

01-20-2023 LETTING ITEM 106

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR HIGHWAY STANDARDS, SEE SHEET NO. 2

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY

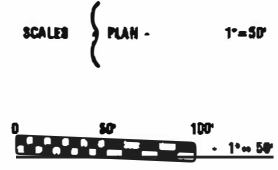
**FAU ROUTE 4309 (CRETE ROAD)
EXCHANGE STREET TO RICHTON ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
SECTION NO.: 15-00049-00-PV
PROJECT NO.: UMBW(332)
VILLAGE of CRETE
WILL COUNTY
JOB NO.: C-91-378-15**

SECTION	COUNTY	TOWNSHIP	RANGE
15-00049-00-PV	WILL	52	1
STA	TO STA.		
501+09.02	544+68.77		



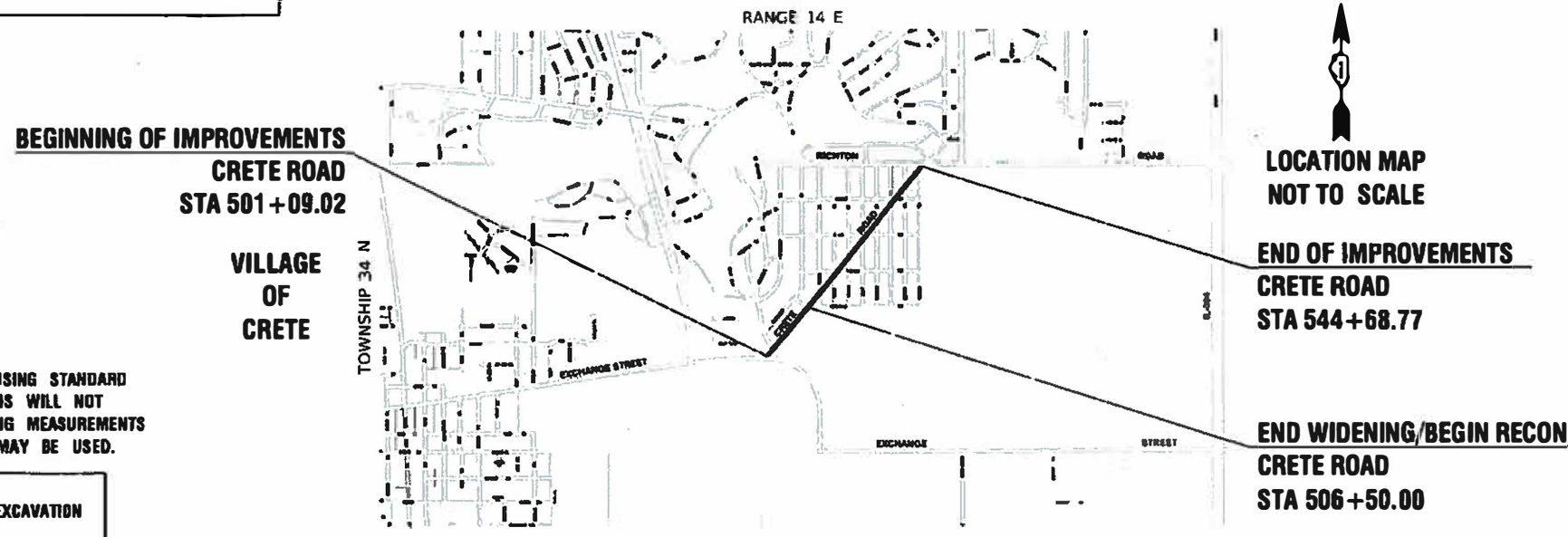
DESIGN DESIGNATION -	CRETE ROAD (FAU 4309) ADT 3,600 PV=400 SU=40 MU=0 % DESIGN TRAFFIC IN DESIGN LANE PV=99% S=1% M=0% ACTUAL TF=.05 SSS=FAIR CRETE ROAD
2019 ADT -	1,925 (1.1% TRUCK)
2040 ADT -	3,600 (1.1% TRUCK)
POSTED SPEED LIMIT -	35 mph
DESIGN PERIOD -	25 YEARS
DESIGN SPEED LIMIT -	35 mph
STREET CLASSIFICATION -	URBAN MINOR COLLECTOR

I.D.O.T. FEDERAL AID PROGRAM ENGINEER: CARMEN RAMOS, PE, SCHAMBOURG, IL
CONSULTANTS: ROBINSON ENGINEERING, LTD., 708-331-6700



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
DR 811



LOCATION MAP
GROSS LENGTH- 4,260 FEET- 0.83 MILES
NET LENGTH- 4,260 FEET- 0.83 MILES

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
[Signature]
09-30-22



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	
APPROVED	<i>[Signature]</i> 10-25-2022 VILLAGE OF CRETE, MAYOR
PASSED	<i>[Signature]</i> 10-31-2022 DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW	<i>[Signature]</i> October 31, 2022 REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY OF
THE STATE OF ILLINOIS**

CONTRACT NO. 61H99

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EARTHWORK QUANTITIES

TOTAL CUT	=	7,701	CY
TOTAL EXISTING PAVEMENT REMOVAL	=	2,100	CY
TOTAL AVAILABLE CUT TO FILL	=	5,601	CY
TOTAL FILL	=	258	CY
CUT TO FILL (15% SHRINKAGE)	=	297	CY
EXCESS HAUL AWAY MATERIAL	=	5,304	CY

GENERAL NOTES

- ALL ROADWAY CONSTRUCTION SHALL CONFORM TO THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022 BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION AND ALL AMENDMENTS THERETO.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING THE UTILITY COMPANIES LOCATE THEIR FACILITIES IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL ALSO BE RESPONSIBLE FOR THE MAINTENANCE AND PRESERVATION OF THESE FACILITIES. THE ENGINEER DOES NOT WARRANT THE LOCATION OF ANY EXISTING UTILITIES SHOWN ON THE PLAN. THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 800-892-0123 AND THE VILLAGE OF CRETE FOR UTILITY LOCATIONS.
- THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE NATURE AND STATUS OF ALL UTILITY RELOCATION WORK PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO ENSURE THAT CONSTRUCTION OPERATIONS DO NOT INTERFERE WITH UTILITY FACILITIES AND RELOCATION WORK. THE SCHEDULE SHOULD REFLECT CONSTRUCTION SEQUENCING, WHICH COORDINATES WITH ALL UTILITY RELOCATION WORK. THE CONTRACTOR SHALL BE REQUIRED TO ADJUST THE ORDER OF ITS WORK FROM TIME TO TIME, TO COORDINATE SAME WITH UTILITY RELOCATION WORK, AND SHALL PREPARE REVISED SCHEDULE (S) IN COMPLIANCE THEREWITH AS DIRECTED BY THE OWNER. THE OWNER AND THE ENGINEER SHALL BE NOTIFIED IN WRITING BY THE CONTRACTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY OPERATION REQUIRED COOPERATION WITH OTHERS. ALL OTHER AGENCIES, UNLESS OTHERWISE NOTED, WILL BE NOTIFIED IN WRITING BY THE CONTRACTOR TEN (10) DAYS PRIOR TO THE START OF ANY SUCH OPERATION. THE UTILITY COMPANIES HAVE BEEN CONTACTED IN REFERENCE TO UTILITIES THEY OWN AND OPERATE WITHIN THE LIMITS FOR THIS PROJECT. ALL KNOWN DATA FROM THESE AGENCIES HAS BEEN INCORPORATED INTO THE PLANS. IT IS HOWEVER, THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM OR ESTABLISH THE EXISTENCE OF ALL UTILITY FACILITIES AND THEIR EXACT LOCATIONS, WHETHER CONTAINED IN THE DATA SUBMITTED BY THESE AGENCIES OR NOT, AND TO SAFELY SCHEDULE ALL UTILITY RELOCATIONS.
- THE CONTRACTOR SHALL PRESERVE ALL CONSTRUCTION STAKES UNTIL THEY ARE NO LONGER NEEDED. ANY STAKES DESTROYED OR DISTURBED BY THE CONTRACTOR PRIOR TO THEIR USE SHALL BE RESET BY THE CONTRACTOR AS APPROVED BY THE ENGINEER.
- REMOVAL OF SPECIFIED ITEMS, INCLUDING BUT NOT LIMITED TO, PAVEMENT, SIDEWALK, CURB, CURB AND GUTTER, CULVERTS, ETC. SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR ANY PERMITS REQUIRED FOR SUCH DISPOSAL. THE REMOVAL SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER.
- THE CONTRACTOR SHALL COLLECT AND REMOVE ALL CONSTRUCTION DEBRIS, EXCESS MATERIALS, TRASH, OIL AND GREASE RESIDUE, MACHINERY, TOOLS AND OTHER MISCELLANEOUS ITEMS WHICH WERE NOT PRESENT PRIOR TO PROJECT COMMENCEMENT AT NO ADDITIONAL EXPENSE TO THE OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ANY AND ALL PERMITS NECESSARY FOR THE HAULING AND DISPOSAL REQUIRED FOR CLEAN-UP AS DIRECTED BY THE ENGINEER OR OWNER. BURNING ON THE SITE IS NOT PERMITTED.
- AT THE CLOSE OF EACH WORKING DAY AND AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS.
- TREES NOT MARKED FOR REMOVAL SHALL BE CONSIDERED AS DESIGNATED TO BE SAVED AND SHALL BE PROTECTED UNDER THE PROVISIONS OF ARTICLE 201.05 OF THE SSRBC.
- THE TRENCHES FOR PIPE INSTALLATION SHALL BE KEPT DRY AT ALL TIMES DURING PIPE PLACEMENT. APPROPRIATE FACILITIES TO MAINTAIN THE DRY TRENCH SHALL BE PROVIDED BY THE CONTRACTOR.
- TRENCH BACKFILL WILL BE REQUIRED TO THE FULL DEPTH ABOVE SEWERS AND WATER MAIN WITHIN TWO (2) FEET OF PROPOSED OR EXISTING PAVEMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS FROM THE NOMINAL THICKNESS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
- ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES BY LIMITING CURB AND GUTTER REPAIR TO ONE-HALF THE DRIVEWAY WIDTH AT ONE TIME AS THROUGH THE USE OF AGGREGATE FOR TEMPORARY ACCESS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON VILLAGE RIGHT OF WAY WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- THE ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD TECHNICIAN, PATRICE HARRIS, AT PATRICE.HARRIS@ILLINOIS.GOV TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS,
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV, ROBINSON ENGINEERING (708)331-6700 AND THE VILLAGE OF CRETE (708)672-5431 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLAN MATERIAL REQUIREMENTS STATED HEREIN.
- INSTALL ALL CHANGEABLE MESSAGE SIGNS, INFORMING THE PUBLIC OF THE DETOUR, AT LEAST ONE WEEK PRIOR TO THE ROADWAY CLOSURE.
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.

STORM SEWER NOTES

- ON ALL IMPROVEMENTS THE FRAMES AND LIDS OF EXISTING CATCH BASINS, INLETS, MANHOLES AND VALVE VAULTS WHICH ARE TO BE ABANDONED DUE TO CONSTRUCTION OF THIS IMPROVEMENT ARE TO REMAIN THE PROPERTY OF THE VILLAGE OF CRETE AND BE SALVAGED. THE OWNER SHALL BE NOTIFIED AS TO AVAILABILITY FOR PICK-UP.
- THE TOP OF ALL STRUCTURES SHALL BE FLUSH WITH THE ADJACENT SURFACE OR AT THE INDICATED ELEVATIONS SHOWN ON THE PLANS.
- FRAME ELEVATIONS ARE GIVEN ONLY TO ASSIST IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED.
- PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER.
- BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ARTICLE 550.07 (b, c) OF THE SSRBC WILL NOT BE ALLOWED.

EARTHWORK NOTES

- GENERAL
 - IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE SOIL AND GROUNDWATER CONDITIONS AT THE SITE.
 - THE CONTRACTOR WILL NOTE THAT THE ELEVATIONS SHOWN ON THE CONSTRUCTION PLANS ARE FINISHED GRADE AND SUBGRADE ELEVATIONS (AS NOTED) AND THAT PAVEMENT THICKNESS, TOPSOIL, ETC. MUST BE ACCOUNTED FOR.
 - THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE DURING CONSTRUCTION, AND PREVENT STORMWATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS. THE FAILURE TO PROVIDE PROPER DRAINAGE WILL NEGATE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF. FINAL GRADES SHALL BE PROTECTED AGAINST DAMAGE FROM EROSION, SEDIMENTATION AND TRAFFIC.
 - PLANS FOR THE SITE DEWATERING, IF EMPLOYED, SHALL BE SUBMITTED AND APPROVED PRIOR TO IMPLEMENTATION.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTATION OF THE "SOIL EROSION AND SEDIMENTATION CONTROL MEASURES". THE INITIAL ESTABLISHMENT OF EROSION CONTROL PROCEDURES AND THE PLACEMENT OF SILT AND FILTER FENCING, ETC. TO PROTECT ADJACENT PROPERTY, WETLANDS, ETC. SHALL OCCUR BEFORE GRADING BEGINS.
 - ALL STORM INLETS SHALL BE PROTECTED BY INLET FILTERS. PLACEMENTS AND MAINTENANCE OR SILT BARRIER SHALL BE AS DIRECTED BY THE ENGINEER, BASED ON ACTUAL GRADING. GRADE THE AREA WITHIN FOUR (4) FEET AROUND STRUCTURES ONE (1) FOOT BELOW RIM TO SERVE AS A SEDIMENTATION BASIN DURING CONSTRUCTION.
 - FINAL LOCATION OF SILT FENCE SHALL BE ADJUSTED BASED ON ACTUAL SITE GRADING CONDITIONS. ADDITIONAL MEASURES SHALL BE ADDED AS DIRECTED BY THE ENGINEER.
 - ALL AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESEEDD AS SOON AS PRACTICAL.
 - THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
 - AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
 - ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

LANDSCAPING NOTES

- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN THIS NOTE SHALL APPLY TO ANY TREE INDIVIDUALLY NOTED ON THE PLANS THAT IS NOT OTHERWISE CALLED OUT FOR REMOVAL. THIS NOTE SHALL NOT APPLY TO REMOVALS NECESSARY FOR GRADING THAT ARE ILLUSTRATED BY AS A BRUSH/TREE LINE ON THE CONTRACT PLANS.

COMMITMENTS

NONE

FILE NAME = 21R0016-INDX-01 - P01	USER NAME =	DESIGNED -- JPH	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION			F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		CHECKED -- WPD	REVISED --		4309	15-00049-00-PV	WILL	52	2			
	PLOT SCALE =	DRAWN -- RG	REVISED --		INDEX OF SHEETS & STATE STANDARDS				CONTRACT NO. 61H99			
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --		SCALE: NONE	SHEET NO. 2	OF 52 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT UMBW(332)

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0003	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
		FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%				
		20101100	TREE TRUNK PROTECTION	EACH	9			9	
	XX	20101200	TREE ROOT PRUNING	EACH	9			9	
	XX	20101700	SUPPLEMENTAL WATERING	UNIT	170	170			
		20200100	EARTH EXCAVATION	CU YD	5,300	5,300			
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	100	100			
		20800150	TRENCH BACKFILL	CU YD	520	520			
		21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	7,500	7,500			
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	17,000			17,000	
	XX	25000210	SEEDING, CLASS 2A	ACRE	3.0			3.0	
	XX	25000310	SEEDING, CLASS 4	ACRE	0.75			0.75	
	XX	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	270			270	

FILE NAME = 21R018-QUAN-01 - 001

USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: NONE | SHEET NO. 3 OF 52 SHEETS | STA. TO STA.

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	3
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61H99	
FED. AID PROJ. ECT UMBW(332)				

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0004	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
FED 80%/LPA 20%		FED 80%/LPA 20%		FED 80%/LPA 20%		FED 80%/LPA 20%			
	XX	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	270			270	
	XX	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	270			270	
	XX	25100630	EROSION CONTROL BLANKET	SQ YD	17,000			17,000	
		28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	360			360	
		28000305	TEMPORARY DITCH CHECKS	FOOT	560			560	
		28000400	PERIMETER EROSION BARRIER	FOOT	11,250			11,250	
		28000510	INLET FILTERS	EACH	41			41	
		28100107	STONE RIPRAP, CLASS A4	SQ YD	75			75	
		28200200	FILTER FABRIC	SQ YD	75			75	
		30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	100	100			
		30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	15,000	15000			
X		31000600	PROCESSING LIME STABILIZED SOIL MIXTURE 12"	SQ YD	7,500	7500			

FILE NAME = 21R0316-QUAN-01 - 002

USER NAME =	DESIGNED -- JPH	REVISED --
PLOT SCALE =	CHECKED -- WPD	REVISED --
PLOT DATE = 11-16-22	DRAWN -- RG	REVISED --
	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 4 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)			CONTRACT NO. 611-99	

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0004	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
		FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%				
		31001500	LIME	TON	375	375			
		40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	50	50			
		40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	28,000	28,000			
		40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	6,820	6,820			
		40600370	LONGITUDINAL JOINT SEALANT	FOOT	8,200	8,200			
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	35	35			
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	4,210	4,210			
		40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1,200	1,200			
		44000100	PAVEMENT REMOVAL	SQ YD	9,800	9,800			
		44000163	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2"	SQ YD	1,810	1,810			
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	260	260			
		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	50	50			

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNCS	TRAINEES
						0004	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
		FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%				
		44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	20	20			
		44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	30	30			
		44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	40	40			
		44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	50	50			
		50105220	PIPE CULVERT REMOVAL	FOOT	180	180			
		54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	2	2			
		54010302	PRECAST CONCRETE BOX CULVERTS 3' X 2'	FOOT	57	57			
		54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	1	1			
		54213681	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 36"	EACH	1	1			
		550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	892	892			
		550A0070	STORM SEWERS, CLASS A, TYPE 1 15"	FOOT	364	364			
		550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	574	574			

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0004	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
FED 80%/LPA 20%		FED 80%/LPA 20%		FED 80%/LPA 20%		FED 80%/LPA 20%			
		550A0120	STORM SEWERS, CLASS A, TYPE 1 24"	FOOT	940	940			
		550A0140	STORM SEWERS, CLASS A, TYPE 1 30"	FOOT	375	375			
		550A0450	STORM SEWERS, CLASS A, TYPE 2 36"	FOOT	1,389	1,389			
X	XX	56106600	ADJUSTING WATER MAIN 12"	FOOT	200	200			
X	XX	56400300	FIRE HYDRANTS TO BE ADJUSTED	EACH	15	15			
		60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	1,000	1,000			
		60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	1	1			
		60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	6	6			
		60204505	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 8 GRATE	EACH	5	5			
		60204805	CATCH BASINS, TYPE A, 5'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	1	1			
		60207605	CATCH BASINS, TYPE C, TYPE 8 GRATE	EACH	1	1			
		60207905	CATCH BASINS, TYPE C, TYPE 11 FRAME AND GRATE	EACH	15	15			

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0003	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%						
		60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1			
		60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	4	4			
		60224005	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 8 GRATE	EACH	1	1			
		60224448	MANHOLES, TYPE A, 7'-DIAMETER, TYPE 8 GRATE	EACH	2	2			
		60224458	MANHOLES, TYPE A, 8'-DIAMETER, TYPE 8 GRATE	EACH	1	1			
		60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	3	3			
		60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	7	7			
		60266600	VALVE BOXES TO BE ADJUSTED	EACH	5.0	5.0			
		60500060	REMOVING INLETS	EACH	1	1			
		60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	8,720	8,720			
		67100100	MOBILIZATION	LSUM	1	1			
		70107025	CHANGEABLE MESSAGE SIGN	CAL DA	90		90		

FILE NAME = 21R016-QUA01-Q06

USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-10-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
SUMMARY OF QUANTITIES

SCALE: NONE SHEET NO. 8 OF 52 SHEETS STA. TO STA.

F.A.U. RATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	8
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61H99	
FED. AID PROJECT			UMBW(332)	

S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0003	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
		FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%	FED 80%/LPA 20%				
	XX	72000100	SIGN PANEL - TYPE 1	SQ FT	48		48		
X		72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	7		7		
		72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1		1		
	XX	72900200	METAL POST - TYPE B	FOOT	90		90		
	XX	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	8,550		8550		
	XX	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	20		20		
X		X0100029	EXPLORATORY EXCAVATION	FOOT	160	160			
X		X0322917	PROPOSED STORM SEWER CONNECTION TO EXISTING MANHOLE	EACH	2	2			
X	XX	X0327112	STUMP REMOVAL, ACRES	ACRE	1			1	
X	XX	X2010400	STUMP REMOVAL ONLY	UNIT	352			352	
X		X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1			
X		Z0004514	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"	SQ YD	340	340			

FILE NAME = 21R0316-QUAN-01 - 007

USER NAME =	DESIGNED - JPH	REVISED -
	CHECKED - WPD	REVISED -
PLOT SCALE =	DRAWN - RG	REVISED -
PLOT DATE = 11-16-22	CHECKED - AG	REVISED -

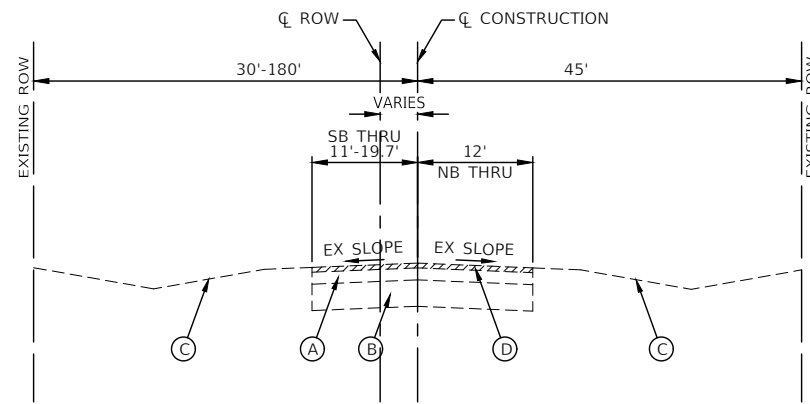
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
SUMMARY OF QUANTITIES

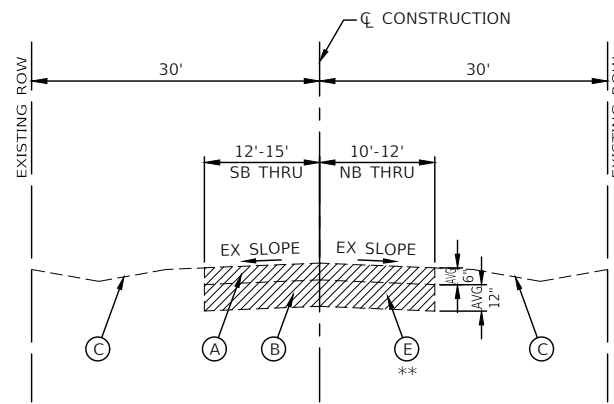
SCALE: NONE SHEET NO. 9 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	9
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

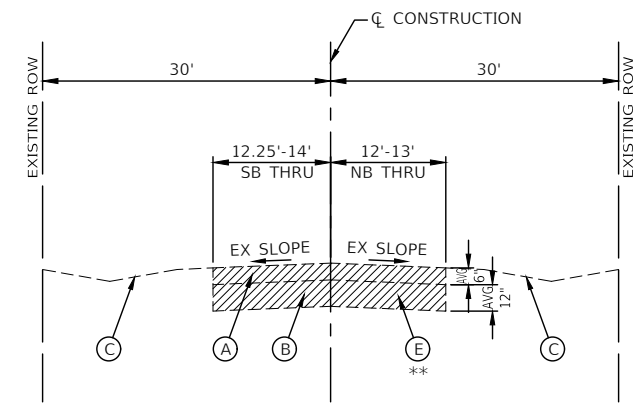
S.P.	SPECIALTY ITEM	PAY CODE	PAY ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE			
						ROADWAY	SAFETY	LNSC	TRAINEES
						0004	0021	0031	0042
						URBAN	URBAN	URBAN	URBAN
FED 80%/LPA 20%		FED 80%/LPA 20%		FED 80%/LPA 20%		FED 80%/LPA 20%			
X		Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	5	5			
X	XX	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	4	4			
X		Z0076600	TRAINEES	HOUR	500				500
		Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500				500



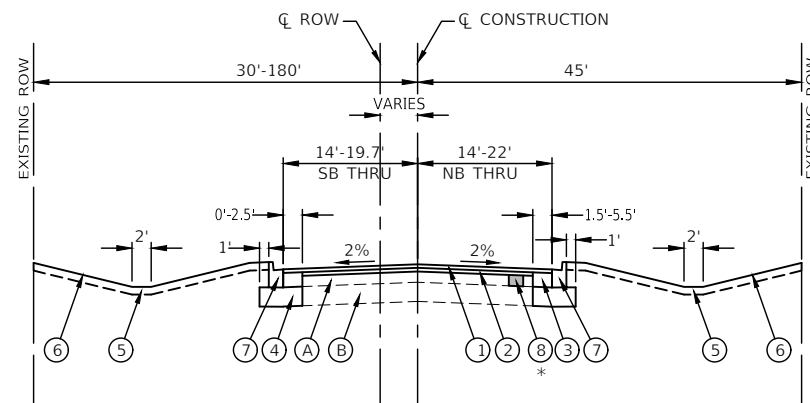
EXISTING TYPICAL SECTION
CRETE ROAD
STA 501+09.02 TO STA 506+50.00



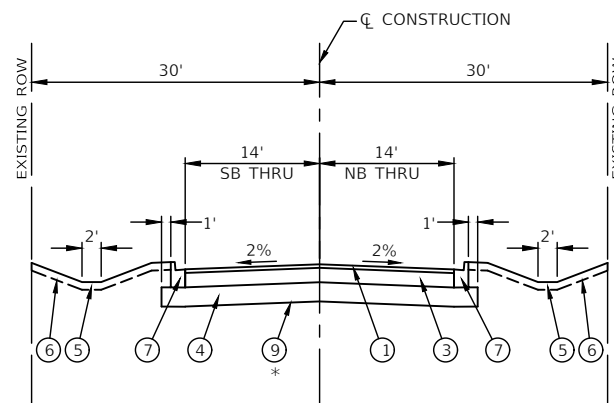
EXISTING TYPICAL SECTION
CRETE ROAD
STA 506+50.00 TO STA 543+50.00



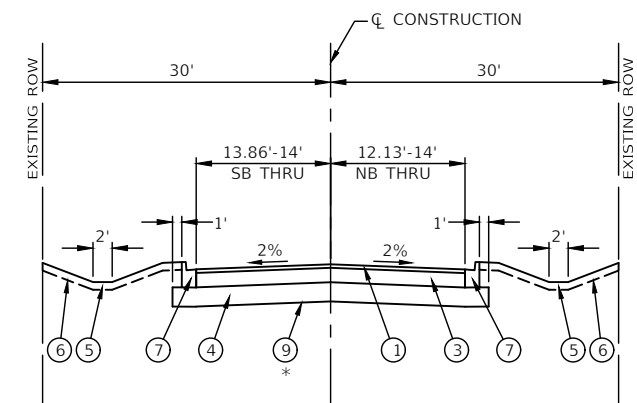
EXISTING TYPICAL SECTION
CRETE ROAD
STA 543+50.00 TO STA 544+68.77



PROPOSED TYPICAL SECTION
CRETE ROAD
STA 501+09.02 TO STA 506+50.00



PROPOSED TYPICAL SECTION
CRETE ROAD
STA 506+50.00 TO STA 543+50.00



PROPOSED TYPICAL SECTION
CRETE ROAD
STA 543+50.00 TO STA 544+68.77

EXISTING LEGEND

- (A) EXISTING PAVEMENT - SEE PROJECT GEOTECHNICAL REPORT
- (B) EXISTING AGGREGATE SUBGRADE
- (C) EXISTING AGGREGATE/TURF SHOULDER
- (D) HOT-MIX ASPHALT SURFACE REMOVAL, 3-1/2"
- (E) ROADWAY TO BE REMOVED
- [Hatched Box] ITEMS TO BE REMOVED

**REMOVAL OF EXISTING HMA ROADWAY MATERIAL SHALL BE PAID FOR AS PAVEMENT REMOVAL. ANY AGGREGATE BASE, STONE SHOULDER OR OTHER NON-HMA MATERIAL REQUIRED TO BE REMOVED SHALL BE PAID FOR AS EARTH EXCAVATION.

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-1/4"
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6"
- (4) AGGREGATE SUBGRADE IMPROVEMENT, 12"
- (5) TOPSOIL, FURNISH AND PLACE, 4"
- (6) SEEDING, CLASS 2A
- (7) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (8) CLASS D PATCH, 6"
- (9) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

*AS DIRECTED BY THE ENGINEER

NOTE:
LONGITUDINAL JOINT SEALANT SHALL BE PLACED BELOW THE SURFACE COURSE THROUGHOUT THE LENGTH OF THE PROJECT.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
PAVEMENT RECONSTRUCTION & WIDENING		
HOT MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"	4% @ 50 Gyr.	LR 1030-2
HOT MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6"	4% @ 50 Gyr.	LR 1030-2
PAVEMENT RESURFACING		
HOT MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"	4% @ 50 Gyr.	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-1/4"	4% @ 50 Gyr.	LR 1030-2
HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"		
HOT MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 4" (IN 2 LIFTS)	4% @ 50 Gyr.	LR 1030-2
PAVEMENT PATCHING		
CLASS D PATCH (HMA BINDER IL-19.0), 6"	4% @ 70 Gyr.	LR 1030-2
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA) PER LR 1030-2		

NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE IS 112 LBS/SQ/IN.
- ALL PATCHING OPERATIONS SHALL TAKE PLACE AFTER SURFACE MILLING HAS BEEN COMPLETED.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

FILE NAME = 21R0316-TYPX-01 - P01

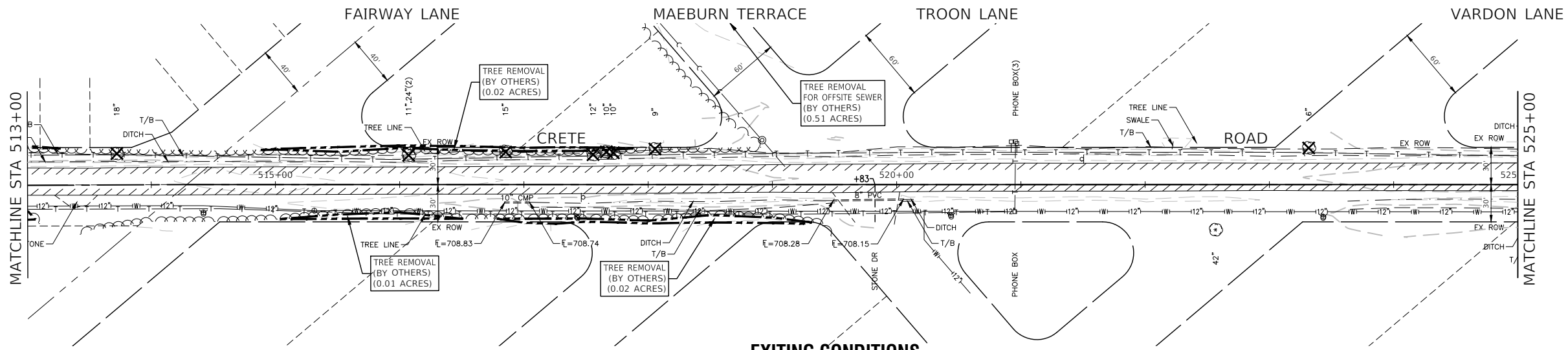
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

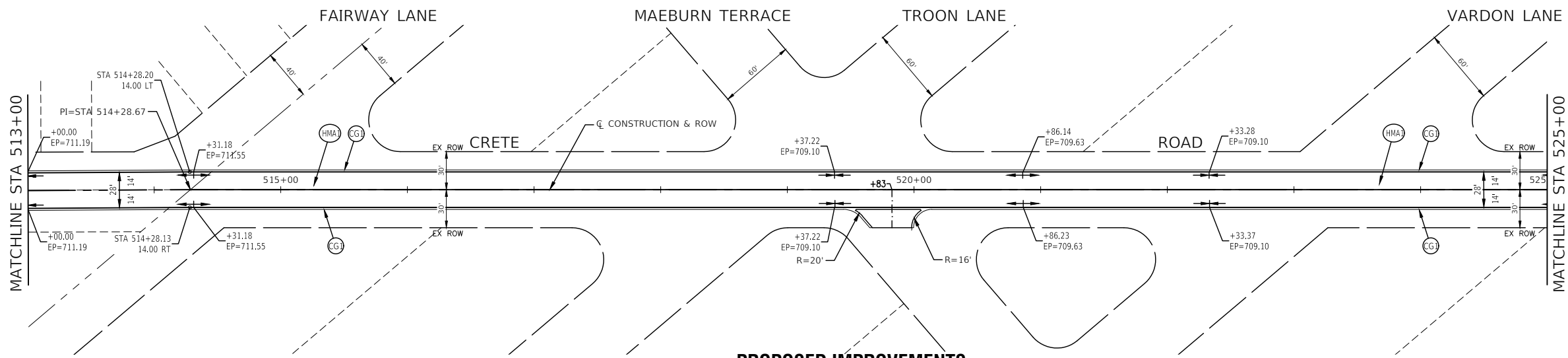
CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
TYPICAL CROSS SECTIONS

SCALE: 1"=10' SHEET NO. 11 OF 52 SHEETS STA. TO STA.

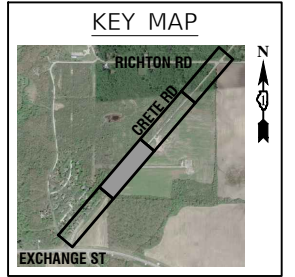
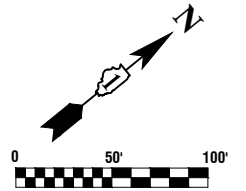
F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 11
CONTRACT NO. 61H99			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)	



EXISTING CONDITIONS

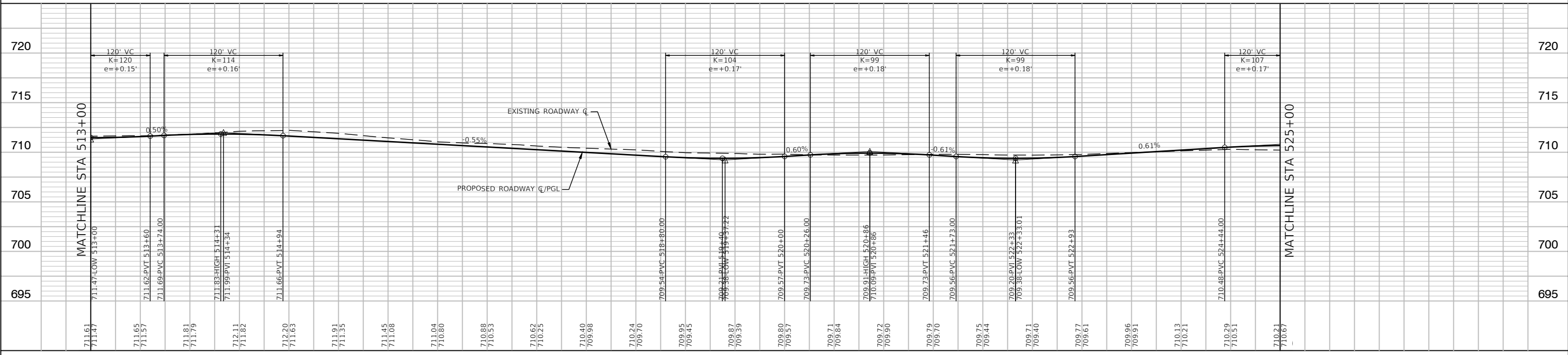


PROPOSED IMPROVEMENTS

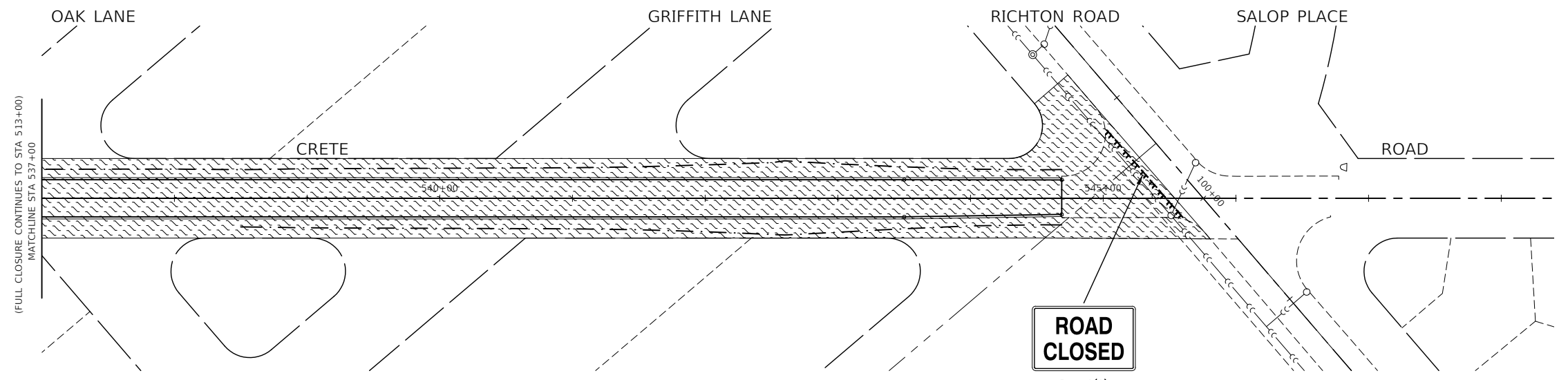
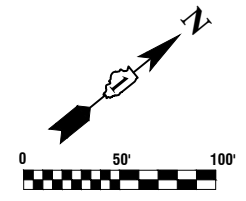
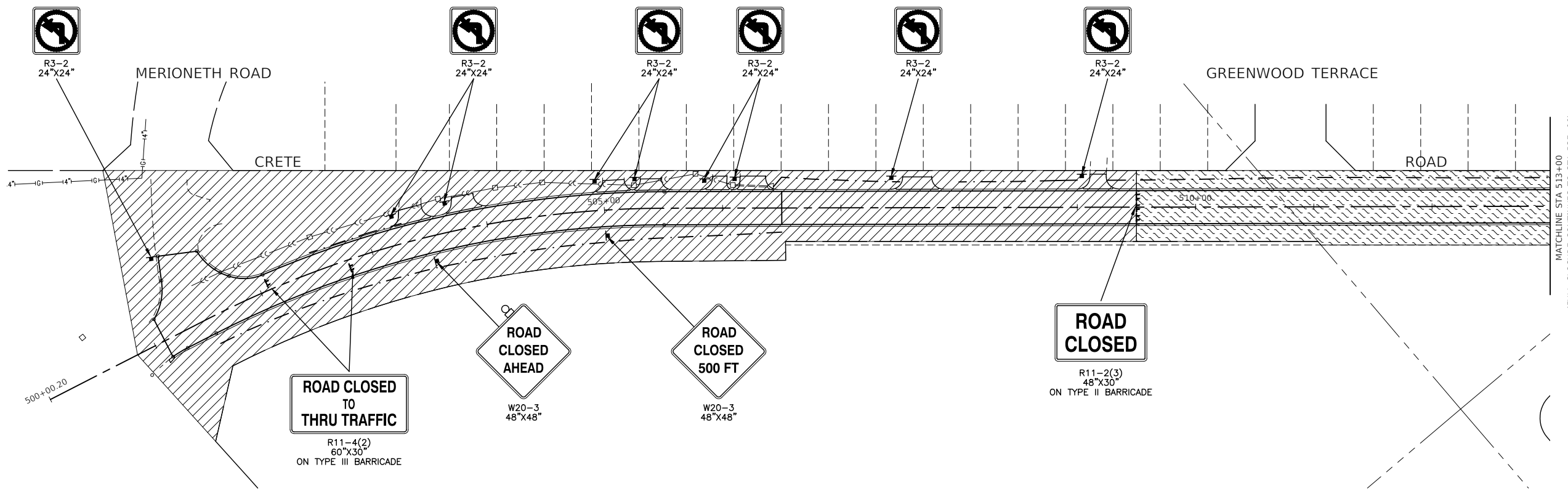


LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 3-1/2"
 - PAVEMENT REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - COMBINATION CURB AND GUTTER REMOVAL
 - HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT
 - TREE TRUNK PROTECTION AND TREE ROOT PRUNING
 - INDIVIDUAL TREE REMOVAL (BY OTHERS)
 - TREE REMOVAL LIMITS (BY OTHERS)
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4"
 - PAVEMENT RECONSTRUCTION & WIDENING
-HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 6"
-AGGREGATE SUBGRADE IMPROVEMENT, 12"
 - RESURFACING
-HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50, 1-1/2"
-HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2-1/4"
- NOTE:
THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.



FILE NAME = 21R0316-PLPR-01 - IDOT PLPR02	USER NAME =	DESIGNED - JPH	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION PLAN & PROFILE			F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 13
	PLOT SCALE =	CHECKED - WPD	REVISD -		SCALE: STA. TO STA.			CONTRACT NO. 61H99				
	PLOT DATE = 11-16-22	DRAWN - RG	REVISD -		FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT UMBW(332)				
		CHECKED - AG	REVISD -									



LEGEND

RESIDENTIAL ACCESS WORK AREA

FULL CLOSURE WORK AREA

- NOTES:**
- AGGREGATE FOR TEMPORARY ACCESS SHALL BE USED TO MAINTAIN ACCESS TO ALL AFFECTED DRIVEWAYS AT THE DIRECTION OF THE ENGINEER.
 - THE ROADWAY WILL BE RECONSTRUCTED EAST OF STA. 506+50. THE CONTRACTOR SHALL ARRANGE THEIR OPERATIONS AND GRADING ON A DAILY BASIS TO ENSURE PASSENGER VEHICLE ACCESS IS POSSIBLE TO THE TWO RESIDENCES EAST OF THIS STATION AT ALL TIMES THROUGHOUT CONSTRUCTION. ANY DEVIATIONS FROM THIS WILL NOT BE ALLOWED WITHOUT 7 DAYS OF ADVANCED NOTICE AND COORDINATION WITH THE ENGINEER AND THE RESIDENTS AND WITHOUT WRITTEN APPROVAL FROM THE ENGINEER.
 - THE RECONSTRUCTED DRIVEWAY AT STA. 519+75 SERVICES VILLAGE OWNED PROPERTY TO THE EAST. SEE PLAN AND PROFILE SHEETS. THE CONTRACTOR SHALL MAINTAIN THIS OFFSITE ACCESS AT ALL TIMES TO A LEVEL ACCESSIBLE BY PUBLIC WORKS VEHICLES.

FILE NAME = 21R0316-MOT-01 - P01

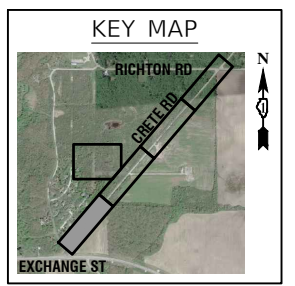
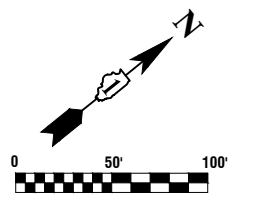
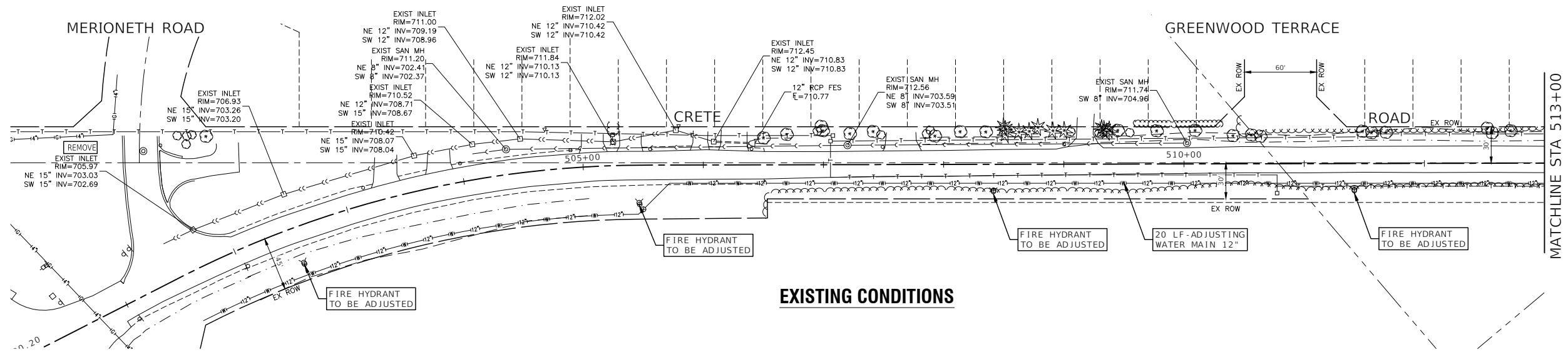
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
SUGGESTED CONSTRUCTION STAGING

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	17
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

SCALE: 1"=50' SHEET NO. 17 OF 52 SHEETS STA. TO STA.



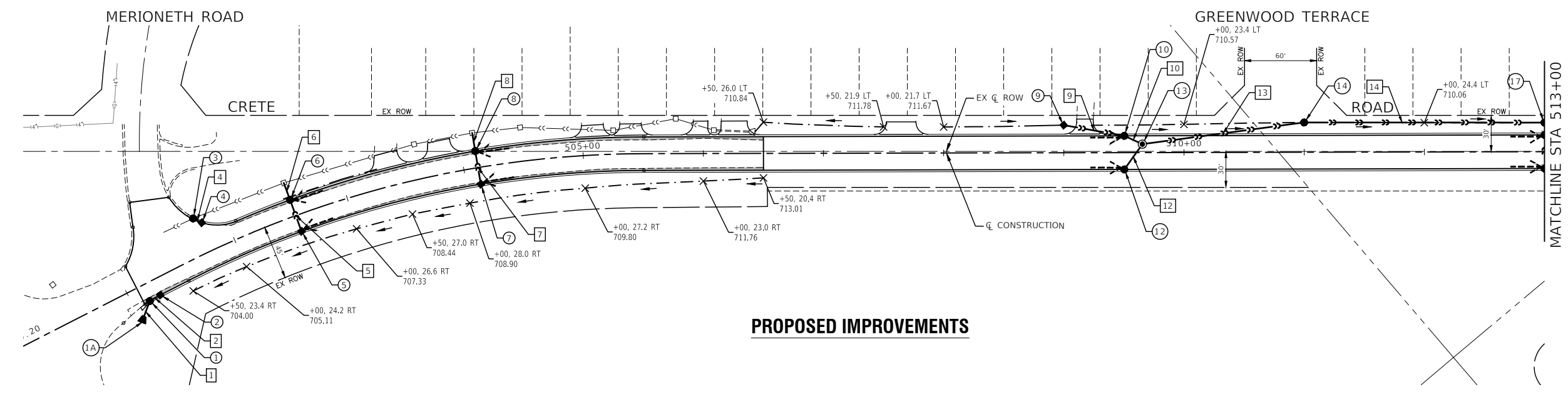
EXISTING CONDITIONS

LEGEND

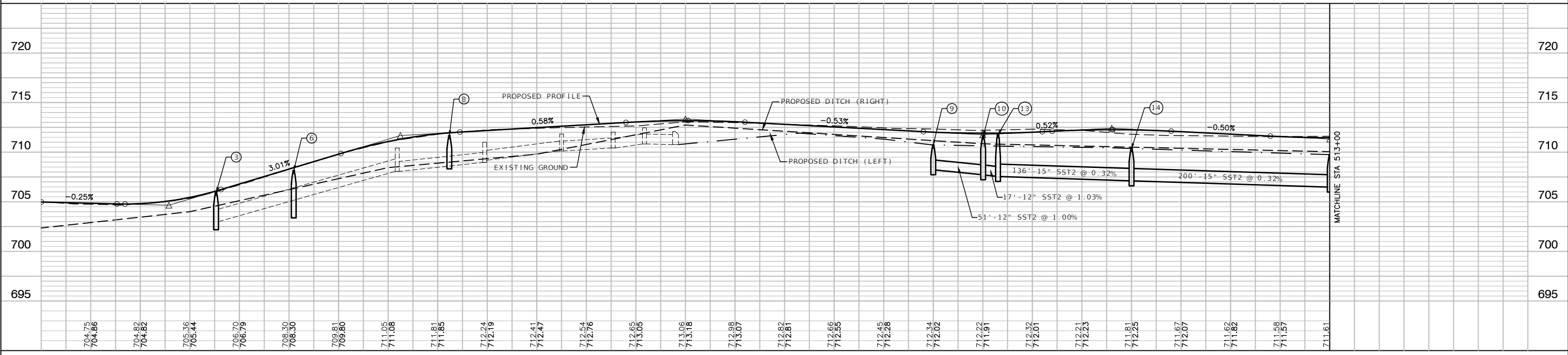
- PROPOSED DITCH
- PROPOSED STORM SEWER
- PIPE UNDERDRAINS, TYPE 2, 4" 25 FT INSTALLED AS INDICATED (TYP)
- PROPOSED DRAINAGE SUMMIT
- PROPOSED DITCH FLOW DIRECTION
- PROPOSED CATCH BASIN
- PROPOSED INLET
- PROPOSED MANHOLE

NOTES

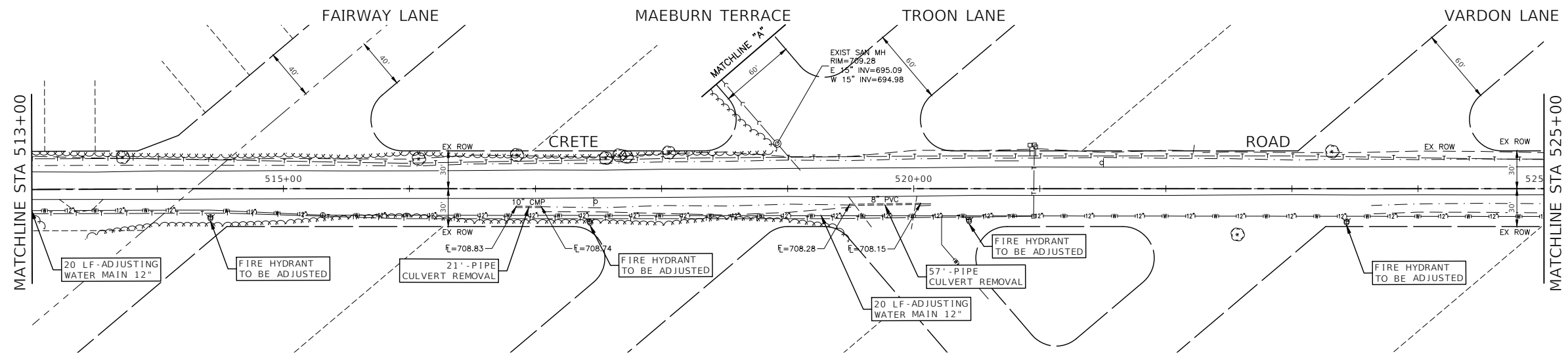
1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
2. ALL STORM SEWERS SHALL HAVE WATERMAIN QUALITY JOINTS AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS.
3. THE CONNECTION OF PROPOSED STORM SEWER TO EXISTING STRUCTURES WILL BE INCLUDED IN THE COST OF THE STORM SEWER.



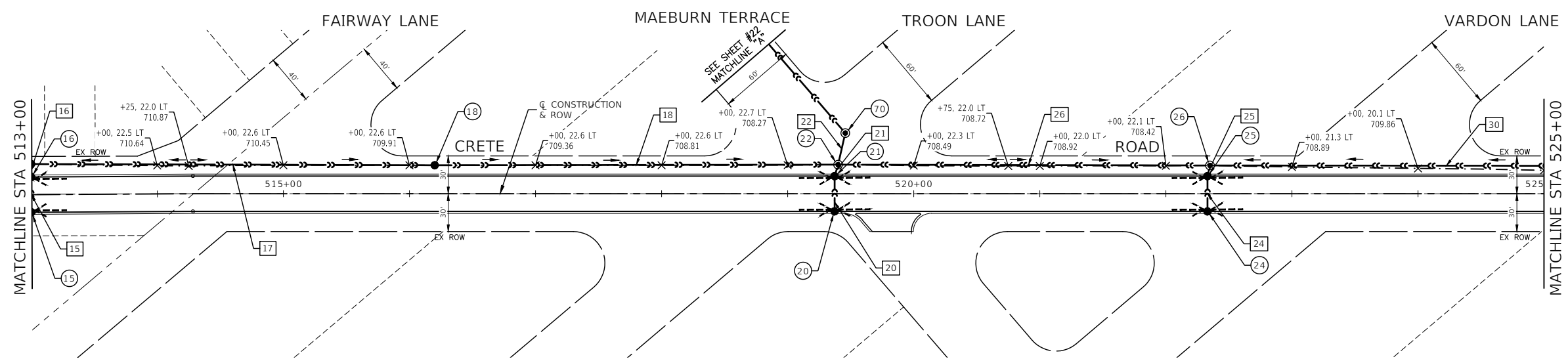
PROPOSED IMPROVEMENTS



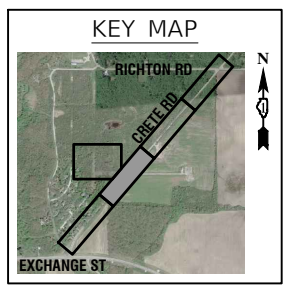
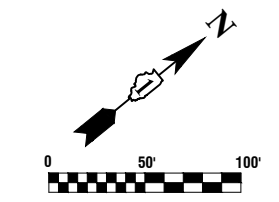
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	PLOT SCALE =	DRAWN -- RG	REVISED --		SCALE: H 1"=50' V 1"=5'	SHEET NO. 18 OF 52 SHEETS	STA. TO STA.	CONTRACT NO. 61H99			
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)						



EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

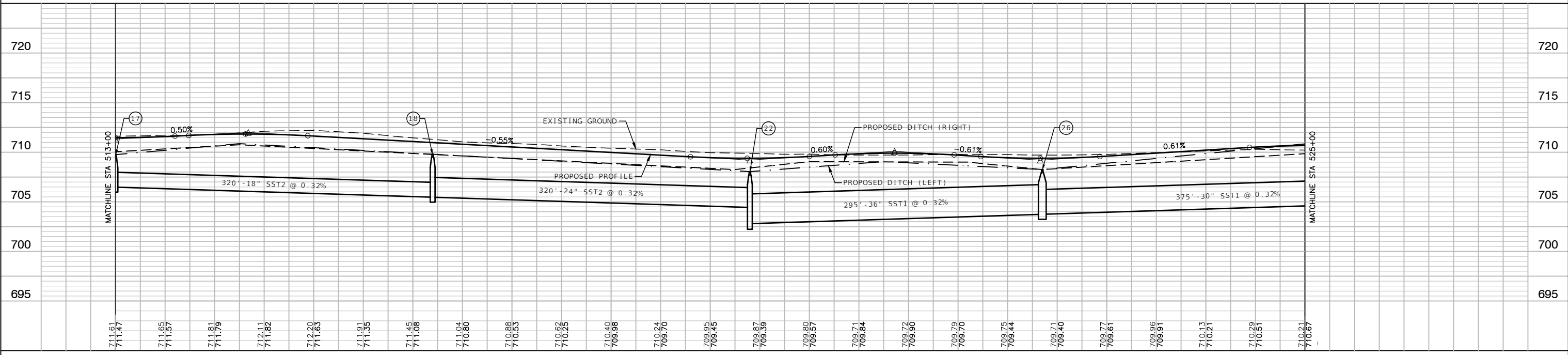


LEGEND

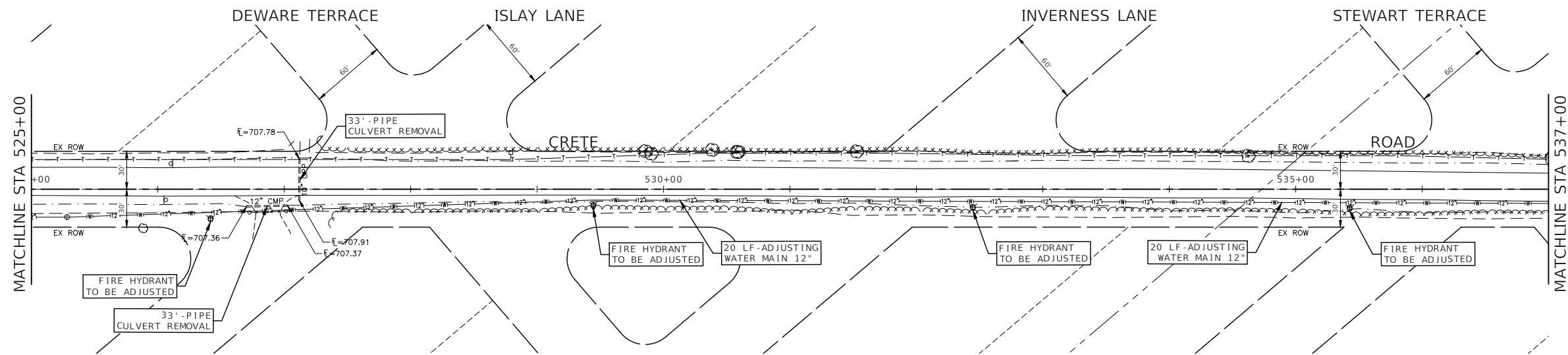
- PROPOSED DITCH
- PROPOSED STORM SEWER
- PIPE UNDERDRAINS, TYPE 2, 4" 25 FT INSTALLED AS INDICATED (TYP)
- PROPOSED DRAINAGE SUMMIT
- PROPOSED DITCH FLOW DIRECTION
- PROPOSED CATCH BASIN
- PROPOSED INLET
- PROPOSED MANHOLE

NOTES

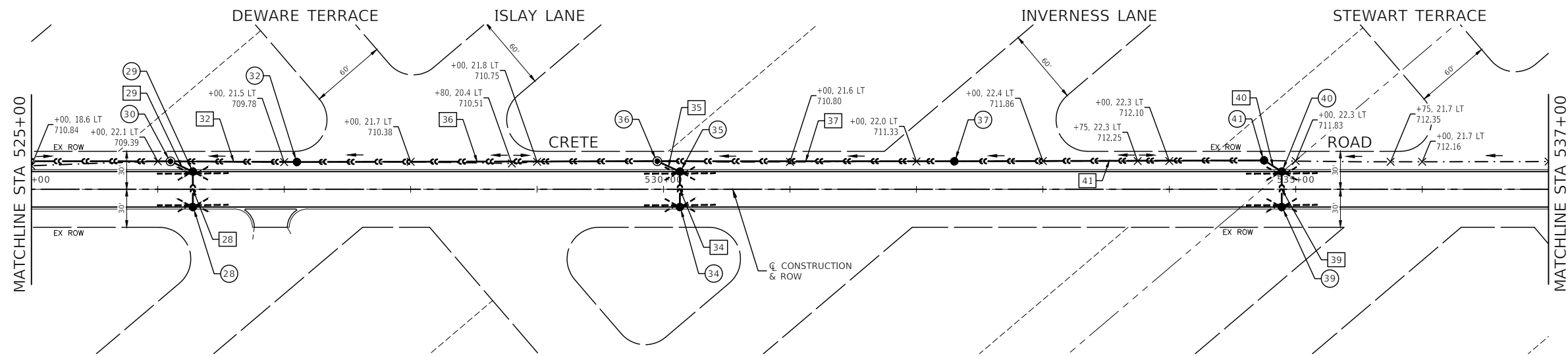
1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
2. ALL STORM SEWERS SHALL HAVE WATERMAIN QUALITY JOINTS AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS.
3. THE CONNECTION OF PROPOSED STORM SEWER TO EXISTING STRUCTURES WILL BE INCLUDED IN THE COST OF THE STORM SEWER.



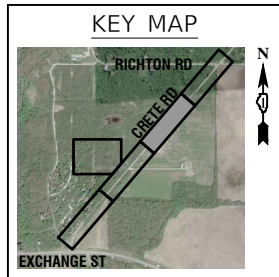
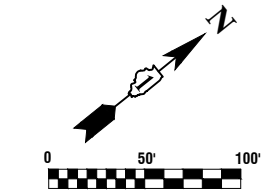
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	PLOT SCALE =	CHECKED -- WPD	REVISED --				CONTRACT NO. 61H99					
	PLOT DATE = 11-16-22	DRAWN -- RG	REVISED --				SCALE: H 1"=50' V 1"=5'		SHEET NO. 19 OF 52 SHEETS		STA. TO STA.	
		CHECKED -- AG	REVISED --				FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT UMBW(332)	



EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

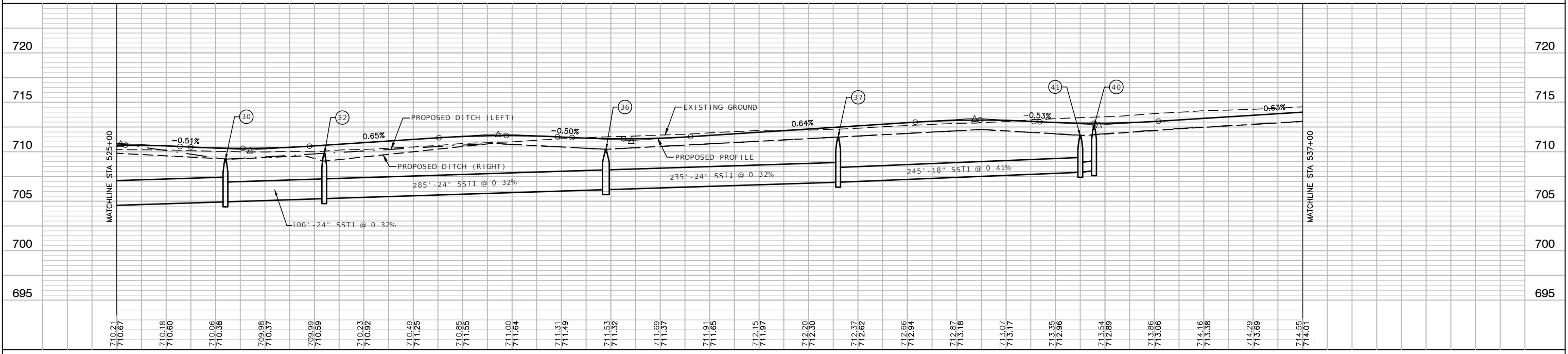


LEGEND

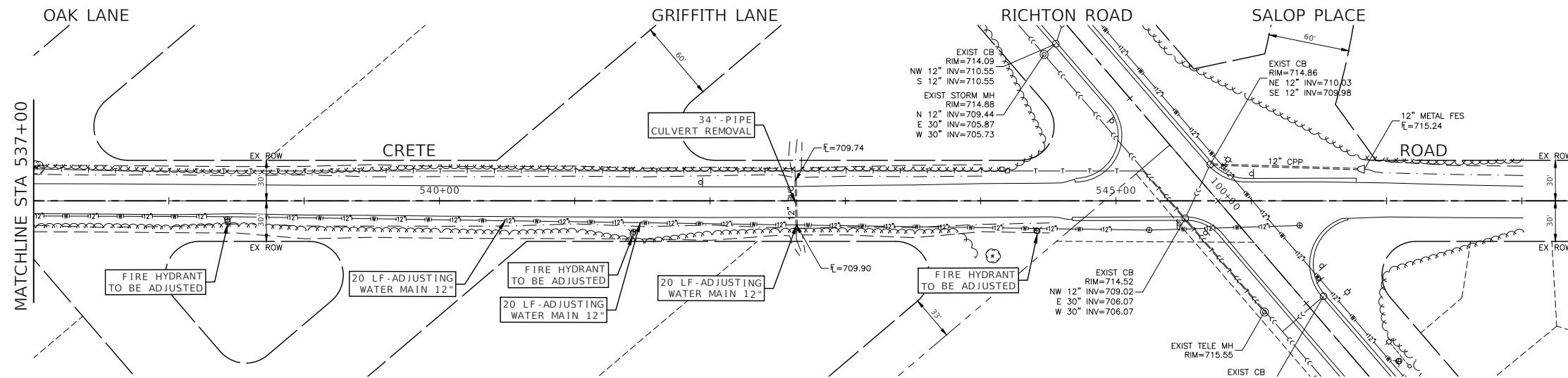
- PROPOSED DITCH
- PROPOSED STORM SEWER
- PIPE UNDERDRAINS, TYPE 2, 4" 25 FT INSTALLED AS INDICATED (TYP)
- PROPOSED DRAINAGE SUMMIT
- PROPOSED DITCH FLOW DIRECTION
- PROPOSED CATCH BASIN
- PROPOSED INLET
- PROPOSED MANHOLE

NOTES

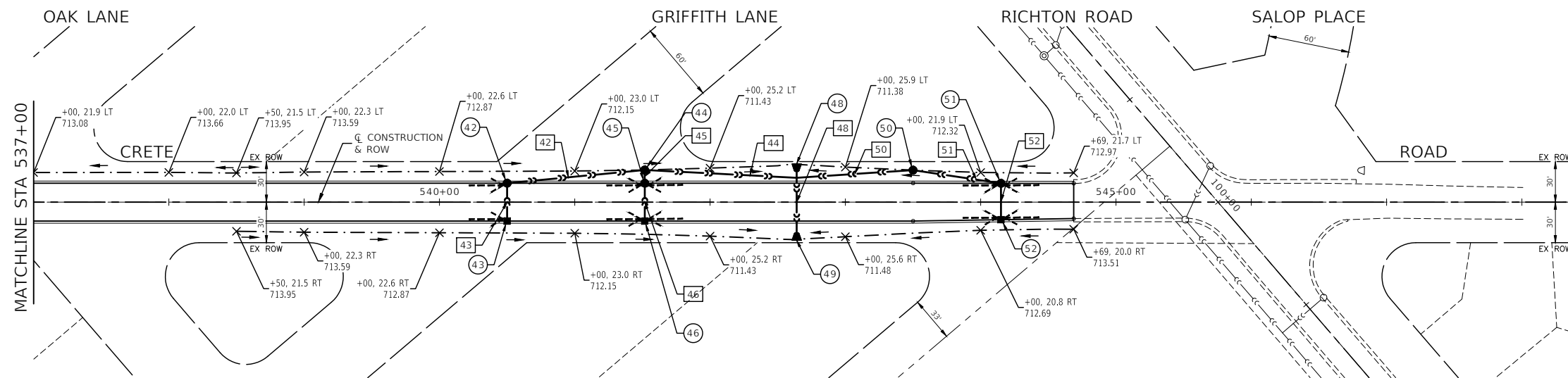
1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
2. ALL STORM SEWERS SHALL HAVE WATERMAIN QUALITY JOINTS AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS.
3. THE CONNECTION OF PROPOSED STORM SEWER TO EXISTING STRUCTURES WILL BE INCLUDED IN THE COST OF THE STORM SEWER.



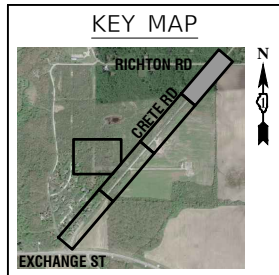
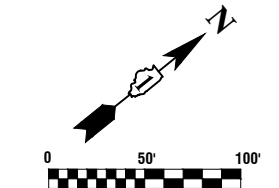
FILE NAME = 21R0316-STRM-01 - STRM03	USER NAME =	DESIGNED -- JPH	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION DRAINAGE & UTILITIES		F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 20	
	PLOT SCALE =	CHECKED -- WPD	REVISED --		SCALE: H 1"=50' V 1"=5'	SHEET NO. 20	OF 52 SHEETS	STA. TO STA.	CONTRACT NO. 61H99			
	PLOT DATE = 11-16-22	DRAWN -- RG	REVISED --		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)							
		CHECKED -- AG	REVISED --									



EXISTING CONDITIONS



PROPOSED IMPROVEMENTS

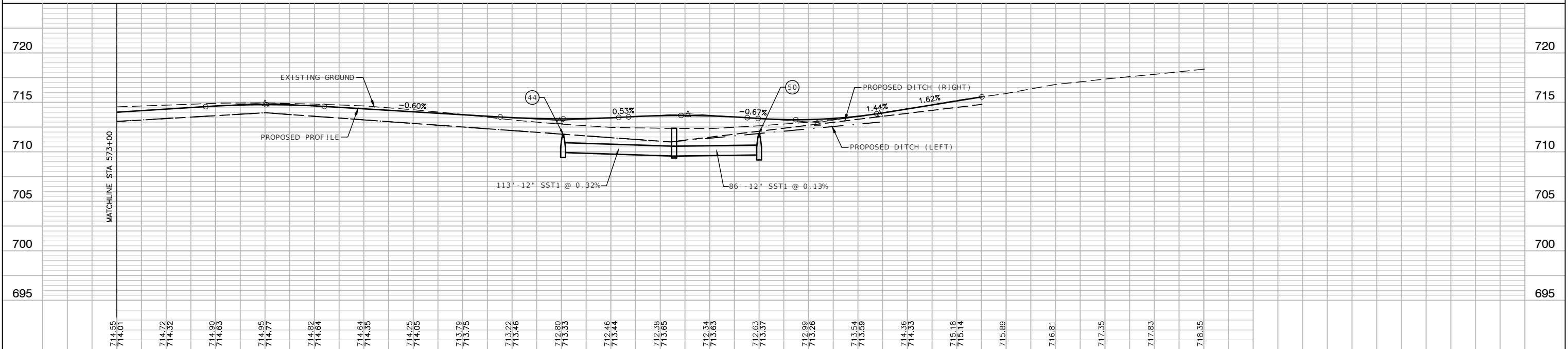


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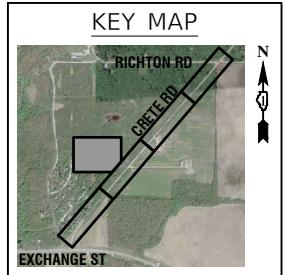
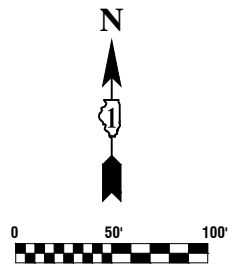
- PROPOSED DITCH
- PROPOSED STORM SEWER
- PIPE UNDERDRAINS, TYPE 2, 4" 25 FT INSTALLED AS INDICATED (TYP)
- PROPOSED DRAINAGE SUMMIT
- PROPOSED DITCH FLOW DIRECTION
- PROPOSED CATCH BASIN
- PROPOSED INLET
- PROPOSED MANHOLE

NOTES

1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
2. ALL STORM SEWERS SHALL HAVE WATERMAIN QUALITY JOINTS AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS.
3. THE CONNECTION OF PROPOSED STORM SEWER TO EXISTING STRUCTURES WILL BE INCLUDED IN THE COST OF THE STORM SEWER.



FILE NAME = 21R0316-STRM-01 - STRM04	USER NAME =	DESIGNED -- JPH	REVISED --	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION DRAINAGE & UTILITIES		F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 21	
	PLOT SCALE =	DRAWN -- RG	REVISED --		SCALE: H 1"=50' V 1"=5'		SHEET NO. 21	OF 52 SHEETS	STA.	TO STA.	CONTRACT NO. 61H99	
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --		FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT UMBW(332)				

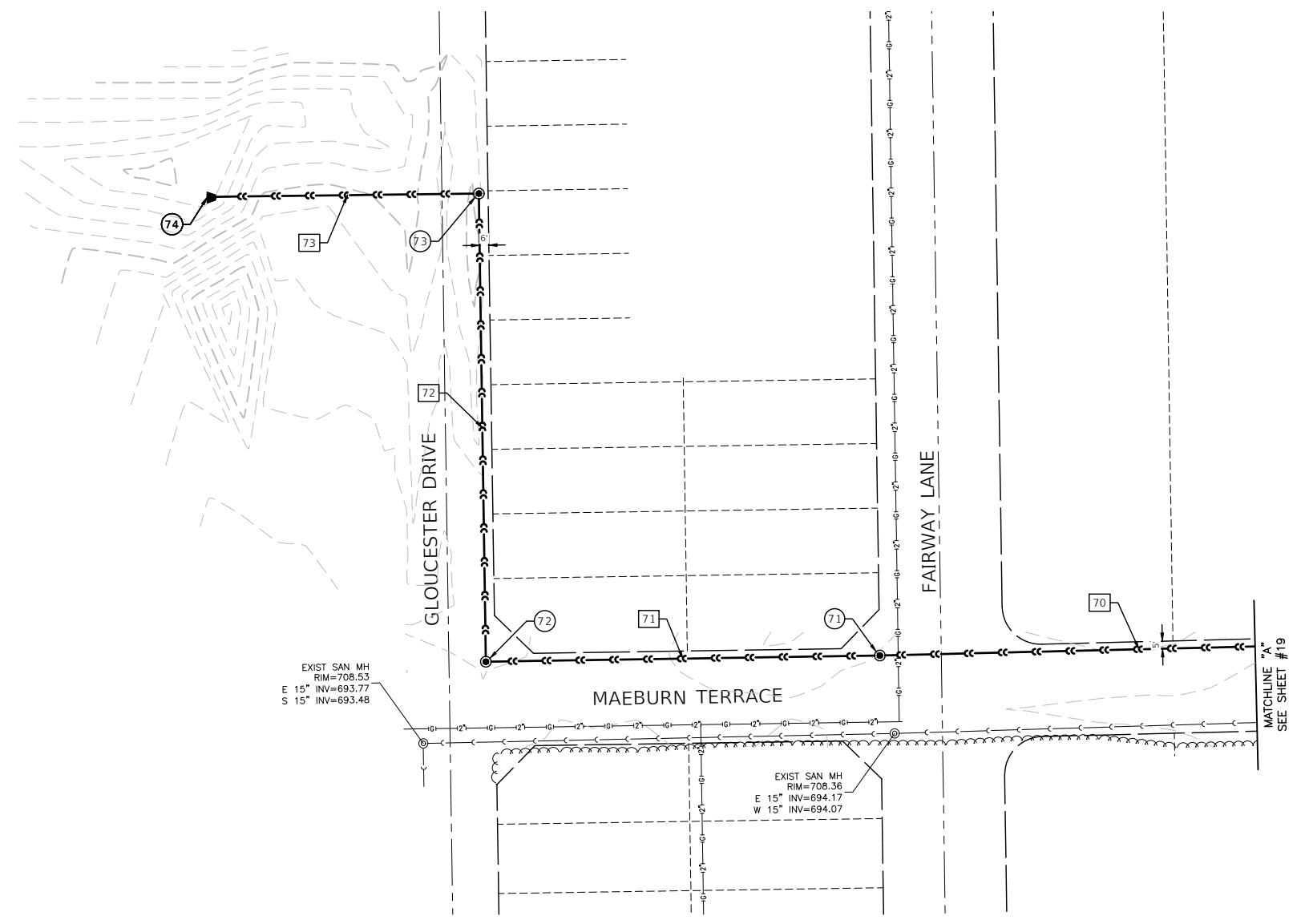


LEGEND

- - - - - PROPOSED DITCH
- PROPOSED STORM SEWER
- > PIPE UNDERDRAINS, TYPE 2, 4" 25 FT INSTALLED AS INDICATED (TYP)
- +--- PROPOSED DRAINAGE SUMMIT
- > PROPOSED DITCH FLOW DIRECTION
- PROPOSED CATCH BASIN
- PROPOSED INLET
- PROPOSED MANHOLE

NOTES

1. ALL OFFSETS AND ELEVATIONS TO CURB LINE STRUCTURES ARE GIVEN TO THE EDGE OF PAVEMENT. OFFSETS AND ELEVATIONS FOR FLARED END SECTIONS ARE GIVEN AT THE FLARED END. ALL OTHER OFFSETS AND ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE.
2. ALL STORM SEWERS SHALL HAVE WATERMAIN QUALITY JOINTS AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWERS.
3. THE CONNECTION OF PROPOSED STORM SEWER TO EXISTING STRUCTURES WILL BE INCLUDED IN THE COST OF THE STORM SEWER.



FILE NAME = 21R0316-STRM-01 - STRM05	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION			
CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION DRAINAGE & UTILITIES			
SCALE: H 1"=50' V 1"=5'		SHEET NO. 22 OF 52 SHEETS	STA. TO STA.
F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52 SHEET NO. 22
CONTRACT NO. 61H99			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)			

1
STA=501+14.00, 14.45' RT
CB TYP C T11G
RIM EL=705.09
12" SE INV=702.59
12" N INV=702.59

1A
STA=500+99.92, 27.28' RT
12" FES
12" NW INV=702.40

2
STA=501+24.00, 14.00' RT
INLET TYP A T11G
RIM EL=705.19
12" S INV=702.69

3
STA=501+76.45, 30.80' LT
CB TYP A 5' T11G
RIM EL=705.97
EX 15" S INV=702.69
PR 12" NE INV=703.07
EX 15" N INV=703.03

4
STA=501+81.23, 24.50' LT
INLET TYP A T11G
RIM EL=706.15
12" SW INV=703.15

5
STA=502+54.78, 14.00' RT
INLET TYP A T11G
RIM EL=708.16
12" W INV=704.16

6
STA=502+54.78, 14.00' LT
CB TYP C T11G
RIM EL=708.16
12" W INV=703.88
12" E INV=703.88

7
STA=504+11.83, 14.00' RT
INLET TYP A T11G
RIM EL=711.67
12" NW INV=709.10

8
STA=504+11.83, 14.00' LT
CB TYP C T11G
RIM EL=711.67
12" NW INV=708.82
12" SE INV=708.82

9
STA=509+00.00, 23.00' LT
INLET TYP A T8G
RIM EL=710.73
12" NE INV=708.23

10
STA=509+50.32, 14.00' LT
CB TYP C T11G
RIM EL=711.63
12" NE INV=707.72
12" SW INV=707.72

12
STA=509+50.32, 14.00' RT
CB TYP C T11G
RIM EL=711.63
12" N INV=707.81

13
STA=509+65.32, 7.00' LT
MH TYP A 5' T1FCL
RIM EL=711.78
15" NE INV=707.55
12" SW INV=707.55
12" S INV=707.55

14
STA=511+00.00, 24.80' LT
CB TA 5' T8G
RIM EL=710.37
15" NE INV=707.11
15" SW INV=707.11

15
STA=512+99.97, 14.00' RT
CB TYP C T11G
RIM EL=711.19
12" NW INV=706.96

16
STA=513+00.00, 14.00' LT
CB TYP C T11G
RIM EL=711.19
12" NW INV=706.68
12" SE INV=706.68

17
STA=513+00.00, 24.28' LT
CB TYP A 5' T8G
RIM EL=709.75
18" NE INV=706.48
15" SW INV=706.48
12" SE INV=706.58

18
STA=516+20.00, 22.60' LT
CB TYP A 5' T8G
RIM EL=709.80
24" NE INV=705.46
18" SW INV=705.46

20
STA=519+37.22, 14.00' RT
CB TYP C T11G
RIM EL=709.10
15" NW INV=705.10

21
STA=519+37.22, 14.00' LT
CB TYP A 4' T11G
RIM EL=709.10
18" NW INV=704.82
15" SE INV=704.82

22
STA=519+40.00, 23.07' LT
MH TYP A 8' T8G
RIM EL=708.04
36" NW INV=702.73
24" SW INV=704.43
18" SE INV=704.73
36" NE INV=702.80

24
STA=522+33.01, 14.00' RT
CB TYP A 4' T11G
RIM EL=709.10
12" NW INV=704.66

25
STA=522+33.01, 14.00' LT
CB TYP A 4' T11G
RIM EL=709.10
12" NW INV=704.38
12" SE INV=704.38

26
STA=522+35.00, 22.43' LT
MH TYP A 7' T8G
RIM EL=708.25
36" SW INV=703.74
30" NE INV=703.74
12" SE INV=704.29

28
STA=526+27.69, 14.00' RT
CB TYP C T11G
RIM EL=710.06
12" NW INV=705.60

29
STA=526+27.69, 14.00' LT
CB TYP C T11G
RIM EL=710.06
12" SW INV=705.32
12" SE INV=705.32

30
STA=526+10.00, 22.08' LT
MH TYP A 7' T8G
RIM EL=709.25
30" SW INV=704.94
24" NE INV=704.94
12" NE INV=705.13

32
STA=527+10.00, 21.57' LT
CB TYP A 5' T8G
RIM EL=709.84
24" SW INV=705.26
24" NE INV=705.26

34
STA=530+12.91, 14.00' RT
CB TYP C T11G
RIM EL=711.03
12" NW INV=706.80

35
STA=530+12.91, 14.00' LT
CB TYP C T11G
RIM EL=711.03
12" SW INV=706.52
12" SE INV=706.52

36
STA=529+95.00, 22.21' LT
MH TYP A 6' T8G
RIM EL=710.25
24" SW INV=706.17
24" NE INV=706.17
12" NE INV=706.32

37
STA=532+30.00, 22.12' LT
CB TYP A 5' T8G
RIM EL=711.48
24" SW INV=706.92
18" NE INV=706.92

39
STA=534+88.96, 14.00' RT
CB TYP C T11G
RIM EL=712.60
12" NW INV=708.37

40
STA=534+88.96, 14.00' LT
CB TYP C T11G
RIM EL=712.60
12" W INV=708.09
12" SE INV=708.09

41
STA=534+75.00, 22.83' LT
CB TYP A 4' T11G
RIM EL=711.65
18" SW INV=707.92
12" E INV=707.92

42
STA=540+50.00, 14.00' LT
CB TYP A 4' T11G
RIM EL=713.47
12" NE INV=710.26
12" SE INV=710.26

43
STA=540+50.00, 14.00' RT
INLET TYP A T11G
RIM EL=713.47
12" NW INV=710.47

44
STA=541+51.61, 23.77' LT
CB TYP A 5' T8G
RIM EL=711.78
12" NE INV=709.93
12" SW INV=709.93
12" SE INV=709.93

45
STA=541+51.71, 14.00' LT
CB TYP C T11G
RIM EL=713.05
12" NW INV=709.96
12" SE INV=709.96

46
STA=541+51.71, 14.00' RT
INLET TYP A T11G
RIM EL=713.05
12" NW INV=710.05

48
STA=542+64.00, 28.25' LT
3'x2' PCC BOX CULVERT END SECTION
36" $\bar{\ell}$ EL=709.59

49
STA=542+64.00, 28.25' RT
3'x2' PCC BOX CULVERT END SECTION
36" $\bar{\ell}$ EL=709.35

50
STA=543+50.00, 23.67' LT
CB TYP C T8G
RIM EL=711.85
12" SW INV=709.68
12" NE INV=709.68

51
STA=544+15.02, 13.92' LT
CB TYP A 4' T11G
RIM EL=713.03
12" SW INV=709.76
12" SE INV=709.76

52
STA=544+14.98, 12.99' RT
INLET TYP A T11G
RIM EL=713.03
12" NW INV=710.03

70
STA=519+45.70, 48.13' LT
MH TYP A 6' T1FCL
RIM EL=709.58
36" W INV=702.62
36" SE INV=702.62

71
STA=517+25.84, 303.49' LT
MH TYP A 6' T1FCL
RIM EL=708.09
36" W INV=701.56
36" E INV=701.56

72
STA=515+60.41, 498.94' LT
MH TYP A 6' T1FCL
RIM EL=707.79
36" N INV=700.74
36" E INV=700.74

73
STA=517+92.79, 695.38' LT
MH TYP A 6' T1FCL
RIM EL=705.62
36" W INV=699.77
36" S INV=699.77

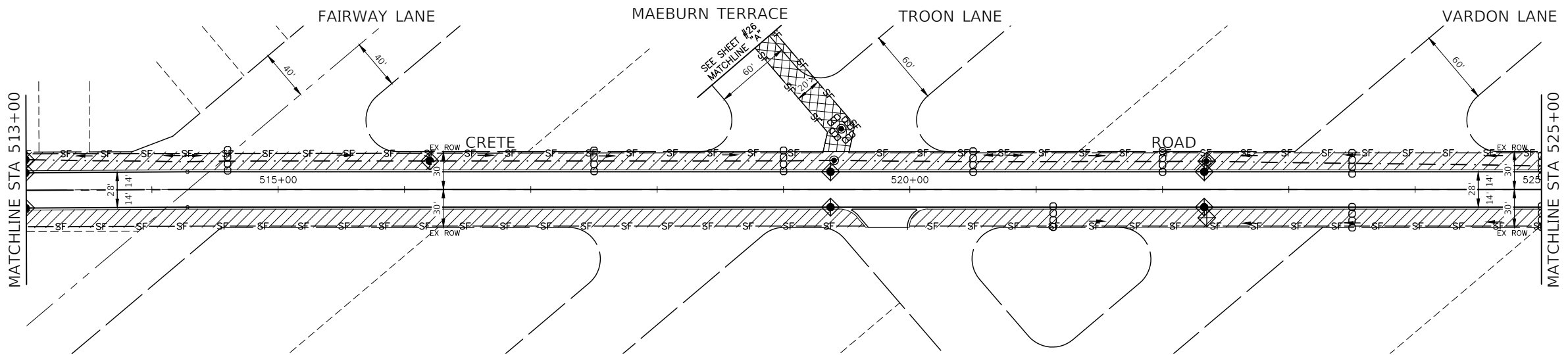
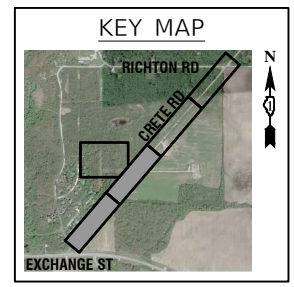
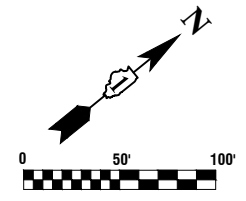
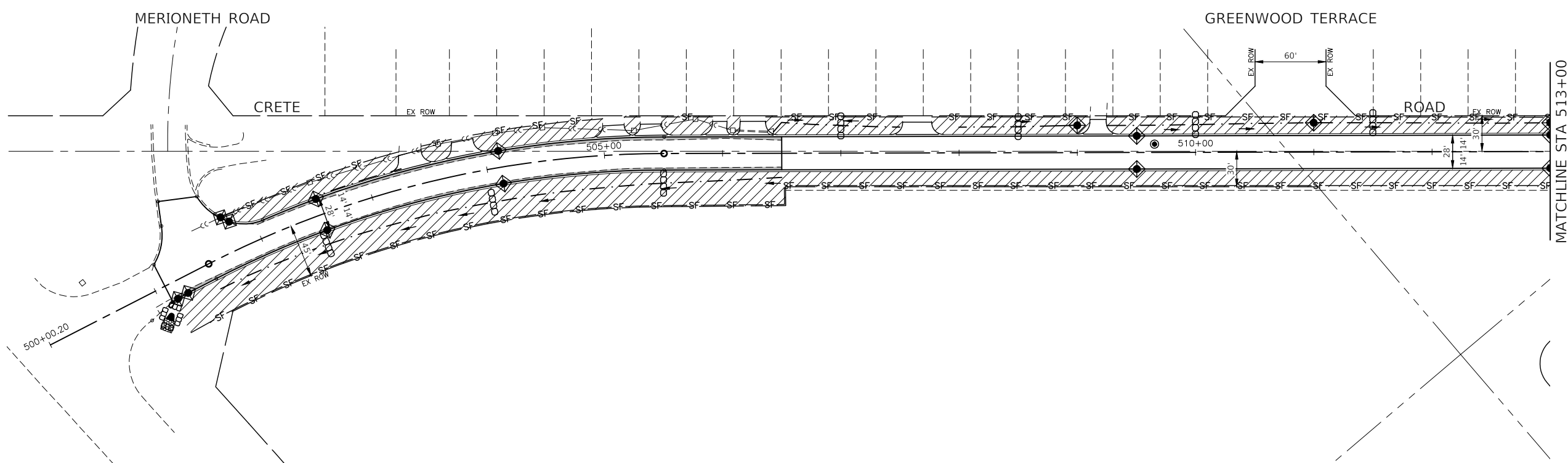
74
STA=516+78.84, 830.29' LT
36" FES
36" E INV=699.22

1	19'-12" RCCP, T1 @ 1.00% (0.1)	44	113'-12" RCCP, T1 @ 0.32% (15.0)
2	10'-12" RCCP, T1 @ 1.00% (0.1)	45	10'-12" RCCP, T1 @ 0.31% (3.0)
4	8'-12" RCCP, T1 @ 1.00% (0.5)	46	28'-12" RCCP, T1 @ 0.32% (3.0)
5	28'-12" RCCP, T1 @ 1.00% (5.2)	48	57'-3"x2' PCC BOX CULVERT @ 0.42% (4.2)
6	15'-12" RCCP, T1 @ 1.00% (3.0)	50	86'-12" RCCP, T1 @ 0.13% (15.0)
7	28'-12" RCCP, T1 @ 1.00% (1.0)	51	66'-12" RCCP, T1 @ 0.12% (10.0)
8	15'-12" RCCP, T1 @ 0.98% (0.8)	52	27'-12" RCCP, T1 @ 1.00% (1.3)
9	51'-12" RCCP, T1 @ 1.00% (4.5)	70	337'-36" RCCP, T2 @ 0.31%
10	17'-12" RCCP, T1 @ 1.03% (3.0)	71	256'-36" RCCP, T2 @ 0.32%
12	26'-12" RCCP, T1 @ 1.01% (8.6)	72	304'-36" RCCP, T2 @ 0.32%
13	136'-15" RCCP, T1 @ 0.32% (18.3)	73	171'-36" RCCP, T2 @ 0.32%
14	200'-15" RCCP, T1 @ 0.32% (27.0)		
15	28'-12" RCCP, T1 @ 1.00% (5.9)		
16	10'-12" RCCP, T1 @ 0.97% (1.5)		
17	320'-18" RCCP, T1 @ 0.32% (40.8)		
18	320'-24" RCCP, T1 @ 0.32% (34.9)		
20	28'-15" RCCP, T1 @ 1.00% (5.0)		
21	9'-18" RCCP, T1 @ 0.95% (1.1)		
22	26'-36" RCCP, T2 @ 0.43% (14.2)		
24	28'-12" RCCP, T1 @ 1.00% (6.5)		
25	9'-12" RCCP, T1 @ 1.04% (1.9)		
26	295'-36" RCCP, T2 @ 0.32% (41.1)		
28	28'-12" RCCP, T1 @ 1.00% (6.5)		
29	19'-12" RCCP, T1 @ 0.98% (4.1)		
30	375'-30" RCCP, T1 @ 0.32% (45.9)		
32	100'-24" RCCP, T1 @ 0.32% (17.9)		
34	28'-12" RCCP, T1 @ 1.00% (5.9)		
35	20'-12" RCCP, T1 @ 1.02% (3.9)		
36	285'-24" RCCP, T1 @ 0.32% (46.1)		
37	235'-24" RCCP, T1 @ 0.32% (37.6)		
39	28'-12" RCCP, T1 @ 1.00% (5.9)		
40	17'-12" RCCP, T1 @ 1.03% (3.1)		
41	245'-18" RCCP, T1 @ 0.41% (41.8)		
42	102'-12" RCCP, T1 @ 0.32% (14.0)		
43	28'-12" RCCP, T1 @ 0.75% (3.0)		

FILE NAME = 21R0316-STRM-01 - TABLES	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION STORM SEWER STRUCTURE AND PIPE SCHEDULES		F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 23
SCALE: H 1"=50' V 1"=5'		SHEET NO. 23 OF 52 SHEETS		STA. TO STA.	CONTRACT NO. 61H99	
		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT UMBW(332)		



KEY	ITEM NAME
— SF —	PERIMETER EROSION BARRIER TO BE PLACED 1' FROM R.O.W.
◆	INLET FILTER
⊞	25 FT PERIMETER EROSION BARRIER
▨	SEEDING CLASS 2A WITH EROSION CONTROL BLANKET
▩	SEEDING CLASS 4 WITH EROSION CONTROL BLANKET
⊞	TEMPORARY DITCH CHECKS
⊞	RIP RAP (STONE RIP RAP CL. A, GRADATION RR4, FILTER FABRIC)

FILE NAME = 21R0316-LNCS-01 - P01

USER NAME =	DESIGNED — JPH	REVISED —
	CHECKED — WPD	REVISED —
PLOT SCALE =	DRAWN — RG	REVISED —
PLOT DATE = 11-16-22	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

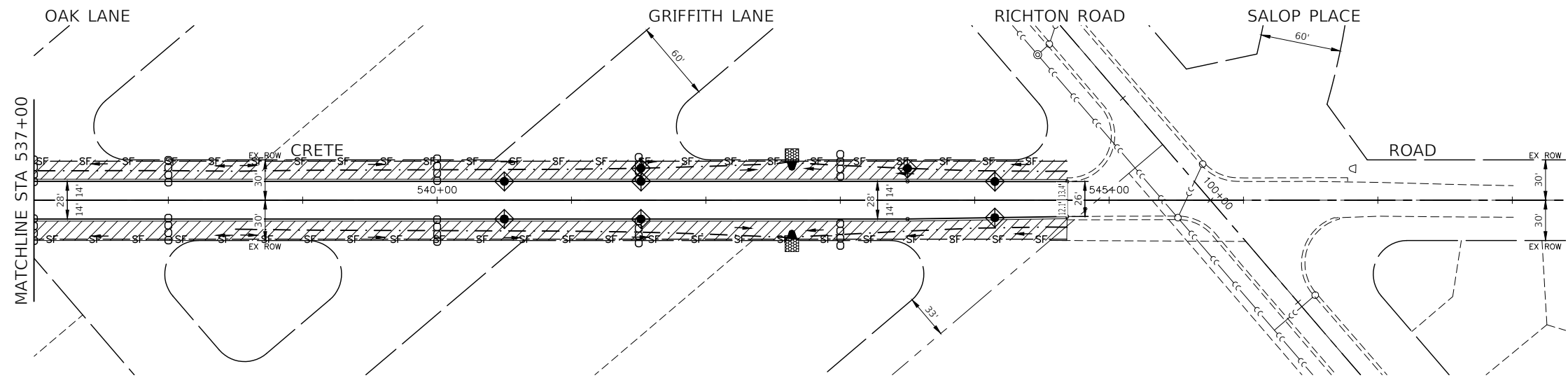
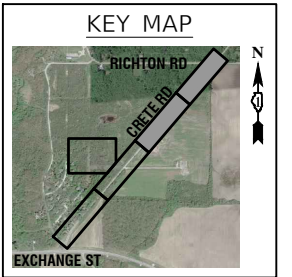
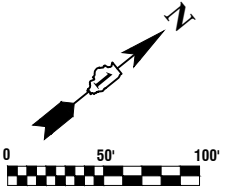
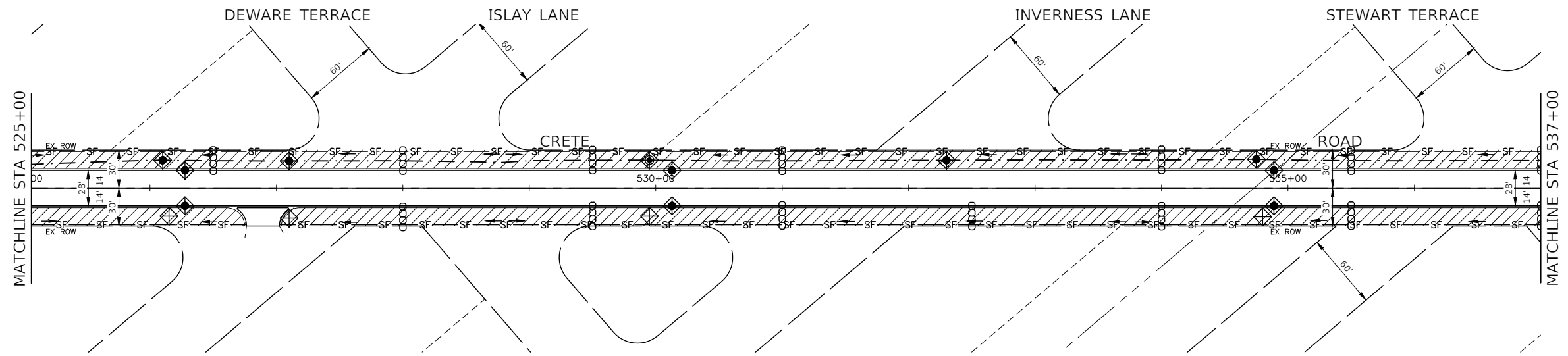
**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
LANDSCAPING & EROSION CONTROL**

SCALE: 1"=50'

SHEET NO. 24 OF 52 SHEETS

STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	24
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	UMBW(332)	



KEY	ITEM NAME
— SF —	PERIMETER EROSION BARRIER TO BE PLACED 1' FROM R.O.W.
◆	INLET FILTER
⊞	25 FT PERIMETER EROSION BARRIER
▨	SEEDING CLASS 2A WITH EROSION CONTROL BLANKET
▩	SEEDING CLASS 4 WITH EROSION CONTROL BLANKET
⊞⊞⊞	TEMPORARY DITCH CHECKS
⊞	RIP RAP (STONE RIP RAP CL. A, GRADATION RR4, FILTER FABRIC)

FILE NAME = 21R0316-LNCS-01 - P02

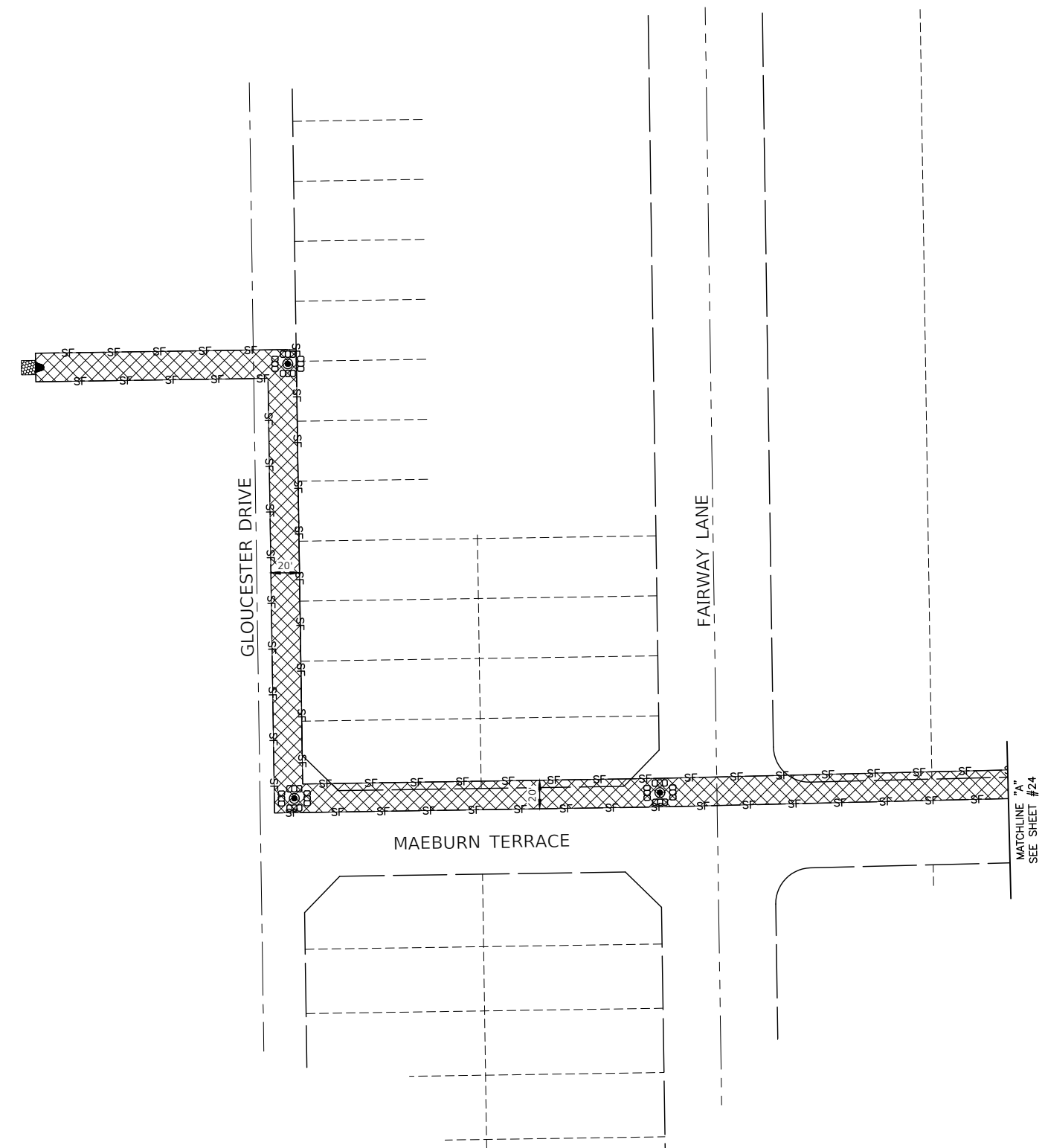
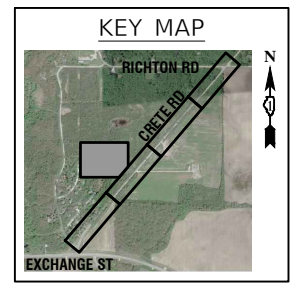
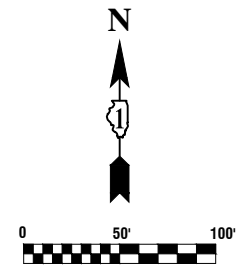
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	CHECKED — WPD	REVISED —
PLOT SCALE =	DRAWN — RG	REVISED —
PLOT DATE = 11-16-22	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
LANDSCAPING & EROSION CONTROL**

SCALE: 1"=50' SHEET NO. 25 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	25
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



KEY	ITEM NAME
— SF —	PERIMETER EROSION BARRIER TO BE PLACED 1' FROM R.O.W.
	INLET FILTER
	25 FT PERIMETER EROSION BARRIER
	SEEDING CLASS 2A WITH EROSION CONTROL BLANKET
	SEEDING CLASS 4 WITH EROSION CONTROL BLANKET
	TEMPORARY DITCH CHECKS
	RIP RAP (STONE RIP RAP CL. A, GRADATION RR4, FILTER FABRIC)

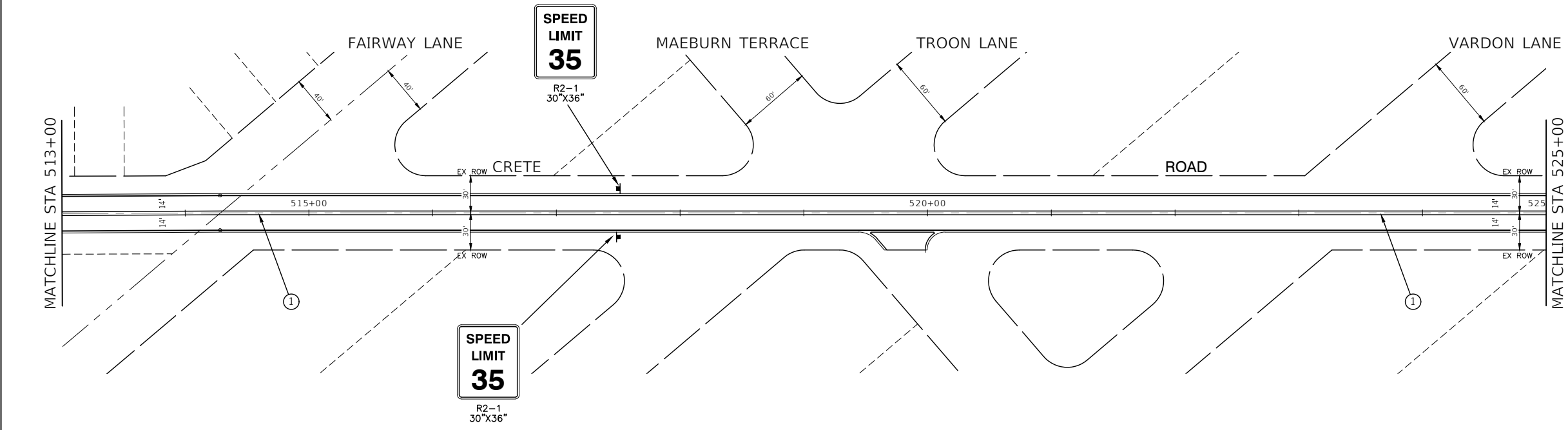
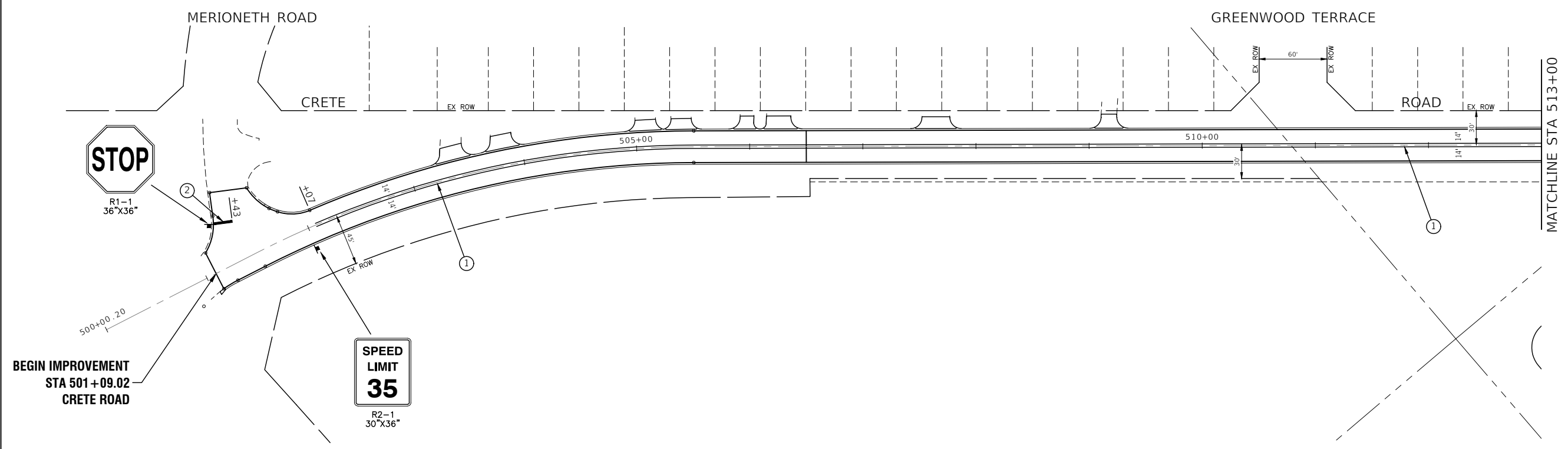
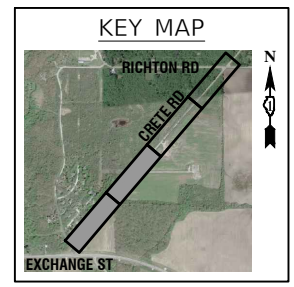
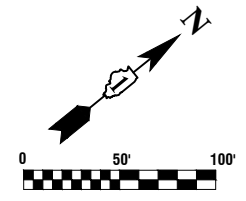
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	PLOT SCALE =	DRAWN — RG	REVISED —
	PLOT DATE = 11-16-22	CHECKED — AG	REVISED —

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
LANDSCAPING & EROSION CONTROL**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	26
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

SCALE: 1"=50' SHEET NO. 26 OF 52 SHEETS STA. TO STA.



- NOTES:**
- SEE IDOT DETAIL TC-13 FOR PAVEMENT MARKINGS.
 - THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON ALL HMA PAVEMENT AREAS.
 - EXISTING SIGNAGE IS IN PLACE CURRENTLY AND SHALL BE REMOVED BEFORE PLACEMENT OF NEW SIGNAGE. QUANTITIES FOR REMOVE SIGN PANEL ASSEMBLY HAVE BEEN PROVIDED FOR THIS WORK.

PAVEMENT MARKING AND SIGNING LEGEND

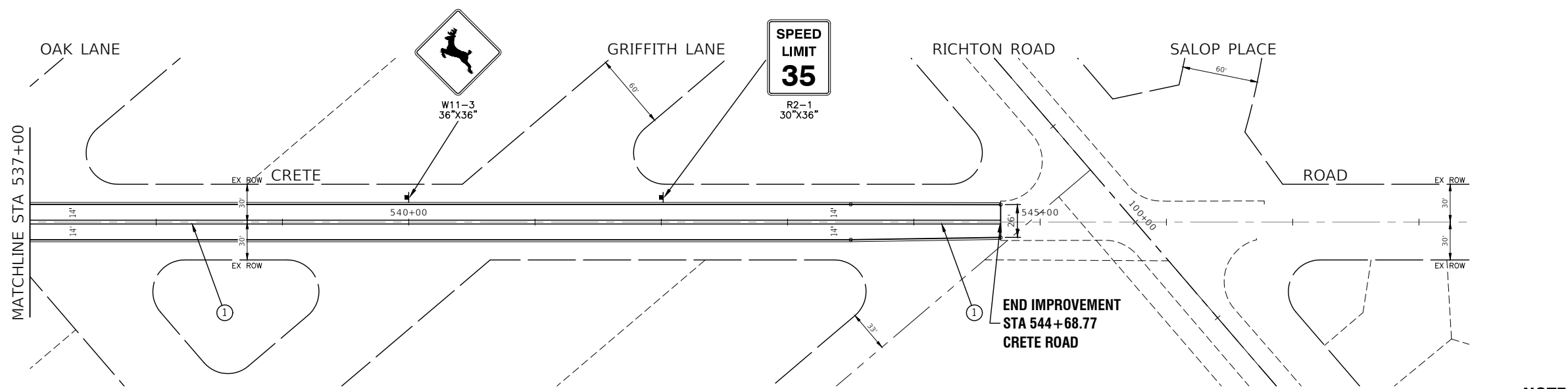
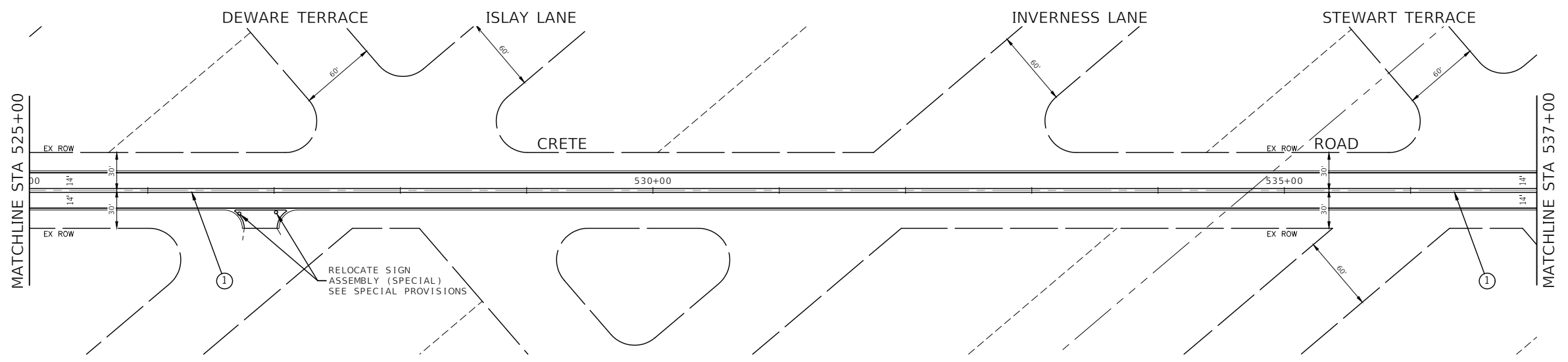
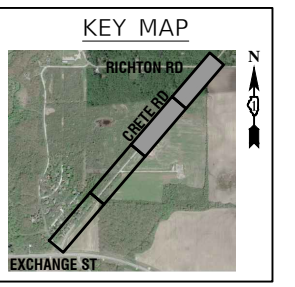
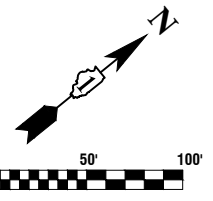
- ① 4" DOUBLE YELLOW CENTERLINE (11" C/C)
- ② 24" WHITE STOP BAR

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		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION PAVEMENT MARKING & SIGNING	
SCALE:	STA. TO STA.
SHEET NO. 27 OF 52 SHEETS	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	27
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



- NOTES:**
1. SEE IDOT DETAIL TC-13 FOR PAVEMENT MARKINGS.
 2. THERMOPLASTIC PAVEMENT MARKINGS SHALL BE USED ON ALL HMA PAVEMENT AREAS.
 3. EXISTING SIGNAGE IS IN PLACE CURRENTLY AND SHALL BE REMOVED BEFORE PLACEMENT OF NEW SIGNAGE. QUANTITIES FOR REMOVE SIGN PANEL ASSEMBLY HAVE BEEN PROVIDED FOR THIS WORK.

PAVEMENT MARKING AND SIGNING LEGEND

- ① 4" DOUBLE YELLOW CENTERLINE (11" C/C)
- ② 24" WHITE STOP BAR

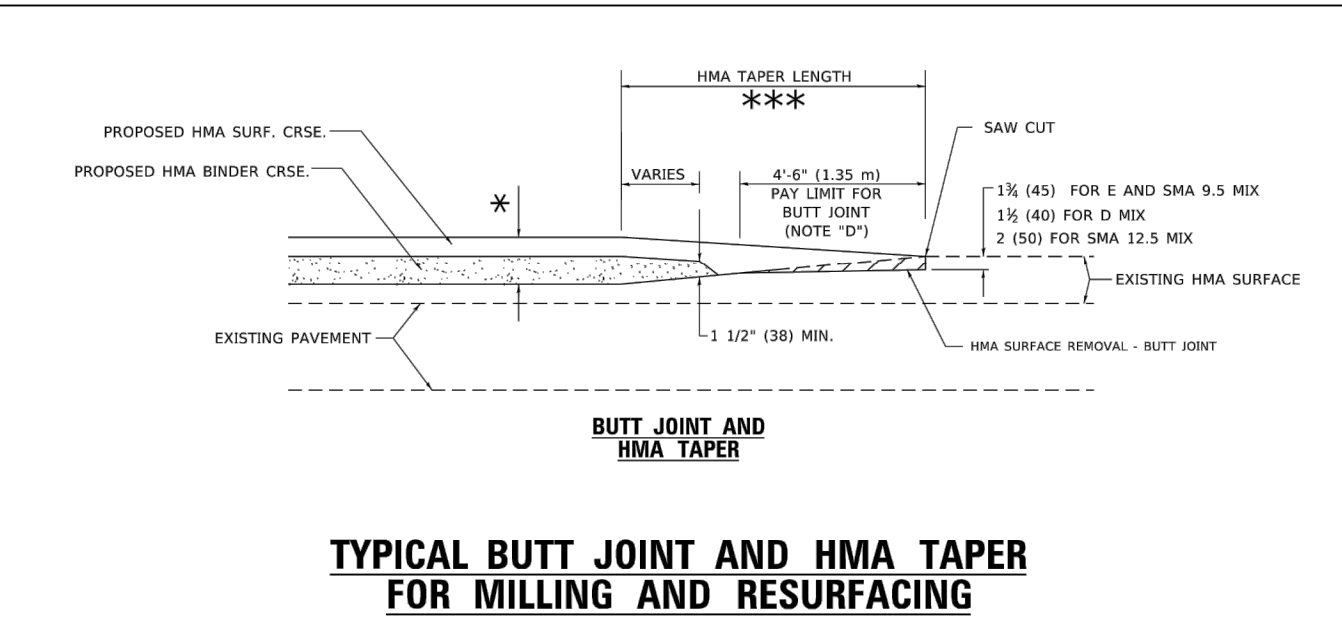
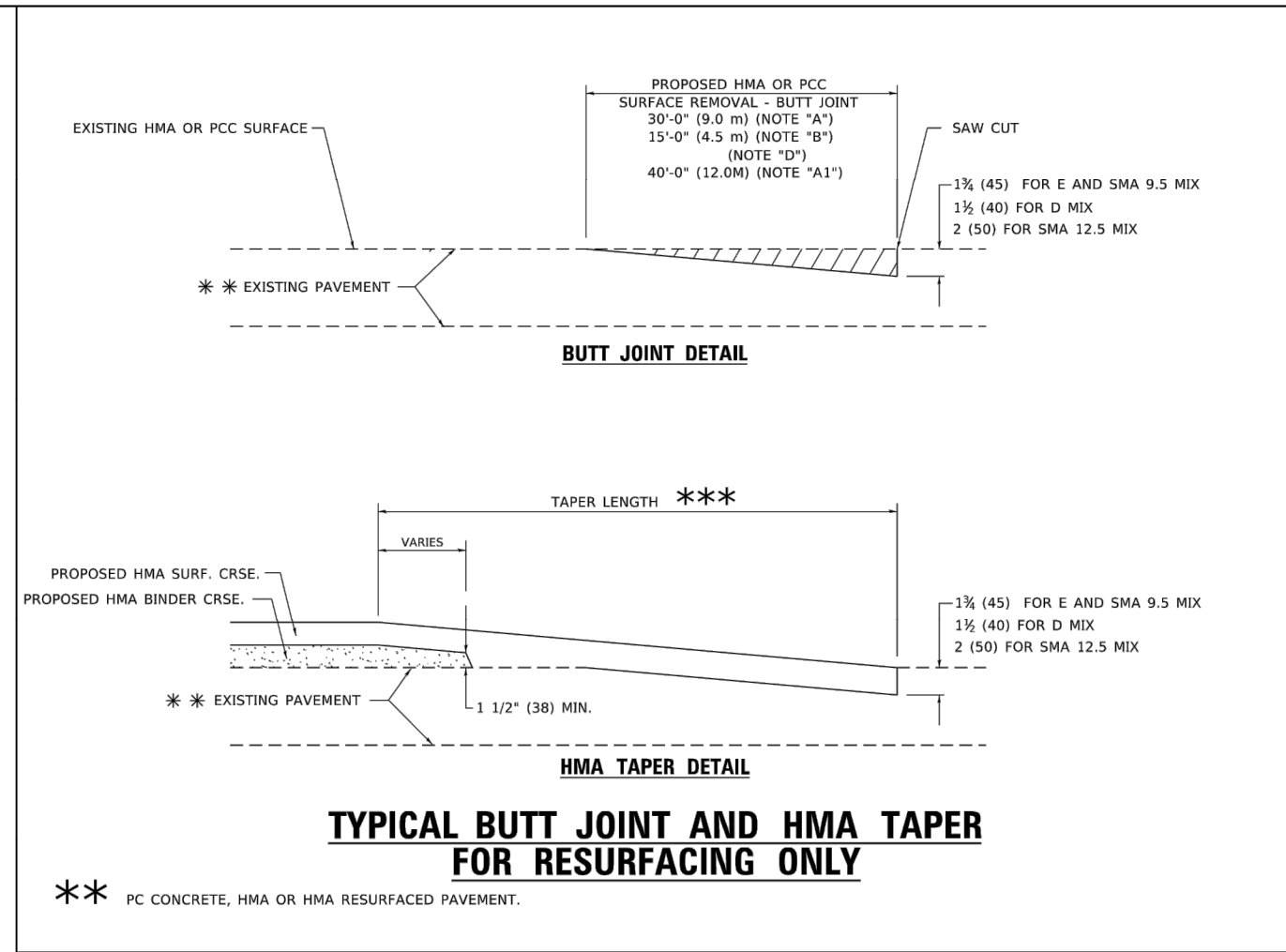
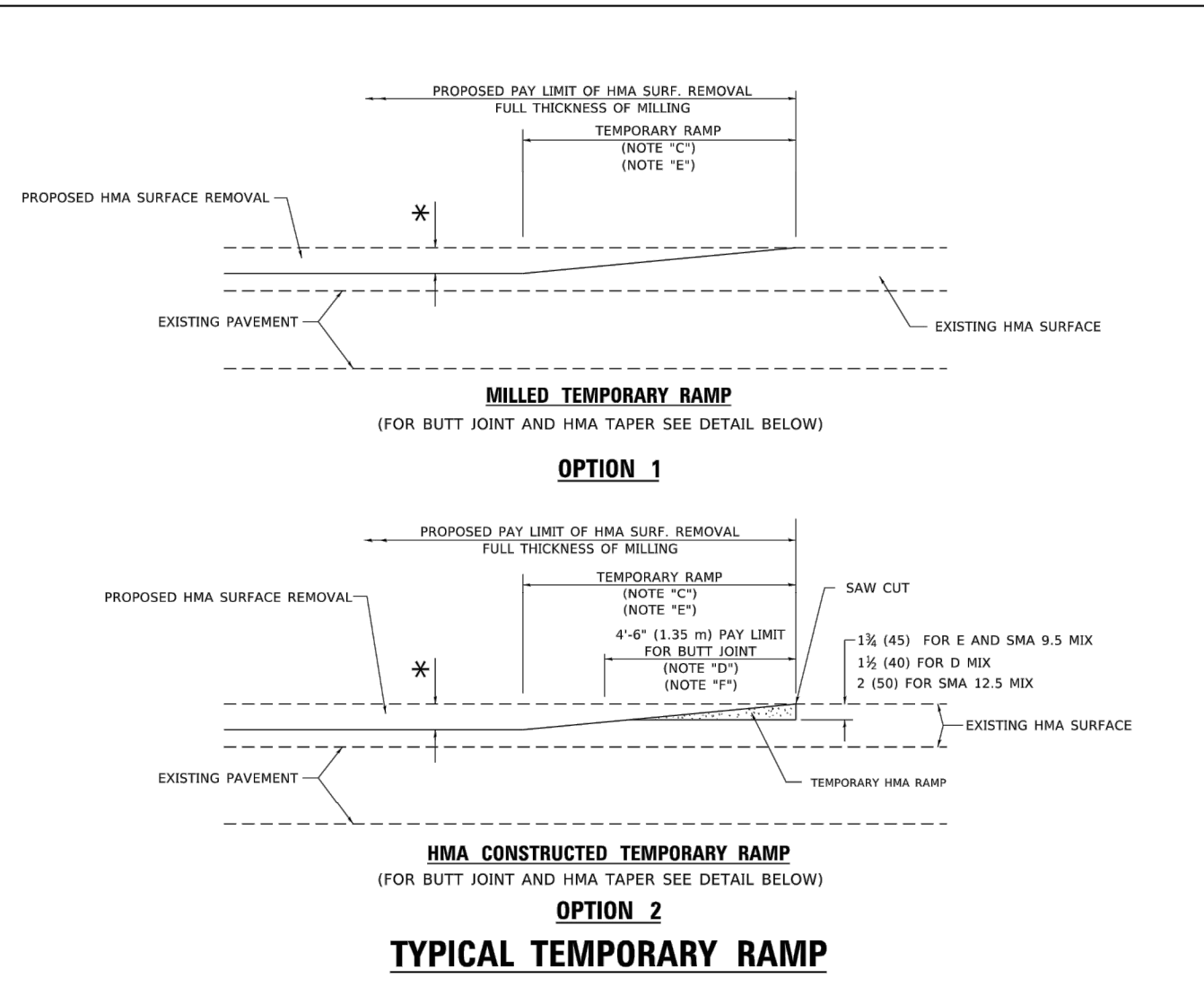
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	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
PAVEMENT MARKING & SIGNING**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	28
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

SCALE: SHEET NO. 28 OF 52 SHEETS STA. TO STA.



GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

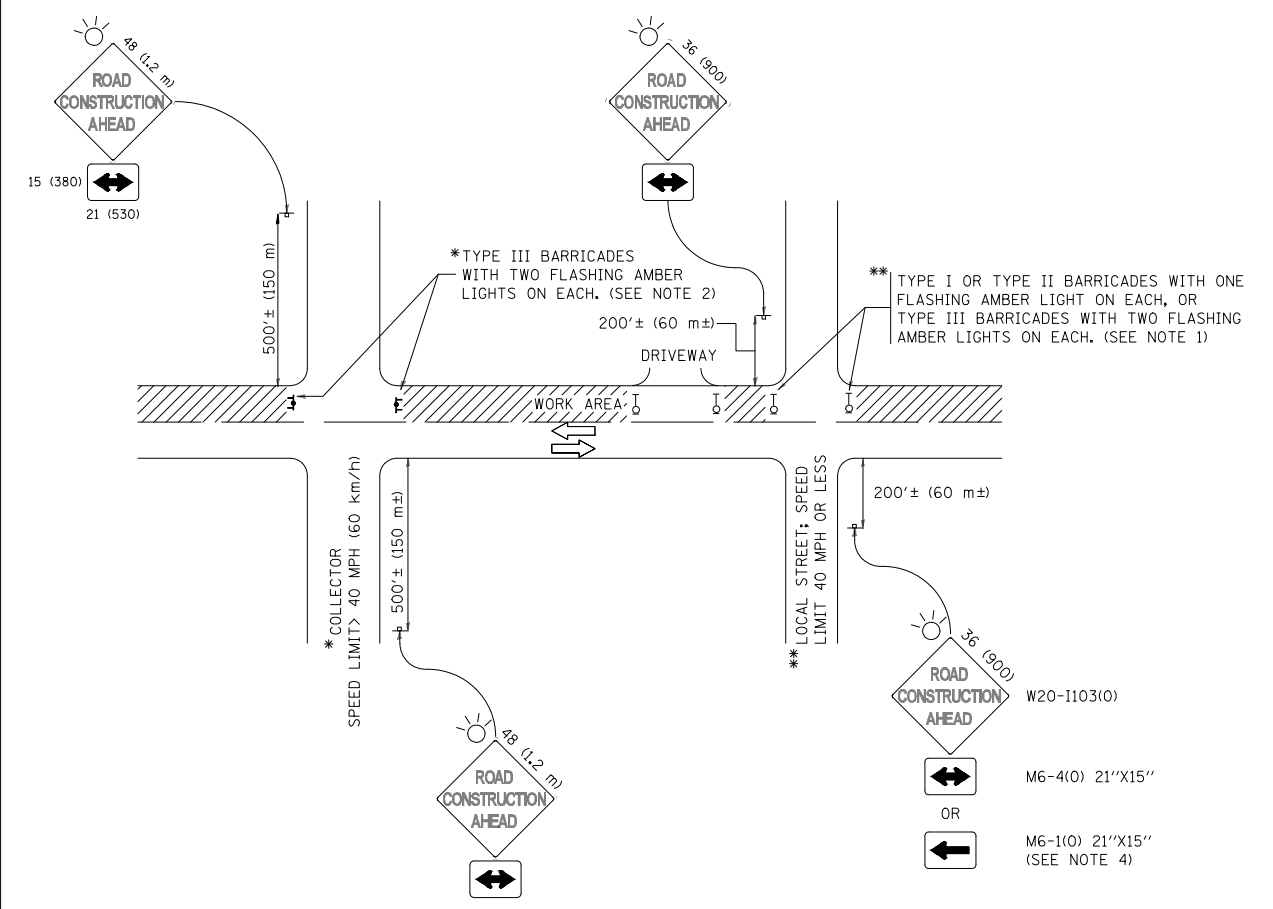
BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".
2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = demanchelt	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01				BD400-05	BD-32	CONTRACT NO.		
PLOT DATE = 2/2/2022	DATE - 06-13-90	REVISED - R. BORO 01-01-07				ILLINOIS	FED. AID PROJECT			

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	CHECKED --	REVISED --	4309				15-00049-00-PV	WILL	52	29	
	PLOT SCALE =	DRAWN --	REVISED --				SCALE:	SHEET NO. 29 OF 52 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

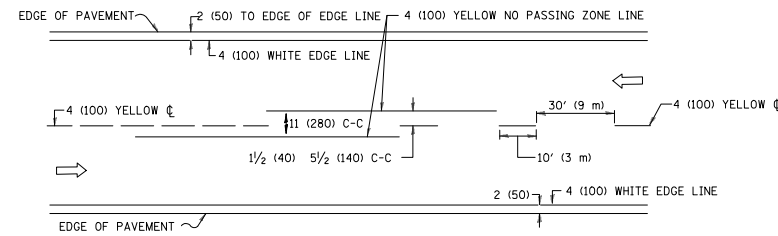
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

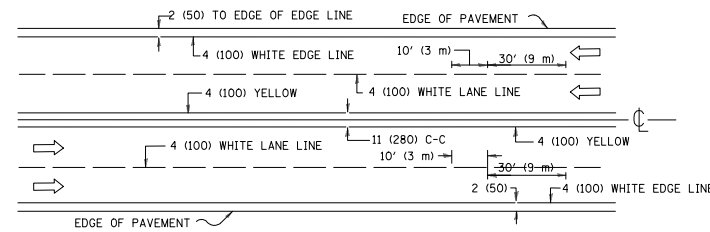
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE				SHEET 1 OF 1 SHEETS				TC-10		CONTRACT NO.		
				STA. TO STA.				ILLINOIS FED. AID PROJECT				

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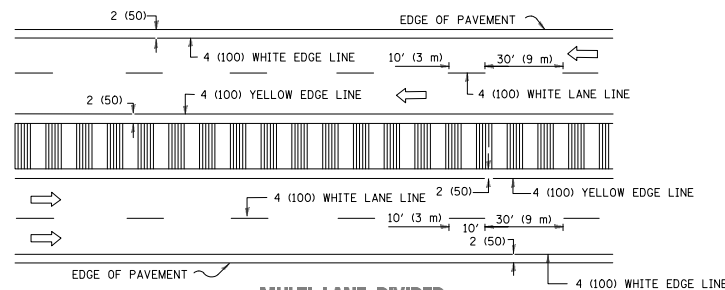
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				STA. TO STA.				WILL 52 30				
								CONTRACT NO. 61H99				
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



2-LANE ROADWAY

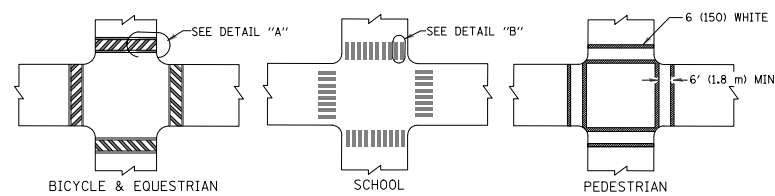


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

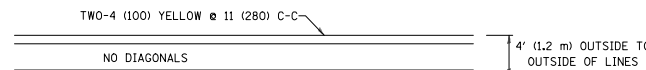


DETAIL "A"

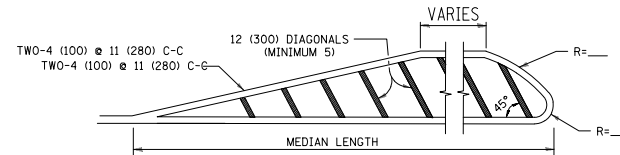
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



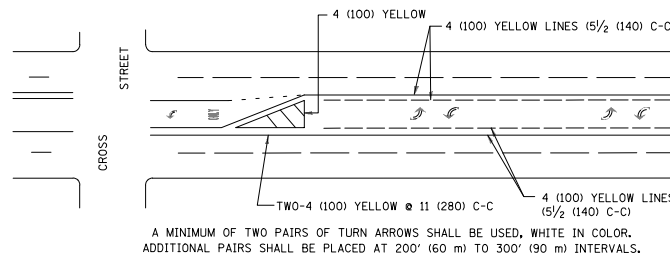
4' (1.2 m) WIDE MEDIANS ONLY



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

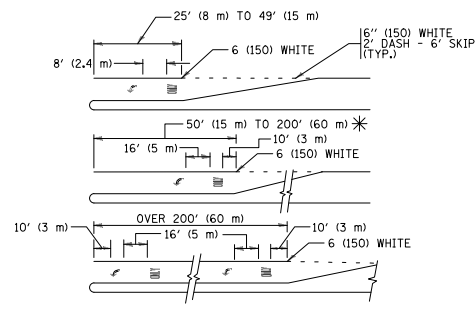
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

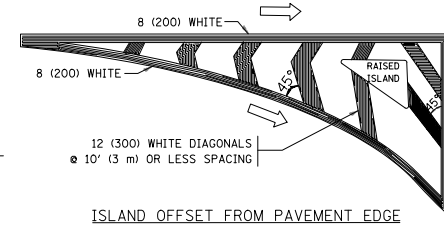
TYPICAL PAINTED MEDIAN MARKING



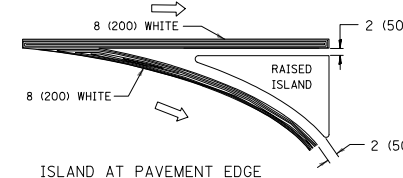
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

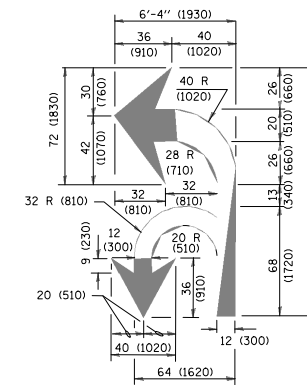


ISLAND OFFSET FROM PAVEMENT EDGE

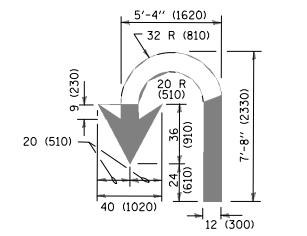


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN); A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13			31
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

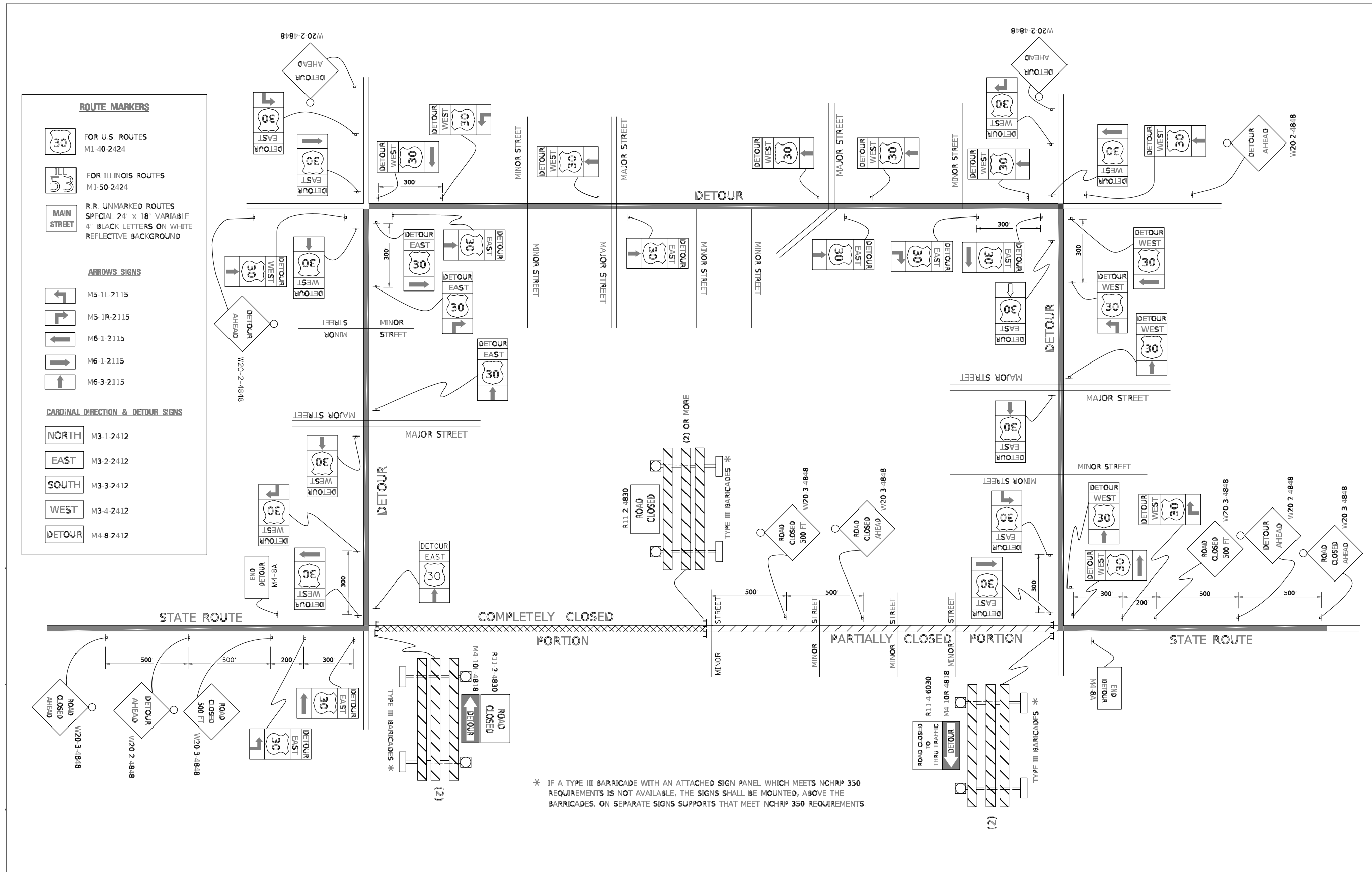
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	PLOT SCALE =	DRAWN -	REVISED -
	PLOT DATE =	CHECKED -	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT 1 DETAILS

SCALE: SHEET NO. 31 OF 52 SHEETS STA. TO STA.

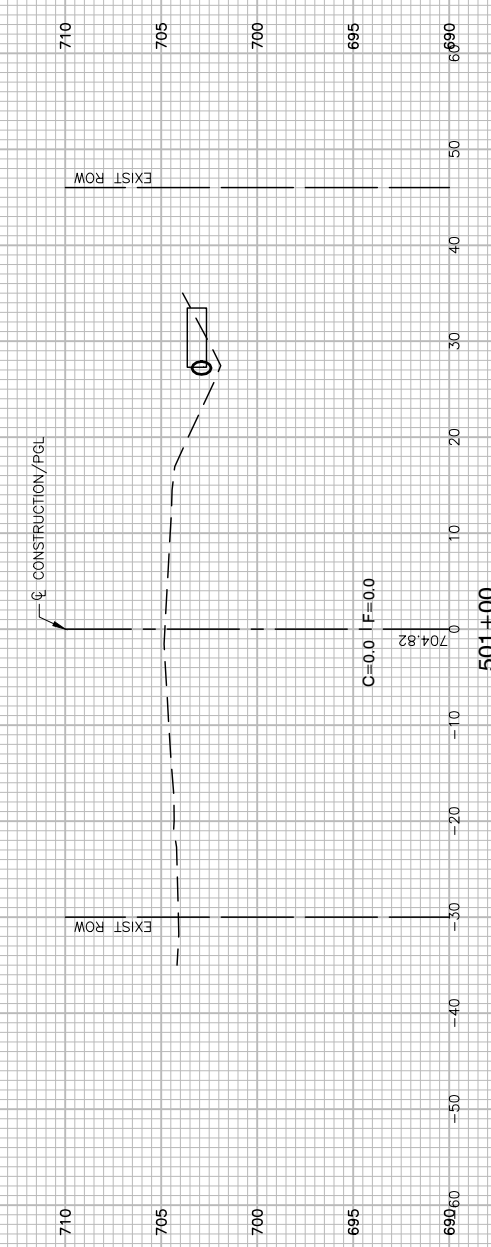
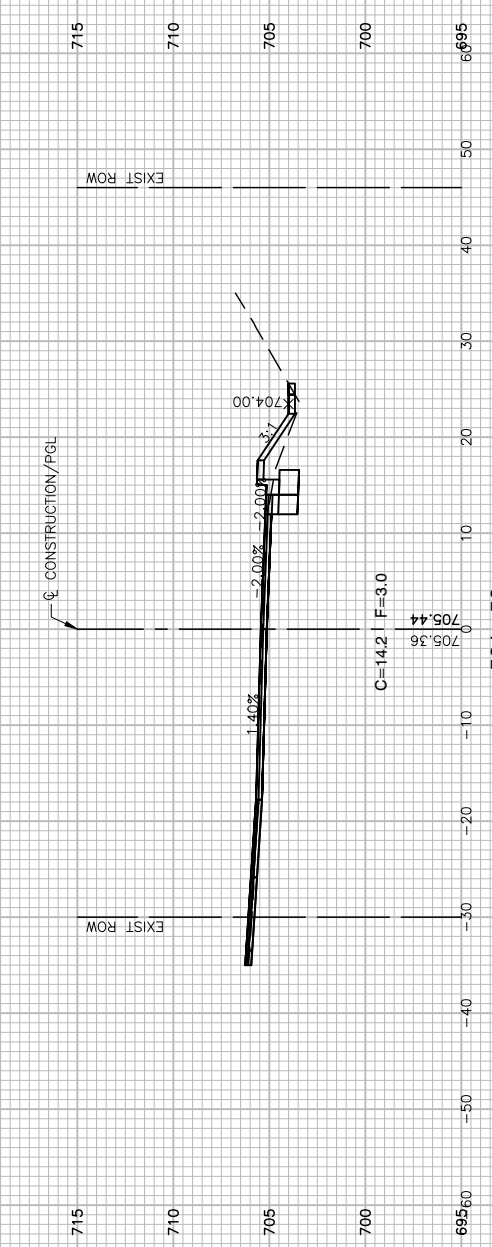
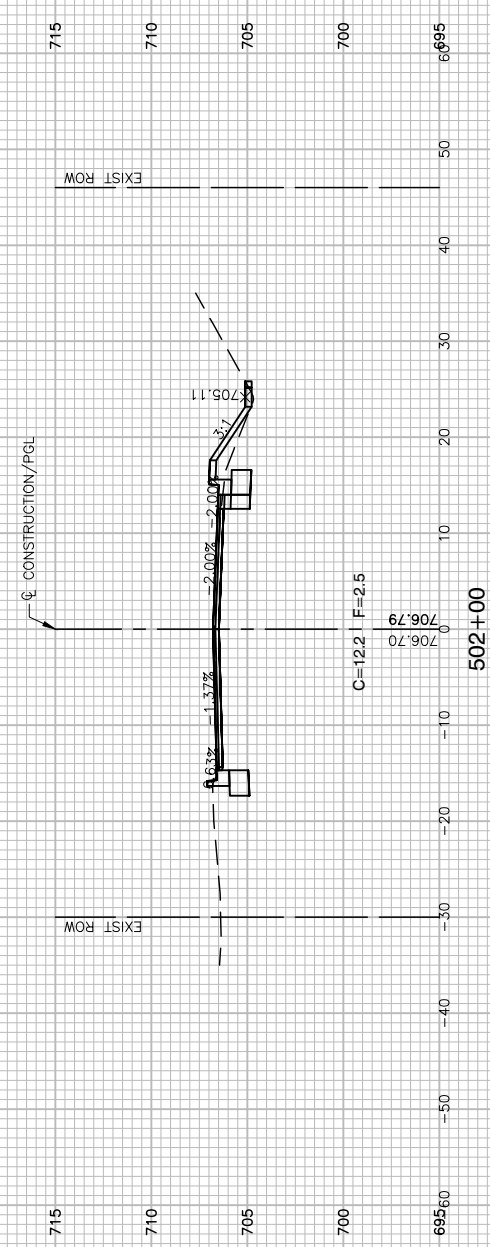
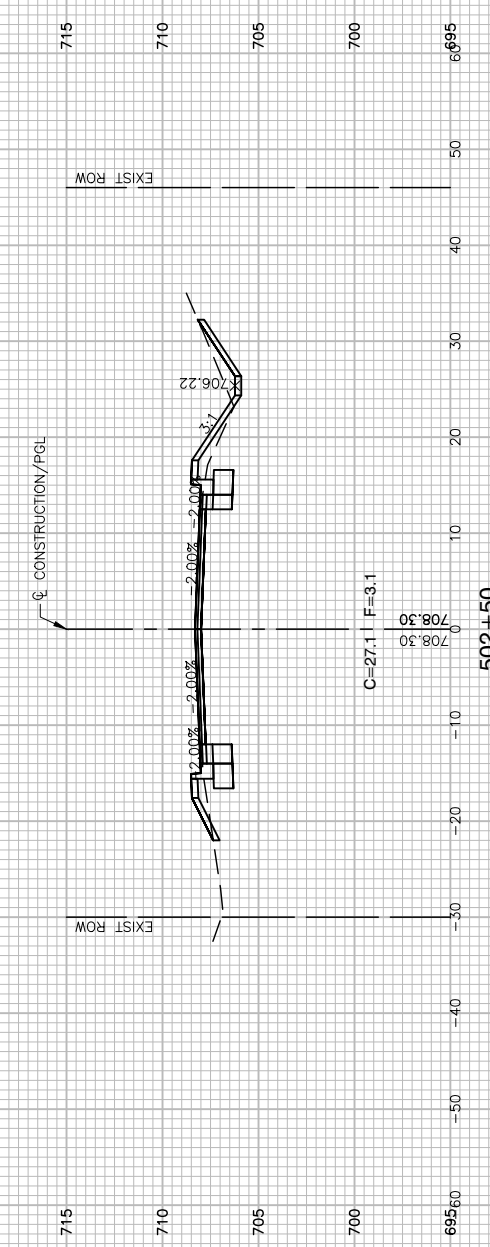
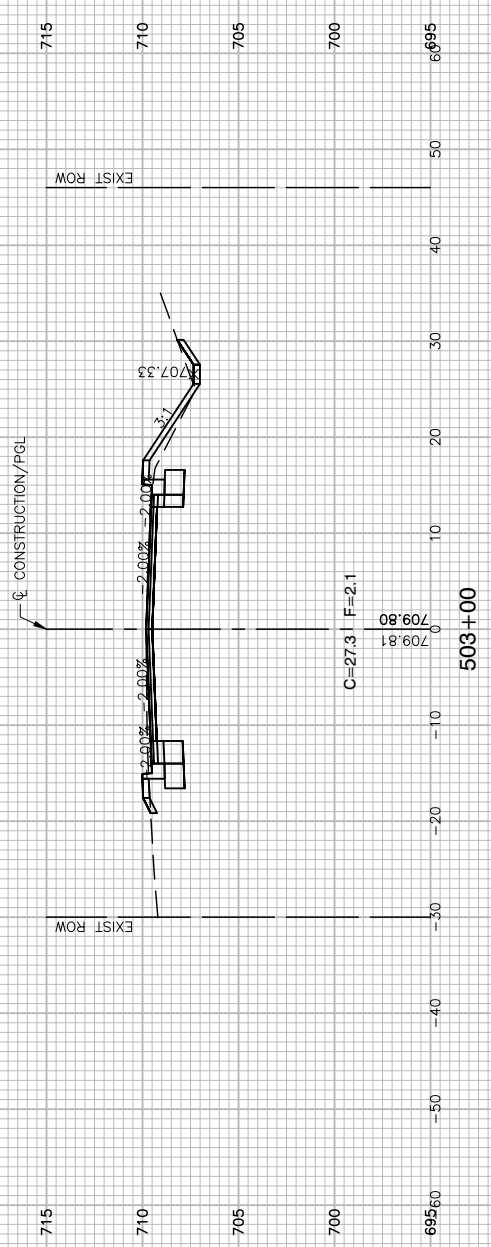
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	31
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



- ROUTE MARKERS**
- FOR U.S. ROUTES
M1-40 2424
 - FOR ILLINOIS ROUTES
M1-50 2424
 - R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND
- ARROWS SIGNS**
- M5 1L 2115
 - M5 1R 2115
 - M6 1 2115
 - M6 1 2115
 - M6 3 2115
- CARDINAL DIRECTION & DETOUR SIGNS**
- NORTH M3 1 2412
 - EAST M3 2 2412
 - SOUTH M3 3 2412
 - WEST M3 4 2412
 - DETOUR M4 8 2412

* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS

USER NAME = foolemj	DESIGNED -	REVISED - 10 18 02	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				DETOUR SIGNING FOR CLOSING STATE HIGHWAYS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50 0000 n	DRAWN -	REVISED - R BORO 09 14 09					SCALE: NONE				SHEET 1	OF 1	SHEETS	STA.	TO STA.	TC-21
PLOT DATE = 3 4 2019	CHECKED -	REVISED -											ILLINOIS FED AID PROJECT			
	DATE -	REVISED -											CONTRACT NO.			

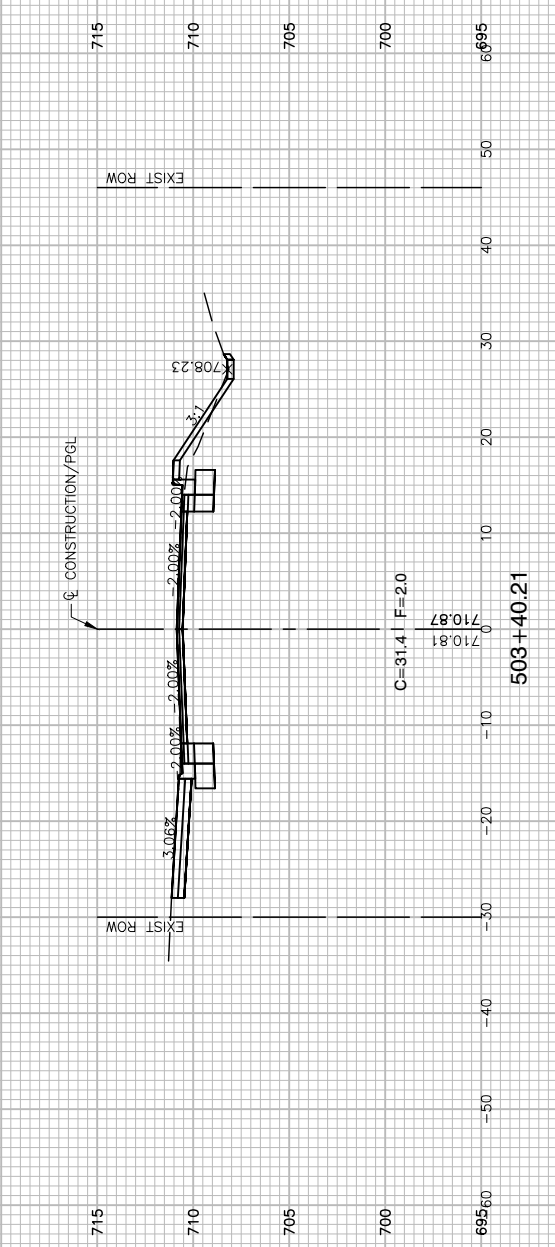
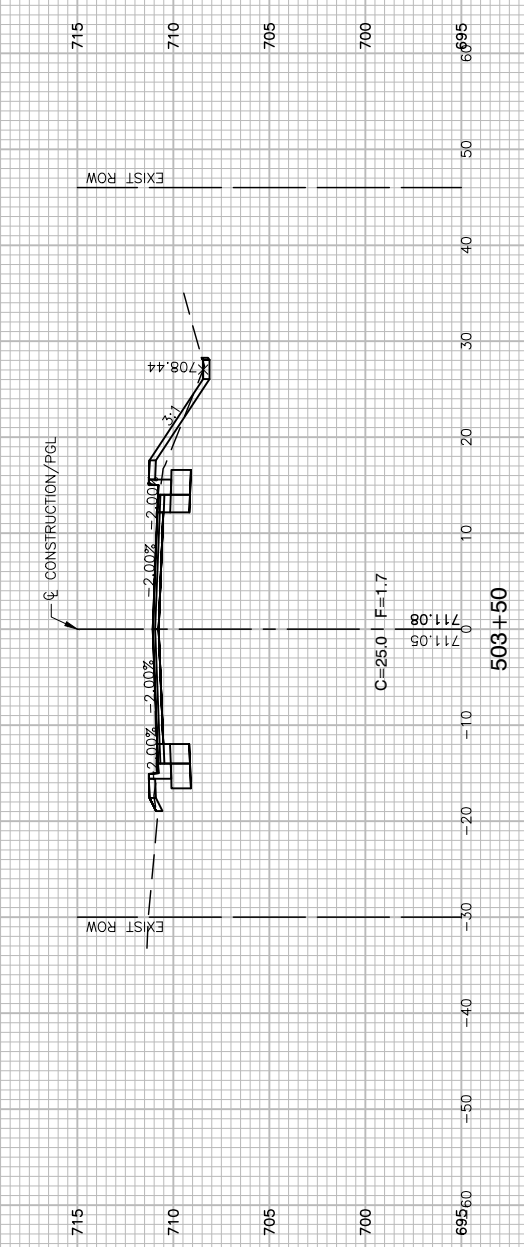
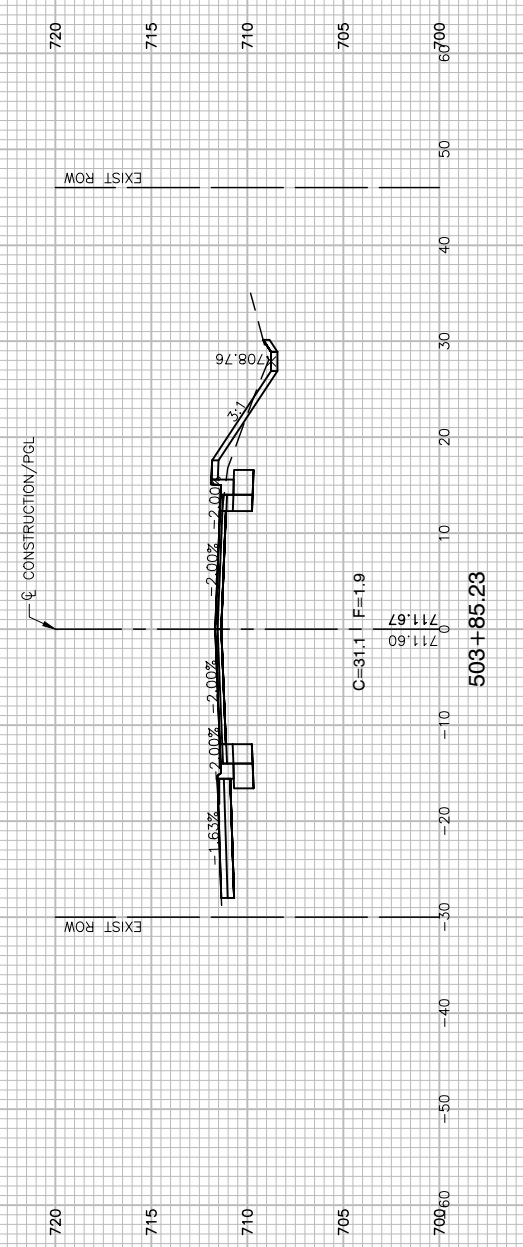
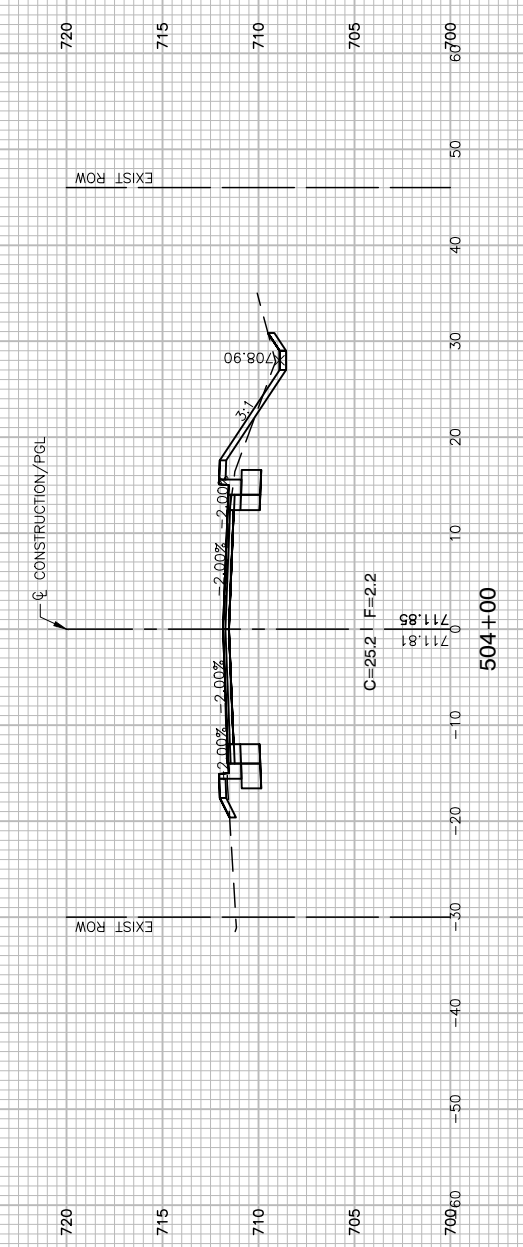
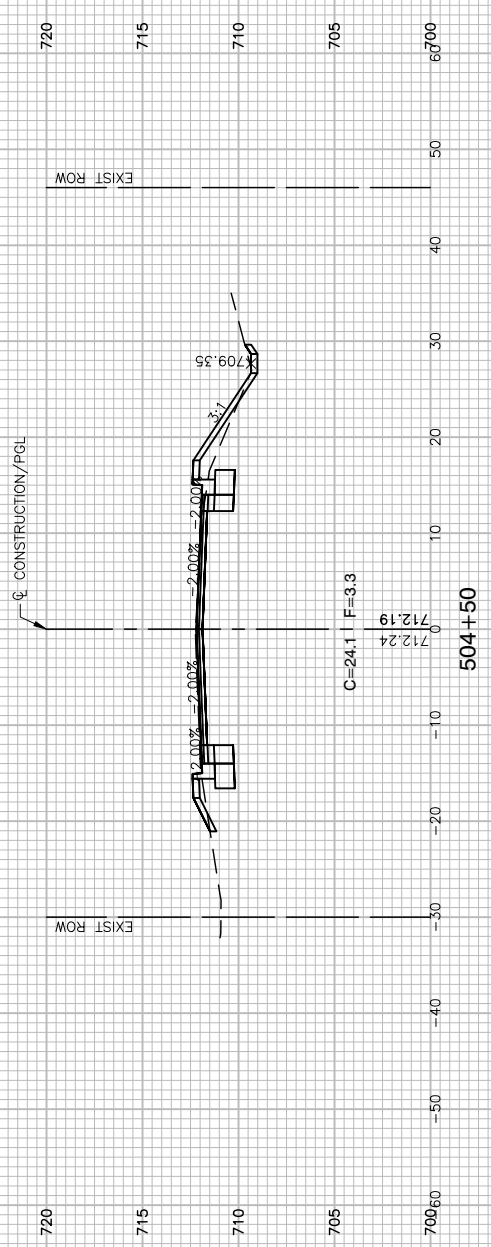


FILE NAME = 21R0316-XSCT-01 - X01	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 33	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	33
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	UMBW(332)	



FILE NAME = 21R0316-XSCT-01 - X02

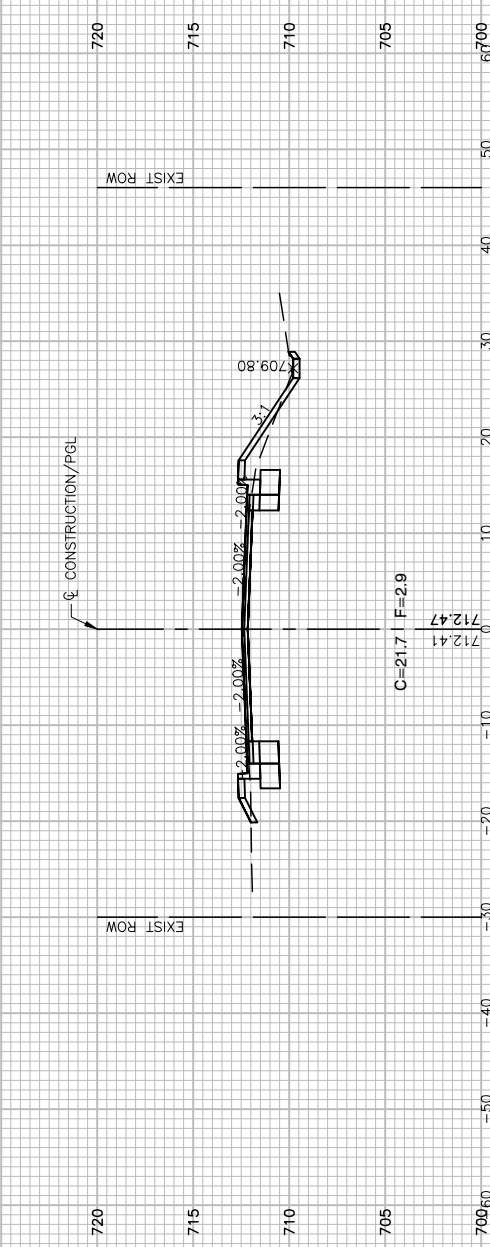
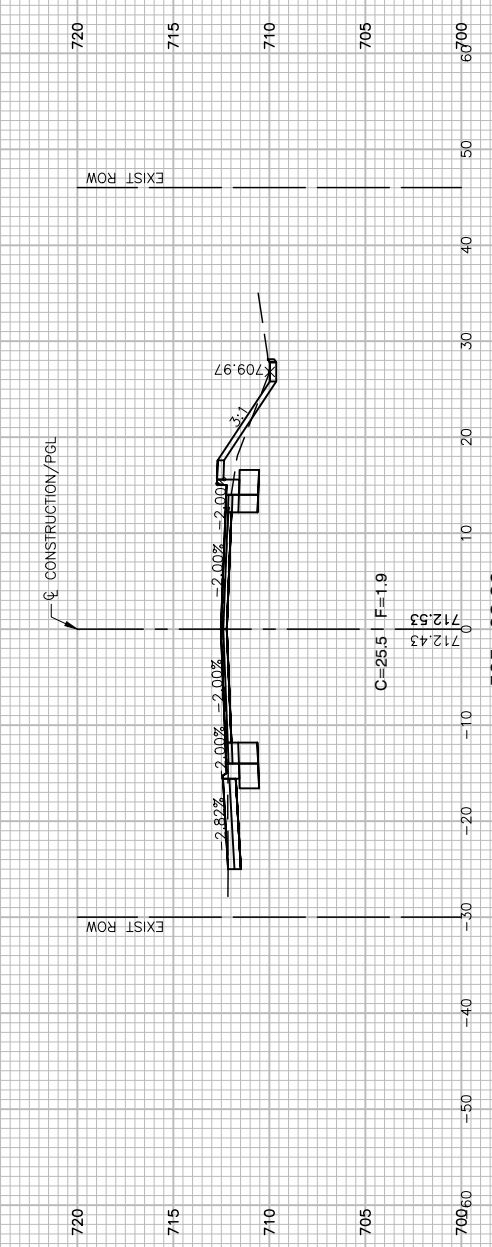
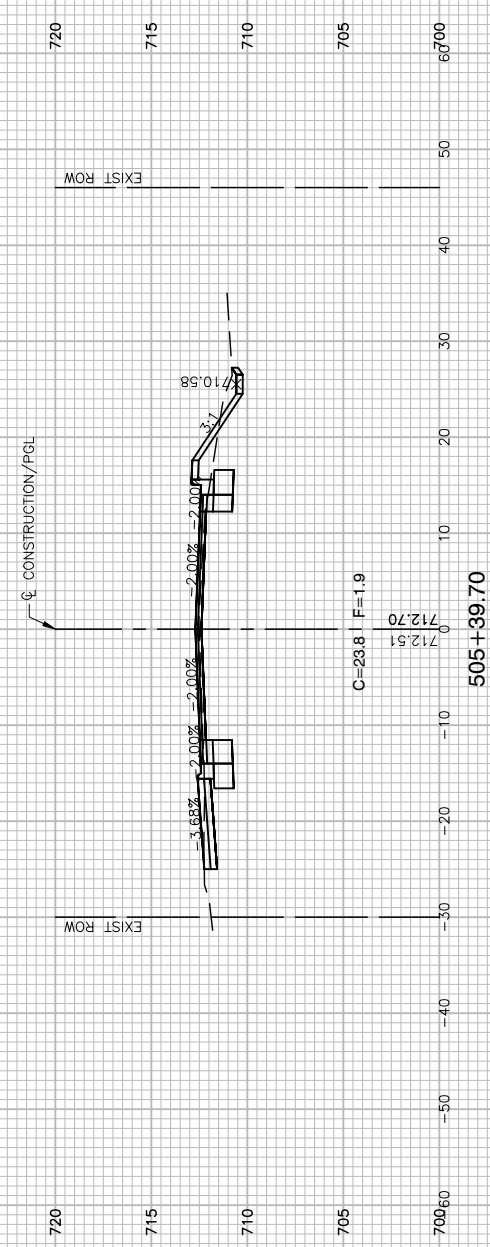
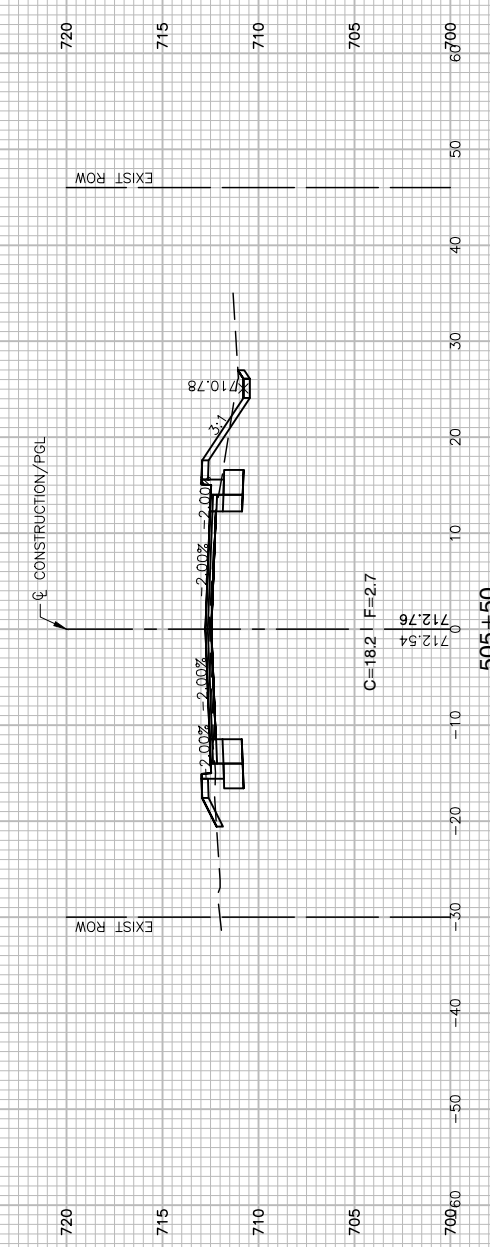
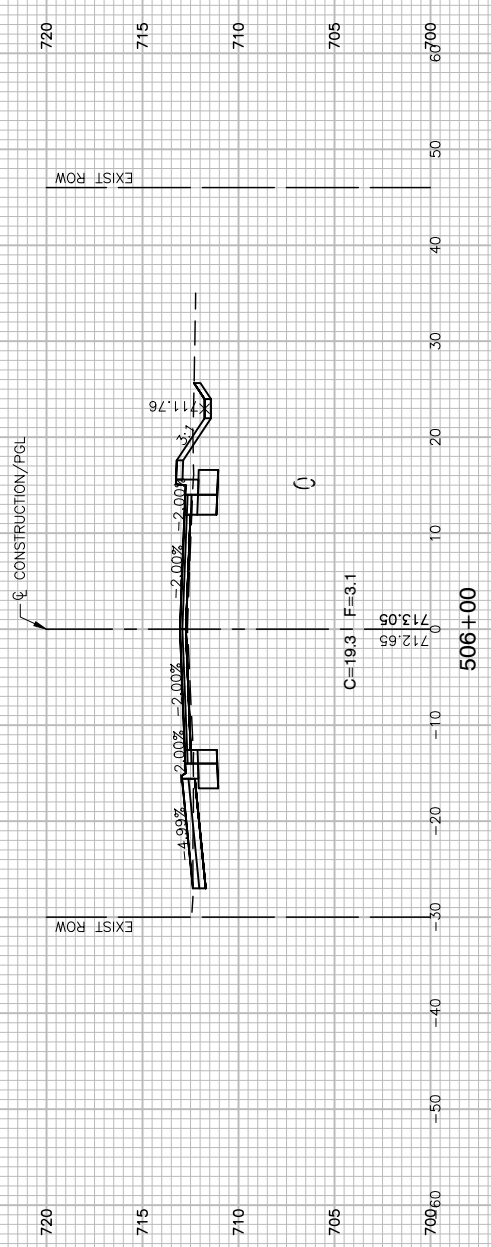
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 34 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	34
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

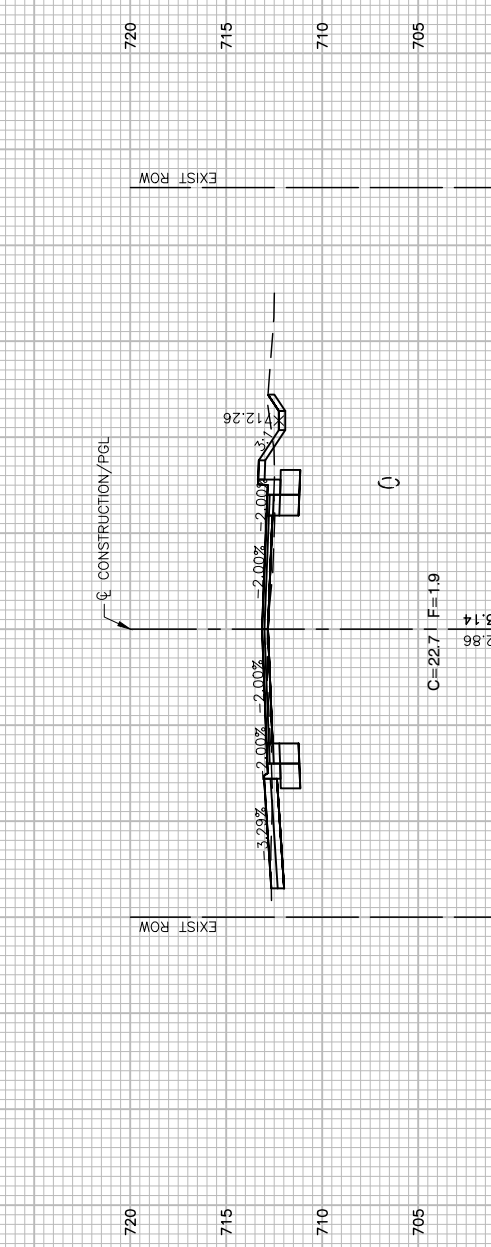
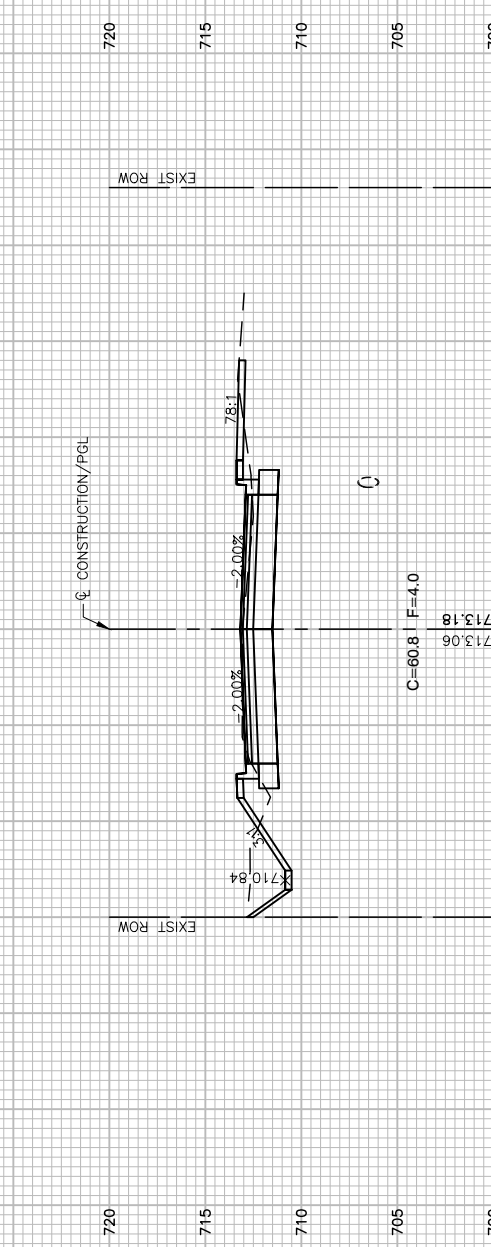
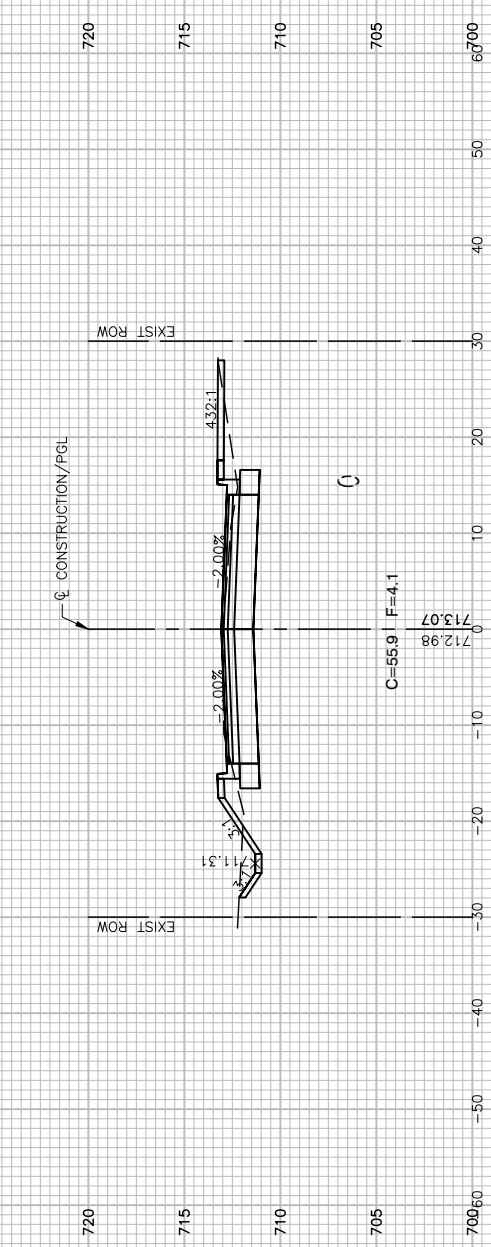
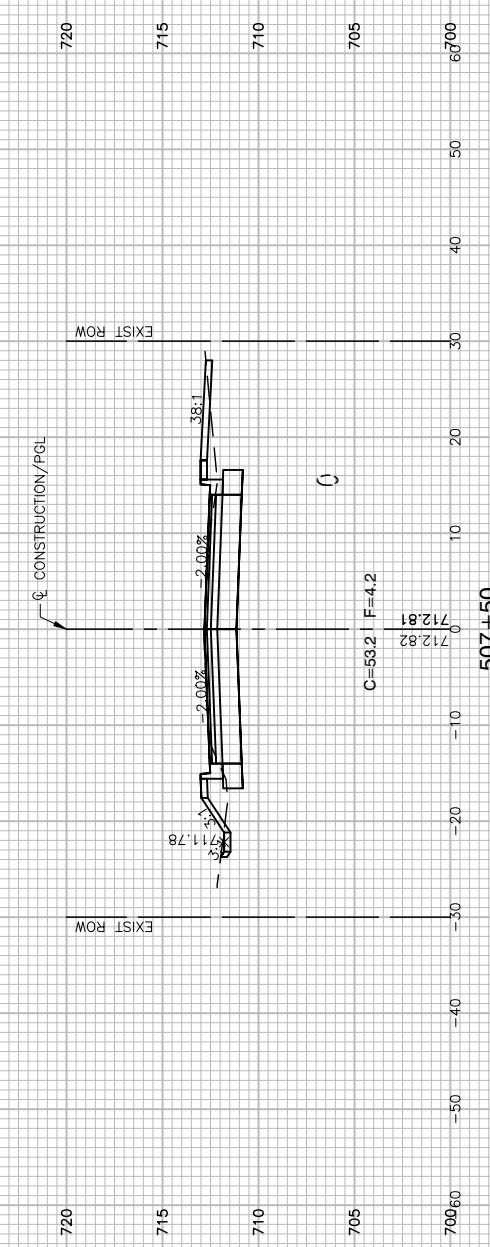
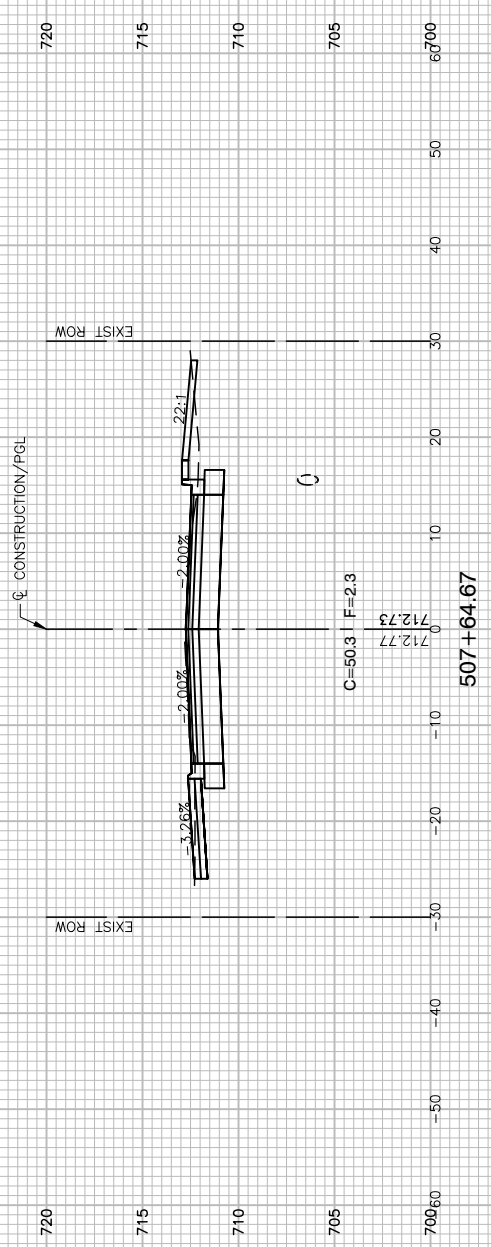


FILE NAME = 21R0316-XSCT-01 - X03	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 35	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	35
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	UMBW(332)	



FILE NAME = 21R0316-XSCT-01 - X04

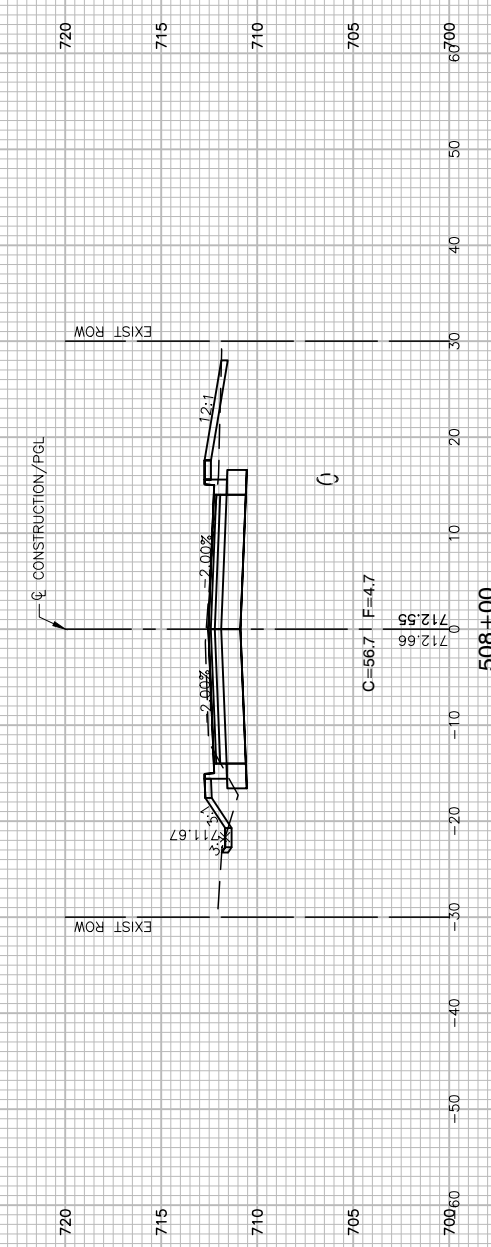
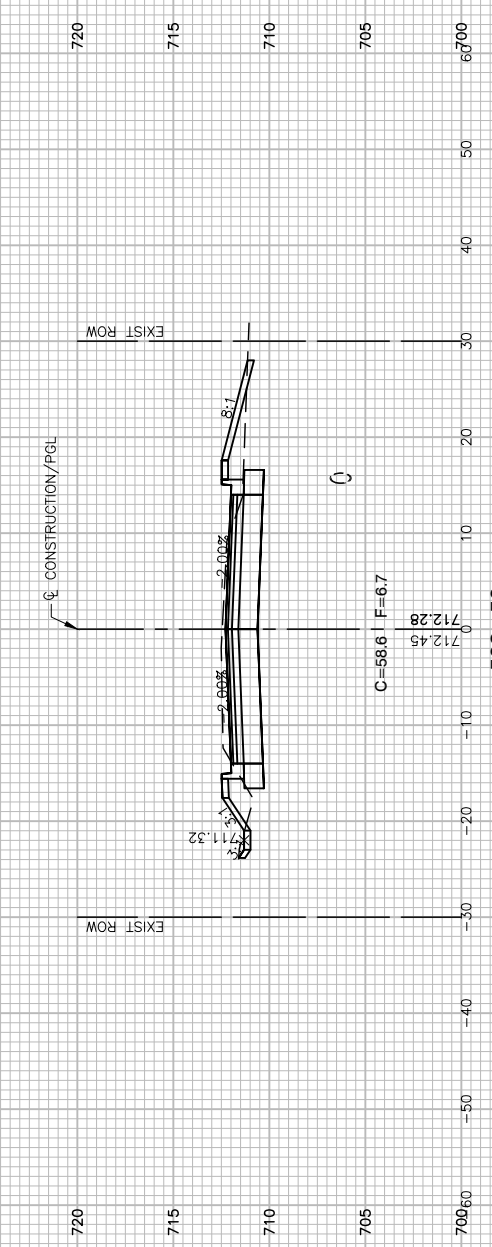
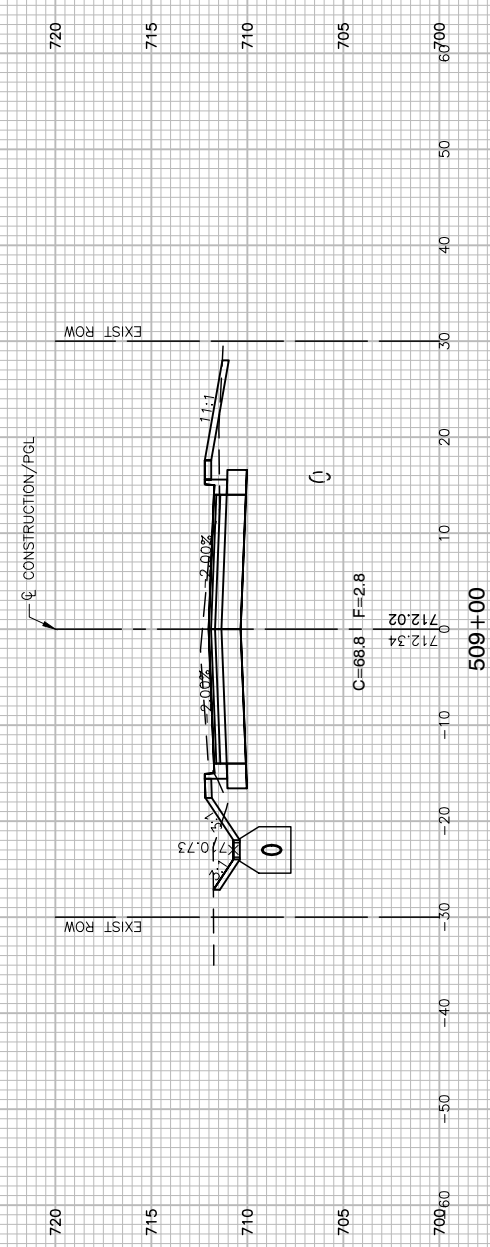
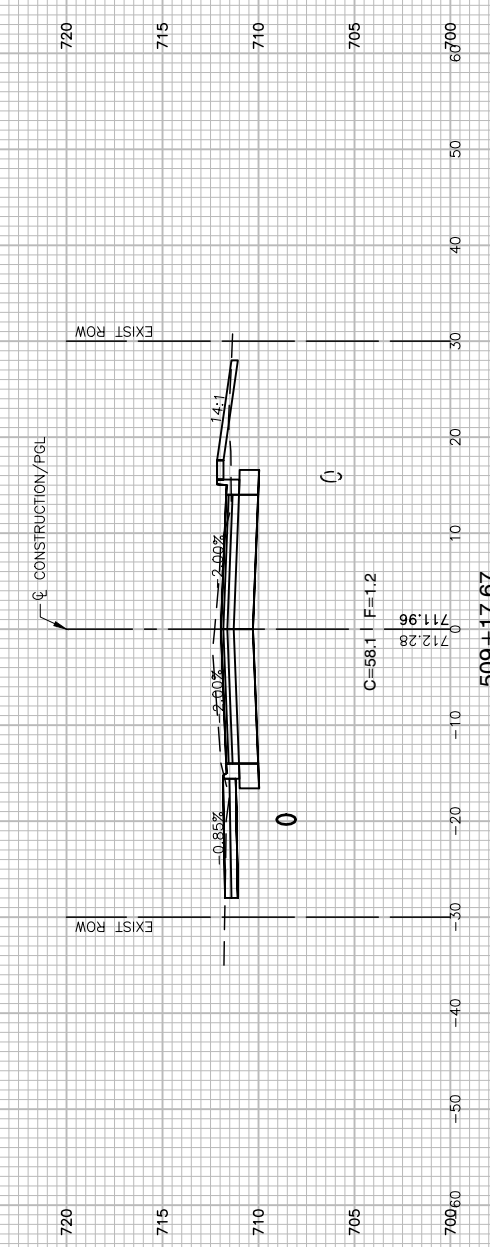
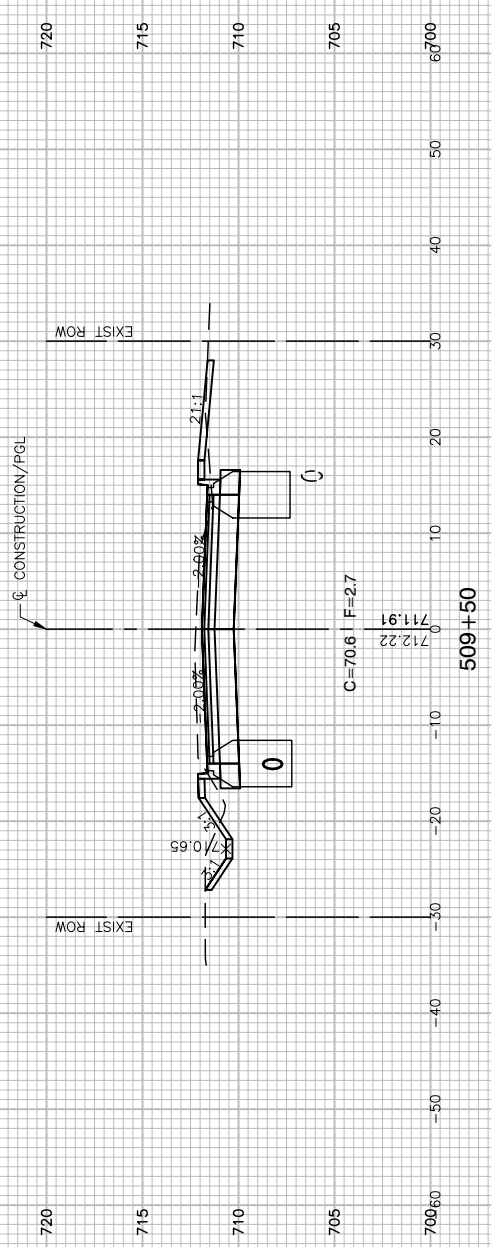
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 36 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	36
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

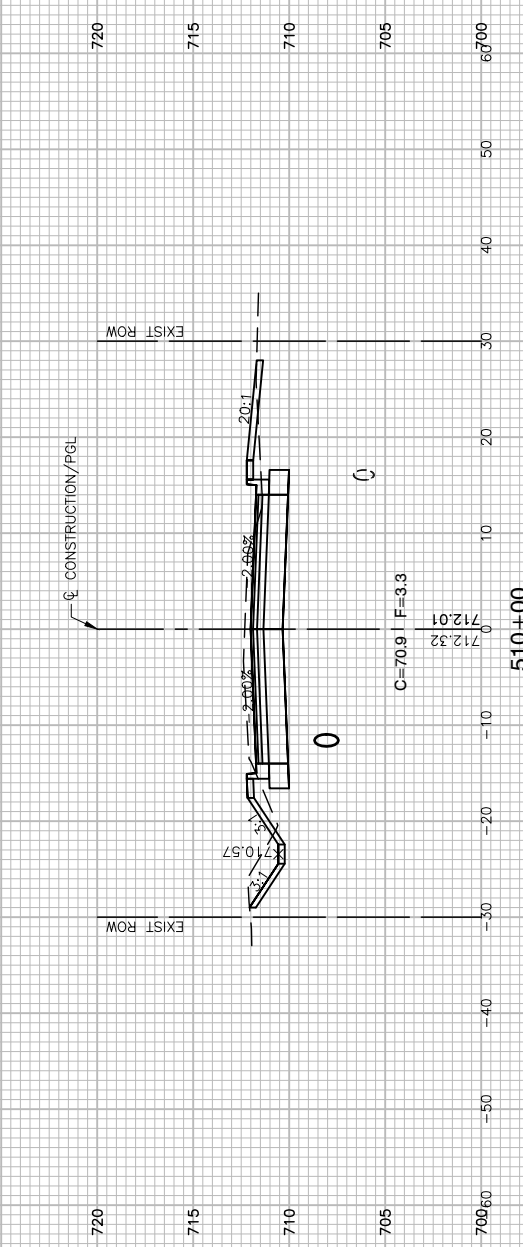
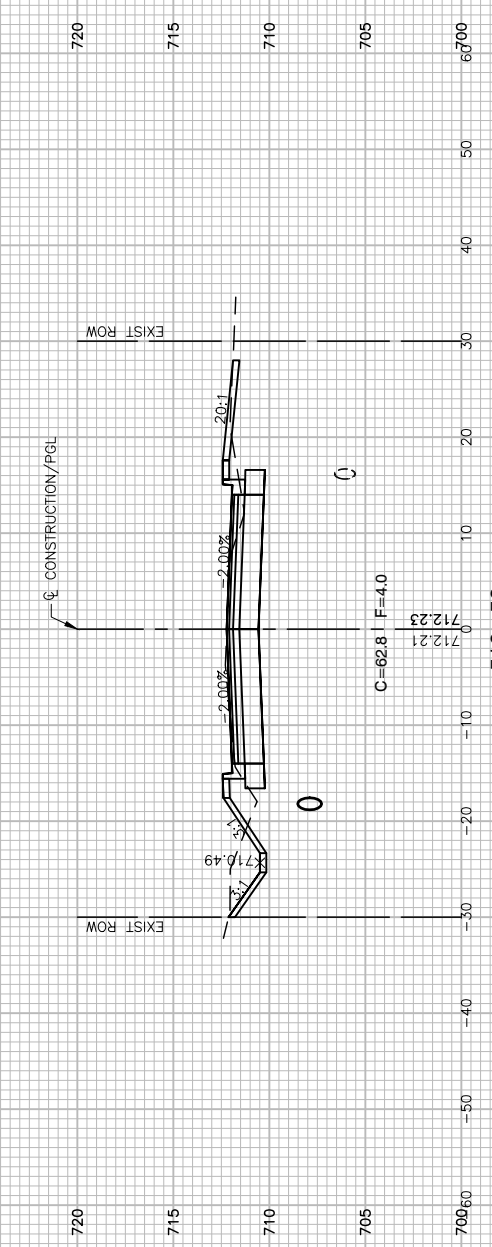
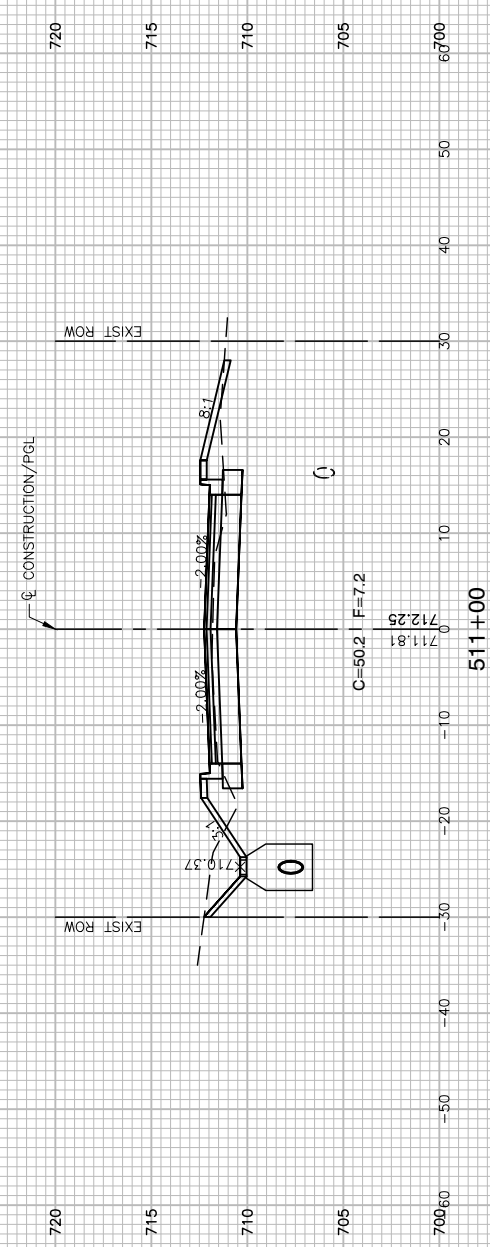
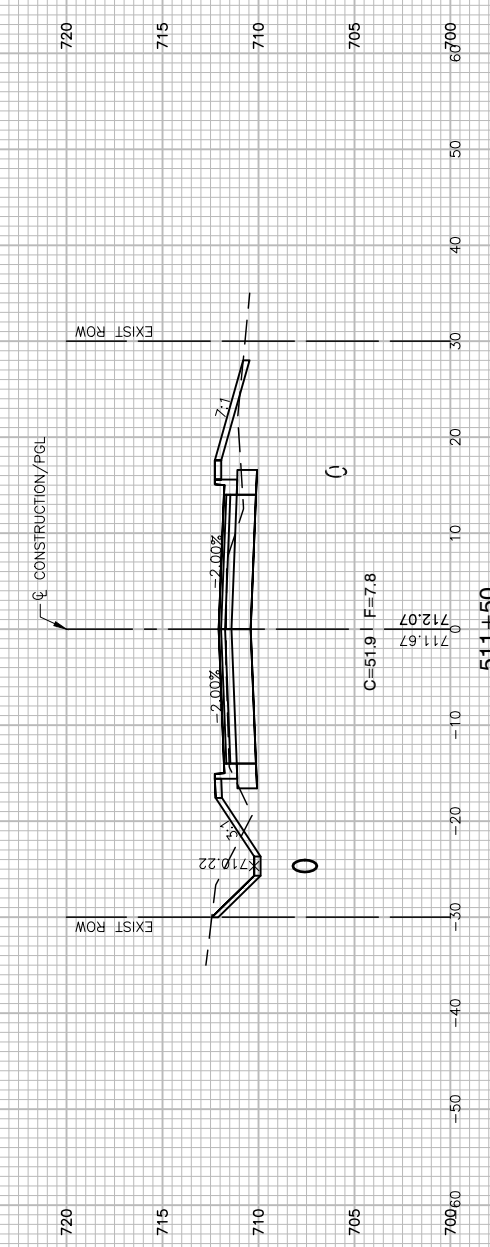
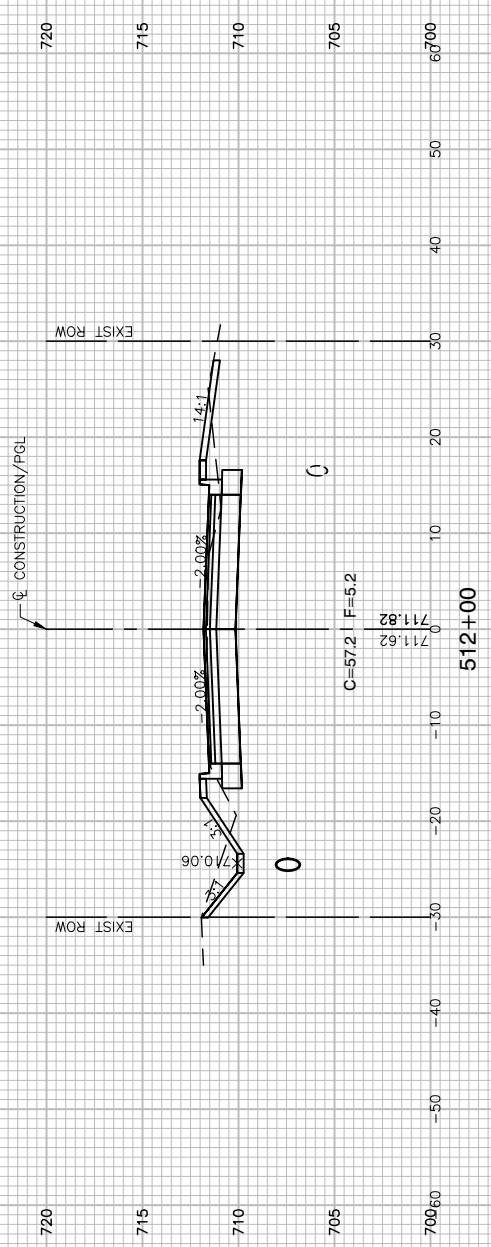


FILE NAME = 21R0316-XSCT-01 - X05	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 37	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	37
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	UMBW(332)	

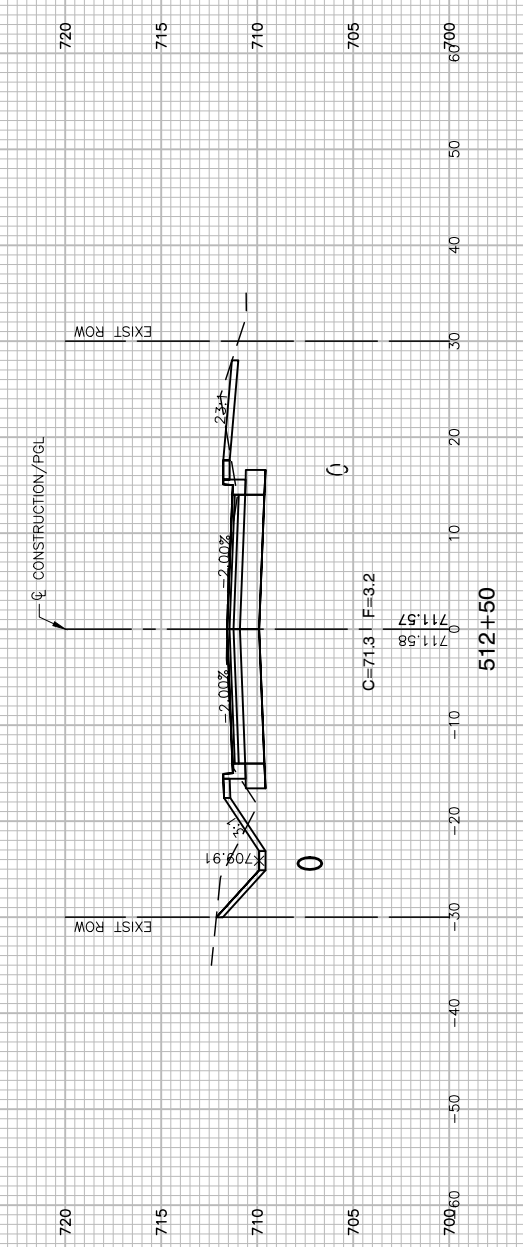
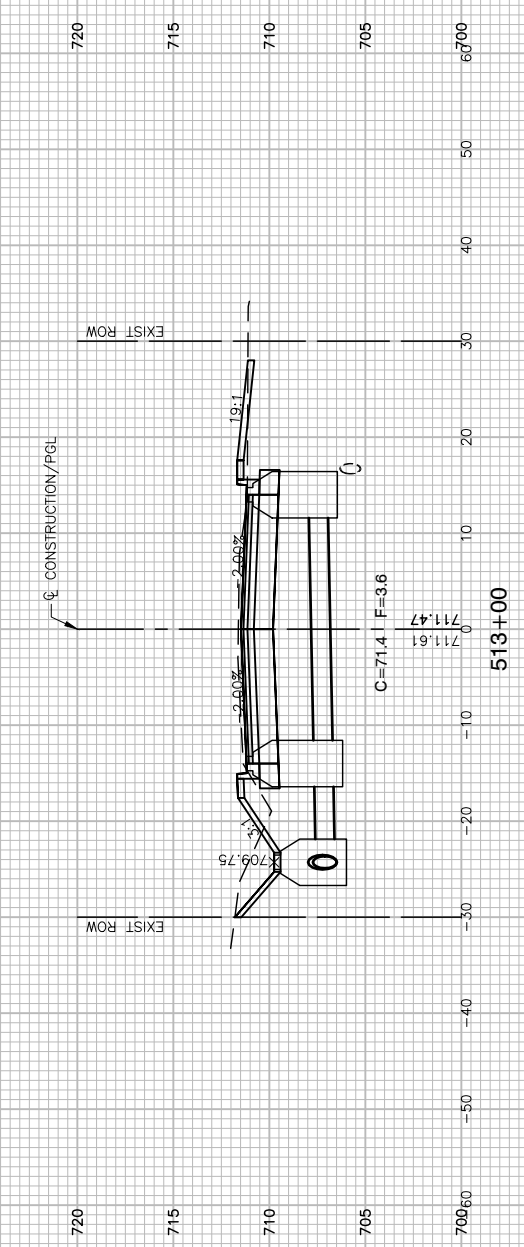
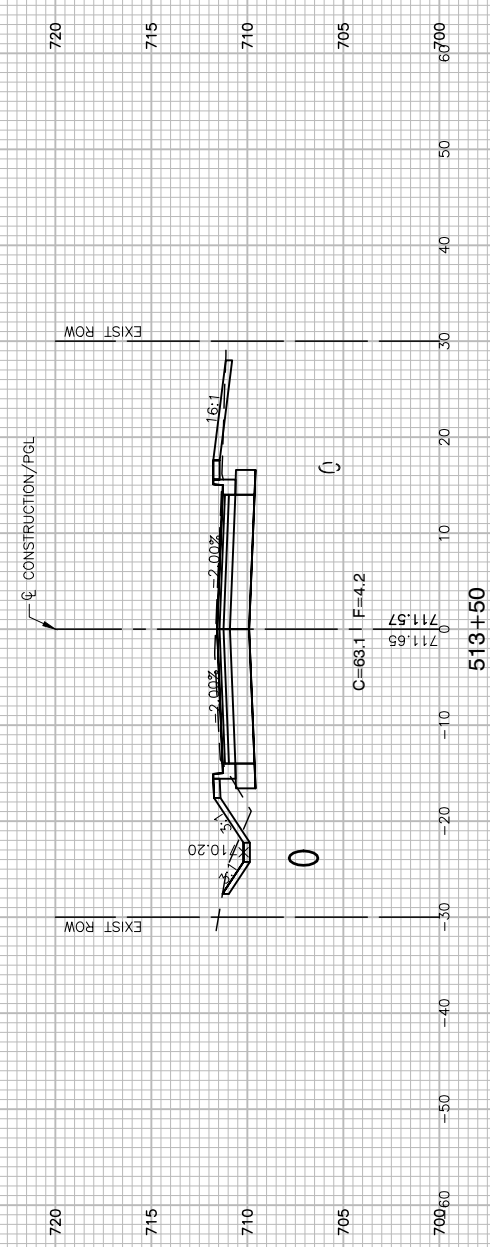
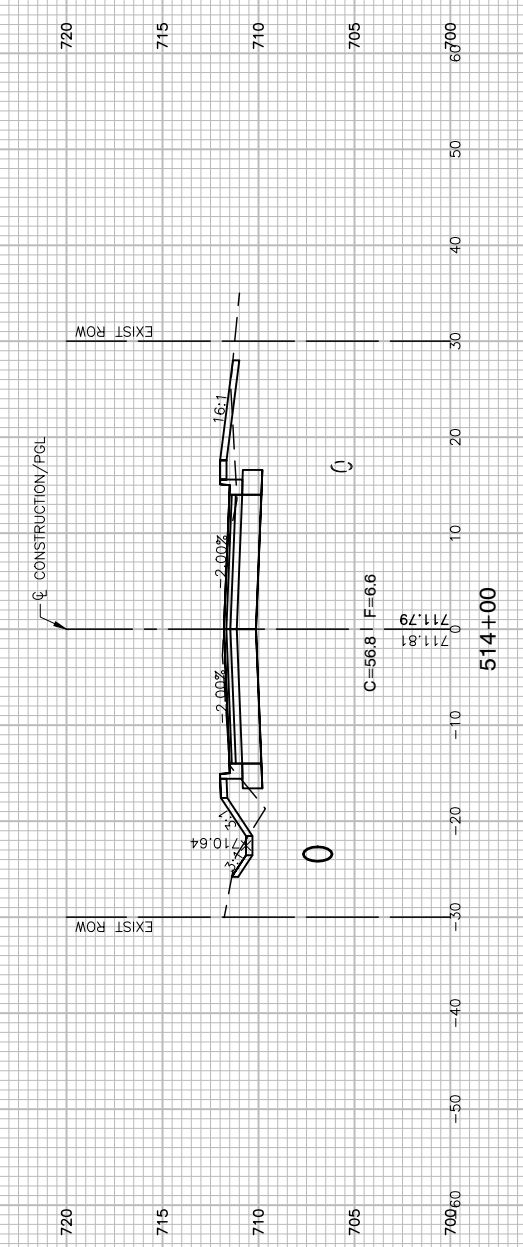
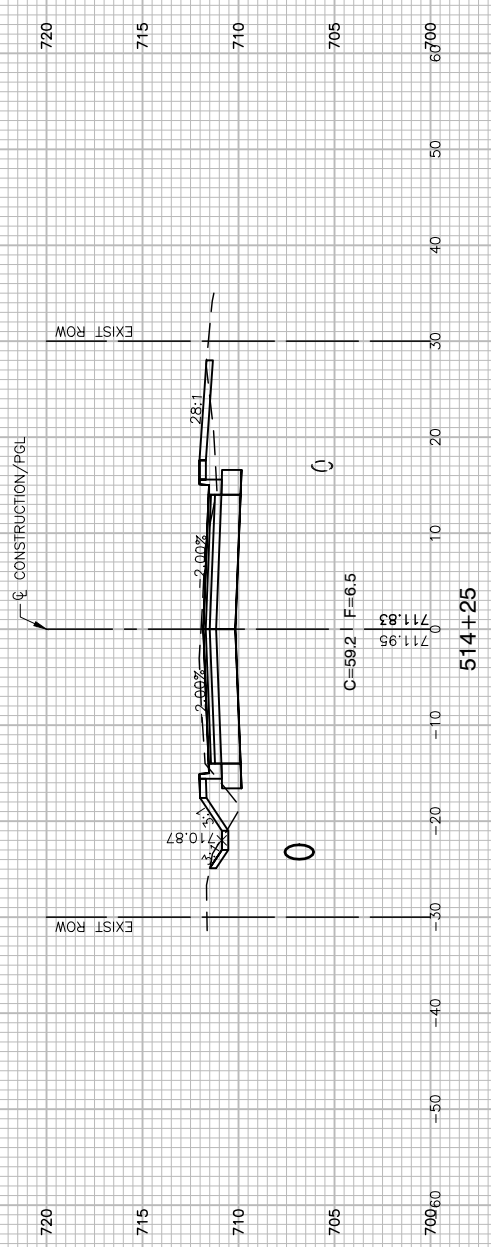


FILE NAME = 21R0316-XSCT-01 - X06	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 38	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 38
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT UMBW(332)	

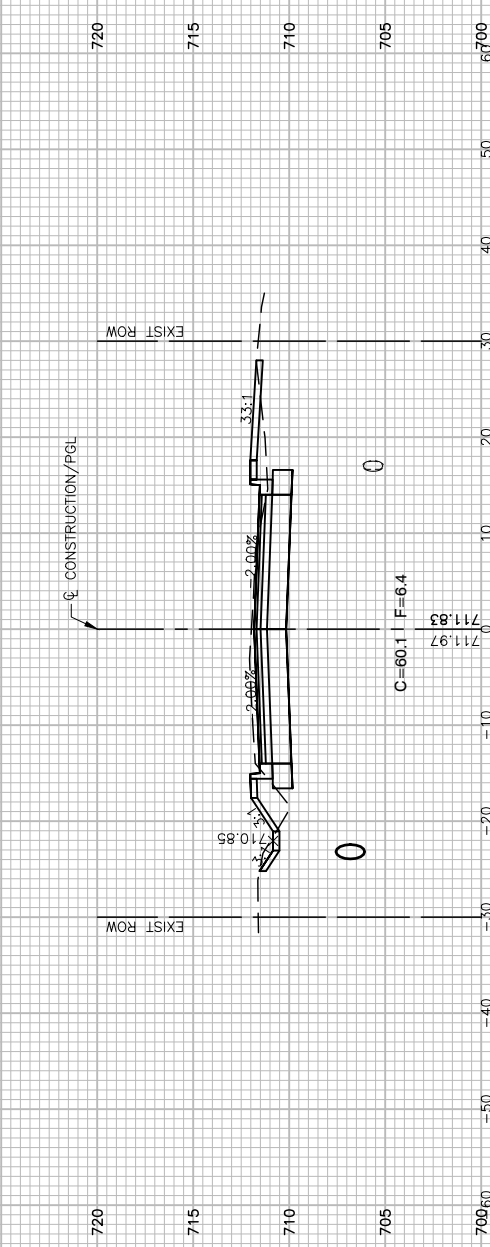
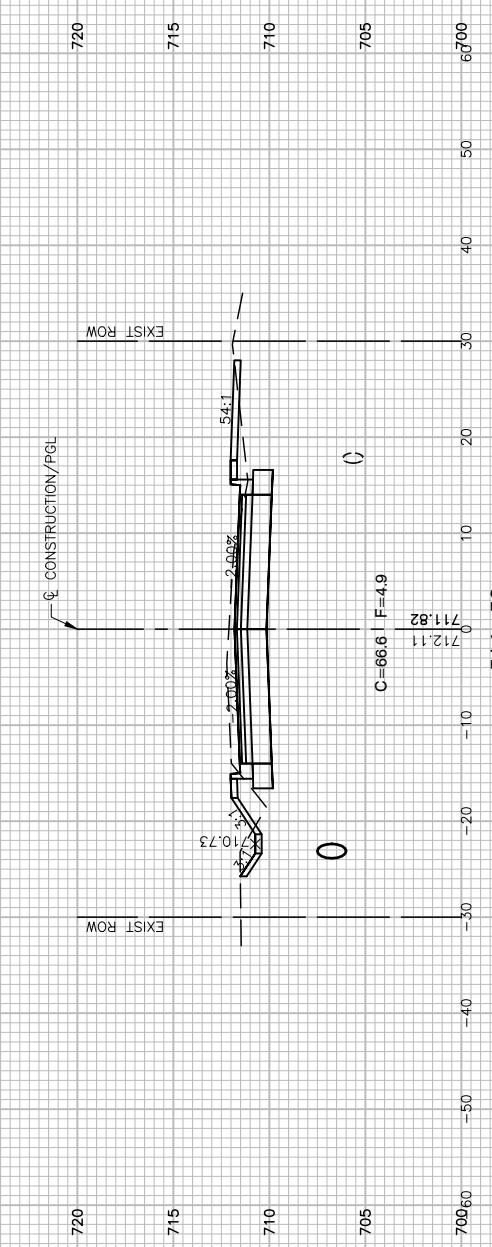
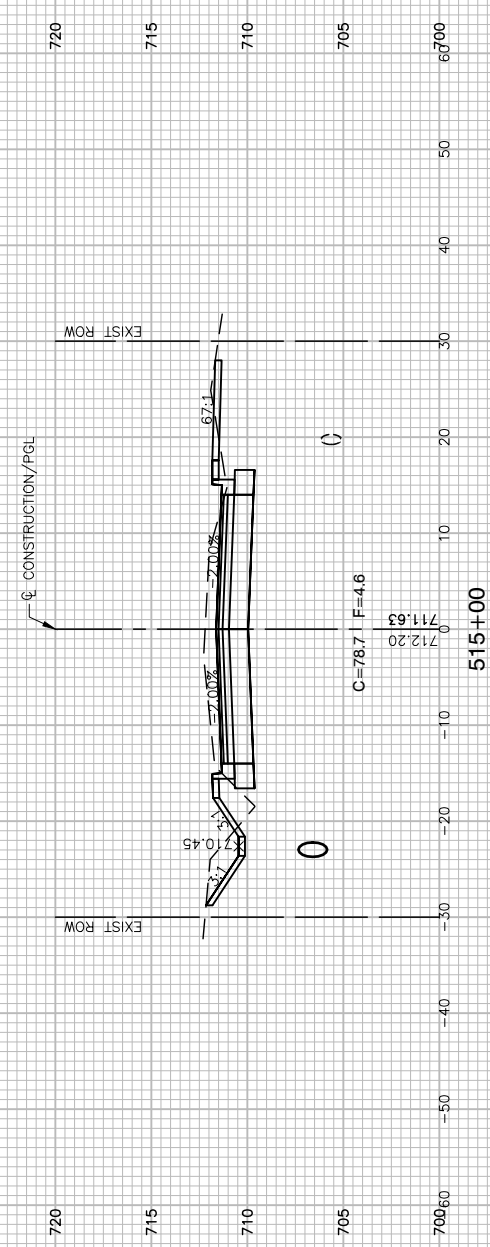
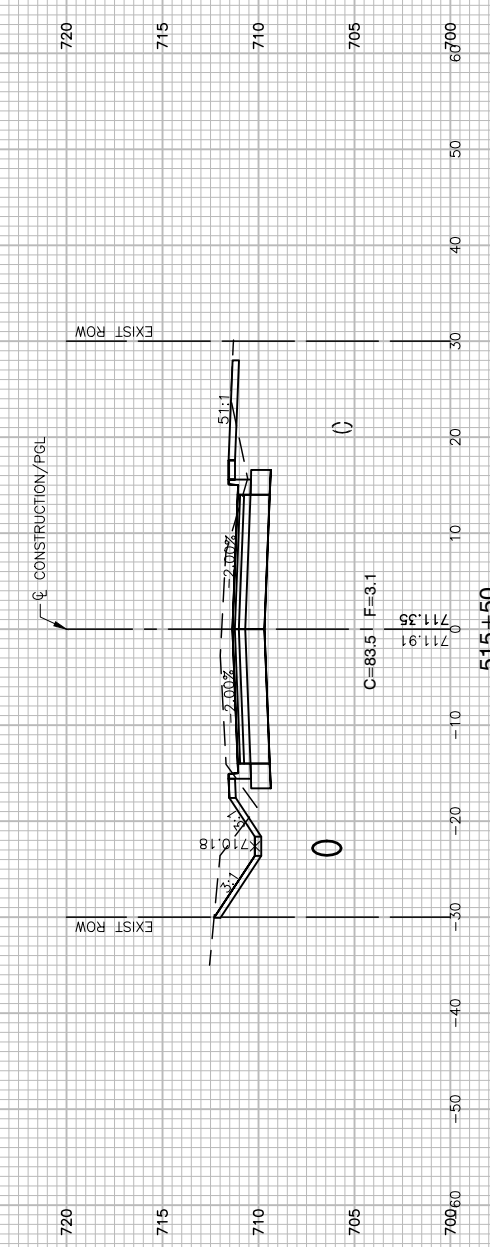
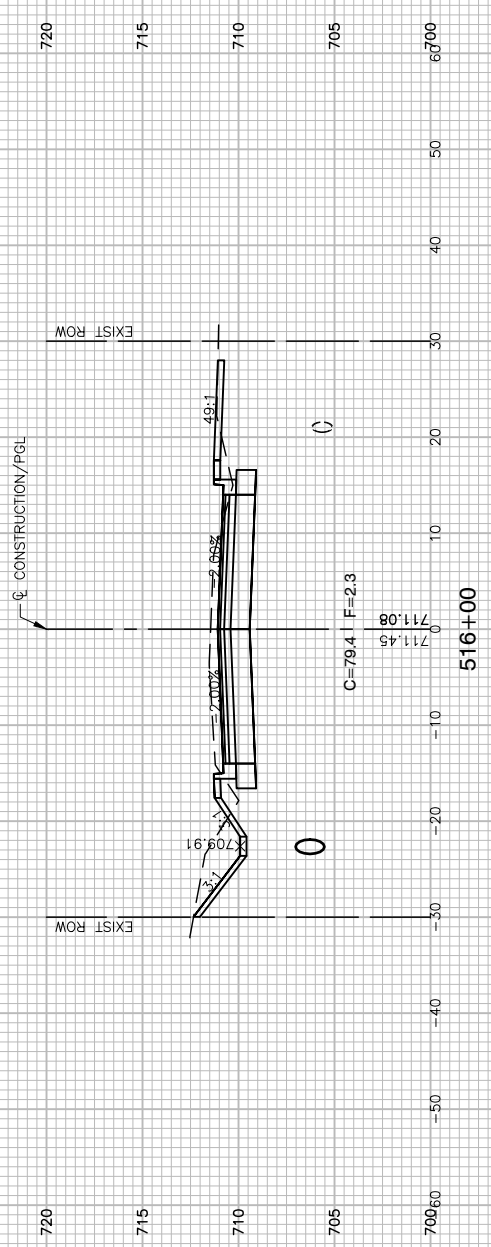


FILE NAME = 21R0316-XSCT-01 - X07	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 39	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	39
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

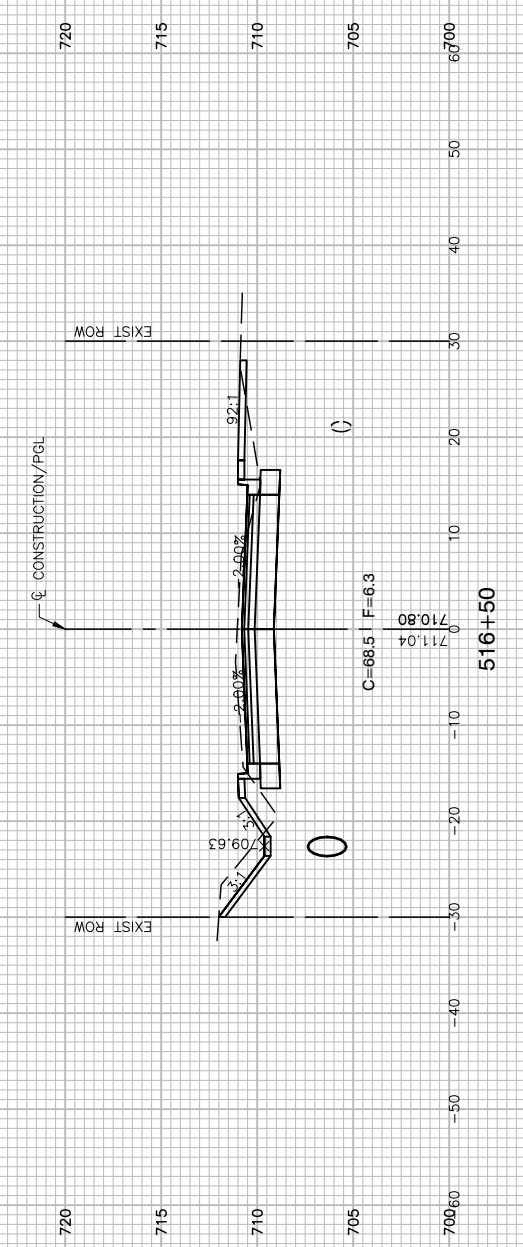
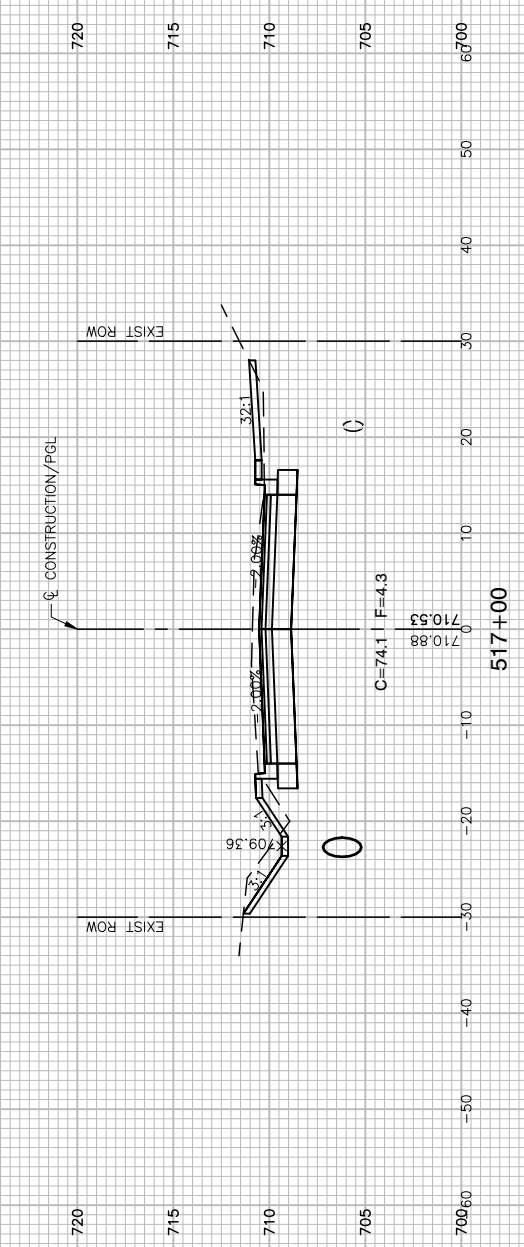
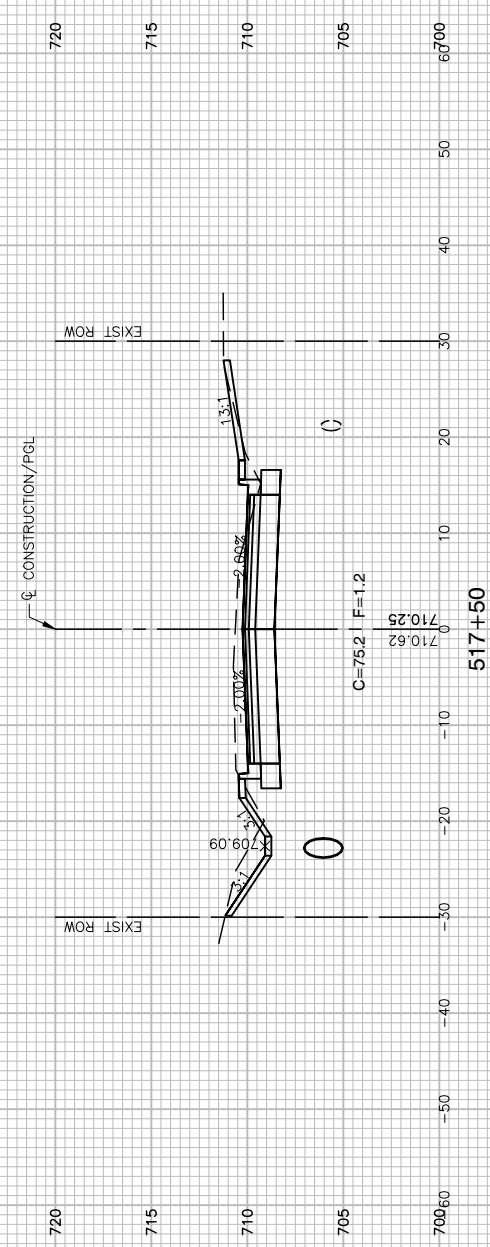
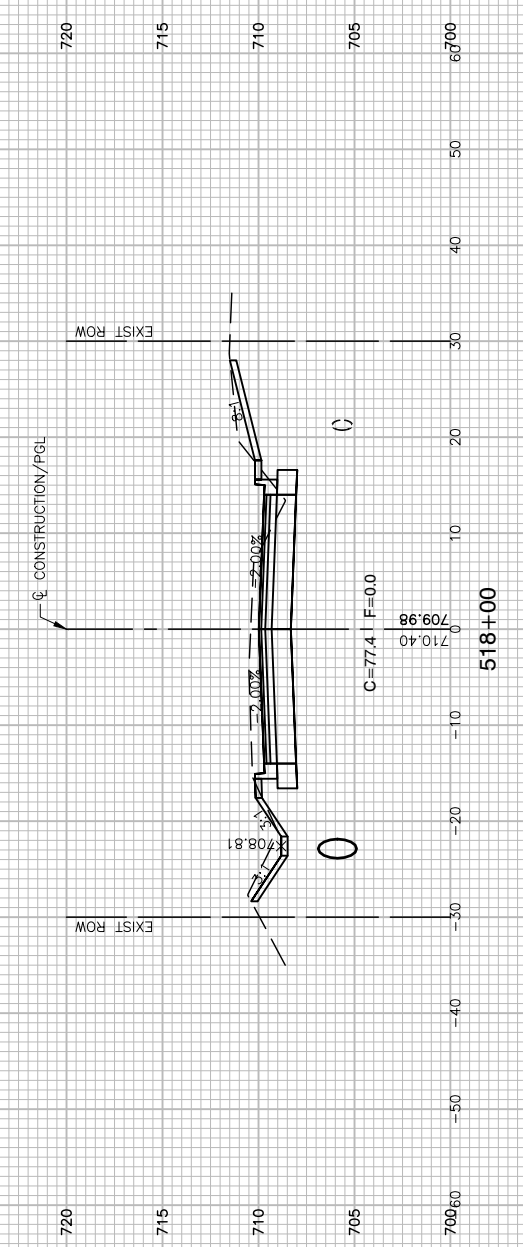
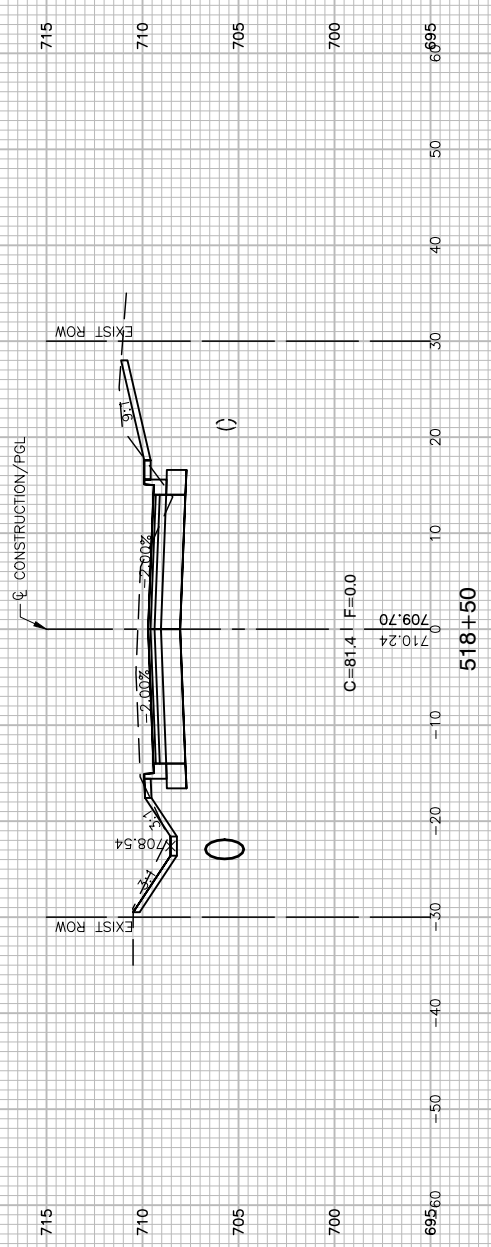


FILE NAME = 21R0316-XSCT-01 - X08	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 40	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	40
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

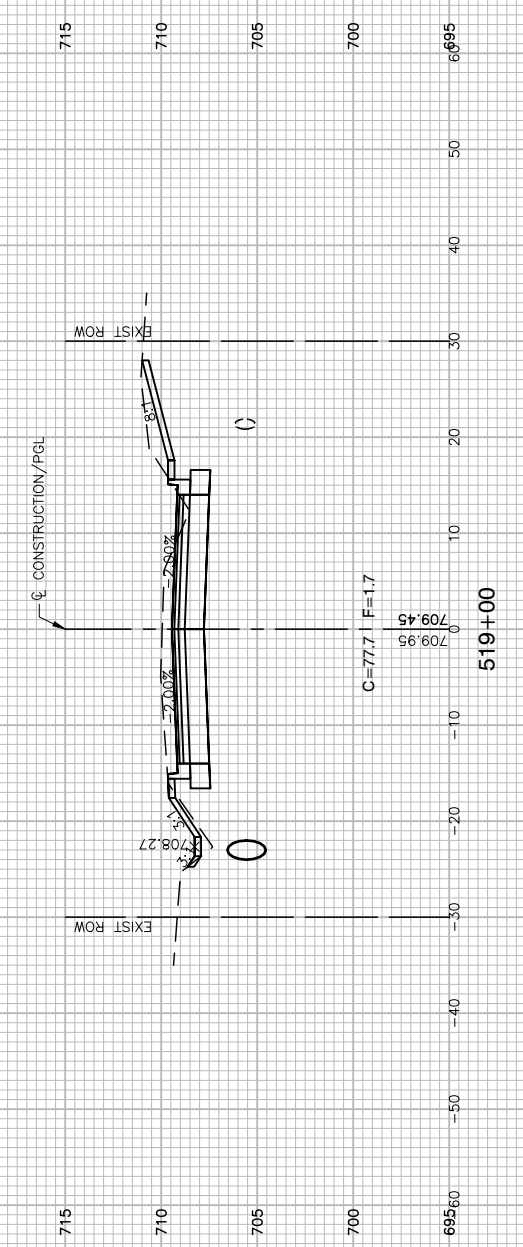
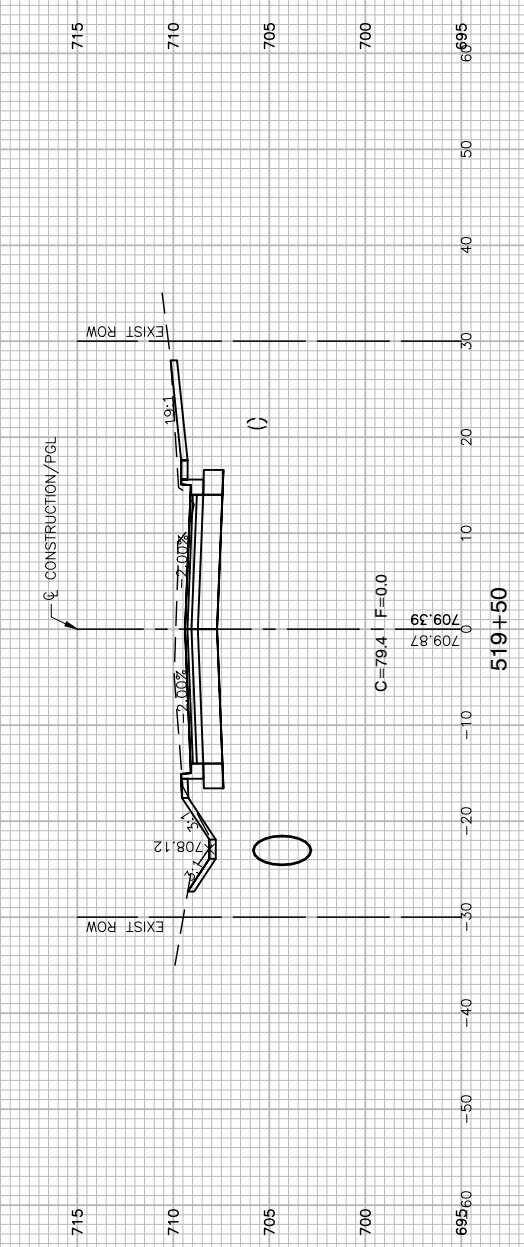
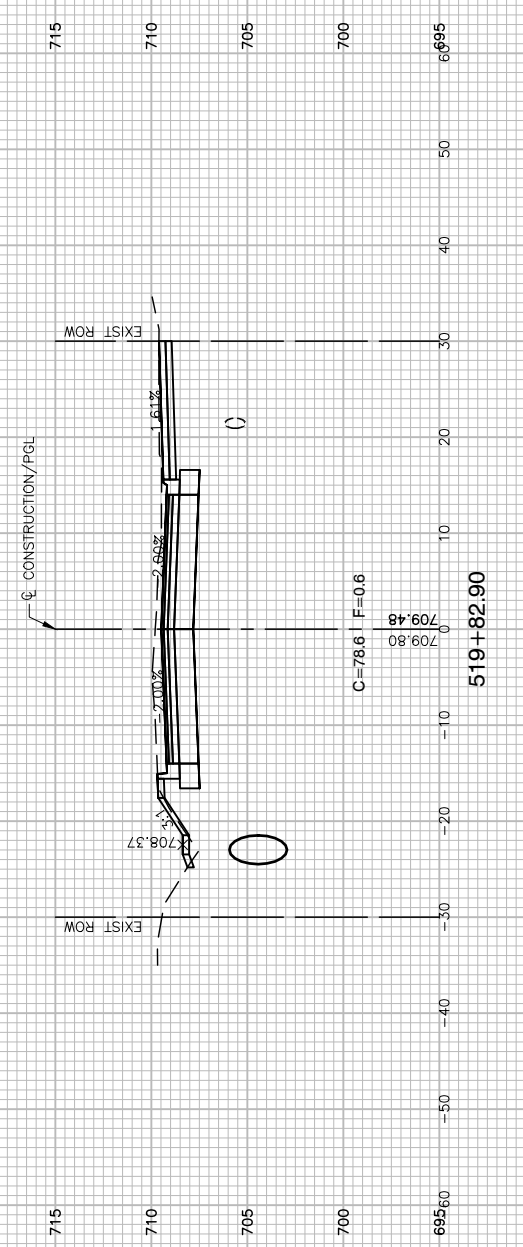
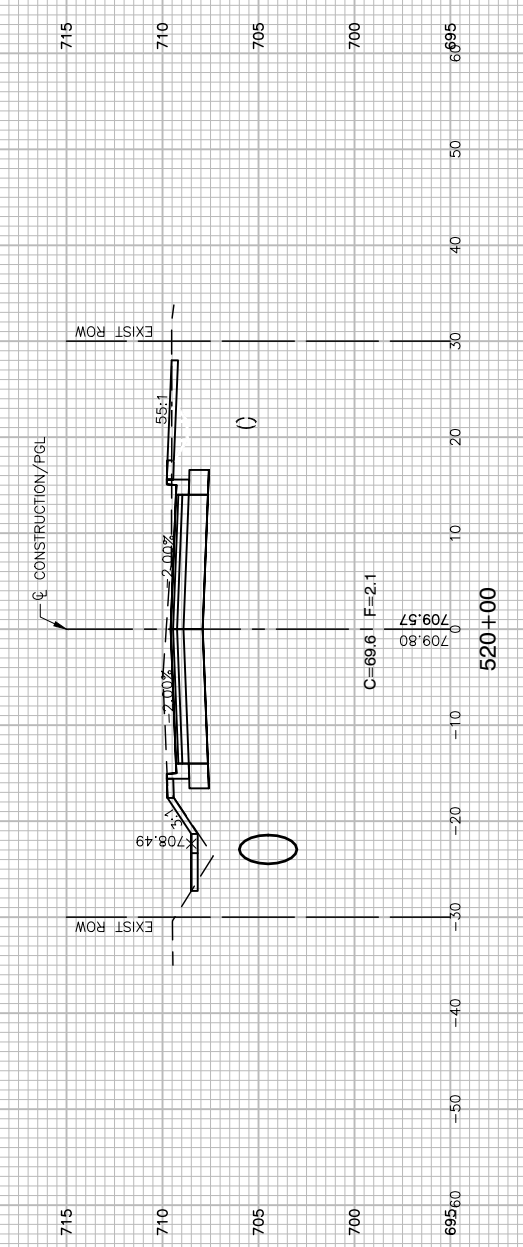
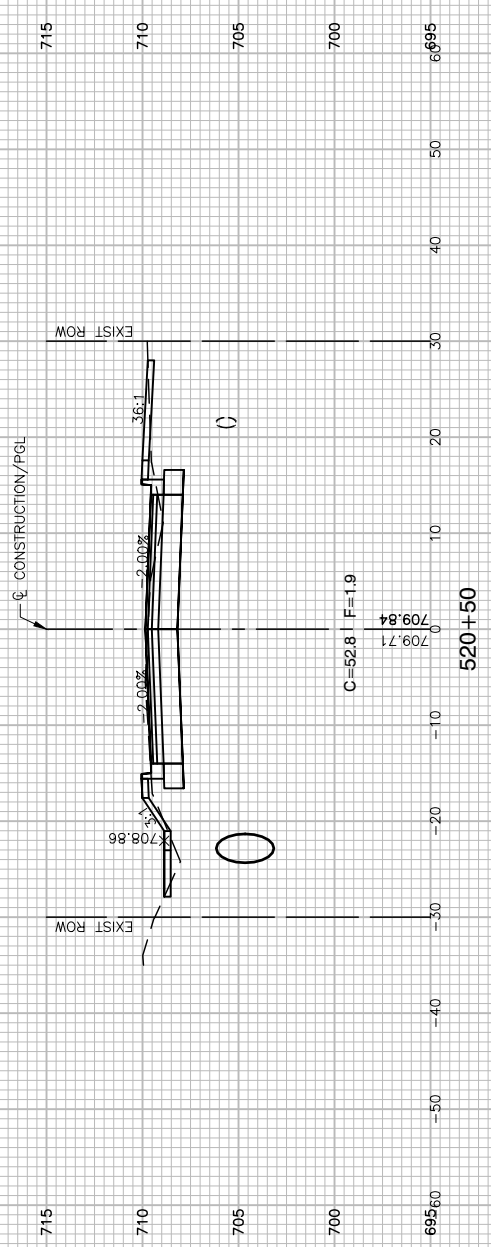


FILE NAME = 21R0316-XSCT-01 - X09	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 41	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE. 4309	SECTION 15-00049-00-PV	COUNTY WILL	TOTAL SHEETS 52	SHEET NO. 41
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT UMBW(332)	



FILE NAME = 21R0316-XSCT-01 - X10

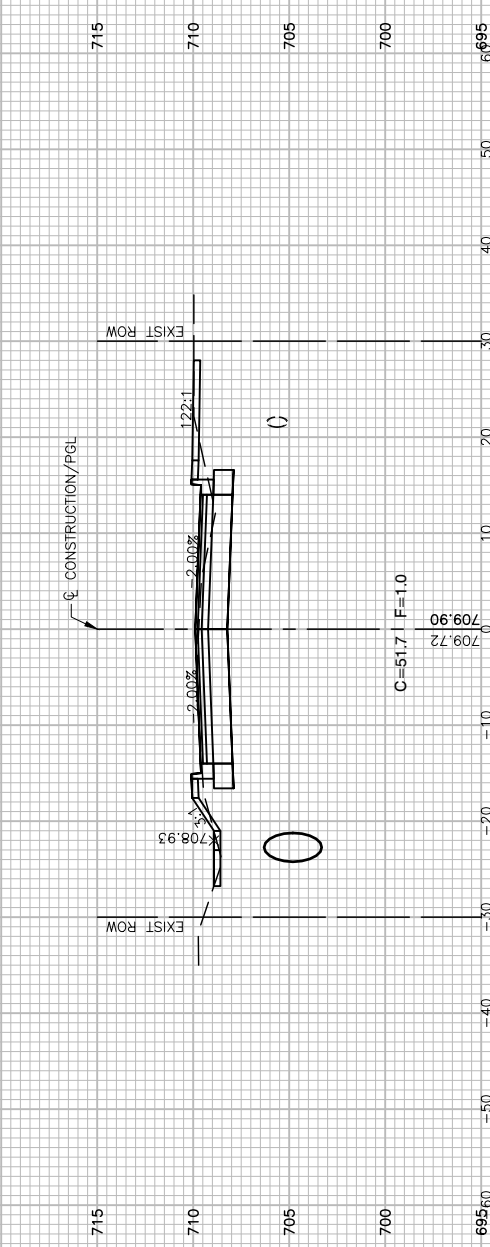
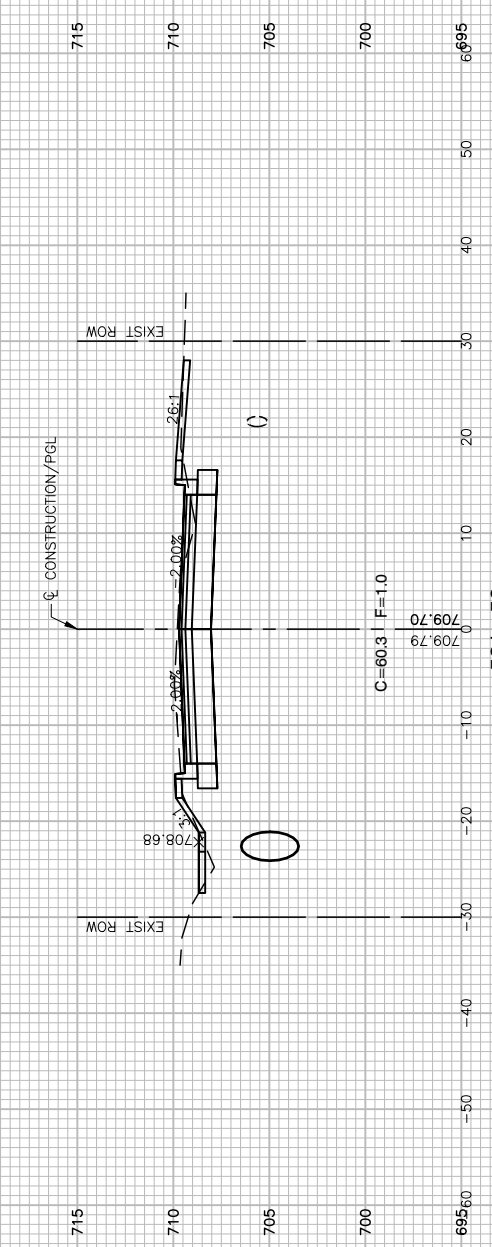
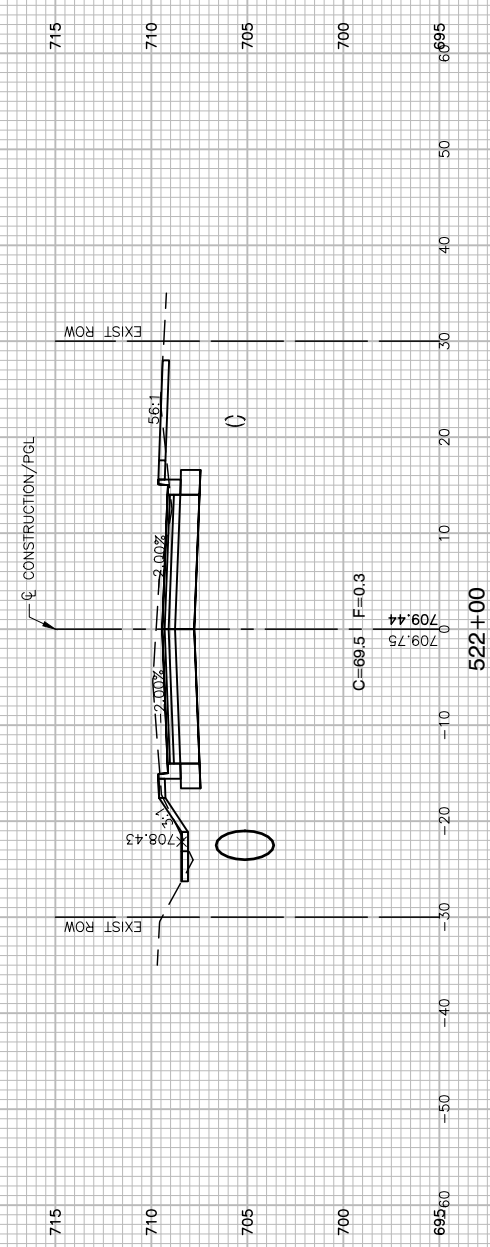
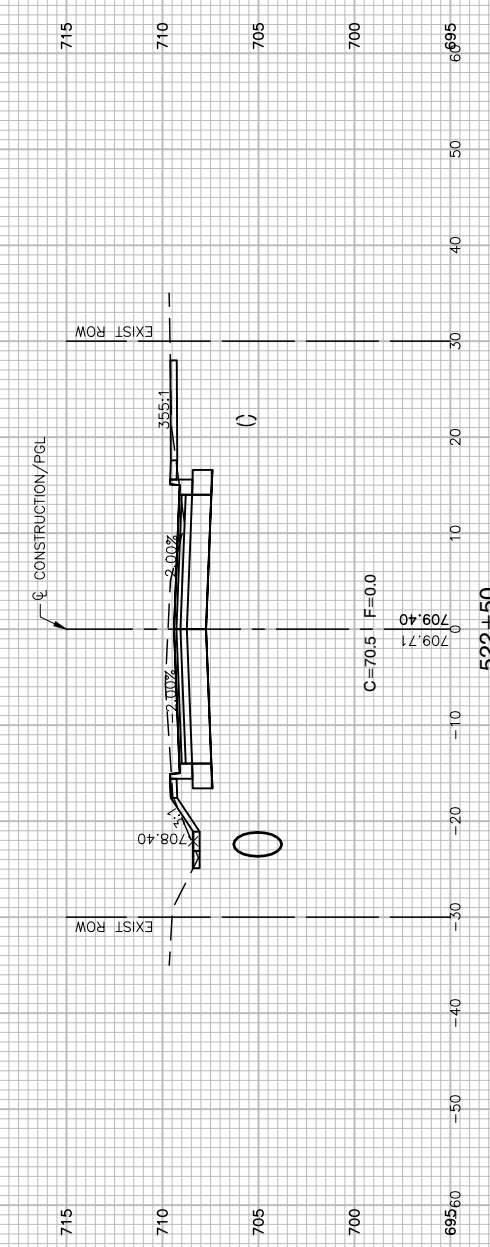
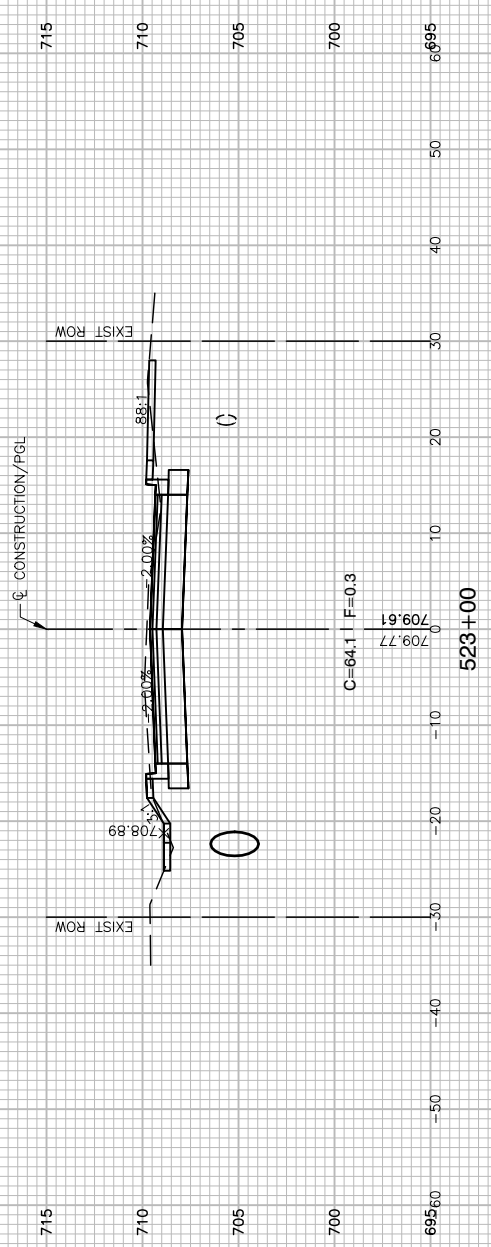
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 42 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	42
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



FILE NAME = 21R0316-XSCT-01 - X11

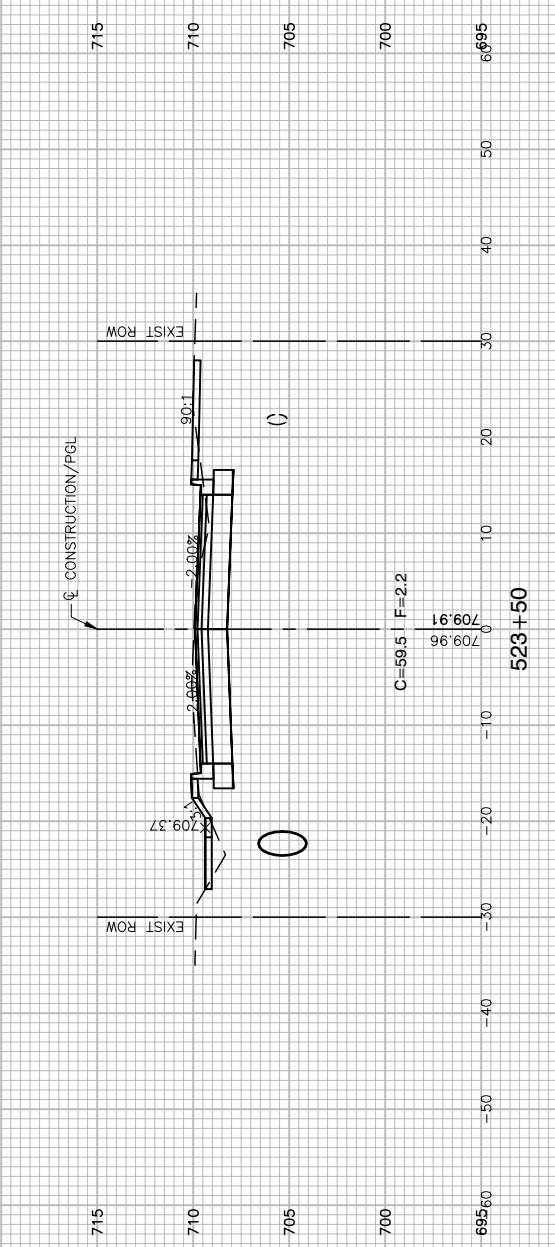
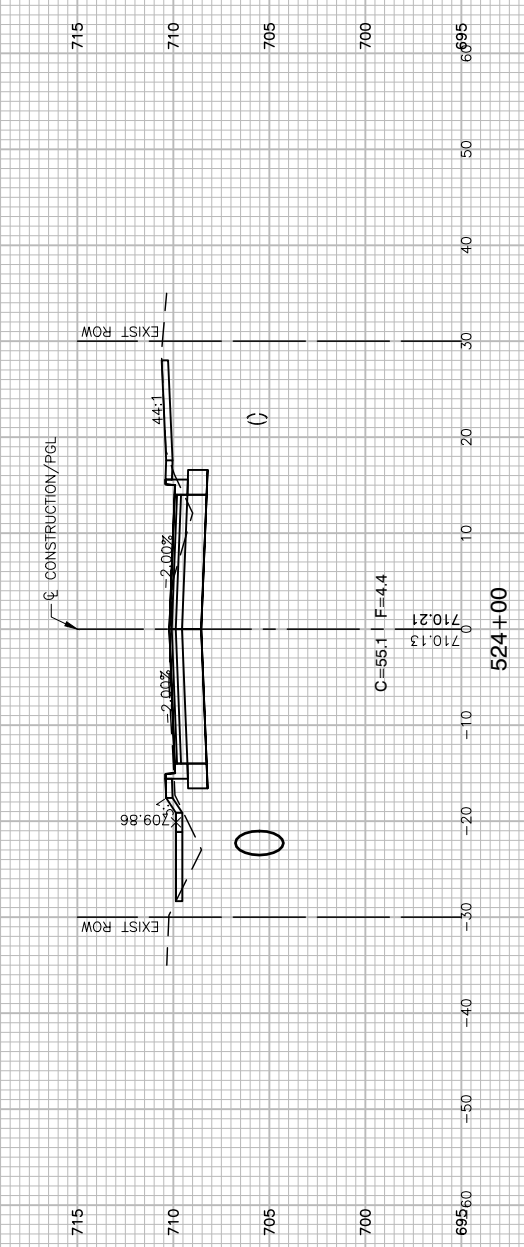
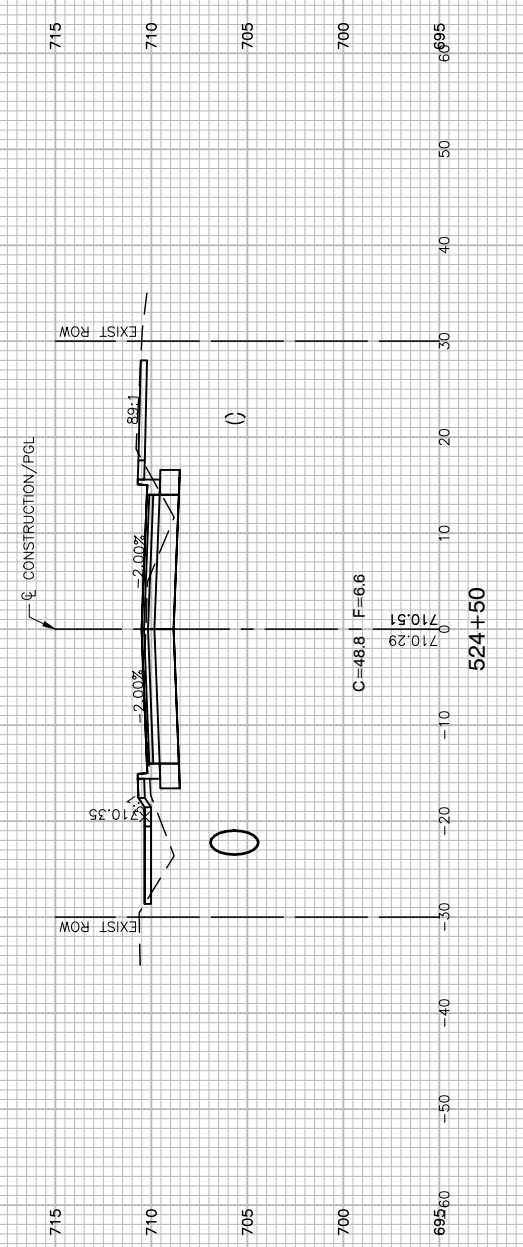
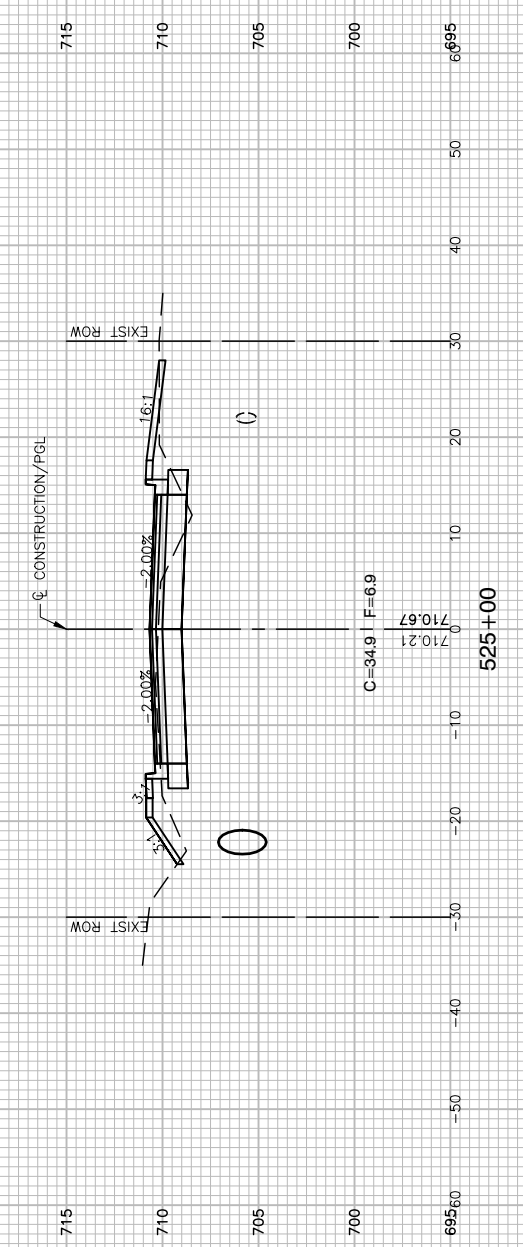
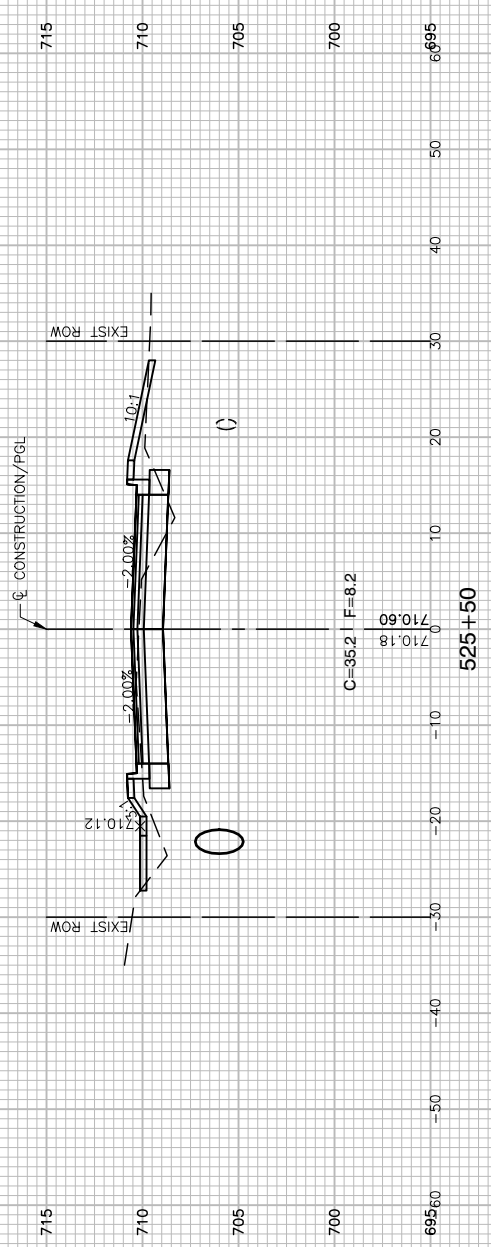
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 43 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	43
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



FILE NAME = 21R0316-XSCT-01 - X12

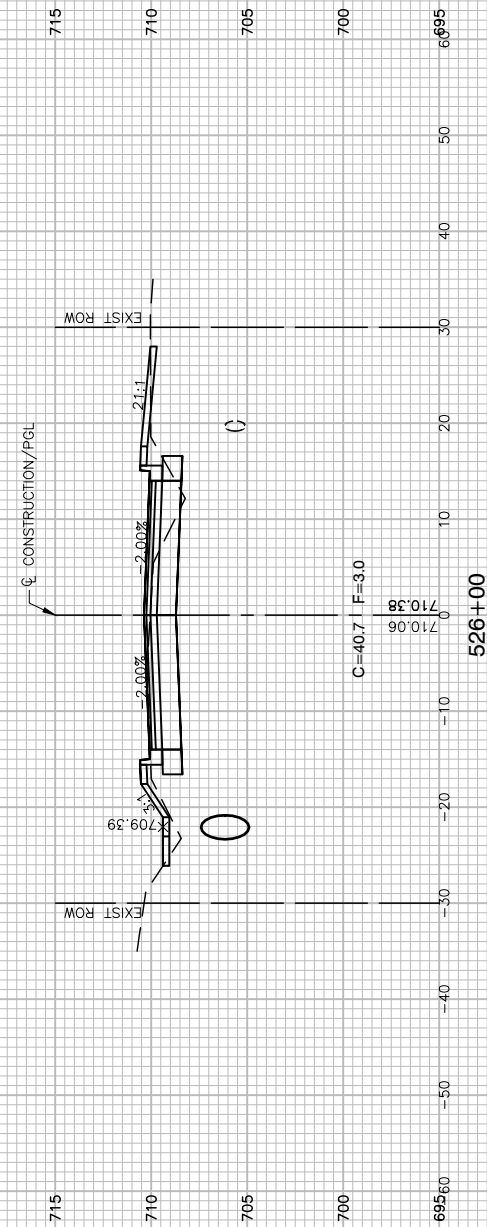
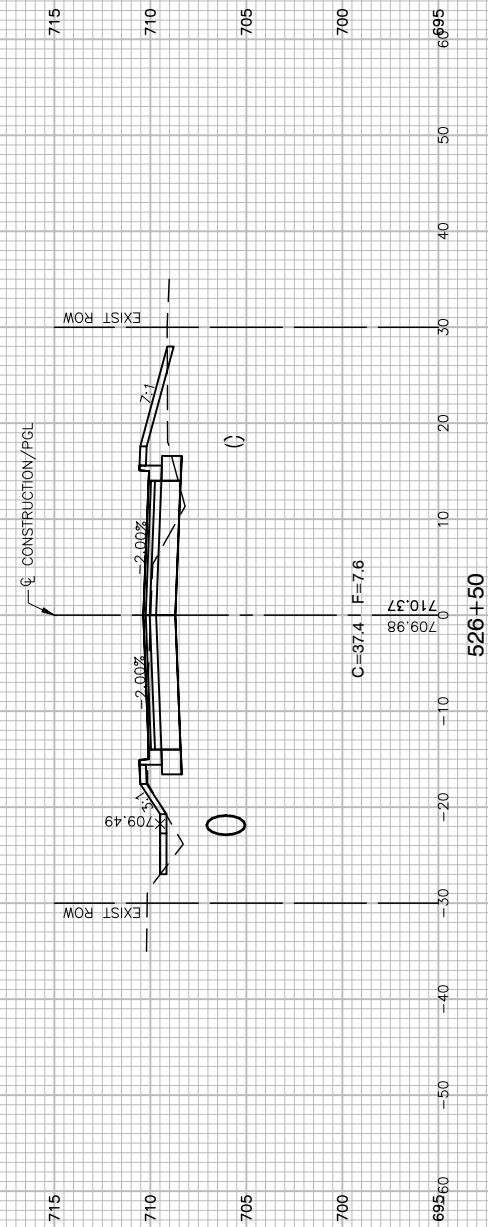
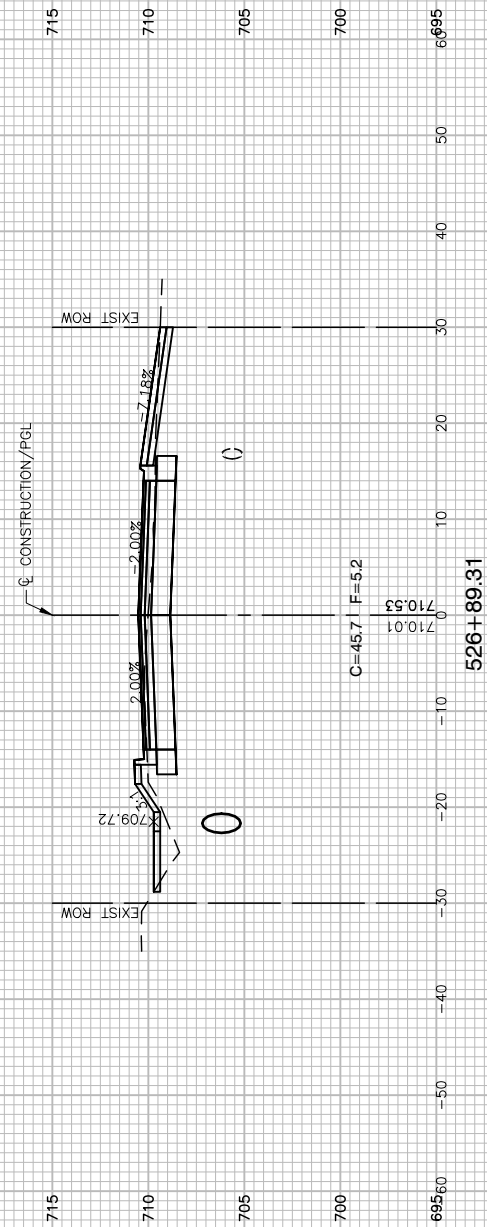
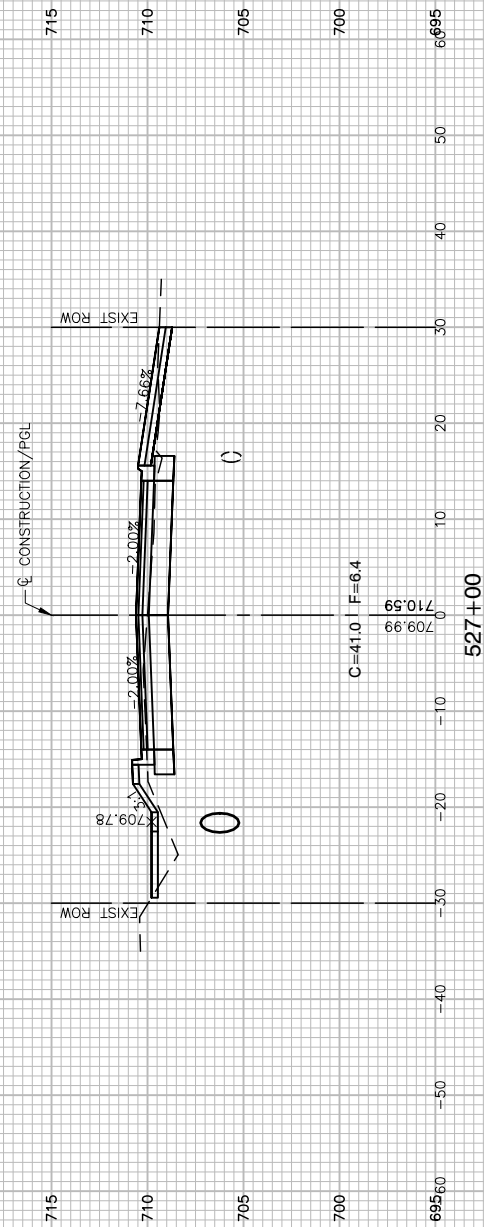
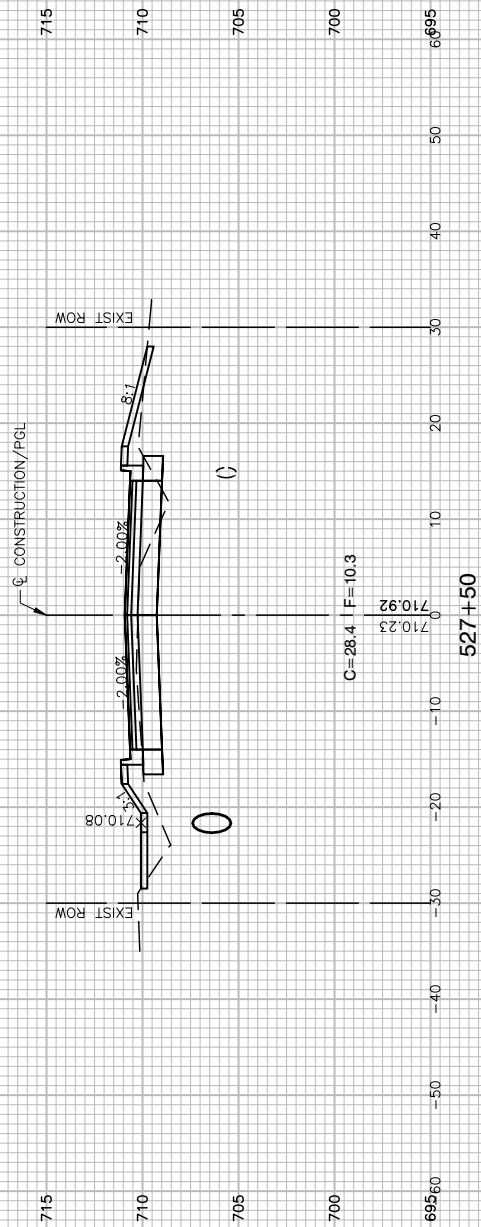
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 44 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	44
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



FILE NAME = 21R0316-XSCT-01 - X13

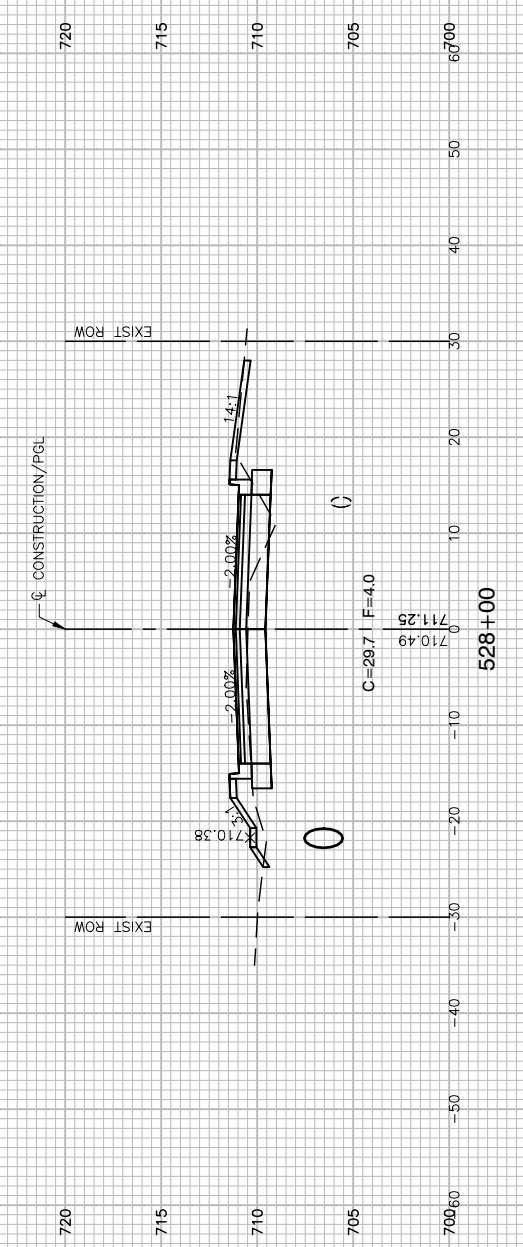
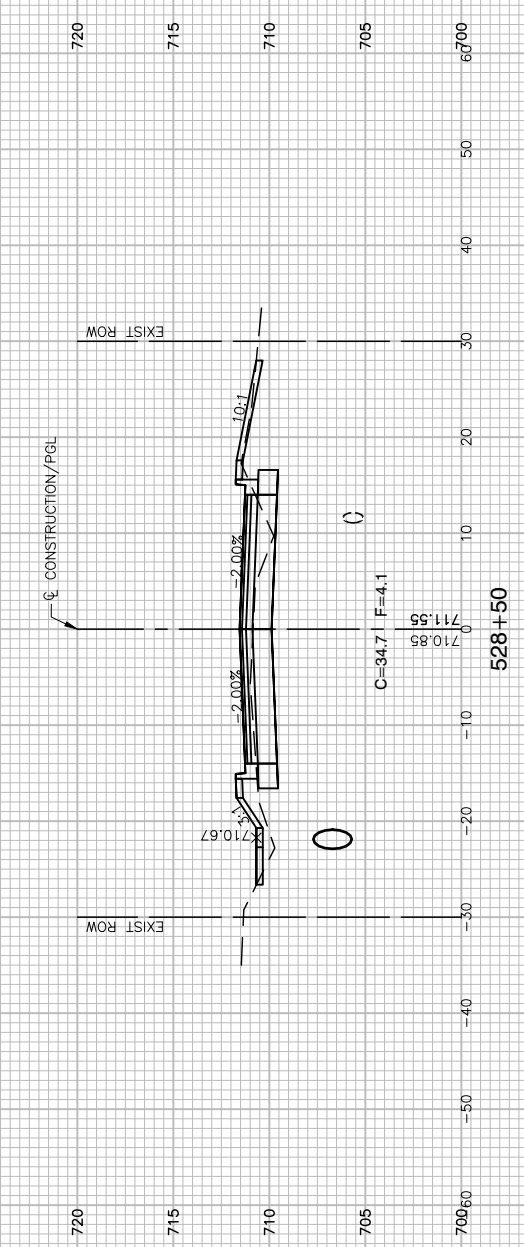
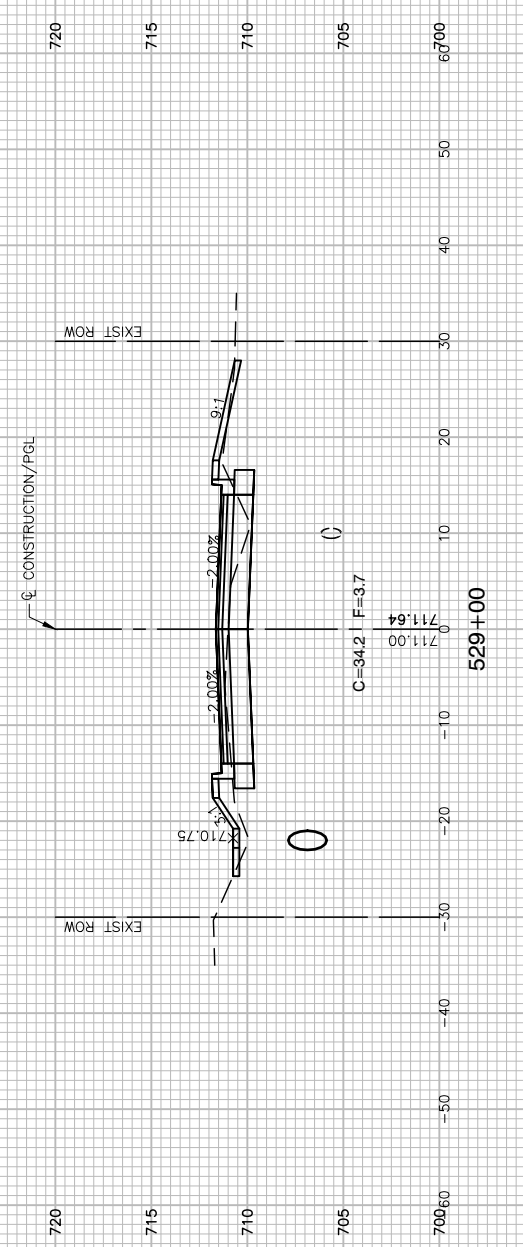
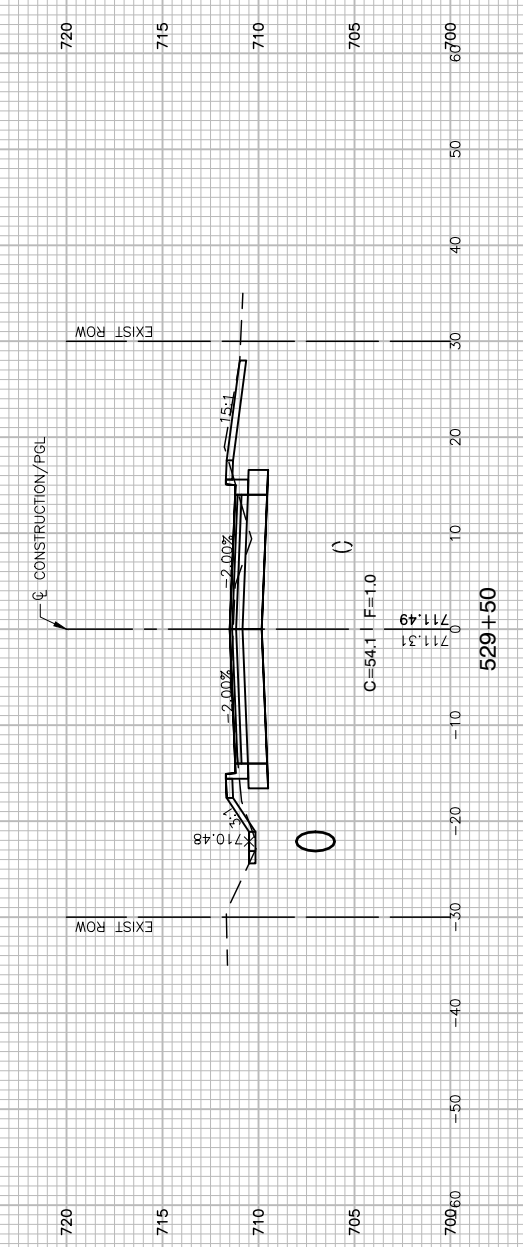
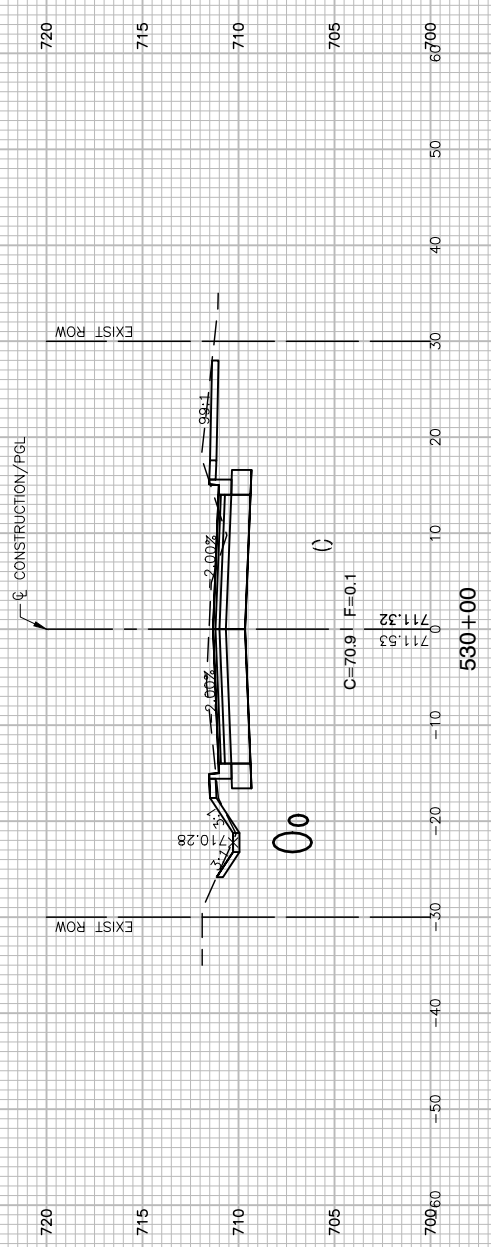
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 45 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	45
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)			CONTRACT NO. 61H99	

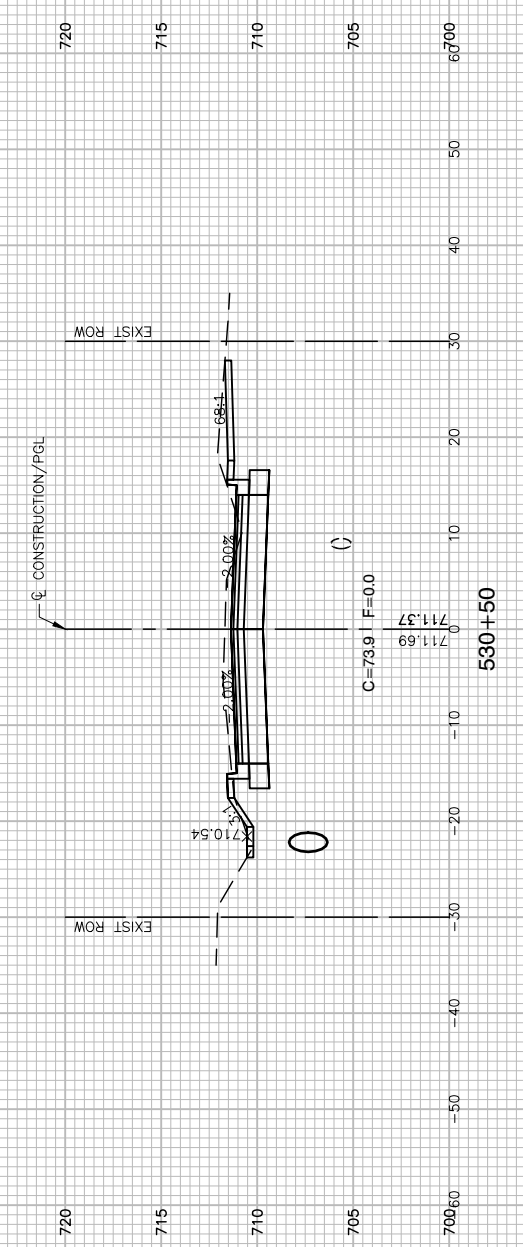
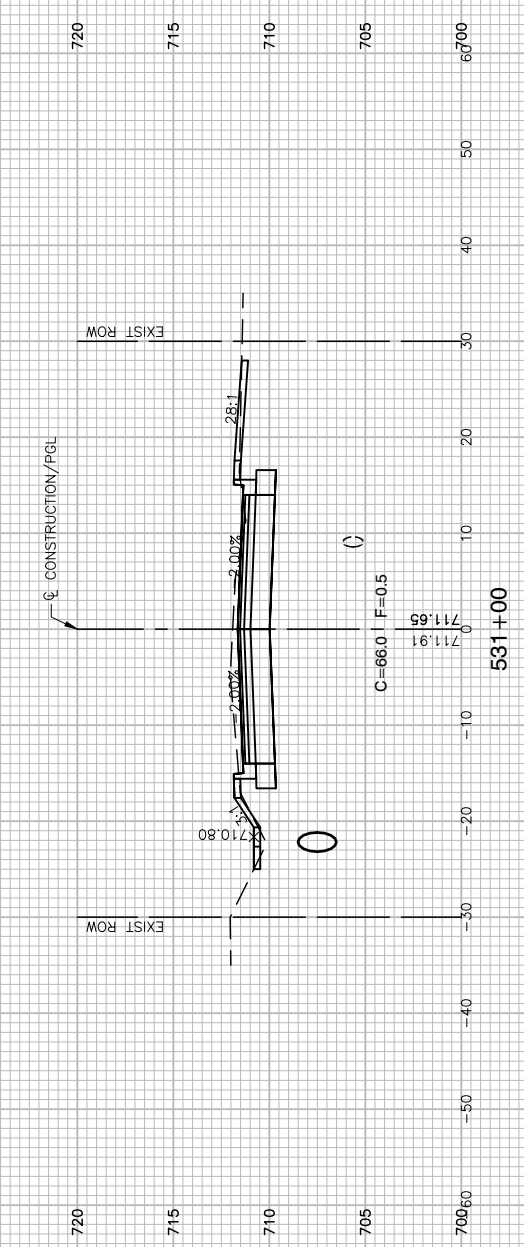
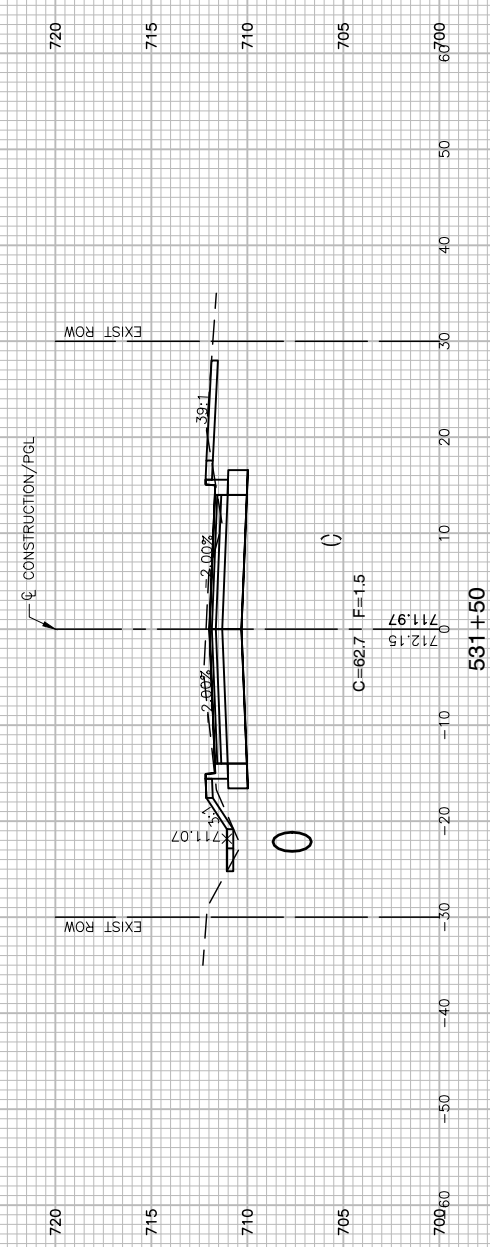
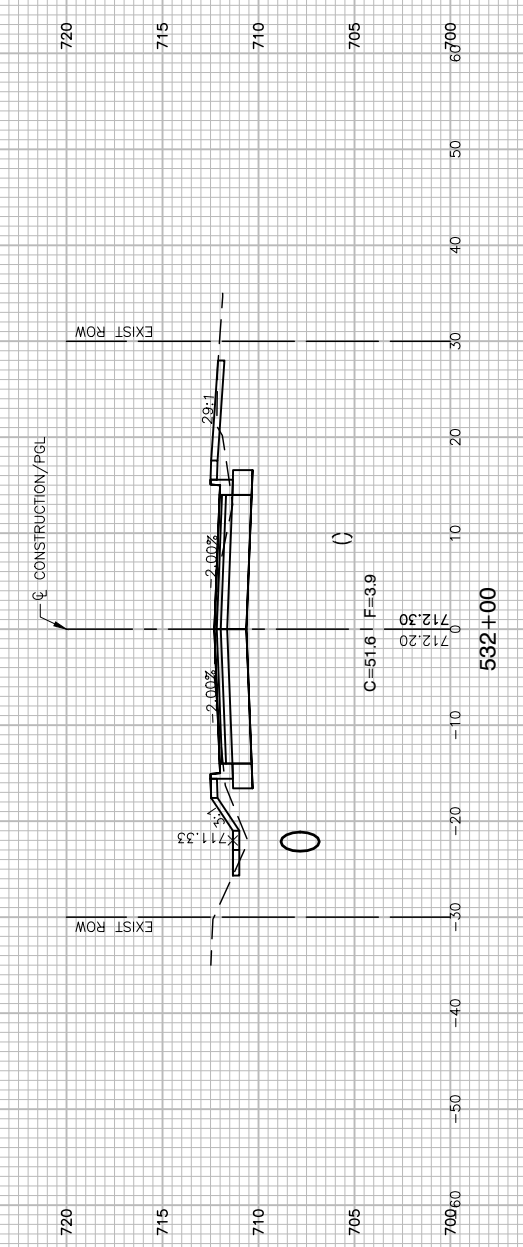
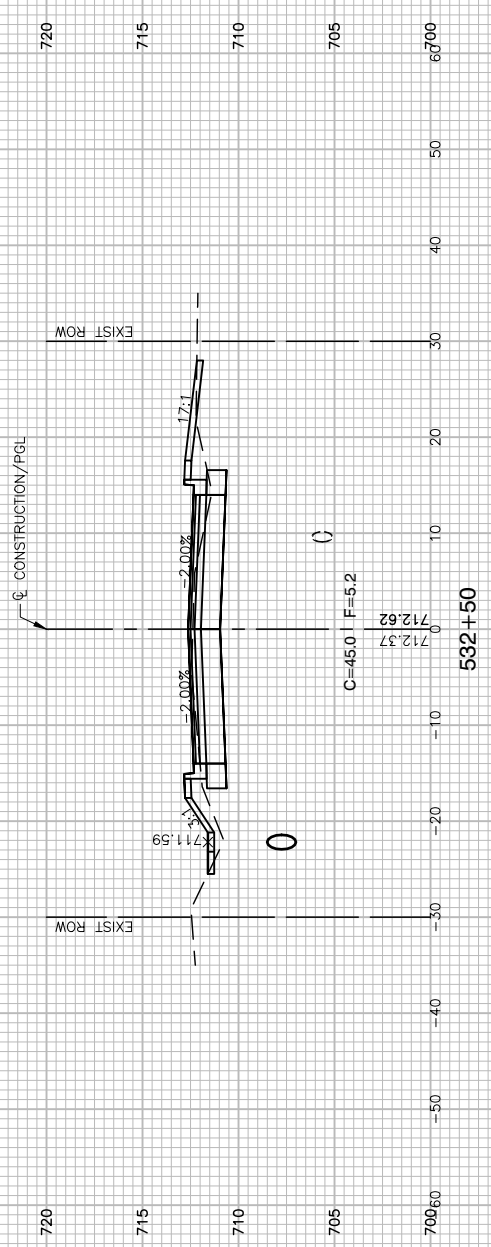


FILE NAME = 21R0316-XSCT-01 - X14	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 46	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	46
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



FILE NAME = 21R0316-XSCT-01 - X15

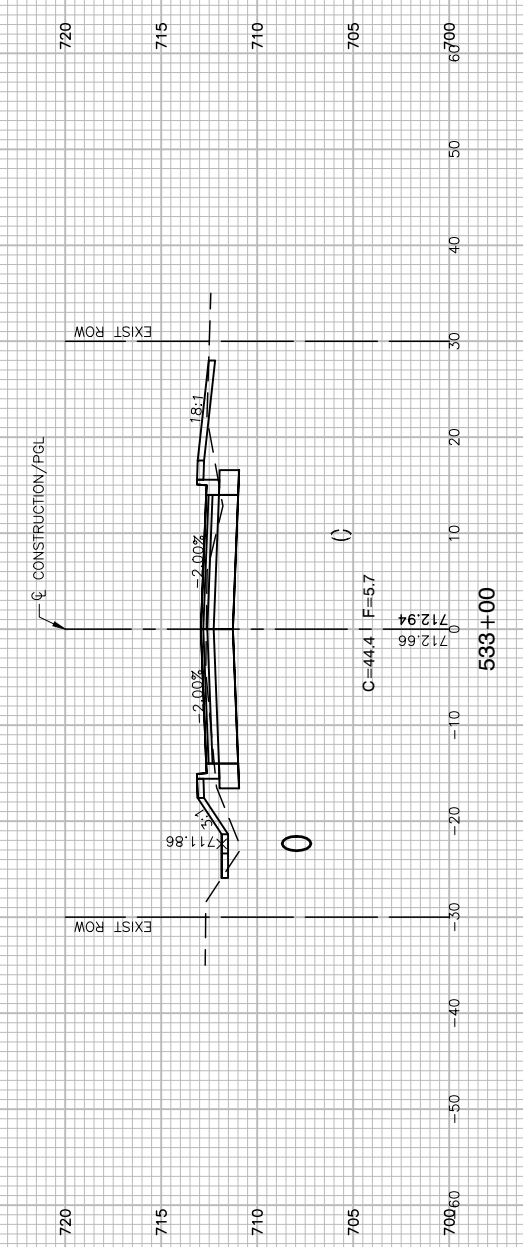
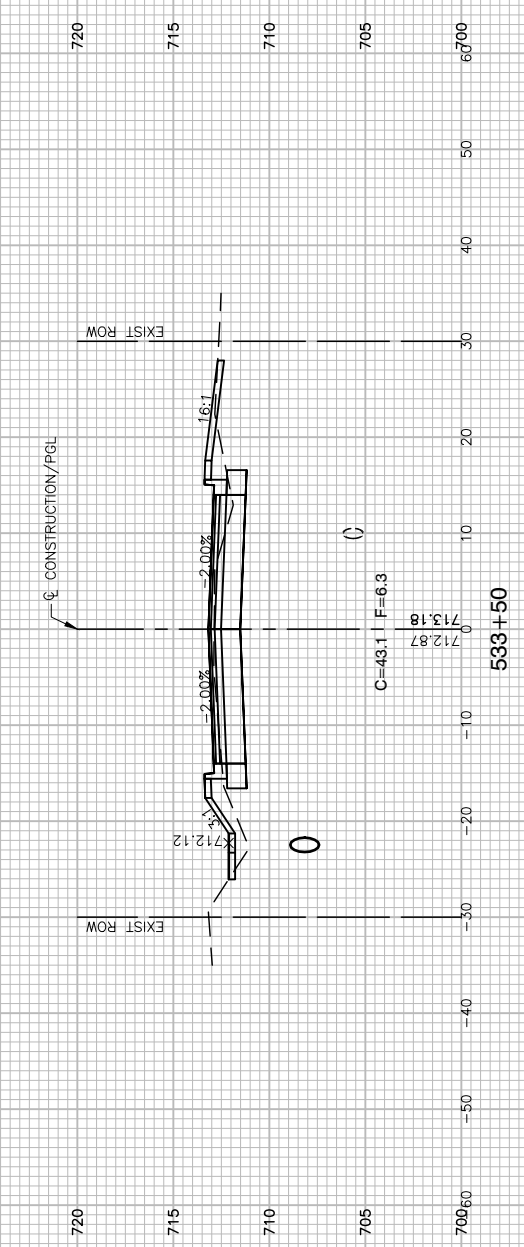
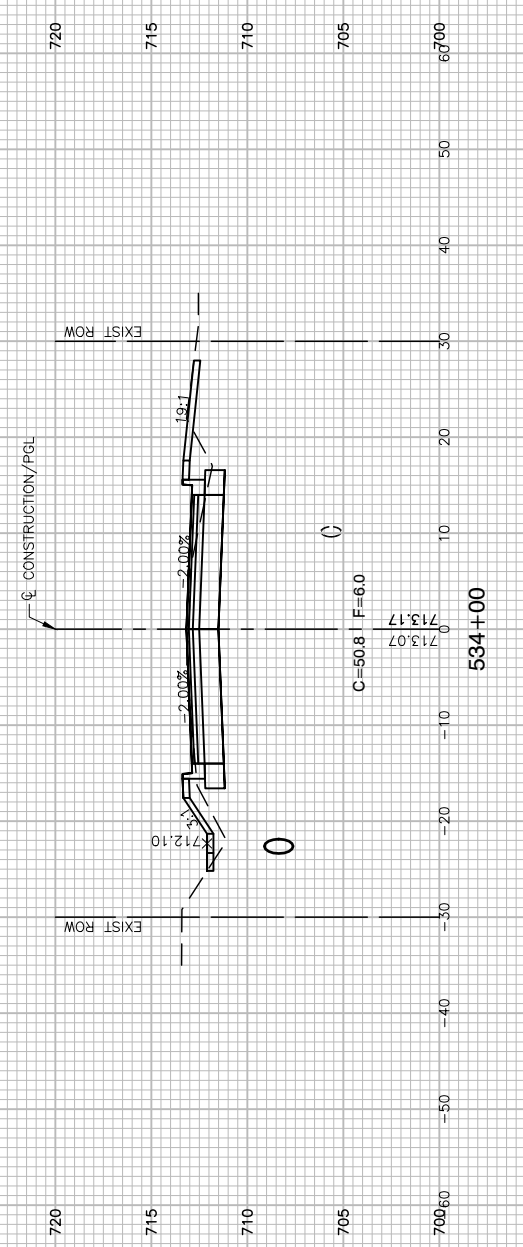
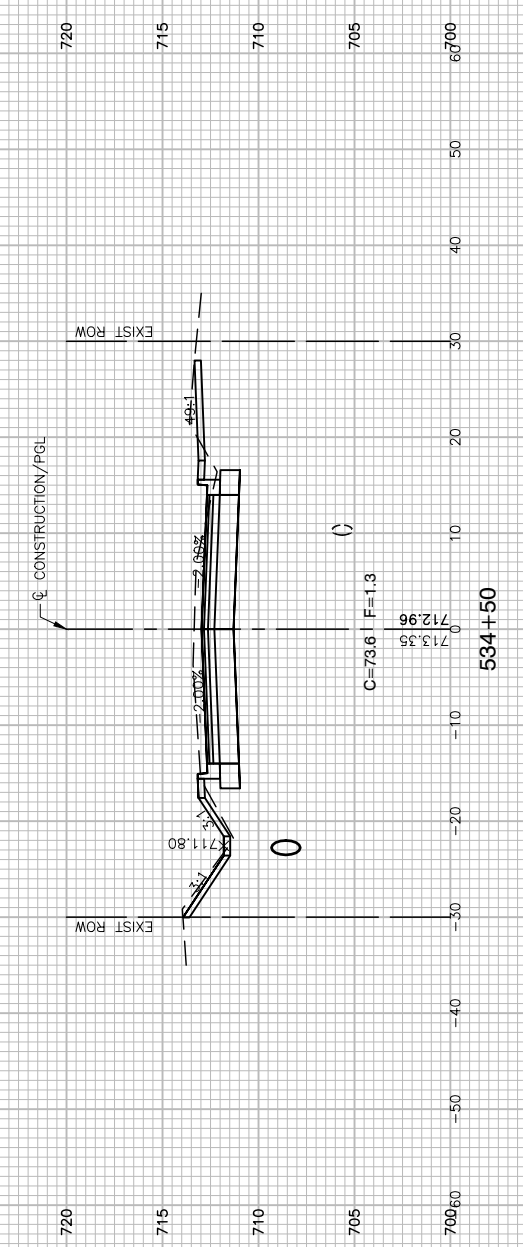
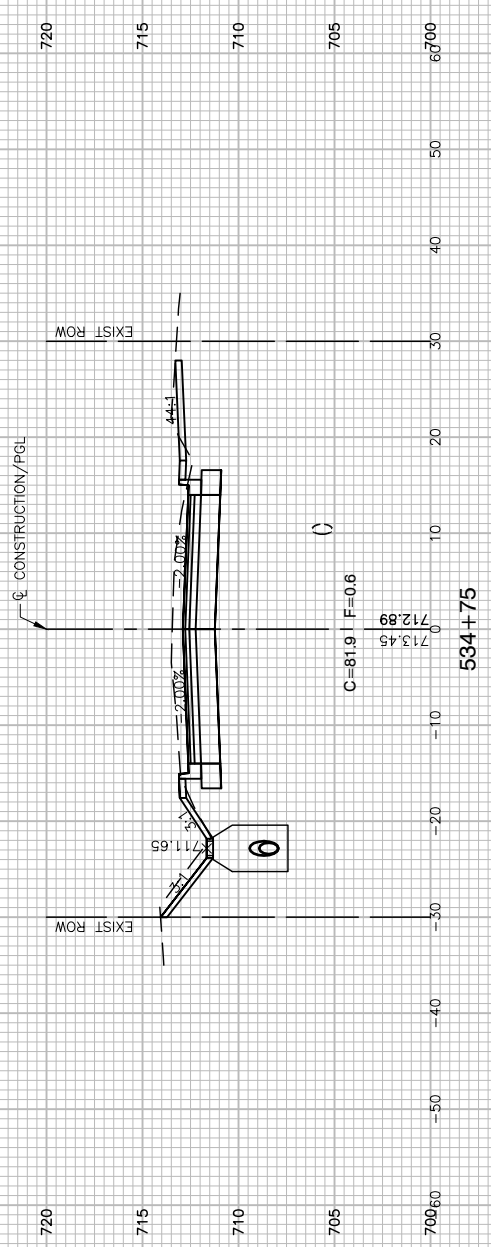
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 47 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	47
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



FILE NAME = 21R0316-XSCT-01 - X16

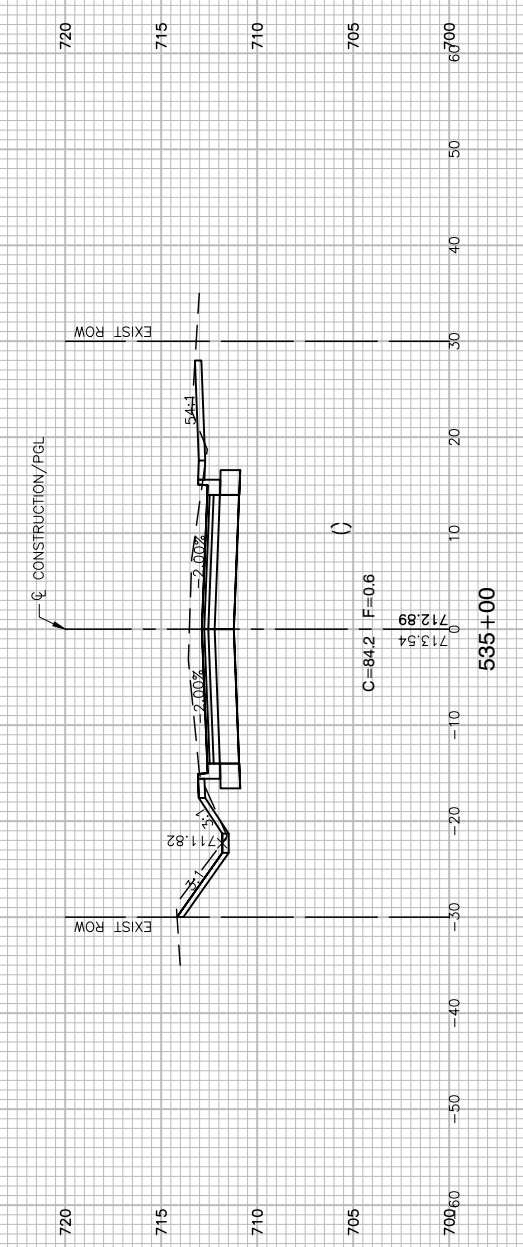
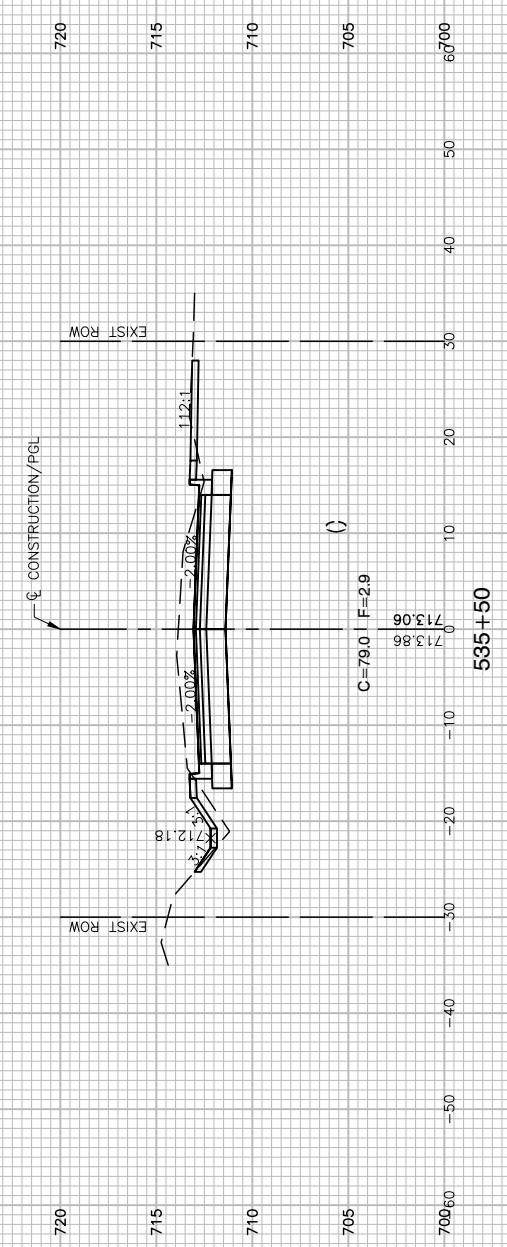
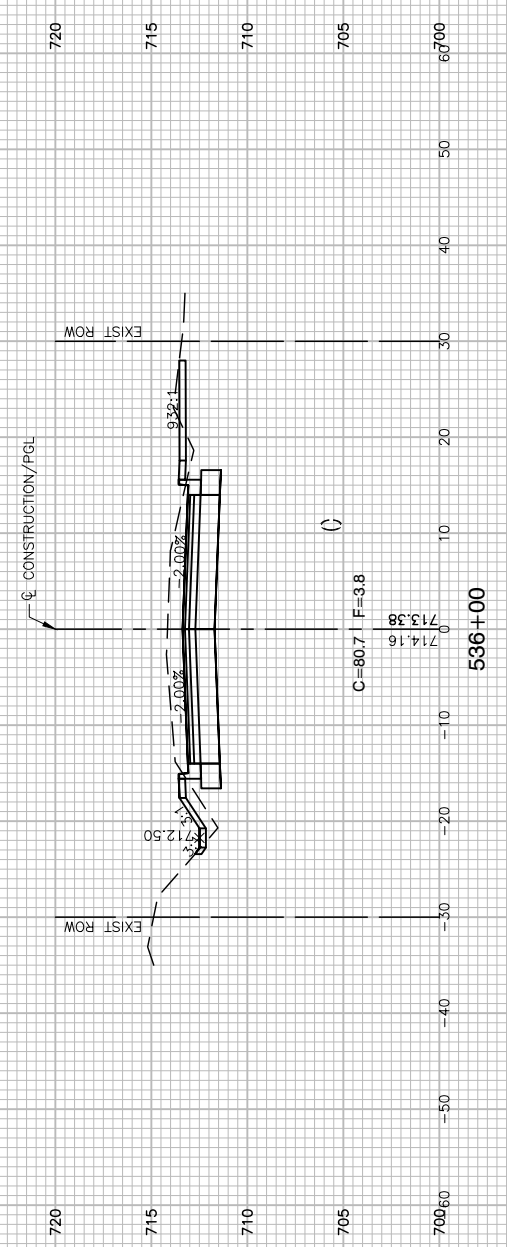
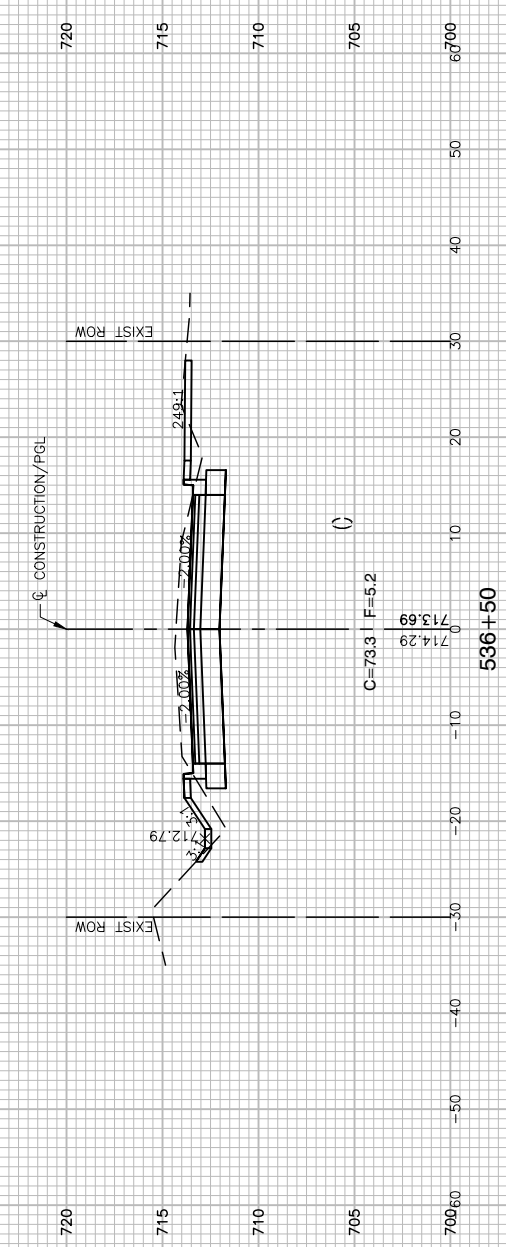
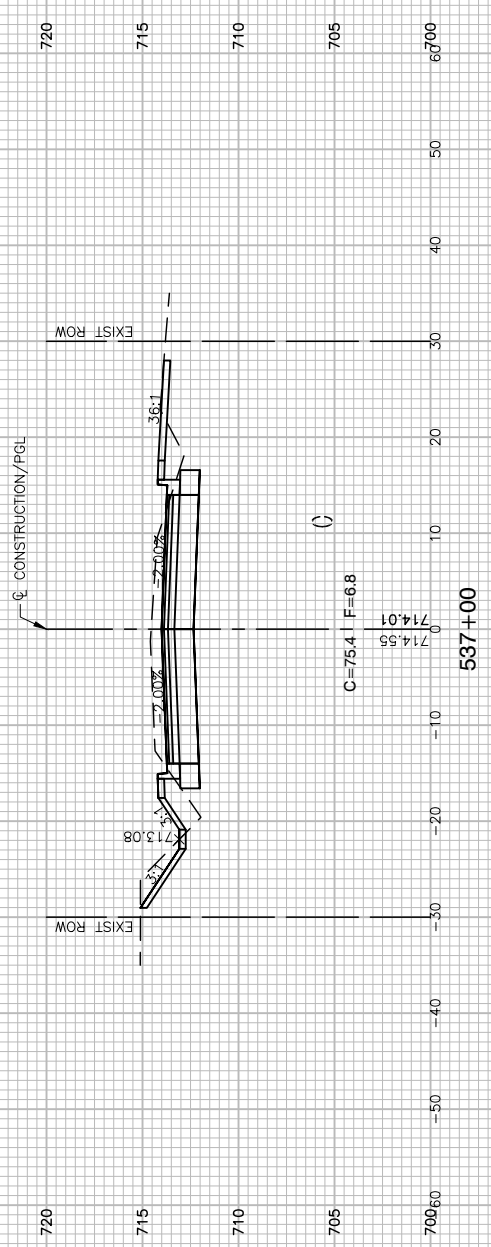
USER NAME =	DESIGNED -- JPH	REVISED --
	CHECKED -- WPD	REVISED --
PLOT SCALE =	DRAWN -- RG	REVISED --
PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS

SCALE: H 1"=10' V 1"=5' SHEET NO. 48 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	48
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				

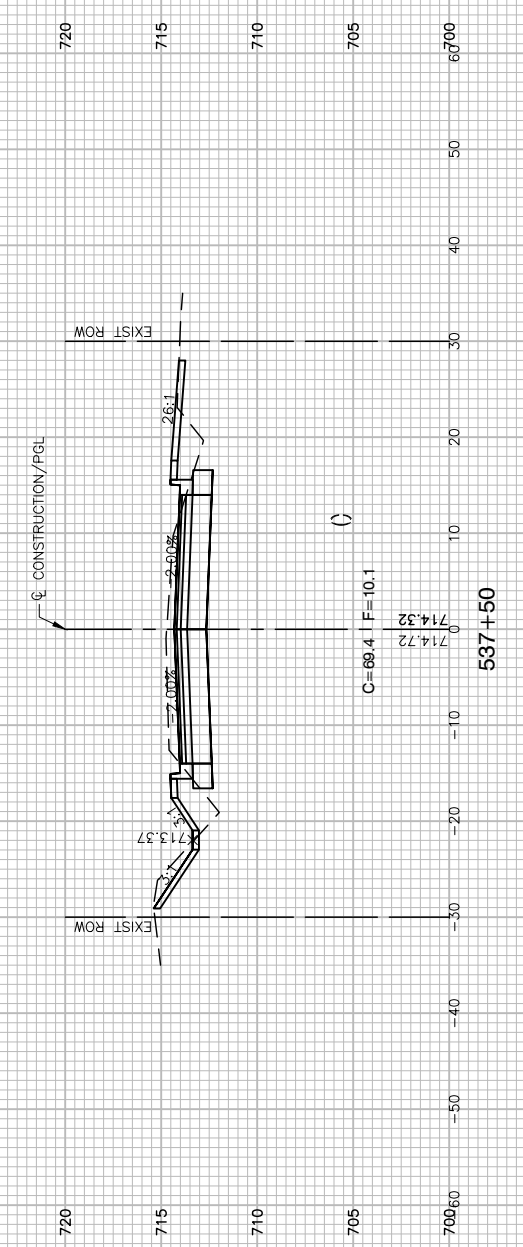
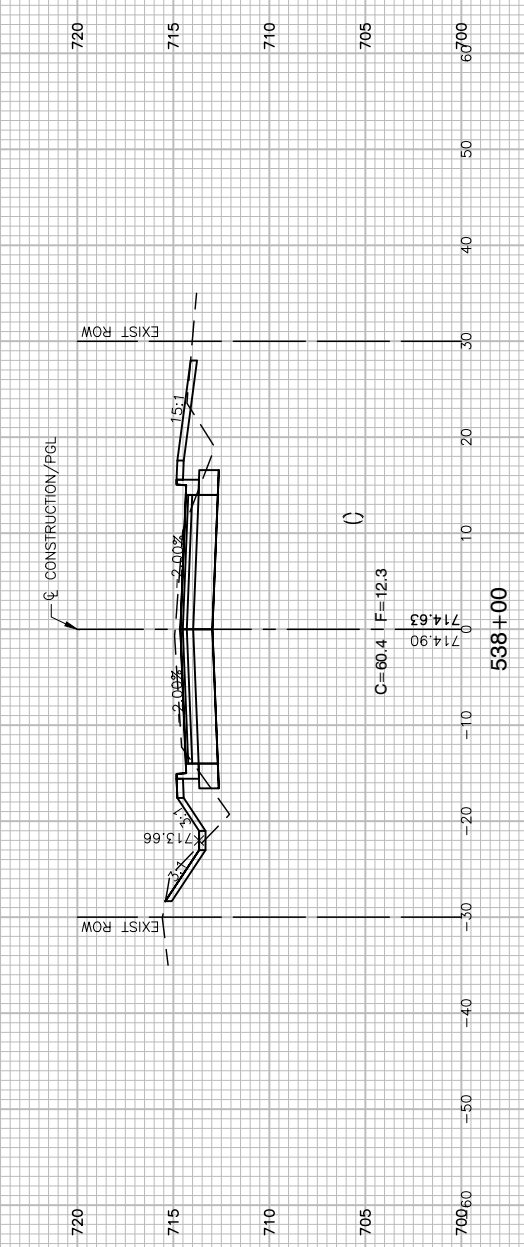
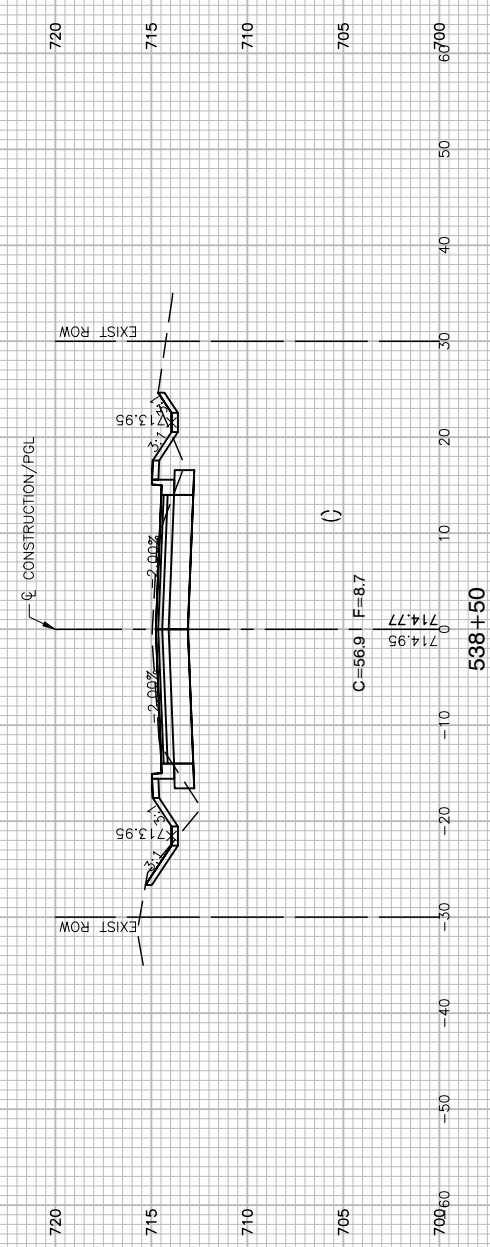
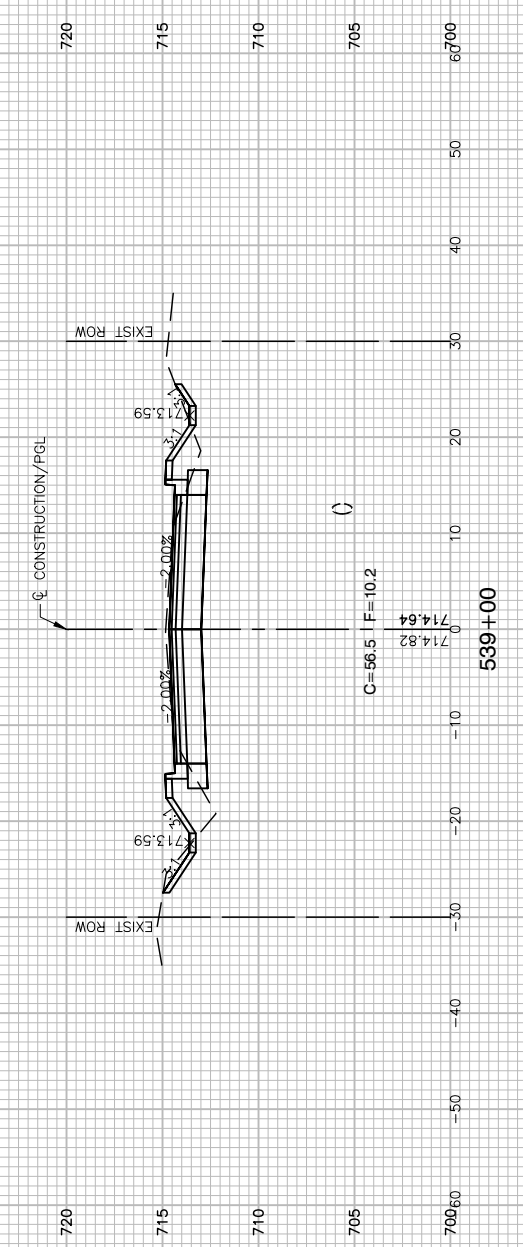
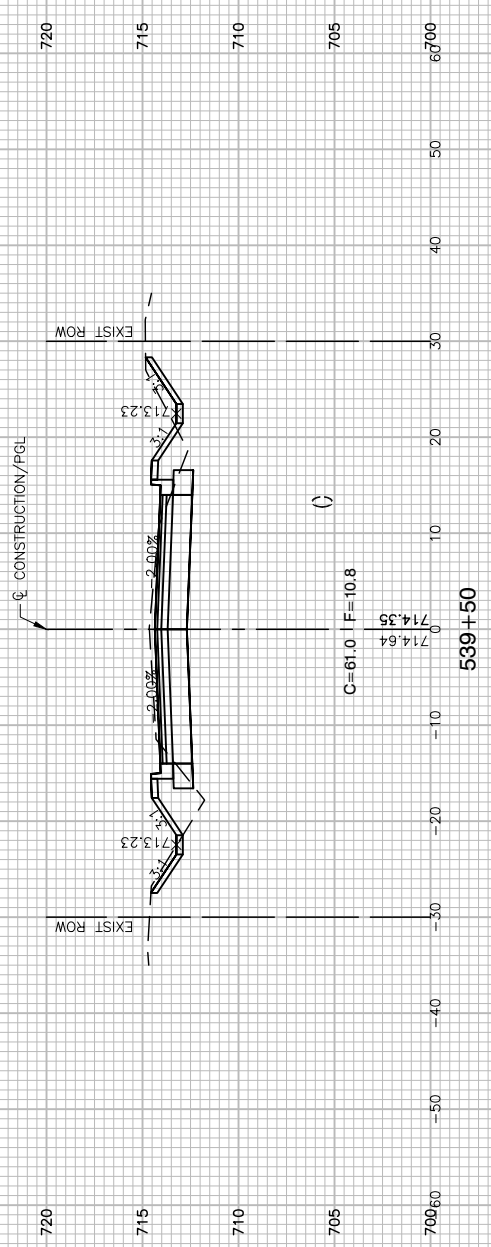


FILE NAME = 21R0316-XSCT-01 - X17	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 49	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	49
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



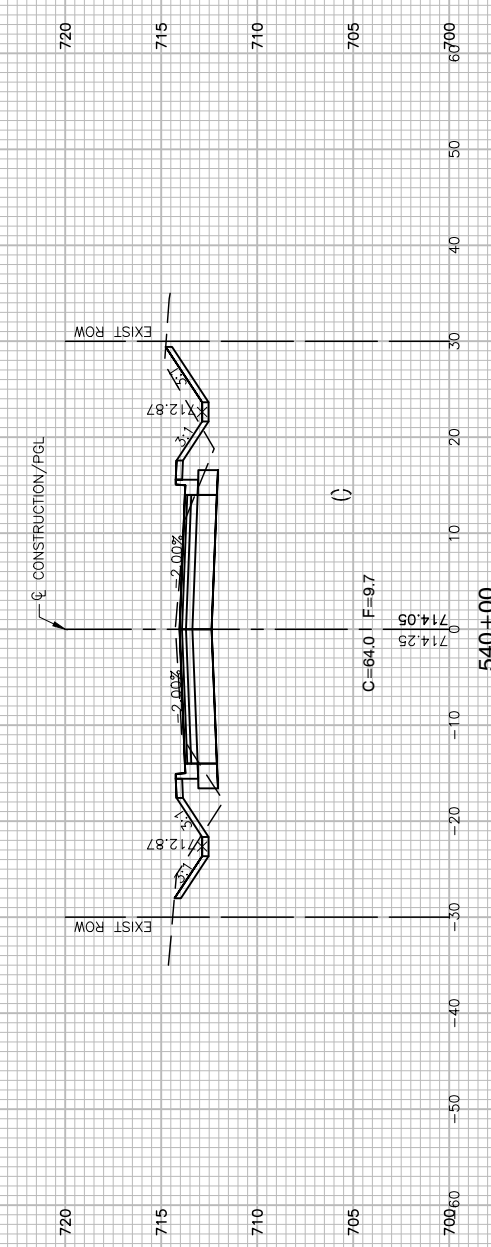
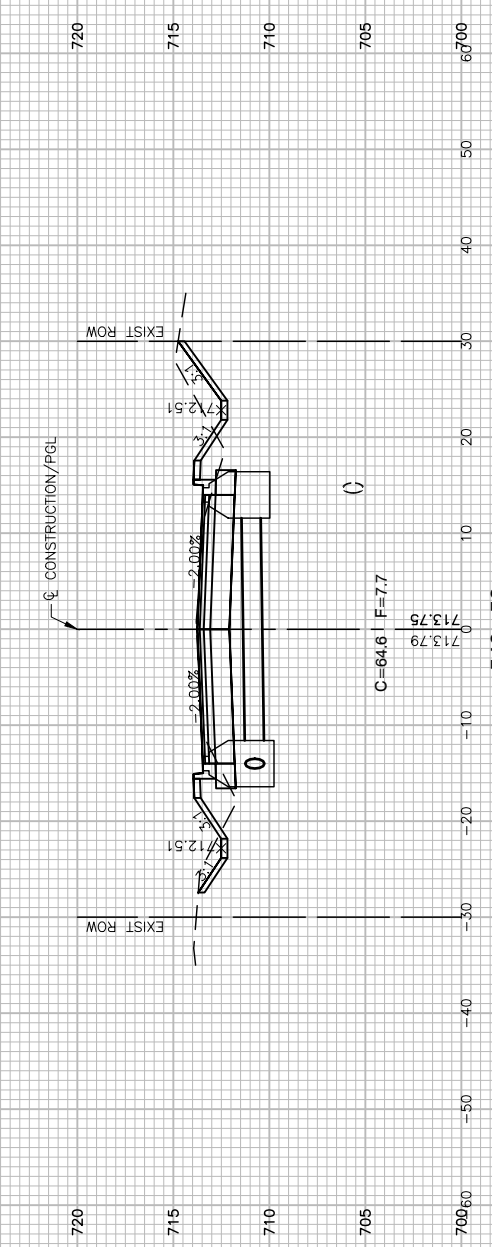
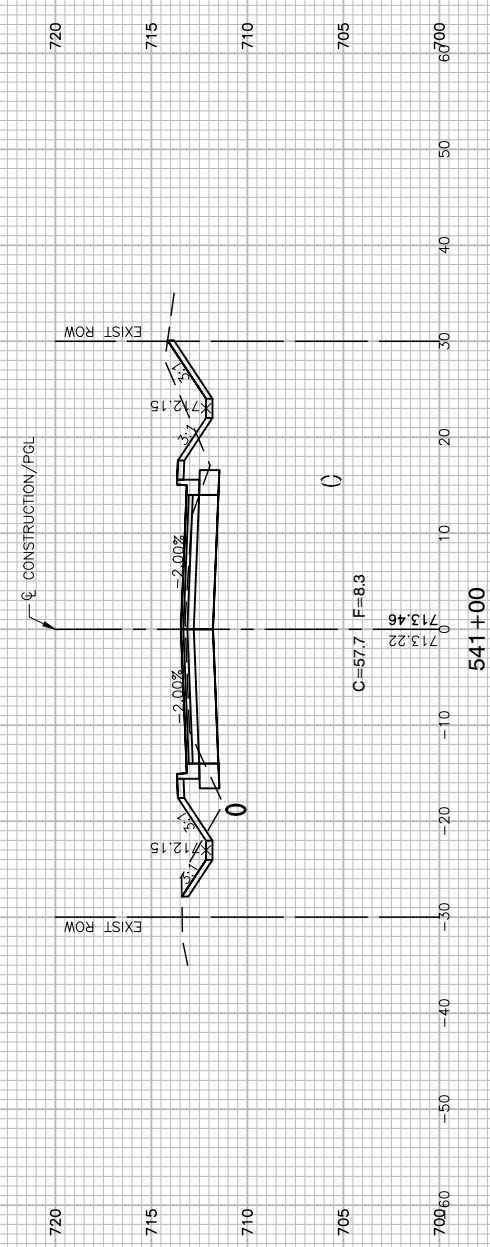
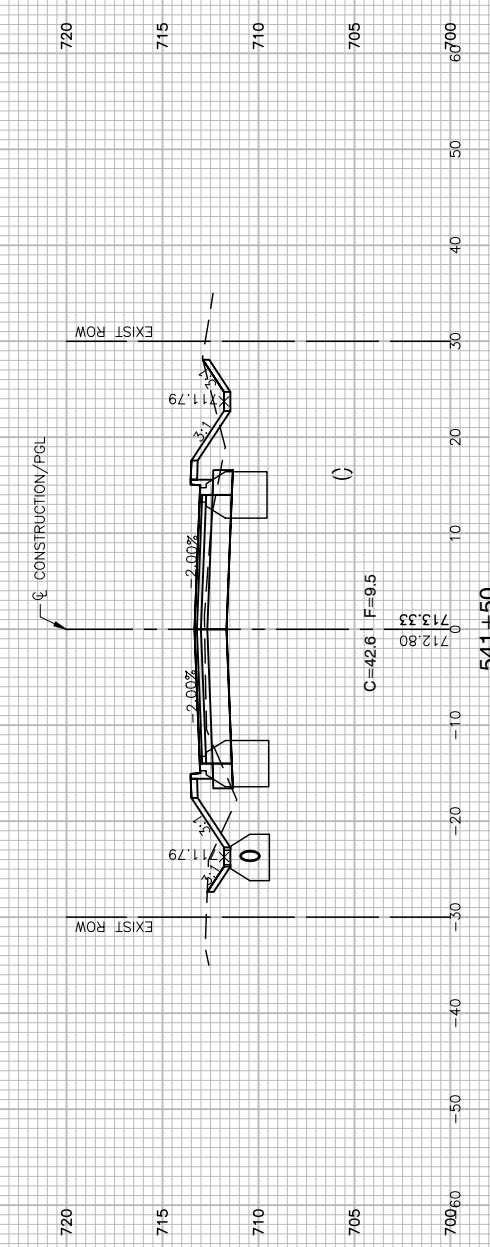
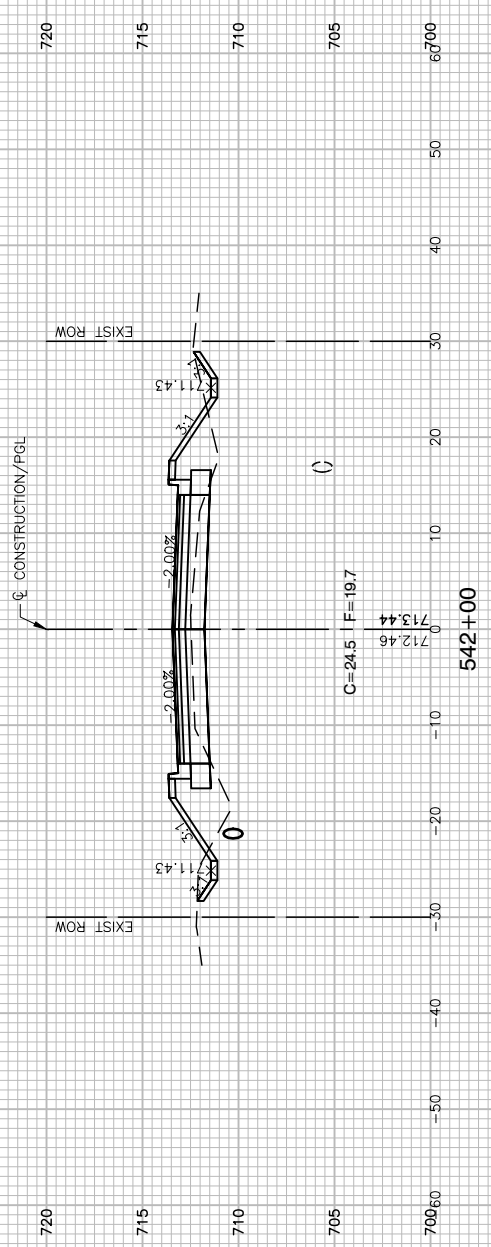
FILE NAME = 21R0316-XSCT-01 - X18	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 50 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	50
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)			CONTRACT NO. 61H99	

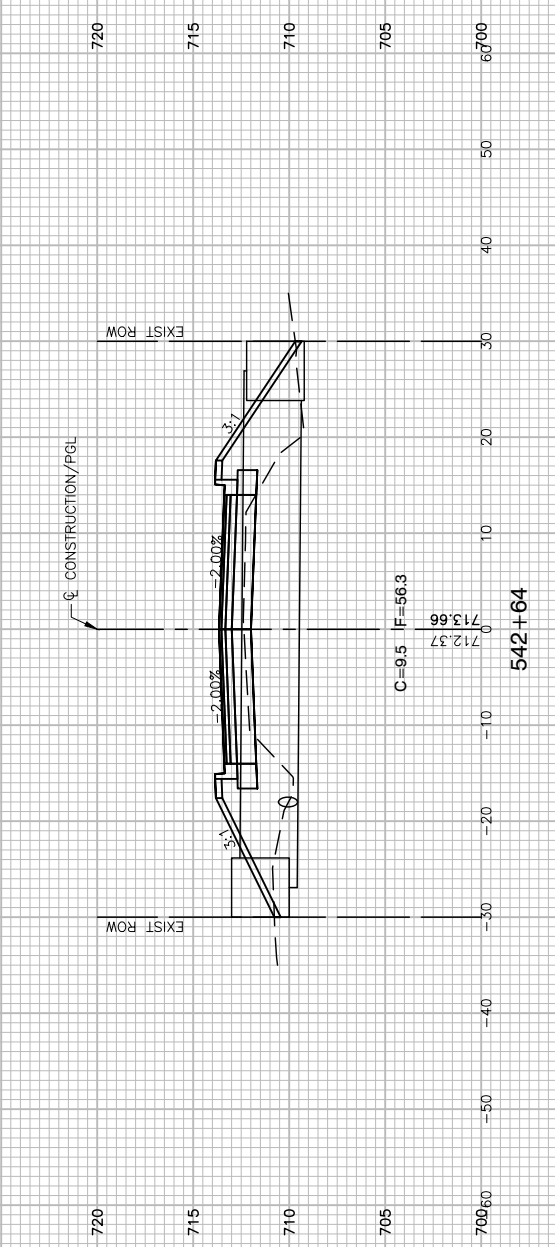
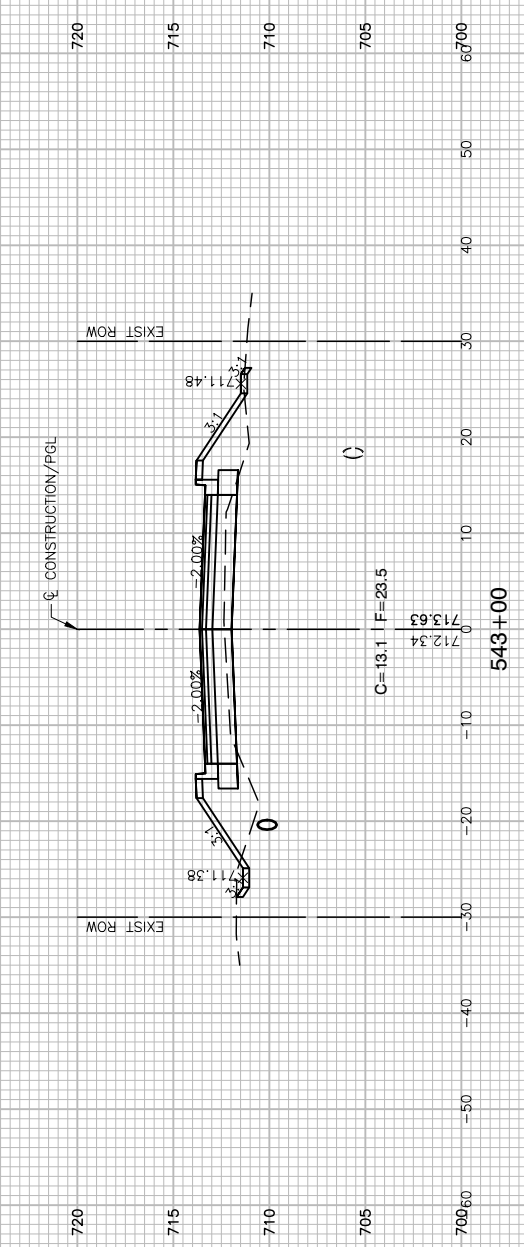
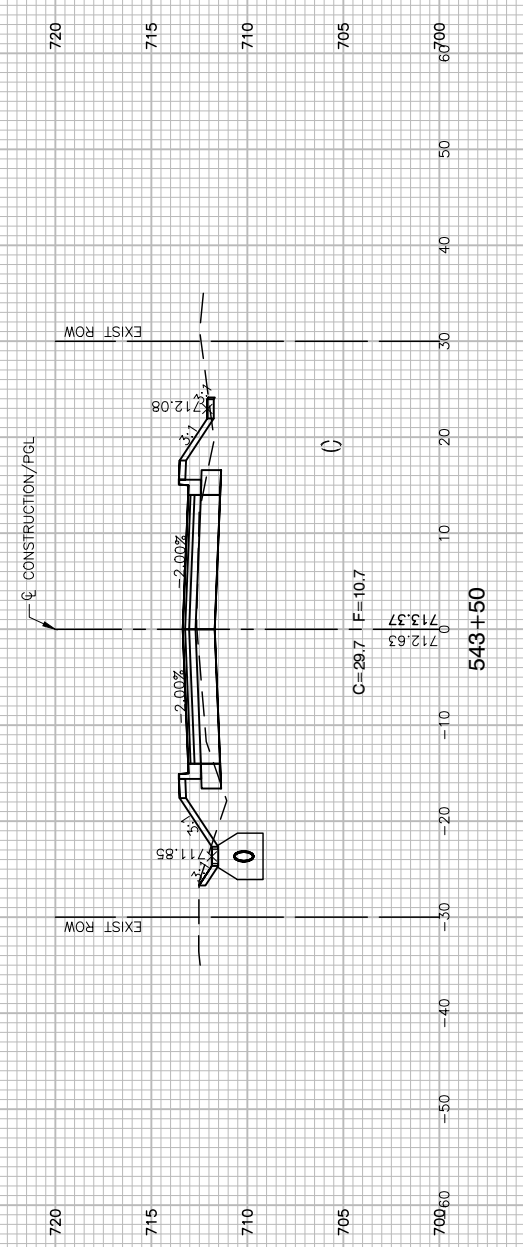
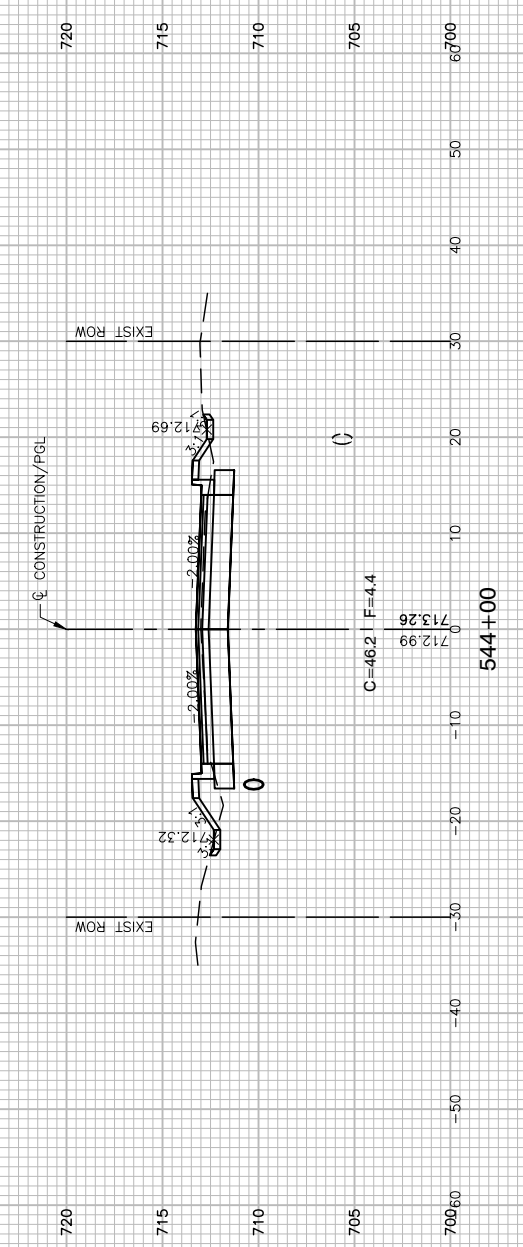
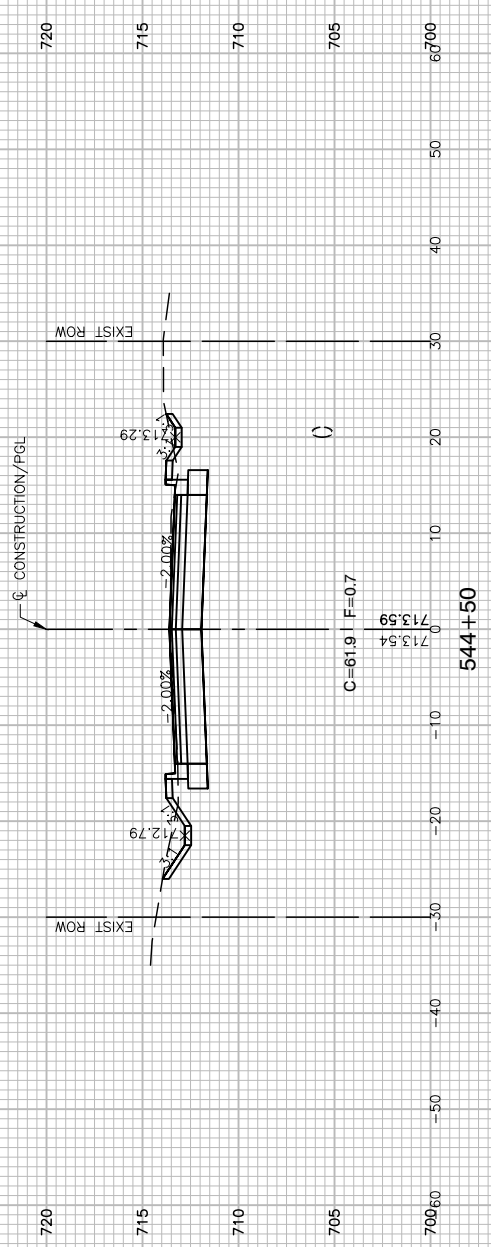


FILE NAME = 21R0316-XSCT-01 - X19	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CRETE ROAD ROADWAY WIDENING/RESURFACING/RECONSTRUCTION CROSS SECTIONS			
SCALE: H 1"=10' V 1"=5'	SHEET NO. 51	OF 52 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	51
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT UMBW(332)				



FILE NAME = 21R0316-XSCT-01 - X20	USER NAME =	DESIGNED -- JPH	REVISED --
		CHECKED -- WPD	REVISED --
	PLOT SCALE =	DRAWN -- RG	REVISED --
	PLOT DATE = 11-16-22	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CRETE ROAD
ROADWAY WIDENING/RESURFACING/RECONSTRUCTION
CROSS SECTIONS**

SCALE: H 1"=10' V 1"=5' SHEET NO. 52 OF 52 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4309	15-00049-00-PV	WILL	52	52
CONTRACT NO. 61H99				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	UMBW(332)	