

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-1RS-3	COOK	36	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 62D22		

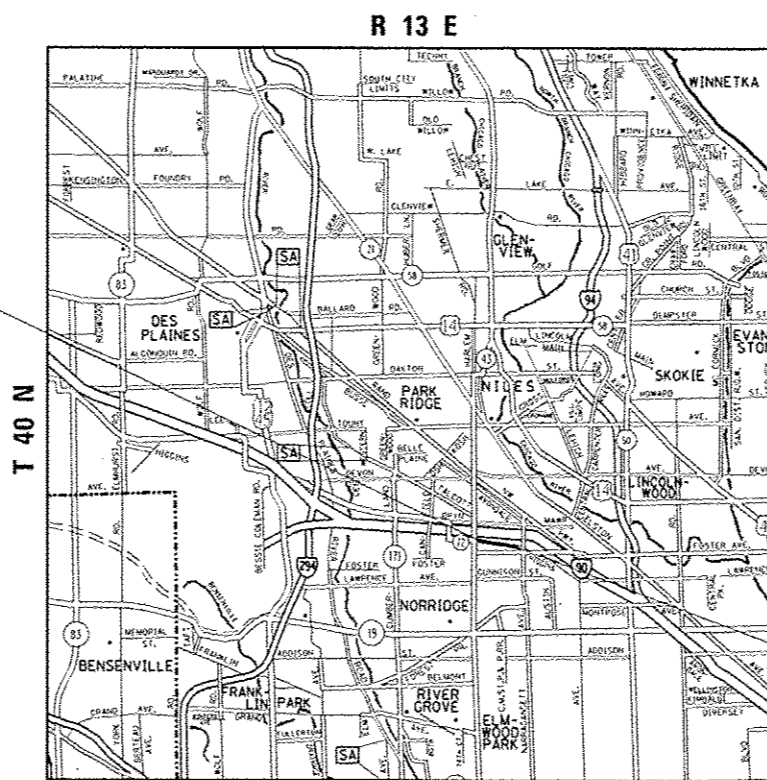
D -91-445-16

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT IS LOCATED
IN THE CITY OF CHICAGO

PROPOSED HIGHWAY PLANS

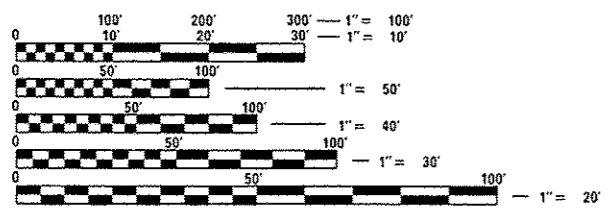
FAU ROUTE 3531: HIGGINS AVENUE
IL 43 (HARLEM AVENUE) TO NAGLE AVENUE
SECTION: 63Z-1RS-3
RESURFACING (3P)/PEDESTRIAN RAMPS
COOK COUNTY
C-91-445-16
ACM-3531(001)



IMPROVEMENT BEGINS
STA. 17+35

TRAFFIC DATA
2014 ADT = 14,100
POSTED SPEED LIMIT = 30 MPH

IMPROVEMENT ENDS
STA. 70+30



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
312-744-7000

PROJECT ENGINEER: JEAN ALAIN MIDY (847) 221-3056
PROJECT MANAGER: ISSAM RAYYAN (847) 705-4178

GROSS AND NET LENGTH OF IMPROVEMENT = 5295 LINEAL FEET = 1.003 MILE

CONTRACT NO. 62D22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 20 2016
John Fortman
REGIONAL ENGINEER

Dec 9 2016
Maureen M. Addis
ENGINEER OF DESIGN AND ENVIRONMENT

Dec 9 2016
David J. ...
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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- 34-36. CITY OF CHICAGO TYPICAL PAVEMENT MARKING (TC-24)

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-03	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
606001-06	COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 2' FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS \geq 40 MPH
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES

CHICAGO NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.

PERMITS FROM THE DEPARTMENT OF SEWERS ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY OR COMBINED SEWER SYSTEM CONSTRUCTION, THE DEPARTMENT OF SEWERS, PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION.

PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS.

OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

GENERAL NOTES

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, TRAFFIC FIELD ENGINEER, AT CORY.JUCIUS@ILLINOIS.GOV. A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH. WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.

ALL PAVEMENT PATCHING LOCATIONS WILL DETERMINED IN THE FIELD BY THE ENGINEER.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO CITY OF CHICAGO TYPICAL PAVEMENT MARKING.

SIDEWALK RAMPS MODIFICATIONS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO THE APPLICABLE HIGHWAY STANDARDS INCLUDED IN THE PLANS.

THE COST OF SURFACE REMOVAL IN THE GUTTER FLAG SHALL BE INCLUDED IN THE COST OF OF "HOT-MIX ASPHALT SURFACE REMOVAL 1 1/2" PAY ITEM.

ALL COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

REV.

FILE NAME *	USER NAME = guillaumefp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES HIGGINS AVENUE - (IL 43 (HARLEM AVE.) TO NAGLE AVE.)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
per\\IL084E81DINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI445\BRM\Notes\Design\DI44516-shr-plan.dgn		CHECKED -	REVISED -			3531	632-1RS-3	COOK	36	2	
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	PLOT DATE = 10/20/2016										

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0005						CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0005					
20200100	EARTH EXCAVATION	CU YD	130	130						42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	10259	10259					
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	300	300						42400800	DETECTABLE WARNINGS	SO FT	692	692					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	4	4						44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SO YD	12070	12070					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	4	4						44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	16350	16350					
25200110	SODDING, SALT TOLERANT	SO YD	300	300						44000600	SIDEWALK REMOVAL	SO FT	10259	10259					
25200200	SUPPLEMENTAL WATERING	UNIT	1	1						44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	260	260					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	17501	17501						44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	140	140					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	43	43						60265700	VALVE VAULTS TO BE ADJUSTED	EACH	1	1					
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	865	865						60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	520	520						60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5					
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	630	630						60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	25	25					
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	2055	2055					* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	120	120						
42001300	PROTECTIVE COAT	SO YD	1595	1595					* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1						
									* 66900530	SOIL DISPOSAL ANALYSIS	EACH	3	3						
									67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	9	9						

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
HIGGINS AVENUE - (IL 43 (HARLEM AVE.) TO NAGLE AVE.)**

D.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-1RS-3	COOK	36	3
SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.
				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

* SPECIALTY ITEM

SUMMARY OF QUANTITIES				URBAN 80% FED 20% STATE				CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0005							
67100100	MOBILIZATION	LSUM	1	1							
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1							
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1							
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1							
70300100	SHORT TERM PAVEMENT MARKING	FOOT	7200	7200							
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	2400	2400							
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	39	39							
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	2200	2200							
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2700	2700							
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	100	100							
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1120	1120							
72400500	RELOCATE SIGN PANEL ASSEMBLY, TYPE A	EACH	9	9							
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	39	39							
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2200	2200							

SUMMARY OF QUANTITIES				URBAN 80% FED 20% STATE				CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	ROADWAY 0005							
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2700	2700							
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100	100							
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1120	1120							
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	80	80							
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	50	50							
* 85000200	^{Existing} MAINTENANCE OF TRAFFIC SIGNAL INSTALLATION	EACH	1	1							
* 89502376	REBUILD EXISTING HANDHOLE	EACH	1	1							
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1							
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	2400	2400							
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	100	100							
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	70	70							
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1805	1805							
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	10	10							
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4							

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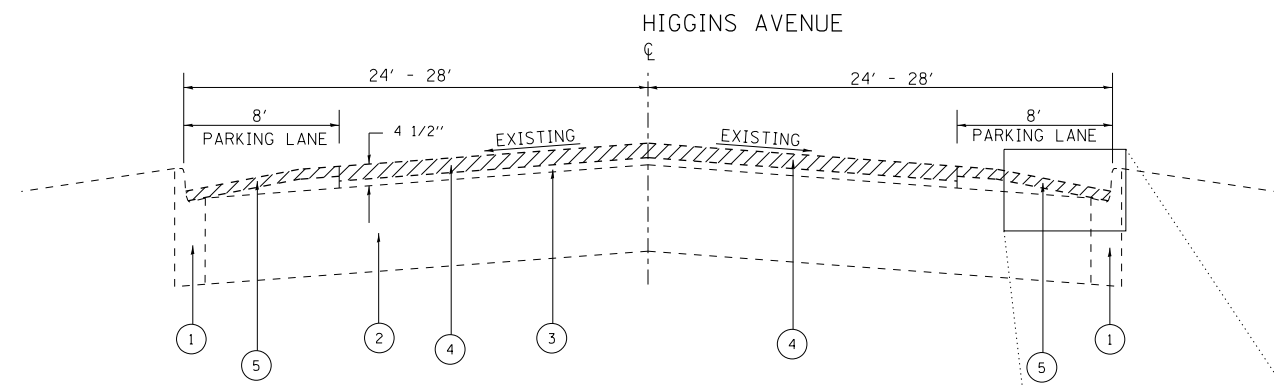
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
HIGGINS AVENUE - (IL 43 (HARLEM AVE.) TO NAGLE AVE.)**

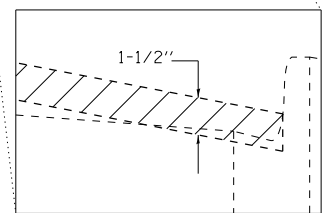
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O.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-1 RS-3	COOK	36	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 6292	

* SPECIALTY ITEM □ NON-PART. (100% STATE)



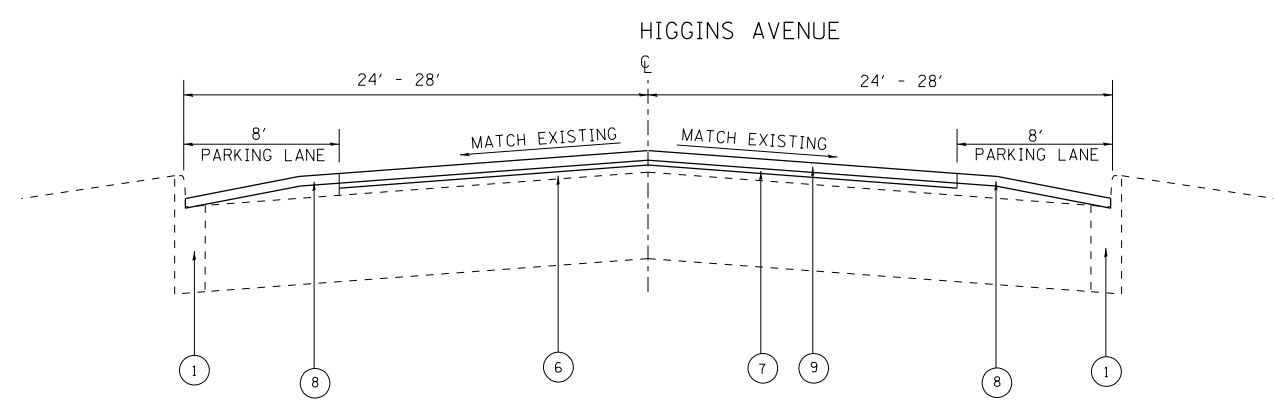
EXISTING TYPICAL CROSS SECTION
LOCATION STA. 17+35 TO STA. 70+30



NOTE: MILLING OF ANY CONCRETE BASE COURSE OR CONCRETE GUTTER FLAG IS INCIDENTAL TO HMA SURFACE REMOVAL 1-1/2"

LEGEND

- ① EXISTING COMBINATION CURB AND GUTTER
- ② EXISTING PORTLAND CEMENT CONCRETE PAVEMENT (10-3/4" AND VARIES)
- ③ EXISTING HMA PAVEMENT (4-1/2" AND VARIES)
- ④ PROPOSED HMA SURFACE REMOVAL (2-1/2")
- ⑤ PROPOSED HMA SURFACE REMOVAL (1-1/2")
- ⑥ HMA OVERLAY AFTER MILLING ± 2"
- ⑦ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 (3/4")
- ⑧ PROPOSED HMA SURFACE COURSE, MIX "D", N70 (1-1/2")
- ⑨ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70 (1-3/4")

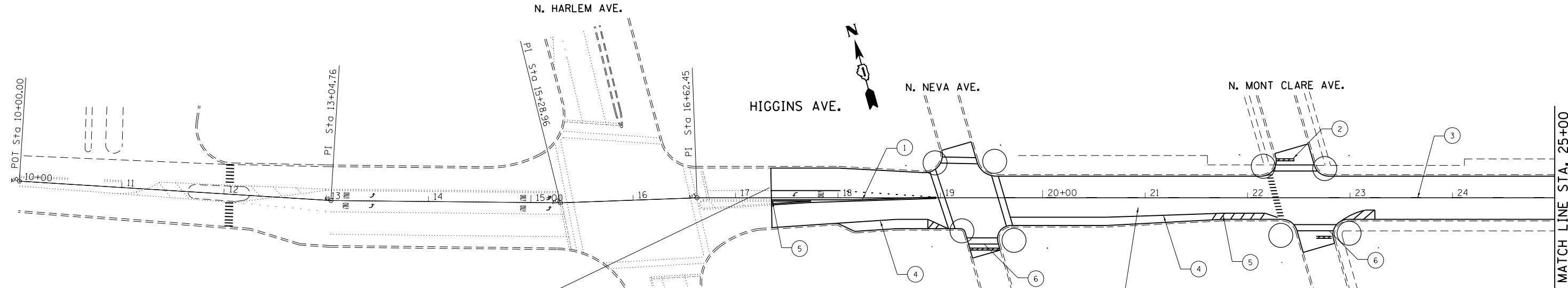


PROPOSED TYPICAL CROSS SECTION
LOCATION STA. 17+35 TO STA. 70+30

MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE USES	VOIDS © Ndes	
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (IL 9.5 mm)	4% AT 70 GYR.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
PARKING RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% AT 70 GYR.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER, IL-19.0 mm)	4% AT 70 GYR.	QC/QA
QMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP)		

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SOYD/IN
 NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
 QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

THE CONTRACTOR SHALL MILL BEFORE PATCHING

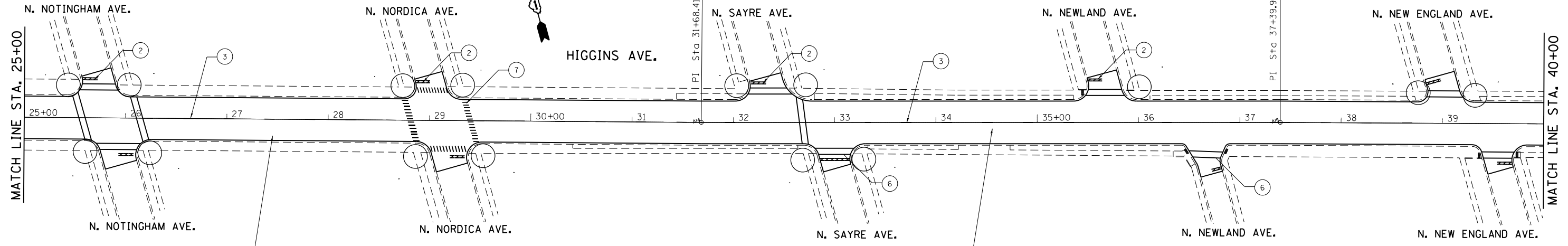


NOTE:
 1. ALL PAVEMENT MARKING SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED. SEE CITY OF CHICAGO DETAIL.

STRIPING LEGEND

- ① 4" DOUBLE YELLOW - 8" C-C
- ② 24" STOP BAR - WHITE
- ③ 4" SKIP DASH CENTERLINE - YELLOW - 6' LINE, 18' SPACE
- ④ 4" LINE - WHITE
- ⑤ 12" DIAGONAL LINES - YELLOW - 20' C-C SPACING
- ⑥ 6" CROSSWALK - WHITE
- ⑦ 24"- LONGITUDINAL BAR (SCHOOL & PEDESTRIAN) - WHITE

(MAIN LINE RESURFACING)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"



(PARKING RESURFACING)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

(MAIN LINE RESURFACING)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

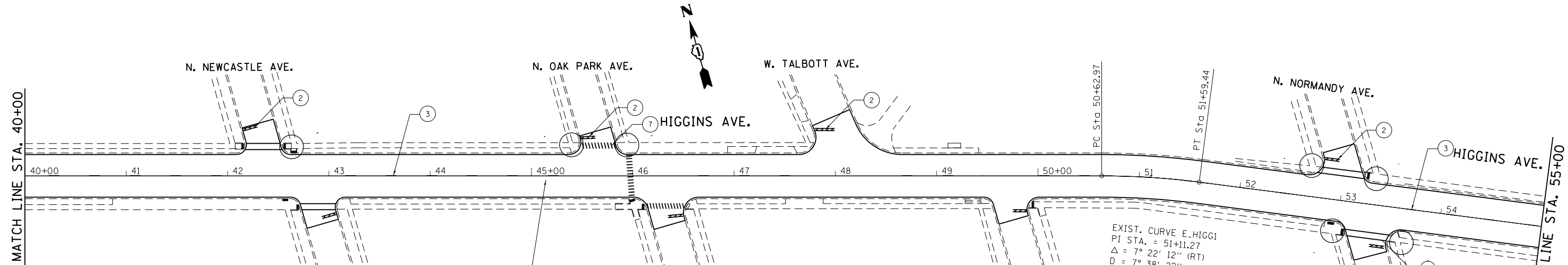
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	PLOT DATE = 10/28/2016	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
 HIGGINS AVE. (IL. RTE. 43 (HARLEM AVE.) TO NAGLE AVE.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-IRS-3	COOK	36	6
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



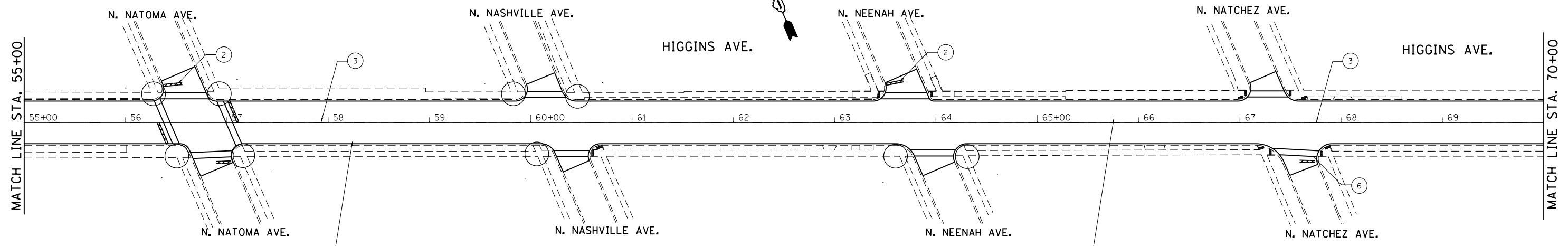
NOTE:
 1. ALL PAVEMENT MARKING SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED. SEE CITY OF CHICAGO DETAIL.

(MAIN LINE RESURFACING)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

EXIST. CURVE E_HIGG1
 PI STA. = 51+11.27
 $\Delta = 7^\circ 22' 12''$ (RT)
 $D = 7^\circ 38' 22''$
 $R = 750.00'$
 $T = 48.30'$
 $L = 96.47'$
 $E = 1.55'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 50+62.97$
 $P.T. STA. = 51+59.44$

STRIPING LEGEND

- ① 4" DOUBLE YELLOW - 8" C-C
- ② 24" STOP BAR - WHITE
- ③ 4" SKIP DASH CENTERLINE - YELLOW - 6' LINE, 18' SPACE
- ④ 4" LINE - WHITE
- ⑤ 12" DIAGONAL LINES - YELLOW - 20' C-C SPACING
- ⑥ 6" CROSSWALK - WHITE
- ⑦ 24"- LONGITUDINAL BAR (SCHOOL & PEDESTRIAN) - WHITE



(PARKING RESURFACING)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

(MAIN LINE RESURFACING)
 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

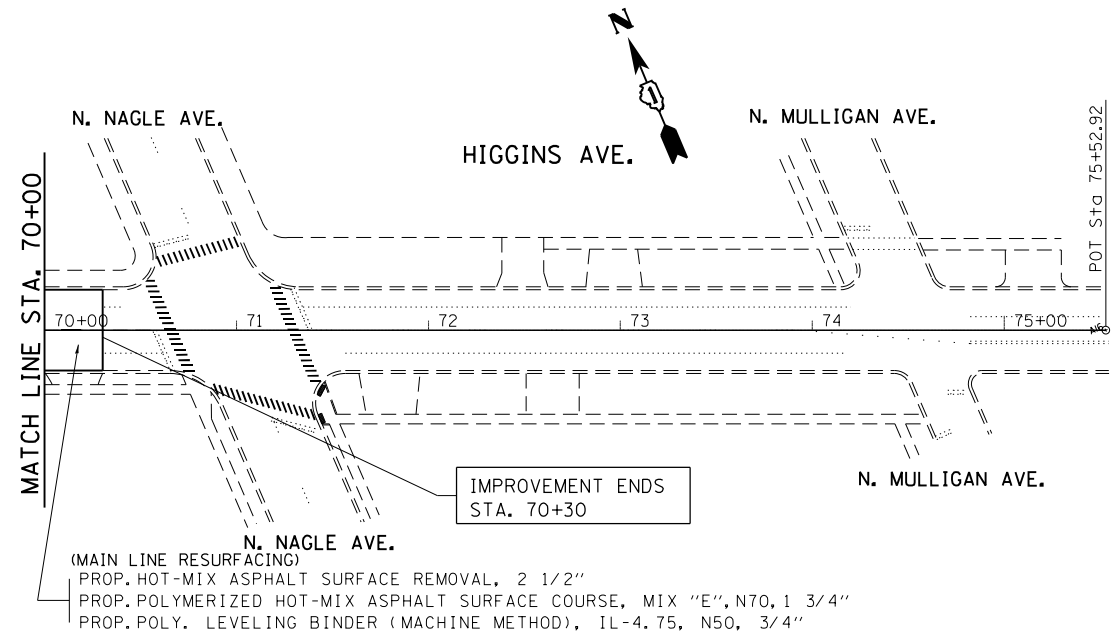
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Default	PLOT DATE = 10/28/2016	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
 HIGGINS AVE. (IL. RTE. 43 (HARLEM AVE.) TO NAGLE AVE.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-IRS-3	COOK	36	7
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				

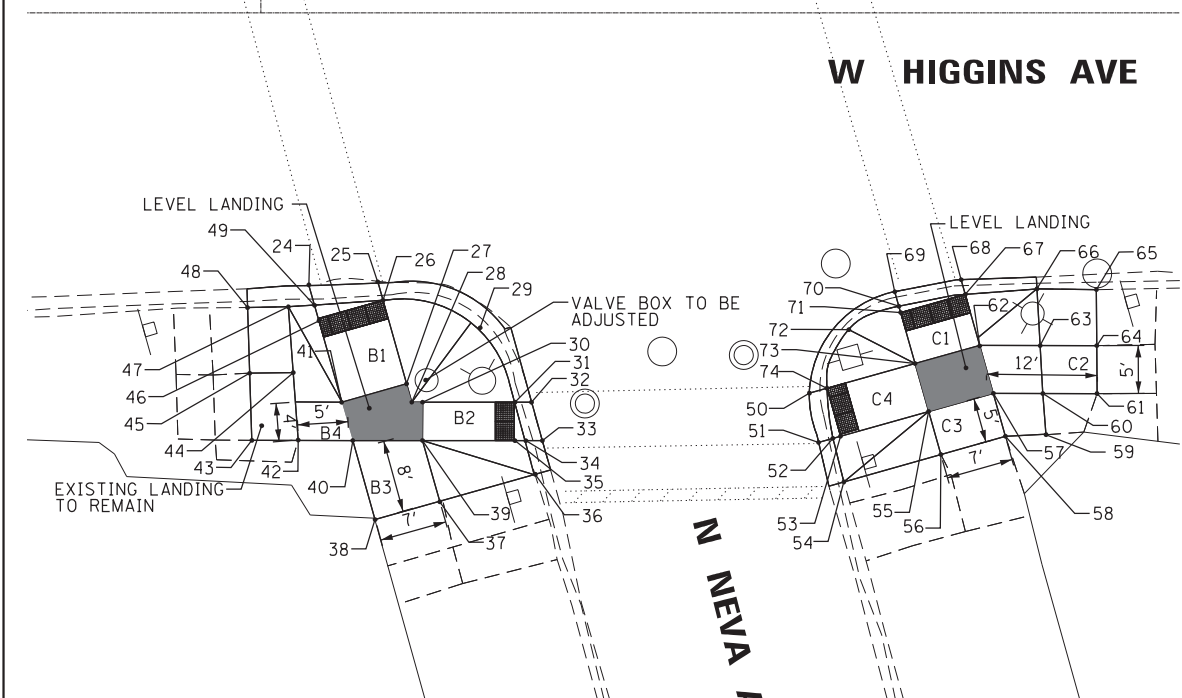
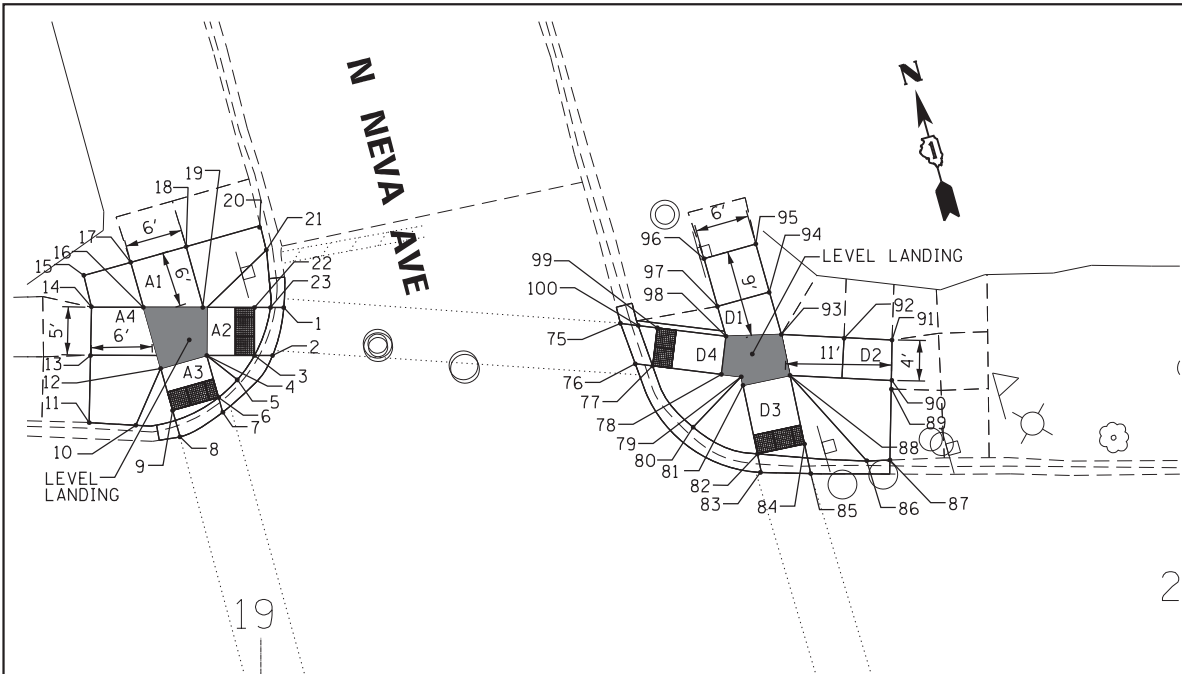


NOTE:
 1. ALL PAVEMENT MARKING SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED. SEE CITY OF CHICAGO DETAIL.

STRIPING LEGEND

- ① 4" DOUBLE YELLOW - 8" C-C
- ② 24" STOP BAR - WHITE
- ③ 4" SKIP DASH CENTERLINE - YELLOW - 6' LINE, 18' SPACE
- ④ 4" LINE - WHITE
- ⑤ 12" DIAGONAL LINES - YELLOW- 20' C-C SPACING
- ⑥ 6" CROSSWALK - WHITE
- ⑦ 24"- LONGITUDINAL BAR (SCHOOL & PEDESTRIAN) - WHITE

FILE NAME =	USER NAME = guilleumfp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN HIGGINS AVE. (IL. RTE. 43 (HARLEM AVE.) TO NAGLE AVE.)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\14456\Drawings\Design\144516-shr-plan.dgn		CHECKED -	REVISED -			3531	63Z-IRS-3	COOK	36	8
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62D22				
	PLOT DATE = 10/28/2016					SCALE: SHEET OF SHEETS STA. TO STA.		ILLINOIS FED. AID PROJECT		



PLAN AT W HIGGINS AVE AND N NEVA AVE

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 19+00	1936064.86	1127650.24	-		
STA 20+00	1936037.68	1127746.48	-		
CP #30	1935998.057	1127764.524	651.8	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION
CP #31	1935965.231	1127654.777	650.948	CUT CROSS IN WALK	SW CORNER, 50' SOUTH OF INTERSECTION
BM #600	1936038.708	1127606.112	651.335	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION

**N NEVA AVE
ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
1	19+02.39	28.07' LT	M.E. (650.62)
2	19+01.23	28.07' LT	M.E. (650.61)
3	18+99.34	28.07' LT	650.60
4	18+94.38	28.07' LT	651.01
5	18+97.56	28.07' LT	650.84
6	18+95.62	28.07' LT	650.82
7	18+96.05	28.07' LT	M.E. (650.83)
8	18+91.58	28.07' LT	M.E. (650.91)
9	18+90.81	28.07' LT	650.90
10	18+86.99	28.07' LT	M.E. (651.20)
11	18+82.13	28.07' LT	M.E. (651.19)
12	18+89.57	28.07' LT	651.13
13	18+82.27	34.45' LT	M.E. (651.23)
14	18+82.36	39.44' LT	M.E. (651.20)
15	18+81.56	42.77' LT	M.E. (651.30)
16	18+87.75	39.50' LT	651.15
17	18+86.43	44.13' LT	M.E. (651.08)
18	18+92.22	45.76' LT	M.E. (650.93)
19	18+94.39	39.42' LT	651.03
20	18+99.97	47.95' LT	M.E. (650.75)
21	19+00.58	45.41' LT	M.E. (650.74)
22	18+99.35	39.42' LT	650.63
23	19+01.02	39.41' LT	650.61
24	19+05.02	28.39' RT	M.E. (650.75)
25	19+12.23	28.16' RT	M.E. (650.79)
26	19+12.74	29.96' RT	650.78
27	19+15.20	38.66' RT	651.40
28	19+15.73	40.57' RT	651.42
29	19+22.87	32.82' RT	651.28
30	19+16.87	40.57' RT	651.40
31	19+26.45	40.58' RT	650.76
32	19+28.24	40.58' RT	M.E. (650.77)
33	19+29.35	44.59' RT	M.E. (650.76)
34	19+27.53	44.58' RT	650.76
35	19+26.44	44.58' RT	650.78
36	19+28.48	48.13' RT	XXX.XX
37	19+18.65	50.91' RT	M.E. (651.32)
38	19+11.92	52.81' RT	M.E. (651.40)
39	19+16.86	44.57' RT	651.42
40	19+09.59	44.56' RT	M.E. (651.52)
41	19+08.46	40.56' RT	651.46
42	19+03.91	44.51' RT	M.E. (651.53)
43	18+99.08	44.55' RT	M.E. (651.51)
44	19+03.39	37.49' RT	M.E. (651.41)

**N NEVA AVE
ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
45	18+98.85	37.56' RT	M.E. (651.39)
46	19+06.00	31.86' RT	650.77
47	19+02.88	30.64' RT	650.91
48	18+98.63	30.71' RT	M.E. (651.19)
49	19+05.61	30.46' RT	650.74
50	19+57.17	39.63' RT	M.E. (650.75)
51	19+58.12	44.76' RT	M.E. (650.76)
52	19+59.63	44.34' RT	650.75
53	19+60.41	44.12' RT	650.76
54	19+60.76	48.87' RT	M.E. (651.02)
55	19+69.60	41.55' RT	651.21
56	19+70.86	46.02' RT	M.E. (651.31)
57	19+76.34	39.66' RT	651.30
58	19+77.60	44.12' RT	M.E. (651.43)
59	19+81.82	43.90' RT	M.E. (651.56)
60	19+81.49	39.59' RT	651.52
61	19+87.14	39.68' RT	M.E. (651.77)
62	19+74.92	34.65' RT	651.25
63	19+81.21	34.67' RT	651.46
64	19+87.11	34.68' RT	M.E. (651.69)
65	19+87.08	28.84' RT	M.E. (651.50)
66	19+80.88	28.77' RT	M.E. (651.48)
67	19+73.42	29.33' RT	650.88
68	19+72.99	27.81' RT	M.E. (650.89)
69	19+66.07	29.07' RT	M.E. (650.81)
70	19+66.50	30.58' RT	650.80
71	19+66.68	31.23' RT	650.81
72	19+61.28	33.01' RT	651.34
73	19+68.18	36.54' RT	651.16
74	19+59.00	39.12' RT	650.74
75	19+37.48	37.77' LT	M.E. (650.62)
76	19+39.03	33.55' LT	M.E. (650.70)
77	19+40.83	33.33' LT	650.69
78	19+48.00	32.45' LT	651.20
79	19+50.07	32.20' LT	651.20
80	19+45.06	26.95' LT	M.E. (651.23)
81	19+50.25	31.31' LT	651.20
82	19+51.77	24.17' LT	650.92
83	19+52.18	22.23' LT	M.E. (650.91)
84	19+56.66	25.20' LT	650.87
85	19+57.31	22.11' LT	M.E. (650.88)
86	19+63.16	23.45' LT	M.E. (651.28)
87	19+65.57	23.53' LT	M.E. (651.28)
88	19+55.15	32.35' LT	651.20

**N NEVA AVE
ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
89	19+65.69	30.92' LT	M.E. (651.40)
90	19+65.70	31.82' LT	651.38
91	19+65.77	36.03' LT	M.E. (651.33)
92	19+60.78	36.28' LT	651.30
93	19+54.24	36.61' LT	M.E. (651.26)
94	19+53.00	41.02' LT	M.E. (651.15)
95	19+51.58	46.04' LT	M.E. (651.05)
96	19+46.20	44.52' LT	M.E. (651.00)
97	19+47.59	39.53' LT	651.12
98	19+48.48	36.42' LT	651.20
99	19+41.32	37.30' LT	650.62
100	19+39.34	37.54' LT	650.61

ADA RAMP SLOPE TABLE

RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	1.51%	1.81%	2.00%
A2	8.28%	0.56%	
A3	4.84%	2.00%	
A4	0.92%	0.40%	
B1	7.80%	0.86%	1.76%
B2	6.88%	0.50%	
B3	1.51%	0.54%	
B4	0.17%	1.44%	
C1	8.00%	1.29%	1.80%
C2	4.35%	0.96%	
C3	2.82%	1.28%	
C4	4.84%	1.00%	
D1	2.37%	1.04%	1.38%
D2	0.60%	1.37%	
D3	4.25%	0.68%	
D4	7.64%	1.00%	



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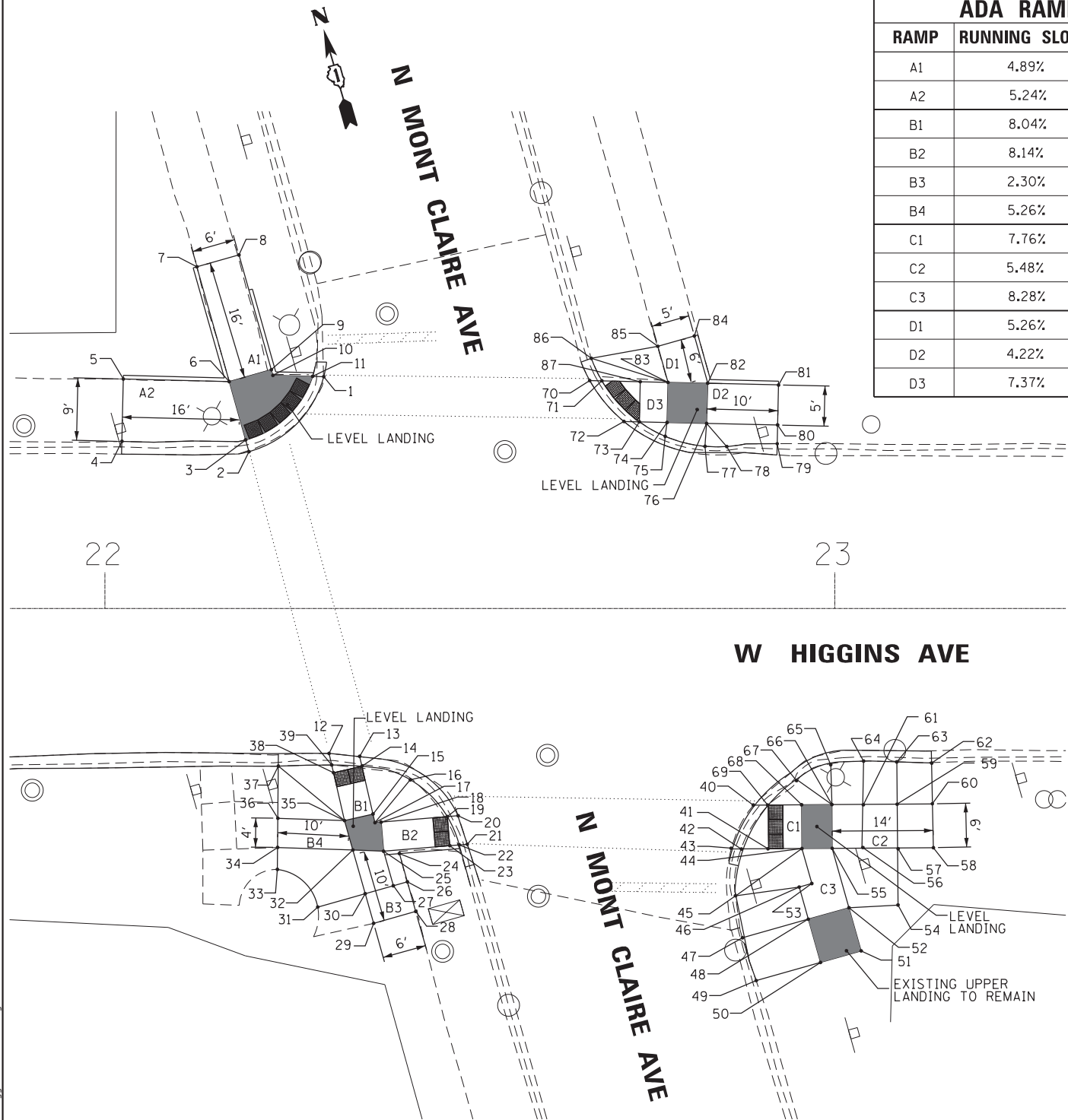


USER NAME : awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DESIGN - HIGGINS & NEVA AVE
W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE**
 SCALE- 1:10 SHEET 1 OF 16 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	9
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	4.89%	2.00%	2.00%
A2	5.24%	0.60%	
B1	8.04%	0.68%	1.90%
B2	8.14%	0.30%	
B3	2.30%	1.90%	
B4	5.26%	1.42%	1.75%
C1	7.76%	1.00%	
C2	5.48%	0.00%	
C3	8.28%	0.70%	1.28%
D1	5.26%	1.27%	
D2	4.22%	0.54%	
D3	7.37%	0.33%	

N MONT CLAIRE AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	22+29.99	31.77' LT	M.E. (651.94)
2	22+19.75	21.49' LT	M.E. (652.12)
3	22+19.29	23.10' LT	652.11
4	22+02.33	22.89' LT	M.E. (652.65)
5	22+02.53	31.44' LT	M.E. (652.92)
6	22+17.04	31.09' LT	652.16
7	22+12.62	46.82' LT	M.E. (652.96)
8	22+18.37	48.45' LT	M.E. (652.73)
9	22+22.81	32.72' LT	652.04
10	22+23.03	31.94' LT	652.03
11	22+28.46	31.81' LT	651.93
12	22+30.72	19.87' RT	652.26
13	22+34.93	20.31' RT	652.24
14	22+35.23	21.62' RT	652.23
15	22+36.73	28.17' RT	652.75
16	22+41.84	23.49' RT	652.56
17	22+37.01	29.36' RT	652.75
18	22+37.85	29.29' RT	652.75
19	22+46.92	28.52' RT	652.04
20	22+46.92	28.52' RT	M.E. (652.05)
21	22+49.64	32.31' RT	M.E. (652.03)
22	22+48.54	32.40' RT	652.02
23	22+47.25	32.51' RT	652.03
24	22+40.35	33.59' RT	652.78
25	22+38.18	33.27' RT	652.74
26	22+41.44	37.46' RT	652.94
27	22+39.52	38.00' RT	652.98
28	22+42.59	41.52' RT	M.E. (652.74)
29	22+36.82	43.15' RT	M.E. (653.04)
30	22+35.67	39.09' RT	M.E. (653.06)
31	22+29.17	40.92' RT	M.E. (653.17)
32	22+33.98	33.11' RT	652.82
33	22+23.64	35.76' RT	M.E. (653.37)
34	22+23.67	32.75' RT	M.E. (653.36)
35	22+32.84	29.06' RT	652.76
36	22+23.70	28.75' RT	M.E. (653.20)
37	22+23.77	21.61' RT	M.E. (652.59)
38	22+31.33	22.51' RT	652.26
39	22+31.08	21.45' RT	652.25
40	22+88.87	26.95' RT	M.E. (652.37)
41	22+90.84	32.93' RT	652.30
42	22+87.15	32.96' RT	652.27
43	22+85.81	32.97' RT	M.E. (652.28)
44	22+95.49	32.90' RT	652.65
45	22+86.39	39.38' RT	M.E. (652.34)

N MONT CLAIRE AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
46	22+95.12	38.18' RT	652.90
47	22+87.34	45.13' RT	M.E. (652.33)
48	22+96.36	42.59' RT	M.E. (653.23)
49	22+89.22	51.00' RT	M.E. (652.28)
50	22+97.95	48.53' RT	M.E. (653.27)
51	23+03.61	46.93' RT	M.E. (653.38)
52	23+01.93	41.01' RT	M.E. (653.43)
53	22+96.84	37.69' RT	652.92
54	23+08.65	40.65' RT	M.E. (653.61)
55	22+99.64	32.88' RT	652.73
56	23+03.71	32.85' RT	652.96
57	23+08.58	32.82' RT	653.22
58	23+13.49	32.78' RT	M.E. (653.49)
59	23+08.54	26.82' RT	653.14
60	23+13.37	26.78' RT	M.E. (653.37)
61	23+03.82	26.85' RT	652.92
62	23+13.26	21.19' RT	M.E. (653.00)
63	23+08.49	21.03' RT	M.E. (652.98)
64	23+03.94	20.92' RT	M.E. (652.90)
65	22+99.44	21.42' RT	M.E. (653.09)
66	22+99.60	26.88' RT	652.73
67	22+94.74	23.59' RT	M.E. (652.61)
68	22+95.45	26.90' RT	652.73
69	22+90.80	26.93' RT	652.36
70	22+66.42	31.23' LT	M.E. (652.23)
71	22+68.08	31.19' LT	652.22
72	22+71.10	25.62' LT	M.E. (652.34)
73	22+73.22	25.58' LT	652.33
74	22+77.02	25.49' LT	652.63
75	22+76.76	23.59' LT	M.E. (652.55)
76	22+82.44	25.36' LT	652.72
77	22+82.22	22.22' LT	M.E. (652.55)
78	22+85.19	22.19' LT	M.E. (652.43)
79	22+92.11	22.58' LT	M.E. (652.78)
80	22+92.16	25.14' LT	M.E. (652.94)
81	22+92.29	30.64' LT	M.E. (653.07)
82	22+82.58	30.86' LT	652.73
83	22+77.14	30.99' LT	652.63
84	22+80.75	37.34' LT	M.E. (652.92)
85	22+75.75	35.93' LT	M.E. (652.86)
86	22+66.64	34.28' LT	M.E. (652.58)
87	22+73.34	31.07' LT	652.31

PLAN AT W HIGGINS AVE AND N MONT CLAIRE AVE

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 22+00	1935983.32	1127938.95	-		
STA 23+00	1935956.14	1128035.19	-		
CP #32	1935959.79	1127914.64	652.846	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
CP #33	1936043.693	1128012.439	652.474	CUT CROSS IN CURB	NE CORNER, 50' NORTH OF INTERSECTION
BM #601	1935918.891	1128076.604	653.272	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88



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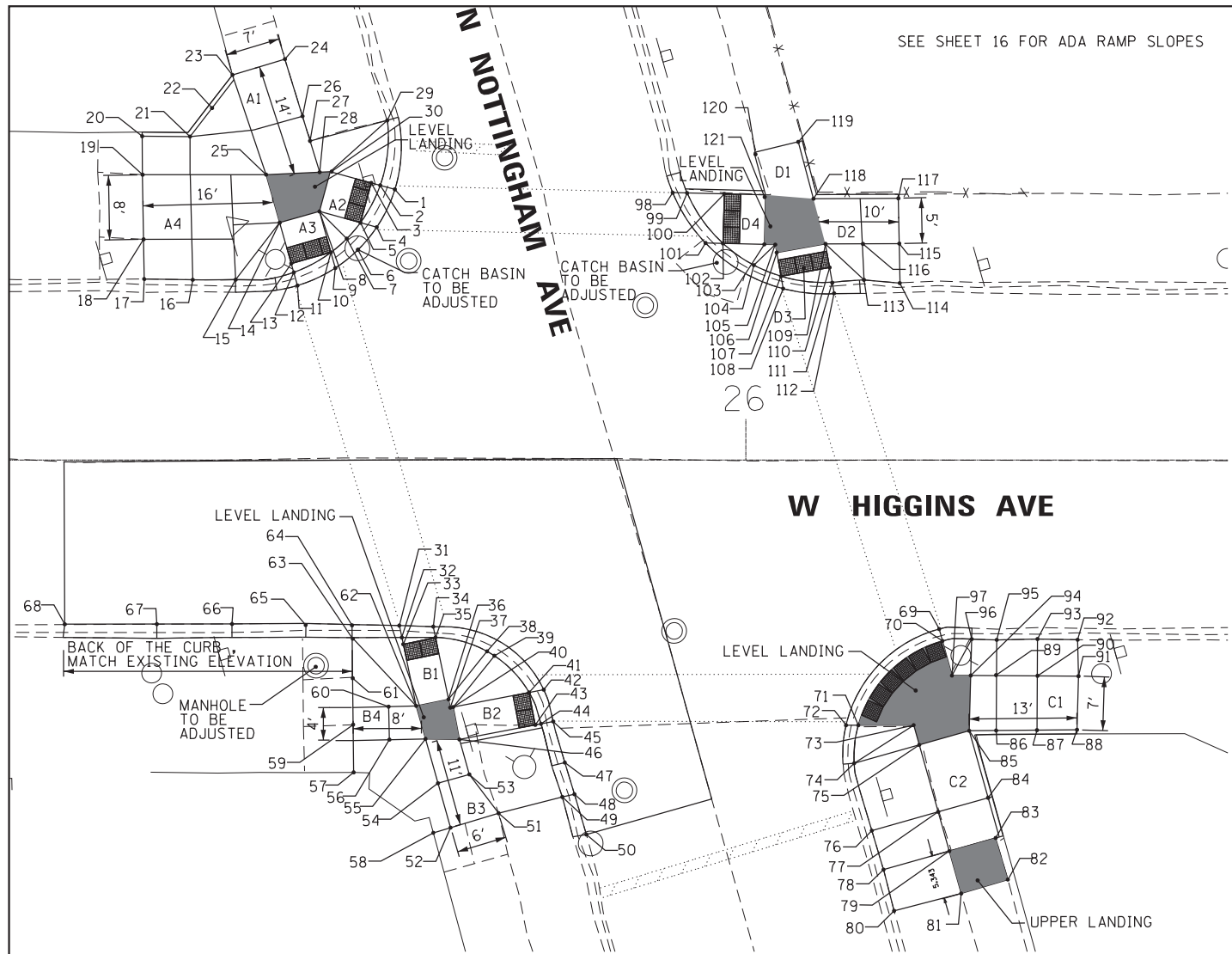


USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & MONT CLAIRE AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE
 SCALE- 1:10 SHEET 2 OF 16 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-IRS-3	COOK	36	10
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



PLAN AT W HIGGINS AVE AND N NOTTINGHAM AVE

POINT NO.:	STATION	OFFSET	ELEVATION
1	25+57.44	32.81' LT	M.E. (651.25)
2	25+55.67	33.29' LT	651.24
3	25+54.58	33.58' LT	651.24
4	25+55.25	28.22' LT	651.16
5	25+53.27	28.76' LT	651.15
6	25+52.98	25.48' LT	CB (651.00)
7	25+51.61	26.83' LT	651.30
8	25+48.29	30.12' LT	651.32
9	25+50.23	23.27' LT	651.12
10	25+49.69	25.16' LT	651.11
11	25+45.63	21.14' LT	M.E. (651.21)
12	25+45.18	22.73' LT	651.20
13	25+44.88	23.81' LT	651.20
14	25+43.48	28.76' LT	651.33
15	25+38.13	21.85' LT	M.E. (651.74)

POINT NO.:	STATION	OFFSET	ELEVATION
16	25+32.93	21.84' LT	M.E. (651.75)
17	25+27.04	21.93' LT	M.E. (651.78)
18	25+26.97	26.75' LT	M.E. (652.06)
19	25+26.83	34.57' LT	M.E. (652.23)
20	25+26.80	39.25' LT	652.42
21	25+32.60	39.21' LT	652.14
22	25+35.27	42.66' LT	652.92
23	25+37.76	46.68' LT	M.E. (652.09)
24	25+44.11	48.60' LT	M.E. (652.03)
25	25+41.83	34.59' LT	651.40
26	25+46.24	41.68' LT	651.67
27	25+47.28	38.71' LT	651.53
28	25+48.33	34.89' LT	651.33
29	25+56.52	41.16' LT	M.E. (651.48)
30	25+49.61	34.95' LT	651.32

POINT NO.:	STATION	OFFSET	ELEVATION
31	25+57.99	20.08' RT	651.46
32	25+58.29	21.53' RT	651.45
33	25+58.46	22.35' RT	651.46
34	25+62.10	20.21' RT	651.39
35	25+62.38	21.53' RT	651.38
36	25+63.95	29.02' RT	651.97
37	25+64.16	30.03' RT	651.97
38	25+68.63	23.20' RT	651.69
39	25+69.52	23.74' RT	651.69
40	25+64.52	29.96' RT	651.97
41	25+73.72	28.19' RT	651.26
42	25+75.50	27.85' RT	651.27
43	25+74.47	32.12' RT	651.24
44	25+75.17	31.99' RT	651.23
45	25+76.65	31.70' RT	651.24
46	25+65.27	33.89' RT	652.00
47	25+78.06	36.69' RT	651.20
48	25+79.23	40.53' RT	651.17
49	25+77.74	40.86' RT	M.E. (651.73)
50	25+80.67	45.40' RT	M.E. (651.13)
51	25+70.02	42.83' RT	M.E. (652.17)
52	25+64.20	44.58' RT	M.E. (652.28)
53	25+66.47	38.13' RT	652.10
54	25+62.68	39.20' RT	652.20
55	25+61.16	33.82' RT	652.05
56	25+56.78	33.92' RT	652.15
57	25+52.45	37.87' RT	M.E. (652.35)
58	25+62.11	45.21' RT	M.E. (652.28)
59	25+52.38	32.09' RT	M.E. (652.25)
60	25+56.69	29.92' RT	652.06
61	25+52.32	26.35' RT	M.E. (651.93)
62	25+60.04	29.84' RT	651.98
63	25+52.27	21.54' RT	M.E. (651.90)
64	25+52.25	19.97' RT	651.48
65	25+46.65	19.83' RT	651.50
66	25+37.69	19.89' RT	651.54
67	25+28.57	19.90' RT	651.58
68	25+17.41	19.92' RT	M.E. (651.62)
69	26+23.46	20.50' RT	M.E. (650.76)
70	26+23.86	21.95' RT	650.75
71	26+13.65	32.17' RT	650.93
72	26+12.17	32.17' RT	M.E. (650.94)
73	26+20.37	32.15' RT	650.98
74	26+13.15	36.77' RT	M.E. (651.60)
75	26+21.04	34.55' RT	650.99
76	26+15.29	44.94' RT	M.E. (651.66)

POINT NO.:	STATION	OFFSET	ELEVATION
77	26+23.34	42.67' RT	651.41
78	26+16.71	49.67' RT	M.E. (651.68)
79	26+24.68	47.42' RT	651.65
80	26+17.99	54.67' RT	M.E. (651.68)
81	26+26.13	52.56' RT	M.E. (651.75)
82	26+31.79	50.88' RT	M.E. (651.68)
83	26+30.31	45.83' RT	M.E. (651.65)
84	26+29.39	40.96' RT	651.42
85	26+27.10	32.84' RT	651.00
86	26+30.37	32.80' RT	651.23
87	26+35.32	32.76' RT	651.44
88	26+40.17	32.71' RT	M.E. (651.56)
89	26+30.41	26.15' RT	650.97
90	26+35.36	26.18' RT	651.14
91	26+40.36	26.21' RT	M.E. (651.31)
92	26+40.25	21.80' RT	M.E. (651.30)
93	26+35.38	21.71' RT	M.E. (651.31)
94	26+27.29	26.13' RT	650.87
95	26+30.44	21.82' RT	M.E. (651.32)
96	26+27.41	21.73' RT	M.E. (651.32)
97	26+25.01	26.14' RT	650.84
98	25+91.14	32.41' LT	M.E. (651.00)
99	25+92.89	32.36' LT	650.99
100	25+97.39	32.25' LT	651.00
101	25+95.12	26.31' LT	M.E. (650.89)
102	25+97.24	26.25' LT	650.91
103	26+02.25	26.13' LT	651.15
104	26+00.97	23.62' LT	M.E. (651.10)
105	26+03.58	26.10' LT	651.15
106	26+03.76	25.14' LT	651.15
107	26+04.27	22.27' LT	650.94
108	26+04.54	20.77' LT	M.E. (650.95)
109	26+09.67	26.20' LT	651.20
110	26+10.18	23.32' LT	650.99
111	26+10.50	21.55' LT	650.98
112	26+10.74	20.16' LT	M.E. (650.99)
113	26+14.34	21.82' LT	M.E. (651.20)
114	26+18.72	21.43' LT	M.E. (651.08)
115	26+18.62	26.23' LT	M.E. (651.35)
116	26+14.13	26.21' LT	M.E. (651.40)
117	26+18.51	31.72' LT	M.E. (651.55)
118	26+08.22	31.67' LT	651.25
119	26+06.36	38.56' LT	M.E. (651.61)
120	26+01.01	37.28' LT	M.E. (651.51)
121	26+02.40	32.13' LT	651.20

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 26+00	1935874.610	1128323.890	-	-	-
STA 27+00	1935847.43	1128420.13	-	-	-
CP #34	1935873.466	1128227.077	652.072	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
CP #35	1935954.609	1128336.283	650.773	CUT CROSS IN WALK	NE CORNER, 50' NORTH OF INTERSECTION
BM #602	1935881.156	1128388.868	651.087	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION



HORIZONTAL CONTROL: ILLINOIS STATE
 PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

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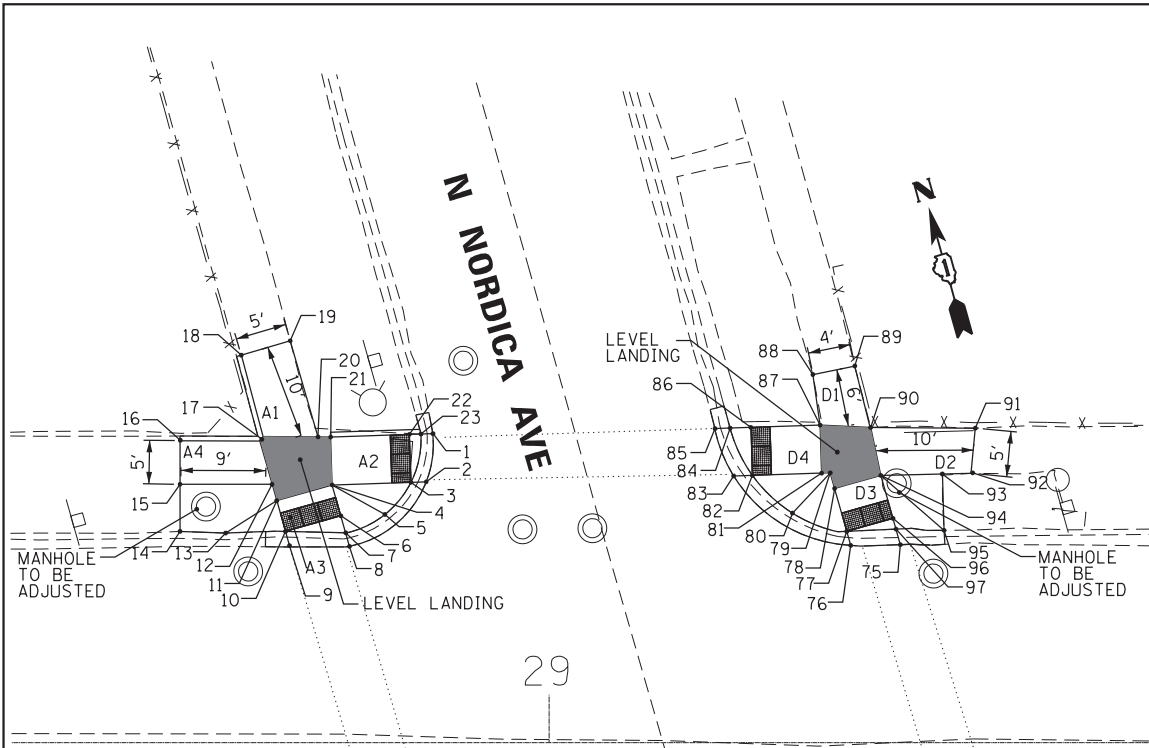
USER NAME : awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & NOTTINGHAM AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE

SCALE: 1:10 SHEET 3 OF 16 SHEETS STA. TO STA.

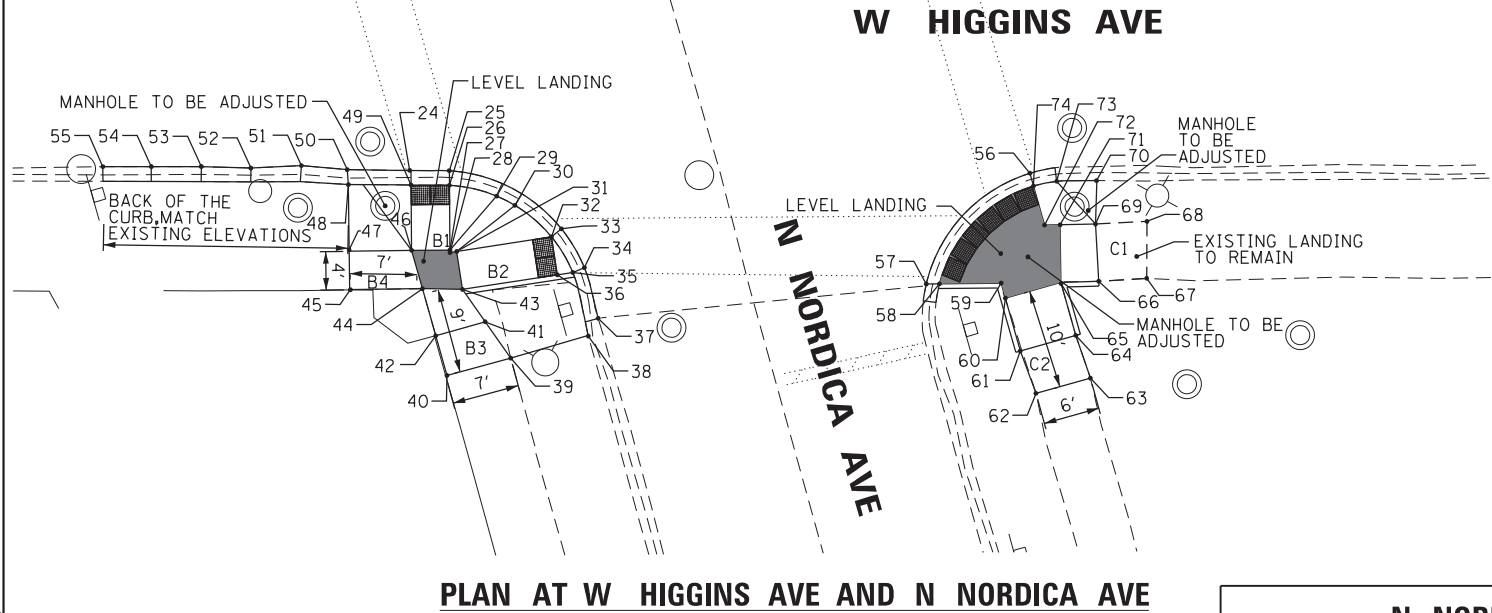
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-1RS-3	COOK	36	11
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	2.16%	0.00%	1.50%
A2	4.63%	1.80%	
A3	7.81%	1.17%	
A4	2.52%	1.27%	
B1	8.20%	1.00%	1.87%
B2	7.20%	1.00%	
B3	1.00%	1.87%	
B4	1.00%	1.65%	
C1	5.40%	1.33%	1.89%
C2	2.90%	1.50%	
D1	0.50%	1.33%	1.33%
D2	1.30%	0.39%	
D3	6.88%	1.40%	
D4	6.93%	1.00%	

N NORDICA AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
12	28+71.13	26.91' LT	650.67
13	28+66.30	21.84' LT	650.67
14	28+61.54	22.01' LT	M.E. (650.73)
15	28+61.55	26.92' LT	M.E. (650.78)
16	28+61.55	31.45' LT	M.E. (650.94)
17	28+70.07	31.61' LT	650.73
18	28+67.93	40.37' LT	M.E. (650.92)
19	28+73.03	41.87' LT	M.E. (650.73)
20	28+75.92	31.86' LT	650.73
21	28+77.24	31.85' LT	650.73
22	28+85.44	32.11' LT	650.35
23	28+86.64	32.15' LT	650.33
24	28+85.60	20.40' RT	650.77
25	28+89.60	20.43' RT	650.73
26	28+89.61	21.95' RT	650.72
27	28+89.67	28.69' RT	651.27
28	28+89.68	28.95' RT	651.27
29	28+94.54	23.08' RT	650.67
30	28+96.44	24.04' RT	650.65
31	28+90.37	28.84' RT	651.27
32	29+00.25	27.33' RT	650.59
33	29+01.29	26.49' RT	650.60
34	29+03.68	30.53' RT	650.55
35	29+02.47	31.03' RT	650.54
36	29+00.86	31.28' RT	650.55
37	29+05.11	35.81' RT	M.E. (651.50)
38	29+04.07	37.60' RT	M.E. (650.49)
39	28+96.00	39.96' RT	M.E. (651.32)
40	28+89.36	41.77' RT	M.E. (651.48)
41	28+93.34	36.17' RT	651.30
42	28+88.19	37.62' RT	M.E. (651.44)
43	28+90.98	32.80' RT	651.27
44	28+86.80	32.71' RT	M.E. (651.34)
45	28+79.23	32.83' RT	M.E. (651.28)
46	28+85.67	28.73' RT	651.27
47	28+79.17	28.83' RT	M.E. (651.19)
48	28+79.09	21.87' RT	M.E. (651.04)
49	28+85.61	21.92' RT	650.76
50	28+78.96	20.32' RT	650.65
51	28+74.19	19.90' RT	650.57
52	28+68.93	20.16' RT	650.48
53	28+63.76	19.99' RT	650.38
54	28+58.55	20.00' RT	650.28
55	28+53.50	20.00' RT	M.E. (650.19)
56	29+50.10	20.69' RT	M.E. (650.64)
57	29+39.30	30.56' RT	M.E. (650.62)

N NORDICA AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
58	29+40.67	32.21' RT	650.61
59	29+47.10	32.14' RT	650.69
60	29+47.54	33.70' RT	650.72
61	29+49.10	39.22' RT	M.E. (650.98)
62	29+50.73	43.63' RT	M.E. (651.01)
63	29+56.40	42.04' RT	M.E. (651.11)
64	29+54.87	37.59' RT	M.E. (651.08)
65	29+53.31	32.07' RT	650.81
66	29+57.27	31.95' RT	M.E. (651.01)
67	29+62.28	31.64' RT	M.E. (651.02)
68	29+62.30	25.71' RT	M.E. (650.94)
69	29+56.94	25.99' RT	M.E. (650.93)
70	29+57.07	21.46' RT	M.E. (650.92)
71	29+53.25	26.07' RT	650.73
72	29+51.62	26.09' RT	650.71
73	29+52.89	21.53' RT	650.92
74	29+50.48	22.02' RT	650.63
75	29+36.60	20.63' LT	M.E. (650.29)
76	29+31.43	20.55' LT	650.36
77	29+31.01	22.01' LT	650.35
78	29+29.75	26.49' LT	650.60
79	29+29.29	28.11' LT	650.60
80	29+25.36	23.91' LT	650.55
81	29+28.40	28.08' LT	650.60
82	29+21.19	27.84' LT	650.10
83	29+19.23	27.78' LT	650.11
84	29+18.87	32.77' LT	650.05
85	29+17.29	32.72' LT	M.E. (650.06)
86	29+21.02	32.85' LT	650.08
87	29+28.24	33.08' LT	650.55
88	29+27.49	38.35' LT	M.E. (650.58)
89	29+31.85	39.24' LT	M.E. (650.65)
90	29+33.50	2.84' LT	650.62
91	29+44.41	32.77' LT	M.E. (650.76)
92	29+43.96	28.06' LT	M.E. (650.68)
93	29+40.98	27.99' LT	M.E. (650.64)
94	29+34.56	27.84' LT	650.60
95	29+41.15	22.13' LT	M.E. (650.41)
96	29+35.84	23.37' LT	650.28
97	29+36.14	22.25' LT	650.28



N NORDICA AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	28+87.88	32.19' LT	650.34
2	28+87.20	27.17' LT	M.E. (650.43)
3	28+85.61	27.12' LT	650.42
4	28+77.40	26.85' LT	650.64
5	28+82.89	23.77' LT	650.60
6	28+78.31	23.64' LT	650.39
7	28+78.80	21.87' LT	650.38
8	28+79.20	20.46' LT	M.E. (650.39)
9	28+72.99	20.38' LT	M.E. (650.47)
10	28+72.53	22.01' LT	650.46
11	28+71.62	25.22' LT	650.65

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 29+00	1935793.07	1128612.60	-		
STA 30+00	1935765.89	1128708.84	-		
CP #36	1935714.999	1128606.71	650.327	CUT CROSS IN WALK	SW CORNER, 50' SOUTH OF INTERSECTION
CP #37	1935739.927	1128704.896	650.565	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION
BM #603	1935791.639	1128701.889	650.501	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & NORDICA AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE

SCALE- 1:10 SHEET 4 OF 16 SHEETS STA. TO STA.

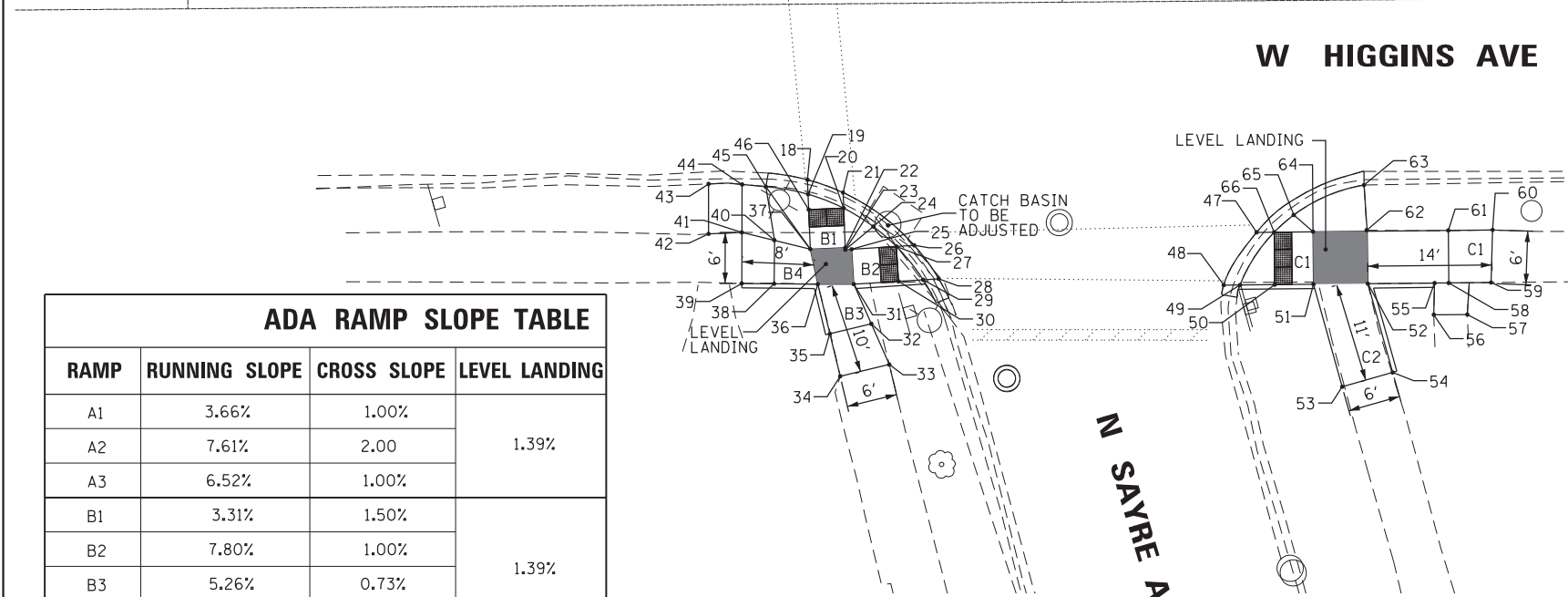
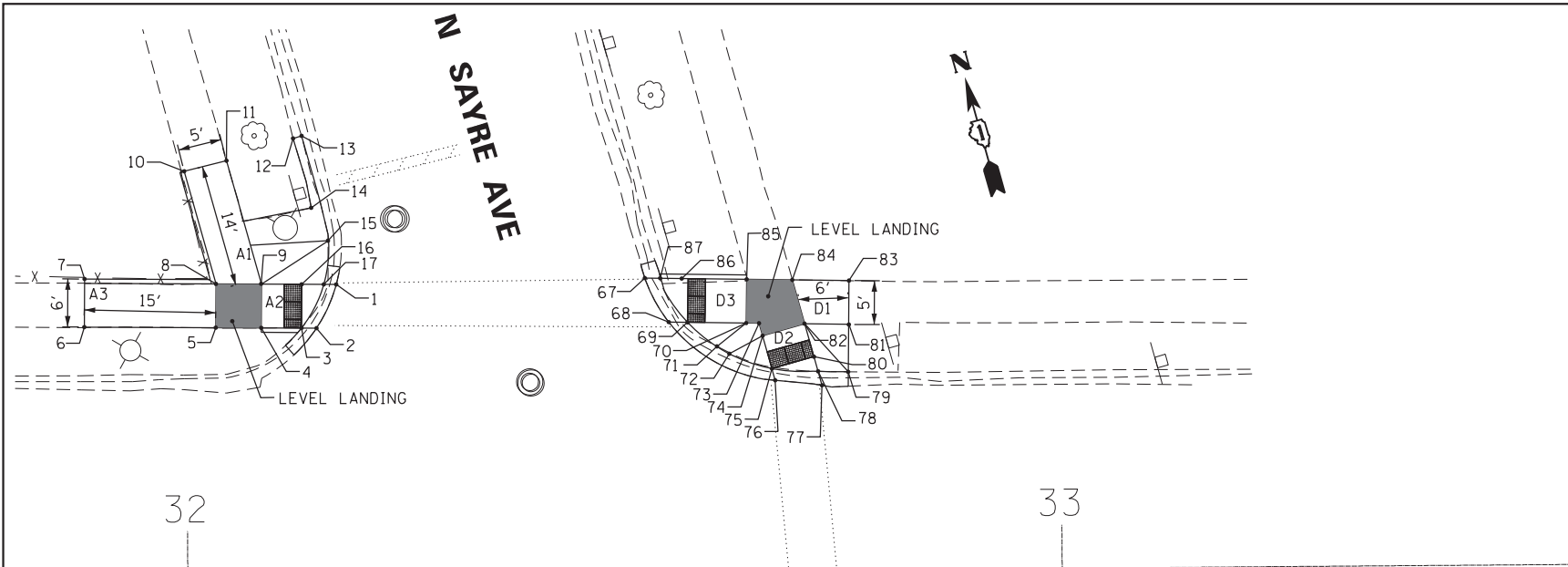
F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	12
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



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AES Services, Inc.
 111 S. Wacker Drive,
 Suite 3910
 Chicago, IL 60606
 Ph: 312-235-6783

USER NAME = awmohammed	DESIGNED - JA	REVISED -
	DRAWN - TOM	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - JA	REVISED -
PLOT DATE = 10/26/2016	DATE - 10/26/2016	REVISED -



PLAN AT W HIGGINS AVE AND N SAYRE AVE

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

ADA RAMP SLOPE TABLE

RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	3.66%	1.00%	1.39%
A2	7.61%	2.00%	
A3	6.52%	1.00%	
B1	3.31%	1.50%	1.39%
B2	7.80%	1.00%	
B3	5.26%	0.73%	
B4	5.20%	0.49%	
C1	4.37%	0.00%	1.94%
C2	6.08%	1.94%	
C3	7.95%	1.67%	
D1	4.30%	0.96%	1.56%
D2	1.88%	1.00%	
D3	1.48%	0.60%	

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 32+00	1935711.76	1128901.37	-		
STA 33+00	1935685.31	1128997.81	-		
CP #38	1935680.672	1128915.887	649.00	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
CP #39	1935710.351	1129025.454	647.484	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION
BM #604	1935637.143	1129072.553	648.468	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION

**N SAYRE AVE
 ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
1	32+17.22	33.06' LT	M.E. (647.99)
2	32+14.93	28.09' LT	M.E. (648.11)
3	32+13.21	28.11' LT	648.10
4	32+08.61	28.17' LT	648.30
5	32+03.40	28.23' LT	648.35
6	31+88.37	28.41' LT	M.E. (649.33)
7	31+88.43	33.41' LT	M.E. (649.35)
8	32+03.46	33.23' LT	648.40
9	32+08.67	33.17' LT	648.35
10	31+99.93	46.13' LT	M.E. (648.89)
11	32+04.79	47.30' LT	M.E. (648.68)
12	32+12.41	49.79' LT	M.E. (647.78)
13	32+13.38	50.08' LT	M.E. (647.76)
14	32+14.42	41.85' LT	M.E. (647.84)
15	32+16.37	38.08' LT	M.E. (648.03)
16	32+13.27	33.11' LT	648.00
17	32+15.79	33.08' LT	647.99
18	32+70.72	20.08' RT	648.05
19	32+70.77	21.73' RT	648.04
20	32+74.83	23.36' RT	648.00
21	32+74.77	21.57' RT	648.01
22	32+74.98	27.88' RT	648.15
23	32+74.99	28.11' RT	648.15
24	32+78.21	25.48' RT	647.96
25	32+75.70	28.07' RT	648.15
26	32+82.87	27.67' RT	647.76
27	32+80.83	27.78' RT	647.75
28	32+85.65	31.52' RT	M.E. (647.80)
29	32+83.86	31.62' RT	647.80
30	32+81.05	31.78' RT	647.79
31	32+75.93	32.06' RT	648.19
32	32+77.89	36.60' RT	648.33
33	32+79.91	41.27' RT	M.E. (648.59)
34	32+74.29	42.58' RT	M.E. (648.79)
35	32+73.15	37.71' RT	648.53
36	32+71.81	32.03' RT	648.22
37	32+70.98	28.01' RT	648.22
38	32+66.82	31.94' RT	648.48
39	32+63.11	31.87' RT	M.E. (648.67)
40	32+66.91	26.99' RT	648.26
41	32+63.18	26.05' RT	M.E. (648.45)
42	32+59.35	26.18' RT	M.E. (648.60)
43	32+59.40	20.48' RT	M.E. (648.54)
44	32+63.24	20.56' RT	M.E. (648.41)
45	32+65.95	20.85' RT	M.E. (648.32)

**N SAYRE AVE
 ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
46	32+70.83	23.49' RT	648.06
47	33+22.03	26.47' RT	M.E. (647.80)
48	33+18.23	32.48' RT	M.E. (647.88)
49	33+20.09	32.48' RT	647.87
50	33+24.06	32.47' RT	647.89
51	33+28.49	32.46' RT	648.14
52	33+34.71	32.45' RT	648.26
53	33+31.70	44.20' RT	M.E. (648.86)
54	33+37.48	42.61' RT	M.E. (648.90)
55	33+42.35	32.43' RT	648.87
56	33+42.23	36.05' RT	M.E. (648.98)
57	33+46.06	36.05' RT	M.E. (648.98)
58	33+43.97	32.43' RT	M.E. (648.88)
59	33+48.80	32.41' RT	M.E. (648.86)
60	33+49.03	26.41' RT	M.E. (648.68)
61	33+43.96	26.43' RT	648.76
62	33+34.70	26.45' RT	648.26
63	33+34.35	21.17' RT	M.E. (647.67)
64	33+28.48	26.46' RT	648.14
65	33+26.30	24.52' RT	M.E. (647.97)
66	33+24.05	26.47' RT	647.79
67	32+52.52	33.53' LT	M.E. (647.86)
68	32+55.22	28.48' LT	M.E. (647.91)
69	32+57.36	28.43' LT	647.90
70	32+64.07	28.30' LT	647.96
71	32+60.71	25.66' LT	648.01
72	32+62.12	24.80' LT	648.01
73	32+65.55	28.27' LT	647.99
74	32+65.94	26.84' LT	647.97
75	32+66.98	23.08' LT	648.04
76	32+67.34	21.74' LT	M.E. (648.05)
77	32+72.70	21.12' LT	M.E. (647.98)
78	32+72.25	22.73' LT	647.97
79	32+75.70	22.64' LT	M.E. (647.93)
80	32+71.80	24.40' LT	647.99
81	32+75.79	28.07' LT	M.E. (647.76)
82	32+70.76	28.17' LT	647.96
83	32+75.86	33.07' LT	M.E. (647.82)
84	32+69.38	33.20' LT	648.01
85	32+64.17	33.30' LT	M.E. (647.98)
86	32+56.74	33.45' LT	647.87
87	32+54.27	33.50' LT	647.85



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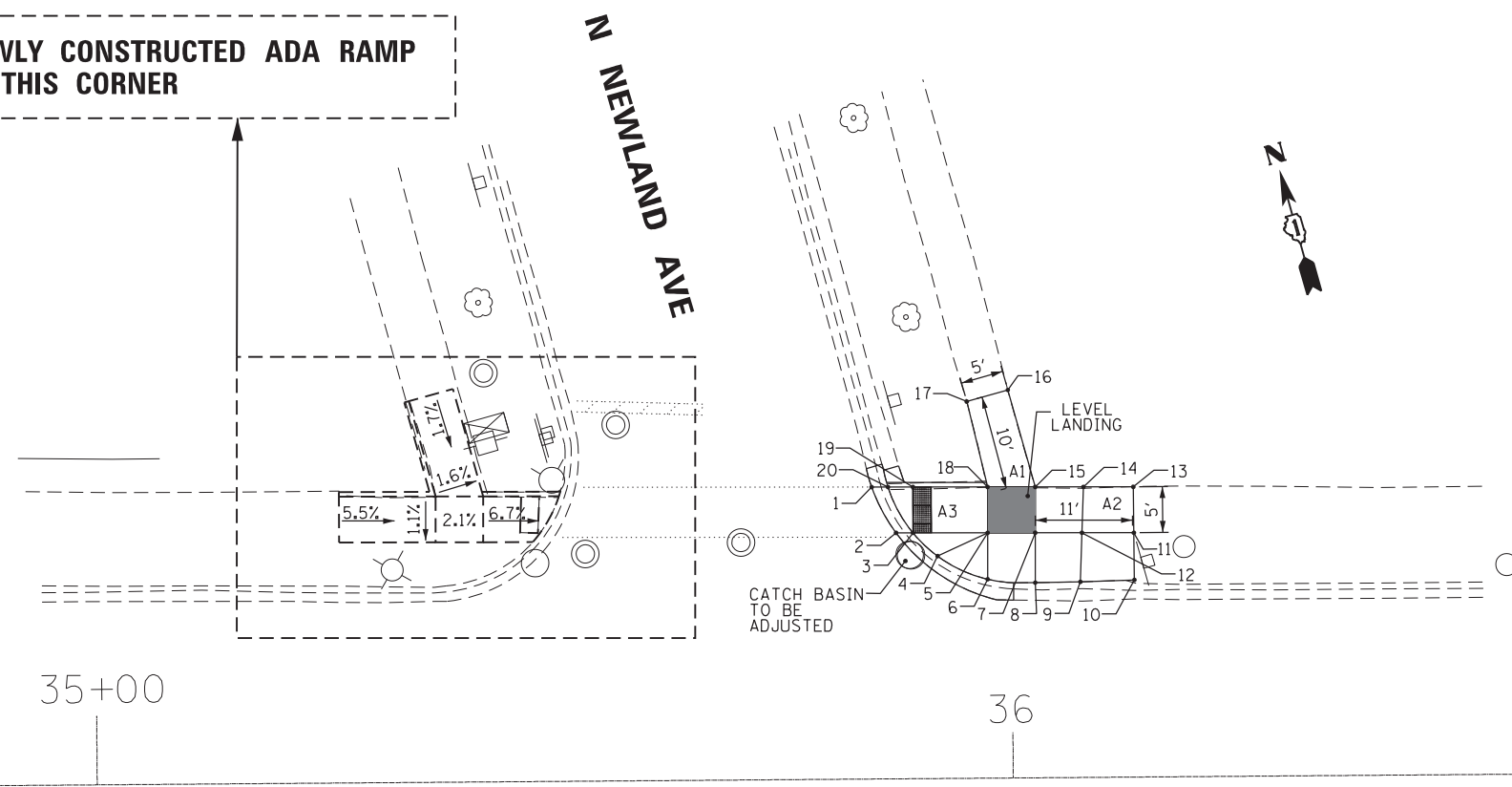
USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DESIGN - HIGGINS & SAYRE AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE**
 SCALE- 1:10 SHEET 5 OF 16 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-IRS-3	COOK	36	13
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				

NEWLY CONSTRUCTED ADA RAMP AT THIS CORNER



**N NEWLAND AVE
ADA RAMP ELEVATION TABLE**

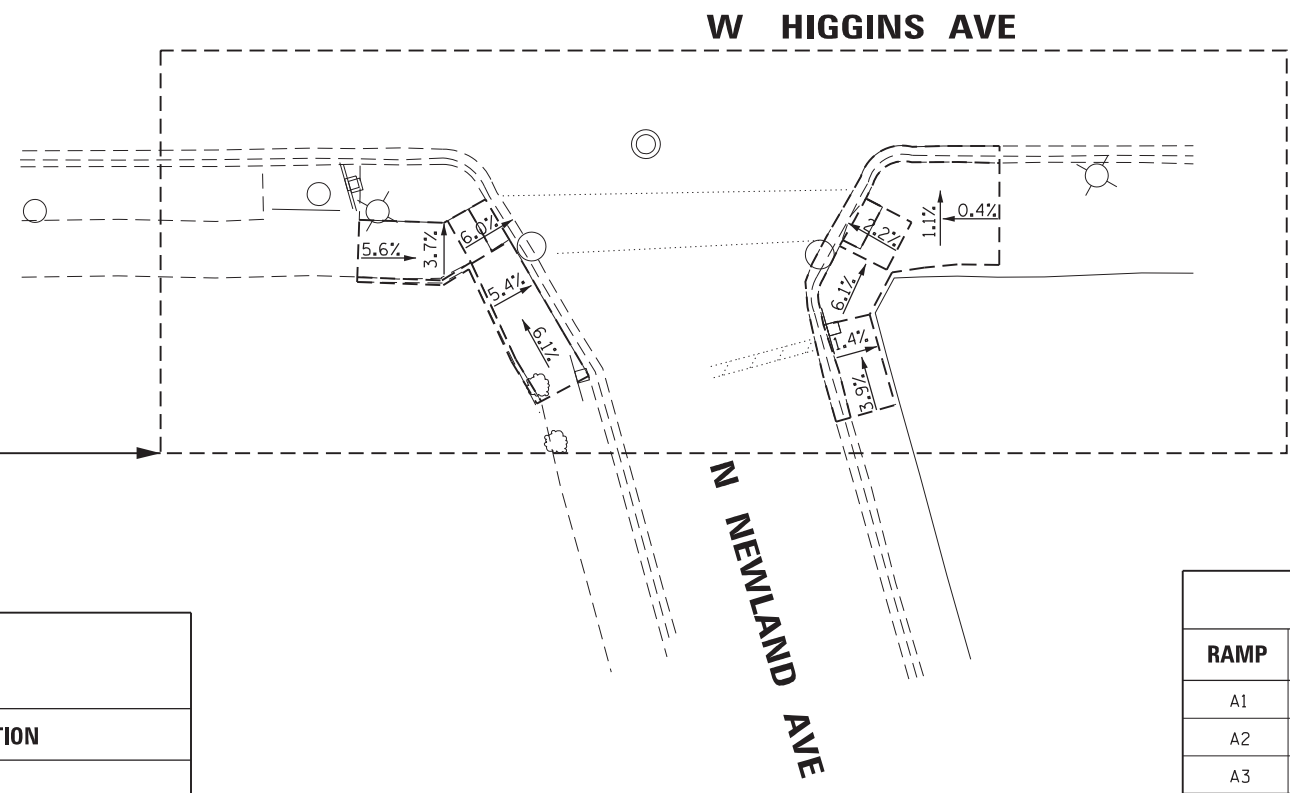
POINT NO.:	STATION	OFFSET	ELEVATION
1	35+84.78	31.85' LT	M.E. (644.76)
2	35+87.40	26.84' LT	M.E. (644.66)
3	35+89.26	26.82' LT	644.65
4	35+91.92	24.27' LT	643.14
5	35+97.40	26.77' LT	645.22
6	35+97.36	21.70' LT	645.22
7	36+02.59	26.73' LT	645.28
8	36+02.56	21.29' LT	645.24
9	36+07.49	21.35' LT	645.10
10	36+13.39	21.51' LT	M.E. (645.04)

**N NEWLAND AVE
ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
11	36+13.36	26.66' LT	M.E. (644.93)
12	36+07.70	26.70' LT	645.15
13	36+13.34	31.66' LT	M.E. (645.10)
14	36+07.90	31.70' LT	645.20
15	36+02.63	31.73' LT	645.28
16	35+99.72	42.34' LT	M.E. (644.82)
17	35+95.23	41.10' LT	M.E. (644.75)
18	35+97.43	31.77' LT	645.23
19	35+89.29	31.82' LT	644.75
20	35+86.56	31.84' LT	644.75

1. SE CORNER SOUTH RAMP LEADING TO THE LOWER LANDING HAS SLOPES > 5%, THEREFORE REQUIRING AN UPPER LEVEL PER IDOT STANDARD 424011.
2. SW CORNER SOUTH RAMP LEADING TO THE LOWER LANDING HAS SLOPES > 5%, THEREFORE REQUIRING AN UPPER LEVEL PER IDOT STANDARD 424011.
3. NW CORNER THE LEVEL LANDING HAS A SLOPE OF 2.1% WHICH IS GREATER THAN THE MAXIMUM > 2% PER IDOT STANDARD 424001.

NEWLY CONSTRUCTED ADA RAMP AT THIS CORNER



HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
VERTICAL CONTROL DATUM: NAVD 88

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 36+00	1935605.94	1129287.12	-		
STA 37+00	1935579.49	1129383.56	-		
CP #40	1935660.551	1129184.268	646.363	CUT CROSS IN WALK	NW CORNER, 50' WEST OF INTERSECTION
CP #41	1935579.901	1129274.563	645.603	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
BM #605	1935616.108	1129348.219	644.59	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION

ADA RAMP SLOPE TABLE

RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	5.42%	0.70%	1.20%
A2	3.24%	0.00%	
A3	7.00%	2.00%	

PLAN AT W HIGGINS AVE AND N NEWLAND AVE



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AES Services, Inc.
111 S. Wacker Drive,
Suite 3910
Chicago, IL 60606
Ph: 312-235-6783

USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DESIGN - HIGGINS & NEWLAND AVE
W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE**

SCALE: 1:10 SHEET 6 OF 16 SHEETS STA. TO STA.

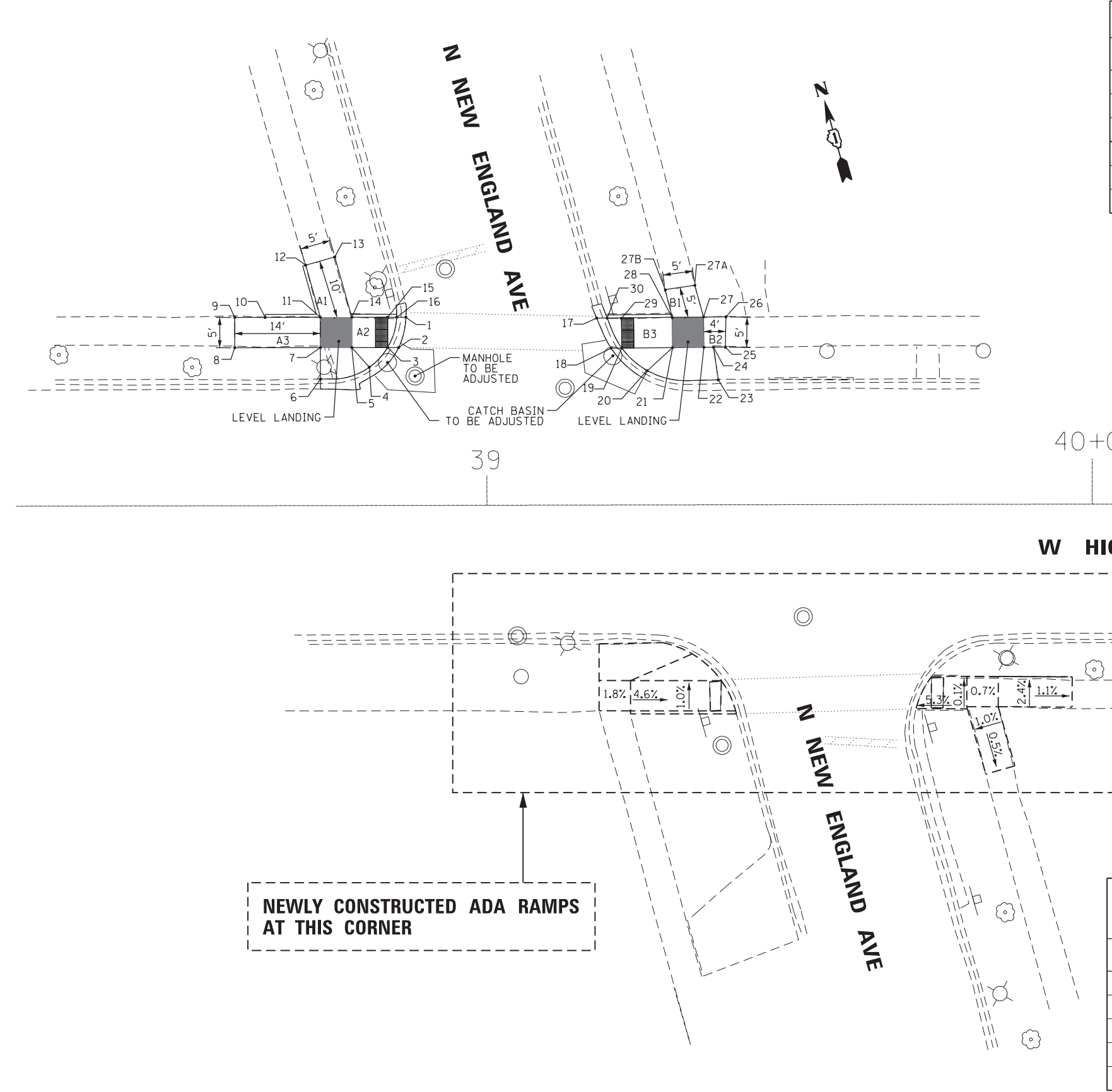
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	14
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				

ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	4.58%	1.40%	1.70%
A2	3.90%	2.00%	
A3	4.52%	1.40%	
B1	2.21%	1.40%	2.00%
B2	4.06%	1.60%	
B3	7.18%	2.00%	

N NEW ENGLAND AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	38+86.66	31.09' LT	M.E. (643.28)
2	38+85.47	26.10' LT	643.18
3	38+83.63	26.10' LT	643.17
4	38+80.60	22.84' LT	M.E. (643.06)
5	38+77.73	26.11' LT	643.40
6	38+72.55	20.91' LT	M.E. (643.50)
7	38+72.53	26.11' LT	643.45
8	38+58.39	26.13' LT	M.E. (644.08)
9	38+58.39	31.13' LT	M.E. (644.16)
10	38+63.41	31.12' LT	643.93
11	38+72.54	31.11' LT	643.52
12	38+70.12	39.73' LT	M.E. (643.93)
13	38+74.94	41.08' LT	M.E. (643.84)
14	38+77.74	31.11' LT	643.45
15	38+83.64	31.10' LT	643.27
16	38+85.15	31.10' LT	643.27
17	39+18.18	30.89' LT	M.E(643.28)
18	39+20.56	25.91' LT	643.18
19	39+22.33	25.93' LT	643.17
20	39+26.55	22.19' LT	M.E. (643.46)
21	39+30.70	25.99' LT	643.78
22	39+35.88	26.03' LT	643.84
23	39+38.25	20.65' LT	M.E. (643.08)
24	39+37.47	26.04' LT	M.E. (643.83)
25	39+39.44	26.06' LT	M.E. (643.83)
26	39+39.54	31.06' LT	M.E. (644.07)
27	39+35.84	31.03' LT	643.92
27A	39+34.41	36.27' LT	M.E. (644.04)
27B	39+29.49	35.59' LT	M.E. (643.85)
28	39+30.66	30.99' LT	643.85
29	39+22.29	30.93' LT	643.30
30	39+19.91	30.91' LT	643.27

CONTROL & BENCHMARK INFORMATION					
POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 39+00	1935525.73	1129576.20	-		
STA 40+00	1935498.74	1129672.49	-		
CP #42	1129576.20	1129537.512	643.65	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
CP #43	1935597.72	1129619.235	643.66	CUT CROSS IN WALK	NE CORNER, 50' NORTH OF INTERSECTION
BM #606	1935513.483	1129719.539	642.45	CUT CROSS IN WALK	NE CORNER, 100' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88



PLAN AT W HIGGINS AVE AND N NEW ENGLAND AVE

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USER NAME = awmohammed	DESIGNED - JA	REVISED -
	DRAWN - TOM	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - JA	REVISED -
PLOT DATE = 10/26/2016	DATE - 10/26/2016	REVISED -

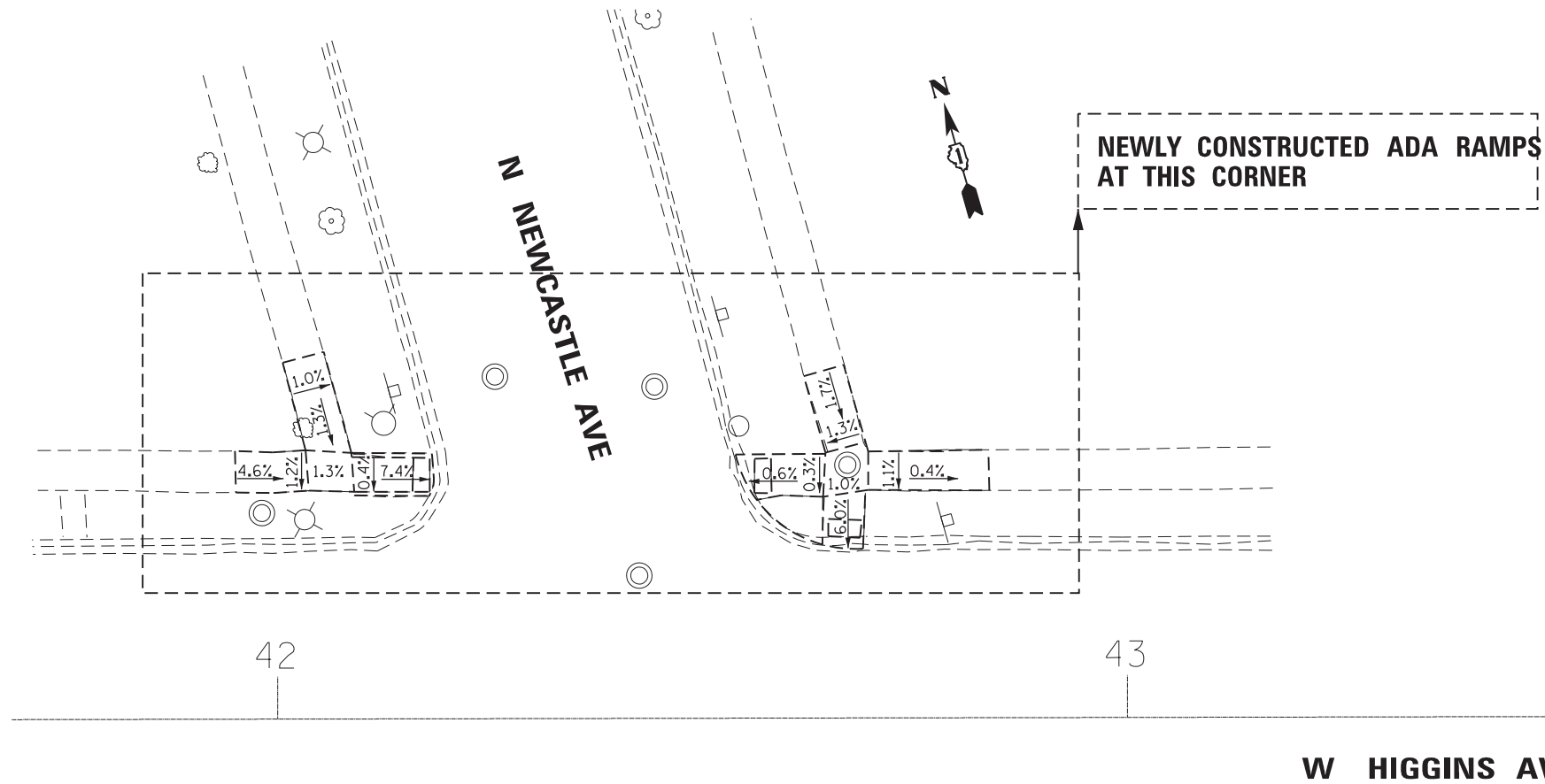
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & NEW ENGLAND AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE
 SCALE- 1:10 SHEET 7 OF 16 SHEETS STA. TO STA.

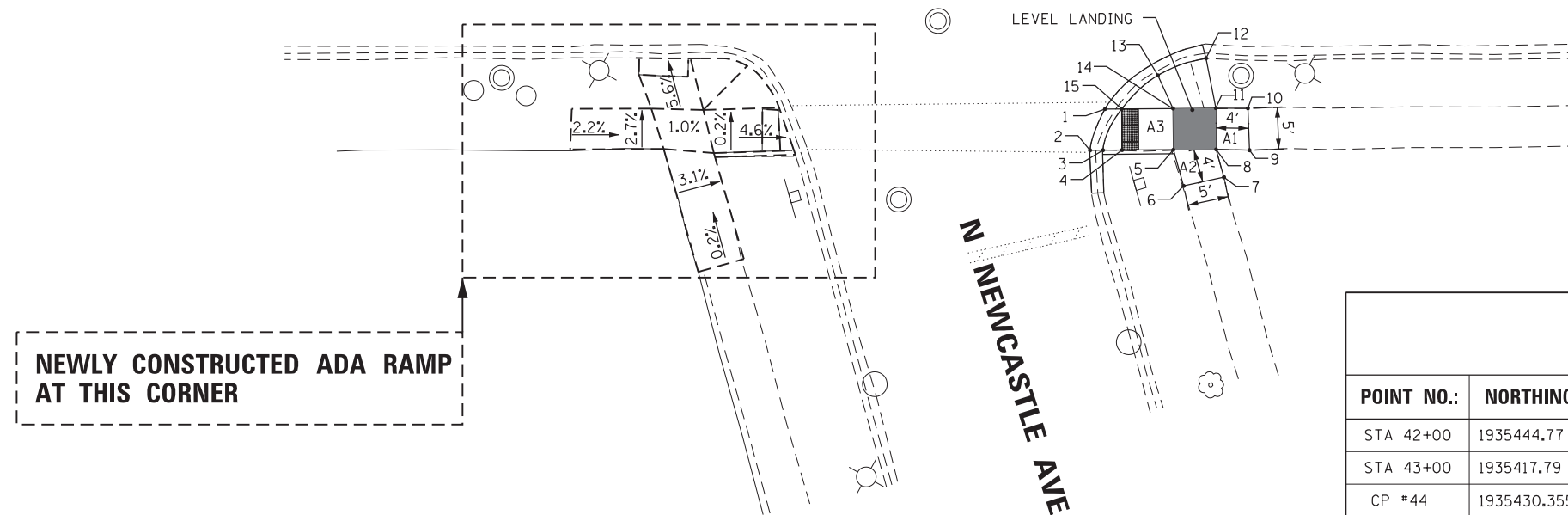
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	15
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				

ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	2.57%	0.80%	1.40%
A2	3.14%	1.20%	
A3	4.82%	2.00%	

N NEWCASTLE AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	43+11.90	28.57' RT	M.E. (639.41)
2	43+10.07	33.58' RT	M.E. (639.51)
3	43+11.63	33.57' RT	639.50
4	43+13.94	33.56' RT	639.50
5	43+20.17	33.53' RT	639.75
6	43+21.40	37.92' RT	M.E. (639.80)
7	43+26.30	36.88' RT	M.E. (639.58)
8	43+25.36	33.51' RT	639.69
9	43+29.41	33.61' RT	M.E. (639.68)
10	43+29.22	28.53' RT	M.E. (639.55)
11	43+25.34	28.51' RT	639.65
12	43+24.16	22.46' RT	M.E. (639.35)
13	43+18.31	24.51' RT	M.E. (639.59)
14	43+20.15	28.53' RT	639.70
15	43+13.92	28.56' RT	639.40



1. SOUTHWEST CORNER THE DETECTABLE TILE LOCATED ON THE NORTH RAMP IS INCORRECTLY ORIENTED PER IDOT STANDARD 424001.



CONTROL & BENCHMARK INFORMATION					
POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 42+00	1935444.77	1129865.07	-		
STA 43+00	1935417.79	1129961.36	-		
CP #44	1935430.355	1129811.243	641.56	CUT CROSS IN WALK	SW CORNER, 100' WEST OF INTERSECTION
CP #45	1935439.406	1129985.499	639.62	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION
BM #607	1935422.493	1130046.456	638.46	CUT CROSS IN WALK	NE CORNER, 100' EAST OF INTERSECTION

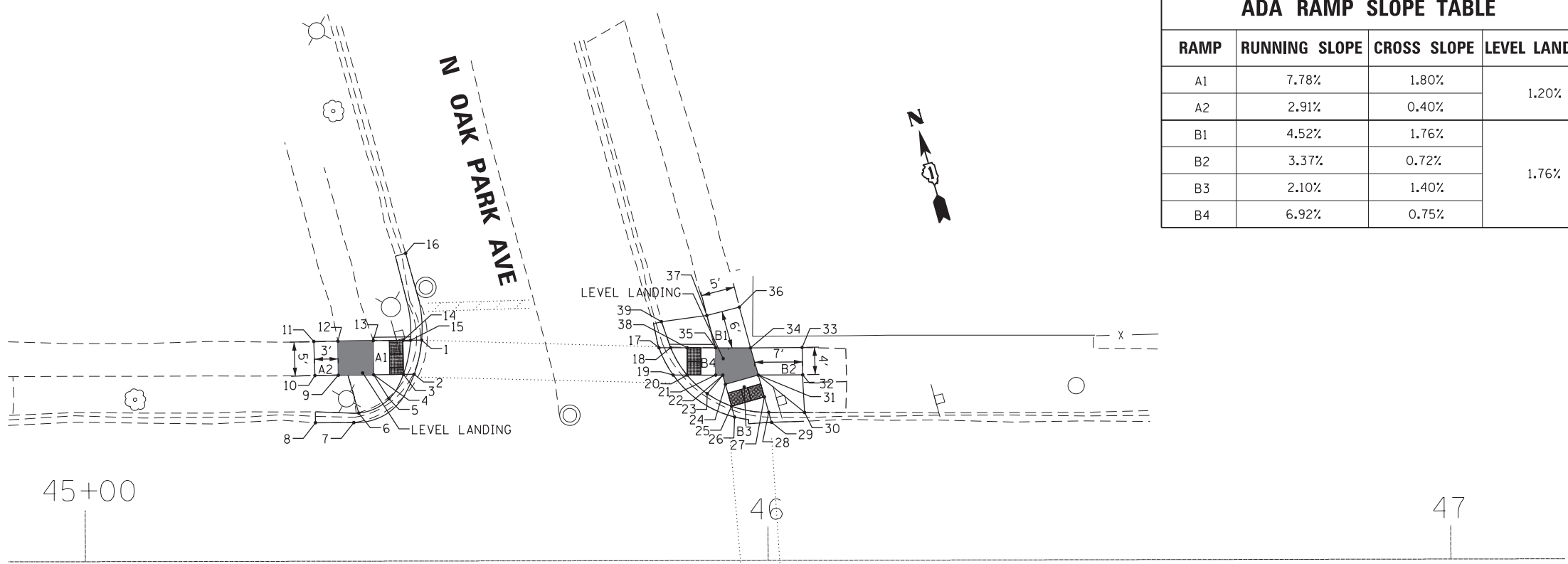
HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

PLAN AT W HIGGINS AVE AND N NEWCASTLE AVE



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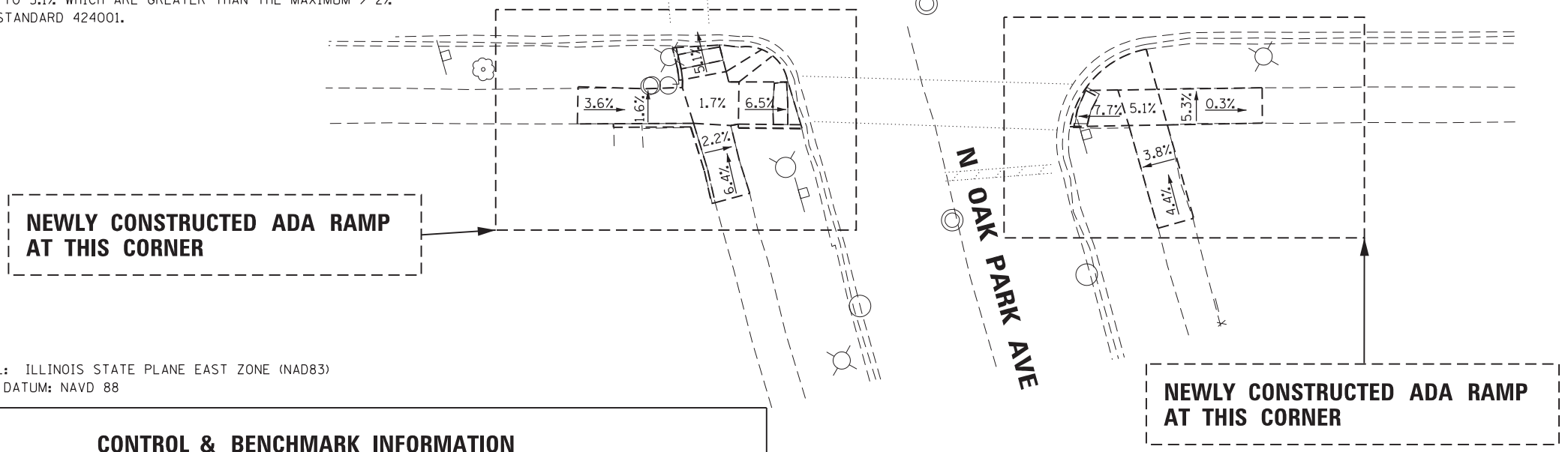
AES Services, Inc. 111 S. Wacker Drive, Suite 3910 Chicago, IL 60606 Ph: 312-235-6783	USER NAME = awmohammed DESIGNED - JA DRAWN - TOM CHECKED - JA DATE - 10/26/2016	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA RAMP DESIGN - HIGGINS & NEWCASTLE AVE W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE	F.A.U. RTE. 3531 SECTION 63Z-IRS-3 COUNTY COOK TOTAL SHEETS 36 SHEET NO. 16	CONTRACT NO. 62D22 ILLINOIS FED. AID PROJECT
	PLOT SCALE = 20.0000' / in. PLOT DATE = 10/26/2016	SCALE- 1:10 SHEET 8 OF 16 SHEETS STA. TO STA.				



RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	7.78%	1.80%	1.20%
A2	2.91%	0.40%	
B1	4.52%	1.76%	1.76%
B2	3.37%	0.72%	
B3	2.10%	1.40%	
B4	6.92%	0.75%	

POINT NO.:	STATION	OFFSET	ELEVATION
1	45+49.32	32.38' LT	636.66
2	45+48.22	27.36' LT	636.76
3	45+46.63	27.35' LT	636.75
4	45+42.29	27.30' LT	637.00
5	45+44.55	23.75' LT	637.27
6	45+40.09	21.70' LT	637.37
7	45+39.37	20.31' LT	M.E. (636.93)
8	45+33.74	20.30' LT	M.E. (637.21)
9	45+37.11	27.25' LT	637.04
10	45+33.68	27.21' LT	M.E. (636.94)
11	45+33.52	32.21' LT	M.E. (637.07)
12	45+37.06	32.25' LT	M.E. (637.06)
13	45+42.23	32.30' LT	M.E. (637.00)
14	45+46.58	32.35' LT	636.66
15	45+47.70	32.36' LT	636.65
16	45+47.00	45.10' LT	M.E. (635.85)
17	45+84.08	31.19' LT	M.E. (636.61)
18	45+85.79	31.18' LT	636.60
19	45+86.15	27.18' LT	M.E. (636.60)
20	45+88.19	27.17' LT	636.59
21	45+92.38	27.16' LT	636.88
22	45+93.42	27.16' LT	636.89
23	45+91.14	24.45' LT	M.E. (636.79)
24	45+93.80	25.80' LT	636.90
25	45+94.70	22.60' LT	636.83
26	45+95.14	21.01' LT	M.E. (636.84)
27	45+99.51	23.95' LT	636.90
28	46+00.14	21.70' LT	636.88
29	46+00.56	20.22' LT	M.E. (636.89)
30	46+05.40	21.66' LT	M.E. (636.97)
31	45+98.62	27.15' LT	636.91
32	46+05.13	27.18' LT	M.E. (637.13)
33	46+05.04	31.18' LT	M.E. (637.05)
34	45+97.50	31.15' LT	M.E. (636.94)
35	45+92.39	31.16' LT	636.85
36	45+95.87	37.11' LT	M.E. (636.66)
37	45+91.10	35.90' LT	M.E. (636.71)
38	45+88.20	31.17' LT	636.61
39	45+84.45	35.01' LT	M.E. (636.66)

- SOUTHEAST CORNER THE DETECTABLE TILE IS INCORRECTLY ORIENTED AND ARE THE INCORRECT TILE SHAPE PER IDOT STANDARD 424001.
- SOUTHEAST CORNER THE LEVEL LANDING HAS SLOPES RANGING FROM 3.4% TO 5.1% WHICH ARE GREATER THAN THE MAXIMUM > 2% PER IDOT STANDARD 424001.



PLAN AT W HIGGINS AVE AND N OAK PARK AVE

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 46+00	1935336.83	1130250.23	-		
STA 47+00	1935309.84	1130346.52	-		
CP #46	1935323.351	1130193.883	637.986	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
CP #47	1935419.061	1130251.805	635.021	CUT CROSS IN WALK	NE CORNER, 50' NORTH OF INTERSECTION
BM #608	1935347.817	1130314.01	636.997	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION



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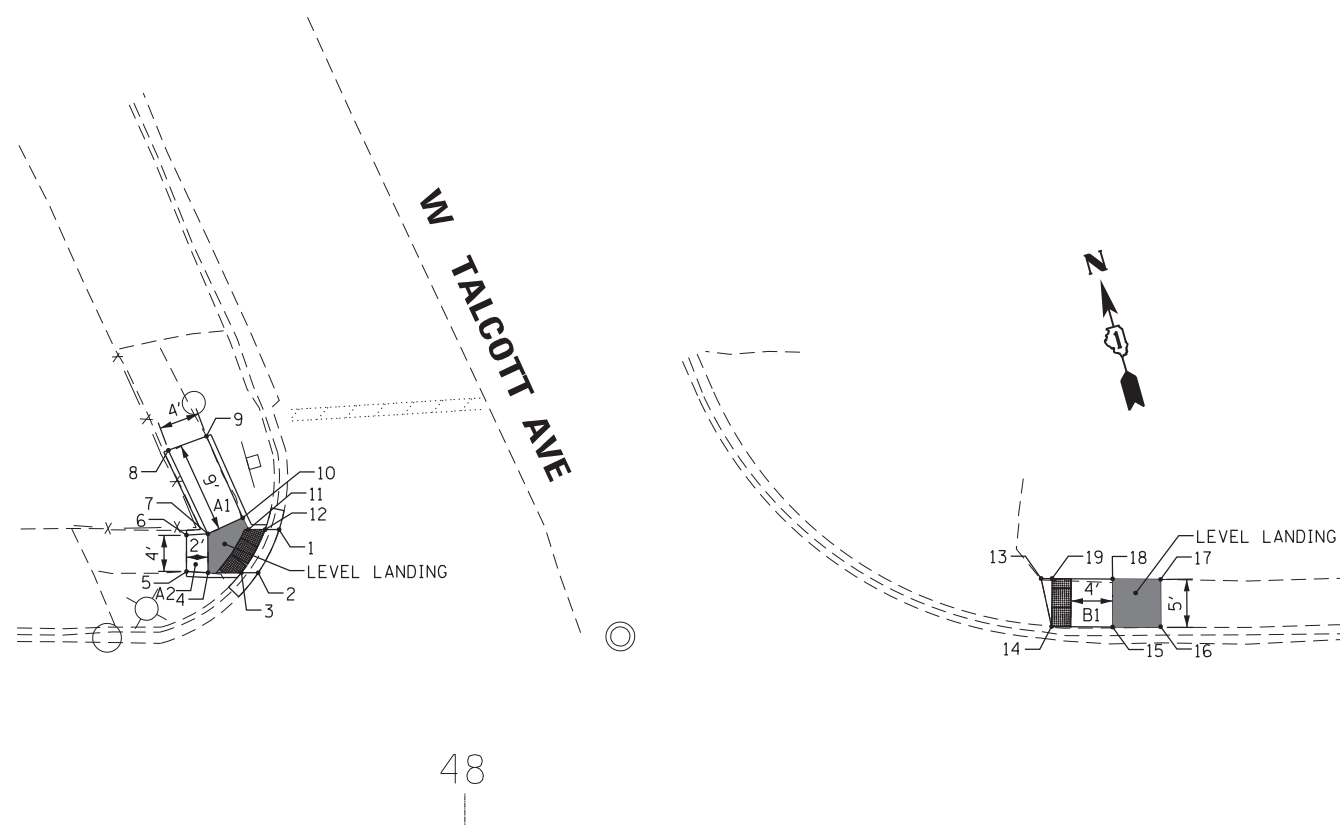
AES Services, Inc.
 111 S. Wacker Drive,
 Suite 3910
 Chicago, IL 60606
 Ph: 312-235-6783

USER NAME : awmohammed	DESIGNED - JA	REVISED -
	DRAWN - TOM	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - JA	REVISED -
PLOT DATE = 10/26/2016	DATE - 10/26/2016	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & OAK PARK AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE
 SCALE- 1:10 SHEET 9 OF 16 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	17
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



CONTROL & BENCHMARK INFORMATION					
POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 48+00	1935282.86	1130442.81	-		
STA 49+00	1935255.87	1130539.10	-		
CP #48	1935333.67	1130377.165	636.542	CUT CROSS IN WALK	NW CORNER, 50' WEST OF INTERSECTION
CP #49	1935373.137	1130420.299	636.106	CUT CROSS IN WALK	NW CORNER, 50' NORTH OF INTERSECTION
CP #50	1935227.903	1130521.499	637.801	CUT CROSS IN WALK	SW CORNER, 100' WEST OF INTERSECTION
CP #51	1935162.411	1130574.801	638.335	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
BM #609	1935246.611	1130492.945	636.382	CUT CROSS IN WALK	SW CORNER, 50' SOUTH OF INTERSECTION
BM #610	1935187.538	1130685.752	634.401	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION

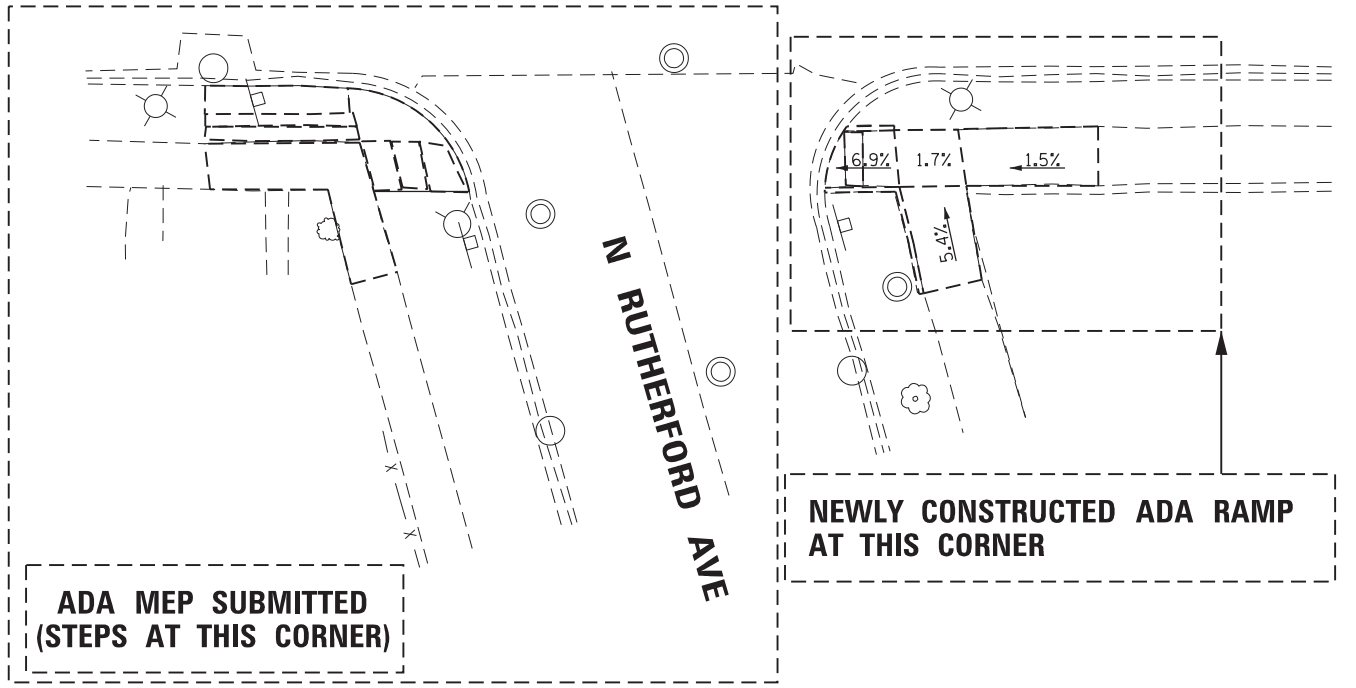
HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88

PLAN AT W HIGGINS AVE AND W TALCOTT AVE

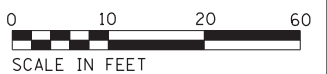
W HIGGINS AVE

W TALCOTT AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	47+80.70	32.49' LT	M.E. (636.13)
2	47+78.51	28.00' LT	M.E. (636.12)
3	47+76.76	28.01' LT	636.11
4	47+73.29	28.02' LT	636.17
5	47+71.05	28.15' LT	M.E. (636.26)
6	47+71.05	31.94' LT	M.E. (636.29)
7	47+73.29	32.07' LT	636.20
8	47+69.19	40.78' LT	M.E. (636.60)
9	47+73.15	42.24' LT	M.E. (636.43)
10	47+76.93	33.73' LT	636.16
11	47+77.50	32.51' LT	636.14
12	47+79.24	32.50' LT	636.12
13	48+60.09	27.25' LT	M.E. (635.61)
14	48+61.18	22.24' LT	M.E. (635.71)
15	48+67.55	22.17' LT	636.04
16	48+72.55	22.12' LT	M.E. (636.02)
17	48+72.54	27.12' LT	M.E. (636.06)
18	48+67.54	27.17' LT	636.05
19	48+61.24	27.23' LT	635.63

ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	3.74%	1.00%	1.69%
A2	4.01%	1.73%	
B1	6.70%	1.60%	0.80%



PLAN AT W HIGGINS AVE AND N RUTHERFORD AVE



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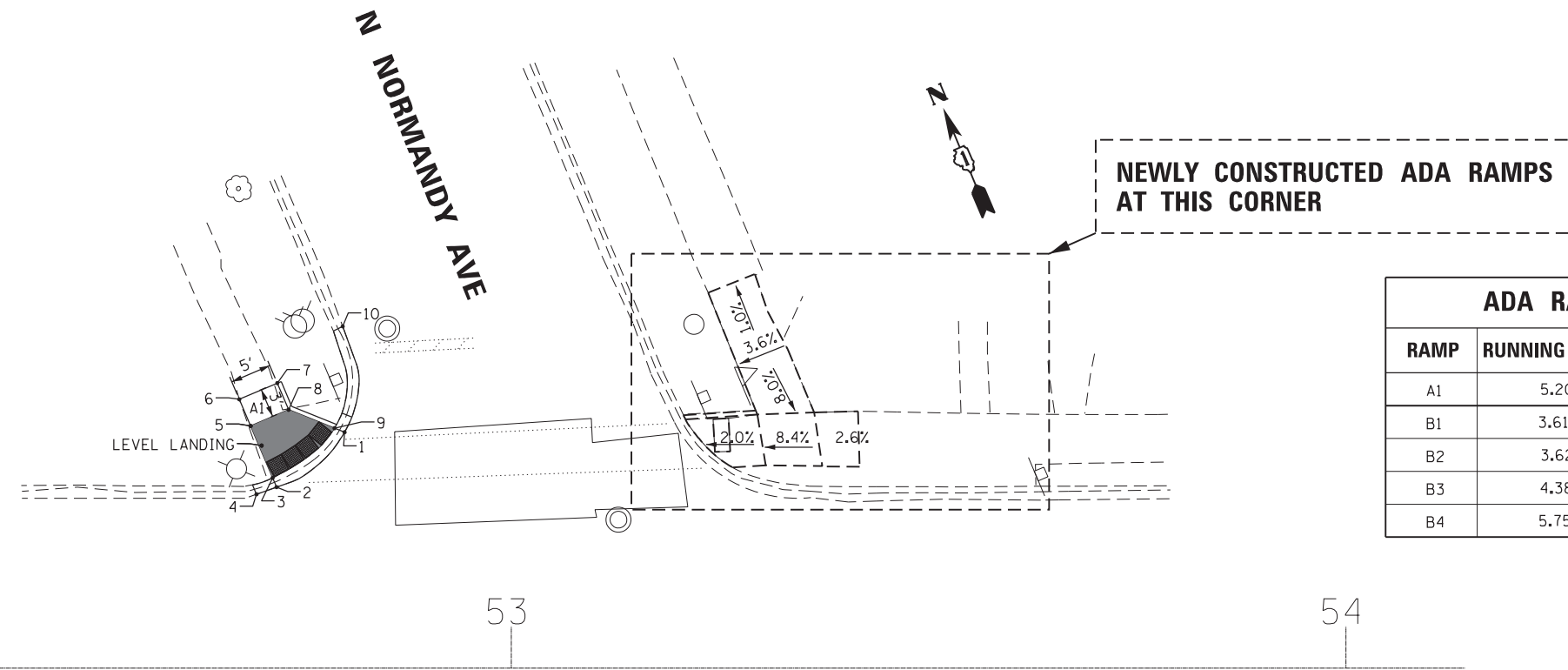
USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & TALCOTT AVE & RUTHERFORD
W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE

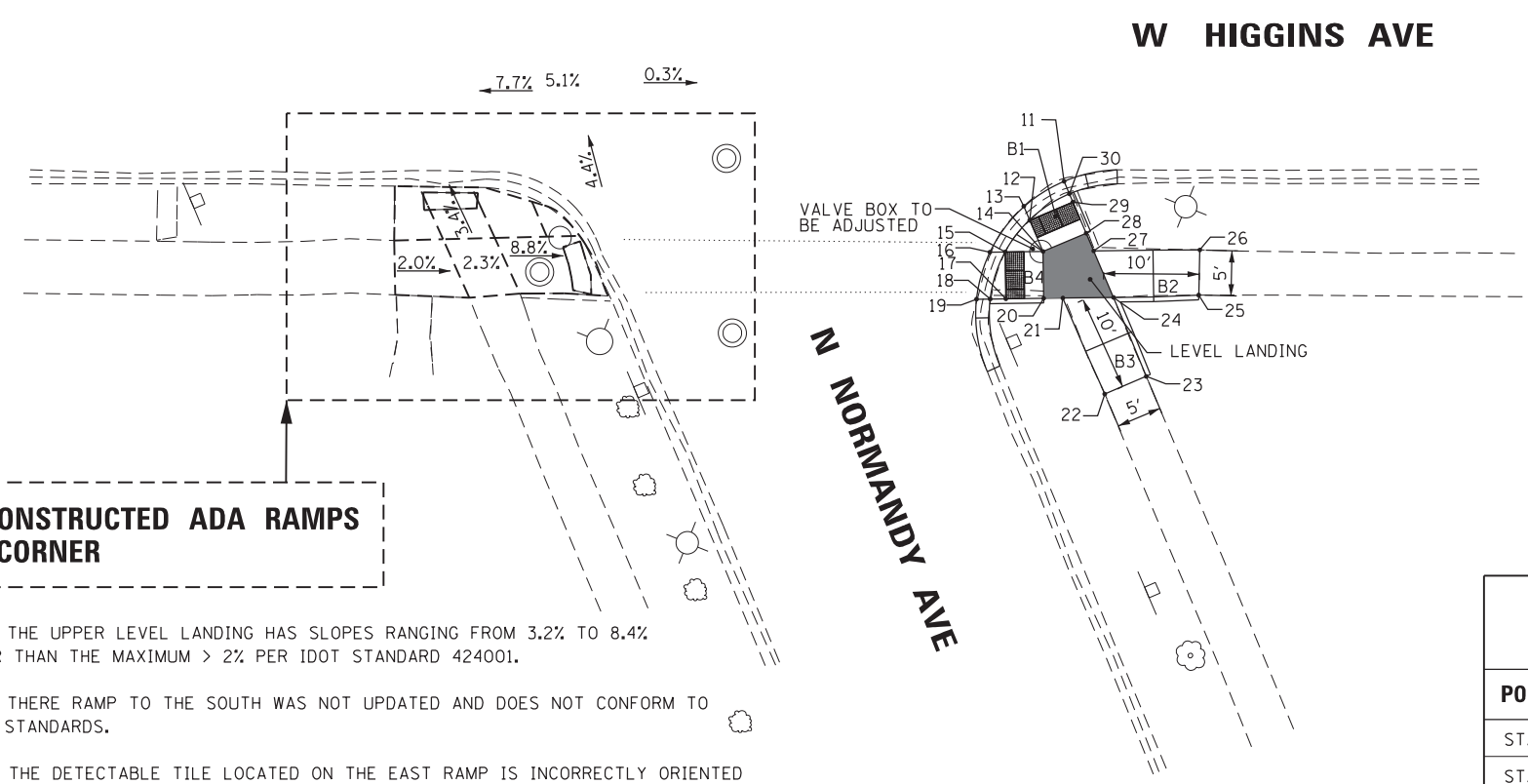
SCALE- 1:10 SHEET 10 OF 16 SHEETS STA. TO STA.

F.A.U. RTE. 3531	SECTION 63Z-IRS-3	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 18
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	5.20%	0.00%	1.99%
B1	3.61%	0.60%	1.00%
B2	3.62%	0.55%	
B3	4.38%	0.74%	
B4	5.75%	1.60%	

POINT NO.:	STATION	OFFSET	ELEVATION
1	52+80.02	28.26' LT	627.90
2	52+71.88	21.68' LT	627.98
3	52+71.40	22.80' LT	627.97
4	52+69.48	20.83' LT	M.E. (628.13)
5	52+68.77	29.01' LT	628.00
6	52+67.41	32.20' LT	M.E. (628.13)
7	52+71.97	34.14' LT	M.E. (627.82)
8	52+73.33	30.95' LT	628.00
9	52+78.82	28.74' LT	627.80
10	52+79.81	40.93' LT	M.E. (627.18)
11	53+60.06	21.67' RT	M.E. (626.67)
12	53+56.37	25.78' RT	626.70
13	53+55.78	24.38' RT	M.E. (626.71)
14	53+57.81	29.17' RT	626.80
15	53+53.74	29.23' RT	626.65
16	53+52.12	29.25' RT	M.E. (626.66)
17	53+53.83	34.23' RT	626.57
18	53+52.16	34.25' RT	626.56
19	53+50.70	34.27' RT	626.57
20	53+57.88	34.17' RT	626.80
21	53+59.93	34.15' RT	626.81
22	53+64.40	44.40' RT	M.E. (627.19)
23	53+68.83	42.52' RT	M.E. (627.25)
24	53+65.33	34.08' RT	626.85
25	53+74.44	33.83' RT	M.E. (627.18)
26	53+74.56	29.01' RT	M.E. (627.02)
27	53+63.22	29.11' RT	626.81
28	53+62.41	27.22' RT	626.80
29	53+61.00	23.89' RT	626.67
30	53+60.62	23.01' RT	626.66



PLAN AT W HIGGINS AVE AND N NORMANDY AVE

- NORTHEAST CORNER THE UPPER LEVEL LANDING HAS SLOPES RANGING FROM 3.2% TO 8.4% WHICH ARE GREATER THAN THE MAXIMUM > 2% PER IDOT STANDARD 424001.
- NORTHEAST CORNER THERE RAMP TO THE SOUTH WAS NOT UPDATED AND DOES NOT CONFORM TO CURRENT IDOT ADA STANDARDS.
- SOUTHWEST CORNER THE DETECTABLE TILE LOCATED ON THE EAST RAMP IS INCORRECTLY ORIENTED AND ARE THE INCORRECT TILE SHAPE PER IDOT STANDARD 424001.
- SOUTHWEST CORNER THE DETECTABLE TILE LOCATED ON THE NORTH RAMP IS INCORRECTLY ORIENTED PER IDOT STANDARD 424001.
- SOUTHWEST CORNER THE EAST RAMP HAS A SLOPE OF 8.8% WHICH IS GREATER THAN THE MAXIMUM > 8.33% PER IDOT STANDARD 424001.
- SOUTHWEST CORNER THE UPPER LEVEL LANDING HAS A SLOPE OF 2.3% WHICH IS GREATER THAN THE MAXIMUM > 2% PER IDOT STANDARD 424001.

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 53+00	1935124.99	1130916.35	-		
STA 54+00	1935085.87	1131008.38	-		
CP #52	1935045.48	1130903.858	627.539	CUT CROSS IN WALK	SW CORNER, 50' SOUTH OF INTERSECTION
CP #53	1935113.081	1131007.784	626.807	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION
BM #611	1935052.862	1131010.185	626.639	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88



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NOTE:
SEE SHEET 16 FOR ADA RAMP SLOPES

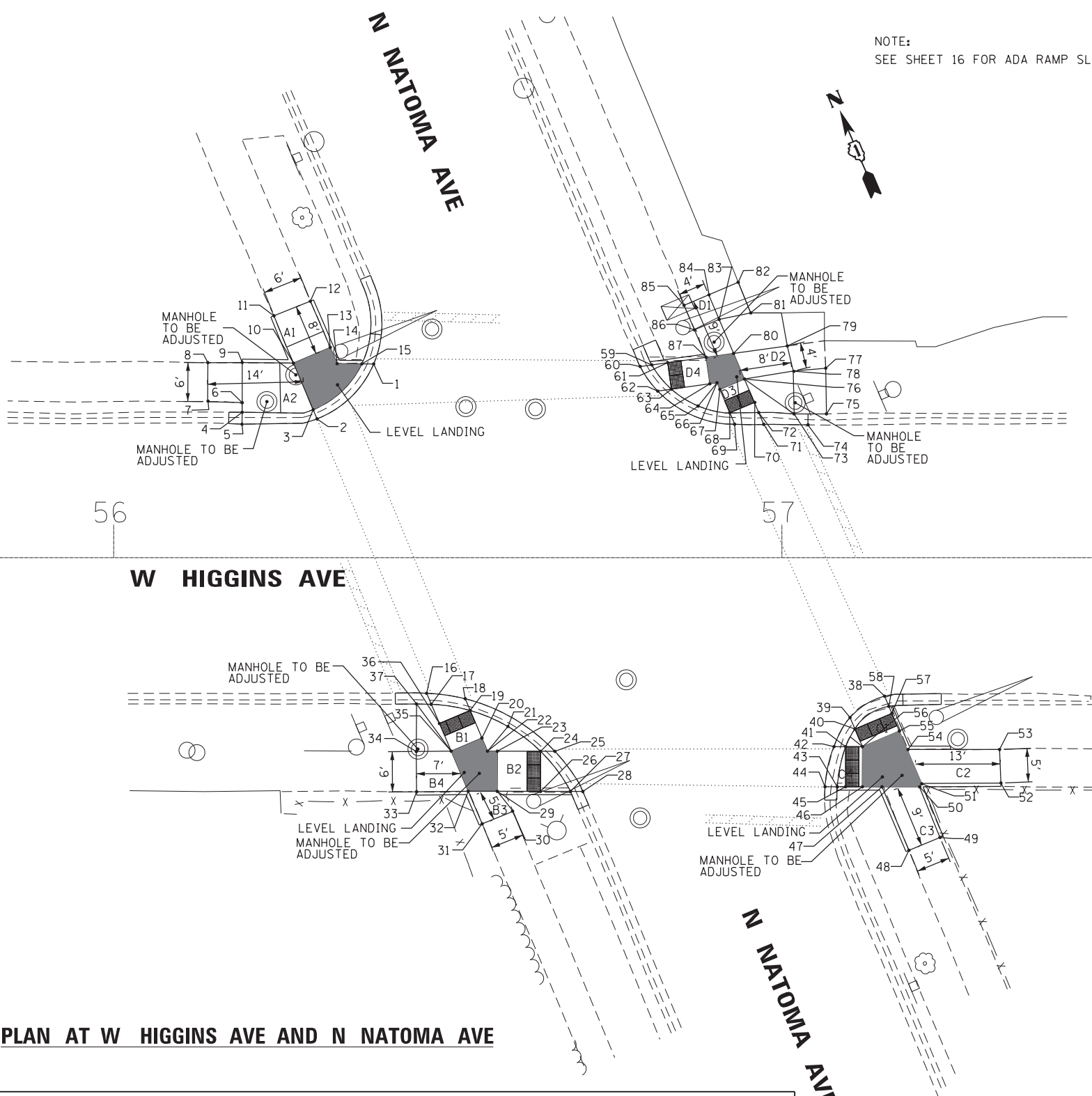


**N NATOMA AVE
ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
1	56+38.91	28.98' LT	623.92
2	56+30.44	20.78' LT	M.E. (624.10)
3	56+29.86	22.15' LT	624.09
4	56+19.23	21.77' LT	624.56
5	56+19.21	19.98' LT	M.E. (624.18)
6	56+19.24	23.21' LT	M.E. (624.56)
7	56+14.06	23.36' LT	M.E. (624.42)
8	56+14.11	29.19' LT	M.E. (624.67)
9	56+19.24	29.16' LT	M.E. (624.58)
10	56+26.90	29.12' LT	624.10
11	56+23.99	36.20' LT	M.E. (624.44)
12	56+29.44	38.38' LT	M.E. (624.24)
13	56+32.42	31.47' LT	623.98
14	56+33.45	29.04' LT	623.93
15	56+37.14	29.00' LT	623.91
16	56+46.82	20.29' RT	M.E. (624.13)
17	56+47.52	21.93' RT	624.12
18	56+52.60	21.10' RT	624.05
19	56+53.34	22.85' RT	624.04
20	56+55.10	26.98' RT	624.20
21	56+59.00	25.25' RT	624.02
22	56+55.94	28.95' RT	624.18
23	56+55.10	26.98' RT	624.14
24	56+63.88	28.97' RT	623.61
25	56+66.02	28.98' RT	M.E. (623.62)
26	56+63.87	34.97' RT	623.61
27	56+68.35	34.98' RT	623.59
28	56+70.20	34.99' RT	M.E. (623.60)
29	56+57.40	34.96' RT	624.24
30	56+59.78	37.96' RT	M.E. (624.18)
31	56+55.15	39.82' RT	M.E. (624.31)
32	56+53.05	34.94' RT	624.20
33	56+45.33	35.02' RT	M.E. (624.48)
34	56+45.29	28.99' RT	M.E. (624.37)
35	56+50.50	28.94' RT	624.20
36	56+48.74	24.81' RT	624.13
37	56+45.33	21.89' RT	M.E. (624.36)
38	57+15.34	20.72' RT	623.45
39	57+10.18	23.93' RT	M.E. (623.57)
40	57+10.90	25.62' RT	623.56
41	57+09.52	28.28' RT	623.58
42	57+07.74	28.28' RT	M.E. (623.59)
43	57+08.27	34.28' RT	623.58
44	57+06.43	34.28' RT	M.E. (623.59)

**N NATOMA AVE
ADA RAMP ELEVATION TABLE**

POINT NO.:	STATION	OFFSET	ELEVATION
45	57+09.51	34.28' RT	623.58
46	57+12.03	34.28' RT	623.73
47	57+14.96	34.29' RT	623.75
48	57+19.00	43.79' RT	M.E. (624.12)
49	57+23.64	41.86' RT	M.E. (624.16)
50	57+20.61	34.29' RT	623.78
51	57+20.91	33.83' RT	623.78
52	57+32.76	33.71' RT	M.E. (624.13)
53	57+32.56	28.70' RT	M.E. (624.02)
54	57+18.73	28.69' RT	623.17
55	57+17.56	25.93' RT	623.68
56	57+12.03	28.28' RT	623.70
57	57+16.43	23.29' RT	623.45
58	57+15.98	22.23' RT	623.44
59	56+80.52	28.85' LT	623.75
60	56+78.78	28.61' LT	M.E. (623.76)
61	56+82.90	29.19' LT	623.76
62	56+81.41	24.94' LT	M.E. (623.82)
63	56+83.45	25.22' LT	623.81
64	56+89.21	26.03' LT	624.12
65	56+87.44	22.69' LT	624.10
66	56+90.28	26.18' LT	624.12
67	56+90.70	25.19' LT	624.12
68	56+92.19	21.67' LT	623.82
69	56+92.91	19.98' LT	623.83
70	56+95.87	23.24' LT	623.82
71	56+97.27	19.96' LT	623.80
72	56+96.52	21.72' LT	623.79
73	57+01.81	21.52' LT	M.E. (624.26)
74	57+03.89	19.88' LT	M.E. (623.57)
75	57+06.65	21.53' LT	M.E. (624.18)
76	56+94.38	26.75' LT	624.12
77	57+06.55	28.36' LT	M.E. (624.42)
78	57+01.69	27.92' LT	M.E. (624.43)
79	57+00.80	31.68' LT	M.E. (624.56)
80	56+92.76	30.56' LT	624.20
81	56+95.33	36.64' LT	M.E. (624.68)
82	56+93.45	41.22' LT	M.E. (624.67)
83	56+90.59	35.66' LT	624.38
84	56+89.03	39.34' LT	M.E. (624.52)
85	56+85.35	37.77' LT	M.E. (624.47)
86	56+86.91	34.10' LT	624.30
87	56+88.66	29.99' LT	624.12



PLAN AT W HIGGINS AVE AND N NATOMA AVE

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 56+00	1935007.65	1131192.44	-		
STA 57+00	1934968.53	1131284.48	-		
CP #54	1935044.247	1131180.627	624.984	CUT CROSS IN WALK	NW CORNER, 50' WEST OF INTERSECTION
CP #55	1935048.143	1131286.388	624.059	CUT CROSS IN WALK	NE CORNER, 50' NORTH OF INTERSECTION
BM #612	1934973.9	1131338.648	624.053	CUT CROSS IN WALK	NE CORNER, 50' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE
PLANE EAST ZONE (NAD83)
VERTICAL CONTROL DATUM: NAVD 88



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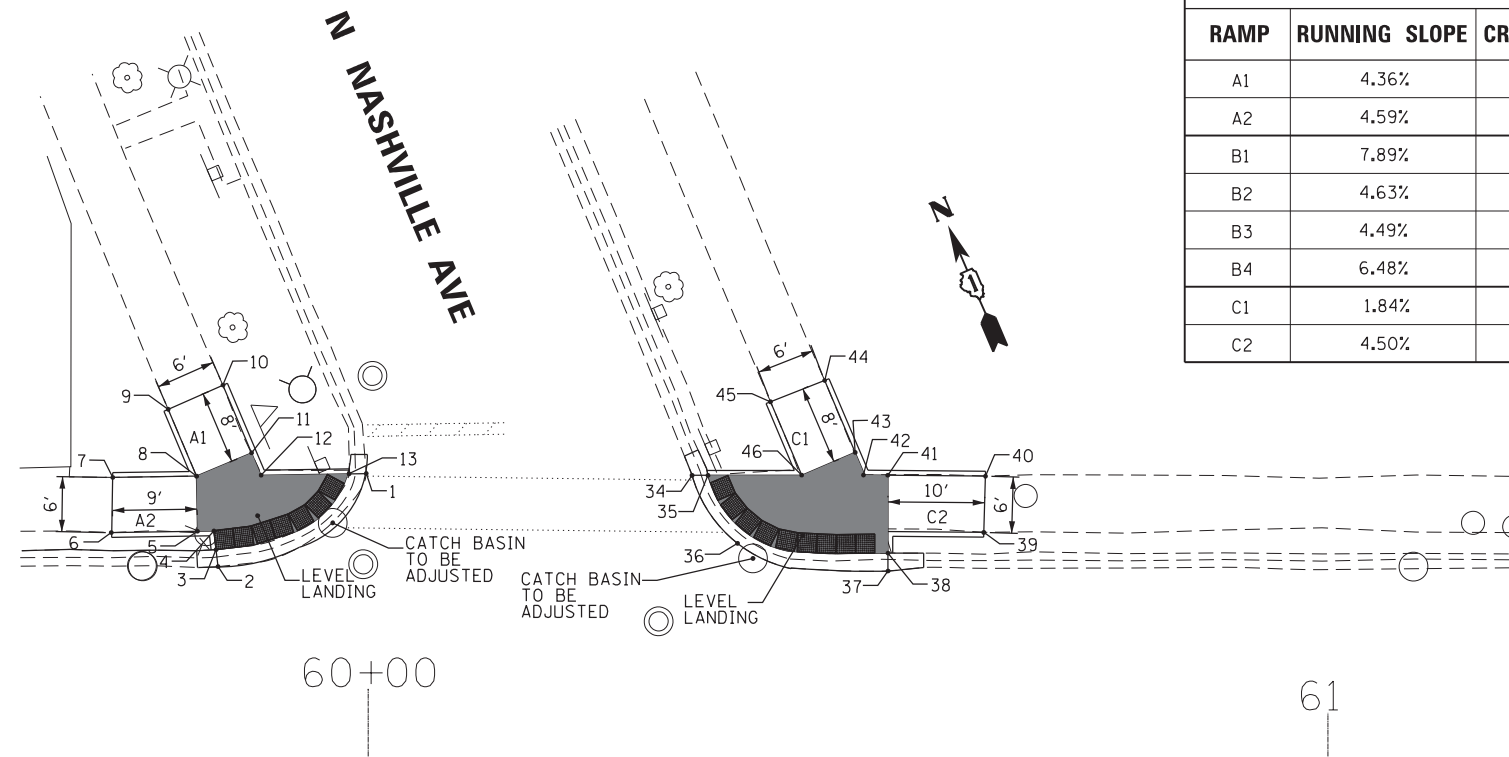


USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DESIGN - HIGGINS & NATOMA AVE
W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE**
SCALE- 1:10 SHEET 12 OF 16 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	20
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				

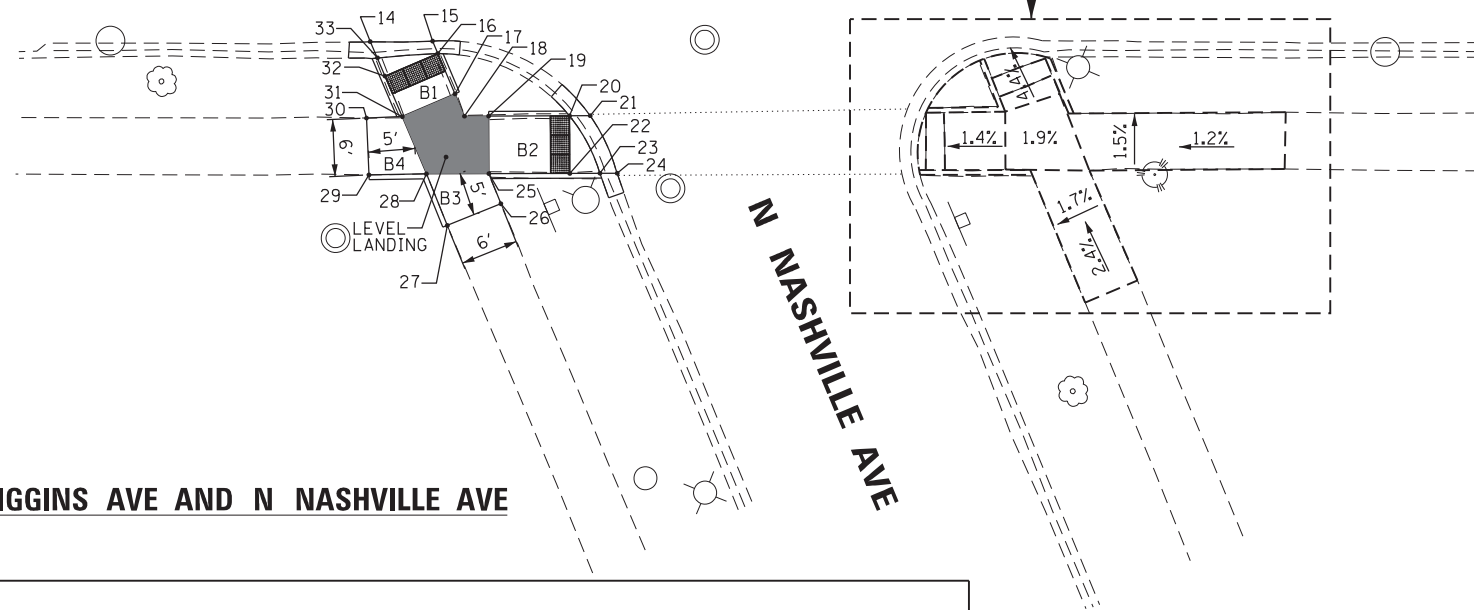


ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	4.36%	1.76%	2.00%
A2	4.59%	1.74%	
B1	7.89%	1.67%	1.67%
B2	4.63%	1.50%	
B3	4.49%	0.31%	
B4	6.48%	1.53%	
C1	1.84%	1.15%	1.72%
C2	4.50%	1.72%	

N NASHVILLE AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	59+99.80	29.95' LT	M.E. (622.22)
2	59+84.35	20.25' LT	M.E. (622.39)
3	59+84.15	22.01' LT	622.38
4	59+83.92	23.96' LT	622.42
5	59+82.21	23.94' LT	622.44
6	59+73.29	23.80' LT	M.E. (622.76)
7	59+73.39	29.54' LT	M.E. (622.95)
8	59+82.12	29.68' LT	622.54
9	59+79.17	36.64' LT	M.E. (622.87)
10	59+84.87	39.16' LT	M.E. (622.71)
11	59+87.86	32.11' LT	622.43
12	59+88.85	29.78' LT	622.39
13	59+98.01	29.92' LT	622.21
14	60+02.61	20.28' RT	M.E. (622.40)
15	60+09.11	20.23' RT	M.E. (622.45)
16	60+09.66	21.53' RT	622.44
17	60+11.44	25.73' RT	622.80
18	60+12.41	28.06' RT	622.80
19	60+14.95	28.05' RT	622.81
20	60+23.38	28.02' RT	622.42
21	60+25.53	28.01' RT	M.E. (622.43)
22	60+23.40	34.02' RT	622.51
23	60+26.54	34.00' RT	622.49
24	60+28.35	34.00' RT	M.E. (622.50)
25	60+14.98	34.05' RT	622.82
26	60+16.22	37.18' RT	M.E. (622.95)
27	60+10.64	39.44' RT	M.E. (623.06)
28	60+08.47	34.07' RT	622.80
29	60+02.48	34.18' RT	M.E. (623.08)
30	60+02.22	28.25' RT	M.E. (622.94)
31	60+05.92	28.08' RT	622.70
32	60+04.13	23.87' RT	622.41
33	60+03.34	22.00' RT	622.39
34	60+33.75	29.76' LT	M.E. (622.37)
35	60+35.41	29.76' LT	622.36
36	60+38.49	22.67' LT	M.E. (622.21)
37	60+54.17	19.79' LT	622.32
38	60+54.16	21.66' LT	622.30
39	60+64.15	23.81' LT	M.E. (622.76)
40	60+64.32	29.67' LT	M.E. (622.80)
41	60+54.14	29.77' LT	622.44
42	60+51.75	29.80' LT	622.45
43	60+50.75	32.15' LT	622.49
44	60+47.56	39.64' LT	M.E. (622.64)
45	60+41.92	37.42' LT	M.E. (622.50)
46	60+45.17	29.78' LT	622.42

W HIGGINS AVE

NEWLY CONSTRUCTED ADA RAMP
AT THIS CORNER



PLAN AT W HIGGINS AVE AND N NASHVILLE AVE

CONTROL & BENCHMARK INFORMATION

POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 60+00	1934851.19	1131560.58	-		
STA 61+00	1934812.07	1131652.61	-		
CP #56	1934766.038	1131560.337	622.874	CUT CROSS IN WALK	SW CORNER, 50' SOUTH OF INTERSECTION
CP #57	1934833.025	1131666.561	622.652	CUT CROSS IN WALK	NE CORNER, 40' EAST OF INTERSECTION
BM #613	1934814.123	1131710.096	622.646	CUT CROSS IN WALK	NE CORNER, 70' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST
ZONE (NAD83)
VERTICAL CONTROL DATUM: NAVD 88



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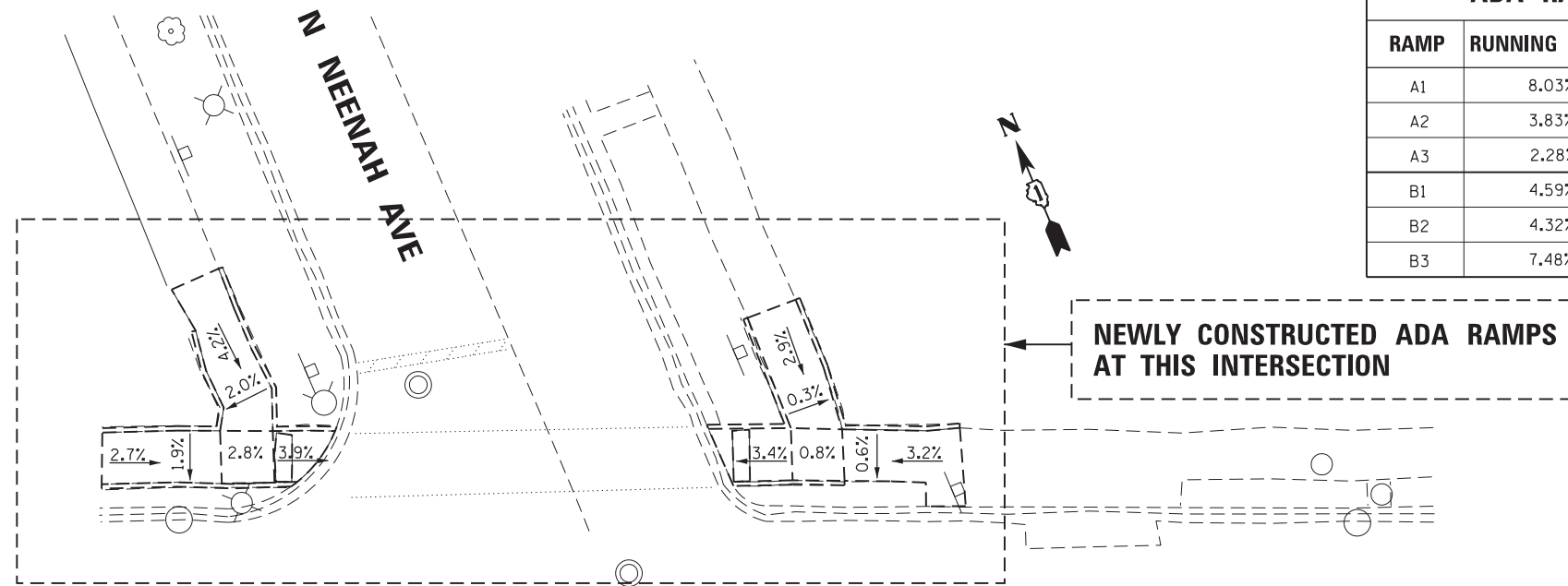
USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DESIGN - HIGGINS & NASHVILLE AVE
W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE

SCALE- 1:10 SHEET 13 OF 16 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	21
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				

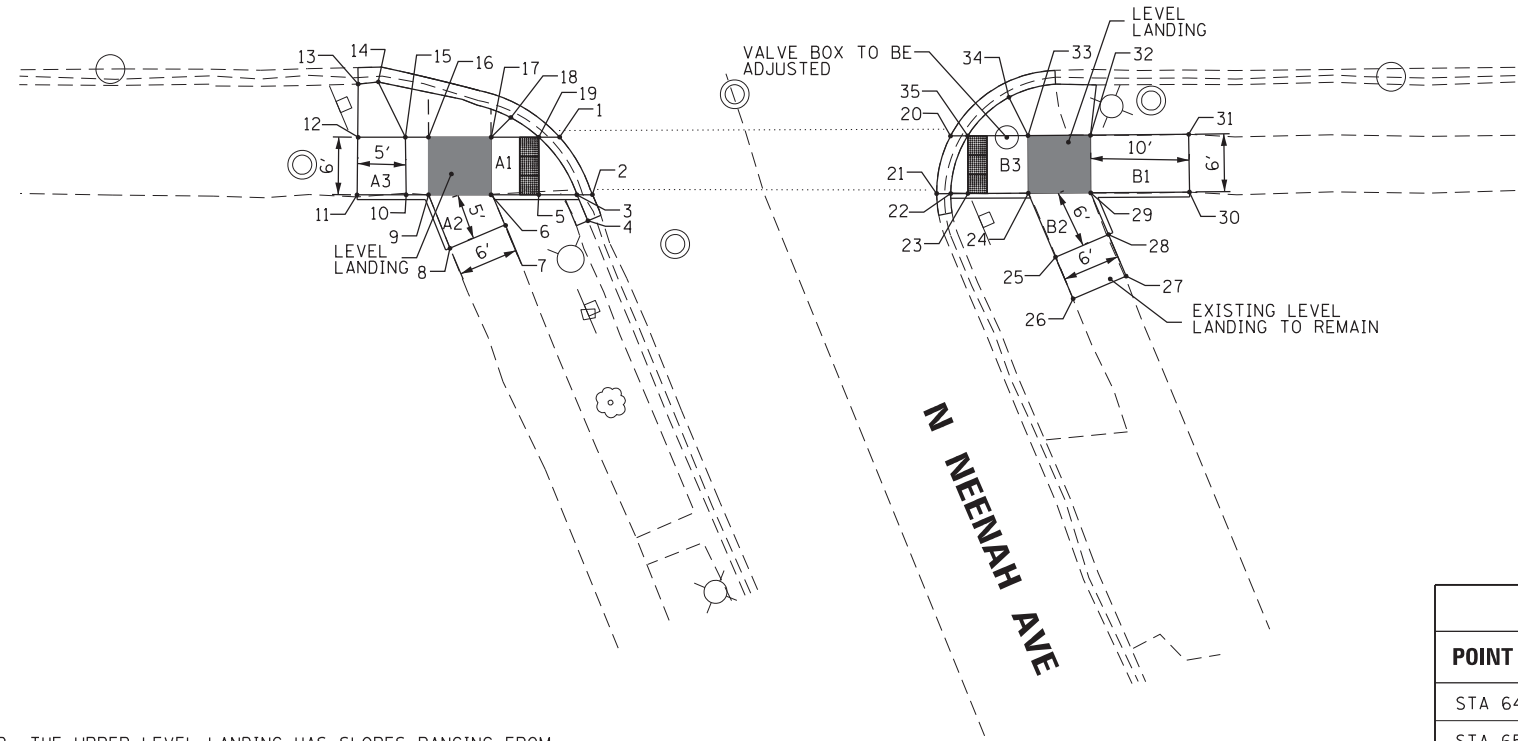


ADA RAMP SLOPE TABLE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	8.03%	0.83%	1.54%
A2	3.83%	1.50%	
A3	2.28%	1.20%	
B1	4.59%	0.00%	1.70%
B2	4.32%	0.83%	
B3	7.48%	1.00%	

N NEENAH AVE ADA RAMP ELEVATION TABLE			
POINT NO.:	STATION	OFFSET	ELEVATION
1	63+78.65	27.63' RT	M.E. (622.02)
2	63+82.07	33.63' RT	M.E. (622.02)
3	63+80.45	33.63' RT	622.01
4	63+81.60	36.35' RT	M.E. (622.18)
5	63+76.49	33.63' RT	622.06
6	63+71.51	33.64' RT	622.42
7	63+72.94	36.78' RT	M.E. (622.48)
8	63+67.23	39.21' RT	M.E. (622.75)
9	63+64.99	33.64' RT	622.52
10	63+62.66	33.64' RT	622.71
11	63+57.54	33.65' RT	M.E. (622.69)
12	63+57.66	27.65' RT	M.E. (622.47)
13	63+57.66	22.10' RT	M.E. (622.45)
14	63+59.76	21.87' RT	M.E. (622.29)
15	63+62.57	27.64' RT	622.47
16	63+64.98	27.64' RT	622.45
17	63+71.52	27.64' RT	622.40
18	63+73.57	25.58' RT	M.E. (622.39)
19	63+76.48	27.63' RT	622.01
20	64+19.38	27.51' RT	M.E. (621.74)
21	64+17.93	33.52' RT	M.E. (621.77)
22	64+19.40	33.51' RT	621.76
23	64+21.20	33.50' RT	621.76
24	64+27.48	33.47' RT	622.25
25	64+30.31	40.13' RT	M.E. (622.39)
26	64+32.15	44.47' RT	M.E. (622.40)
27	64+37.68	42.12' RT	M.E. (622.56)
28	64+35.84	37.79' RT	622.50
29	64+33.99	33.45' RT	622.30
30	64+44.28	33.35' RT	M.E. (622.77)
31	64+44.20	27.34' RT	M.E. (622.55)
32	64+33.96	27.45' RT	622.30
33	64+27.46	27.47' RT	622.19
34	64+25.47	23.48' RT	621.70
35	64+21.17	27.50' RT	621.73

63 _____ 64

W HIGGINS AVE



1. NORTHWEST CORNER THE UPPER LEVEL LANDING HAS SLOPES RANGING FROM 2.2% TO 2.8% WHICH ARE GREATER THAN THE MAXIMUM > 2% PER IDOT STANDARD 424001.

PLAN AT W HIGGINS AVE AND N NEENAH AVE

CONTROL & BENCHMARK INFORMATION					
POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 64+00	1934694.73	1131928.71	-		
STA 65+00	1934655.62	1132020.74	-		
CP #58	1934761.8	1131836.045	622.628	CUT CROSS IN WALK	NW CORNER, 50' WEST OF INTERSECTION
CP #59	1934635.94	1131995.811	622.572	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION
BM #614	1934696.813	1131989.878	622.316	CUT CROSS IN WALK	NE CORNER, 40' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
 VERTICAL CONTROL DATUM: NAVD 88



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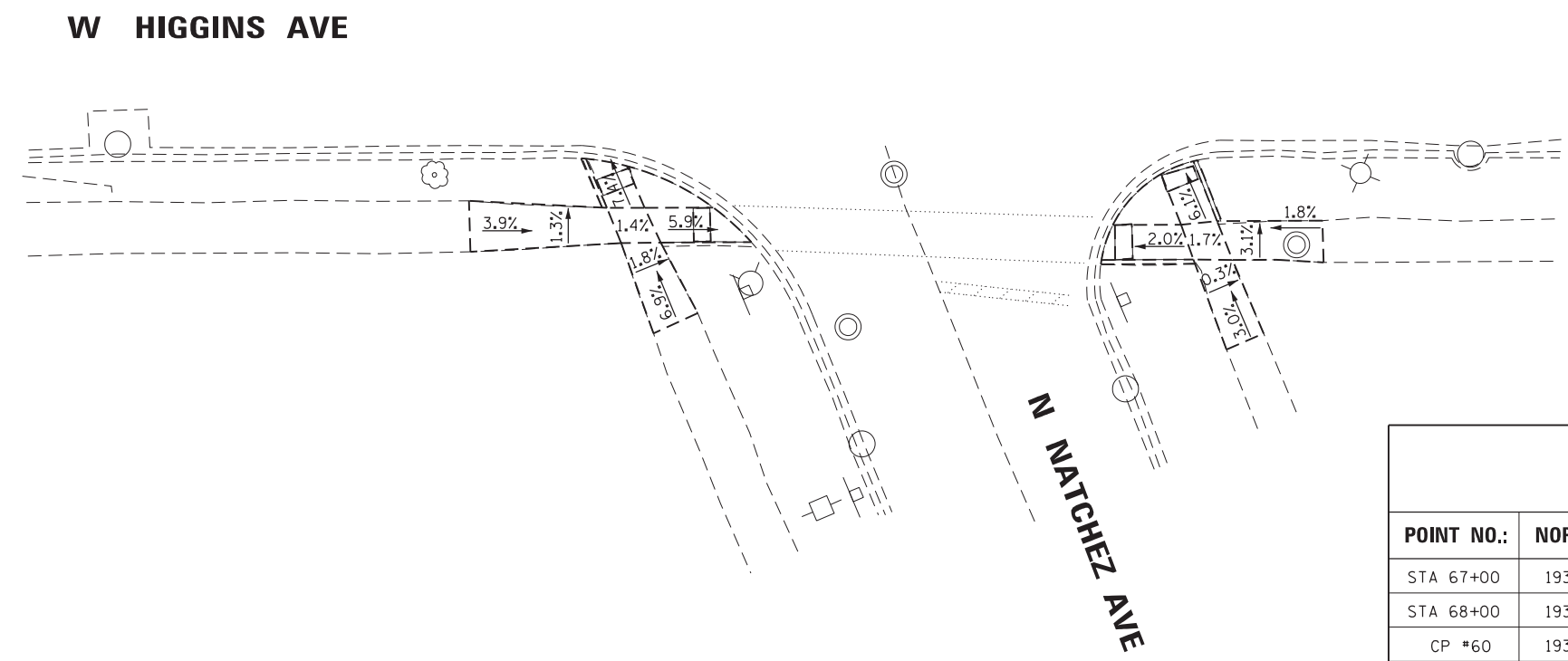
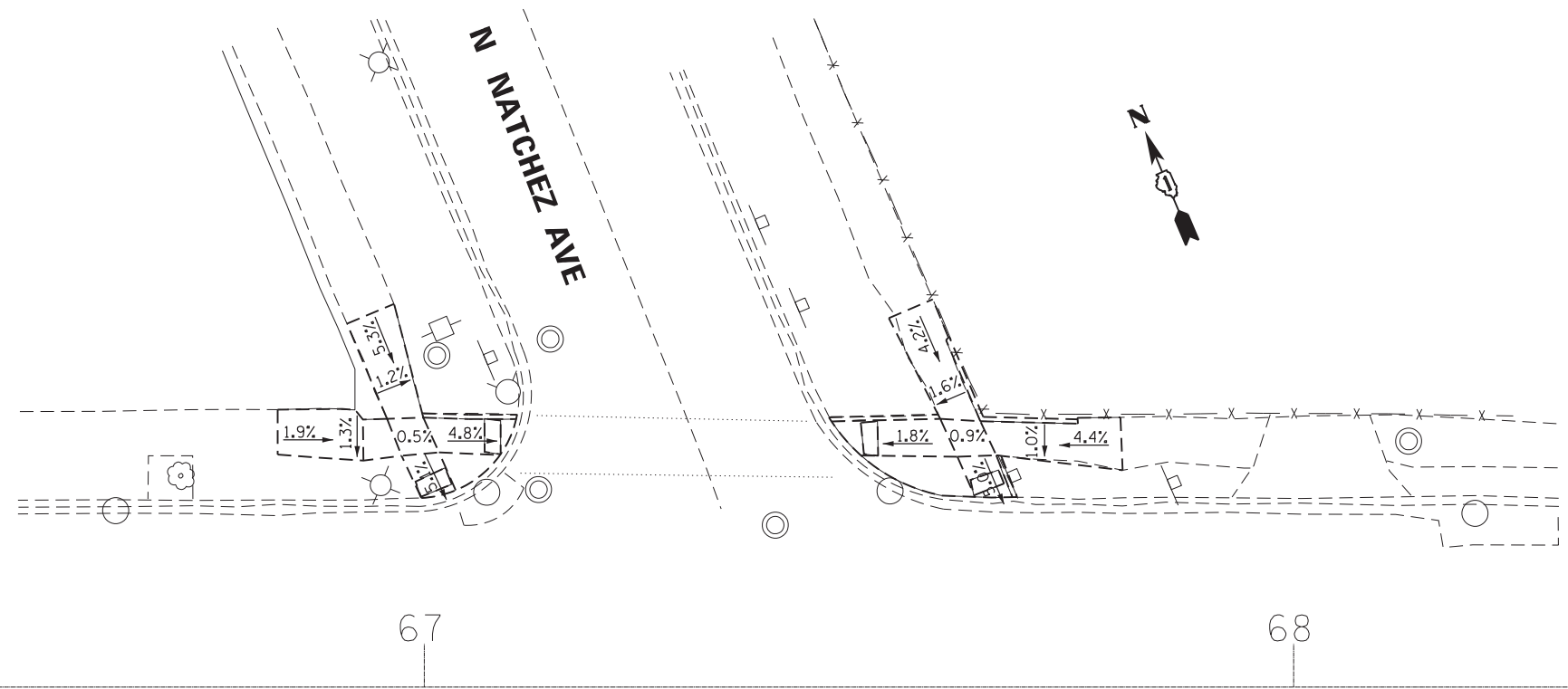


USER NAME = awmohammed	DESIGNED - JA	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - TOM	REVISED -
PLOT DATE = 10/26/2016	CHECKED - JA	REVISED -
	DATE - 10/26/2016	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DESIGN - HIGGINS & NEENAH AVE
 W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE**
 SCALE- 1:10 SHEET 14 OF 16 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	22
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



**NEWLY CONSTRUCTED ADA RAMP
AT THIS INTERSECTION**

CONTROL & BENCHMARK INFORMATION					
POINT NO.:	NORTHING	EASTING	ELEVATION	DESCRIPTION	LOCATION
STA 67+00	1934577.39	1132204.81	-		
STA 68+00	1934538.27	1132296.84	-		
CP #60	1934564.292	1132165.572	622.35	CUT CROSS IN WALK	SW CORNER, 50' WEST OF INTERSECTION
CP #61	1934633.066	1132271.243	622.315	CUT CROSS IN WALK	NE CORNER, 50' NORTH OF INTERSECTION
BM #615	1934495.415	1132323.534	622.46	CUT CROSS IN WALK	SE CORNER, 50' EAST OF INTERSECTION

HORIZONTAL CONTROL: ILLINOIS STATE PLANE EAST ZONE (NAD83)
VERTICAL CONTROL DATUM: NAVD 88

PLAN AT W HIGGINS AVE AND N NATCHEZ AVE



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AES Services, Inc. 111 S. Wacker Drive, Suite 3910 Chicago, IL 60606 Ph: 312-235-6783	USER NAME = awmohammed DESIGNED - JA DRAWN - TOM CHECKED - JA DATE - 10/26/2016	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA RAMP DESIGN - HIGGINS & N NATCHEZ AVE W HIGGINS AVE FROM N NEVA AVE TO N NATCHEZ AVE	F.A.U. RTE. 3531 SECTION 63Z-IRS-3 COUNTY COOK TOTAL SHEETS 36 SHEET NO. 23	CONTRACT NO. 62D22 ILLINOIS FED. AID PROJECT
	PLOT SCALE = 20.0000' / in. PLOT DATE = 10/26/2016	SCALE- 1:10 SHEET 15 OF 16 SHEETS STA. TO STA.				

ADA RAMP SLOPE TABLE HIGGINS AT NOTTINGHAM AVE				
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING	UPPER LANDING
A1	4.33%	1.21%	1.64%	NONE
A2	3.30%	1.80%		
A3	4.08%	1.80%		
A4	5.53%	1.16%		
B1	7.71%	2.00%	1.71%	NONE
B2	8.11%	0.75%		
B3	2.05%	1.20%		
B4	2.44%	1.70%		
C1	4.28%	1.94%	1.94%	1.87%
C2	4.93%	0.15%		
D1	5.80%	0.85%	1.27%	NONE
D2	2.91%	0.88%		
D3	7.13%	0.85%		
D4	4.80%	1.50%		

ADA RAMP SLOPE TABLE HIGGINS AT N NATOMA AVE			
RAMP	RUNNING SLOPE	CROSS SLOPE	LEVEL LANDING
A1	4.45%	2.00%	2.00%
A2	4.45%	0.13%	
B1	3.57%	1.80%	1.56%
B2	8.20%	0.00%	
B3	2.07%	1.38%	
B4	3.65%	0.00%	
C1	8.01%	1.83%	1.67%
C2	2.90%	1.26%	
C3	4.66%	0.53%	
C4	5.18%	0.50%	
D1	4.10%	1.93%	1.93%
D2	4.43%	1.93%	
D3	7.92%	0.00%	
D4	6.34%	1.25%	

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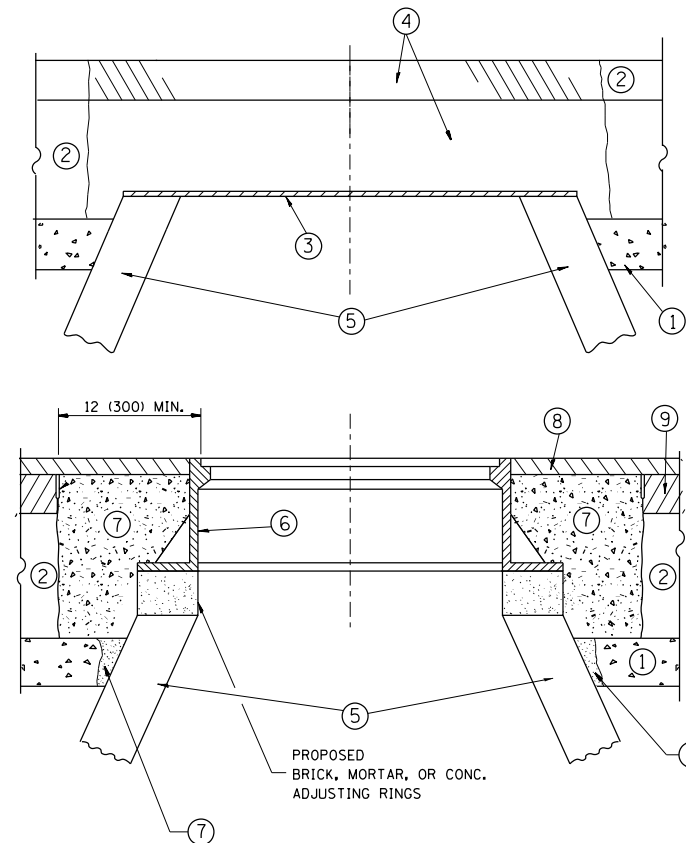
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PLOT DATE = 10/26/2016	DATE - 10/26/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP SLOPES

SCALE- 1:10 SHEET 16 OF 16 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	24
CONTRACT NO. 62D22				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

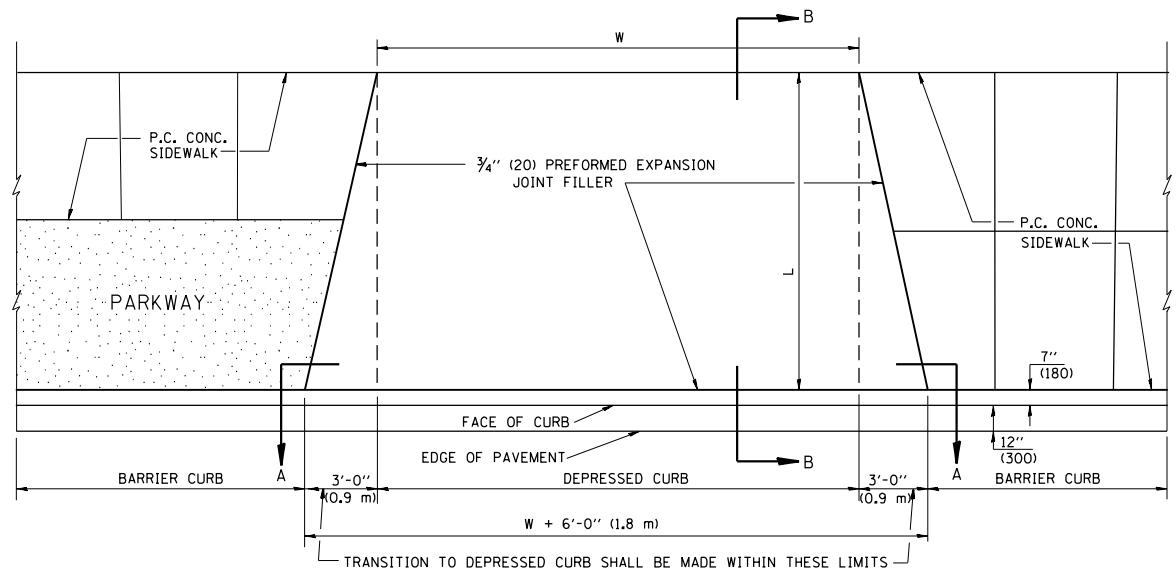
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = guillaumefp	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT DATE = 10/28/2016	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

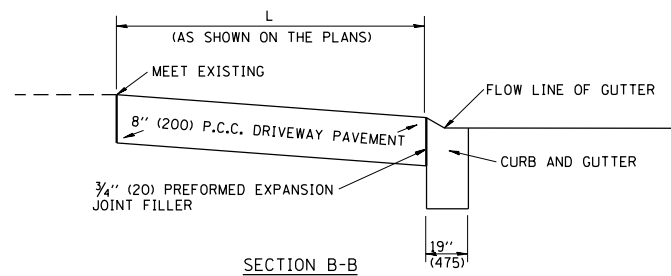
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-1RS-3	COOK	36	25
BD600-03 (BD-8)		CONTRACT NO. 62D22		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



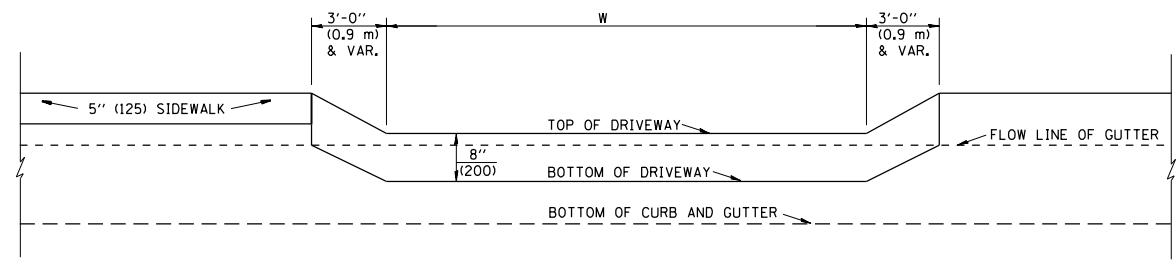
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

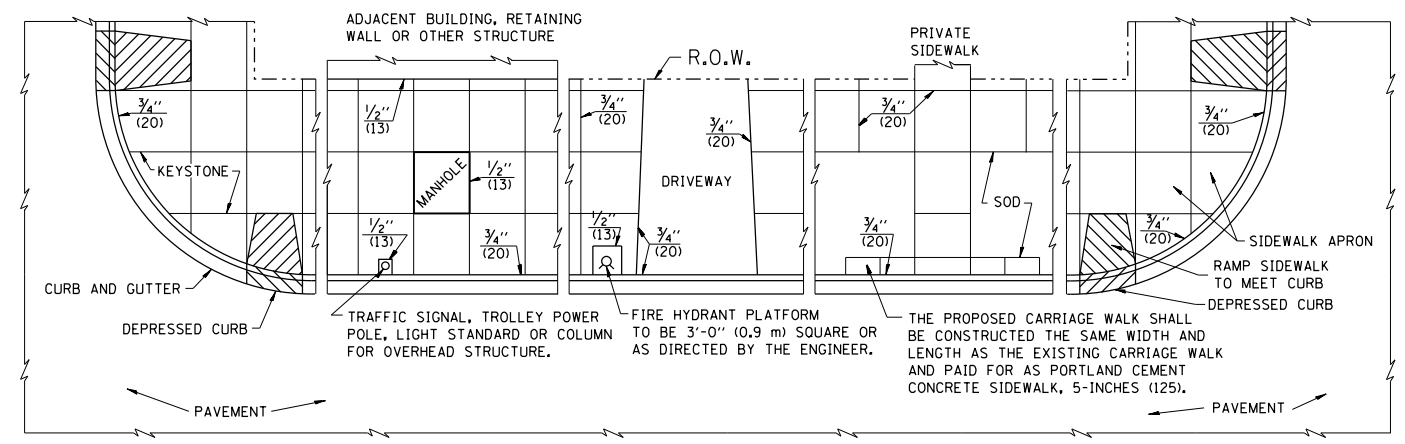


SECTION B-B



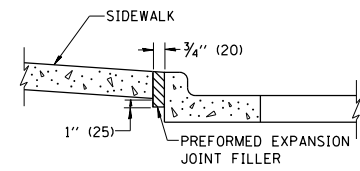
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

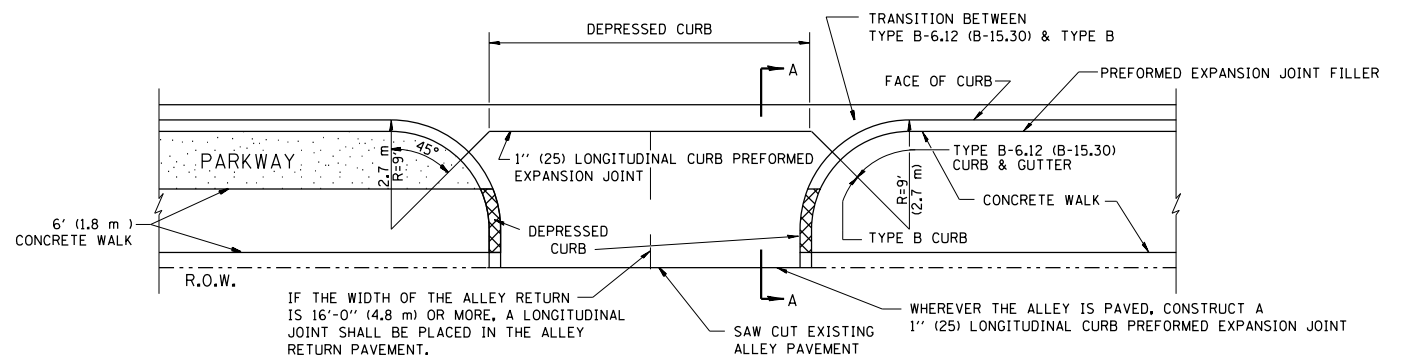
1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK, WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

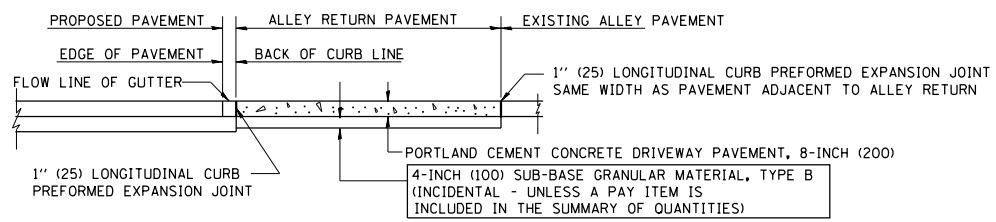
PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



SECTION A-A

ALLEY RETURN DETAIL



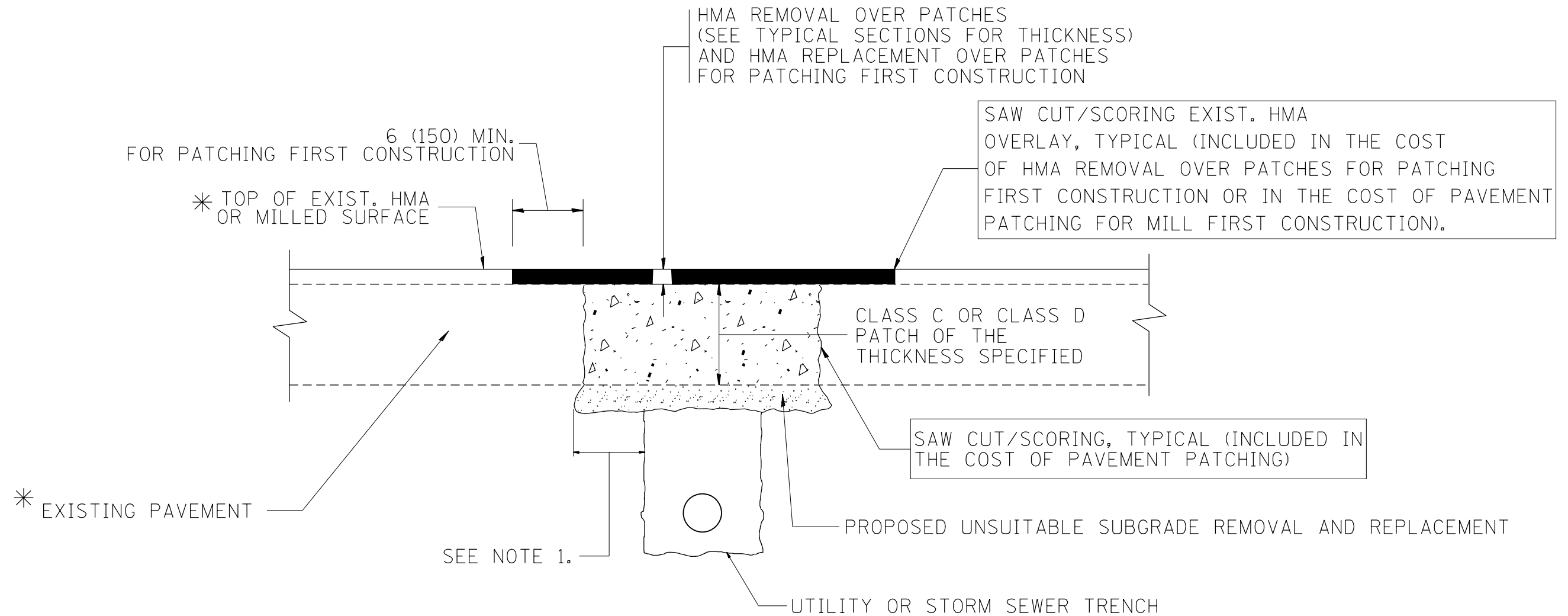
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		DATE - 06-13-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO	
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	26
BD400-03 (BD-17)		CONTRACT NO. 62D22		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

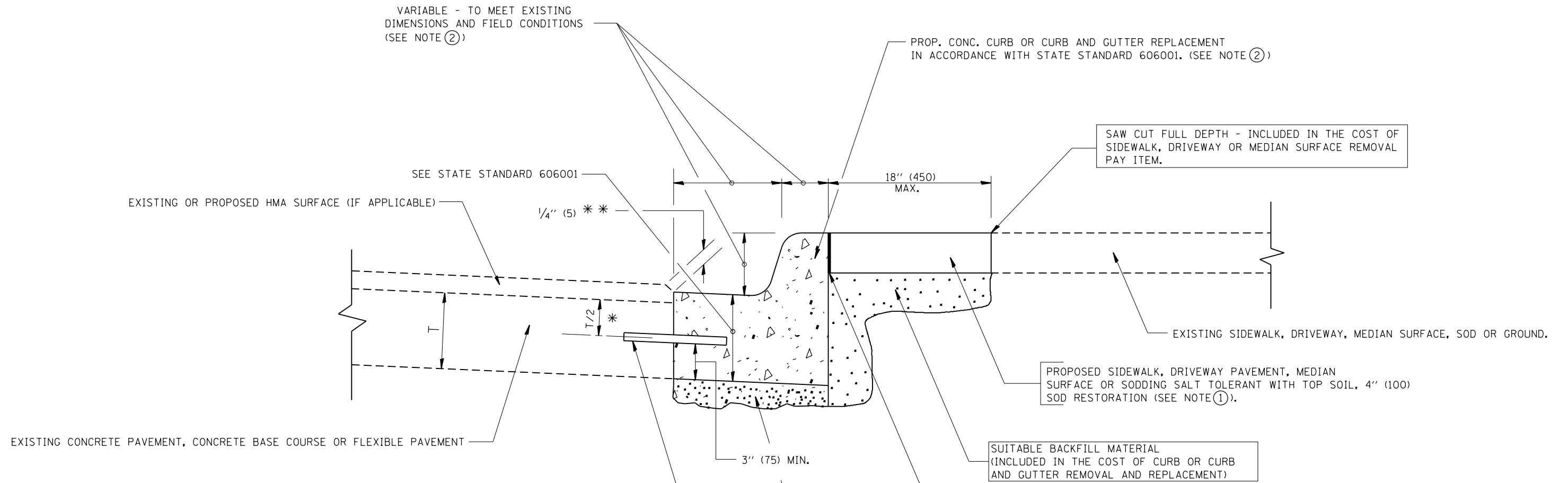
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = guillaumejp	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI445\BROWNS\Design\Diststd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07					3531	63Z-1RS-3	COOK	36	27
	PLOT DATE = 10/28/2016	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62022				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
 - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

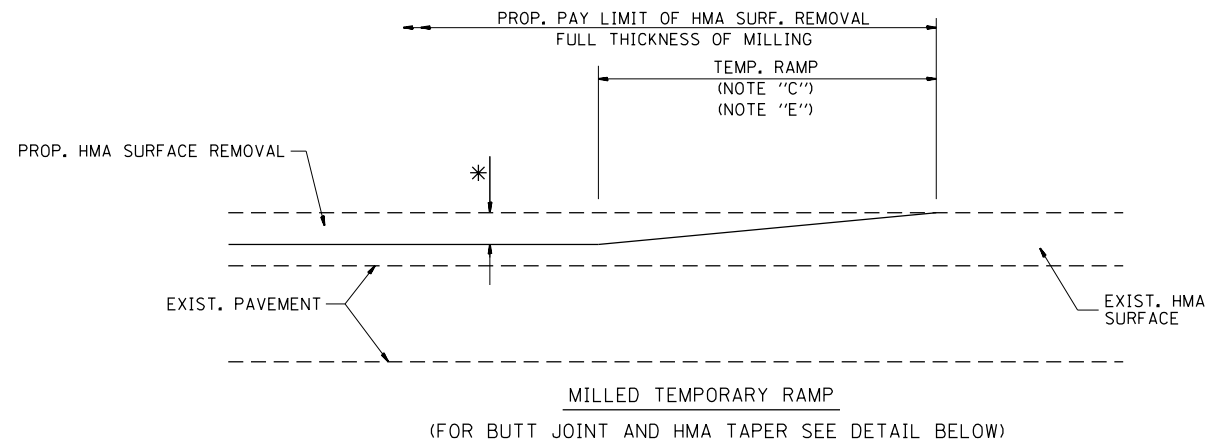
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

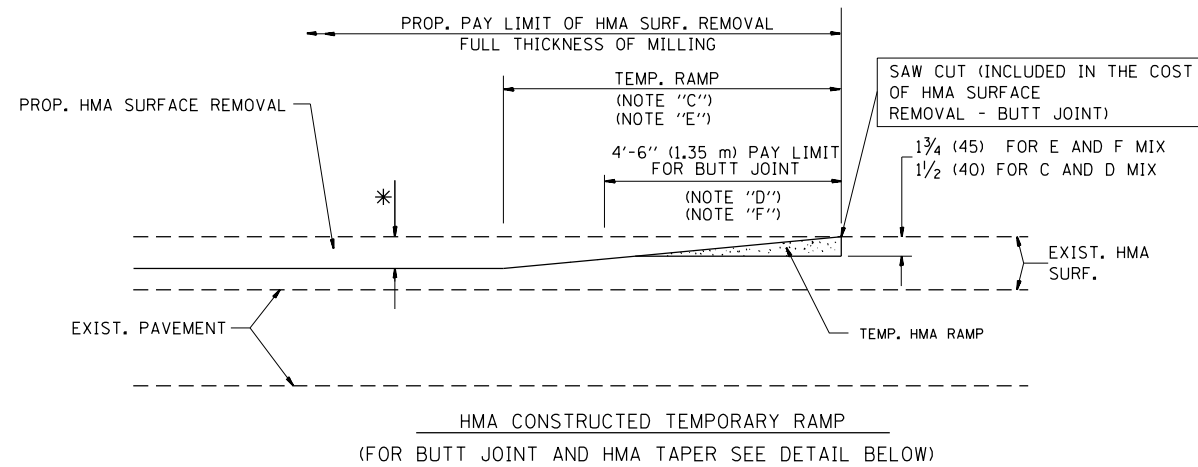
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = guillaumejp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			3531	63Z-1RS-3	COOK	36	28	
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 62022			
	PLOT DATE = 10/28/2016	DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

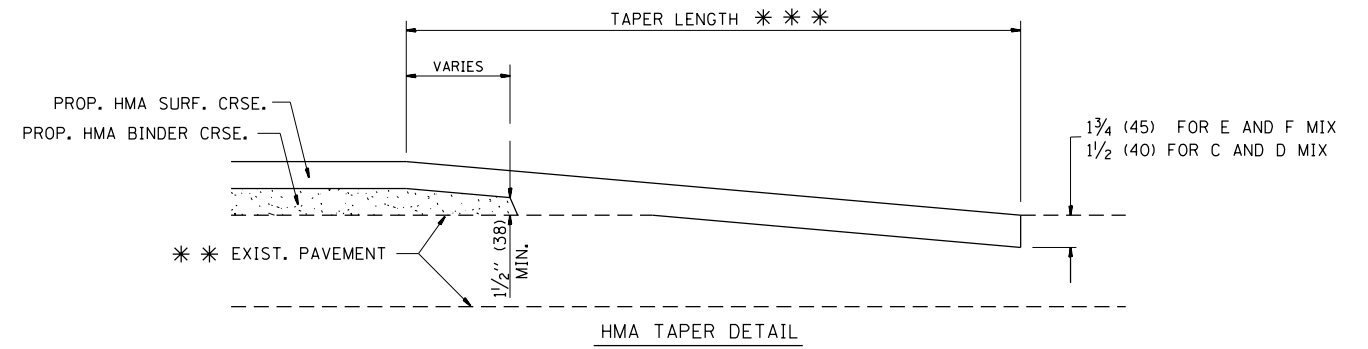
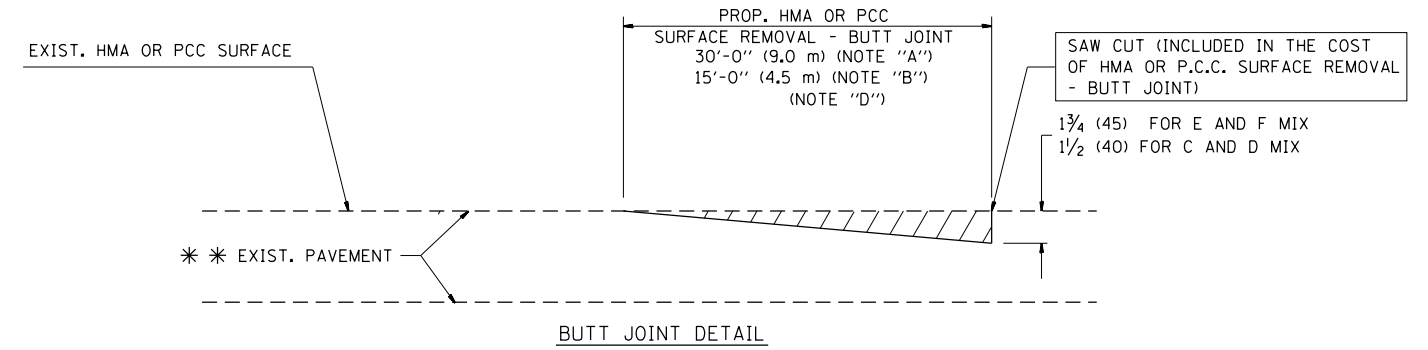


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

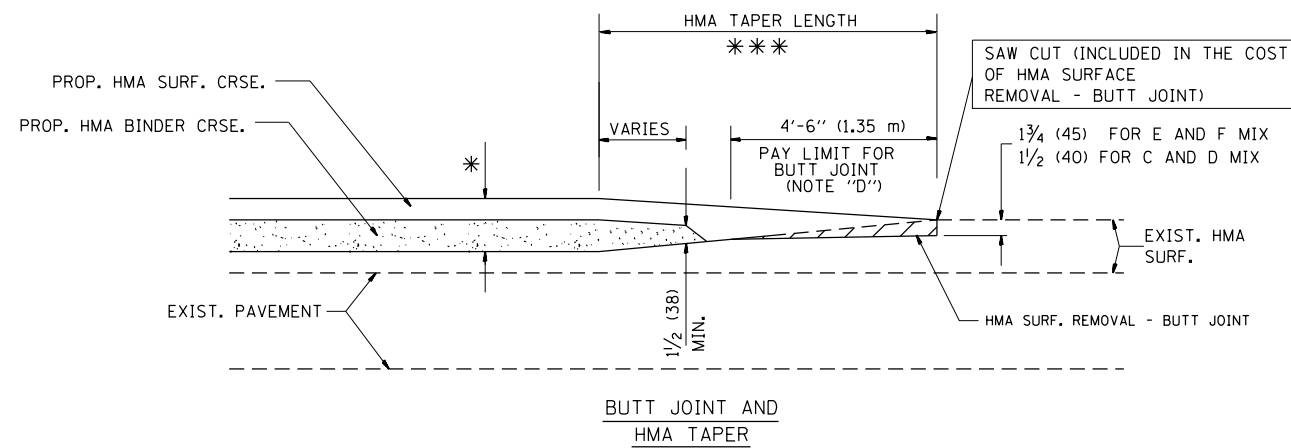
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



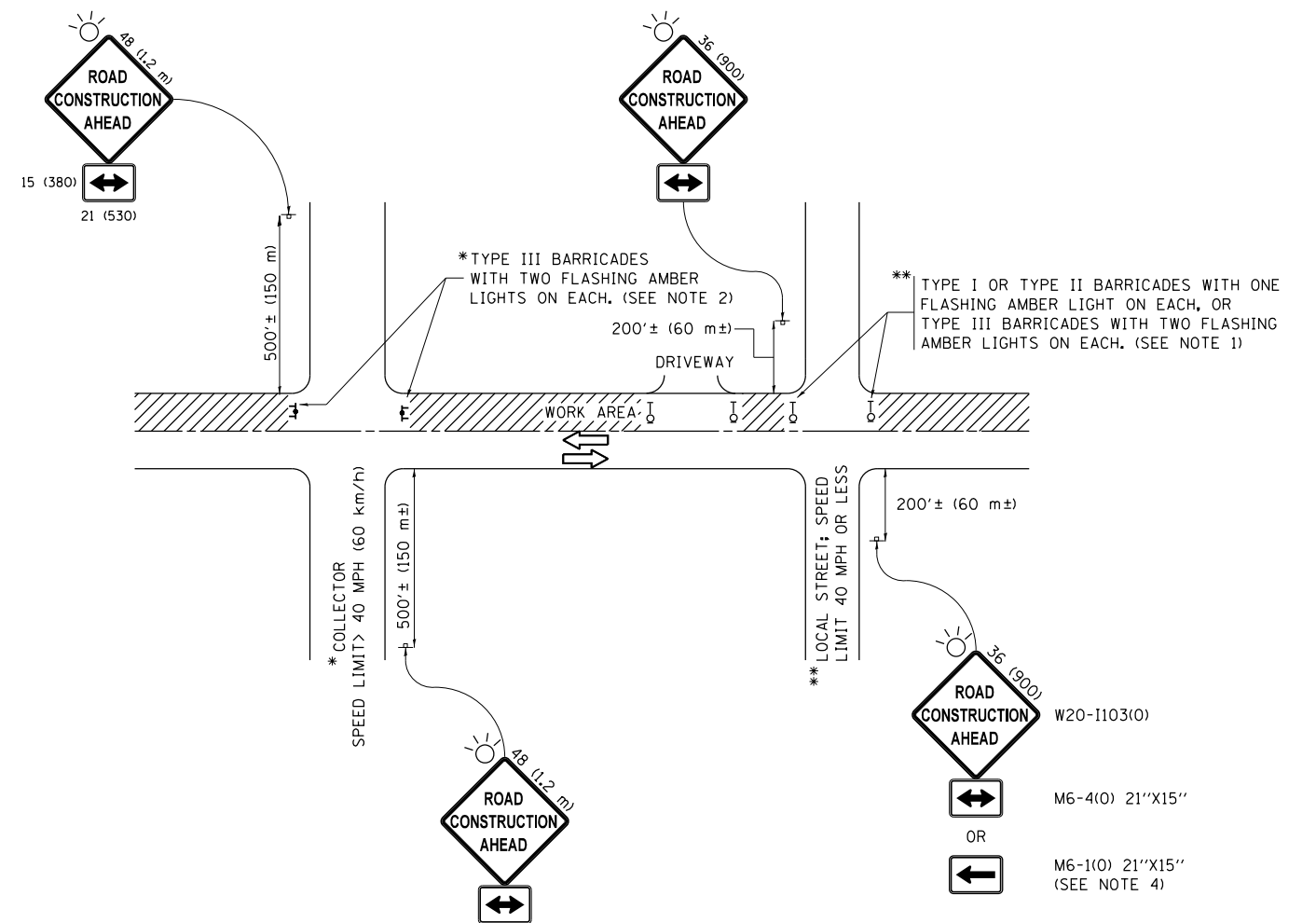
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME =	USER NAME = guillaumejp	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 10/28/2016	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-1RS-3	COOK	36	29
BD400-05 BD32		CONTRACT NO. 62D22		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = guillaumejp	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pw:\IL\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\0145\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
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	PLOT DATE = 10/28/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-1RS-3	COOK	36	30
TC-10			CONTRACT NO. 62022	
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

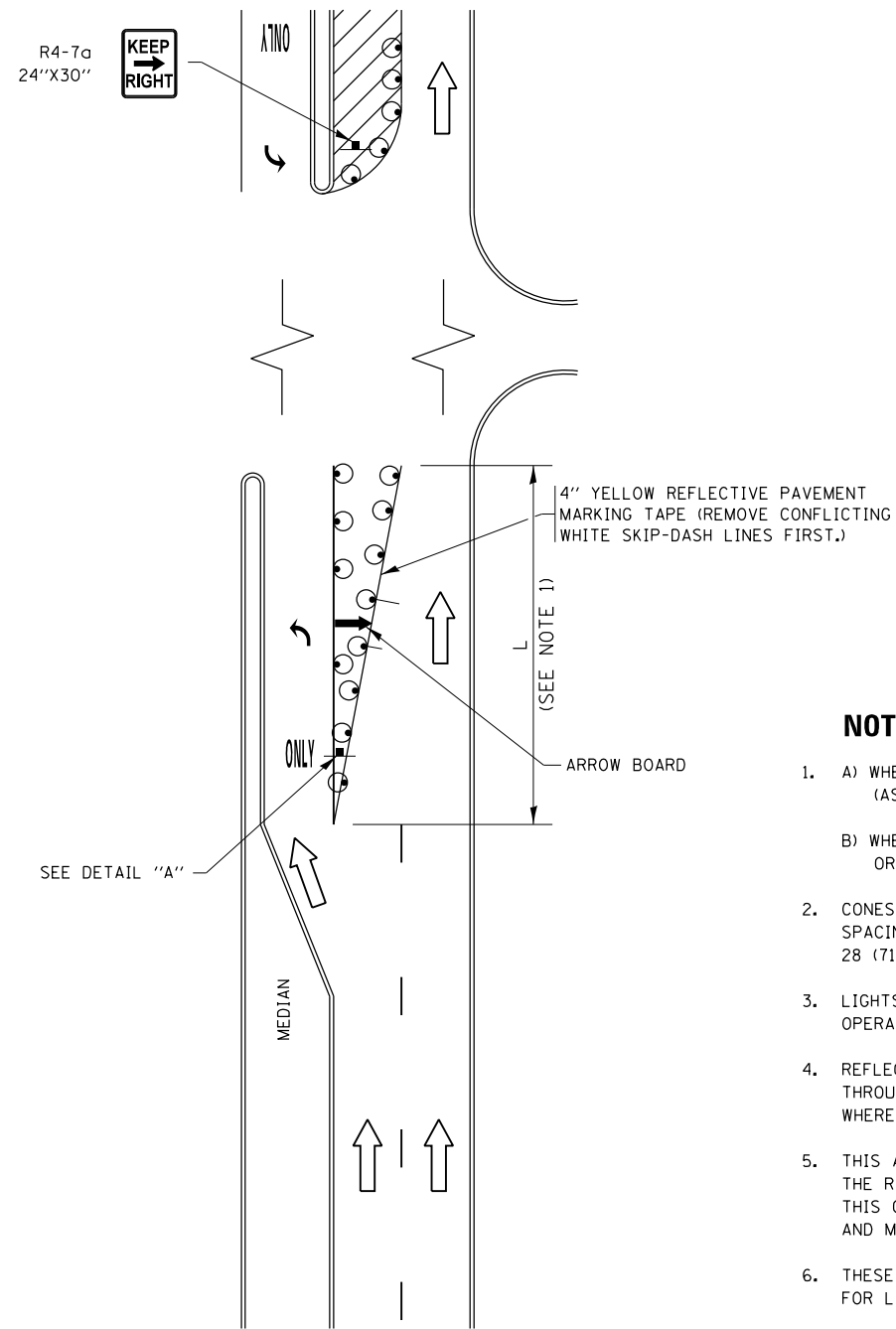


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

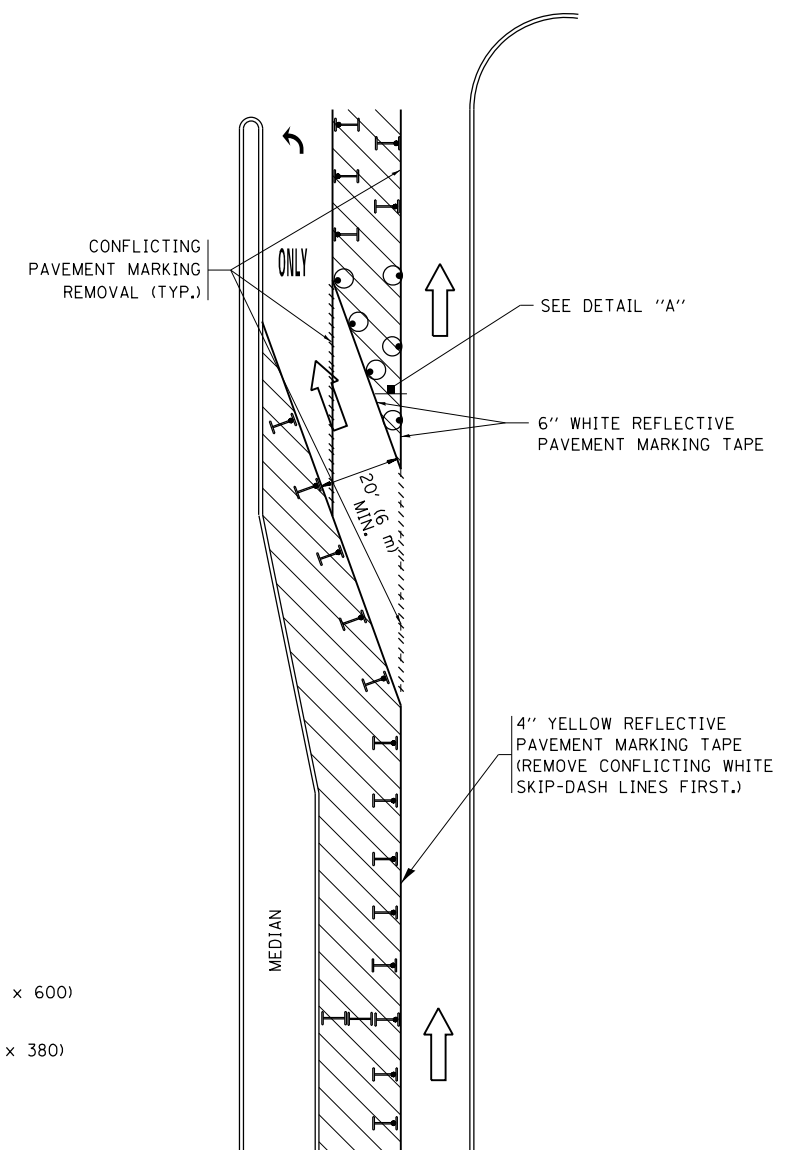
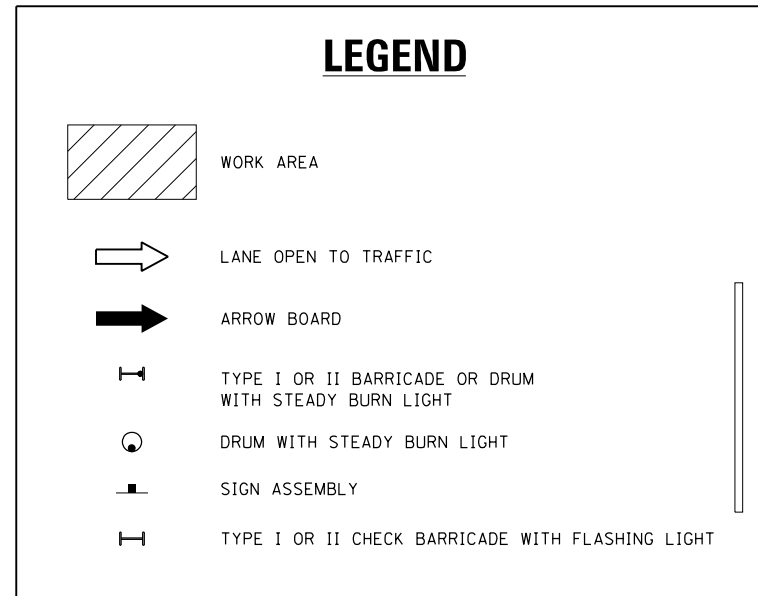
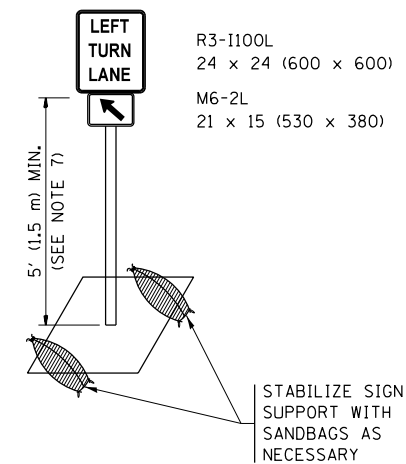


FIGURE 2



NOTES:

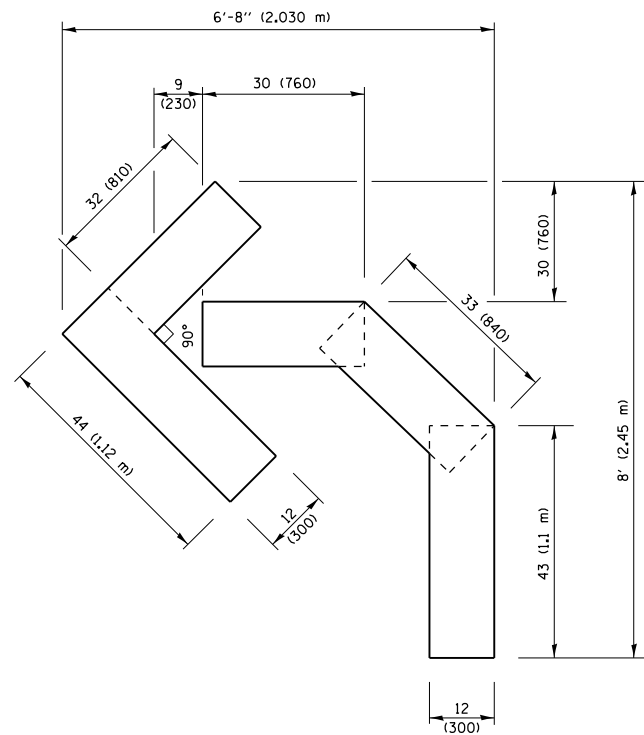
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



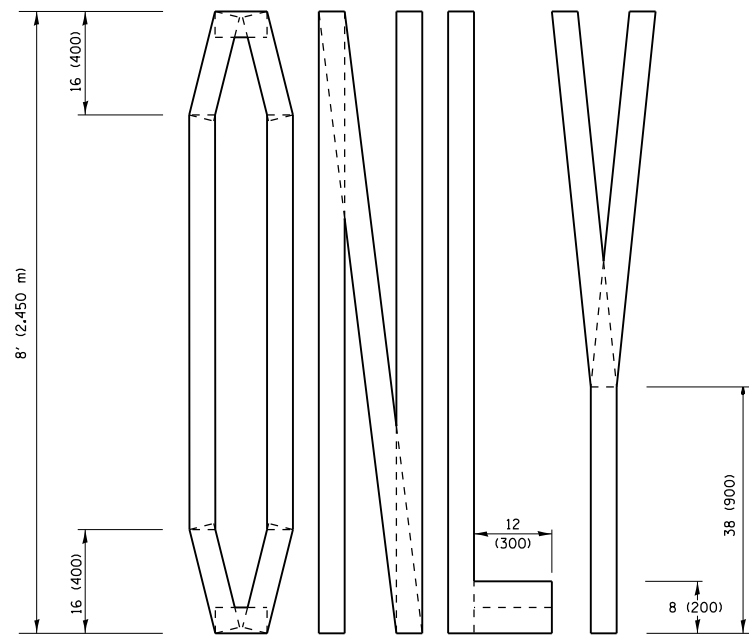
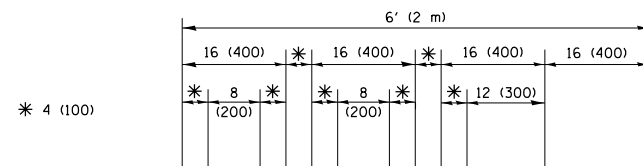
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

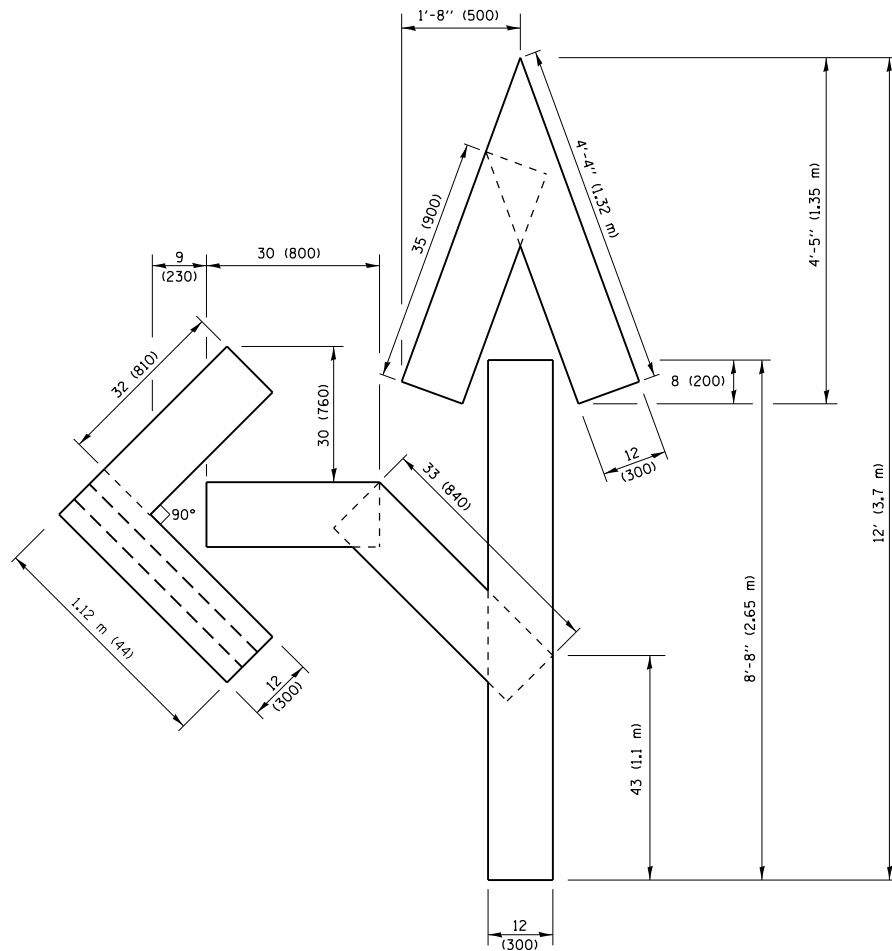
FILE NAME =	USER NAME = guillaumejp	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13					3531	63Z-IRS-3	COOK	36	31
	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO. 62D22				
	PLOT DATE = 10/28/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)



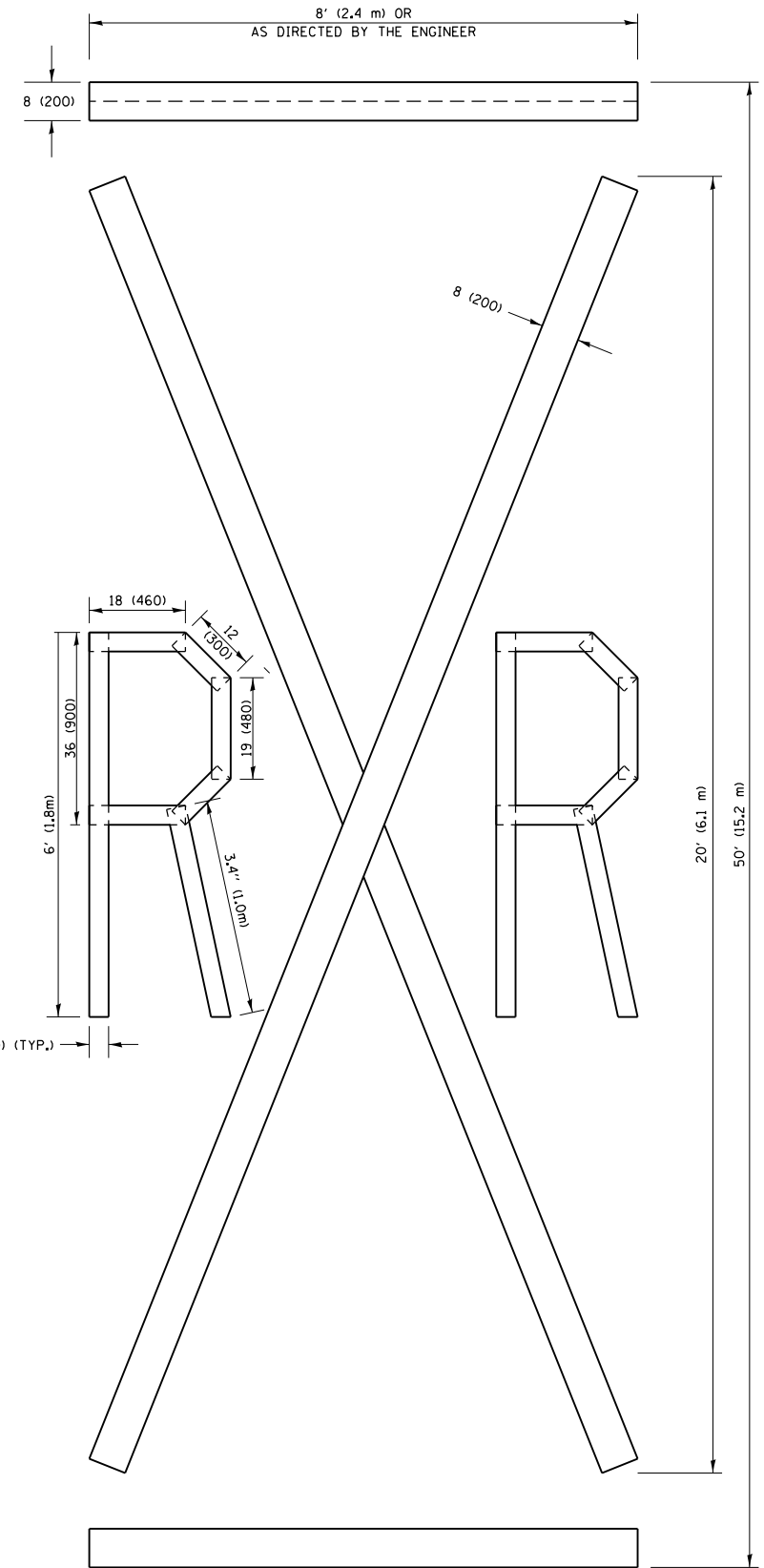
QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

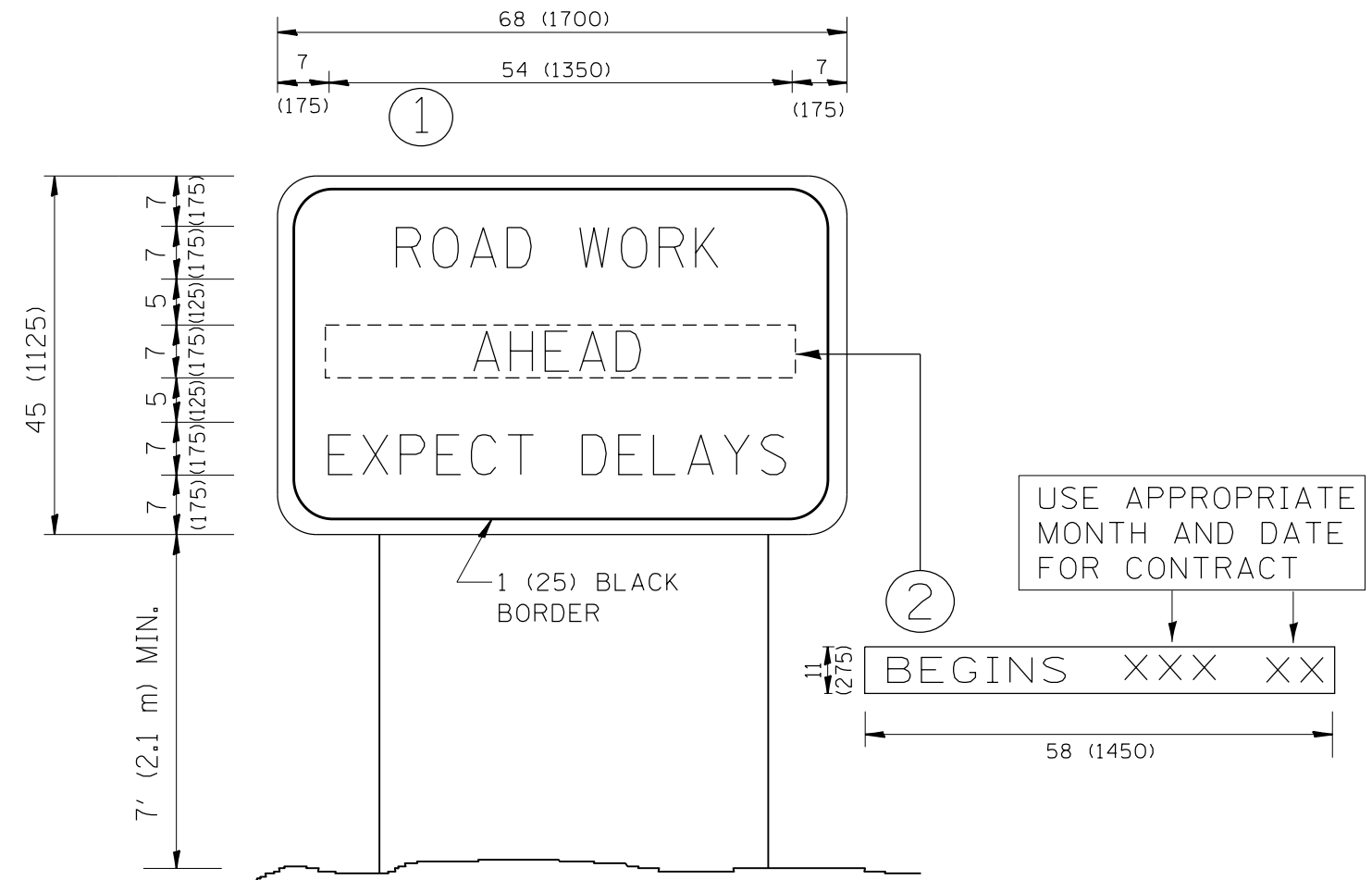
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pw:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\0145\BROWNotes\Design\Diststd.dgn			REVISED - E. GOMEZ 08-28-00
		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-IRS-3	COOK	36	32
TC-16		CONTRACT NO. 62D22		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

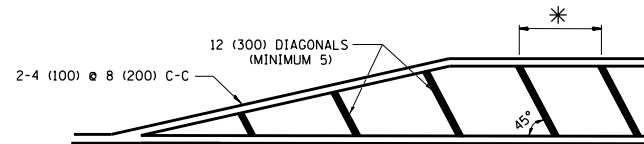
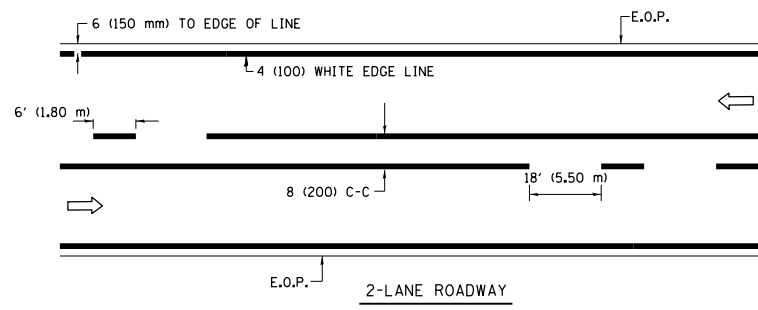


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

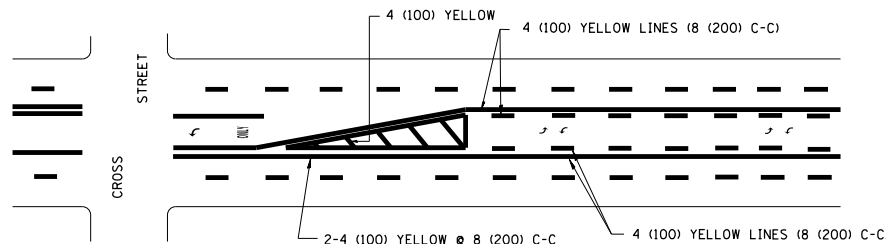
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p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1445\Drawings\Design\Diststd.dgn		REVISOR	REVISED - R. MIRS 12-11-97			3531	63Z-IRS-3	COOK	36	33
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				TC-22		CONTRACT NO. 62D22		
PLOT DATE = 10/28/2016	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>	



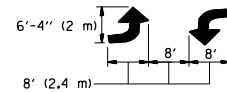
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

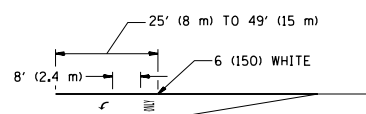
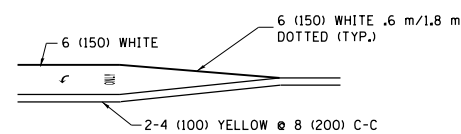


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

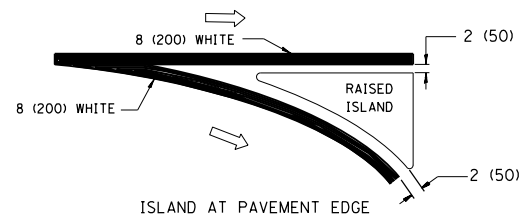
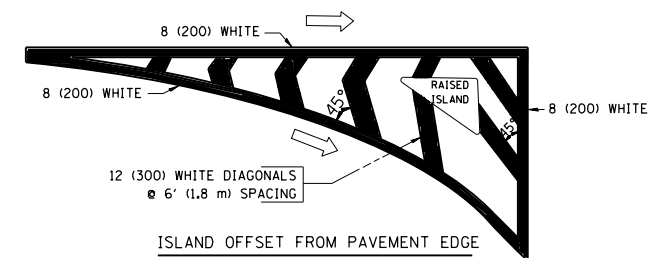


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

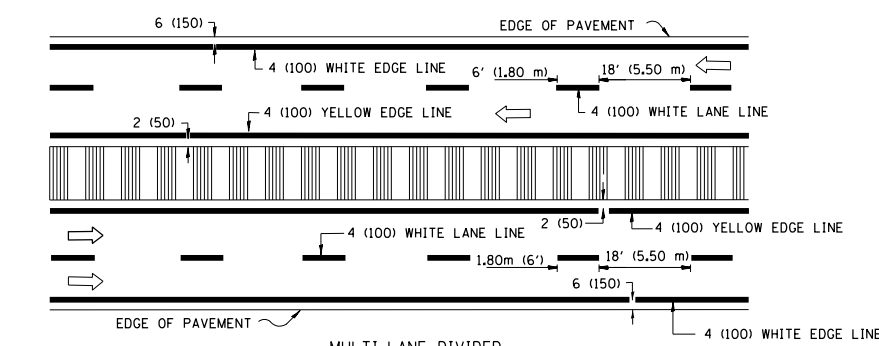
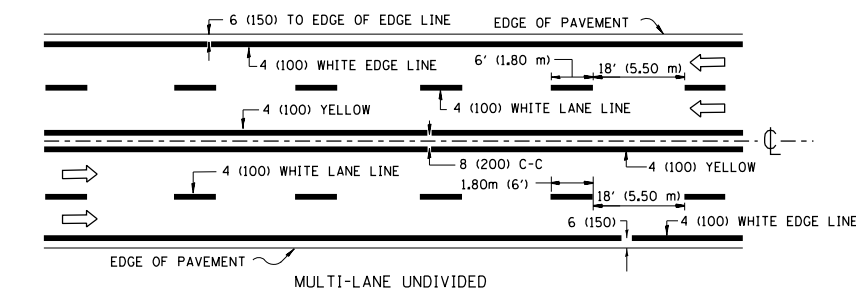


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4 m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

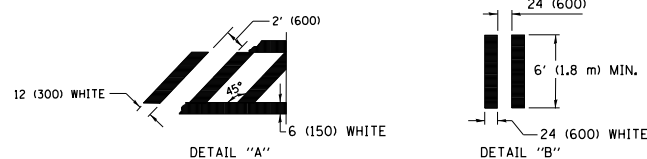
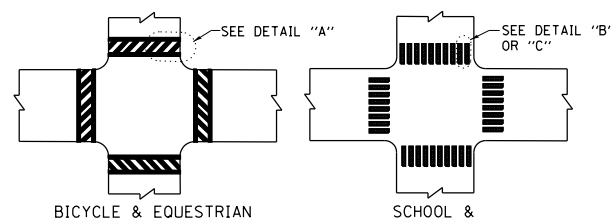
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

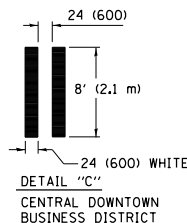


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



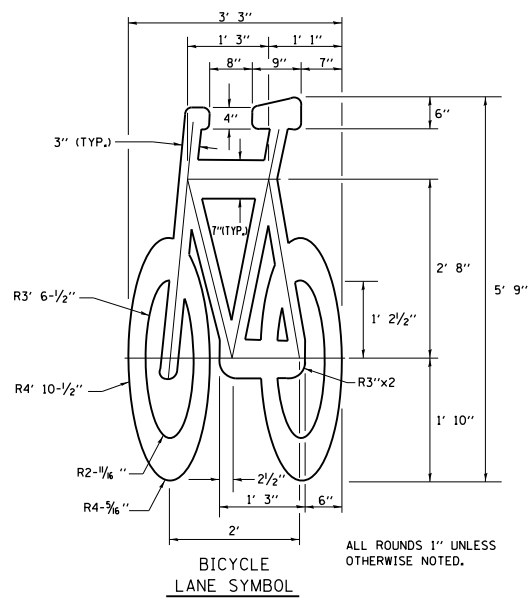
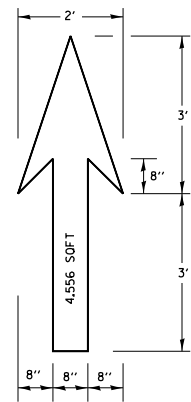
**DETAIL "C"
CENTRAL DOWNTOWN BUSINESS DISTRICT**

FILE NAME =	USER NAME = guillaumejp	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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		DATE -	REVISED -
			REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

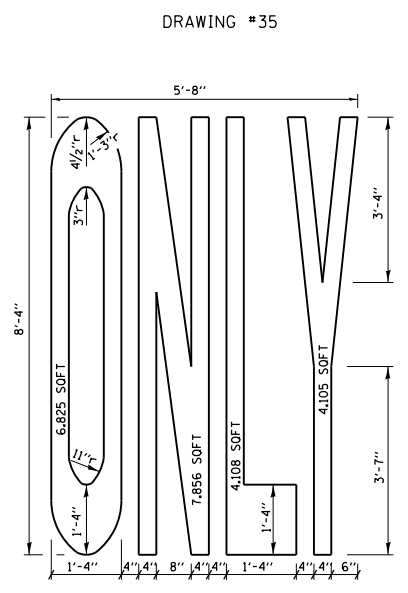
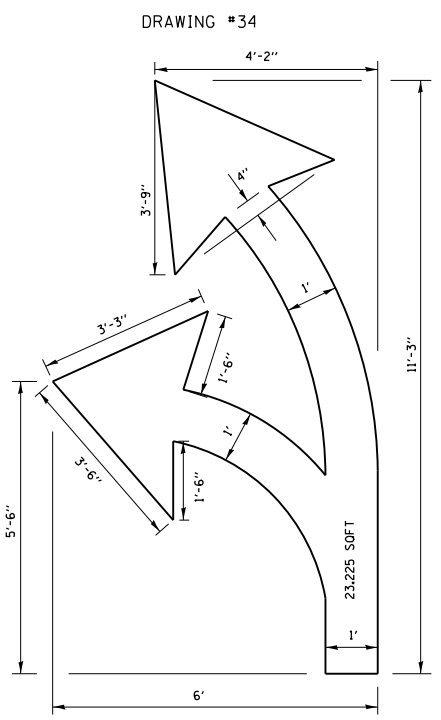
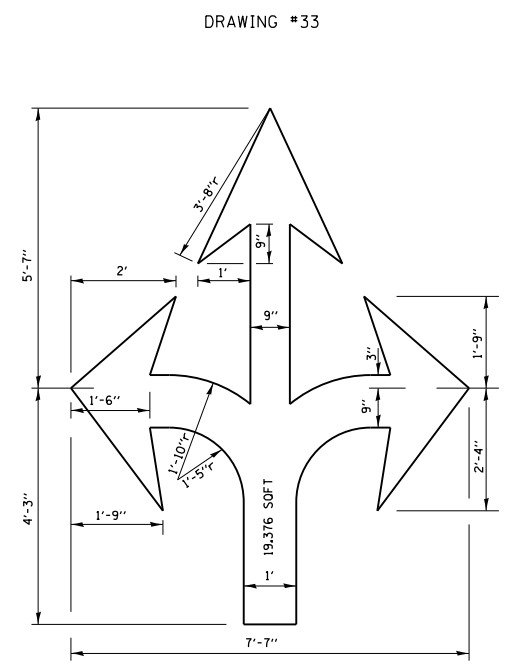
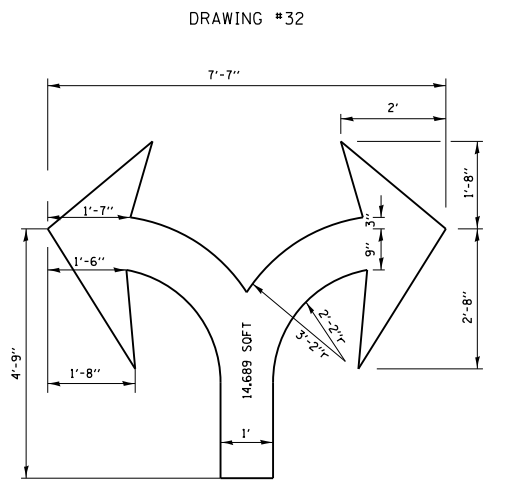
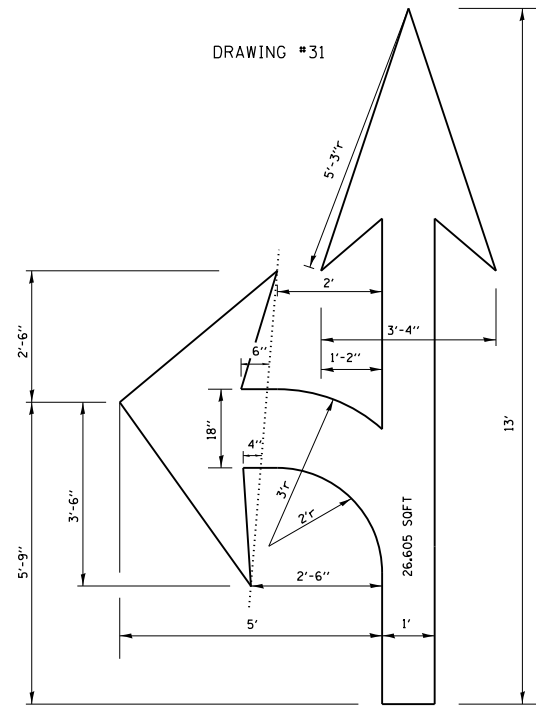
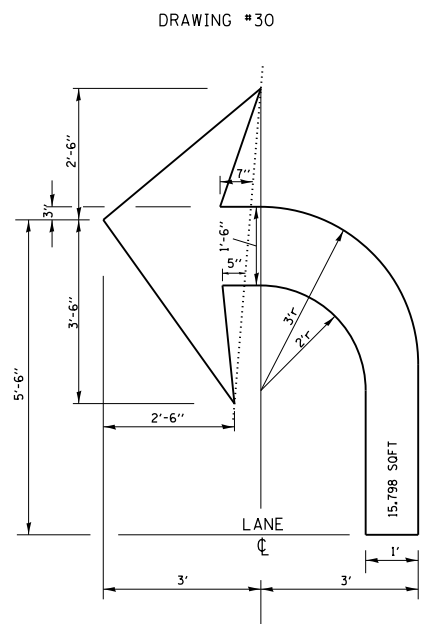
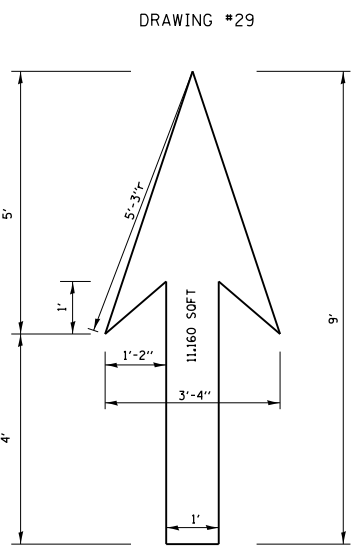
CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	34
TC-24		CONTRACT NO. 62D22		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTE:
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
 DRAWING #28



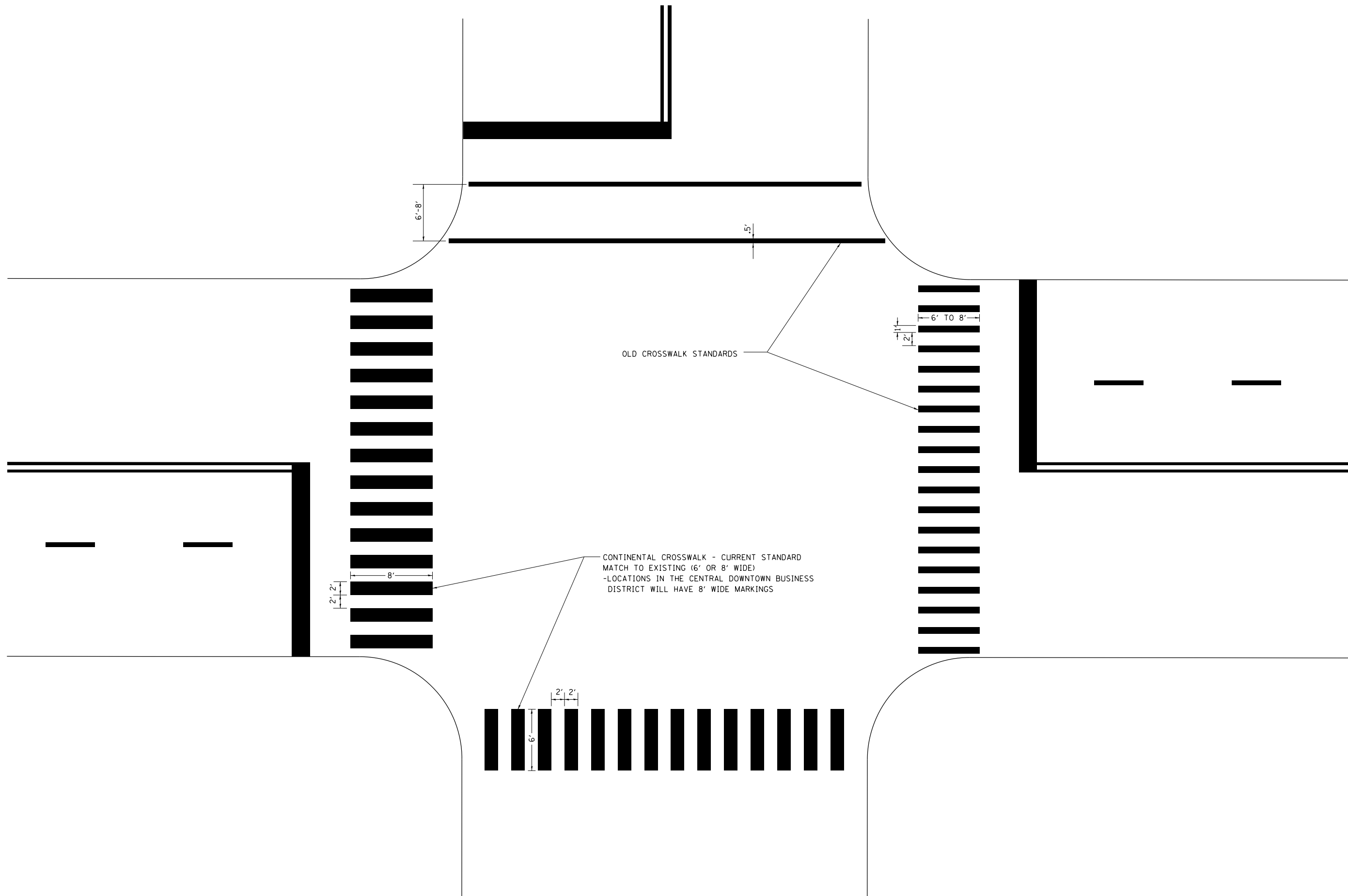
NOTE: ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = guillaumejp	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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		DATE -	REVISED -
			REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	632-1RS-3	COOK	36	35
TC-24		CONTRACT NO. 62022		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



OLD CROSSWALK STANDARDS

CONTINENTAL CROSSWALK - CURRENT STANDARD
 MATCH TO EXISTING (6' OR 8' WIDE)
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS
 DISTRICT WILL HAVE 8' WIDE MARKINGS

FILE NAME =	USER NAME = guillaumejp	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -
	PLOT DATE = 10/28/2016		REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3531	63Z-IRS-3	COOK	36	36
TC-24			CONTRACT NO. 62022	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				