109+00.00	109+50.00	86.7	13.0	73.7	55.3	49.1	6.2	0.0	85.6
109+50.00	110+00.00	107.8	16.2	91.7	68.7	31.6	37.1	0.0	77.9
110+00.00	110+50.00	120.1	18.0	102.1	76.6	13.2	63.4	0.0	70.8
110+50.00	111+00.00	113.6	17.0	96.5	72.4	8.4	64.0	0.0	66.0
111+00.00	111+50.00	118.0	17.7	100.3	75.2	4.3	71.0	0.0	48.5
111+50.00	112+00.00	104.6	15.7	88.9	66.7	12.9	53.7	0.0	48.1
112+00.00	112+50.00	84.5	12.7	71.9	53.9	28.8	25.1	0.0	64.0
112+50.00	113+00.00	87.4	13.1	74.2	55.7	25.3	30.4	0.0	62.3
113+00.00	113+50.00	128.8	19.3	109.5	82.1	22.7	59.4	0.0	68.4
113+50.00	114+00.00	158.6	23.8	134.8	101.1	48.1	53.1	0.0	82.2
114+00.00	114+50.00	109.8	16.5	93.4	70.0	106.8	-36.8	0.0	78.9
114+50.00	115+00.00	73.0	11.0	62.1	46.5	138.9	-92.3	0.0	71.2
115+00.00	115+50.00	73.8	11.1	62.7	47.1	88.3	-41.2	0.0	70.3
115+50.00	116+00.00	80.7	12.1	68.6	51.4	22.5	29.0	0.0	57.1
116+00.00	116+50.00	85.1	12.8	72.3	54.2	1.5	52.8	0.0	45.0
116+50.00	117+00.00	74.2	11.1	63.1	47.3	2.5	44.8	0.0	44.9
117+00.00	117+50.00	63.4	9.5	53.9	40.4	3.5	36.9	0.0	45.0
117+50.00	118+00.00	64.1	9.6	54.5	40.9	4.5	36.4	0.0	47.9
118+00.00	118+50.00	73.8	11.1	62.7	47.0	3.6	43.4	0.0	43.8
118+50.00	119+00.00	64.7	9.7	55.0	41.3	1.8	39.4	0.0	41.6
119+00.00	119+42.00	39.2	5.9	33.4	25.0	2.0	23.0	0.0	38.2
119+42.00	119+50.00	6.1	0.9	5.1	3.9	0.5	3.4	0.0	5.6
119+50.00	120+00.00	30.2	4.5	25.7	19.3	3.7	15.6	0.0	23.1
120+00.00	120+50.00	31.8	4.8	27.0	20.3	2.5	17.7	0.0	20.3
120+50.00	121+00.00	47.4	7.1	40.3	30.2	0.6	29.6	0.0	9.9
121+00.00	121+50.00	47.4	7.1	40.3	30.2	0.4	29.8	0.0	8.7
121+50.00	122+00.00	35.9	5.4	30.5	22.9	1.3	21.5	0.0	17.8
122+00.00	122+50.00	33.3	5.0	28.3	21.2	1.3	19.9	0.0	15.2

CROSS SECTION EARTHWORK LOCATION STA.	TO STA.	EARTH EXCAVATION	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (15% OF EARTH EXC.)	TOTAL SUITABLE EARTH EXCAVATION (85% OF EARTH EXCAVATION)	SUITABLE EARTH EXC. TO BE USED IN EMBANKMENT (25% SHRINKAGE)	EMBANKMENT REQUIRED	EARTHWORK BALANCE SEE NOTE 3	NON-SPECIAL WASTE DISPOSAL	TOPSOIL REMOVAL, PAID AS REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL
		(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
EX. ST. CLAIR (CONT.)									
122+50.00	123+00.00	26.7	4.0	22.7	17.0	0.7	16.4	0.0	11.5
123+00.00	123+50.00	40.0	6.0	34.0	25.5	0.3	25.2	0.0	5.5
123+50.00	124+00.00	40.3	6.0	34.3	25.7	0.3	25.4	0.0	4.4
124+00.00	124+50.00	24.7	3.7	21.0	15.7	0.6	15.1	0.0	8.7
124+50.00	125+00.00	22.6	3.4	19.2	14.4	0.8	13.6	0.0	8.1
125+00.00	125+36.54	13.9	2.1	11.8	8.8	0.8	8.0	0.0	5.1
125+36.54	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	2,472.0	370.8	2,101.2	1,575.9	739.7	836.2	0.0	1,625.4
W.T. DRIVE									
900+50.00	901+00.00	61.1	9.2	51.9	39.0	3.9	35.0	0.0	5.3
901+00.00	901+50.00	24.2	3.6	20.5	15.4	54.2	-38.8	0.0	20.8
901+50.00	902+00.00	0.0	0.0	0.0	0.0	295.8	-295.8	0.0	70.7
902+00.00	902+50.00	0.0	0.0	0.0	0.0	782.3	-782.3	0.0	124.0
902+50.00	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	85.3	12.8	72.5	54.4	1,136.3	-1,081.9	0.0	220.8
EXCHANGE AVENUE									
35+12.00	35+50.00	107.4	16.1	91.3	68.4	0.7	67.7	0.0	29.9
35+50.00	36+00.00	178.6	26.8	151.8	113.9	2.9	110.9	0.0	63.8
36+00.00	36+50.00	183.2	27.5	155.7	116.8	8.3	108.5	0.0	75.8
36+50.00	37+00.00	166.7	25.0	141.7	106.3	15.5	90.7	0.0	87.0
37+00.00	39+00.00	652.9	97.9	554.9	416.2	158.7	257.5	0.0	431.8
39+00.00	39+50.00	159.9	24.0	135.9	102.0	69.1	32.8	0.0	114.0
39+50.00	40+00.00	145.2	21.8	123.4	92.5	69.0	23.5	0.0	102.7
40+00.00	40+50.00	137.4	20.6	116.8	87.6	51.8	35.9	0.0	101.4
40+50.00	41+00.00	113.1	17.0	96.1	72.1	39.9	32.2	0.0	100.4
41+00.00	41+50.00	84.8	12.7	72.1	54.1	37.0	17.0	0.0	96.6
41+50.00	42+00.00	122.4	18.4	104.0	78.0	30.4	47.6	0.0	98.1
42+00.00	42+50.00	126.4	19.0	107.4	80.6	30.8	49.8	0.0	95.3
42+50.00	43+00.00	115.8	17.4	98.4	73.8	35.3	38.5	0.0	88.9
43+00.00	43+50.00	149.4	22.4	126.9	95.2	30.4	64.8	0.0	89.3
43+50.00	44+00.00	158.4	23.8	134.6	101.0	28.7	72.3	0.0	87.9
44+00.00	44+75.84	327.9	49.2	278.7	209.0	87.5	121.6	0.0	158.4
44+75.84	45+50.00	336.4	50.5	285.9	214.5	74.7	139.7	0.0	150.3
45+50.00	46+00.00	220.4	33.1	187.3	140.5	11.7	128.8	0.0	81.9
46+00.00	46+50.00	243.5	36.5	207.0	155.3	6.4	148.8	0.0	81.5
46+50.00	47+00.00	242.6	36.4	206.2	154.7	2.8	151.9	0.0	80.2
47+00.00	47+50.00	279.0	41.8	237.1	177.9	1.4	176.5	0.0	80.9
47+50.00	48+00.00	311.3	46.7	264.6	198.5	1.3	197.1	0.0	81.4
48+00.00	48+50.00	321.9	48.3	273.6	205.2	1.4	203.9	0.0	80.9
48+50.00	49+00.00	306.2	45.9	260.3	195.2	1.3	193.9	0.0	82.1
49+00.00	49+50.00	294.2	44.1	250.1	187.5	1.4	186.2	0.0	79.9
49+50.00	50+00.00	339.3	50.9	288.4	216.3	1.0	215.2	0.0	71.3
50+00.00	51+50.00	1,454.0	218.1	1,235.9	926.9	3.3	923.6	0.0	243.1
51+50.00	52+00.00	563.1	84.5	478.6	359.0	1.4	357.5	0.0	86.7
52+00.00	52+50.00	508.5	76.3	432.2	324.2	1.4	322.8	0.0	77.7
52+50.00	53+00.00	502.6	75.4	427.2	320.4	1.5	318.9	0.0	79.2
53+00.00	53+50.00	498.5	74.8	423.7	317.8	1.4	316.4	0.0	79.5
53+50.00	54+00.00	444.7	66.7	378.0	283.5	1.4	282.1	0.0	75.5
54+00.00	54+50.00	360.5	54.1	306.4	229.8	1.4	228.4	0.0	68.2
54+50.00	55+00.00	239.4	35.9	203.5	152.6	3.9	148.8	0.0	55.6
55+00.00	55+41.84	71.5	10.7	60.7	45.6	2.6	42.9	0.0	19.9
55+41.84	END	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SUBTOTAL	10,466.8	1,570.0	8,896.8	6,672.6	817.6	5,855.0	0.0	3,476.9
						TO BE ADJUSTED PENDING NON-SPECIAL WASTE		ESTIMATED QUANTITY	
RETENTION BASIN		27,253	4,088	23,165	17,374	11,051	6,323	0	5,729
TRENCH BACKFILL		0	0	0	0	-852	852	0	0
NON-SPECIAL WASTE		0	0	0	-10,000	0	-10,000	10,000	0
	TOTALS	52,710.1	7,906.5	44,803.6	23,602.7	95,457.8	-71,855.1	10,000.0	12,542.7

FURNISHED EXCAVATION TOTAL

PAID FOR AS:
EARTH EXCAVATION OR DISPOSAL OF UNSUITABLE MATERIAL

52,710.1	- 7,907	= 44,803.6	EARTH EXCAVATION TOTAL
7,906.5	+ 12,542.7	= 20,449.2	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL TOTAL

NOTES:
1. EARTH EXCAVATION, STOCKPILE (IF REQUIRED) AND SUBSEQUENT PLACEMENT (WHEN APPLICABLE) SHALL ONLY BE MEASURED AND PAID FOR ONCE, AND NOT FOR EACH TRANSPORT.

2. SUITABLE EARTH EXCAVATION SHALL REMAIN ON SITE WITHIN THE I-70 CORRIDOR AND BE PLACED AT THE DIRECTION OF THE ENGINEER.

3. (-) INDICATES SHORTAGE. FURNISHED EXCAVATION OR EARTH EXCAVATION IS REQUIRED.
(+) INDICATES SURPLUS. EARTH EXCAVATION TO BE USED ELSEWHERE WITHIN THE PROJECT LIMITS.

TREE REMOVAL, ACRES — TOTAL 7.75 ACRE

LOCATION	I.D.	STA.	OFF.	AREA SQ FT	SUBTOTALS
DARLING	A1	1017+13	35 LT	6,142	0.14
	A2	1018+82	17 LT		
	A3	1019+42	15 LT		
	A4	1019+96	19 LT		
	A5	1019+59	35 LT		
	A6	1019+43	58 LT		
	A7	1018+96	45 LT		
	A8	1017+08	64 LT		
EXCHANGE	B1	35+90	194 RT	50,214	1.15
	B2	37+15	164 RT		
	B3	37+18	138 RT		
	B4	41+36	31 RT		
	B5	41+10	4 RT		
	B6	39+30	11 LT		
	B7	37+59	13 LT		
	B8	37+08	13 RT		
	B9	37+01	70 RT		
	B10	36+04	99 RT		
EXCHANGE	C1	35+66	20 LT	8,378	0.19
	C2	36+83	31 LT		
	C3	37+13	90 LT		
	C4	35+90	90 LT		
	C5	35+56	57 LT		
EXCHANGE	D1	37+08	30 LT	24,053	0.55
	D2	39+05	35 LT		
	D3	40+55	25 LT		
	D4	42+27	26 LT		
	D5	42+21	55 LT		
	D6	40+24	65 LT		
	D7	39+66	90 LT		
	D8	37+34	90 LT		
	D9	37+07	58 LT		
EXCHANGE	E1	39+99	77 LT	134	0.00
	E2	40+01	90 LT		
	E3	39+80	90 LT		
EXCHANGE	F1	40+61	67 LT	908	0.02
	F2	41+06	79 LT		
	F3	40+91	90 LT		
	F4	40+41	90 LT		
EXCHANGE	G1	41+73	65 LT	4,830	0.11
	G2	42+25	61 LT		
	G3	42+69	41 LT		
	G4	43+00	42 LT		
	G5	43+00	90 LT		
	G6	41+58	90 LT		
EXCHANGE	H1	41+76	75 RT	1,062	0.02
	H2	42+35	54 RT		
	H3	42+17	51 RT		
	H4	41+13	74 RT		
EXCHANGE	I1	42+64	51 RT	650	0.01
	I2	43+00	59 RT		
	I3	43+00	33 RT		
	I4	42+63	41 RT		
EXCHANGE	G4	43+00	42 LT	39,159	0.90
	G5	43+00	90 LT		
	G7	43+56	35 LT		
	G8	44+29	40 LT		
	G9	46+03	9 LT		
	G10	46+91	19 LT		
	G11	46+88	54 LT		
	G12	47+53	54 LT		
	G13	47+64	78 LT		
	G14	47+87	76 LT		
	G15	47+94	25 LT		
	G16	49+23	8 LT		
	G17	49+24	29 LT		
	G18	49+86	28 LT		
	G19	49+82	56 LT		
	G20	49+61	67 LT		
	G21	48+57	90 LT		

TREE REMOVAL, ACRES (CONTINUED)

LOCATION	I.D.	STA.	OFF.	AREA SQ FT	SUBTOTALS
EXCHANGE	I2	43+00	33 RT	24,900	0.57
	I3	43+00	58 RT		
	I5	44+31	63 RT		
	I6	47+51	49 RT		
	I7	48+09	59 RT		
	I8	49+55	74 RT		
	I9	49+49	23 RT		
	I10	46+03	18 RT		
	I11	43+53	22 RT		
EXCHANGE	J1	47+64	75 RT	372	0.01
	J2	48+23	75 RT		
	J3	47+74	62 RT		
EXCHANGE	K1	51+25	48 LT	1,762	0.04
	K2	51+41	48 LT		
	K3	51+89	90 LT		
	K4	51+22	90 LT		
EXCHANGE	L1	52+71	74 LT	525	0.01
	L2	53+15	80 LT		
	L3	52+99	90 LT		
	L4	52+66	90 LT		
EX. ST. CLAIR	M1	119+04	65 LT	1,574	0.04
	M2	118+99	39 LT		
	M3	119+55	35 LT		
	M4	119+55	65 LT		
EX. ST. CLAIR	N1	117+00	50 RT	4,981	0.11
	N2	117+00	70 RT		
	N3	119+55	70 RT		
	N4	117+55	50 RT		
EX. ST. CLAIR	O1	117+00	65 LT	4,454	0.10
	O2	117+00	28 LT		
	O3	117+99	27 LT		
	O4	118+38	65 LT		
EX. ST. CLAIR	O1	117+00	65 LT	2,200	0.05
	O2	117+00	28 LT		
	O5	116+40	65 LT		
	O6	116+39	30 LT		
EX. ST. CLAIR	N1	117+00	50 RT	15,637	0.36
	N2	117+00	70 RT		
	N5	109+00	50 RT		
	N6	109+00	70 RT		
CONNECTOR	P1	25+98	87 LT	88,490	2.03
	P2	26+70	45 RT		
	P3	28+36	131 RT		
	P4	28+89	101 RT		
	P5	31+47	96 RT		
	P6	32+12	118 RT		
	P7	32+36	75 RT		
	P8	32+38	58 LT		
	P9	32+00	153 LT		
	P10	31+89	154 LT		
	P11	31+94	66 LT		
	P12	27+36	72 LT		
EX. ST. CLAIR	N5	109+00	50 RT	2,539	0.06
	N6	109+00	70 RT		
	N7	107+79	48 RT		
	N8	107+76	70 RT		
PR. REL. IL RTE 3	R1	1668+14	190 RT	48,219	1.11
	R2	1669+75	177 RT		
	R3	1669+21	90 LT		
	R4	1669+95	158 LT		
	R5	1671+34	142 LT		

TREE REMOVAL, ACRES (CONTINUED)

LOCATION	I.D.	STA.	OFF.	AREA SQ FT	SUBTOTALS
	R6	1671+63	168 LT		
	R7	1670+99	208 LT		
	R8	1669+47	198 LT		
	R9	1669+42	185 LT		
	R10	1668+68	189 LT		
	R11	1668+22	176 LT		
	R12	1668+12	144 LT		
	R13	1668+49	134 LT		
	R14	1668+60	32 LT		
	R15	1668+32	56 RT		
	R16	1668+64	54 RT		
	R17	1668+75	126 RT		
	R18	1668+11	166 RT		
PR. REL. IL RTE 3	T1	1675+90	159 LT	7,838	0.18
	T2	1677+32	159 LT		
	T3	1677+25	241 LT		
	T4	1676+06	198 LT		

TRENCH BACKFILL — TOTAL 852 CU YD

LOCATION	FROM STATION	OFFSET		TO STATION	OFFSET		SUBTOTALS
EXCHANGE	38+70.0	8.0	RT	38+70.0	4.0	RT	0.2
EXCHANGE	38+70.0	4.0	RT	39+33.4	4.0	RT	5.8
EXCHANGE	39+36.0	42.9	LT	39+33.4	4.0	RT	3.7
EXCHANGE	39+33.4	4.0	RT	39+31.1	46.8	RT	3.6
EXCHANGE	43+97.0	4.0	RT	43+97.0	8.0	RT	0.2
EXCHANGE	43+97.0	8.0	RT	43+97.0	38.6	RT	1.1
EXCHANGE	44+44.3	46.5	RT	45+08.1	46.5	RT	6.7
EXCHANGE	44+43.0	48.9	LT	45+06.6	47.9	LT	8.2
EXCHANGE	52+35.0	39.0	LT	52+35.0	40.0	RT	7.2
EX ST CLAIR	111+22.8	35.5	LT	111+70.0	39.3	LT	4.7
EX ST CLAIR	120+76.0	35.4	LT	121+04.1	35.4	LT	0.1
EX ST CLAIR	123+46.2	34.5	RT	123+68.1	34.5	LT	0.1
EX ST CLAIR	123+46.2	32.5	RT	123+68.1	32.5	LT	0.1
CONNECTOR	23+30.0	56.5	LT	23+30.0	55.1	RT	62.3
CONNECTOR	23+90.9	64.3	LT	24+58.1	57.7	LT	35.3
CONNECTOR	27+50.0	46.1	LT	27+50.0	43.1	RT	38.3
CONNECTOR	31+50.0	407.0	LT	31+50.0	45.0	RT	16.9
REL IL RTE 3	1671+38.7	76.7	RT	1671+56.0	110.2	LT	298.6
REL IL RTE 3	1668+38.5	3.6	LT	1668+38.5	8.3	LT	1.9
REL IL RTE 3	1668+48.5	3.6	LT	1668+48.5	8.3	LT	1.8
REL IL RTE 3	1668+48.5	8.3	LT	1668+38.5	8.3	LT	1.9
REL IL RTE 3	1668+38.5	8.3	LT	1668+38.5	86.0	LT	61.2
REL IL RTE 3	1674+30.0	7.0	LT	1674+30.0	21.0	LT	4.4
REL IL RTE 3	1674+30.0	21.0	LT	1671+80.0	21.0	LT	75.6
REL IL RTE 3	1671+80.0	7.0	LT	1671+80.0	21.0	LT	4.8
REL IL RTE 3	1671+80.0	21.0	LT	1671+80.0	90.8	LT	55.5
REL IL RTE 3	1686+50.0	17.4	LT	1686+50.0	6.0	LT	1.0
REL IL RTE 3	1686+50.0	6.0	LT	1689+00.0	13.3	LT	76.8
REL IL RTE 3	1689+00.0	22.0	LT	1689+00.0	13.3	LT	1.0
REL IL RTE 3	1689+00.0	13.3	LT	1690+25.0	18.0	LT	16.3
REL IL RTE 3	1690+25.0	18.0	LT	1691+50.0	18.0	LT	18.0
REL IL RTE 3	1692+29.0	20.4	LT	1692+29.0	18.0	LT	0.4
REL IL RTE 3	1692+29.0	18.0	LT	1692+14.0	18.0	LT	2.1
REL IL RTE 3	1692+14.0	18.0	LT	1691+50.0	18.0	LT	18.4
REL IL RTE 3	1691+50.0	22.0	LT	1691+50.0	18.0	LT	1.5
REL IL RTE 3	1691+50.0	18.0	LT	1691+50.0	39.2	RT	14.1
DARLING	1009+37.7	22.5	RT	1009+61.2	23.0	RT	1.9

TREE REMOVAL (OVER 15 UNITS DIAMETER) — TOTAL 34 UNIT

LOCATION	STA.	OFF.	SIZE (INCHES)	SUBTOTALS
PR. REL. IL RTE 3	1667+62	54 LT	16	16
	1669+90	85 RT	18	18

TREE REMOVAL (6 TO 15 UNITS DIAMETER) — TOTAL 158 UNIT

LOCATION	STA.	OFF.	SIZE (INCHES)	SUBTOTALS
PR. REL. IL RTE 3	1667+54	49 LT	14	14
	1667+63	63 LT	14	14
	1667+71	51 LT	12	12
	1668+27	68 LT	14	14
	1670+18	17 RT	10	10
	1670+40	141 RT	8	8
	1670+64	22 RT	12	12
	1670+95	27 LT	12	12
	1670+97	15 LT	12	12
	1671+03	55 RT	14	14
	1671+09	5 LT	12	12
	1690+89	116 LT	12	12
	1690+97	104 LT	12	12

EARTH EXCAVATION (SPECIAL) — TOTAL 2,135 CU YD

LOCATION	EXCAVATION BLOCK	STATION	OFFSET	AREA SQ FT	DEPTH FT	SUBTOTALS
CONNECTOR	EB 12	22+43.20	87.10' LT			
		22+55.98	87.53' LT	2,428	4.0	360
		22+71.56	59.67' RT			
		22+50.97	58.76' RT			
	EB 16	31+16.33	26.44' RT			
		31+27.84	22.24' RT	495	6.0	110
		31+36.36	65.31' RT			
		31+24.15	64.51' RT			
	EB 17	29+02.77	4.43' RT			
		29+07.78	6.55' LT	2,004	6.0	445
		30+78.17	28.75' RT			
		30+75.85	40.16' RT			
	EB 18	28+39.21	45.03' RT			
		28+47.30	33.64' RT	186	6.0	42
		28+60.46	39.43' RT			
		28+53.48	50.85' RT			
	EB 19	27+60.07	16.81' LT			
		27+63.82	21.30' LT	497	6.0	110
		28+03.28	7.75' RT			
		27+96.95	20.39' RT			
	EB 20	26+40.73	40.76' LT			
		26+48.27	47.23' LT	1,603	6.0	356
		27+51.32	56.32' RT			
		27+30.86	56.91' RT			
		ADD 50% CONTINGENCY				712

STONE RIPRAP, CLASS A3 — TOTAL 98 SQ YD

LOCATION	STATION	OFF.	TO OFF.	AREA SQ FT	SUBTOTALS
PR. REL. IL RTE 3	1668+38.50	91.6 LT	101.1 LT	73	8
PR. REL. IL RTE 3	1671+56.00	115.5 LT	130.8 LT	210	23
PR. REL. IL RTE 3	1671+80.00	119.0 LT	128.9 LT	73	8
PR. REL. IL RTE 3	1691+50.00	44.9 RT	53.4 RT	62	7
CONNECTOR	23+30.00	60.7 RT	70.7 RT	93	10
CONNECTOR	27+50.00	49.6 RT	59.6 RT	93	10
CONNECTOR	31+50.00	49.6 RT	65.5 RT	181	20
EXCHANGE	39+33.00	55.7 RT	65.5 RT	101	11

FILTER FABRIC — TOTAL 52 SQ YD

STONE RIPRAP, CLASS A5 — TOTAL 52 SQ YD

LOCATION	STATION	OFFSET	OFFSET	AREA SQ FT	SUBTOTALS
CONNECTOR	23+00.00	84.8 LT	105 LT	468	52
CONNECTOR	23+00.00	84.7 LT	105.0 LT	468	52

STABILIZED SUBBASE - HOT-MIX ASPHALT, 4" — TOTAL 30,247 SQ YD

LOCATION	STATION	STATION	AREA SQ FT	SUBTOTALS
RAMP C	110+68.51	116+40.00	11,042	1,227
RAMP C	110+68.51	110+39.97	541	60
RAMP C	116+40.00	120+30.22	11,249	1,250
RAMP D	250+22.64	254+00.00	13,418	1,491
RAMP D	258+00.00	254+00.00	9,439	1,049
EXCHANGE	35+11.34	43+00.00	36,276	4,031
EXCHANGE	43+00.00	52+53.99	51,350	5,706
CONNECTOR	20+47.00	22+50.00	11,105	1,234
CONNECTOR	22+50.00	32+40.65	36,594	4,066
WT DRIVE	900+50.00	902+99.68	6,001	667
PR. REL. IL RTE 3	1668+15.65	1676+87.75	47,499	5,278
PR. REL. IL RTE 3	1684+69.18	1687+40.00	10,398	1,155
PR. REL. IL RTE 3	1687+40.00	1692+96.82	27,313	3,035

AGGREGATE BASE COURSE, TYPE B — TOTAL 11 TON

LOCATION	STATION		AREA (SF)	THICKNESS (IN)	TONS/CY	SUBTOTALS
RAMP C	120+06.74	120+19.28	136	14	1.8	10.58

AGGREGATE BASE COURSE, TYPE B 6" — TOTAL 1,333 SQ YD

LOCATION	STATION	STATION	AREA (SQ FT)	SUBTOTALS
RAMP C	119+85.13	120+13.28	775	86.1
PR. REL. IL RTE 3	1671+39.76	1676+87.75	6,937	770.8
	1685+27.51	1687+40.00	1,952	216.9
	1687+40.00	1689+77.98	2,330	258.9

BITUMINOUS MATERIALS (PRIME COAT) — TOTAL 245 GALLON

DESCRIPTION	AREA SQ YD	SUBTOTALS
RESURFACING ON EX. ST. CLAIR - ITEM 40603340	4,074	204
PAVEMENT WIDENING ON EX. ST. CLAIR - ITEM 40701996	566	28
TEMPORARY PAVEMENT ON EXCHANGE - ITEM Z0062456	253	13

APPLICATION RATE 0.05 GAL/SQ YD

AGGREGATE (PRIME COAT) — TOTAL 5 TON

DESCRIPTION	AREA SQ YD	SUBTOTALS
RESURFACING ON EX. ST. CLAIR - ITEM 40603340	4,074	4.1
PAVEMENT WIDENING ON EX. ST. CLAIR - ITEM 40701996	566	0.6
TEMPORARY PAVEMENT ON EXCHANGE - ITEM Z0062456	253	0.3

APPLICATION RATE 2 LBS/SQ YD

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 — TOTAL 456 TON

LOCATION	STA	STA	LT / RT	AREA SQ FT	THICKNESS IN.	SUBTOTALS
EX. ST. CLAIR	107+15.07	109+00.00	LT & RT	5,510	2	69
EX. ST. CLAIR	109+00.00	117+00.00	LT & RT	23,357	2	291
EX. ST. CLAIR	117+00.00	119+42.50	LT & RT	7,124	2	89
EX. ST. CLAIR	118+49.80		LT	675	2	8

HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 15 3/4" — TOTAL 566 SQ YD

LOCATION	STATION	STATION	LT / RT	AREA SQ FT	SUBTOTALS
EX. ST. CLAIR	108+47.72	109+00.00	RT	45	5
EX. ST. CLAIR	109+00.00	117+00.00	RT	4,805	534
EX. ST. CLAIR	117+00.00	118+31.65	RT	243	27

PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED) — TOTAL 18,509 SQ YD

LOCATION	STATION	STATION	AREA SQ FT	SUBTOTALS
RAMP C	110+40.00	110+68.46	456	51
RAMP C	110+68.46	116+40.00	9,298	1,033
RAMP C	116+40.00	120+29.95	9,370	1,041
RAMP D	254+00.00	250+22.40	11,980	1,331
RAMP D	258+00.00	254+00.00	8,213	913
EXCHANGE	35+11.34	43+00.00	31,883	3,543
EXCHANGE	43+00.00	52+52.49	46,201	5,133
CONNECTOR	20+47.00	22+50.00	10,427	1,159
CONNECTOR	22+50.00	32+40.64	33,429	3,714
WT DRIVE	900+50.00	903+01.13	5,328	592

PORTLAND CEMENT CONCRETE PAVEMENT 11" (JOINTED) — TOTAL 7,467 SQ YD

LOCATION	STATION	STATION	OFFSET	AREA SQ FT	SUBTOTALS
PR. REL. IL RTE 3	1668+15.65	1675+87.75		37,491	4,166
PR. REL. IL RTE 3	1685+70.43	1687+40.00	LT	2,775	308
PR. REL. IL RTE 3	1685+70.43	1687+40.00	RT	2,035	226
PR. REL. IL RTE 3	1687+40.00	1693+05.42		24,906	2,767

PORTLAND CEMENT CONCRETE PAVEMENT 12" (JOINTED) — TOTAL 651 SQ YD

LOCATION	STATION	STATION	LT / RT	AREA SQ FT	SUBTOTALS
EXCHANGE	52+52.49	54+74.68	RT	2,872	319
EXCHANGE	52+52.49	54+77.20	LT	2,985	332

PROTECTIVE COAT — TOTAL 41,235 SQ YD

	LENGTH FT	WIDTH FT	SUBTOTALS
FROM ITEM 42000501 PORTLND CEMENT CONCRETE PAVEMENT 10" (JOINTED)			18,509
FROM ITEM 42000521 PORTLND CEMENT CONCRETE PAVEMENT 11" (JOINTED)			7,467
FROM ITEM 42000541 PORTLND CEMENT CONCRETE PAVEMENT 12" (JOINTED)			651
FROM ITEM 42001420 BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)			662
FROM ITEM 42300400 PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH			523
FROM ITEM 42400200 PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH			298
FROM ITEM 48300500 PORTLAND CEMENT CONCRETE SHOULDERS 10"			5,572
FROM ITEM 48300600 PORTLAND CEMENT CONCRETE SHOULDERS 11"			4,014
FROM ITEM 48300600 PORTLAND CEMENT CONCRETE SHOULDERS 12"			387
FROM ITEM 60603800 COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12	901	2	200
FROM ITEM 60605000 COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24	1,214	3	405
FROM ITEM 60608600 COMBINATION CONCRETE CURB & GUTTER, TYPE M-6.06	34	1.5	6
FROM ITEM 60610400 COMBINATION CONCRETE CURB & GUTTER, TYPE M-6.24	255	3	85
FROM ITEM 60618320 CONCRETE MEDIAN SURFACE, 6 INCH			1,348
FROM ALL CONCRETE MEDIANS, TYPE B & TYPE M			805
FROM ITEM 60600095 CLASS SI CONCRETE (OUTLET)	100	4	303

BRIDGE APPROACH PAVEMENT CONNECTOR (PCC) — TOTAL 662 SQ YD

LOCATION	FROM STATION	TO STATION	NB / SB	AREA (SQ FT)	SUBTOTALS
PR. REL. IL RTE 3	1684+70.43	1685+70.43	SB	2,331	259.0
PR. REL. IL RTE 3	1684+70.43	1685+70.43	NB	1,216	135.1
PR. REL. IL RTE 3	1675+87.75	1676+87.75	SB	1,209	134.3
PR. REL. IL RTE 3	1675+87.75	1676+87.75	NB	1,200	133.3

PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH — TOTAL 523 SQ YD

LOCATION	STATION	LT / RT	AREA SQ FT	SUBTOTALS
EXCHANGE	44+75.84	RT	2,107	234
EXCHANGE	44+75.84	LT	2,597	289

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH — TOTAL 2,683 SQ FT

LOCATION	FROM STA.	OFFSET	TO STA.	OFFSET	WIDTH FEET	SUBTOTALS
EX. ST.CLAIR	120+00.0	22.94 RT	125+36.6	23.57 RT	5	2,683

PAVEMENT REMOVAL — TOTAL 8,598 SQ YD

LOCATION	STA.	OFF		TO STA.	TO OFF		TYPE	AREA (SF)	SUBTOTALS
LANDSDOWNE ST.	111+45.62	14.00	LT	111+48.00	944.00	LT	ASPHALT	17,038	1,893
EXCHANGE	35+11.34	12.24	LT	43+00.00	19.35	RT	ASPHALT	13,731	1,526
	43+00.00	0.51	LT	49+30.36	16.00	RT	ASPHALT	11,151	1,239
	49+30.36	7.75	RT	52+52.49	12.00	LT	CONCRETE	11,093	1,233
PR REL IL ROUTE 3	1689+95.31	116.87	LT	1692+59.32	15.54	RT	ASPHALT	18,967	2,107
	1692+16.04	126.86	LT	1691+89.62	120.00	LT	ASPHALT	205	23
DARLING	1017+75.00	11.00	LT	1020+13.70	8.30	RT	ASPHALT	5,200	578

HOT-MIX ASPHALT SURFACE REMOVAL, 2" — TOTAL 3,988 SQ YD

LOCATION	STA	STA	AREA SQ FT	SUBTOTALS
EX. ST. CLAIR	119+42.50	117+00.00	7,062	785
EX. ST. CLAIR	117+00.00	109+00.00	23,346	2,594
EX. ST. CLAIR	109+00.00	107+15.07	5,481	609

DRIVEWAY PAVEMENT REMOVAL — TOTAL 147 SQ YD

LOCATION	STA.	OFF		STA.	OFF.		AREA SQFT	SUBTOTALS
EXCHANGE	49+98.84	25.24	LT	50+38.64	12.17	LT	534	59
EXCHANGE	50+67.99	44.50	LT	51+03.86	13.00	LT	788	88

COMBINATION CURB AND GUTTER REMOVAL — TOTAL 1,376 FOOT

LOCATION	STA.	OFF.		STA.	OFF.		SUBTOTALS
EXCHANGE	49+30.26	7.75	LT	55+41.60	22.02	LT	623
	49+30.36	18.00	RT	50+63.19	70.00	RT	164
	49+88.98	11.79	LT	50+22.84	28.29	LT	47
	50+52.74	13.87	LT	50+88.86	44.50	LT	78
	50+97.19	70.00	RT	55+41.50	22.08	RT	464

SIDEWALK REMOVAL — TOTAL 2,683 SQ FT

LOCATION	FROM STA.	OFFSET	TO STA.	OFFSET	AREA SQ FT	SUBTOTALS
EX. ST.CLAIR	120+00.0	22.94 RT	125+36.6	28.57 RT	2,683	2,683

PAVED SHOULDER REMOVAL — TOTAL 674 SQ YD

LOCATION	STATION	OFFSET	STATION	OFFSET	AREA SQ FT	TYPE	SUBTOTALS
EXCHANGE	51+76.38	-12	55+41.84	-20	2,935	ASPHALT	326
EXCHANGE	51+45.19	12	55+41.84	20	3,130	ASPHALT	348

CLASS D PATCHES, TYPE I, 14 INCH — TOTAL 10 SQ YD

LOCATION	STA.	OFF	AREA SQ FT	SUBTOTALS
EX. ST. CLAIR	119+41.50	LT	44	4.89
EX. ST. CLAIR	119+41.50	RT	45.1	5.01

SAW CUTS — TOTAL 2,449 FOOT

LOCATION	STATION	OFFSET	STATION	OFFSET	TYPE	SUBTOTALS
EX. ST. CLAIR	107+15.07	15.79 RT	109+00.00	15.58 RT	LONGITUDINAL X2	370
EX. ST. CLAIR	109+00.00	15.58 RT	117+00.00	15.23 RT	LONGITUDINAL X2	1,600
EX. ST. CLAIR	117+00.00	15.23 RT	119+39.50	15.17 RT	LONGITUDINAL X2	479

AGGREGATE SHOULDERS, TYPE B 6" — TOTAL 3,540 SQ YD

LOCATION	STATION	STATION		AREA SQ FT	SUBTOTALS
CONNECTOR	20+99.7	22+50.0	RT	300	33
CONNECTOR	22+50.0	32+26.7	RT	1,991	221
CONNECTOR	21+15.0	22+50.0	LT	274	30
CONNECTOR	22+50.0	24+11.3	LT	360	40
CONNECTOR	24+42.8	32+30.4	LT	1,661	185
EX. ST. CLAIR	107+15.1	109+00.0	LT	370	41
EX. ST. CLAIR	109+00.0	113+75.6	LT	941	105
EX. ST. CLAIR	115+43.5	117+00.0	LT	318	35
EX. ST. CLAIR	117+00.0	118+37.3	LT	273	30
EX. ST. CLAIR	118+62.3	119+42.5	LT	158	18
EX. ST. CLAIR	119+42.5	125+36.6	RT	5,019	558
EXCHANGE	35+11.3	36+67.2	RT	312	35
EXCHANGE	39+39.4	43+00.0	RT	714	79
EXCHANGE	35+11.3	43+00.0	LT	1,578	175
EXCHANGE	43+00.0	44+38.3	RT	274	30
EXCHANGE	45+13.3	49+50.6	RT	872	97
EXCHANGE	51+48.7	52+52.5	RT	210	23
EXCHANGE	43+00.0	44+35.5	LT	274	30
EXCHANGE	45+16.2	52+52.5	LT	1,476	164
EXCHANGE	52+52.5	54+61.1	LT	424	47
EXCHANGE	52+52.5	54+53.3	RT	393	44
RAMP C	110+40.0	110+68.7	RT	57	6
RAMP C	110+40.0	110+67.7	LT	57	6
RAMP C	110+68.7	116+40.0	RT	1,126	125
RAMP C	110+67.7	116+40.0	LT	1,193	133
RAMP C	116+40.0	119+45.9	LT	618	69
RAMP C	116+40.0	119+61.0	RT	634	70
RAMP D	250+94.6	254+00.0	LT	628	70
RAMP D	250+32.4	254+00.0	RT	825	92
RAMP D	254+00.0	258+00.0	LT	853	95
RAMP D	254+00.0	258+00.0	RT	785	87
PR. REL. IL RTE 3	1668+15.7	1670+26.7	LT	485	54
PR. REL. IL RTE 3	1670+83.6	1676+87.8	LT	1,161	129
PR. REL. IL RTE 3	1668+15.7	1676+87.8	RT	1,350	150
PR. REL. IL RTE 3	1684+67.8	1687+40.0	RT	461	51
PR. REL. IL RTE 3	1684+72.8	1687+40.0	LT	265	29
PR. REL. IL RTE 3	1687+40.0	1692+77.8	LT	1,068	119
PR. REL. IL RTE 3	1687+40.0	1692+84.6	RT	1,131	126
WT DRIVE	900+50.0	902+74.4	LT	495	55
WT DRIVE	900+50.0	902+69.4	RT	478	53

HOT-MIX ASPHALT SHOULDERS, 8" — TOTAL 1,824 SQ YD

LOCATION	STATION	STATION		AREA SQ FT	SUBTOTALS
EX. ST. CLAIR	117+00.0	119+42.5	RT	1,940	216
EX. ST. CLAIR	109+00.0	117+00.0	RT	6,401	711
EX. ST. CLAIR	107+15.1	109+00.0	RT	1,480	164
EX. ST. CLAIR	119+42.48	117+00.00	LT	1,940	216
EX. ST. CLAIR	117+00.00	115+64.27	LT	1,086	121
EX. ST. CLAIR	112+28.57	109+00.00	LT	2,628	292
EX. ST. CLAIR	109+00.00	107+15.09	LT	941	105

PORTLAND CEMENT CONCRETE SHOULDERS 10" — TOTAL 5,572 SQ YD

LOCATION	STATION	STATION		AREA SQ FT	SUBTOTALS
RAMP C	110+68.46	116+40.00	RT	3,408	379
RAMP C	110+67.92	116+40.00	LT	2,373	264
RAMP C	110+39.97	110+68.46	RT	171	19
RAMP C	110+39.97	110+67.92	LT	114	13
RAMP C	116+40.00	119+55.41	RT	1,866	207
RAMP C	116+40.00	119+61.95	LT	1,225	136
RAMP D	254+00.00	250+26.55	RT	2,680	298
RAMP D	254+00.00	250+80.18	LT	1,245	138
RAMP D	258+00.00	254+00.00	RT	2,381	265
RAMP D	258+00.00	254+00.00	LT	1,693	188
EXCHANGE	35+11.34	43+00.00	LT	6,312	701
EXCHANGE	35+11.34	36+90.26	RT	1,428	159
EXCHANGE	39+47.09	43+00.00	RT	2,744	305
EXCHANGE	43+00.00	52+52.49	LT	7,620	847
EXCHANGE	43+00.00	49+66.25	RT	4,364	485
EXCHANGE	51+93.46	52+52.49	RT	480	53
CONNECTOR	22+50.00	32+28.46	RT	4,038	449
CONNECTOR	22+50.00	24+11.32	LT	735	82
CONNECTOR	24+39.16	32+32.37	LT	3,357	373
CONNECTOR	20+52.52	22+50.00	RT	942	105
CONNECTOR	20+50.40	22+50.00	LT	974	108

PORTLAND CEMENT CONCRETE SHOULDERS 11" — TOTAL 4,014 SQ YD

LOCATION	STATION	STATION		AREA SQ FT	SUBTOTALS
PR. REL. IL RTE 3	1668+15.65	1669+64.09	LT	1,079	120
PR. REL. IL RTE 3	1668+15.65	1676+87.75	RT	10,625	1,181
PR. REL. IL RTE 3	1671+17.43	1676+87.75	LT	5,931	659
PR. REL. IL RTE 3	1684+71.95	1687+40.00	LT	3,478	386
PR. REL. IL RTE 3	1684+68.63	1687+40.00	RT	2,969	330
PR. REL. IL RTE 3	1687+40.00	1692+84.63	LT	6,015	668
PR. REL. IL RTE 3	1687+40.00	1693+08.10	RT	6,025	669

PORTLAND CEMENT CONCRETE SHOULDERS 12" — TOTAL 387 SQ YD

LOCATION	STATION	STATION	LT / RT	AREA SQ FT	SUBTOTALS
EXCHANGE	52+52.49	54+77.20	LT	1,789	199
EXCHANGE	52+52.49	54+74.68	RT	1,692	188

PIPE CULVERT REMOVAL — TOTAL 22 FOOT

LOCATION	FROM STA	OFFSET	TO STA	OFFSET	SUBTOTALS
EX. ST. CLAIR	123+46.24	38.2 RT	123+68.13	38.2 RT	22

STORM SEWER REMOVAL 6" — TOTAL 119 FOOT

LOCATION	STA	OFF	RT/LT	STA	OFF	RT/LT	SUBTOTALS
PR. REL. IL RTE 3	1668+50.29	6.3	RT	1668+99.31	16.3	LT	54
PR. REL. IL RTE 3	1668+99.31	16.3	LT	1669+42.87	35.0	LT	47
PR. REL. IL RTE 3	1692+60.62	21.7	LT	1692+73.43	10.51	LT	18

STORM SEWER REMOVAL 8" — TOTAL 85 FOOT

LOCATION	STA	OFF	RT/LT	STA	OFF	RT/LT	SUBTOTALS
PR. REL. IL RTE 3	1689+95.62	120.0	LT	1690+17.02	106.4	LT	25
PR. REL. IL RTE 3	1689+97.46	80.2	LT	1690+17.77	105.9	LT	33
PR. REL. IL RTE 3	1690+09.62	71.2	LT	1690+36.66	74.0	LT	27

STORM SEWER REMOVAL 12" — TOTAL 502 FOOT

LOCATION	STA	OFF	RT/LT	STA	OFF	RT/LT	SUBTOTALS
PR. REL. IL RTE 3	1690+15.55	120.00	LT	1690+17.77	105.90	LT	14
PR. REL. IL RTE 3	1690+36.66	73.98	LT	1690+44.99	5.63	RT	80
EXCHANGE	50+23.63	33.98	RT	50+29.22	20.08	RT	15
EXCHANGE	50+32.00	18.21	RT	50+68.49	17.52	RT	36
EXCHANGE	51+65.00	18.00	RT	51+65.00	0.99	LT	19
EXCHANGE	51+65.00	10.00	LT	51+65.00	6.99	LT	3
EXCHANGE	51+85.60	19.96	LT	51+66.87	12.72	LT	20
STORM SEWER, UNVERIFIED, SIZE UNKNOWN							
RAMP C	118+34.28	12.10	RT	119+02.93	102.21	RT	113
EXCHANGE	40+75.52	52.42	RT	41+88.40	75.00	RT	119
EXCHANGE	45+36.52	75.00	RT	45+37.95	62.63	RT	12
EXCHANGE	45+37.95	62.63	RT	45+65.00	68.74	RT	28
EXCHANGE	45+65.00	68.74	RT	45+64.00	75.00	RT	6
PR. REL. IL RTE 3	1690+92.76	93.54	LT	1690+55.76	89.56	LT	37

STORM SEWER REMOVAL 15" — TOTAL 147 FOOT

LOCATION	STA	OFF	RT/LT	STA	OFF	RT/LT	SUBTOTALS
PR. REL. IL RTE 3	1690+10.53	120.0	LT	1690+17.77	105.9	LT	16
PR. REL. IL RTE 3	1690+17.77	105.9	LT	1690+36.66	74.0	LT	37
PR. REL. IL RTE 3	1691+94.55	120.0	RT	1692+88.68	12.6	RT	94

STORM SEWER REMOVAL 18" — TOTAL 383 FOOT

LOCATION	STA	OFF	RT/LT	STA	OFF	RT/LT	SUBTOTALS
PR. REL. IL RTE 3	1689+50.83	11.91	RT	1692+74.45	9.61	LT	324
EXCHANGE	50+71.19	53.09	RT	50+71.23	6.26	LT	59

FIRE HYDRANTS TO BE REMOVED — TOTAL 1 EACH

LOCATION	STA.	OFF.	SUBTOTALS
RAMP C	118+19.74	89.6 RT	1

CONCRETE HEADWALLS FOR PIPE DRAINS — TOTAL 10 EACH

LOCATION	STA.	OFF.	SUBTOTALS
RAMP C	118+47.77	17.0 RT	1
RAMP C	118+47.77	30.0 LT	1
RAMP D	251+44.75	20.0 RT	1
RAMP D	251+44.75	39.0 LT	1
EXCHANGE	51+68.21	47.9 RT	1
EXCHANGE	51+68.21	38.0 LT	1
CONNECTOR	31+09.00	35.4 RT	1
CONNECTOR	31+09.00	25.7 LT	1
PR. REL. IL RTE 3	1668+35.46	44.5 RT	1
PR. REL. IL RTE 3	1668+35.46	57.0 LT	1

PIPE UNDERDRAINS 4" — TOTAL 651 FOOT

LOCATION	FROM STA.	FROM OFF.	TO STA.	TO OFF.	SUBTOTALS
RAMP C	118+47.77	16.0 LT	118+47.77	6.0 RT	20
RAMP C	118+47.77	0.0 LT	118+47.77	20.0 LT	6
RAMP D	251+44.75	0.0 RT	251+44.75	6.87 RT	7
RAMP D	251+44.75	0.0 LT	251+44.75	28.7 LT	29
EXCHANGE	51+68.21	0.0 RT	51+68.21	41.2 RT	41
EXCHANGE	51+68.21	0.0 LT	51+68.21	31.0 LT	31
CONNECTOR	31+09.00	0.0 RT	31+09.00	28.0 RT	28
CONNECTOR	31+09.00	0.0 LT	31+09.00	18.1 LT	18
PR. REL. IL RTE 3	1668+35.46	0.0 RT	1668+35.46	33.0 RT	33
PR. REL. IL RTE 3	1668+35.46	0.0 LT	1668+35.46	45.0 LT	45
EX. ST. CLAIR	117+61.00	14.7 LT	119+41.50	15.4 LT	181
EX. ST. CLAIR	117+61.00	17.9 RT	119+41.50	15.7 RT	181
EX. ST. CLAIR	119+41.50	15.7 RT	119+41.50	15.4 LT	31

PIPE UNDERDRAINS 4" (SPECIAL) — TOTAL 113 FOOT

LOCATION	FROM STA.	FROM OFF.	TO STA.	TO OFF.	SUBTOTALS
RAMP C	115+47.77	6.0 RT	115+47.77	17.0 RT	11
RAMP C	115+47.77	20.0 LT	115+47.77	30.0 LT	10
RAMP D	251+44.75	6.87 RT	251+44.75	20.0 RT	13.13
RAMP D	251+44.75	28.7 LT	251+44.75	39.0 LT	10.3
EXCHANGE	51+68.21	41.2 RT	51+68.21	47.9 RT	6.7
EXCHANGE	51+68.21	31.0 LT	51+68.21	38.0 LT	7
CONNECTOR	31+09.00	28.0 RT	31+09.00	35.4 RT	7.4
CONNECTOR	31+09.00	18.1 LT	31+09.00	25.7 LT	7.6
PR. REL. IL RTE 3	1668+35.46	33.0 RT	1668+35.46	44.5 RT	11.5
PR. REL. IL RTE 3	1668+35.46	45.0 LT	1668+35.46	57.0 LT	12
EX. ST. CLAIR	119+41.50	15.4 LT	119+41.50	31.5 LT	16

MANHOLES TO BE ADJUSTED — TOTAL 12 EACH

LOCATION	STATION	OFFSET		EX. RIM	PR. RIM	SUBTOTALS
EXCHANGE	36+19.87	60.29	LT	422.35	422.35	1
EXCHANGE	38+79.90	97.43	RT	413.45	412.00	1
EXCHANGE	39+30.01	70.4	RT	415.06	414.00	1
EXCHANGE	41+24.74	60.84	RT	417.90	417.90	1
EXCHANGE	43+90.39	12.14	LT	416.62	418.44	1
EXCHANGE	46+95.50	11.3	LT	416.78	417.41	1
EXCHANGE	50+71.23	6.26	LT	416.21	416.21	1
EXCHANGE	51+65.00	3.99	LT	415.94	415.96	1
EXCHANGE	51+93.76	3.09	LT	416.01	416.01	1
EXCHANGE	54+22.82	15.59	RT	418.89	418.89	1
EXCHANGE	55+00.27	15.09	LT	419.37	419.38	1
EXCHANGE	55+13.04	17.78	LT	419.32	419.35	1

MANHOLES TO BE RECONSTRUCTED — TOTAL 1 EACH

LOCATION	STATION	OFFSET		EX. RIM ELEV	PR. RIM ELEV	SUBTOTALS
EXCHANGE	41+35.76	48.49	RT	416.56	414.39	1

VALVE VAULTS TO BE ADJUSTED — TOTAL 16 EACH

LOCATION	STATION	OFFSET		SUBTOTALS
EXCHANGE	42+31.18	36.88	RT	1
EXCHANGE	44+30.45	55.21	RT	1
EXCHANGE	49+84.51	28.72	RT	1
EXCHANGE	49+90.05	28.77	RT	1
EXCHANGE	51+84.23	7.00	RT	1
EX. ST. CLAIR	109+15.51	31.64	RT	1
EX. ST. CLAIR	109+15.46	40.41	RT	1
EX. ST. CLAIR	115+68.52	23.75	LT	1
EX. ST. CLAIR	115+77.10	21.97	LT	1
EX. ST. CLAIR	115+82.62	12.84	LT	1
EX. ST. CLAIR	115+90.43	12.97	LT	1
EX. ST. CLAIR	116+11.93	51.09	LT	1
EX. ST. CLAIR	116+26.23	19.99	LT	1
EX. ST. CLAIR	116+47.88	37.57	RT	1
EX. ST. CLAIR	116+55.75	27.42	RT	1
EX. ST. CLAIR	116+67.68	22.20	LT	1

CLASS SI CONCRETE (OUTLET) — TOTAL 7.4 CU YD

LOCATION	STATION	OFFSET	STATION	OFFSET	SUBTOTALS
EXCHANGE	54+60.20	-29.4	55+00.00	-20.7	3.60
	54+52.22	29.9	55+00.00	20.7	3.79

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 — TOTAL 901 FOOT

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
PR. REL. IL RTE 3	1685+27.51	-10.0	1687+40.00	-22.0	213
	1685+27.51	-6.0	1687+40.00	-6.0	212
	1687+40.00	-22.0	1689+77.98	-22.0	238
	1687+40.00	-6.0	1689+77.98	-18.0	238

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 — TOTAL 1,214 FOOT

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
EXCHANGE	38+47.90	34.0	38+67.18	34.4	20
EXCHANGE	55+00.00	-20.7	55+41.84	-20.0	44
EXCHANGE	55+00.00	20.7	55+41.84	20.0	40
PR. REL. IL RTE 3	1671+41.01	-23.0	1676+87.75	-23.0	554
PR. REL. IL RTE 3	1671+39.53	-12.7	1676+87.75	-5.0	556

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06 — TOTAL 34 FOOT

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
RAMP C	120+06.0	1.6	120+20.0	-5.3	15
RAMP C	120+05.0	3.6	120+20.0	21.2	19

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24 — TOTAL 255 FOOT

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
RAMP C	119+33.9	-20.0	120+13.6	-32.7	76
RAMP C	119+55.7	3.3	120+24.5	96.0	118
RAMP D	251+04.7	-30.1	250+39.3	-42.3	61

CONCRETE MEDIAN SURFACE, 6 INCH — TOTAL 12,130 SQ FT

LOCATION	STATION	STATION	SUBTOTALS
RAMP C	119+85.13	120+13.28	775
	120+06.74	120+19.28	136
PR. REL. IL RTE 3	1671+39.76	1676+87.75	6,937
	1685+27.51	1687+40.00	1,952
	1687+40.00	1689+77.98	2,330

CONCRETE MEDIAN, TYPE SB-6.12 — TOTAL 5,211 SQ FT

LOCATION	DESCRIPTION	FROM STA.	TO STA.	AREA SQ FT	SUBTOTALS
EXCHANGE	MEDIAN	38+53.6	43+00.00	2,664	2,664
	MEDIAN	43+00.00	44+14.40	673	673
PR. REL. IL RTE 3	MEDIAN	1684+70.43	1685+27.51	346	346
	MEDIAN	1689+77.98	1692+42.0	1,528	1,528

CONCRETE MEDIAN, TYPE SB-6.24 — TOTAL 1,912 SQ FT

LOCATION	DESCRIPTION	FROM STA.	TO STA.	AREA SQ FT	SUBTOTALS
PR. REL. IL RTE 3	MEDIAN NOSE	1668+15.65	1670+06.64	1,831	1,831
PR. REL. IL RTE 3	MEDIAN NOSE	1671+30.52	1671+41.01	81	81

CONCRETE MEDIAN, TYPE SM-6.24 — TOTAL 119 SQ FT

LOCATION	DESCRIPTION	FROM STA.	TO STA.	SUBTOTALS
EXCHANGE	MEDIAN NOSE @ RAMPS C&D	38+01.78	38+20.05	119

STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS — TOTAL 700 FOOT

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
PR. REL. IL RTE 3	1673+75.00	22.9	1676+62.50	17.0	287.5
PR. REL. IL RTE 3	1684+64.16	17.0	1685+39.16	17.0	75.0
PR. REL. IL RTE 3	1676+16.58	-45.0	1676+91.58	-45.0	75.0
PR. REL. IL RTE 3	1684+98.21	-45.0	1687+60.71	-45.0	262.5

TRAFFIC BARRIER TERMINAL, TYPE 2 — TOTAL 2 EACH

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
PR. REL. IL RTE 3	1676+04.08	-45.0	1676+16.58	-45.0	1
PR. REL. IL RTE 3	1685+39.16	17.0	1685+51.66	17.0	1

TRAFFIC BARRIER TERMINAL, TYPE 5 — TOTAL 2 EACH

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
PR. REL. IL RTE 3	1676+91.58	-45.0	1677+06.25	-45.4	1
PR. REL. IL RTE 3	1684+49.49	17.4	1684+64.16	17.0	1

TRAFFIC BARRIER TERMINAL, TYPE 6 — TOTAL 2 EACH

LOCATION	STATION	OFFSET	STATION	OFFSET	SUBTOTALS
PR. REL. IL RTE 3	1676+62.50	17.0	1677+06.25	17.4	1
PR. REL. IL RTE 3	1684+54.46	-45.4	1684+98.21	-45.0	1

TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT — TOTAL 2 EACH

LOCATION	FROM STATION	FROM OFFSET	TO STATION	TO OFFSET	SUBTOTALS
PR. REL. IL RTE 3	1673+25.00	25.3	1673+75.00	17.0	1
PR. REL. IL RTE 3	1687+60.71	-45.0	1688+10.71	-46.0	1

DELINEATORS — TOTAL 83 EACH

LOCATION	FROM STA.	TO STA.	LT / RT	LENGTH	RADIUS	SPACING	NO. SIDES	SUBTOTALS
RAMP C	110+40.00	112+52.71	LT	213	835	80	1	3
	112+52.71	117+07.96	LT	455	410	55	1	9
	117+07.96	120+00.00	LT	292	NA	110,170	1	2
RAMP D	251+00.00	252+81.19	LT	181	NA	110	1	2
	252+81.19	258+00.00	LT	519	380	55	1	10
CONNECTOR	21+15.00	31+16.63	LT & RT	1002	600	70	2	30
EXCHANGE	35+11.34	55+41.84	LT	2031	NA	400	1	6
	35+11.31	36+67.74	RT	156	NA	400	1	1
	39+92.66	44+08.44	RT	416	NA	400	1	2
	45+43.44	49+59.78	RT	416	NA	400	1	2
	52+52.49	55+41.84	RT	289	NA	400	1	1
PR. REL. IL RTE 3	1668+15.65	1674+00.00	RT	584	NA	400	1	2
	1671+43.65	1674+80.00	LT	336	NA	400	1	1
	1687+00.00	1692+18.75	RT	519	NA	400	1	2
	1687+27.51	1692+18.75	LT	491	NA	400	1	2
EX. ST. CLAIR	107+15.07	119+42.50	LT & RT	1227	NA	400	2	8

CHAIN LINK FENCE, 6' — TOTAL 5,044 FOOT

LOCATION	FROM STA.	FROM OFF.	TO STA.	TO OFF.	SUBTOTALS
DARLING	1006+91.37	61.1 RT	1009+40.24	32.59 RT	254
	1009+59.81	32.38 RT	1011+66.89	41.7 RT	212
	1011+66.89	41.7 RT	1016+60.03	31.2 RT	488
	1016+60.03	31.2 RT	1019+94.57	18.2 RT	365
RAMP C	115+75.69	83.1 RT	116+18.60	92.6 RT	35
	116+18.60	92.6 RT	119+44.11	101.4 RT	305
PR. REL. IL RTE 3	1685+91.83	164.0 LT	1687+48.73	141.9 LT	158
	1687+48.73	141.9 LT	1692+41.63	283.7 LT	530
	1685+65.56	166.0 RT	1687+40.00	143.2 RT	176
	1687+40.00	143.2 RT	1692+71.72	166.3 RT	559
EXCHANGE	41+13.11	74.0 RT	43+00.00	74.0 RT	187
	43+00.00	74.0 RT	44+55.06	74.0 RT	155
	44+96.38	74.0 RT	49+05.19	74.0 RT	409
	52+46.48	68.5 RT	55+41.45	48.6 RT	276
EX. ST. CLAIR	119+20.00	64.0 LT	118+59.92	64.0 LT	60
	118+39.92	64.0 LT	117+00.00	64.0 LT	140
	117+00.00	64.0 LT	115+50.09	115.0 LT	179
PR. REL. IL RTE 3	1672+01.37	163.4 RT	1677+56.36	127.2 RT	556

CHAIN LINK GATES, 6' X 20' DOUBLE — TOTAL 2 EACH

LOCATION	STA	SUBTOTALS
EX. ST. CLAIR	118+49.92	1
DARLING	1009+50.00	1

FURNISHING AND ERECTING RIGHT OF WAY MARKERS — TOTAL 36 EACH

ALIGNMENT	STA.	OFF.			SUBTOTALS
PR. REL. IL RTE 3	1666+56.62	159.93	RT		1
PR. REL. IL RTE 3	1666+59.26	199.84	RT		1
PR. REL. IL RTE 3	1666+59.26	175.38	LT		1
PR. REL. IL RTE 3	1668+97.05	163.00	LT		1
CONNECTOR	22+12.96	105.00	LT		1
CONNECTOR	23+69.00	105.00	LT		1
CONNECTOR	24+54.96	292.60	LT		1
CONNECTOR	25+38.86	105.00	LT		1
CONNECTOR	26+00.00	105.00	LT		1
CONNECTOR	27+00.00	75.00	LT		1
CONNECTOR	31+16.63	75.00	LT		1
CONNECTOR	31+39.37	75.00	LT		1
EX. ST. CLAIR	116+01.07	65.00	LT		1
EX. ST. CLAIR	119+55.00	65.00	LT		1
EX. ST. CLAIR	119+55.00	29.52	LT	METHOD B	1
EX. ST. CLAIR	119+55.00	50.48	RT	METHOD B	1
EX. ST. CLAIR	119+55.00	70.00	RT		1
PR. REL. IL RTE 3	1677+56.42	128.18	RT		1
PR. REL. IL RTE 3	1677+82.52	68.32	RT		1
EX. ST. CLAIR	107+78.90	70.00	RT		1
PR. REL. IL RTE 3	1678+62.72	68.69	RT		1
PR. REL. IL RTE 3	1679+88.24	60.48	RT		1
PR. REL. IL RTE 3	1680+88.54	130.00	LT		1
PR. REL. IL RTE 3	1685+65.69	167.00	RT		1
PR. REL. IL RTE 3	1687+69.43	140.00	LT		1
PR. REL. IL RTE 3	1689+25.00	120.00	RT		1
PR. REL. IL RTE 3	1689+25.00	120.00	LT		1
PR. REL. IL RTE 3	1691+90.76	120.00	LT		1
PR. REL. IL RTE 3	1692+20.85	120.00	RT		1
EXCHANGE	34+96.00	15.45	RT	METHOD B	1
EXCHANGE	35+26.40	43.36	RT		1
EXCHANGE	47+58.32	75.00	RT		1
EXCHANGE	49+05.19	75.00	RT		1
EXCHANGE	52+46.48	69.50	RT		1
EXCHANGE	55+41.84	49.50	RT		1
RAMP D	251+79.31	236.68	RT		1

SEPARATION JOINT WITH SLEEPER SLAB — TOTAL 676 FOOT

LOCATION	STA.	OFF.	STA.	OFF.	SUBTOTALS
EXCHANGE	36+90.26	26.6 RT	36+91.34	23.0 RT	4
	36+91.34	23.0 RT	39+58.62	23.0 RT	267
	39+58.62	23.0 RT	39+59.11	25.8 RT	3
EXCHANGE	49+66.25	35.3 RT	49+66.25	23.0 RT	12.3
	49+66.25	23.0 RT	51+91.25	23.0 RT	225
PR. REL. IL RTE 3	51+91.25	23.0 RT	51+92.12	28.1 RT	5
	1669+64.09	50.4 LT	1669+65.00	47.0 LT	4
	1669+65.00	47.0 LT	1671+15.00	47.0 LT	150
	1671+15.00	47.0 LT	1671+17.43	52.5 LT	6

REMOVE ABANDONED GAS MAIN — TOTAL 300 FOOT

LOCATION	FROM STA	FROM OFF	TO STA	TO OFF	SUBTOTALS
PR. REL. IL RTE 3	1689+52.80	39.2 RT	1692+52.12	23.0 RT	300

UTILITY STRUCTURE REMOVAL — TOTAL 31 EACH

LOCATION	STA	OFF	RT/LT	SUBTOTALS
PR. REL. IL RTE 3	1668+78.13	14.57	LT	1
PR. REL. IL RTE 3	1668+99.31	16.30	LT	1
PR. REL. IL RTE 3	1669+20.78	18.87	LT	1
PR. REL. IL RTE 3	1690+17.77	105.90	LT	1
PR. REL. IL RTE 3	1690+36.66	73.98	LT	1
PR. REL. IL RTE 3	1690+44.94	5.63	RT	1
PR. REL. IL RTE 3	1690+93.63	80.05	RT	1
PR. REL. IL RTE 3	1691+57.22	77.78	RT	1
PR. REL. IL RTE 3	1692+48.55	114.53	RT	1
PR. REL. IL RTE 3	1692+74.53	9.56	LT	1
PR. REL. IL RTE 3	1692+84.93	30.54	RT	1
PR. REL. IL RTE 3	1692+94.12	25.35	LT	1
PR. REL. IL RTE 3	1692+94.03	22.81	LT	1
EXCHANGE	42+13.11	17.07	LT	1
EXCHANGE	42+30.29	24.98	LT	1
EXCHANGE	42+30.90	12.72	LT	1
EXCHANGE	42+31.88	17.24	LT	1
EXCHANGE	48+01.49	32.30	LT	1
EXCHANGE	48+04.09	34.05	LT	1
EXCHANGE	50+23.20	35.36	RT	1
EXCHANGE	50+30.00	18.24	RT	1
EXCHANGE	50+70.99	17.48	RT	1
EXCHANGE	51+65.00	20.00	RT	1
EXCHANGE	51+65.00	12.00	LT	1
EXCHANGE	51+84.23	7.00	RT	1
EXCHANGE	51+87.00	20.50	LT	1
EXCHANGE	52+22.87	57.75	RT	1
UNVERIFIED				
RAMP C	118+34.28	12.10	RT	1
EXCHANGE	36+08.32	65.93	LT	1
EXCHANGE	35+86.34	75.93	LT	1
PR. REL. IL RTE 3	1690+92.76	93.54	LT	1

AGGREGATE SURFACE COURSE, TYPE B 8" — TOTAL 160 SQ YD

LOCATION	STATION	OFFSET LT / RT	SUBTOTALS
EX. ST. CLAIR	120+95.29	RT	51.9
EX. ST. CLAIR	123+57.11	RT	32.8
DARLING	1009+50.00	RT	75.0

WATER MAIN REMOVAL, 4" — TOTAL 428 FOOT

LOCATION	FROM STA	FROM OFF	TO STA	TO OFF	SUBTOTALS
PR. REL. IL RTE 3	1689+80.06	3.8 RT	1691+54.02	120.0 LT	228
PR. REL. IL RTE 3	1689+88.31	2.0 LT	1691+36.71	120.0 LT	200

WATER MAIN REMOVAL, 6" — TOTAL 300 FOOT

LOCATION	FROM STA	FROM OFF	FROM RT/LT	TO STA	TO OFF	TO RT/LT	SUBTOTALS
RAMP C	118+65.43	0.1	RT	118+29.20	101.9	RT	108
RAMP C	118+19.74	89.6	RT	118+32.20	93.9	RT	13
RAMP C	11678.7	79.11	LT	11645.82	97.62	RT	179

WATER MAIN REMOVAL, 8" — TOTAL 432 FOOT

LOCATION	FROM STA	FROM OFF	TO STA	TO OFF	SUBTOTALS
PR. REL. IL RTE 3	1689+51.65	23.2 RT	1693+28.38	1.1 LT	385
PR. REL. IL RTE 3	1689+74.77	6.0 RT	1689+79.41	52.3 RT	47

WATER MAIN REMOVAL, 10" — TOTAL 335 FOOT

LOCATION	FROM STA	FROM OFF	TO STA	TO OFF	SUBTOTALS
PR. REL. IL RTE 3	1689+50.79	11.3 RT	1691+42.04	120 LT	248
	1689+99.99	38.08 LT	1690+11.26	120 LT	87

CHAIN LINK FENCE REMOVAL — TOTAL 3,123 FOOT

LOCATION	FROM STA	OFFSET	TO STA	OFFSET	SUBTOTALS
CONNECTOR	25+50.76	105.00 LT	31+97.47	196.20 RT	652
PR. REL. IL RTE 3	1667+54.81	166.71 LT	1671+76.57	175.19 LT	433
EXCHANGE	36+16.41	90.00 LT	36+77.12	48.82 LT	128
	37+25.35	39.22 LT	42+23.50	32.73 LT	502
	41+12.68	71.83 RT	43+00.00	38.58 RT	261
	43+00.00	38.58 RT	49+49.50	29.43 RT	650
	47+77.53	28.63 RT	47+78.16	47.26 RT	19
	47+77.04	70.37 RT	47+77.24	75.00 RT	5
	48+00.52	27.81 LT	49+19.64	26.83 LT	119
RAMP C	115+75.51	83.49 RT	116+18.40	92.85 RT	35
	116+18.44	92.85 RT	119+43.87	100.36 RT	305
DARLING	1019+93.33	11.75 RT	1019+93.68	18.74 RT	7
DARLING	1019+92.13	12.22 LT	1019+91.79	19.21 LT	7

CHAIN LINK GATES (SPECIAL) — TOTAL 1 EACH

LOCATION	FROM STA.	FROM OFF.		SUBTOTALS
EXCHANGE	44+75.84	77 RT	POLARVILLE DW	1

CHAIN LINK GATES REMOVAL — TOTAL 1 EACH

LOCATION	FROM STA	OFFSET	LT/RT	TO STA	OFFSET	LT/RT	SUBTOTALS
DARLING	1019+92.13	12.22	LT	1019+93.33	11.75	RT	1

WOVEN WIRE FENCE REMOVAL — TOTAL 654 FOOT

LOCATION	FROM STA	FROM OFF	TO STA	TO OFF	SUBTOTALS
EX. ST. CLAIR	119+55.09	33.41 LT	117+00.00	37.74 LT	255
EX. ST. CLAIR	117+00.00	37.74 LT	116+47.24	65.00 LT	78
EX. ST. CLAIR	115+94.25	65.00 LT	112+79.89	56.58 LT	321

AGGREGATE SUBGRADE 12" — TOTAL 41,326 SQ YD

LOCATION	FROM STATION	TO STATION	AREA SQ FT	SUBTOTALS
RAMP C	110+40.00	110+68.80	769	85
RAMP C	110+68.80	116+40.00	15,658	1,740
RAMP C	116+40.00	120+30.17	14,309	1,590
RAMP D	250+22.60	254+00.00	17,192	1,910
RAMP D	254+00.00	258+00.00	12,691	1,410
EXCHANGE	35+11.34	43+00.00	45,243	5,027
EXCHANGE	43+00.00	52+52.49	62,897	6,989
EXCHANGE	52+52.49	55+41.84	4,984	554
EXCHANGE	52+52.49	55+41.84	5,204	578
PR. REL. IL RTE 3	1668+15.65	1676+87.75	64,100	7,122
PR. REL. IL RTE 3	1684+70.43	1687+40.00	16,520	1,836
PR. REL. IL RTE 3	1687+40.00	1692+98.33	37,530	4,170
CONNECTOR	20+47.00	22+50.00	12,560	1,396
CONNECTOR	22+50.00	32+40.64	42,727	4,747
WT DRIVE	900+48.00	902+99.68	6,294	699
EX. ST. CLAIR	107+91.80	109+00.00	71	8
EX. ST. CLAIR	109+00.00	117+00.00	5,204	578
EX. ST. CLAIR	117+00.00	118+96.20	314	35
EX. ST. CLAIR	119+42.48	117+00.00	2,234	248
SHOULDERS	117+00.00	115+64.27	1,246	138
LT SIDE ONLY	112+28.57	109+00.00	3,018	335
	109+00.00	107+15.09	1,167	130

CONCRETE REMOVAL (SPECIAL) — TOTAL 2,984 SQ YD

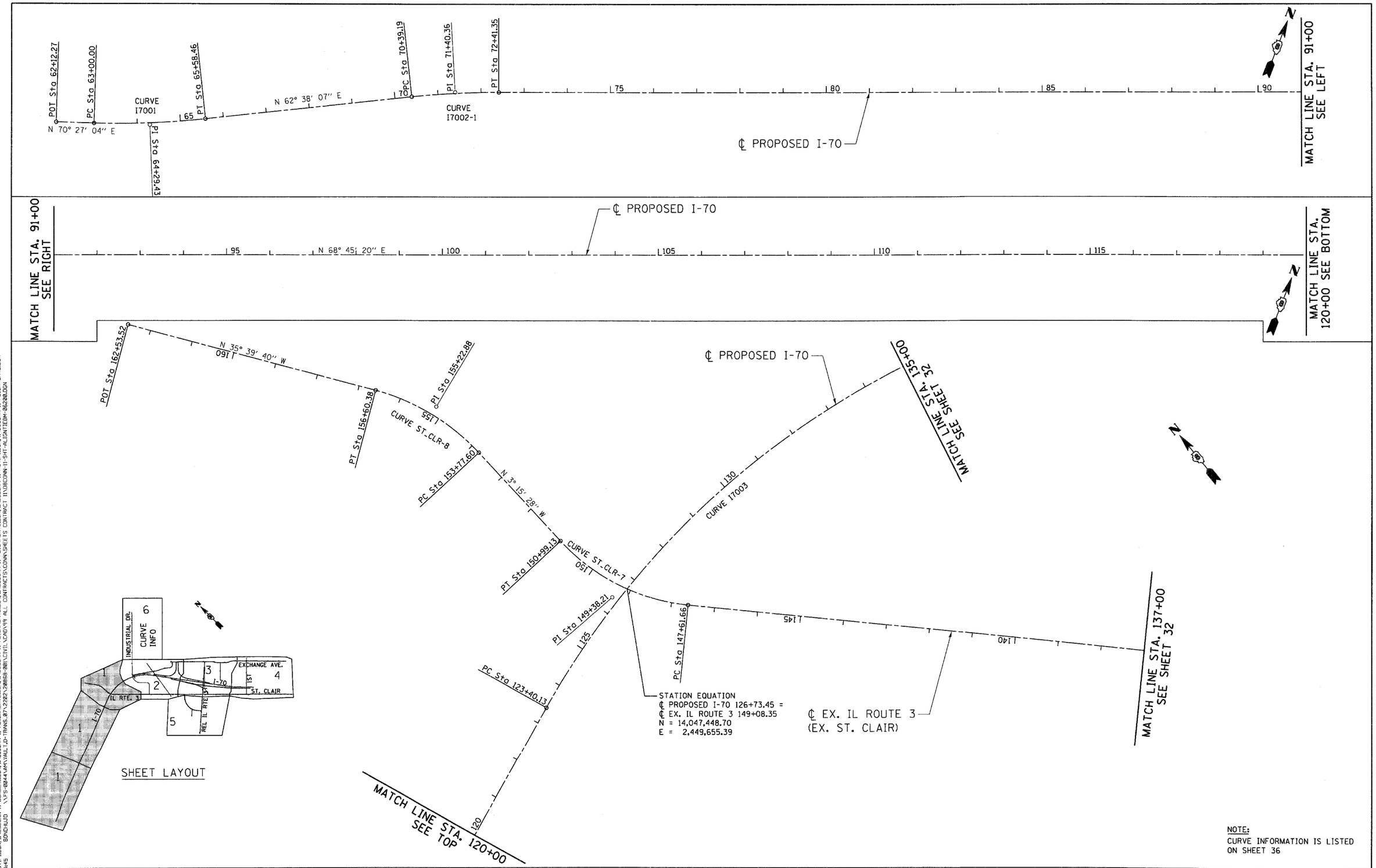
LOCATION	FROM STA.	FROM OFF.	TO STA.	TO OFF.	AREA SQFT	SUBTOTALS
EXCHANGE	36+85.11	171.79 RT	38+06.70	108.65 RT	961.0	107
	41+75.38	90.00 LT	43+00.00	75.18 LT	1,775.9	197
	43+00.00	90.00 LT	47+91.23	61.35 LT	8,941.2	993
	46+75.68	62.60 LT	47+96.10	29.46 LT	3,744.8	416
	46+75.16	29.27 RT	46+85.71	43.34 RT	154.2	17
	48+70.72	90.00 LT	50+24.76	39.12 LT	6,456.6	717
	43+04.48	55.07 RT	45+19.34	75.00 RT	4,823.0	536

FENCE REMOVAL — TOTAL 336 FOOT

LOCATION	FROM STA.	FROM OFF.	TO STA.	TO OFF.	SUBTOTALS
EX. ST. CLAIR	109+18.75	62 LT	111+30.71	63 LT	212
EX. ST. CLAIR	111+56.70	63 LT	112+80.02	60 LT	124

HOT-MIX ASPHALT BINDER COURSE, IL19 FG, N70 — TOTAL 34 TON

LOCATION	STA	LT / RT	AREA SQ FT	THICKNESS IN.	RATE LBS/SY/IN	SUBTOTALS
EX. ST. CLAIR	118+49.92	LT	675	8	112	34



NOTE:
CURVE INFORMATION IS LISTED ON SHEET 36

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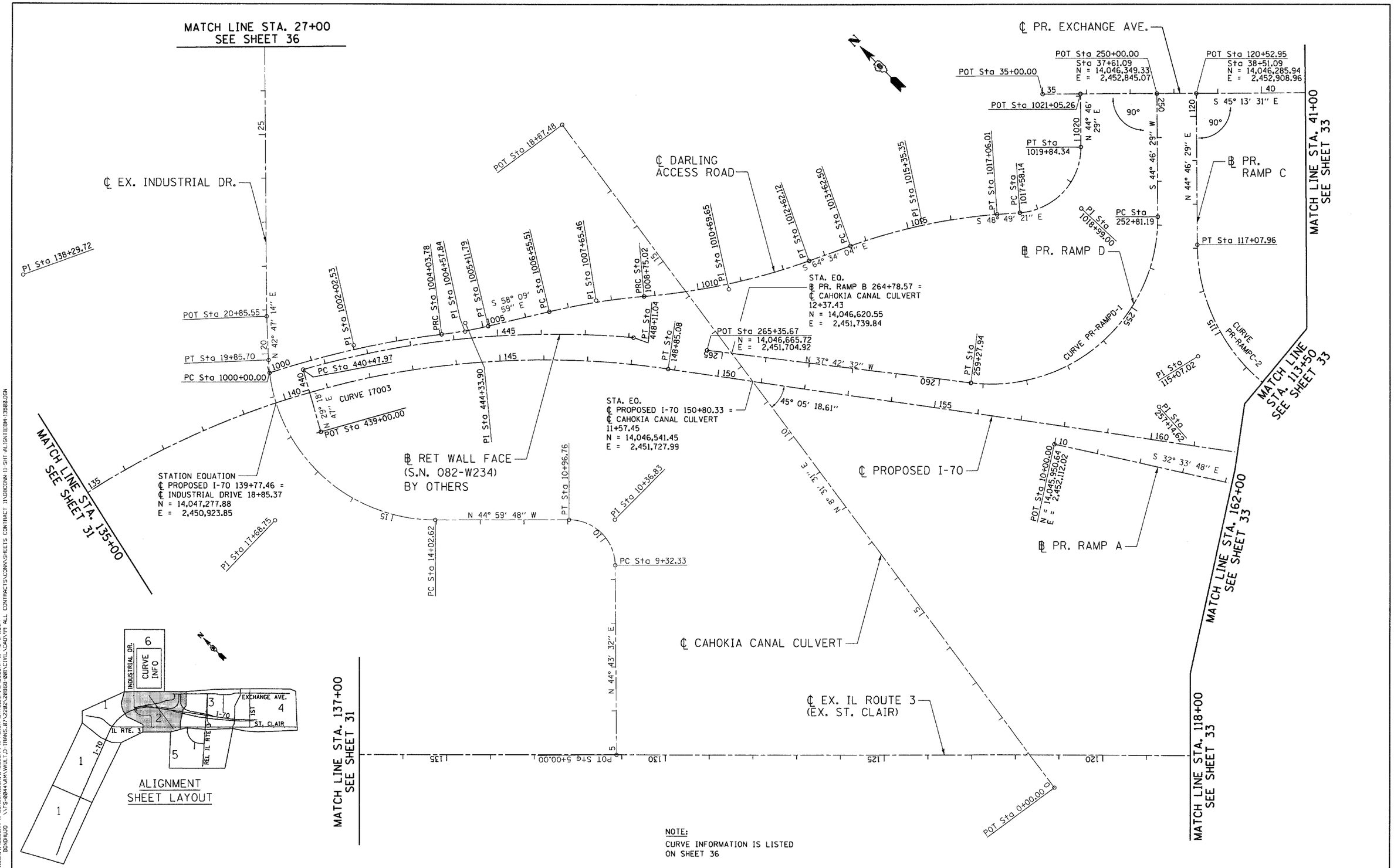
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353 / 31
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	SCALE: 1"=100' / SHEET NO. 1 OF 8 SHEETS / STA. TO STA.	CONTRACT NO. 76E06			
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



NOTE:
CURVE INFORMATION IS LISTED
ON SHEET 36

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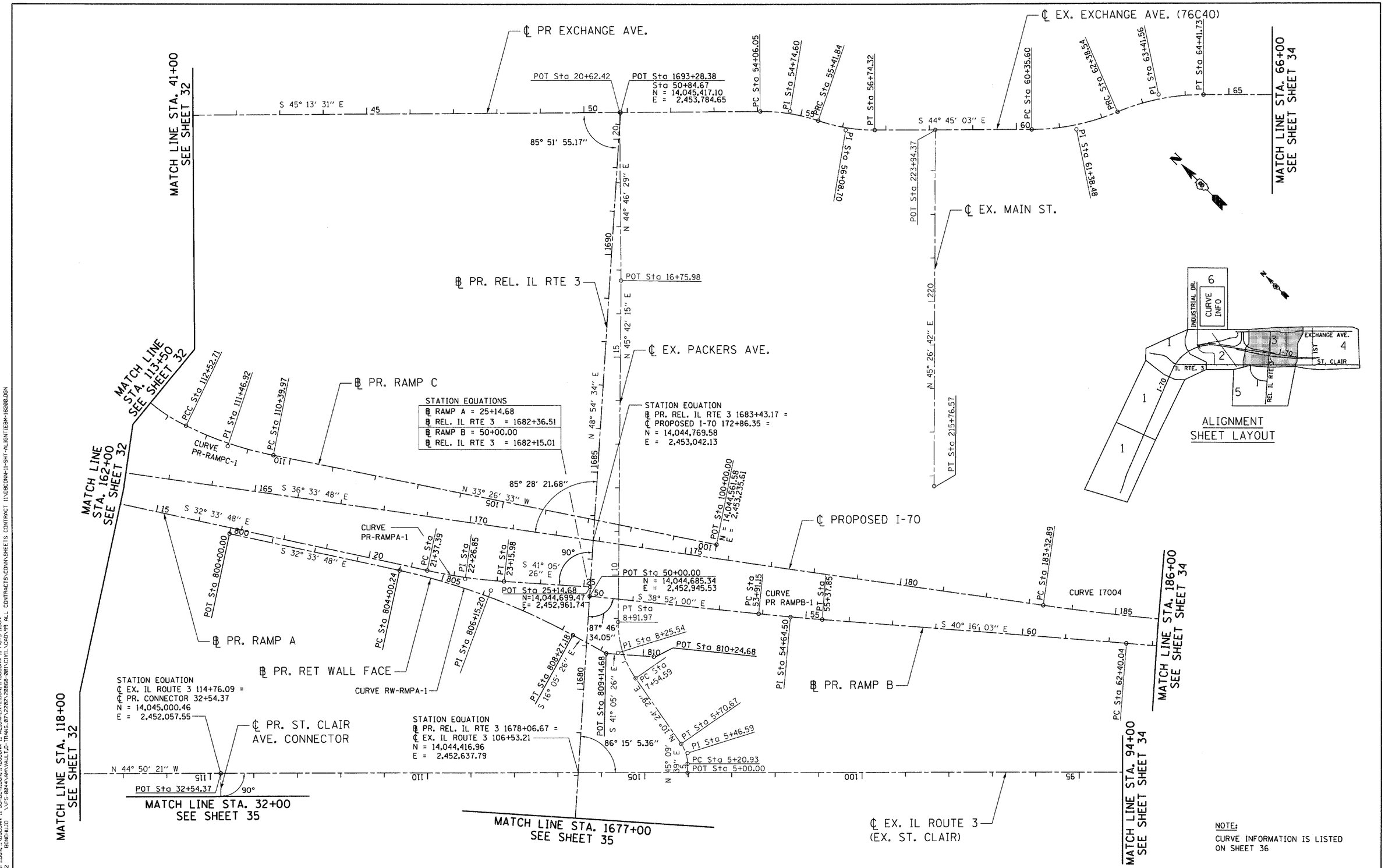
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	32
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE				CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=100' SHEET NO. 2 OF 8 SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



NOTE:
CURVE INFORMATION IS LISTED ON SHEET 36

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	ALIGNMENTS, TIES & BENCHMARKS 3	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	33
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -			CONTRACT NO. 76E06				
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=100' / SHEET NO. 3 OF 8 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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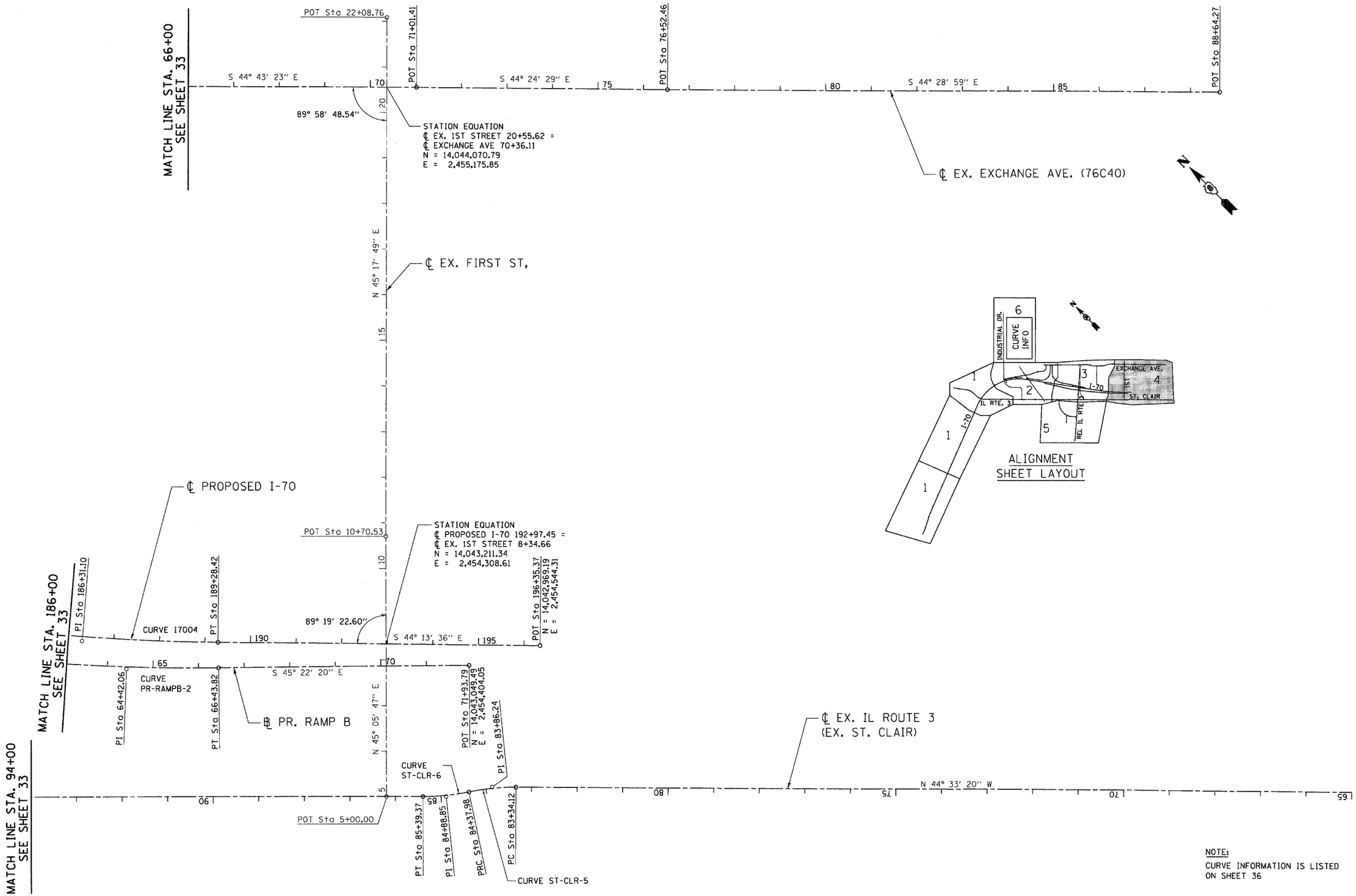

NOTE:
CURVE INFORMATION IS LISTED ON SHEET 36

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	ALIGNMENTS, TIES & BENCHMARKS 4			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - JB	REVISED -					998	82-2-1K	ST. CLAIR	353 / 34
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -							CONTRACT NO. 76E06	
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=100'	SHEET NO. 4 OF 8 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

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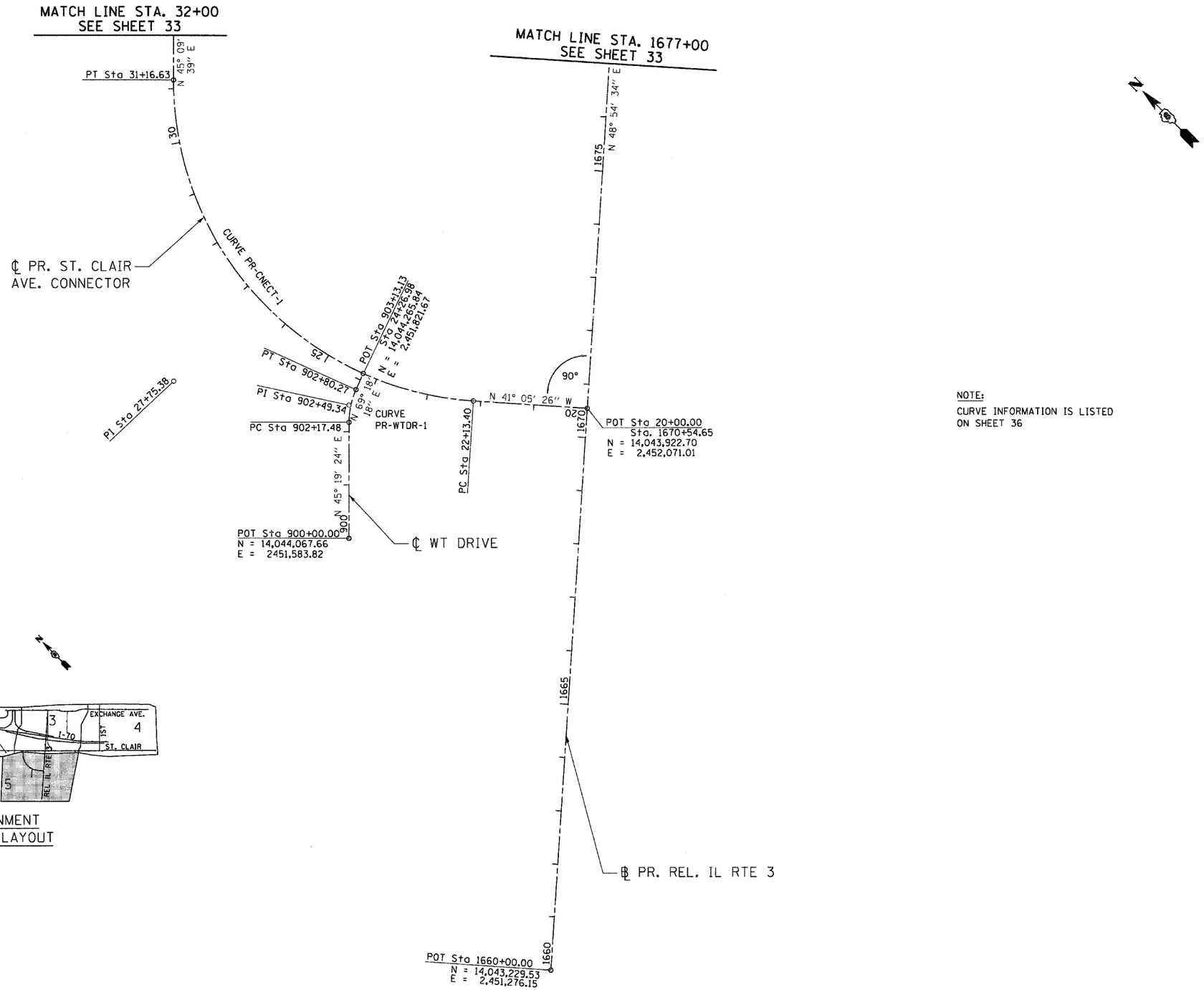
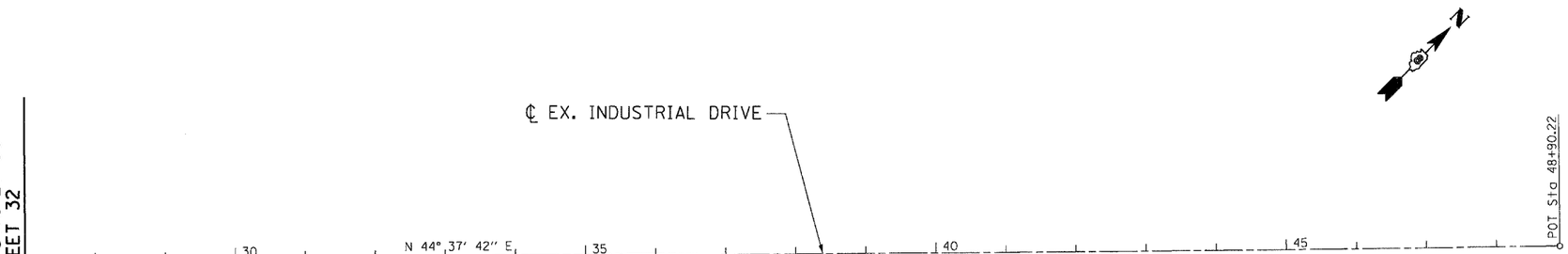


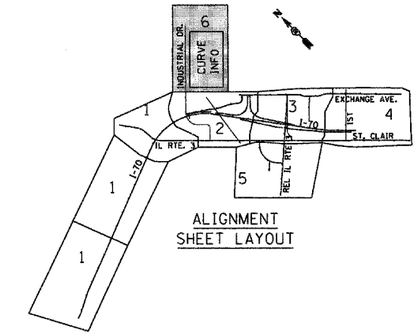
NOTE:
CURVE INFORMATION IS LISTED ON SHEET 36

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENTS, TIES & BENCHMARKS 5	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE		998	82-2-1K	ST. CLAIR	353	35
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=100' / SHEET NO. 5 OF 8 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\\DBCONN-99-MOTIF1.DGN,\\DBCONN-11-BORDER1.DGN,\\DBCONN-11-ALIGNMENT2.DGN,\\DBCONN-11-MOTIF1.DGN
10-19-2011,14:29:24 BONDHJJO \\VS-0044\VAULT\D-TRANS.07\22B2\2B8SB-08\CIVIL\CADV59 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN

MATCH LINE STA. 27+00
SEE SHEET 32





PROPOSED I-70

PROP. CURVE I7001
PI STA. = 64+29.43
N = 14,045,143.80
E = 2,443,857.63
Δ = 7° 48′ 56″ (LT)
D = 3° 01′ 26″
R = 1,894.74′
T = 129.43′
L = 258.46′
E = 4.42′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 63+00.00
N = 14,045,100.50
E = 2,443,735.66
P.T. STA. = 65+58.46
N = 14,045,203.30
E = 2,443,972.57

PROP. CURVE I7002-1
PI STA. = 71+40.36
N = 14,045,470.77
E = 2,444,489.36
Δ = 6° 07′ 13″ (RT)
D = 3° 01′ 38″
R = 1,892.60′
T = 101.18′
L = 202.16′
E = 2.70′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 70+39.19
N = 14,045,424.27
E = 2,444,399.51
P.T. STA. = 72+41.35
N = 14,045,507.43
E = 2,444,583.66

PROP. CURVE I7003
PI STA. = 138+29.72
N = 14,047,894.72
E = 2,450,724.31
Δ = 74° 40′ 52″ (RT)
D = 2° 56′ 04″
R = 1,952.50′
T = 1,489.60′
L = 2,544.95′
E = 503.34′
e = 5.40%
T.R. = 107′
S.E. RUN = 360′
P.C. STA. = 123+40.13
N = 14,047,354.97
E = 2,449,335.94
P.T. STA. = 148+85.08
N = 14,046,698.27
E = 2,451,611.67

PROP. CURVE I7004
PI STA. = 186+31.10
N = 14,043,689.47
E = 2,453,843.21
Δ = 7° 39′ 48″ (LT)
D = 1° 17′ 13″
R = 4,452.50′
T = 298.21′
L = 595.53′
E = 9.98′
e = 3.30%
T.R. = 75′
S.E. RUN = 180′
P.C. STA. = 183+32.89
N = 14,043,929.00
E = 2,453,665.57
P.T. STA. = 189+28.42
N = 14,043,475.78
E = 2,454,051.21

RAMP A

PROP. CURVE PR_RAMPA-1
PI STA. = 22+26.85
N = 14,044,916.65
E = 2,452,772.34
Δ = 8° 31′ 38″ (LT)
D = 4° 46′ 29″
R = 1,200.00′
T = 89.46′
L = 178.60′
E = 3.33′
e = R.C.
T.R. = 83.33′
S.E. RUN = 66.67′
P.C. STA = 21+37.39
N = 14,044,992.05
E = 2,452,724.19
P.T. STA = 23+15.98
N = 14,044,849.22
E = 2,452,831.14

RAMP B

PROP. CURVE PR_RAMPB-1
PI STA. = 54+64.50
N = 14,044,323.67
E = 2,453,237.02
Δ = 1° 24′ 03″ (LT)
D = 0° 57′ 18″
R = 6,000.00′
T = 73.35′
L = 146.70′
E = 0.45′
e = N.C.
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 53+91.15
N = 14,044,380.79
E = 2,453,190.99
P.T. STA = 55+37.85
N = 14,044,267.70
E = 2,453,284.43

PROP. CURVE PR_RAMPB-2
PI STA. = 64+42.06
N = 14,043,577.76
E = 2,453,868.87
Δ = 5° 06′ 17″ (LT)
D = 1° 15′ 51″
R = 4,532.00′
T = 202.02′
L = 403.77′
E = 4.50′
e = 3.3%
T.R. = 0′
S.E. RUN = 126.43′
P.C. STA = 62+40.04
N = 14,043,731.91
E = 2,453,738.30
P.T. STA = 66+43.82
N = 14,043,435.84
E = 2,454,012.65

RAMP C

PROP. CURVE PR_RAMPC-1
PI STA. = 111+46.92
N = 14,045,518.62
E = 2,452,603.55
Δ = 14° 35′ 50″ (RT)
D = 6° 51′ 42″
R = 835.00′
T = 106.95′
L = 212.73′
E = 6.82′
e = 6.0%
T.R. = 0′
S.E. RUN = 150′
P.C. STA = 110+39.97
N = 14,045,429.38
E = 2,452,662.48
P.T. STA = 112+52.71
N = 14,045,619.83
E = 2,452,569.00

PROP. CURVE PR_RAMPC-2
PI STA. = 115+07.02
N = 14,045,860.51
E = 2,452,486.86
Δ = 63° 37′ 12″ (RT)
D = 13° 58′ 28″
R = 410.00′
T = 254.31′
L = 455.25′
E = 72.47′
e = 6.0%
T.R. = 0′
S.E. RUN = 100′
P.C. STA = 112+52.71
N = 14,045,619.83
E = 2,452,569.00
P.T. STA = 117+07.96
N = 14,046,041.04
E = 2,452,665.97

RAMP D

PROP. CURVE PR_RAMPD-1
PI STA. = 257+14.62
N = 14,045,842.03
E = 2,452,341.74
Δ = 97° 30′ 58″ (RT)
D = 15° 04′ 40″
R = 380.00′
T = 433.43′
L = 646.75′
E = 196.42′
e = 6.0%
T.R. = 0′
S.E. RUN = 150′
P.C. STA = 252+81.19
N = 14,046,149.72
E = 2,452,647.02
P.T. STA = 259+27.94
N = 14,046,184.93
E = 2,452,076.63

RET WALL RAMP A

PROP. CURVE RW_RMPA-1
PI STA. = 806+15.20
N = 14,044,855.95
E = 2,452,794.99
Δ = 16° 28′ 22″ (RT)
D = 3° 51′ 30″
R = 1,485.00′
T = 214.95′
L = 426.94′
E = 15.48′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 804+00.24
N = 14,045,037.12
E = 2,452,679.30
P.T. STA = 808+27.18
N = 14,044,649.42
E = 2,452,854.57

PR. ST. CLAIR AVE. CONNECTOR

PROP. CURVE PR_CNECT-1
PI STA. = 27+75.38
N = 14,044,507.08
E = 2,451,561.39
Δ = 86° 15′ 05″ (RT)
D = 9° 32′ 57″
R = 600.00′
T = 561.98′
L = 903.22′
E = 222.08′
e = 3.8%
T.R. = 30′
S.E. RUN = 75′
P.C. STA = 22+13.40
N = 14,044,083.53
E = 2,451,930.75
P.T. STA = 31+16.63
N = 14,044,903.34
E = 2,451,959.88

WT DRIVE

PROP. CURVE PR_WTDR-1
PI STA. = 902+49.34
N = 14,044,242.97
E = 2,451,761.12
Δ = 23° 58′ 54″ (RT)
D = 38° 11′ 50″
R = 150.00′
T = 31.86′
L = 62.78′
E = 3.35′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 902+17.48
N = 14,044,220.57
E = 2,451,738.47
P.T. STA = 902+80.27
N = 14,044,254.23
E = 2,451,790.92

PACKERS AVE.

EXIST. CURVE EX_PACK-1
PI STA. = 5+46.59
N = 14,044,271.33
E = 2,452,848.31
Δ = 34° 45′ 10″ (LT)
D = 69° 52′ 22″
R = 82.00′
T = 25.66′
L = 49.74′
E = 3.92′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 5+20.93
N = 14,044,253.23
E = 2,452,830.12
P.T. STA. = 5+70.67
N = 14,044,296.57
E = 2,452,852.95

EXIST. CURVE EX_PACK-2
PI STA. = 8+25.54
N = 14,044,547.24
E = 2,452,898.99
Δ = 35° 17′ 45″ (RT)
D = 25° 41′ 35″
R = 223.00′
T = 70.95′
L = 137.37′
E = 11.01′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 7+54.59
N = 14,044,477.46
E = 2,452,886.18
P.T. STA. = 8+91.97
N = 14,044,596.78
E = 2,452,949.77

EXCHANGE AVE.

PROP. CURVE PR_EXCH-1
PI STA. = 54+74.60
N = 14,045,142.47
E = 2,454,061.46
Δ = 19° 27′ 04″ (RT)
D = 14° 19′ 26″
R = 400.00′
T = 68.56′
L = 135.80′
E = 5.83′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 54+06.05
N = 14,045,190.75
E = 2,454,012.79
P.T. STA. = 55+41.84
N = 14,045,080.73
E = 2,454,091.27

PROP. CURVE PR_EXCH-2
PI STA. = 56+08.70
N = 14,045,020.53
E = 2,454,120.34
Δ = 18° 58′ 37″ (LT)
D = 14° 19′ 26″
R = 400.00′
T = 66.85′
L = 132.48′
E = 5.55′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 55+41.84
N = 14,045,080.73
E = 2,454,091.27
P.T. STA. = 56+74.32
N = 14,044,973.05
E = 2,454,167.40

PROP. CURVE PR_EXCH-3
PI STA. = 61+38.48
N = 14,044,643.41
E = 2,454,494.18
Δ = 23° 15′ 21″ (LT)
D = 11° 27′ 33″
R = 500.00′
T = 102.89′
L = 202.95′
E = 10.48′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 60+35.60
N = 14,044,716.48
E = 2,454,421.75
P.T. STA. = 62+38.54
N = 14,044,604.88
E = 2,454,589.58

PROP. CURVE PR_EXCH-4
PI STA. = 63+41.56
N = 14,044,566.30
E = 2,454,685.10
Δ = 23° 17′ 01″ (RT)
D = 11° 27′ 33″
R = 500.00′
T = 103.02′
L = 203.19′
E = 10.50′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 62+38.54
N = 14,044,604.88
E = 2,454,589.58
P.T. STA. = 64+41.73
N = 14,044,493.11
E = 2,454,757.59

INDUSTRIAL DRIVE

EXIST. CURVE INDRD01
PI STA. = 10+36.83
N = 14,046,541.81
E = 2,451,281.88
Δ = 89° 43′ 20″ (LT)
D = 54° 34′ 03″
R = 105.00′
T = 104.49′
L = 164.42′
E = 43.13′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 9+32.33
N = 14,046,467.57
E = 2,451,208.35
P.T. STA. = 10+96.76
N = 14,046,615.70
E = 2,451,208.00

EXIST. CURVE INDRD02
PI STA. = 17+68.75
N = 14,047,090.90
E = 2,450,732.86
Δ = 87° 47′ 02″ (RT)
D = 15° 03′ 19″
R = 380.57′
T = 366.13′
L = 583.08′
E = 147.52′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 14+02.62
N = 14,046,831.99
E = 2,450,991.73
P.T. STA. = 19+85.70
N = 14,047,359.59
E = 2,450,981.56

EX. IL RTE 3 (ST. CLAIR AVE.)

PROP. CURVE ST_CLR-5
PI STA. = 83+86.24
N = 14,042,825.09
E = 2,454,249.99
Δ = 11° 54′ 06″ (LT)
D = 11° 27′ 33″
R = 500.00′
T = 52.12′
L = 103.86′
E = 2.71′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 83+34.12
N = 14,042,787.95
E = 2,454,286.56
P.T. STA. = 84+37.98
N = 14,042,853.89
E = 2,454,206.55

PROP. CURVE ST_CLR-6
PI STA. = 84+88.85
N = 14,042,881.99
E = 2,454,164.16
Δ = 11° 37′ 06″ (RT)
D = 11° 27′ 33″
R = 500.00′
T = 50.87′
L = 101.39′
E = 2.58′
e = NC
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 84+37.98
N = 14,042,853.89
E = 2,454,206.55
P.T. STA. = 85+39.37
N = 14,042,918.06
E = 2,454,128.29

PROP. CURVE ST_CLR-7
PI STA. = 149+38.21
N = 14,047,455.42
E = 2,449,616.35
Δ = 41° 34′ 53″ (RT)
D = 12° 19′ 18″
R = 465.00′
T = 176.55′
L = 337.46′
E = 32.39′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 147+61.66
N = 14,047,330.23
E = 2,449,740.84
P.T. STA. = 150+99.13
N = 14,047,631.69
E = 2,449,606.32

PROP. CURVE ST_CLR-8
PI STA. = 155+22.88
N = 14,048,054.76
E = 2,449,582.24
Δ = 32° 24′ 12″ (LT)
D = 11° 27′ 33″
R = 500.00′
T = 145.28′
L = 282.77′
E = 20.68′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA. = 153+77.60
N = 14,047,909.71
E = 2,449,590.49
P.T. STA. = 156+60.38
N = 14,048,172.79
E = 2,449,497.54

DARLING ACCESS ROAD

PROP. CURVE PR_DRLG-1
PI STA. = 1002+02.53
N = 14,047,245.24
E = 2,451,142.76
Δ = 11° 06′ 01″ (RT)
D = 2° 44′ 57″
R = 2,084.17′
T = 202.53′
L = 403.78′
E = 9.82′
e = N.C.
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 1000+00.00
N = 14,047,336.94
E = 2,450,962.18
P.T. STA = 1004+03.78
N = 14,047,120.49
E = 2,451,302.30

PROP. CURVE PR_DRLG-2
PI STA. = 1004+57.84
N = 14,047,087.19
E = 2,451,344.89
Δ = 6° 11′ 18″ (LT)
D = 5° 43′ 46″
R = 1,000.00′
T = 54.06′
L = 108.01′
E = 1.46′
e = N.C.
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 1004+03.78
N = 14,047,120.49
E = 2,451,302.30
P.T. STA = 1005+11.79
N = 14,047,058.68
E = 2,451,390.81

PROP. CURVE PR_DRLG-3
PI STA. = 1007+65.46
N = 14,046,924.88
E = 2,451,606.32
Δ = 8° 23′ 05″ (RT)
D = 3° 49′ 11″
R = 1,500.00′
T = 109.95′
L = 219.51′
E = 4.02′
e = N.C.
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 1006+55.51
N = 14,046,982.88
E = 2,451,512.91
P.T. STA = 1008+75.02
N = 14,046,853.88
E = 2,451,690.28

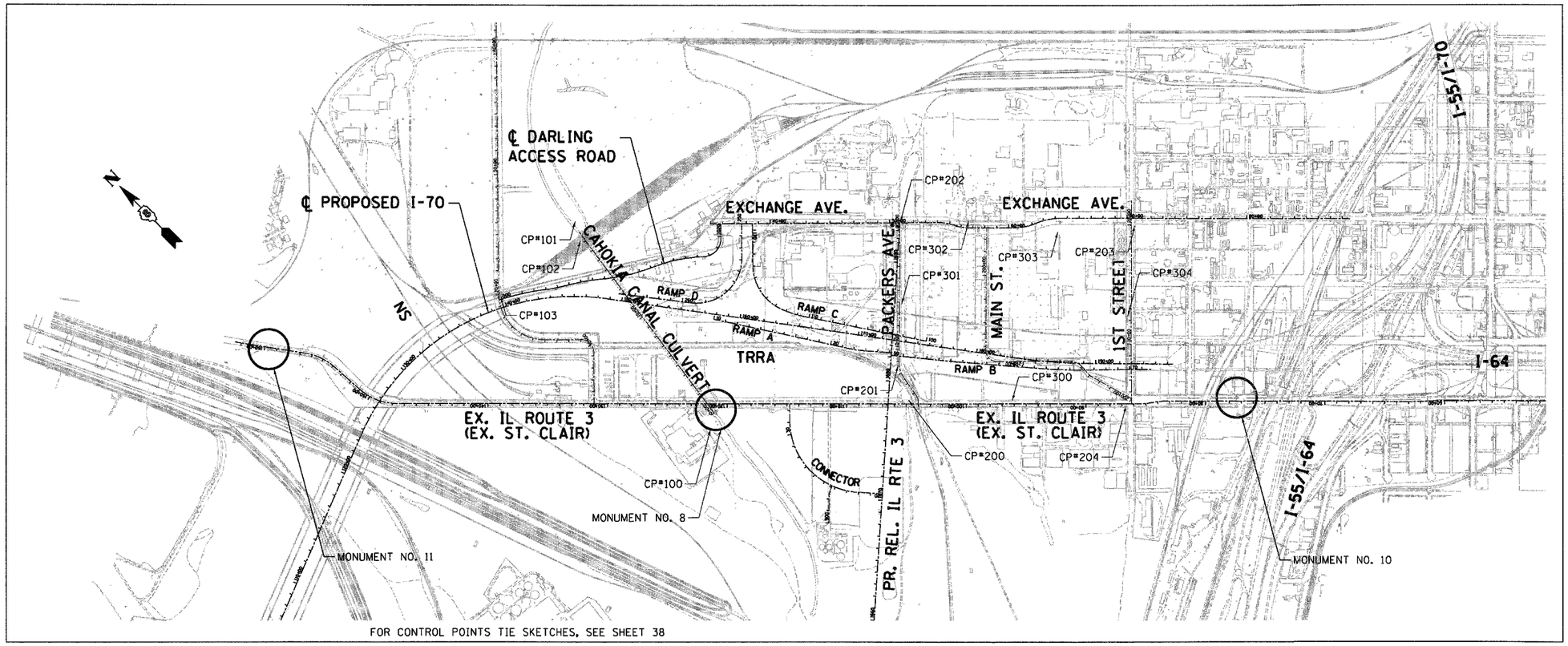
PROP. CURVE PR_DRLG-4
PI STA. = 1010+69.65
N = 14,046,728.21
E = 2,451,838.90
Δ = 14° 47′ 10″ (LT)
D = 3° 49′ 11″
R = 1,500.00′
T = 194.63′
L = 387.10′
E = 12.57′
e = N.C.
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 1008+75.02
N = 14,046,853.88
E = 2,451,690.28
P.T. STA = 1012+62.12
N = 14,046,644.63
E = 2,452,014.67

PROP. CURVE PR_DRLG-5
PI STA. = 1015+35.35
N = 14,046,527.29
E = 2,452,261.42
Δ = 15° 44′ 44″ (RT)
D = 4° 35′ 01″
R = 1,250.00′
T = 172.85′
L = 343.51′
E = 11.89′
e = N.C.
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 1013+62.50
N = 14,046,601.52
E = 2,452,105.32
P.T. STA = 1017+06.01
N = 14,046,413.49
E = 2,452,391.52

PROP. CURVE PR_DRLG-6
PI STA. = 1018+99.00
N = 14,046,286.43
E = 2,452,536.77
Δ = 86° 24′ 10″ (LT)
D = 38° 11′ 50″
R = 150.00′
T = 140.87′
L = 226.20′
E = 55.77′
e = 2.60%
T.R. = 32′
S.E. RUN = 19′
P.C. STA = 1017+58.14
N = 14,046,379.17
E = 2,452,430.75
P.T. STA = 1019+84.34
N = 14,046,386.43
E = 2,452,635.99

DARLING RET WALL

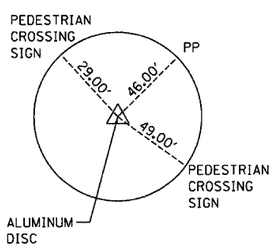
PROP. CURVE RW_DRLG-1
PI STA. = 444+33.90
N = 14,047,100.74
E = 2,451,359.12
Δ = 21° 08′ 30″ (RT)
D = 2° 46′ 14″
R = 2,068.00′
T = 385.92′
L = 763.07′
E = 35.70′
e = N/A
T.R. = N/A
S.E. RUN = N/A
P.C. STA = 440+47.97
N = 14,047,287.85
E = 2,451,021.59
P.T. STA = 448+11.04
N = 14,046,804.48
E = 2,451,606.45



FOR CONTROL POINTS TIE SKETCHES, SEE SHEET 38

MONUMENT NO. 11

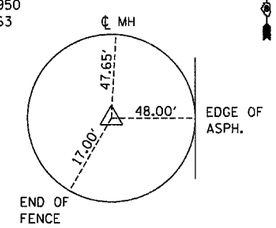
MRB (U.S. FOOT)
(PROJECT COORDINATE)
N = 14,048,411.5720
E = 2,449,306.9042
ELEV. = 414.02



DESCRIPTION:
SET IN THE BACK OF A 4.5' CONCRETE WALK (END OF WALK), LOCATED ON THE WEST SIDE OF ILLINOIS ROUTE 3 APPROXIMATELY 0.3 MILES SOUTH OF CANAL STREET. (BROOKLYN)

MONUMENT NO. 8

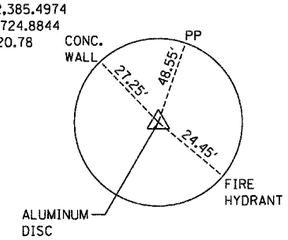
MRB (U.S. FOOT)
(PROJECT COORDINATE)
N = 14,045,393.2950
E = 2,451,580.4763
ELEV. = 401.95



DESCRIPTION:
SET IN THE SOUTH END OF A HEADWALL TO A BOX CULVERT UNDER ILLINOIS ROUTE 3, 0.7 MILES SOUTH OF CANAL STREET, 0.1 MILES SOUTH OF INDUSTRIAL DRIVE AND NORTH OF SINGLE RAILROAD TRACK.

MONUMENT NO. 10

MRB (U.S. FOOT)
(PROJECT COORDINATE)
N = 14,042,385.4974
E = 2,454,724.8844
ELEV. = 420.78



DESCRIPTION:
SET IN THE CONCRETE MEDIAN ON THE EAST SIDE OF ROUTE 3 (ST. CLAIR AVE.) BETWEEN TWO RAILROAD BRIDGES. APPROXIMATELY 0.1 MILE SOUTH OF 1ST STREET (IN FRONT OF 301 ST. CLAIR AVE. WAREHOUSE & STORE FIXTURE CO.)

COORDINATE SYSTEM

THE MISSISSIPPI RIVER BRIDGE (MRB) PROJECT COORDINATE SYSTEM IS BASED ON A MODIFIED UNIVERSAL TRANSVERSE MERCATOR (UTM) SYSTEM. THE MRB PROJECT COORDINATE SYSTEM HAS CONVERTED FROM UTM ZONE 15 NORTH BY AN AVERAGE PROJECTION FACTOR AND ALSO CONVERTED FROM METERS TO U.S. SURVEY FEET.

HORIZONTAL DATUM

THE DATUM USED IS NAD-83

THE MRB PROJECT COORDINATES HAVE BEEN TRANSFORMED FROM UTM BY USING AN AVERAGE PROJECTION FACTOR IN THE PROJECT AREA.

AVERAGE GRID FACTOR = 1.000339495
PROJECTION FACTOR = 1/GRID = 0.999660620

THE BASE POINT THAT ALL UTM COORDINATES WERE SCALED FROM WAS THE CENTRALLY LOCATED MONUMENT NO. 10. EACH VECTOR FROM MONUMENT NO. 10 TO ALL OTHER MONUMENTS WAS MULTIPLIED BY THE PROJECTION FACTOR TO CALCULATE A SURFACE VECTOR AND THEN THIS SURFACE VECTOR WAS USED TO CALCULATE THE SURFACE COORDINATE (MRB). (NOTE: 1 METER EQUALS 3.28083333 U.S. SURVEY FOOT).

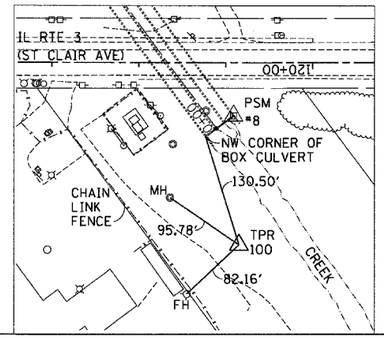
UTM ZONE 15 NORTH (METERS) = PROJECTED GRID COORDINATES
MRB (FEET) = PROJECT SURFACE CORRDINATES

VERTICAL DATUM

THE DATUM USED IS NAVD 1988

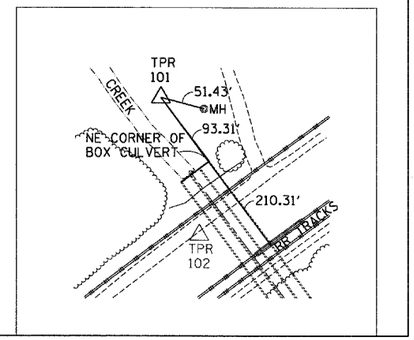
CONTROL POINT #100 (IRON ROD W/CAP)

N =14,045,283.692
E =2,451,478.950
ELEV. = 409.230



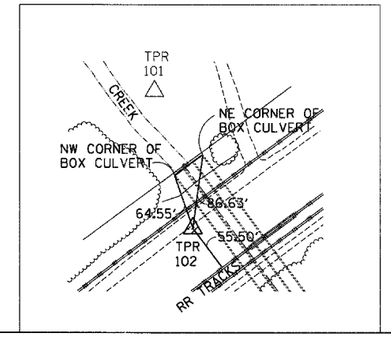
CONTROL POINT #101 (IRON ROD W/CAP)

N = 14,047,349.416
E = 2,451,873.863
ELEV. = 401.580



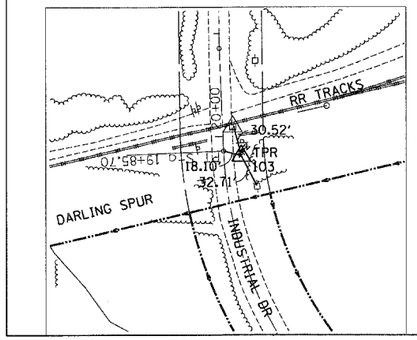
CONTROL POINT #102 (IRON ROD W/CAP)

N = 14,047,204.910
E = 2,451,791.600
ELEV. = 414.750



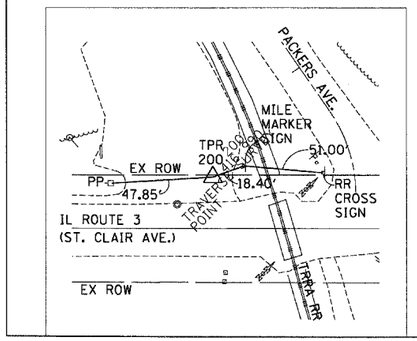
CONTROL POINT #103 (IRON ROD W/CAP)

N = 14,047,344.143
E = 2,450,990.211
ELEV. = 414.820



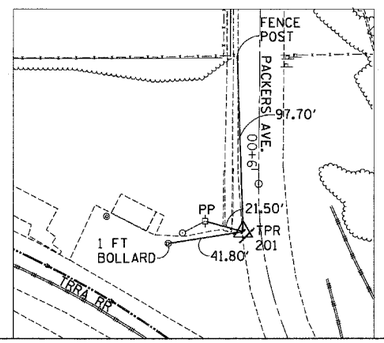
CONTROL POINT #200 (IRON ROD W/CAP)

N = 14,044,316.431
E = 2,452,780.630
ELEV. = 416.990



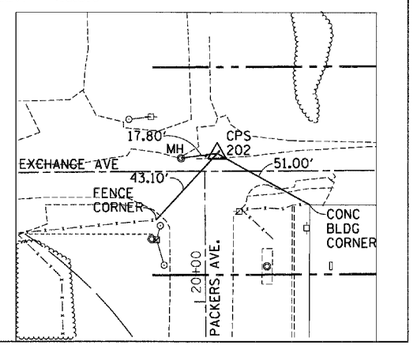
CONTROL POINT #201 (PK NAIL)

N = 14,044,584.131
E = 2,452,924.827
ELEV. = 417.190



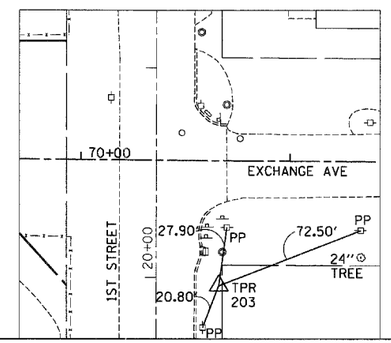
CONTROL POINT #202 (COTTON PICKER SPINDLE)

N = 14,045,420.418
E = 2,453,793.293
ELEV. = 416.310



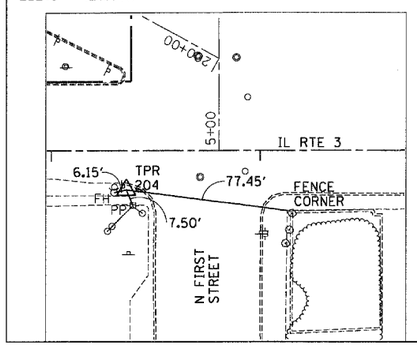
CONTROL POINT #203 (IRON ROD W/CAP)

N = 14,044,007.578
E = 2,455,154.610
ELEV. = 416.500



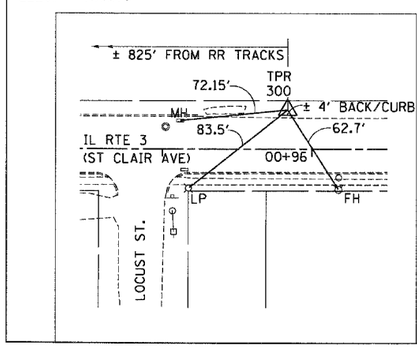
CONTROL POINT #204 (IRON ROD W/CAP)

N = 14,042,993.287
E = 2,454,026.962
ELEV. = 420.970



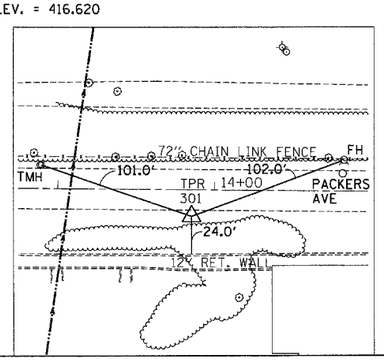
CONTROL POINT #300 (IRON ROD W/CAP)

N = 14,043,700.040
E = 2,453,387.437
ELEV. = 418.970



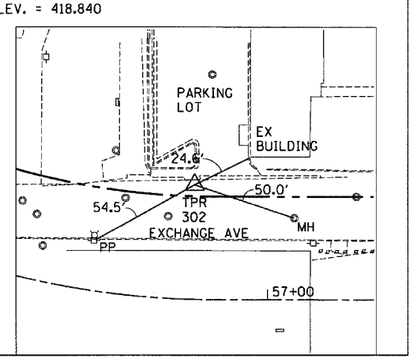
CONTROL POINT #301 (IRON ROD W/CAP)

N = 14,044,929.344
E = 2,453,314.775
ELEV. = 416.620



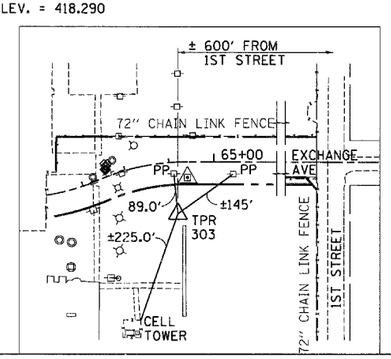
CONTROL POINT #302 (CUT CROSS)

N = 14,045,018.363
E = 2,454,200.089
ELEV. = 418.840



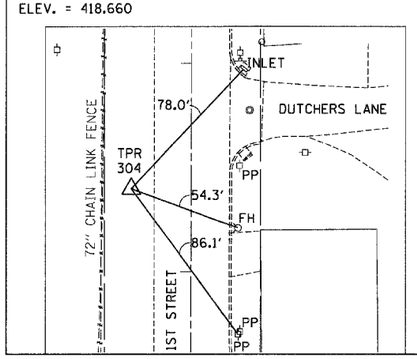
CONTROL POINT #303 (IRON ROD W/CAP)

N = 14,044,427.212
E = 2,454,660.603
ELEV. = 418.290



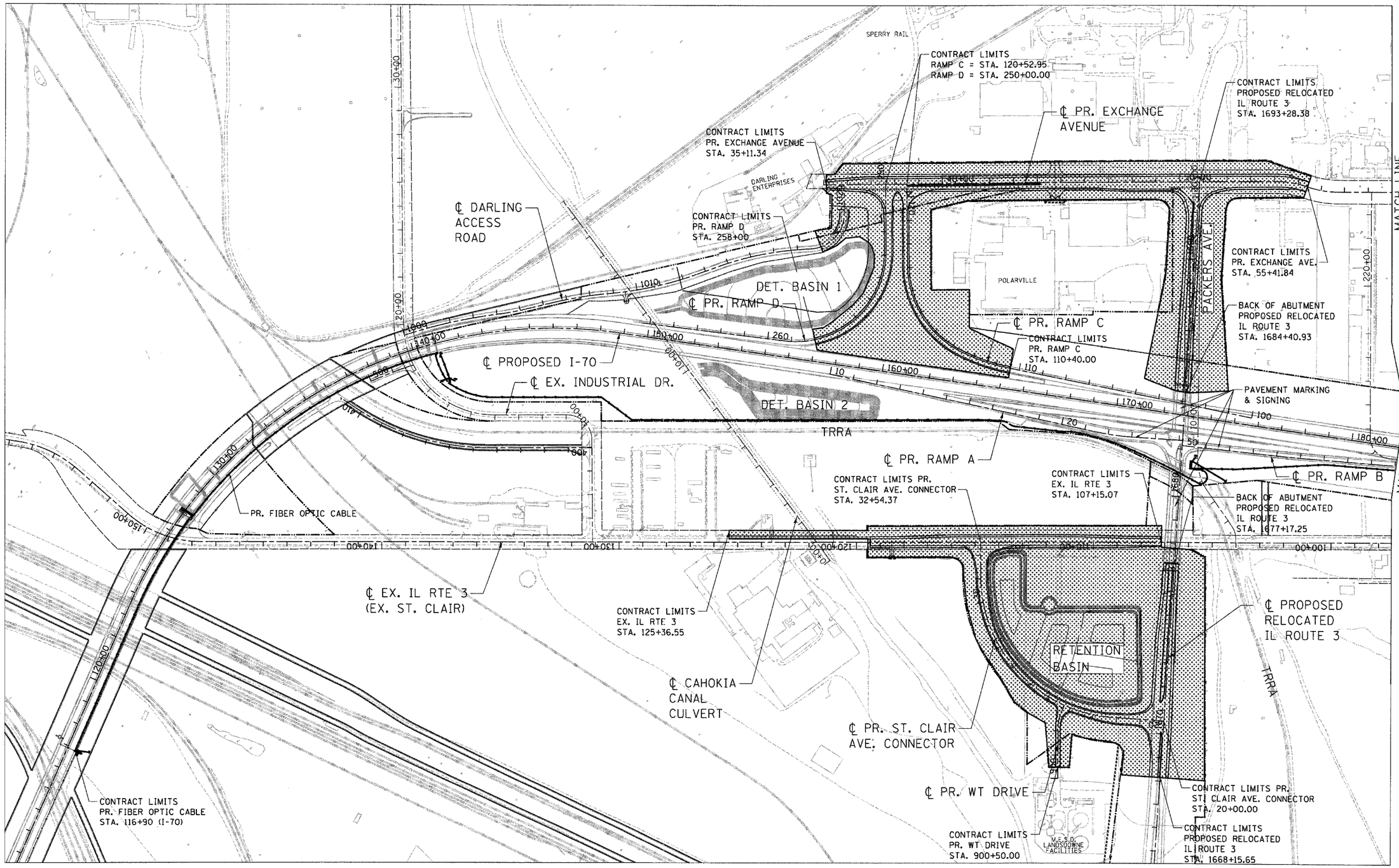
CONTROL POINT #304 (IRON ROD W/CAP)

N = 14,043,517.419
E = 2,454,575.981
ELEV. = 418.660



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	ALIGNMENTS, TIES & BENCHMARKS 8	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353 / 38
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06	
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=200' / SHEET NO. 8 OF 8 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

..\DBCONN-99-NOTIFILDGN ..\DBCONN-11-BERGERILDGN ..\DBCONN-11-LAYOUTILDGN ..\DBCONN-99-ALIGNMENT2.DGN ..\DBCONN-03-GEOMETRICS1.DGN ..\DBCONN-03-TOPOILDGN
10-19-2011,14:30:03 BONGINJJO \\FS-0044\WKVHUL\D-THANS_07\2262\206159-001\CIVIL\CADD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-ALIGNTIEBMB.DGN



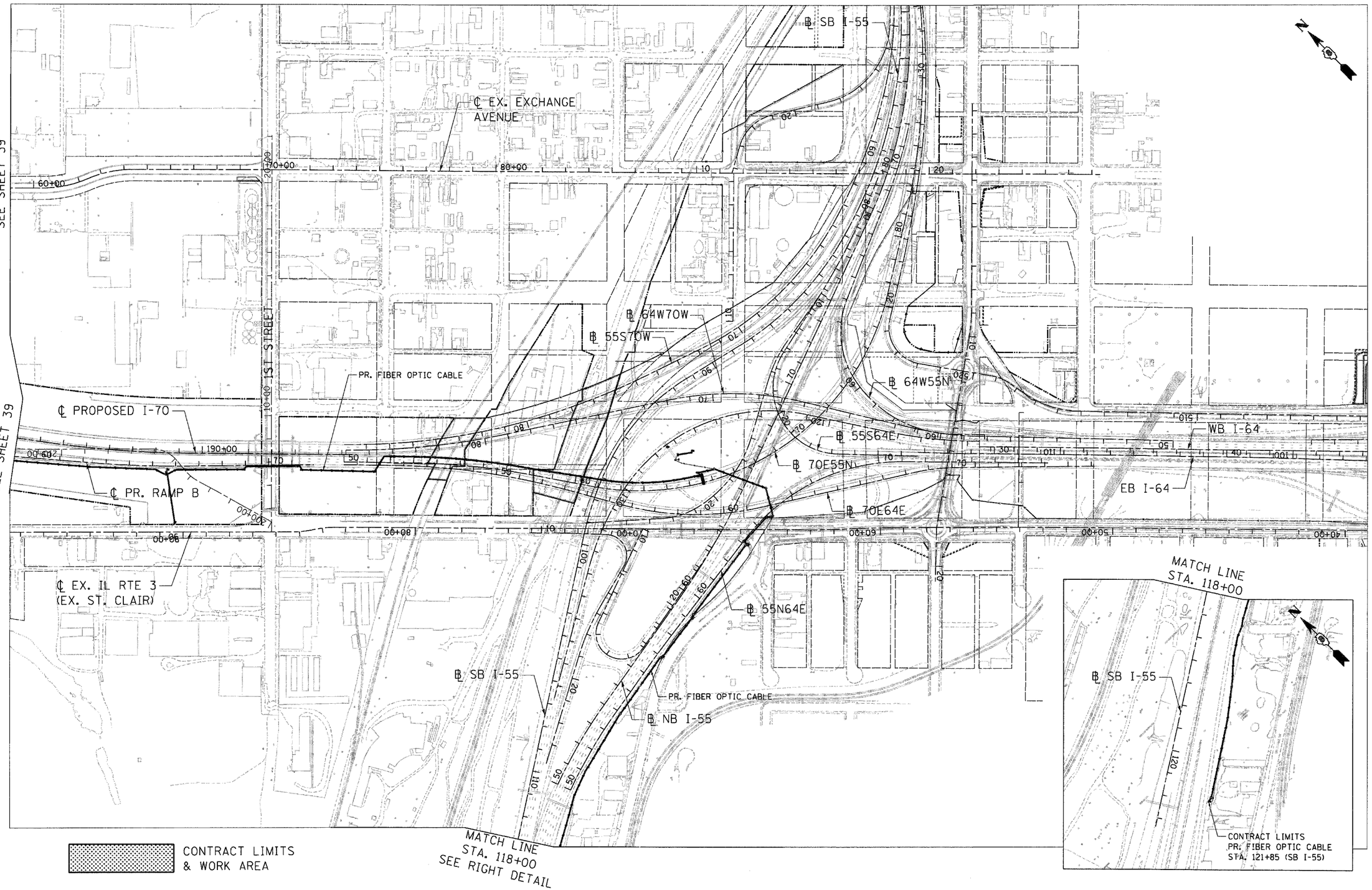
FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

CONSTRUCTION LIMITS 1

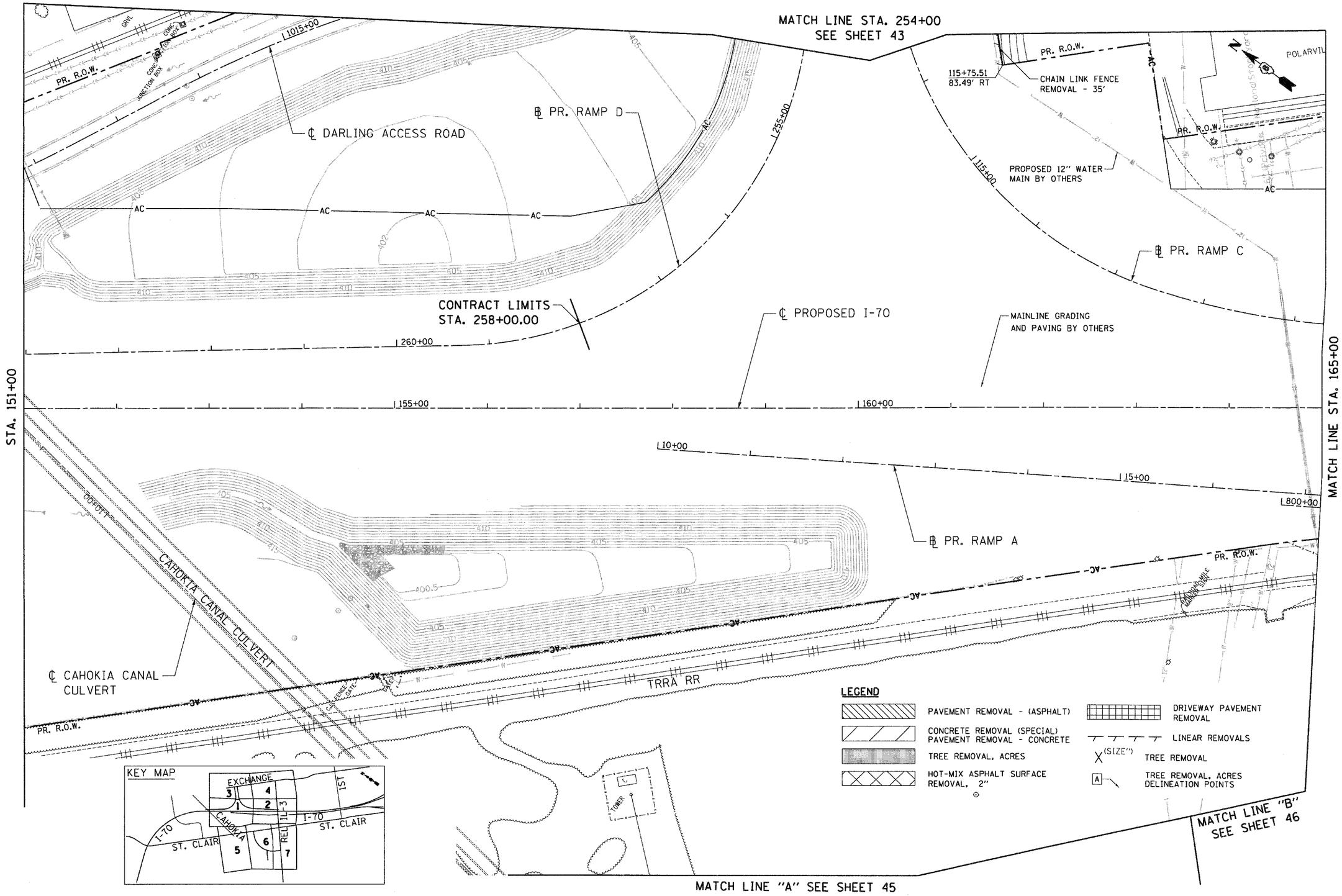
SCALE: 1"=200' | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	39
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	**STATE OF ILLINOIS**	**CONSTRUCTION LIMITS 2**	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - JB	REVISED -	**DEPARTMENT OF TRANSPORTATION**		998	82-2-1K	ST. CLAIR	353 / 40
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE				CONTRACT NO. 76E06	
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=200' / SHEET NO. 2 OF 2 SHEETS / STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

..\DBCONN-99-MOTIF1.DGN,..\DBCONN-11-BERGER1.DGN,..\DBCONN-11-ALIGNMENT3.DGN,..\DBCONN-99-ALIGNMENT2.DGN,..\DBCONN-10-MOTIF1.DGN,..\DBCONN-11-MOTIF1.DGN,..\DBCONN-05-MOTIF1.DGN,..\DBCONN-05-MOTIF1.DGN,..\DBCONN-11-SH1-ALIGNTIEBM9.DGN,..\DBCONN-11-ALIGNMENTS.DGN,..\DBCONN-11-MOTIF1.DGN,..\DBCONN-11-ALIGNME
10-19-2011 14:31:26 SONDHLUD \\FS-0044\WK\PAL_TD-TRANS.07\2202\20658-001\CIVIL\CHU\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SH1-ALIGNTIEBM9-2.DGN



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

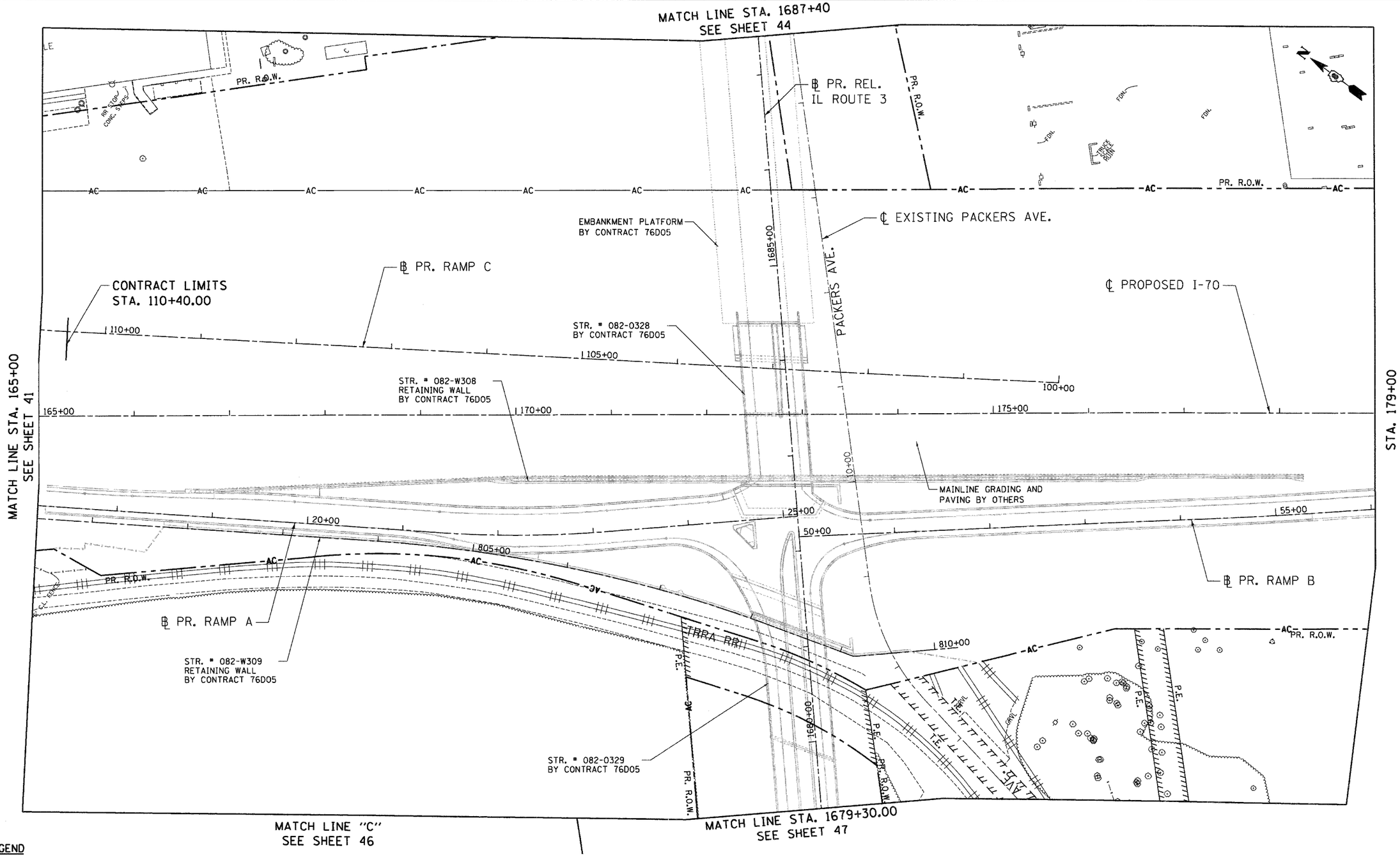
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED I-70
EXISTING PLAN & REMOVALS
STA. 151+00 TO STA. 165+00

SCALE: 1"=50' | SHEET NO. 1 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	41
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF.DGN,..\DBCONN-11-BORDER.DGN,..\DBCONN-11-MOTIF\DGN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-LAYOUT.DGN,..\DBCONN-05-MOTIF.DGN,..\DBCONN-11-SHT-REMOVAL-151-165.DGN
10-19-2011,14:31:57 BONDALJD \\VS-0044\WAVALI\D-TRANS.87\22ZX\20868-001\CIVIL\CADD\99 ALL CONTRACT SHEETS\CONTRACT 11\DBCONN-11-SHT-REMOVAL-151-165.DGN



LEGEND

- PAVEMENT REMOVAL - (ASPHALT)
- CONCRETE REMOVAL (SPECIAL) PAVEMENT REMOVAL - CONCRETE
- TREE REMOVAL, ACRES
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- DRIVEWAY PAVEMENT REMOVAL
- LINEAR REMOVALS
- X (SIZE") TREE REMOVAL
- A TREE REMOVAL, ACRES DELINEATION POINTS

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

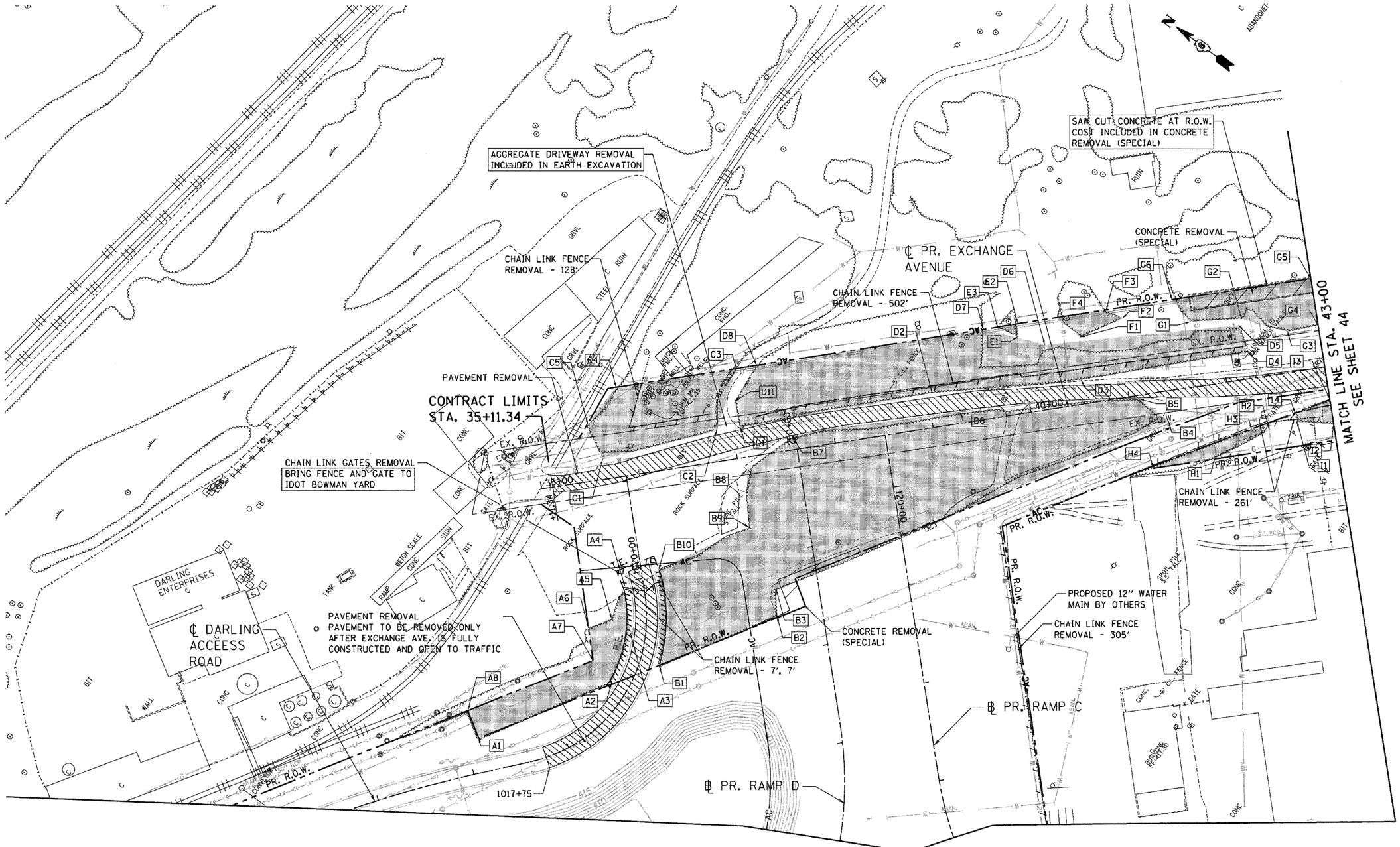
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED I-70
EXISTING PLAN & REMOVALS
STA. 165+00 TO STA. 179+00

SCALE: 1"=50' | SHEET NO. 2 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	42
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN, ..\DBCONN-11-BORDER1.DGN, ..\DBCONN-11-MOTIF1.DGN, ..\DBCONN-11-LEGEND1.DGN, ..\DBCONN-11-LAYOUT1.DGN, ..\DBCONN-05-MOTIF1.DGN, ..\DBCONN-06-MOTIF1.DGN, ..\DBCONN-08-MOTIF1.DGN, ..\DBCONN-10-MOTIF1.DGN
10-19-2011, 14:32:25 BONDHUJO \\FS-0044\AM\VAULT_D-TRANS_07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-REMOVAL1-16500.DGN



LEGEND

PAVEMENT REMOVAL - (ASPHALT)	DRIVEWAY PAVEMENT REMOVAL
CONCRETE REMOVAL (SPECIAL) PAVEMENT REMOVAL - CONCRETE	LINEAR REMOVALS
TREE REMOVAL, ACRES	X (SIZE") TREE REMOVAL
HOT-MIX ASPHALT SURFACE REMOVAL, 2"	A TREE REMOVAL, ACRES DELINEATION POINTS

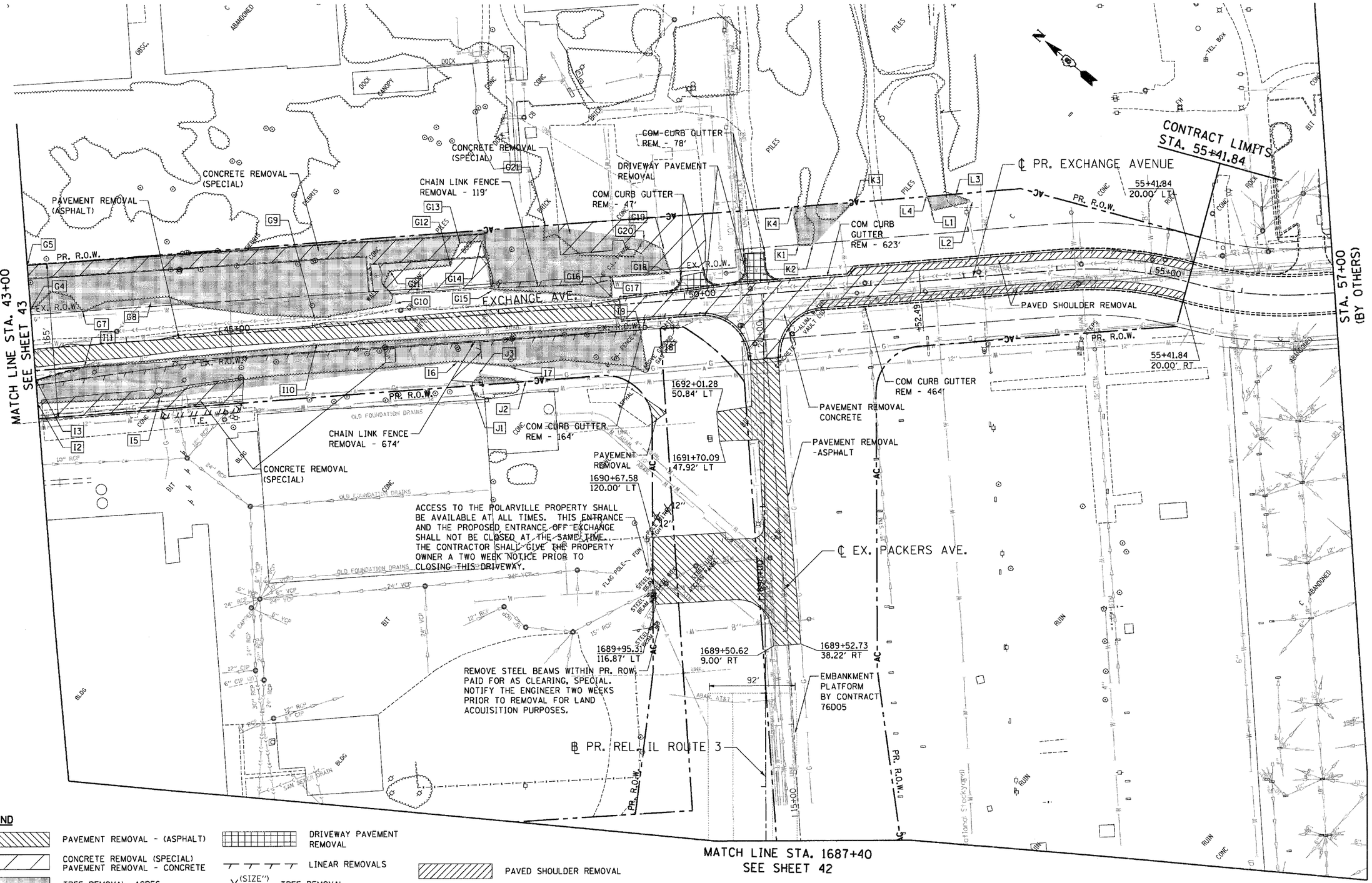
FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED EXCHANGE AVENUE
EXISTING PLAN & REMOVALS
STA. 35+00 TO STA. 43+00

SCALE: 1"=50' | SHEET NO. 3 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	43
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

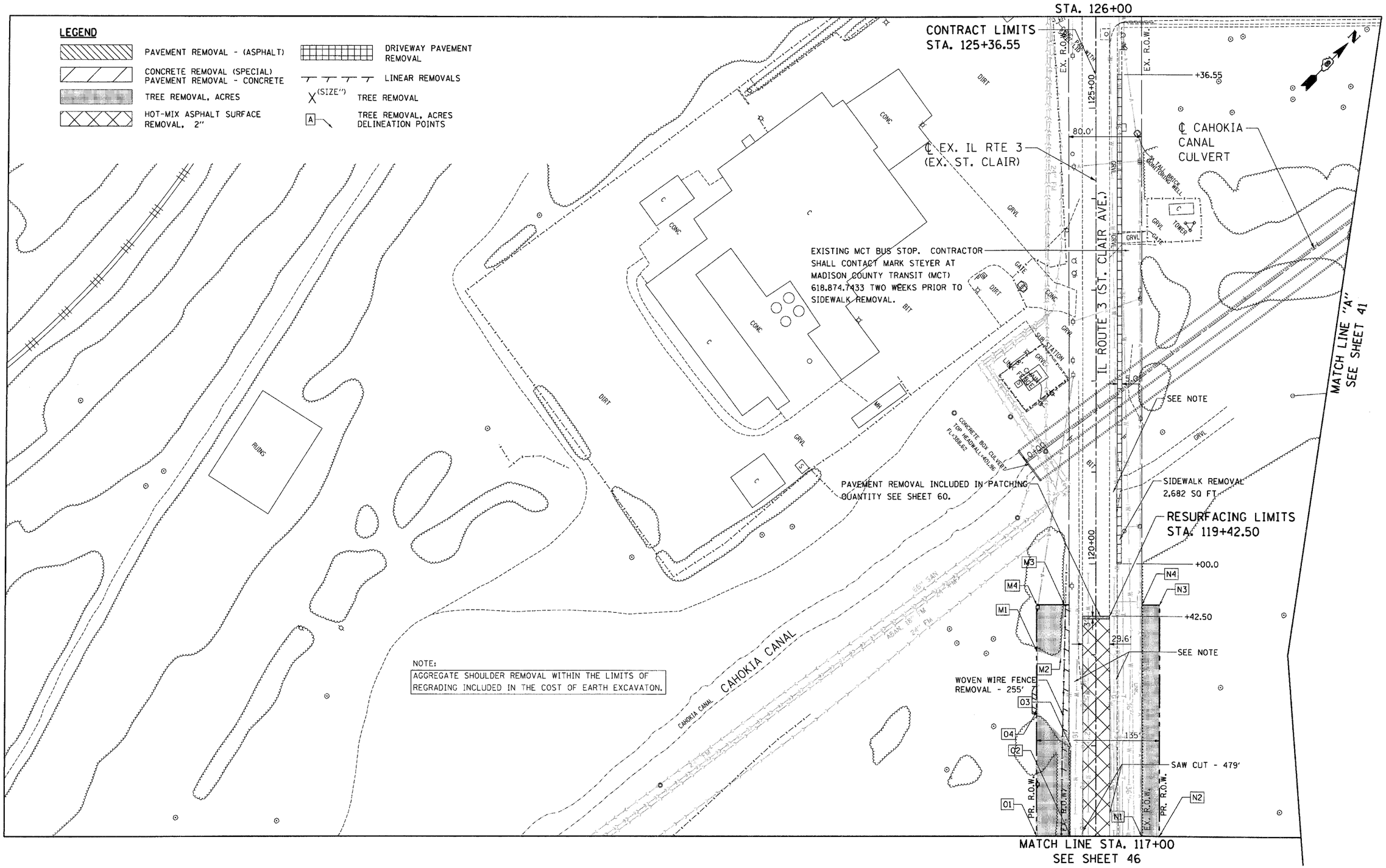


LEGEND

- PAVEMENT REMOVAL - (ASPHALT)
- CONCRETE REMOVAL (SPECIAL) PAVEMENT REMOVAL - CONCRETE
- TREE REMOVAL, ACRES
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- DRIVEWAY PAVEMENT REMOVAL
- LINEAR REMOVALS
- X (SIZE") TREE REMOVAL
- A TREE REMOVAL, ACRES DELINEATION POINTS
- PAVED SHOULDER REMOVAL

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED EXCHANGE AVENUE EXISTING PLAN & REMOVALS STA. 43+00 TO STA. 57+00
		DRAWN - JB	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -		SCALE: 1"=50' SHEET NO. 4 OF 7 SHEETS STA. TO STA.
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	44
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



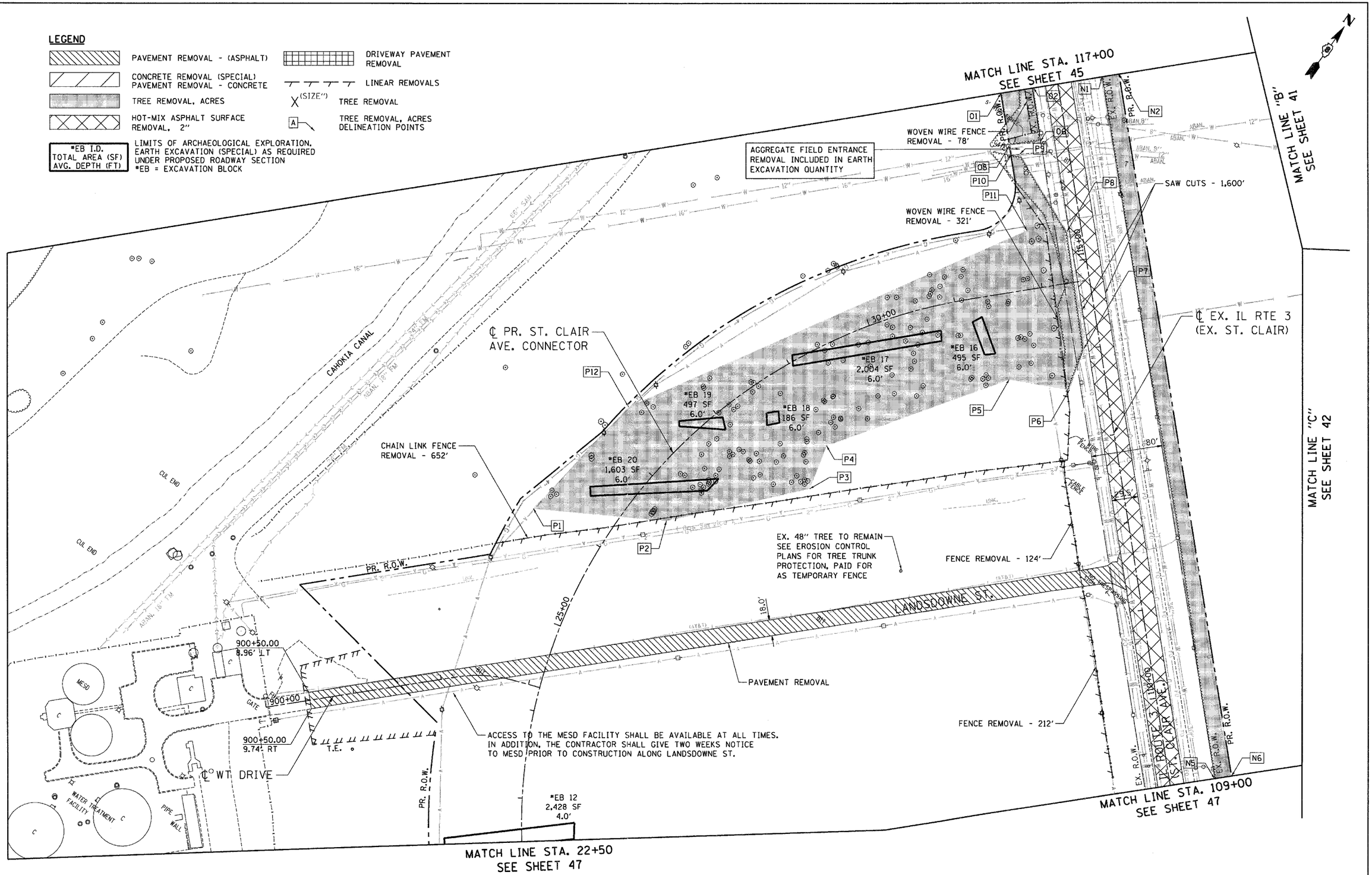
NOTE:
AGGREGATE SHOULDER REMOVAL WITHIN THE LIMITS OF REGRADING INCLUDED IN THE COST OF EARTH EXCAVATON.

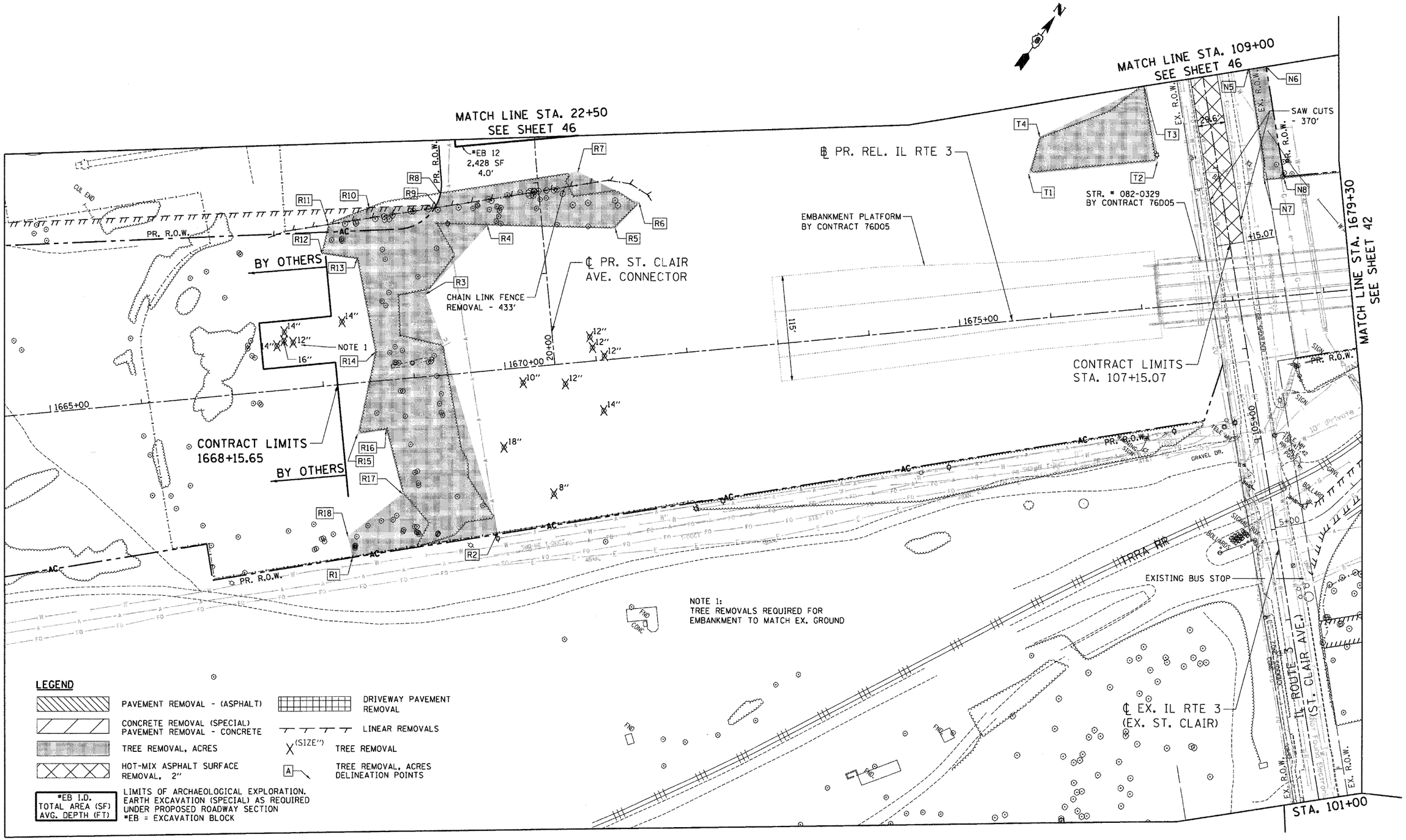
FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	EXISTING ST. CLAIR AVENUE EXISTING PLAN & REMOVALS STA. 117+00 TO 126+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	45
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 5 OF 7 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF.DGN,..\DBCONN-11-BORDER.DGN,..\DBCONN-11-MOTIF.DGN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-LAYOUT.DGN,..\DBCONN-95-MOTIF.DGN,..\DBCONN-95-MOTIF.DGN
10:15:28 11/14/02 DONCALD \\FS-6044\ANYVAULT\JP THANG_8?\2202\20595-001\CIVIL\CAD\99 ALL CONTRACTS\DCONN\SHEETS CONTRACT 1\DBCONN-11-SH1-REMOVAL1-11700.DGN

LEGEND

- PAVEMENT REMOVAL - (ASPHALT)
- CONCRETE REMOVAL (SPECIAL) PAVEMENT REMOVAL - CONCRETE
- TREE REMOVAL, ACRES
- HOT-MIX ASPHALT SURFACE REMOVAL, 2"
- DRIVEWAY PAVEMENT REMOVAL
- LINEAR REMOVALS
- X (SIZE") TREE REMOVAL
- A TREE REMOVAL, ACRES DELINEATION POINTS
- *EB I.D. / TOTAL AREA (SF) / AVG. DEPTH (FT) — LIMITS OF ARCHAEOLOGICAL EXPLORATION. EARTH EXCAVATION (SPECIAL) AS REQUIRED UNDER PROPOSED ROADWAY SECTION. *EB = EXCAVATION BLOCK


MATCH LINE STA. 117+00
SEE SHEET 45
MATCH LINE "B"
SEE SHEET 41
MATCH LINE "C"
SEE SHEET 42
MATCH LINE STA. 109+00
SEE SHEET 47
MATCH LINE STA. 22+50
SEE SHEET 47
WOVEN WIRE FENCE REMOVAL - 78'
AGGREGATE FIELD ENTRANCE REMOVAL INCLUDED IN EARTH EXCAVATION QUANTITY
SAW CUTS - 1,600'
WOVEN WIRE FENCE REMOVAL - 321'
℄ EX. IL RTE 3 (EX. ST. CLAIR)
℄ PR. ST. CLAIR AVE. CONNECTOR
CAHOKIA CANAL
CHAIN LINK FENCE REMOVAL - 652'
*EB 16 495 SF 6.0'
*EB 17 2,004 SF 6.0'
*EB 18 186 SF 6.0'
*EB 19 497 SF 6.0'
*EB 20 1,603 SF 6.0'
*EB 12 2,428 SF 4.0'
EX. 48" TREE TO REMAIN SEE EROSION CONTROL PLANS FOR TREE TRUNK PROTECTION. PAID FOR AS TEMPORARY FENCE
FENCE REMOVAL - 124'
FENCE REMOVAL - 212'
PAVEMENT REMOVAL
LANDSDOWNE ST.
PR. R.O.W.
EX. R.O.W.
ACCESS TO THE MESD FACILITY SHALL BE AVAILABLE AT ALL TIMES. IN ADDITION, THE CONTRACTOR SHALL GIVE TWO WEEKS NOTICE TO MESD PRIOR TO CONSTRUCTION ALONG LANDSDOWNE ST.
900+50.00 8.96' LT
900+50.00 9.74' RT
℄ WT. DRIVE
MESD
WATER TREATMENT FACILITY
PIPE WALL
CUL END
GATE
T.E.
ABAN. 18" TM
ABAN. 18" PM
ABAN. 8" PM
ABAN. 8" TM
ABAN. 12" W
ABAN. 8" W
ABAN. 9" W
ABAN. 12" W
ABAN. 8" W
ABAN. 12" W
ABAN. 8" W
ABAN. 12" W
ABAN. 8



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

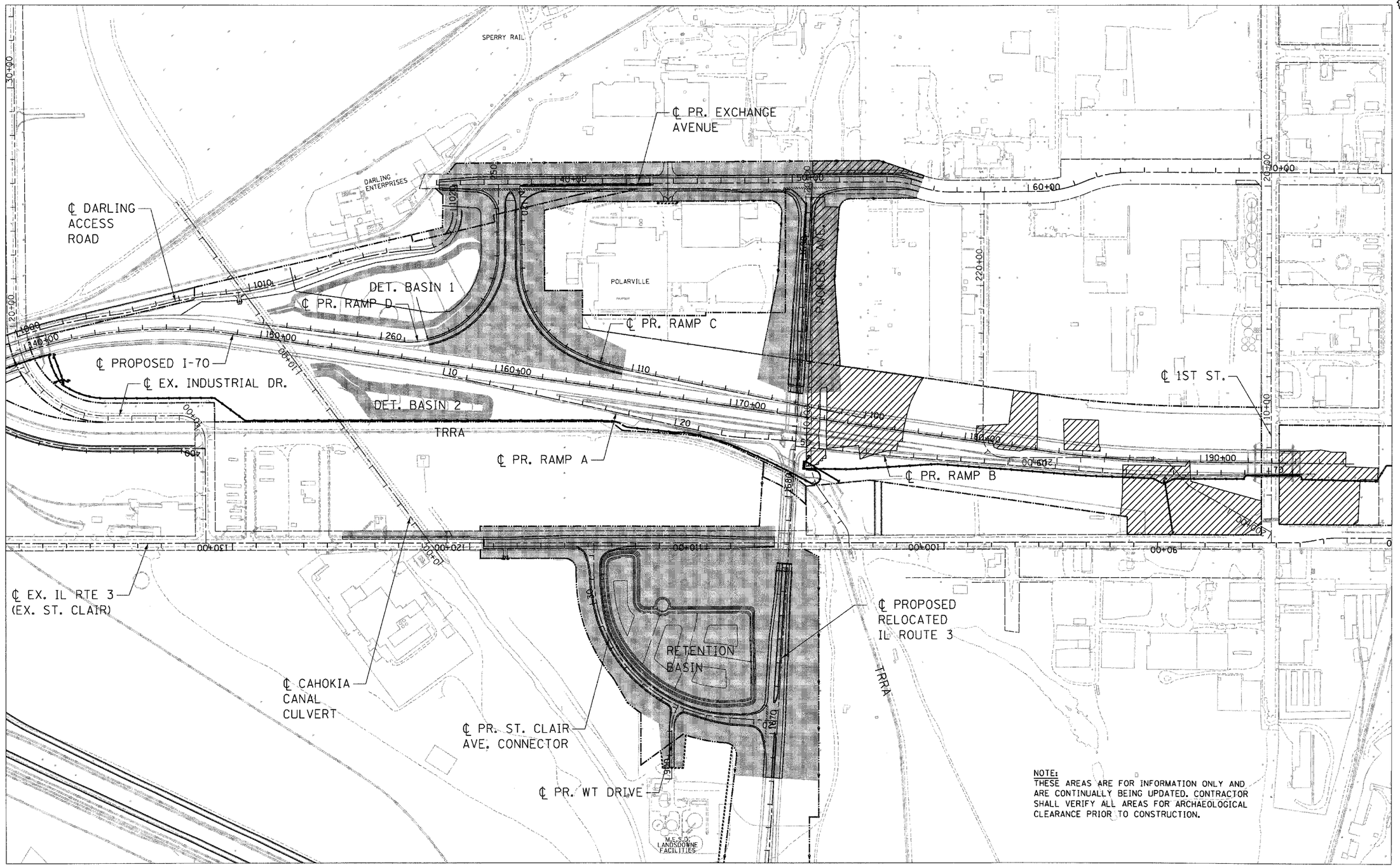
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RELOCATED ILLINOIS ROUTE 3
EXISTING PLAN & REMOVALS
STA. 1665+00 TO STA. 1679+30

SCALE: 1"=50' | SHEET NO. 7 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	47
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

...\DBCONN-99-MOTIFI.DGN, ...\DBCONN-11-BORDER1.DGN, ...\DBCONN-11-MOTIFI.DGN, ...\DBCONN-11-LEGEND1.DGN, ...\DBCONN-11-LAYOUT1.DGN, ...\DBCONN-05-MOTIFI.DGN, ...\DBCONN-06-MOTIFI.DGN, ...\DBCONN-08-MOTIFI.DGN, ...\DBCONN-10-MOTIFI.DGN
10-19-2011, 14:35:11 BONDHLJD \\FS-0044\AM\VAULT_D-TRANS_07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-REMOVAL1-166500.DGN



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

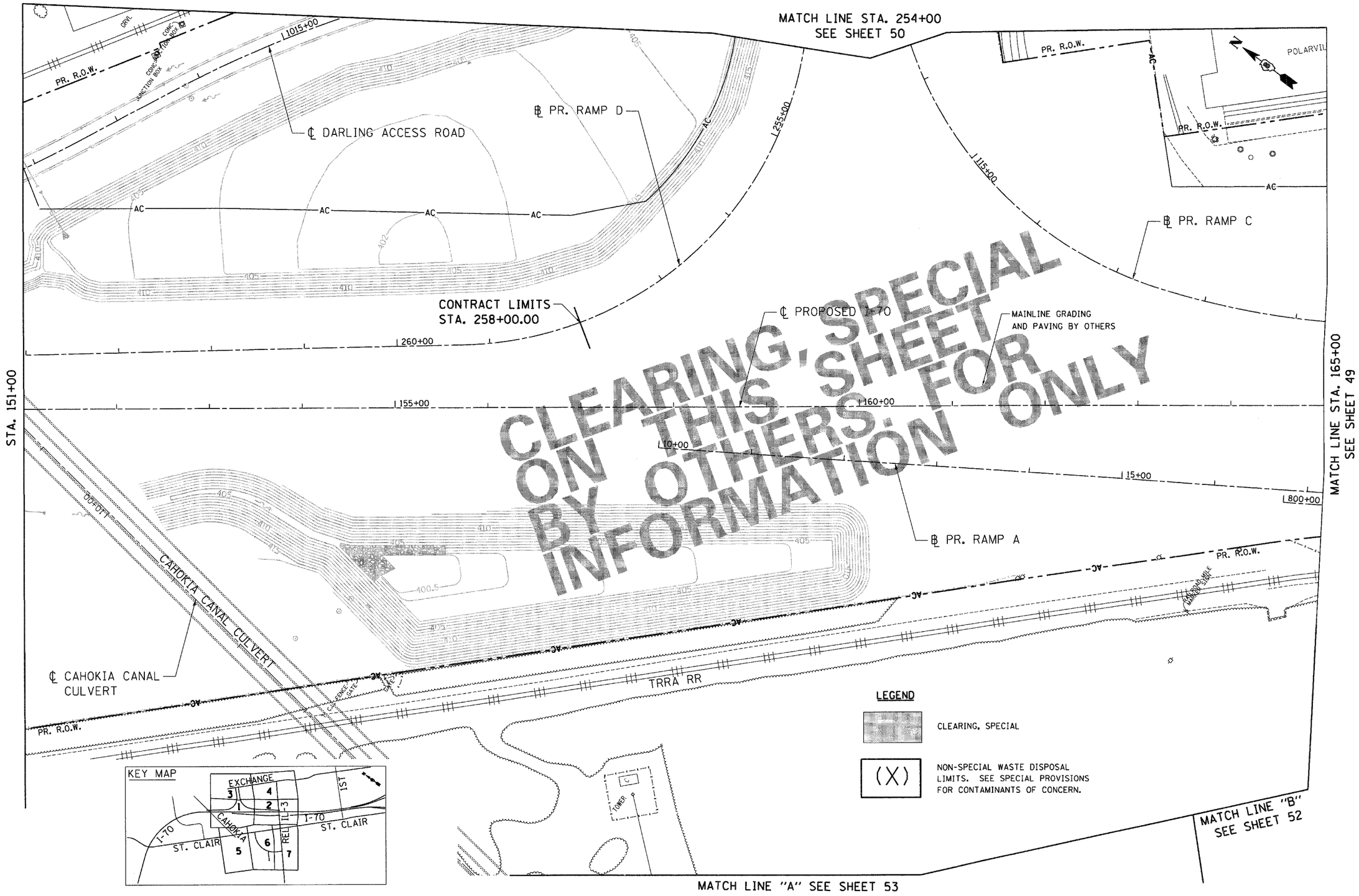
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

ARCHAEOLOGICAL AREAS NOT YET CLEARED AS OF 10/21/11
(FOR INFORMATION ONLY)

SCALE: 1"=200' | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	48
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\\DBCONN-99-MOTIFLIGN,..\DBCONN-11-BORDERLIGN,..\DBCONN-11-ALIGNMENT3.DGN,..\DBCONN-99-ALIGNMENT2.DGN,..\DBCONN-10-MOTIFLIGN,..\DBCONN-11-MOTIFLIGN,..\DBCONN-06-MOTIFLIGN,..\DBCONN-05-MOTIFLIGN
12-19-2011,14:36:03 BONDHUJO \\FS-0844\PAR\PAULTJ-TRANS.87\2282\28868 001\CIVIL\CAD\99 ALL CONTRACTS\CONNSHEETS CONTRACT 11\DBCONN-11-SH1-HAZMAT.DGN



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - MDN	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

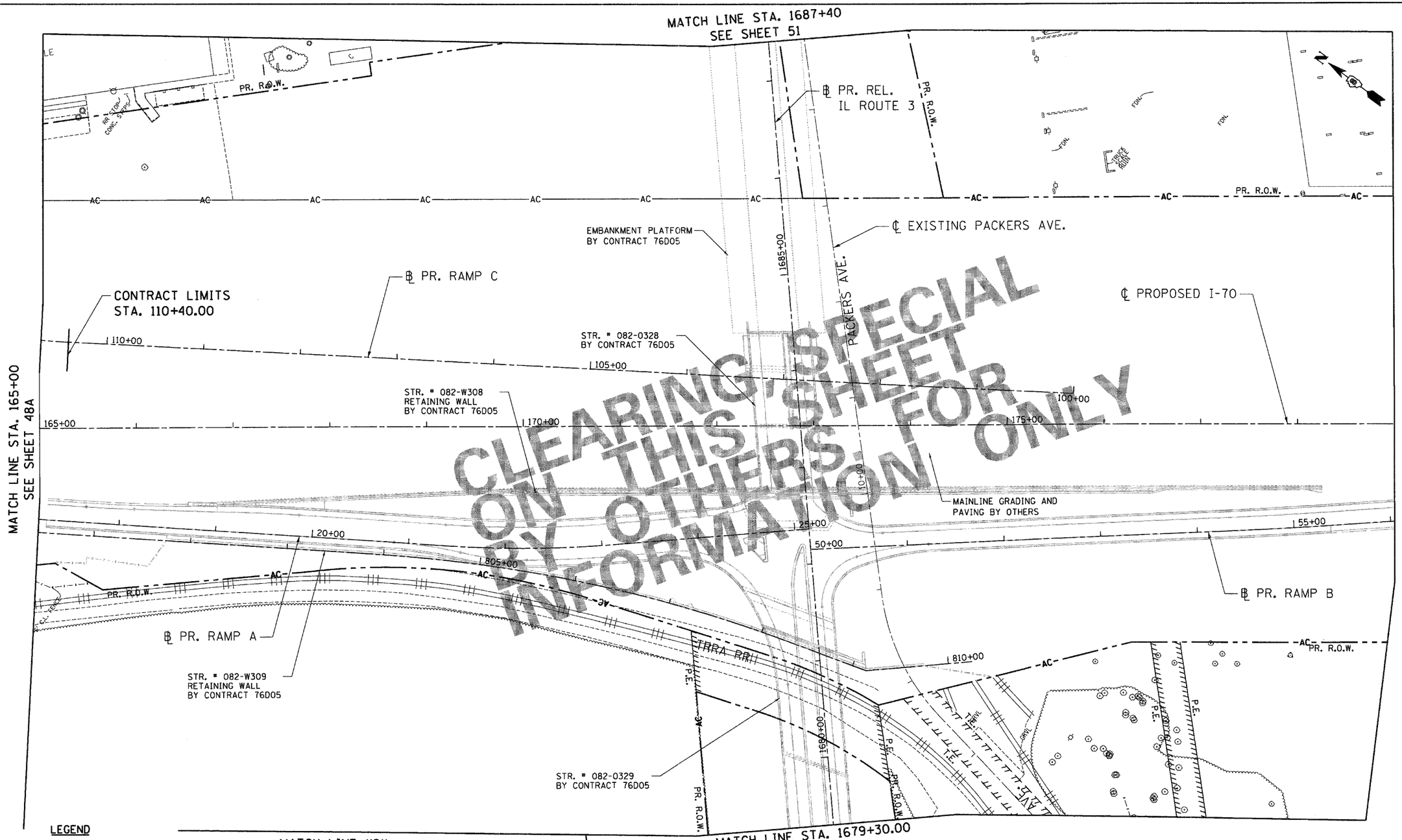
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED I-70
CLEARING PLAN
STA. 151+00 TO STA. 165+00

SCALE: 1"=50' | SHEET NO. 1 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	48A
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIFLIGN,..\DBCONN-11-BORDER\LIGN,..\DBCONN-11-MOTIFLIGN,..\DBCONN-11-LEGEND\LIGN,..\DBCONN-11-LAYOUT\LIGN,..\DBCONN-05-MOTIFLIGN,..\DBCONN-08-MOTIFLIGN
10-19-2011,14:36:27 BONGHUJO \\FS-0044\SAN\VAULT\D-TRANS,97\2282\208863-882\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-14-MAZMIN-15\MXMXN



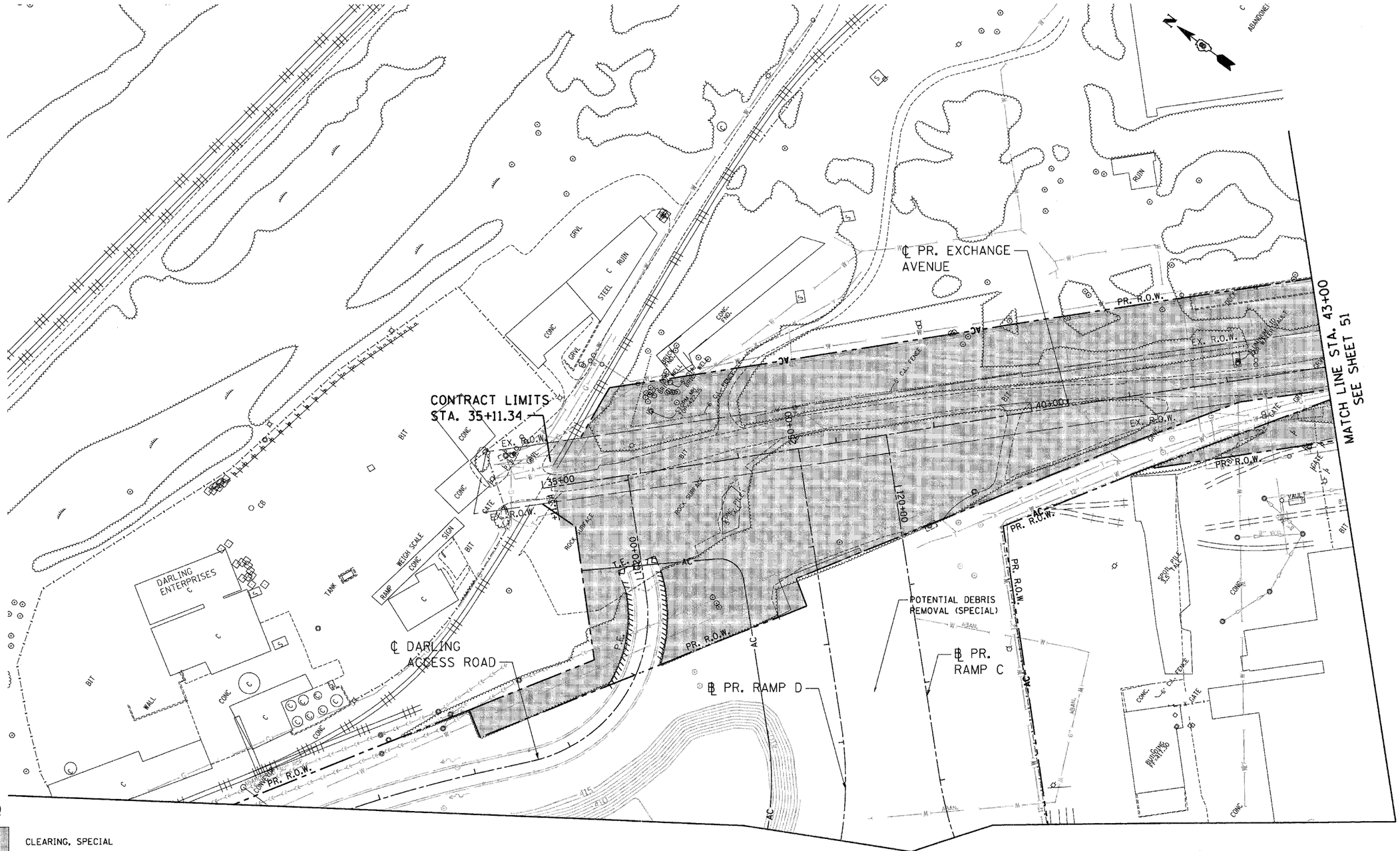
CLEARING, SPECIAL ON THIS SHEET BY OTHERS FOR INFORMATION ONLY

LEGEND

CLEARING, SPECIAL

(X) NON-SPECIAL WASTE DISPOSAL LIMITS. SEE SPECIAL PROVISIONS FOR CONTAMINANTS OF CONCERN.

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED I-70 CLEARING PLAN STA. 165+00 TO STA. 179+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MDN	REVISED -			998	82-2-1K	ST. CLAIR	353	49
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE		CONTRACT NO. 76E06				
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 2 OF 7 SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



LEGEND

CLEARING, SPECIAL

(X) NON-SPECIAL WASTE DISPOSAL LIMITS. SEE SPECIAL PROVISIONS FOR CONTAMINANTS OF CONCERN.

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - MDN	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED EXCHANGE AVENUE
CLEARING PLAN
STA. 35+00 TO STA. 43+00

SCALE: 1"=50' | SHEET NO. 3 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	50
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN, ..\DBCONN-11-BORDER1.DGN, ..\DBCONN-11-MOTIF1.DGN, ..\DBCONN-11-LEGEND1.DGN, ..\DBCONN-11-LAYOUT1.DGN, ..\DBCONN-05-MOTIF1.DGN, ..\DBCONN-06-MOTIF1.DGN, ..\DBCONN-08-MOTIF1.DGN
10-19-2011, 14:37:24 BONDHUJO \\FS-0044\AM\VAULT_D-TRANS.07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-HAZMAT1-3500.DGN

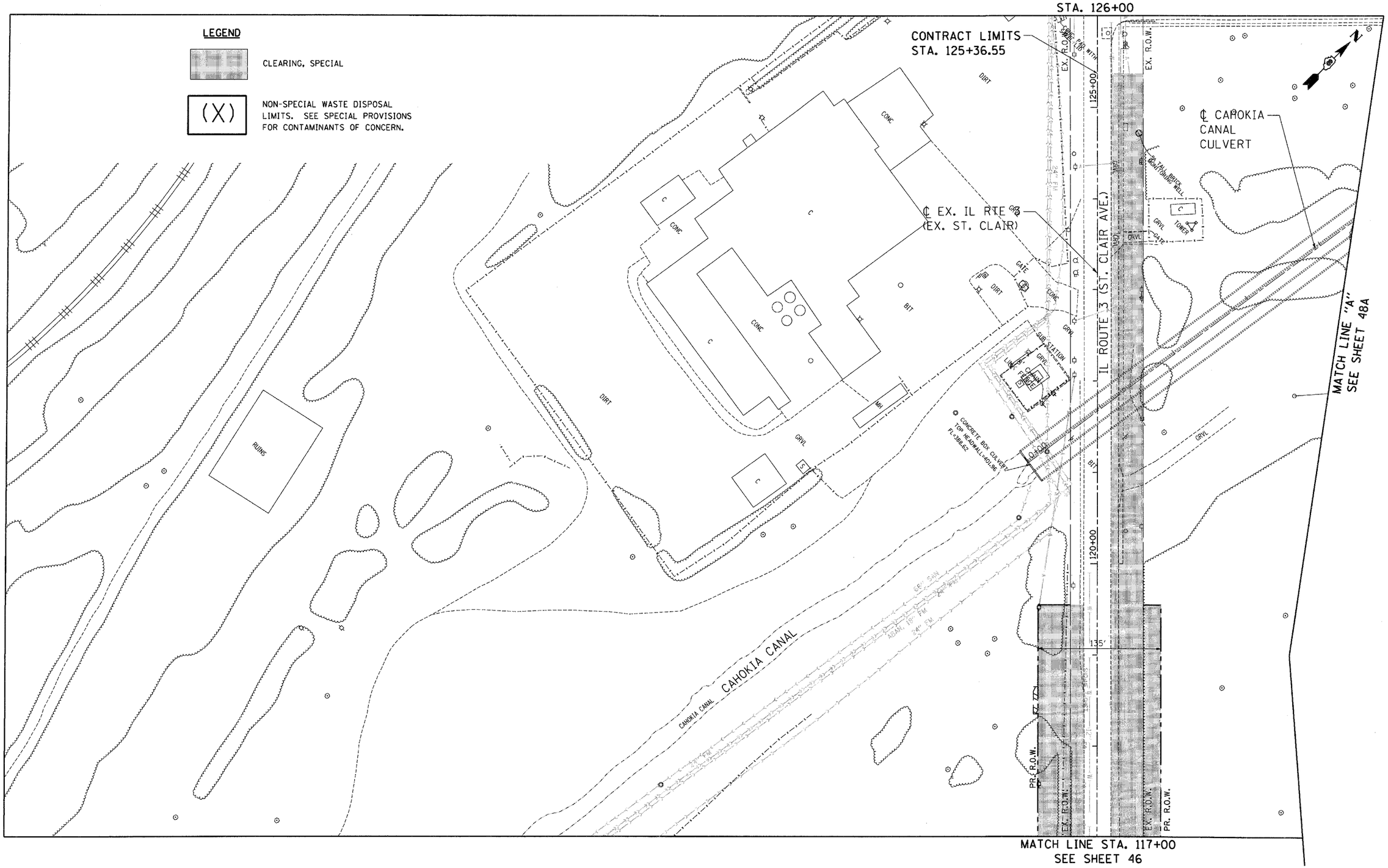
LEGEND

CLEARING, SPECIAL

(X) NON-SPECIAL WASTE DISPOSAL LIMITS. SEE SPECIAL PROVISIONS FOR CONTAMINANTS OF CONCERN.

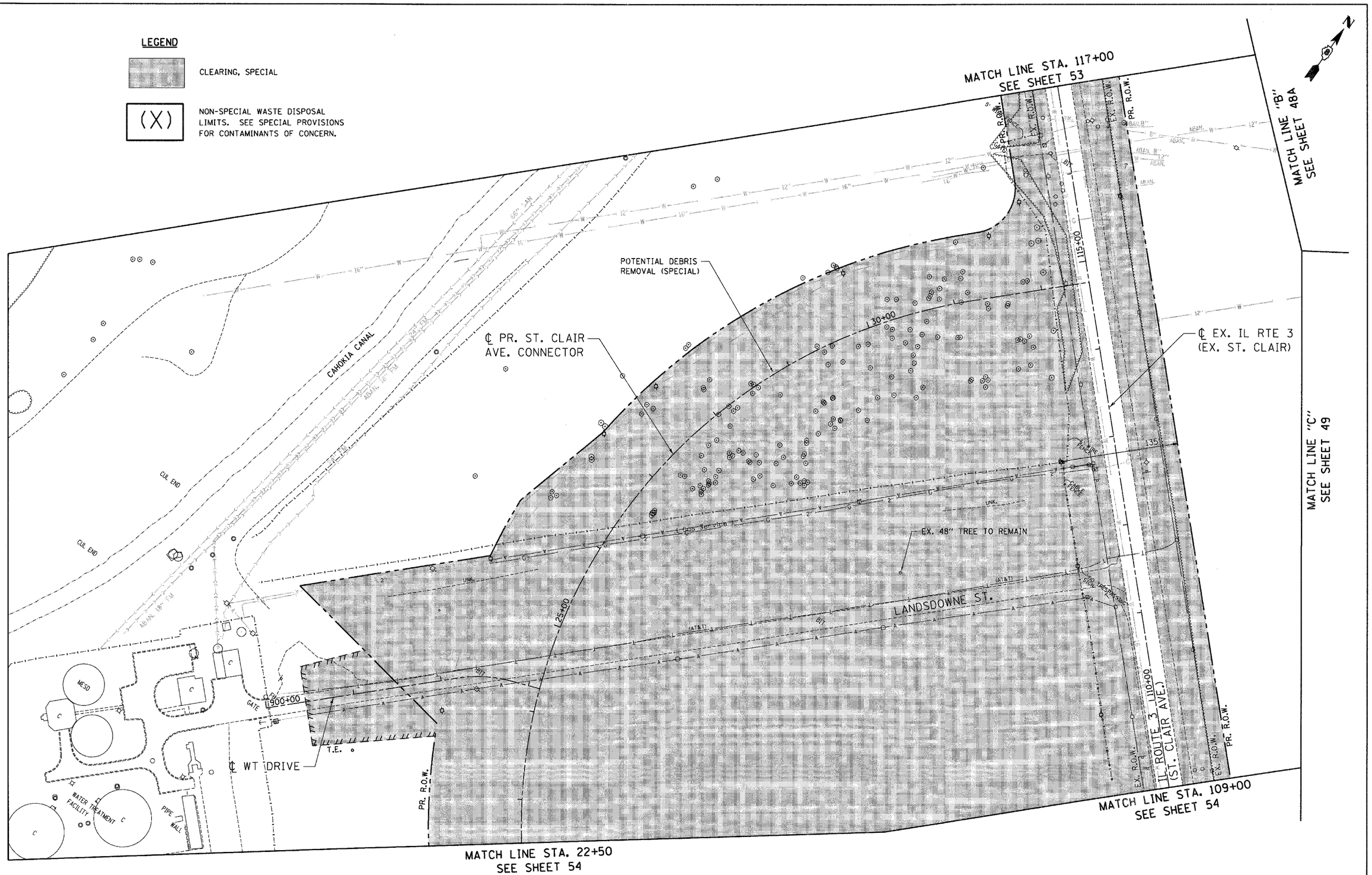
FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION** PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	**PROPOSED EXCHANGE AVENUE CLEARING PLAN STA. 43+00 TO STA. 57+00**	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MDN	REVISED -			998	82-2-1K	ST. CLAIR	353	51
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 4 OF 7 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN,..\DBCONN-11-BORDER1.DGN,..\DBCONN-11-MOTIF1.DGN,..\DBCONN-11-LEGEND1.DGN,..\DBCONN-11-LAYOUT1.DGN,..\DBCONN-05-MOTIF1.DGN,..\DBCONN-06-MOTIF1.DGN,..\DBCONN-03-GEOMETRICS1.DGN,..\DBCONN-10-MOTIF1.DGN
10-19-2011,14:37:52 BONG#AJO \\FS-0044\DMV\PROJ_JD-THMAS_BY\22822-26865-PROJ\CIVIL\CADD\V9 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-HAZMAT1-4.DGN



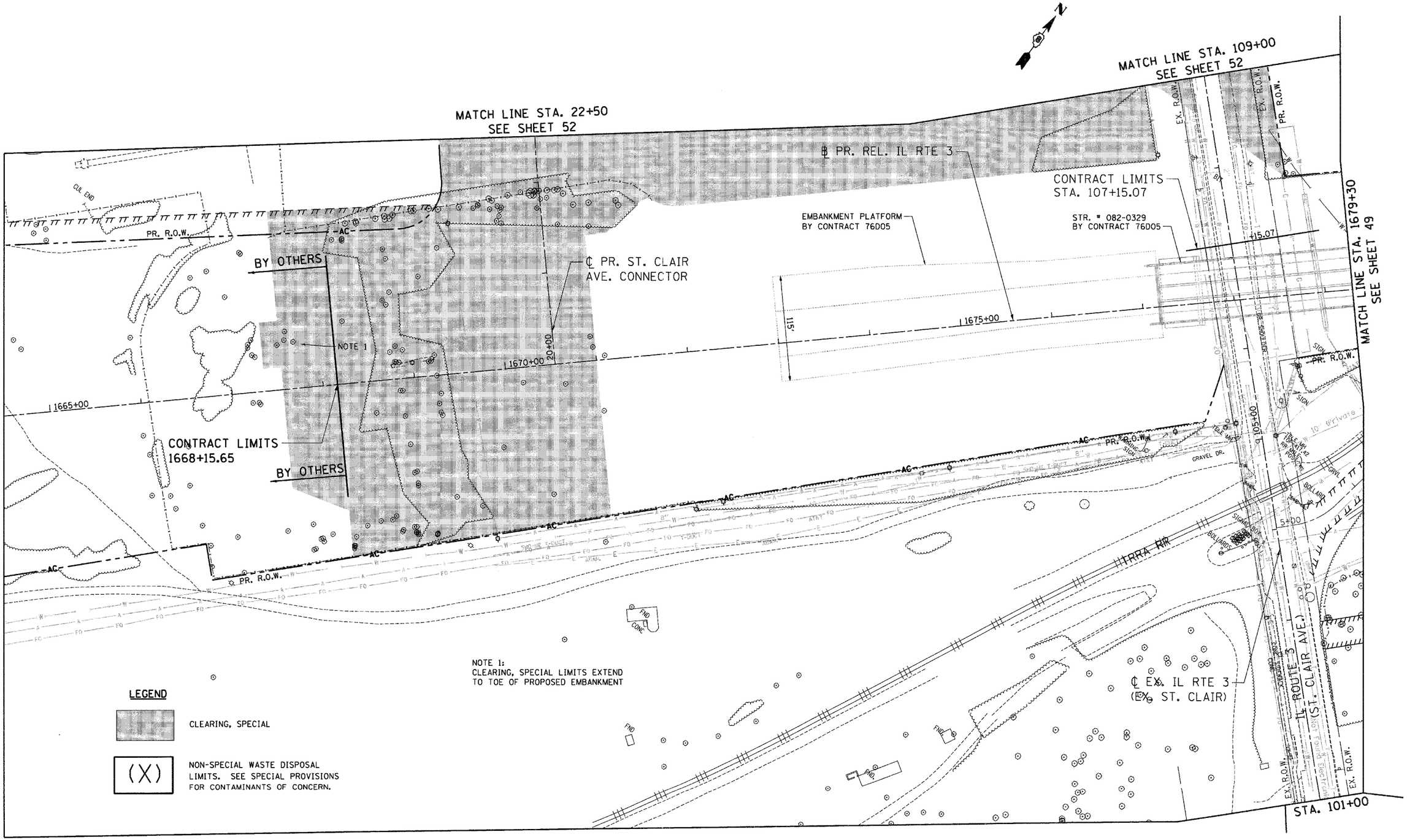
FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	EXISTING ST. CLAIR AVENUE CLEARING PLAN STA. 117+00 TO 126+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - MDN	REVISED -			998	82-2-1K	ST. CLAIR	353	52
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -			CONTRACT NO. 76E06				
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 5 OF 7 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

$DGCONN-99-MOTIF1.DGN,..\DBCONN-11-BORDER1.DGN,..\DBCONN-11-MOTIF1.DGN,..\DBCONN-11-LEGEND1.DGN,..\DBCONN-11-LAYOUT1.DGN,..\DBCONN-05-MOTIF1.DGN,..\DBCONN-08-MOTIF1.DGN
10-19-2011,14:38:19 BDKOHLIO \\FS-02\44\DRV\HDL,TD-TRANS,07\2202\26050-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SH1-HAZMAT1-1700.DGN

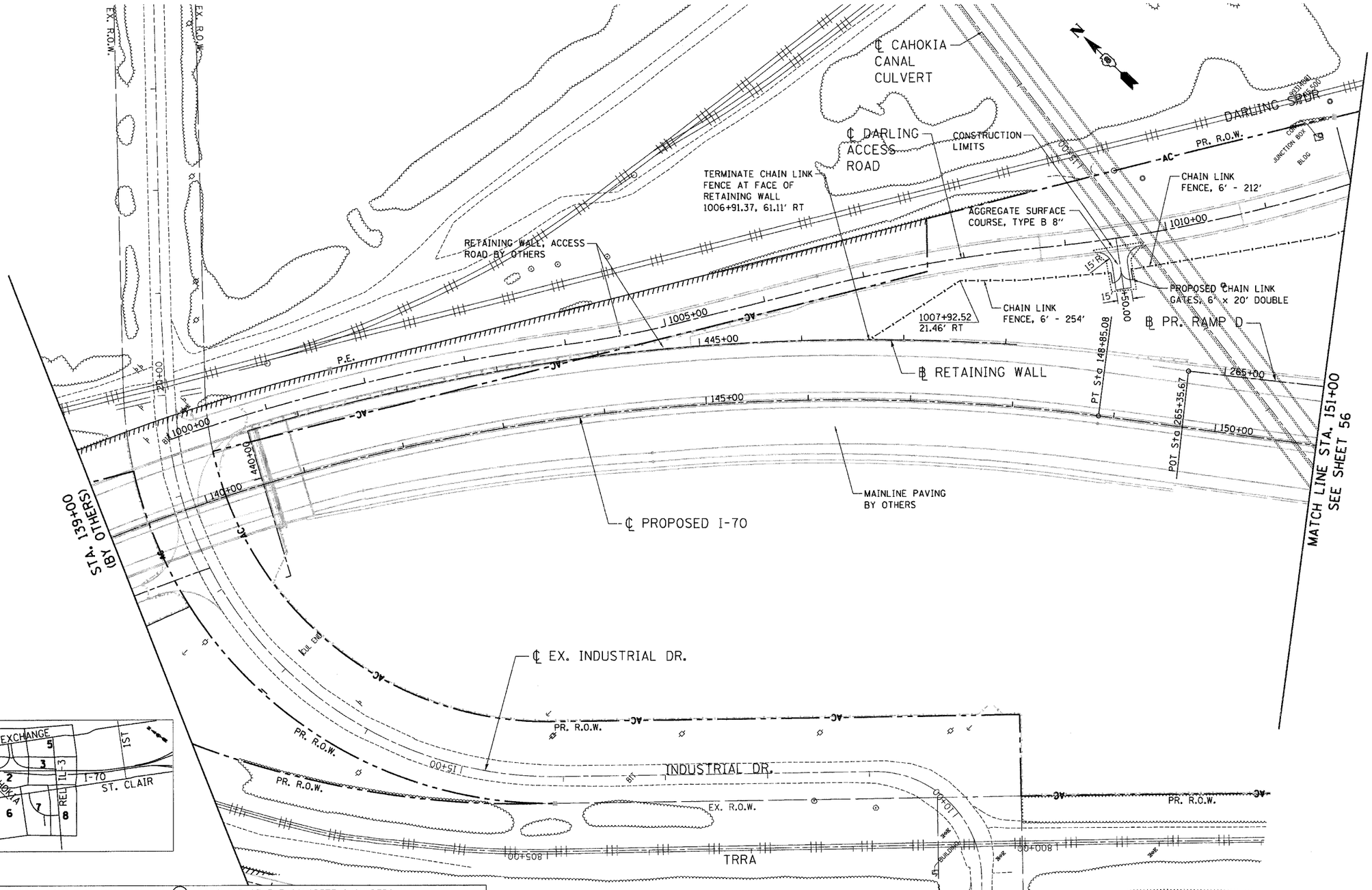


FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	PROPOSED CONNECTOR ROAD AND EXISTING ST. CLAIR AVENUE CLEARING PLAN STA. 109+00 TO STA. 117+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - MDN	REVISED -			998	82-2-1K	ST. CLAIR	353 / 53
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06	
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 6 OF 7 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

...\DBCONN-99-MOTIF1.DGN,...\DBCONN-11-BORDER.DGN,...\DBCONN-11-MOTIF1.DGN,...\DBCONN-11-LEGEND.DGN,...\DBCONN-11-LAYOUT.DGN,...\DBCONN-05-MOTIF1.DGN,...\DBCONN-05-MOTIF1.DGN
10-19-2011, 14:38:48 BONGHUJO \\FS-0044\AMY\PALL\P-FRAMS.BY\22822\20868-00\CIVIL\CHOV99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-HRZMNT1-OPTOB.DGN



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED RELOCATED ILLINOIS ROUTE 3 CLEARING PLAN STA. 1665+00 TO STA. 1679+30	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353 / SHEET NO. 54
		DRAWN - MDN	REVISED -					CONTRACT NO. 76E06	
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	SCALE: 1"=50' / SHEET NO. 7 OF 7 SHEETS / STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -						



LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

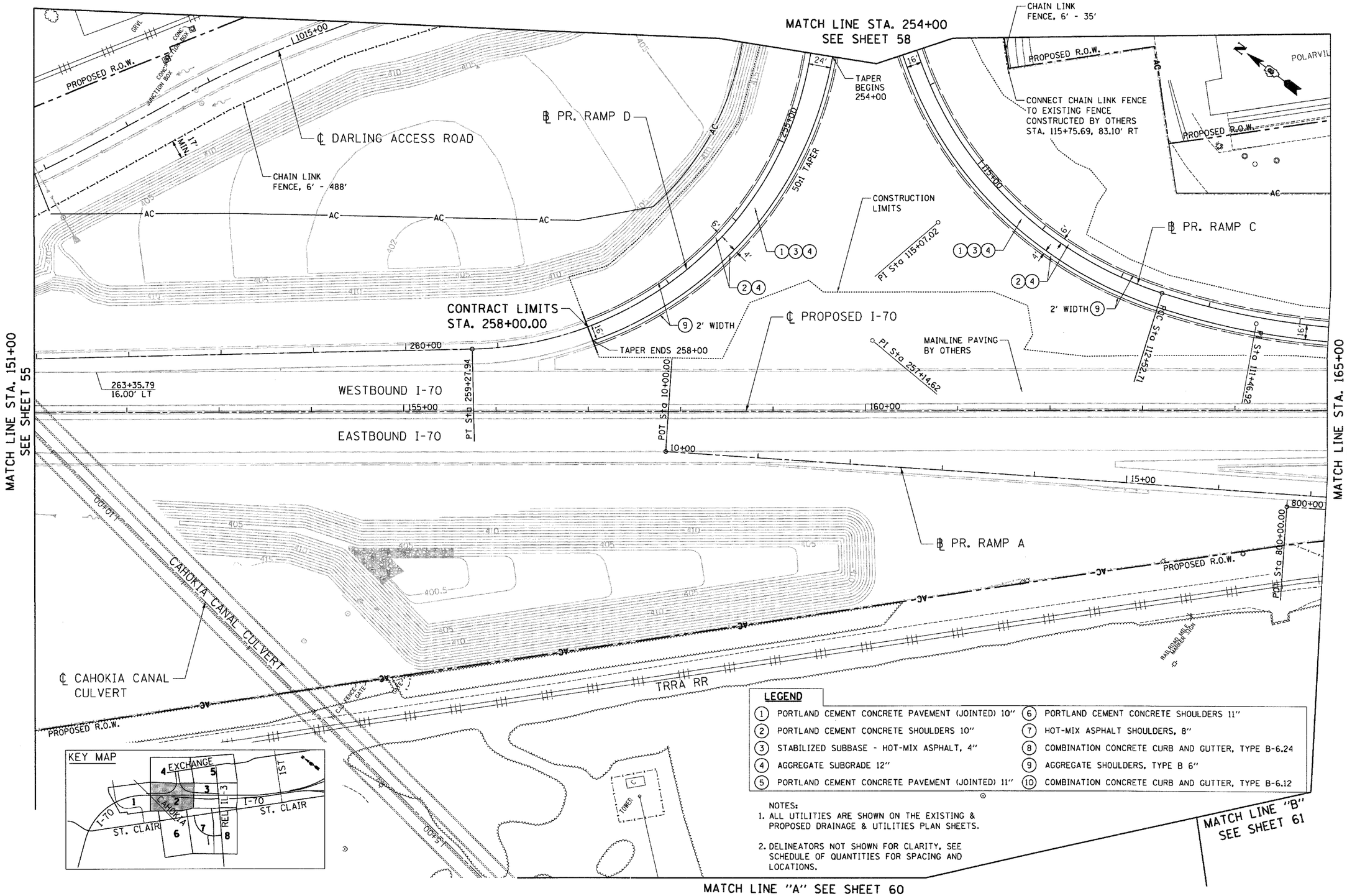
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED I-70
ROADWAY PLAN
STA. 139+00 TO STA. 151+00

SCALE: 1"=50' | SHEET NO. 1 OF 8 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	55
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN ..\DBCONN-11-BORDER1.DGN ..\DBCONN-11-MOTIF1.DGN ..\DBCONN-11-LEGEND1.DGN ..\DBCONN-11-LAYOUT1.DGN ..\DBCONN-05-MOTIF1.DGN ..\DBCONN-06-MOTIF1.DGN ..\DBCONN-08-MOTIF1.DGN
10-19-2011, 14:39:53 BONDHUJD \\FS-0044\AM\VAULT\D-TRANS.07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PLAN1-13900.DGN



LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

NOTES:
1. ALL UTILITIES ARE SHOWN ON THE EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN SHEETS.
2. DELINEATORS NOT SHOWN FOR CLARITY, SEE SCHEDULE OF QUANTITIES FOR SPACING AND LOCATIONS.

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED I-70
ROADWAY PLAN
STA. 151+00 TO STA. 165+00

SCALE: 1"=50' | SHEET NO. 2 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	56
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN, ..\DBCONN-11-BORDER1.DGN, ..\DBCONN-11-MOTIF1.DGN, ..\DBCONN-11-LEGEND1.DGN, ..\DBCONN-11-LAYOUT1.DGN, ..\DBCONN-05-MOTIF1.DGN, ..\DBCONN-06-MOTIF1.DGN, ..\DBCONN-08-MOTIF1.DGN, ..\DBCONN-06-CONTOUR1.DGN, ..\DBCONN-10-MOTIF1.DGN
10-19-2011, 14:40:29 BONDHUJD \\FS-0044\AM\VAULT_D-TRANS.07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PLAN1-15100.DGN

MATCH LINE STA. 1687+40
SEE SHEET 59

MATCH LINE STA. 165+00
SEE SHEET 56

STA. 179+00

MATCH LINE "C"
SEE SHEET 61

MATCH LINE STA. 1679+30.00
SEE SHEET 62

1687+27.51
23.00' LT

CONCRETE MEDIAN SURFACE, 6"
AGGREGATE BASE COURSE, TYPE B 6"

℄ PR. REL.
IL ROUTE 3

CHAIN LINK FENCE, 6' - 158'

SPBGR TY A
6FT POSTS
- 262.5'
PCC SHLD 11
FOR GUARDRAIL
STABILIZATION
3' ADDTL WIDTH

CONSTRUCTION
LIMITS

CONNECT TO EX. FENCE
CONSTRUCTED BY OTHERS
1685+91.83, 164.00 LT

CHAIN LINK FENCE, 6' - 176'

CONNECT TO EX. FENCE
CONSTRUCTED BY OTHERS

1685+65.69
167.00' RT

FURNISHING AND ERECTING RIGHT OF WAY MARKERS (TYP.)
SEE SCHEDULE OF QUANTITIES FOR ALL LOCATIONS.

BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)
1684+70.43 TO 1685+70.43

TRAF BAR TERM T2
1685+51.66

1685+27.51
11.00' LT

1' ADJACENT
TO GUARDRAIL
2' WIDTH BEYOND
GUARDRAIL

SPBGR TY A
6FT POSTS - 75'
PCC SHLD 11
FOR GUARDRAIL
STABILIZATION
3' ADDTL WIDTH

2' WIDTH

CONSTRUCTION
LIMITS

CONTRACT LIMITS
STA. 110+40.00

℄ PR. RAMP C

TRAF BAR
TERM T6

CONCRETE MEDIAN
TYPE SB-6.12

PAVING LIMITS
1684+70.43

TRAF BAR TERM T5

APPROACH SLAB

MAINLINE PAVING
BY OTHERS

STR. # 082-0328
BY CONTRACT 76D05

BACK OF N. ABUTMENT
1684+40.93

PACKERS AVE.

℄ PROPOSED I-70

STR. # 082-W308
RETAINING WALL
BY CONTRACT 76D05

110+00
105+00
100+00
165+00
170+00
175+00
120+00
805+00
25+00
50+00
55+00
810+00

℄ PR. RAMP B

℄ EXISTING PACKERS AVE.

℄ PR. RAMP A

STR. # 082-W309
RETAINING WALL
BY CONTRACT 76D05

TRRA RR

1680+88.54
130.00' LT

1680+00

1679+88.24
60.48' RT

STR. # 082-0329
BY CONTRACT 76D05

PR. R.O.W.

P.E.

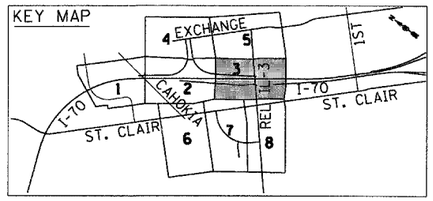
AVE.

LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

NOTE:
ALL UTILITIES ARE SHOWN ON THE EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN SHEETS.

KEY MAP



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

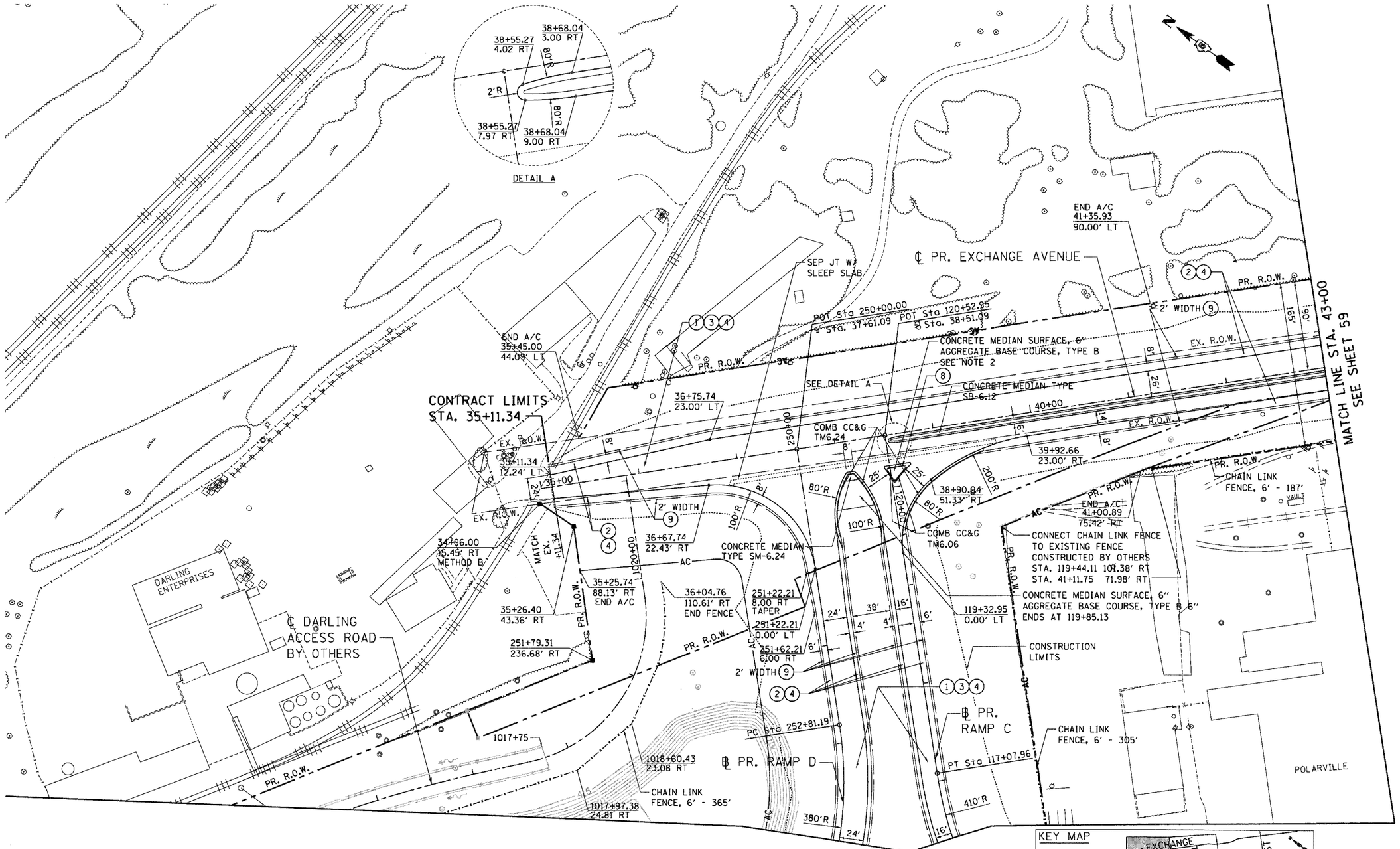
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED I-70
ROADWAY PLAN
STA. 165+00 TO STA. 179+00

SCALE: 1"=50' | SHEET NO. 3 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	57
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\\DBCONN-99-MOTIFLIDGN,..\DBCONN-11-BORDER1.DGN,..\DBCONN-11-MOTIFLIDGN,..\DBCONN-11-LEGEND1.DGN,..\DBCONN-11-LAYOUT1.DGN,..\DBCONN-05-MOTIFLIDGN,..\DBCONN-08-MOTIFLIDGN
10-17-2011, 14:41:25 BENCHMUD \\FS-W044\DMA\PWL\D-PWMS.B\V2002\20859 #01\CIVIL\CND\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PLAN-165300.DGN



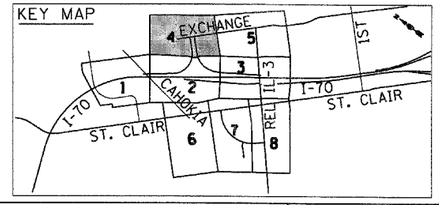
LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

NOTES:

1. ALL UTILITIES ARE SHOWN ON THE EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN SHEETS.
2. USE AGGREGATE BASE COURSE TYPE B (TONS) TO FILL BETWEEN CONCRETE MEDIAN SURFACE AND AGG SUBGRADE 12" PER STANDARD 606301-04

KEY MAP



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	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

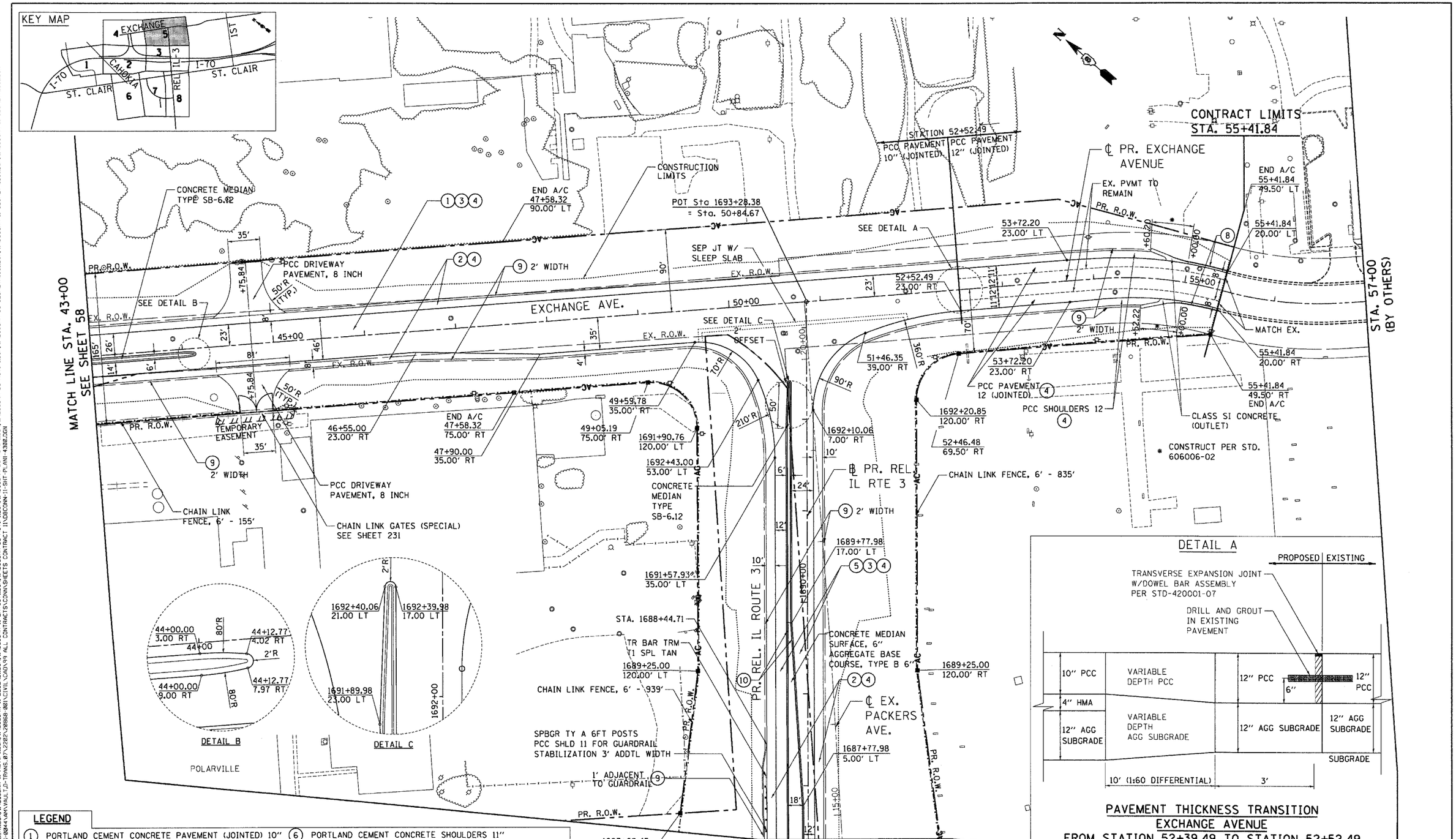
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED EXCHANGE AVENUE
ROADWAY PLAN
STA. 35+00 TO STA. 43+00

SCALE: 1"=50' | SHEET NO. 4 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	58
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN, ..\DBCONN-11-BORDER1.DGN, ..\DBCONN-11-MOTIF1.DGN, ..\DBCONN-11-LEGEND1.DGN, ..\DBCONN-11-LAYOUT1.DGN, ..\DBCONN-05-MOTIF1.DGN, ..\DBCONN-08-MOTIF1.DGN, ..\DBCONN-05-ALIGNMENT3.DGN, ..\DBCONN-11-GEOMETRICS1.DGN
10-19-2011, 14:41:37 BONDHUJD \\FS-0044\AM\VAULT\D-TRANS.07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PLAN1-3500.DGN



PAVEMENT THICKNESS TRANSITION
EXCHANGE AVENUE
FROM STATION 52+39.49 TO STATION 52+52.49

NOTE:
ALL MATERIALS AND WORK ASSOCIATED WITH THE CONSTRUCTION OF THE PAVEMENT TRANSITION AND TRANSVERSE EXPANSION JOINT SHALL BE INCLUDED IN THE COST OF THE PROPOSED P.C.C. PAVEMENT 10" (JOINTED) AND AGGREGATE SUBGRADE 12"

NOTE:
ALL UTILITIES ARE SHOWN ON THE EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN SHEETS.

LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

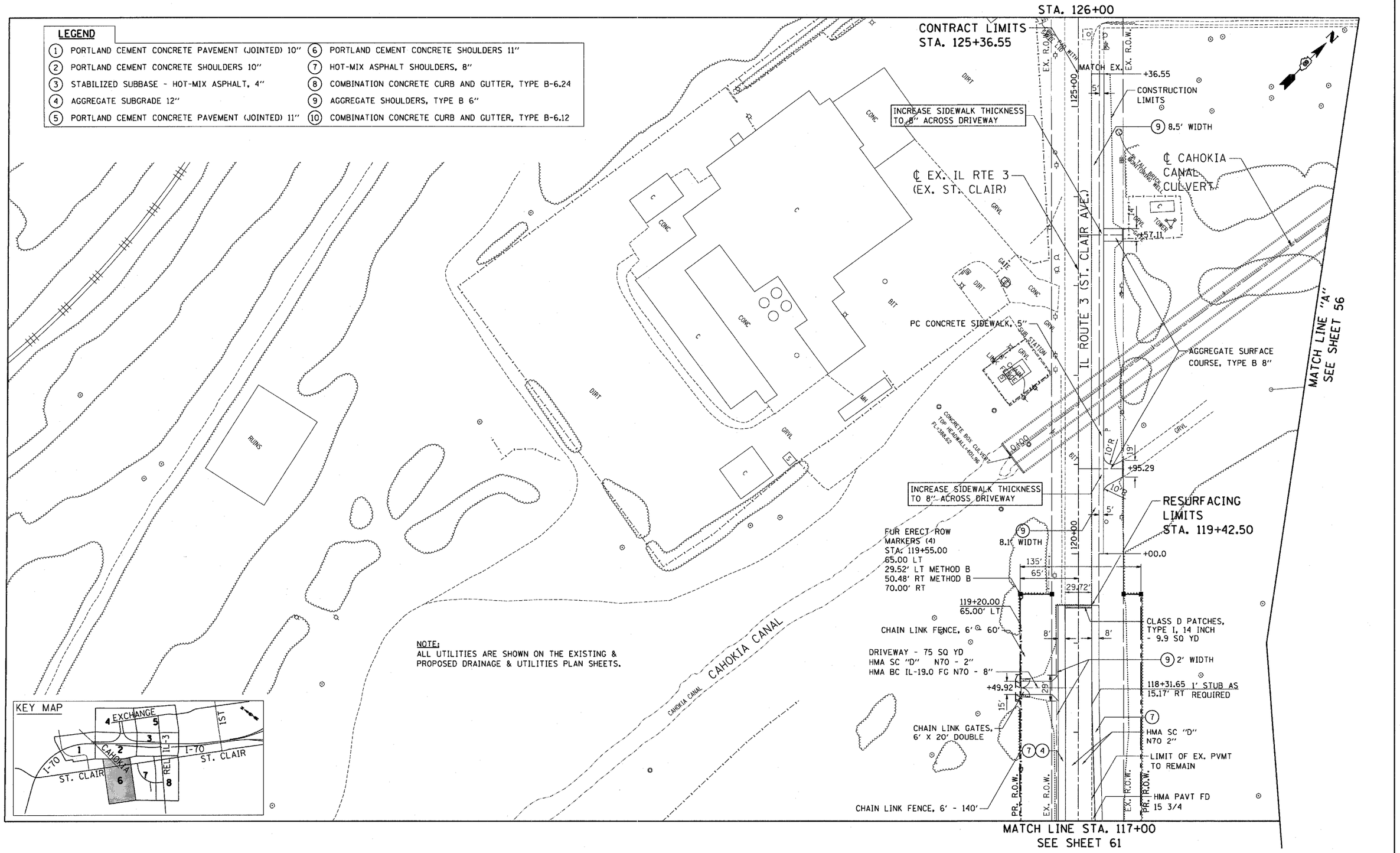
PROPOSED EXCHANGE AVENUE
ROADWAY PLAN
STA. 43+00 TO STA. 57+00

SCALE: 1"=50' | SHEET NO. 5 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	59
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

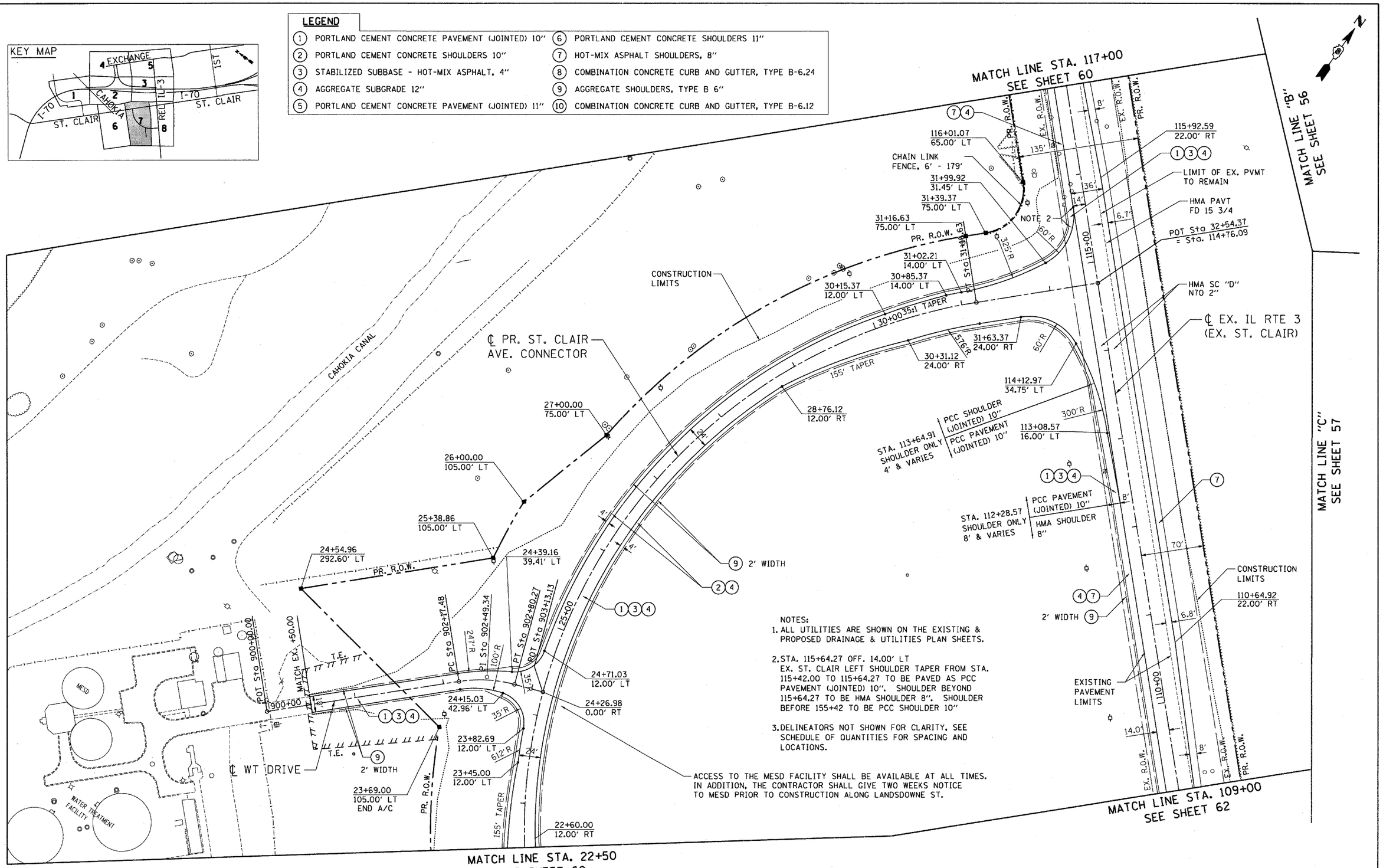
..\DBCONN-99-MOTIF1.DGN,..\DBCONN-11-BORDER1.DGN,..\DBCONN-11-MOTIF1.DGN,..\DBCONN-11-LEGEND1.DGN,..\DBCONN-11-LAYOUT1.DGN,..\DBCONN-85-MOTIF1.DGN,..\DBCONN-08-MOTIF1.DGN,..\DBCONN-83-GEOMETRICS1.DGN,..\DBCONN-11-GEOMETRICS1.DGN
10-19-2011, 14:42:02 BONDHUJD \\FS-0044\WKVAULT_10-TRANS.87\2202\28868-001\CIVIL\CAD\94 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PLAN-43002.DGN



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	EXISTING ST. CLAIR AVENUE ROADWAY PLAN STA. 117+00 TO 126+00	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353 / SHEET NO. 60
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	DRAWN - JB	REVISED -					CONTRACT NO. 76E06	
	PLOT DATE = $DATE$	CHECKED - ACL	REVISED -		SCALE: 1"=50' / SHEET NO. 6 OF 8 SHEETS / STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	
		DATE - 10/21/11	REVISED -						

LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12



NOTES:

1. ALL UTILITIES ARE SHOWN ON THE EXISTING & PROPOSED DRAINAGE & UTILITIES PLAN SHEETS.
2. STA. 115+64.27 OFF. 14.00' LT EX. ST. CLAIR LEFT SHOULDER TAPER FROM STA. 115+42.00 TO 115+64.27 TO BE PAVED AS PCC PAVEMENT (JOINTED) 10". SHOULDER BEYOND 115+64.27 TO BE HMA SHOULDER 8". SHOULDER BEFORE 155+42 TO BE PCC SHOULDER 10"
3. DELINEATORS NOT SHOWN FOR CLARITY. SEE SCHEDULE OF QUANTITIES FOR SPACING AND LOCATIONS.

ACCESS TO THE MESD FACILITY SHALL BE AVAILABLE AT ALL TIMES. IN ADDITION, THE CONTRACTOR SHALL GIVE TWO WEEKS NOTICE TO MESD PRIOR TO CONSTRUCTION ALONG LANDSDOWNE ST.

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	
		DRAWN - JB	REVISED -	
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -	

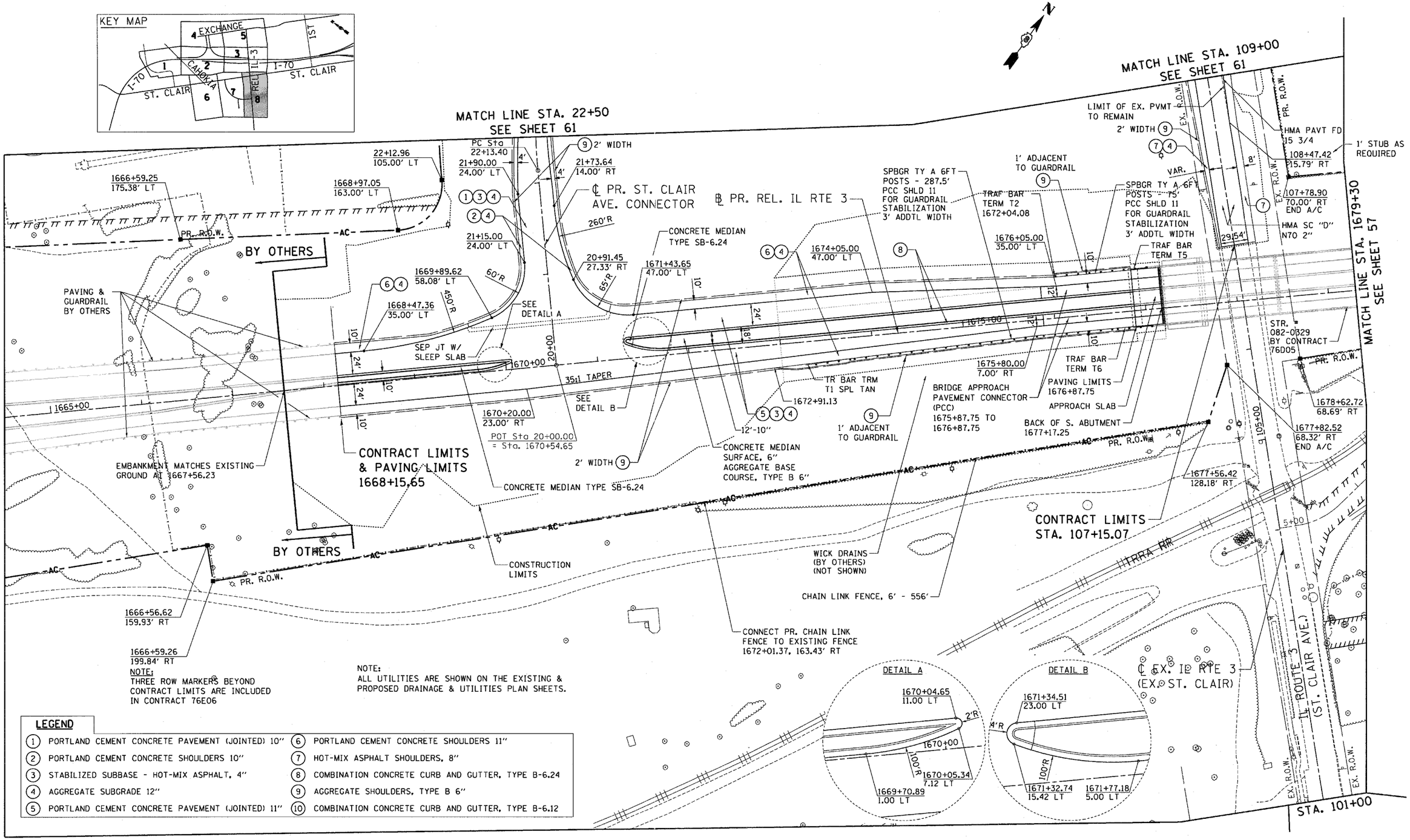
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED CONNECTOR ROAD AND EXISTING ST. CLAIR AVENUE
ROADWAY PLAN
STA. 109+00 TO STA. 117+00

SCALE: 1"=50' | SHEET NO. 7 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	61
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF.DGN,..\DBCONN-11-BORDER.DGN,..\DBCONN-11-MOTIF.DGN,..\DBCONN-11-LEGEND.DGN,..\DBCONN-11-LAYOUT.DGN,..\DBCONN-05-MOTIF.DGN,..\DBCONN-11-SHT-PLAN-10%NOR.DGN
11-04-2011, 16:25:46 BONCHAJO \\FS-0044\AM\WAULT-TRANS.07\202\20868-00\CIVIL\CAD\94 ALL CONTRACTS\CONNSHEETS CONTRACT 11\DBCONN-11-SHT-PLAN.DGN



LEGEND

1. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 10"
2. PORTLAND CEMENT CONCRETE SHOULDERS 10"
3. STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"
4. AGGREGATE SUBGRADE 12"
5. PORTLAND CEMENT CONCRETE PAVEMENT (JOINTED) 11"
6. PORTLAND CEMENT CONCRETE SHOULDERS 11"
7. HOT-MIX ASPHALT SHOULDERS, 8"
8. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
9. AGGREGATE SHOULDERS, TYPE B 6"
10. COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -
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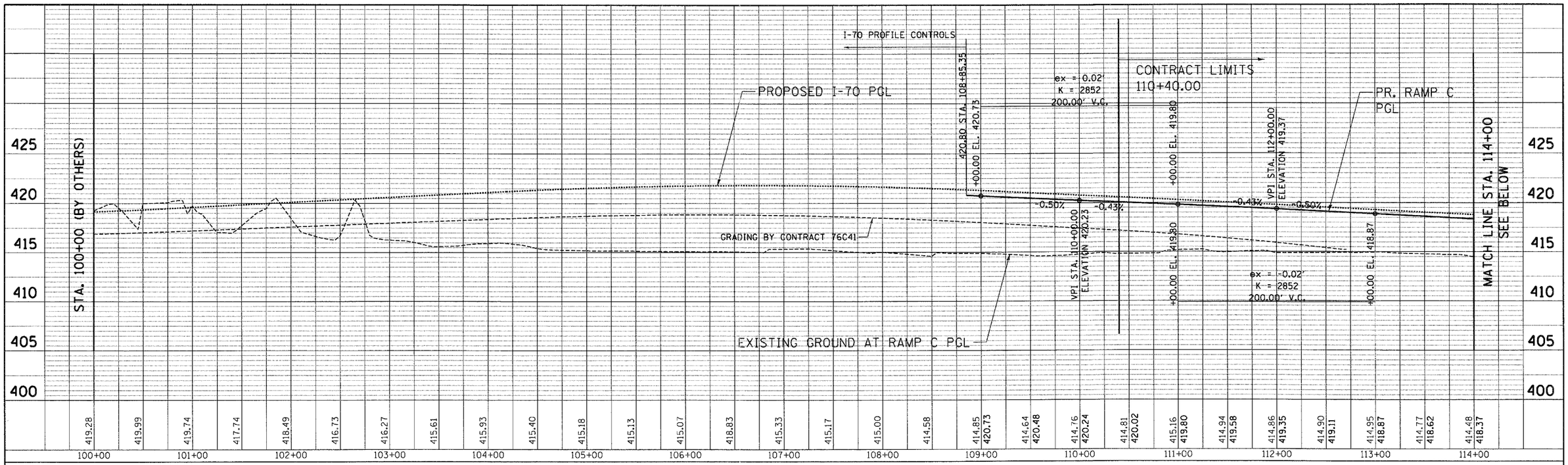
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

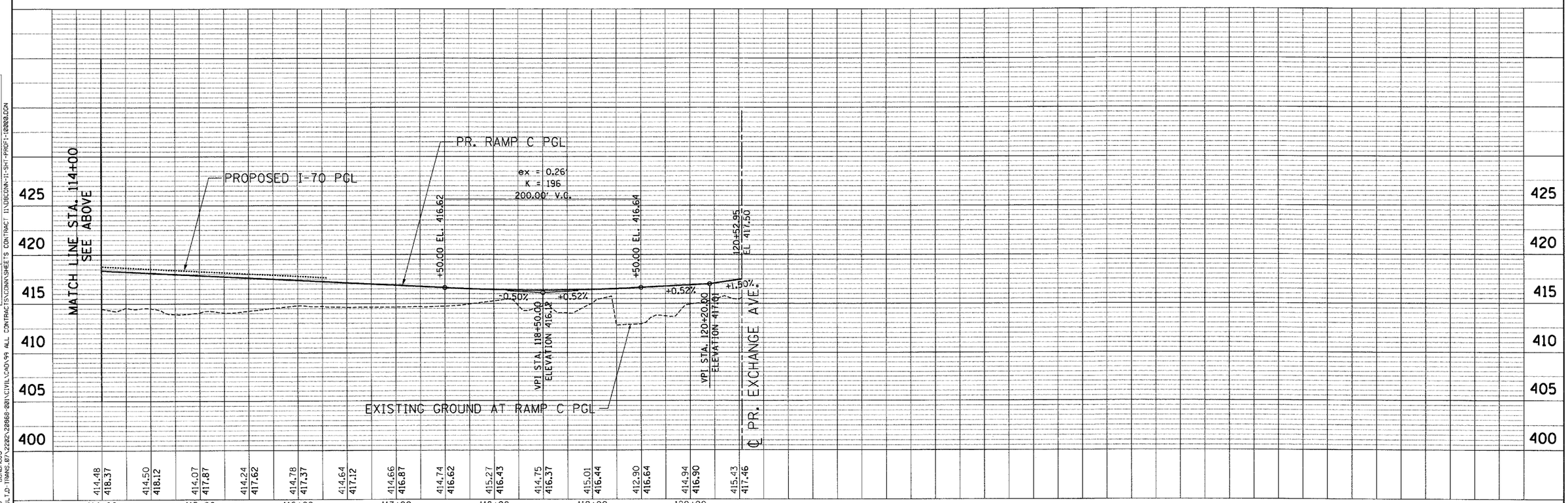
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RELOCATED ILLINOIS ROUTE 3
ROADWAY PLAN
STA. 1665+00 TO STA. 1679+30

SCALE: 1"=50' | SHEET NO. 8 OF 8 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	62
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		





PLAN	BY	DATE
SURVEYED		
PLOTTED		
ALIGNMENT CHECKED		
RT. OF WAY CHECKED		
BOND FILE MADE		
NOTE BOOK NO.		

PROFILE	BY	DATE
SURVEYED		
PLOTTED		
GRADES CHECKED		
B.M. NOTED		
STRUCTURE NOTATNS CK'RD		
NOTE BOOK NO.		

12-19-2011 14:43:25 BONDHUJO
\\FS-0244\AMVAULT_D-TRANS.07\2202\28868-801\CIVIL\CADD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\UBCONN-11-SHT-PROF1-12000.DGN

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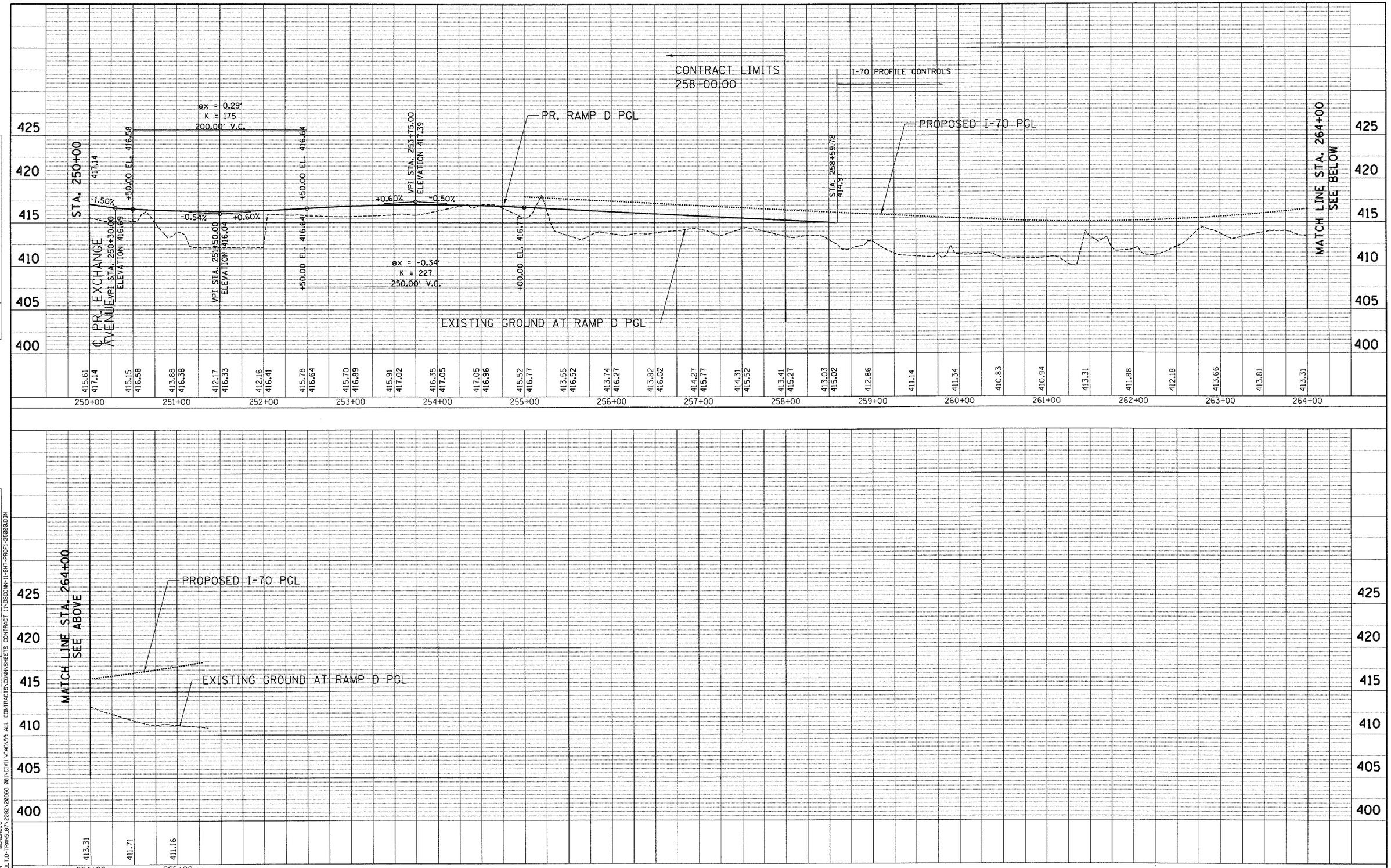
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RAMP C
ROADWAY PROFILE
STA. 100+00 TO STA. 120+53

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	63
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PLAN
NOTE BOOK NO. ____
SURVEYED
PLOTTED
ALIGNMENT CHECKED
RT. OF WAY CHECKED
CADD FILE NAME
BY
DATE

PROFILE
NOTE BOOK NO. ____
SURVEYED
PLOTTED
GRADES CHECKED
B.M. NOTED
STRUCTURE NOTATIONS CHKD
BY
DATE

10-19-2011 14:43:29 BONGHUJO
\\FS-0014\WM\VAULT_D-TRANS_07\2202\20066-001\CIVIL\CADD99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PROFI-25002.DGN

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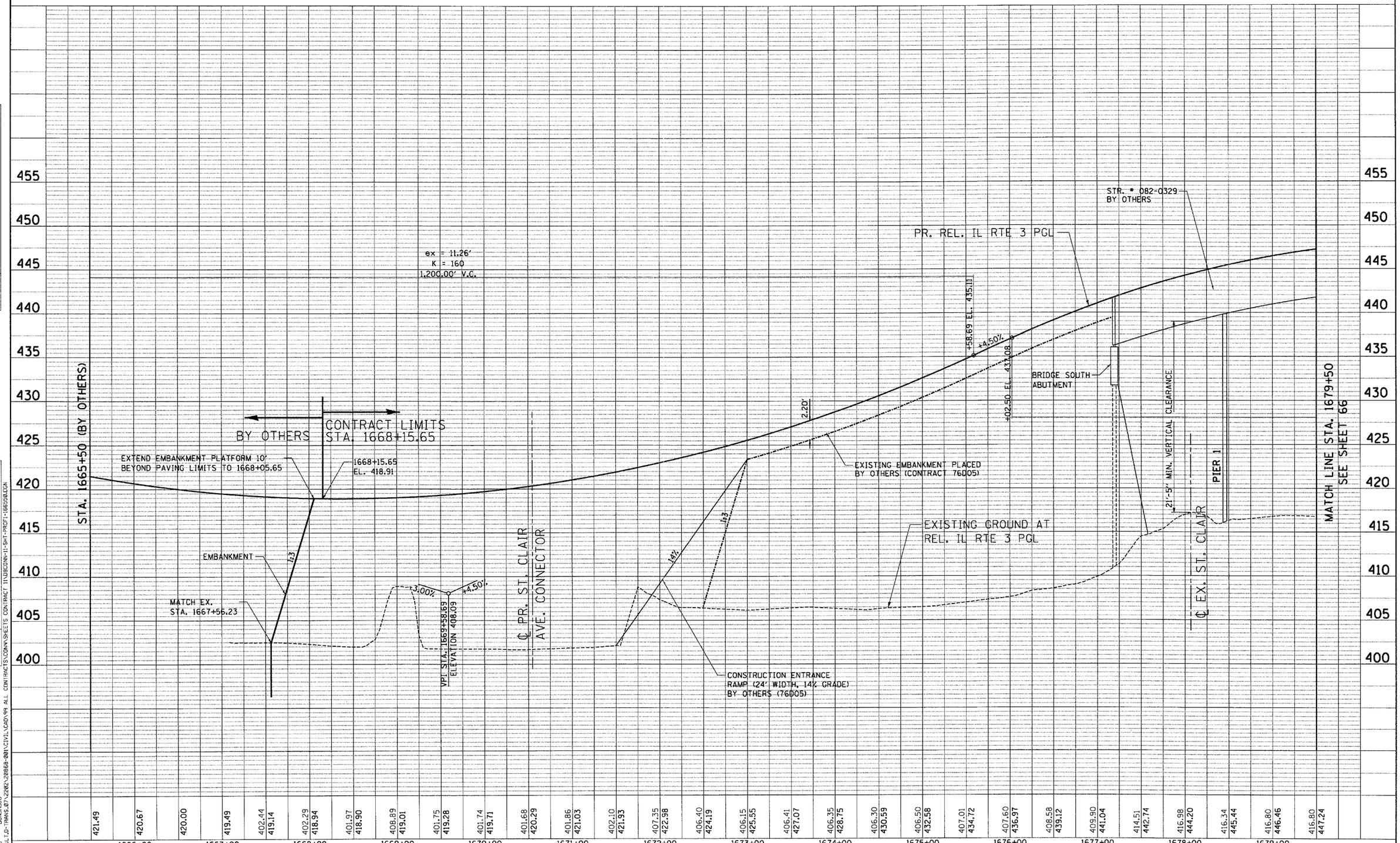
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RAMP D
ROADWAY PROFILE
STA. 250+00 TO STA. 265+56

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	64
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PLAN	BY	DATE
SURVEYED		
PLOTTED		
ALIGNMENT CHECKED		
RWY/TWY CHECKED		
CADD FILE NAME		
NOTE BOOK		
NO.		

PROFILE	BY	DATE
SURVEYED		
PLOTTED		
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RWY/TWY CHECKED		
STRUCTURE NOTATIONS CHKD		
NOTE BOOK		
NO.		

18-19-2011 11:43:33 BONDHLIO
\\FS-02\44\ARYVAU_TJD-TRANS.07\22022\28668-001\CIVIL\CADD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PROFI-166550LOCA

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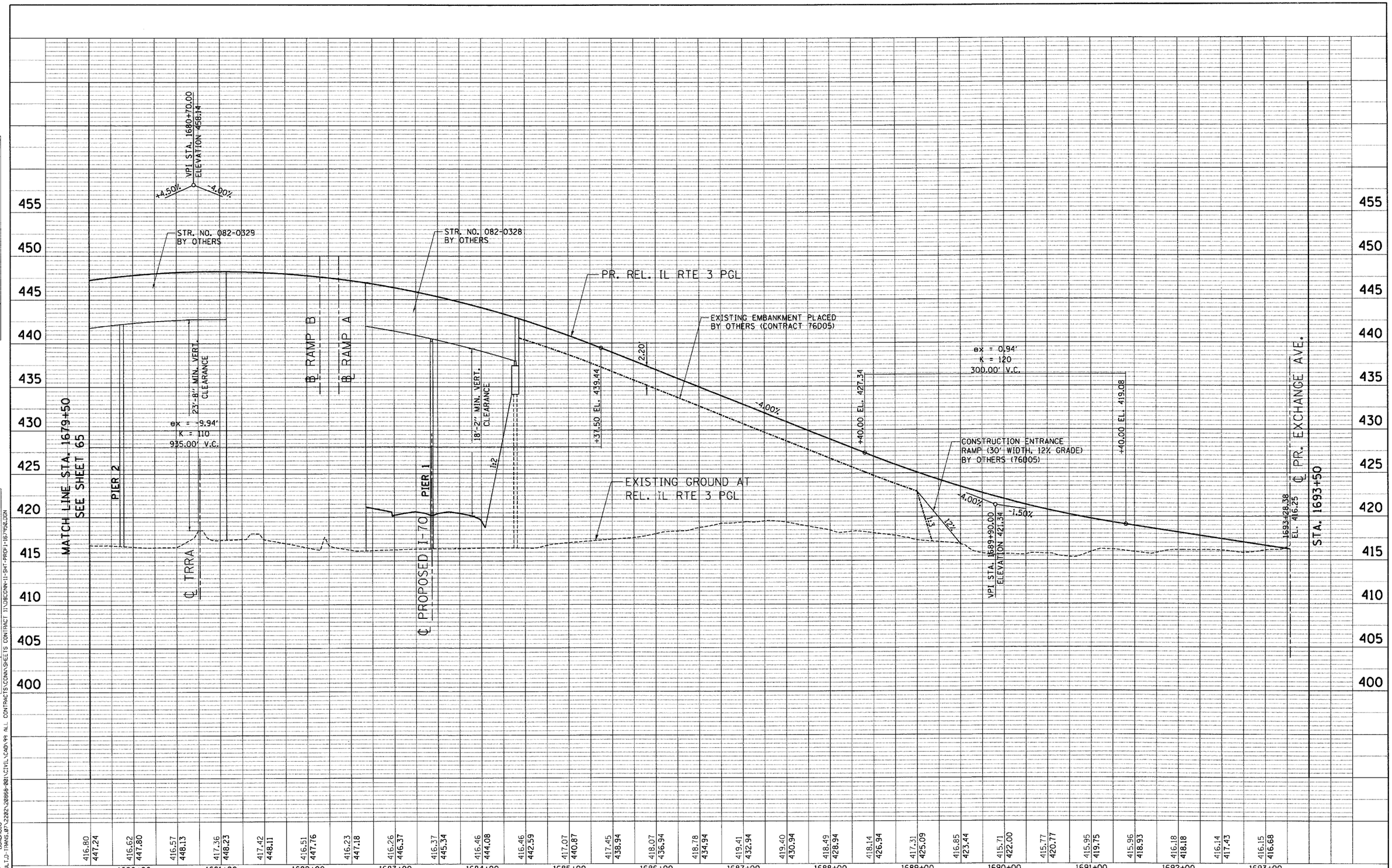
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RELOCATED ILLINOIS ROUTE 3
ROADWAY PROFILE
STA. 1665+50 TO STA. 1679+50
SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	65
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



PLAN

NOTE BOOK NO.	SURVEYED	PLOTTED	ALIGNMENT CHECKED	PLAN/PROFILE CHECKED	CADD FILE NAME	BY	DATE

PROFILE

NOTE BOOK NO.	SURVEYED	PLOTTED	GRADES CHECKED	ALIGNMENT CHECKED	STRUCTURE NOTAT'NS CHKD	BY	DATE

10-19-2011 14:13:27 BONDHELIO
\\FS-BRIA1\AMY\VAULT\D-TRANS.07\2282-20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\GBCONN-11-SHT-PROF-I-167PGL.DGN

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TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = #DATE#	DATE - 10/21/11	REVISED -

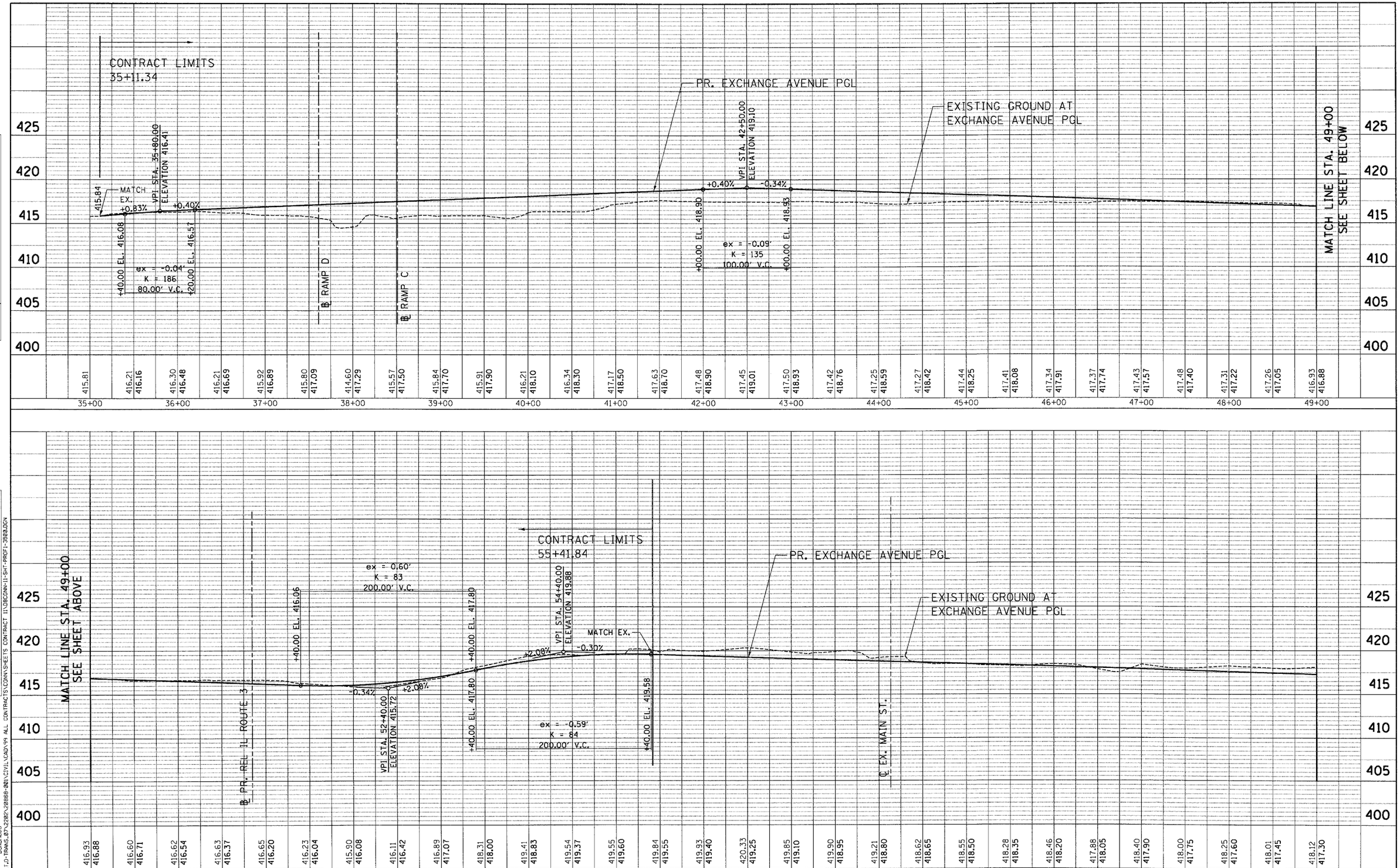
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RELOCATED ILLINOIS ROUTE 3
ROADWAY PROFILE
STA. 1679+50 TO STA. 1693+50

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	66
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



PLAN	BY	DATE
SURVEYED		
PLOTTED		
ALIGNMENT CHECKED		
RT. OF WAY CHECKED		
CADD FILE NAME		
NOTE BOOK		
NO.		

PROFILE	BY	DATE
SURVEYED		
PLOTTED		
GRADES CHECKED		
R.O.W. CHECKED		
STRUCTURE NOTATIONS CHKD		
NOTE BOOK		
NO.		

10-19-2011 14:43:41 BCNDHLLD
\\FS-02\41\WKVAL\TJD-TRANS\07\2202\20869-001\CIVIL\CADD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\09CONN-11-SHT-PROFI-3502.DGN

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TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
CHICAGO, ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED EXCHANGE AVENUE EXTENSION
ROADWAY PROFILE
STA. 35+00 TO STA. 63+00

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	67
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

PLAN
SURVEYED
PLOTTED
ALIGNMENT CHECKED
RELOCATION CHECKED
CADD FILE NAME
NOTE BOOK
NO.
BY
DATE

PROFILE
SURVEYED
PLOTTED
GRADES CHECKED
ALIGNMENT
STRUCTURE NBTAFRS CHKD
NOTE BOOK
NO.
BY
DATE

10-19-2011 14:43:45 RONDALD
\\FS-2014\AVAULT_D-TRANS.07\2222-20669-201\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-PROFI-2000.DGN

FILE NAME = #FILEL#	USER NAME = #USER#	DESIGNED - JB	REVISED -
		DRAWN - JB	REVISED -
	PLOT SCALE = #SCALE#	CHECKED - ACL	REVISED -
	PLOT DATE = #DATE#	DATE - 10/21/11	REVISED -

TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
CHICAGO, ILLINOIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

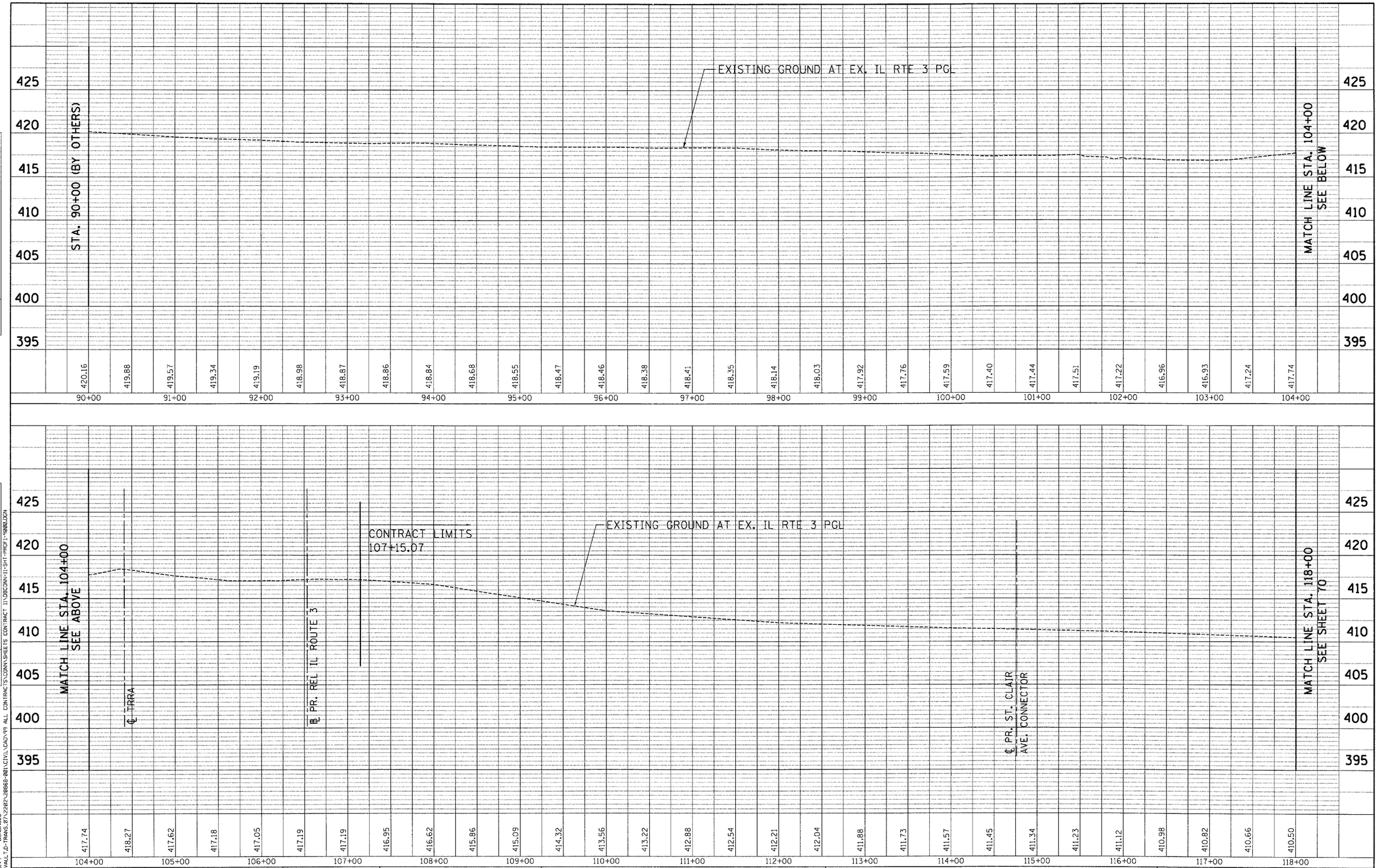
PROPOSED ST. CLAIR AVENUE CONNECTOR
ROADWAY PROFILE
STA. 20+00 TO STA. 32+51

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	68

CONTRACT NO. 76E06

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



PLAN	BY	DATE
SURVEYED		
PLOTTED		
ROUTINE HIGHWAY CHECKED		
R.O.W. CHECKED		
CADD FILE NAME		
NOTE BOOK NO.		

PROFILE	BY	DATE
SURVEYED		
PLOTTED		
GRADES CHECKED		
R.O.W. CHECKED		
STRUCTURE NOTAT'NS CHKD		
NOTE BOOK NO.		

12-19-2011 14:43:49 BONDHLJD
\\FS-0244\AM\VAULT_TD-TRANS.07\2202\20868-001\CIVIL\CAD\99 ALL CONTRACTS\CONTRACTS\SHEETS CONTRACT 11\08CONN-11-SHT-PROF1-9000.DGN

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TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS

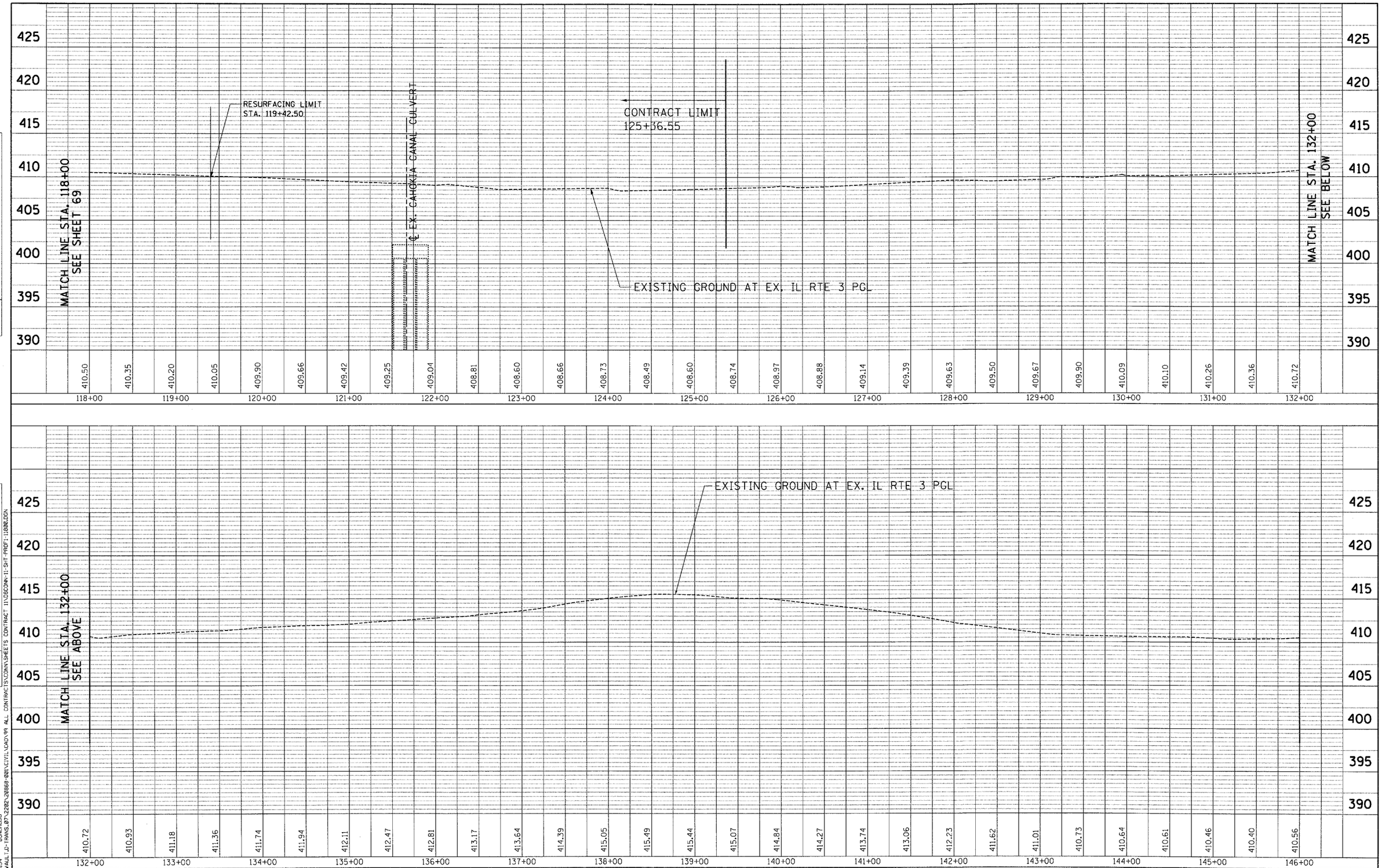
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

EXISTING ILLINOIS ROUTE 3 (ST. CLAIR AVENUE)
ROADWAY PROFILE
STA. 90+00 TO STA. 118+00

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	69
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



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SURVEYED
PLOTTED
ALIGNMENT CHECKED
GRADES CHECKED
S.M. NOTES CHECKED
NOTE BOOK
NO.
CADD FILE NAME
BY
DATE

PROFILE
SURVEYED
PLOTTED
GRADES CHECKED
S.M. NOTES NOTATNS CHKD
NOTE BOOK
NO.
BY
DATE

10-19-2011,14:43:54 BONDHUJD
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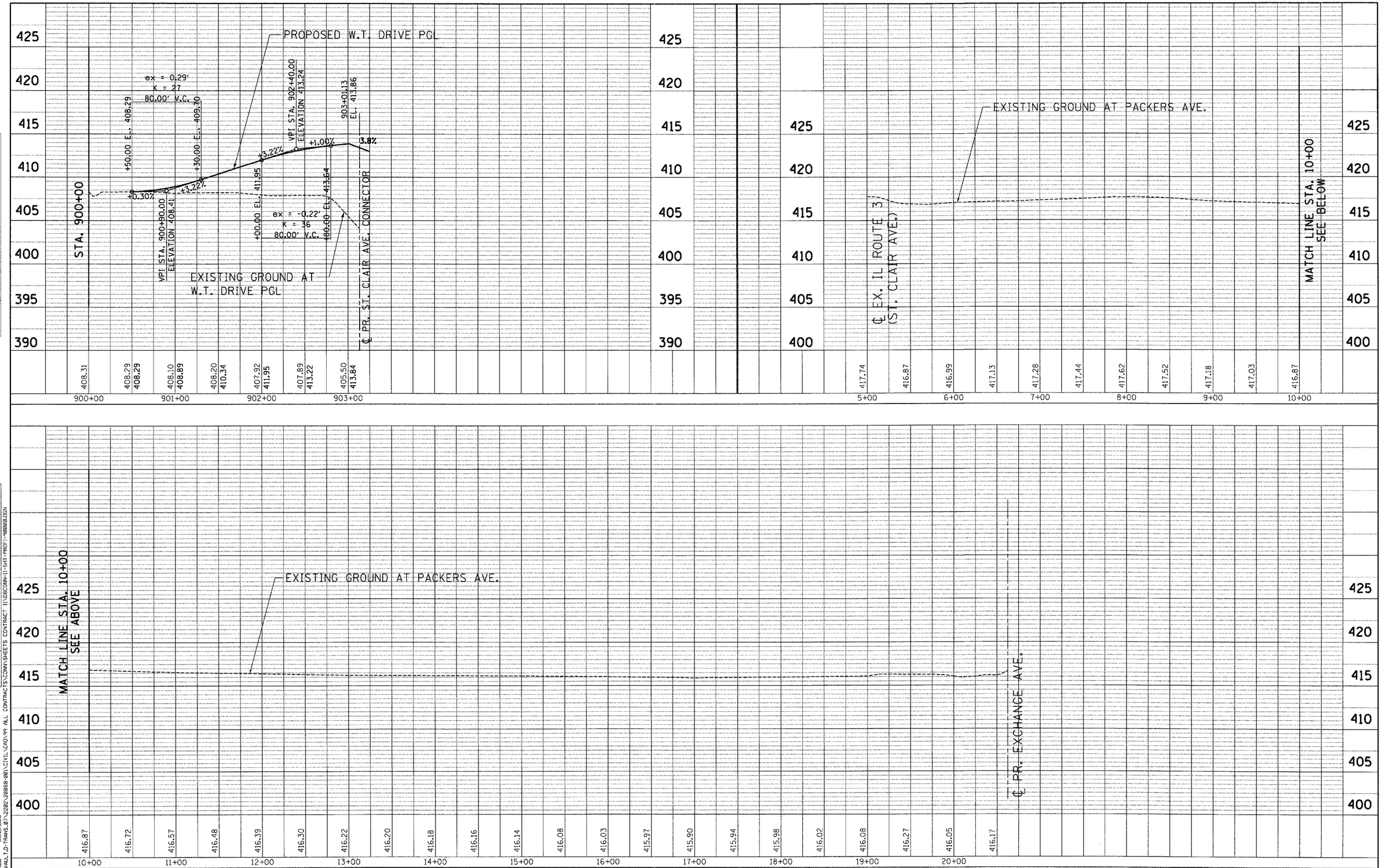
		DRAWN - JB	REVISED -		
	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -		
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	70
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



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PROFILE — SURVEYED / PLOTTED / GRADES CHECKED / R.O.W. CHECKED / STRUCTURE NOTATNS CHKD — NOTE BOOK NO. — BY — DATE

10-19-2011 14:44:00 RONCHELID
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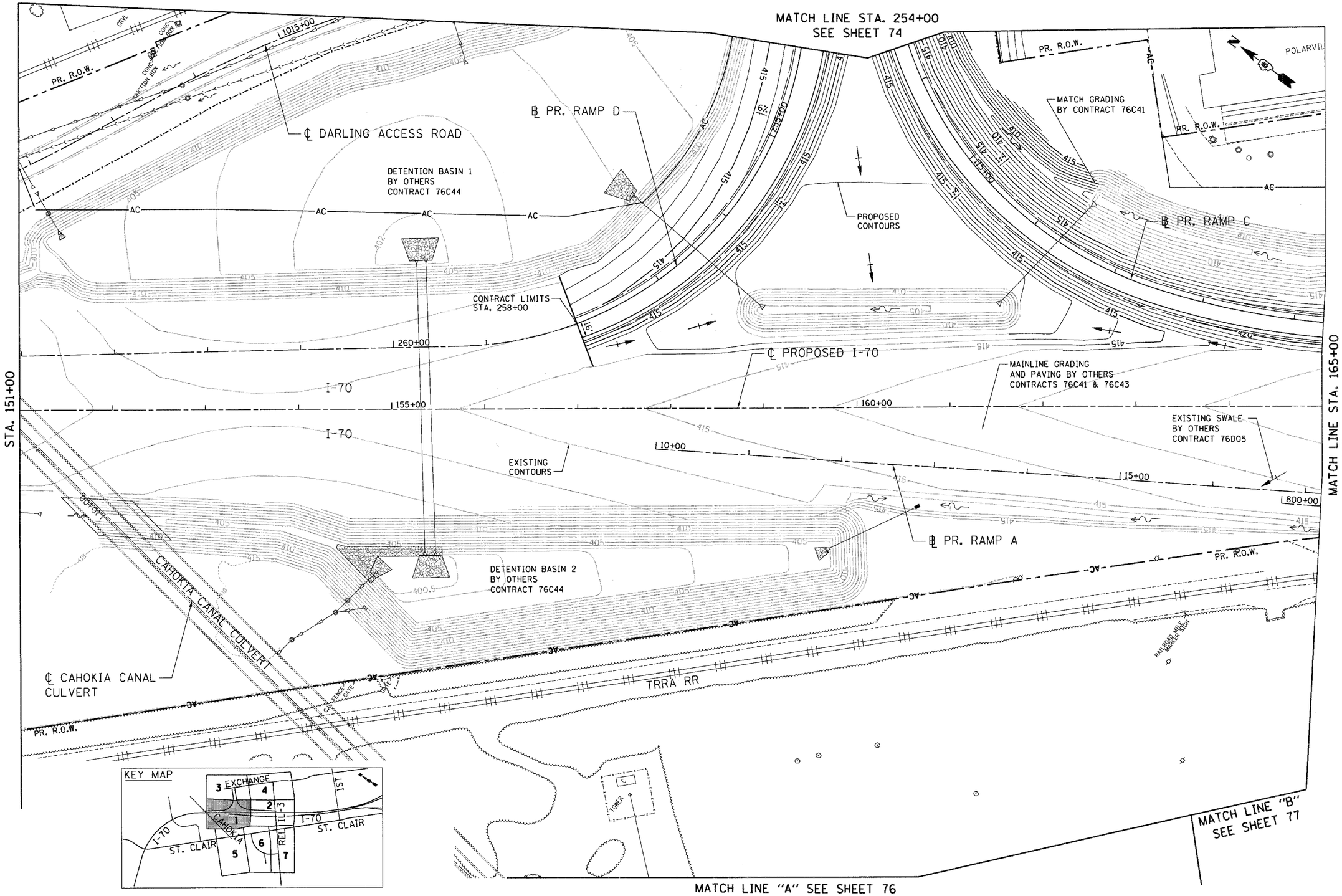
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED W.T. DRIVE AND EXISTING PACKERS AVENUE
ROADWAY PROFILE

SCALE: HORIZ. 1"=50' VERT. 1"=5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	71
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED I-70 GRADING PLAN STA. 151+00 TO STA. 165+00	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353	SHEET NO. 72
		DRAWN - JB	REVISED -							
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE				CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 1 OF 7 SHEETS STA. 151+00 TO STA. 165+00	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MATCH LINE STA. 1687+40
SEE SHEET 75

MATCH LINE STA. 165+00
SEE SHEET 72

STA. 179+00

℄ PR. REL.
IL ROUTE 3

PR. R.O.W.

EXISTING DITCH
BY OTHERS
CONTRACT 76C41

℄ PR. RAMP C

CONTRAC LIMITS
STA. 110+40.00

STR. # 082-0328
BY CONTRACT 76D05

MAINLINE GRADING AND
PAVING BY OTHERS
CONTRACTS 76C41 & 76C43

℄ PROPOSED I-70

EXISTING SWALE
BY OTHERS
CONTRACT 76D05

STR. # 082-W308
RETAINING WALL
BY CONTRACT 76D05

℄ PR. RAMP B

℄ PR. RAMP A

STR. # 082-W309
RETAINING WALL
BY CONTRACT 76D05

STR. #
082-0329
BY CONTRACT
76D05

PACKERS AVE.

IRRA RR

P.E.

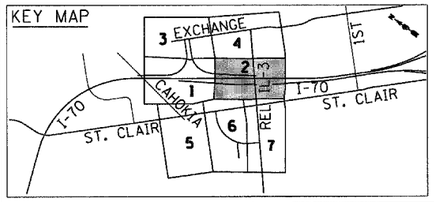
PROPOSED EASEMENT

110+00
105+00
100+00
165+00
170+00
175+00
20+00
25+00
50+00
55+00
805+00
810+00
1680+00

MATCH LINE "C"
SEE SHEET 77

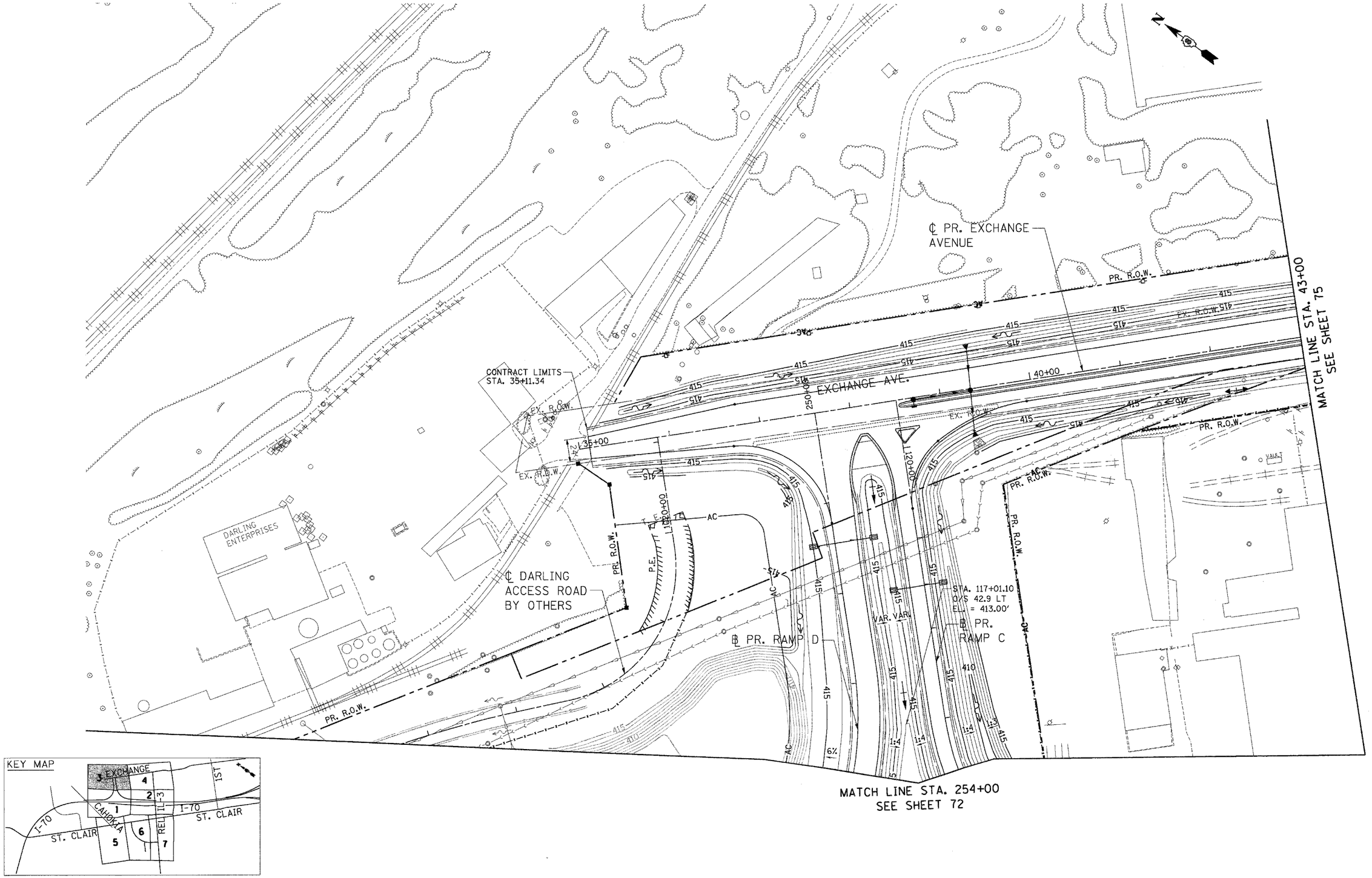
MATCH LINE STA. 1679+30.00
SEE SHEET 78

KEY MAP

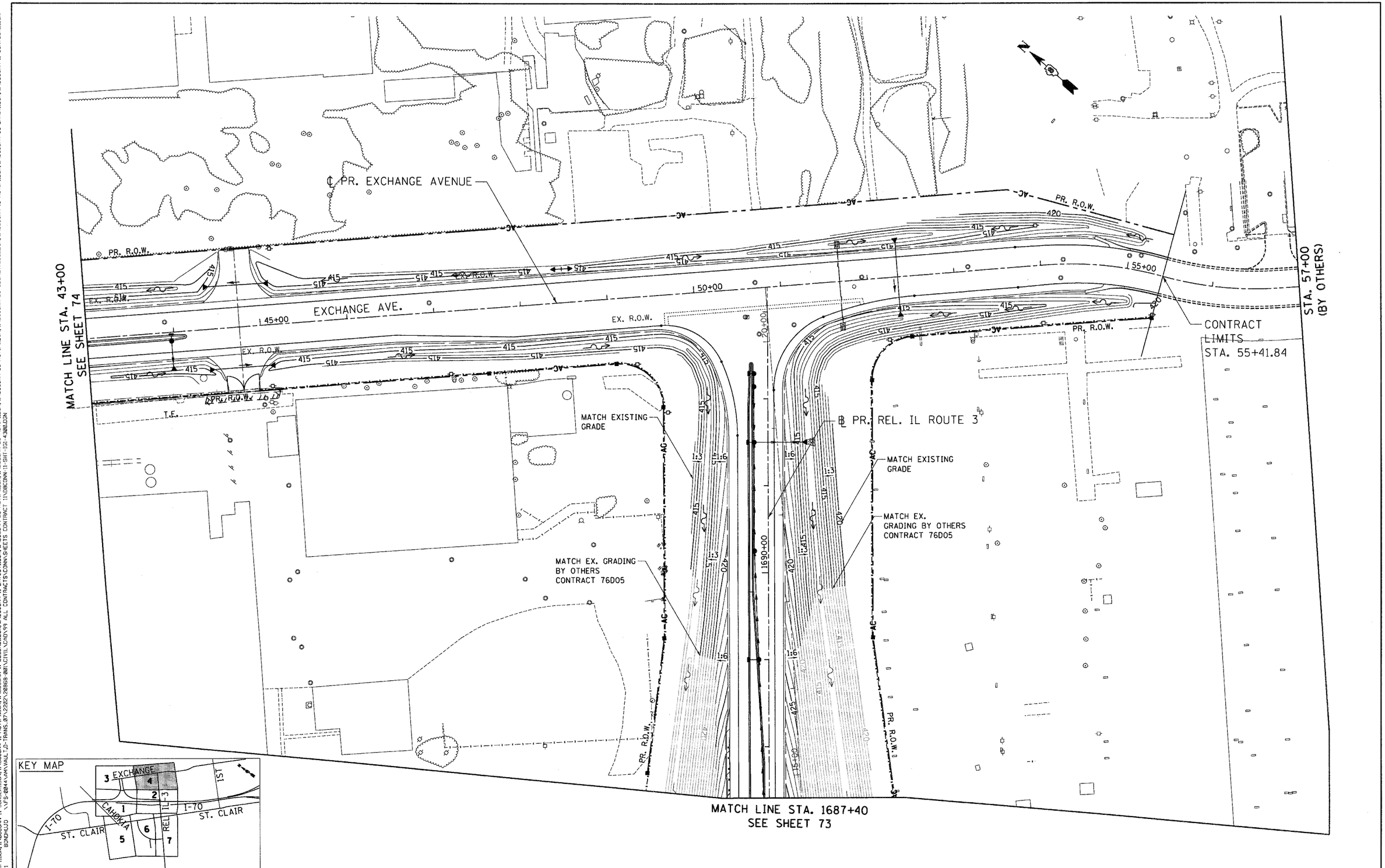


FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	PROPOSED I-70 GRADING PLAN STA. 165+00 TO STA. 179+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	73
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -			CONTRACT NO. 76E06				
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 2 OF 7 SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\\DBCONN-99-MOTIF1.DGN, \\DBCONN-11-MOTIF1.DGN, \\DBCONN-11-BORDER1.DGN, \\DBCONN-11-LEGEND1.DGN, \\DBCONN-11-LAYOUT1.DGN, \\DBCONN-05-MOTIF1.DGN, \\DBCONN-06-MOTIF1.DGN, \\DBCONN-10-MOTIF1.DGN, \\DBCONN-06-DRAIN1.DGN, \\DBCONN-11-CONTOUR2.DGN
10-19-2011, 14:45:18 BONDPALLO \\FS-9004-04\MVIVAL1.LD-TRANS.BY\22202\208869-001\CTL\CAD-99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SH1 IGI-16SPACION



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED EXCHANGE AVENUE GRADING PLAN STA. 35+00 TO STA. 43+00	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353	SHEET NO. 74
TENG TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	DRAWN - JB	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE				CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	CHECKED - ACL	REVISED -		SCALE: 1"=50' SHEET NO. 3 OF 7 SHEETS STA. 35+00 TO STA. 43+00	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		DATE - 10/21/11	REVISED -							



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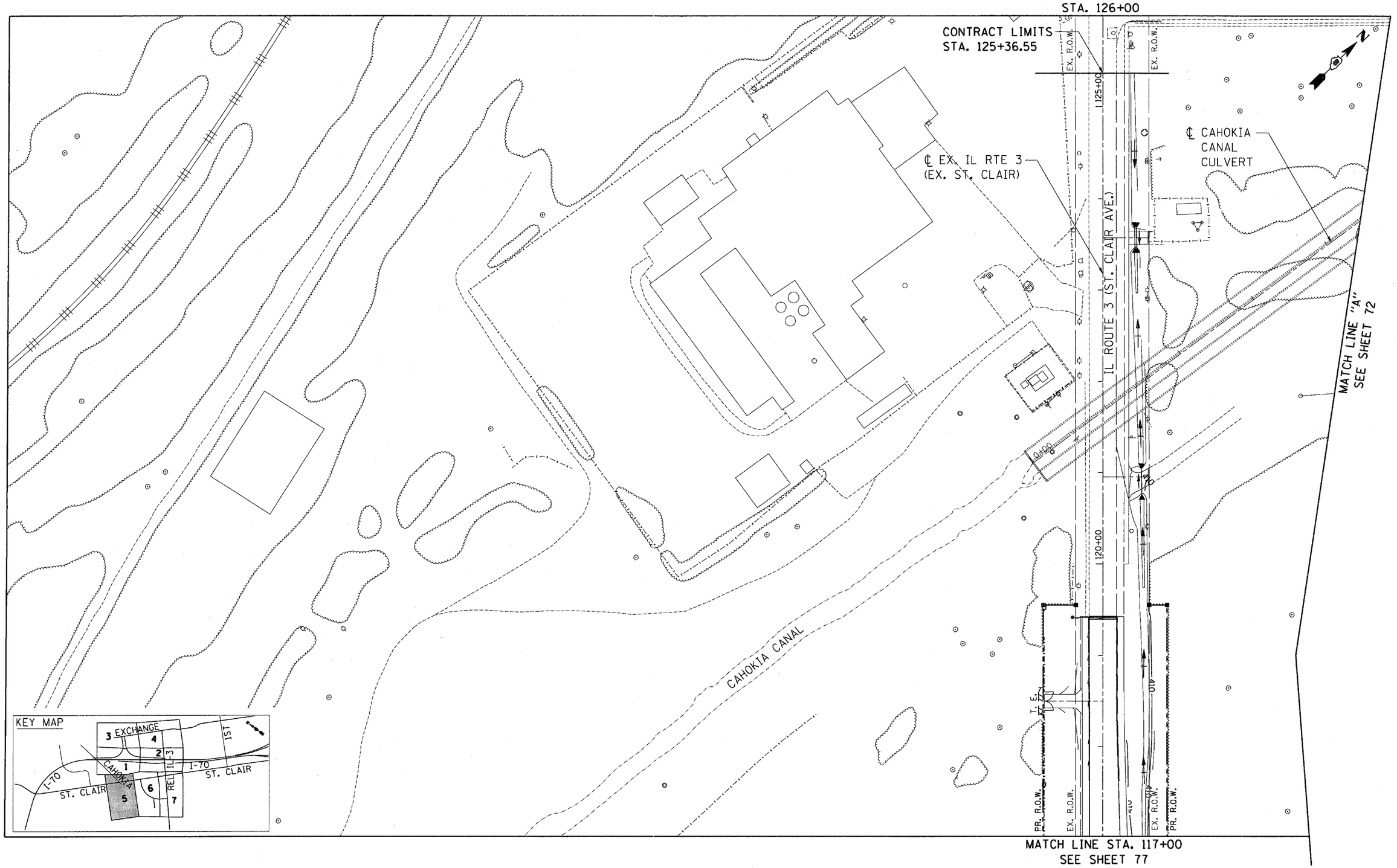
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED EXCHANGE AVENUE
GRADING PLAN
STA. 43+00 TO STA. 57+00

SCALE: 1"=50' | SHEET NO. 4 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	75
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION** PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	**EXISTING ST. CLAIR AVENUE GRADING PLAN STA. 117+00 TO 126+00**	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - JB	REVISED -			998	82-2-1K	ST. CLAIR	353	76
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	CHECKED - ACL	REVISED -					CONTRACT NO. 76E06		
	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 5 OF 7 SHEETS / STA. 117+00 TO STA. 126+00	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN, ..\DBCONN-11-BORDER.DGN, ..\DBCONN-11-MOTIF1.DGN, ..\DBCONN-11-LEGEND.DGN, ..\DBCONN-11-LAYOUT1.DGN, ..\DBCONN-05-MOTIF1.DGN, ..\DBCONN-06-MOTIF1.DGN, ..\DBCONN-08-MOTIF1.DGN, ..\DBCONN-10-MOTIF1.DGN, ..\DBCONN-11-CONTOUR2.DGN
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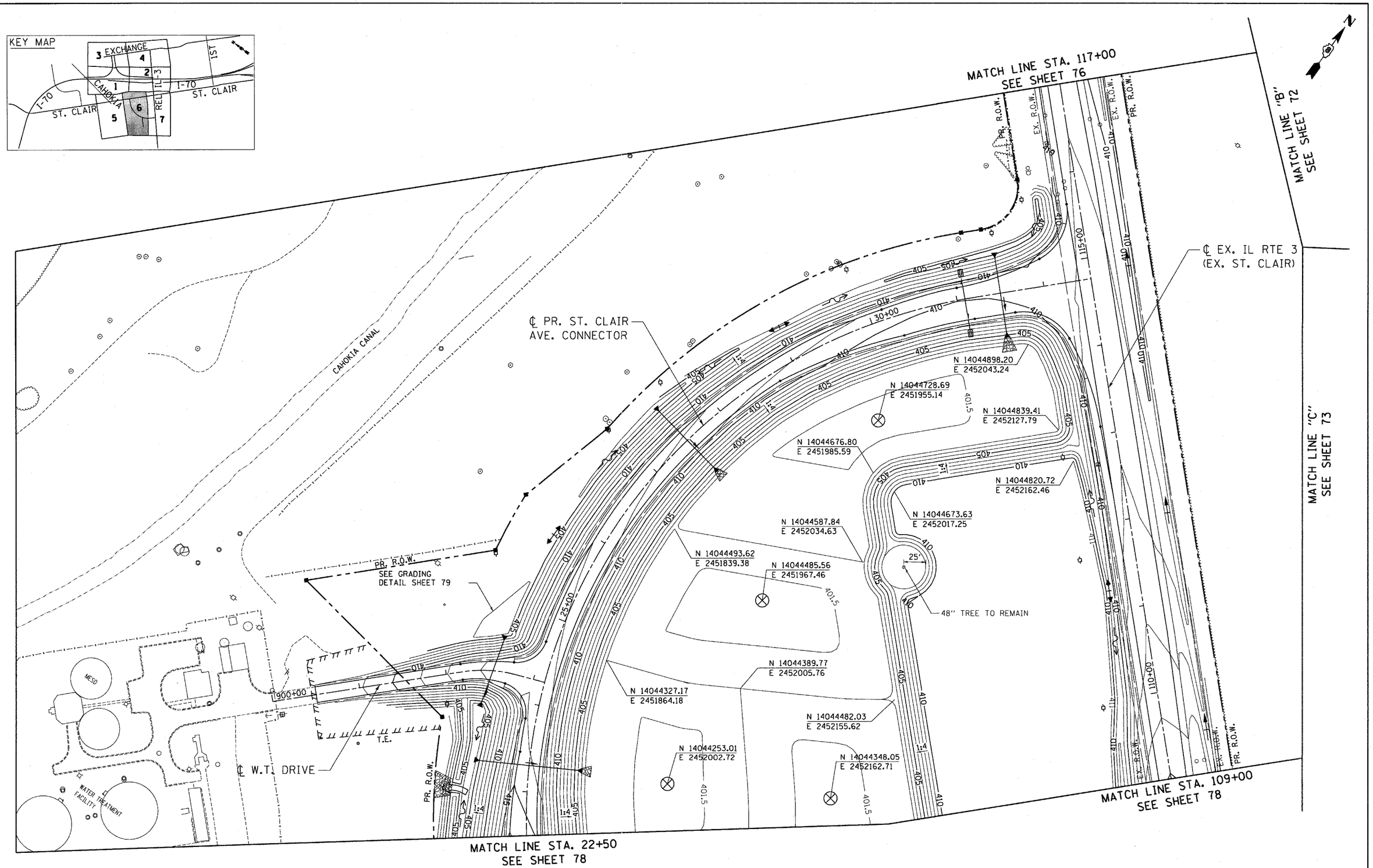
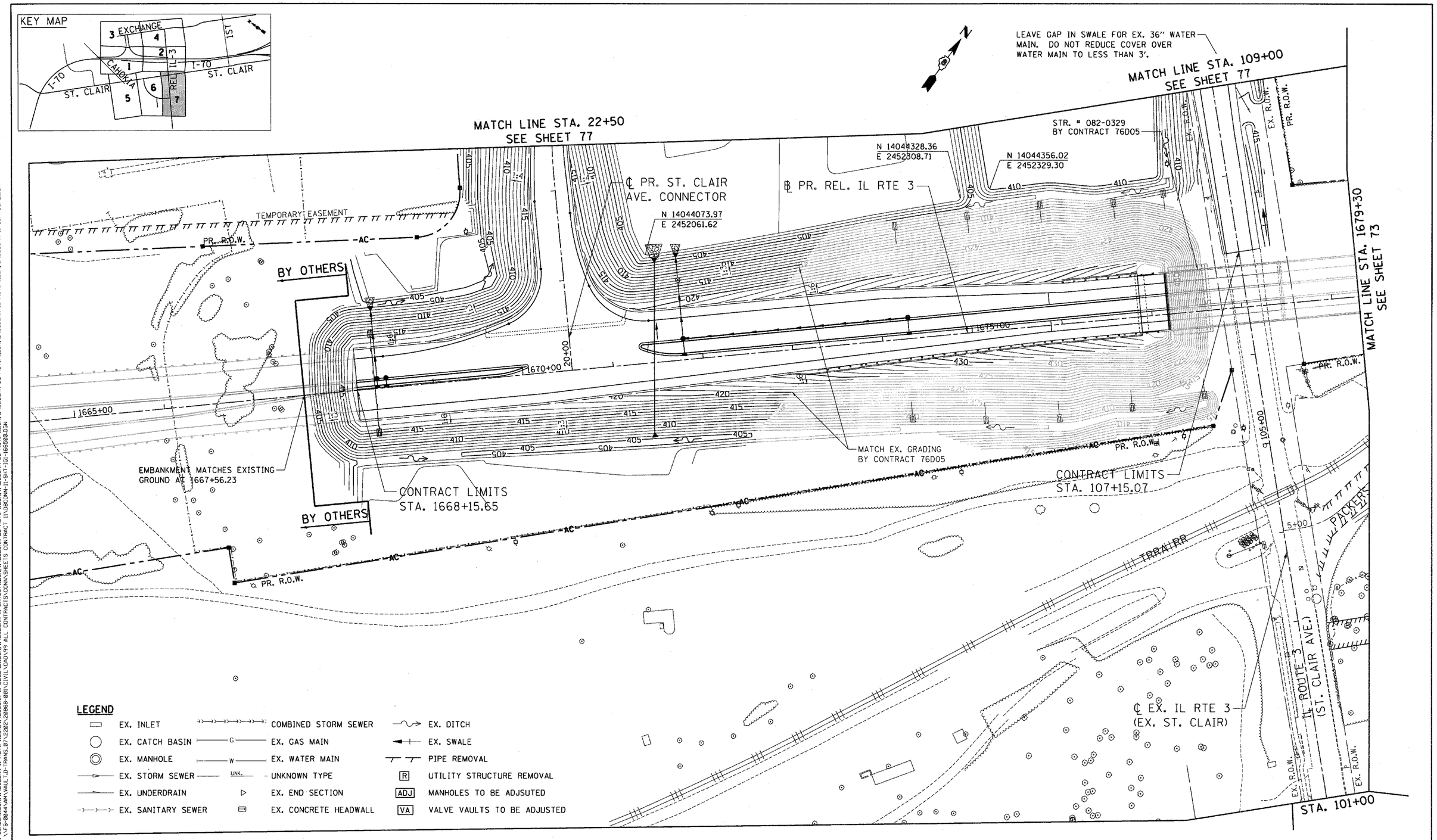


FILE NAME = $FILEL$	USER NAME = $USER$	DESIGNED - JB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED CONNECTOR ROAD AND EXISTING ST. CLAIR AVENUE GRADING PLAN STA. 109+00 TO STA. 117+00	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353 / SHEET NO. 77
TENG & ASSOCIATES, INC. ENGINEERS/ARCHITECTS/PLANNERS CHICAGO, ILLINOIS	PLOT SCALE = $SCALE$	DRAWN - JB	REVISED -	PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE	SCALE: 1"=50' / SHEET NO. 6 OF 7 SHEETS / STA. 109+00 TO STA. 117+00			CONTRACT NO. 76E06	
	PLOT DATE = $DATE$	CHECKED - ACL	REVISED -			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	
		DATE - 10/21/11	REVISED -						



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	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED RELOCATED ILLINOIS ROUTE 3
GRADING PLAN
STA. 1665+00 TO STA. 1679+30

SCALE: 1"=50' | SHEET NO. 7 OF 7 SHEETS | STA. 1665+00 TO STA. 1679+30

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	78
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN, ..\DBCONN-11-BORDER1.DGN, ..\DBCONN-11-MOTIF1.DGN, ..\DBCONN-11-LEGEND1.DGN, ..\DBCONN-11-LAYOUT1.DGN, ..\DBCONN-05-MOTIF1.DGN, ..\DBCONN-08-MOTIF1.DGN, ..\DBCONN-10-MOTIF1.DGN, ..\DBCONN-11-CONTOUR2.DGN
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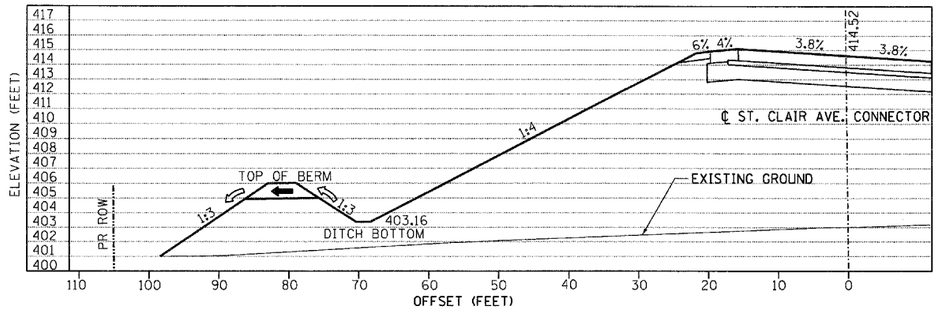

SECTION A-A
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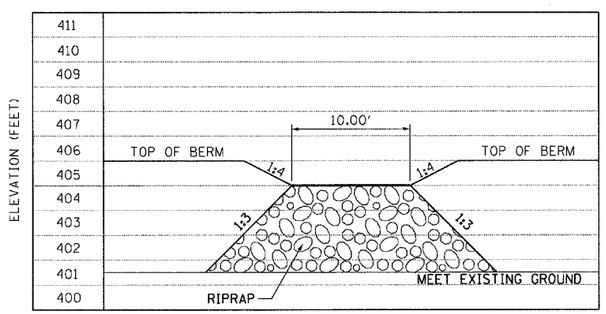
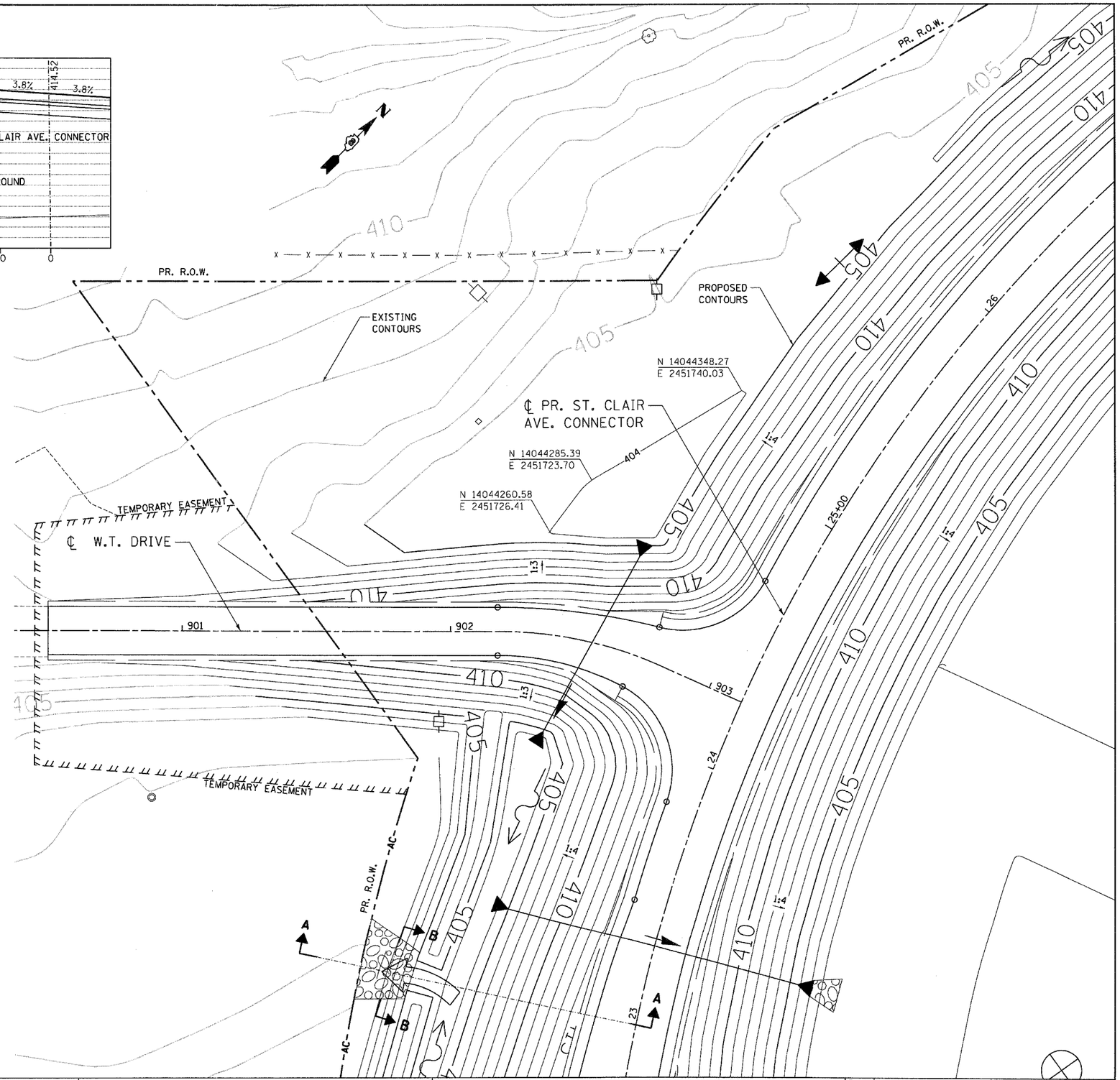


SECTION B-B
NOT TO SCALE



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	PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PROPOSED ST. CLAIR AVENUE CONNECTOR
GRADING PLANS
W.T. DRIVE DETAIL

SCALE: 1"=20' | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	79
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

..\DBCONN-99-MOTIF1.DGN,..\DBCONN-06-MOTIF1.DGN,..\DBCONN-08-MOTIF1.DGN,..\DBCONN-10-MOTIF1.DGN,..\DBCONN-11-MOTIF1.DGN,..\DBCONN-11-LEGEND1.DGN,..\DBCONN-11-LAYOUT1.DGN,..\DBCONN-05-MOTIF1.DGN,..\DBCONN-11-CONTOUR1.DGN,..\DBCONN-11-CONTOUR2.DGN,..\DBCONN-11-CONTOUR3.DGN
10-19-2011,14:48:34 BONDHUJD \\FS-0044\AM\VAULT\D-TRANS_07\2202\20868-00\CIVIL\CAD\99 ALL CONTRACTS\CONN\SHEETS CONTRACT 11\DBCONN-11-SHT-TG2-2500A.DGN

SUGGESTED MAINTENANCE OF TRAFFIC - PR. EXCHANGE AVE., PR. REL. IL RTE. 3 AND RAMPS C AND D

PRESTAGE

1. MAINTAIN DETOUR SIGNING FOR PACKERS AVE. FROM CONTRACT 76C41 AT COMPLETION OF CONTRACT 76C41.
2. CONSTRUCT TEMPORARY PAVEMENT ON THE NORTHBOUND SIDE OF EX. EXCHANGE AVE. FROM STA 39+51.00 TO STA 41+50.00 AS SHOWN IN STAGE 1 PLANS.
3. PERFORM CLEARING, TREE REMOVAL AND CONSTRUCTION OF REL. IL RTE. 3 EMBANKMENT.

STAGE 1

1. CONSTRUCT NORTHBOUND OUTSIDE LANE (14') AND SHOULDER (8') ON EXCHANGE AVE. FROM STA 41+50.00 TO STA 55+41.84 AS SHOWN IN PLANS. ACCESS TO ALL DRIVEWAYS MUST BE MAINTAINED AT ALL TIMES.

STAGE 2

1. SHIFT TRAFFIC FLOW TO EXCHANGE AVE.'S NORTHBOUND OUTSIDE LANE AND SHOULDER. PROVIDE PAVEMENT WEDGES AS NECESSARY PER ART. 701.07 OF THE STANDARD SPECIFICATIONS. THIS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT HOT-MIX ASPHALT PAY ITEMS.
2. CONSTRUCT EXCHANGE AVE.'S NORTHBOUND INSIDE LANE, MEDIAN, AND ENTIRE SOUTHBOUND AREA FROM STA 37+00.00 TO STA 55+41.84 AS SHOWN IN PLANS. PROVIDE ACCESS TO ONE OF THE TWO ENTRANCES TO POLARVILLE AT ALL TIMES.
3. CONSTRUCT RAMP C AND RAMP D TO CONTRACT LIMITS. CONSTRUCT DITCH PRIOR TO RAMP C/D EMBANKMENT TO KEEP RUNOFF WITHIN R.O.W.
4. CONSTRUCT REL. IL RTE. 3 FROM STA 1684+40.00 TO STA 1692+92.00 INCLUDING APPROACH PAVEMENT.

STAGE 3

1. CONSTRUCT EXCHANGE AVE. FROM STA 35+11.34 TO STA 37+00.00 PER. STD. 701501-06. ACCESS TO DARLING ENTERPRISES AND OTHER EXISTING DRIVEWAYS MUST BE MAINTAINED AT ALL TIMES.
2. REMOVE TEMPORARY PAVEMENT ON EXCHANGE AVE. FROM STA 39+51.00 TO STA 41+50.00 AND CONSTRUCT NORTHBOUND OUTSIDE LANE (14') AND SHOULDER (8') FROM STA 37+00.00 TO STA 41+50.00 AS SHOWN IN PLANS.
3. PLACE PROPOSED PAVEMENT MARKINGS ALONG EXCHANGE AVE., RAMPS C & D AND REL. IL RTE. 3 UNDER THE HIGHWAY STD. 701311-03.

GENERAL NOTES:

1. THE CONTRACTOR SHALL PROVIDE ALL SIGNS, BARRICADES, DRUMS AND NECESSARY TRAFFIC CONTROL DEVICES AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE INSTALLED TO THE SATISFACTION OF THE ENGINEER PRIOR TO THE START OF CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL COORDINATE THE MAINTENANCE OF TRAFFIC WITH THE ADJACENT CONTRACTS INCLUDING BUT NOT LIMITED TO CONTRACT 76C41, 76C43, 76C44 AND 76D05.
2. "AGGREGATE FOR TEMPORARY ACCESS" (12" THICKNESS) TO BE USED AS NECESSARY FOR ACCESS TO DARLING ENTERPRISES, POLARVILLE, WATER TREATMENT PLANT AND ALL OTHER EXISTING DRIVEWAYS. ANY OTHER NECESSARY BUILD UP BELOW THE AGGREGATE SHOULD BE COVERED FROM ONSITE EMBANKMENT.

SUGGESTED MAINTENANCE OF TRAFFIC - PR. ST. CLAIR AVE. CONNECTOR, PR. REL. IL RTE. 3 AND EX. IL RTE. 3 (ST. CLAIR AVE.)

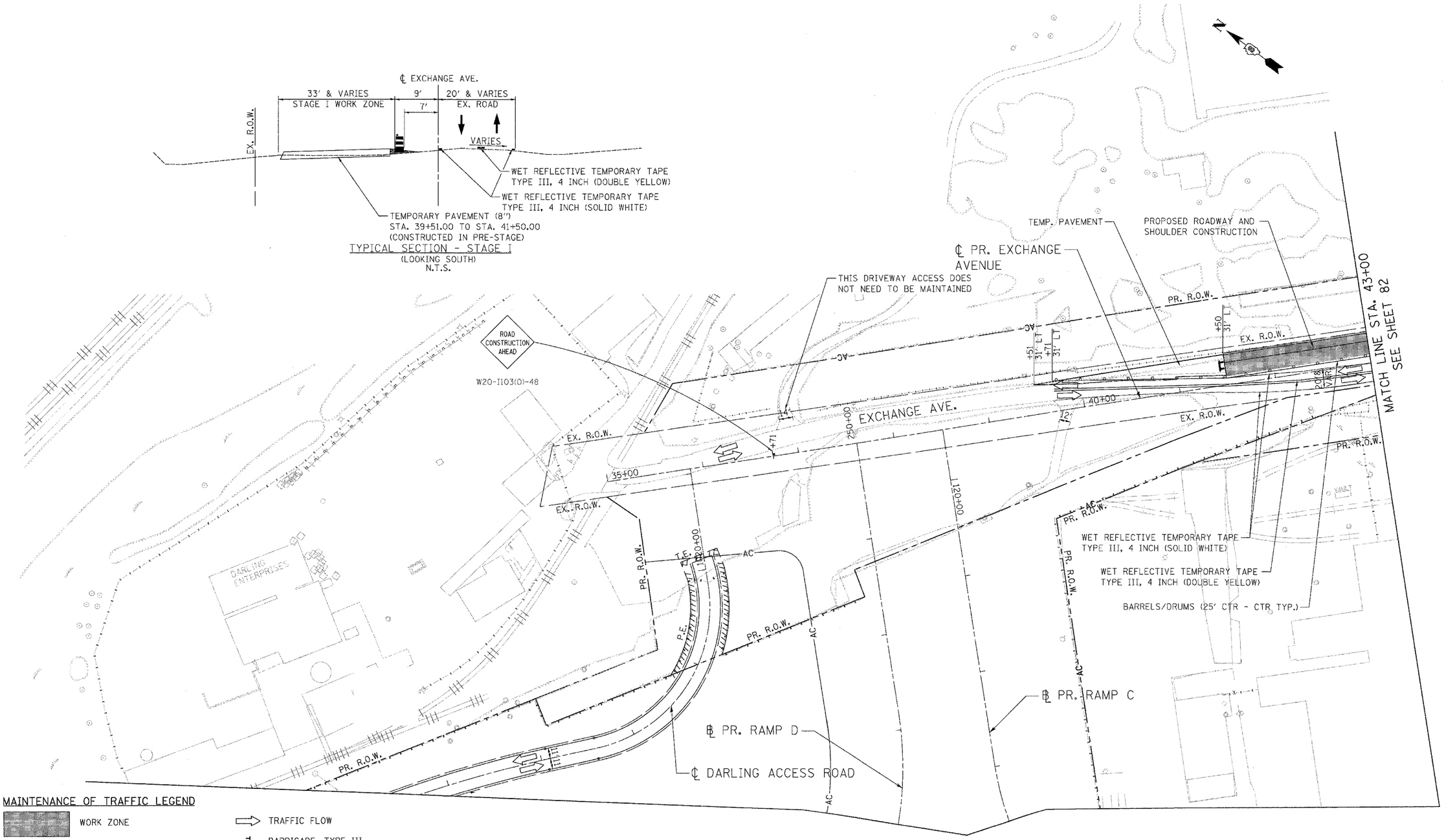
STAGE 1

1. CONSTRUCT DITCH REGRADING FOR WEST HALF OF EXISTING IL RTE. 3 FROM STA 107+10.00 TO STA 116+00.00 UNDER STD. 701006-03. IN ADDITION, EXCAVATE PROPOSED RETENTION BASIN TO PROVIDE SITE DRAINAGE. DO NOT REMOVE LANDSDOWNE STREET UNTIL TEMPORARY ACCESS TO THE WATER TREATMENT PLANT IS AVAILABLE.
2. CONSTRUCT WEST HALF OF EXISTING IL RTE. 3 FROM STA 107+15.07 TO STA 119+42.50 UNDER STD. 701306-03. THIS CONSISTS OF MILLING, RESURFACING, WIDENING AND SHOULDER CONSTRUCTION.
3. INSTALL PROPOSED TEMPORARY CULVERT UNDER THE TEMPORARY AGGREGATE ACCESS ROAD FOR THE WATER TREATMENT PLANT.
4. INSTALL PROPOSED CULVERTS TO BE PLACED UNDER THE PR. ST. CLAIR AVE. CONNECTOR, WT DRIVE AND LANDSDOWNE STREET.
5. CONSTRUCT TEMPORARY AGGREGATE ACCESS ROAD FOR THE WATER TREATMENT PLANT. (MAXIMUM PROFILE SLOPE 10%)
6. CONSTRUCT THE EARTHWORK FOR THE PR. ST. CLAIR AVE. CONNECTOR.
7. TEMPORARY AGGREGATE ACCESS ROAD SHOULD BE KEPT LEVEL WITH THE EARTHWORK FOR THE PR. ST. CLAIR AVE. CONNECTOR.
8. PLACE AGGREGATE SUBGRADE FOR PR. ST. CLAIR AVE. CONNECTOR AND PROVIDE ACCESS TO THE TRAFFIC FROM WATER TREATMENT PLANT THROUGH THE PR. ST. CLAIR AVE. CONNECTOR TO THE EX. IL RTE. 3.
9. REMOVE LANDSDOWNE STREET AND THE TEMPORARY AGGREGATE ACCESS ROAD AND TEMPORARY CULVERT EAST OF THE PR. ST. CLAIR AVE. CONNECTOR.
10. CONSTRUCT PR. ST. CLAIR AVE. CONNECTOR, WT DRIVE AND DRIVEWAY AT STA 111+45.62 ALONG EX. IL RTE. 3 SUCH THAT ACCESS TO THE WATER TREATMENT PLANT IS MAINTAINED AT ALL TIMES.
11. CONSTRUCT REL. IL RTE. 3 FROM STA 1668+15.65 TO STA 1676+87.75 INCLUDING APPROACH PAVEMENT.
12. PLACE ROAD CLOSED SIGNS WITH TYPE III BARRICADES ON REL. IL RTE. 3 AT STATION 1670+00.00 TO PREVENT TRAFFIC FROM DRIVING WEST ON REL. IL RTE. 3.

STAGE 2

1. CONSTRUCT DITCH REGRADING FOR EAST HALF OF EXISTING IL RTE. 3 FROM STA 107+10.00 TO STA 116+00.00 UNDER STD. 701006-03.
2. CONSTRUCT EAST HALF OF EXISTING IL RTE. 3 FROM STA 107+15.07 TO STA 125+36.55 UNDER STD. 701306-03. THIS CONSISTS OF MILLING, RESURFACING, WIDENING AND SHOULDER CONSTRUCTION.
3. PLACE PROPOSED PAVEMENT MARKINGS ALONG EX. ST. CLAIR AVE. & CONNECTOR, RAMPS A & B AND REMAINDER OF PR. REL. IL. RTE. 3 UNDER THE HIGHWAY STD. 701311-03.

TYPICAL SECTION - STAGE I
(LOOKING SOUTH)
N.T.S.

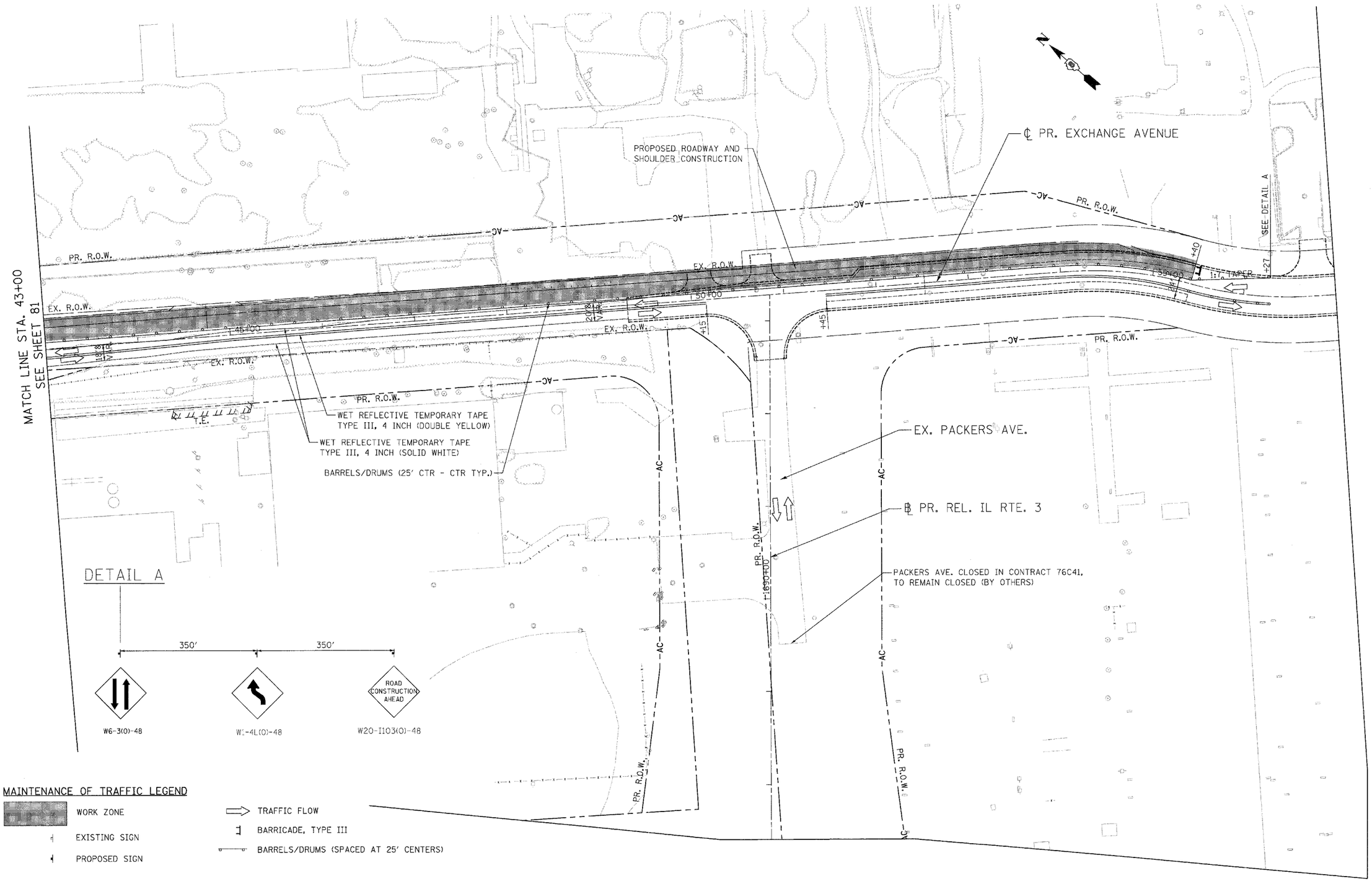


MAINTENANCE OF TRAFFIC LEGEND

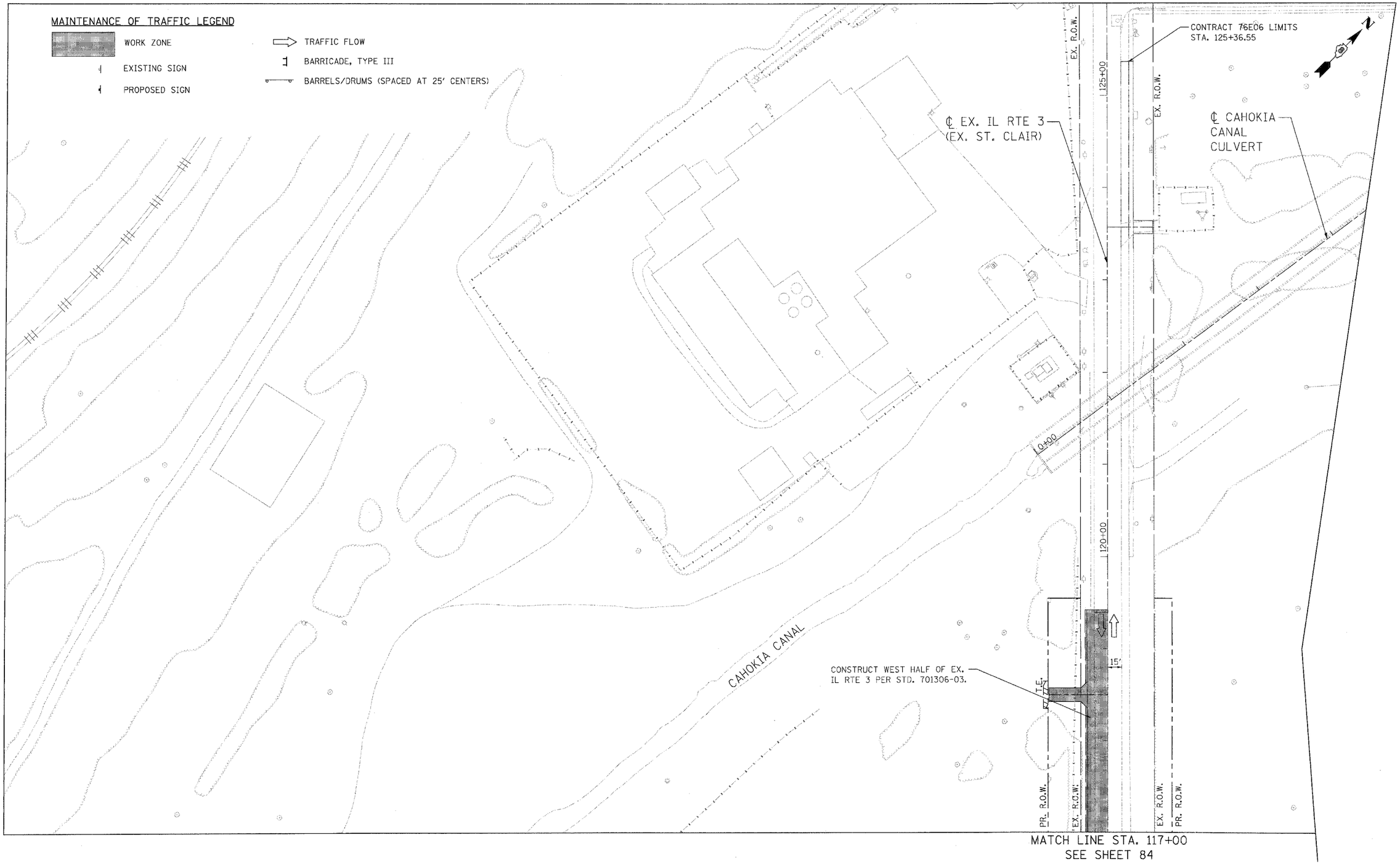
- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE	PR. EXCHANGE AVE., RAMPS C AND D MAINTENANCE OF TRAFFIC – STAGE 1 STA. 35+00 TO STA. 43+00
		DRAWN - RK	REVISED -		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -		
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 2 OF 15 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	81
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PR. EXCHANGE AVE. AND PR. REL. IL RTE. 3 MAINTENANCE OF TRAFFIC - STAGE 1 STA. 43+00 TO STA. 57+00	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353	SHEET NO. 82
		DRAWN - RK	REVISED -					CONTRACT NO. 76E06		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE						
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 3 OF 15 SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	
	DRAWN - RK	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	
PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

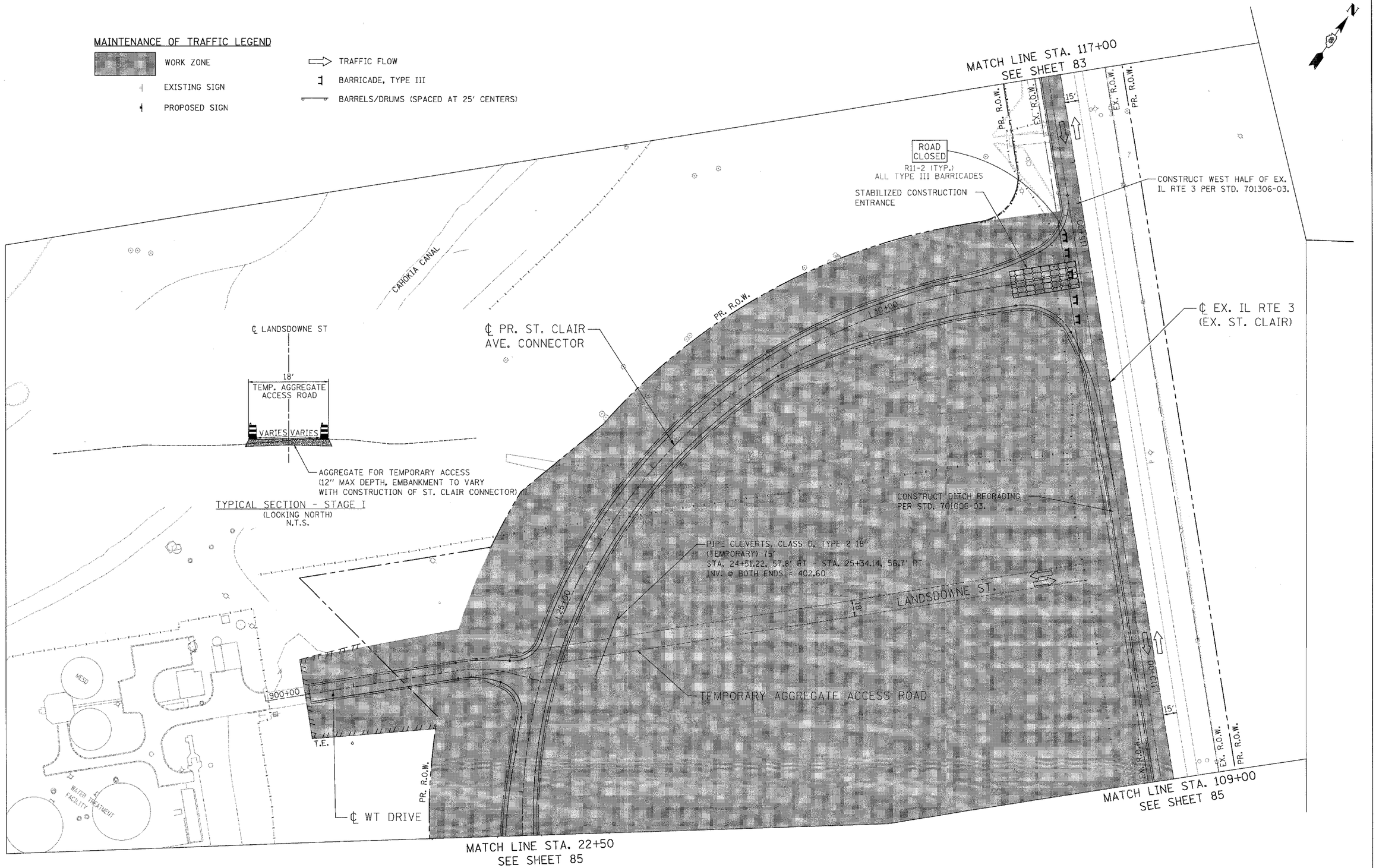
EX. IL RTE. 3 (ST. CLAIR AVE.)
MAINTENANCE OF TRAFFIC – STAGE 1
STA. 117+00 TO 126+00

SCALE: 1"=50' | SHEET NO. 4 OF 15 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	83
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MAINTENANCE OF TRAFFIC LEGEND

WORK ZONE
EXISTING SIGN
PROPOSED SIGN
TRAFFIC FLOW
BARRICADE, TYPE III
BARRELS/DRUMS (SPACED AT 25' CENTERS)



LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE	PR. ST. CLAIR AVE. CONNECTOR AND EX. IL RTE. 3 (ST. CLAIR AVE.) MAINTENANCE OF TRAFFIC – STAGE 1 STA. 109+00 TO STA. 117+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - RK	REVISED -			998	82-2-1K	ST. CLAIR	353 / 84
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -					CONTRACT NO. 76E06	
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 5 OF 15 SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

MATCH LINE STA. 22+50
SEE SHEET 84

MATCH LINE STA. 109+00
SEE SHEET 84

℄ PR. ST. CLAIR AVE. CONNECTOR

℄ PR. REL. IL RTE 3

CONSTRUCT WEST HALF OF EX. IL RTE 3 PER STD. 701306-03.

CONSTRUCT DITCH REGRADING PER STD. 701006-03.

CONTRACT 76E06 LIMITS
STA. 1668+15.65

ROAD CLOSED
RT 2

1665+00

1670+00

20+00

1675+00

105+00

PR. R.O.W.

EX. R.O.W.

T.E.

P.E.

AC

℄ EX. IL RTE 3
(EX. ST. CLAIR)

IL RTE 3
(ST. CLAIR AVE.)

NOTES:

1. TYPE III BARRICADES TO BE TIED TOGETHER USING CHAIN, ROPE OR STEEL WIRE AS APPROVED BY THE ENGINEER.

2. MAINTENANCE AND REPAIR OF THE TIE MATERIAL SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, SPECIAL.

3. TYPE III BARRICADES WILL REMAIN IN PLACE AFTER CONTRACT COMPLETION, THE COST OF WHICH IS INCLUDED IN TRAFFIC CONTROL AND PROTECTION, SPECIAL.

MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	
	DRAWN - RK	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	
PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -	

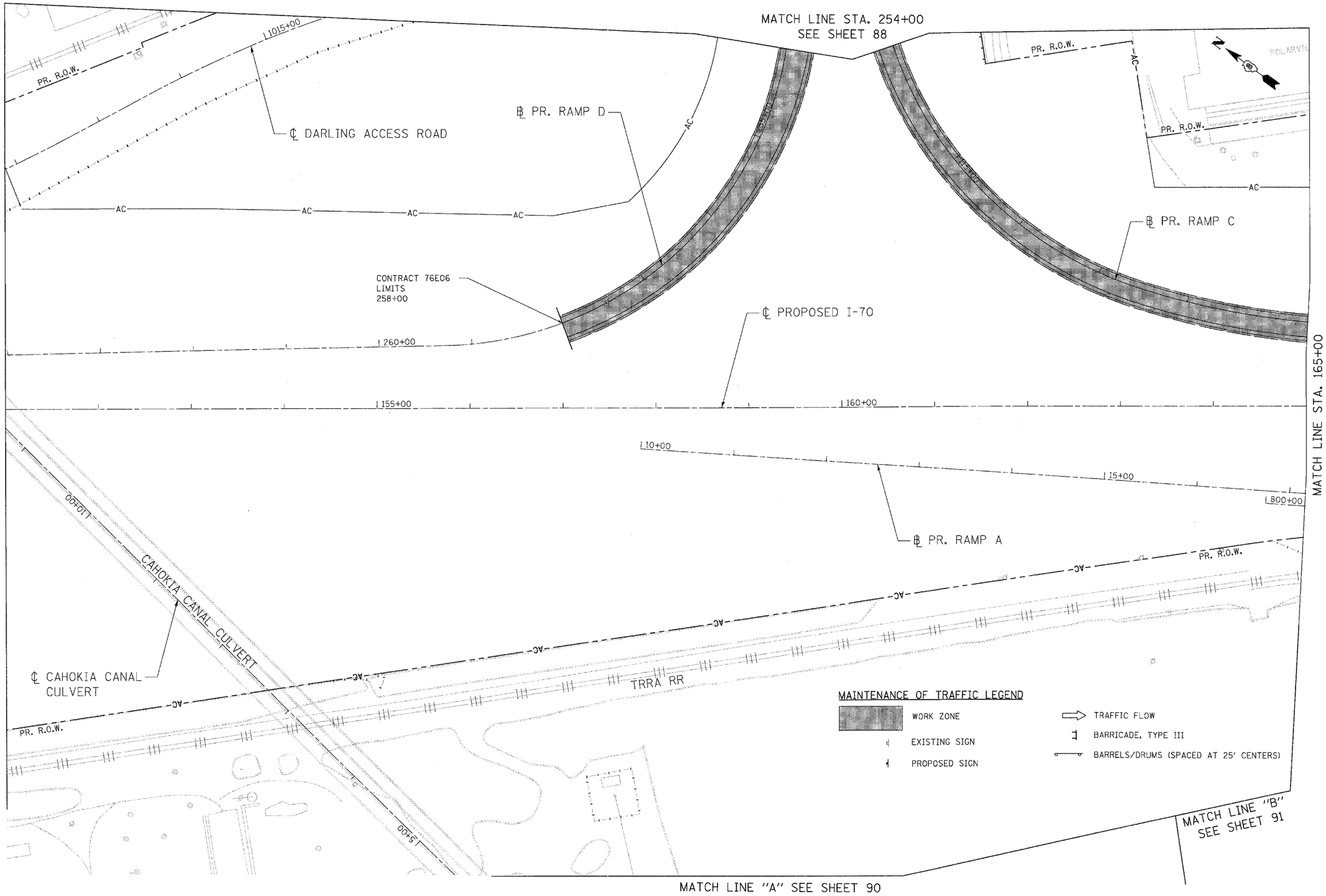
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

PR. REL. IL RTE. 3, PR. ST. CLAIR AVE. CONNECTOR AND EX. IL RTE. 3
MAINTENANCE OF TRAFFIC – STAGE 1
STA. 1665+00 TO STA. 1679+30

SCALE: 1"=50' | SHEET NO. 6 OF 15 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	85
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

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	DRAWN - RK	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	
PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -	

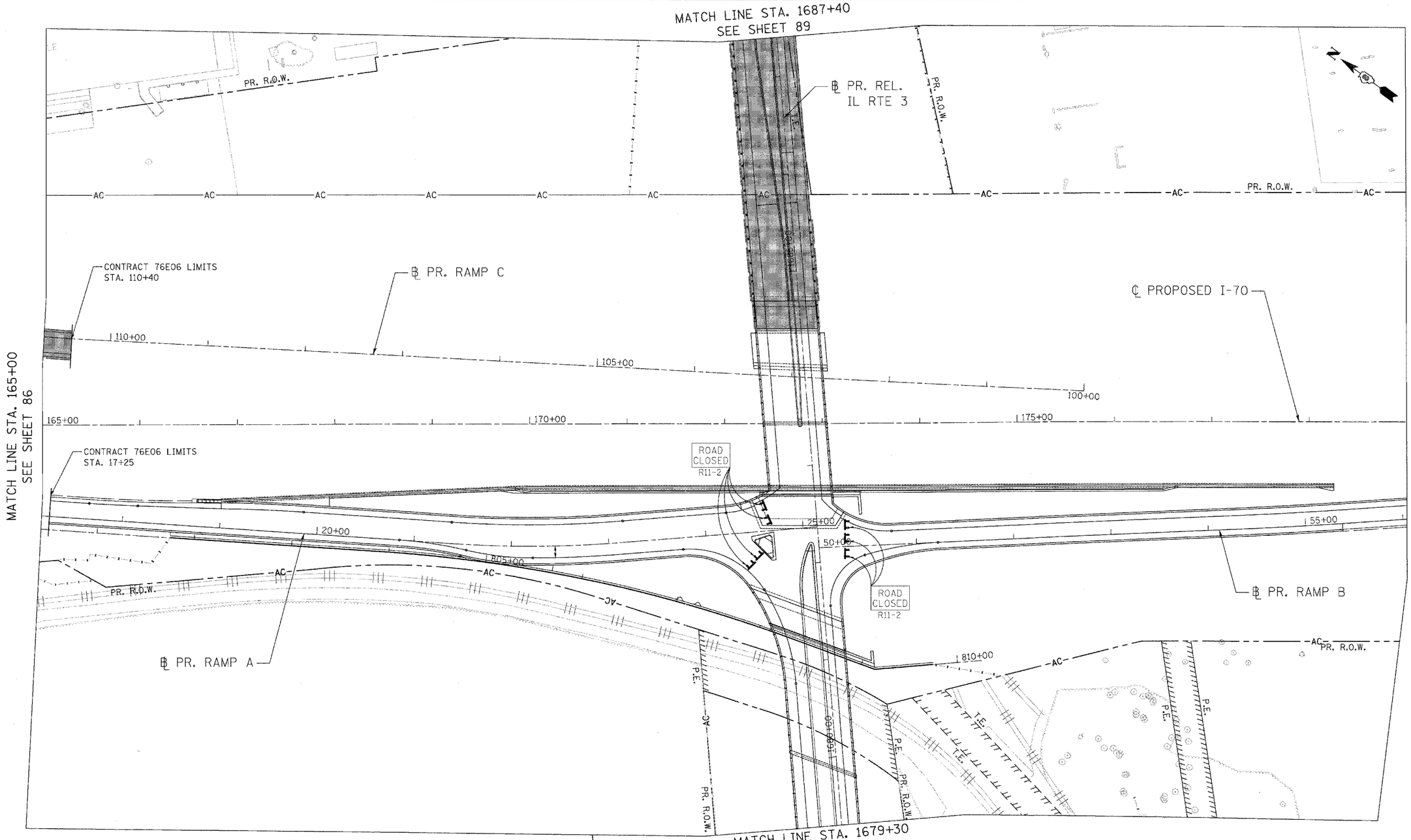
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

RAMPS C AND D
MAINTENANCE OF TRAFFIC – STAGE 2
STA. 254+00 TO STA. 258+00, STA. 110+69 TO STA. 116+40

SCALE: 1"=50' | SHEET NO. 7 OF 15 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	86
		CONTRACT NO. 76E06		

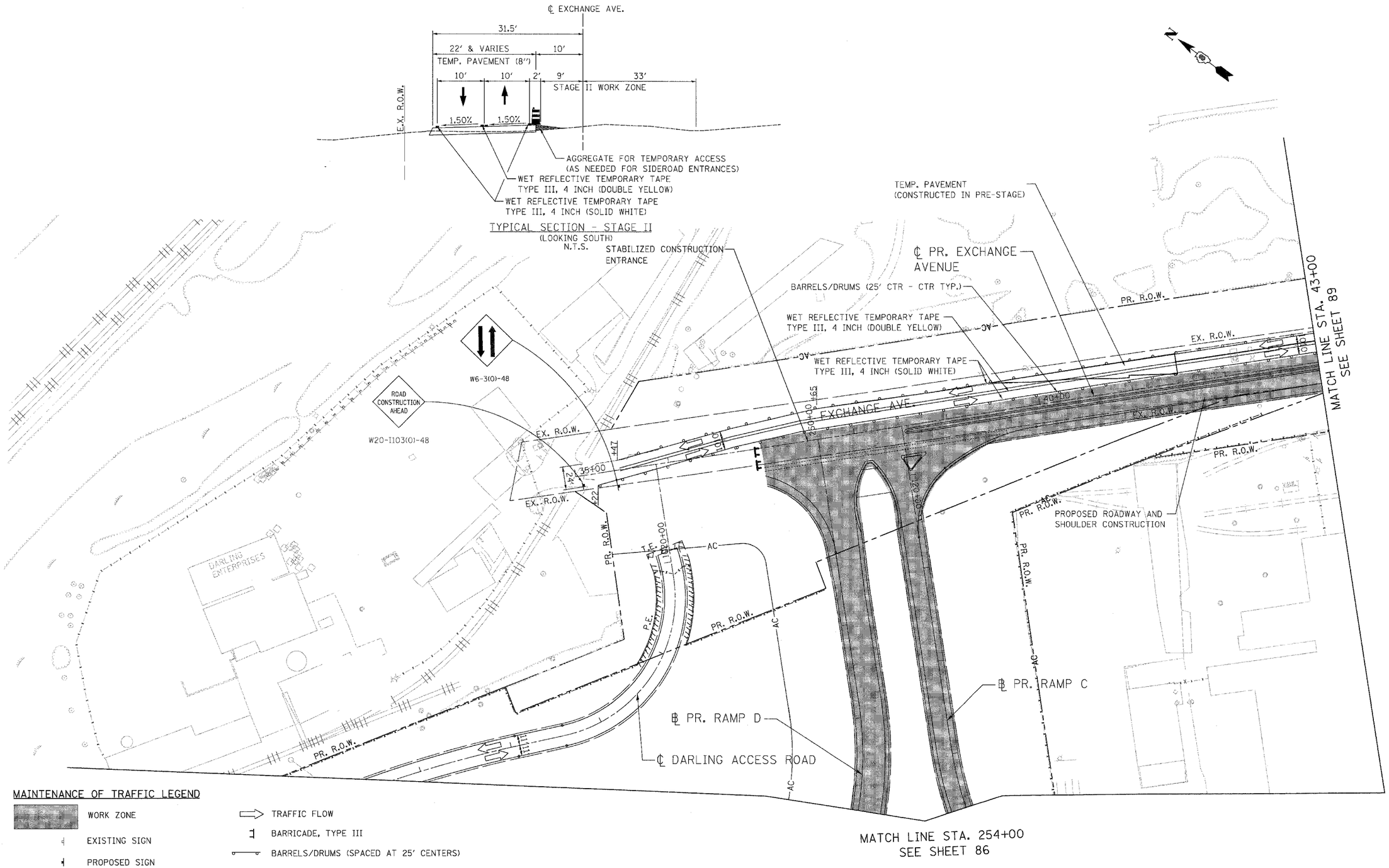
FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REL. IL RTE. 3, RAMPS A AND B MAINTENANCE OF TRAFFIC - STAGE 2 STA. 1679+30 TO STA. 1687+40	F.A.P. RTE. 998	SECTION 82-2-1K	COUNTY ST. CLAIR	TOTAL SHEETS 353	SHEET NO. 87
		DRAWN - RK	REVISED -					CONTRACT NO. 76E06		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE	SCALE: 1"=50' SHEET NO. 8 OF 15 SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -							



MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE	PR. EXCHANGE AVE., RAMPS C AND D MAINTENANCE OF TRAFFIC - STAGE 2 STA. 35+00 TO STA. 43+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - RK	REVISED -			998	82-2-1K	ST. CLAIR	353	88
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -					CONTRACT NO. 76E06		
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 9 OF 15 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SPECIAL
36"x18"
4" BLACK LETTERS
ON ORANGE REFLECTIVE
BACKGROUND

POLARVILLE
ENTRANCE OPEN

M6-1-2115

R11-2 ROAD CLOSED

℄ PR. EXCHANGE AVENUE

BARRELS/DRUMS (25' CTR - CTR TYP.)

SEE DETAIL A

PR. R.O.W.

EX. R.O.W.

45+00

50+00

55+00

+42

+66

1:6 TAPER

MATCH LINE STA. 43+00
SEE SHEET 88

T.E.

WET REFLECTIVE TEMPORARY TAPE
TYPE III, 4 INCH (DOUBLE YELLOW)

WET REFLECTIVE TEMPORARY TAPE
TYPE III, 4 INCH (SOLID WHITE)

PROPOSED ROADWAY AND
SHOULDER CONSTRUCTION

AC

℄ PR. REL. IL RTE 3

ACCESS TO BE PROVIDED UNTIL PR. REL. IL RTE 3
IS PAVED, AT WHICH TIME ACCESS TO POLARVILLE
WILL ONLY BE AVAILABLE USING THE EXCHANGE AVE.
ENTRANCE.

ROAD
CLOSED
R11-2

DETAIL A

350' 350'

W6-3(O)-48

W1-4R(O)-48

ROAD CONSTRUCTION AHEAD

W20-I103(O)-48

MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

MATCH LINE STA. 1687+40
SEE SHEET 87

LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE	PR. EXCHANGE AVE. AND PR. REL. IL RTE. 3 MAINTENANCE OF TRAFFIC – STAGE 2 STA. 43+00 TO STA. 57+00	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS / SHEET NO.
		DRAWN - RK	REVISED -			998	82-2-1K	ST. CLAIR	353 / 89
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -					CONTRACT NO. 76E06	
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -		SCALE: 1"=50' / SHEET NO. 10 OF 15 SHEETS / STA. / TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

MAINTENANCE OF TRAFFIC LEGEND

WORK ZONE

EXISTING SIGN

PROPOSED SIGN

TRAFFIC FLOW

BARRICADE, TYPE III

BARRELS/DRUMS (SPACED AT 25' CENTERS)

NOTE:
THE CONTRACTOR SHALL LIMIT THE LENGTH OF TIME THAT THE SIDEWALK IS CLOSED TO 14 CONTIGUOUS CALENDAR DAYS, OR AS APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE VILLAGE OF FAIRMONT CITY FIRE AND POLICE DEPARTMENT WITH A 2 WEEK WRITTEN AND PHONE NOTICE PRIOR TO THE TEMPORARY CLOSURE OF THIS SIDEWALK.

SIDEWALK CLOSED
R9-9-2412

CONTRACT 76E06 LIMITS
STA. 125+36.55

N

EX. R.O.W.

L125+00

EX. R.O.W.

℄ CAHOKIA CANAL CULVERT

℄ EX. IL RTE 3
(EX. ST. CLAIR)

L120+00

MATCH LINE "A"
SEE SHEET 86

SIDEWALK CLOSED
R9-9-2412

CAHOKIA CANAL

14'

CONSTRUCT EAST HALF OF EX. IL RTE 3 PER STD. 701306-03.

T.E.

CONSTRUCT DITCH REGRADING PER STD. 701006-03.

PR. R.O.W.

EX. R.O.W.

14'

EX. R.O.W.

PR. R.O.W.

MATCH LINE STA. 117+00
SEE SHEET 91

LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -
		DRAWN - RK	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -
	PLOT DATE = 10/18/2011	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

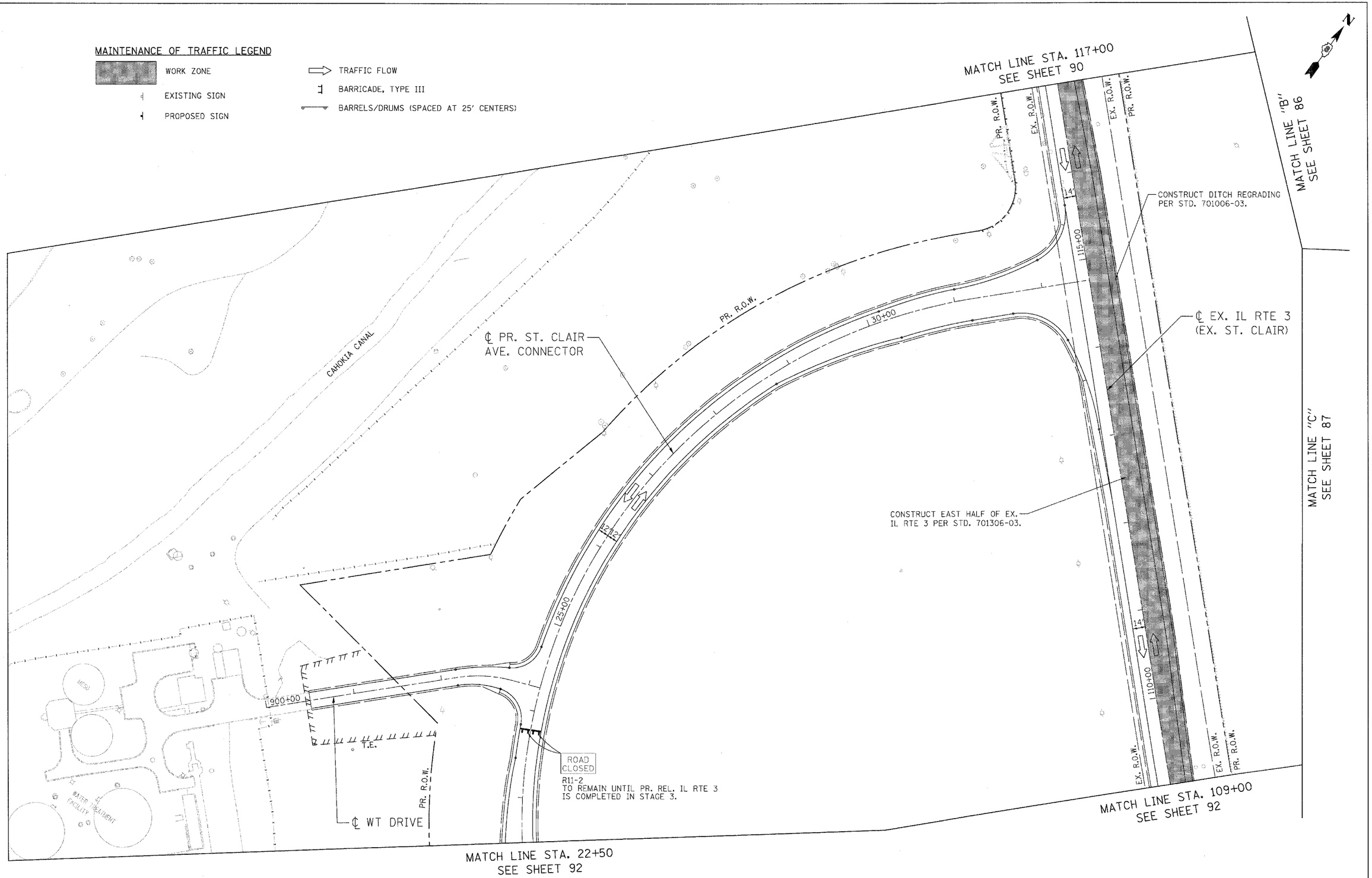
EX. IL RTE. 3 (ST. CLAIR AVE.)
MAINTENANCE OF TRAFFIC – STAGE 2
STA. 117+00 TO 126+00

SCALE: 1"=50' | SHEET NO. 11 OF 15 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	90
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)



LIN ENGINEERING,LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -
		DRAWN - RK	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

ST. CLAIR AVE. CONNECTOR AND EX. IL RTE. 3 (ST. CLAIR AVE.)
MAINTENANCE OF TRAFFIC – STAGE 2
STA. 109+00 TO STA. 117+00

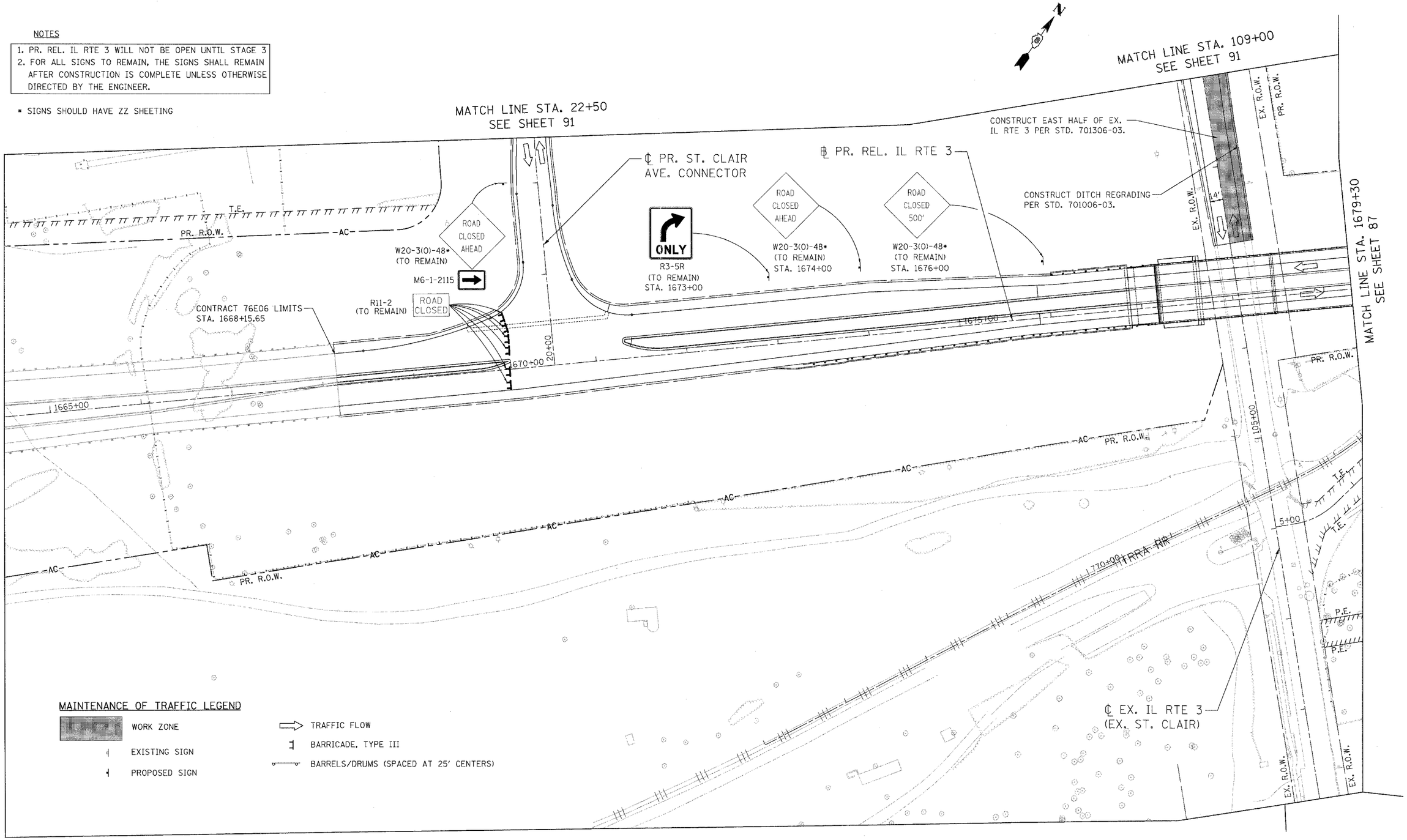
SCALE: 1"=50' | SHEET NO. 12 OF 15 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	91
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

NOTES

1. PR. REL. IL RTE 3 WILL NOT BE OPEN UNTIL STAGE 3
2. FOR ALL SIGNS TO REMAIN, THE SIGNS SHALL REMAIN AFTER CONSTRUCTION IS COMPLETE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

* SIGNS SHOULD HAVE ZZ SHEETING



MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	
	DRAWN - RK	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	
PLOT DATE = 10/18/2011	DATE - 10/21/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

PR. REL. IL RTE. 3, PR. ST. CLAIR AVE. CONNECTOR AND EX. IL RTE. 3
MAINTENANCE OF TRAFFIC – STAGE 2
STA. 1665+00 TO STA. 1679+30

SCALE: 1"=50' | SHEET NO. 13 OF 15 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	92
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

CONSTRUCT EXCHANGE AVE. FROM STA 35+00 TO STA 37+00 PER. STD. 701501-06.

PROPOSED ROADWAY AND SHOULDER CONSTRUCTION

BARRELS/DRUMS (25' CTR - CTR TYP.)

℄ PR. EXCHANGE AVENUE

WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH (SOLID WHITE)

PAVEMENT SHALL ONLY BE REMOVED ONCE EXCHANGE AVE. IS FULLY CONSTRUCTED AND OPEN TO TRAFFIC.

DARLING ACCESS ROAD SHALL ONLY BE CLOSED ONCE EXCHANGE AVE. IS FULLY CONSTRUCTED AND OPEN TO TRAFFIC.

W20-3(0)-48

ROAD CLOSED 500'

ROAD CLOSED R11-2

R3-2-2424

℄ PR. RAMP C

℄ PR. RAMP D

℄ DARLING ACCESS ROAD

MATCH LINE STA. 43+00 SEE SHEET 94

EXCHANGE AVE.

EX. R.O.W.

PR. R.O.W.

DARLING ENTERPRISES

MAINTENANCE OF TRAFFIC LEGEND

- WORK ZONE
- EXISTING SIGN
- PROPOSED SIGN
- TRAFFIC FLOW
- BARRICADE, TYPE III
- BARRELS/DRUMS (SPACED AT 25' CENTERS)

LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	
	DRAWN - RK	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	
PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -	

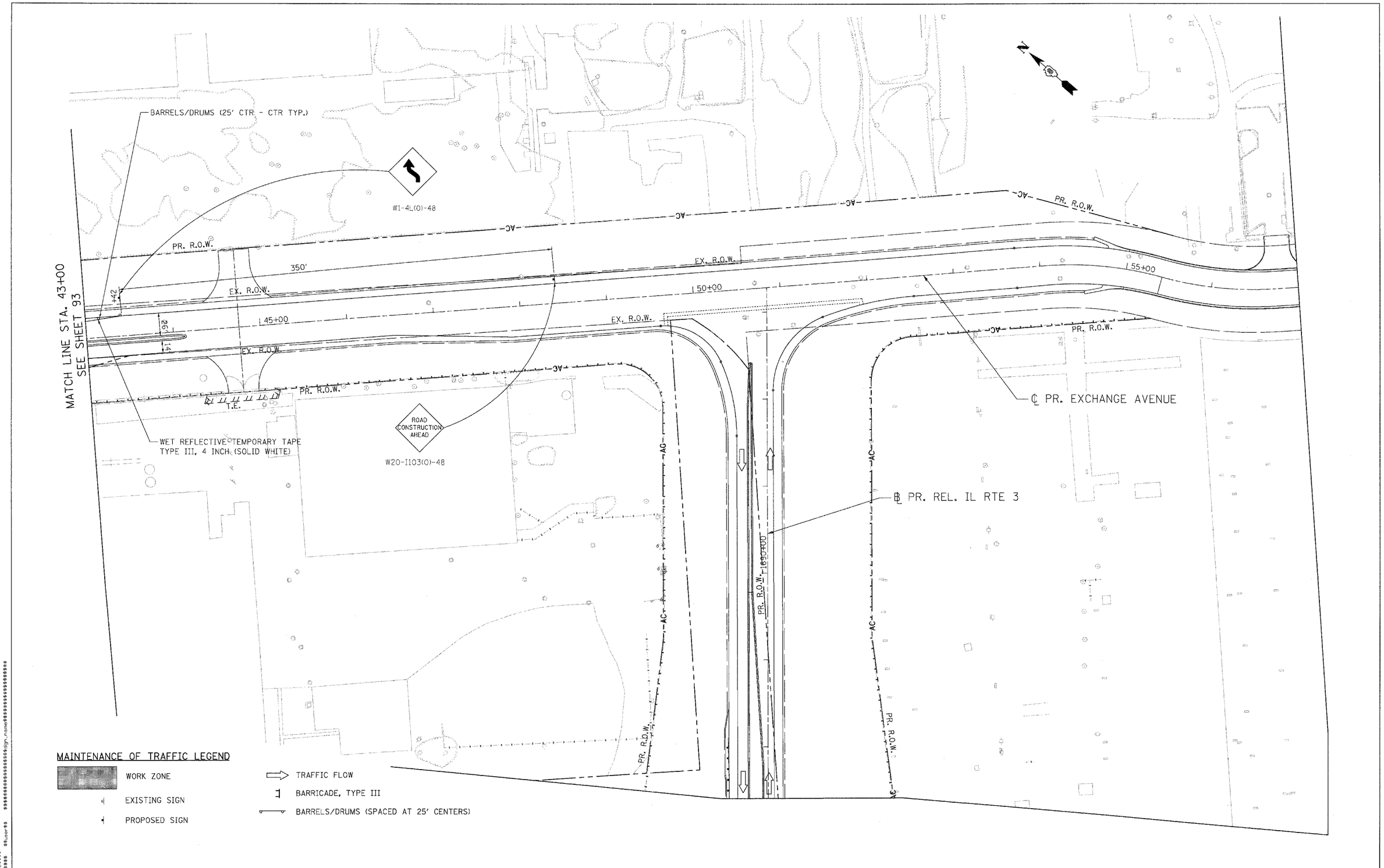
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

PR. EXCHANGE AVE., RAMPS C AND D
MAINTENANCE OF TRAFFIC – STAGE 3
STA. 35+00 TO STA. 43+00

SCALE: 1"=50' | SHEET NO. 14 OF 15 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	93
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



LIN ENGINEERING,LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

USER NAME = Plotted by Scott Wilkinson	DESIGNED - SEW	REVISED -	
	DRAWN - RK	REVISED -	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -	
PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

PR. EXCHANGE AVE. AND PR. REL. IL RTE. 3
MAINTENANCE OF TRAFFIC – STAGE 3
STA. 43+00 TO STA. 57+00

SCALE: 1"=50' | SHEET NO. 15 OF 15 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	94
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

AGGREGATE FOR TEMPORARY ACCESS

DESCRIPTION	VOLUME (CU YD)	VOLUME (TON)
TEMP ACCESS DRIVE STAGE 1	900	1701
EXCHANGE AVE ACCESS	250	473
	TOTAL	2174

UNIT WEIGHT: 1.89TON/CY

PIPE CULVERTS, CLASS D, TYPE 2 18" (TEMPORARY)

ALIGNMENT	STATION	OFFSET	RT/LT	LENGTH (FT)
CONNECTOR	24+51	62	RT/LT	75
			TOTAL	75

TRAFFIC CONTROL AND PROTECTION, STANDARD 701306

STAGES 1 & 2 EX. IL RTE. 3	1	L SUM

TRAFFIC CONTROL AND PROTECTION, STANDARD 701501

STAGE 3 PR. EXCHANGE STA. 35+11 TO STA. 37+00	1	L SUM

TRAFFIC CONTROL SURVEILLANCE

PROJECT LENGTH	30	CAL DA
ONCE PER WORK WEEK		

CHANGEABLE MESSAGE SIGN

JOB SITE	30	CAL MO
2 SIGNS AT 15 MONTHS EACH		

WORK ZONE PAVEMENT MARKING REMOVAL

PAY ITEM	LENGTH	WIDTH	AREA
	FT	FT	FT^2
70300210	N/A	N/A	478
70300220	39359	0.33	13120
70300250	460	0.66	304
70300260	1566	1	1566
70300280	371	2	742
X7030030	14326.52	0.33	4776
		TOTAL	20985

DETOUR SIGNING

STAGES 1 & 2	1	L SUM

TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS

AREA	DESC
15.6	ARROW
25.9	THRU AND TURN ARROW

ALIGNMENT	STATION	TYPE	AREA (SQ FT)
PR. EXCHANGE	38+87	LT	15.6
PR. EXCHANGE	41+34	LT	15.6
PR. EXCHANGE	43+86	LT	15.6
PR. EXCHANGE	45+58	LT	15.6
PR. EXCHANGE	48+00	RT	15.6
PR. EXCHANGE	49+76	RT	15.6
PR. EXCHANGE	51+27	LT	15.6
PR. EXCHANGE	52+80	LT	15.6
PR. REL. IL RTE. 3	1671+59	RT	15.6
PR. REL. IL RTE. 3	1672+27	RT	15.6
PR. REL. IL RTE. 3	1683+50	LT	15.6
PR. REL. IL RTE. 3	1685+18	LT	15.6
PR. REL. IL RTE. 3	1689+90	LT	15.6
PR. REL. IL RTE. 3	1689+90	RT	15.6
PR. REL. IL RTE. 3	1692+24	LT	15.6
PR. REL. IL RTE. 3	1692+24	RT	15.6
EX. IL RTE. 3	112+32	LT	15.6
EX. IL RTE. 3	114+32	LT	15.6
RAMP A	22+44	RT	15.6
RAMP A	22+44	LT	15.6
RAMP A	24+42	LT	15.6
RAMP A	24+35	RT	15.6
RAMP C	119+93	LT	15.6
CONNECTOR	31+12	RT	15.6
CONNECTOR	31+12	LT	15.6
CONNECTOR	31+87	RT	15.6
CONNECTOR	31+87	LT	15.6
CONNECTOR	20+85	LT	15.6
CONNECTOR	21+65	LT	15.6
		TOTAL	452

WET REFLECTIVE TEMPORARY TAPE TYPE III, 4 INCH

TYPE	DESC
4SW	4" SOLID WHITE
4SY	4" SOLID YELLOW
4SDW	4" SKIP-DASH WHITE
4DY	4" DOUBLE YELLOW LINE

STAGE 1

ALIGNMENT	STATION	OFFSET	STATION	OFFSET	TYPE	LENGTH (FT)
PR. EXCHANGE	39+69	LT	43+00	RT	4SW	330.50
PR. EXCHANGE	39+69	RT	50+15	LT	4DY	2092.00
PR. EXCHANGE	39+71	LT	43+00	RT	4SW	330.97
PR. EXCHANGE	43+00	RT	49+30	RT	4SW	630.50
PR. EXCHANGE	43+00	LT	55+00	LT	4SW	1328.56
PR. EXCHANGE	51+45	LT	55+39	LT	4DY	787.34
					SUBTOTAL	5499.87

STAGE 2

ALIGNMENT	STATION	OFFSET	STATION	OFFSET	TYPE	LENGTH (FT)
PR. EXCHANGE	35+55	LT	43+00	LT	4SW	746.09
PR. EXCHANGE	35+65	RT	43+00	LT	4SW	736.80
PR. EXCHANGE	35+65	RT	43+00	LT	4DY	1473.60
PR. EXCHANGE	43+00	LT	55+79	LT	4SW	1285.38
PR. EXCHANGE	43+00	LT	55+79	LT	4DY	2570.76
PR. EXCHANGE	43+00	LT	56+68	RT	4SW	1371.46
					SUBTOTAL	8184.09

STAGE 3

ALIGNMENT	STATION	OFFSET	STATION	OFFSET	TYPE	LENGTH (FT)
PR. EXCHANGE	37+00	LT	43+00	LT	4SW	599.92
PR. EXCHANGE	43+00	LT	43+42	LT	4SW	42.64
					SUBTOTAL	642.56
					TOTAL	14327

TEMPORARY PAVEMENT MARKING - LINE 12"

TYPE	DESC
12SW	12" SOLID WHITE
12SY	12" SOLID YELLOW

ALIGNMENT	STATION	OFFSET	RT/LT	STATION	OFFSET	RT/LT	TYPE	LENGTH (FT)
PR. EXCHANGE	35+42	N/A	N/A	37+78	N/A	N/A	12SY	140
PR. EXCHANGE	45+43	N/A	N/A	49+95	N/A	N/A	12SY	158
PR. EXCHANGE	51+19	N/A	N/A	56+11	N/A	N/A	12SY	152
PR. REL. IL RTE. 3	1671+35	N/A	N/A	1676+05	N/A	N/A	12SY	212
EX. IL RTE. 3	108+15	N/A	N/A	109+00	N/A	N/A	12SY	19
EX. IL RTE. 3	109+00	N/A	N/A	112+00	N/A	N/A	12SY	96
EX. IL RTE. 3	115+25	N/A	N/A	117+00	N/A	N/A	12SY	67
EX. IL RTE. 3	117+00	N/A	N/A	119+43	N/A	N/A	12SY	40
RAMP A	23+00	N/A	N/A	24+80	N/A	N/A	12SY	176
RAMP A	23+97	N/A	N/A	24+80	N/A	N/A	12SW	234
RAMP C	119+86	N/A	N/A	120+30	N/A	N/A	12SW	132
CONNECTOR	20+66	N/A	N/A	22+50	N/A	N/A	12SW	118
CONNECTOR	22+50	N/A	N/A	23+45	N/A	N/A	12SW	11
CONNECTOR	22+50	N/A	N/A	23+00	N/A	N/A	12SW	11
							TOTAL	1566

TEMPORARY PAVEMENT MARKING - LINE 24"

TYPE	DESC
24SW	24" SOLID WHITE

ALIGNMENT	STATION	OFFSET	RT/LT	STATION	OFFSET	RT/LT	TYPE	LENGTH (FT)
PR. EXCHANGE	38+68	3.00	RT	38+68	23.00	LT	24SW	26
PR. EXCHANGE	49+89	9	RT	49+89	41.85	RT	24SW	33
PR. EXCHANGE	51+19	7.05	RT	51+19	23	LT	24SW	30
PR. REL. IL RTE. 3	1671+35	23	LT	1671+35	47.65	LT	24SW	25
PR. REL. IL RTE. 3	1681+19	20	RT	1682+07	5	LT	24SW	30
PR. REL. IL RTE. 3	1683+28	23.00	LT	1683+28	35.00	LT	24SW	12
PR. REL. IL RTE. 3	1683+41	11.00	LT	1683+41	23.00	LT	24SW	12
PR. REL. IL RTE. 3	1692+39	17	LT	1692+39	13.39	N/A	24SW	30
RAMP A	24+47	46.68	RT	24+56	20.64	RT	24SW	18
RAMP A	24+56	0	CL	24+56	22	LT	24SW	17
RAMP C	120+06	26.57	LT	120+15	10.47	LT	24SW	16
RAMP D	250+10	22	RT	250+44	37	RT	24SW	40
CONNECTOR	20+66	0	CL	20+77	37.91	LT	24SW	42
CONNECTOR	32+10	0	CL	32+00	36.77	RT	24SW	40
							TOTAL	371

TEMPORARY PAVEMENT MARKING - LINE 8"

TYPE	DESC
8SW	8" SOLID WHITE

ALIGNMENT	STATION	OFFSET	RT/LT	STATION	OFFSET	RT/LT	TYPE	LENGTH (FT)
RAMP C	119+86	0.42	LT	120+30	50.20	RT	8SW	202
RAMP A	23+97	0	CL	24+60	23	RT	8SW	137
PR. REL. IL RTE. 3	1681+62	35	LT	1682+36	35	LT	8SW	121
							TOTAL	460

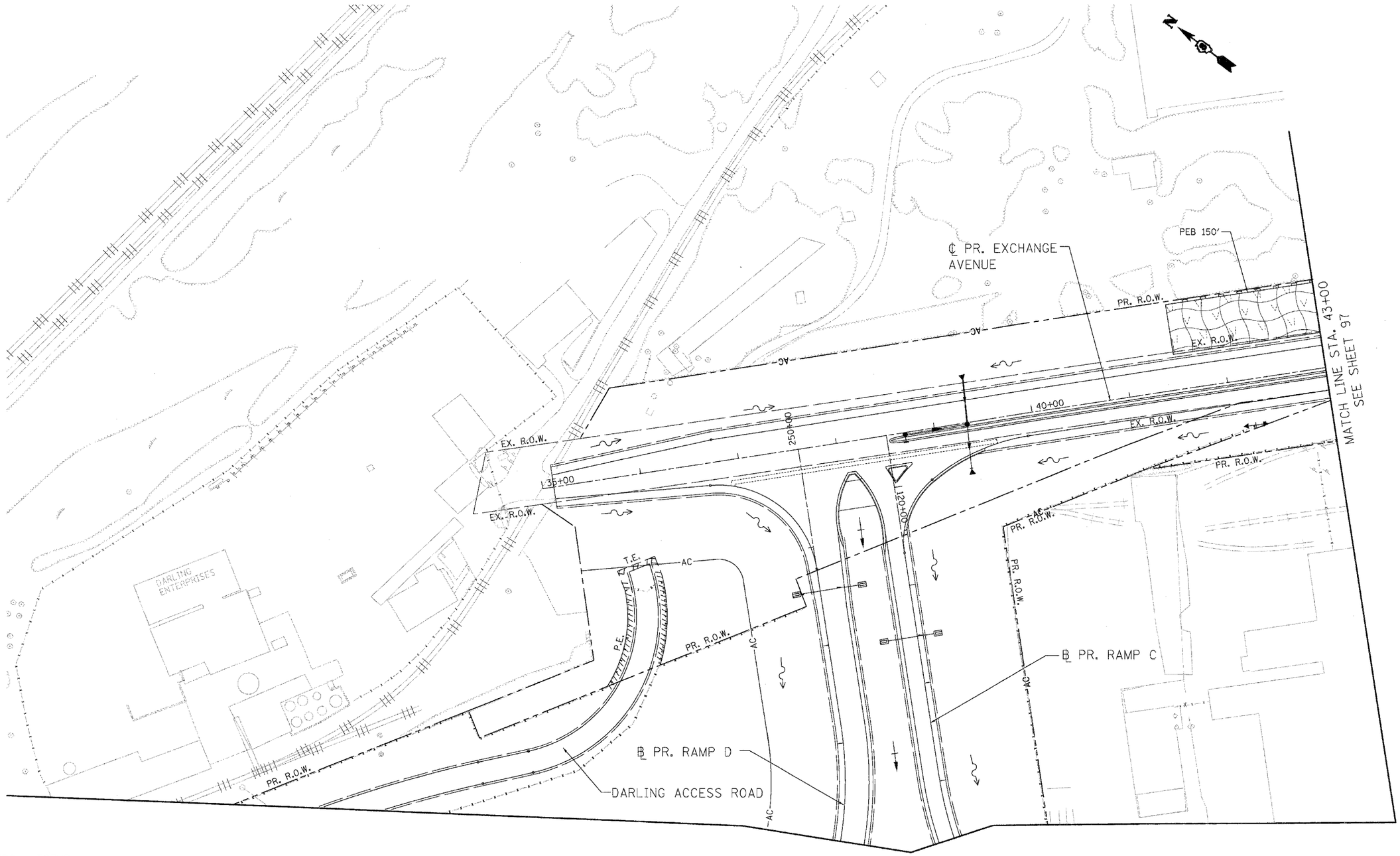
TEMPORARY PAVEMENT

ALIGNMENT	STATION	OFFSET	RT/LT	STATION	OFFSET	RT/LT	AREA (SQ YD)
PR. EXCHANGE	39+76	N/A	LT	41+50	N/A	LT	253.46
						TOTAL	253

TEMPORARY PAVEMENT MARKING - LINE 4"

TYPE	DESC
4SW	4" SOLID WHITE
4SY	4" SOLID YELLOW
4SDW	4" SKIP-DASH WHITE
4DY	4" DOUBLE YELLOW LINE

ALIGNMENT	STATION	OFFSET	RT/LT	STATION	OFFSET	RT/LT	TYPE	LENGTH (FT)
EX. IL RTE. 3	107+15	0	CL	112+20	5	RT	4DY	1010
EX. IL RTE. 3	107+15	14	LT	112+29	14	LT	4SW	514
EX. IL RTE. 3	107+15	0	CL	114+50	2	LT	4DY	1470
EX. IL RTE. 3	107+15	15	RT	119+43	15	RT	4SW	1228
EX. IL RTE. 3	110+65	11	RT	112+21	10	RT	4SDW	38
EX. IL RTE. 3	112+21	10	RT	114+50	10	RT	4SW	229
EX. IL RTE. 3	115+26	2	RT	119+43	0	CL	4DY	834
EX. IL RTE. 3	115+26	2	LT	119+43	0	LT	4DY	348
EX. IL RTE. 3	115+64	14	LT	119+43	14.68	LT	4SW	379
PR. EXCHANGE	35+14	0	LT	37+78	6	RT	4DY	528
PR. EXCHANGE	35+14	12	LT	55+44	23	LT	4SW	2030
PR. EXCHANGE	35+14	12	RT	254+00	22	RT	4SW	869
PR. EXCHANGE	35+44	0	RT	37+78	6	RT	4DY	468
PR. EXCHANGE	38+55	4	RT	43+00	4	RT	4SY	445
PR. EXCHANGE	38+55	8	RT	43+00	8	RT	4SY	445
PR. EXCHANGE	38+68	9	LT	44+00	9	LT	4SW	532
PR. EXCHANGE	43+00	3	RT	44+15	3	RT	4SY	115
PR. EXCHANGE	43+00	9	RT	44+15	9	RT	4SY	115
PR. EXCHANGE	43+00	23	RT	1692+39	5	LT	4SW	503
PR. EXCHANGE	45+43	9	LT	46+58	9	LT	4SW	115
PR. EXCHANGE	45+43	6	RT	49+95	5	RT	4DY	904
PR. EXCHANGE	45+43	6	RT	49+95	5	RT	4DY	904
PR. EXCHANGE	45+43	23	RT	1687+40	35	LT	4SW	1009
PR. EXCHANGE	46+54	23	RT	47+90	23	RT	4SDW	34
PR. EXCHANGE	46+58	9	LT	47+91	10	LT	4SDW	32
PR. EXCHANGE	47+90	23	RT	49+89	23	RT	4SW	199
PR. EXCHANGE	51+19	9	LT	52+93	9	LT	4SW	174
PR. EXCHANGE	51+19	9	RT	56+11	1	RT	4DY	984
PR. EXCHANGE	51+19	9	RT	56+11	1	LT	4DY	984
PR. EXCHANGE	52+93	9	LT	54+00	10	LT	4SDW	27
PR. REL. IL RTE. 3	1669+89	23	RT	56+00	0	CL	4SW	1796
PR. REL. IL RTE. 3	1671+35	35	LT	1676+05	23	LT	4SY	470
PR. REL. IL RTE. 3	1671+35	5	LT	1682+21	5	LT	4SY	1086
PR. REL. IL RTE. 3	1671+35	23	LT	1682+21	23	LT	4SY	1086
PR. REL. IL RTE. 3	1682+64	7	RT	55+40	20	RT	4SW	1444
PR. REL. IL RTE. 3	1682+75	35	LT	1687+40	35	LT	4SW	465
PR. REL. IL RTE. 3	1683+28	23	LT	1685+28	23	LT	4SW	200
PR. REL. IL RTE. 3	1685+28	23	LT	1687+28	23	LT	4SDW	50
PR. REL. IL RTE. 3	1683+43	11	LT	1692+42	23	LT	4SY	899
PR. REL. IL RTE. 3	1683+43	7	LT	1692+42	5	LT	4SY	899
PR. REL. IL RTE. 3	1687+78	5	LT	1689+78	5	LT	4SDW	50
PR. REL. IL RTE. 3	1689+78	5	LT	1692+39	5	LT	4SW	261
RAMP A	17+25	16	LT	24+80	16	LT	4SY	755
RAMP A	17+25	0	CL	1679+30	35	LT	4SW	1000
RAMP A	20+85	0	CL	22+00	0	CL	4SDW	36
RAMP A	22+22	0	CL	23+97	0	CL	4SW	175
RAMP B	50+00	49.25	LT	56+04	16	LT	4SY	604
RAMP B	56+05	0	CL	58+14	0	CL	4SW	209
RAMP B	56+05	16	LT	58+14	16	LT	4SY	209
RAMP C	110+40	0	CL	110+68	0	CL	4SW	28
RAMP C	110+40	16	LT	110+68	16	LT	4SY	28
RAMP C	114+64	16	LT	165+00	16	LT	4SY	690
RAMP C	114+64	0	CL	165+00	0	CL	4SW	668
RAMP C	116+60	0	CL	43+00	23	RT	4SW	791
RAMP C	116+60	16	LT	119+62	16	LT	4SY	325
RAMP D	250+80	27	LT	258+00	21	LT	4SY	751
RAMP D	250+81	12	LT	252+83	12	LT	4SDW	50
RAMP D	254+00	0	CL	258+00	0	CL	4SW	519
CONNECTOR	20+66	12	LT	22+50	12	LT	4SW	184
CONNECTOR	20+66	0	CL	32+10	0	CL	4DY	2288
CONNECTOR	22+50	19.28	LT	900+50	9	RT	4SW	400
CONNECTOR	22+50	12	RT	32+41	248	RT	4SW	1212
CONNECTOR	22+50	21	RT	1679+30	35	LT	4SW	1027
CONNECTOR	29+35	1	RT	30+84	12	RT	4SDW	37
CONNECTOR	30+84	12	RT	32+10	12	RT	4SW	126
WT DRIVE	900+50	9	LT	32+41	14	LT	4SW	1075
							TOTAL	39359



EROSION CONTROL LEGEND

- INLET FILTERS/INLET & PIPE PROTECTION
- PERIMETER EROSION BARRIER (PEB)
- TEMPORARY EROSION CONTROL SEEDING/ EROSION CONTROL BLANKET
- TEMPORARY DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING/ MULCH, METHOD 2
- STABILIZED CONSTRUCTION ENTRANCE

GENERAL NOTE:

1. ONCE TEMPORARY EROSION CONTROL IS PLACED FOR A STAGE IT SHALL REMAIN IN PLACE UNTIL COMPLETION OF CONSTRUCTION OR UNTIL PERMANENT LANDSCAPING MEASURES ARE INSTALLED (BY OTHERS). INLET FILTERS SHALL REMAIN IN PLACE UNTIL PERMANENT LANDSCAPING ITEMS ARE INSTALLED.

LIN ENGINEERING,LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

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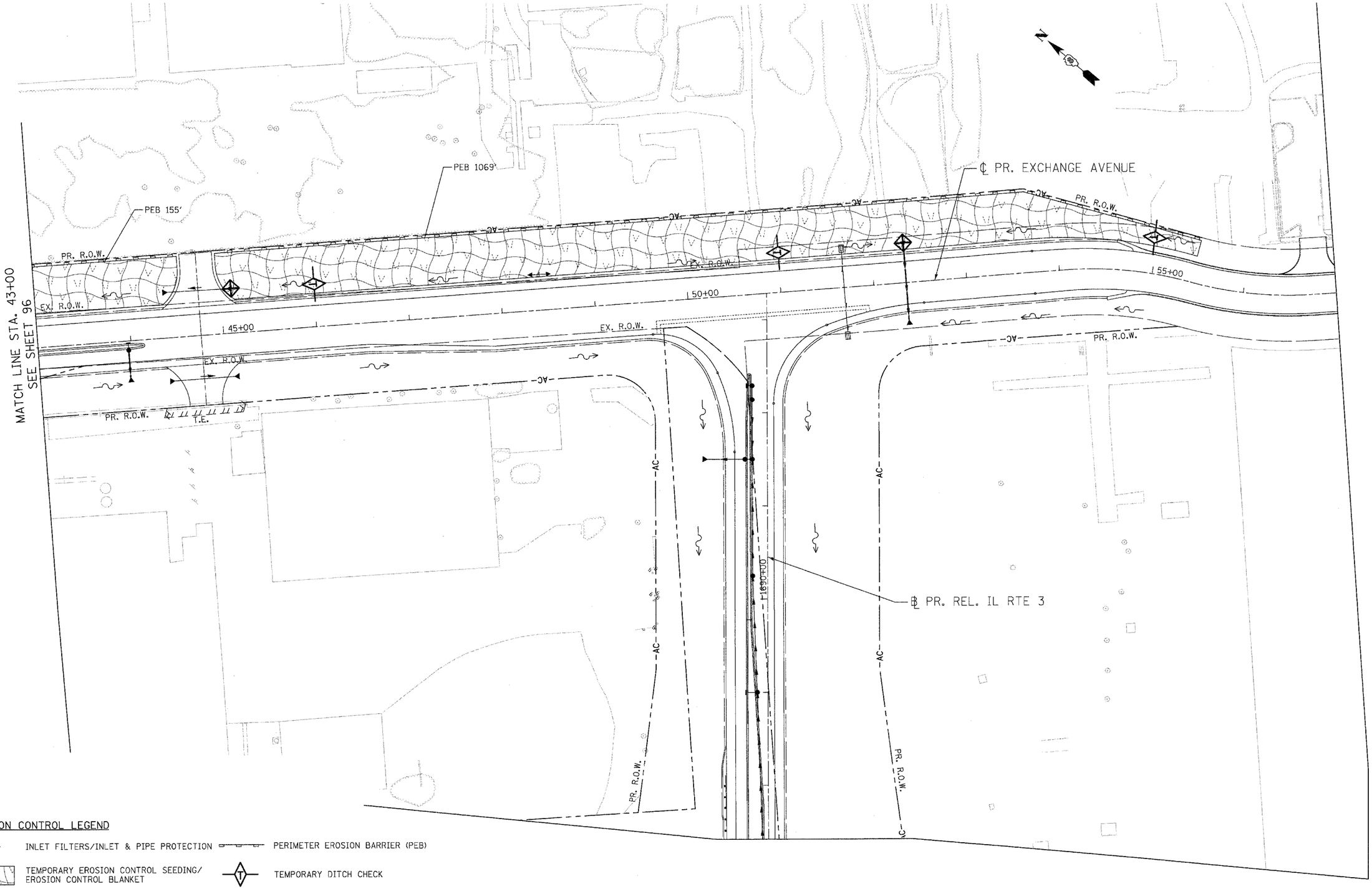
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

PR. EXCHANGE AVE., RAMPS C AND D
EROSION CONTROL PLANS – STAGE 1
STA. 35+00 TO STA. 43+00

SCALE: 1"=50' | SHEET NO. 1 OF 13 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	96
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT



EROSION CONTROL LEGEND

- INLET FILTERS/INLET & PIPE PROTECTION
- PERIMETER EROSION BARRIER (PEB)
- TEMPORARY EROSION CONTROL SEEDING/ EROSION CONTROL BLANKET
- TEMPORARY DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING/ MULCH, METHOD 2
- STABILIZED CONSTRUCTION ENTRANCE

LIN ENGINEERING, LTD.
Consulting Engineers
Chatham, Illinois
Westmont, Illinois

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

PR. EXCHANGE AVE. AND PR. REL. IL RTE. 3
EROSION CONTROL PLANS – STAGE 1
STA. 43+00 TO STA. 57+00

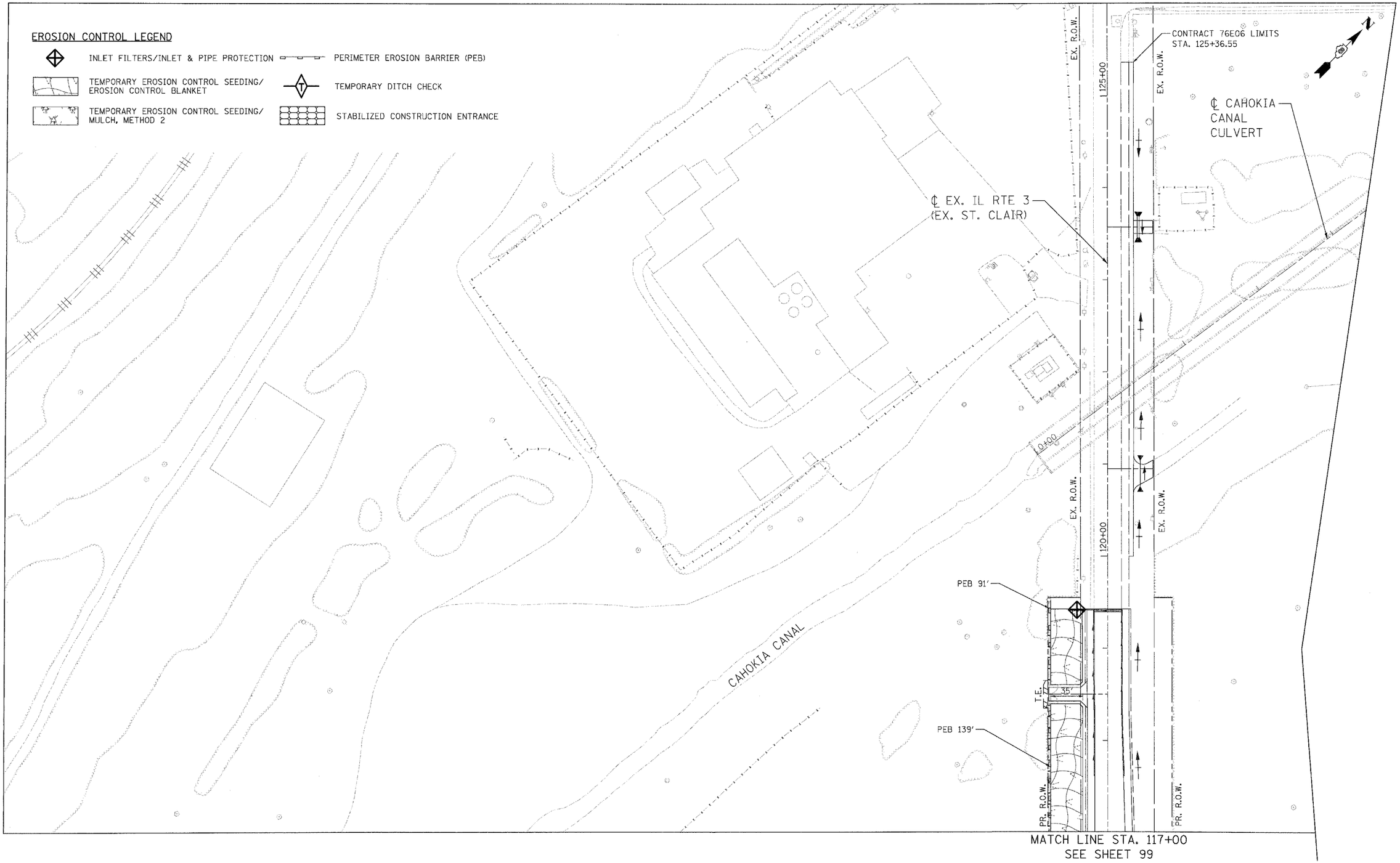
SCALE: 1"=50' | SHEET NO. 2 OF 13 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	97
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

EROSION CONTROL LEGEND

- INLET FILTERS/INLET & PIPE PROTECTION
- PERIMETER EROSION BARRIER (PEB)
- TEMPORARY EROSION CONTROL SEEDING/ EROSION CONTROL BLANKET
- TEMPORARY DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING/ MULCH, METHOD 2
- STABILIZED CONSTRUCTION ENTRANCE



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PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE

EX. IL RTE. 3 (ST. CLAIR AVE.)
EROSION CONTROL PLANS – STAGE 1
STA. 117+00 TO 126+00

SCALE: 1"=50' | SHEET NO. 3 OF 13 SHEETS | STA. | TO STA.

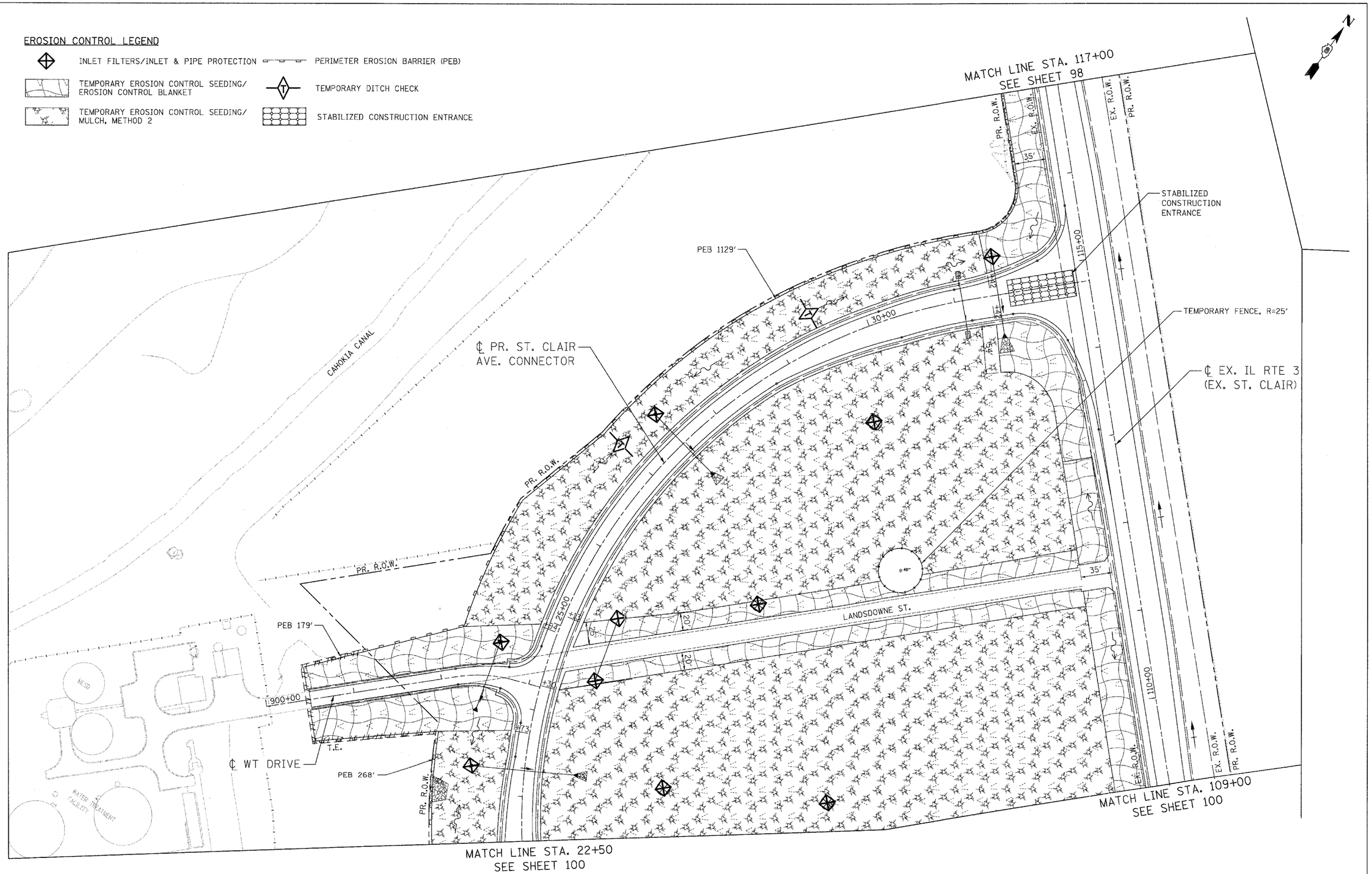
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	98
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT

$$$$ref_list_name$$$$ $$date_time$$$$ $$user$$ $$$$$

EROSION CONTROL LEGEND

- INLET FILTERS/INLET & PIPE PROTECTION
- PERIMETER EROSION BARRIER (PEB)
- TEMPORARY EROSION CONTROL SEEDING/ EROSION CONTROL BLANKET
- TEMPORARY DITCH CHECK
- TEMPORARY EROSION CONTROL SEEDING/ MULCH, METHOD 2
- STABILIZED CONSTRUCTION ENTRANCE



LIN ENGINEERING, LTD. Consulting Engineers Chatham, Illinois Westmont, Illinois	USER NAME = Plotted by Scott Wilkinson	DESIGNED - AS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED RELOCATED ILLINOIS ROUTE 3 INTERCHANGE	PR. ST. CLAIR AVE. CONNECTOR AND EX. IL RTE. 3 (ST. CLAIR AVE.) EROSION CONTROL PLANS – STAGE 1 STA. 109+00 TO STA. 117+00
		DRAWN - AS	REVISED -		
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - ST	REVISED -		
	PLOT DATE = 10/17/2011	DATE - 10/21/11	REVISED -		SCALE: 1"=50' SHEET NO. 4 OF 13 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	99
		CONTRACT NO. 76E06		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

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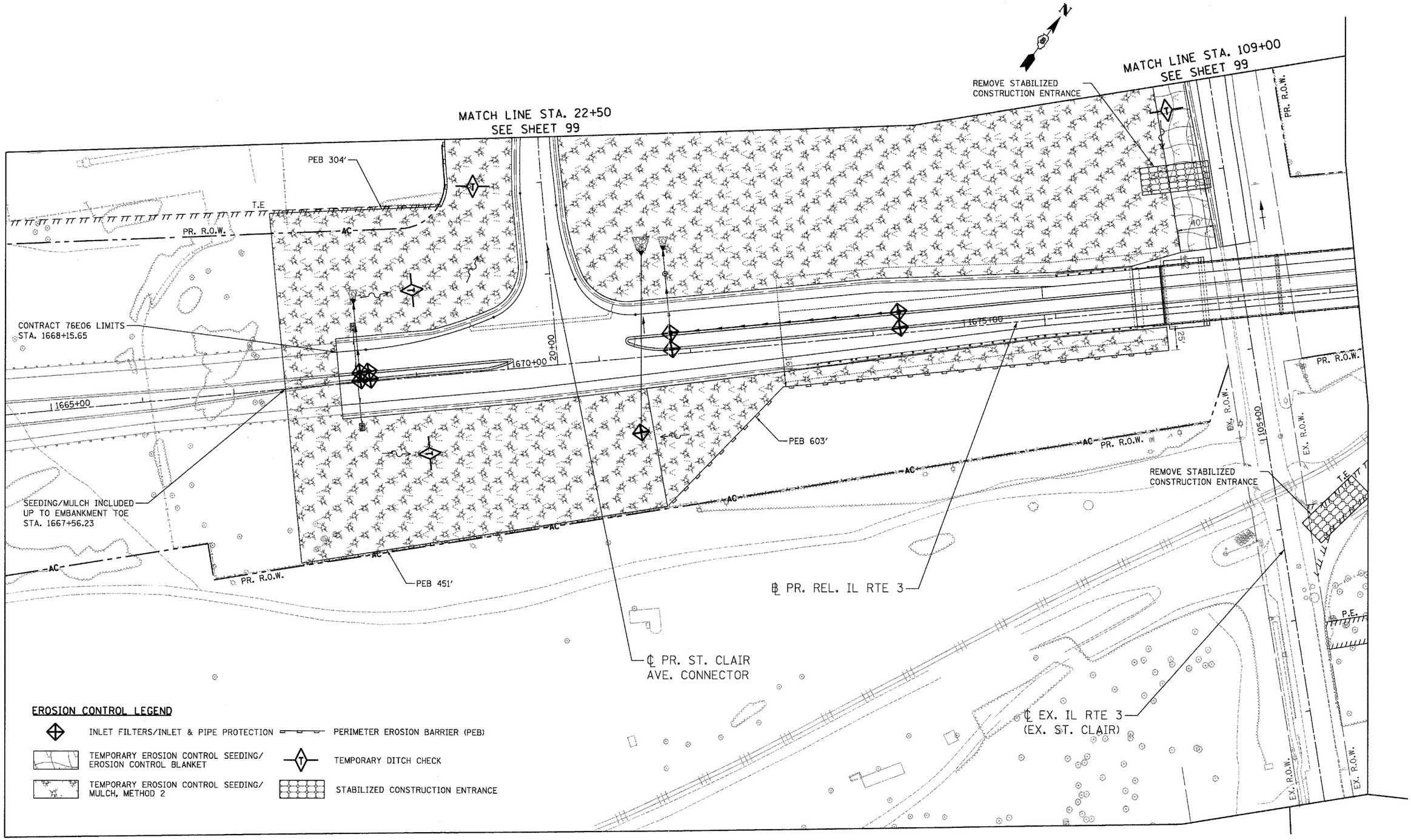

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Westmont, Illinois

USER NAME = $USER$	DESIGNED - AS	REVISED -	
	DRAWN - AS	REVISED -	
PLOT SCALE = $SCALE$	CHECKED - ST	REVISED -	
PLOT DATE = $DATE$	DATE - 10/21/11	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
PROPOSED RELOCATED IL ROUTE 3 INTERCHANGE

PR. REL. IL RTE. 3, PR. ST. CLAIR AVE. CONNECTOR AND EX. IL RTE. 3
EROSION CONTROL PLANS - STAGE 1
STA. 1665+00 TO STA. 1679+30

SCALE: 1"=50' | SHEET NO. 5 OF 13 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
998	82-2-1K	ST. CLAIR	353	100
		CONTRACT NO. 76E06		

FED. ROAD DIST. NO. | ILLINOIS | FED. AID PROJECT