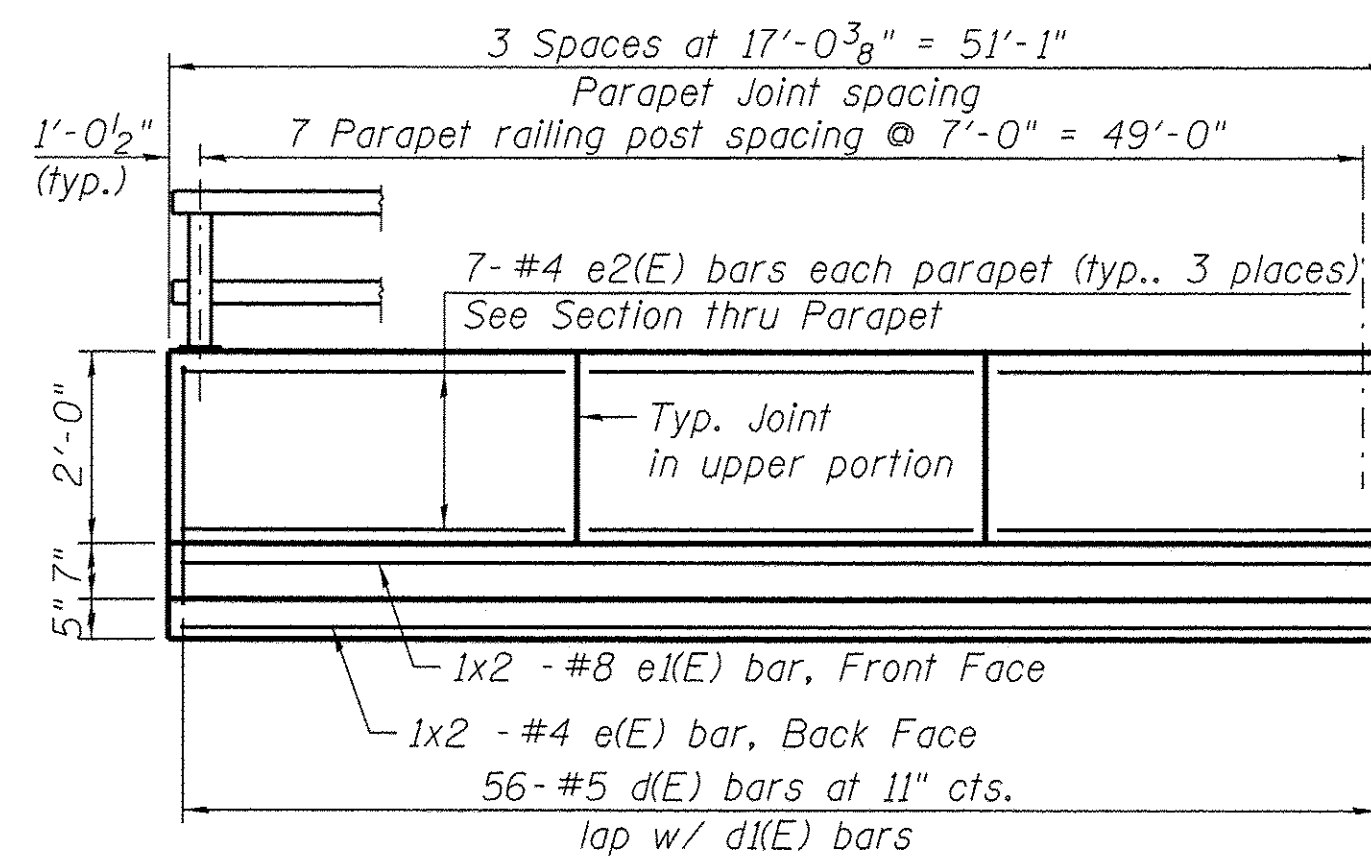


INSIDE ELEVATION OF NORTH PARAPET

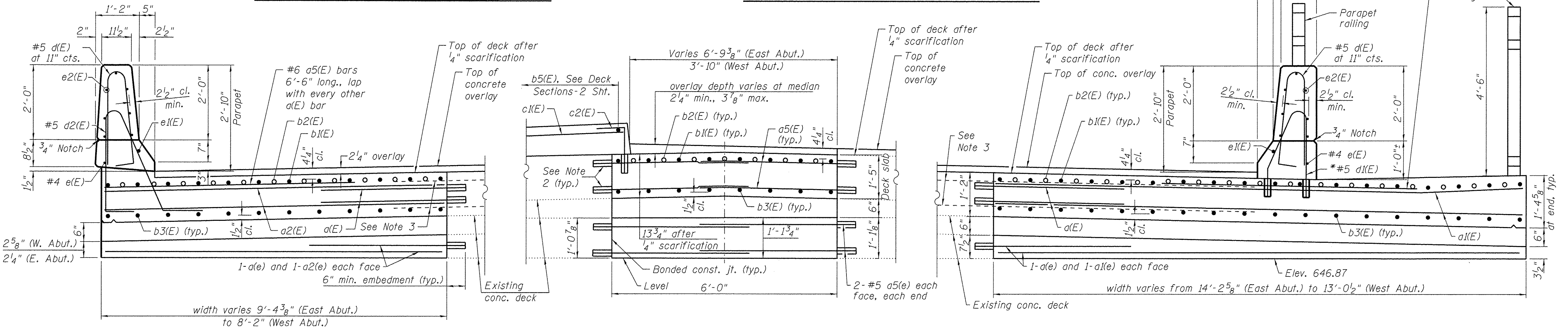


INSIDE ELEVATION OF SOUTH PARAPET

- Notes:
1. Reinforcement bars designated (E) shall be epoxy coated.
 2. Epoxy grout bars a(E) and a5(E) in minimum 7" deep drilled holes. The material and installation shall be in accordance with Standard Specification Section 584.
 3. Existing Transverse reinforcement shall be preserved, cleaned, straightened and reincorporated into new work.

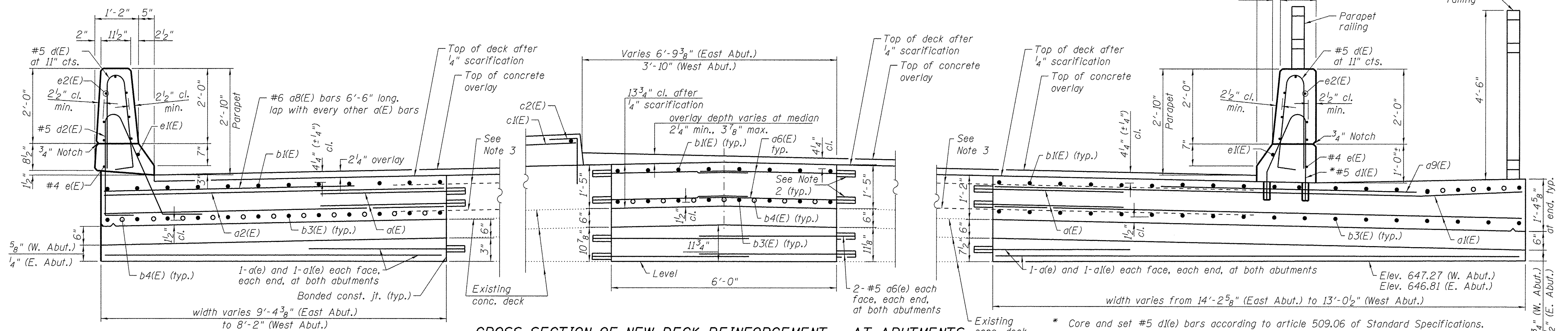
MINIMUM BAR LAP

- (deck slab)
- #5 Bar = 2'-7"
 - #6 Bar = 3'-1"
 - #7 Bar = 4'-8"
 - #8 Bar = 6'-2"



CROSS SECTION OF NEW DECK REINFORCEMENT - AT PIER

(Looking East)



CROSS SECTION OF NEW DECK REINFORCEMENT - AT ABUTMENTS

(Looking East)

* Core and set #5 d1(e) bars according to article 509.06 of Standard Specifications. Cored holes shall be roughened or scored per manufacturers recommendations. Maximum depth of hole shall not exceed 6".

FILE NAME = p:\WCD04_25367381_95\Drawings\Contract\North Structures\Bridge (Spring Brook)\Control Copy - Final Design\IB-Superstructure Details - I.dgn

URS
100 S. WACKER DR.
SUITE 500
CHICAGO IL 60606
TEL (312) 939-1000
FAX (312) 939-4198

USER NAME = STB*
DESIGNED - STB
CHECKED - NPP
DRAWN - SOI
CHECKED - NPP
PLOT SCALE = 8:0" = 1'-0"
PLOT DATE = 1/21/2016

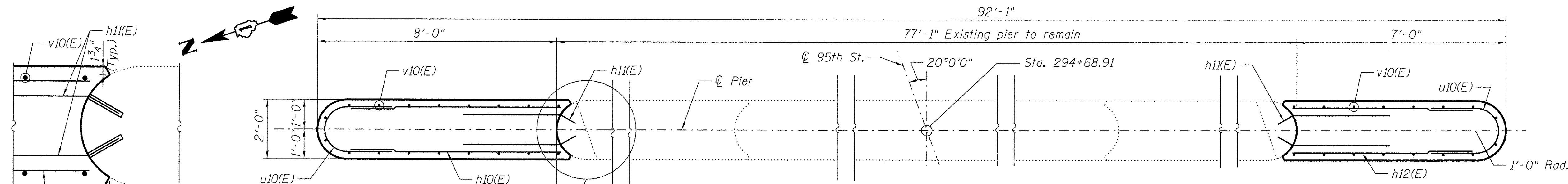
REVISOR -
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REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

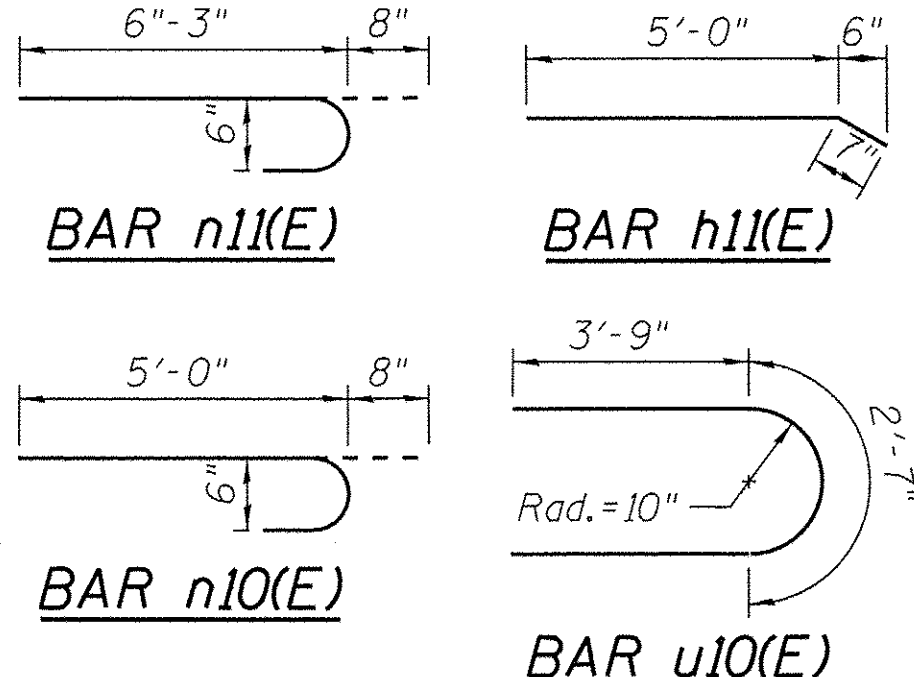
STRUCTURE NO. 099-3105
SHEET NO. 10 OF 19 SHEETS

SECTION 12-00181-01-FP
COUNTY WILL
TOTAL SHEETS 179
SHEET NO. 102
CONTRACT NO. 61A30

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TOP PLAN

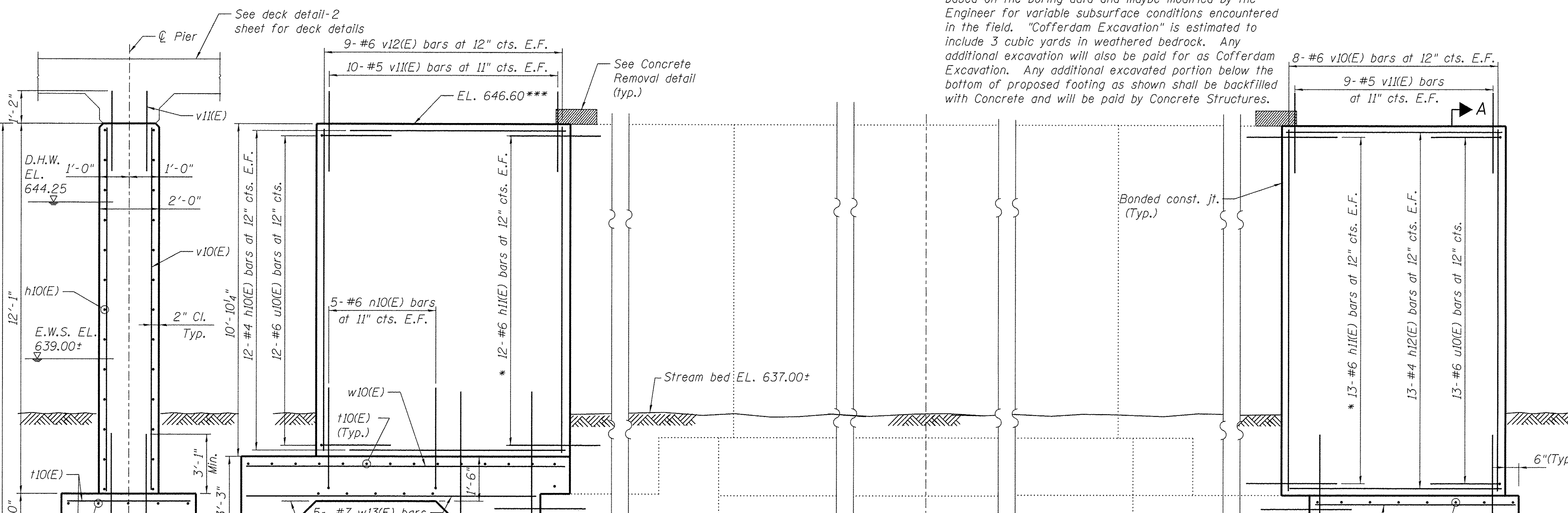


DETAIL 1

Notes:
 1. See Note 1, on Estimated Quantities and General Notes sheet regarding payment for pay item "Cofferdam (Type 2)".
 2. The estimated Cofferdam Excavation quantity shown is based on the boring data and maybe modified by the Engineer for variable subsurface conditions encountered in the field. "Cofferdam Excavation" is estimated to include 3 cubic yards in weathered bedrock. Any additional excavation will also be paid for as Cofferdam Excavation. Any additional excavated portion below the bottom of proposed footing as shown shall be backfilled with Concrete and will be paid by Concrete Structures.

PIER BILL OF MATERIAL

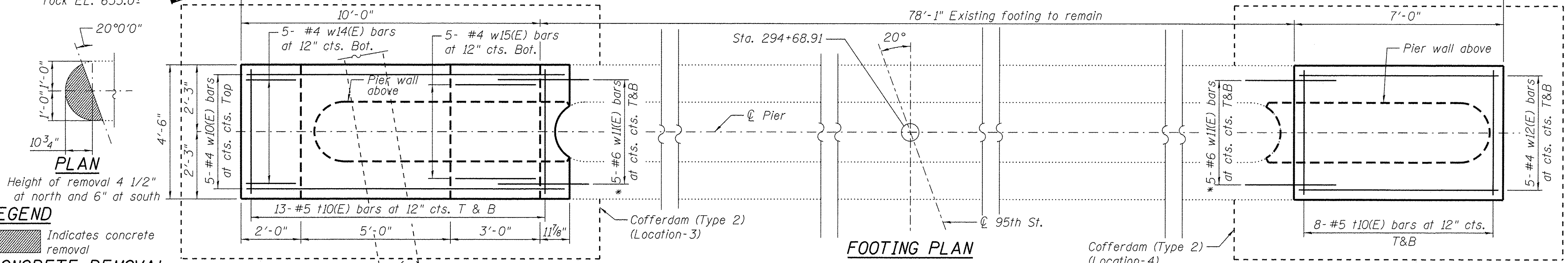
Bar	No.	Size	Length	Shape	
n10(E)	24	#4	6'-10"	—	
h11(E)	50	#6	3'-11"	—	
h12(E)	26	#4	5'-10"	—	
n10(E)	26	#6	5'-8"	—	
n11(E)	8	#6	6'-11"	—	
f10(E)	42	#5	4'-2"	—	
u10(E)	25	#6	10'-1"	—	
v10(E)	16	#6	11'-11"	—	
v11(E)	38	#5	2'-0"	—	
v12(E)	18	#6	10'-8"	—	
w10(E)	5	#4	10'-8"	—	
w11(E)	20	#6	3'-9"	—	
w12(E)	10	#4	6'-10"	—	
w13(E)	5	#7	9'-8"	—	
w14(E)	5	#4	1'-8"	—	
w15(E)	5	#4	2'-8"	—	
Reinforcement Bars, Epoxy Coated				Pound	2,340
Concrete Structures				Cu. Yds.	19.4
Cofferdam Excavation				Cu. Yds.	27
Cofferdam (Type 2) (Location-3)				Each	1
Cofferdam (Type 2) (Location-4)				Each	1
Concrete Removal				Cu. Yds.	0.1



ELEVATION
(Looking East)

SECTION A-A

Estimated top of rock EL. 633.0±
 Existing 1'-0" Conc. encasement
 Existing 16" φ watermain. Top of pipe EL. 634.00. See note.



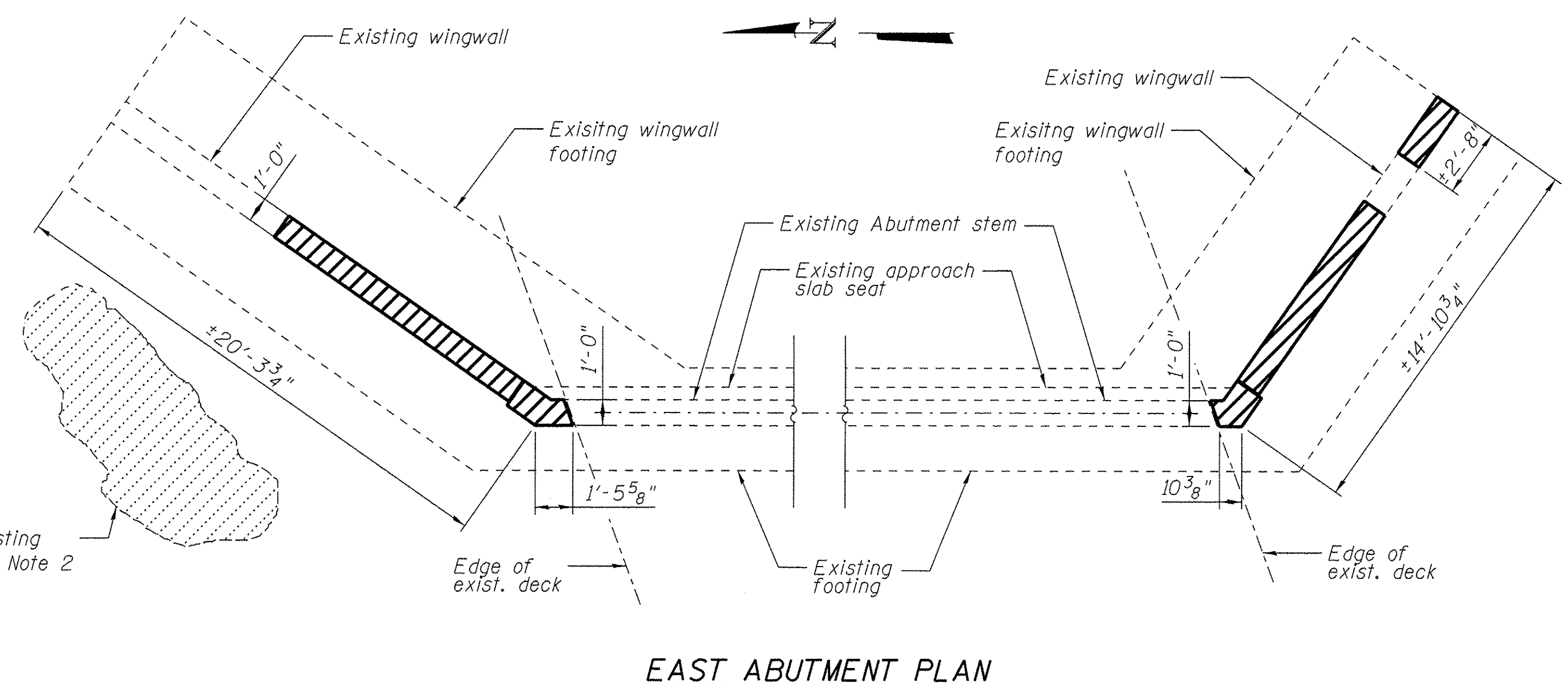
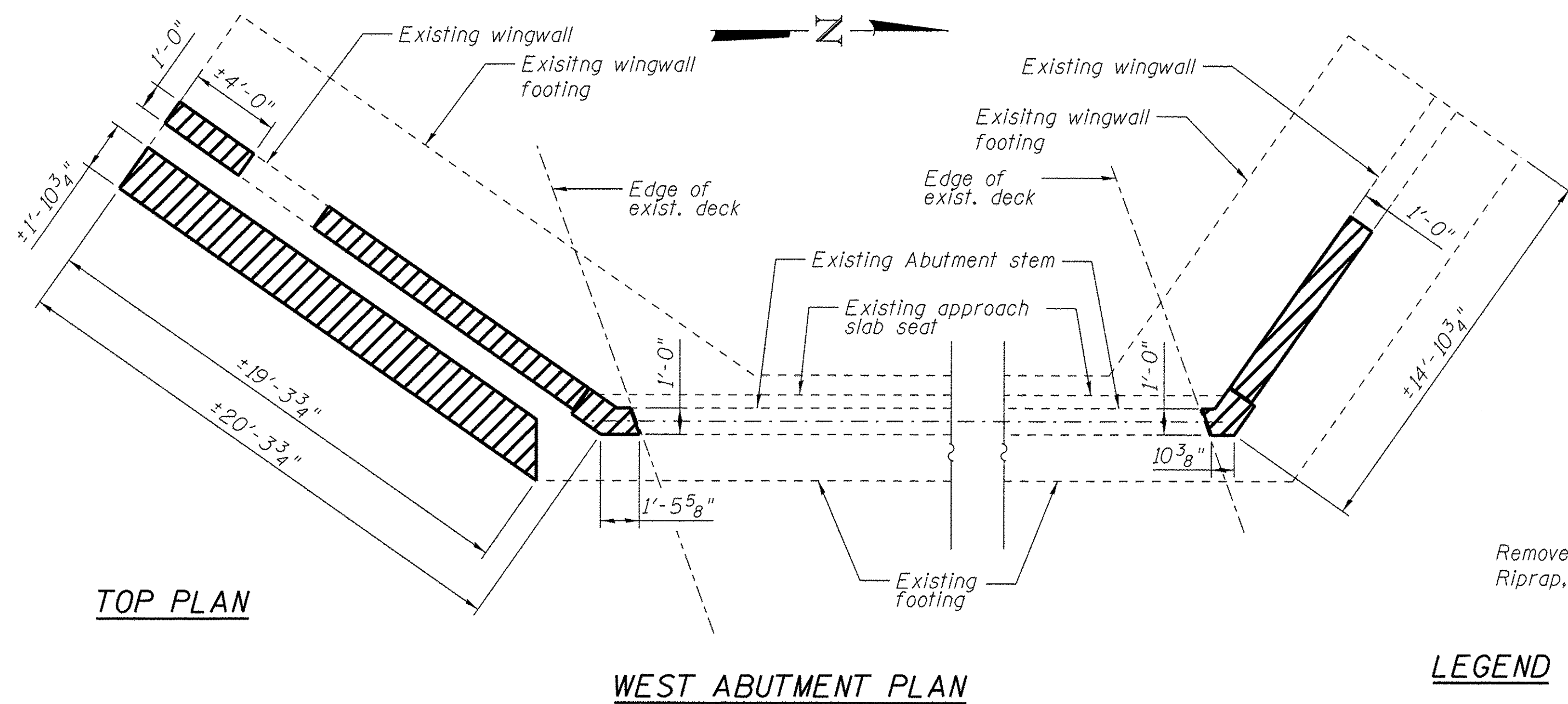
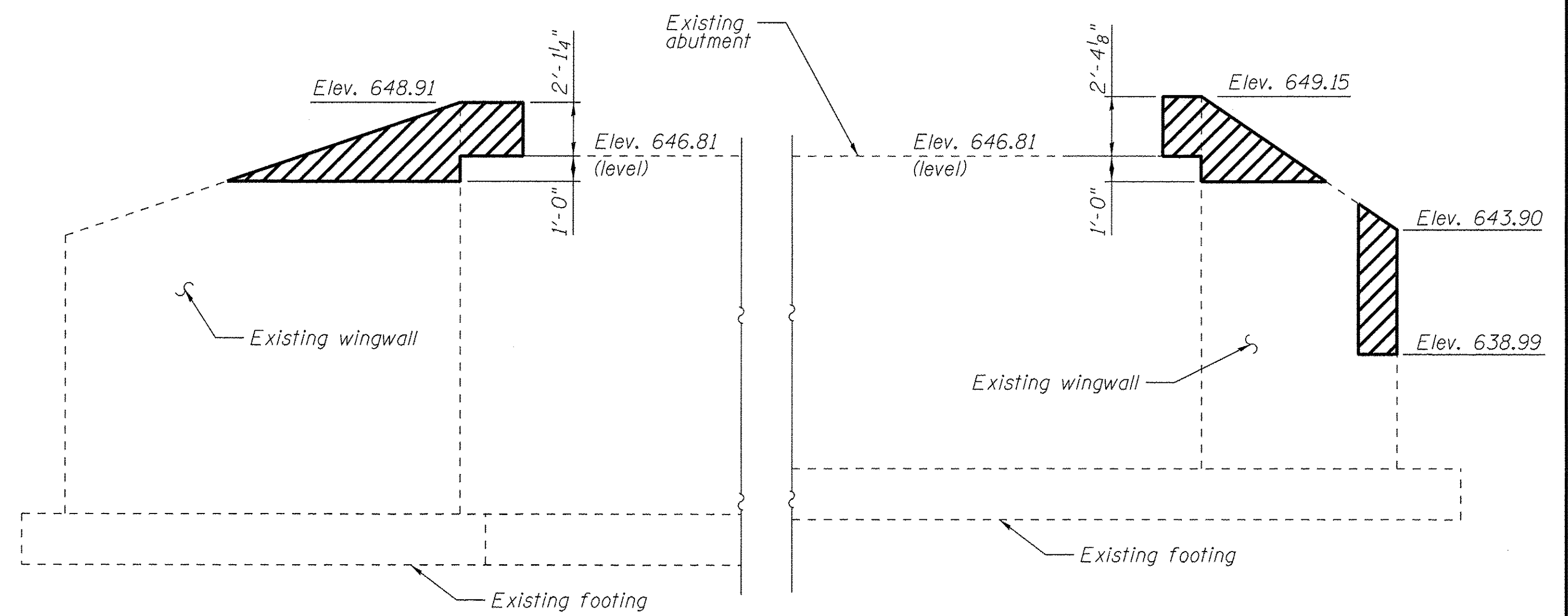
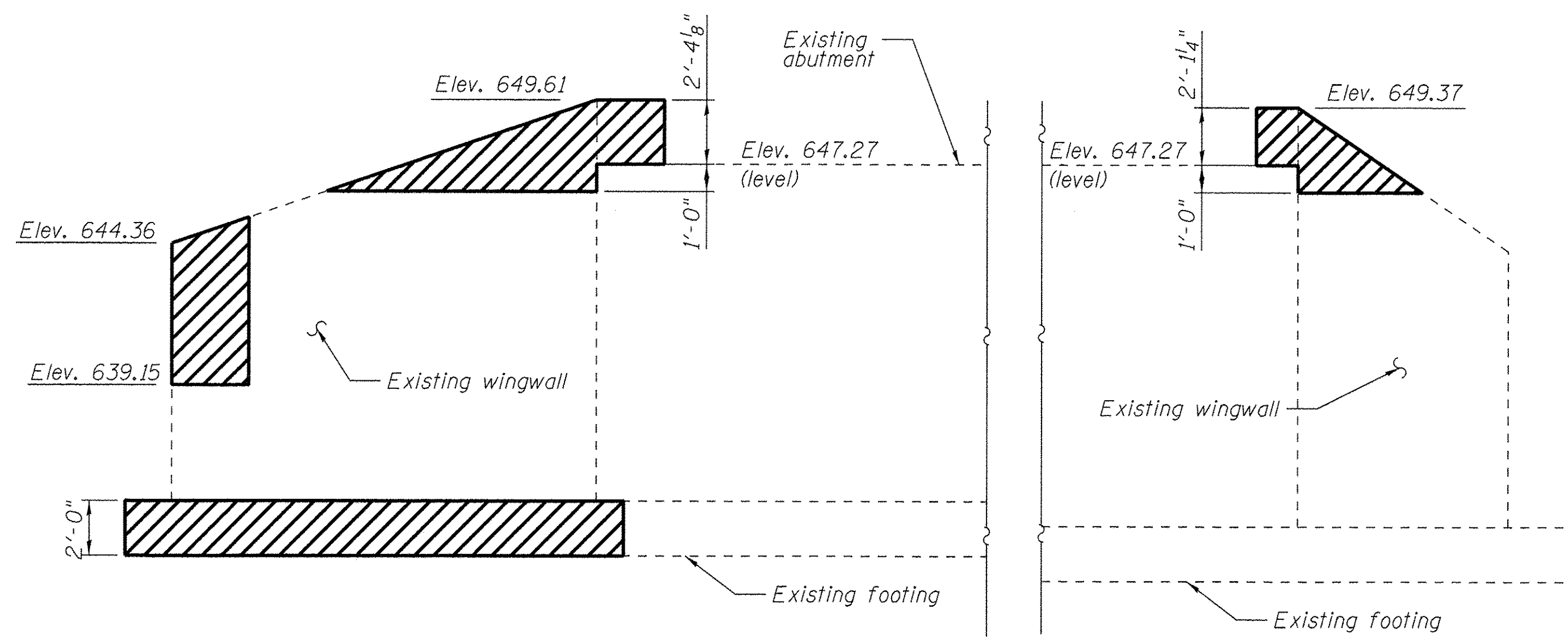
FOOTING PLAN

LEGEND
 Indicates concrete removal

*Epoxy grout bars h11(E) and w11(E) in minimum 7" deep drilled holes. The material and installation shall be in accordance with Standard Specification Section 584.
 **If a portion of the pier wall or concrete encasement is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.
 ***Elevations are taken from 1994 existing bridge rehabilitation plans and are not guaranteed.
 Based on preliminary information from City of Naperville, Department of Public Utility Waste Water Division, top of existing 16" watermain is at EL. 633.00 with 1'-0" concrete encasement around and above it. This information is subject to variance. Verify location and elevation of existing watermain before ordering reinforcement steel and designing and erecting cofferdam. No consideration for extra payment will be made for accommodating this existing watermain.

FILE NAME = p:\WCD04\25367381\95th\Drawings\Contract\North\Structures\Bridg... (Spring Brook)\Control Copy - Final Design\12-Pier.dgn

<p>100 S. WACKER DR. SUITE 500 CHICAGO IL 60606 TEL (312) 939-1000 FAX (312) 939-4198</p>	USER NAME = #STB*	DESIGNED - STB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PIER STRUCTURE NO. 099-3105	F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 104	
	PLOT SCALE = 1:11,999.0 '1" = 119.99'	DRAWN - KJZ	REVISED -			SHEET NO. 12 OF 19 SHEETS			CONTRACT NO. 61A30		
	PLOT DATE = 1/21/2016	CHECKED - NPP	REVISED -						ILLINOIS FED. AID PROJECT		



LEGEND

- Limits of concrete removal
- Limits of riprap removal

NOTES:

1. Elevations are taken from 1994 existing bridge rehabilitation plans and are not guaranteed. If actual conditions in the field vary from those shown, removal limits may need to be adjusted accordingly.
2. Existing riprap gabion basket adjacent to Northeast wingwall shall be removed. Cost is considered included with the unit price of "Concrete Removal" item.

BILL OF MATERIAL

Concrete Removal	Cu. Yds.	7.0

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100 S. WACKER DR.,
SUITE 500
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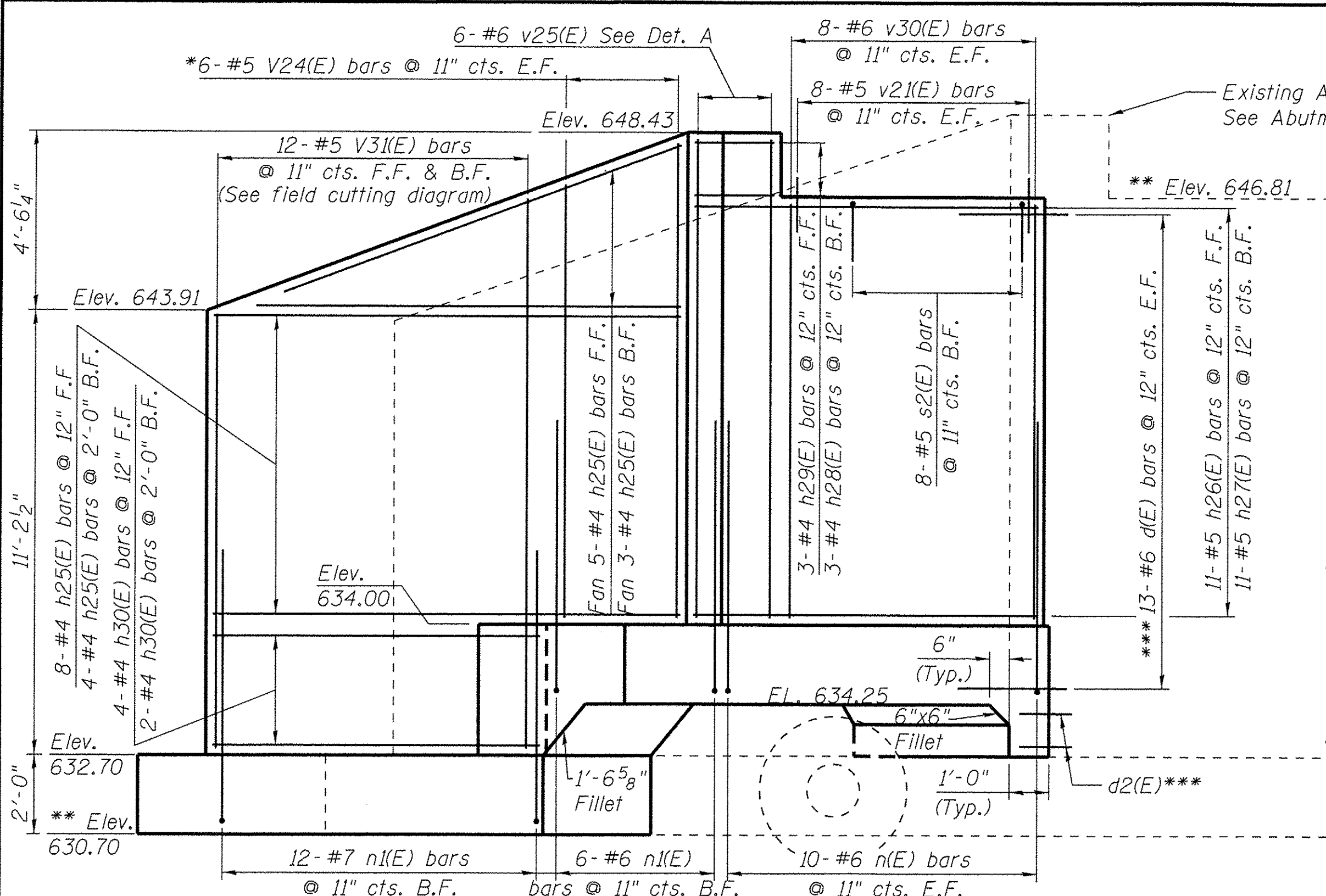
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PLOT DATE = 1/21/2016	DRAWN - KJZ	REVISD -
	CHECKED - NPP	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

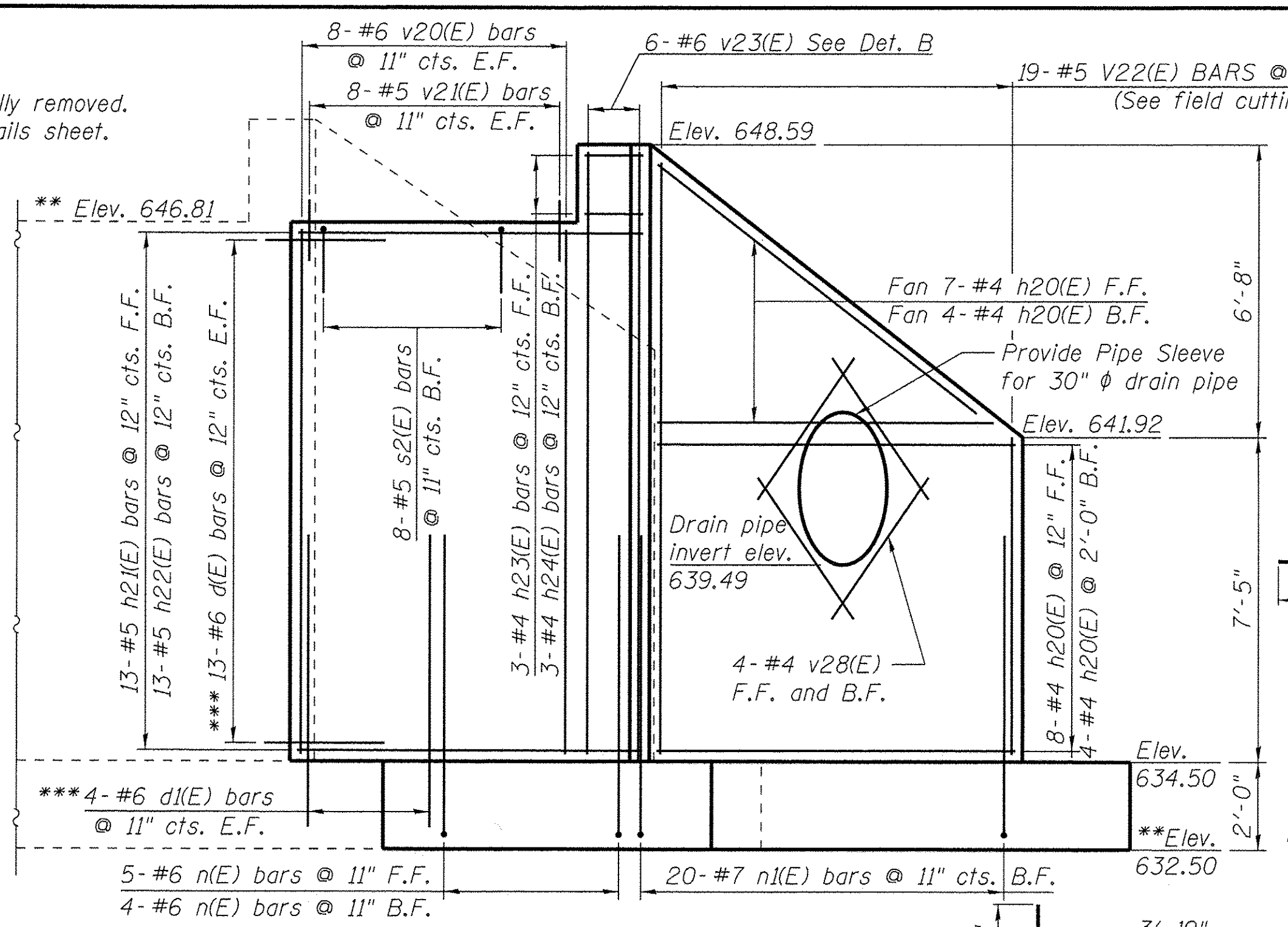
**ABUTMENT REMOVAL DETAILS
STRUCTURE NO. 099-3105**

SHEET NO. 13 OF 19 SHEETS

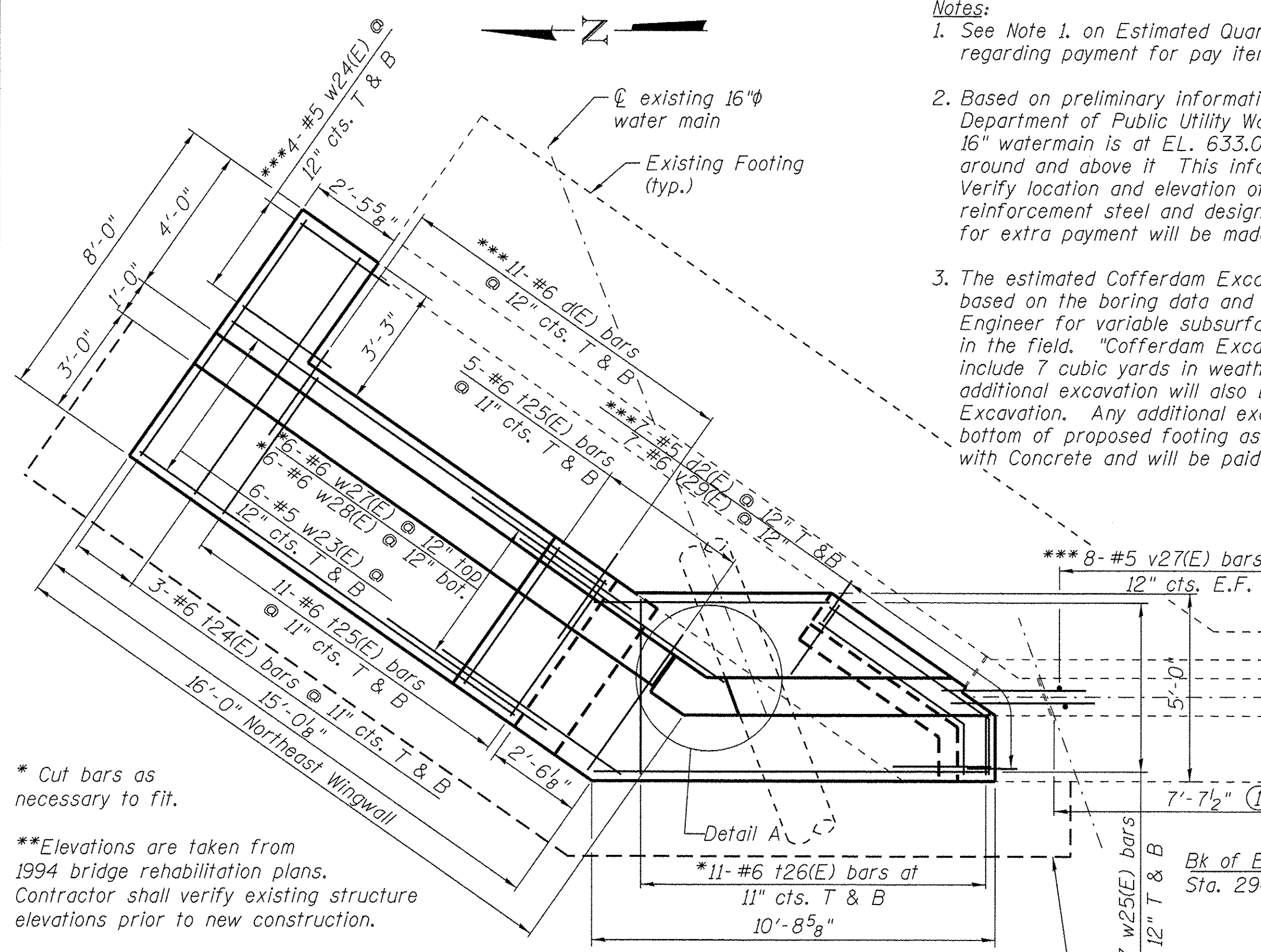
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1644	12-00181-01-FP	WILL	179	105
				CONTRACT NO. 61A30
ILLINOIS FED. AID PROJECT				



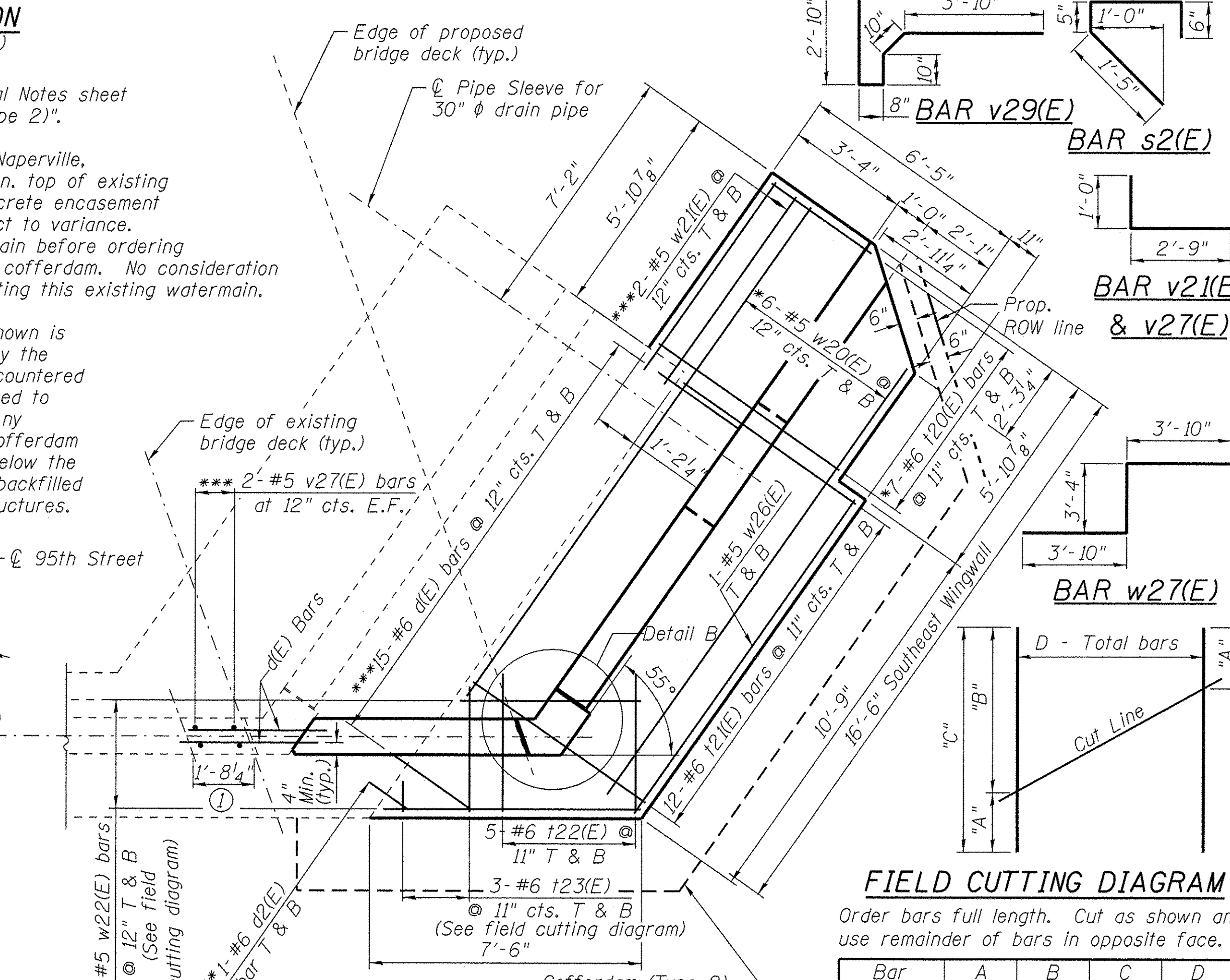
ELEVATION
(Looking East)



ELEVATION
(Looking East)



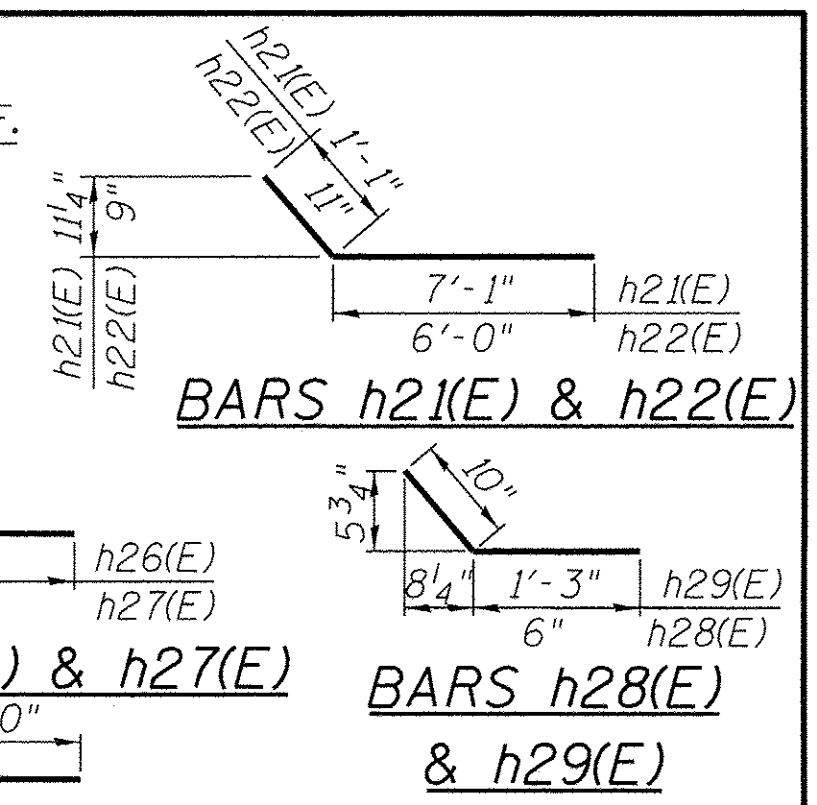
PLAN



FIELD CUTTING DIAGRAM

Order bars full length. Cut as shown and use remainder of bars in opposite face.

Bar	A	B	C	D
t23(E)	10"	3'-5"	4'-3"	3
v22(E)	7'-3"	13'-9"	21'-0"	19
v24(E)	11'-0"	15'-2"	26'-2"	18
v31(E)	10'-11"	13'-10"	24'-9"	12
w22(E)	5'-0"	7'-1"	12'-1"	5



BILL OF MATERIAL

Bar	Size	No.	Length	Shape
d(E)	#6	104	3'-10"	
d1(E)	#6	24	5'-8"	
d2(E)	#5	16	1'-4"	
h20(E)	#4	23	14'-8"	
h21(E)	#5	13	8'-2"	
h22(E)	#5	13	6'-11"	
h23(E)	#4	3	1'-10"	
h24(E)	#4	3	1'-3"	
h25(E)	#4	20	14'-6"	
h26(E)	#5	11	8'-6"	
h27(E)	#5	11	7'-4"	
h28(E)	#4	3	1'-4"	
h29(E)	#4	3	2'-1"	
h30(E)	#4	6	12'-2"	
n(E)	#6	19	7'-7"	
n1(E)	#7	38	8'-1"	
s2(E)	#5	16	3'-7"	
t20(E)	#6	14	6'-1"	
t21(E)	#6	24	5'-9"	
t22(E)	#6	10	3'-8"	
t23(E)	#6	3	4'-3"	
t24(E)	#6	6	7'-8"	
t25(E)	#6	32	4'-5"	
t26(E)	#6	22	4'-8"	
v20(E)	#6	16	12'-1"	
v21(E)	#5	90	3'-9"	
v22(E)	#5	19	21'-0"	
v23(E)	#6	22	13'-11"	
v24(E)	#5	18	26'-2"	
v25(E)	#6	6	12'-1"	
v27(E)	#5	20	3'-9"	
v28(E)	#4	8	4'-1"	
v29(E)	#6	7	9'-0"	
v30(E)	#6	16	10'-5"	
v31(E)	#5	12	24'-9"	
w20(E)	#5	12	16'-6"	
w21(E)	#5	4	6'-4"	
w22(E)	#5	5	12'-1"	
w23(E)	#5	12	12'-2"	
w24(E)	#5	8	2'-10"	
w25(E)	#7	12	10'-5"	
w26(E)	#5	2	10'-11"	
w27(E)	#6	6	11'-0"	
w28(E)	#6	6	11'-6"	
Cofferdam Excavation	Cu. Yd.		192	
Concrete Structures	Cu. Yd.		41.8	
Reinforcement Bars, Epoxy Coated	Pound		7,270	
Cofferdam (Type 2) (Location-5)	Each		1	
Cofferdam (Type 2) (Location-6)	Each		1	
Geocomposite Wall	Sq. Yd.		38	
Drain				

* See Field Cutting Diagram "Cofferdam Excavation" includes approximately 7 cubic yards of rock excavation.

- Notes:**
- See Note 1. on Estimated Quantities and General Notes sheet regarding payment for pay item "Cofferdam (Type 2)".
 - Based on preliminary information from City of Naperville, Department of Public Utility Waste Water Division, top of existing 16" watermain is at EL. 633.00 with 1'-0" concrete encasement around and above it. This information is subject to variance. Verify location and elevation of existing watermain before ordering reinforcement steel and designing and erecting cofferdam. No consideration for extra payment will be made for accommodating this existing watermain.
 - The estimated Cofferdam Excavation quantity shown is based on the boring data and maybe modified by the Engineer for variable subsurface conditions encountered in the field. "Cofferdam Excavation" is estimated to include 7 cubic yards in weathered bedrock. Any additional excavation will also be paid for as Cofferdam Excavation. Any additional excavated portion below the bottom of proposed footing as shown shall be backfilled with Concrete and will be paid by Concrete Structures.

* Cut bars as necessary to fit.

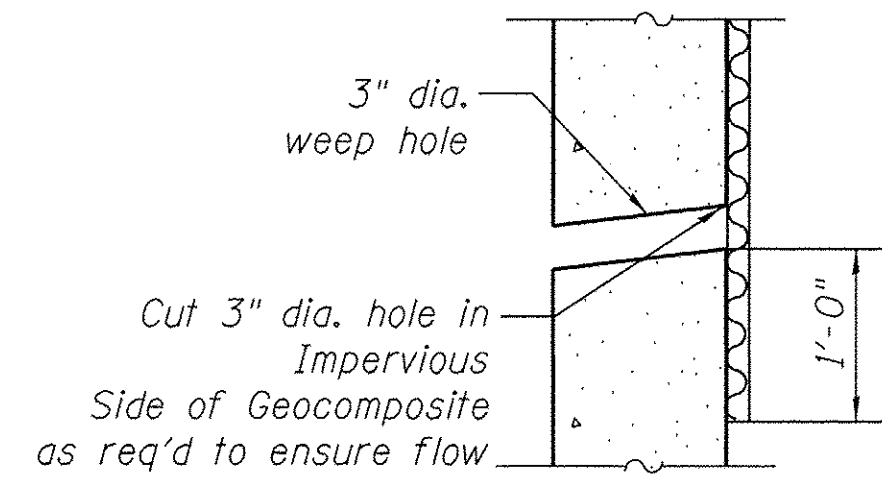
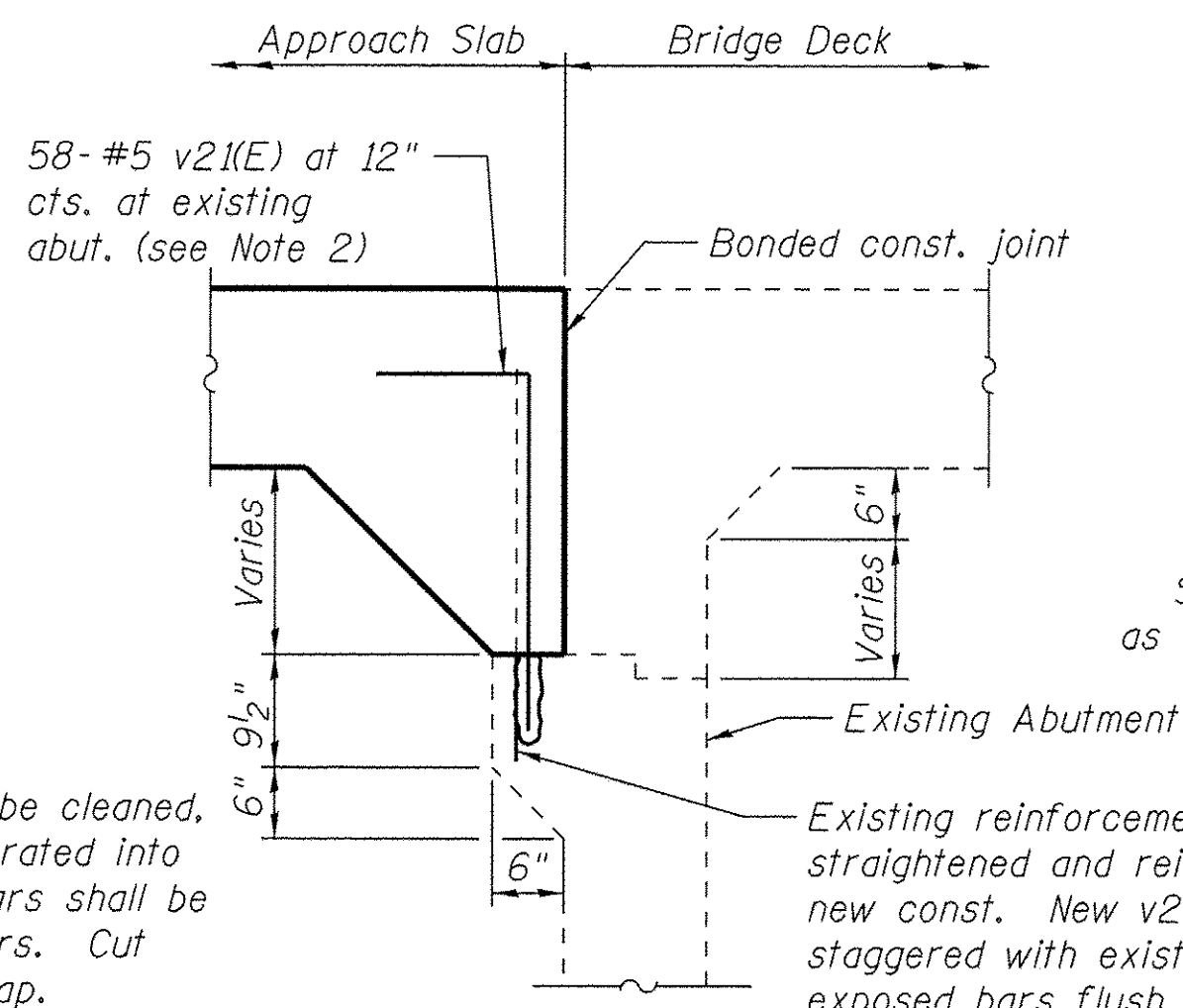
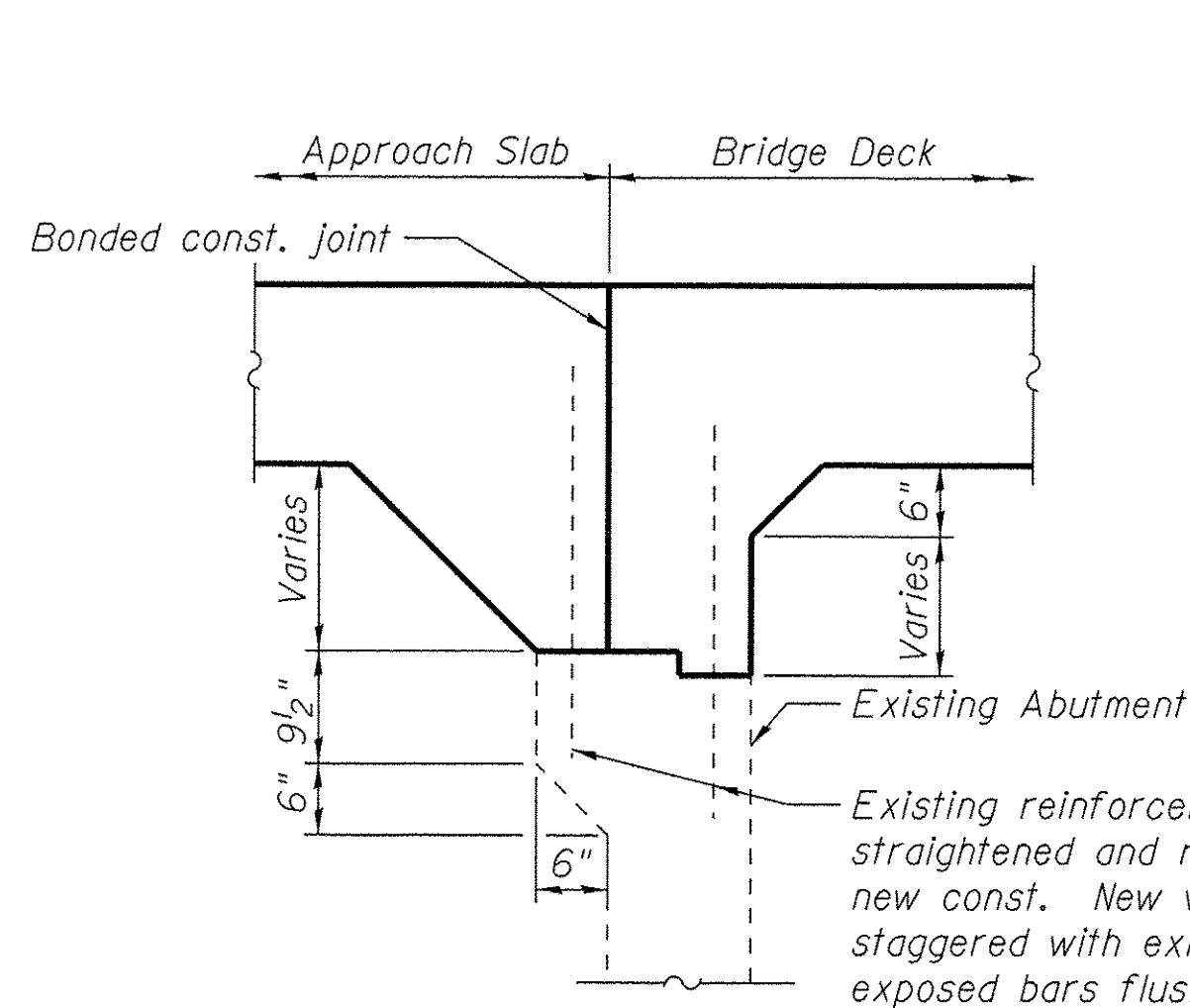
**Elevations are taken from 1994 bridge rehabilitation plans. Contractor shall verify existing structure elevations prior to new construction.

***Epoxy grout bars d(E), d2(E), v27(E), w21(E), w24(E) and bars d1(E) in minimum 8" deep drilled holes. The material and installation shall be in accordance with Standard Specification Section 584.

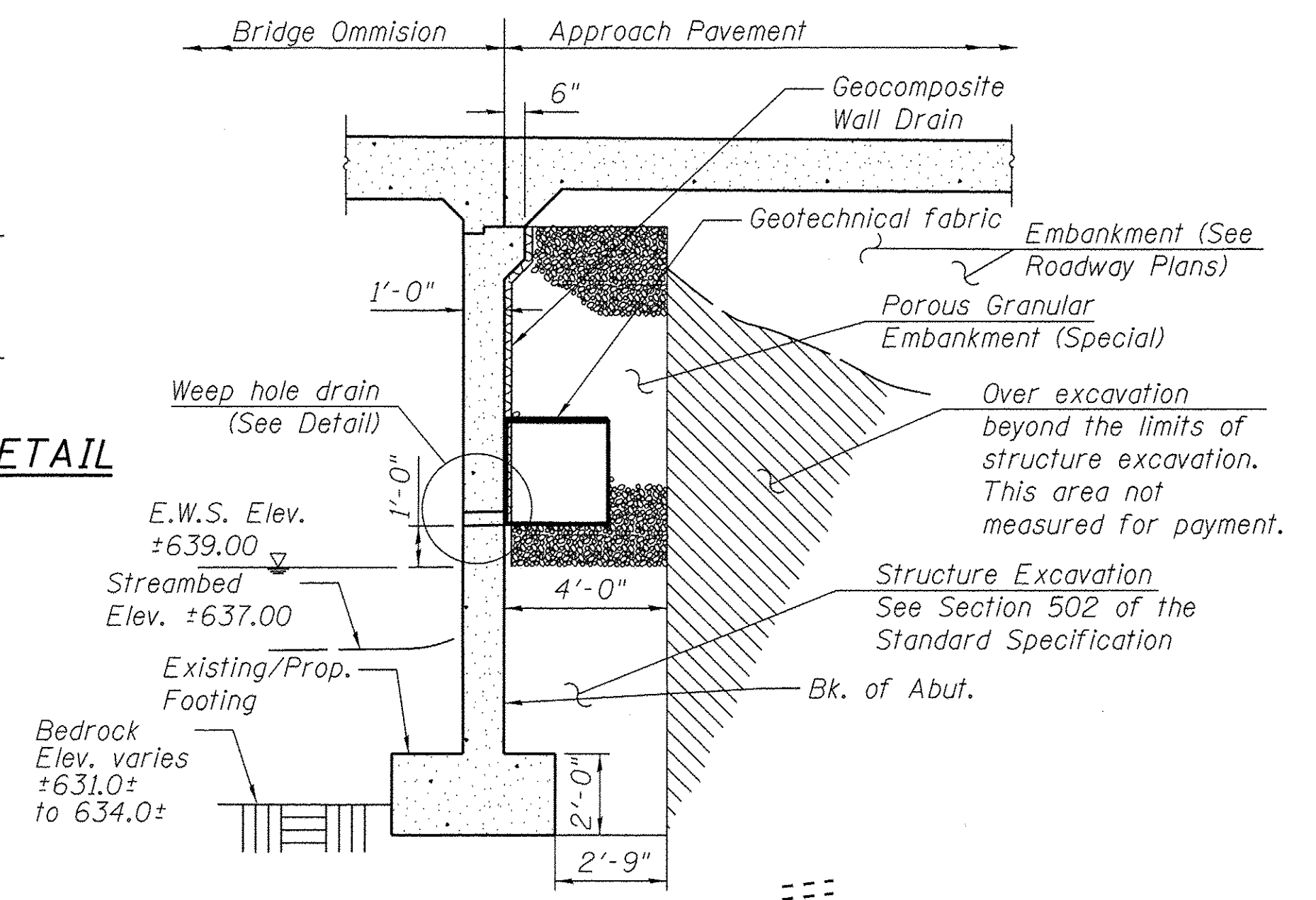
See sheet East Abutment Details for Details A & B

Cost of pipe sleeve is included with unit price of "Concrete Structures".

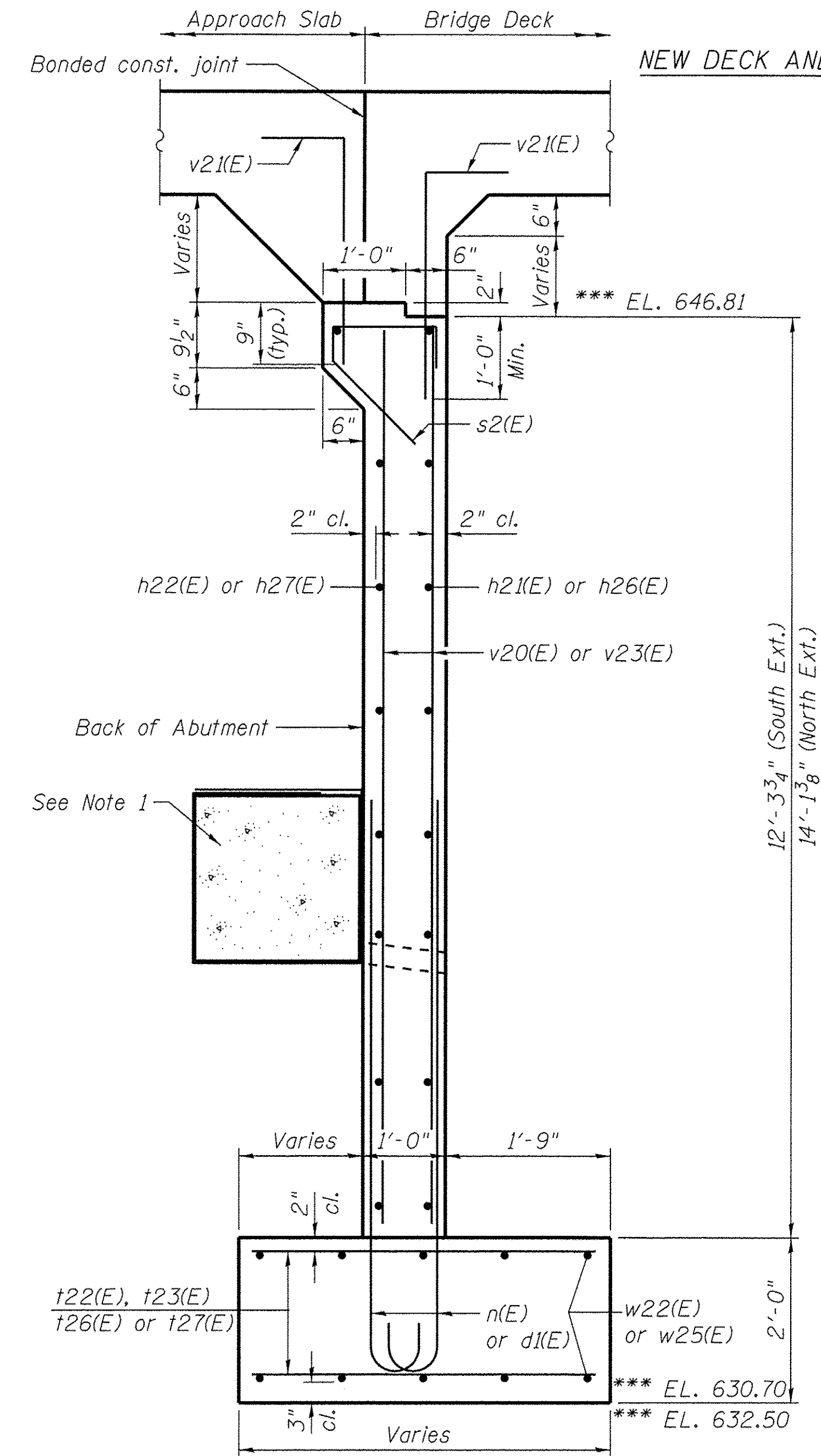
① During removal of the existing deck in this area the existing vertical reinforcement to be cleaned, straightened and reincorporated into new const. New v27(E) bars are staggered with existing bars



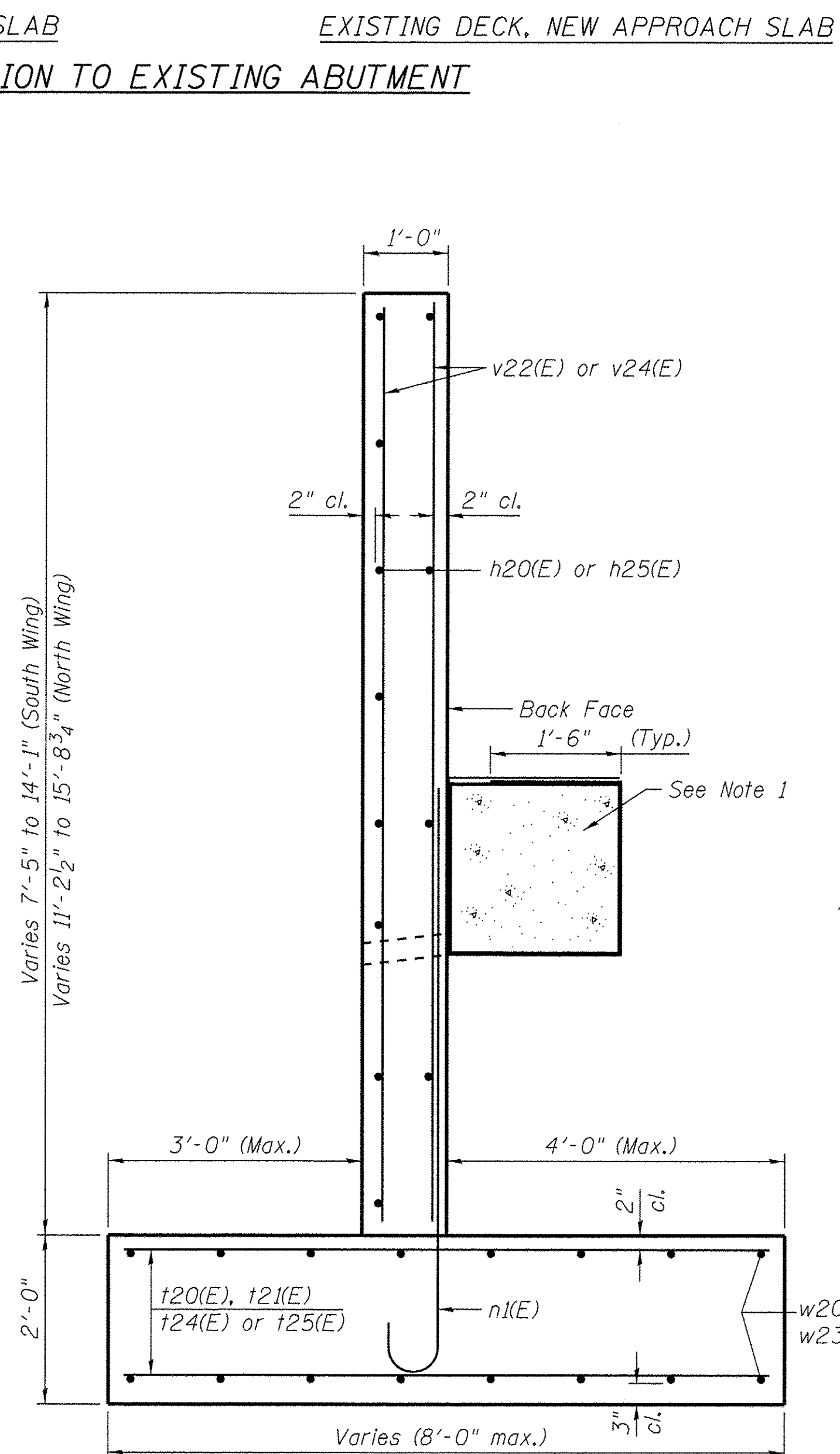
WEEP HOLE DRAIN DETAIL



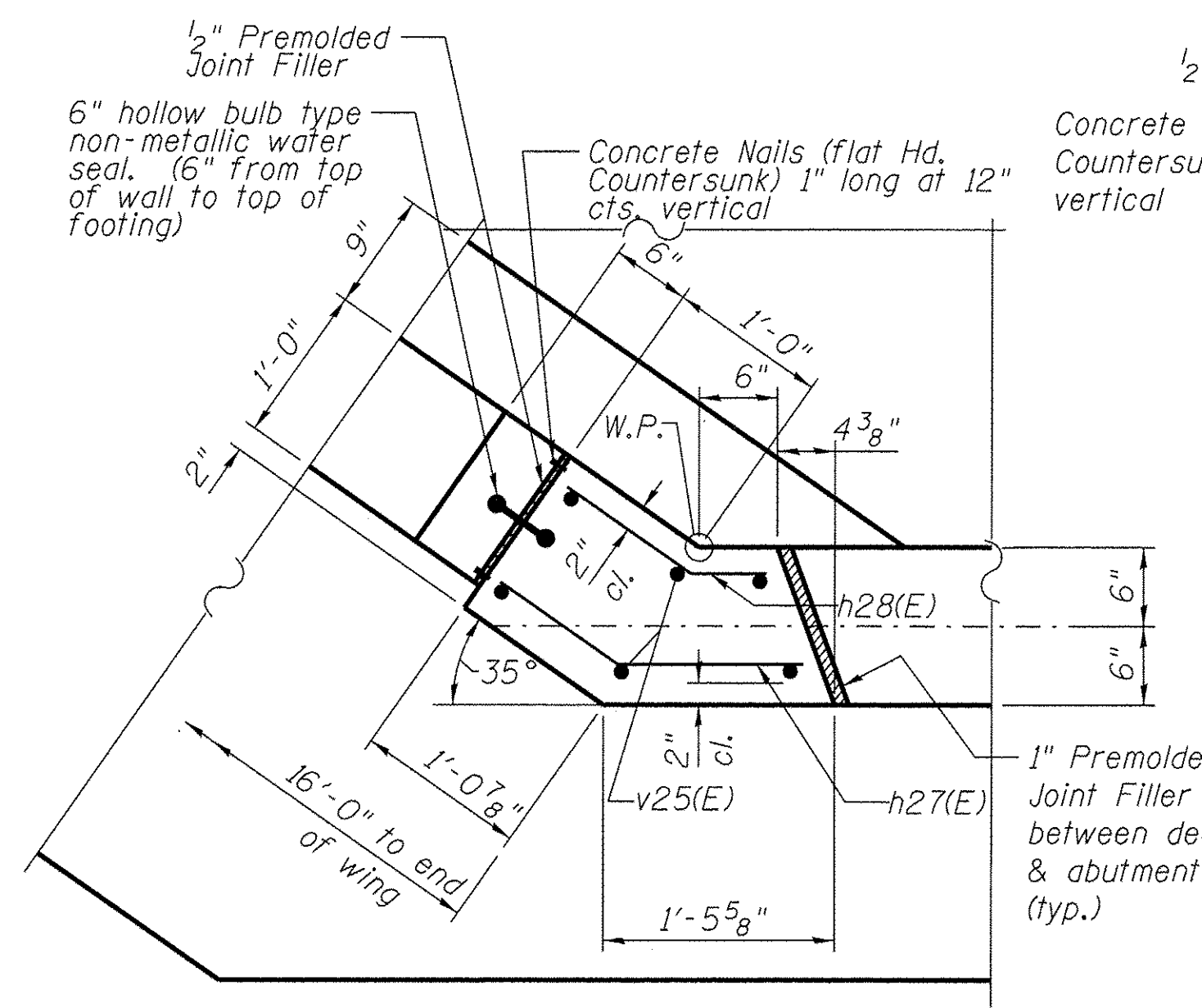
SECTION THRU ABUTMENT



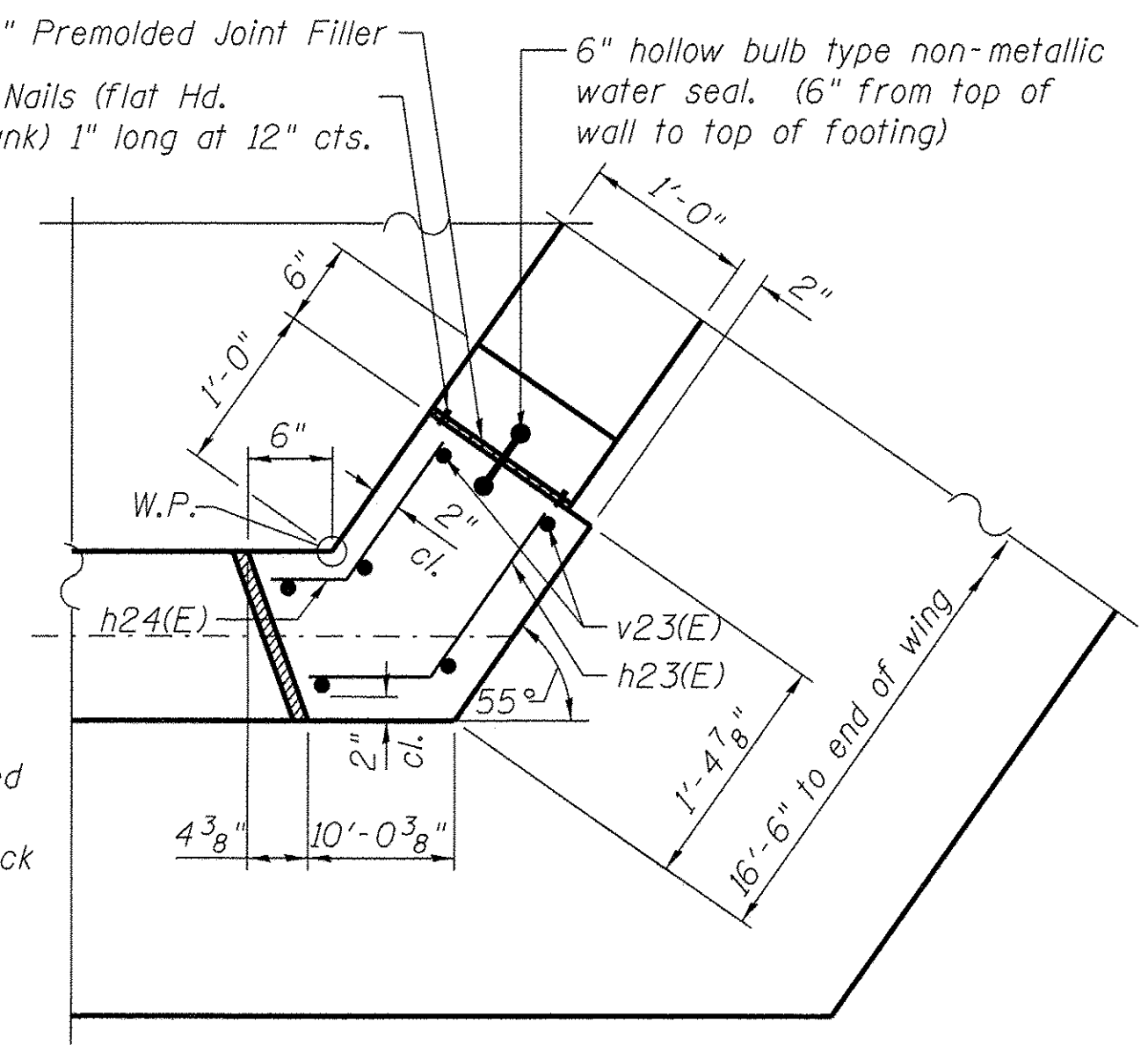
NEW DECK AND APPROACH SLAB CONNECTION TO EXISTING ABUTMENT



SECTION THROUGH WING



DETAIL A



DETAIL B

- Notes:
- 2'x2'x2' area completely wrapped with Geotechnical fabric with overlap shown. The area shall be backfilled with uncompacted porous granular embankment with gradation of CA-5 or Ca-7. Cost included with "Porous Granular Embankment (Special)"
 - Epoxy grout bars v21(E) in minimum 7" deep drilled holes. The material and installation shall be in accordance with Standard Specification Section 584.
 - ***Elevations are taken from 1994 bridge rehabilitation plans. Contractor shall verify existing structure elevations prior to new construction.
 - See sheet East Abutment for locations of Details A & B

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USER NAME = STB*
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PLOT SCALE = 1/4" = 1'-0"
PLOT DATE = 1/21/2016

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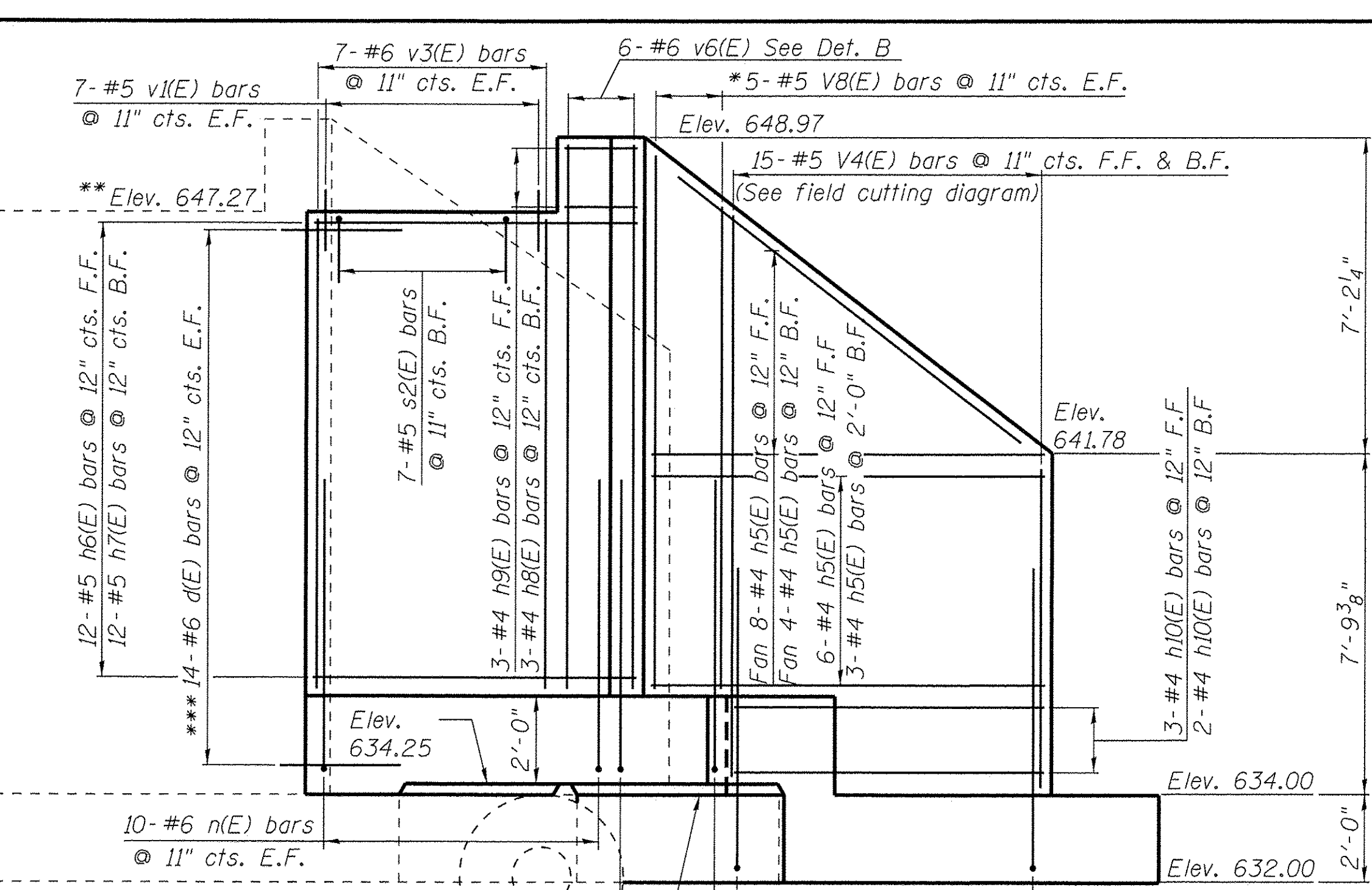
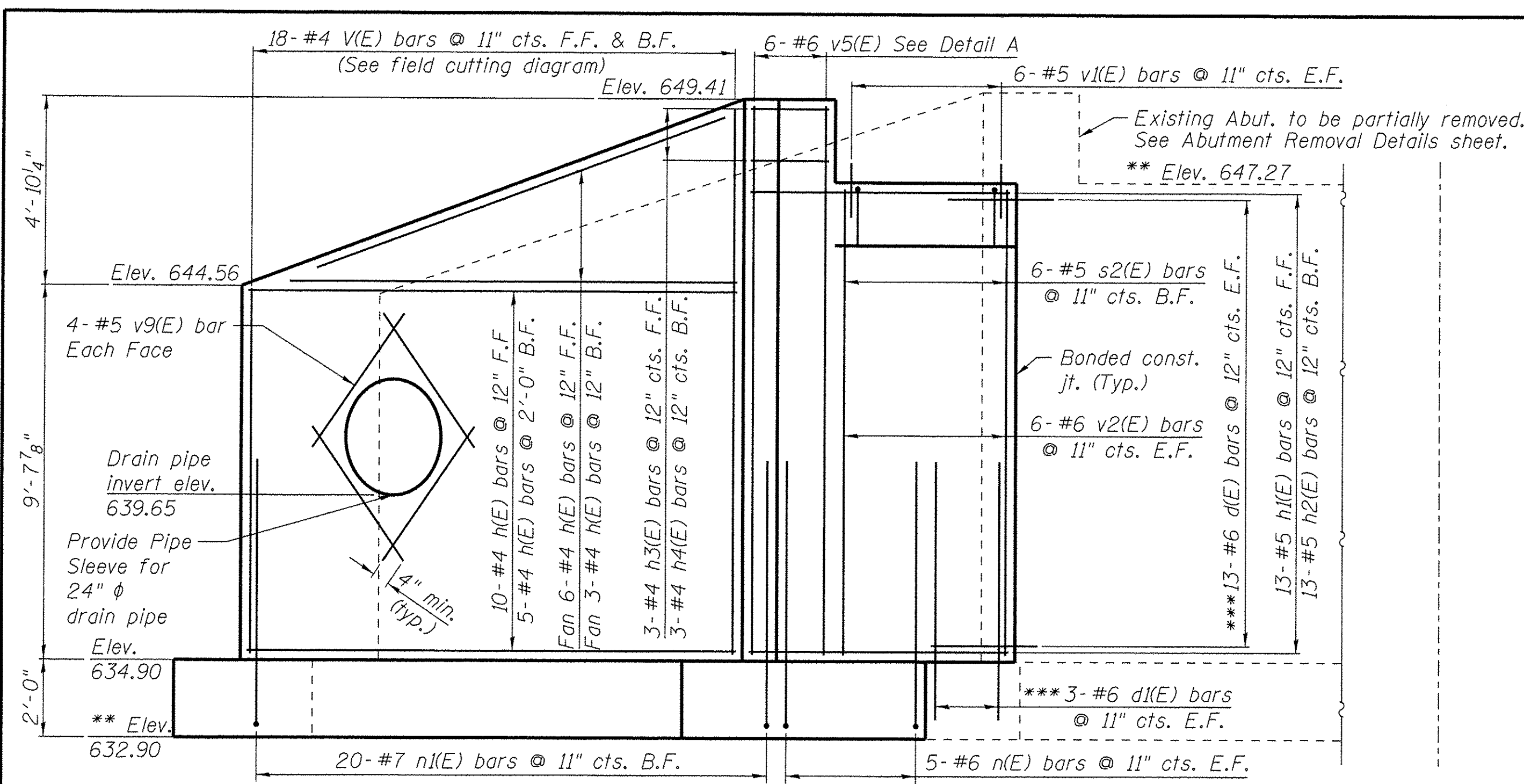
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EAST ABUTMENT DETAILS
STRUCTURE NO. 099-3105**

SHEET NO. 15 OF 19 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	107
CONTRACT NO. 61A30				
ILLINOIS FED. AID PROJECT				

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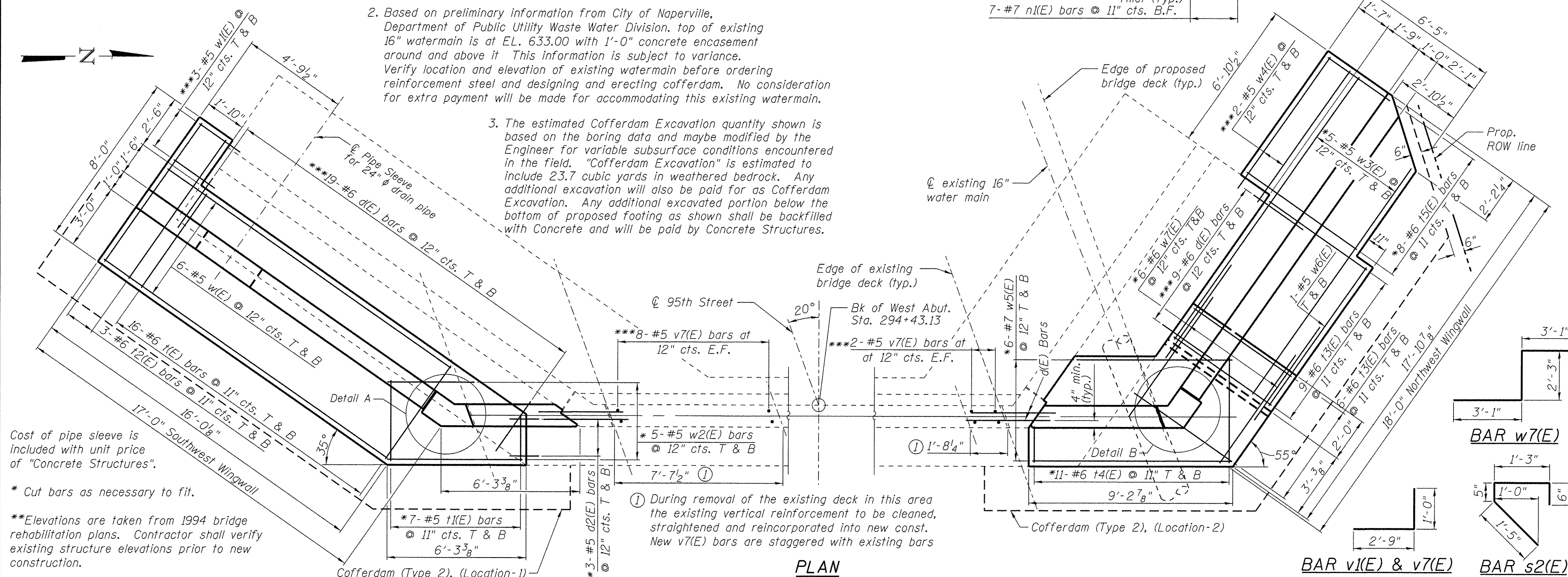
BILL OF MATERIAL

Bar	Size	No.	Length	Shape
d(E)	#6	110	3'-10"	—
d1(E)	#6	6	5'-8"	—
d2(E)	#5	6	1'-4"	—
h(E)	#4	24	15'-6"	—
h1(E)	#5	13	6'-6"	—
h2(E)	#5	13	5'-4"	—
h3(E)	#4	3	2'-1"	—
h4(E)	#4	3	1'-4"	—
h5(E)	#4	24	15'-11"	—
h6(E)	#5	12	7'-8"	—
h7(E)	#5	12	6'-6"	—
h8(E)	#4	3	1'-3"	—
h9(E)	#4	3	1'-10"	—
h10(E)	#4	5	12'-6"	—
n(E)	#6	30	7'-7"	—
n1(E)	#7	49	8'-1"	—
s2(E)	#5	13	3'-7"	—
t(E)	#6	32	5'-2"	—
t1(E)	#6	14	3'-8"	—
t2(E)	#6	6	7'-8"	—
t3(E)	#6	30	5'-5"	—
t4(E)	#6	22	4'-9"	—
t5(E)	#6	16	6'-1"	—
v(E)	#5	18	23'-6"	—
v1(E)	#5	84	3'-9"	—
v2(E)	#6	12	12'-2"	—
v3(E)	#6	14	10'-8"	—
v4(E)	#5	20	22'-2"	—
v5(E)	#6	6	14'-4"	—
v6(E)	#6	6	12'-4"	—
v7(E)	#5	20	3'-9"	—
v8(E)	#5	10	12'-2"	—
v9(E)	#5	8	3'-6"	—
w(E)	#5	12	15'-9"	—
w1(E)	#5	6	2'-3"	—
w2(E)	#5	10	6'-1"	—
w3(E)	#5	10	14'-6"	—
w4(E)	#5	4	7'-3"	—
w5(E)	#7	12	9'-7"	—
w6(E)	#5	2	7'-8"	—
w7(E)	#6	12	8'-5"	—
Cofferdam Excavation	Cu. Yd.	194.7		
Concrete Structures	Cu. Yd.	40.2		
Reinforcement Bars, Epoxy Coated	Pound	6,590		
Cofferdam (Type 2) (Location-1)	Each	1		
Cofferdam (Type 2) (Location-2)	Each	1		
Geocomposite Wall Drain	Sq. Yd.	39		

- Notes:**
- See Note 1. on Estimated Quantity and General Notes sheet regarding payment for pay item "Cofferdam (Type 2)".
 - Based on preliminary information from City of Naperville, Department of Public Utility Waste Water Division, top of existing 16" watermain is at EL. 633.00 with 1'-0" concrete encasement around and above it. This information is subject to variance. Verify location and elevation of existing watermain before ordering reinforcement steel and designing and erecting cofferdam. No consideration for extra payment will be made for accommodating this existing watermain.
 - The estimated Cofferdam Excavation quantity shown is based on the boring data and maybe modified by the Engineer for variable subsurface conditions encountered in the field. "Cofferdam Excavation" is estimated to include 23.7 cubic yards in weathered bedrock. Any additional excavation will also be paid for as Cofferdam Excavation. Any additional excavated portion below the bottom of proposed footing as shown shall be backfilled with Concrete and will be paid by Concrete Structures.

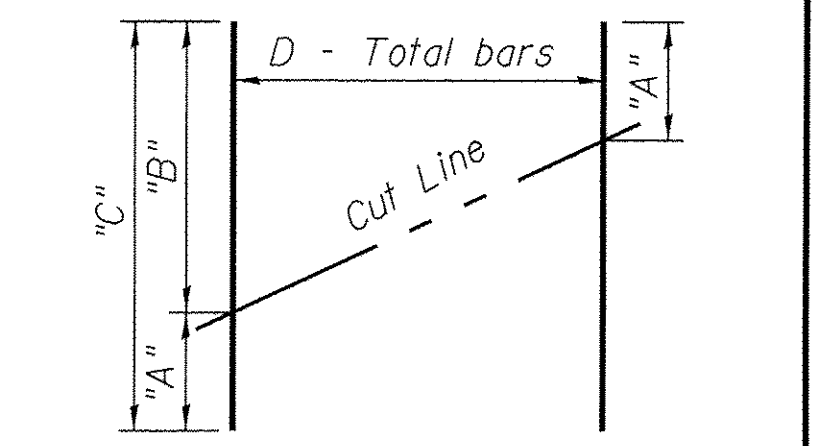
ELEVATION
(Looking West)

Existing 1'-0" conc. encasement
Existing 16" watermain. Top of pipe EL. 634.00. See note.
6"x6" Corner fillet (Typ.)
7-#7 n1(E) bars @ 11" cts. B.F.



PLAN

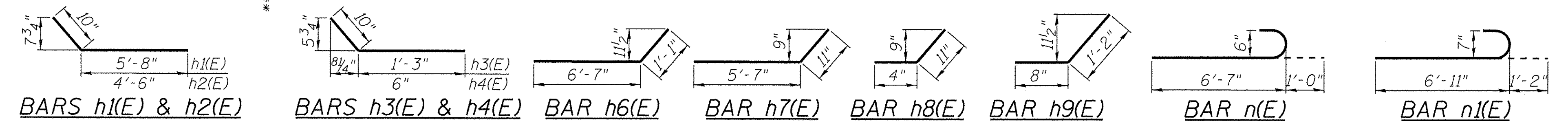
- See Field Cutting Diagram "Cofferdam Excavation" includes approximately 23.7 cubic yards of rock excavation.



FIELD CUTTING DIAGRAM

Order bars full length. Cut as shown and use remainder of bars in opposite end.

Bar	A	B	C	D
t4(E)	11"	3'-4"	4'-3"	3
v(E)	9'-5"	14'-1"	23'-6"	18
v4(E)	7'-7"	13'-0"	20'-7"	15
w5(E)	9'-1"	10'-1"	19'-2"	6

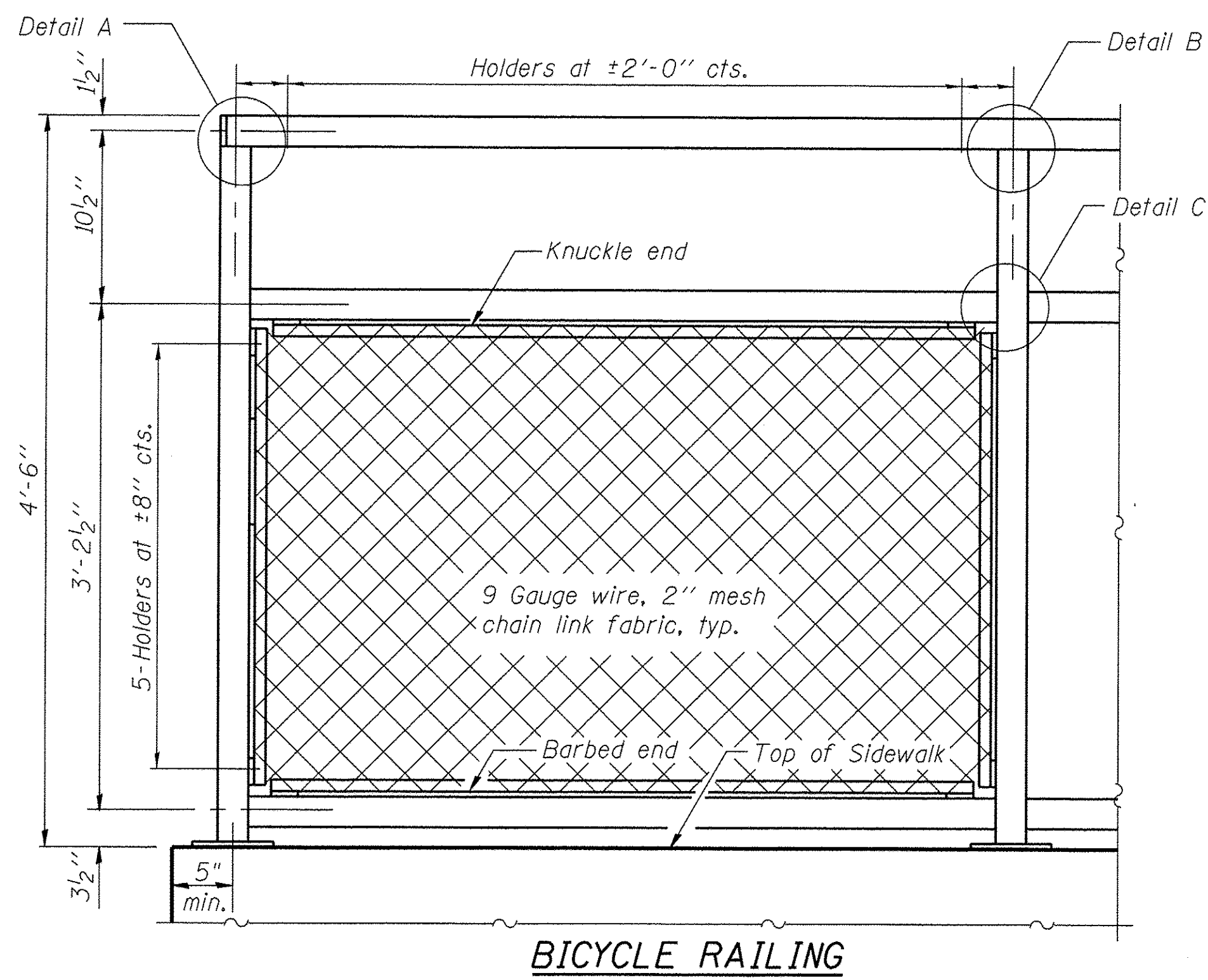


Cost of pipe sleeve is included with unit price of "Concrete Structures".
* Cut bars as necessary to fit.
**Elevations are taken from 1994 bridge rehabilitation plans. Contractor shall verify existing structure elevations prior to new construction.
***Epoxy grout bars d(E), d2(E), v7(E), w1(E), w4(E) and bars d1(E) in minimum 8", deep drilled holes. The material and installation shall be in accordance with Standard Specification Section 584.
See sheet West Abutment Details for Details A & B

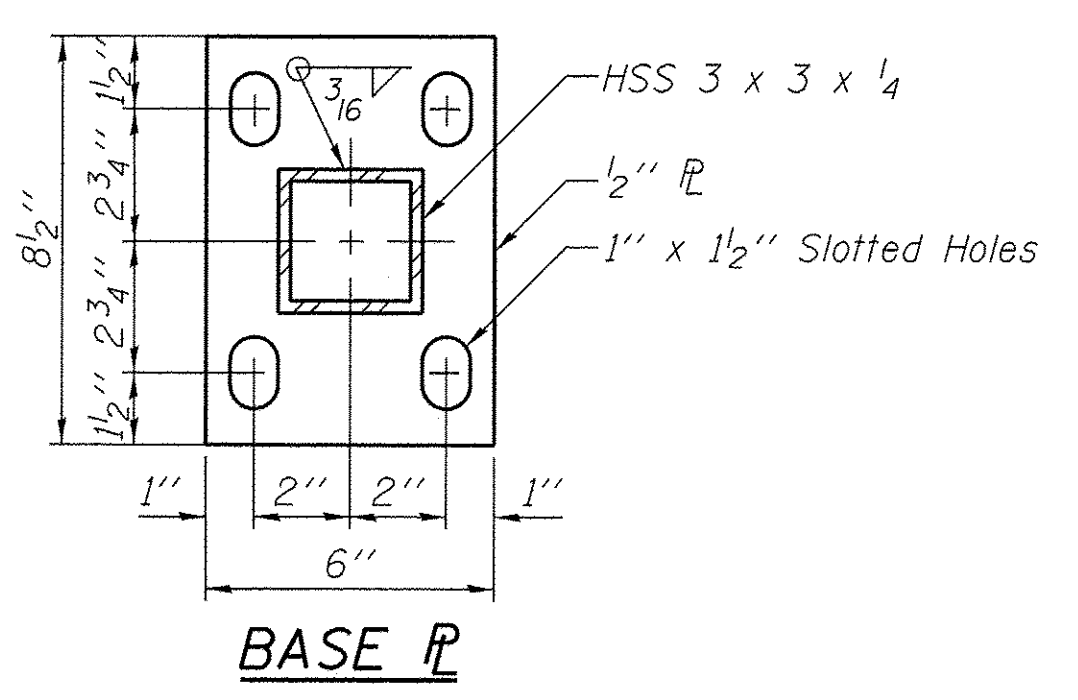
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<p>100 S. WACKER DR. SUITE 500 CHICAGO IL 60606 TEL (312) 939-1000 FAX (312) 939-4198</p>	USER NAME = STB	DESIGNED - STB	REVISED -	S01	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	<p align="center">WEST ABUTMENT STRUCTURE NO. 099-3105</p>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 2/8" = 1' / 1"	CHECKED - NPP	REVISED -				1644	12-00181-01-FP	WILL	179	108
	PLOT DATE = 1/21/2016	DRAWN - SOI	REVISED -				CONTRACT NO. 61A30				
		CHECKED - NPP	REVISED -				ILLINOIS FED. AID PROJECT				

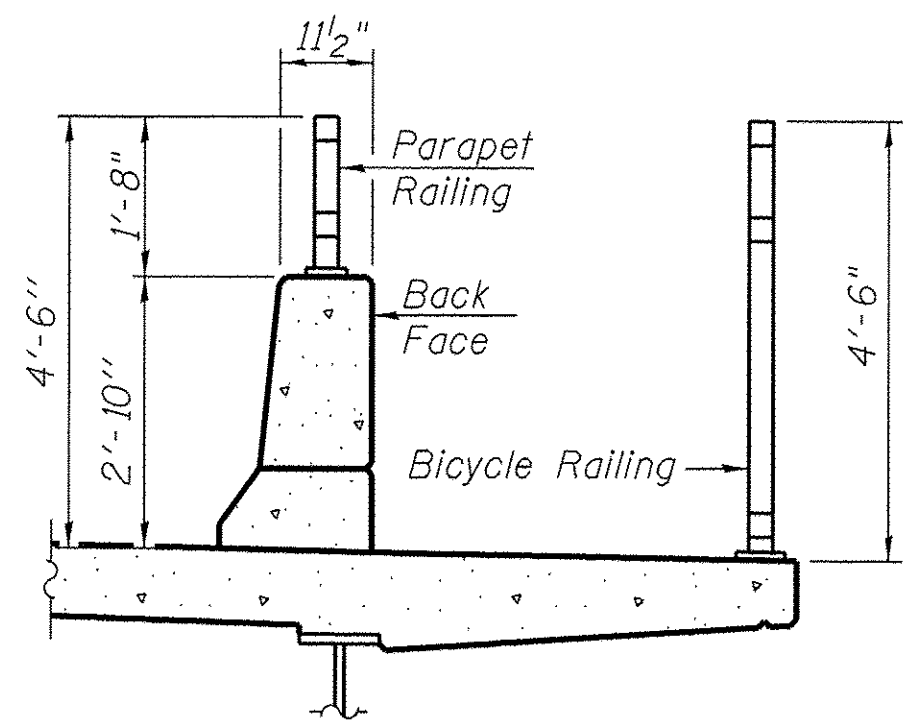
SHEET NO. 16 OF 19 SHEETS



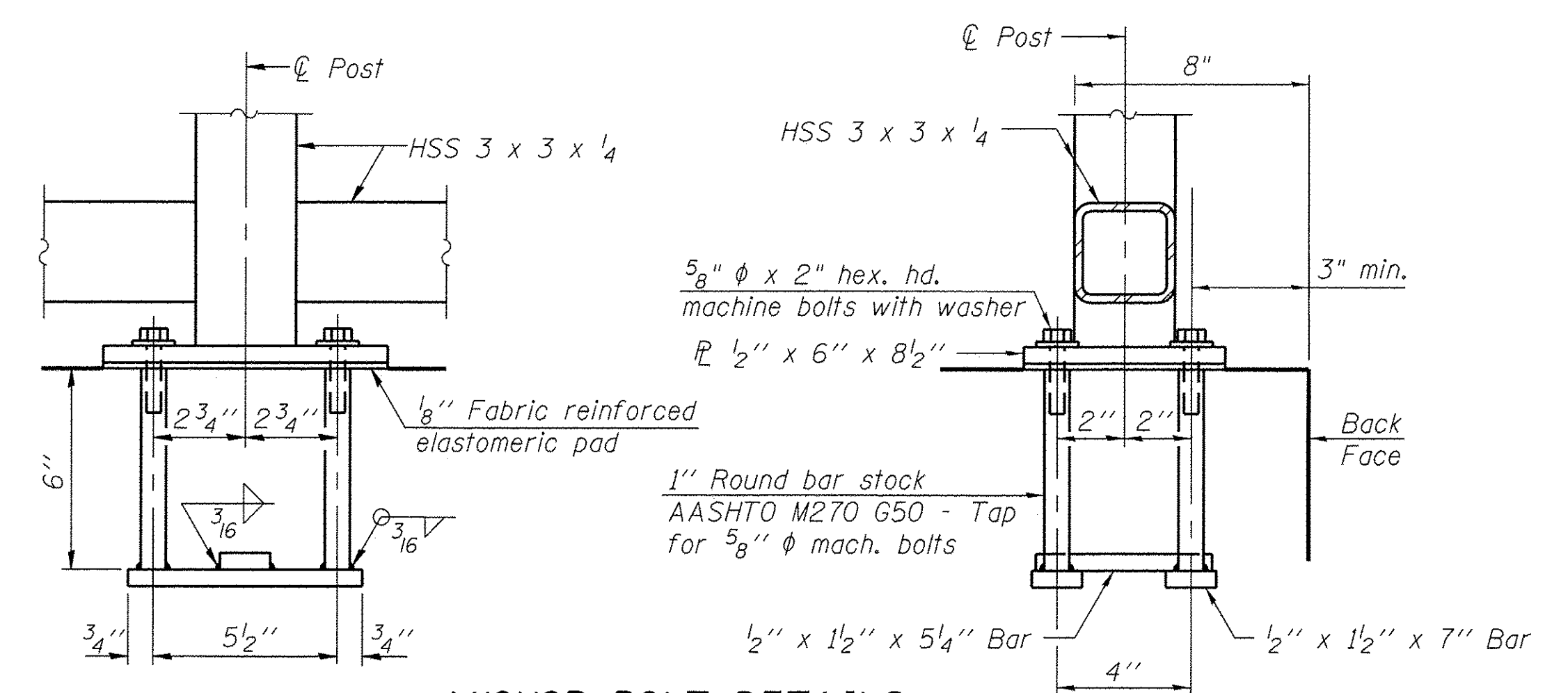
BICYCLE RAILING



BASE PL

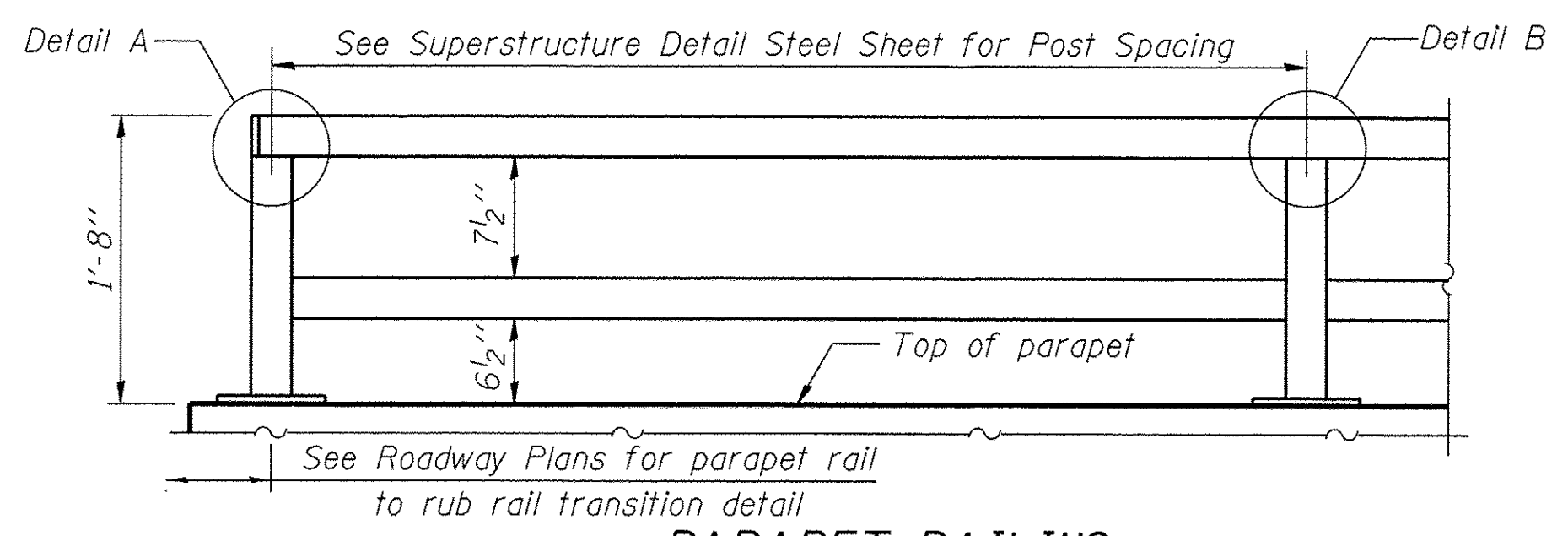


SECTION THRU DECK

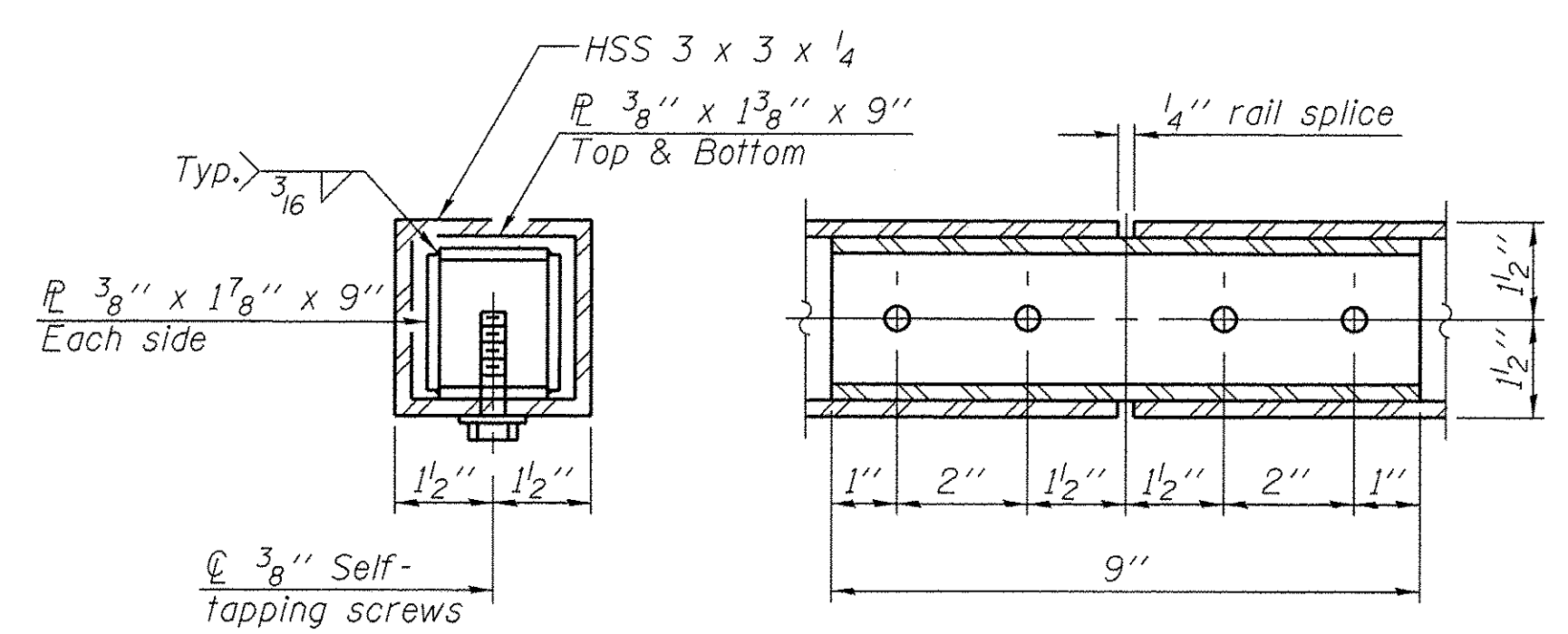


ANCHOR BOLT DETAILS

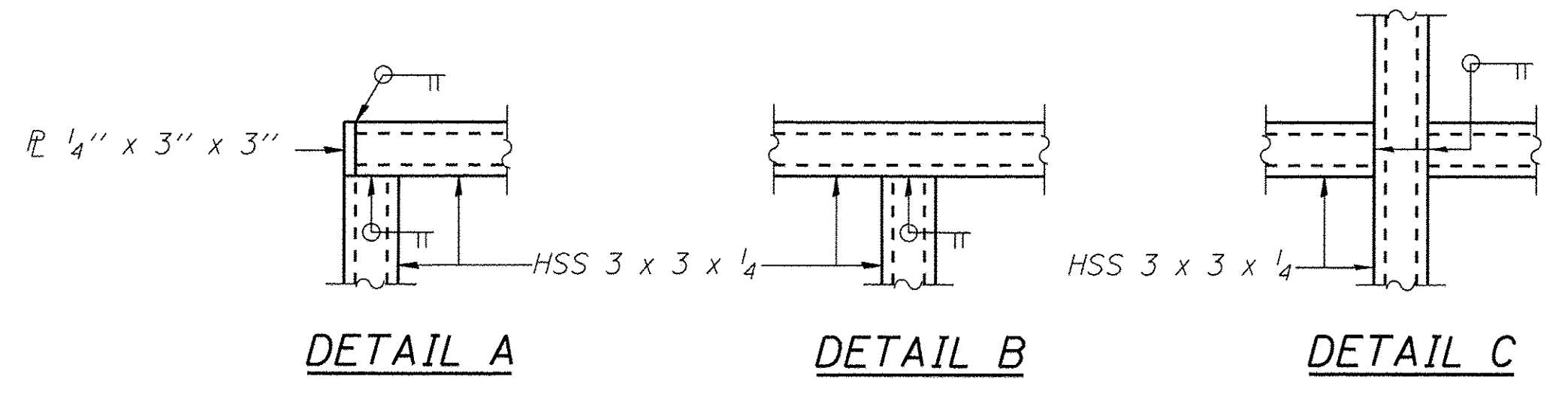
In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" ϕ anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



PARAPET RAILING ELEVATION
(Inside Face of Two Element Rail)



RAIL SPLICE

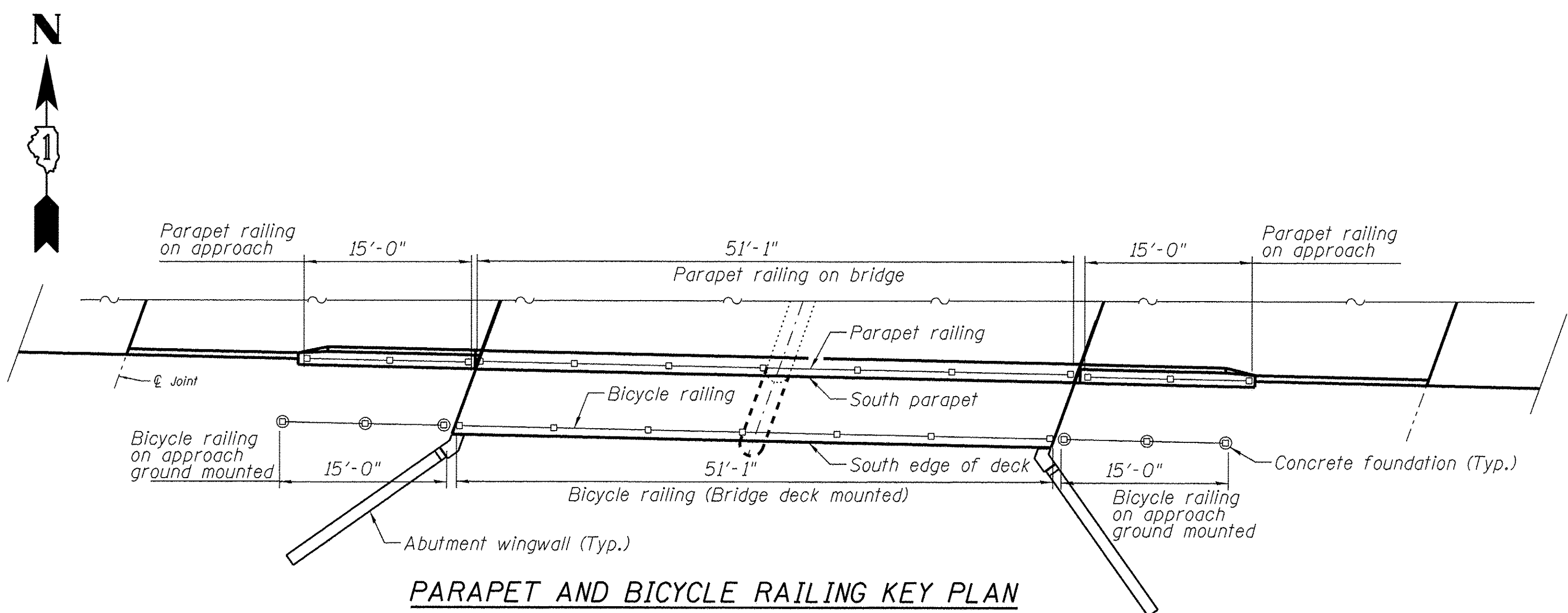


DETAIL A

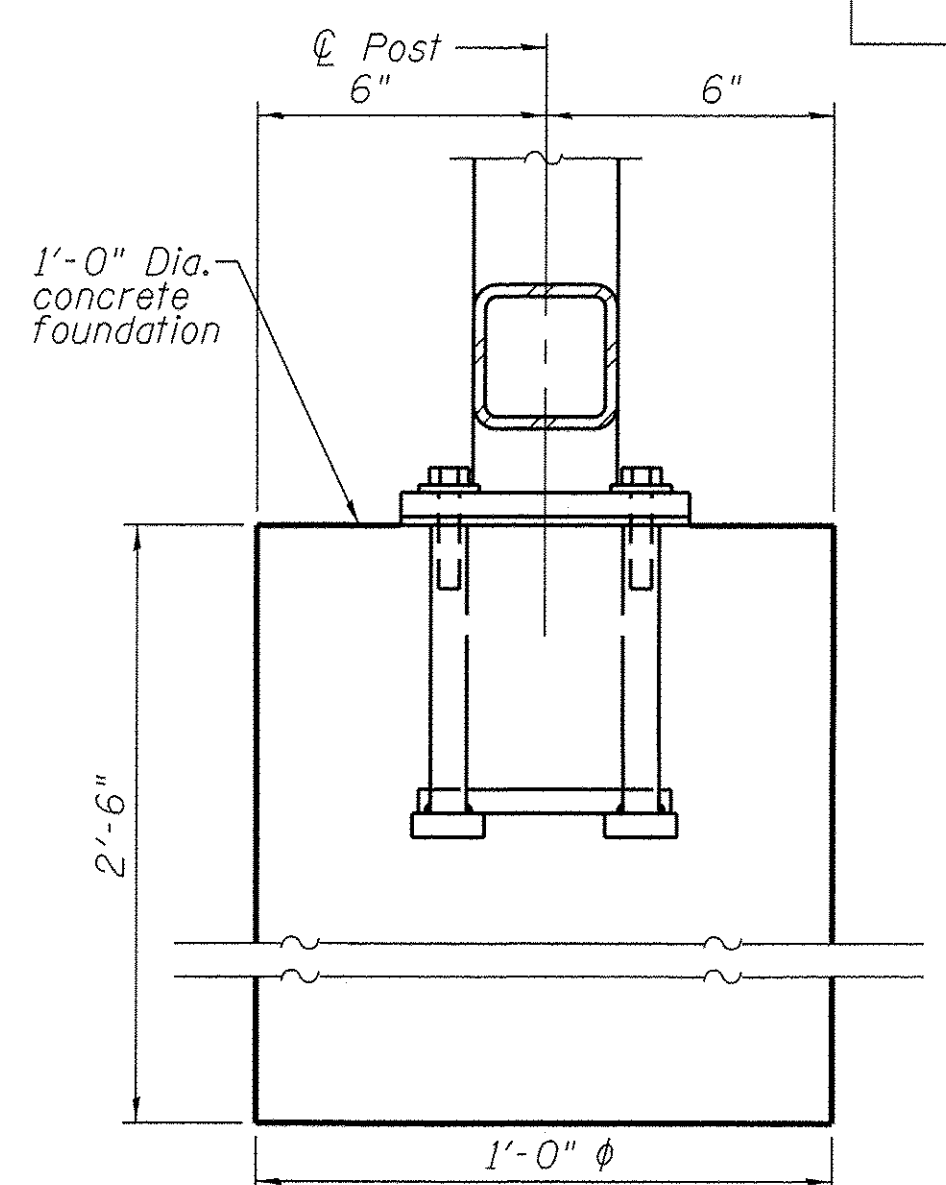
DETAIL B

DETAIL C

All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.



PARAPET AND BICYCLE RAILING KEY PLAN



ANCHOR BOLT DETAILS AT APPROACH

Information not shown is similar to Anchor Bolt Details

BILL OF MATERIAL

Item	Unit	Quantity
Bicycle Railing	Foot	81
Parapet Railing	Foot	81
Concrete Structures	Cu. Yd.	0.5

FILE NAME = p:\MCD\DL25367381\95\tdh\Drawings\Contract\North\Structures\Bridge (Spring Brook)\Control\Copied - Final Design\18 Bicycle Railing.dgn

URS
100 S. WACKER DR.
SUITE 500
CHICAGO, IL 60606
TEL (312) 939-1000
FAX (312) 939-4198

USER NAME = #STB#
DESIGNED - STB
CHECKED - NPP
DRAWN - SOI
CHECKED - NPP
PLOT SCALE = 0.4" = 1'-0"
PLOT DATE = 1/21/2016

DESIGNED - STB
CHECKED - NPP
DRAWN - SOI
CHECKED - NPP
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BICYCLE RAILING
STRUCTURE NO. 099-3105**

SHEET NO. 18 OF 19 SHEETS

F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 110
CONTRACT NO. 61A30				

ILLINOIS FED. AID PROJECT



Illinois Department of Transportation
Division of Highways
Applied GeoScience

SOIL BORING LOG

Page 1 of 1

Date 9/14/10

ROUTE FAU 1644 DESCRIPTION _____ LOGGED BY MG

SECTION 106B-TR LOCATION Near Springbrook Creek Bridge, SEC. 2, TWP. 37N, RNG. 9E, 3rd PM

COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE _____

STRUCT. NO.	D	B	U	M	Surface Water Elev.
Station	E	L	C	O	Stream Bed Elev.
BORING NO.	P	O	S	I	Groundwater Elev.:
Station	T	W	Qu	S	First Encounter
Offset	H	S		T	Upon Completion
Ground Surface Elev.	(ft)	(/6")	(tsf)	(%)	After _____ Hrs.
099-3105					_____ ft
SB-S01					_____ ft
295+20					631.0 ft ▼
55.00ft LT					_____ ft
641.83 ft					_____ ft
FILL- Silty clay with topsoil, sand, and gravel, brown	5				
	6		17.0		
	9				
	9				
	10		9.0		
	8				
	4				
	5	1.8	21.0		
636.33	8	B			
CLAY, with organic, black, medium stiff to soft	2				
	3	0.6	38.0		
	4	B			
	1				
	3	0.4	20.0		
632.43	14	B			
WEATHERED ROCK, with sand and gravel, brown-gray, dense to extremely dense	-10				
	11				
	16				
	27				
	14				
	49				
	100				
628.03					
627.63					
BEDROCK, fractured limestone, wet, auger and spoon refusal at 14.2 feet	-15				
End of Boring					

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)

BBS, from 137 (Rev. 8-99)

SB- S1

SOIL BORINGS
95TH STREET OVER SPRING BROOK
F.A.U. RTE. 1644
SEC. 106B-TR
WILL COUNTY
STATION 294+68.91
STRUCTURE NO. 099-3105

FILE NAME = pt:\CDDH\25367381\95th\Drawings\Contract\North\Structures\Bridges (Spring Brook)\Control Copy - Final Design\19-Soil Boring.dgn

URS
100 S. WACKER DR.
SUITE 500
CHICAGO IL 60606
TEL (312) 939-1000
FAX (312) 939-4198

USER NAME = #STB#	DESIGNED - STB	REVISED -
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PLLOT DATE = 1/21/2016	DRAWN - SOI	REVISED -
	CHECKED - NPP	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING
STRUCTURE NO. 099-3105

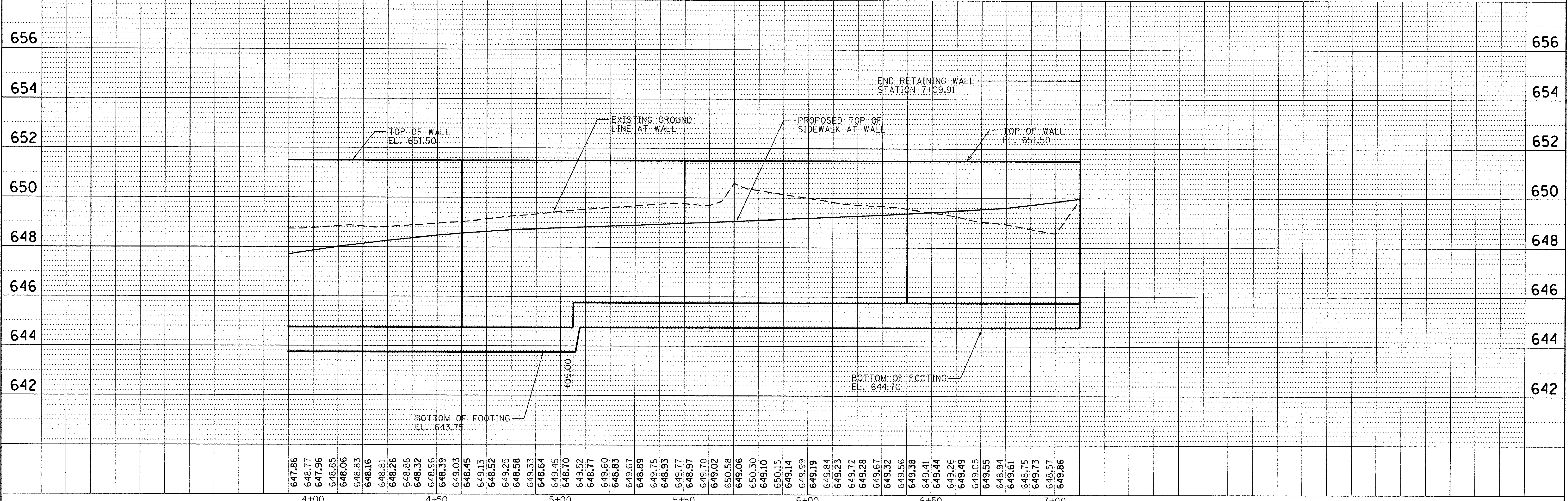
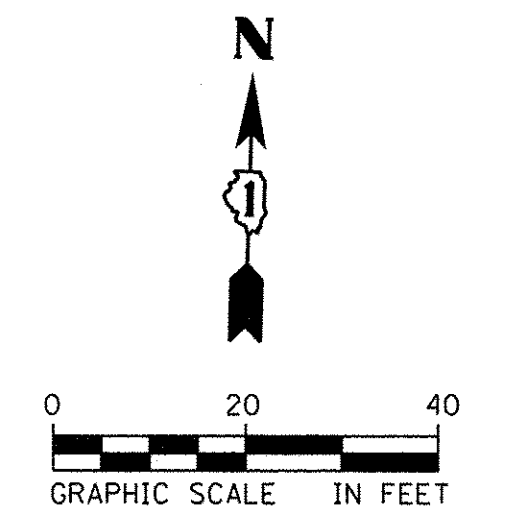
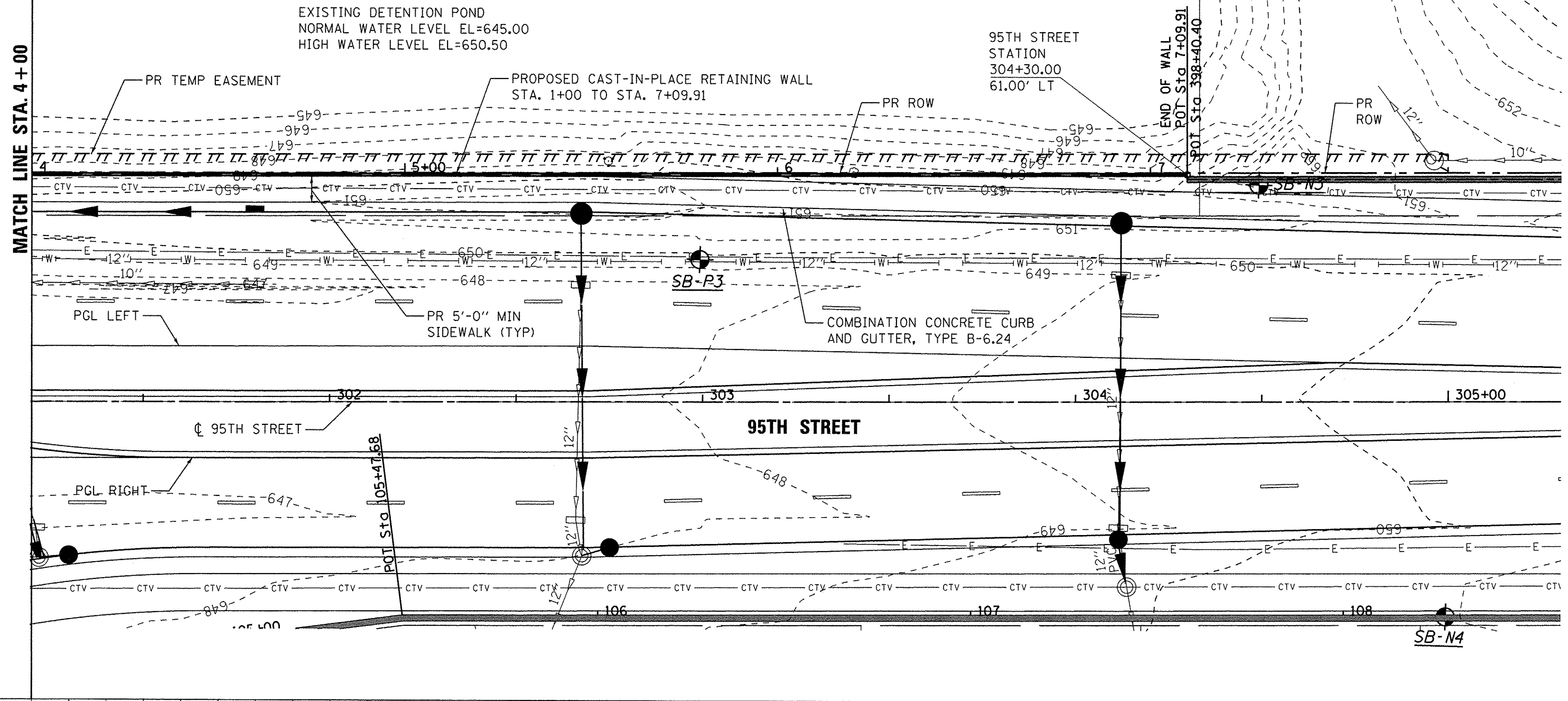
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1644	12-00181-01-FP	WILL	179	111
CONTRACT NO. 61A30				
ILLINOIS FED. AID PROJECT				

SHEET NO. 19 OF 19 SHEETS

PLAN	REVIEWED	DATE
	PLOTTED	
	ALIGNED	
	CHECKED	
	BY	
	NO.	
	FILE NAME	

PROFILE	REVIEWED	DATE
	PLOTTED	
	GRADES CHECKED	
	STRUCTURE NOTATIONS OK'D	
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	FILE NAME	

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4+00										4+50					5+00					5+50					6+00					6+50					7+00																											
DESIGNED - STB										REVISED										DESIGNED - STB										REVISED																																
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DATE - 01/20/2016										REVISED										DATE - 01/20/2016										REVISED																																

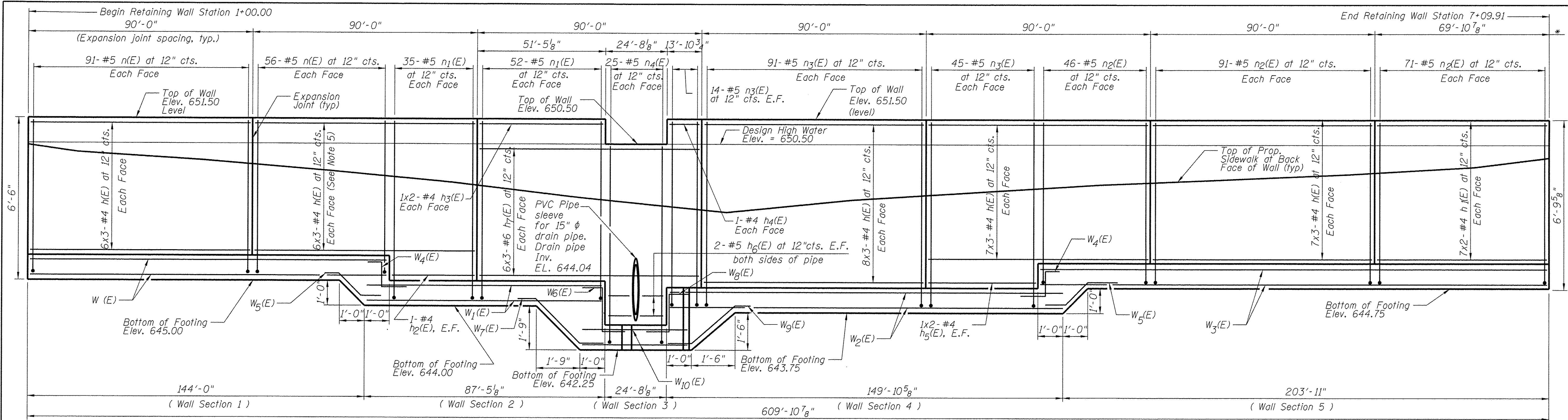


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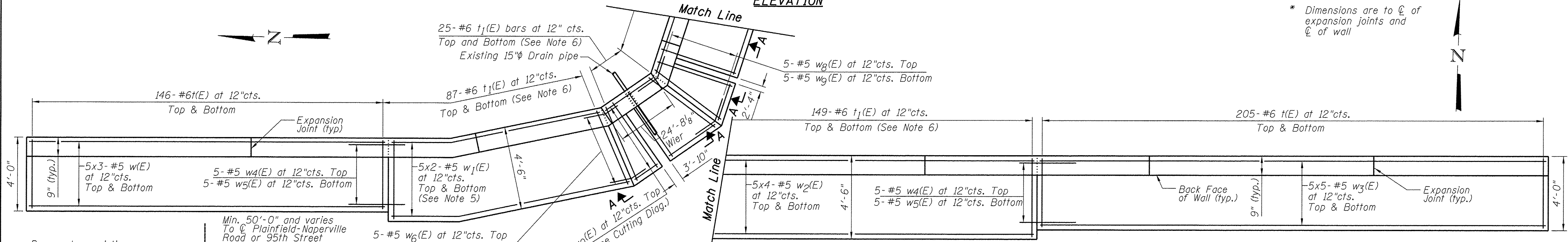
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DETENTION POND RETAINING WALL
 PLAN & PROFILE - SHEET 2
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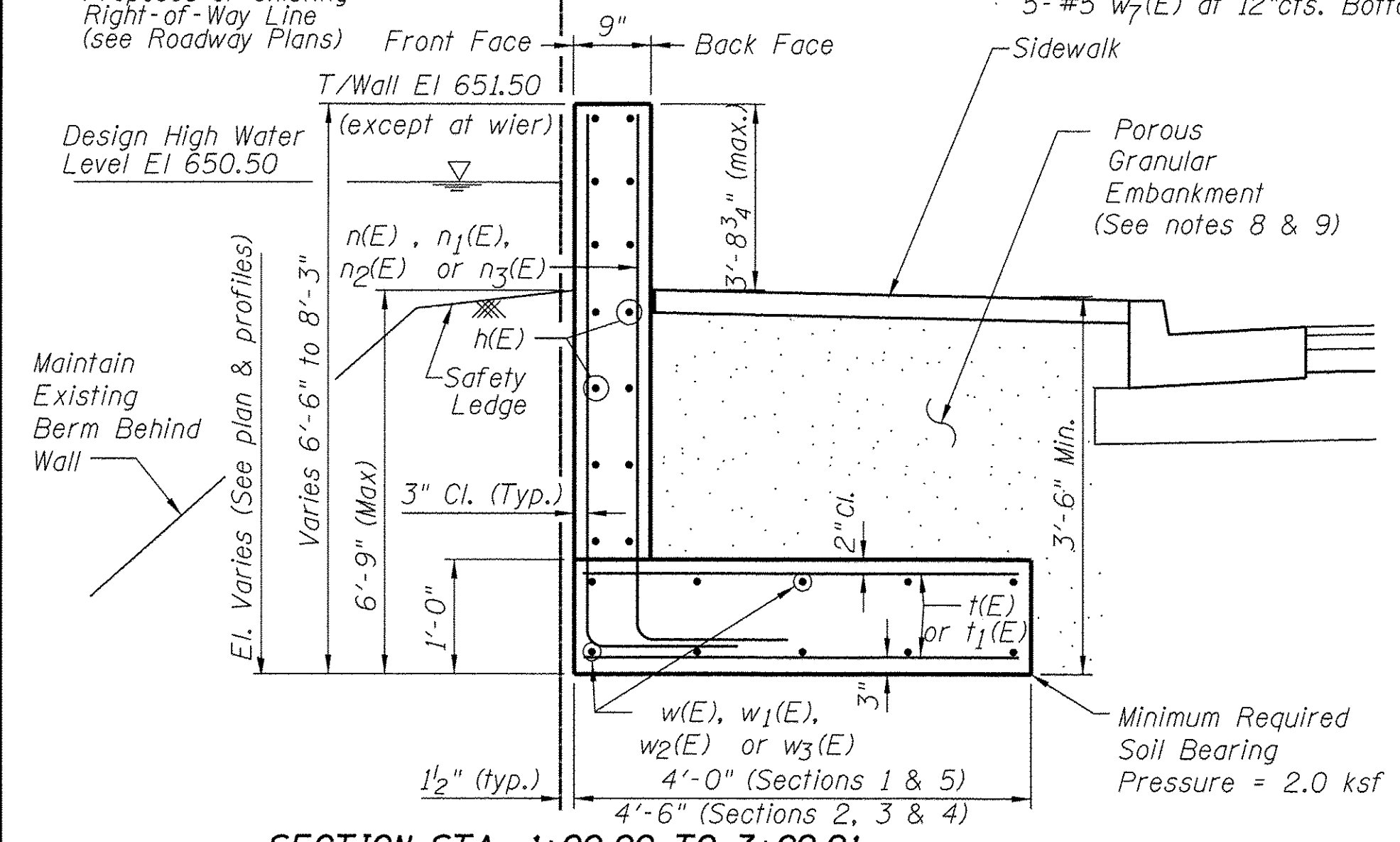
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CONTRACT NO. 61A30				
ILLINOIS FED. AID PROJECT				



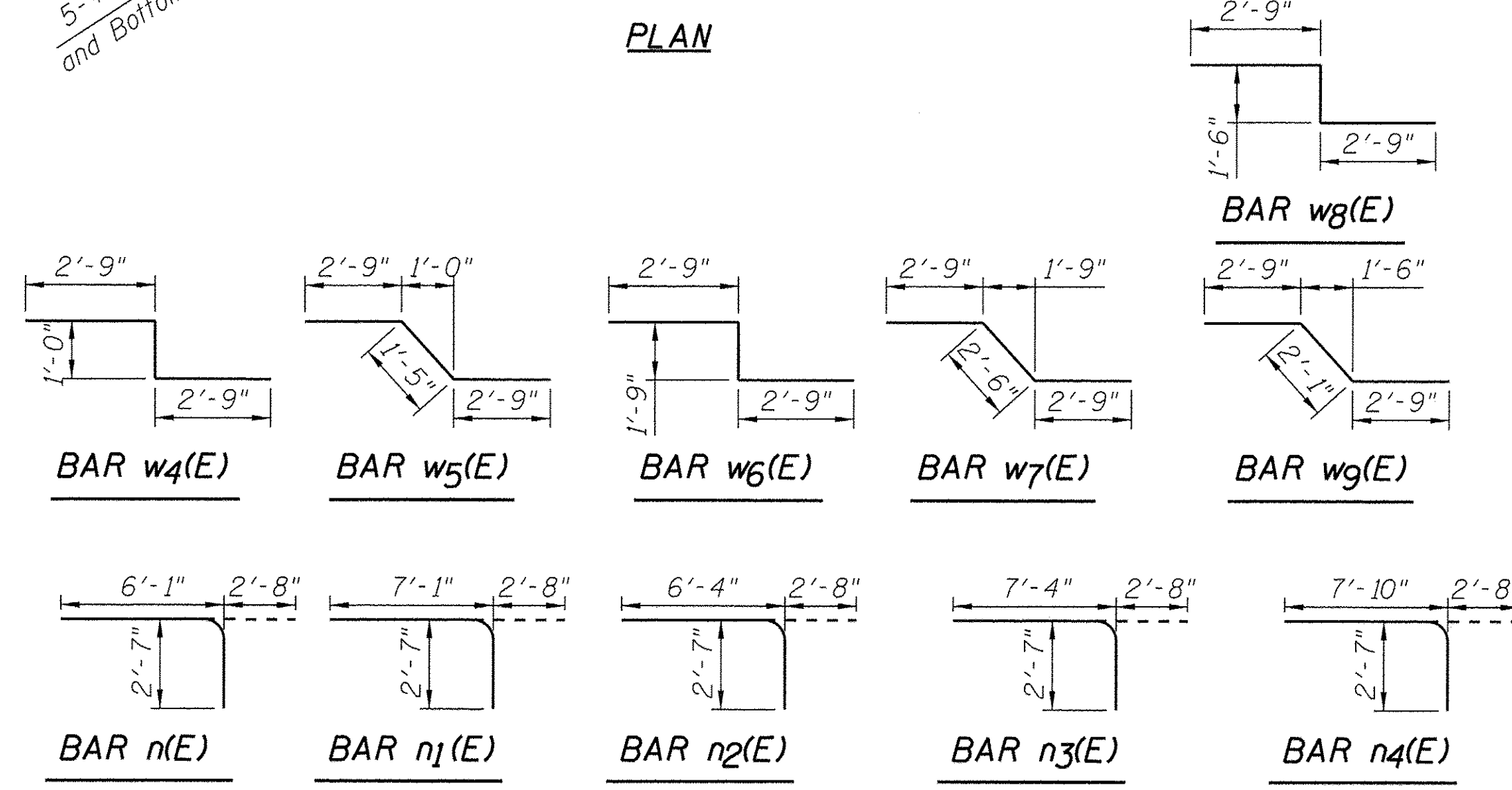
ELEVATION



PLAN



SECTION STA. 1+00.00 TO 7+09.91



NOTES:

- Reinforcement bars designated (E) shall be epoxy coated.
- Concrete clear cover shall be 2" minimum unless noted otherwise.
- All edges shall have standard 3/4" chamfers except as noted.
- Min. lap for #4 bar = 2'-1". Min. lap for #5 bar = 2'-7".
- Bend bars in field as necessary to follow alignment of wall.
- Fan bars as required.
- Bars indicated thus 6x3-#4 ect. indicates 6 lines of bars with 3 lengths per line.
- Excavation for placing Porous Granular Embankment is included in cost of Structure Excavation. Porous Granular Embankment shall be compacted to 95% Modified Proctor Density and its coarse aggregate gradation shall be CA 18.
- Any additional area excavated beyond pay limits for Structure Excavation shall also be backfilled with Porous Granular Embankment. Furnishing and placing of this portion of that material will not be measured for payment.
- Construction joint spacing shall be maximum 30'-0" cts.
- Caulk the space between the drain pipe and pipe sleeve.
- See sheet "DETENTION POND RETAINING WALL DETAILS 2" for section A-A

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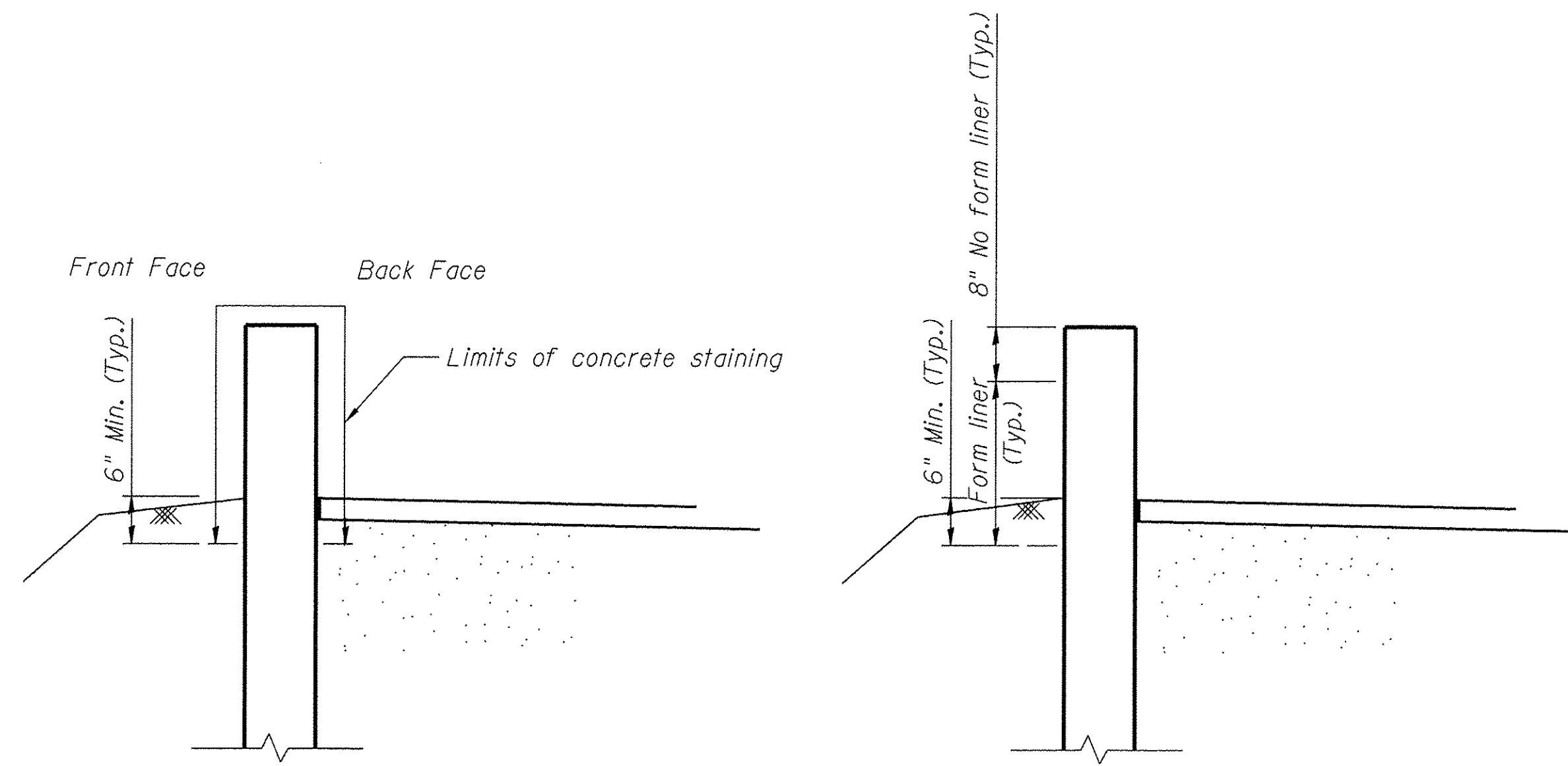
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

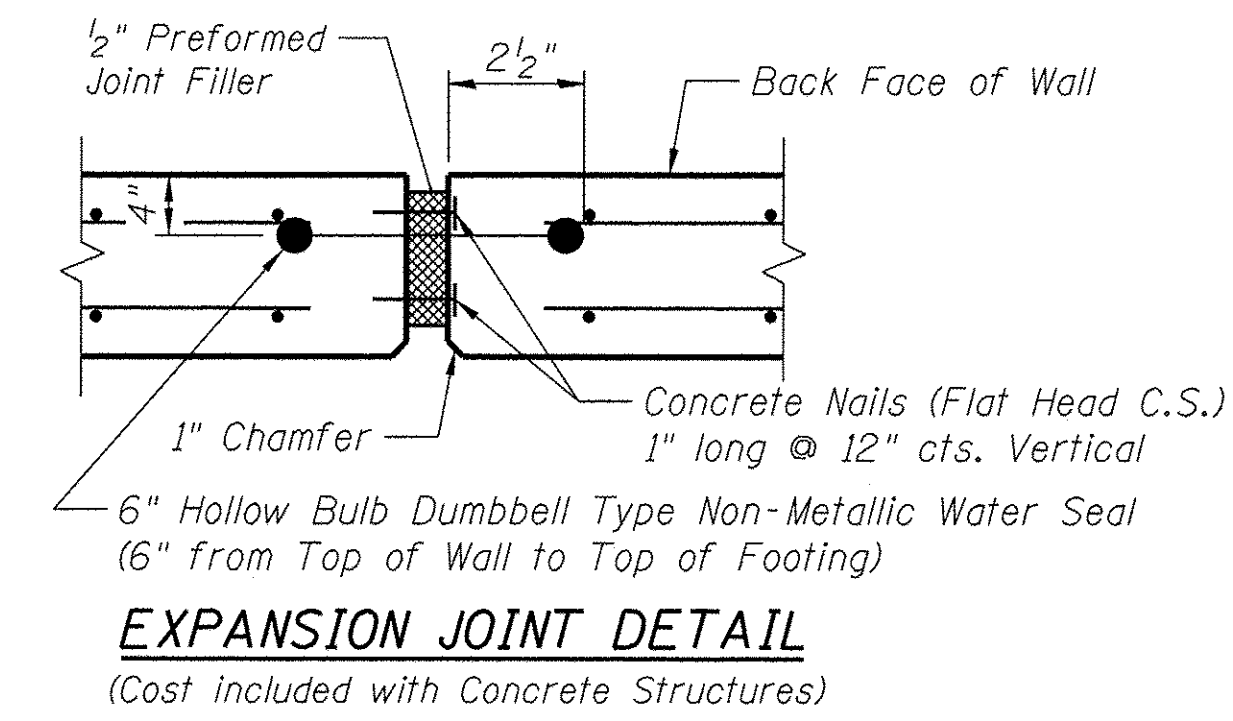
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DETAILS 1**

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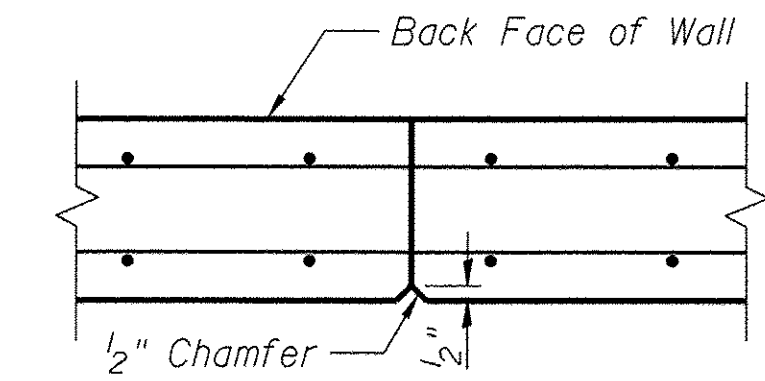
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CONTRACT NO. 61A30				
ILLINOIS FED. AID PROJECT				



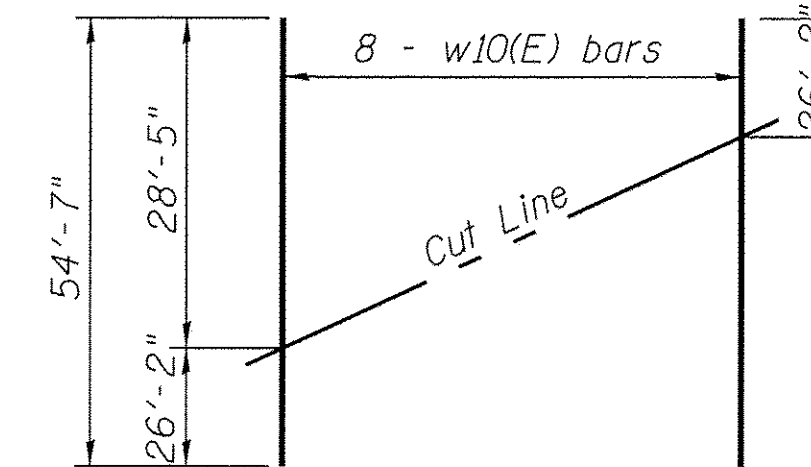
LIMITS OF FORM LINER AND CONCRETE STAINING



EXPANSION JOINT DETAIL
(Cost included with Concrete Structures)



CONSTRUCTION JOINT DETAIL



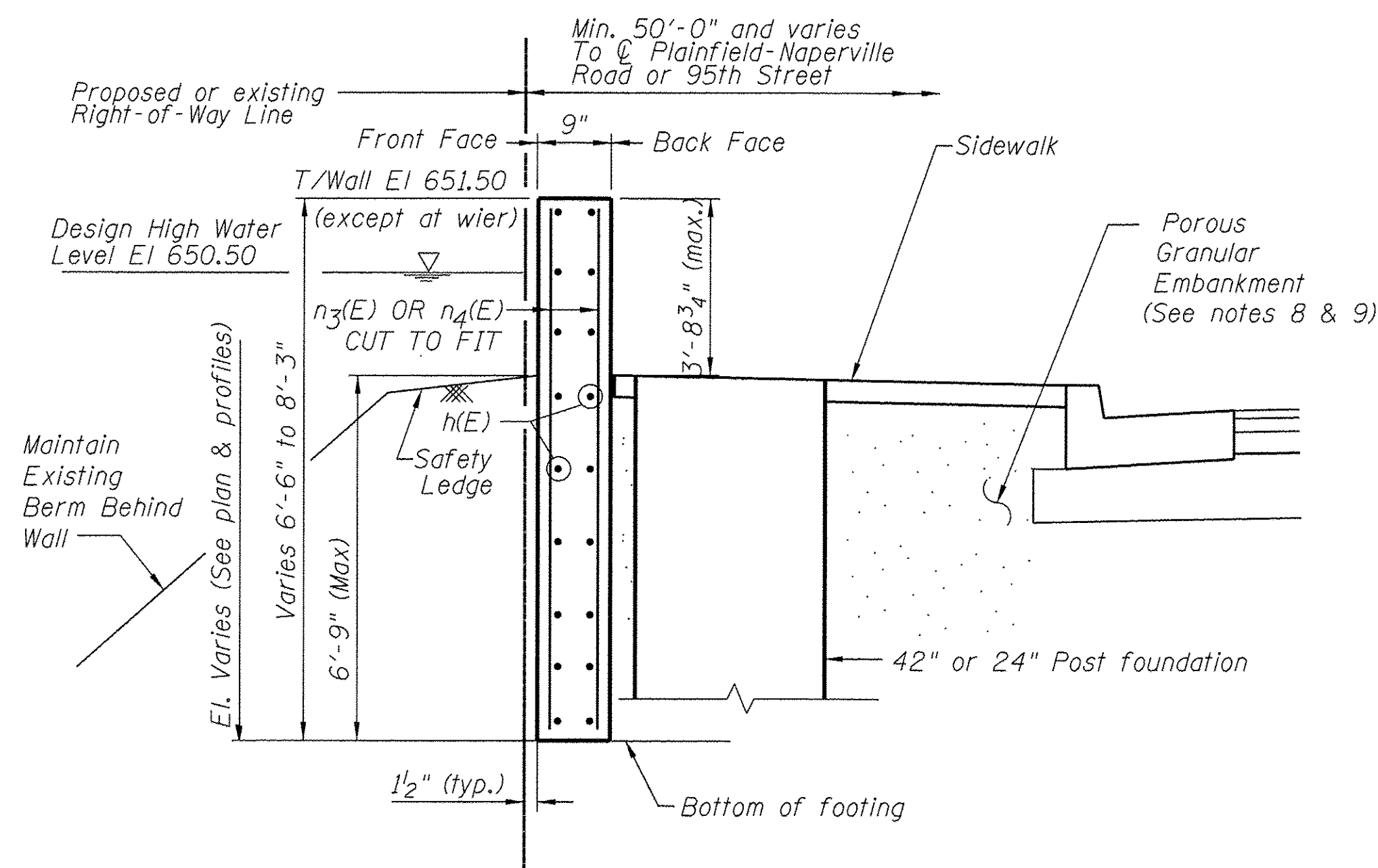
FIELD CUTTING DIAGRAM

Order bars w10(E) full length. Cut as shown and use remainder of bars in opposite face.

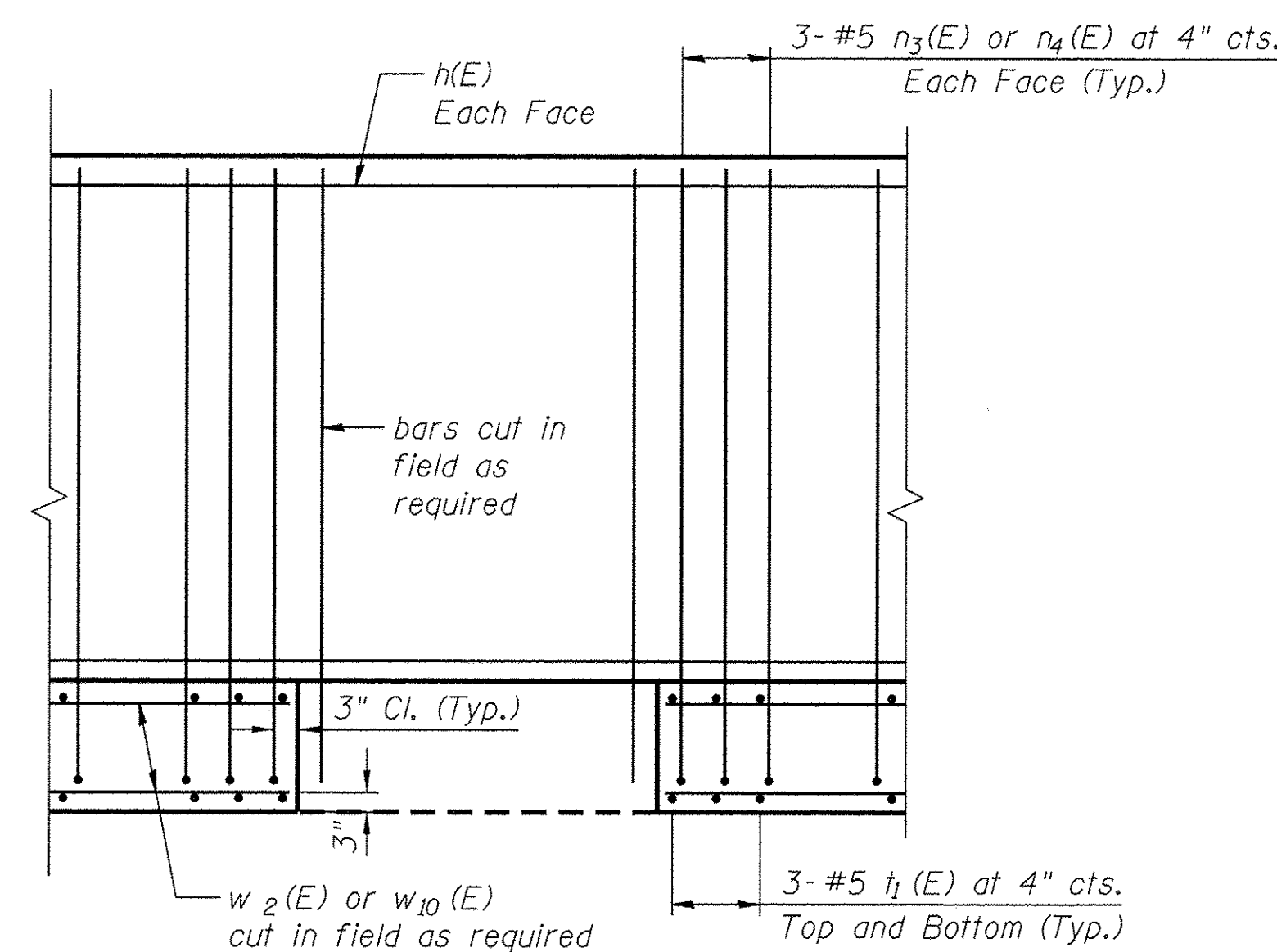
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
n (E)	204	#4	31'-4"	—
n1 (E)	28	#4	35'-10"	—
n2 (E)	2	#4	34'-8"	—
n3 (E)	4	#4	26'-7"	—
n4 (E)	2	#4	13'-6"	—
n5 (E)	4	#4	23'-5"	—
n6 (E)	8	#4	10'-9"	—
n7 (E)	36	#6	32'-0"	—
n (E)	294	#5	8'-9"	┘
n1 (E)	174	#5	9'-9"	┘
n2 (E)	416	#5	9'-0"	┘
n3 (E)	308	#5	10'-0"	┘
n4 (E)	58	#5	10'-6"	┘
t (E)	702	#5	3'-8"	—
t1 (E)	528	#5	4'-2"	—
w (E)	30	#5	50'-0"	—
w1 (E)	20	#5	44'-10"	—
w2 (E)	40	#5	39'-7"	—
w3 (E)	50	#5	43'-0"	—
w4 (E)	10	#5	6'-6"	┘
w5 (E)	10	#5	6'-11"	┘
w6 (E)	5	#5	7'-3"	┘
w7 (E)	5	#5	8'-0"	┘
w8 (E)	5	#5	7'-0"	┘
w9 (E)	5	#5	7'-7"	┘
w10 (E)	5	#5	54'-7"	┘
Structure Excavation		Cu Yd	967	
Concrete Structures		Cu Yd	200	
Form Liner Textured Surface		Sq Ft	3,582	
Reinforcement Bars, Epoxy Coated		Pound	27,230	
Porous Granular Embankment, Special		Cu Yd	294	
Staining Concrete Structures		Sq Ft	4,872	

*See Field Cutting Diagram



SECTION AT POST FOUNDATION



SECTION A-A

See sheet "DETENTION POND RETAINING WALL DETAILS 1" for location of Section A-A

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

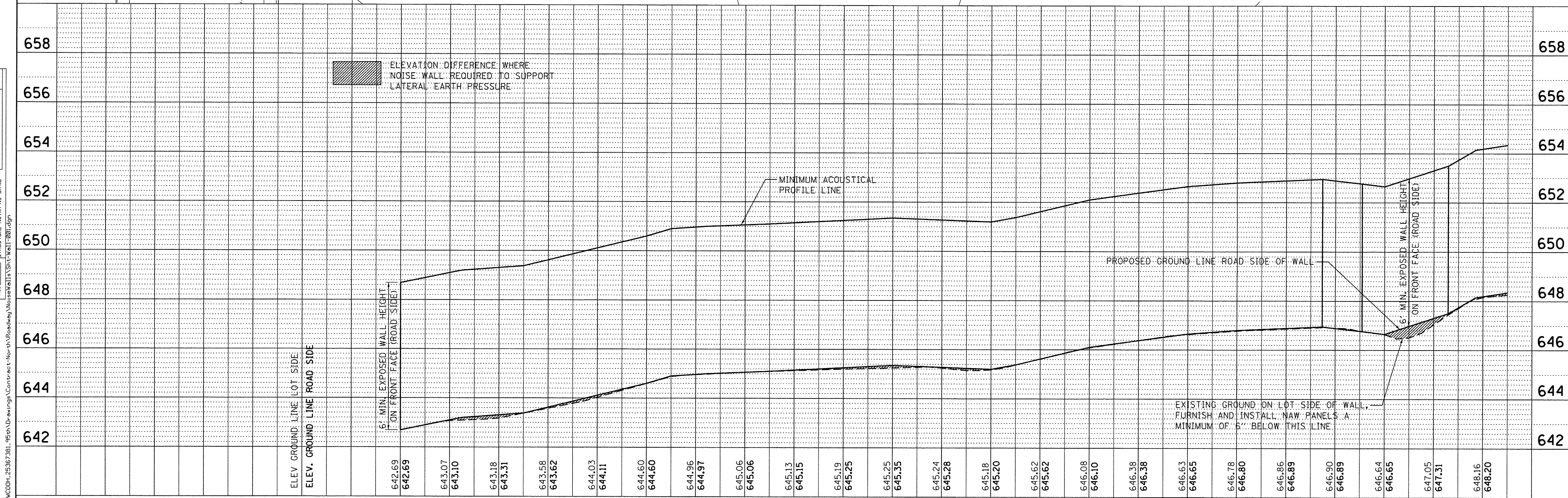
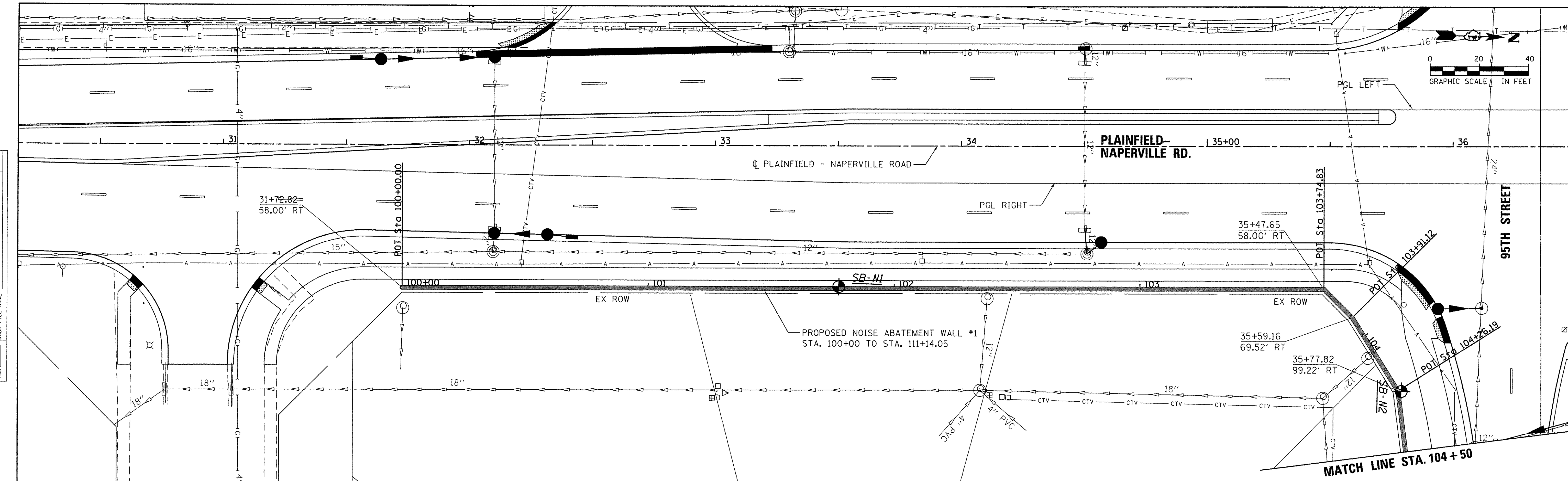
**DETENTION POND RETAINING WALL
DETAILS 2**

SCALE: DRAWING NO. 4 OF 4 STA. TO STA.

F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 115
CONTRACT NO. 61A30			ILLINOIS FED. AID PROJECT	

PLAN	SURVEYED	DATE
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	BY	

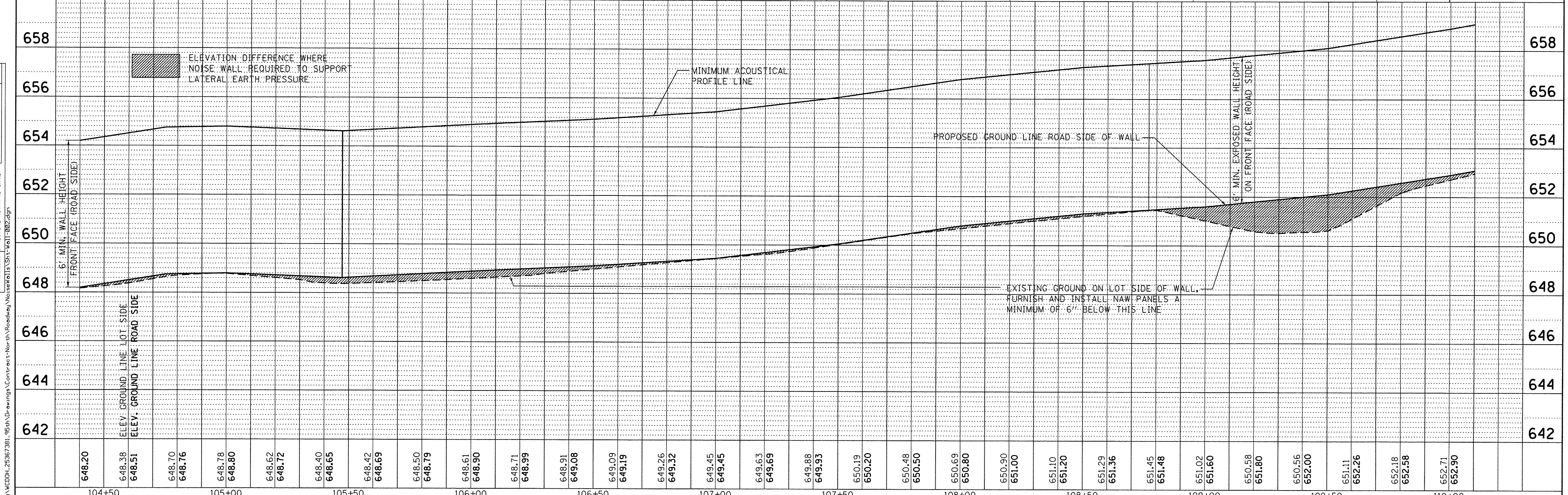
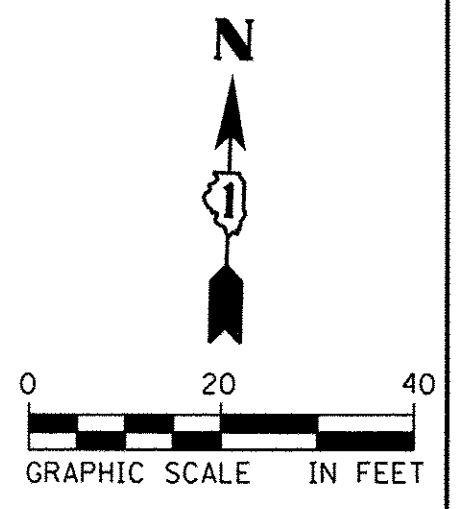
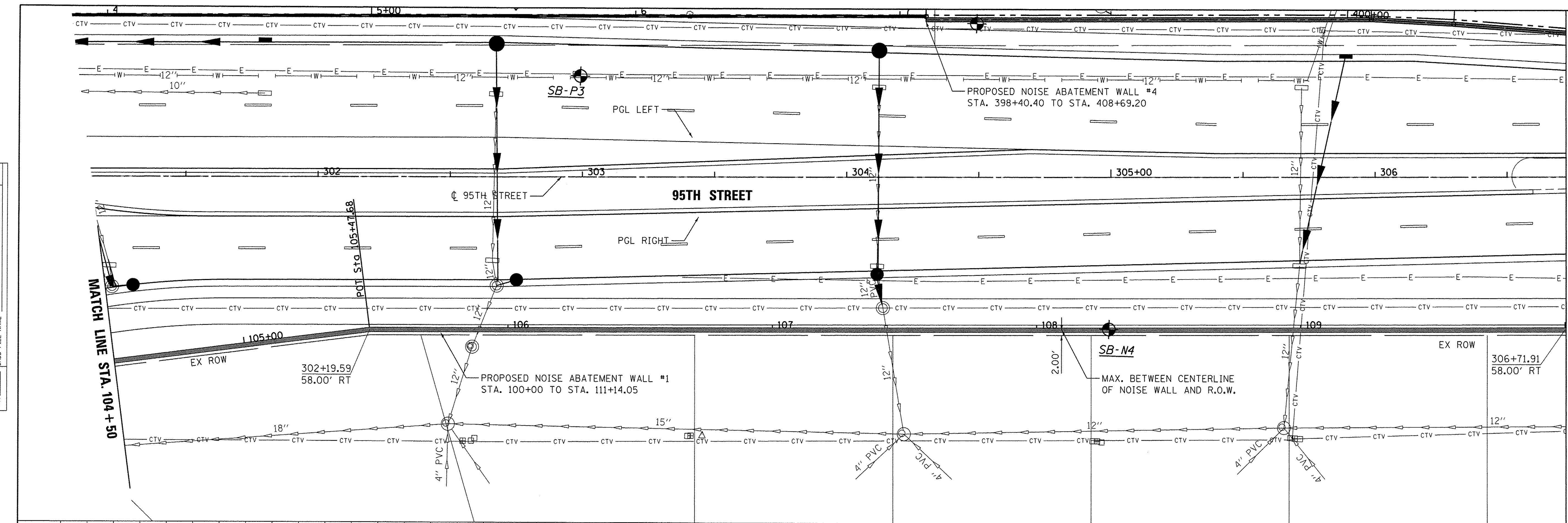
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	PLOTTED	
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	CHECKED	
	NO. /	
	BY	



URS 100 S. WACKER DR., SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME = james_push	DESIGNED - KJB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	NOISE ABATEMENT WALL #1-1 PLAN AND PROFILE		F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 117	
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	PLOT DATE = 1/21/2016	DATE - 11/22/2013	REVISED -									

PLAN	SURVEYED	DATE
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	ALIGNED	
	CHECKED	
	NO. _____	
	NOTE BOOK	
	NO. _____	
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PROFILE	SURVEYED	DATE
	PLOTTED	BY
	ALIGNED	
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	NOTE BOOK	
	NO. _____	
	STRUCTURE NOTATIONS CHKD	



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 100 S. WACKER DR.
 SUITE 500
 CHICAGO, IL 60606
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 FAX (312) 939-4198

USER NAME = james.push	DESIGNED - KJB	REVISED -
DRAWN - KJB	REVISED -	
CHECKED - NPP	REVISED -	
DATE - 11/22/2013	REVISED -	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**NOISE ABATEMENT WALL #1-2
 PLAN AND PROFILE**

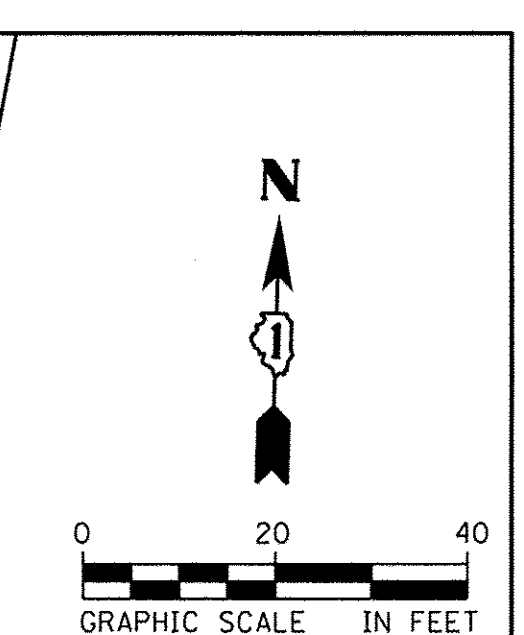
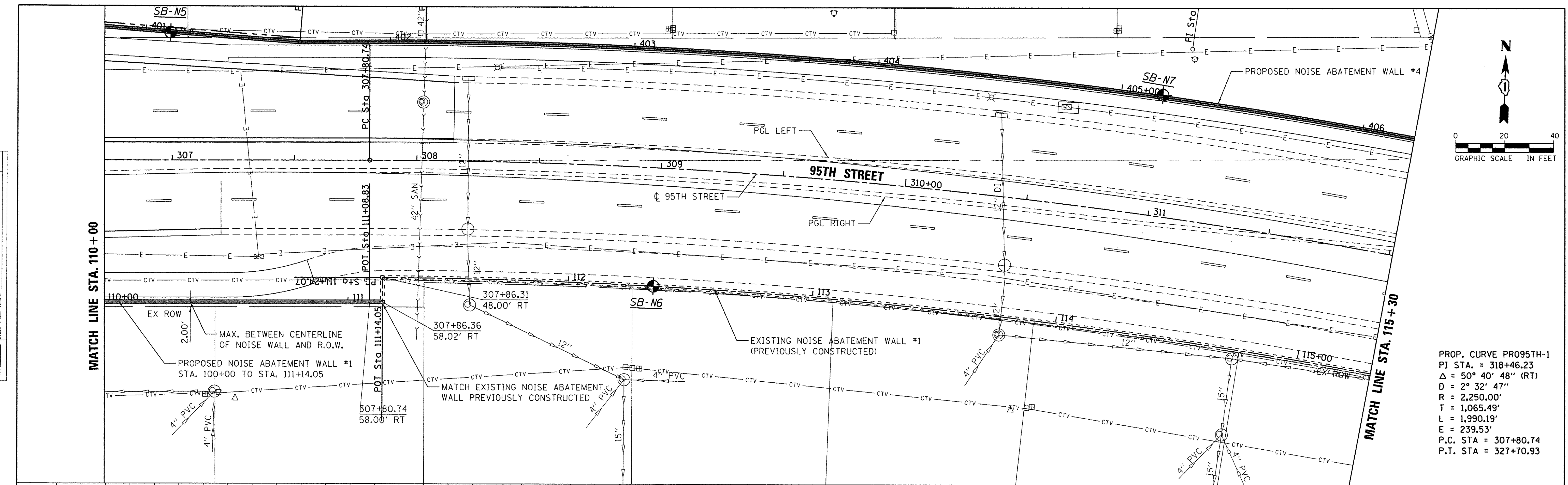
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61A30				
ILLINOIS FED. AID PROJECT				

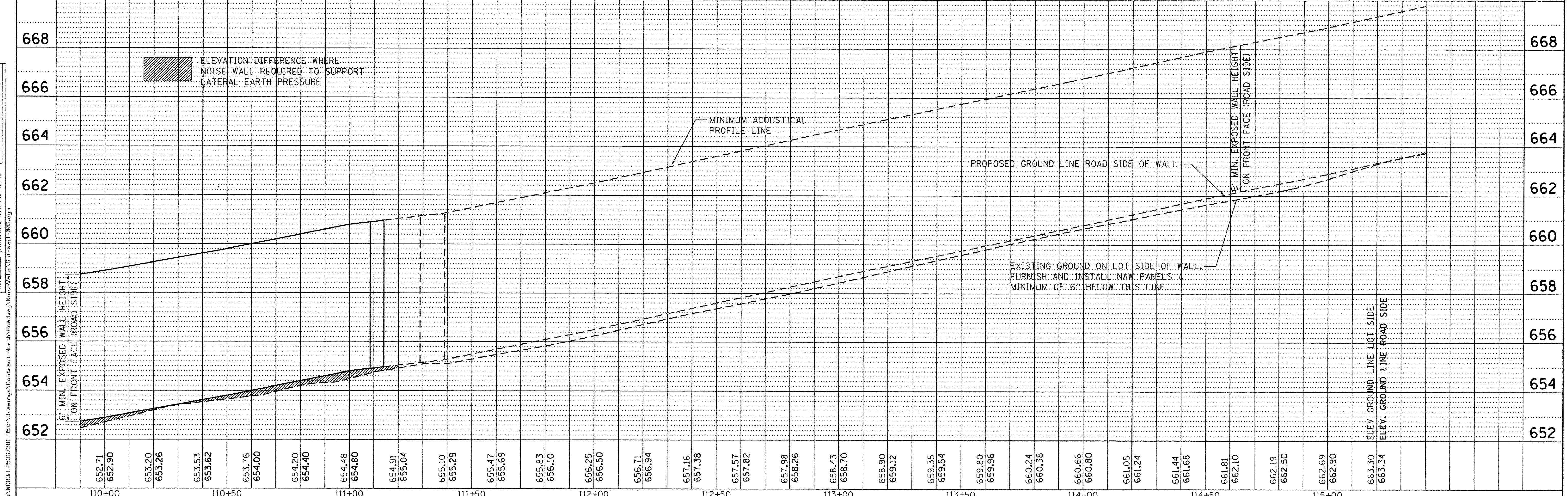
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PROFILE	SURVEYED	DATE
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	NOTATION	
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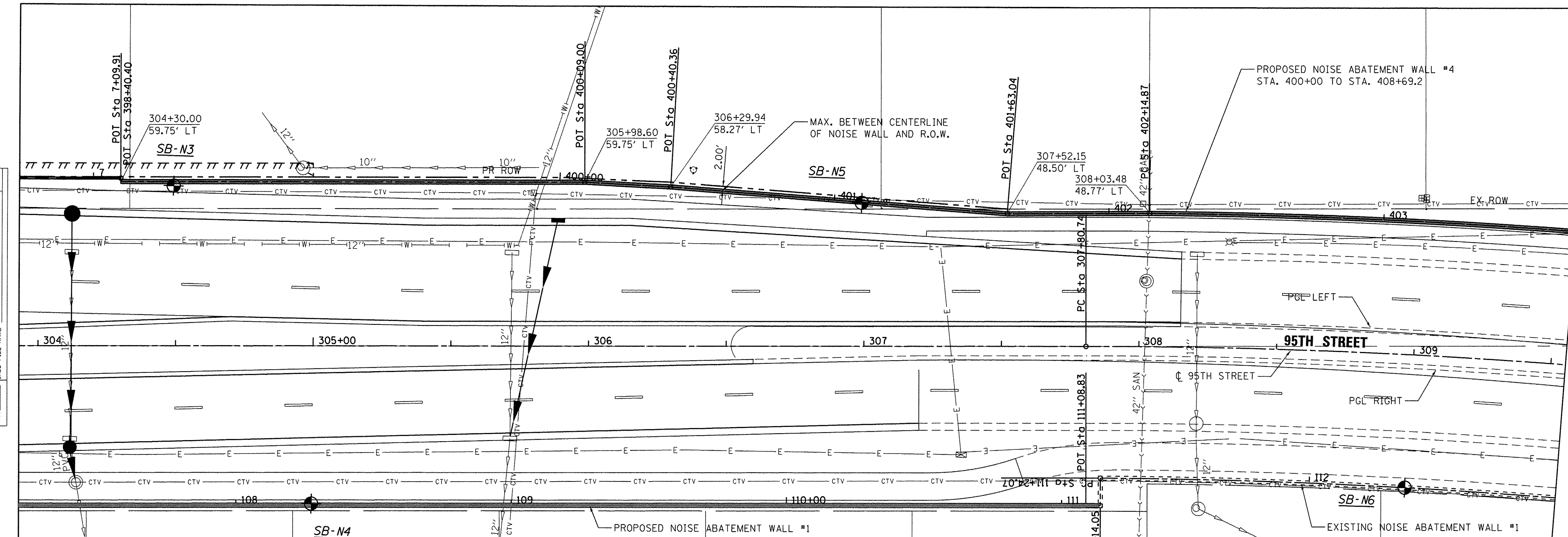
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 PI STA. = 318+46.23
 $\Delta = 50^\circ 40' 48''$ (RT)
 $D = 2^\circ 32' 47''$
 $R = 2,250.00'$
 $T = 1,065.49'$
 $L = 1,990.19'$
 $E = 239.53'$
 P.C. STA = 307+80.74
 P.T. STA = 327+70.93



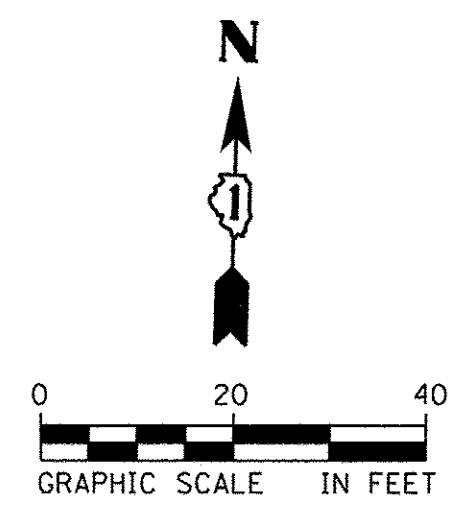
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PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
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	NO.	
	NO.	

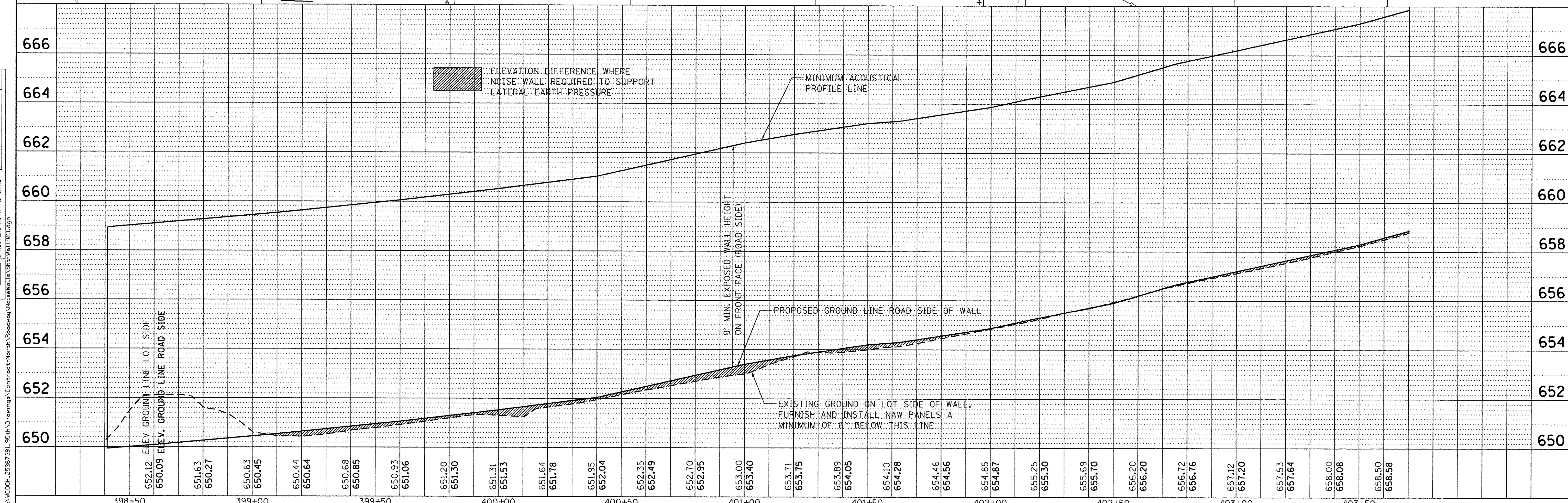
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	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NO.	
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	NO.	



MATCH LINE STA. 403 + 70



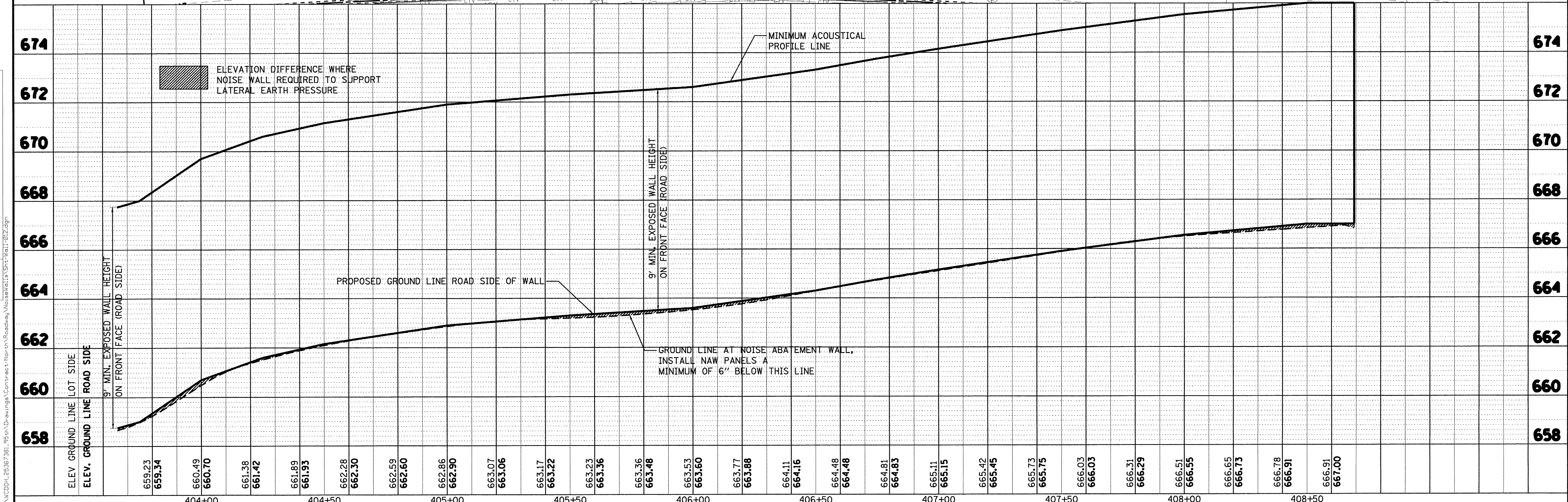
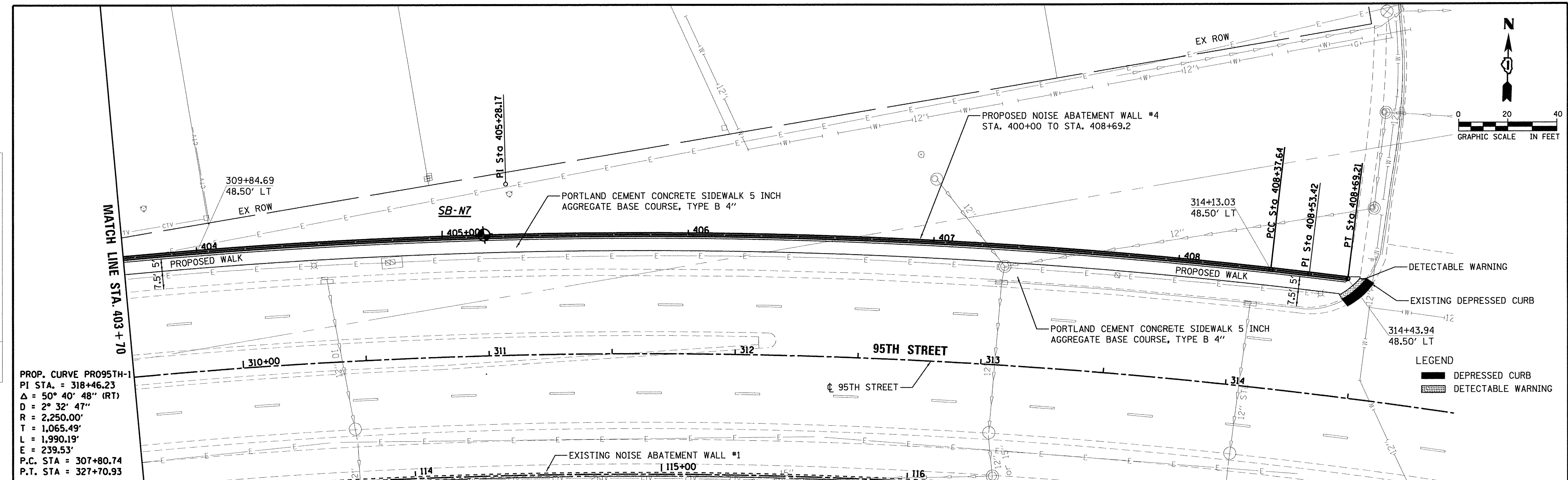
PROP. CURVE PRO95TH-1
 PI STA. = 318+46.23
 $\Delta = 50^\circ 40' 48''$ (RT)
 $D = 2^\circ 32' 47''$
 $R = 2,250.00'$
 $T = 1,065.49'$
 $L = 1,990.19'$
 $E = 239.53'$
 P.C. STA = 307+80.74
 P.T. STA = 327+70.93



URS 100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME = james.push	DESIGNED - KJB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	NOISE ABATEMENT WALL #4 PLAN & PROFILE		F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 120	
	PLOT SCALE = 20.0000' / 1"	CHECKED - NPP	REVISED -		SCALE: 1" = 20'	DRAWING NO. 5 OF 6	STA. TO STA.	CONTRACT NO. 61A30		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 1/21/2016	DATE - 11/22/2013	REVISED -									

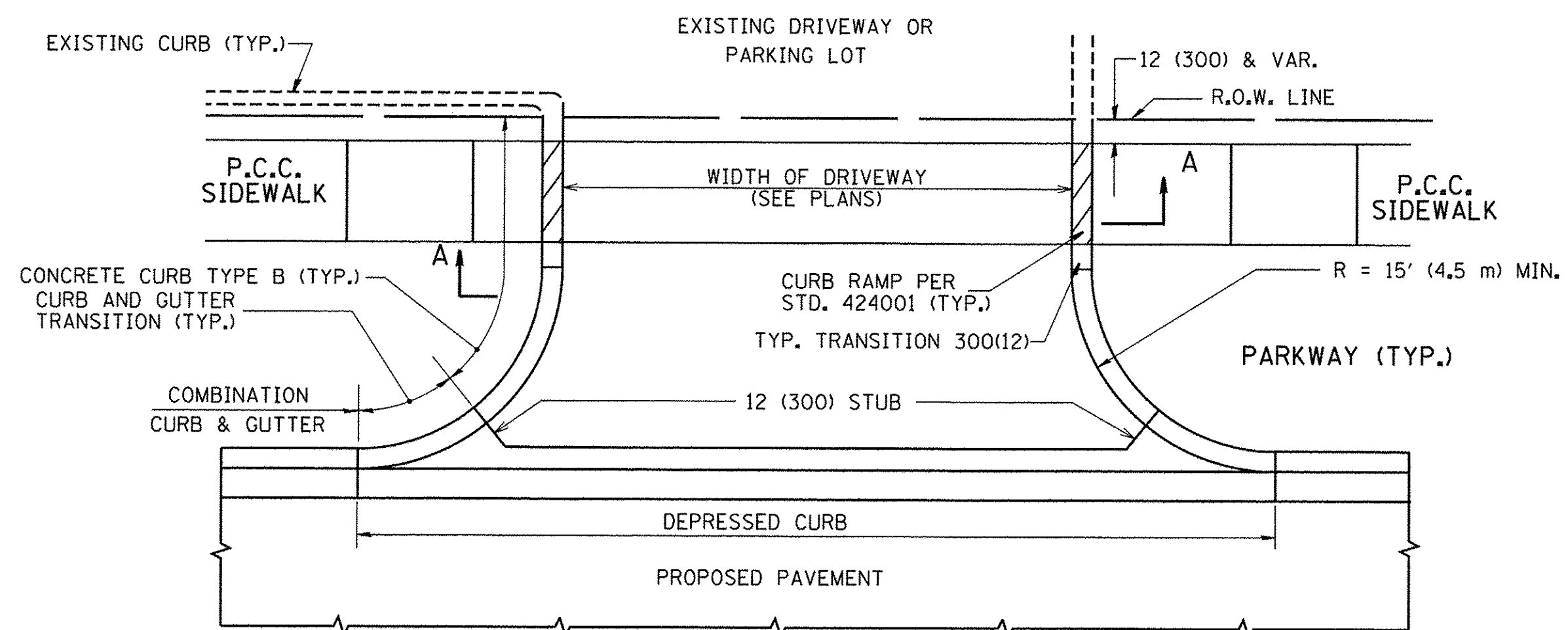
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	ALIGNED	
	CHECKED	
	RT. OF WAY	
	CHECKED	
	NO.	
	ADD FILE NAME	

PROFILE	SURVEYED	DATE
	GRADES	
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	STRUCTURE	
	NOTATION	
	CHKD	

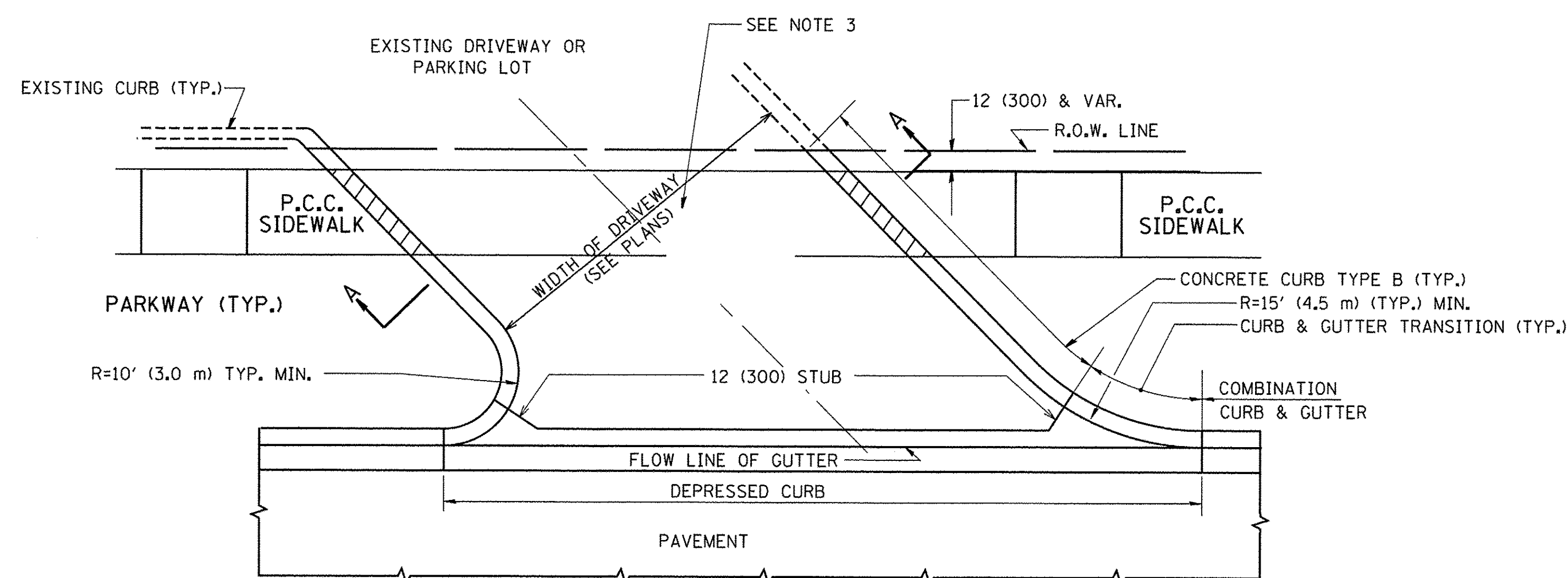


URS 100 S. WACKER DR. SUITE 500 CHICAGO, IL 60606 TEL (312) 939-1000 FAX (312) 939-4198	USER NAME = james.push DESIGNED - KJB DRAWN - KJB CHECKED - NPP DATE - 10/13/2016	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	NOISE ABATEMENT WALL #4 PLAN & PROFILE	F.A.U. RTE. 1644 SECTION 12-00181-01-FP COUNTY WILL TOTAL SHEETS 179 SHEET NO. 121 CONTRACT NO. 61A30 ILLINOIS FED. AID PROJECT
	PLOT SCALE = 40,000.00' / 1" IN. PLOT DATE = 10/13/2016	SCALE: 1" = 20' DRAWING NO. 6 OF 6 STA. TO STA.			

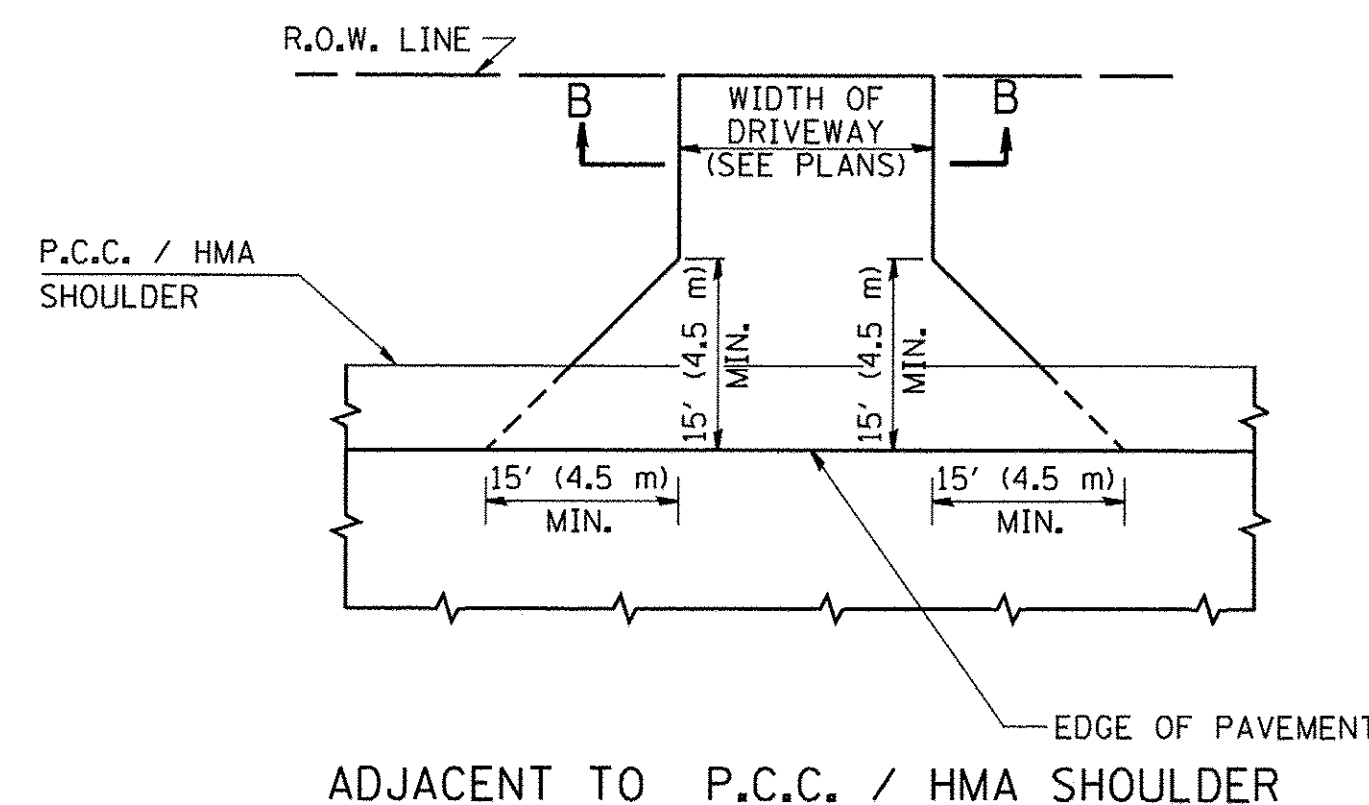
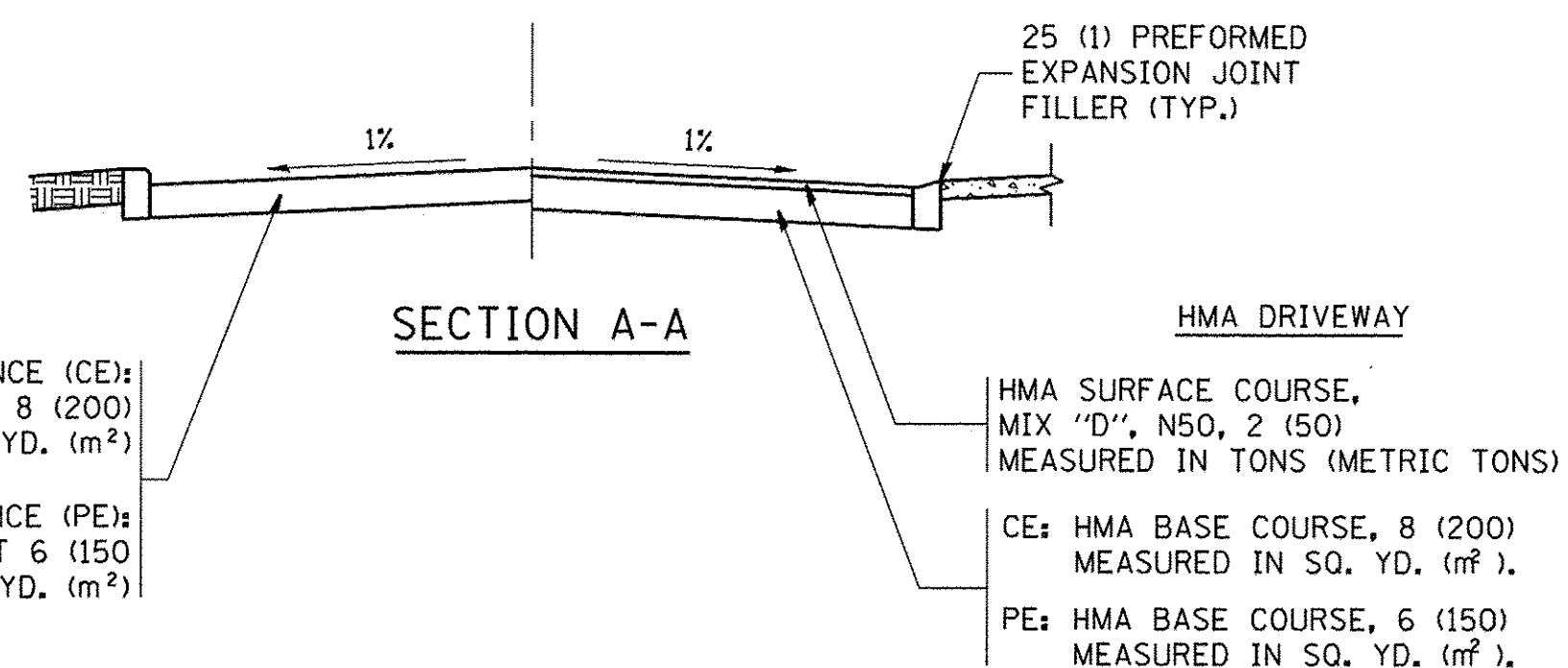
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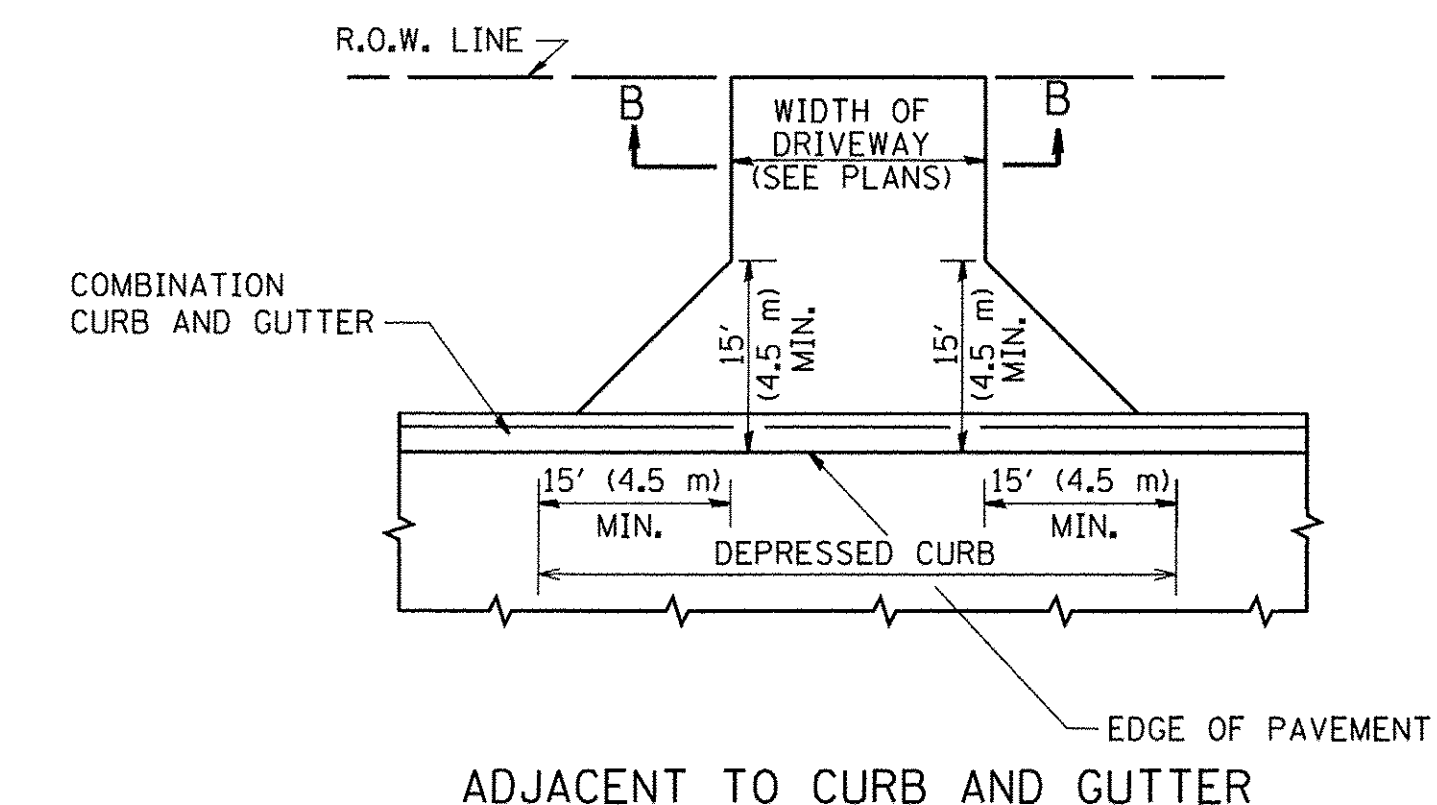
WITH CONCRETE CURB, TYPE B



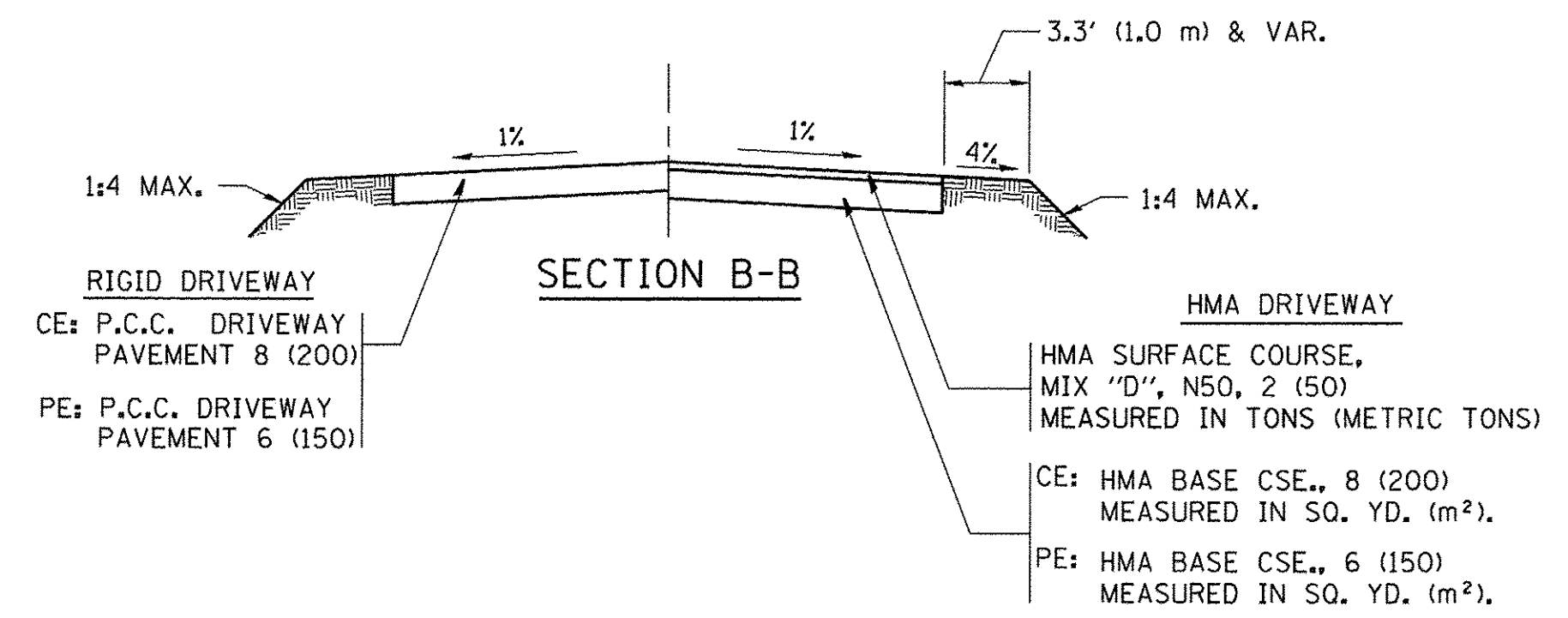
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS, SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

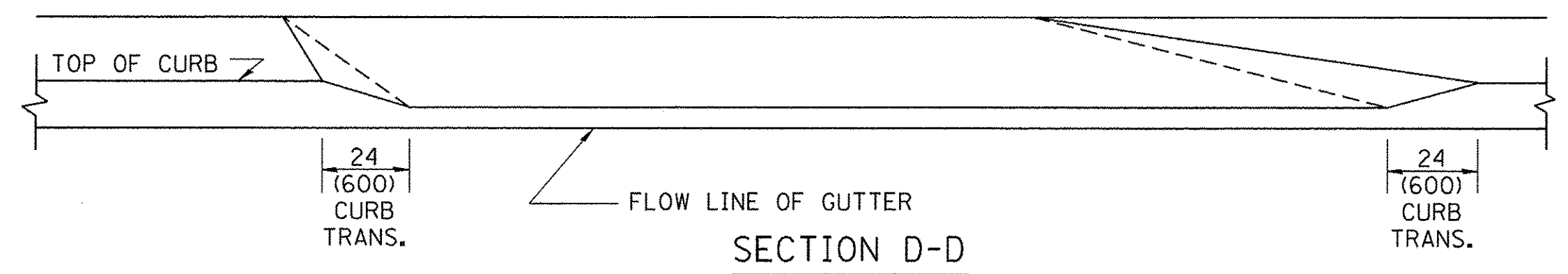
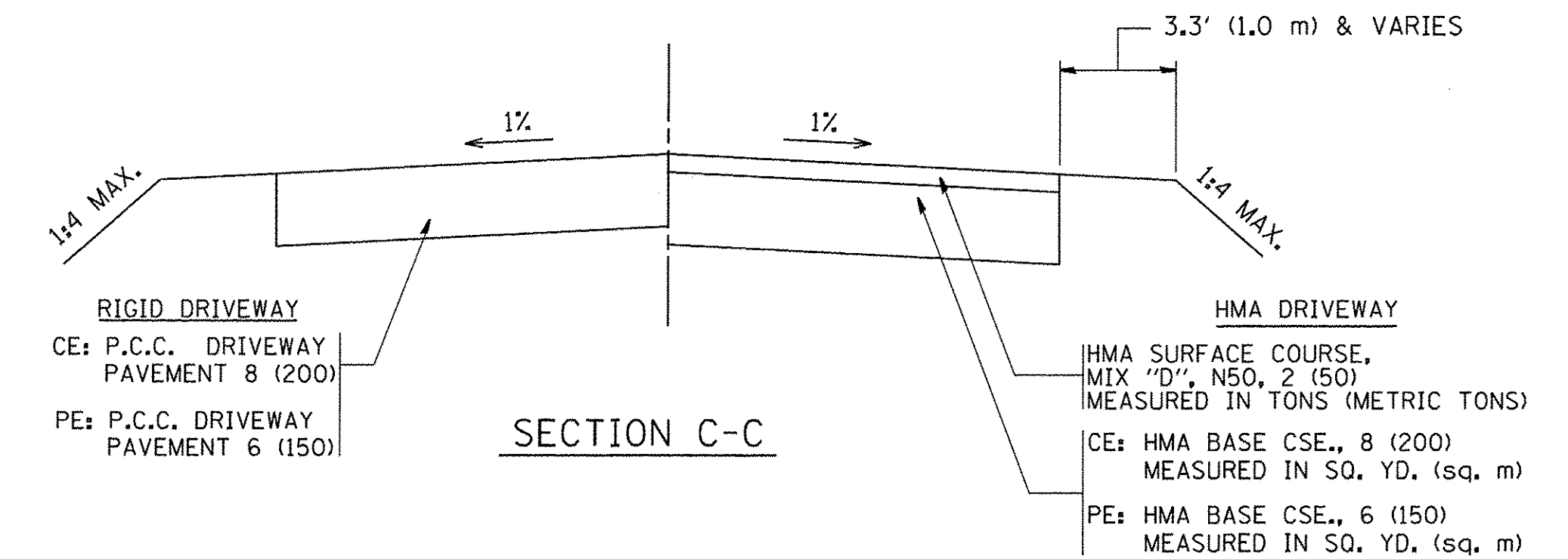
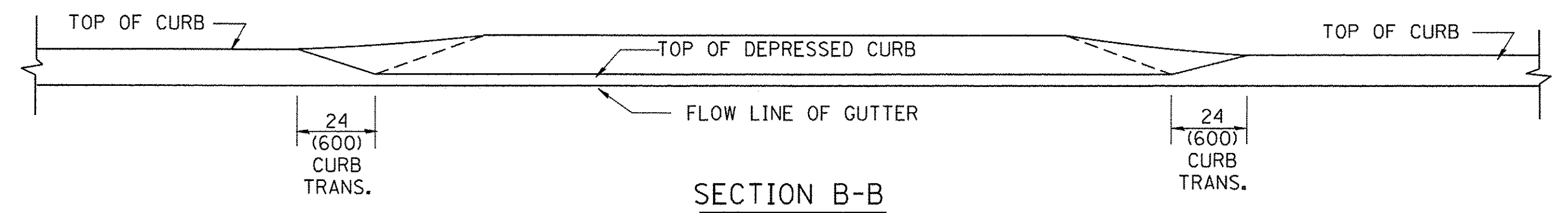
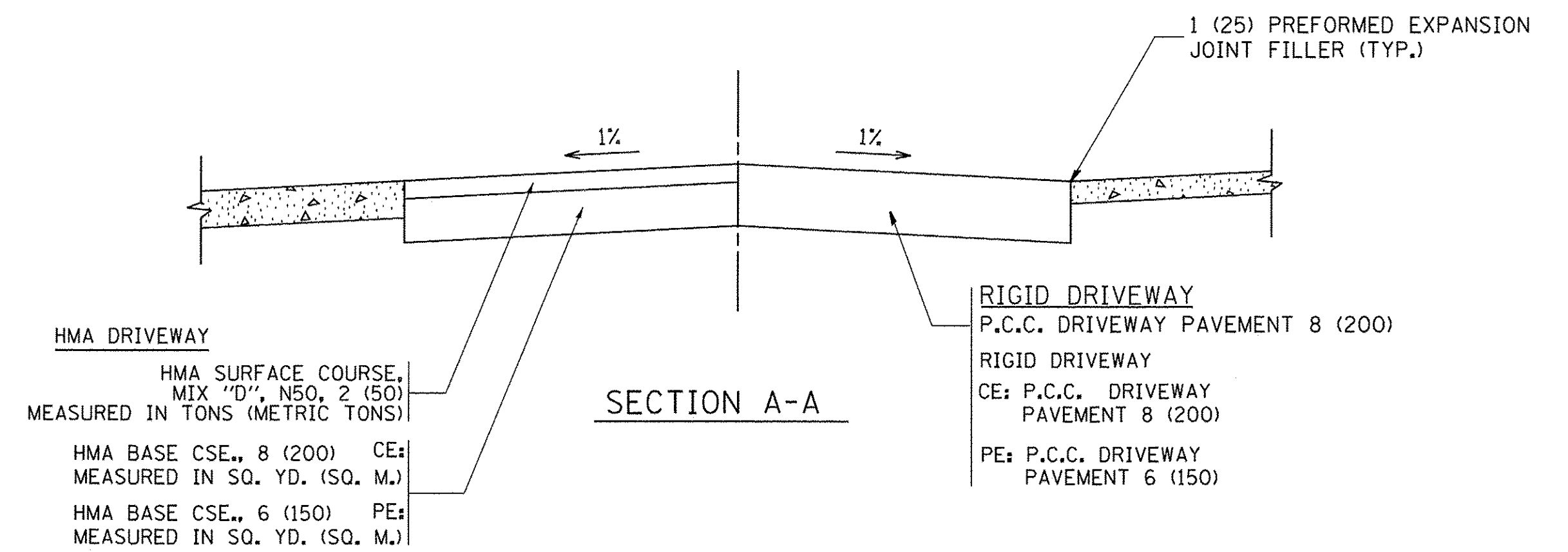
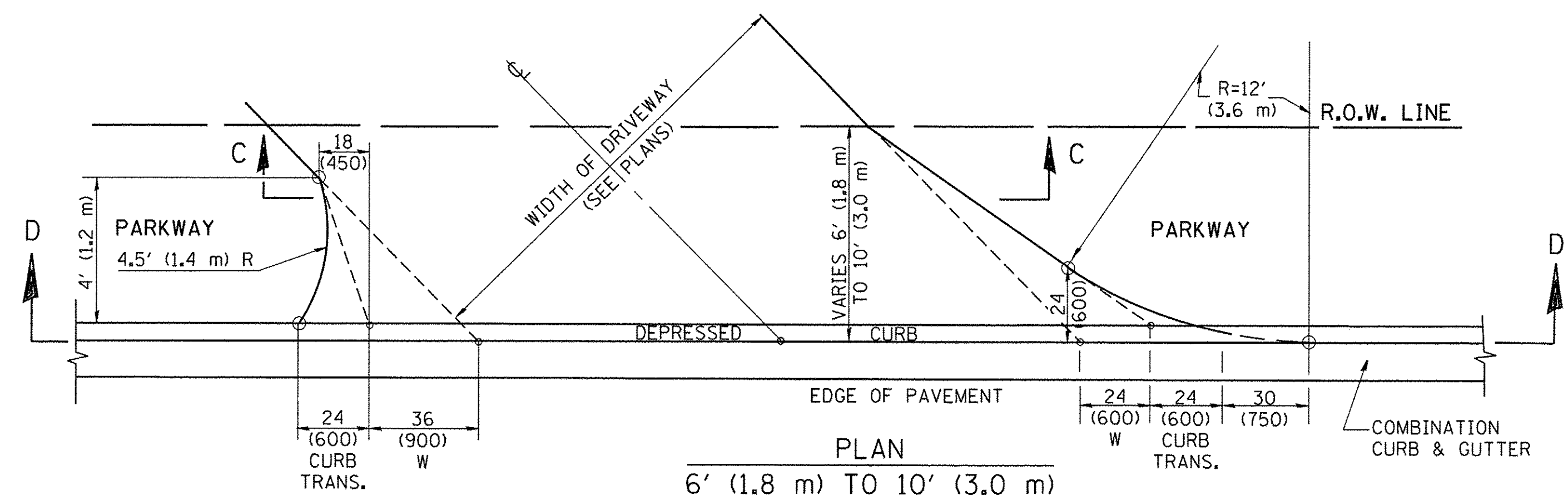
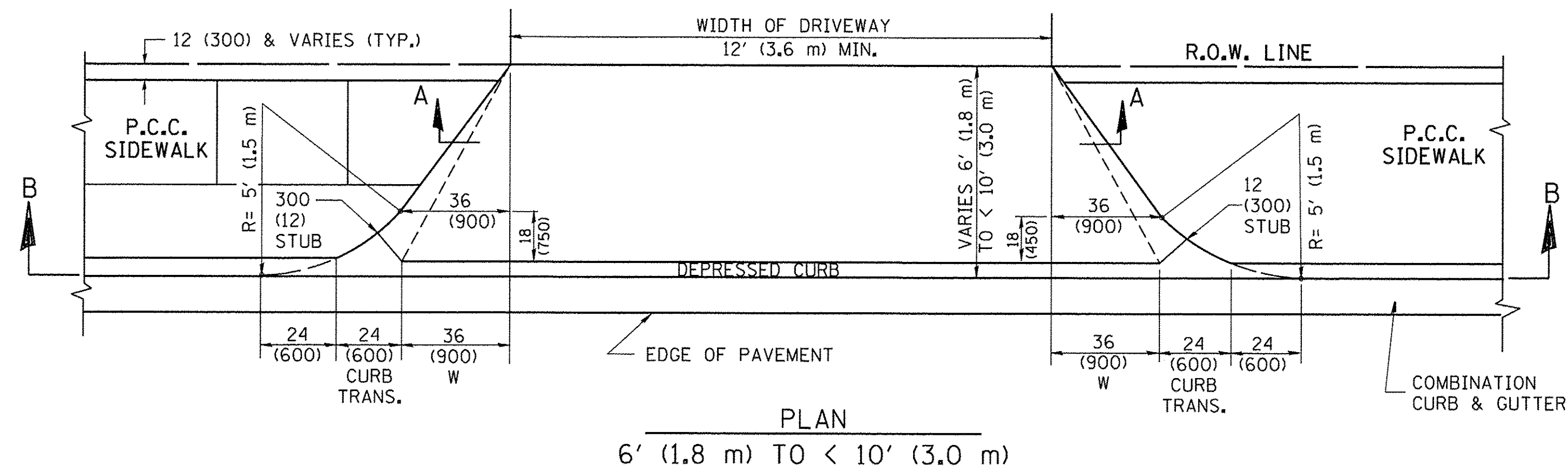
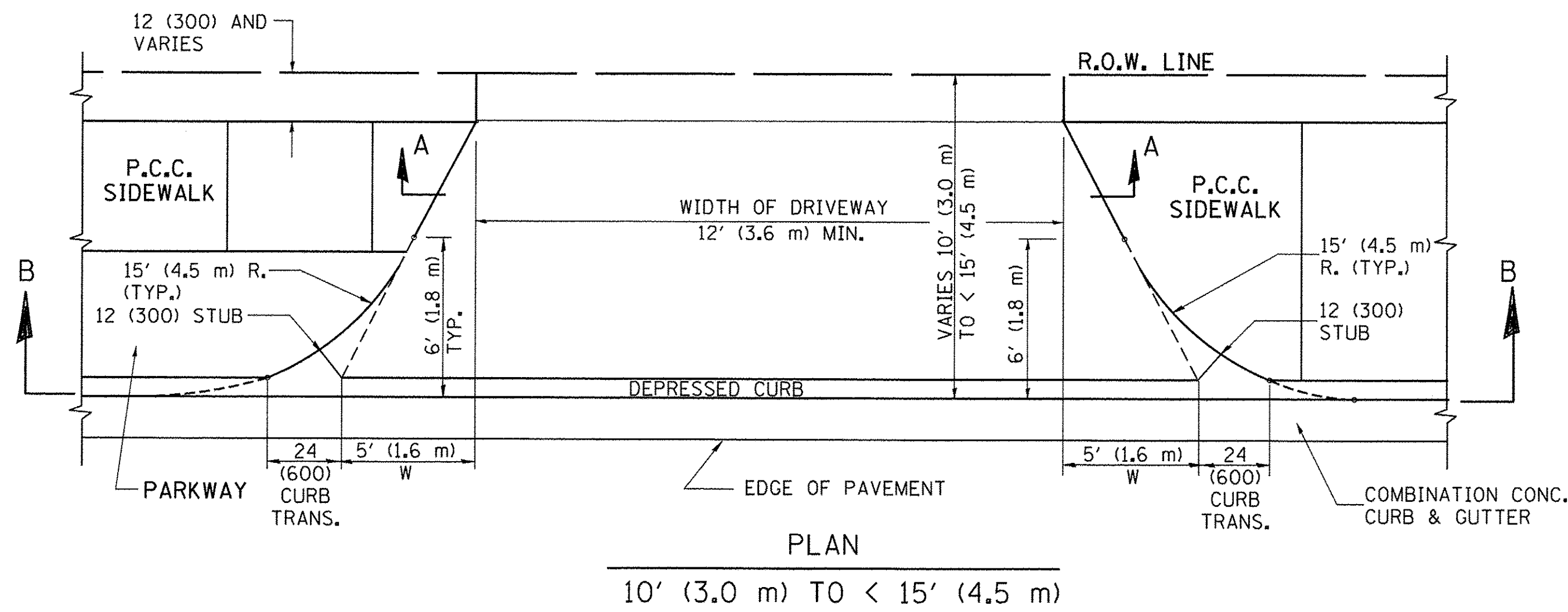
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
 AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	122
BD0156-07 (BD-01)			CONTRACT NO. 61A30	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

FILE NAME =	USER NAME = lejso	DESIGNED - R. SHAH	REVISED - P. LGFLUER 04-15-03
cd:\pw_work\pwidth\1ejso\d0108315\bd01.dwg		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50,0000 ' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 9/6/2011	DATE - 11-04-95	REVISED - R. BORO 09-06-11



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

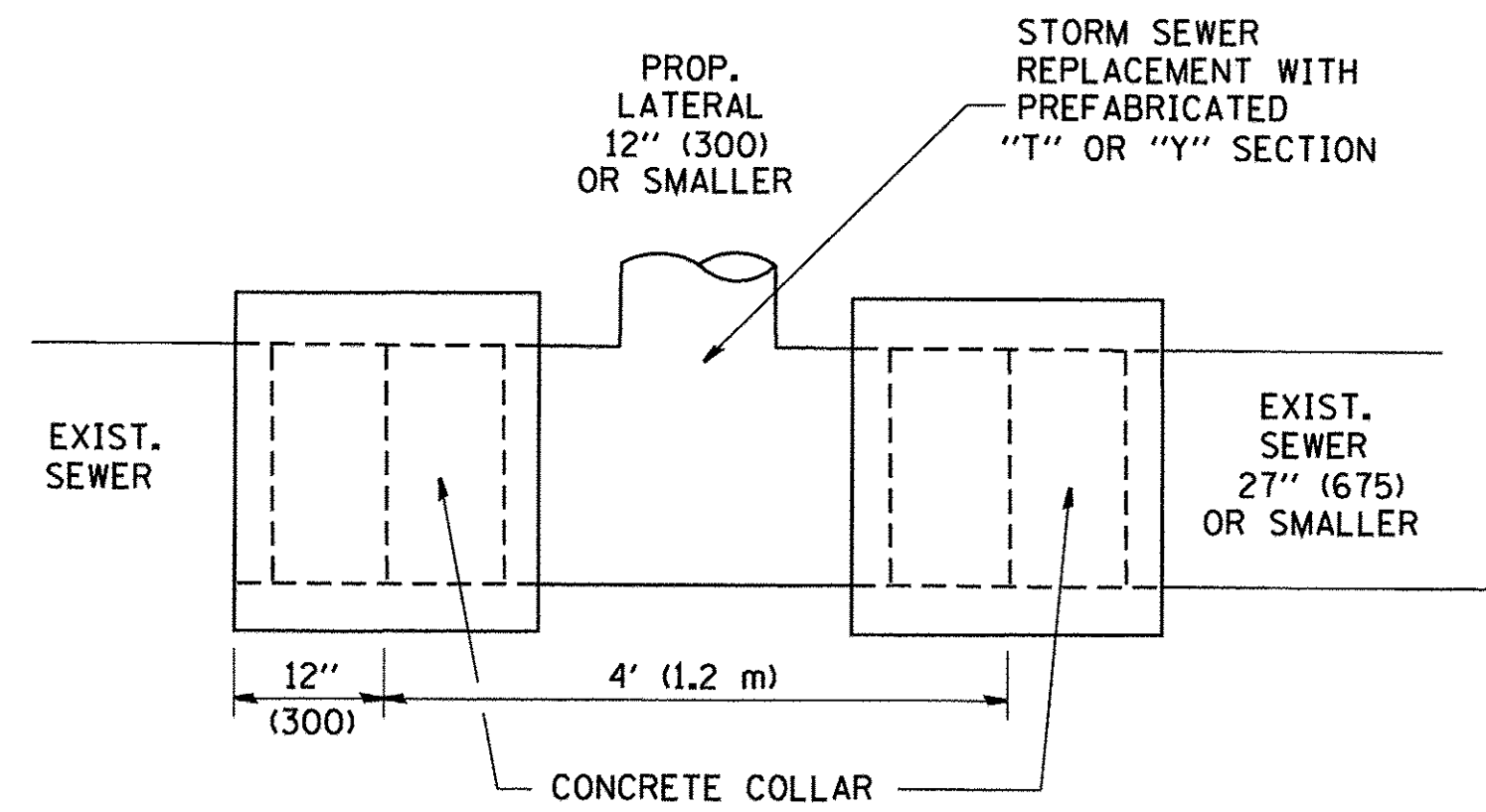
FILE NAME =	USER NAME = jeyso	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
α\p\work\pwsdot\jeyso\0108315\bd02.dwg		DRAWN -	REVISED - P. LoFLEUR 04-15-03
	PLOT SCALE = 50,0000' / 1"	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)**

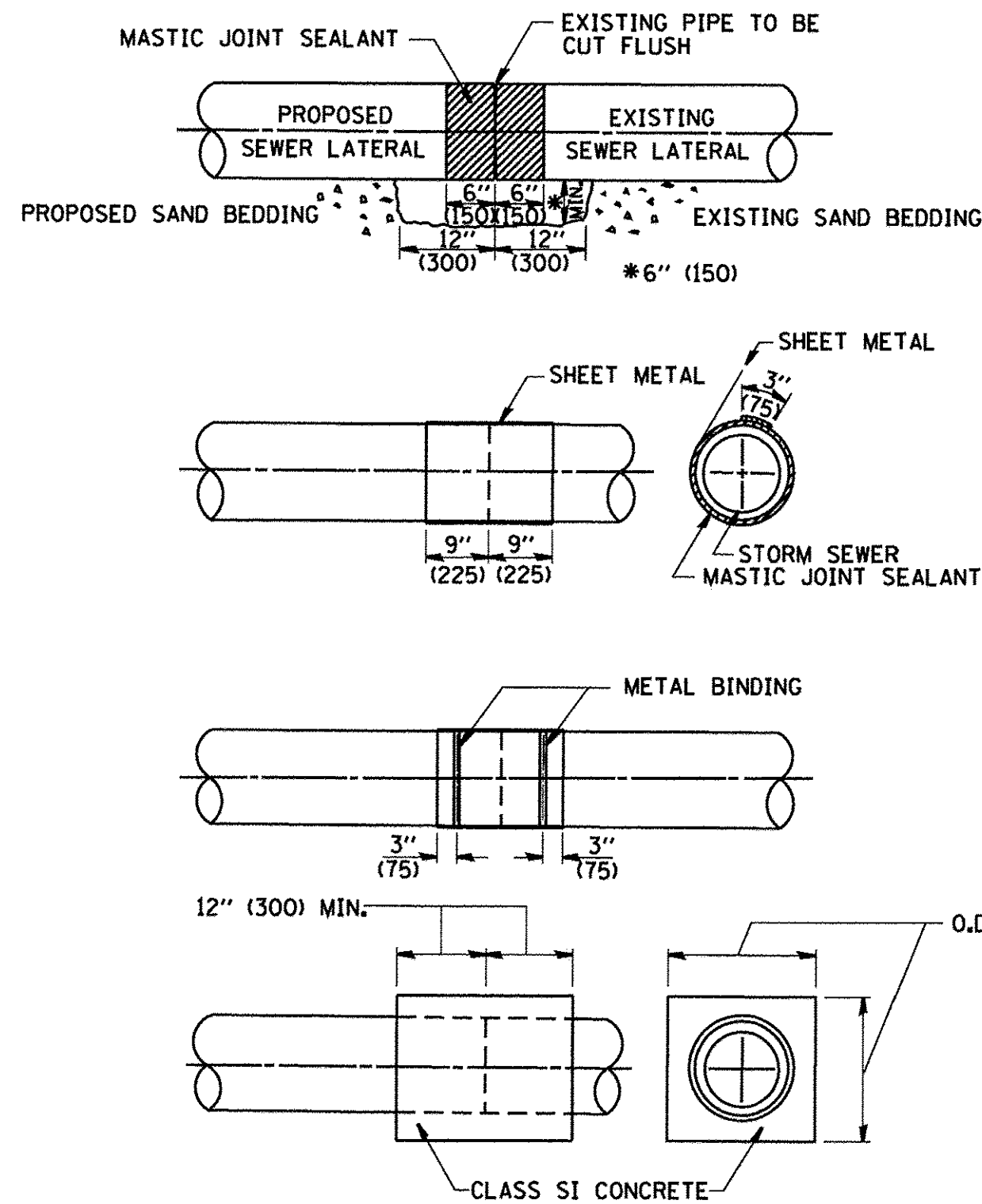
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	123
BD400-02 (BD-02)		CONTRACT NO. 61A30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

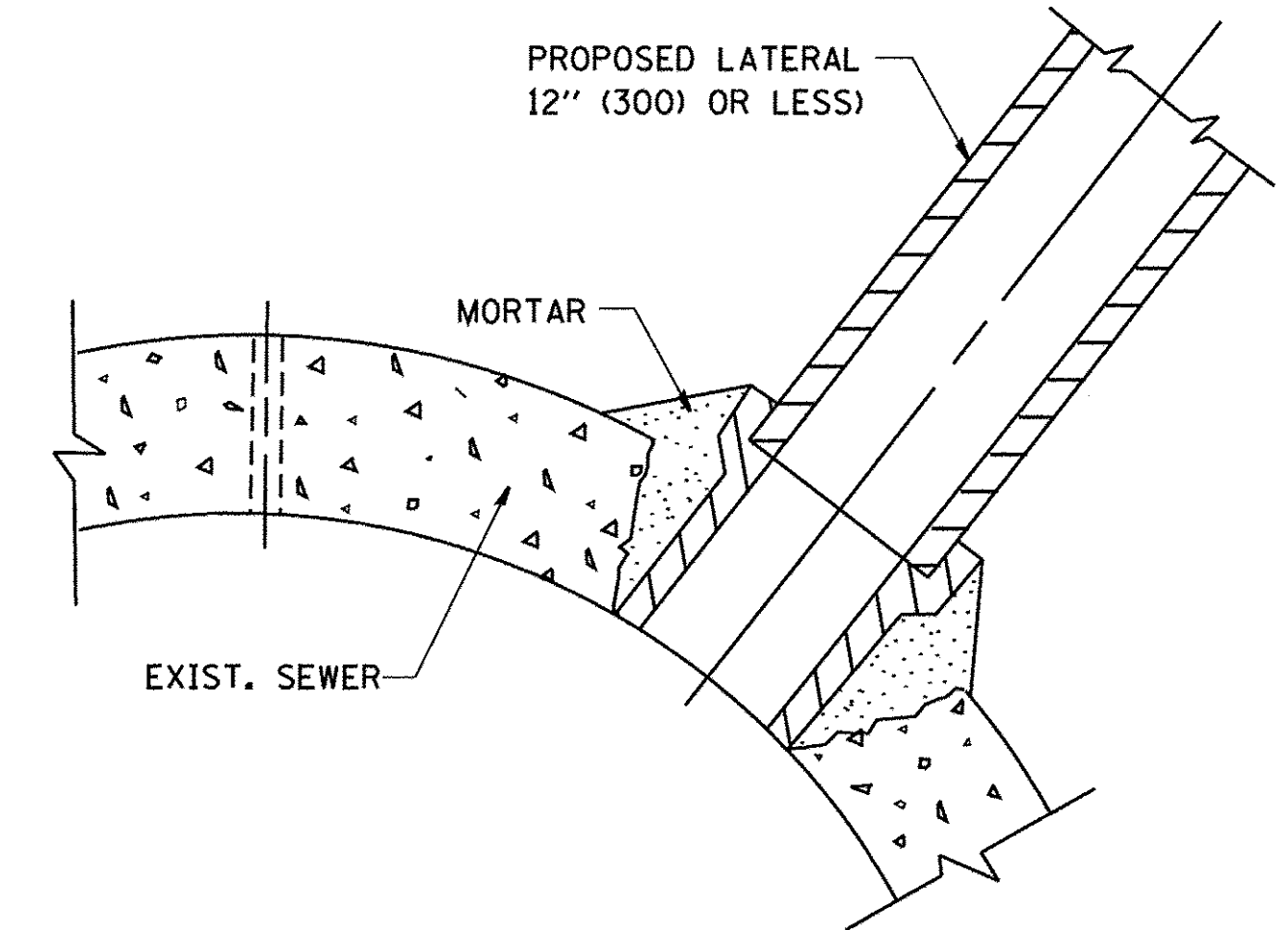


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

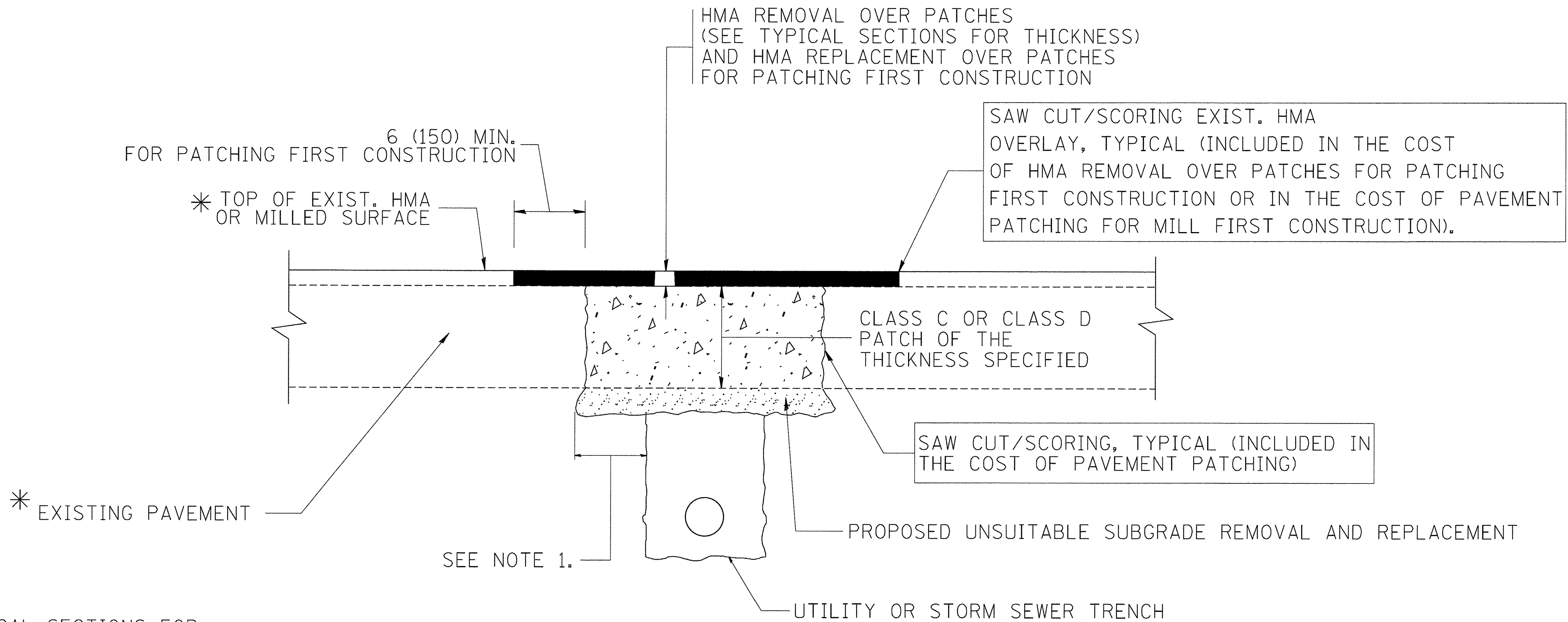
REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd07.dgn	USER NAME = geglienobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. SHAH 09-09-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	1644	12-00181-01-FP	WILL	179 124
		PLOT SCALE = 50,000 ' / IN.	REVISED - R. SHAH 10-25-94						BD500-01 (BD-7)		CONTRACT NO. 61A30	
		PLOT DATE = 1/4/2008	REVISED - R. SHAH 06-12-96		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

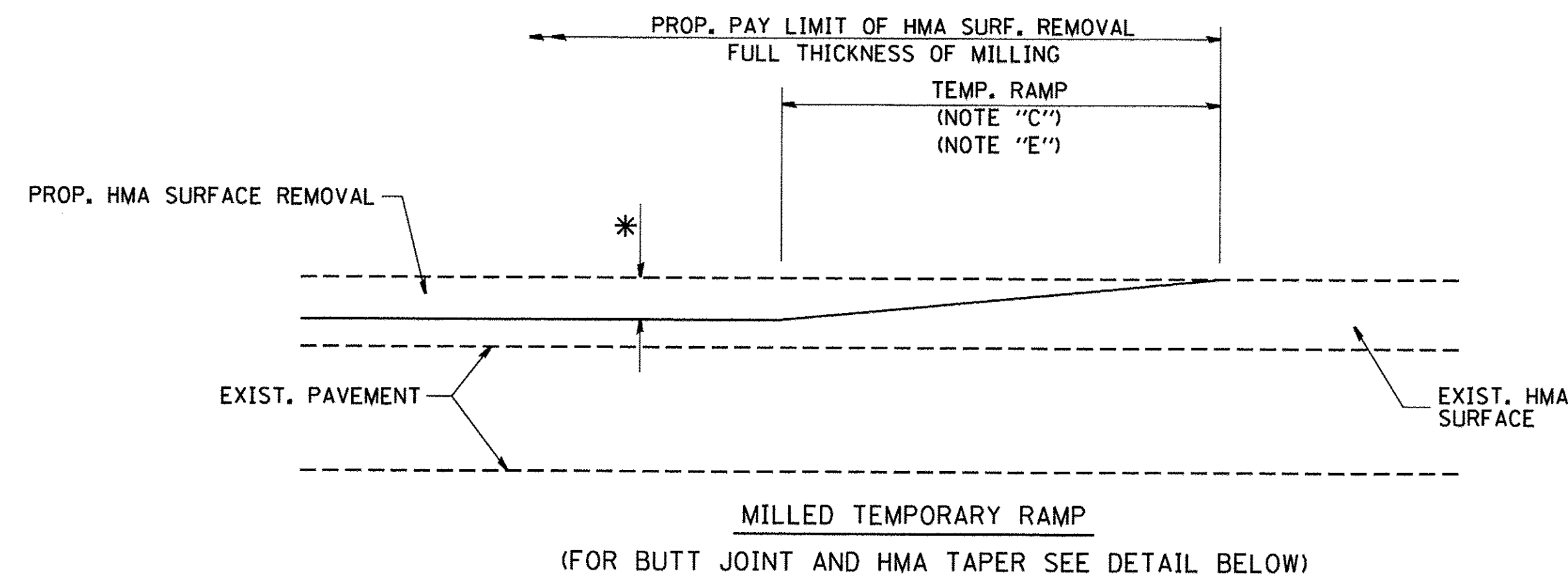
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

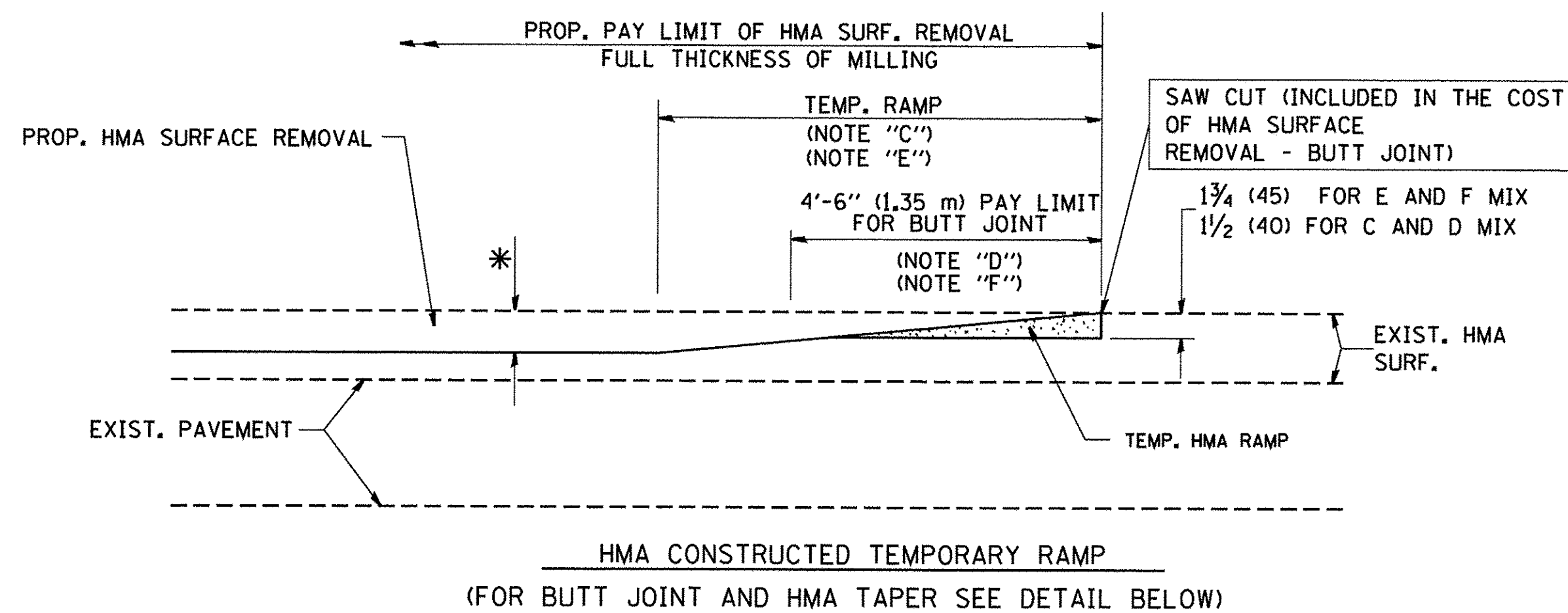
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\distatd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. BORO 01-01-07		1644	12-00181-01-FP	WILL	179	125			
		PLOT SCALE = 50.000' / IN.	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 61A30				
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

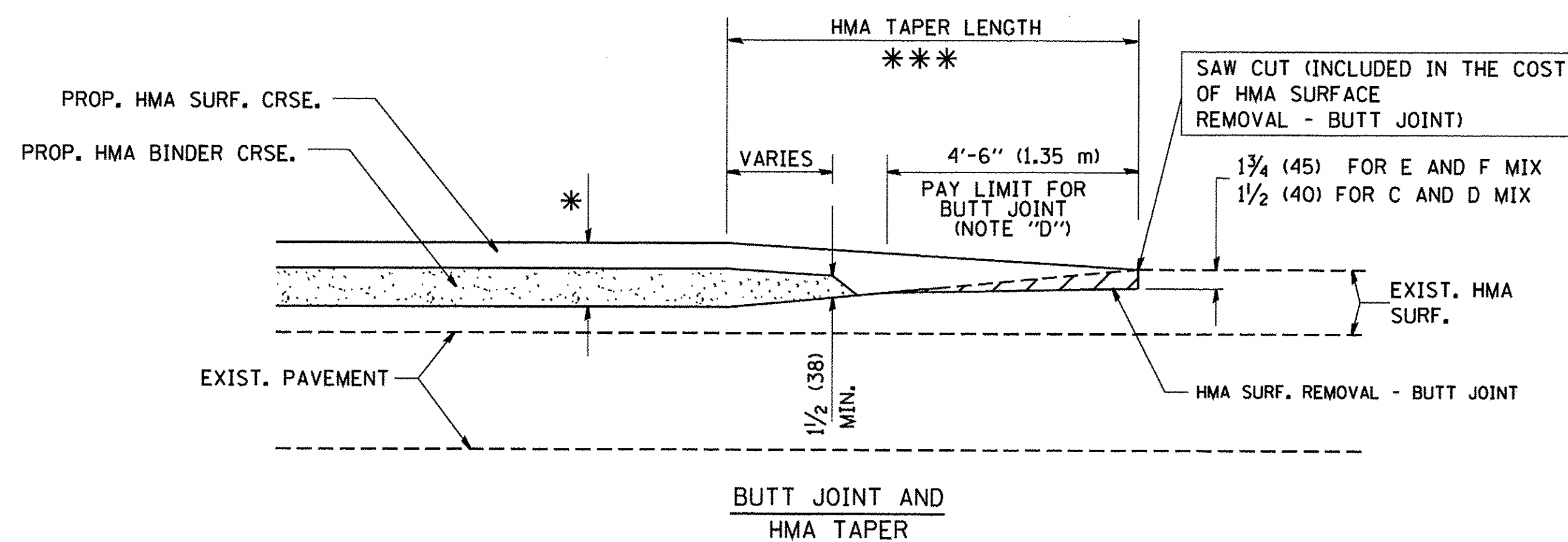


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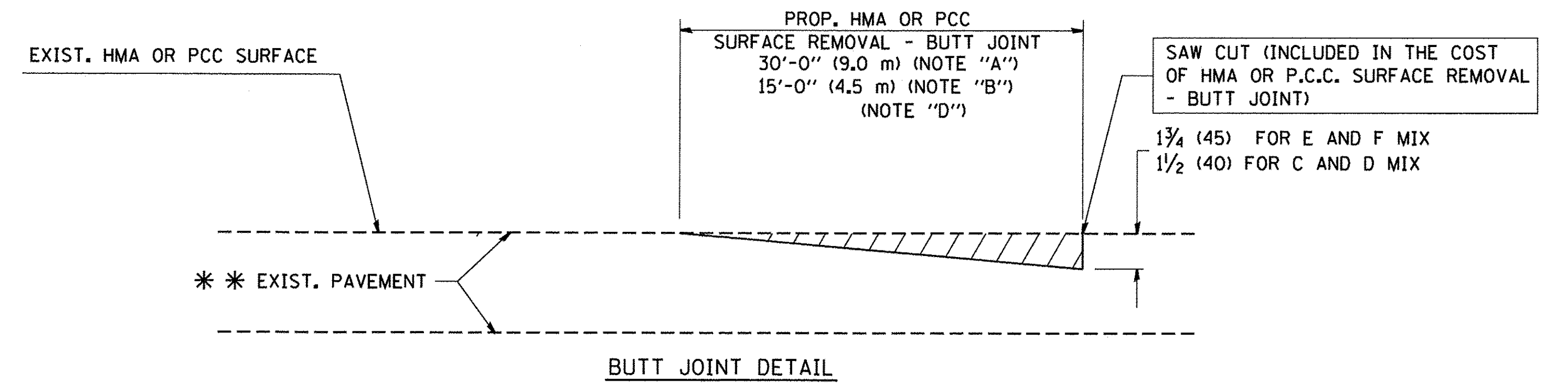


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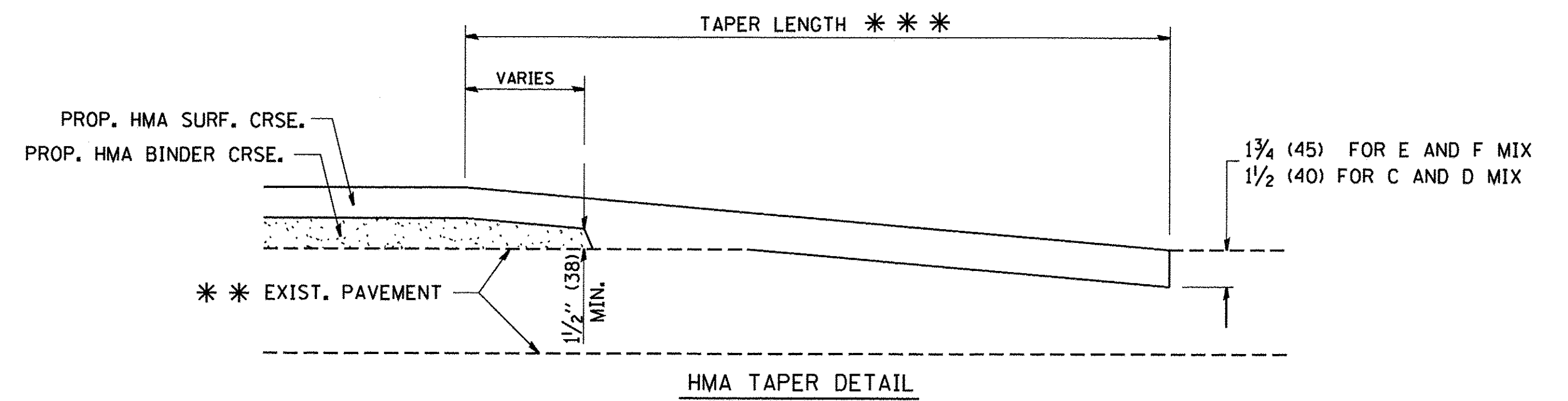
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

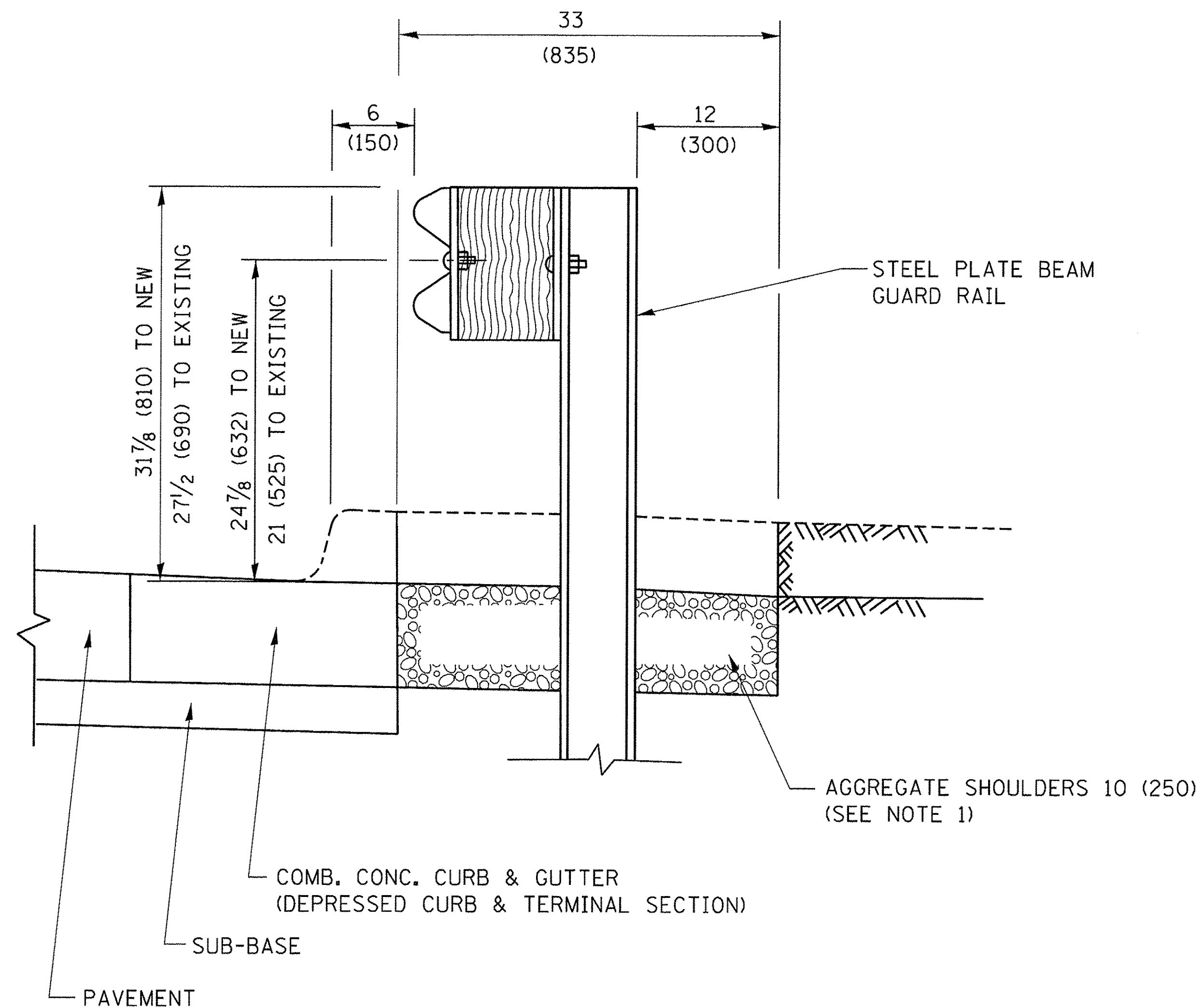
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	126
BD400-05 BD32		CONTRACT NO. 61A30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

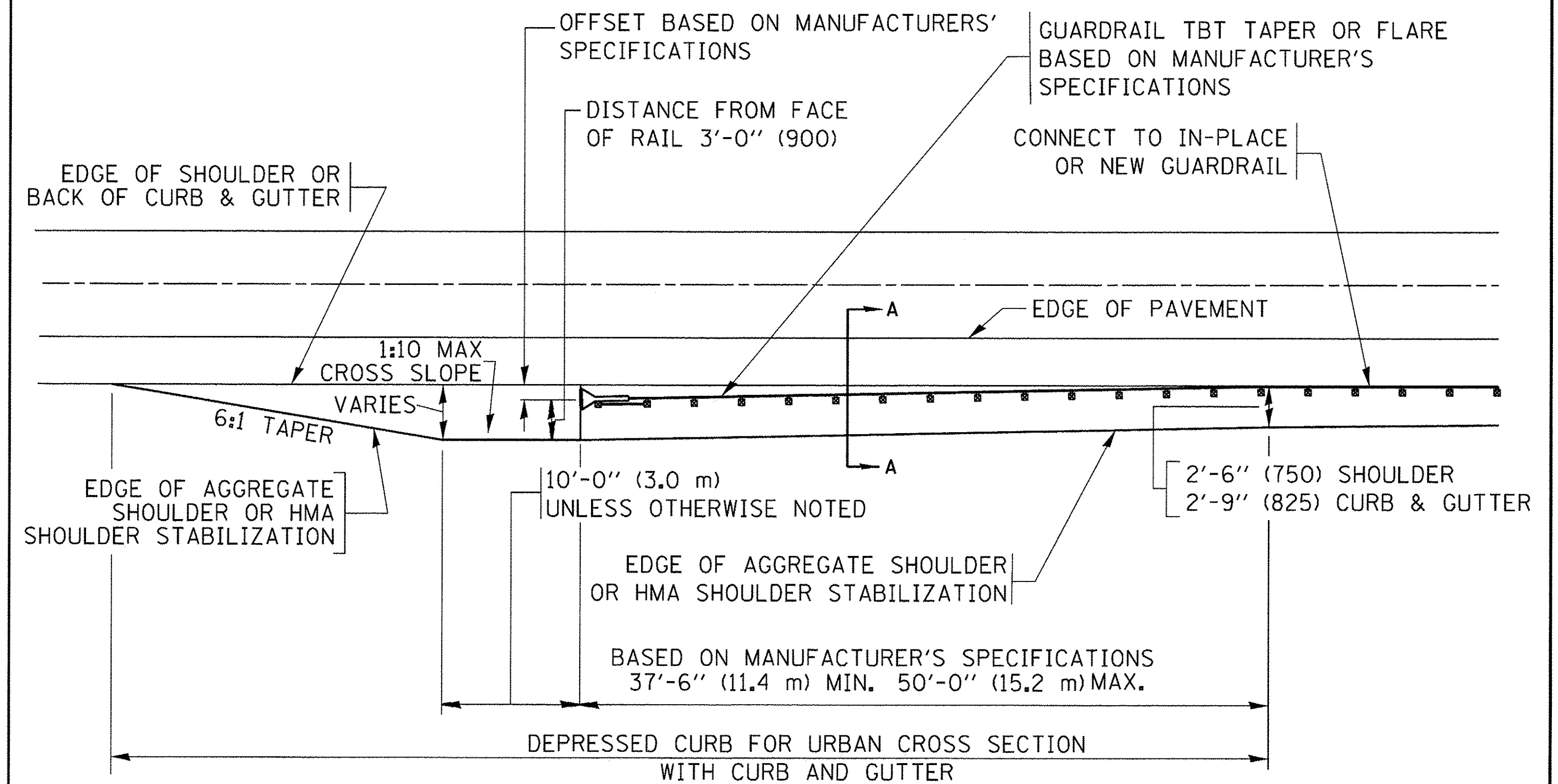


SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10 (250) OR HMA SHOULDER, 6 (150) (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
 2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER

[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]



DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.

AGGREGATE SHOULDER, 10 (250) WILL BE PAID ACCORDING TO SECTION 481.

HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID ACCORDING TO SECTION 482.

COMB. CONC. C&G, STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

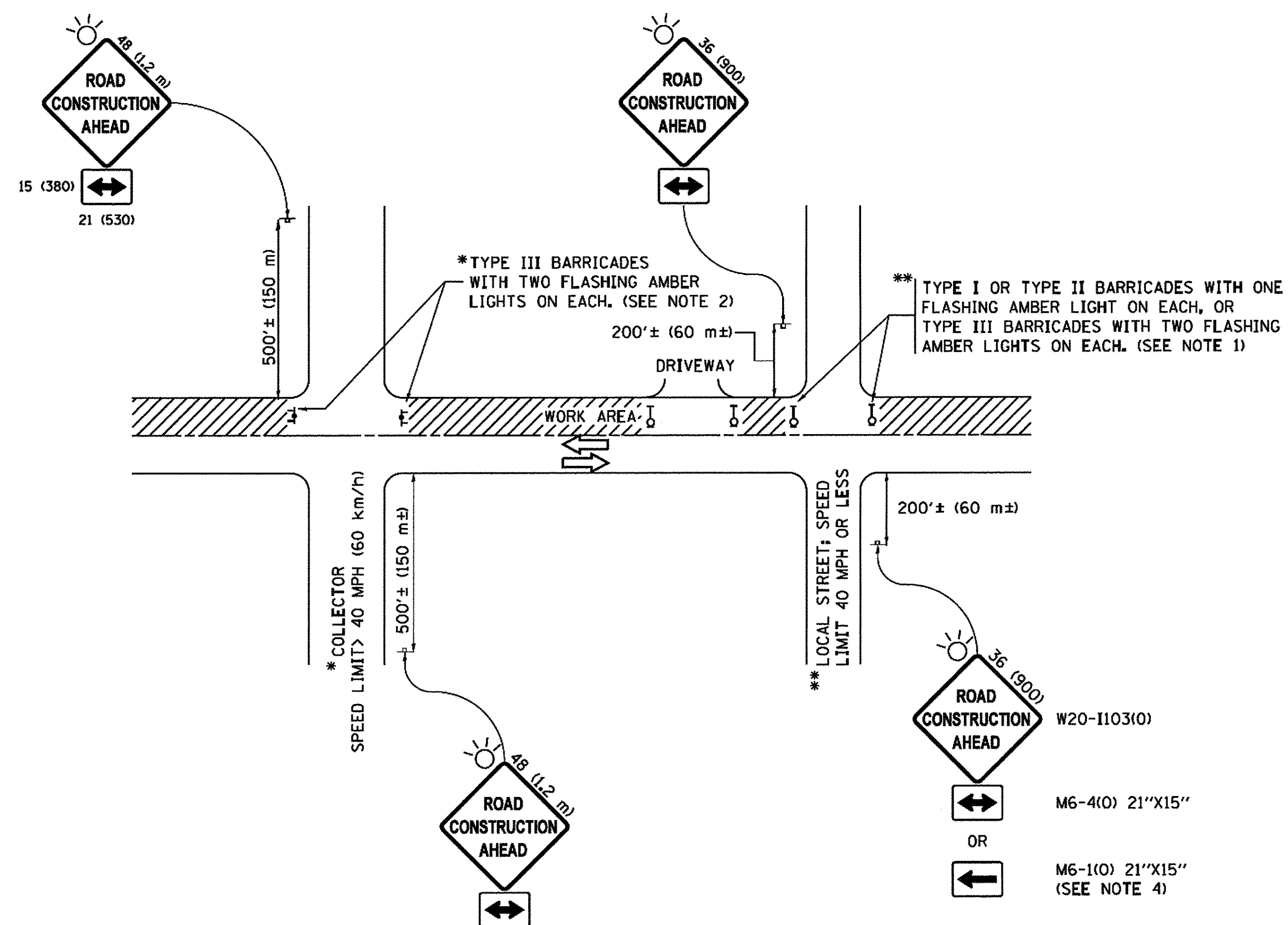
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Default	PLOT SCALE = 50.0000' / 1"	CHECKED -	DATE - 09-22-90
	PLOT DATE = 12/21/2015	DATE - 09-22-90	REVISED - R. BORO 05-08-2015

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	127
BD600-10 (BD 34)		CONTRACT NO. 61A30		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

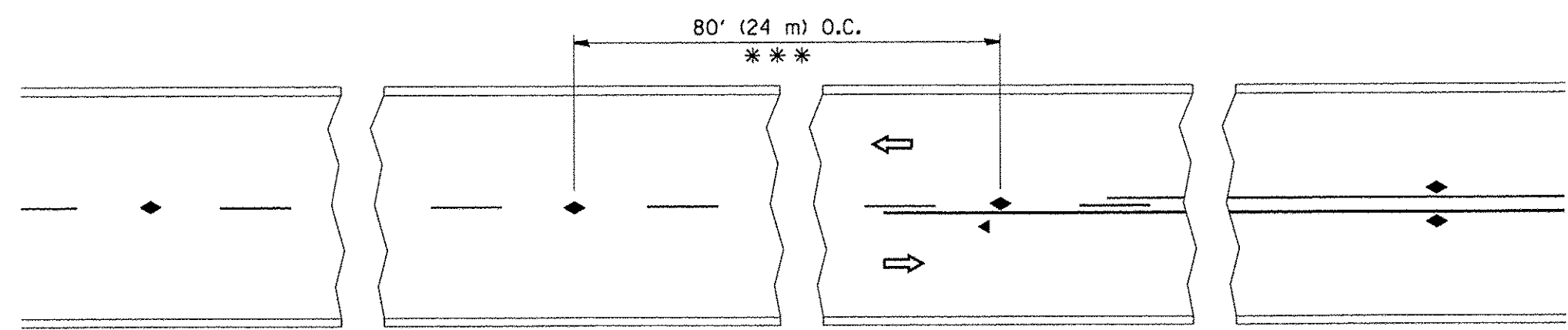
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	PLOT DATE = 9/15/2016		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

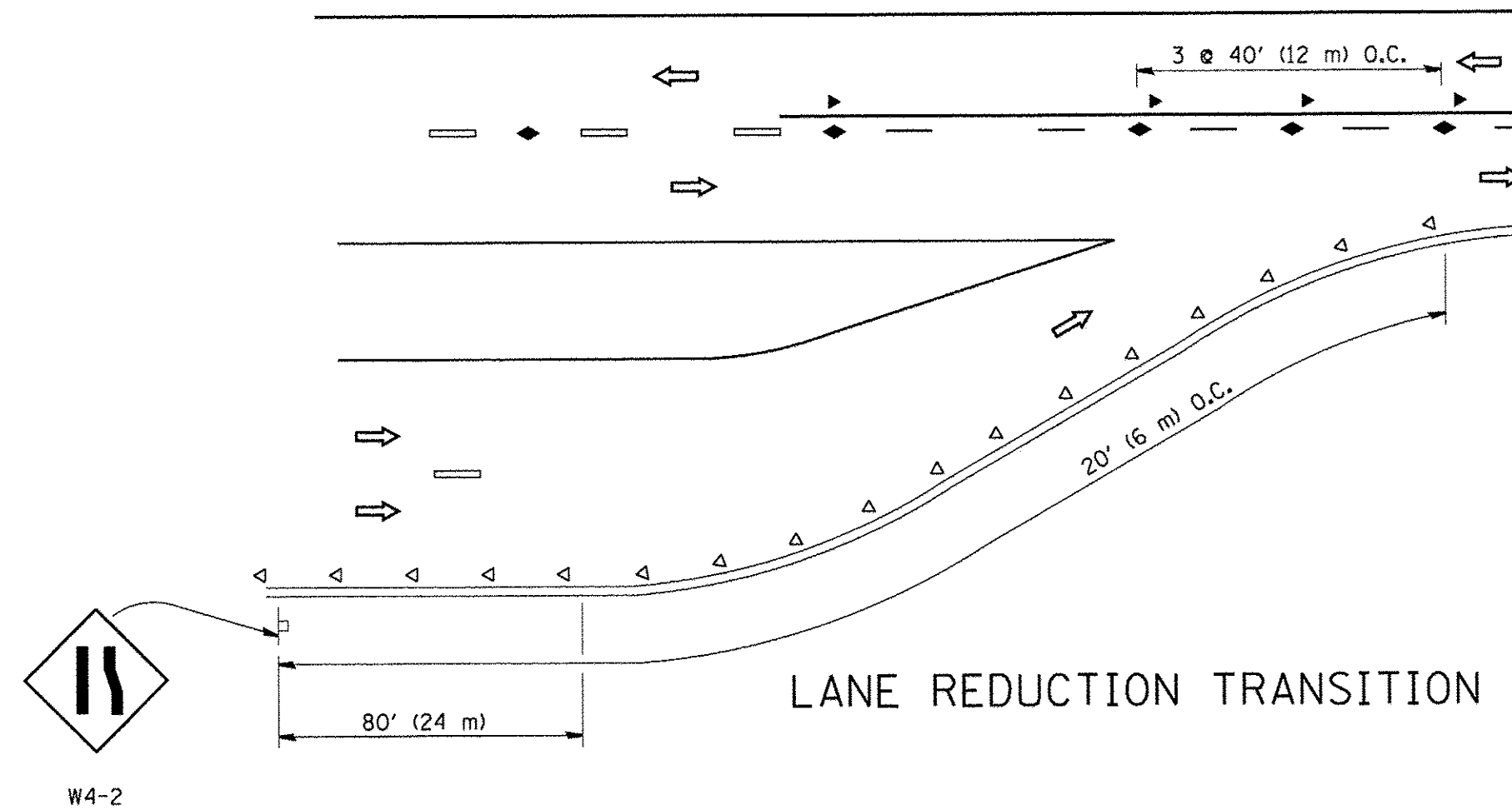
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	128
TC-10			CONTRACT NO. 61A30	
ILLINOIS FED. AID PROJECT				

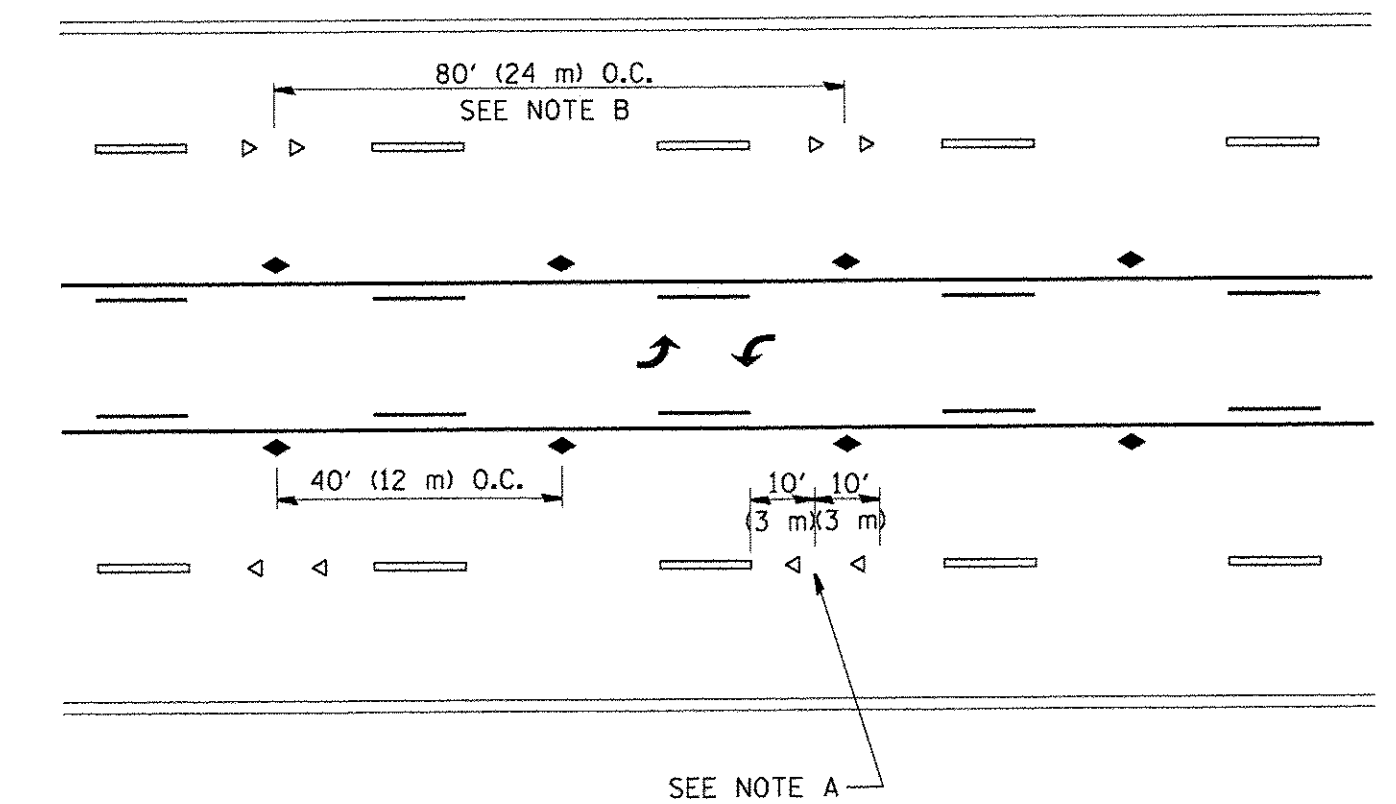


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

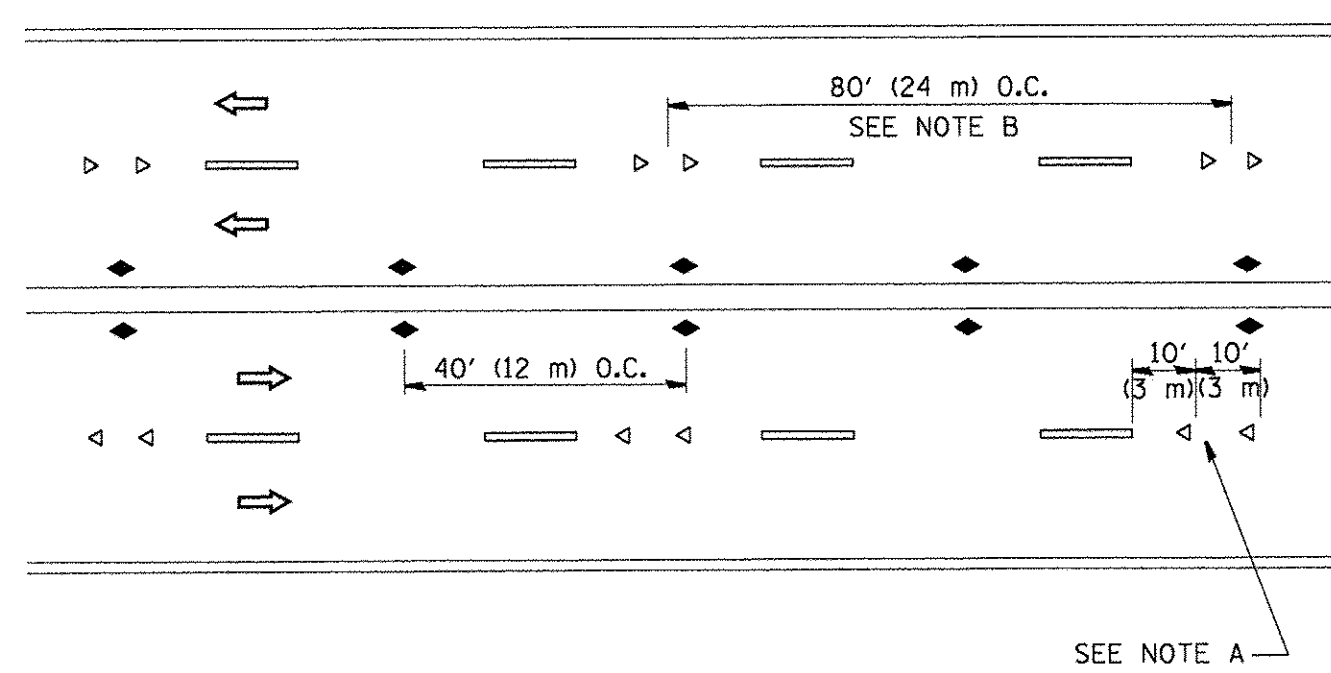
TWO-LANE/TWO-WAY



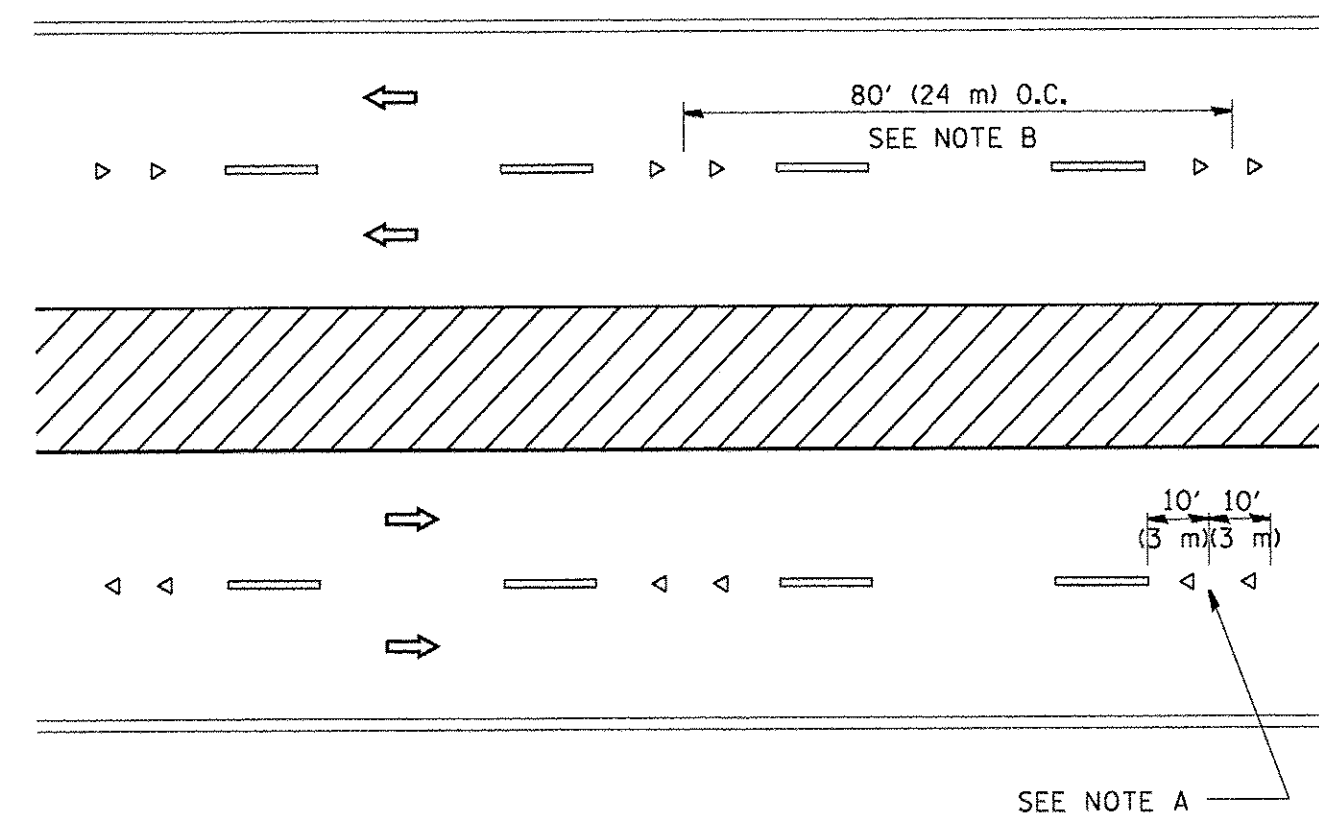
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

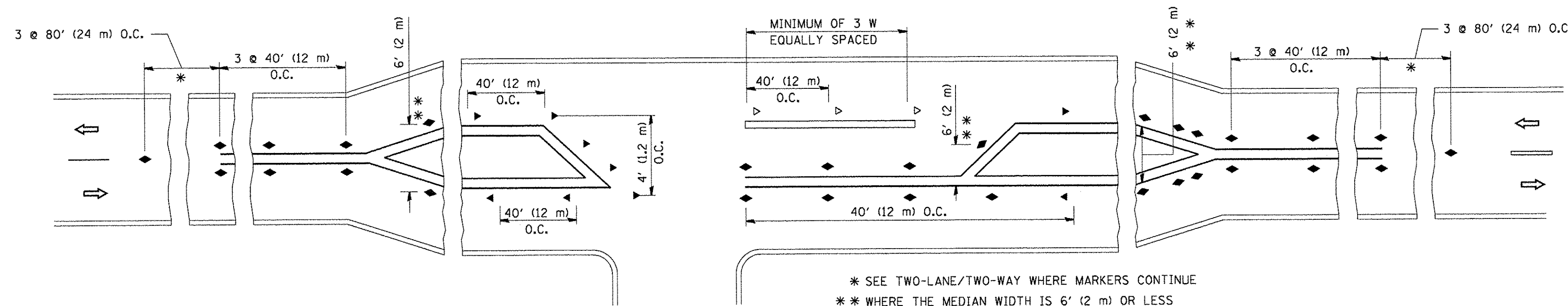
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

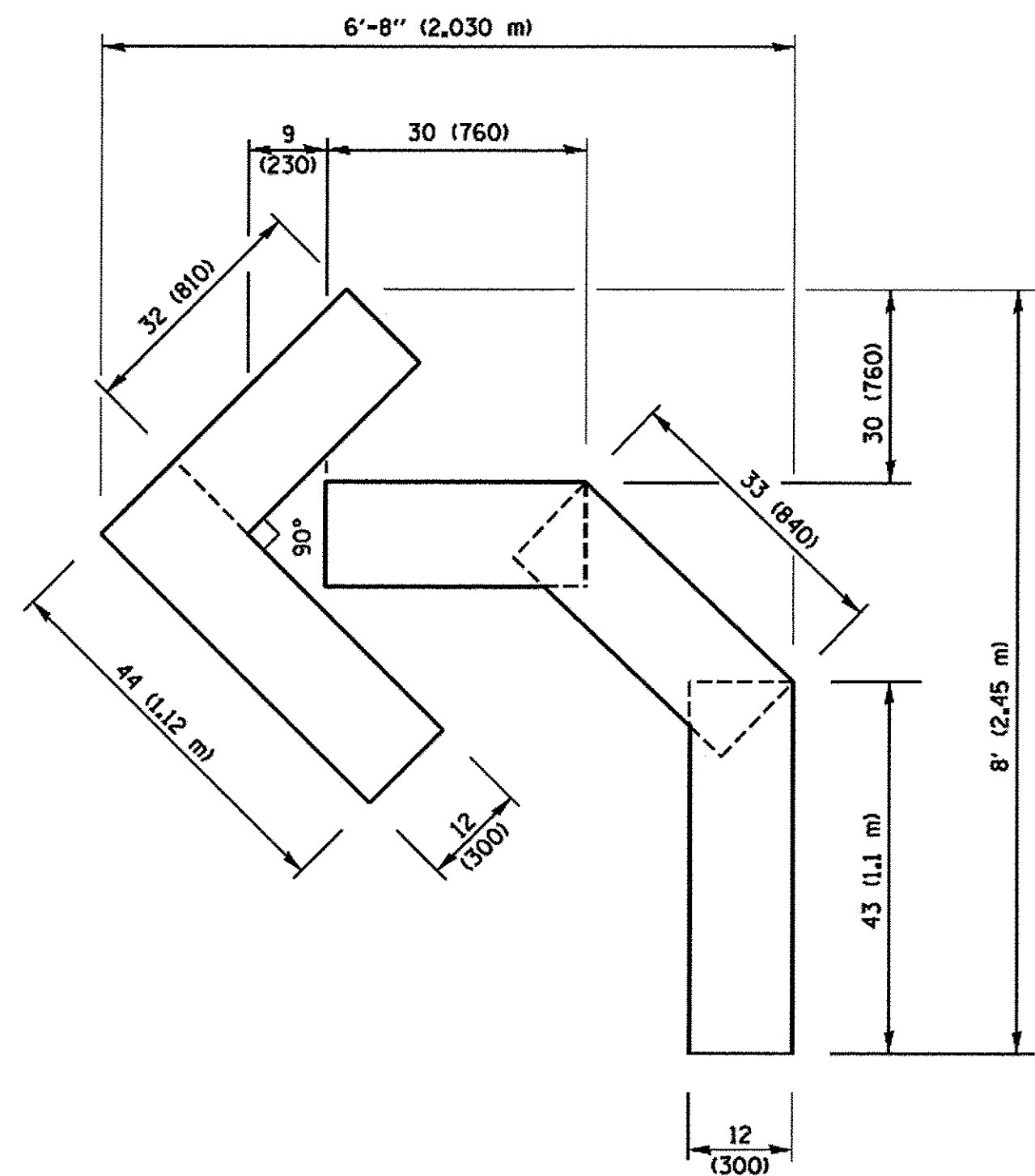


LEFT TURN

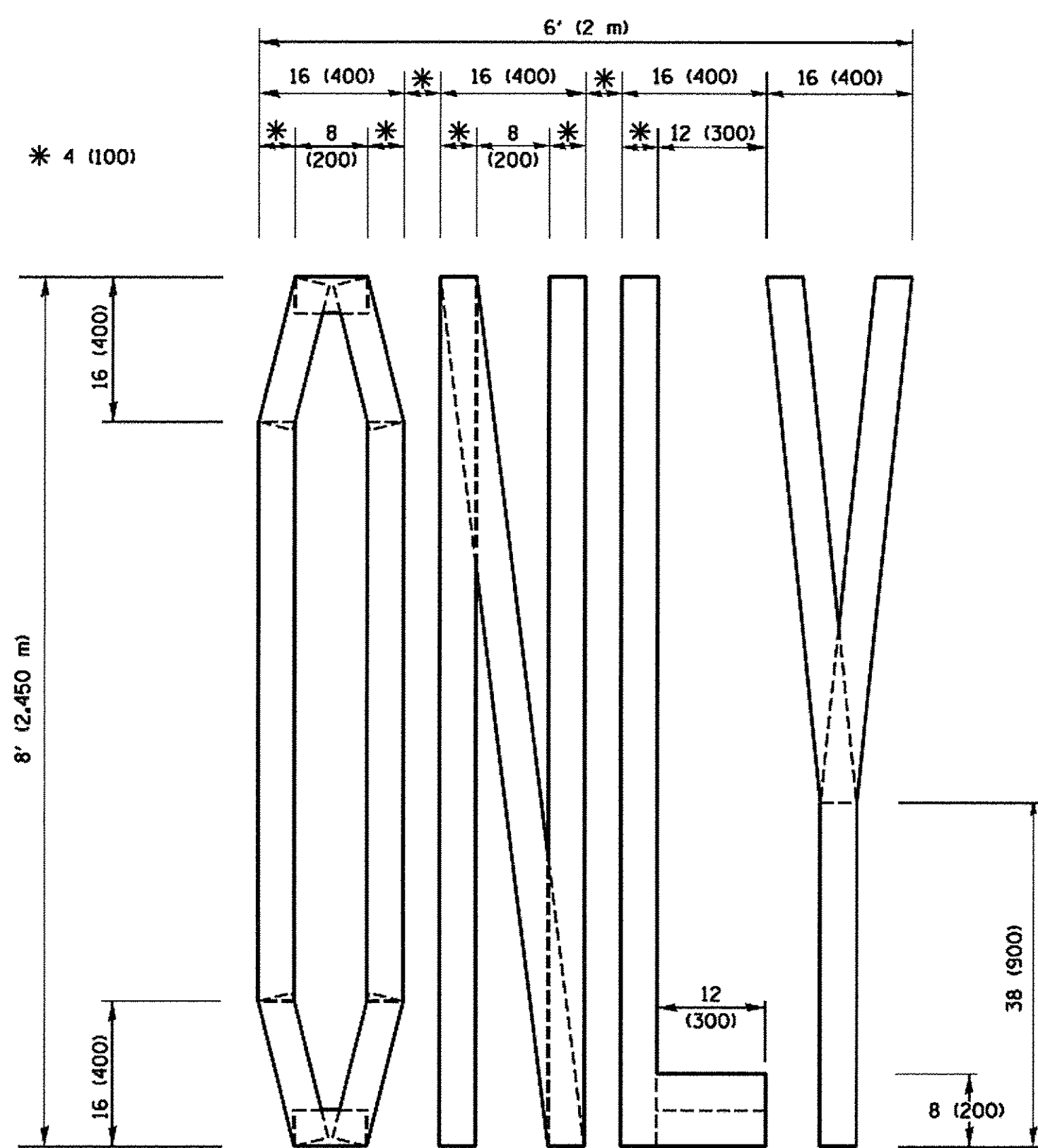
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

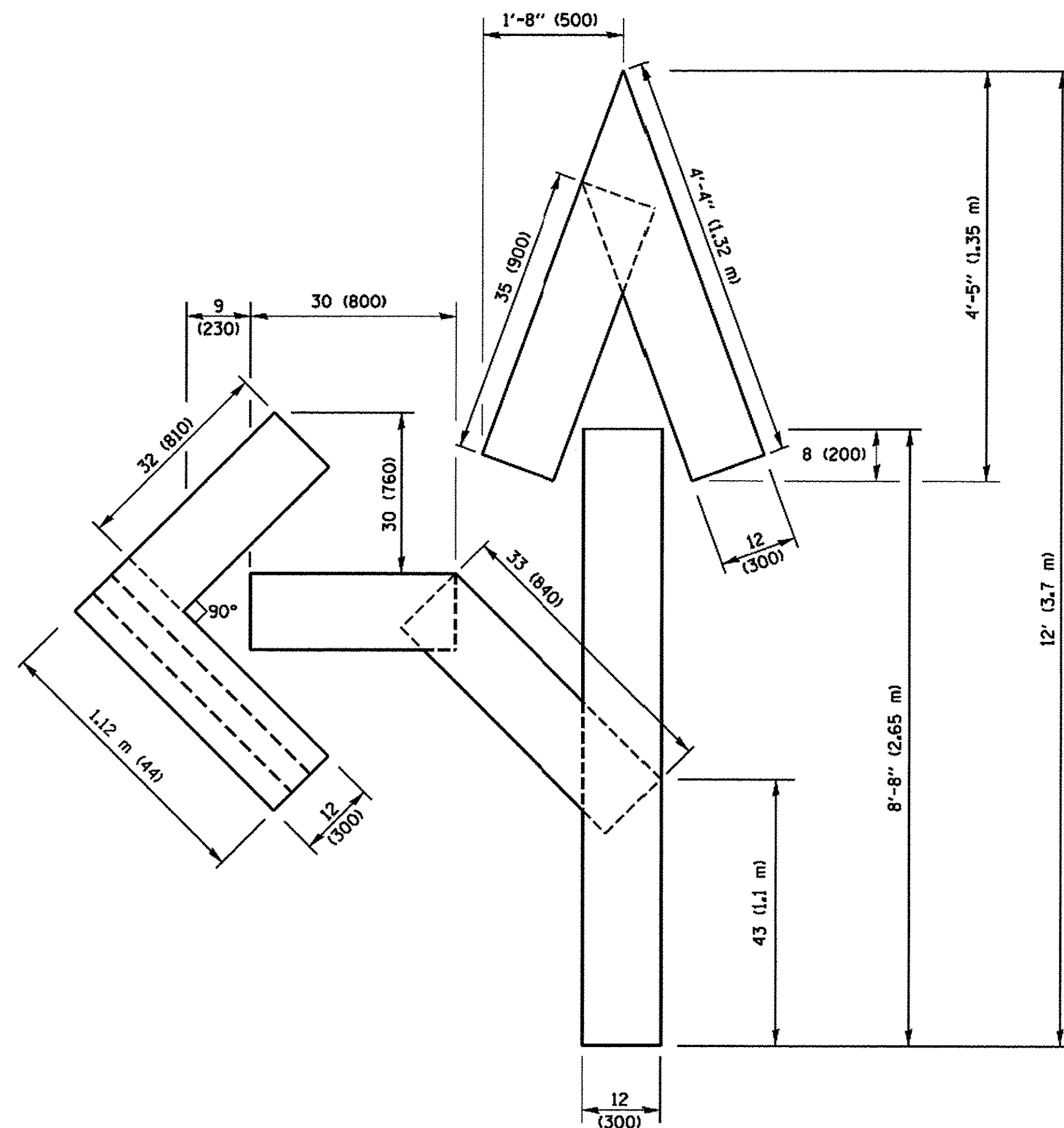
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PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

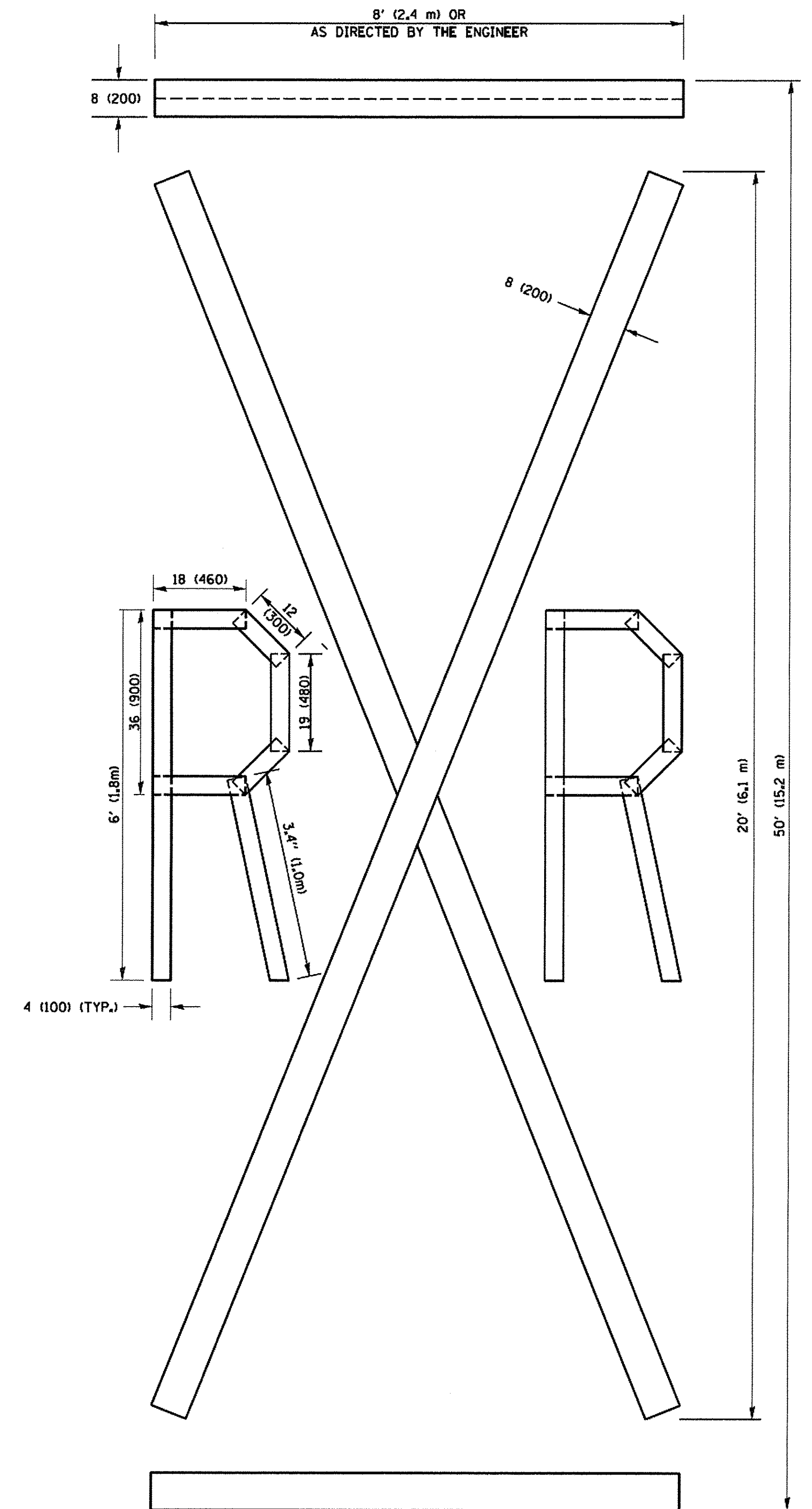


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

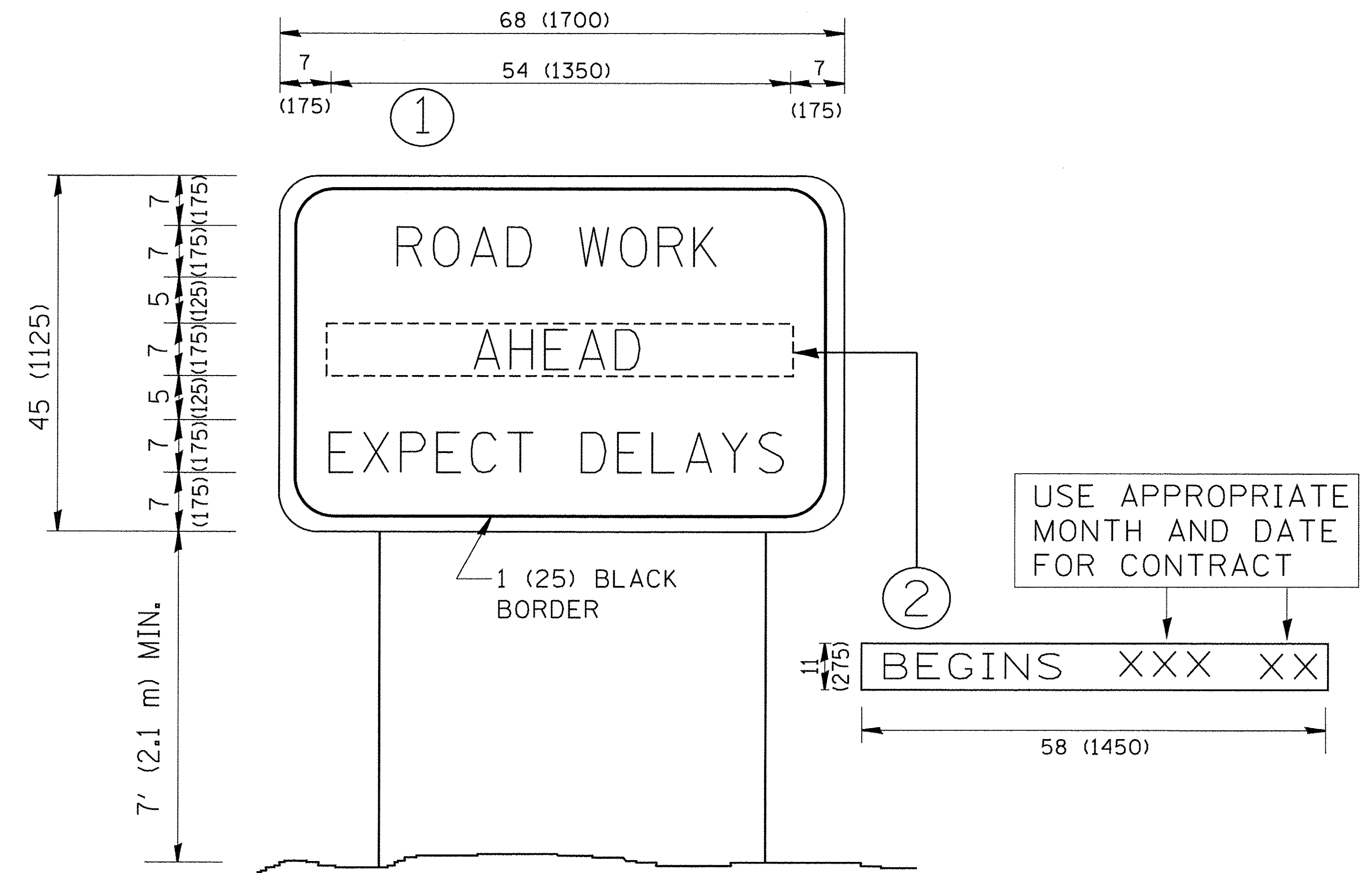
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		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	132
TC-16		CONTRACT NO. 61A30		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

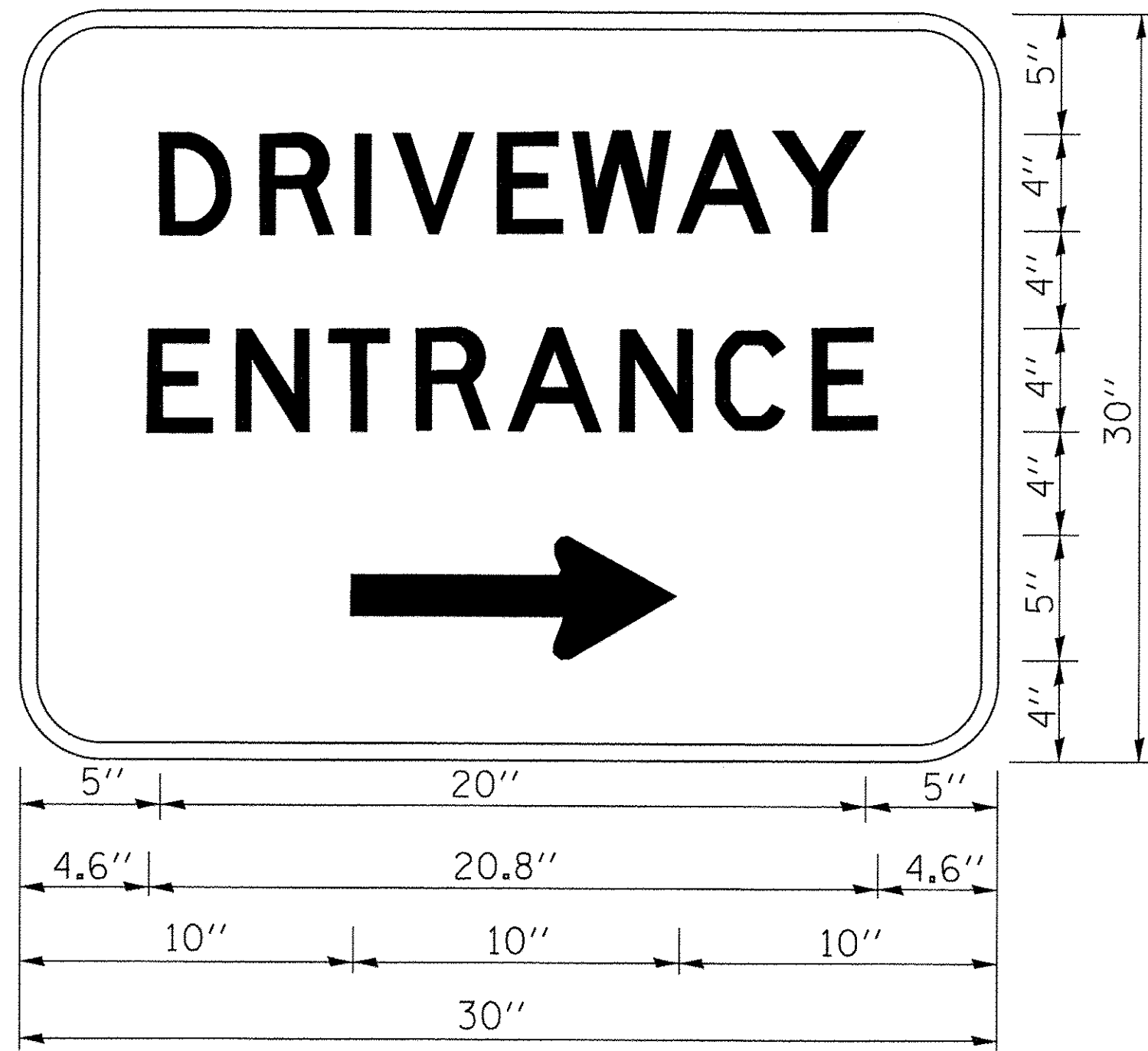
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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	133
TC-22		CONTRACT NO. 61A30		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

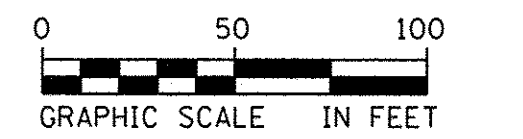
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	134
TC-26			CONTRACT NO. 61A30	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				



IMPROVEMENT BEGINS
STATION 291 + 70.56

295+20.00
55.00' LT
N=1837163.17
E=1029169.25
ELEV=641.83

299+50.00
75.00' LT
N=1837197.21
E=1029598.37
ELEV=648.27

303+00.00
38.00' LT
N=1837171.20
E=1029949.47
ELEV=649.53

SB-S1

95TH STREET

SB-P2

SB-P3

290

SB-P1

300

SB-N2

MATCH LINE STA. 303 + 75

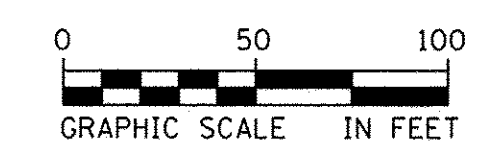
295+80.00
4.00' LT
N=1837114.16
E=1029230.89
ELEV=648.98

301+00.00
72.00' RT
N=1837055.03
E=1029752.98
ELEV=647.41

PLAINFIELD-
NAPERVILLE RD.

NOTE:
SEE DRAWING NO. 2 OF 2 FOR BORING
LOCATIONS ON PLAINFIELD-NAPERVILLE ROAD

CITY OF NAPERVILLE



IMPROVEMENT BEGINS
STATION 308 + 15.00

311+00.00
49.00' LT
N=1837184.46
E=1030755.30
ELEV=663.07

304+50.00
58.00' LT
N=1837195.86
E=1030098.77
ELEV=652.04

SB-N3

307+00.00
52.00' LT
N=1837197.64
E=1030348.84
ELEV=653.39

SB-N5

SB-N7 95TH STREET

CHEYENNE DRIVE

305

310

315

SB-N4

305+00.00
58.00' RT
N=1837081.47
E=1030152.36
ELEV=650.97

309+00.00
49.00' RT
N=1837100.02
E=1030549.82
ELEV=657.04

SB-N6

CITY OF NAPERVILLE

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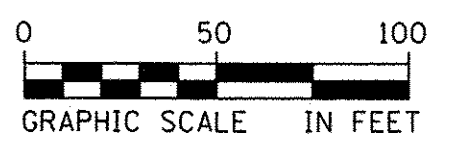


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PLOT DATE = 1/21/2016	CHECKED - JRP	REVISED -
	DATE - 01/20/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

95TH STREET SOIL BORING LOCATION PLAN	
SCALE: 1"=50'	DRAWING NO. 1 OF 2
STA. 291+70.56 TO STA. 318+75	

F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 135
CONTRACT NO. 61A30				ILLINOIS FED. AID PROJECT



IMPROVEMENT BEGINS
STATION 27 + 70.50

PLAINFIELD - NAPERVILLE ROAD

PLAINFIELD - NAPERVILLE ROAD

25

30

35

SB-N1

SB-P2

MATCH LINE STA. 38 + 50

TIMBER CREEK LANE

33+50.00
58.00' RT
N=18376825.03
E=1029717.38
ELEV=645.22

301+00.00
72.80' RT
N=1837055.03
E=1029752.98
ELEV=647.71

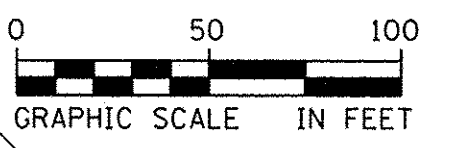
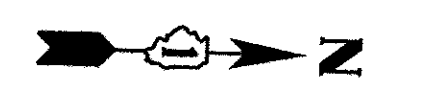
SB-N2

95TH STREET

299+50.00
75.00' LT
N=1837197.21
E=1029598.37
ELEV=648.27

NOTE:
SEE DRAWING NO. 1 OF 2 FOR
BORING LOCATION PLAN
ON 95TH STREET

CITY OF NAPERVILLE



40+20.00
45.00' LT
N=1837491.93
E=1029596.04
ELEV=649.96

MATCH LINE STA. 38 + 50

SB-P17

40

PLAINFIELD - NAPERVILLE ROAD

45

50

SB-P18

PLAINFIELD - NAPERVILLE ROAD

BRECKINRIDGE LANE

43+25.00
38.00' RT
N=1837799.02
E=1029670.94
ELEV=651.98

IMPROVEMENT ENDS
STATION 47 + 26.30

CITY OF NAPERVILLE

FILE NAME = pt:\CDDH\25367281\15\Drawings\Contract\North\Roadway\Soil\Profiles\Shots\Boring 005.dgn



USER NAME = james_push	DESIGNED - JRP	REVISED -
PLOT SCALE = 50.0000' / in.	DRAWN - MRF	REVISED -
PLOT DATE = 1/21/2016	CHECKED - JRP	REVISED -
	DATE - 01/20/2016	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAINFIELD - NAPERVILLE ROAD
SOIL BORING LOCATION PLAN

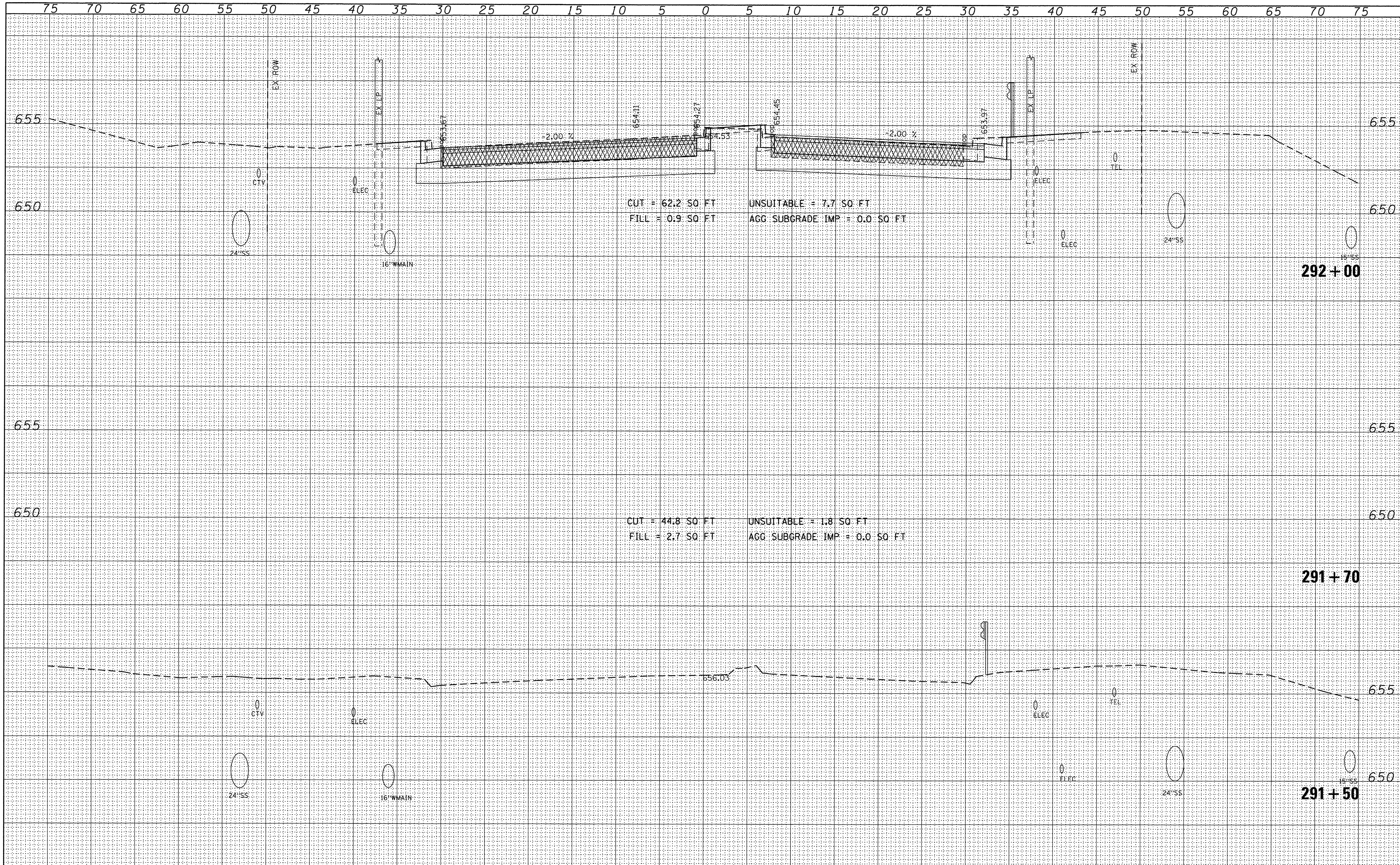
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1644	12-00181-01-FP	WILL	179	136
CONTRACT NO. 61A30				

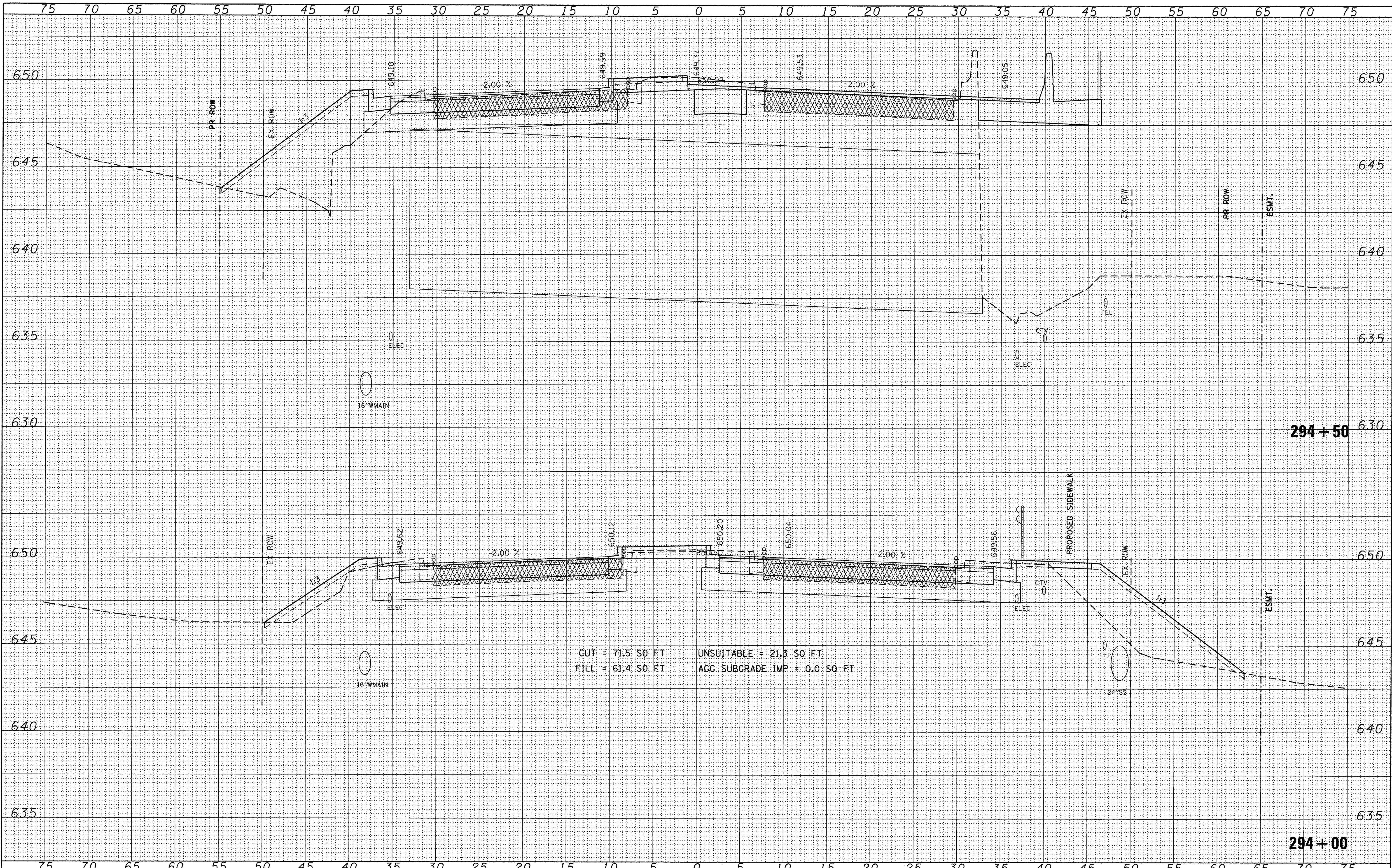
ILLINOIS FED. AID PROJECT

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



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*FILEL#	PLOT SCALE = 5,0000' / 1in.	DRAWN - M.R.F.	REVISED -			SCALE: 1"=5'H; 1"=2.5'V	SHEET NO. 1 OF 22 SHEETS	STA. 291+50	TO STA. 292+00	CONTRACT NO. 61A30		
	PLOT DATE = 1/21/2016	CHECKED - J.R.P.	REVISED -			ILLINOIS FED. AID PROJECT						
	DATE = 01/20/2016	DATE - 01/20/2016	REVISED -									



CUT = 71.5 SQ. FT UNSUITABLE = 21.3 SQ. FT
 FILL = 61.4 SQ. FT AGG. SUBGRADE IMP. = 0.0 SQ. FT

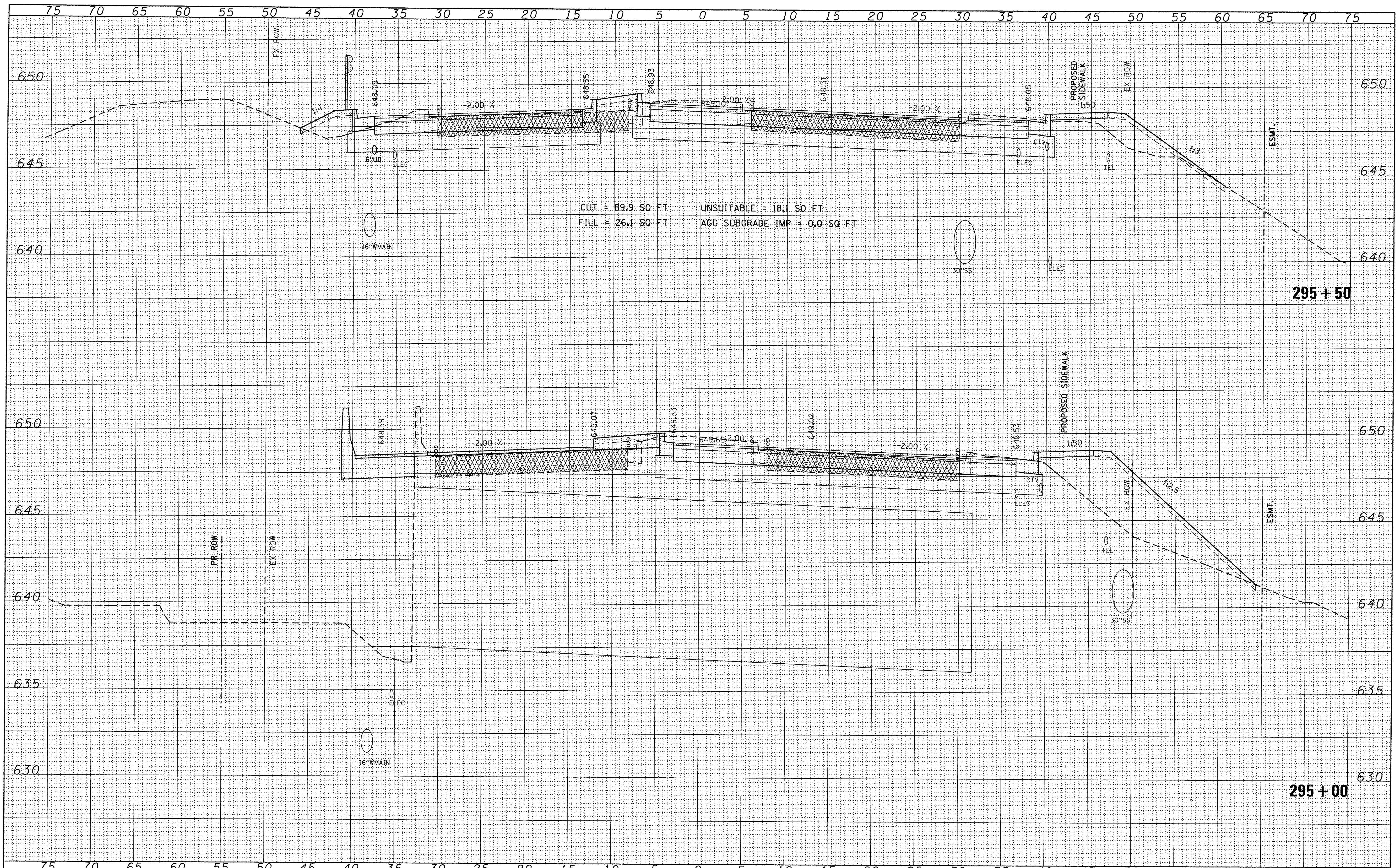
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NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS CHECKED	

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	PLOT DATE = 1/21/2016	CHECKED - J.R.P.	REVISED -								
		DATE - 01/20/2016	REVISED -								

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BY	
SURVEYED	
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NOTE BOOK	
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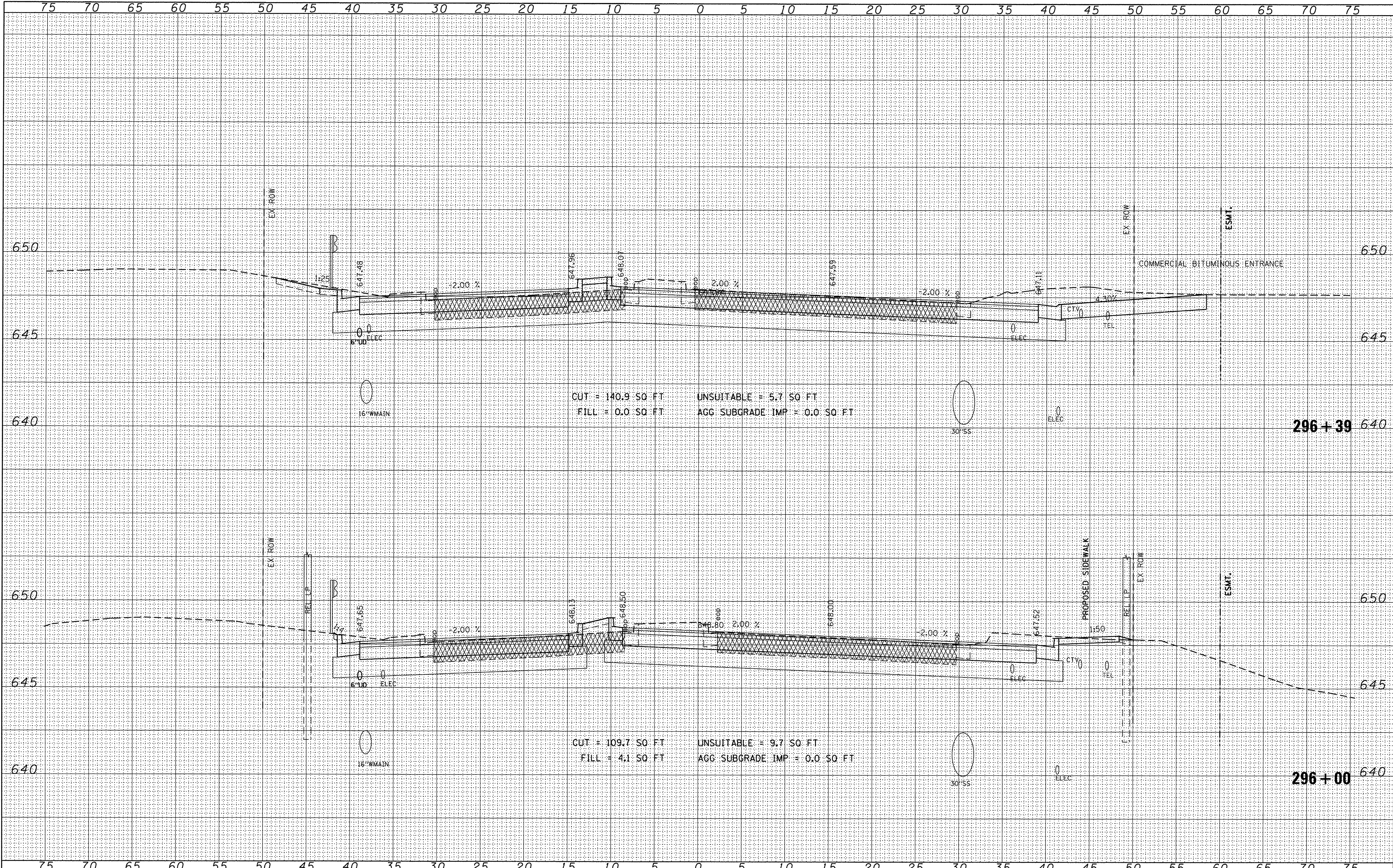
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BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



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 FILL = 26.1 SQ. FT. AGG SUBGRADE IMP = 0.0 SQ. FT.

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		CHECKED - J.R.P.	REVISED -			CONTRACT NO. 61A30					
		DATE - 01/20/2016	REVISED -			ILLINOIS FED. AID PROJECT					
		PLOT SCALE = 5.0000' / 1".		SCALE: 1"=5'H; 1"=2.5'V		SHEET NO. 4 OF 22 SHEETS		STA. 295+00 TO STA. 295+50			

X:\sc\Draw_95th_Age 12/2016 8:19:41 AM



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

FILE NAME =	USER NAME = james_push	DESIGNED - J.A.L.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS 95th STREET	F.A.U. RTE. 1644	SECTION 12-00181-01-FP	COUNTY WILL	TOTAL SHEETS 179	SHEET NO. 141			
#FILE#	PLOT SCALE = 5.0000' / in.	CHECKED - J.R.P.	REVISED -			SCALE: 1"=5'H 1"=2.5'V	SHEET NO. 5 OF 22 SHEETS	STA. 296+00	TO STA. 296+39	CONTRACT NO. 61A30			
	PLOT DATE = 1/21/2016	DATE - 01/20/2016	REVISED -			ILLINOIS FED. AID PROJECT							

X:\a_95th_95b.dwg 1/21/2016 8:19:42 AM