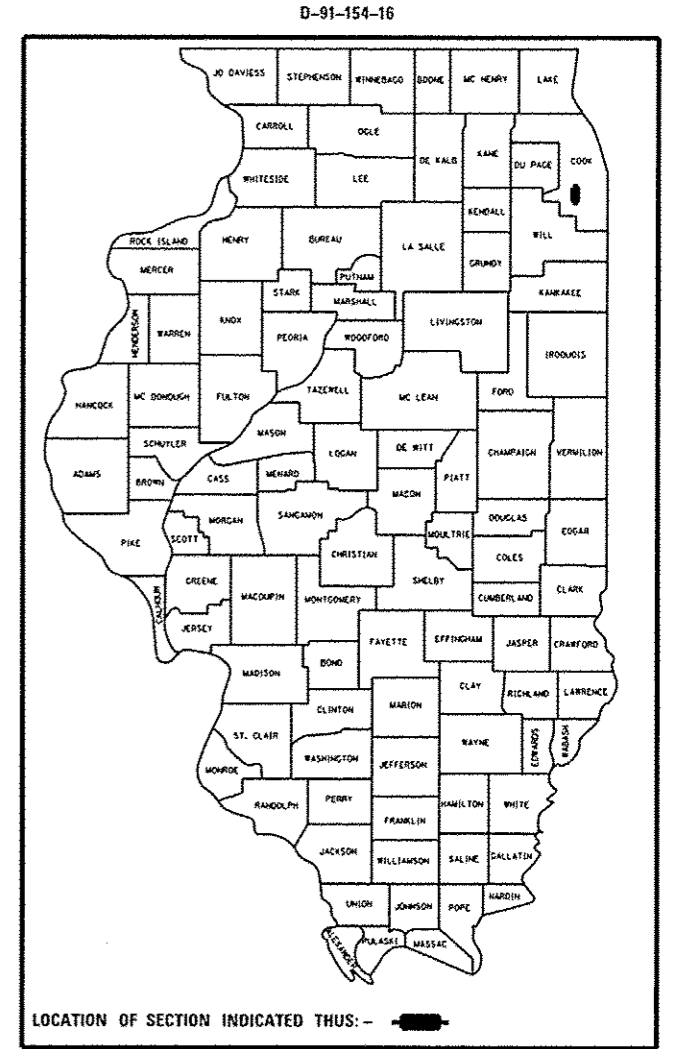


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	1
		ILLINOIS	CONTRACT NO.: 62A73	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF DES PLAINES

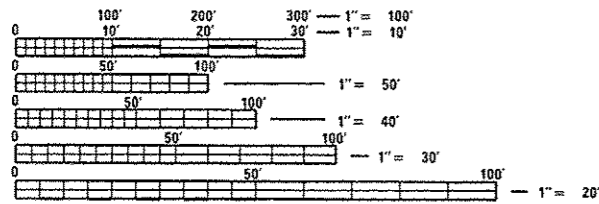
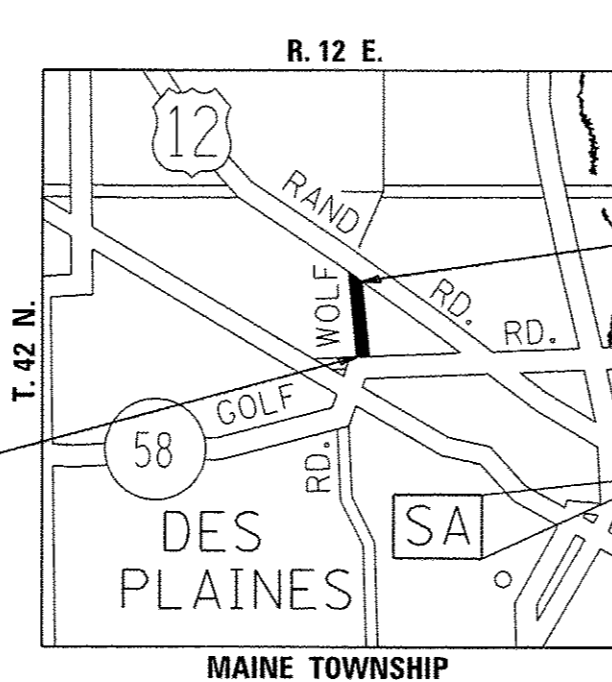
**PROPOSED HIGHWAY PLANS**  
**F.A.U. 2692 (WOLF RD.)**  
**US 12 (RAND RD.) TO IL 58 (GOLF RD.)**  
**RESURFACING (3P)**  
**SECTION NO.: 3539 RS-1**  
**PROJECT: ACM-2692(005)**  
**COOK COUNTY**  
**C-91-303-15**



**TRAFFIC DATA:**  
 2014 ADT - 11950  
 SPEED LIMIT - 30 MPH

PROJECT ENDS STA. 74 + 46

PROJECT BEGINS STA. 54 + 00



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1-800-892-0123 OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

GROSS AND NET LENGTH OF PROJECT - 2046 FEET - .39 MILES

CONTRACT NO. 62A73

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 21, 2016*  
*John Fortman*  
 REGION ENGINEER

*Dec 9 2016*  
*Maureen M. Addis*  
 acting ENGINEER OF DESIGN AND ENVIRONMENT

*Dec 9 2016*  
 DIRECTOR OF PROGRAM DEVELOPMENT 2

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION	STATE STANDARDS	GENERAL NOTES:
1	COVER SHEET	000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS	BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	442001-04 CLASS C AND D PATCHES	
3-5	SUMMARY OF QUANTITIES	604001-04 FRAMES AND LIDS, TYPE 1	10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
6-7	TYPICAL SECTIONS	604006-05 FRAMES AND GRATES, TYPE 23	
8-9	ROADWAY AND PAVEMENT MARKING PLANS	606001-06 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER	THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE CITY OF DES PLAINES.
10-14	SIDEWALK DETAILS	701101-05 OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE	
15	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB AND EDGE OF SHOULDER GREATER THAN OR EQUAL TO 15 FT. (4.5 M) (BD-01)	701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS LESS THAN OR EQUAL TO 45 MPH	THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
16	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB AND EDGE OF SHOULDER LESS THAN OF EQUAL TO 15 FT. (4.5 M) (BD-02)	701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON-TRAVERSABLE MEDIAN	
17	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)	701602-08 URBAN LANE CLOSURE, MULTILANE, 2W WITH BI-DIRECTIONAL LEFT TURN LANE	ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
18	DETAILS FOR PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION	
19	DETAILS FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	701901-06 TRAFFIC CONTROL DEVICES	ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
20	BUTT JOINT AND HMA TAPER DETAILS (BD-32)	780001-05 TYPICAL PAVEMENT MARKING	
21	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)	781001-04 TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS	ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
22	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)		LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
23	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)		DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
24	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)		IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
25	PAVEMENT MARKING--LETTERS AND SYMBOLS FOR TRAFFIC STAGING (TC-16)		FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.
26	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)		BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
27	DETECTOR LOOP LOCATION DETAILS		WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3).
28	ARTERIAL ROAD INFORMATION SIGNING (TC-22)		

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL CENTER AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)

THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC ENGINEER, AT CORY.JUCIUS@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

URBAN					URBAN				
SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE		SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005					
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
20200100	EARTH EXCAVATION	CU YD	29	29					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	432	432					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	6	6					
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	6	6					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	6	6					
25200110	SODDING, SALT TOLERANT	SO YD	432	432					
25200200	SUPPLEMENTAL WATERING	UNIT	12	12					
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	28	28					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	6796	6796					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	15	15					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	416	416					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	117	117					
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3	3					
40603365	HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	987	987					
42001300	PROTECTIVE COAT	SO YD	501	501					
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	33	33					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2352	2352					
42400800	DETECTABLE WARNINGS	SO FT	84	84					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	10068	10068					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	61	61					
44000600	SIDEWALK REMOVAL	SO FT	2495	2495					
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	89	89					
60255500	MANHOLES TO BE ADJUSTED	EACH	6	6					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	30	30					
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					

\* SPECIALTY ITEMS  
 NON-PARTICIPATING ITEMS

14

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		SUMMARY OF QUANTITIES FAU 2692 (WOLF RD.)--US 12 (RAND RD.) TO IL 58 (GOLF RD.)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED -	REVISED -						2692	3539 RS-1	COOK	28	3
		DATE -	REVISED -						CONTRACT NO. 62A73				
									FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

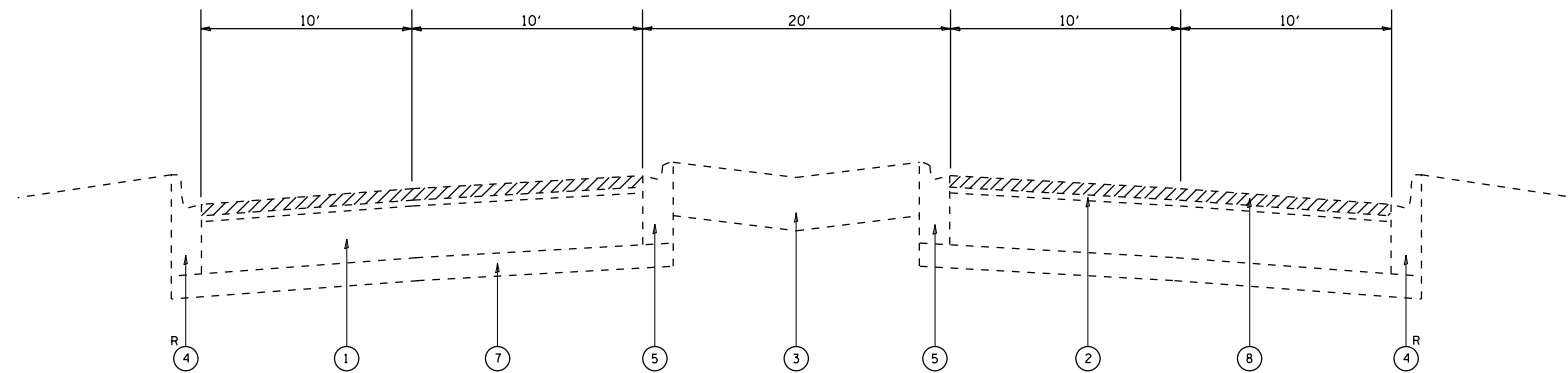
SUMMARY OF QUANTITIES			URBAN CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
67100100	MOBILIZATION	LSUM	1	1			
70102630	TRAFFIC CONTROL AND PROTECTION. STANDARD 701601	LSUM	1	1			
70102632	TRAFFIC CONTROL AND PROTECTION. STANDARD 701602	LSUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION. STANDARD 701701	LSUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	244	244			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	41	41			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	37	37			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4045	4045			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	101	101			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	85	85			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	26	26			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	122	122			
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	37	37			

SUMMARY OF QUANTITIES			URBAN CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4045	4045			
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	101	101			
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	85	85			
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	26	26			
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	25	25			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	23	23			
*85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1	1			
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	72	72			
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	1537	1537			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	5			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1110	1110			

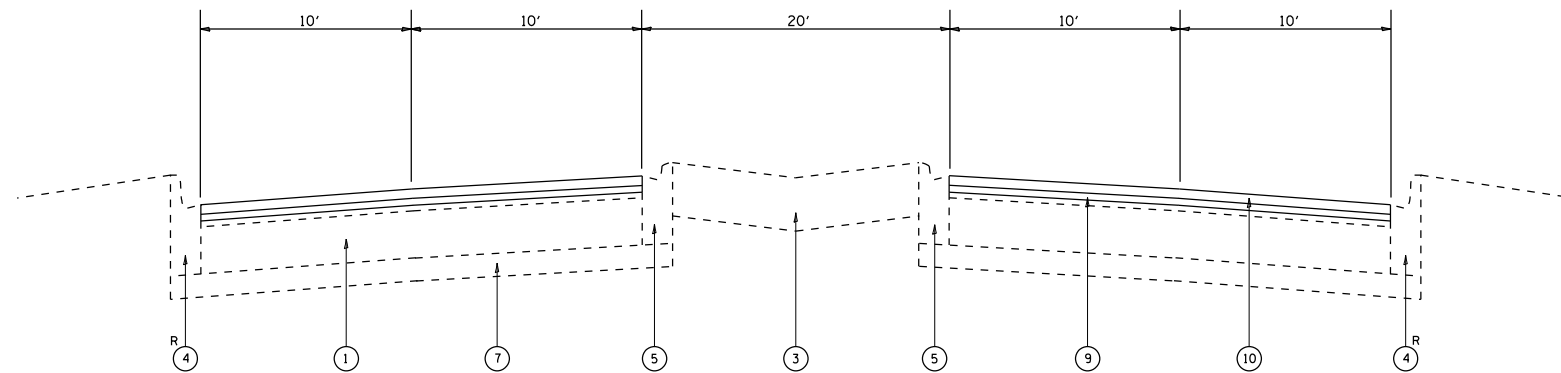
\* SPECIALTY ITEMS  
 NON-PARTICIPATING ITEMS

URBAN					CONSTRUCTION TYPE CODE											
SUMMARY OF QUANTITIES			TOTAL QUANTITIES	80% FED 20% STATE												
CODE NO	ITEM	UNIT		0005												
☐Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	7	7												
Z0030850	TEMPORARY INFORMATION SIGNING	50 FT	51.4	51.4												

\* SPECIALTY ITEMS  
 ☐ NON-PARTICIPATING ITEMS



EXISTING TYPICAL CROSS SECTION  
STA. 54+00 TO 71+77



PROPOSED TYPICAL CROSS SECTION  
STA. 54+00 TO 71+77

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±8"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±4"
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
  - ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑥ EXISTING P.C.C. SIDEWALK
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT  
(AS DIRECTED BY THE ENGINEER)

**MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
<b>PATCHING</b>		
CLASS "D" PATCHES (HMA BINDER COURSE, IL-19MM)	4% @ 70 GYR.	QC / OA
<b>PAVEMENT RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (IL 9.5MM)	4% @ 70 GYR.	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.	QC / OA
<b>DRIVEWAYS (PRIVATE ENTRANCE)</b>		
HOT-MIX ASPHALT BASE COURSE, 6" (HMA BINDER IL-19MM)	4% @ 50 GYR.	OCP
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2" (IL-9.5MM)	4% @ 50 GYR.	OCP
OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (OCP); QUALITY CONTROL/ QUALITY ASSURANCE (QC/ OA); PAY FOR PERFORMANCE (PFP)		

**NOTES:**

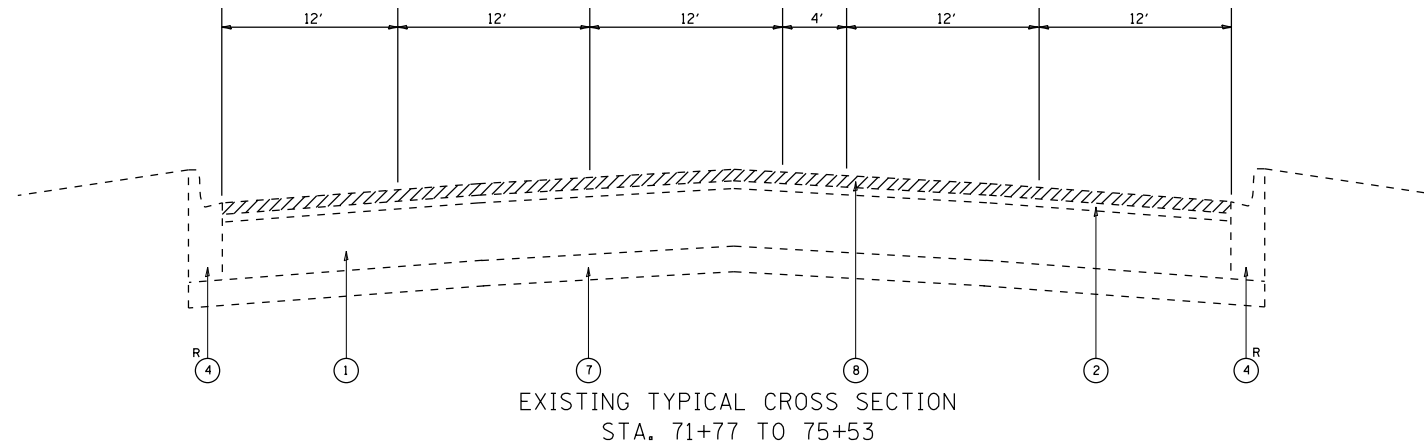
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

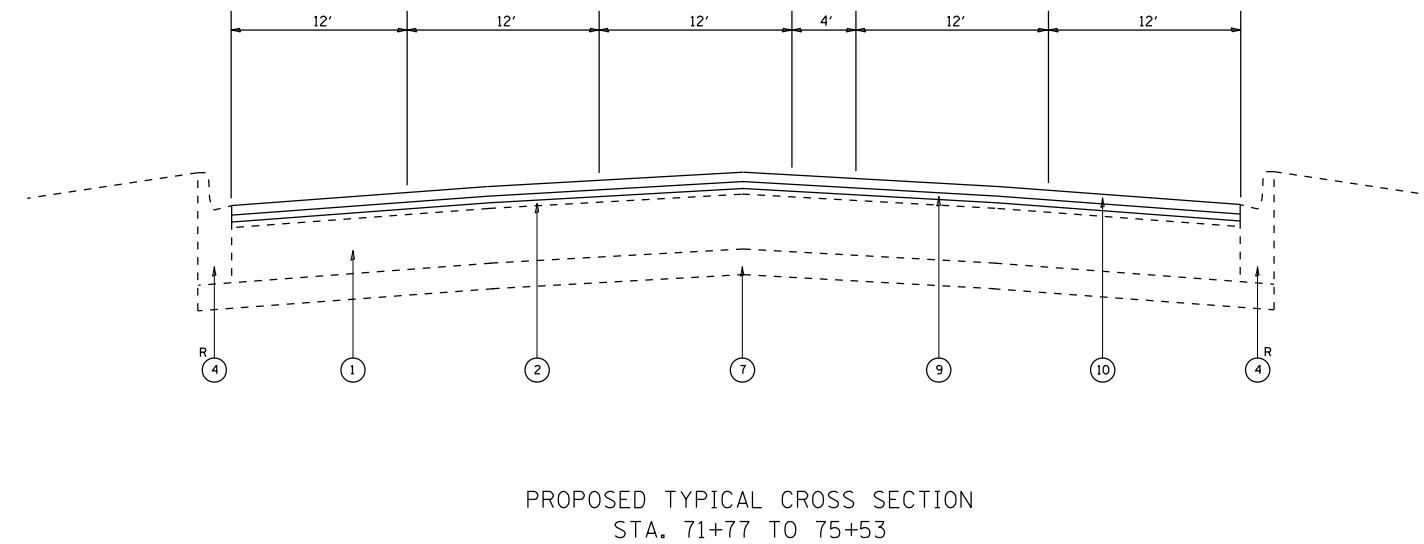
THE PATCHING SHALL BE DONE PRIOR TO THE MILLING

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

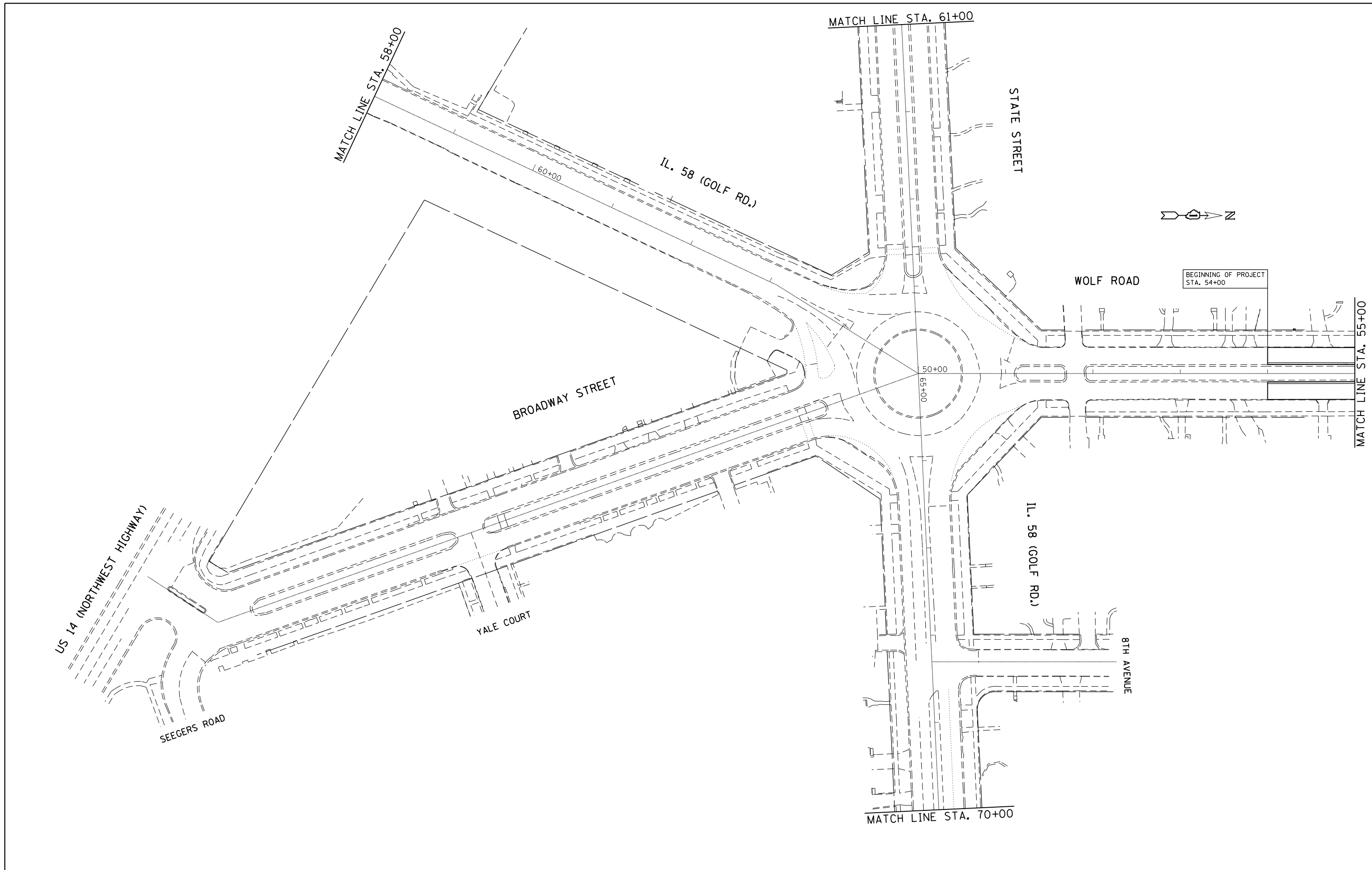
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS



- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT ±8"
  - ② EXISTING HOT-MIX ASPHALT SURFACE, ±4"
  - ③ EXISTING LANDSCAPE MEDIAN
  - ④ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12
  - ⑤ EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - ⑥ EXISTING P.C.C. SIDEWALK
  - ⑦ EXISTING STABILIZED SUB-BASE
  - ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
  - ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
  - ⑩ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1¾"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT  
(AS DIRECTED BY THE ENGINEER)



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS</b>				F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 62A73				
	PLOT DATE = 10/31/2016								ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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	PLOT DATE = 10/26/2016	DATE -	REVISED -

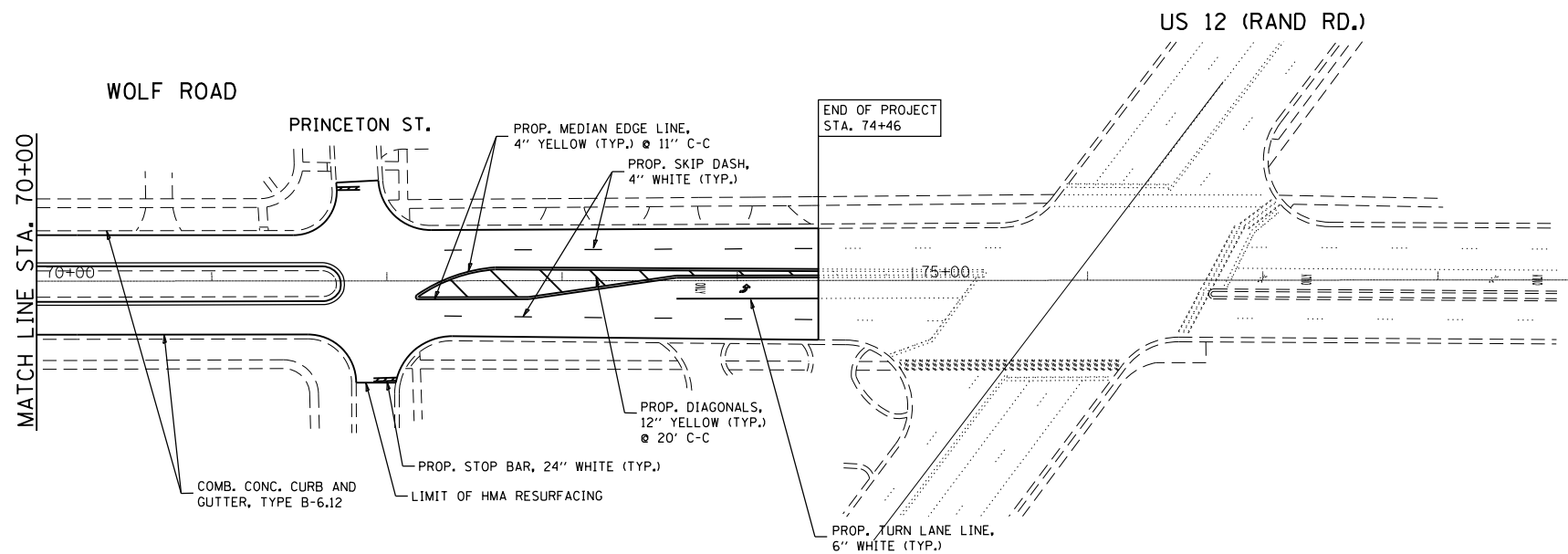
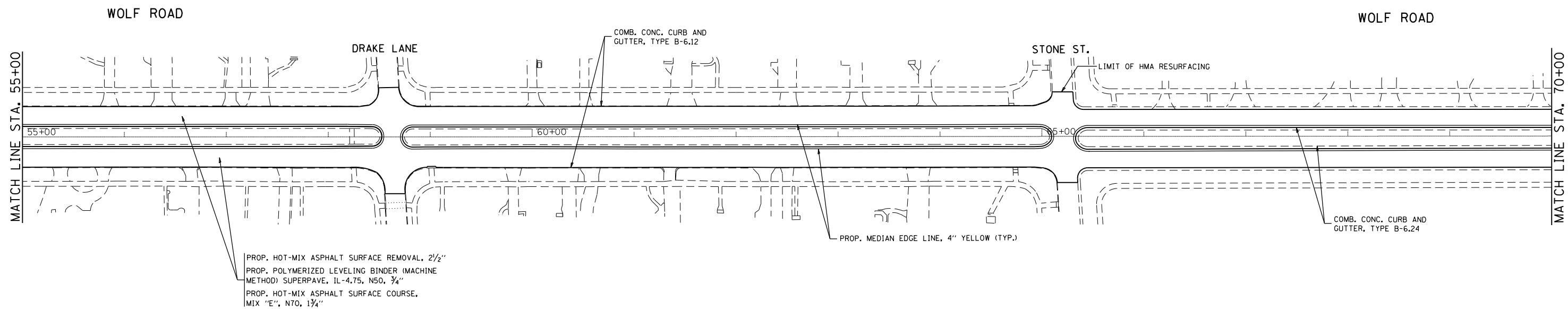
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
FAU 2692 (WOLF RD.)—US 12 (RAND RD.) TO IL 58 (GOLF RD.)**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	8
CONTRACT NO. 62A73				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.





FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -
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	PLOT DATE = 10/26/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN  
FAU 2692 (WOLF RD.)—US 12 (RAND RD.) TO IL 58 (GOLF RD.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	9
CONTRACT NO. 62A73				
ILLINOIS FED. AID PROJECT				

WOLF RD INTERSECTION	EARTH EXCAVATION	TOPSOIL FURNISH AND PLACE, 4"	NITROGEN FERTILIZER NUTRIENT	PHOSPHORUS FERTILIZER NUTRIENT	POTASSIUM FERTILIZER NUTRIENT	SODDING, SALT TOLERANT	SUPPLEMENTAL WATERING	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	DETECTABLE WARNINGS	COMBINATION CURB AND GUTTER REMOVAL	SIDEWALK REMOVAL	MANHOLES TO BE ADJUSTED	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
	CU YD	SO YD	POUND	POUND	POUND	SO YD	UNIT	SO FT	SO FT	FOOT	SO FT	EACH	FOOT	FOOT
	20200100	21101615	25000400	25000500	25000600	25200110	25200200	42400200	42400800	44000500	44000600	60255500	60603800	20004562
PRINCETON STREET	10.1	103.1	2.1	2.1	2.1	103.1	4.3	1074	54	10	1099	2	10	98
STONE STREET	18.6	177.6	3.5	3.5	3.5	177.6	7.5	1344	40	0	1396	4	0	109
TOTAL	28.7	280.7	5.6	5.6	5.6	280.7	11.8	2418	94	10	2495	6	10	207

FILE NAME :  
62A73-eh1-S0081.dgn  
Default

USER NAME : Duhita  
PLOT SCALE : 2.0000' / in.  
PLOT DATE : 10/19/2016

DESIGNED -  
DRAWN -  
CHECKED -  
DATE -

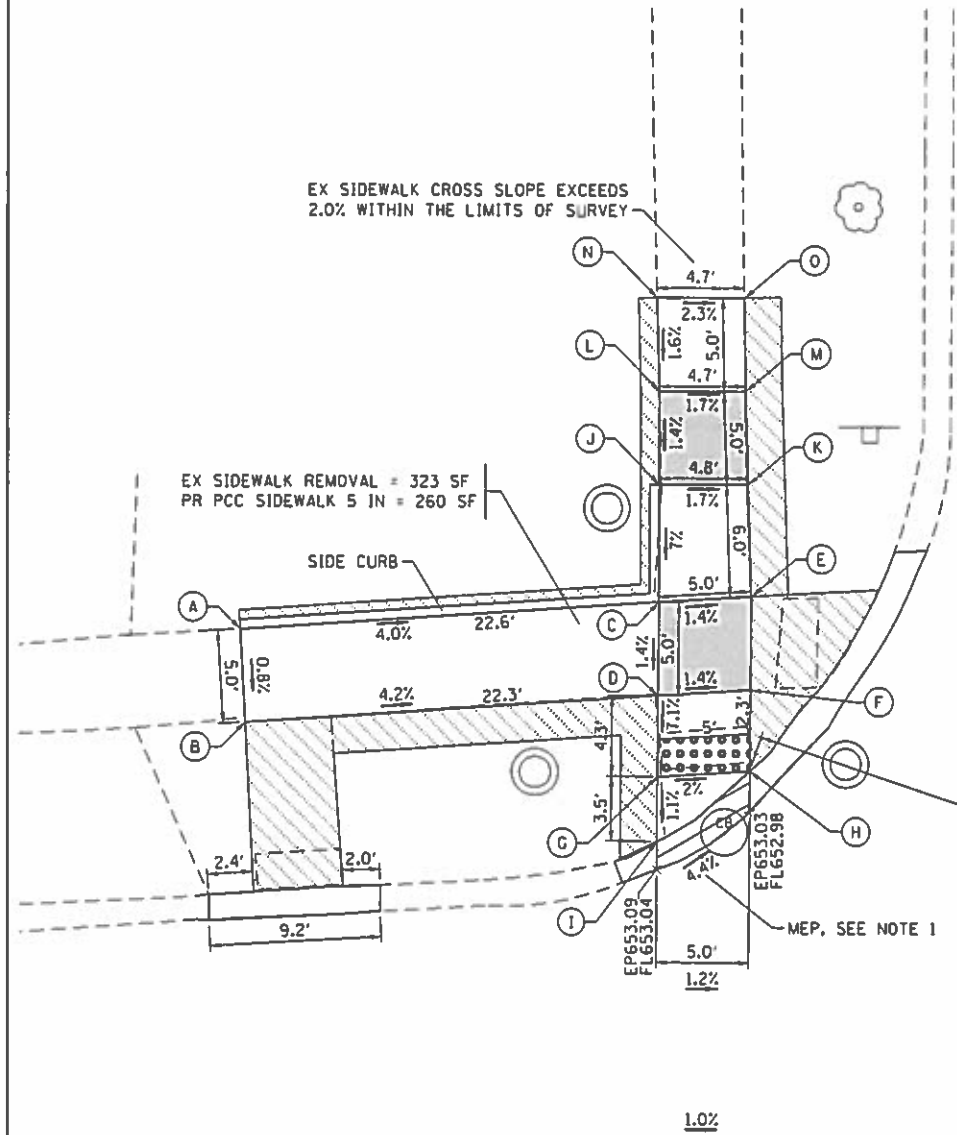
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN - SCHEDULE OF QUANTITIES  
WOLF ROAD. (PRINCETON ST. TO STONE ST.)

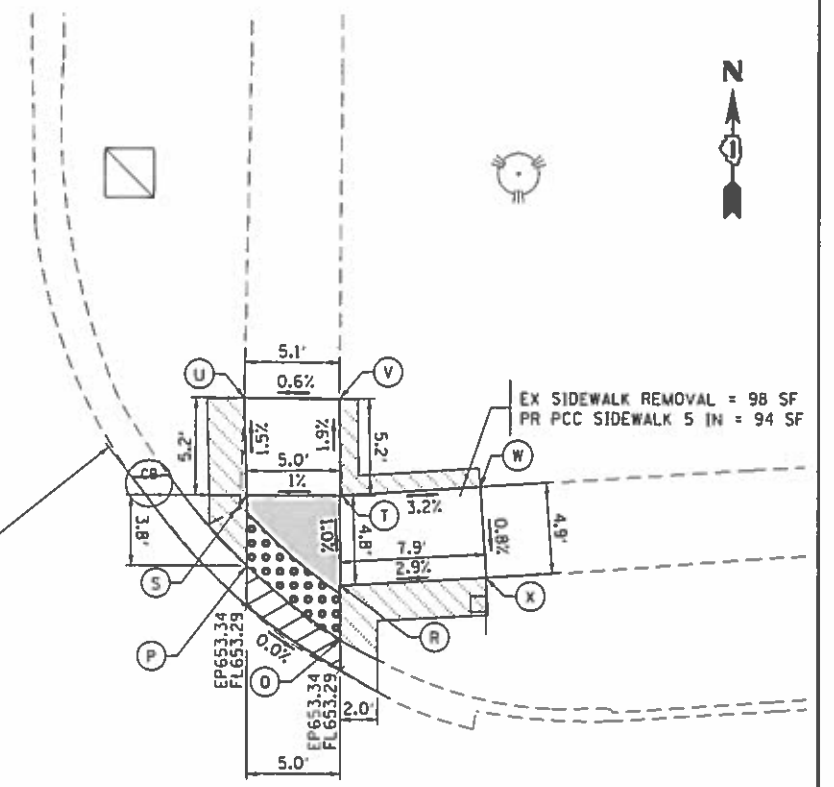
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	10
CONTRACT NO. 62A73			ILLINOIS FED. AID PROJECT	



NOTES:  
 1) PROPOSED CROSS SLOPE EXCEEDS CURRENT POLICY VALUE. RAMP IS DESIGNED TO THE MAXIMUM EXTENT PRACTICABLE (MEP).

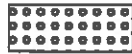

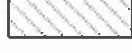

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B	654.36	MATCH	O	653.98	MATCH
C	653.50	TW	P	653.33	TW=TC
D	653.43	TW	Q	653.33	TW=TC
E	653.43	TW	R	653.38	TW
F	653.36	TW	S	653.38	TW
G	653.12	TW	T	653.43	TW
H	653.02	TW=TC	U	653.30	MATCH
I	653.08	TW=TC	V	653.33	MATCH
J	653.94	TW	W	653.19	MATCH
K	653.86	TW	X	653.15	MATCH
L	654.01	TW			
M	653.93	TW			



PRINCETON ST

WOLF RD

**LEGEND**

-  DETECTABLE WARNINGS
-  DEPRESSED CURB AND GUTTER
-  SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD
-  PROPOSED SIDEWALK TURNING SPACE
- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EDP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION
- EP EXISTING EDGE-OF-PAVEMENT ELEVATION
- FL PROPOSED DEPRESSED CURB FLOWLINE ELEVATION

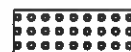



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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

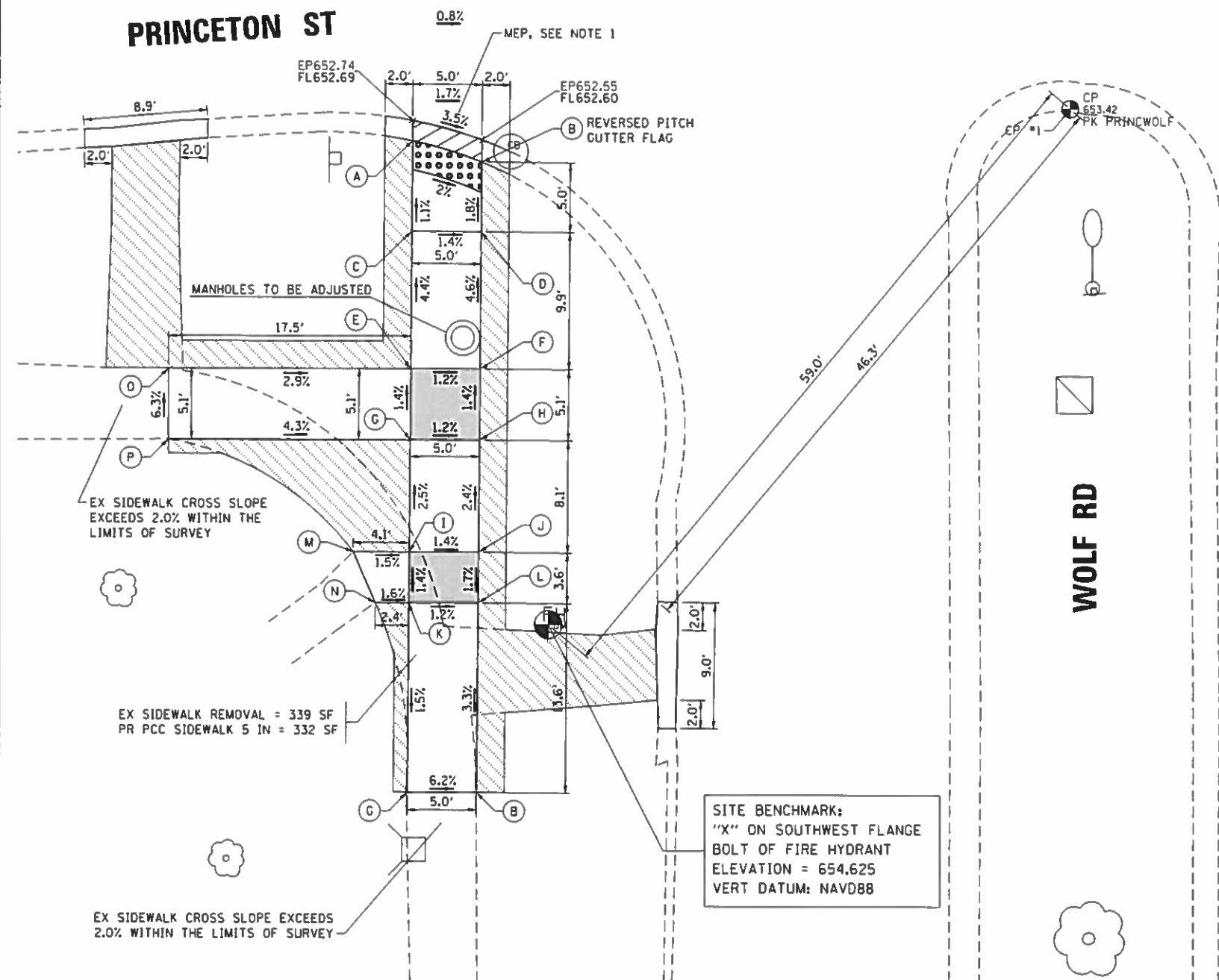
SIDWALK DETAIL PLAN	
WOLF ROAD AT PRINCETON STREET	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2692	SECTION 3539 RS-1	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 11
CONTRACT NO. 62473			ILLINOIS FED. AID PROJECT	

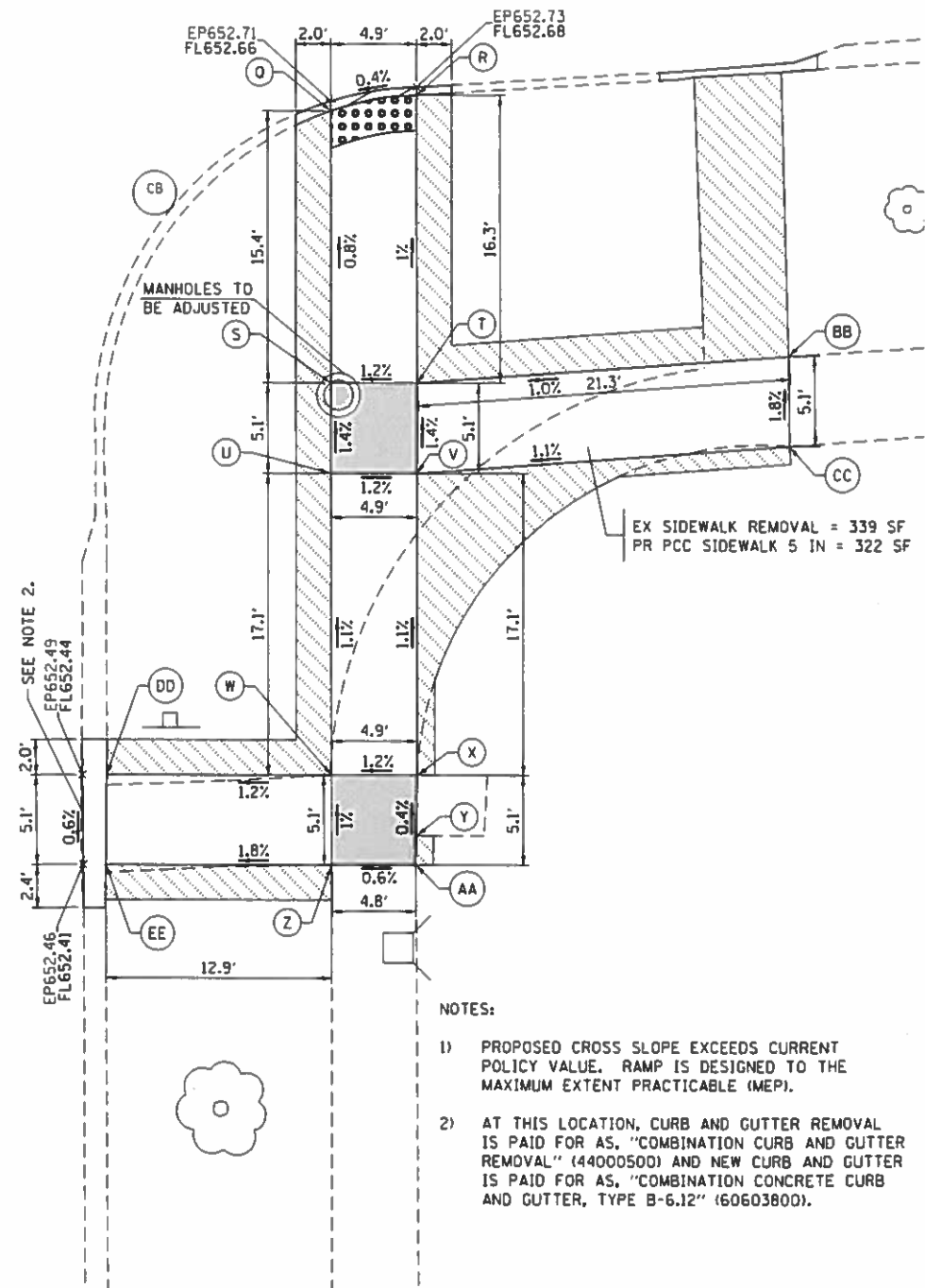
**LEGEND**

-  DETECTABLE WARNINGS
-  DEPRESSED CURB AND GUTTER
-  SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD
-  PROPOSED SIDEWALK TURNING SPACE

- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EOP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION
- EP EXISTING EDGE-OF-PAVEMENT ELEVATION
- FL PROPOSED DEPRESSED CURB FLOWLINE ELEVATION



POINT	ELEV	NOTE
A	652.73	TW=TC
B	652.64	TW=TC
C	652.80	TW
D	652.73	TW
E	653.24	TW
F	653.18	TW
G	653.31	TW
H	653.25	TW
I	653.51	TW
J	653.44	TW
K	653.44	MATCH
L	653.38	MATCH
M	653.57	MATCH
N	653.48	MATCH
O	653.74	MATCH
P	654.06	MATCH
Q	652.70	TW=TC
R	652.72	TW=TC
S	652.82	TW
T	652.88	TW
U	652.89	TW
V	652.95	TW
W	653.09	TW
X	653.15	TW
Y	653.16	MATCH
Z	653.14	MATCH
AA	653.17	MATCH
BB	653.10	MATCH
CC	653.19	MATCH
DD	652.94	TW=TC
EE	652.91	TW=TC



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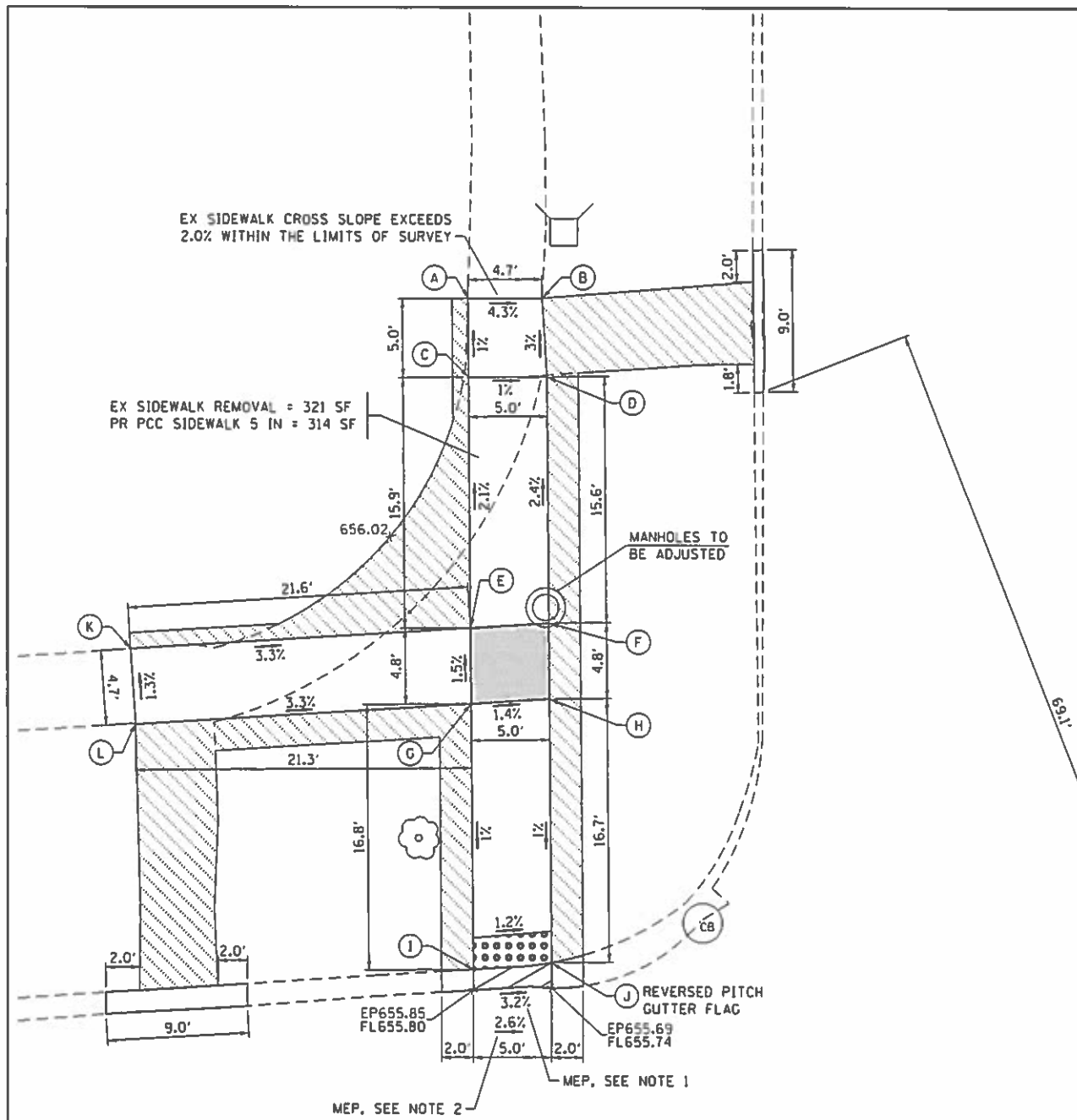
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PLOT DATE = 11/2/2016	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN**  
**WOLF ROAD AT PRINCETON STREET**

SCALE: 1" = 5'  
SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	12
CONTRACT NO. 62473				
ILLINOIS FED. AID PROJECT				



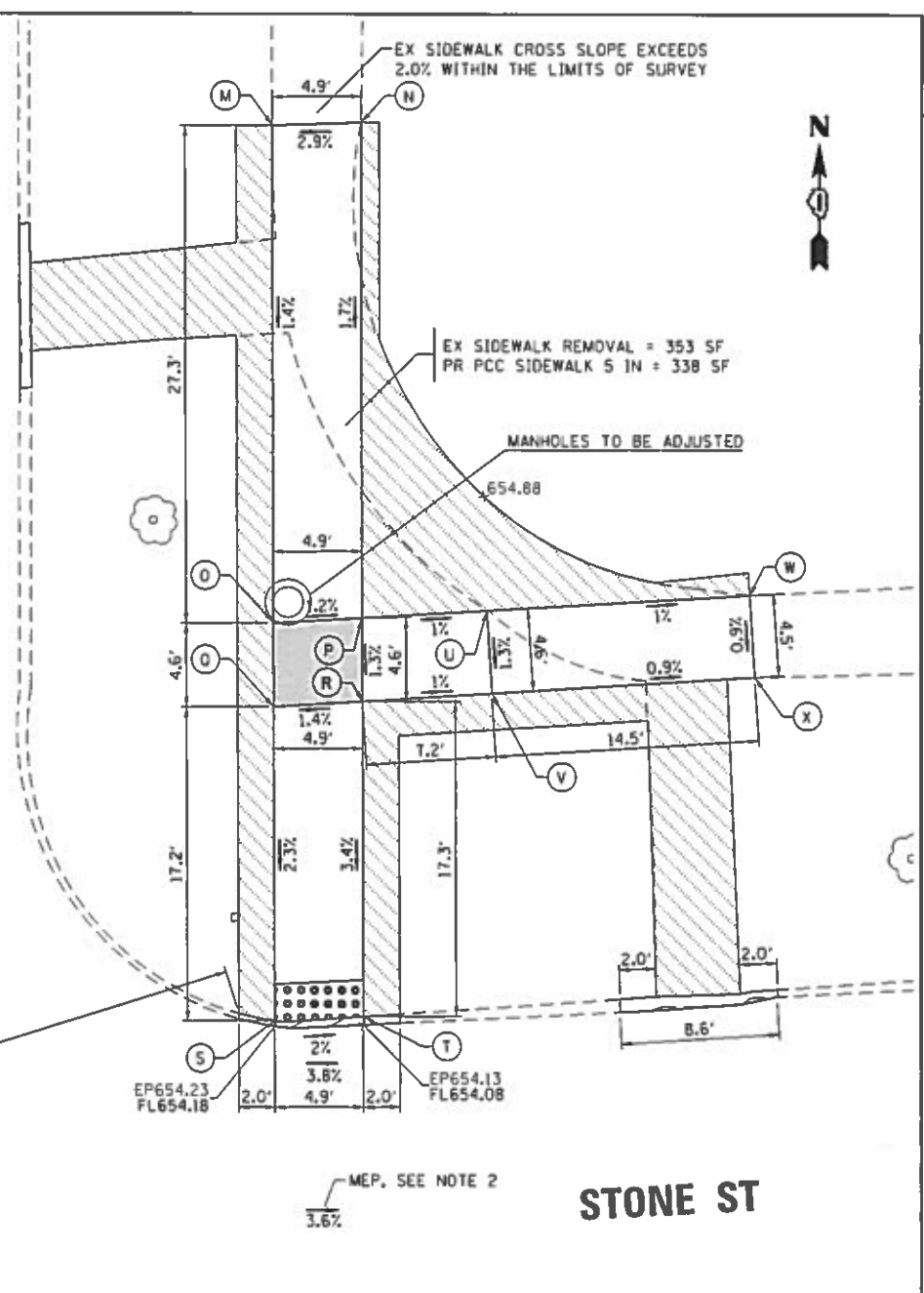
**WOLF RD**

POINT	ELEV	NOTE
A	655.55	MATCH
B	655.35	MATCH
C	655.60	TW
D	655.50	TW
E	655.94	TW
F	655.87	TW
G	656.01	TW
H	655.94	TW
I	655.84	TW=TC
J	655.78	TW=TC
K	656.65	MATCH
L	656.71	MATCH

POINT	ELEV	NOTE
M	655.06	MATCH
N	655.20	MATCH
O	654.69	TW
P	654.75	TW
Q	654.62	TW
R	654.69	TW
S	654.22	TW=TC
T	654.12	TW=TC
U	654.82	TW
V	654.76	TW
W	654.67	MATCH
X	654.63	MATCH

**NOTES:**

- 1) PROPOSED CROSS SLOPE EXCEEDS CURRENT POLICY VALUE. RAMP IS DESIGNED TO THE MAXIMUM EXTENT PRACTICABLE (MEP).
- 2) EXISTING CROSSWALK CROSS SLOPE EXCEEDS CURRENT POLICY VALUE.



**STONE ST**

**LEGEND**

- DETECTABLE WARNINGS
- DEPRESSED CURB AND GUTTER
- SIDEWALK REMOVAL REPLACE W/TOP SOIL & SOD
- PROPOSED SIDEWALK TURNING SPACE
- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EOP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION
- EP EXISTING EDGE-OF-PAVEMENT ELEVATION
- FL PROPOSED DEPRESSED CURB FLOWLINE ELEVATION





FILE NAME 62A73.sht plan83.dgn	USER NAME Dwhite	DESIGNED -	REVISED -
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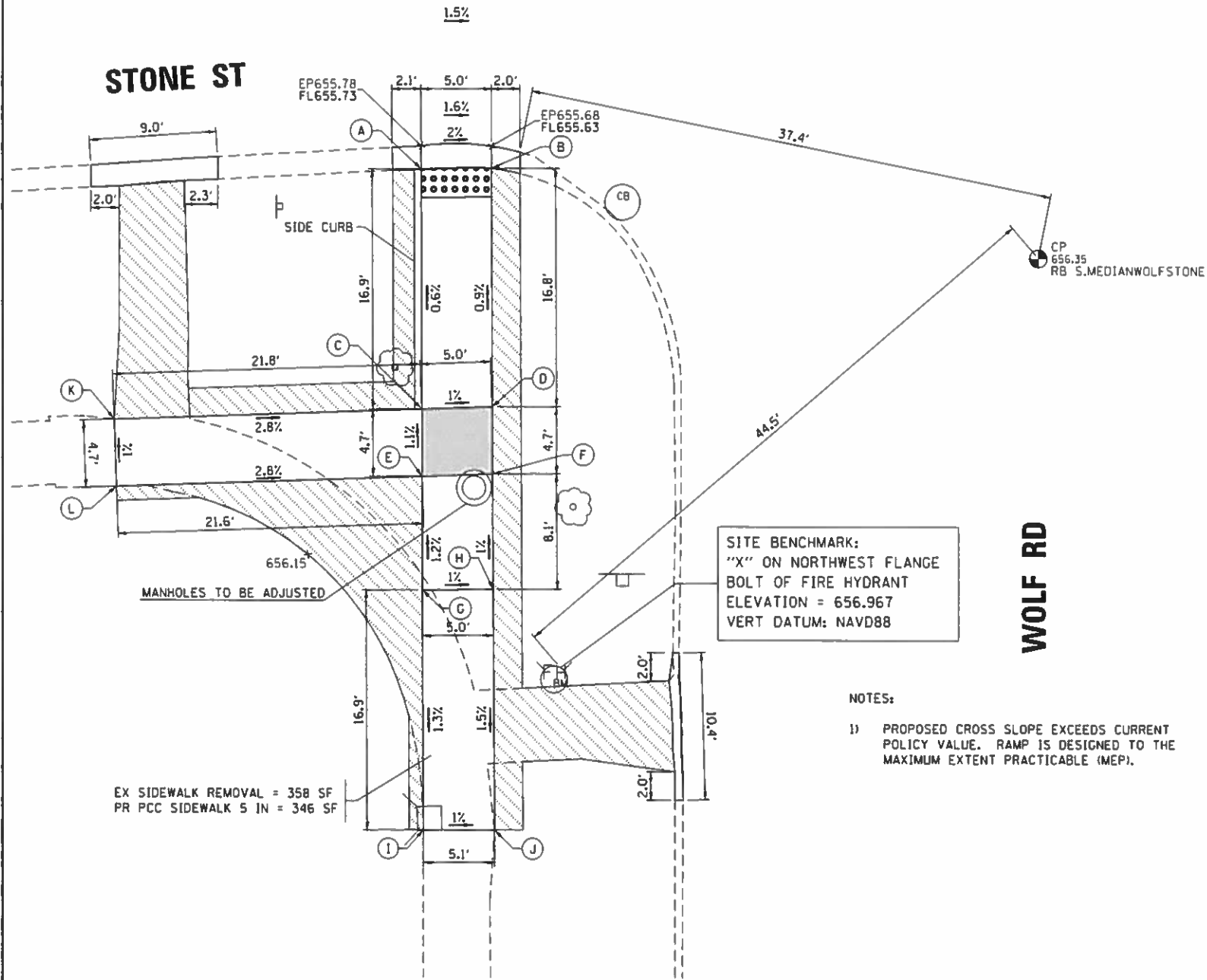
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SIDEWALK DETAIL PLAN WOLF ROAD AT STONE STREET</b>	
SCALE: 1" = 5'	SHEET OF SHEETS STA. TO STA.

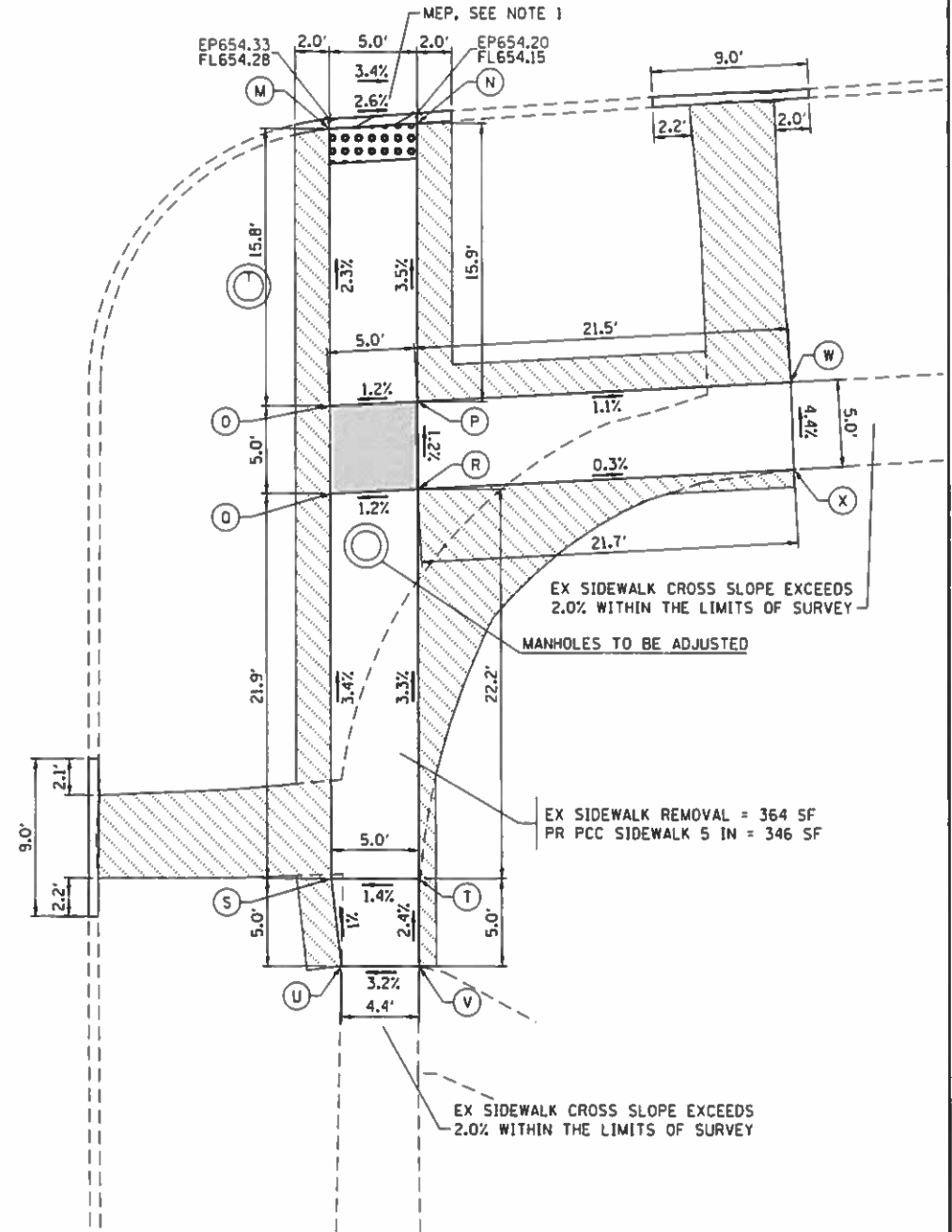
F.A.U. RTE. 2692	SECTION 3539 RS-1	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 13
CONTRACT NO. 62A73			ILLINOIS FED. AID PROJECT	

**LEGEND**

-  DETECTABLE WARNINGS
-  DEPRESSED CURB AND GUTTER
-  SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD
-  PROPOSED SIDEWALK TURNING SPACE
  
- TW PROPOSED TOP-OF-WALK ELEVATION
- TC PROPOSED TOP-OF-CURB ELEVATION
- EOP PROPOSED EDGE-OF-PAVEMENT ELEVATION
- MATCH MATCH EXISTING ELEVATION
- EP EXISTING EDGE-OF-PAVEMENT ELEVATION
- FL PROPOSED DEPRESSED CURB FLOWLINE ELEVATION



POINT	ELEV	NOTE
A	655.77	TW=TC
B	655.67	TW=TC
C	655.88	TW
D	655.83	TW
E	655.93	TW
F	655.88	TW
G	656.03	TW
H	655.96	TW
I	655.81	MATCH
J	655.71	MATCH
K	656.49	MATCH
L	656.54	MATCH
M	654.32	TW=TC
N	654.19	TW=TC
O	654.69	TW
P	654.75	TW
Q	654.75	TW
R	654.81	TW
S	655.41	TW
T	655.48	TW
U	655.46	MATCH
V	655.60	MATCH
W	654.52	MATCH
X	654.74	MATCH



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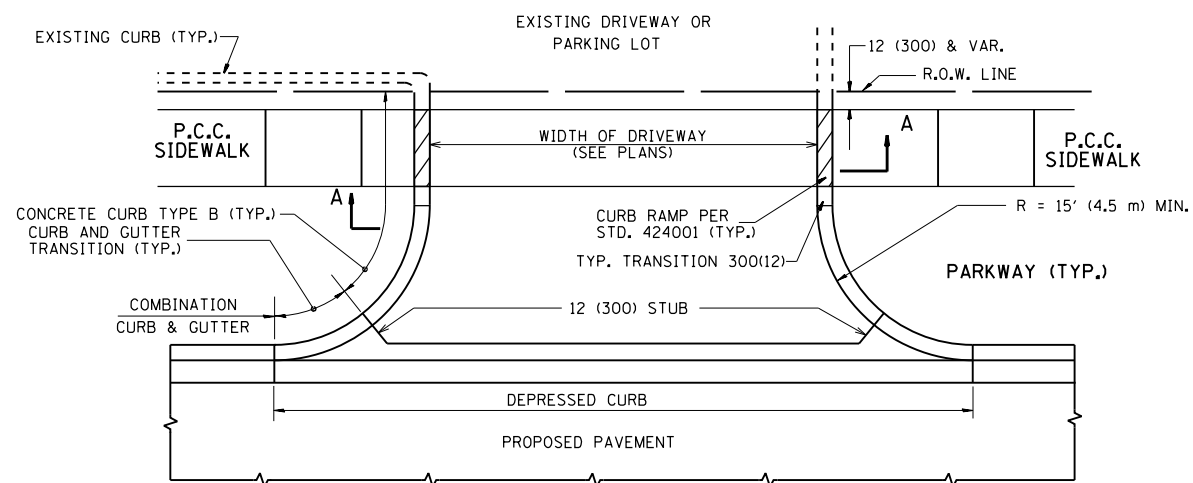
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 PLOT DATE = 10/19/2016

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

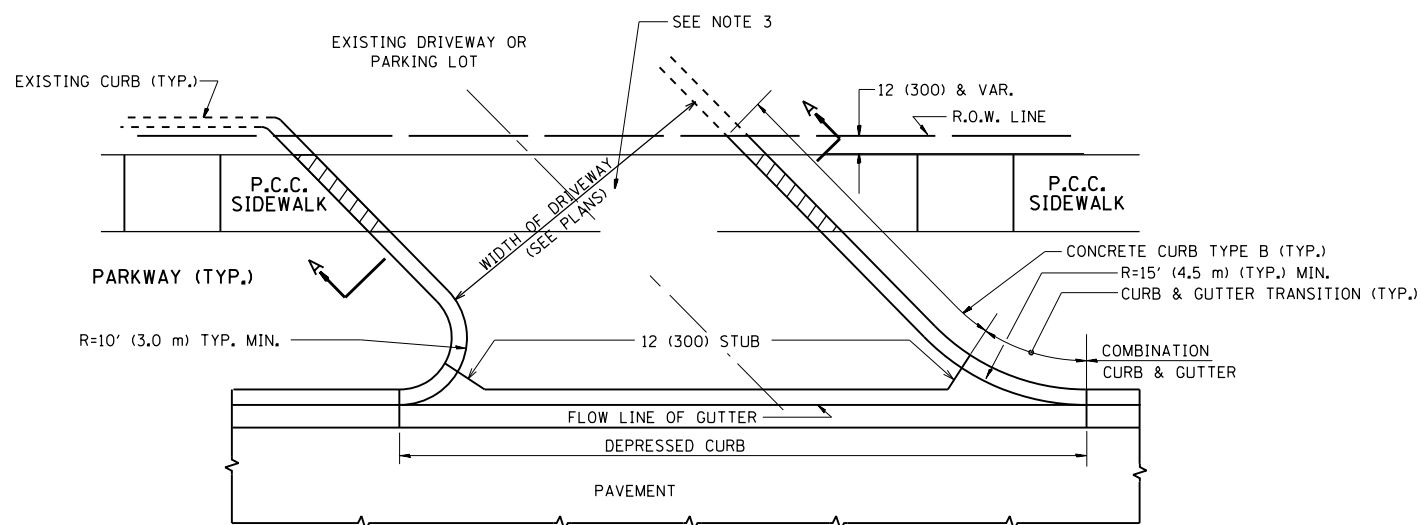
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SIDWALK DETAIL PLAN  
 WOLF ROAD AT STONE STREET  
 SCALE: 1" = 5'  
 SHEET OF SHEETS STA. TO STA.

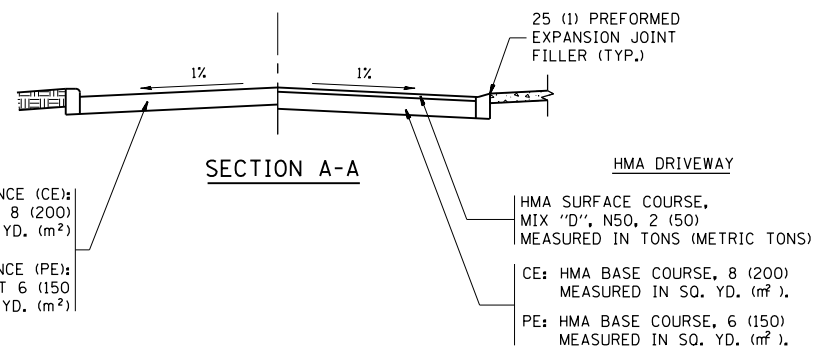
F.A.U. RTE. 2692	SECTION 3539 RS-1	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 14
CONTRACT NO. 62A73			[ILLINOIS] FED. AID PROJECT	



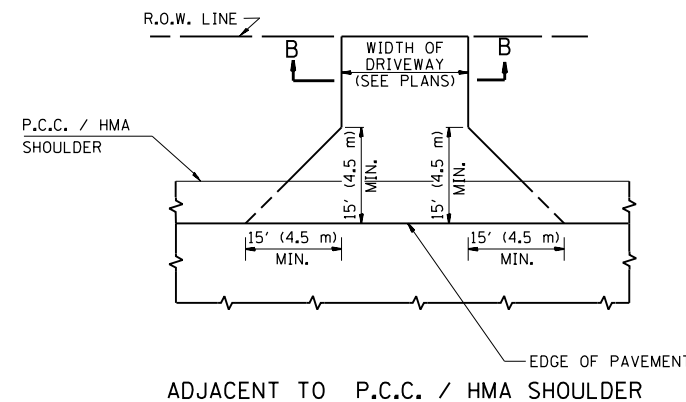
WITH CONCRETE CURB, TYPE B



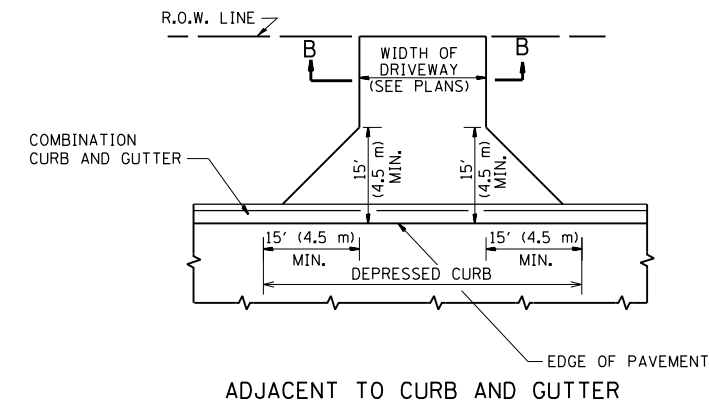
WITH CONCRETE CURB, TYPE B



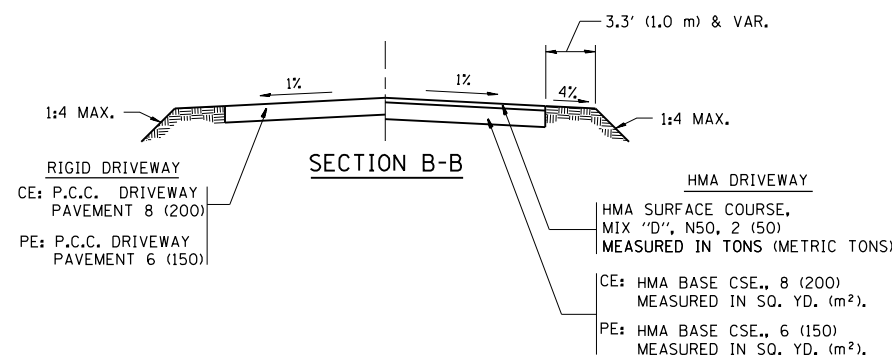
SECTION A-A



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



SECTION B-B

RURAL FIELD ENTRANCE (FE)

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

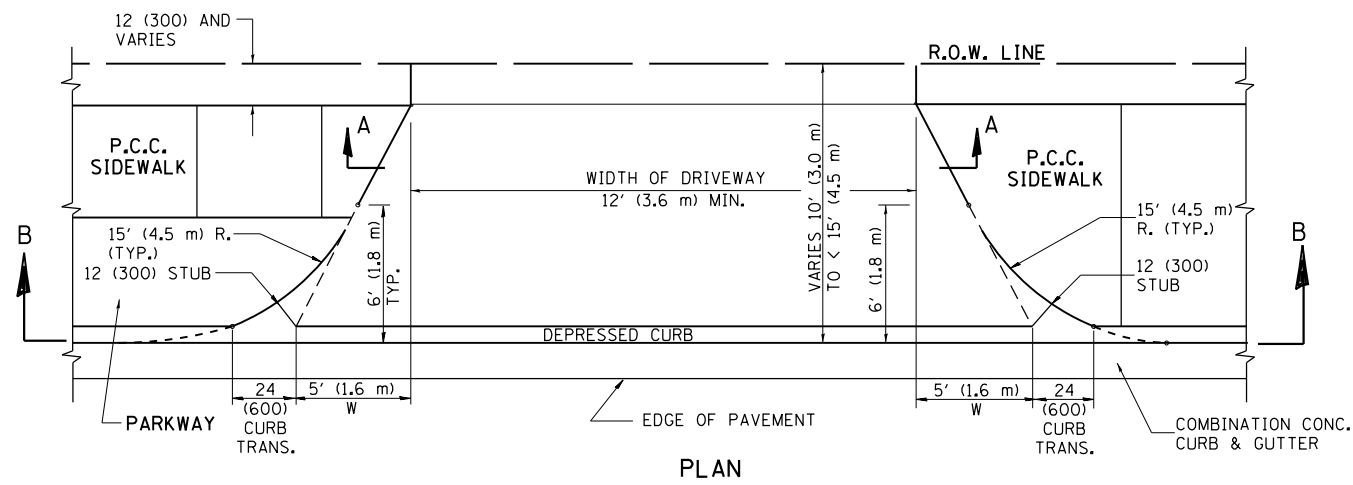
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PLOT SCALE = 100.0002' / in.		DATE - 11-04-95	REVISED - R. BORO 06-11-08
PLOT DATE = 10/26/2016			REVISED - R. BORO 09-06-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

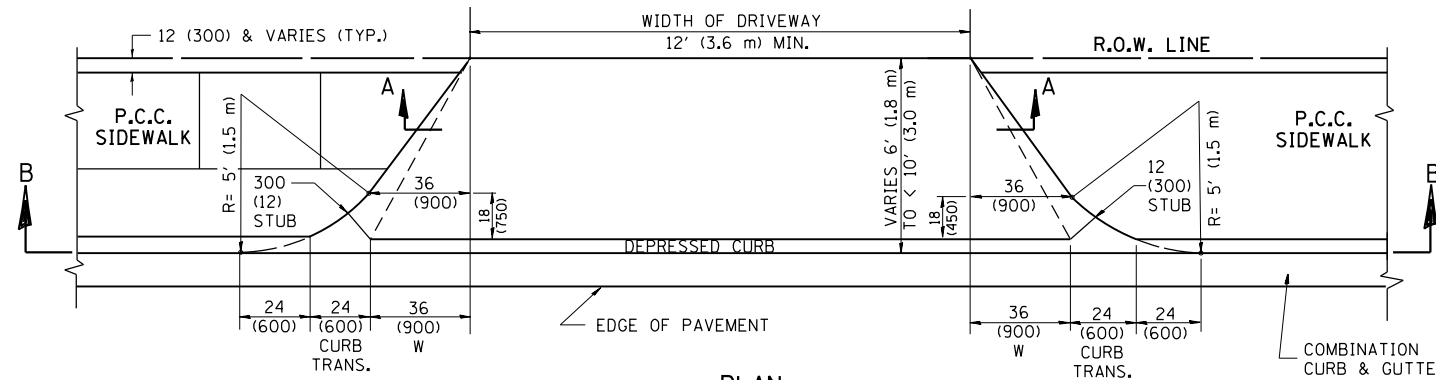
DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.  
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

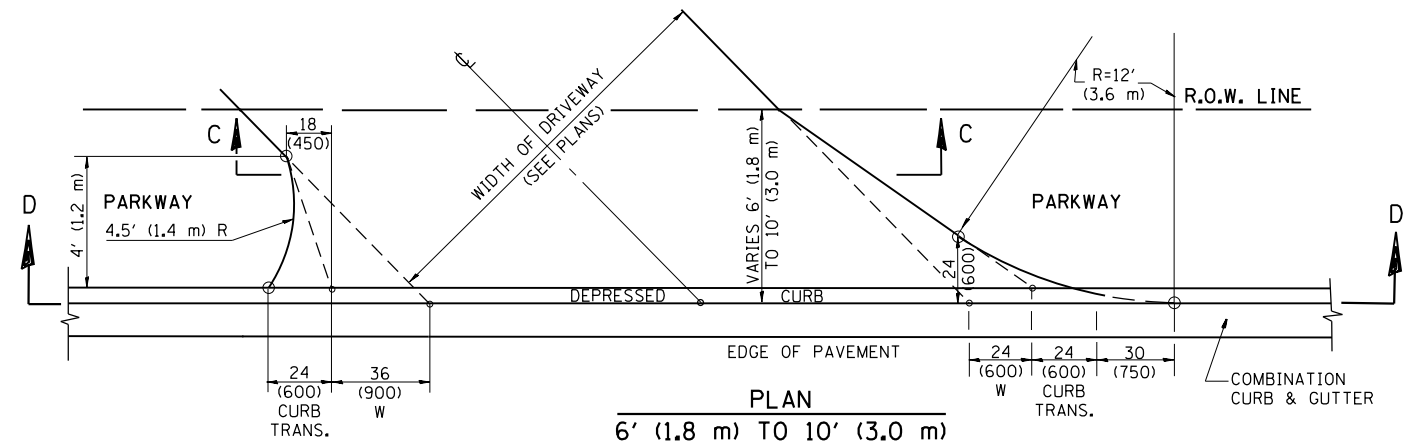
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BD0156-07 (BD-01)		CONTRACT NO. 62A73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



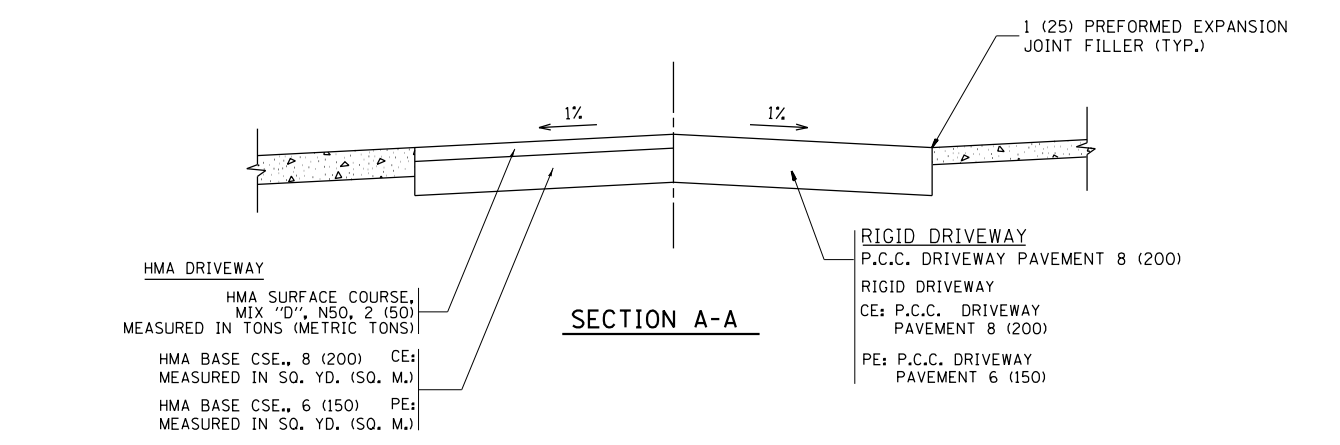
PLAN  
10' (3.0 m) TO < 15' (4.5 m)



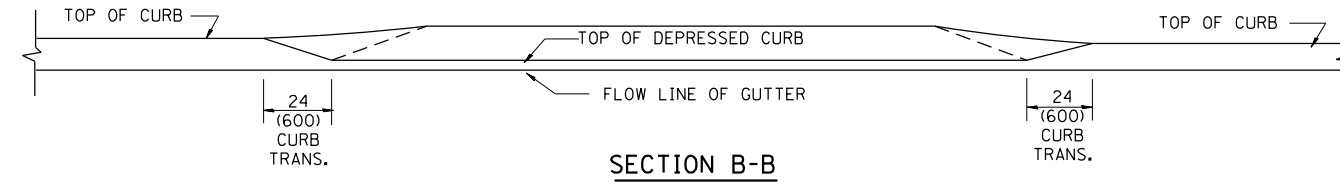
PLAN  
6' (1.8 m) TO < 10' (3.0 m)



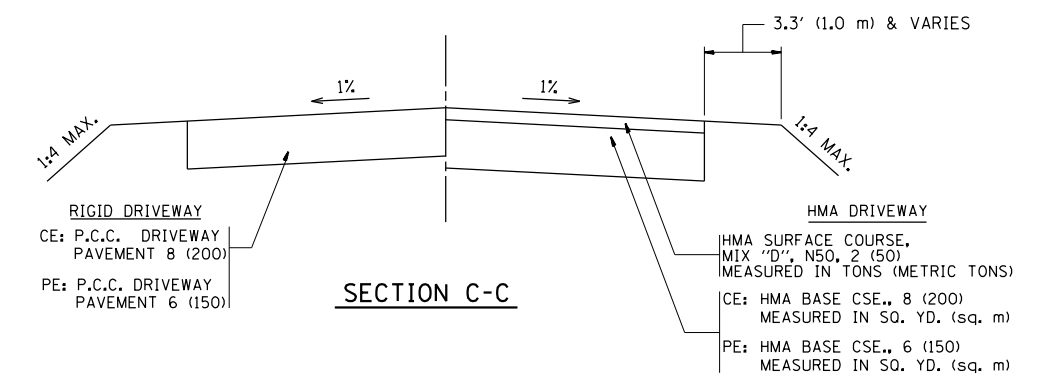
PLAN  
6' (1.8 m) TO 10' (3.0 m)



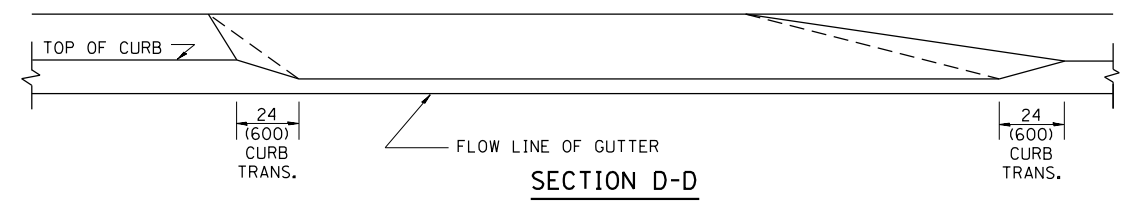
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

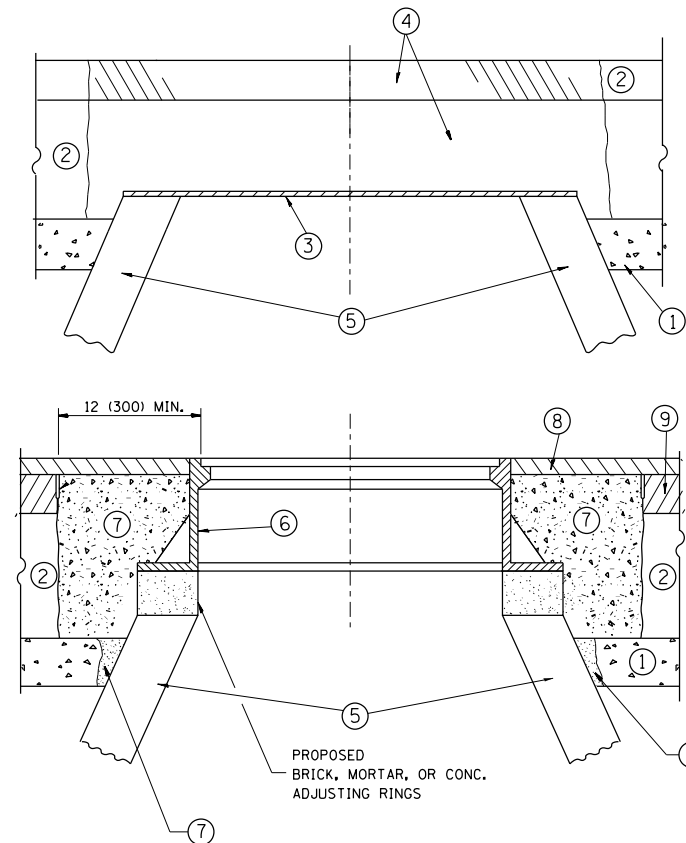
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PLOT SCALE = 100.0002' / 1"		DATE - 11-06-95	REVISED - R. BORO 01-01-07
PLOT DATE = 10/26/2016			REVISED - R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	16
BD400-02 (BD-02)		CONTRACT NO. 62A73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

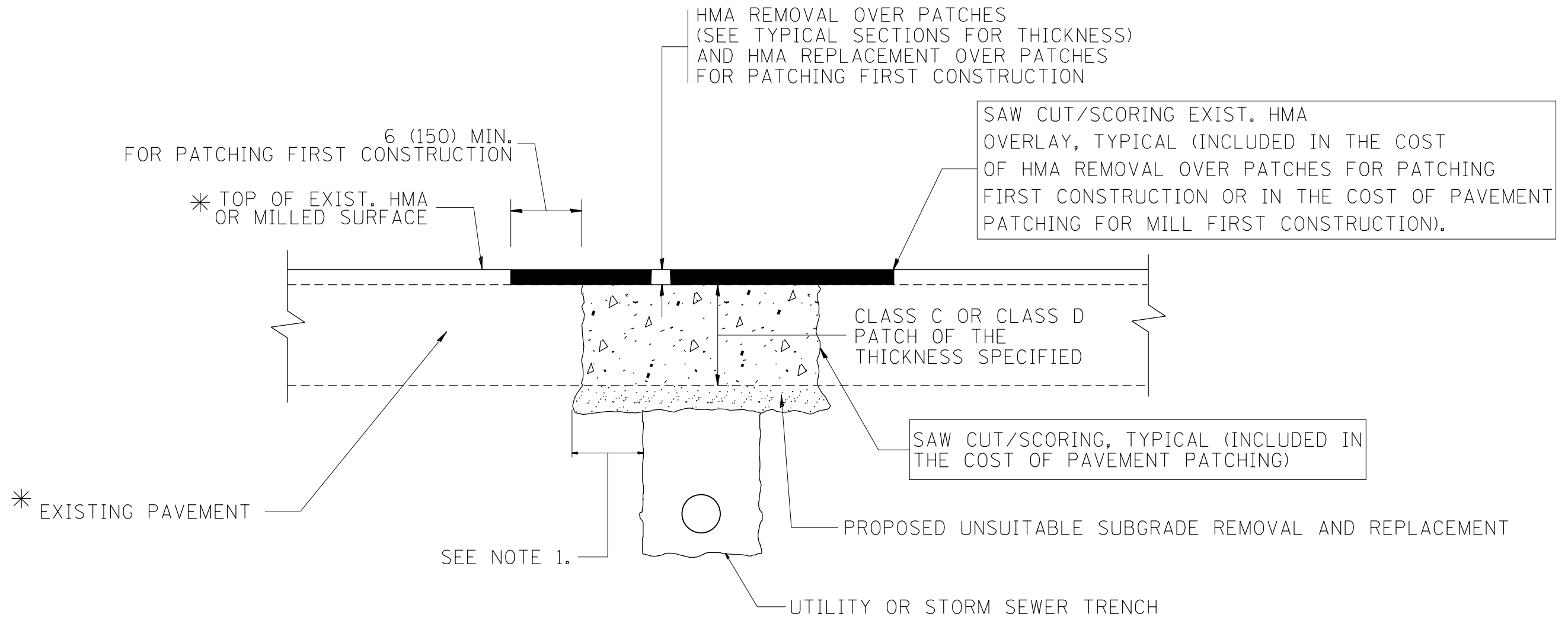
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = STEEDPA	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
			REVISED - R. BORO 01-01-07
		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	17
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62A73</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn	PLOT SCALE = 100.0002' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					2692	3539 RS-1	COOK	28	18
	PLOT DATE = 10/26/2016	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62A73				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

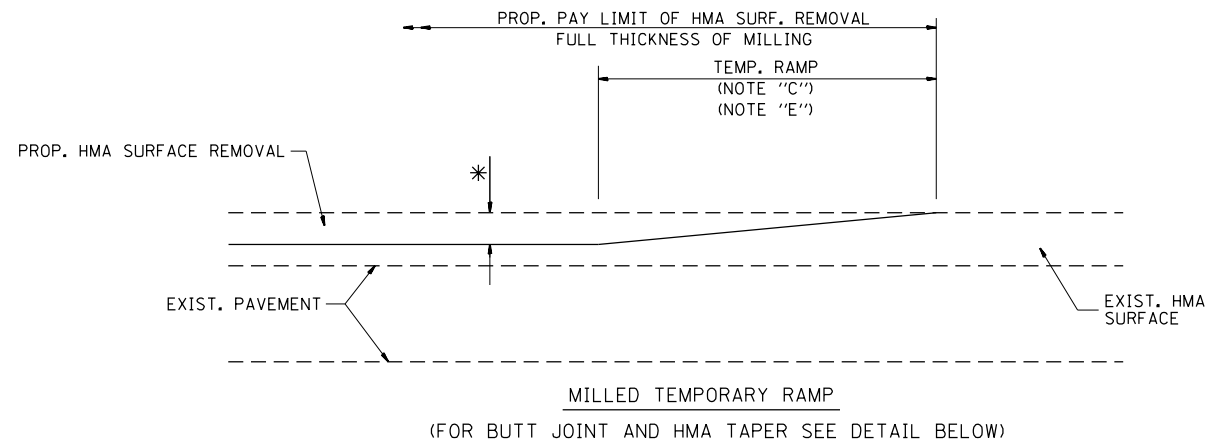
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

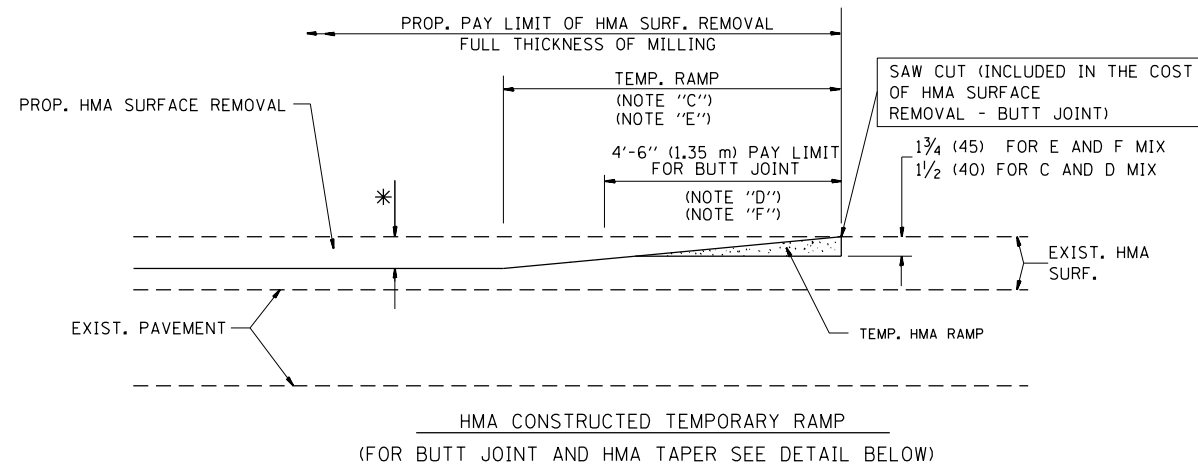
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			2692	3539 RS-1	COOK	28	19
PLOT SCALE = 100.0002' / 1".	CHECKED -	REVISED - R. BORO 12-15-09				<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62A73</b>		
PLOT DATE = 10/26/2016	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT

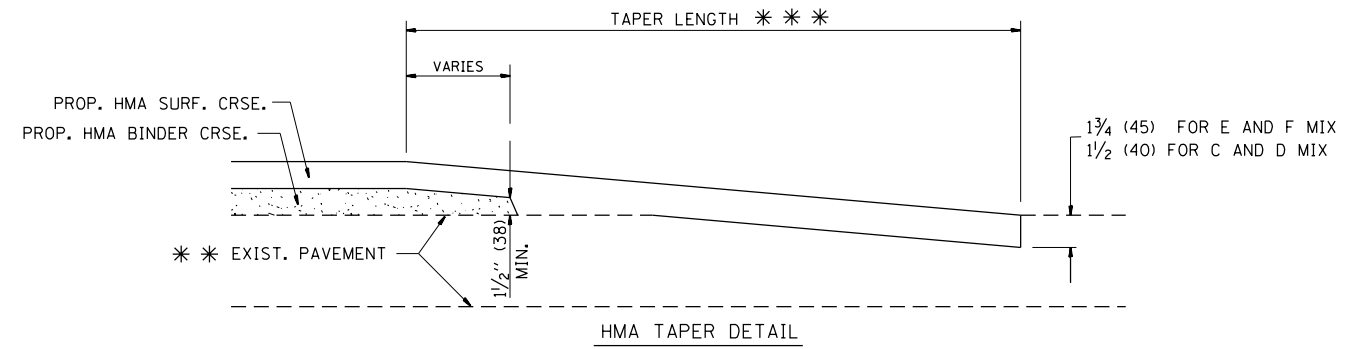
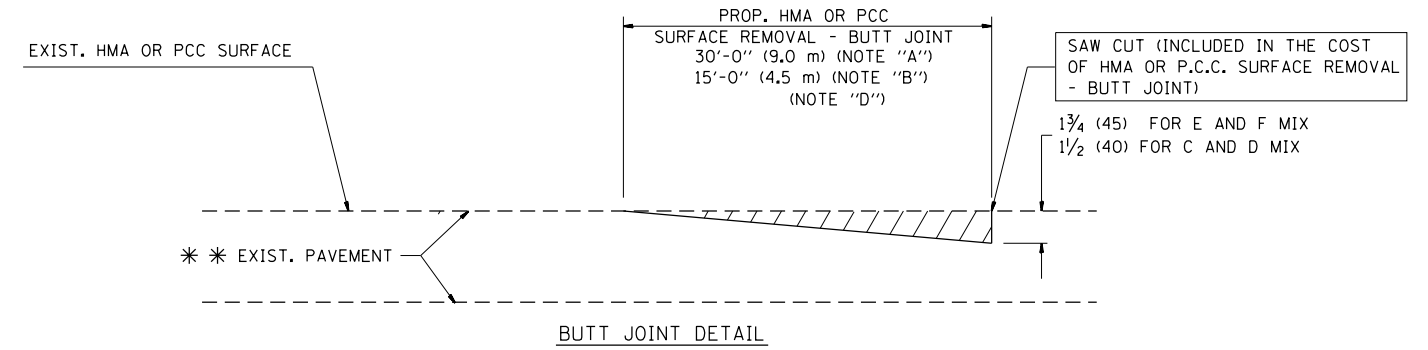


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

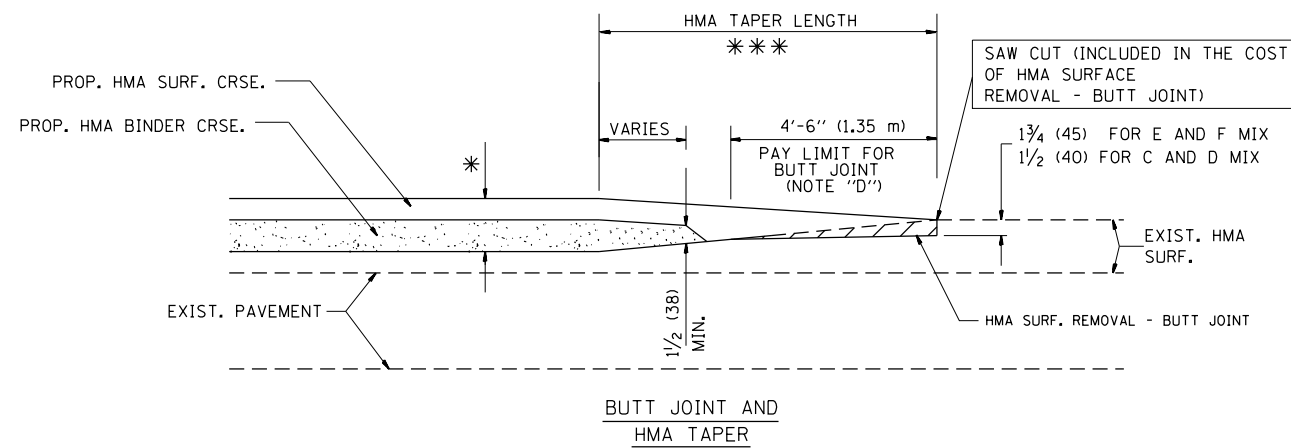
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



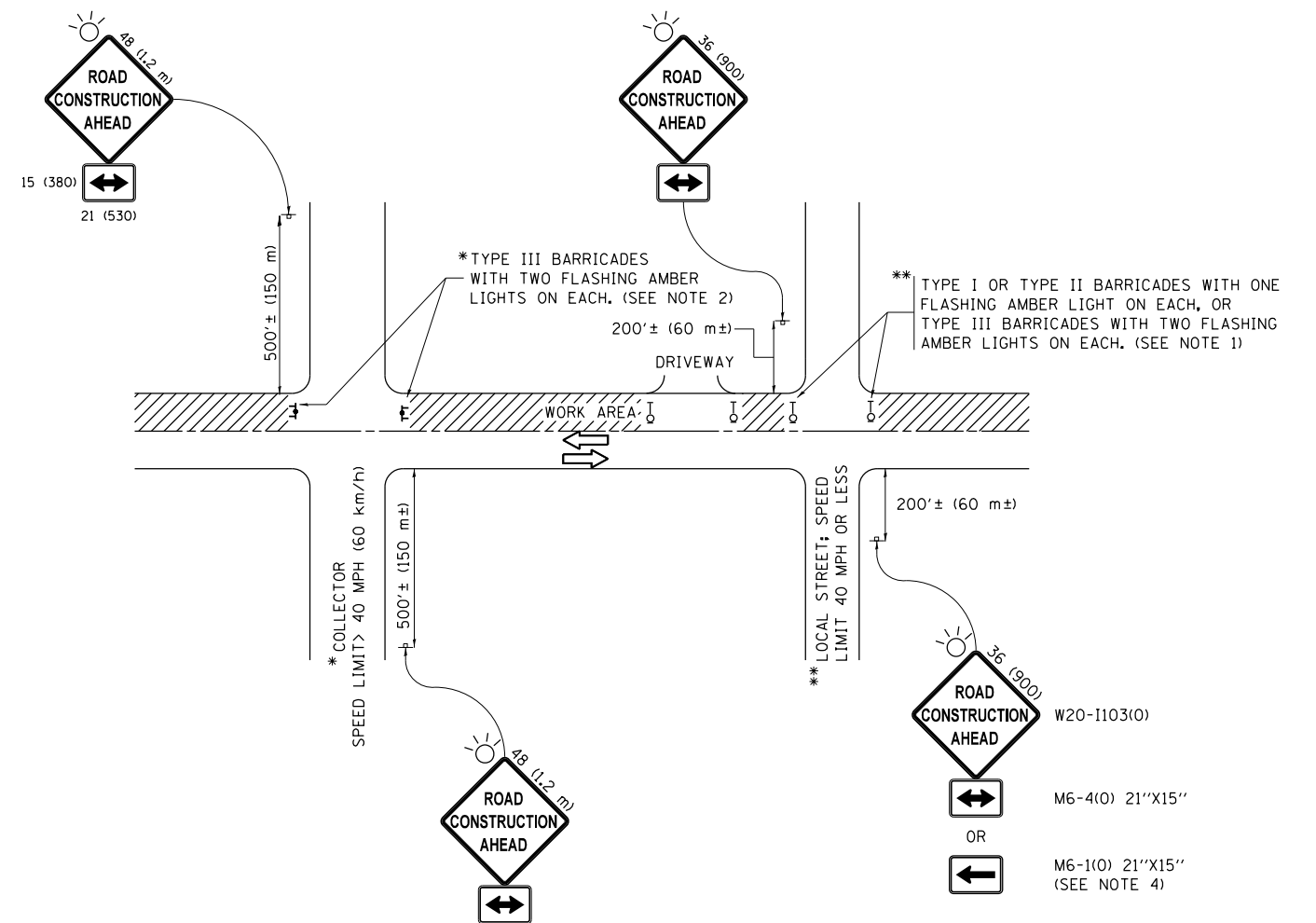
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = STEEDPA	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0002' / 1"	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/26/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	20
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 62A73</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

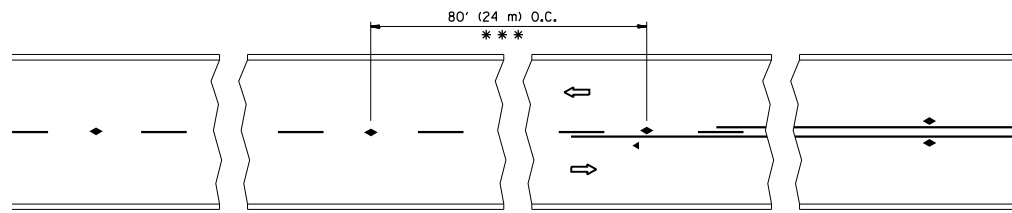
FILE NAME =	USER NAME = STEEDPA	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 10/26/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

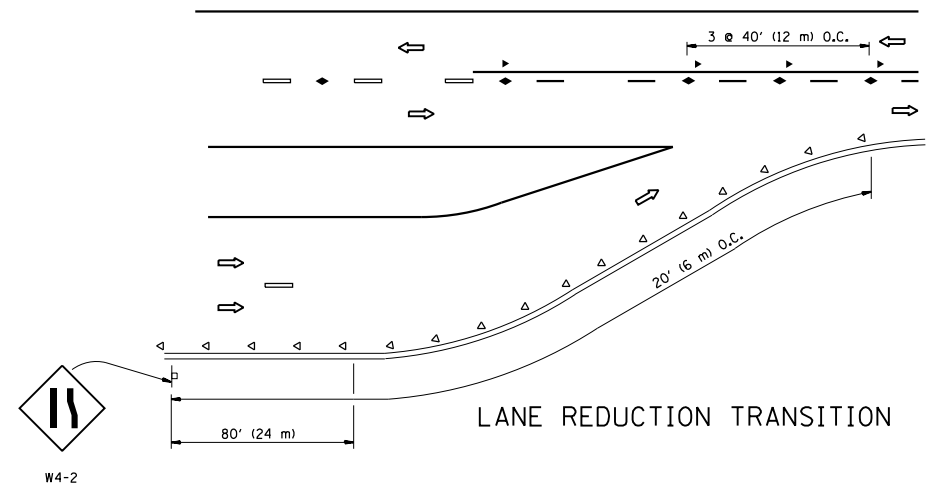
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	21
<b>TC-10</b>			<b>CONTRACT NO. 62A73</b>	
ILLINOIS FED. AID PROJECT				

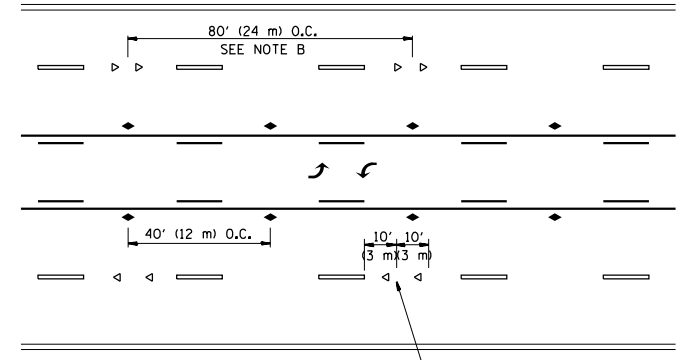


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

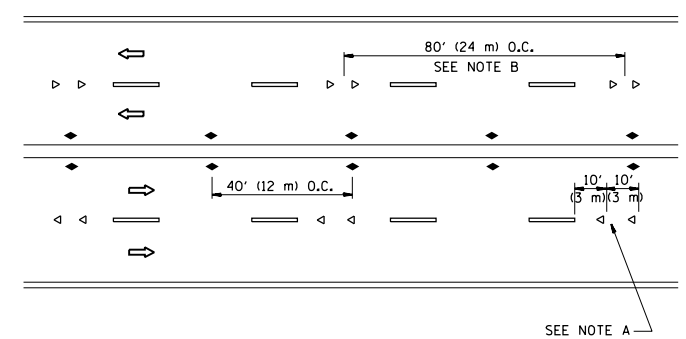
TWO-LANE/TWO-WAY



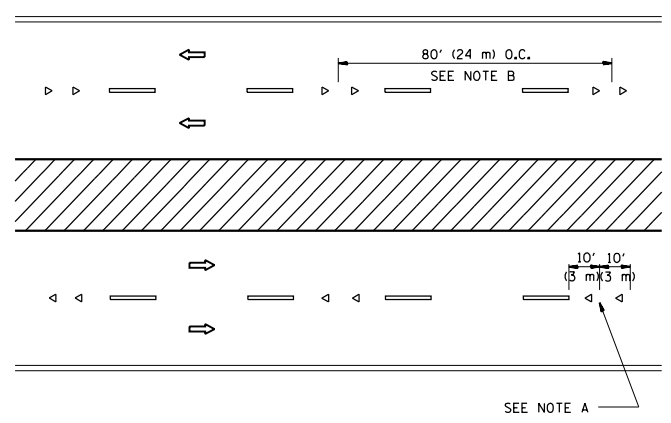
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

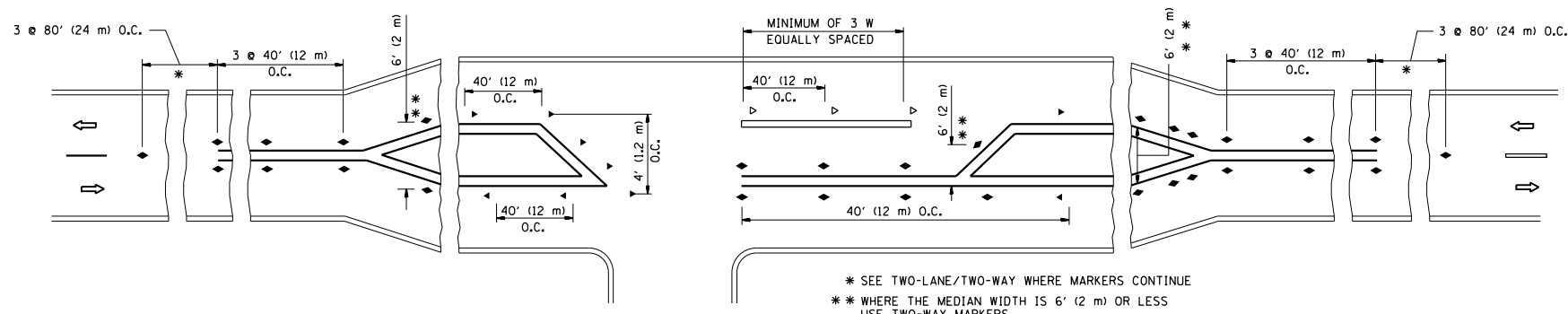
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

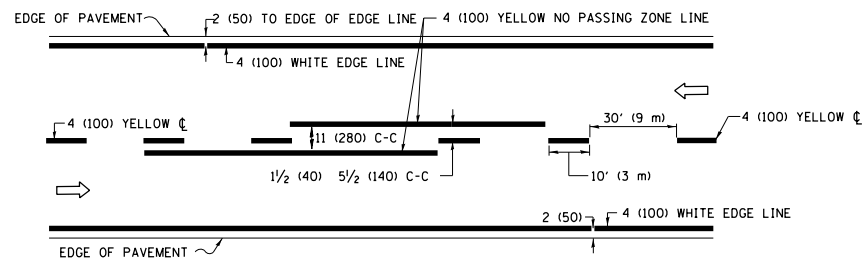


LEFT TURN

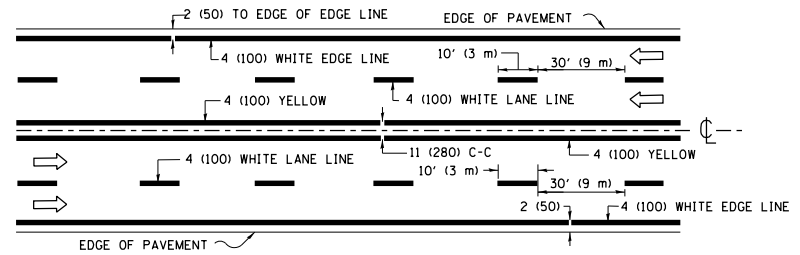
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

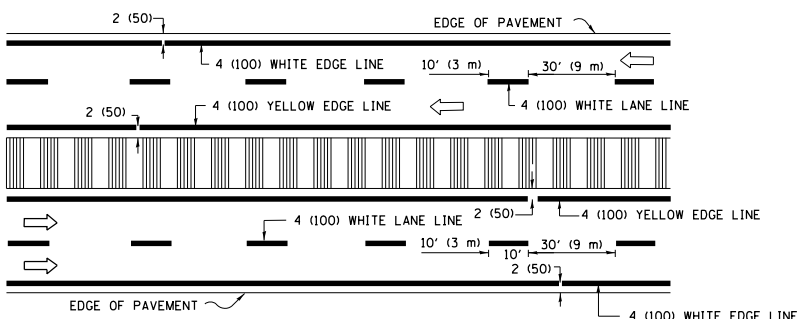
FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					2692	3539 RS-1	COOK	28	22	
PLOT DATE = 10/26/2016	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09		<b>TC-11</b>			<b>CONTRACT NO. 62A73</b>					
					SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



**2-LANE ROADWAY**

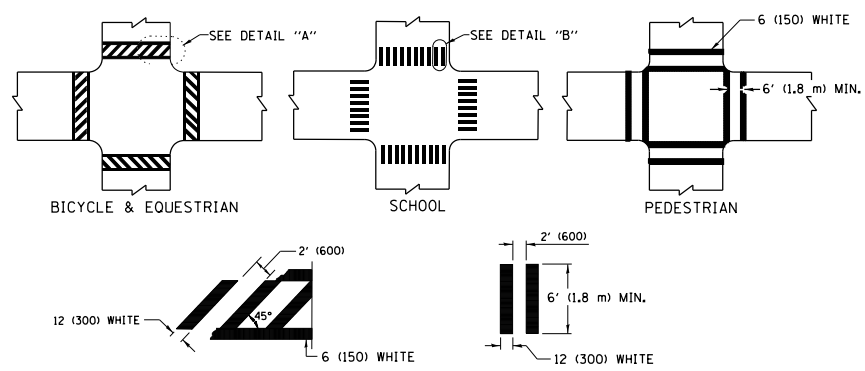


**MULTI-LANE UNDIVIDED**



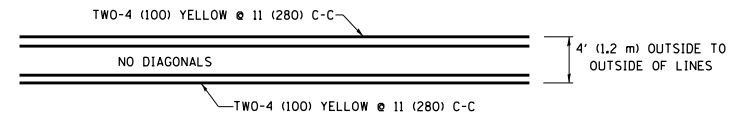
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

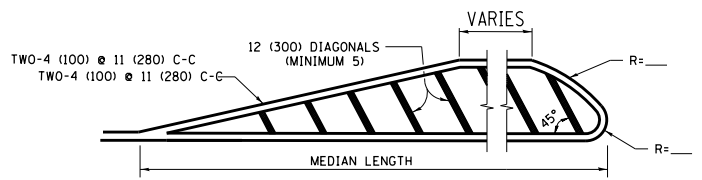


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

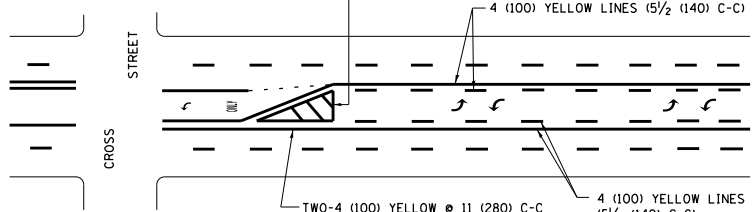


**4' (1.2 m) WIDE MEDIANS ONLY**



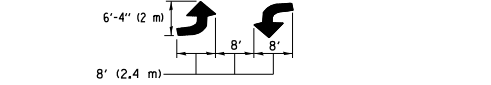
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

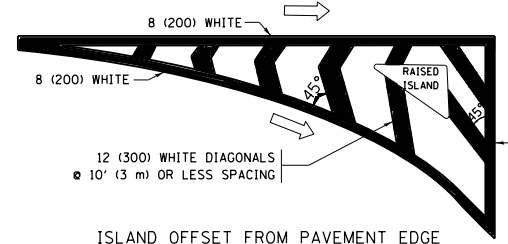
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



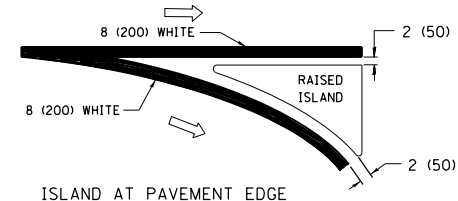
**TYPICAL LEFT (OR RIGHT) TURN LANE**

FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

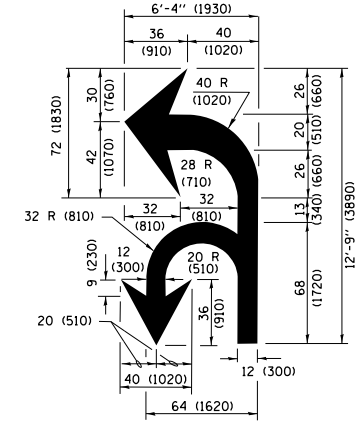
**TYPICAL TURN LANE MARKING**



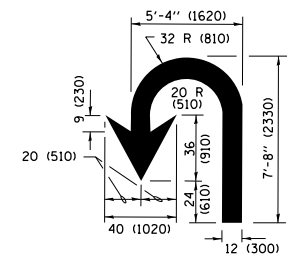
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**  
\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWDD\Documents\DOT Offices\District 1\Projects\01303\Drawings\Design\Diststd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 10/26/2016	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

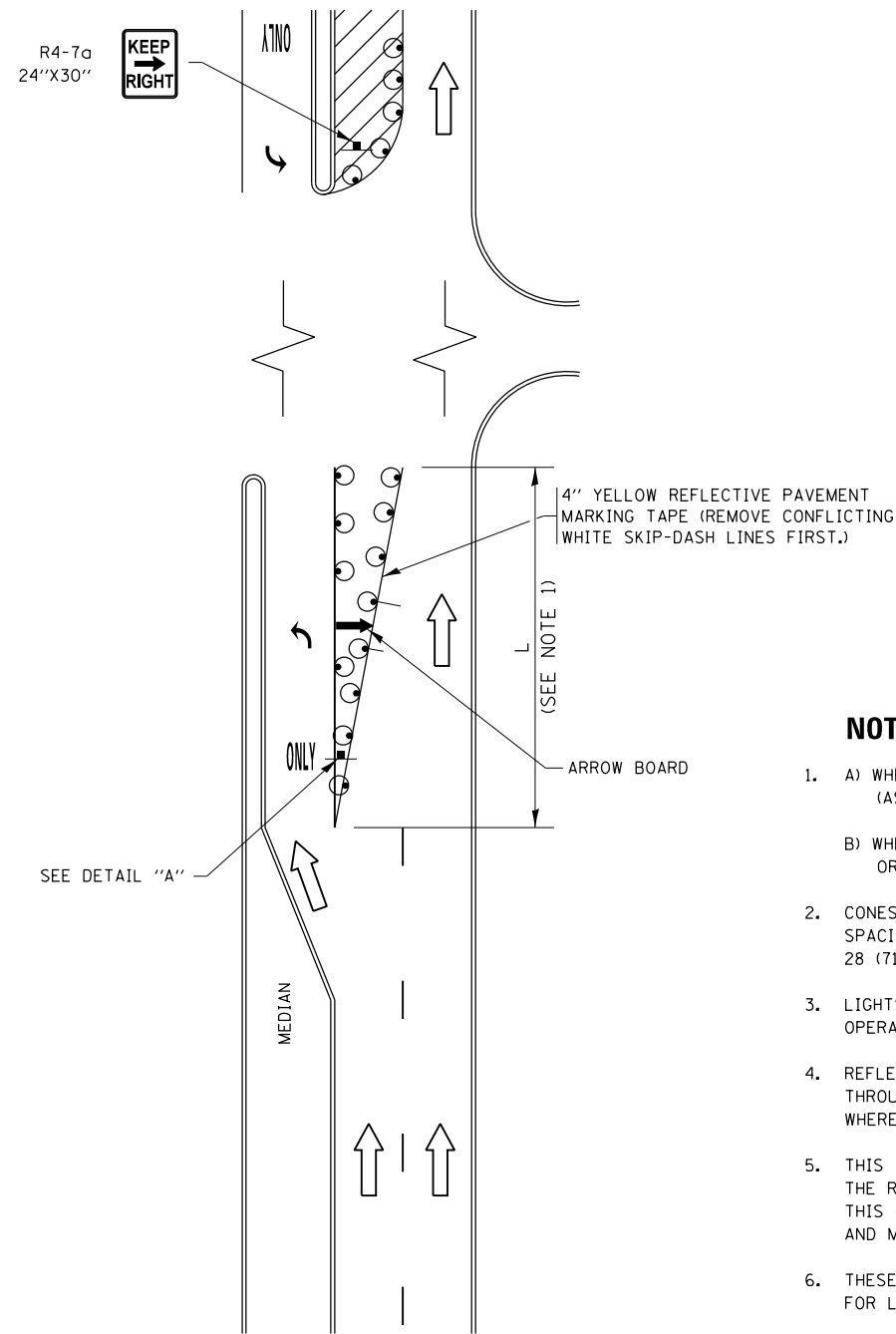
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.
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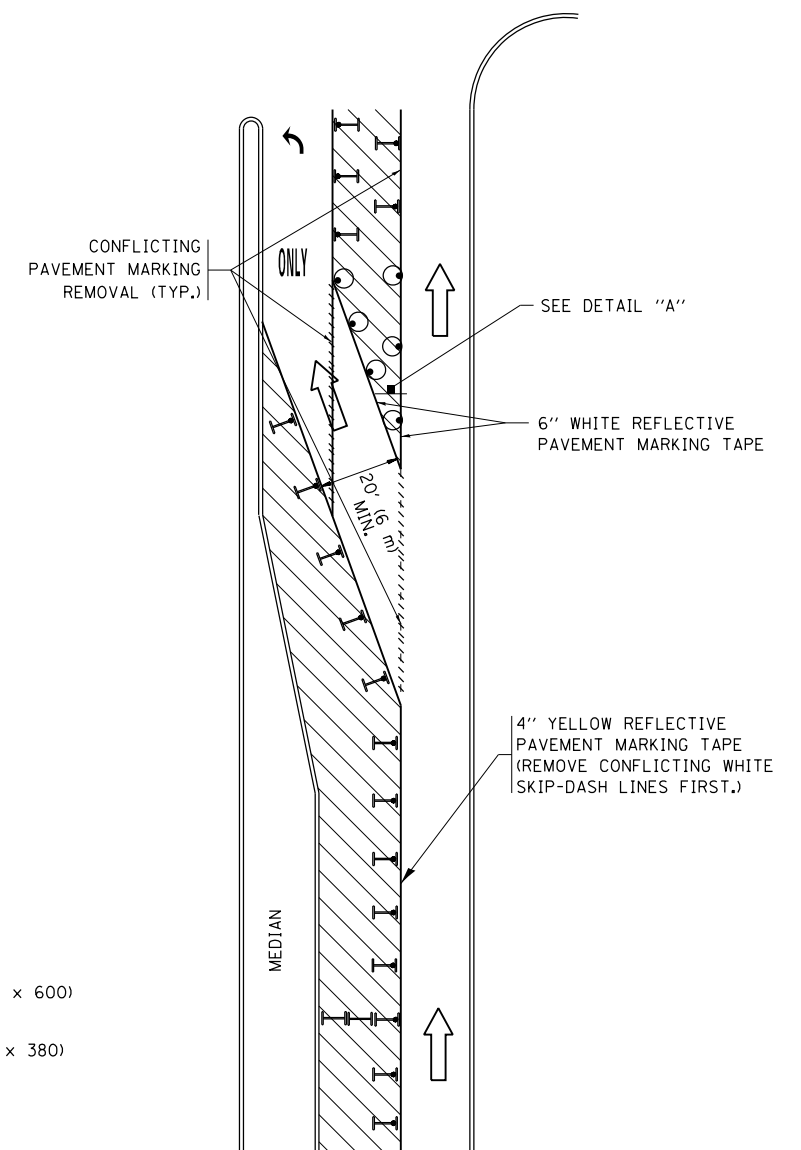
F.A.U R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	23
<b>TC-13</b>		<b>CONTRACT NO. 62A73</b>		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



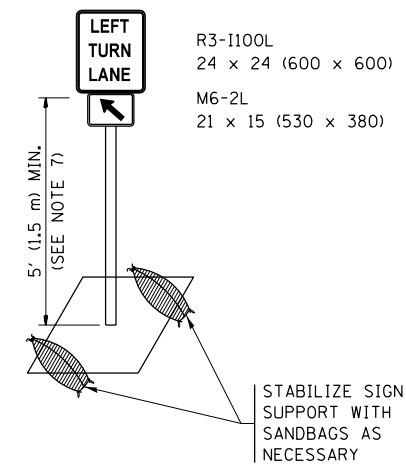
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

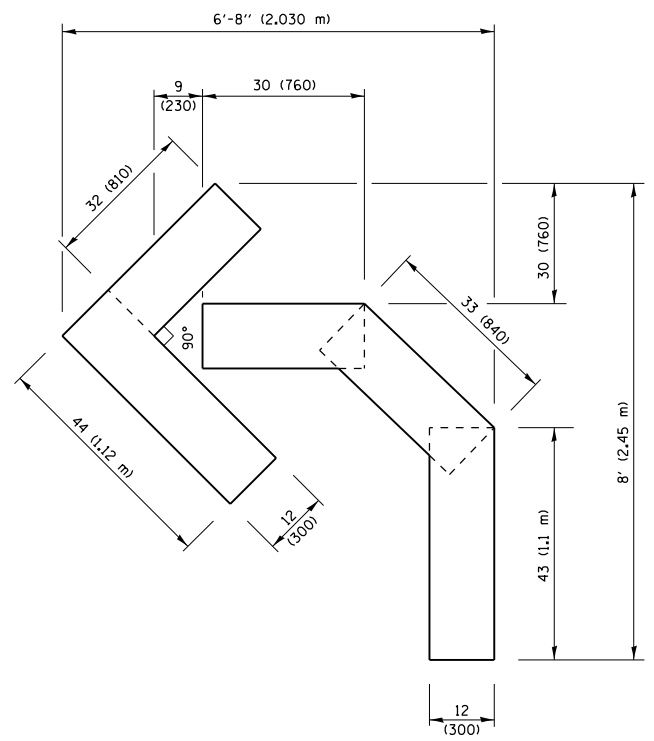


**DETAIL A**

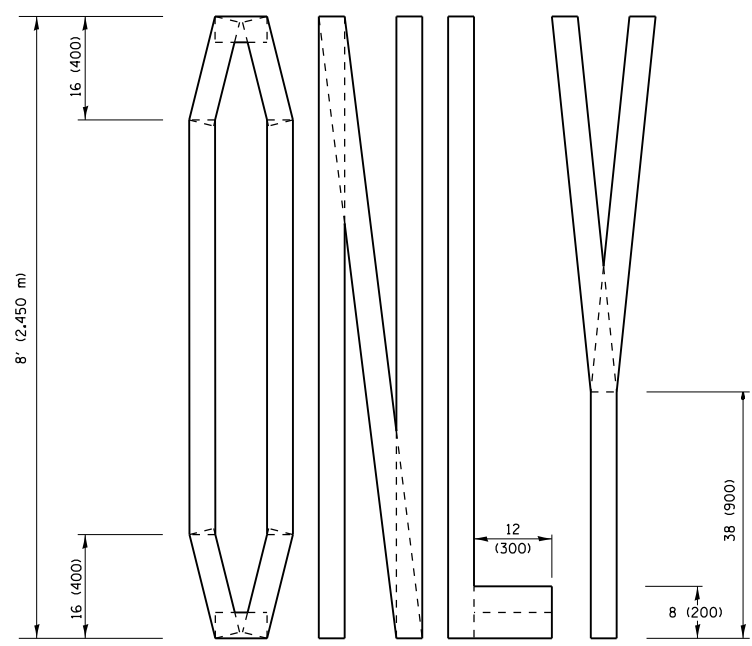
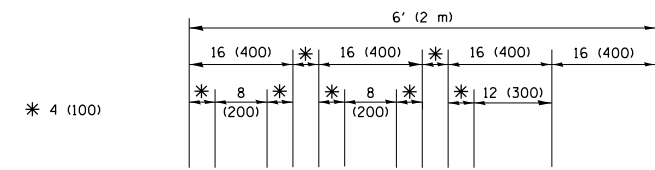
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13			2692	3539 RS-1	COOK	28	24	
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			<b>TC-14</b>		<b>CONTRACT NO. 62A73</b>			
	PLOT DATE = 10/26/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	

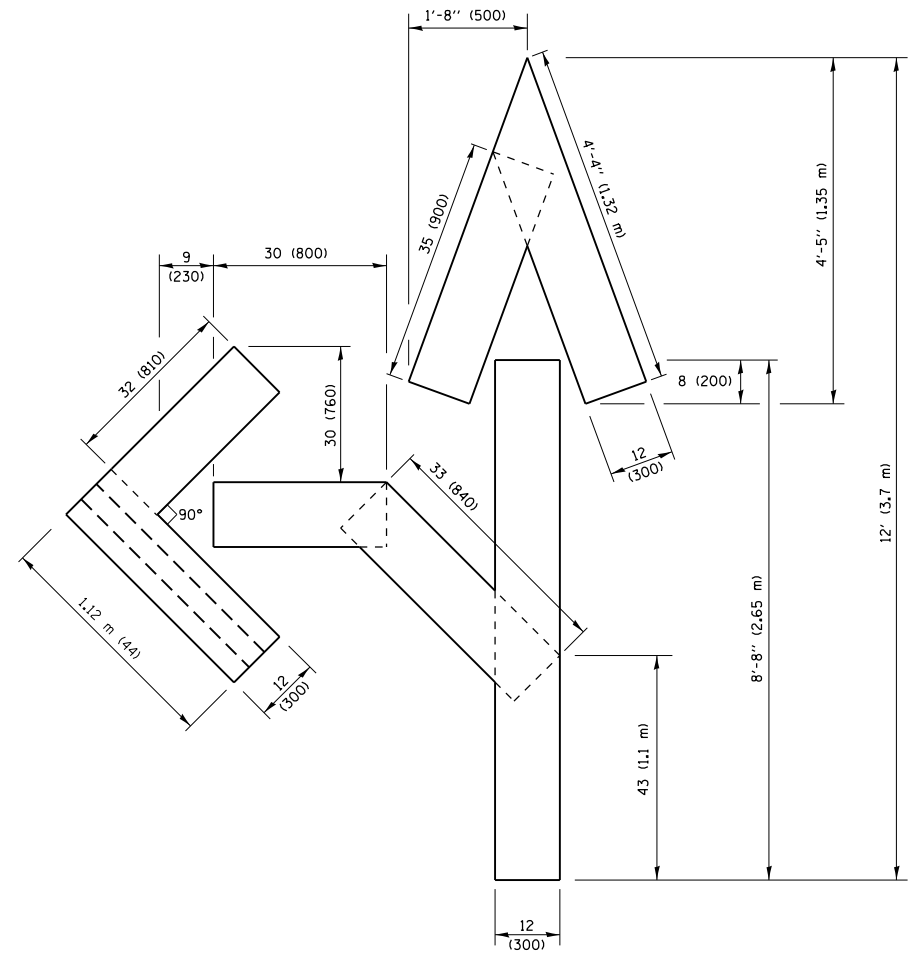




**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

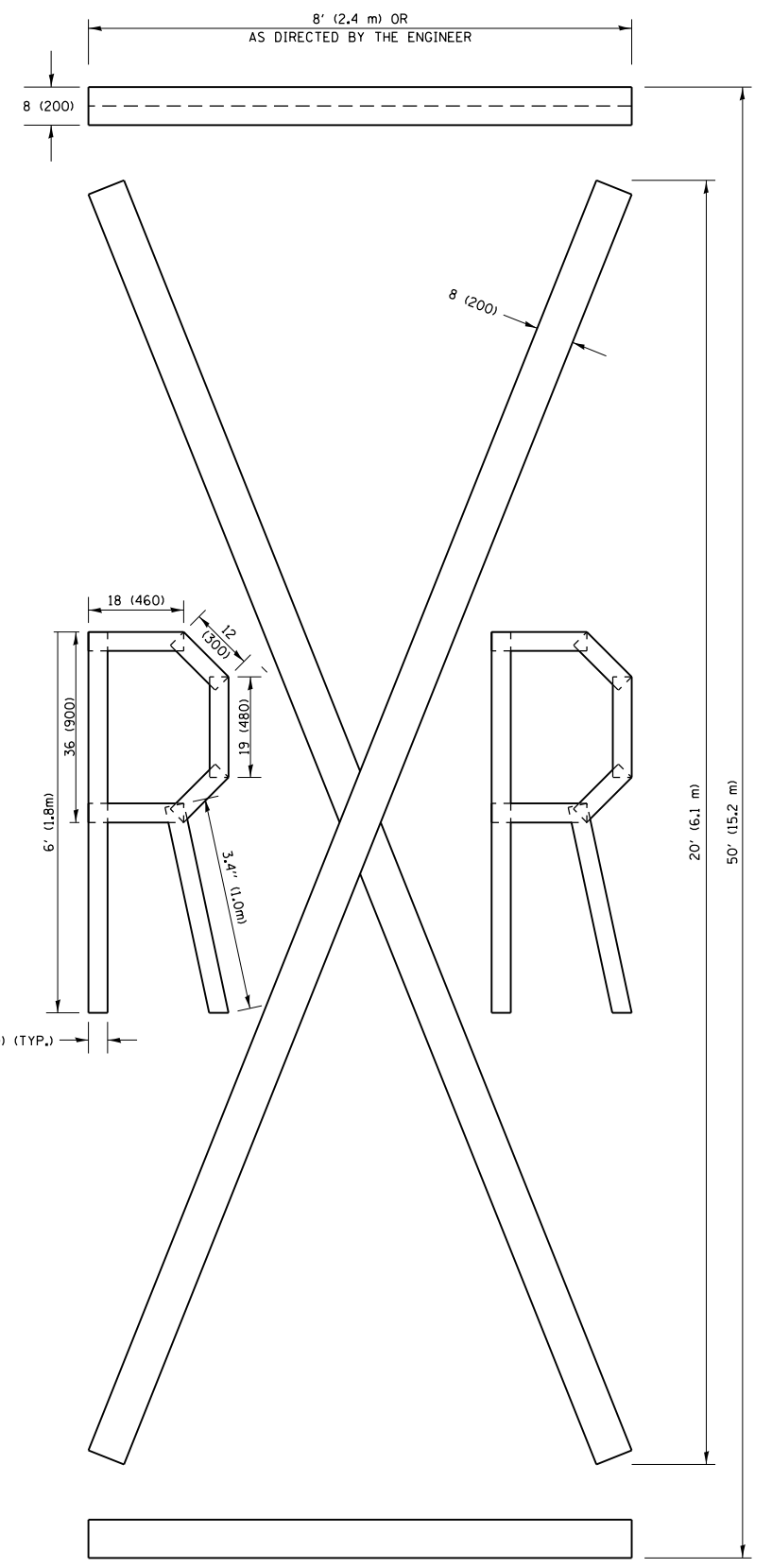


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01303\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

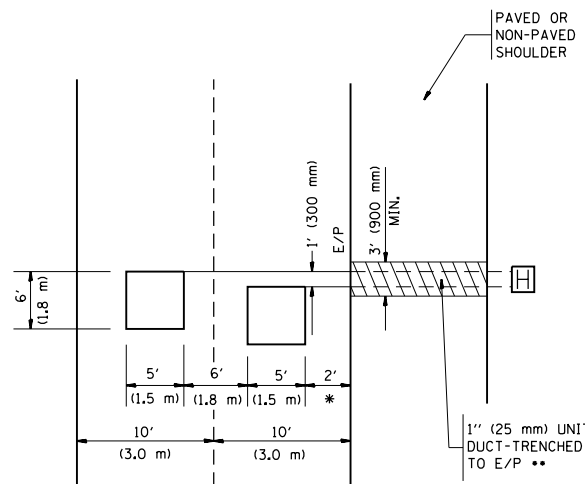
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	25
<b>TC-16</b>		<b>CONTRACT NO. 62A73</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



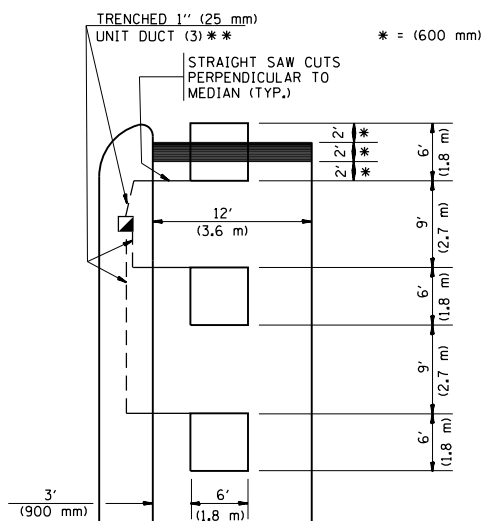
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

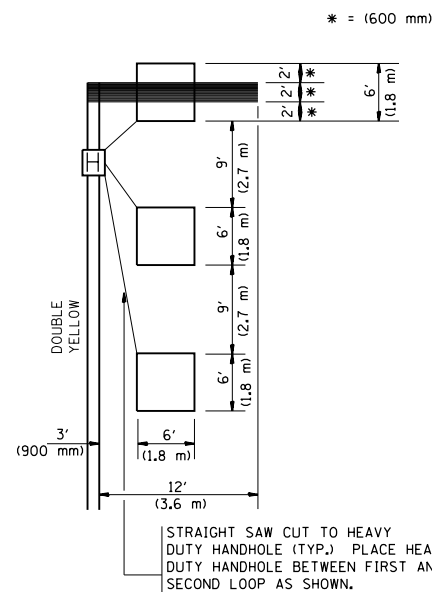


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

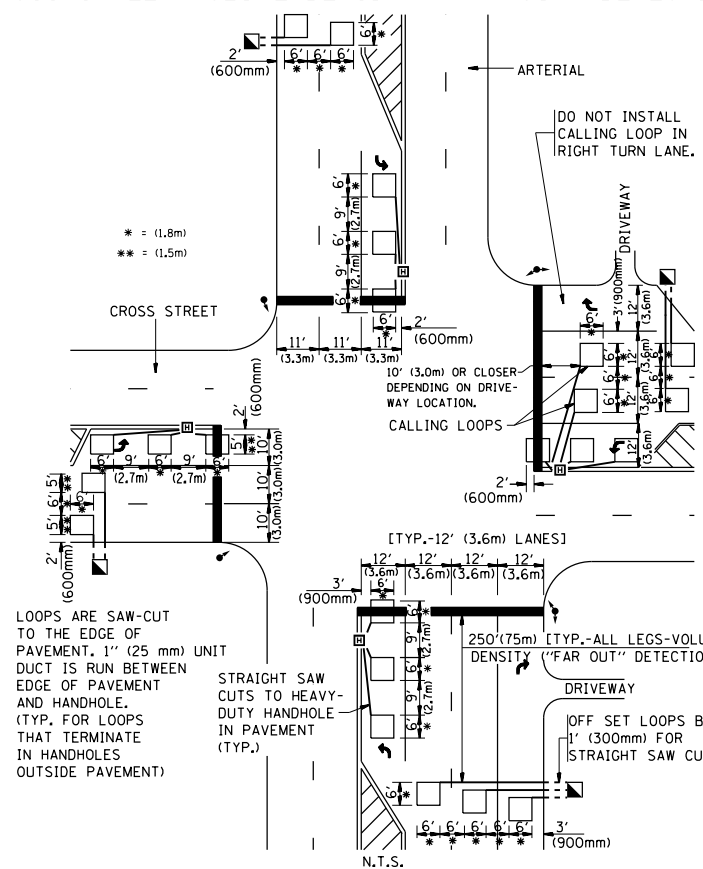
LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



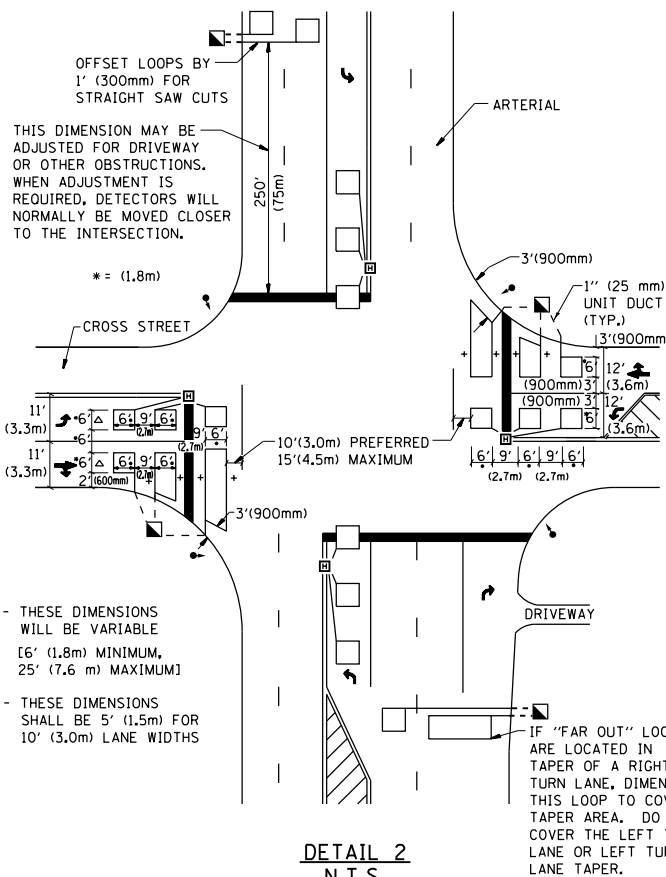
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

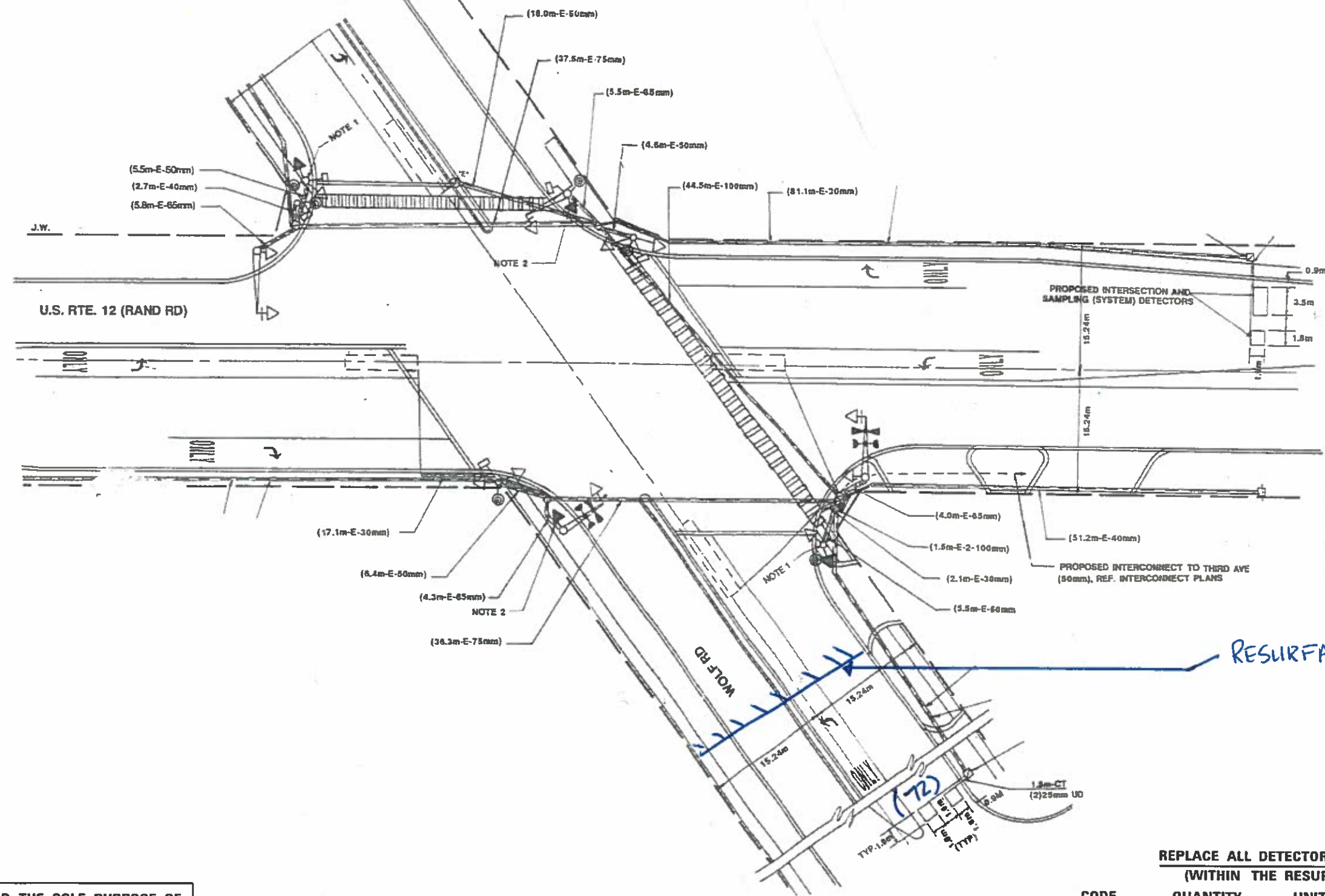
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>	F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn		DRAWN -	REVISED -			2692	3539 RS-1	COOK	28	26
PLOT SCALE = 100.0000' / 1"		CHECKED - R.K.F.	REVISED -			<b>TS-07</b>		<b>CONTRACT NO. 62A73</b>		
PLOT DATE = 10/26/2016		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	

WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "TRAFFIC SIGNAL SIGNAL SPECIFICATIONS FOR DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION ON ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS". SPECIAL ATTENTION MUST BE MADE TO THE SECTIONS "INSPECTION OF CONSTRUCTION" AND "DETECTOR LOOP REPLACEMENT" FOR INSTALLATION AND INSPECTION REQUIREMENTS. LOOP REPLACEMENT WORK THAT DOES NOT MEET THE CONTRACT REQUIREMENTS SHALL NOT BE PAID. WORK NECESSARY TO COMPLETE THE LOOP REPLACEMENT WORK MAY BE ASSIGNED BY THE ENGINEER TO IDOT'S ELECTRICAL MAINTENANCE CONTRACTOR (EMC); ALL RELATED COSTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

**TRAFFIC SIGNAL LEGEND**

	PROPOSED	EXISTING
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD		
GALVANIZED STEEL CONDUIT IN TRENCH OR PUSHED		
DETECTOR LOOP		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		

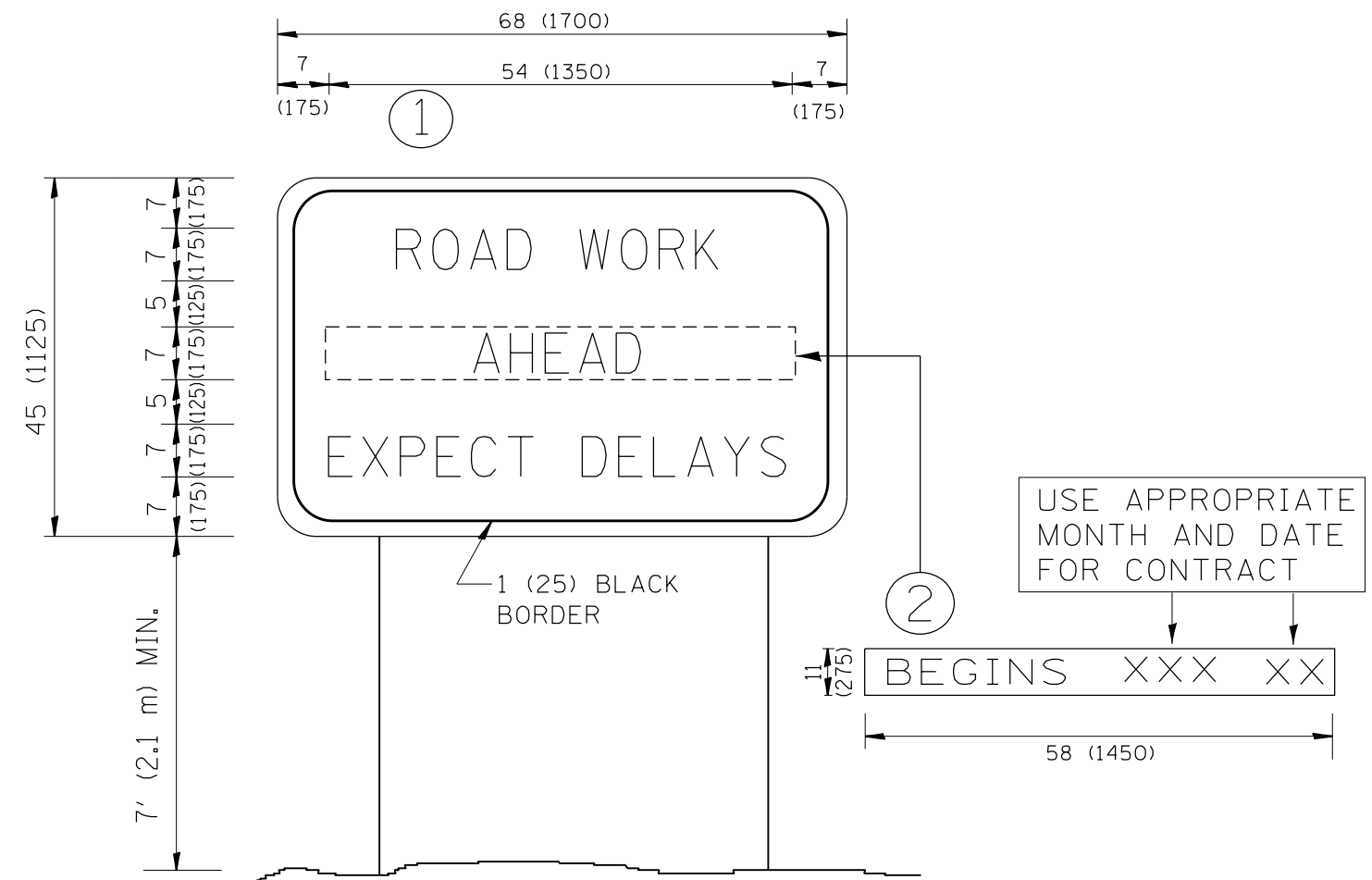


**THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY**

**REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)**

CODE	QUANTITY	UNIT	ITEM
88600600	72	FOOT	DETECTOR LOOP REPLACEMENT

FILE NAME = P:\Traffic\Flasher.dgn	USER NAME = pociornel	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE - DETECTOR LOOP REPLACEMENT WOLF RD AT US 12 (RAND RD)</b>	F.A. RTE. 2692	SECTION 3539 RS-1	COUNTY COOK	TOTAL SHEETS 28	SHEET NO. 27	
PLOT SCALE = 1/8"=1'-0"	CHECKED -	REVISED -	SCALE: NTS			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS/FED. AID PROJECT	CONTRACT NO. 62A73	
PLOT DATE = 1/5/2012	DATE -	REVISED -									



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI303\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2692	3539 RS-1	COOK	28	28
TC-22			CONTRACT NO. 62A73	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				