

INDEX OF SHEETS

1	TITLE SHEET
2-7	TYPICAL SECTIONS
7A	DETAIL OF SUBSURFACE DRAINS
8A-8B	SUMMARY OF QUANTITIES
9-13	SCHEDULE OF QUANTITIES
14-23	PLAN & PROFILE SHEETS - FAI 55
24-28	PLAN & PROFILE SHEETS - EAST FRONTAGE ROAD & SERVICE DRIVE
29-30	PLAN & PROFILE SHEETS - C.H. 17
31-32	PLAN & PROFILE SHEETS - T.R. 7 & T.R. 17 (FOR INFORMATION ONLY)
33-36	PROFILE SHEETS - SOUTHBOUND FAI 55
37-40	PROFILE SHEETS - RAMPS
41-42	PROFILE SHEETS - WEST FRONTAGE ROAD & ACCESS ROADS
43-57	C.H. 17 INTERCHANGE PLANS & DETAILS
58-59	TEMPORARY CROSSOVERS
60-80	C.H. 17 BRIDGE PLANS
81-88	SPECIAL CULVERT DETAILS & BOX CULVERT ETCHINGS; INSPECTION WELL DETAIL
89-91	LIGHTING PLANS
92-149	CROSS SECTIONS - FAI 55
150-173	CROSS SECTIONS - EAST FRONTAGE ROAD & SERVICE DRIVE
174-186	CROSS SECTIONS - C.H. 17
187-198C	CROSS SECTIONS - RAMPS
199-206	CROSS SECTIONS - WEST FRONTAGE ROAD & ACCESS ROADS

STANDARDS	1686-3 & 1766-6, 1976, 1981, 1987, 1998, 2051
STANDARDS	1744-2, 2115-3, 2135 & 2143-3
STANDARD	1909-10
STANDARDS	2113-1 & 2130-3
STANDARD	2122-4, 2149-7
STANDARD	2153-8, 2158-6
STANDARD	2169-4
STANDARD	2171
STANDARDS	2224-8
STANDARD	2225-3
STANDARD	2230-6
STANDARD	2231-3
STANDARDS	2235-3 & 2237-6
STANDARD	2241-2
STANDARD	2242-2
STANDARD	2244-2
STANDARD	2246-2
STANDARD	2247-2
STANDARD	2250
STANDARD	2251
STANDARD	2253-3
STANDARDS	2228-1 & 2258-1
STANDARD	2261-3
STANDARD	2262
STANDARDS	2179-4 & 1683-2
STANDARD	2298-2
STANDARD	2299-1
STANDARD	2300
STANDARD	2302-1
STANDARD	2303-2
STANDARD	2305-1
STANDARD	2314-1
STANDARD	2315-2

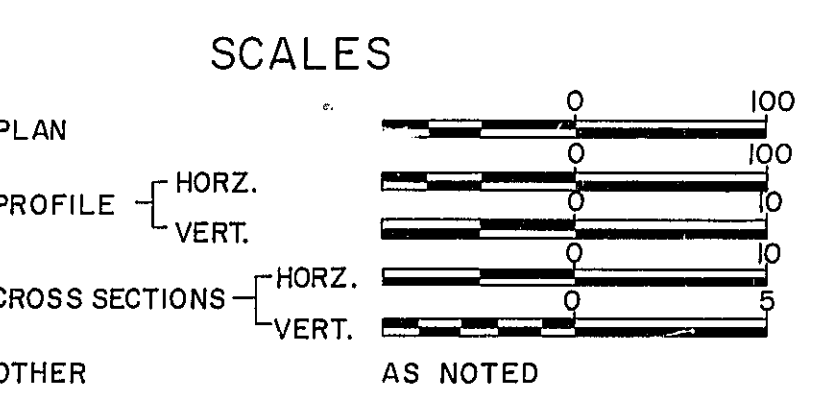
DESIGN DESIGNATION

C.H. 17
634 (79)-C-O.387(P.C.C.-20)
MAXIMUM GRADE = +2.00%
LENGTH OF MAXIMUM GRADE = 1100 FT.
MINIMUM HORIZONTAL RADIUS = 716.20 FT.
MINIMUM STOPPING SIGHT DISTANCE = 460 FT.

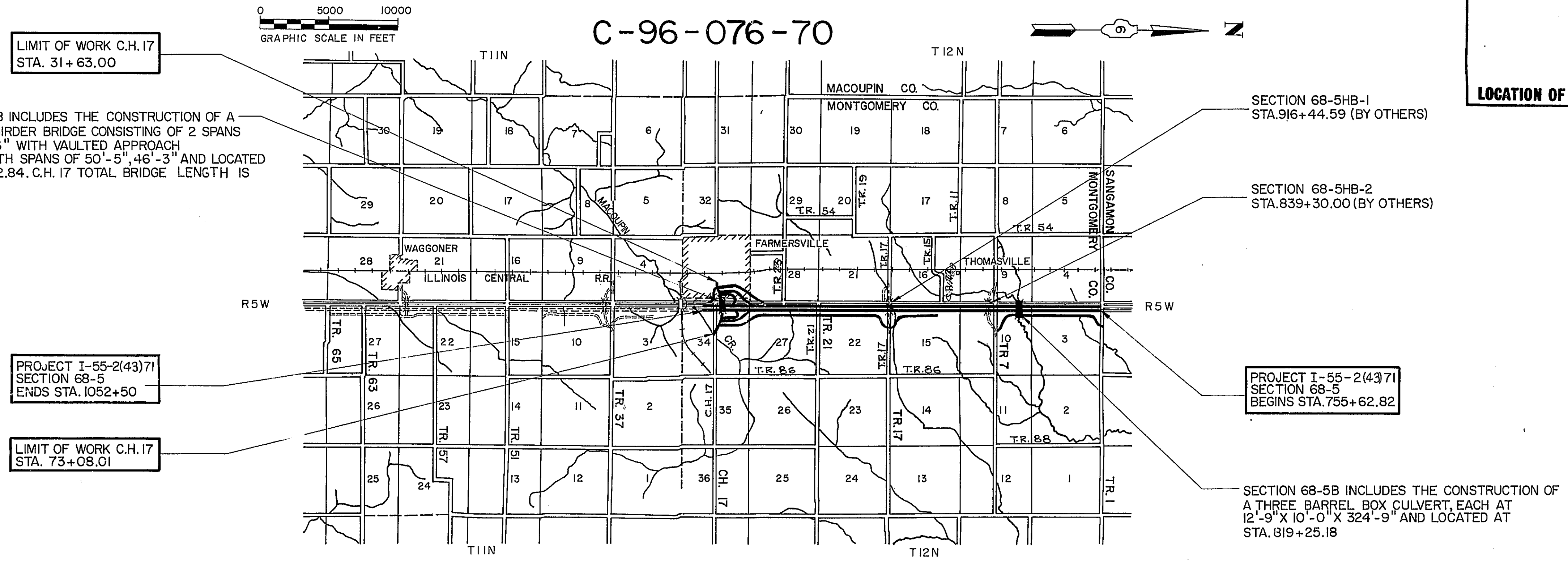
F.A.I. 55
3361 (90)-A-1 TRUNK-I556 (C.R.C.-20)
MAXIMUM GRADE = +1.409%
LENGTH OF MAXIMUM GRADE = 355 FT.
MINIMUM HORIZONTAL RADIUS = INFINITE LENGTH
MINIMUM STOPPING SIGHT DISTANCE = 850 FT.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID INTERSTATE HIGHWAY
ROUTE 55

PROJECT I-55-2(43)71
SECTIONS 68-5, 68-5HB, 68-5B



MONTGOMERY COUNTY
ROADWAY CONSTRUCTION
C-96-076-70



SECTION 68-5HB INCLUDES THE CONSTRUCTION OF A STEEL PLATE GIRDER BRIDGE CONSISTING OF 2 SPANS @ 101'-6", 101'-6" WITH VAULTED APPROACH ABUTMENTS WITH SPANS OF 50'-5", 46'-3" AND LOCATED AT STA. 1044+12.84. C.H. 17 TOTAL BRIDGE LENGTH IS 299.79 FT.

PROJECT I-55-2(43)71
SECTION 68-5
ENDS STA. 1052+50

LIMIT OF WORK C.H. 17
STA. 73+08.01

SECTION 68-5HB-1
STA. 916+44.59 (BY OTHERS)

SECTION 68-5HB-2
STA. 839+30.00 (BY OTHERS)

PROJECT I-55-2(43)71
SECTION 68-5
BEGINS STA. 755+62.82

SECTION 68-5B INCLUDES THE CONSTRUCTION OF A THREE BARREL BOX CULVERT, EACH AT 12'-9" X 10'-0" X 32'-9" AND LOCATED AT STA. 819+25.18

PROJECT I-55-2(43)71 & SECTION 68-5 GROSS LENGTH = 29,687.18 FT. = 5.623 MILES

PLANS PREPARED BY
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILLINOIS

* 68-5, 68-5HB, 68-5B

FEDERAL-AID ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-55	*	MONTGOMERY	206	1
R. P. R. REG. NO. 1 ILLINOIS PROJECT 1-55-2(43)71				



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

SUBMITTED: 12-10-70

EXAMINED: 3-4-71

PASSED: 3-4-71

APPROVED: 3-4-71

APPROVED: 3-4-71

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

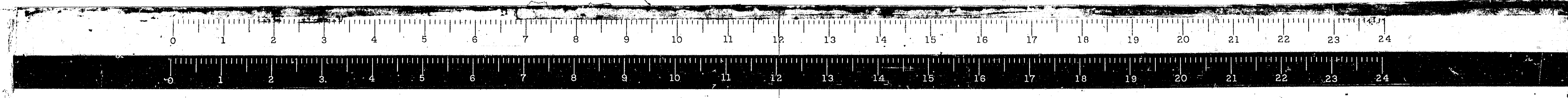
APPROVED

DIVISION ENGINEER

DATE

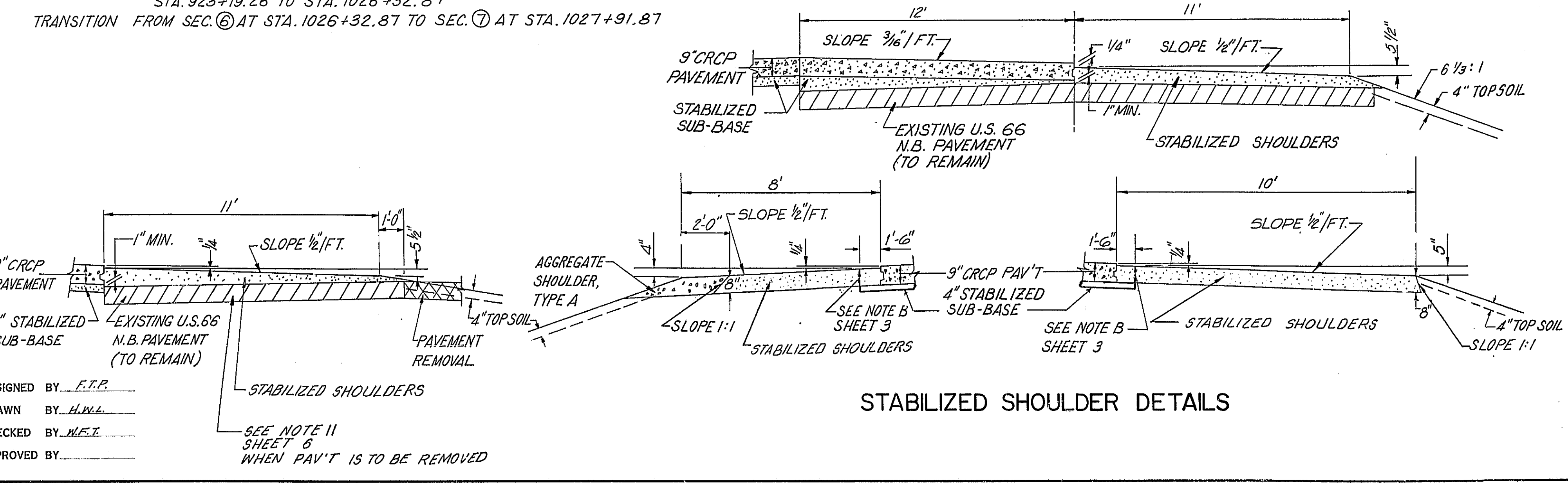
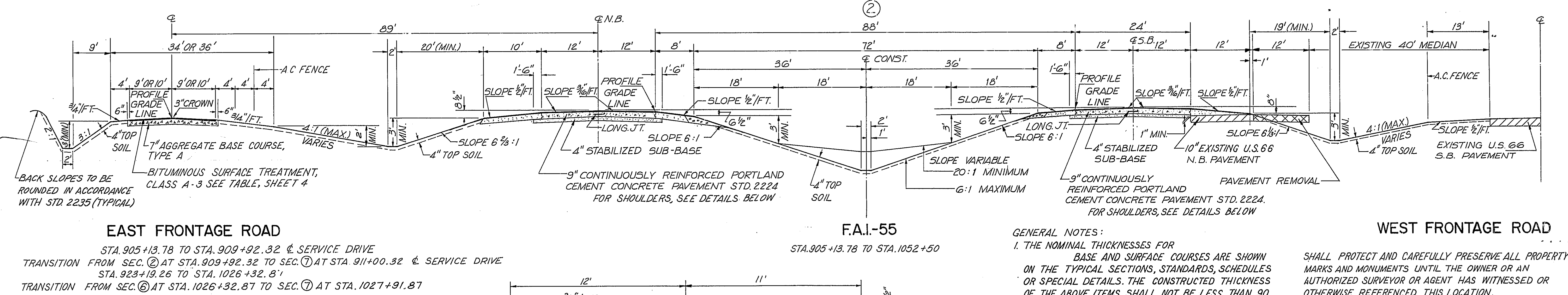
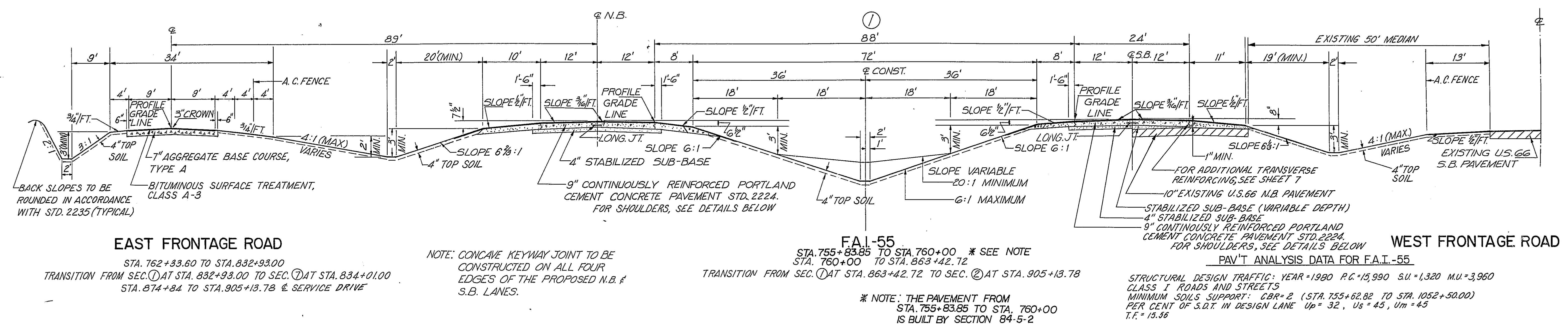
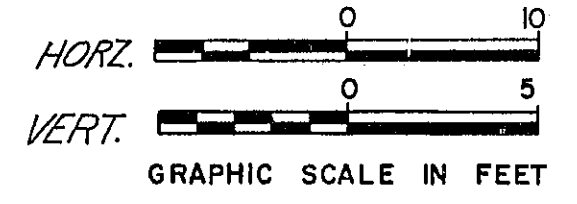
CONTRACT NO. 23418

MONTGOMERY COUNTY SECTION 68-5, 68-5HB, 68-5B FA. ROUTE I-55



* 68-5, 69-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	*	MONT.	206	2
FED ROAD DIST. NO. 4 ILLINOIS PROJ.				



- GENERAL NOTES:**
1. THE NOMINAL THICKNESSES FOR BASE AND SURFACE COURSES ARE SHOWN ON THE TYPICAL SECTIONS, STANDARDS, SCHEDULES OR SPECIAL DETAILS. THE CONSTRUCTED THICKNESS OF THE ABOVE ITEMS SHALL NOT BE LESS THAN 90 PER CENT OF THE NOMINAL THICKNESS AT ANY LOCATION.
 2. THE THICKNESS OF BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
 3. IN AREAS WHERE THE NEW PAVEMENT IS TOTALLY OFFSET FROM THE PRESENT NORTHBOUND LANE, THE EDGE OF THE EXISTING PAVEMENT IS TO BE USED AS A FORM LINE REGARDLESS OF THE NEW PAVEMENT'S ALIGNMENT.
 4. CONCRETE FOUNDATIONS, CONCRETE SLABS, EXISTING CULVERTS, HEADWALLS TO BE REMOVED, ETC. SHALL BE CONSIDERED AS INCIDENTAL TO EARTH EXCAVATION IN ACCORDANCE WITH SECTION 202 OF THE STANDARD SPECIFICATIONS. CULVERTS TO BECOME PROPERTY OF CONTRACTOR.
 5. WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR

SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS UNTIL THE OWNER OR AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THIS LOCATION.

6. THE CROWN SHALL BE REMOVED FROM ALL PAVEMENTS BUILT WITH SUPERELEVATION. THE CROWN SHALL BE REMOVED OR INCORPORATED IN THE LENGTH OF SUPERELEVATION ATTAINMENT.

7. SEE STD. NO. 2224 & 2225 FOR PAVEMENT & REINFORCEMENT DETAILS.

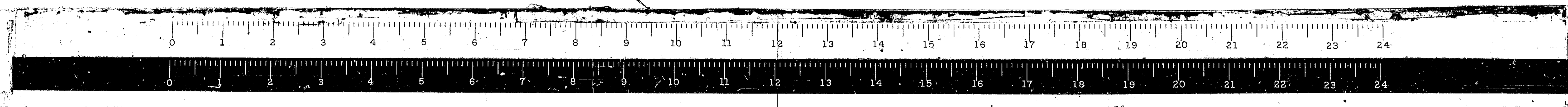
8. ALL EXISTING FLEXIBLE MEDIAN CROSS OVERS SHALL BE REMOVED. WORK CONSIDERED INCIDENTAL TO EARTH EXCAVATION.

9. SEE ADDITIONAL NOTES ON SHEET 6.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

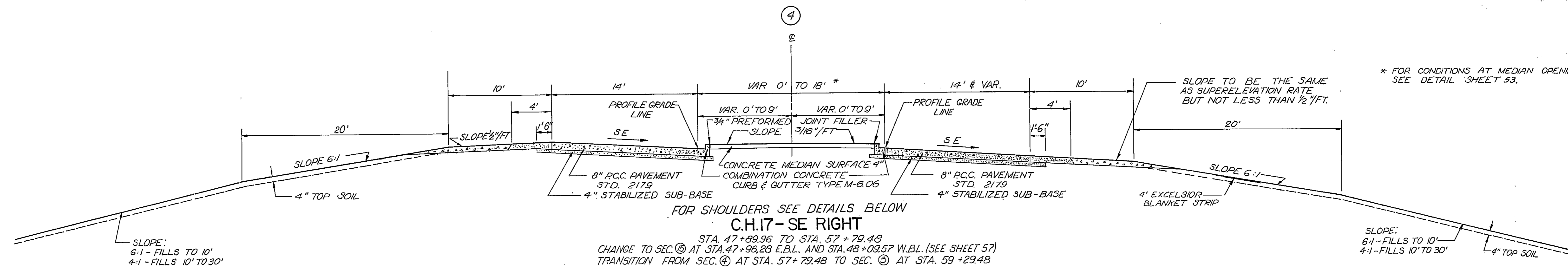
TYPICAL SECTIONS

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



* 68-5, 68-54B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 33	#	MONT.	206	3
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



FOR SHOULDERS SEE DETAILS BELOW
C.H. 17-SE RIGHT
 STA. 47+69.96 TO STA. 57+79.48
 CHANGE TO SEC. ② AT STA. 47+96.28 E.B.L. AND STA. 48+09.57 W.B.L. (SEE SHEET 57)
 TRANSITION FROM SEC. ④ AT STA. 57+79.48 TO SEC. ③ AT STA. 59+29.48

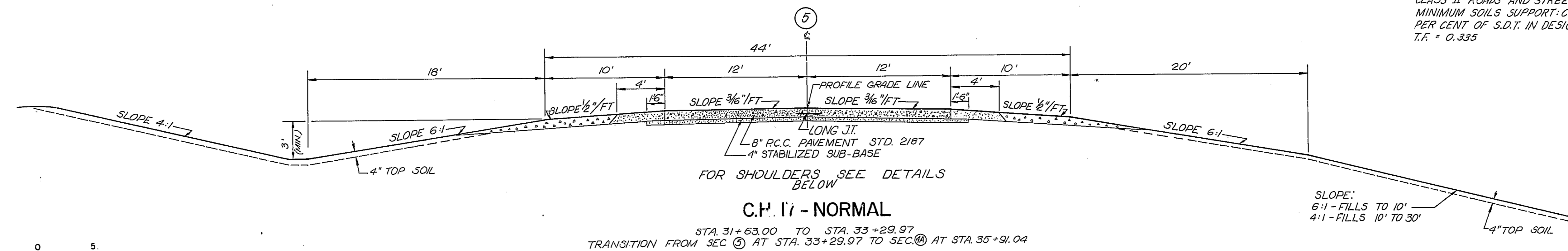
* FOR CONDITIONS AT MEDIAN OPENING.
 SEE DETAIL SHEET 53.

SLOPE: 6:1 - FILLS TO 10'
 4:1 - FILLS 10' TO 30'

C.H. 17-SE LEFT

STA. 35+91.04 TO STA. 38+53.64
 TRANSITION FROM SEC. ④ AT STA. 38+53.64 TO SEC. ③ AT STA. 39+53.64
 STA. 62+80.16 TO STA. 68+80.00
 TRANSITION FROM SEC. ④ AT STA. 68+80.00 TO SEC. ③ AT STA. 71+00.00 (PARTIAL S.E.)

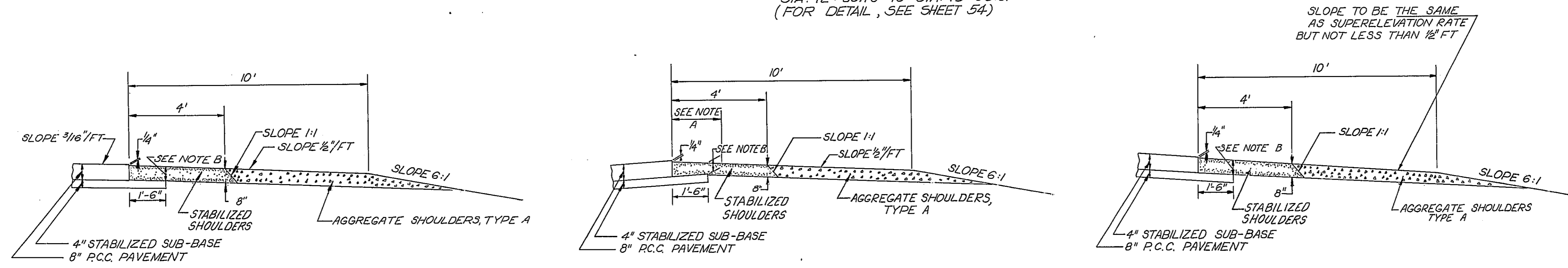
PAV'T ANALYSIS DATA FOR C.H. 17
 STRUCTURAL DESIGN TRAFFIC: YEAR 1979 PC=2432 U.S.=578 MU=30
 CLASS II ROADS AND STREETS
 MINIMUM SOILS SUPPORT: CBR=2 (STA. 13+63.00 TO STA. 73+08.01)
 PER CENT OF S.D.T. IN DESIGN LANE: U_p=50, U_s=50, U_m=50
 T.F. = 0.335



FOR SHOULDERS SEE DETAILS BELOW
C.H. 17-NORMAL
 STA. 31+63.00 TO STA. 33+29.97
 TRANSITION FROM SEC. ② AT STA. 33+29.97 TO SEC. ④ AT STA. 35+91.04

SLOPE: 6:1 - FILLS TO 10'
 4:1 - FILLS 10' TO 30'

TRANSITION FROM PARTIAL S.E. AT STA. 71+00 TO NORMAL AT STA. 72+85.70
 STA. 72+85.70 TO STA. 73+08.01
 (FOR DETAIL, SEE SHEET 54.)



NOTE:

- WHEN THE SUPERELEVATION OF PAVEMENT EXCEEDS 0.04 FT/FT, A 2 FOOT PORTION OF THE SHOULDER THAT IS ADJACENT TO PAVEMENT SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 0.08 FT/FT.
- THICKNESS WILL VARY WITH THE THICKNESS OF PAVEMENT, EXTENDED LENGTH OF SUB-BASE, AND THE SLOPE OF THE PAVEMENT. WHEN THIS THICKNESS IS LESS THAN 8 INCHES THE STABILIZED SHOULDER AND SUB-BASE SHALL BE STEPPED DOWN AT THIS LINE TO PROVIDE AN 8 INCH MINIMUM DEPTH STABILIZED SHOULDER.

NORMAL SE-OUTSIDE SE-INSIDE

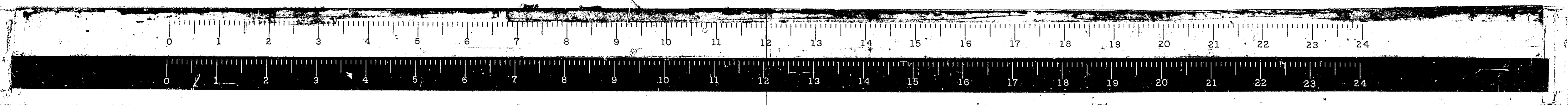
C.H. 17 SHOULDER DETAILS

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

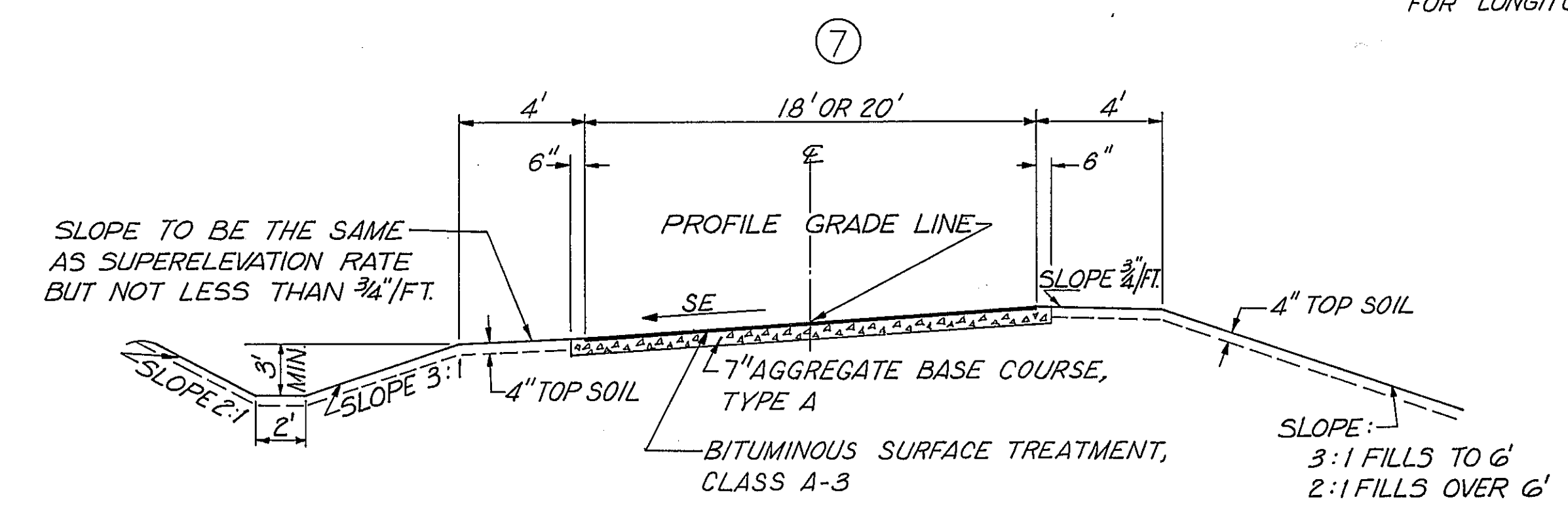
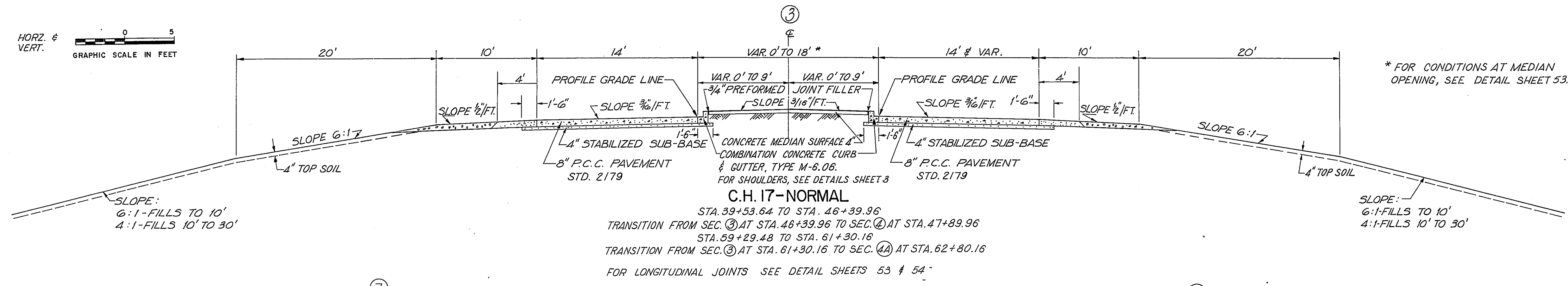
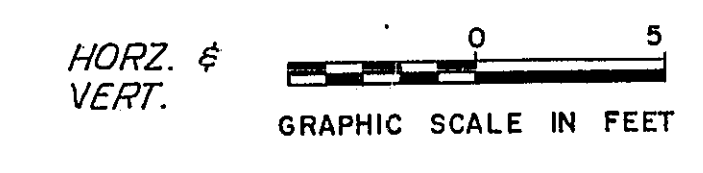
TYPICAL SECTIONS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

DESIGNED BY F.T.P.
 DRAWN BY B.K.
 CHECKED BY W.F.T.
 APPROVED BY _____

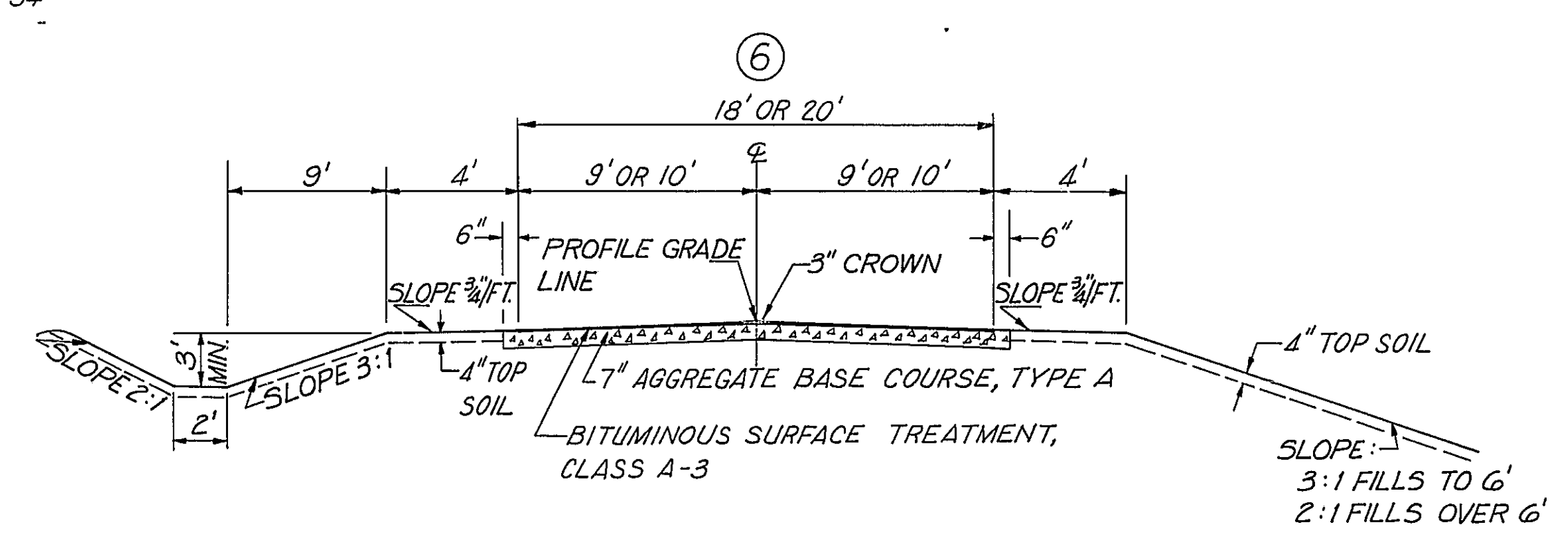


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	4
ROAD DIST. NO. 4		ILLINOIS	PROJ.	

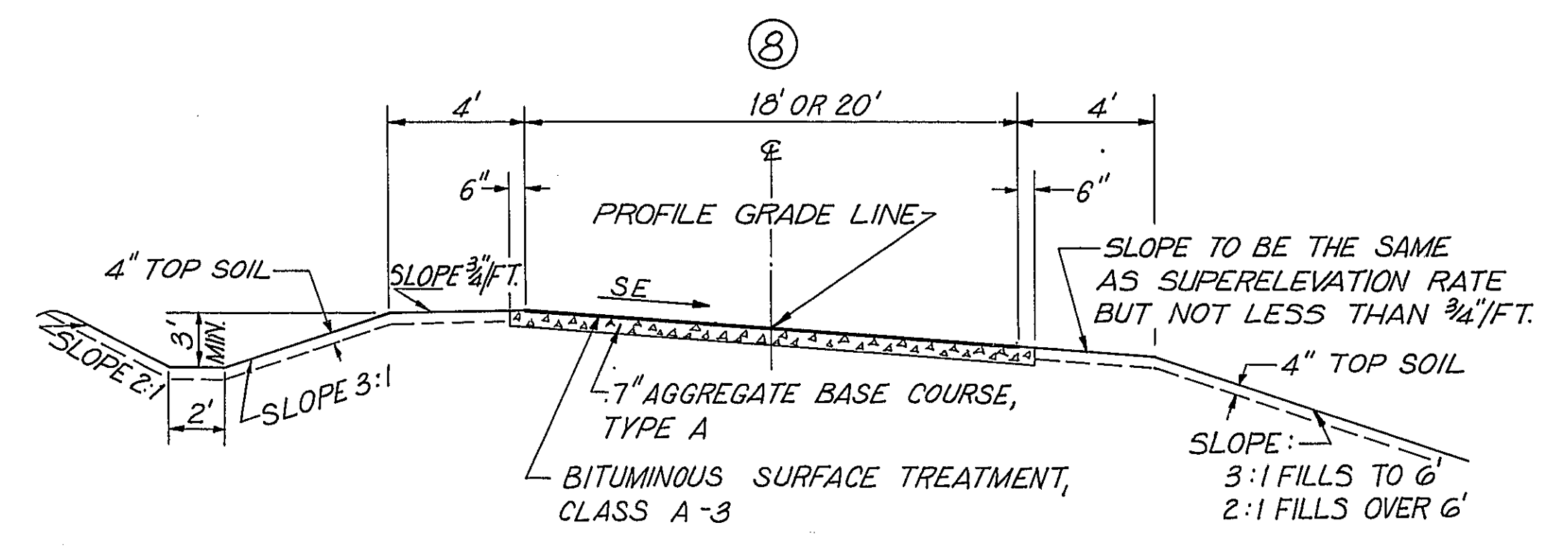


STA. 758+44.17 TO STA. 761+25.60
 TRANSITION FROM SEC. ⑦ AT STA. 761+25.60 TO SEC. ⑧ AT STA. 762+33.60
 STA. 834+01.00 TO STA. 837+61.43
 TRANSITION FROM SEC. ⑦ AT STA. 837+61.43 TO SEC. ⑧ AT STA. 838+63.43
 STA. 911+00.32 TO STA. 914+53.08 & SERVICE DRIVE
 TRANSITION FROM SEC. ⑦ AT STA. 914+53.08 TO SEC. ⑧ AT STA. 916+74.32 & SERVICE DRIVE

STA. 918+57.18 TO STA. 922+11.26
 TRANSITION FROM SEC. ⑦ AT STA. 922+11.26 TO SEC. ⑧ AT STA. 923+19.26
 STA. 1027+91.87 TO STA. 1033+76.65
 TRANSITION FROM SEC. ⑦ AT STA. 1033+76.65 TO SEC. ⑧ AT STA. 1035+35.65



STA. 752+31.01 TO STA. 753+24.48
 TRANSITION FROM SEC. ⑥ AT STA. 753+24.48 TO SEC. ⑥ AT STA. 754+32.48
 STA. 838+69.43 TO STA. 839+19.20
 TRANSITION FROM SEC. ⑥ AT STA. 839+19.20 TO SEC. ⑥ AT STA. 840+27.20
 STA. 919+91.18 TO STA. 920+81.52 & SERVICE DRIVE
 STA. 912+33.94 TO STA. 913+33.67
 TRANSITION FROM SEC. ⑥ AT STA. 913+33.67 TO SEC. ⑥ AT STA. 914+41.67
 STA. 1035+35.65 TO STA. 1037+15.11
 TRANSITION FROM SEC. ⑥ AT STA. 1037+15.11 TO SEC. ⑥ AT STA. 1039+07.11
 STA. 1050+67.30 TO STA. 1052+15.09
 STA. 10+26.22 TO STA. 10+99.12 & E. ACCESS ROAD
 TRANSITION FROM SEC. ⑥ AT STA. 10+99.12 TO SEC. ⑥ AT STA. 11+99.12 & E. ACCESS ROAD



STA. 754+32.48 TO STA. 755+94.76
 TRANSITION FROM SEC. ⑧ AT STA. 755+94.76 TO SEC. ⑧ AT STA. 758+44.17
 STA. 840+27.20 TO STA. 841+05.20
 TRANSITION FROM SEC. ⑧ AT STA. 840+27.20 TO SEC. ⑧ AT STA. 842+12.89
 STA. 916+74.32 TO STA. 918+83.18 & SERVICE DRIVE
 TRANSITION FROM SEC. ⑧ AT STA. 918+83.18 TO SEC. ⑧ AT STA. 919+91.18 & SERVICE DRIVE

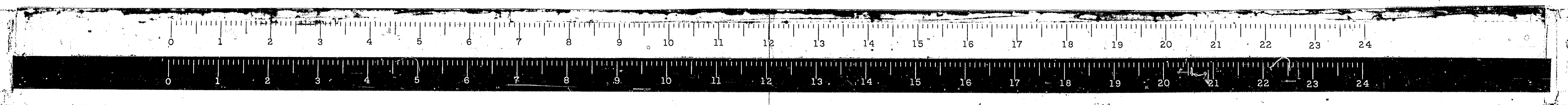
STA. 914+41.67 TO STA. 916+40.65
 TRANSITION FROM SEC. ⑧ AT STA. 916+40.65 TO SEC. ⑧ AT STA. 918+57.18
 STA. 1039+07.11 TO STA. 1048+75.30
 TRANSITION FROM SEC. ⑧ AT STA. 1048+75.30 TO SEC. ⑧ AT STA. 1050+67.30
 STA. 11+99.12 TO STA. 14+65.48 & E. ACCESS ROAD
 TRANSITION FROM SEC. ⑧ AT STA. 14+65.48 TO SEC. ⑧ AT STA. 15+75 & E. ACCESS ROAD

EAST FRONTAGE ROAD			
LOCATION & STATIONING	PAV'T WIDTH	SHLD. WIDTH	A.D.T. 1979
E. FR. RD. #1, 752+31.01 TO 842+12.89	18'	4'	52
SERV. DR. #1, 874+84 TO 920+81.52	18'	4'	—
E. FR. RD. #2, 912+33.94 TO 1052+15.09	20'	4'	87
E. ACCESS RD., 10+26.22 TO 15+75	18'	4'	—

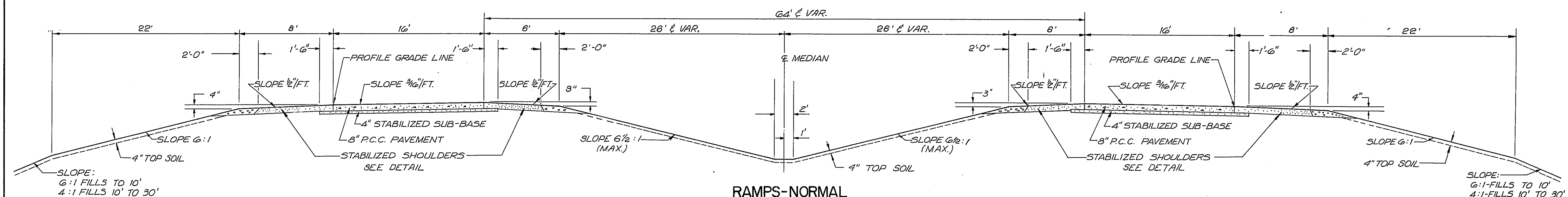
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

TYPICAL SECTIONS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



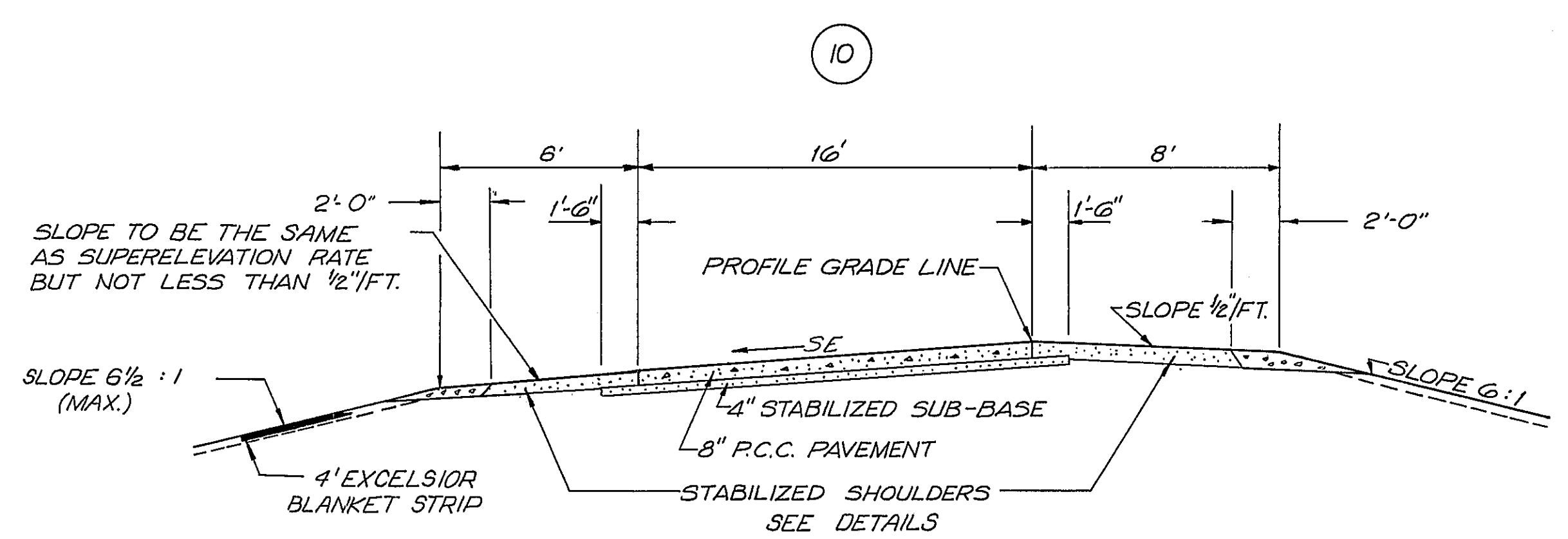
*68-5, 68-5HB, 65-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	5
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



RAMPS-NORMAL

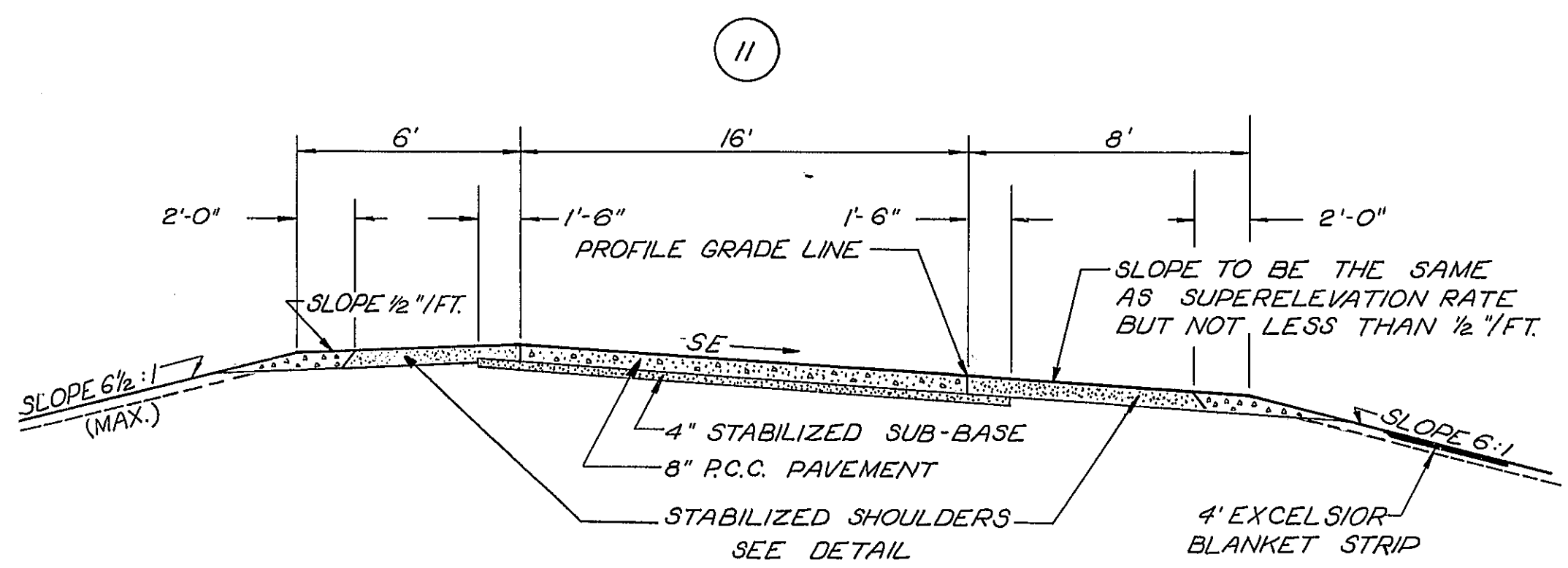
STA. 31+61.43 TO STA. 34+46.98 RAMP "A"
 STA. 11+26.48 TO STA. 14+42.54 RAMP "B"
 TRANSITION FROM SEC. (9) AT STA. 14+42.54 TO SEC. (1) AT STA. 15+98.54 RAMP "B"
 STA. 29+46.02 TO STA. 32+37.88 RAMP "C"
 STA. 11+07.88 TO STA. 14+50.69 RAMP "D"
 TRANSITION FROM SEC. (9) AT STA. 14+50.69 TO SEC. (1) AT STA. 16+08.69 RAMP "D"

PAV'T ANALYSIS DATA FOR RAMPS
 STRUCTURAL DESIGN TRAFFIC YEAR 1979 P.C. = 2432 U.S. = 570 MU = 30
 CLASS II ROADS AND STREETS
 MINIMUM SOILS SUPPORT: CBR = 2 (THROUGHOUT)
 PER CENT OF S.D.T. IN DESIGN LANE $U_p = 50$, $U_s = 50$, $U_m = 50$
 T.F. = 0.335



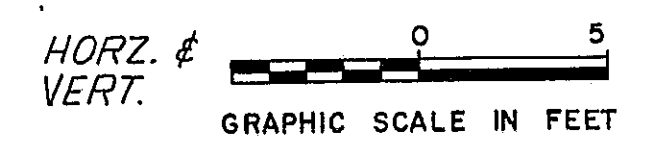
RAMP - SE LEFT

STA. 26+28.81 TO STA. 30+05.43 RAMP "A"
 TRANSITION FROM SEC. (1) AT STA. 30+05.43 TO SEC. (9) AT STA. 31+61.43 RAMP "A"
 STA. 18+08.89 TO STA. 20+05.80 RAMP "D"
 TRANSITION FROM SEC. (1) AT STA. 20+05.80 TO SEC. (1) AT STA. 23+53.82 RAMP "D"

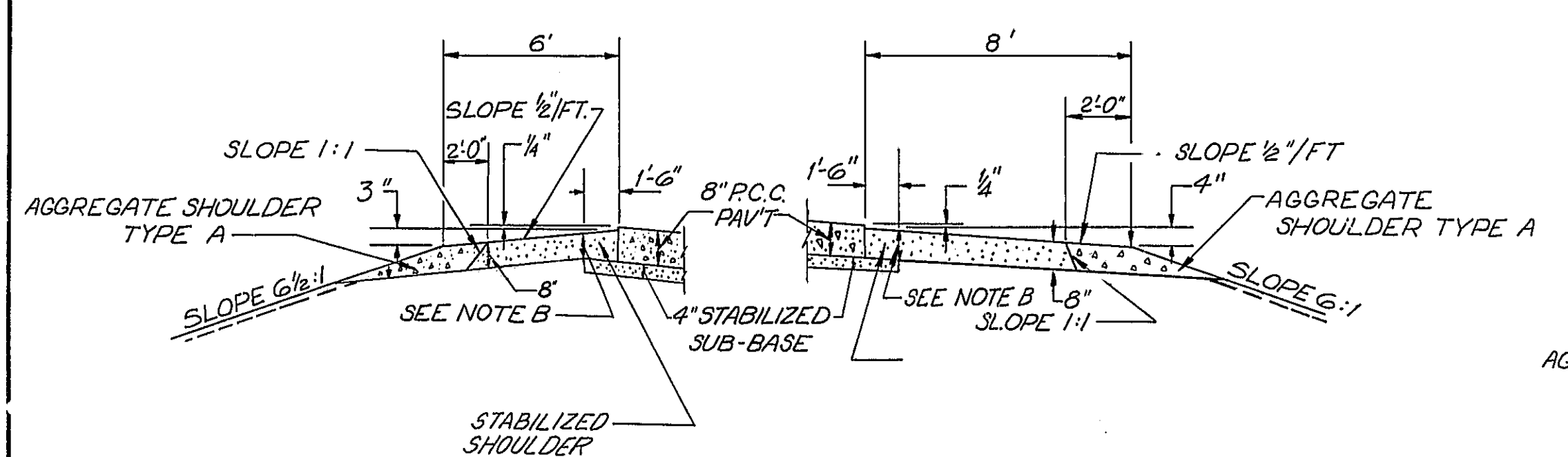


RAMP - SE RIGHT

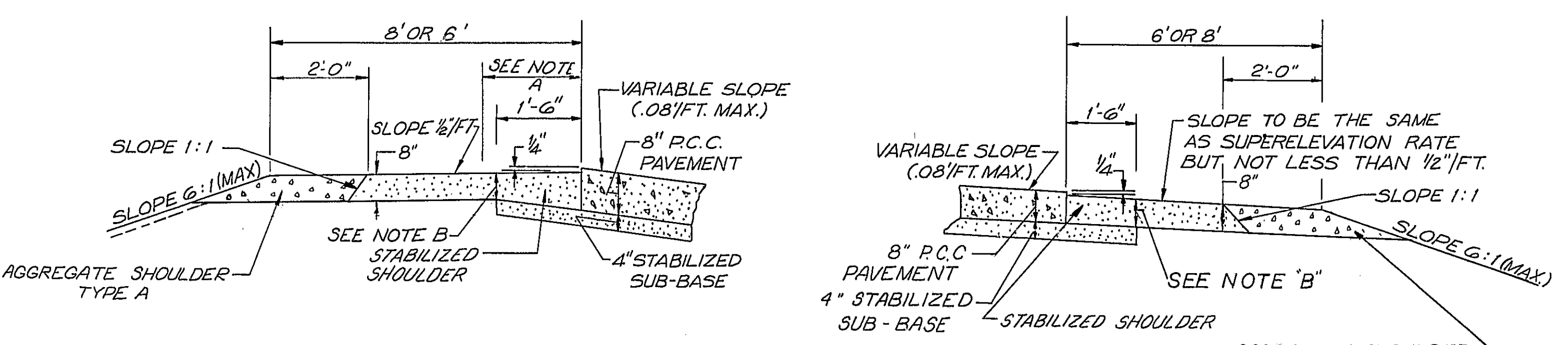
TRANSITION FROM RAMP TERMINAL AT STA. 13+03.10 TO SEC. (1) AT STA. 16+62.40 RAMP "A"
 STA. 16+62.40 TO STA. 22+78.81 RAMP "A"
 TRANSITION FROM SEC. (1) AT STA. 22+78.81 TO SEC. (1) AT STA. 26+28.81 RAMP "A"
 STA. 15+98.54 TO STA. 25+76.84 RAMP "B"
 TRANSITION FROM SEC. (1) AT STA. 25+76.84 TO RAMP TERMINAL AT STA. 26+72.84 RAMP "B"
 TRANSITION FROM RAMP TERMINAL AT STA. 13+03.10 TO SEC. (1) AT STA. 16+62.39 RAMP "C"
 STA. 16+62.39 TO STA. 27+89.62 RAMP "C"
 TRANSITION FROM SEC. (1) AT STA. 27+89.62 TO SEC. (9) AT STA. 29+46.02 RAMP "C"
 STA. 23+53.82 TO STA. 29+58 RAMP "D"
 TRANSITION FROM SEC. (1) AT STA. 29+58 TO RAMP TERMINAL AT STA. 30+54 RAMP "D"



- NOTES:**
- WHEN THE SUPERELEVATION OF PAVEMENT EXCEEDS 0.04 FT/FT A 2 FOOT PORTION OF THE SHOULDER THAT IS ADJACENT TO PAVEMENT SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 0.08 FT/FT
 - THICKNESS WILL VARY WITH THE THICKNESS OF PAVEMENT, EXTENDED LENGTH OF SUB-BASE, AND THE SLOPE OF PAVEMENT. WHEN THIS THICKNESS IS LESS THAN 8 INCHES THE STABILIZED SHOULDER AND SUB-BASE SHALL BE STEPPED DOWN AT THIS LINE TO PROVIDE AN 8 INCH MINIMUM DEPTH STABILIZED SHOULDER



STABILIZED SHOULDER DETAILS-NORMAL



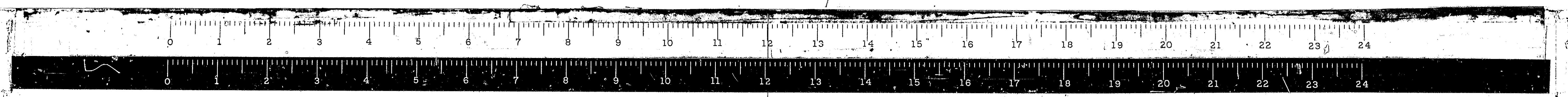
STABILIZED SHOULDER DETAILS-SUPERELEVATED

DESIGNED BY: F.T.P.
 DRAWN BY: H.W.L.
 CHECKED BY: W.E.T.
 APPROVED BY:

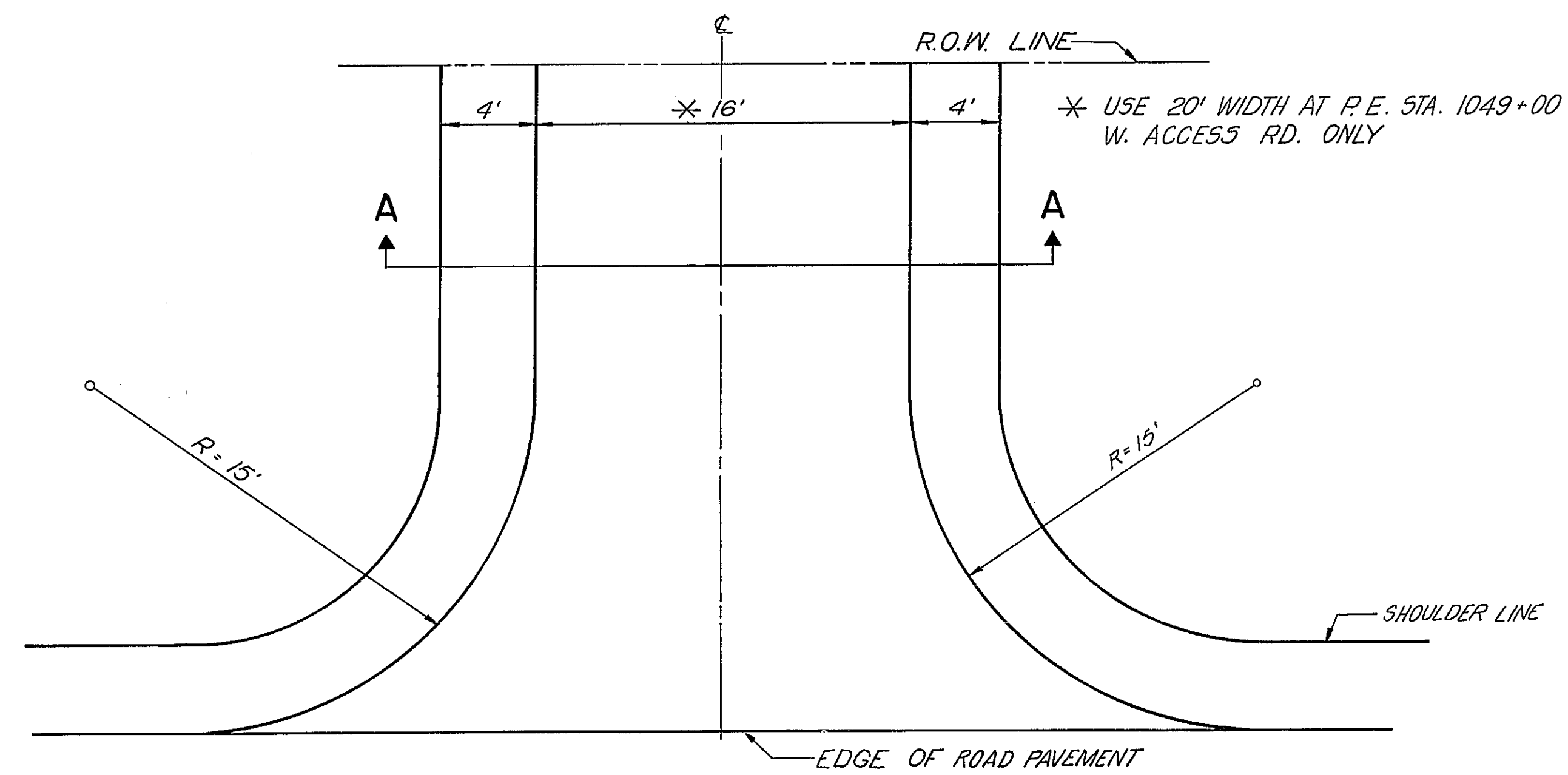
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

TYPICAL SECTIONS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

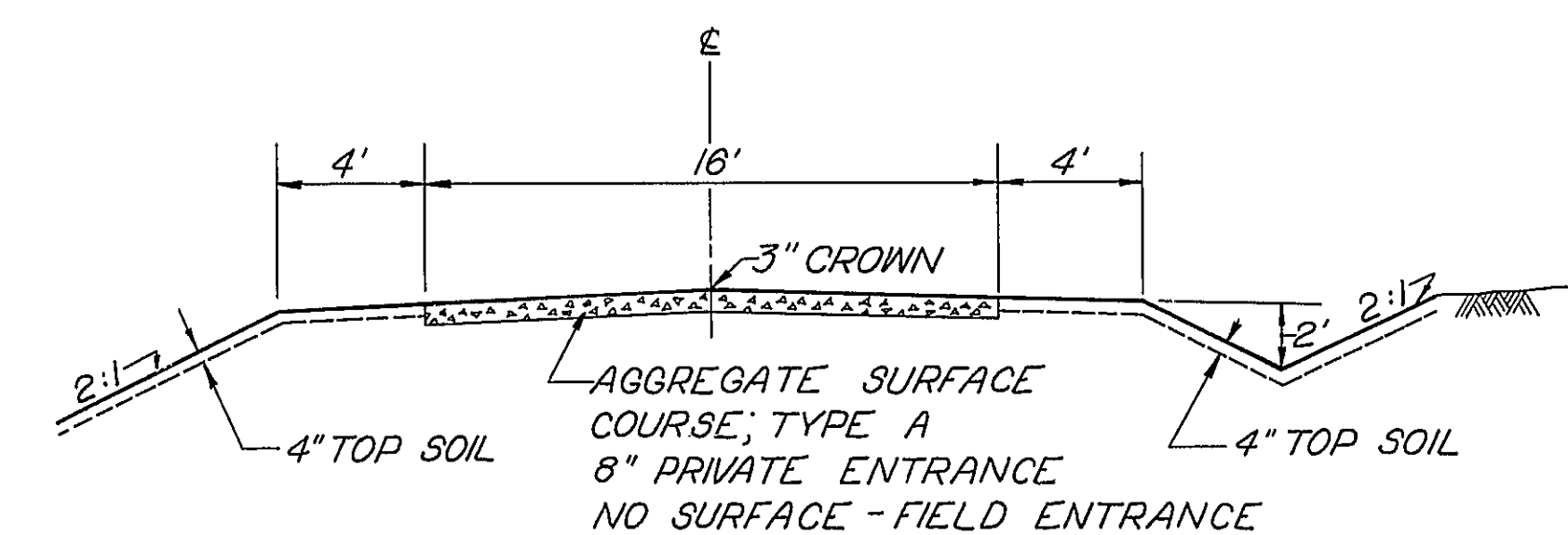


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RAI. 55	*	MONT.	206	6
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

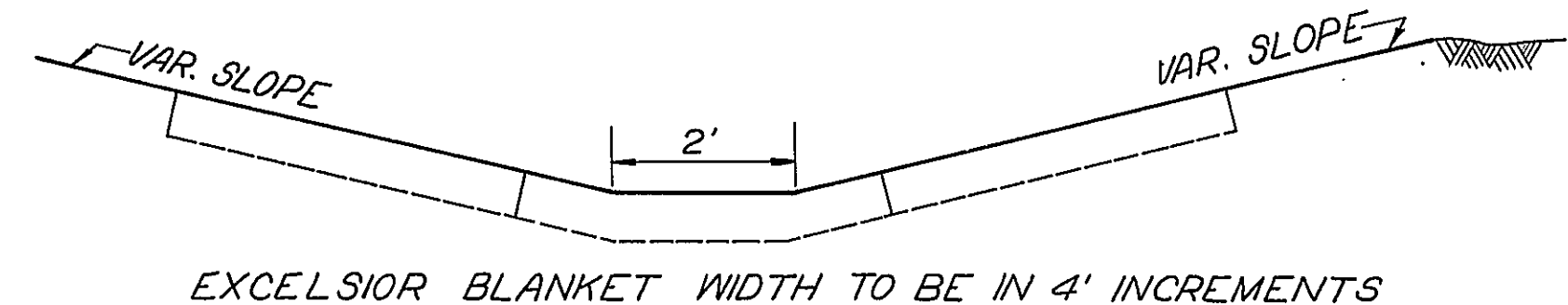


PRIVATE ENTRANCE & FIELD ENTRANCE

SCALE 1" = 5'



SECTION A-A

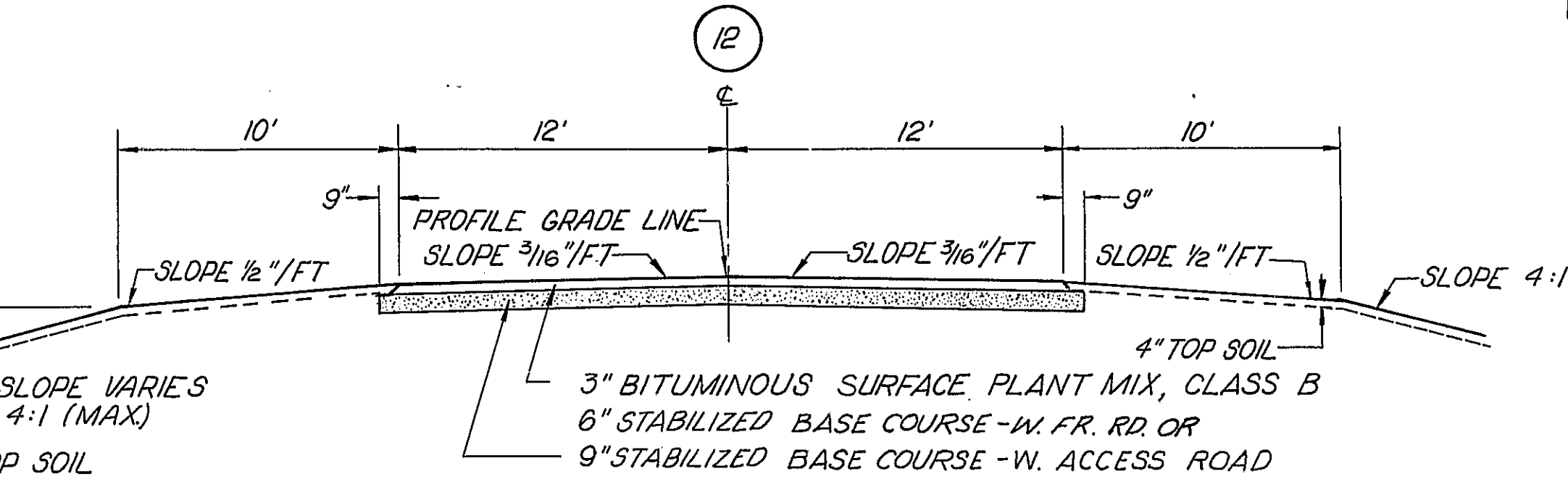


LINED DITCH

SCALE 1" = 2'

DESIGNED BY F.T.P.
 DRAWN BY H.M.L.
 CHECKED BY W.E.T.
 APPROVED BY _____

PAV'T ANALYSIS DATA FOR WEST FRONTAGE ROAD
 STRUCTURAL DESIGN TRAFFIC: YEAR 1979 P.C.=262 U.S.=104 M.U.=4
 CLASS II ROADS AND STREETS
 MINIMUM SOILS SUPPORT: CBR=2 (STA. 1021+76.82 TO STA. 1051+29.58)
 PER CENT OF S.D.T. IN DESIGN LANE Up=50, Us=50, Um=50
 T.F. 0.0103



WEST FRONTAGE ROAD - NORMAL

ADT (1979)
 W. FR. RD. - 370
 W. ACCESS ROAD - 740

STA. 1034+56.39 TO STA. 1036+74.27
 TRANSITION FROM SEC. ② AT STA. 1036+74.27 TO SEC. ③ AT STA. 1038+90.27
 STA. 1049+78.70 TO STA. 1051+06.77
 STA. 1042+22.78 TO STA. 1042+73.97 & W. ACCESS ROAD
 TRANSITION FROM SEC. ② AT STA. 1042+73.97 TO SEC. ⑤ AT STA. 1043+73.97 & W. ACCESS ROAD
 STA. 1045+27.33 TO STA. 1047+93.81 & W. ACCESS ROAD
 TRANSITION FROM SEC. ③ AT STA. 1047+93.81 TO SEC. ④ AT STA. 1048+93.81 & W. ACCESS ROAD
 STA. 1053+14.11 TO STA. 1053+75 & W. ACCESS ROAD (BY SEC. 68-4B)

GENERAL NOTES:
 (CONTINUED)

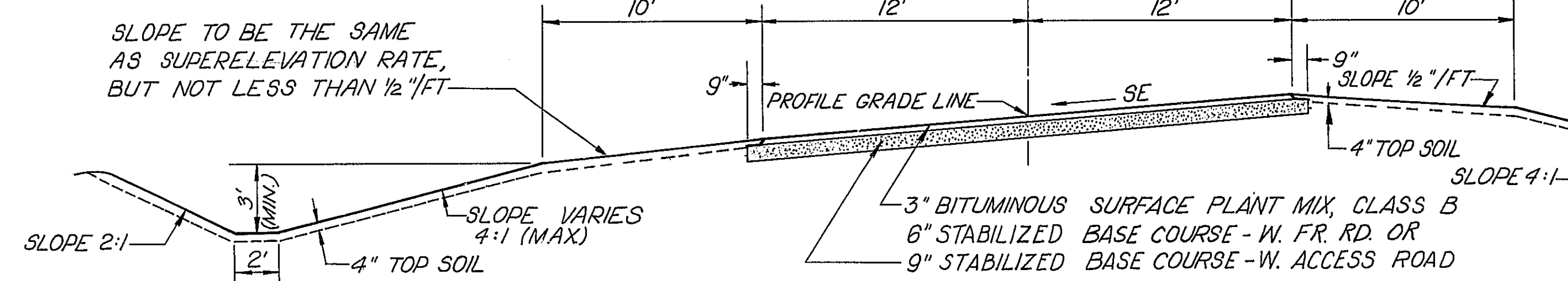
10. THE STANDARD REVISION NUMBER SHOWN IN THE INDEX OF SHEETS SHALL BE USED IN THE CONSTRUCTION OF THIS SECTION.

11. IN AREAS WHERE THE ENTIRE U.S. 66 N.B. PAVEMENT IS REMOVED, THE RIGHT SHOULDER FOR F.A.I. 55 S.B. PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE 11' STABILIZED SHOULDER DETAIL USED

TO MAINTAIN THE 11' SHOULDER FOR F.A.I. 55 S.B. PAVEMENT.

12. PAVED DITCHES SHALL BE TYPE B-12 CONSTRUCTED IN ACCORDANCE WITH STANDARD 2258.

4' OF EXCELSIOR BLANKET SHALL BE PLACED ON EACH SIDE OF PAVED DITCH
 13. TWO PROJECT SIGNS (STA. 2193) ARE TO BE LOCATED AS SHOWN ON SHEETS 14 & 23. SPECIAL SYMBOLS



WEST FRONTAGE ROAD - SE LEFT

STA. 1038+90.27 TO STA. 1047+62.70
 TRANSITION FROM SEC. ③ AT STA. 1047+62.70 TO SEC. ⑥ AT STA. 1049+78.90
 STA. 1043+73.97 TO STA. 1044+27.33 & W. ACCESS ROAD
 TRANSITION FROM SEC. ⑥ AT STA. 1044+27.33 TO SEC. ⑦ AT STA. 1045+27.33 & W. ACCESS ROAD

WEST FRONTAGE ROAD - SE RIGHT

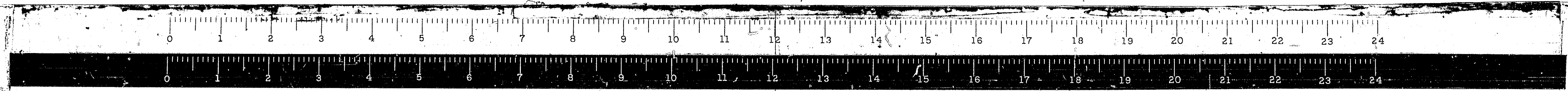
TRANSITION FROM U.S. 66 AT STA. 1021+76.82 TO SEC. ⑧ AT STA. 1023+81.82
 STA. 1023+81.82 TO STA. 1032+51.39
 TRANSITION FROM SEC. ⑧ AT STA. 1032+51.39 TO SEC. ⑨ AT STA. 1034+56.39
 STA. 1048+93.81 TO STA. 1051+75 & W. ACCESS ROAD
 STA. 1051+75 TO STA. 1052+14.11 & W. ACCESS ROAD (BY SEC. 68-4B)
 TRANSITION FROM SEC. ⑨ AT STA. 1052+14.11 TO SEC. ⑩ AT STA. 1053+14.11 & W. ACCESS ROAD (BY SEC. 68-4B)

- M.L. MATCH LINE
- T.E. TEMPORARY EASEMENT
- C.C. CHANNEL CHANGE
- T.G. TOP OF GRATE
- ▲ PERMANENT SURVEY MARKER
- W.W.F. WOVEN WIRE FENCE
- LINED DITCH
- PAVED DITCH
- R.C.C. PAVEMENT REMOVAL
- PROPERTY OWNED BY STATE OF ILLINOIS

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

TYPICAL SECTIONS

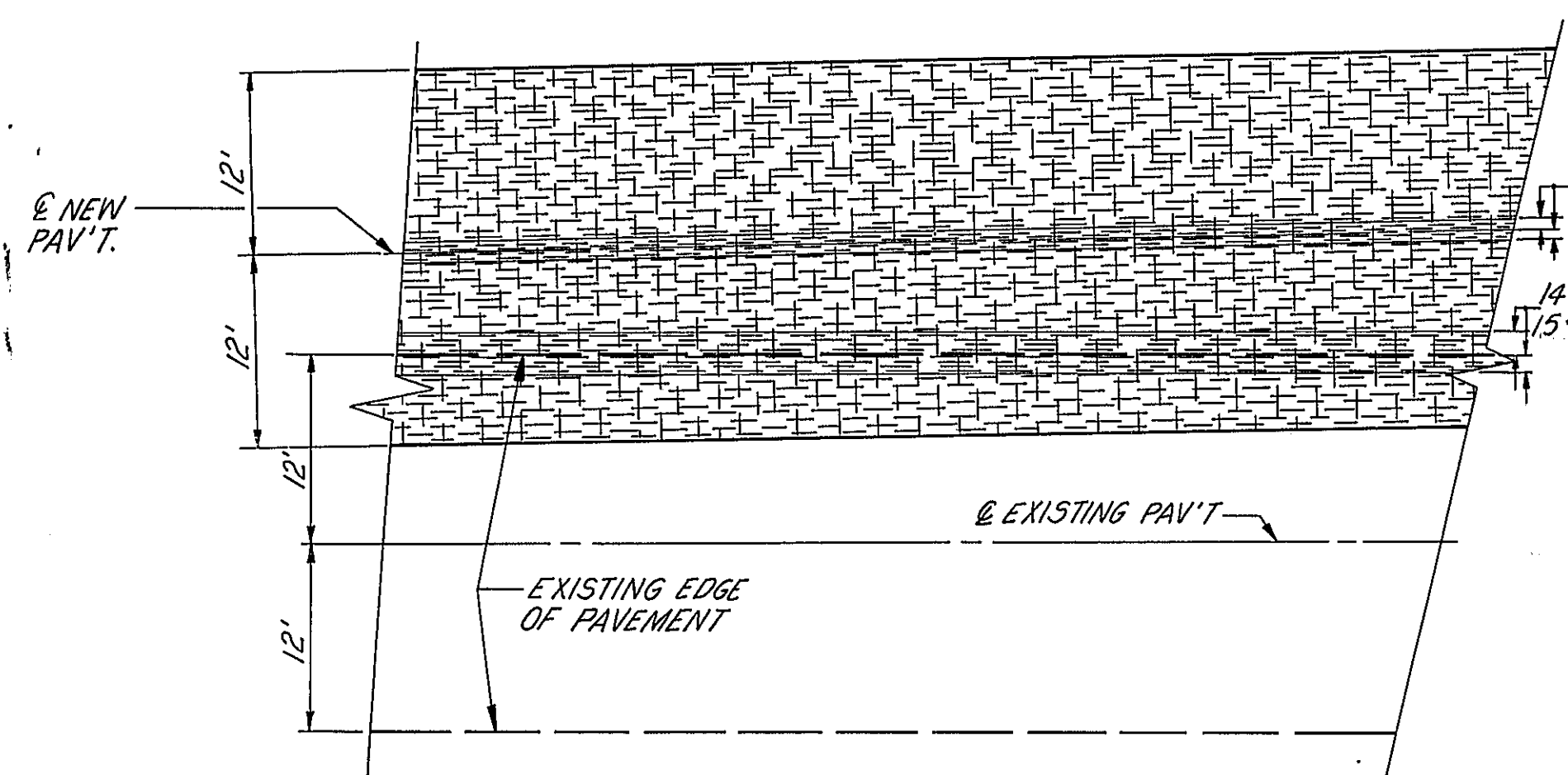
WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



* 68-5, 68-5HD, 68-5B

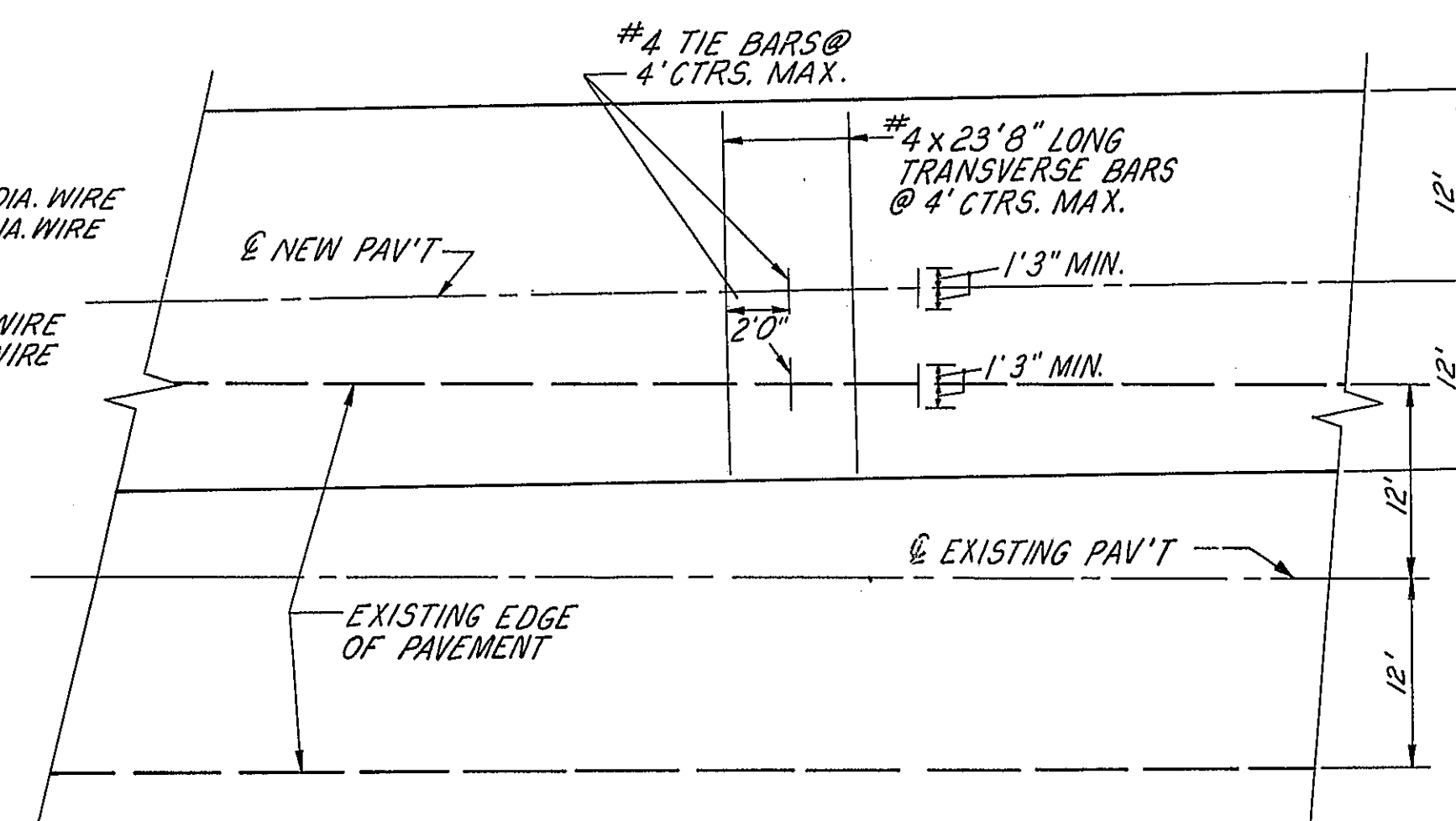
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MONT.	206	7
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	

NOTE: FOR ADDITIONAL DETAILS OF PAVEMENT REINFORCEMENT SEE STANDARD #222A.



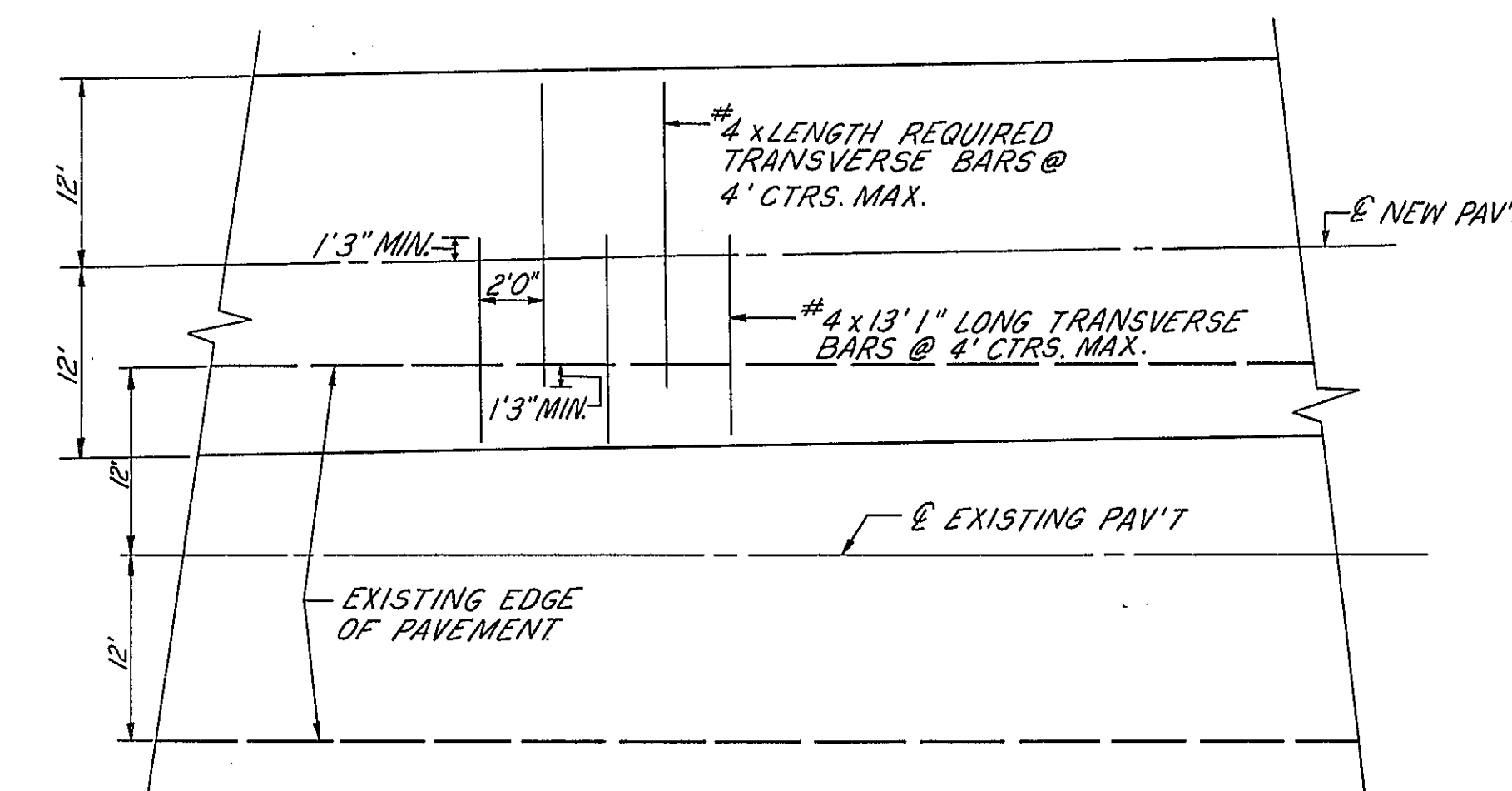
FABRIC REINFORCEMENT ASSEMBLY I

PLAN OF OPTIONAL LONGITUDINAL SAWED JOINT TIE STEEL SHOWING EXTRA REINFORCEMENT OVER EXISTING PAVEMENT EDGE.



BAR REINFORCEMENT ASSEMBLY II

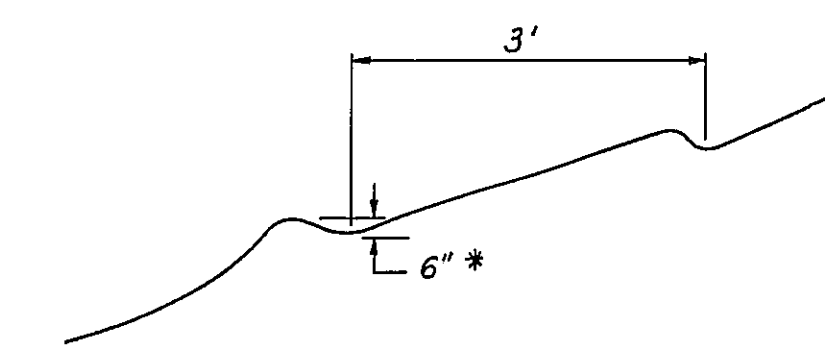
PLAN OF OPTIONAL LONGITUDINAL SAWED JOINT TIE STEEL SHOWING EXTRA REINFORCEMENT OVER EXISTING PAVEMENT EDGE.



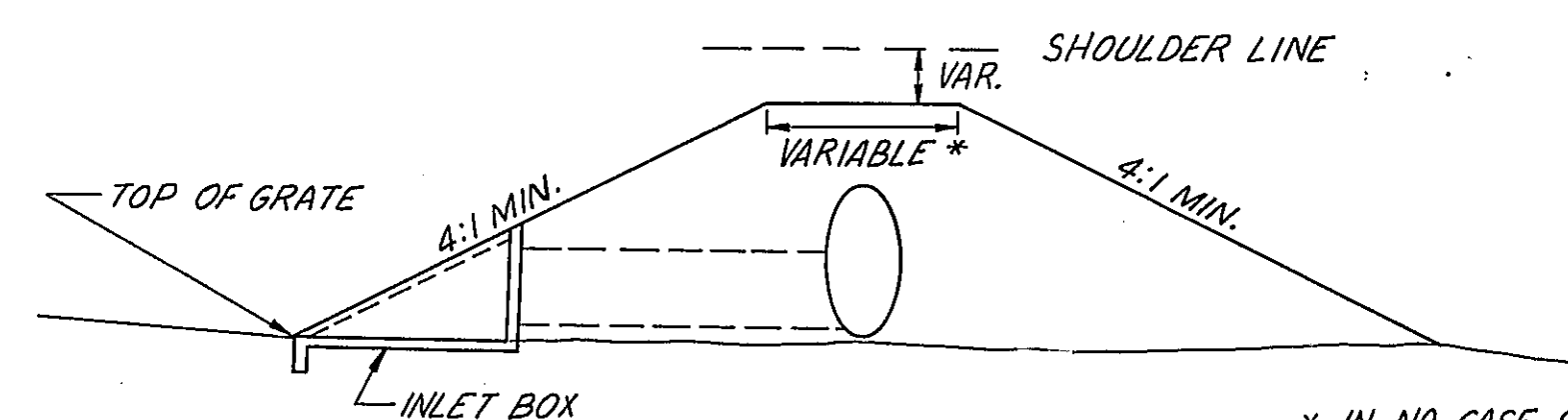
BAR REINFORCEMENT ASSEMBLY III

PLAN OF OPTIONAL LONGITUDINAL SAWED JOINT TIE STEEL SHOWING EXTRA REINFORCEMENT OVER EXISTING PAVEMENT EDGE.

EXTRA PAVEMENT REINFORCEMENT FOR TRANSITION AREAS
STA. 863+42.72 TO STA. 905+13.78
SCALE: NONE

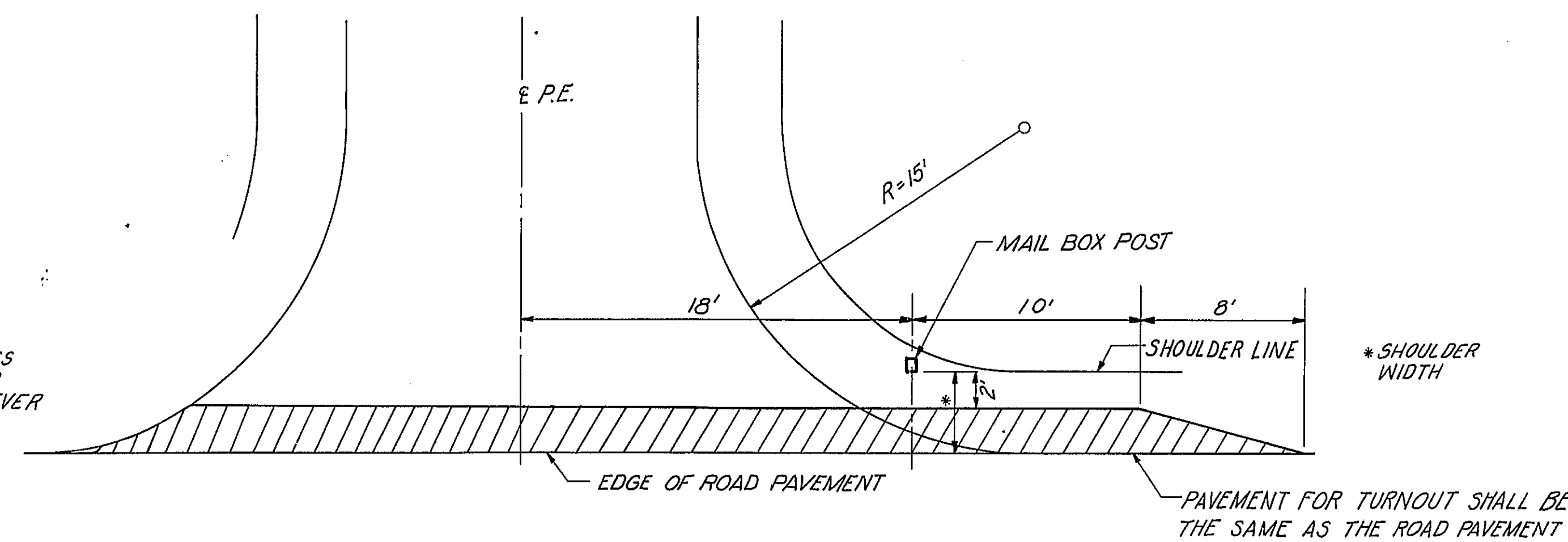


* NOTE: TRENCHES TO BE PARALLEL TO CONTOUR LINES FOR THE FILL



EARTH DITCH CHECK
SCALE: NONE

* IN NO CASE SHALL WIDTH BE LESS THAN PIPE CULV., BOX CULV., OR SPECIAL JUNCTION BOX, WHICHEVER IS OF GREATER WIDTH.



MAIL BOX TURNOUT AT
STA. 820+82 E. FR. RD. #1
STA. 970+68 E. FR. RD. #2
STA. 33+25 CH 17
STA. 1049+00 W. ACCESS RD.
SCALE: NONE

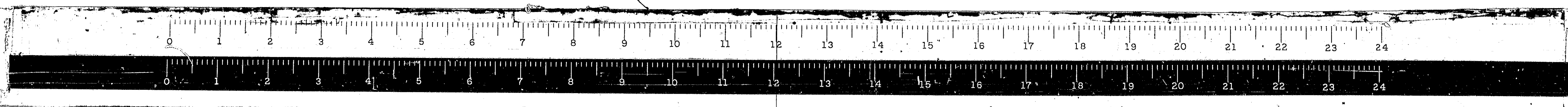
PLOWING METHOD FOR CLASS III SEEDING
SCALE: NONE

DESIGNED BY F.F.R.
DRAWN BY S.D.D.
CHECKED BY W.F.T.
APPROVED BY _____

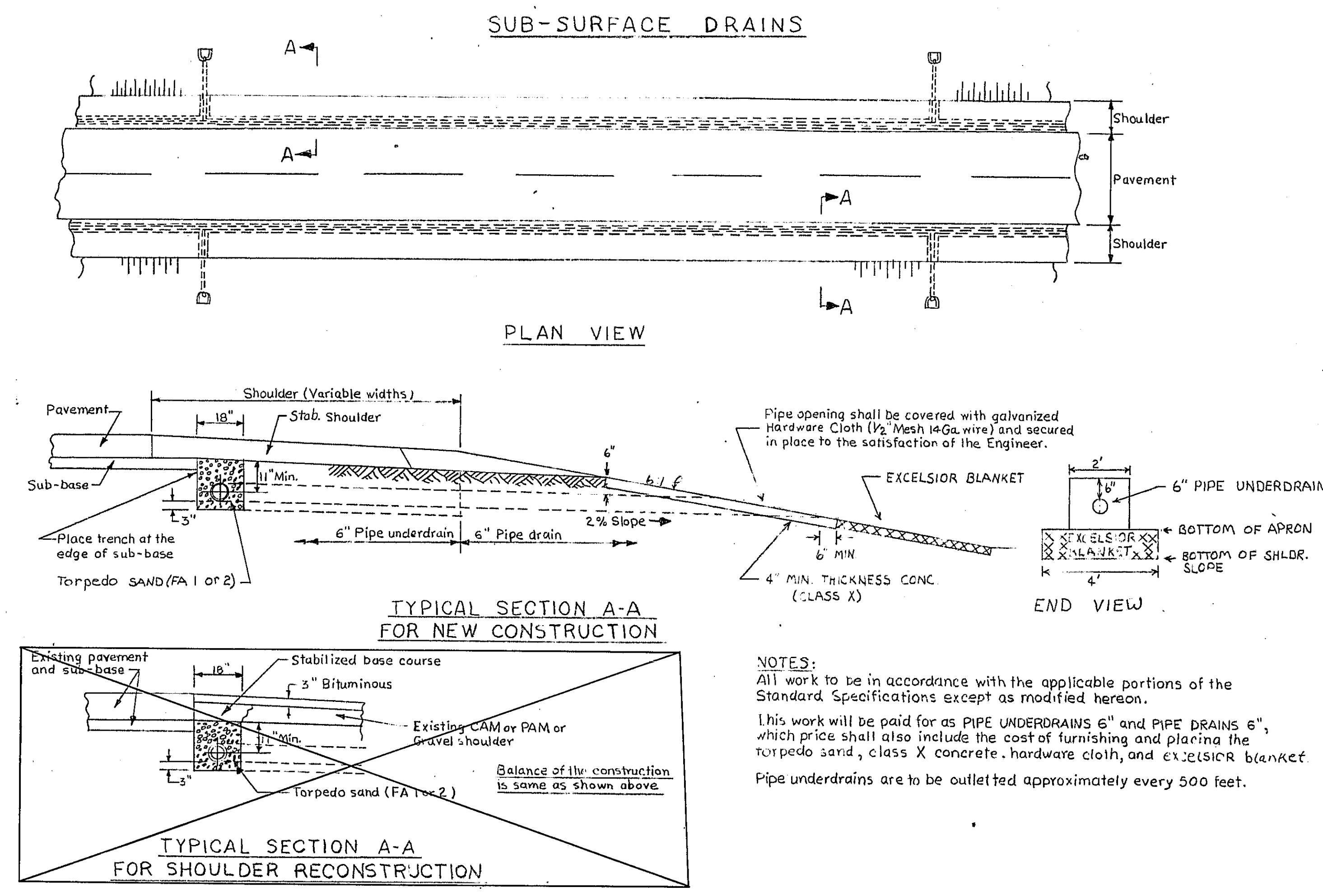
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

TYPICAL DETAILS

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



ROAD DIST. NO. 7	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	7A
STATE OF MONTGOMERY				
FEE: ROAD DIST. NO. 7 ILLINOIS FEE: AID PROJECT				



OUTLET STA.	RT.	LT.	PIPE DRAIN	PIPE UNDERDRAIN
RAMP A				
15+00	5.75	11.40	5.75	
22+00	3.75	3.75	22.73	9.50
27+00	1.75	1.75	22.97	9.50
32+00	1	3.75	24.45	9.50
RAMP B				
12+00	1	1	24.45	9.50
17+00	5.75	1	22.73	9.50
21+57	3.75	1	22.93	9.50
27+00	1.75	1	11.40	5.75
31+90	1	1	11.40	5.75
36+90	5.75	1	11.10	6.75
RAMP C				
13+50	5.75	1	11.52	5.75
19+50	3.75	3.75	22.93	9.50
24+12	3.75	1	22.73	9.50
29+00	1.75	1	24.45	9.50
RAMP D				
13+00	1	1	24.45	9.50
16+00	5.75	1	22.93	9.50
21+01	1.75	1	24.45	9.50
25+00	3.75	1	22.93	9.50
30+00	1.75	1	13.15	5.75
35+00	5.75	1	11.40	5.75
39+94	1	1	11.40	5.75
CH 17				
35+00	11.78	11.78	22.20	15.50
38+00	1	1	22.00	15.50
43+00	1	1	22.20	15.50
55+00	1	1	22.20	15.50
58+00	1	1	22.00	15.50
62+00	1	1	22.00	15.50
67+50	1	1	22.00	15.50
71+00	1	1	22.20	15.50
TOTAL	29	21	574.70	294.50
TOTAL OUTLETS	= 224			
TOTAL PIPE DRAIN	= 2700 LIN. FT.			

STA. TO STA.	RT.	LT.	PERFORATED PIPE UNDERDRAIN
RAMP A			
16+00 TO 16+62.40	662.40	—	662.40
16+62.40 TO 34+73.85	1811.45	1811.45	3622.90
34+73.85 TO 35+00.97	107.12	—	107.12
RAMP B			
10+25 TO 10+83	60	—	60
10+83 TO 25+51.71	1468.71	1468.71	2937.42
25+51.71 TO 37+22.80	1171.09	—	1,171.09
RAMP C			
10+00 TO 16+62.40	662.40	—	662.40
16+62.40 TO 32+68.62	1,606.22	1,606.22	3,212.44
32+68.62 TO 33+69.36	100.74	—	100.74
RAMP D			
10+23 TO 10+87.02	64.02	—	64.02
10+87.02 TO 29+32.33	1,845.31	1,845.31	3,690.62
29+32.33 TO 41+03.95	1,171.62	—	1,171.62
CH 17			
35+00 TO 38+85	385	385	770
39+70 TO 48+45	875	—	875
39+70 TO 44+30	—	460	460
46+20 TO 48+45	—	225	225
51+46 TO 55+40	—	394	394
57+06 TO 73+08	—	1,602	1,602
51+46 TO 73+08	2,162	—	2,162
TOTAL	14,530.8	9,797.69	23,950.77
CH 17 = 6,488.00 LIN. FT.			
RAMPS = 17,462.77 LIN. FT.			
MAINLINE = 86,338.69 LIN. FT.			
LATERALS = 11,32.25 LIN. FT.			
TOTAL PIPE UNDERDRAINS = 111,527 LIN. FT.			

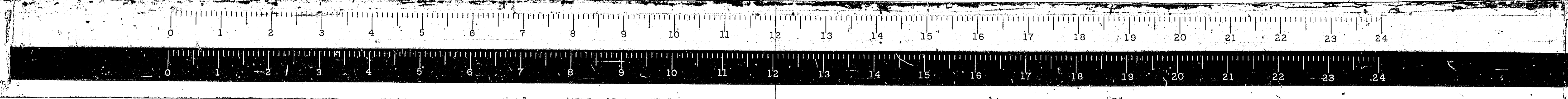
* 68-5, 68-5HB, 68-5B

NOTES:
 All work to be in accordance with the applicable portions of the Standard Specifications except as modified hereon.
 This work will be paid for as PIPE UNDERDRAINS 6" and PIPE DRAINS 6", which price shall also include the cost of furnishing and placing the torpedo sand, class X concrete, hardware cloth, and excelsior blanket.
 Pipe underdrains are to be outletted approximately every 500 feet.

OUTLET STA. MAINLINE	N.B. 55 OUT	S.B. 55 MED. OUT	PIPE DRAIN	UNDERDRAIN	OUTLET STA. MAINLINE	N.B. 55 OUT	S.B. 55 MED. OUT	PIPE DRAIN	UNDERDRAIN			
764+00	7.75	15.75	15.75	36.04	19.25	923+00	1	1	36.04	19.25		
769+00	1	1	1	36.04	19.25	924+72	1	1	12.07	7.75		
773+68	1	1	1	36.04	19.25	928+00	1	1	36.04	19.25		
779+00	1	1	1	36.04	19.25	928+75	1	1	12.07	7.75		
784+00	1	1	1	36.04	19.25	932+00	1	1	36.04	19.25		
789+00	1	1	1	36.04	19.25	937+00	1	1	36.04	19.25		
794+00	1	1	1	36.04	19.25	942+00	1	1	36.04	19.25		
799+00	1	1	1	36.04	19.25	947+77	1	1	36.04	19.25		
804+00	1	1	1	36.04	19.25	953+00	1	1	36.04	19.25		
809+00	1	1	1	36.04	19.25	958+00	1	1	36.04	19.25		
814+00	1	1	1	36.04	19.25	969+00	1	1	7.75	27.00		
818+66	1	1	1	36.04	19.25	974+00	1	1	36.04	19.25		
819+65	1	1	1	12.07	7.75	978+00	1	1	36.04	19.25		
824+00	1	1	1	36.04	19.25	983+00	1	1	36.04	19.25		
834+00	1	1	1	48.11	27.00	988+00	1	1	36.04	19.25		
839+00	1	1	1	48.11	27.00	992+80	1	1	36.04	19.25		
844+00	1	1	1	36.04	19.25	998+00	1	1	36.04	19.25		
849+00	1	1	1	36.04	19.25	1003+00	1	1	36.04	19.25		
853+50	1	1	1	36.04	19.25	1008+00	1	1	36.04	19.25		
859+00	1	1	1	36.04	19.25	1013+00	1	1	36.04	19.25		
864+00	1	1	1	36.04	19.25	1018+00	1	1	36.04	19.25		
869+00	1	1	1	36.04	19.25	1023+00	1	1	36.04	19.25		
874+00	1	1	1	36.04	19.25	1028+00	1	1	23.04	11.50		
880+08	1	1	1	36.04	19.25	1033+00	1	1	17.5	48.11		
885+00	1	1	1	36.04	19.25	1038+00	1	1	12.07	48.11		
890+00	1	1	1	36.04	19.25	1043+00	1	1	23.04	11.50		
900+00	1	1	1	36.04	19.25	1048+00	1	1	36.04	19.25		
905+00	1	1	1	36.04	19.25	1052+00	1	1	36.04	19.25		
910+00	1	1	1	36.04	19.25	MAINLINE TOTAL	54	56	56	8	208,880	124,50
913+41	1	1	1	36.04	19.25	U= PIPE UNDERDRAIN						
918+00	1	1	1	36.04	19.25	D= PIPE DRAIN						

STA. TO STA.	OUT	N.B.	MED.	MED.	S.B.	OUT
760+00 TO 827+50	6,750	6,750	6,750	—	—	—
818+85 TO 819+65	—	—	—	—	—	80
834+00 TO 839+70	570	570	570	570	570	—
839+70 TO 924+72	8,502	8,502	8,502	—	—	—
924+72 TO 928+75	403	403	403	403	403	—
928+75 TO 966+50	3,775	3,775	3,775	—	—	—
969+00 TO 973+05	405	405	405	405	405	—
973+05 TO 1016+90.43	4,385.43	4,385.43	4,385.43	—	—	—
1016+90.43 TO 1028+65.12	—	1,174.69	1,174.69	—	—	—
1028+65.12 TO 1029+76.56	1,114.44	1,114.44	1,114.44	—	—	—
1029+76.56 TO 1040+01.79	1,025.23	1,025.23	1,025.23	1,025.23	1,025.23	—
1040+01.79 TO 1040+59.21	—	57.42	57.42	57.42	57.42	—
1040+59.21 TO 1046+64.53	—	605.32	605.32	—	—	—
1046+64.53 TO 1052+50	585.47	585.47	585.47	16.10	16.10	—
MAINLINE TOTAL	26,512.57	28,350.00	28,350.00	2,556.75	—	—

DETAIL OF SUBSURFACE DRAINS
 ROUTE F.A.I. 55
 SEC. 68-5, 68-5HB, 68-5B
 MONTGOMERY COUNTY



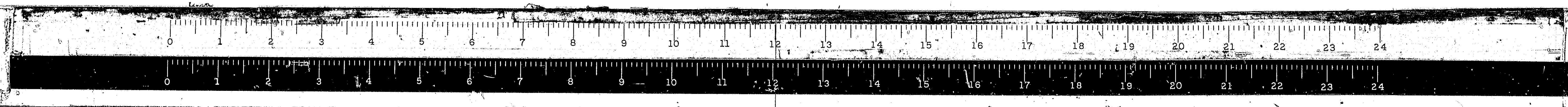
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL.SS	*	MONT.	206	8A
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	7223	CONSTRUCTION TYPE CODE	X028	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	7223	CONSTRUCTION TYPE CODE	X028
201001	TREE REMOVAL (6 TO 15 INCH DIAMETER)	IN DIA	320	320			511051	PIPE CULVERTS, TYPE 1 RCCP 36"	LIN FT	96	96		
201002	TREE REMOVAL (OVER 15 INCH DIAMETER)	IN DIA	2,341	2,341			511055	PIPE CULVERTS, TYPE 1 RCCP 60"	LIN FT	52	52		
201005	TREE REMOVAL, ACRES	ACRE	7.7	7.7			511056	PIPE CULVERTS, TYPE 1 RCCP 66"	LIN FT	436	436		
201006	HEDGE REMOVAL	UNIT	6	6			511085	PIPE CULVERTS, TYPE 2A, 18"	LIN FT	60	60		
202001	EARTH EXCAVATION	CU YD	295,321	295,321			511086	PIPE CULVERTS, TYPE 2A, 21"	LIN FT	70	70		
203001	CHANNEL EXCAVATION	CU YD	2,077		2,077		511088	PIPE CULVERTS, TYPE 2A, 30"	LIN FT	66	66		
204001	BORROW EXCAVATION	CU YD	987,592	987,592			511108	PIPE CULVERTS, TYPE 2 24"	LIN FT	14	14		
209002	POROUS GRANULAR EMBANKMENT	CU YD	572	312		260	511129	PIPE CULVERTS, TYPE 2 RCCP 24"	LIN FT	402	402		
210001	TRENCH BACKFILL	CU YD	30	30			511130	PIPE CULVERTS, TYPE 2 RCCP 30"	LIN FT	310	310		
211001	POROUS GRANULAR BACKFILL	CU YD	1,876	1,876			511131	PIPE CULVERTS, TYPE 2 RCCP 36"	LIN FT	108	108		
213001	SUB-BASE GRANULAR MATERIAL, TYPE A	TON	3,257	3,257			511291	PIPE CULVERTS, TYPE 4 RCCP 36"	LIN FT	204	204		
213012	STABILIZED SUB-BASE 4"	SQ YD	192,874	192,874			511375	PIPE CULVERTS, TYPE 5 RCCP 60"	LIN FT	280	280		
X21301	STABILIZED SUB-BASE (VARIABLE DEPTH)	SQ YD	15,645	15,645			511586	PIPE CULVERTS, TYPE 1A, ARCH-DIAMETER EQUIVALENT 54"	LIN FT	88	88		
215004	AGGREGATE SHOULDERS, TYPE B	TON	107	107			511808	METAL END SECTIONS, ARCH-DIAMETER EQUIVALENT 54"	EACH	2	2		
X21504	STABILIZED SHOULDERS (VARIABLE DEPTH)	SQ YD	35,145	35,145			511830	CULVERT END SECTIONS 18"	EACH	4	4		
X21601	TOP SOIL EXCAVATION	CU YD	93,704	93,704			511831	CULVERT END SECTIONS 21"	EACH	4	4		
X21602	TOP SOIL PLACEMENT	SQ YD	771,334	771,334			511832	CULVERT END SECTIONS 24"	EACH	7	7		
301001	AGGREGATE BASE COURSE, TYPE A	TON	25,365	25,365			511834	CULVERT END SECTIONS 30"	EACH	3	3		
301004	AGGREGATE BASE COURSE, TYPE B	TON	2,669	2,669			511835	CULVERT END SECTIONS 36"	EACH	8	8		
301015	STABILIZED BASE COURSE 6"	SQ YD	8,610	8,610			511837	CULVERT END SECTIONS 48"	EACH	2	2		
301017	STABILIZED BASE COURSE 8"	SQ YD	4,654	4,654			511906	PRECAST REINFORCED CONCRETE FLARED END SECTION 18"	EACH	1	1		
301018	STABILIZED BASE COURSE 9"	SQ YD	2,840	2,840			511908	PRECAST REINFORCED CONCRETE FLARED END SECTION 24"	EACH	7	7		
402001	AGGREGATE SURFACE COURSE, TYPE A	TON	333	333			511910	PRECAST REINFORCED CONCRETE FLARED END SECTION 30"	EACH	1	1		
403001	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	22,726	22,726			511925	REINFORCED CONCRETE PIPE TEE, 24" PIPE WITH 24" RISER	EACH	6	6		
403003	BITUMINOUS MATERIALS (COVER AND SEAL COATS)	GALLON	54,465	54,465			511926	REINFORCED CONCRETE PIPE TEE, 30" PIPE WITH 24" RISER	EACH	4	4		
403005	COVER COAT AGGREGATE	TON	1,208	1,208			511A02	PIPE CULVERTS, TYPE 1, 15" (TEMPORARY)	LIN FT	26	26		
403006	SEAL COAT AGGREGATE	TON	604	604			511A03	PIPE CULVERTS, TYPE 1, 18" (TEMPORARY)	LIN FT	620	620		
405003	BITUMINOUS MIXTURE COMPLETE	TON	1,960	1,960			511A05	PIPE CULVERTS, TYPE 1, 24" (TEMPORARY)	LIN FT	1,178	1,178		
406007	BITUMINOUS CONCRETE BINDER COURSE	TON	480	480			511A07	PIPE CULVERTS, TYPE 1, 36" (TEMPORARY)	LIN FT	90	90		
406008	BITUMINOUS CONCRETE SURFACE COURSE, CLASS I	TON	342	342			512001	REINFORCEMENT BARS	POUND	642,261	113,621	208,220	320,420
408003	PORTLAND CEMENT CONCRETE PAVEMENT 8"	SQ YD	26,916	26,916			513005	FURNISHING CROSSED PILES 20.1 TO 38 FEET	LIN FT	504	504		
408006	PORTLAND CEMENT CONCRETE PAVEMENT 16"-10 1/2"-16 1/2"	SQ YD	260	260			513013	FURNISHING STEEL PILES 10BPM2	LIN FT	2,240	2,240		
408013	PAVEMENT FABRIC	SQ YD	26,916	26,916			513022	DRIVING TIMBER PILES	LIN FT	504	504		
408014	PROTECTIVE COAT	SQ YD	187,822	187,822			513026	DRIVING STEEL PILES	LIN FT	2,240	2,240		
409004	CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 9"	SQ YD	160,906	160,906			513033	TEST PILE STEEL 10BPM2	EACH	3	3		
409014	PAVEMENT REINFORCEMENT 9"	SQ YD	160,906	160,906			514001	NAME PLATES	EACH	2	1		
501003	REMOVAL OF EXISTING STRUCTURES NO. 1	EACH	1	1			603003	STORM SEWERS, TYPE 1 8"	LIN FT	488	488		
501004	REMOVAL OF EXISTING STRUCTURES NO. 2	EACH	1	1			603026	STORM SEWERS, TYPE 2 4"	LIN FT	175	175		
501026	EXPANSION BOLTS 3/4 INCH	EACH	182	182			603027	STORM SEWERS, TYPE 2 6"	LIN FT	760	760		
502001	CLASS A EXCAVATION FOR STRUCTURES	CU YD	261	261	261		603028	STORM SEWERS, TYPE 2 8"	LIN FT	325	325		
503004	PROTECTIVE COAT	SQ YD	1,757	1,757	1,757		603031	STORM SEWERS, TYPE 2 15"	LIN FT	670	670		
504003	CLASS X CONCRETE	CU YD	3,442.6	636.9	1,217.4	1,588.3	607002	PIPE GRANS 6"	LIN FT	2847	2847		
505009	FURNISHING AND ERECTING PRECAST PRESTRESSED CONCRETE I-BEAMS, 42 IN.	LIN FT	651	651	651		607182	PIPE UNDERDRAIN 6"	LIN FT	11,750	11,750		
507025	STUD SHEAR CONNECTORS	EACH	3,528	3,528	3,528		607197	PIPE UNDERDRAINS PERFORATED CORRUGATED STEEL PIPE 6"	LIN FT	1944	1944		
507030	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1	1	1		607273	PIPE DRAINS CORRUGATED STEEL OR ALUMINUM ALLOY 8" INLETS, TYPE A TYPE 1 FRAME, OPEN LID	LIN FT	96	96		
508005	ALUMINUM RAILING	LIN FT	591	591	591		612168	INLETS, TYPE A, TYPE 15 FRAME AND LID	EACH	6	6		
511005	PIPE CULVERTS, TYPE 1A, 18"	LIN FT	52	52			612436	MEDIAN INLET (2250)	EACH	7	7		
511006	PIPE CULVERTS, TYPE 1A, 21"	LIN FT	44	44			612437	MEDIAN INLET (2251)	EACH	3	3		
511007	PIPE CULVERTS, TYPE 1A, 24"	LIN FT	220	220			614057	GRATING	SQ FT	1,265	1,265		
511008	PIPE CULVERTS, TYPE 1A, 30"	LIN FT	52	52			616065	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	LIN FT	5,514	5,514		
511009	PIPE CULVERTS, TYPE 1A, 36"	LIN FT	164	164			616071	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12	LIN FT	126	126		
511013	PIPE CULVERTS, TYPE 1A, 60"	LIN FT	62	62			616083	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24	LIN FT	1,314	1,314		
511025	PIPE CULVERTS, TYPE 1 15"	LIN FT	686	686			616121	PAVED DITCH 10 FEET	LIN FT	80	80		
511028	PIPE CULVERTS, TYPE 1 24"	LIN FT	506	506			616137	CONCRETE MEDIAN, TYPE C-4	SQ FT	1,170	1,170		
511029	PIPE CULVERTS, TYPE 1 30"	LIN FT	148	148			616202	CONCRETE MEDIAN SURFACE 4 INCHES	SQ FT	31,856	31,856		
511031	PIPE CULVERTS, TYPE 1 42"	LIN FT	42	42			617001	PAVEMENT REMOVAL	SQ YD	50,902	50,902		
511032	PIPE CULVERTS, TYPE 1 48"	LIN FT	56	56			617005	COMBINATION CURB AND GUTTER REMOVAL	LIN FT	1,102	1,102		
511047	PIPE CULVERTS, TYPE 1 RCCP 18"	LIN FT	52	52			617006	SIDEWALK REMOVAL	SQ FT	1,566	1,566		
511049	PIPE CULVERTS, TYPE 1 RCCP 24"	LIN FT	680	680			618001	SLOPE WALL 4 INCH	SQ YD	140	140		
511050	PIPE CULVERTS, TYPE 1 RCCP 30"	LIN FT	16	16									

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SUMMARY OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



CODE NUMBER	ITEM	UNIT	CONSTRUCTION TYPE CODE		CODE NUMBER	ITEM	UNIT	CONSTRUCTION TYPE CODE		ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			7223	X771				X028	7223	X771	X028	FAI. 53	*	MONT.
624002	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SQ FT	1,020	1,020	Z10535	DETOUR ROADWAY REMOVAL	SQ YD	11,926	11,926	FED. ROAD DIST. NO. 4	ILLINOIS	PROJ.		
628001	STEEL PLATE BEAM GUARD RAIL, SINGLE RAIL	LIN FT	587.5	587.5	Z10621	SOIL STERILANT	LIN FT	61,669	61,669					
X62801	TERMINAL SECTION, SINGLE RAIL	EACH	6	6	X04105	TRAFFIC CONTROL AND PROTECTION, TEMPORARY CONNECTION #1	L SUM	1	1					
630001	WOVEN WIRE FENCE, 4'	LIN FT	61,669	61,669	X04106	TRAFFIC CONTROL AND PROTECTION, TEMPORARY CONNECTION #2	L SUM	1	1					
632001	DELINEATORS	EACH	461	461	X04107	TRAFFIC CONTROL AND PROTECTION, TEMPORARY CONNECTION #3	L SUM	1	1					
633001	METAL PLATE GUARD RAIL REMOVAL	LIN FT	176	176	X04108	TRAFFIC CONTROL AND PROTECTION, TEMPORARY CONNECTION #4	L SUM	1	1					
633003	STEEL PLATE BEAM GUARD RAIL REMOVAL	LIN FT	325	325	XZ1016	TRAFFIC CONTROL AND PROTECTION, STANDARD 2310	EACH	1	1					
639001	FURNISHING AND ERECTING RIGHT OF WAY MARKERS	EACH	139	139	XZ1087	TRAFFIC CONTROL AND PROTECTION, STANDARD 2312	L SUM	1	1					
* 642002	SEEDING, CLASS II	ACRE	182.7	182.7	XZ1088	TRAFFIC CONTROL AND PROTECTION, STANDARD 2315	L SUM	1	1					
* 642003	SEEDING, CLASS III	ACRE	1.0	1.0	XZ1089	TRAFFIC CONTROL AND PROTECTION, STANDARD 2316	L SUM	1	1					
* 642004	NITROGEN FERTILIZER NUTRIENT	POUND	18,370	18,370	XZ1090	P.C. CONCRETE BRIDGE APPROACH SHOULDER PAVEMENT	SQ YD	22	22					
* 642005	PHOSPHOROUS FERTILIZER NUTRIENT	POUND	9,235	9,235	X04114	FILLING GAS STORAGE TANKS	EACH	4	4					
* 642006	POTASSIUM FERTILIZER NUTRIENT	POUND	9,235	9,235	** L02311	POLE DOWN GUY	EACH	8	8					
* 642007	AGRICULTURAL GROUND LIMESTONE	TON	367.4	367.4	** L02403	LUMINAIRE, MERCURY VAPOR, WITH BUILT-IN REGULATOR BALLAST, 400 WATTS	EACH	18	18					
* 643001	MULCH	TON	367.4	367.4	** L02812	LAMP, MERCURY VAPOR, 400 WATTS, TYPE H33-1-CD	EACH	18	18					
* 643004	EXCELSIOR BLANKET	SQ YD	11,276	11,276	** L04410	SERVICE INSTALLATION, TYPE CB RCS-60	EACH	2	2					
646001	ENGINEER'S FIELD OFFICE, TYPE A	EACH	1	1	** L05224	POLE, WOOD, WIRED ASSEMBLY, 40 FT. MH, 15 FT. MAST ARM	EACH	18	18					
646003	ENGINEER'S FIELD LABORATORY	EACH	1	1	** L05228	ELECTRIC CABLE AERIAL, 300 VOLT 2/C #4AL WITH ASCR MESSENGER WIRE	LIN FT	3,108	3,108					
*** XZ1100	TRAINEES	HOUR	5,000	5,000	** L05469	REMOVAL OF LIGHTING SYSTEM COMPLETE (LOCATION A)	L SUM	1	1					
Z10039	BUILDING REMOVAL NO. 3	L SUM			** L05470	REMOVAL OF LIGHTING SYSTEM COMPLETE (LOCATION B)	L SUM	1	1					
Z10040	BUILDING REMOVAL NO. 4	L SUM			** L05471	REMOVAL OF LIGHTING SYSTEM COMPLETE (LOCATION C)	L SUM	1	1					
Z10041	BUILDING REMOVAL NO. 5	L SUM			** L05472	REMOVAL OF LIGHTING SYSTEM COMPLETE (LOCATION D)	L SUM	1	1					
Z10042	BUILDING REMOVAL NO. 6	L SUM			** L05483	REMOVE EXISTING FLASHING LIGHT	L SUM	1	1					
Z10043	BUILDING REMOVAL NO. 7	L SUM												
Z10044	BUILDING REMOVAL NO. 8	L SUM												
Z10045	BUILDING REMOVAL NO. 9	L SUM												
Z10046	BUILDING REMOVAL NO. 10	L SUM												
Z10047	BUILDING REMOVAL NO. 11	L SUM												
Z10048	BUILDING REMOVAL NO. 12	L SUM												
Z10049	BUILDING REMOVAL NO. 13	L SUM												
Z10050	BUILDING REMOVAL NO. 14	L SUM												
Z10051	BUILDING REMOVAL NO. 15	L SUM												
Z10052	BUILDING REMOVAL NO. 16	L SUM												
Z10053	BUILDING REMOVAL NO. 17	L SUM												
Z10054	BUILDING REMOVAL NO. 18	L SUM												
Z10055	BUILDING REMOVAL NO. 19	L SUM												
Z10056	BUILDING REMOVAL NO. 20	L SUM												
Z10057	BUILDING REMOVAL NO. 21	L SUM												
Z10058	BUILDING REMOVAL NO. 22	L SUM												
Z10059	BUILDING REMOVAL NO. 23	L SUM												
Z10060	BUILDING REMOVAL NO. 24	L SUM												
Z10061	BUILDING REMOVAL NO. 25	L SUM												
Z10062	BUILDING REMOVAL NO. 26	L SUM												
Z10063	BUILDING REMOVAL NO. 27	L SUM												
Z10064	BUILDING REMOVAL NO. 28	L SUM												
Z10065	BUILDING REMOVAL NO. 29	L SUM												
Z10066	BUILDING REMOVAL NO. 30	L SUM												
Z10067	BUILDING REMOVAL NO. 31	L SUM												
Z10068	BUILDING REMOVAL NO. 32	L SUM												
Z10104	BUILDING REMOVAL NO. 68	L SUM												
Z10105	BUILDING REMOVAL NO. 69	L SUM												
Z10106	BUILDING REMOVAL NO. 70	L SUM												
Z10227	EXPLORATION TRENCH (52 IN DEPTH)	LIN FT	100	100										
Z10229	FILLING EXISTING CISTERNS	EACH	2	2										
XZ1083	FILLING EXISTING WELLS-DUG	EACH	13	13										
Z10274	PERMANENT BARRICADES	LIN FT	388	388										
Z10281	PERMANENT SURVEY MARKERS, TYPE I	EACH	5	5										
Z10282	PERMANENT SURVEY MARKERS, TYPE II	EACH	48	48										
Z10294	PREFORMED JOINT SEALER	LIN FT	140	140										

ALTERNATE A

XZ1501	STABILIZED SHOULDERS, BITUMINOUS AGGREGATE MIXTURE 8"	SQ. YD.	10,896	10,896
XZ1502	STABILIZED SHOULDERS BITUMINOUS AGGREGATE MIXTURE 9"	SQ. YD.	71,100	71,100
Z15001	AGGREGATE SHOULDERS, TYPE A	TON	19,764	19,764

ALTERNATE B

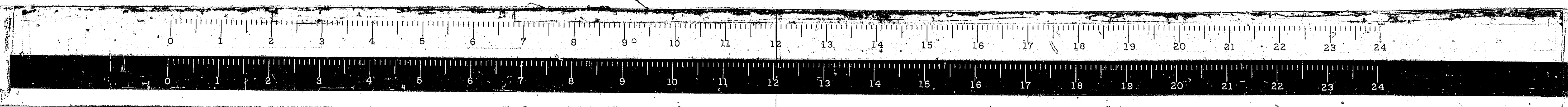
Z15002	STABILIZED SHOULDERS, PORTLAND CEMENT CONCRETE 8"	SQ. YD.	10,896	10,896
Z15002	STABILIZED SHOULDERS, PORTLAND CEMENT CONCRETE 9"	SQ. YD.	71,100	71,100
Z15002	AGGREGATE SHOULDERS TYPE A	TON	13,461	13,461

* CONST. CODE TYPE Y005
 ** CONST. CODE TYPE Y030
 *** CONST. CODE TYPE Y080

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

SUMMARY OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT	206	9
FED. ROAD DIST. NO. 4			ILLINOIS PROJ.	

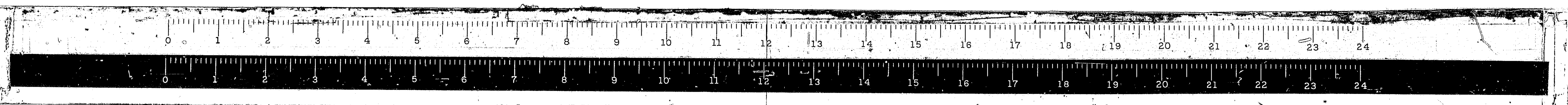
9" C.R.P.C.C. PAVEMENT					P.C.C. PAVEMENT 8"					CLASS X CONCRETE										
ROADWAY	STATION TO STATION	LENGTH FT.	WIDTH FT.	PAVEMENT AREA SQ. YD.	ROADWAY	STATION TO STATION	LENGTH FT.	WIDTH FT.	AREA 8" SQ. YD.	ROADWAY	STATION	LOCATION	STRUCTURE	CU. YD.	ROADWAY	STATION	LOCATION	STRUCTURE	CU. YD.	
FAI-55 N.B. & S.B.	760+00.00 1052+50.00	29,250.00	2 @ 24	156,000	RAMP A	16+62.40 23+42.81	687.57*	16	1,223	FAI 55	761+00	MED	INLET BOX	1.3	FAI 55	880+35	NB LT	INLET BOX	1.6	
RAMP A	10+00.00 15+29.93	529.93	TAPER	882		23+42.81 25+74.81	232.00	16	412		772+93	NB LT	INLET BOX	1.2		912+36	AR E. FR. RD.	BOX CULV	36.2	
	15+29.93 15+89.93	60.00	19 to 17	120		25+74.81 30+57.43	471.46*	16	838		773+31	NB LT	INLET BOX	1.2		912+75	MED	INLET BOX	1.2	
	15+89.93 16+62.40	72.47	16	129		30+57.43 35+80.97	523.54	16 & VAR	1,161		773+48.31	NB AR	BOX CULV	150.0		912+96.98	AR NB	BOX CULV	98.5	
150' TAPER	150.00	6 to 1	58		RAMP B	10+23.90 15+46.54	523.54	16 & VAR	1,169		799+00	MED	INLET BOX	1.3		913+15	MED	INLET BOX	1.2	
RAMP B	26+72.84 27+72.84	100.00	14	156		15+46.54 22+72.84	749.54*	16	1,333		799+00	NB LT	INLET BOX	1.3		913+28.7	SB RT	INLET BOX	1.2	
	27+72.84 29+72.86	200.02	20 to 16	400		22+72.84 24+72.84	203.20*	16	361		810+00	MED	INLET BOX	1.3		913+69.3	SB RT	INLET BOX	1.2	
	29+72.86 37+22.80	749.94	16 to 1	708		24+72.84 25+98.53	126.03*	16	224		810+00	NB LT	INLET BOX	1.3		917+42	MED	SP P COLLAR	0.2	
RAMP C	10+00.00 15+29.93	529.93	TAPER	882		25+98.53 26+72.84	76.07*	16 to 14	127		819+25.18	AR	TRIPLE BOX CULV	1588 3		917+42	MED	SP JUNCTION BOX	2.4	
	15+29.93 15+89.93	60.00	19 to 17	120	RAMP C	16+62.39 18+62.39	202.10*	16	359		827+50	MED	4" SLOPE HDWL	0.1		917+59	MED	INLET BOX	1.6	
	15+89.93 16+62.40	72.47	16	129		18+62.39 20+62.39	203.20*	16	361		827+50	NB LT	4" SLOPE HDWL	0.1		936+00	MED	INLET BOX	1.9	
150' TAPER	150.00	6 to 1	58			20+62.39 28+41.62	800.94*	16	1,424		830+00	MED	INLET BOX	1.3		947+44	SB RT	INLET BOX	1.2	
RAMP D	30+54.00 31+54.00	100.00	14'	156		28+41.62 33+69.36	527.74	16 & VAR	1,409		830+00	NB LT	INLET BOX	1.3		947+44	MED	INLET BOX	1.2	
	31+54.00 33+54.02	200.02	20 to 14	400	RAMP D	10+23.00 15+54.69	531.69	16 & VAR	1,114		830+50	MED	4" SLOPE HDWL	0.1		947+44	NB LT	INLET BOX	1.2	
	33+54.02 41+03.95	749.93	16 to 1	708		15+54.69 20+57.80	492.52*	16	875		830+50	NB LT	4" SLOPE HDWL	0.1		947+62.50	SB LT	SP P COLLAR	4.2	
TOTAL				160,906		20+57.80 22+89.82	232.02	16	413		844+97	MED	INLET BOX	1.6		947+62.50	NB RT	HEADWALL	9.0	
						22+89.89 29+79.15	695.88	16	1,237		844+97	SB RT	INLET BOX	1.6		947+87	SB RT	INLET BOX	1.2	
						29+79.15 30+54.00	76.35	16 to 14	127		845+10	MED	SP P COLLAR	0.6		947+82	MED	INLET BOX	1.2	
						31+63.04 32+44.04	INTERSECTION		506		845+10	SB RT	SP P COLLAR	0.6		947+82	NB LT	INLET BOX	1.2	
						32+44.04 33+29.97	85.93	24	229		845+10	SB RT	SP P COLLAR	0.6		959+00	MED	INLET BOX	1.3	
						33+29.97 38+35.36	505.39	VAR	1,583		853+07	SB RT	INLET BOX	1.6		959+00	NB LT	SP JUNCTION BOX	2.4	
						38+35.36 40+77.16	242.80	VAR	920		853+07	MED	INLET BOX	1.6		959+13	NB LT	INLET BOX	1.2	
						40+77.16 42+77.16	200.00	28 to 40	756		853+25	SB RT	SP P COLLAR	0.6		966+50	MED	4" SLOPE HDWL	0.1	
						42+77.16 44+77.16	200.00	40	889		853+25	SB RT	SP JUNCTION BOX	2.6		966+50	NB LT	4" SLOPE HDWL	0.1	
						44+77.16 46+03.16	126.00	VAR	563		853+25	SB RT	BOX CULV	2.5		981+14.40	SB RT	INLET BOX	1.2	
						46+03.16 47+96.28	193.14	28	601		853+25	MED	BOX CULV	7.8		981+14	MED	INLET BOX	1.2	
						47+96.28 48+09.57	13.29	14	21		853+25	MED	SP JUNCTION BOX	3.0		981+14.50	NB LT	INLET BOX	1.2	
						51+85.05 51+86.88	1.83	14	3		853+43	SB RT	INLET BOX	1.6		981+28.60	SB RT	SP P COLLAR	0.6	
						51+86.88 53+86.88	200.00	28 to 40	756		853+43	MED	INLET BOX	1.6		981+28.60	SB LT	SP P COLLAR	0.6	
						53+86.88 55+81.19	194.31	40	864		870+00	SB RT	INLET BOX	1.6		981+28.60	MED	SP JUNCTION BOX	3.1	
						55+81.19 56+97.18	116.99	VAR	546		870+00	SB RT	INLET BOX	1.6		991+72	SB RT	INLET BOX	1.2	
						56+97.18 62+42.05	544.87	28	1,695		870+20.7	SB RT	SP P COLLAR	0.6		991+72	MED	INLET BOX	1.2	
						62+42.05 63+61.05	119.03	VAR	524		870+20.7	SB RT	SP JUNCTION BOX	3.0		991+86.70	SB	SP P COLLAR	2.9	
						63+61.05 63+84.95	23.90	28	74		870+20.7	SB RT	BOX CULV	0.8		991+86.70	MED	SP JUNCTION BOX	4.9	
						63+84.95 71+00.00	715.05	VAR	2,220		870+20.7	MED	SP P COLLAR	0.3		991+86.70	AR NB	BOX CULV	67.8	
						71+00.00 80.00	80.00	24	215		870+20.7	MED	INLET BOX	1.9		992+01.50	SB RT	INLET BOX	1.2	
											870+40	SB RT	INLET BOX	1.6		992+01.50	MED	INLET BOX	1.2	
											880+08	SB RT	INLET BOX	1.2		1013+00	NB LT	INLET BOX	1.9	
											880+08	MED	INLET BOX	1.6		1013+00	MED	INLET BOX	1.9	
											880+08	NB LT	INLET BOX	1.6		1025+00	MED	INLET BOX	1.9	
											880+21.8	SB RT	SP JUNCTION BOX	2.6		1037+55.70	SB RT	BOX CULV	132.3	
											880+21.8	SB RT	BOX CULV	1.6	CH 17	48+45.28 51+46.07		BRIDGE	1,217.4	
											880+21.8	SB	SP P COLLAR	0.6		39+81	RT.	SP HEADWALL	0.3	
											880+21.8	MED	BOX CULV	8.7		45+50	RT.	SP HEADWALL	0.3	
											880+21.8	MED	SP JUNCTION BOX	2.6		54+00	RT. & LT.	HEADWALL	10.8	
											880+21.8	NB LT	SP JUNCTION BOX	3.0		56+00	RT.	SP HEADWALL	0.3	
											880+35	SB RT	INLET BOX	1.2		62+00	LT.	SP HEADWALL	0.3	
											880+35	MED	INLET BOX	1.6						
																			TOTAL	3442.6

*SEE CROSS SECTIONS FOR BURIED PAVEMENT REMOVAL

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SCHEDULES OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	X	MONTE	206	10
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

WOVEN WIRE FENCE

ROADWAY	LOCATION	STATION TO STATION	LENGTH FT.
FAI 55	LT	756+81.97 758+00	118
	LT	758+00 759+91.08	191
	LT	759+91.08 833+65	7,374
	LT	833+65 836+82.75	318
	LT	836+82.75 838+39	156
	LT	838+39 839+24	85
	LT	839+24 874+64	3,510
	LT	874+64 874+98	34
	LT	874+98 912+00	3,702
	LT	912+00 912+56.28	59
	LT	912+56.28 968+68.28	12
	LT	912+68.28 913+00	58
	LT	913+00 915+31.70	232
	LT	915+31.70 916+31.70	102
	LT	916+31.70 917+66.54	105
	LT	917+66.54 920+60	293
	LT	920+60 922+47.26	188
	LT	922+47.26 1027+38.87	10,492
	RT	756+81.97 1023+13.82	26,637
	RT	1023+13.82 1028+41	555
	RT	1028+41 1031+92.60	352
E. FR. RD.	RT	1027+58.87 1034+29.65	709
	RT	1034+29.65 1038+43	413
RAMP D	RT	1038+43 16+35	315
	RT	16+35 11+20	534
CH 17	LT	11+20 61+49.16	366
E. FR. RD.	RT	61+49.16 1049+39.30	154
CH 17	RT	12+70 60+90	81
	RT	60+90 57+50	330
	RT	57+50 52+00	474
	RT	52+00 1046+00	106
FAI 55	LT	1046+00 1052+50	650
RAMP A	RT	1031+92.60 23+42.81	380
	RT	23+42.81 27+00	380
	RT	27+00 28+90	236
	RT	28+90 30+63.88	215
	RT	30+63.88 34+73	420
CH 17	LT	40+30 43+25	305
	RT	1046+15 42+30	43
	RT	42+30 44+85	256
	RT	44+85 44+98.62	31
	RT	44+98.62 1047+09.65	182
FAI 55	RT	1047+09.65 1050+04	334
W. ACCESS RD.	LT	1050+04 1051+75	187
		TOTAL	61,869

BITUMINOUS SURFACE TREATMENT (CLASS A-3)

ROADWAY	STATION TO STATION	LENGTH	7" AGGREGATE BASE COURSE, TYPE A		BITUMINOUS MATERIALS PRIME COAT		COVER COAT		SEAL COAT		COVER COAT AGGREGATE TONS	SEAL COAT AGGREGATE TONS
			WIDTH (FEET)	TONS	WIDTH (FEET)	GAL	WIDTH (FEET)	GAL	WIDTH (FEET)	GAL		
EAST FRONTAGE ROAD #1	RETURNS - TR 1		VAR	57	VAR	50	VAR	86	VAR	43	2	1
	752+31.01 842+12.89	8,981.88	19	7,559	19	6,637	18	10,780	18	5,390	360	180
	RETURNS - TR 7		VAR	57	VAR	50	VAR	86	VAR	43	2	1
SERVICE DRIVE #1	874+84.00 920+81.52	4,597.52	19	3,869	19	3,397	18	5,518	18	2,759	184	92
	RETURNS - TR 17		VAR	57	VAR	50	VAR	86	VAR	43	2	1
EAST FRONTAGE ROAD #2	RETURNS - TR 17		VAR	57	VAR	50	VAR	86	VAR	43	2	1
	912+33.94 1052+10.91	13,976.97	21	13,000	21	11,415	20	18,636	20	9,318	622	311
	RETURNS		VAR	133	VAR	117	VAR	200	VAR	100	6	3
EAST ACCESS ROAD	RETURNS		VAR	118	VAR	104	VAR	178	VAR	89	6	3
	10+30.78 15+75.00	544.22	19	458	19	402	18	654	18	327	22	11
CH 17 EAST END DETOUR	0+75.92 6+58.86	582.94			20							
	TOTALS			25,365		22,726		36,310		18,955	1,208	604

COMBINATION CONCRETE CURB & GUTTER

TYPE M-606

ROADWAY	STATION TO STATION	L.F.
CH 17	35+90.29 38+78±	575
	39+44± 44+95±	1,109
	ISLAND	106
	45+51± 48+45.78	591
	51+45.57 55+99±	911
	ISLAND	86
	56+49± 62+72±	1,258
	63+32 67+70.00	878
	TOTAL	5,514

COMBINATION CONCRETE CURB & GUTTER - TYPE M-6.24

ROADWAY	STATION TO STATION	L.F.
CH 17	RAMP A RAMP B	170
	RAMP C RAMP D	172
W. ACCESS RD.	RT 1042+80.65 1048+00.00	519
	LT 1043+47.46 1048+00.00	453
	TOTAL	1,314

COMBINATION CONCRETE CURB & GUTTER - TYPE M-6.12

ROADWAY	STATION TO STATION	L.F.
CH 17	WEST ACCESS RD. ISLAND	56
	EAST FRONTAGE RD. ISLAND	70
	TOTAL	126

BITUMINOUS SURFACE PLANT MIX (CLASS B)

ROADWAY	STATION TO STATION	LENGTH FT.	6" STABILIZED BASE COURSE		9" STABILIZED BASE COURSE		3" BITUMINOUS MIX COMPLETE		2" BITUMINOUS MIX COMPLETE	
			WIDTH FT.	SQ. YD.	WIDTH FT.	SQ. YD.	WIDTH	TONS	WIDTH	TONS
WEST FRONTAGE ROAD	1021+76.82 1051+05.77	2,928.95	25.5	8,299			24	1,312		
	RETURNS		VAR	311			VAR	52		
WEST ACCESS ROAD	1042+22.78 1051+75	952.22			25.5	2,698	24	427		
	RETURNS				VAR	142	VAR	24		
CH 17 EAST END DETOUR		582.94							20	145
	TOTAL			8,610		2,840		1,815		145

AGGREGATE SURFACE COURSE 8" TYPE A PRIVATE ENTRANCES

ROADWAY & SIDE	STATION	LENGTH FT.	WIDTH FT.	TON
E. FRONT. RD. LT	825+25	41	16	38
E. FRONT. RD. LT	970+68	15	16	17
W. ACCESS RD. LT	1049+00	76	20	82
CH 17 LT	33+25	14	16	12
CH 17 RT	33+68	14	16	12
E. ACCESS RD. LT	13+00	200	16	172
	TOTAL			333

CONCRETE MEDIAN TYPE C-4

ROADWAY	STATION TO STATION	LENGTH FT.	WIDTH FT.	AREA (SQ. FT.)
CH 17	35+20.29 36+34.62	114.33	VAR	454
	66+99.76 68+80.00	180.24	VAR	1,716
	TOTAL			1,170

STEEL PLATE BEAM GUARD RAIL, SINGLE RAIL

ROADWAY	SIDE	STATION TO STATION	LENGTH FT.
FAI 55	RT	818+31 820+18.5	187.5
	LT	818+25 820+25	200.0
CH 17	RT	47+53 48+28	75.0
	LT	51+54 52+29	75.0
W.FR.RD	RT	1051+60± 1051+85±	25.0
	RT	1052+35±	25.0
	TOTAL		587.5

CONCRETE MEDIAN SURFACE 4 INCHES

ROADWAY	STATION TO STATION	LENGTH FT.	WIDTH FT.	AREA (SQ. FT.)
CH 17	35+90.29 38+78±	288±	VAR	3,364
	W. ACCESS RD. ISLAND			143
	39+44± 44+95±	551±	VAR	4,832
	45+51± 48+45.78	295±	VAR	4,458
	RAMP A ISLAND			396
	51+45.57 55+99±	454±	VAR	3,274
	RAMP C ISLAND			286
	56+49± 62+72±	623±	VAR	9,686
	E. FRONTAGE RD. ISLAND			266
	63+32± 67+70.00	438±	VAR	5,151
	TOTAL			31,856

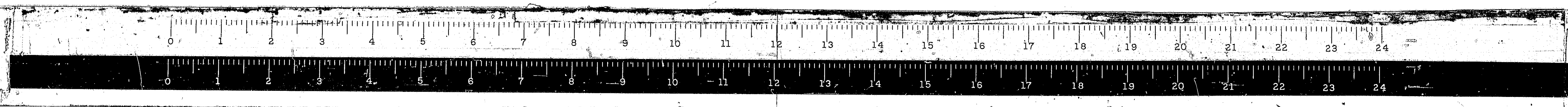
TERMINAL SECTIONS, SINGLE RAIL

ROADWAY	SIDE	STATION TO STATION	EACH
FAI 55	RT	818+06 818+31	1
		820+18.5 820+43.5	1
	LT	818+00 818+25	1
		820+25 820+50	1
CH 17	RT	47+28 47+53	1
	LT	52+29 52+54	1
	TOTAL		6

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SCHEDULES OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	#	MONT.	206	12
FED. ROAD DIST. NO. 4			ILLINOIS PROJ.	

AGGREGATE SHOULDER TYPE A - CONT'D

ROADWAY	STATION TO STATION	LENGTH (FT)	TONS	
			ALT. A	ALT. B
RAMP B	RT 10+23.00 11+26.46	157.88*	35	24
	11+26.46 15+46.54	420.08	92	62
	15+46.54 22+72.84	703.06*	196	133
	22+72.84 24+72.84	196.80*	55	37
	24+72.84 26+72.84	197.90*	55	37
	26+72.84 36+72.80	999.96	220	149
	36+72.80 37+22.80	50.00	10	7
	LT 10+83.00 15+46.54	463.54	109	74
	15+46.54 22+72.84	790.22*	186	126
	22+72.84 24+72.84	208.80*	49	33
24+72.84 25+51.71	81.15	19	13	
RAMP C	RT 10+00.00 10+50.00	50.00	8	5
	10+50.00 16+62.39	612.39	135	92
	16+62.39 18+62.39	197.89*	55	37
	18+62.39 20+62.39	196.80*	55	37
	20+62.39 28+41.62	757.53*	211	143
	28+41.62 32+37.88	396.26*	87	59
	32+37.88 33+69.36	195.79*	43	29
RAMP C	LT 16+62.39 18+62.39	205.79*	48	33
	18+62.39 20+62.39	208.80*	48	33
	20+62.39 28+41.62	838.92*	197	134
	28+41.62 32+60.00	418.38	98	66
RAMP D	RT 10+23.00 11+07.68	127.94*	28	19
	11+07.68 15+54.69	447.01	98	66
	15+54.69 20+57.80	513.61*	113	77
	20+57.80 22+89.82	232.02	58	39
	22+89.82 30+54.00	756.14*	211	143
	30+54.00 40+53.95	999.95	220	149
	40+53.95 41+03.95	50.00	8	5
RAMP D	LT 10+87.02 15+54.69	467.67	111	75
	15+54.69 20+57.80	474.23*	148	100
	20+57.80 22+89.82	232.02	64	43
	22+89.82 29+32.33	661.10*	156	106
	29+32.33 32+19.00	281.00	154	103
	32+19.00 38+05.88	305.88	258	169
	38+05.88 40+34.18	228.30	92	64
CH 17	40+34.18 43+43.17	308.99	260	182
	43+43.17 46+76.62	333.45	141	96
	46+76.62 47+96.28	119.56	104	73
	47+96.28 48+45.78	49.50	24	17
	48+45.78 51+86.88	41.41	17	12
	51+86.88 54+60.02	273.14	238	167
	54+60.02 57+54.50	294.48	132	93
	57+54.50 58+29.48	74.98	65	46
	58+29.48 61+51.95	322.47	212	149
	61+51.95 63+92.92	240.97	92	63
	63+92.92 73+08.07	915.15	775	544
			13764	13461

* ARC LENGTH

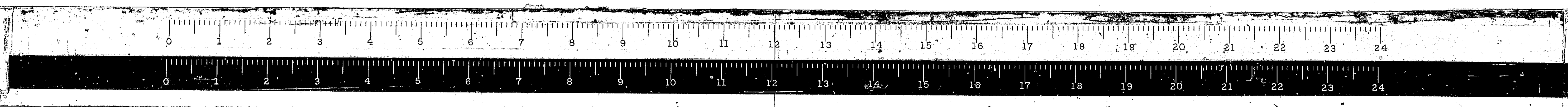
AGGREGATE SHOULDER TYPE A

ROADWAY	STATION TO STATION	LENGTH (FT)	TONS	
			ALT. A	ALT. B
FAI-55 N.B. RT& S.B. LT	760+00.00 1052+50.00	29,250.00	12,880	8,733
RAMP A	RT 10+00.00 10+50.00	50.00	6	4
	10+50.00 16+62.40	612.40	135	92
	16+62.40 23+42.81	673.24*	188	128
	23+42.81 25+74.81	232.00	58	39
	25+74.81 30+57.43	493.78	109	75
	30+57.43 34+46.98	389.55	86	58
	34+46.98 36+80.97	204.22*	45	30
	36+80.97 37+22.80	50.00	10	7
	LT 16+62.40 23+42.81	700.10*	165	112
	23+42.81 25+74.81	232.00	64	43
25+74.81 30+57.43	451.93	141	96	
30+57.43 34+68.00	410.00	97	66	

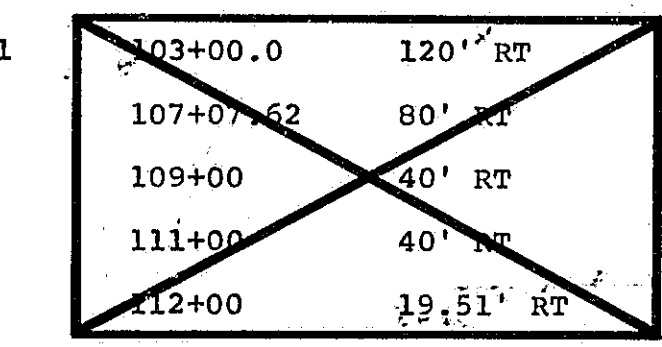
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SCHEDULES OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



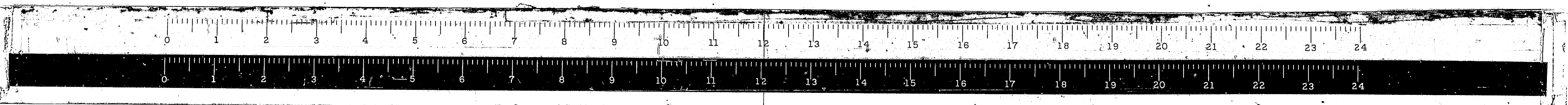
BUILDING REMOVAL				FURNISHING AND ERECTING RIGHT OF WAY MARKERS						PERMANENT SURVEY MARKERS, TYPE II						ROUTE NO. SECTION COUNTY TOTAL SHEETS SHEET NO.		
BLDG NUMBER	STATION	OFFSET	REMARKS	ROADWAY	STATION	OFFSET & SIDE	ROADWAY	STATION	OFFSET & SIDE	ROADWAY	STATION	OFFSET & SIDE	ROADWAY	STATION	OFFSET & SIDE	REMARKS	GRID CO-ORDINATES NORTH	EAST
1	756+65	56' LT.	1 STORY, WOOD FRAME, BLOCK-CELLAR	FAI 55	756+81.97	125' LT	E. FRONT RD.	752+66.50	35' LT	W. ACCESS RD.	1043+90	100' RT	FAI 55	755+83.85	0		1,040,855.673	647,020.846
2	756+80	140' LT.	WOOD FRAME		758+00	125' LT		753+96.48	35' LT		1046+15	81.24' LT		755+83.85	24.15' RT	TRANSIT LINE	1,040,855.261	646,996.702
3	804+40	30' LT.	1 STORY, WOOD FRAME, BLOCK FOUNDATION		759+91.08	130' LT		753+96.48	40' RT		1048+59.81	70' RT		768+15.80	0		1,039,623.992	647,041.887
4	805+20	30' LT.	1 STORY, WOOD FRAME, BLOCK FOUNDATION		761+61.60	180' LT		756+30.76	35' LT		1049+24.87	70' RT		768+15.80	31' RT	TRANSIT LINE	1,039,623.462	647,010.894
5	805+20	80' LT.	WOOD FRAME		768+15.80	180' LT		756+30.76	40' RT	E. ACCESS RD.	12+20	53.11' LT		779+00	0			
6	805+00	130' LT.	WOOD FRAME		771+50	180' LT		757+78.17	40' RT		12+70	62' RT		790+33.63	0		1,037,406.647	647,080.144
7	804+70	190' LT.	WOOD FRAME		772+50	195' LT		757+78.17	35' LT		14+00	50' RT		790+33.63	31.12' RT	TRANSIT LINE	1,037,406.110	647,049.031
8	808+20	120' LT.	WOOD FRAME, CONCRETE FOUNDATION		773+65	195' LT		760+04.17	40' RT		14+00	60' LT		807+41.22	0		1,035,699.431	647,109.600
9	833+40	75' LT.	BLOCK CONSTRUCTION, FOUNDATION & BASEMENT		775+00	180' LT		837+44.19	45' RT		15+75	35' RT		807+41.22	31.04' RT	TRANSIT LINE	1,035,698.896	647,078.567
10	833+40	118' LT.	WOOD FRAME, CONCRETE FLOOR		798+00	180' LT		837+97.43	45' LT	TOWN RD. #1	103+00.0	120' RT		817+45.81	31.09' RT	TRANSIT LINE	1,034,694.524	647,095.846
11	833+40	145' LT.	WOOD FRAME, BLOCK FOUNDATION		817+00	190' LT		839+91.20	40' LT		107+07.62	80' RT		832+41.52	0		1,033,199.676	647,152.729
12	832+55	135' LT.	OUT HOUSE		819+00	220' LT		840+75.00	30' RT		109+00	40' RT		837+08.62	0		1,032,732.713	647,162.586
13	835+50	90' LT.	WOOD FRAME, CONCRETE FLOOR		821+00	195' LT		841+69.98	40' LT		111+00	40' RT		837+08.62	32.15' RT	TRANSIT LINE	1,032,732.035	647,130.445
14	899+85	125' LT.	WOOD FRAME, CONCRETE FOUNDATION		830+00	195' LT		913+90	50' LT		112+00	19.51' RT		840+00	0			
15	900+60	125' LT.	WOOD FRAME, BLOCK FOUNDATION		833+65	190' LT		913+90	50' RT	C. H. 17	33+67	30' LT		852+00	0			
16	901+00	70' LT.	WOOD FRAME, BLOCK FOUNDATION		836+82.75	125' LT		914+05.68	50' RT		34+25.66	29.92' RT		863+42.72	0		1,030,099.382	647,218.173
17	901+15	70' LT.	WOOD FRAME		863+42.72	125' LT		914+05.68	50' LT		34+25.66	35.17' RT		863+42.72	30' RT	TRANSIT LINE	1,030,098.749	647,188.182
18	901+10	35' LT.	2 STORY WOOD FRAME, BLOCK FOUNDATION		874+64	125' LT		914+96.08	50' RT		35+75.29	51.19' RT		876+87.52	0		1,028,755.033	647,249.140
19	924+00	40' LT.	WOOD FRAME ON 6 BLOCK HIGH WALL, CONCRETE FLOOR		874+98	128' LT		914+96.08	60' LT		36+60	65.78' LT		876+87.52	24.47' RT	TRANSIT LINE	1,028,754.470	647,224.678
20	923+40	0'	WOOD FRAME		889+74.03	128' LT		916+34.33	40' LT		38+26.36	108.14' RT		891+00	0			
21	966+90	145' LT.	CONCRETE SLAB		889+74.44	180' LT		916+34.33	40' RT		38+30	100' LT		899+93.67	0		1,026,449.656	647,302.245
22	966+50	80' LT.	CONCRETE SLAB		910+64.32	180' LT		916+84.66	50' RT		40+30	150' LT		899+93.67	32.48' RT	TRANSIT LINE	1,026,448.908	647,269.776
23	966+70	80' LT.	CONCRETE SLAB		913+00	160' LT		916+84.66	50' LT		40+50	130' LT		905+13.78	0		1,025,929.719	647,314.222
24	967+00	130' LT.	CONCRETE BLOCK CONSTRUCTION		922+47.26	185' LT		918+14.18	40' LT		42+30	170' RT		915+00	0			
25	968+00	95' LT.	2 STORY WOOD FRAME, BLOCK CELLAR		932+50	185' LT		918+14.18	40' RT		44+98.62	116.92' RT		916+39.21	116.90' RT	SECTION CORNER		
26	970+85	90' LT.	2 STORY WOOD FRAME, CONCRETE & BLOCK FOUNDATION		940+00	200' LT		918+14.18	40' RT		52+00	140' RT		918+00	0			
27	972+50	55' LT.	1 STORY WOOD FRAME		919+20.19	40' LT		919+20.19	40' RT		57+50	160' RT		927+01.33	0		1,023,742.860	647,362.722
28	972+25	135' LT.	1 STORY METAL GARAGE		950+00	200' LT		919+20.19	40' LT		60+90	133' RT		927+01.33	33.09' RT	TRANSIT LINE	1,023,742.126	647,329.642
29	1048+00	85' LT.	CONCRETE BLOCK CONSTRUCTION, CONCRETE FOUNDATION		963+50	183' LT		920+15	40' RT		61+49.16	113.02' LT		927+01.33	0		1,021,709.212	647,407.824
30	1051+00	100' LT.	BLOCK CONSTRUCTION, CONCRETE FOUNDATION		969+51.36	183' LT		920+83.78	59.97' RT		64+60	124' LT		947+35.62	33.08' RT	TRANSIT LINE	1,021,708.479	647,374.754
					1027+38.87	170' LT		1027+38.87	35' LT		64+70	104.73' RT		958+00	0			
					971+00	170' LT		1034+29.65	35' LT		67+80	115' RT		969+36.08	0		1,019,509.447	647,456.610
					973+00	185' LT		1038+43.11	50' LT		68+00	80' LT		969+36.08	32.72' RT	TRANSIT LINE	1,019,508.722	647,423.500
					976+00	185' LT		1039+25	60' RT		71+00	60' LT		986+73.06	0		1,017,773.016	647,495.120
					990+00	200' LT		1040+00	55' RT		72+00	60' RT		986+73.06	32.69' RT	TRANSIT LINE	1,017,772.291	647,462.440
					995+00	180' LT		1043+25	50' LT		72+90	60' RT		1005+97.20	0		1,015,849.484	647,537.780
					1009+00	180' LT		1046+25	60' LT		73+01.50	35' RT		1005+97.20	32.69' RT	TRANSIT LINE	1,015,848.759	647,505.100
					1014+50	190' LT		1049+39.30	60' LT		73+33.58	35' LT		1022+92.13	166.83' RT	SECTION CORNER		
					1020+00	190' LT		1049+39.30	55' RT		73+45	60' LT		1023+04.02	0		1,014,143.203	647,575.622
					1023+13.82	218.61' RT	W. FRONT RD.	1026+00	60' RT		TOTAL	139		1023+04.02	33' RT	TRANSIT LINE	1,014,142.471	647,542.632
					1029+58	222.02' RT		1029+57.70	50' LT					1030+00	0			
					1031+92.60	223.27' RT		1033+19.39	50' LT					1035+92.95	0		1,012,854.680	647,604.199
					1046+00	130' LT		1033+19.39	60' RT					1035+92.95	33.09' RT	TRANSIT LINE	1,012,853.946	647,571.119
					1047+09.65	231.29' RT		1038+18.27	50' LT					1043+00	0			
					1052+00	130' LT		1038+18.27	50' LT					1046+25.88	0		1,011,822.076	647,627.100
					RAMP A	23+42.81	60' RT	1044+00	50' RT					1046+25.88	32.95' RT	TRANSIT LINE	1,011,821.345	647,594.160
						27+00	75' RT	1045+00	50' LT					TOTAL	48			
						28+90	90' RT	1045+50	60' RT									
						30+63.88	100.68 RT.	1047+50	85' LT									
						34+50	130' RT	1047+65	80' RT									
						RAMP D	11+20	130' RT										
						16+35	80' RT											



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SCHEDULES OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	138
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ. 7-85-2177	

TREE REMOVAL									
STATION	OFFSET	6" to 15" IN. DIA.	OVER 15" IN. DIA.	ACRES	STATION	OFFSET	6" to 15" IN. DIA.	OVER 15" IN. DIA.	ACRES
FAI-55					FAI-55				
755+95	12' RT.	12			795+18	20' RT.		31	
755+98	38' LT.	7			795+46	28' RT.		29	
756+05	95' LT.		24		803+27-809+52				5.7
756+32	95' RT.		21		811+85	18' RT.	15		
756+32	95' LT.	10			812+85	18' RT.		17	
756+57	22' RT.		22		817+75	24' RT.		17	
756+75	95' LT.		24		818+55	18' RT.		23	
756+86	15' LT.		20		819+50	20' RT.		22	
756+95	95' LT.	12			820+50	19' RT.		27	
751+05	25' LT.	7			828+58	182' LT.	14		
757+35	95' LT.	10			824+25	184' LT.	14		
757+42	28' LT.	9			830+30	18' RT.		22	
757+45	5' RT.		21		832+54-834+70				2.0
757+45	5' RT.		19		835+10	15' LT.		29	
757+92	22' RT.		20		835+16	28' LT.		29	
758+92	20' RT.		22		835+58	0		33	
759+95	15' RT.		27		836+20	52' LT.		28	
760+35	95' RT.		28		867+00	18' RT.		18	
769+00	25' RT.		32		867+30	18' RT.		20	
769+93	25' RT.		35		867+90	18' RT.		20	
771+15	35' LT.		44		868+05	18' RT.		20	
771+45	0		50		898+60	8' RT.	6		
771+75	117' LT.		57		899+00	8' RT.	6		
772+07	38' LT.	13			899+20	8' RT.	6		
773+06	5' LT.		20		899+25	8' LT.	7		
773+50	130' LT.		32		899+40	8' RT.	6		
773+60	20' RT.		41		899+60	8' RT.	7		
773+70	20' LT.		20		899+65	8' RT.	7		
773+72	12' LT.		41		899+80	8' RT.	6		
774+60	16' RT.		26		899+85	8' LT.	10		
775+85	12' RT.		24		899+95	8' LT.	12		
776+06	24' RT.		27		899+98	8' RT.	6		
776+35	18' RT.		20		902+75	8' RT.		27	
780+82	18' RT.		32		925+45	12' RT.		19	
781+82	15' RT.		29		926+38	30' RT.	15		
781+92	95' RT.		23		931+40	30' LT.		17	
782+30	95' RT.		26		931+62	18' LT.		28	
785+90	18' RT.		20		967+50	30' LT.		21	
787+86	18' RT.		27		967+50	125' LT.		41	
789+70	15' RT.	14			967+55	55' LT.		21	
789+80	15' RT.		24		967+84	55' LT.	12		
794+05	16' RT.		31		967+90	55' LT.		23	
794+60	25' RT.		31		968+30	90' LT.	14		
CH-17					CH-17				
					35+85	55' LT.	11		
					36+60	48' LT.	11		
					37+26	74' LT.		25	
M. Access Road					M. Access Road				
					1043+04	42' RT.	6		
					1043+30	32' RT.	6		
					1048+22	17' LT.	11		
TOTAL					TOTAL				
							320	2341	7.7

DITCH LININGS									
ROADWAY	STATION TO STATION	SIDE	LENGTH	PAVED DITCH	EXCELSIOR BLANK				
				FT.	SQ. YDS.				
FAI 55	912+29 TO 912+56.28	LT	80'	80					
E. FRONTAGE RD.	910+75 TO 912+40	RT	165'		220				
	912+75 TO 914+25	RT	150'		200				
	771+00 TO 775+00	LT	400'		533				
	810+00 TO 818+75	LT	875'		1,167				
	820+00 TO 830+00	LT	1,000'		1,333				
RAMP A	16+62 TO 24+20	RT	758'		337				
	22+00	LT	25'		22				
	23+82 TO 31+00	LT	718'		957				
	25+75 TO 31+61	MED	586'		521				
RAMP B	14+42 TO 26+73	RT	1,231'		547				
	15+00 TO 20+50	MED	550'		733				
RAMP C	16+62 TO 29+46	RT	1,284'		565				
	18+37	RT	25'		22				
	21+50	RT	25'		11				
	24+20 TO 29+94	LT	574'		765				
RAMP D	21+62 TO 30+54	RT	892'		396				
	14+00 TO 21+00	LT	700'		933				
	14+51 TO 20+06	LT	555'		493				
CH 17	34+91 TO 38+50	LT	359'		160				
	36+40	RT	8'		4				
	39+82	RT	120'		53				
	45+50	RT	92'		41				
	44+75 TO 46+50	RT	175'		233				
	46+40 TO 48+45	RT	205'		91				
	48+20	LT	10'		4				
	51+46 TO 59+29	RT	783'		348				
	51+51	RT	10'		4				
	54+00 TO 54+25	RT	25'		56				
	55+81	RT	10'		4				
	56+00	RT	108'		44				
	61+30 TO 62+30	LT	100'		11				
	62+00	LT	25'		41b				
	63+50 TO 72+86	LT	936'		4				
	67+65	RT	10'		4				
TOTALS				80	11,276				

FILL EXISTING WELLS		
ROADWAY	STATION	OFFSET
FAI 55	757+12	43' LT
	771+75	75' LT
	805+90	55' LT
	807+62	100' LT
	833+75	92' LT
	835+65	79' LT
	901+15	70' LT
	924+52	9' RT
	931+50	20' LT
	967+95	142' LT
	971+25	20' LT
	1050+25	120' LT
	1051+50	123' LT
TOTAL		
13		

FILL EXISTING CISTERNS		
ROADWAY	STATION	OFFSET
FAI 55	777+10	96' LT
	901+75	70' LT
TOTAL		
2		

COMBINATION CURB AND GUTTER REMOVAL			
ROADWAY	STATION TO STATION	LENGTH (FT.)	
CH 17	R LT 32+63 38+26	568	
	R RT 32+61 37+79	534	
TOTAL		1102	

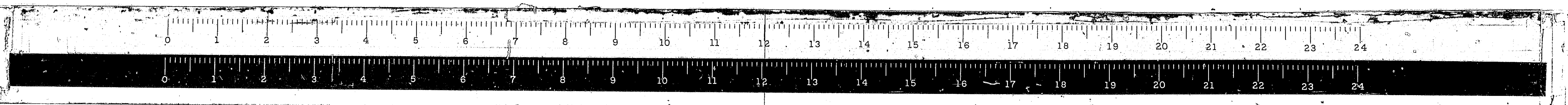
HEDGE REMOVAL									
STATION	OFFSET	UNITS	ROADWAY	STATION TO STATION	LENGTH (FT.)	WIDTH (FT.)	AREA (SQ. FT.)		
FAI 55			CH 17	R LT 35+09 37+65	256	4	1024		
983+16	60' LT.	1		R LT 36+42	17	6	102		
1035+95-1036+10	240' RT.	5		R LT 36+96	25	8	200		
				R LT 37+61	30	8	240		
TOTAL		6					1566		

EXISTING GUARD RAIL REMOVAL									
ROADWAY	SIDE	STATION	METAL G.R.	SPBGR					
FAI 55	RT	818+90±	63						
	RT	819+60±	113						
	RT	838+99 TO 840+61.5		162.5					
	RT	916+04 TO 917+66.5		162.5					
TOTALS			176	325.0					

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SCHEDULES OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAJ.55	*	MONT.	206	13C
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	

BITUMINOUS PAVEMENT									
ROADWAY	STATION TO STATION	LENGTH FT.	STABILIZED WIDTH FT.	BASE COURSE 8" DEPTH SQ. YD.	BITUMINOUS CONCRETE BINDER COURSE WIDTH FEET	TONS	BITUMINOUS CONCRETE I SURFACE COURSE CLASS I WIDTH FEET	TONS	AGGREGATE SHOULDER TYPE B TON
CH 17	71+00 72+85.70	185.70	VAR.	500	VAR.	70	VAR.	49	
	72+85.70 73+08.01	22.31	24	60	24	8	24	6	
FAI 55	TEMPORARY CONNECTIONS 1 & 2	793.50 & 637.19	24 & VAR.	2030	24 & VAR.	199	24 & VAR.	142	52
FAI 55	TEMPORARY CONNECTIONS 3 & 4	1022.34 & 821.50	24 & VAR.	2064	24 & VAR.	203	24 & VAR.	145	55
TOTAL				4654		480		342	107

SUBBASE GRANULAR MATERIAL, TYPE A					
ROADWAY	STATION TO STATION	LENGTH FT.	END AREA SQ. FT.	CU. YDS.	TONS
W. FR. RD. #1	818+85 819+65	80	13.50	40	82
C. H. 17	47+96.28 48+45.78	49.50	10.39	19	39
	48+09.57 48+45.78	36.21	10.39	14	29
	51+45.57 51+85.05	39.48	10.39	15	31
	51+45.57 51+86.88	41.31	10.39	16	33
	71+00 72+85.70	185.70	32.61	224	459
	72+85.70 73+08.01	22.31	32.53	27	55
FAI. N.B.	827+50 834+00	650.0	22.4	540.3	1108
	966+50 969+00	250.0	22.4	207.8	425
FAI. S.B.	827+50 834+00	650.0	12.1	290.3	595
	966+50 969+00	250.0	21.1	195.4	401
TOTAL					3257

DETOUR ROADWAY REMOVAL			
ROADWAY	STATION TO STATION	LENGTH FT.	SQ. YD.
F.A.I. 55 NB	TEMPORARY CONNECTION 1 - SEC. 84-5	757.36	1,185
SB	TEMPORARY CONNECTION 2 - SEC. 84-5	629.84	845
NB	TEMPORARY CONNECTION 1 - SEC. 68-4B	793.50	1,281
SB	TEMPORARY CONNECTION 2 - SEC. 68-4B	637.16	864
T.R. 1	DETOUR		754
T.R. 7	DETOUR		754
C.H. 16B	DETOUR		756
T.R. 17	DETOUR		782
T.R. 21	DETOUR		1,288
T.R. 23	DETOUR		782
C.H. 17	DETOUR AT F.A.I. 55		1,340
	DETOUR EAST END		1,295
TOTAL			11,926

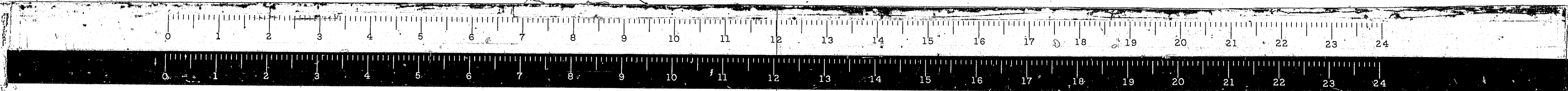
PERMANENT BARRICADES		
ROADWAY	STATION	LENGTH (FEET)
F.A.I. 55 NB	760+32±	44
U.S. 66 SB	760+45±	44
F.A.I. 55 SB	1052+23±	44
U.S. 66 NB	1053+64±	44
W. ACCESS RD.	1051+50±	44
U. S. 66 SB	817+00±	44
U. S. 66 SB	821+00±	44
U. S. 66 SB	1019+50±	44
EXIST. C.H. 17	47+10±	36
TOTAL		388

DESIGNED BY F.T.P.
 DRAWN BY BA
 CHECKED BY WET
 APPROVED BY _____

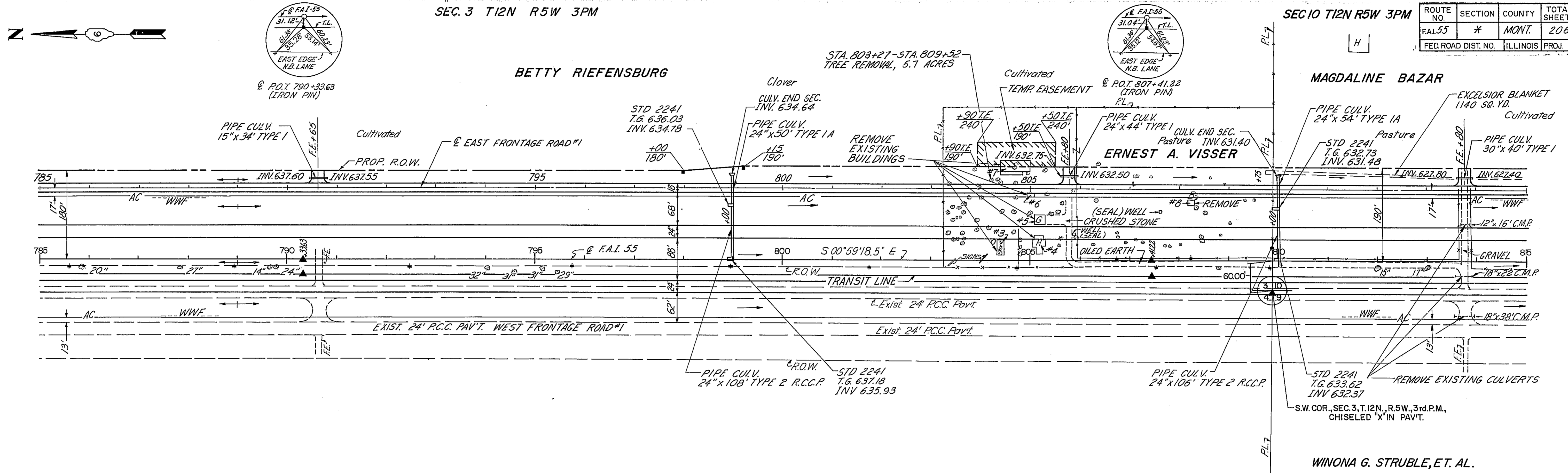
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

SCHEDULES OF QUANTITIES

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



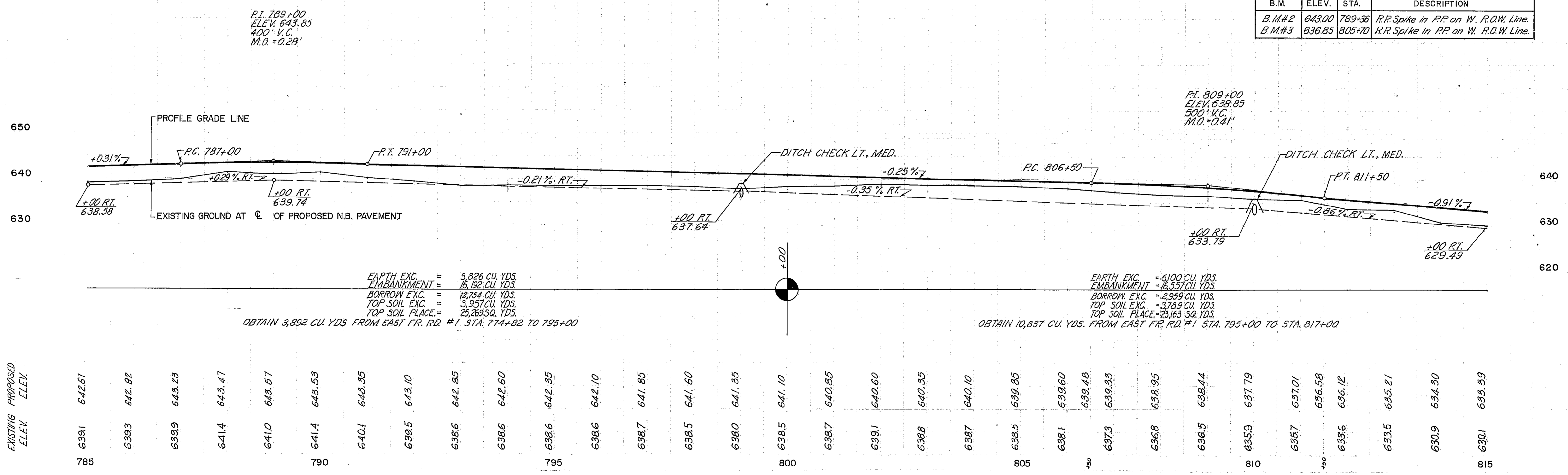
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	15
FED. ROAD DIST. NO.			ILLINOIS	PROJ.



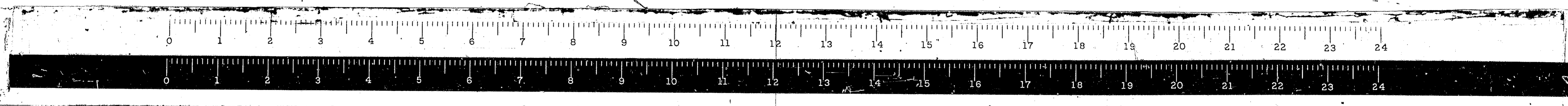
RUSSEL & HENRY THOMAS
 SEC. 4 T12N R5W 3PM

SEC. 9 T12N R5W 3PM

B.M.	ELEV.	STA.	DESCRIPTION
B.M.#2	643.00	789+36	R.R. Spike in PP on W. R.O.W. Line
B.M.#3	636.85	805+70	R.R. Spike in PP on W. R.O.W. Line

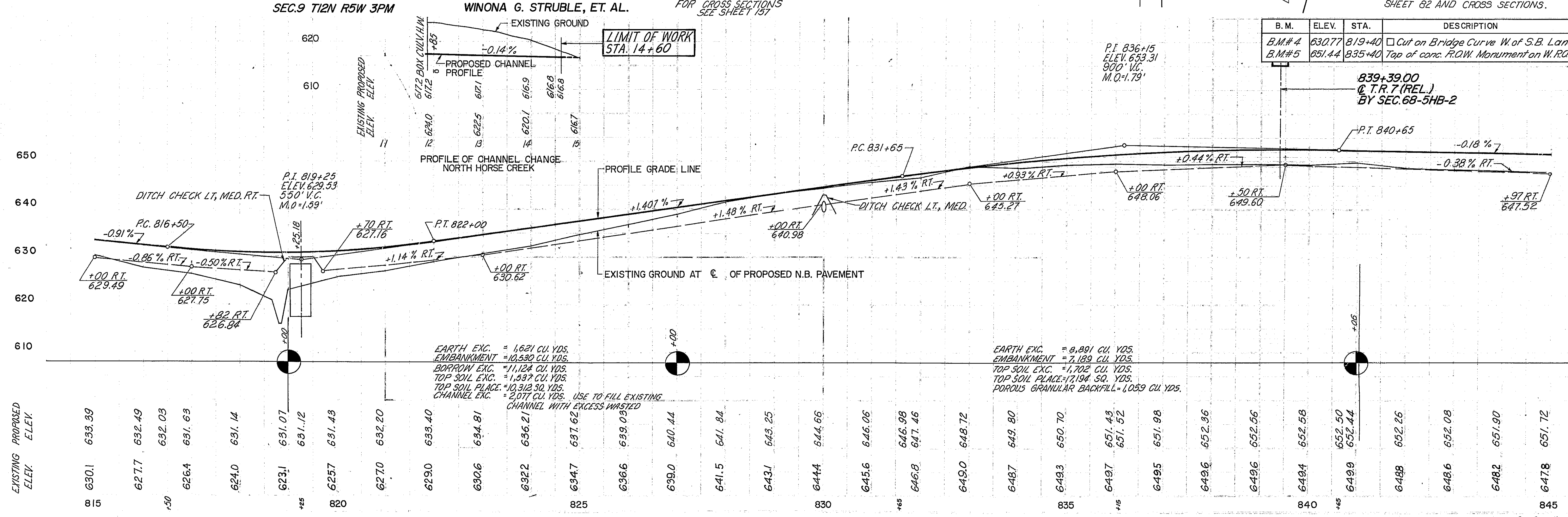
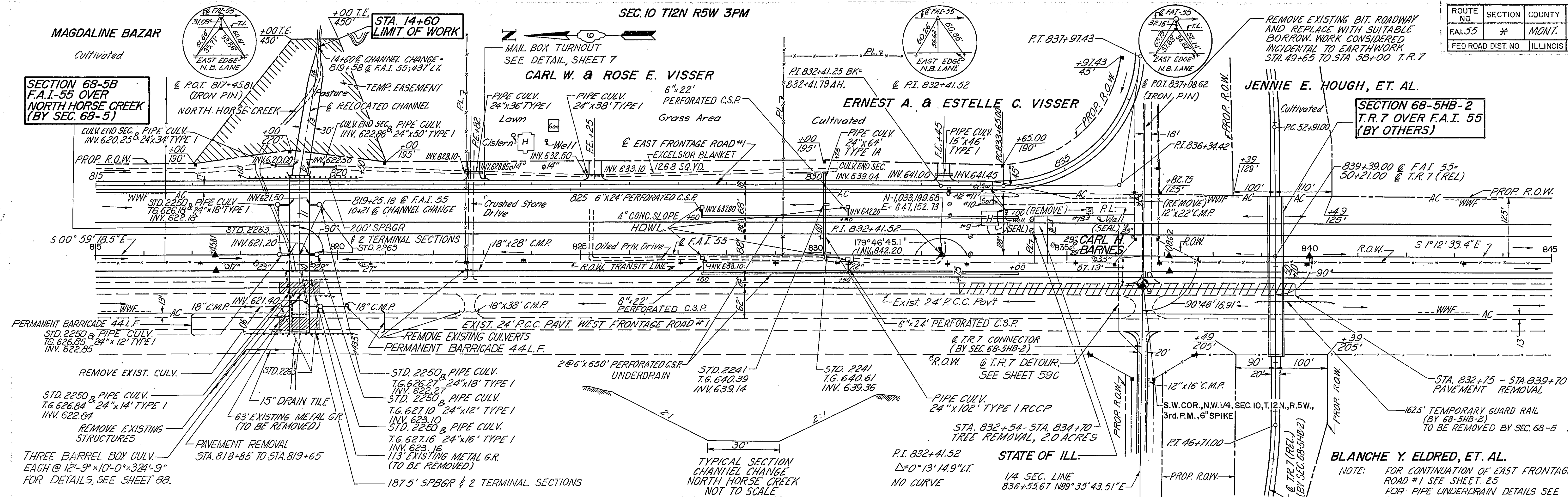


STA. 785+00 TO STA. 815+00 FAI.55



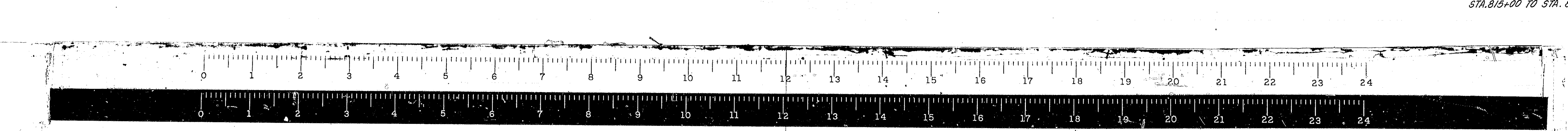
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	16
FED ROAD DIST NO.		ILLINOIS	PRO.	



B.M.	ELEV.	STA.	DESCRIPTION
B.M.# 4	630.77	819+40	Cut on Bridge Curve W. of S.B. Lane.
B.M.# 5	651.44	835+40	Top of conc. R.O.W. Monument on W.R.O.W. Line

839+39.00
Q.T.R. 7 (REL.)
BY SEC. 68-5HB-2



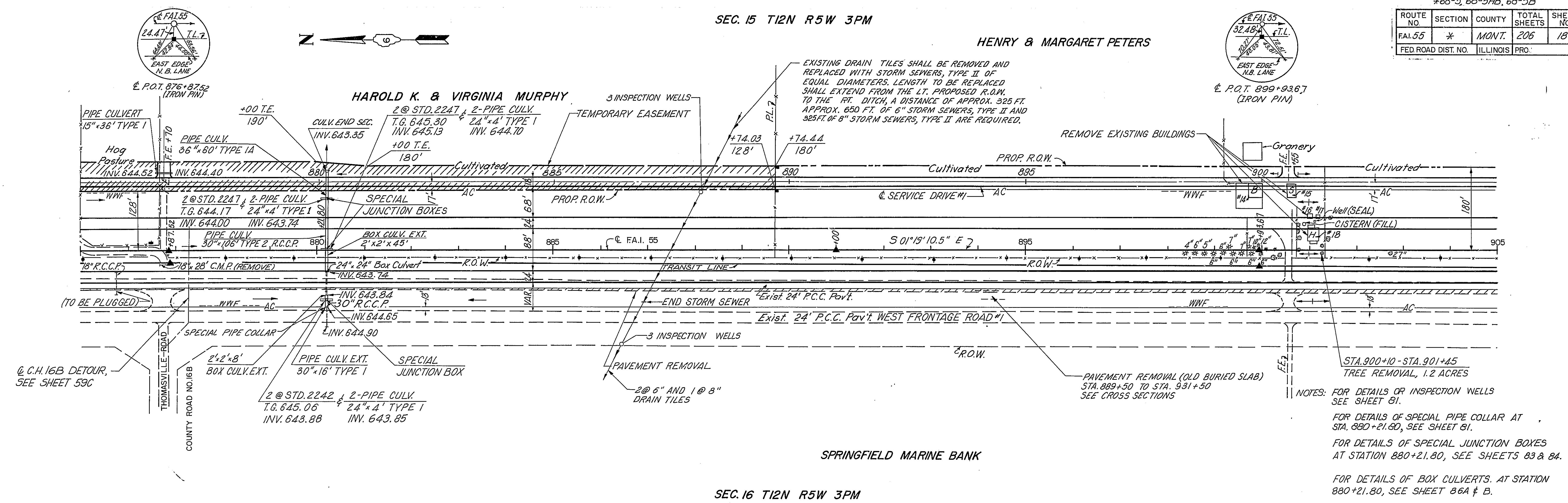
STA. 815+00 TO STA. 845+00 F.A.I. 55

#68-5, 68-54B, 68-55

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	18
FED. ROAD DIST. NO.		ILLINOIS PRO.		

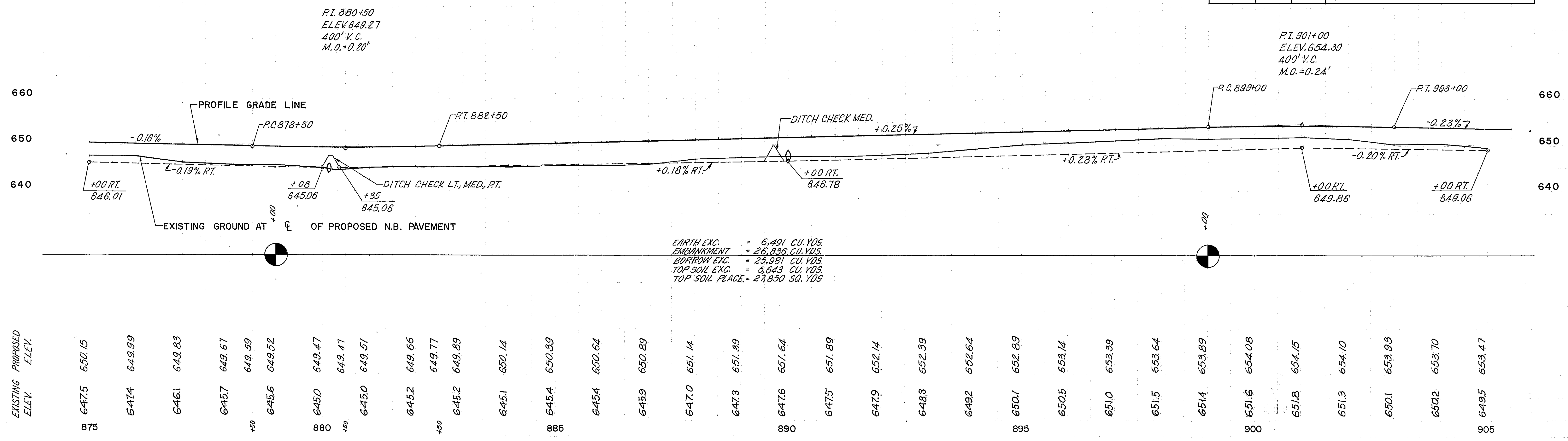
SEC. 15 T12N R5W 3PM

HENRY & MARGARET PETERS



SEC. 16 T12N R5W 3PM

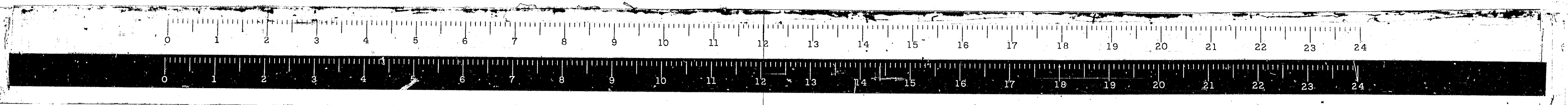
B.M.	ELEV.	STA.	DESCRIPTION
B.M.#9	648.06	877+78	Top of Conc. R.O.W. Monument on W. R.O.W. Line.
B.M.#10	649.26	889+75	Top of Conc. R.O.W. Monument on W. R.O.W. Line.



EARTH EXC. = 6,491 CU. YDS.
 EMBANKMENT = 26,836 CU. YDS.
 BORROW EXC. = 25,981 CU. YDS.
 TOP SOIL EXC. = 3,643 CU. YDS.
 TOP SOIL PLACE. = 27,850 30. YDS.

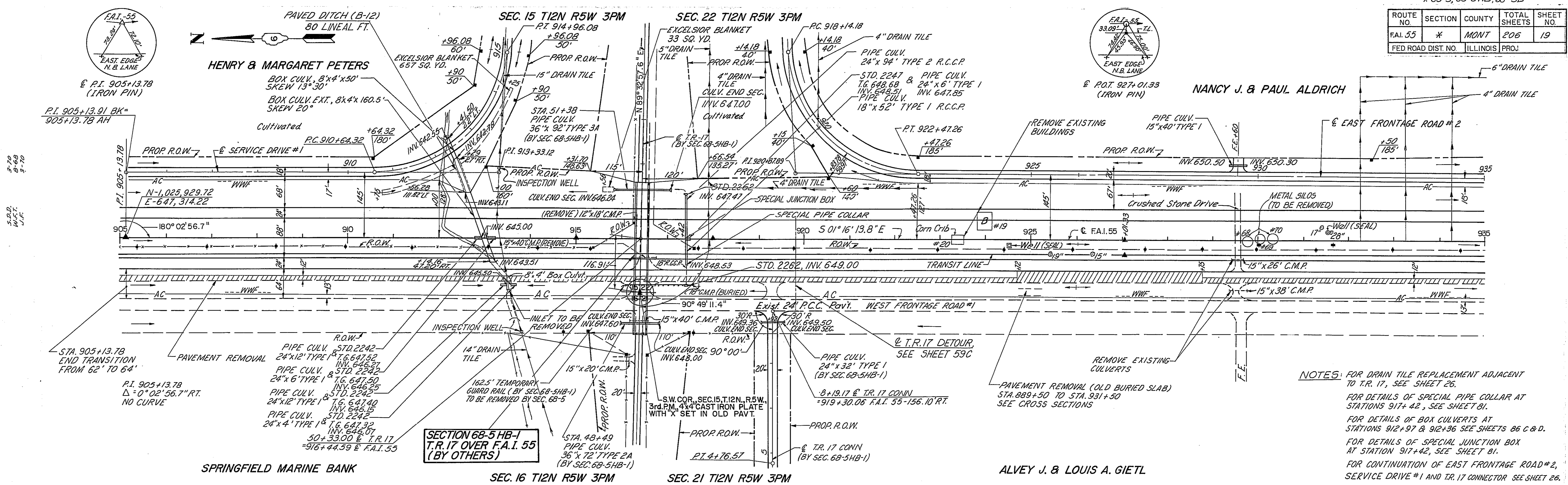
EXISTING ELEV.	PROPOSED ELEV.
647.5	650.15
6474	649.99
6461	649.83
6457	649.67
6459	649.59
6456	649.52
6450	649.47
6450	649.47
6450	649.51
6452	649.66
6452	649.77
6452	649.89
6451	650.14
6454	650.39
6454	650.64
6459	650.89
647.0	651.14
6473	651.39
6476	651.64
6475	651.89
6479	652.14
6483	652.39
6482	652.64
6501	652.89
6505	653.14
6510	653.39
6515	653.64
6514	653.89
6516	654.08
651B	654.15
6513	654.10
6501	653.93
6502	653.70
6495	653.47

STA. 875+00 TO STA. 905+00 FAI. 55



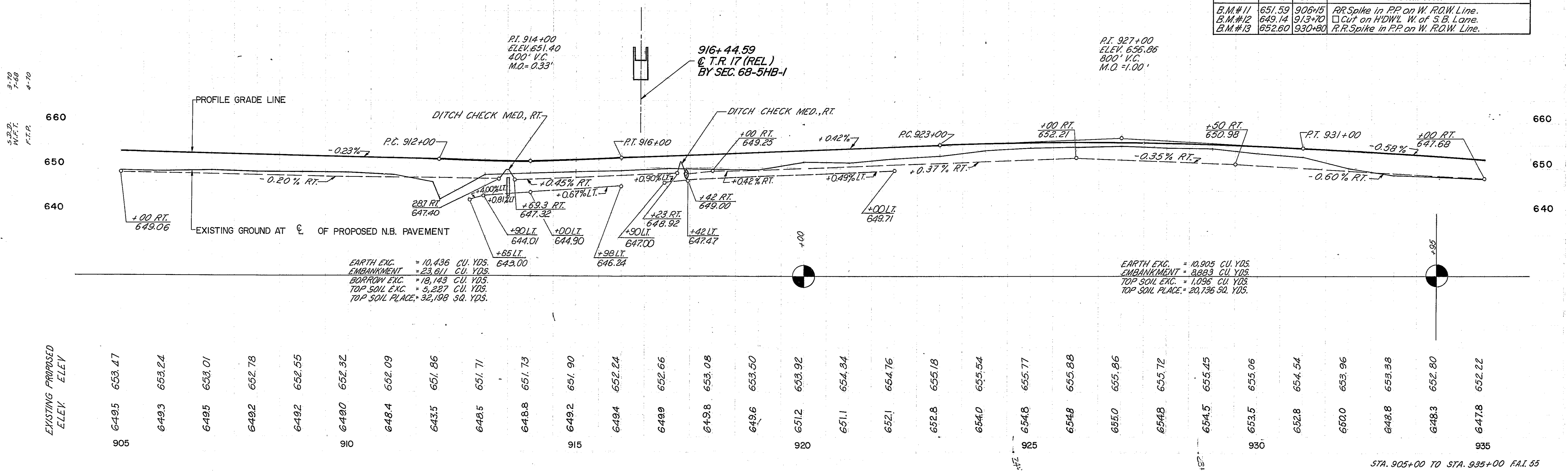
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT	206	19
FED ROAD DIST NO. ILLINOIS PROJ.				



NOTES: FOR DRAIN TILE REPLACEMENT ADJACENT TO T.R. 17, SEE SHEET 26.
 FOR DETAILS OF SPECIAL PIPE COLLAR AT STATIONS 912+97 & 912+96 SEE SHEETS 66 C & D.
 FOR DETAILS OF SPECIAL JUNCTION BOX AT STATION 917+42, SEE SHEET 81.
 FOR CONTINUATION OF EAST FRONTAGE ROAD #2, SERVICE DRIVE #1 AND T.R. 17 CONNECTOR, SEE SHEET 26.

B.M.	ELEV.	STA.	DESCRIPTION
B.M.#11	651.59	906+15	RR Spike in PP on W. R.O.W. Line.
B.M.#12	649.14	913+70	Cut on HDWL W. of S.B. Lane.
B.M.#13	652.60	930+80	RR Spike in PP on W. R.O.W. Line.

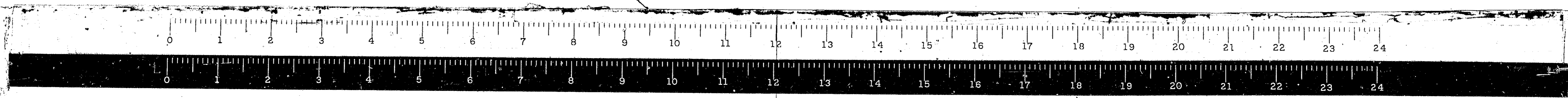


EARTH EXC. = 10,436 CU. YDS.
 EMBANKMENT = 23,611 CU. YDS.
 BORROW EXC. = 18,143 CU. YDS.
 TOP SOIL EXC. = 3,227 CU. YDS.
 TOP SOIL PLACE = 32,198 CU. YDS.

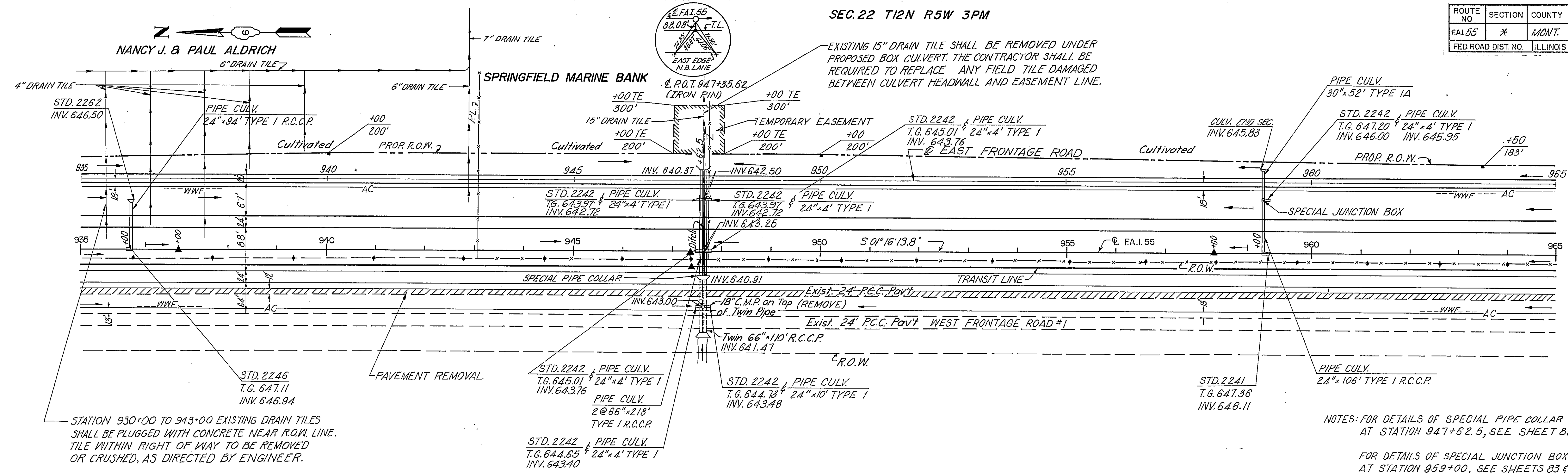
EARTH EXC. = 10,905 CU. YDS.
 EMBANKMENT = 8,893 CU. YDS.
 TOP SOIL EXC. = 1,096 CU. YDS.
 TOP SOIL PLACE = 20,736 CU. YDS.

EXISTING PROPOSED ELEV. ELEV.	EXISTING PROPOSED ELEV. ELEV.	EXISTING PROPOSED ELEV. ELEV.	EXISTING PROPOSED ELEV. ELEV.	EXISTING PROPOSED ELEV. ELEV.	
6485	653.47	6493	653.24	6495	653.01
6492	652.78	6492	652.55	6490	652.32
6484	652.09	6435	651.86	6485	651.71
6488	651.73	6492	652.24	6499	652.66
6498	653.08	6496	653.50	6512	653.92
6511	654.34	6521	654.76	6528	655.18
6540	655.54	6548	655.77	6548	655.88
6550	655.86	6548	655.72	6545	655.45
6535	655.06	6528	654.54	6535	655.06
6500	653.96	6488	653.38	6483	652.80
6478	652.22				

STA. 905+00 TO STA. 935+00 F.A.I. 55



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	20
FED. ROAD DIST. NO.			ILLINOIS	PROJ.

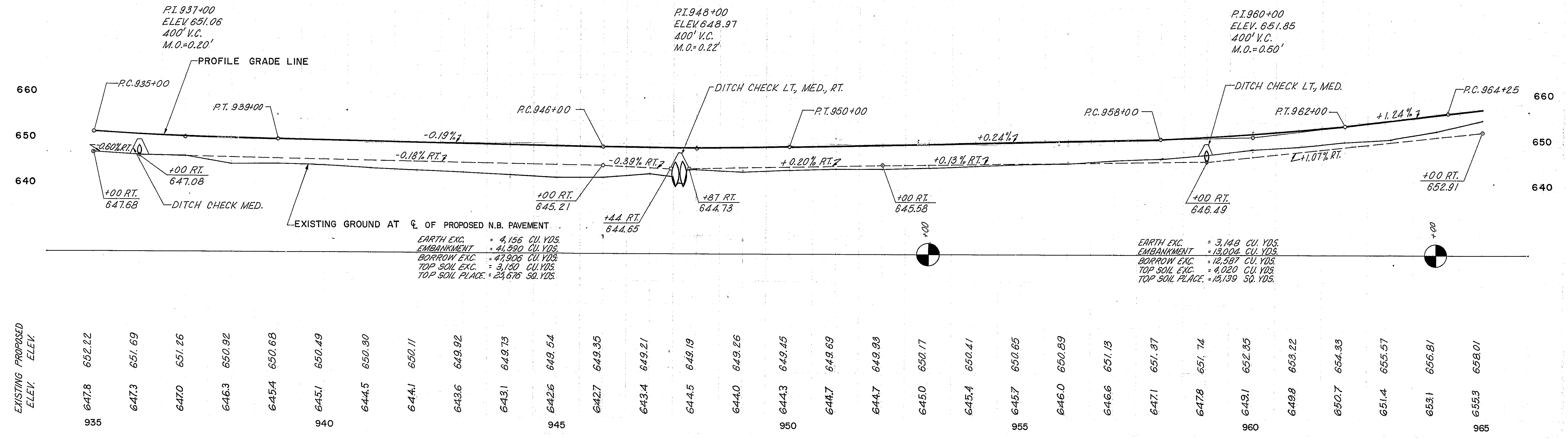


STATION 930+00 TO 943+00 EXISTING DRAIN TILES SHALL BE PLUGGED WITH CONCRETE NEAR R.O.W. LINE. TILE WITHIN RIGHT OF WAY TO BE REMOVED OR CRUSHED, AS DIRECTED BY ENGINEER.

NOTES: FOR DETAILS OF SPECIAL PIPE COLLAR AT STATION 947+62.5, SEE SHEET 81.
FOR DETAILS OF SPECIAL JUNCTION BOX AT STATION 959+00, SEE SHEETS 83 & 84.
FOR DETAILS OF HEADWALL AT STATION 947+62.5, SEE SHEET 82.

ALVEY J. & LOUIS A. GIETL
SEC. 21 T12N R5W 3PM

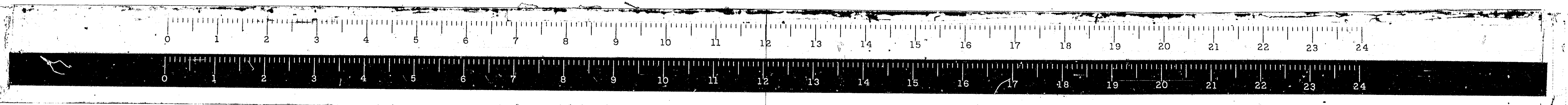
B.M.	ELEV.	STA.	DESCRIPTION
B.M. 14	648.04	947+60	□ Cut on HDWL West of South Bound Lane



EXISTING PROPOSED ELEV. ELEV.

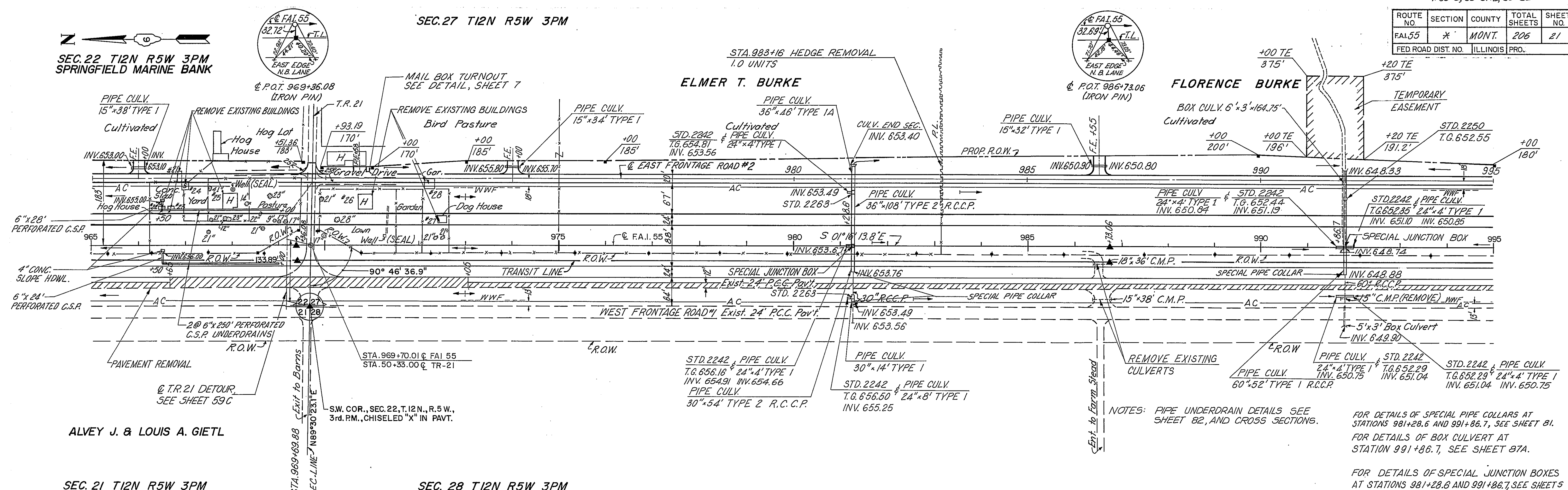
647.8	652.22	935	647.3	651.69		647.0	651.26		646.3	650.92		645.4	650.68		645.1	650.49		644.5	650.30		644.1	650.11		643.6	649.92		643.1	649.73		642.6	649.54		642.7	649.35		643.4	649.21		644.5	648.19		644.0	649.26		644.3	649.45		644.7	649.69		644.7	649.98		645.0	650.17		645.4	650.41		645.7	650.65		646.0	650.89		646.6	651.13		647.1	651.37		647.8	651.74		649.1	652.35		649.8	653.22		650.7	654.33		651.4	655.57		653.1	656.81		653.3	658.01	965
-------	--------	-----	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	--	-------	--------	-----

STA. 935+00 TO STA. 965+00 FAI.55



*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	21
FED. ROAD DIST. NO.		ILLINOIS		PRO.



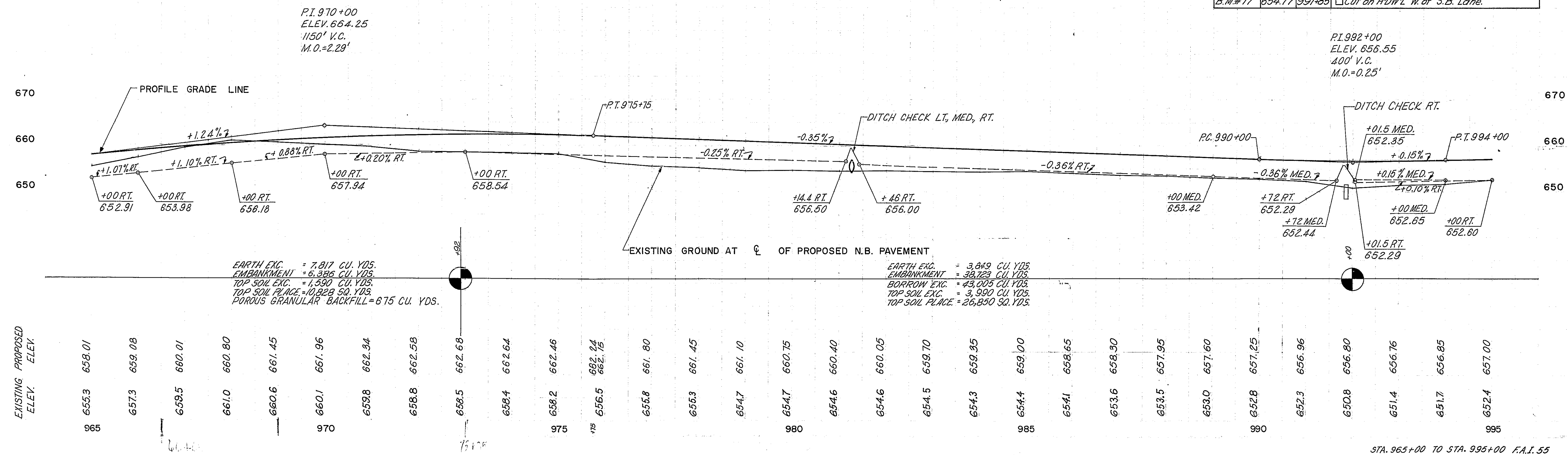
ALVEY J. & LOUIS A. GIETL

SEC. 21 T12N R5W 3PM

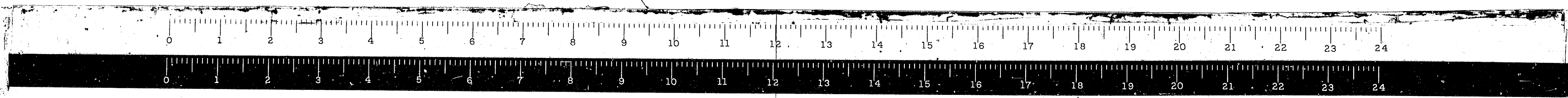
SEC. 28 T12N R5W 3PM

NOTES: PIPE UNDERDRAIN DETAILS SEE SHEET 82, AND CROSS SECTIONS.
 FOR DETAILS OF SPECIAL PIPE COLLARS AT STATIONS 981+28.6 AND 991+86.7, SEE SHEET 81.
 FOR DETAILS OF BOX CULVERT AT STATION 991+86.7, SEE SHEET 87A.
 FOR DETAILS OF SPECIAL JUNCTION BOXES AT STATIONS 981+28.6 AND 991+86.7, SEE SHEETS 83 & 84.

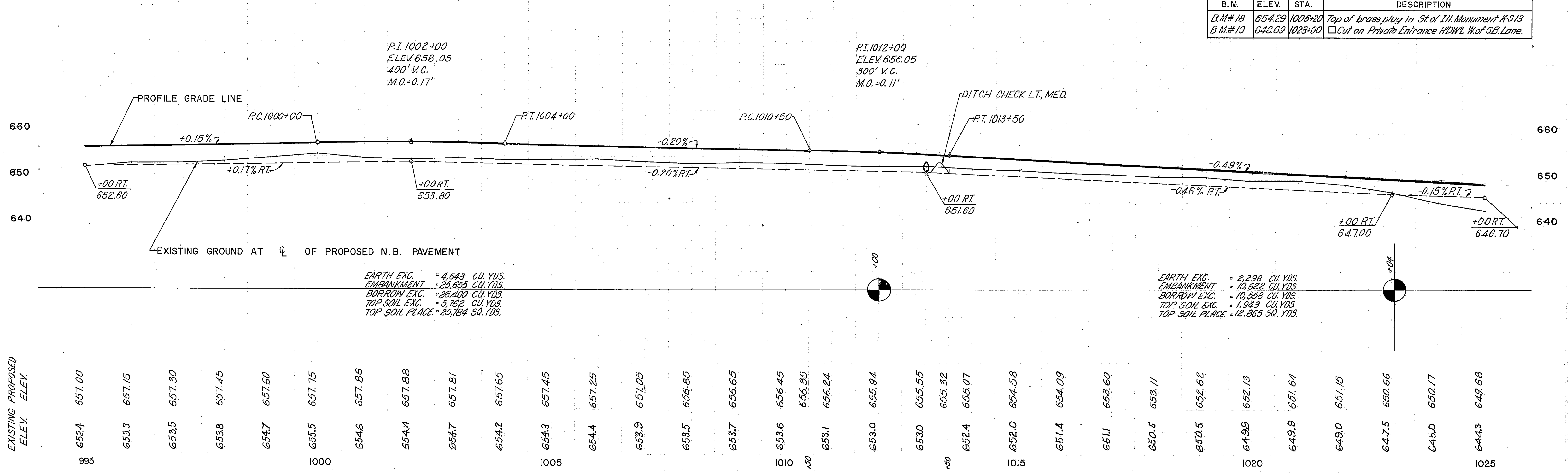
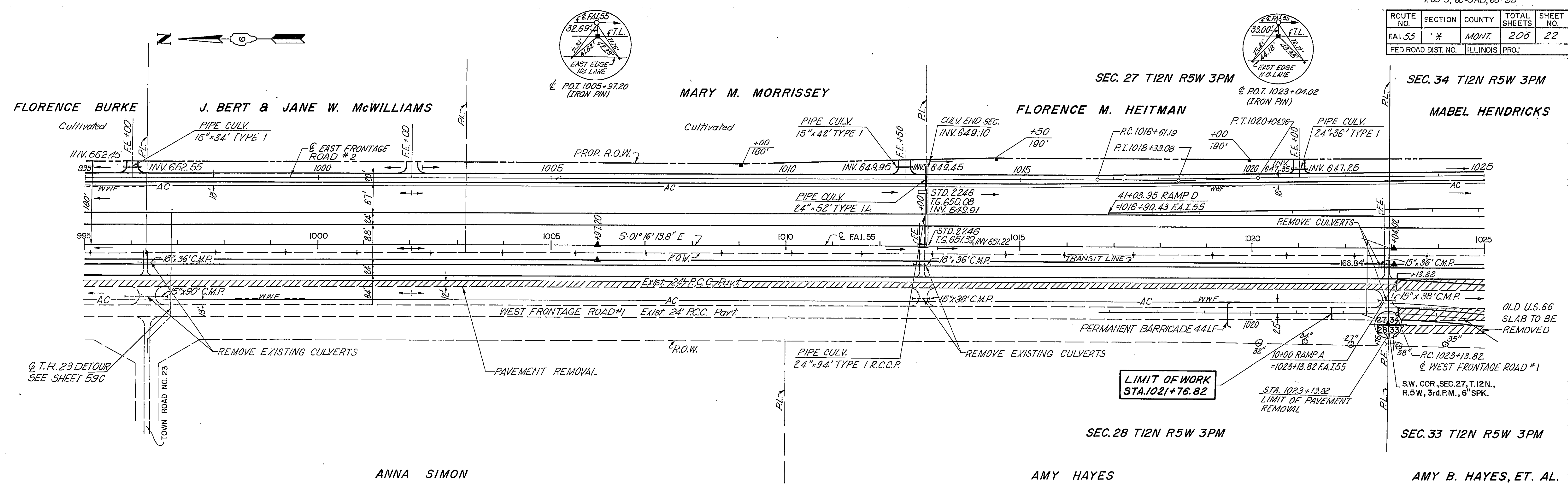
B.M.	ELEV.	STA.	DESCRIPTION
B.M.# 15	660.66	969+20	Top of brass plug in St of Ill. Monument W-S 14.
B.M.# 16	656.94	981+30	Cut on H'DWL W. of S.B. Lane.
B.M.# 17	654.77	991+85	Cut on H'DWL W. of S.B. Lane.



STA. 965+00 TO STA. 995+00 F.A.I. 55



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	X	MONT.	206	22
FED. ROAD DIST. NO.	ILLINOIS	PROJ.		

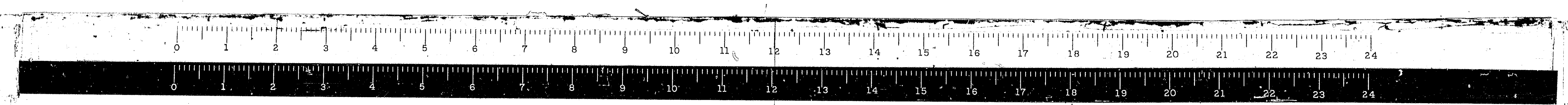


B.M.	ELEV.	STA.	DESCRIPTION
B.M.# 18	654.29	1006+20	Top of brass plug in St of Ill. Monument #513
B.M.# 19	648.69	1023+00	□ Cut on Private Entrance HDWL. W of SB Lane.

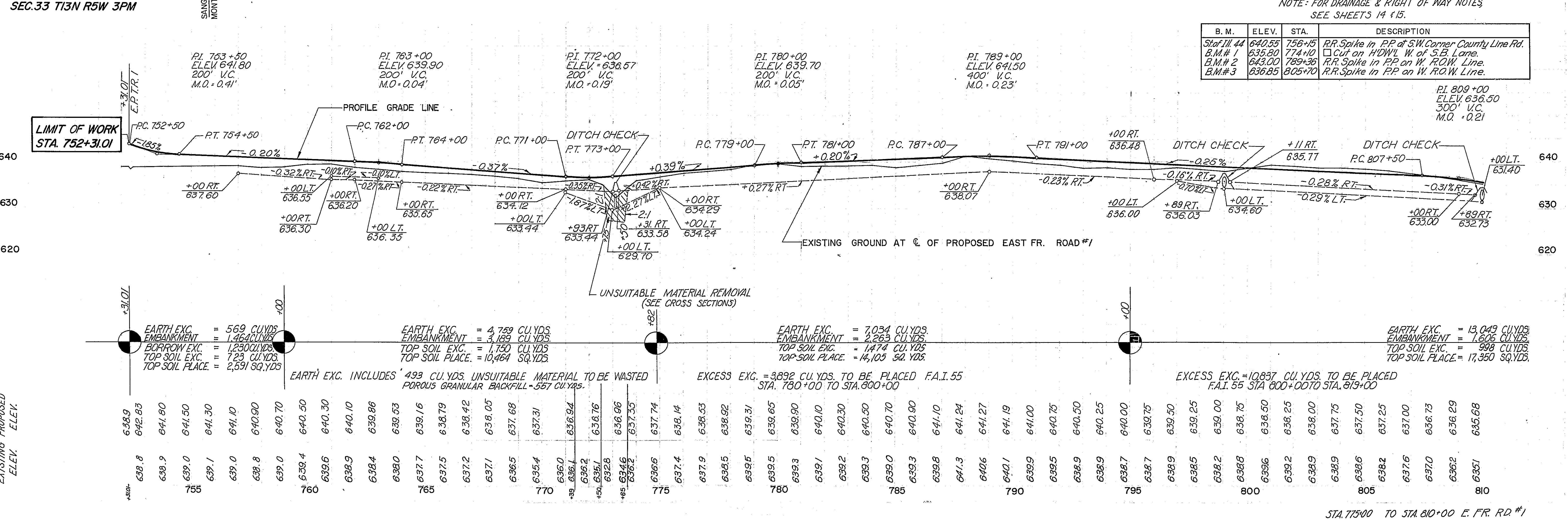
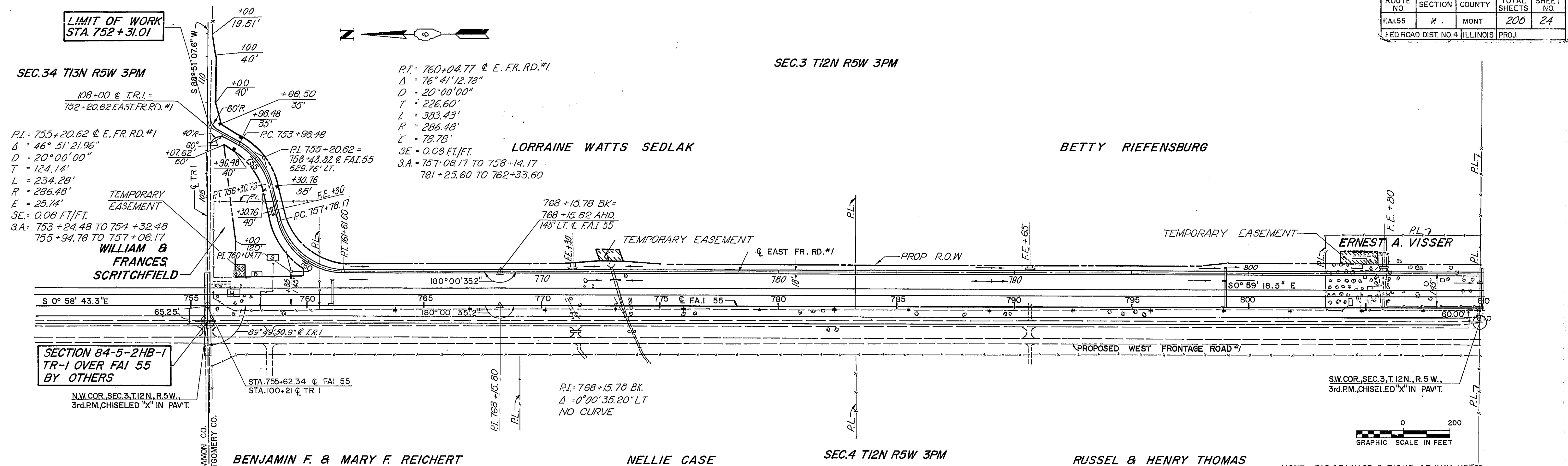
EARTH EXC. = 4,643 CU. YDS.
 EMBANKMENT = 25,655 CU. YDS.
 BORROW EXC. = 26,400 CU. YDS.
 TOP SOIL EXC. = 3,762 CU. YDS.
 TOP SOIL PLACE = 29,784 SQ. YDS.

EARTH EXC. = 2,299 CU. YDS.
 EMBANKMENT = 10,622 CU. YDS.
 BORROW EXC. = 10,558 CU. YDS.
 TOP SOIL EXC. = 1,943 CU. YDS.
 TOP SOIL PLACE = 12,865 SQ. YDS.

STA. 995+00 TO STA. 1025+00 FAI. 55

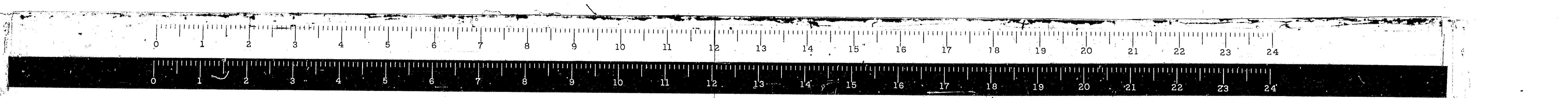


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	#	MONT	206	24
FED ROAD DIST. NO. 4 ILLINOIS PROJ.				



B. M.	ELEV.	STA.	DESCRIPTION
Sta. 771.44	640.55	756+15	RR Spike in P.P. of SW Corner County Line Rd.
B.M.# 1	635.90	774+10	Cut on HDWL W. of S.B. Lane.
B.M.# 2	643.00	789+30	RR Spike in P.P. on W. ROW Line.
B.M.# 3	636.85	805+70	RR Spike in P.P. on W. ROW Line.

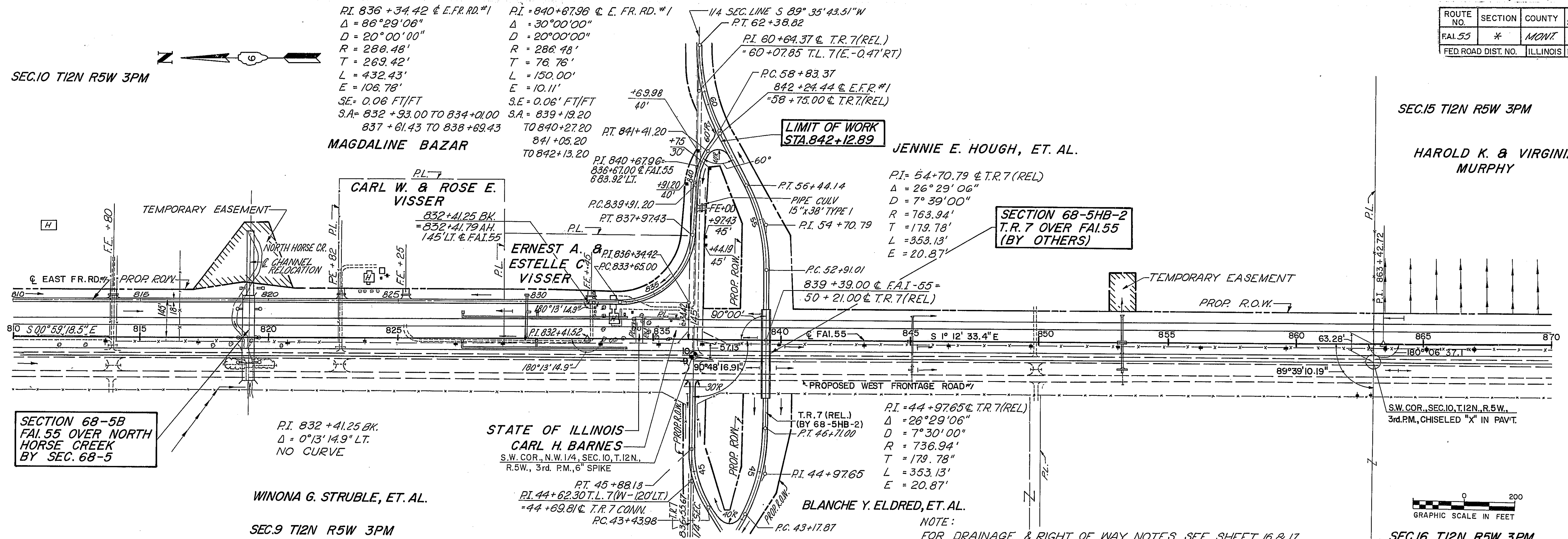
STA.	ELEV.	EXCISEMENTS	EMBANKMENTS	TOP SOIL EXC.	TOP SOIL PLACE.
755	638.8	569 CU.YDS	1,464 CU.YDS	1,250 CU.YDS	725 CU.YDS
760	639.6	493 CU.YDS UNSUITABLE MATERIAL TO BE WASTED POROUS GRANULAR BACKFILL-557 CU.YDS.	4,759 CU.YDS	1,750 CU.YDS	10,464 SQ.YDS
775	637.4	EXCESS EXC. = 3,892 CU.YDS TO BE PLACED F.A.I. 55 STA. 760+00 TO STA. 800+00	7,034 CU.YDS	1,474 CU.YDS	14,105 SQ.YDS
780	639.3	EXCESS EXC. = 10,837 CU.YDS TO BE PLACED F.A.I. 55 STA. 800+00 TO STA. 819+00	13,049 CU.YDS	998 CU.YDS	17,350 SQ.YDS
795	638.7				
800	639.6				
805	637.5				
810	635.7				



STA. 775+00 TO STA. 800+00 E. FR. RD. #1

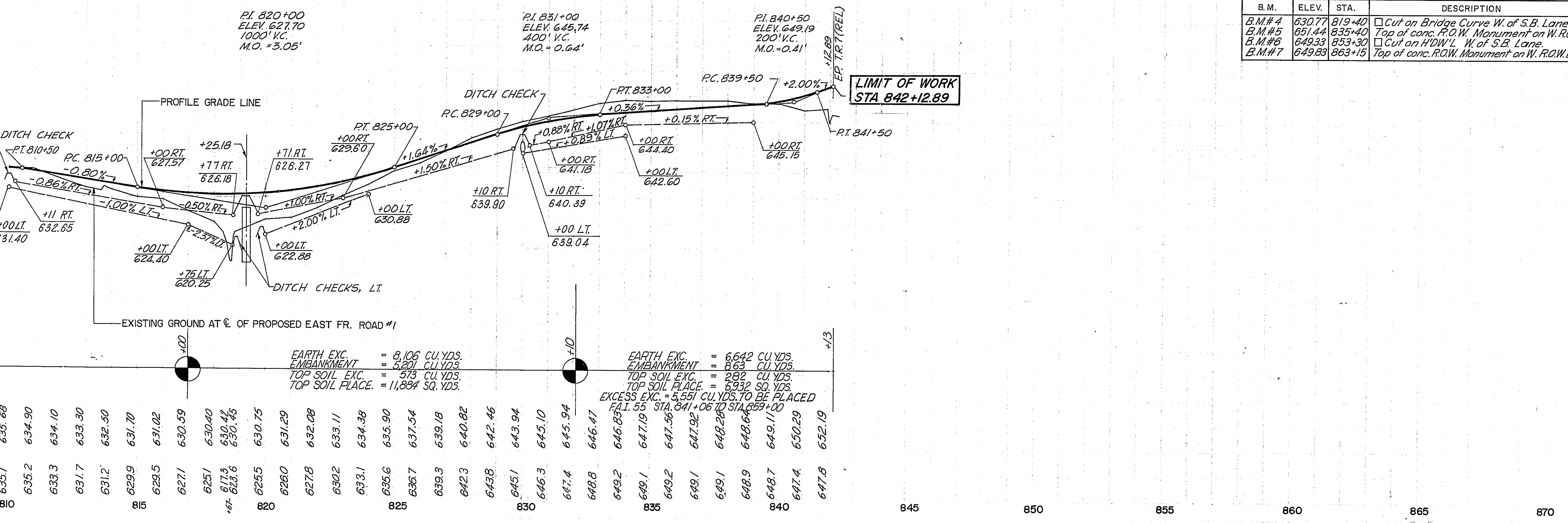
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONI	206	25
FED. ROAD DIST. NO.		ILLINOIS PROJ.		



SECTION 68-5B
FAI.55 OVER NORTH
HORSE CREEK
BY SEC. 68-5

SECTION 68-5HB-2
T.R.7 OVER FAI.55
(BY OTHERS)



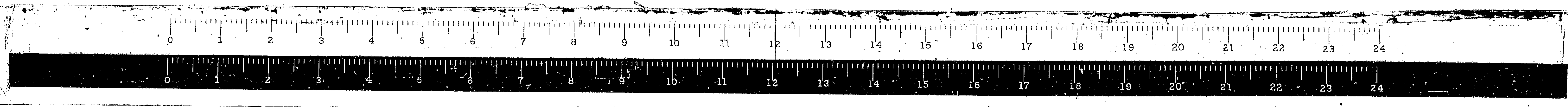
B.M.	ELEV.	STA.	DESCRIPTION
B.M.#4	630.77	819+40	Cut on Bridge Curve W. of S.B. Lane
B.M.#5	651.44	835+40	Top of conc. F.O.W. Monument on W. R.O.W. Line
B.M.#6	649.33	853+30	Cut on H'D.W.L. W. of S.B. Lane
B.M.#7	649.83	863+15	Top of conc. F.O.W. Monument on W. R.O.W. Line

EXISTING PROPOSED ELEV. ELEV.	810	815	820	825	830	835	840	845	850	855	860	865	870
	631.40	632.85	633.30	633.75	634.38	635.90	637.54	639.18	640.82	642.46	644.10	645.74	647.38
	631.40	632.85	633.30	633.75	634.38	635.90	637.54	639.18	640.82	642.46	644.10	645.74	647.38

EARTH EXC. = 8,106 CU. YDS.
EMBANKMENT = 2,201 CU. YDS.
TOP SOIL EXC. = 573 CU. YDS.
TOP SOIL PLACE. = 11,884 SQ. YDS.

EARTH EXC. = 6,642 CU. YDS.
EMBANKMENT = 863 CU. YDS.
TOP SOIL EXC. = 282 CU. YDS.
TOP SOIL PLACE. = 5,932 SQ. YDS.

EXCESS EXC. = 5,551 CU. YDS. TO BE PLACED
FAI.55 STA. 841+06 TO STA. 859+00



STA. 810+00 TO STA. 870+00 E. FR. RD. #1

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	#2	MONT.	206	26
FED. ROAD DIST. NO.	ILLINOIS		PROJ.	

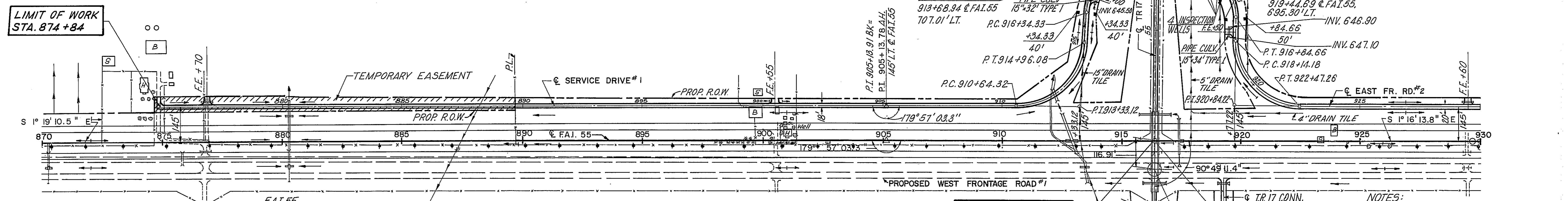
PI. 913+33.12 @ SER. DR.#1
 $\Delta = 86^\circ 21' 10.49''$
 $D = 20^\circ 00' 00''$
 $T = 268.80'$
 $L = 431.76'$
 $R = 286.48'$
 $E = 106.37'$
 $S.E. = 0.06 \text{ FT/FT}$
 $S.A. = 909+92.32 \text{ TO } 911+00.32$
 $914+58.08 \text{ TO } 915+66.08$

PI. 917+90.44 @ SER. DR.#1
 $\Delta = 57^\circ 10' 21.90''$
 $D = 20^\circ 00' 00''$
 $T = 156.11'$
 $L = 285.86'$
 $R = 286.48'$
 $E = 59.77'$
 $S.E. = 0.06 \text{ FT/FT}$
 $S.A. = 915+66.08 \text{ TO } 916+74.32$
 $918+83.18 \text{ TO } 919+91.18$

PI. 915+57.85 @ E.F.R.D.#2
 $\Delta = 56^\circ 47' 48.46''$
 $D = 20^\circ 00' 00''$
 $T = 151.67'$
 $L = 278.98'$
 $R = 286.48'$
 $E = 151.68'$
 $S.E. = 0.06 \text{ FT/FT}$
 $S.A. = 913+33.67 \text{ TO } 914+41.87$
 $916+40.65 \text{ TO } 917+49.18$

PI. 920+84.22 @ E.F.R.D.#2
 $\Delta = 86^\circ 36' 59.87''$
 $D = 20^\circ 00' 00''$
 $T = 270.04'$
 $L = 433.08'$
 $R = 286.48'$
 $E = 107.22'$
 $S.E. = 0.06 \text{ FT/FT}$
 $S.A. = 917+49.18 \text{ TO } 918+57.18$
 $922+11.26 \text{ TO } 923+19.26$

HAROLD K. & VIRGINIA MURPHY HENRY & MARGARET PETERS

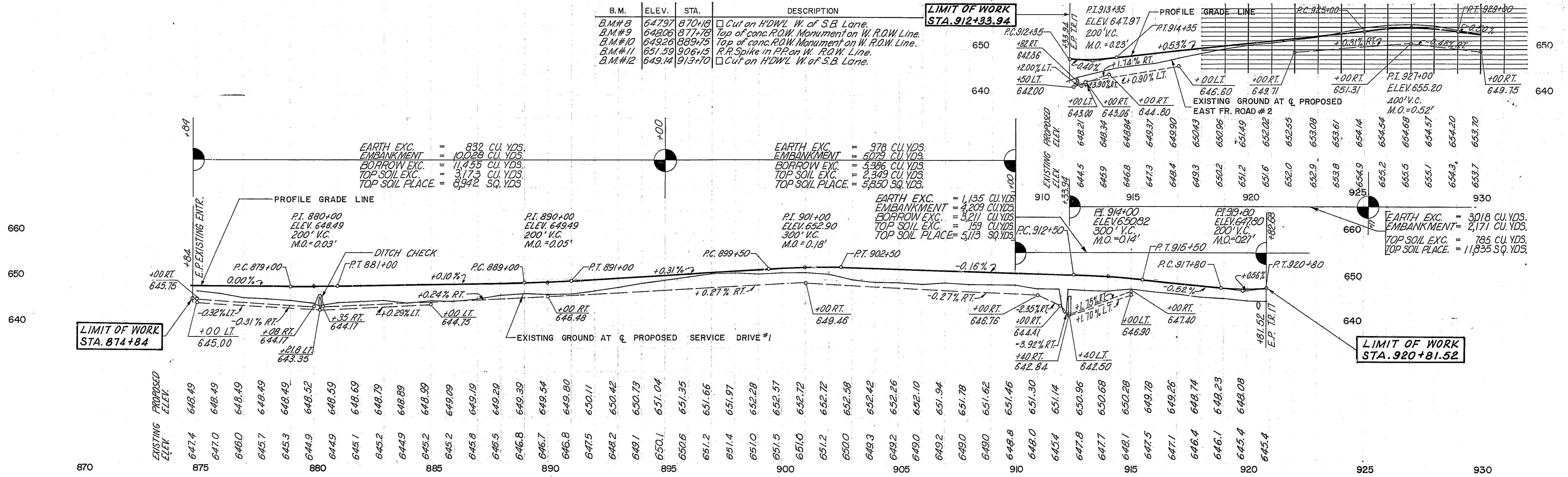


STATION 913+25 EXISTING DRAIN TILES SHALL BE REMOVED AND REPLACED WITH STORM SEWERS, TYPE II OF EQUAL DIAMETERS. LENGTH TO BE REPLACED SHALL EXTEND FROM THE LT. PROPOSED R.O.W. TO THE EDGE OF EXIST. 3'x4' BOX CULVERT IN MAINLINE AREA AND BETWEEN PROPOSED R.O.W. IN FRONTAGE ROADS AREAS. 670 FT. OF 15" STORM SEWERS, TYPE II IS REQUIRED. STATION 917+50 TO 922+50 EXISTING DRAIN TILES SHALL BE PLUGGED WITH CONCRETE NEAR MAINLINE R.O.W. LINE, TILE WITHIN MAINLINE AREA TO BE REMOVED OR CRUSHED, AS DIRECTED BY ENGINEER.

EAST FRONTAGE ROAD #2 STATIONS 913+90 AND 915+55 EXISTING DRAIN TILES SHALL BE REMOVED AND REPLACED WITH STORM SEWERS, TYPE II OF EQUAL DIAMETERS. LENGTH TO BE REPLACED SHALL EXTEND BETWEEN PROPOSED R.O.W. LINES, APPROX. 110 FT. OF 6" STORM SEWERS, TYPE II AND 175 FT. OF 4" STORM SEWERS, TYPE II ARE REQUIRED. *5" DRAIN TILE TO BE REPLACED WITH 6" STORM SEWERS.

NOTES:
 FOR DETAILS OF INSPECTION WELL, SEE SHEET 01.
 FOR DRAINAGE & RIGHT OF WAY NOTES, SEE SHEETS 18 & 19.

B.M.	ELEV.	STA.	DESCRIPTION
B.M.#8	647.97	870+18	Cut on HDWL W. of S.B. Lane.
B.M.#9	648.08	877+78	Top of conc. R.O.W. Monument on W. R.O.W. Line.
B.M.#10	649.26	889+75	Top of conc. R.O.W. Monument on W. R.O.W. Line.
B.M.#11	651.39	906+15	R.P. Spike in R.P. on W. R.O.W. Line.
B.M.#12	649.14	913+10	Cut on HDWL W. of S.B. Lane.

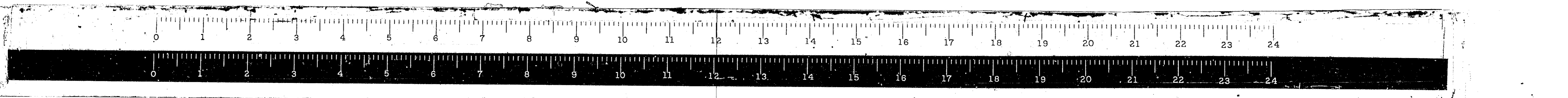


EARTH EXC. = 832 CU. YDS.
EMBANKMENT = 10,028 CU. YDS.
BORROW EXC. = 11,455 CU. YDS.
TOP SOIL EXC. = 3,173 CU. YDS.
TOP SOIL PLACE = 8,942 SQ. YDS.

EARTH EXC. = 978 CU. YDS.
EMBANKMENT = 5,073 CU. YDS.
BORROW EXC. = 5,386 CU. YDS.
TOP SOIL EXC. = 2,349 CU. YDS.
TOP SOIL PLACE = 5,850 SQ. YDS.

EARTH EXC. = 1,135 CU. YDS.
EMBANKMENT = 4,209 CU. YDS.
BORROW EXC. = 3,211 CU. YDS.
TOP SOIL EXC. = 159 CU. YDS.
TOP SOIL PLACE = 5,113 SQ. YDS.

EARTH EXC. = 3,018 CU. YDS.
EMBANKMENT = 2,171 CU. YDS.
TOP SOIL EXC. = 785 CU. YDS.
TOP SOIL PLACE = 11,835 SQ. YDS.



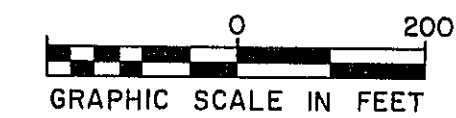
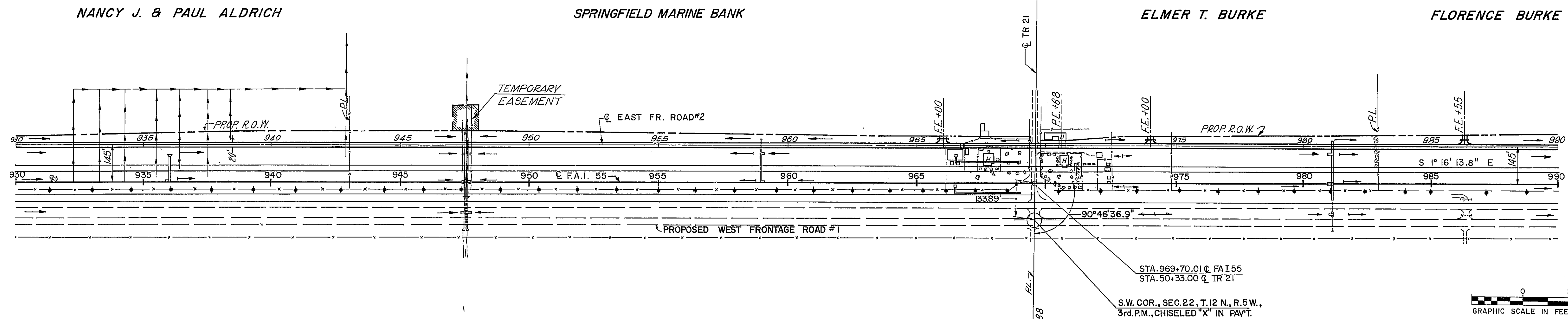
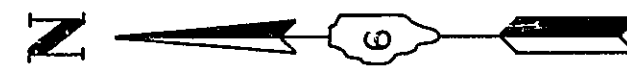
STA. 870+00 TO STA. 930+00 SERV. DR.#1
E.F.R.D.#2

SEC. 22 T12N R5W 3PM

SEC. 27 T12N R5W 3PM

*68-5, 68-54B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	27
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

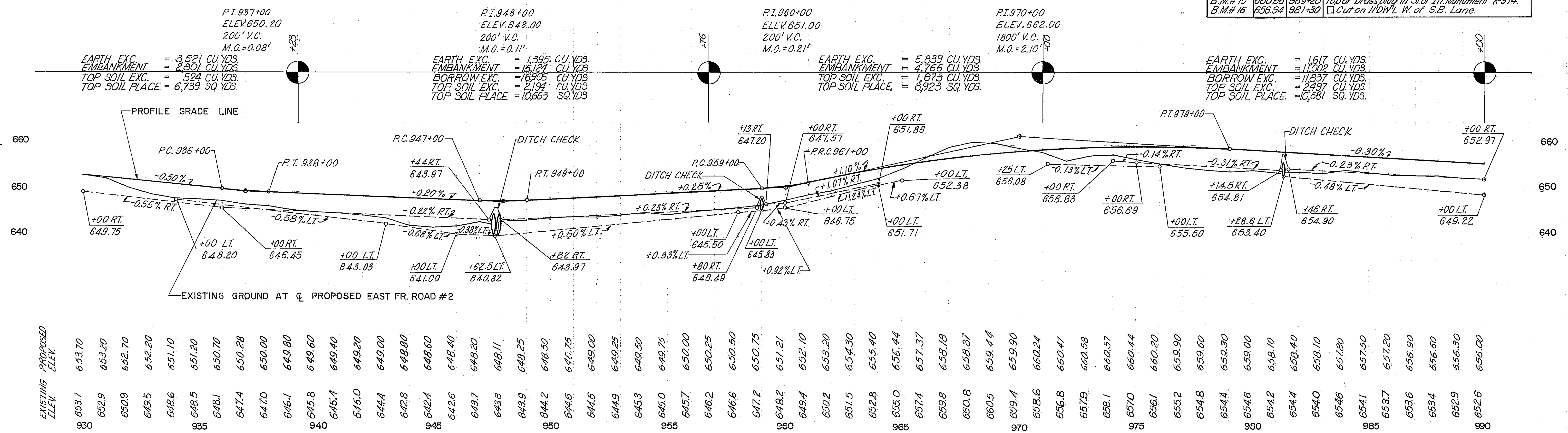


ALVEY J. & LOUIS A. GIETL
SEC. 21 T12N R5W 3PM

SEC. 28 T12N R5W 3PM

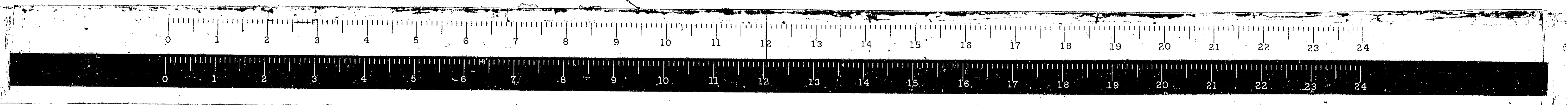
NOTE: FOR DRAINAGE & RIGHT OF WAY NOTES, SEE SHEETS 20 & 21.

B.M.	ELEV.	STA.	DESCRIPTION
B.M. #13	652.60	930+80	R.R. Spike in PP on W. R.O.W. Line.
B.M. #14	648.04	947+60	Cut on HDWL W. of S.B. Lane.
B.M. #15	650.66	969+20	Top of brass plug in Sta. III Monument K-914.
B.M. #16	656.34	981+30	Cut on HDWL W. of S.B. Lane.

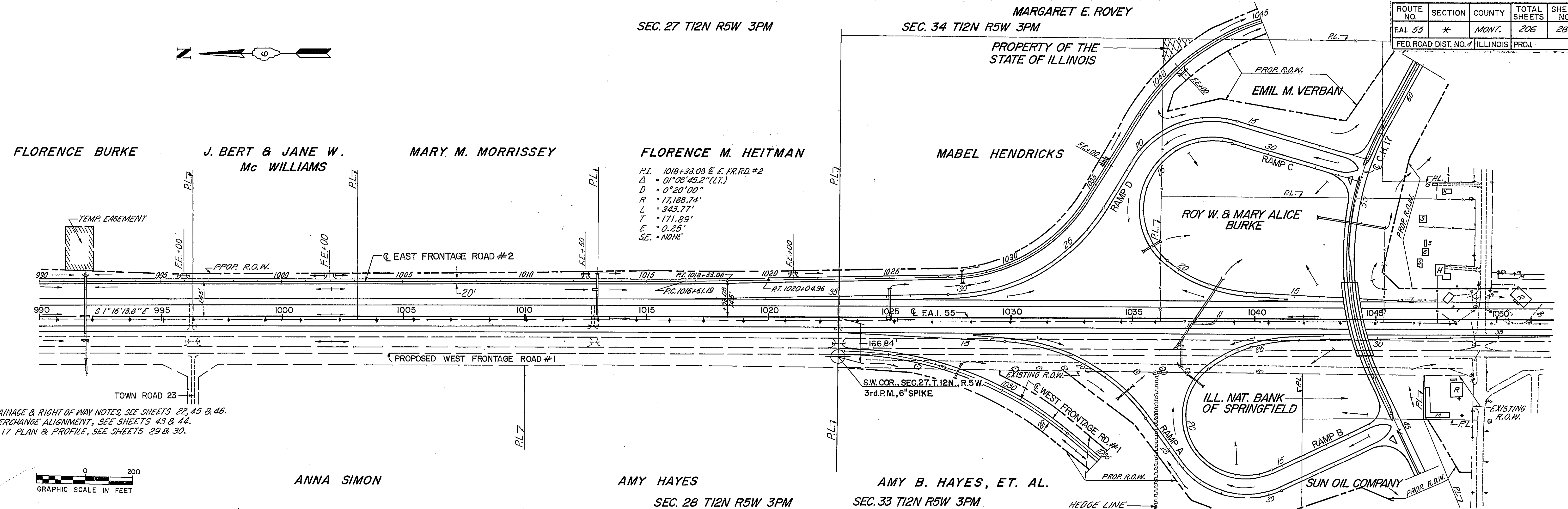


EXISTING PROPOSED ELEV.	EXISTING PROPOSED ELEV.
653.70	653.70
653.20	653.20
652.70	652.70
652.20	652.20
651.10	651.10
651.20	651.20
650.70	650.70
650.28	650.28
650.00	650.00
649.80	649.80
649.60	649.60
649.40	649.40
649.20	649.20
649.00	649.00
648.80	648.80
648.60	648.60
648.40	648.40
648.20	648.20
648.11	648.11
648.25	648.25
648.50	648.50
648.75	648.75
649.00	649.00
649.25	649.25
649.50	649.50
649.75	649.75
650.00	650.00
650.25	650.25
650.50	650.50
650.75	650.75
651.21	651.21
652.10	652.10
653.20	653.20
654.30	654.30
655.40	655.40
656.44	656.44
657.37	657.37
658.18	658.18
658.87	658.87
659.44	659.44
659.90	659.90
660.24	660.24
660.47	660.47
660.58	660.58
660.57	660.57
660.44	660.44
660.20	660.20
659.90	659.90
659.60	659.60
659.30	659.30
659.00	659.00
658.10	658.10
658.40	658.40
658.10	658.10
657.80	657.80
657.50	657.50
657.20	657.20
656.90	656.90
656.60	656.60
656.30	656.30
656.00	656.00
655.20	655.20
655.00	655.00

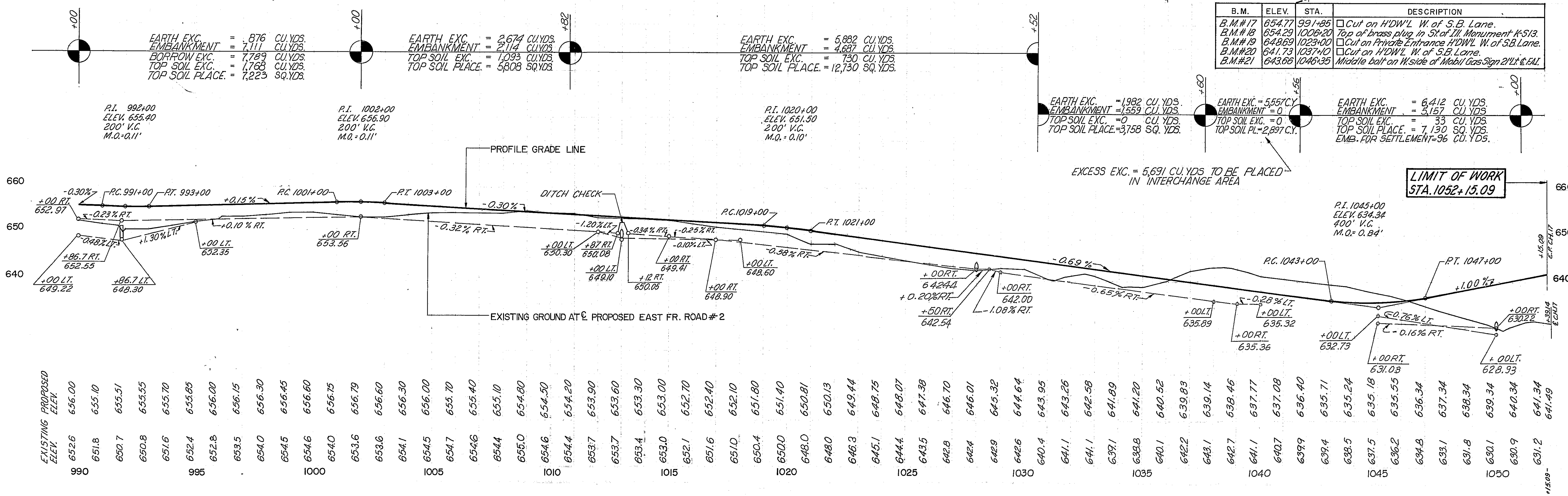
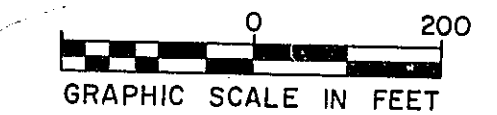
STA. 930+00 TO STA. 990+00 E. FR. RD. #2



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	28
FED. ROAD DIST. NO. # ILLINOIS PROJ.				

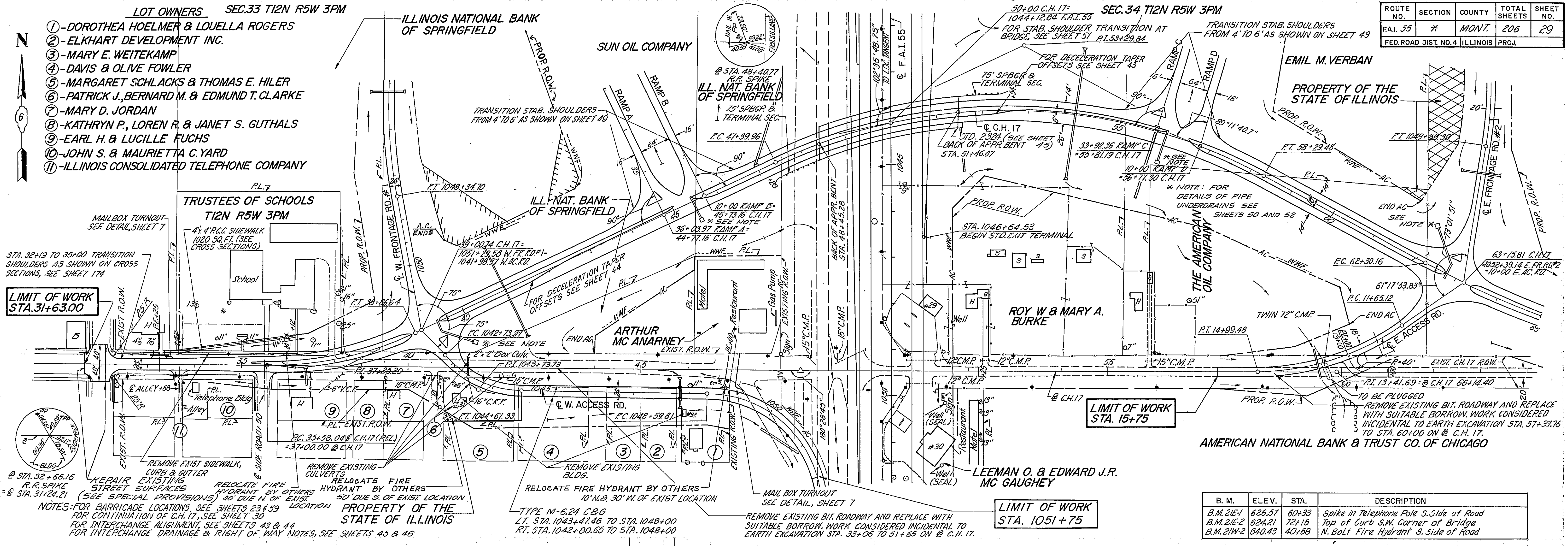


NOTES:
 FOR DRAINAGE & RIGHT OF WAY NOTES, SEE SHEETS 22, 45 & 46.
 FOR INTERCHANGE ALIGNMENT, SEE SHEETS 43 & 44.
 FOR C.H. 17 PLAN & PROFILE, SEE SHEETS 29 & 30.

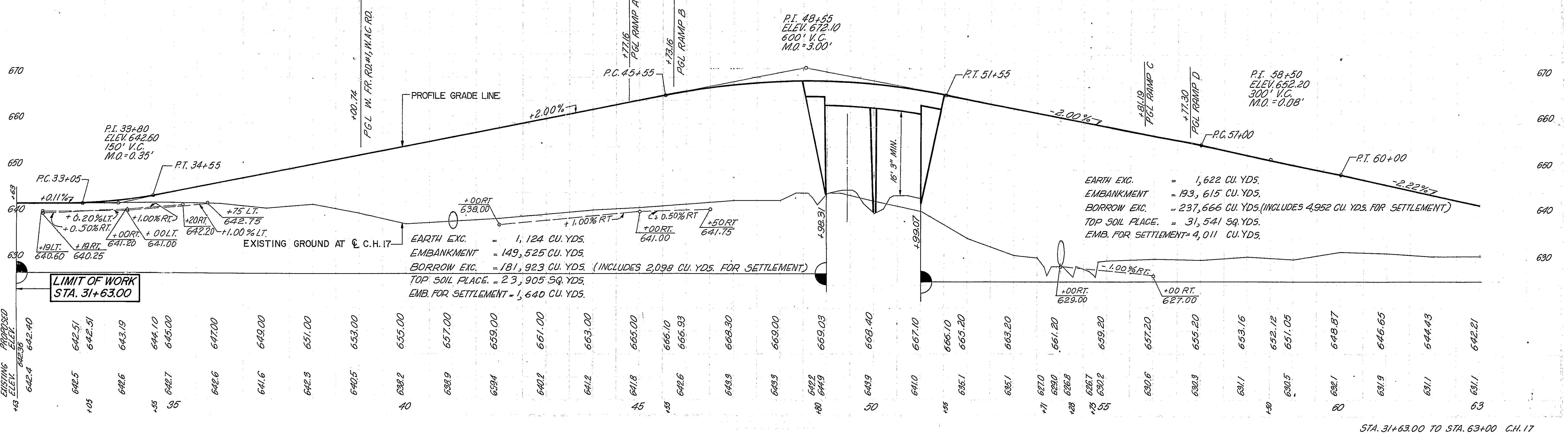


EXISTING PROPOSED ELEV.	990	995	1000	1005	1010	1015	1020	1025	1030	1035	1040	1045	1050
652.6	651.8	650.7	650.8	651.6	652.4	653.7	653.4	653.0	653.0	652.1	651.6	651.0	650.4
655.0	655.10	655.51	655.55	655.70	655.85	656.00	656.15	656.30	656.45	656.60	656.75	656.90	657.05
658.00	658.15	658.30	658.45	658.60	658.75	658.90	659.05	659.20	659.35	659.50	659.65	659.80	659.95
662.00	662.15	662.30	662.45	662.60	662.75	662.90	663.05	663.20	663.35	663.50	663.65	663.80	663.95
667.00	667.15	667.30	667.45	667.60	667.75	667.90	668.05	668.20	668.35	668.50	668.65	668.80	668.95
672.00	672.15	672.30	672.45	672.60	672.75	672.90	673.05	673.20	673.35	673.50	673.65	673.80	673.95
678.00	678.15	678.30	678.45	678.60	678.75	678.90	679.05	679.20	679.35	679.50	679.65	679.80	679.95
684.00	684.15	684.30	684.45	684.60	684.75	684.90	685.05	685.20	685.35	685.50	685.65	685.80	685.95
690.00	690.15	690.30	690.45	690.60	690.75	690.90	691.05	691.20	691.35	691.50	691.65	691.80	691.95
696.00	696.15	696.30	696.45	696.60	696.75	696.90	697.05	697.20	697.35	697.50	697.65	697.80	697.95
702.00	702.15	702.30	702.45	702.60	702.75	702.90	703.05	703.20	703.35	703.50	703.65	703.80	703.95
708.00	708.15	708.30	708.45	708.60	708.75	708.90	709.05	709.20	709.35	709.50	709.65	709.80	709.95
714.00	714.15	714.30	714.45	714.60	714.75	714.90	715.05	715.20	715.35	715.50	715.65	715.80	715.95
720.00	720.15	720.30	720.45	720.60	720.75	720.90	721.05	721.20	721.35	721.50	721.65	721.80	721.95
726.00	726.15	726.30	726.45	726.60	726.75	726.90	727.05	727.20	727.35	727.50	727.65	727.80	727.95
732.00	732.15	732.30	732.45	732.60	732.75	732.90	733.05	733.20	733.35	733.50	733.65	733.80	733.95
738.00	738.15	738.30	738.45	738.60	738.75	738.90	739.05	739.20	739.35	739.50	739.65	739.80	739.95
744.00	744.15	744.30	744.45	744.60	744.75	744.90	745.05	745.20	745.35	745.50	745.65	745.80	745.95
750.00	750.15	750.30	750.45	750.60	750.75	750.90	751.05	751.20	751.35	751.50	751.65	751.80	751.95
756.00	756.15	756.30	756.45	756.60	756.75	756.90	757.05	757.20	757.35	757.50	757.65	757.80	757.95
762.00	762.15	762.30	762.45	762.60	762.75	762.90	763.05	763.20	763.35	763.50	763.65	763.80	763.95
768.00	768.15	768.30	768.45	768.60	768.75	768.90	769.05	769.20	769.35	769.50	769.65	769.80	769.95
774.00	774.15	774.30	774.45	774.60	774.75	774.90	775.05	775.20	775.35	775.50	775.65	775.80	775.95
780.00	780.15	780.30	780.45	780.60	780.75	780.90	781.05	781.20	781.35	781.50	781.65	781.80	781.95
786.00	786.15	786.30	786.45	786.60	786.75	786.90	787.05	787.20	787.35	787.50	787.65	787.80	787.95
792.00	792.15	792.30	792.45	792.60	792.75	792.90	793.05	793.20	793.35	793.50	793.65	793.80	793.95
798.00	798.15	798.30	798.45	798.60	798.75	798.90	799.05	799.20	799.35	799.50	799.65	799.80	799.95
804.00	804.15	804.30	804.45	804.60	804.75	804.90	805.05	805.20	805.35	805.50	805.65	805.80	805.95
810.00	810.15	810.30	810.45	810.60	810.75	810.90	811.05	811.20	811.35	811.50	811.65	811.80	811.95
816.00	816.15	816.30	816.45	816.60	816.75	816.90	817.05	817.20	817.35	817.50	817.65	817.80	817.95
822.00	822.15	822.30	822.45	822.60	822.75	822.90	823.05	823.20	823.35	823.50	823.65	823.80	823.95
828.00	828.15	828.30	828.45	828.60	828.75	828.90	829.05	829.20	829.35	829.50	829.65	829.80	829.95
834.00	834.15	834.30	834.45	834.60	834.75	834.90	835.05	835.20	835.35	835.50	835.65	835.80	835.95
840.00	840.15	840.30	840.45	840.60	840.75	840.90	841.05	841.20	841.35	841.50	841.65	841.80	841.95
846.00	846.15	846.30	846.45	846.60	846.75	846.90	847.05	847.20	847.35	847.50	847.65	847.80	847.95
852.00	852.15	852.30	852.45	852.60	852.75	852.90	853.05	853.20	853.35	853.50	853.65	853.80	853.95
858.00	858.15	858.30	858.45	858.60	858.75	858.90	859.05	859.20	859.35	859.50	859.65	859.80	859.95
864.00	864.15	864.30	864.45	864.60	864.75	864.90	865.05	865.20	865.35	865.50	865.65	865.80	865.95
870.00	870.15	870.30	870.45	870.60	870.75	870.90	871.05	871.20	871.35	871.50	871.65	871.80	871.95
876.00	876.15	876.30	876.45	876.60	876.75	876.90	877.05	877.20	877.35	877.50	877.65	877.80	877.95
882.00	882.15	882.30	882.45	882.60	882.75	882.90	883.05	883.20	883.35	883.50	883.65	883.80	883.95
888.00	888.15	888.30	888.45	888.60	888.75	888.90	889.05	889.20	889.35	889.50	889.65	889.80	889.95
894.00	894.15	894.30	894.45	894.60	894.75	894.90	895.05	895.20	895.35	895.50	895.65	895.80	895.95
900.00	900.15	900.30	900.45	900.60	900.75	900.90	901.05	901.20	901.35	901.50	901.65	901.80	901.95
906.00	906.15	906.30	906.45	906.60	906.75	906.90	907.05	907.20	907.35	907.50	907.65	907.80	907.95
912.00	912.15	912.30	912.45	912.60	912.75	912.90	913.05	913.20	913.35	913.50	913.65	913.80	913.95
918.00	918.15	918.30	918.45	918.60	918.75	918.90	919.05	919.20	919.35	919.50	919.65	919.80	919.95
924.00	924.15	924.30	924.45	924.60	924.75	924.90	925.05	925.20	925.35	925.50	925.65	925.80	925.95
930.00	930.15	930.30	930.45	930.60	930.75	930.90	931.05	931.20	931.35	931.50	931.65	931.80	931.95
936.00	936.15	936.30	936.45	936.60	936.75	936.90	937.05	937.20	937.35	937.50	937.65	937.80	937.95
942.00	942.15	942.30	942.45	942.60	942.75	942.90	943.05	943.20	943.35	943.50	943.65	943.80	943.95
948.00	948.15	948.30	948.45	948.60	948.75	948.90	949.05	949.20	949.35	949.50	949.65	949.80	949.95
954.00	954.15	954.30	954.45	954.60	954.75	954.90	955.05	955.20	955.35	955.50	955.65	955.80	955.95
960.00	960.15	960.30	960.45	960.60	960.75	960.90	961.05	961.20	961.35	961.50	961.65	961.80	961.95
966.00	966.15	966.30	966.45	966.60	966.75	966.90	967.05	967.20	967.35	967.50	967.65	967.80	967.95
972.00	972.15	972.30	972.45	972.60	972.75	972.90	973.05	973.20	973.35	973.50	973.65	973.80	973.95
978.00	978.15	978.30	978.45	978.60	978.75	978.90	979.05	979.20	979.35	979.50	979.65	979.80	979.95
984.00	984.15	984.30	984.45	984.60	984.75	984.90	985.05	985.20	985.35	985.50	985.65	985.80	985.95
990.00	990.15	990.30	990.45	990.60	990.75	990.90	991.05	991.20	991.35	991.50	991.65	991.80	991.95
996.00	996.15	996.30	996.45	996.60	996.75	996.90	997.05	997.20	997.35	997.50	997.65	997.80	997.95
1002.00	1002.15	1002.30	1002.45	1002.60	1002.75	1002.90	1003.05	1003.20	1003.35	1003.50	1003.65	1003.80	1003.95
1008.00	1008.15	1008.30	1008.45	1008.60	1008.75	1008.90	1009.05	1009.20	1009.35	1009.50	1009.65	1009.80	1009.95
1014.00	1014.15	1014.30	1014.45	1014.60	1014.75	1014.90	1015.05	1015.20	1015.35	1015.50	1015.65	1015.80	1015.95
1020.00	1020.15	1020.30	1020.45	1020.60	1020.75	1020.90	1021.05	1021.20	1021.35	1021.50	1021.65	1021.80	1021.95
1026.00	1026.15	1026.30	1026.45	1026.60	1026.75	1026.90	1027.05	1027.20	1027.35	1027.50	1027.65	1027.80	1027.95
1032.00	1032.15	1032.30	1032.45	1032.60	1032.75	1032.90	1033.05	1033.20	1033.35	1033.50			

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 35	*	MONT.	206	29
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

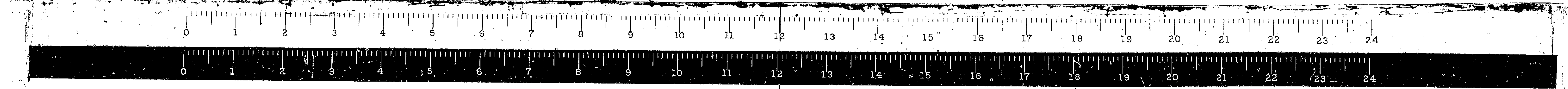


B. M.	ELEV.	STA.	DESCRIPTION
B.M. 21E-1	626.57	60+33	Spike in Telephone Pole S. Side of Road
B.M. 21E-2	624.21	72+15	Top of Curb S.W. Corner of Bridge
B.M. 21W-2	640.43	40+68	N. Bolt Fire Hydrant S. Side of Road



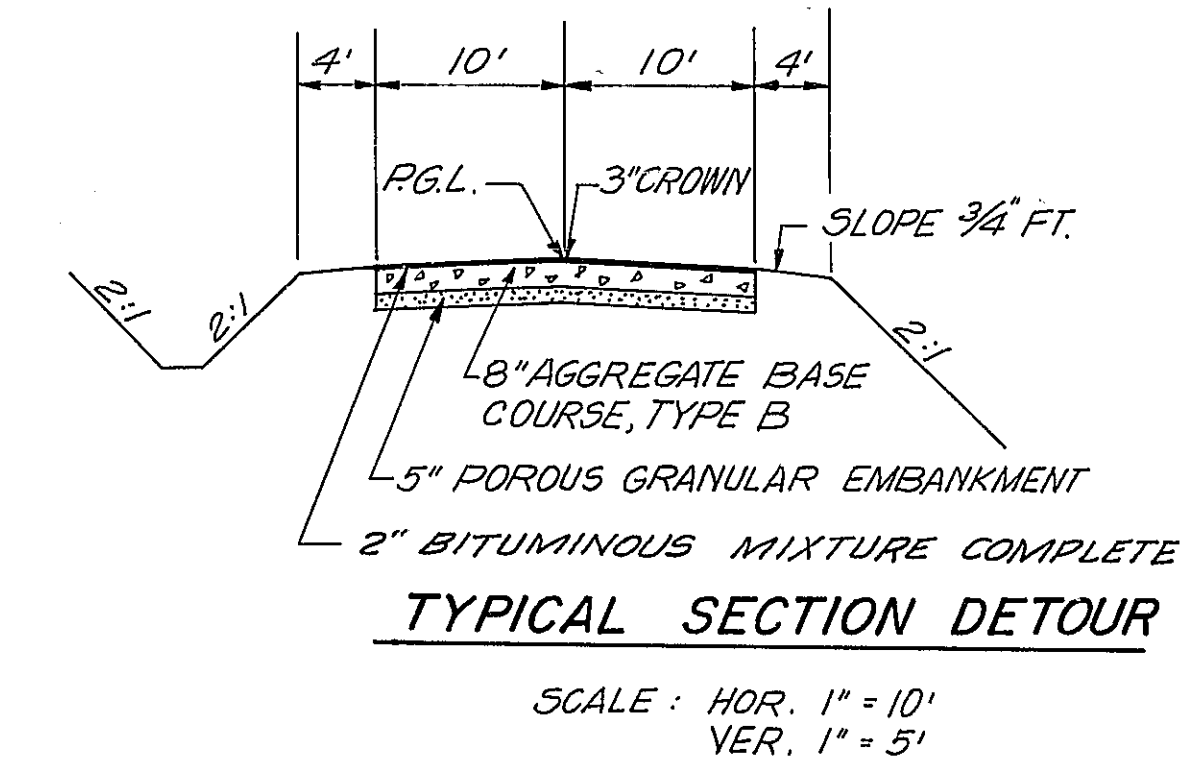
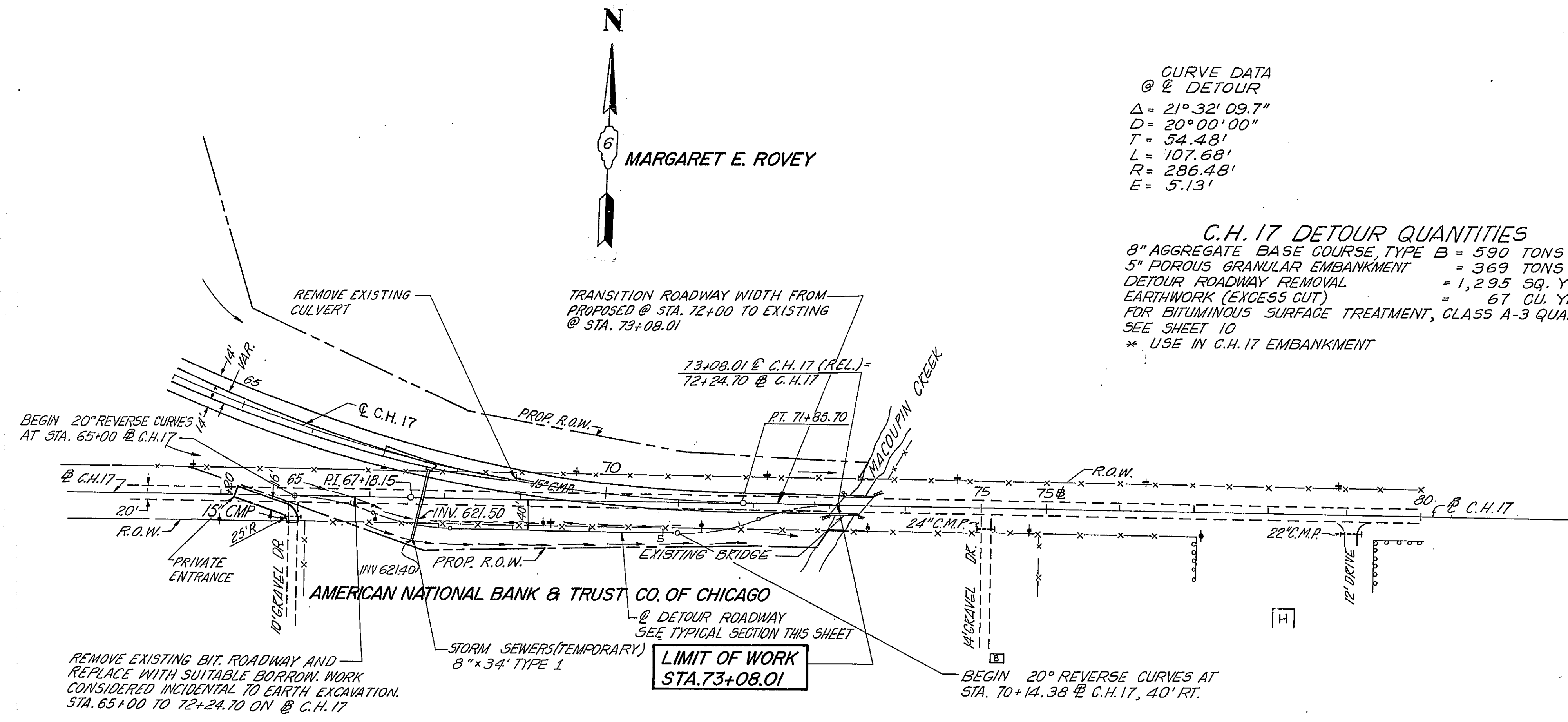
EXISTING ELEV.	PROPOSED ELEV.	STA.
642.4	642.40	35
642.5	642.51	35
642.6	642.51	35
642.7	643.19	35
642.8	644.10	35
642.9	645.00	35
643.0	647.00	35
643.1	649.00	35
643.2	651.00	35
643.3	653.00	35
643.4	655.00	35
643.5	657.00	35
643.6	659.00	35
643.7	661.00	35
643.8	663.00	35
643.9	665.00	35
644.0	666.10	35
644.1	666.93	35
644.2	668.30	35
644.3	669.00	35
644.4	669.03	35
644.5	669.40	35
644.6	667.10	35
644.7	666.10	35
644.8	665.20	35
644.9	663.20	35
645.0	661.20	35
645.1	659.20	35
645.2	659.20	35
645.3	657.20	35
645.4	655.20	35
645.5	653.16	35
645.6	652.12	35
645.7	651.05	35
645.8	649.87	35
645.9	648.65	35
646.0	647.43	35
646.1	646.21	35

STA. 31+63.00 TO STA. 63+00 C.H. 17



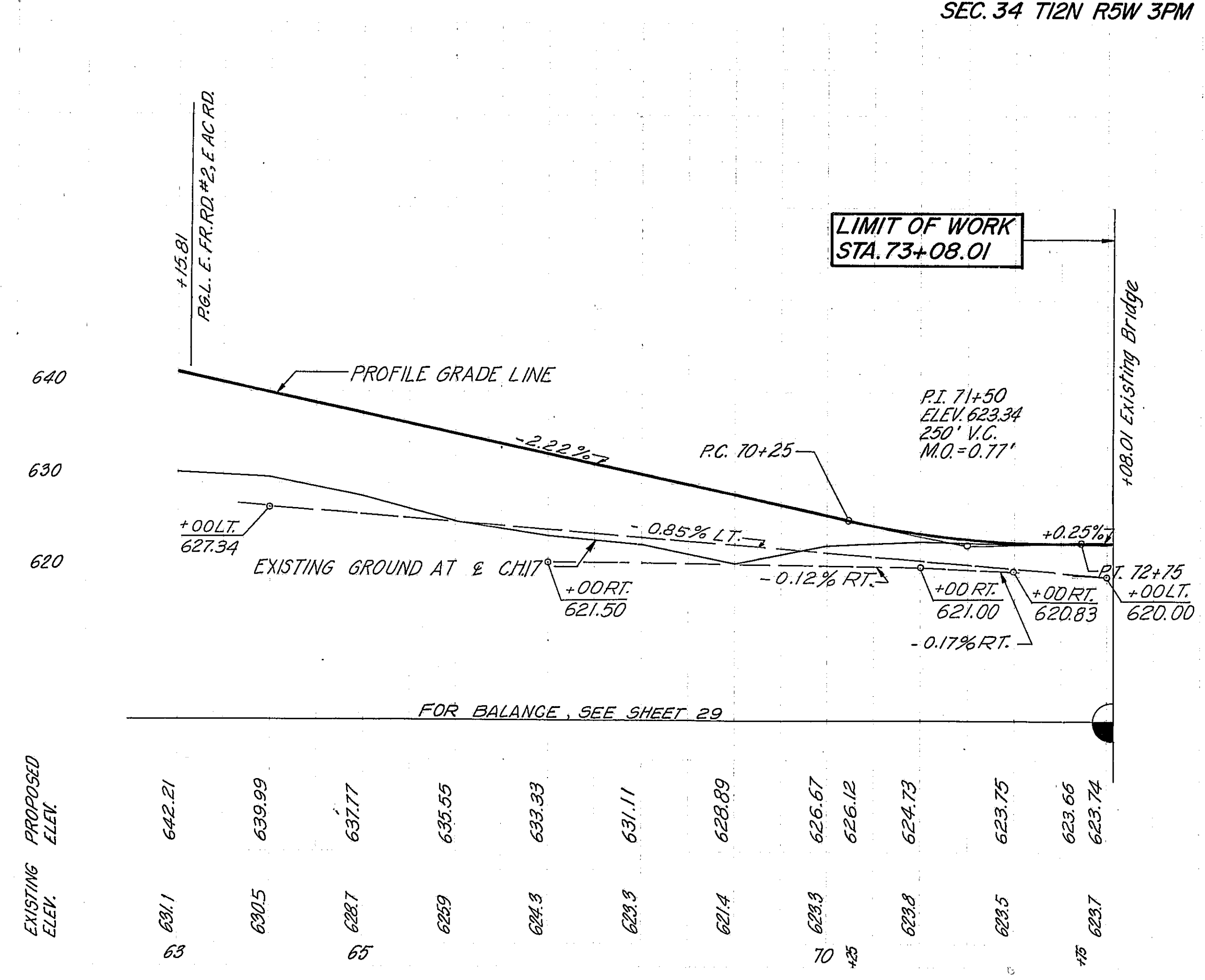
*68-5, 68-5HB, 68-5 B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	30
FEED ROAD DIST. NO. 4 ILLINOIS PROJ.				

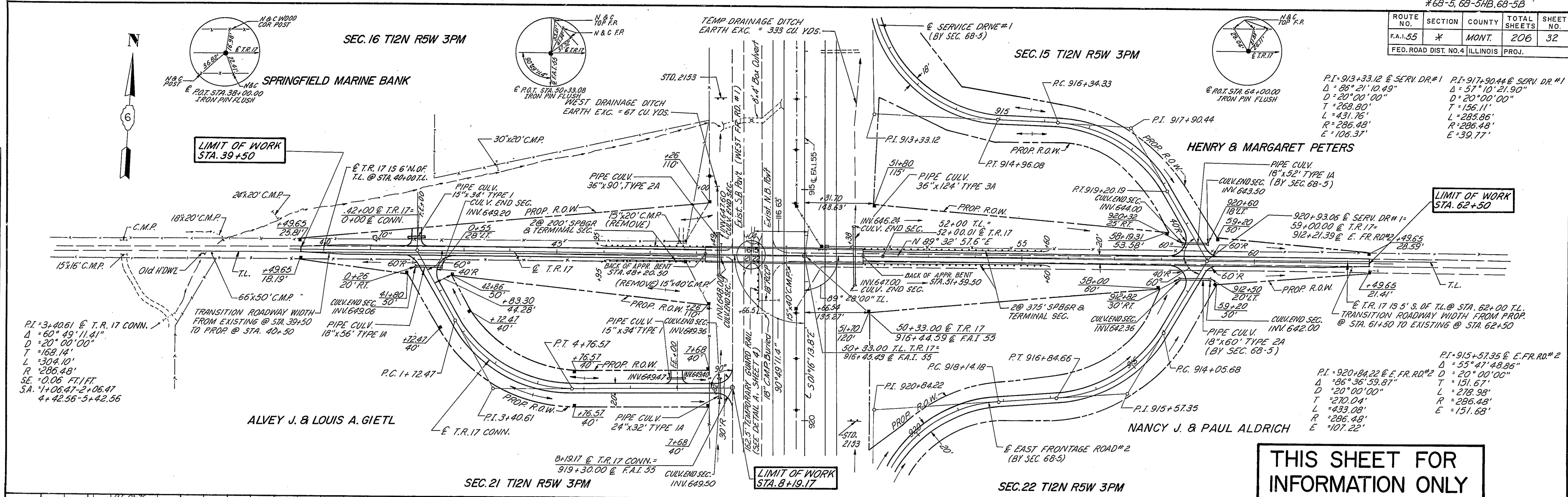


NOTES: USE TRAFFIC CONTROL AND PROTECTION STD. 2312 FOR ONE LANE TRAFFIC ON THE BRIDGE WHILE CONSTRUCTING ROADWAY FROM STA. 70+00 TO STA. 73+08.01 UNDER TRAFFIC. CONSTRUCT DETOUR AND USE TRAFFIC CONTROL AND PROTECTION STD. 2310 FOR THE REMAINDER OF CONSTRUCTION OF C.H. 17 FROM STA. 64+00 TO STA. 70+00.

NOTES: FOR INTERCHANGE ALIGNMENT, SEE SHEETS 43 & 44. FOR INTERCHANGE DRAINAGE & RIGHT OF WAY NOTES SEE SHEETS 45 & 46. FOR CONTINUATION OF C.H. 17, SEE SHEET 29.

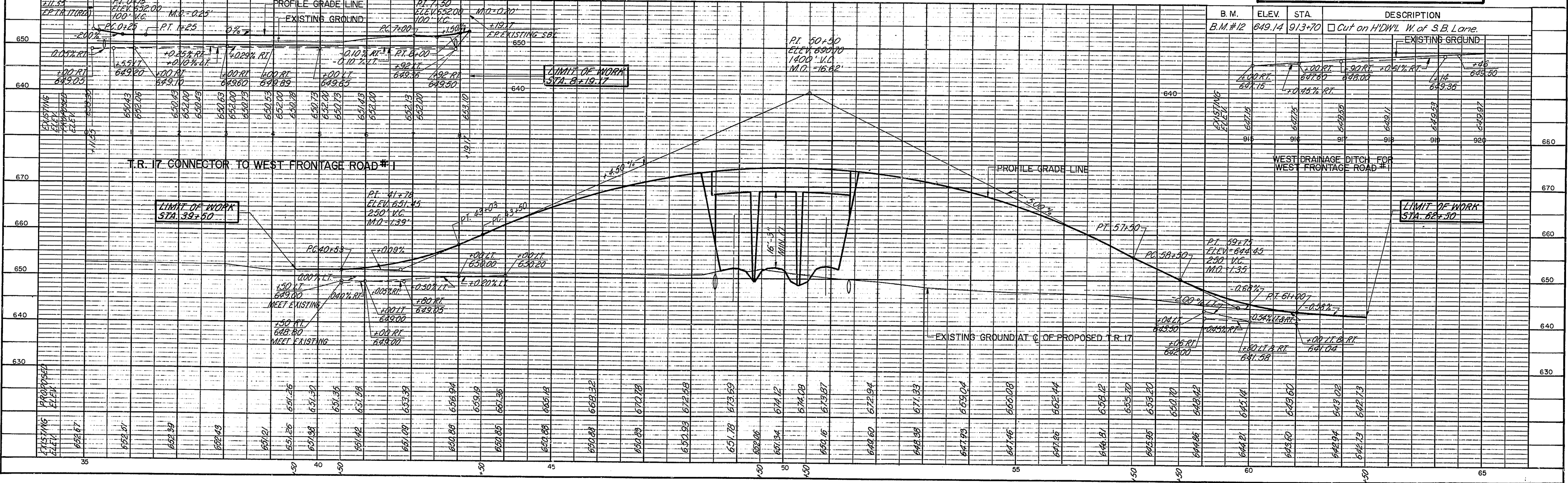


*68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
F.A.I. 65	*	MONT.	206
FED. ROAD DIST. NO. 4		ILLINOIS	PROJ.
			SHEET NO. 32



DATE	BY	REVISION
2-7-72	J.P.D.	AS BUILT
2-7-72	J.P.D.	ALIGNED CHECKED
2-7-72	J.P.D.	BY PLAN CHECKED

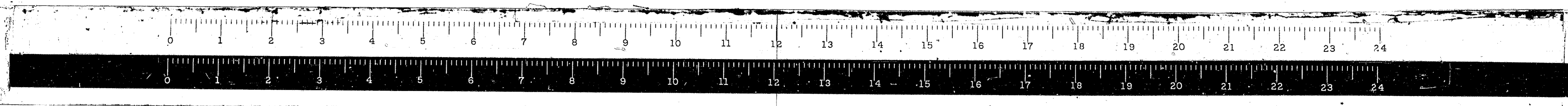
DATE	BY	REVISION
2-7-72	J.P.D.	AS BUILT
2-7-72	J.P.D.	ALIGNED CHECKED
2-7-72	J.P.D.	BY PLAN CHECKED



THIS SHEET FOR INFORMATION ONLY

PLATE 1 PLAN - PROFILE & P.R. STANDARD
ENGINE WORKS CO., CHICAGO - NEW YORK

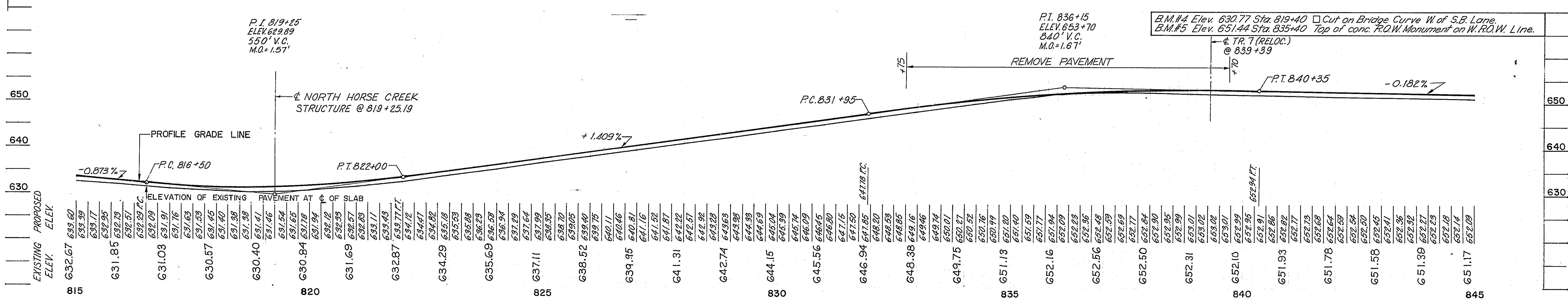
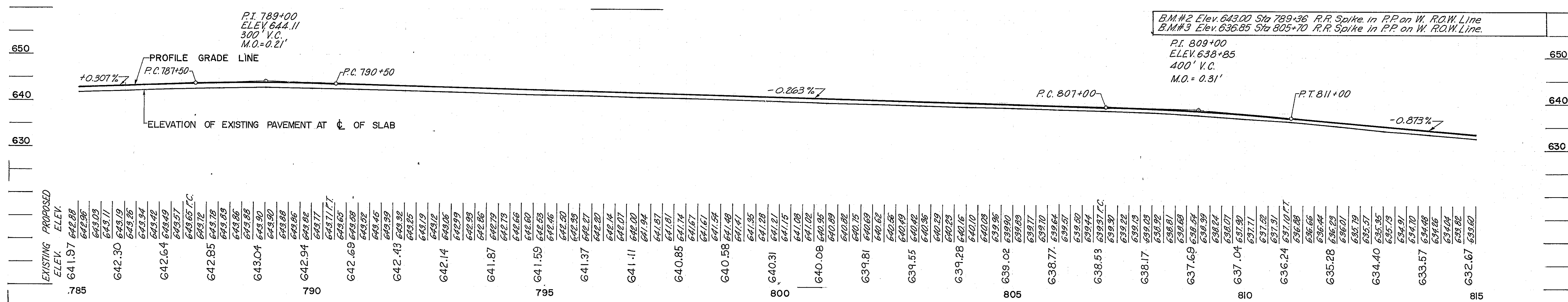
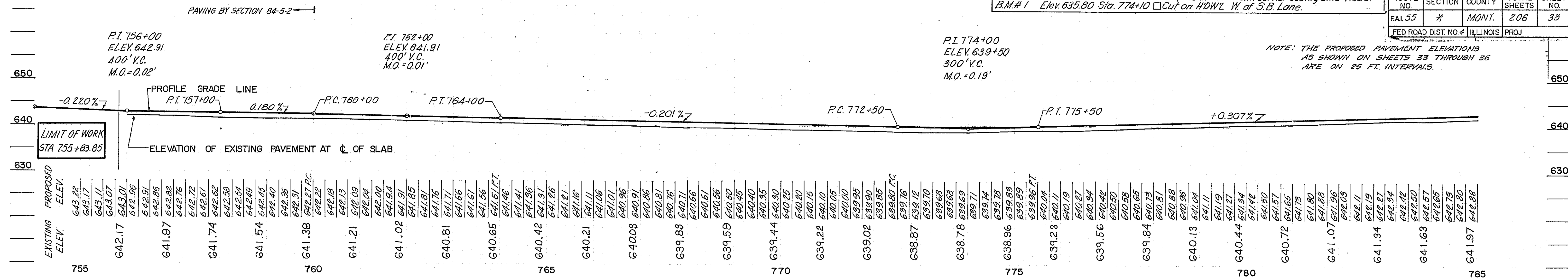
TR17



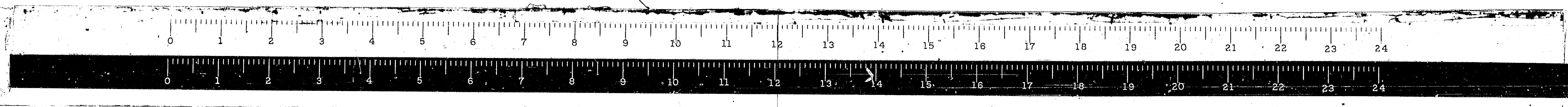
Sta. 755+00 Elev. 640.55 Sta. 756+15 R.R. Spike in P.P. at S.W. Corner County Line Road.
 B.M.#1 Elev. 635.60 Sta. 774+10 Cut on HDWL W. of S.B. Lane.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT.	206	33
FED. ROAD DIST. NO. 7 ILLINOIS PROJ.				

NOTE: THE PROPOSED PAVEMENT ELEVATIONS AS SHOWN ON SHEETS 33 THROUGH 36 ARE ON 25 FT. INTERVALS.



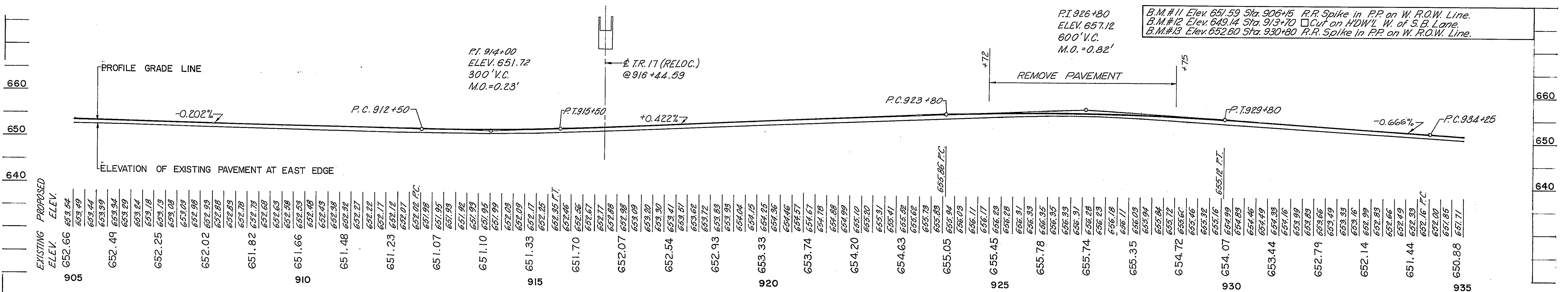
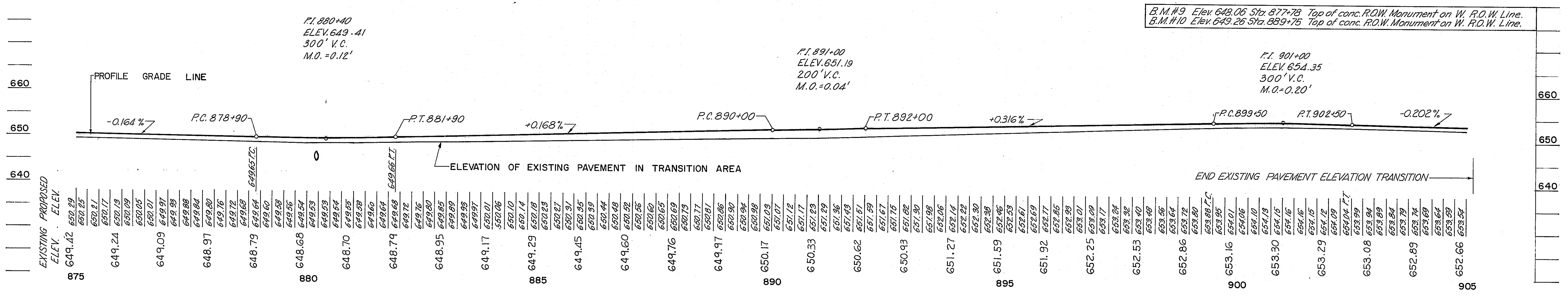
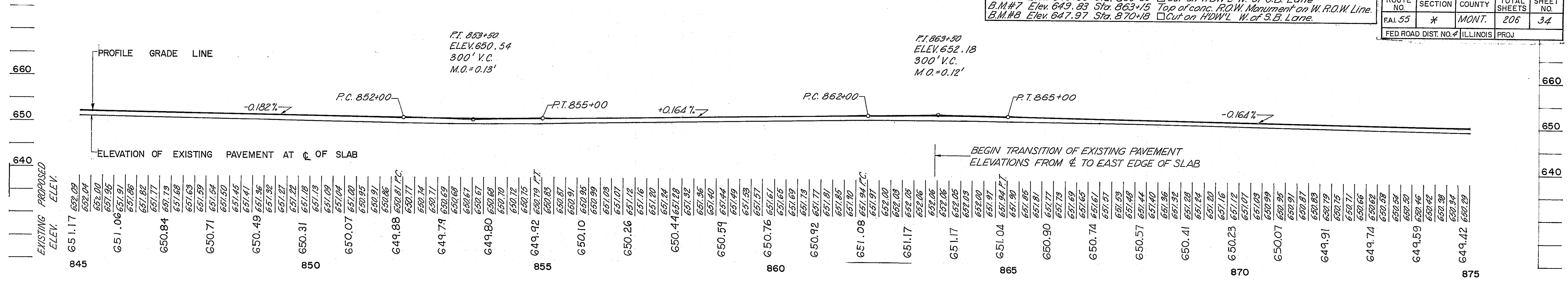
PROFILE OF SOUTHBOUND FAI 55 PAVEMENT SECTION 68-5 PROJECT 1-55-2(6)71 MONTGOMERY COUNTY



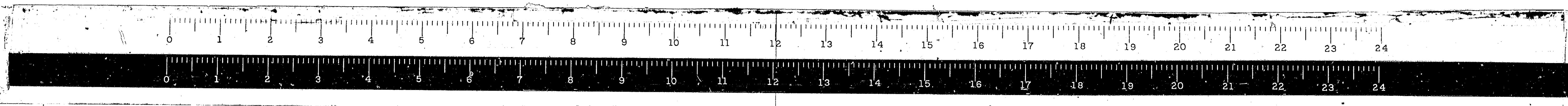
* 68-5, 68-54B, 68-5B

B.M.#6 Elev. 649.33 Sta. 853+30 Cut on HDWL W. of S.B. Lane
 B.M.#7 Elev. 649.83 Sta. 863+15 Top of conc. ROW Monument on W. R.O.W. Line.
 B.M.#8 Elev. 647.97 Sta. 870+18 Cut on HDWL W. of S.B. Lane.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT.	206	34
FED. ROAD DIST. NO. 7 ILLINOIS PROJ.				



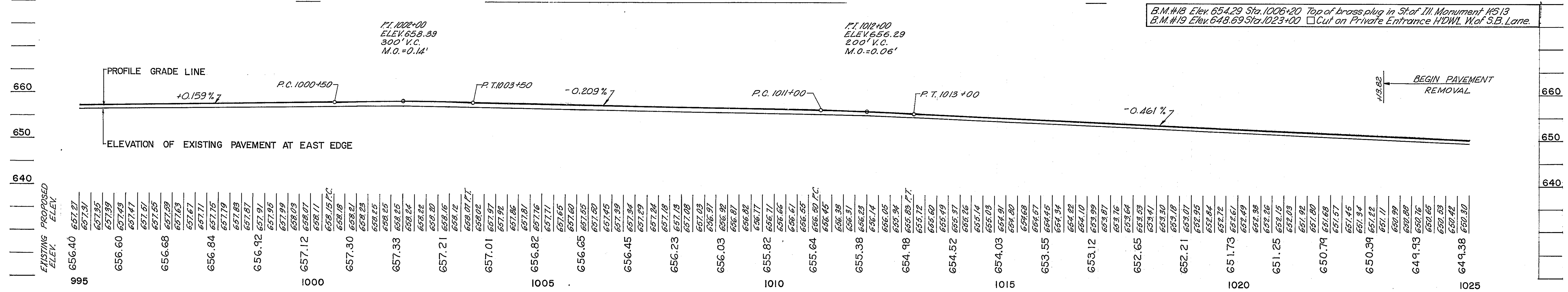
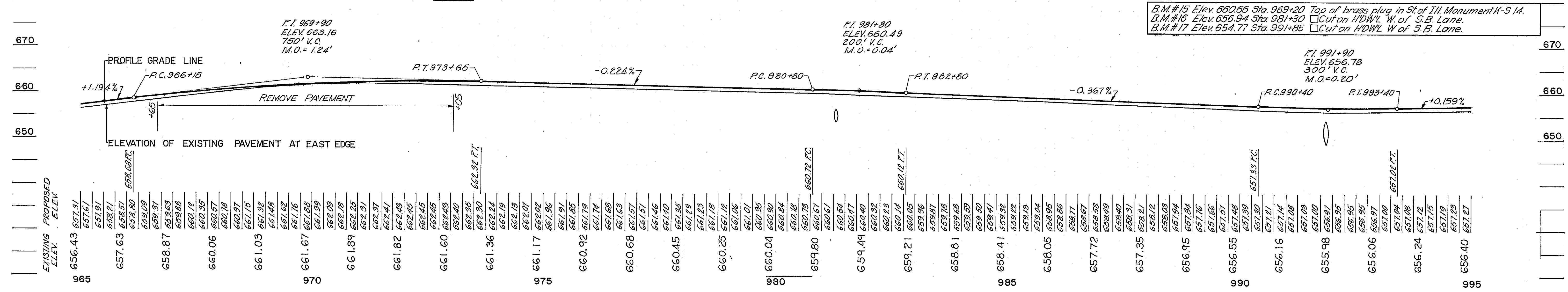
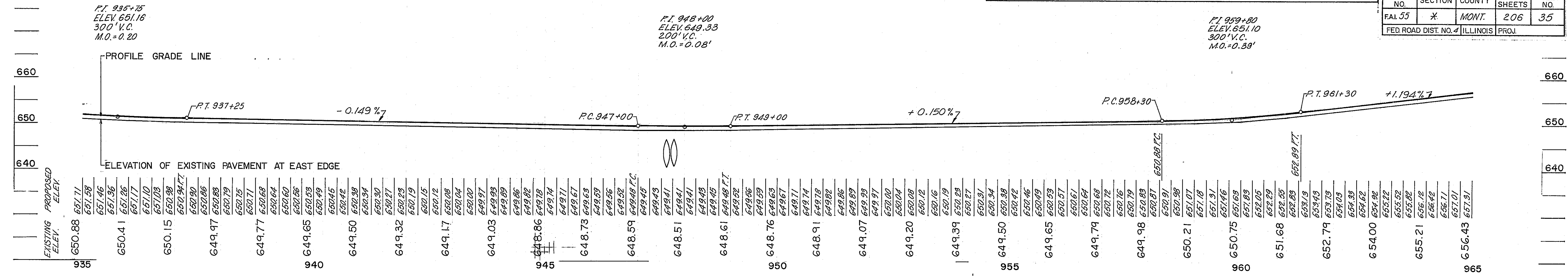
PROFILE OF SOUTHBOUND FAI 55 PAVEMENT
 SECTION 68-5 PROJECT 1-55-2(6)71 MONTGOMERY COUNTY



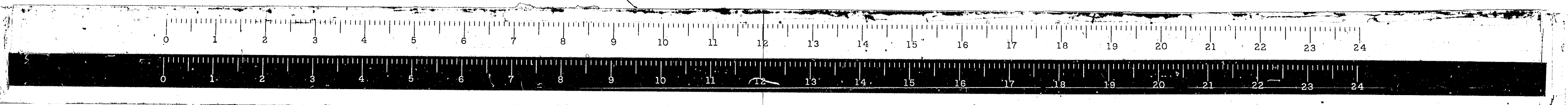
* 68-5,68-54B,68-5B.

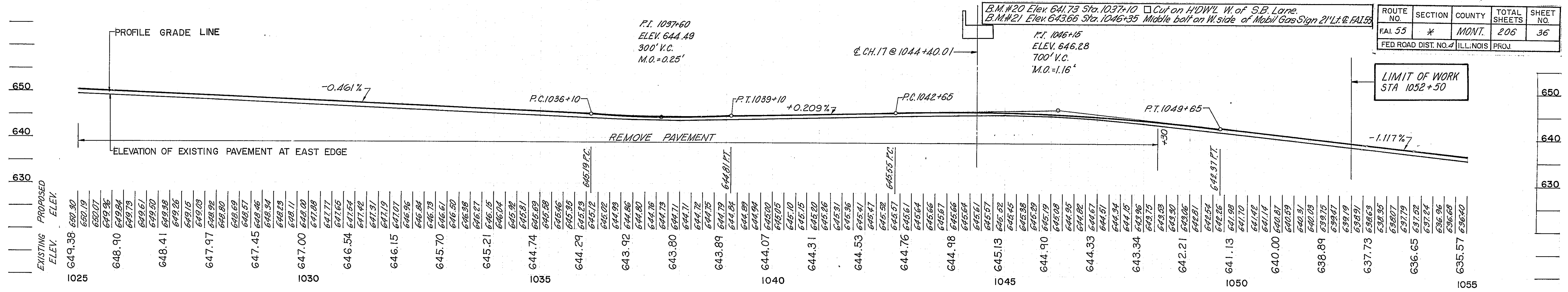
B.M.#14 Elev. 648.04 Sta. 947+60 Cut on H/DWL W of S.B. Lane.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	X	MONT.	206	35
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



PROFILE OF SOUTHBOUND FAI 55 PAVEMENT
SECTION 68-5 PROJECT 1-55-2(6)71 MONTGOMERY COUNTY

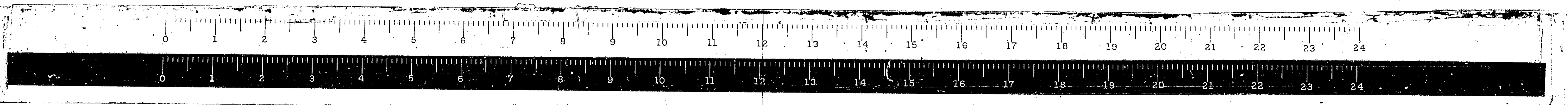




ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	X	MONT.	205	36
FED. ROAD DIST. NO. 4			ILL. NOIS PROJ.	

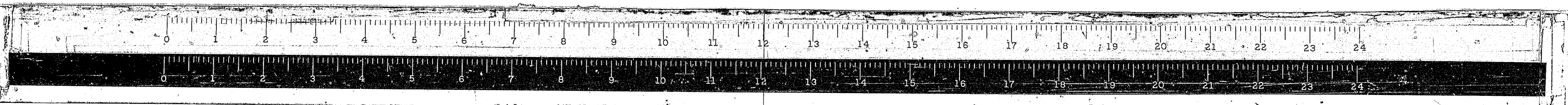
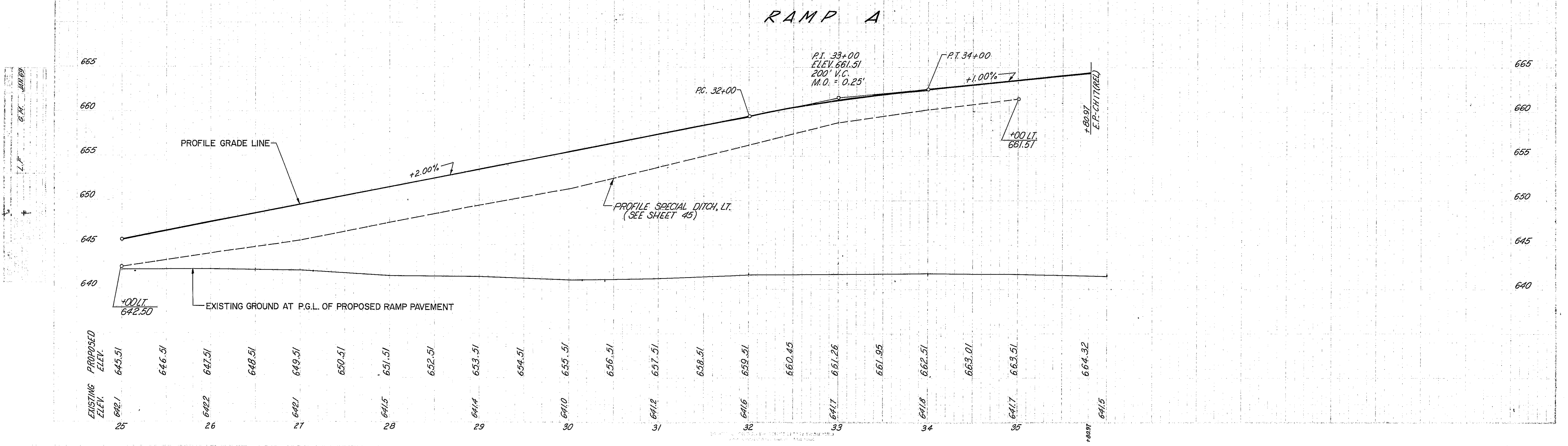
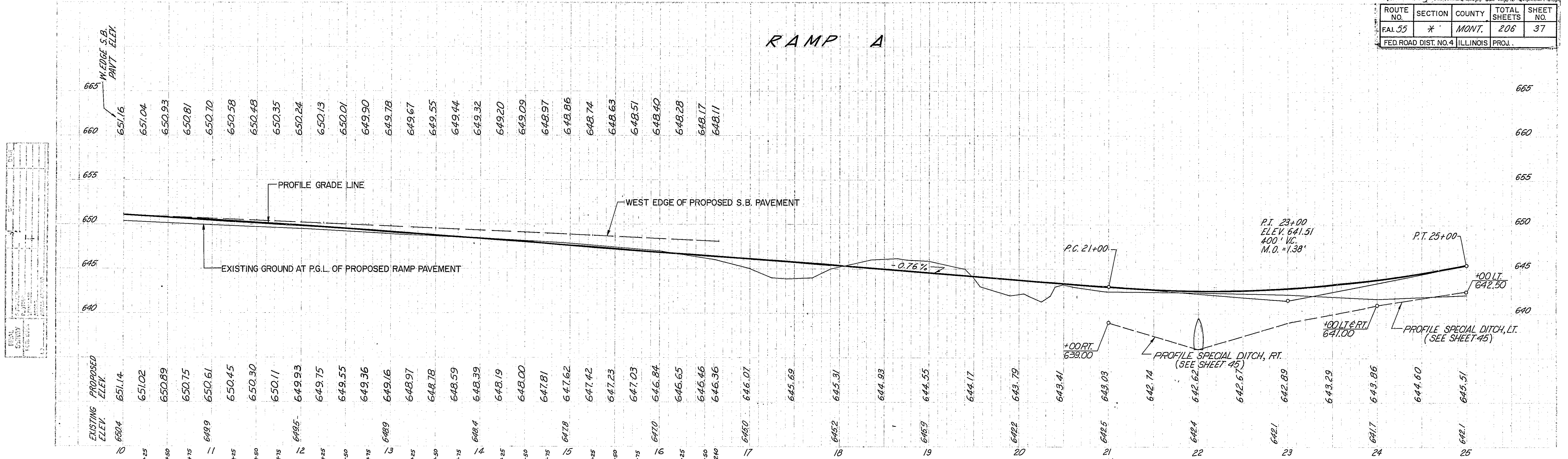
B.M.#22 Elev. 635.75 Sta. 1056+20 S.E. Flange Bolt on Fire Hydrant at S.W. Corner intersection, 2 blocks S. of Farmersville Road

PROFILE OF SOUTHBOUND F.A.I. 55 PAVEMENT
SECTION 68-5 PROJECT I-55 -2(6) 71 MONTGOMERY COUNTY



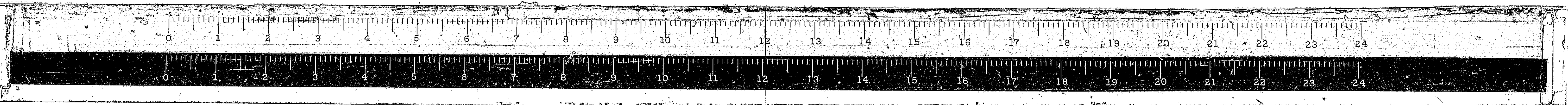
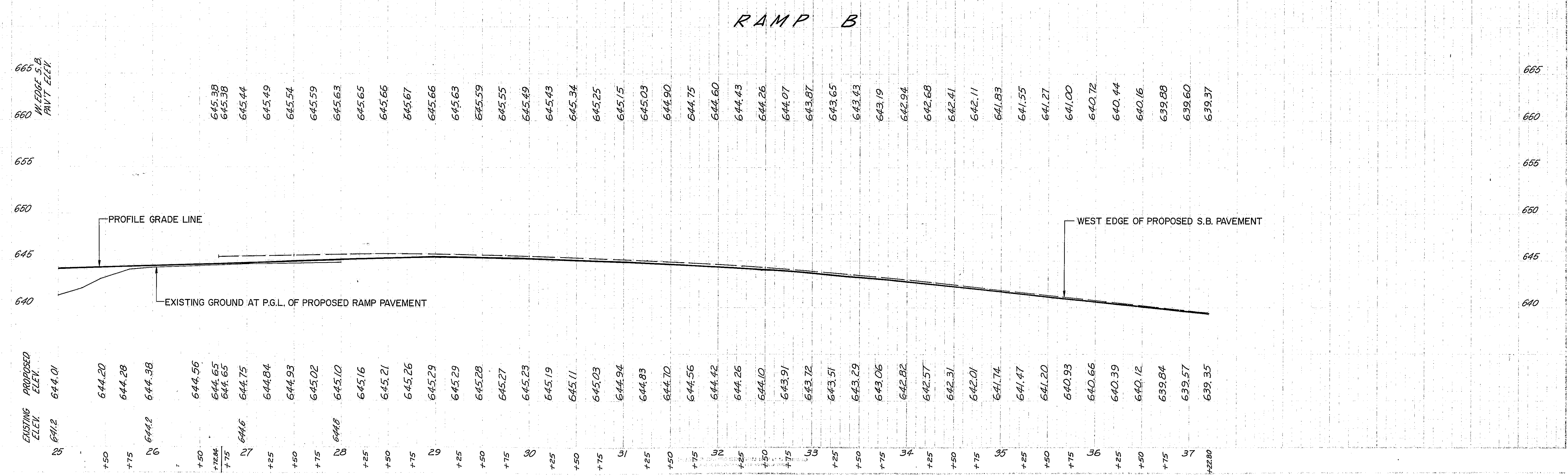
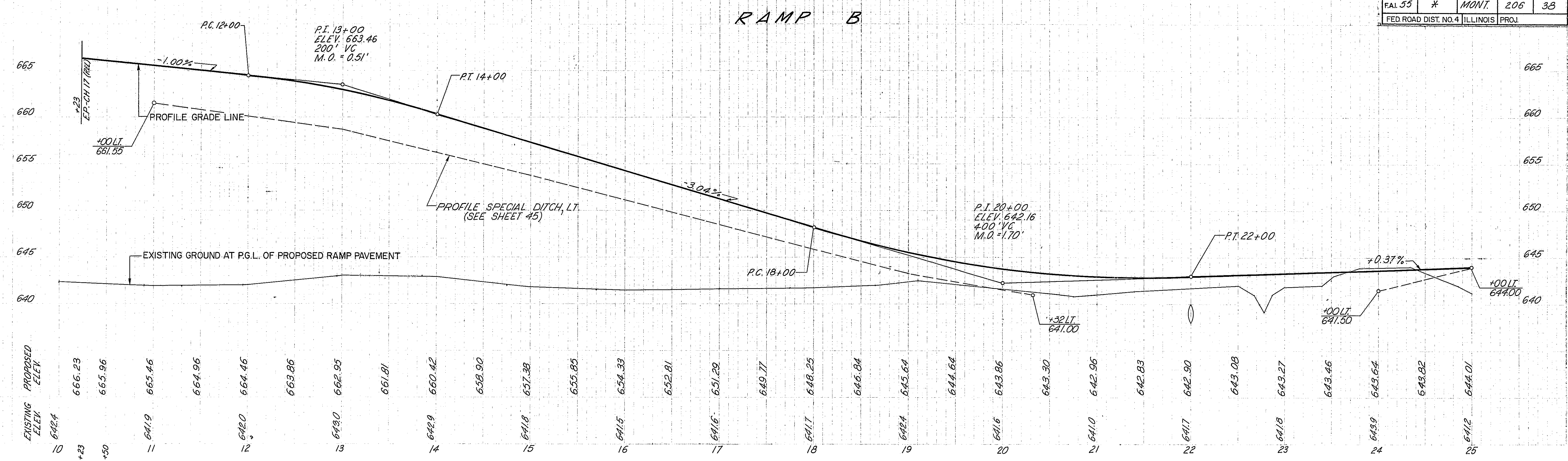
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-5	*	MONT.	206	37
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



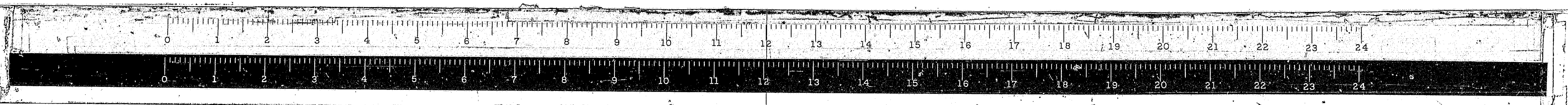
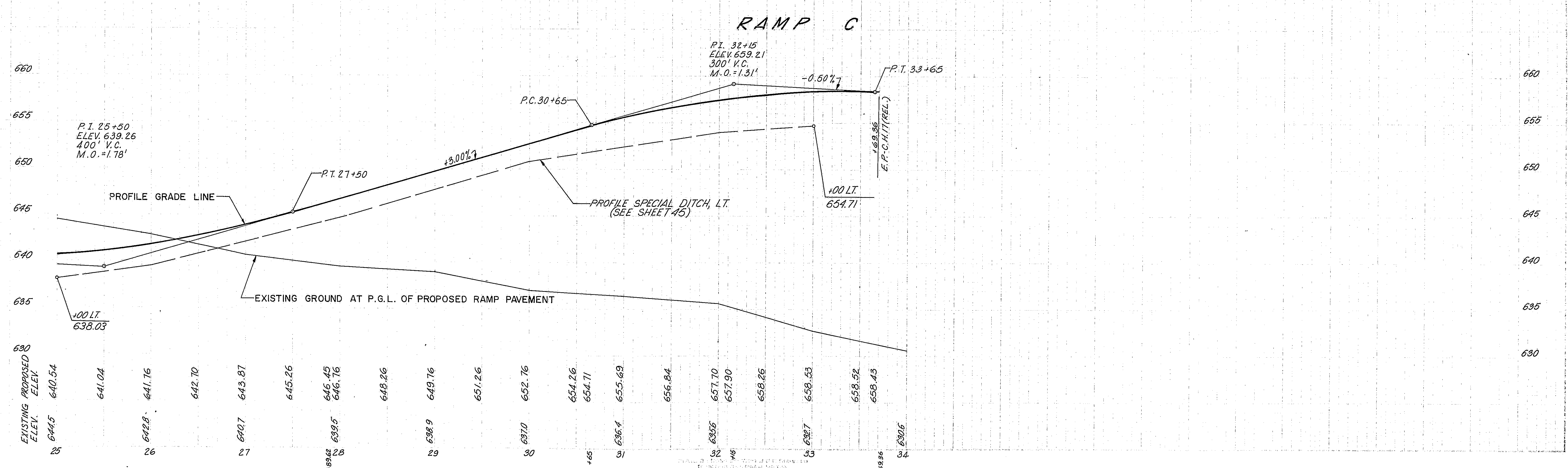
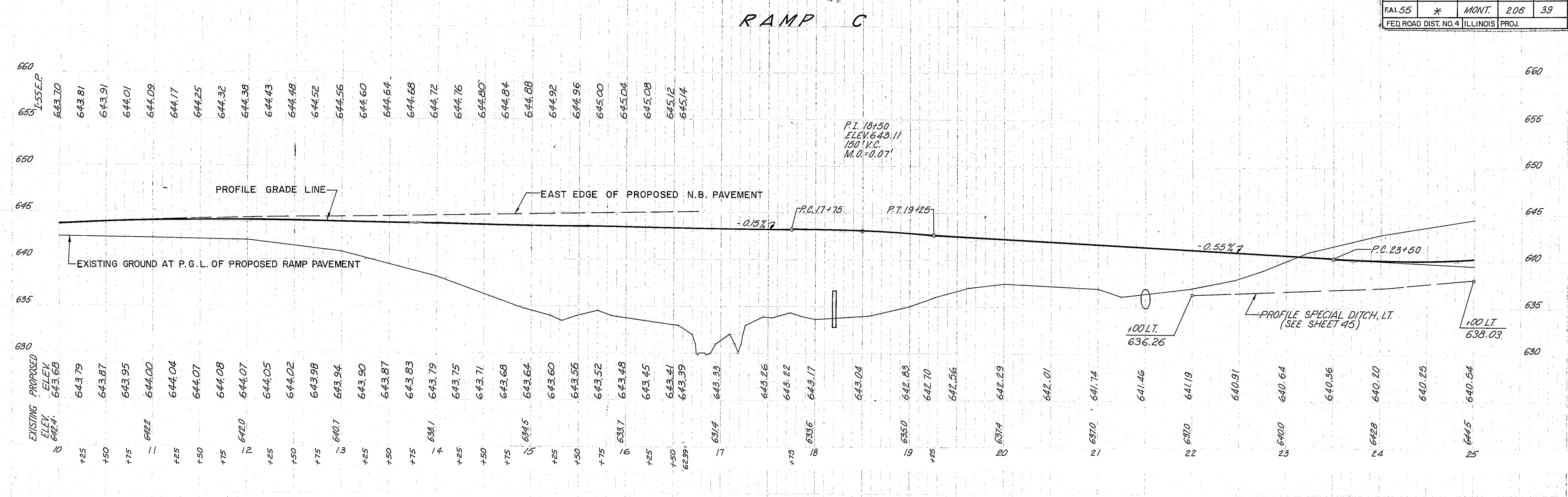
X 65-5, 68-5HB, 68-5B

ROUTE NO.	SECTION #	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	35
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



* 68-5, 68-5HB, 68-5B

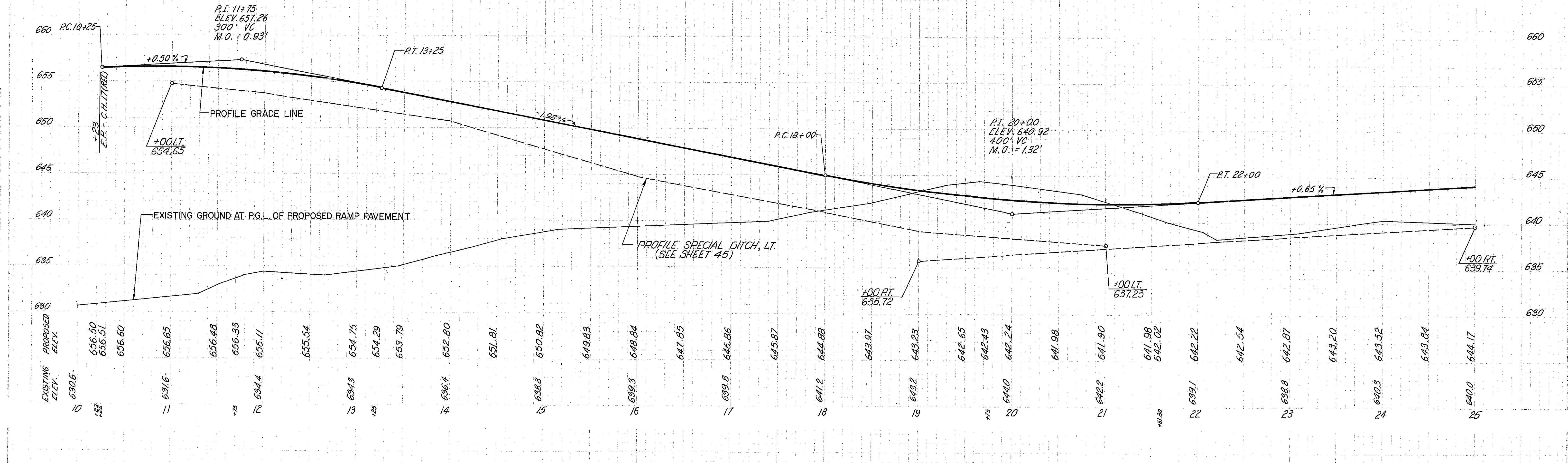
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	39
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



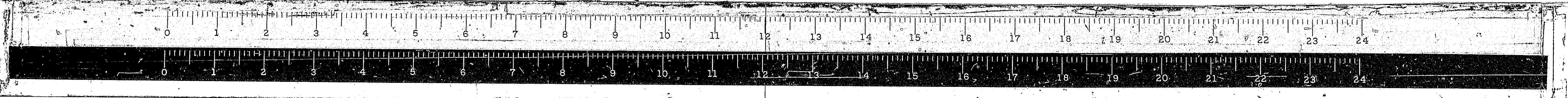
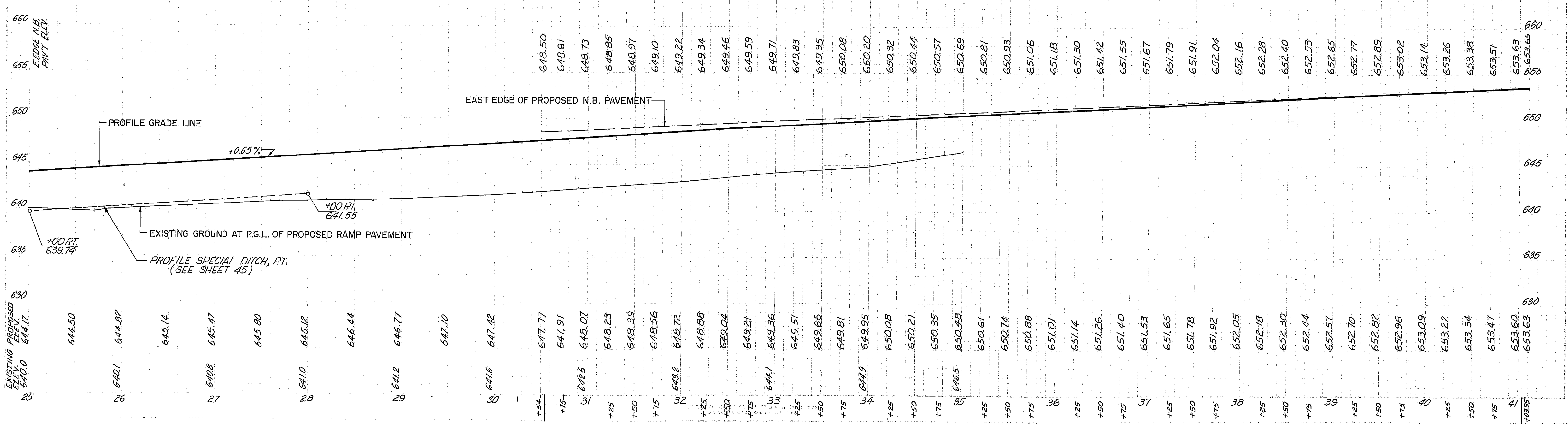
*68-5, 68-5A, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	40
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

RAMP D



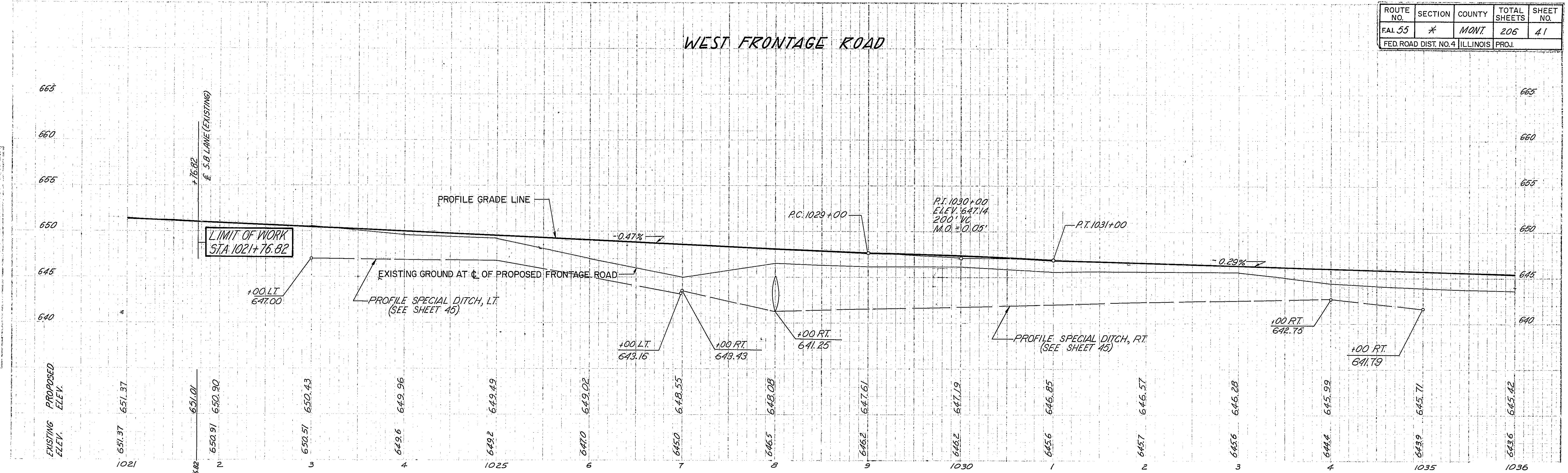
RAMP D



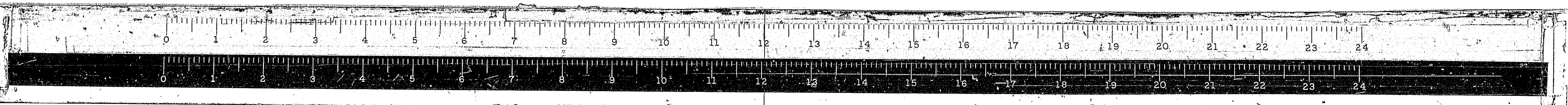
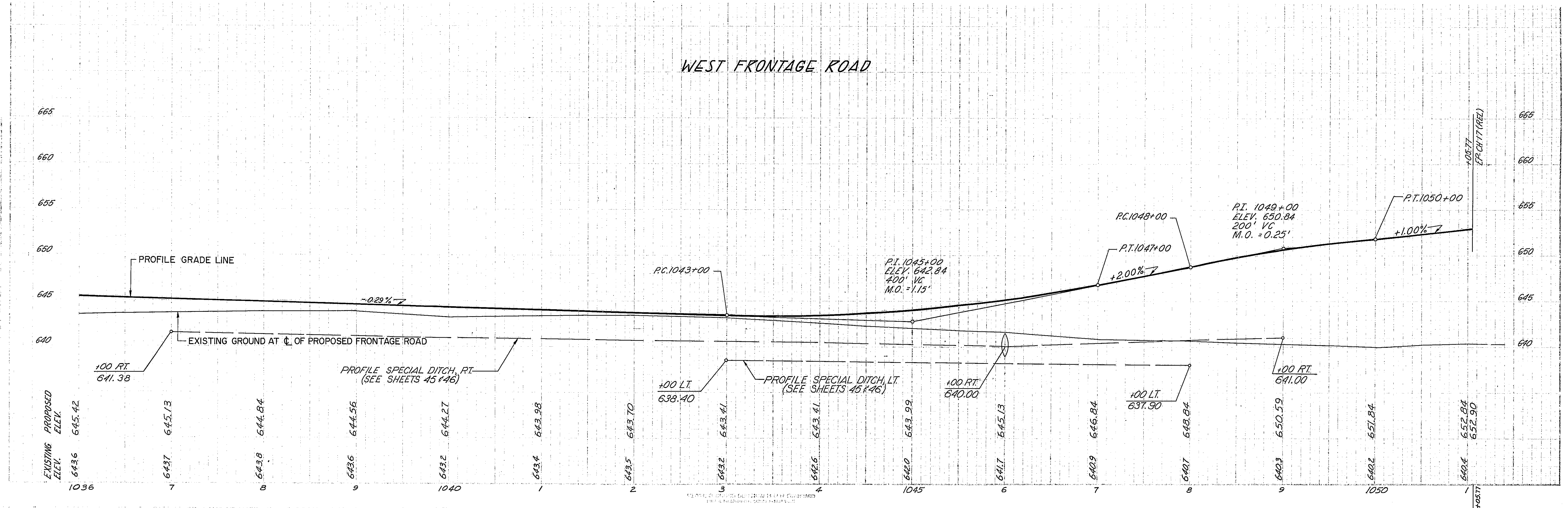
#68-5, 68-5AB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT	206	41
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

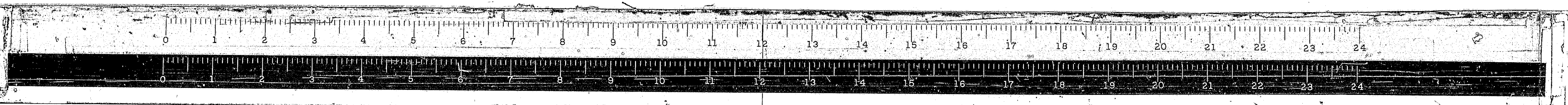
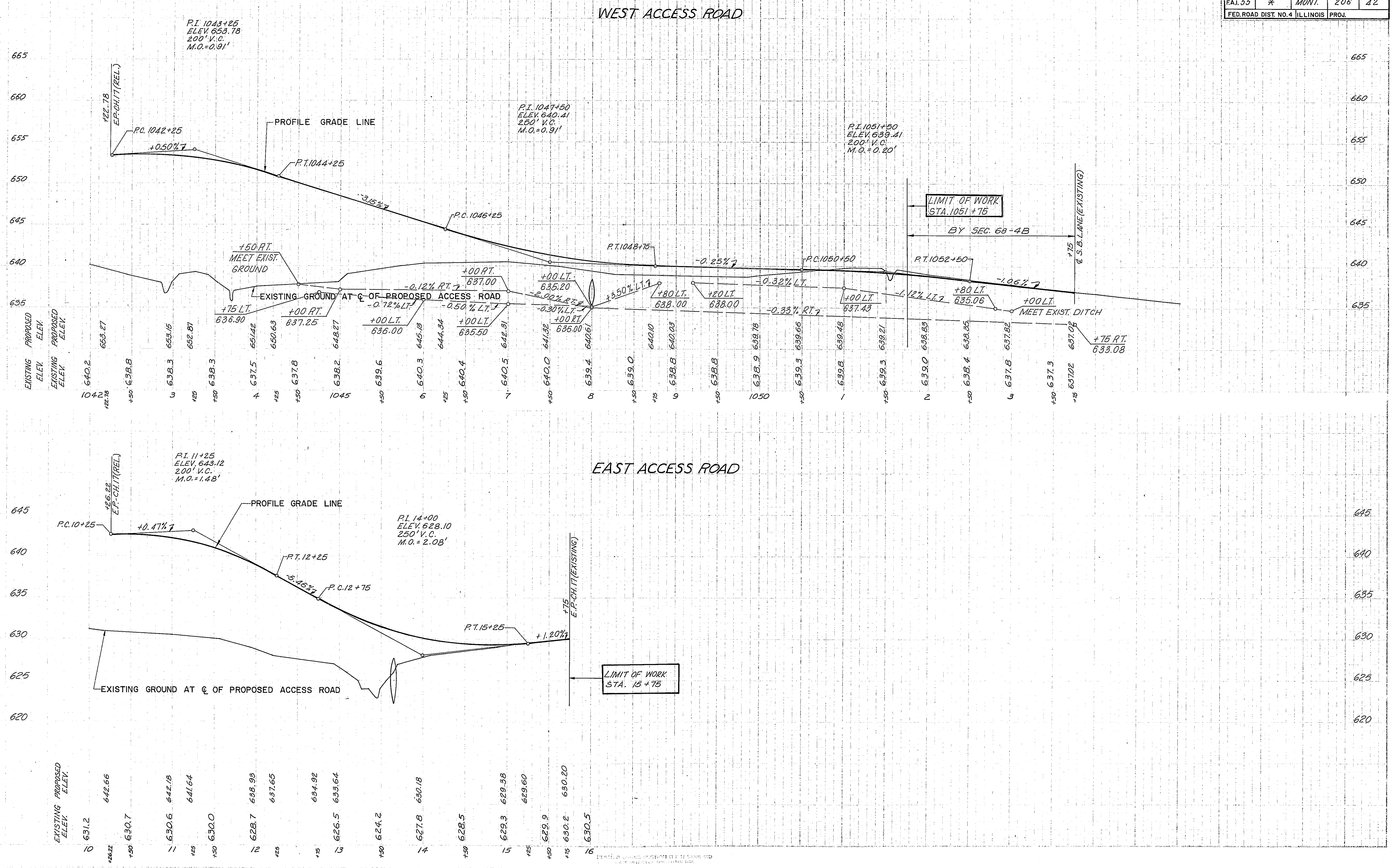
WEST FRONTAGE ROAD



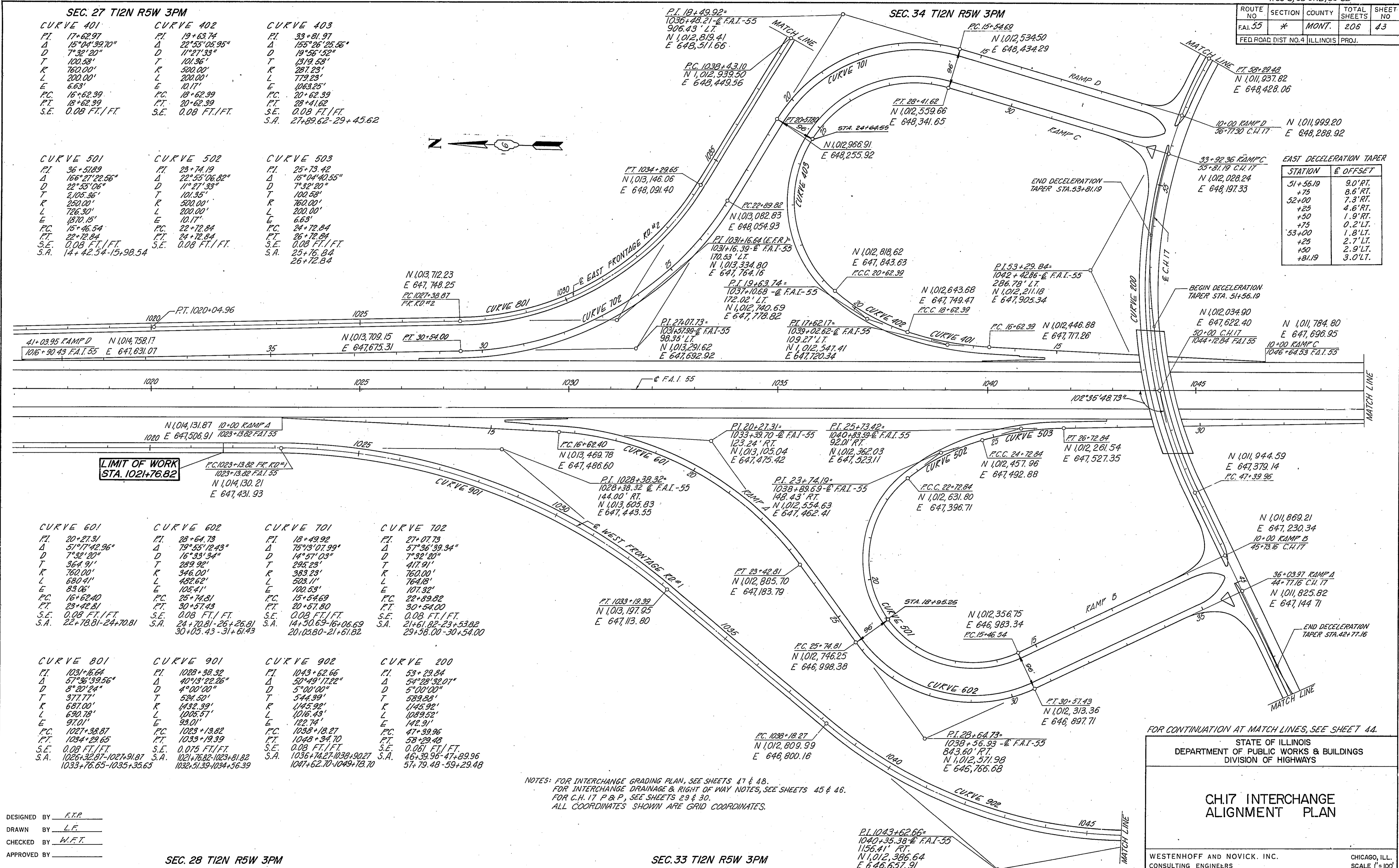
WEST FRONTAGE ROAD



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	* *	MONT.	206	42
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	#	MONT.	2.06	43
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



SEC. 27 T12N R5W 3PM

CURVE 401	CURVE 402	CURVE 403
PI 17+62.97 Δ 15°04'39.70" D 7°32'20" T 120.58' K 762.20' L 200.00' E 6.63' P.C. 16+62.39 P.T. 18+62.39 S.E. 0.08 FT./FT.	PI 19+63.74 Δ 22°53'05.95" D 11°27'33" T 101.36' K 502.00' L 200.00' E 10.17' P.C. 18+62.39 P.T. 20+62.39 S.E. 0.08 FT./FT.	PI 33+81.97 Δ 155°26'25.56" D 19°56'52" T 1319.58' K 281.23' L 779.29' E 1063.25' P.C. 20+62.39 P.T. 28+41.62 S.E. 0.08 FT./FT. S.A. 27+89.62-29+45.62

CURVE 501	CURVE 502	CURVE 503
PI 36+51.89 Δ 166°21'22.56" D 22°53'06" T 2105.38' K 762.30' L 1870.15' E 146.54' P.C. 22+72.84 P.T. 22+72.84 S.E. 0.08 FT./FT. S.A. 14+42.34-15+98.54	PI 23+74.19 Δ 22°53'06.82" D 11°27'33" T 101.36' K 502.00' L 200.00' E 10.17' P.C. 22+72.84 P.T. 24+72.84 S.E. 0.08 FT./FT.	PI 25+73.42 Δ 15°04'40.55" D 7°32'20" T 102.89' K 160.00' L 200.00' E 6.63' P.C. 24+72.84 P.T. 26+72.84 S.E. 0.08 FT./FT. S.A. 25+76.34-26+72.84

CURVE 601	CURVE 602	CURVE 701	CURVE 702
PI 20+27.31 Δ 51°07'42.96" D 7°32'20" T 364.91' K 762.00' L 680.41' E 83.06' P.C. 16+62.40 P.T. 23+42.81 S.E. 0.08 FT./FT. S.A. 22+78.81-24+70.81	PI 28+64.73 Δ 79°55'12.43" D 16°33'34" T 289.92' K 346.00' L 488.02' E 105.41' P.C. 25+74.81 P.T. 30+57.43 S.E. 0.08 FT./FT. S.A. 24+70.81-26+26.91 30+03.43-31+61.43	PI 18+49.92 Δ 75°03'07.99" D 14°57'03" T 296.28' K 383.28' L 508.11' E 107.32' P.C. 15+54.69 P.T. 20+57.80 S.E. 0.08 FT./FT. S.A. 14+50.63-16+06.63 20+03.80-21+61.82	PI 27+07.79 Δ 57°36'39.34" D 7°32'20" T 417.91' K 760.00' L 764.78' E 107.32' P.C. 22+89.82 P.T. 30+54.00 S.E. 0.08 FT./FT. S.A. 21+61.82-23+53.82 29+38.00-30+54.00

CURVE 801	CURVE 901	CURVE 902	CURVE 200
PI 1031+16.64 Δ 57°36'39.56" D 8°20'24" T 371.77' K 687.00' L 690.78' E 97.01' P.C. 1027+38.87 P.T. 1034+29.65 S.E. 0.08 FT./FT. S.A. 1026+32.87-1027+91.87 1033+76.83-1035+38.63	PI 1028+38.32 Δ 40°13'22.26" D 4°00'00" T 581.50' K 1432.39' L 1005.57' E 93.01' P.C. 1023+13.82 P.T. 1033+19.39 S.E. 0.075 FT./FT. S.A. 1021+76.82-1023+81.82 1032+31.33-1034+36.39	PI 1043+62.66 Δ 50°49'17.22" D 5°00'00" T 544.99' K 1445.92' L 1016.43' E 122.74' P.C. 1035+13.27 P.T. 1048+34.70 S.E. 0.08 FT./FT. S.A. 1034+14.17-1039+30.27 1047+62.70-1049+78.70	PI 53+29.84 Δ 54°28'32.07" D 5°00'00" T 589.80' K 589.80' L 1089.52' E 142.91' P.C. 47+39.96 P.T. 58+29.48 S.E. 0.08 FT./FT. S.A. 46+39.96-47+29.96 57+79.48-59+29.48

EAST DECELERATION TAPER

STATION	OFFSET
51+56.19	9.0' RT.
+75	8.6' RT.
52+00	7.3' RT.
+25	4.6' RT.
+50	1.9' RT.
+75	0.2' LT.
53+00	1.8' LT.
+25	2.7' LT.
+50	2.9' LT.
+81.19	3.0' LT.

NOTES: FOR INTERCHANGE GRADING PLAN, SEE SHEETS 47 & 48.
FOR INTERCHANGE DRAINAGE & RIGHT OF WAY NOTES, SEE SHEETS 45 & 46.
FOR C.H. 17 P.B.P., SEE SHEETS 29 & 30.
ALL COORDINATES SHOWN ARE GRID COORDINATES.

FOR CONTINUATION AT MATCH LINES, SEE SHEET 44.

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

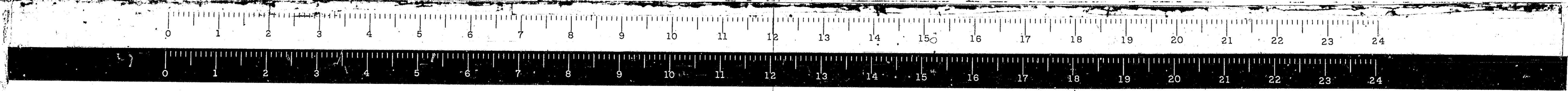
CH.I7 INTERCHANGE ALIGNMENT PLAN

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.
SCALE 1"=100'

DESIGNED BY F.R.P.
DRAWN BY L.E.
CHECKED BY W.F.T.
APPROVED BY

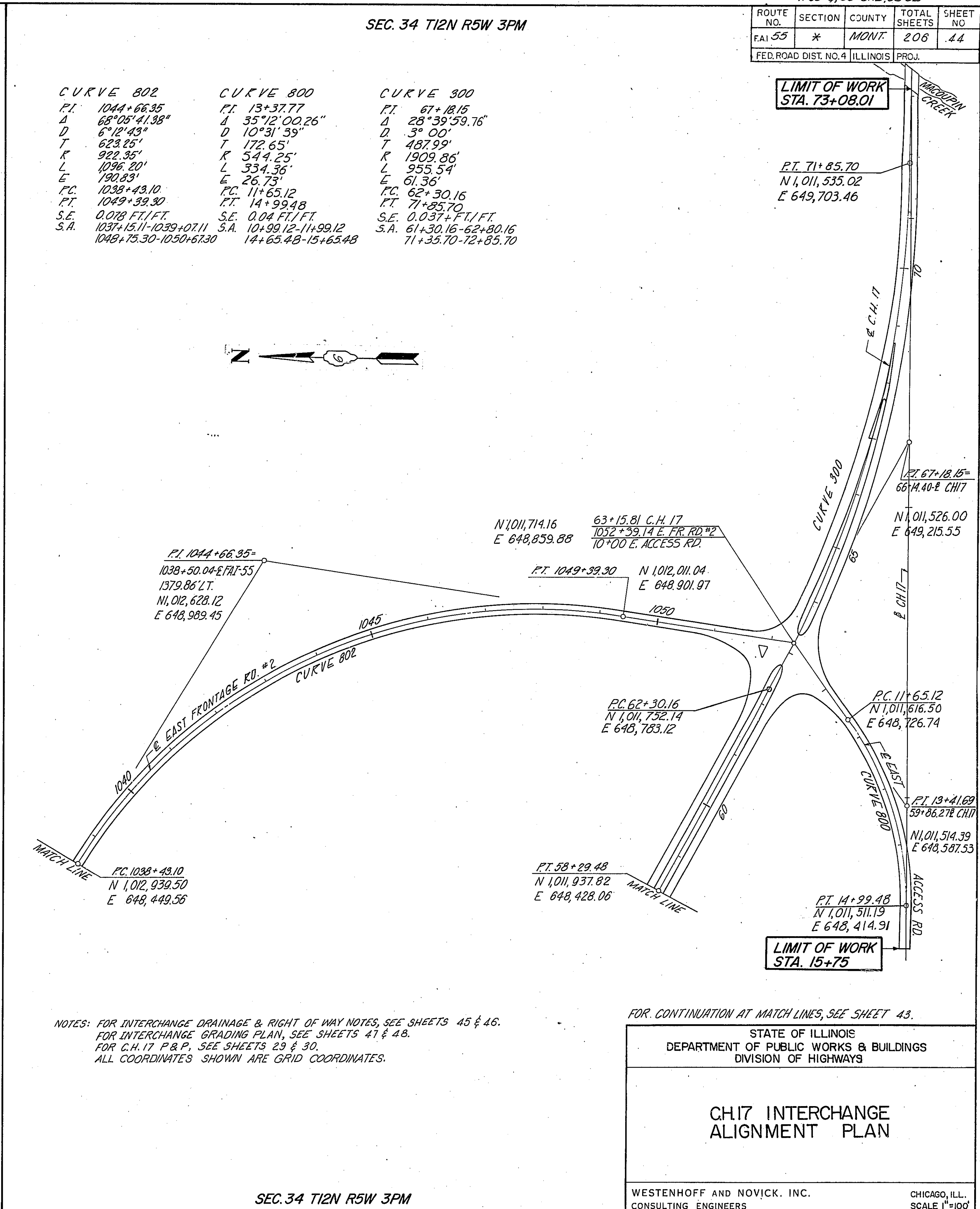
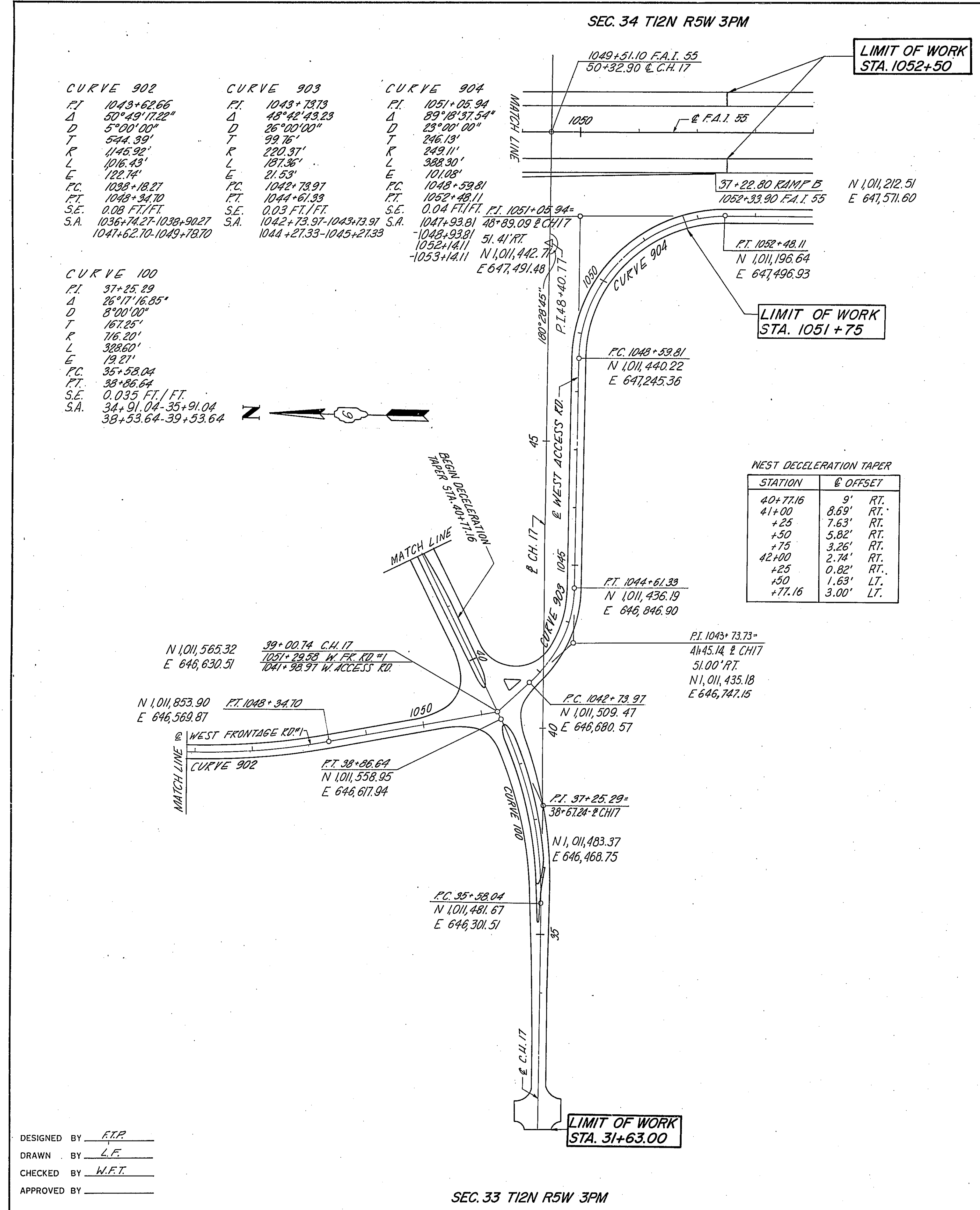
SEC. 28 T12N R5W 3PM

SEC. 33 T12N R5W 3PM



*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT.	206	44
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



DESIGNED BY F.P.
 DRAWN BY L.F.
 CHECKED BY M.F.T.
 APPROVED BY _____

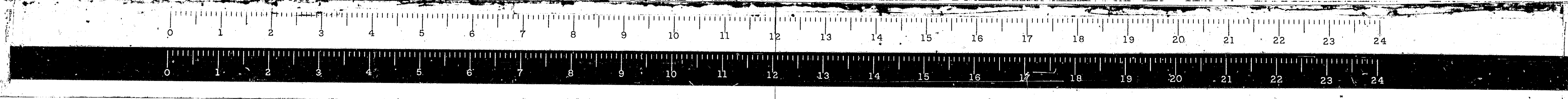
FOR CONTINUATION AT MATCH LINES, SEE SHEET 43.

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

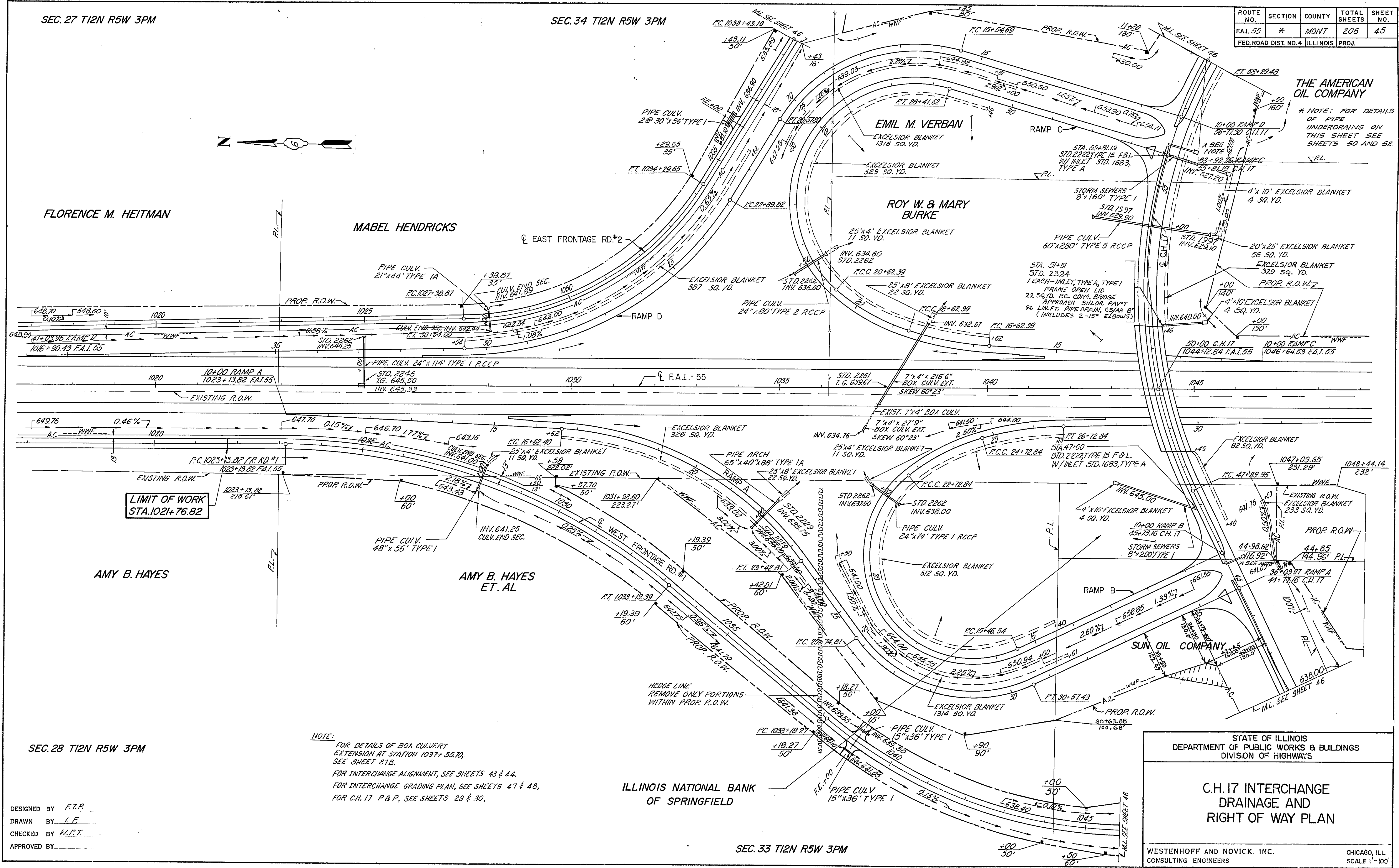
CH.17 INTERCHANGE ALIGNMENT PLAN

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

CHICAGO, ILL.
 SCALE 1"=100'

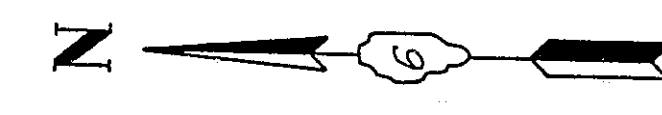


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	* 4	MONT	206	45
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



THE AMERICAN OIL COMPANY

* NOTE: FOR DETAILS OF PIPE UNDERDRAINS ON THIS SHEET SEE SHEETS 50 AND 52.



FLORENCE M. HEITMAN

MABEL HENDRICKS

ROY W. & MARY BURKE

AMY B. HAYES

AMY B. HAYES ET. AL

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

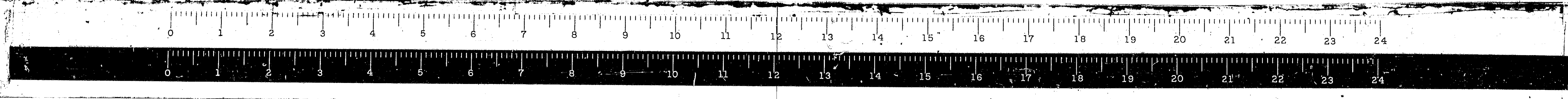
C.H. 17 INTERCHANGE
DRAINAGE AND
RIGHT OF WAY PLAN

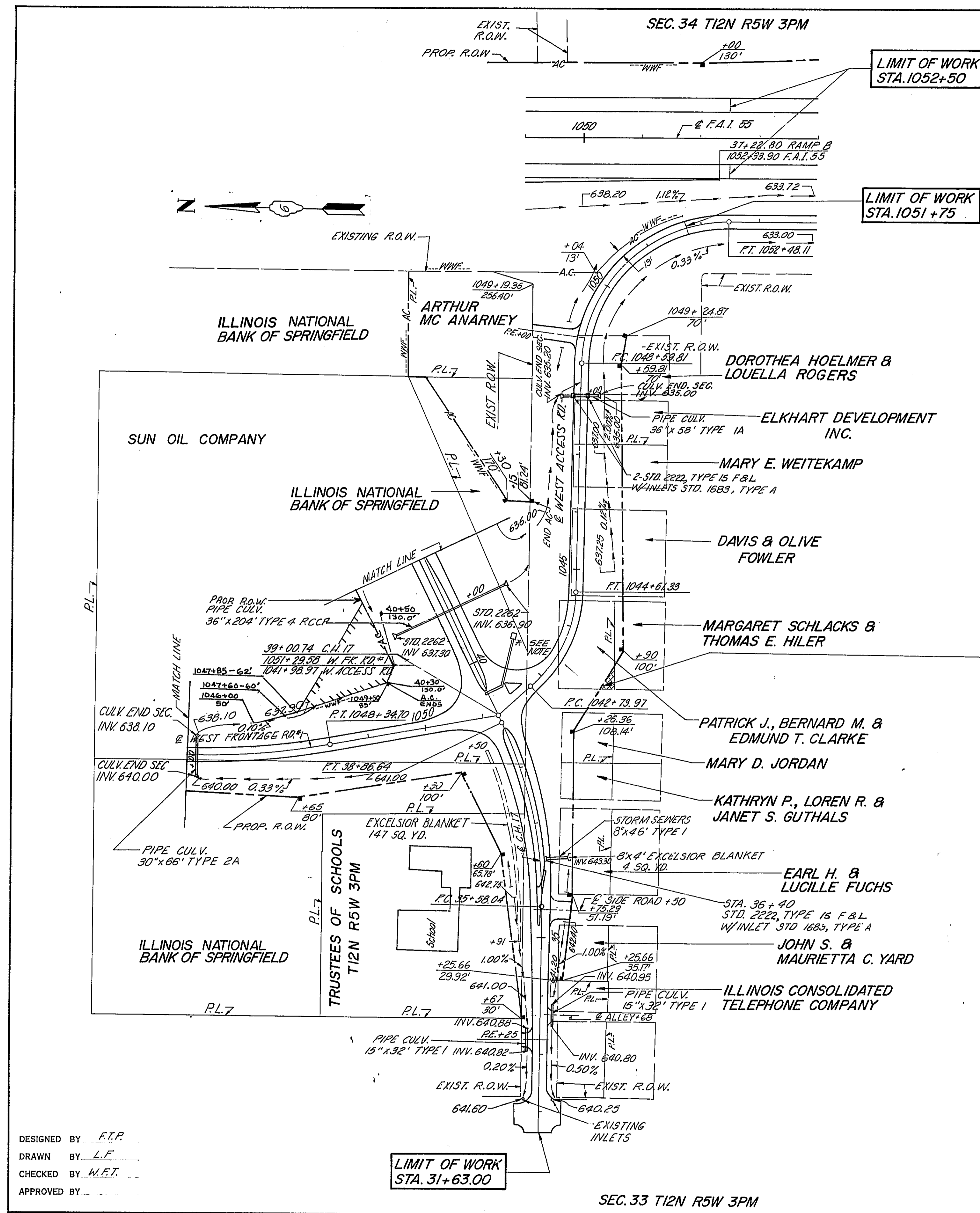
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.
SCALE 1" = 100'

NOTE:
FOR DETAILS OF BOX CULVERT
EXTENSION AT STATION 1037+55.70,
SEE SHEET 81B.
FOR INTERCHANGE ALIGNMENT, SEE SHEETS 43 & 44.
FOR INTERCHANGE GRADING PLAN, SEE SHEETS 47 & 48,
FOR C.H. 17 P & P, SEE SHEETS 29 & 30.

DESIGNED BY F.T.P.
DRAWN BY L.F.
CHECKED BY M.E.T.
APPROVED BY

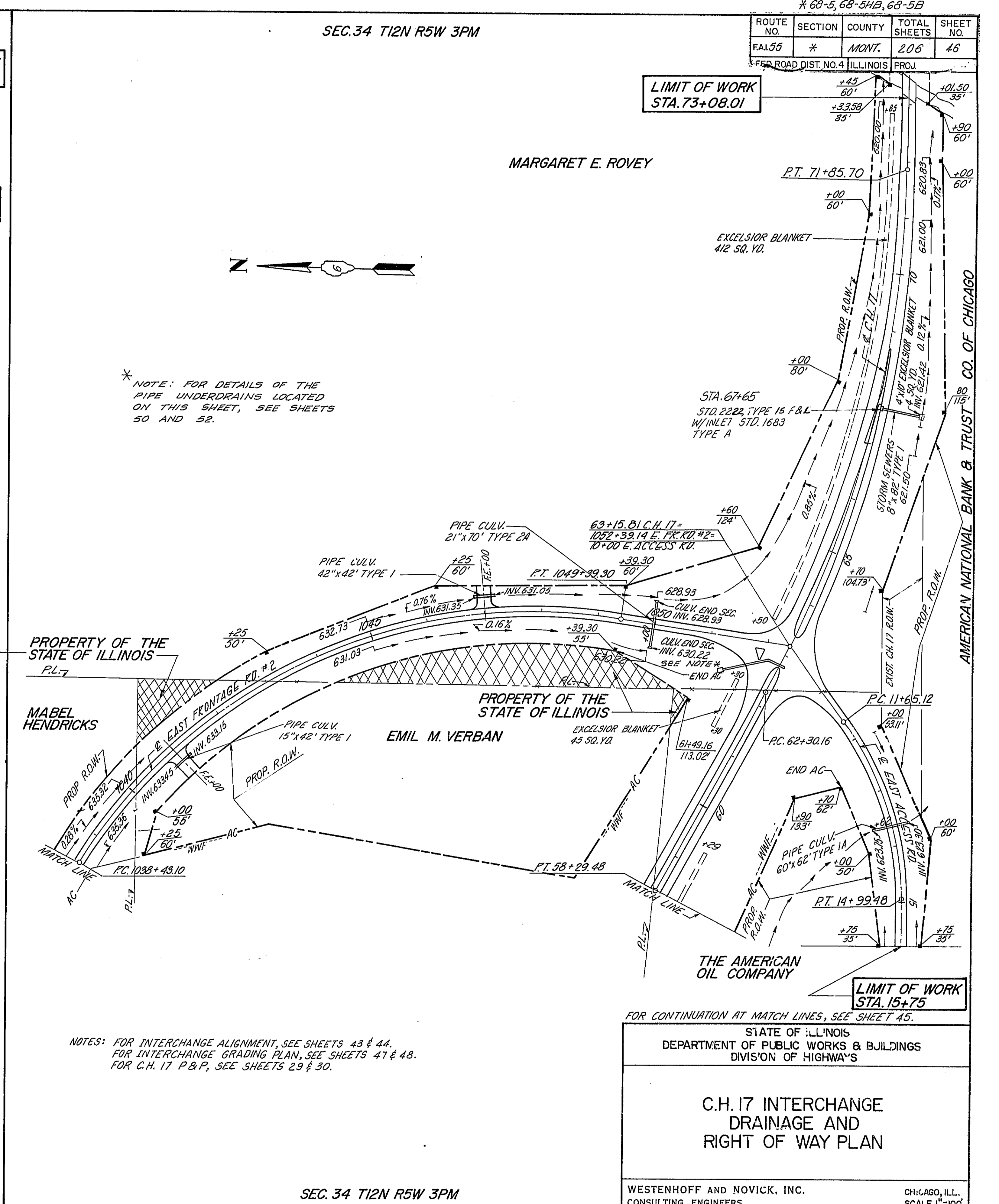
ILLINOIS NATIONAL BANK
OF SPRINGFIELD





DESIGNED BY *F.T.P.*
 DRAWN BY *L.F.*
 CHECKED BY *M.E.T.*
 APPROVED BY _____

SEC. 33 T12N R5W 3PM



NOTES: FOR INTERCHANGE ALIGNMENT, SEE SHEETS 43 & 44.
 FOR INTERCHANGE GRADING PLAN, SEE SHEETS 47 & 48.
 FOR C.H. 17 P&P, SEE SHEETS 29 & 30.

SEC. 34 T12N R5W 3PM

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-5	68-54B	ILLINOIS	46	46

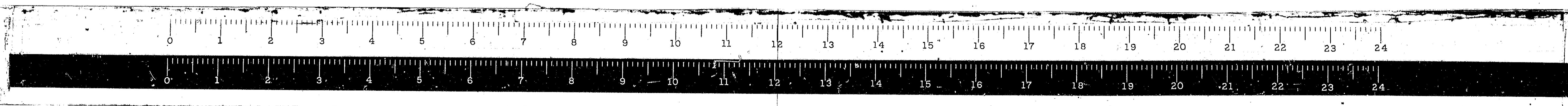
LIMIT OF WORK STA. 73+08.01

LIMIT OF WORK STA. 15+75

FOR CONTINUATION AT MATCH LINES, SEE SHEET 45.
 STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

C.H. 17 INTERCHANGE
 DRAINAGE AND
 RIGHT OF WAY PLAN

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS
 CHICAGO, ILL.
 SCALE 1"=100'



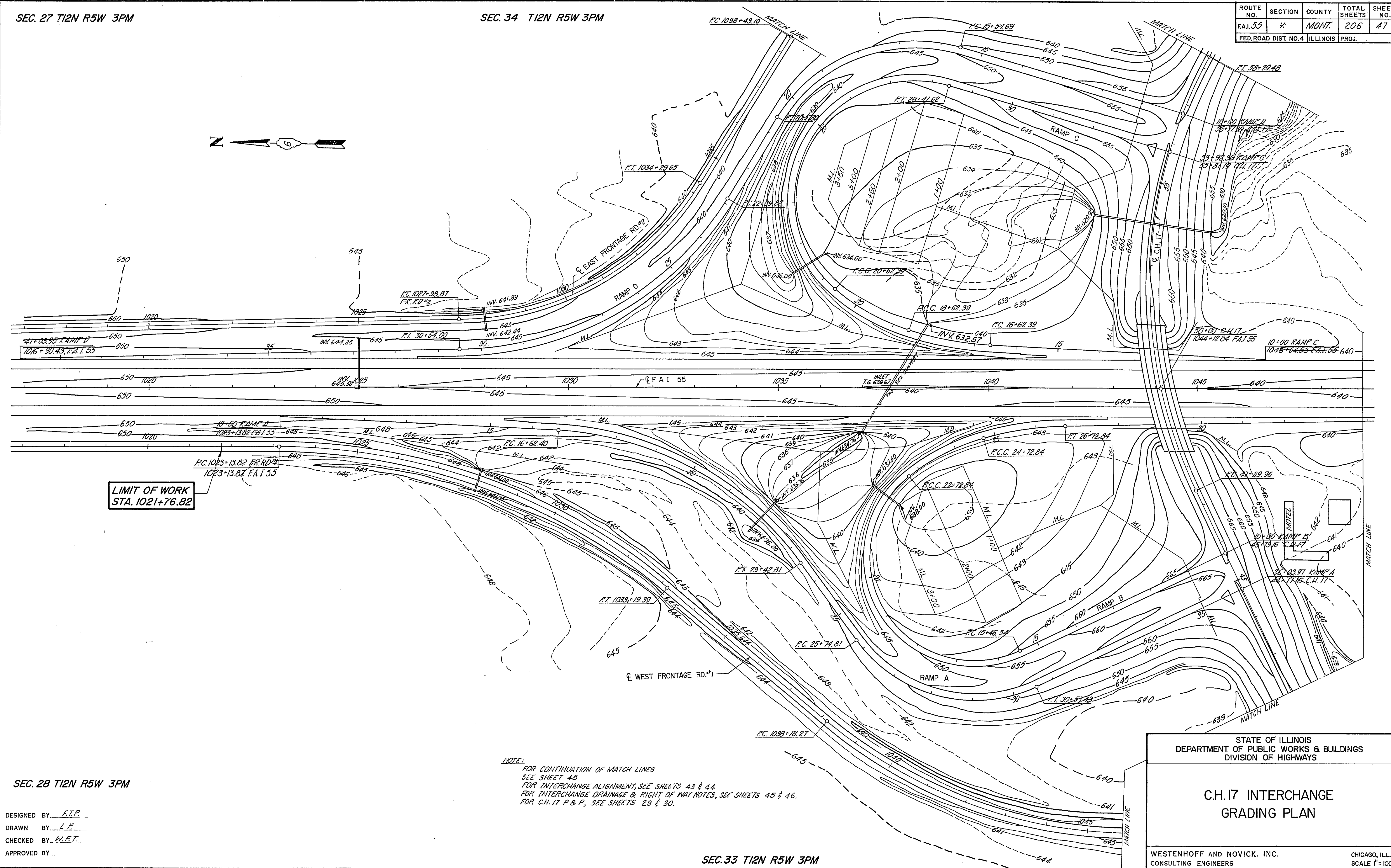
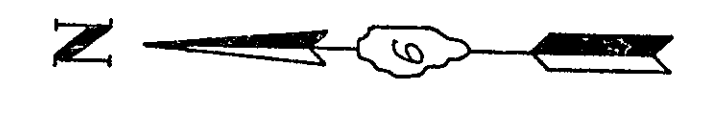
*68-5, 68-5B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	47
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

SEC. 27 T12N R5W 3PM

SEC. 34 T12N R5W 3PM

SEC. 33 T12N R5W 3PM



LIMIT OF WORK
STA. 1021+76.82

NOTE:
FOR CONTINUATION OF MATCH LINES
SEE SHEET 48
FOR INTERCHANGE ALIGNMENT, SEE SHEETS 43 & 44.
FOR INTERCHANGE DRAINAGE & RIGHT OF WAY NOTES, SEE SHEETS 45 & 46.
FOR C.H. 17 P & P, SEE SHEETS 29 & 30.

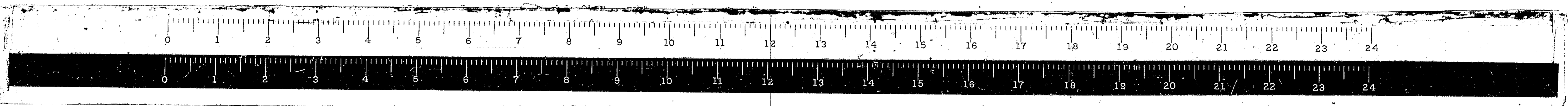
SEC. 28 T12N R5W 3PM

DESIGNED BY L.T.P.
DRAWN BY L.F.
CHECKED BY M.E.T.
APPROVED BY _____

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

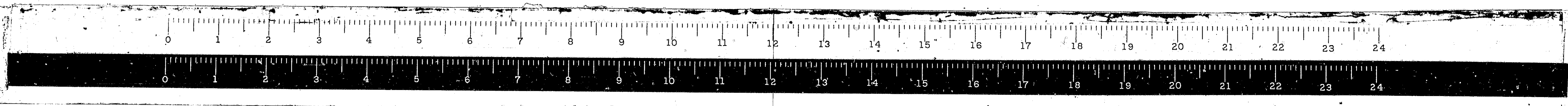
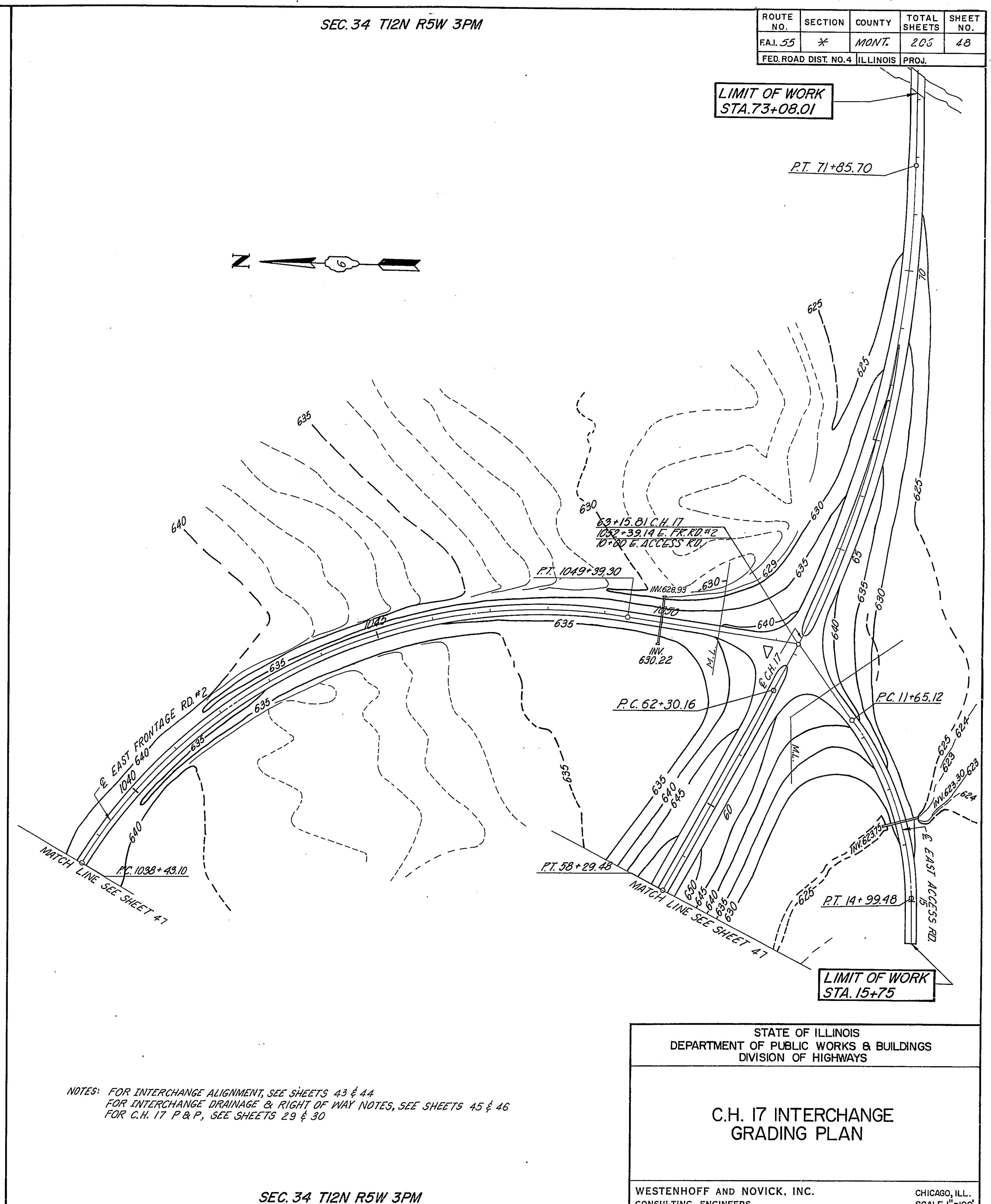
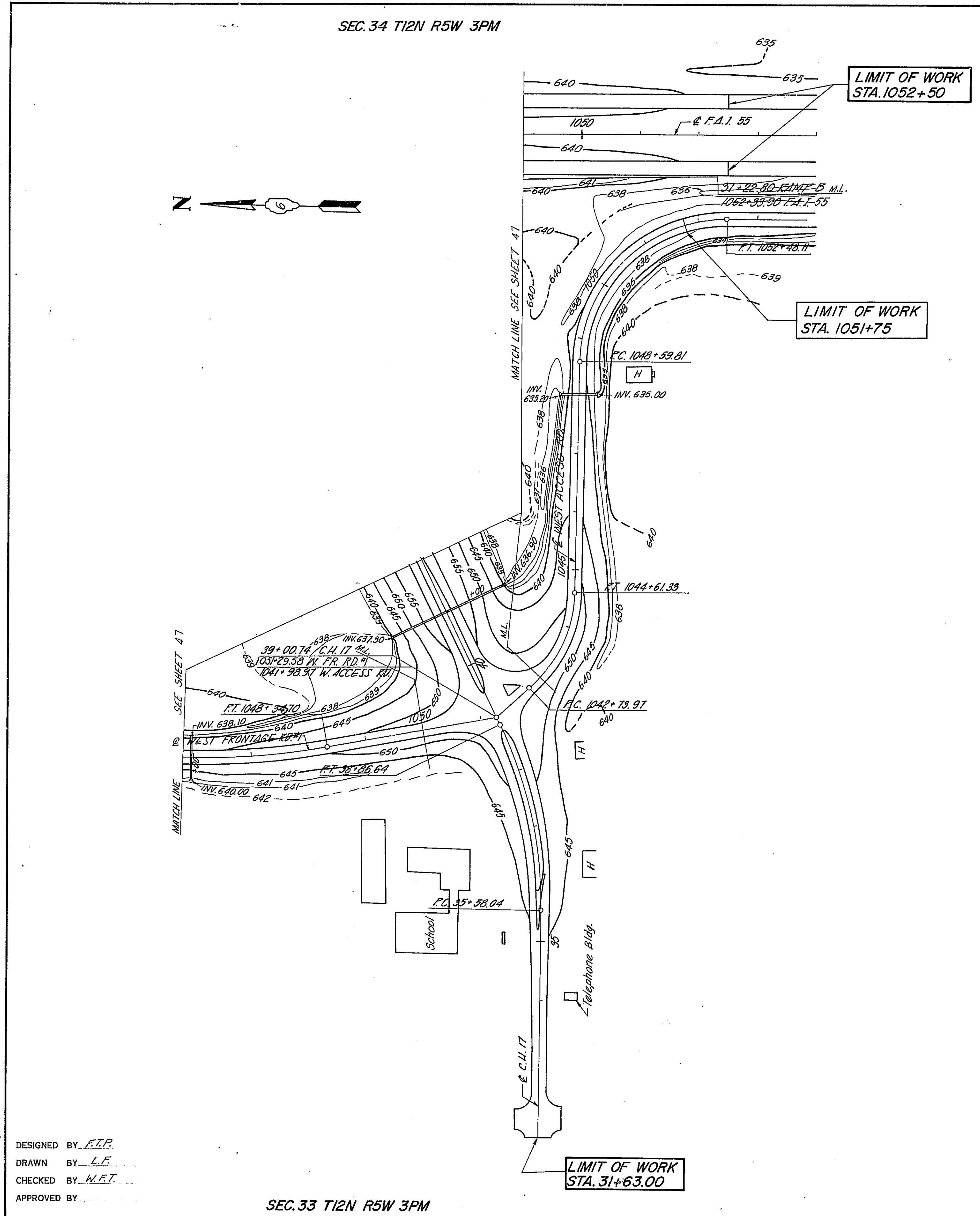
**C.H.17 INTERCHANGE
GRADING PLAN**

WESTENHOFF AND NOVICK, INC. CHICAGO, ILL.
CONSULTING ENGINEERS SCALE 1"=100'

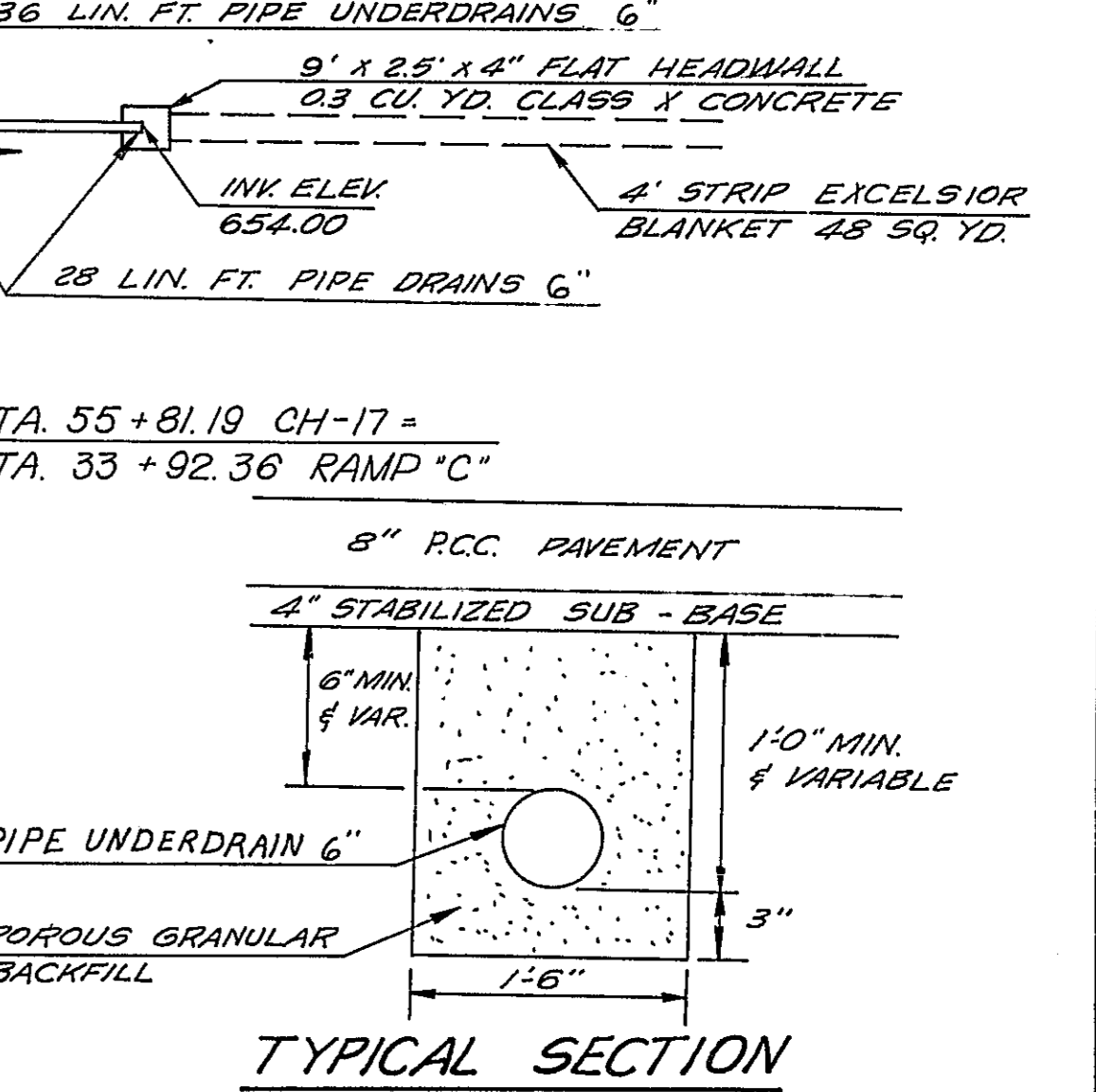
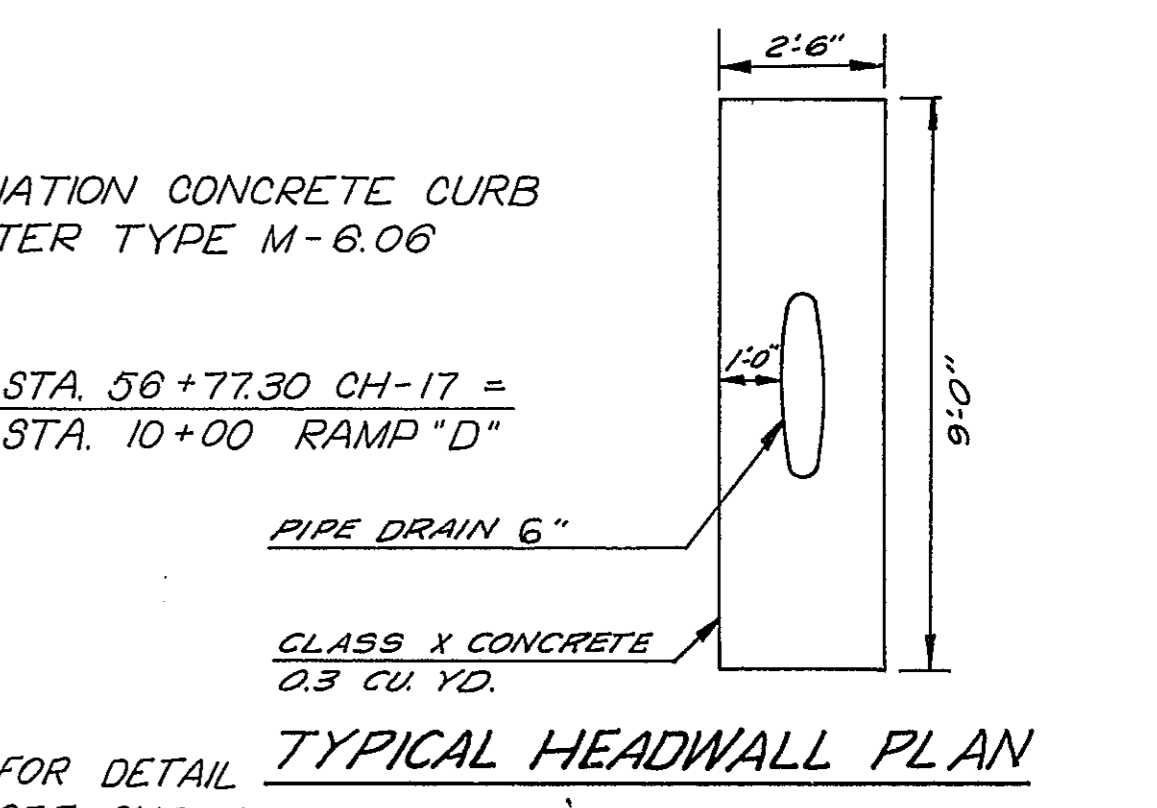
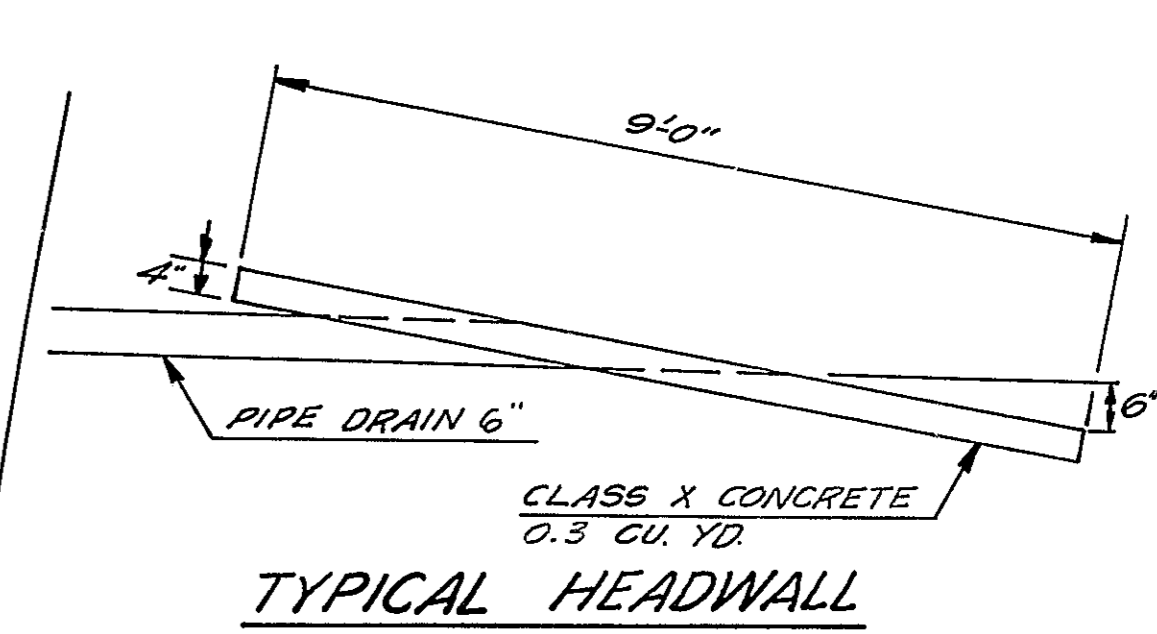
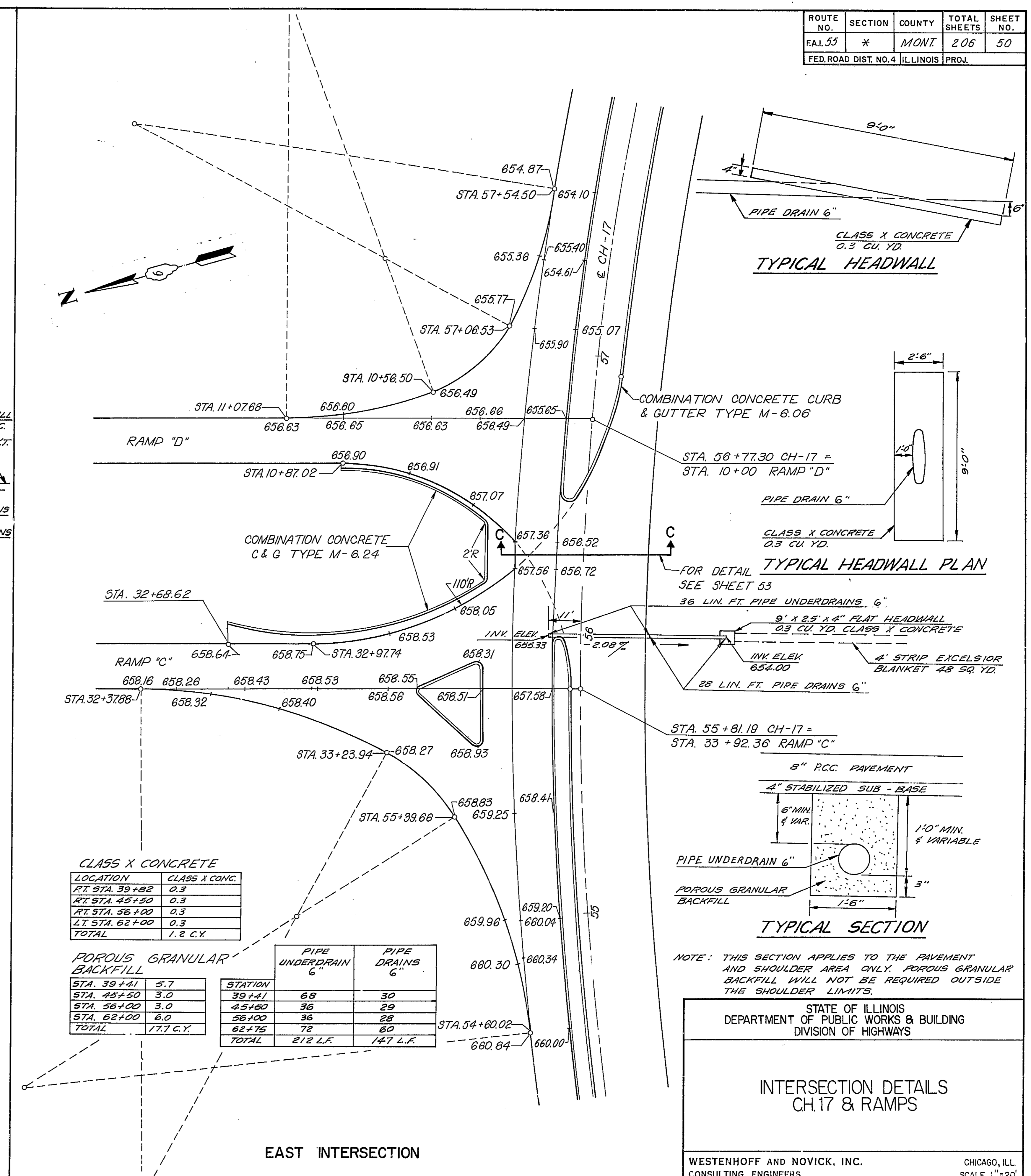
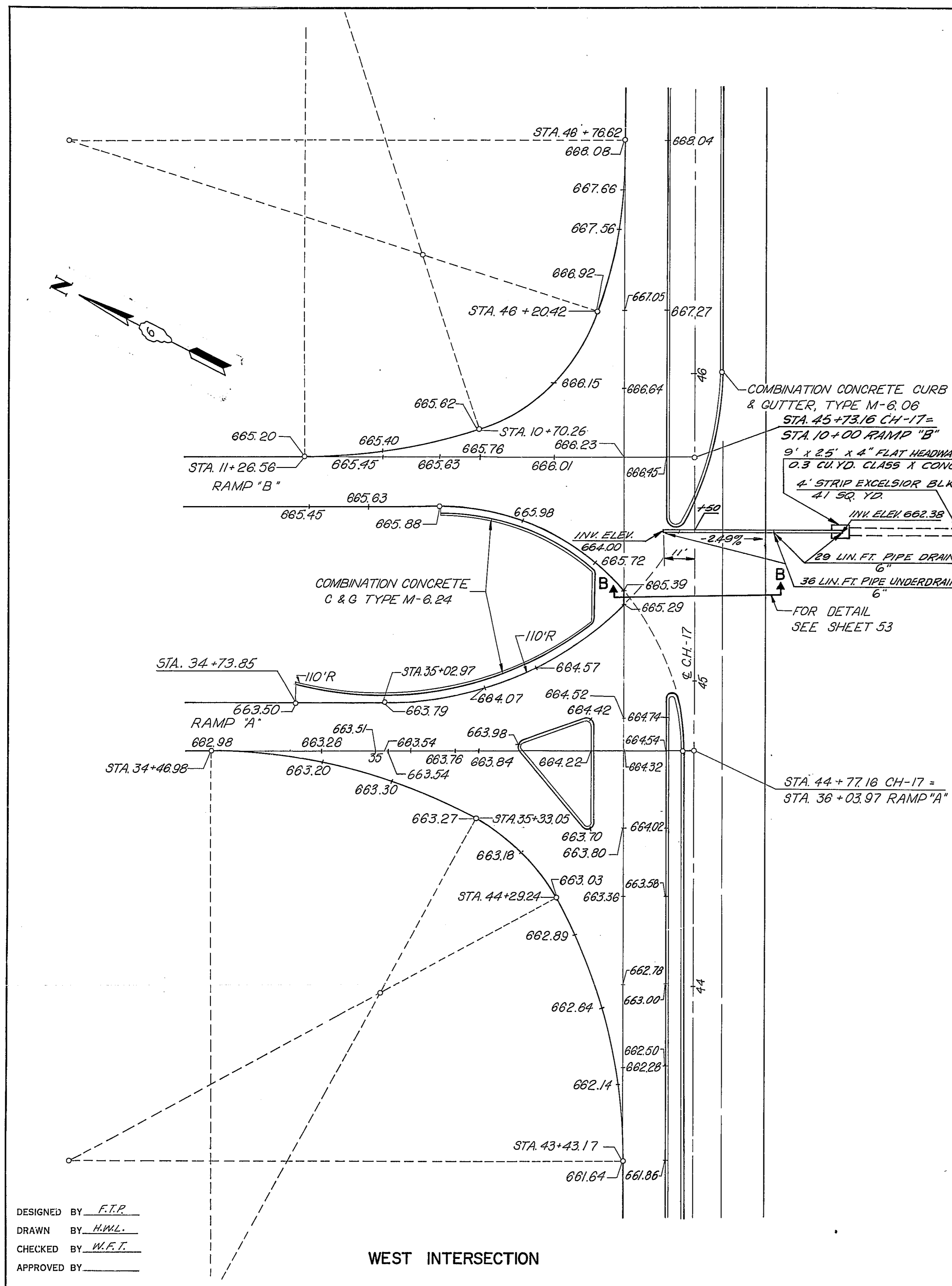


* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	205	48
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	50
FED. ROAD DIST. NO. 4			ILLINOIS PROJ.	



NOTE: THIS SECTION APPLIES TO THE PAVEMENT AND SHOULDER AREA ONLY. POROUS GRANULAR BACKFILL WILL NOT BE REQUIRED OUTSIDE THE SHOULDER LIMITS.

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDING
 DIVISION OF HIGHWAYS

**INTERSECTION DETAILS
 CH.17 & RAMPS**

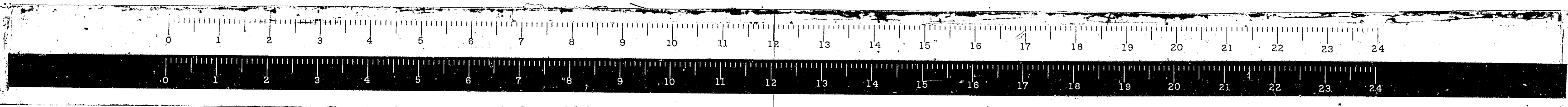
WESTENHOFF AND NOVICK, INC. CHICAGO, ILL.
 CONSULTING ENGINEERS SCALE 1"=20'

CLASS X CONCRETE

LOCATION	CLASS X CONC.
RT STA. 39+82	0.3
RT STA. 45+80	0.3
RT STA. 56+00	0.3
LT STA. 62+00	0.3
TOTAL	1.2 C.Y.

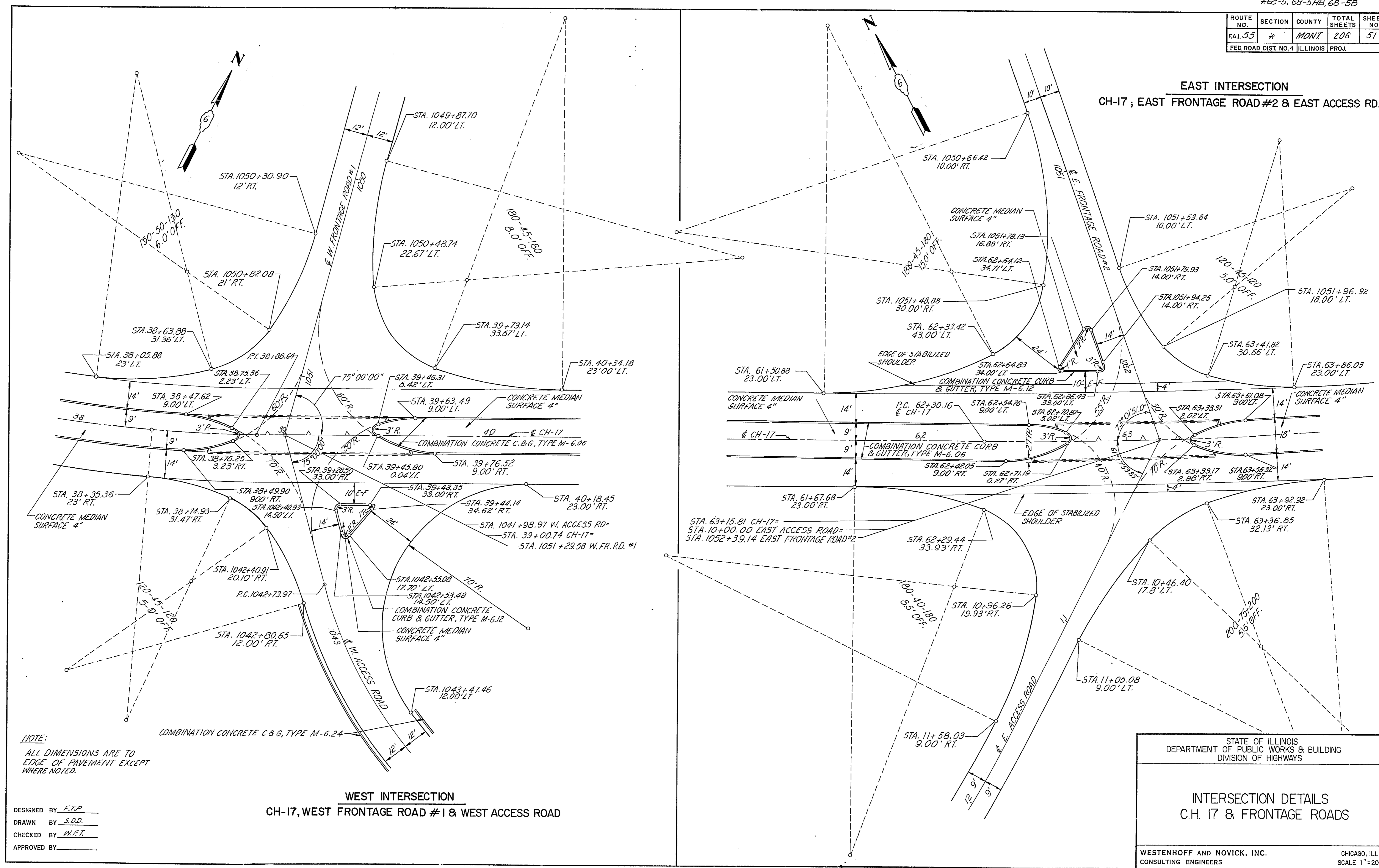
POROUS GRANULAR BACKFILL

STATION	STATION	PIPE UNDERDRAIN 6"	PIPE DRAINS 6"
STA. 39+41	39+41	68	30
STA. 45+80	45+80	36	29
STA. 56+00	56+00	36	28
STA. 62+00	62+75	72	60
TOTAL	TOTAL	212 L.F.	147 L.F.



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-5	#	MONT.	206	51
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

EAST INTERSECTION
CH-17; EAST FRONTAGE ROAD #2 & EAST ACCESS RD.



NOTE:
ALL DIMENSIONS ARE TO
EDGE OF PAVEMENT EXCEPT
WHERE NOTED.

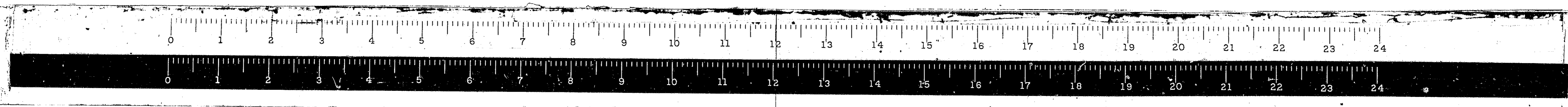
DESIGNED BY F.T.P.
DRAWN BY S.D.D.
CHECKED BY M.E.E.
APPROVED BY _____

WEST INTERSECTION
CH-17, WEST FRONTAGE ROAD #1 & WEST ACCESS ROAD

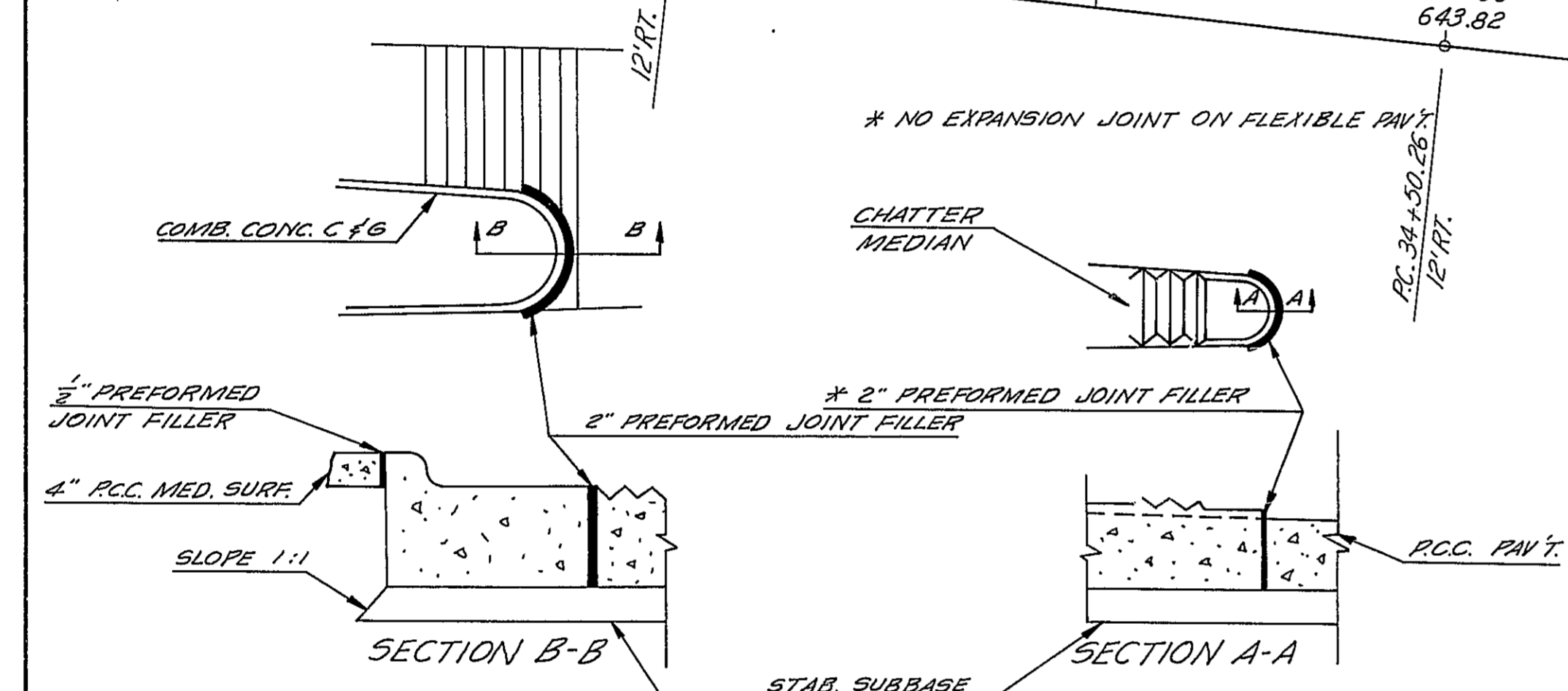
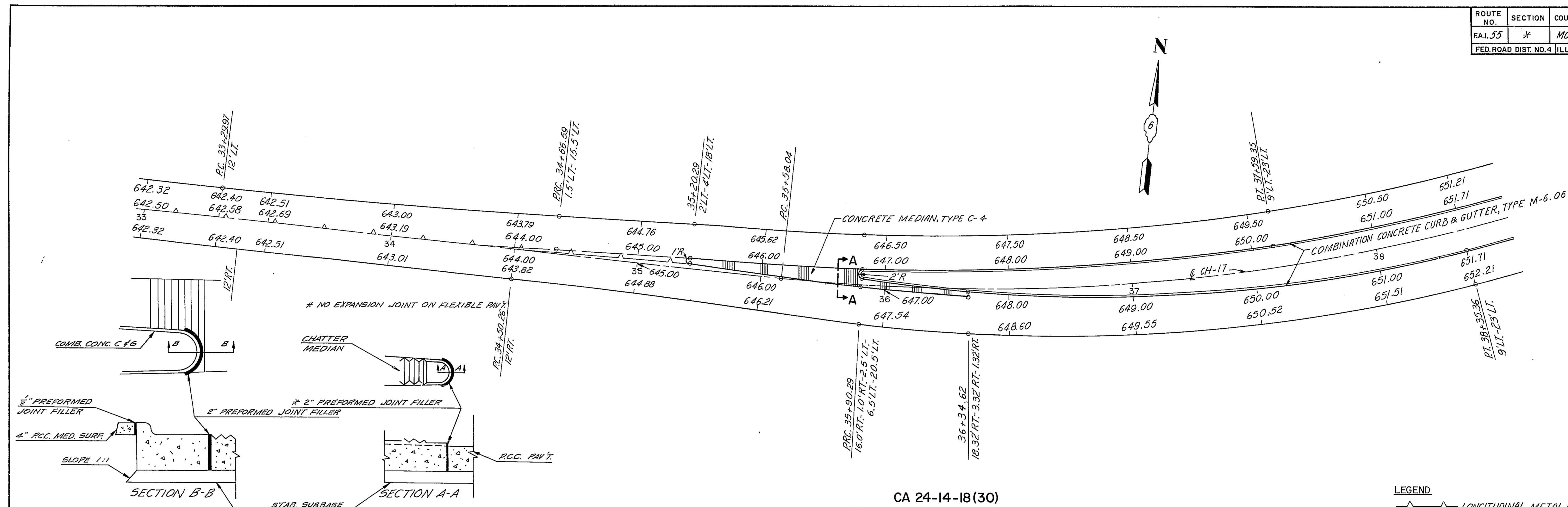
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDING
DIVISION OF HIGHWAYS

INTERSECTION DETAILS
C.H. 17 & FRONTAGE ROADS

WESTENHOFF AND NOVICK, INC. CHICAGO, ILL.
CONSULTING ENGINEERS SCALE 1" = 20'



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 35	*	MONT.	206	53
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

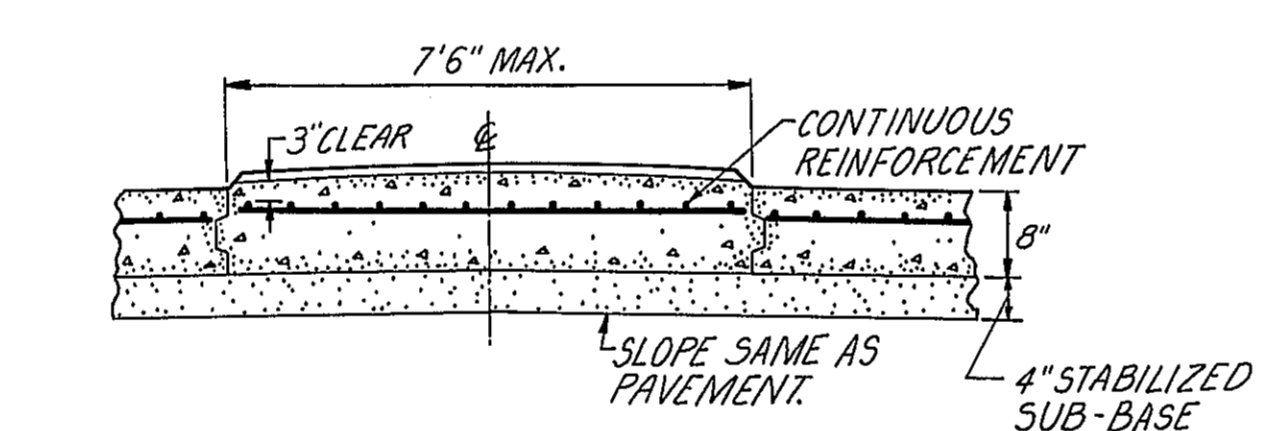


DETAIL OF EXPANSION JOINTS AT CHANNELIZED APPROACHES AND BETWEEN P.C.C. MEDIAN SURFACE AND CURB.

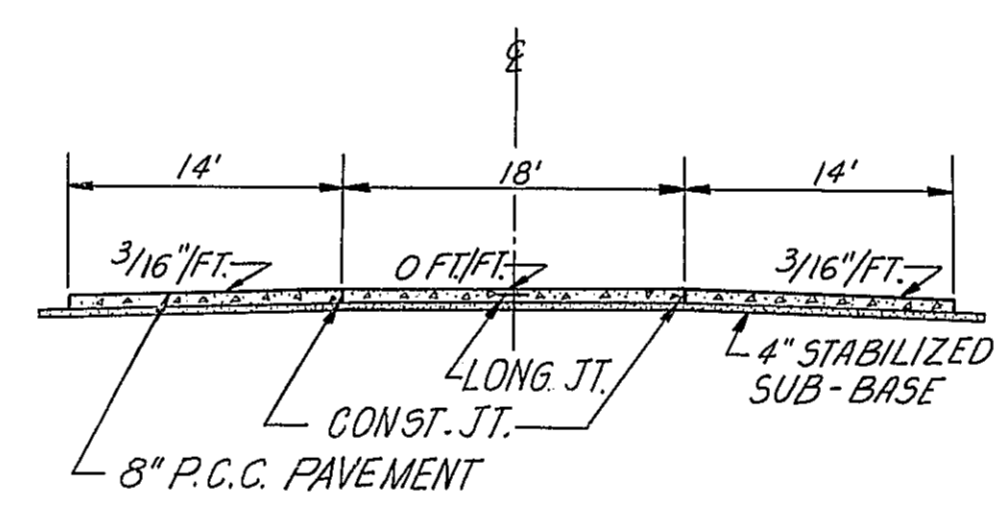
CA 24-14-18(30)

LEGEND
 LONGITUDINAL METAL OR SAWED JOINT.

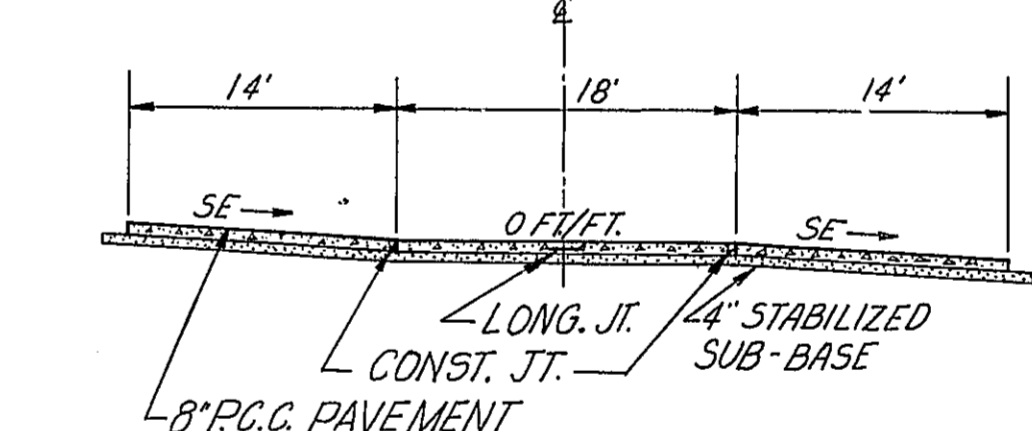
STATION	OFFSET TO LEFT		OFFSET TO RIGHT	
	E.P.	M.E.P.	M.E.P.	E.P.
33+29.97	12.00			12.00
+50	12.08			12.00
+75	12.38			12.00
34+00	12.92			12.00
+25	13.69			12.00
+50	14.70			12.00
+75	15.92			12.13
35+00	17.11			12.50
+25	18.19	4.19	-1.86	13.14
+50	19.17	5.17	-0.97	14.03
+75	20.03	6.03	0.17	15.17
36+00	20.78	6.78	1.54	16.54
+25	21.42	7.42	2.84	17.84
+50	21.95	7.95	2.43	19.00
+75	22.38	8.38	4.08	20.00
37+00	22.69	8.69	5.50	20.87
+25	22.90	8.90	6.67	21.58
+50	22.99	8.99	7.61	22.15
+75	23.00	9.00	8.30	22.58
38+00	23.00	9.00	8.76	22.85
+25	23.00	9.00	8.98	22.99
+50	23.00	9.00	9.00	23.00



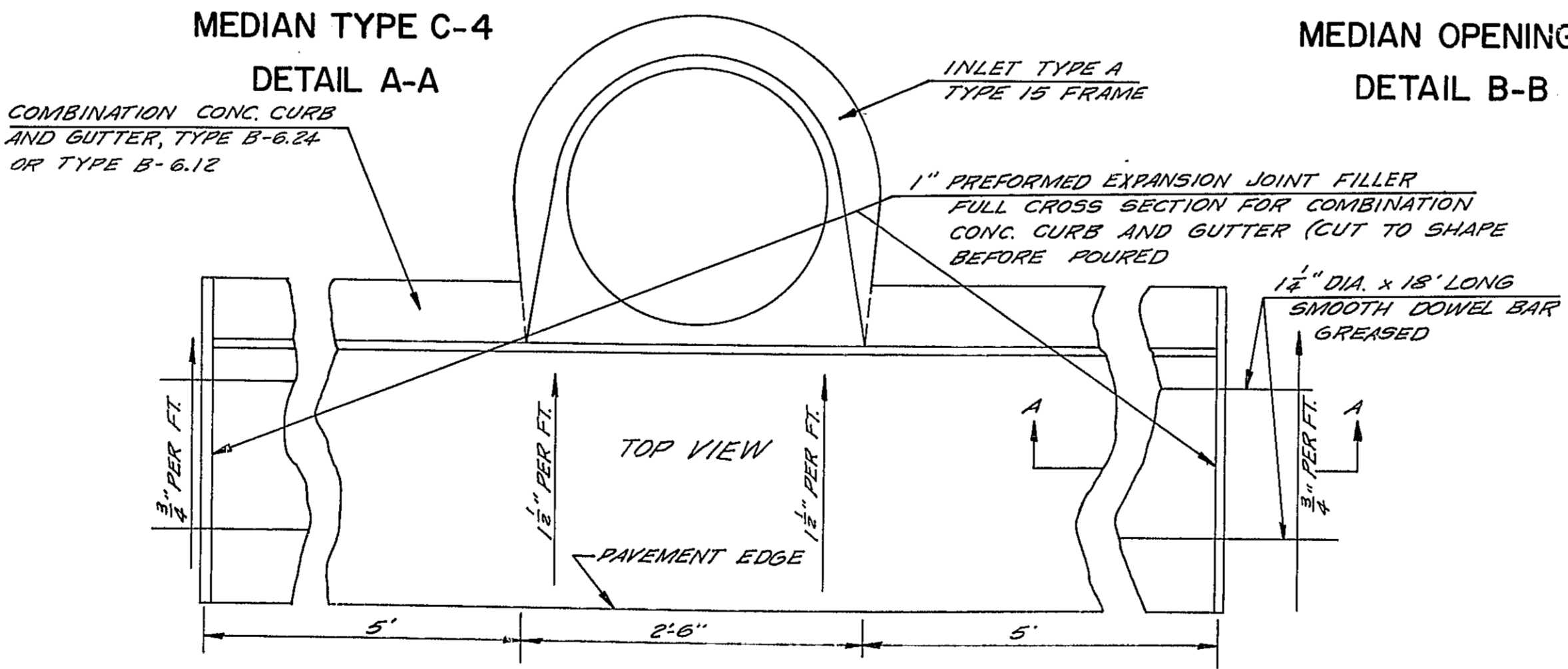
TYPICAL TRANSVERSE SECTION OF MEDIAN TYPE C-4



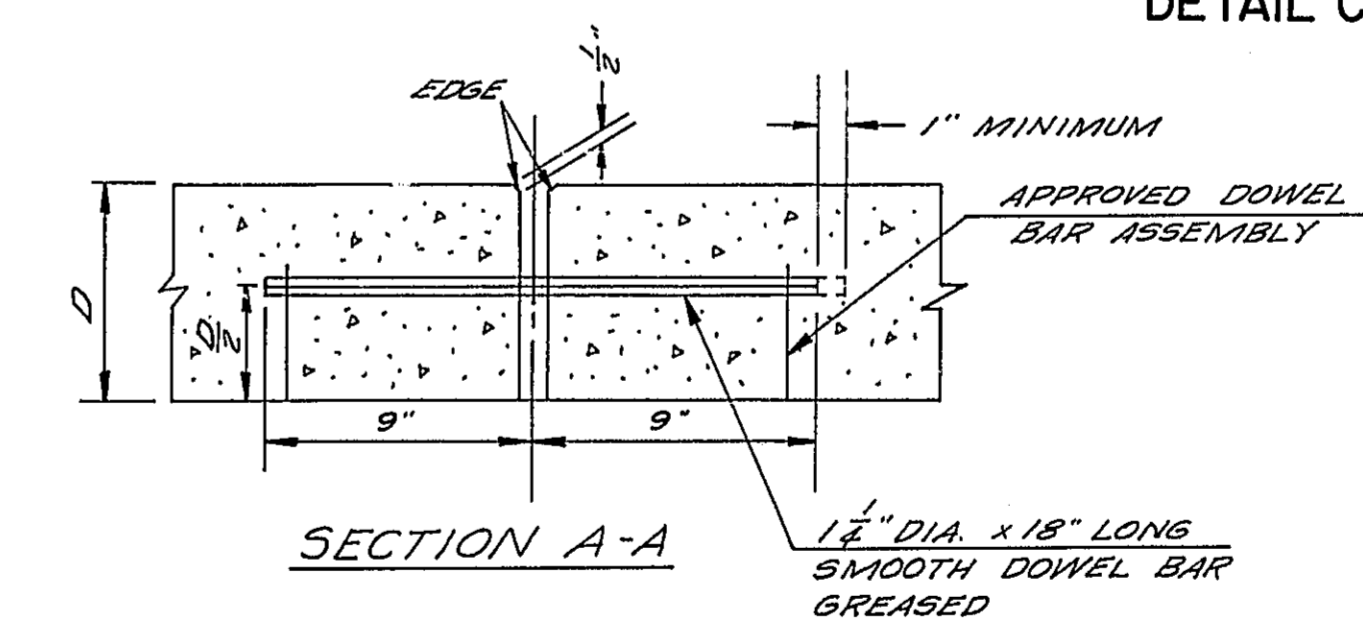
CH. 17 NORMAL MEDIAN OPENING DETAIL B-B



CH. 17 SE RT. OR LT. MEDIAN OPENING DETAIL C-C



DETAIL A-A
 THE CONSTRUCTION OF EXPANSION JOINT SHALL BE INCIDENTAL TO THE CONTRACT UNIT PRICE FOR COMBINATION CONCRETE CURB AND GUTTER OF THE TYPE SPECIFIED.



DETAIL OF CURB AND GUTTER EXPANSION

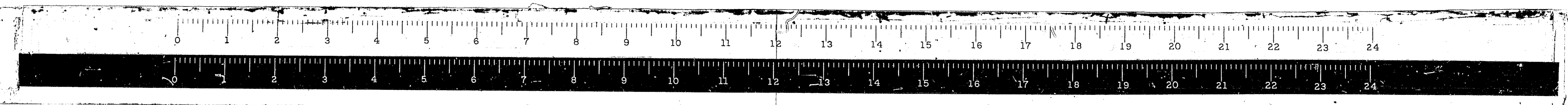
DESIGNED BY F.T.P.
 DRAWN BY S.D.D.
 CHECKED BY W.F.T.
 APPROVED BY _____

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDING
 DIVISION OF HIGHWAYS

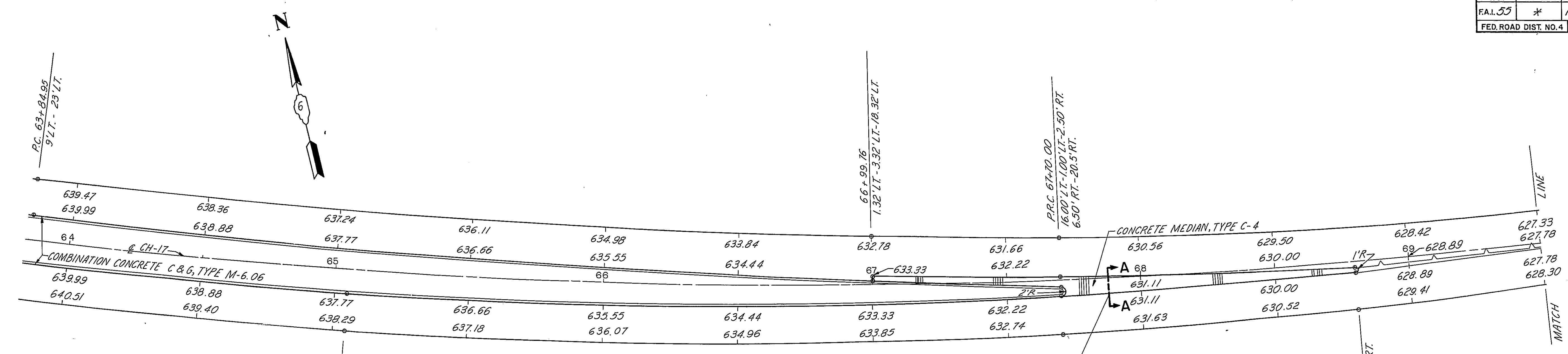
WEST CHANNELIZATION TRANSITION
 COUNTY HIGHWAY 17

WESTENHOFF AND NOVICK, INC.
 CHICAGO, ILL.
 CONSULTING ENGINEERS

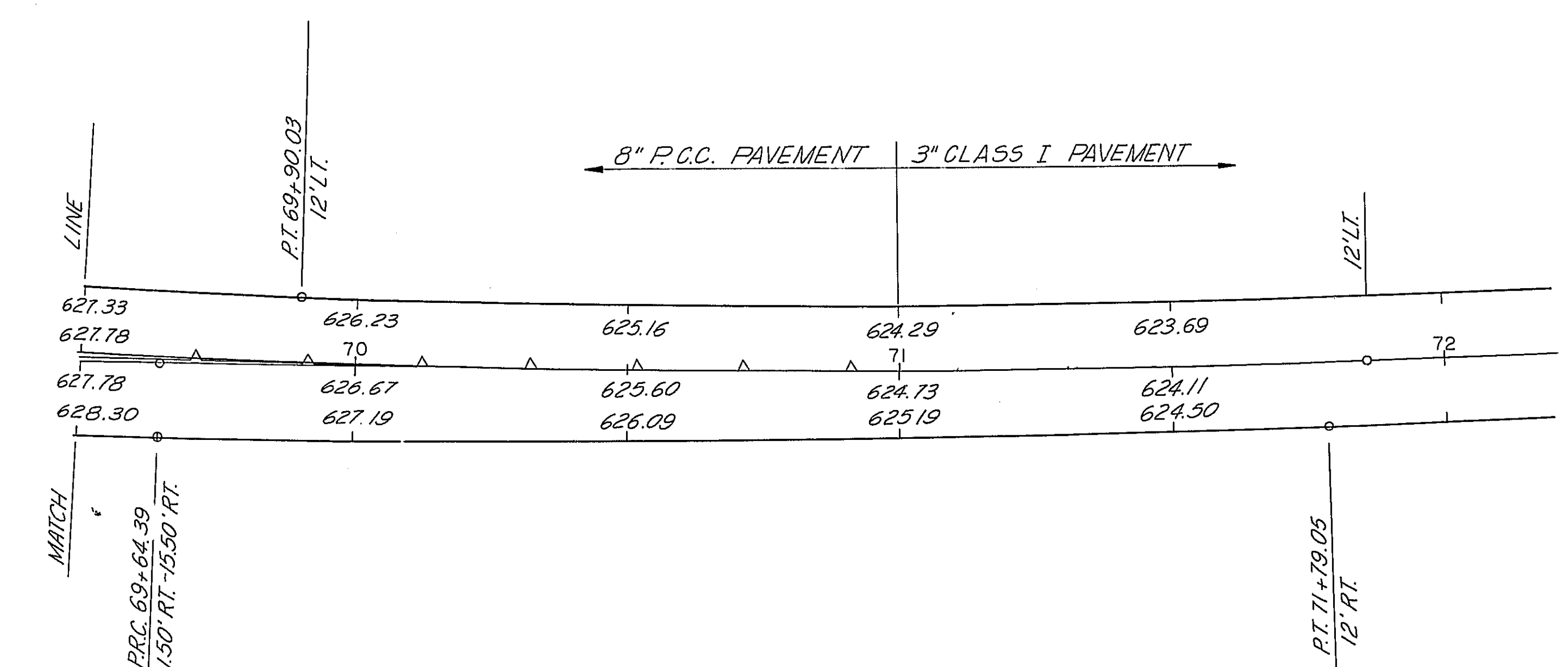
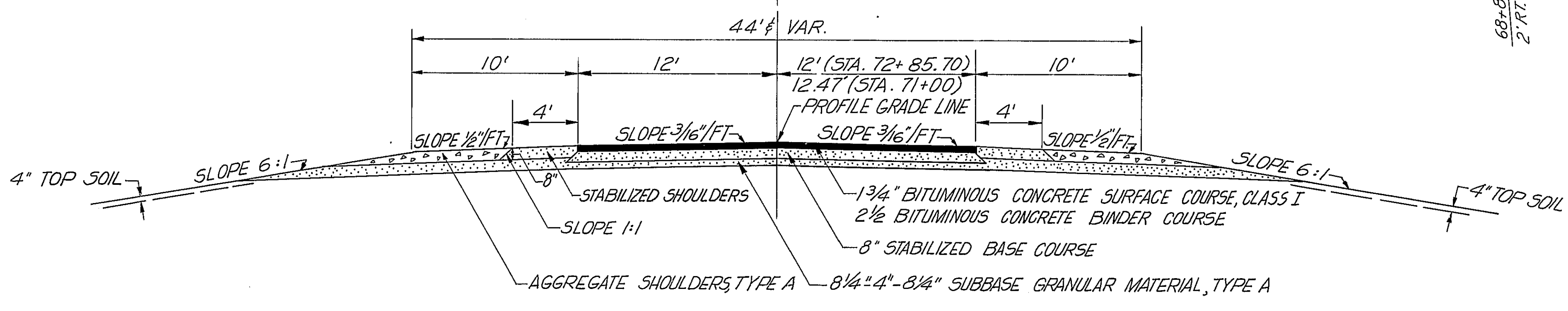
SCALE 1"=20'



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-5	A	MONT.	206	54
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



STATION	OFFSET TO LEFT		OFFSET TO RIGHT	
	E.P.	M.E.P.	M.E.P.	E.P.
63+84.95	23.00	9.00	9.00	23.00
64+00	22.99	8.98	9.00	23.00
+25	22.92	8.88	9.00	23.00
+50	22.80	8.67	9.00	23.00
+75	22.62	8.37	9.00	23.00
65+00	22.37	7.97	9.00	23.00
+25	22.07	7.48	8.98	22.98
+50	21.71	6.89	8.93	22.93
+75	21.30	6.20	8.82	22.82
66+00	20.82	5.42	8.68	22.68
+25	20.28	4.53	8.48	22.48
+50	19.68	3.55	8.25	22.25
+75	19.03	2.48	7.97	21.97
67+00	18.31	3.31	7.64	21.64
+25	17.54	2.54	7.28	21.28
+50	16.71	1.71	6.86	20.86
+75	15.82	0.82	6.40	20.40
68+00	14.98	-0.02	5.90	19.90
+25	14.25	-0.75	5.36	19.36
+50	13.62	-1.38	4.77	18.77
+75	13.09	-1.91	4.13	18.13
69+00	12.67			17.45
+25	12.35			16.73
+50	12.13			15.96
+75	12.02			15.16
70+00	12.00			14.43
+25	12.00			13.80
+50	12.00			13.26
+75	12.00			12.82
71+00	12.00			12.47
+25	12.00			12.22
+50	12.00			12.06
+75	12.00			12.00
+78.05	12.00			12.00



DESIGNED BY... B.J.B.
 DRAWN BY... M.J.D.
 CHECKED BY... E.F.M.
 APPROVED BY... W.F.T.

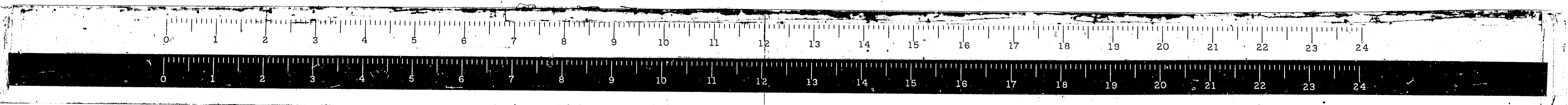
CA 24-14-18(50)

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDING
 DIVISION OF HIGHWAYS

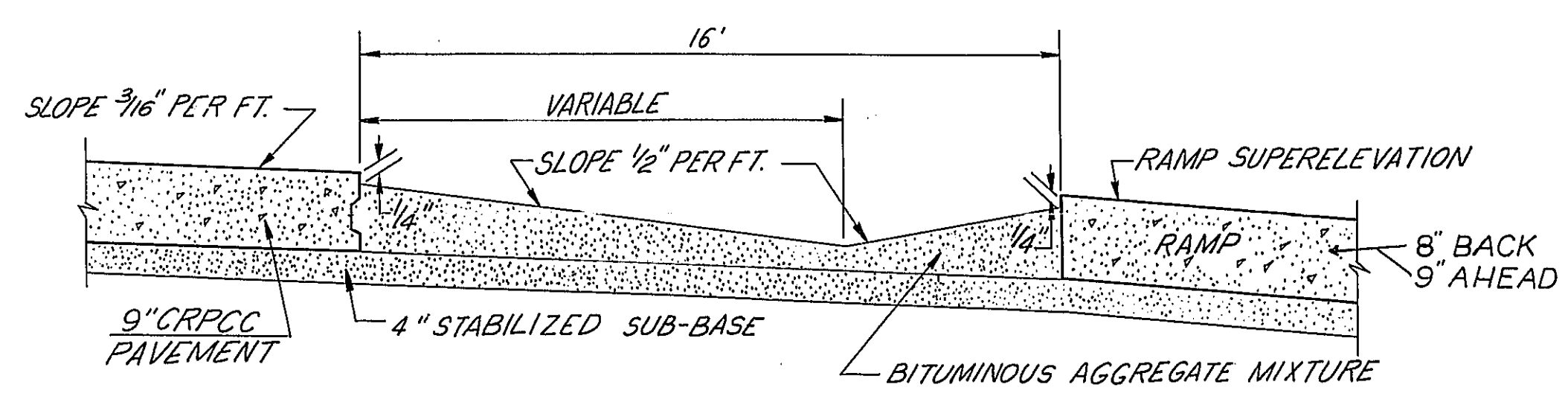
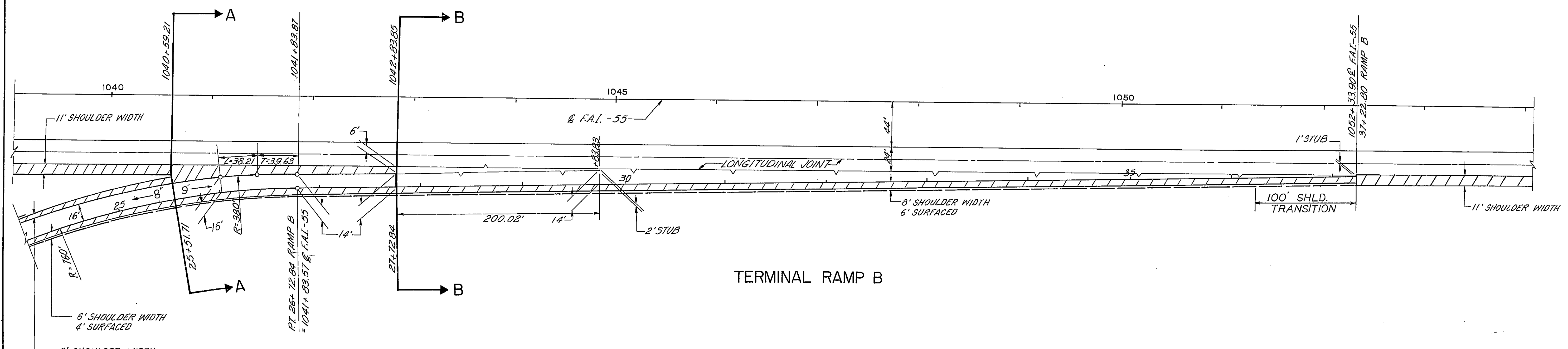
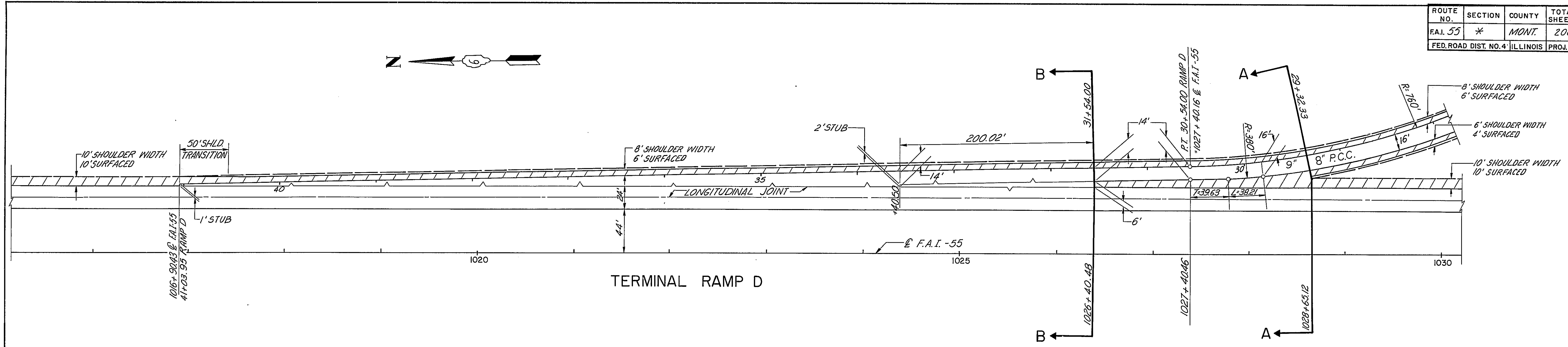
EAST CHANNELIZATION TRANSITION
 COUNTY HIGHWAY 17

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

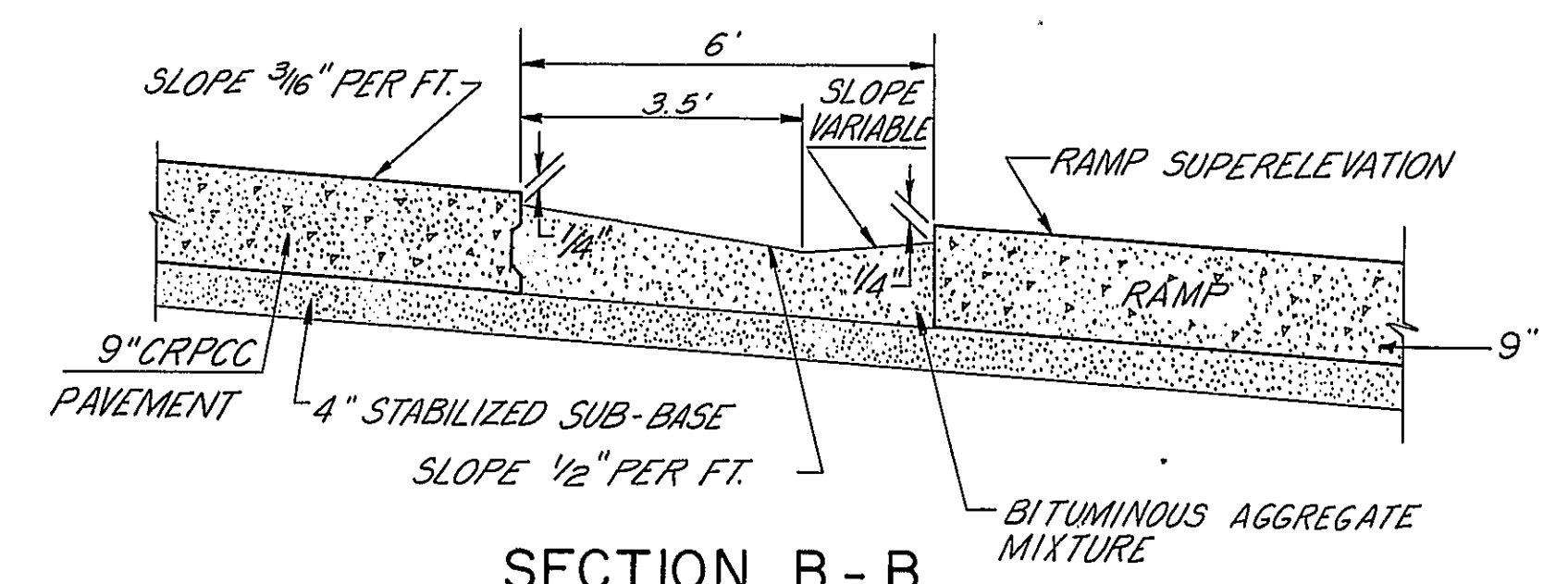
CHICAGO, ILL.
 SCALE 1"=20'



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	55
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



SECTION A-A



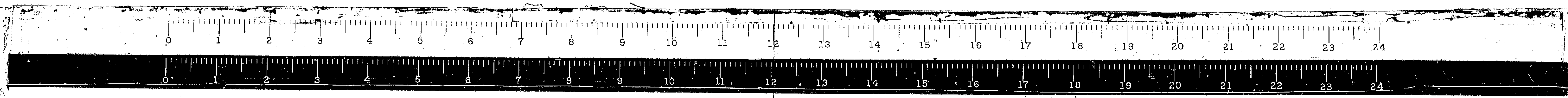
SECTION B-B

DESIGNED BY: F.T.P.
 DRAWN BY: S.D.D.
 CHECKED BY: W.F.T.
 APPROVED BY:

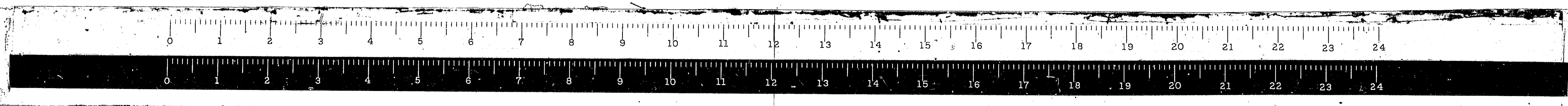
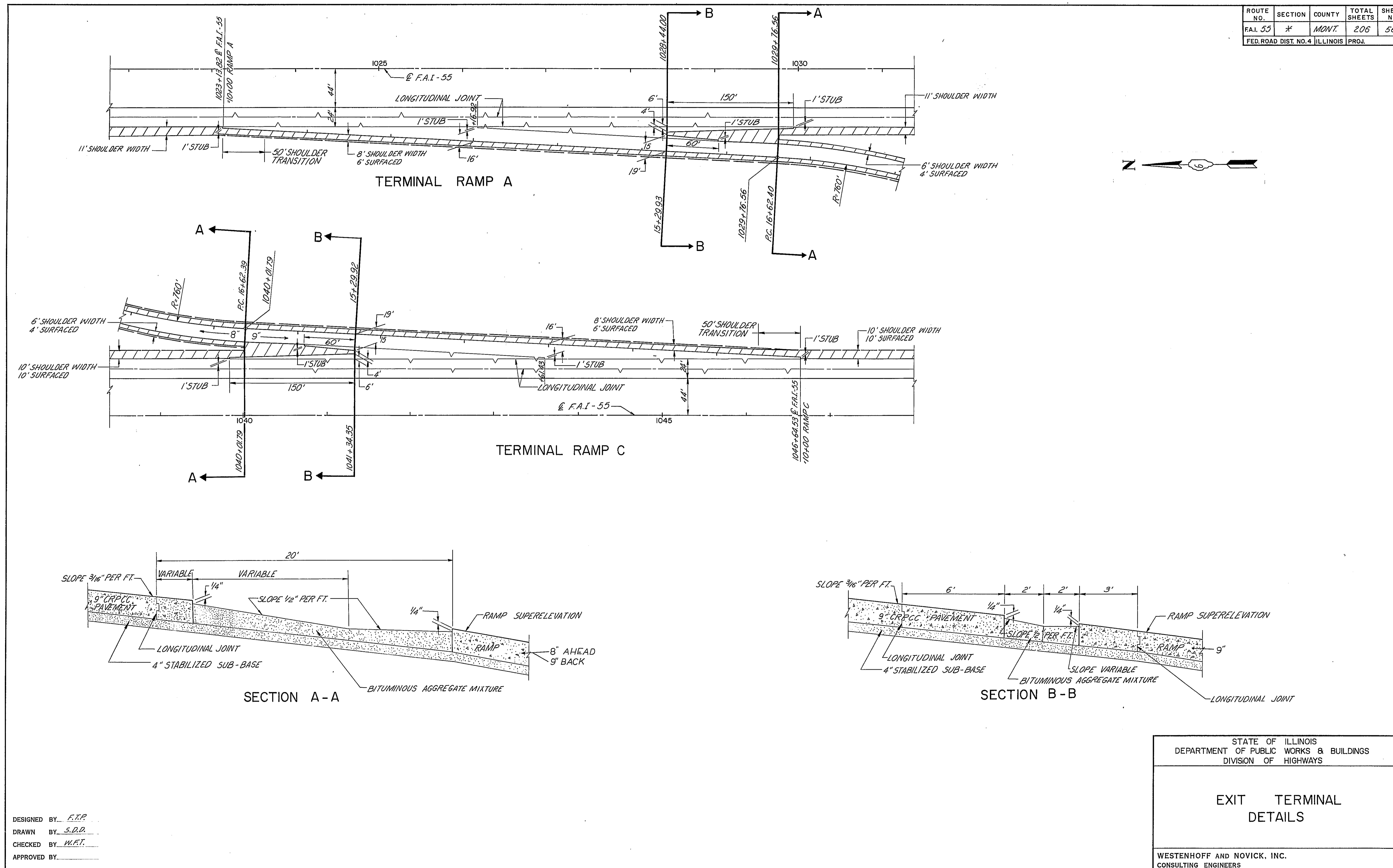
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

**ENTRANCE TERMINAL
 DETAILS**

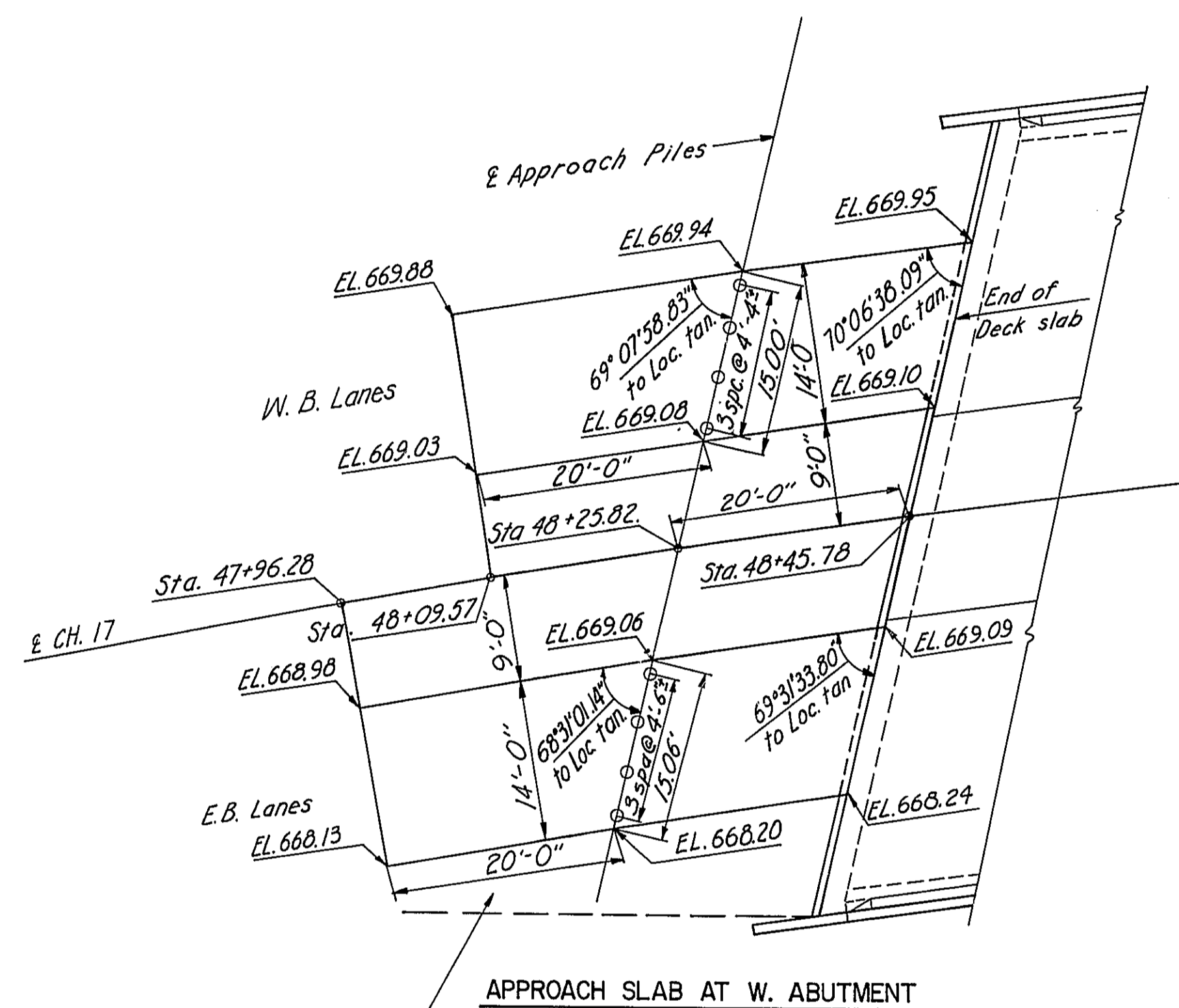
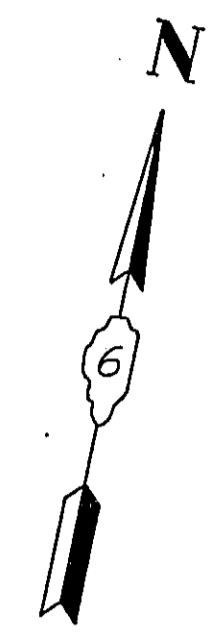
WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	56
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

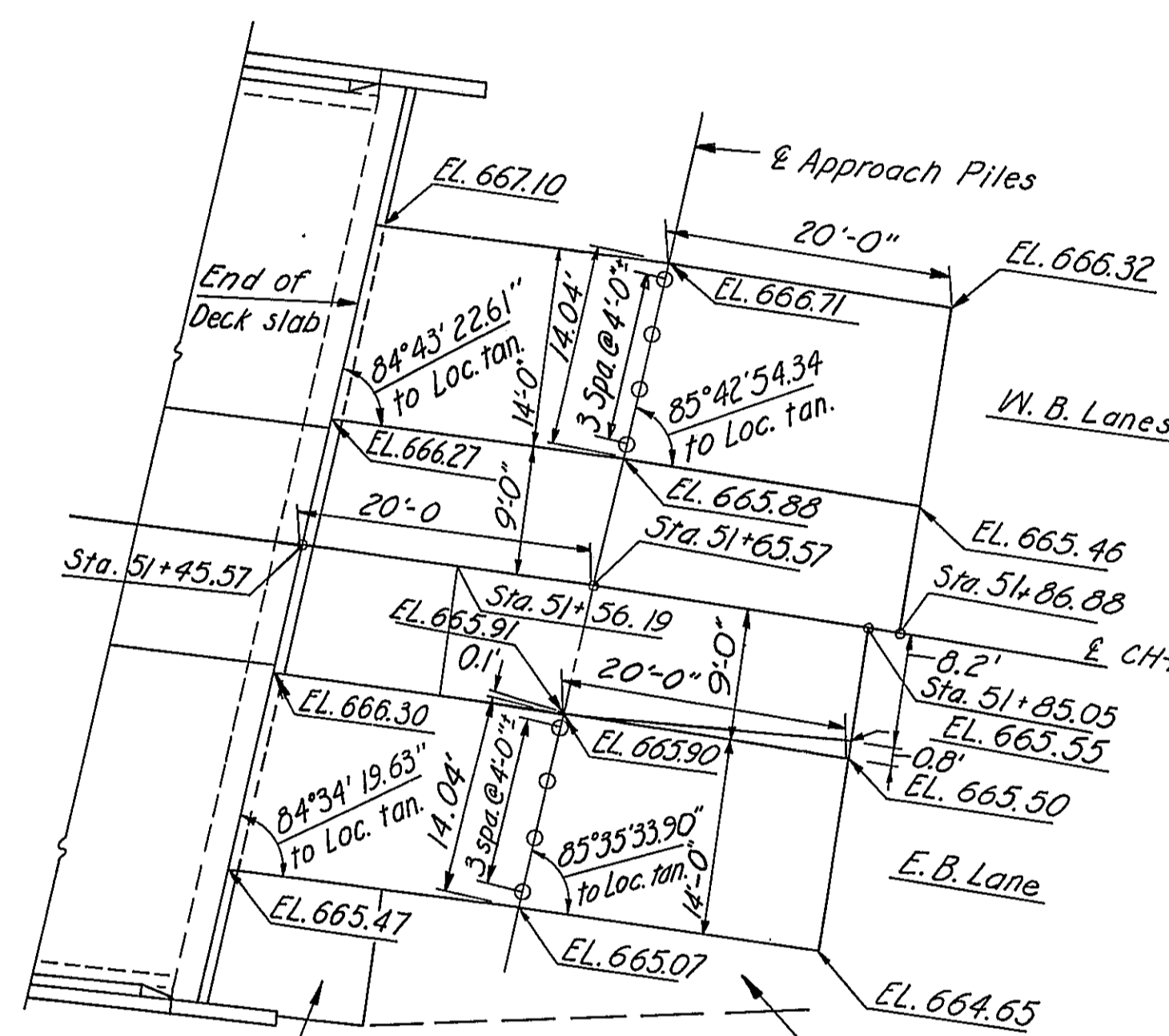


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	57
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



APPROACH SLAB AT W. ABUTMENT

TRANSITION STABILIZED SHOULDERS FROM 4' AT END APPROACH SLAB TO 10' AT BRIDGE ABUTMENT (TYPICAL, EXCEPT AS NOTED).



APPROACH SLAB AT E. ABUTMENT

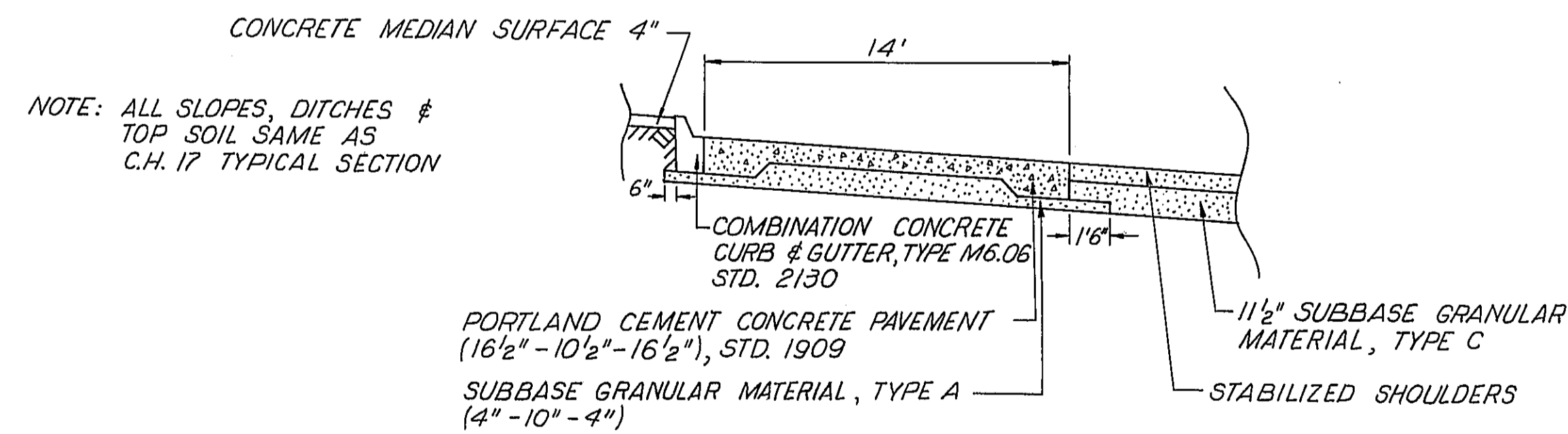
TRANSITION STABILIZED SHOULDERS FROM 4' AT END APPROACH SLAB TO 10' AT END P.C. CONCRETE BRIDGE APPROACH SHOULDER.

QUANTITIES OF MATERIAL

	APPROACH AT W. ABUT.		APPROACH AT E. ABUT.	
	W.B. Lanes	E.B. Lanes	W.B. Lanes	E.B. Lanes
Pavement 16 1/2"-10 1/2"-16 1/2" Method II Sq. Yds.	666	668	628	637
Pavement	No.	62	62	66
	Length	14'-4"	14'-4"	13'-7"
	Weight	927#	927#	935#
Transverse Bars #5 Method II	No.	42 @ 21'-4"	42 @ 21'-4"	42 @ 21'-4"
	Length	16 @ 17'-4"	16 @ 17'-4"	16 @ 17'-4"
	Weight	3133#	3133#	3133#
Longitudinal Bar #8 Method II	No.	42 @ 21'-4"	42 @ 21'-4"	42 @ 21'-4"
	Length	16 @ 17'-4"	16 @ 17'-4"	16 @ 17'-4"
	Weight	3133#	3133#	3133#
Total Weight of Bars	Lbs.	4060	4060	4068
Length	Feet	15.00	15.00	14.04
Net Volume	Cu. Yds.	1.04	1.04	.98
R.C. CAP	No.	14	14	14
	Length	14'-6"	14'-6"	13'-9"
Beam Bars #8	No.	8	8	8
Total Weight of Bars	Lbs.	356	356	339

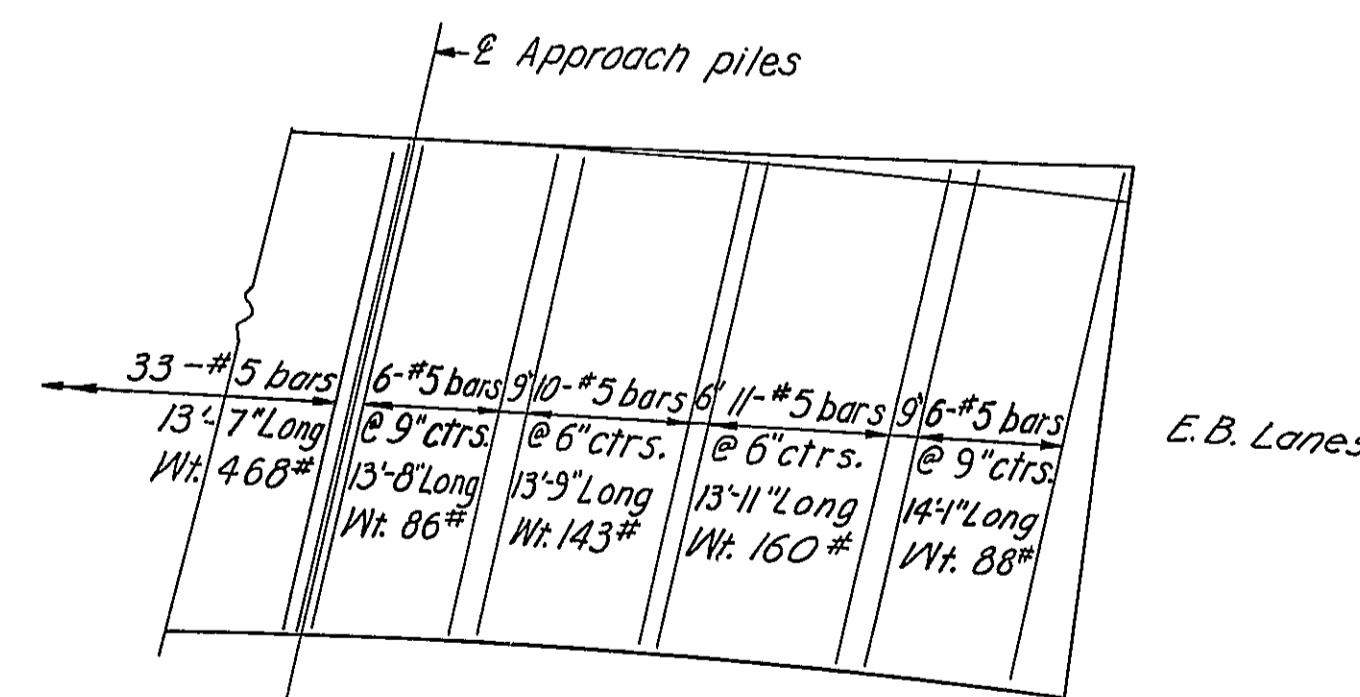
* See Reinforcement Details this Sheet.

Note: For General notes and details, see standard 1909.



BRIDGE APPROACH SLABS

STA. 47+96.28 TO STA. 48+45.78 E.B.L.
 STA. 48+09.57 TO STA. 48+45.78 W.B.L.
 STA. 48+45.78 TO STA. 51+45.57 BRIDGE OMISSION *
 STA. 51+45.57 TO STA. 51+85.05 E.B.L.
 STA. 51+45.57 TO STA. 51+86.88 W.B.L.
 * SEE SHEET 60 FOR BRIDGE TYPICAL SECTION



REINFORCEMENT DETAILS

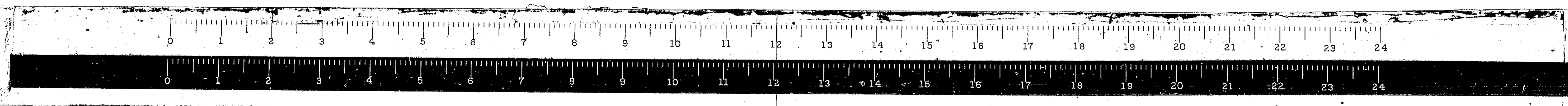
AT E. ABUTMENT E.B. LANES
 (For details not shown see std. 1909)

DESIGNED BY B.M.
 DRAWN BY M.R.
 CHECKED BY E.T.P.
 APPROVED BY _____

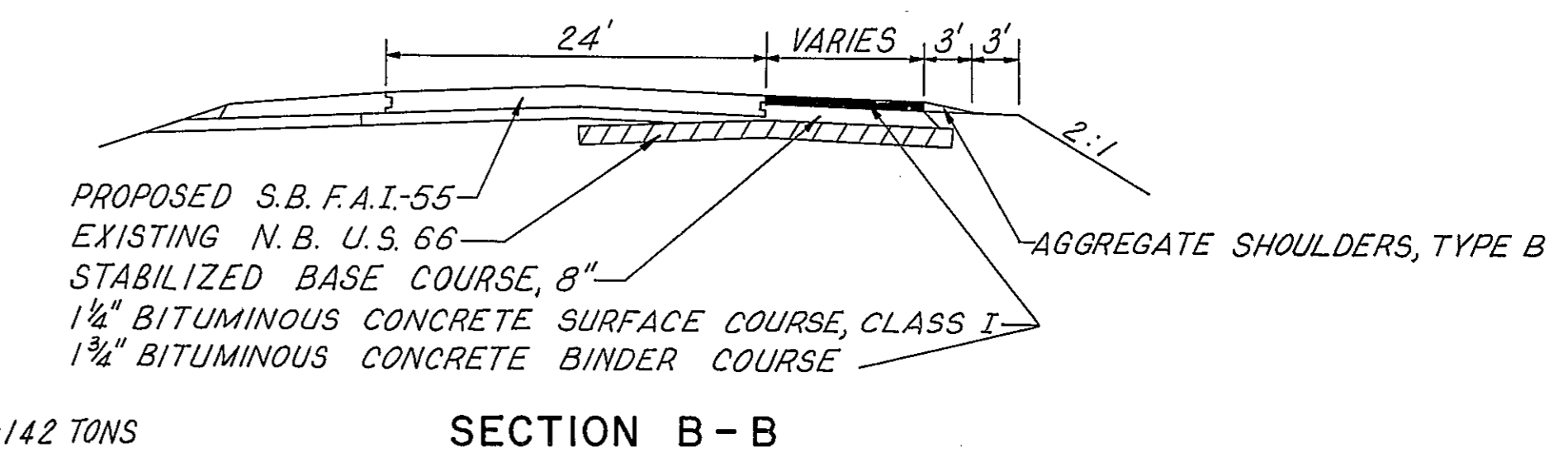
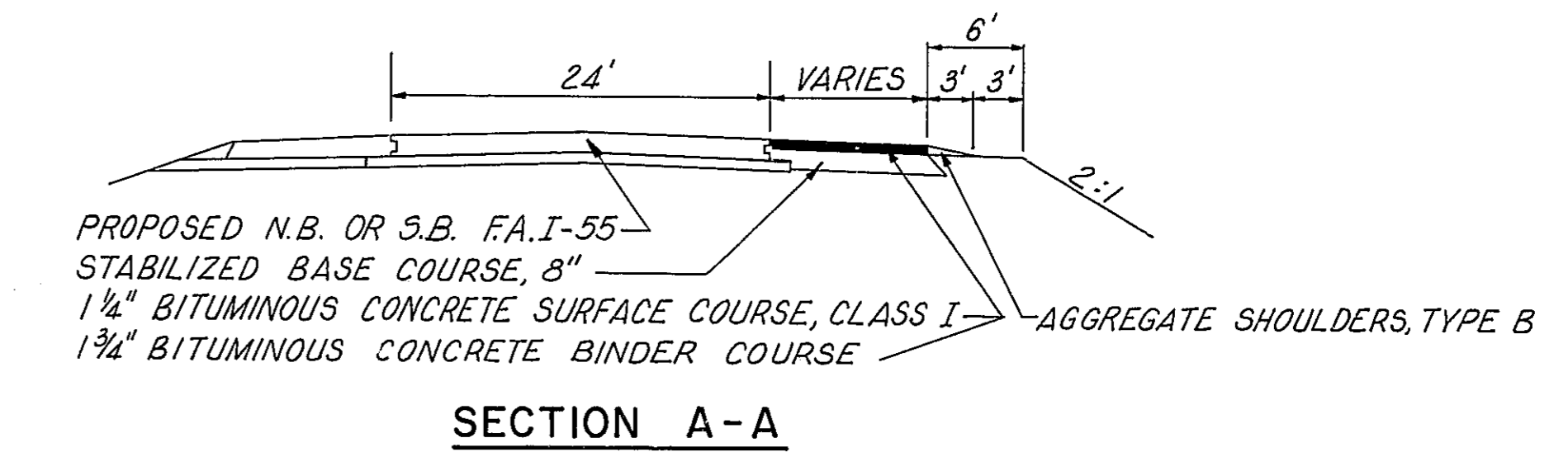
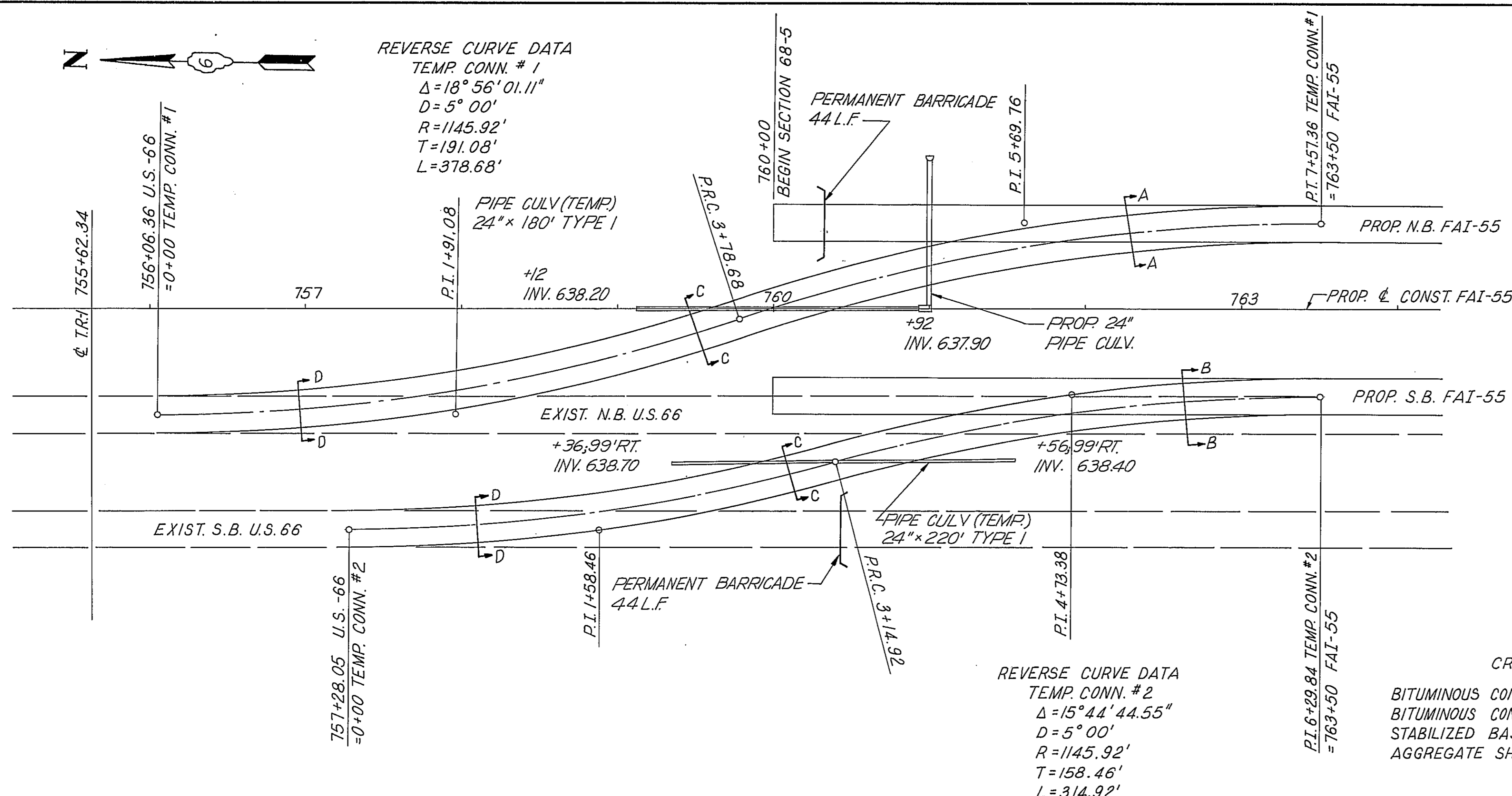
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORK & BUILDINGS
 DIVISION OF HIGHWAYS

BRIDGE APPROACH
 SLABS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

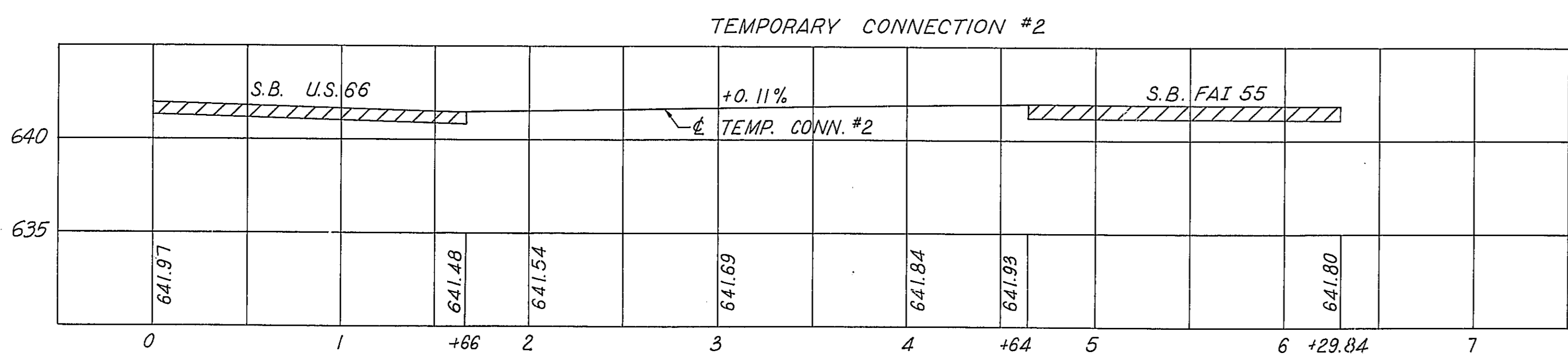
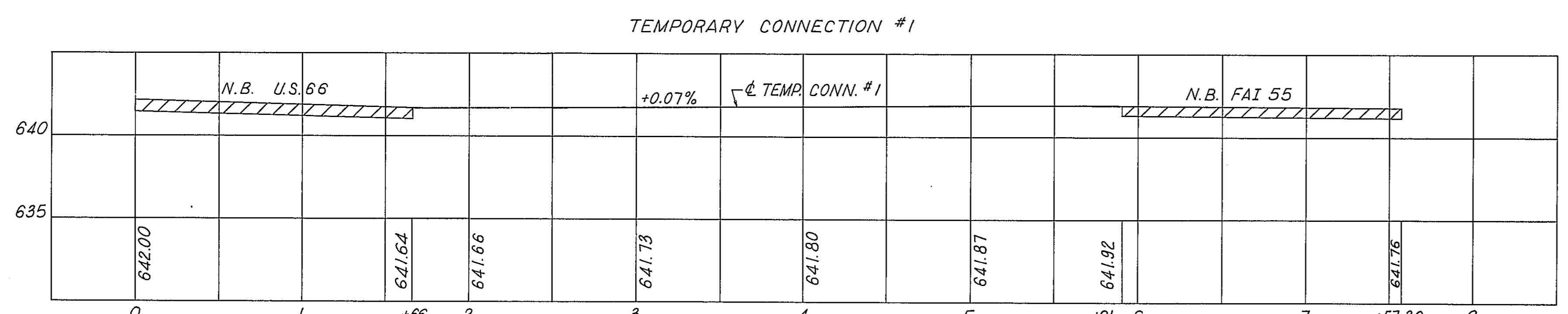
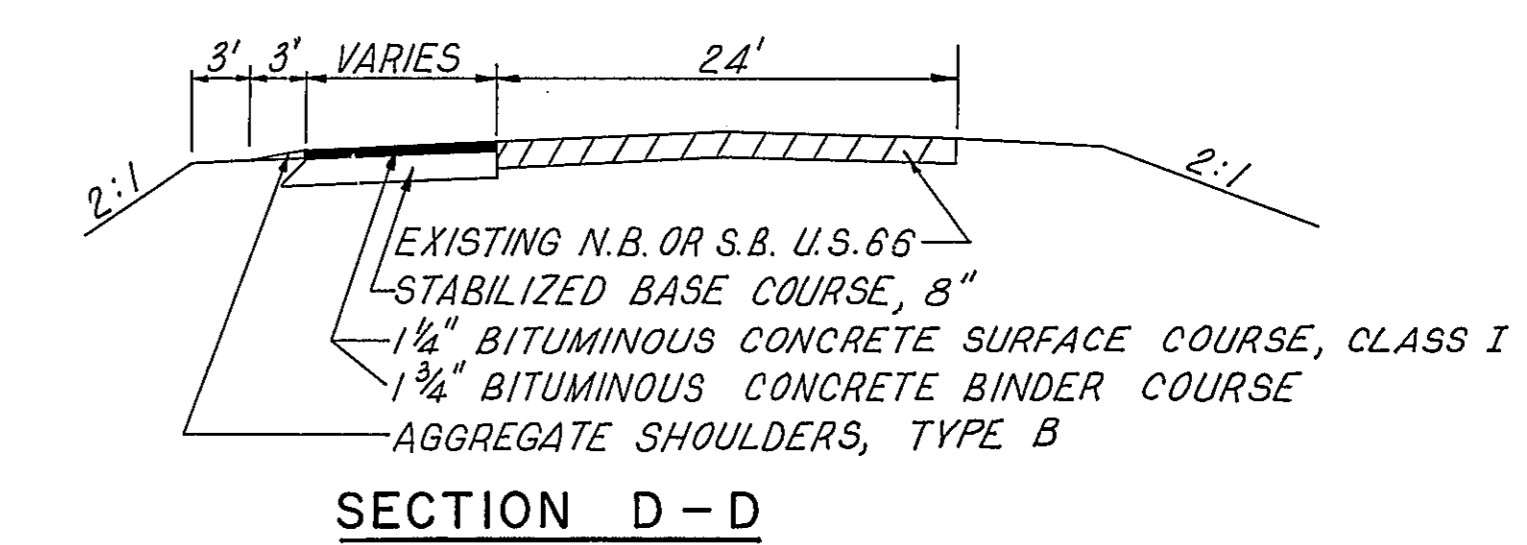
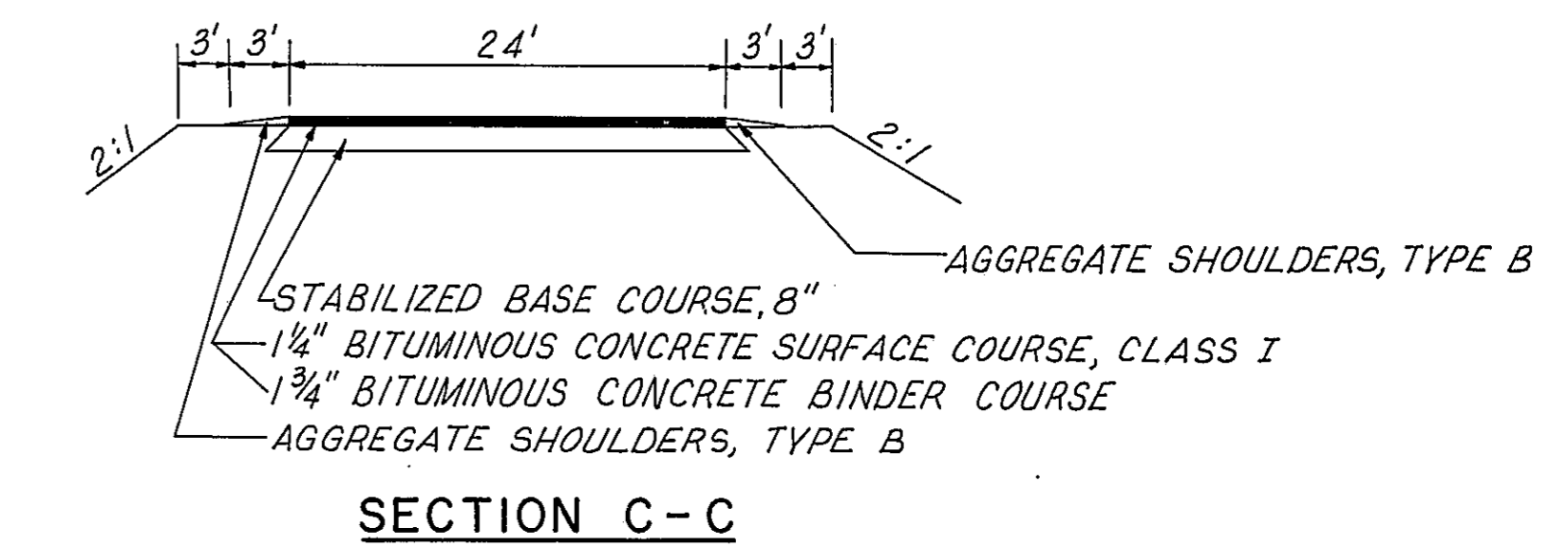


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	*	MONT.	206	58
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



CROSSOVER QUANTITIES

BITUMINOUS CONCRETE SURFACE COURSE, CLASS I	= 142 TONS
BITUMINOUS CONCRETE BINDER COURSE	= 199 TONS
STABILIZED BASE COURSE 8"	= 2030 SQ YDS
AGGREGATE SHOULDERS, TYPE B	= 52 TONS

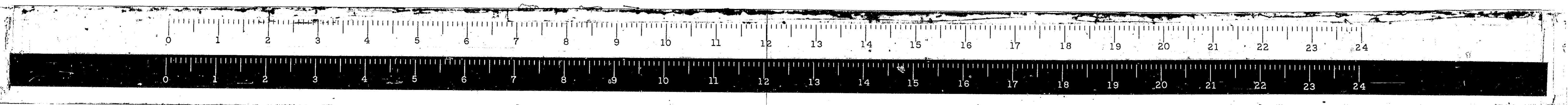


TRAFFIC CONTROL AND PROTECTION, TEMPORARY CONNECTIONS SHALL BE USED FOR TEMPORARY CONNECTIONS #1, #2, #3 AND #4 (SEE DETAIL ON SHEET 59B). BEFORE ANY TEMPORARY CONNECTION IS OPEN TO TRAFFIC THE LIGHTING SYSTEM FOR THE TEMPORARY CONNECTION SHALL BE COMPLETELY INSTALLED AND OPERATIONAL. THE WIRING OF THE TEMPORARY LIGHTING SHALL BE PHASED SO THAT ONLY THE LIGHTING FOR THE PARTICULAR TEMPORARY CONNECTION BEING OPENED TO TRAFFIC SHALL BE OPERATIONAL.

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

TEMPORARY CONNECTIONS 1 & 2
 DETAILS

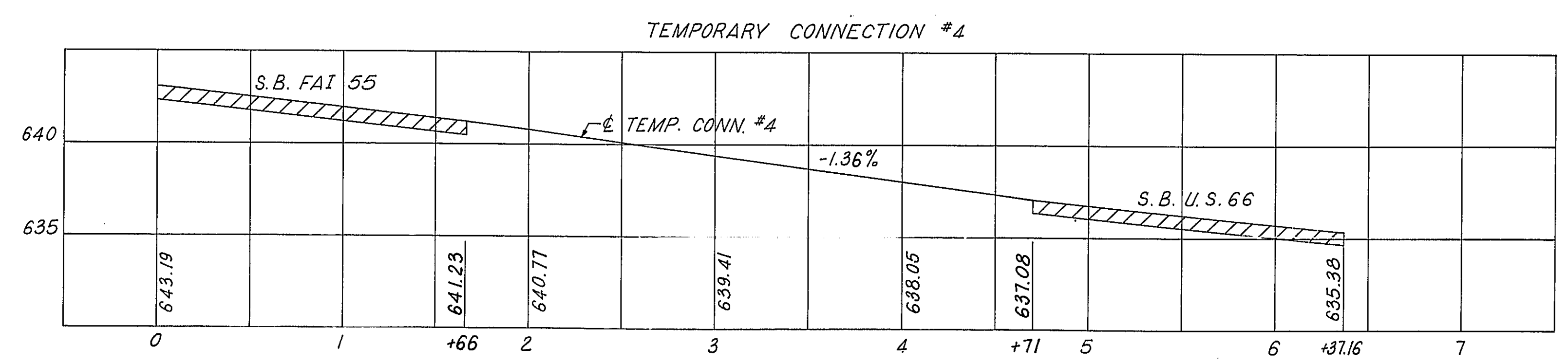
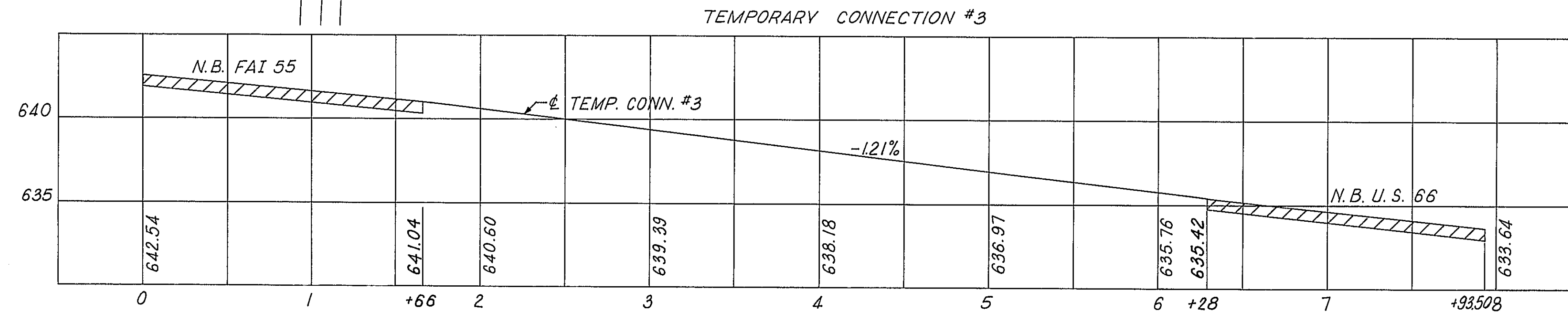
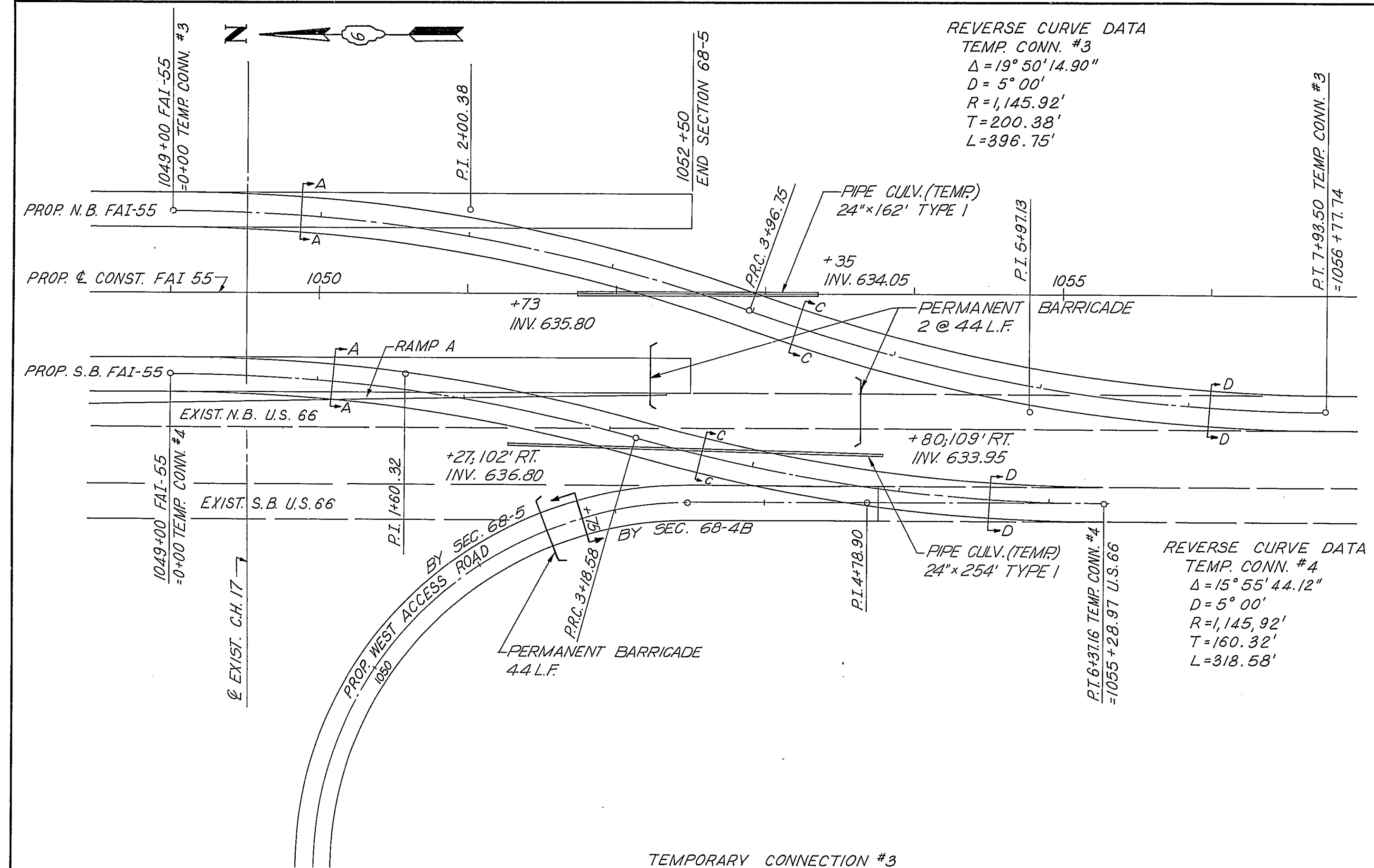
WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	*	MONT.	206	59A
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

REVERSE CURVE DATA
 TEMP. CONN. #3
 $\Delta = 19^\circ 50' 14.90''$
 $D = 5^\circ 00'$
 $R = 1,145.92'$
 $T = 200.38'$
 $L = 396.75'$

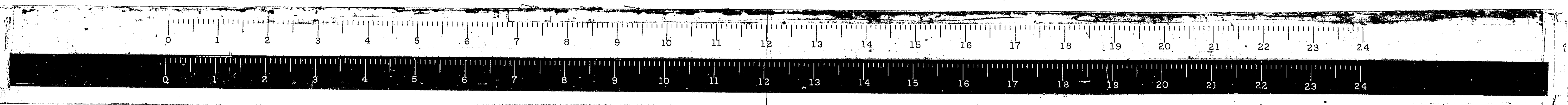
CROSSOVER QUANTITIES
 BITUMINOUS CONCRETE SURFACE COURSE, CLASS I = 145 TONS
 BITUMINOUS CONCRETE BINDER COURSE = 203 TONS
 STABILIZED BASE COURSE B" = 2064 SQ. YDS.
 AGGREGATE SHOULDERS, TYPE B = 55 TONS

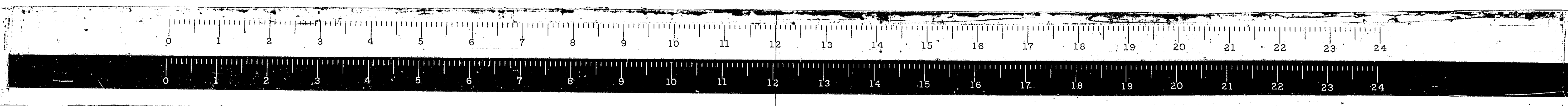
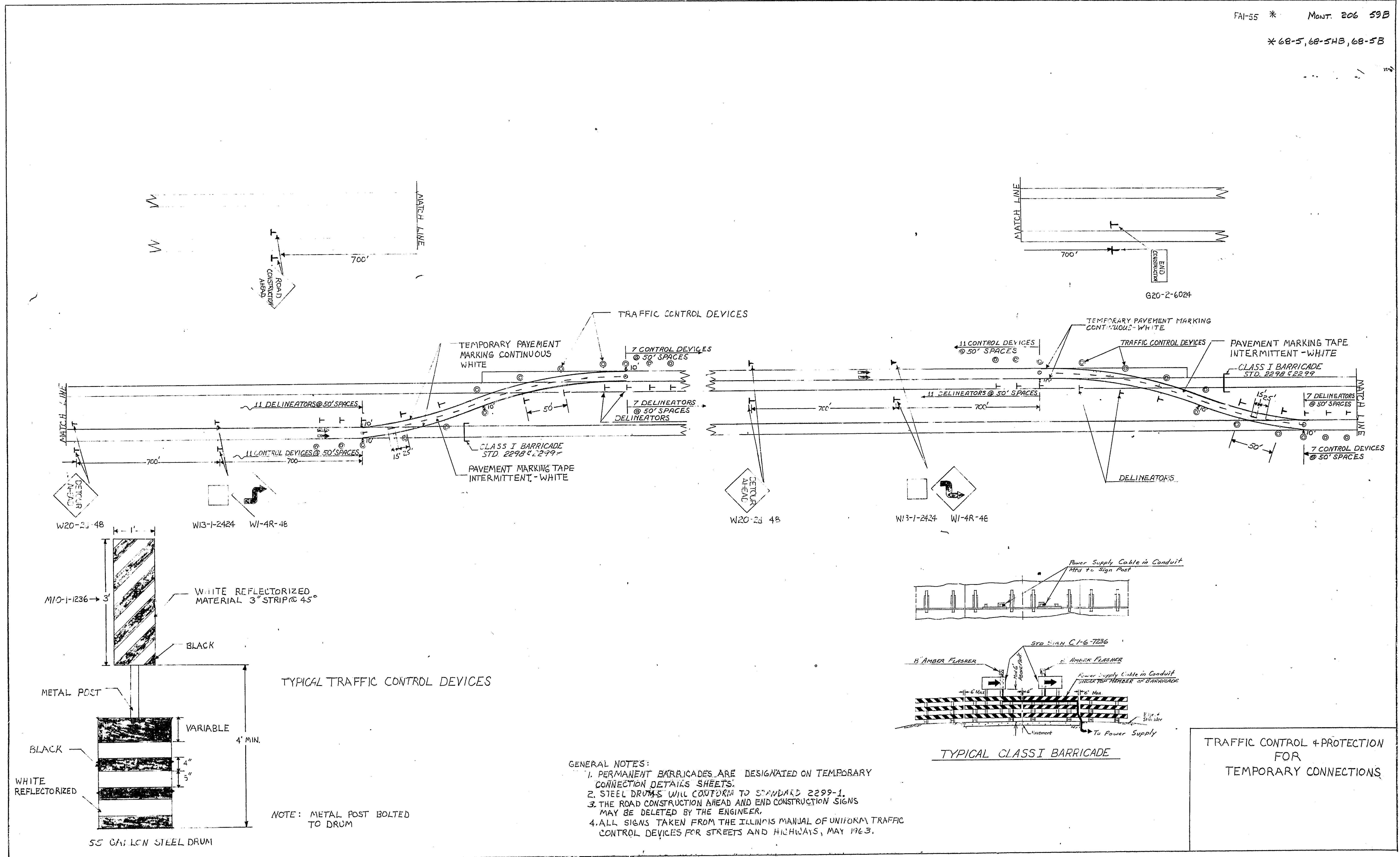


STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

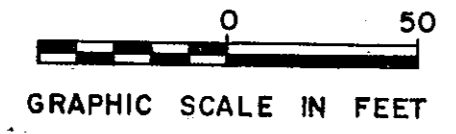
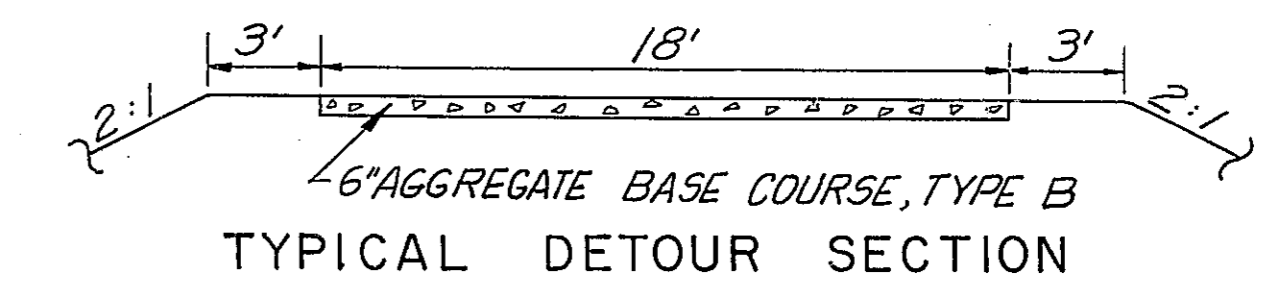
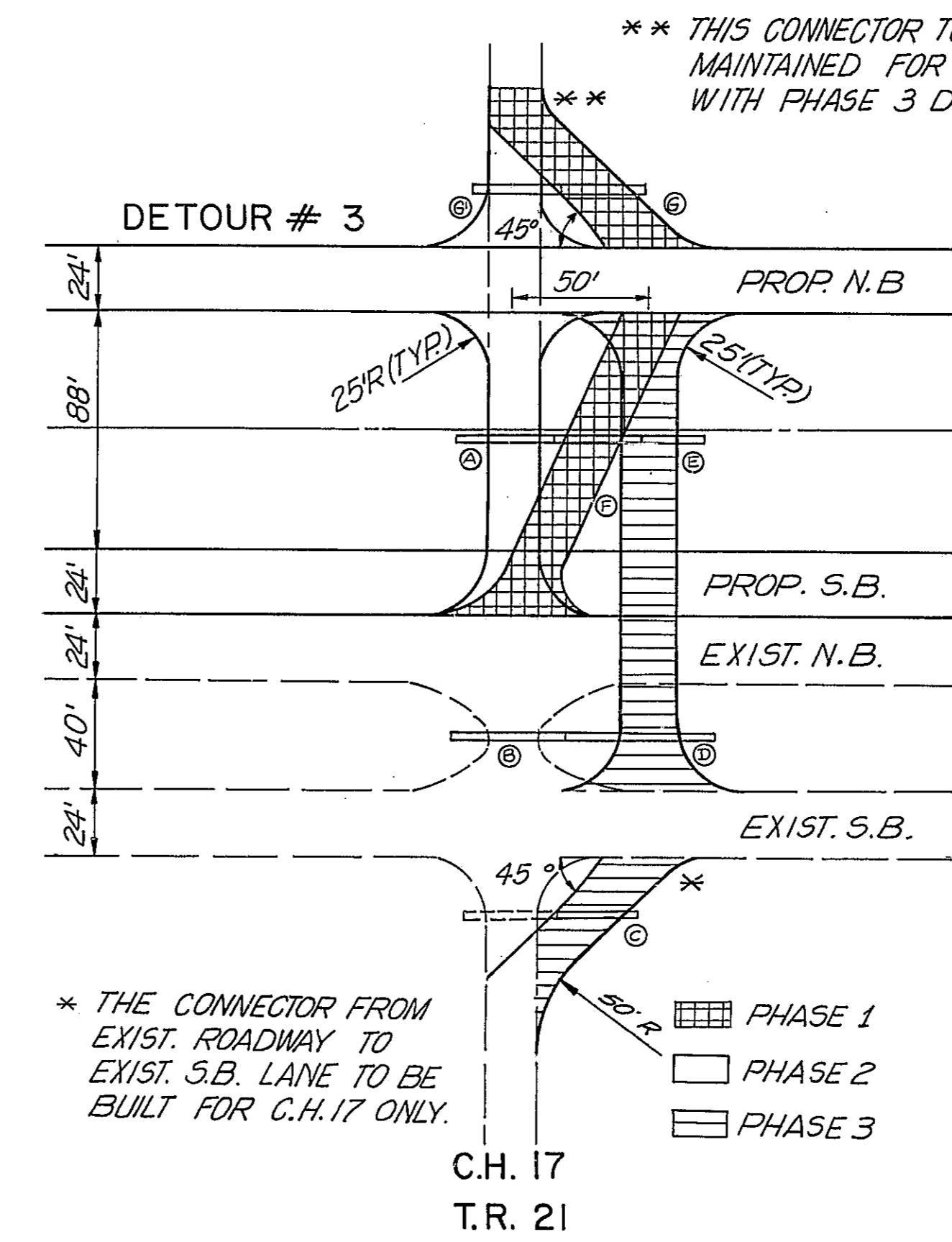
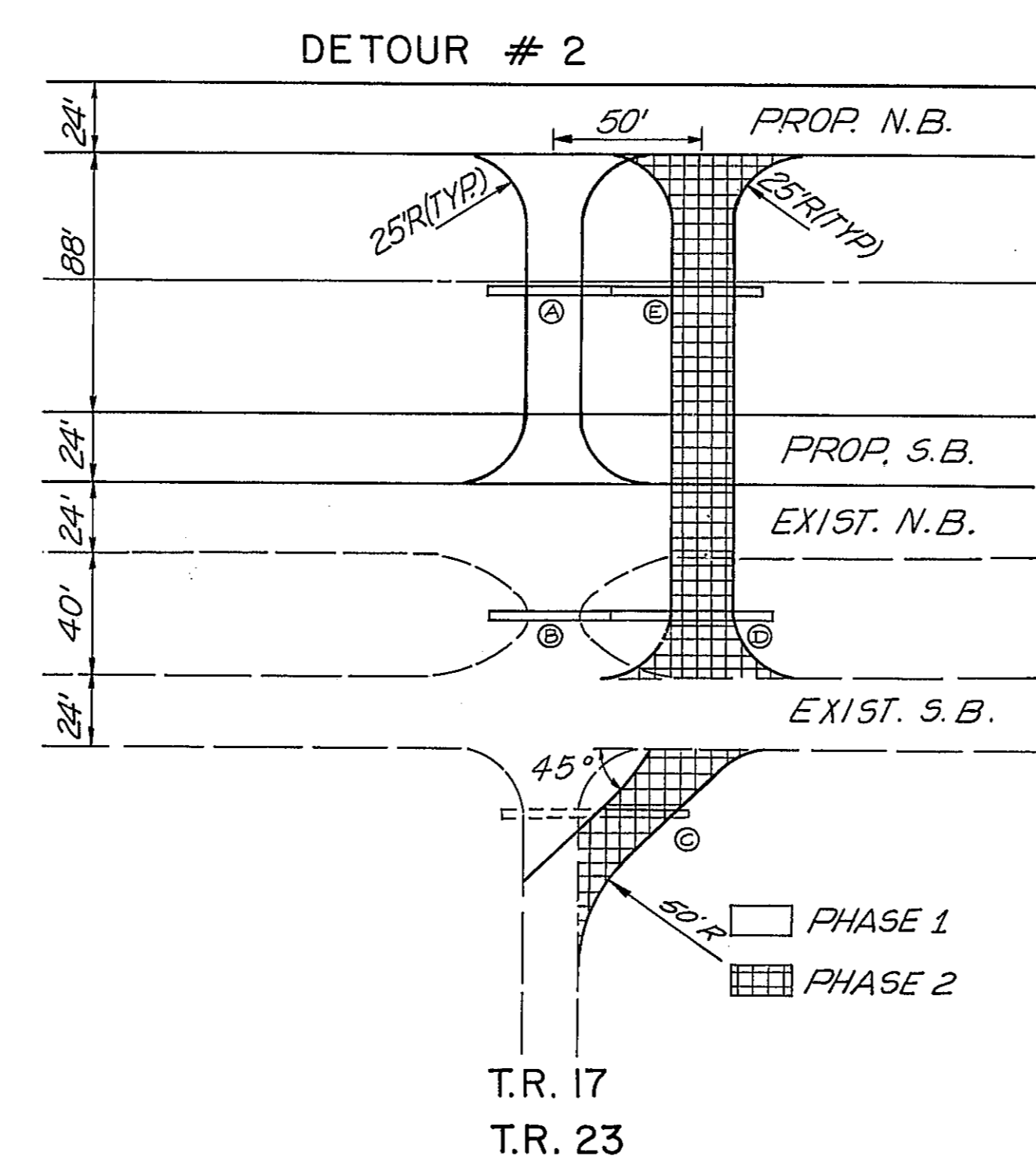
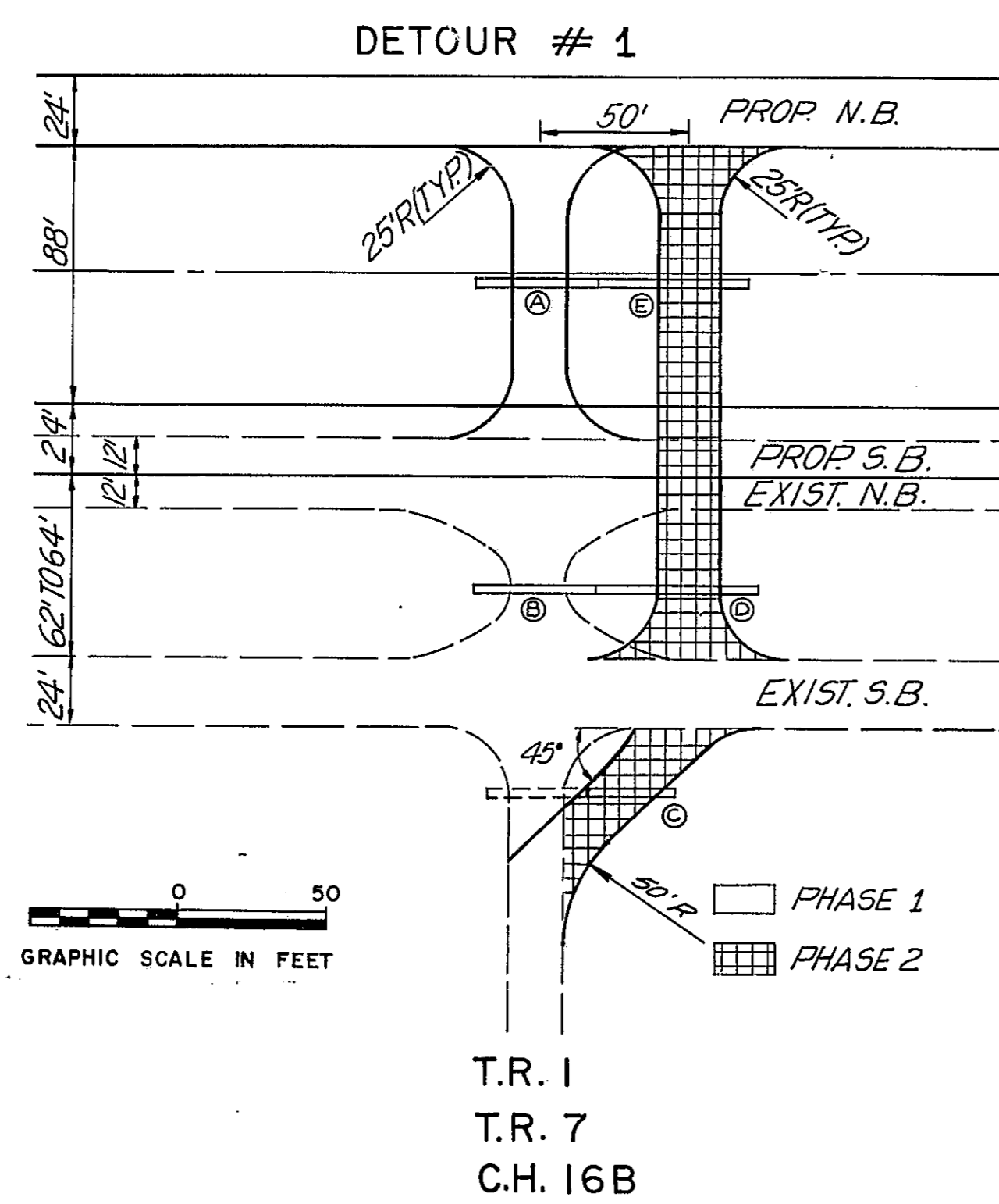
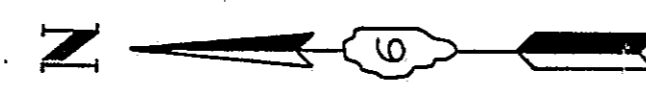
TEMPORARY CONNECTIONS 3 & 4
 DETAILS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS





ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MCNT.	206	59C
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



ROADWAY	STATION	AGGREGATE BASE COURSE TYPE B (TONS)	BORROW EXCAVATION (C.Y.)	DETOUR ROADWAY REMOVAL SQ. YDS.	PIPE CULVERTS TYPE 1 (L.F.)
T.R. 1	758+40±	258	484	754	Ⓐ 18" x 40' Ⓑ 24" x 26' Ⓒ 24" x 50' Ⓓ 18" x 50'
T.R. 7	836+56±	258	505	754	Ⓐ 18" x 42' Ⓑ 18" x 50'
C.H. 16B	876+76±	258	761	756	Ⓐ 18" x 42' Ⓑ 18" x 50'
T.R. 17	919+30±	267	772	782	Ⓐ 18" x 42' Ⓑ 24" x 26' Ⓒ 24" x 50' Ⓓ 18" x 50'
T.R. 21	969+70±	379	304	1288	Ⓐ 18" x 42' Ⓑ 18" x 42' Ⓒ 18" x 26' Ⓓ 18" x 26' Ⓔ 18" x 26'
T.R. 23	996+36±	267	772	782	Ⓐ 18" x 42' Ⓑ 15" x 26' Ⓒ 18" x 50'
C.H. 17	1049+49±	392	1334	1340	Ⓐ 24" x 42' Ⓑ 24" x 42' Ⓒ 24" x 26' Ⓓ 24" x 50' Ⓔ 24" x 50'

PHASING (DETOURS # 1 AND # 2)

- UPON COMPLETION OF THE N.B. LANE, CONSTRUCT DETOUR, PHASE 1 TO ALLOW ACCESS TO N.B. LANE.
- CONSTRUCT S.B. LANE, EXCEPT FOR 50 FT. PAVING GAP AT EXISTING TOWNSHIP ROAD OR COUNTY HIGHWAY. TRAFFIC ON THE TOWNSHIP ROAD OR COUNTY HIGHWAY WILL NOT BE AFFECTED. CONSTRUCT DETOUR, PHASE 2.
- MOVE TRAFFIC ONTO DETOUR AND POUR GAP.
- OPEN S.B. LANE, MOVE TRAFFIC BACK ONTO PHASE 1 DETOUR AND REMOVE PHASE 2 DETOUR.
- TOWNSHIP ROAD OR COUNTY HIGHWAY TO REMAIN OPEN UNTIL ENTIRE SECTION IS COMPLETED. AT THIS TIME PHASE 1 DETOURS TO BE REMOVED.

PHASING (DETOUR # 3)

- CONSTRUCT N.B. LANE, EXCEPT FOR 50 FT. PAVING GAP AT EXISTING TOWNSHIP ROAD OR COUNTY HIGHWAY. TRAFFIC ON THE TOWNSHIP ROAD OR COUNTY HIGHWAY WILL NOT BE AFFECTED. CONSTRUCT DETOUR, PHASE 1.
- MOVE TRAFFIC ONTO DETOUR AND POUR GAP. CONSTRUCT DETOUR, PHASE 2.
- OPEN N.B. LANE, MOVE TRAFFIC ONTO PHASE 2 DETOUR AND REMOVE WEST HALF OF PHASE 1 DETOUR.

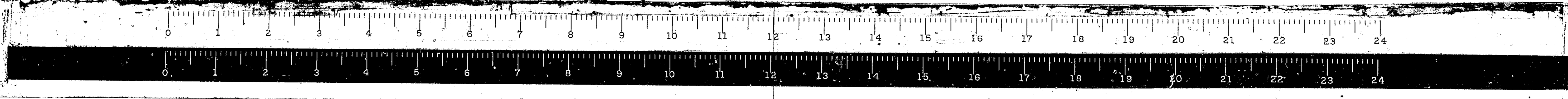
- CONSTRUCT S.B. LANE, EXCEPT FOR 50 FT. PAVING GAP AT EXISTING TOWNSHIP ROAD OR COUNTY HIGHWAY. TRAFFIC ON THE TOWNSHIP ROAD OR COUNTY HIGHWAY WILL NOT BE AFFECTED. CONSTRUCT DETOUR, PHASE 3.
- MOVE TRAFFIC ONTO DETOUR AND POUR GAP.
- OPEN S.B. LANE, MOVE TRAFFIC BACK ONTO PHASE 2 DETOUR AND REMOVE PHASE 3 DETOUR, AND EAST HALF OF PHASE 1 DETOUR.
- TOWNSHIP ROAD OR COUNTY HIGHWAY TO REMAIN OPEN UNTIL ENTIRE SECTION IS COMPLETED. AT THIS TIME PHASE 2 DETOURS TO BE REMOVED.

DESIGNED BY: F.T.R.
DRAWN BY: A.J.
CHECKED BY: W.F.T.
APPROVED BY: _____

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

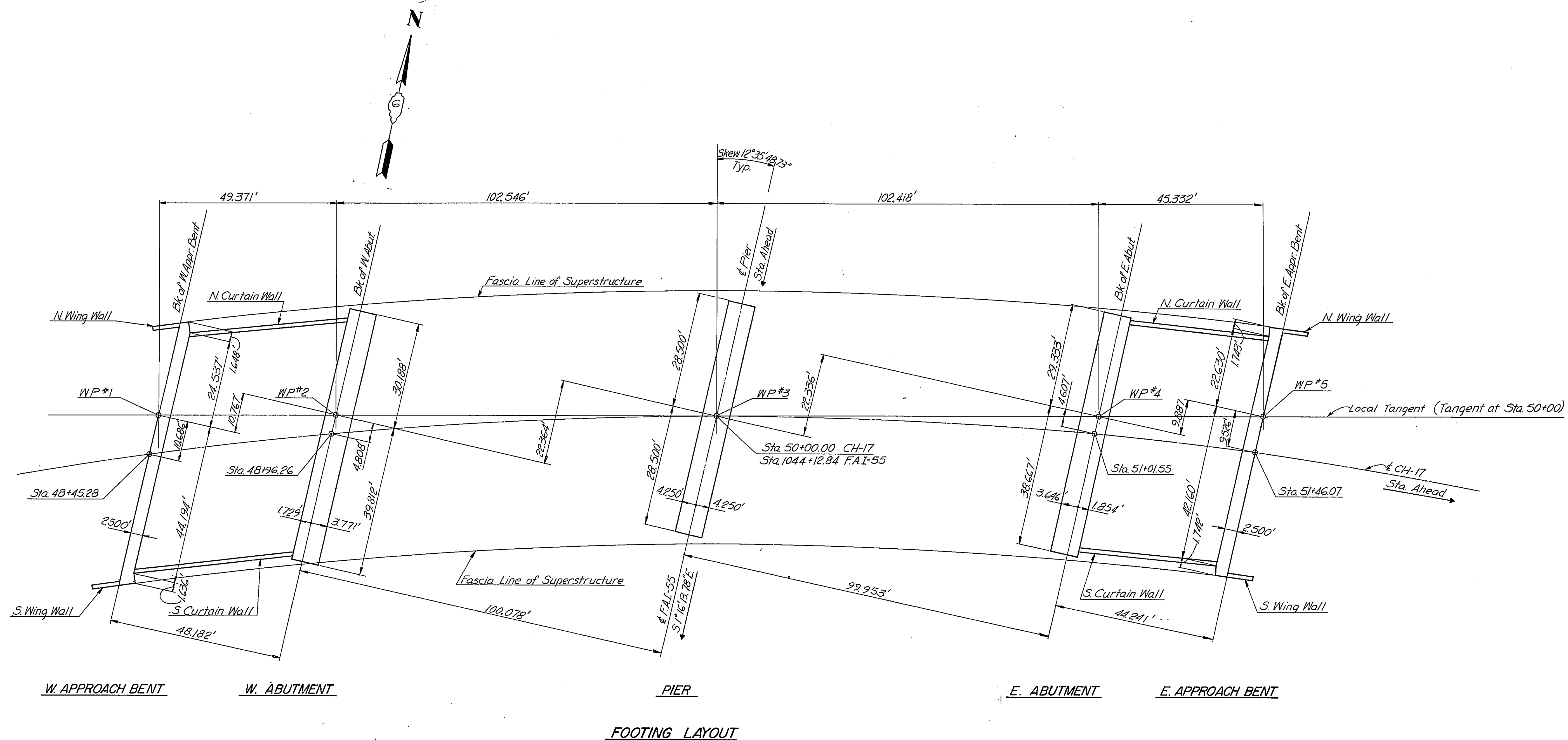
DETOUR ROADWAYS

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 2
FAI-55	68-5HB	MONTGOMERY	206	61	
FED. ROAD REG. NO. 4	ILLINOIS	PROJECT	1-55-2()		

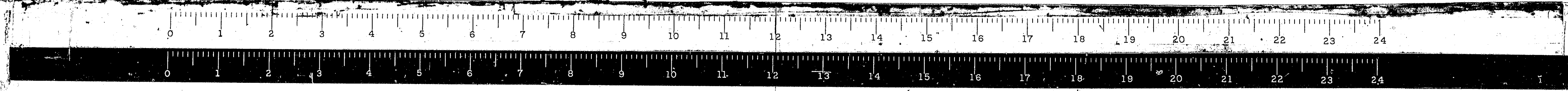


FOOTING LAYOUT

FOOTING LAYOUT
CH-17 OVER FAI-55
STA. 1044+12.84

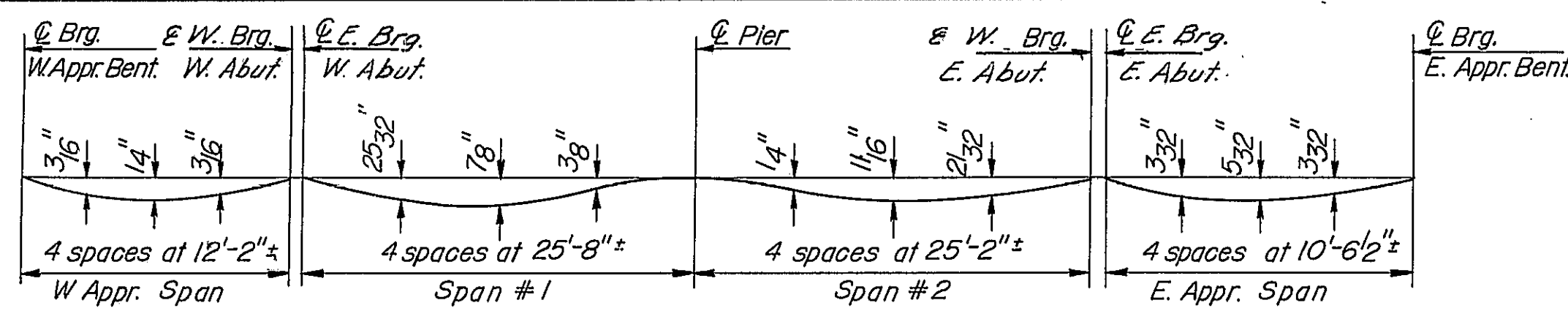
F.A.I. ROUTE-55 PROJECT I-55-2()
SECTION 68-5HB MONTGOMERY COUNTY
Scale: None Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

DESIGNED	B.M.
CHECKED	N.M.S.
DRAWN	K.A.C.
CHECKED	B.M.



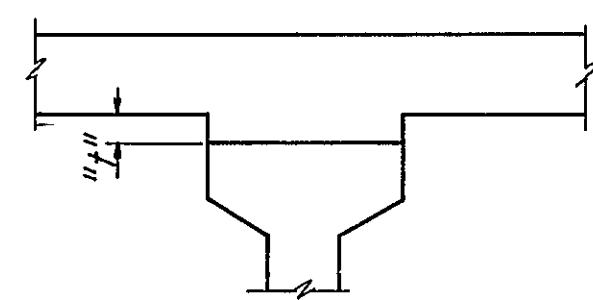
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-55	68-5HB	MONTGOMERY	206	62
FED. ROAD REG. NO. 4	ILLINOIS PROJECT	I-55-2		

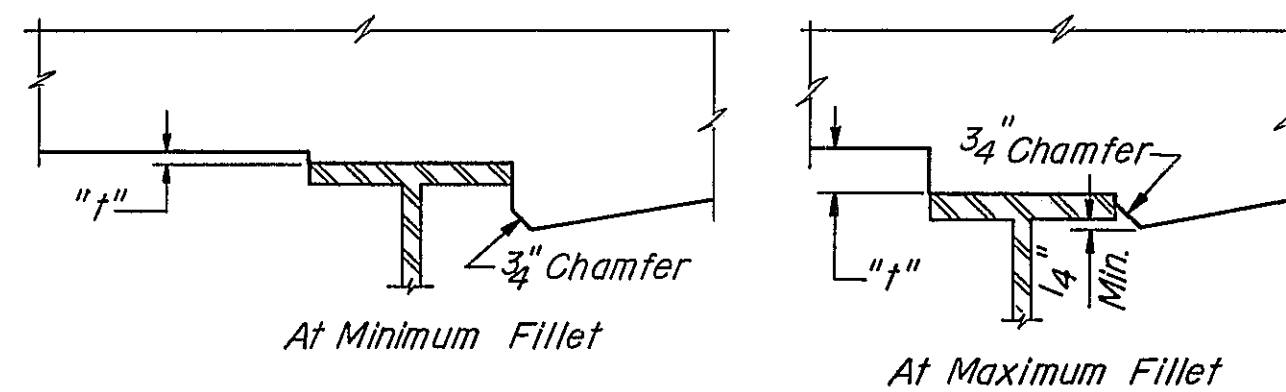


DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only)
Note: The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown below.

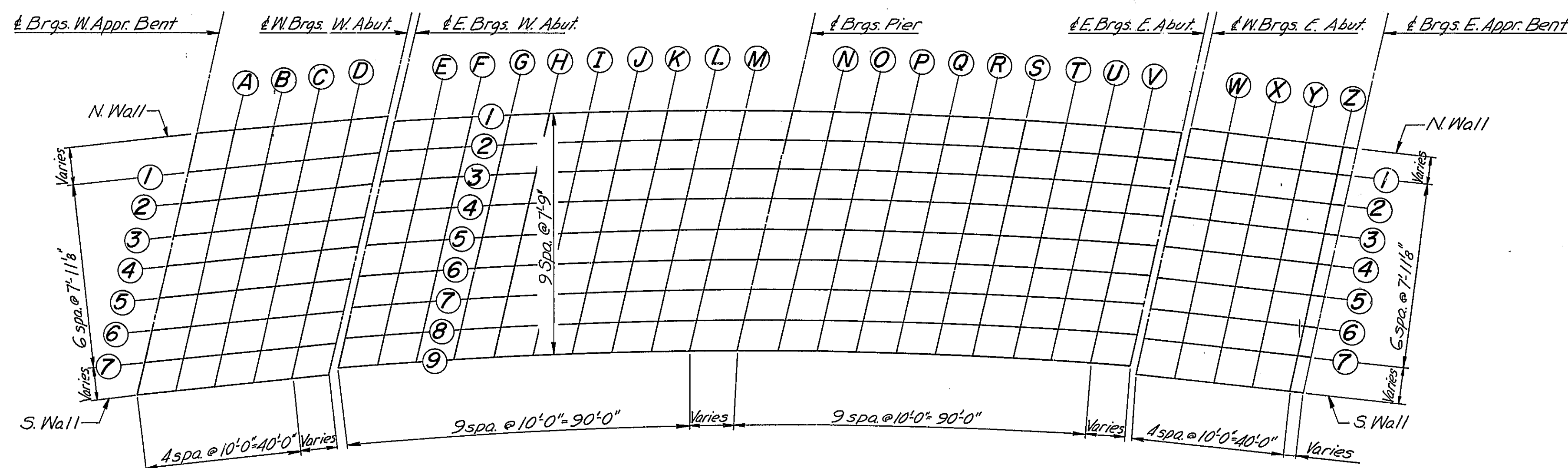


To determine "f" After all precast prestressed beams have been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted algebraically from the "Theoretical Grade Elevations Adjusted for Dead Load Deflections" shown below, minus slab thickness, equals the fillet heights "f". A positive value of "f" equals the fillet height above the top of the beam. A negative value of "f" not to exceed 1/2", equals the embedment of the beam above theoretical bottom of slab elevations.



To determine "f" After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown below, minus slab thickness, equals the fillet heights "f" above top flange of beams.

FILLET HEIGHTS



PLAN OF DECK SURFACE ELEVATIONS

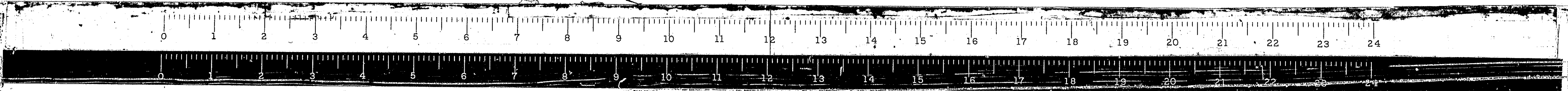
Location	Beam	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
E. Brg. W. Appr. Bent	N. Wall	48+58.272	32.276	670.520	670.520
	1	48+55.241	23.634	669.993	669.993
	2	48+52.434	15.749	669.512	669.512
	3	48+49.589	7.871	669.030	669.030
	4	48+46.705	0.000	669.098	669.098
	5	48+43.781	7.864	669.165	669.165
	S. Wall	48+40.816	15.720	668.683	668.683
A	N. Wall	48+37.809	23.568	668.202	668.202
	1	48+34.441	32.222	667.669	667.669
	2	48+31.997	32.103	670.504	670.504
	3	48+29.039	23.539	669.984	669.984
	4	48+26.299	15.630	669.503	669.516
	5	48+23.521	7.727	669.022	669.036
	S. Wall	48+20.704	0.169	669.110	669.025
B	N. Wall	48+17.849	8.058	669.157	669.171
	1	48+15.953	15.939	668.676	668.689
	2	48+13.017	23.813	668.195	668.208
	3	48+10.070	32.406	667.669	667.669
	4	48+7.125	32.016	670.487	670.487
	5	48+4.188	23.529	669.973	669.994
	S. Wall	48+1.264	15.596	669.493	669.514
C	N. Wall	48+0.434	7.670	669.012	669.035
	1	48+0.706	0.250	669.111	669.135
	2	48+0.919	8.164	669.148	669.171
	3	48+1.094	16.070	668.668	668.689
	4	48+1.229	23.970	668.187	668.208
	5	48+1.421	32.500	667.667	667.667
	S. Wall	48+1.453	32.013	670.469	670.469
D	N. Wall	48+1.636	23.605	669.962	669.982
	1	48+1.830	15.649	669.481	669.501
	2	48+2.037	7.669	669.001	669.023
	3	48+2.245	0.245	669.099	669.122
	4	48+2.451	8.182	669.138	669.160
	5	48+2.657	16.113	668.657	668.677
	S. Wall	48+2.863	24.040	668.177	668.197
E. Brg. W. Abut.	N. Wall	48+3.063	32.505	667.663	667.663
	1	48+3.266	32.096	670.450	670.450
	2	48+3.473	23.766	669.949	669.961
	3	48+3.683	15.788	669.469	669.481
	4	48+3.893	7.815	668.989	669.002
	5	48+4.106	0.152	669.076	669.090
	S. Wall	48+4.323	8.112	669.126	669.139
E. Brg. E. Appr. Bent	N. Wall	48+4.537	16.067	668.646	668.658
	1	48+4.757	24.015	668.165	668.177
	2	48+4.979	32.419	667.657	667.657
	3	48+5.203	32.232	670.432	670.432
	4	48+5.429	23.976	669.937	669.937
	5	48+5.657	15.978	669.457	669.457
	S. Wall	48+5.887	7.986	668.977	668.977
E. Brg. E. Appr. Bent	N. Wall	48+6.115	0.000	669.046	669.046
	1	48+6.345	7.981	669.115	669.115
	2	48+6.575	15.955	668.635	668.635
	3	48+6.804	23.924	668.154	668.154
	4	48+7.034	32.267	667.651	667.651
	5	48+7.264	32.267	667.651	667.651
	S. Wall	48+7.494	32.267	667.651	667.651

**TOP OF SLAB ELEVATIONS I
CH-17 OVER FA.I-55
STA. 1044+12.84**

F.A.I. ROUTE 55 SECTION 68-5HB PROJECT I-55-2()
MONTGOMERY COUNTY

Scale: None Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

DESIGNED	B.M.
CHECKED	N.M.S. & N.O.S.
DRAWN	K.A.C.
CHECKED	B.M.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-55	68-SHB	MONTGOMERY	206	63
FED. ROAD REG. NO. 4	ILLINOIS PROJECT	I-55-2()		

SHEET NO. 4

Location	Beam	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
E. Brgs. W. Abut.	1	49+06.973	31.000	670.352	670.352
	2	49+04.614	23.250	669.887	669.887
	3	49+02.222	15.500	669.422	669.422
	4	48+99.796	7.750	668.957	668.957
	5	48+97.336	0.000	668.492	668.492
	6	48+94.840	7.750	668.027	668.027
	7	48+92.309	15.500	667.562	667.562
	8	48+89.740	23.250	667.097	667.097
	9	48+87.134	31.000	666.632	666.632
E	1	49+16.710	31.000	670.315	670.315
	2	49+14.415	23.250	669.850	669.850
	3	49+12.088	15.500	669.385	669.385
	4	49+09.729	7.750	668.920	668.920
	5	49+07.336	0.000	668.455	668.455
	6	49+04.908	7.750	667.990	667.990
	7	49+02.446	15.500	667.525	667.525
	8	48+99.947	23.250	667.060	667.060
	9	48+97.412	31.000	666.595	666.595
F	1	49+26.446	31.000	670.272	670.272
	2	49+24.216	23.250	669.807	669.807
	3	49+21.955	15.500	669.342	669.342
	4	49+19.662	7.750	668.877	668.877
	5	49+17.336	0.000	668.412	668.412
	6	49+14.976	7.750	667.947	667.947
	7	49+12.583	15.500	667.482	667.482
	8	49+10.154	23.250	667.017	667.017
	9	49+07.690	31.000	666.552	666.552
G	1	49+36.183	31.000	670.222	670.222
	2	49+34.017	23.250	669.757	669.757
	3	49+31.822	15.500	669.292	669.292
	4	49+29.595	7.750	668.827	668.827
	5	49+27.336	0.000	668.362	668.362
	6	49+25.044	7.750	667.897	667.897
	7	49+22.720	15.500	667.432	667.432
	8	49+20.362	23.250	666.967	666.967
	9	49+17.968	31.000	666.502	666.502
H	1	49+45.920	31.000	670.167	670.167
	2	49+43.819	23.250	669.702	669.702
	3	49+41.688	15.500	669.237	669.237
	4	49+39.527	7.750	668.772	668.772
	5	49+37.336	0.000	668.307	668.307
	6	49+35.113	7.750	667.842	667.842
	7	49+32.857	15.500	667.377	667.377
	8	49+30.569	23.250	666.912	666.912
	9	49+28.246	31.000	666.447	666.447
I	1	49+55.636	31.000	670.104	670.104
	2	49+53.620	23.250	669.639	669.639
	3	49+51.555	15.500	669.174	669.174
	4	49+49.460	7.750	668.709	668.709
	5	49+47.336	0.000	668.244	668.244
	6	49+45.181	7.750	667.779	667.779
	7	49+42.994	15.500	667.314	667.314
	8	49+40.776	23.250	666.849	666.849
	9	49+38.524	31.000	666.384	666.384
J	1	49+65.393	31.000	670.036	670.036
	2	49+63.421	23.250	669.571	669.571
	3	49+61.421	15.500	669.106	669.106
	4	49+59.393	7.750	668.641	668.641
	5	49+57.336	0.000	668.176	668.176
	6	49+55.249	7.750	667.711	667.711
	7	49+53.131	15.500	667.246	667.246
	8	49+50.983	23.250	666.781	666.781
	9	49+48.803	31.000	666.316	666.316

Location	Beam	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
K	1	49+75.129	31.000	669.961	670.005
	2	49+73.222	23.250	669.503	669.548
	3	49+71.288	15.500	669.046	669.090
	4	49+69.326	7.750	668.588	668.633
	5	49+67.336	0.000	668.130	668.175
	6	49+65.317	7.750	667.672	667.717
	7	49+63.268	15.500	667.214	667.259
	8	49+61.190	23.250	666.756	666.801
	9	49+59.081	31.000	666.298	666.343
L	1	49+84.866	31.000	669.880	669.925
	2	49+83.023	23.250	669.423	669.468
	3	49+81.154	15.500	668.966	669.011
	4	49+79.259	7.750	668.509	668.554
	5	49+77.336	0.000	668.052	668.097
	6	49+75.385	7.750	667.595	667.640
	7	49+73.406	15.500	667.138	667.183
	8	49+71.397	23.250	666.681	666.726
	9	49+69.359	31.000	666.224	666.269
M	1	49+94.603	31.000	669.792	669.837
	2	49+92.824	23.250	669.336	669.381
	3	49+91.021	15.500	668.880	668.925
	4	49+89.191	7.750	668.424	668.469
	5	49+87.336	0.000	667.968	668.013
	6	49+85.453	7.750	667.512	667.557
	7	49+83.543	15.500	667.056	667.101
	8	49+81.604	23.250	666.600	666.645
	9	49+79.637	31.000	666.144	666.189
E. Brgs. Pier	1	50+06.741	31.000	669.675	669.675
	2	50+05.090	23.250	669.218	669.218
	3	50+03.416	15.500	668.762	668.762
	4	50+01.720	7.750	668.306	668.306
	5	50+00.000	0.000	667.850	667.850
	6	49+98.256	7.750	667.394	667.394
	7	49+96.488	15.500	666.938	666.938
	8	49+94.694	23.250	666.482	666.482
	9	49+92.875	31.000	666.026	666.026
N	1	50+16.477	31.000	669.573	669.573
	2	50+14.891	23.250	669.117	669.117
	3	50+13.283	15.500	668.661	668.661
	4	50+11.653	7.750	668.205	668.205
	5	50+10.000	0.000	667.749	667.749
	6	50+08.324	7.750	667.293	667.293
	7	50+06.625	15.500	666.837	666.837
	8	50+04.901	23.250	666.381	666.381
	9	50+03.153	31.000	665.925	665.925
O	1	50+26.214	31.000	669.465	669.465
	2	50+24.692	23.250	669.009	669.009
	3	50+23.150	15.500	668.554	668.554
	4	50+21.586	7.750	668.099	668.099
	5	50+20.000	0.000	667.643	667.643
	6	50+18.392	7.750	667.187	667.187
	7	50+16.762	15.500	666.731	666.731
	8	50+15.108	23.250	666.275	666.275
	9	50+13.431	31.000	665.819	665.819
P	1	50+35.951	31.000	669.351	669.351
	2	50+34.493	23.250	668.895	668.895
	3	50+33.016	15.500	668.440	668.440
	4	50+31.519	7.750	667.984	667.984
	5	50+30.000	0.000	667.528	667.528
	6	50+28.460	7.750	670.072	670.072
	7	50+26.899	15.500	667.616	667.616
	8	50+25.315	23.250	667.160	667.160
	9	50+23.709	31.000	666.704	666.704

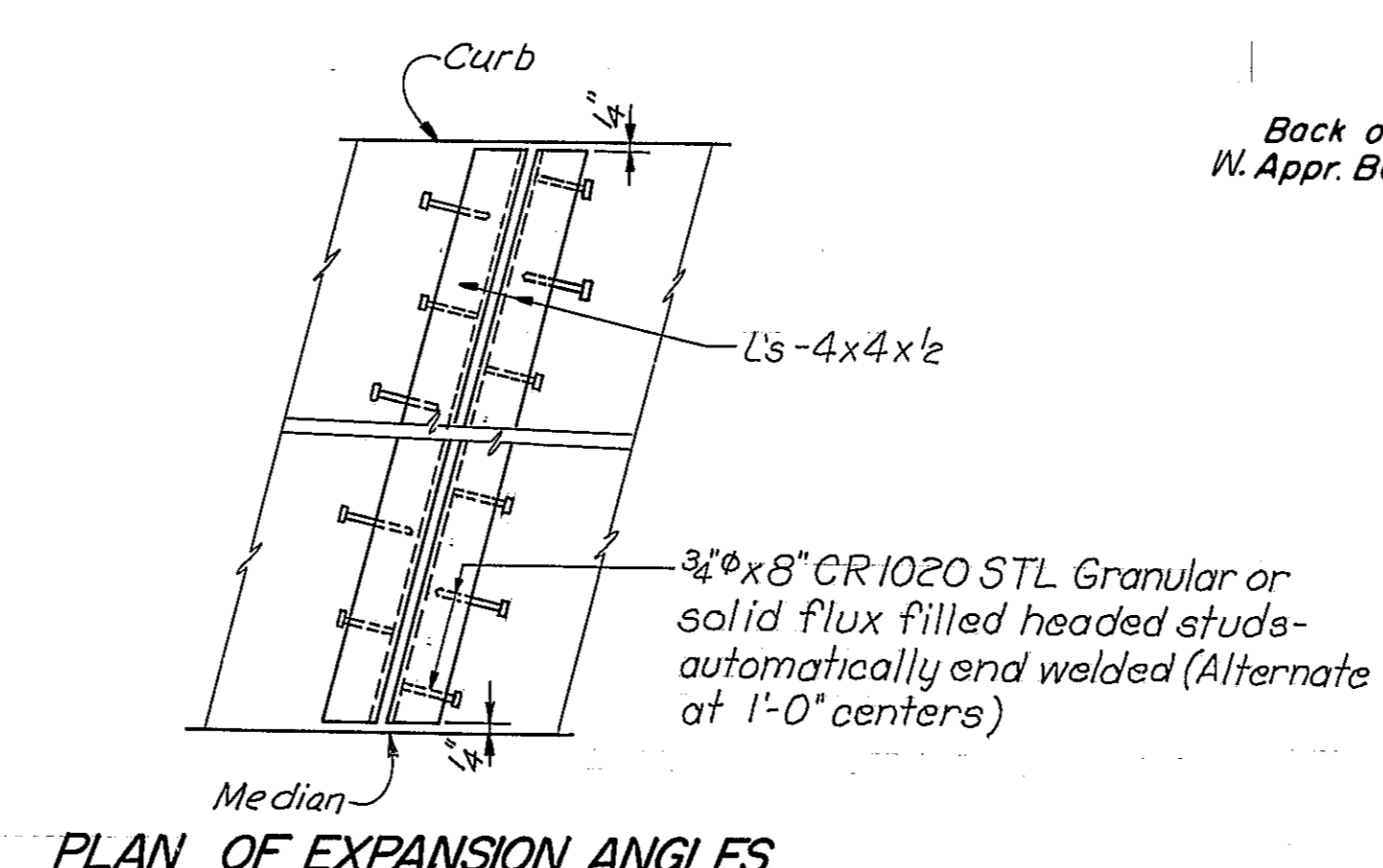
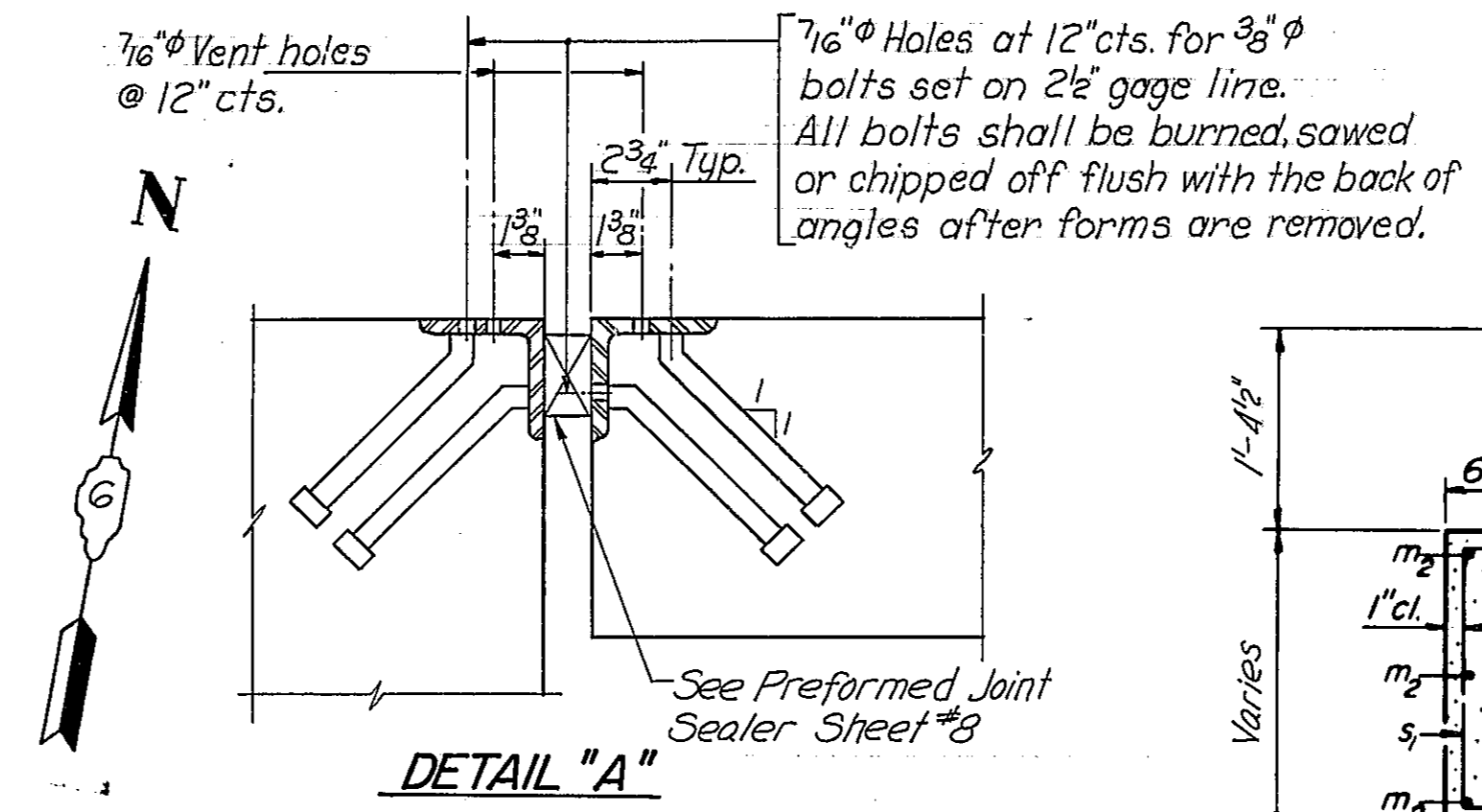
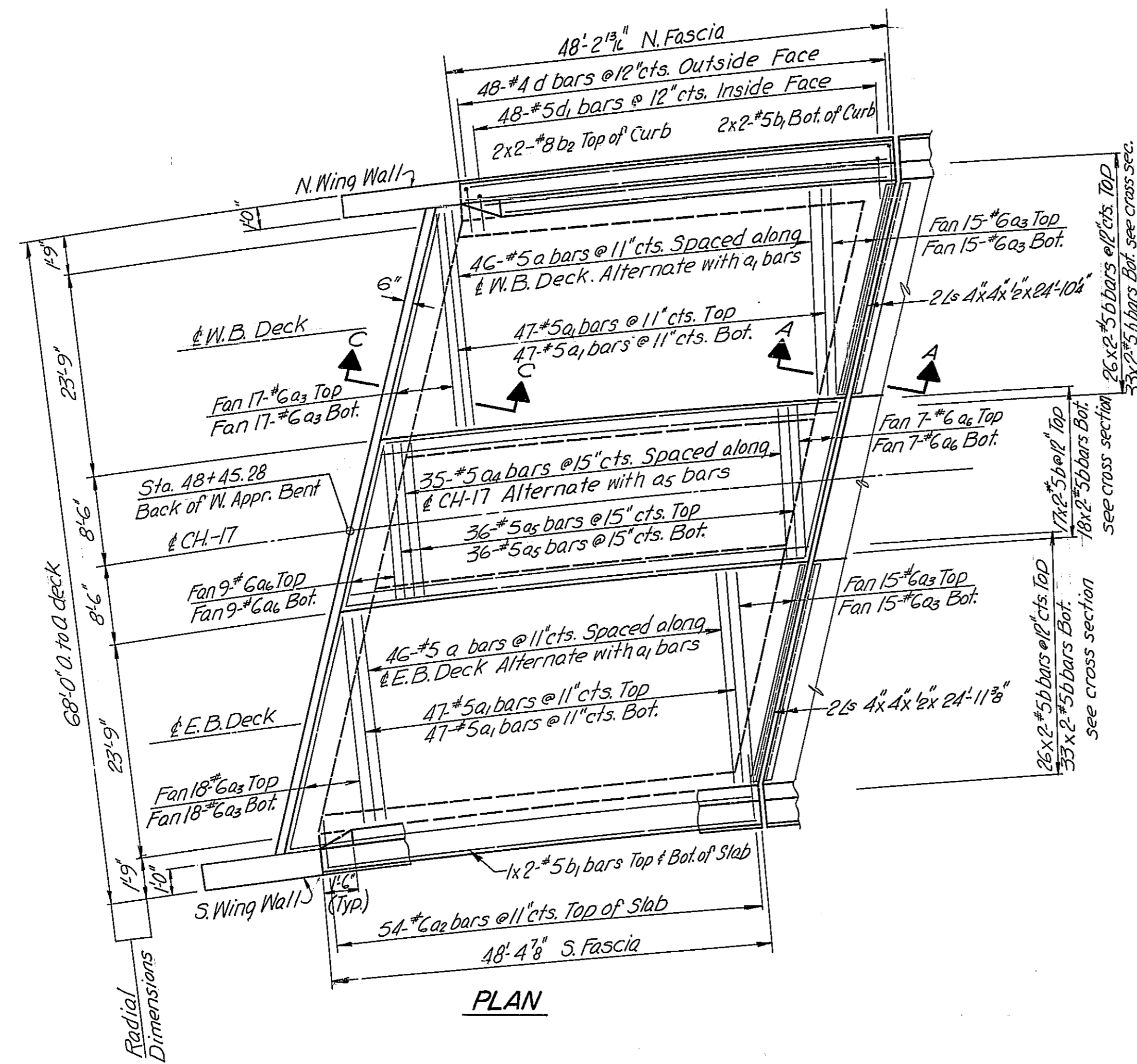
Location	Beam	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Q	1	50+45.687	31.000	669.230	669.275
	2	50+44.294	23.250	668.775	668.820
	3	50+42.883	15.500	668.320	668.365
	4	50+41.451	7.750	667.865	667.910
	5	50+40.000	0.000	667.410	667.455
	6	50+38.528	7.750	666.955	667.000
	7	50+37.036	15.500	666.500	666.545
	8	50+35.522	23.250	666.045	666.090
	9	50+34.000	31.000	665.590	665.635
R	1	50+55.424	31.000	669.103	669.148
	2	50+54.036	23.250	668.648	668.693
	3	50+52.719	15.500	668.193	668.238
	4	50+51.384	7.750	667.738	667.783
	5	50+50.000	0.000	667.283	667.328
	6	50+48.597	7.750	666.828	666.873
	7	50+47.173	15.500	666.373	666.418
	8	50+45.730	23.250	665.918	665.963
	9	50+44.265	31.000	665.463	665.508
S	1	50+65.160	31.000	668.970	669.015
	2	50+63.897	23.250	668.515	668.560
	3	50+62.616	15.500	668.060	668.105
	4	50+61.317	7.750	667.605	667.650
	5	50+60.000	0.000	667.150	667.195
	6	50+58.665	7.750	666.695	666.740
	7	50+57.310	15.500	666.240	666.285
	8	50+55.937	23.250	665.785	665.830
	9	50+54.543	31.000	665.330	665.375
T	1	50+74.897	31.000	668.830	668.875
	2	50+73.698	23.250	668.375	668.420
	3	50+72.482	15.500	667.920	667.965
	4	50+71.250	7.750	667.465	667.510
	5	50+70.000	0.000	667.010	667.055
	6	50+68.733	7.750	666.555	666.600
	7	50+67.447	15.500	666.100	666.145
	8	50+66.144	23.250	665.645	665.690
	9	50+64.821	31.000	665.190	665.235
U	1	50+84.634	31.000	668.684	668.729
	2	50+83.499	23.250	668.229	668.274
	3	50+82.349	15.500	667.774	667.819
	4	50+81.183	7.750	667.319	667.364
	5	50+80.000	0.000	666.864	666.909
	6	50+78.801	7.750	666.409	666.454
	7	50+77.585	15.500	665.954	665.999
	8	50+76.351	23.250	665.500	665.545
	9	50+75.099	31.000	665.045	665.090
V	1	50+94.370	31.000	668.532	668.577
	2	50+93.300	23.250	668.077	668.122
	3	50+92.215	15.500	667.622	667.667
	4	50+91.115	7.750	667.167	667.212
	5	50+90.000	0.000	666.712	666.757
	6	50+88.869	7.750	666.257	666.302
	7	50+87.722	15.500	665.802	665.847
	8	50+86.558	23.250	665.347	665.392
	9	50+85.377	31.000	664.892	664.937
E. Brgs. E. Abut.	1	51+04.650	31.000	668.365	668.410
	2	51+03.648	23.250	667.908	667.953
	3	51+02.673	15.500	667.452	667.497
	4	51+01.625	7.750	666.996	667.041
	5	51+00.642	0.000	666.540	666.585
	6	50+99.606	7.750	666.084	666.129
	7	50+98.555	15.500	665.628	665.673
	8	50+97.490	23.250	665.172	665.217
	9	50+96.410	31.000	664.716	664.761

Location	Beam	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
E. Brgs. E. Abut.	N Wall	51+06.419	32.237	668.410	668.410
	1	51+05.355	23.732	667.955	667.955

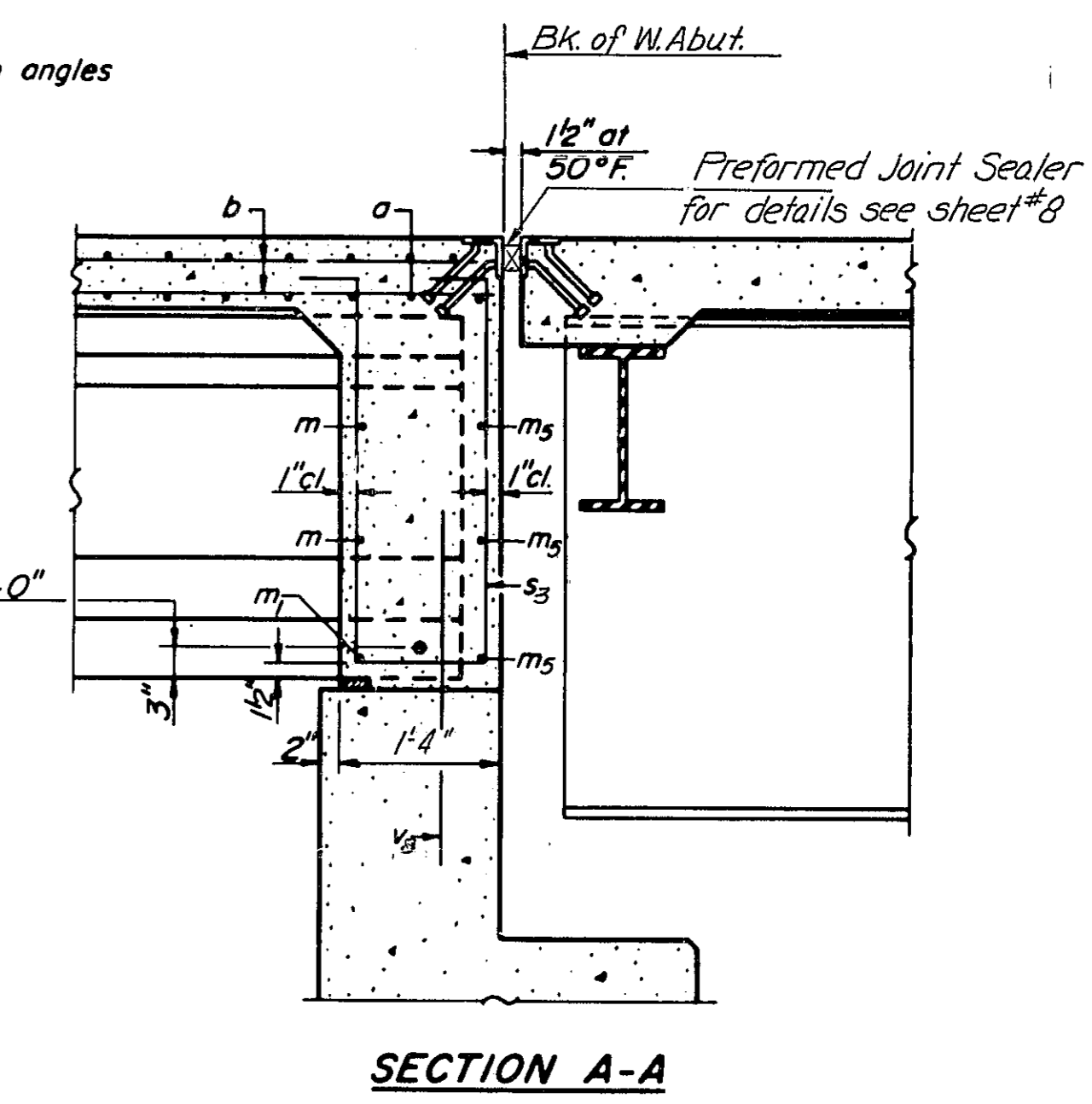
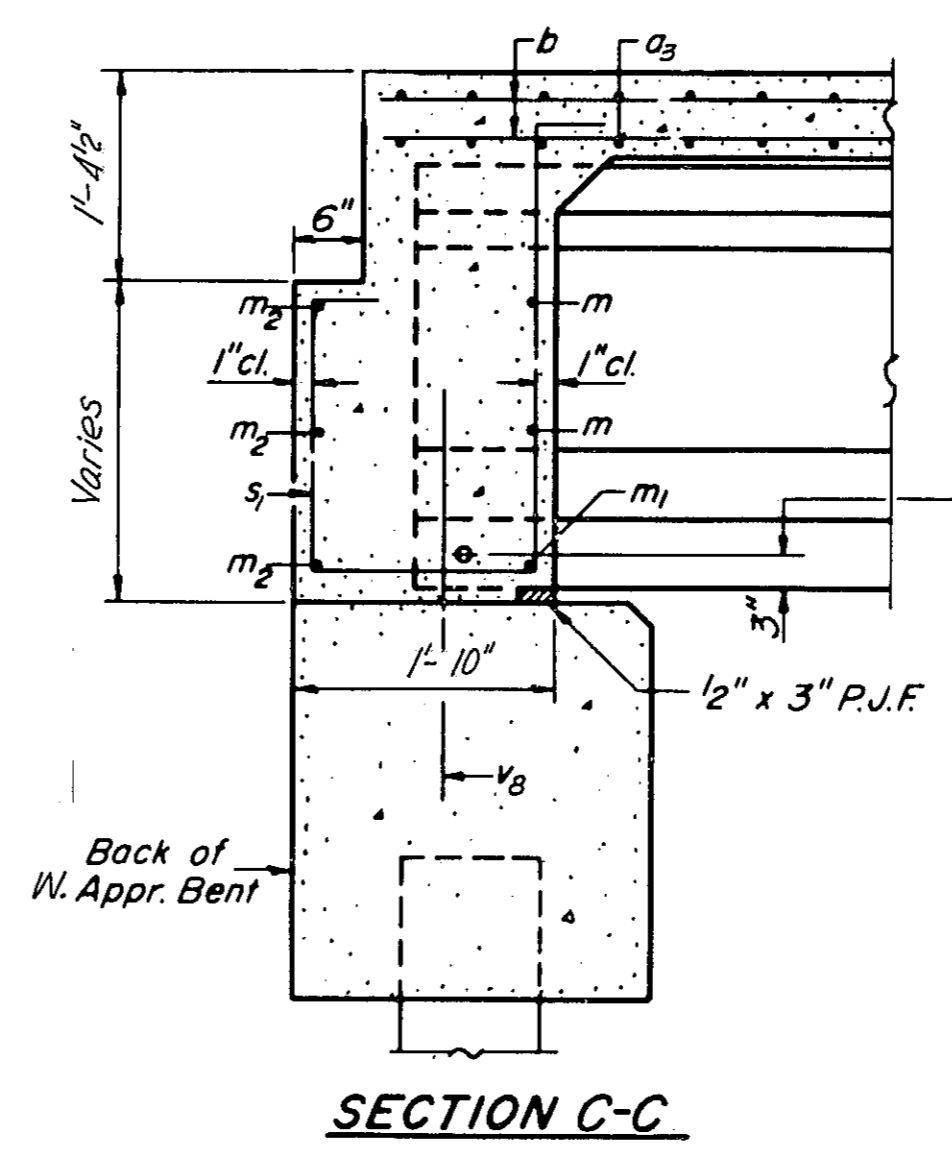
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	68-5HB	MONTGOMERY	206	65
FED. ROAD REG. NO. 4		ILLINOIS PROJECT	I-55-2()	

SHEET NO. 6



NOTE: For details of expansion angles see Detail 'A'

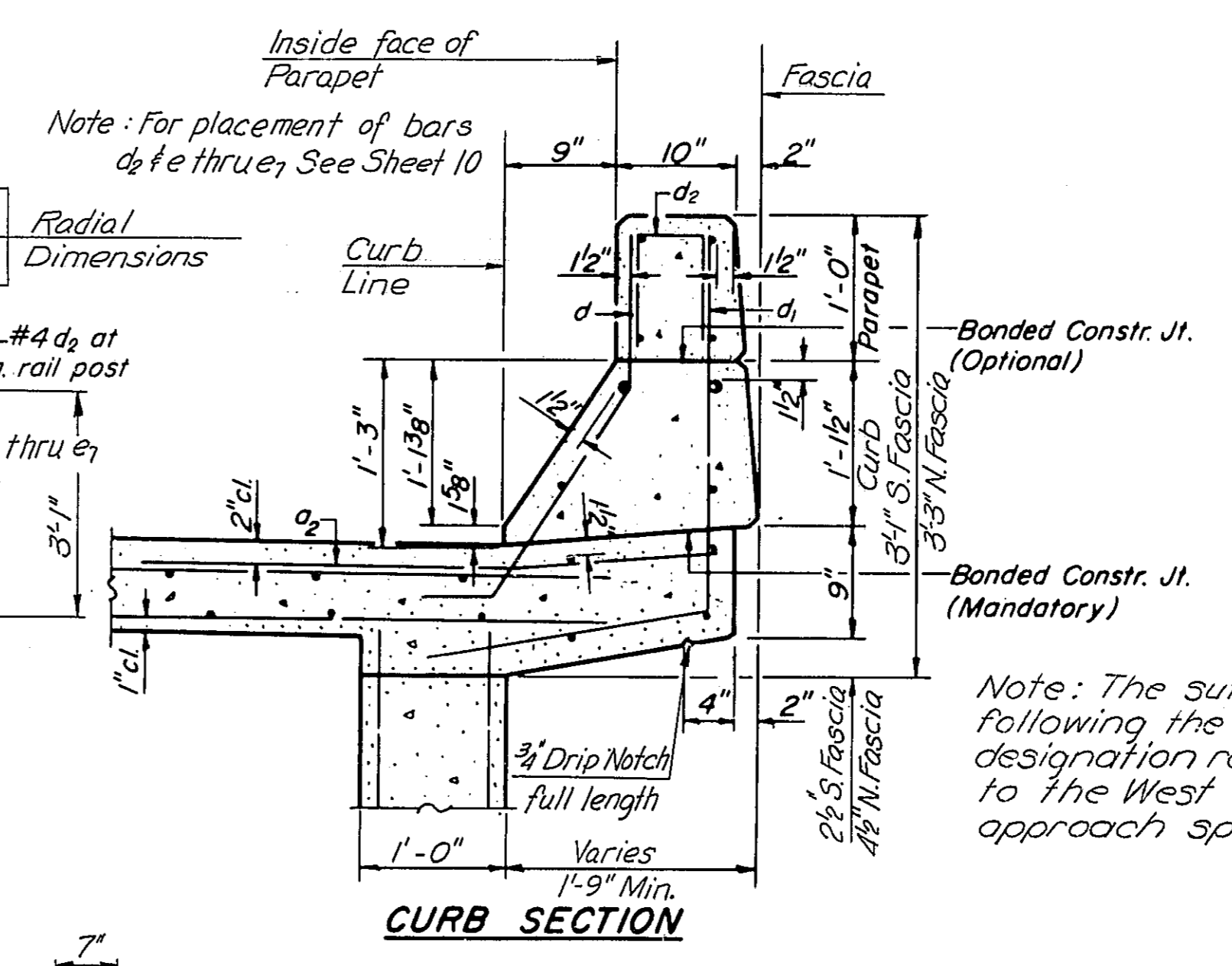
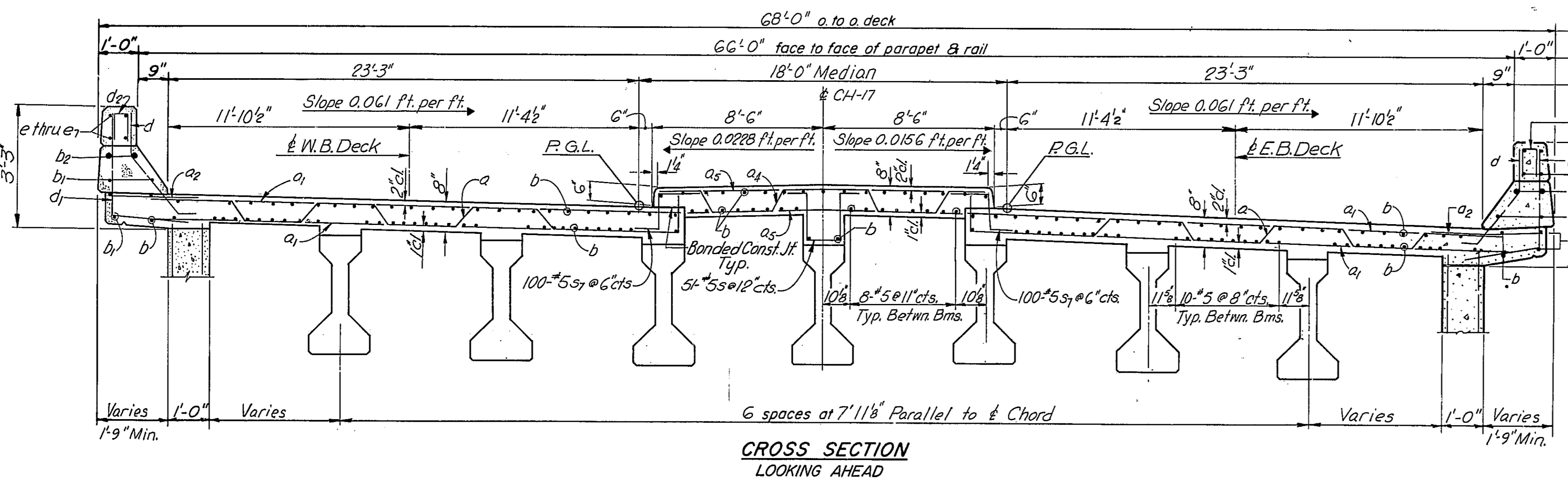


Note: For placement and details of bars m thru m₅ and s thru s₄ see sheet #7

W. APPROACH SPAN
BILL OF MATERIAL

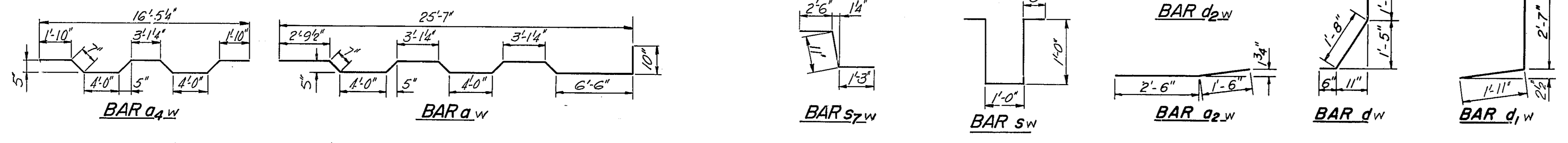
Bar	No.	Size	Length	Shape
a _{1w}	92	#5	27'-3"	
a _{2w}	188	#5	25'-9"	
a _{3w}	108	#6	4'-0"	
a _{4w}	130	#6	25'-6"	
a _{5w}	35	#5	17'-1"	
a _{6w}	72	#5	14'-3"	
a _{7w}	32	#C	14'-3"	
d _{1w}	96	#5	3'-3"	
d _{2w}	96	#4	11'-6"	
d _{3w}	20	#4	2'-1"	
m _{1w}	32	#4	7'-8"	
m _{2w}	9	#5	6'-2"	
m _{3w}	16	#4	7'-0"	
m _{4w}	16	#4	5'-9"	
m _{5w}	9	#5	22'-9"	
s _{1w}	51	#5	3'-6"	
s _{2w}	32	#4	8'-5"	
s _{3w}	10	#4	8'-11"	
s _{4w}	30	#4	9'-0"	
s _{5w}	10	#4	10'-2"	
s _{6w}	30	#4	8'-0"	
s _{7w}	10	#4	9'-0"	
s _{8w}	200	#5	4'-8"	
b _{1w}	306	#5	25'-9"	
b _{2w}	12	#5	24'-9"	
b _{3w}	8	#8	25'-3"	
Reinforcement Bars		Lbs.	28,100	
Class X Concrete		Cu Yds.	130.7	

Longitudinal Reinforcement and Class X Concrete in Parapet are billed on sheet #10



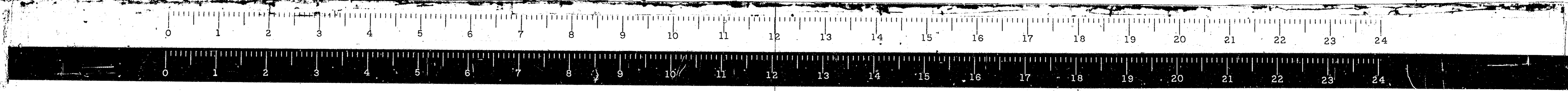
Note: The suffix w following the bar designation refers to the West approach span.

DESIGNED B.M.
CHECKED N.O.S.
DRAWN K.A.C.
CHECKED B.M.



DECK SLAB-W APPR. SPAN
CH-17 OVER FAI-55
STA. 1044+12.84

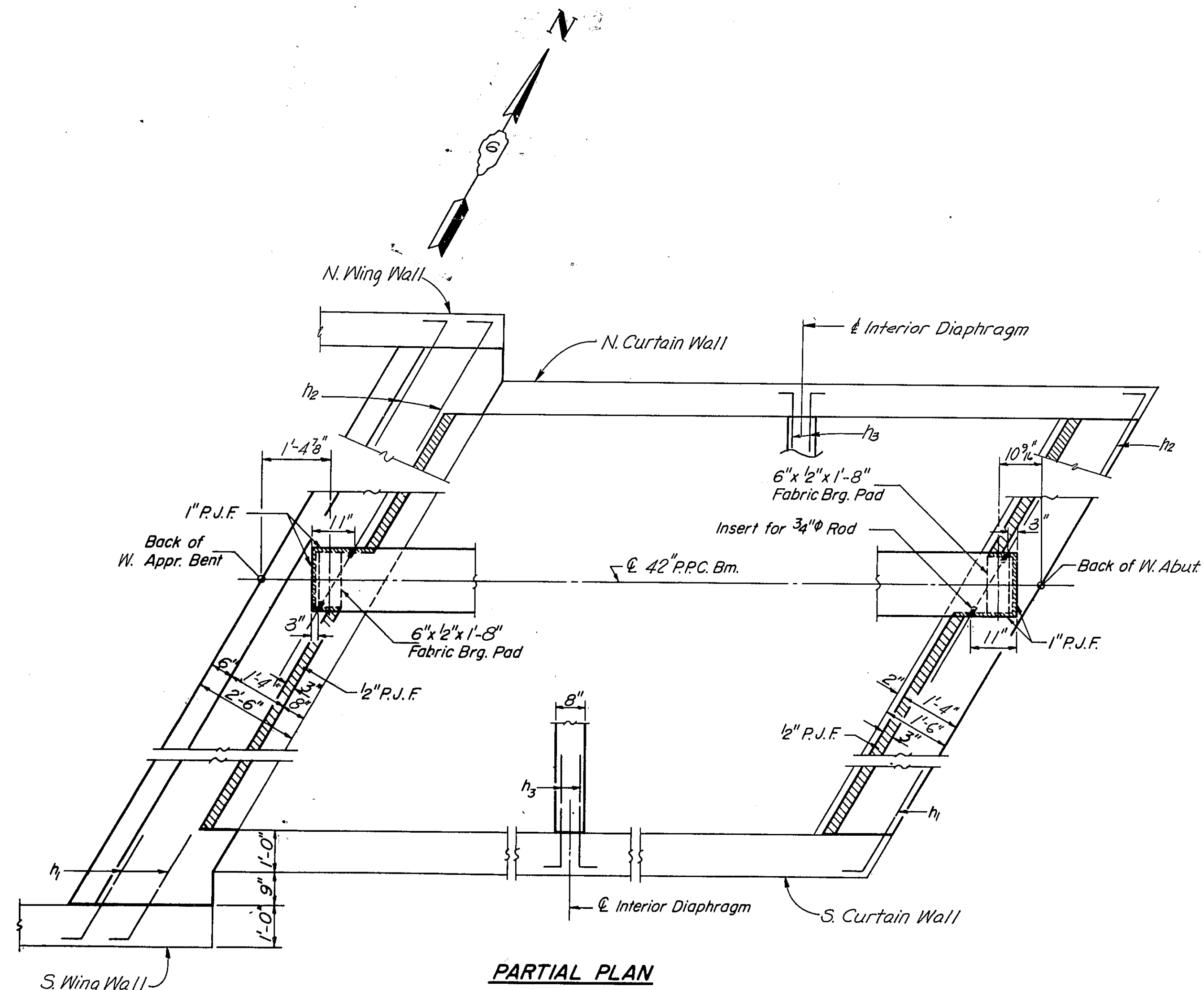
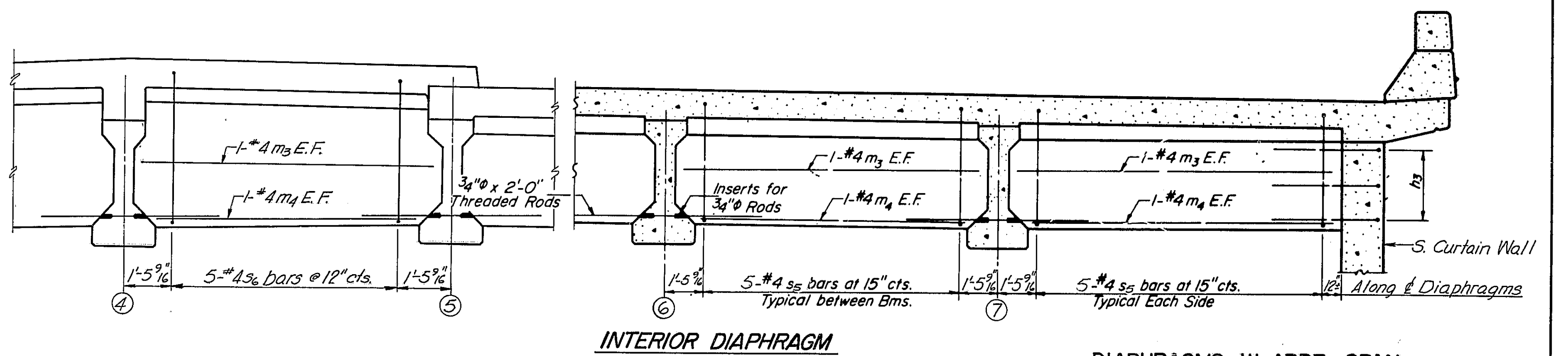
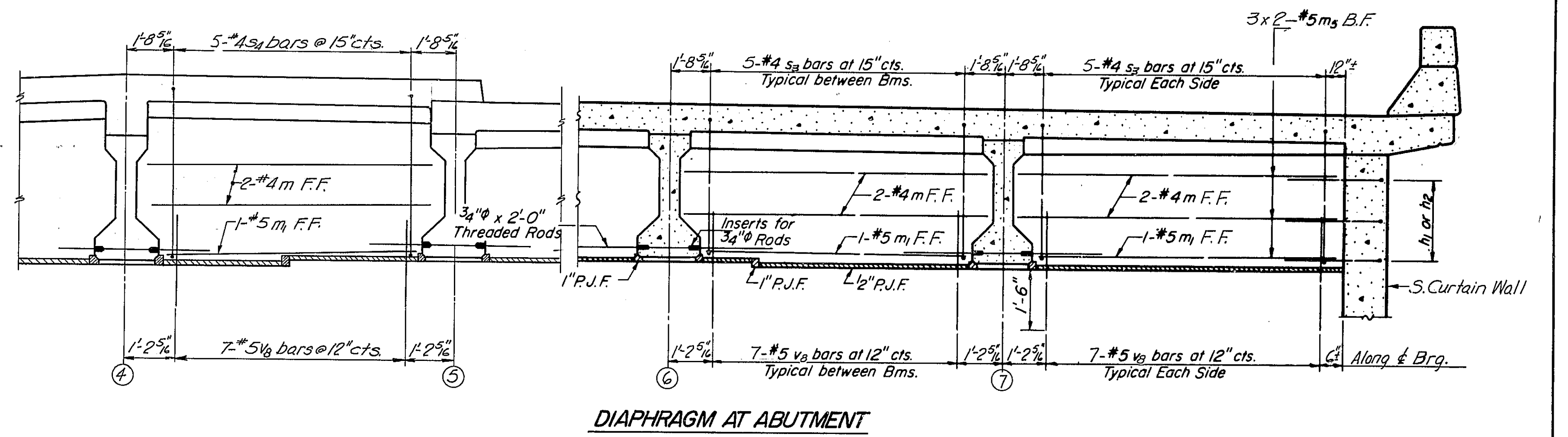
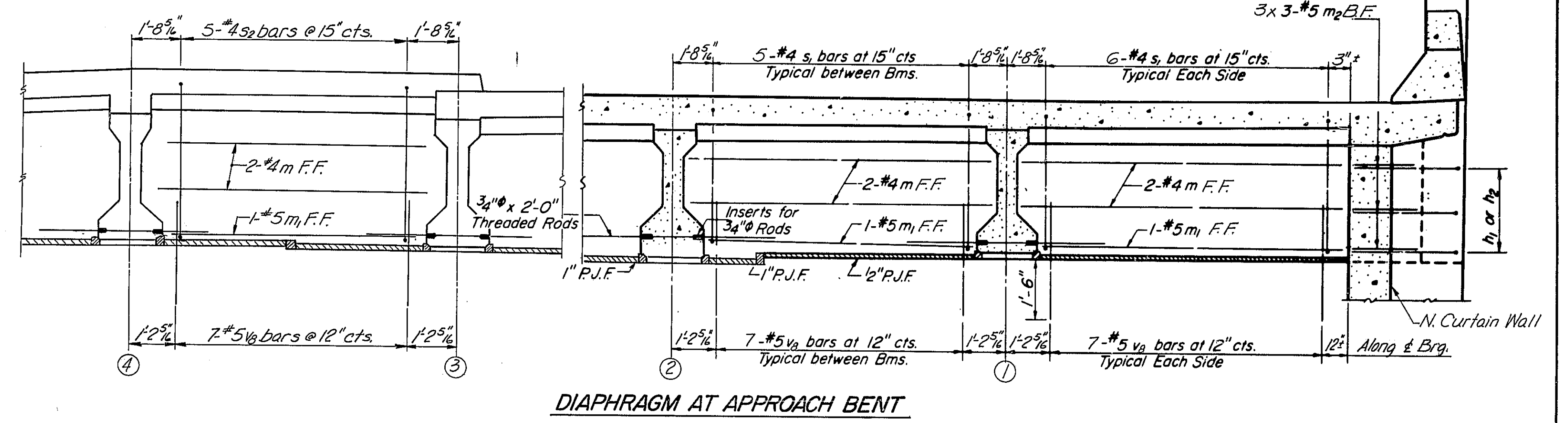
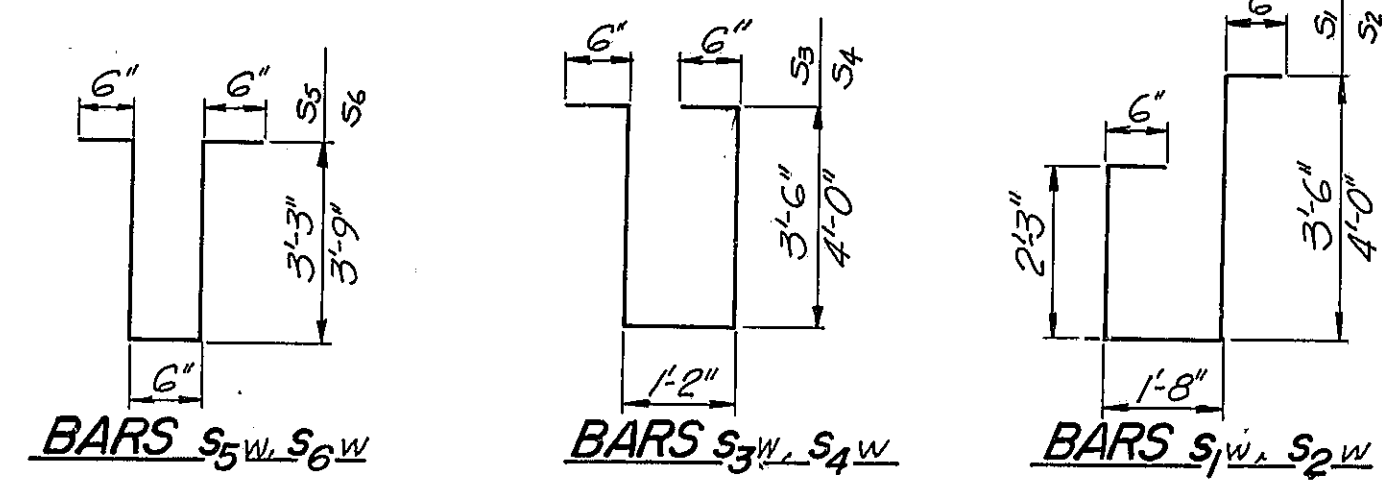
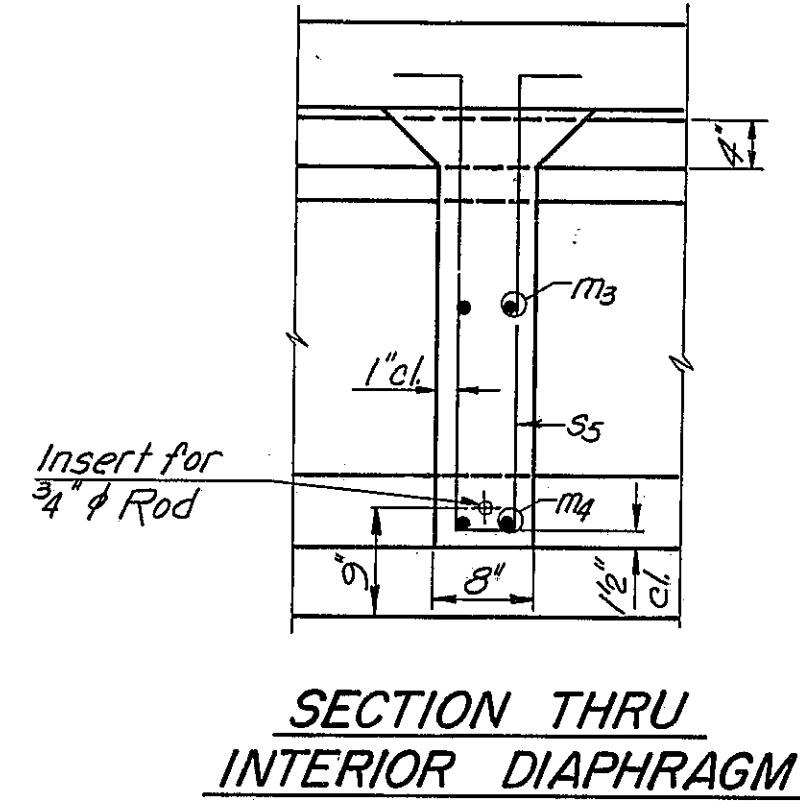
F.A.I. ROUTE 55
SECTION 68-5HB
Scale: None
Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I-55	68-5HB	MONTGOMERY	206	66
FED. ROAD REG. NO. 2	ILLINOIS	PROJECT I-55-2()		

SHEET NO. 7

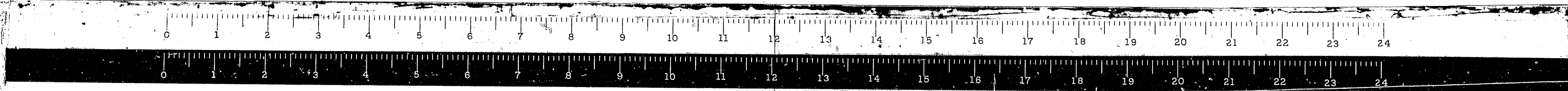


DIAPHRAGMS-W. APPR. SPAN
CH-17 OVER FA.I-55
STA. 1044+12.84

NOTES:
Bars s_1 thru s_6 and m thru m_2 are billed with W. Approach Slab Bill of Material on sheet #6
Bars h_1, h_2, h_3, a, v_1 are billed with Abutment Bill of Material on sheet #6
See sheet #6 for sections thru abutment and approach bent diaphragms.

F.A.I. ROUTE 55
SECTION 68-5HB
Scale: None
Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

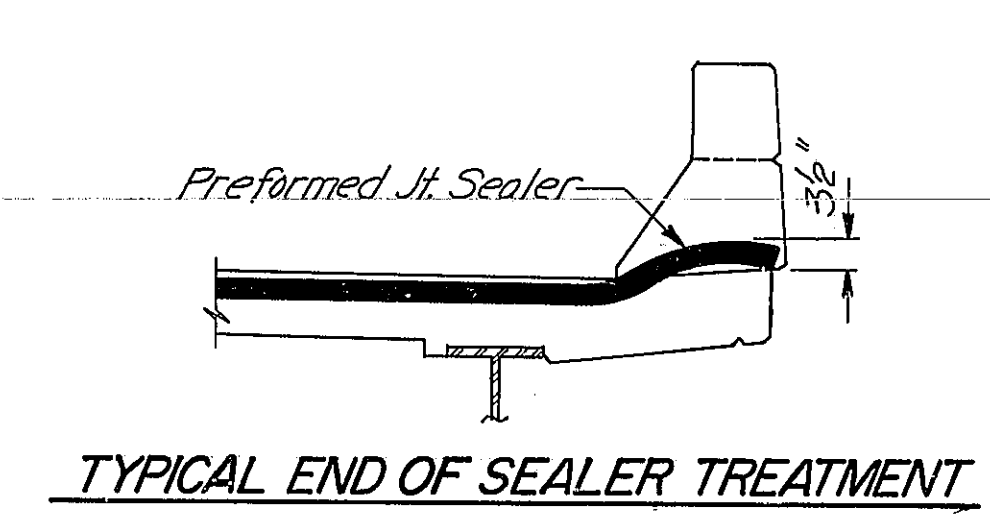
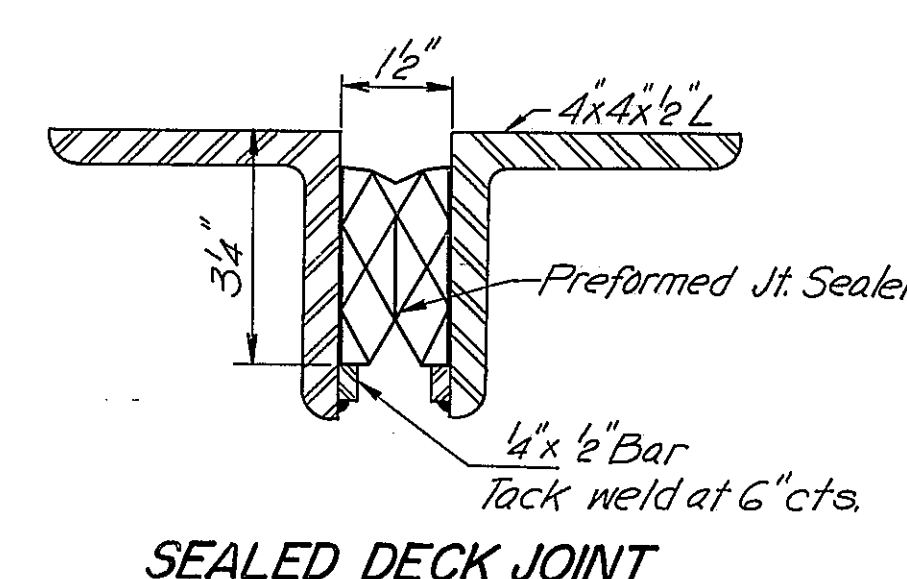
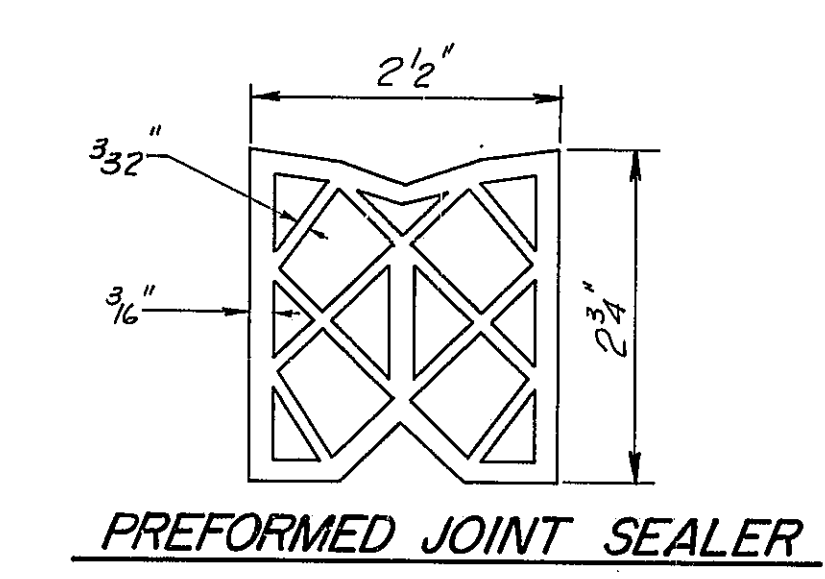
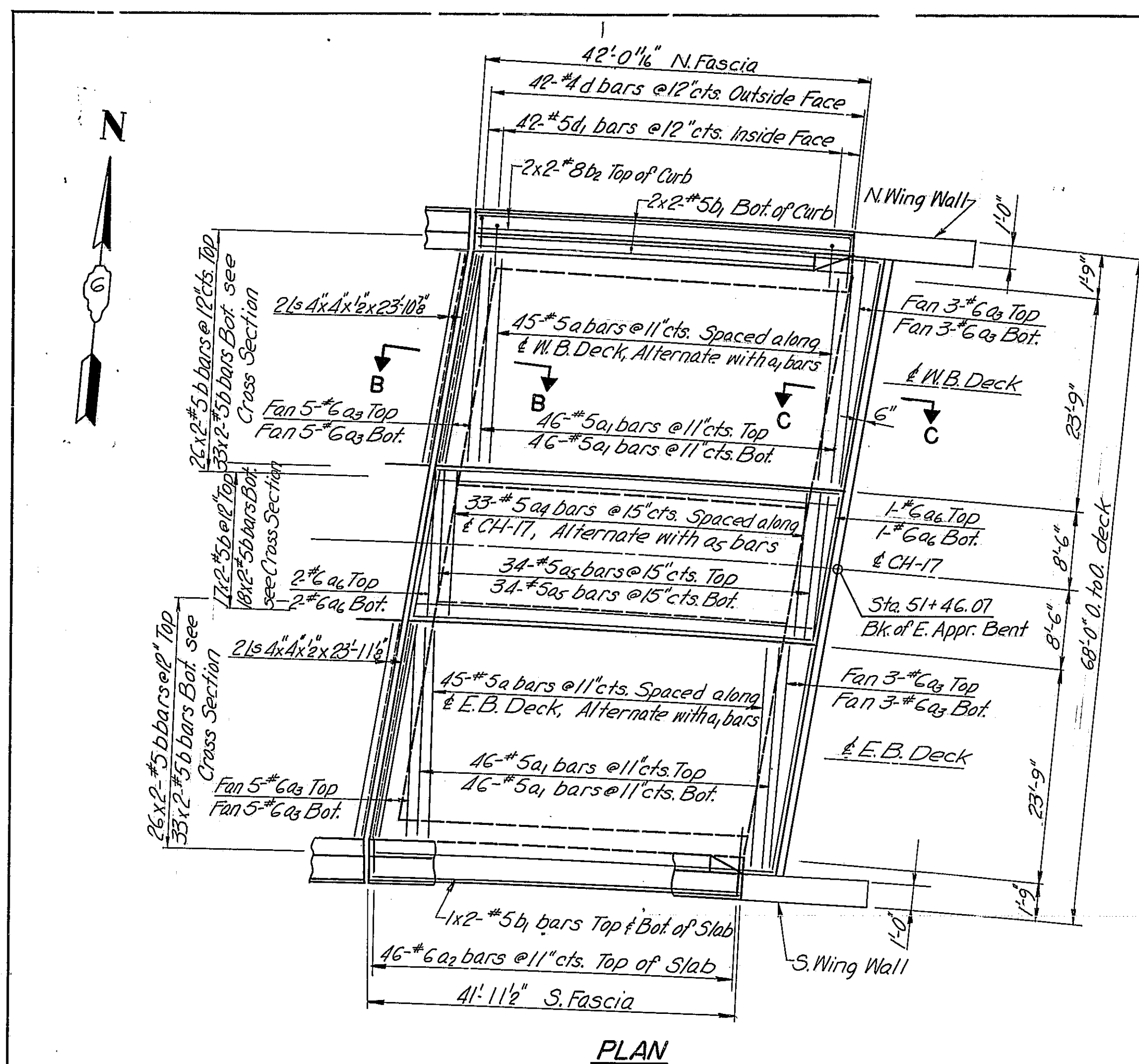
DESIGNED	B.M.
CHECKED	N.O.S.
DRAWN	K.A.C.
CHECKED	B.M.



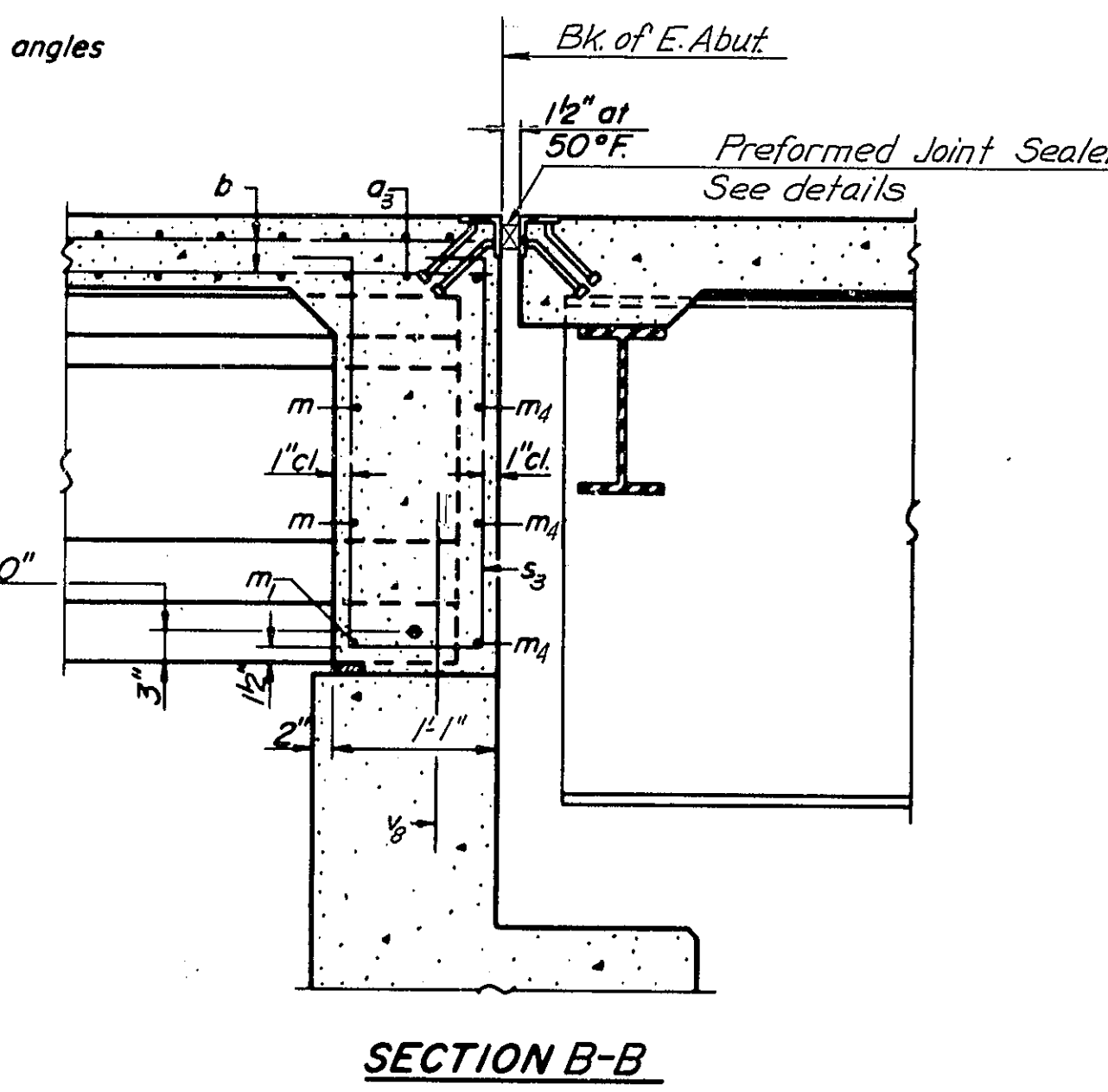
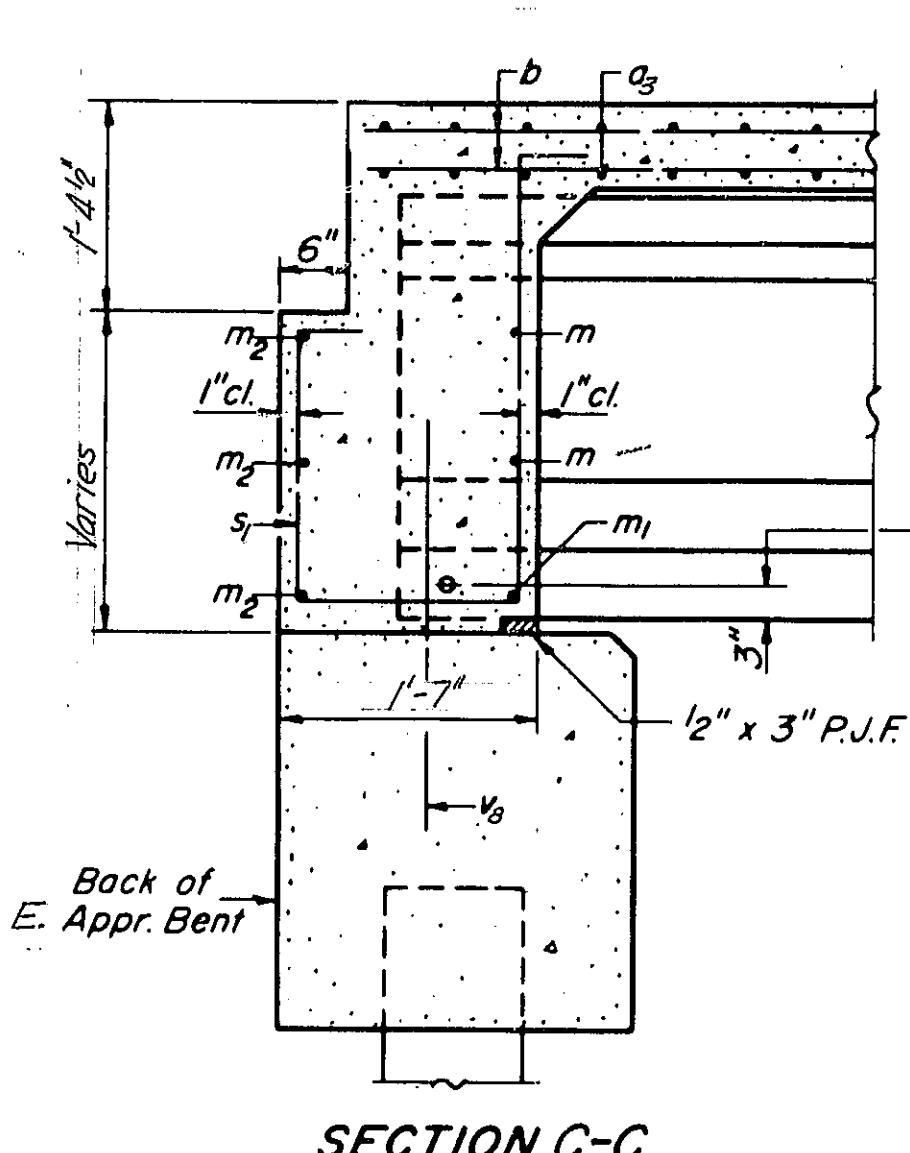
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	68-5HB	MONTGOMERY	206	67
FED. ROAD REG. NO. 4	ILLINOIS	PROJECT I-55-2()		

SHEET NO. 8



NOTE: For details of expansion angles see Detail 'A' sheet #6



Note: For placement and details of bars m thru m₄ and s thru s₄ see sheet #9

E. APPROACH SPAN
BILL OF MATERIAL

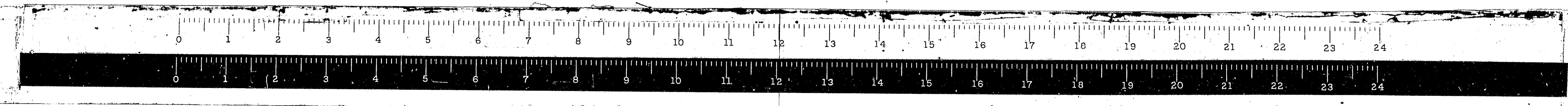
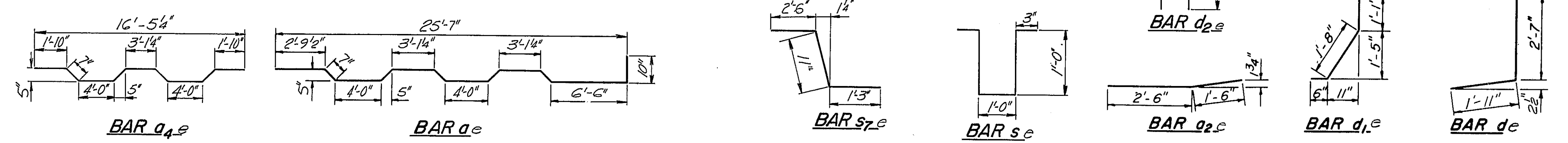
Bar	No.	Size	Length	Shape
a _{2e}	90	#5	27'-3"	
a _{1e}	184	#5	25'-9"	
a _{2e}	92	#6	4'-0"	
a _{3e}	32	#6	25'-6"	
a _{4e}	33	#5	17'-1"	
a _{5e}	68	#5	14'-3"	
a _{6e}	6	#6	14'-3"	
d _{2e}	81	#5	3'-3"	
d _{1e}	84	#4	4'-6"	
d _{3e}	20	#4	2'-1"	
m _{2e}	48	#4	7'-2"	
m _{1e}	16	#5	5'-10"	
m _{3e}	9	#5	22'-9"	
m _{4e}	16	#4	5'-10"	
m _{5e}	9	#5	21'-9"	
s _{2e}	44	#5	3'-6"	
s _{1e}	30	#4	8'-5"	
s _{3e}	10	#4	8'-11"	
s _{4e}	30	#4	9'-2"	
s _{5e}	10	#4	10'-2"	
s _{6e}	30	#4	8'-0"	
s _{7e}	10	#4	9'-0"	
b _{2e}	174	#5	4'-8"	
b _{1e}	306	#5	22'-6"	
b _{3e}	12	#5	21'-6"	
b _{4e}	8	#8	22'-0"	
Reinforcement Bars			Lbs.	21,360
Class X Concrete			Cu. Yds.	111.7

Longitudinal Reinforcement and Class X Concrete in parapet are billed on sheet #10

DECK SLAB-E. APPR. SPAN
CH-17 OVER FAI-55
STA. 1044+12.84

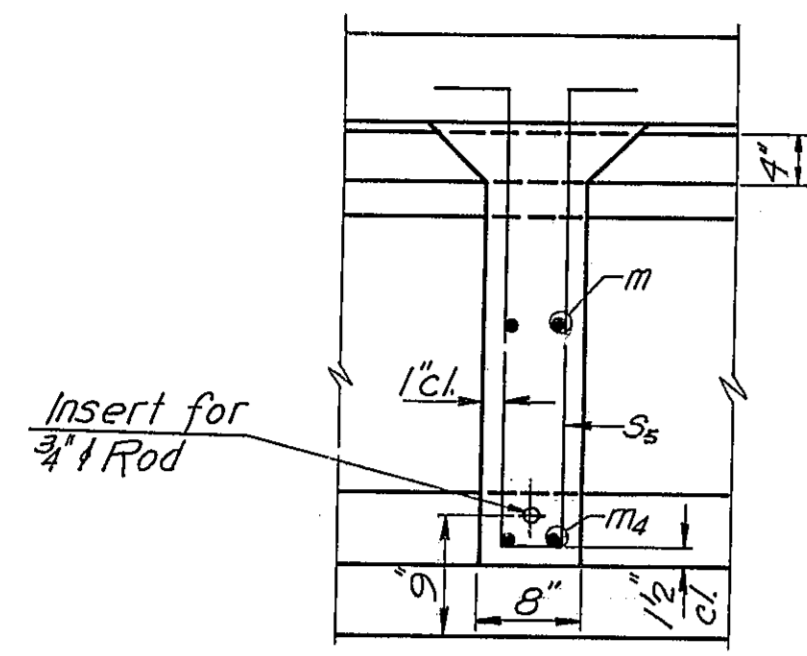
F.A.I. ROUTE 55
SECTION 68-5HB
Scale: None
Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

DESIGNED B.M.
CHECKED N.O.S.
DRAWN K.A.C.
CHECKED B.M.

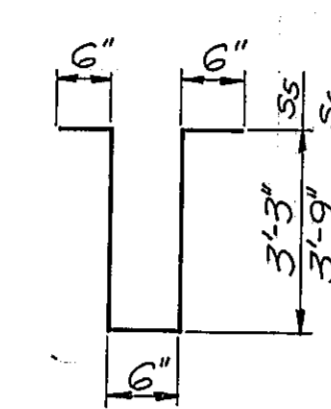


STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

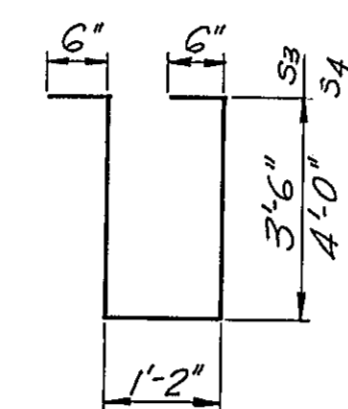
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 9
FA. I-55	68-SHB	MONTGOMERY	206	68	
FED. ROAD REG. NO. 4	ILLINOIS	PROJECT I-55-2()			



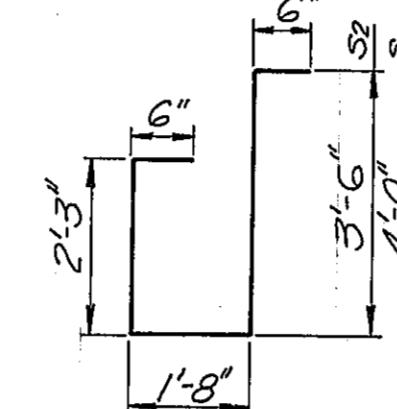
SECTION THRU
INTERIOR DIAPHRAGM



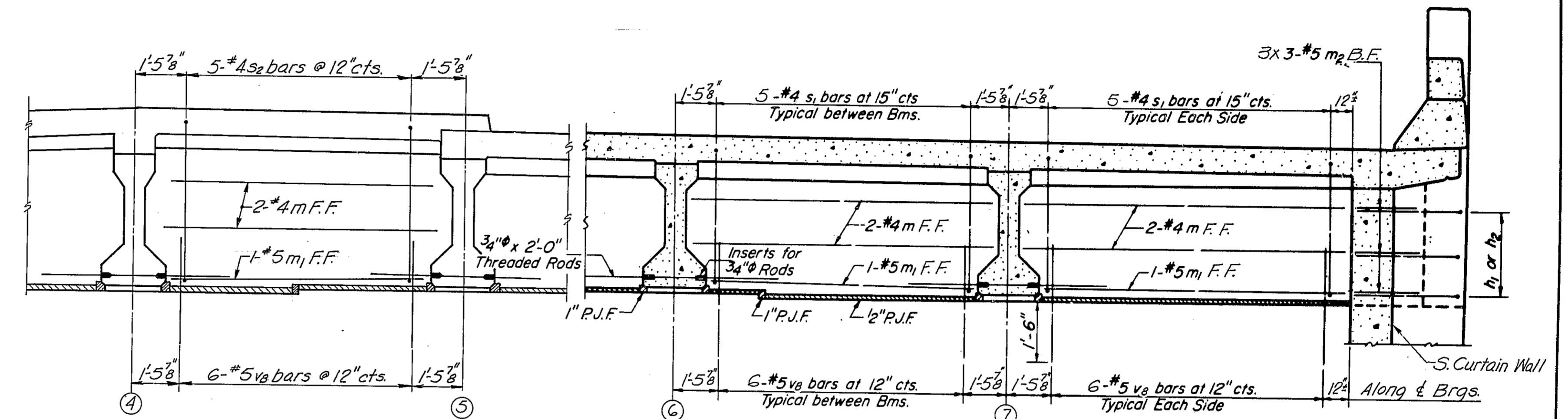
BARS s_5, s_6



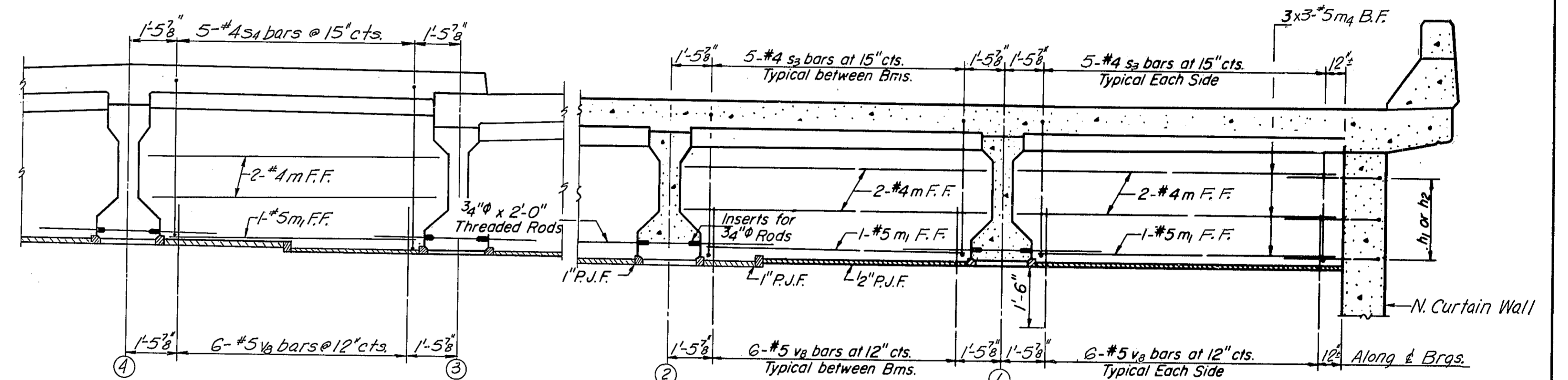
BARS s_3, s_4



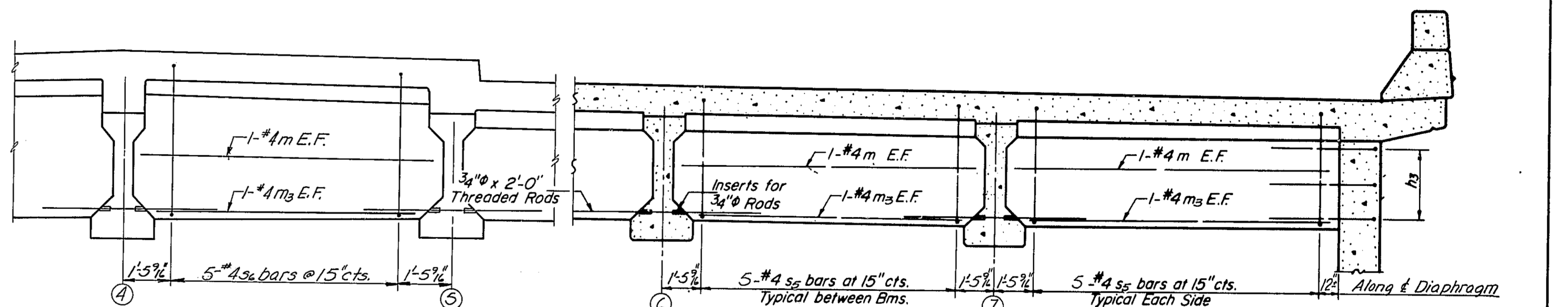
BARS s_1, s_2



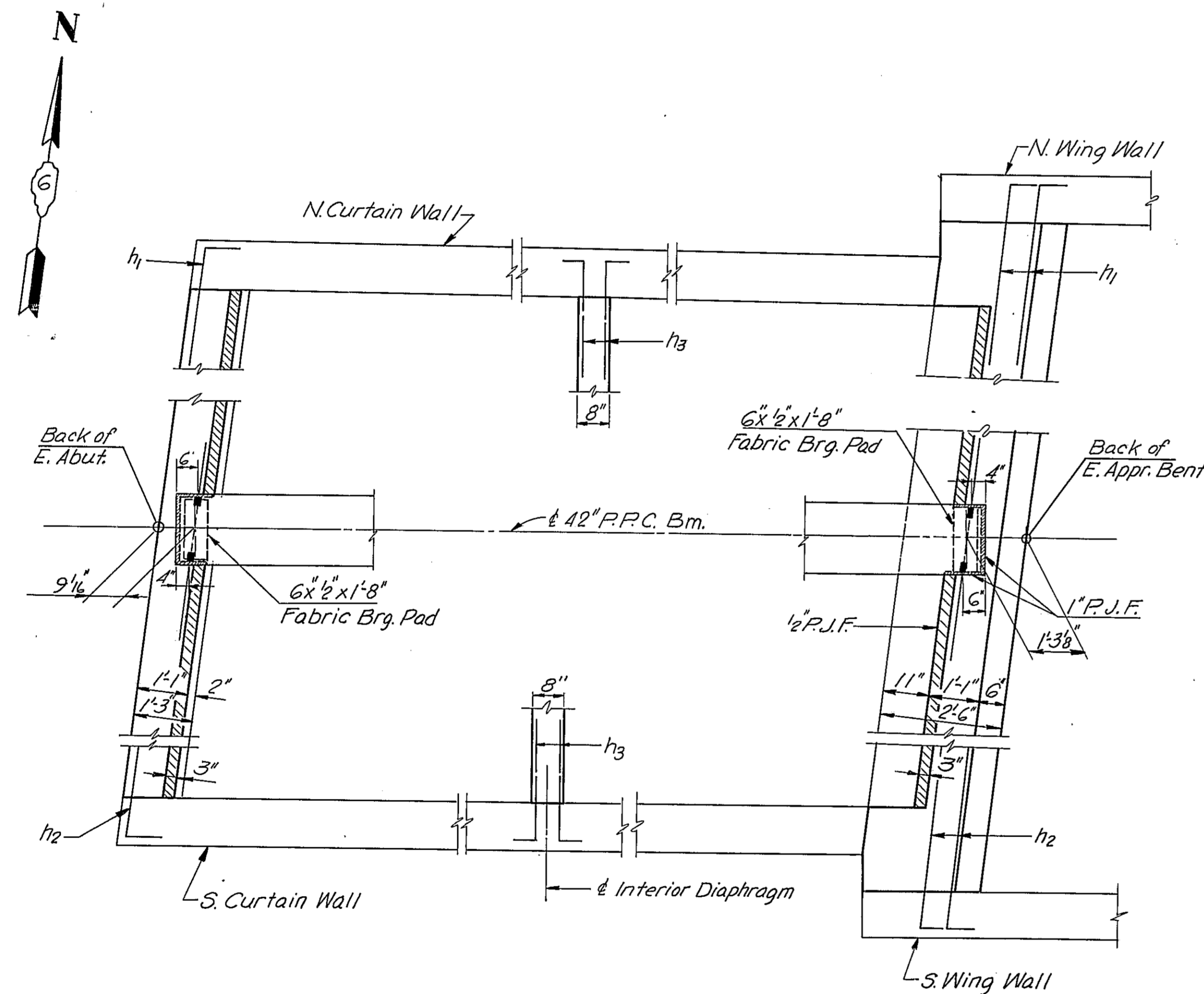
DIAPHRAGM AT APPROACH BENT



DIAPHRAGM AT ABUTMENT



INTERIOR DIAPHRAGM



PARTIAL PLAN

NOTES:
Bars s_1 thru s_6 and m thru m_4 are billed with E. Approach Slab Bill of Material on sheet #8
Bars h_1, h_2, h_3 & v_6 are billed with Abutment Bill of Material on sheet #18
See sheet #8 for sections thru abutment and approach bent diaphragms.

DIAPHRAGMS-E. APPR. SPAN
CH-17 OVER FA. I-55
STA. 1044+12.84

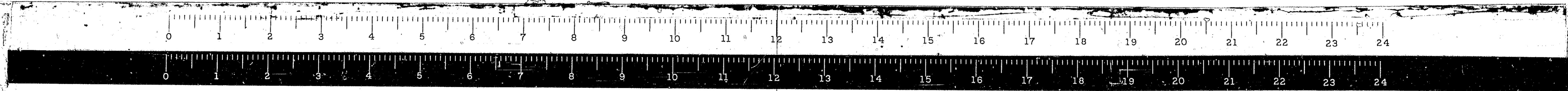
F.A.I. ROUTE 55 SECTION 68-SHB PROJECT I-55-2() MONTGOMERY COUNTY

Scale: None

Date:

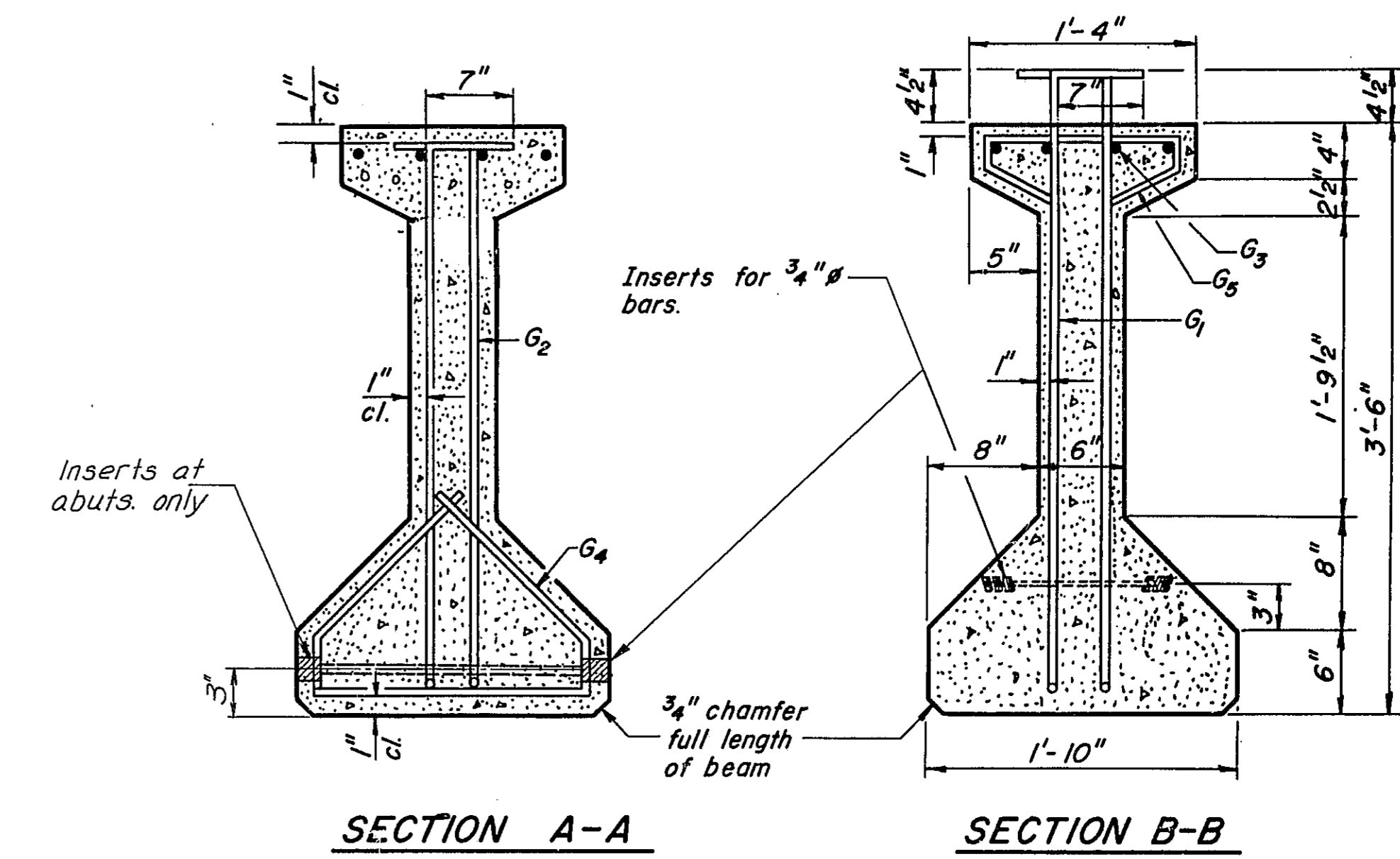
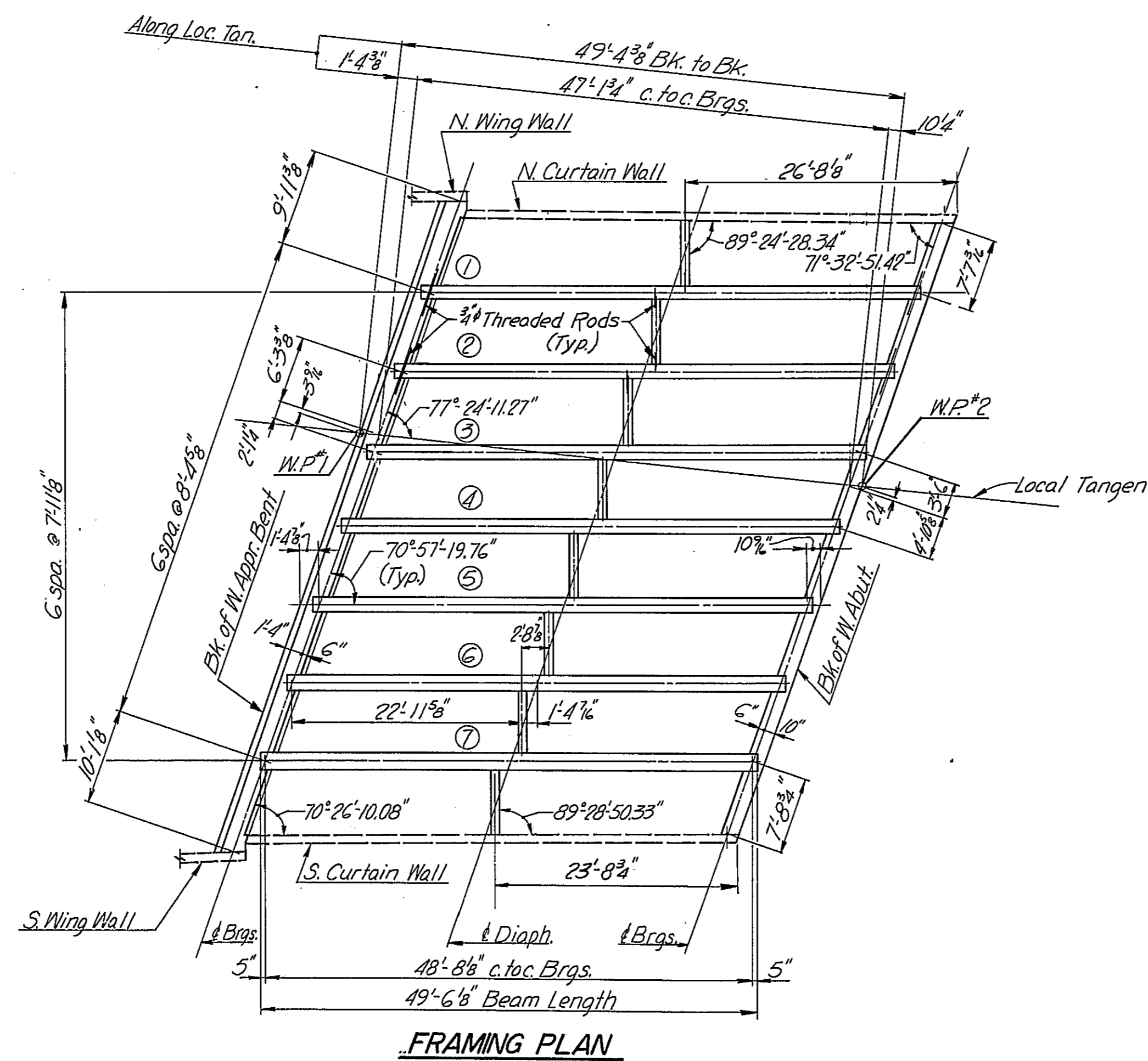
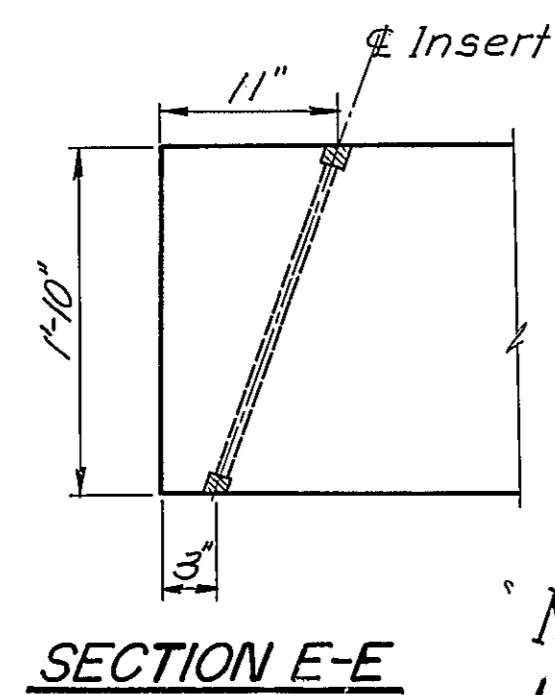
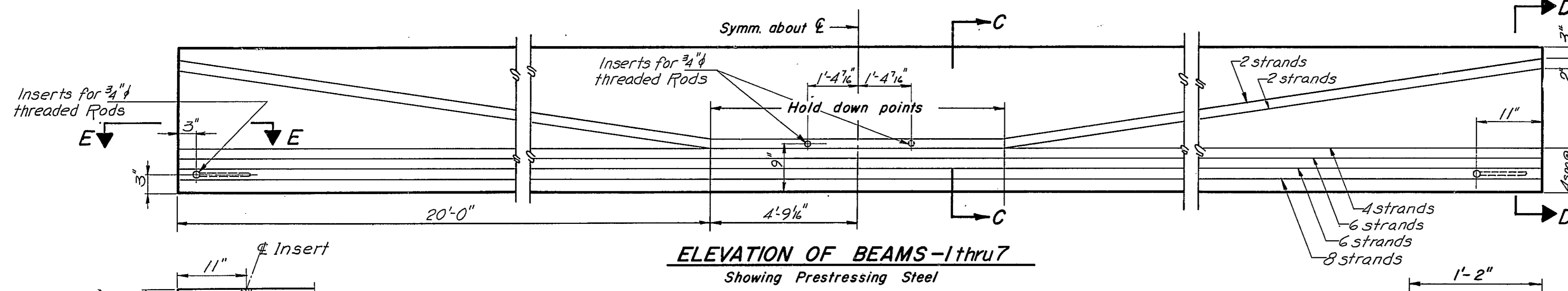
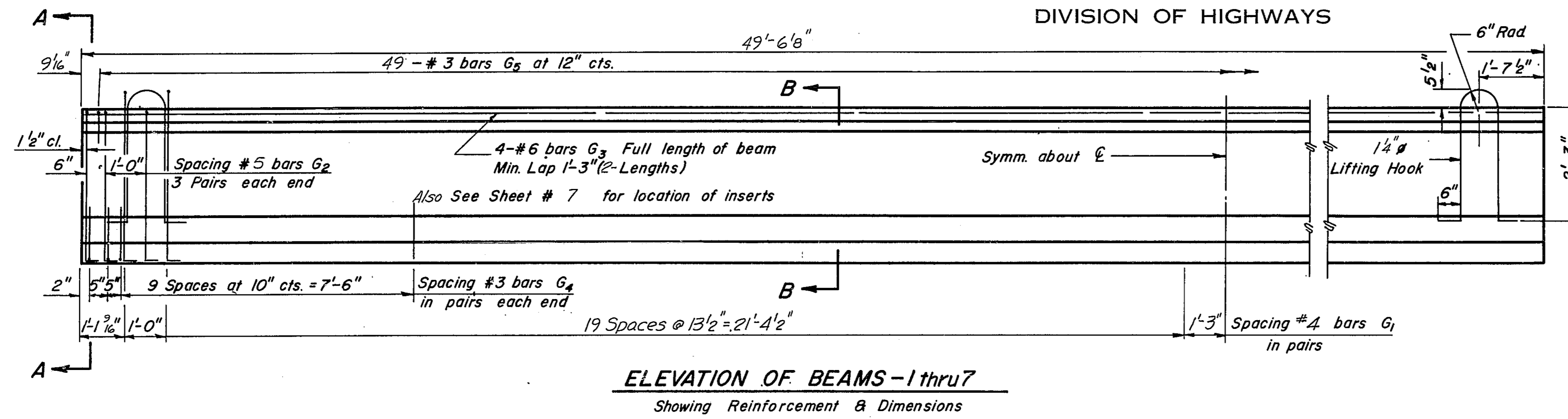
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

DESIGNED	B.M.
CHECKED	N.O.S.
DRAWN	K.A.C.
CHECKED	B.M.

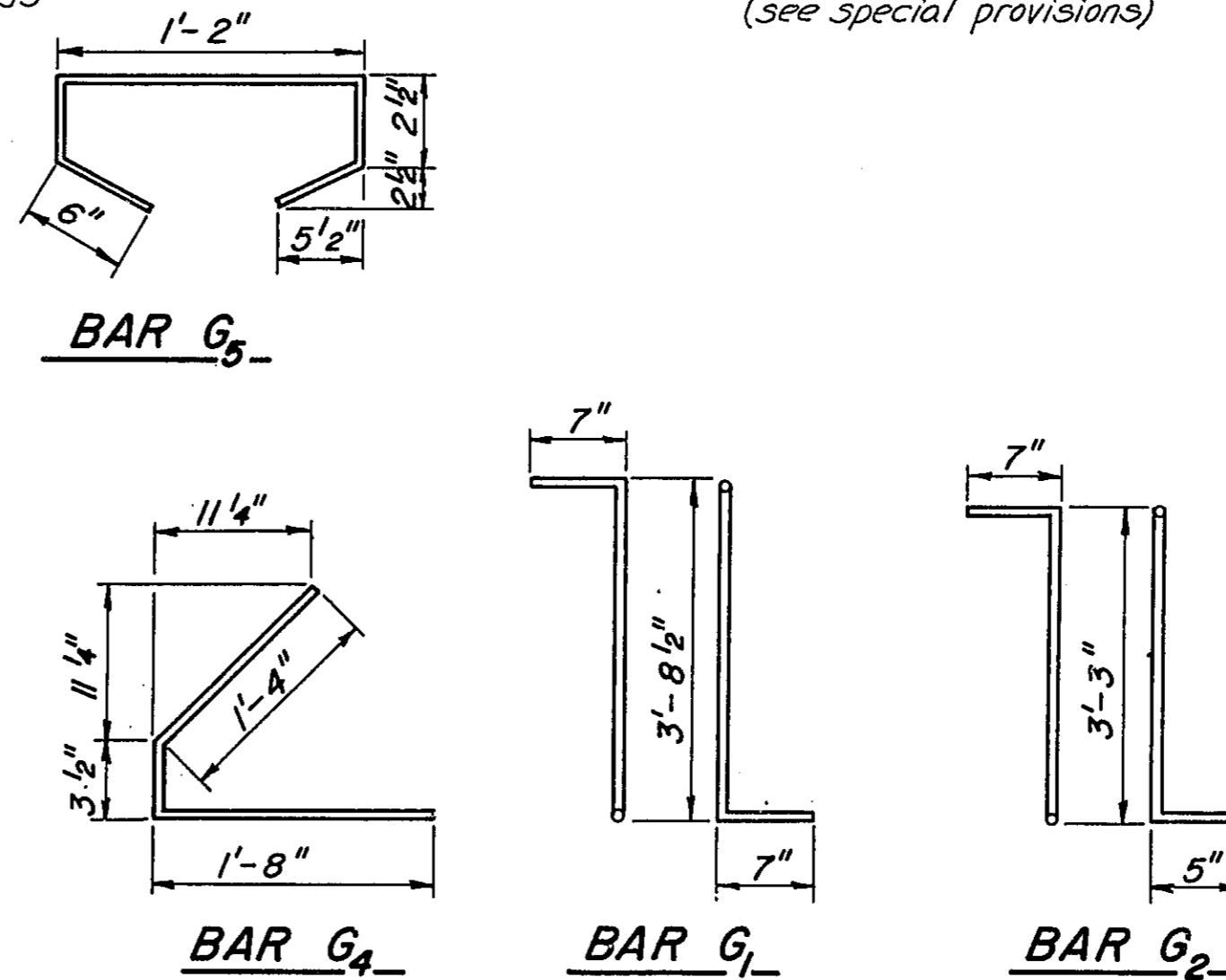
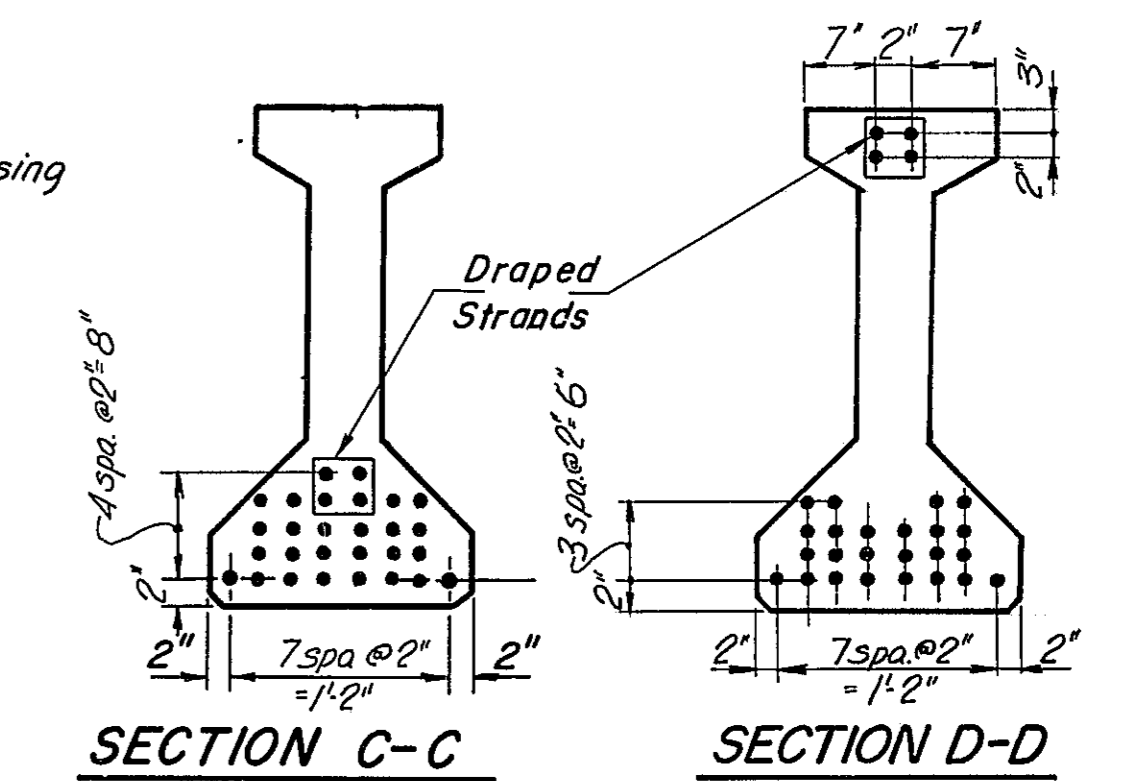


STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO.//
F.A.I.-55	68-5HB	MONTGOMERY	206	70	
FED. ROAD REG. NO. 4	ILLINOIS	PROJECT I-55-2()			



Note: An alternate strand pattern using Extra High Strength Prestressing Strand (270 KSI) is permitted. (See special provisions)



*** BAR LIST**

Bar	No.	Size	Length	Shape
G1	86	#4	4'-10 1/2"	TL
G2	12	#5	4'-3"	TL
G3	8	#6	25'-6"	—
G4	48	#3	3'-3 1/2"	—
G5	49	#3	2'-7"	—

* For one beam only.

NOTES

All inserts and threaded rods for inserts, reinforcing and Prestressing Steel, and other items which are cast into the Precast Concrete I-Beams shall be included in the contract unit price per lineal foot of "Furnishing And Erecting Precast Prestressed Concrete I-Beams, 42 In."

See Specifications for additional information regarding materials, Prestressing equipment, construction and handling methods and other requirements for Precast Prestressed Concrete I-Beams.

Prestressing Steel shall have a nominal diameter of 1/8."

Inserts for 3/4" threaded rods are to be two strut, coil type for interior I-Beams and single coil, flared loop type for exterior I-Beams.

Steel for lifting hooks shall be non-deformed bars of structural or intermediate grade billet steel.

BILL OF MATERIAL

Item	Unit	Total
Furnishing & Erecting Precast Prestressed Concrete I-Beams, 42"	Lin. Ft.	347

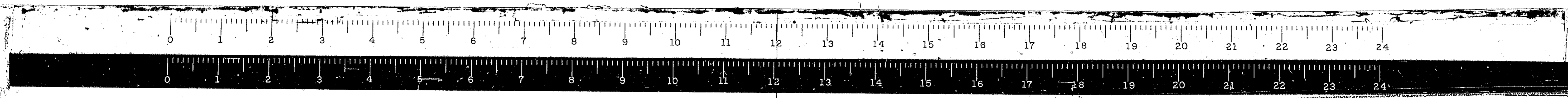
BEAM DETAILS W. APPROACH SPAN
CH-17 OVER F.A.I. 55
STA. 1044+12.84

F.A.I. ROUTE 55 PROJECT I-55-2()
SECTION 68-5HB MONTGOMERY COUNTY

Scale: None Date: _____

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

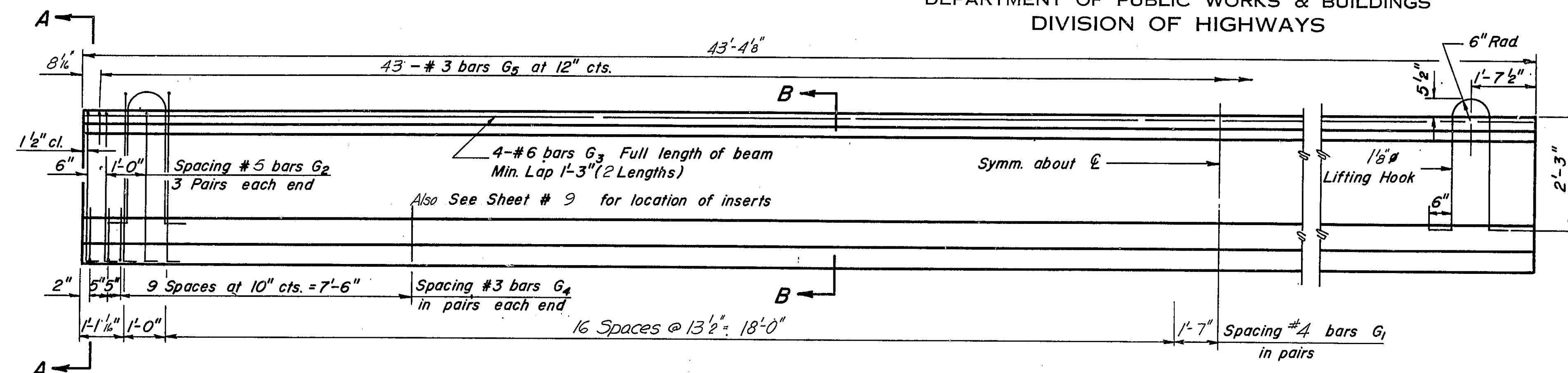
DESIGNED N.O.S.
CHECKED B.M. & R.D.W.
DRAWN K.A.C.
CHECKED B.M.



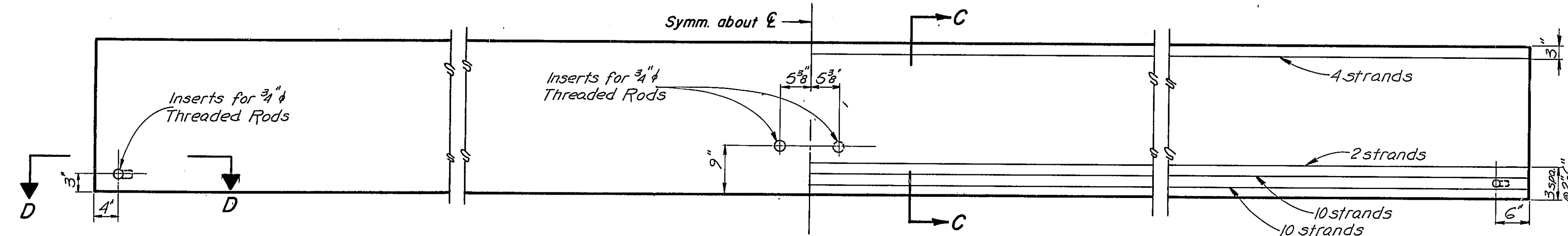
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I-55	68-5HB	MONTGOMERY	206	71
FED. ROAD REG. NO. 4	ILLINOIS	PROJECT	1-55-2()	

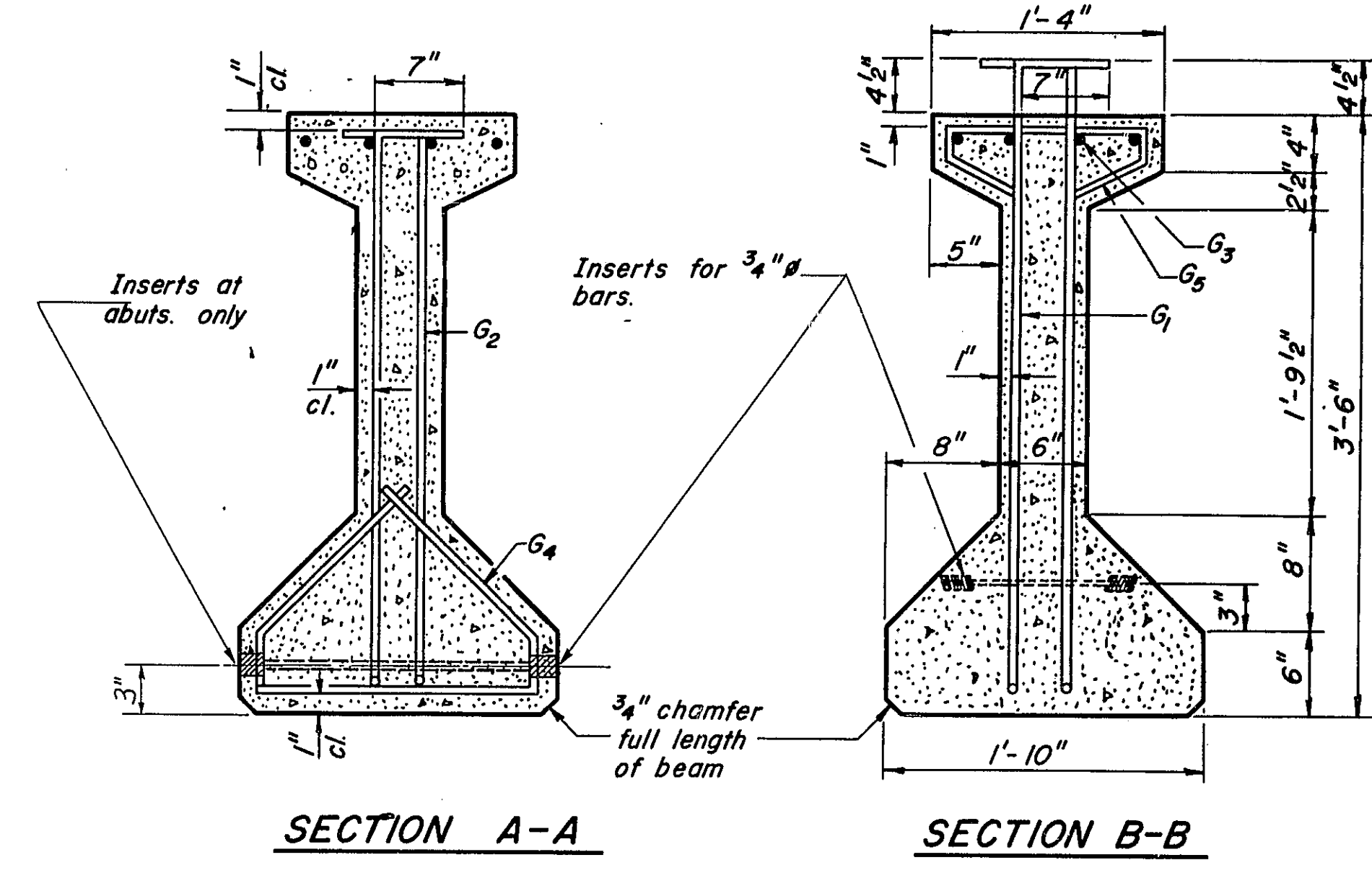
SHEET NO. 12



ELEVATION OF BEAMS-1 thru 7
Showing Reinforcement & Dimensions

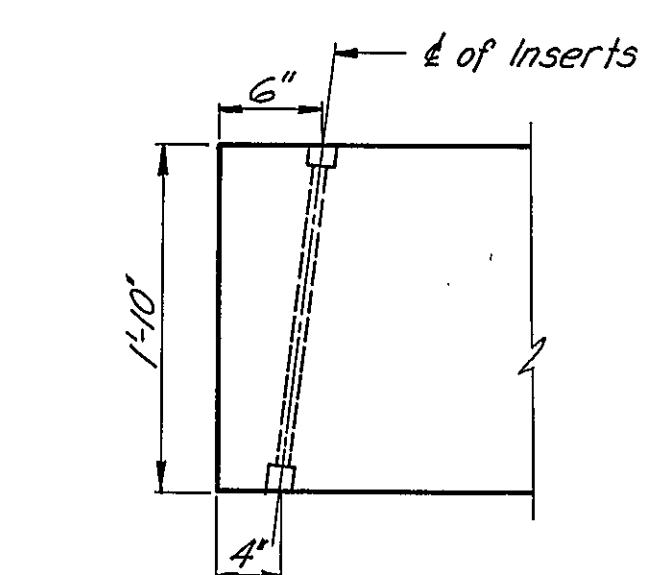


ELEVATION OF BEAMS-1 thru 7
Showing Prestressing Steel

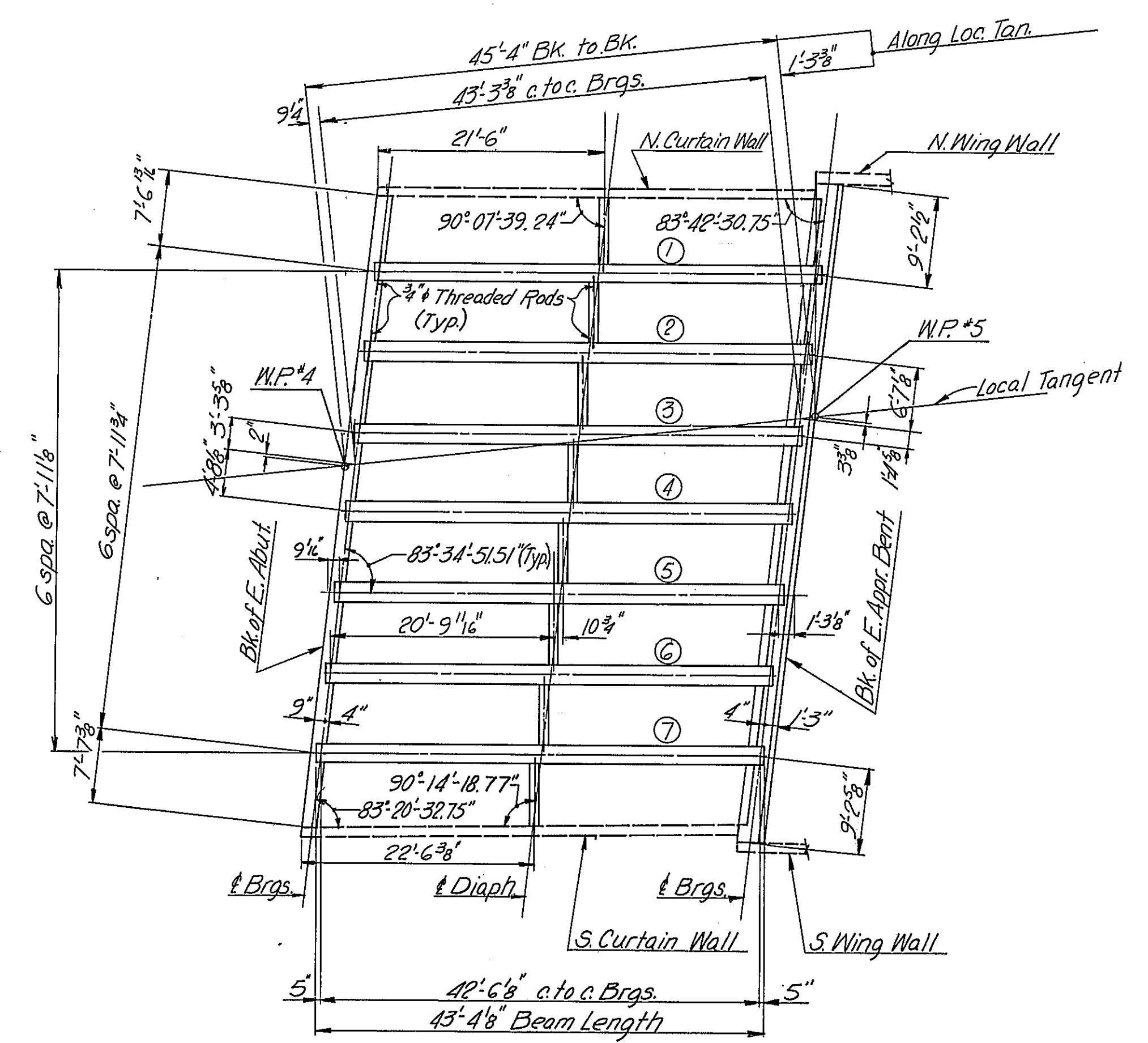


SECTION A-A **SECTION B-B**

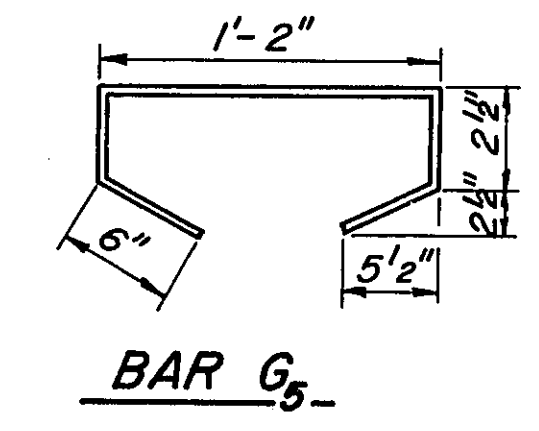
Note: An alternate strand pattern using Extra High Strength Prestressing Strand (270 K.S.I.) is permitted (See Special provisions)



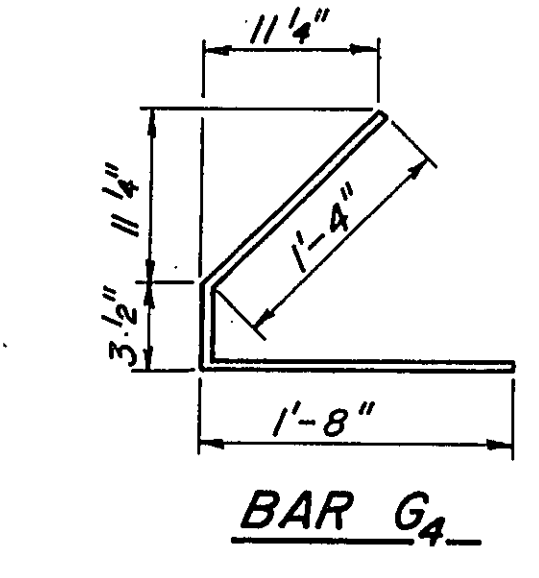
SECTION D-D



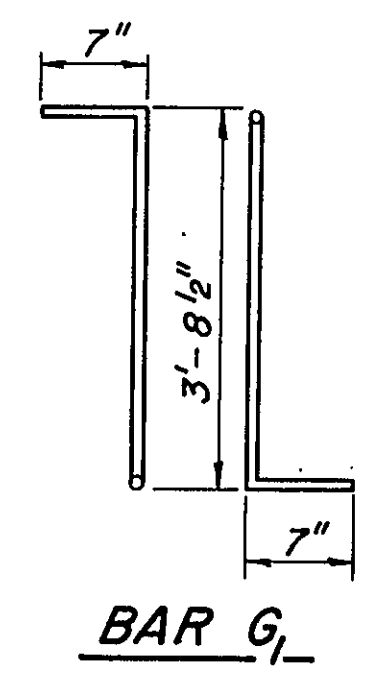
FRAMING PLAN



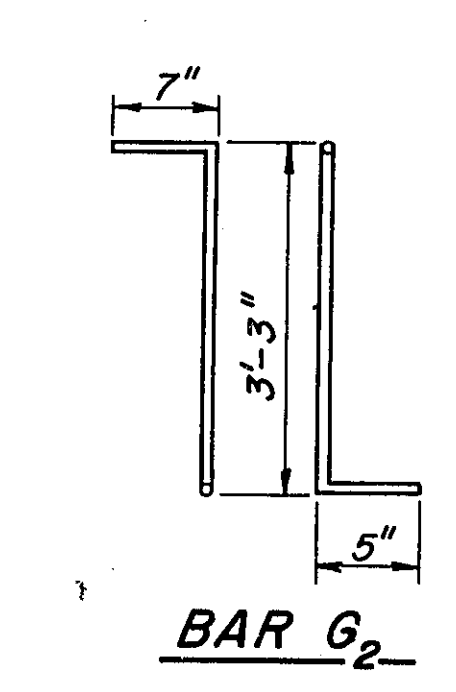
BAR G3



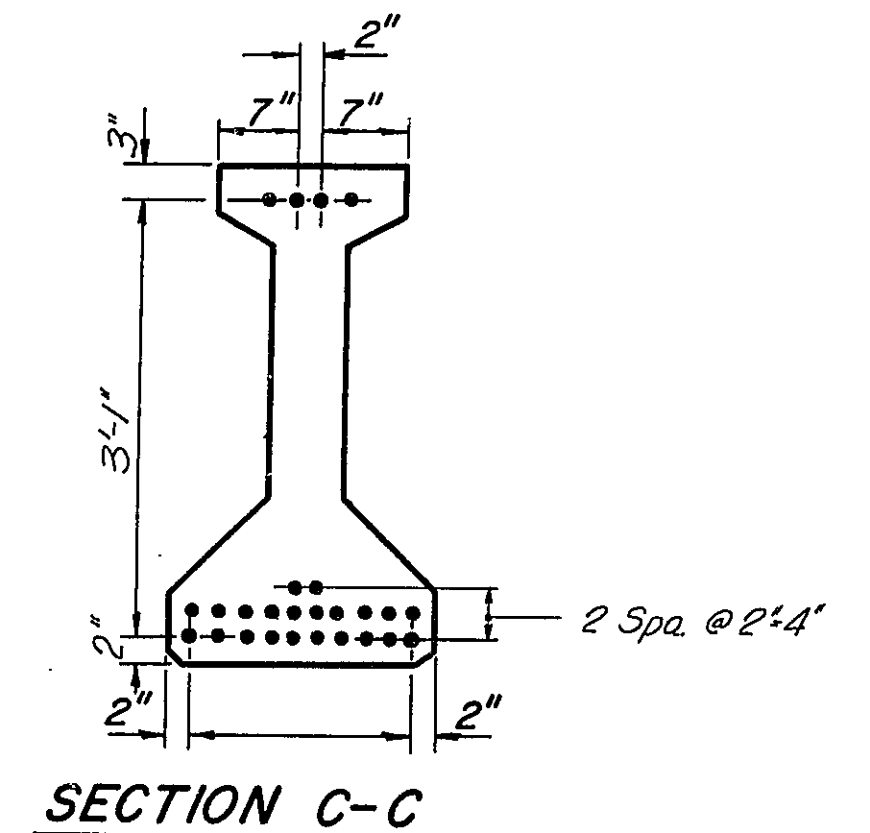
BAR G4



BAR G1



BAR G2



SECTION C-C

*** BAR LIST**

Bar	No.	Size	Length	Shape
G1	74	# 4	4'-10 1/2"	TL
G2	12	# 5	4'-3"	TL
G3	8	# 6	22'-6"	—
G4	18	# 3	3'-3 1/2"	∟
G5	13	# 3	2'-7"	□

* For one beam only.

BILL OF MATERIAL

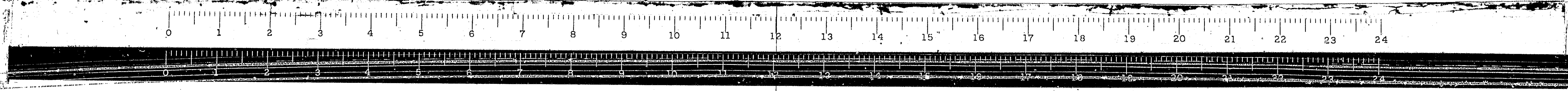
Item	Unit	Total
Furnishing & Erecting Precast Prestressed Concrete I-Beams, 42"	Lin. Ft.	304

**BEAM DETAILS E. APPROACH SPAN
CH-17 OVER FA.I. 55
STA. 1044+12.84**

F.A.I. ROUTE 55 PROJECT I-55-2()
SECTION 68-5HB MONTGOMERY COUNTY

Scale: None Date: _____
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

DESIGNED	N.O.S.
CHECKED	B.M. & R.D.W.
DRAWN	K.A.C.
CHECKED	B.M.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I-55	68-5HB	MONTGOMERY	206	72
FED. ROAD REG. NO. 4	ILLINOIS PROJECT	55-2()		

SHEET NO. 13

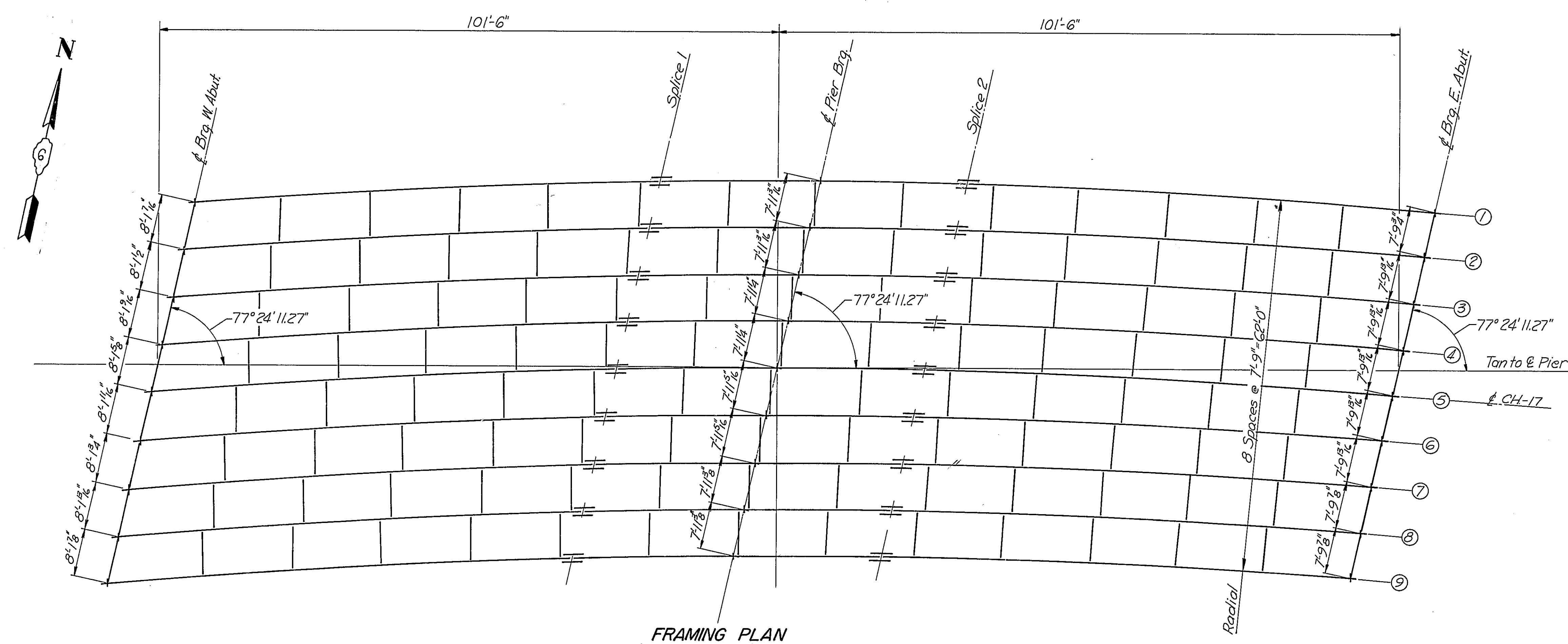


TABLE OF MOMENTS AND REACTIONS
INTERIOR BEAMS (Primary Loads)

	MOMENTS (kip x Ft.)		REACTIONS (kips)	
	0.4 Span	Pier	Abut	Pier
Dead Load	631.	1741.	36.4	145.8
Superimposed Dead Load	259.	510.	13.8	47.2
Live Load	906.	812.	49.9	75.4
Impact	199.	179.	11.0	11.5
TOTALS	1995.	3242.	111.1	279.9

PROPERTIES

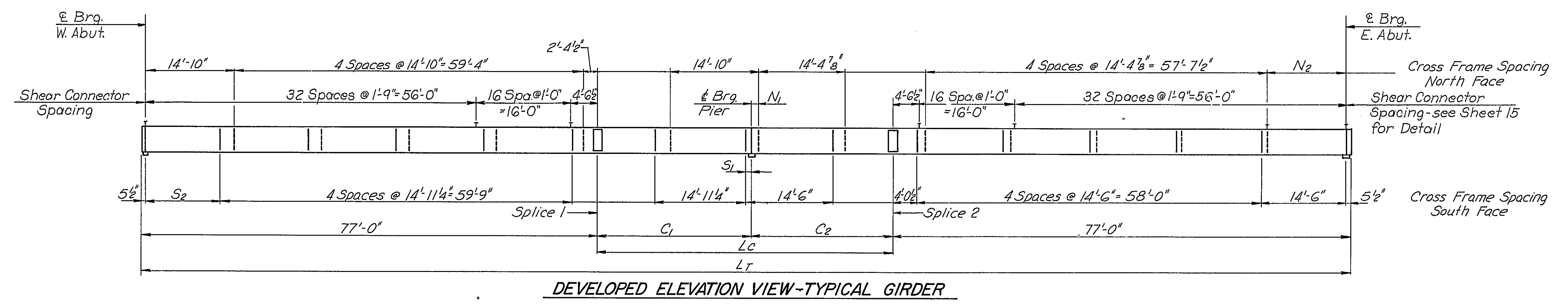
	STEEL SECTION		
	Section 0.4	Section 07	AT Pier
$I_s (IN^4)$	24,775.	34,438.	58,125.
$S_Ts (IN^3)$	770.	1,270.	2,114.
$S_Bs (IN^3)$	1,142.	1,270.	2,114.
COMPOSITE SECTION (n=10)			
$I_c (IN^4)$	64,942.	34,438.	58,125.
$S_Tc (IN^3)$	3,000.	1,270.	2,114.
$S_Bc (IN^3)$	1,580.	1,270.	2,114.

I_s = Moment of inertia, Steel Section
 S_Ts = Sec. Mod. top steel section
 S_Bs = Sec. Mod. bottom steel section
 I_c = Moment of inertia, Composite Section
 S_Tc = Sec. Mod. top comp. section
 S_Bc = Sec. Mod. bottom comp. section

STRUCTURAL STEEL
BILL OF MATERIALS

Girders	431,030 #
Cross Frames & Conn.	41,990
Splices	13,090
Stiffeners	5,630
Expansion Guards	2,720
Bearings	12,440
Total Structural Steel	506,900

Shear Connector = 3528 Each



DIMENSIONS - E GIRDER

G Girder No.	R_g Radius of Girder	L_T	L_C	C_1	C_2	N_1	S_1	N_2	S_2	W. Abut.	Splice 1	Pier	Splice 2	E. Abut.	G Girder No.
1	1176.92	203'-11 1/2"	49'-11 1/2"	25'-11 1/2"	24'-0 1/2"	—	10 1/2"	—	12'-6 1/4"	669.592	669.067	668.821	668.565	667.605	1
2	1169.17	204'-0 1/2"	50'-0 1/2"	25'-11 1/2"	24'-0 1/2"	10 1/2"	10 1/2"	13'-4 1/2"	12'-6"	669.127	668.609	668.364	668.110	667.148	2
3	1161.42	204'-0 3/4"	50'-0 3/4"	26'-0 1/4"	24'-0 1/2"	10 3/4"	10 3/4"	13'-4 1/4"	12'-5 1/4"	668.662	668.151	667.908	667.655	666.692	3
4	1153.67	204'-1 1/8"	50'-1 1/8"	26'-0 3/8"	24'-0 3/4"	10 1/2"	10 3/4"	13'-4 1/8"	12'-5 1/2"	668.197	667.699	667.452	667.201	666.236	4
5	1145.92	204'-2 1/4"	50'-2 1/4"	26'-1 1/2"	24'-1 1/2"	10 1/2"	10 1/2"	13'-4 1/4"	12'-5 1/4"	668.280	667.791	667.545	667.295	666.329	5
6	1138.17	204'-3 1/8"	50'-3 1/8"	26'-2 1/8"	24'-1 1/2"	10 3/8"	10 3/8"	13'-4 1/8"	12'-5 1/8"	668.363	667.883	667.638	667.389	666.422	6
7	1130.42	204'-4 1/2"	50'-4 1/2"	26'-2 1/2"	24'-1 1/2"	10 1/2"	10 1/2"	13'-4 1/4"	12'-4 3/4"	667.897	667.419	667.182	666.930	666.966	7
8	1122.67	204'-5 1/4"	50'-5 1/4"	26'-3 1/4"	24'-2 1/2"	10 1/4"	10 1/4"	13'-4 1/4"	12'-4 1/2"	667.431	666.962	666.726	666.474	665.511	8
9	1114.92	204'-6 3/8"	50'-6 3/8"	26'-4 1/4"	24'-2 3/4"	10 1/4"	—	13'-4 1/4"	—	666.964	666.503	666.270	666.018	666.055	9

FIELD ERECTION ELEVATIONS-TOP OF WEB

FRAMING PLAN
CH-17 OVER FA.I-55
STA. 1044+12.84

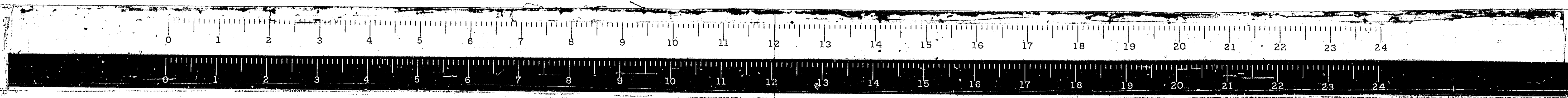
F.A.I. ROUTE-55 SECTION NO. 68-5HB PROJECT I-55-2() MONTGOMERY COUNTY

Scale: NONE

Date:

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

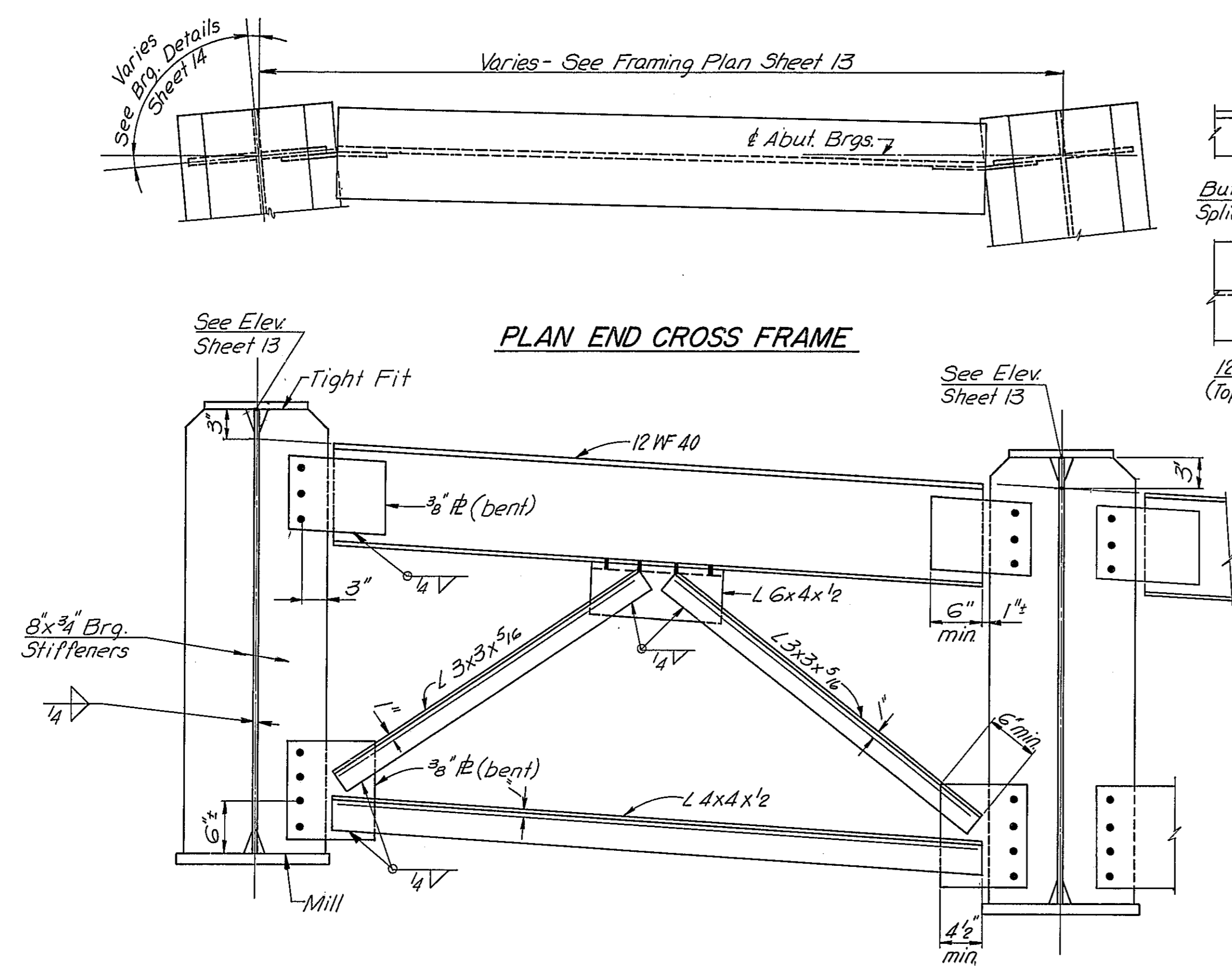
DESIGNED PAT
CHECKED NOS
DRAWN KAC
CHECKED PAT NOS



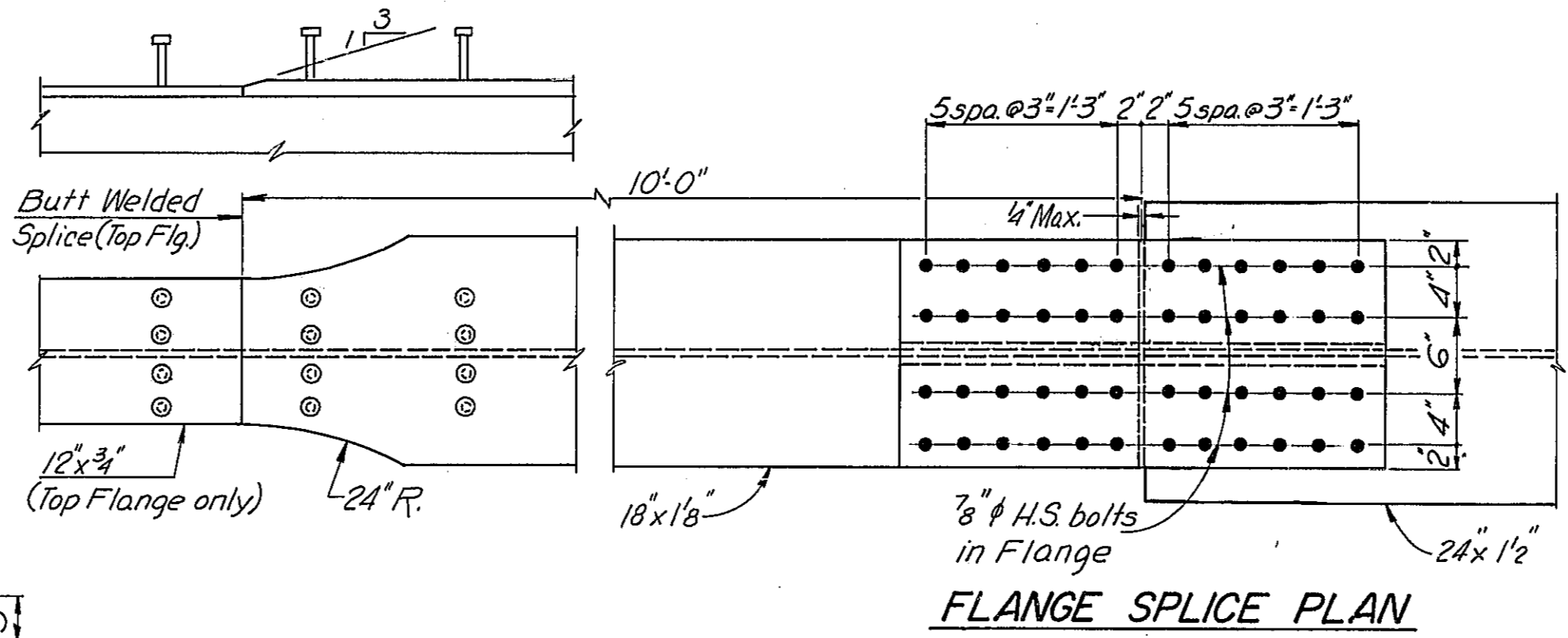
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	68-5HB	MONTGOMERY	206	74
FED. ROAD REG. NO. 4	ILLINOIS	PROJECT	I-55-2()	

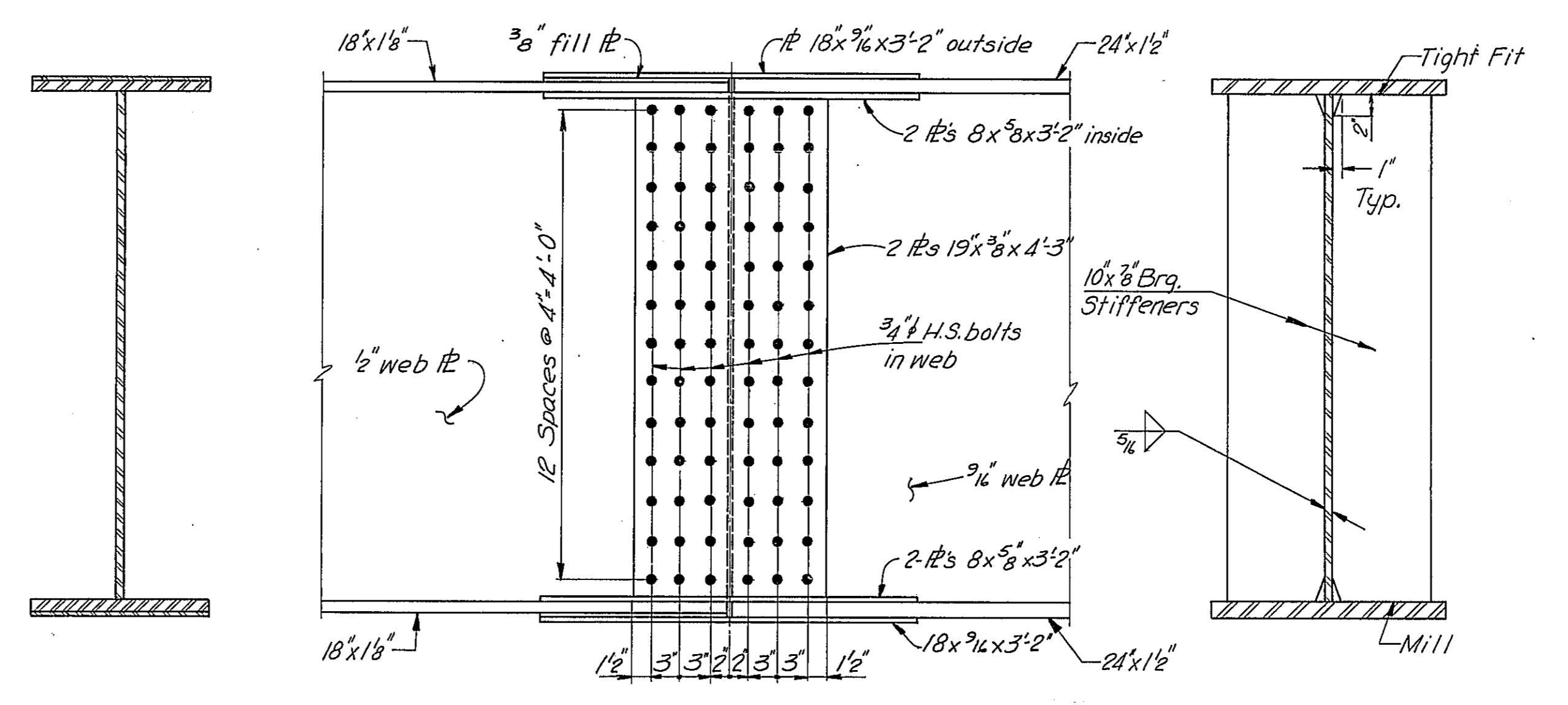
SHEET NO. 15



TYPICAL END CROSS FRAME
& BEARING STIFFENERS AT ABUTMENTS

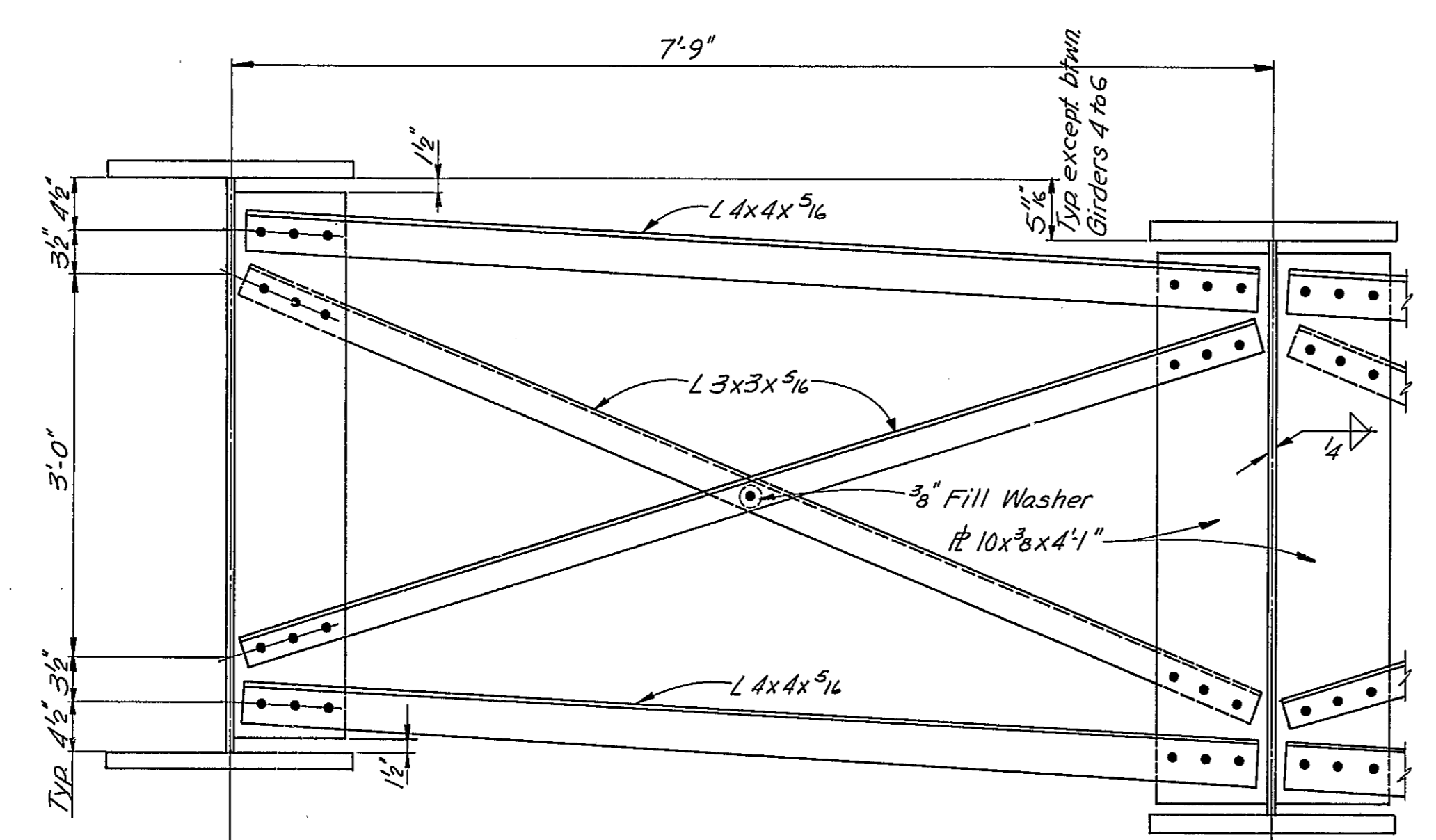


FLANGE SPLICE PLAN

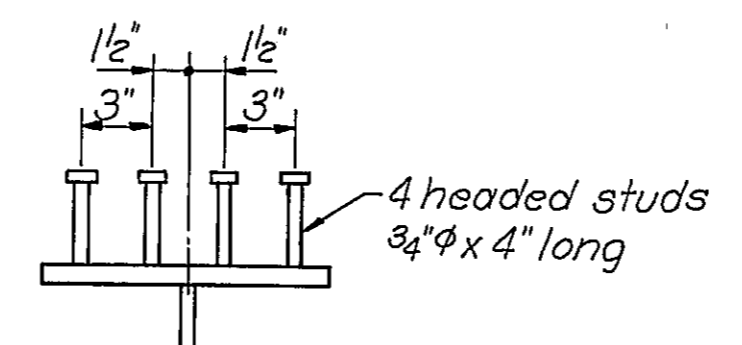


GIRDER SPLICE ELEVATION

BEARING STIFFENERS AT PIER



TYPICAL INTERIOR CROSS FRAME

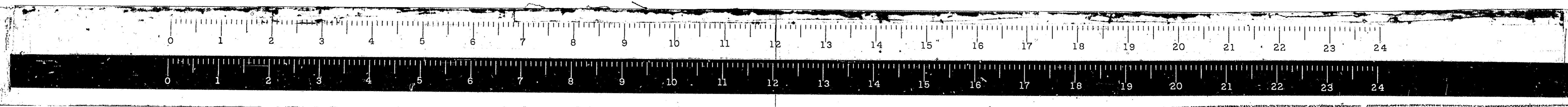


SHEAR CONNECTOR DETAIL
See Sheet 13 for locations

DESIGNED	PAT
CHECKED	NOS
DRAWN	KAC
CHECKED	PAT NOS

5/16 inch
4x5
and
5/16

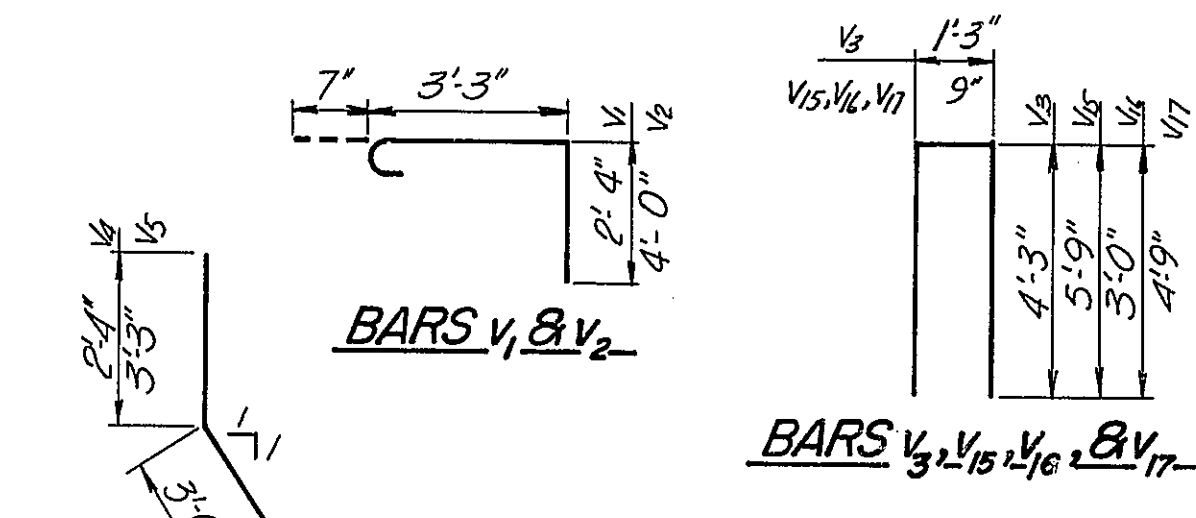
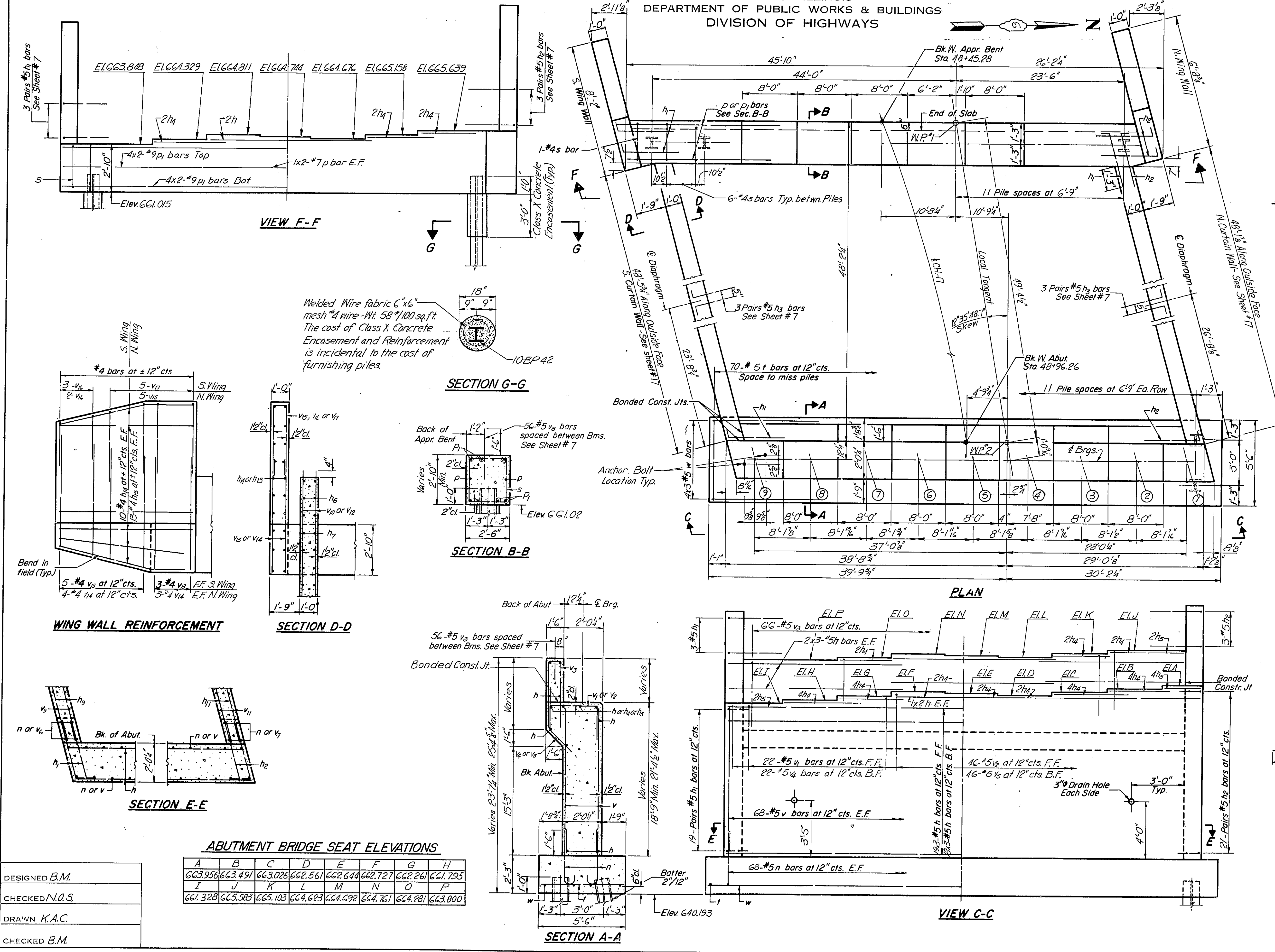
STEEL DETAILS
CH-17 OVER FA.I-55
STA. 1044+12.84
F.A.I. ROUTE-55 SECTION NO. 68-5HB PROJECT I-55-2() MONTGOMERY COUNTY
Scale: NONE Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I-55	68-5HB	MONTGOMERY	206	75
FED. ROAD REG. NO. 4	ILLINOIS PROJECT	I-55-2()		

SHEET NO. 16



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h	129	#5	23'-4"	
h1	53	#5	4'-1"	
h2	57	#5	4'-1"	
h3	12	#5	3'-0"	
h4	34	#5	9'-4"	
h5	8	#5	5'-0"	
h6	28	#5	25'-9"	
h7	28	#5	24'-6"	
h8	16	#4	20'-6"	
h9	16	#5	27'-6"	
h10	16	#5	18'-6"	
h11	16	#5	22'-6"	
h12	6	#8	25'-0"	
h13	6	#8	22'-0"	
h14	20	#4	7'-10"	
h15	26	#4	6'-4"	
h16	144	#5	3'-10"	
h17	136	#5	18'-6"	
h18	22	#5	6'-2"	
h19	46	#5	7'-10"	
h20	66	#5	9'-9"	
h21	22	#5	5'-10"	
h22	46	#5	6'-9"	
h23	4	#5	24'-7"	
h24	4	#5	27'-3"	
h25	112	#5	3'-0"	
h26	42	#5	30'-9"	
h27	16	#5	6'-0"	
h28	35	#5	34'-3"	
h29	28	#5	9'-0"	
h30	16	#4	8'-0"	
h31	14	#4	6'-6"	
h32	5	#4	12'-3"	
h33	5	#4	6'-9"	
h34	5	#4	10'-3"	
h35	62	#4	10'-1"	
h36	4	#7	37'-3"	
h37	16	#9	37'-6"	
h38	70	#5	5'-2"	
h39	12	#5	24'-3"	
Reinforcement Bars				Lbs. 19,890
Class X Concrete				Cu. Yds. 239.0
Steel Piles (10BP42)				Lin. Ft. 881
Test Piles				Each 1

W. ABUTMENT
CH-17 OVER FA.I-55
STA. 1044+12.84

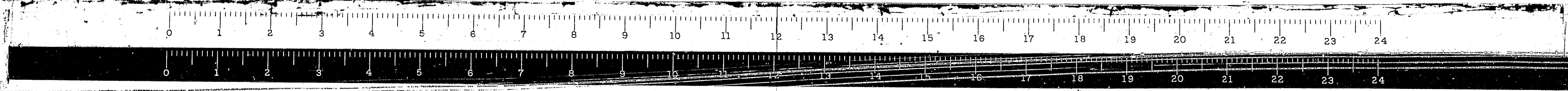
F.A.I. ROUTE 55
SECTION 68-5HB
Scale: None

PROJECT I-55-2()
MONTGOMERY COUNTY

Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

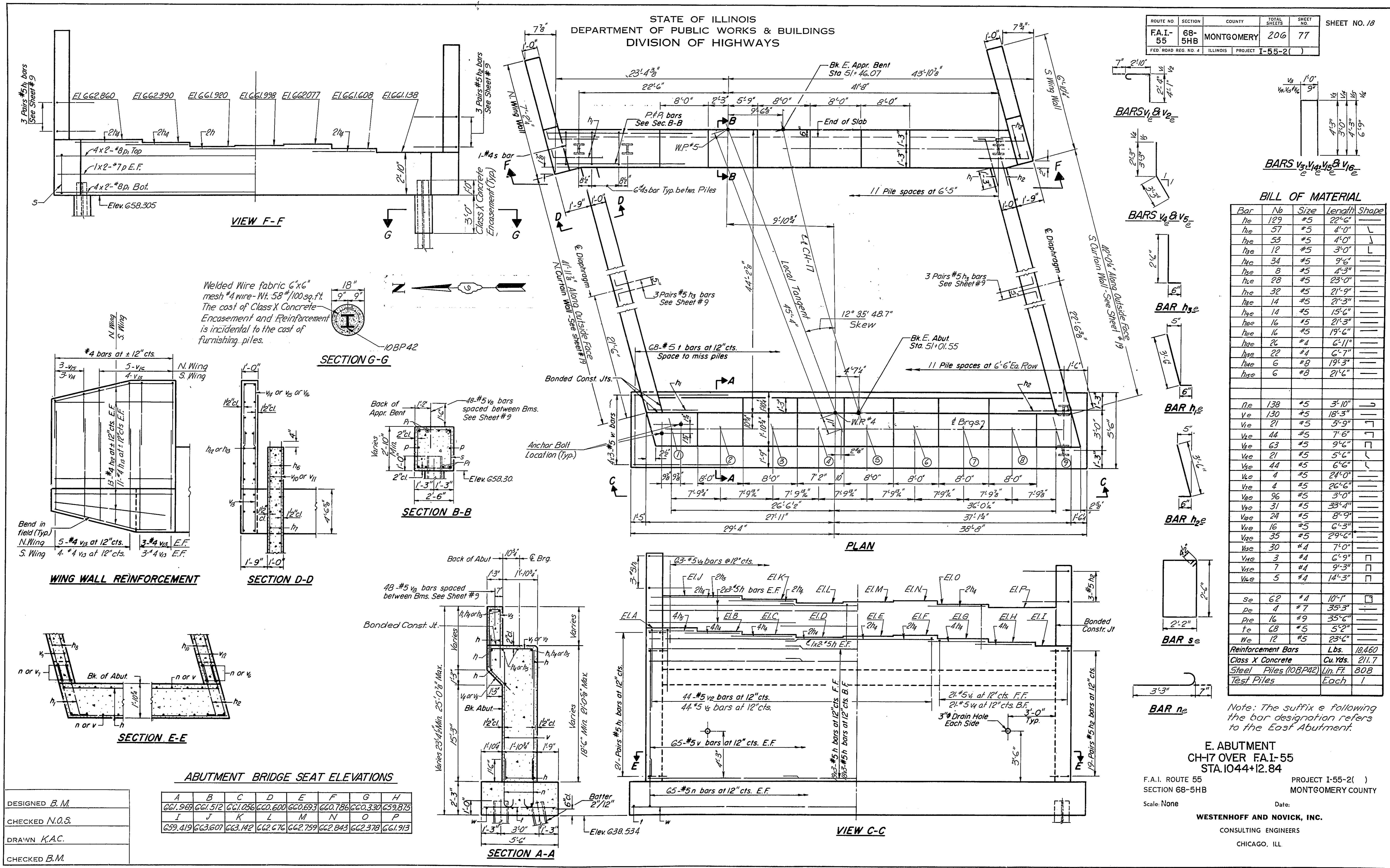
DESIGNED B.M.
CHECKED N.O.S.
DRAWN K.A.C.
CHECKED B.M.

A	B	C	D	E	F	G	H
663.956	663.491	663.026	662.561	662.096	661.631	661.166	660.701
661.328	665.383	665.103	664.623	664.692	664.761	664.830	663.800



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.-55	68-5HB	MONTGOMERY	206	77
FED. ROAD REG. NO. 4	ILLINOIS PROJECT	I-55-2()		



BILL OF MATERIAL

Bar	Nb	Size	Length	Shape
he	129	#5	22'-6"	
he	57	#5	4'-0"	
he	53	#5	4'-0"	
he	12	#5	3'-0"	
he	34	#5	9'-6"	
he	8	#5	4'-3"	
he	28	#5	23'-0"	
he	32	#5	21'-9"	
he	14	#5	21'-3"	
he	14	#5	15'-6"	
he	16	#5	21'-3"	
he	16	#5	19'-6"	
he	26	#4	6'-11"	
he	22	#4	6'-7"	
he	6	#8	19'-3"	
he	6	#8	21'-6"	
he	138	#5	3'-10"	
ve	130	#5	18'-3"	
ve	21	#5	5'-9"	
ve	44	#5	7'-8"	
ve	63	#5	9'-6"	
ve	21	#5	5'-6"	
ve	44	#5	6'-6"	
ve	4	#5	24'-0"	
ve	4	#5	26'-6"	
ve	96	#5	3'-0"	
ve	31	#5	33'-4"	
ve	24	#5	8'-9"	
ve	16	#5	6'-3"	
ve	35	#5	29'-6"	
ve	30	#4	7'-0"	
ve	3	#4	6'-9"	
ve	7	#4	9'-3"	
ve	5	#4	14'-3"	
se	62	#4	10'-1"	
pe	4	#7	35'-3"	
pe	16	#9	35'-6"	
te	68	#5	5'-2"	
we	12	#5	23'-6"	

Reinforcement Bars Lbs. 18,460
Class X Concrete Cu Yds. 211.7
Steel Piles (108P42) Lin. Ft. 808
Test Piles Each 1

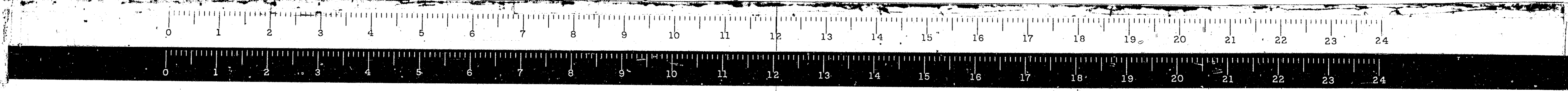
Note: The suffix e following the bar designation refers to the East Abutment.

E. ABUTMENT
CH-17 OVER F.A.I.-55
STA. 1044+12.84
F.A.I. ROUTE 55
SECTION 68-5HB
Scale: None
PROJECT I-55-2()
MONTGOMERY COUNTY
Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

DESIGNED	B.M.
CHECKED	N.O.S.
DRAWN	K.A.C.
CHECKED	B.M.

ABUTMENT BRIDGE SEAT ELEVATIONS

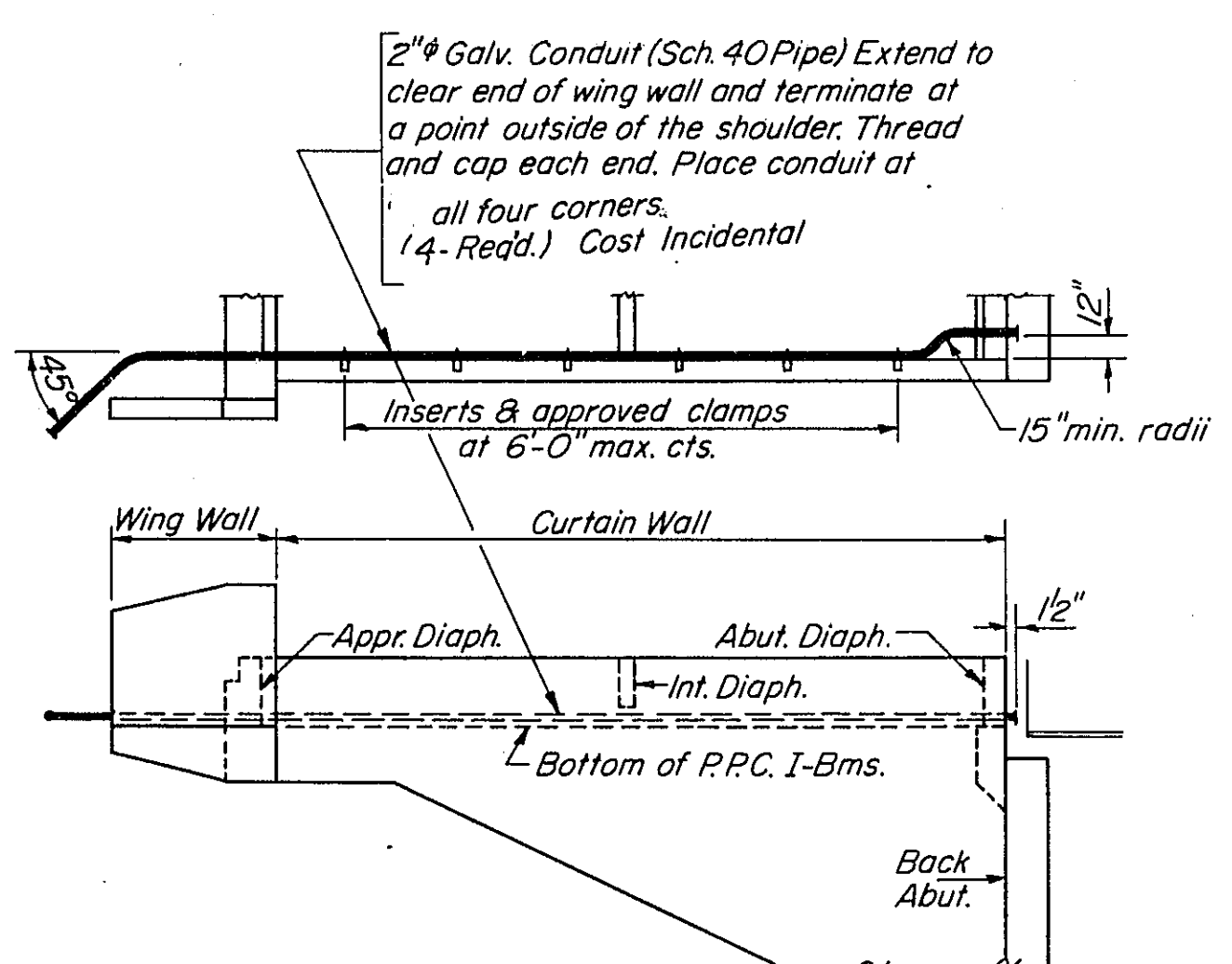
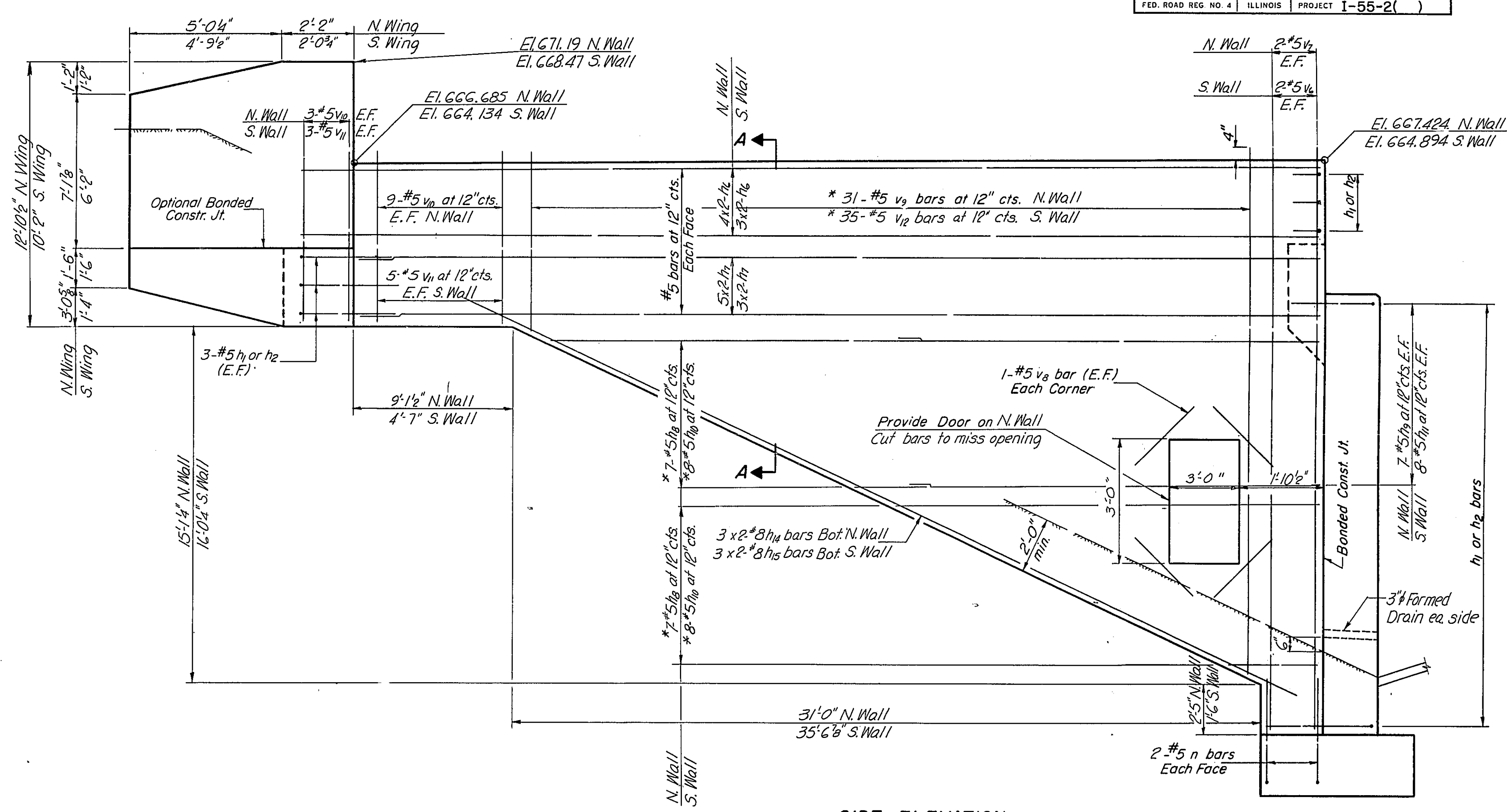
	A	B	C	D	E	F	G	H
661.969	661.512	661.056	660.600	660.144	660.688	660.786	660.330	659.875
659.419	663.607	663.142	662.676	662.220	662.759	662.843	662.378	661.913



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I.-55	68-5HB	MONTGOMERY	206	78
FED. ROAD REG. NO. 2		ILLINOIS PROJECT	I-55-2	

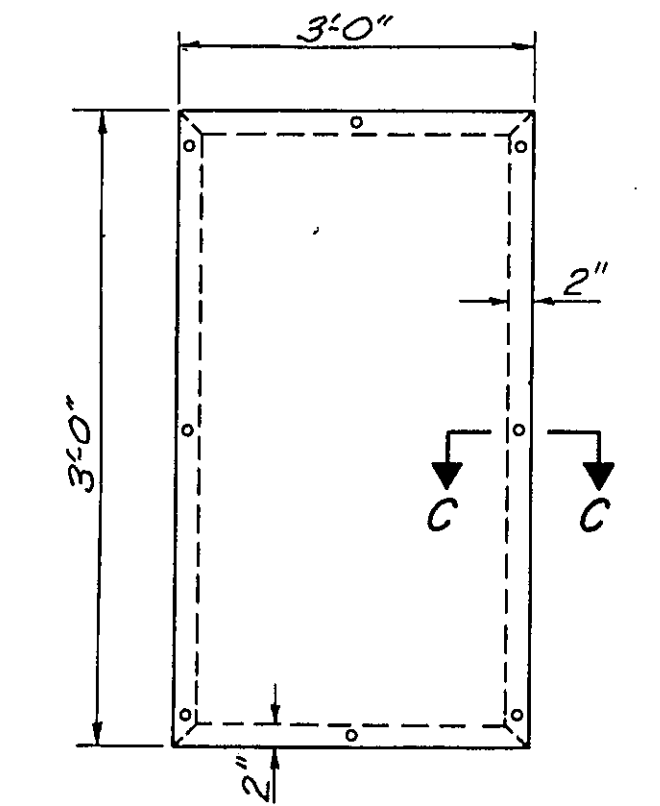
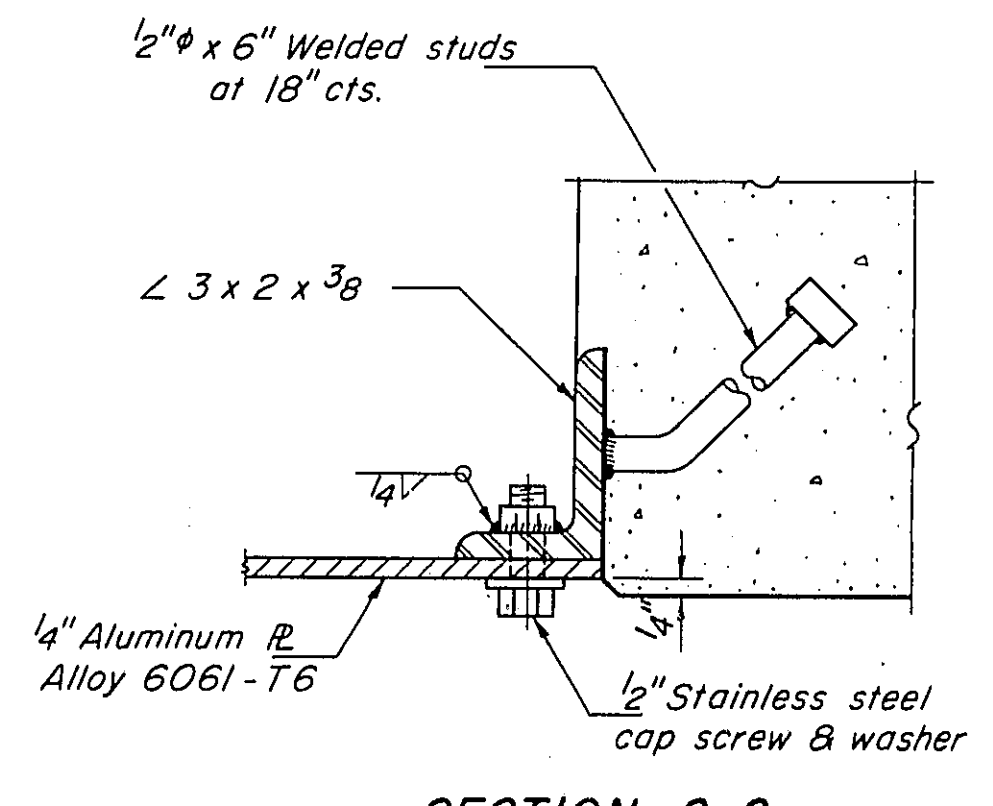
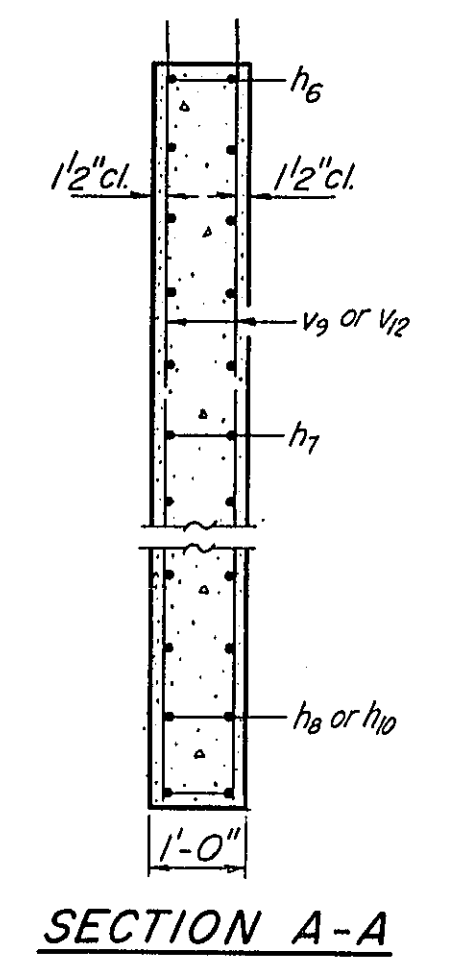
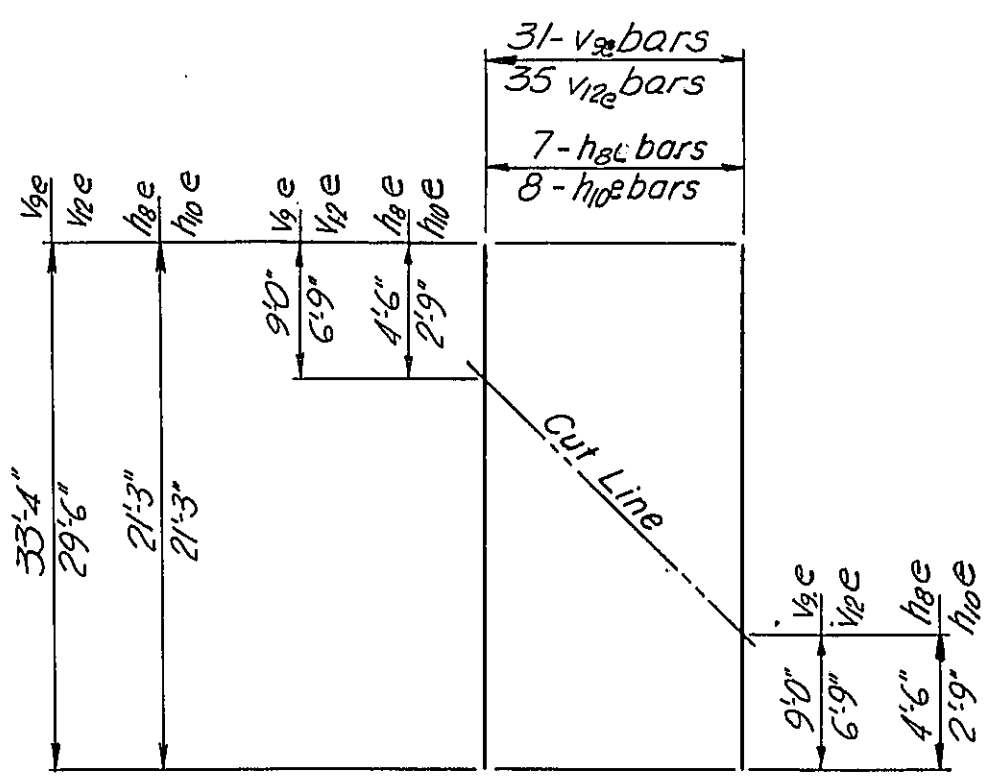
SHEET NO. /9



Note: For Rustication Details See Sheet No. 17.

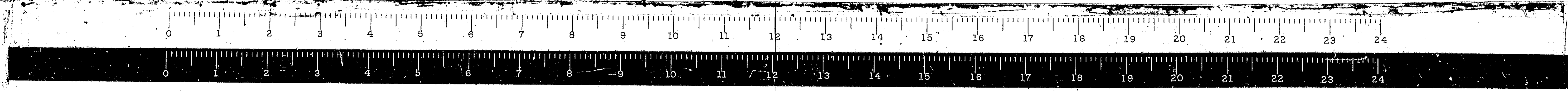
APPR. BENT-PILE DATA
 Type 10BP42 H Piles
 Capacity Refusal
 Est. Length 39'
 No. Req'd. *11
 * Include 1 Test Pile

ABUT-PILE DATA
 Type 10BP42 H Piles
 Capacity Refusal
 Est. Length 19'
 No. Req'd. 22



E. ABUTMENT CURTAIN WALLS
 CH-17 OVER FA.I-55
 STA. 1044+12.84
 F.A.I. ROUTE 55
 SECTION 68-5HB
 Scale: NONE
 PROJECT I-55-2()
 MONTGOMERY COUNTY
 Date:
WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS
 CHICAGO, ILL.

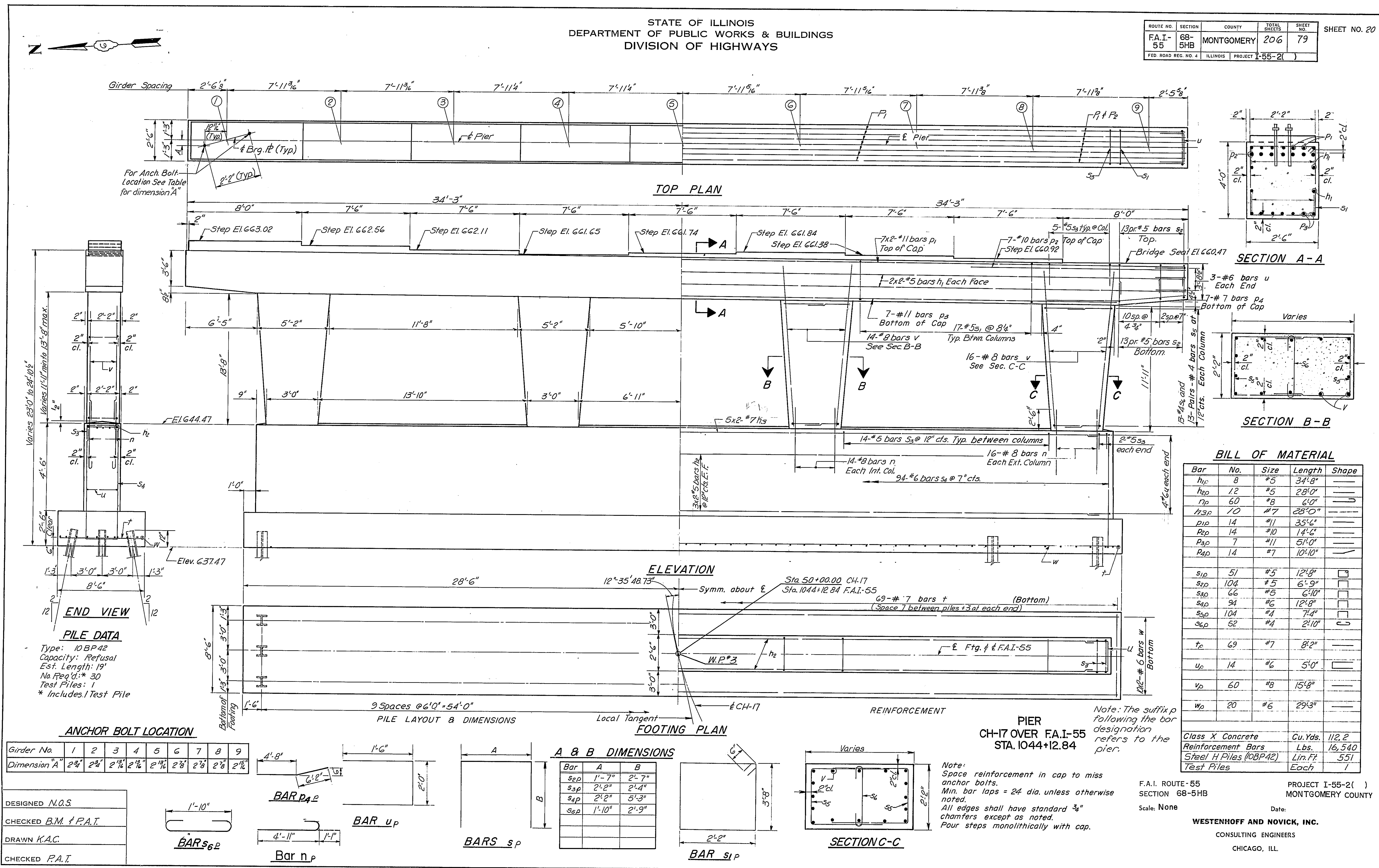
DESIGNED B.M.
CHECKED N.Q.S.
DRAWN K.A.C.
CHECKED B.M.



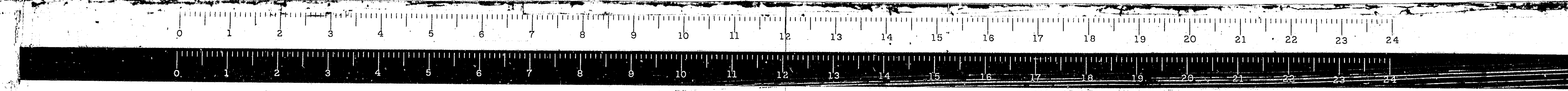
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO. F.A.I.-55	SECTION 68-5HB	COUNTY MONTGOMERY	TOTAL SHEETS 206	SHEET NO. 79
FED. ROAD REG. NO. 4		ILLINOIS PROJECT I-55-2()		

SHEET NO. 20



P-6 7-2-62



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 21
FA.I-55	68-5HB	MONTGOMERY	206	80	
FED. ROAD REG. NO. 4		ILLINOIS PROJECT	I-55-2()		

Boring No. 1
Station 49+98
Offset 30' LT

Elevation	N	Qu	w (%)	Surface Water EL	Groundwater EL at Completion	After Hours
643.7	0					
641.6				Brown Silty (Sandy) Clay Loam		
				Very Dense		
					616.1	
	4	0.5	D	Fine Grain Silty Sand (Shale)		
	7	0.8	B	Very Dense		
	7	0.7	B	20% Strain		
631.1				Brown Silty (Sand) Clay		
	39	1.5	S		606.6	
	29	1.3	S			
	19	1.0	S			
	15	1.4	S			
621.1						

Boring No. 3
Station 51+37
Offset 22' LT

Elevation	N	Qu	w (%)	Surface Water EL	Groundwater EL at Completion	After Hours
639.03	0					
637.5				Black Silty Clay Loam		
				20% Strain		
	9	0.8	B	Gray Shale (Micaceous) Soft	614.5	
	10	0.7	B	20% Strain		
632.0				Mottled Brown Clay Stiff		
	13	1.1	B	20% Strain		
629.5				Brown Clay Loam Very Stiff		
	15	2.0	S			
627.0				Brown Silty Loam (Sandy) Stiff		
	30	1.0	S		602.5	
	26	1.1	S			
622.0				Brown Silty Loam (Sandy) Very Dense		
	16					
	68					

Boring No. 5
Station 48+90
Offset 38' LT

Elevation	N	Qu	w (%)	Surface Water EL	Groundwater EL at Completion	After Hours
645.03	0					
				Brown And Gray Silty Clay Medium		
				Very Dense		
					620.0	
				Gray Shale (Micaceous) Dry		
	6	1.0	B	20% Strain		
	10	1.5	B	20% Strain		
	17	1.6	S	10% Strain		
	13	1.2	S	15% Strain		
	44			Light Tan Fine Sand Medium Moist	614.0	
	27					
	14					

Boring No. 2
Station 51+01
Offset 38' LT

Elevation	N	Qu	w (%)	Surface Water EL	Groundwater EL at Completion	After Hours
639.93	0					
638.9				Black Silty Clay Loam		
				Very Dense		
					618.9	
	10	0.9	B	Gray Shale (Micaceous) Medium Moist		
	16	1.2	B	Soft		
	15	1.8	B	20% Strain		
627.9				Brown Silty Loam Very Stiff		
	34	2.5	S		603.4	
	32	2.3	S			
	32	2.1	S			
620.4				Gray Silty (Sandy) Very Dense		
	71					
617.9						

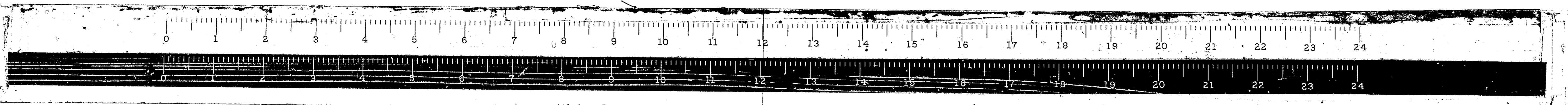
Boring No. 4
Station 48+66
Offset 15' LT

Elevation	N	Qu	w (%)	Surface Water EL	Groundwater EL at Completion	After Hours
644.23	0					
				Gray And Yellow Brown Silty Clay (Fill) Medium Moist		
				Hard	620.2	
	8	1.0	B	20% Strain		
	7	1.2	B	20% Strain		
	12	1.2	B	10% Strain		
	18	1.7	S	15% Strain		
	19	1.4	S	15% Strain		
	31	1.5	B	Moist	613.2	
	48			Tan Sand (Fine Micaceous) Dense		
	28			Moist		

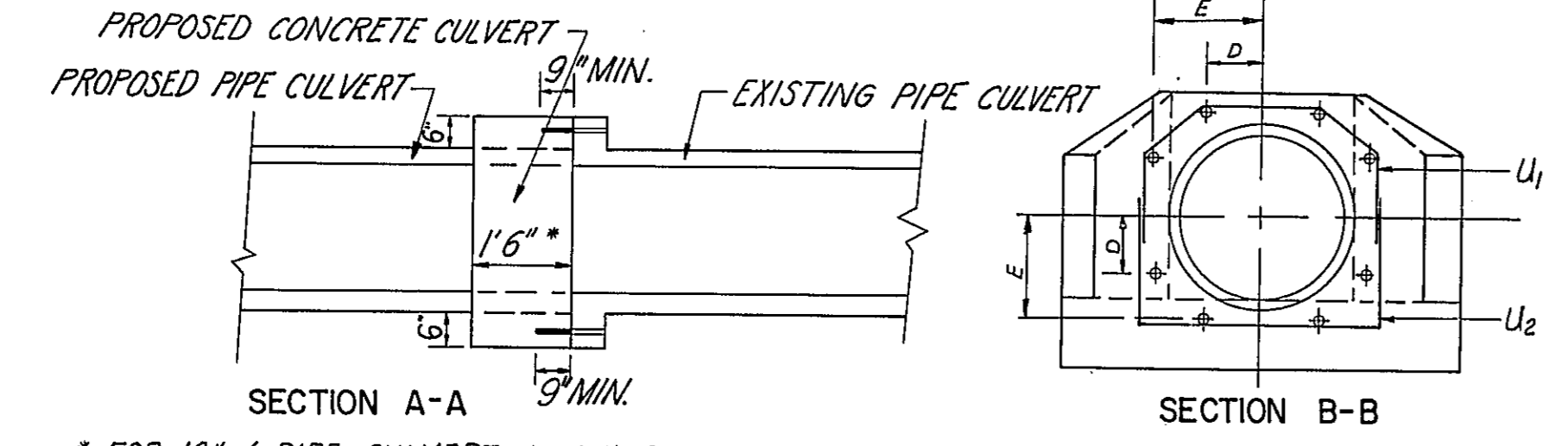
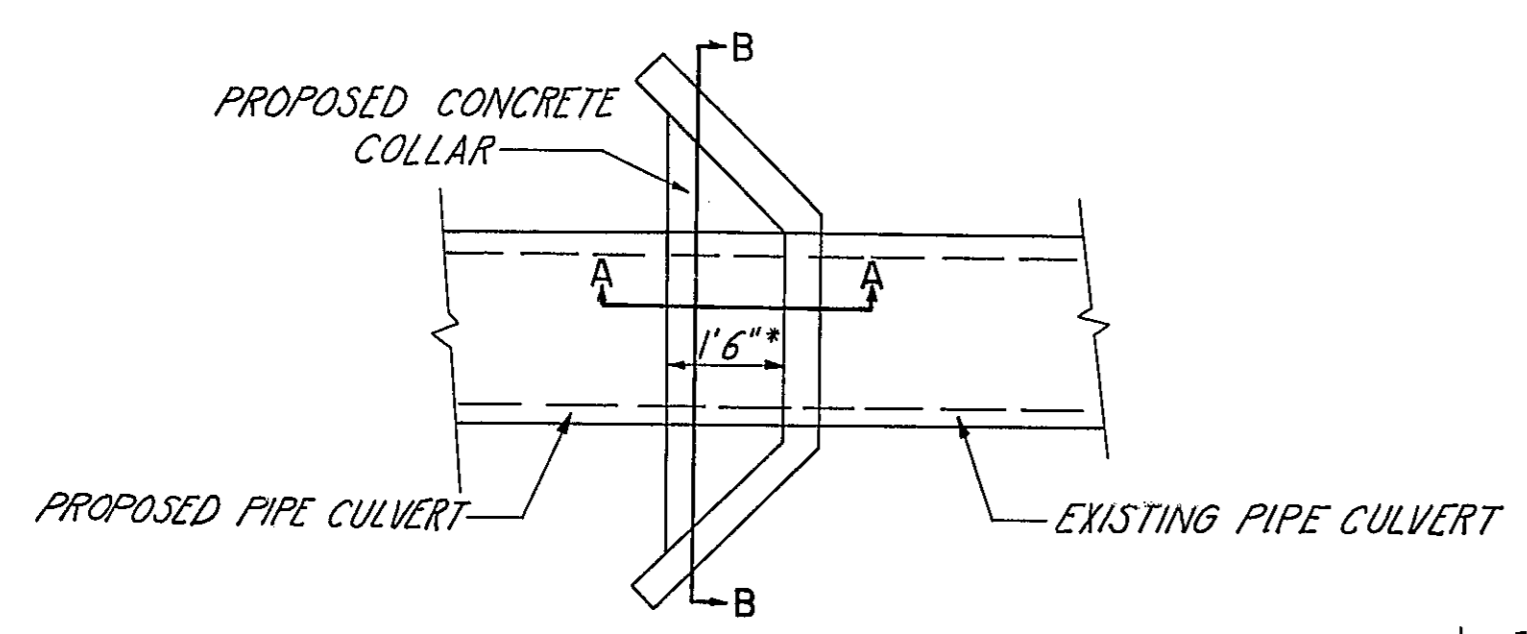
DESIGNED
CHECKED
DRAWN K.A.C.
CHECKED B.M.

N - Standard Penetration Test - Blows per foot to drive 2" O.D. Split Spoon Sampler 12" with 140# hammer falling 30"
Qu - Unconfined Compressive Strength - 1/4"
w - Water Content - percentage of oven dry weight - %
Type failure:
B - Bulge Failure
S - Shear Failure
E - Estimated Value
P - Penetrometer

BORING DATA
CH-17 OVER FA.I-55
STA. 1044+12.84
F.A.I. ROUTE 55 SECTION 68-5HB
Scale: NONE
PROJECT I-55-2()
MONTGOMERY COUNTY
Date:
WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS
CHICAGO, ILL.

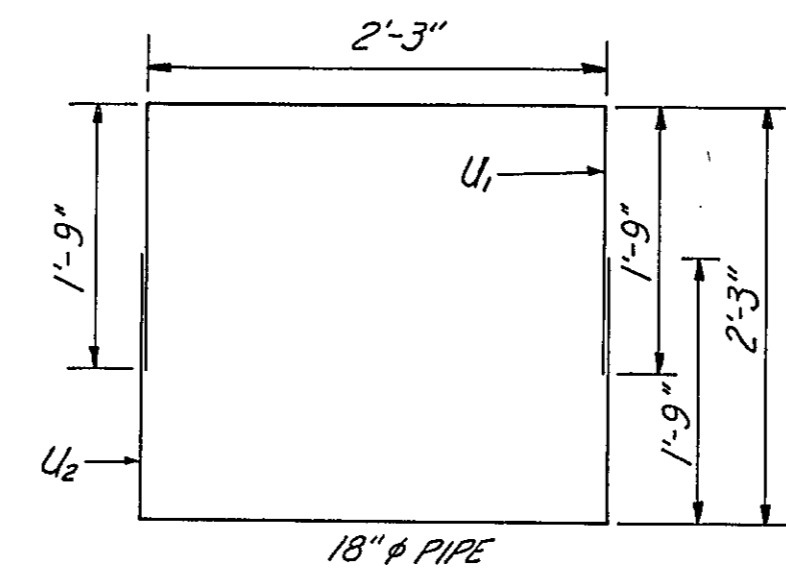
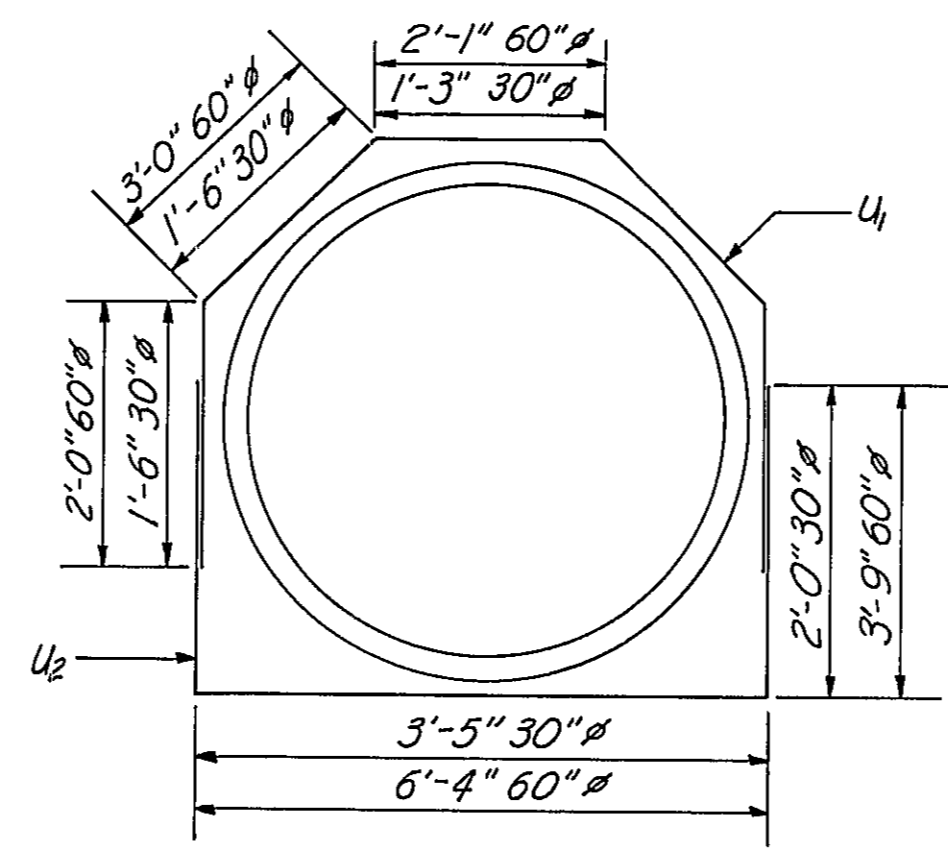


ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 33	*	MONT.	206	81
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

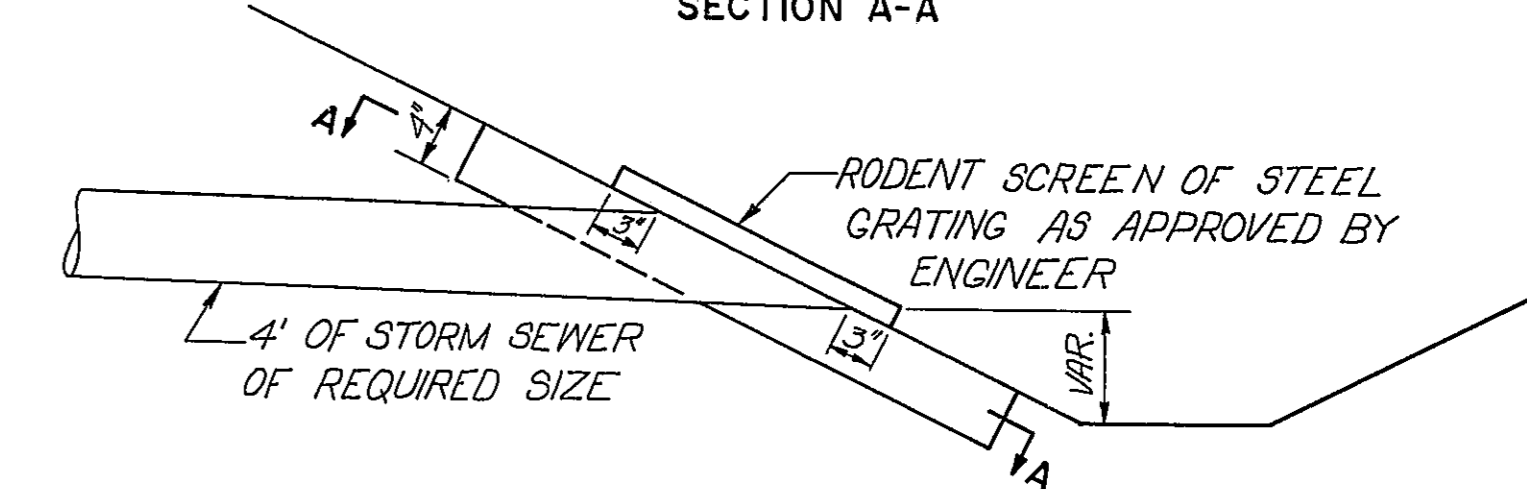
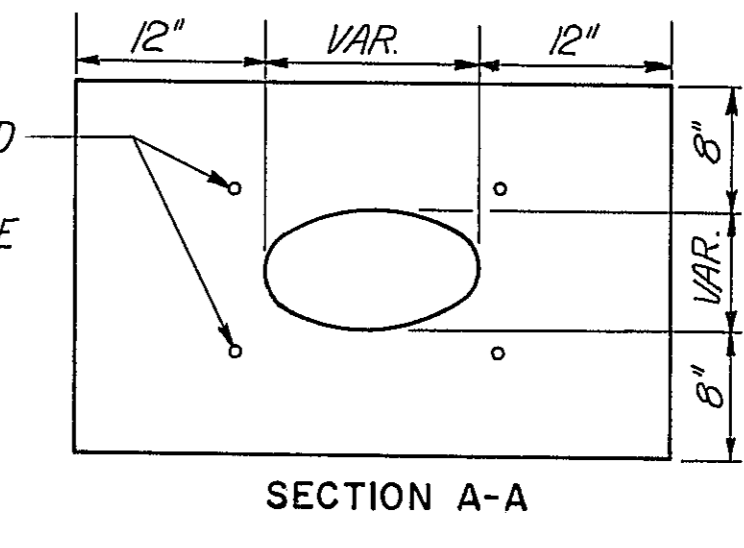


* FOR 18" # PIPE CULVERT USE 1'-0" AND FOR 60" # PIPE CULVERT USE 2'-5" SPECIAL PIPE COLLAR FOR
 2 @ STATION 845 + 10 - RT.
 STATION 845 + 10 - MED.
 STATION 853 + 25 - RT.
 STATION 870 + 20.7 - RT.
 STATION 880 + 21.80 - RT.
 STATION 917 + 42 - MED.
 STATION 981 + 28.6 - MED.
 STATION 981 + 28.6 - RT.
 STATION 991 + 86.7 - MED
 SCALE: NONE

NOTES: 1. CLASS X CONCRETE SHALL BE USED THROUGHOUT.
 2. EXPANSION BOLTS SHALL CONSIST OF SELF-DRILLING EXPANSION SHIELDS AND 3/4" HOOKED BOLTS. HOOKED BOLTS SHALL EXTEND A MINIMUM OF 9" INTO NEW CONCRETE.
 3. EXPANSION BOLT SPACING IS SYMMETRICAL ABOUT ϕ OF OLD CULVERT.



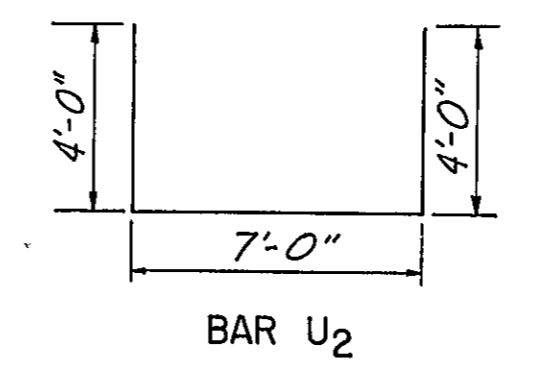
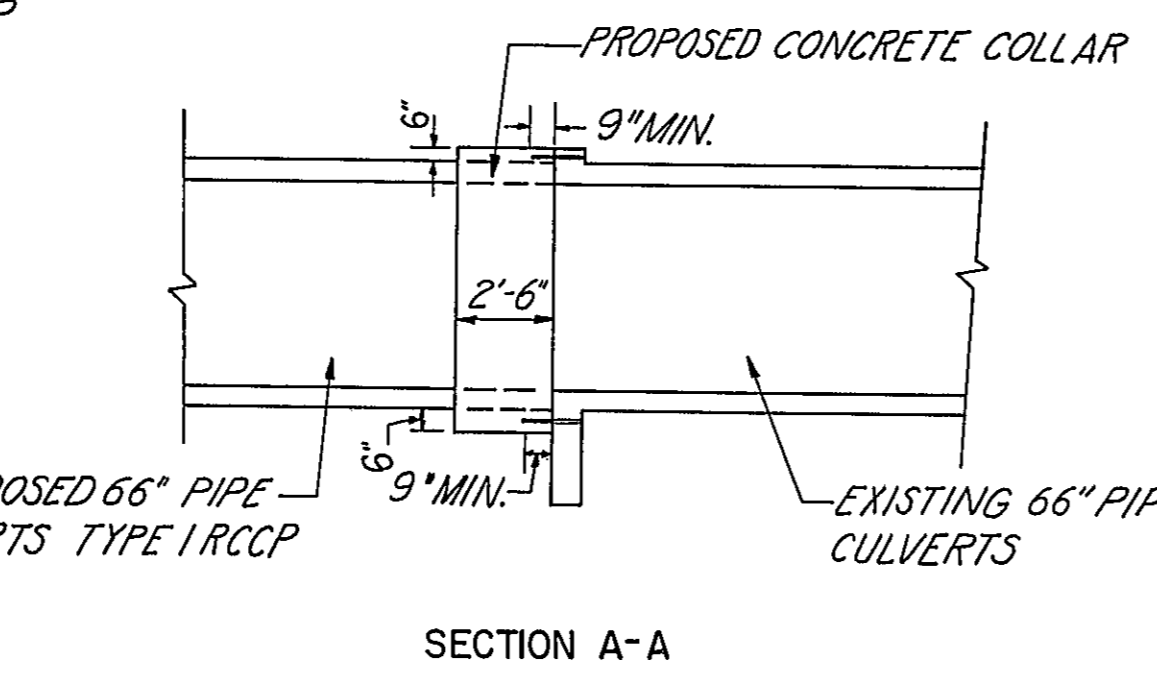
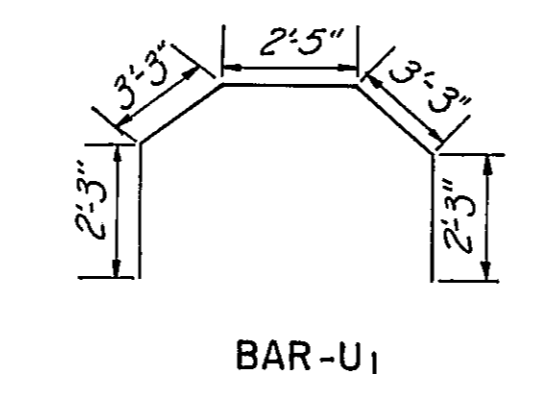
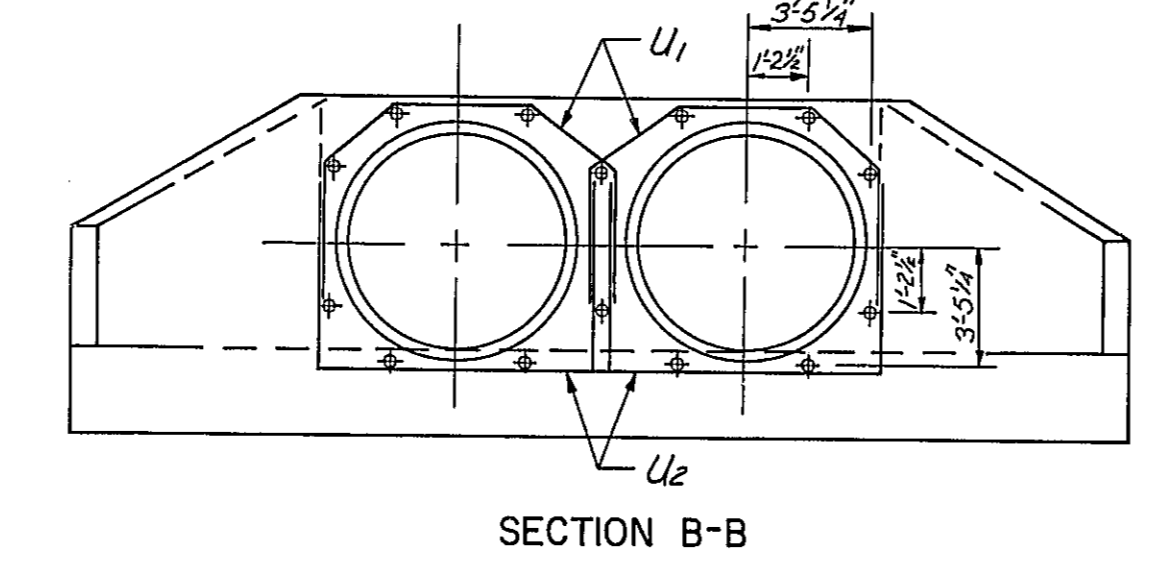
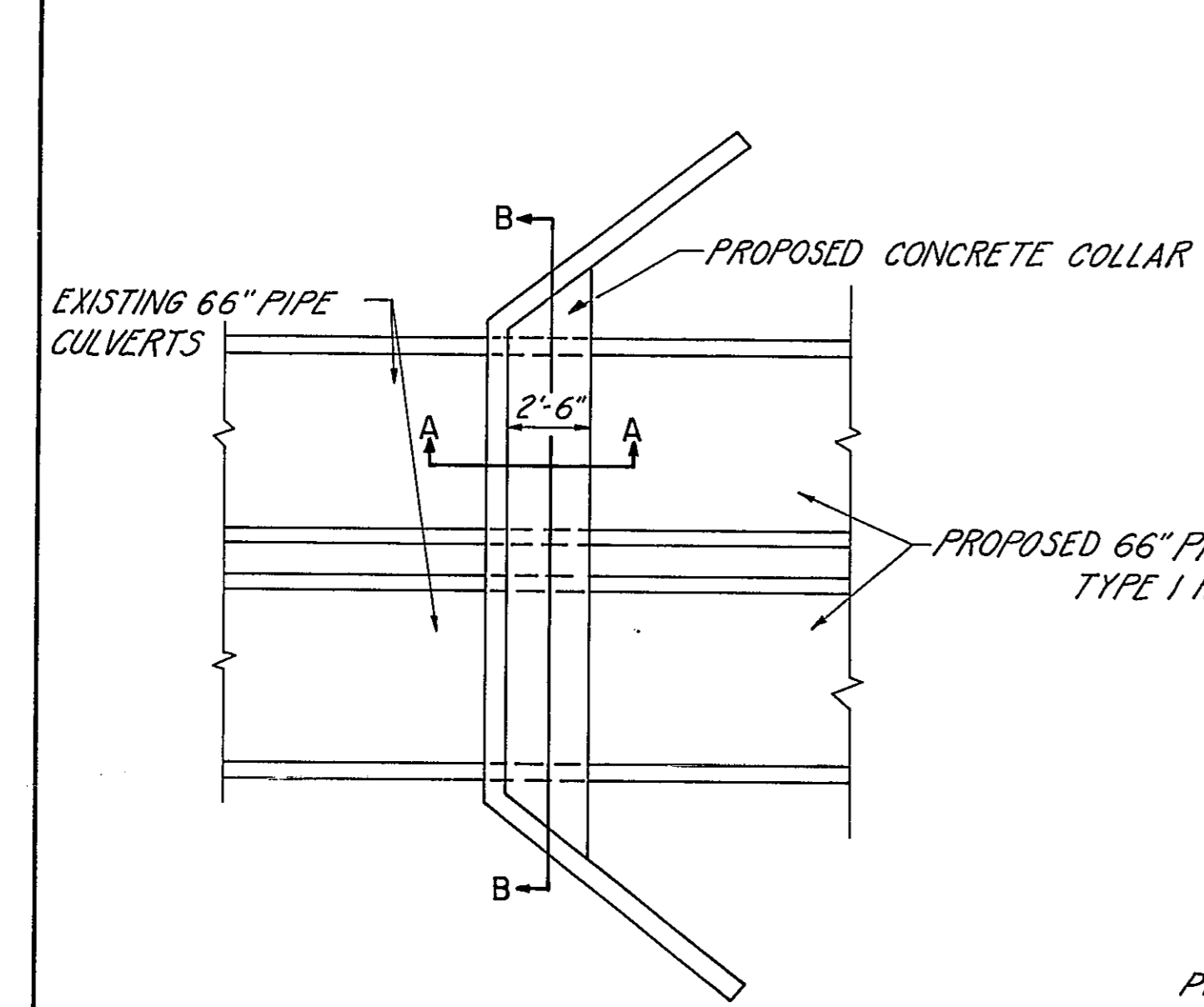
ANCHOR BOLTS PLACED TO ENABLE GRATING TO BE BOLTED IN PLACE



PAVED OUTLET AND RODENT SCREEN FOR FIELD TILE
 SCALE 1" = 1'-0"

NOTE: THE COST OF THE PAVED OUTLET AND RODENT SCREEN SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNIT PRICE PER LINEAL FOOT FOR STORM SEWERS OF THE TYPE SPECIFIED. (SEE SPECIAL PROVISIONS)

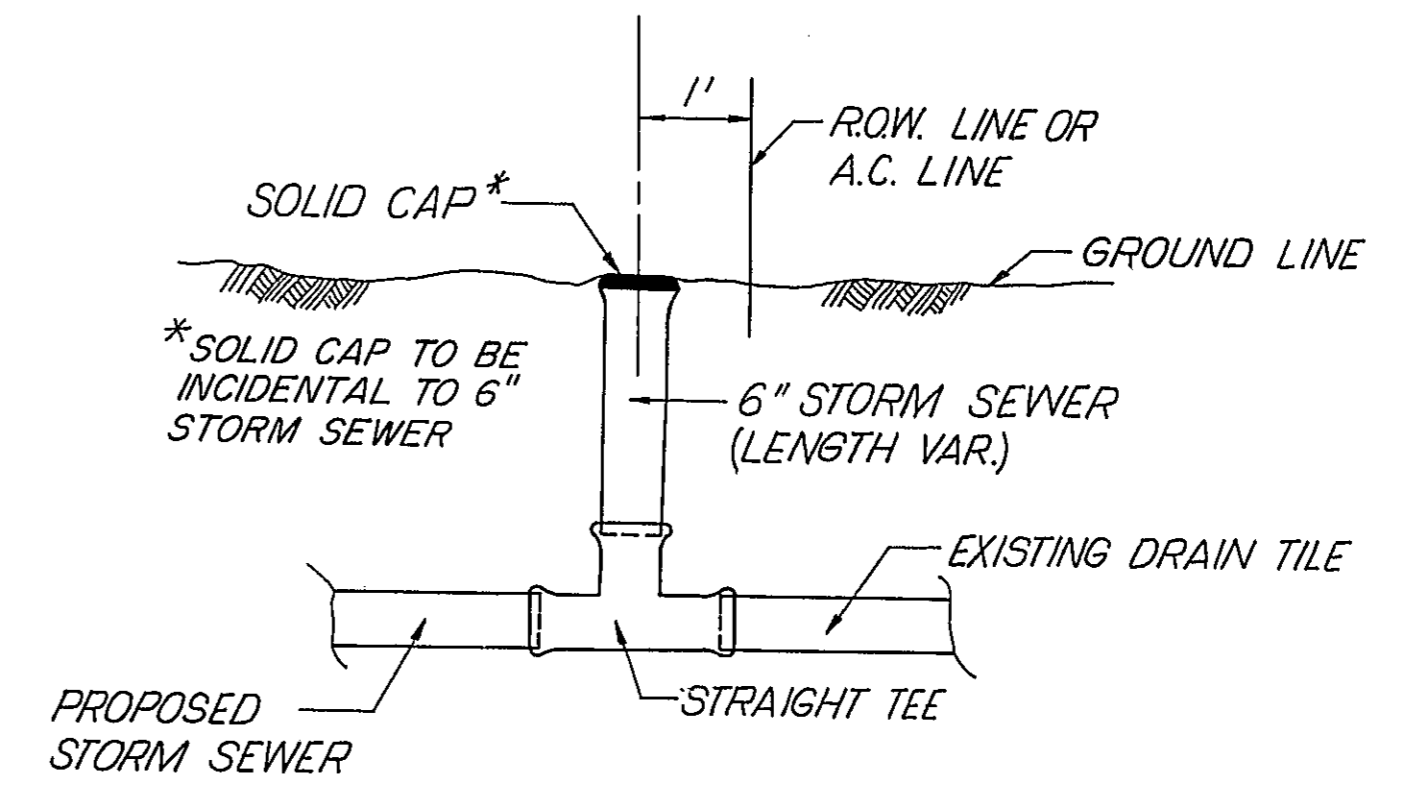
STATION & P. CULV.	LOCATION	SIZE	NO.	SIZE	LENGTH	NO.	SIZE	LENGTH	CLASS X CONCRETE CU. YDS.	REINF. BARS LBS.	EXP. BOLTS 3/4" EACH	D	E
28845 + 10 RT.	30"	4	#6	7'-3"	4	#6	7'-5"	1.2	88	16	7 1/2"	1'-7 3/4"	
845 + 10 MED.	30"	2	#6	7'-3"	2	#6	7'-5"	0.6	44	8	7 1/2"	1'-7 3/4"	
853 + 25 RT.	30"	2	#6	7'-3"	2	#6	7'-5"	0.6	44	8	7 1/2"	1'-7 3/4"	
.870 + 20.7 RT.	30"	2	#6	7'-3"	2	#6	7'-5"	0.6	44	8	7 1/2"	1'-7 3/4"	
880 + 21.80 RT.	30"	2	#6	7'-3"	2	#6	7'-5"	0.6	44	8	7 1/2"	1'-7 3/4"	
917 + 42 MED.	18"	2	#6	5'-9"	2	#6	5'-9"	0.2	35	8	3"	1'-0 3/4"	
981 + 28.6 MED.	30"	2	#6	7'-3"	2	#6	7'-5"	0.6	44	8	7 1/2"	1'-7 3/4"	
981 + 28.6 RT.	30"	2	#6	7'-3"	2	#6	7'-5"	0.6	44	8	7 1/2"	1'-7 3/4"	
991 + 86.7 MED.	60"	2	#6	12'-1"	2	#6	13'-10"	2.9	78	8	12 1/2"	3'-1 3/4"	



SPECIAL PIPE COLLAR FOR STATION 947 + 62.5 - MED.
 SCALE: NONE

NOTE: 1. CLASS X CONCRETE SHALL BE USED THROUGHOUT
 2. EXPANSION BOLTS SHALL CONSIST OF SELF-DRILLING EXPANSION SHIELDS AND 3/4" HOOKED BOLTS. HOOKED BOLTS SHALL EXTEND A MINIMUM OF 9" INTO NEW CONCRETE.
 3. EXPANSION BOLT SPACING IS SYMMETRICAL ABOUT ϕ OF OLD CULVERT.

BAR	NO.	SIZE	LENGTH
U1	4	#6	13'-5"
U2	4	#6	15'-0"
CLASS X CONC.	C.Y.	4.2	
REINF. BARS	LBS	170	
EXP BOLTS 3/4"	EACH	14	



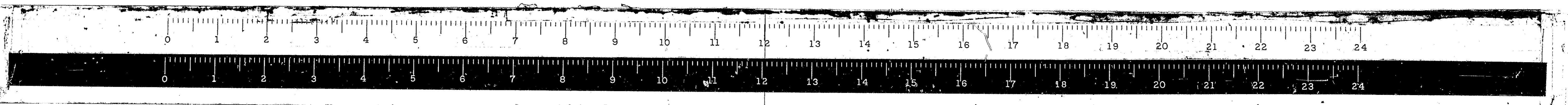
INSPECTION WELL
 SCALE: NONE

DESIGNED BY: FTP
 DRAWN BY: MD
 CHECKED BY: FTP
 APPROVED BY: _____

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

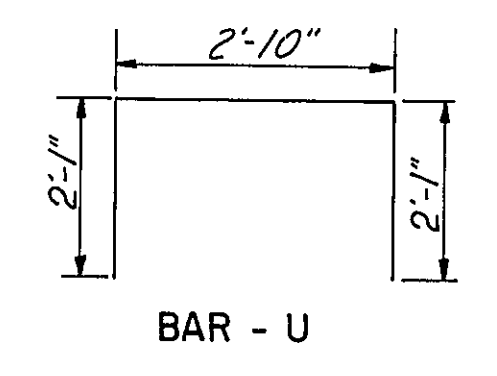
**SPECIAL CULVERT
 DETAILS**

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



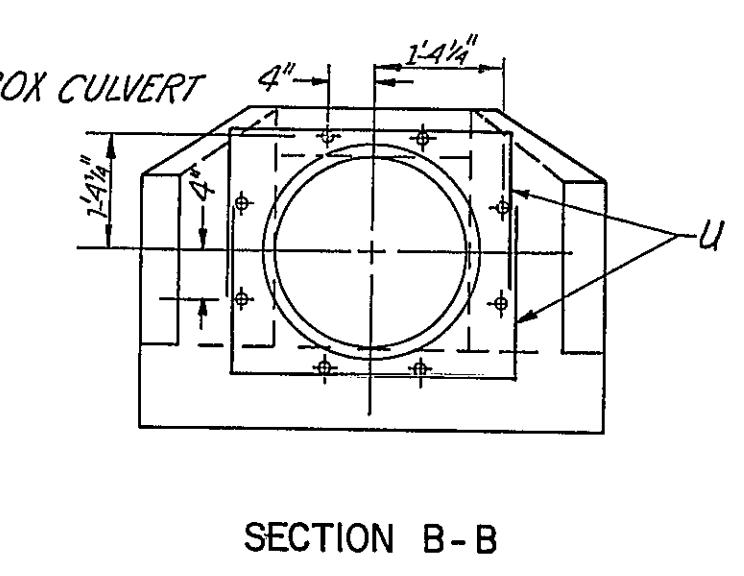
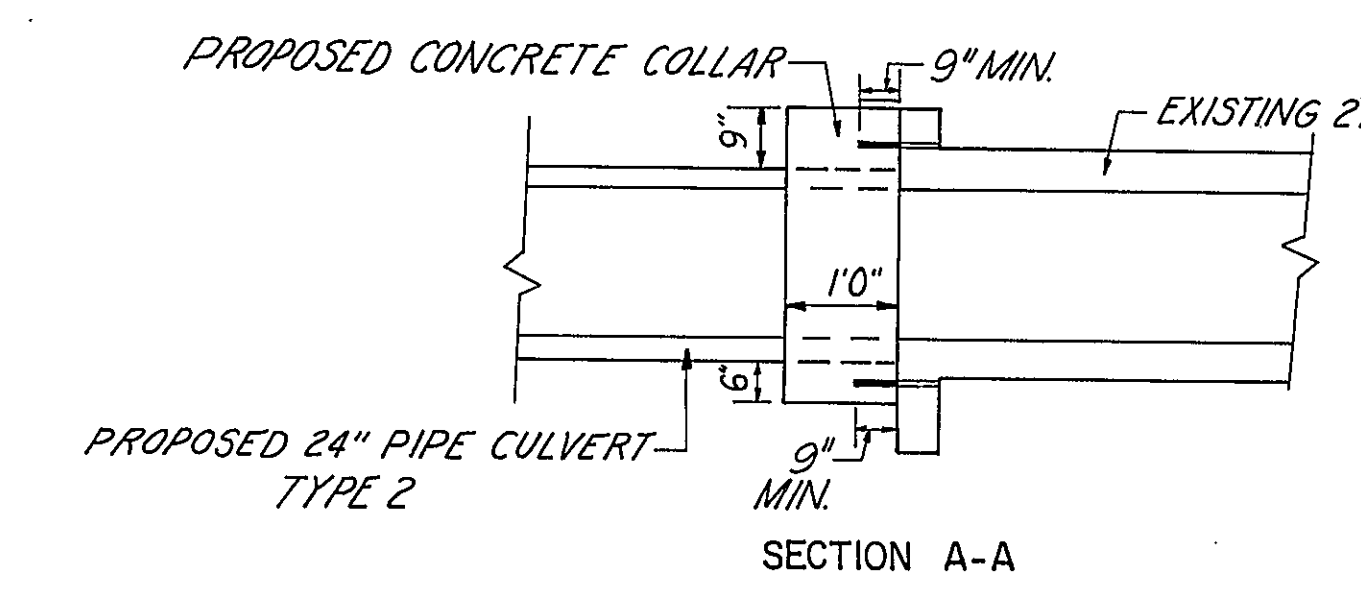
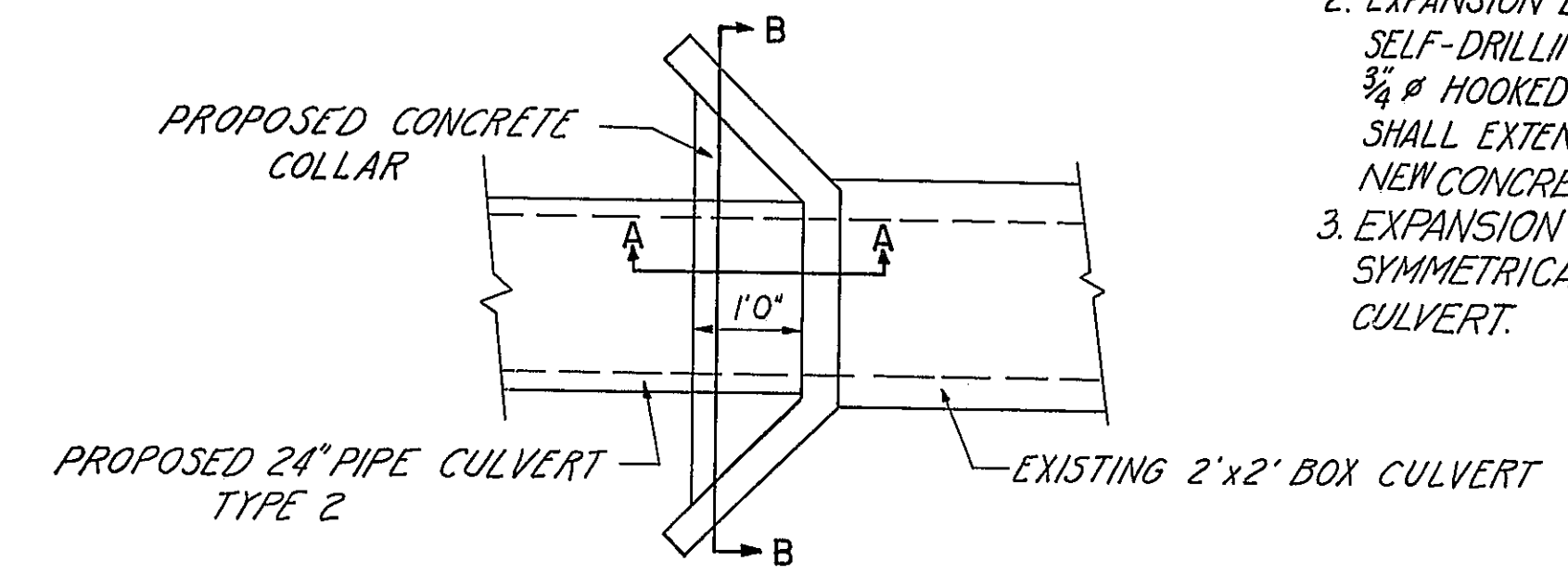
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	82
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

- NOTES: 1. CLASS X CONCRETE SHALL BE USED THROUGHOUT.
 2. EXPANSION BOLTS SHALL CONSIST OF SELF-DRILLING EXPANSION SHIELDS AND 3/8" HOOKED BOLTS. HOOKED BOLTS SHALL EXTEND A MINIMUM OF 9" INTO NEW CONCRETE.
 3. EXPANSION BOLT SPACING IS SYMMETRICAL ABOUT $\frac{1}{2}$ OF OLD CULVERT.

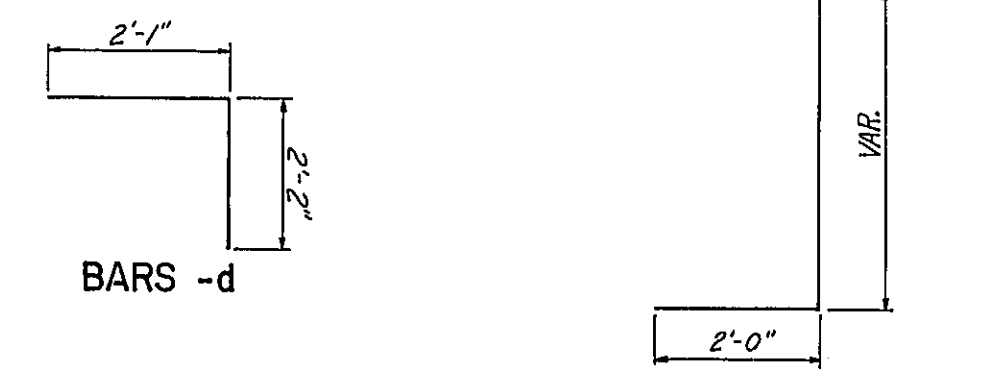
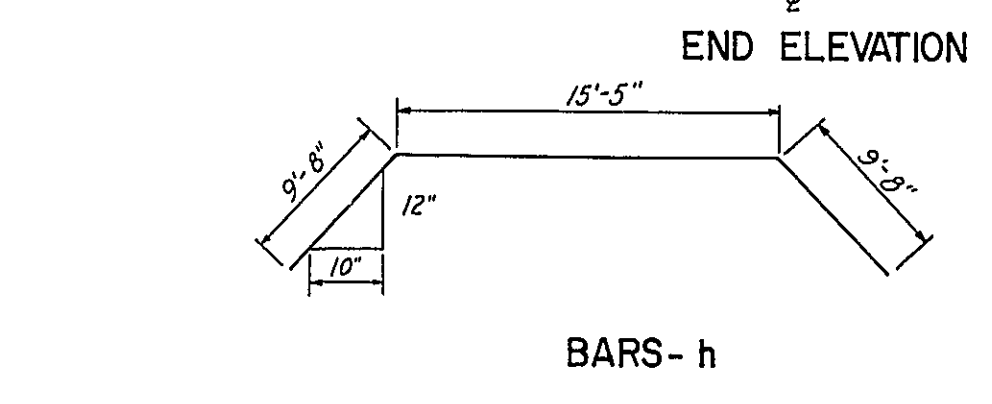
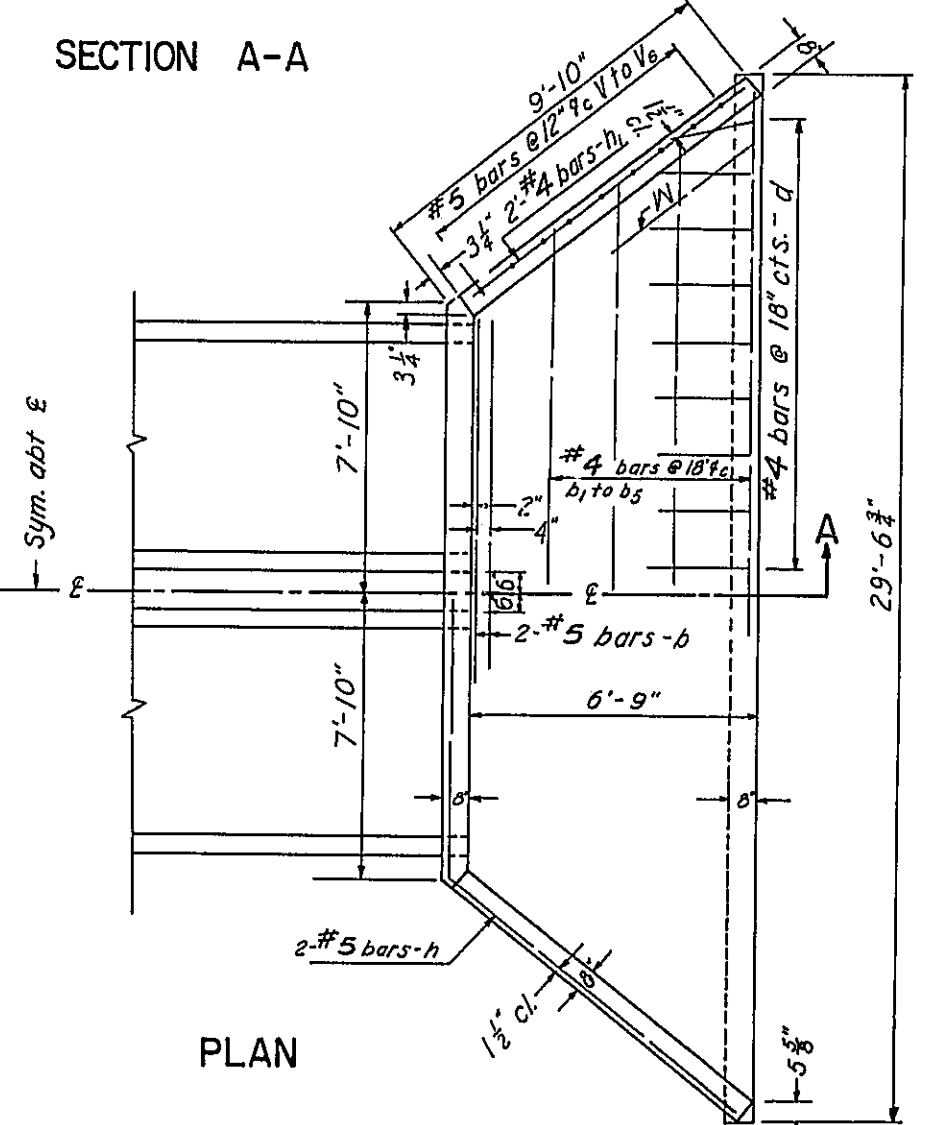
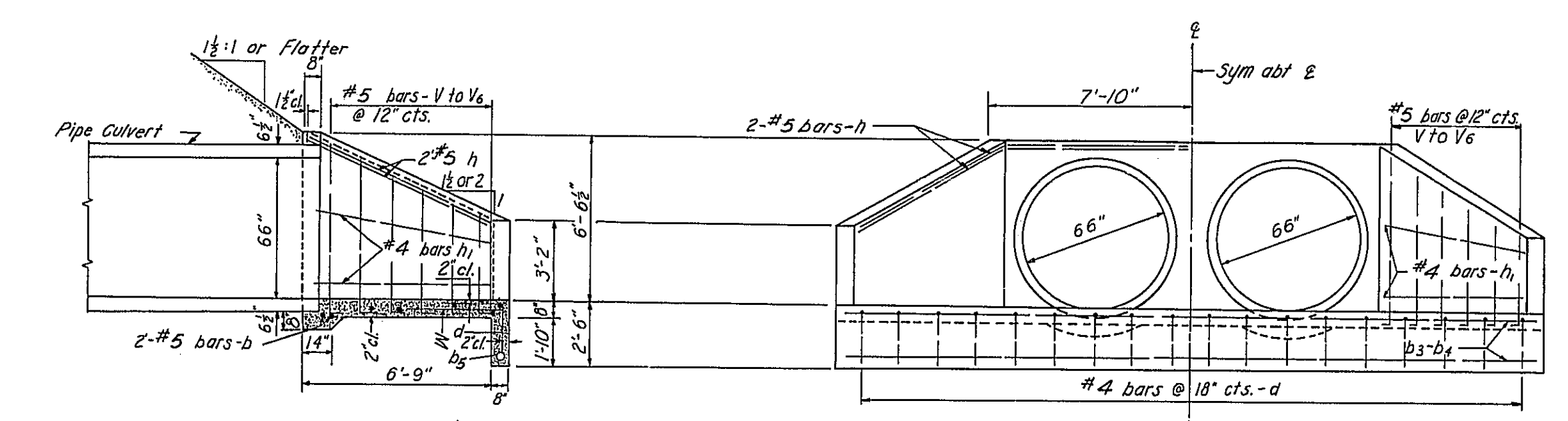


BAR	NO.	SIZE	LENG.
U	4	#6	7'-0"

CLASS X CONC.	CY	0.3
REINFC BARS	LBS.	40
EXP BOLTS	EACH	8



SPECIAL PIPE COLLAR FOR STATION 870+20.7 - MED. SCALE: NONE



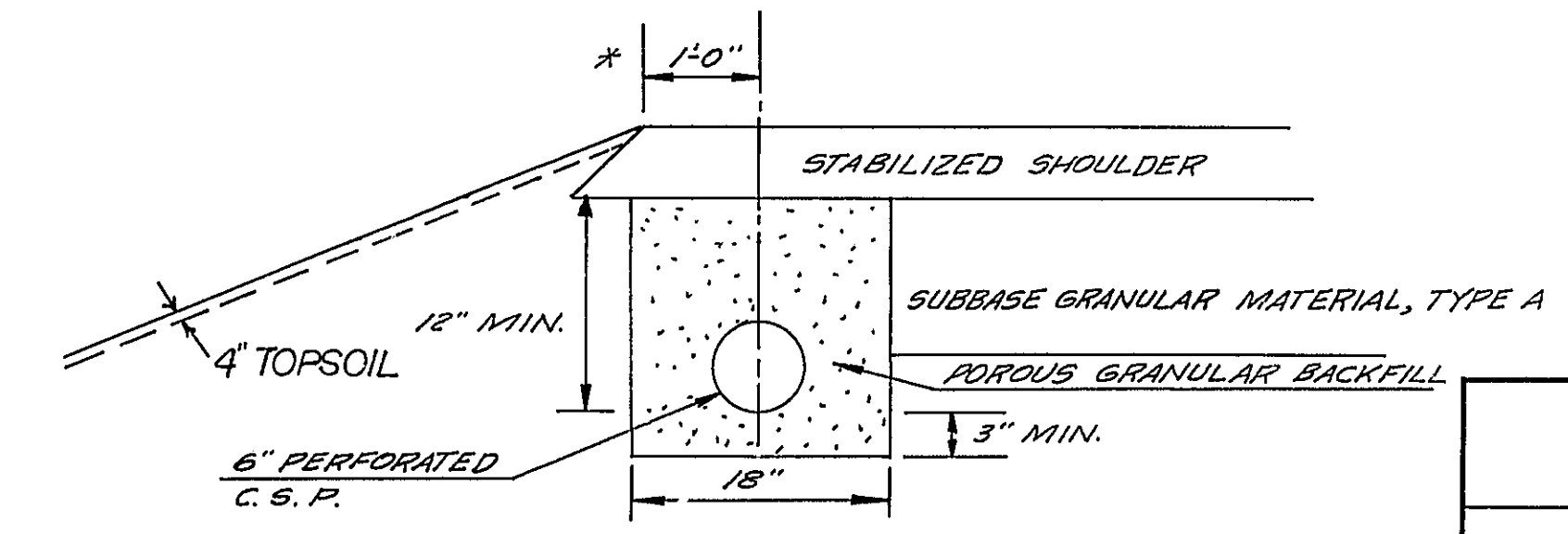
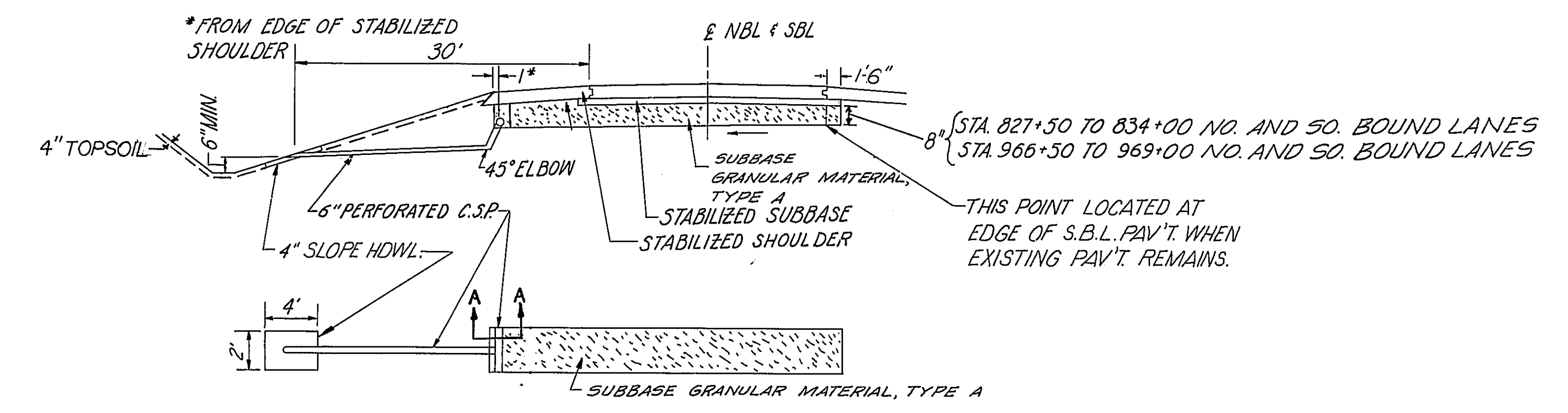
QUANTITIES FOR ONE HEADWALL D-66-2 CULVERT

BARS	NO.	SIZE	LENGTH
a	2	#5	15'-5"
b	2	#5	16'-0"
c	1	#4	17'-8"
d	1	#4	20'-0"
e	1	#4	22'-8"
f	2	#5	24'-8"
g	2	#5	24'-8"
h	2	#4	19'-8"
i	2	#5	8'-8"
j	2	#5	8'-8"
k	2	#4	4'-0"

CLASS X CONCRETE CULVERTS TO BE REINFORCED WITH BARS

NOTE: CLASS X CONCRETE SHALL BE USED THROUGHOUT. BUILD TOPS OF HEADWALLS PARALLEL TO GRADE LINE.

REINFORCED CONCRETE HEADWALL FOR TWIN 66" PIPE CULVERTS @ RIGHT ANGLE TO ROADWAY STA. 947+62.50 SCALE: NONE



SECTION A-A

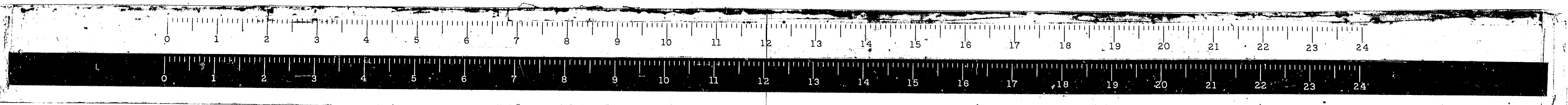
SCALE: NONE

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDING
 DIVISION OF HIGHWAYS

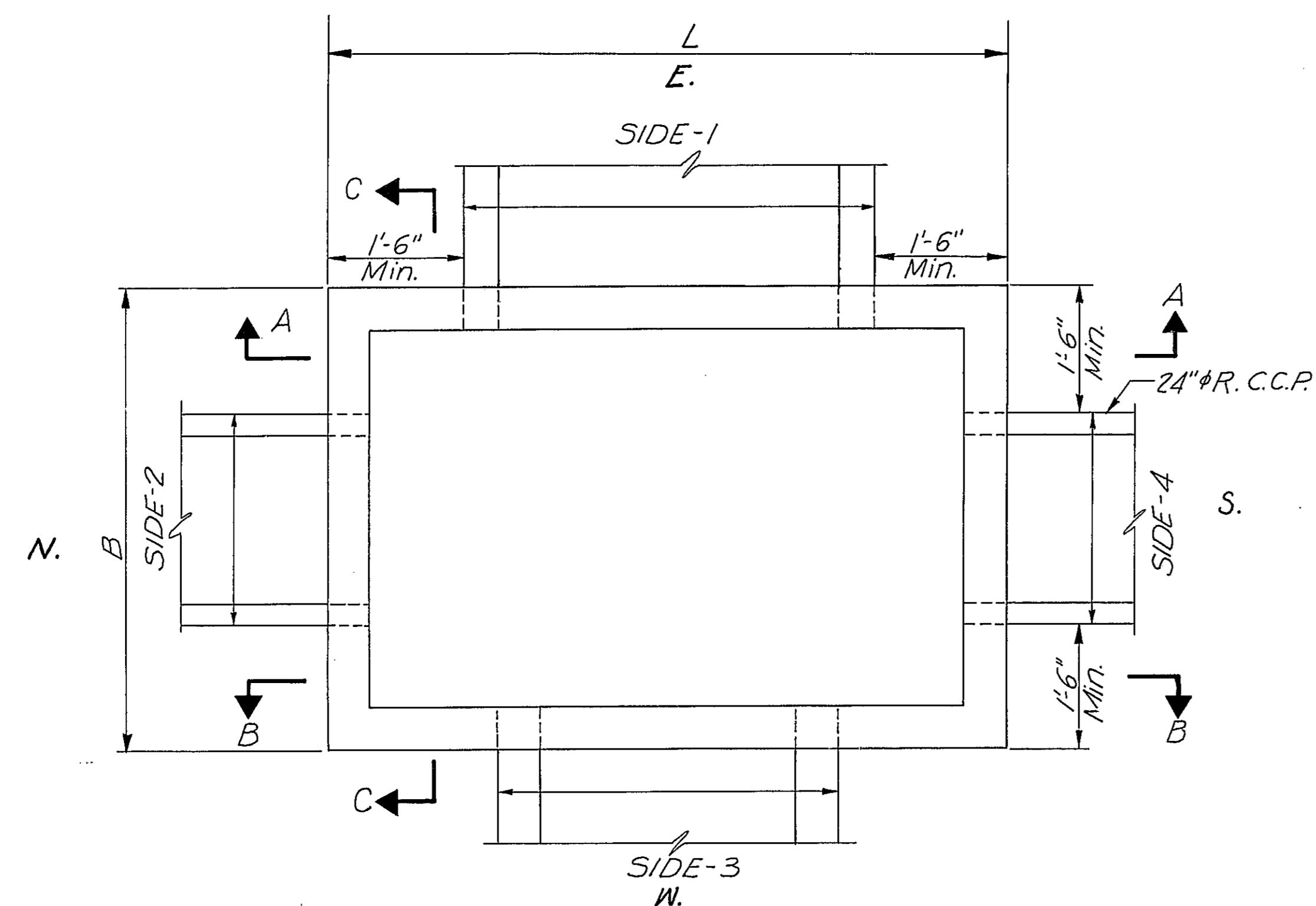
**SPECIAL CULVERT
 DETAILS**

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

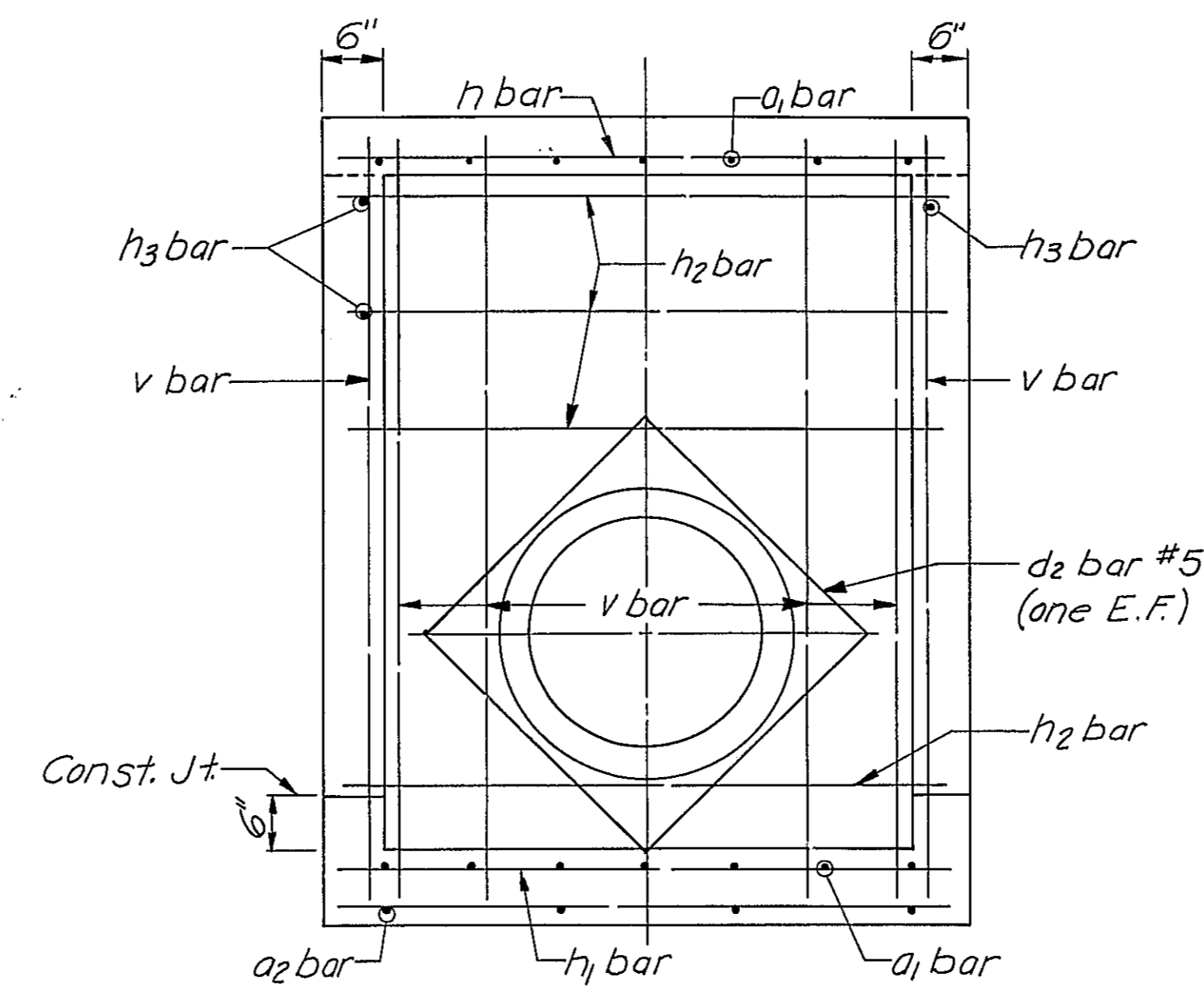
DESIGNED BY FTP
 DRAWN BY MD
 CHECKED BY KDP
 APPROVED BY _____



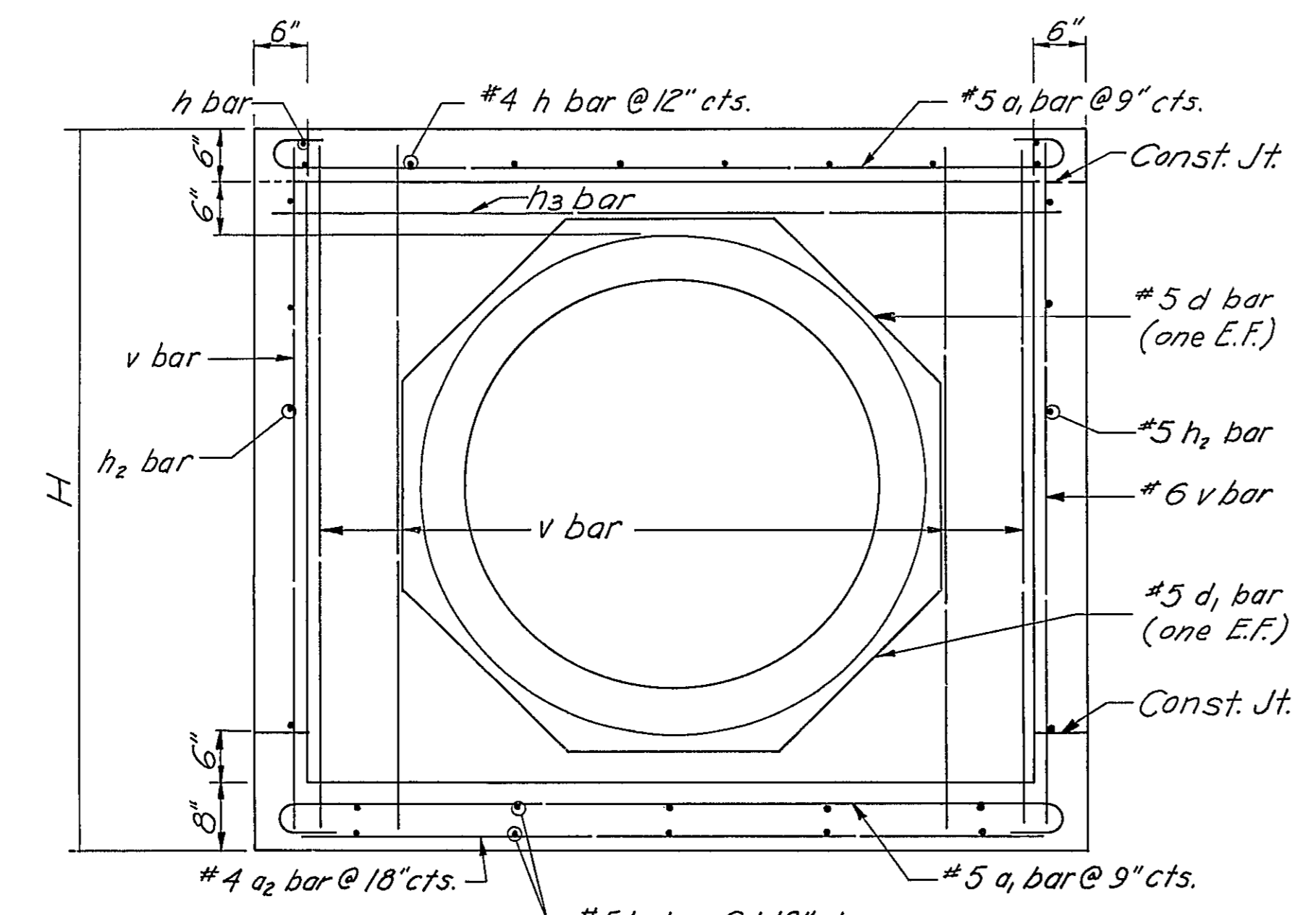
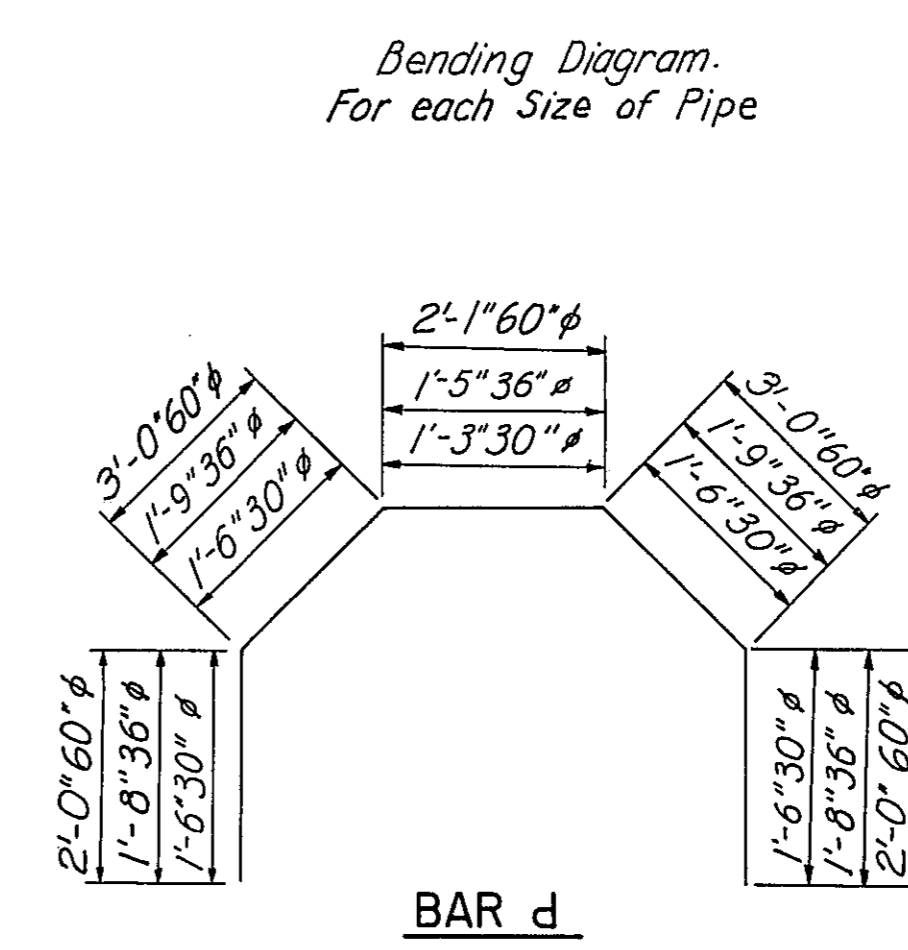
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MOVT.	206	83
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



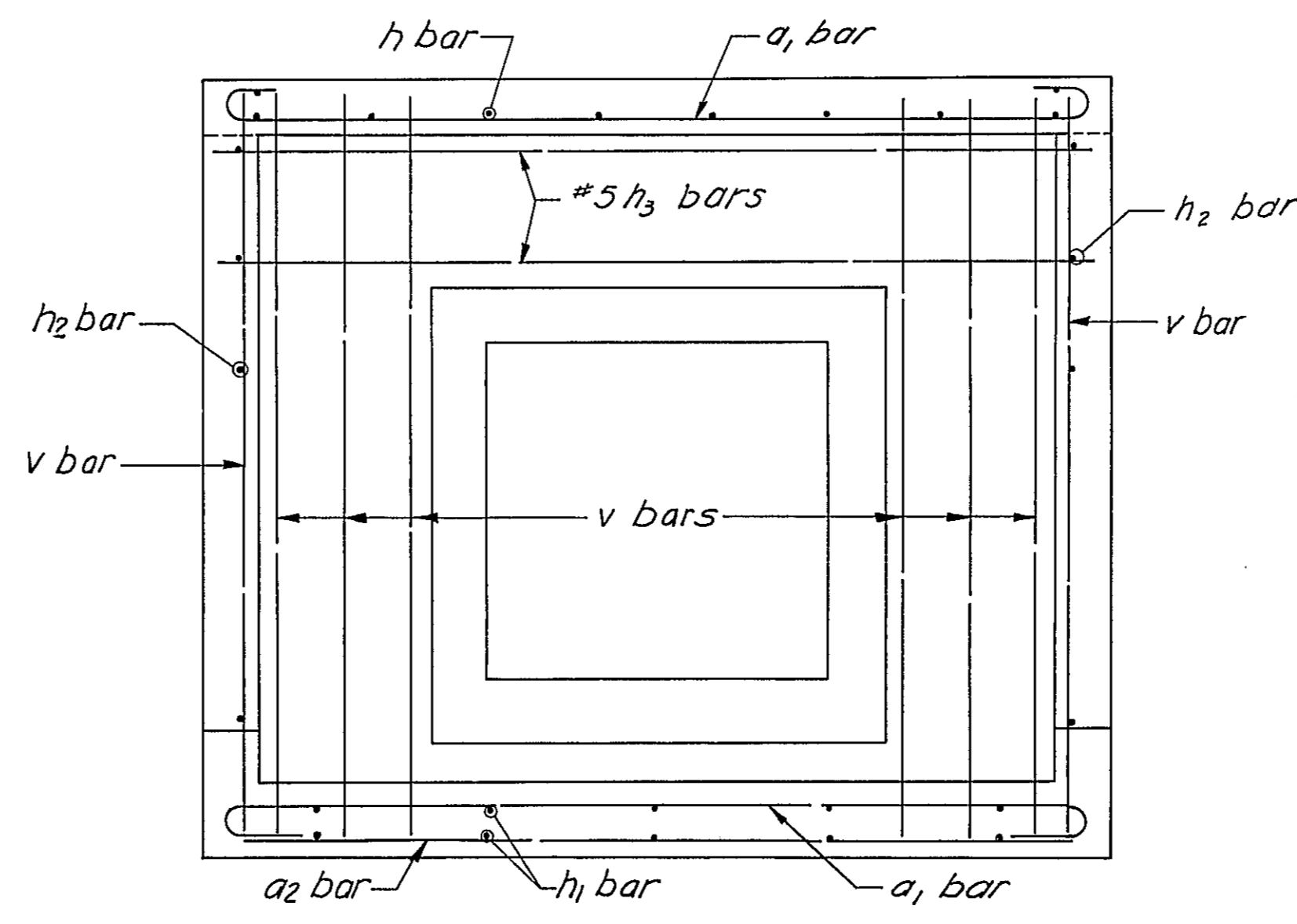
PLAN
Scale: 3/4"=1'-0"



SECTION C-C
Scale: 3/4"=1'-0"



SECTION A-A
Scale: 3/4"=1'-0"



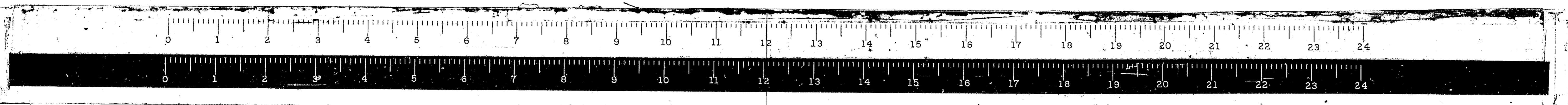
SECTION B-B
Scale: 3/4"=1'-0"

DESIGNED BY MS
 DRAWN BY MJD
 CHECKED BY F.T.P.
 APPROVED BY _____

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORK & BUILDINGS
 DIVISION OF HIGHWAYS

JUNCTION CHAMBER DETAILS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS



JUNCTION CHAMBER TABLE

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	84
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

STATION	LOCATION	SIZE OF CULVERT				SIZE OF CHAMBER FEET			CONCRETE QUANTITY CU. YDS.	REINFORCEMENT													TOTAL REINFORCEMENT LBS.				
		EAST	NORTH	WEST	SOUTH	WIDTH B	LENGTH L	HEIGHT H		MARK OF BARS																	
										a ₁	a ₂	d	d ₁	d ₂	h	h ₁	h ₂	h ₃	v								
853+25	WFR-SBL	2'x2' BOX INV. 645.21	24" Ø INV. 645.93	30" Ø INV. 645.25	24" Ø INV. 645.93	5'-6"	6'-1"	5'-3"	2.6	SIZE NO. 16	#5 7'-0"	#4 14	#5 7'-3"	#5 15	#5 4'-3"	#5 9	#4 2'-8"	#5 5'-3"	#5 28	#5 5'-3"	#5 33	#5 5'-0"	#6 16	#6 2	#6 16	448	
853+25	MED	36" Ø INV. 645.06	24" Ø INV. 645.19	2'x2' BOX INV. 645.06	24" Ø INV. 645.25	5'-6"	6'-8"	5'-10"	3.0	SIZE NO. 16	#5 7'-7"	#4 16	#5 8'-3"	#5 17	#5 2'-2"	#5 11	#5 16	#4 9	#5 10	#5 8	#5 5'-3"	#5 33	#5 5'-3"	#5 3	#6 18	#6 5'-7"	518
870+20.7 **	WFR-SBL	2'x2' BOX INV. 644.33	24" Ø INV. 646.20	30" Ø INV. 644.25	24" Ø INV. 646.12	5'-6"	6'-1"	6'-3"	3.0	SIZE NO. 16	#5 7'-0"	#4 14	#5 7'-3"	#5 15	#5 4'-3"	#5 9	#4 2'-8"	#5 5'-3"	#5 28	#5 5'-3"	#5 33	#5 5'-0"	#6 16	#6 2	#6 16	473	
880+21.80	WFR-SBL	2'x2' BOX INV. 643.84	24" Ø INV. 643.85	30" Ø INV. 643.86	24" Ø INV. 643.85	5'-6"	6'-1"	5'-3"	2.6	SIZE NO. 16	#5 7'-0"	#4 14	#5 7'-3"	#5 15	#5 4'-3"	#5 9	#4 2'-8"	#5 5'-3"	#5 28	#5 5'-3"	#5 33	#5 5'-0"	#6 16	#6 2	#6 16	448	
880+21.80	MED	30" Ø INV. 643.66	24" Ø INV. 644.70	24" Ø INV. 643.66	24" Ø INV. 644.70	5'-6"	6'-1"	5'-3"	2.6	SIZE NO. 16	#5 7'-0"	#4 14	#5 7'-3"	#5 15	#5 4'-3"	#5 9	#4 2'-8"	#5 5'-3"	#5 28	#5 5'-3"	#5 33	#5 5'-0"	#6 18	#6 2	#6 18	485	
880+21.80	EFR-NBL	36" Ø INV. 643.48	24" Ø INV. 643.74	30" Ø INV. 643.48	24" Ø INV. 643.74	5'-6"	6'-8"	5'-10"	3.0	SIZE NO. 16	#5 7'-7"	#4 16	#5 8'-3"	#5 17	#5 2'-2"	#5 11	#5 16	#4 9	#5 10	#5 8	#5 5'-3"	#5 33	#5 5'-3"	#5 3	#6 18	#6 5'-7"	545
917+42	MED	24" Ø INV. 647.60	—	18" Ø INV. 647.60	24" Ø INV. 647.85	5'-6"	5'-6"	4'-8"	2.4	SIZE NO. 16	#5 6'-5"	#4 13	#5 4'-9"	#5 13	#5 2'-8"	#5 67	#4 5'-3"	#5 5'-3"	#5 28	#5 5'-3"	#5 38	#5 5'-3"	#6 16	#6 3	#6 21	463	
959+00	EFR	30" Ø INV. 645.93	—	24" Ø INV. 645.93	24" Ø INV. 645.95	5'-6"	6'-1"	5'-3"	2.4	SIZE NO. 16	#5 7'-0"	#4 14	#5 7'-3"	#5 15	#5 4'-3"	#5 9	#4 2'-8"	#5 5'-3"	#5 28	#5 5'-3"	#5 33	#5 5'-0"	#6 18	#6 2	#6 18	503	
981+28.6	MED	36" Ø INV. 653.67	24" Ø INV. 654.66	30" Ø INV. 653.67	—	5'-6"	6'-8"	5'-10"	3.1	SIZE NO. 16	#5 7'-7"	#4 16	#5 8'-3"	#5 17	#5 2'-2"	#5 11	#5 16	#4 9	#5 10	#5 8	#5 5'-3"	#5 33	#5 5'-3"	#5 3	#6 21	#6 5'-7"	552
991+86.7	MED	6'x3' BOX INV. 648.75	24" Ø INV. 650.84	60" Ø INV. 648.75	24" Ø INV. 650.85	5'-6"	10'-0"	8'-2"	4.9	SIZE NO. 16	#5 10'-11"	#4 25	#5 9'-3"	#5 25	#5 12'-1"	#5 17	#5 8'-1"	#4 2'-8"	#5 16	#4 12	#5 5'-3"	#5 42	#5 5'-3"	#5 8	#6 4	#6 18	712

TOTAL CONCRETE=29.6 CU.YDS.

TOTAL REINFORCEMENT= 5147 LBS.

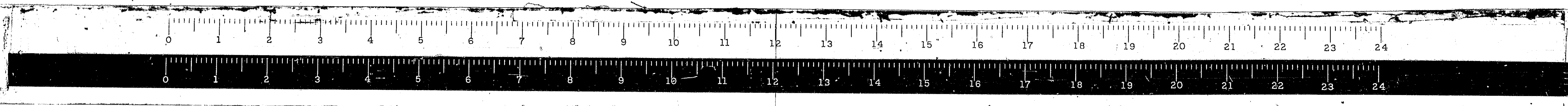
** 30" Ø SHALL CONTROL JUNCTION BOX HEIGHT AND A 1'6" CLEARANCE BETWEEN TOP OF PIPE AND BOTTOM OF TOP SLAB SHALL BE USED INSTEAD OF 6" AS SHOWN ON TYPICAL DETAIL

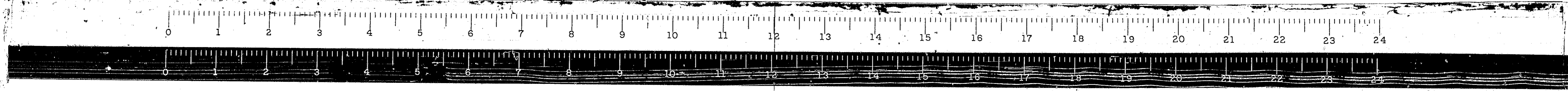
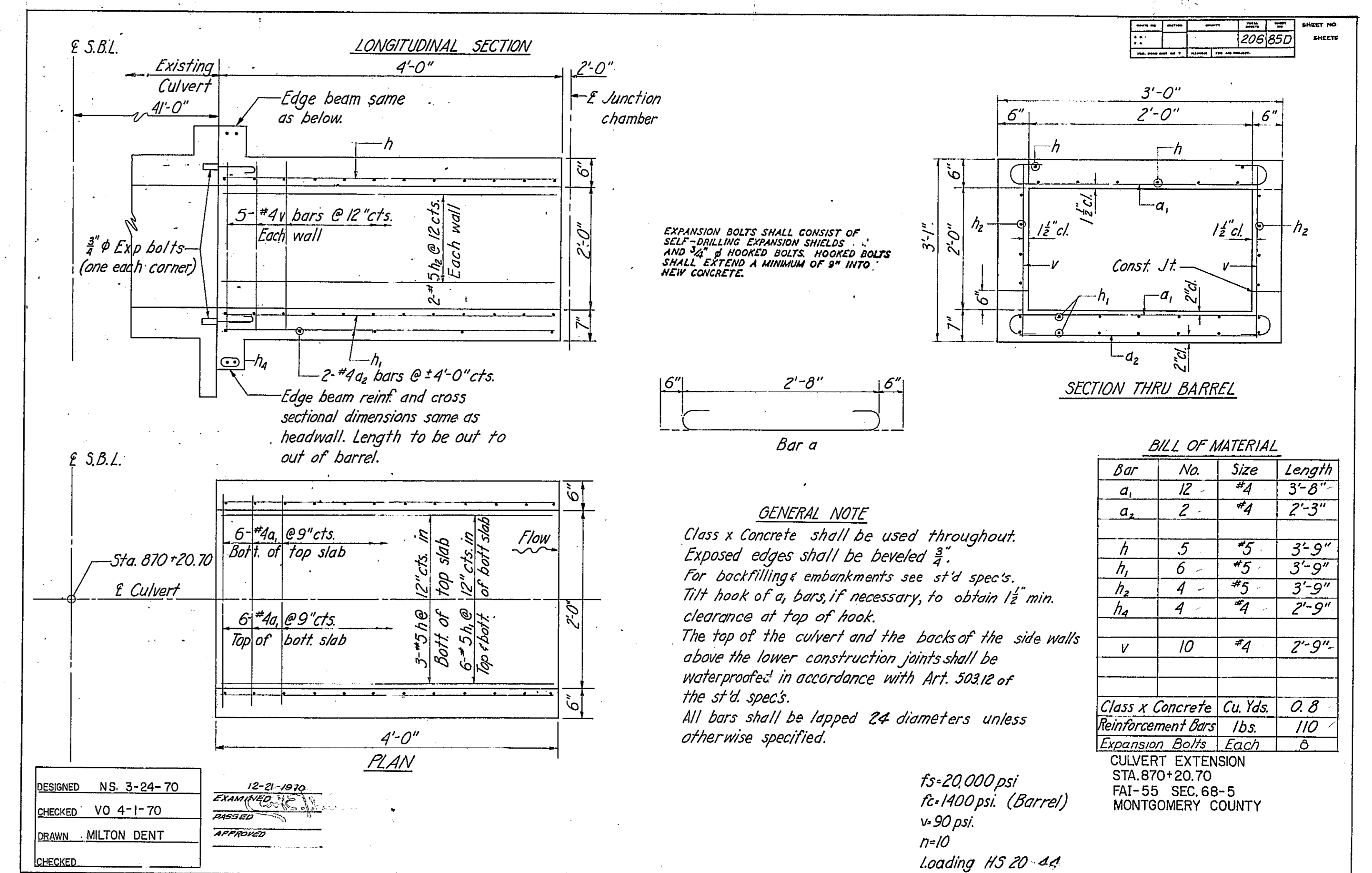
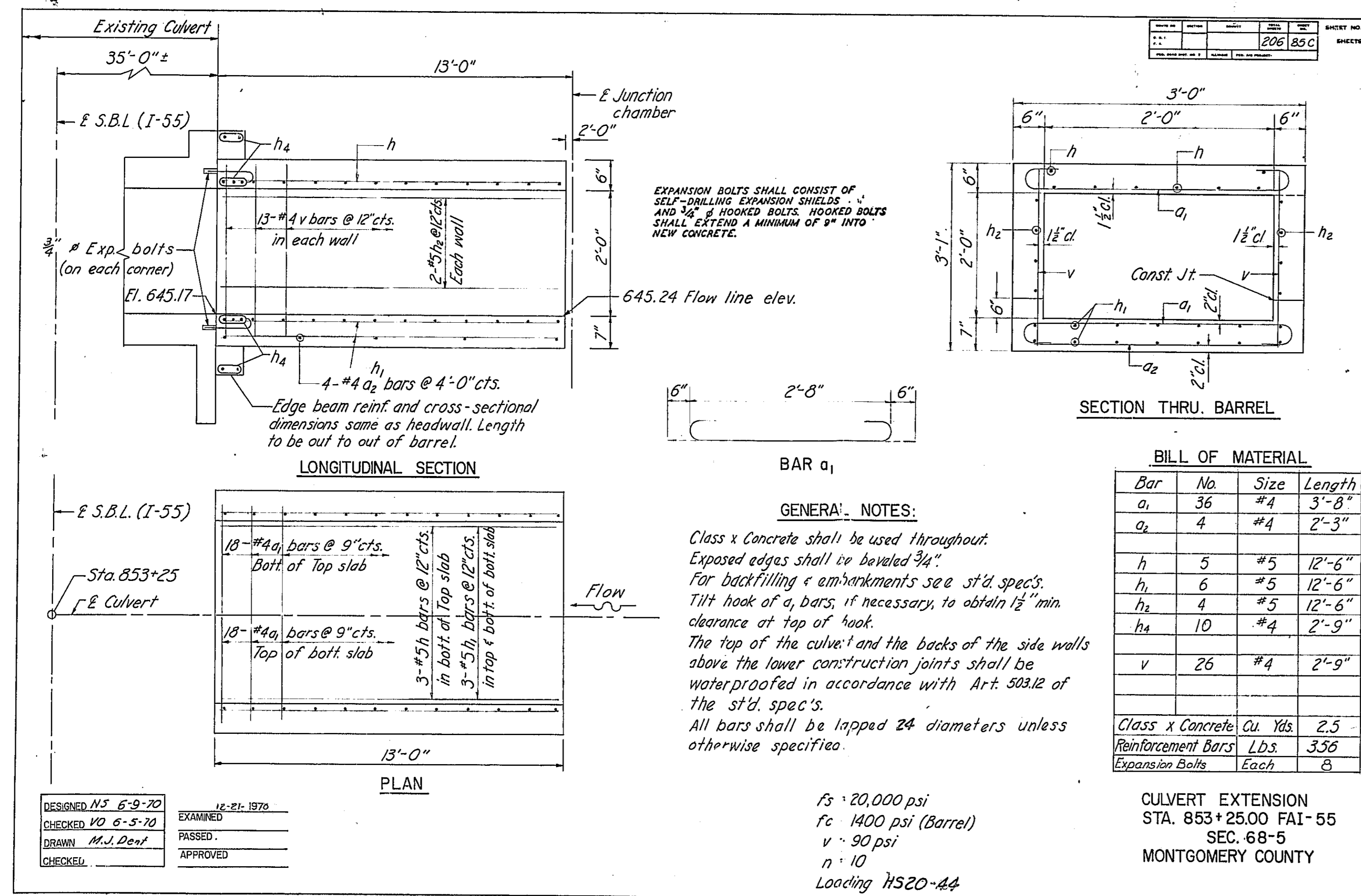
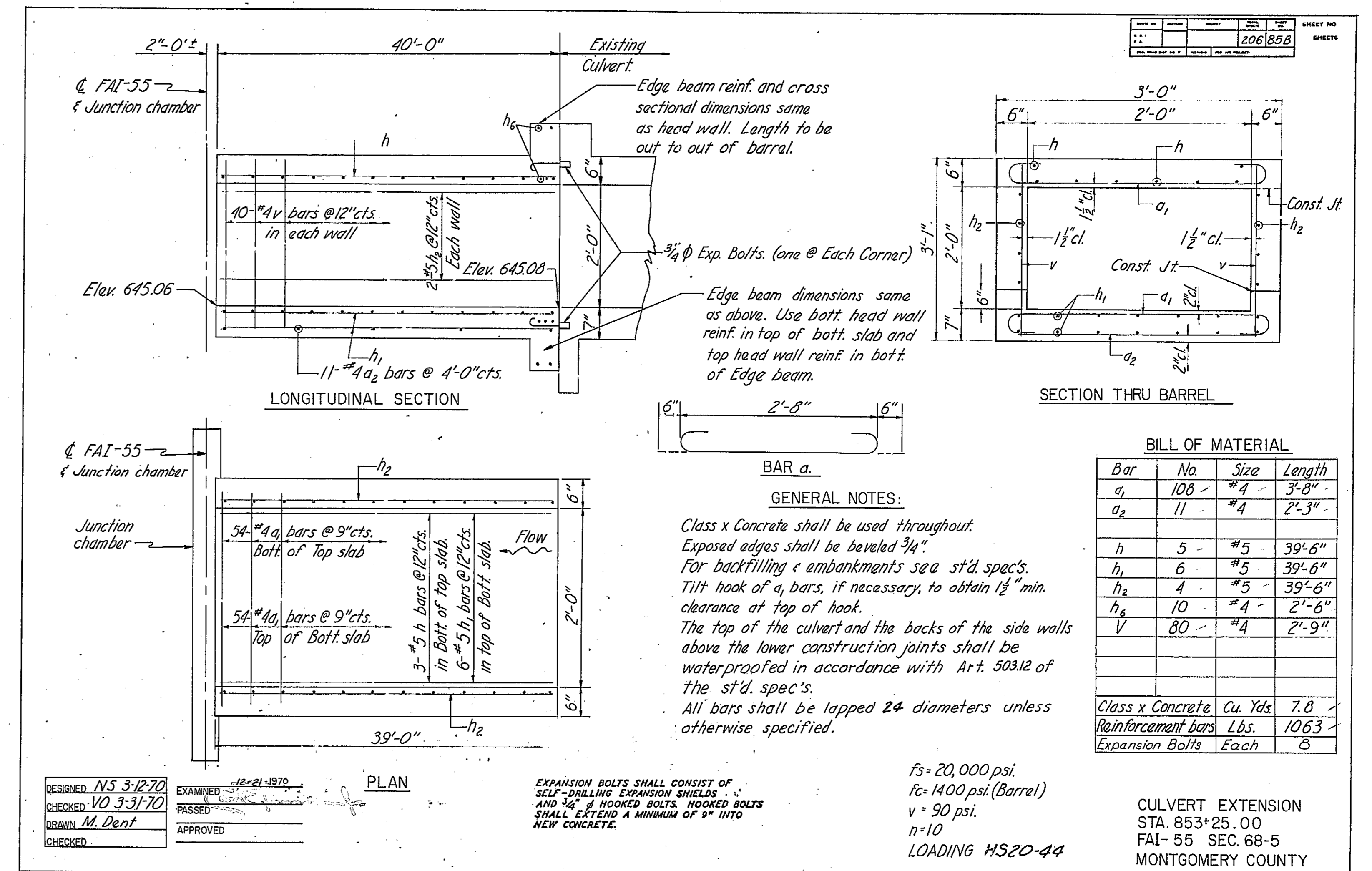
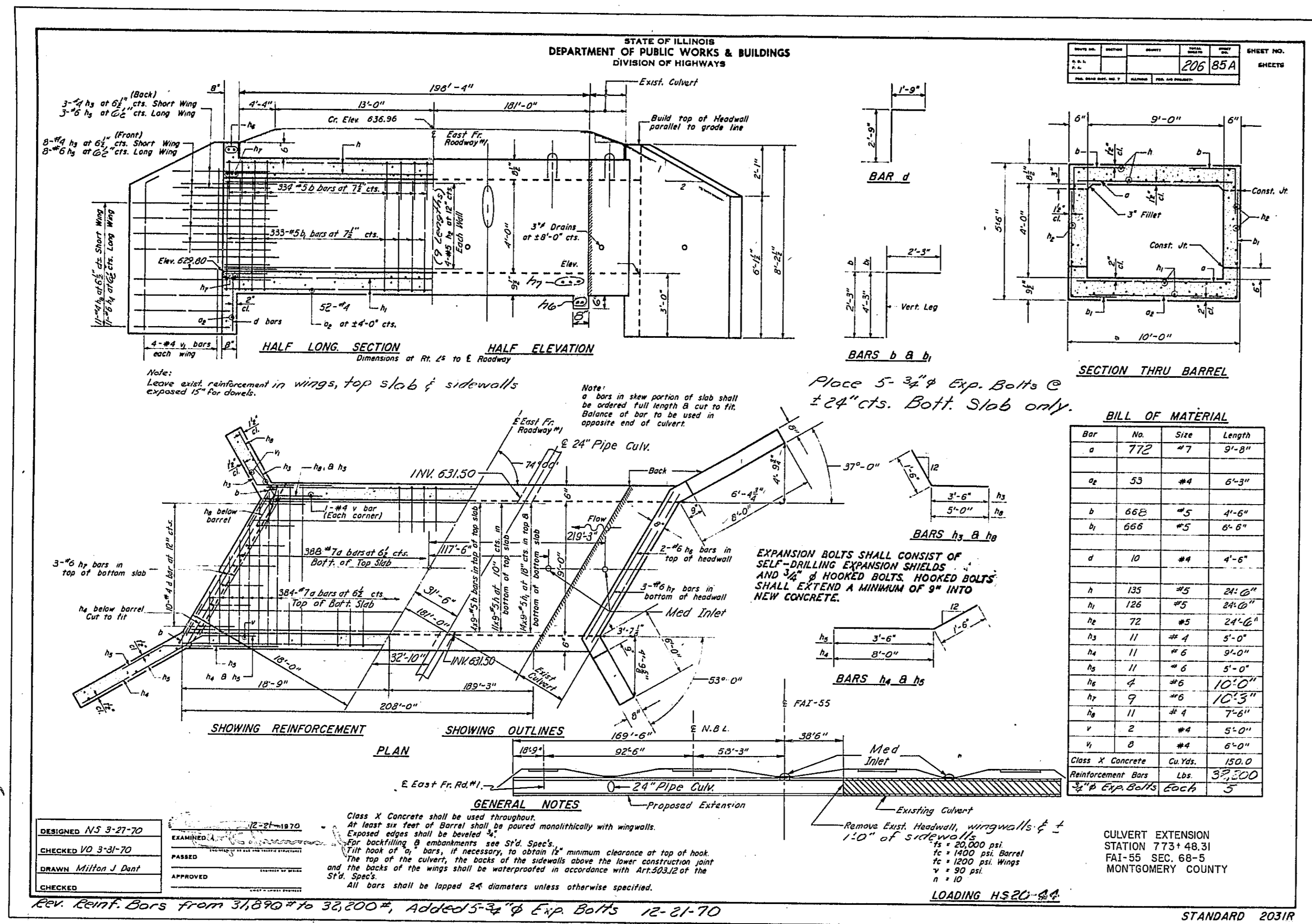
DESIGNED BY N.S.
 DRAWN BY M.J.D.
 CHECKED BY F.T.P.
 APPROVED BY _____

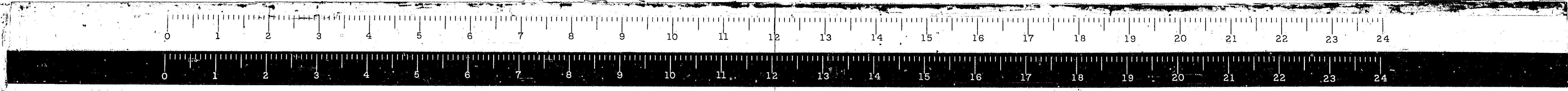
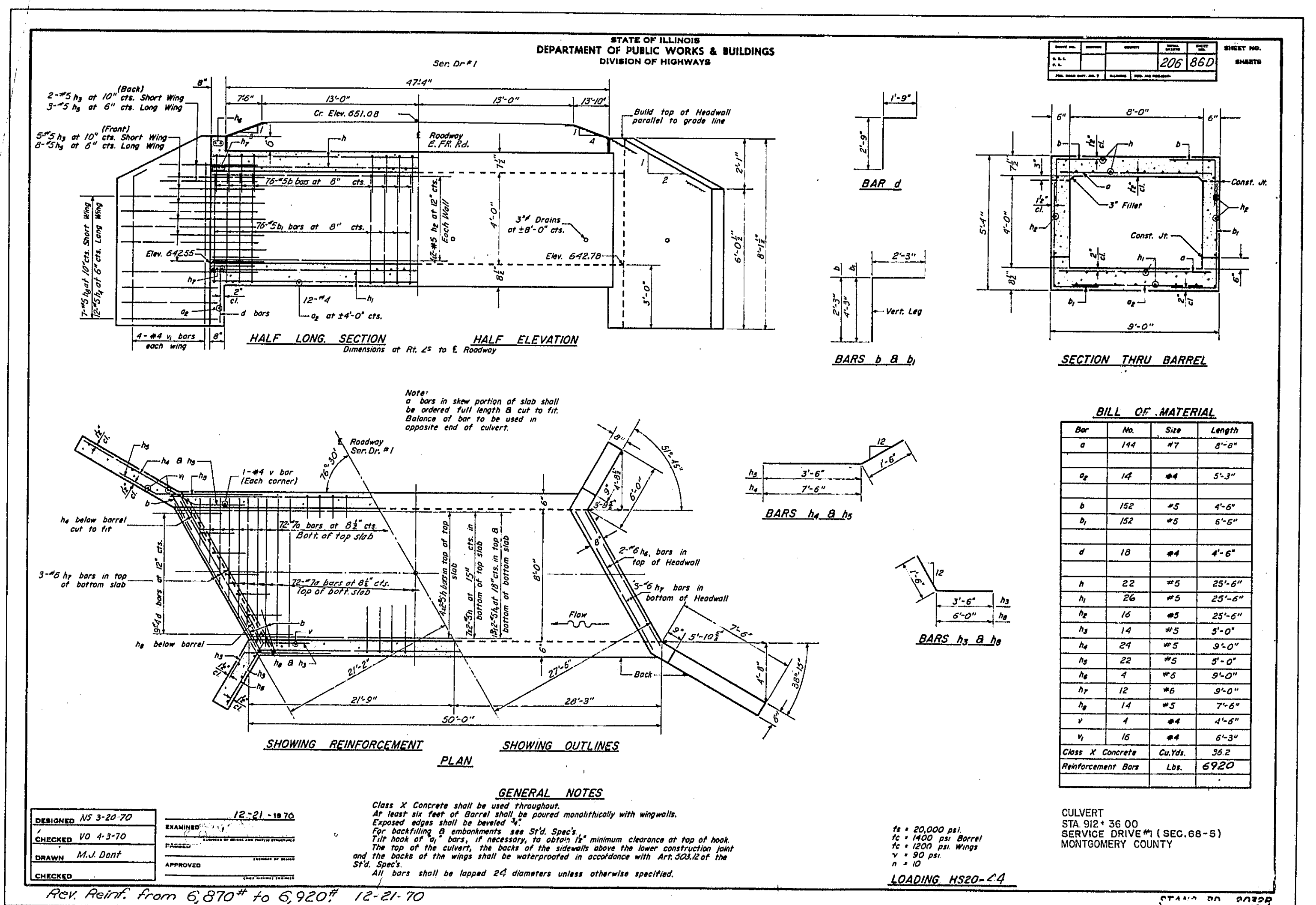
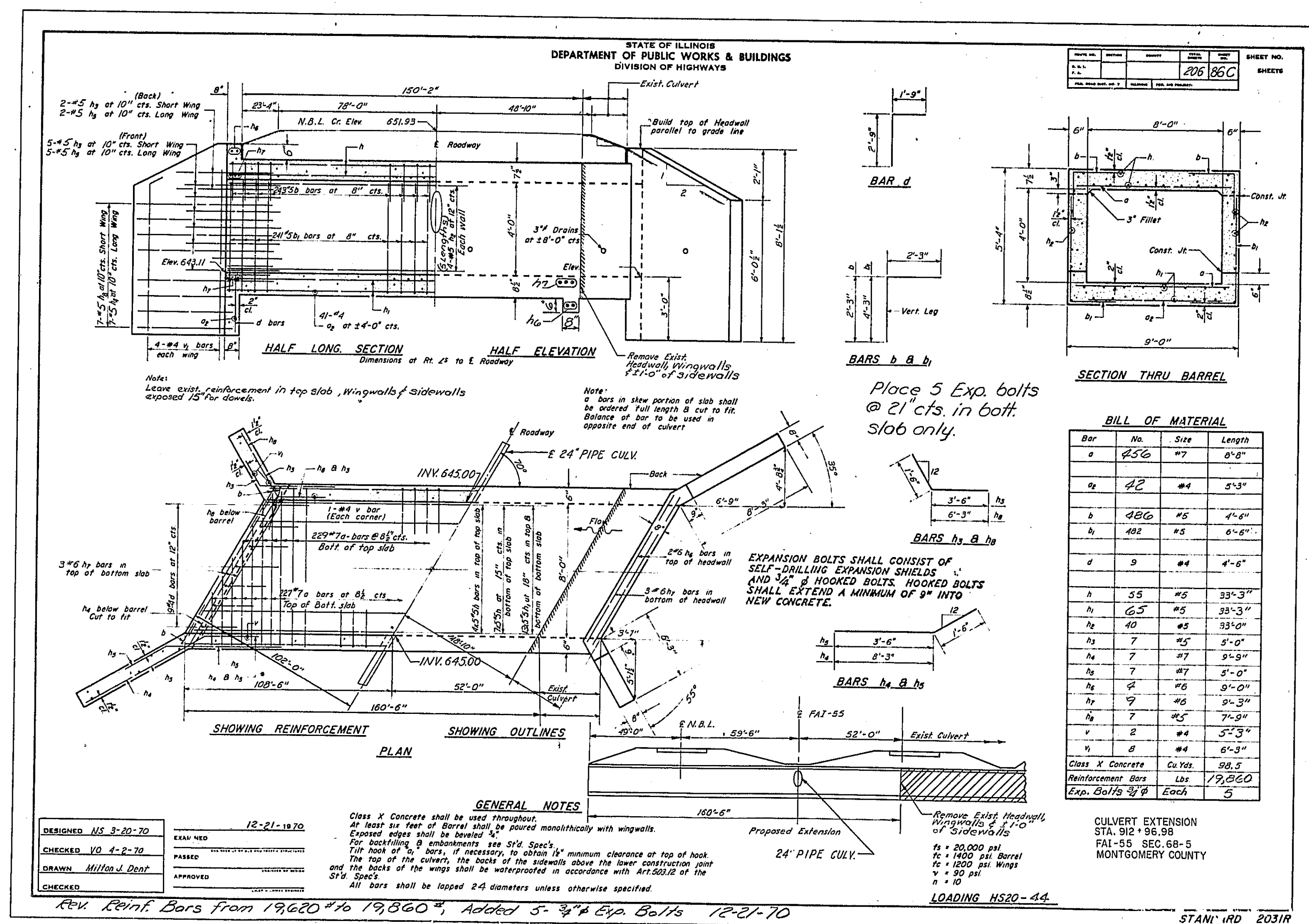
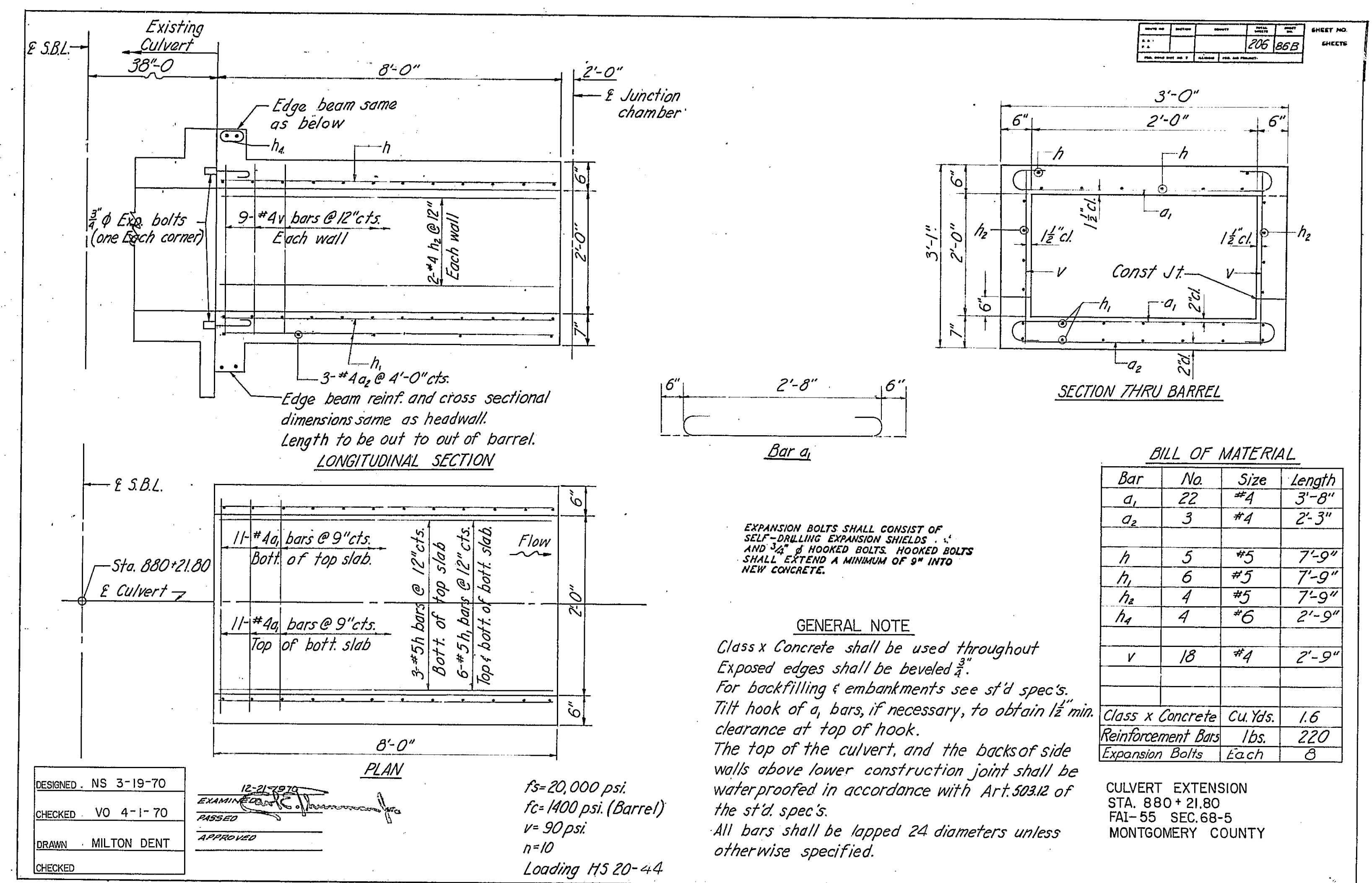
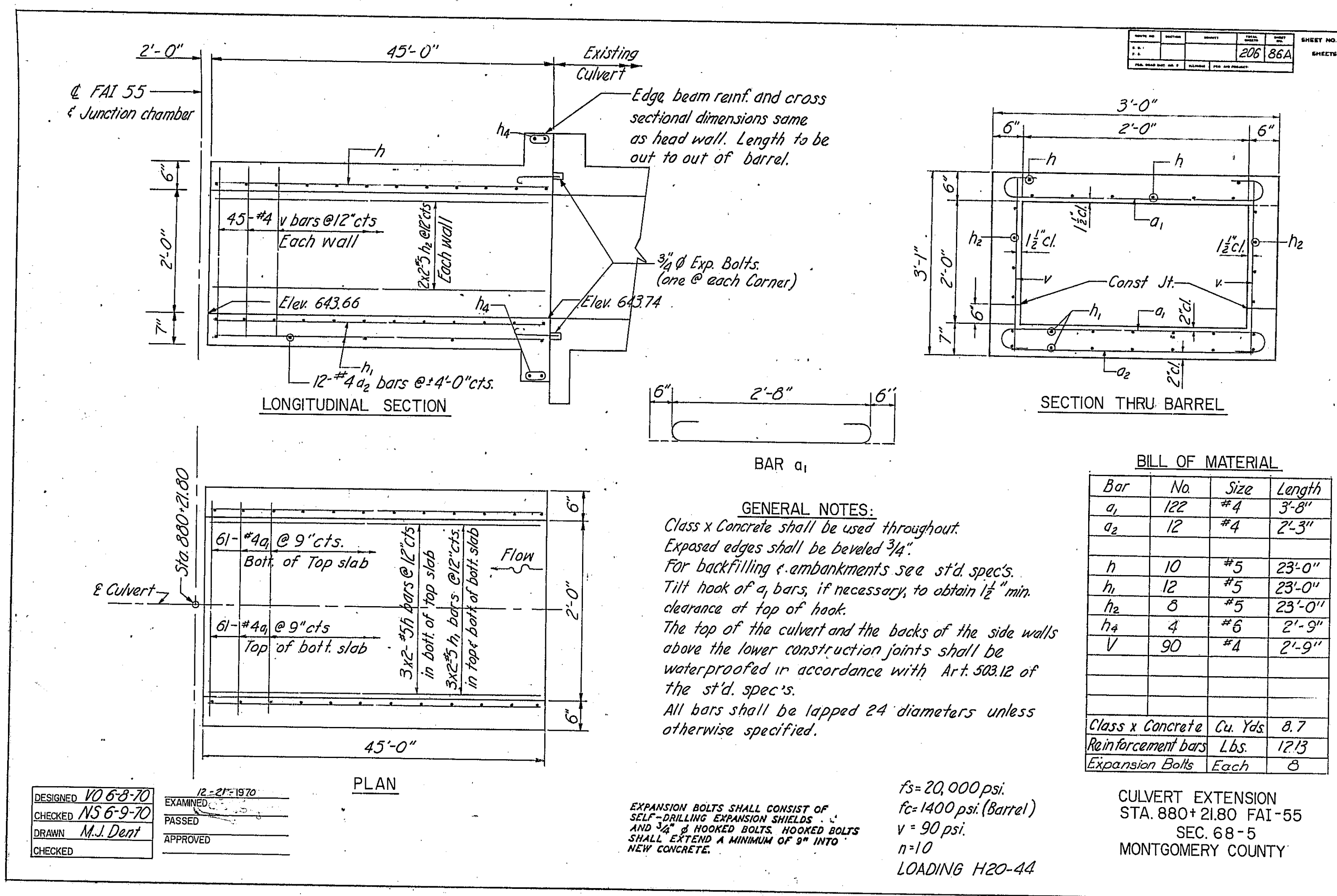
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORK & BUILDINGS
 DIVISION OF HIGHWAYS

JUNCTION CHAMBER DETAILS

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

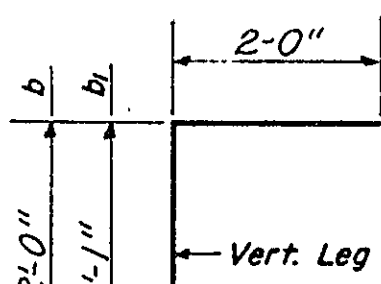
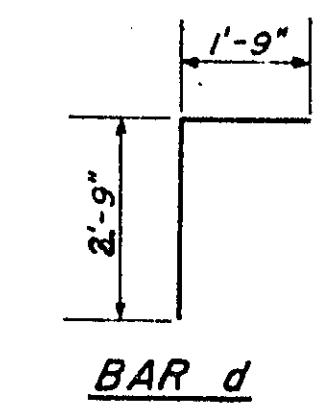
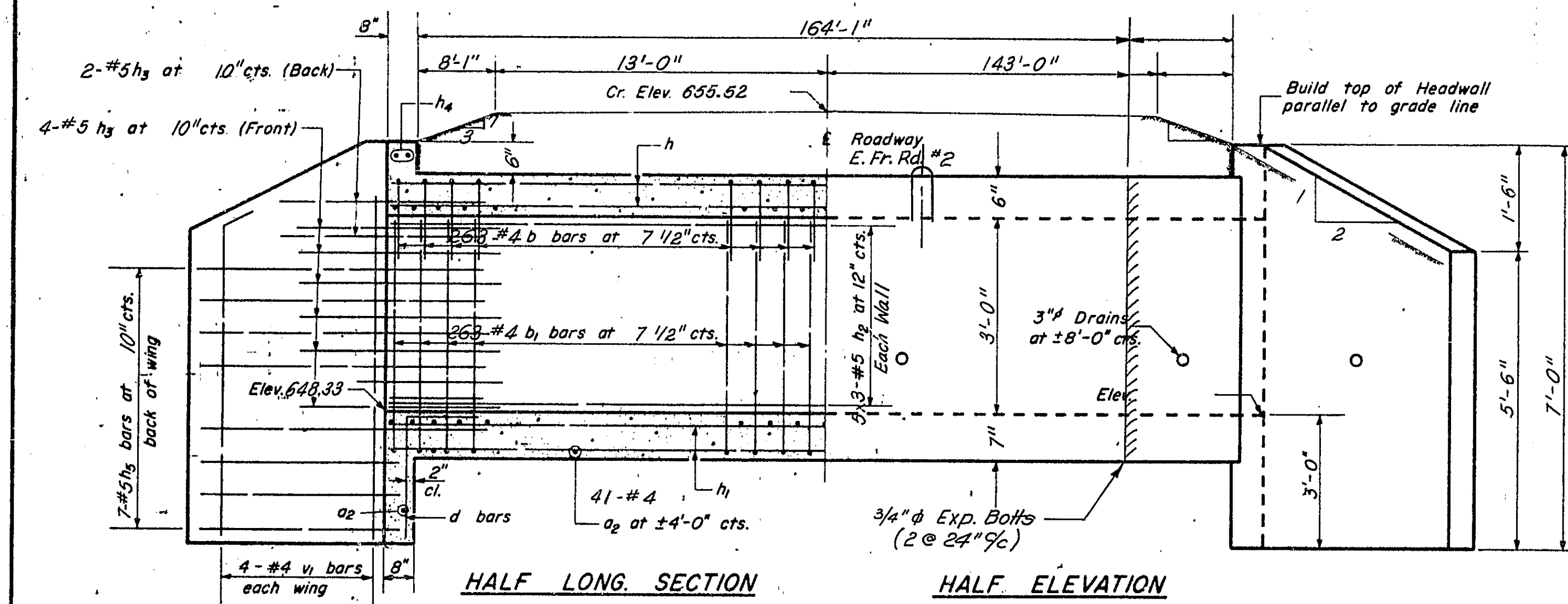




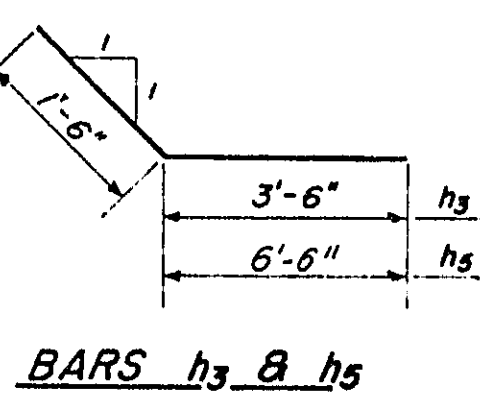


STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

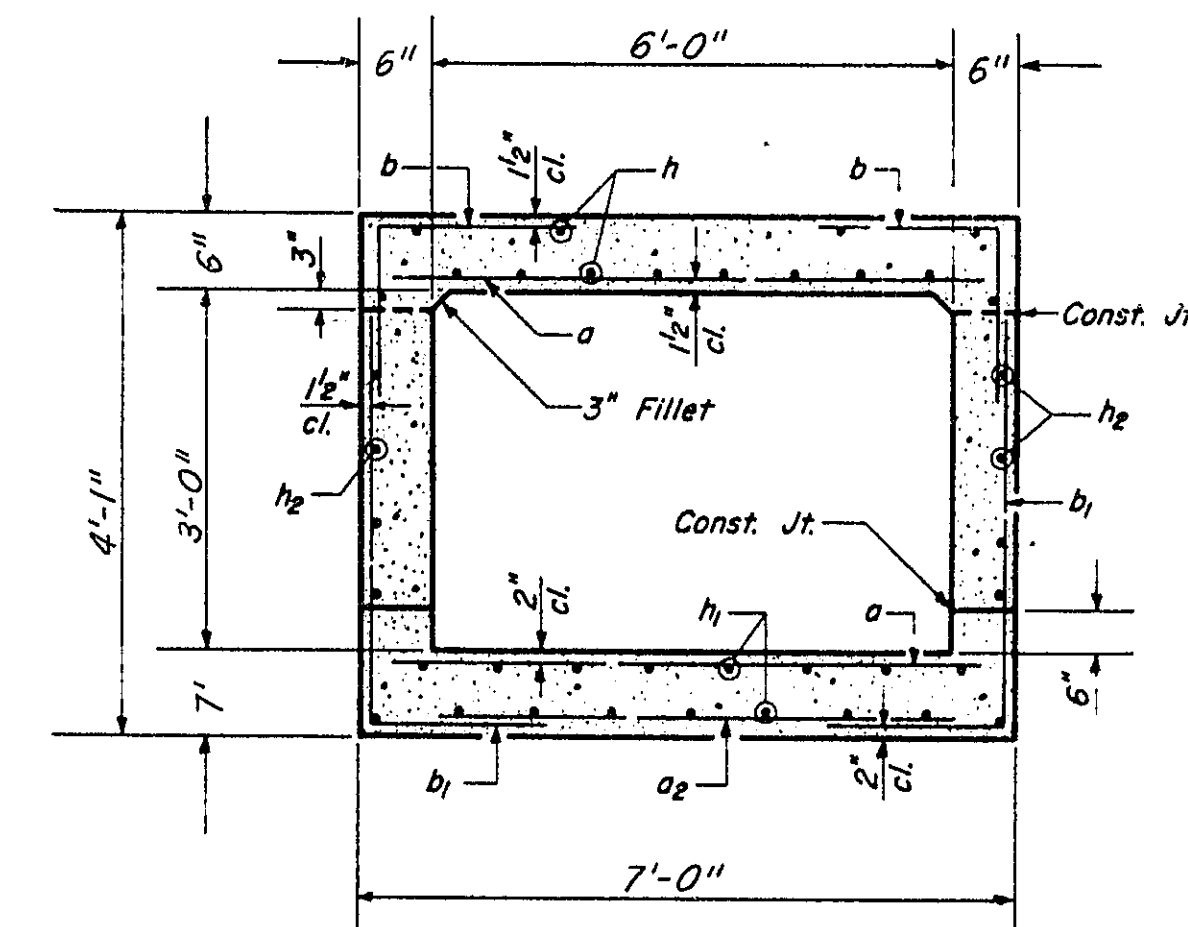
PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			206	87A
SHEETS				



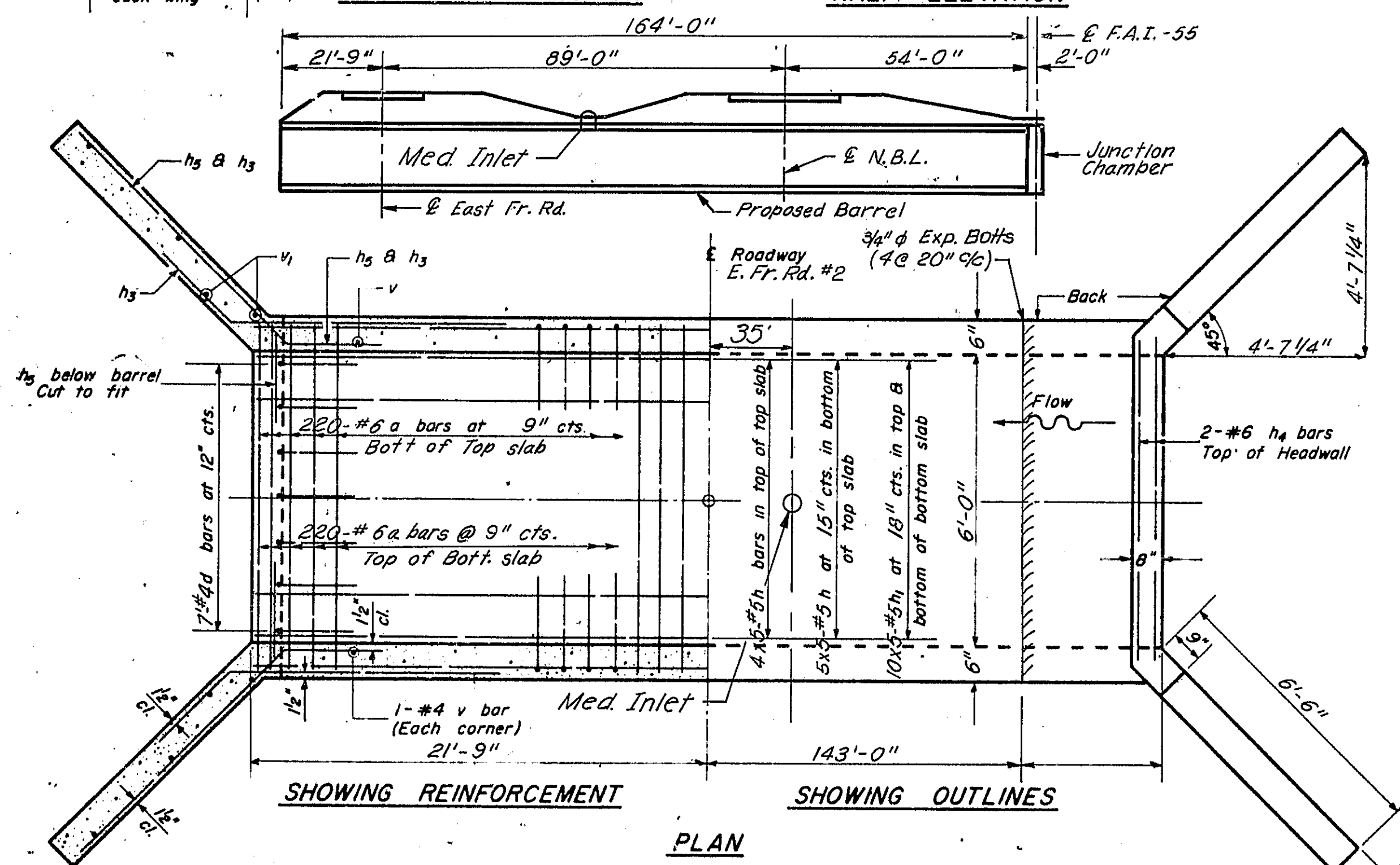
BARS b & b1



BARS h3 & h5



SECTION THRU BARREL



SHOWING REINFORCEMENT
SHOWING OUTLINES
PLAN

BILL OF MATERIAL

Bar	No.	Size	Length
a	440	#6	6'-8"
a2	42	#4	3'-9"
b	526	#4	4'-0"
b1	526	#4	5'-1"
d	7	#4	4'-6"
h	45	#5	34'-0"
h1	50	#5	34'-0"
h2	30	#5	34'-0"
h3	12	#5	5'-0"
h4	2	#6	6'-6"
h5	14	#5	8'-0"
v2	2	#4	3'-6"
v1	8	#4	6'-0"
Class X Concrete			Cu. Yds. 67.8
Reinforcement Bars			Lbs. 12,390
Expansion Bolts			Each 12

EXPANSION BOLTS SHALL CONSIST OF SELF-DRILLING EXPANSION SHIELDS AND 3/4" HOOKED BOLTS. HOOKED BOLTS SHALL EXTEND A MINIMUM OF 9" INTO NEW CONCRETE.

GENERAL NOTES

Class X Concrete shall be used throughout.
At least six feet of Barrel shall be poured monolithically with wingwalls.
Exposed edges shall be beveled.
For backfilling & embankments see Std. Specs.
The top of the Culvert, the backs of the sidewalls above the lower construction joint and the backs of the wings shall be waterproofed in accordance with Art. 503.12 of the Std. Specs.
All bars shall be lapped 24 diameters unless otherwise specified.

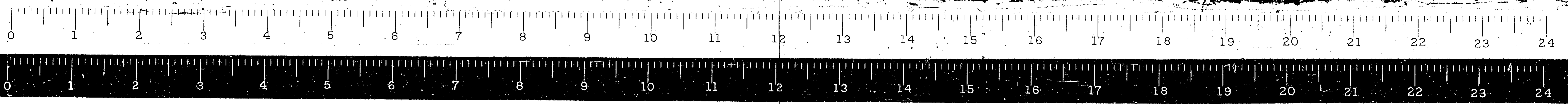
fs = 20,000 psi.
fc = 1400 psi. Barrel
fc = 1200 psi. Wings
v = 30 psi.
n = 10

LOADING HS20-44

CULVERT EXTENSION
STATION 991+86.70
FAI-55 SEC. 68-5
MONTGOMERY COUNTY

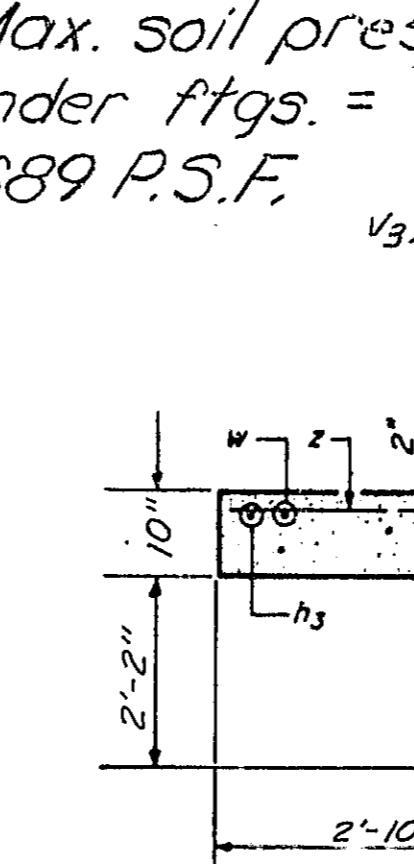
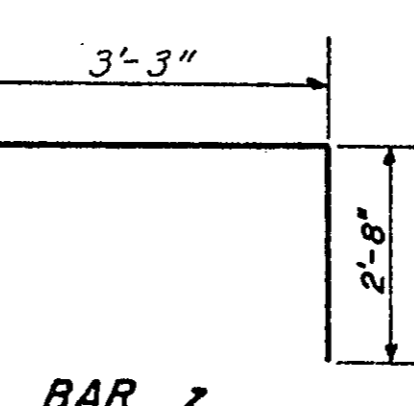
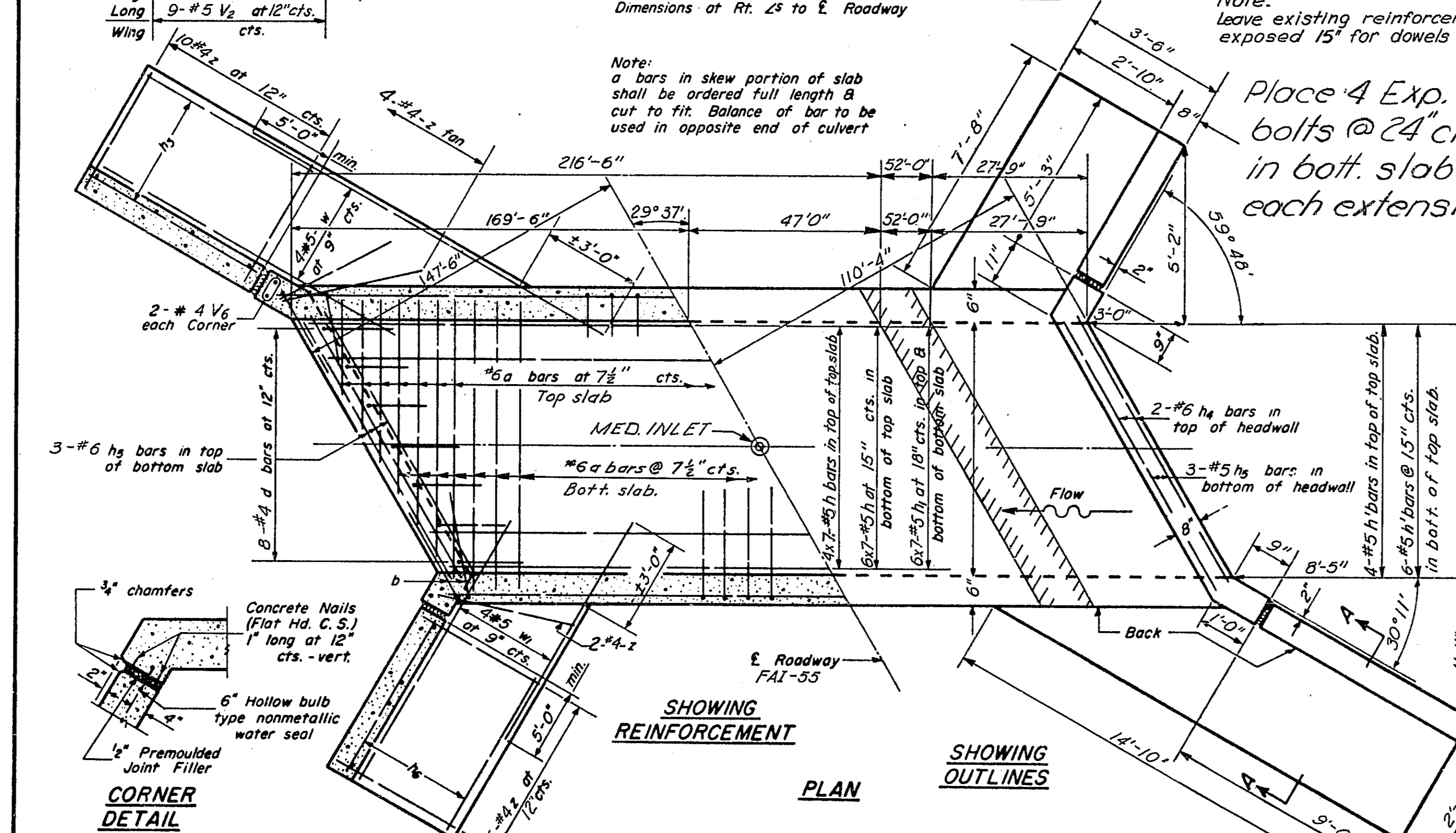
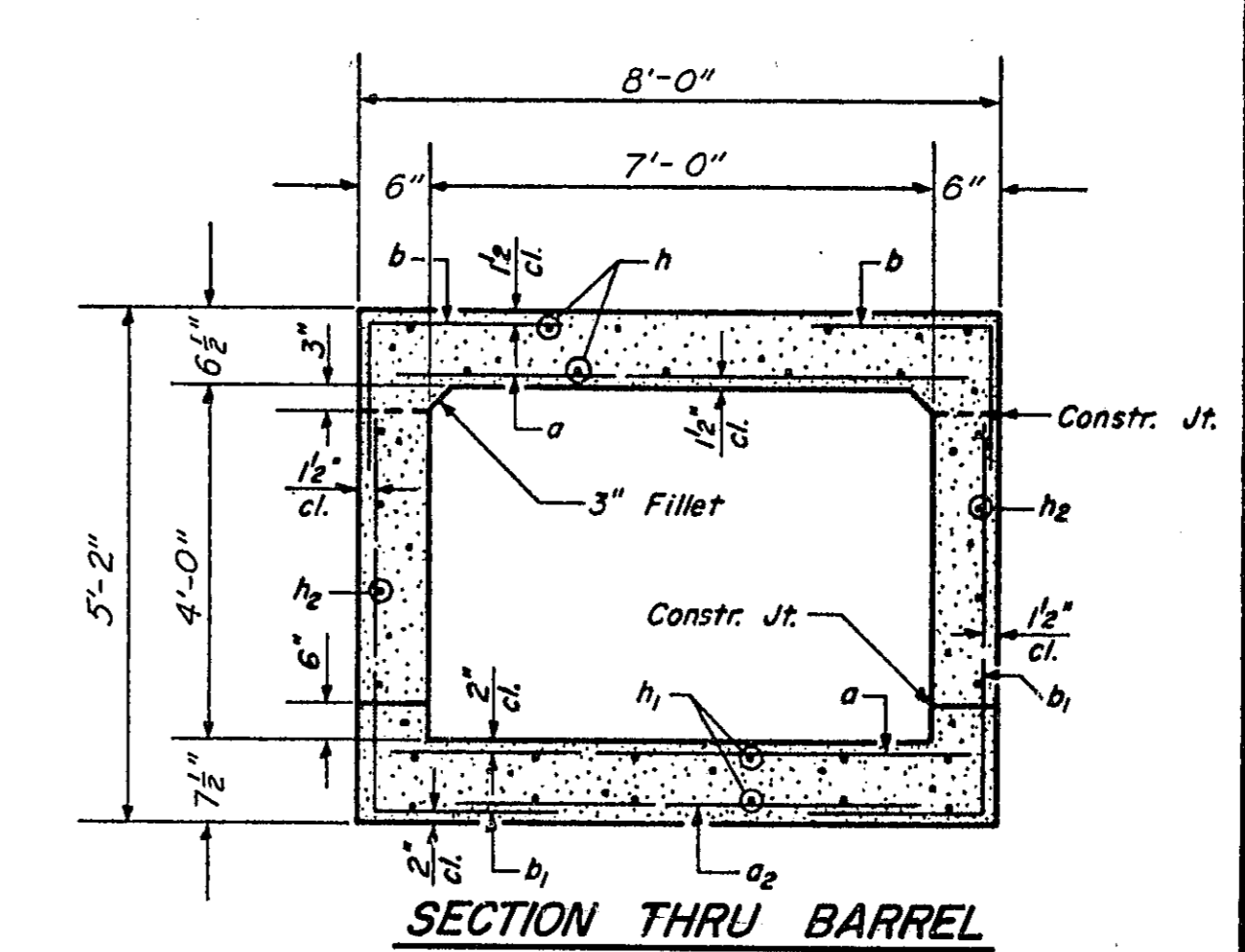
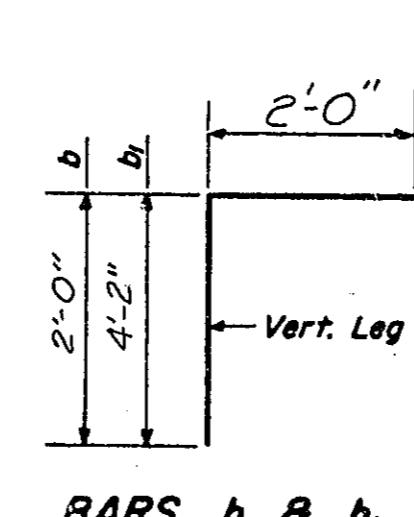
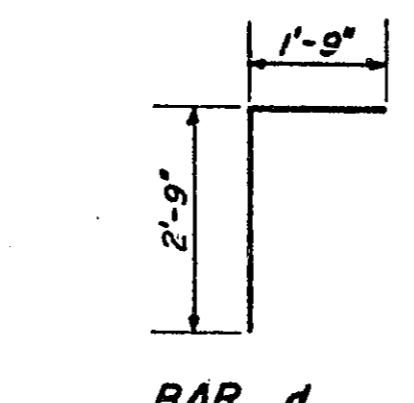
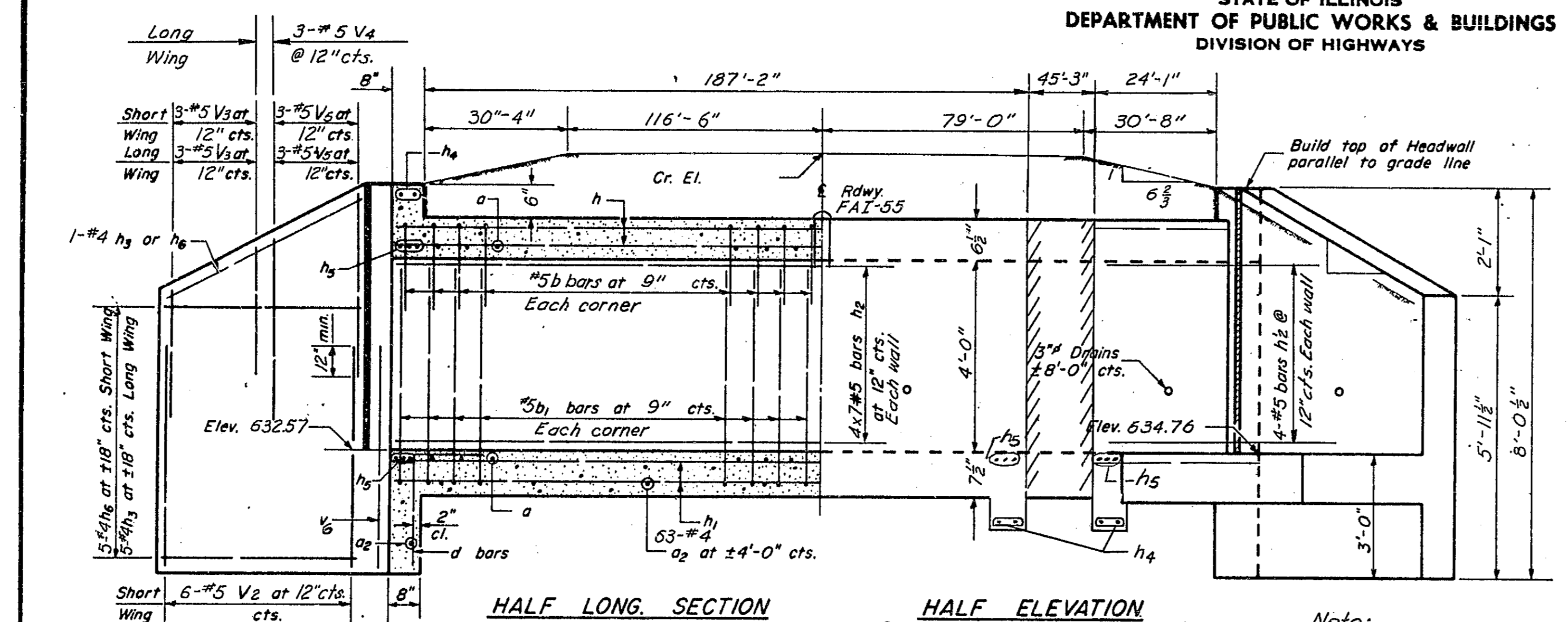
STANDARD 2030R

DESIGNED N.S. 3-20-70	EXAMINED 12-21-1970
CHECKED K.O. 4-3-70	PASSED
DRAWN S.D.D.	APPROVED
CHECKED	



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PROJECT NO.	206	SHEET NO.	87B
DATE		SCALE	



Note:
Leave existing reinforcement exposed 15' for dowels

Place 4 Exp. bolts @ 24" cts. in batt. slab of each extension.

LOADING HS20-44

Max. soil press. under ftgs. = 1689 P.S.F.

BILL OF MATERIAL

Bar	No.	Size	Length
h'	10	#5	28'-9"
h1	12	#5	28'-9"
h2	8	#5	27'-6"
a	788	#6	7'-8"
a2	65	#4	4'-9"
b	660	#5	4'-0"
b1	656	#5	6'-2"
d	16	#4	4'-6"
h	70	#5	32'-3"
h1	84	#5	32'-3"
h2	56	#5	32'-0"
h3	16	#4	8'-8"
h4	8	#6	9'-2"
h5	18	#6	9'-2"
h6	16	#4	5'-0"
h7	30	#4	4'-6"
v3	12	#4	2'-9"
v4	6	#4	3'-6"
v5	12	#4	4'-3"
v6	8	#4	7'-0"
w	8	#5	13'-0"
w1	8	#5	10'-0"
z	44	#4	5'-11"

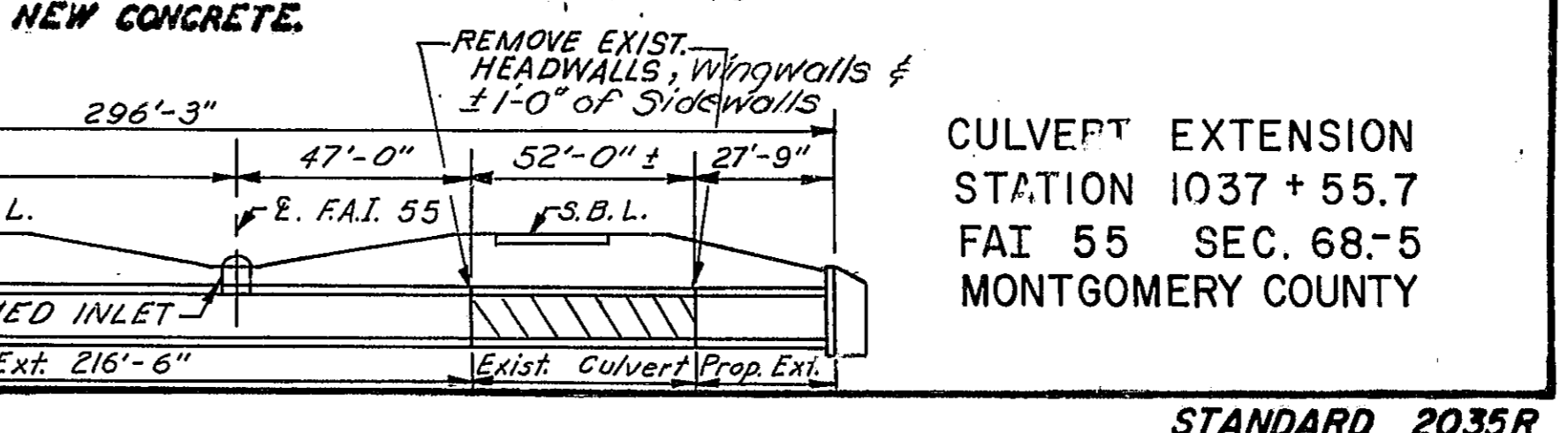
Class X Concrete Cu. Yds. 132.3
Reinforcement Bars Lbs. 25,307
Exp. Bolts 3/4" Each 8

GENERAL NOTES

Class X Concrete shall be used throughout.
Exposed edges shall be beveled 3/4".
For backfilling and embankment see Std. Specs.
All bars shall be lapped 2d diameters unless otherwise specified.
The top of the culvert, the backs of the sidewalls above the lower construction joint and backs of the wings above the tops of the footings shall be waterproofed in accordance with Art. 503.12 of the Std. Specs.
Nonmetallic water seal used in the wingwall joints shall extend from the top of the footing to within 6" of the top of the headwall.

DESIGNED: [] EXAMINED: 12-21-1970
CHECKED: [] PASSED: []
DRAWN: [] APPROVED: []

Added 3-3/4" Exp. Bolts. 12-21-70

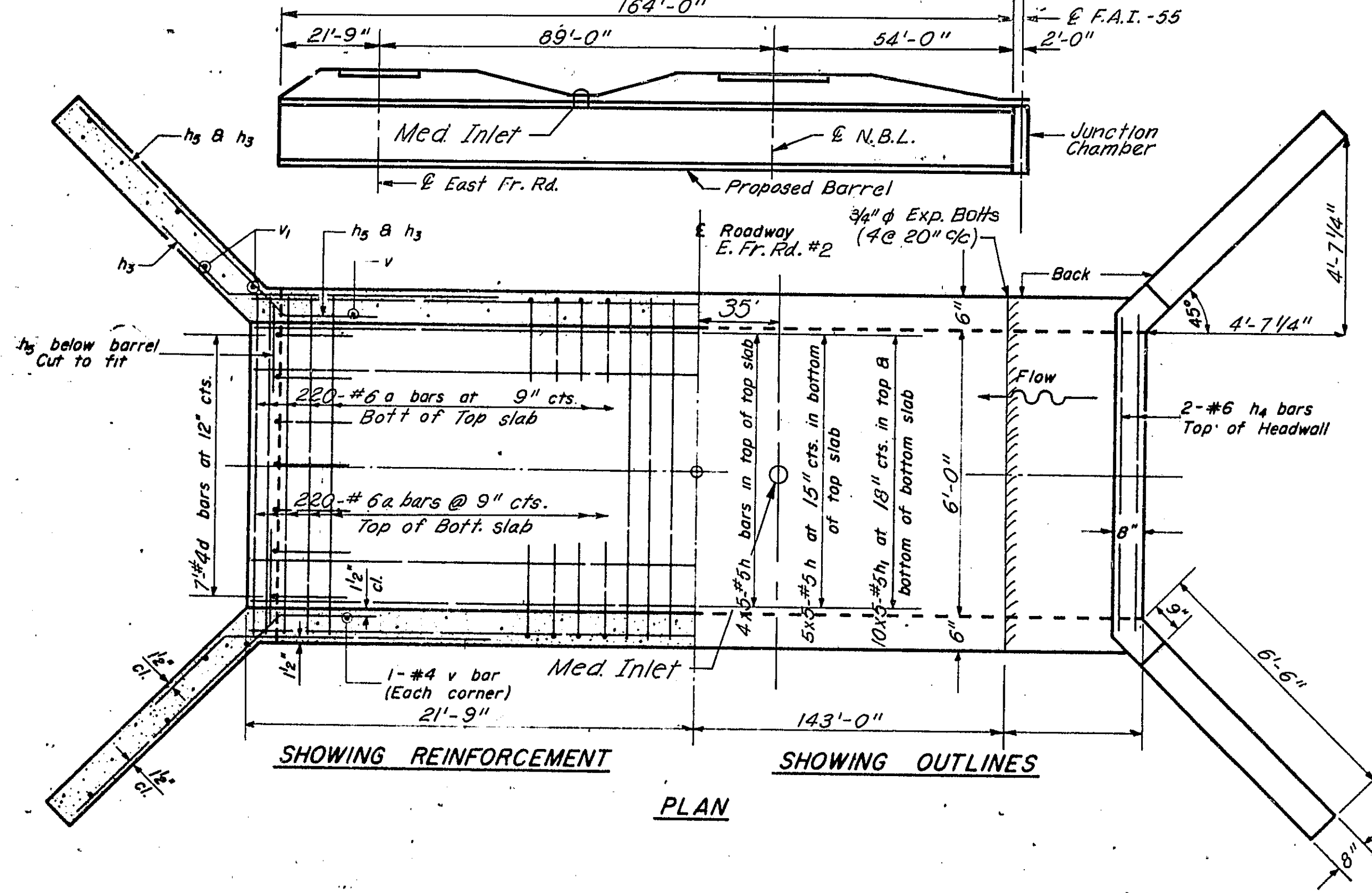
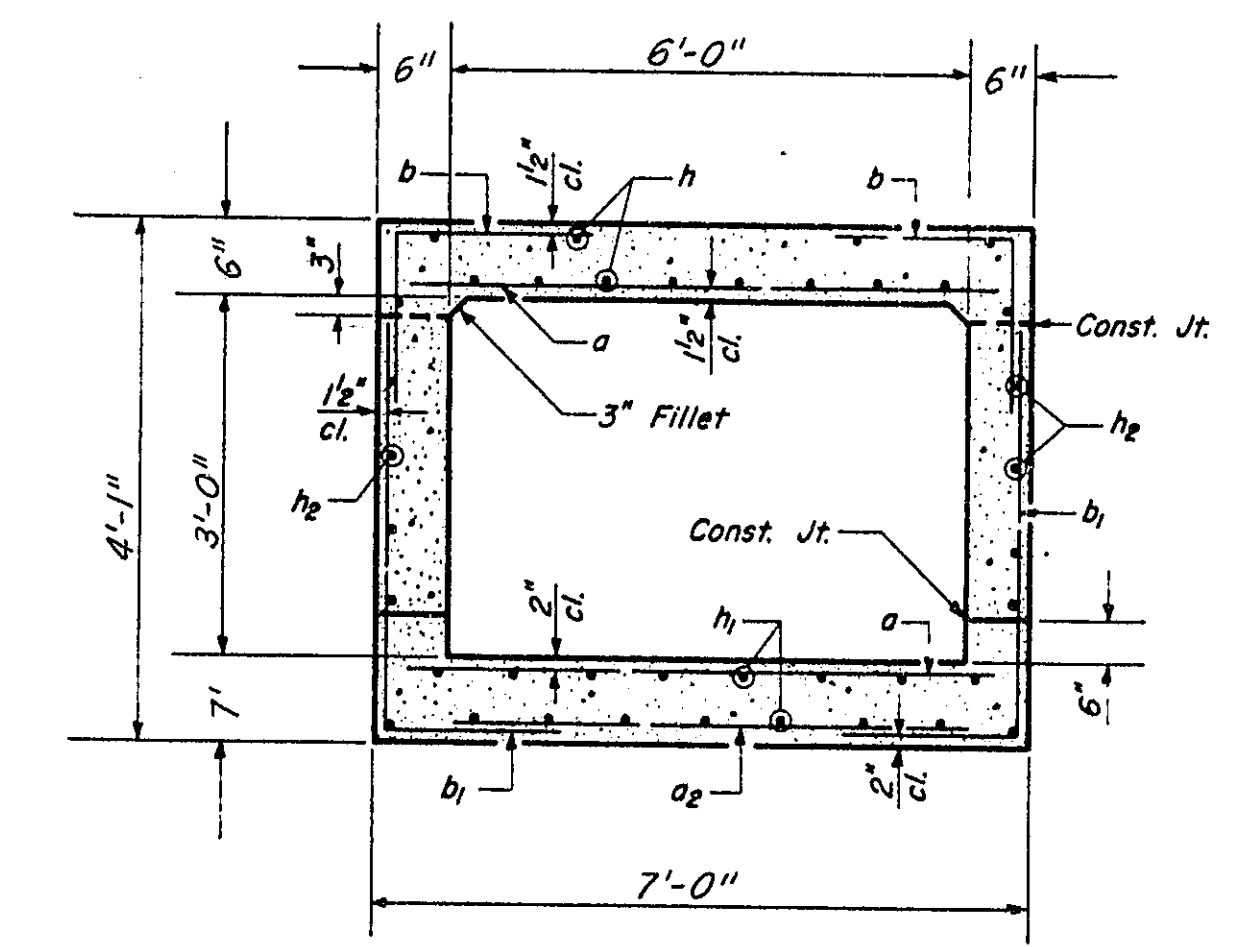
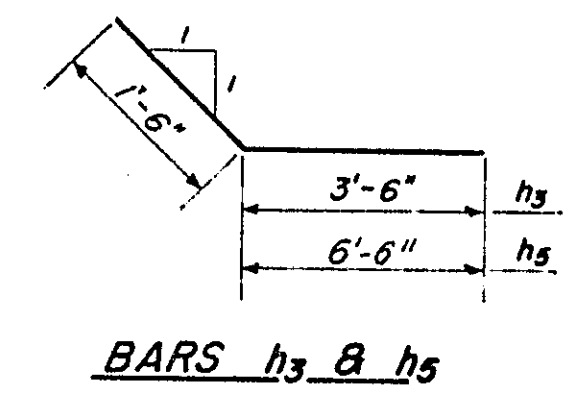
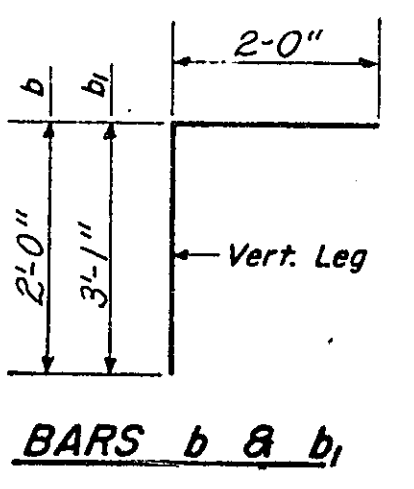
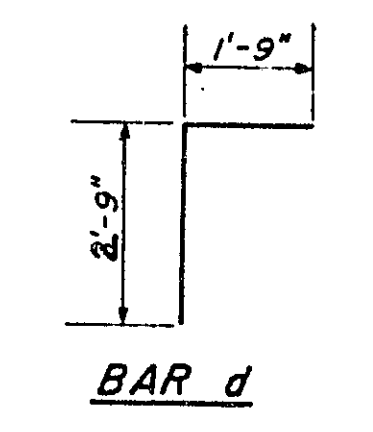
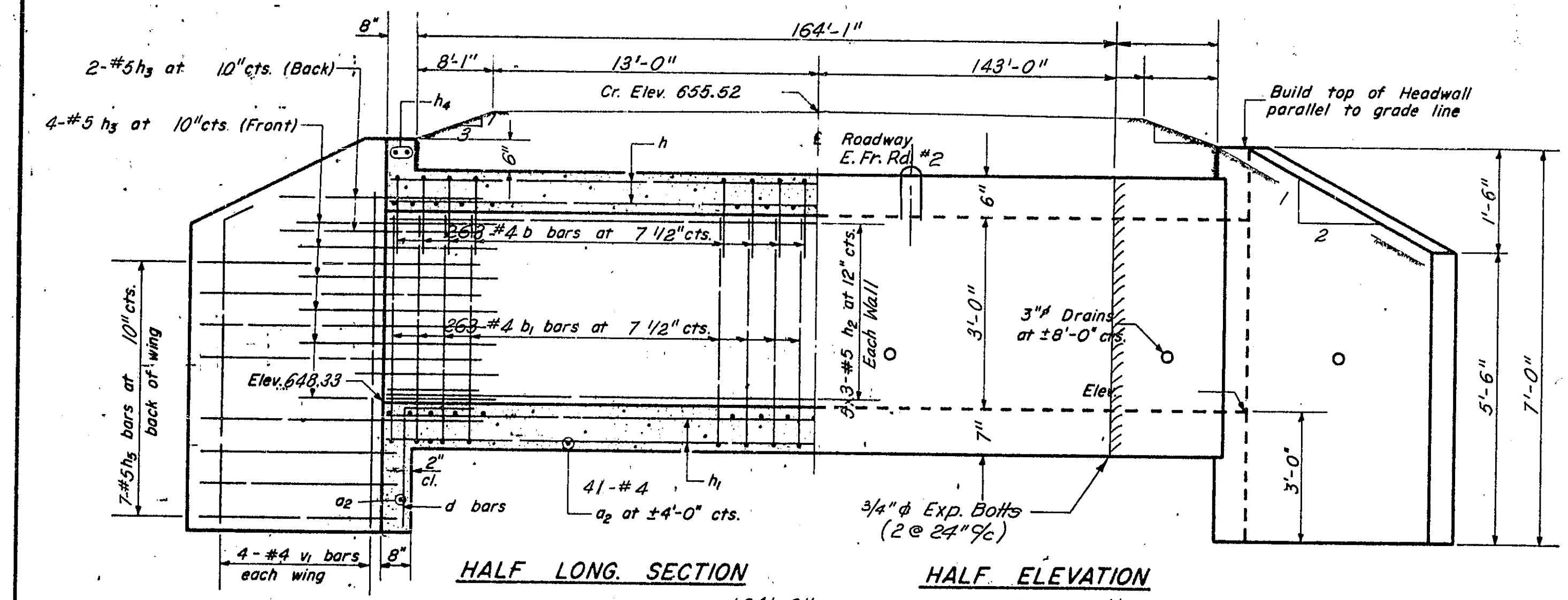


CULVERT EXTENSION
STATION 1037+55.7
FAI 55 SEC. 68-5
MONTGOMERY COUNTY

STANDARD 2035R

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

PROJECT NO.	SECTION	DATE	SHEET NO.
206		874	
SHEETS			



BILL OF MATERIAL

Bar	No.	Size	Length
a	40	#6	6'-8"
a2	42	#4	3'-9"
b	526	#4	4'-0"
b1	526	#4	5'-1"
d	7	#4	4'-6"
h	45	#5	34'-0"
h1	50	#5	34'-0"
h2	30	#5	34'-0"
h3	12	#5	5'-0"
h4	2	#6	6'-6"
h5	14	#5	8'-0"
v2	2	#4	3'-6"
v1	8	#4	6'-0"
Class X Concrete		Cu. Yds.	67.8
Reinforcement Bars		Lbs.	12,390
Expansion Bolts		Each	12

EXPANSION BOLTS SHALL CONSIST OF SELF-DRILLING EXPANSION SHIELDS AND 3/4" HOOKED BOLTS. HOOKED BOLTS SHALL EXTEND A MINIMUM OF 9" INTO NEW CONCRETE.

GENERAL NOTES

Class X Concrete shall be used throughout.
At least six feet of Barrel shall be poured monolithically with wingwalls.
Exposed edges shall be beveled 1/4".
For backfilling & embankments see Std. Spec.
The top of the culvert, the backs of the sidewalls above the lower construction joint and the backs of the wings shall be waterproofed in accordance with Art. 503.12 of the Std. Spec's.
All bars shall be lapped 24 diameters unless otherwise specified.

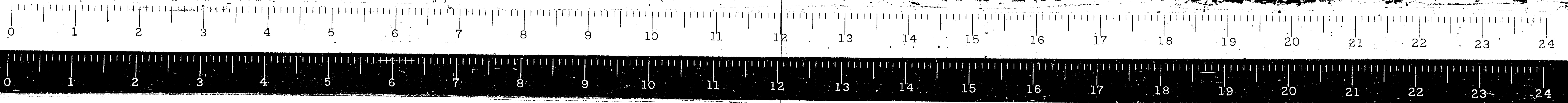
DESIGNED N.S. 3-20-70	EXAMINED 12-21-1970
CHECKED V.O. 4-3-70	PASSED
DRAWN S.D.D.	APPROVED
CHECKED	

f_s = 20,000 psi.
f_c = 1400 psi. Barrel
f_c = 1200 psi. Wings
v = 90 psi.
n = 10

LOADING HS20-44

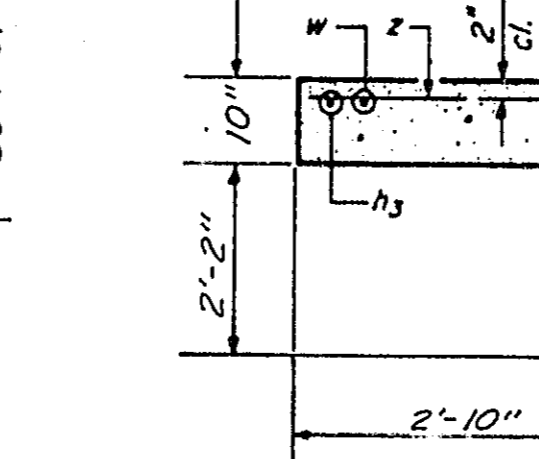
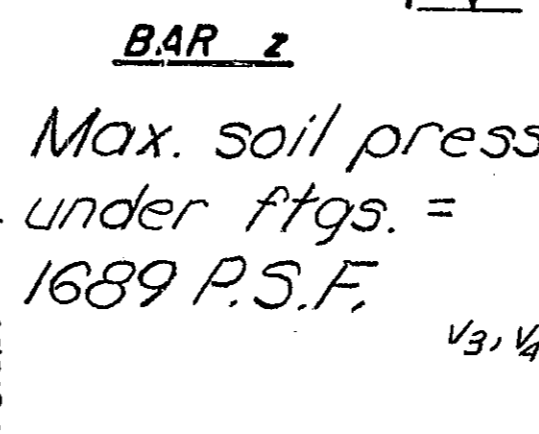
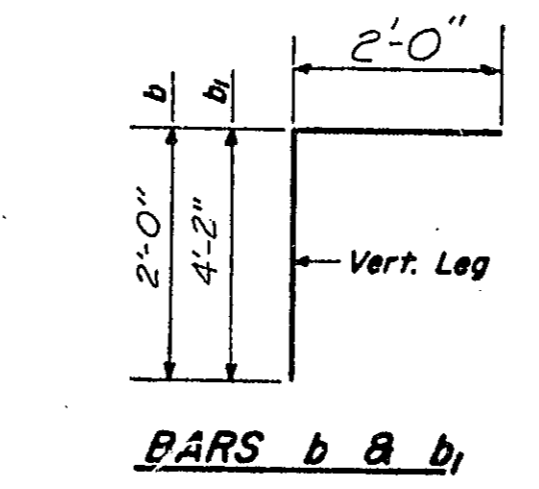
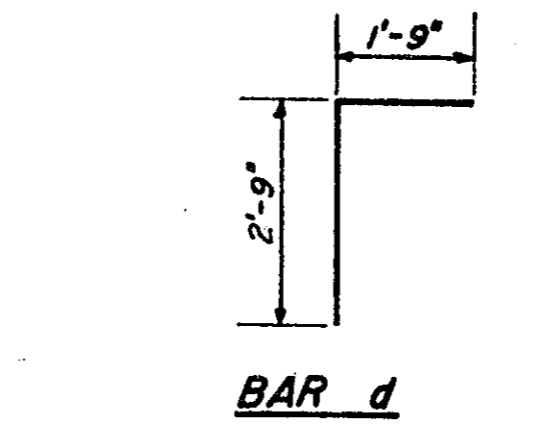
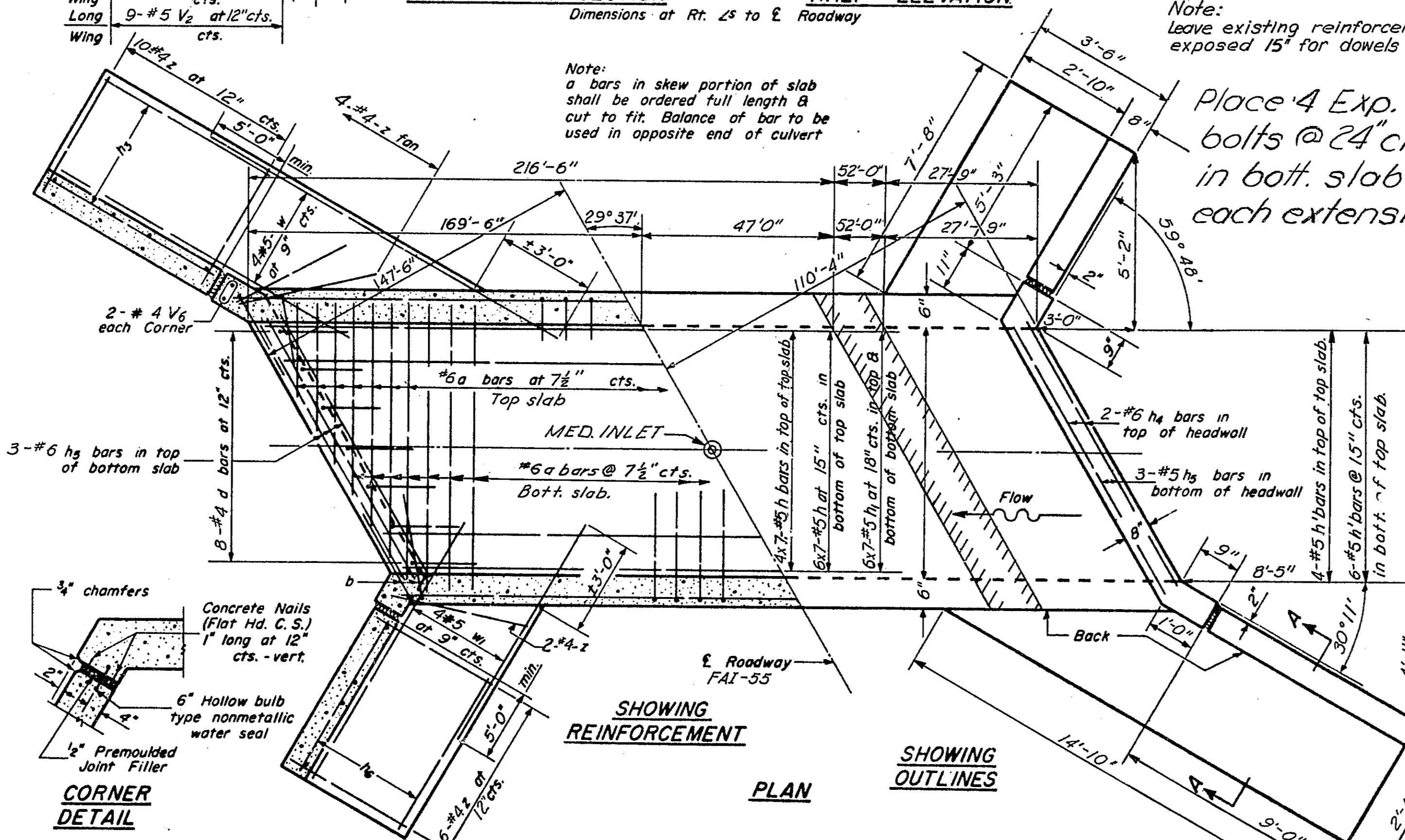
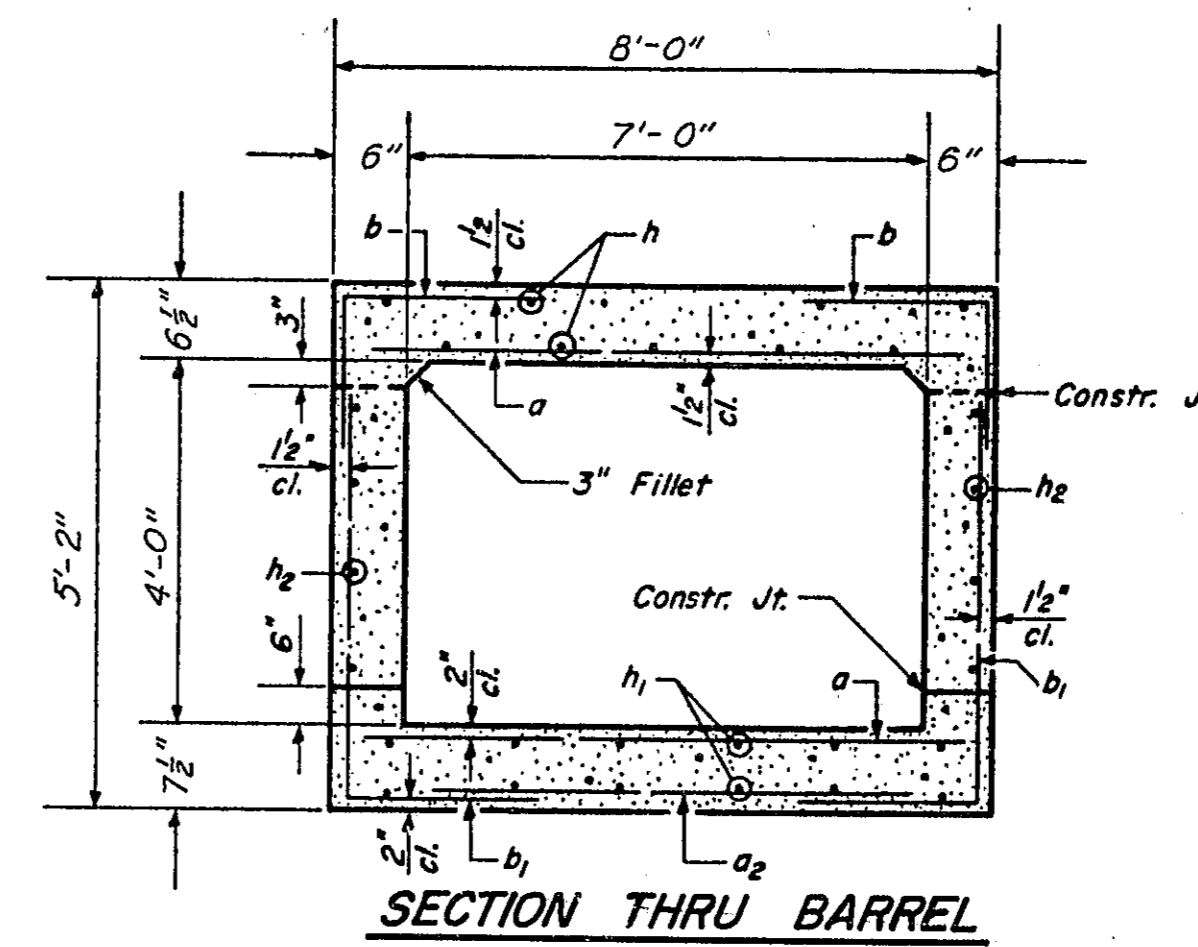
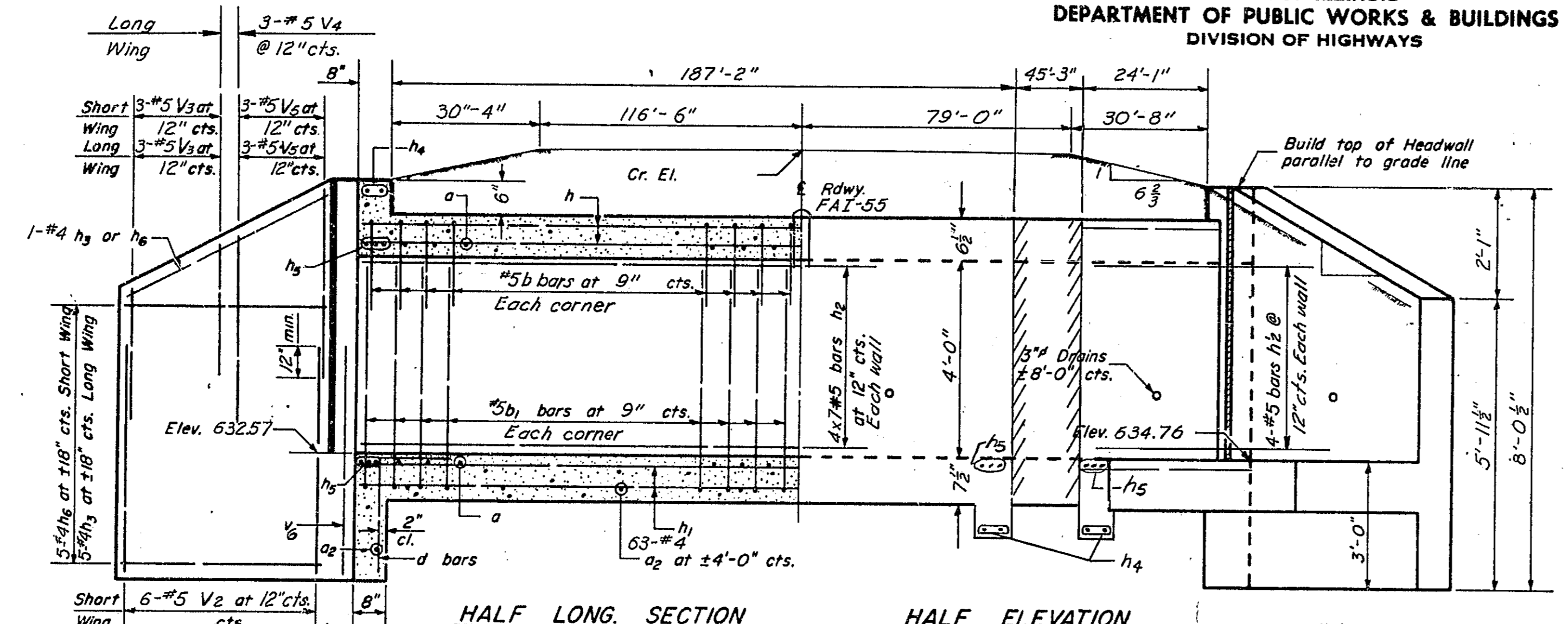
CULVERT EXTENSION
STATION 991+86.70
FAI-55 SEC. 68-5
MONTGOMERY COUNTY

STANDARD 2030R



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

SHEET NO.
206 B7B
SHEETS



Note:
Leave existing reinforcement exposed 15" for dowels

Place 4 Exp. bolts @ 24" cts. in bott. slab of each extension.

LOADING HS20-44
fs = 20,000 psi
fc = 1400 psi, barrel
fc = 1000 psi, wings
v = 90 psi, barrel
v = 75 psi, footing
n = 10

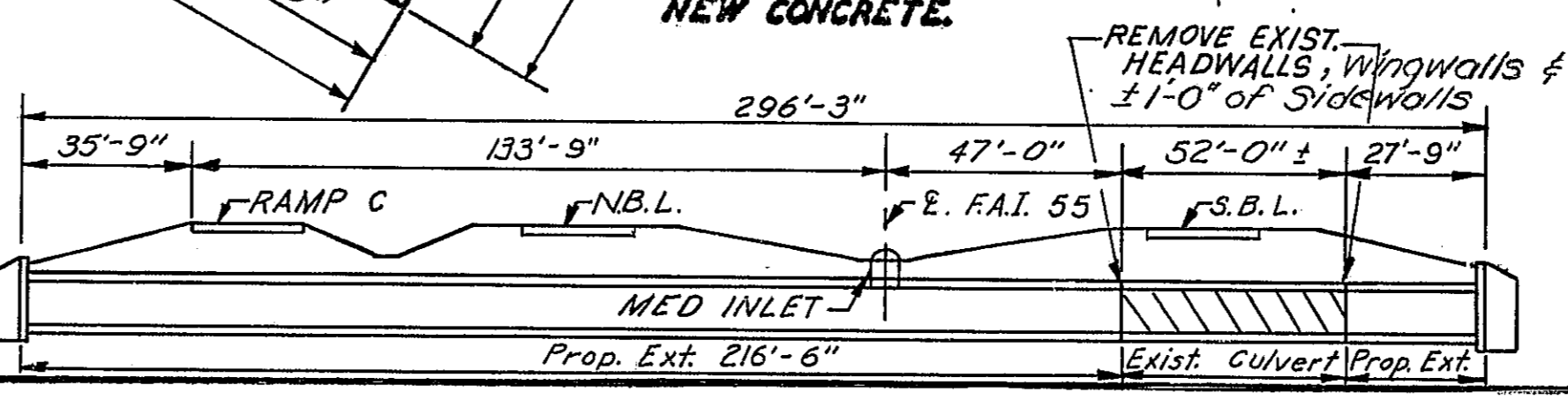
Max. soil press. under ftgs. = 1689 P.S.F.

BILL OF MATERIAL

Bar	No.	Size	Length
h'	10	#5	28'-9"
h1	12	#5	28'-9"
h2	8	#5	27'-6"
a	788	#6	7'-8"
a2	65	#4	4'-9"
b	660	#5	4'-0"
b1	656	#5	6'-2"
d	16	#4	4'-6"
h	70	#5	32'-3"
h1	84	#5	32'-3"
h2	56	#5	32'-0"
h3	16	#4	8'-8"
h4	8	#6	9'-2"
h5	18	#6	9'-2"
h6	16	#4	5'-0"
h7	30	#4	4'-6"
v3	12	#4	2'-9"
v4	6	#4	3'-6"
v5	12	#4	4'-3"
v6	8	#4	7'-0"
w	8	#5	13'-0"
w1	8	#5	10'-0"
z	44	#4	5'-11"
Class X Concrete			Cu. Yds. 132.3
Reinforcement Bars			Lbs. 25,307
Exp. Bolts 3/4" #			Each 8

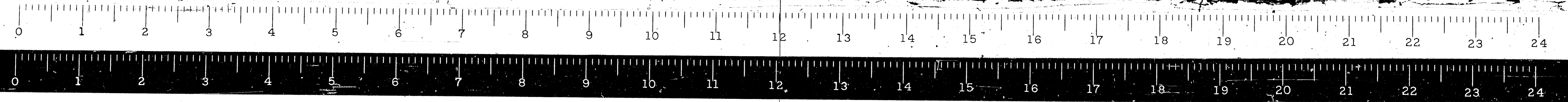
DESIGNED	12-21-1970
CHECKED	
DRAWN	
CHECKED	

GENERAL NOTES
Class X Concrete shall be used throughout. Exposed edges shall be beveled 3/4". For backfilling and embankment see Std. Spec's. All bars shall be lapped 2-3 diameters unless otherwise specified. The top of the culvert, the backs of the sidewalls above the lower construction joint and backs of the wings above the tops of the footings shall be waterproofed in accordance with Art. 503.12 of the Std. Spec's. Nonmetallic water seal used in the wingwall joints shall extend from the top of the footing to within 6" of the top of the headwall.



CULVERT EXTENSION
STATION 1037 + 55.7
FAI 55 SEC. 68-5
MONTGOMERY COUNTY

STANDARD 2035R



B.M. No. 4 on S.W. corner of existing bridge, 145' Di. El. 620.77
 Existing Structures:

Build. or	Year	Superstructure	Substructure	Existing Traffic
S.B.I. Ete. 126 Dec 11/18	1930	R.C. Deck Girder	Closed Abut.	South Bound
S.B.I. Ete. 126 Dec 11/18	1930	R.C. Deck Girder	Closed Abut.	North Bound

The existing North and South bound structures shall be removed by the Br. Contractor after North and South bound traffic has been routed over their proposed lanes. Existing S. Bd. Struct. 32'0" Bk. to Bk. Abutments. 43'4" o. to o. Existing N. Bd. Struct. 29' Bk. to Bk. Abutments. 46'0" o. to o.

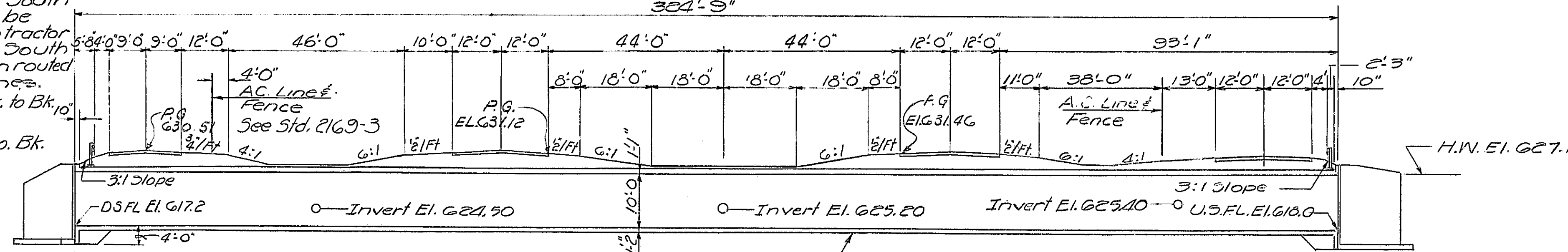
STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET
P.A. I. 55	68-58	Montgomery	206	BPA

FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT

GENERAL NOTES

All reinforcement bars shall be lapped 24 diameters unless otherwise shown.
 The following surfaces of the culvert shall be waterproofed; the top of the top slab, the backs of the exterior walls above the lower construction joint and the backs of the wings above the tops of the footings.
 Nonmetallic water seal used in the wingwall joints shall extend from the top of the footing to within 6" of the top of the headwall.
 All exposed edges shall be beveled 3".
 Class X Concrete shall be used throughout.
 For backfilling & embankment see std. specs.
 Special Guardrail Post cost included in Steel Plate Beam Guardrail.
 Fence Post Anchorage cost included in the unit price bid for fencing.

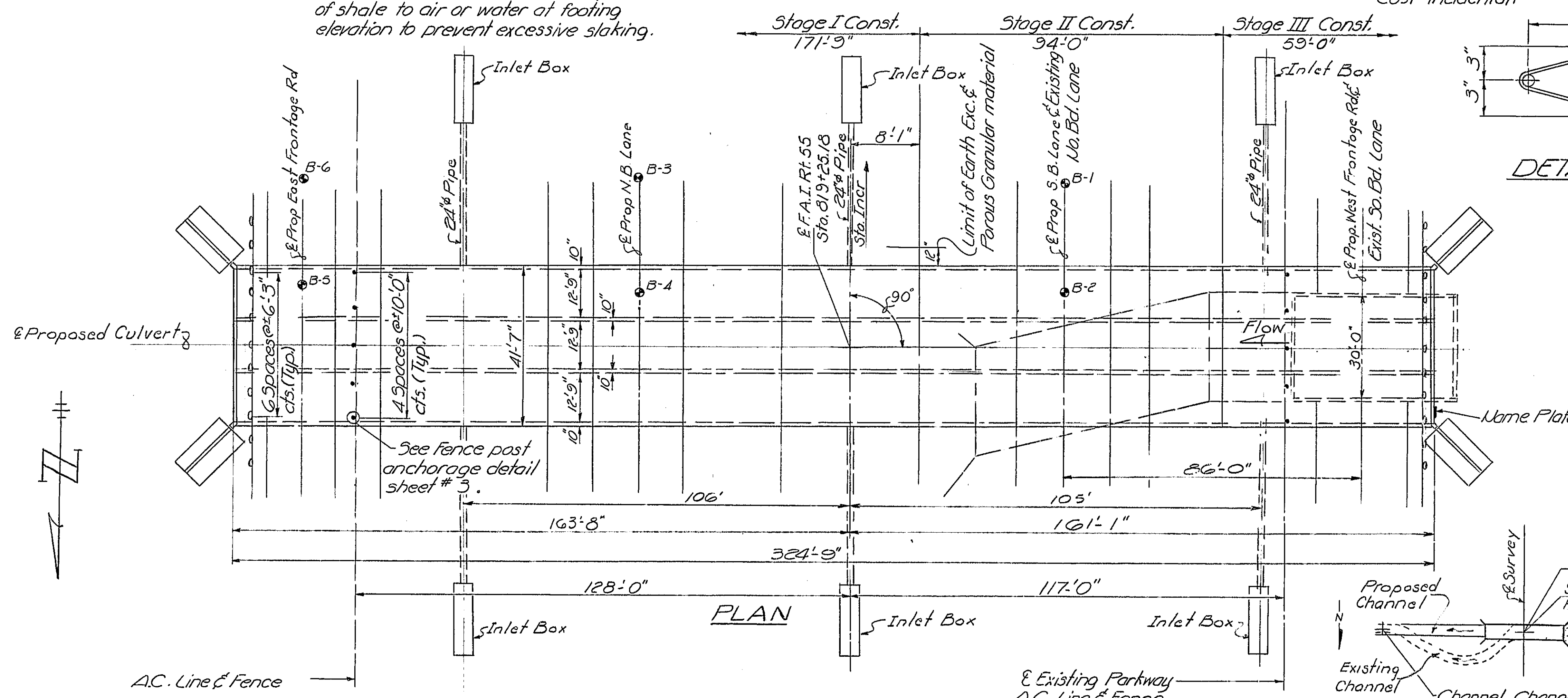


Note: Invert elevations are identical on both sides of culvert.
 Contractor shall avoid undue exposure of shale to air or water at footing elevation to prevent excessive slaking.

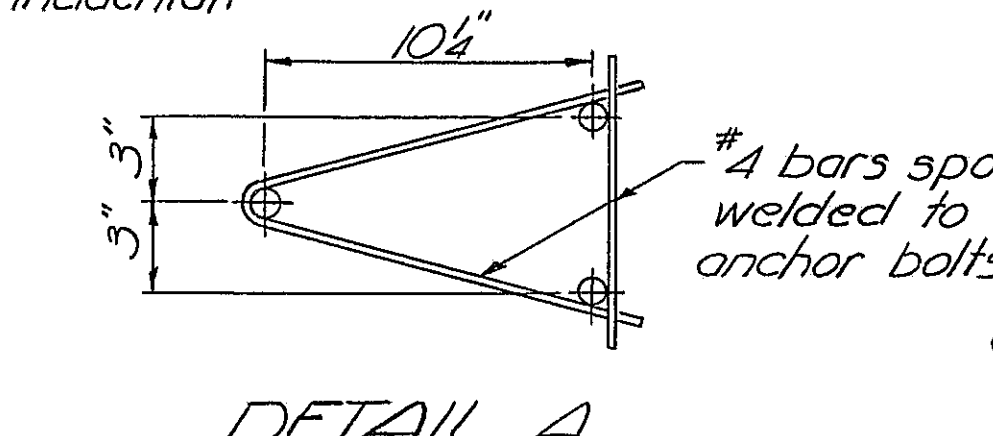
A 6" Granular Blanket to be used under the barrel as a construction pad.

Note: Special Guardrail Posts and Fence post anchorages to be provided in culvert top slab. Cost incidental.

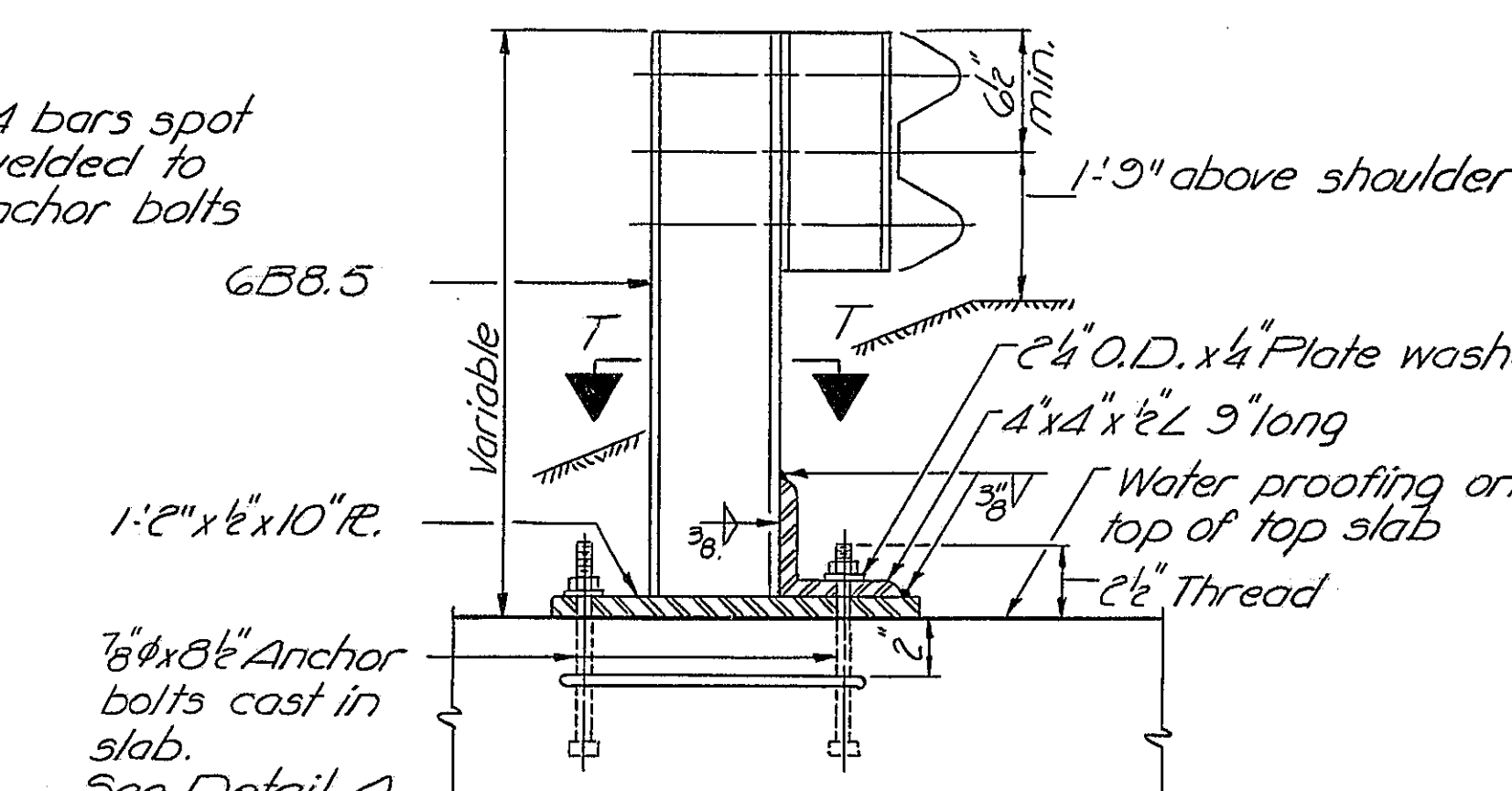
ELEVATION



PLAN

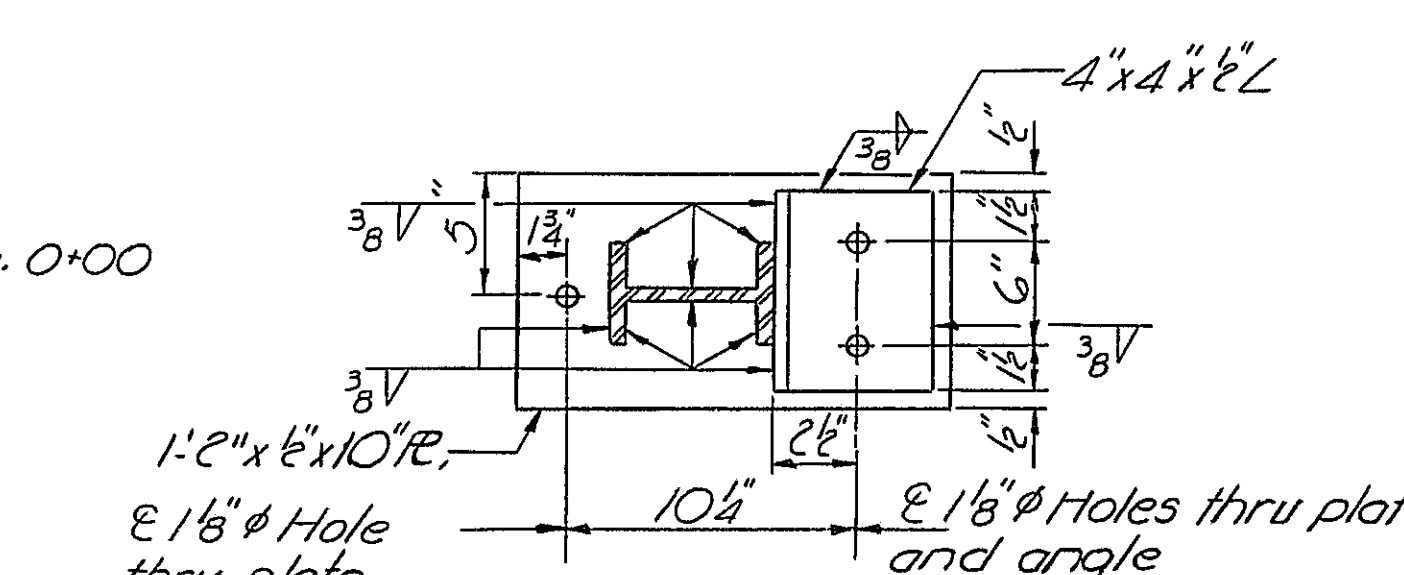


DETAIL A

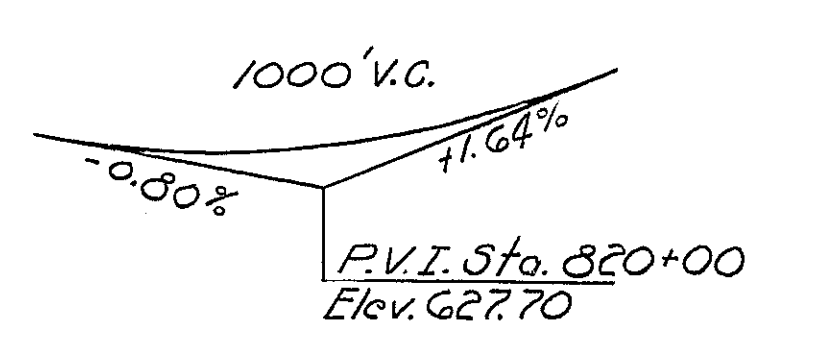


DETAIL OF SPECIAL GUARDRAIL POST

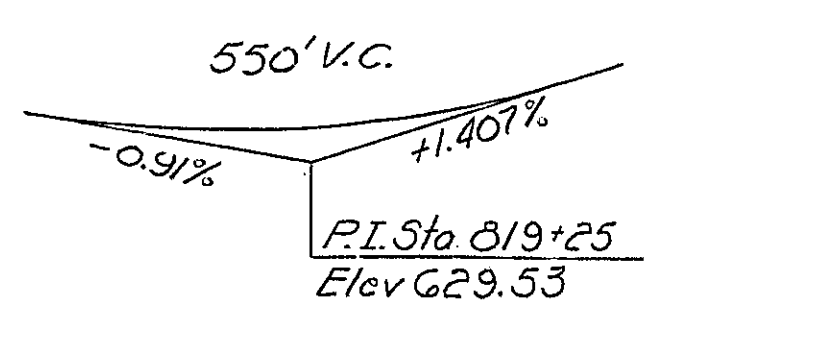
Posts shall be spaced at 6'3" cts. See Std. 2230 for details of rail element.



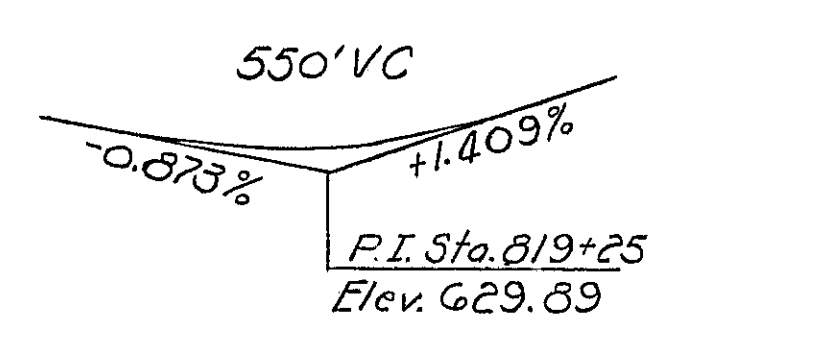
SEC. T-T



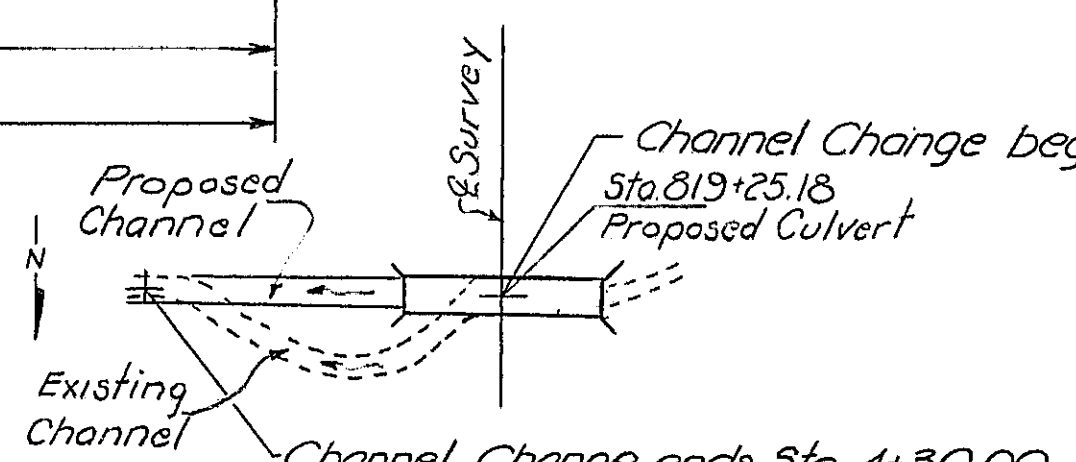
PROFILE OF EAST FRONTAGE RD
 (Grade applies of E. Frontage Road)



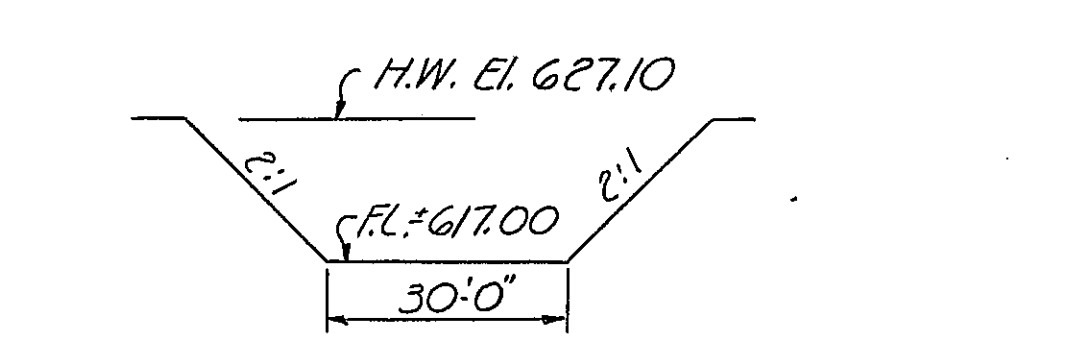
PROFILE OF FAI RT 55 (NBL)
 (Grade applies of median edge)



PROFILE OF FAI RT 55 (SBL)
 (Grade applies of median edge)



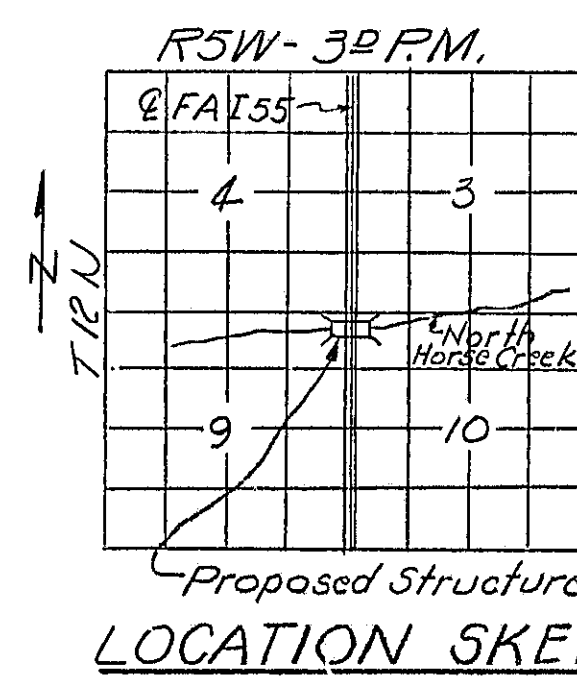
CHANNEL RELOCATION



PROP. CHANNEL SECTION

TOTAL BILL OF MATERIAL

Item	Unit	Quantity
Channel Excavation	Cu. Yds.	3030
Removal of Exst. Structures	Each	2
Class X Concrete	Cu. Yds.	1588.3
Reinforcement Bars	Lbs.	370420
Name Plates	Each	1
Porous Granular Embank.	Cu. Yds.	260



LOCATION SKETCH

GENERAL PLAN & ELEVATION OVER NORTH HORSE CREEK

PROJ. I-55-2(43)75
 FAI RT 55 SEC. 68-58
 MONTGOMERY CO.
 STA. 819+25.18

DESIGNED	J. M. Patel
CHECKED	R. G. K.
DRAWN	
CHECKED	R. G. K.

EXAMINED
 PASSED
 APPROVED

STATION 819+25.18
 BUILT BY
 STATE OF ILLINOIS
 FAI RT 55 SEC. 68-58
 FAI PROJ. I-55-2(43)
 LOADING HS 20-44 & ALI

NAME PLATE
 See Std. 2113

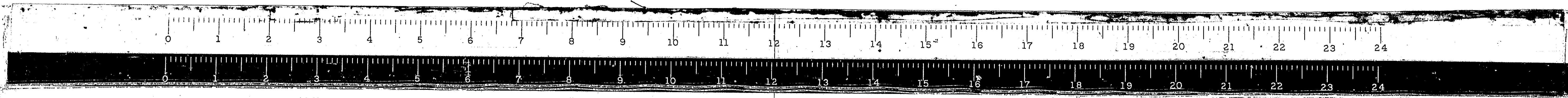
WATERWAY INFORMATION

Drainage Area 505 1/2 Acres
 Character Level, clay & cultivated
 Reg'd Opening 36.0 Sq. Ft.
 Present Opening 215 Sq. Ft.
 Proposed Opening 382.5 Sq. Ft.

Design Frequency discharge Q=2350 c.f.s
 Crested head = 0.15 Ft.

DESIGN STRESSES

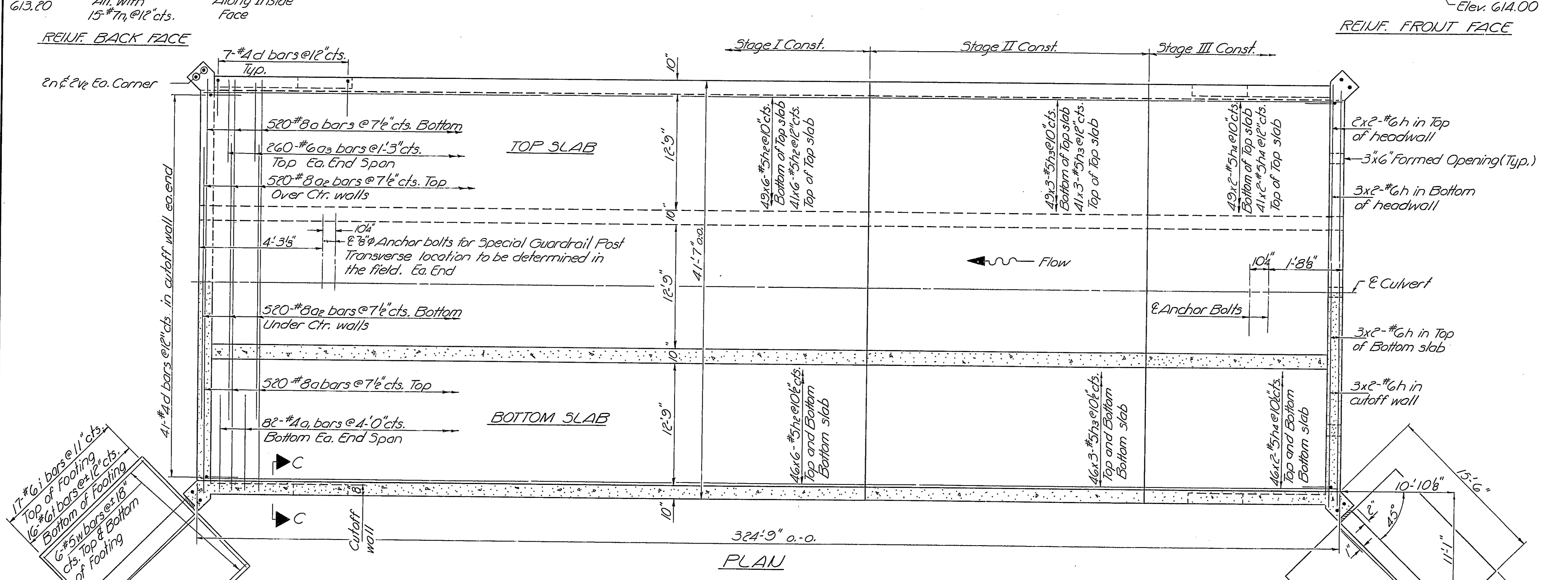
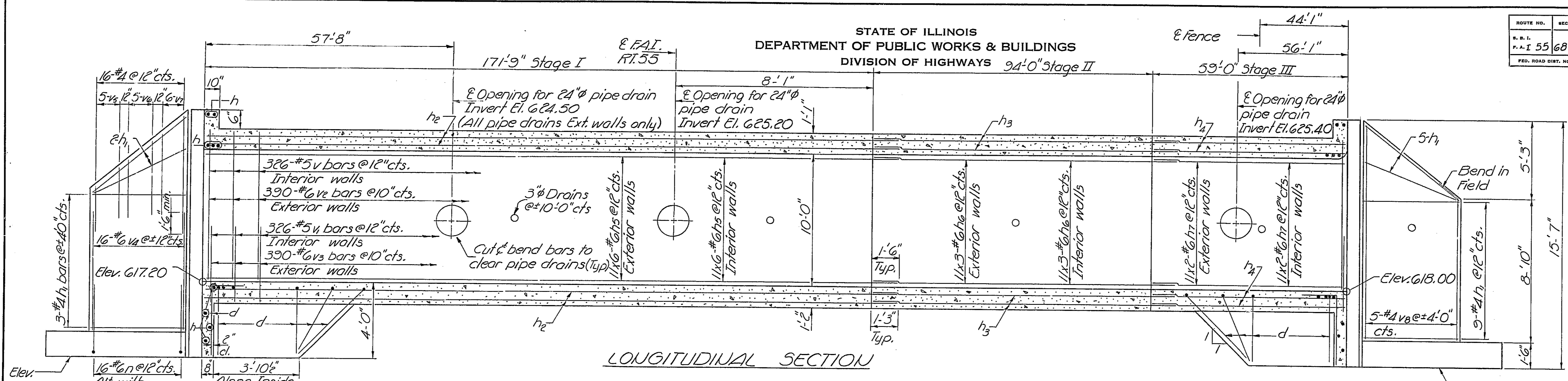
Fc = 1400 psi (Barrel)
 Fs = 20000 psi (Reinf.)
 Fc = 10000 psi (Wings)
 Vc = 90 psi (Barrel)
 Vc = 75 psi (Figs)
 n = 10
 LOADING HS 20-44 & ALI



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
P.A.I. 55	68-5B	Montgomery	206	888
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

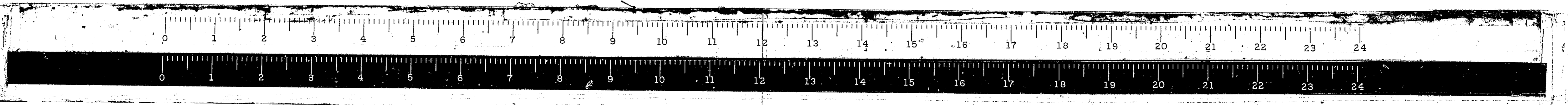
SHEET NO. 2
4 SHEETS



DESIGNED	J. M. Patel	EXAMINED	[Signature]
CHECKED	Rao. G.K.	PASSED	[Signature]
DRAWN	J.D.	APPROVED	[Signature]
CHECKED	Rao. G.K.		

Notes:
Bars indicated thus 20x3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
Min. bar laps = 24 dia.
See Sheet #3 for Sec. CC.

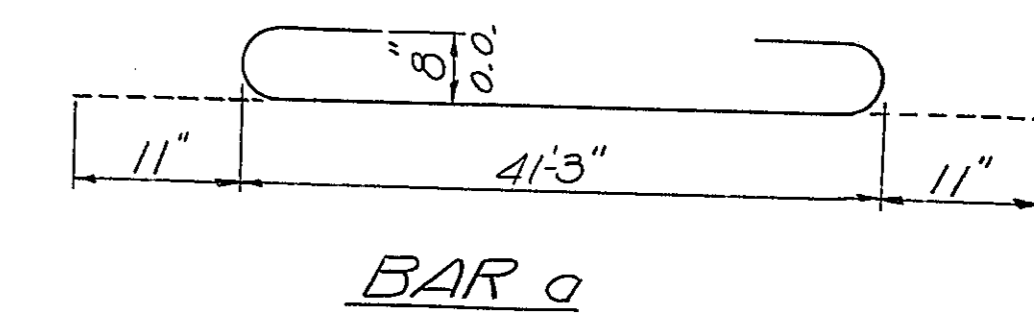
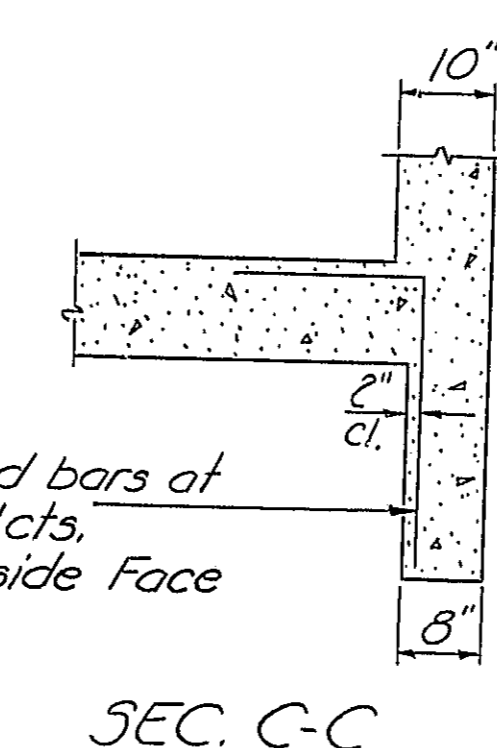
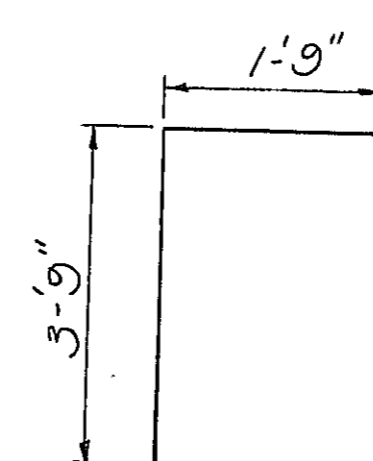
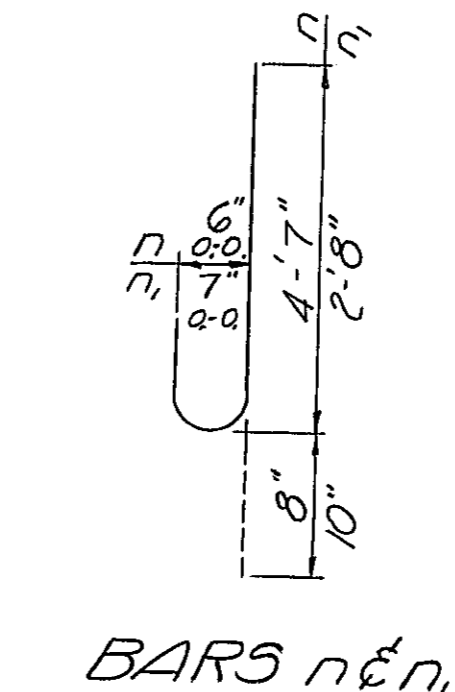
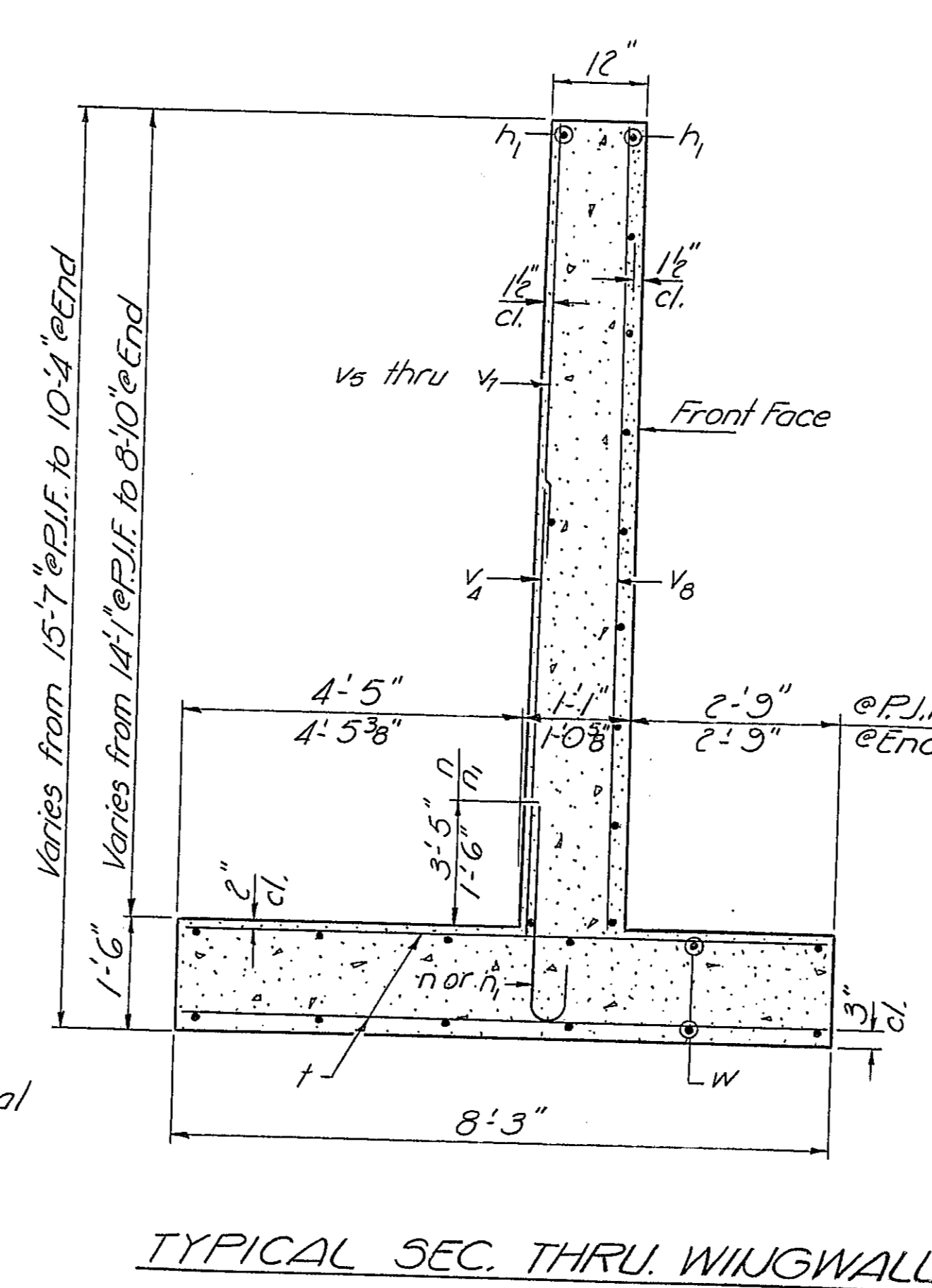
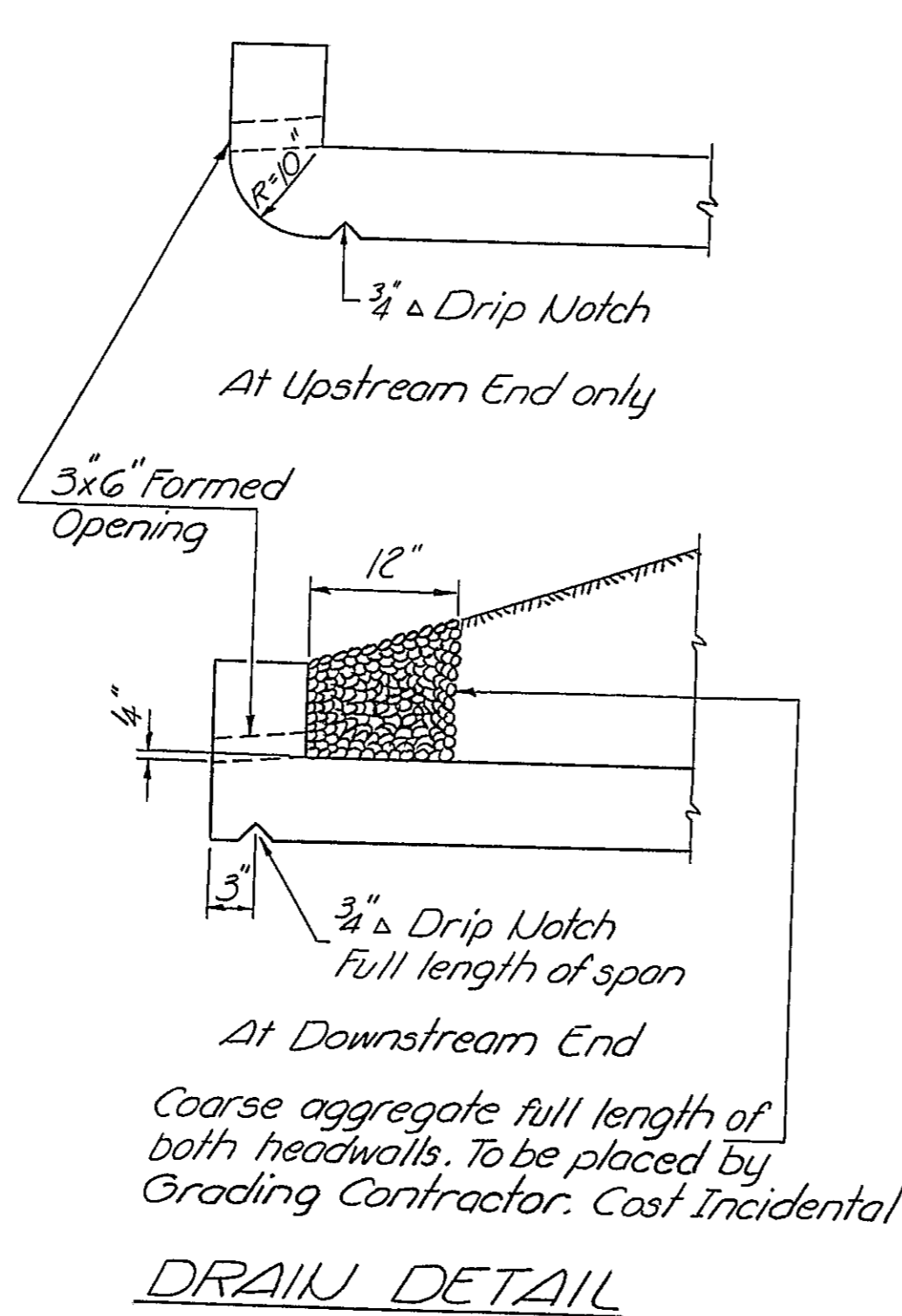
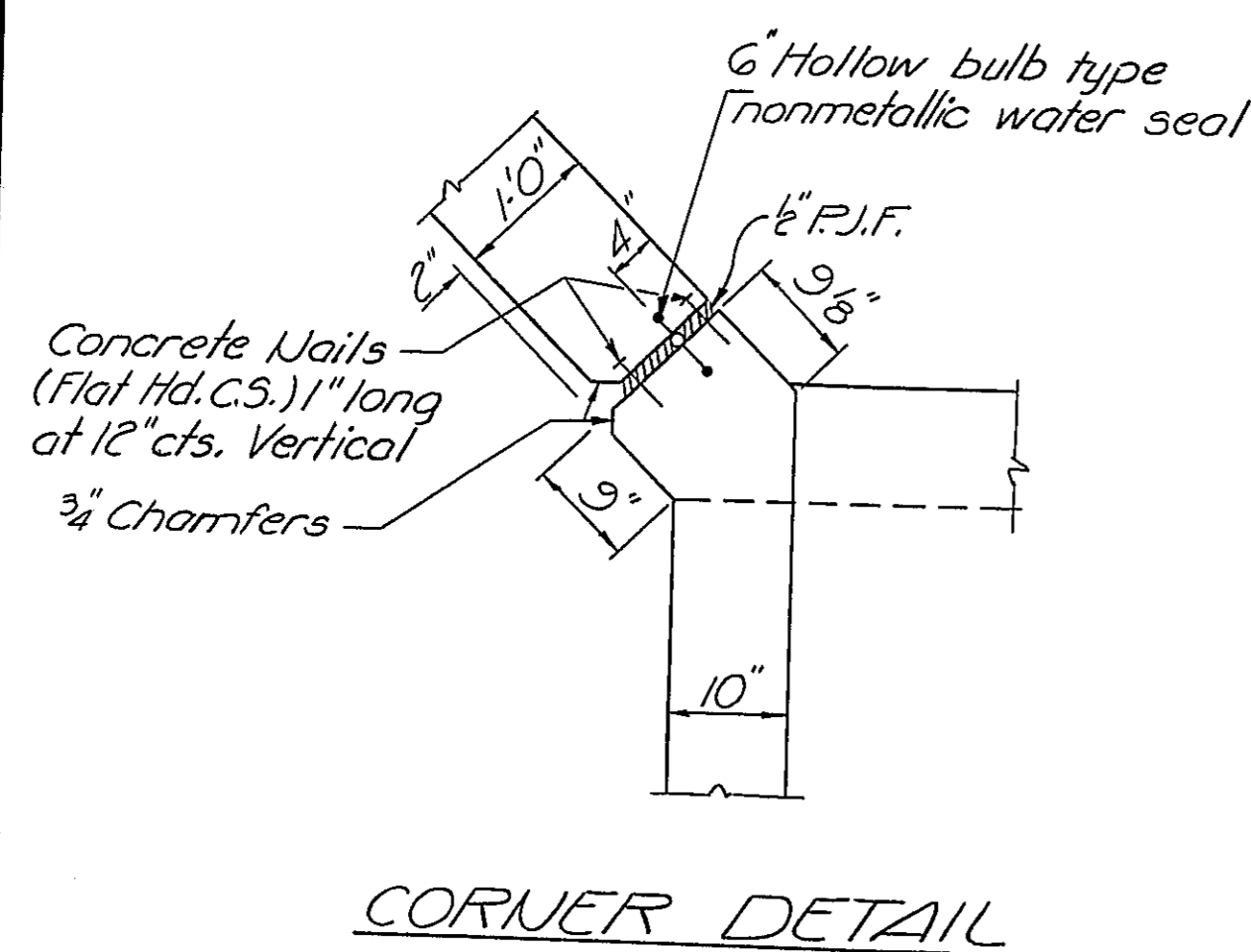
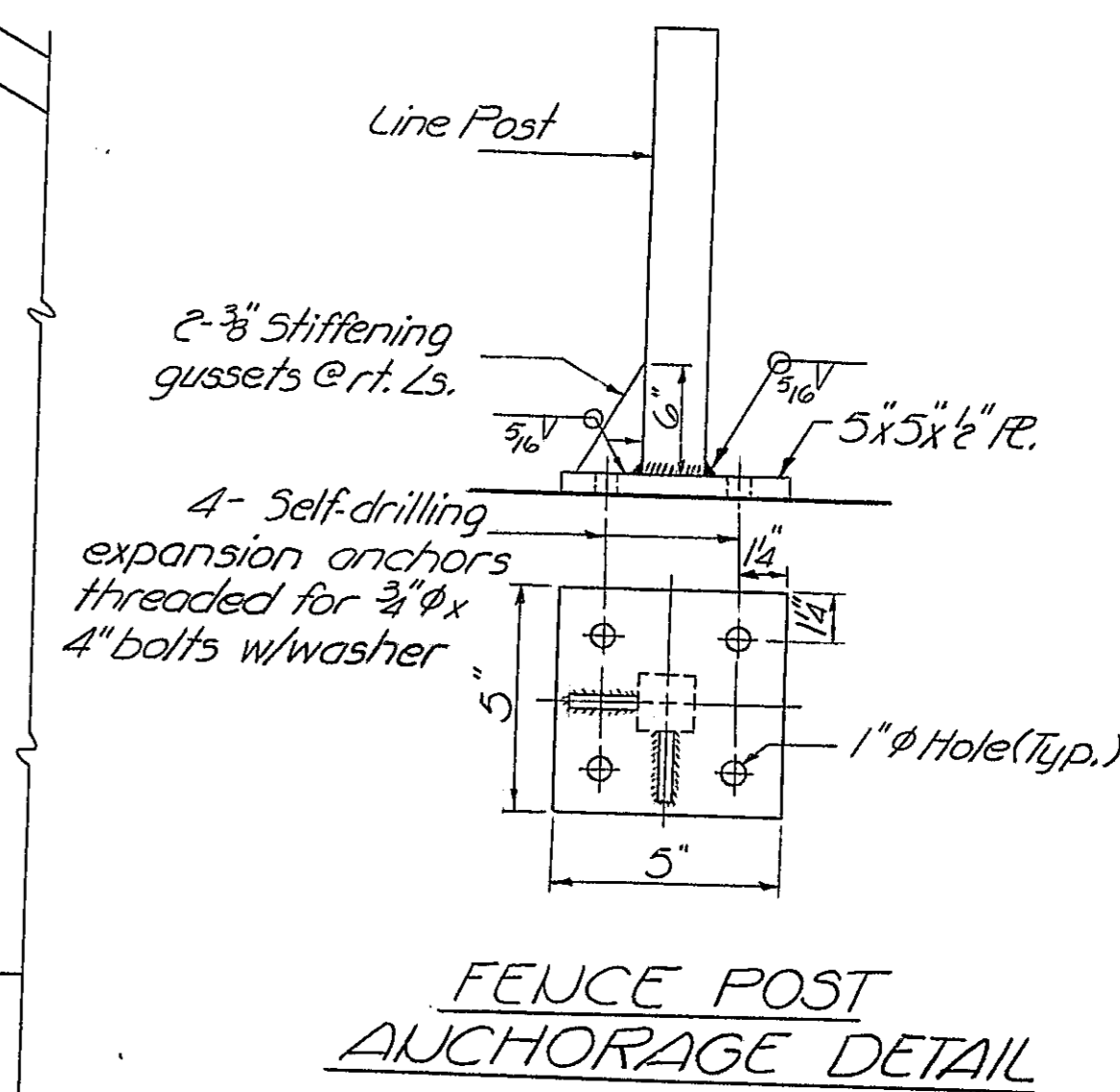
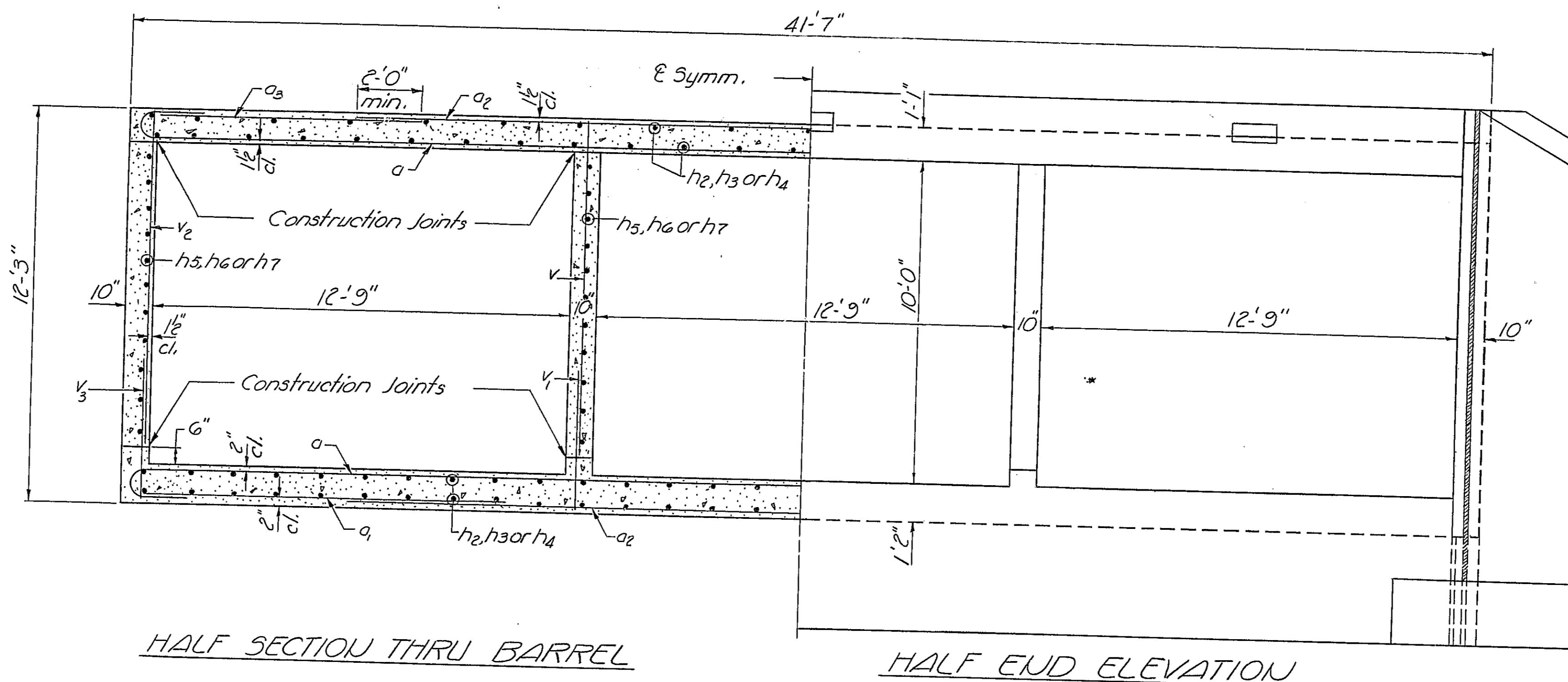
CULVERT DETAILS
F.A.I. RT. 55 SEC. 68-5B
MONTGOMERY COUNTY
STA. 819+25.18



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-5B	Montgomery	206	88C	4 SHEETS
FED. ROAD DIST. NO. 7	PLANNO.	FED. AID PROJECT		

Location	Stage I	Stage II	Stage III
	Bar No.	Bar No.	Bar No.
Top of Top Slab	h2	h3	h4
Bottom of Top Slab	h2	h3	h4
Top of Bottom Slab	h2	h3	h4
Bottom of Bottom Slab	h2	h3	h4
Each Outside Wall	h5	h6	h7
Each Center Wall	h5	h6	h7



BILL OF MATERIAL

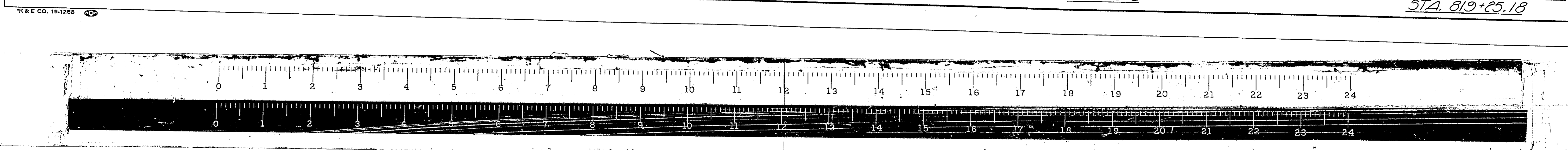
Bar	No.	Size	Length	Bar	No.	Size	Length
a	1040	#8	43'-1"	v	652	#5	10'-5"
a	164	#4	9'-0"	v	652	#5	2'-10"
ae	1040	#8	26'-9"	v	788	#6	10'-5"
as	520	#6	9'-0"	vs	780	#6	3'-1"
d	110	#4	5'-6"	va	64	#6	6'-0"
h	44	#6	21'-6"	vs	20	#4	5'-0"
h	76	#4	14'-6"	v	24	#4	9'-0"
h	1028	#5	30'-0"	v	20	#4	13'-9"
h3	546	#5	32'-6"				
h4	364	#5	30'-3"				
h5	264	#6	30'-0"				
h6	132	#6	33'-0"	n	72	#6	5'-3"
h7	88	#6	30'-3"	n	60	#7	3'-6"
t	132	#6	8'-0"	w	48	#5	14'-6"
		Class X Concrete		Cu. Yds.		1588.3	
		Reinforcement Bars		Lbs.		320420	

CULVERT DETAILS
F.A.I. RT. 55 SEC. 68-5B
MONTGOMERY COUNTY
STA. 819+25.18

DESIGNED J. M. Patel
CHECKED Rao. G. K.
DRAWN J.D.
CHECKED Rao. G. K.

EXAMINED [Signature]
PASSED [Signature]
APPROVED [Signature]

Comp. Max. Soil Pressure = 3020 Lbs. per Sq. Ft.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
P.A. 55	68-58	Montgomery	206	880
FED. ROAD DIST. NO. 7			BLDG. NO.	FED. AID PROJECT

SHEET NO. 4
4 SHEETS

Surface Water El. _____
Groundwater El. at Completion _____
After 24 Hours 623.8

Boring No.	Station	Offset	Elevation	N	Ch 1/4 L	W (%)
1	819+66	CL BRM.	630.8	0		
Ground Surface						
GRAY AND BROWN MOTTLED SILTY CLAY (VERY SLT) MOIST (VERY SLTFF)						
			629.8	8	2.2	20
DARK GRAY AND RED BROWN SILTY CLAY LOAM MOIST (MEDIUM)						
			629.3	5	0.9	27
GRAY & LT. GRAY MOTTLED SILTY CLAY LOAM MOIST (MEDIUM)						
			621.3	4	0.5	27
GRAY GREEN AND DARK GRAY MOTTLED CLAY MOIST (SLTFF)						
			621.2	9	2.1	23
BROWN AND GRAY MARBLED SHALE MOIST						
			616.8	6	1.0	19
GRAY SHALE FAIRLY DRY						
			612.8	50		18
THIN LIMY LAYERS (SOFT)						
			100	11		16
SEATED ON LIMESTONE BORING COMPLETED						
			603.8	30		21
			100	11		18
			603.8	100		18
			603.8	100		18

Surface Water El. _____
Groundwater El. at Completion _____
After 24 Hours _____

Boring No.	Station	Offset	Elevation	N	Ch 1/4 L	W (%)
2	819+21	CL BR NY	631.2	0		
Ground Surface						
DARK GRAY AND YELLOW BROWN MOTTLED SILTY CLAY (VERY SLT) MOIST (SLTFF)						
			629.2	9	1.6	23
DARK GRAY AND GRAY GREEN MOTTLED SILTY CLAY MOIST (MEDIUM)						
			621.2	5	0.7	26
DARK GRAY SILTY CLAY LOAM MOIST (SOFT)						
			621.2	4	0.4	29
GRAY GREEN AND YELLOW BROWN MOTTLED CLAY LOAM MOIST (VERY SLTFF)						
			614.7	15	2.8	17
BROWN AND GRAY SHALE (WEATHERED) MOIST						
			612.2	33		18
GRAY SHALE FAIRLY DRY						
			603.7	51		17
THIN LAYERS LIMESTONE						
			100	11		10
GRAY LIMESTONE - TOP 1" SHALE, THIN BEDDED, SOFT. BOTTOM, FOSSILIFEROUS, HARD.						
			601.7	30		15
GRAY SHALE FAIRLY DRY						
			600.2	100		14
BORING COMPLETED						

Surface Water El. _____
Groundwater El. at Completion _____
After 24 Hours _____

Boring No.	Station	Offset	Elevation	N	Ch 1/4 L	W (%)
3	819+25	2" 1/2 CL BR ML	626.7	0		
Ground Surface						
BLACK AND DARK BROWN MOTTLED SILTY CLAY LOAM MOIST (SLTFF)						
			626.7	5	1.8	10
GRAY AND YELLOW BROWN MOTTLED LOAM MOIST (MEDIUM)						
			617.7	3	NR	-
GRAY AND YELLOW BROWN MOTTLED CLAY LOAM MOIST (SOFT)						
			614.7	4	0.4	18
GREENISH BROWN TO GRAY SHALE (MOIST)						
			612.2	23		22
(FAIRLY DRY)						
			605.7	26		25
THIN LAYERS OF LIMESTONE (FAIRLY DRY)						
			605.7	33		18
SEATED ON LIMESTONE BORING COMPLETED						
			605.7	100		16

Surface Water El. _____
Groundwater El. at Completion _____
After 24 Hours _____

Boring No.	Station	Offset	Elevation	N	Ch 1/4 L	W (%)
4	819+36	CL BR ML	627.1	0		
Ground Surface						
BLACK AND DARK BROWN MOTTLED SILTY CLAY LOAM MOIST (SLTFF)						
			627.1	7	1.8	18
GRAY AND YELLOW BROWN MOTTLED CLAY LOAM MOIST (MEDIUM)						
			620.1	4	0.7	27
GRAY GREEN TO GRAY SHALE MOIST						
			616.1	8	0.7	27
FAIRLY DRY						
			605.1	22	2.3	22
DARK GRAY TO LIGHT GRAY LIMESTONE TOP 8" - HARD, SHALEY, MIDDLE 10" FOSSILIFEROUS, HARD, BOTTOM SHALEY						
			601.6	49	2.2	16
GRAY SHALE, MICACEOUS HARD						
			601.6	73	4.2	14
BORING COMPLETED						
			601.6	100		15

Surface Water El. _____
Groundwater El. at Completion _____
After 24 Hours _____

Boring No.	Station	Offset	Elevation	N	Ch 1/4 L	W (%)
5	819+36	CL S. FR. RD.	625.7	0		
Ground Surface						
BLACK AND DARK BROWN MOTTLED SILTY CLAY LOAM MOIST (SLTFF)						
			625.7	7	1.8	19
YELLOW BROWN AND GRAY MOTTLED CLAY LOAM MOIST (MEDIUM)						
			617.2	5	0.9	18
GRAY SHALE (FAIRLY DRY)						
			606.1	66		33
LIMESTONE FRAGMENTS						
			604.7	40		13
SEATED ON LIMESTONE BORING COMPLETED						
			604.7	100		14

Surface Water El. _____
Groundwater El. at Completion _____
After 24 Hours _____

Boring No.	Station	Offset	Elevation	N	Ch 1/4 L	W (%)
6	819+66	CL S. FR. RD.	627.1	0		
Ground Surface						
BLACK AND BROWN MOTTLED SILTY CLAY LOAM MOIST (VERY SLTFF)						
			623.1	8	2.2	18
GRAY AND YELLOW BROWN MOTTLED SILTY CLAY LOAM MOIST (SOFT)						
			621.1	7	1.8	20
YELLOW BROWN AND GRAY SHALE (WEATHERED) MOIST (SOFT)						
			615.1	12	2.2	23
GRAY SHALE FAIRLY DRY						
			615.1	25	2.7	21
GRAY SHALE FAIRLY DRY						
			606.1	50		13
THIN LAYERS OF LIMESTONE						
			100	11		12
DARK GRAY TO LIGHT GRAY LIMESTONE SHALEY, FINE GRAINED - TOP 9" FROM 9" TO BOTTOM - COARSE, FOSSILIFEROUS,						
			603.6	64		17
GRAY SHALE HARD DRY						
			602.1	100		16
BORING COMPLETED						

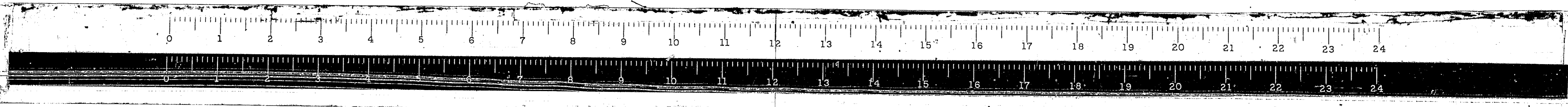
DESIGNED J.M. Patel
CHECKED Rao.G.K.
DRAWN Bev Robinson
CHECKED Rao.G.K.

EXAMINED *[Signature]*
PASSED *[Signature]*
APPROVED *[Signature]*

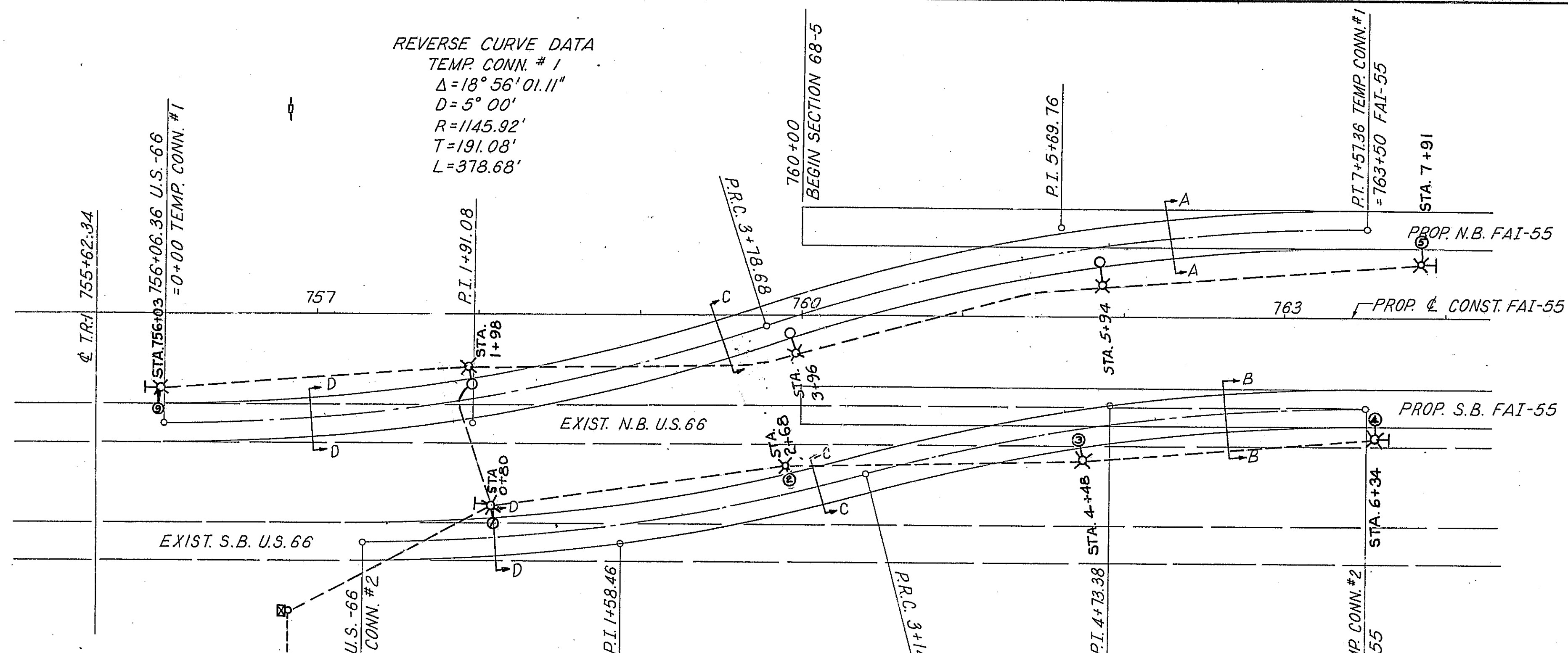
N-Standard Penetration Test - Blows per foot to drive 2" O.D. Split Spoon Sampler 12" with 140 # hammer falling 30"
Qu-Unconfined Compressive Strength - 1/sf
w-Water Content - percentage of oven dry weight - %
Type failure
B-Bulge Failure
S-Shear Failure
E-Estimated Value
P-Penetrometer

BORING DATA
F.A.I. RT 55 SEC. 68-5B
MONTGOMERY COUNTY
STATION 819+23.18

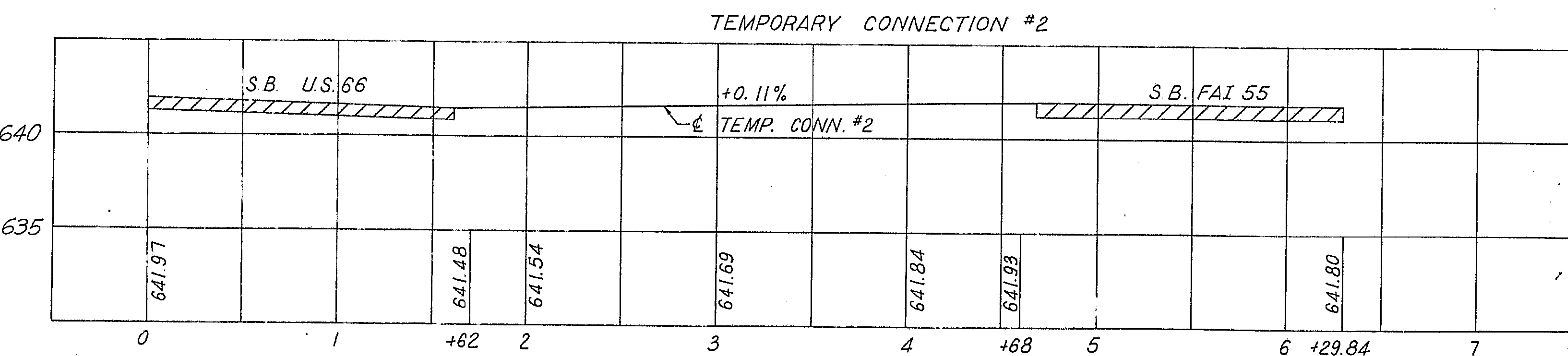
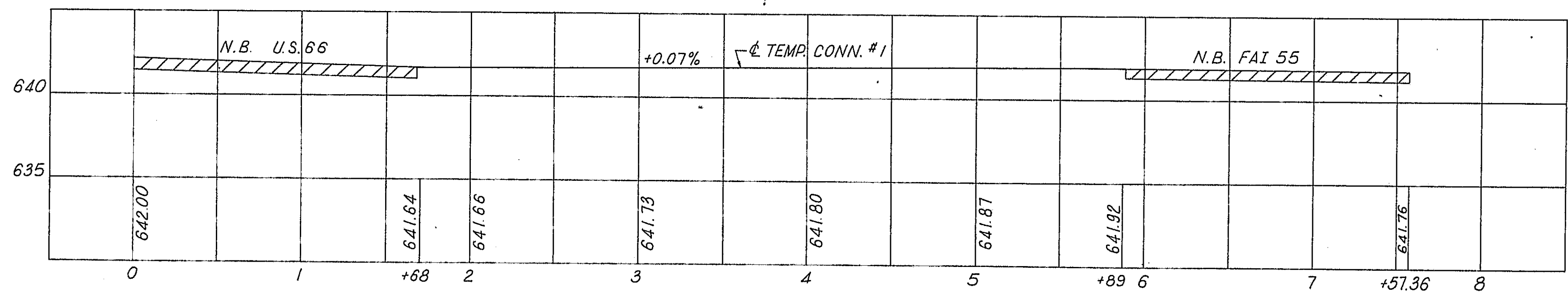
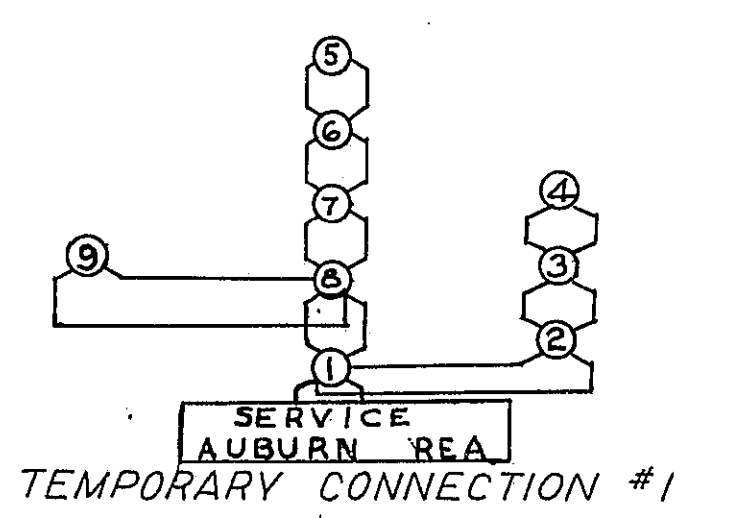
F.A.I. RT 55 SEC. 68-5B MONTGOMERY COUNTY



ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	*	MONT.	206	89
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



SCHEMATIC CIRCUIT DIAGRAM



LIGHTING LEGEND

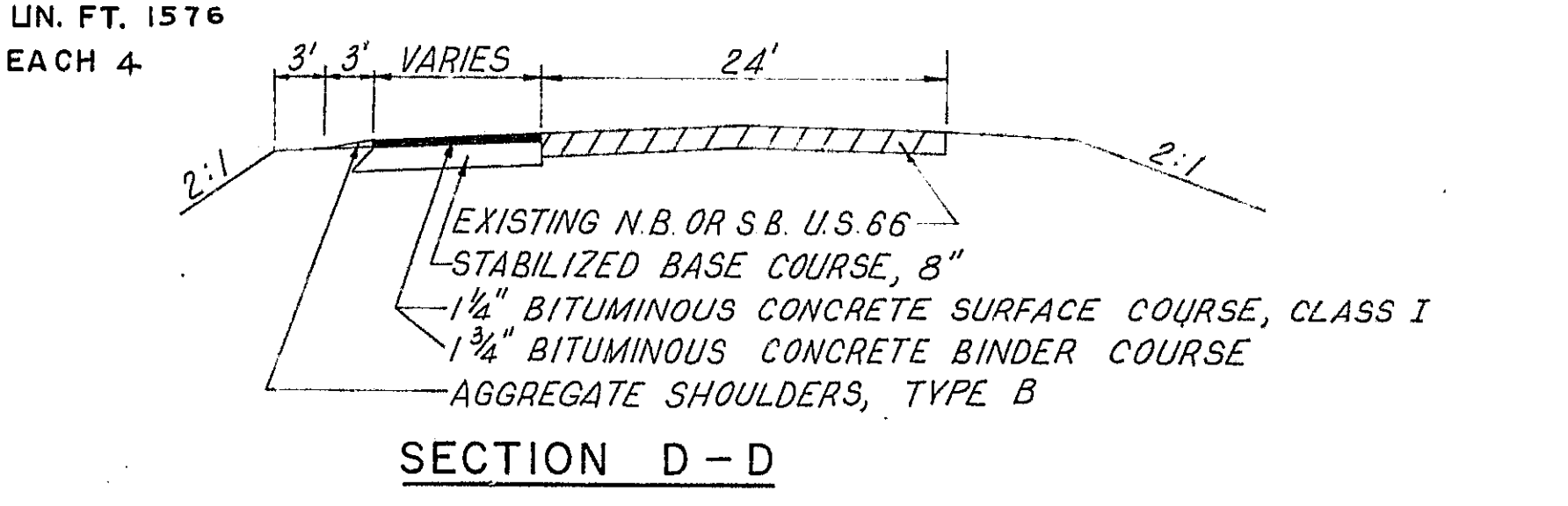
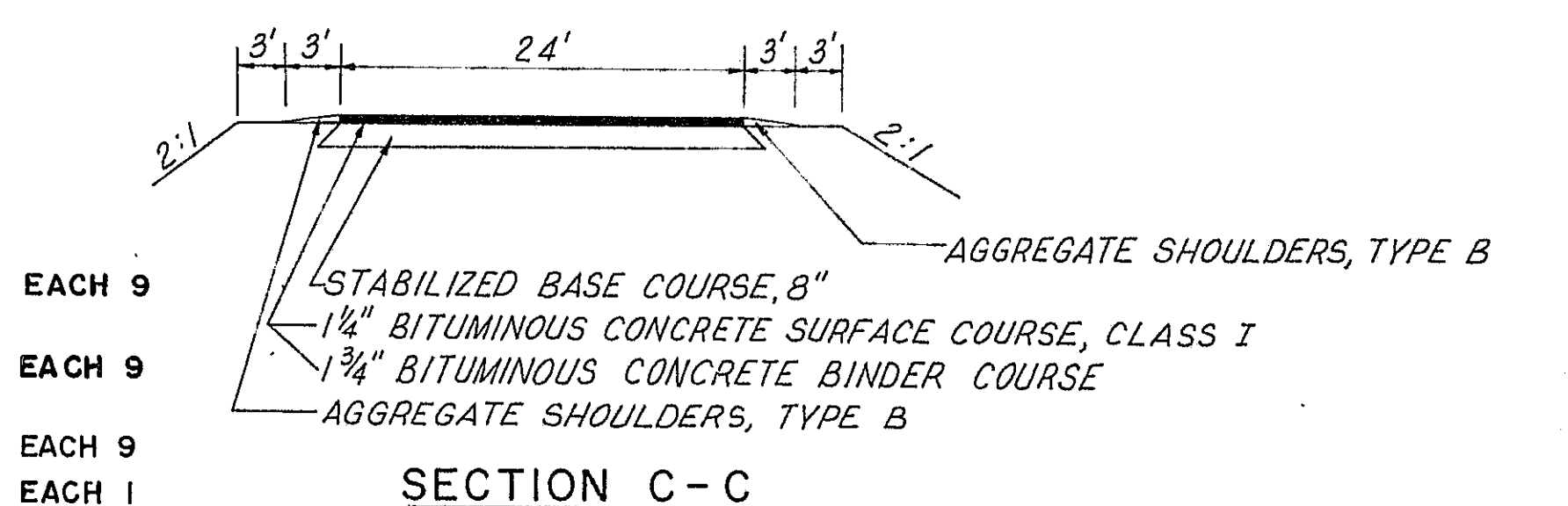
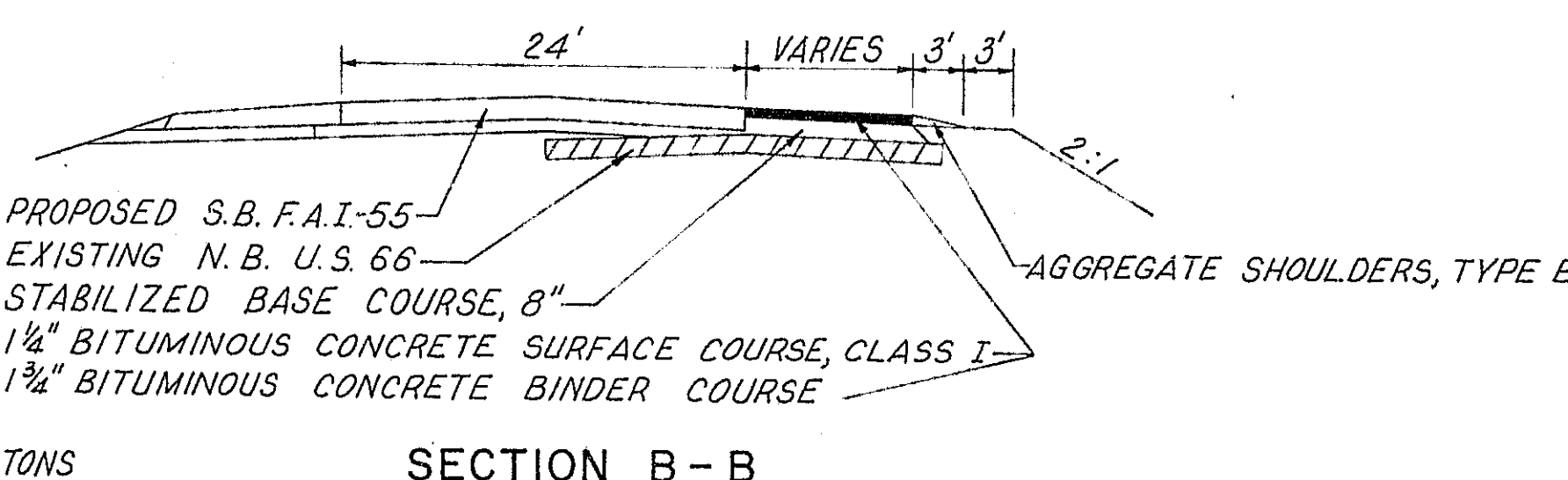
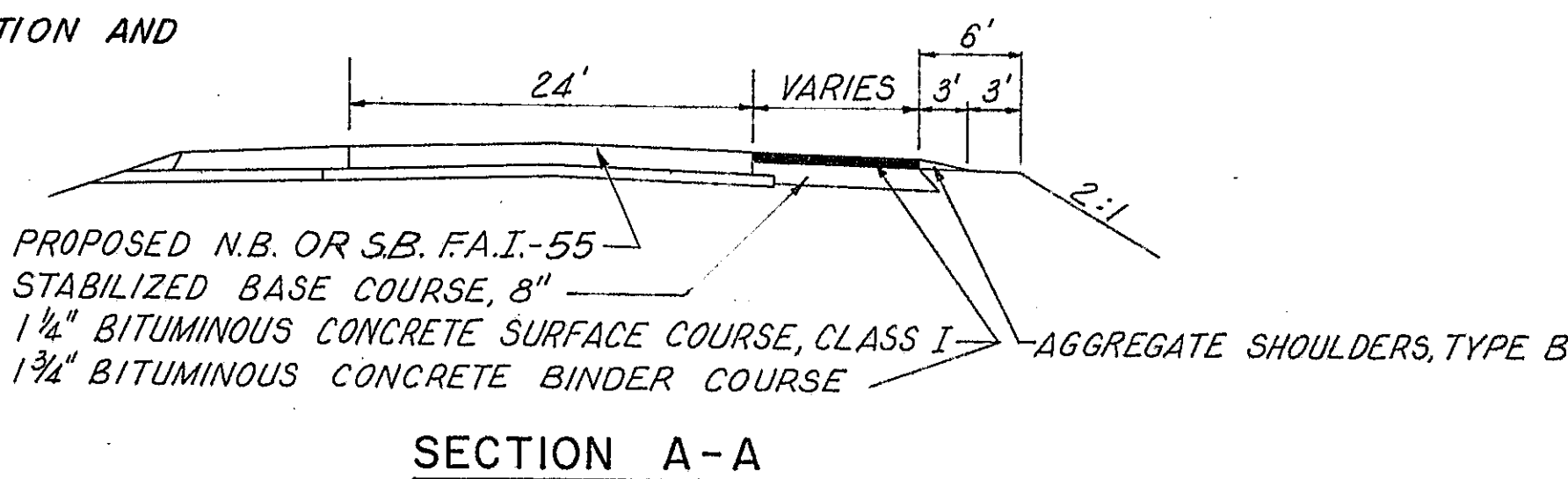
- LIGHTING UNIT
- POLE DOWN GUY
- CONTROL INSTALLATION AND SERVICE POLE
- CABLE

CROSSOVER QUANTITIES

- BITUMINOUS CONCRETE SURFACE COURSE, CLASS I = 142 TONS
- BITUMINOUS CONCRETE BINDER COURSE = 199 TONS
- STABILIZED BASE COURSE 8" = 2030 SQ. YDS.
- AGGREGATE SHOULDERS, TYPE B = 52 TONS

LIGHTING QUANTITIES

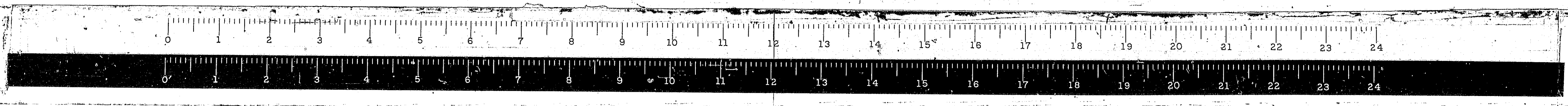
- LUMINAIRE MERCURY VAPOR WITH BUILT IN REGULATOR BALLAST, 400 WATTS
- LAMPS, MERCURY VAPOR, 400 WATT TYPE H33-1-CD
- POLES, WOOD, WIRED ASSEMBLY, 40 FT. M.H., 15 FT. MAST ARM.
- SERVICE INSTALLATION, TYPE CB RCS- 60
- ELECTRIC CABLE, AERIAL, 300 VOLT 2/C#4AL WITH ASCR MESSENGER WIRE
- POLE DOWN GUY.



STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

TEMPORARY CONNECTIONS 1 & 2
DETAILS

WESTENHOFF AND NOVICK, INC.
CONSULTING ENGINEERS

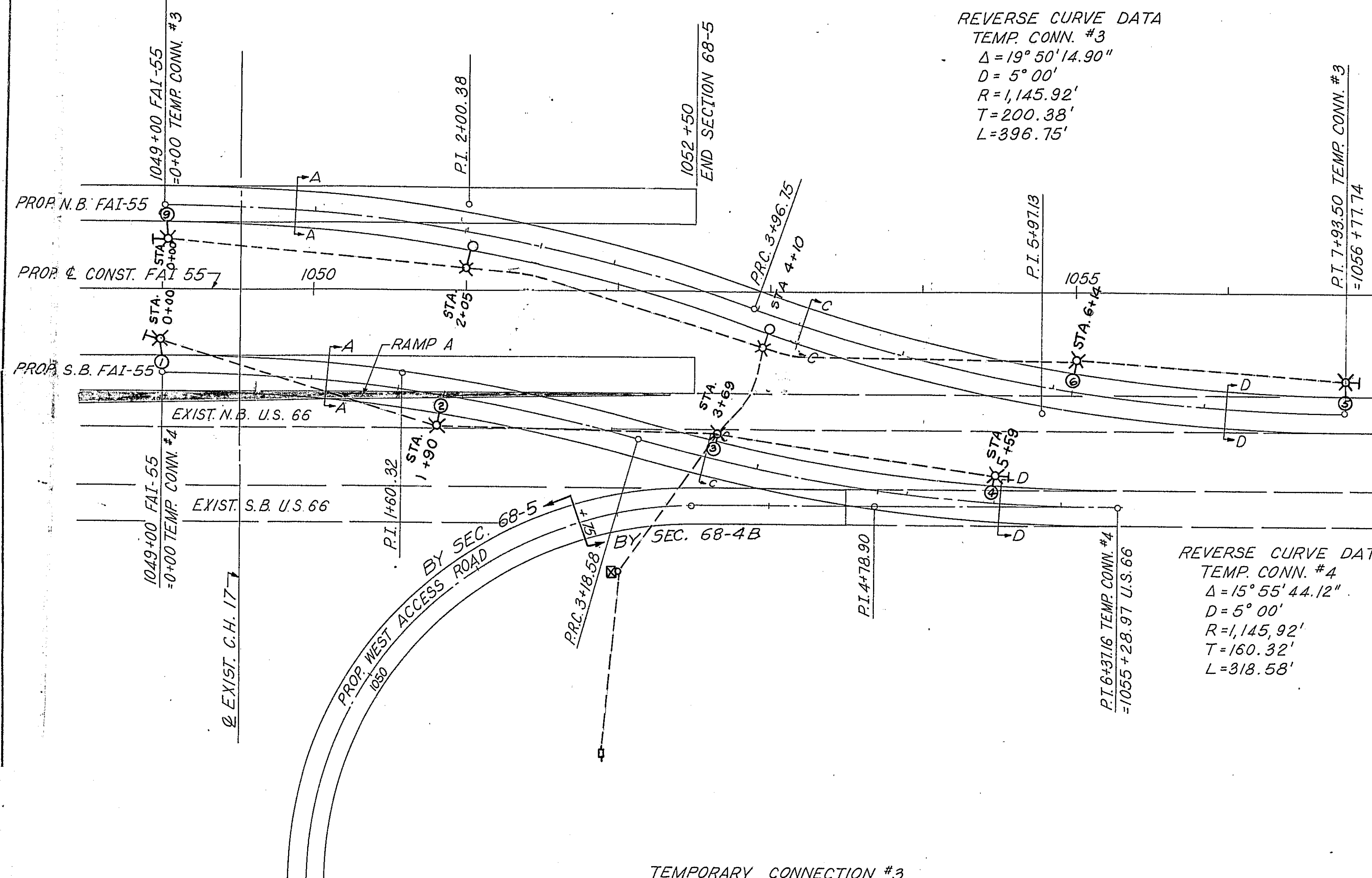


*68-5, 68-5HB, 68-5B

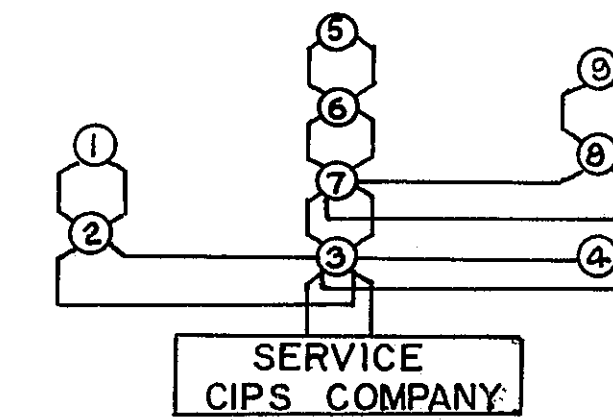
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	#	MONT	206	90
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	

REVERSE CURVE DATA
 TEMP. CONN. #3
 $\Delta = 19^\circ 50' 14.90''$
 $D = 5^\circ 00'$
 $R = 1,145.92'$
 $T = 200.38'$
 $L = 396.75'$

CROSSOVER QUANTITIES
 BITUMINOUS CONCRETE SURFACE COURSE, CLASS I = 145 TONS
 BITUMINOUS CONCRETE BINDER COURSE = 203 TONS
 STABILIZED BASE COURSE 8" = 2064 SQ. YDS.
 AGGREGATE SHOULDERS, TYPE B = 55 TONS



SCHEMATIC CIRCUIT DIAGRAM



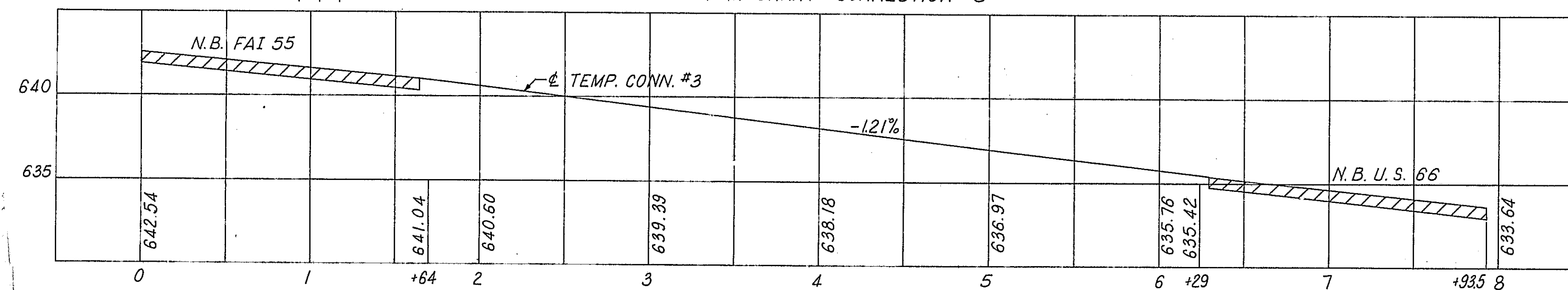
LIGHTING LEGEND

- LIGHTING UNIT
- POLE DOWN GUY
- CONTROL INSTALLATION AND SERVICE POLE
- CABLE

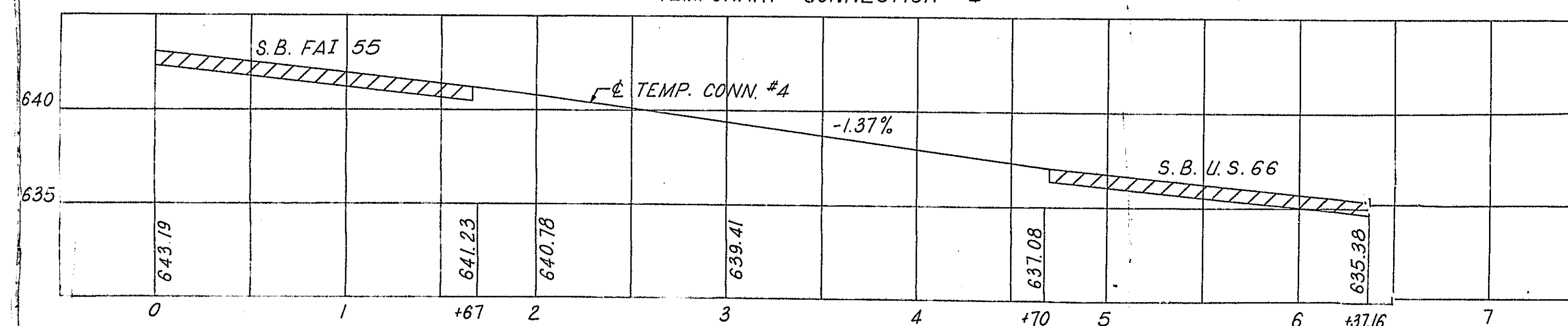
LIGHTING QUANTITIES

- LUMINAIRE MERCURY VAPOR WITH BUILT IN REGULATOR BALLAST, 400 WATTS EACH 9
- LAMPS, MERCURY VAPOR, 400 WATT TYPE H33-1-CD EACH 9
- POLES, WOOD, WIRED ASSEMBLY, 40 FT M.H., 15 FT MAST ARM EACH 9
- SERVICE INSTALLATION, TYPE CB RCS-60 EACH 1
- ELECTRIC CABLE, AERIAL, 300 VOLT 2/C #4 AL WITH ASCR LIN. FT. 1532
- MESSENGER WIRE EACH 4
- POLE DOWN GUY

TEMPORARY CONNECTION #3



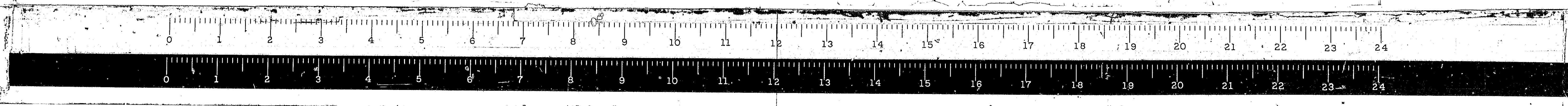
TEMPORARY CONNECTION #4

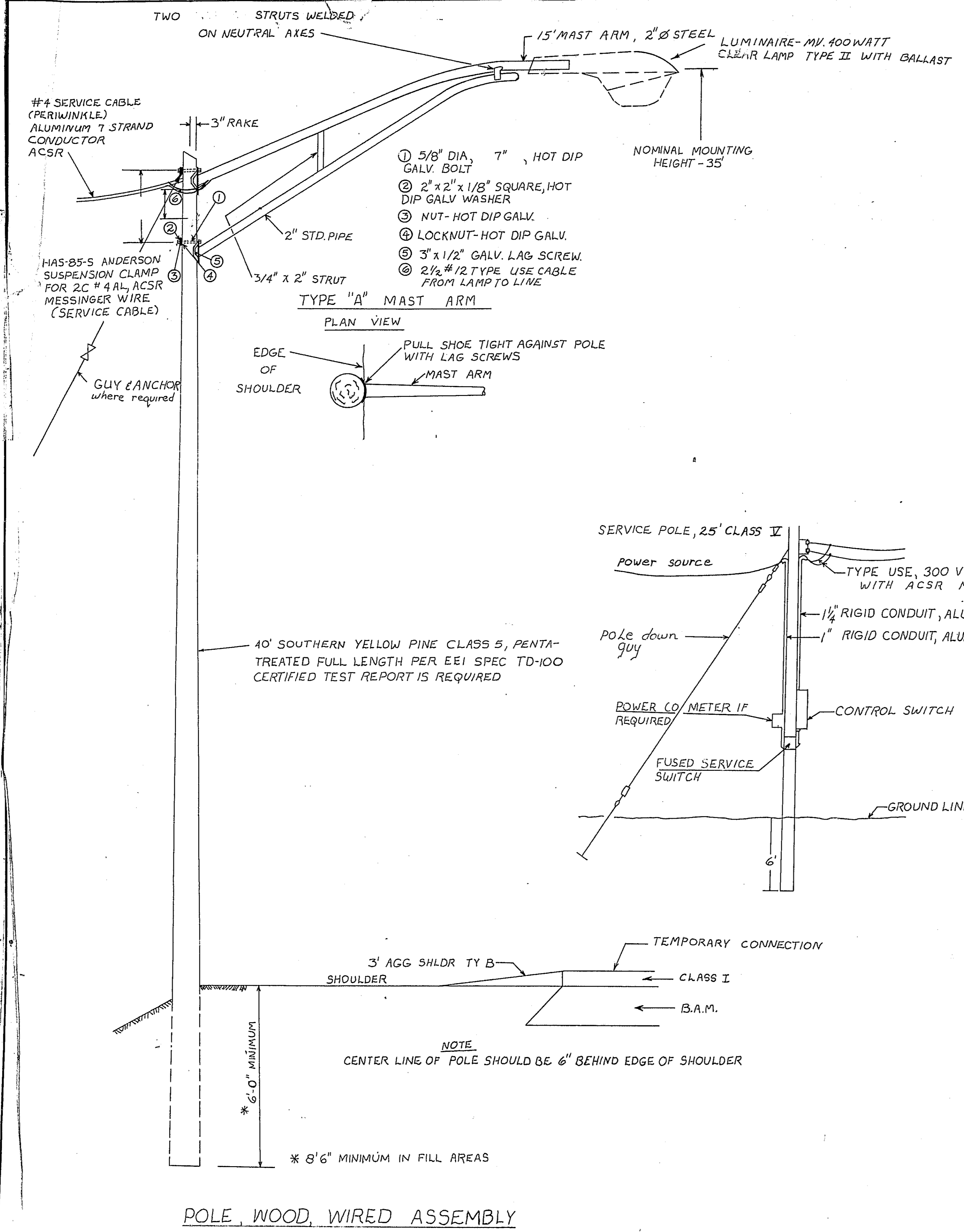


STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

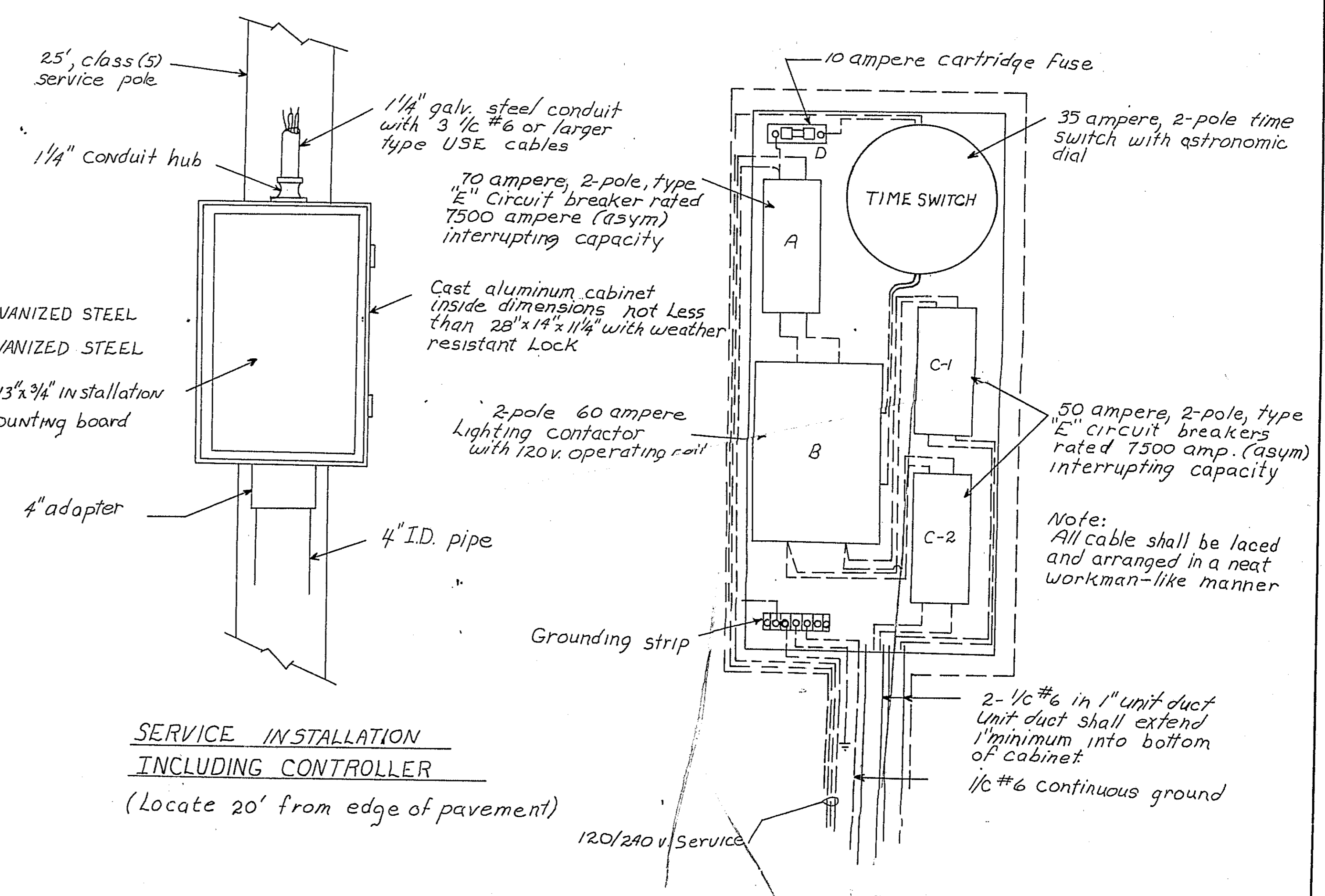
**TEMPORARY CONNECTIONS 3 & 4
 DETAILS**

WESTENHOFF AND NOVICK, INC.
 CONSULTING ENGINEERS

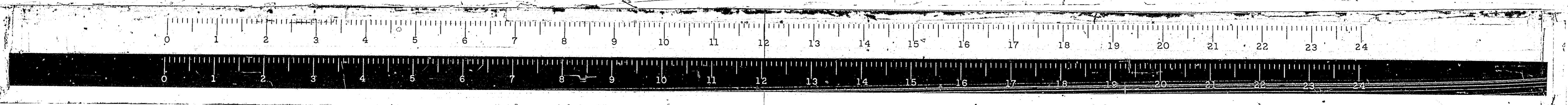




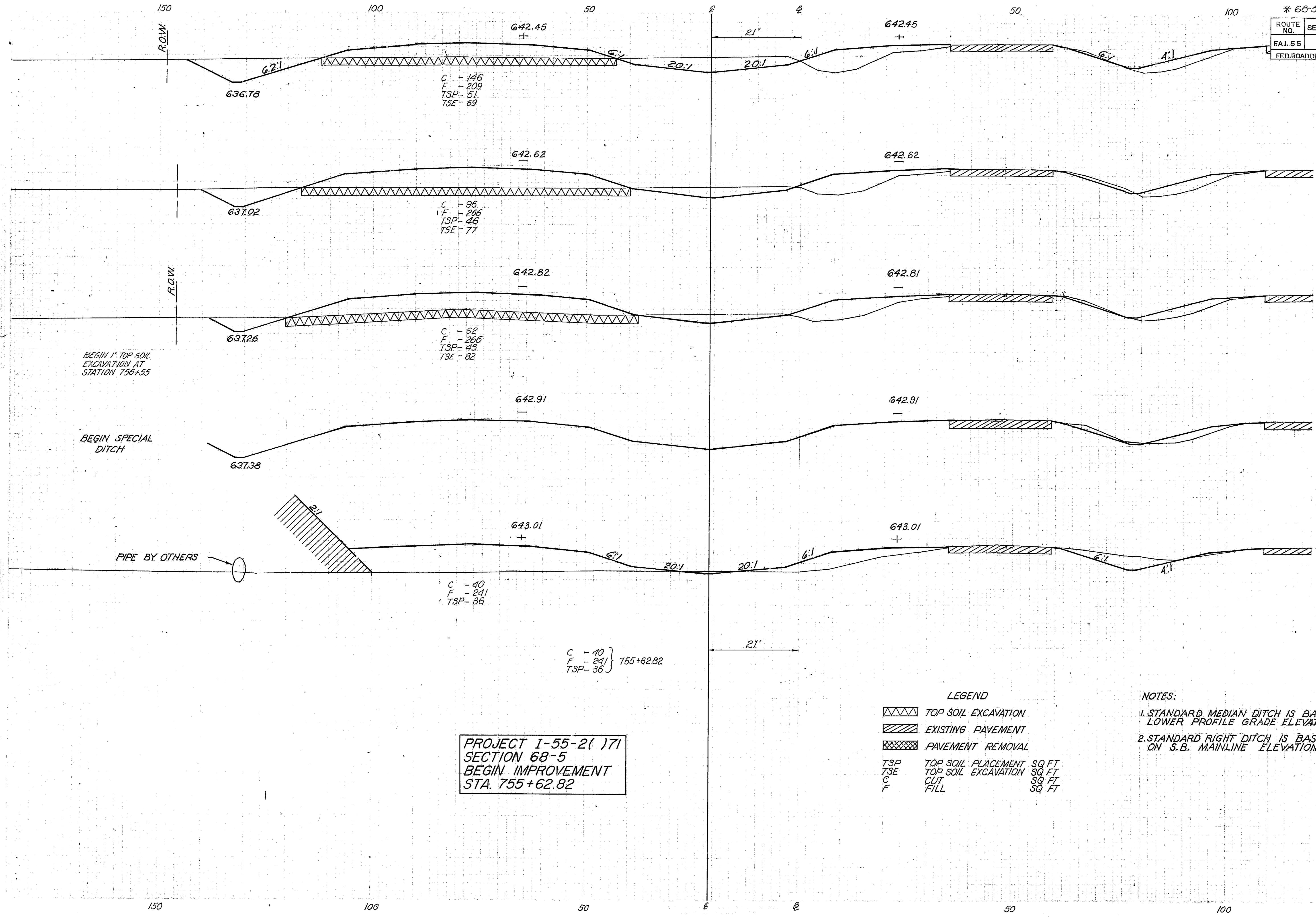
PAY ITEM	CODE NO.	A	B	C1, C2	D	TIME SWITCH
SERVICE INSTALLATION, TYPE CB-RCS-60	L04110	TYPE "E" 70 AMPERE	2-POLE 60 AMPERE	TYPE "E" 50 AMPERE	10 AMP	35 AMPERE 2-POLE



DETAILS SERVICE INSTALLATION POLE, WOOD, WIRED ASSEMBLY



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	92
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.			640	



759
00

758
00

635

640

757
00

635

640

756
50

635

640

756
00

635

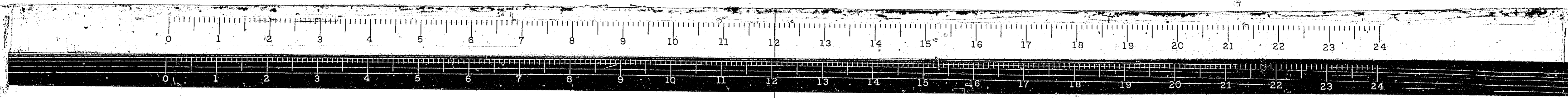
PROJECT I-55-2()71
SECTION 68-5
BEGIN IMPROVEMENT
STA. 755+62.82

LEGEND

	TOP SOIL EXCAVATION	
	EXISTING PAVEMENT	
	PAVEMENT REMOVAL	
TSP	TOP SOIL PLACEMENT	SQ. FT.
TSE	TOP SOIL EXCAVATION	SQ. FT.
C	CUT	SQ. FT.
F	FILL	SQ. FT.

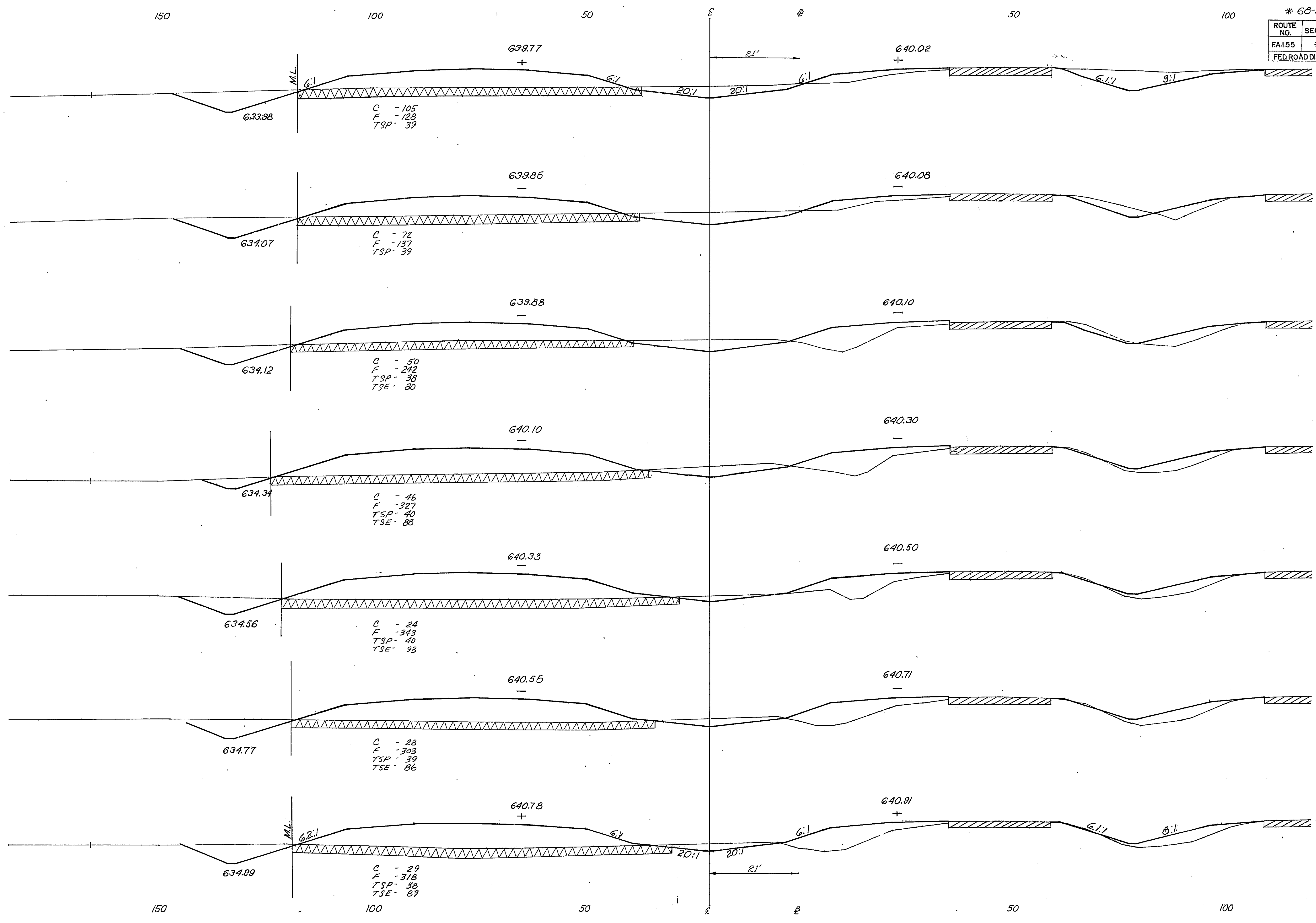
NOTES:

- STANDARD MEDIAN DITCH IS BASED ON LOWER PROFILE GRADE ELEVATION.
- STANDARD RIGHT DITCH IS BASED ON S.B. MAINLINE ELEVATIONS.



* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	94
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



77/39

77/13

77/00

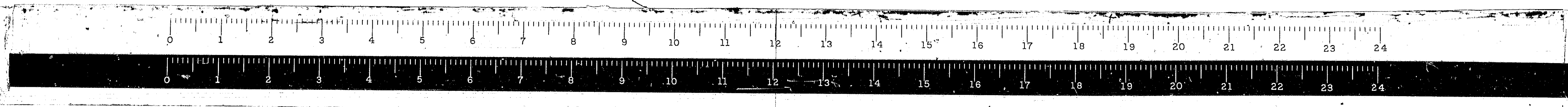
77/00

769/00

768/00

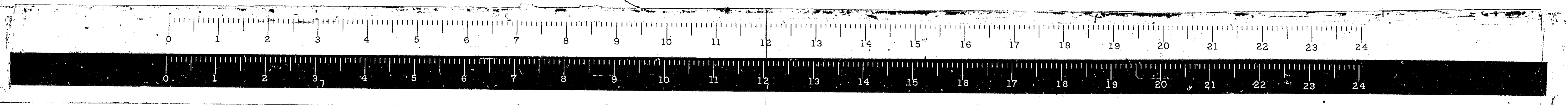
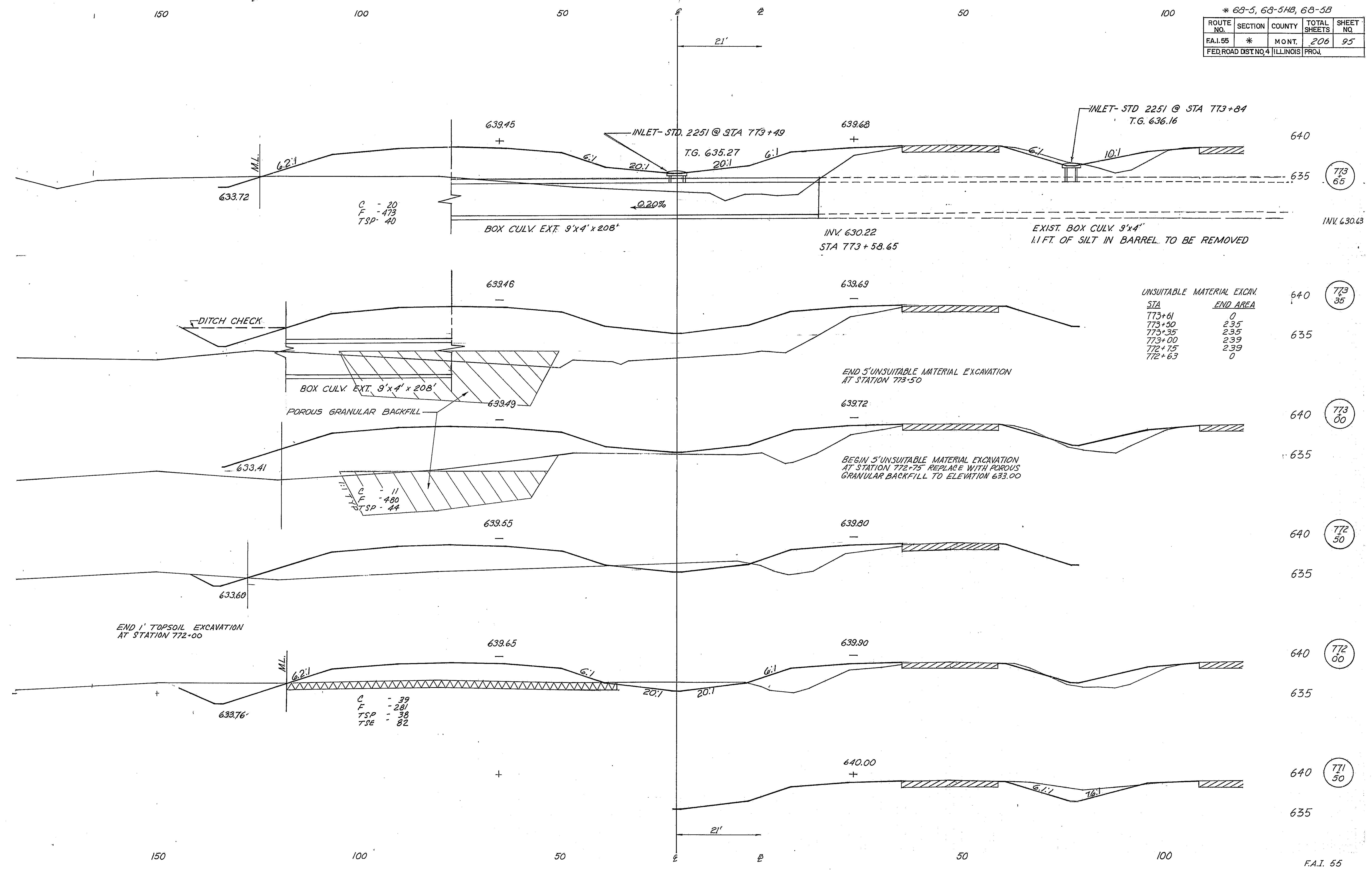
767/00

FAI. 55



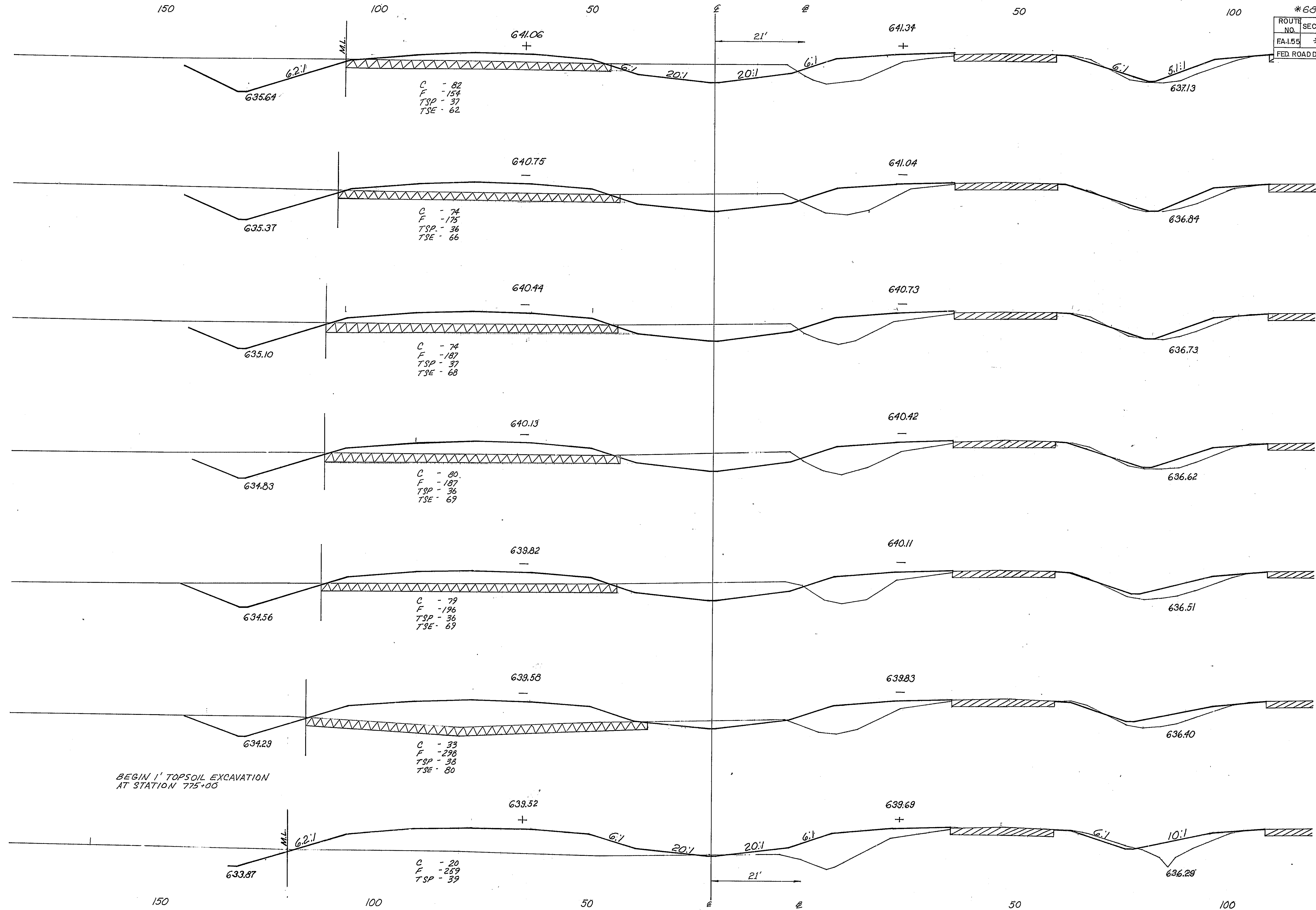
* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	95
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



*63-5, 63-5A, 63-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.155	*	MONT.	206	96
FED. ROAD DIST. NO. ILLINOIS			PROJ.	



780
00
635

779
00
640
635

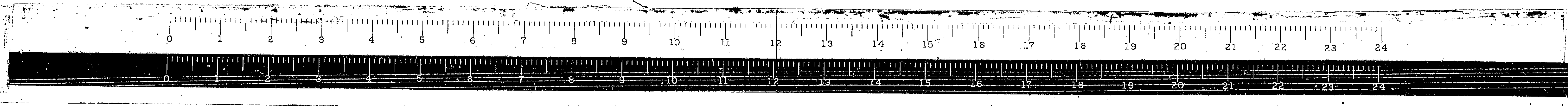
778
00
640
635

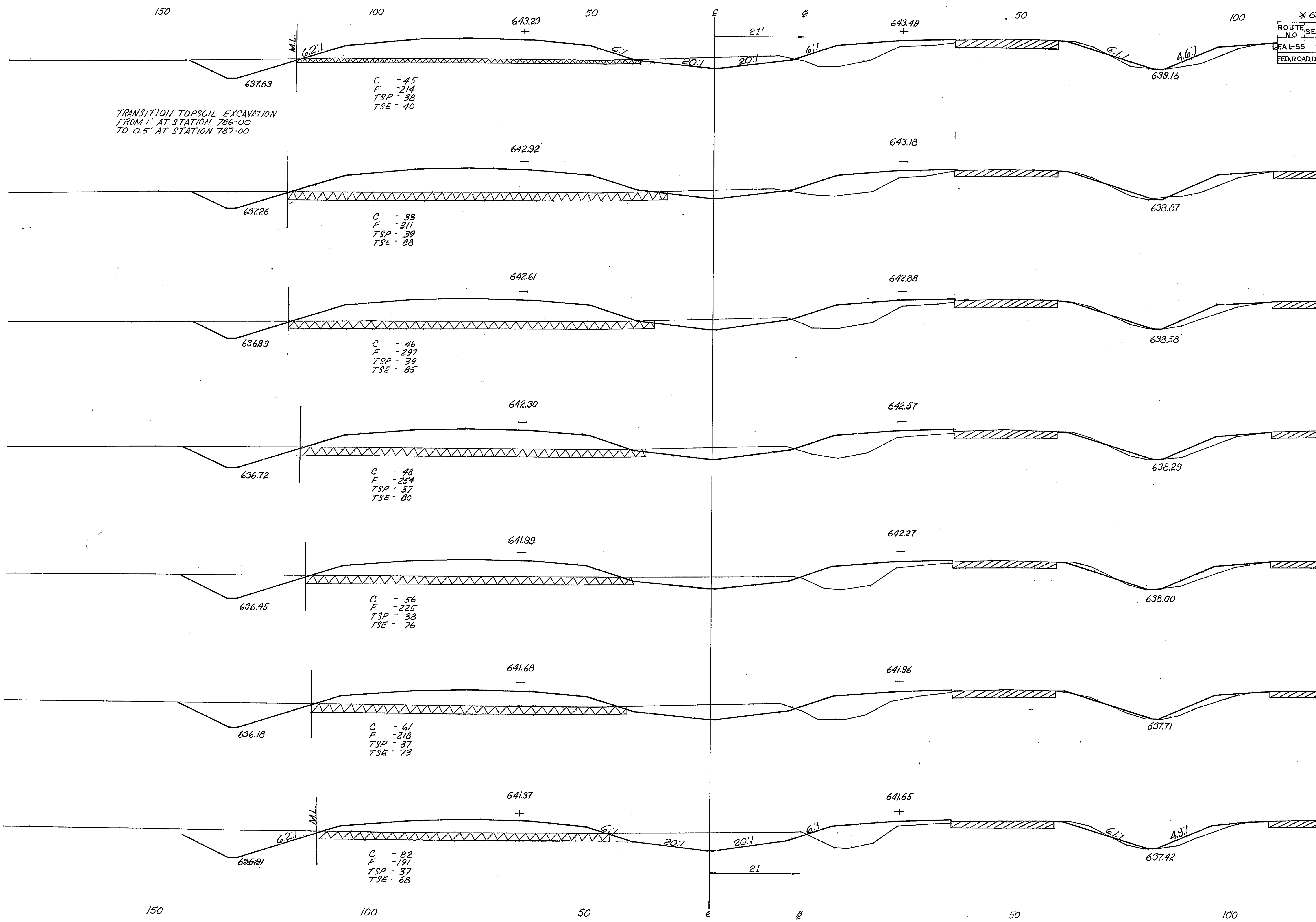
777
00
640
635

776
00
640
635

775
00
640
635

774
00
640
635





* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI-55	*	MONT.	206	97
FED. ROAD DIST. NO. ILLINOIS			PRJ.	

787
00

635

786
00

640

635

785
00

640

635

784
00

640

635

783
00

640

635

782
00

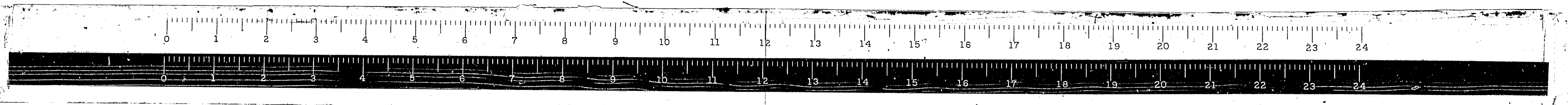
640

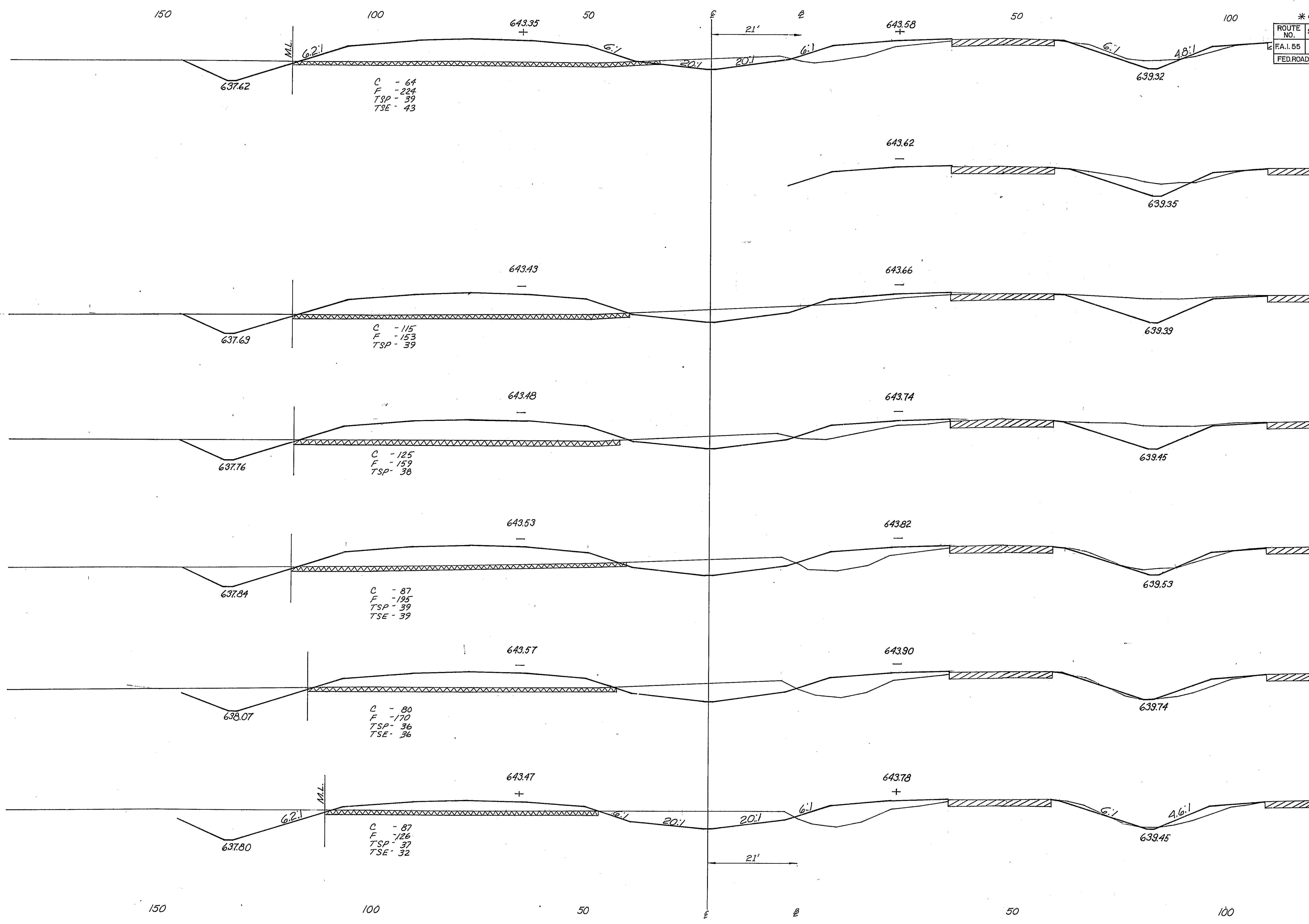
635

781
00

640

635





* 63-5, 63-54B, 63-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I. 55	*	MONT.	206	98
FED. ROAD DIST. NO. 6 ILLINOIS PROJ.			640	

791
00

635

640

790
85

635

640

790
66

635

640

790
37

635

640

790
00

635

640

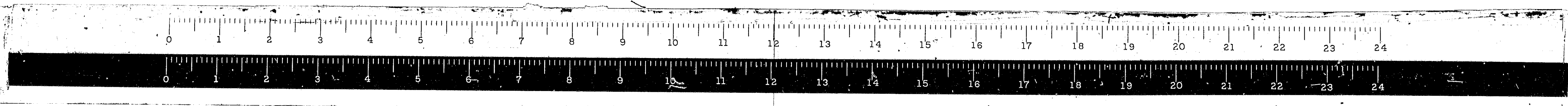
789
00

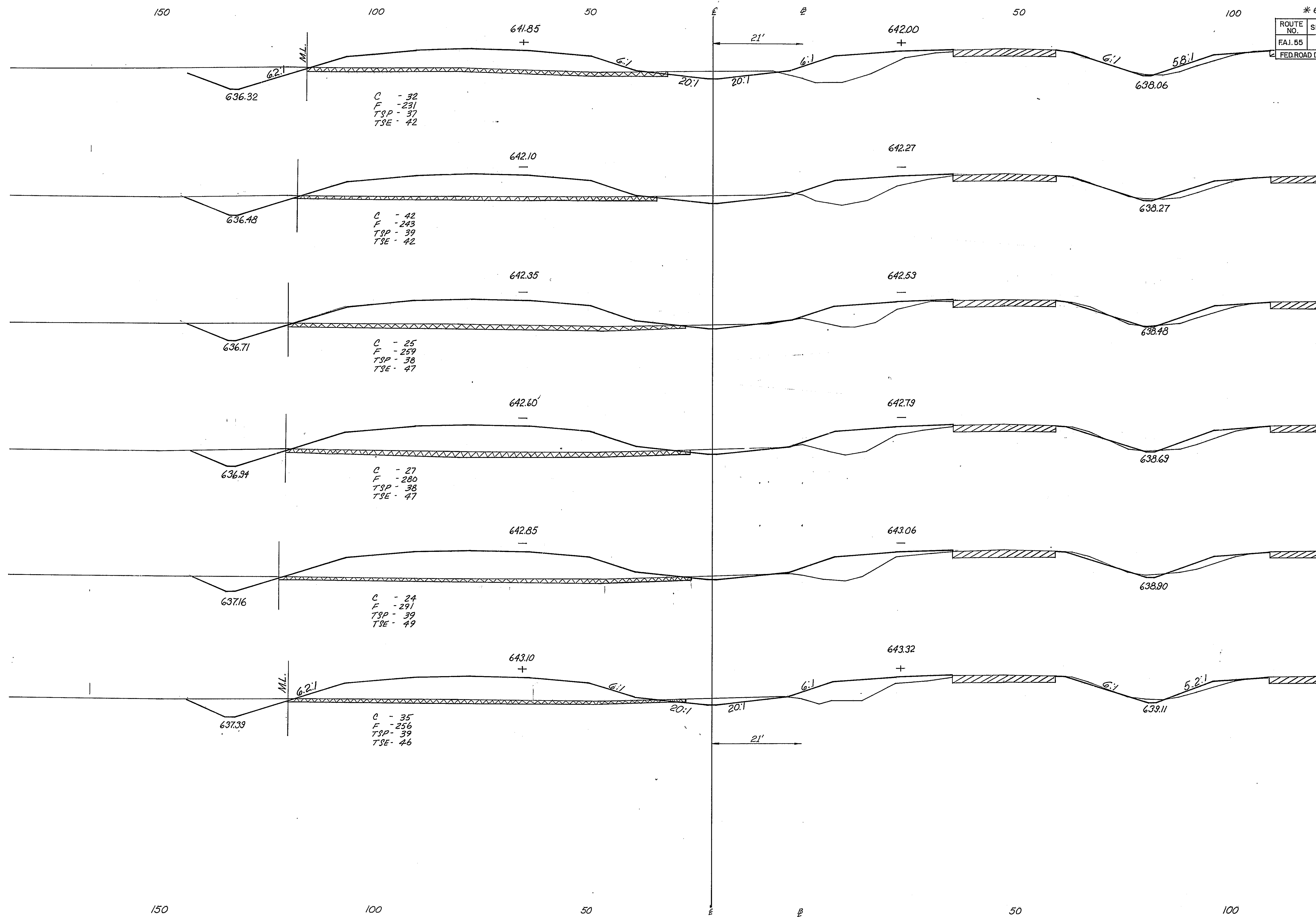
635

640

788
00

635





* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	99
FED. ROAD DIST. NO. 6 ILLINOIS PROJ.			640	

797
00

645

640
796
00

645

640
795
00

645

640
794
00

645

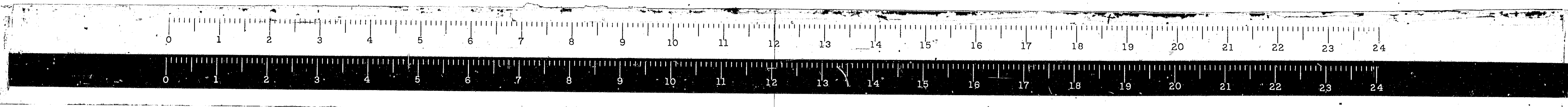
640
793
00

645

640
792
00

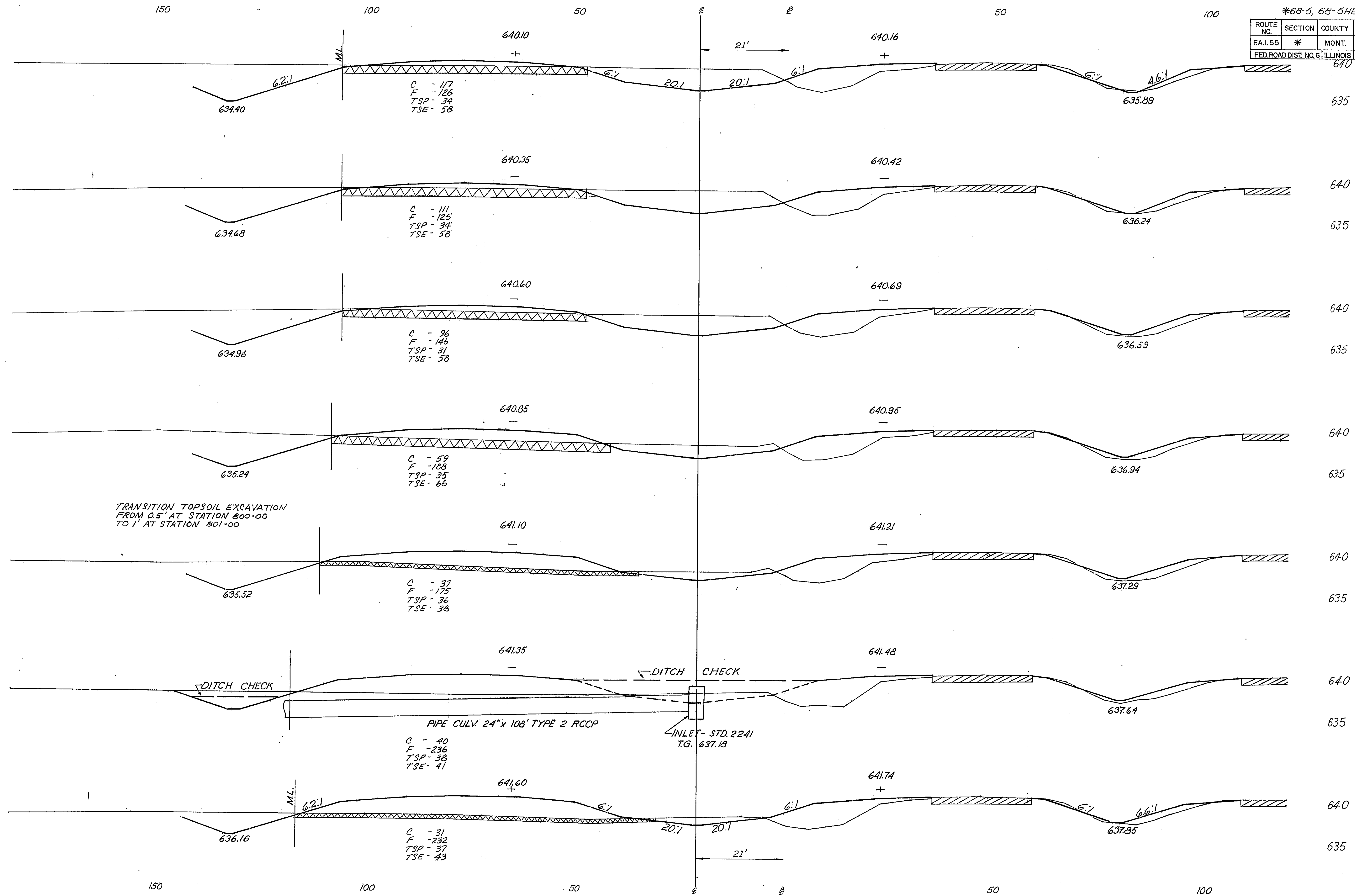
150 100 50 E E 50 100

F.A.I. 55



*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	100
FED. ROAD DIST. NO. 6 ILLINOIS PROJ.				



804
00

803
00

802
00

801
00

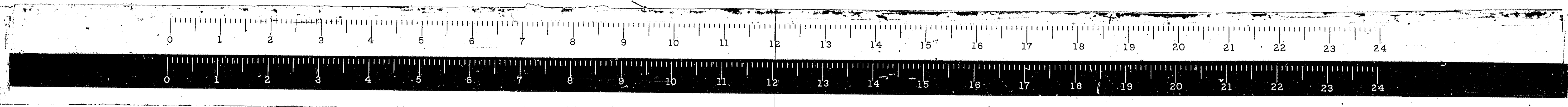
800
00

799
00

798
00

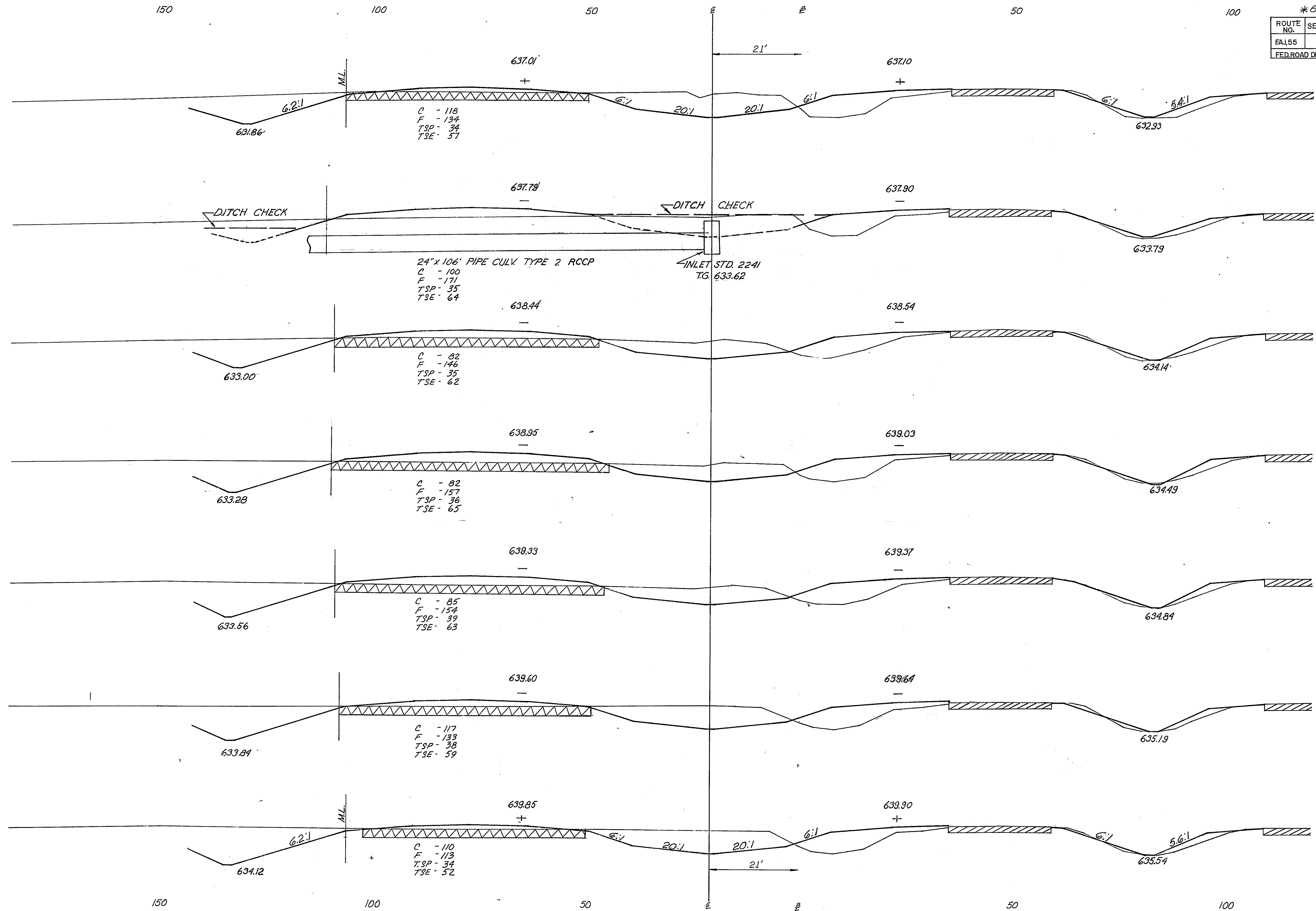
TRANSITION TOPSOIL EXCAVATION
FROM 0.5' AT STATION 800+00
TO 1' AT STATION 801+00

PIPE CULV. 24" x 108' TYPE 2 RCCP
INLET - STD. 2241
T.G. 637.13



* 68-5, 68-54B, 68-58

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAJ55	*	MONT.	206	101
FED. ROAD DIST. NO. 4 ILLINOIS PROJ. 640				



635 (811 00)

640

635 (810 00)

640

635 (809 00)

640

635 (808 00)

640

635 (807 00)

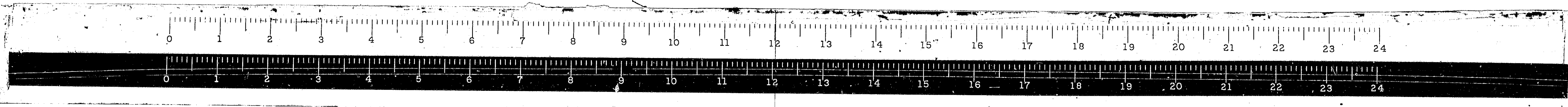
640

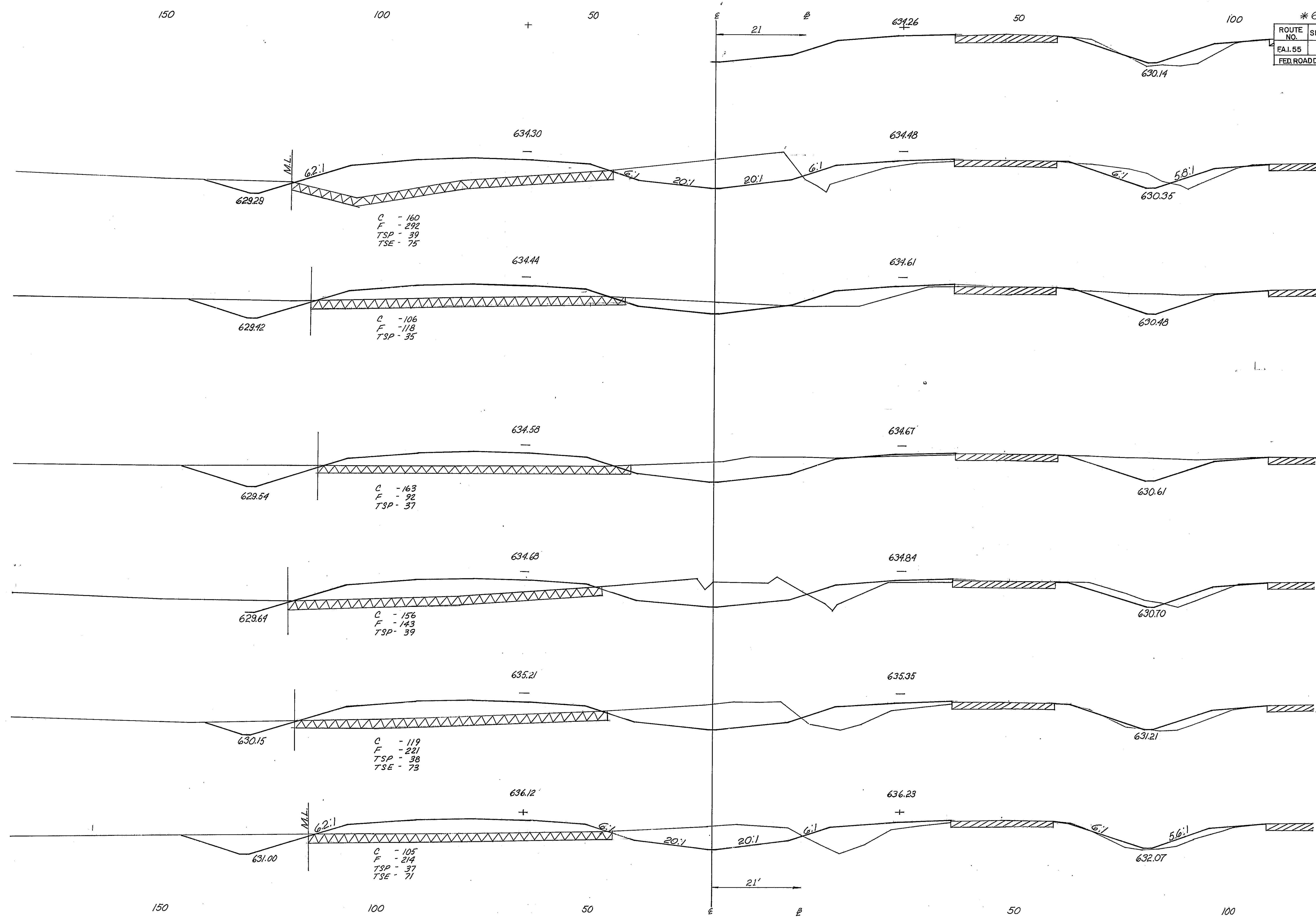
635 (806 00)

640

635 (805 00)

F.A.I. 55





* 68-5, 68-54B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MON T.	206	102
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.			630	

8/14
25

8/14
00

8/13
85

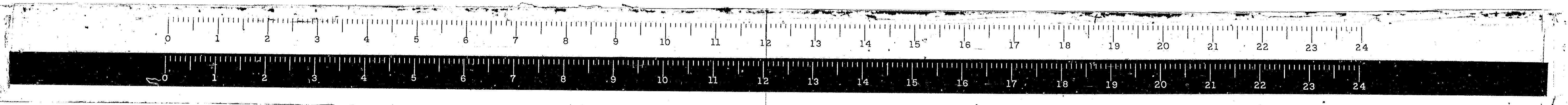
8/13
70

8/13
50

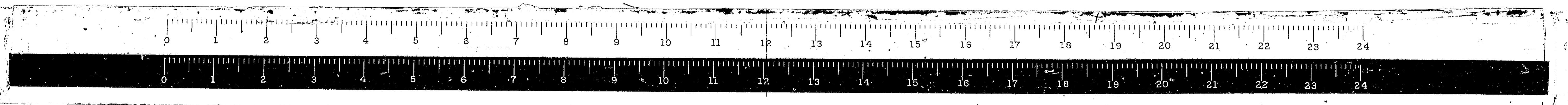
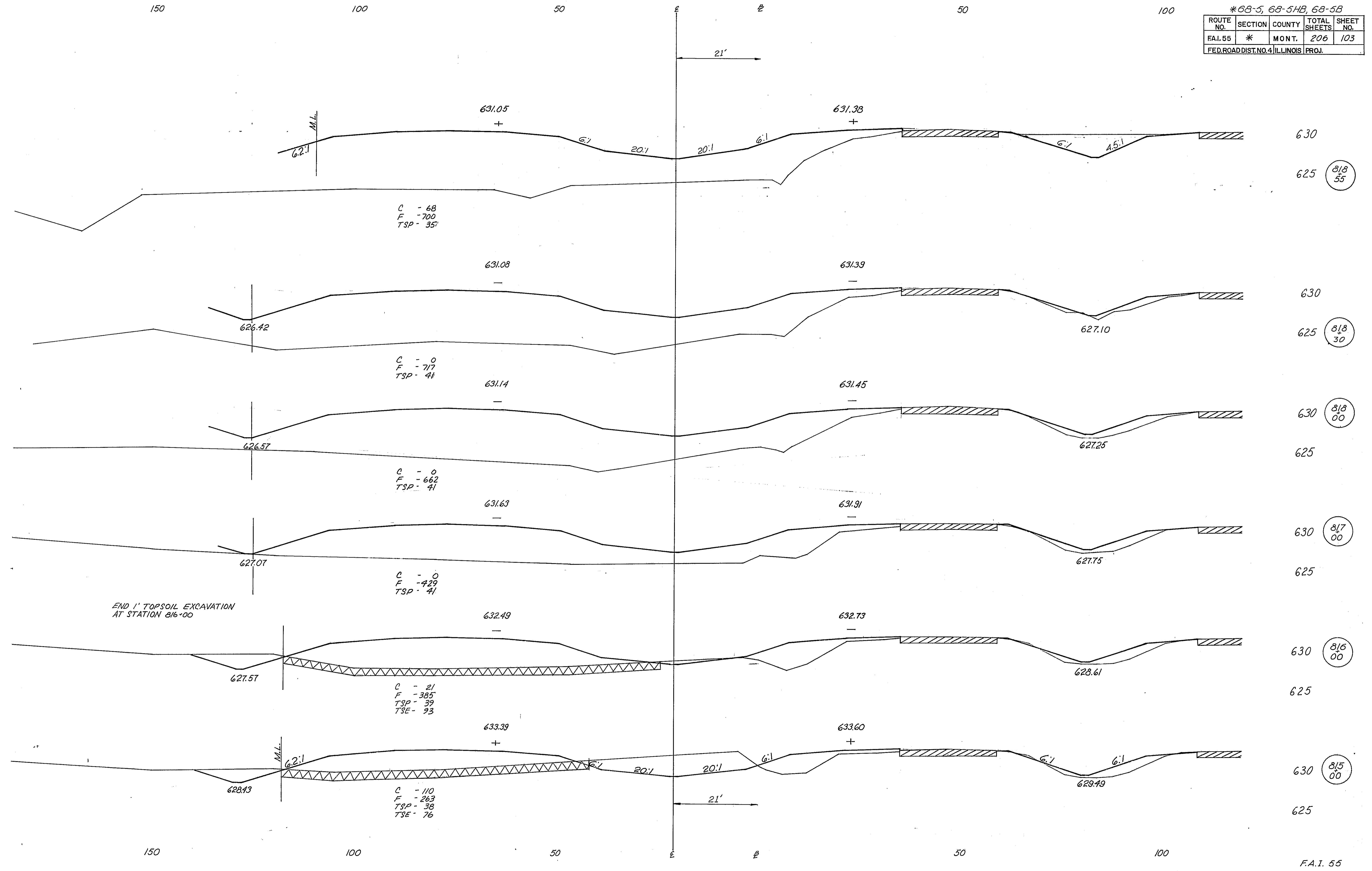
8/13
00

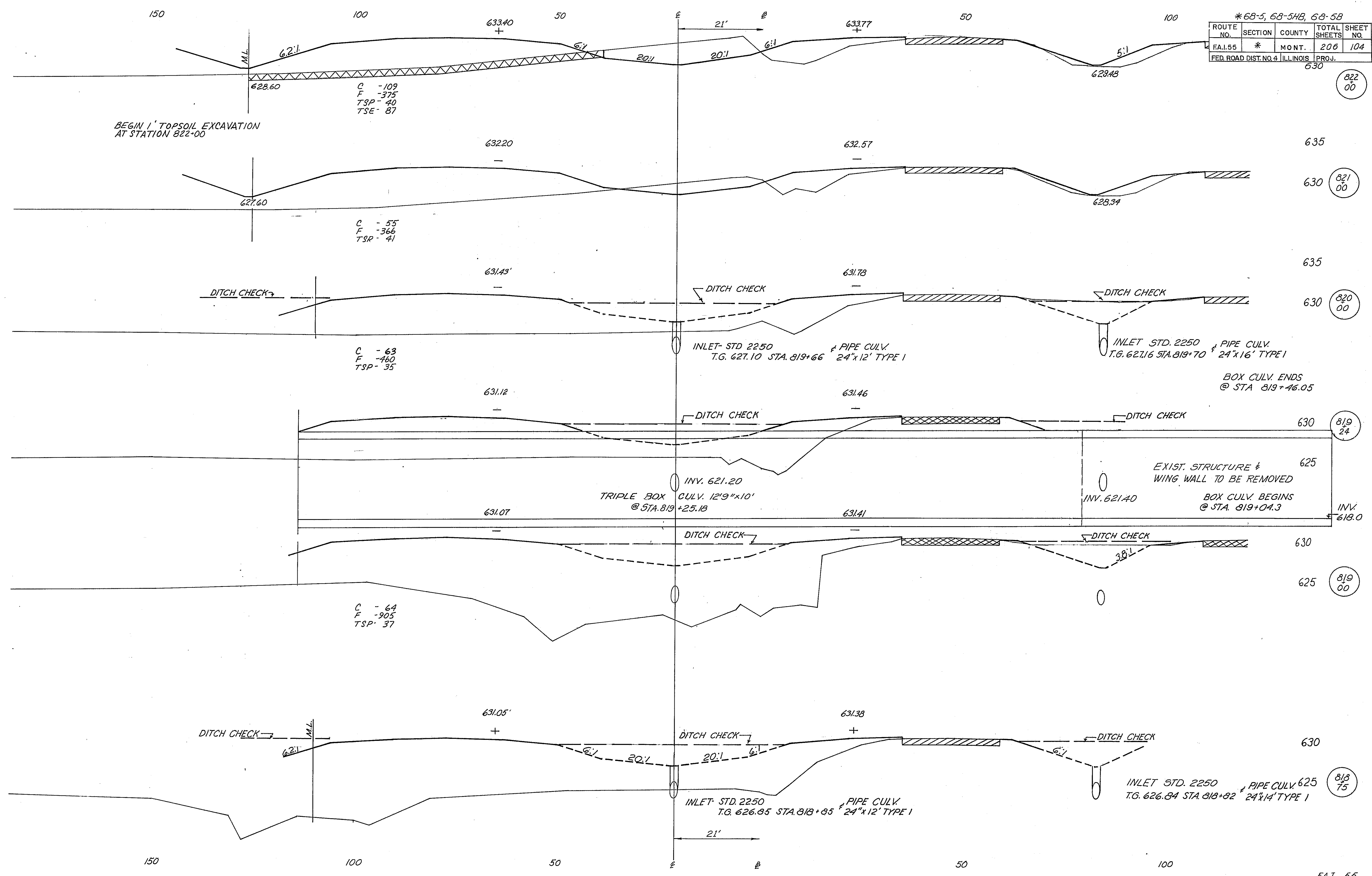
8/12
00

FAI. 55



*68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI.55	*	MONT.	206
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.			SHEET NO. 103





*68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI.55	*	MONT.	206
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ. 630

822
00

821
00

820
00

819
24

819
00

818
75

150

100

50

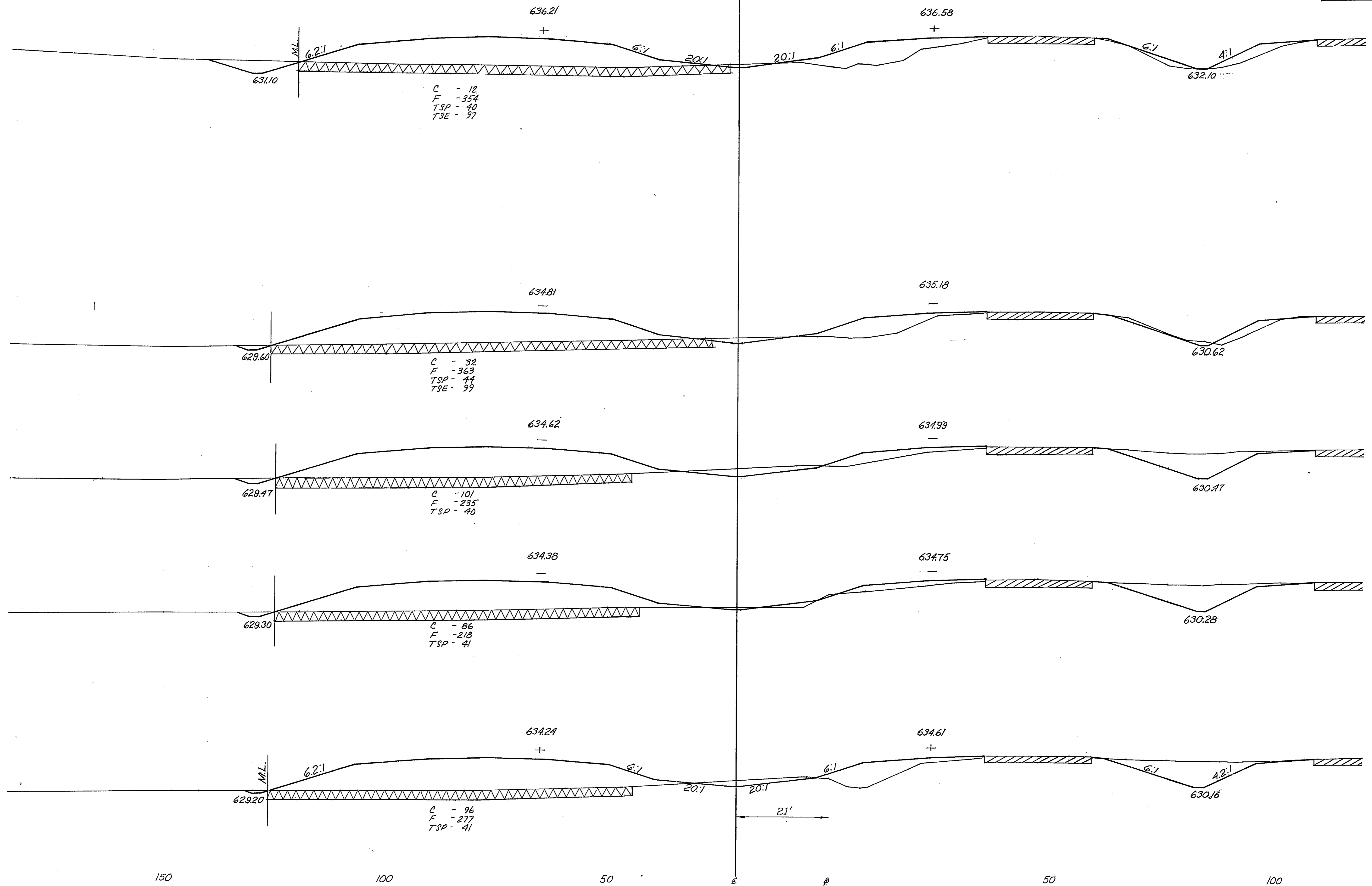
E

2

50

100

* 68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI-55	*	MONT.	206
FED. ROAD DIST. NO. 4		ILLINOIS	PROJ.



635 (824 00)

630

635

630 (823 00)

635

630 (822 87)

635

630 (822 70)

635

630 (822 60)

150

100

50

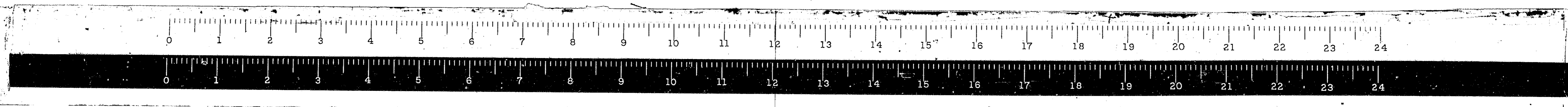
E

2

50

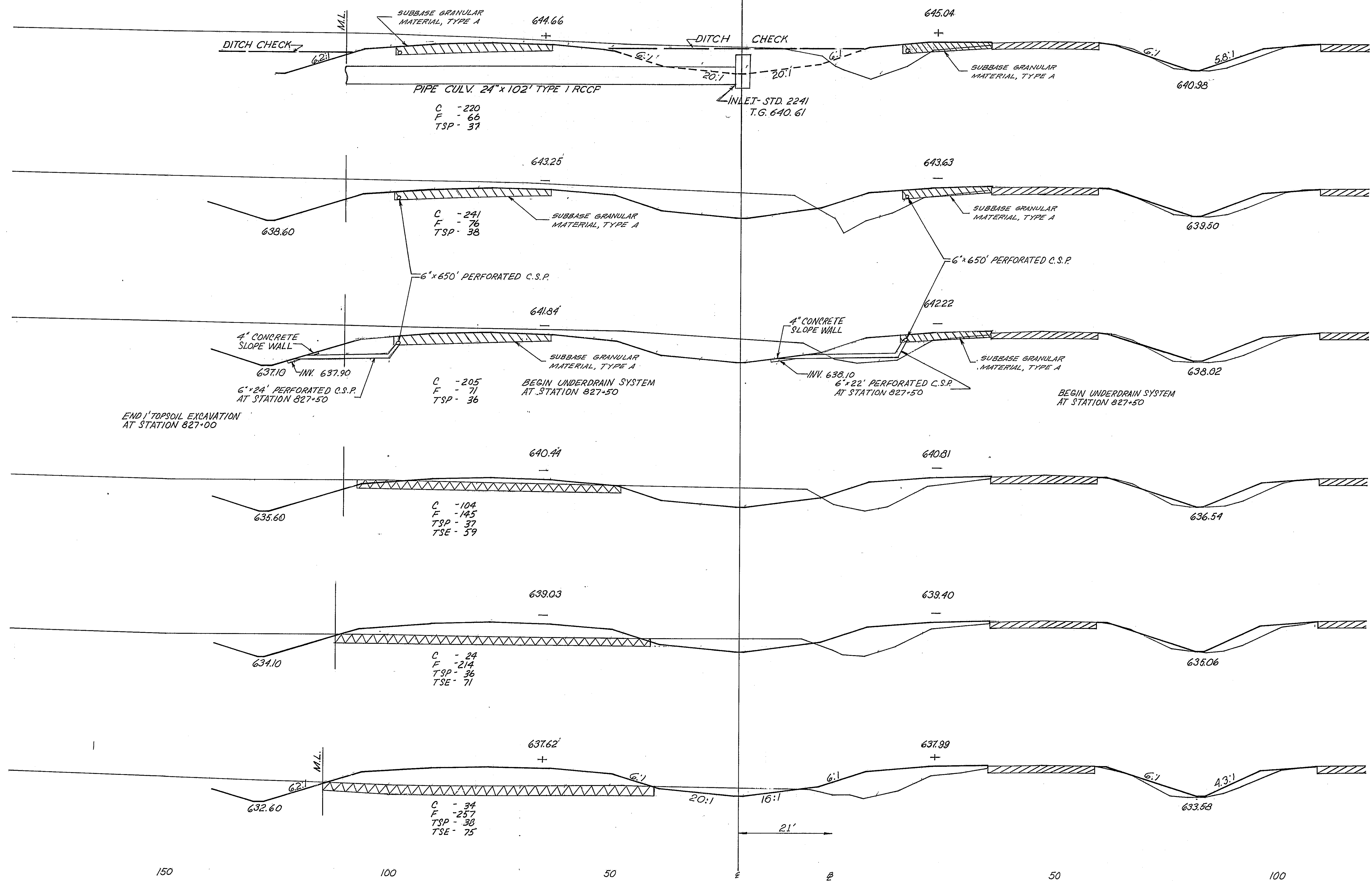
100

FAI 55



150 100 50 50 100

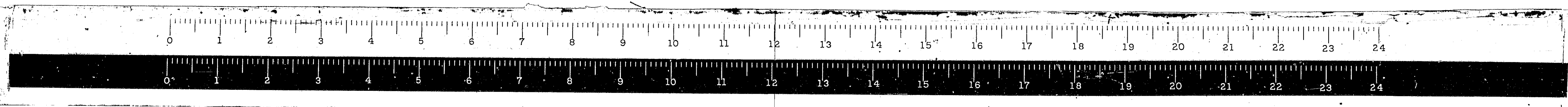
* 68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEET NO.
F.A.I. 55	*	MONT.	206 106
FED. ROAD DIST. NO. 4		ILLINOIS	PROJ.



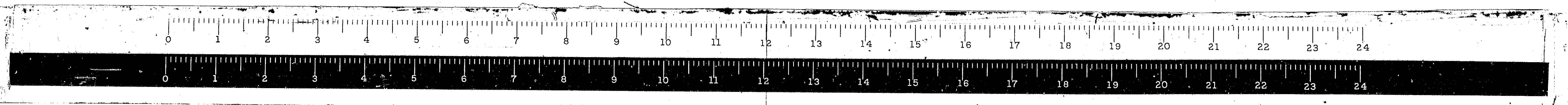
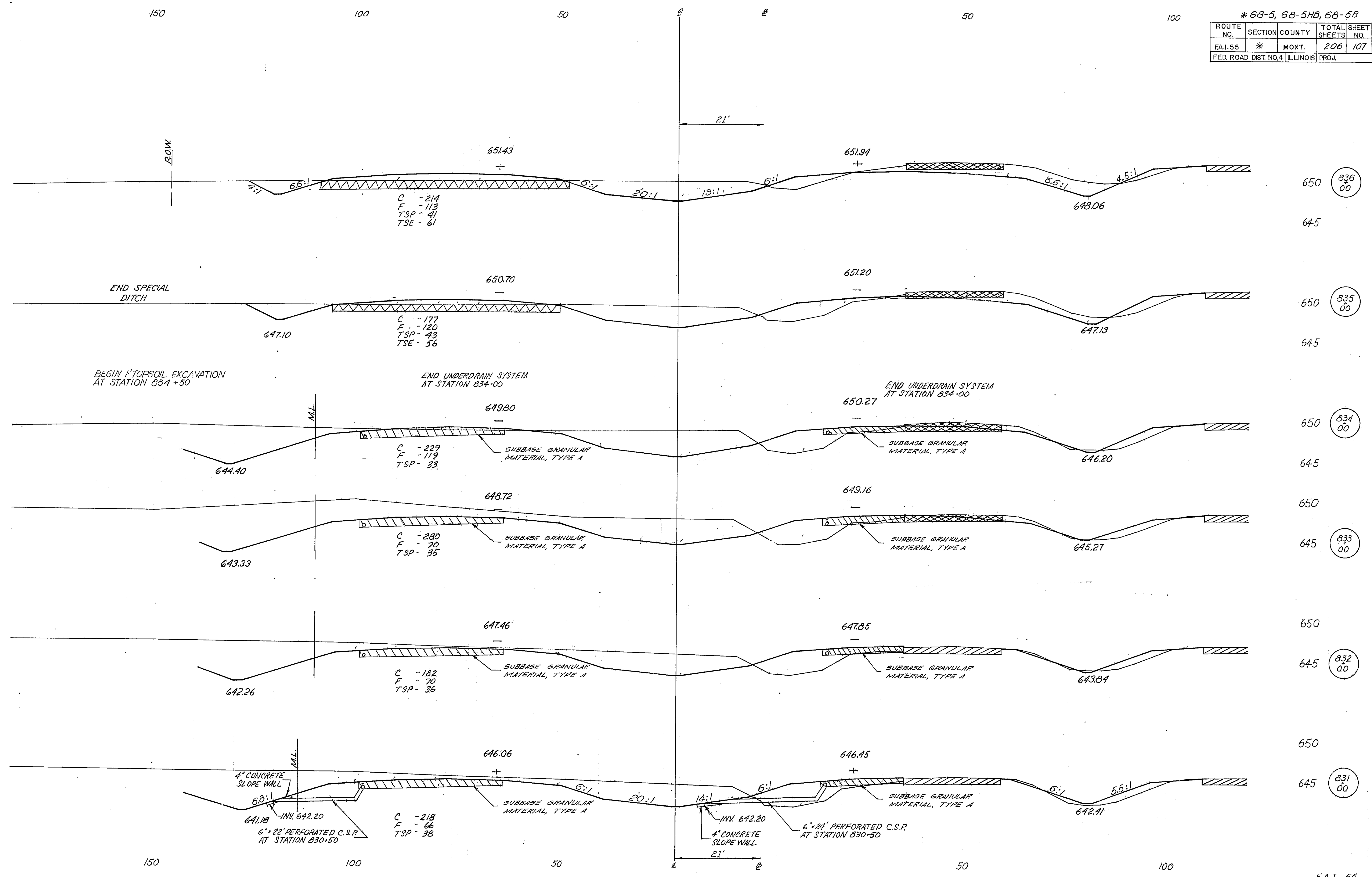
645 (830 00)
640
645 (829 00)
640 (828 00)
640 (827 00)
635
640
635 (826 00)
640
635 (825 00)

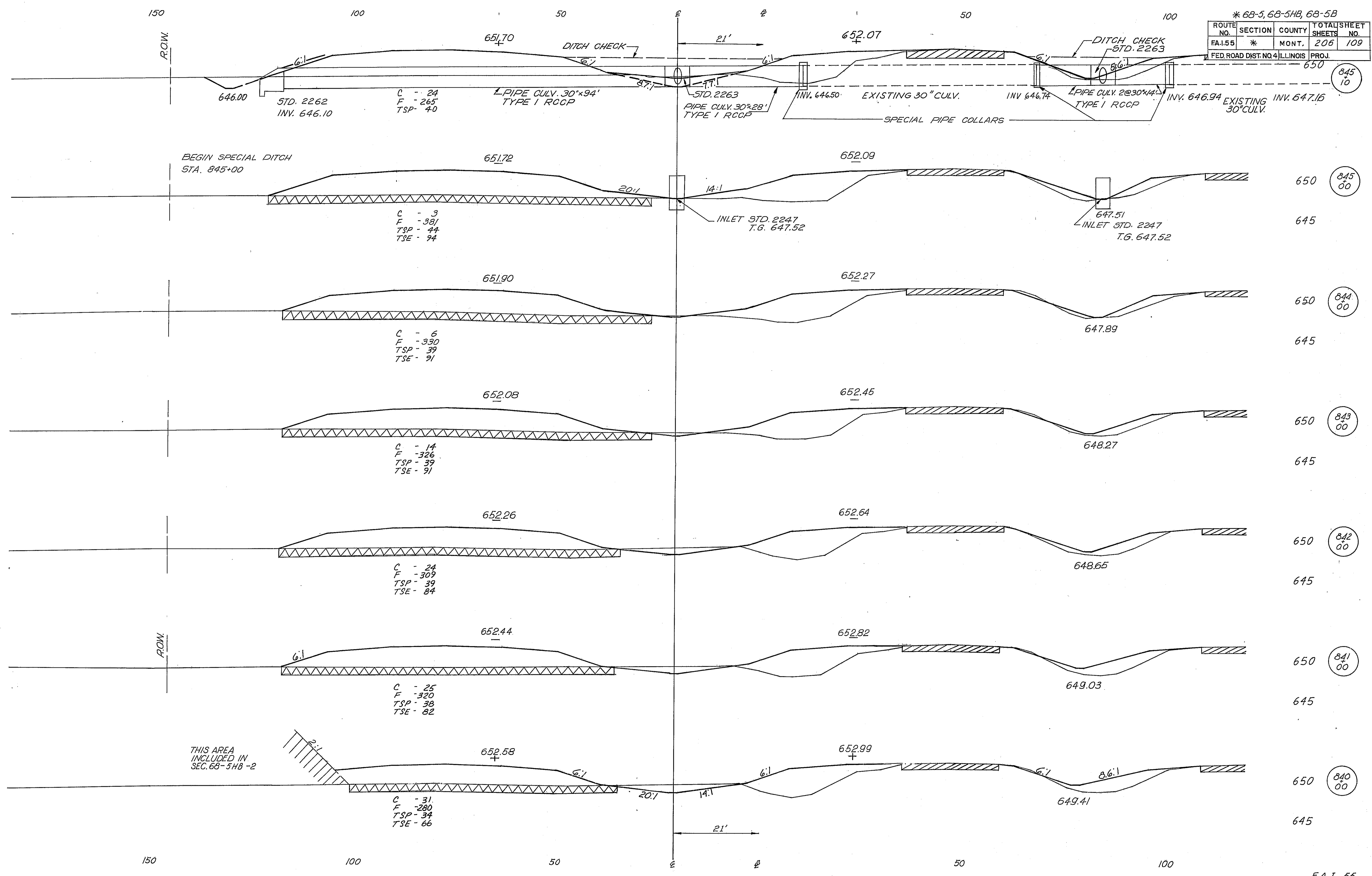
150 100 50 50 100

F.A.I. 55



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	107
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				





845 10

845 00

645

844 00

645

843 00

645

842 00

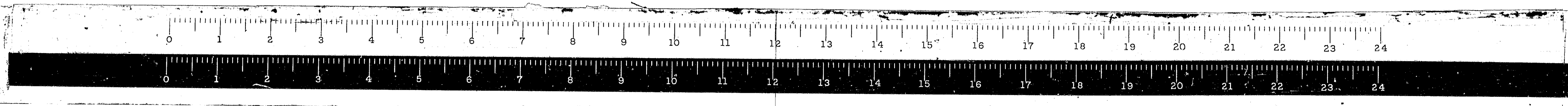
645

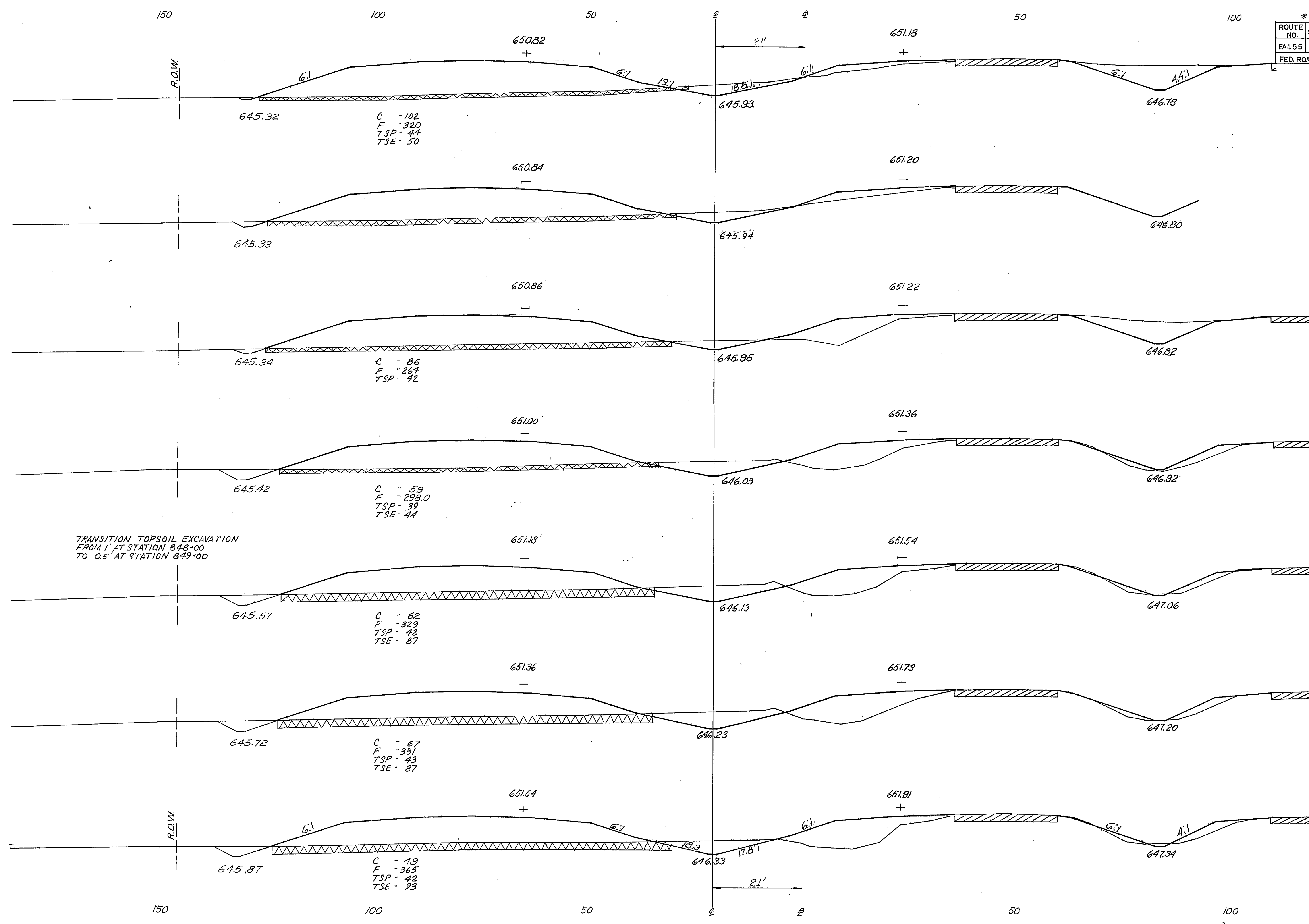
841 00

645

840 00

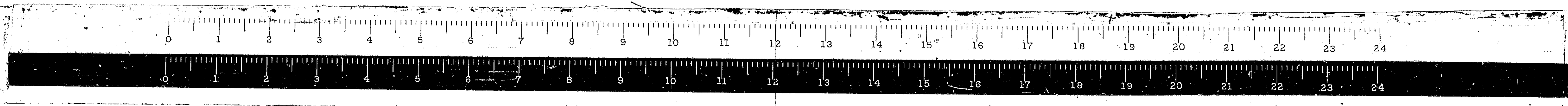
645





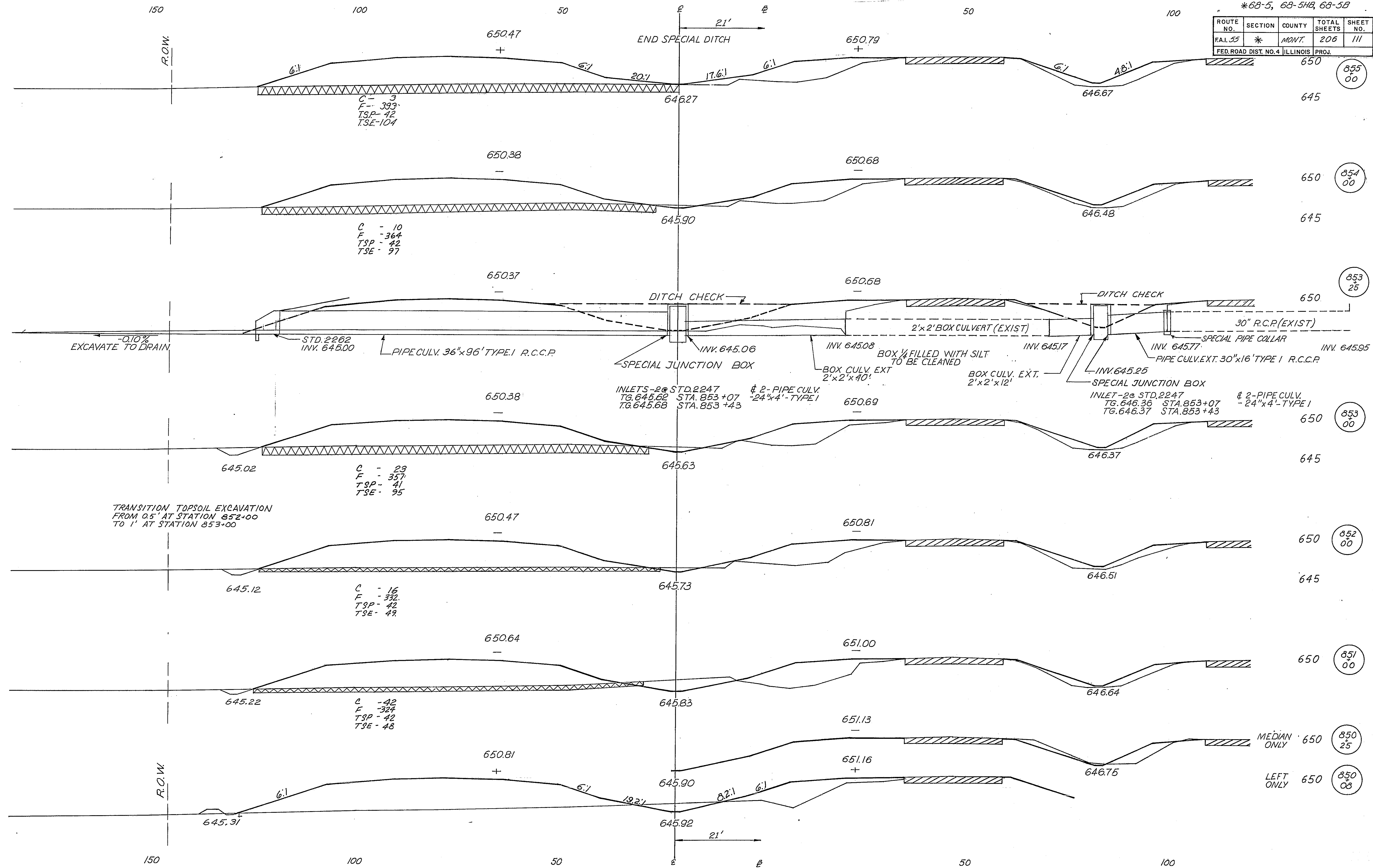
* 63-5, 63-5HB, 63-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEET NO.
FAI.55	*	MONT.	206
FED. ROAD DIST. NO. ILLINOIS PROJ.			650

- 850
00
- 849
87
- 849
79
- 849
00
- 848
00
- 847
00
- 846
00



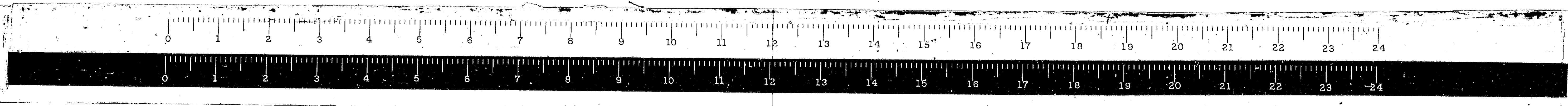
*68-5, 68-5A, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I. 35	*	MONT.	206	111
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				650

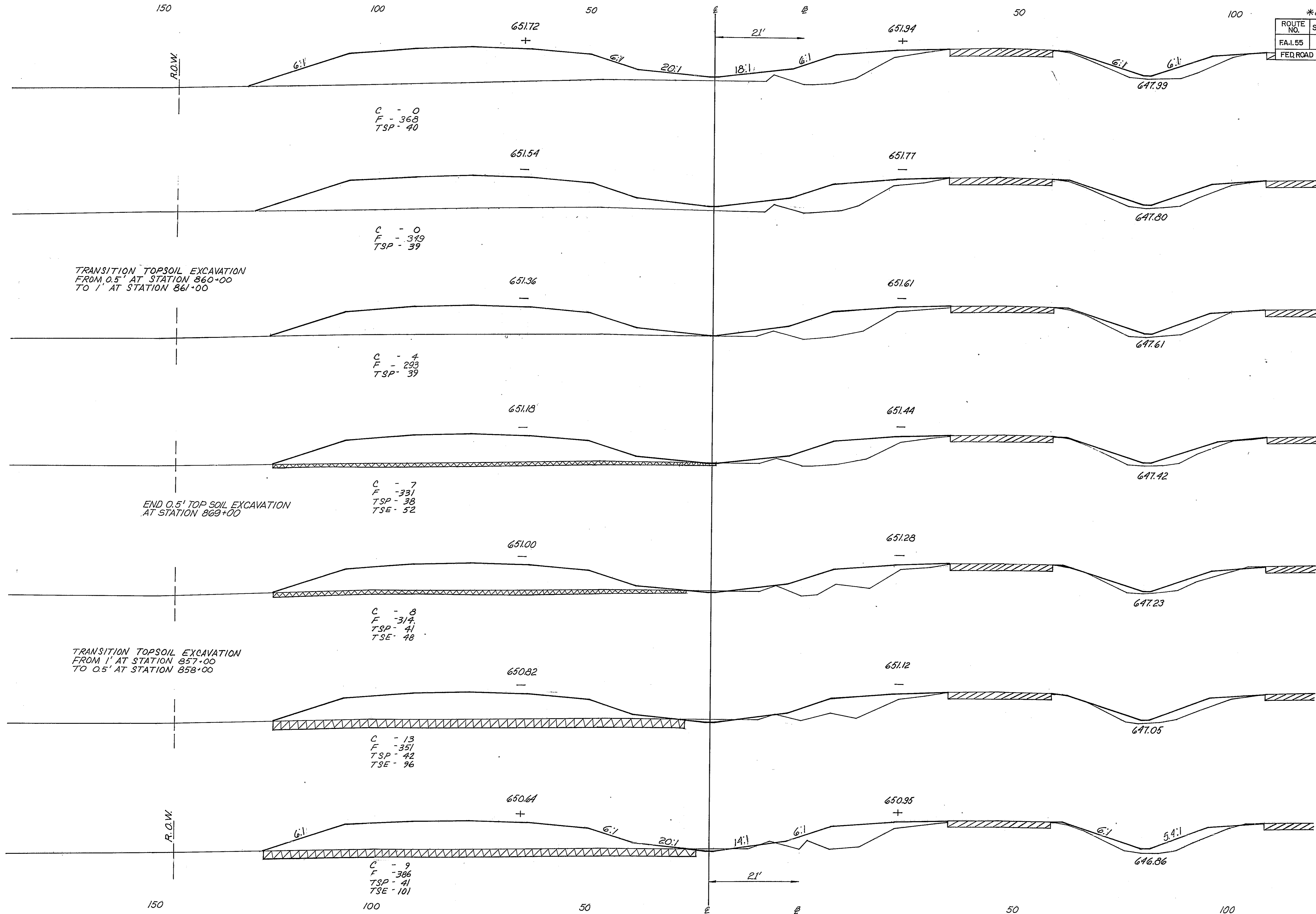


TRANSITION TOPSOIL EXCAVATION
FROM 0.5' AT STATION 852+00
TO 1' AT STATION 853+00

FA.I. 55



*68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	112
FED. ROAD DIST. NO. # ILLINOIS PROJ.			650	852 00



C - 0
F - 368
TSP - 40

C - 0
F - 349
TSP - 39

TRANSITION TOPSOIL EXCAVATION
FROM 0.5' AT STATION 860+00
TO 1' AT STATION 861+00

C - 4
F - 293
TSP - 39

END 0.5' TOP SOIL EXCAVATION
AT STATION 869+00

C - 7
F - 331
TSP - 36
TSE - 52

TRANSITION TOPSOIL EXCAVATION
FROM 1' AT STATION 857+00
TO 0.5' AT STATION 858+00

C - 8
F - 314
TSP - 41
TSE - 48

C - 13
F - 351
TSP - 42
TSE - 96

C - 9
F - 386
TSP - 41
TSE - 101

852 00

851 00

850 00

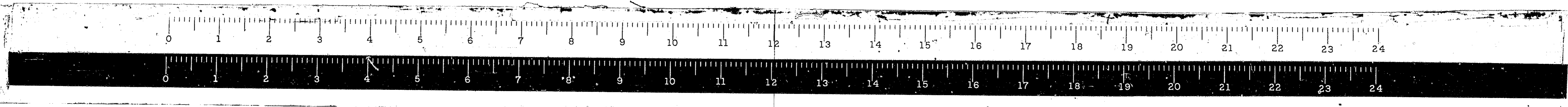
859 00

858 00

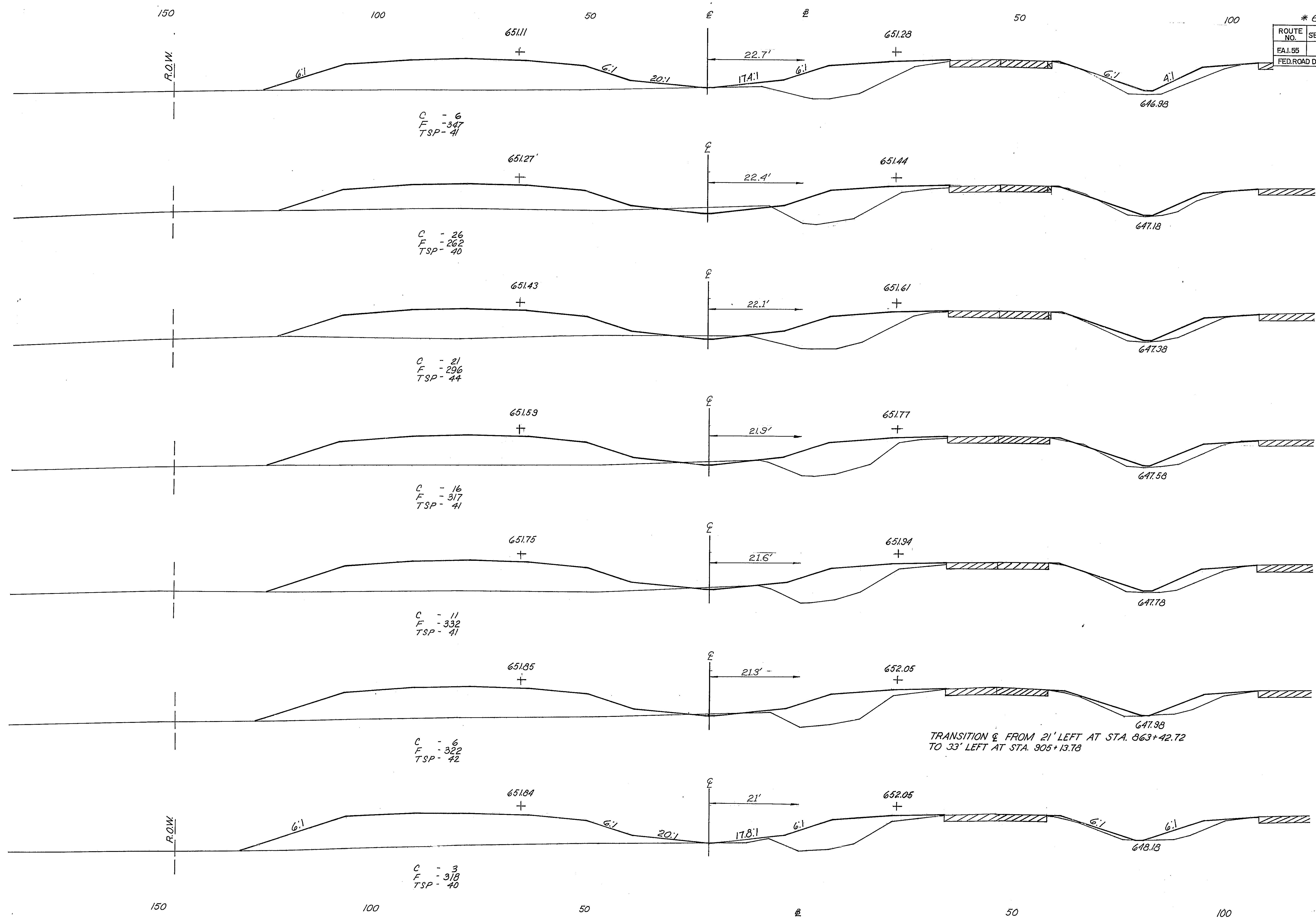
857 00

856 00

FAI.55

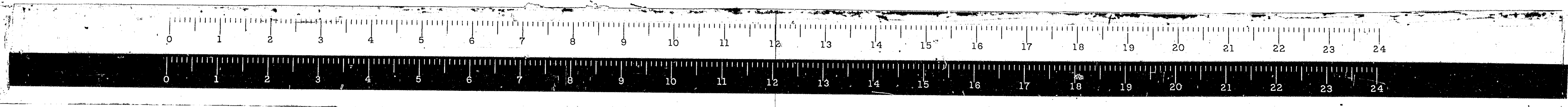


* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MONT.	206	113
FED. ROAD DIST. NO. 4 ILLINOIS			FROM	650

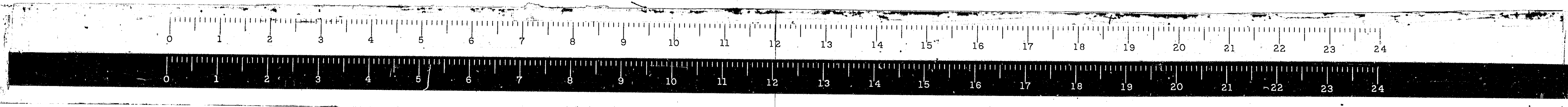
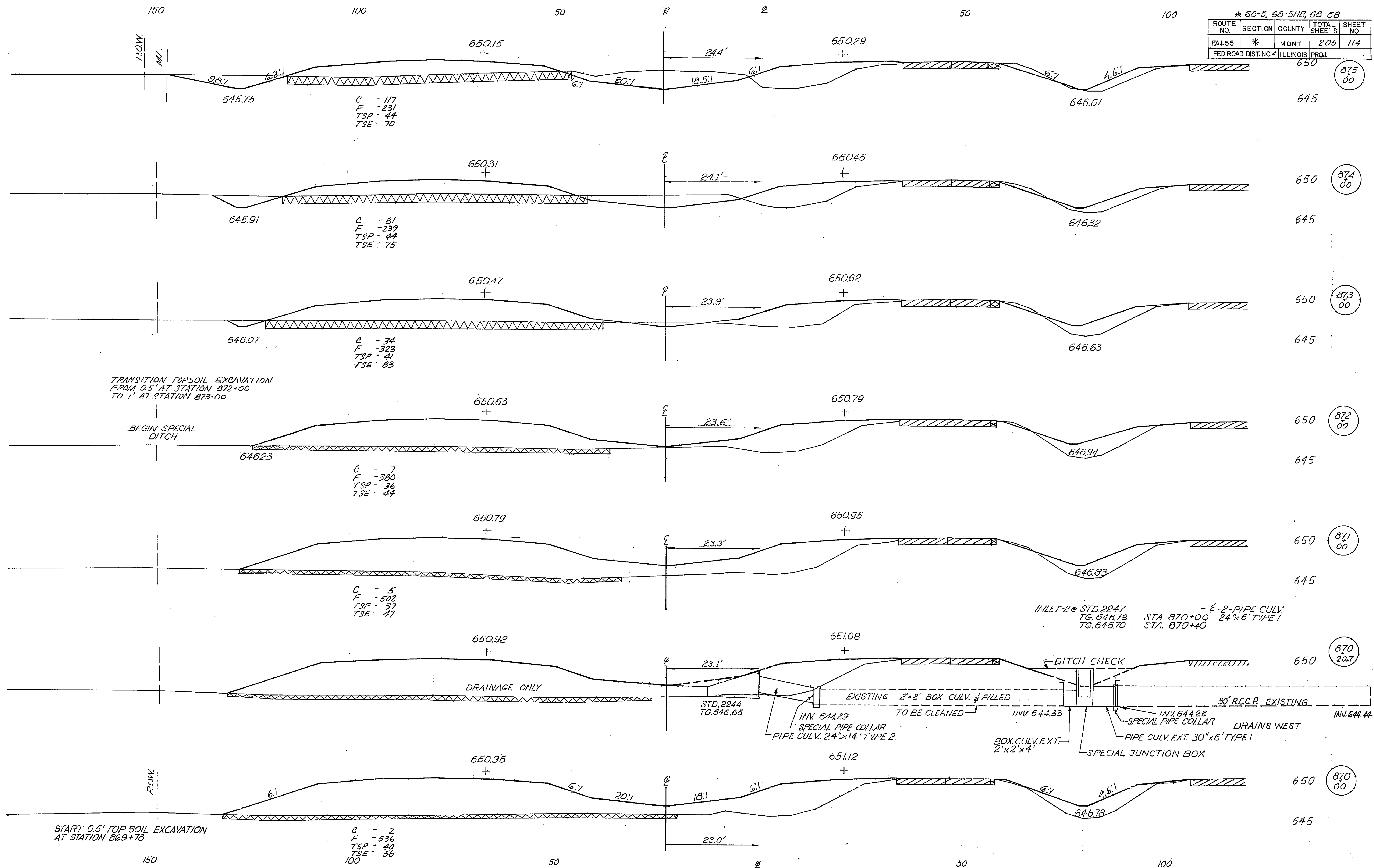


650 (869 00)
645
650 (868 00)
645
650 (867 00)
645
650 (866 00)
645
650 (865 00)
645
650 (864 00)
645
650 (863 00)
645

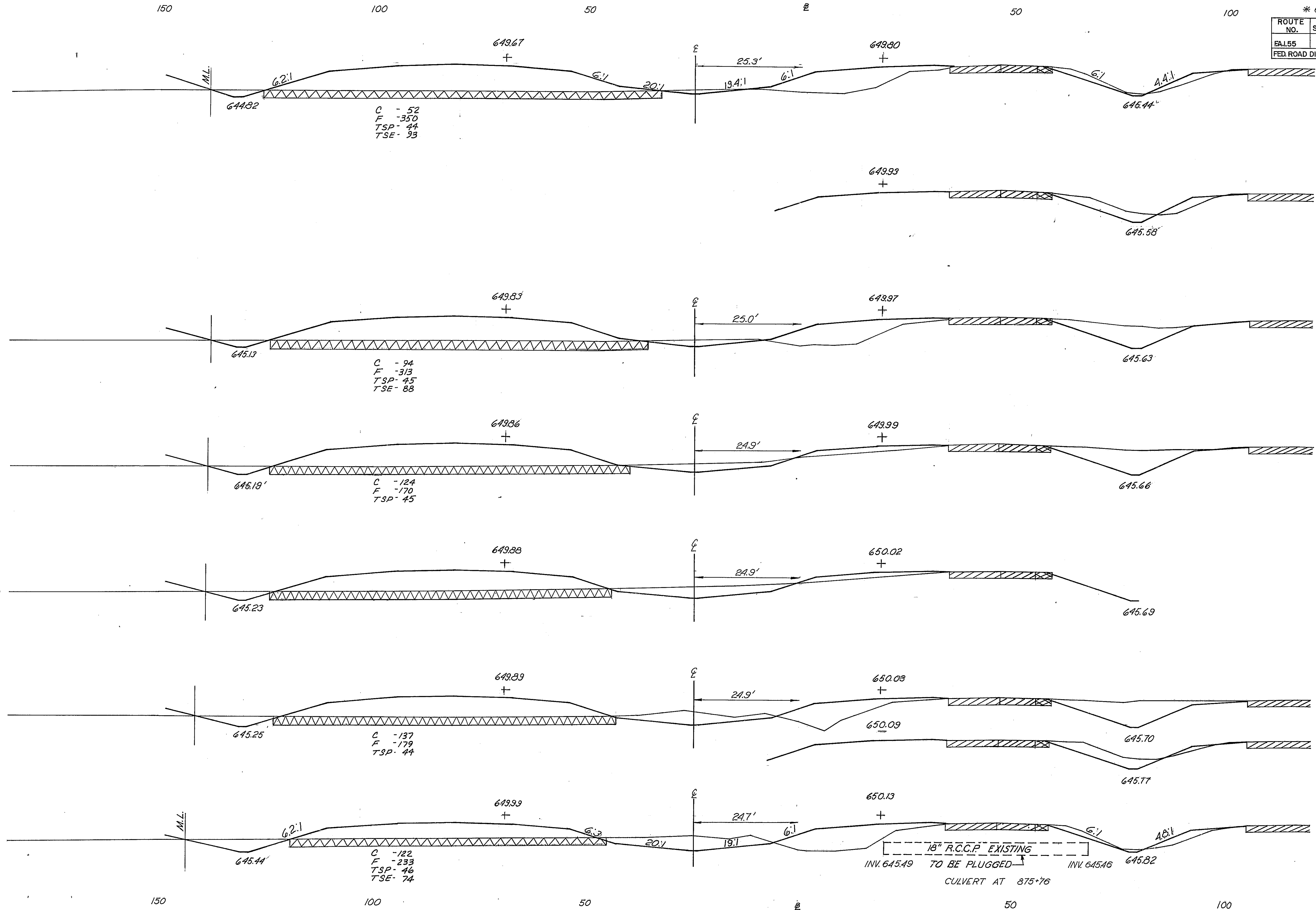
TRANSITION & FROM 21' LEFT AT STA. 863+42.72 TO 33' LEFT AT STA. 905+13.78



* 63-5, 63-5HB, 63-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAL 55	*	MONT	206
FED. ROAD DIST. NO. 1 ILLINOIS PROJ.			114



* 68-5, 68-5A, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI.55	*	MONT.	206
FED. ROAD DIST. NO. # ILLINOIS			PROJ.
			850



645 (878 00)

650

645 (877 25)

650

645 (877 00)

650

645 (876 82)

650

645 (876 67)

650

645 (876 61)

650

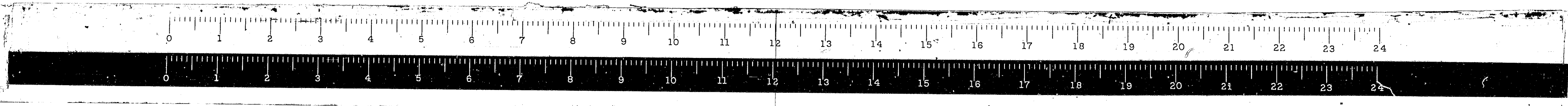
645 (876 25)

650

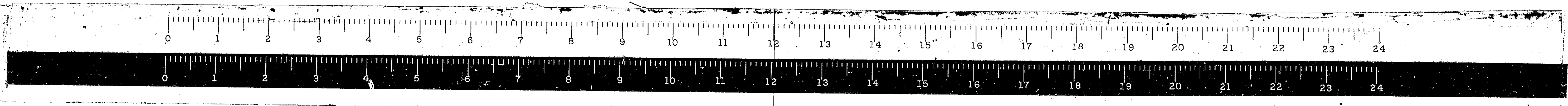
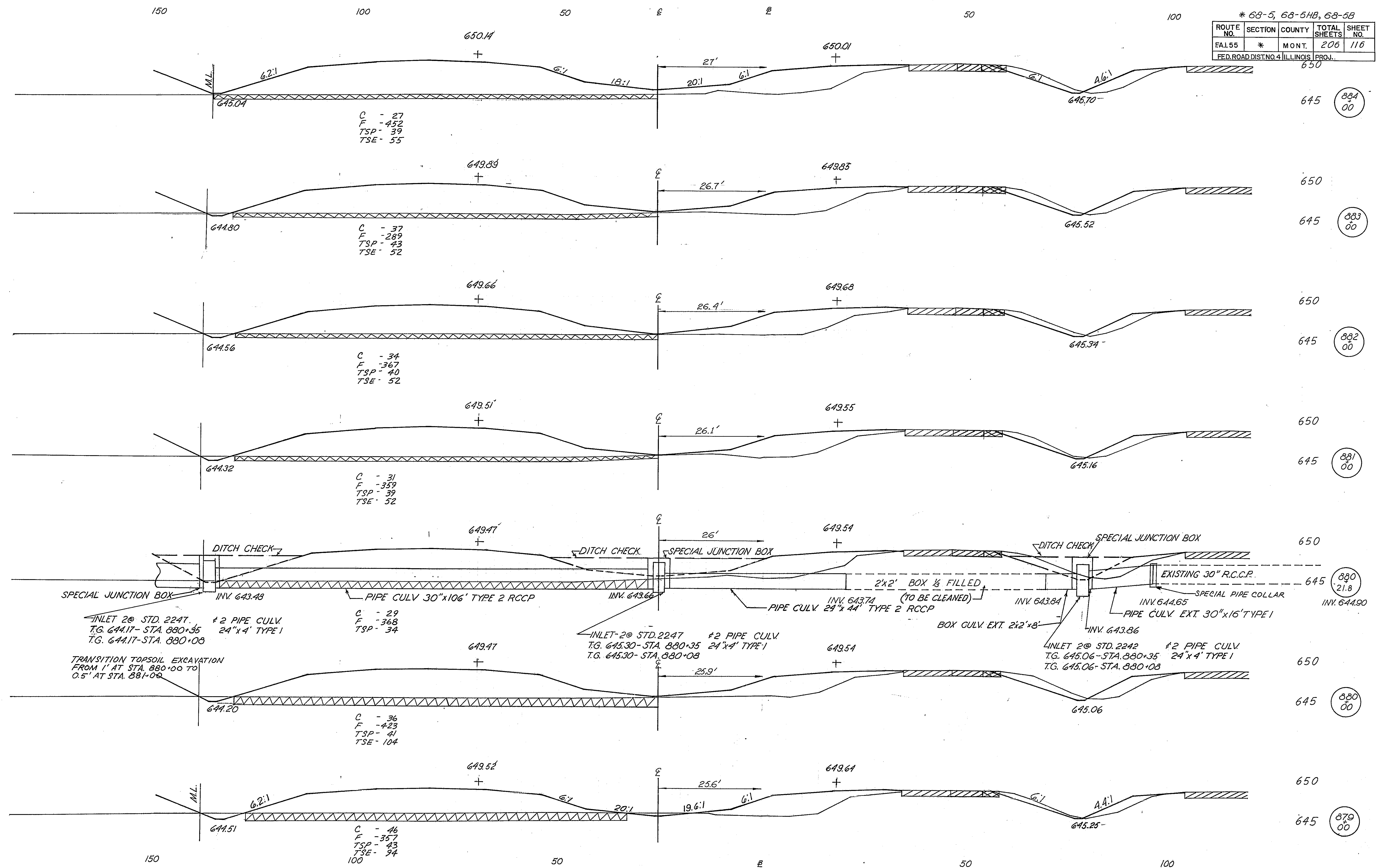
645 (876 00)

645

18" R.C.C.P. EXISTING
TO BE PLUGGED
INV. 645.49 TO BE PLUGGED
INV. 645.46
CULVERT AT 875+76

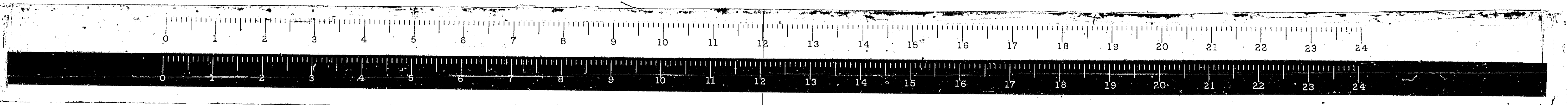
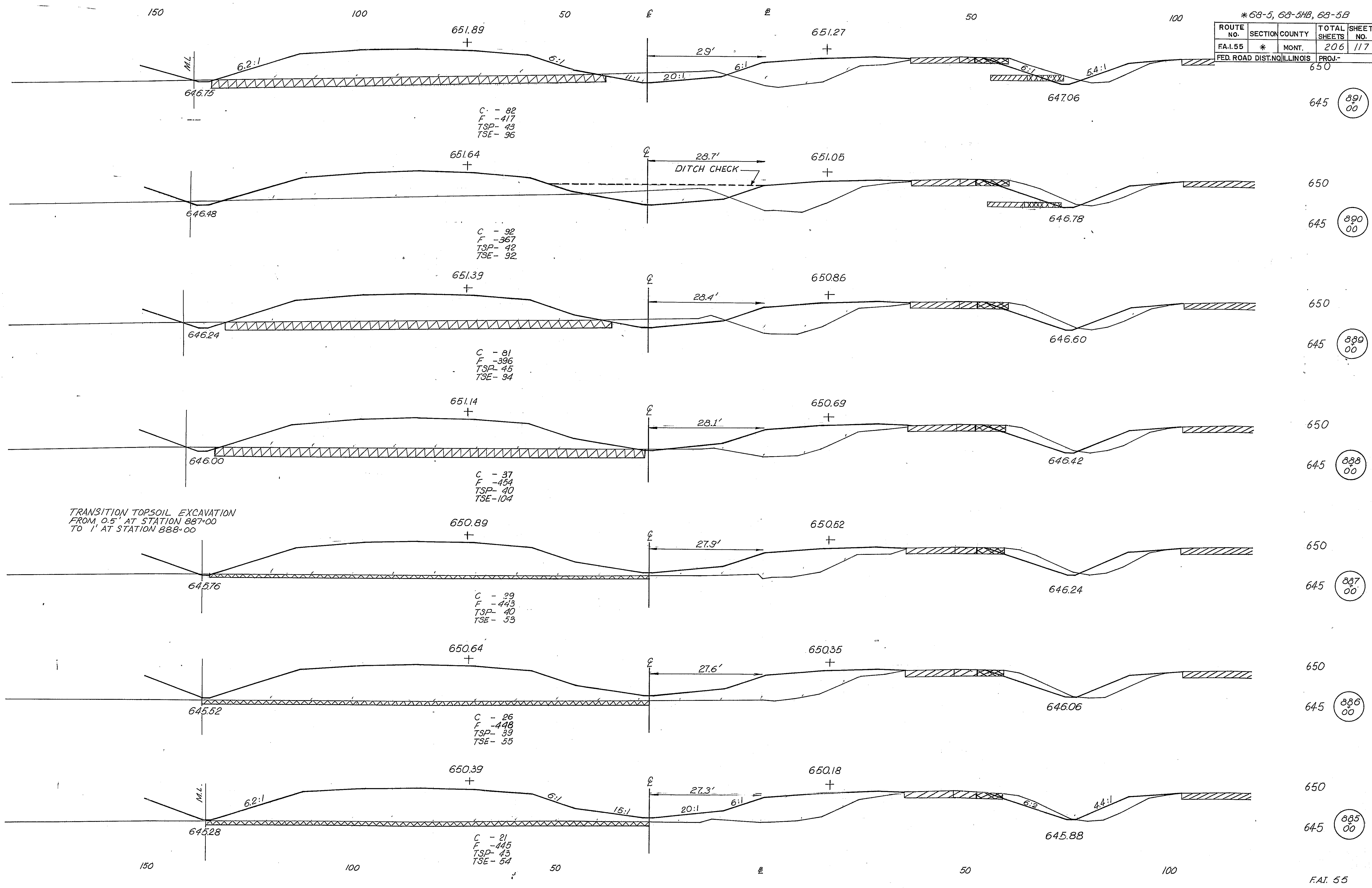


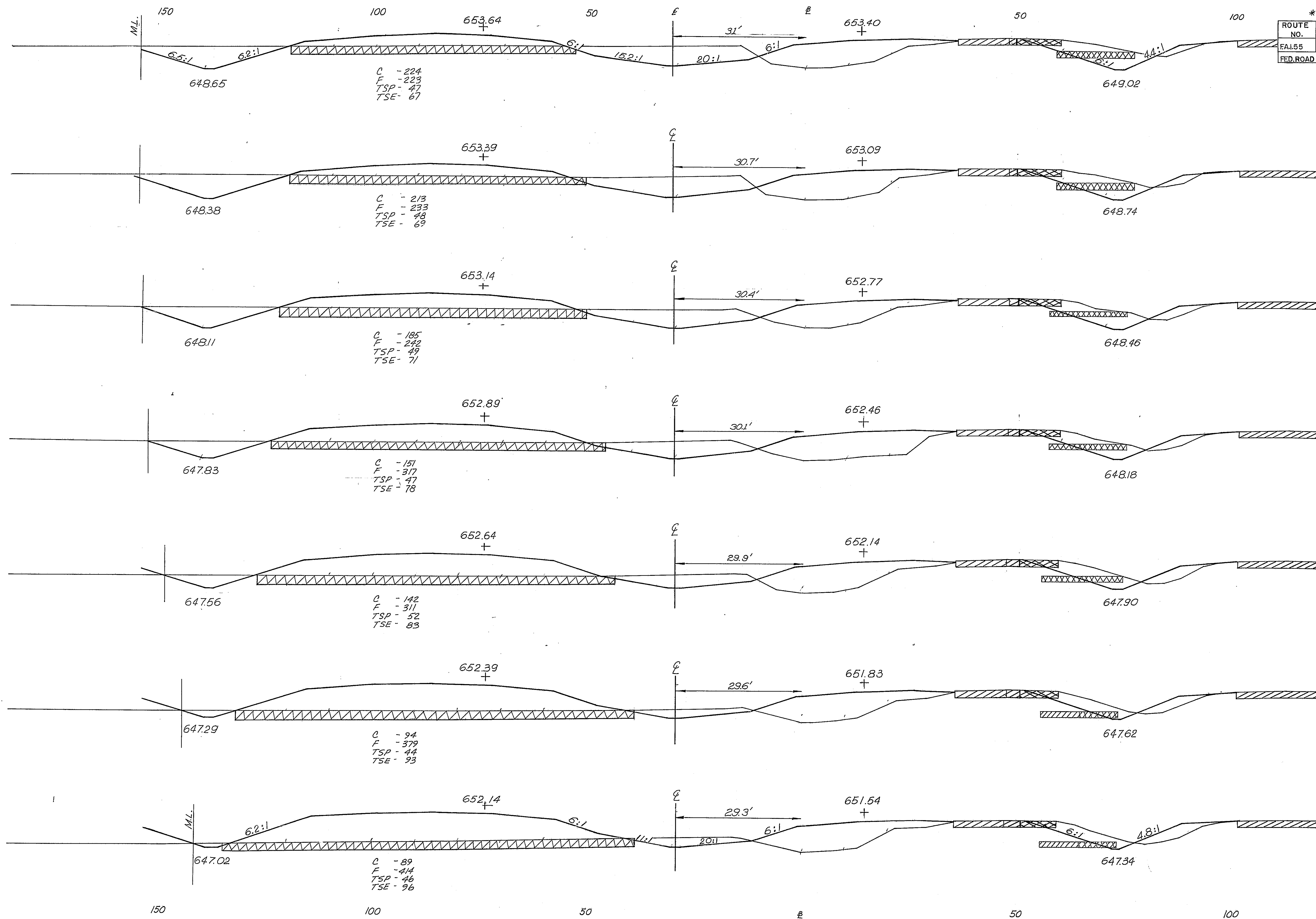
* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	116
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



F.A.I. 55

* 63-5, 63-5B, 63-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEET NO.
FA. 155	*	MONT.	206 / 117
FED. ROAD DIST. NO. ILLINOIS			PROJ. 650





* 63-5, 63-5HB, 63-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	118
FED. ROAD DIST. NO. 6 ILLINOIS			PROJ.	650

898
00

655

897
00

655

896
00

655

895
00

655

894
00

655

893
00

655

892
00

150

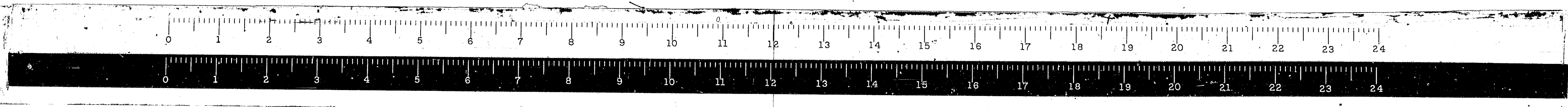
100

50

CL

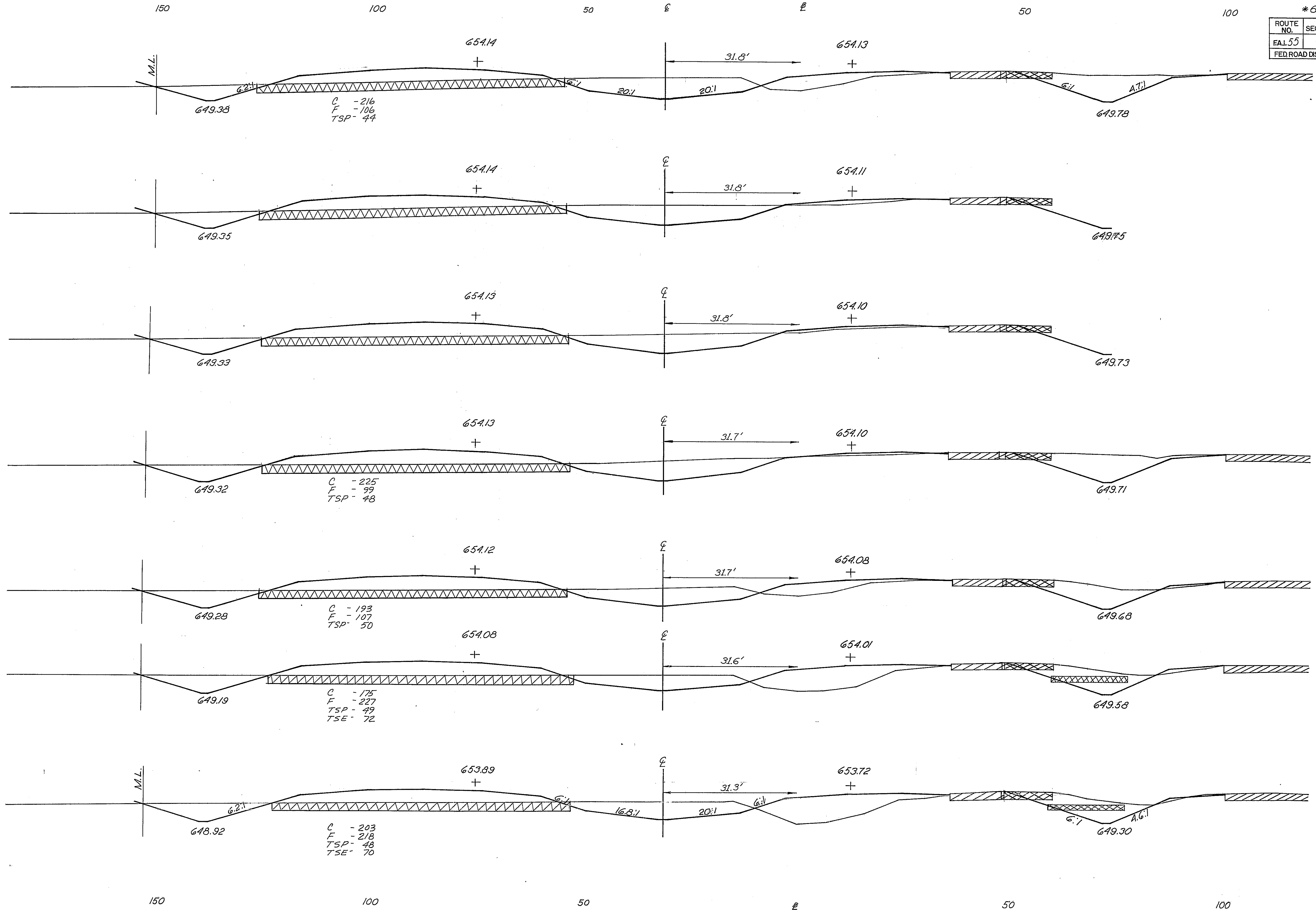
50

100



* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I. 55	*	MONT.	206	119
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	655



650 (900 72)

655 (900 60)

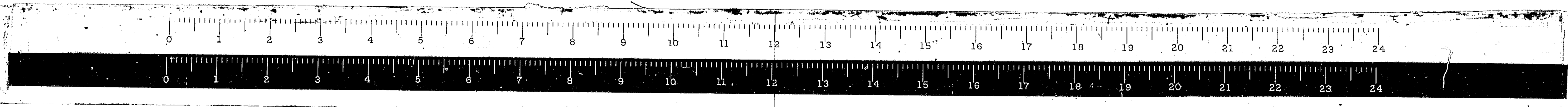
650 (900 53)

650 (900 47)

650 (900 34)

650 (900 00)

650 (899 00)



150

100

50

E

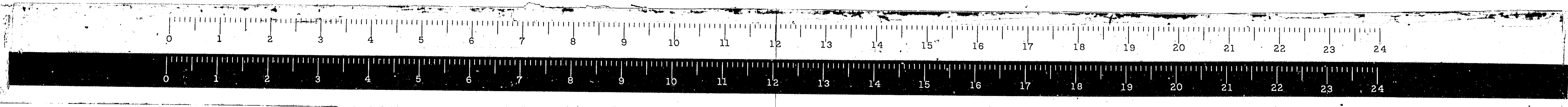
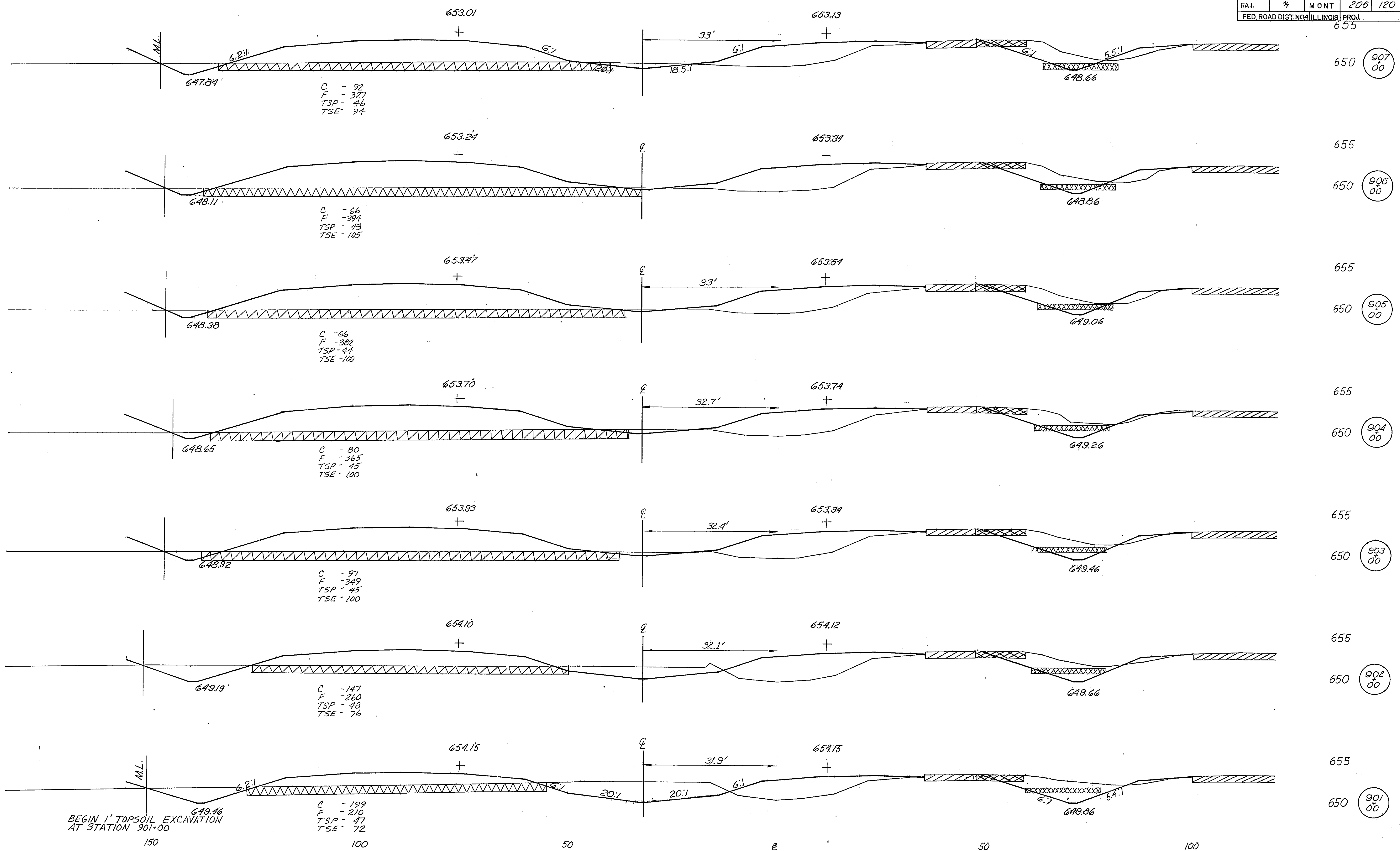
E

50

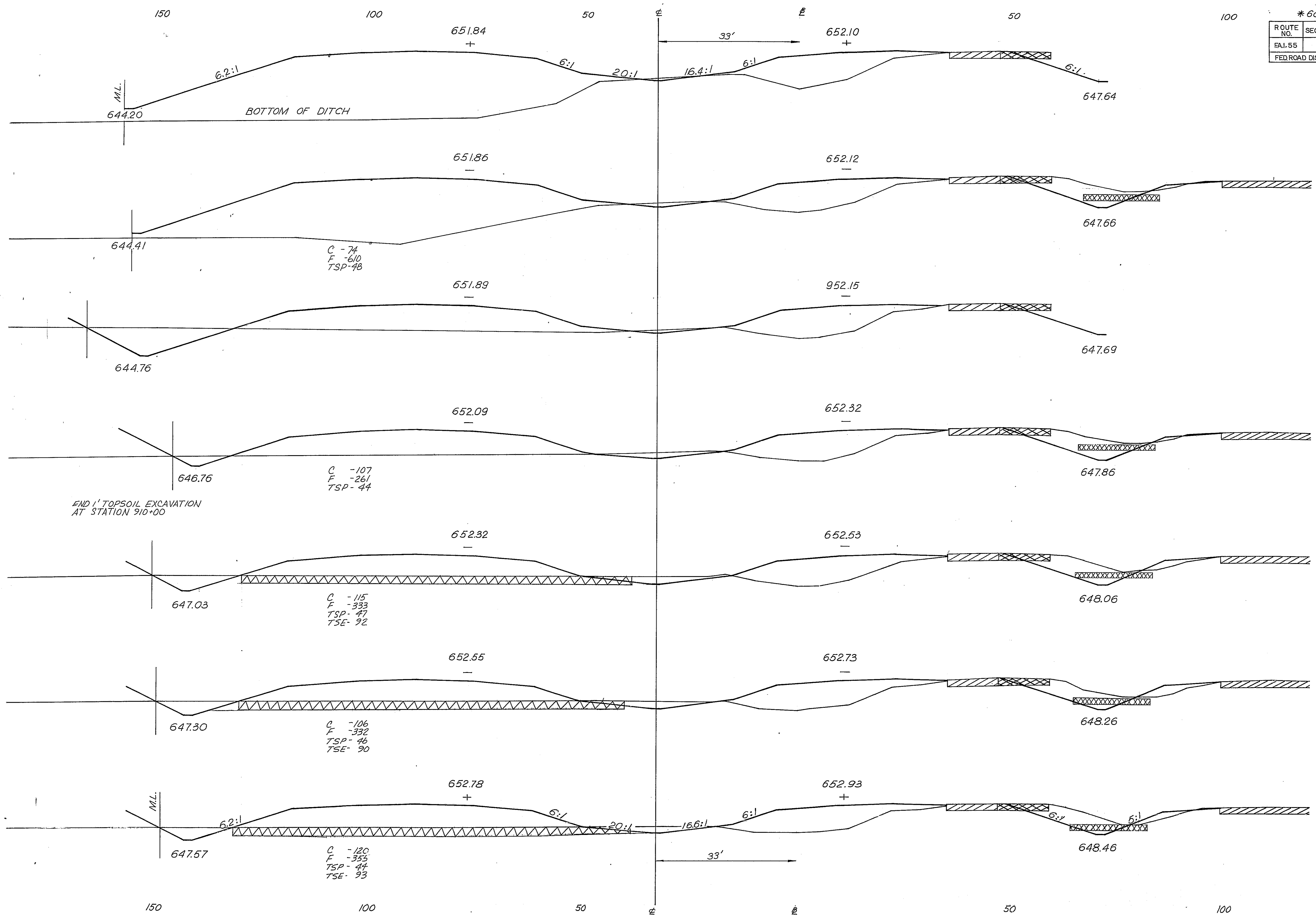
100

* 68-5, 68-54B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I.	*	MONT	206	120
FED. ROAD DIST. NO. ILLINOIS PROJ. 655				



* 68-5, 68-5HB, 68-5HB			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI.55	*	MON T.	206
FED. ROAD DIST. NO. 4			ILLINOIS PROJ.
			650



9/2 09

655

650 9/2 00

655

650 9/1 85

655

650 9/1 00

655

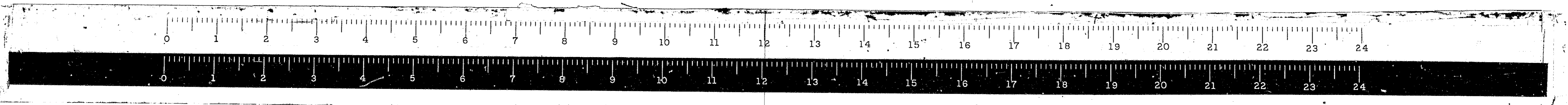
650 9/0 00

655

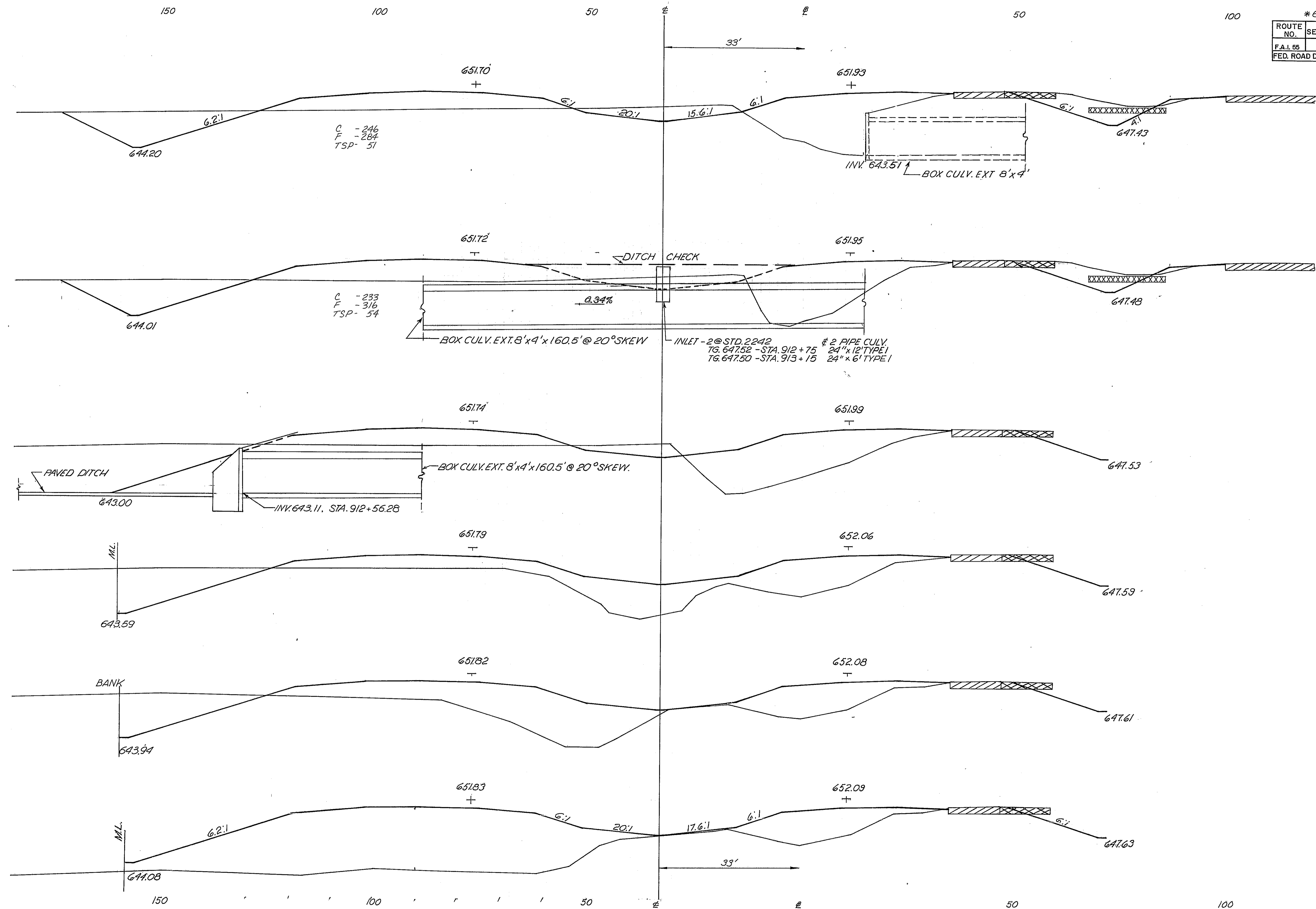
650 9/0 00

655

650 9/0 00



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	122
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



650 (9/3 15.4)

645

650 (9/2 90)

645

650 (9/2 65)

645

650 (9/2 35)

645

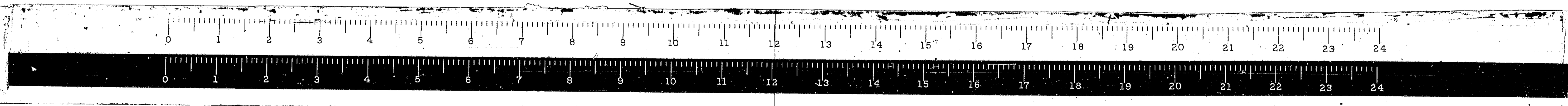
650 (9/2 20)

645

650 (9/2 14)

645

F.A.I. 55



150

100

50

±

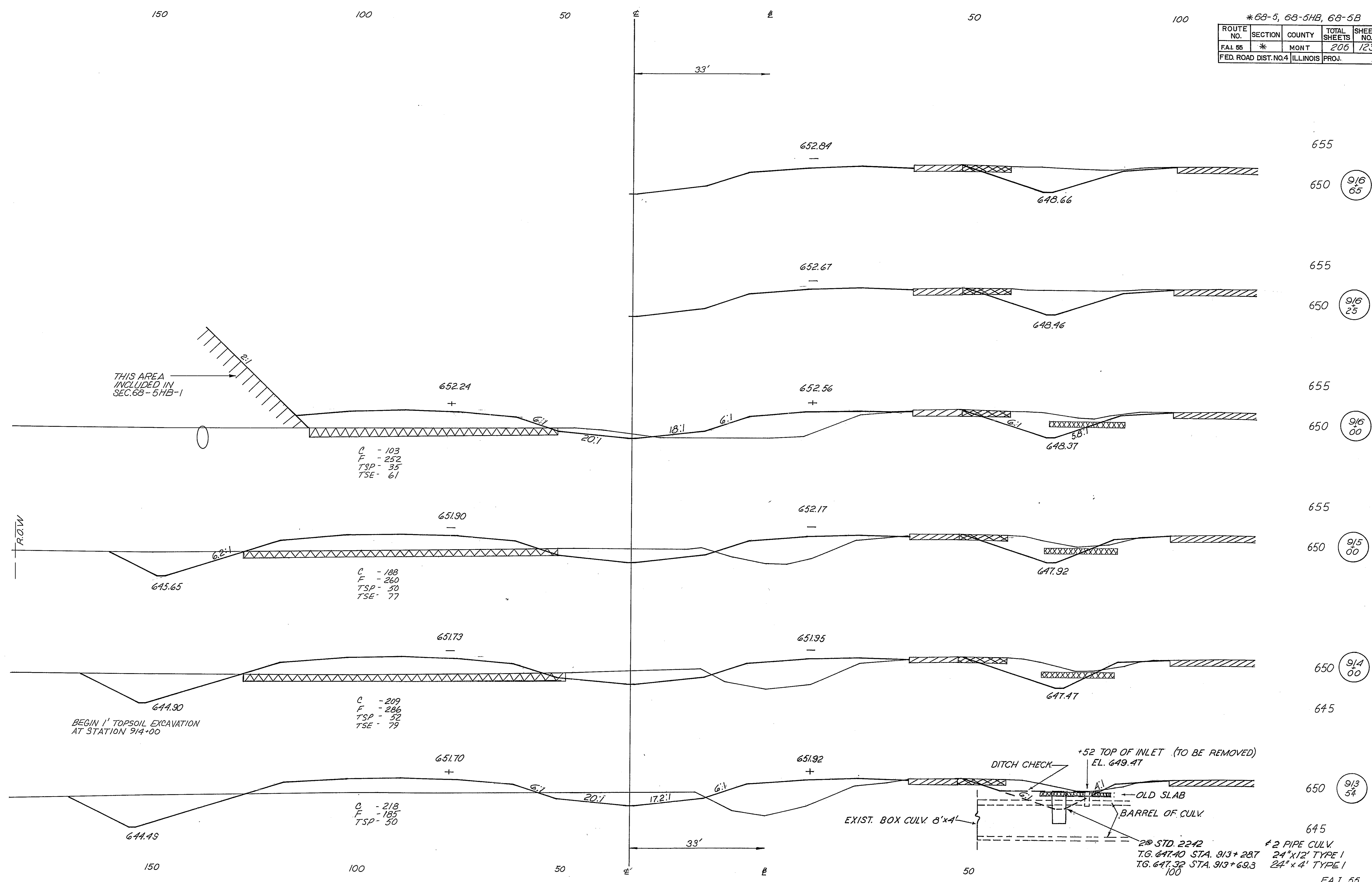
±

50

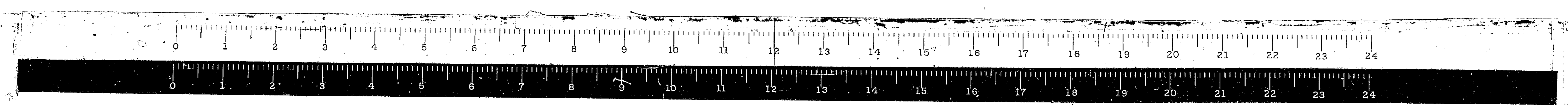
100

* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT	206	123
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

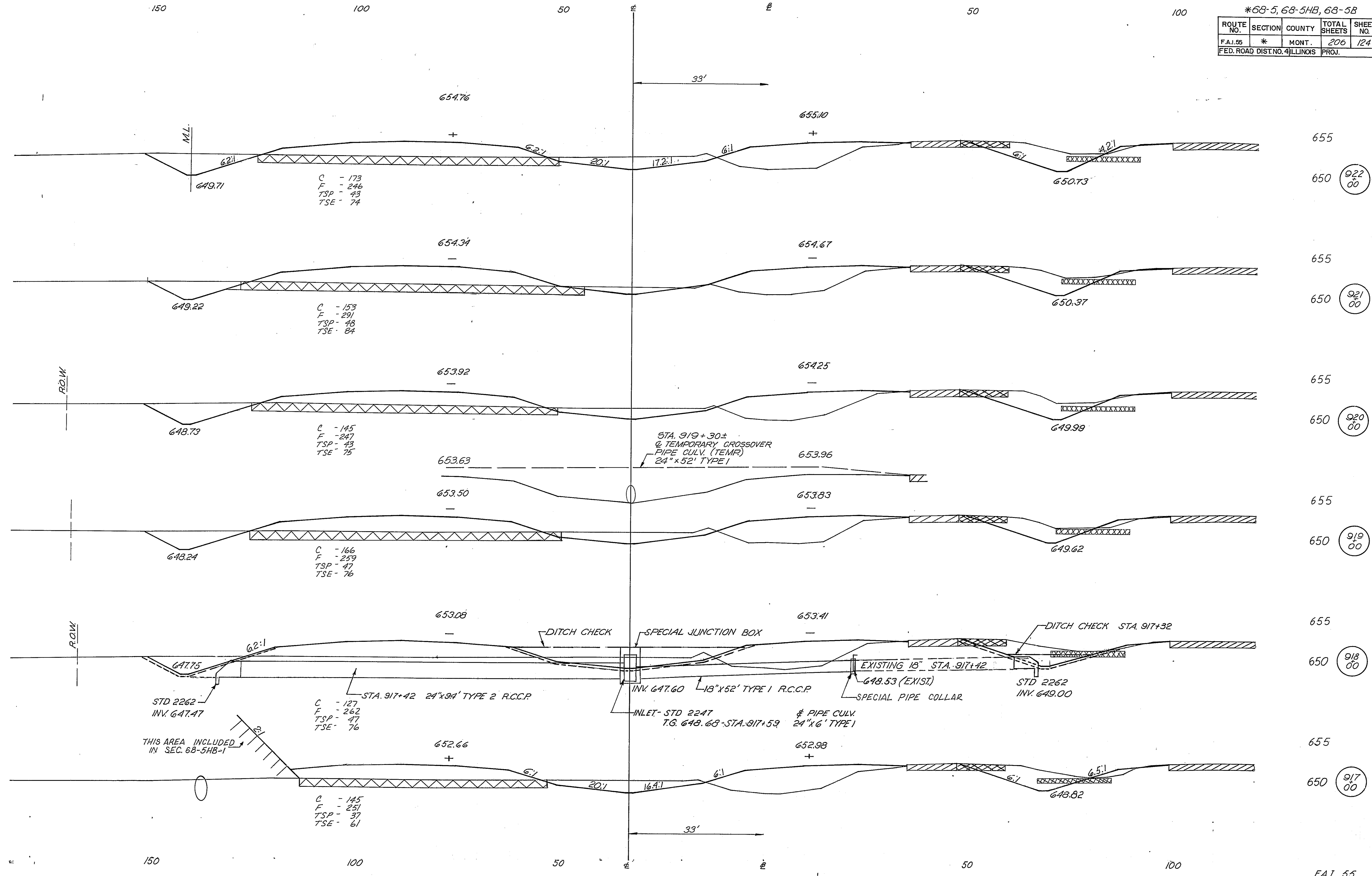


655
 650 (9/6 65)
 655
 650 (9/6 25)
 655
 650 (9/6 00)
 655
 650 (9/5 00)
 655
 650 (9/4 00)
 645
 650 (9/3 54)
 645



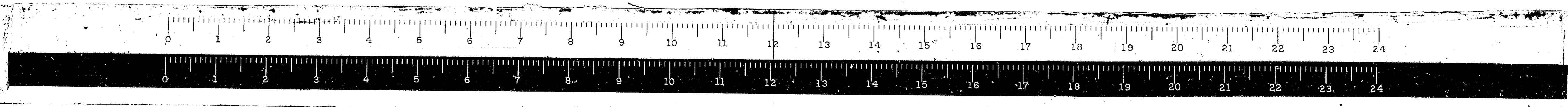
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	124
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	

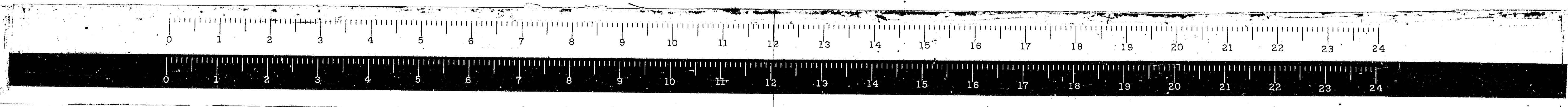
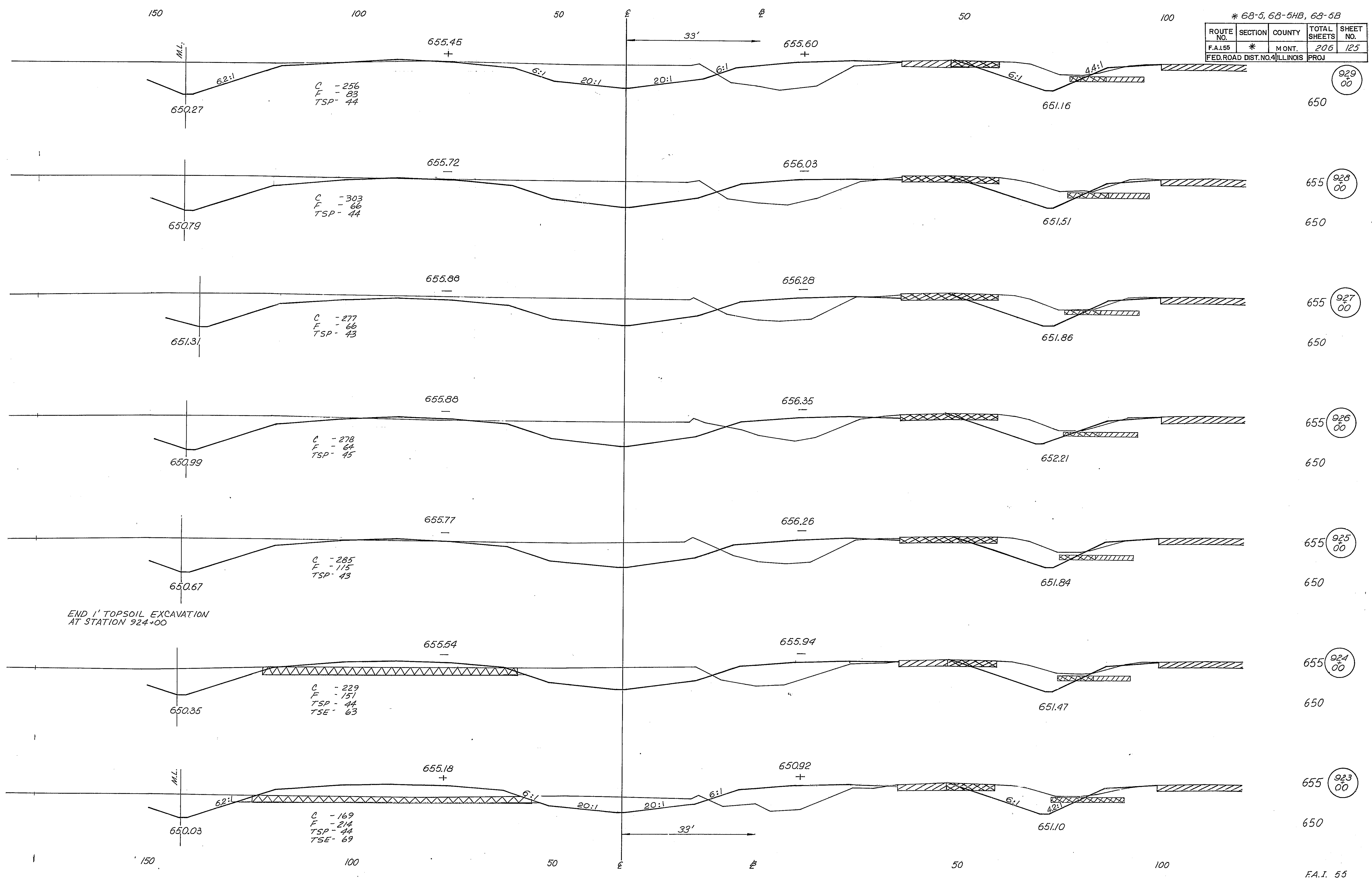


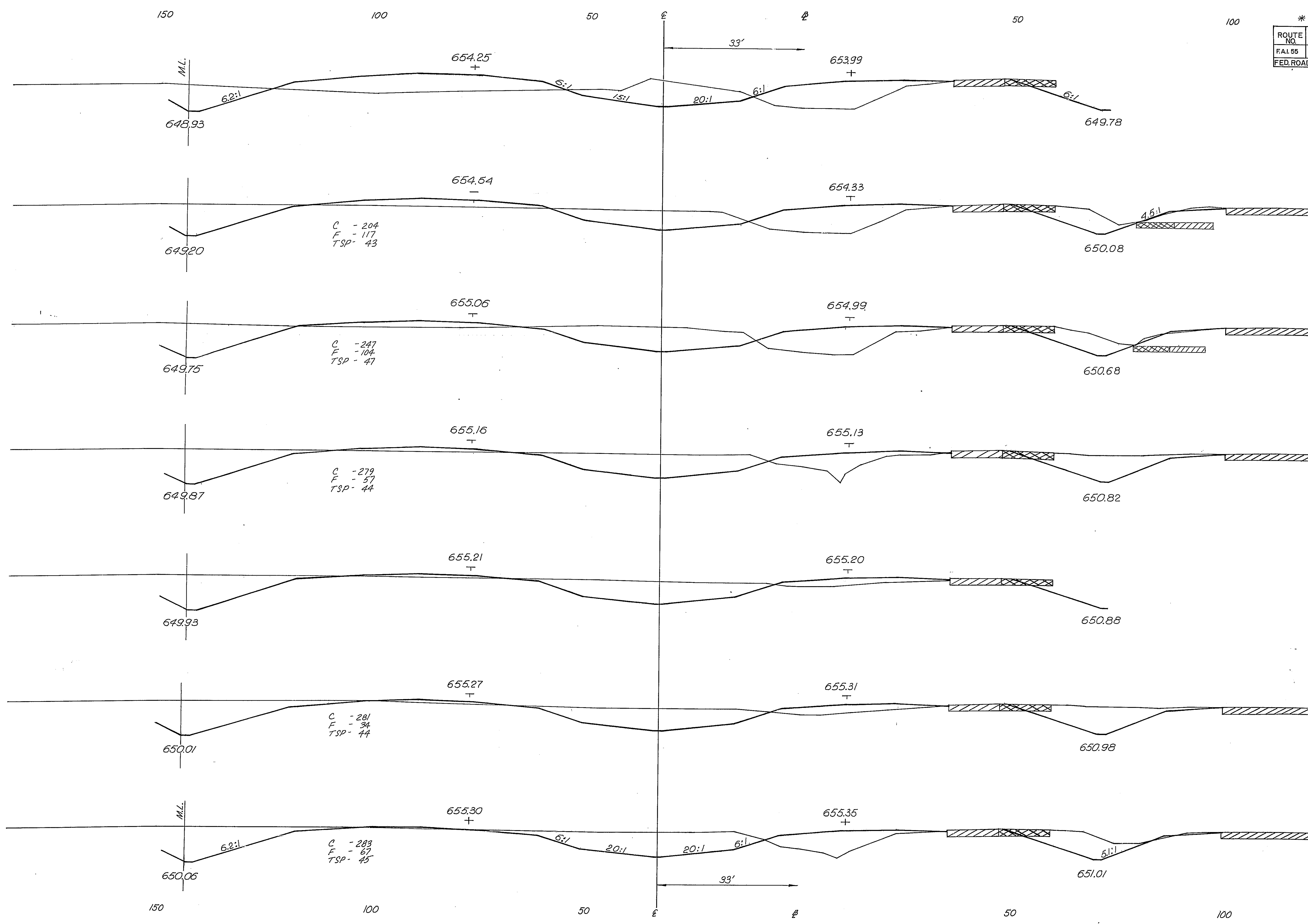
655
650 (922/00)
655
650 (921/00)
655
650 (920/00)
655
650 (919/00)
655
650 (918/00)
655
650 (917/00)

F.A.I. 55



* 68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
F.A. 55	*	MONT.	206
FED. ROAD DIST. NO. 4	ILLINOIS	PROJ.	125

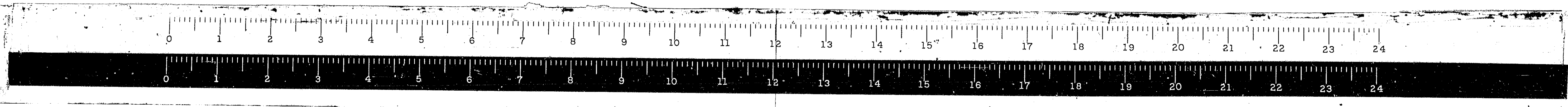




* 68-5, 68-5HB, 68-5B

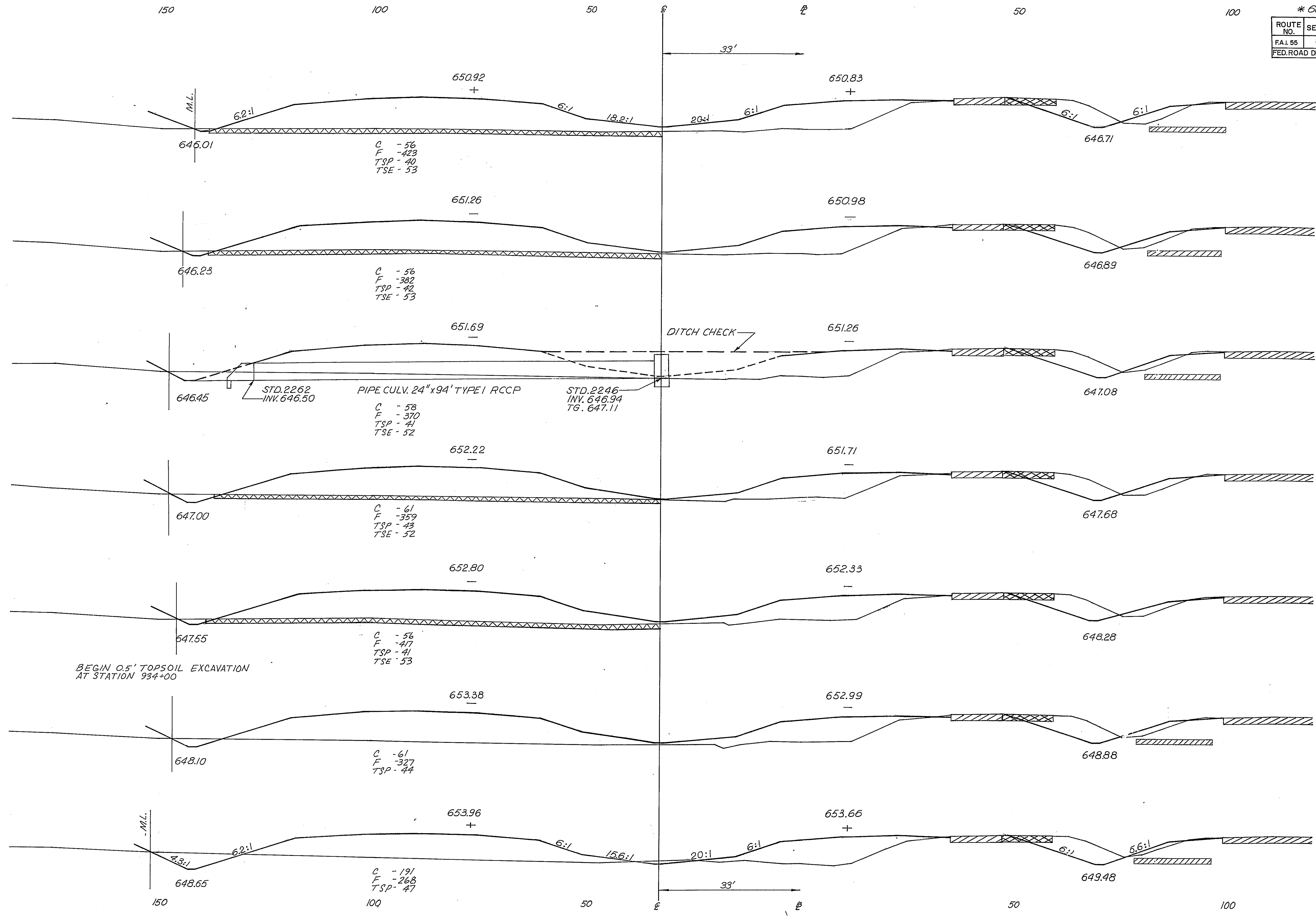
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	200	126
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

655
 650 (93/50)
 655 (93/00)
 650
 655 (93/00)
 650
 655 (929/77)
 650
 655 (929/66)
 650
 655 (929/50)
 650
 655 (929/41.2)
 650



* 68-5, 68-5A, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 65	*	MONT.	206	127
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



650 (938 00)

645

650 (937 00)

645

650 (936 00)

645

650 (935 00)

645

650 (934 00)

645

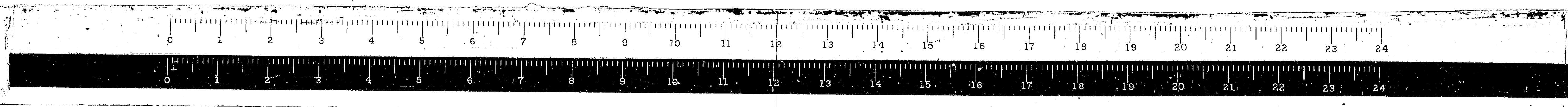
650 (933 00)

645

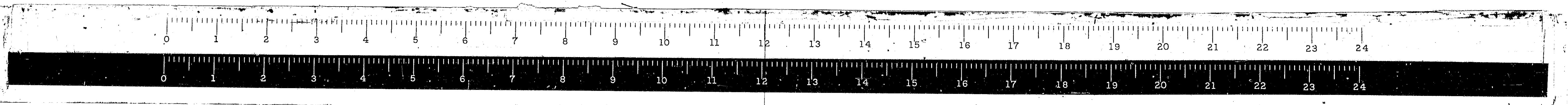
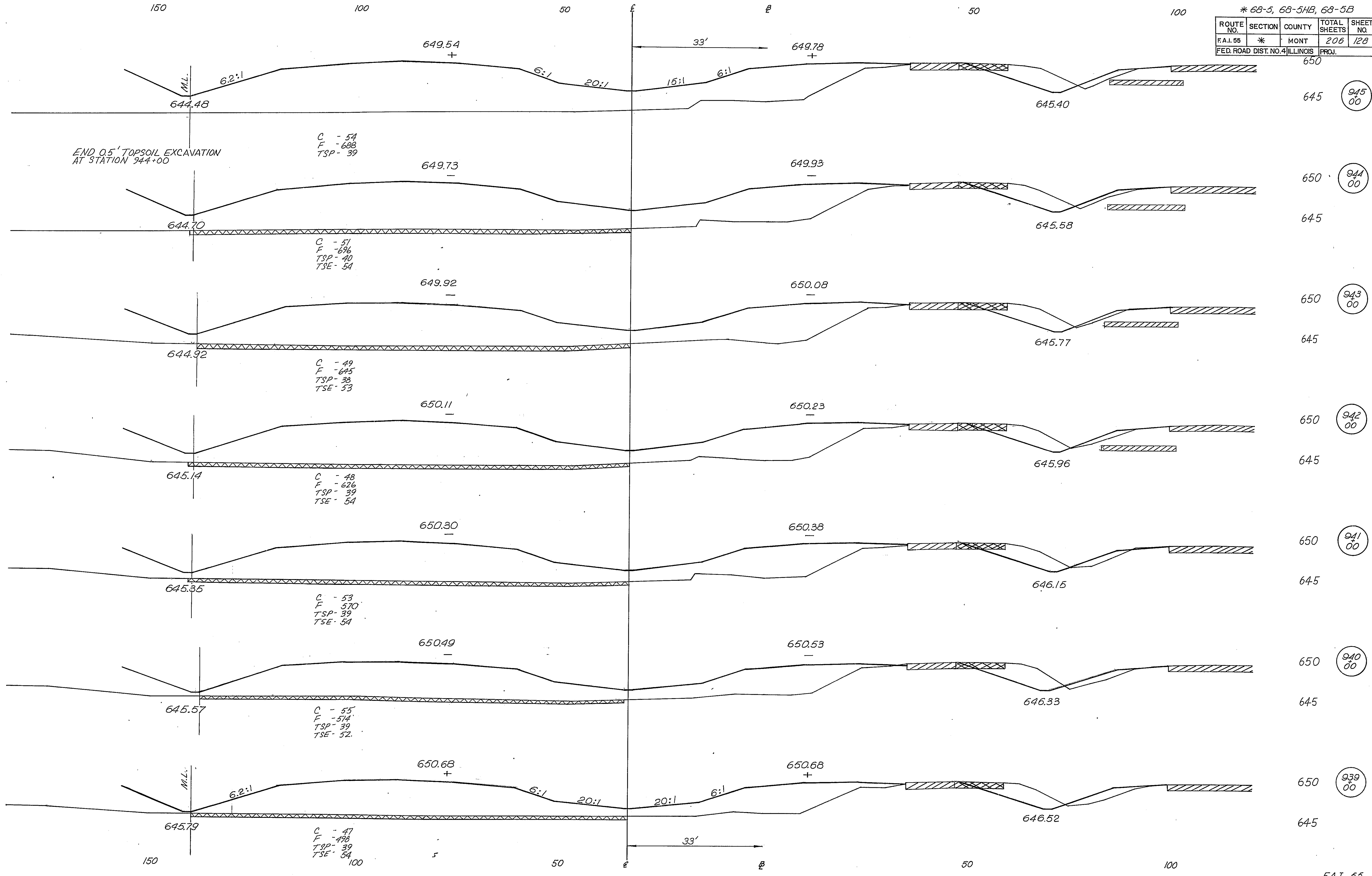
650 (932 00)

645

F.A.I. 65

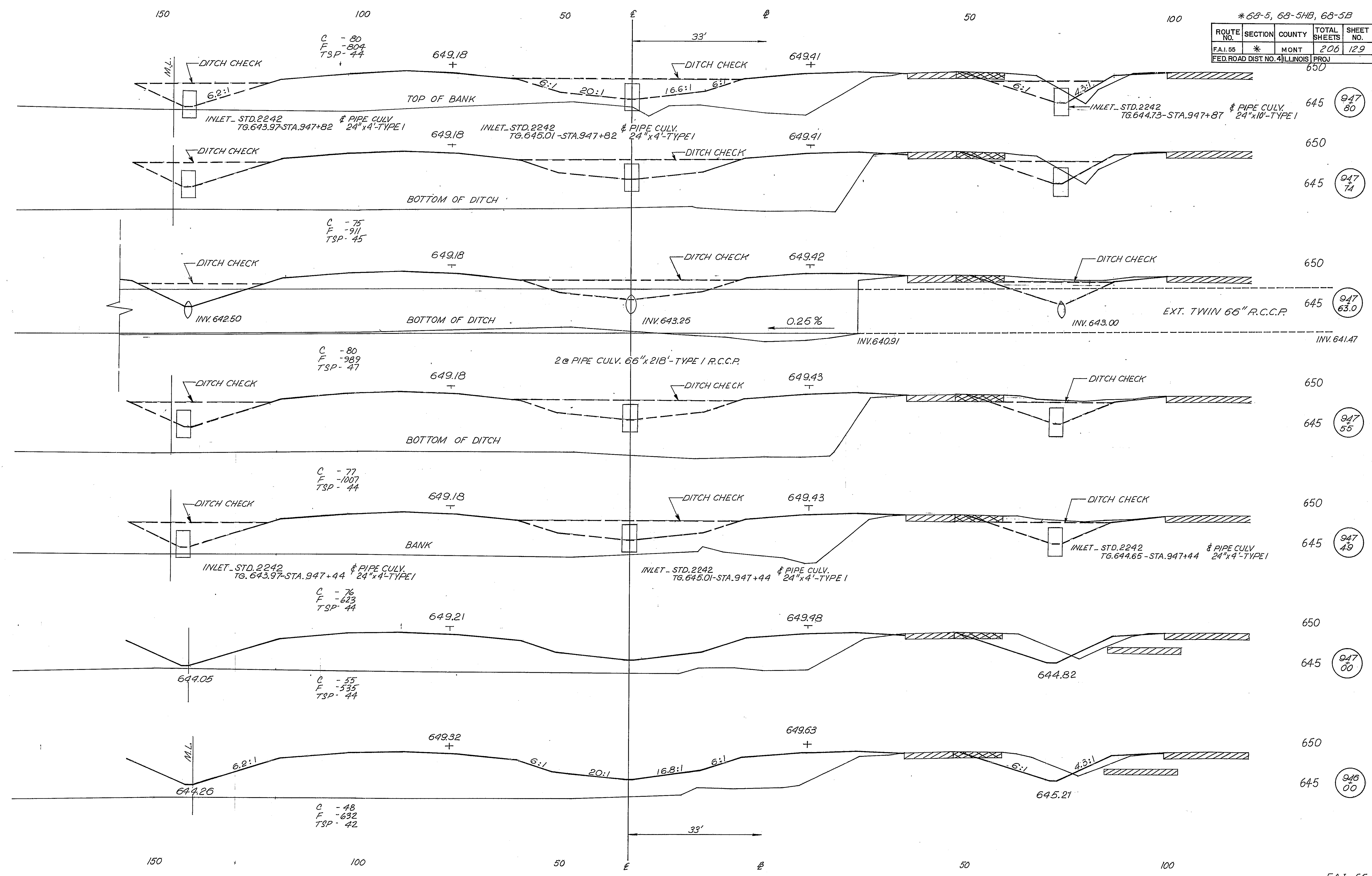


* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT	206	128
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	

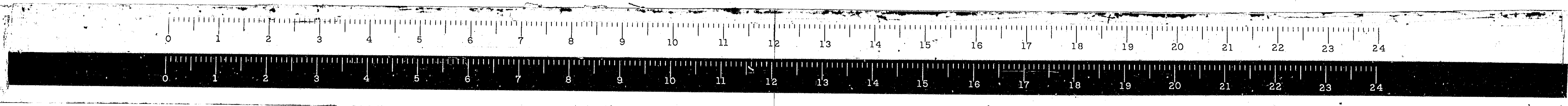


* 68-5, 68-5HB, 68-5B

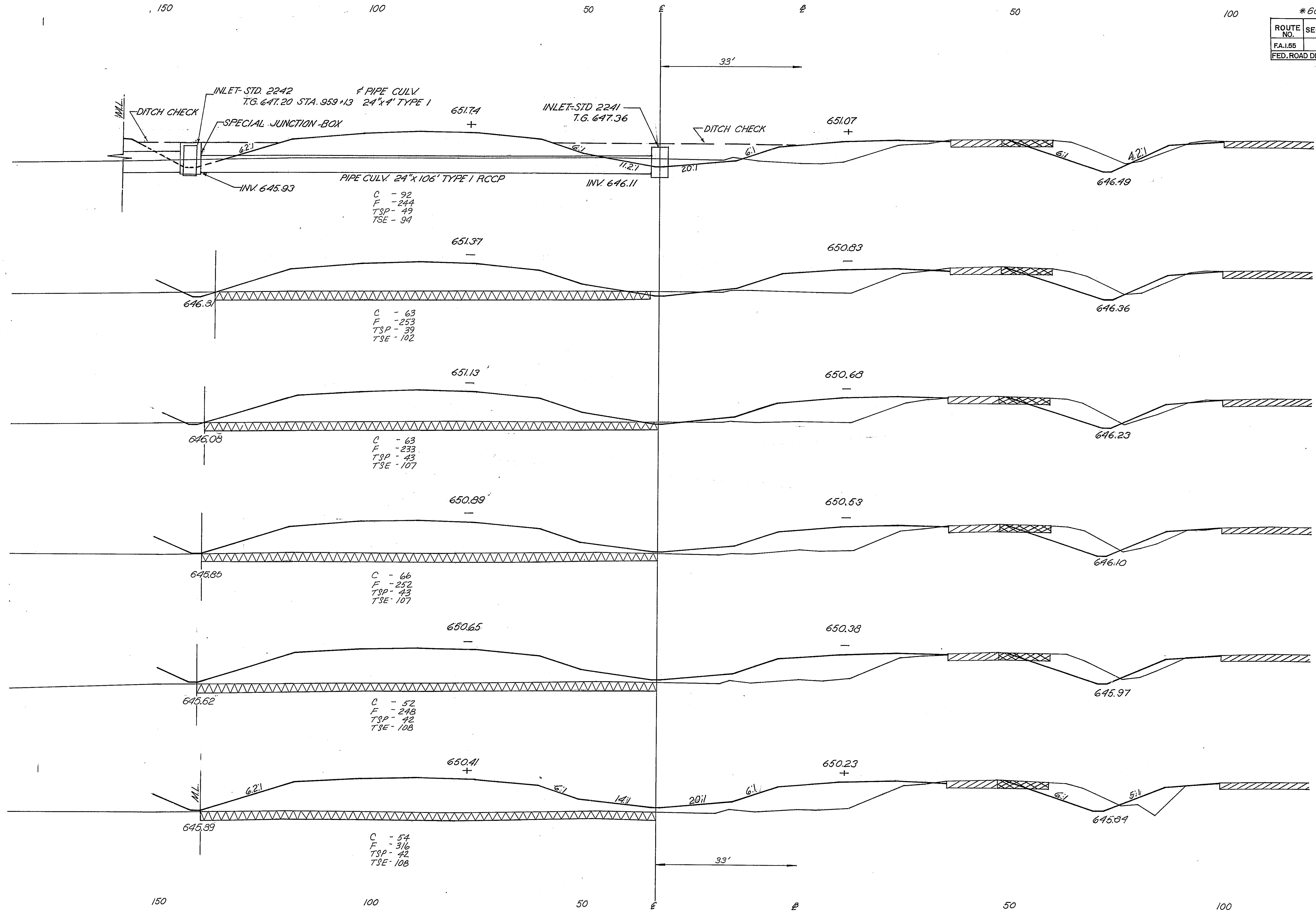
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT	206	129
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.			650	



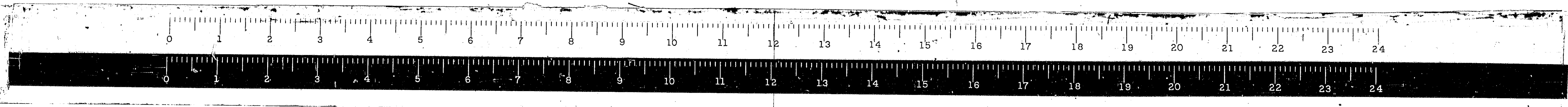
F.A.I. 55



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.165	*	MONT.	206	131
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



655
 650 (959 00)
 655
 650 (958 00)
 655
 650 (957 00)
 655
 650 (956 00)
 655
 650 (955 00)
 655
 650 (954 00)



150

100

50

ℰ

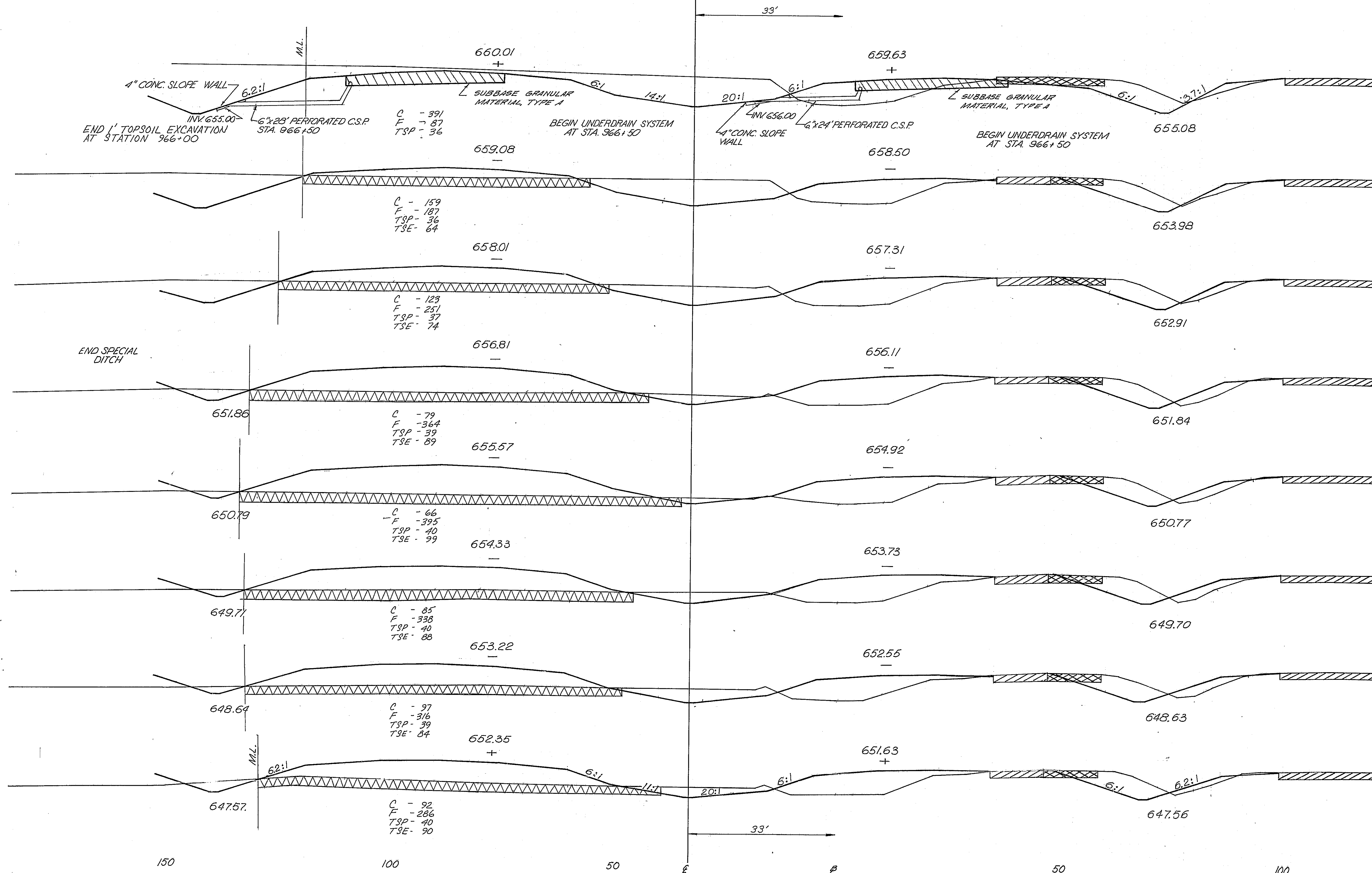
ℰ

50

100

* 63-5, 63-5HB, 63-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI. 55	*	MONT.	206	132
FED. ROAD DIST. NO. 4 ILLINOIS				



665

660 (967 00)

660

655 (966 00)

655 (965 00)

655 (964 00)

650 (963 00)

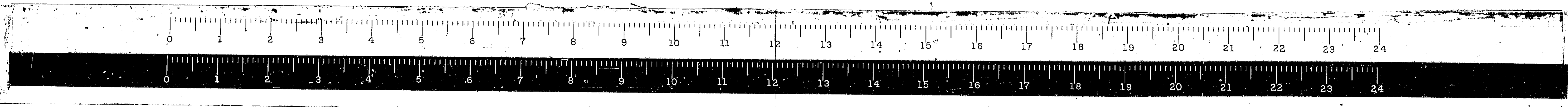
650 (962 00)

650 (961 00)

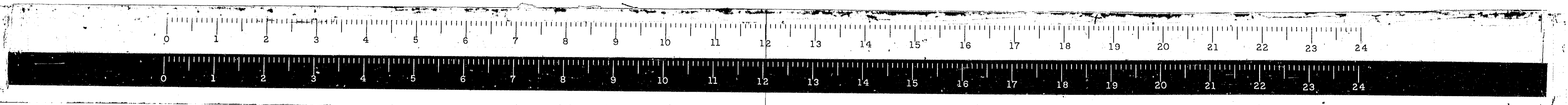
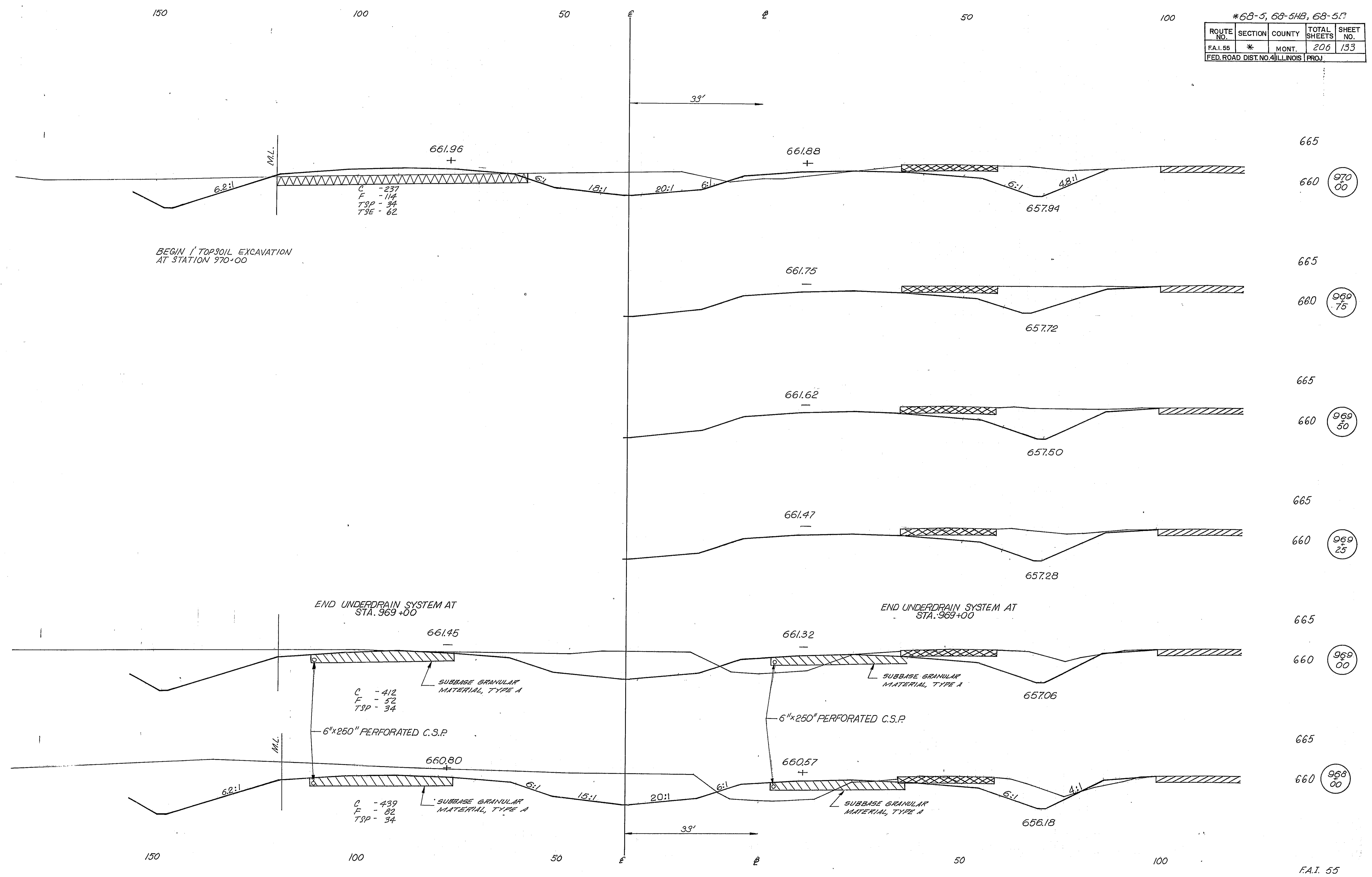
655

650 (960 00)

FAI. 55

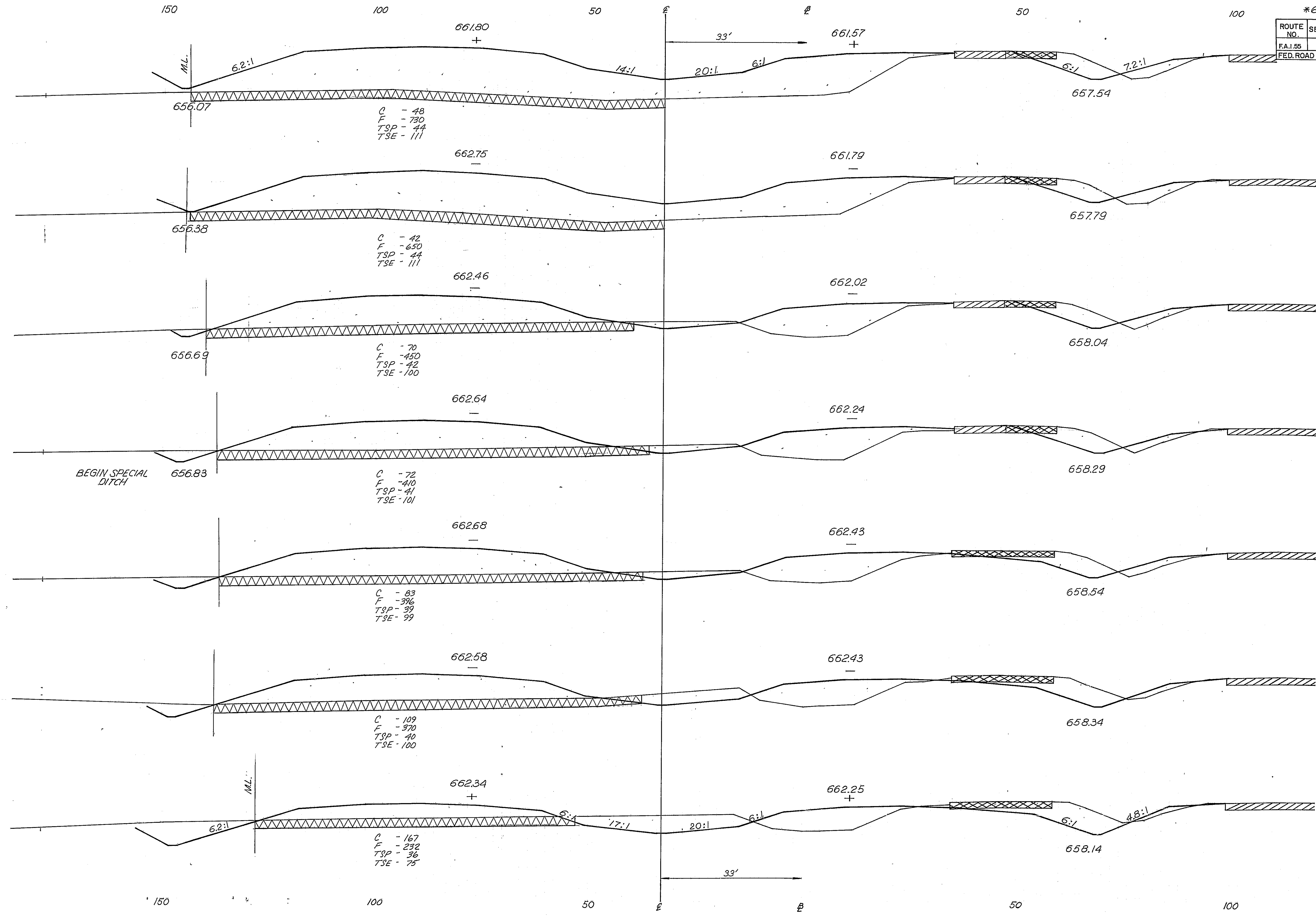


*68-5, 68-5HB, 68-5.7				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I. 55	*	MONT.	206	133
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	134
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



977
00

976
00

975
00

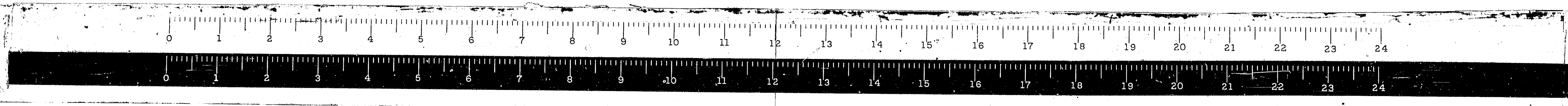
974
00

973
00

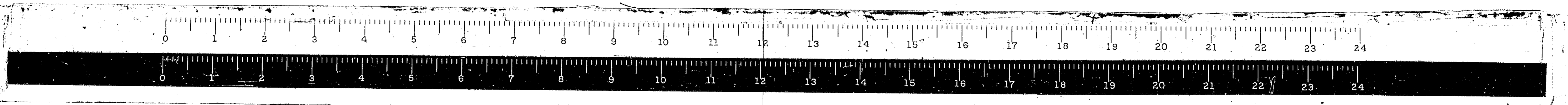
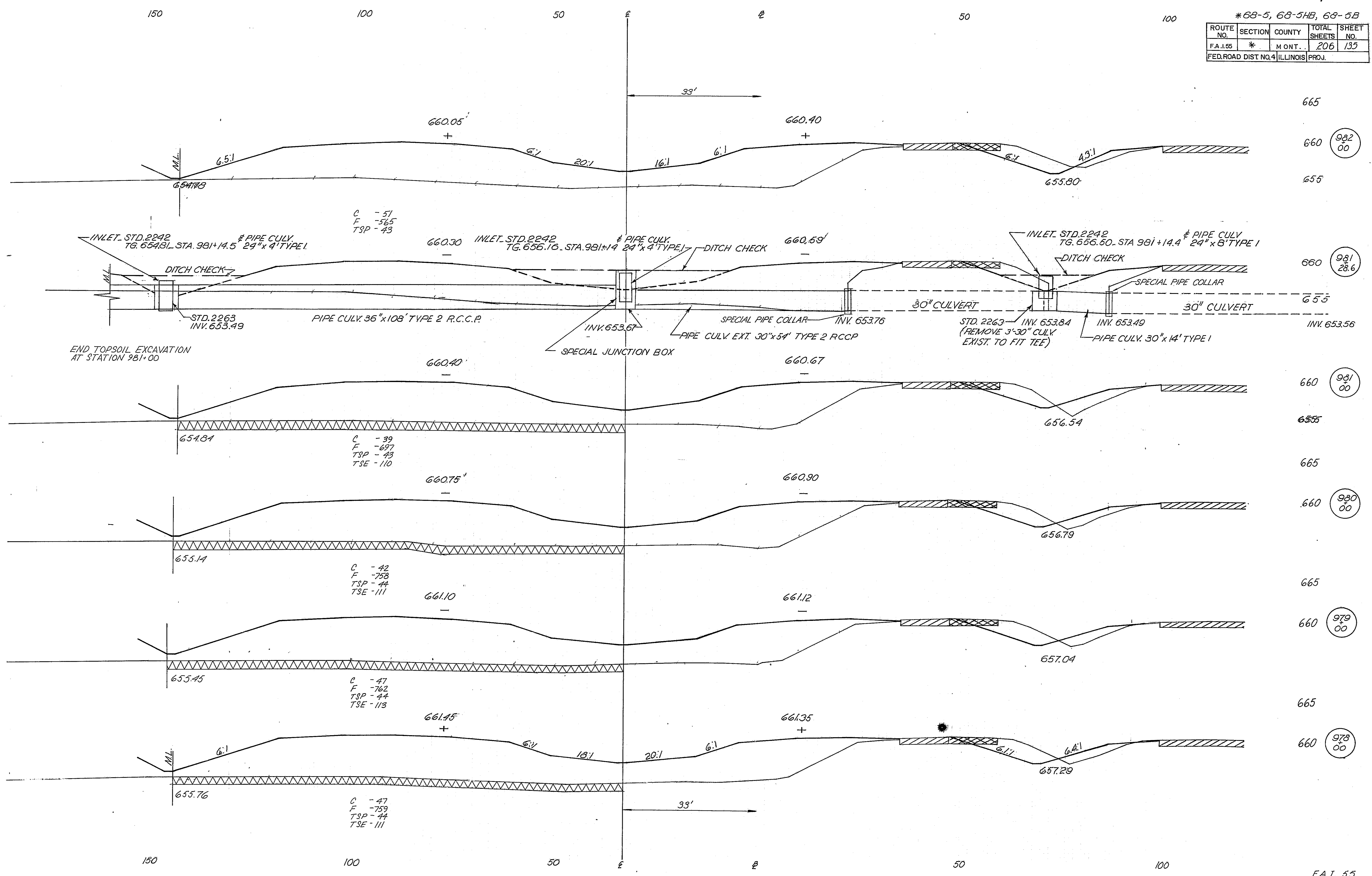
972
00

971
00

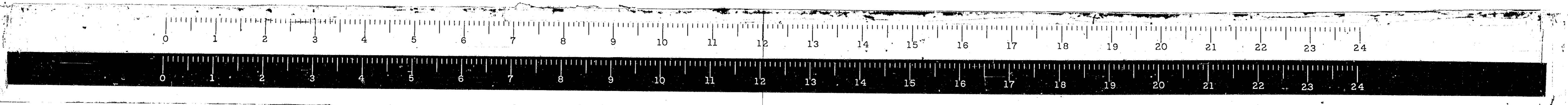
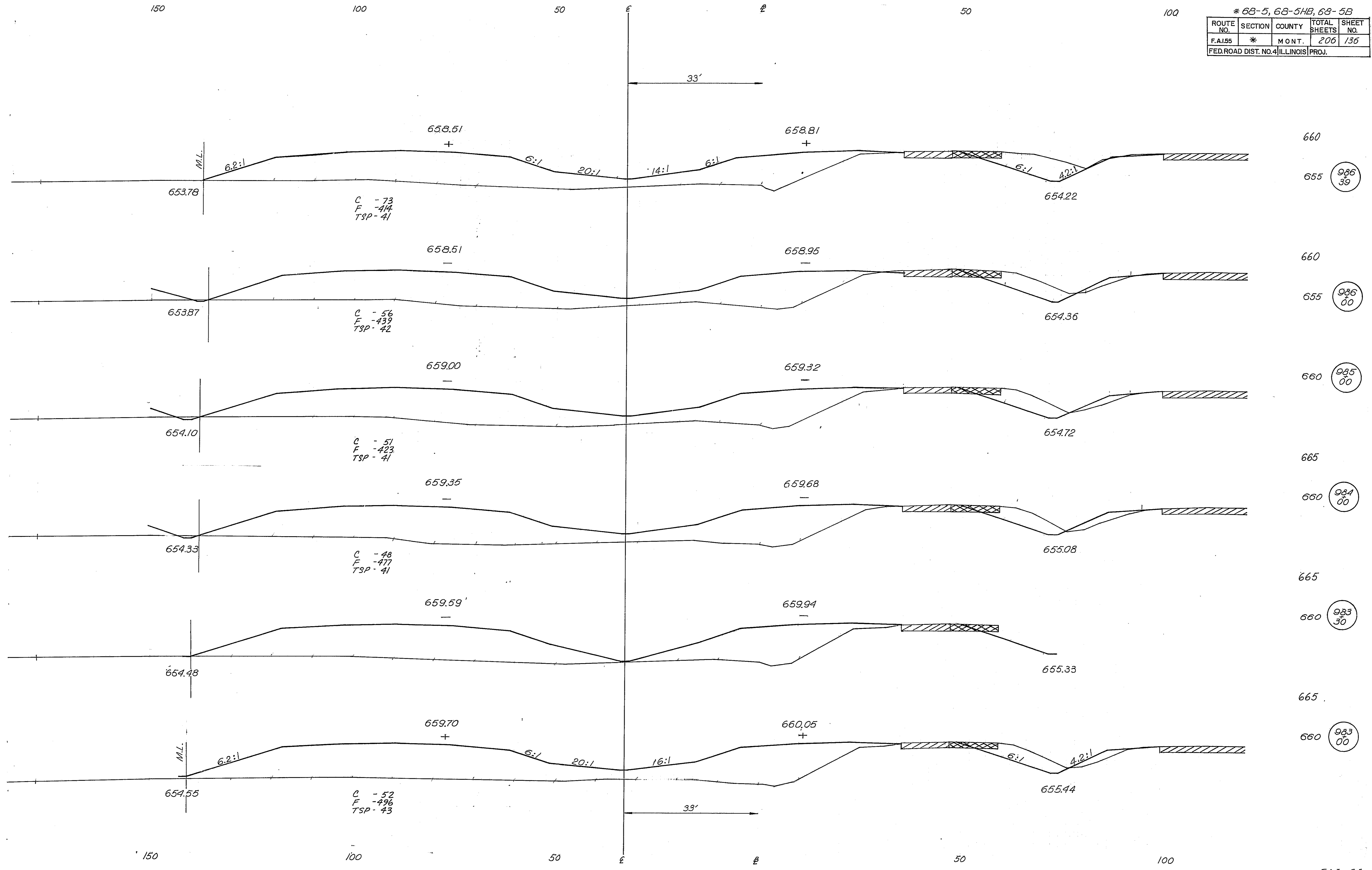
F.A.I. 55



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A. 185	*	MONT.	206	135
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



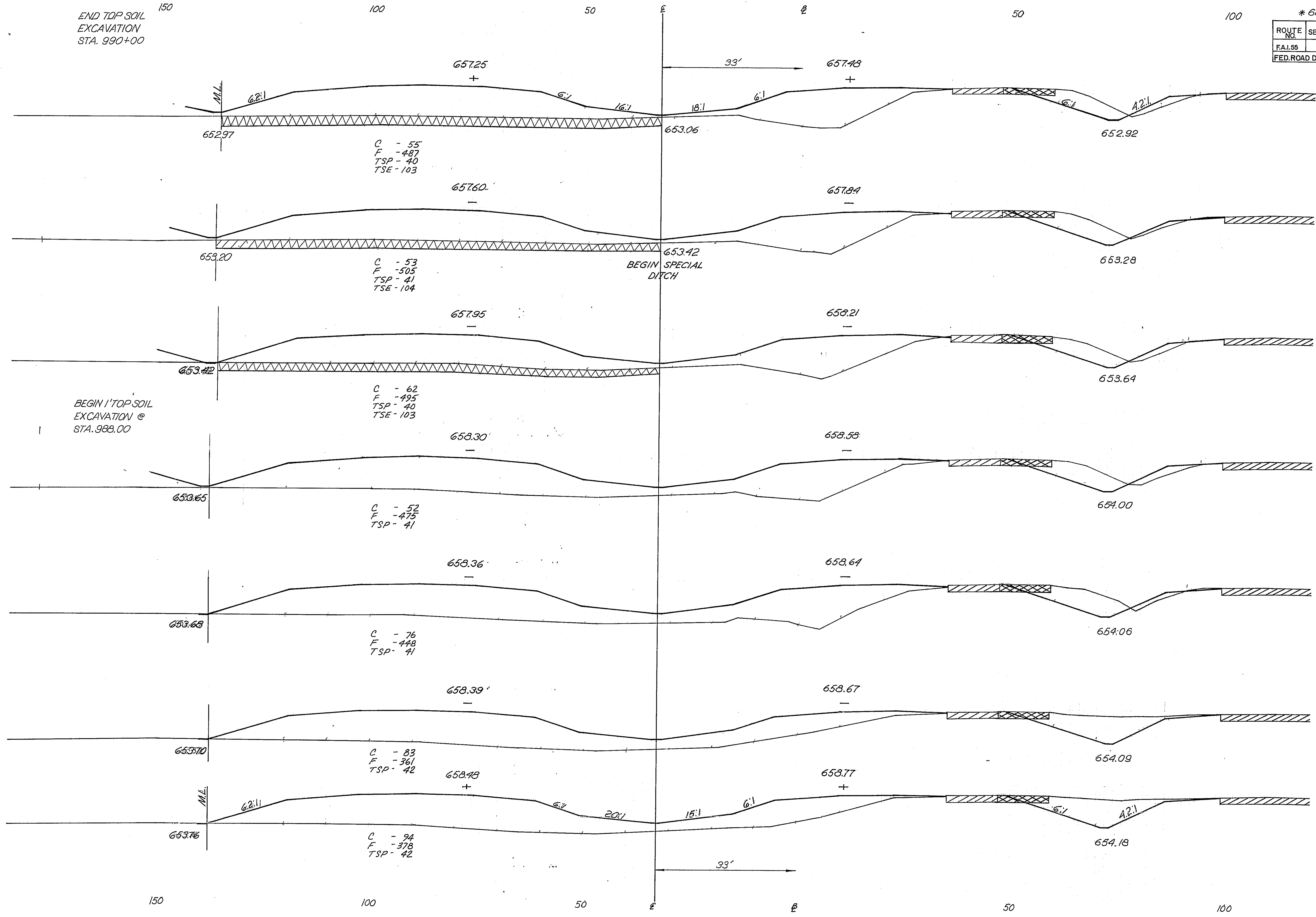
* 6B-5, 6B-5HB, 6B-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I.55	*	MONT.	206	136
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



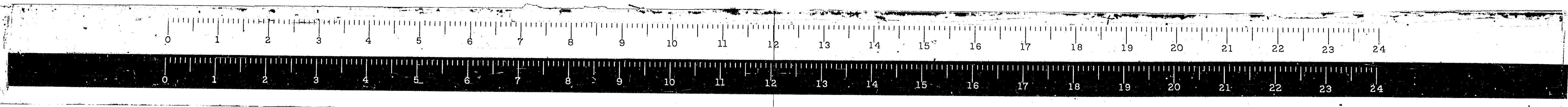
END TOP SOIL
EXCAVATION
STA. 990+00

* 6B-5, 6B-5A, 6B-5B

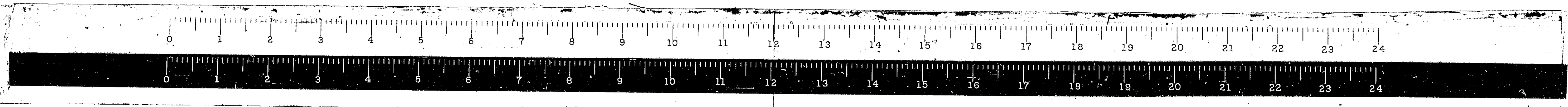
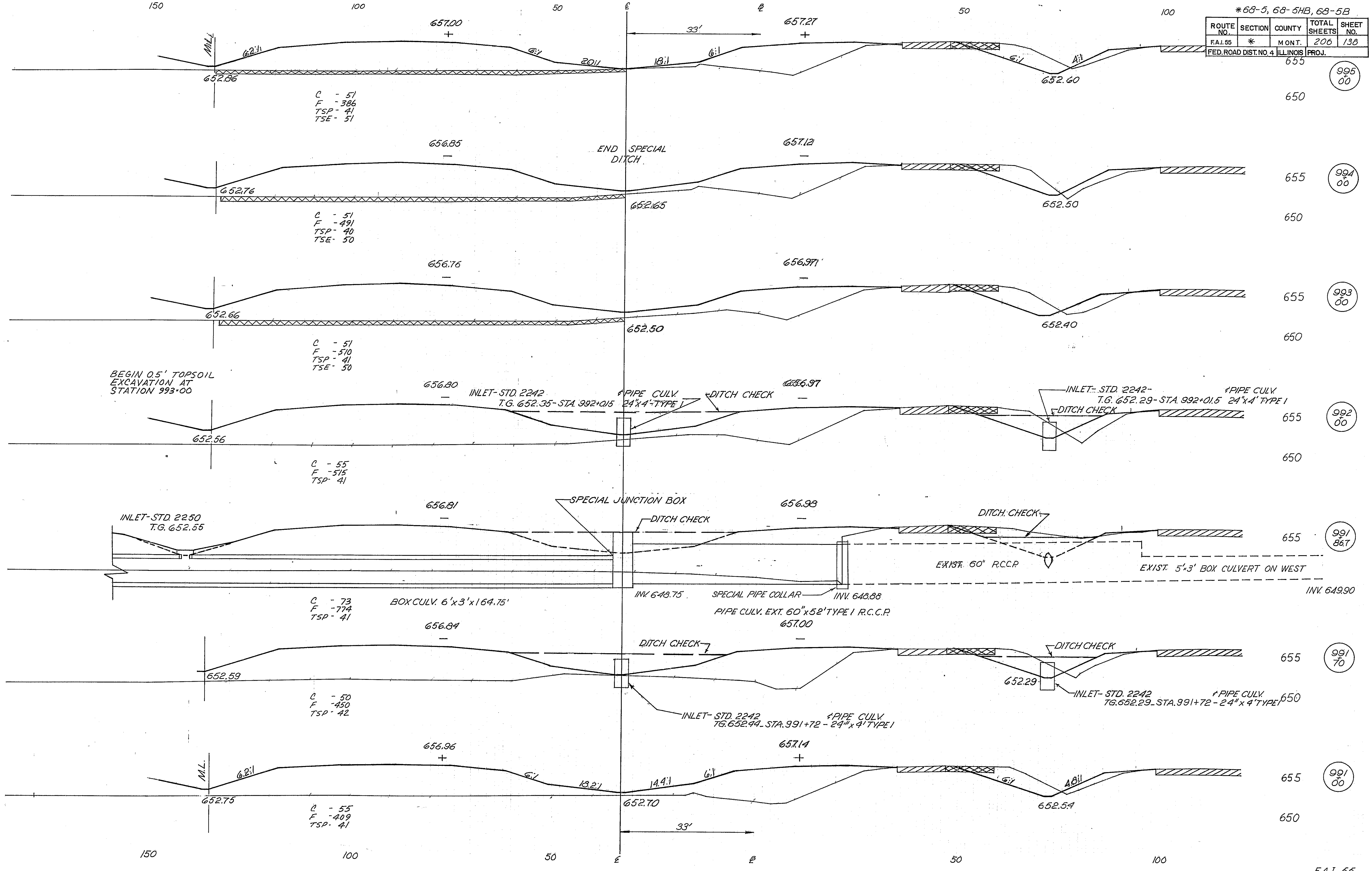
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI.55	*	MONT.	206	137
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



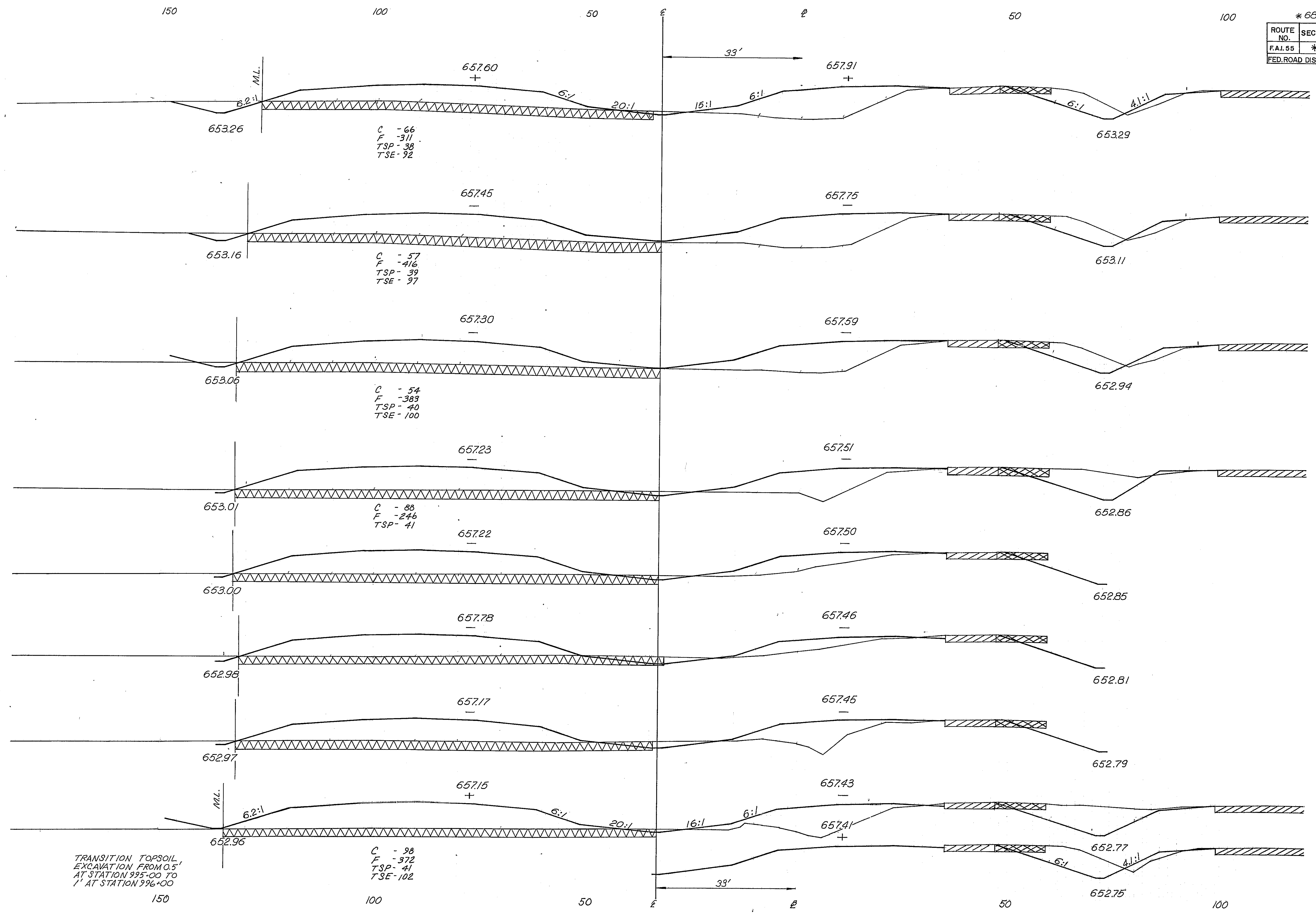
- 655 (990 00)
- 650
- 655 (989 00)
- 650
- 655 (988 00)
- 650
- 655 (987 00)
- 650
- 655 (986 84)
- 650
- 655 (986 75)
- 650
- 655 (986 50)
- 650



*68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
F.A.I. 55	*	MONT.	206
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.			138



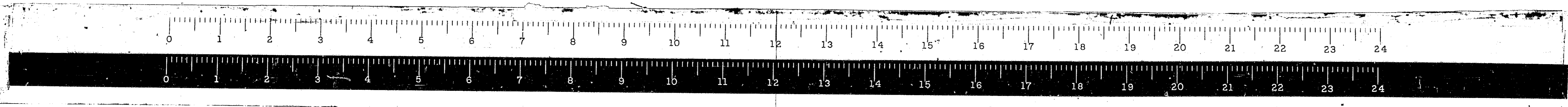
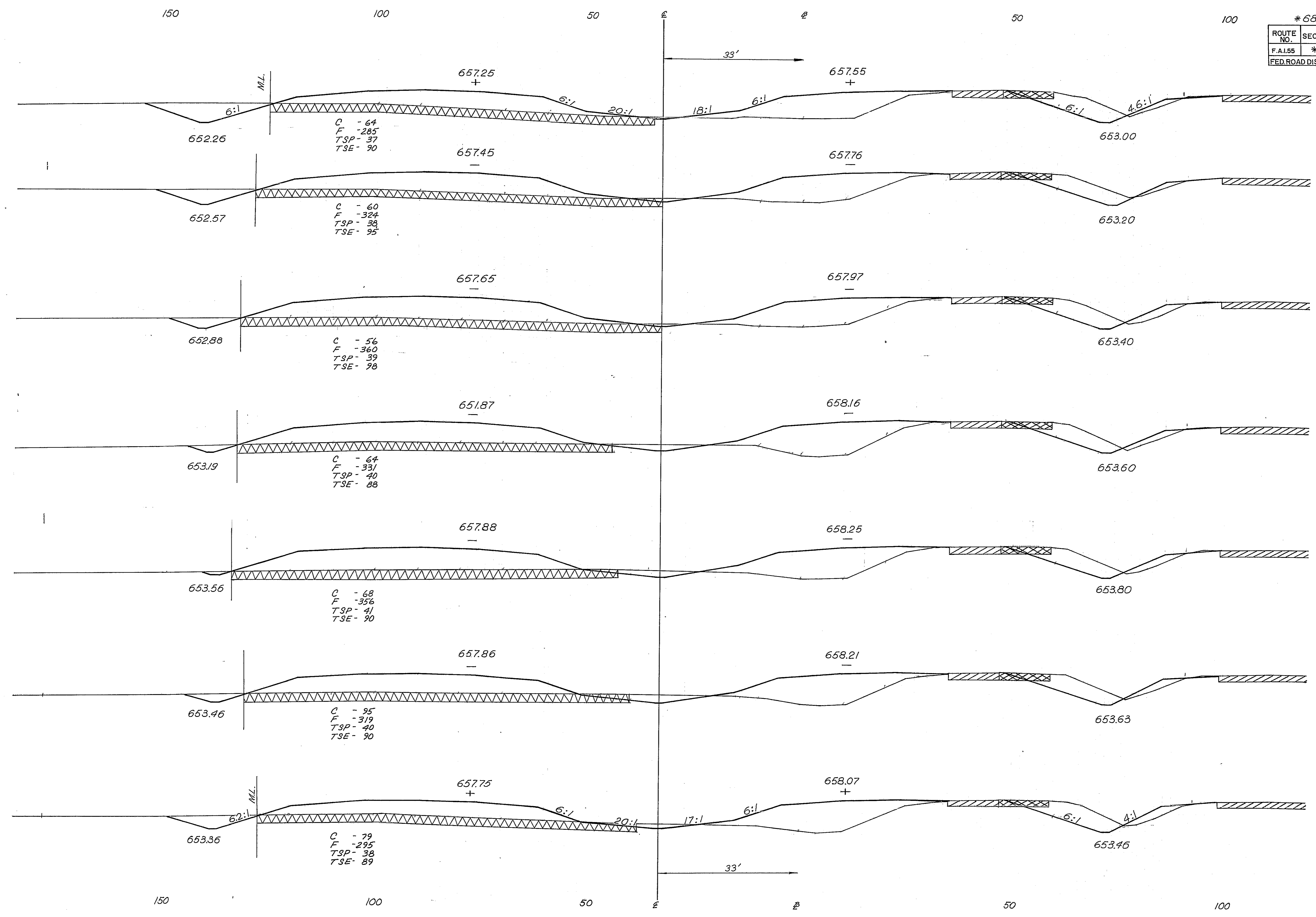
* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	139
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



- 655 (990/00)
- 650
- 655 (998/00)
- 650
- 655 (997/00)
- 650
- 655 (996/54)
- 650
- 655 (996/15)
- 650
- 655 (996/27)
- 650
- 655 (996/12)
- 650
- 655 (996/00)
- 655 (996/86)
- 650

F.A.I. 55

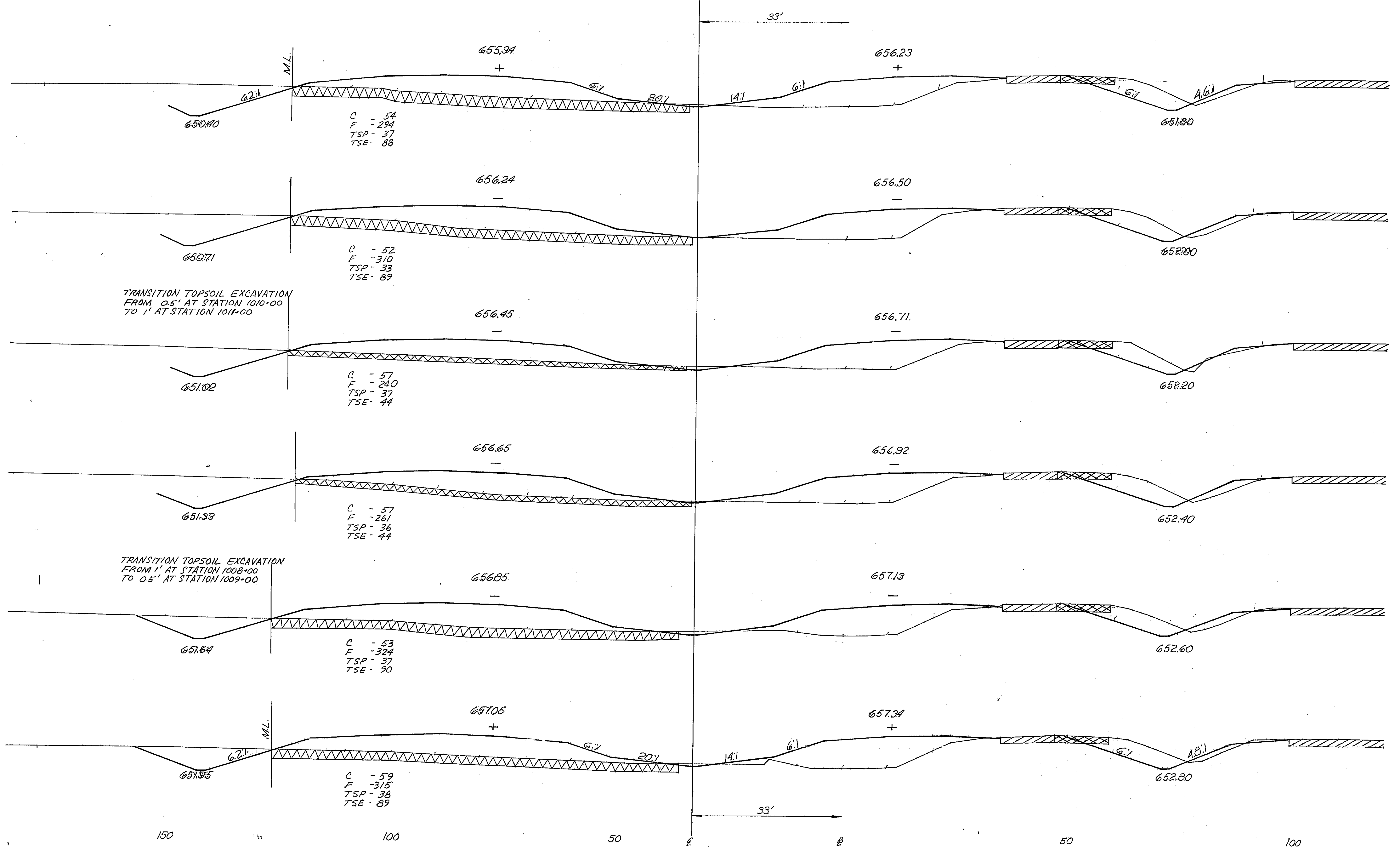
* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A. 55	*	MONT.	205	140
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



150 100 50 0 50 100

* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	171
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



655 (1012/00)

650

655 (1011/00)

650

655 (1010/00)

650

655 (1009/00)

650

655 (1008/00)

650

655 (1007/00)

650

TRANSITION TOPSOIL EXCAVATION
FROM 0.5' AT STATION 1010+00
TO 1' AT STATION 1011+00

TRANSITION TOPSOIL EXCAVATION
FROM 1' AT STATION 1008+00
TO 0.5' AT STATION 1009+00

C - 54
F - 294
TSP - 37
TSE - 88

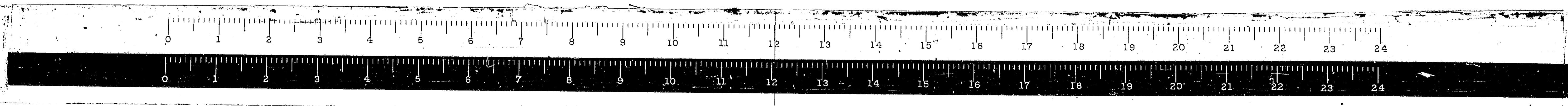
C - 52
F - 310
TSP - 33
TSE - 89

C - 57
F - 240
TSP - 37
TSE - 44

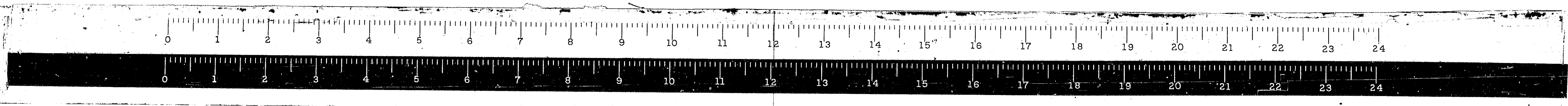
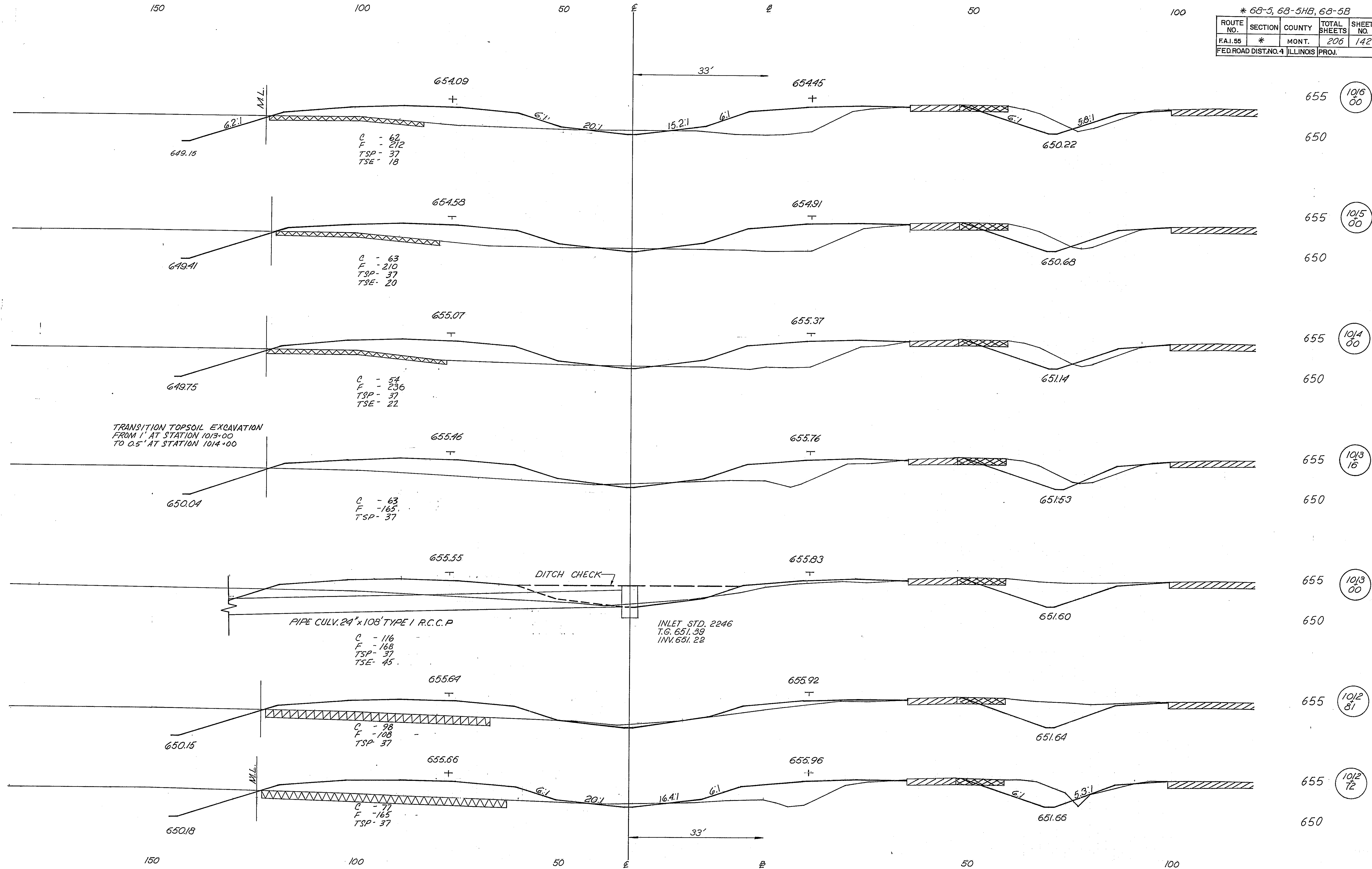
C - 57
F - 261
TSP - 36
TSE - 44

C - 53
F - 324
TSP - 37
TSE - 90

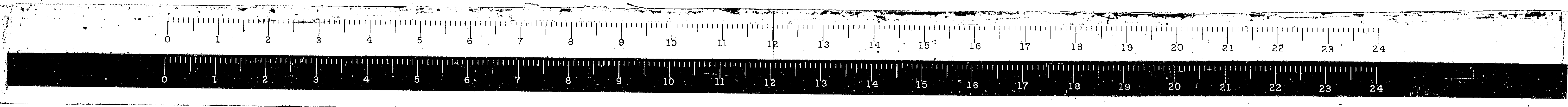
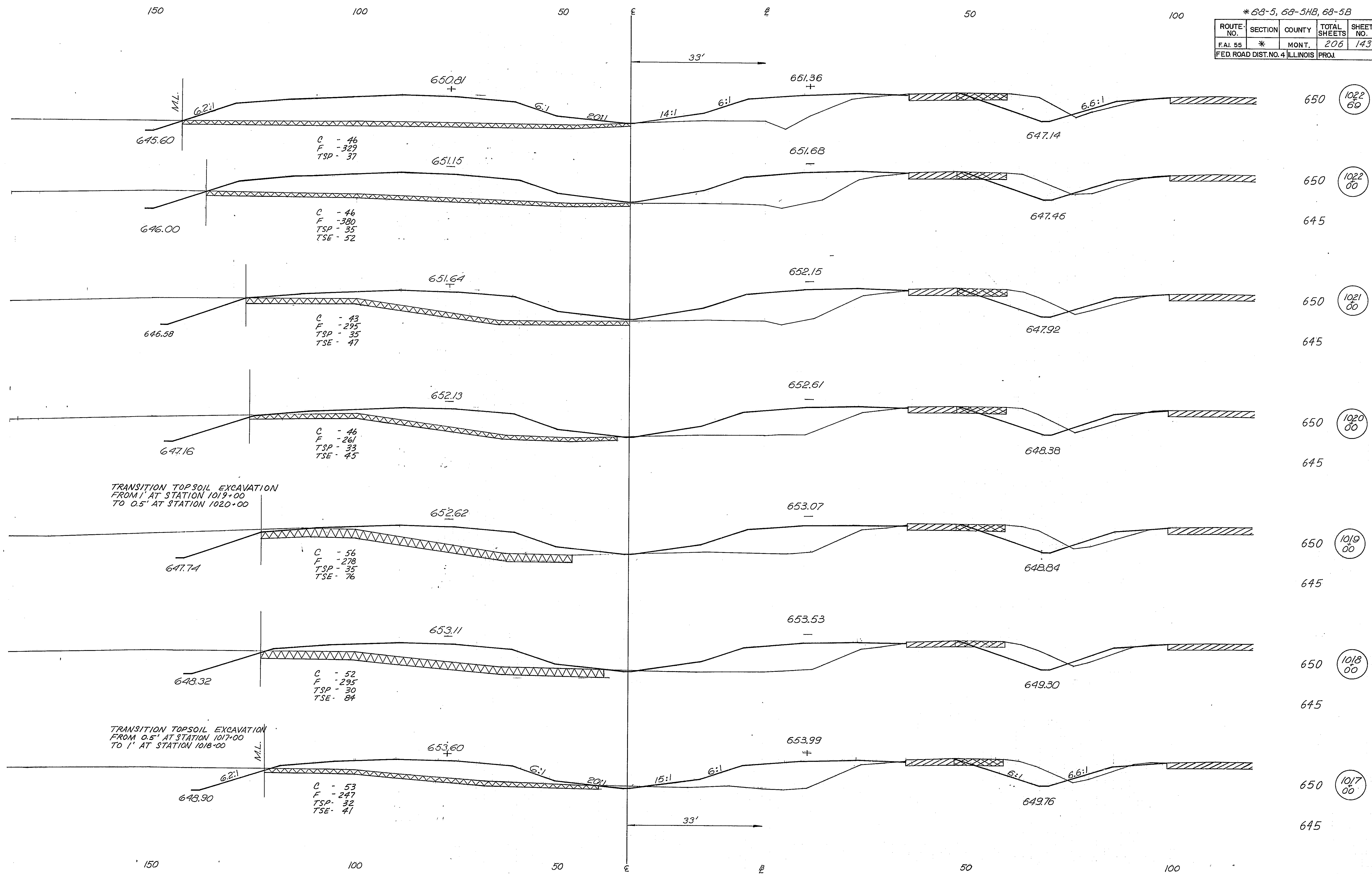
C - 59
F - 315
TSP - 38
TSE - 89



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	142
FED. ROAD DIST. NO. 4		ILLINOIS PROJ.		

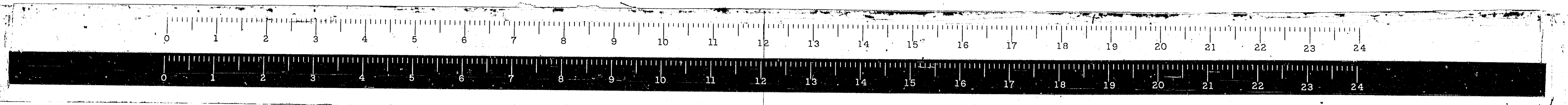
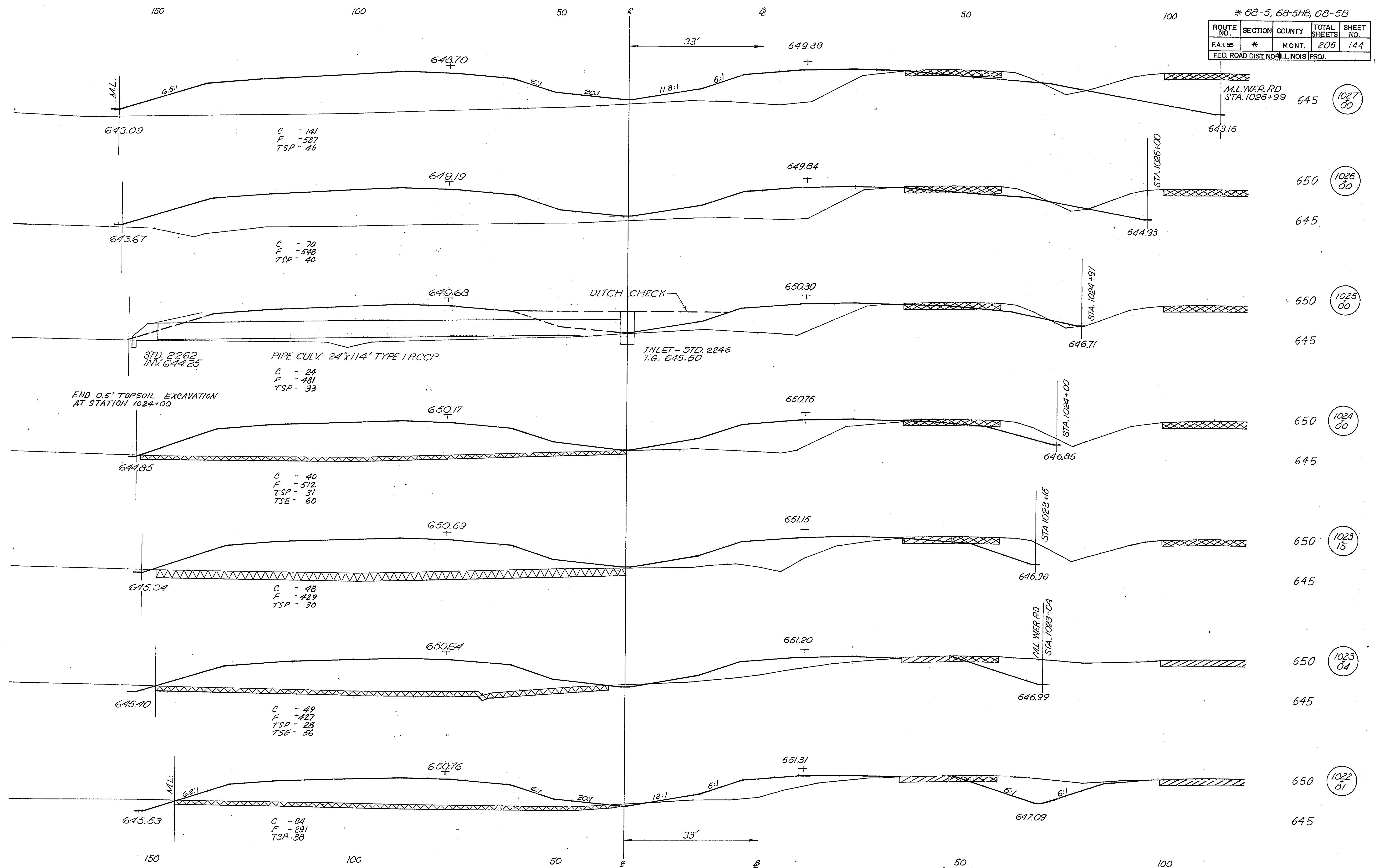


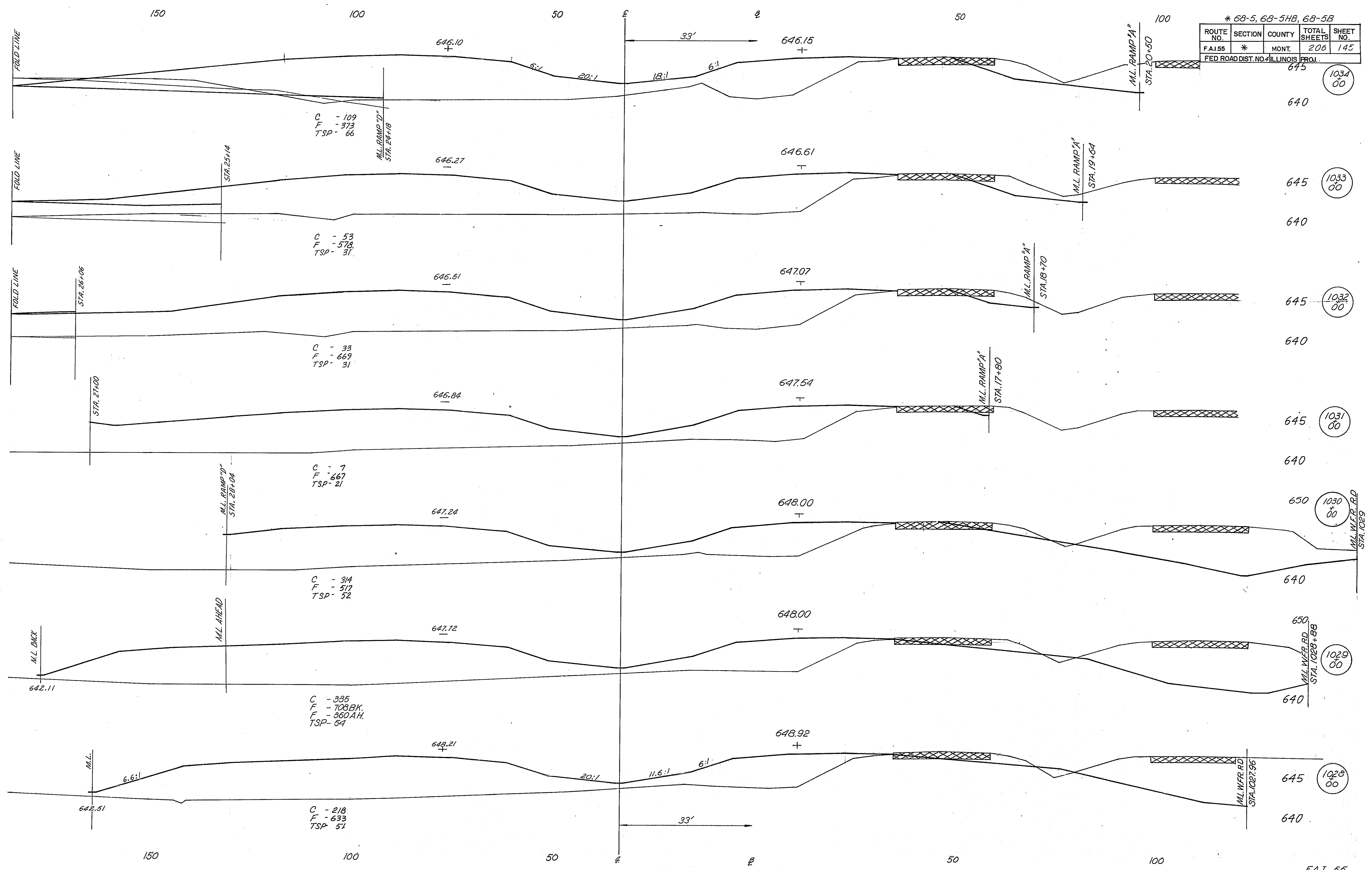
* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	143
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



* 63-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA.I. 55	*	MONT.	206	144
FED. ROAD DIST. NO. ILLINOIS PROJ.				





* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	145
FED ROAD DIST. NO. ILLINOIS PROJ.			645	1034 00

1034 00

1033 00

1032 00

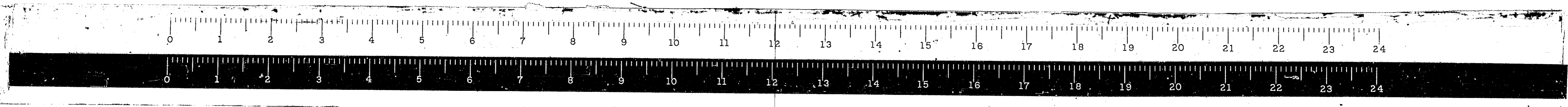
1031 00

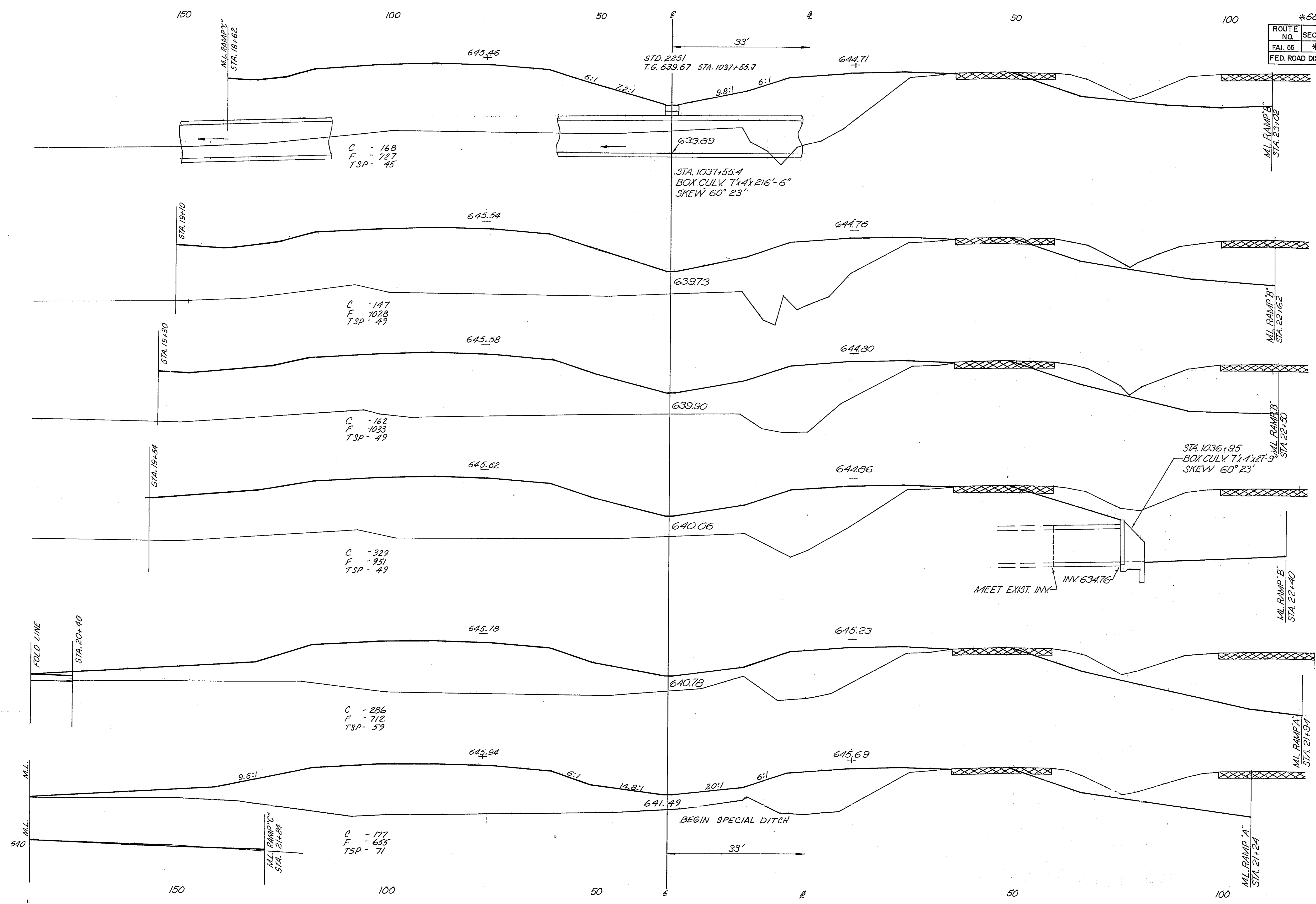
1030 00

1029 00

1028 00

F.A.I. 55





*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 55	*	MONT.	206	146
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

640 (1038 00)

635

640 (1037 47)

635

640 (1037 23)

635

640 (1037 00)

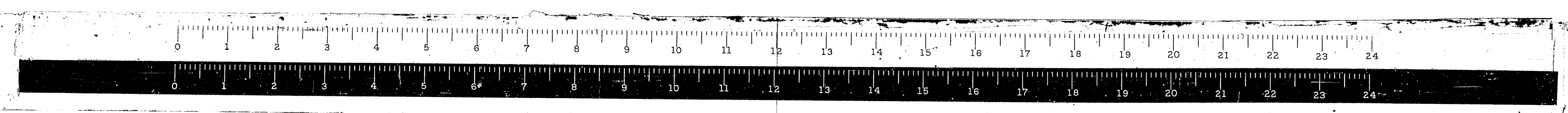
635

645 (1036 00)

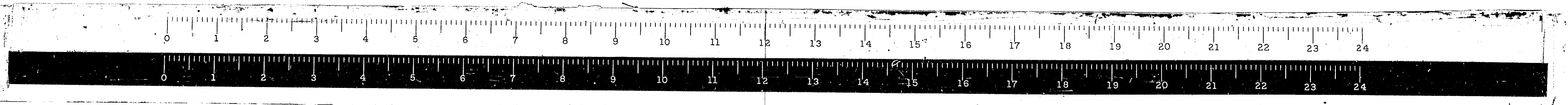
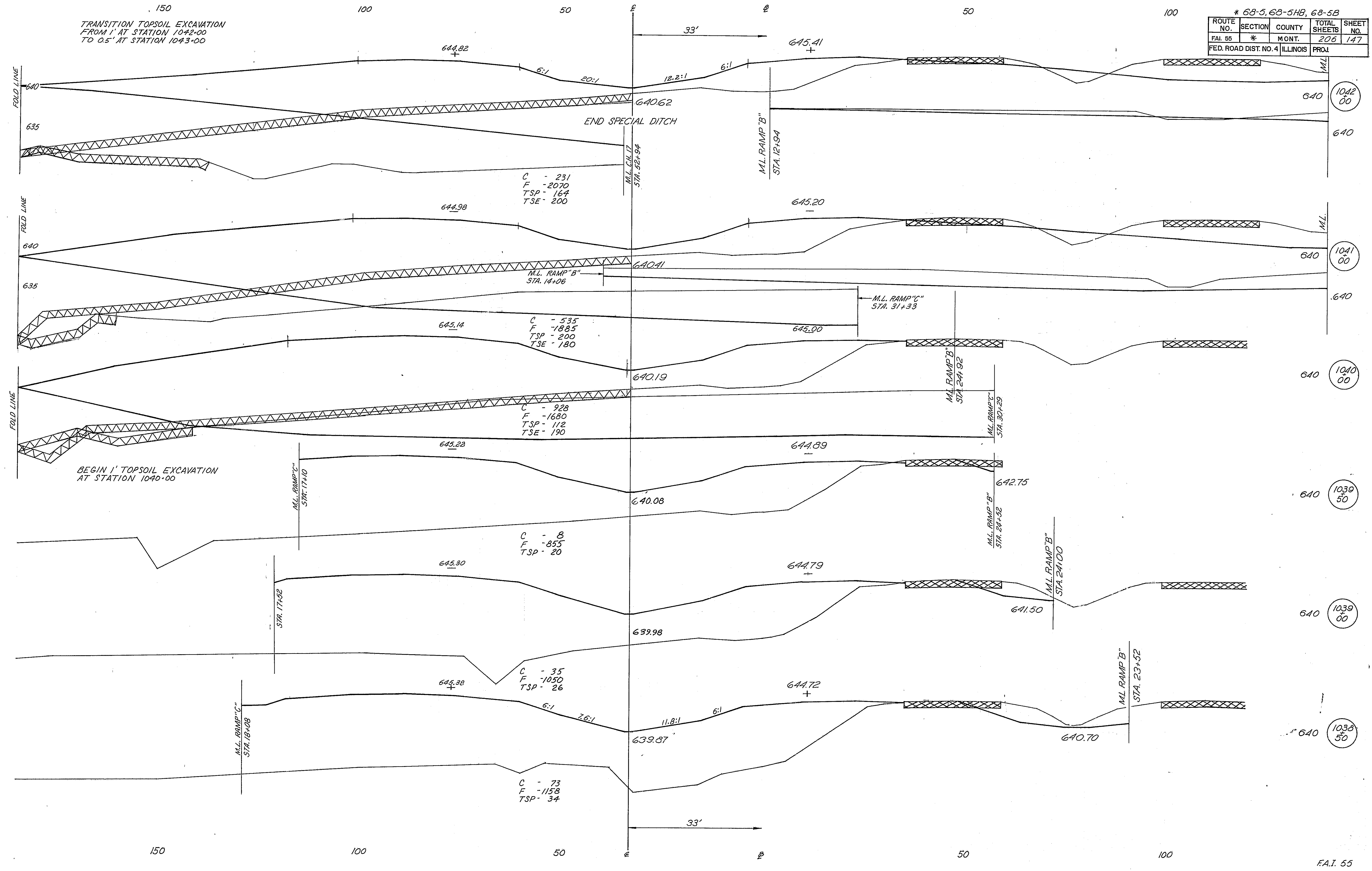
640

645 (1035 00)

640



* 68-5, 68-5HB, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAL 55	*	MONT.	206
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.			SHEET NO. 147



150

100

50

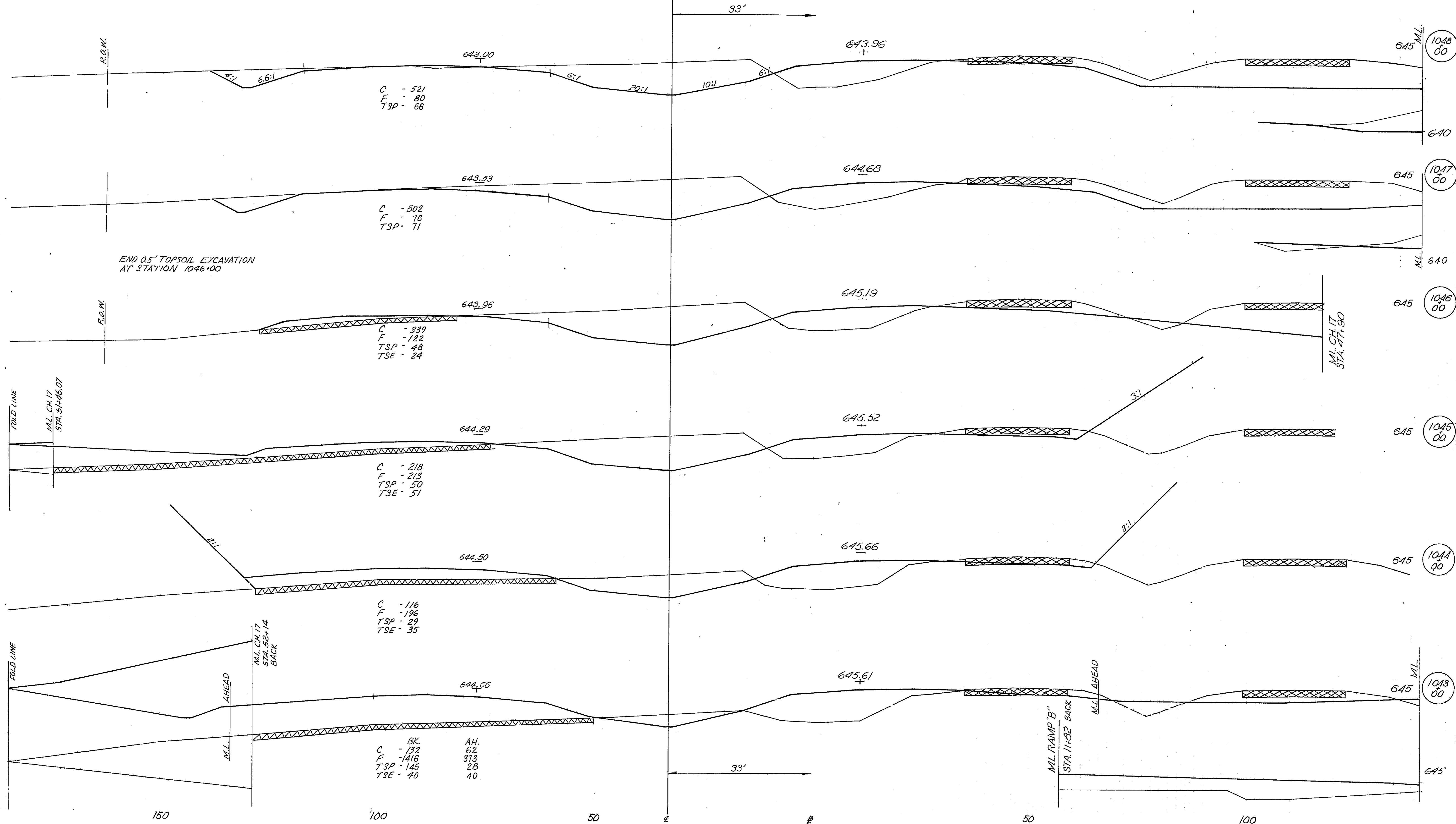
E

B

50

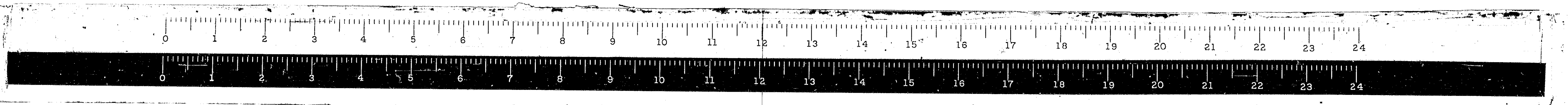
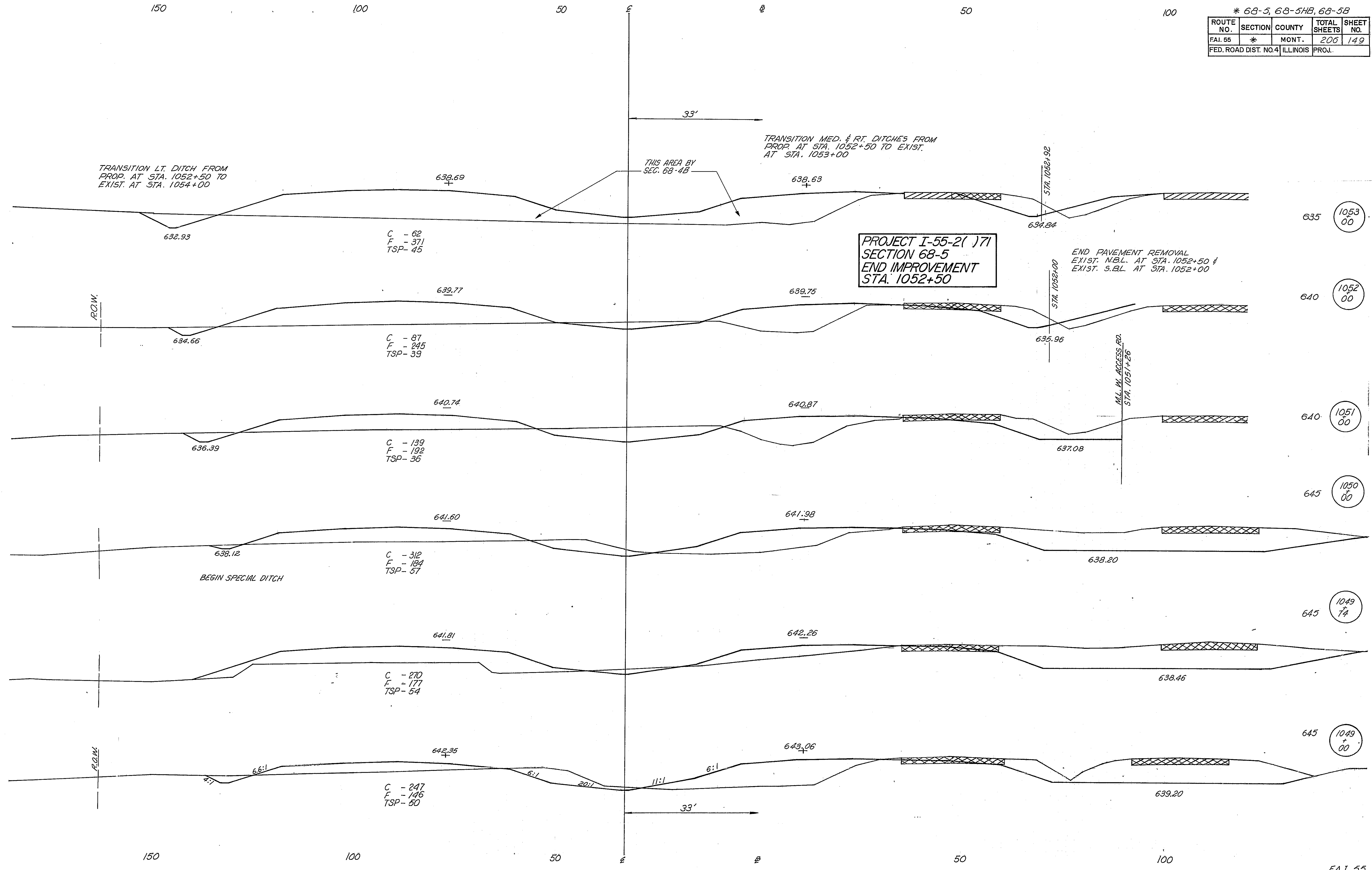
100

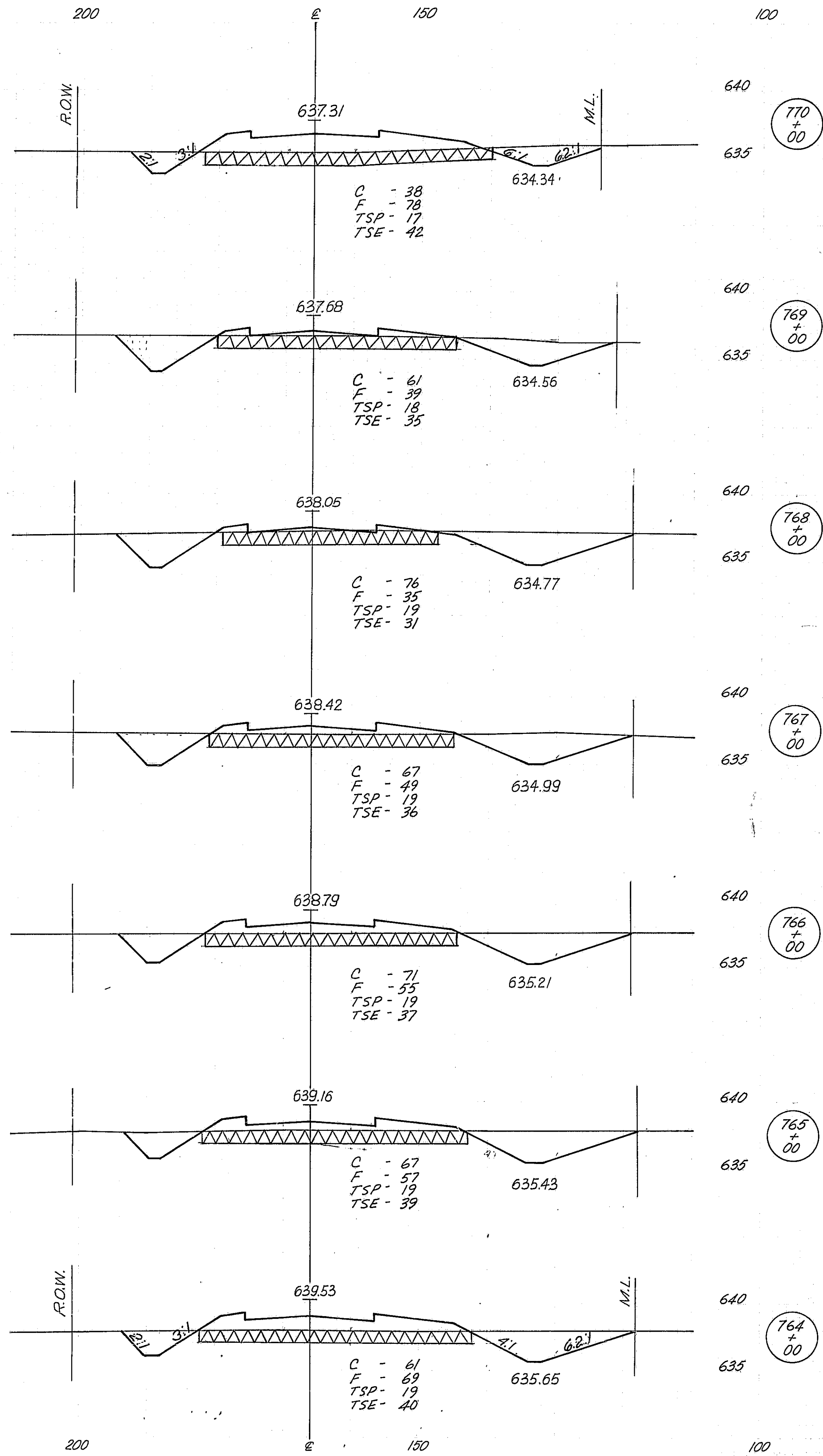
* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MON T.	206	148
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



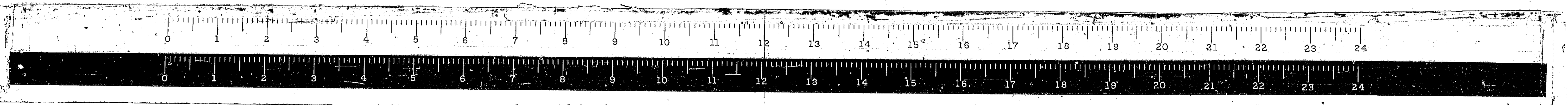
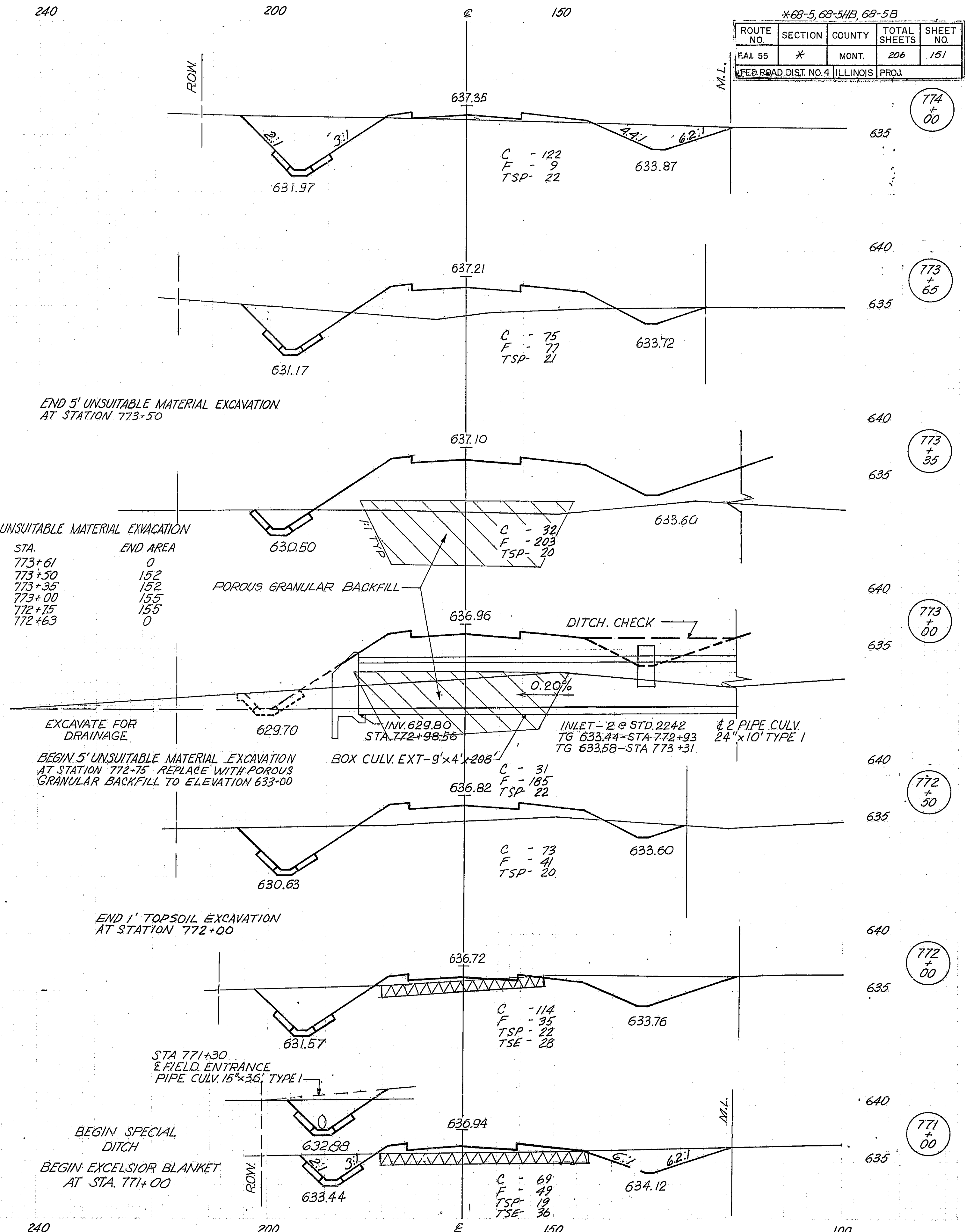
F.A.I. 55

* 68-5, 68-5A, 68-5B			
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS
FAI. 55	*	MONT.	206
FED. ROAD DIST. NO. 4	ILLINOIS	PROJ.	149

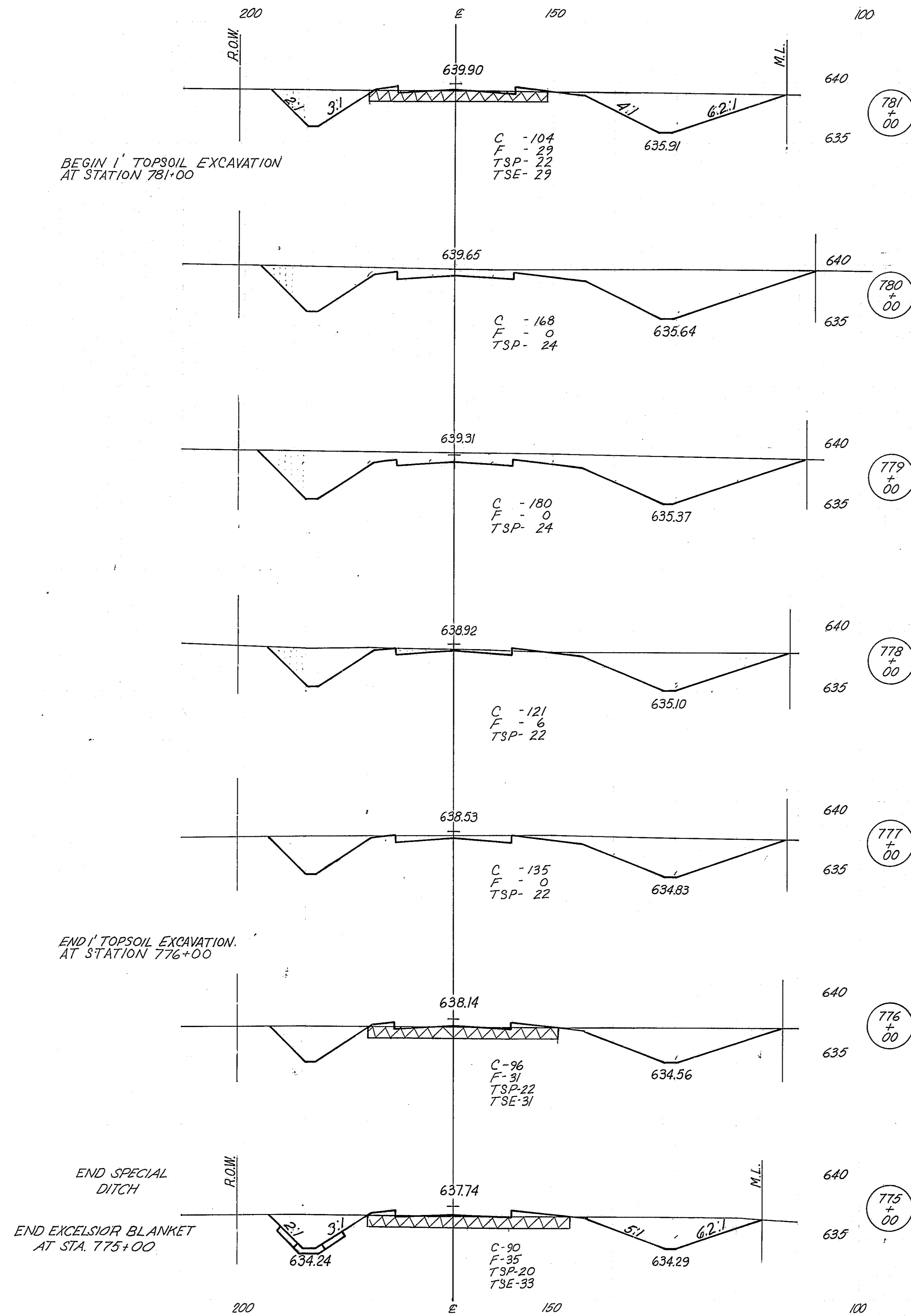




*68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT.	206	151
FEB. ROAD DIST. NO. 4 ILLINOIS PROJ.				

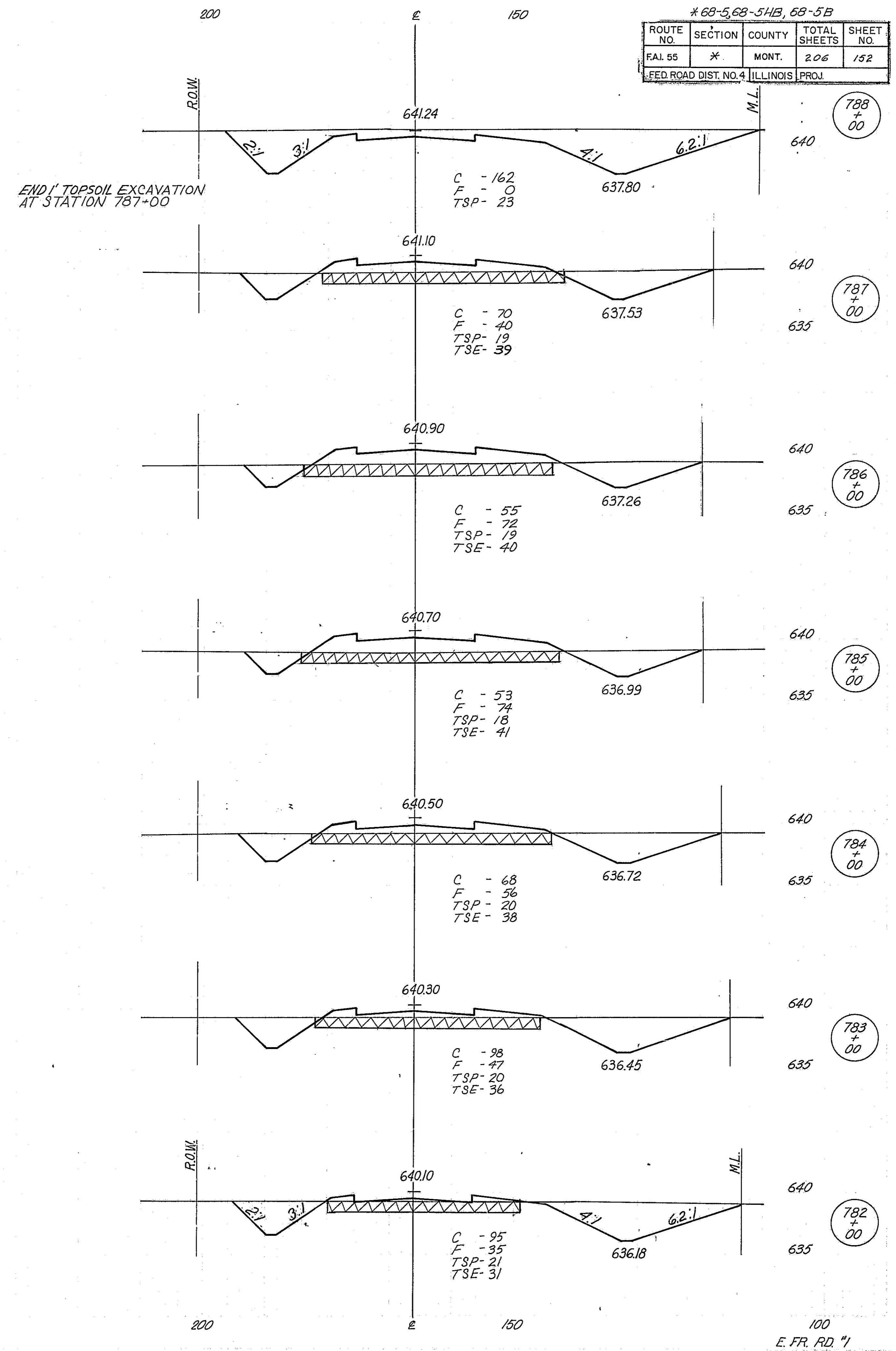


250

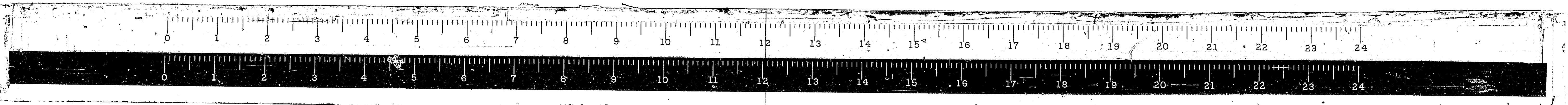


250

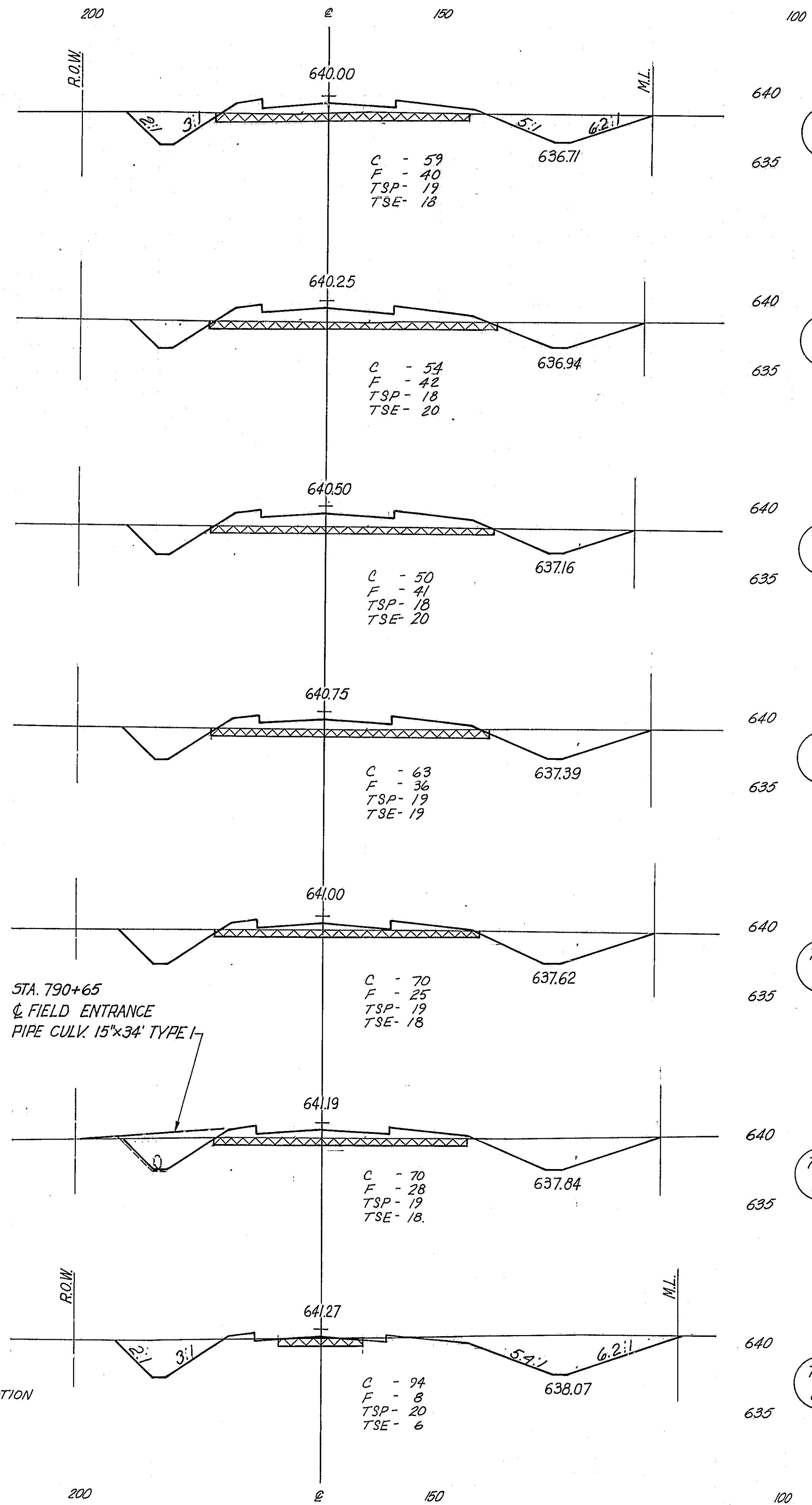
250



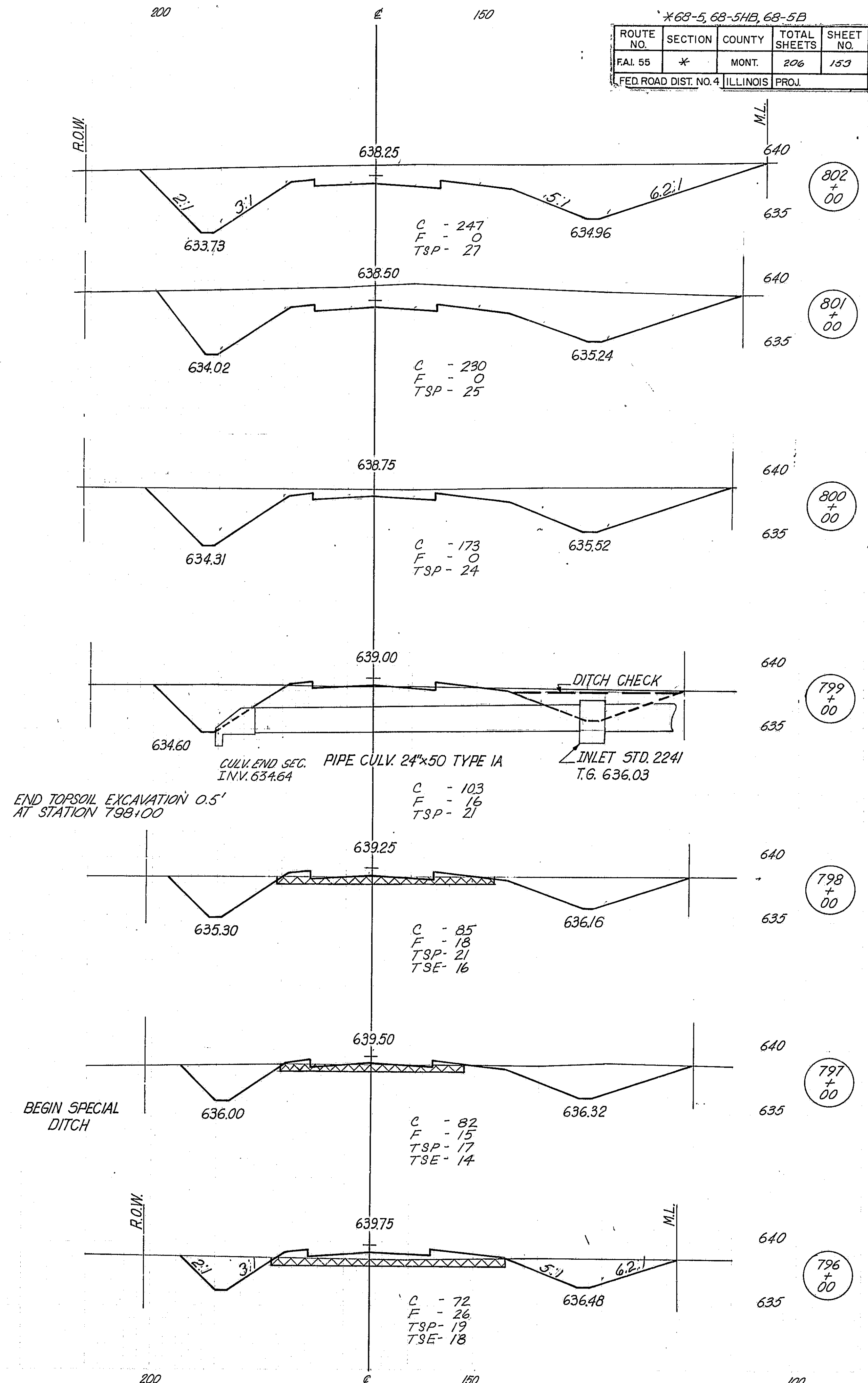
250



250



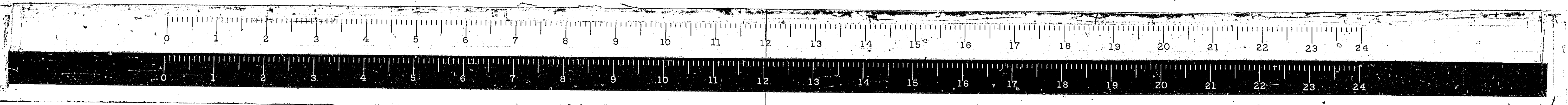
250



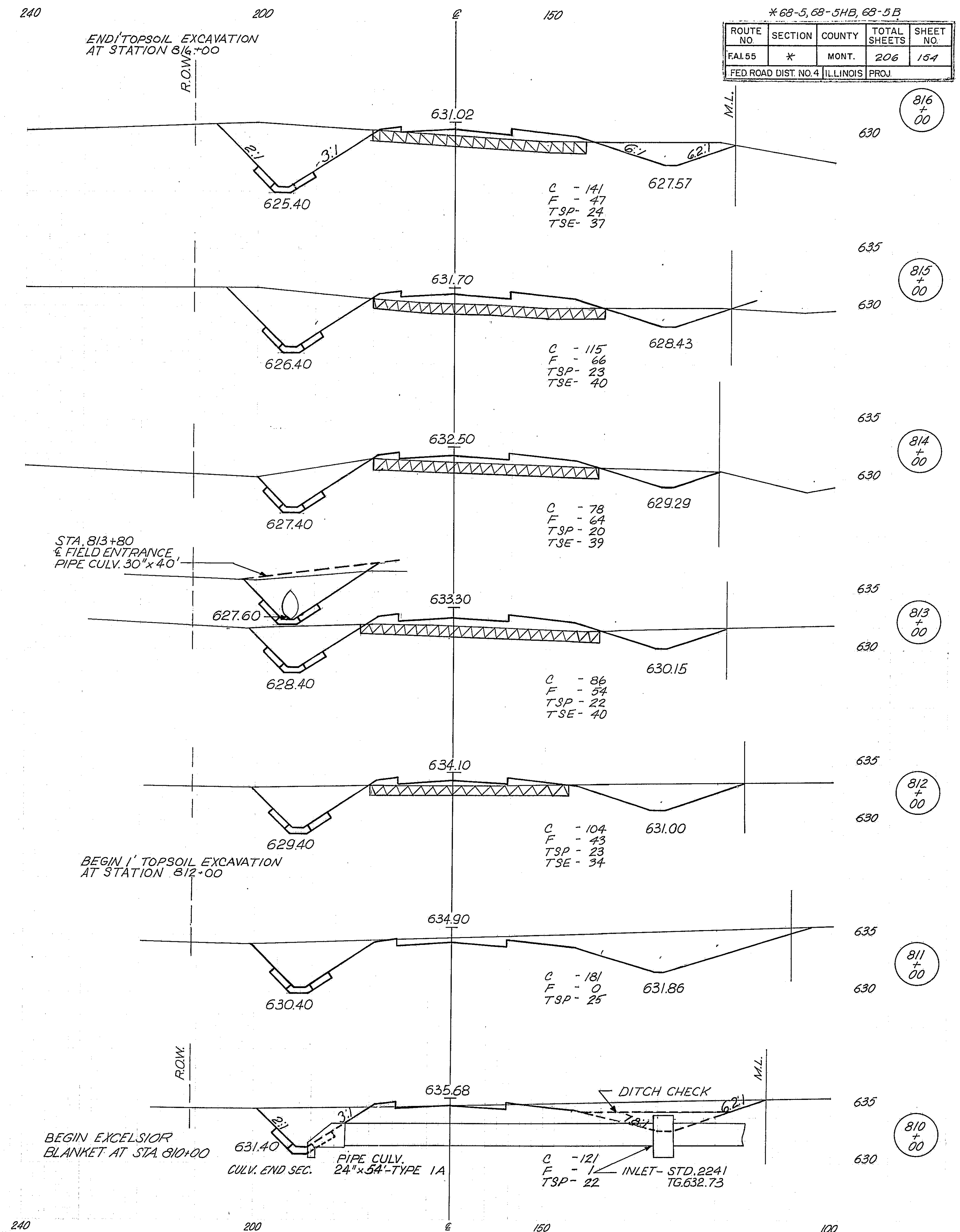
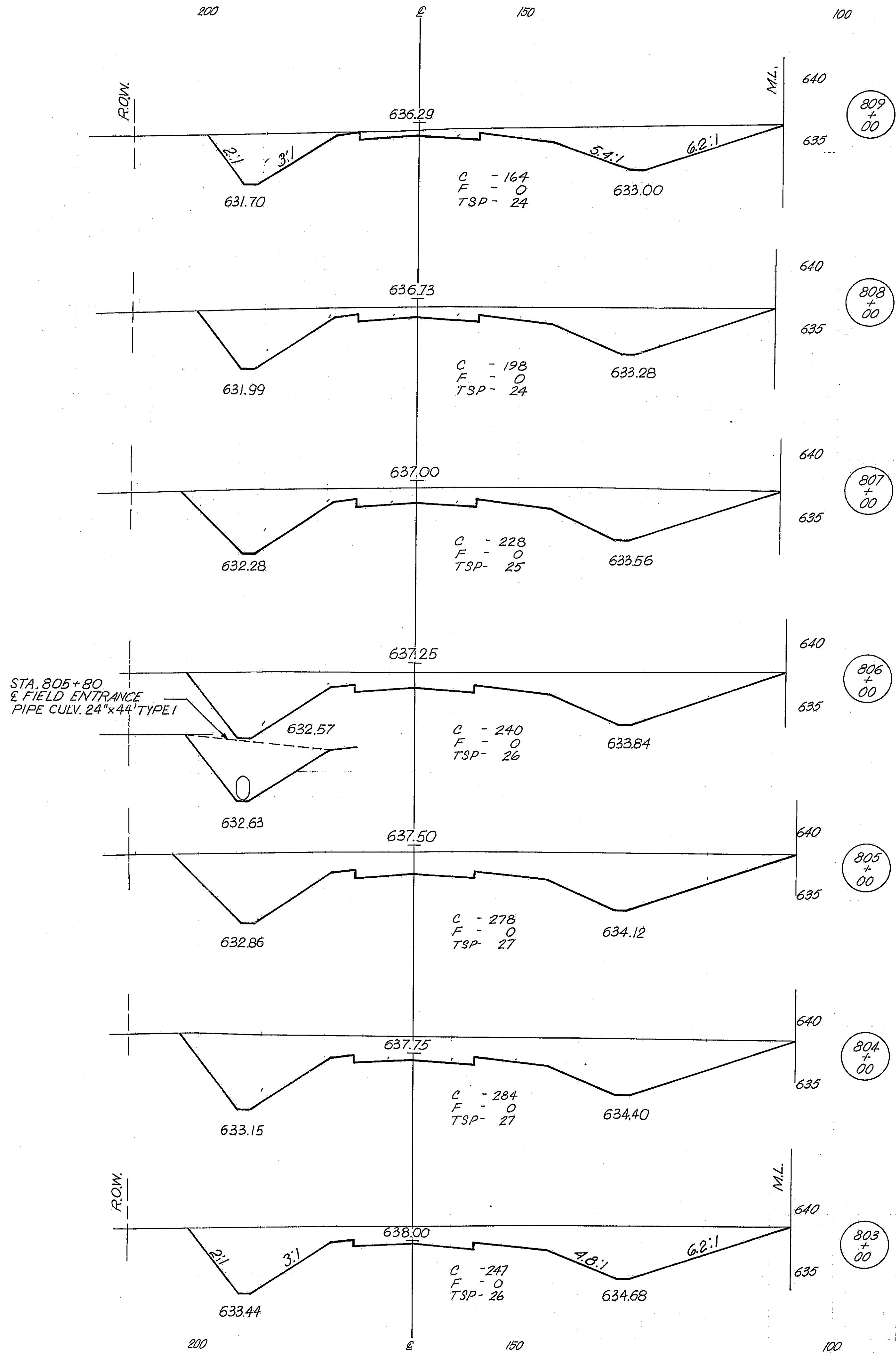
*69-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	153
FED. ROAD DIST. NO. 4			ILLINOIS PROJ.	

250

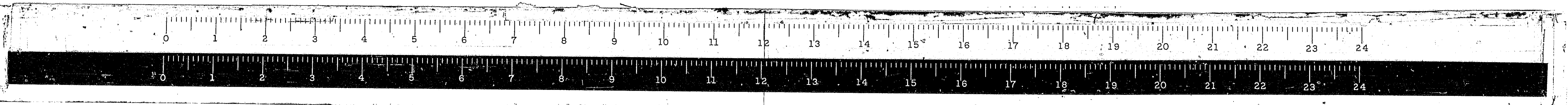


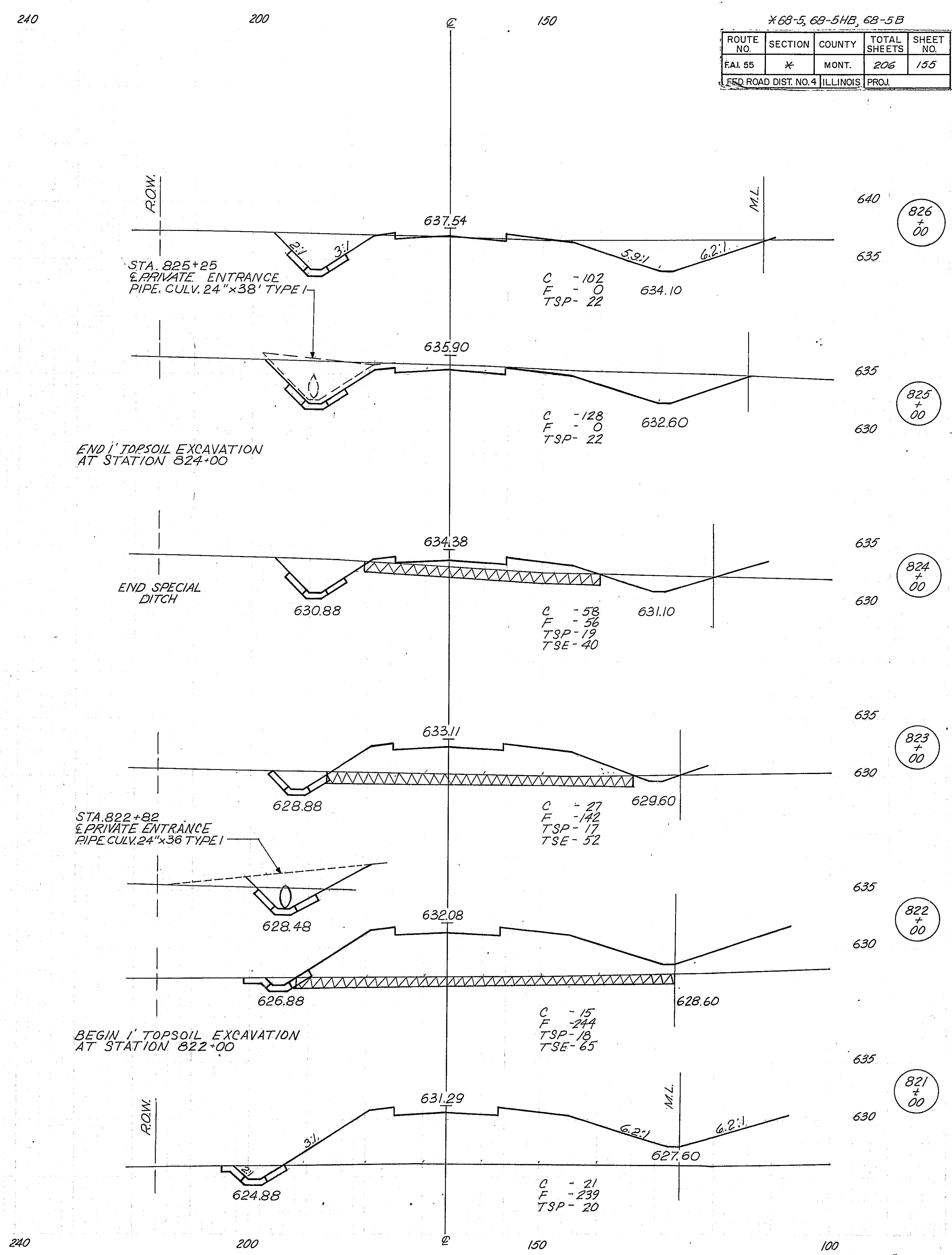
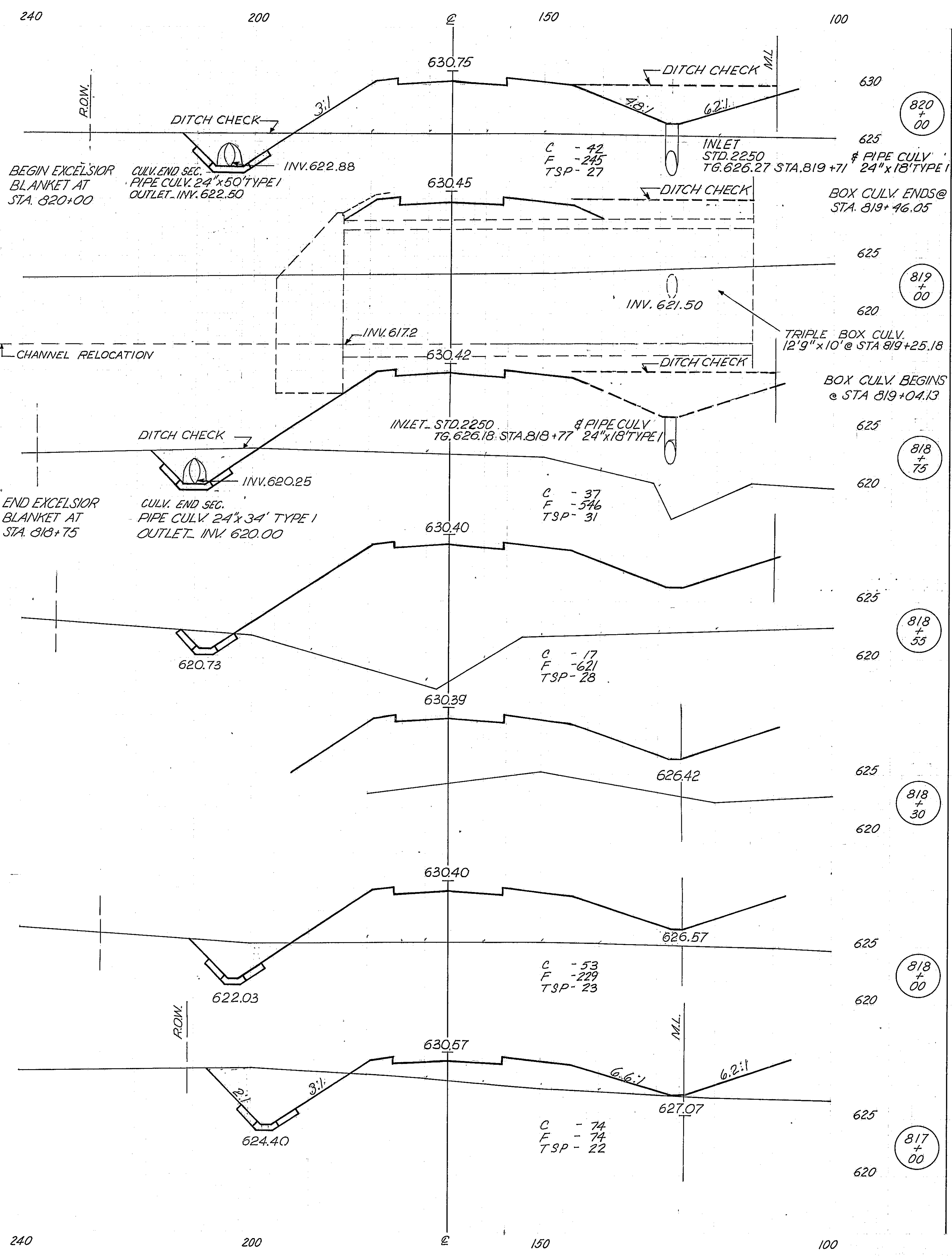
E. FR. RD. #1



* 68-5, 68-5HB, 68-5B

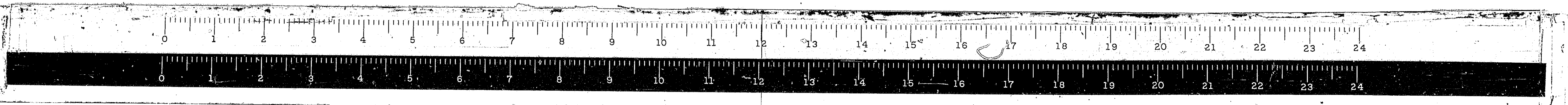
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL55	*	MONT.	206	154
FED ROAD DIST. NO. 4 ILLINOIS PROJ.				



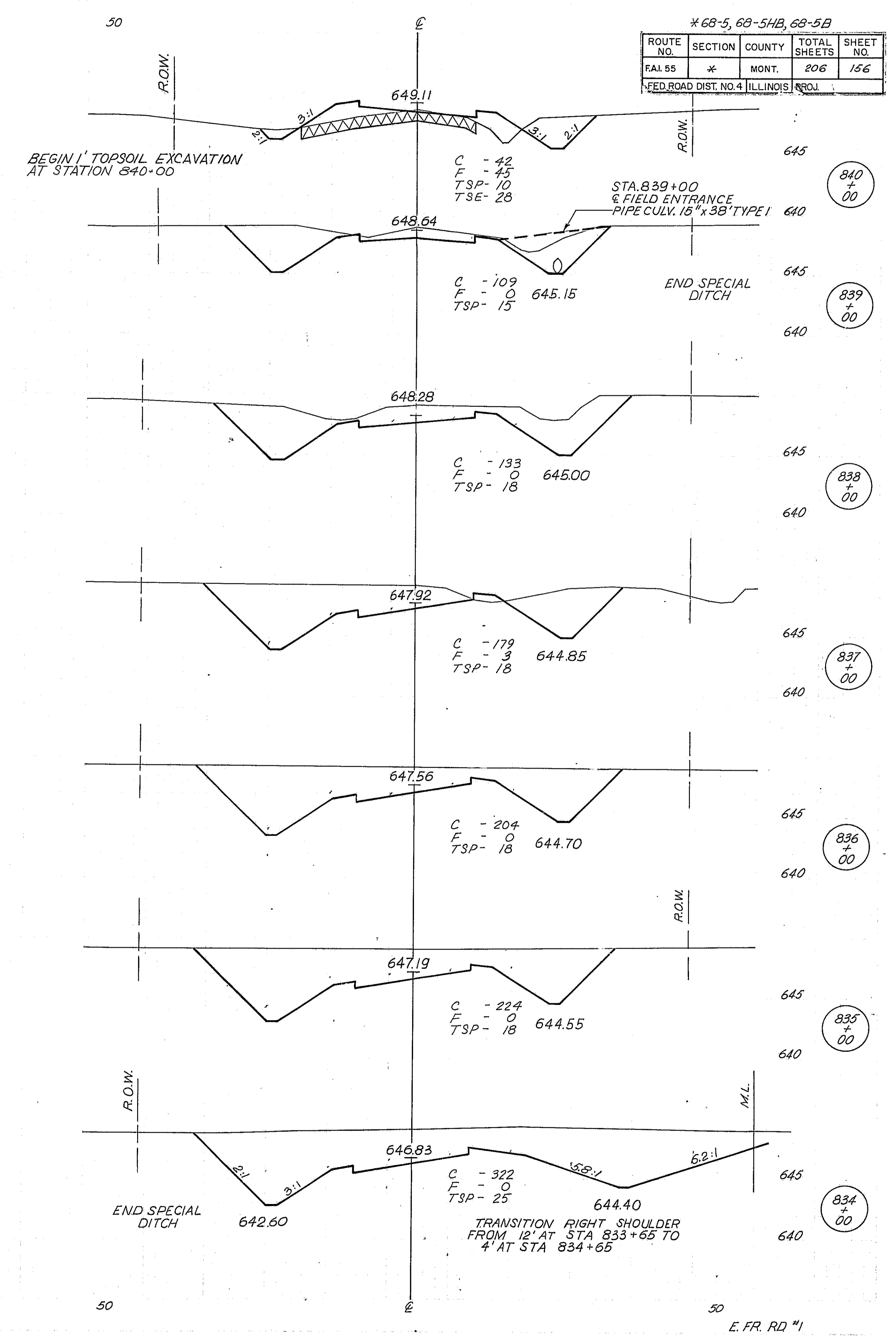
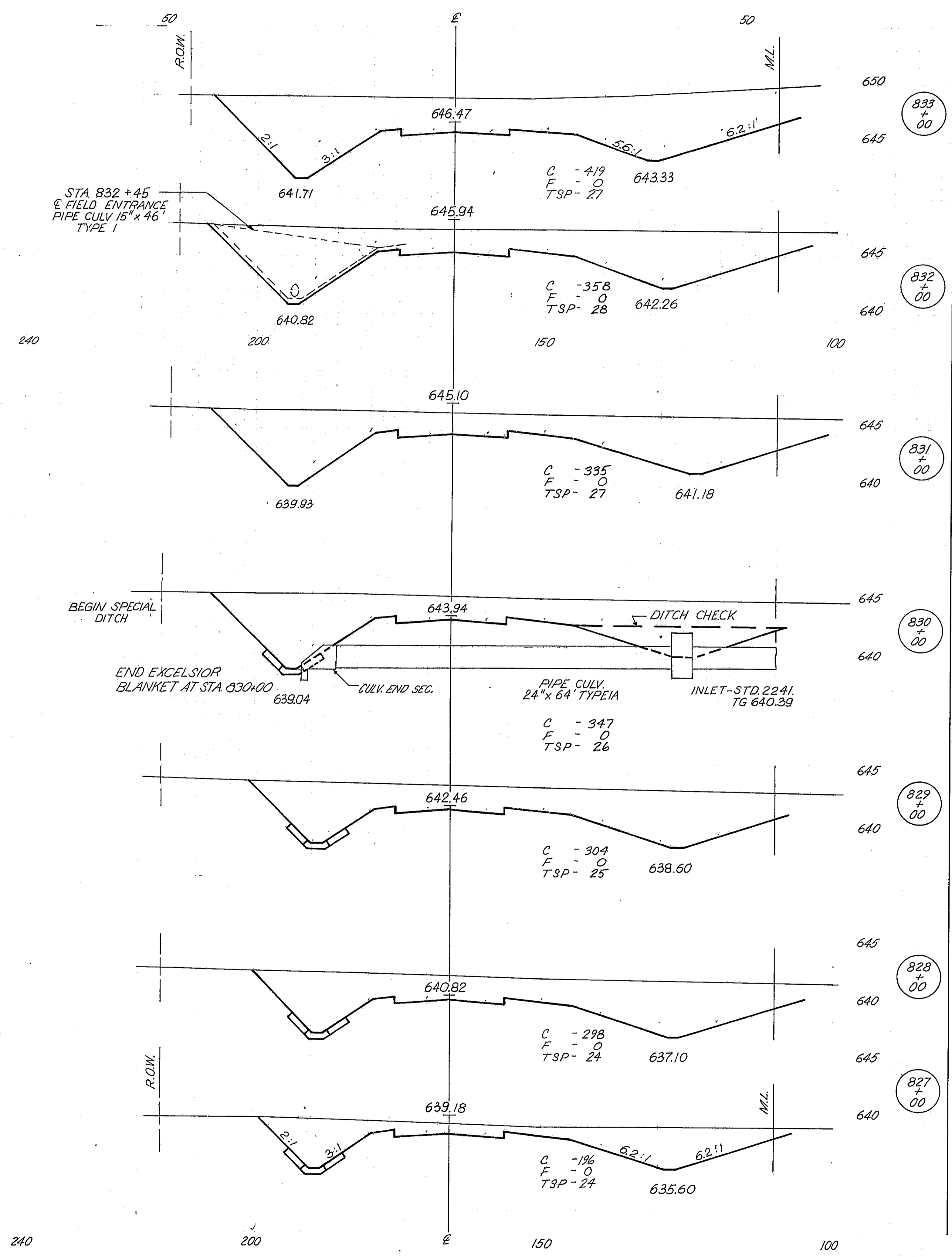


*68-5, 69-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MONT.	206	155
REQ. ROAD DIST. NO. 4 ILLINOIS PROJ.				

240 200 150 100 240 200 150 100 240 200 150 100



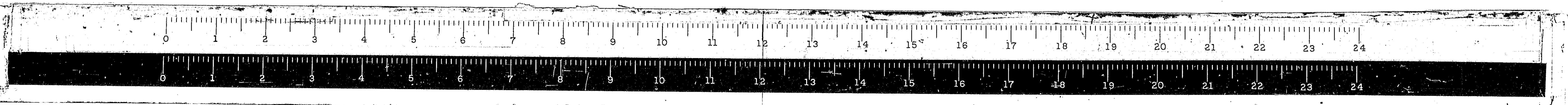
100
E. FR. RD. "1"



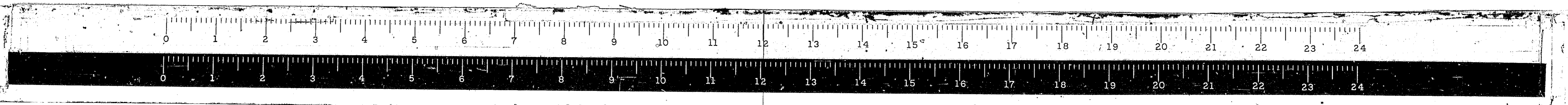
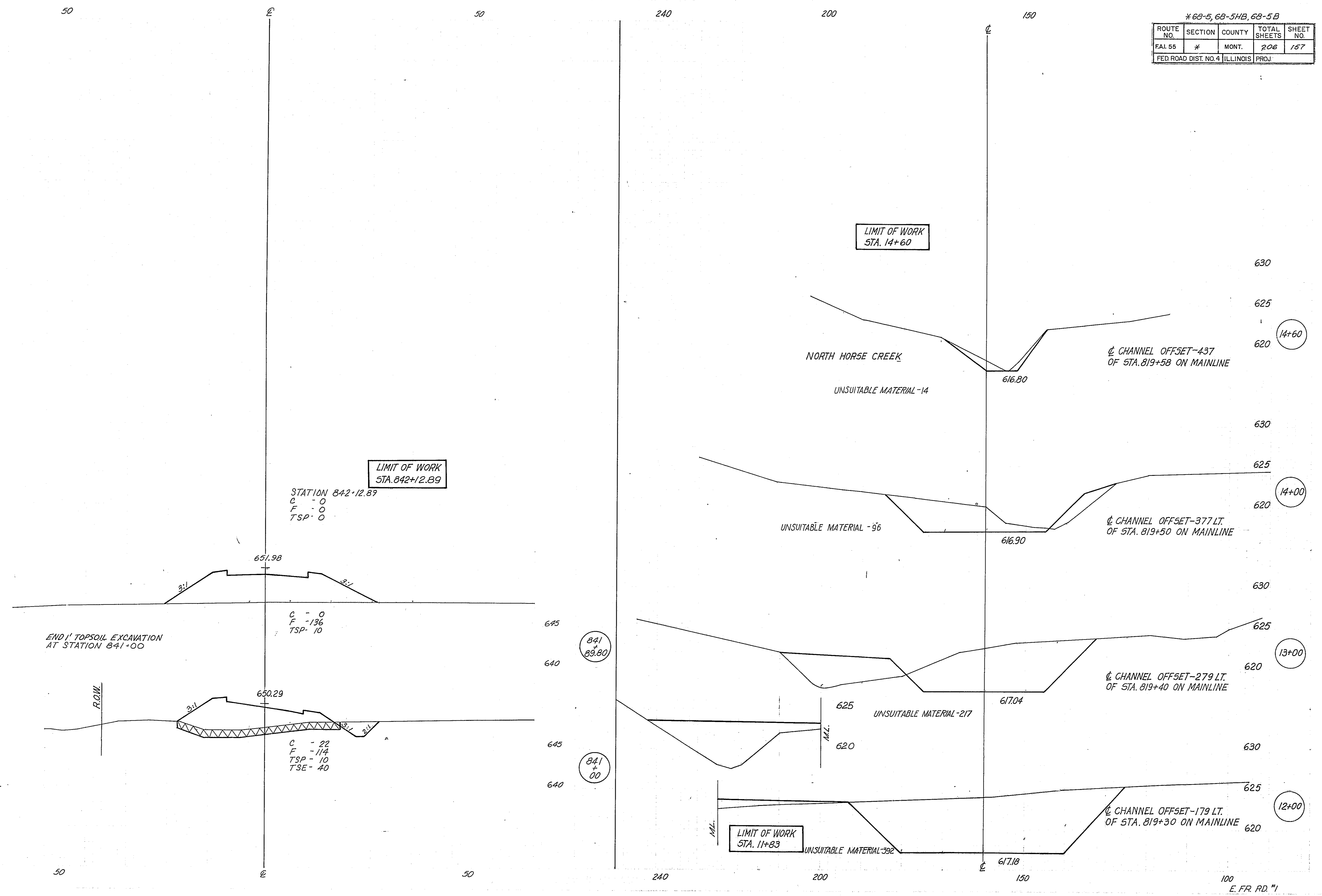
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MONT.	206	156

FED. ROAD DIST. NO. 4 ILLINOIS PROJ.



* 69-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	#	MONT.	206	157
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

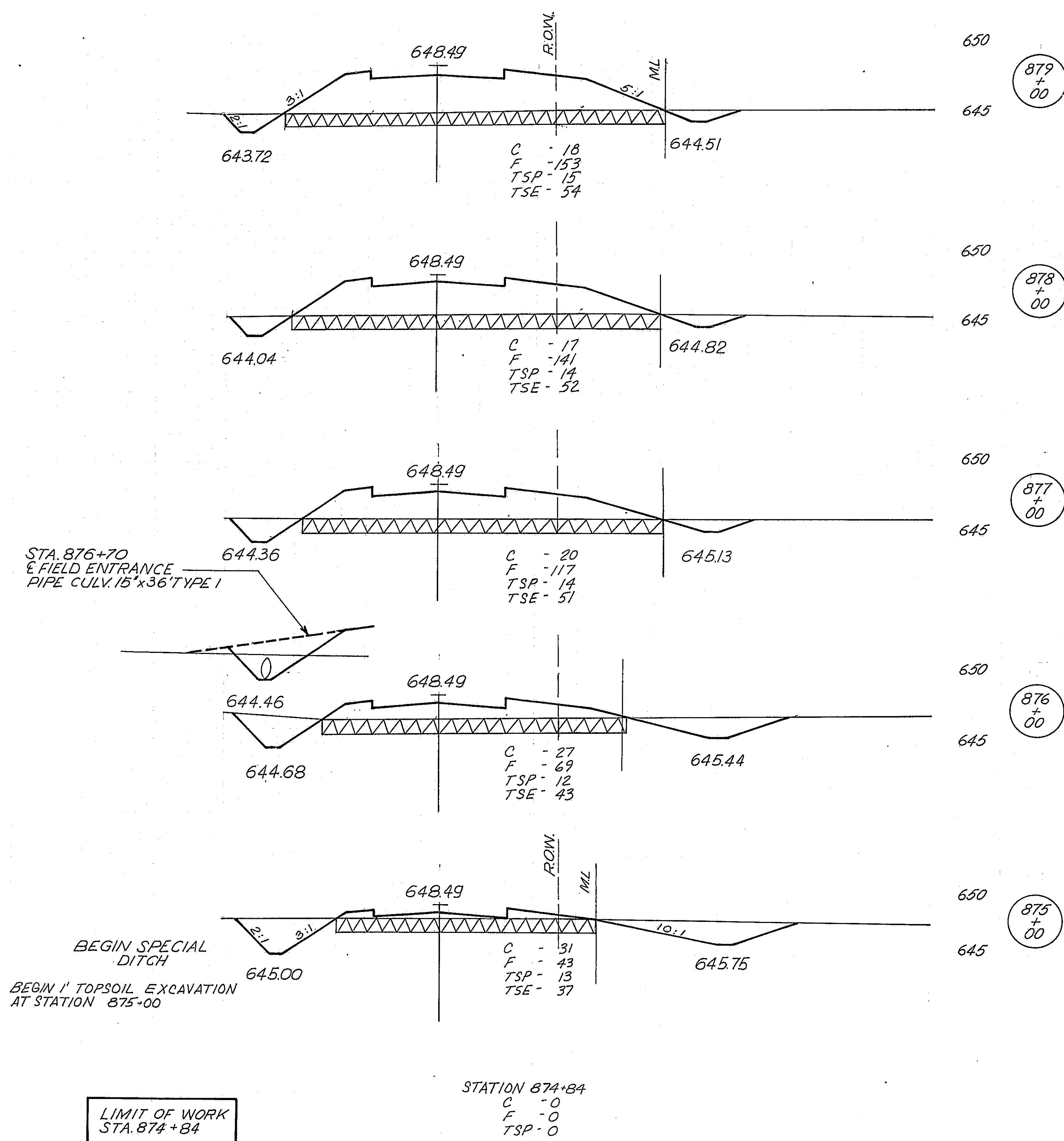


240

200

150

100

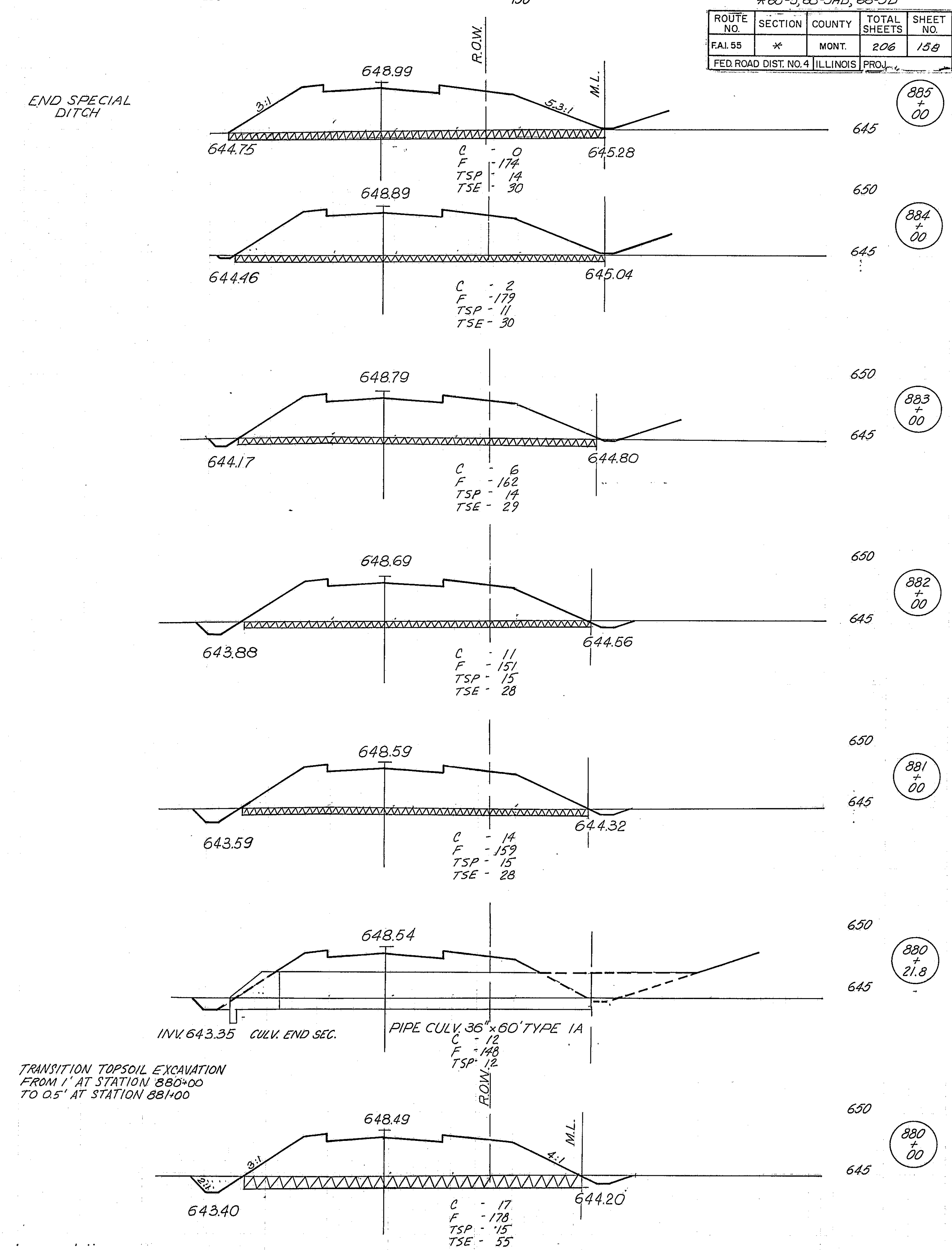


240

200

150

END SPECIAL
DITCH



* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	158
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

240

200

150

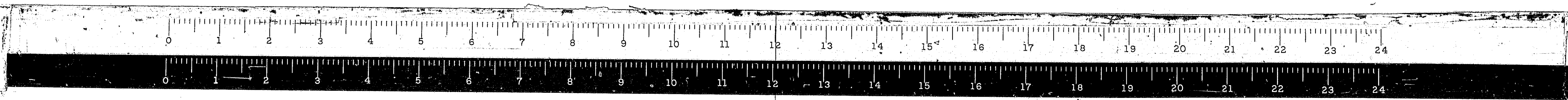
100

240

200

150

100
SERVICE DR. #1

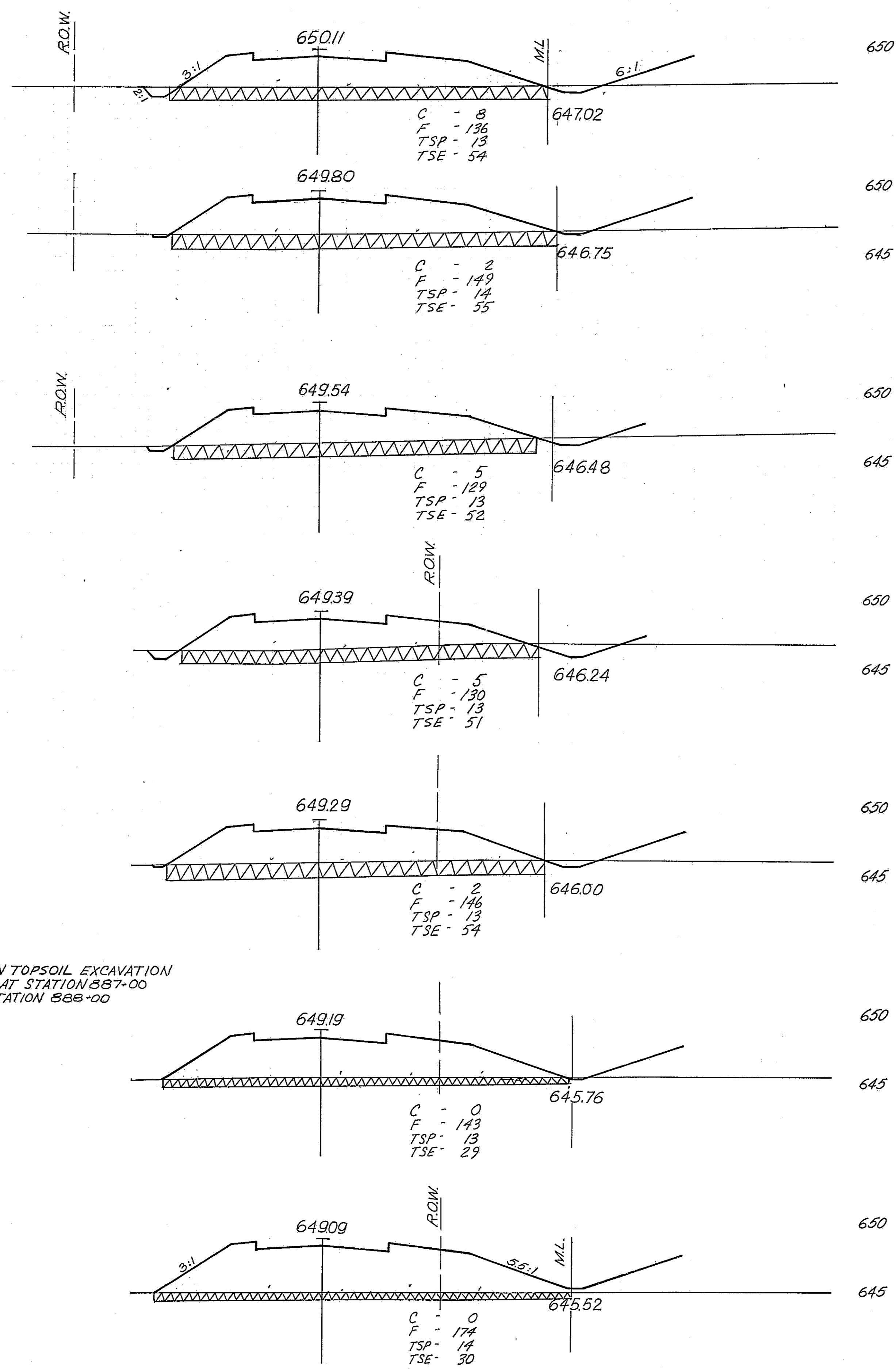


240

200

150

100



TRANSITION TOPSOIL EXCAVATION
FROM 0.5' AT STATION 887+00
TO 1' AT STATION 888+00

892 + 00
891 + 00
890 + 00
889 + 00
888 + 00
887 + 00
886 + 00

240

200

150

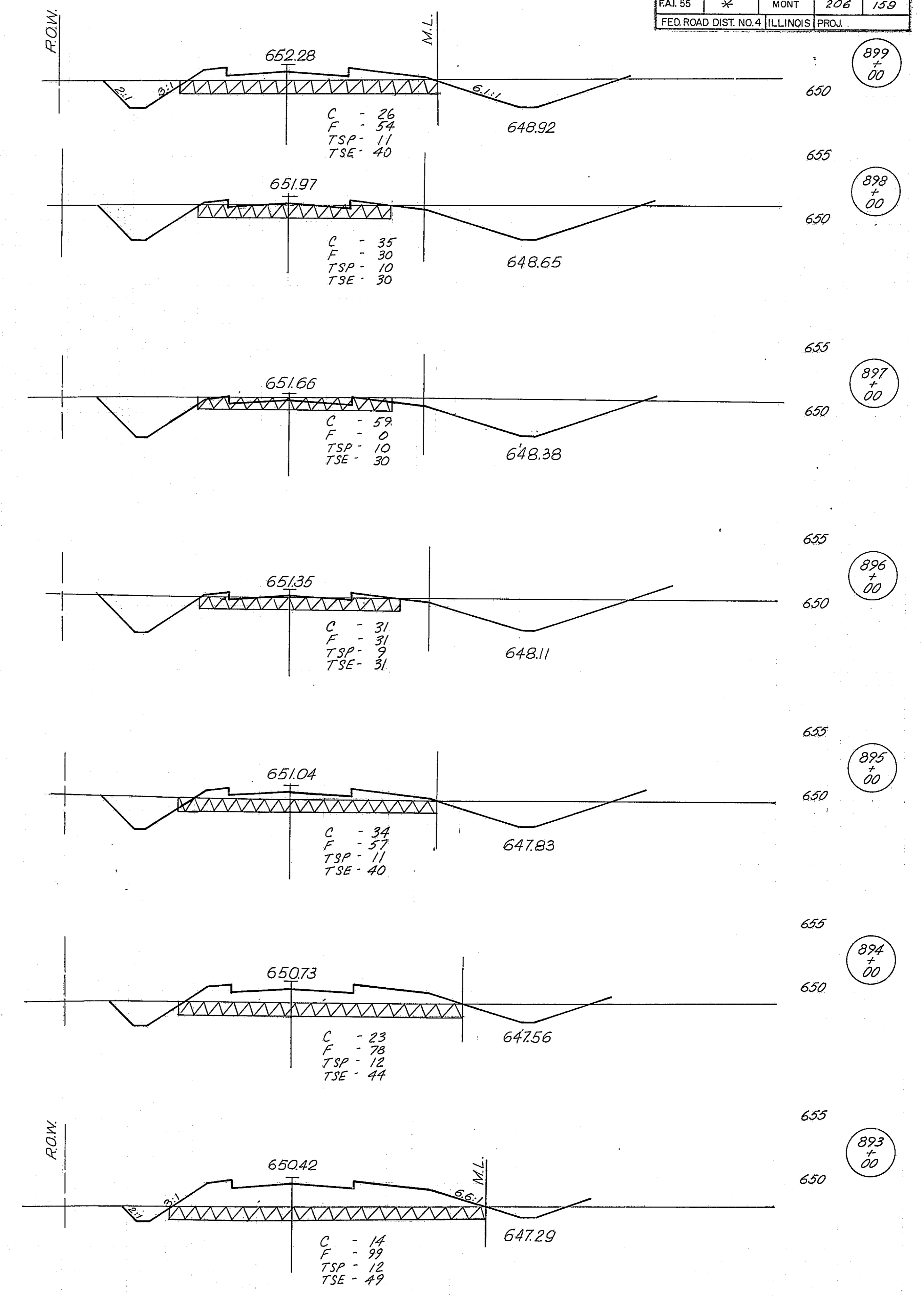
100

240

200

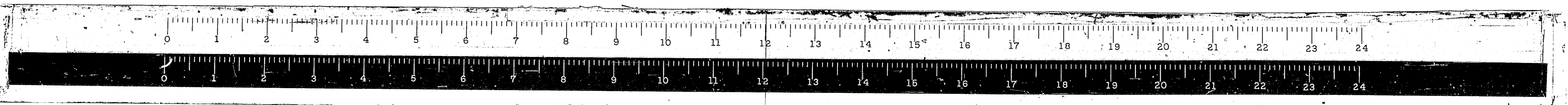
150

*88-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT	206	159
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

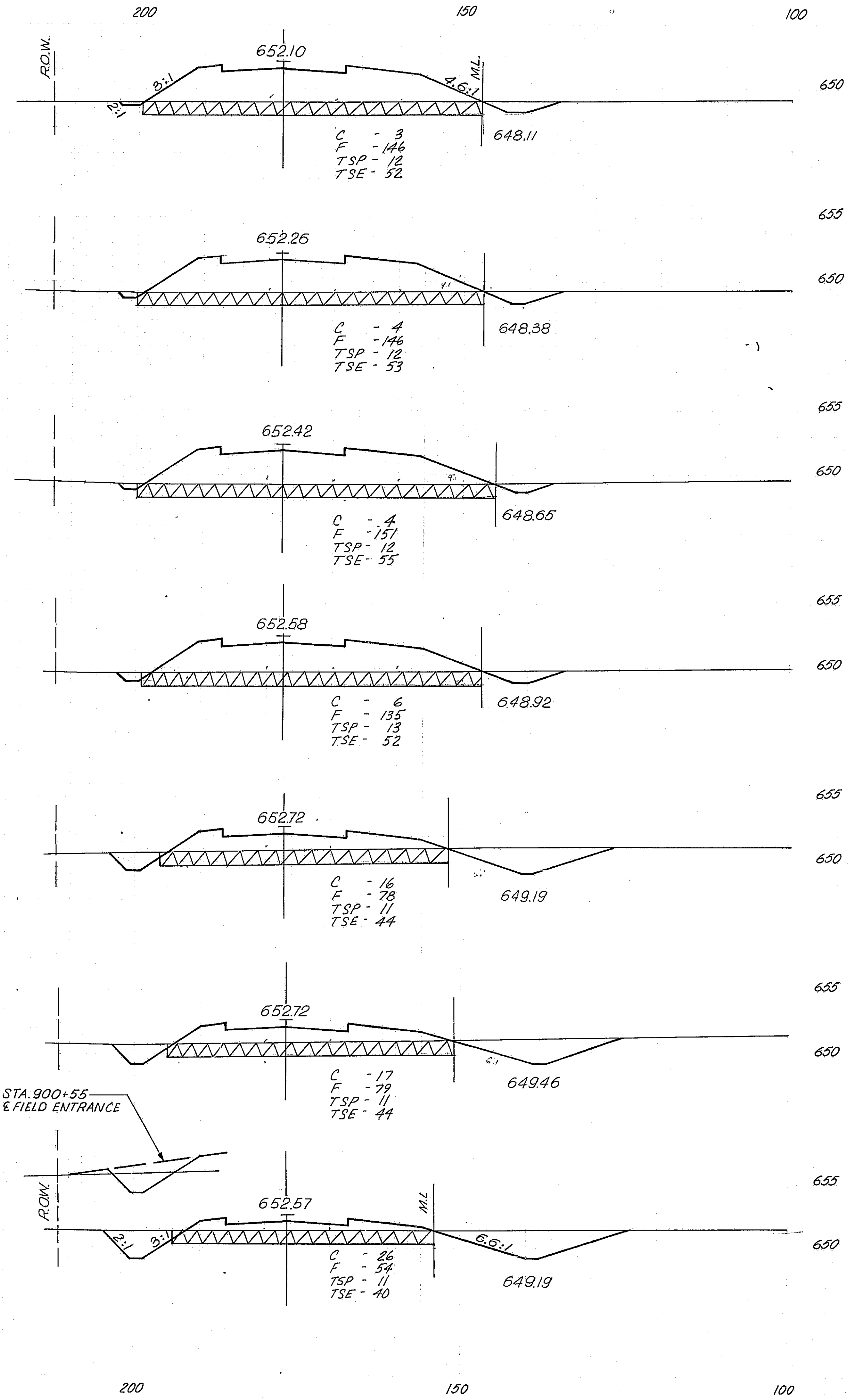


899 + 00
898 + 00
897 + 00
896 + 00
895 + 00
894 + 00
893 + 00

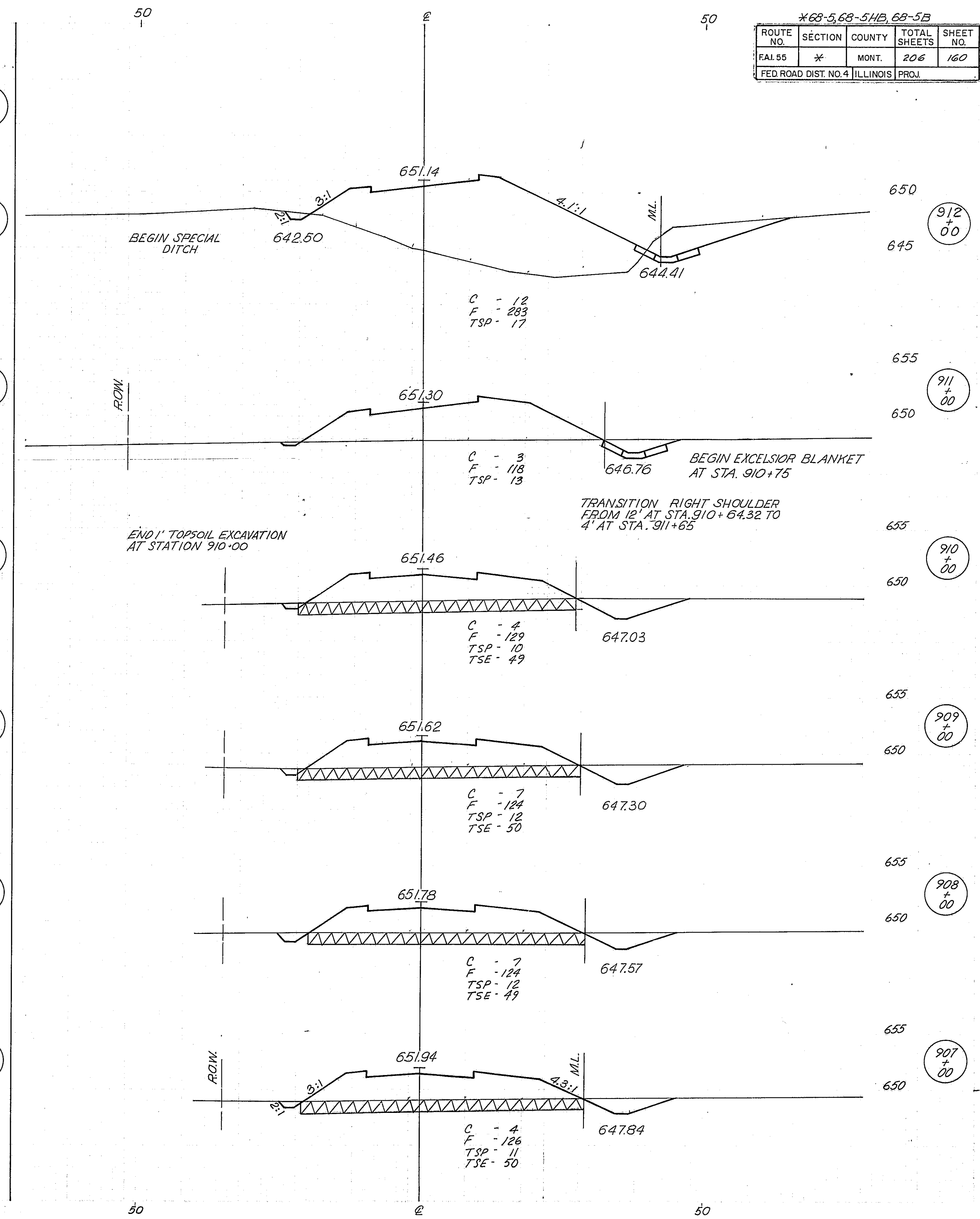
100 SERVICE DR. "1"



240



50

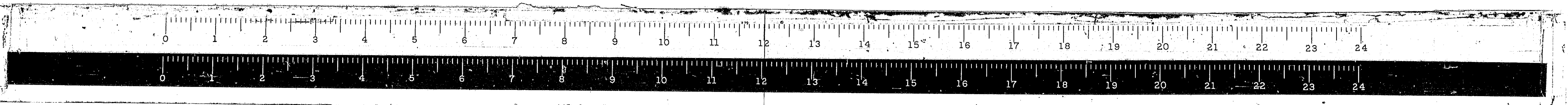


*68-5,68-5HB,68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	160

FED. ROAD DIST. NO. 4 ILLINOIS PROJ.

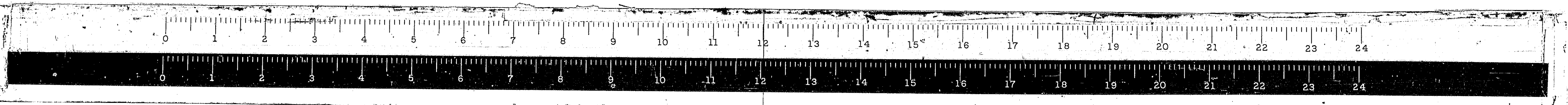
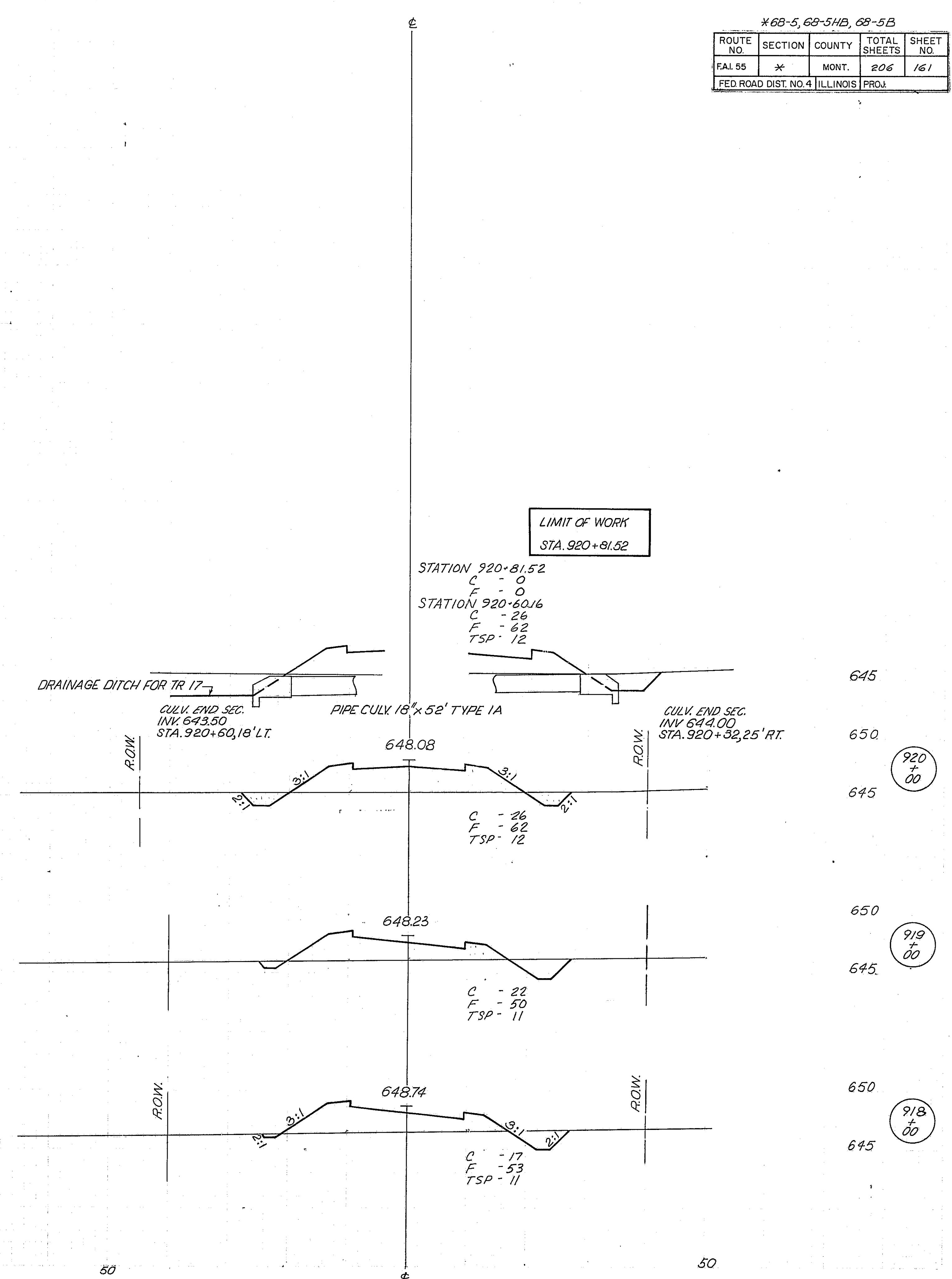
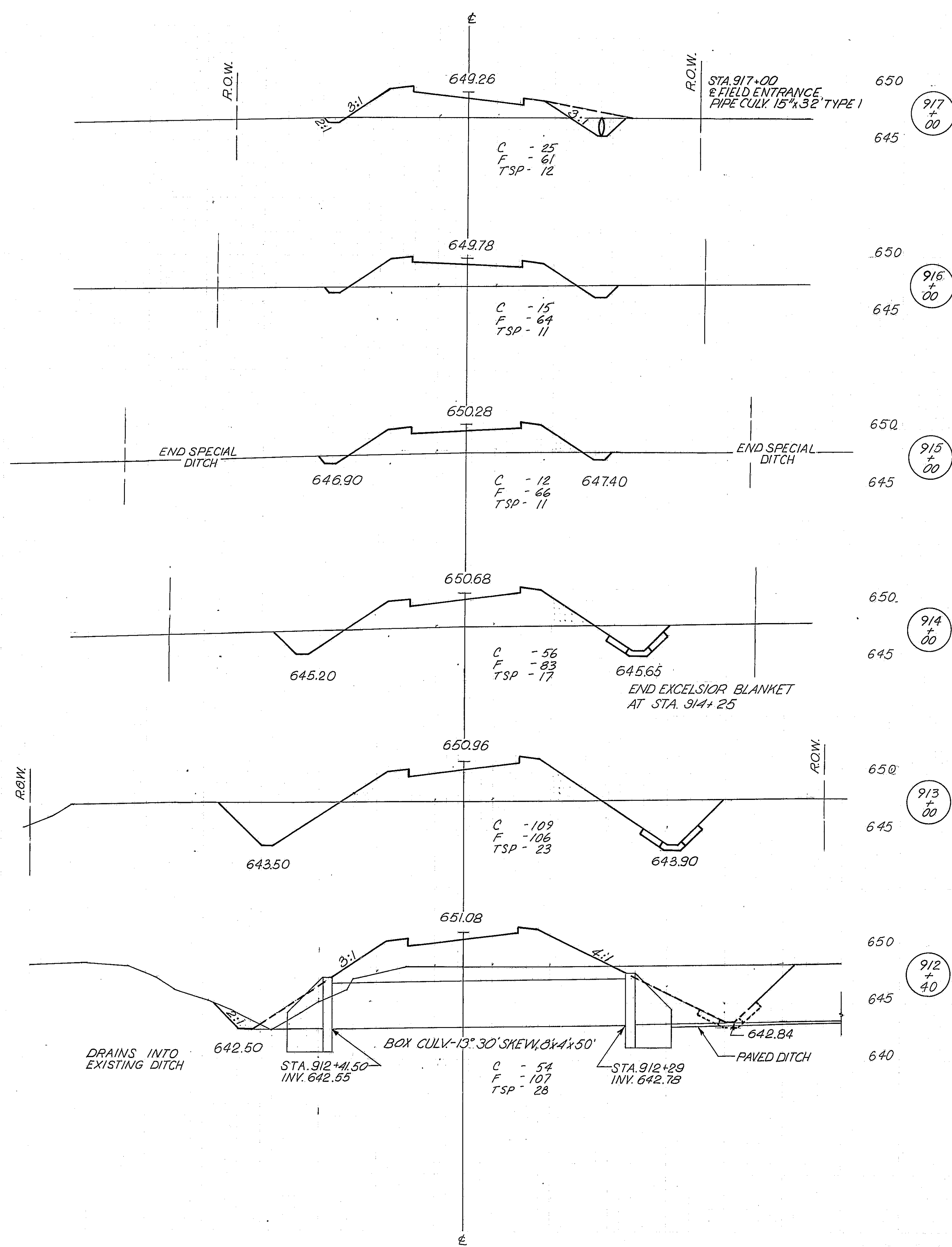
240



SERVICE DR. #1

*68-5, 68-5HB, 68-5B

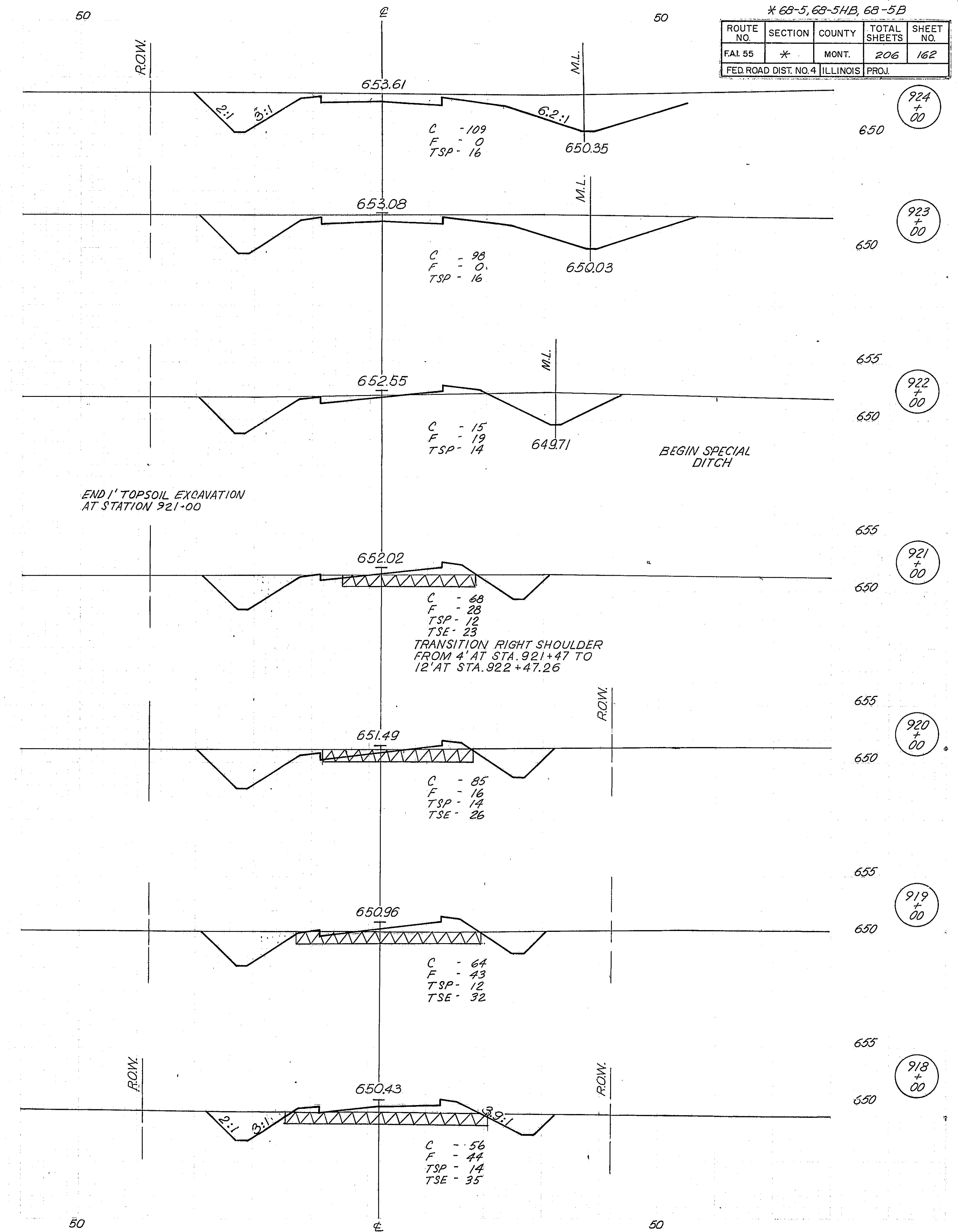
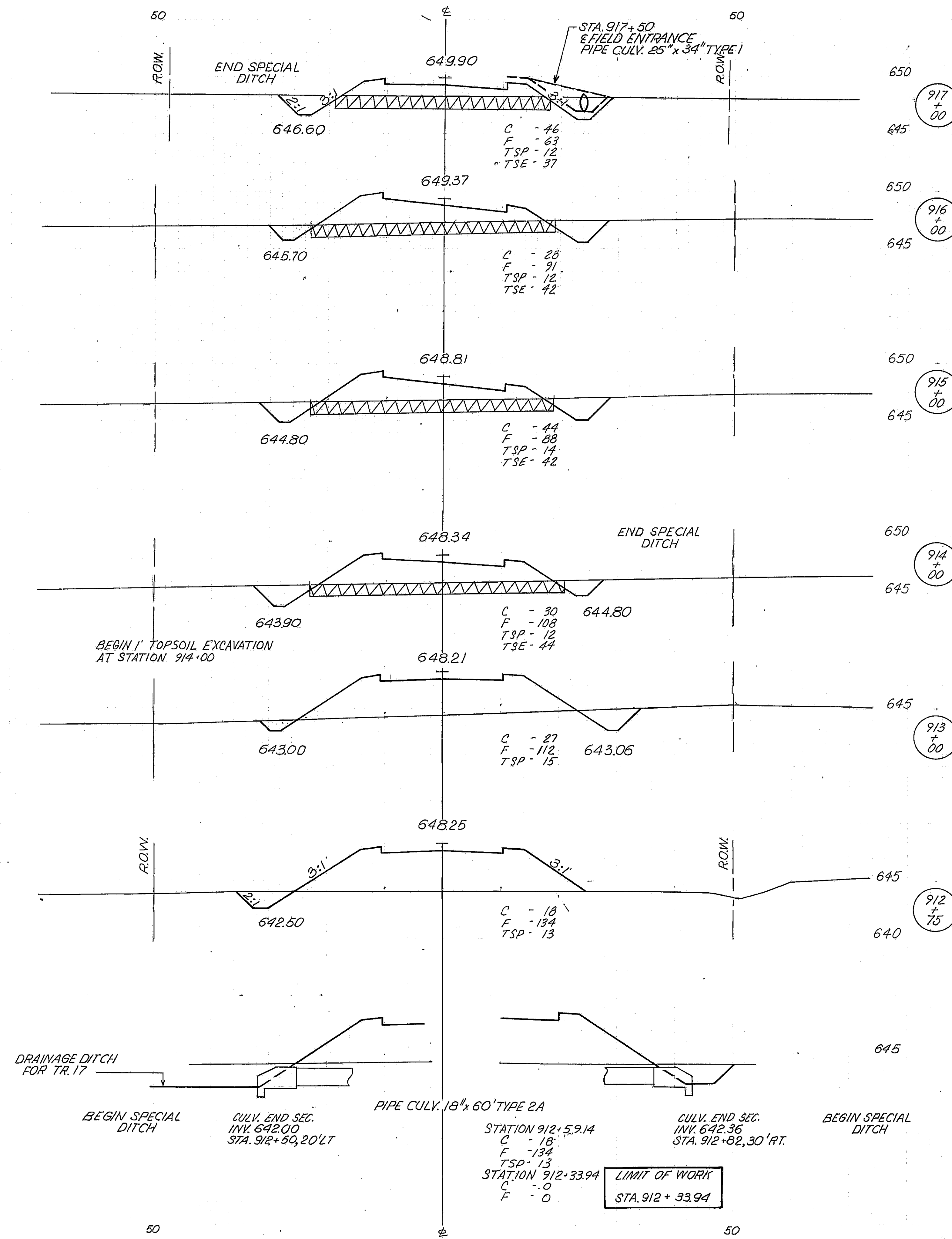
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	161
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



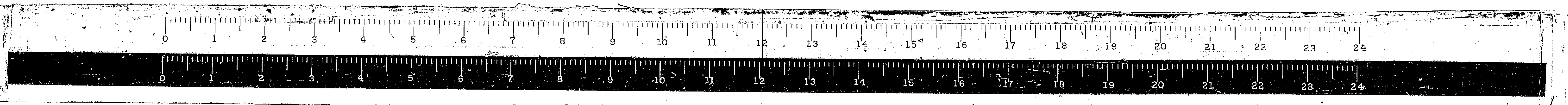
SERVICE DR. '1

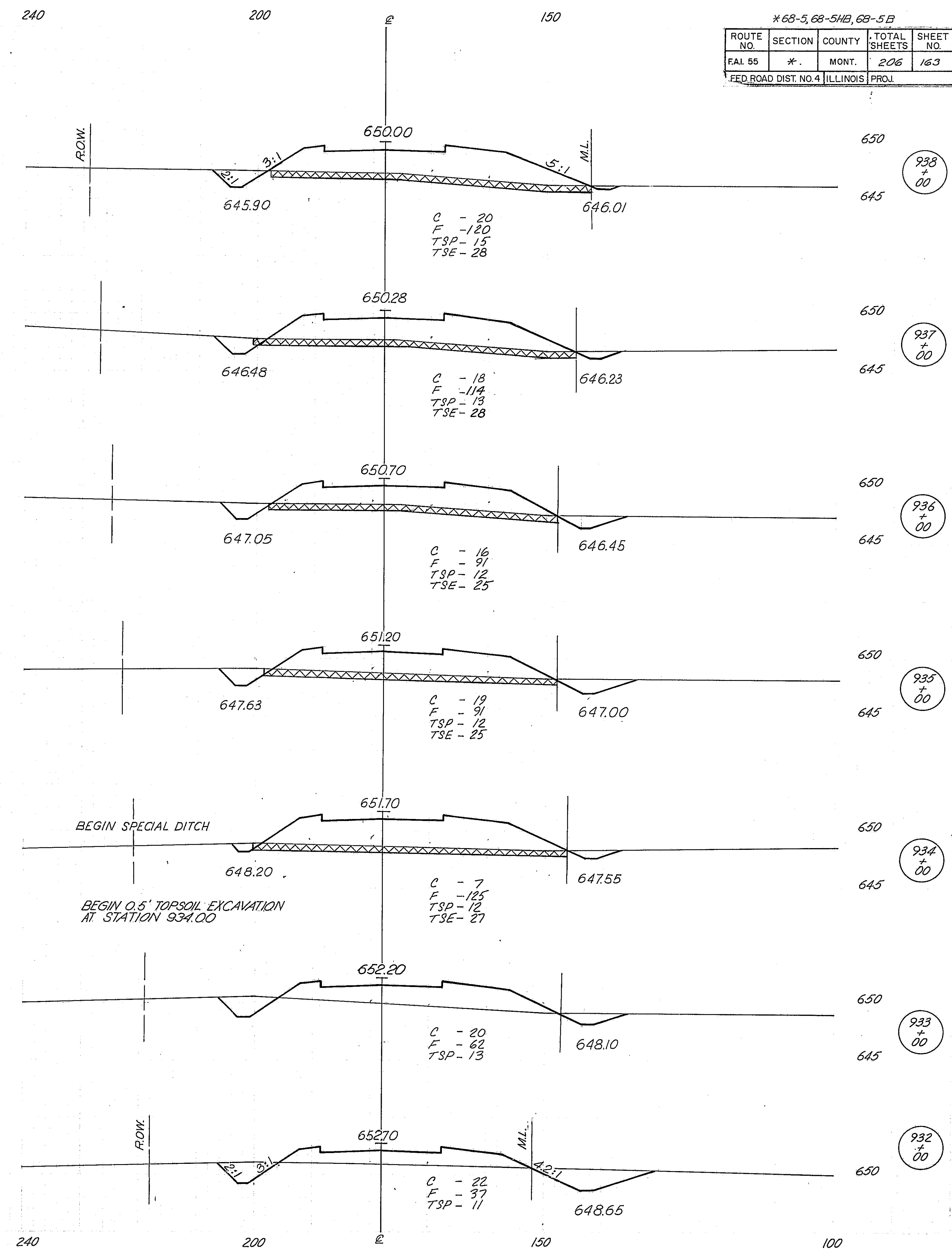
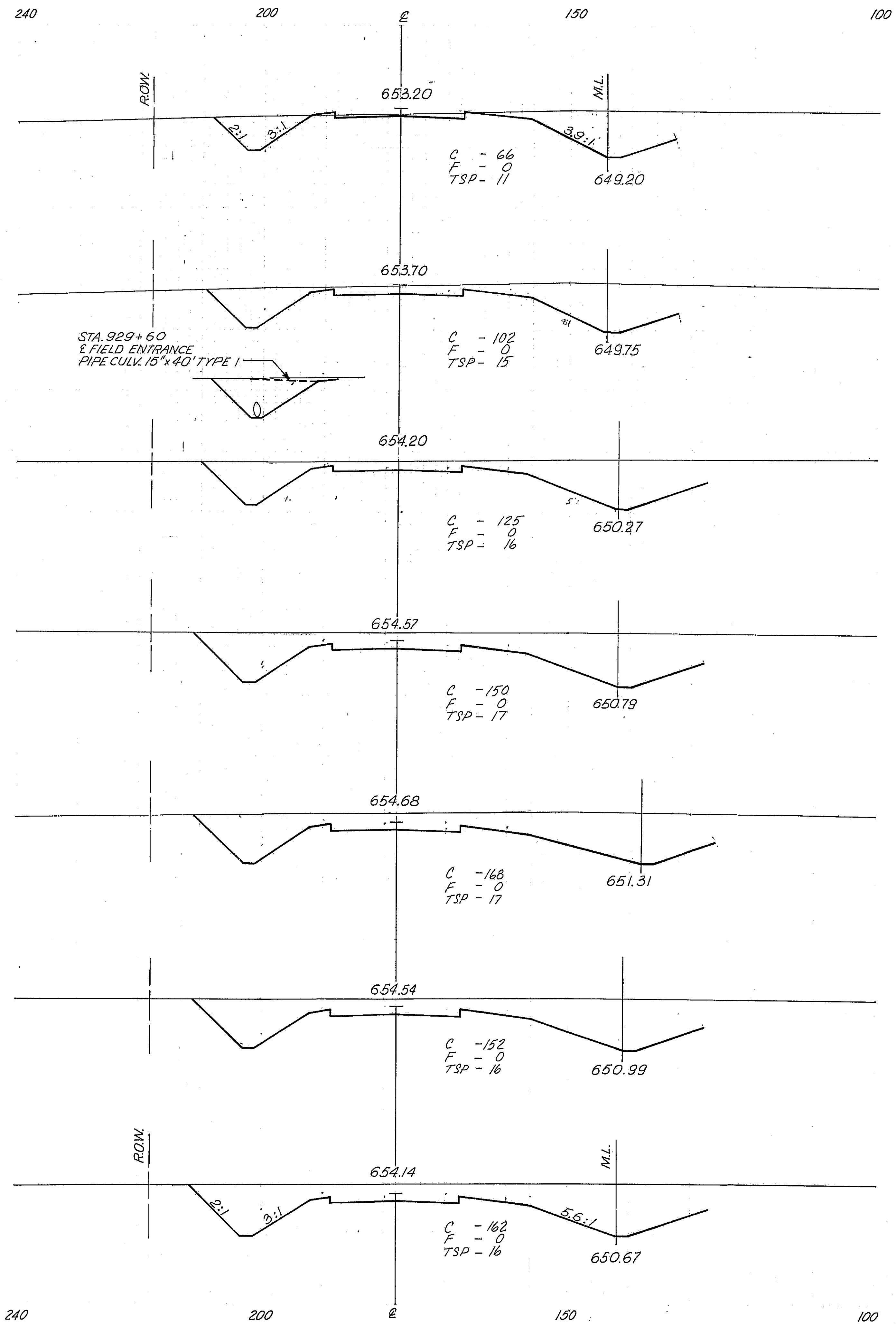
* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT.	206	162
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



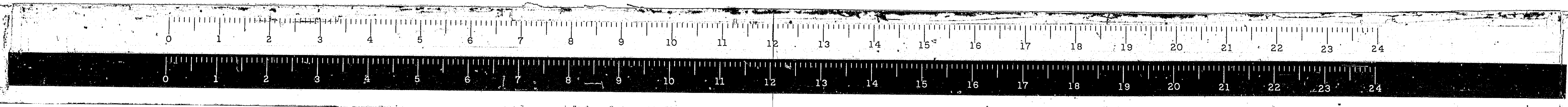
E. FR. RD. 72

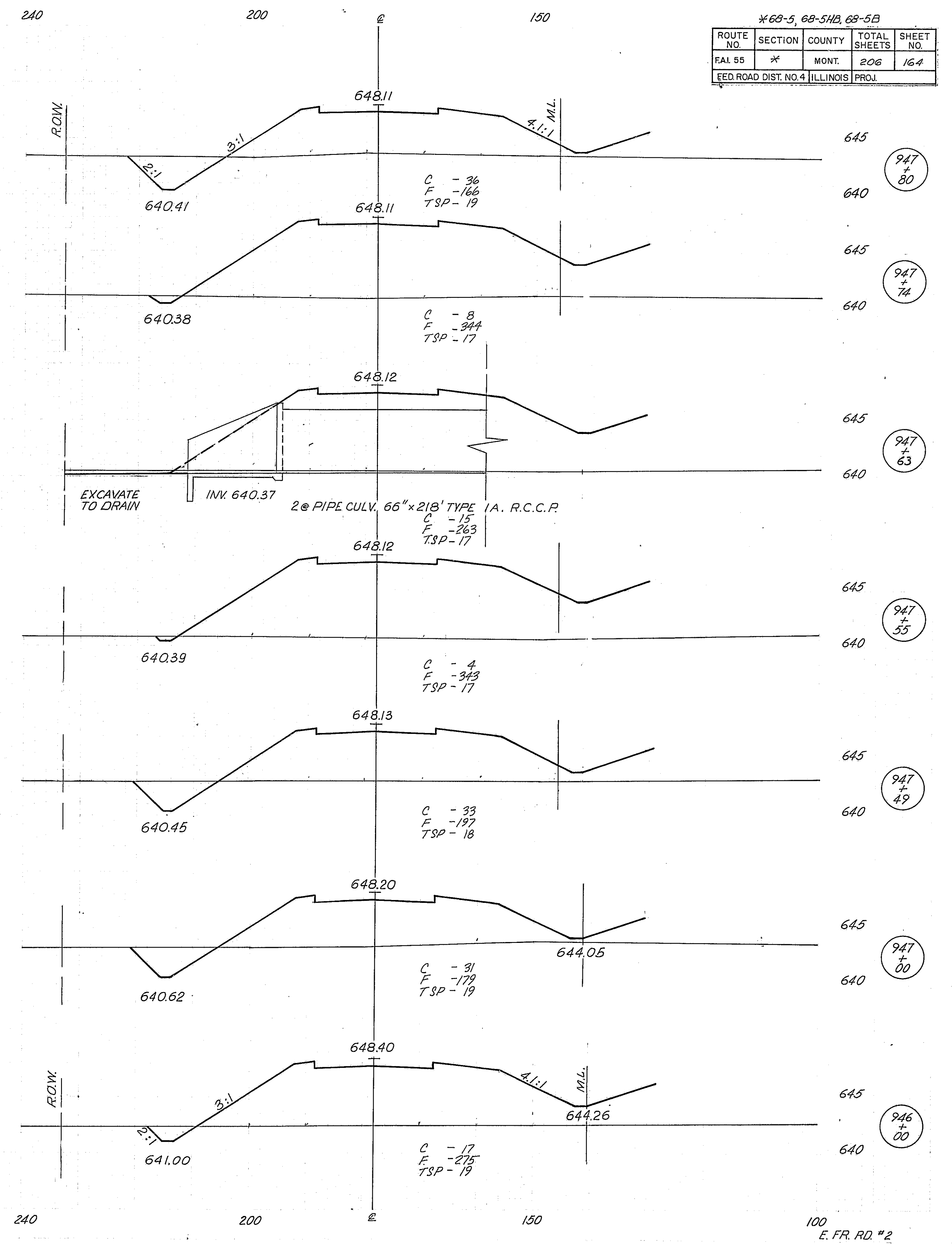
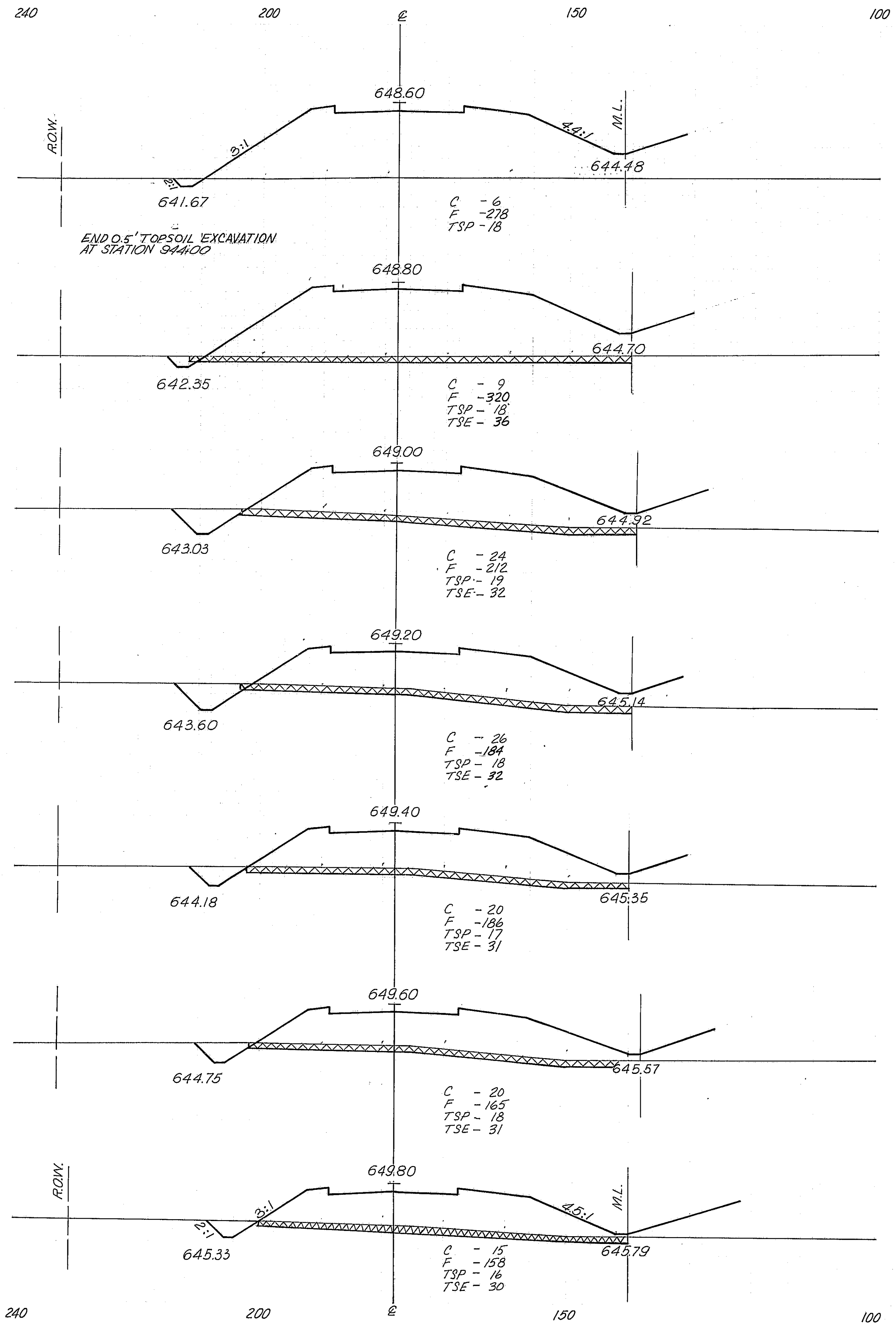




*68-5, 68-54B, 68-5E

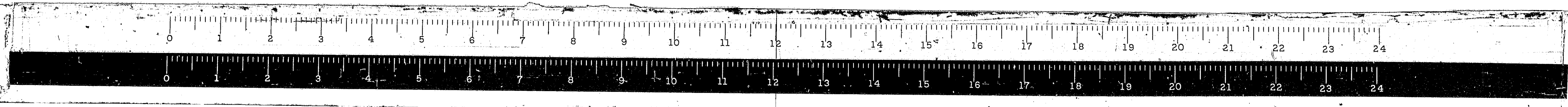
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	163
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



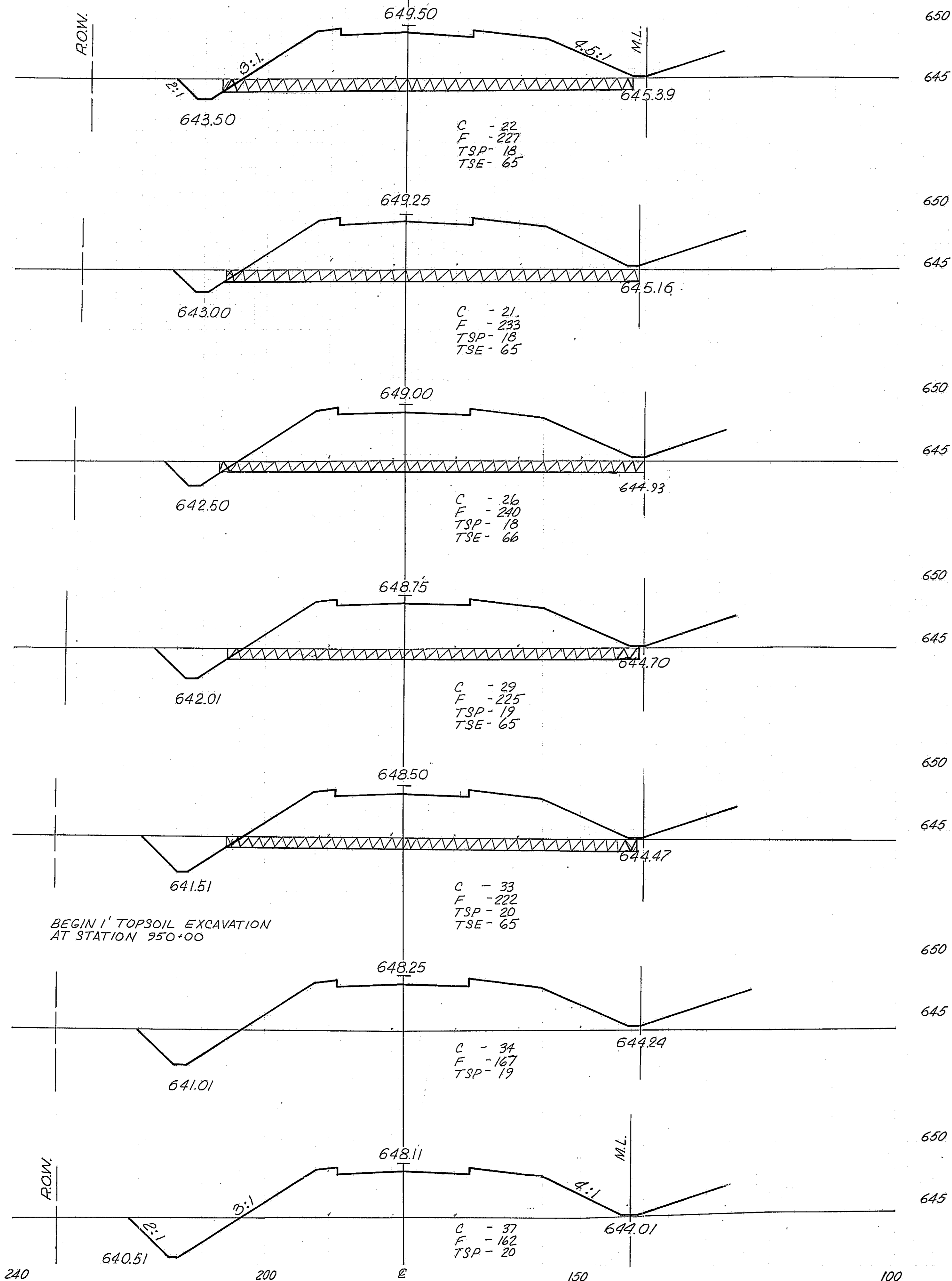


*68-5, 68-SHA, 68-5B

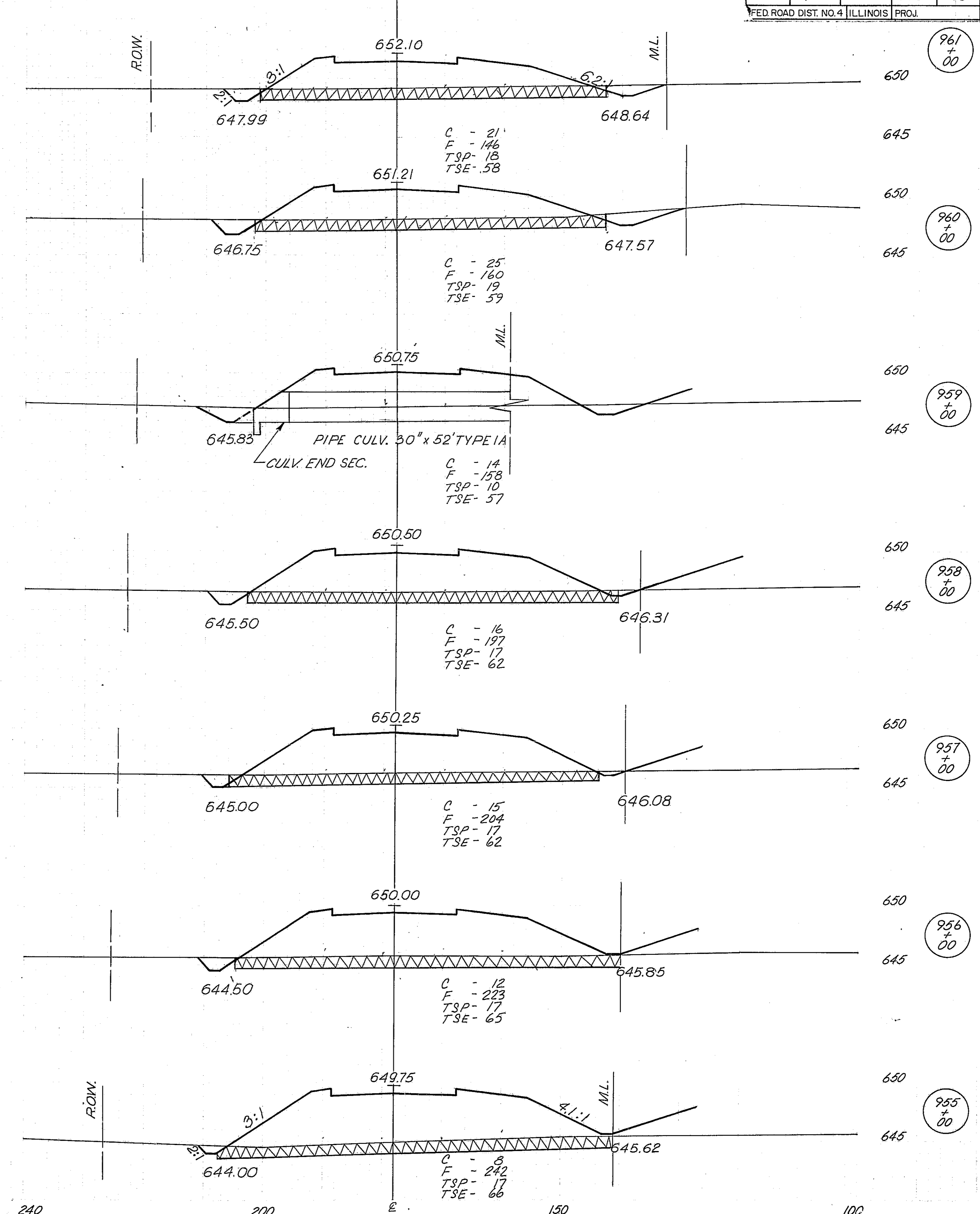
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	164
EEO ROAD DIST. NO. 4 ILLINOIS PROJ.				



240 200 E 150 100



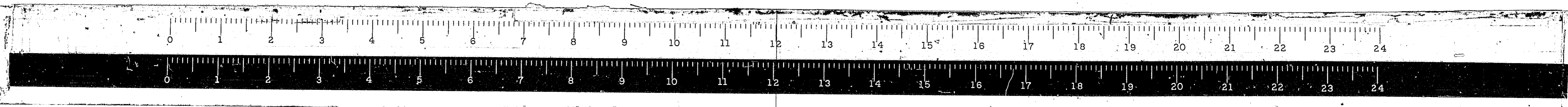
240 200 E 150 100



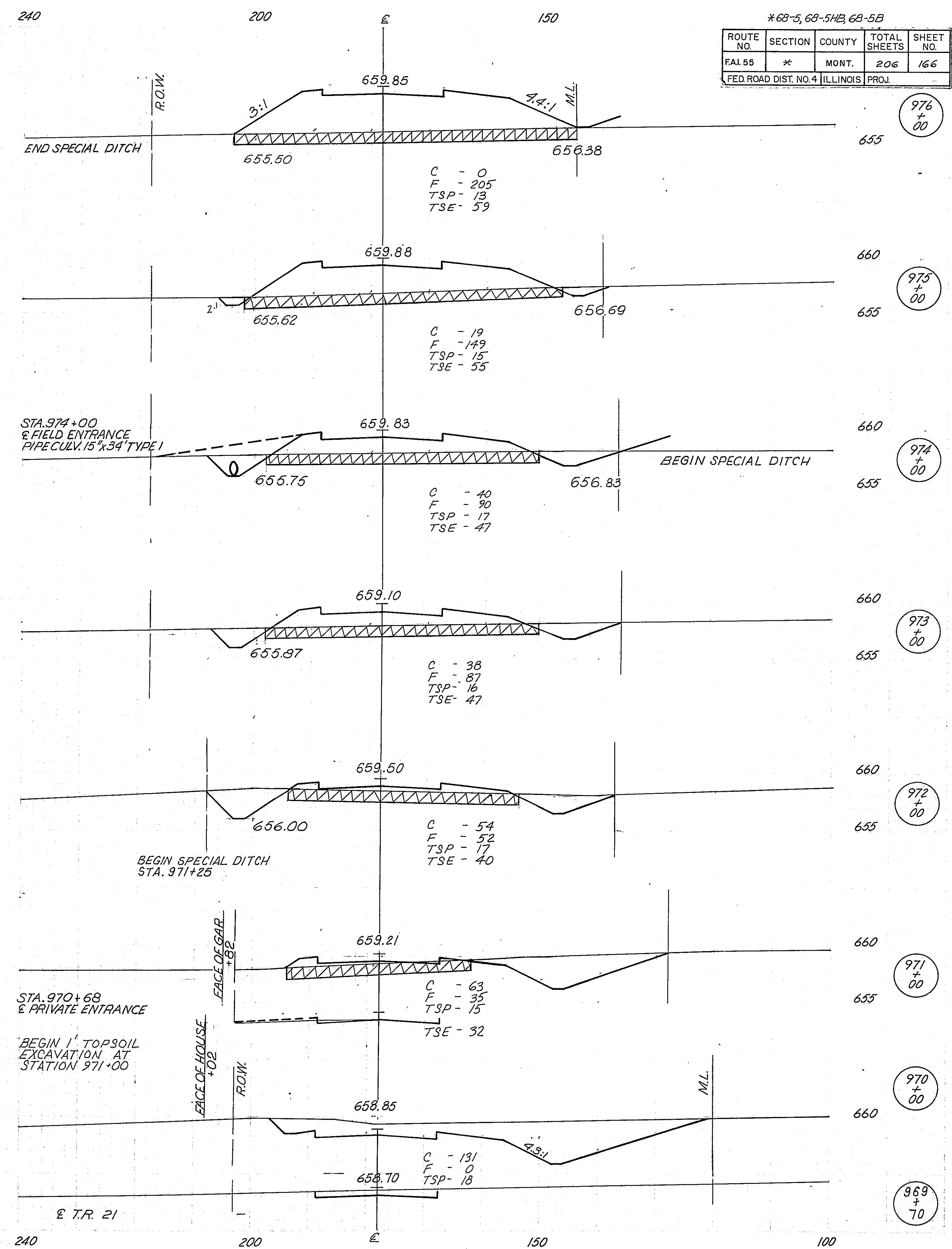
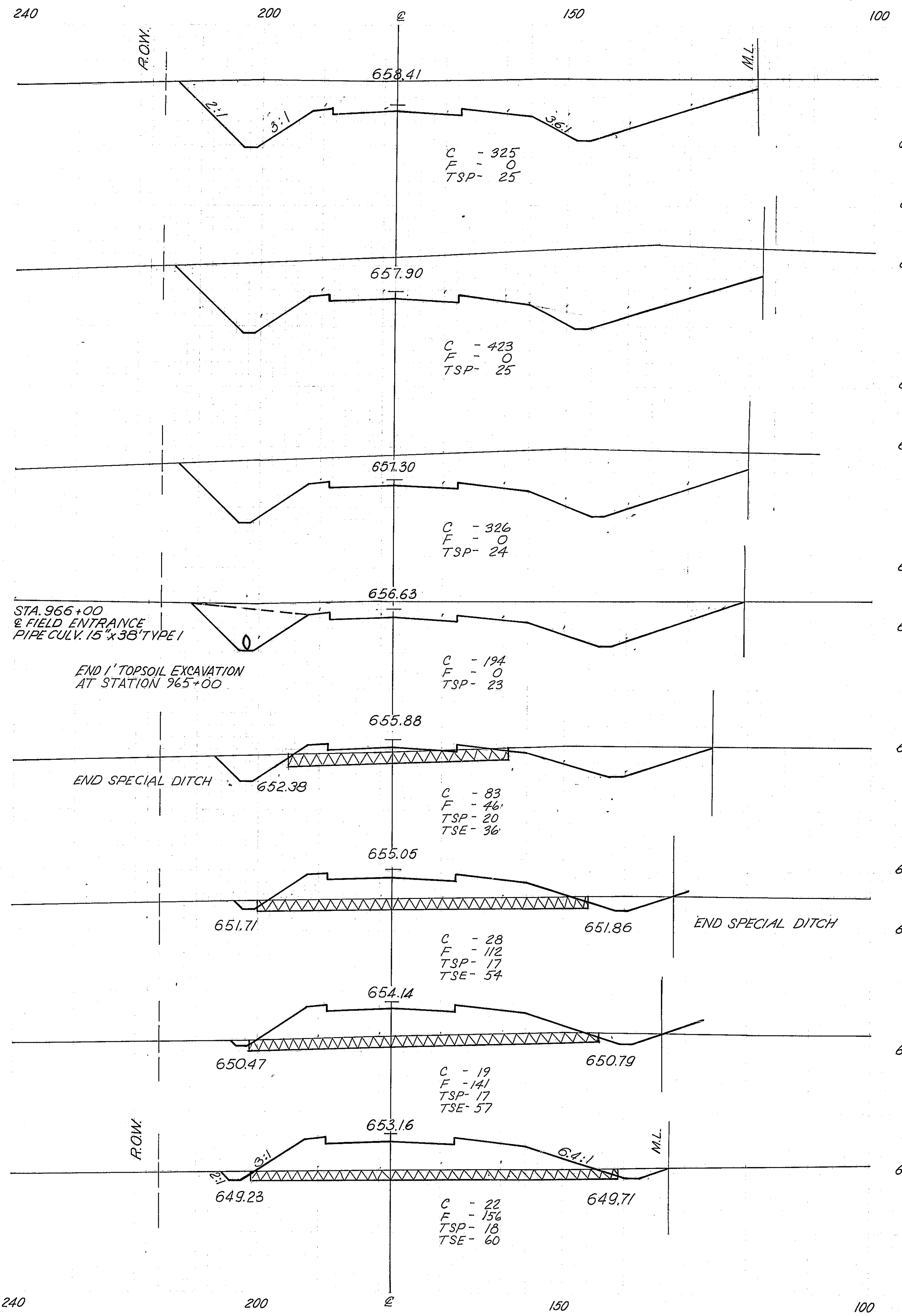
*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	185

FED. ROAD DIST. NO. 4 ILLINOIS PROJ.

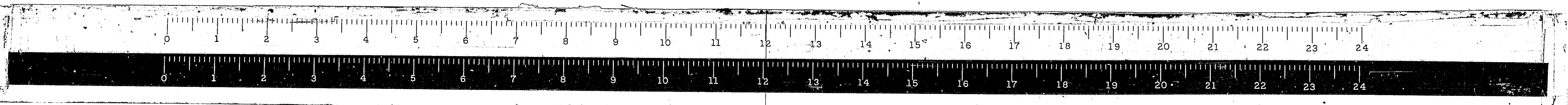


100 E. FR. RD. #2

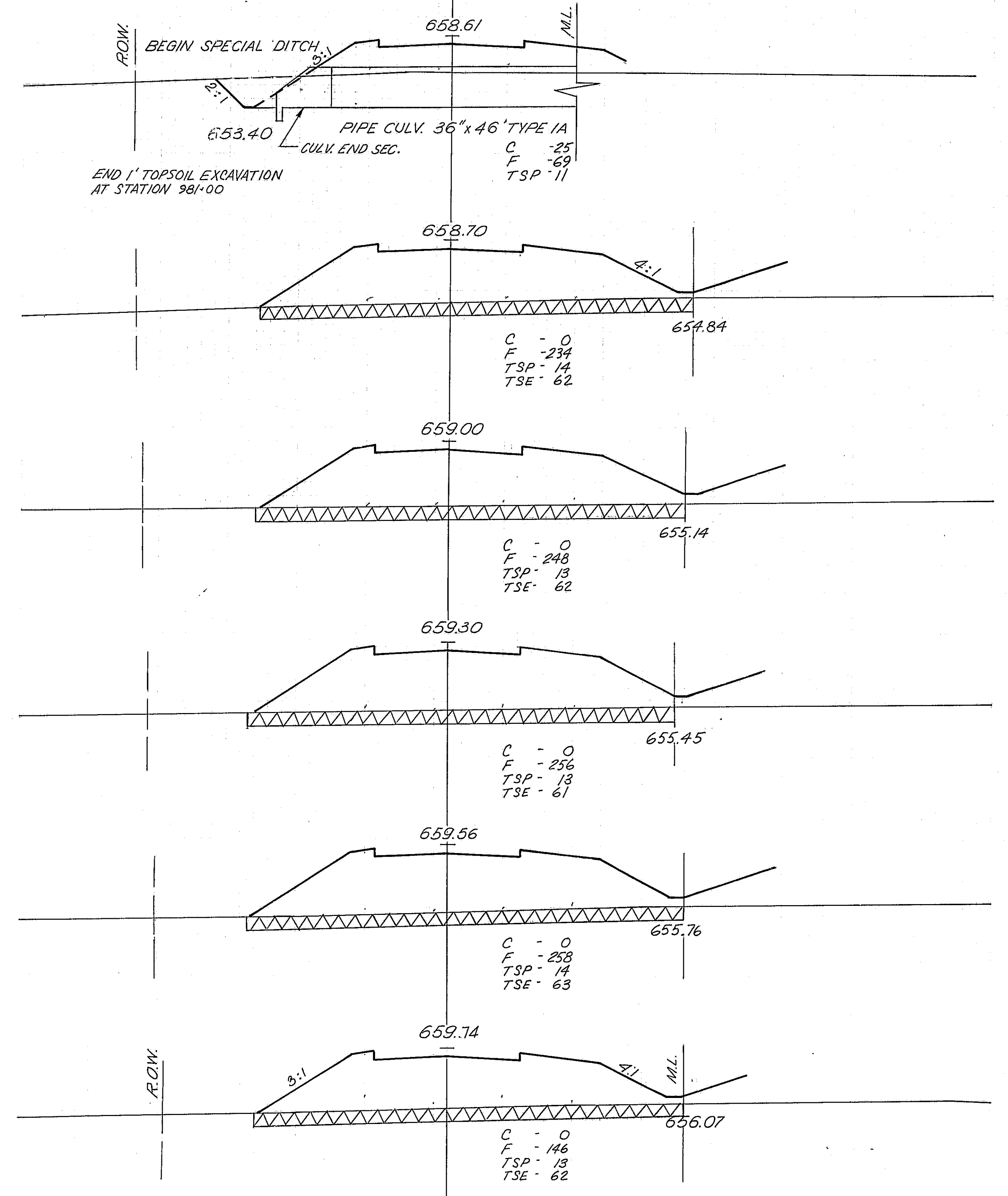


*68-5, 68-5H2, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONT.	206	166
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



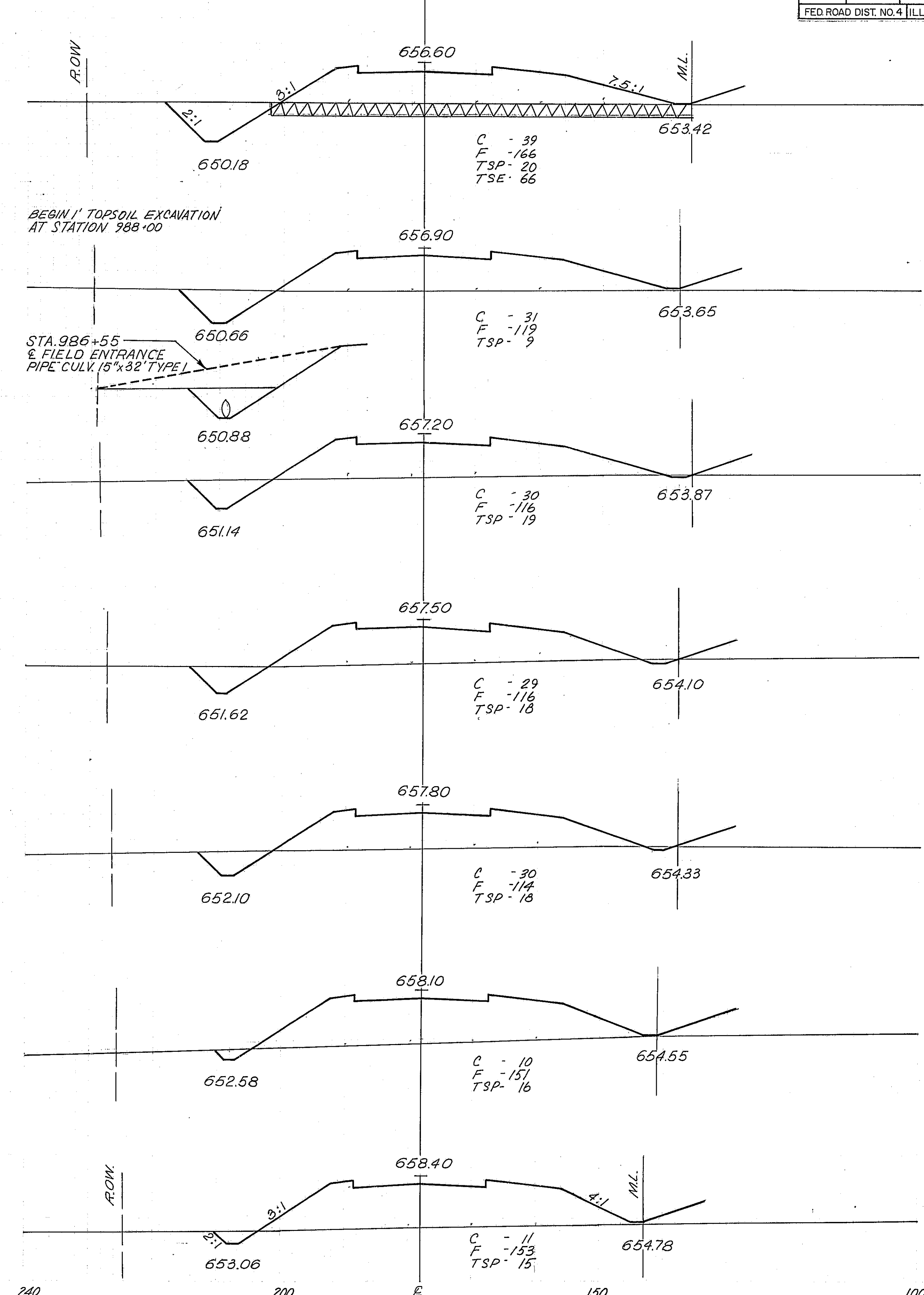
240 200 150 100



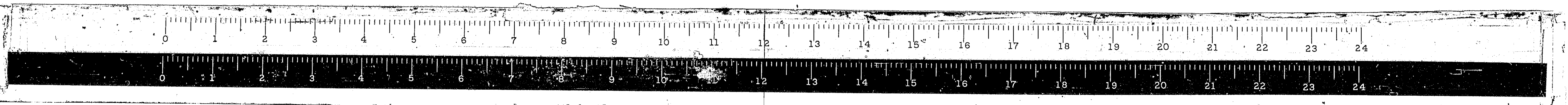
240 200 150 100

240 200 150 100

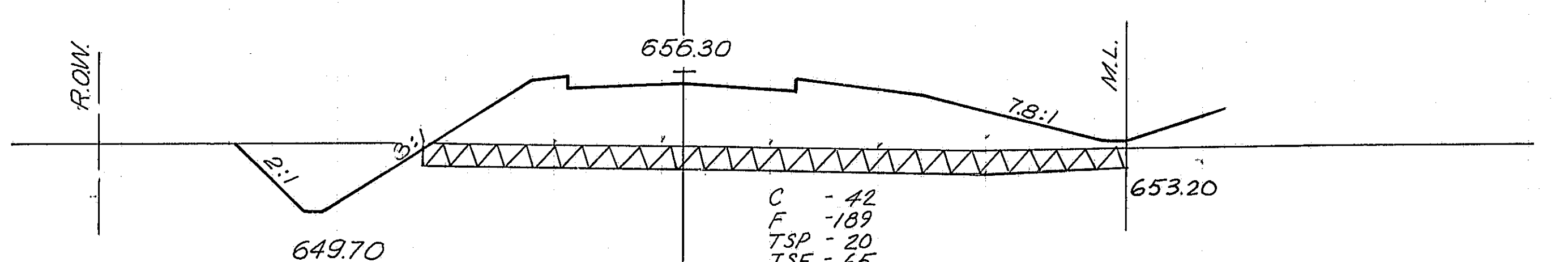
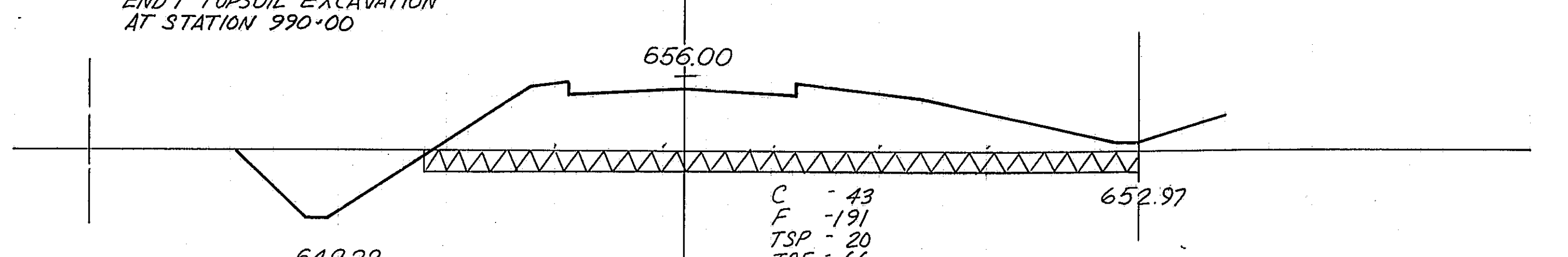
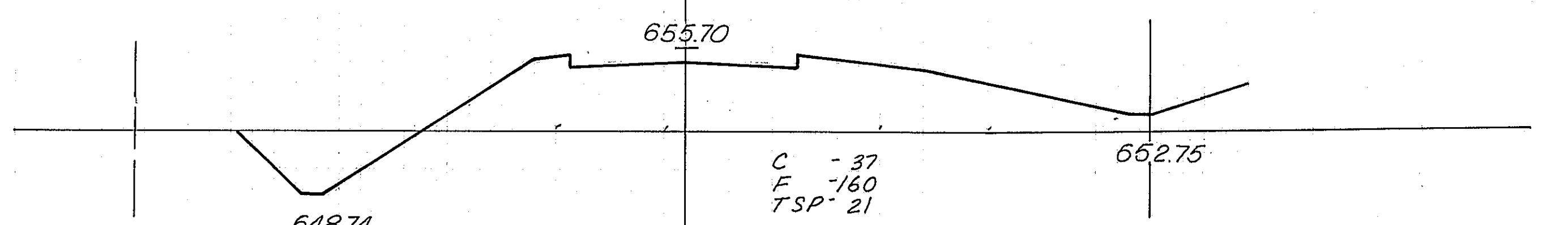
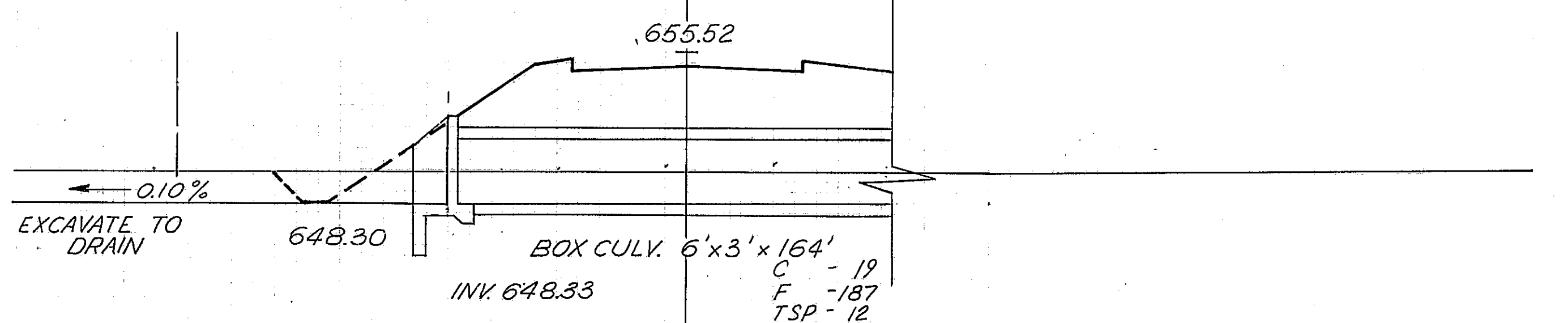
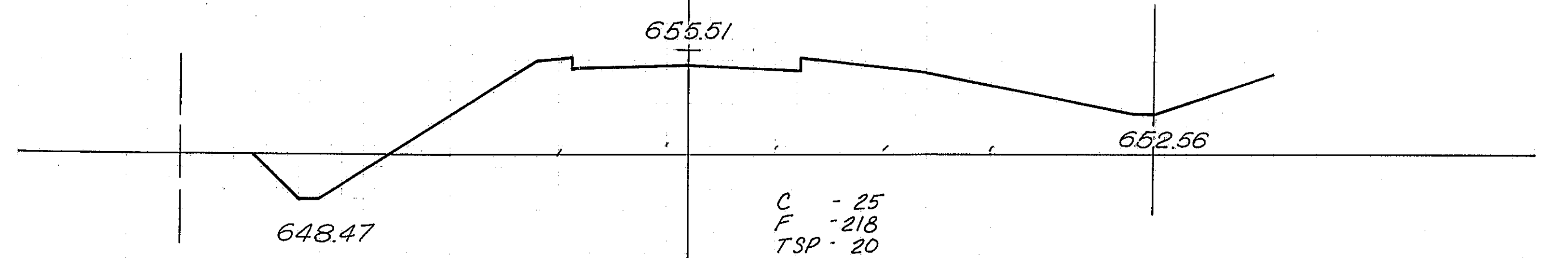
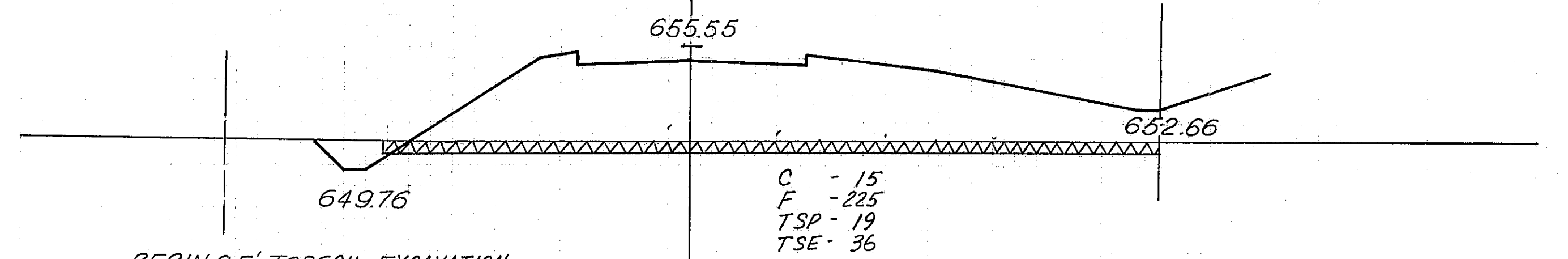
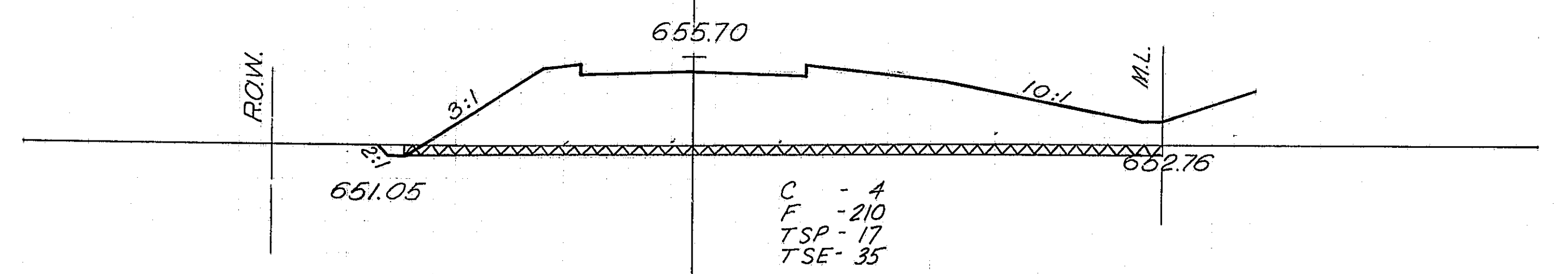
* 68-5, 69-5A, 69-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	167
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



E. FR. RD. #2



240 200 150 100

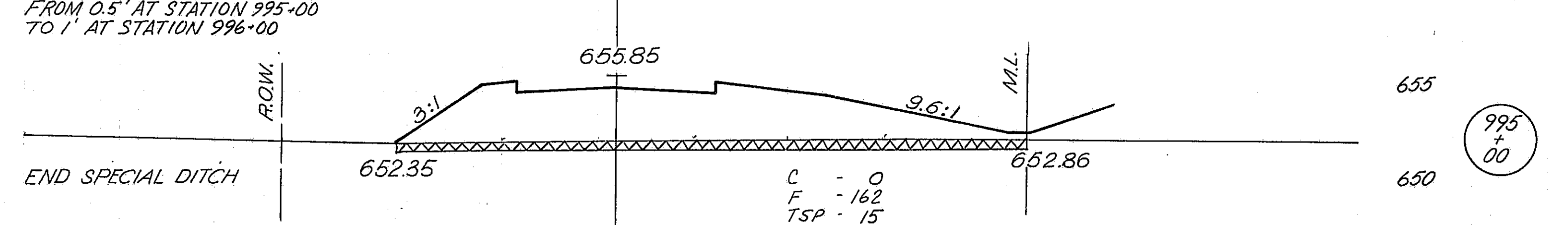
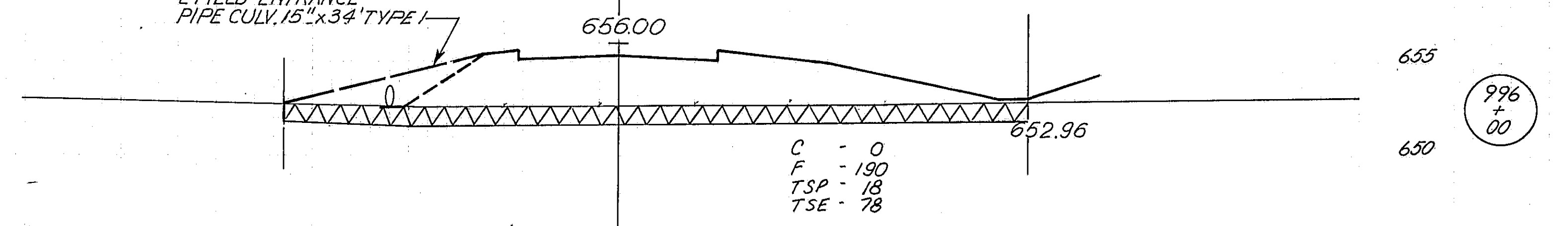
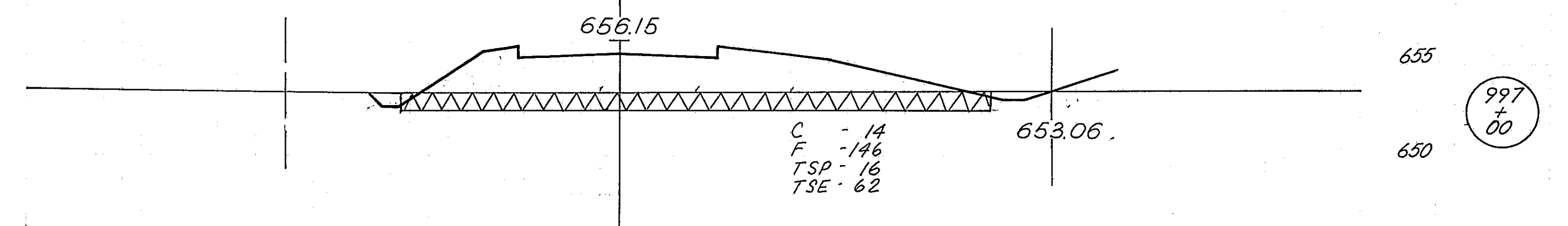
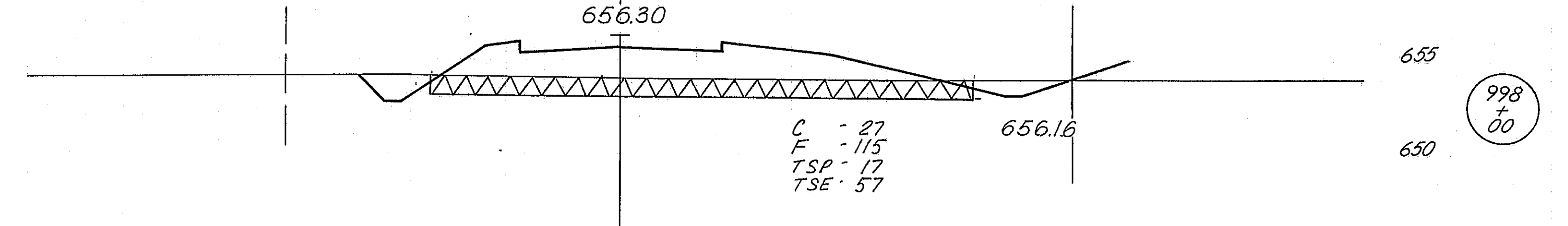
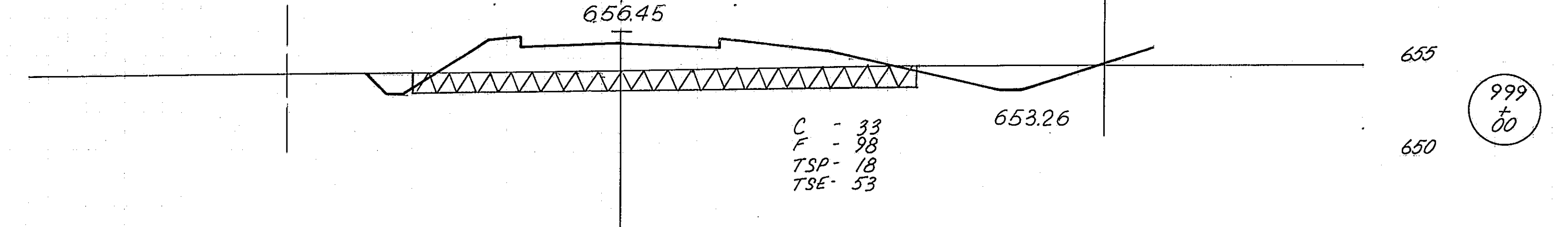
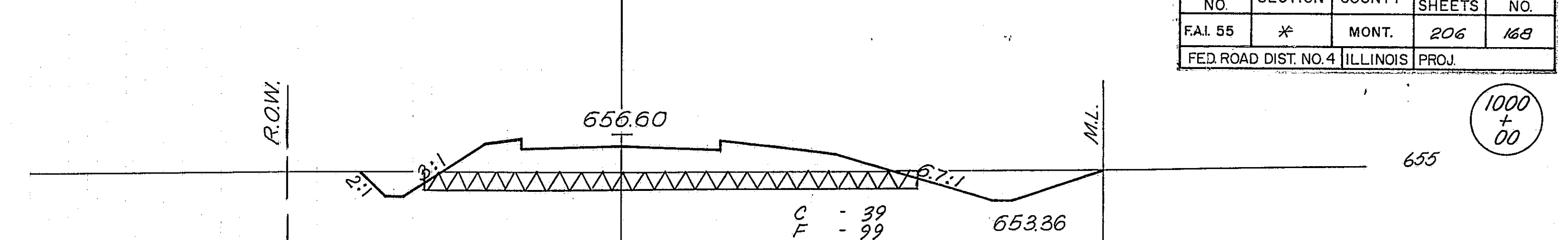


240 200 150 100

240 200 150 100

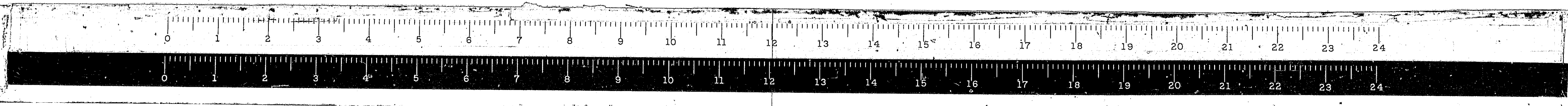
*68-5, 68-5HB, 68-5B

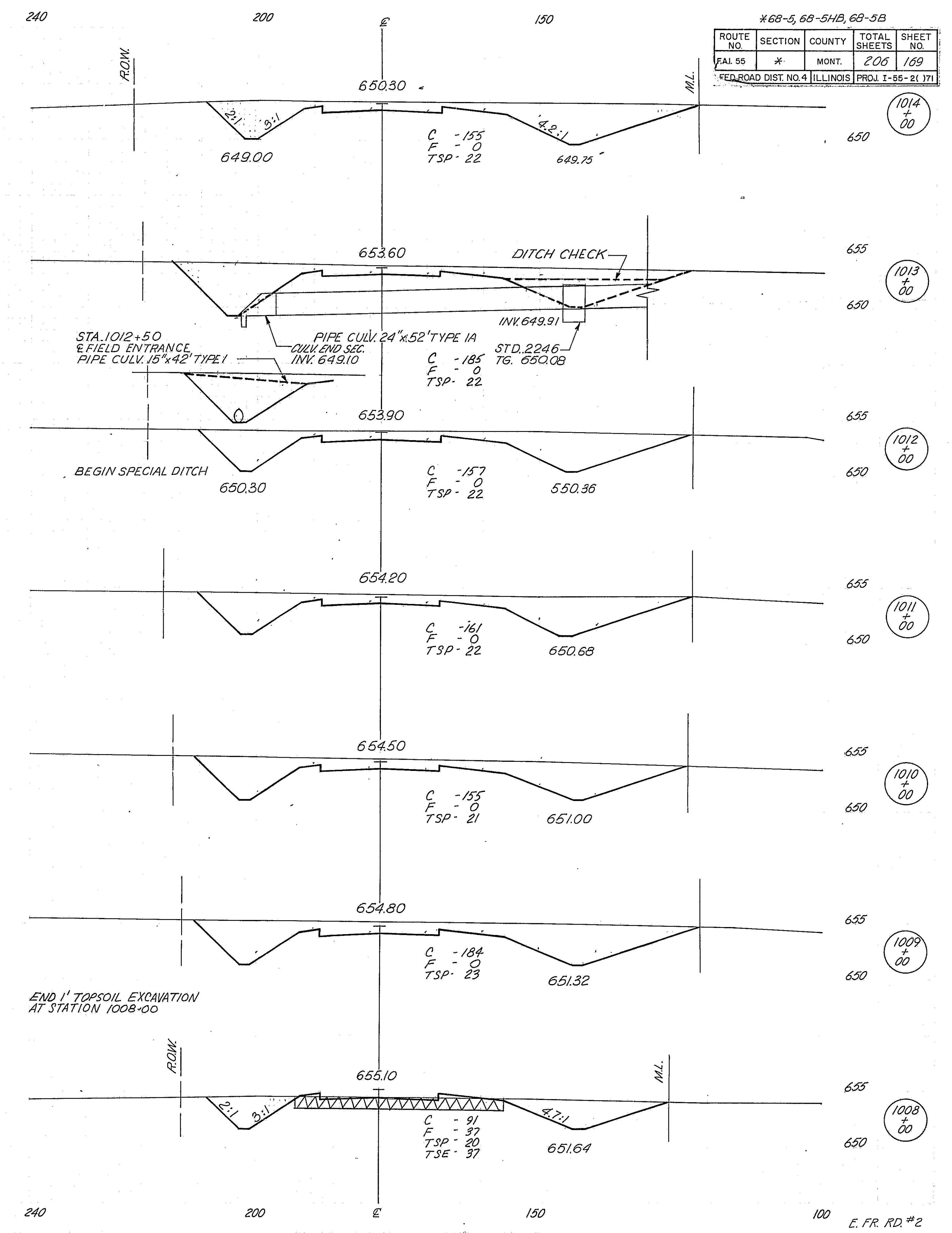
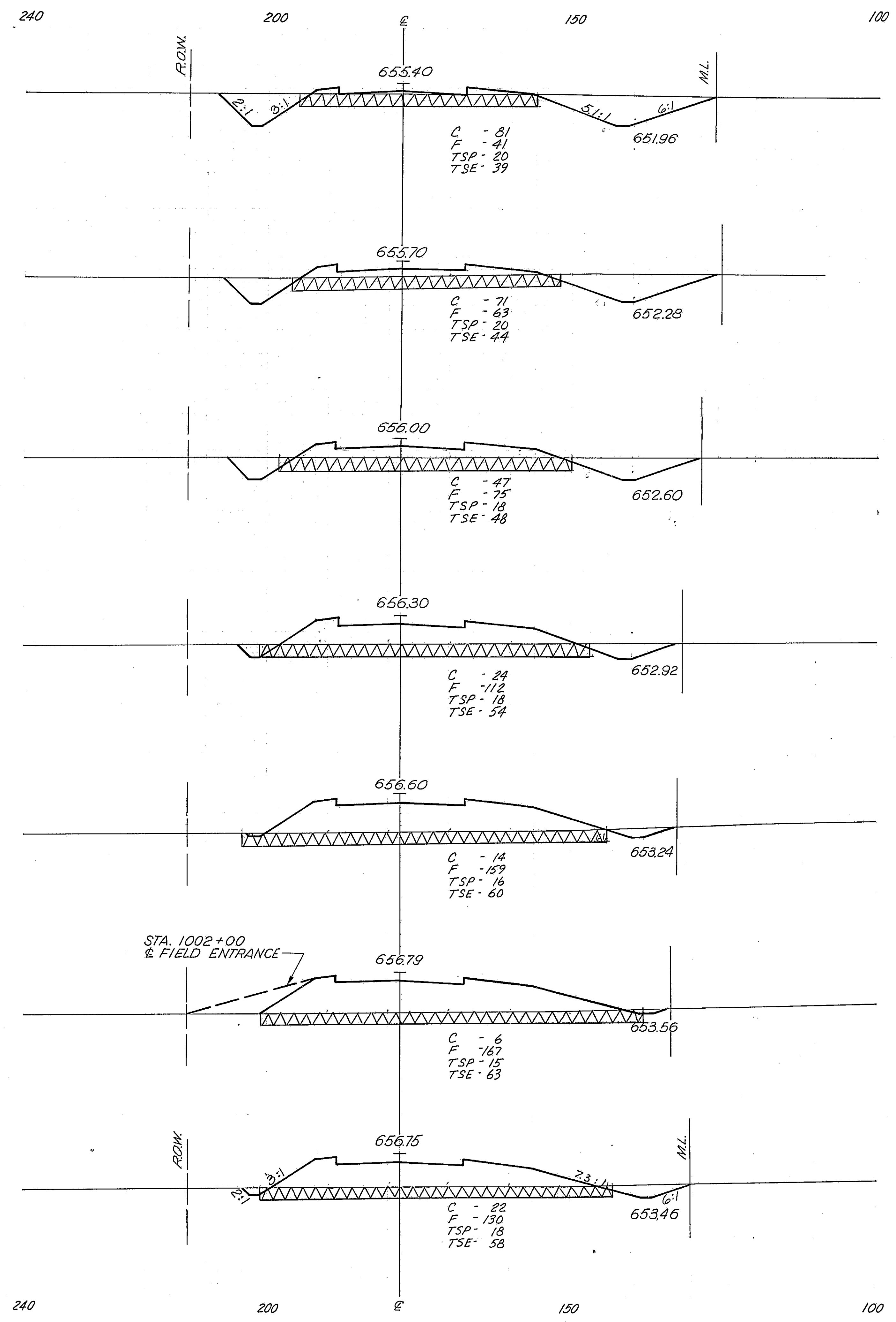
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	168
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



240 200 150 100

E. FR. RD. #2

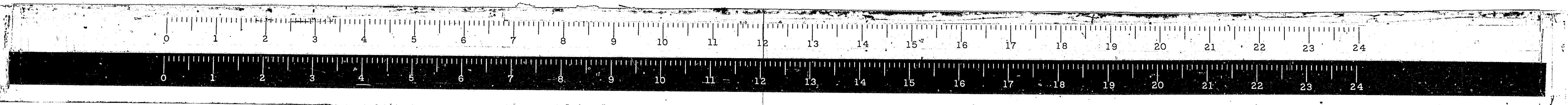


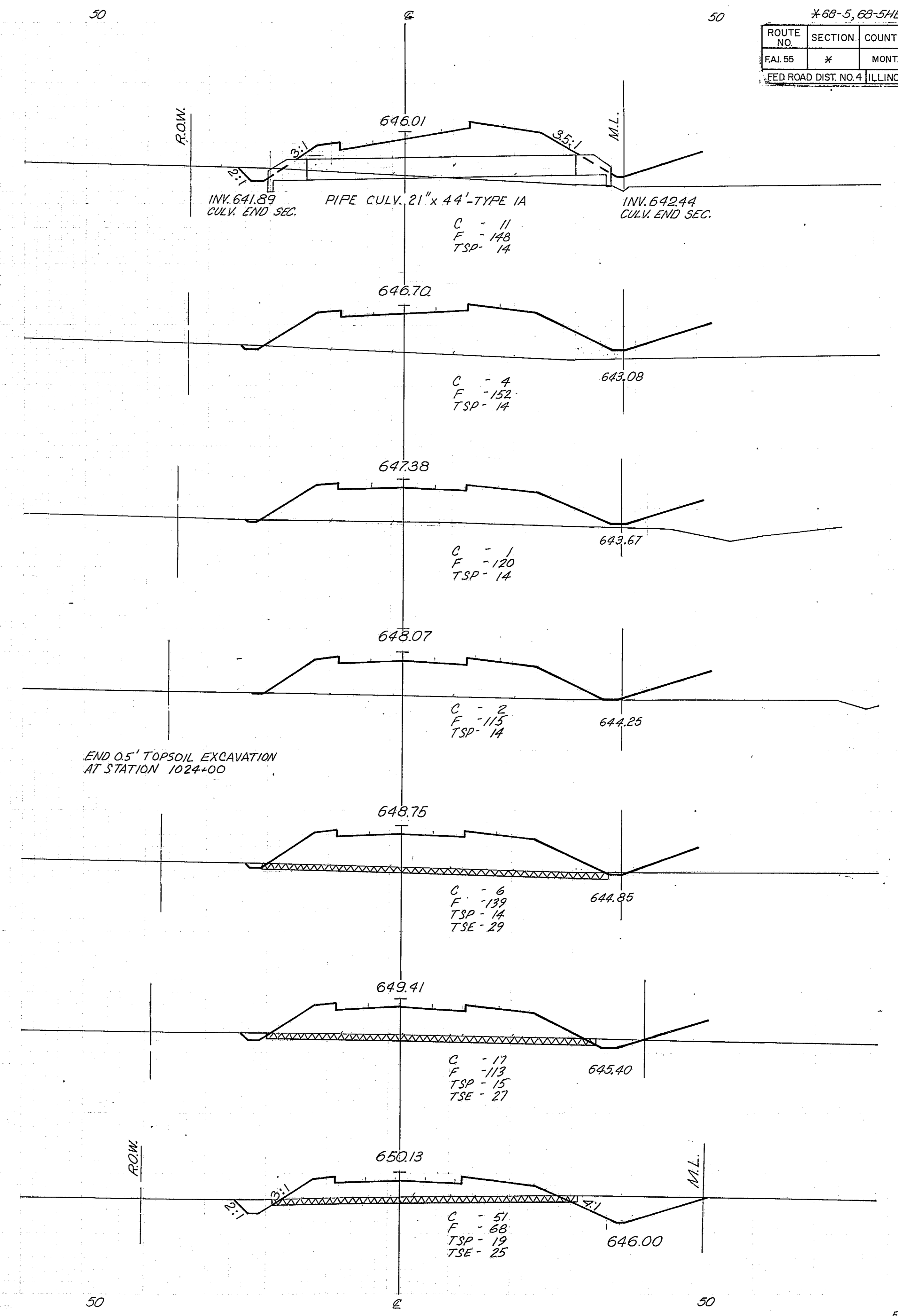
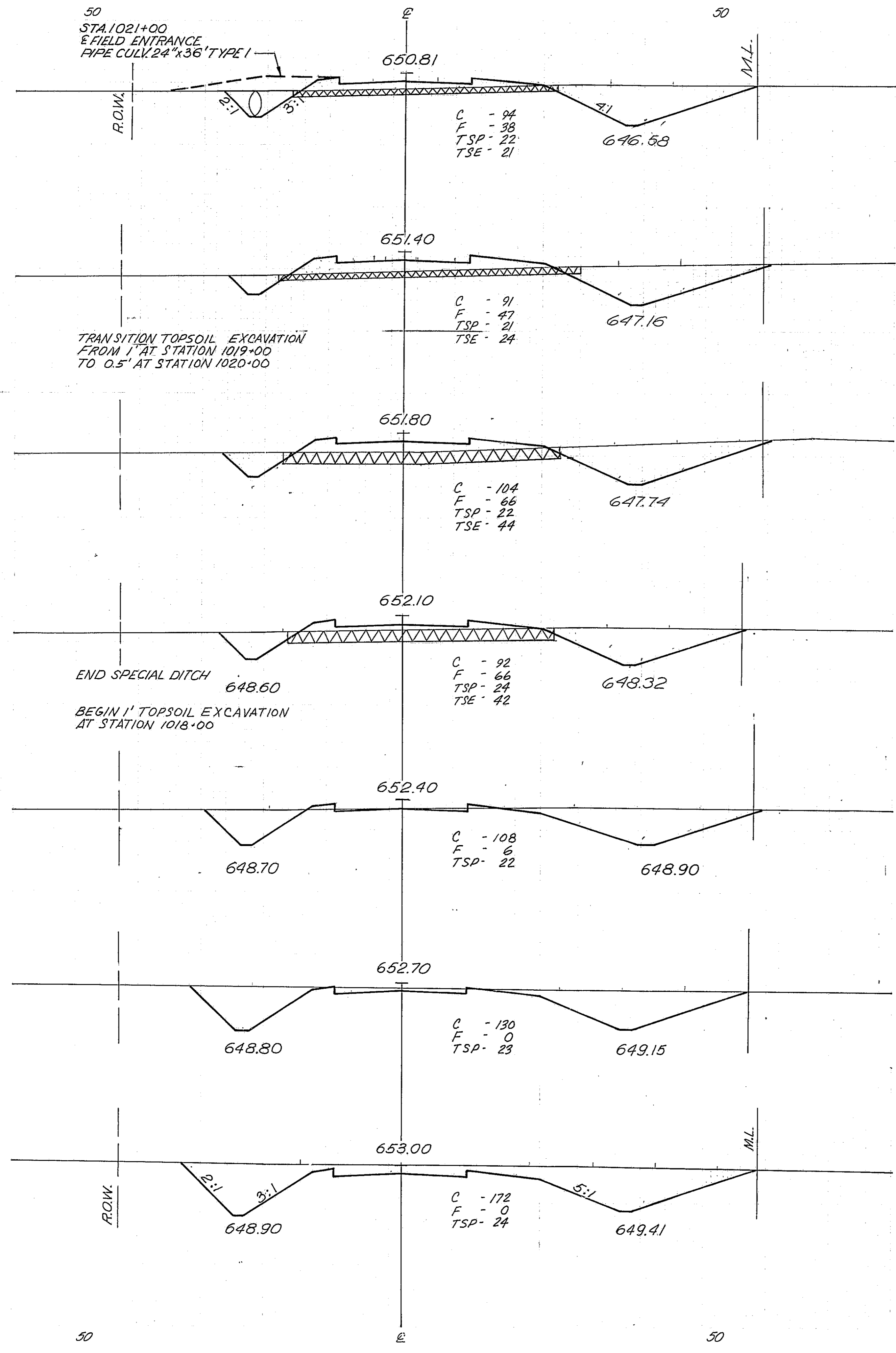


*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MONT.	206	169

FED. ROAD DIST. NO. 4 ILLINOIS PROJ. I-55-21 171



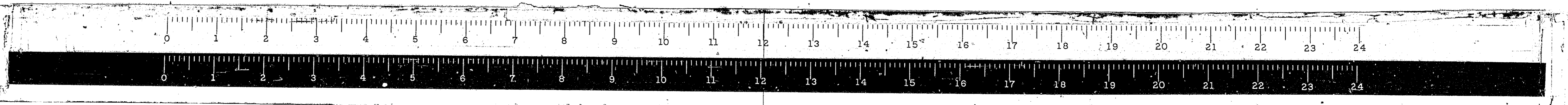


*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
EAL 55	*	MONT.	206	170

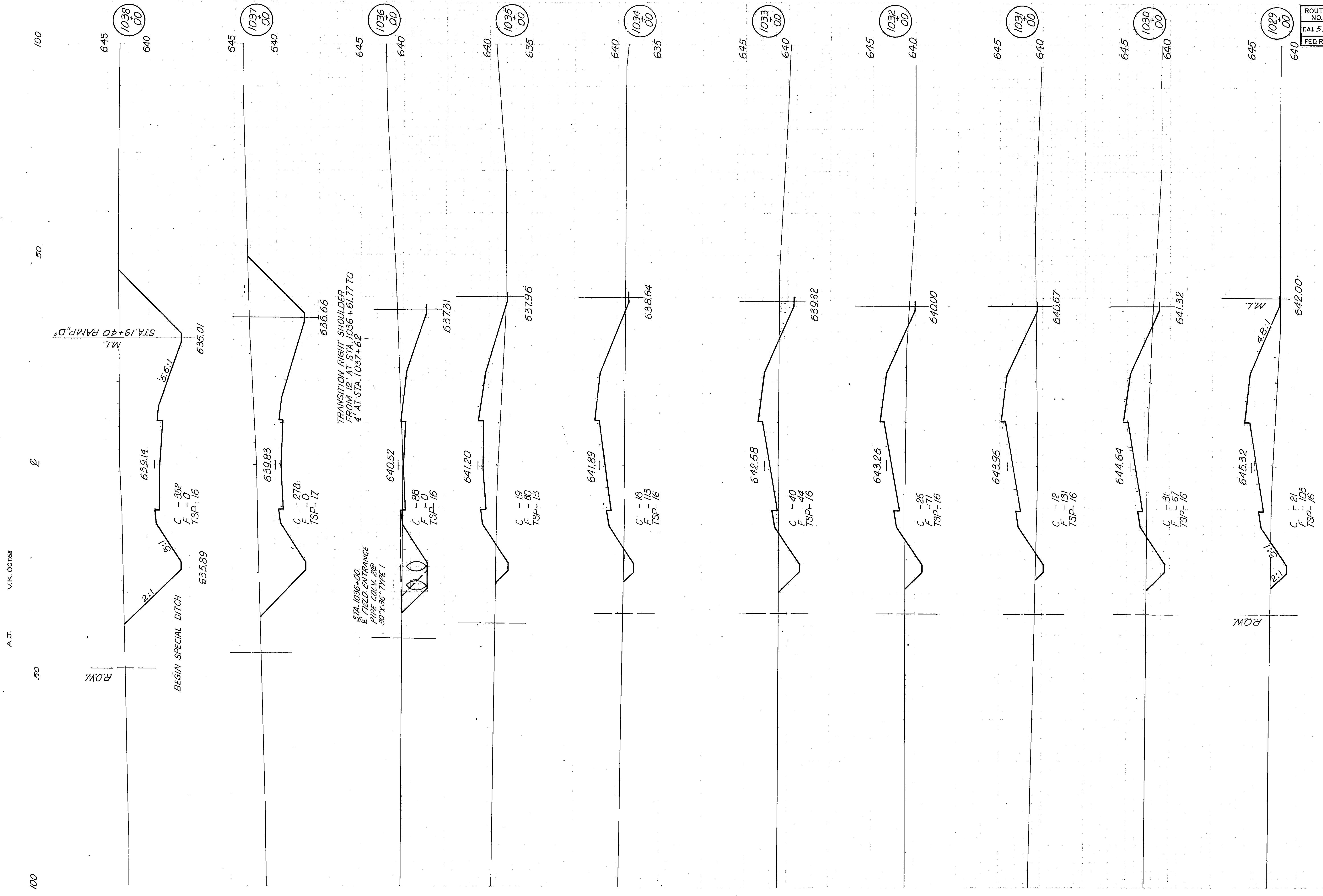
FED. ROAD DIST. NO. 4 ILLINOIS PROJ. I-55-21 171

E. FR. RD. #2



* 68-5,68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	171
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ. 1-59-21 171	



A.J. V.I.K. OCT 68

100

50

0

50

100

100

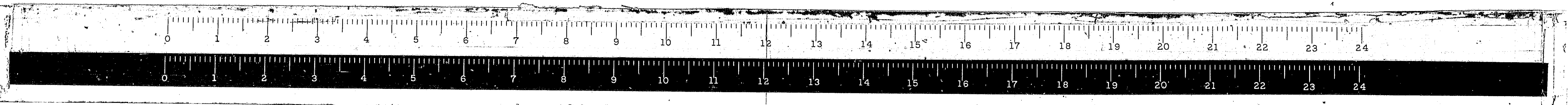
50

0

50

100

EAST FR. RD.#2

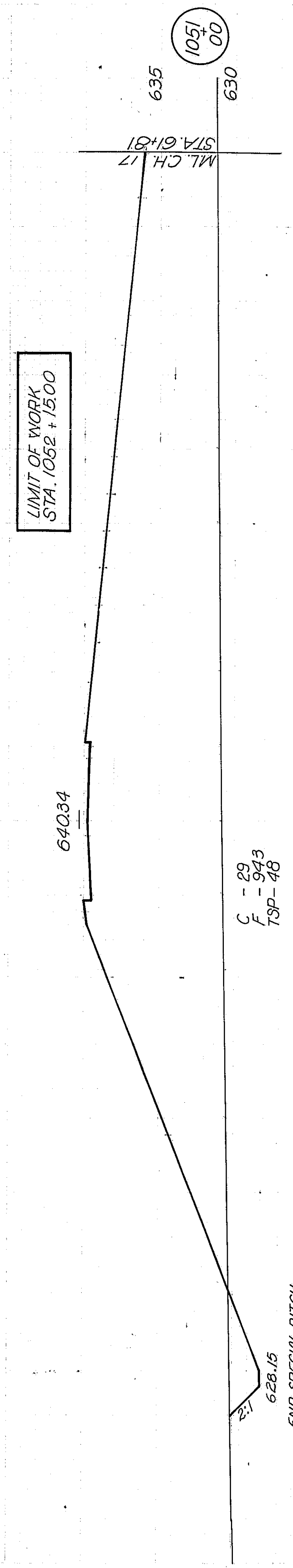


100 50 2 50 100

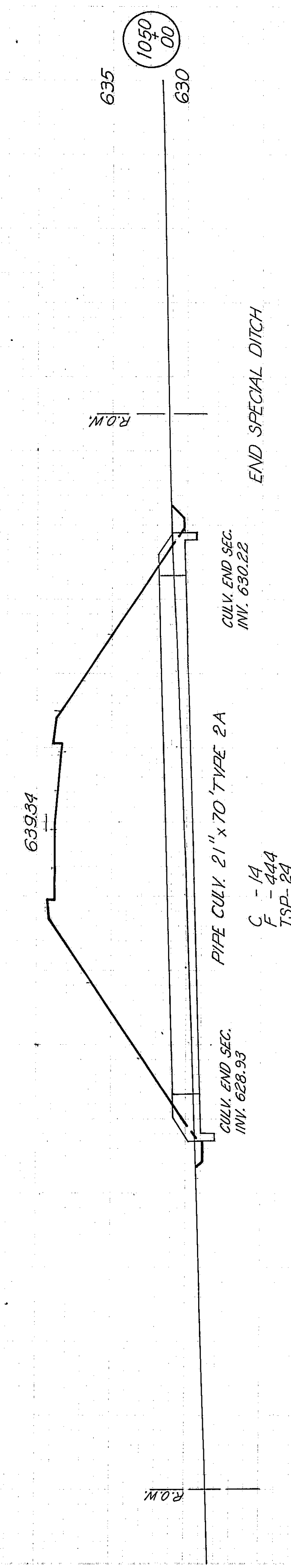
A.J. VIK OCT 68

X 68-5, 68-5HB, 68-5B

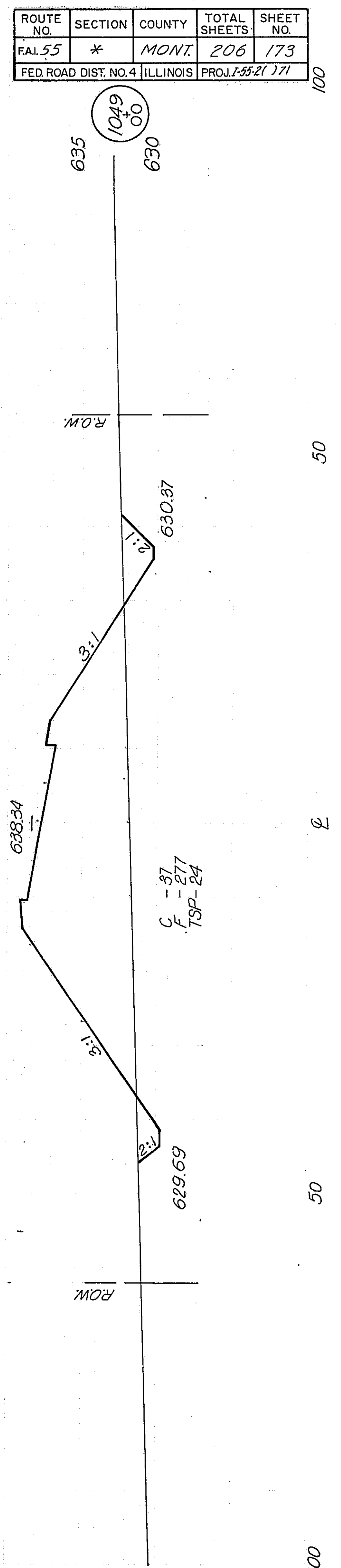
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT	206	173
FED. ROAD DIST. NO. 4		ILLINOIS	PROJ. 155-21 171	



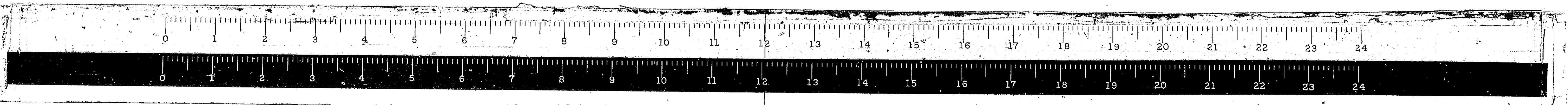
C - 22
F - 24
TSP - 48



C - 14
F - 44
TSP - 24



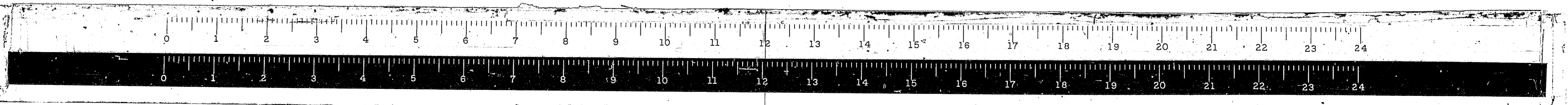
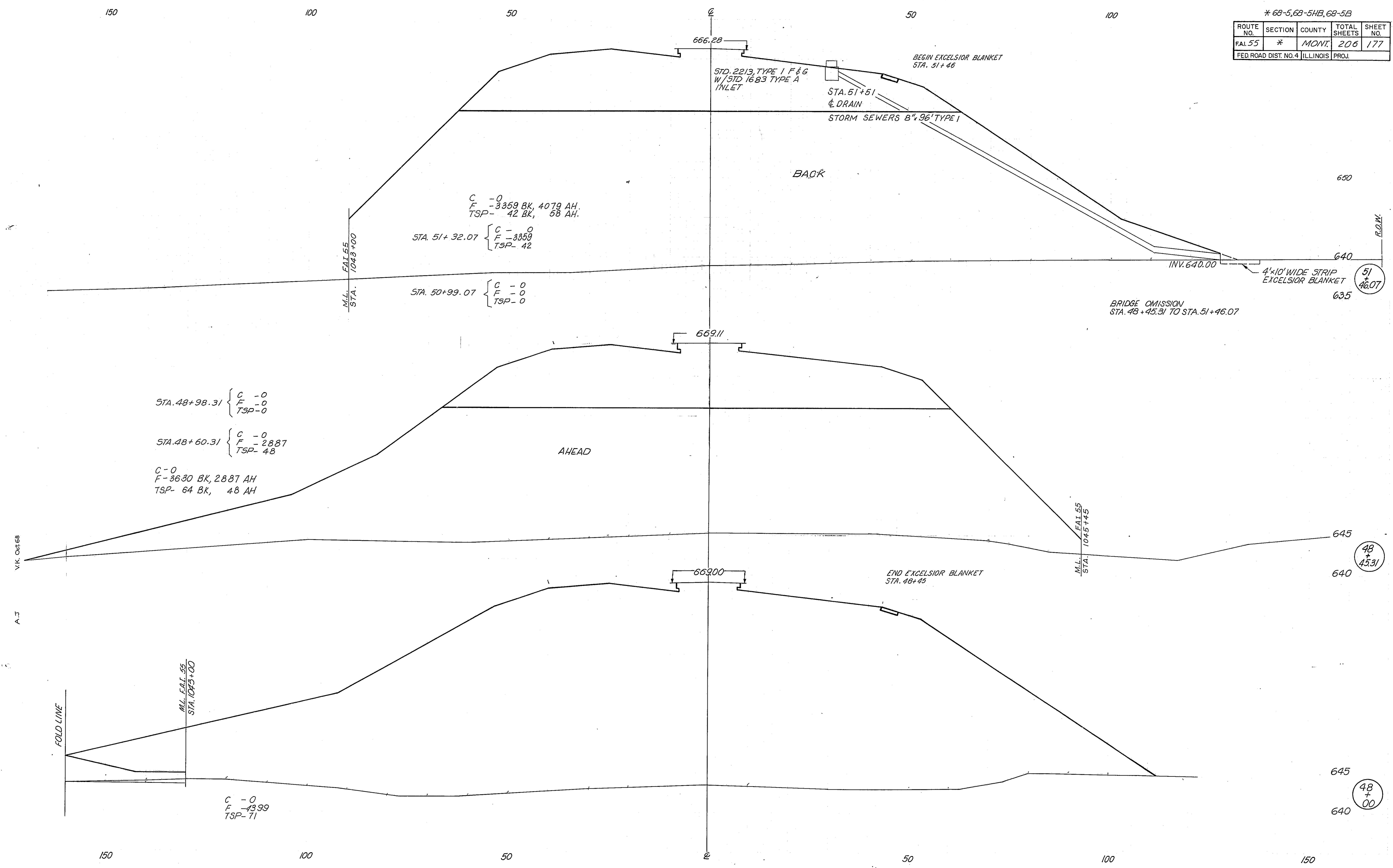
C - 37
F - 27
TSP - 24



EAST FR. RD. #2

* 68-5,68-5HB,68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	177
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



CH 17 / 1

150

100

50

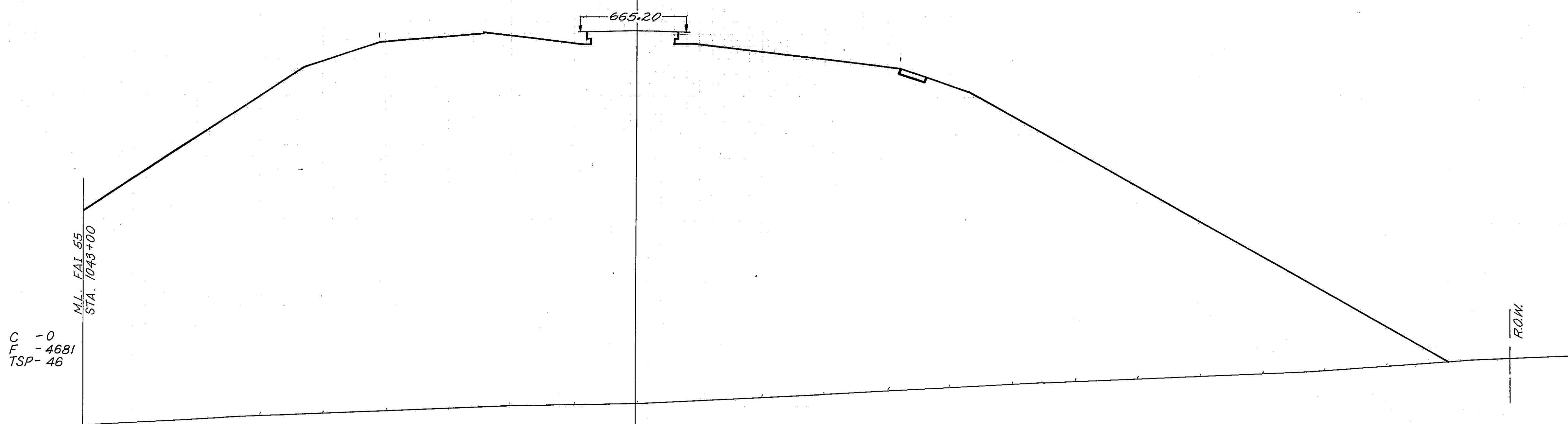
0

50

100

*68-5, 68-5B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT	206	178
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



C - 0
 F - 4681
 TSP - 46

M.L. FAL 55
 STA. 10+33+00

ROAD

635
 640
 650
 660

52
 00

V.K. OSEB

A.J.

150

100

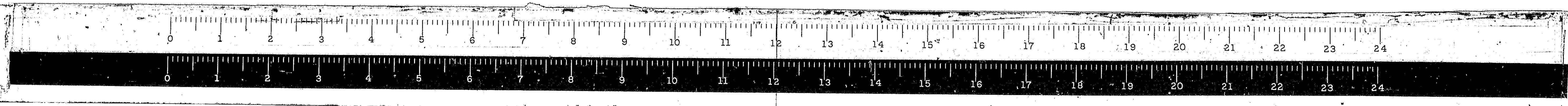
50

0

50

100

150

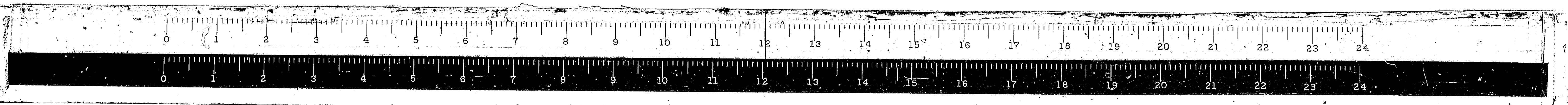
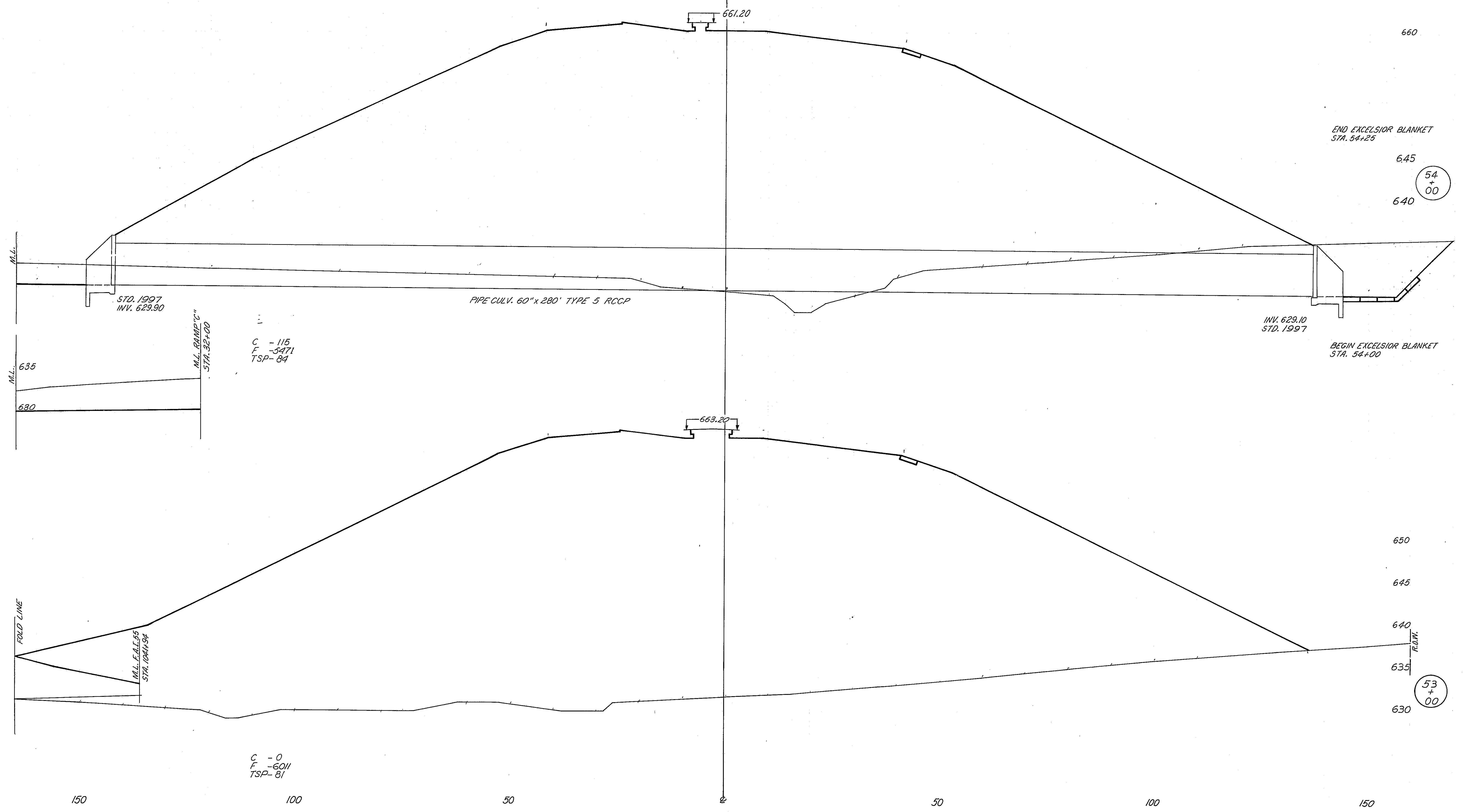


CH 17 7/9

150 100 50 0 50 100 150

*63-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	179
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



150

100

50

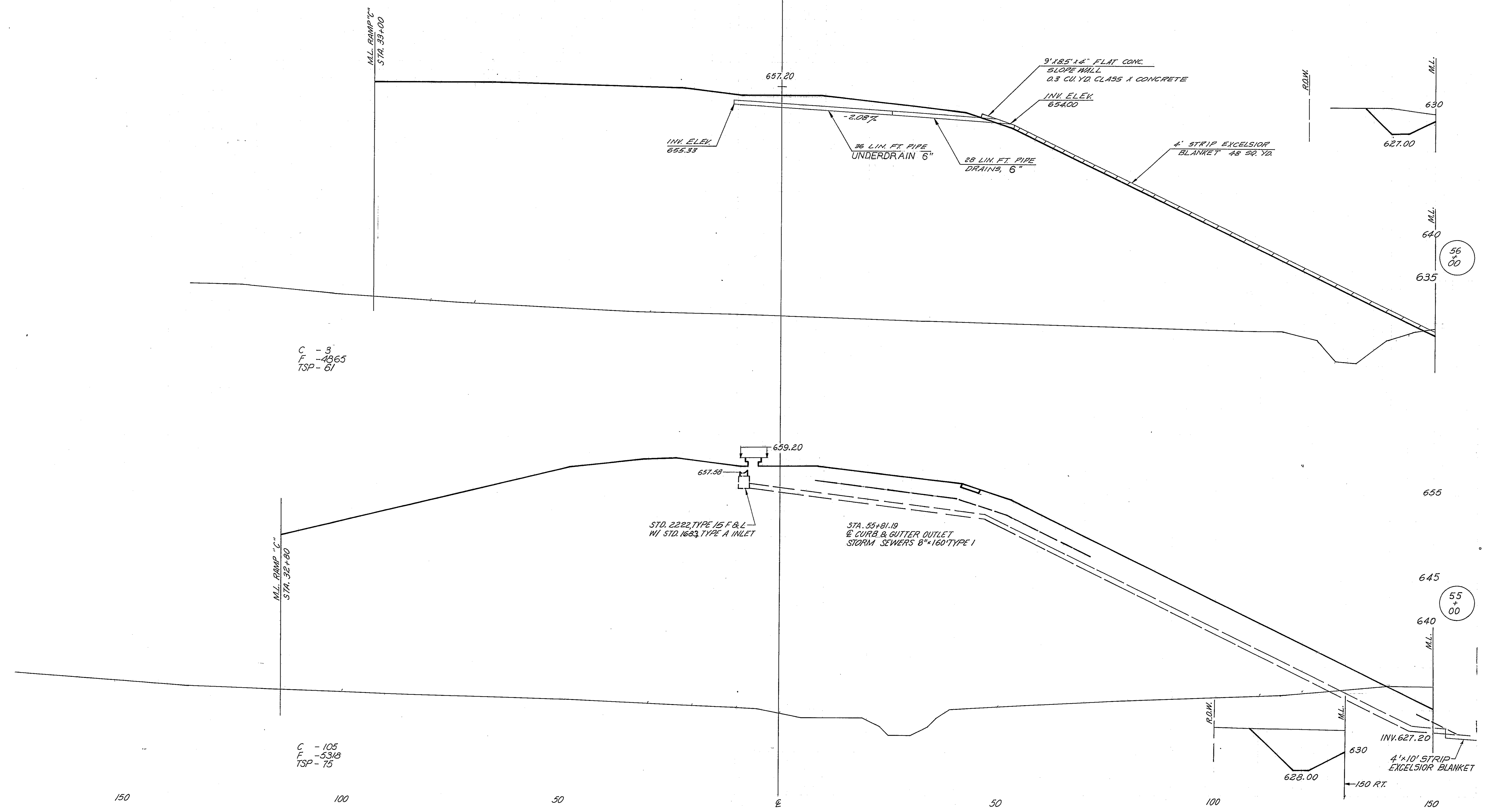
0

50

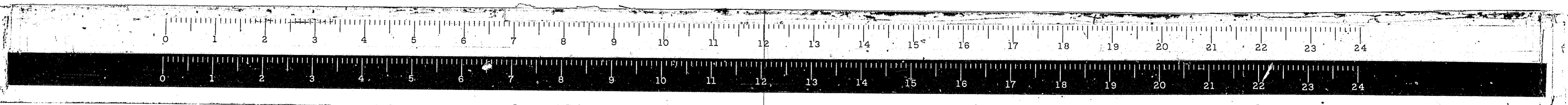
100

*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	180
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



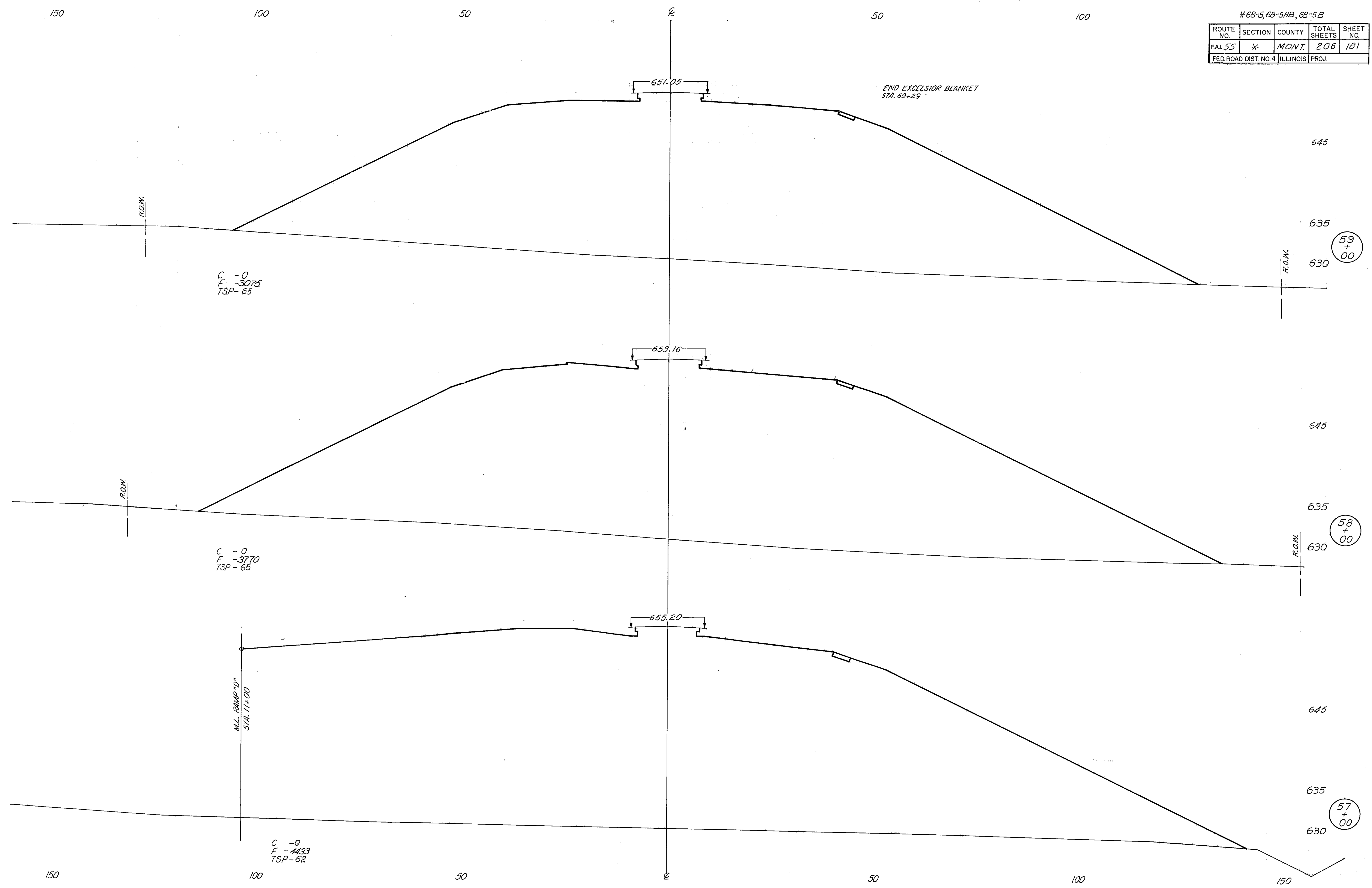
V/K 08168
A.T.



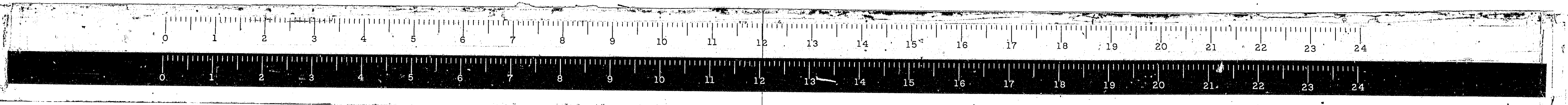
CH 17

*68-5,68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	181
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



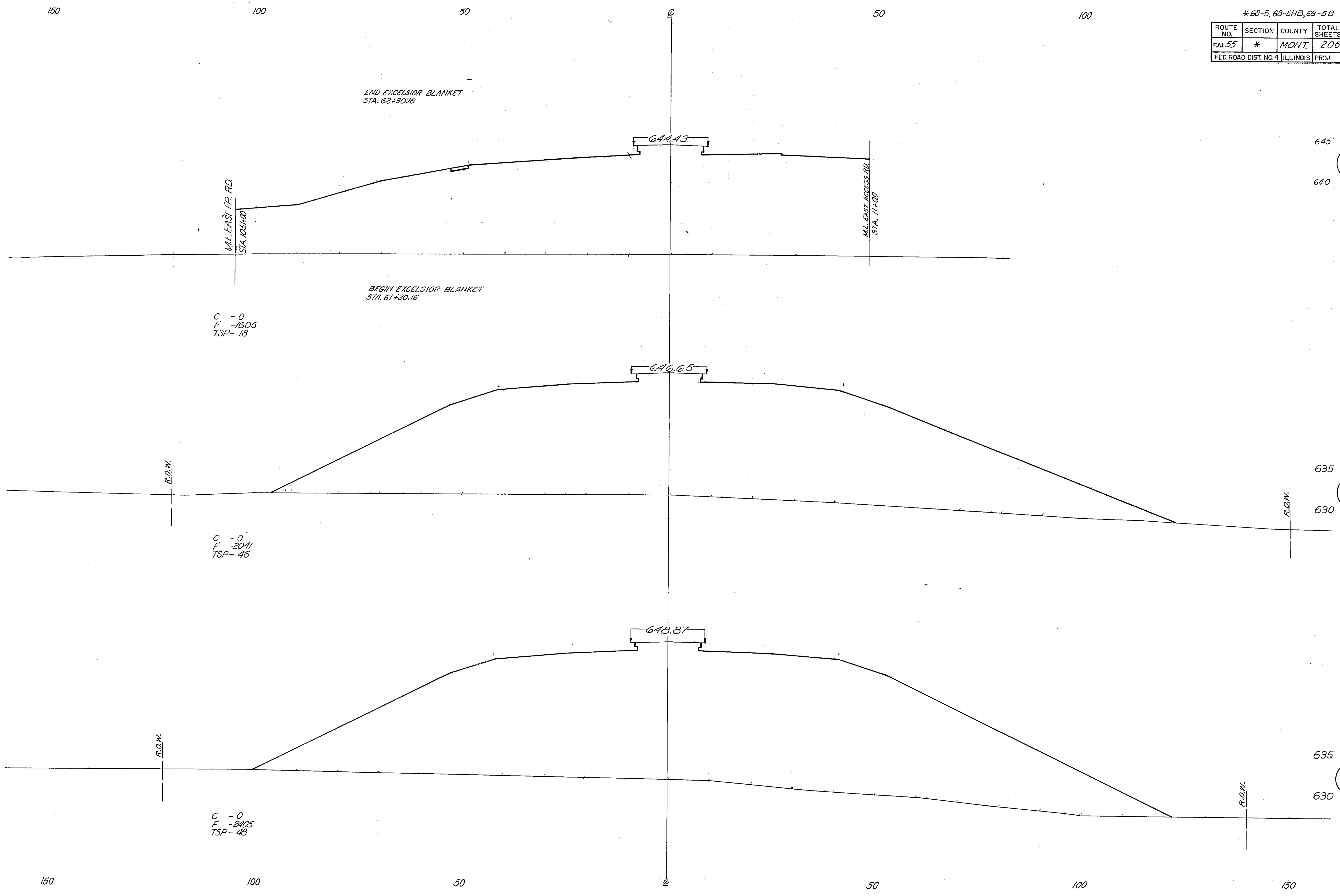
V.K. Oct 68
A.J.



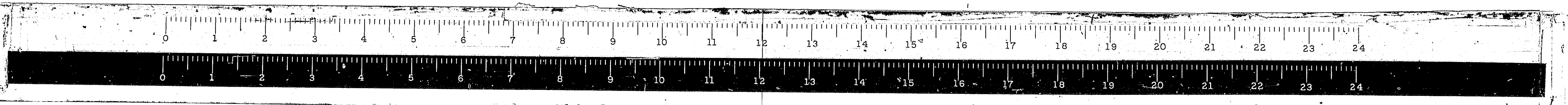
CH 17

*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-55	*	MONT.	206	182
FED. ROAD DIST. NO. 4		ILLINOIS		PROJ.



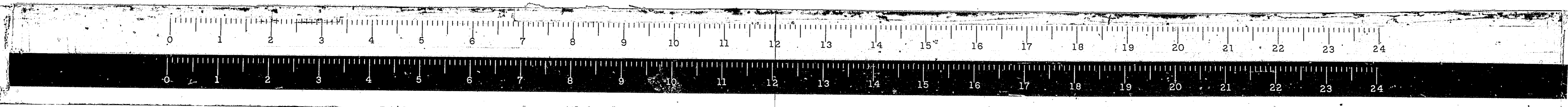
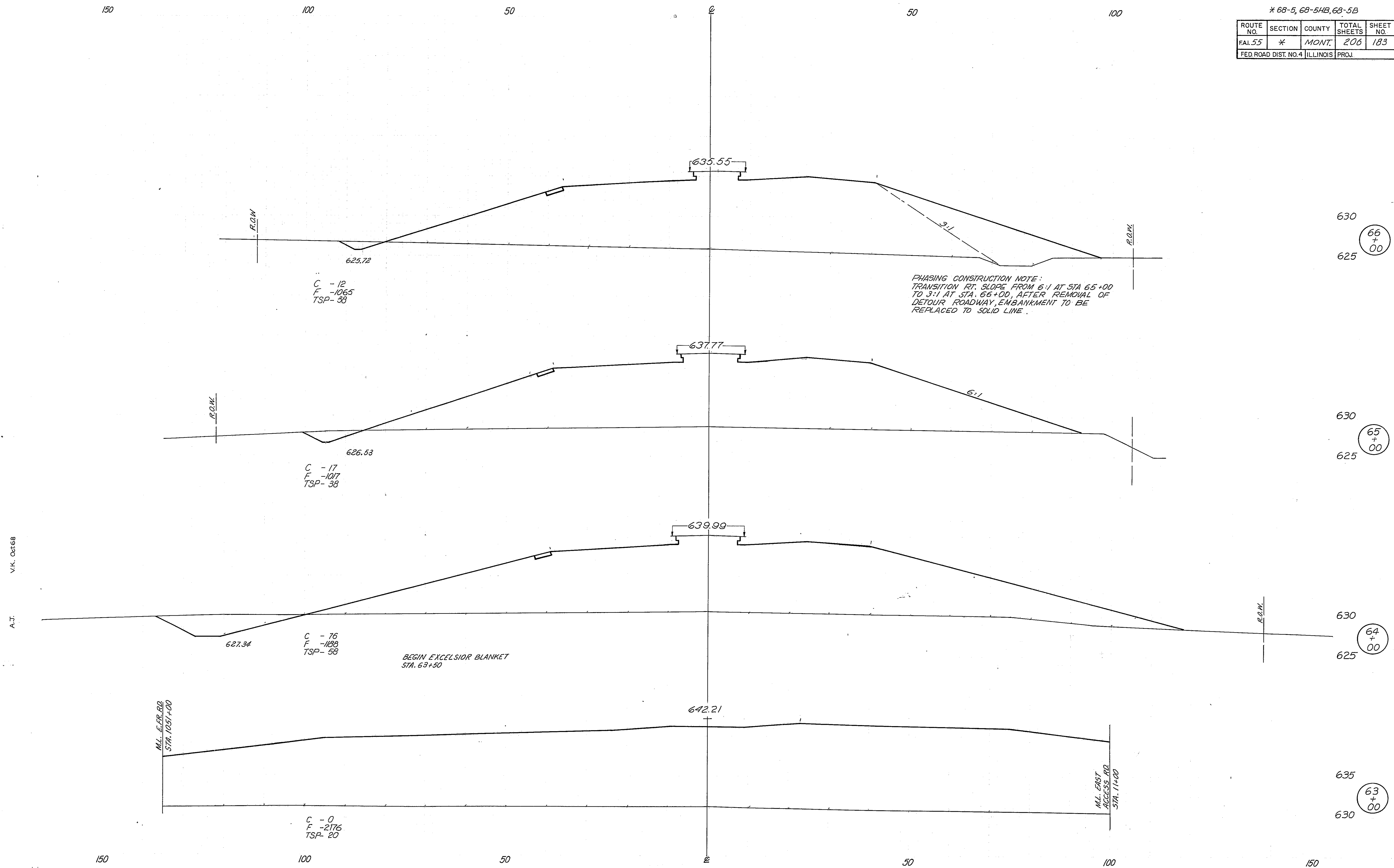
V.K. 08/68
A.J.



CH 17

* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	183
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



CH 17

150

100

50

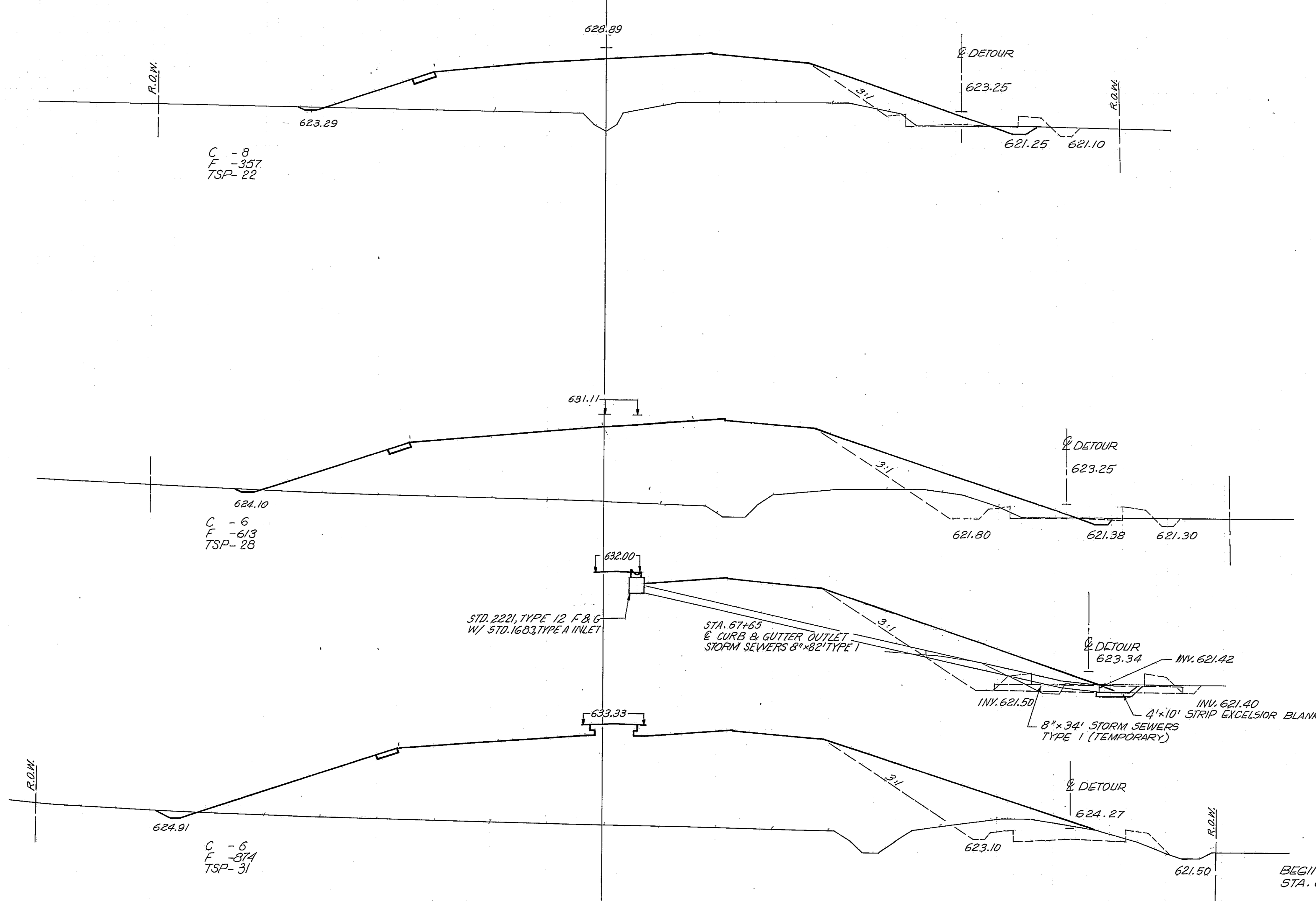
0

50

100

* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	184
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



625

69
+
00

620

625

68
+
00

620

625

67
+
00

620

625

67
+
00

620

V.K. 0468

A.J.

150

100

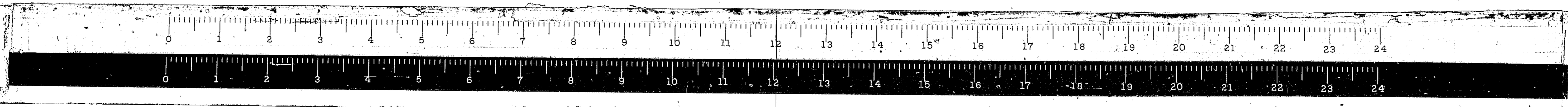
50

0

50

100

150



CH 17

150

100

50

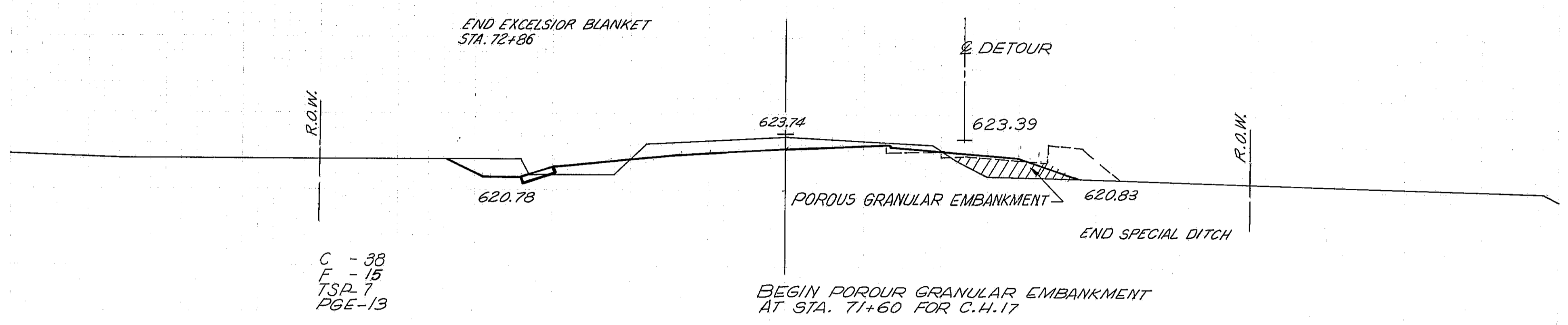
0

50

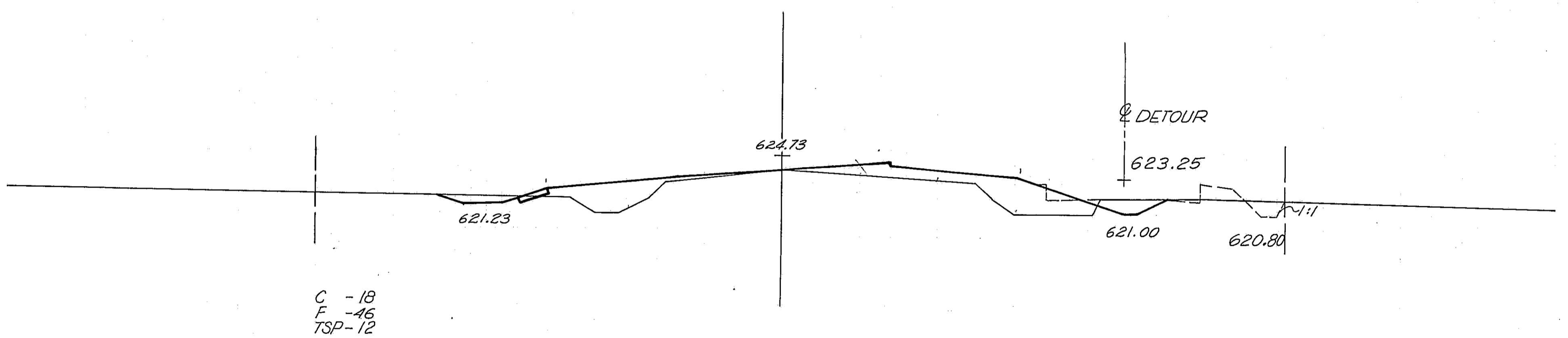
100

* 68-5, 68-5HB, 68-5B

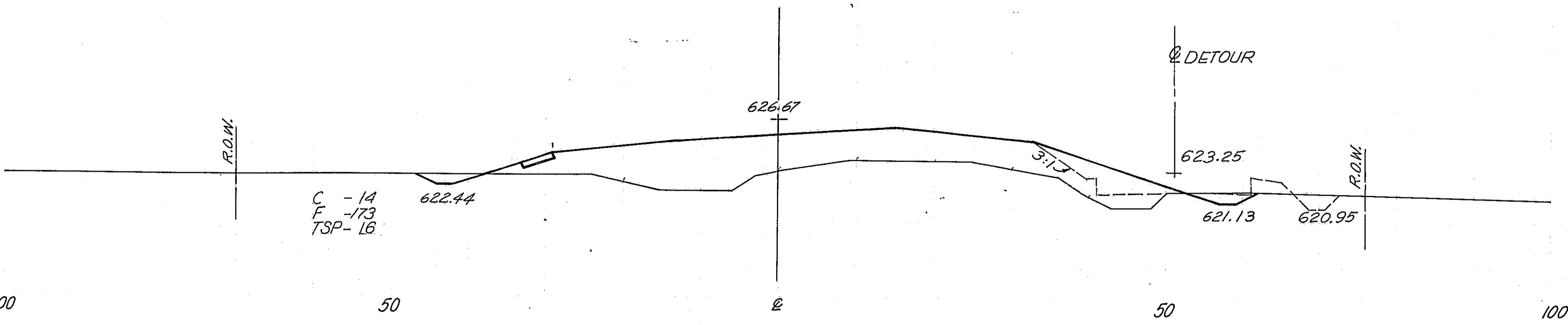
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	185
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



625
 72
 +
 00
 620



625
 71
 +
 00
 620



625
 70
 +
 00
 620

150

100

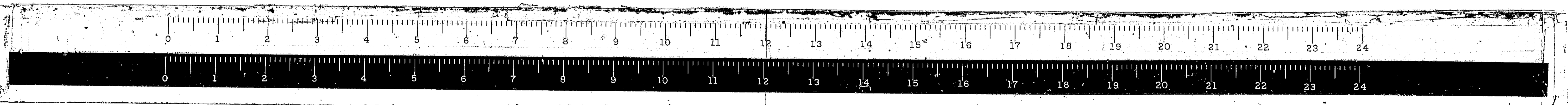
50

0

50

100

150



CH 17 1/2

150

100

50

0

50

100

*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	186
FED ROAD DIST. NO. 4 ILLINOIS			PROJ.	

V.K. 08/68

A.T.

C - 40
 F - 24
 TSP - 14
 PGE - 48

LIMIT OF WORK
 STA. 73+08.01

END POROUS GRANULAR EMBANKMENT
 AT STA. 73+08.01 FOR C.H. 17

DITCHES DISCHARGE
 TO CREEK

TRANSITION ROADWAY WIDTH FROM
 24' @ STA. 72+00 TO EXISTING @
 STA. 73+08.01

620.00
 C - 40
 F - 24
 TSP - 14
 PGE - 48

623.70

625

73
 +
 00

620

150

100

50

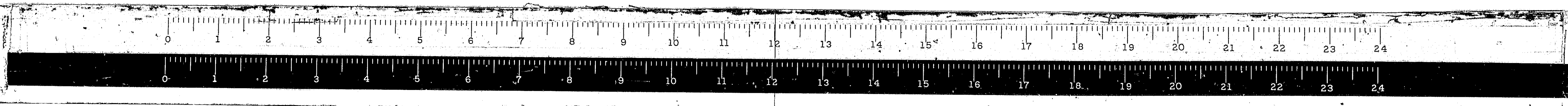
0

50

100

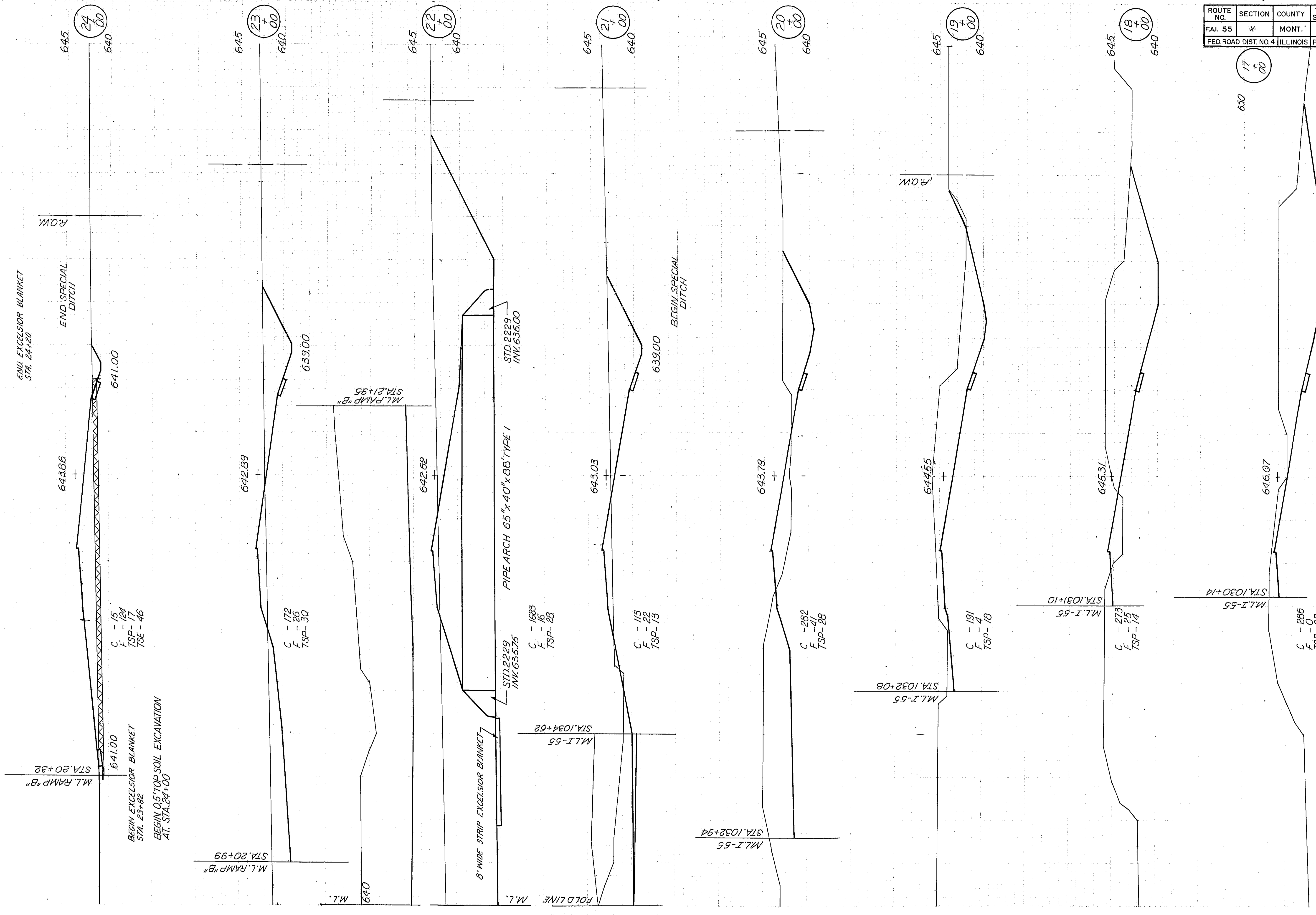
150

CH 17



100 50 PGL 50 100

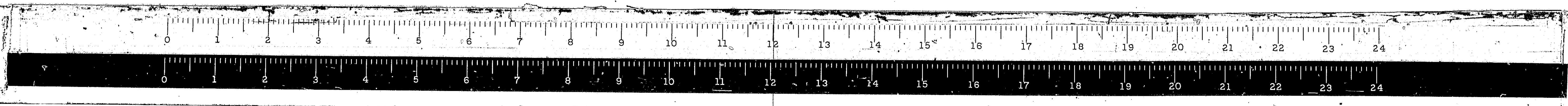
C.W. A.J. OCT 66

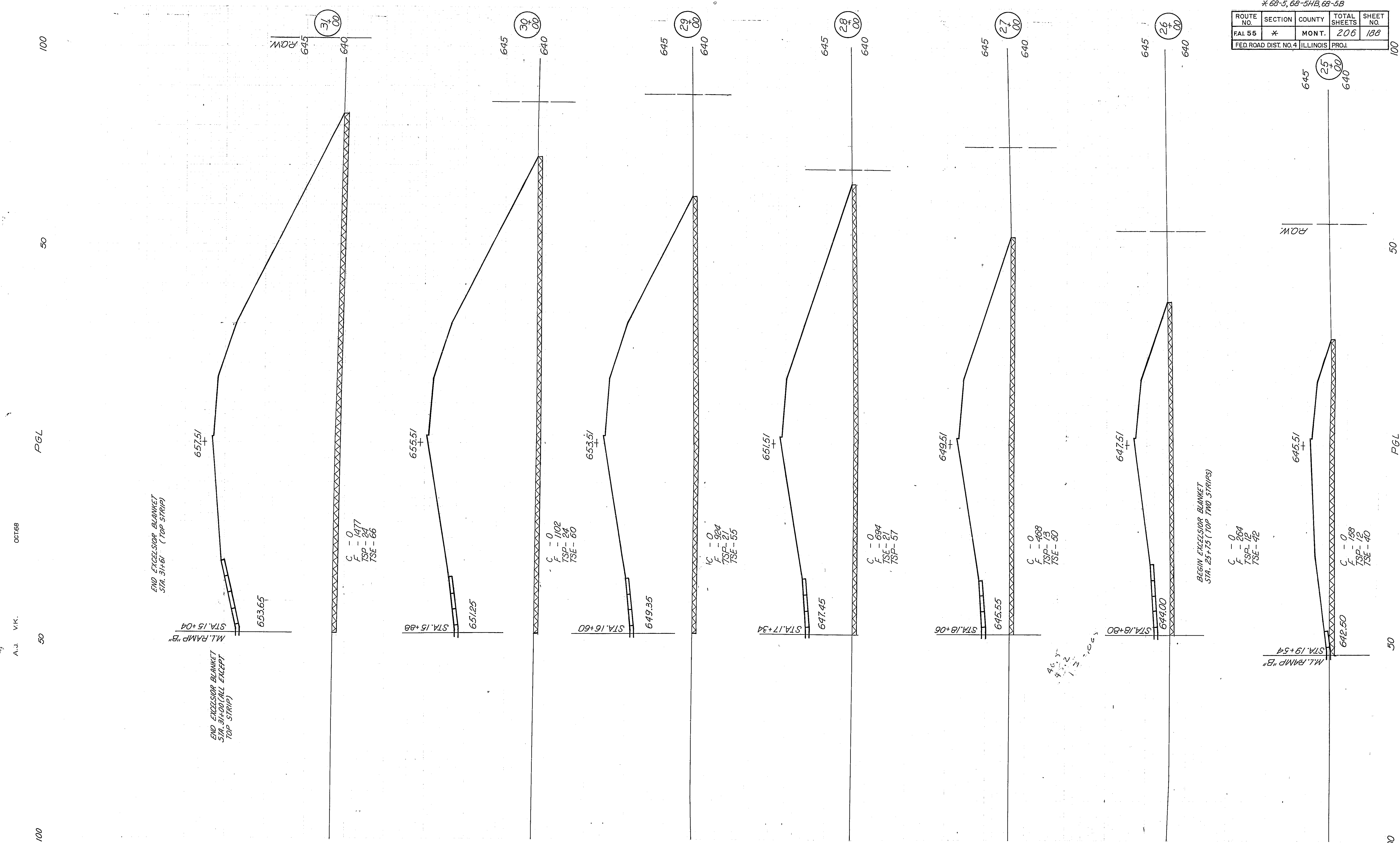


*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	187
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

RAMP "A"



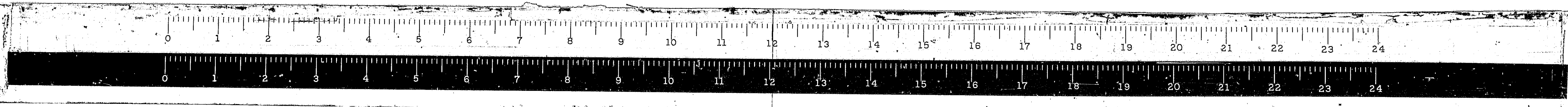


* 68-5, 68-54B, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	188
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

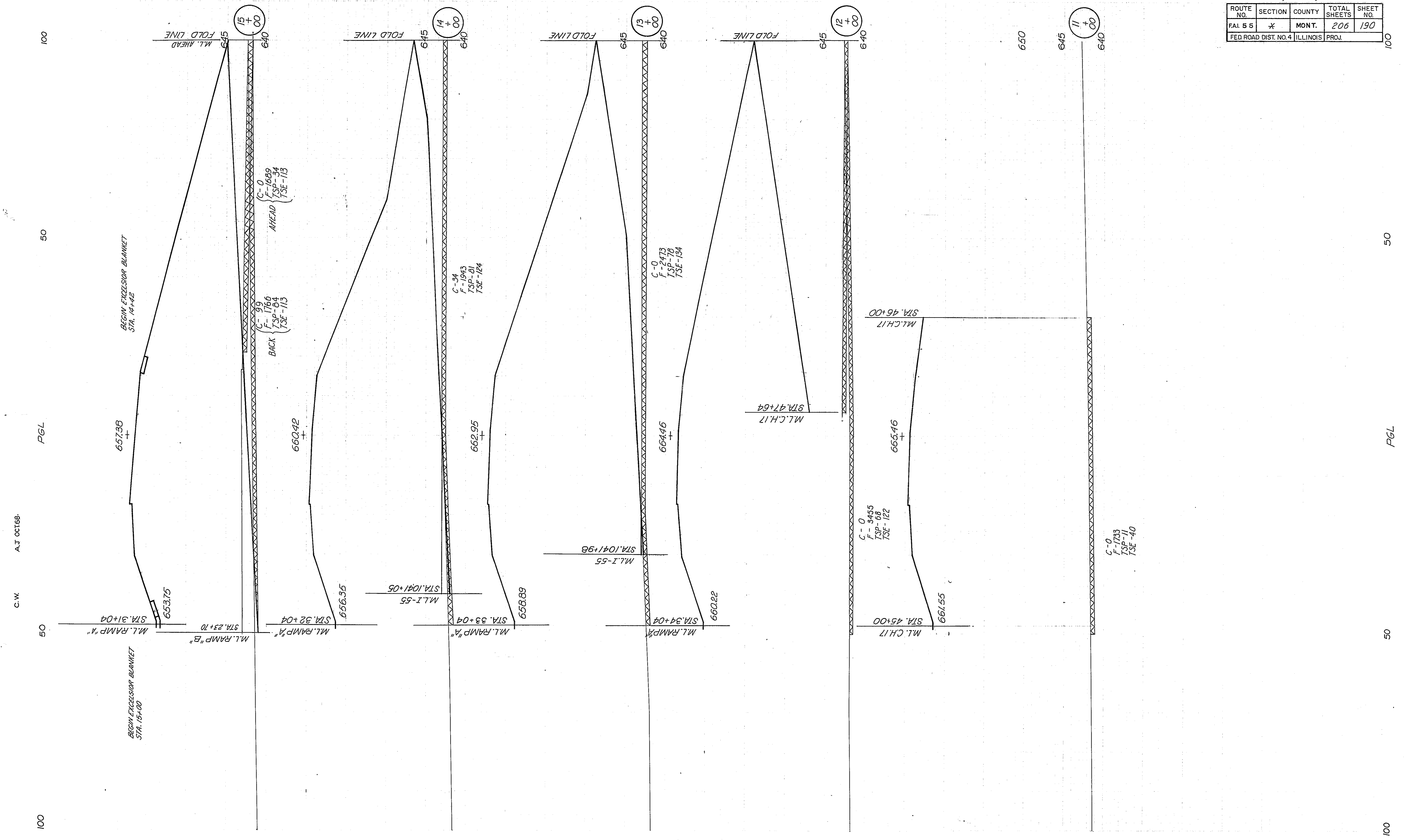
A.J. V.K.
OCT 1968

100 50 PGL 50 100



RAMP "A"

* 68-5, 68-5HB, 68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	190
FED ROAD DIST. NO. 4		ILLINOIS PROJ.		



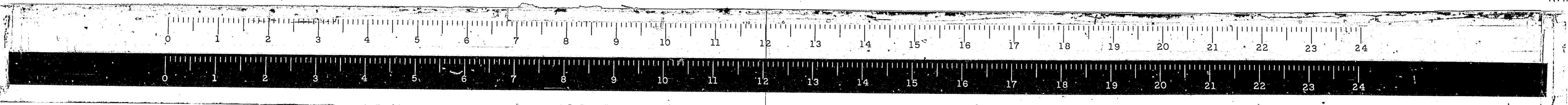
C.W. A.J. OCT 68.

100 50 PGL 50 100

BEGIN EXCESSOR BLANKET STA. 15+00

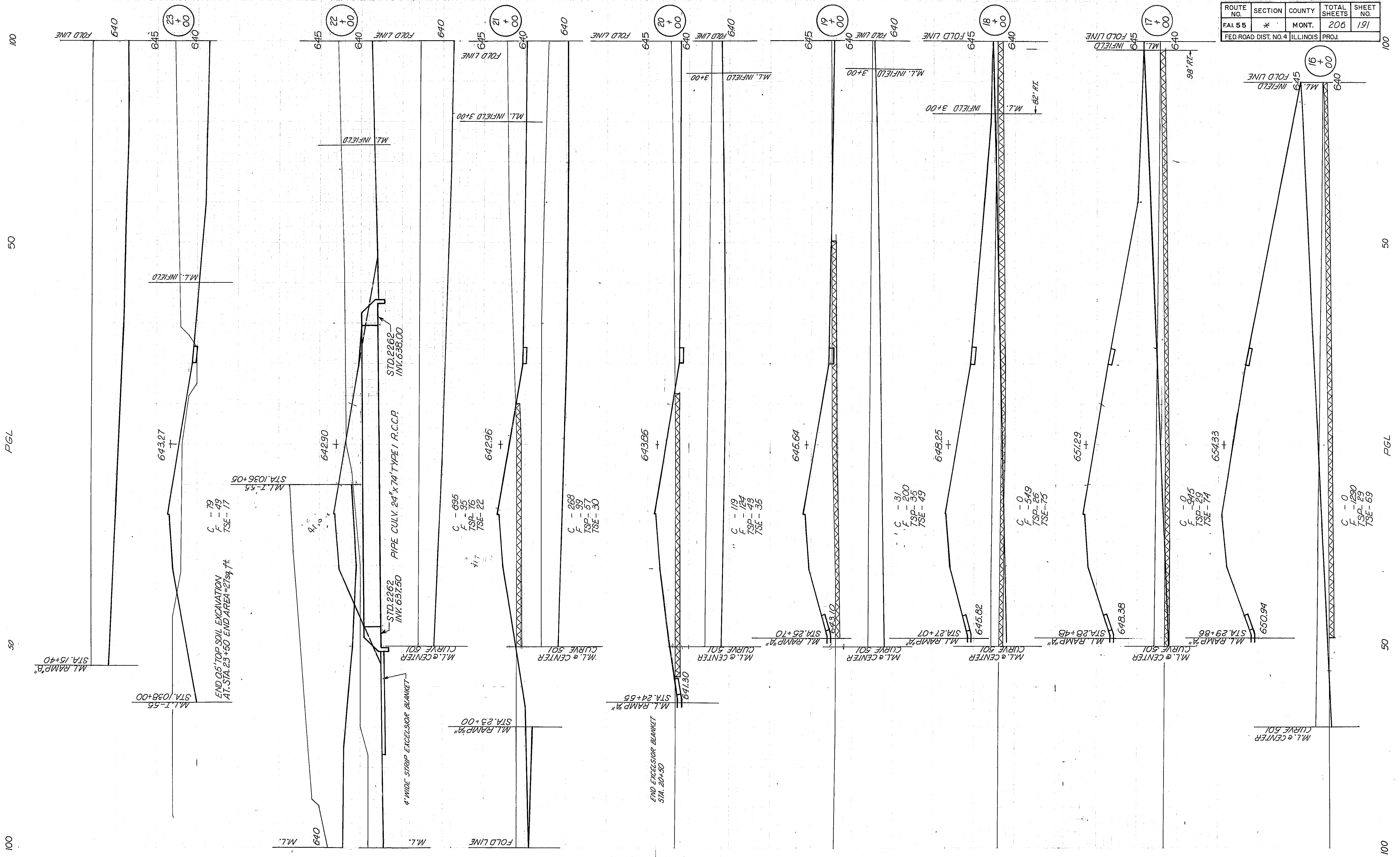
BEGIN EXCESSOR BLANKET STA. 14+42

RAMP B



*68-5, 68-5A, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	191
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



CW A.J. OCT 68

PGL

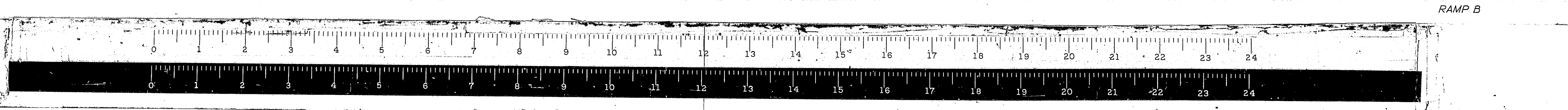
100

100

50

50

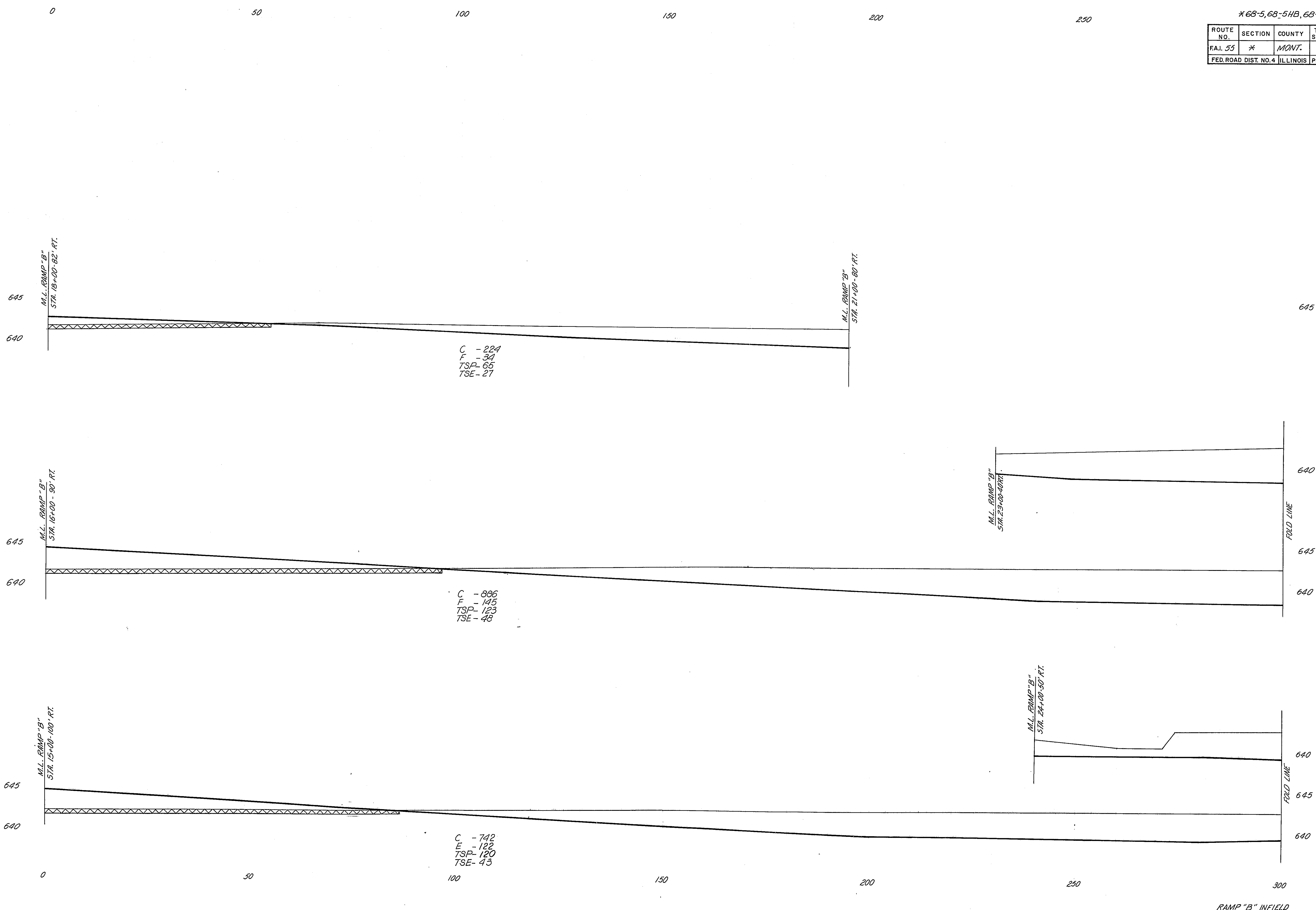
100



RAMP B

X 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAJ. 55	*	MONT.	206	193
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	

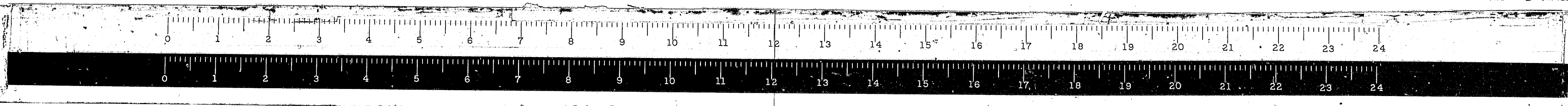


645
3
00

645
2
00

645
1
00

RAMP "B" INFIELD



C.W. A. J. OCT 1968

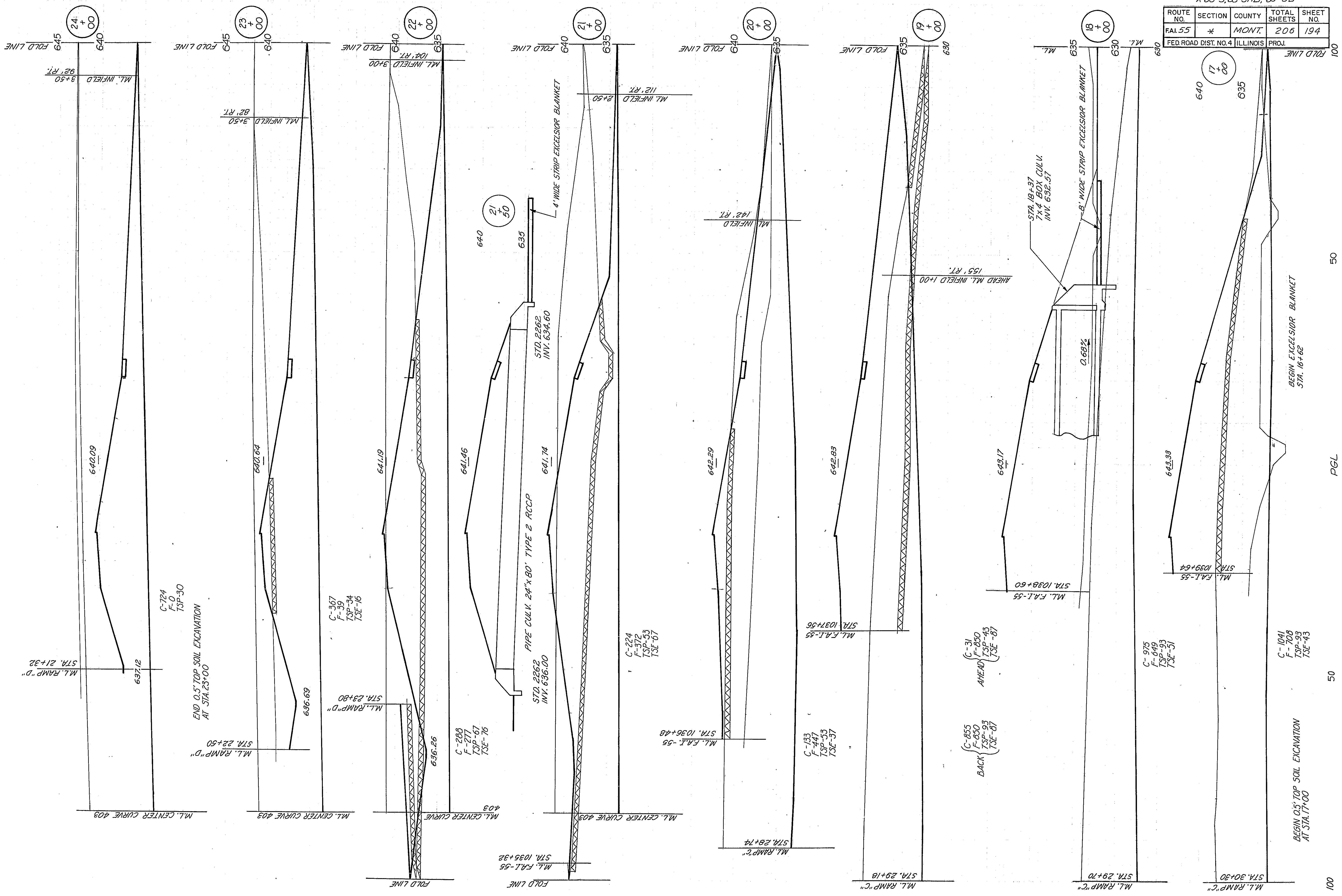
100

50

PGL

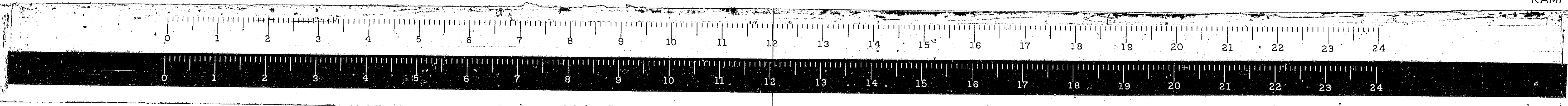
50

100



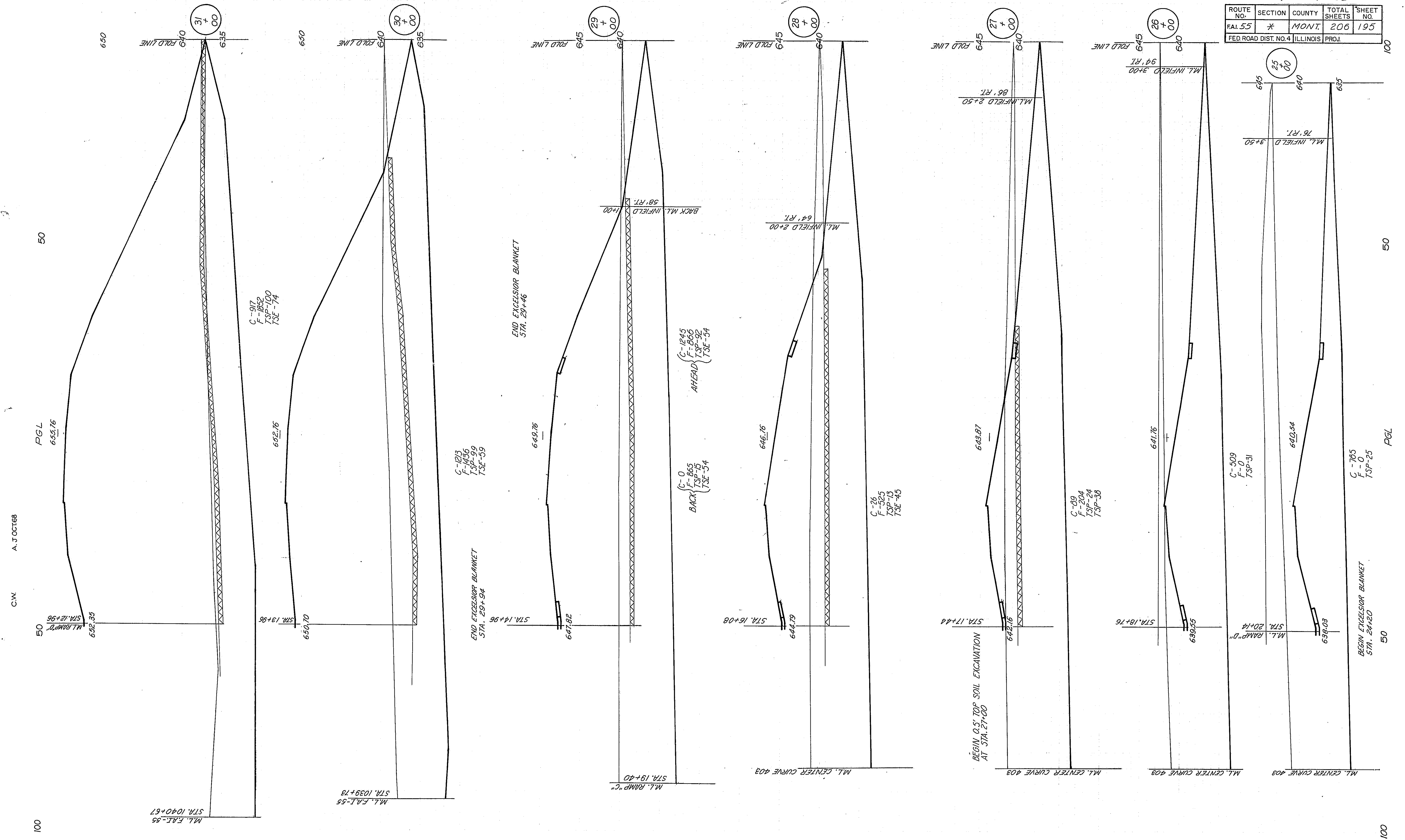
* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL.55	*	MONT.	206	194
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



*68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	195
FED. ROAD DIST. NO. 4		ILLINOIS PROJ.		

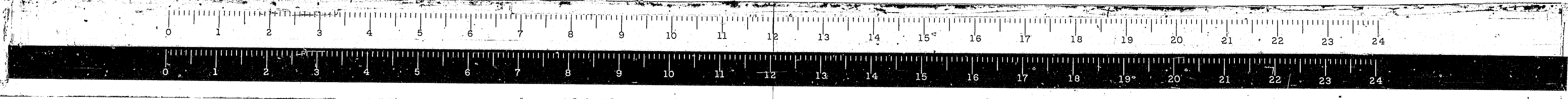


C.W. A. J. OCT 68

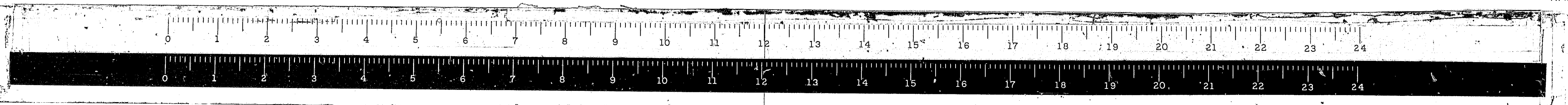
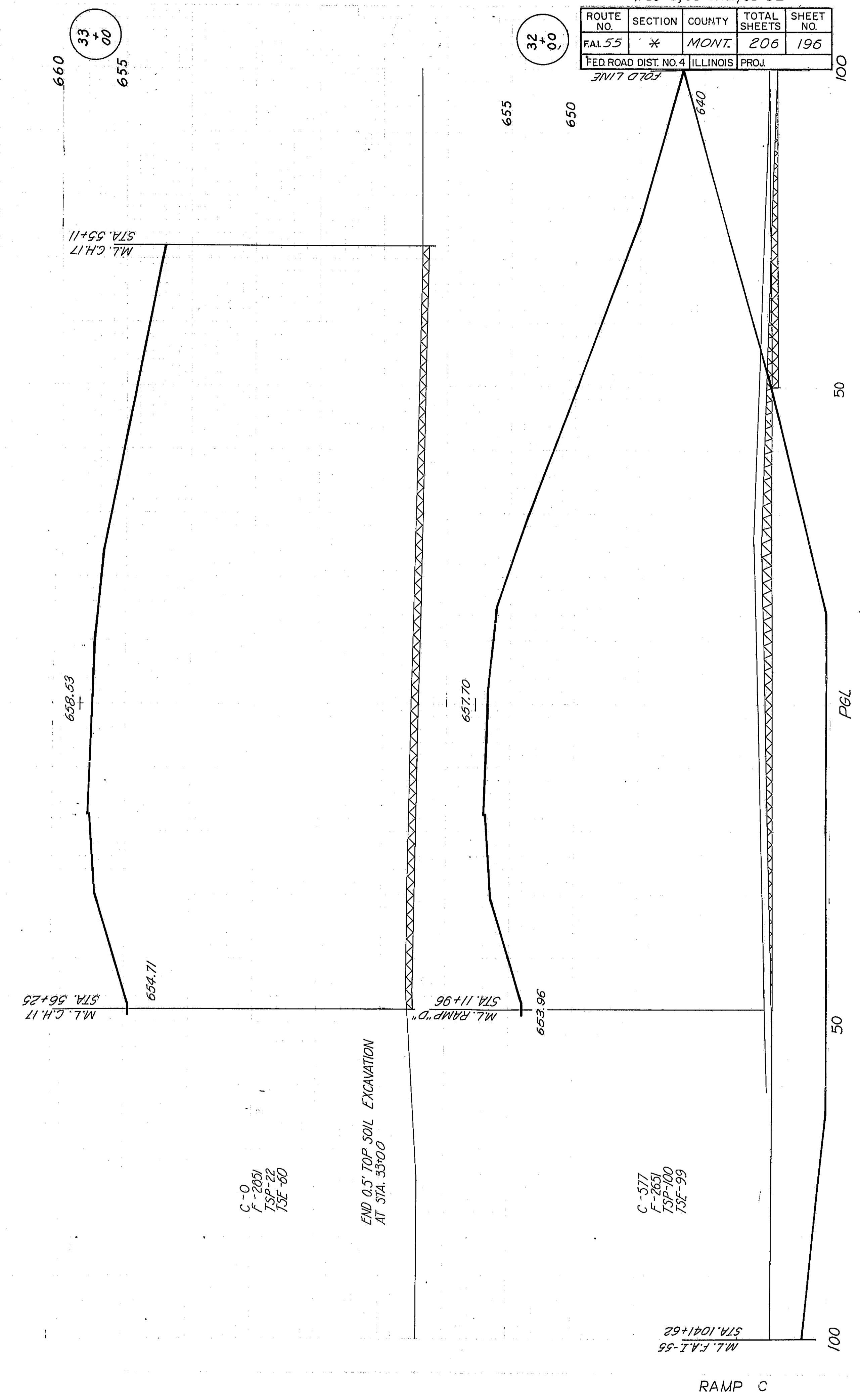
100

100

RAMP C

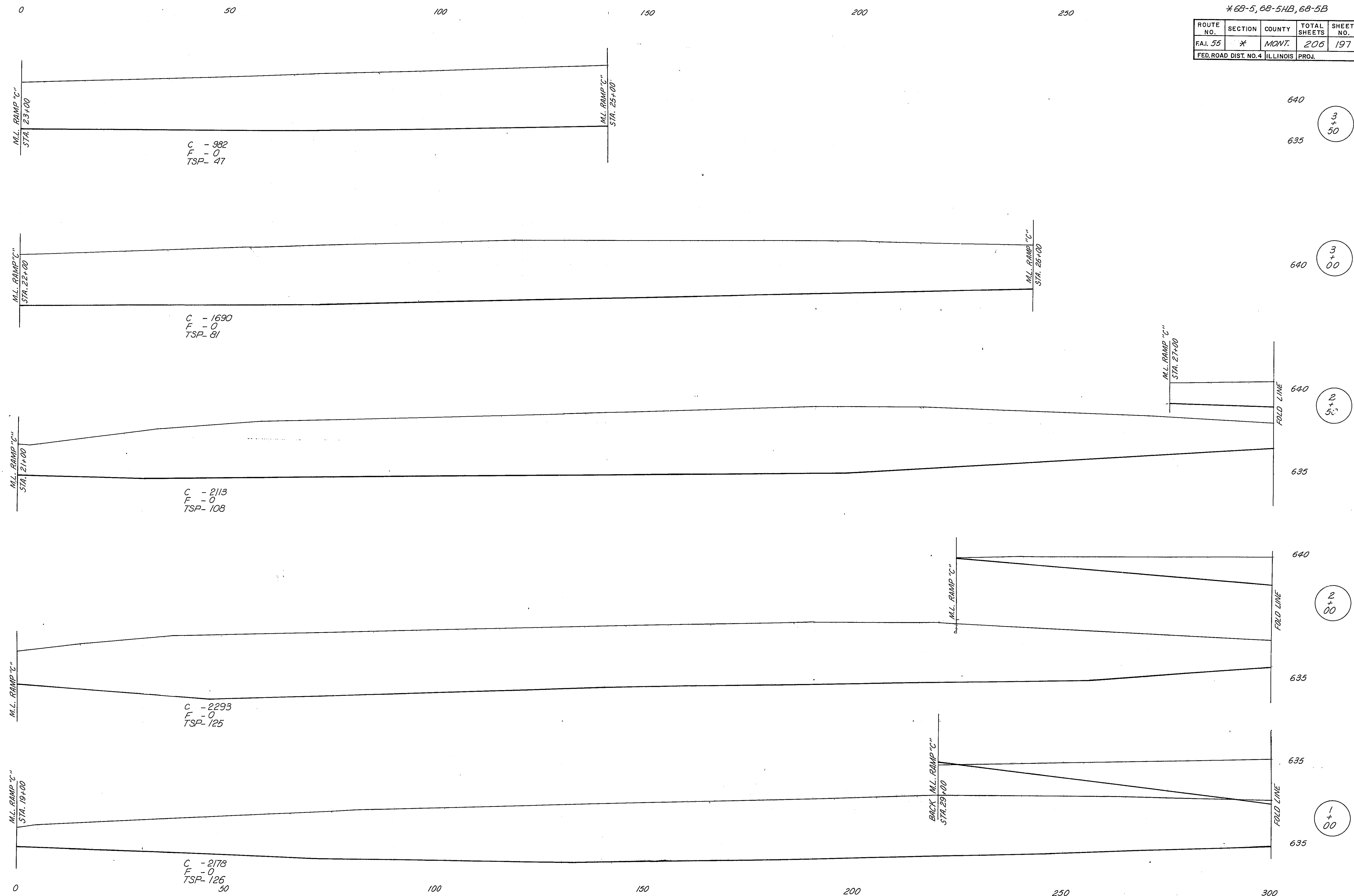


100
50
100
C.W. A.J. OCT 68
P6L
50
100



* 6B-5, 6B-5HD, 6B-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	197
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



640
3
+
50

640
3
+
00

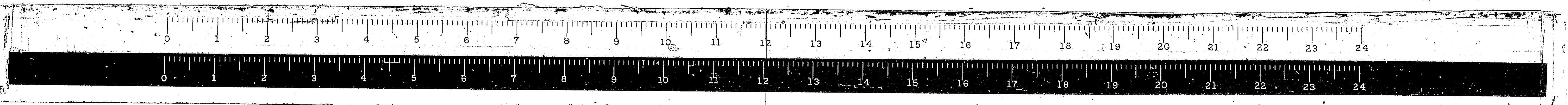
640
2
+
50

640
2
+
00

635
1
+
00

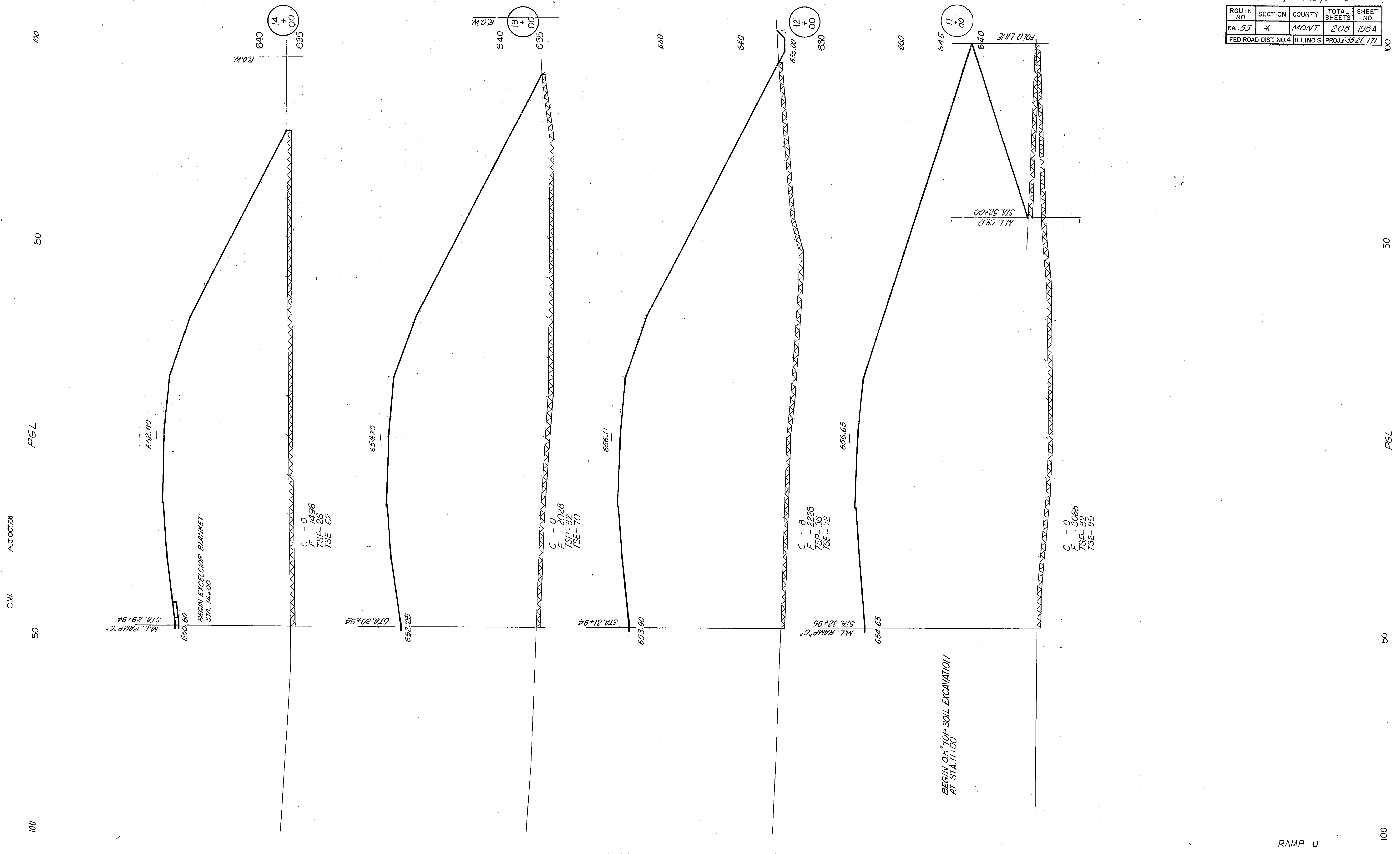
5-1870
1/4"

RAMP "C" INFIELD

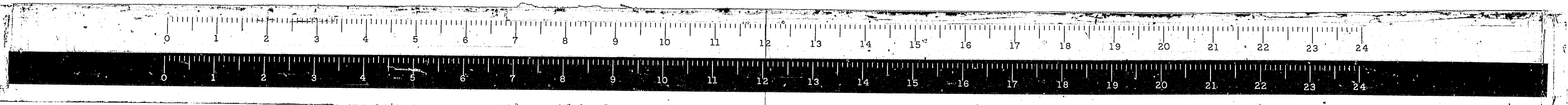


* 68-5, 68-5A, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	200	198A
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ. 5521 171	

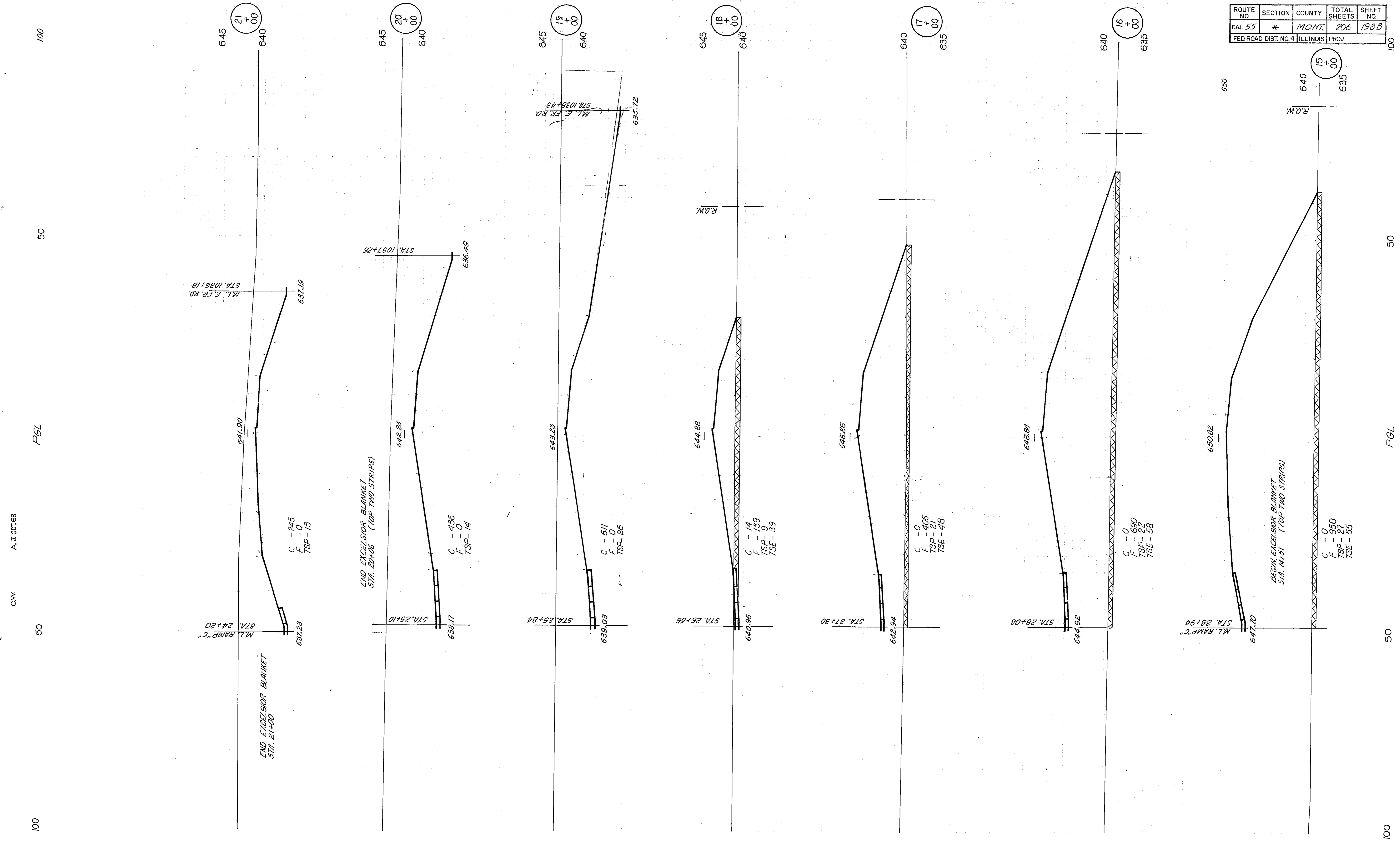


RAMP D



* 68-5, 68-5WB, 68-5B.

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
68-5	*	MONI.	206	198B
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



C.V. A.J. OCT 68

P6L

100

50

100

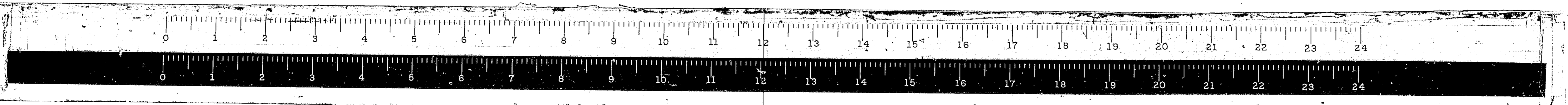
50

P6L

50

100

RAMP D



100

50

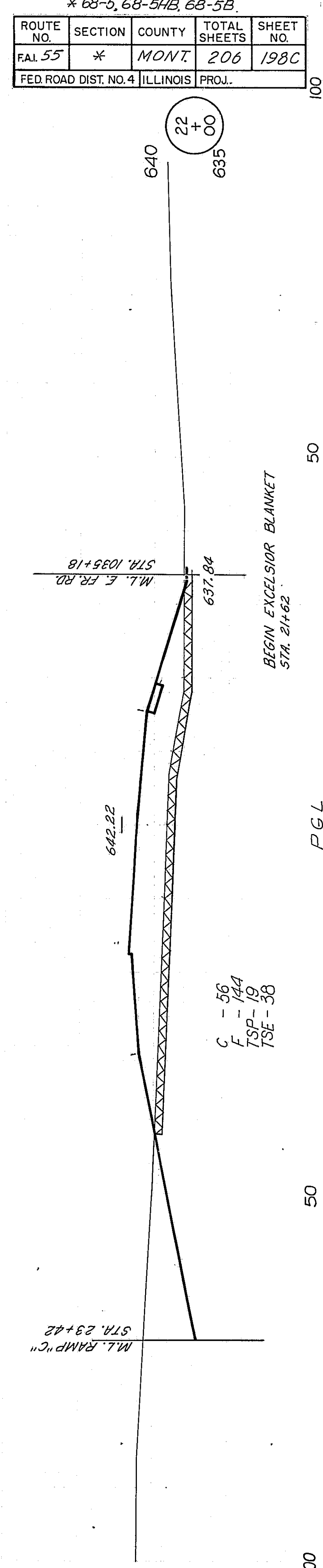
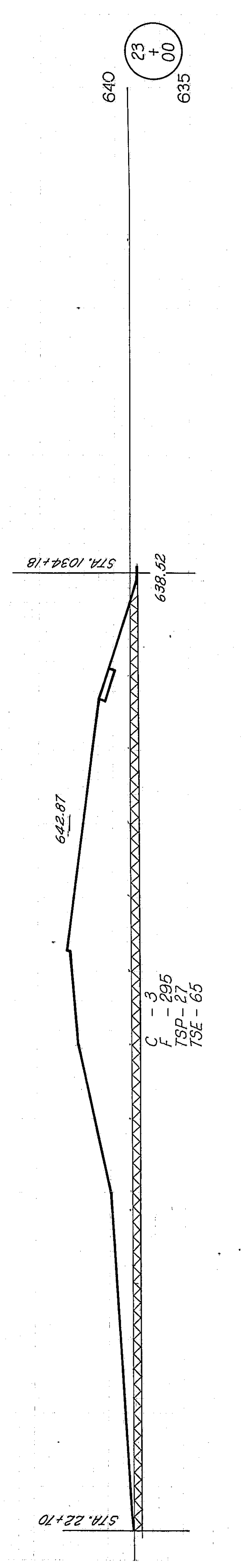
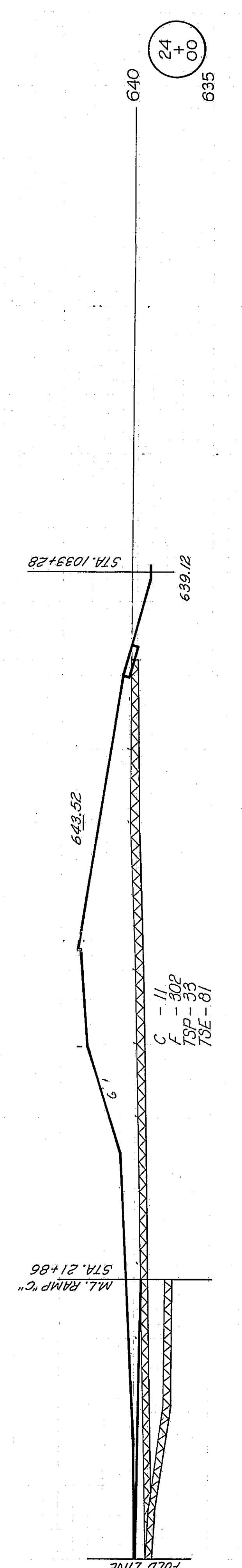
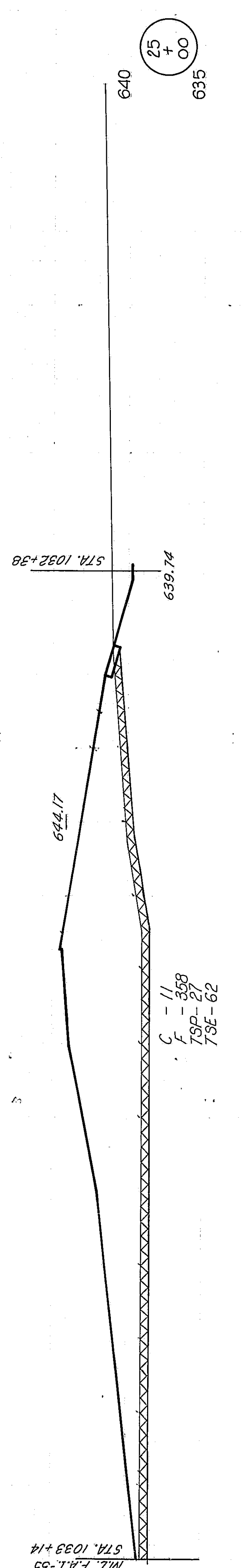
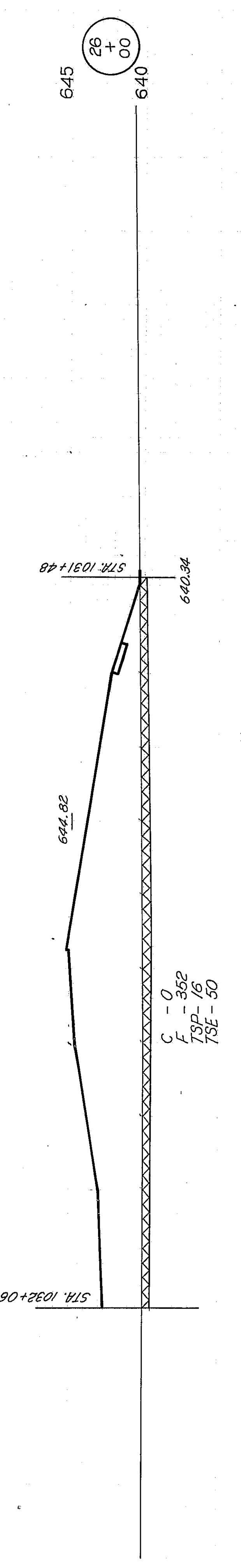
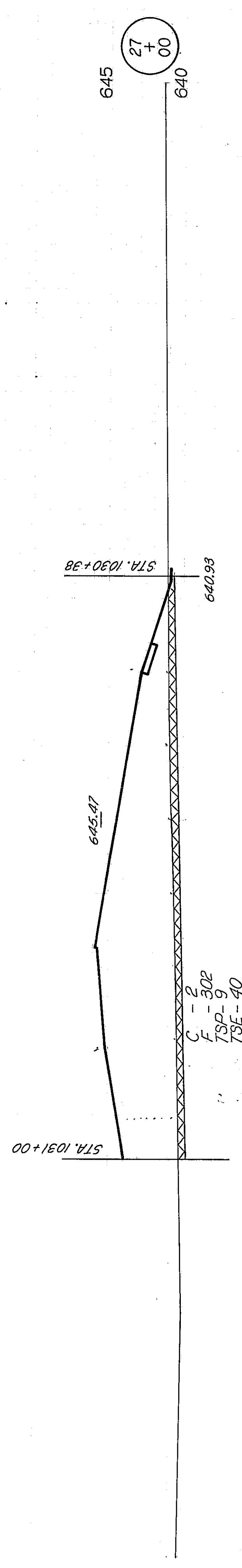
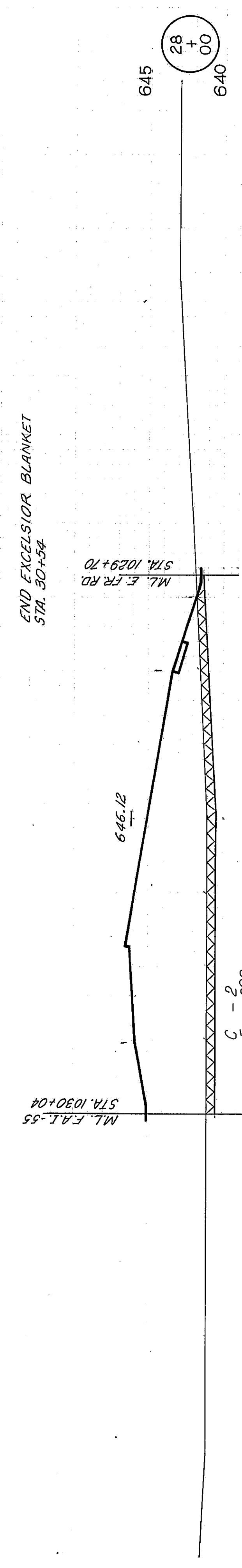
FGL

50

100

C.W. A.J. OCT 68

$$\left\{ \begin{array}{l} C = 0 \\ F = 308 \\ TSP = 7 \\ TSE = 0 \end{array} \right.$$
 STA. 20+00



* 68-5, 68-5A, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	198C
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

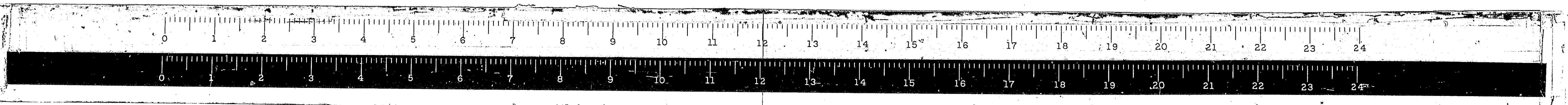
50

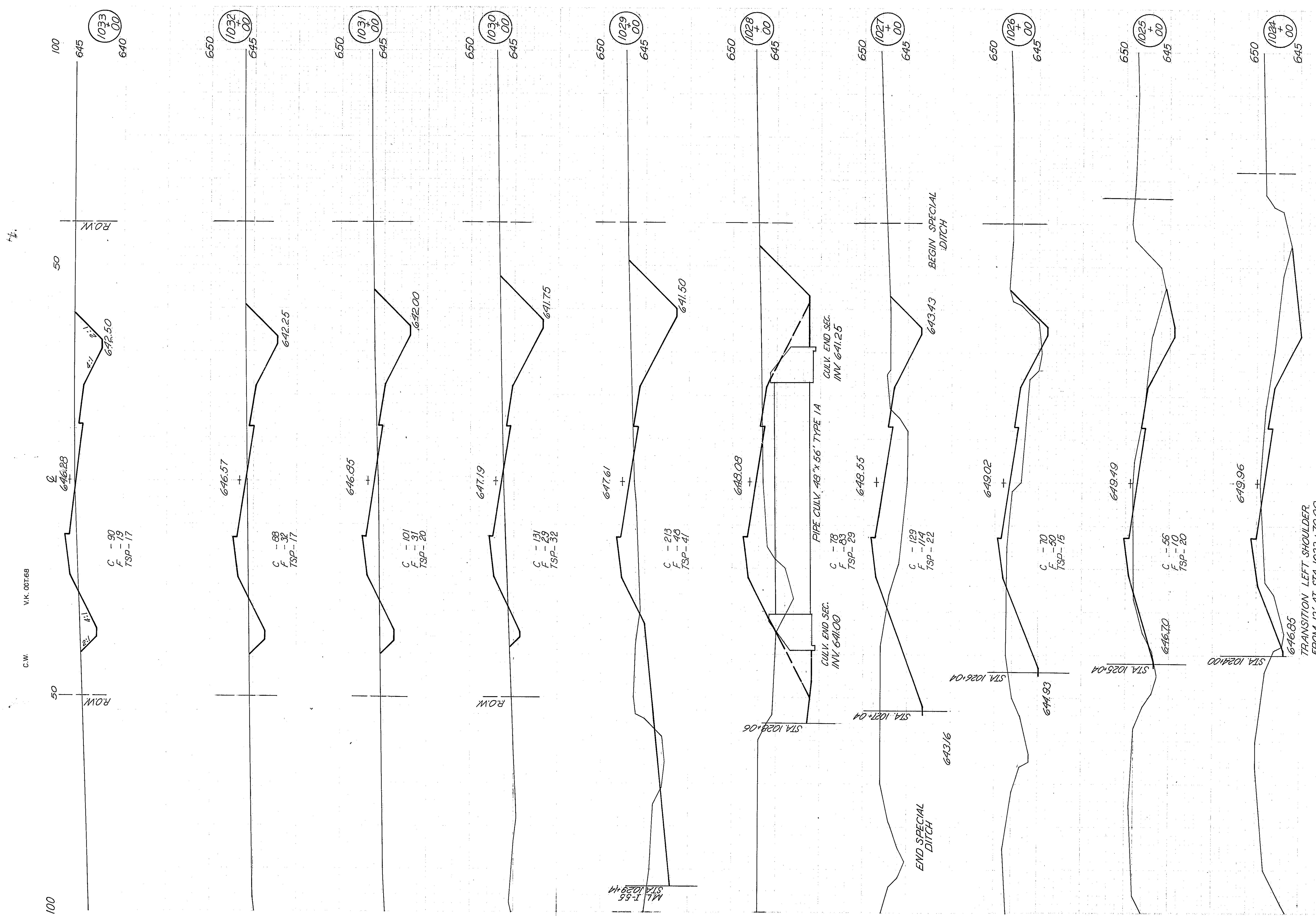
FGL

50

100

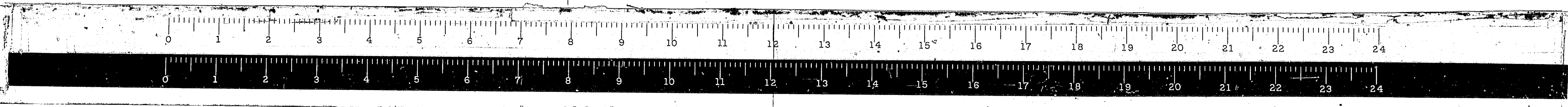
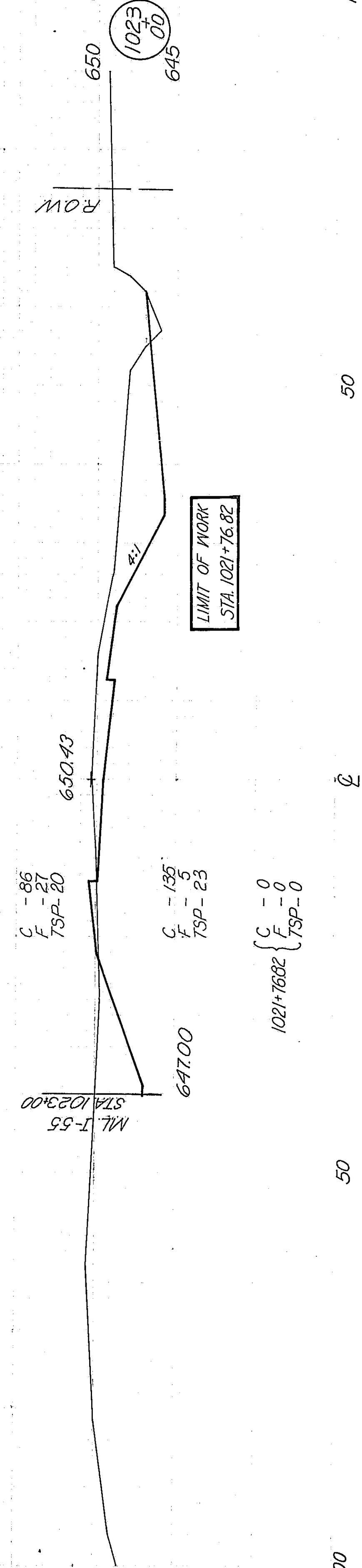
RAMP D





* 69-5, 69-5HB, 69-5B

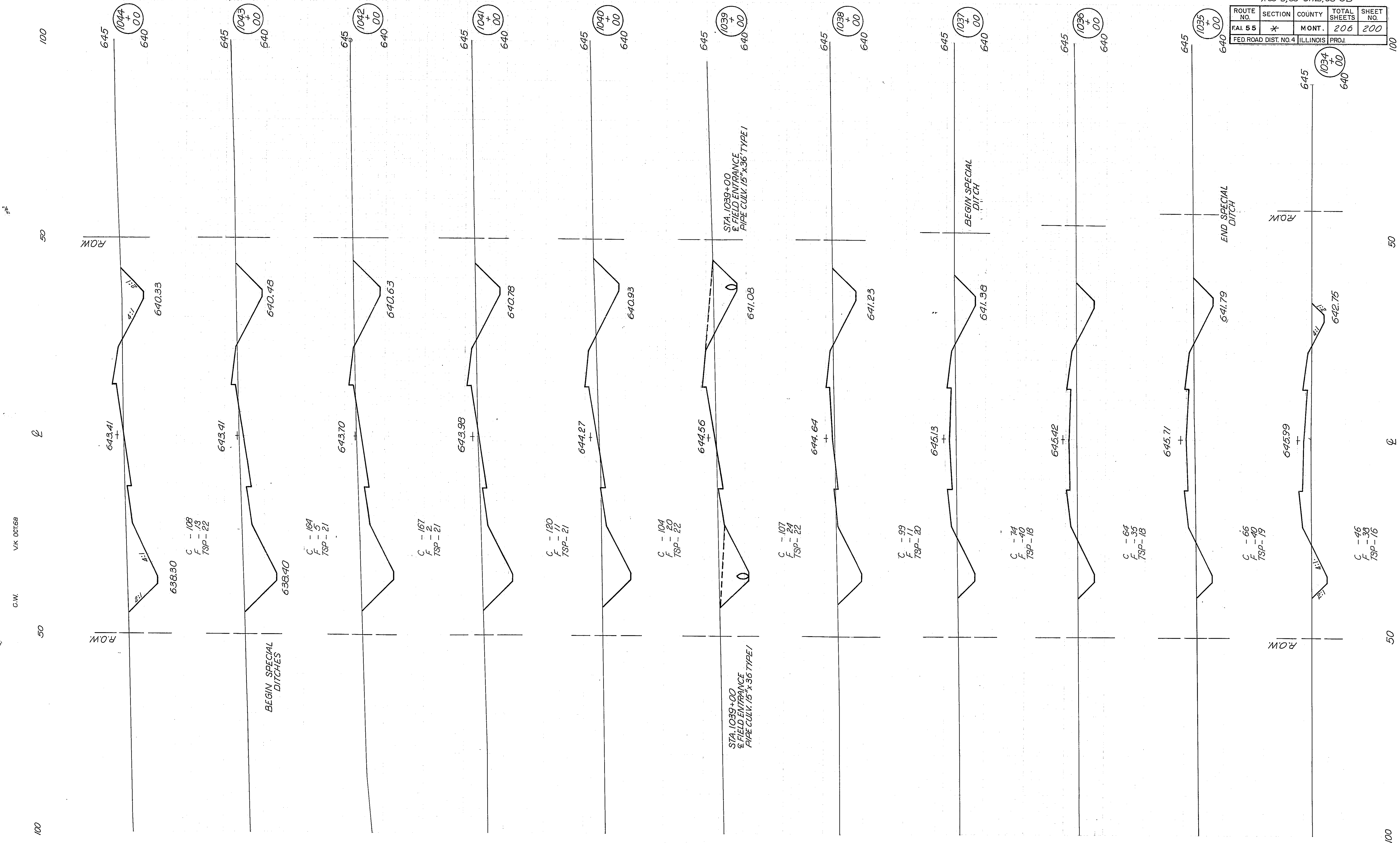
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FA155	X	MONT.	206	199
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



WEST FR. RD #1

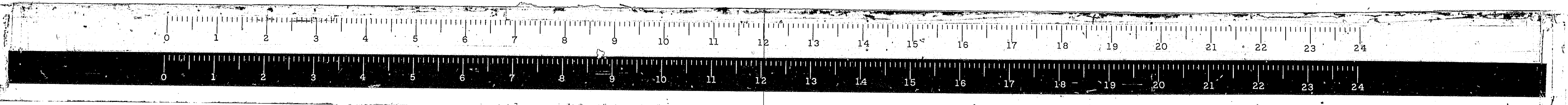
X 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	200
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



C.W. VJK 007659

100 50 50 100



WEST FR. RD.#1

C.W. V.K. OCT 68

100

50

0

50

100

645
1051
+ 00
640

M.L. CH. 17
STA. 40+23

651.84

ROW

645
1050
+ 00
640

C - 0
F - 1219
TSP - 41

END SPECIAL DITCH

645
1049
+ 00
640

M.L.

C - 163
F - 735
TSP - 49

641.00

635

648.84

645
1048
+ 00
640

END SPECIAL DITCH

640.67

C - 96
F - 312
TSP - 30

ROW

645
1047
+ 00
640

646.84

638.00

C - 45
F - 320
TSP - 28

640.34

645.13

645
1046
+ 00
640

CULV. END SEC.
INR 6581.0

PIPE CULV. 30" x 66" TYPE 2A

INV. 640.00
CULV. END SEC.

C - 88
F - 135
TSP - 23

ROW

ROW

645
1045
+ 00
640

643.99

638.20

C - 79
F - 56
TSP - 22

640.18

* 68-5,68-5HB,68-5B				
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
645	*	MONT.	206	201
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

100

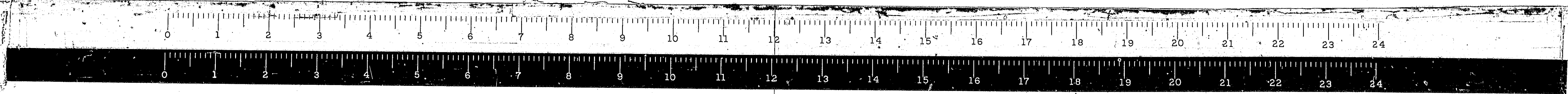
50

0

50

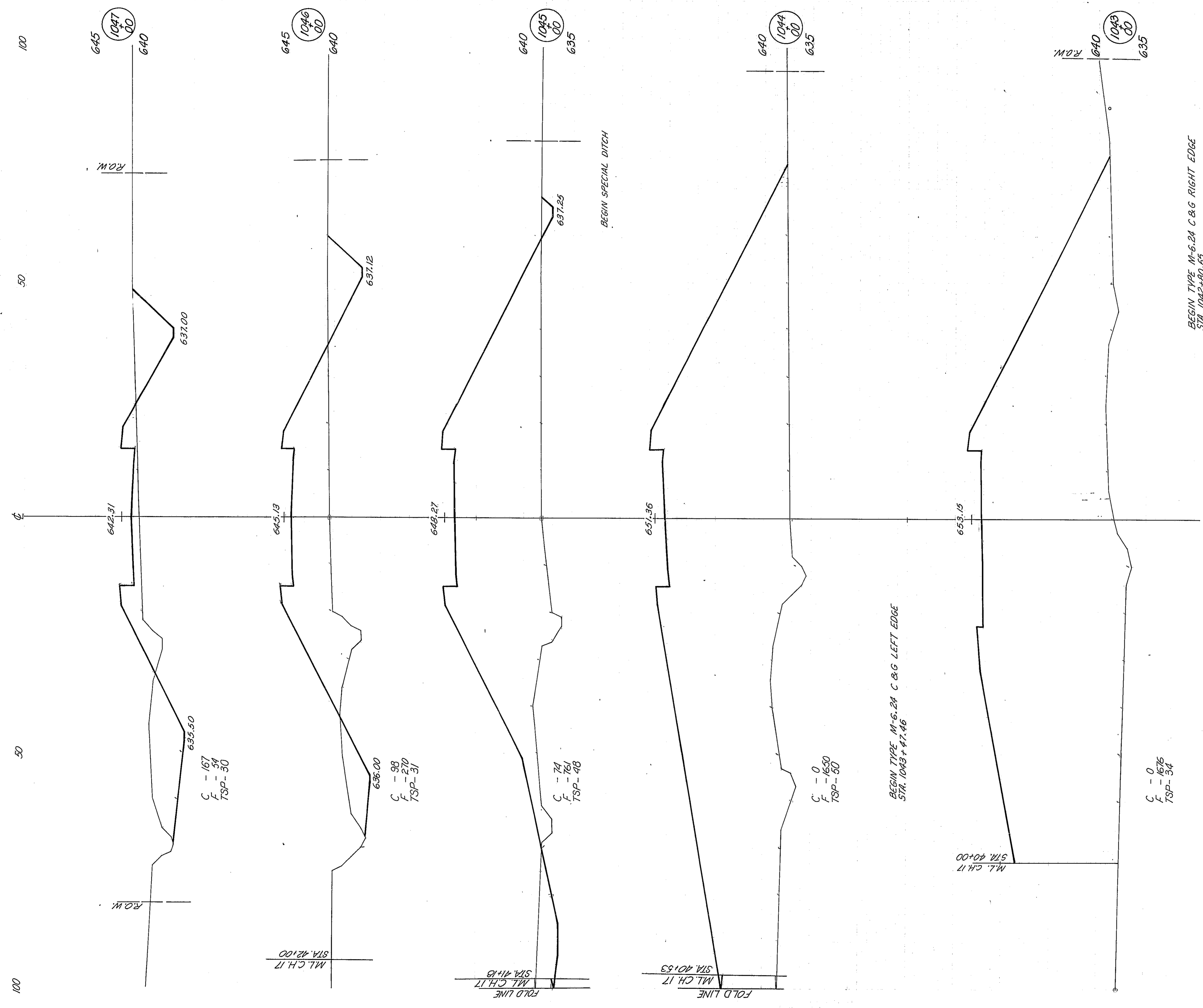
100

WEST FR. RD.#1



* 68-5, 68-54B, 68-5B

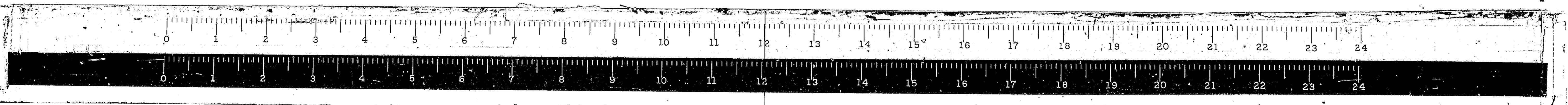
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL. 55	*	MONT.	206	203
FED. ROAD DIST. NO. 4 ILLINOIS			PROJ.	



BEGIN TYPE M-6-24 C & G RIGHT EDGE
STA. 1042+80.65

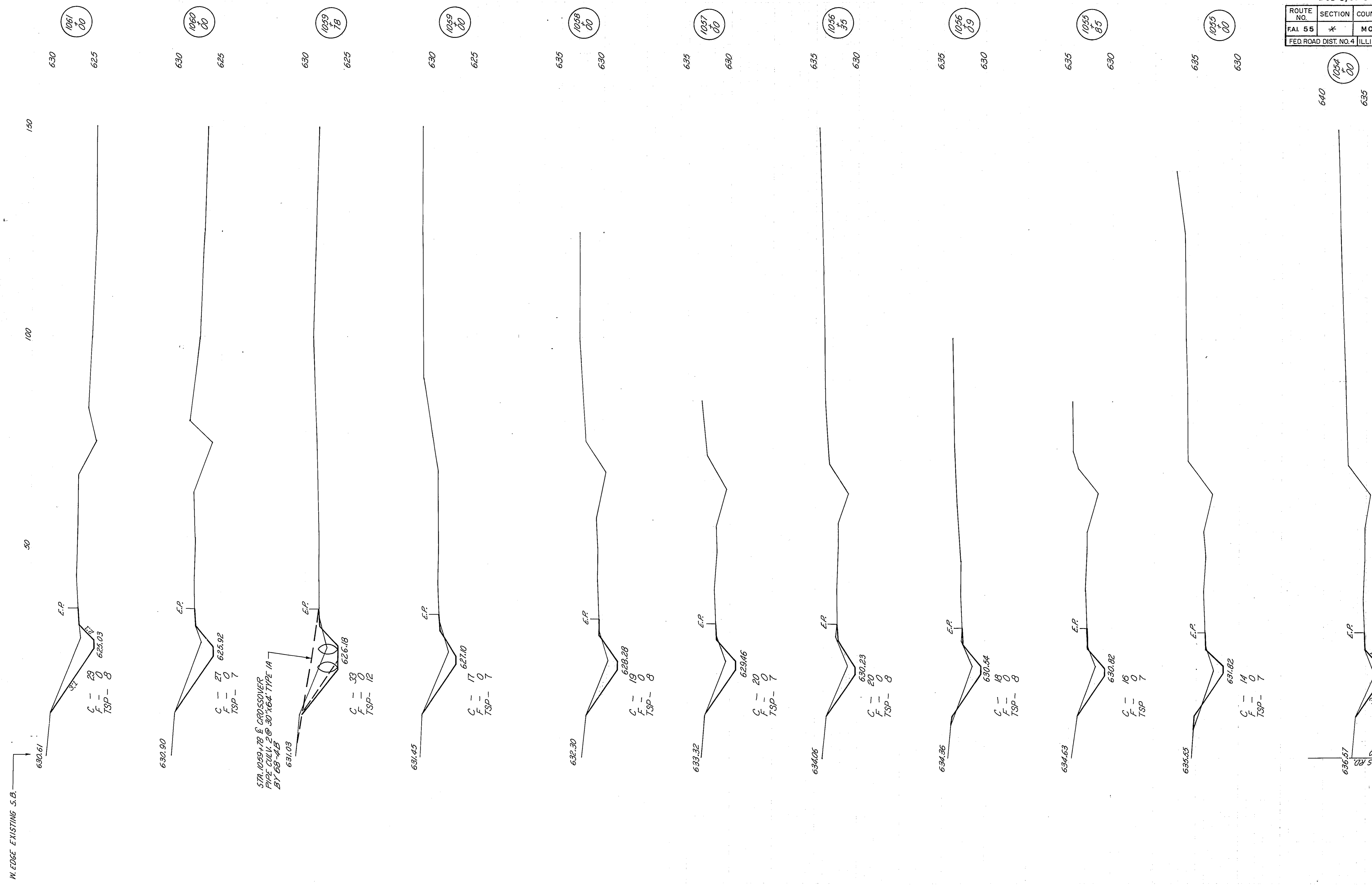
LIMIT OF WORK
STA. 1042+22.78

WEST ACCESS RD.



* 68-5, 68-5HB, 68-5B

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	205
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				



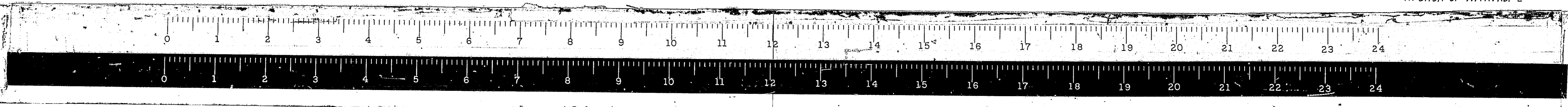
STA. 1059+78 & CROSSOVER
 1712' CURV. Z @ 30' 164. TYPE 1A
 21' 68' 445
 631.03

STA. 1053+75
 BEGIN DITCH @ ELEV. 633.08

M.L. W. ACCESS RD.
 STA. 1054+00

W. EDGE EXISTING S.B.

W. DITCH OF W. FR. RD. #2
 EXISTING S.B. (PROPOSED W. FR. RD.) #2



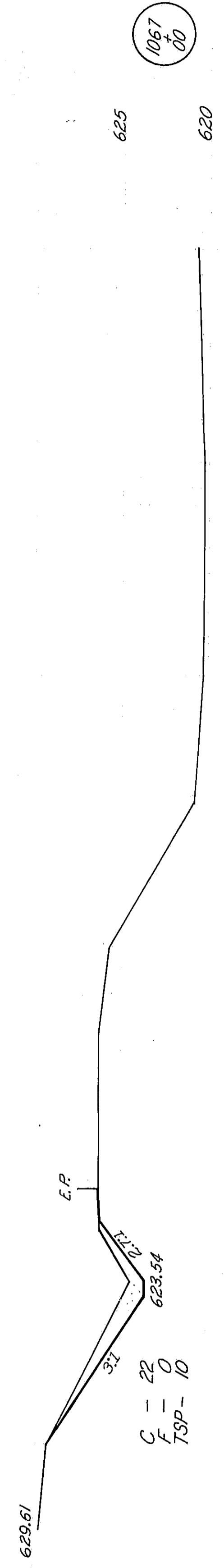
W. DITCH OF W. FR. RD. #2

W. EDGE EXISTING S.B.

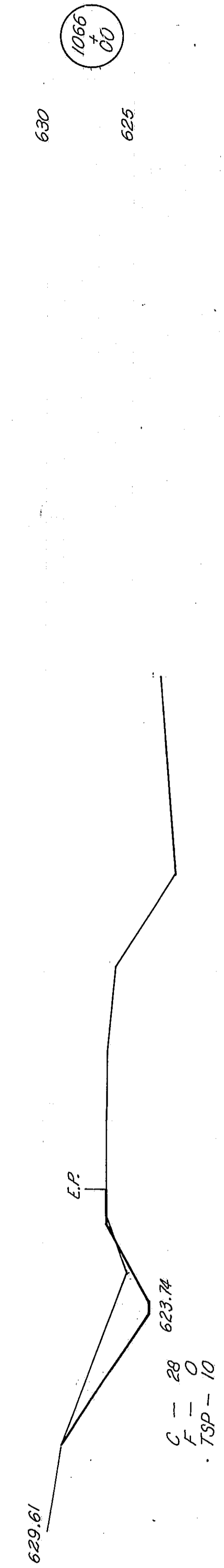
50 100 150

STA. 1067+43
DITCH DRAINS TO 36" DIA. PIPE
CUT INTO EXIST. RETAINING
WALL @ ELEV. 623.45

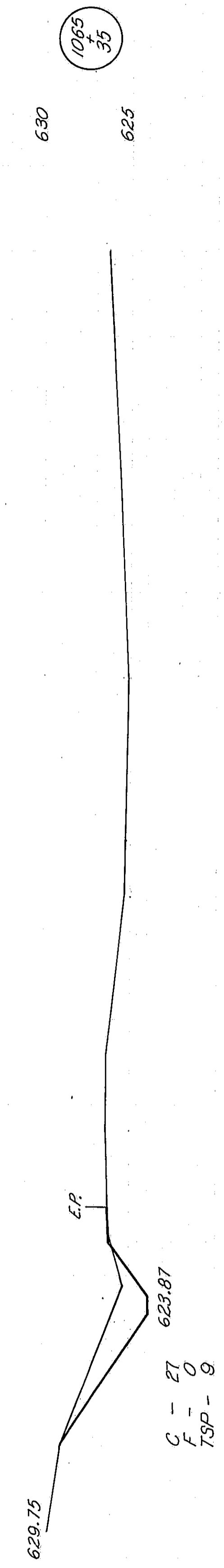
STA. 1067+43
C = 22
F = 0
TSP = 10



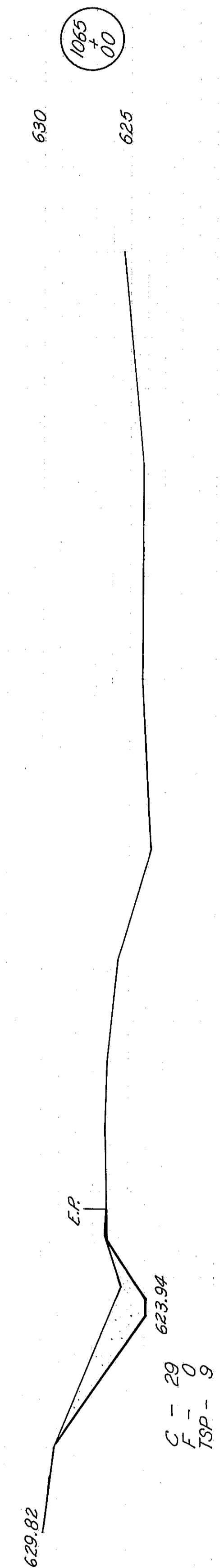
STA. 1068+00
C = 28
F = 0
TSP = 10



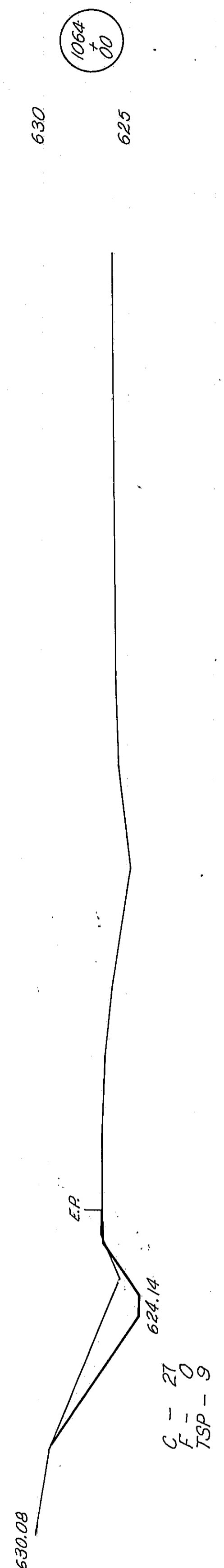
STA. 1068+57
C = 27
F = 0
TSP = 9



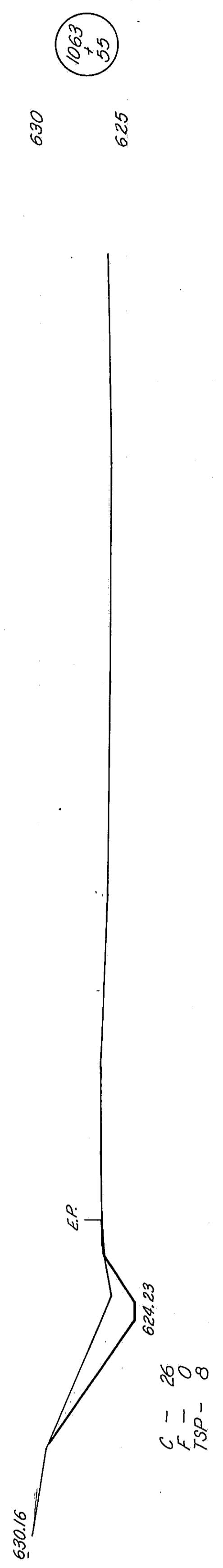
STA. 1069+14
C = 20
F = 0
TSP = 9



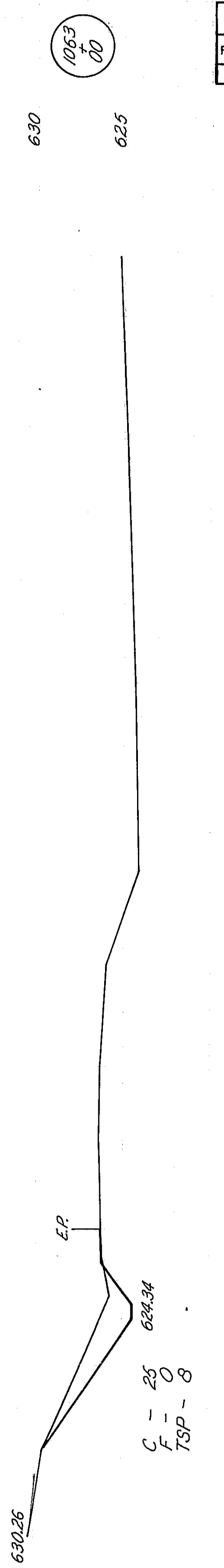
STA. 1069+71
C = 27
F = 0
TSP = 9



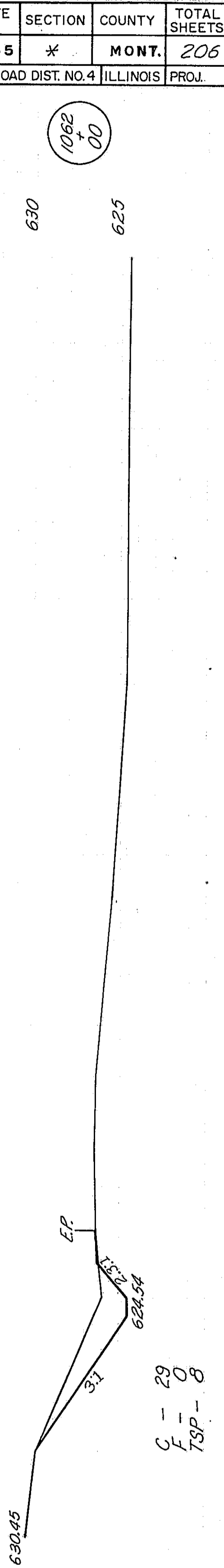
STA. 1070+28
C = 26
F = 0
TSP = 8



STA. 1070+85
C = 25
F = 0
TSP = 8



STA. 1071+42
C = 29
F = 0
TSP = 8



* 68-5, 68-5HB, 68-5B

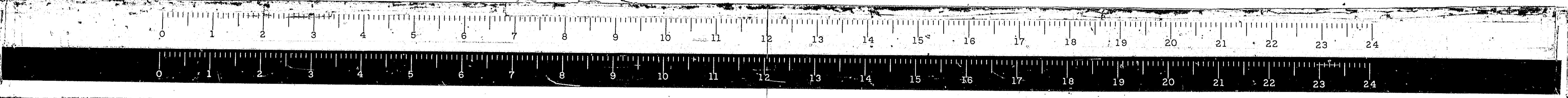
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
FAL 55	*	MONT.	206	206
FED. ROAD DIST. NO. 4 ILLINOIS PROJ.				

1062
00

W. DITCH OF W. FR. RD. #2

W. EDGE PAVEMENT
EXISTING S.B. (PROPOSED W. FR. RD. #2)

50 100 150



SECTION NO.	DES.	COUNTY	TOTAL SHEETS	SHEET NO.
FAI 55	*	MONTGOMERY	206	206A
PED ROAD DIST. NO. 7 ILLINOIS PROJECT				
* 68-5, 68-5A, 68-5B				

